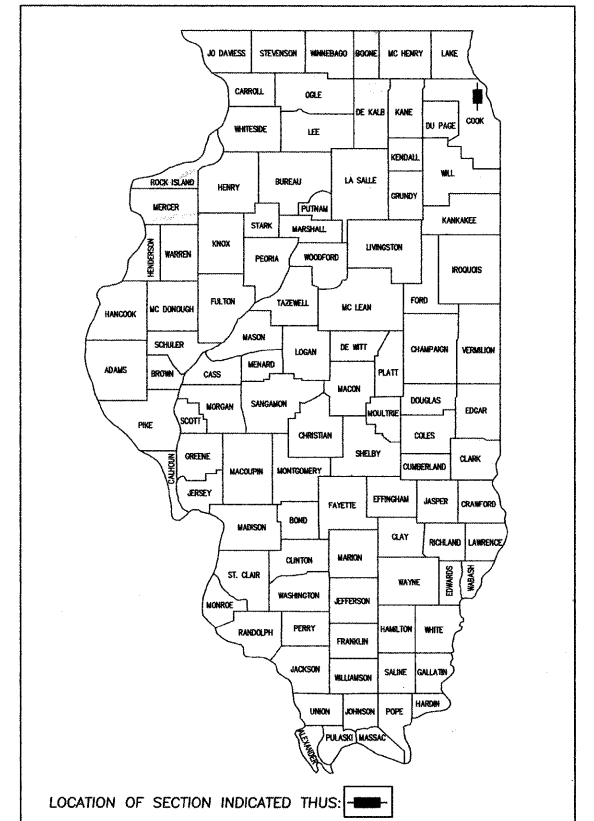


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

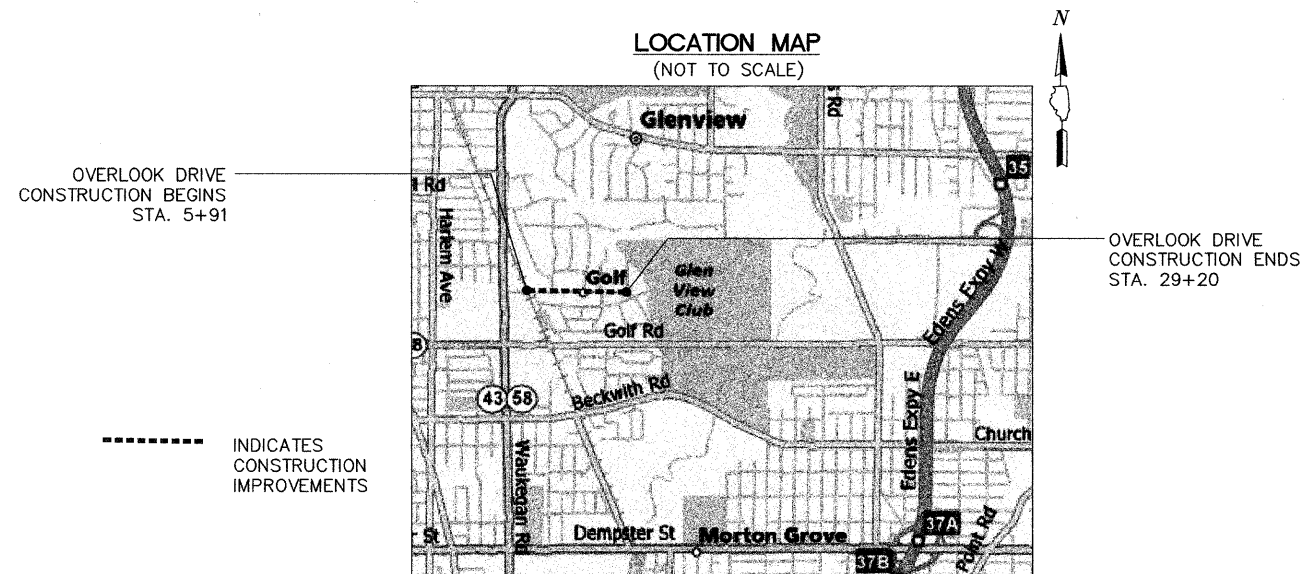
FAU 1311/ OVERLOOK DRIVE
WAUKEGAN ROAD TO GOLF ROAD
SECTION: 09-00006-00-RS
JOB NO. 91-585-09
VILLAGE OF GOLF, ILLINOIS
COOK COUNTY



INDEX OF SHEETS

1. COVER SHEET, INDEX OF SHEETS, LIST OF STATE STANDARDS
2. GENERAL NOTES & SUMMARY OF QUANTITIES
3. TYPICAL SECTIONS
4. TRAFFIC CONTROL & PROTECTION FOR SIDE ROADS, INTERSECTIONS & DRIVEWAYS
5. DISTRICT ONE TYPICAL PAVEMENT MARKINGS
6. BUTT JOINT AND HMA TAPER DETAILS

LOCATION MAP
(NOT TO SCALE)



PROJECT INFORMATION
GROSS LENGTH & NET LENGTH = 2,329 FT (0.44 MI)
ADT = 2450 VPD (2009)
POSTED SPEED LIMIT = 20 MPH
ROAD CLASSIFICATION = URBAN COLLECTOR

GEWALT HAMILTON
ASSOCIATES, INC.
Consulting Engineers & Surveyors
850 Forest Edge Drive
Vernon Hills, IL 60061
847-478-9700
FAX 847-478-9701

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED: <u>5-28-09</u>	2009
<u>[Signature]</u> VILLAGE OF GOLF - VILLAGE PRESIDENT	
PASSED: <u>MAY 29, 2009</u>	2009
<u>[Signature]</u> DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS	
RELEASING FOR BID BASED ON LIMITED REVIEW: <u>MAY 29</u>	2009
<u>[Signature]</u> DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER	

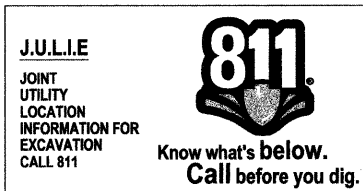
PROFESSIONAL ENGINEER'S SIGNATURE & SEAL

[Signature]
PATRICK J. GLENN
EXPIRES: 11-30-09

SEAL

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

THIS PLAN SET CONTAINS NO DRAWINGS
AT STANDARD ENGINEERING SCALES



EXISTING UTILITIES: WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF THE ENGINEER AS TO THE LOCATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE BIDDER. THE ENGINEER AND OWNER ASSUME NO RESPONSIBILITY WHATEVER IN RESPECT TO THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS RELATIVE TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES OR THE MANNER IN WHICH THEY ARE TO BE REMOVED OR ADJUSTED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES. HE SHALL ALSO OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES, DETAILED INFORMATION RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULES OF THE UTILITY COMPANIES FOR REMOVING OR ADJUSTING THEM.

CONTRACTOR IS RESPONSIBLE FOR CONTACTING J.U.L.I.E. AT 1-800-892-0123 AND MUST ACQUIRE A DIG NUMBER A MINIMUM OF 72 HOURS PRIOR TO ANY WORK BEING DONE.

CONTRACT NO. 63191

FILE NAME = 9434-008_Overlook.dwg	USER NAME =	DESIGNED - BVS	REVISED - 5/18/09 PER IDOT COMMENTS	OVERLOOK DRIVE RESURFACING PROGRAM	TITLE SHEET	F.A.U. RTE. 1311	SECTION 09-00006-00-RS	COUNTY COOK	TOTAL SHEETS 6	SHEET NO. 1
	PLOT SCALE = N.T.S.	CHECKED - PJG	REVISED - 5/28/09 PER IDOT COMMENTS							
	PLOT DATE = 4/7/09	DATE - 4/7/09				SCALE: NONE	SHEET NO. 1 OF 6	SHEETS STA.	TO STA.	ILLINOIS FED. AID PROJECT

ASSOCIATE FIELD ENGINEER: KEVIN STALLWORTH (847) 705-4169

GENERAL NOTES

A-1 THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" LATEST EDITION, THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" LATEST EDITION, PROJECT SPECIFICATIONS, ALL APPLICABLE REQUIREMENTS OF THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, MUNICIPALITY, ORDINANCES OF AUTHORITIES HAVING JURISDICTION AND ALL ADDENDA THERE TO SHALL GOVERN THIS WORK.

A-2 ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.

A-3 NO CONSTRUCTION PLANS SHALL BE USED FOR CONSTRUCTION UNLESS SPECIFICALLY MARKED 'FOR CONSTRUCTION'. PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR MUST VERIFY THE ENGINEERS' LINE AND GRADES. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTION FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSION OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTION, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.

A-4 IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.

A-5 EASEMENTS FOR THE EXISTING UTILITIES, BOTH PUBLIC AND PRIVATE, AND UTILITIES WITHIN PUBLIC RIGHTS-OF-WAY ARE SHOWN ON THE PLANS ACCORDING TO AVAILABLE RECORDS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE EXACT LOCATION IN THE FIELD OF THESE UTILITY LINES AND THEIR PROTECTION FROM DAMAGE DUE TO CONSTRUCTION OPERATIONS. IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT IN LOCATION WITH NEW CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT MAY BE RESOLVED.

A-6 SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.

A-7 THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

A-8 OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.

A-9 HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

A-10 QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL IDENTIFY FINAL PATCH LOCATIONS IN THE FIELD.

A-11 WHENEVER, DURING CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF GUTTERS, DRAINAGE STRUCTURES, DITCHES, ETC. SUCH THAT THE NATURAL FLOW LINE OF WATER IS OBSTRUCTED, THE LOOSE MATERIAL WILL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATION, ALL DRAINAGE STRUCTURES AND FLOW LINES SHALL BE FREE FROM DIRT AND DEBRIS. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR'S FAILURE TO PROVIDE THE ABOVE WILL PRECLUDE ANY POSSIBLE ADDED COMPENSATION REQUESTED DUE TO DELAYS OR UNSUITABLE MATERIALS CREATED AS A RESULT THEREOF.

STORM SEWERS, WATER MAINS, AND UTILITIES

B-1 EXISTING UTILITIES: WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF THE ENGINEER AS TO THE LOCATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE CONTRACTOR. THE ENGINEER AND OWNER ASSUME NO RESPONSIBILITY WHATSOEVER IN RESPECT TO THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS RELATIVE TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES OR THE MANNER IN WHICH THEY ARE TO BE REMOVED OR ADJUSTED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PROCESS OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. HE SHALL ALSO OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULES OF THE UTILITY COMPANIES FOR REMOVING OR ADJUSTING THEM.

B-2 THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING JULIE AS WELL AS LOCAL CABLE TV COMPANIES AND THE VILLAGE. THE FACILITIES SHALL BE LOCATED PRIOR TO ANY WORK WITHIN ANY EASEMENT, R.O.W, OR SUSPECTED UTILITY LOCATION.

B-3 THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THROUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.

B-4 ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.

B-5 THE CONTRACTOR SHALL ENSURE THAT ALL WATER SYSTEM VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE TO THE VILLAGE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISABLE AT ALL TIMES.

B-6 LOCATION OF STRUCTURES TO BE ADJUSTED WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

SIGNING AND STRIPING

C-1 SEE IDOT STANDARD DETAIL 780001 AND DISTRICT ONE DETAIL TC-13.

C-2 THERMOPLASTIC STRIPING SHALL MATCH EXISTING LOVATIONS

TRAFFIC CONTROL

D-1 SEE TRAFFIC CONTROL HIGHWAY STANDARDS CONCERNING TRAFFIC CONTROL AND PROTECTION.

DEBRIS REMOVAL

MATERIALS RESULTING FROM THE REMOVAL OF ASPHALT SURFACES, UTILITY ADJUSTMENTS, RESTORATION WORK, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGEMENT OF THE CITY, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE CITY WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR SHALL BE BILLED (CHARGED) ACCORDINGLY.

WATER SUPPLY

THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE. IF THE WATER IS FROM A SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO USE OF THE WATER.

DRIVEWAY CLOSING

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE VILLAGE WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO SIDEWALK REPLACEMENT, AND/OR DRIVEWAY REPLACEMENT. AT LOCATIONS WHERE THE DRIVEWAY IS SCHEDULED TO BE REMOVED, THE CONTRACTOR SHALL CONTACT THE BUSINESS/HOMEOWNER 24 HOURS PRIOR TO REMOVING THE CURB, SIDEWALK, OR DRIVE APPROACH. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES. THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE A DRIVEWAY FOR MORE THAN 8 HOURS UNDER ANY CIRCUMSTANCE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE BARRICADES TO PREVENT TRAFFIC FROM USING THE DRIVEWAYS DURING THIS PERIOD.

STREET SWEEPING AND PREPARATION

THE CONTRACTOR SHALL BE RESPONSIBLE FOR SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF PLACING PRIME COAT AND THE LAYING OF HMA, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS AND ALL LOOSE MATERIAL.

SUMMARY OF QUANTITIES

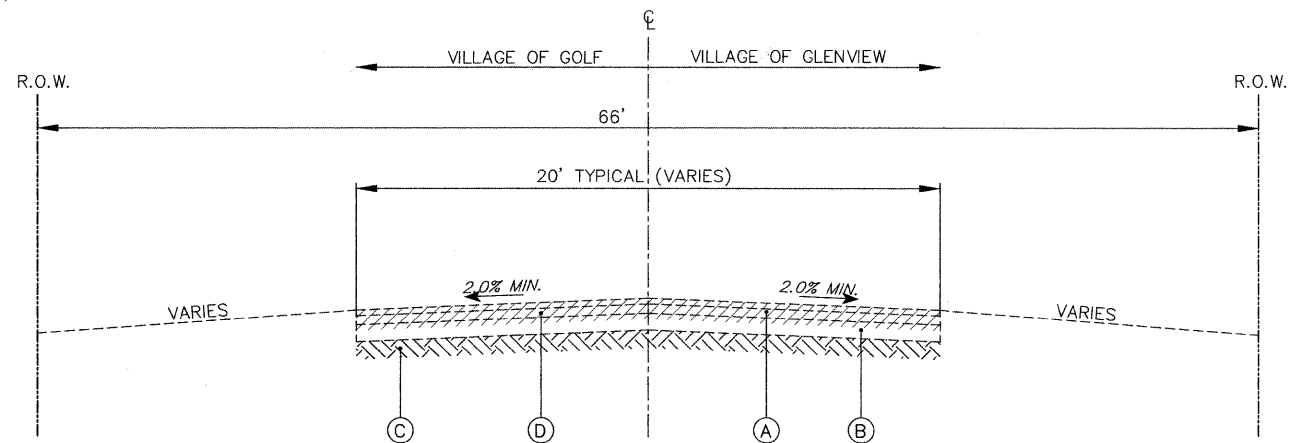
PAYCODE	DESCRIPTION	UNIT	VILLAGE OF GOLF	VILLAGE OF GLENVIEW	ROADWAY
					100% FEDERAL
20201200	Removal and Disposal of Unsuitable Material	CY	25.00	25.00	50.00
21101505	Topsoil Excavation and Placement	CY	83.50	83.50	167.00
XXXX0115	Seeding - Class IA (Special)	SY	750.00	750.00	1500.00
25000400	Nitrogen Fertilizer Nutrient	LBS	14.00	14.00	28.00
25000500	Phosphorus Fertilizer Nutrient	LBS	14.00	14.00	28.00
25000600	Potassium Fertilizer Nutrient	LBS	14.00	14.00	28.00
25100630	Erosion Control Blanket, NAG DS150 ECB	SY	750.00	750.00	1500.00
35101600	Aggregate Base Course, Type B, 4"	SY	12.50	12.50	25.00
35800100	Preparation of Base	SY	2,655.00	2,655.00	5310.00
35800200	Aggregate Base Repair	TON	118.00	118.00	236.00
40201000	Aggregate for Temporary Access	TON	70.00	70.00	140.00
40600200	Bituminous Materials (Prime Coat)	GAL	291.50	291.50	583.00
40600300	Aggregate (Prime Coat)	TON	12.00	12.00	24.00
40600982	HMA Surface Removal - Butt Joint	SY	405.00	405.00	810.00
40603080	HMA Binder Course, IL-19.0, N50	TON	655.00	655.00	1310.00
40603310	HMA Surface Course, Mix "C", N50	TON	327.50	327.50	655.00
42300100	Portland Cement Concrete Driveway Pavement, 5"	SY	12.50	12.50	25.00
44000100	Pavement Removal, 6"	SY	2,655.00	2,655.00	5310.00
44000200	Driveway Pavement Removal	SY	86.00	86.00	172.00
44000500	Combination Curb and Gutter Removal	FT	25.00	25.00	50.00
60300310	Frames and Lids To Be Adjusted (Special)	Each	6.00	5.00	11.00
60603800	Combination Concrete Curb and Gutter, Type B-6.12	FT	25.00	25.00	50.00
67100100	Mobilization	LS	0.50	0.50	1.00
70102620	Traffic Control and Protection - Standard 701501	LS	0.50	0.50	1.00
* 78000100	Thermoplastic Pavement Marking - Letters and Symbols	SF	30.60	30.60	61.20
* 78000650	Thermoplastic Pavement Marking - Line 24"	LF	18.00	18.00	36.00
Z0048665	Railroad Protective Liability Insurance	LS	0.50	0.50	1.00

* DENOTES SPECIALTY ITEM

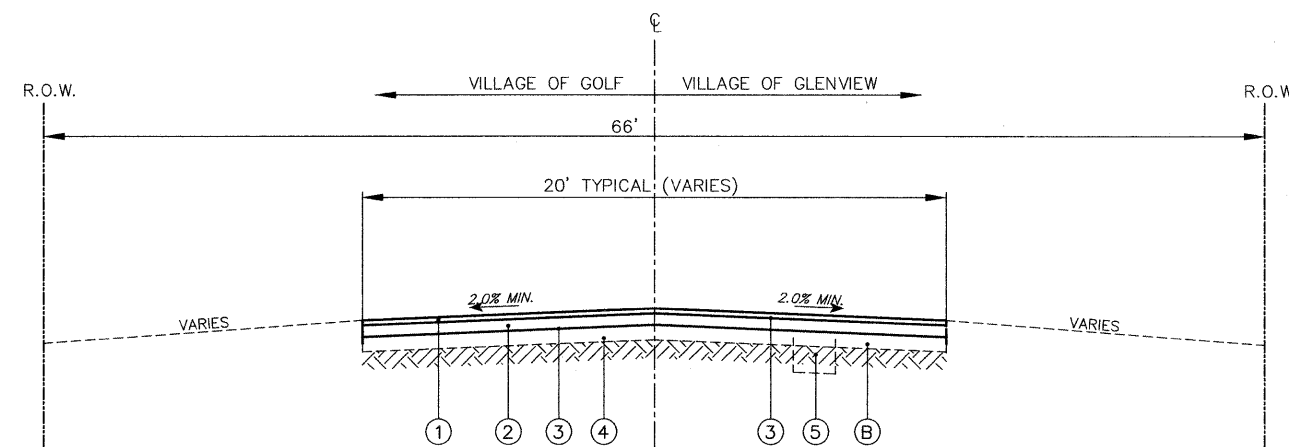
HIGHWAY STANDARDS:

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701501-05	LANE CLOSURE, 2L, 2W, UNDIVIDED
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS

FILE NAME = 9434-008_Overlook.dwg	USER NAME =	DESIGNED - BVS	REVISED - 5/18/09 PER IDOT COMMENTS	OVERLOOK DRIVE RESURFACING PROGRAM	GENERAL NOTES AND SUMMARY OF QUANTITIES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 4/7/09	CHECKED - PJG	DATE - 4/7/09			CONTRACT NO. 63191				
						SCALE: NONE	SHEET NO. 2	OF 6	SHEETS	STA. TO STA.
ILLINOIS FED. AID PROJECT										



EXISTING TYPICAL CROSS SECTION
OVERLOOK DRIVE (FAU ROUTE 1311)
STA 5+91 TO STA 29+20



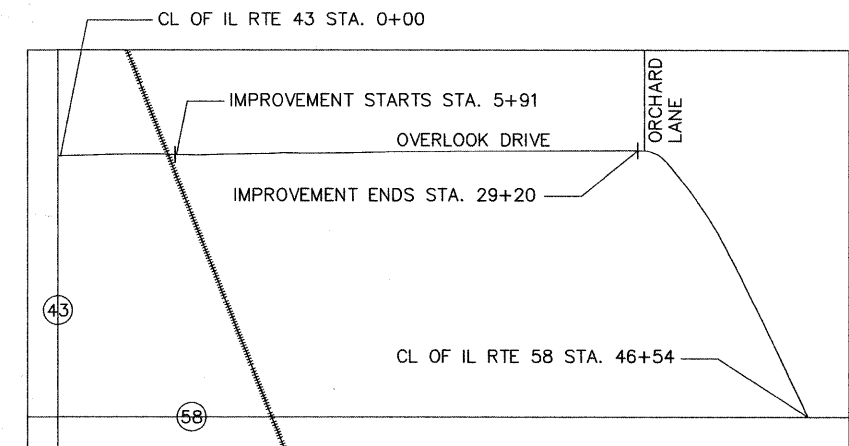
PROPOSED TYPICAL CROSS SECTION
OVERLOOK DRIVE (FAU ROUTE 1311)
STA 5+91 TO STA 29+20

NOTES:

- PAVEMENT REMOVAL SHALL INCLUDE REMOVAL OF 2"-4" EXISTING HMA SURFACE AND 2"-4" STONE BASE COURSE TO A DEPTH OF 6"
- INSTALLATION OF HMA BINDER COURSE SHALL IMMEDIATELY FOLLOW PAVEMENT REMOVAL TO LEAVE NO MORE THAN A 2" DIFFERENTIAL IN PAVEMENT SURFACE ELEVATIONS AT THE END OF EACH DAY
- CURB AND GUTTER REMOVAL AND REPLACEMENT IN LOCATIONS AS MARKED BY THE ENGINEER.
- RESIDENTIAL DRIVEWAY HMA PAVEMENT TO BE REMOVED AND REPLACED AS MARKED IN THE FIELD BY THE ENGINEER (2.00" THICKNESS).

EXISTING LEGEND:

- (A) EXISTING HMA PAVEMENT, 2"-4" & VARIES
- (B) EXISTING AGGREGATE BASE, TYPE B, 7"-9" & VARIES
- (C) EXISTING SUBBASE
- (D) PAVEMENT REMOVAL, 6" (2"-4" HMA SURFACE & 2"-4" STONE BASE)



PROJECT SCHEMATIC

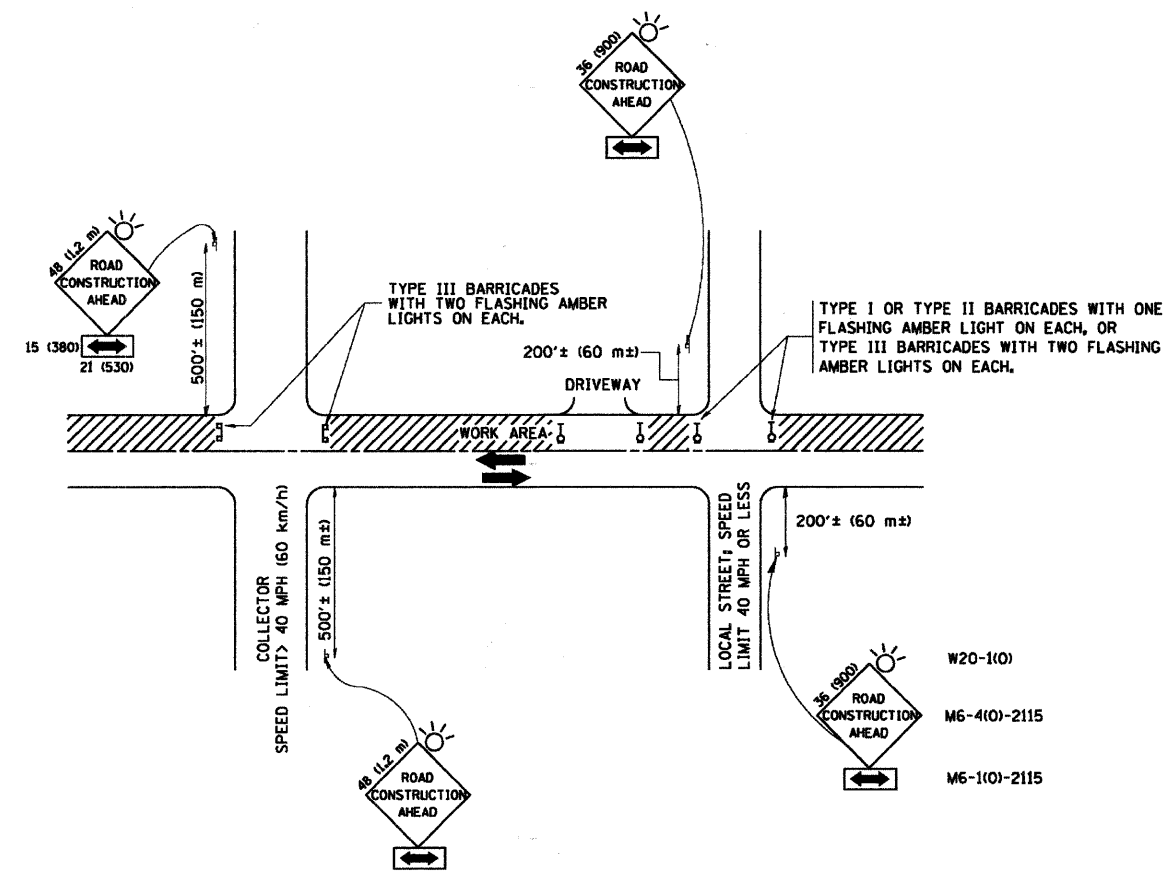
PROPOSED LEGEND

- ① HOT-MIX ASPHALT SURFACE COURSE, MIX 'C', N50, 2"
- ② HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 4" (TWO LIFTS)
- ③ BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)
- ④ REPAIR/PREPARATION OF AGGREGATE SUB-BASE
- ⑤ REMOVAL OF UNSUITABLE MATERIAL (AS NECESSARY)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS TABLE

MIXTURE TYPE	AC TYPE	AIR VOIDS
HOT MIX ASPHALT SURFACE COURSE, MIX 'C', N50, 2" (IL-9.5mm)	PG 64 -22	4% @ 50 GYR
HOT MIX ASPHALT BINDER COURSE IL-19.0, N50	PG 64 -22*	4% @ 50 GYR

THE UNIT WEIGHT TO CALCULATE ALL HMA SURFACE MIXTURE QUALITIES IS 112 LBS/SQ/IN
*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

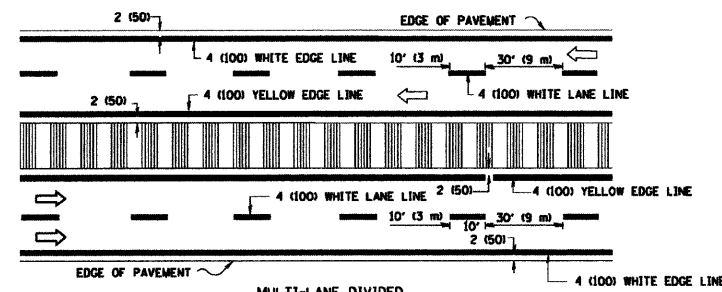
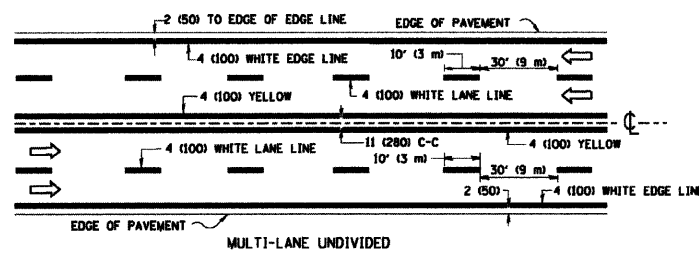
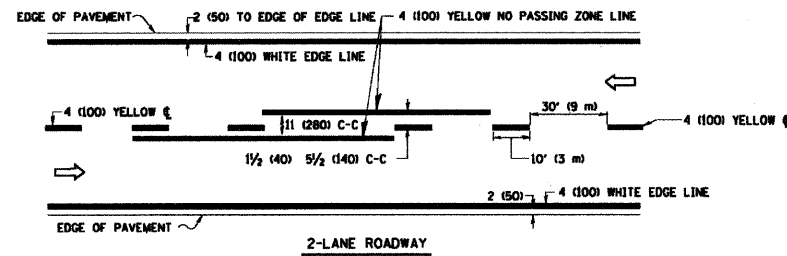
- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

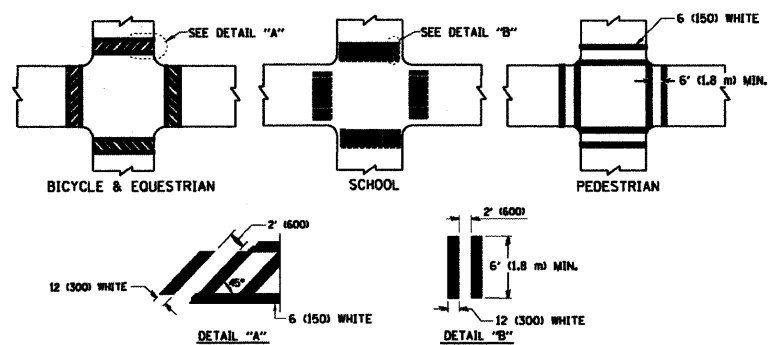
All dimensions are in millimeters (inches) unless otherwise shown.

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	PLOT DATE = 1/4/2009	DATE - 06-89	REVISOR - A. HOUSEH 10-15-96				TO STA.	CONTRACT NO.				
			REVISOR - T. RAMMACHER 01-06-00					ILLINOIS FED. AID PROJECT				

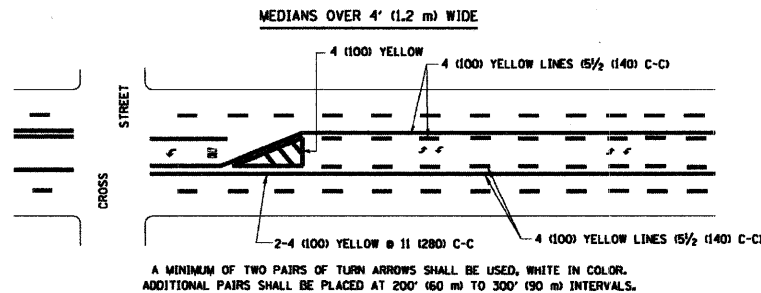
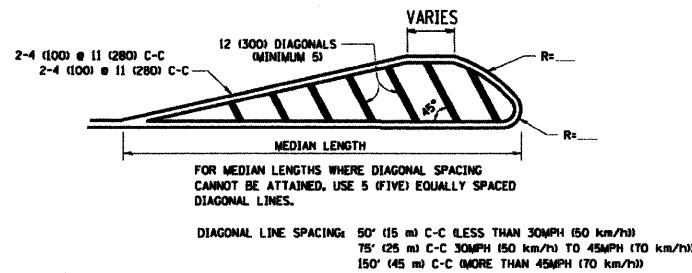
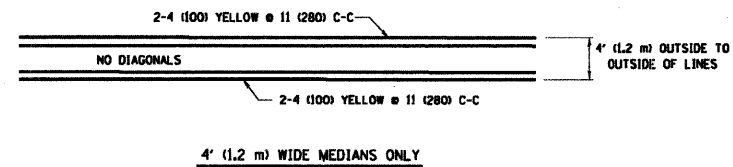
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	PLOT DATE = 4/7/09	DATE - 4/7/09					TO STA.	CONTRACT NO. 63191				
								ILLINOIS FED. AID PROJECT				



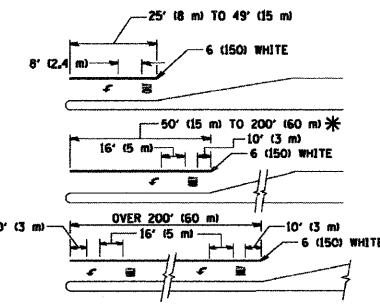
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

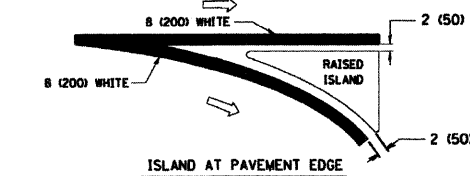
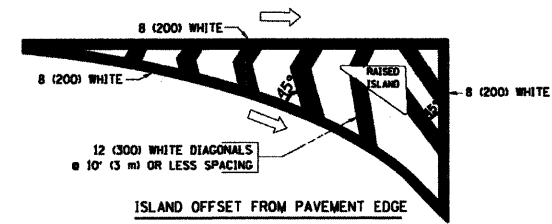


TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 1/4 AREA = 15.6 SQ. FT. (1.5 m²) 1/2 AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE, FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN A. DIAGONALS (GORE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL))	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH) 20' (6 m) C-C (30MPH TO 45MPH) 30' (9 m) C-C (OVER 45MPH)
RAILROAD CROSSING	24 (600) TRANSVERSE LINES "RR" IS 6' (1.8 m) LETTERS 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD T80001 AREA OF "X" IS 3.6 SQ. FT. (0.33 m ²) EACH "X" IS 54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH) 75' (23 m) C-C (30 MPH TO 45MPH) 150' (45 m) C-C (OVER 45MPH)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD T80001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\data\22x34\to13.dgn	USER NAME = geglianobt	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
	PLOT SCALE = 58,000' / IN.	DRAWN - BVS	REVISED - A. HOUSEH 10-09-96
	PLOT DATE = 1/4/2008	CHECKED - P.J.G.	REVISED - A. HOUSEH 10-17-96
		DATE - 03-19-90	REVISED - T. RAMMACHER 01-06-00

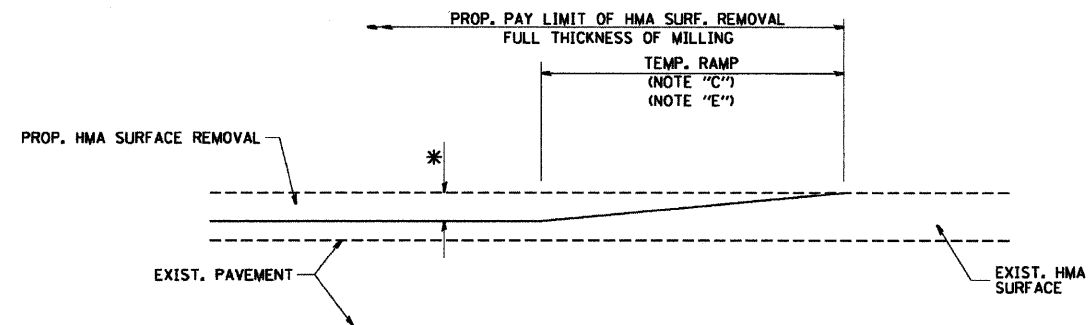
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS						
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	CONTRACT NO.		
		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE NAME = 9434-008_Overlook.dwg	USER NAME =	DESIGNED - BVS	REVISED - 5/18/09 PER IDOT COMMENTS
	PLOT SCALE = N.T.S.	DRAWN - BVS	REVISED - 5/28/09 PER IDOT COMMENTS
	PLOT DATE = 4/7/09	CHECKED - P.J.G.	
		DATE - 4/7/09	

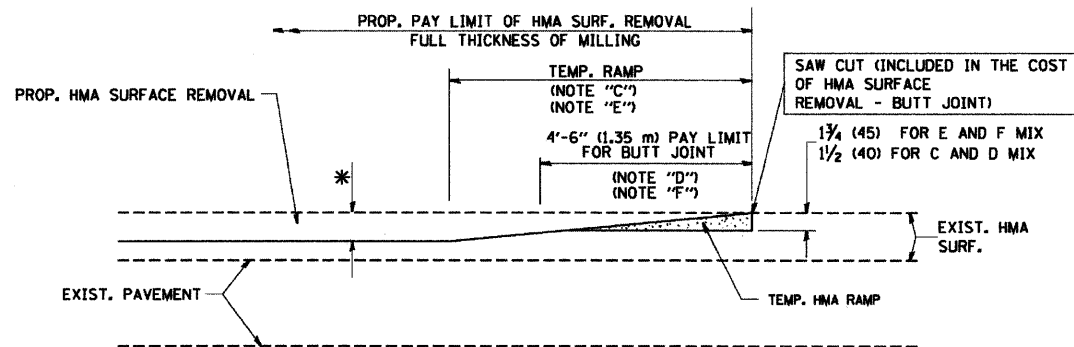
OVERLOOK DRIVE
RESURFACING PROGRAM

DISTRICT ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DETAILS						
SCALE: NONE	SHEET NO. 5 OF 6 SHEETS	STA.	TO STA.	CONTRACT NO. 63191		
		ILLINOIS FED. AID PROJECT				



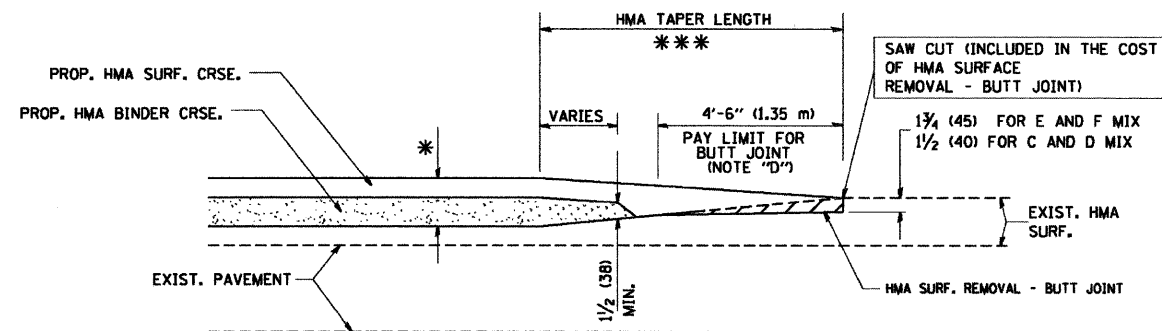
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

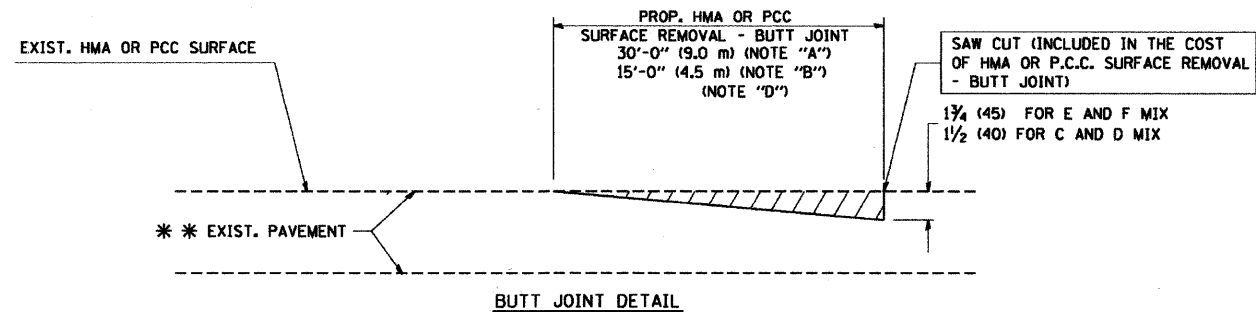


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

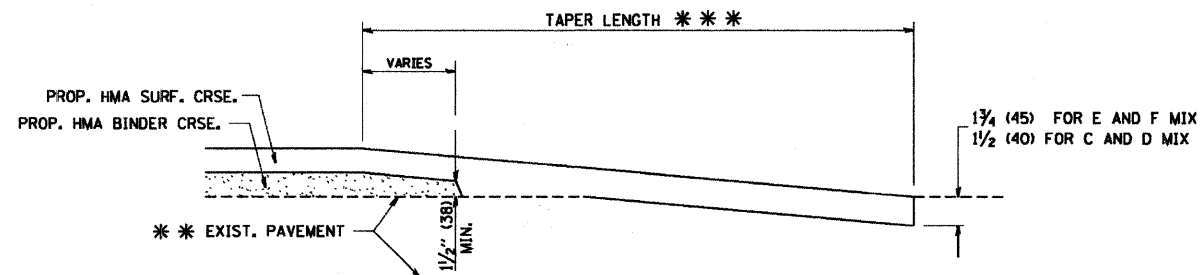
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- *** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dststd\22x34\bd32.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
	PLOT SCALE = 58.0000' / IN.	DRAWN - M. DE YONG	REVISED - A. ABBAS 03-21-97
	PLOT DATE = 1/4/2008	CHECKED - P.J.G.	REVISED - M. GOMEZ 04-06-01
		DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
BD400-05	BD32		6	6
CONTRACT NO.				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE NAME =
9434-008_Overlook.dwg

USER NAME =	DESIGNED - BVS	REVISED - 5/18/09 PER IDOT COMMENTS
PLOT SCALE = N.T.S.	DRAWN - BVS	REVISED - 5/28/09 PER IDOT COMMENTS
PLOT DATE = 4/7/09	CHECKED - P.J.G.	
	DATE - 4/7/09	

OVERLOOK DRIVE
RESURFACING PROGRAM

DETAILS

SCALE: NONE SHEET NO. 6 OF 6 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I311	09-00006-00-RS	COOK	6	6
CONTRACT NO. 63191				
ILLINOIS FED. AID PROJECT				