

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	09-00053-00-RS	COOK	13	1
FED. ROAD DIST. NO.		ILLINOIS	CONTRACT NO. 63213	

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**  
**PLANS FOR PROPOSED**  
**FEDERAL AID HIGHWAY**  
**FAU 1321 (IRVING PARK ROAD\IL ROUTE 19)**  
**MADISON DRIVE TO BARTLETT ROAD**  
**RESURFACING**  
**SECTION 09-00053-00-RS**  
**PROJECT ARA-9003(304)**  
**VILLAGE OF STREAMWOOD**  
**COOK COUNTY**  
**C-91-582-09**

**INDEX OF SHEETS**

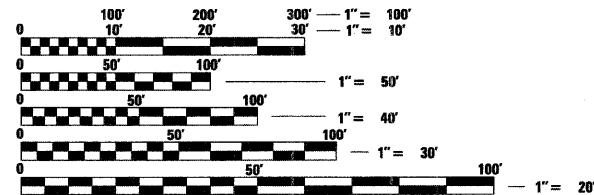
- 1 COVER SHEET
- 2 GENERAL NOTES AND HIGHWAY STANDARDS
- 3 SUMMARY OF QUANTITIES
- 4 TYPICAL SECTIONS
- 5 ROADWAY PLAN
- 6-13 DETAILS

**TRAFFIC DATA**

ADT:  
IL ROUTE 19      22,200 VPD (2009)  
MINOR ARTERIAL (URBAN)


**POSTED SPEED**

40 MPH (EXISTING)  
40 MPH (PROPOSED)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION CALL 811



Know what's below.  
Call before you dig.

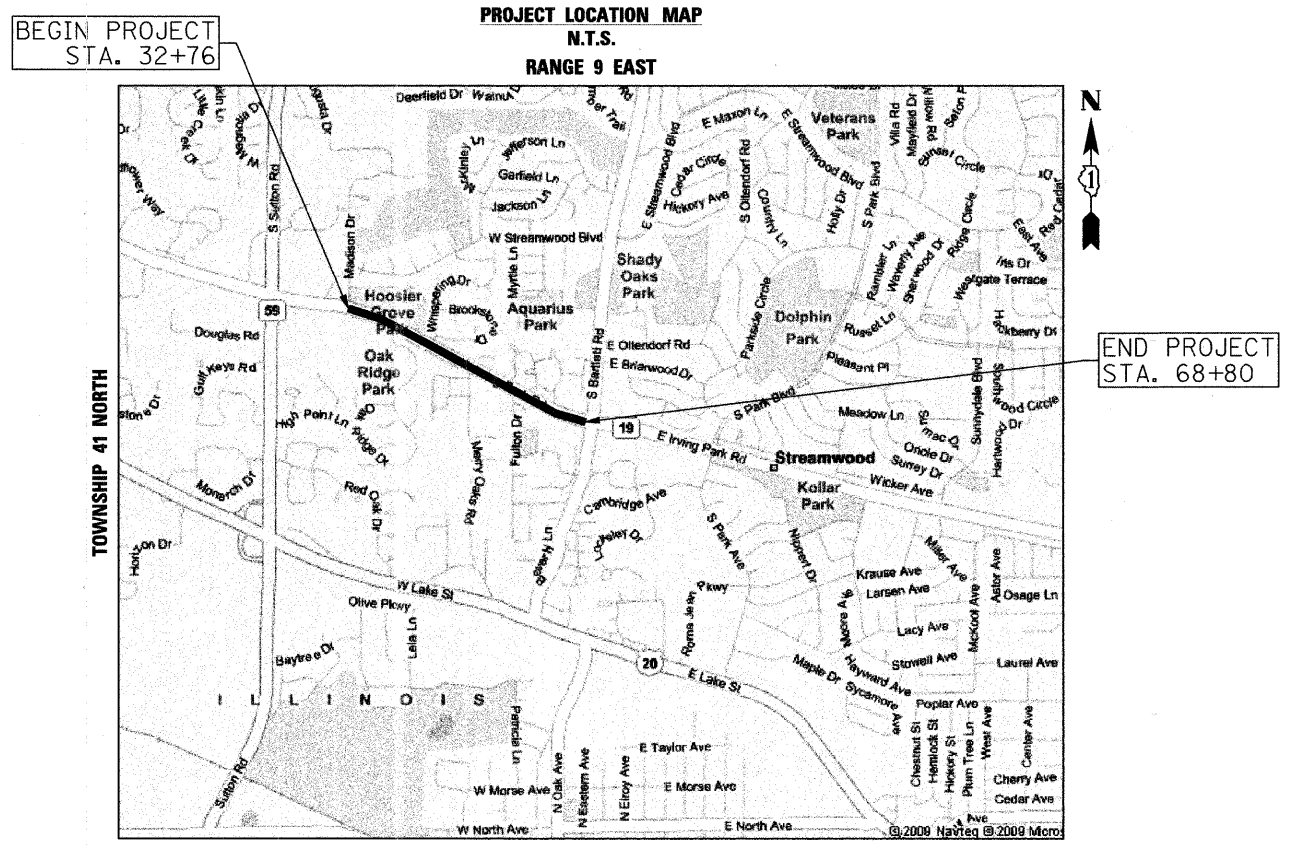
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**PROJECT MANAGER: A. CHAUDHRY**  
**PROJECT ENGINEER: C. HARTKE**

**CONTRACT NO. 63213**



**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**

APPROVED 5/22 2009  
*Mark M. ...*  
VILLAGE OF STREAMWOOD

PASSED JUNE 1 2009  
*Christopher M. Hartke*  
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW JUNE 1 2009  
*Diana M. O'Keefe*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

**PROFESSIONAL ENGINEER'S SIGN & SEAL**

*Christopher M. Hartke* 5/22/2009  
CHRISTOPHER M. HARTKE, P.E.  
EXPIRES: 11-30-09

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**OF THE STATE OF ILLINOIS**

COMPANY NAME: Smith  
 PROJECT CONTACT: A. CHAUDHRY  
 CLIENT: VILLAGE OF STREAMWOOD  
 DATE PLOTTED: 5/18/2009 4:25:01 PM  
 FILE NAME: 090118-cvr.dgn  
 PLOT DRIVER: McHenry-1050-plot11r-AS.JS.plt  
 PLOT TABLE: Standard.ctb

ASSOCIATE FIELD ENGINEER: KEVIN STALLWORTH (847)-705-4169

**GENERAL NOTES**

1. ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2007.
2. ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
4. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 1-800-892-0123 AND THE VILLAGE OF STREAMWOOD AT 1-630-736-3851 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
5. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR VILLAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT OR VILLAGE.
6. SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE ITEM REMOVED.
7. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
8. OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
9. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
10. THE BITUMINOUS MATERIAL PRIME COAT QUANTITIES HAVE BEEN DETERMINED USING A RATE OF 0.10 GAL/SQ-YD.
11. THE AGGREGATE PRIME COAT QUANTITIES HAVE BEEN DETERMINED USING A RATE OF 4 LBS/SQ-YD.
12. QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL IDENTIFY FINAL PATCH LOCATIONS IN THE FIELD.
13. THE CONTRACTOR SHALL NOTIFY IDOT BUREAU OF MATERIALS AT (847) 705-4337 AT LEAST 24 HOURS PRIOR TO THE PLACEMENT OF HOT-MIX ASPHALT OR CONCRETE.
14. FOR STEEL BARS CERTIFICATION, CONTACT IDOT BUREAU OF MATERIALS AT (847) 705-4337.

**STORM SEWERS, WATER MAINS, AND UTILITIES**

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
2. THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.
4. ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.

**GENERAL NOTES (CONTINUED)**

5. THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
6. THE CONTRACTOR SHALL ENSURE THAT ALL WATER SYSTEM VALVES, VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE TO THE VILLAGE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.
7. ANY LOOSE MATERIAL DEPOSITED IN THE FLOWLINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.

**SIGNING AND STRIPING**

1. SEE IDOT STANDARD DETAIL 780001. DISTRICT ONE DETAIL AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.
2. SIGNS SHALL NOT BE MOVED OR COVERED UNTIL PROGRESS OF WORK NECESSITATES IT.
3. THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SUCH SIGNS THAT INTERFERE WITH CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.

**TRAFFIC CONTROL**

1. SEE SPECIFICATIONS FOR TRAFFIC CONTROL AND PROTECTION.
2. THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMNT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

**HIGHWAY STANDARDS**

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
701101-02	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701501-05	URBAN LANE CLOSURE, MULTILANE, 2L, 2W, UNDIVIDED
701606-06	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

Smith  
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PLOT DATE = 5/28/2009	CHECKED - T. HAMILTON	REVISED -
	DATE - 5/22/09	REVISED -

**VILLAGE OF STREAMWOOD**

**GENERAL NOTES AND HIGHWAY STANDARDS**

SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	09-00053-00-RS	COOK	13	2
CONTRACT NO. 63213				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT ARA-9003(304)				

**SUMMARY OF QUANTITIES**

SPECIALTY ITEM	CODE NUMBER	PAY ITEM DESCRIPTION	UNITS	ROADWAY 100% FEDERAL 1000
	40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	2,055
	40600300	AGGREGATE (PRIME COAT)	TON	45
	40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	1,235
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	920
	40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2,430
	42400800	DETECTABLE WARNINGS	SQ FT	120
	44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	21,540
	44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	300
	44004610	SIDEWALK REMOVAL AND REPLACEMENT (SPECIAL)	SQ FT	625
	44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	60
	44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	220
	44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	220
	44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	60
	48101200	AGGREGATE SHOULDERS, TYPE B	TON	200
	60266600	VALVE BOXES TO BE ADJUSTED	EACH	3
	60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	8
	67100100	MOBILIZATION	L SUM	1
	70101800	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1
	70106800	CHANGEABLE MESSAGE SIGN	CAL MO	4
	70300100	SHORT-TERM PAVEMENT MARKING	FOOT	2,860
	70300200	TEMPORARY PAVEMENT MARKING	FOOT	720
*	78000100	THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS	SQ FT	410
*	78000200	THERMOPLASTIC PAVEMENT MARKING, LINE 4 INCH	FOOT	14,050
*	78000400	THERMOPLASTIC PAVEMENT MARKING, LINE 6 INCH	FOOT	1,280
*	78000600	THERMOPLASTIC PAVEMENT MARKING, LINE 12 INCH	FOOT	320
*	78000650	THERMOPLASTIC PAVEMENT MARKING, LINE 24 INCH	FOOT	210
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	200
	78300100	PAVEMENT MARKING REMOVAL	SQ FT	280
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	200
*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	100
	X0322256	TEMPORARY INFORMATION SIGN	SQ FT	170
	X0656100	DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT	SQ YD	50

COMPANY NAME: Smith  
 PROJECT CONTACT: CHAUDHRY  
 CLIENT: VILLAGE OF STREAMWOOD  
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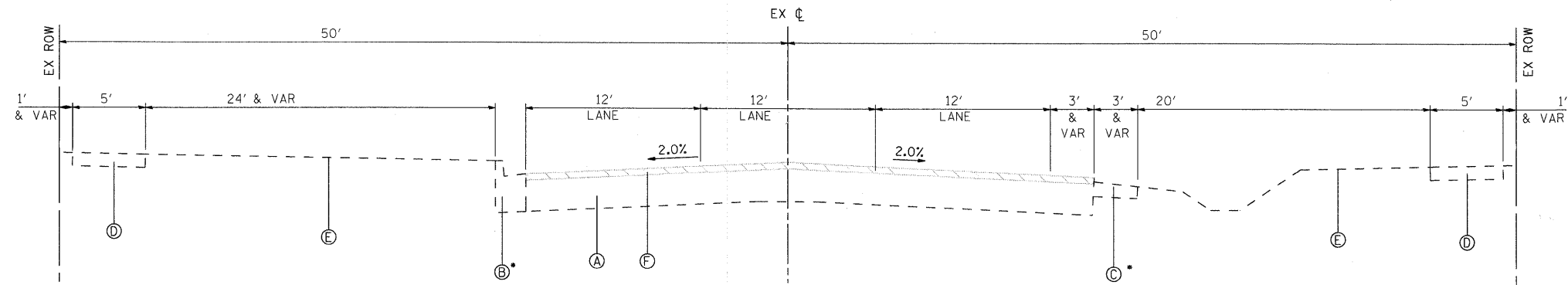


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	DRAWN - J. ATTANASEO	REVISED -
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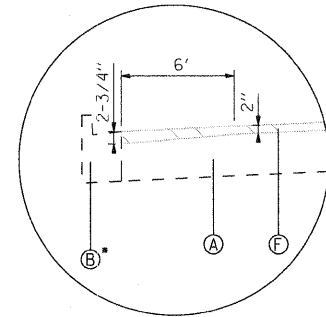
**VILLAGE OF STREAMWOOD**

**SUMMARY OF QUANTITIES**

SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	F.A.U. RTE. 1321	SECTION 09-00053-00-RS	COUNTY COOK	TOTAL SHEETS 13	SHEET NO. 3
			CONTRACT NO. 63213				
			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT ARA-9003(304)				

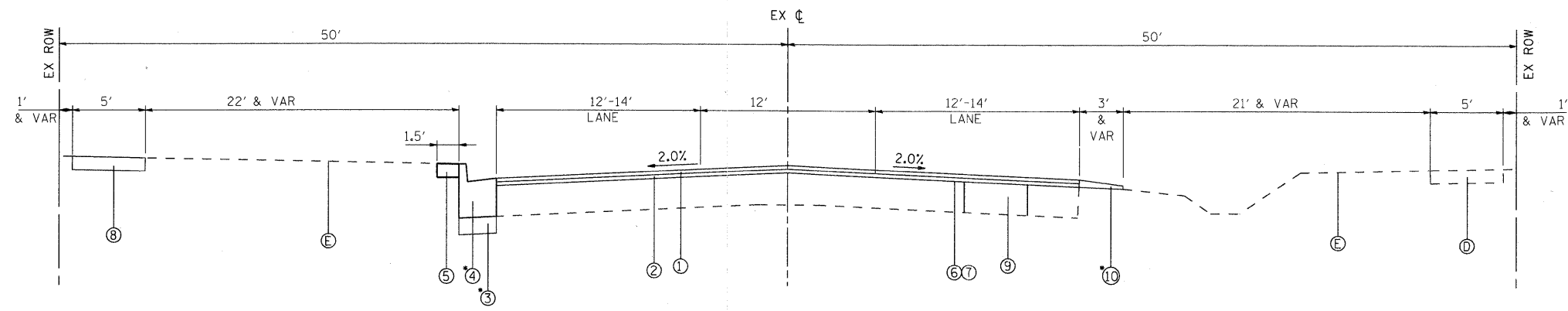


**EXISTING TYPICAL SECTION  
IRVING PARK ROAD (IL ROUTE 19)**  
STA. 32+76 TO STA. 68+80



**DETAIL "A"**

\* COMBINATION CONCRETE CURB & GUTTER LOCATIONS VARY. SEE ROADWAY PLANS FOR ADDITIONAL INFORMATION. FINISHED ROADWAY SURFACE SHALL BE 1/4" ABOVE GUTTER FLAG AT THE EDGE OF PAVEMENT.



**PROPOSED TYPICAL SECTION  
IRVING PARK ROAD (IL ROUTE 19)**  
STA. 32+76 TO STA. 68+80

**EXISTING LEGEND**

- (A) HOT-MIX ASPHALT PAVEMENT, DEPTH VARIES
- (B) COMBINATION CONCRETE CURB & GUTTER, TYPE VARIES
- (C) AGGREGATE SHOULDER, 6"
- (D) PORTLAND CEMENT CONCRETE SIDEWALK, 5"
- (E) EXISTING GROUND
- (F) HOT-MIX ASPHALT SURFACE REMOVAL, 2" REMOVAL SHALL BE INCREASED TO 2-3/4" AT LOCATIONS OF COMBINATION CONCRETE CURB & GUTTER. ADDITIONAL THICKNESS OF SURFACE REMOVAL IS INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL, 2". SEE DETAIL "A".

**PROPOSED LEGEND**

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (2) LEVELING BINDER (MACHINE METHOD), N70, 1" (MIN.)
- (3) AGGREGATE BASE COURSE, TYPE B CA-7, 6" (INCLUDED IN THE COST OF CURB & GUTTER)
- (4) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE RESIDENT ENGINEER)
- (5) SODDING, SALT TOLERANT & TOPSOIL 4" (TO BE INCLUDED IN THE COST OF CURB & GUTTER REMOVAL & REPLACEMENT)
- (6) BITUMINOUS MATERIALS (PRIME COAT)
- (7) AGGREGATE (PRIME COAT)
- (8) SIDEWALK REMOVAL AND REPLACEMENT, SPECIAL (AS DIRECTED BY THE RESIDENT ENGINEER)
- (9) CLASS D PATCHES, 8-INCH (AS DIRECTED BY THE RESIDENT ENGINEER)
- (10) AGGREGATE SHOULDERS, TYPE B, TAPER 3" TO 1" THICKNESS

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING.

MIXTURE TYPE	AC TYPE	VOIDS
<b>RESURFACING</b>		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm)	PG 64-22	4% @ 70 GYR
LEVELING BINDER (MACHINE METHOD), N70 (IL 9.5mm)	PG 64-22*	4% @ 70 GYR
<b>PATCHING</b>		
CLASS D PATCHES (HMA BINDER IL-19mm), 8-INCH	PG 64-22*	4% @ 70 GYR
<b>DRIVEWAYS</b>		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm)	PG 64-22	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN  
\*WHEN RAP EXCEEDS 20% THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

COMPANY NAME: SEC Group, Inc.  
 CONTACT: J. ATTANASEO  
 CLIENT: VILLAGE OF STREAMWOOD  
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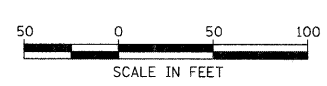
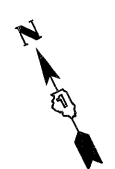
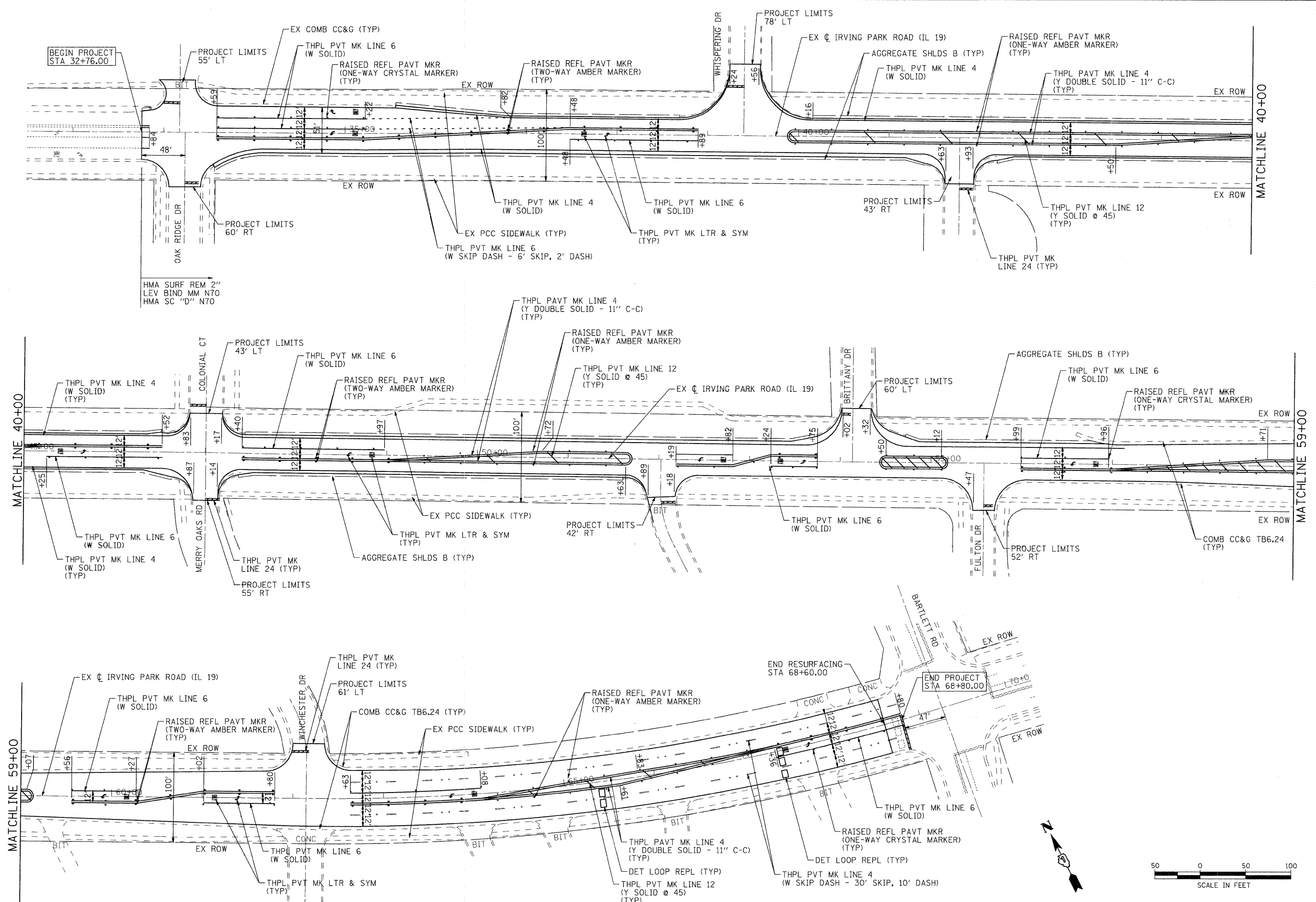
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PLOT DATE = 5/26/2009	DATE - 5/22/09	REVISED -

**VILLAGE OF STREAMWOOD**

**EXISTING AND PROPOSED TYPICAL SECTIONS**

F.A.U. RTE. 1321	SECTION 09-0053-00-RS	COUNTY COOK	TOTAL SHEETS 13	SHEET NO. 4
CONTRACT NO. 63213				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT ARA-9003(304)				

SCALE: ILLINOIS ROUTE 19 OF SHEETS STA. TO STA.



COMPANY NAME: Smith, Galloway  
 PROJECT CONTACT: VILLAGE OF STREAMWOOD  
 CLIENT: VILLAGE OF STREAMWOOD  
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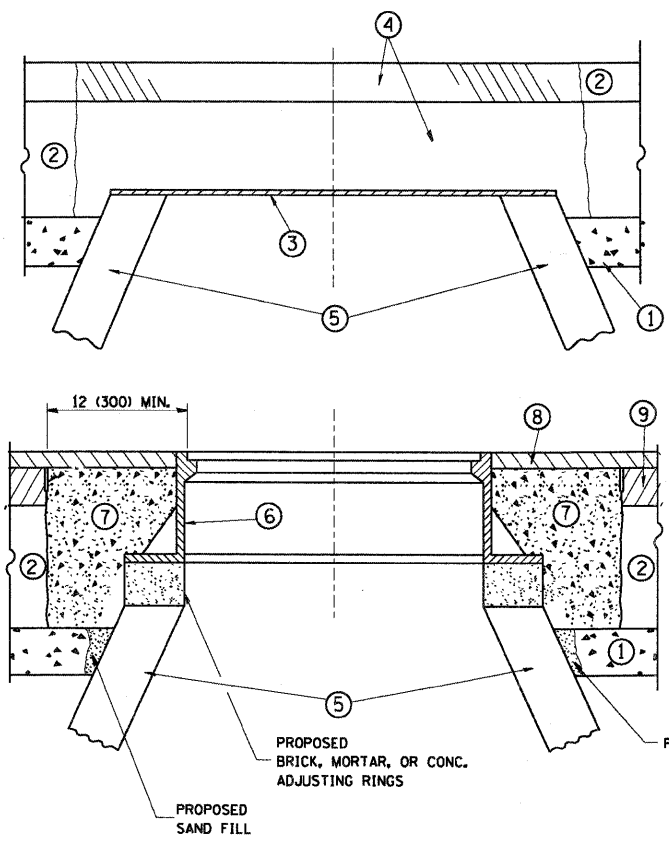
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PLOT DATE = 5/28/2009	CHECKED - T. HAMILTON	REVISED -
	DATE - 5/22/09	REVISED -

**VILLAGE OF STREAMWOOD**

**ROADWAY PLAN - IRVING PARK ROAD (IL ROUTE 19)**

SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. 32+76.00 TO STA. 68+80.00

F.A.J. RTE. 1321	SECTION 09-0053-00-RS	COUNTY COOK	TOTAL SHEETS 13	SHEET NO. 5
CONTRACT NO. 63213				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT ARA-9003(304)				



**CONSTRUCTION PROCEDURES**

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
  - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
  - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
  - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

COMPANY NAME: Smith  
 PROJECT CONTACT: A. GHADIRY  
 CLIENT: VILLAGE OF STREAMWOOD  
 DATE PLOTTED: 5/26/2009 10:52:29 AM  
 PLOT NUMBER: 10-22-039  
 PEN TABLE: standard-trans.tbl

**SEC Group, Inc.**  
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 Landscape Architecture  
 McHenry • Yorkville • New Lenox • Chicago  
 www.secgroupinc.com

USER NAME = charlke	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95
	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE =	CHECKED -	REVISED - R. WIEDEMAN 05-14-04
PLOT DATE = 5/26/2009	DATE - 10-22-039	REVISED - R. BORO 01-01-07

**VILLAGE OF STREAMWOOD**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	09-00053-00-RS	COOK	13	6
BD600-03 (BD-8)			CONTRACT NO. 63213	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT ARA-9003(204)		

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT TOLERANT SOD AND TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

T/2 \*

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

3" (75) MIN.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

COMPANY NAME: Smith  
PROJECT CONTACT: A. CHAUDHRY  
PROJECT: VILLAGE OF STREAMWOOD  
CLIENT: VILLAGE OF STREAMWOOD  
DATE PLOTTED: 5/26/2009 11:58:33 AM  
FILE NAME: 090118-dc1-03.dgn  
PLOT DRIVER: pdf.plt  
PEN TABLE: standard-trans.tbl



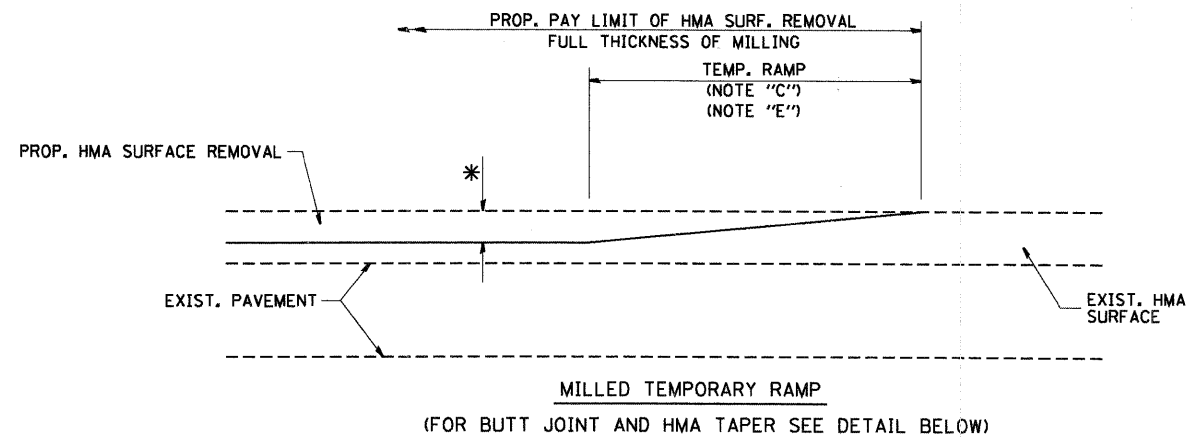
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	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE =	CHECKED -	REVISED - M. GOMEZ 01-22-01
PLOT DATE = 5/26/2009	DATE - 03-22-09	REVISED - R. BORO 01-01-07

VILLAGE OF STREAMWOOD

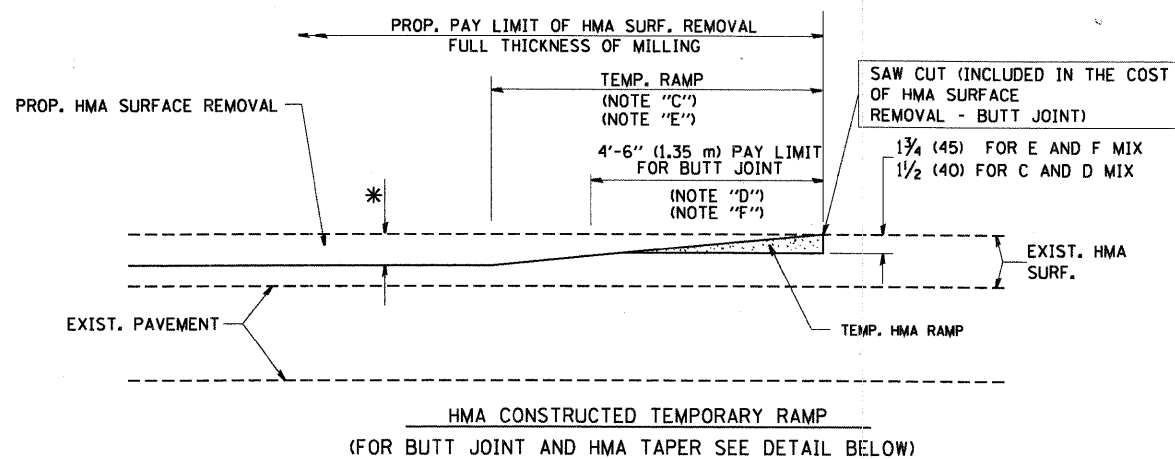
CURB OR CURB AND GUTTER  
REMOVAL AND REPLACEMENT

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	09-00053-00-RS	COOK	13	7
BD600-06 (BD-24)			CONTRACT NO. 63213	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	ARA-9003(304)	

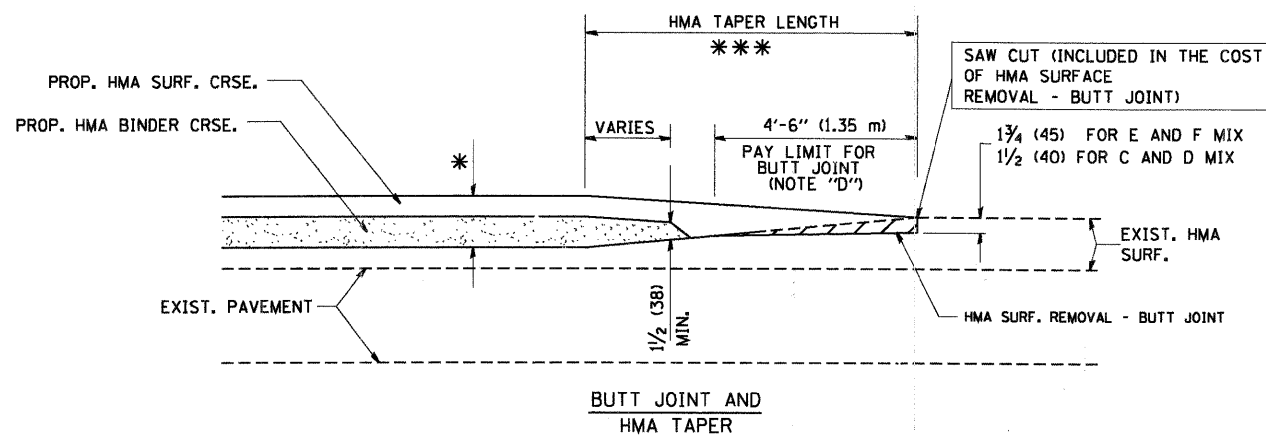


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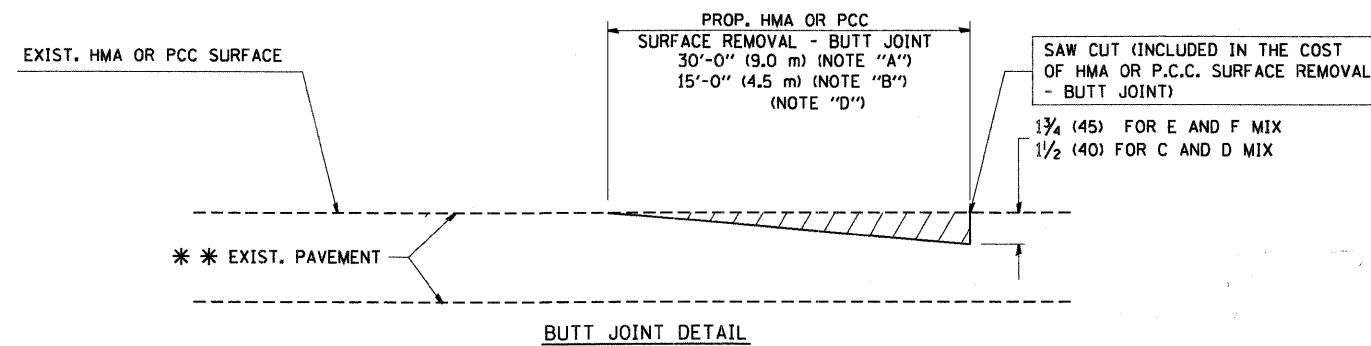


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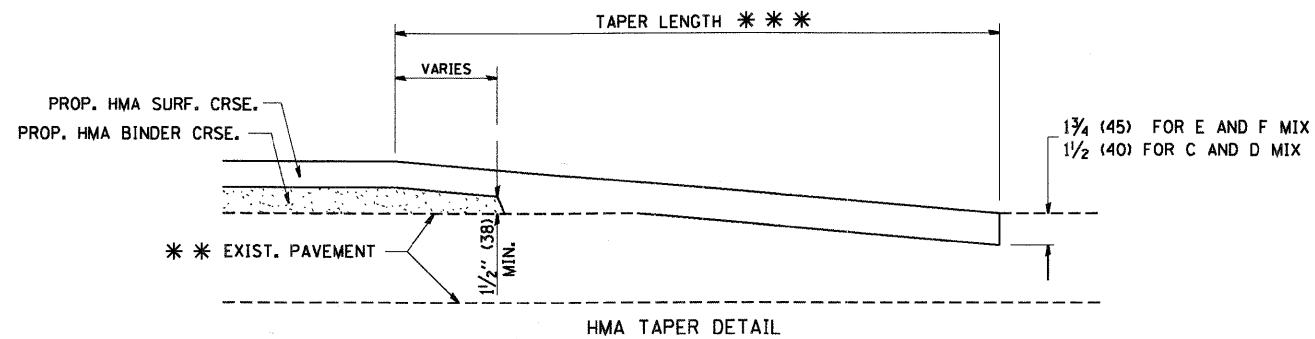
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

COMPANY NAME: Smith  
 PROJECT CONTACT: A. CHALDIRY  
 PROJECT: VILLAGE OF STREAMWOOD  
 CLIENT: 5/26/2009 11:56:38 AM  
 DATE PLOTTED: 09/01/09-08:04:00  
 FILE NAME: 090118-d01-04.dgn  
 PLOT DRIVER: pdfplot  
 PEN TABLE: standard-trans.tbl



USER NAME = charlke	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE =	CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 5/26/2009	DATE - 06-23-09	REVISED - R. BORO 01-01-07

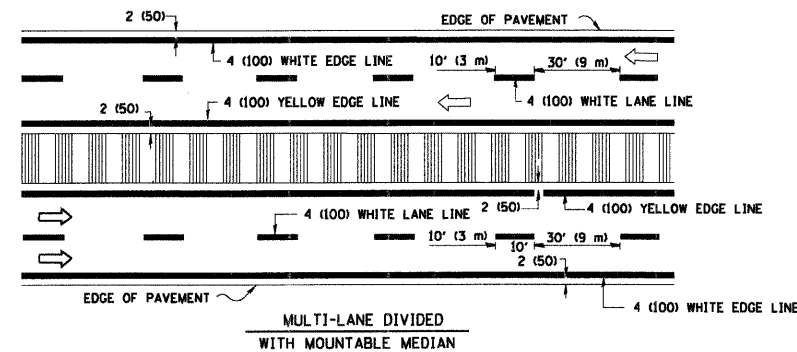
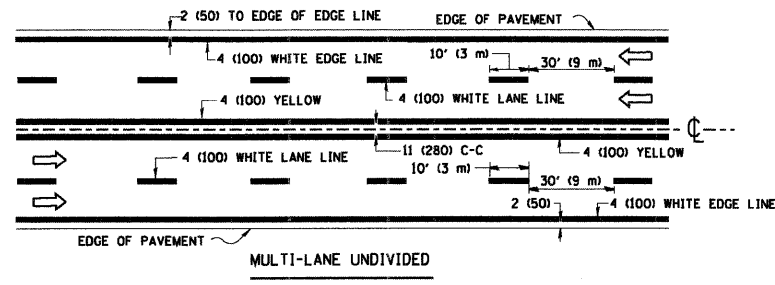
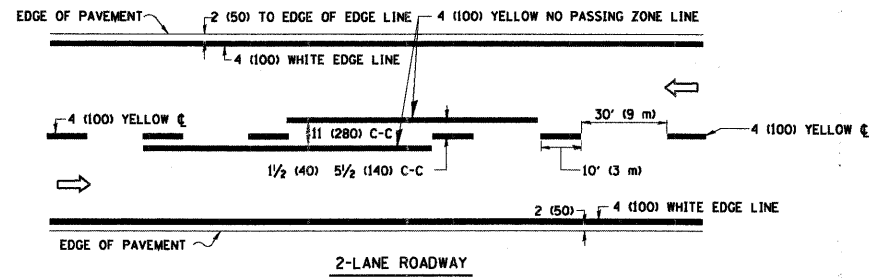
**VILLAGE OF STREAMWOOD**

**BUTT JOINT AND HMA TAPER DETAILS**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

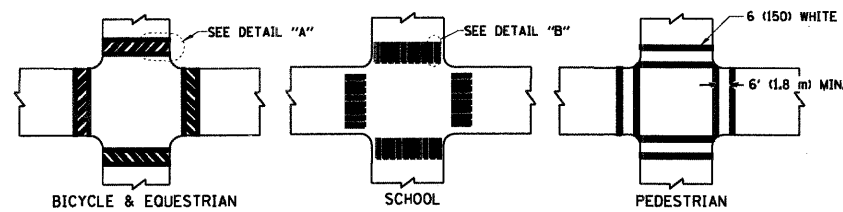
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD400-05 BD32		CONTRACT NO. 63213		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT ARA-9003(304)		



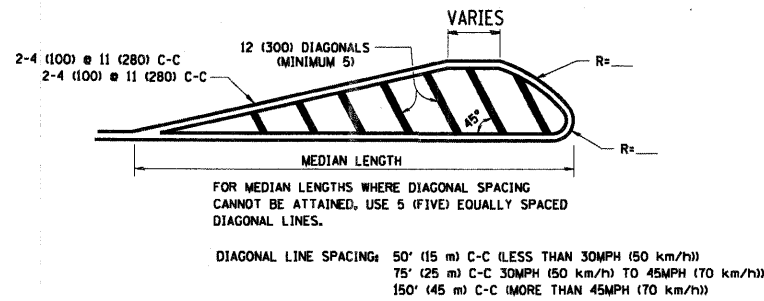
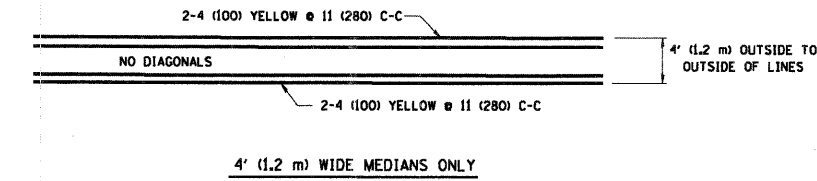


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

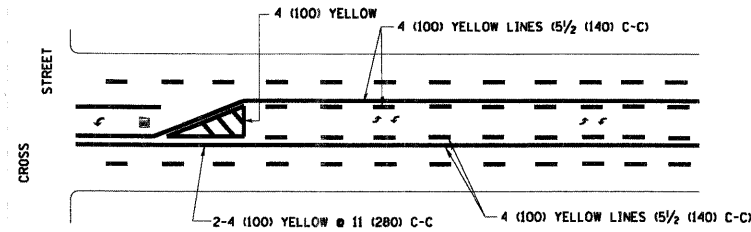
**TYPICAL LANE AND EDGE LINE MARKING**



**TYPICAL CROSSWALK MARKING**



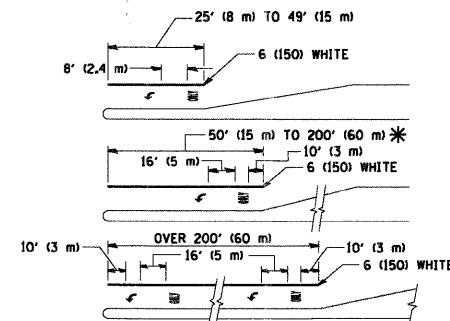
**MEDIANS OVER 4' (1.2 m) WIDE**



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



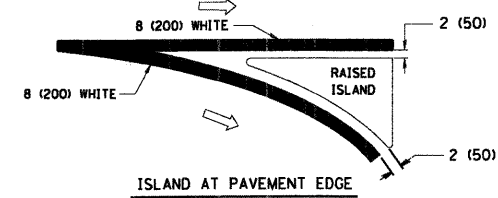
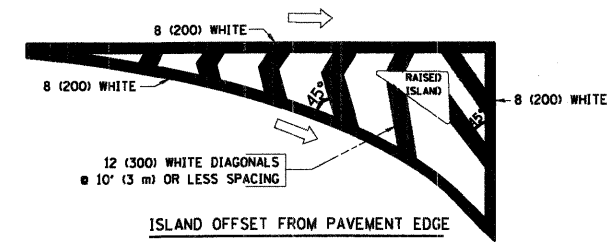
**TYPICAL PAINTED MEDIAN MARKING**



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL TURN LANE MARKING**



**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

COMPANY NAME: SMITH  
PROJECT CONTACT: A. CHAUDHRY  
CLIENT: VILLAGE OF STREAMWOOD  
DATE PLOTTED: 5/26/2009 11:58:41 AM  
FILE NAME: 090118-061-05.dgn  
PLOT DRIVER: pdf.plt  
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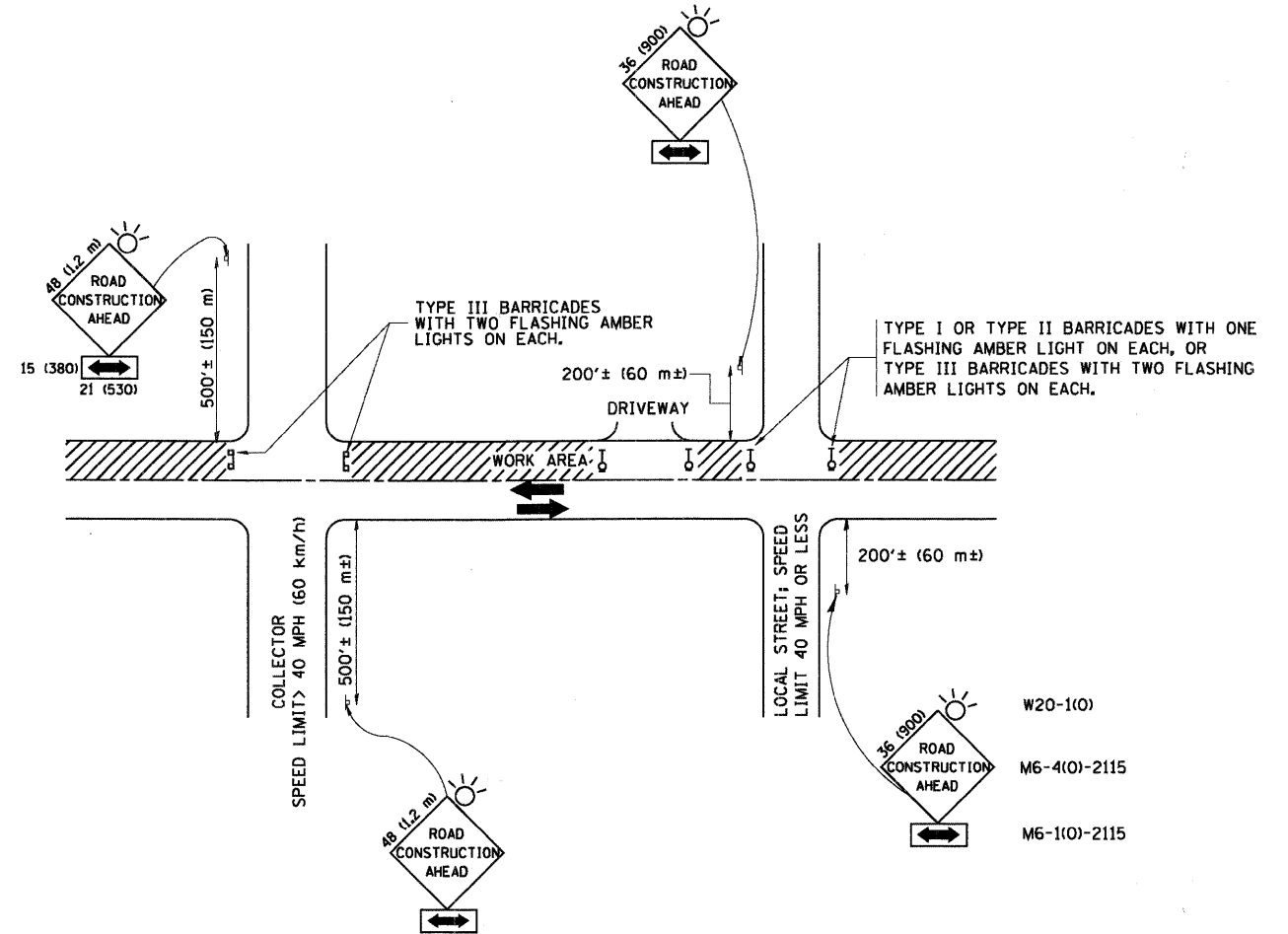
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USER NAME = char-uke	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
PLOT SCALE =	DRAWN -	REVISED - A. HOUSEH 10-09-96
PLOT DATE = 5/26/2009	CHECKED -	REVISED - A. HOUSEH 10-17-96
	DATE = 03-22-99	REVISED - T. RAMMACHER 01-06-00

**VILLAGE OF STREAMWOOD**

**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

SCALE:	SHEET NO. OF SHEETS STA. TO STA.	F.A.U. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
		1321 09-00053-00-RS COOK 13 9
		TC-13 CONTRACT NO. 63213
		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT ARA-9003(304)



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

COMPANY NAME: Smith  
 PROJECT CONTACT: A. CHAUDHRY  
 CLIENT: VILLAGE OF STREAMWOOD  
 DATE PLOTTED: 5/26/2009 11:58:46 AM  
 FILE NAME: 090118-061-06.dwg  
 PLOT DRIVER: pdcfpl  
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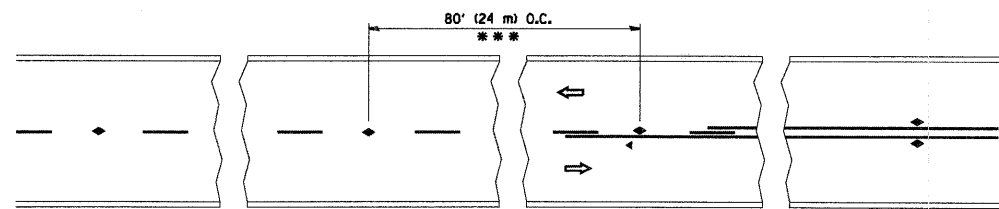
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PLOT SCALE =	DRAWN -	REVISED - A. HOUSEH 03-06-96
PLOT DATE = 5/26/2009	CHECKED -	REVISED - A. HOUSEH 10-15-96
	DATE - 06/23/09	REVISED - T. RAMMACHER 01-06-00

VILLAGE OF STREAMWOOD

TRAFFIC CONTROL AND PROTECTION FOR  
 SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

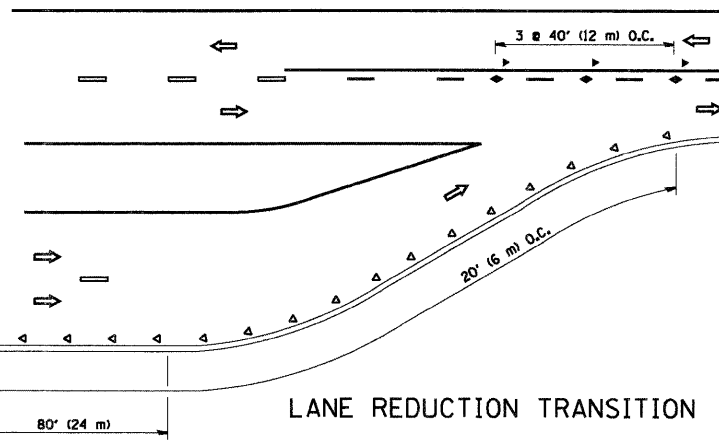
SCALE:	SHEET NO. OF SHEETS	STA. TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	09-00053-00-R5	COOK	13	10
TC-10			CONTRACT NO. 63213	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	ARA-9003(304)	

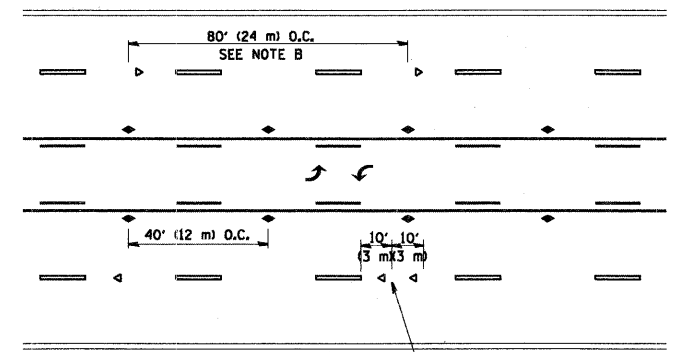


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

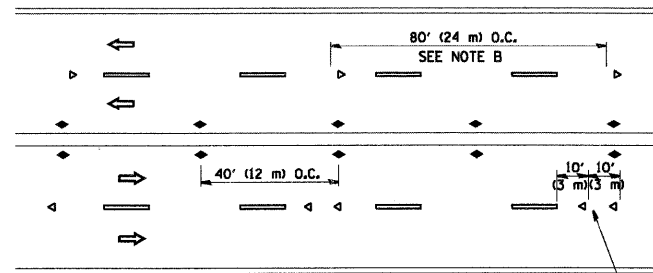
TWO-LANE/TWO-WAY



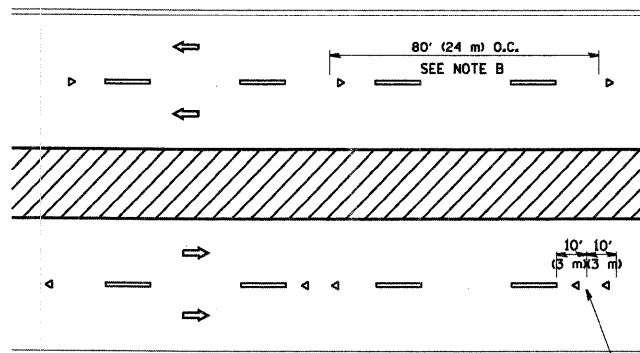
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

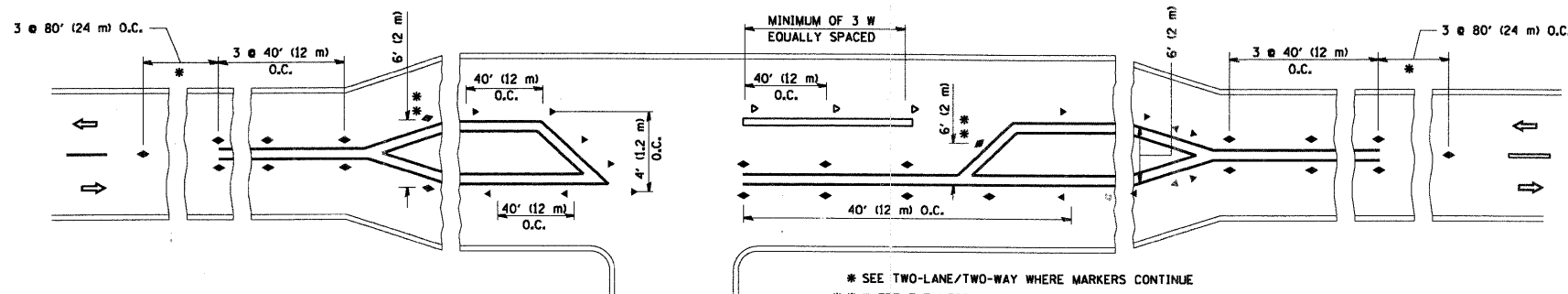
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◄ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

COMPANY NAME: CHAUNDRY  
 PROJECT CONTACT: VILLAGE OF STREAMWOOD  
 CLIENT: 5/26/2009 11:56:51 AM  
 DATE PLOTTED: 09/01/09  
 FILE NAME: 090118-det-07.dgn  
 PLOT DRIVER: pdf.plt  
 PEN TABLE: standard-trans.tbl



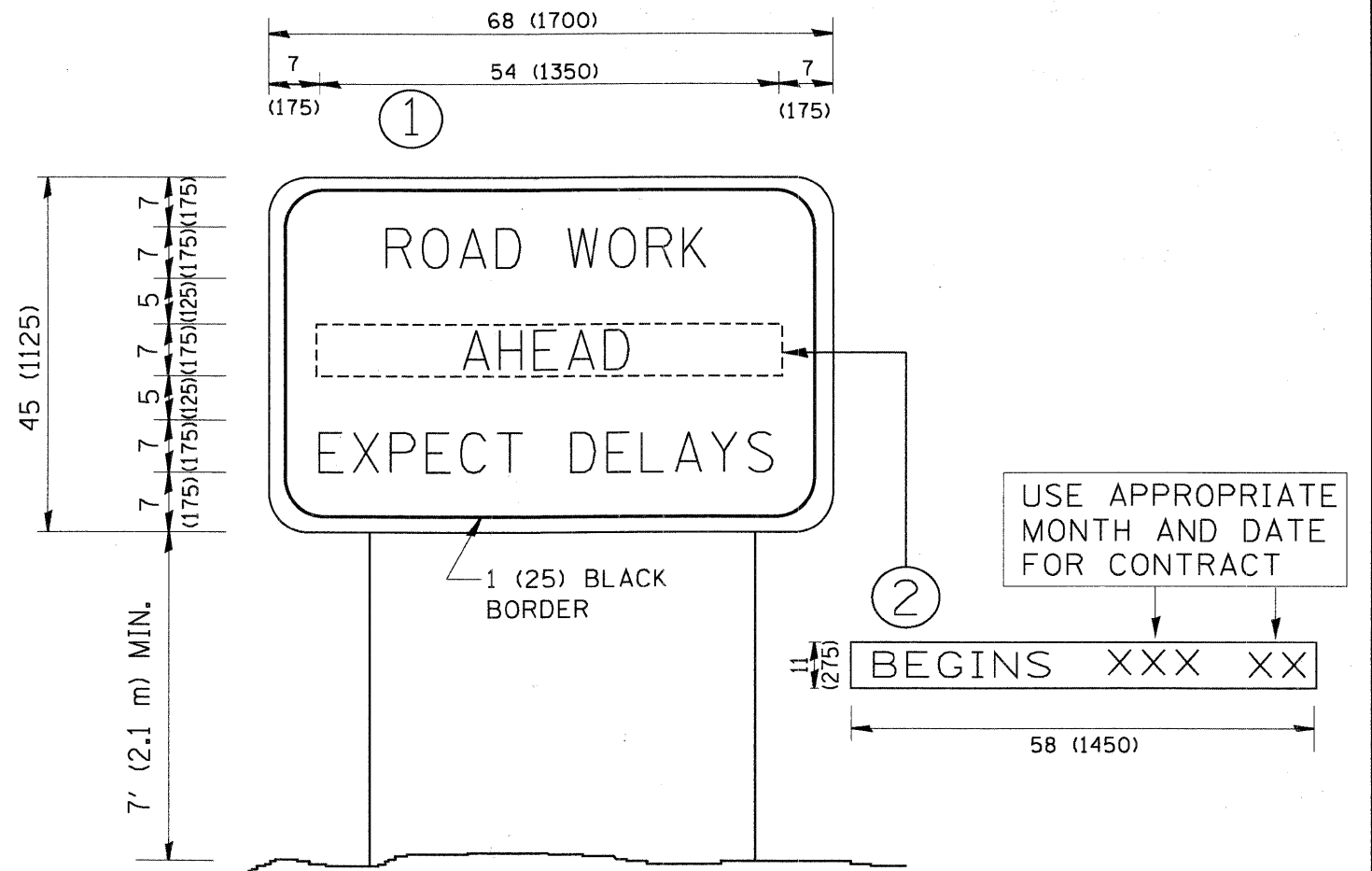
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	DRAWN -	REVISED - T. RAMMACHER 03-12-99
PLOT SCALE =	CHECKED -	REVISED - T. RAMMACHER 01-06-00
PLOT DATE = 5/26/2009	DATE - 5/22/09	REVISED -

VILLAGE OF STREAMWOOD

TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	09-00053-00-RS	COOK	13	11
TC-11		CONTRACT NO. 63213		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT ARA-9003(304)		



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

COMPANY NAME: SMITH CONSULTING  
 PROJECT CONTACT: VILLAGES OF STREAMWOOD  
 DATE PLOTTED: 5/26/2009 11:58:54 AM  
 FILE NAME: 090118-ar-08.dgn  
 PLOT DRIVER: pdf.plt  
 PEN TABLE: standard-trans.tbl

**SEC Group, Inc.**  
 Illinois Professional Geograph Firm # 104-000108  
 Engineering  
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 Planning  
 Landscape Architecture  
 McHenry • Yorkville • New Lenox • Chicago  
 www.secgroupinc.com

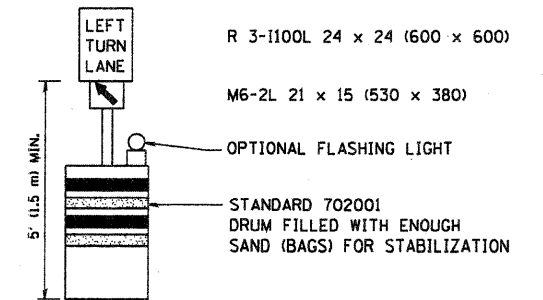
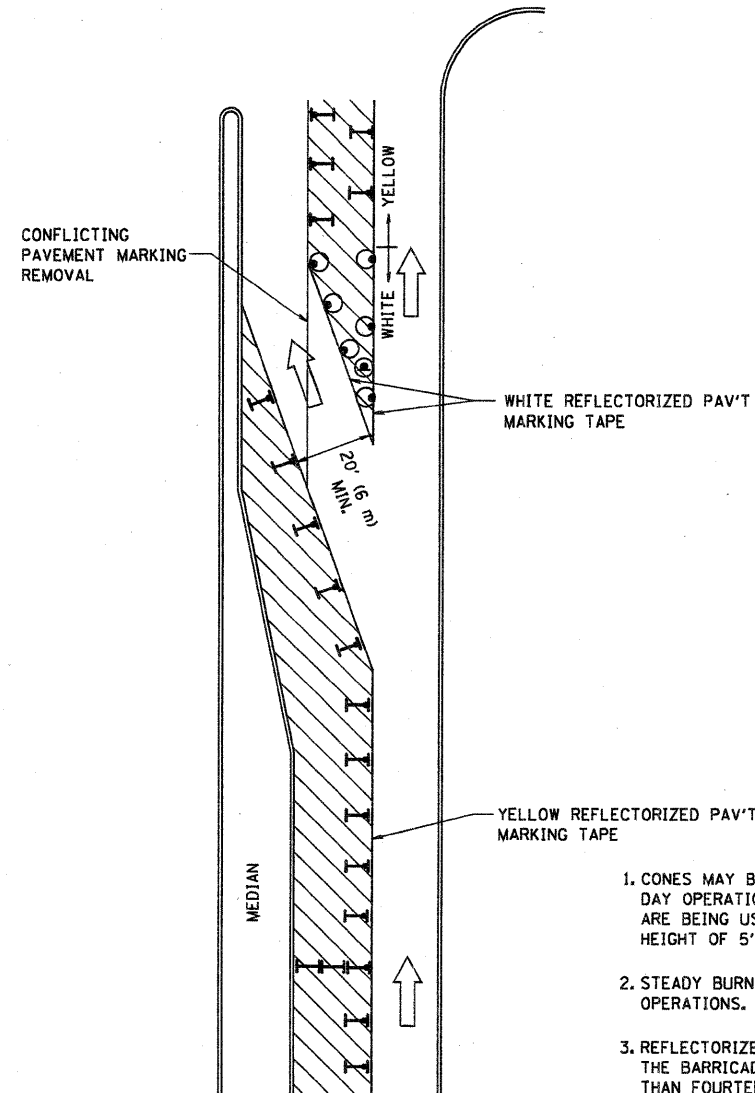
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	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE =	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 5/26/2009	DATE - 5/22/09	REVISED - C. JUCIUS 01-31-07

**VILLAGES OF STREAMWOOD**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	09-00053-00-RS	COOK	13	12
<b>TC-22</b>			CONTRACT NO. 63213	
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	ARA-9003(304)		

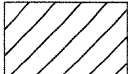







**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = geglentobt	DESIGNED -	REVISED -T. RAMMACHER 09-08-94
W:\diststd\22x34\ta14.dgn		DRAWN -	REVISED - A. HOUSEH 11-07-95
	PLOT SCALE = 50.0000 "/ IN.	CHECKED -	REVISED - A. HOUSEH 10-12-96
	PLOT DATE = 1/4/2008	DATE -	REVISED -T. RAMMACHER 01-06-00

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-14		13	13
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO.	