

DIXON MUNICIPAL AIRPORT CHARLES R. WALGREEN FIELD DIXON, ILLINOIS

CONSTRUCTION PLANS FOR DIXON MUNICIPAL AIRPORT REHABILITATE T-HANGAR TAXIWAYS

ILLINOIS PROJECT: C73-3914
A.I.P. PROJECT: 3-17-0036-B12

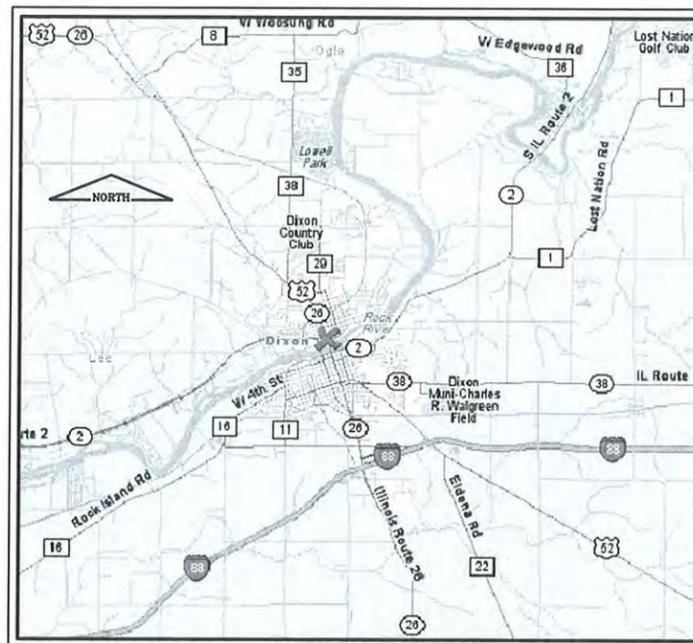


Exp: 11/30/09
Sheets 1-7, 9-10, 13,
20-22

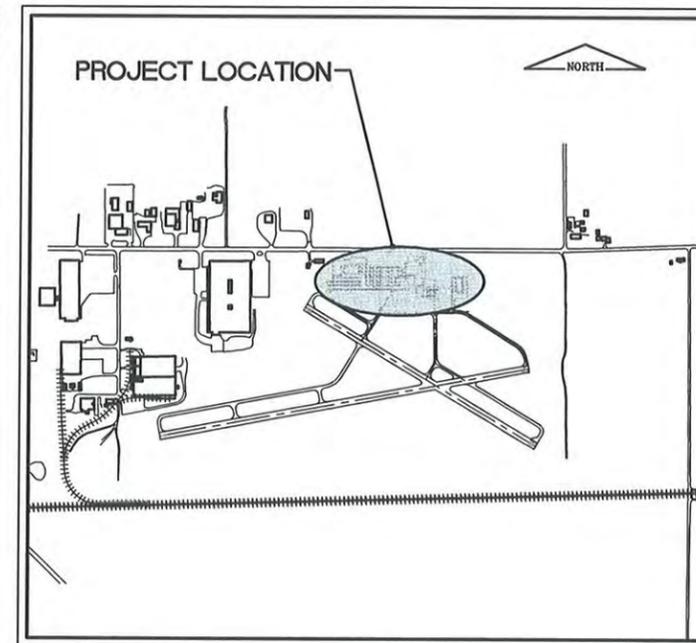
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JUNE 19, 2009



LOCATION MAP



SITE PLAN

DESIGN INFORMATION

DESIGN AIRCRAFT APPROACH CATEGORY A
DESIGN AIRCRAFT GROUP I



CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS

09244-02

SUBMITTED BY *Craig Louden*
CRAIG LOUDEN, P.E.
DATE 6/19/09 2009

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 FILE: K:\Dixon\02\024402\Draw Sheets\02 SUMMARY OF QUANTITIES.dwg
 UPDATE BY: Craig Loudon
 LAYOUT: Layout1
 REF: DWG: 6MP1111.dwg
 IMAGE FILE: :STILL_E09WMI

SUMMARY OF QUANTITIES

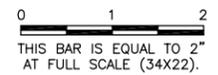
BASE BID				
ITEM NUMBER	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR150520	MOBILIZATION	LS	1	
AR152410	UNCLASSIFIED EXCAVATION	CY	4,500	
AR156510	SILT FENCE	LF	375	
AR156513	SEPARATION FABRIC	SY	3,525	
AR156520	INLET PROTECTION	EACH	1	
AR156531	EROSION CONTROL BLANKET	SY	4,840	
AR201610	BITUMINOUS BASE COURSE	TON	1,025	
AR208515	POROUS GRANULAR EMBANKMENT	CY	1,470	
AR209607	CRUSHED AGG. BASE COURSE - 7"	SY	770	
AR209608	CRUSHED AGG. BASE COURSE - 8"	SY	8,755	
AR401610	BITUMINOUS SURFACE COURSE	TON	1,135	
AR401650	BITUMINOUS PAVEMENT MILLING	SY	1,410	
AR401655	BUTT JOINT CONSTRUCTION	SY	25	
AR401900	REMOVE BITUMINOUS PAVEMENT	SY	9,435	
AR401910	REMOVE & REPLACE BIT. PAVEMENT	SY	10	
AR501605	5" PCC SIDEWALK	SF	6,935	
AR501910	REMOVE & REPLACE PCC PAVEMENT	SY	90	
AR602510	BITUMINOUS PRIME COAT	GAL	1,990	
AR603510	BITUMINOUS TACK COAT	GAL	1,125	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	1,600	
AR701512	12" RCP, CLASS IV	LF	318	
AR701515	15" RCP, CLASS IV	LF	171	
AR701900	REMOVE PIPE	LF	35	
AR705524	4" PERFORATED UNDERDRAIN W/sock	LF	2,050	
AR705640	UNDERDRAIN CLEANOUT	EACH	2	
AR751001	TRENCH DRAIN	LF	208	
AR751411	INLET - TYPE A	EACH	2	
AR751412	INLET - TYPE B	EACH	1	
AR751540	MANHOLE 4'	EACH	2	
AR751900	REMOVE INLET	EACH	1	
AR754710	CONCRETE FLUME	LF	83	
AR800162	TEMPORARY MOORING EYE	EACH	90	
AR901510	SEEDING	ACRE	1	

ADDITIVE ALTERNATE #1				
ITEM NUMBER	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AS152410	UNCLASSIFIED EXCAVATION	CY	775	
AS156513	SEPARATION FABRIC	SY	1,410	
AS201610	BITUMINOUS BASE COURSE	TON	145	
AS208515	POROUS GRANULAR EMBANKMENT	CY	470	
AS209607	CRUSHED AGG. BASE COURSE - 7"	SY	175	
AS209608	CRUSHED AGG. BASE COURSE - 8"	SY	1,235	
AS401610	BITUMINOUS SURFACE COURSE	TON	35	
AS401900	REMOVE BITUMINOUS PAVEMENT	SY	1,410	
AS501605	5" PCC SIDEWALK	SF	1,560	
AS602510	BITUMINOUS PRIME COAT	GAL	250	
AS701515	15" RCP, CLASS IV	LF	99	
AS705524	4" PERFORATED UNDERDRAIN W/sock	LF	165	
AS751001	TRENCH DRAIN	LF	104	
AS751540	MANHOLE 4'	EACH	1	
AS754915	REMOVE CONCRETE FLUME	LF	186	

IL CONTRACT: **DIO25**
 IL LETTING ITEM: **12A**
 IL PROJECT: **C73-3914**
 A.I.P. PROJECT: **3-17-0036-B12**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT
 DIXON, ILLINOIS
 REHABILITATE T-HANGAR PAVEMENTS**

SUMMARY OF QUANTITIES

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DESIGN BY:	JRL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DLP
DATE:	06/19/2009
JOB No:	09244-02

FINAL

KREF: DWG: 12121.dwg
 Dixon-plan-base.dwg

IMAGE FILES:

UPDATE BY: Craig Louden
 LAYOUT: Layout1

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 FILE: K:\Dixon\12121\02\Draw Sheets\03 SITE PLAN - PROJECT CONTROL PLAN.dwg

VERTICAL CONTROL				
BENCHMARK	DESCRIPTION	NORTHING	EASTING	ELEVATION
BM #1	NGS MARKER - STAINLESS STEEL ROD IN SLEEVE IN TURF AREA JUST WEST OF RUNWAY 12/30 - RUNWAY 8/26 INTERSECTION (NOT SHOWN)	1882891.680	2493225.960	778.96
BM #2	NGS MARKER - STAINLESS STEEL ROD IN SLEEVE IN TURF AREA JUST EAST OF TAXIWAY (NOT SHOWN)	1882579.037	2491500.381	778.23

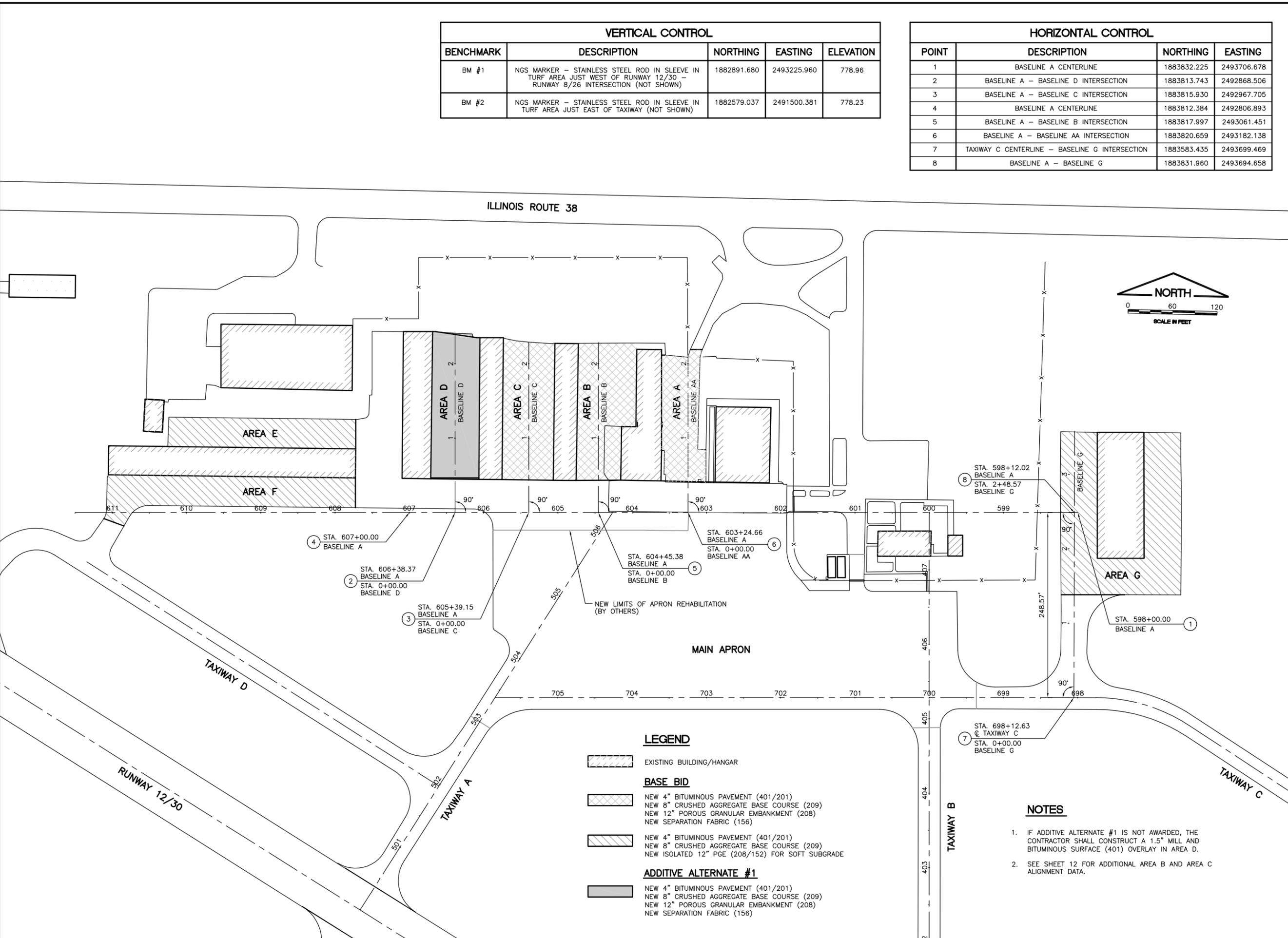
HORIZONTAL CONTROL			
POINT	DESCRIPTION	NORTHING	EASTING
1	BASELINE A CENTERLINE	1883832.225	2493706.678
2	BASELINE A - BASELINE D INTERSECTION	1883813.743	2492868.506
3	BASELINE A - BASELINE C INTERSECTION	1883815.930	2492967.705
4	BASELINE A CENTERLINE	1883812.384	2492806.893
5	BASELINE A - BASELINE B INTERSECTION	1883817.997	2493061.451
6	BASELINE A - BASELINE AA INTERSECTION	1883820.659	2493182.138
7	TAXIWAY C CENTERLINE - BASELINE G INTERSECTION	1883583.435	2493699.469
8	BASELINE A - BASELINE G	1883831.960	2493694.658

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0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



LEGEND

- EXISTING BUILDING/HANGAR
- BASE BID**
 - NEW 4" BITUMINOUS PAVEMENT (401/201)
 - NEW 8" CRUSHED AGGREGATE BASE COURSE (209)
 - NEW 12" POROUS GRANULAR EMBANKMENT (208)
 - NEW SEPARATION FABRIC (156)
- ADDITIVE ALTERNATE #1**
 - NEW 4" BITUMINOUS PAVEMENT (401/201)
 - NEW 8" CRUSHED AGGREGATE BASE COURSE (209)
 - NEW 12" POROUS GRANULAR EMBANKMENT (208)
 - NEW SEPARATION FABRIC (156)

NOTES

1. IF ADDITIVE ALTERNATE #1 IS NOT AWARDED, THE CONTRACTOR SHALL CONSTRUCT A 1.5" MILL AND BITUMINOUS SURFACE (401) OVERLAY IN AREA D.
2. SEE SHEET 12 FOR ADDITIONAL AREA B AND AREA C ALIGNMENT DATA.

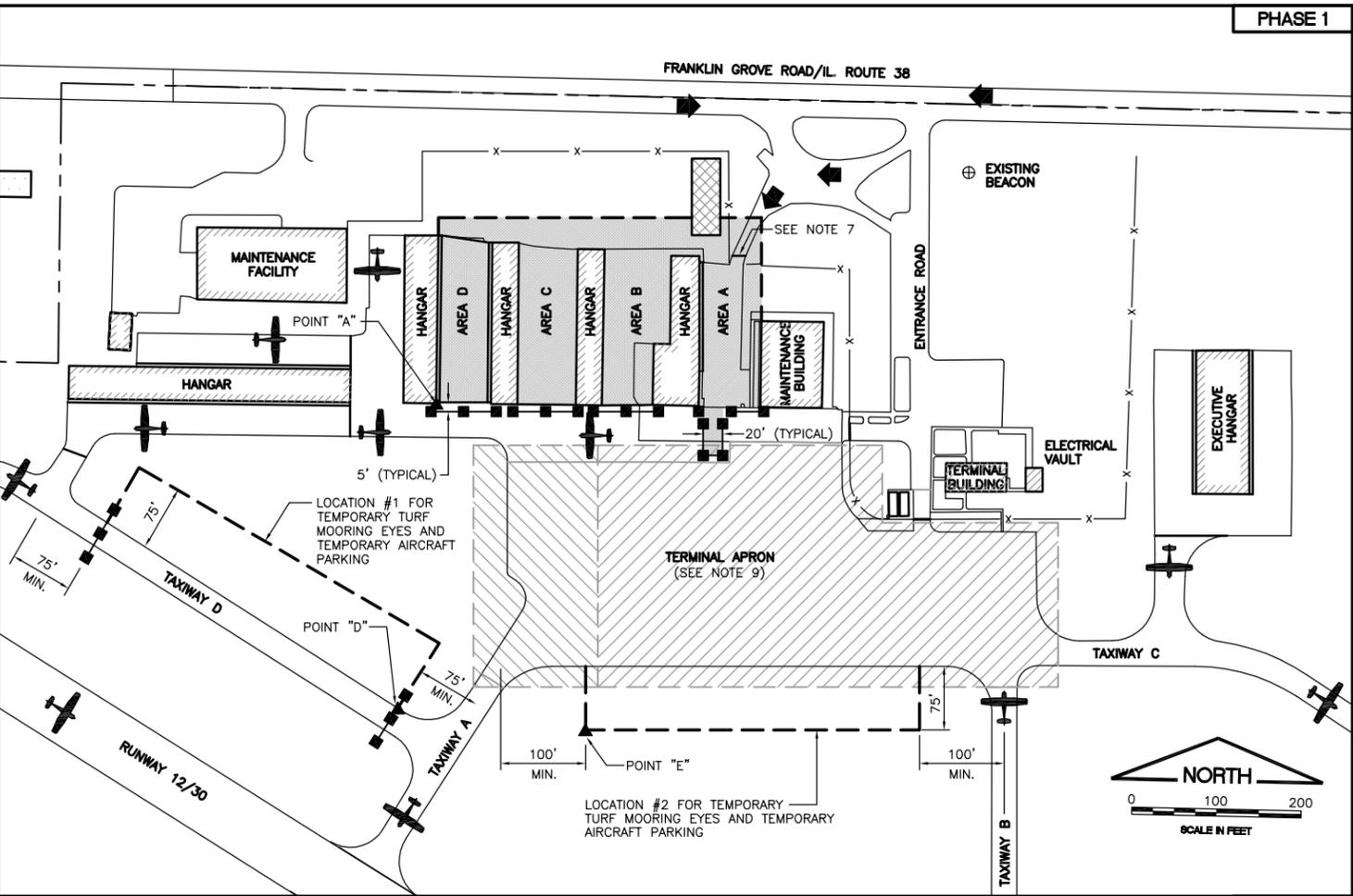
DIXON MUNICIPAL AIRPORT
 DIXON, ILLINOIS
 REHABILITATE T-HANGAR PAVEMENTS
 PROJECT CONTROL PLAN
 SITE PLAN/

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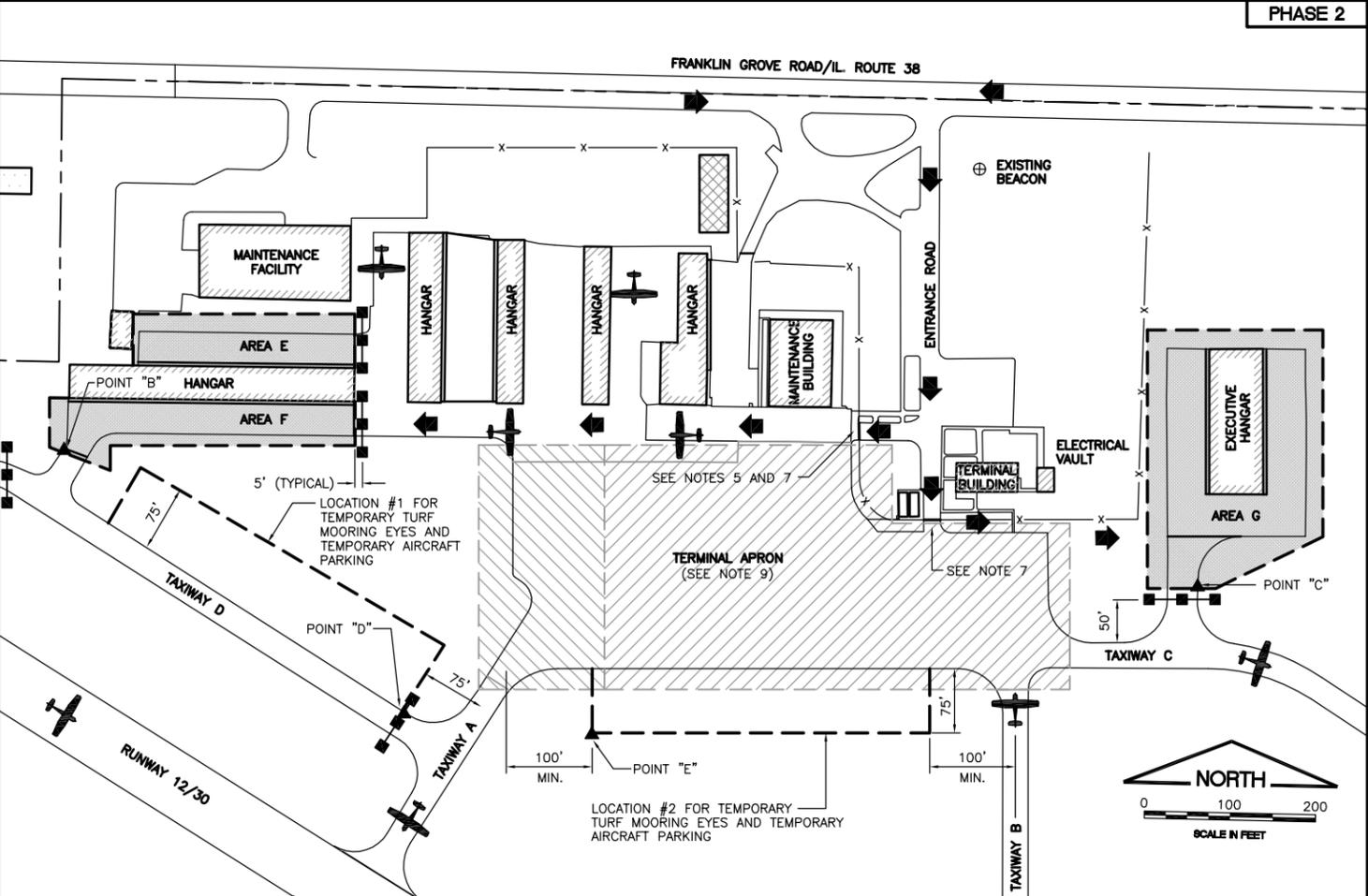
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DRAWN BY:	JRO
CHECKED BY:	CAL
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DATE:	06/19/2009
JOB No:	09244-02

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 UPDATE BY: Craig Loudon
 LAYOUT: Layout1
 REF DWG: Dixon-print-base.dwg
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PHASE 1



PHASE 2

SUGGESTED SEQUENCE OF CONSTRUCTION (EACH PHASE)

- COORDINATE RELOCATION OF ANY AIRCRAFT IN WORK AREA (BY OTHERS) WITH RESIDENT ENGINEER AND AIRPORT MANAGER.
- PLACE BARRICADES AS SHOWN.
- REMOVE EXISTING PAVEMENTS PER PLAN.
- CONSTRUCT STORM SEWER AND EARTHWORK.
- GRADE/CONSTRUCT BASE COURSE.
- CONSTRUCT CONCRETE PAVEMENTS.
- CONSTRUCT BITUMINOUS PAVEMENTS.
- INSTALL PAVEMENT MARKINGS PER PLAN.
- REMOVE BARRICADES AND MISCELLANEOUS DEBRIS FROM CONSTRUCTION AREA AND CLEAN PAVEMENTS.
- COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER SO THAT DISPLACED AIRCRAFT CAN BE RELOCATED TO THEIR ORIGINAL STORAGE AREAS (BY OTHERS).

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT:

- REHABILITATE TERMINAL APRON (SEE NOTE 9)

LEGEND

- TERMINAL APRON CONSTRUCTION AREA PHASE 1 (BY OTHERS)(SEE NOTE 9)
- TERMINAL APRON CONSTRUCTION AREA PHASE 2 (BY OTHERS)(SEE NOTE 9)
- PROJECT PHASE WORK LIMITS
- EXISTING HANGAR / BUILDING
- EXISTING PAVEMENT
- CONTRACTOR STAGING, STORAGE AND PARKING/ENGINEER'S FIELD OFFICE
- EXISTING AIRPORT PROPERTY LINE
- TEMPORARY BARRICADE PLACEMENT
- AIRCRAFT MOVEMENT AREA
- CONTRACTOR'S ACCESS / HAUL ROUTE
- NEAREST POINT ON CONSTRUCTION SITE TO RUNWAYS

NOTES

1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON EACH PHASE OR AS DIRECTED BY THE RESIDENT ENGINEER.
2. MARK THE AIR OPERATIONS AREA (A.O.A.) WITH LATHE AND RIBBON AS SHOWN BY THE CONSTRUCTION SETBACK LINE OR AS DIRECTED BY THE RESIDENT ENGINEER.
3. THE AIRPORT WILL REQUIRE SEVEN (7) CALENDAR DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING ANY PHASE TO COORDINATE WITH THE TENANTS.
4. FOR ALL PHASES OF WORK, THE CONTRACTOR SHALL RETURN THE EQUIPMENT TO THE STAGING AND STORAGE AREA UNLESS PERMISSION IS GIVEN BY THE AIRPORT TO DO OTHERWISE.
5. CONTRACTOR SHALL HAVE A DEDICATED FLAGGER ONSITE WHEN WORKING ADJACENT TO OR HAULING ON ACTIVE TAXIWAYS AND YIELD TO ALL ONCOMING AIRCRAFT. COST SHALL BE INCIDENTAL TO THE CONTRACT.
6. CONTRACTOR SHALL COORDINATE WITH THE AIRPORT MANAGER AND ENGINEER ON THE LOCATION OF ACCESS ROUTES OF AIRPORT-OWNED FUELING TRUCKS TO ALL AIRCRAFT AND TEMPORARY PARKING AREAS.
7. THE CONTRACTOR SHALL POST A SECURITY GUARD AT THE GATE WHEN THE GATE IS LEFT OPEN FOR THE PURPOSES OF HAULING MATERIAL IN OR OUT OF THE JOBSITE. SHOULD THE CONTRACTOR CHOOSE TO CLOSE THE GATE AFTER EACH ARRIVAL OR DEPARTURE, NO GUARD SHALL BE REQUIRED. THE AIRPORT SHALL RESERVE THE RIGHT TO REQUIRE THE CONTRACTOR TO CHANGE GUARD PERSONNEL SHOULD THEY PERFORM UNSATISFACTORILY.
8. THE CONTRACTOR SHALL RESTORE HIS STAGING, STORAGE AND PARKING AREA TO THE PRE-CONSTRUCTION STATE AT THE COMPLETION OF THE PROJECT. RESTORATION COSTS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
9. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT MANAGER, ENGINEER AND TERMINAL APRON CONSTRUCTION PERSONNEL REGARDING THE TIMING, PHASING AND HANGAR ACCESS. THE FINAL PLAN SHALL BE ACCEPTABLE AND APPROVED BY THE AIRPORT MANAGER. NO ADDITIONAL COMPENSATION WILL BE MADE TO ACCOMMODATE COORDINATION OF CONSTRUCTION WITH THE REHABILITATE TERMINAL APRON PROJECT.
10. AT LEAST ONE AIRPORT ENTRANCE MUST BE OPEN AT ALL TIMES.

CRITICAL POINTS

- RUNWAY 8**
- LATITUDE: 41° 49' 56.843" (NAD 83)
 - LONGITUDE: 89° 27' 16.139" (NAD 83)
 - EXISTING ELEVATION: 781.0'
- RUNWAY 26**
- LATITUDE: 41° 50' 03.593" (NAD 83)
 - LONGITUDE: 89° 26' 25.428" (NAD 83)
 - EXISTING ELEVATION: 785.0'
- RUNWAY 12**
- LATITUDE: 41° 50' 10.036" (NAD 83)
 - LONGITUDE: 89° 26' 56.129" (NAD 83)
 - EXISTING ELEVATION: 777.0'
- RUNWAY 30**
- LATITUDE: 41° 49' 55.629" (NAD 83)
 - LONGITUDE: 89° 26' 24.507" (NAD 83)
 - EXISTING ELEVATION: 785.0'
- NEAREST POINTS ON CONSTRUCTION SITE TO ACTIVE RUNWAYS 8/26 AND 12/30**
- PHASE 1: POINT "A"**
- LATITUDE: 41° 50' 12.29"
 - LONGITUDE: 89° 26' 47.30"
 - EXISTING ELEVATION: 777.20'
- PHASE 2: POINT "B"**
- LATITUDE: 41° 50' 11.69"
 - LONGITUDE: 89° 26' 53.10"
 - EXISTING ELEVATION: 775.00'
- PHASE 2: POINT "C"**
- LATITUDE: 41° 50' 10.30"
 - LONGITUDE: 89° 26' 35.55"
 - EXISTING ELEVATION: 781.00'
- TEMPORARY AIRCRAFT PARKING: POINT "D"**
- LATITUDE: 41° 50' 08.12"
 - LONGITUDE: 89° 26' 47.82"
 - EXISTING ELEVATION: 782.00'
- TEMPORARY AIRCRAFT PARKING: POINT "E"**
- LATITUDE: 41° 50' 08.50"
 - LONGITUDE: 89° 26' 44.06"
 - EXISTING ELEVATION: 779.00'

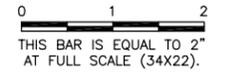
DESIGN AIRCRAFT APPROACH CATEGORY: A
DESIGN AIRPORT GROUP: I

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 25 FEET

IL CONTRACT: **DIO25**
IL LETTING ITEM: **12A**
IL PROJECT: **C73-3914**
A.I.P. PROJECT: **3-17-0036-B12**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE T-HANGAR PAVEMENTS
SEQUENCE OF CONSTRUCTION PER
AC 150/5370-2E (LATEST EDITION)**

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DRAWN BY:	JRO
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APPROVED BY:	DLP
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 LAYOUT: Layout1
 XREF DWG: 0924402.dwg
 IMAGE FILES:

GENERAL NOTES

- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED. ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2E (LATEST EDITION) OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION AND AC 150/5300-13 AIRPORT DESIGN.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- THE AIRPORT MANAGER, IN CONSULTATION WITH THE RESIDENT ENGINEER, SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE NOTED.
- FOR WORK ON AIRPORT PROPERTY: PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES AT 10-FOOT CENTERS WITH ONE ORANGE FLAG (24" x 24") BETWEEN EACH SET OF BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A FLASHING RED LIGHT AND CONFORM TO IDOT STANDARD 702001, TYPE II. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER SEVEN (7) CALENDAR DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION (SEE FLAG DETAIL, THIS SHEET). VEHICLES USED FOR HAULING PURPOSES ARE EXEMPT FROM FLAG REQUIREMENTS.
- IN THE CASE OF AN EMERGENCY, THE CONTRACTOR SHALL NOTIFY THE AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- THE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE RESIDENT ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
- A MATERIAL/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.
- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM POINT TO POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF AN FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS, IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.

- COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE RESIDENT ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN THE AIR OPERATIONS AREA (A.O.A.) FOR ACTIVE TAXIWAYS/RUNWAYS UNLESS OTHERWISE APPROVED BY THE AIRPORT MANAGER.
- CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER, AIRPORT MANAGER AND UTILITY OWNER.
- ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MAINTENANCE. ANY DEFICIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.

CONTRACTOR CROSSING RUNWAY AND TAXIWAY AIR OPERATIONS AREA (A.O.A.)

- ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE AIR TRAFFIC SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURRENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
- ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

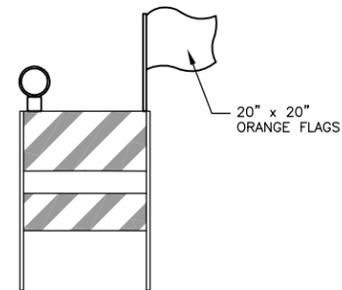
LIMITATIONS ON CONSTRUCTION WITHIN AIRPORT OPERATIONS AREA (A.O.A.)

RUNWAYS:

ANY WORK WITHIN THE A.O.A. OF AN ACTIVE RUNWAY SHALL EITHER BE DONE ON WEEKENDS, OFF-PEAK DAYTIME OR NIGHTTIME HOURS, LOCAL TIME AS SHOWN ON THE SEQUENCE OF CONSTRUCTION PLAN SHEETS. ON ANY DAY WHEN CONSTRUCTION IS WITHIN THE A.O.A. OF THE RUNWAY, THE RUNWAY SHALL BE CLOSED. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. STEEL PLATES IF NECESSARY SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE A.O.A. IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT.

TAXIWAYS / TAXILANES / APRONS:

CONSTRUCTION WILL BE ALLOWED UP TO THE EDGE OF PAVEMENTS WITHOUT CLOSURE ON A LIMITED BASIS. WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

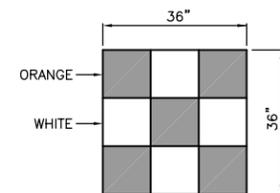


FLASHER BARRICADE DETAIL
NOT TO SCALE

NOTES

- FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
- SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- PLACE AT TEN-FOOT (10') INTERVALS.
- ONE 20"x20" ORANGE FLAG TO BE INSTALLED ON EACH BARRICADE.

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 25 FEET



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
NOT TO SCALE

IL. CONTRACT: **DIO25**
IL. LETTING ITEM: **12A**
IL. PROJECT: **C73-3914**
A.I.P. PROJECT: **3-17-0036-B12**

SURVEY BOOK # BOOK #

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE T-HANGAR PAVEMENTS**

**SEQUENCE OF CONSTRUCTION
NOTES AND DETAILS**

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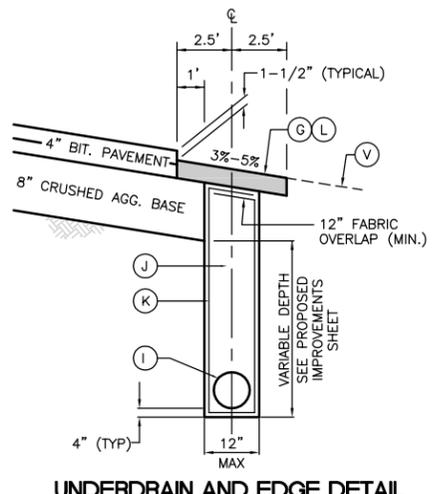
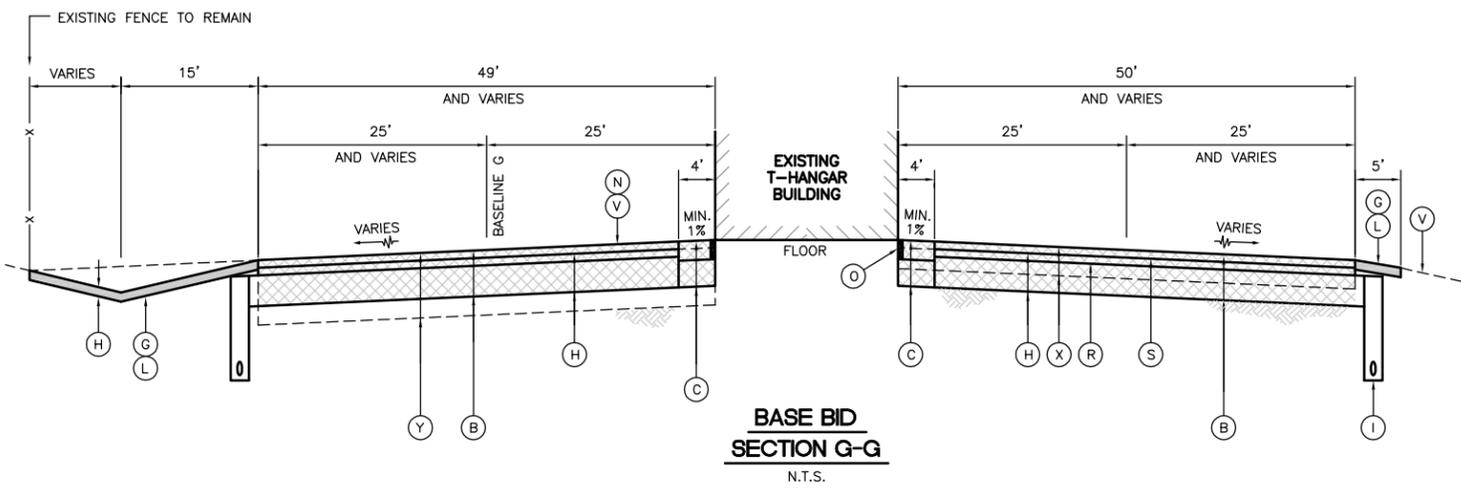
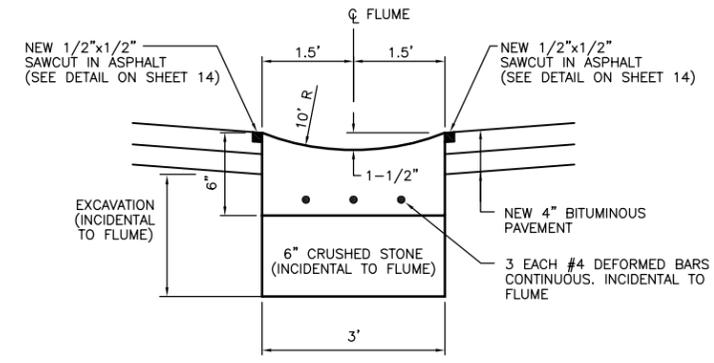
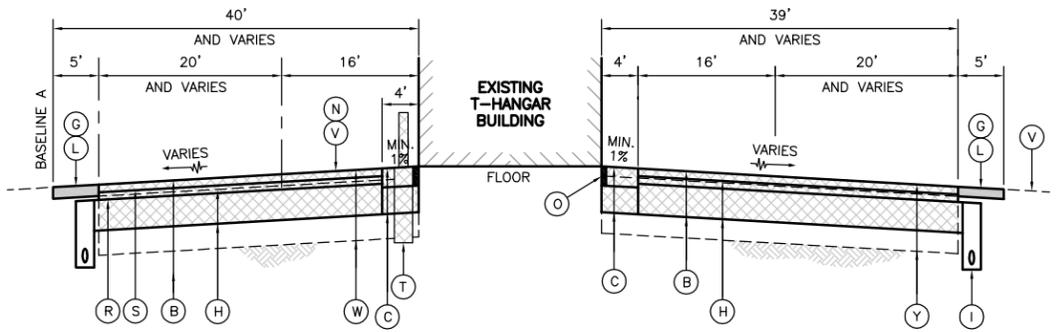
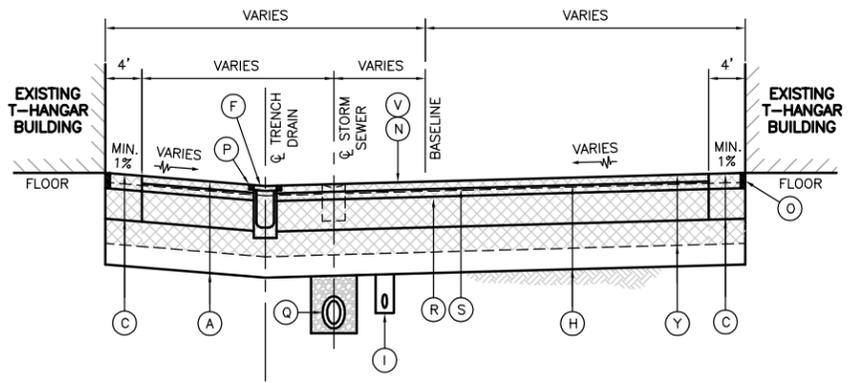
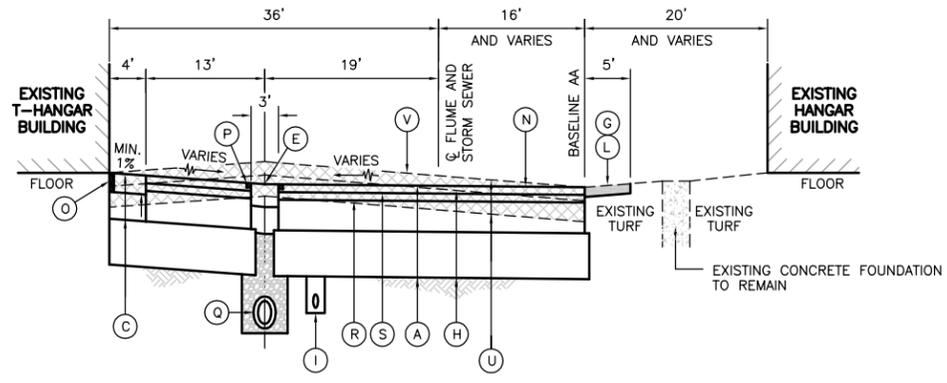
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APPROVED BY:	DLP
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 FILE: K:\Dixon\0924402\Drawn Sheets\06 TYPICAL SECTIONS - SHEET 1.dwg
 UPDATE BY: Craig Loudon
 LAYOUT: Layout1
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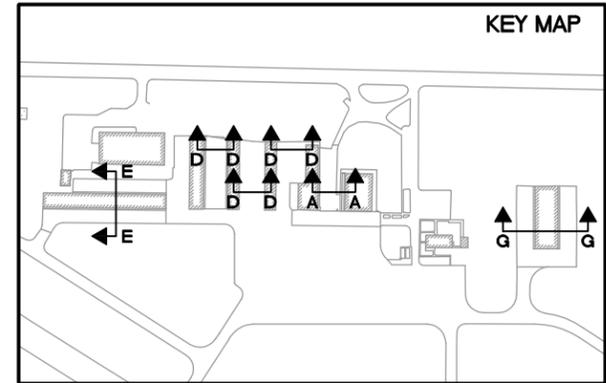


LEGEND

- (A) NEW BASE BID AND ADDITIVE ALTERNATE #1 PAVEMENT STRUCTURE
2" BITUMINOUS SURFACE COURSE (401)
2" BITUMINOUS BASE COURSE (201)
8" CRUSHED AGGREGATE BASE COURSE (209)
12" POROUS GRANULAR EMBANKMENT (208)(152)
SEPARATION FABRIC (156)
- (B) NEW BASE BID PAVEMENT STRUCTURE
2" BITUMINOUS SURFACE COURSE (401)
2" BITUMINOUS BASE COURSE (201)
8" CRUSHED AGGREGATE BASE COURSE (209)
ISOLATED 12" PGE FOR SOFT SUBGRADE (208)(152)(NOT SHOWN)
- (C) NEW 5" PCC SIDEWALK (501)
NEW 7" CRUSHED AGGREGATE BASE COURSE (209)
- (E) NEW 3' WIDE, 6" CONCRETE FLUME (754)
NEW 6" CRUSHED AGGREGATE BASE COURSE (INCIDENTAL TO FLUME)(209)
- (F) NEW TRENCH DRAIN (751)
- (G) NEW TOPSOIL PLACEMENT 4" MIN. (152)
- (H) NEW UNCLASSIFIED EXCAVATION (152)
- (I) NEW 4" PERFORATED UNDERDRAIN W/ SOCK - SOCK INCIDENTAL TO 4" UNDERDRAIN (705)
- (J) NEW GRAVEL BACKFILL CA-16/CA-14 COST INCIDENTAL TO 4" UNDERDRAIN (705)
- (K) NEW UNDERDRAIN TRENCH FABRIC ENVELOPE - COST INCIDENTAL TO 4" UNDERDRAIN (705)
- (L) NEW SEEDING AND EROSION CONTROL BLANKET (901 AND 156)
- (N) NEW GROUNDLINE
- (O) NEW 1" EXPANSION JOINT (INCIDENTAL TO 501)
- (P) NEW 1/2"x1/2" SAWCUT IN ASPHALT (SEE DETAIL ON SHEET 14)
- (Q) NEW STORM SEWER WITH TRENCH BACKFILL (701)
- (R) NEW PRIME COAT (602)
- (S) NEW TACK COAT (603)
- (T) EXISTING WOOD POST AND FOUNDATION TO BE REMOVED (INCIDENTAL TO BITUMINOUS PAVEMENT REMOVAL)(401)
- (U) EXISTING PAVEMENT TO BE REMOVED IN NEW PAVEMENT SECTION 3'-1/2" (AVG.) BITUMINOUS PAVEMENT (401)
5'-1/2" (AVG.) AGGREGATE (152)
- (V) EXISTING GROUNDLINE
- (W) EXISTING PAVEMENT TO BE REMOVED IN NEW PAVEMENT SECTION 3" (AVG.) BITUMINOUS PAVEMENT (401)
15'-1/2" (AVG.) AGGREGATE (152)
- (X) EXISTING PAVEMENT TO BE REMOVED IN NEW PAVEMENT SECTION 2" (AVG.) BITUMINOUS PAVEMENT (401)
5'-1/2" (AVG.) AGGREGATE (152)
- (Y) EXISTING PAVEMENT TO BE REMOVED IN NEW PAVEMENT SECTION 2'-1/2" (AVG.) BITUMINOUS PAVEMENT (401)
15" (AVG.) AGGREGATE (152)

NOTES

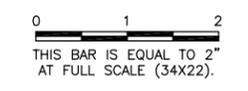
- FOR TRENCH DRAIN DETAIL SEE SHEET 16.



IL. CONTRACT: **DIO25**
 IL. LETTING ITEM: **12A**
 IL. PROJECT: **C73-3914**
 A.I.P. PROJECT: **3-17-0036-B12**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT
 DIXON, ILLINOIS
 REHABILITATE T-HANGAR PAVEMENTS**
TYPICAL SECTIONS

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Dixon-pm1-base.dwg

IMAGE FILES:

UPDATE BY: Craig Louden
LAYOUT: Layout1

DATE: Friday, June 19, 2009 3:19:52 PM
FILE: K:\Dixon\12-11-2009\Drawn Sheets\14 DRAINAGE DETAILS.dwg

DETAIL B

* DEPTH AS RECOMMENDED BY SEALANT MANUFACTURER

1/2" ± 1/16"

1/4" MIN. - 3/8" MAX BELOW PAVEMENT

5/8" BACKER ROD MATERIAL

HOT-POURED JOINT SEALER (ASTM D6690)

CONSTRUCTION JOINTS

CONTRACTION JOINTS

TYPE H DUMMY

SYMBOL ---

TYPE D DOWELED

SYMBOL --- x x x ---

EXISTING PAVEMENT

SEE TABLE 2 FOR SIZE AND SPACING DOWEL BAR

DRILL d + 1/4" DIA. HOLE INTO EXISTING PAVEMENT. SET DOWEL BAR IN CHEMICAL ADHESIVE PER 501 SPEC. CONTRACTOR MAY PROPOSE ALTERNATE METHODS FOR APPROVAL BY THE RESIDENT ENGINEER.

GREASE THIS END

TABLE 1

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T, INCHES
9	2.25"

TABLE 2

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA. (d)	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
9	1"	19"	12"	#5	30"	30"

DIMENSION TABLES

JOINT NOTES

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY OR MECHANICALLY INSTALL PER ARTICLE 420.10 JOINTS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES OR MECHANICAL METHOD SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
- COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
- SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
- ALL JOINTS TO BE HOT-POUR JOINT SEAL PER THESE DETAILS.

PERSPECTIVE VIEW

5' MAX C. TO C.

48" MIN. FENCE POSTS, DRIVEN 24" MINIMUM INTO GROUND

SILT FENCE (FABRIC FENCE)

24" MIN. HEIGHT OF FILTER

6" MIN.

SECTION

48" MIN. FENCE POST

24" MIN.

24" MIN.

EMBEDDED FILTER CLOTH

UNDISTURBED GROUND

EROSION CONTROL FABRIC FENCE DETAIL

NOT TO SCALE

CONSTRUCTION NOTES FOR SILT (FABRIC) FENCE

- WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVER-LAPPED BY 6-INCH MIN. AND FOLDED.
- MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE, SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.
- SILT FENCE SHALL BE INSTALLED PER STORM WATER POLLUTION PREVENTION PLAN OR AS DIRECTED BY THE ENGINEER.

DRAINAGE STRUCTURE FILTER WRAP

NOT TO SCALE

GRATE

FILTER FABRIC

DRAINAGE FRAME

NOTES:

- FILTER WRAP TO BE PLACED IN ALL SLOPE BOXES, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
- FABRIC SHALL BE IN CONFORMANCE WITH MATERIALS SPECIFIED FOR FABRIC FENCE.
- FABRIC SHALL OVERLAY FRAME BY 2-INCH (MINIMUM).
- CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
- FABRIC SHALL REMAIN IN PLACE UNTIL TURFED AREAS HAVE DEVELOPED A MINIMUM OF 80% OF COVERAGE.
- COST OF FILTER WRAP SHALL BE CONSIDERED INCIDENTAL TO BALES PLACED FOR INLET PROTECTION.

CONCRETE COLLAR - STORM SEWER

NOT TO SCALE

ITEM 610 P.C. CONCRETE

EXISTING PIPE

PROPOSED PIPE

8" TOP AND SIDES

8" BOTTOM

MATCH INVERTS

STABLE SUBGRADE

NOTE: COST INCIDENTAL TO INSTALLATION OF PROPOSED PIPE

IF SUBGRADE IS DISTURBED OVER EXCAVATE AND FILL "HAUNCH" WITH GRANULAR BACKFILL (COST INCIDENTAL TO PROPOSED STORM SEWER INSTALLATION)

BUTT JOINT DETAIL

NOT TO SCALE

NEW BITUMINOUS SURFACE COURSE

NEW BUTT JOINT CONSTRUCTION PARTIAL DEPTH REMOVAL 2" AND LESS

NEW 2" DEEP SAWCUT (COST INCIDENTAL)

10' AND VARIES SEE PLAN SHEETS

EXISTING PAVEMENT

NEW TACK COAT ON VERTICAL AND HORIZONTAL SURFACES

INLET PLACEMENT

NOT TO SCALE

PROPOSED BALES (8 PER STRUCTURE) AND EROSION CONTROL FENCING

INLET, CATCH BASIN OR MANHOLE WITH FILTER WRAP

EMBEDDED 4" MIN.

INLET PLACEMENT

8" MAX.

8" MAX.

LIMITS OF LANDSCAPING (SEE NOTE 6)

INLET PROTECTION (INLET/MANHOLES)

NOT TO SCALE

PCC SIDEWALK DETAIL

NOT TO SCALE

EXISTING HANGAR

NEW 1" EXPANSION JOINT (CERAMAR BY WR MEADOWS OR EQUAL) SEALED W/ HOT POURED JOINT SEALANT (ASTM D6690)

EXISTING HANGAR FOUNDATION/FLOOR

EXISTING CONCRETE PIER

NEW 5" PCC SIDEWALK

NEW 7" CRUSHED AGGREGATE BASE

NEW 1" EXPANSION JOINT (CERAMAR BY WR MEADOWS OR EQUAL)

NEW ASTM 6690 HOT POURED JOINT SEALANT

NEW BITUMINOUS PAVEMENT

NEW SAWED/TOOLED JOINT (TYPICAL)

4'

5'

(TYPICAL)

JOINT SEALING AT CONCRETE ASPHALT INTERFACE

N.T.S.

NOTE: JOINT DIMENSIONS SHALL MEET MANUFACTURER'S RECOMMENDATION

1/2" X 1/2" SAWCUT IN ASPHALT JOINT SEALING COMPOUND (ASTM D 6690) OR EQUAL WITH BACKER ROD.

CONCRETE

ASPHALT

AREA A

NEW BITUMINOUS PAVEMENT

NEW 1/2"x1/2" SAWCUT IN ASPHALT (SEE DETAIL)

REMOVE AND REPLACE 89 S.Y. OF 9" PCC

EXISTING 9" PCC

TYPE D JOINT

TYPE H JOINT (TYPICAL)

TYPE H JOINT (TYPICAL)

10' (TYPICAL)

10' (TYPICAL)

10' (TYPICAL)

4 SPACES @ 10' = 40' (MATCH EXISTING JOINTING)

BASELINE AA

EXISTING 9" PCC

TYPE D JOINT

TYPE H JOINT (TYPICAL)

BASELINE A

NEW 1/2"x1/2" SAWCUT IN ASPHALT (SEE DETAIL)

REMOVE AND REPLACE 7 S.Y. 3" BITUMINOUS PAVEMENT REMOVE AND REPLACE

2 SPACES @ 10' = 20' (MATCH EXISTING JOINTING)

PCC REMOVAL DETAIL

NOT TO SCALE

IL CONTRACT: **DIO25**

IL LETTING ITEM: **12A**

IL PROJECT: **C73-3914**

A.I.P. PROJECT: **3-17-0036-B12**

SURVEY BOOK # BOOK #

REVISIONS

NUMBER	BY	DATE

0 1 2

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE T-HANGAR PAVEMENTS**

MISCELLANEOUS DETAILS

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APPROVED BY: DLP

DATE: 06/19/2009

JOB No: 09244-02

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SHEET 13 OF 22 SHEET



CITY OF DIXON, ILLINOIS

Post Office Box 386
Dixon, Illinois 61021
Telephone: 815.288.1485

DIXON MUNICIPAL AIRPORT
CHARLES R. WALGREEN FIELD

REHABILITATE T-HANGAR TAXIWAYS

AIP PROJECT NO. 3-17-0036-B12
IDA PROJECT NO. C73-3914

UNDERDRAIN SCHEDULE

Structure Data				Pipe Data			
Structure	Station	Offset (ft.)	Type	Rim EL.	Invert EL.	Pay Length (ft.)	Slope (%)
U9	902+01.40	88.35	LT Underdrain Connection	---	---	774.76	
						90.0	0.43
K14	901+97.40	178.54	LT Manhole Connection	777.87	SE 774.37 NE 774.77		
						94.0	0.31
U14	902+01.40	272.57	LT Cleanout	778.08	S 775.06		
U10	903+02.79	88.19	LT Underdrain Connection	---	---	775.35	
						84.0	0.35
K16	902+98.79	171.94	LT Manhole Connection	778.49	SE 775.06 NE 775.25		
						97.5	0.37
U17	903+02.79	269.22	LT Cleanout	778.63	S 775.61		

NOTES:

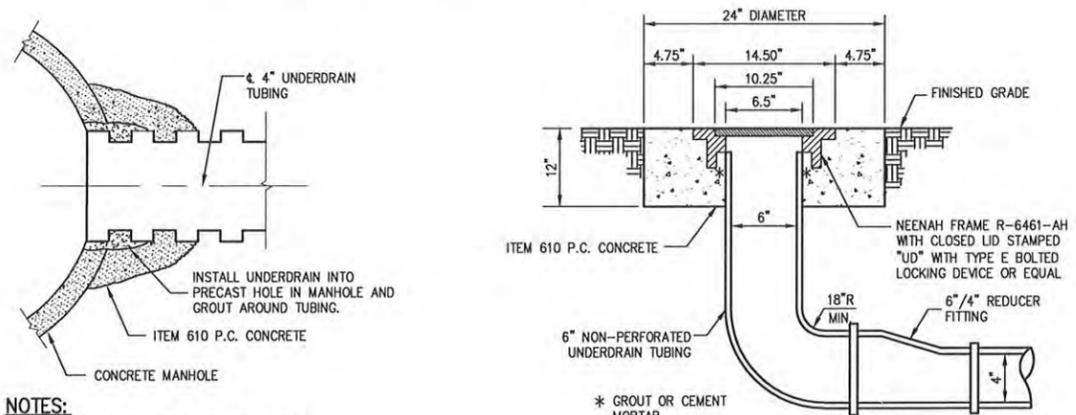
- SEE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS FOR PIPE MATERIAL SPECIFICATIONS.
- SEE DETAILS AND SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS FOR TRENCH BACKFILL AND COMPACTION REQUIREMENTS.
- STATION AND OFFSET IS GIVEN TO THE CENTER OF THE CLEANOUTS.
- SEE DETAILS FOR ADDITIONAL STRUCTURE INFORMATION.
- REFER TO UNDERDRAIN CLEANOUT DETAIL FOR FRAME AND LID INFORMATION.

TRENCH DRAIN SCHEDULE

Structure	Station	Offset	Type	Rim EL.	Invert EL.	Pay Length	Slope %
T5	901+92.23	102.54	LT Trench Drain	777.75	N 776.66		
						56.0	1.00
K14	901+97.40	178.54	LT Manhole Connection	777.87	S 776.10 N 776.10		
						56.0	-1.00
T6	901+92.23	254.54	LT Trench Drain	777.75	S 776.66		
T7	902+93.61	111.94	LT Trench Drain	778.24	N 777.15		
						40.0	1.40
K16	902+98.79	171.94	LT Manhole Connection	778.49	S 776.59 N 776.59		
						56.0	-1.00
T8	902+93.62	247.94	LT Trench Drain	778.24	S 777.15		

NOTES:

- SEE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS FOR PIPE MATERIAL SPECIFICATIONS.
- SEE DETAILS AND SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS FOR TRENCH BACKFILL AND COMPACTION REQUIREMENTS.
- STATIONS AND OFFSETS ARE GIVEN TO THE CENTERLINE OF TRENCH DRAINS.
- TRENCH DRAIN SLOPES BASED ON DISTANCE FROM T5 AND T6, T7 AND T8 TO TRENCH DRAIN ENDS, RESPECTIVELY. PVC PIPES CONNECTING TRENCH DRAINS TO MANHOLES TO BE SLOPED AT 1.4%.



NOTES:

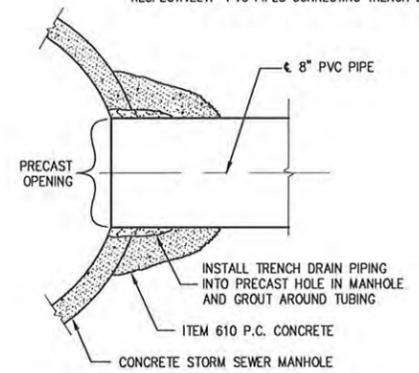
- HOLE FOR UNDERDRAIN TO BE PRECAST INTO MANHOLES AT PROPER INVERT.
- ALL WORK INCIDENTAL TO UNDERDRAIN.

UNDERDRAIN CONCRETE COLLAR AND GROUT CONNECTION

NOTES:

- FRAME AND LID, CONCRETE, 6" NON-PERFORATED TUBING, REDUCER FITTINGS AND ELBOW FITTINGS ARE INCIDENTAL TO CLEANOUT.
- ALL STEEL ITEMS SHALL BE U.S. DOMESTIC STEEL.

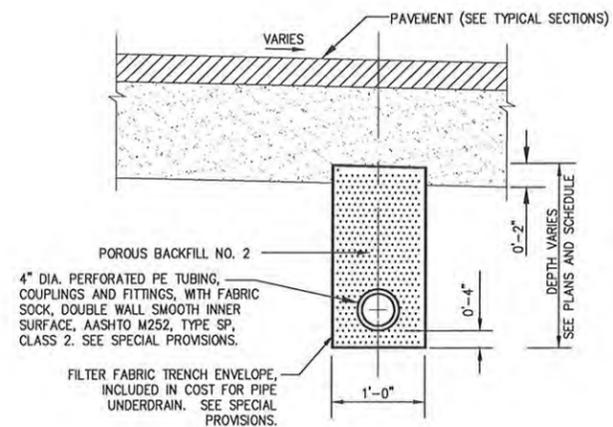
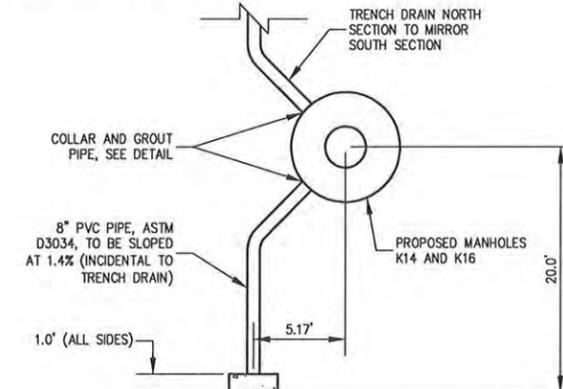
UNDERDRAIN CLEANOUT



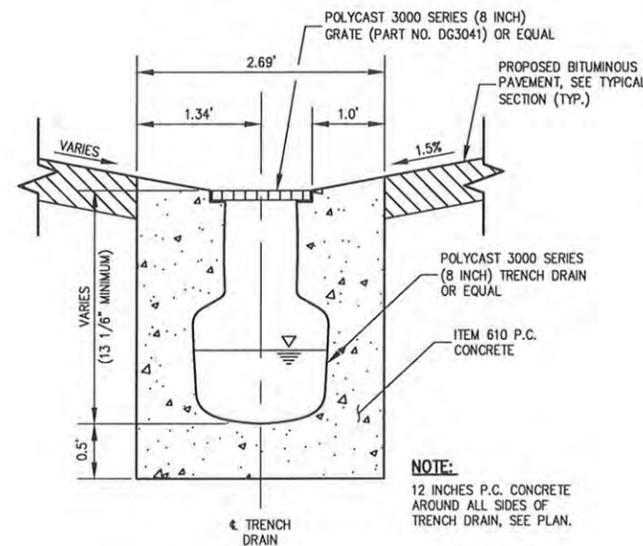
NOTES

- HOLE FOR TRENCH DRAIN PIPING TO BE PRECAST INTO MANHOLE AT ELEVATION SPECIFIED IN TRENCH DRAIN SCHEDULE.
- CONNECTIONS INCIDENTAL TO PIPE.

TRENCH DRAIN CONCRETE COLLAR AND GROUT CONNECTION

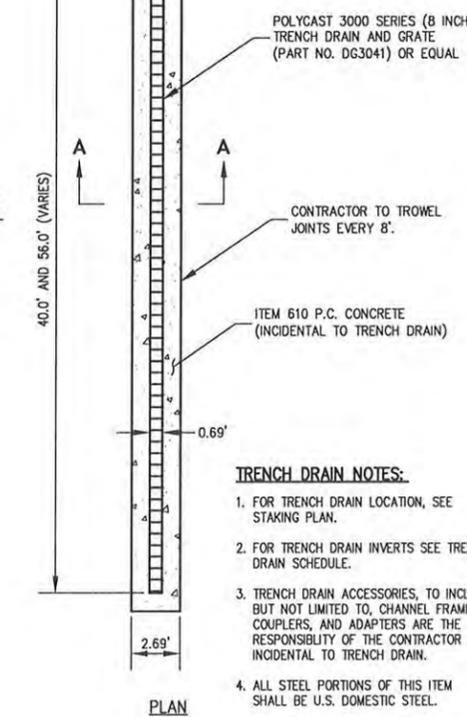


UNDERDRAIN UNDER PAVEMENT



SECTION A-A

TRENCH DRAIN



TRENCH DRAIN NOTES:

- FOR TRENCH DRAIN LOCATION, SEE STAKING PLAN.
- FOR TRENCH DRAIN INVERTS SEE TRENCH DRAIN SCHEDULE.
- TRENCH DRAIN ACCESSORIES, TO INCLUDE BUT NOT LIMITED TO, CHANNEL FRAMES, COUPLERS, AND ADAPTERS ARE THE RESPONSIBILITY OF THE CONTRACTOR AND INCIDENTAL TO TRENCH DRAIN.
- ALL STEEL PORTIONS OF THIS ITEM SHALL BE U.S. DOMESTIC STEEL.

DETAILS SHOWN ARE NOT TO SCALE

DETAILS SHOWN ARE NOT TO SCALE



Lindsay Hausman
EXP 11/30/09

No.	Drawing Issue Description	Date	By

Date

JUNE 19, 2009

Sheet Title

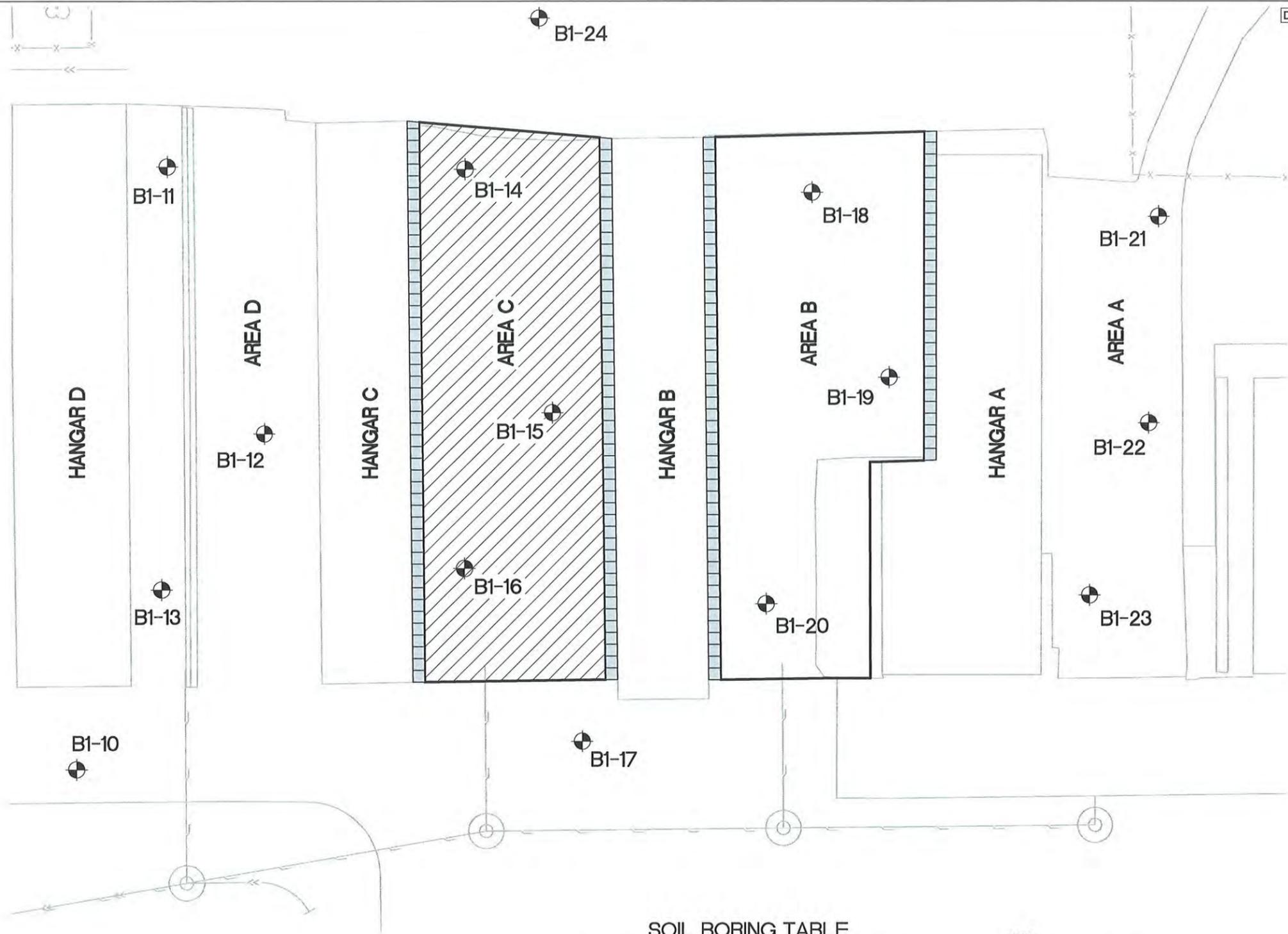
UNDERDRAIN AND TRENCH DRAIN SCHEDULES AND DETAILS

843-04B8007	Project Number
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TMM	01/09/04
RMH	06/12/09
KLK	06/12/09
KLK	TMM LDH

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Lindsay Hausman
EXP. 11/30/09



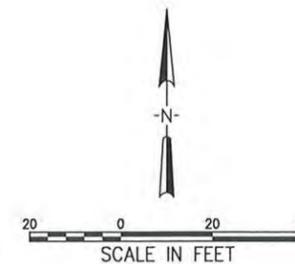
SOIL BORING TABLE

BORING NUMBER	NORTHING	EASTING	DEPTH (FT.)
B1-10	1,883,830.17	2,492,816.28	10
B1-11	1,884,034.86	2,492,846.81	10
B1-12	1,883,944.16	2,492,879.74	10
B1-13	1,883,891.18	2,492,845.20	10
B1-14	1,884,034.06	2,492,948.03	10
B1-15	1,883,951.38	2,492,977.75	10
B1-16	1,883,898.40	2,492,948.03	10
B1-17	1,883,839.81	2,492,988.19	10
B1-18	1,884,026.26	2,493,066.21	10
B1-19	1,883,963.42	2,493,092.63	10
B1-20	1,883,886.36	2,493,050.86	10
B1-21	1,884,018.00	2,493,185.01	10
B1-22	1,883,947.89	2,493,181.76	10
B1-23	1,883,889.30	2,493,161.48	10
B1-24	1,884,085.34	2,492,972.84	10

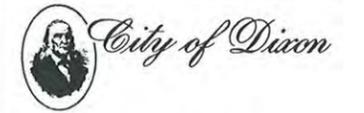
NOTE:

LOCATIONS ARE IN STATE PLANE (ILLINOIS - WEST) COORDINATES.

PAVING AREA

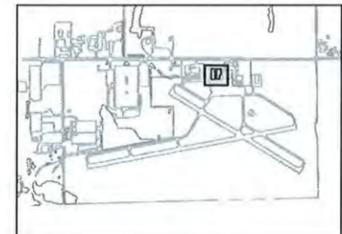


HANSON
HANSON PROFESSIONAL SERVICES INC.
815 Commerce Drive Suite 200
Oak Brook, Illinois 60523
Telephone: 630.990.3800
Fax: 630.990.3801



CITY OF DIXON, ILLINOIS
Post Office Box 386
Dixon, Illinois 61021
Telephone: 815.288.1485

DIXON MUNICIPAL AIRPORT
CHARLES R. WALGREEN FIELD
REHABILITATE T-HANGAR TAXIWAYS
AIP PROJECT NO. 3-17-0036-B12
IDA PROJECT NO. C73-3914



KEY PLAN

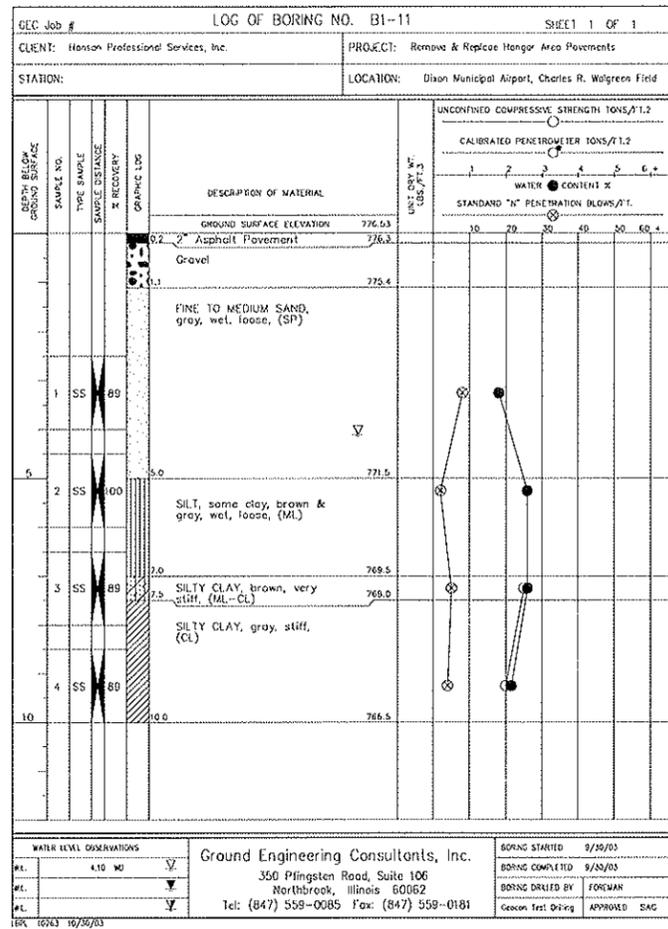
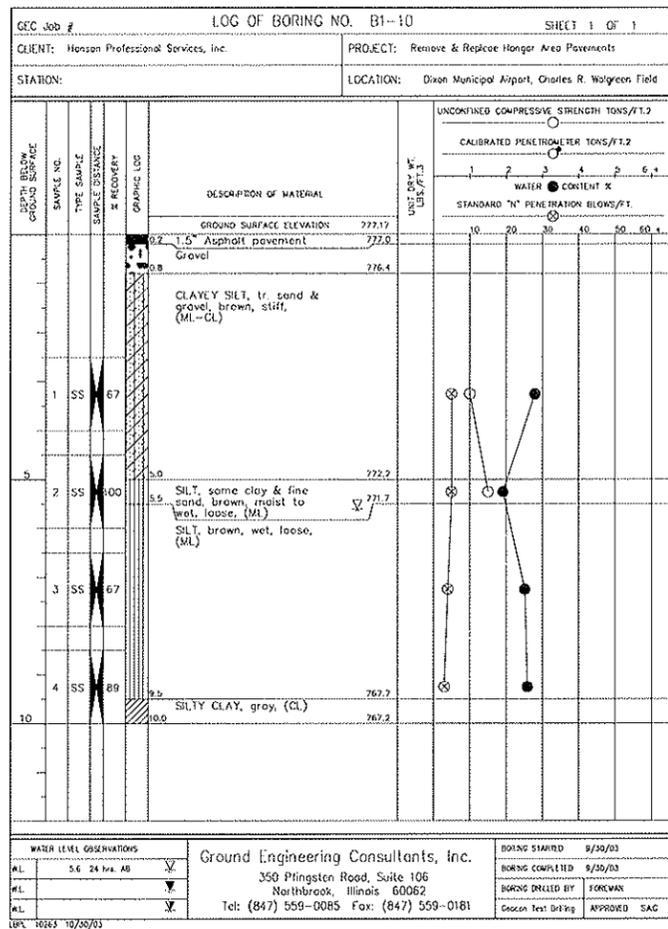
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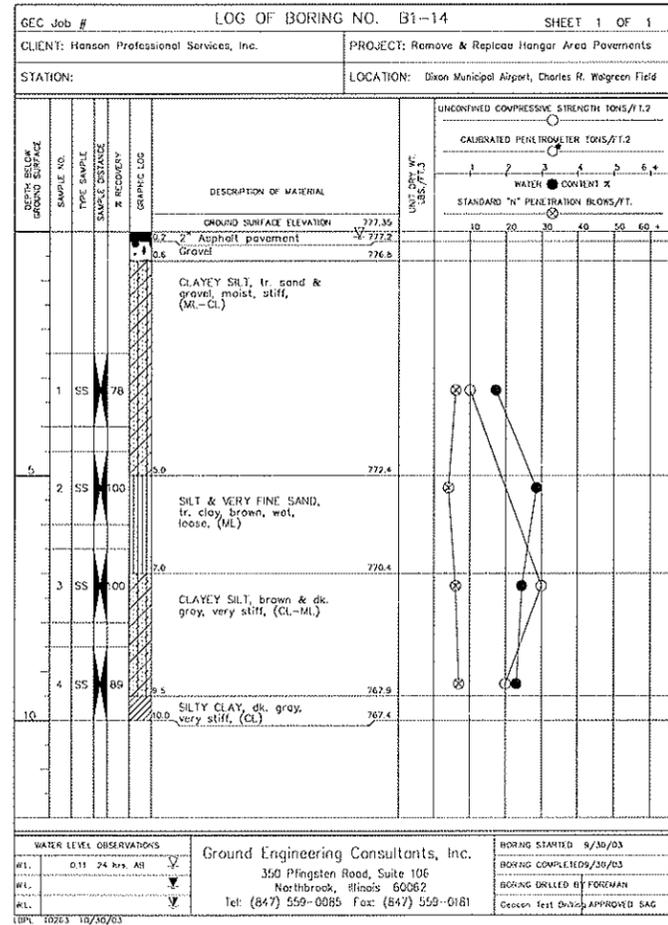
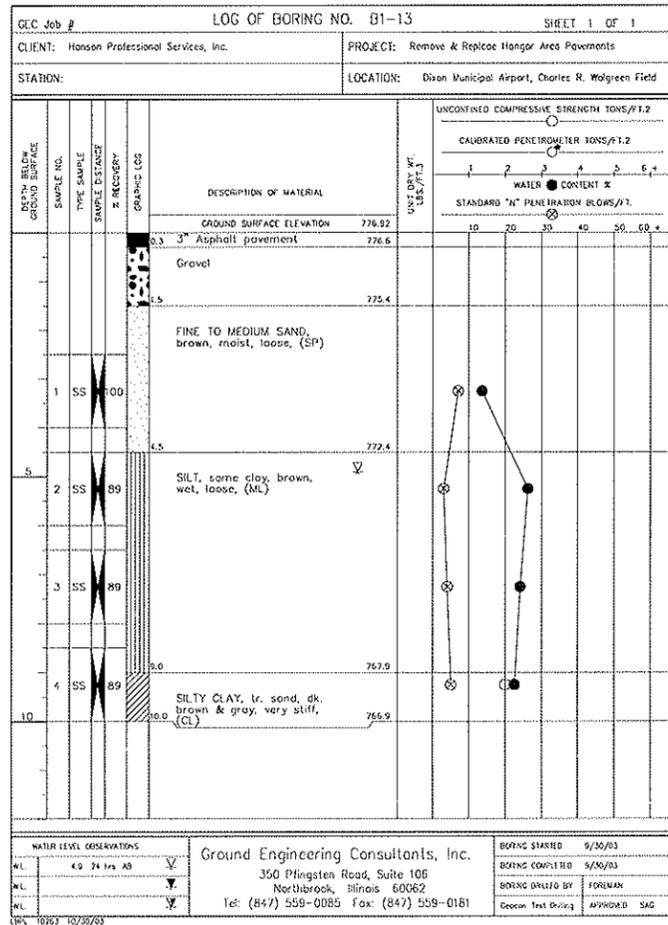
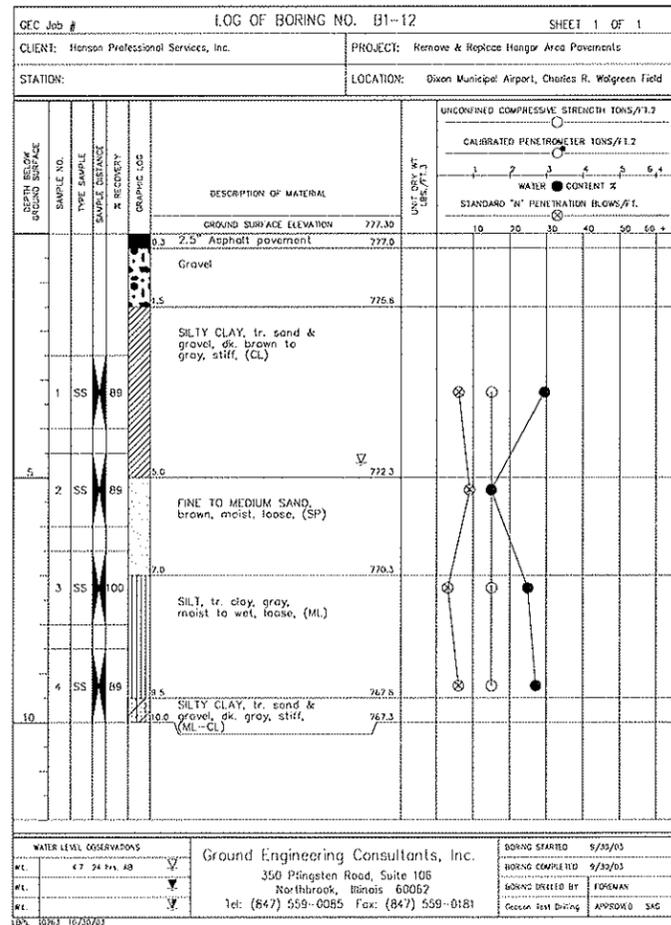
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843-04B8007
Project Number
KLK 02/03/05
Layout By Date
KLK, TMM 02/03/05
Designed By Date
RMH 06/12/09
Reviewed By Date
KLK | DSY | LDH
Drawn By Sheet No.

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SOILS INVESTIGATION PERFORMED BY:
GROUND ENGINEERING CONSULTANTS, INC.
350 PFINGSTON ROAD, SUITE 106
NORTHBROOK, ILLINOIS 60062



D1025



815 Commerce Drive Suite 200
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DIXON MUNICIPAL AIRPORT
CHARLES R. WALGREEN FIELD
REHABILITATE T-HANGAR TAXIWAYS
AIP PROJECT NO. 3-17-0036-B12
IDA PROJECT NO. C73-3914

No.	Drawing Issue Description	Date	By

Date: JUNE 19, 2009
Sheet Title:

SOIL BORING LOG
B1-10 THRU B1-14

843-0488007
Project Number
KLK 02/03/05
KLK, TMM 02/03/05
RMH 06/12/09
KLK LDH



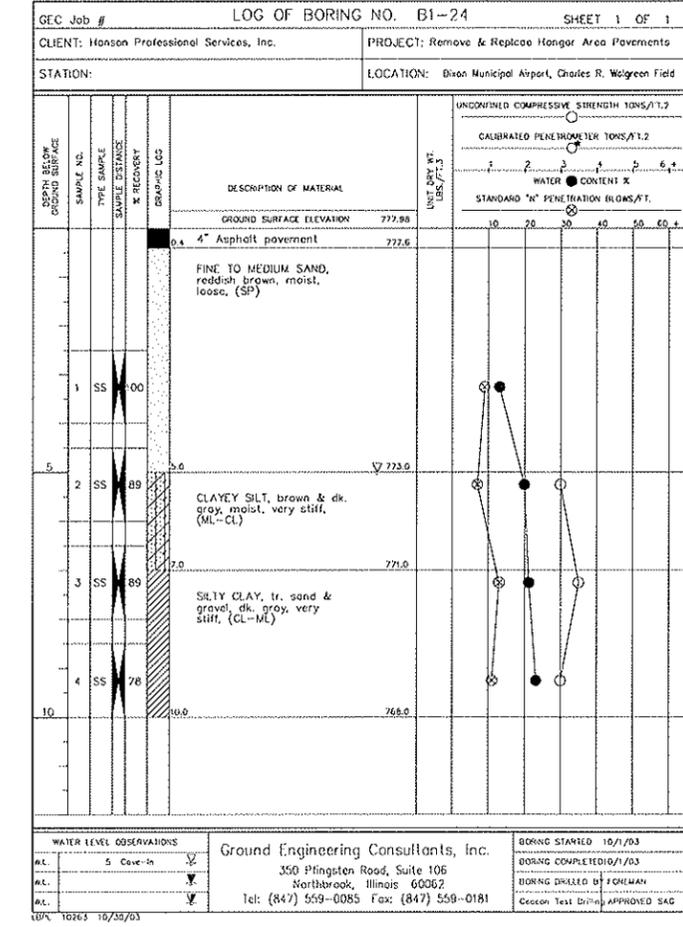
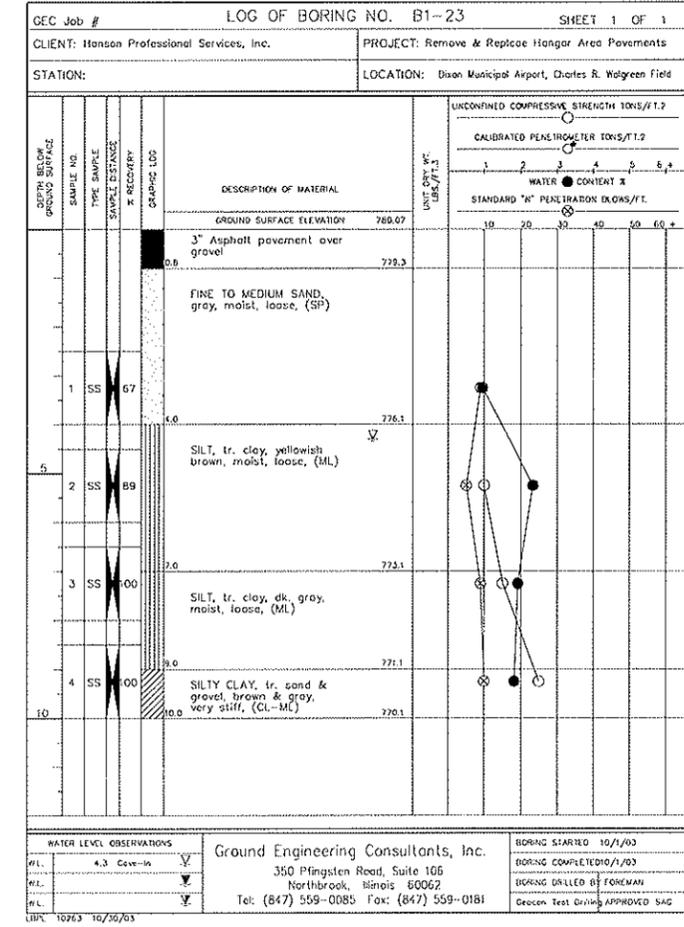
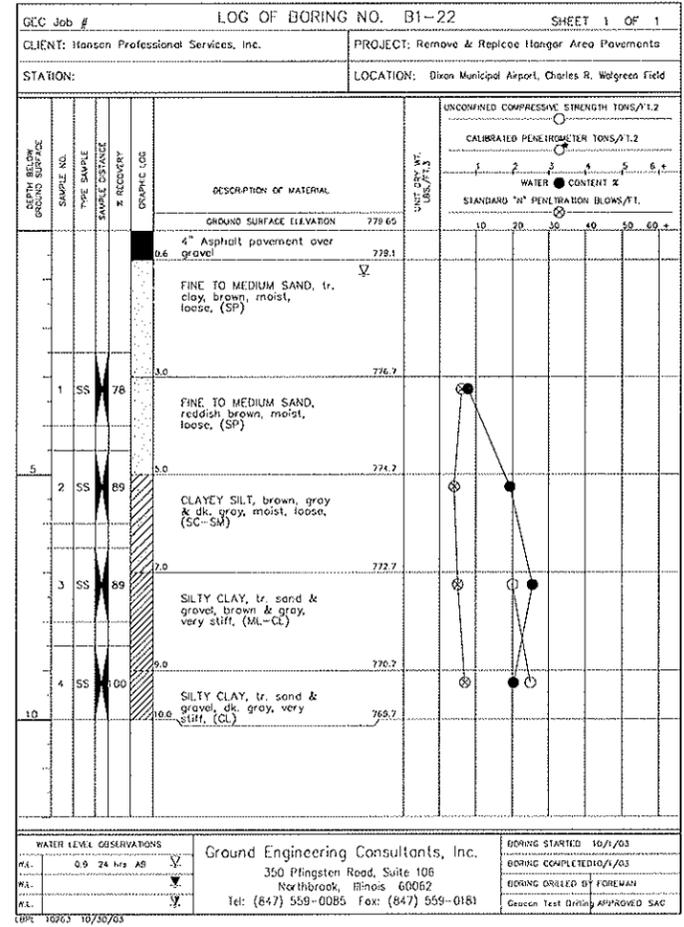
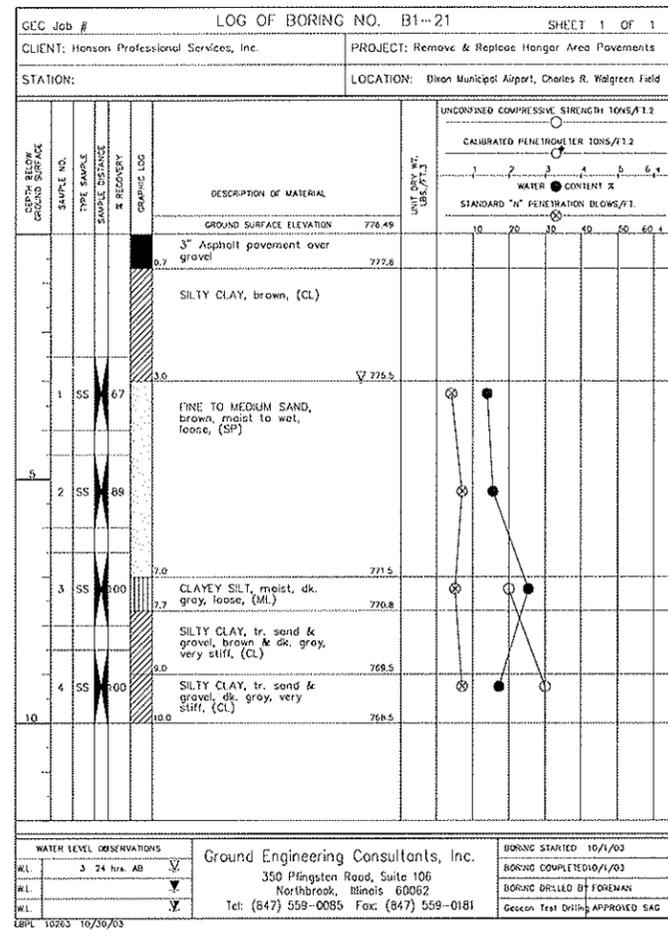
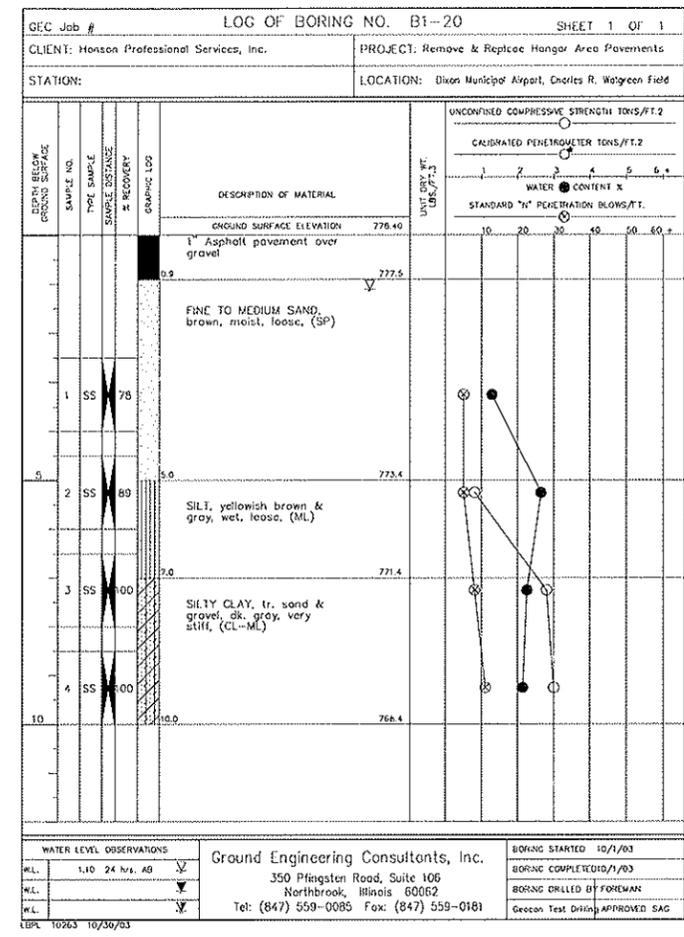
815 Commerce Drive Suite 200
Oak Brook, Illinois 60523
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Post Office Box 386
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DIXON MUNICIPAL AIRPORT
CHARLES R. WALGREEN FIELD
REHABILITATE T-HANGAR TAXIWAYS
AIP PROJECT NO. 3-17-0036-B12
IDA PROJECT NO. C73-3914

SOILS INVESTIGATION PERFORMED BY:
GROUND ENGINEERING CONSULTANTS, INC.
350 PINGSTON ROAD, SUITE 106
NORTHBROOK, ILLINOIS 60062



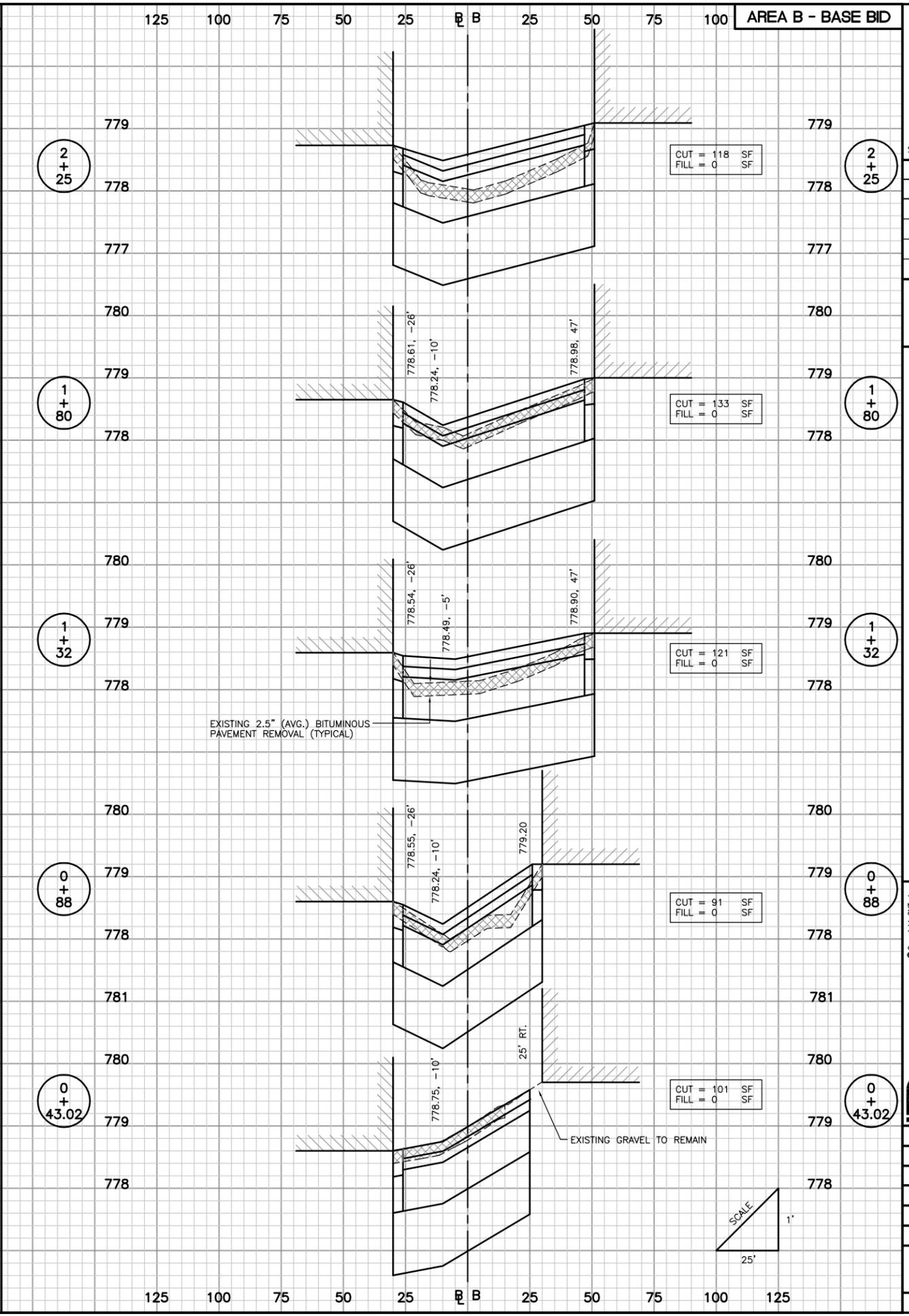
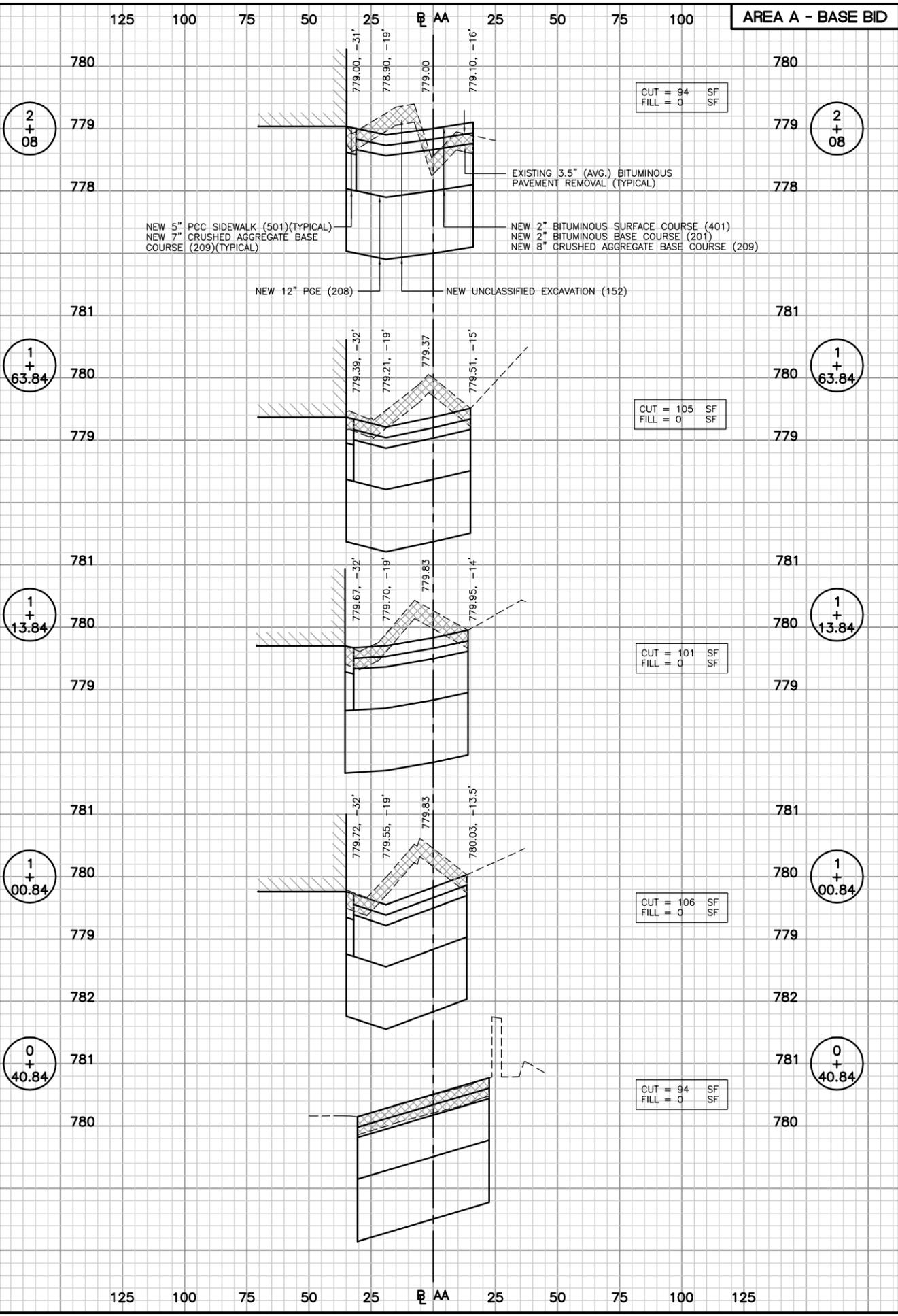
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 Project Name: REHABILITATE T-HANGAR TAXIWAYS
 Layout By: KLK Date: 02/03/05
 Designed By: KLK, TMM Date: 02/03/05
 Drawn By: RMH Date: 06/12/09
 Checked By: KLK Date: 06/12/09
 Approved By: LDH Date: 06/12/09

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 I.L. LETTING ITEM: 12A
 I.L. PROJECT: C73-3914
 A.I.P. PROJECT: 3-17-0036-B12

SURVEY BOOK # BOOK #
 REVISIONS
 NUMBER BY DATE

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 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DIXON MUNICIPAL AIRPORT
 DIXON, ILLINOIS
 REHABILITATE T-HANGAR PAVEMENTS**

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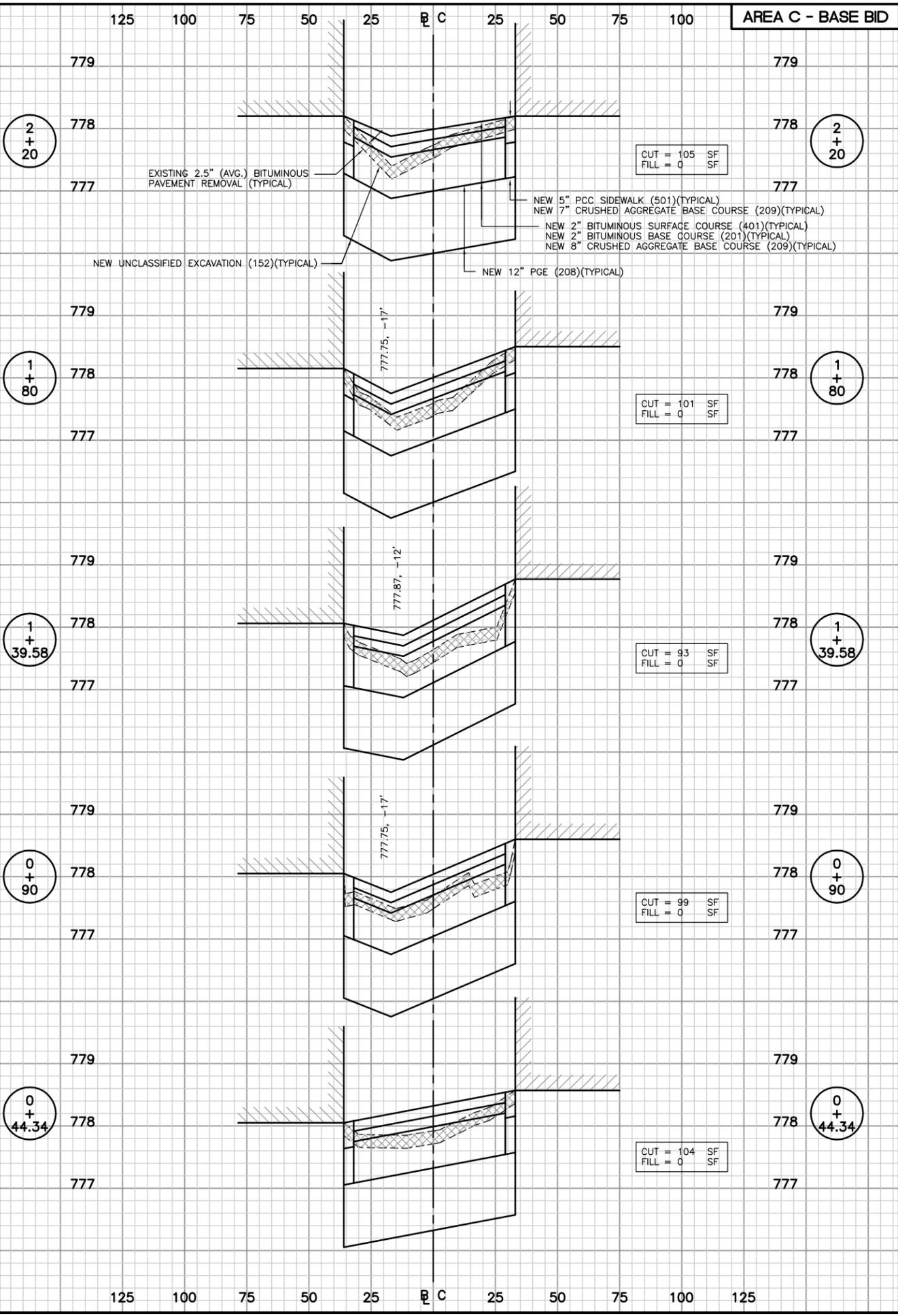
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 CHECKED BY: CAL
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 DATE: 06/19/2009
 JOB No: 09244-02

FINAL

SHEET 20 OF 22 SHEET

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FILL = 0 SF

CUT = 101 SF
FILL = 0 SF

CUT = 93 SF
FILL = 0 SF

CUT = 99 SF
FILL = 0 SF

CUT = 104 SF
FILL = 0 SF

2
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20

1
+
80

1
+
39.58

0
+
90

0
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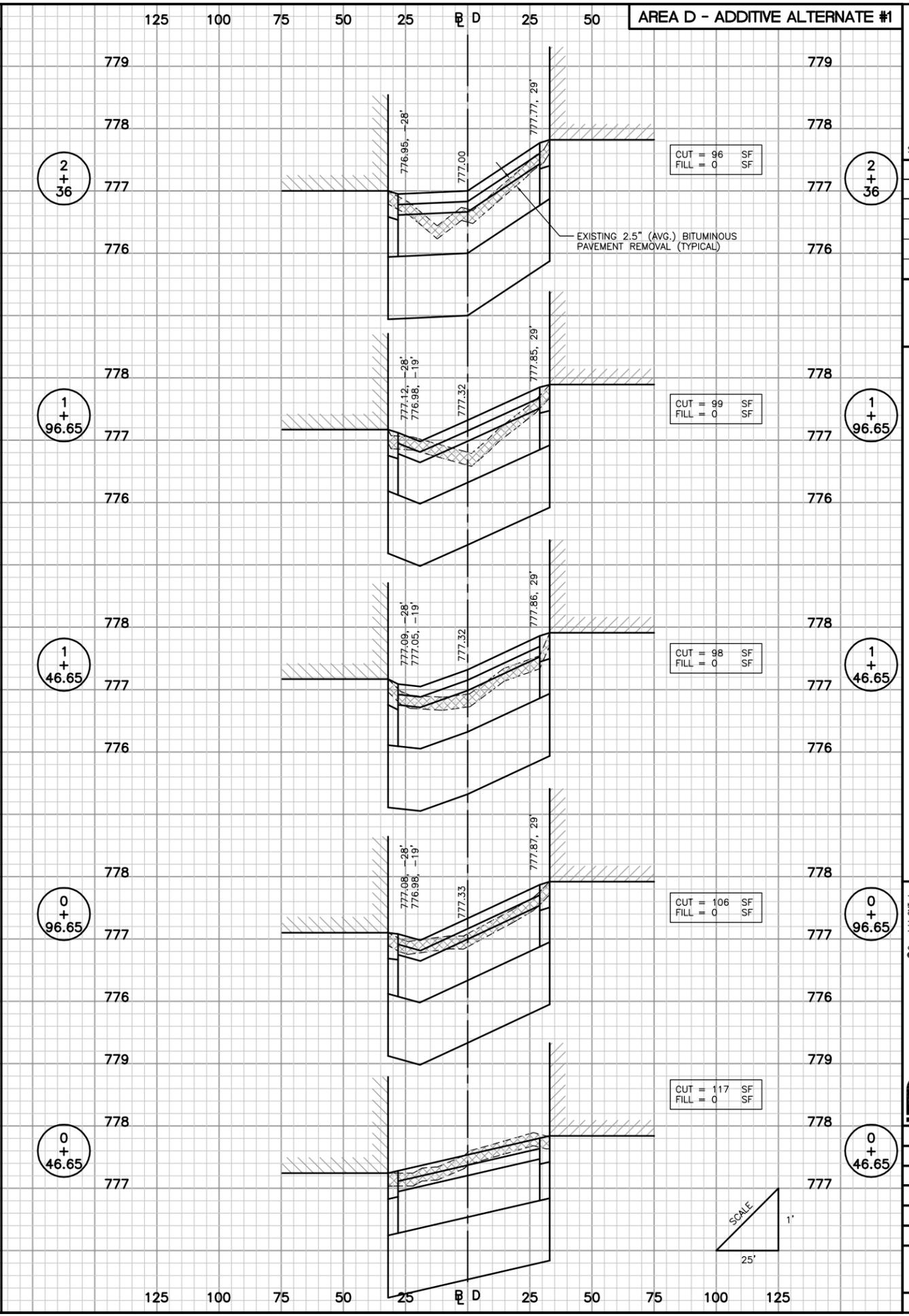
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20

1
+
80

1
+
39.58

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90

0
+
44.34



CUT = 96 SF
FILL = 0 SF

CUT = 99 SF
FILL = 0 SF

CUT = 98 SF
FILL = 0 SF

CUT = 106 SF
FILL = 0 SF

CUT = 117 SF
FILL = 0 SF

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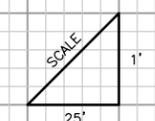
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IL CONTRACT: DIO25
IL LETTING ITEM: 12A
IL PROJECT: C73-3914
A.I.P. PROJECT: 3-17-0036-B12

SURVEY BOOK # BOOK #

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DIXON, ILLINOIS
REHABILITATE T-HANGAR PAVEMENTS
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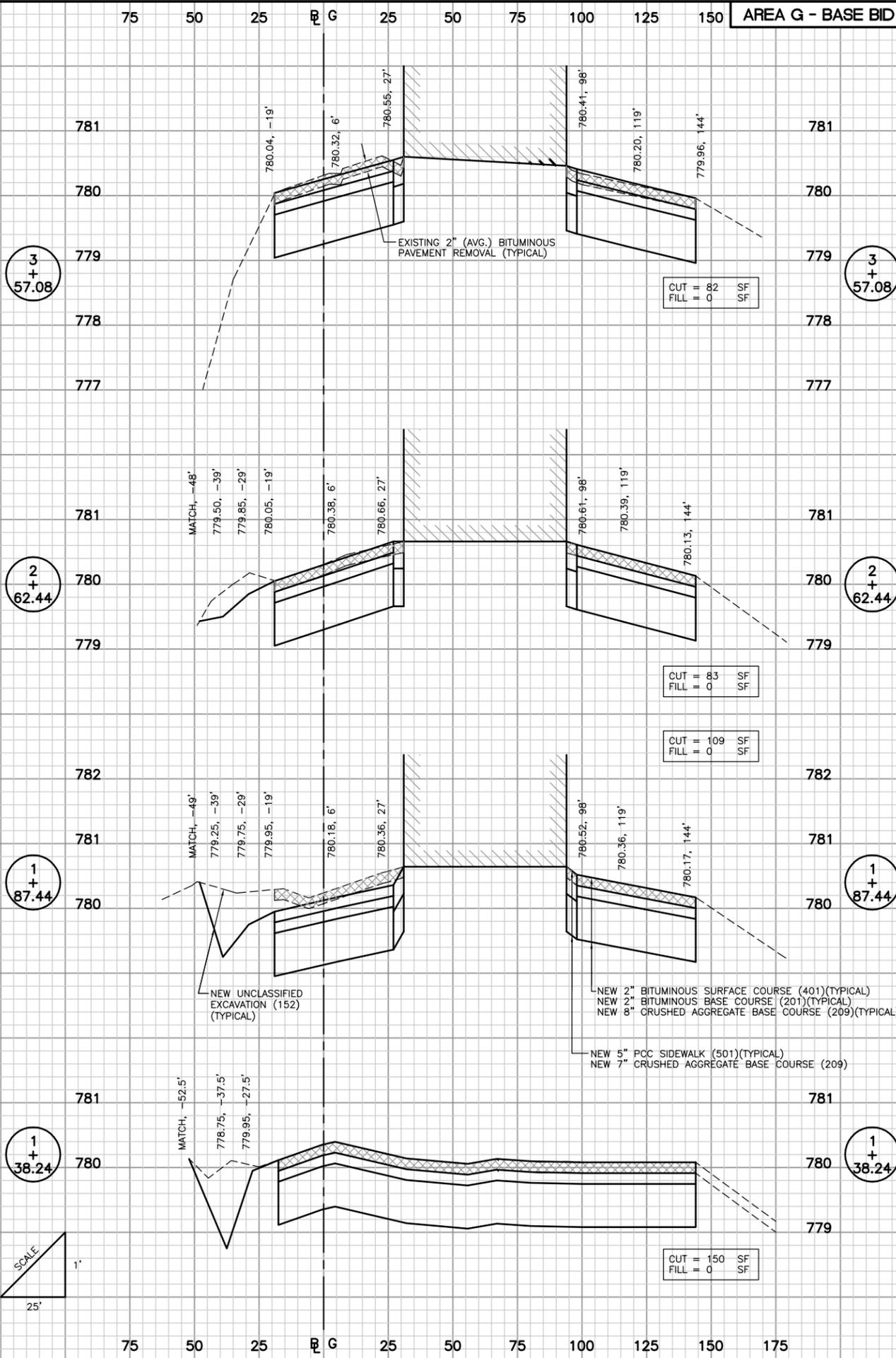
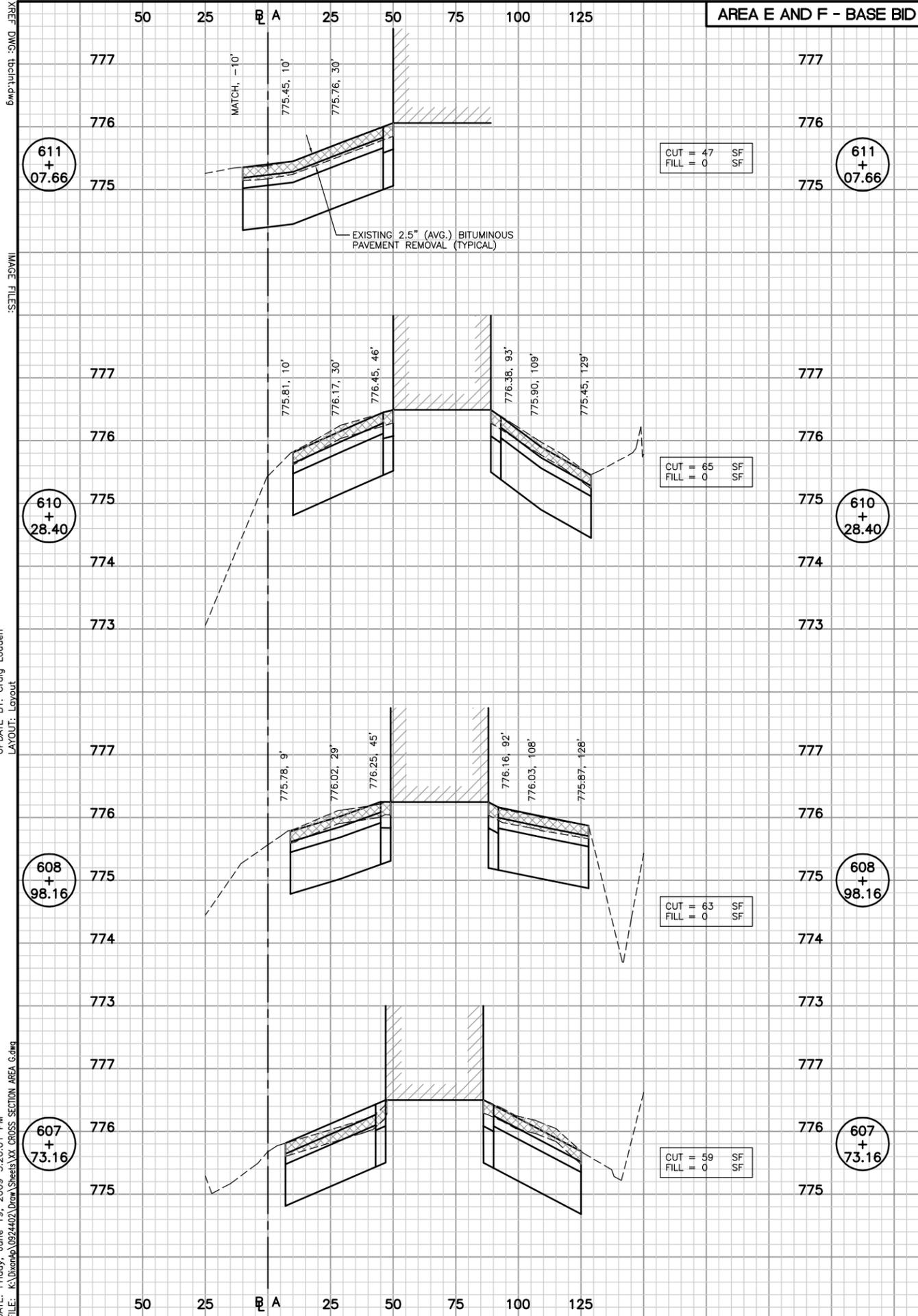
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CHECKED BY:	CAL
APPROVED BY:	DLP
DATE:	06/19/2009
JOB No:	09244-02

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SHEET 21 OF 22 SHEET

AREA E AND F - BASE BID

AREA G - BASE BID



IL CONTRACT: DIO25
IL LETTING ITEM: 12A
IL PROJECT: C73-3914
A.I.P. PROJECT: 3-17-0036-B12

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DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE T-HANGAR PAVEMENTS

CROSS SECTION - SHEET 3

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