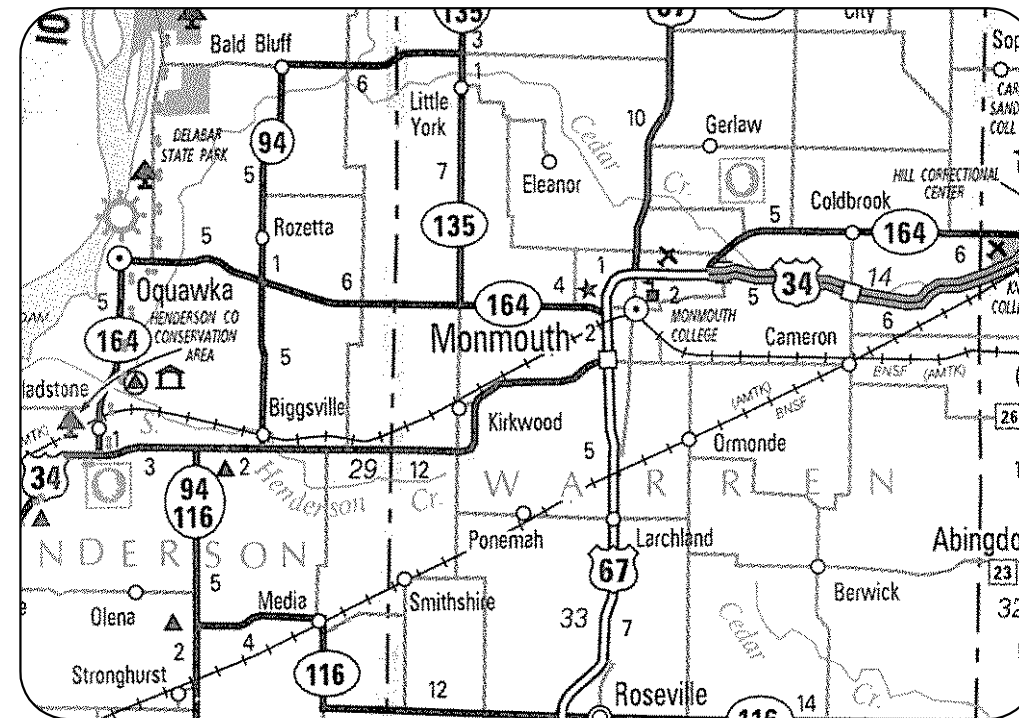


CONSTRUCTION PLANS FOR MONMOUTH MUNICIPAL AIRPORT MONMOUTH, WARREN COUNTY, ILLINOIS REMOVE AND RECONSTRUCT T-HANGAR TAXIWAYS

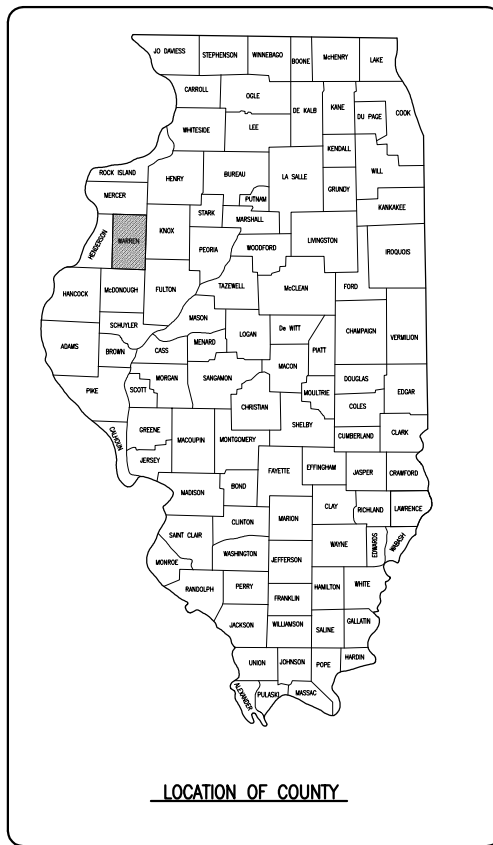
SCOPE OF WORK

THIS PROJECT CONSISTS OF THE REHABILITATION OF THE PAVED AREAS ADJACENT TO THE EXISTING HANGAR BUILDING. THE REHABILITATION CONSISTS OF THE REMOVAL AND RECONSTRUCTION OF THE EXISTING BITUMINOUS PAVEMENTS IN ACCORDANCE WITH THE CONSTRUCTION PLANS. ASSOCIATED WORK INCLUDES EARTHWORK, PAVING, MARKING, SEEDING AND INCIDENTALS.



LOCATION

ILL. PROJ.: C66-3871
A.I.P. PROJ.: 3-17-0069-B2
LATITUDE: 40° 55' 47"
LONGITUDE: 90° 37' 52"
ELEVATION: 753.0' M.S.L.
DATE: JUNE 19, 2009



LOCATION OF COUNTY

PLANS PREPARED BY:

HANSON
Hanson Professional Services Inc.
DESIGN ENGINEER

Submitted by: _____ ENG'R
Date Submitted: _____
Lic. Exp. Date: _____

CITY OF MONMOUTH, ILLINOIS

Approved: MAYOR
Date: 6/19/08

Approved: CITY MGR.
Date: 6/19/08

DATE	REVISION	BY

**MONMOUTH MUNICIPAL AIRPORT
MONMOUTH, ILLINOIS**

IL. PROJ.: C66-3871 A.I.P. PROJ.: 3-17-0069-B2

LAYOUT	MLH	MLH	RAW
	03/23/09	03/23/09	06/19/09
DRAWN	MLH	MLH	RAW
REVIEWED	RAW	RAW	RAW

HANSON
Hanson Professional Services Inc. 2009
Hanson Professional Services Inc.
1525 South Sixth Street
Springfield, Illinois 62703-2886
www.hanson-hinc.com
Offices Nationwide

REMOVE AND RECONSTRUCT
T-HANGAR TAXIWAYS

COVER SHEET

JUN 26, 2009 1:17 PM HARRI01115
F:\AIRPORTS\MONMOUTH\08A0188D\CADD\AIRPORT\SHEET\R-001.CVR.DWG

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITIES	AS BUILT QUANTITIES
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150530	TRAFFIC MAINTENANCE	L.S.	1	
AR150540	HAUL ROUTE	L.S.	1	
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	2,000	
AR152540	SOIL STABILIZATION FABRIC	S.Y.	5,650	
AR156510	SILT FENCE	L.F.	850	
AR156531	EROSION CONTROL BLANKET	S.Y.	1,100	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	2,020	
AR401610	BITUMINOUS SURFACE COURSE	TON	1,360	
AR401655	BUTT JOINT CONSTRUCTION	S.Y.	64	
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	3,580	
AR501900	REMOVE PCC PAVEMENT	S.Y.	135	
AR602510	BITUMINOUS PRIME COAT	GAL.	1,970	
AR603510	BITUMINOUS TACK COAT	GAL.	285	
AR620520	PAVEMENT MARKING-WATERBORNE	S.F.	1,209	
AR620525	PAVEMENT MARKING-BLACK BORDER	S.F.	126	
AR701218	18" CMP	L.F.	200	
AR751940	ADJUST INLET	EA.	1	
AR752218	METAL END SECTION 18"	EA.	2	
AR754610	PAVED DITCH	L.F.	590	
AR901510	SEEDING	ACRE	1	
AR908510	MULCHING	ACRE	1	

INDEX TO SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	SUMMARY OF QUANTITIES AND INDEX TO SHEETS
3	PROPOSED SAFETY PLAN
4	EXISTING SITE PLAN AND PROPOSED PAVEMENT PREPARATION PLAN
5	PROPOSED CONSTRUCTION PLAN
6	PROPOSED TYPICAL SECTIONS AND CONSTRUCTION DETAILS
7	PROPOSED PLAN AND PROFILE - WEST ALIGNMENT
8	PROPOSED PLAN AND PROFILE - EAST ALIGNMENT
9	PROPOSED PAVEMENT MARKING PLAN
10	PROPOSED STORMWATER POLLUTION PREVENTION PLAN
11	PROPOSED CROSS SECTIONS - WEST ALIGNMENT STA. 10+00 TO STA. 12+50
12	PROPOSED CROSS SECTIONS - WEST ALIGNMENT STA. 13+00 TO STA. 15+50
13	PROPOSED CROSS SECTIONS - WEST ALIGNMENT STA. 16+00 TO STA. 16+51
14	PROPOSED CROSS SECTIONS - EAST ALIGNMENT STA. 10+00 TO STA. 13+90

DATE	REVISION	BY

**MONMOUTH MUNICIPAL AIRPORT
MONMOUTH, ILLINOIS**

IL. PROJ.: C66-3871 A.I.P. PROJ.: 3-17-0069-B2

HEL Project No. 08A0188D_0800	LAYOUT	MLH	02/24/09
Filename R-002FLP.DWG	DRAWN	MLH	02/24/09
Scale N/A	REVIEWED	RAW	06/19/09
Date 06/19/09			

HANSON
 Hanson Professional Services Inc. 2009
 1525 South Skrin Street
 Springfield, Illinois 62703-2886
 www.hanson-hinc.com
 Offices Nationwide

**REMOVE AND RECONSTRUCT
T-HANGAR TAXIWAYS**

SUMMARY OF QUANTITIES
AND
INDEX TO SHEETS

SCOPE OF WORK

THIS PROJECT CONSISTS OF THE REHABILITATION OF THE PAVED AREAS ADJACENT TO THE EXISTING HANGAR BUILDING. THE REHABILITATION CONSISTS OF THE REMOVAL AND RECONSTRUCTION OF THE EXISTING BITUMINOUS PAVEMENTS IN ACCORDANCE WITH THE CONSTRUCTION PLANS. ASSOCIATED WORK INCLUDES EARTHWORK, PAVING, MARKING, SEEDING AND INCIDENTALS.

PROPOSED SAFETY PLAN

ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR 150/5370-2, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE AIRPORT MANAGEMENT AT THE PRECONSTRUCTION CONFERENCE OR DURING THE COURSE OF THE CONTRACT.

GENERAL - THE MONMOUTH MUNICIPAL AIRPORT IS COMPRISED OF ONE PAVED RUNWAY. THE PROPOSED CONSTRUCTION WILL NECESSITATE CLOSING RUNWAY 2-20 ANY TIME THE CONTRACTOR IS WORKING WITHIN 200' OF THE RUNWAY CENTERLINE. THE RUNWAY WILL BE CLOSED ONLY DURING THE CONSTRUCTION DAY. AT THE END OF EACH CONSTRUCTION DAY THE CONTRACTOR WILL SMOOTH GRADE ALL AREAS WITHIN THE SAFETY AREA TO THE SATISFACTION OF THE RESIDENT ENGINEER AND RE-OPEN THE RUNWAY. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE CONTRACTOR WILL BE REQUIRED TO SUBMIT TO THE RESIDENT ENGINEER, FOR APPROVAL BY THE AIRPORT MANAGEMENT, A SCHEDULE OF CONSTRUCTION ACTIVITIES, TO INCLUDE A SCHEDULE OF RUNWAY AND TAXIWAY CLOSURES PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR IS REQUIRED TO ALLOW SUFFICIENT TIME PRIOR TO THE ENDING OF THE CLOSURE TIME FOR CURING OF PLACED MATERIALS AND CLEAN UP OF THE WORK AREA.

THE CONTRACTOR IS REQUIRED TO NOTIFY THE AIRPORT MANAGER, THROUGH THE RESIDENT ENGINEER, A MINIMUM OF 7 DAYS PRIOR TO THE START OF CONSTRUCTION. THIS WILL ALLOW THE AIRPORT MANAGER TO ISSUE ALL NECESSARY NOTAMS REGARDING THE CONSTRUCTION ACTIVITIES OCCURRING AROUND THE AIRFIELD. THE CONTRACTOR IS REQUIRED TO PROVIDE A MINIMUM OF 24 HOUR NOTICE TO THE AIRPORT DIRECTOR PRIOR TO CLOSING A RUNWAY.

THE CONTRACTOR IS REQUIRED TO IMPLEMENT A PRACTICAL AND EFFECTIVE STAGING PLAN THAT WILL MINIMIZE DISRUPTION TO NORMAL AIRPORT ACTIVITY WHILE NOT COMPROMISING SAFETY OF PERSONNEL OR THE QUALITY OF THE PROJECT.

THE AIRPORT MANAGEMENT, OR DESIGNATED REPRESENTATIVE, WILL ISSUE ALL NOTICES TO AIRMAN (NOTAM) RELATED TO OPENING AND CLOSING PAVEMENTS THROUGHOUT THE PROJECT.

BARRICADES AND TRAFFIC CONES

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND TRAFFIC CONES AS DIRECTED BY THE AIRPORT MANAGER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING OR RED STEADY-BURN LIGHTS AND 20" SQUARE ORANGE FLAGS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED PART OF ITEM AR150530, TRAFFIC MAINTENANCE, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

HEIGHT OF CONSTRUCTION EQUIPMENT

THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 20 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A TANDEM TRUCK IN THE UPRIGHT POSITION. DUE TO RESTRICTIVE TURNING MOVEMENT AREA, ONLY TANDEM TRUCKS SHALL BE UTILIZED.

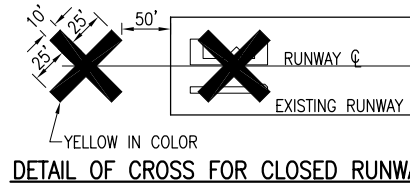
HAUL ROUTE AND EQUIPMENT PARKING/MATERIAL STORAGE AREA

THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND EQUIPMENT PARKING/MATERIAL STORAGE AREA AS SHOWN ON THIS SHEET. THE PROPOSED PARKING/MATERIAL STORAGE AREA WILL BE NO GREATER THAN 125' X 100'. THE EQUIPMENT PARKING/MATERIAL STORAGE AREA SHALL BE NO CLOSER THAN 220' FROM THE RUNWAY CENTERLINE, AND NO EQUIPMENT GREATER THAN 14' IN HEIGHT SHALL REMAIN IN THIS AREA. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND STAGING AREA THROUGHOUT THE COURSE OF THE PROJECT. ANY AREAS DAMAGED OUTSIDE OF THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AND AT THE CONTRACTOR'S OWN EXPENSE. AT THE CONCLUSION OF THE PROJECT THE CONTRACTOR WILL RESTORE THE HAUL ROUTE AND STAGING AREA TO ITS PRE-CONSTRUCTION STATE. RESTORATION OF THE HAUL ROUTE AND STAGING AREA WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

CONTROL POINT DATA				
NO.	DESCRIPTION	NORTHING	EASTING	ELEVATION
1	NGS MONUMENT - MONPORT AZ MK	1551521.9430	2167894.7780	752.06
2	CP# 3 - 5/8" IRON PIN	1552261.5030	2168172.3390	N/A
3	CP# 4 - 5/8" IRON PIN	1551738.6910	2168281.5670	N/A
4	CHISELED □ ON NW CORNER OF CONC. PAD	---	---	751.35
5	CHISELED X ON CENTER CAP BOLT OF HYDRANT	---	---	750.51

CRITICAL POINT

LATITUDE - 40° 55' 40.78"
LONGITUDE - 90° 37' 55.10"
ELEVATION - 750.9'



TEMPORARY RUNWAY CLOSURE NOTE

FAA CRITERIA REQUIRES A RUNWAY BE CLOSED IF CONSTRUCTION ACTIVITIES OR PERSONNEL ARE WITHIN 200 FT OF A RUNWAY CENTERLINE.

COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. ALL RUNWAY CLOSURES WILL BE IN ACCORDANCE WITH ALL FAA ADVISORY CIRCULARS THAT RELATE TO RUNWAY CLOSURES DURING CONSTRUCTION OPERATIONS. THE RUNWAY CLOSURE PROCEDURES SHALL BE REVIEWED BY THE AIRPORT MANAGER AND COORDINATED WITH THE RESIDENT ENGINEER. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL, AS APPROVED BY THE RESIDENT ENGINEER AND REVIEWED BY THE AIRPORT MANAGER. THE CROSSES WILL BE PLACED OVER THE NUMERALS OR OFF EACH END OF THE RUNWAY AND SECURED IN A MANNER APPROVED BY THE AIRPORT MANAGER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

RUNWAY CLOSURE PROCEDURES:

- CONTACT THE AIRPORT MANAGEMENT OR ASSIGNED REPRESENTATIVE.
- ISSUANCE OF NOTAM BY THE AIRPORT MANAGEMENT OR ASSIGNED REPRESENTATIVE.
- PLACEMENT OF CROSSES (SEE DETAIL THIS SHEET).
- PLACEMENT OF LIGHTED BARRICADES AND CONES.

ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS WITHIN 200 FT. OF THE AFFECTED RUNWAY CENTERLINE BEGIN.

RUNWAY RE-OPENING PROCEDURES:

- REMOVE CROSSES.
- REMOVE LIGHTED BARRICADES AND CONES.
- NOTIFY THE AIRPORT MANAGEMENT OR REPRESENTATIVE TO CANCEL THE NOTAM.
- CANCELLATION OF THE NOTAM.

A CLOSED RUNWAY WILL NOT BE RE-OPENED UNTIL ALL EQUIPMENT AND WORK ARE FURTHER THAN 200 FT. FROM THE AFFECTED RUNWAY CENTERLINE.

150-ENGINEER'S FIELD OFFICE

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE FURNISHED, MAINTAINED, AND REMOVED IN ACCORDANCE WITH ITEM AR150510 "ENGINEER'S FIELD OFFICE" OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.

THE LOCATION OF THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.

THE ENGINEERING FIRM WILL MAKE PAYMENT FOR ALL LONG DISTANCE TELEPHONE CALLS IN EXCESS OF ONE HUNDRED DOLLARS (\$100.00) PER MONTH.

THE CONTRACTOR WILL FURNISH A CELL PHONE TO THE RESIDENT ENGINEER FOR HIS EXCLUSIVE USE FOR THE DURATION OF THIS PROJECT. THE RESIDENT ENGINEER WILL USE THIS PHONE FOR PROJECT BUSINESS ONLY. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL CHARGES ASSOCIATED WITH THIS CELL PHONE.

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE PAID FOR UNDER ITEMS:
AR150510 ENGINEER'S FIELD OFFICE ____ 1 L.S.

CONTRACTOR RESPONSIBILITIES

THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.

THE CONTRACTOR IS REQUIRED TO LIMIT THE USE OF CONSTRUCTION EQUIPMENT ON THE EXISTING PAVEMENTS. ONLY EQUIPMENT NEEDED TO COMPLETE THE SPECIFIC WORK ON THE EXISTING PAVEMENT WILL BE PERMITTED. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ANY DAMAGE TO EXISTING PAVEMENTS CAUSED BY HIS PERSONNEL OR EQUIPMENT.

NO OPEN HOLES OR TRENCHES WILL BE ALLOWED WITHIN 200' OF AN ACTIVE RUNWAY, WITHIN 40' OF AN ACTIVE TAXIWAY OR TAXILANE, NOR WILL EITHER HOLES OR OPEN TRENCHES BE ALLOWED TO REMAIN EXPOSED OR OPEN OVERNIGHT.

IDENTIFICATION - WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CONTRACTOR WILL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.

RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (122.80 MHZ.) WITH THE AIRPORT UNICOM WHENEVER HIS PERSONNEL ARE ON THE AIRPORT PROPERTY. THIS WILL ALLOW THE CONTRACTOR TO RESPOND TO AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

AIRPORT SECURITY NOTE

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL CLOSE AND LOCK THE EXISTING GATE IN THE HAUL ROUTE AT THE END OF EACH WORKING DAY. ANY ACCESS GATES WILL BE CLOSED AT ALL TIMES WHEN NOT IN USE. IF CONTINUOUS HAULING IS REQUIRED, THEN THE GATE ACCESS WILL BE MONITORED TO PREVENT A BREACH FROM OUTSIDE, NON-CONSTRUCTION RELATED TRAFFIC.

AIRCRAFT OPERATION LINE

THE AIRCRAFT OPERATION LINE PARALLELS THE RUNWAY AT A DISTANCE OF 200' FROM THE RUNWAY'S CENTERLINE. THE CONTRACTOR WILL LOCATE THIS LINE AT THE START OF CONSTRUCTION AND WILL PLACE A FRANGIBLE MARKER EVERY 150' ALONG IT. THIS LINE WILL BE THE LIMITS THAT ALL CONTRACTOR PERSONNEL MAY VENTURE WHEN A RUNWAY IS NOT CLOSED. THE CONTRACTOR WILL MAINTAIN THE LINE FOR RUNWAYS AND REMOVE THE MARKERS AT THE CONCLUSION OF THE PROJECT.

EROSION CONTROL

THIS PROJECT WILL DISTURB MORE THAN 1 ACRE OF LAND, THEREFORE A STORMWATER POLLUTION PREVENTION PLAN (SWPPP) IS REQUIRED AND INCLUDED IN THESE PLANS.

UTILITY NOTE

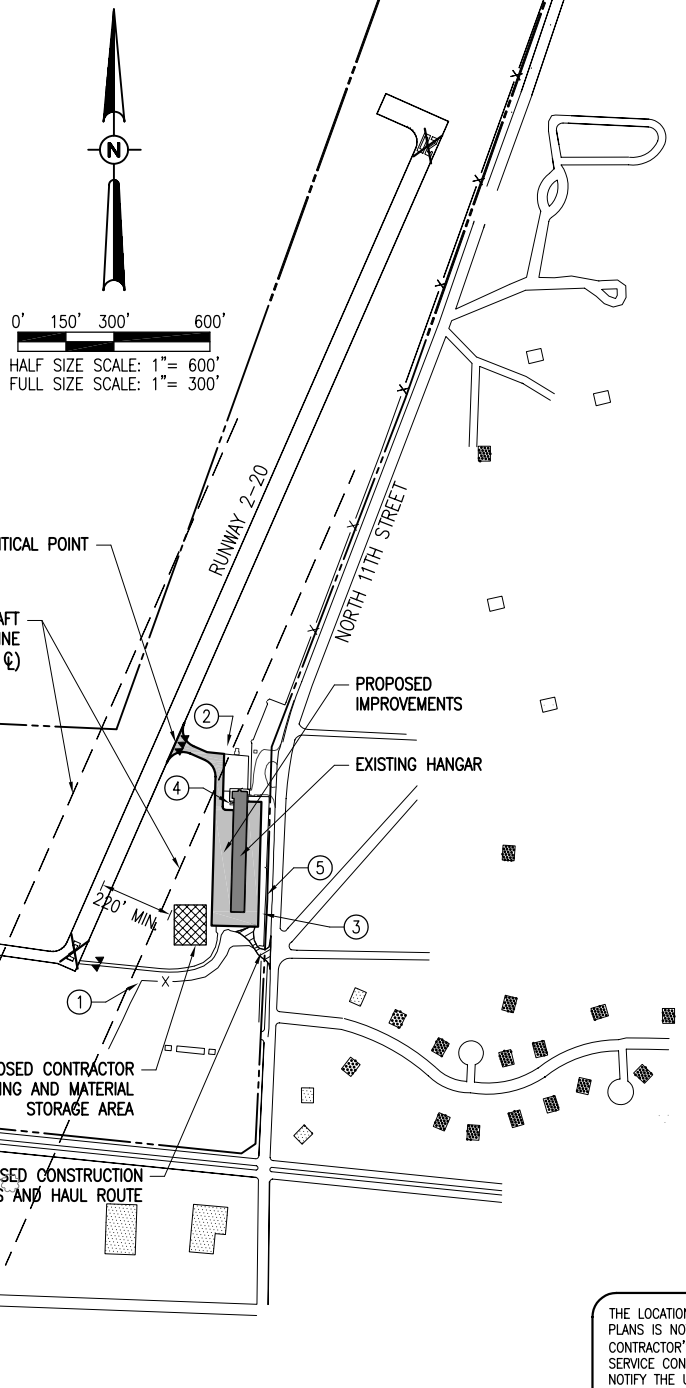
THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES AND AGENCIES WHICH HAVE LINES OR CONDUITS IN THE PROPOSED WORK AREA. ALL LINES AND CONDUITS SHALL BE LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. THE CONTRACTOR WILL CALL J.U.L.I.E. (1-800-892-0123) TO ACCOMPLISH THE ABOVE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO IDENTIFY ALL UNDERGROUND NON-JULIE UTILITIES LOCATED WITHIN THE PROPOSED CONSTRUCTION LIMITS. THESE UNDERGROUND IMPROVEMENTS WILL BE LOCATED AT THE CONTRACTOR'S OWN EXPENSE PRIOR TO THE START OF CONSTRUCTION ACTIVITIES.

J.U.L.I.E. INFORMATION

COUNTY WARREN
CITY MONMOUTH
TOWNSHIP MONMOUTH
SECTION NO. 20
ADDRESS MONMOUTH MUNICIPAL AIRPORT
1300 N. 11TH STREET
MONMOUTH, ILLINOIS 61462

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123.



LEGEND

	EXISTING IMPROVEMENTS
	EXISTING AIRPORT BUILDINGS
	EXISTING BUILDINGS
	PROPOSED IMPROVEMENTS
	PROPOSED HAUL ROUTE
	PROPOSED EQUIPMENT PARKING/MATERIAL STORAGE AREA
①	CONTROL POINT
	PROPOSED BARRICADES
-x-	EXISTING FENCE
- - -	AIRPORT PROPERTY LINE

JUN 26, 2009 9:53 AM HARRI01115
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DATE	REVISION	BY

**MONMOUTH MUNICIPAL AIRPORT
MONMOUTH, ILLINOIS**
A.I.P. PROJ.: 3-17-0069-B2
IL PROJ.: 666-3871

HEL Project No. 08A0188D_0800	04/18/09
Filename R-003SFY.DWG	04/21/09
Scale 1" = 300'	06/19/09
Date	RAW
LAYOUT	MLH
DRAWN	MLH
REVIEWED	RAW

HANSON
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**REMOVE AND RECONSTRUCT
T-HANGAR TAXIWAYS**
PROPOSED SAFETY PLAN

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. AT 1-800-892-0123 FOR UTILITY INFORMATION.

220' MIN. FROM RWY Q

- EXISTING ELECTRIC CABLES INCLUDING, BUT NOT LIMITED TO:
 - ROTATING BEACON
 - WIND TEE
 - APRON LIGHTING
 - RUNWAY SERIES CIRCUIT
 - TAXIWAY SERIES CIRCUIT
 - NORTH VASI (ABANDONED)
 - SOUTH VASI (ABANDONED)
 - COUNTERPOISE
- FIELD LOCATE PRIOR TO ANY EXCAVATION

RUNWAY 2-20 (2900' X 60')

EXISTING LIGHT BASE TO REMAIN

EQUIPMENT PARKING AND MATERIAL STORAGE AREA (14' HEIGHT LIMIT)

EXISTING TAXIWAY (UNIMPROVED SURFACE)

EXISTING BITUMINOUS PAVEMENT TO BE REMOVED (3,580 S.Y.)

EXISTING CONCRETE PAVEMENT TO BE REMOVED (135 S.Y.)

EXISTING 6-WAY ELECTRICAL DUCT

EXISTING ELECTRICAL VAULT

EXISTING BITUMINOUS PAVEMENT TO BE REMOVED

PROPOSED MILLING FOR BUTT JOINT CONSTRUCTION (64 S.Y.)

EXISTING BITUMINOUS APRON

EXISTING ADMINISTRATION OFFICE

EXISTING FUEL TANK AND PAD

EXISTING HANGAR

EXISTING SWING GATE

EXISTING GRAVEL DRIVE (HAUL ROUTE)

EXISTING DRAINAGE INLET TO REMAIN

11TH STREET

DATE	
REVISION	
BY	

MONMOUTH MUNICIPAL AIRPORT
MONMOUTH, ILLINOIS

IL. PROJ.: C66-3871 A.I.P. PROJ.: 3-17-0069-B2

HEL Project No. 08A0188D_0800
Filename R-1111ST.DWG
Scale 1"=30'
Date 06/19/09
04/21/09
MLH
04/21/09
MLH
06/19/09
RAW

HANSON
Professional Services Inc. 2009
1525 South Sixth Street
Springfield, Illinois 62703-2886
www.hanson-hinc.com
Offices Nationwide

REMOVE BITUMINOUS PAVEMENT (AR401900)

THE CONTRACTOR WILL REMOVE THE EXISTING BITUMINOUS MATERIAL SHOWN TO BE REMOVED TO THE EXISTING EARTH BASE. ALL REMOVED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE.

WHERE THE PROPOSED REMOVAL AREA ABUTS THE EXISTING PAVEMENT, THE PAVEMENT WILL BE SAWED. THE SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ANY ADJACENT PAVEMENT DAMAGED BY THE PAVEMENT REMOVAL OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM AR401900 REMOVE BITUMINOUS PAVEMENT, PER SQUARE YARD.

REMOVE CONCRETE PAVEMENT (AR501900)

THE CONTRACTOR SHALL REMOVE THE EXISTING CONCRETE PAVEMENT SHOWN TO BE REMOVED TO THE EXISTING EARTH BASE. ALL REMOVED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE.

WHERE THE PROPOSED REMOVAL AREA ABUTS THE EXISTING PAVEMENT, THE PAVEMENT WILL BE SAWED. THE SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CONTRACTOR SHALL TAKE NECESSARY PRECAUTIONS TO AVOID DAMAGE TO THE EXISTING DRAINAGE INLET AND BUILDING DURING CONCRETE REMOVAL.

ANY ADJACENT PAVEMENT DAMAGED BY THE PAVEMENT REMOVAL OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

ALL WORK NECESSARY TO COMPLETE THE REMOVAL, INCLUDING SAWCUTTING AND DISPOSAL OF THE CONCRETE SHALL BE INCLUDED IN THE UNIT PRICE OF THE ITEM.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM AR501900 REMOVE CONCRETE PAVEMENT, PER SQUARE YARD.

BITUMINOUS PAVEMENT MILLING

THE AREAS SHOWN ON THIS SHEET TO BE MILLED WILL BE CUT OR TRIMMED TO A DEPTH OF 1-1/2 INCHES FOR THE PURPOSE OF CONSTRUCTING A BITUMINOUS BUTT JOINT FOR THE BITUMINOUS SURFACE COURSE. THE CUTTING OR TRIMMING WILL BE DONE BY ROTO-MILLING. THE TOLERANCE OF THE MILLING WILL BE AS STATED IN THE STANDARD SPECIFICATIONS.

ALL MILLED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE. ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

IF A TRUE VERTICAL FACE IS NOT ESTABLISHED ALONG THE BUTT JOINT FROM THE MILLING OPERATIONS, THE CONTRACTOR IS REQUIRED TO SAWCUT THE JOINT. THE SAWING, IF NECESSARY, SHALL BE CONSIDERED INCIDENTAL TO THE BUTT JOINT CONSTRUCTION AND NO ADDITIONAL COMPENSATION ALLOWED.

ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE CONTRACTOR AND APPROVED BY THE RESIDENT ENGINEER.

THE PAVEMENT MILLING WILL BE PAID FOR UNDER ITEM AR401655 BUTT JOINT CONSTRUCTION, PER SQUARE YARD.

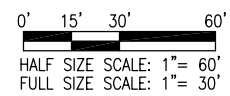
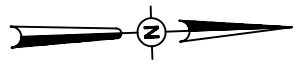
ELECTRICAL NOTES

AIRFIELD CABLES SHALL REMAIN IN PLACE BUT PROTECTED AGAINST DAMAGE DURING CONSTRUCTION. BASED ON RECORD DRAWINGS, IT IS UNCLEAR WHETHER CABLES ARE DIRECT BURIED OR IN DUCT. CAUTION SHALL BE EXERCISED WHEN EXCAVATING OR GRADING IS UNDERTAKEN. USE INSULATED HAND TOOLS ONLY FOR EXCAVATION IN THIS AREA.

ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGEMENT. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY AND HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).

LEGEND

[Symbol]	EXISTING BUILDING
[Symbol]	EXISTING PAVEMENT
[Symbol]	PROPOSED BITUMINOUS PAVEMENT REMOVAL
[Symbol]	PROPOSED CONCRETE PAVEMENT REMOVAL
[Symbol]	PROPOSED MILLING FOR BUTT JOINT CONSTRUCTION
[Symbol]	EXISTING FENCE

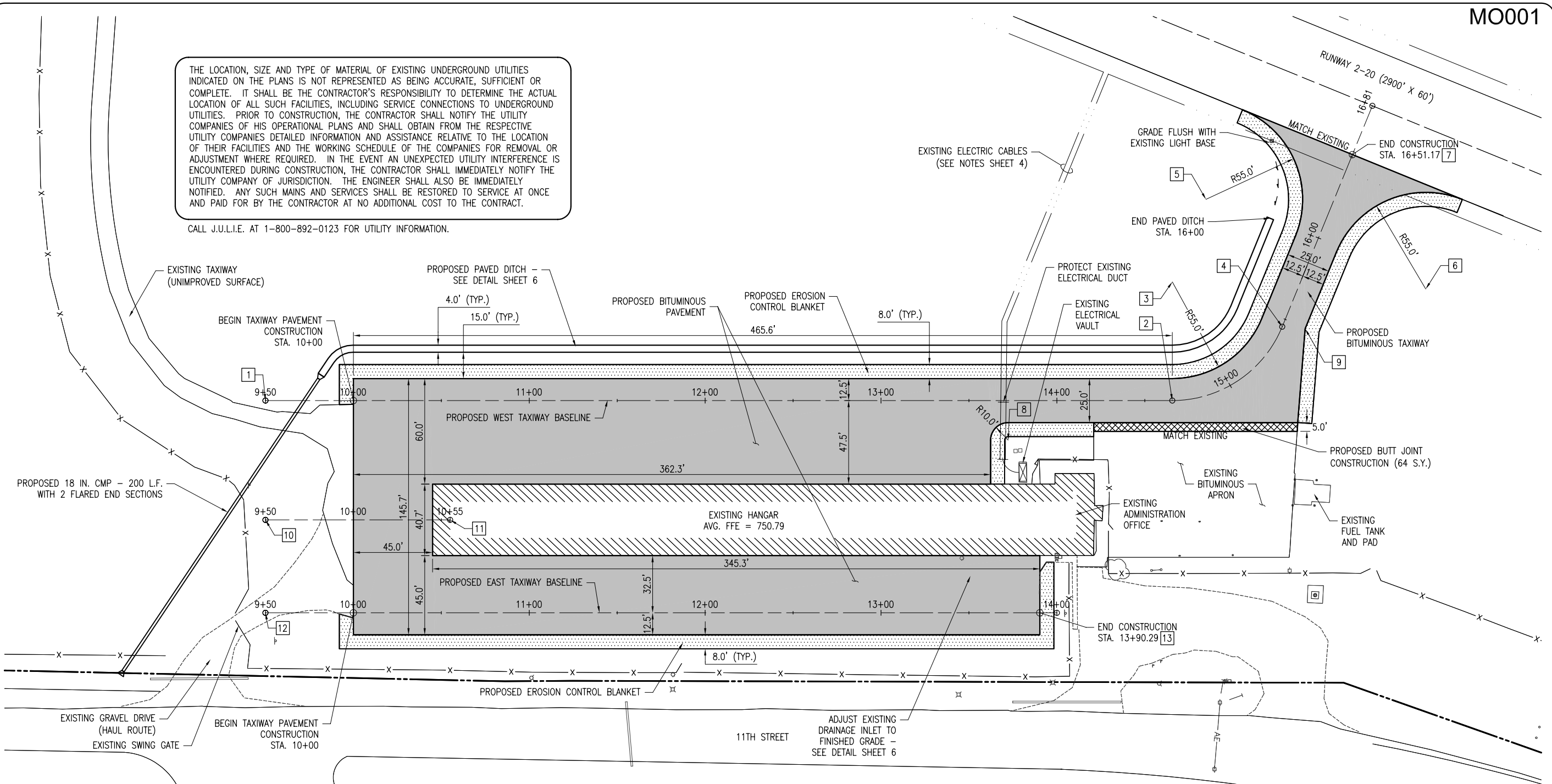


JUN 26, 2009 12:07 PM HARR01115
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REMOVE AND RECONSTRUCT
T-HANGAR TAXIWAYS
EXISTING SITE PLAN AND
AND
PROPOSED PREPARATION PLAN

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. AT 1-800-892-0123 FOR UTILITY INFORMATION.



COORDINATE DATA TABLE					
NO.	STATION	OFFSET	NORTHING	EASTING	DESCRIPTION
1	9+50.00	0'	1551651.6541	2168134.2736	BEGIN WEST ALIGNMENT
2	14+65.61	0'	1552167.0863	2168147.9376	ALIGNMENT P.C.
3	14+65.61	67.5' LT.	1552168.8750	2168080.4613	RADIUS POINT
4	15+45.50	0'	1552230.6768	2168107.6052	ALIGNMENT P.T.
5	15+96.17	67.5' LT.	1552189.2520	2168034.0666	RADIUS POINT
6	15+96.17	67.5' RT.	1552312.8556	2168088.3543	RADIUS POINT
7	16+51.17	0'	1552273.1710	2168010.8534	END CONSTRUCTION AT RUNWAY 2-20
8	13+72.29	22.5' RT	1552073.2036	2168167.9567	RADIUS POINT
9	15+49.33	12.5' RT	1552243.6647	2168109.1184	EDGE OF PAVEMENT
10	9+50.00	0'	1551649.8558	2168202.1075	BEGIN CENTER ALIGNMENT
11	10+55.00	0'	1551754.8190	2168204.8900	END CENTER ALIGNMENT
12	9+50.00	0'	1551648.4551	2168254.9467	BEGIN EAST ALIGNMENT
13	13+90.29	0'	1552088.5945	2168266.6147	END CONSTRUCTION AT EAST ALIGNMENT

BITUMINOUS MATERIAL

BITUMINOUS TAXIWAY PAVEMENTS SHALL COMPLY WITH THE SPECIAL PROVISIONS FOR AIRCRAFT WEIGHING LESS THAN 60,000 POUNDS, CHECK SHEET 21.

EARTHWORK

FOLLOWING REMOVAL OF THE EXISTING PAVEMENT, THE CONTRACTOR SHALL THEN BE REQUIRED TO EXCAVATE, REWORK AND COMPACT THE SUBGRADE PRIOR TO FILL PLACEMENT OR AGGREGATE BASE PLACEMENT IN ACCORDANCE WITH ITEM 152 OF THE SPECIFICATIONS AND TO THE SATISFACTION OF THE RESIDENT ENGINEER. EXCAVATED MATERIAL WILL BE HAULED OFF-SITE AND DISPOSED OF IN A LEGAL MANNER BY THE CONTRACTOR. EXCAVATION, REWORK, AND COMPACTION OF THE EXISTING EARTH SUBGRADE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION ALLOWED.

AREAS FOUND TO BE SOFT OR OTHERWISE UNSUITABLE FOR PLACEMENT OF SPECIFIED MATERIALS AND UNABLE TO REACH COMPACTION REQUIREMENTS SHALL BE UNDERCUT AND BACKFILLED WITH APPROVED MATERIAL FROM ON-SITE TO THE SATISFACTION OF THE RESIDENT ENGINEER.

ONCE THE SUBGRADE IS ACCEPTED BY THE RESIDENT ENGINEER, THE CONTRACTOR SHALL PLACE SOIL STABILIZATION FABRIC AND CONSTRUCT THE PAVEMENT SECTION IN ACCORDANCE WITH THESE CONSTRUCTION PLANS, THE SPECIFICATIONS, AND THE SPECIAL PROVISIONS.

ALL EARTHWORK WILL BE PAID FOR UNDER ITEM AR152410 UNCLASSIFIED EXCAVATION, PER CUBIC YARD.

ALL DISTURBED AREAS OUTSIDE OF PAVEMENT WILL BE SEEDED AND MULCHED.

BUTT JOINT CONSTRUCTION

THE PROPOSED BUTT JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ITEM AR401655 BUTT JOINT CONSTRUCTION AS STATED IN THE SUPPLEMENTAL SPECIFICATION AND RECURRING SPECIAL PROVISIONS.

THIS ITEM SHALL CONSIST OF REMOVING THE EXISTING BITUMINOUS PAVEMENT SURFACE BY MILLING IN ORDER TO CONSTRUCT A BUTT JOINT FOR THE TRANSITION OF THE PROPOSED BITUMINOUS PAVEMENT AT THE LOCATIONS AS SHOWN ON THE PLANS.

THE PAVEMENT SURFACE WILL BE MILLED TO ACCOMMODATE A MINIMUM DEPTH OF 1.5 INCHES OF BITUMINOUS SURFACE AT THE BUTT JOINT. ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

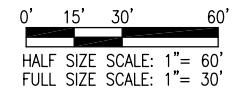
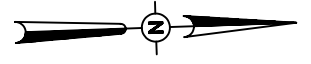
IF A TRUE VERTICAL FACE IS NOT ESTABLISHED ALONG THE BUTT JOINT FROM THE MILLING OPERATIONS, THE CONTRACTOR IS REQUIRED TO SAWCUT THE JOINT. THE SAWING, IF NECESSARY, SHALL BE CONSIDERED INCIDENTAL TO THE BUTT JOINT CONSTRUCTION AND NO ADDITIONAL COMPENSATION ALLOWED.

PRIOR TO APPLYING THE BITUMINOUS MATERIAL ALL MILLED MATERIAL WILL BE BROOMED AND BLOWN CLEAN AND A BITUMINOUS TACK COAT APPLIED. THE VERTICAL FACE WILL BE PAINTED WITH A LIQUID ASPHALT. THIS TACK COAT SHALL BE INCIDENTAL TO THE BUTT JOINT CONSTRUCTION AND NO ADDITIONAL COMPENSATION ALLOWED.

ALL WORK RELATED TO THE CONSTRUCTION OF THE BUTT JOINT WILL BE PAID FOR UNDER ITEM AR401655 BUTT JOINT CONSTRUCTION, PER SQUARE YARD.

LEGEND

- EXISTING BUILDING
- EXISTING PAVEMENT
- PROPOSED BITUMINOUS PAVEMENT
- PROPOSED EROSION CONTROL BLANKET
- PROPOSED BUTT JOINT CONSTRUCTION
- EXISTING FENCE
- PROPOSED DITCH



DATE	REVISION	BY

MONMOUTH MUNICIPAL AIRPORT
MONMOUTH, ILLINOIS

IL. PROJ.: C66-3871 A.I.P. PROJ.: 3-17-0069-B2

HEL Project No. 08A0188D_0800	04/21/09
Filename R-121CON.DWG	MLH
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Date 06/19/09	RAW
LAYOUT	REVIEWED
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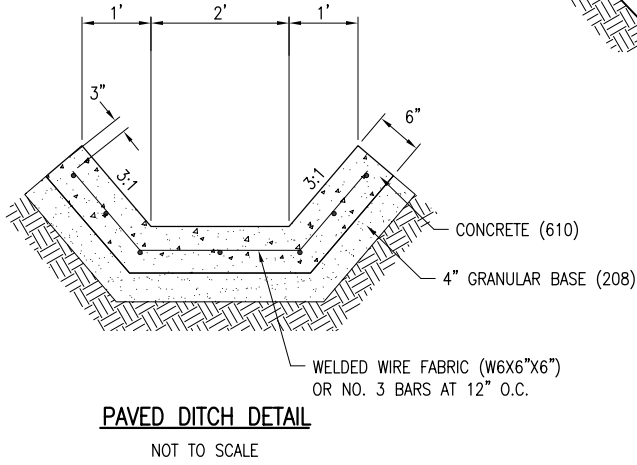
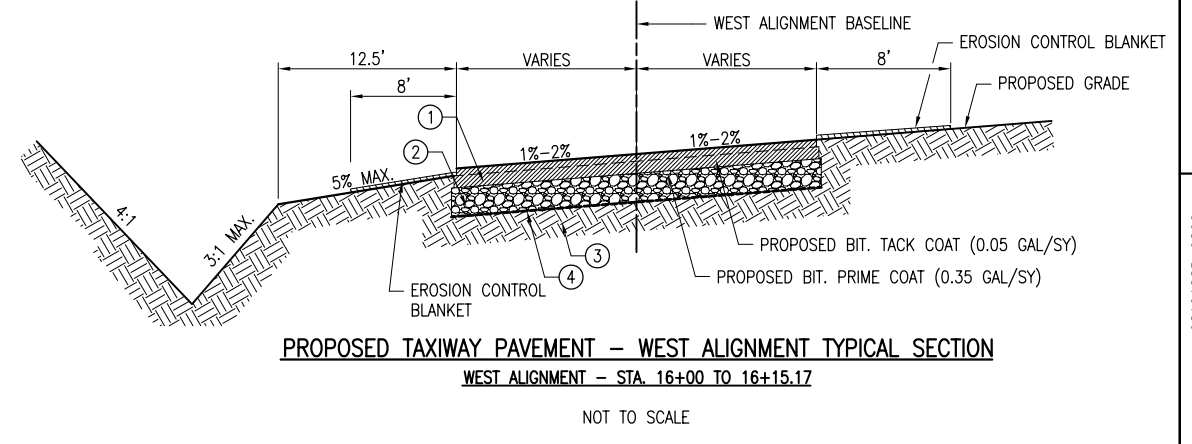
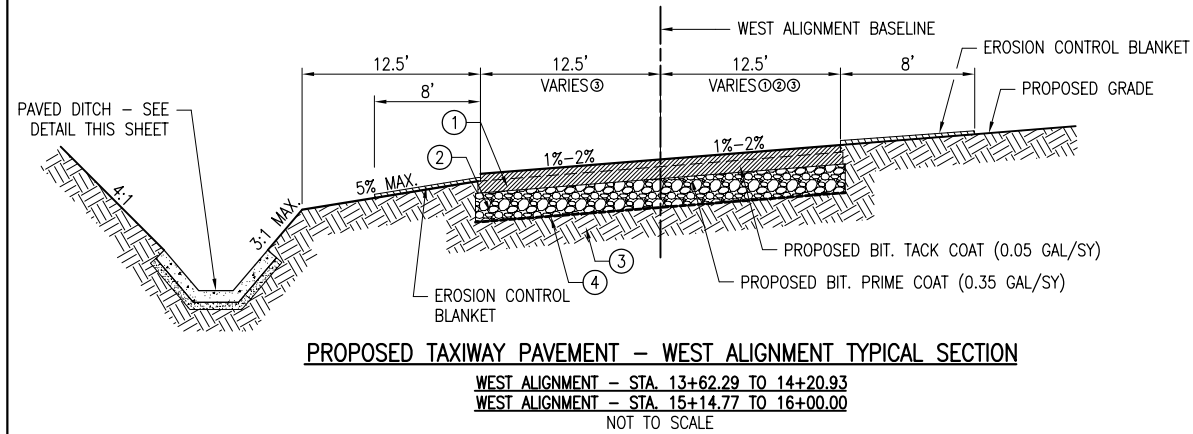
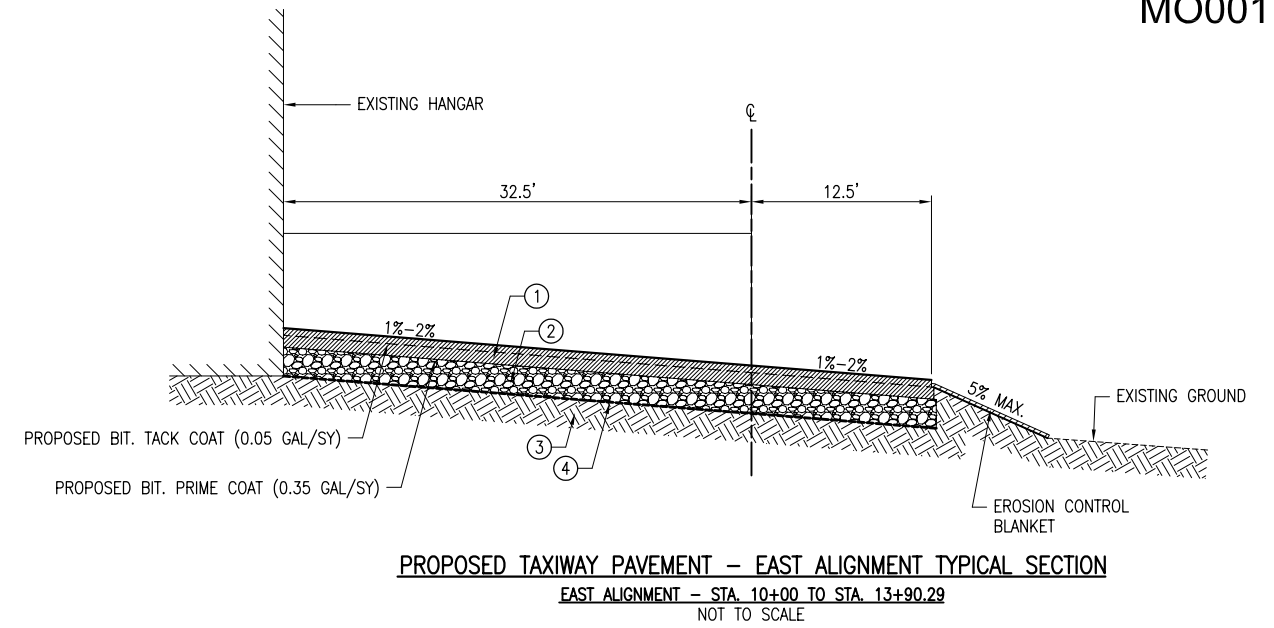
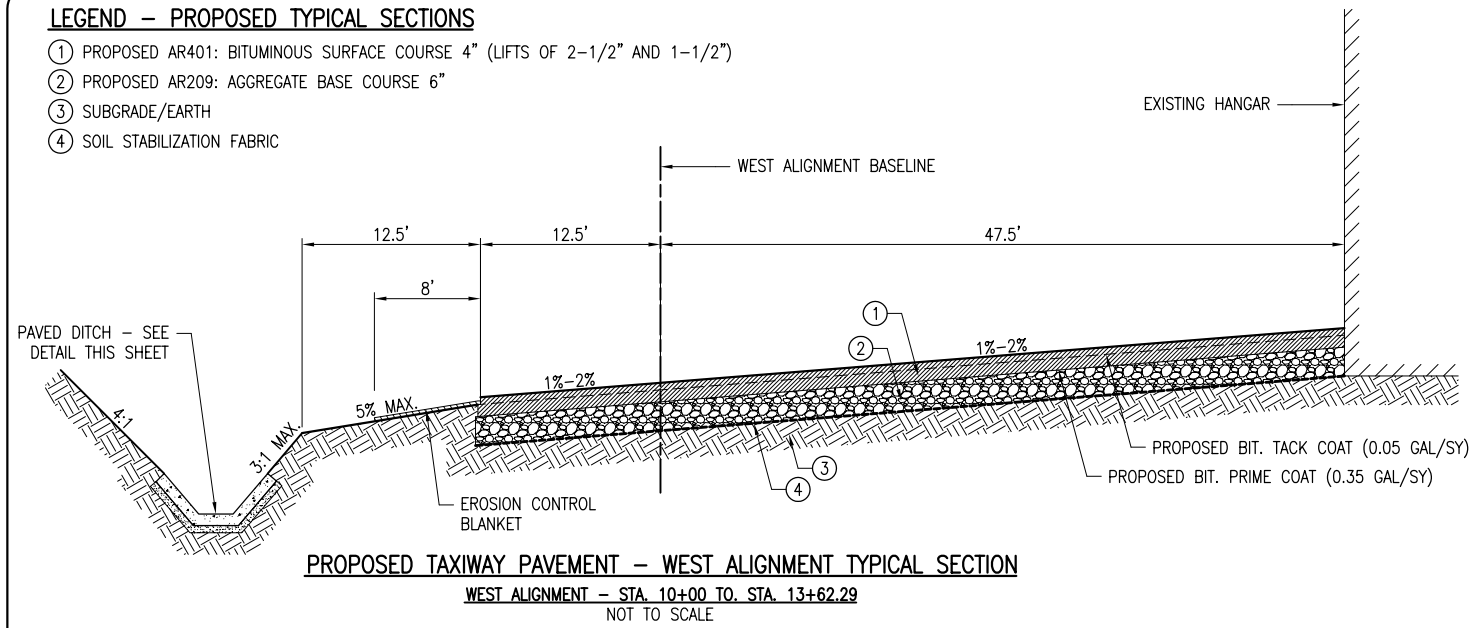
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REMOVE AND RECONSTRUCT
T-HANGAR TAXIWAYS

PROPOSED CONSTRUCTION PLAN

LEGEND – PROPOSED TYPICAL SECTIONS

- ① PROPOSED AR401: BITUMINOUS SURFACE COURSE 4" (LIFTS OF 2-1/2" AND 1-1/2")
- ② PROPOSED AR209: AGGREGATE BASE COURSE 6"
- ③ SUBGRADE/EARTH
- ④ SOIL STABILIZATION FABRIC



PAVED DITCH NOTES
 PAVED DITCH SHALL BE CONSTRUCTED IN ACCORDANCE WITH ITEM 754 OF THE SUPPLEMENTAL AND RECURRING SPECIAL PROVISIONS.

CONCRETE SHALL BE IN ACCORDANCE WITH ITEM 610 OF THE SUPPLEMENTAL AND RECURRING SPECIAL PROVISIONS.

EXPANSION JOINTS SHALL BE FORMED OR CUT EVERY 40'. CONTRACTION JOINTS SHALL BE CUT OR TOOLED AT SPACING NOT TO EXCEED 10'.

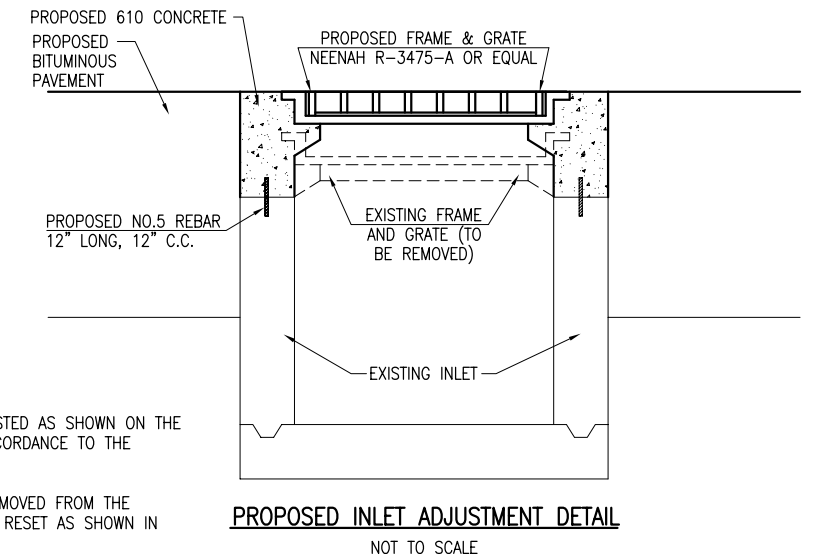
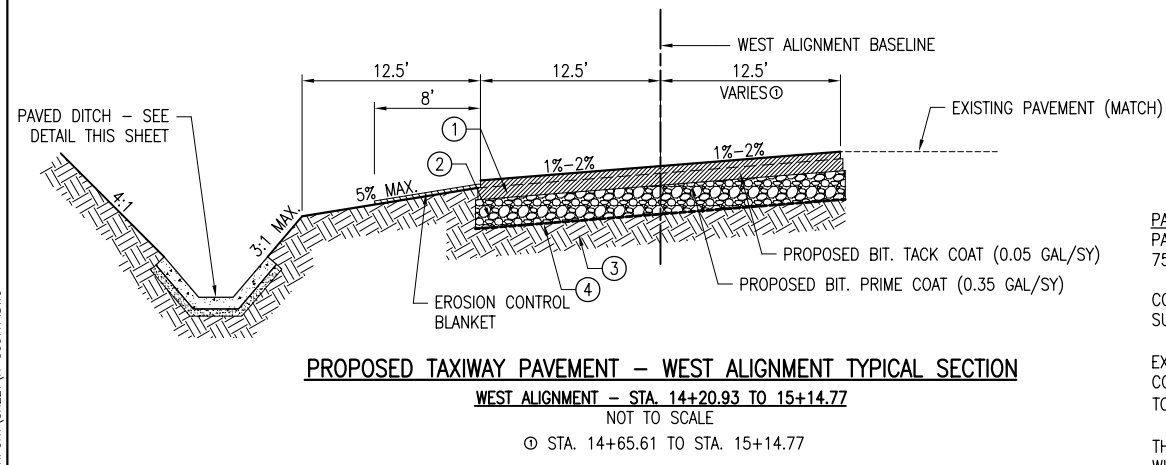
THE 4" GRANULAR BASE SHALL BE CONSTRUCTED IN ACCORDANCE WITH ITEM 208 OF THE SUPPLEMENTAL AND RECURRING SPECIAL PROVISIONS, AND SHALL BE INCIDENTAL TO ITEM AR754610, PAVED DITCH, AND NO ADDITIONAL COMPENSATION ALLOWED.

PAVED DITCH WILL BE PAID FOR UNDER ITEM AR754610, PAVED DITCH, PER L.F., AND WILL INCLUDE ALL MATERIAL AND LABOR TO COMPLETE THE ITEM.

INLET ADJUSTMENT NOTES
 THE EXISTING INLET WILL BE ADJUSTED AS SHOWN ON THE DETAIL ON THIS SHEET AND IN ACCORDANCE TO THE SPECIAL PROVISIONS.

THE EXISTING FRAME SHALL BE REMOVED FROM THE EXISTING INLET AND A NEW FRAME RESET AS SHOWN IN THE DETAIL.

THE PROPOSED INLET ADJUSTMENT WILL BE PAID FOR UNDER ITEM AR751940, ADJUST INLET, PER EACH.



BY	
REVISION	
DATE	

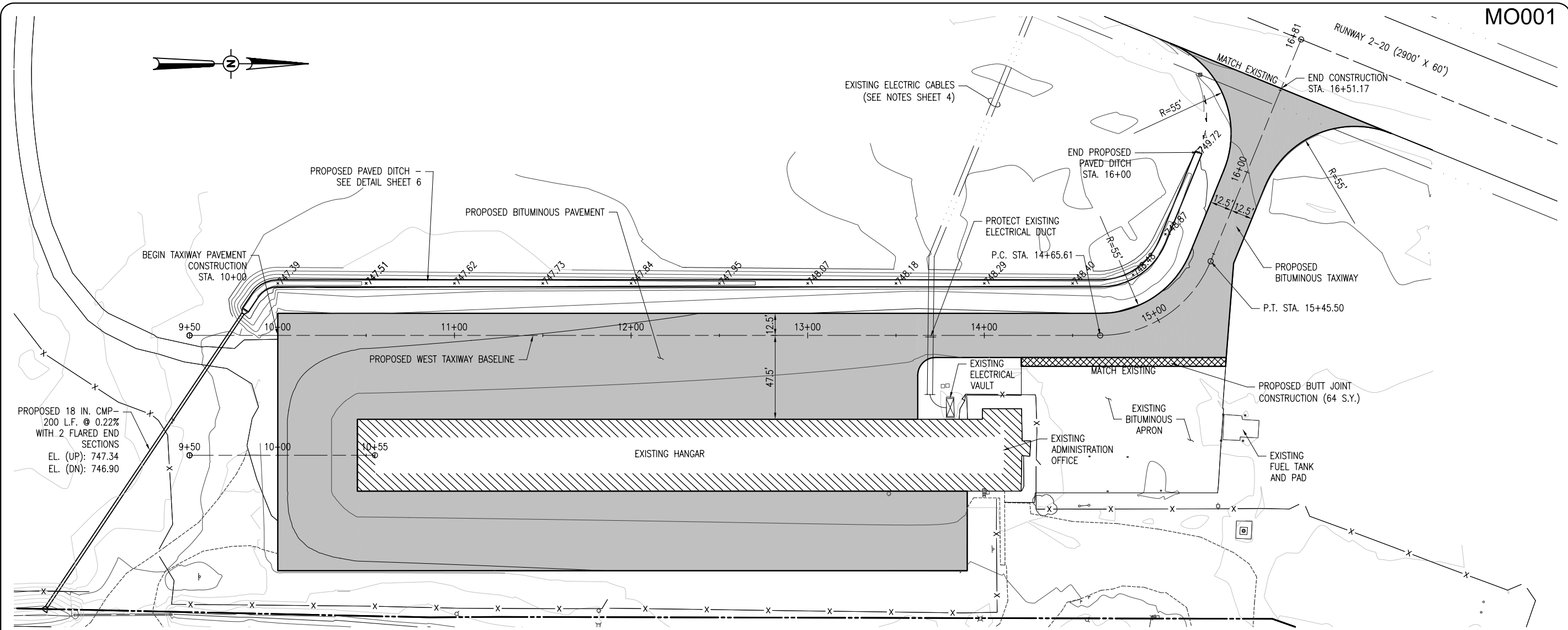
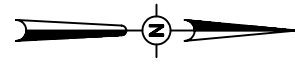
**MONMOUTH MUNICIPAL AIRPORT
 MONMOUTH, ILLINOIS**
 A.I.P. PROJ.: 3-17-0069-B2
 I.L. PROJ.: C66-3871

HEL Project No. 08A0188D_0800	06/06/09
Filename R-3001XP.DWG	MLH
Scale NOT TO SCALE	MLH
Date 06/19/09	RAW
LAYOUT	
DRAWN	
REVIEWED	

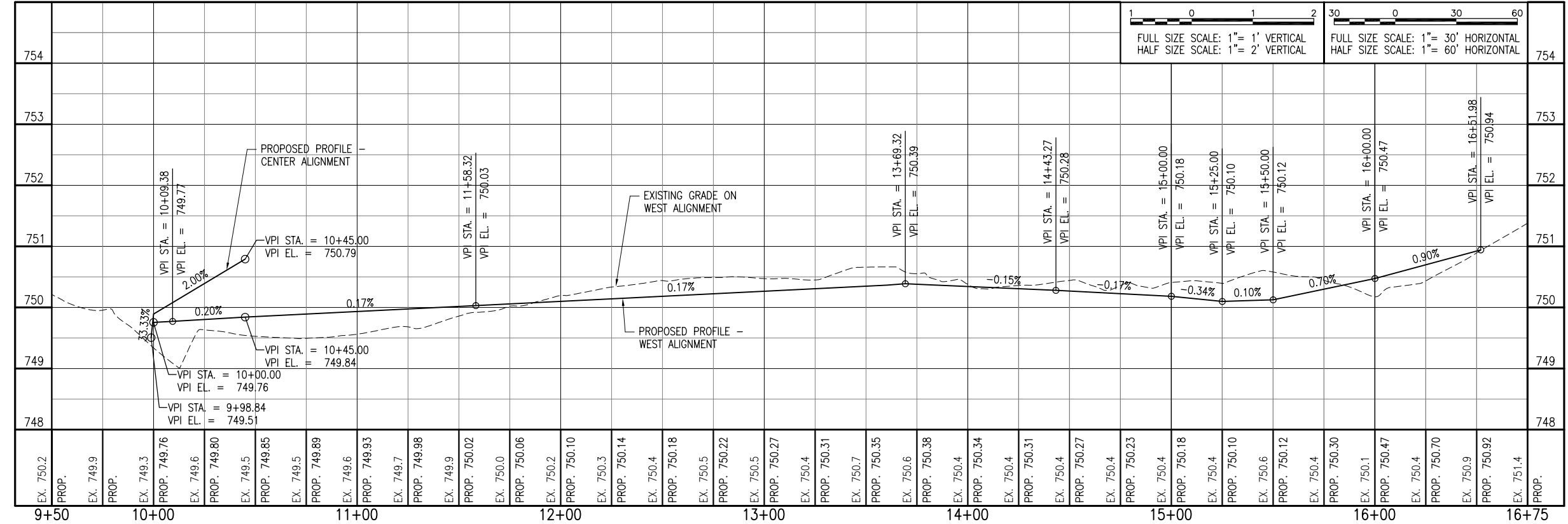
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**REMOVE AND RECONSTRUCT
 T-HANGAR TAXIWAYS**
 PROPOSED TYPICAL SECTIONS
 AND CONSTRUCTION DETAILS

JUN 26, 2009 3:15 PM HARRI01115
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PROPOSED 18 IN. CMP -
200 L.F. @ 0.22%
WITH 2 FLARED END
SECTIONS
EL. (UP): 747.34
EL. (DN): 746.90



1 0 1 2
FULL SIZE SCALE: 1" = 1' VERTICAL
HALF SIZE SCALE: 1" = 2' VERTICAL

30 0 30 60
FULL SIZE SCALE: 1" = 30' HORIZONTAL
HALF SIZE SCALE: 1" = 60' HORIZONTAL

JUN 26, 2009 3:50 PM HARRI01115
F:\AIRPORTS\MONMOUTH\08A0188D\CADD\AIRPORT\SHEET\R-701.DWG

DATE	REVISION	BY

**MONMOUTH MUNICIPAL AIRPORT
MONMOUTH, ILLINOIS**

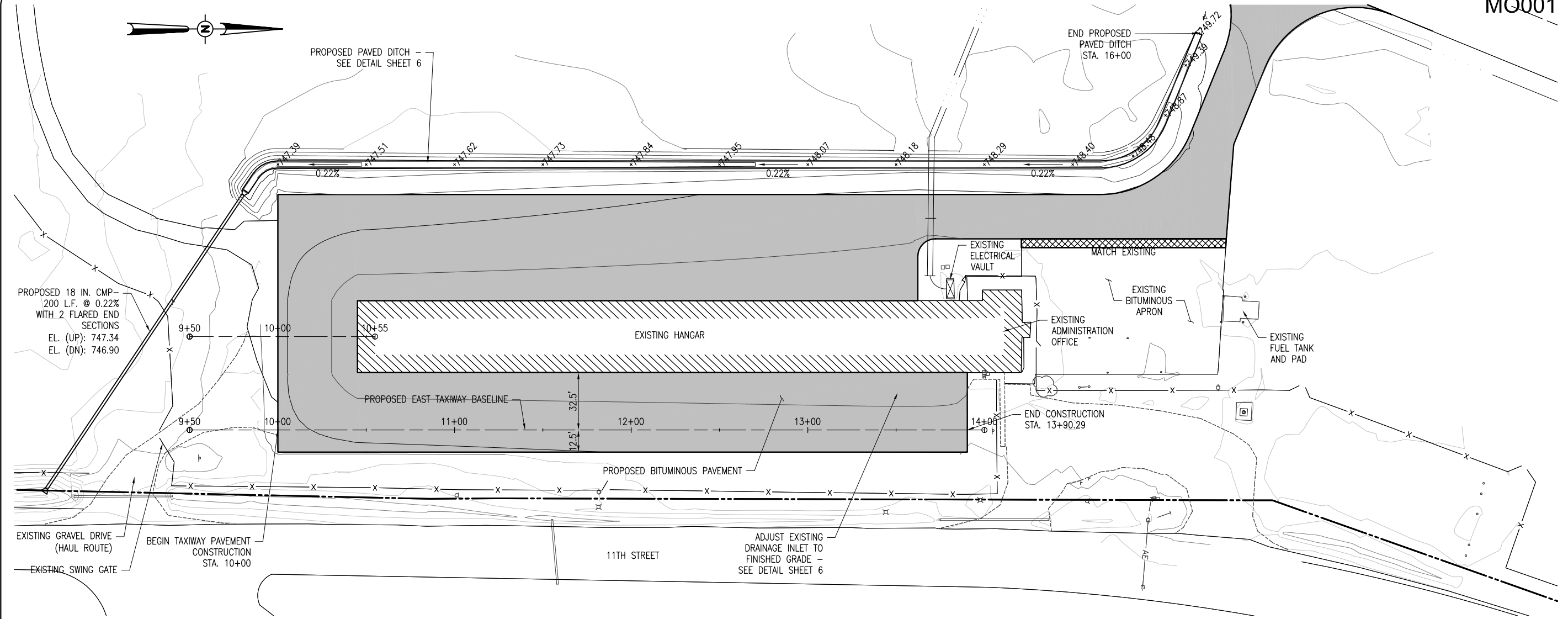
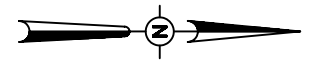
IL. PROJ.: C66-3871 A.I.P. PROJ.: 3-17-0069-B2

HEL Project No. 08A0188D_0800
Filename R-701.DWG
Scale 1"=30' H, 1"=1' V
Date 06/19/09

LAYOUT	DAK	06/08/09
DRAWN	MLH	06/08/09
REVIEWED	RAW	06/19/09



**REMOVE AND RECONSTRUCT
T-HANGAR TAXIWAYS**
PROPOSED PLAN AND PROFILE -
WEST ALIGNMENT



PROPOSED 18 IN. CMP -
200 L.F. @ 0.22%
WITH 2 FLARED END
SECTIONS
EL. (UP): 747.34
EL. (DN): 746.90

EXISTING GRAVEL DRIVE
(HAUL ROUTE)
EXISTING SWING GATE

BEGIN TAXIWAY PAVEMENT
CONSTRUCTION
STA. 10+00

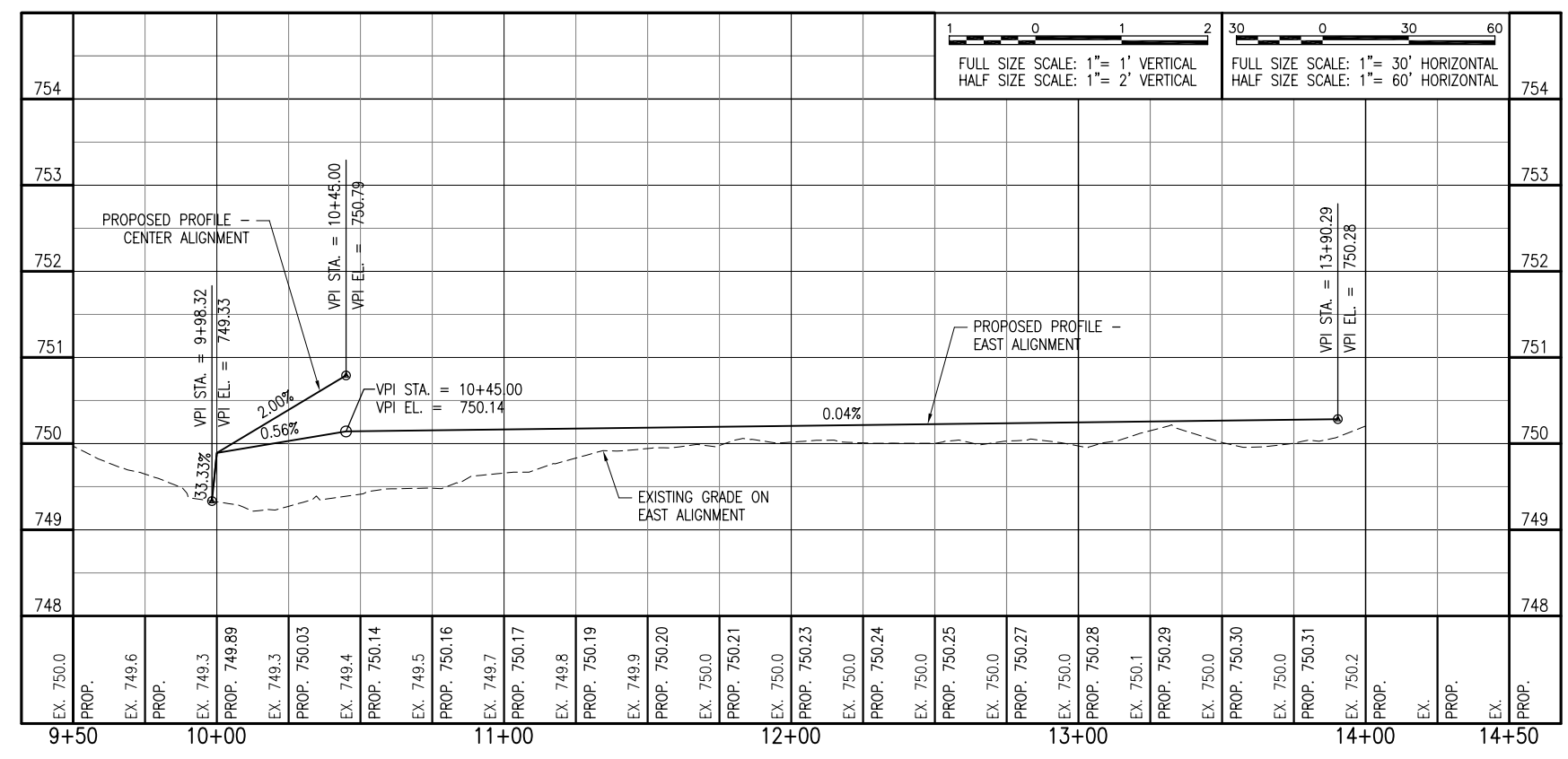
11TH STREET

ADJUST EXISTING
DRAINAGE INLET TO
FINISHED GRADE -
SEE DETAIL SHEET 6

END PROPOSED
PAVED DITCH
STA. 16+00

END CONSTRUCTION
STA. 13+90.29

JUN 26, 2009 3:56 PM HARRI01115
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DATE	REVISION	BY

**MONMOUTH MUNICIPAL AIRPORT
MONMOUTH, ILLINOIS**

IL. PROJ.: C66-3871 A.I.P. PROJ.: 3-17-0069-B2

FILE PROJECT No.	DATE	REVIEWED
08A0188D_0800	06/08/09	DAK
Filename: R-702.DWG	06/08/09	MLH
Scale: 1"=30' H, 1"=1' V	06/19/09	RAW
Date: 06/19/09		

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**REMOVE AND RECONSTRUCT
T-HANGAR TAXIWAYS**

PROPOSED PLAN AND PROFILE -
EAST ALIGNMENT

DATE	REVISION	BY

**MONMOUTH MUNICIPAL AIRPORT
MONMOUTH, ILLINOIS**

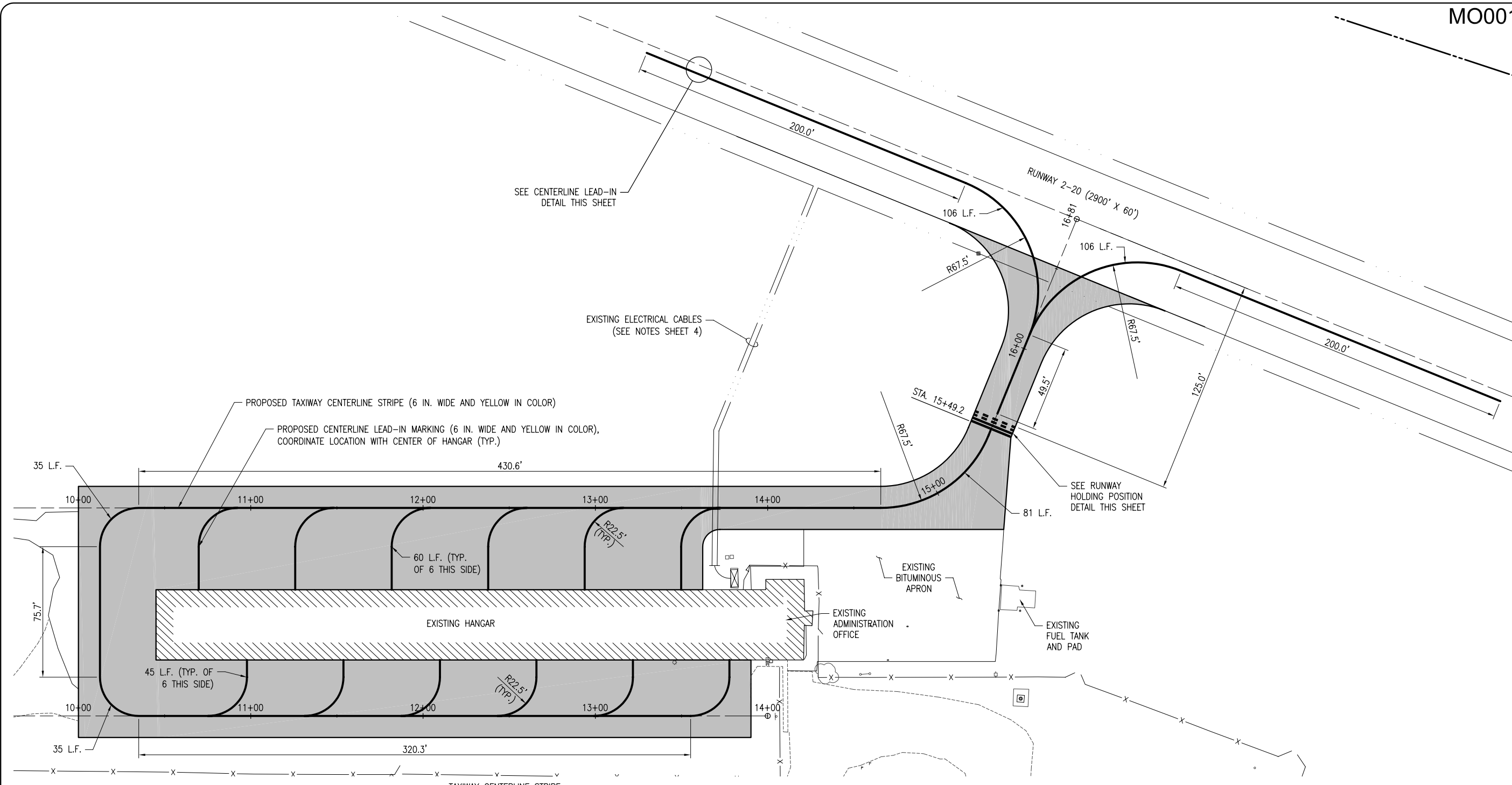
IL PROJ.: C66-3871 A.I.P. PROJ.: 3-17-0069-B2

LAYOUT	MLH	DATE

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**REMOVE AND RECONSTRUCT
T-HANGAR TAXIWAYS**

PROPOSED PAVEMENT
MARKING PLAN



PAVEMENT MARKING NOTES

ALL PROPOSED TAXIWAY CENTERLINE/LEAD-IN MARKINGS SHALL BE SOLID, 6" IN WIDTH, AND YELLOW IN COLOR. CENTERLINE MARKING WILL NOT BE OUTLINED WITH BLACK BORDER.

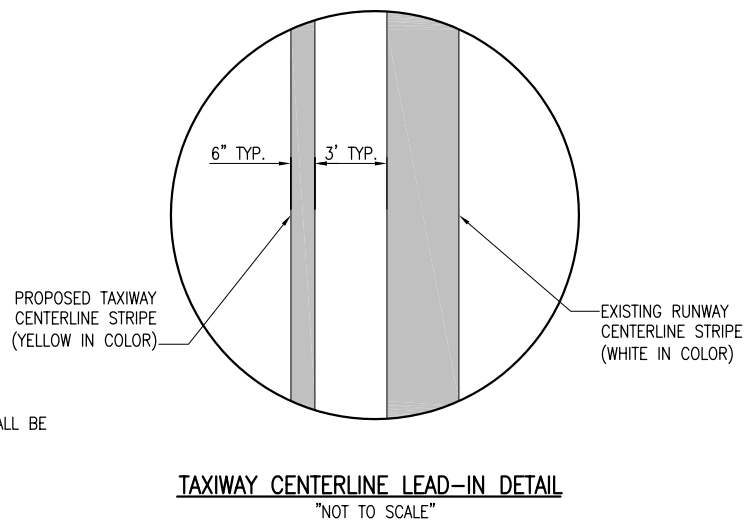
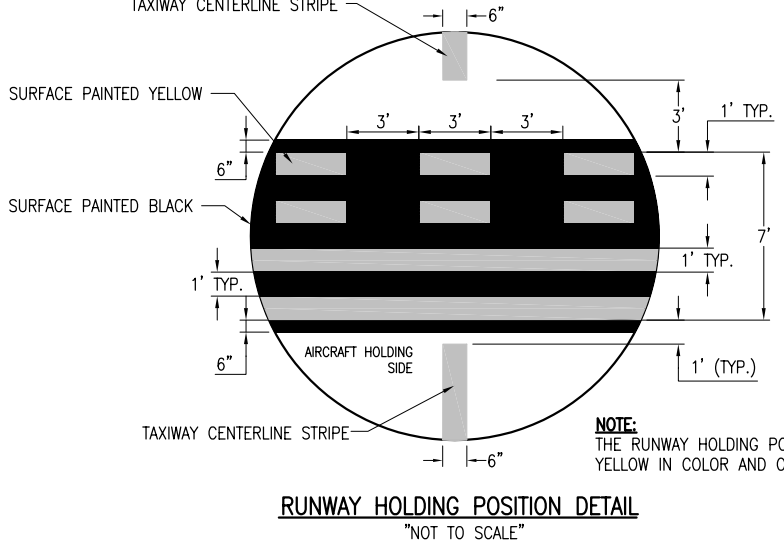
THE RUNWAY HOLDING POSITION MARKINGS SHALL BE YELLOW AND OUTLINED IN BLACK, AS DETAILED.

ALL PROPOSED MARKING WILL BE PAINTED WITH TWO APPLICATIONS. ALL PROPOSED MARKING (EXCEPT FOR BLACK BORDERS) WILL BE APPLIED WITH A REFLECTIVE MEDIA ON THE SECOND APPLICATION. APPLICATION RATES WILL BE IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

CUT-OFF SHEETS WILL BE REQUIRED TO ENSURE STRAIGHT EDGES.

PAVEMENT MARKING WILL BE PAID FOR UNDER ITEM AR620520, PAVEMENT MARKING-WATERBORNE, PER S.F..

BLACK BORDER WILL BE PAID FOR UNDER ITEM AR620525, PAVEMENT MARKING-BLACK BORDER, PER S.F..



LEGEND

- EXISTING BUILDING
- EXISTING PAVEMENT
- PROPOSED PAVEMENT MARKING

SCALE

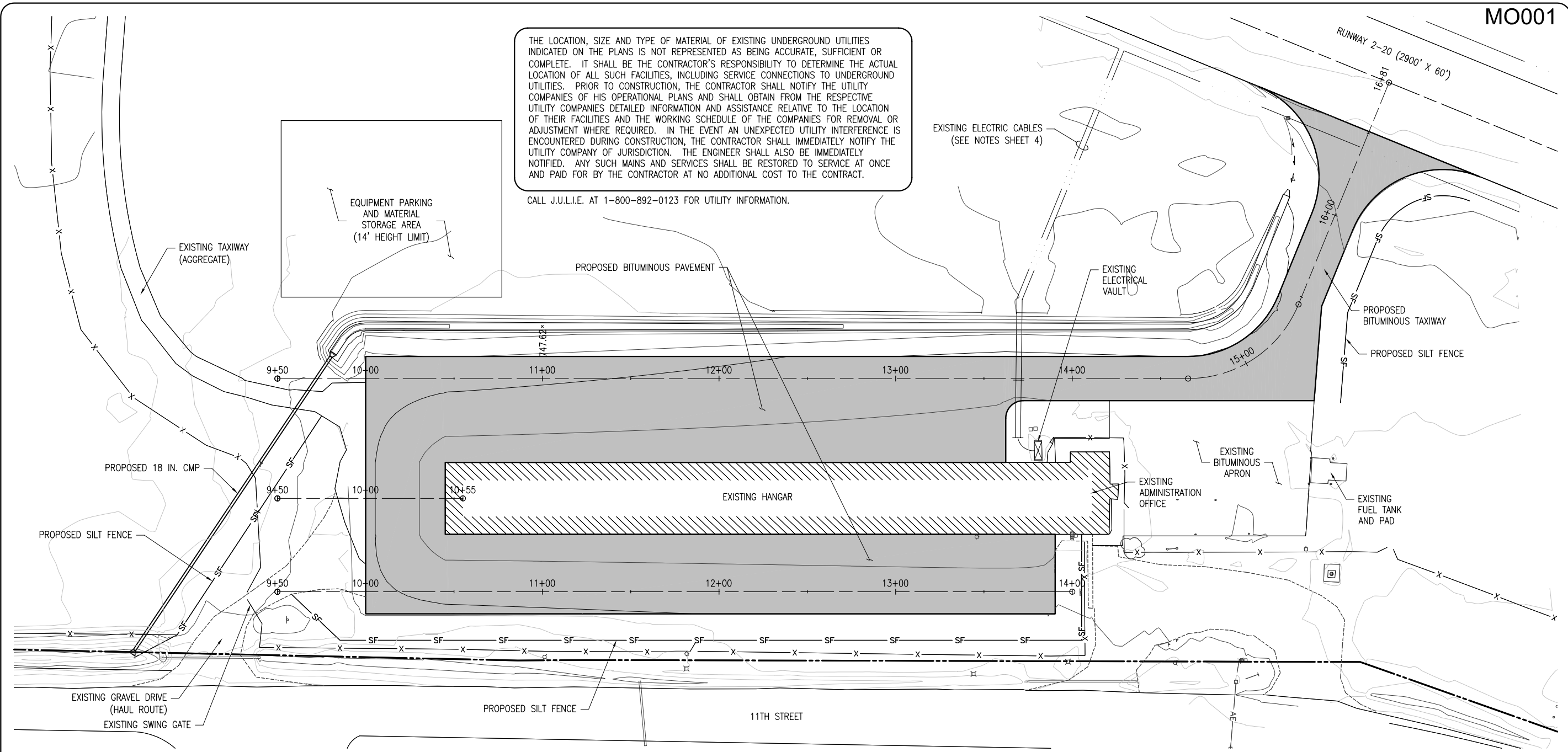
0' 15' 30' 60'

HALF SIZE SCALE: 1" = 60'
FULL SIZE SCALE: 1" = 30'

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THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.I.E. AT 1-800-892-0123 FOR UTILITY INFORMATION.



DATE	REVISION	BY

**MONMOUTH MUNICIPAL AIRPORT
MONMOUTH, ILLINOIS**

IL. PROJ.: C66-3871 A.I.P. PROJ.: 3-17-0069-B2

HEL Project No. 08A0188D_0800	04/21/09
Filename R-141SWPPP.DWG	MLH
Scale 1"=30'	06/19/09
Date 06/19/09	MLH
LAYOUT	RAW
DRAWN	
REVIEWED	

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**REMOVE AND RECONSTRUCT
T-HANGAR TAXIWAYS**

**PROPOSED STORMWATER POLLUTION
PREVENTION PLAN (SWPPP)**

EROSION CONTROL

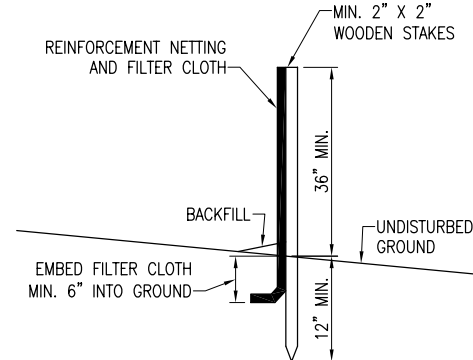
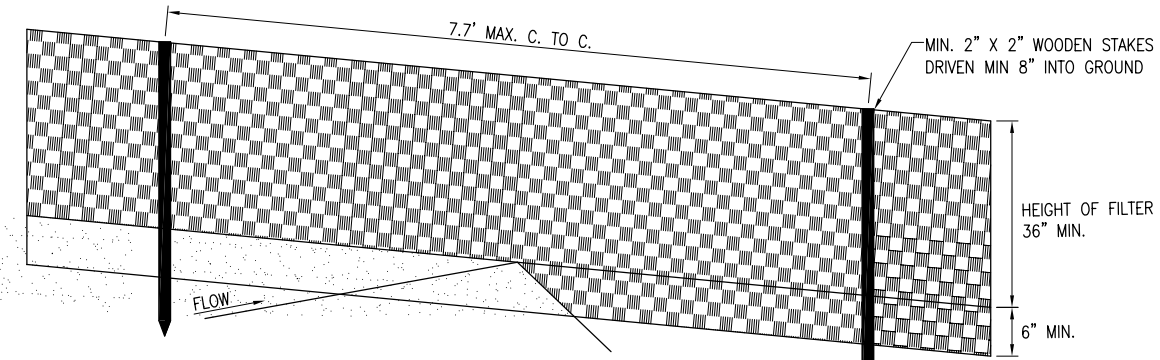
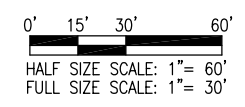
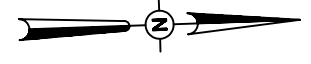
ALL PROPOSED EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS. EROSION CONTROL MEASURES ARE GOVERNED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY'S STANDARDS AND SPECIFICATIONS FOR SOIL EROSIONS AND SPECIFICATION FOR SOIL EROSIONS AND SEDIMENT CONTROL.

THE EXACT LOCATIONS OF THE EROSION CONTROL FENCE WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION. EROSION CONTROL FENCE SHALL BE REMOVED WHEN IT HAS SERVED ITS USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM WATER DRAINAGE.

COST OF REMOVAL AND REPLACEMENT SHALL BE INCLUDED IN THE UNIT PRICE FOR ITEM AR156510 SILT FENCE.

LEGEND

- EXISTING BUILDING
- EXISTING PAVEMENT
- PROPOSED BITUMINOUS PAVEMENT
- EXISTING FENCE
- PROPOSED SILT FENCE
- PROPOSED DITCH



PERIMETER SILT FENCE DETAIL

DATE	REVISION	BY

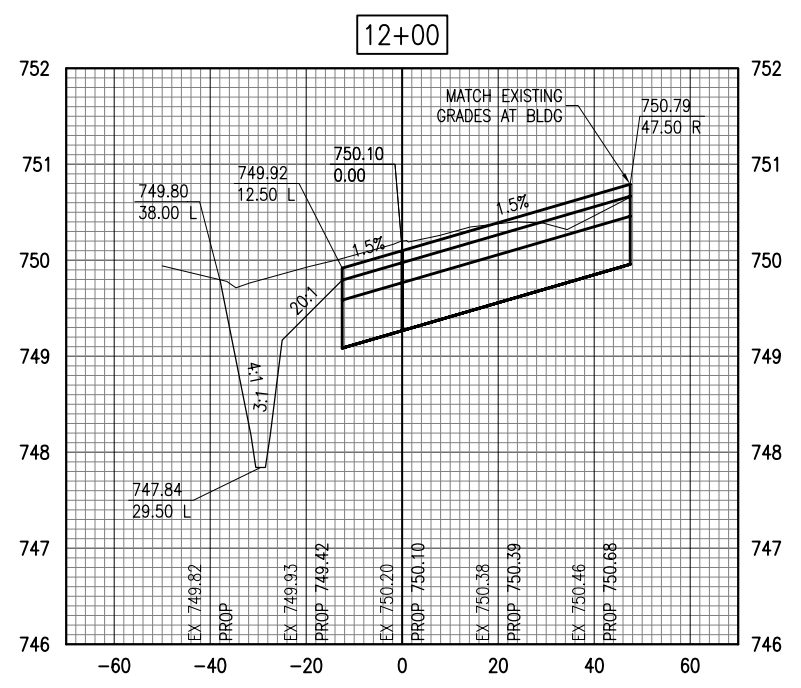
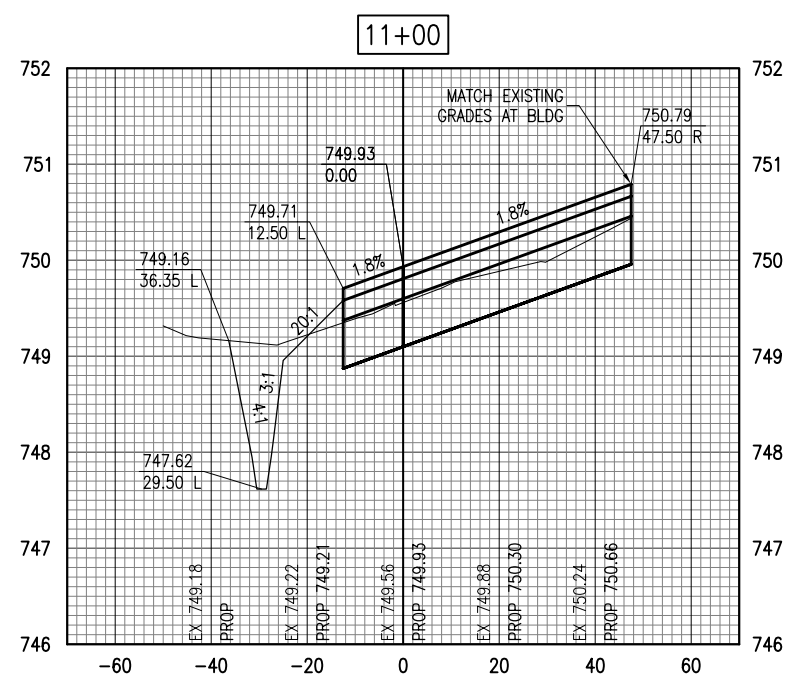
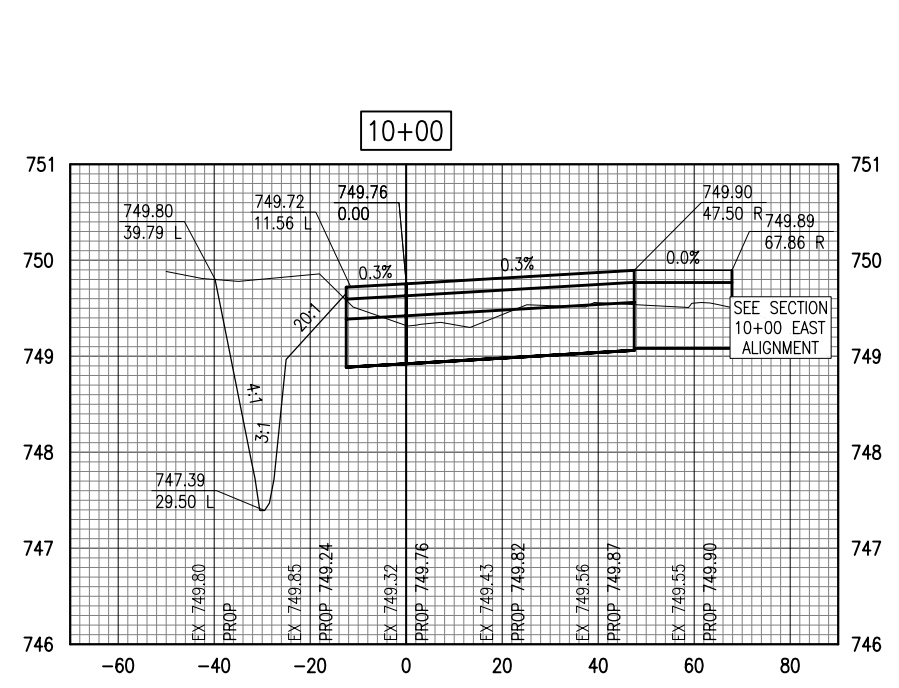
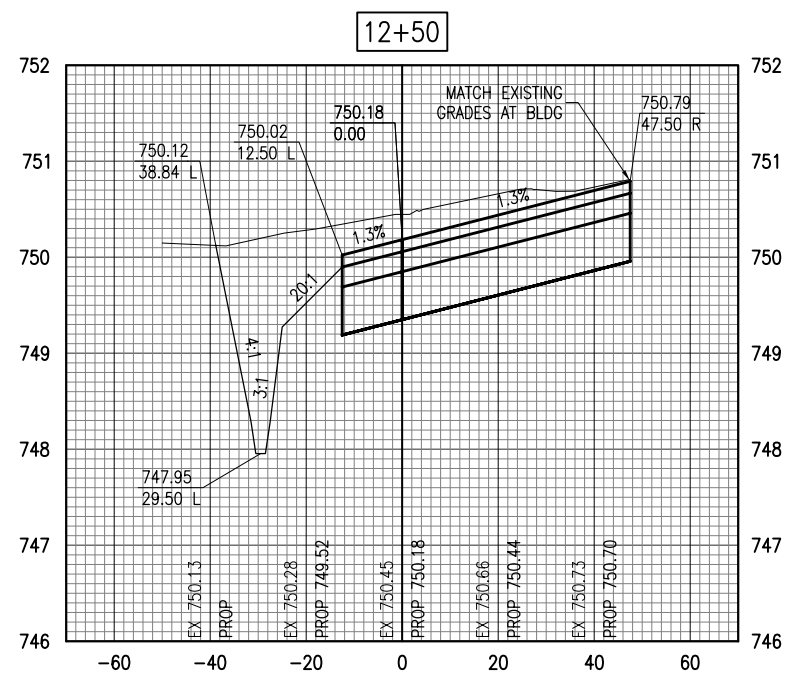
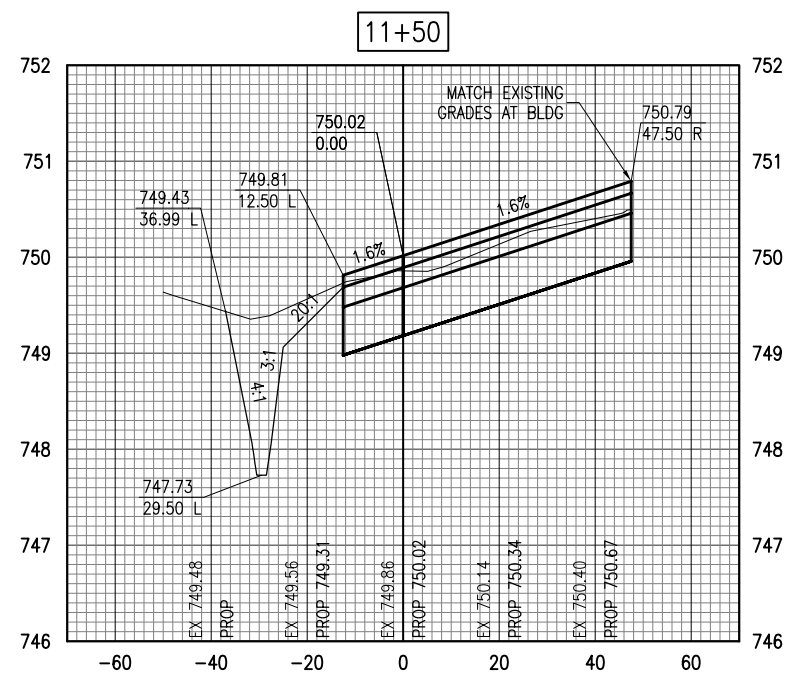
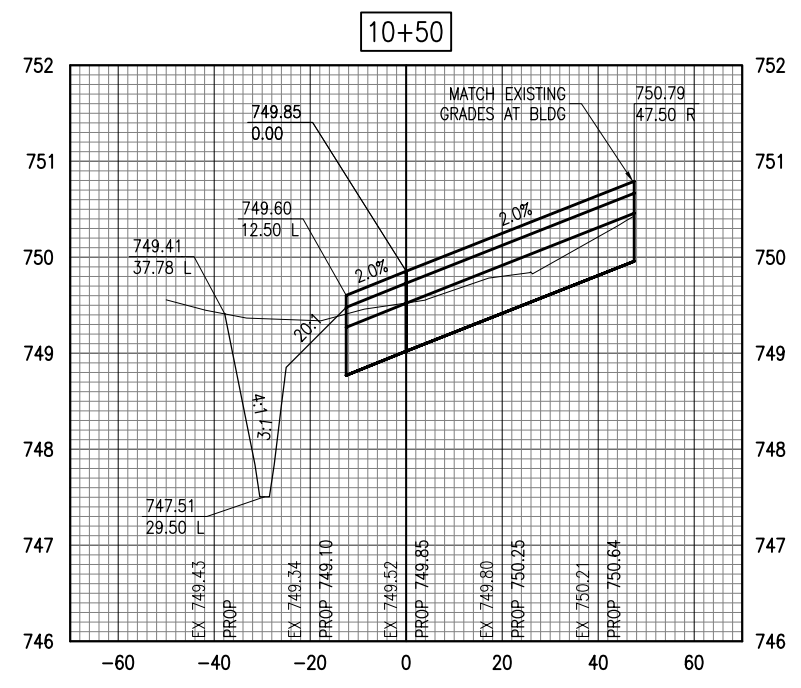
**MONMOUTH MUNICIPAL AIRPORT
MONMOUTH, ILLINOIS**

IL. PROJ.: C66-3871 A.I.P. PROJ.: 3-17-0069-B2

FILE PROJECT No. 08A0188D_0800	DATE	06/19/09
FILENAME R-301SECT.DWG	LAYOUT	DAK
SCALE 1"=20'H 1"=1'V	DRAWN	DAK
DATE 06/19/09	REVIEWED	RAW
		06/19/09

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**REMOVE AND RECONSTRUCT
T-HANGAR TAXIWAYS**
 PROPOSED CROSS SECTIONS -
 WEST ALIGNMENT
 STA. 10+00 TO STA. 12+50



JUN 26, 2009 4:25 PM HARRI01115
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DATE	REVISION	BY

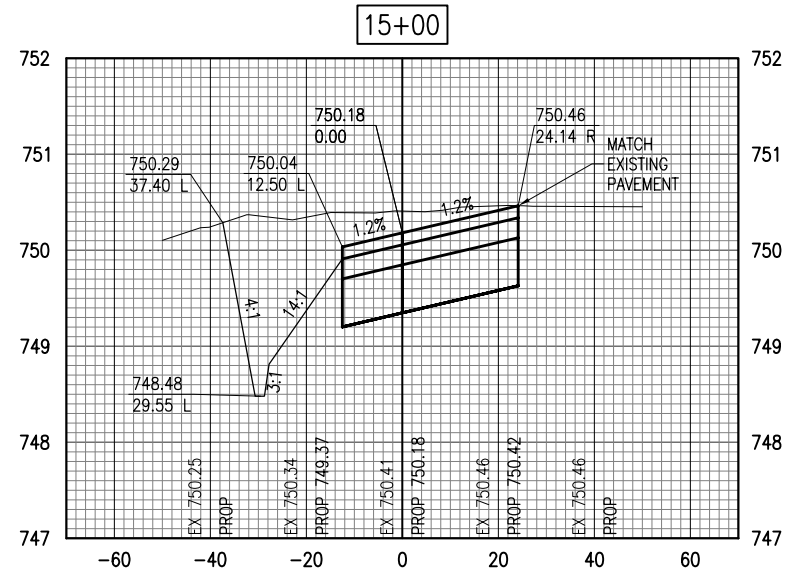
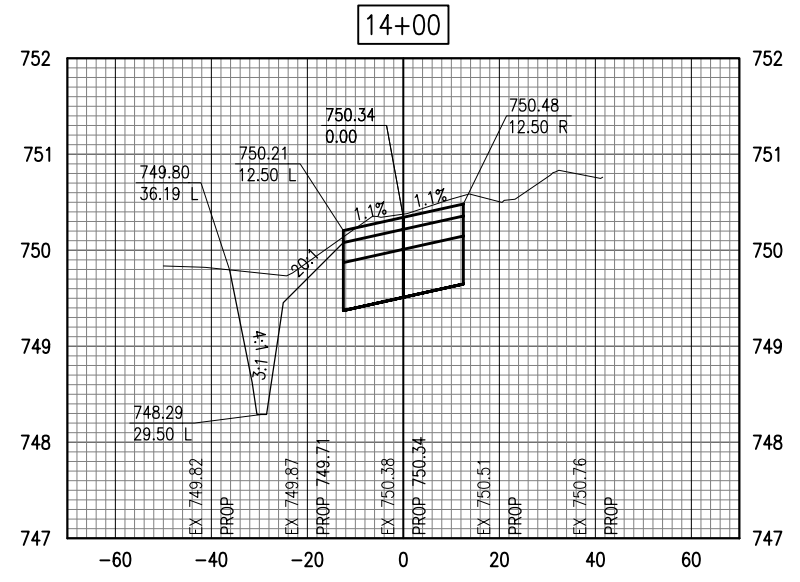
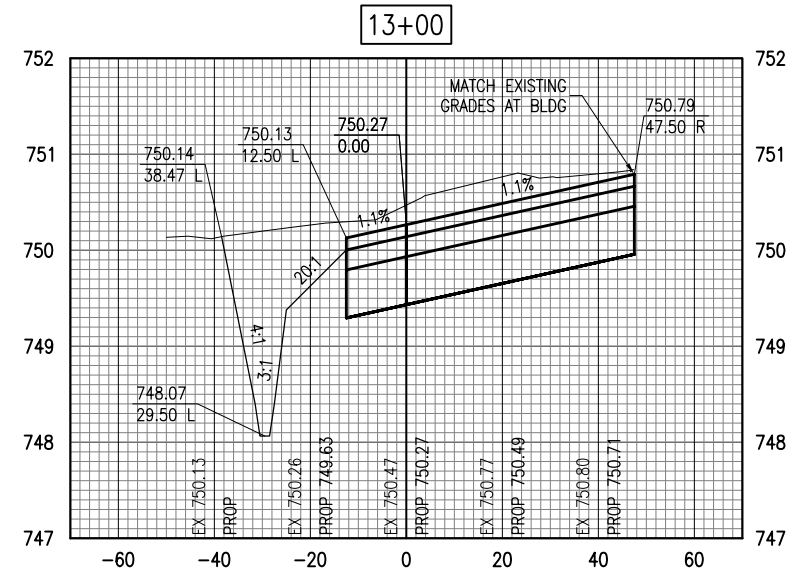
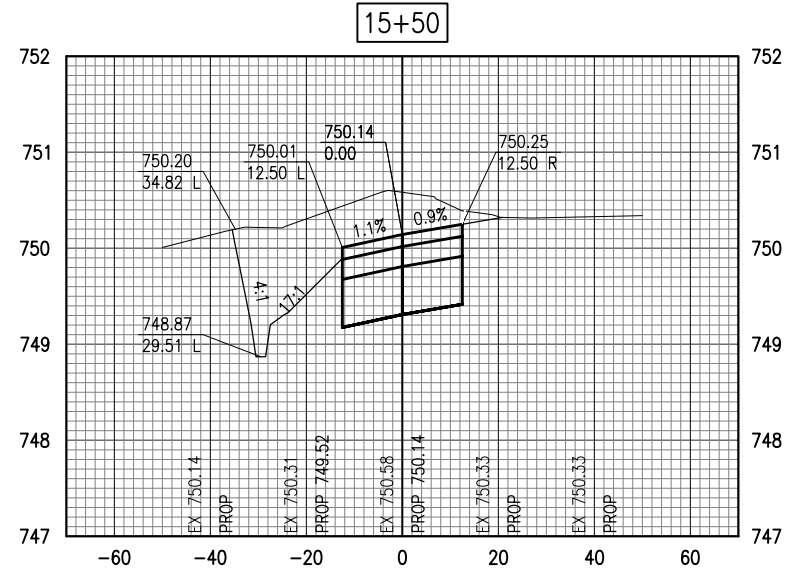
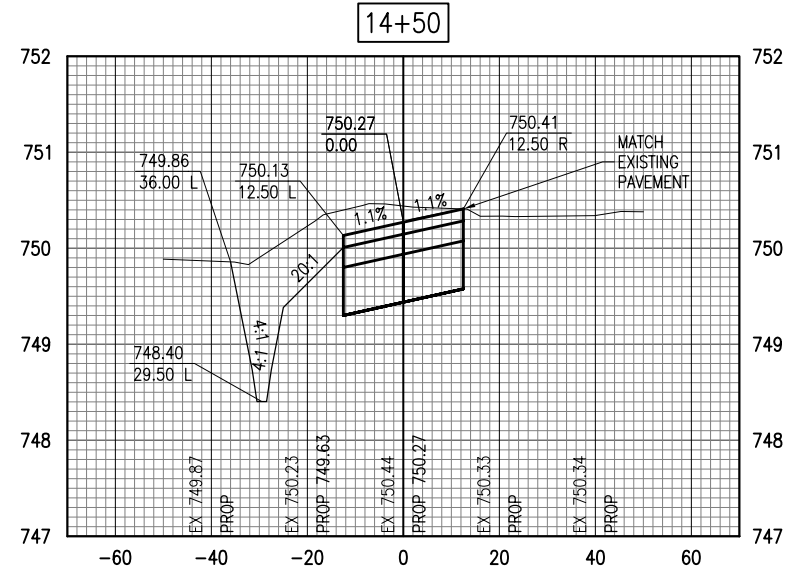
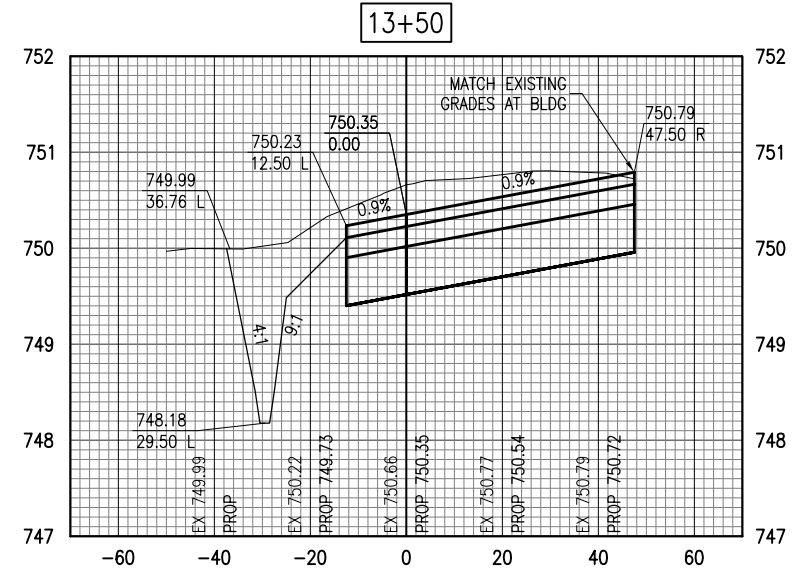
**MONMOUTH MUNICIPAL AIRPORT
MONMOUTH, ILLINOIS**

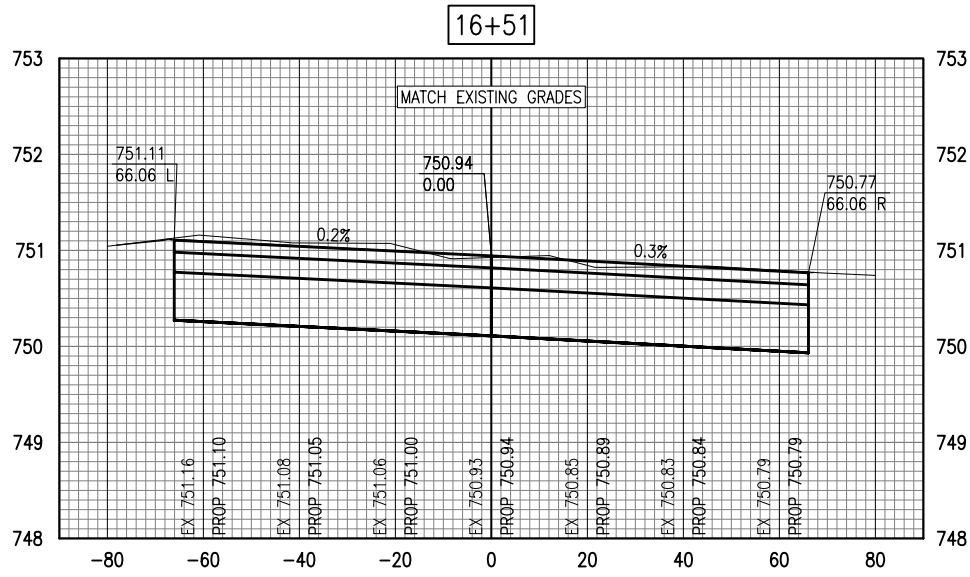
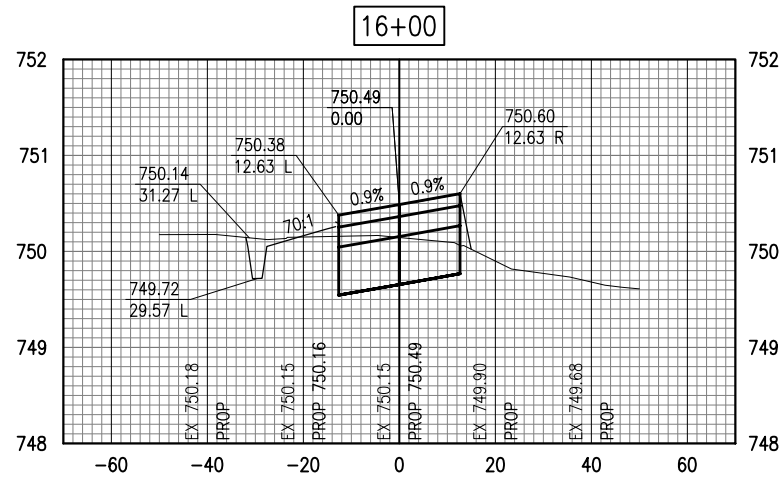
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FILE PROJECT No. 08A0188D_0800	DATE	06/19/09
FILENAME R-301SECT.DWG	DATE	06/19/09
SCALE 1"=20' H 1"=1' V	DATE	06/19/09
DATE 06/19/09	LAYOUT	DAK
	DRAWN	DAK
	REVIEWED	RAW

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T-HANGAR TAXIWAYS**
 PROPOSED CROSS SECTIONS -
 WEST ALIGNMENT
 STA. 13+00 TO STA. 15+50





FILE Project No.	08A0188D_0800
Filename	R-301\SECT.DWG
Scale	1"=20'H, 1"=1'V
Date	06/19/09
LAYOUT	DAK
DRAWN	DAK
REVIEWED	RAW
	06/19/09
	06/19/09
	06/19/09

**MONMOUTH MUNICIPAL AIRPORT
 MONMOUTH, ILLINOIS**

IL. PROJ.: C66-3871 A.I.P. PROJ.: 3-17-0069-B2



**REMOVE AND RECONSTRUCT
 T-HANGAR TAXIWAYS**

PROPOSED CROSS SECTIONS -
 WEST ALIGNMENT
 STA. 16+00 TO STA. 16+51

DATE	REVISION	BY

DATE	REVISION	BY

**MONMOUTH MUNICIPAL AIRPORT
MONMOUTH, ILLINOIS**

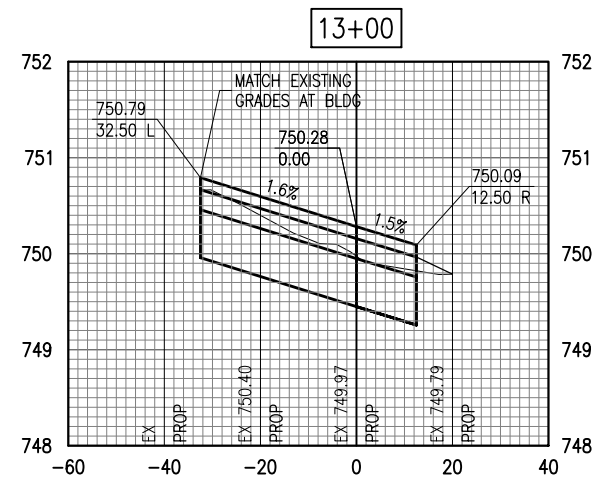
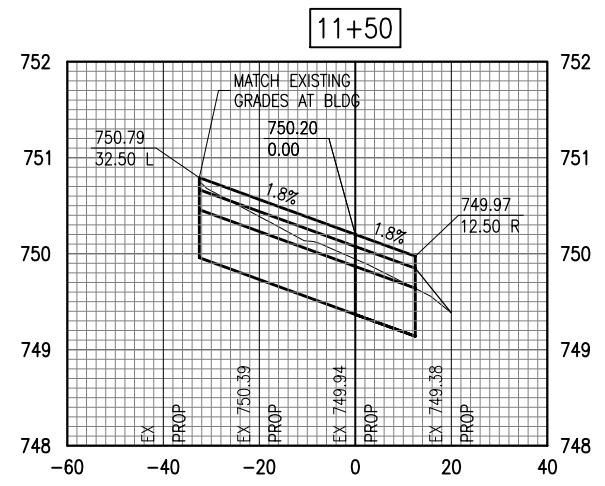
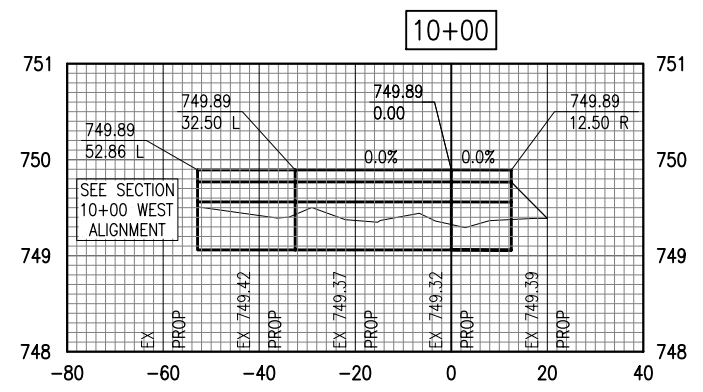
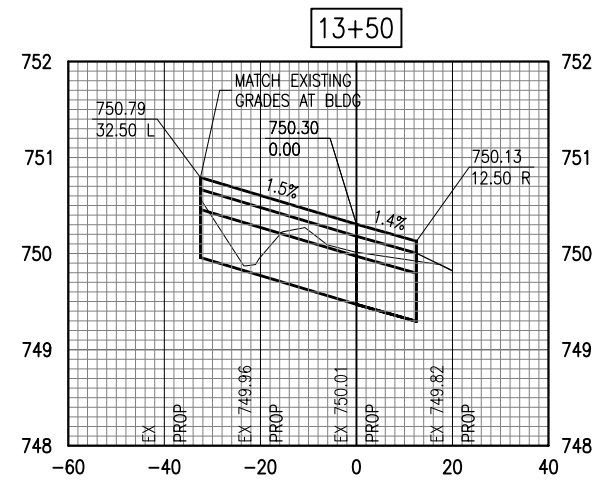
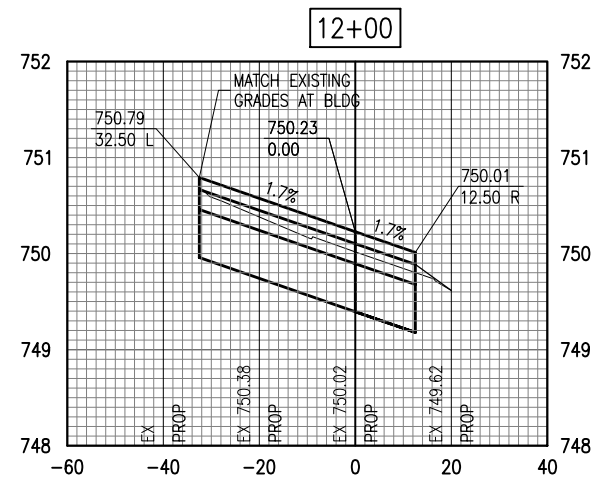
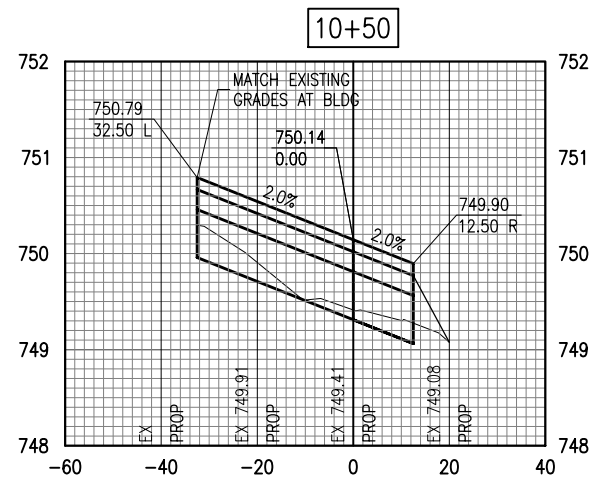
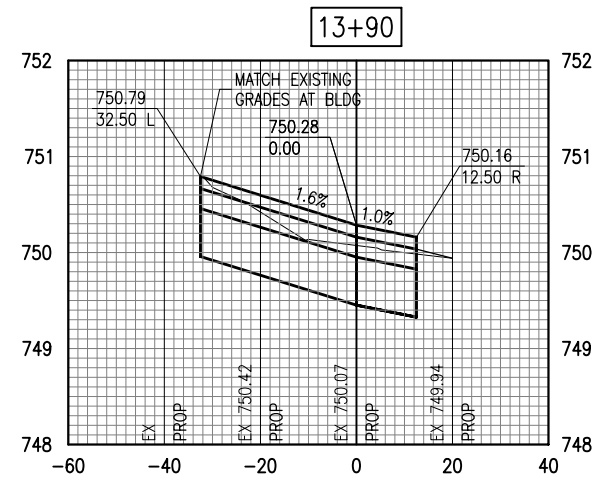
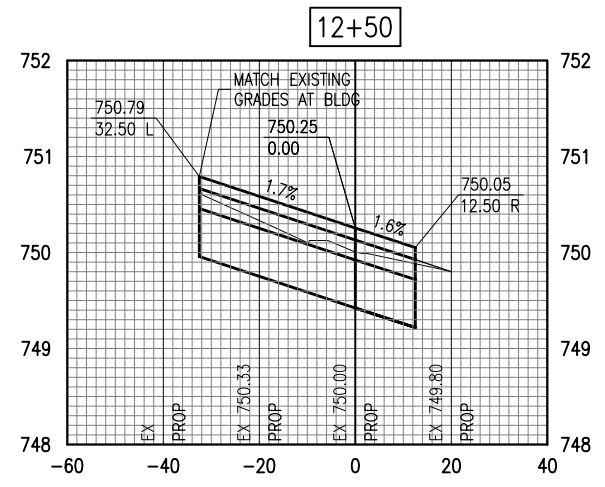
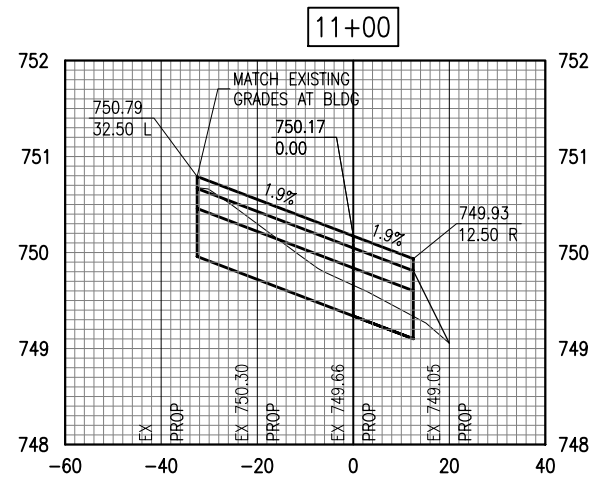
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HEL Project No. 08A0188D_0800	DATE	06/19/09
Filename R-301SECT.DWG	DAK	06/19/09
Scale 1"=20'H 1"=1'V	DAK	06/19/09
Date 06/19/09	RAW	06/19/09
LAYOUT		
DRAWN		
REVIEWED		

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PROPOSED CROSS SECTIONS -
EAST ALIGNMENT
STA. 10+00 TO STA. 13+90



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