

GENERAL NOTES

THE THICKNESS OF HMA SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA IS PLACED.

THE HMA SURFACE OF ALL PRIVATE ENTRANCES, COMMERCIAL ENTRANCES, AND SIDE ROADS SHALL BE MADE NEATLY, IN A WORKMANLIKE MANNER, AND SHALL ACCURATELY CONFORM TO THE SHAPES AND DIMENSIONS SHOWN ON THE PLAN DETAILS. IF REQUIRED BY THE ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SAW CUT THE HMA SURFACE TO CONFORM TO THE SHAPES AND DIMENSIONS SHOWN ON THE PLAN DETAILS. THIS WORK SHALL BE INCLUDED IN THE COST OF THE HMA SURFACE

THE ENGINEER WILL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS HMA LIFTS.

FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER LEG.

THE CONTRACTOR WILL BE RESPONSIBLE FOR STAKING EXISTING NO PASSING ZONES SO THEY MAY BE RE-ESTABLISHED AFTER RESURFACING IS COMPLETED.

ON EXISTING PAVEMENT WHICH MAY BE SUPERELEVATED, THE NEW HMA PAVEMENT SHALL BE BUILT WITH THE SAME SUPERELEVATION UNLESS NEW SUPERELEVATION RATES ARE GIVEN ON THE PLANS.

ADDITIONAL LEVELING BINDER HAS BEEN ADDED TO THE QUANTITIES FOR CROWN CORRECTION WHERE REQUIRED.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER SHOWN IN THE LIST OF STANDARDS INCLUDED IN THESE PLANS.

COMMITMENTS
NONE

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

GRANULAR MATERIALS	2.05 TONS / CU YD
BITUMINOUS MAT PRIME COAT	0.10 GAL / SQ YD OR 0.375 GAL / SQ YD
AGGREGATE PRIME COAT	0.002 TONS / SQ YD
HMA RESURFACING	112 LBS / SQ YD / IN
SHORT TERM PAVEMENT MARKING	10 FT / 100 FT OF APPLICATION

MEMBERS OF JULIE KNOWN TO BE WITHIN THE LIMITS OF THE IMPROVEMENT ARE:

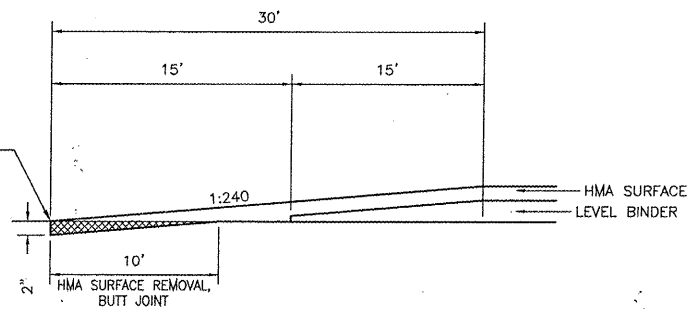
- A. T. & T. (SBC)
- COM ED
- NICOR GAS
- COMCAST
- MORRIS WATER & SEWER DEPARTMENT
- MORRIS FIRE PROTECTION DISTRICT

PG GRADE	HMA LEVEL BINDER	HMA SURFACE
PG 64-22	PG 64-22	PG 64-22
MAX % RAP ALLOWABLE **	25%	15%
DESIGN AIR VOIDS	4.0% @ N50	4.0% @ N50
MIXTURE COMPOSITION	IL 9.5	IL 9.5
FRICITION AGGREGATE		MIXTURE D
DENSITY TEST METHOD	CORRELATION	CORES

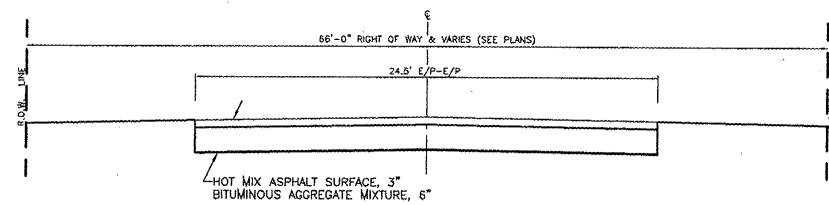
* MATERIAL SHALL BE COMPACTED TO 93.0-97.4 PERCENT OF THE MAXIMUM THEORETICAL DENSITY, EXCEPT THAT WHEN PLACED AS FIRST LIFT ON AN UNIMPROVED SUBGRADE THE MINIMUM PERCENT COMPACTION SHALL BE 92.0 PERCENT. THE MAXIMUM THEORETICAL DENSITY SHALL BE DETERMINED FROM THE MOVING AVERAGE AS SPECIFIED IN THE QC/QA SPECIFICATION.

** IF RAP OPTION IS SELECTED, THE ASPHALT CEMENT GRADE MAY NEED TO BE ADJUSTED. THIS WILL BE DETERMINED BY THE ENGINEER.

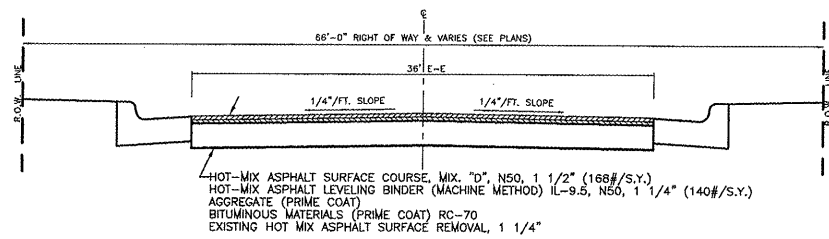
CONTRACTOR SHALL SAWCUT ALONG THE EDGE OF THE BUTT JOINT TO PROVIDE A VERTICAL EDGE AGAINST THE EXISTING PAVEMENT



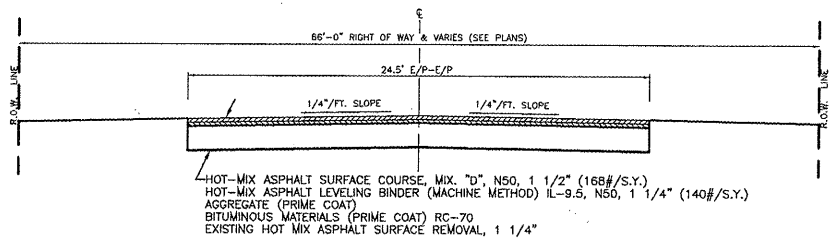
BUTT JOINT DETAIL
TYPICAL AT ALL ENDS OF IMPROVEMENTS AND AT INTERSECTIONS AS SHOWN ON THE PLANS



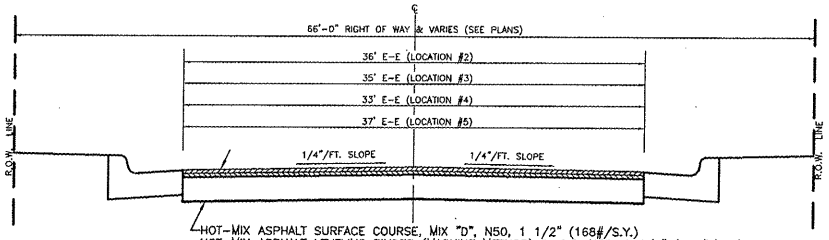
EXISTING
LOCATION #1 - LISBON ROAD (FAU 5966)
STA. 78+90 TO STA. 102+40
(BRIDGE OMISSION STA. 89+51 TO STA. 91+54)



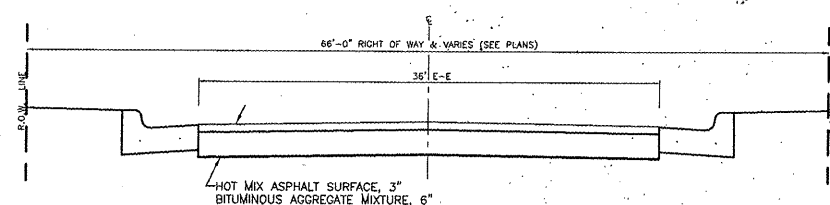
PROPOSED
LOCATION #1 - LISBON ROAD (FAU 5966)
STA. 49+00 TO STA. 78+90
& STA. 102+40 TO STA. 104+55



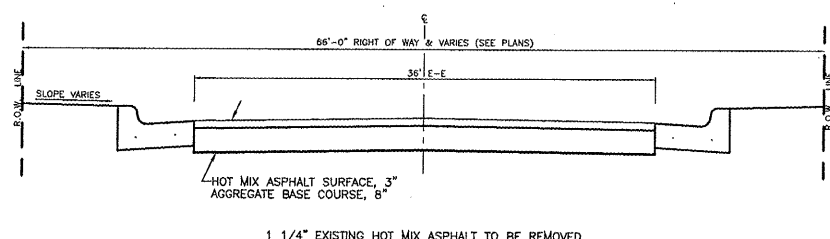
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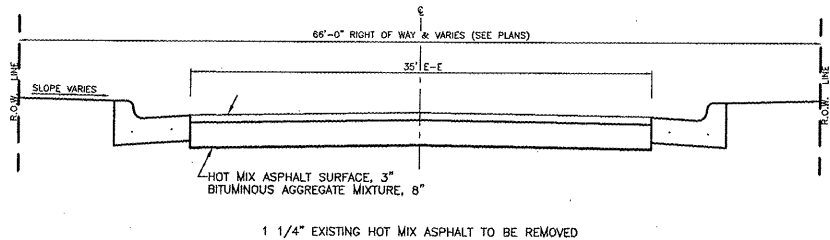
PROPOSED
LOCATION #2 - LISBON STREET (FAU 5967)
LOCATION #3 - HIGH STREET (FAU 5953)
LOCATION #4 - JOHNSON AVENUE (FAU 5954)
LOCATION #5 - WASHINGTON STREET (FAU 5962)



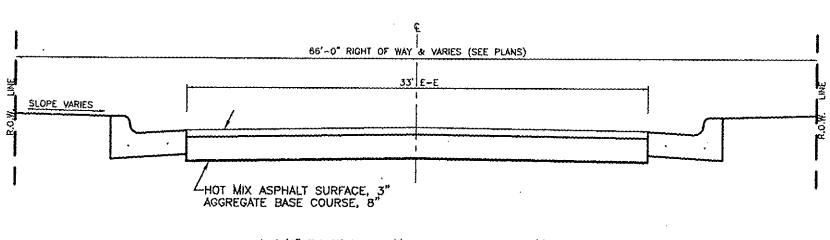
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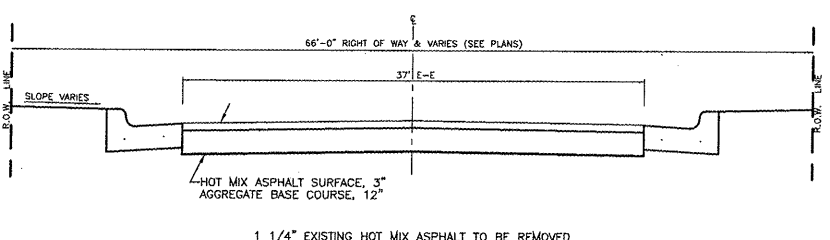
EXISTING
LOCATION #2 - LISBON STREET (FAU 5967)



EXISTING
LOCATION #3 - HIGH STREET (FAU 5953)



EXISTING
LOCATION #4 - JOHNSON AVENUE (FAU 5954)



EXISTING
LOCATION #5 - WASHINGTON STREET (FAU 5962)

DRAWN BY: MAB
CHECKED BY: RJS
DATE: 4/1/09

DATE	BY
5/12/09	MAB
5/21/09	MEF

CHAMLIN ASSOCIATES
PERU MORRIS ILLINOIS

MORRIS ARRA PROJECT 2009
CITY OF MORRIS, GRUNDY COUNTY, ILLINOIS

GENERAL NOTES & TYPICAL SECTIONS
SCALE: 1 = 20
SHEET 3
FILE NO.: 12883
OF 12