

ITEM 8A

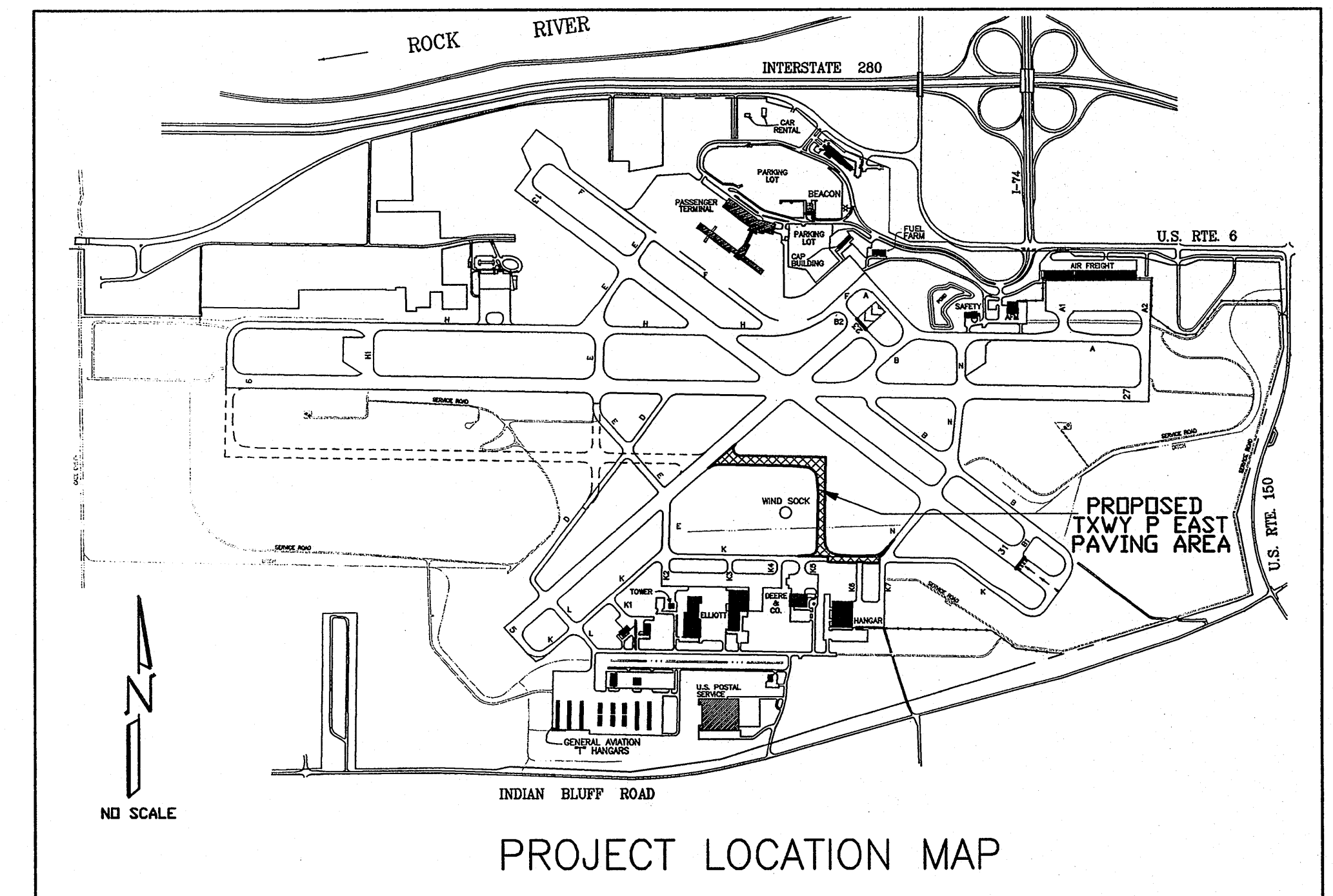
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF AERONAUTICS CONSTRUCTION PLANS

FOR

QUAD-CITY INTERNATIONAL AIRPORT

ROCK ISLAND COUNTY, ILLINOIS

INDEX OF SHEETS	
SHEET NO.	TITLE
1	COVER SHEET AND INDEX OF SHEETS
2	SUMMARY OF QUANTITIES / GENERAL NOTES
3-6	PROPOSED SAFETY PLAN
7-11	TYPICAL SECTIONS
12	PROJECT AREA PLAN
13	TAXIWAY P PLAN & PROFILE, STA 635+80 TO STA 646+60
14	TAXIWAY P PLAN & PROFILE, STA 646+60 TO STA 657+40
15	TAXIWAY P PLAN & PROFILE, STA 657+40 TO STA 668+20
16	SURVEY LINE P2 (EAST CONNECTOR) PLAN & PROFILE
17	TAXIWAY K PLAN & PROFILE, STA 949+20 TO STA 960+80
18-20	TAXIWAY P JOINT PLAN
21	JOINT DETAILS
22-24	TAXIWAY P STAKING PLAN
25-27	DRAINAGE PLAN
28-32	DRAINAGE - PIPE PROFILES
33-35	DRAINAGE DETAILS
36-37	PROPOSED PAVEMENT MARKINGS
38-42	LIGHTING PLAN
43-45	GUIDANCE SIGN PLAN
46-49	AIRFIELD LIGHTING DETAILS
50	ELECTRICAL VAULT MODIFICATION PLAN
51	ELECTRICAL VAULT ONE-LINE RISER DIAGRAM
52-53	ELECTRICAL PANEL AND REGULATOR SCHEDULES
54	MISCELLANEOUS DETAILS
55-56	PROPOSED STORM WATER POLLUTION PREVENTION PLAN
57-66	PROPOSED TAXIWAY P CROSS SECTIONS
67-72	PROPOSED SURVEY LINE P2 (EAST CONNECTOR) CROSS SECTIONS
73-74	PROPOSED TAXIWAY K CROSS SECTIONS



WARNING



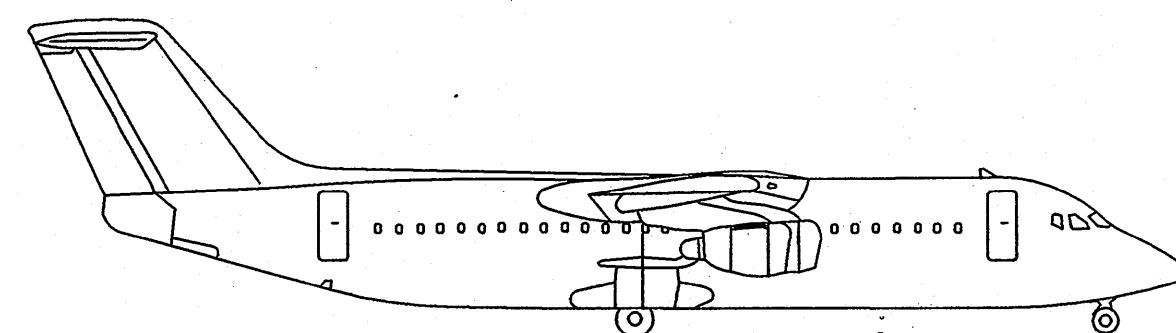
CALL BEFORE
YOU DIG


TAXIWAY P, PHASE IV - EAST PAVING

TAXIWAY P (1,160' X 100' AND 982' X 75') PAVEMENT
AND TAXIWAY K WIDENING (1,560' X 12.5')
INCLUDING EARTHWORK, LIGHTING, UNDERDRAINS, MARKING,
GROOVING, UTILITY ADJUSTMENTS AND TURFING.

ILLINOIS PROJECT MLI-3873
A.I.P. PROJECT NO. 3-17-0068-XX
AIRPORT CLASSIFICATION - AIR CARRIER

AIRCRAFT APPROACH CATEGORY - C
AIRPLANE DESIGN GROUP - III
LATITUDE 41°26'52.4", LONGITUDE 90°30'33.9", ELEVATION 589'M.S.L.



<p>CONSULTING ENGINEERS MISSMAN, STANLEY & ASSOC.</p> <p>ENGINEER'S SIGNATURE <i>Dennis Martin</i></p> <p>SEALED & SIGNED <i>6/23/09</i></p> <p>DATE OF LICENSE EXPIRATION <i>11/30/09</i></p> <p></p>	<p>METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND COUNTY, ILLINOIS</p> <p><i>Bruce Carter</i></p> <p><i>6/25/09</i> DATE APPROVED</p>
<p>JUNE 25, 2009 OFFICIAL DATE OF PLANS</p>	<p>PREPARED BY MISSMAN, STANLEY & ASSOCIATES Consulting Civil Engineers ROCK ISLAND, ILLINOIS</p>

SUMMARY OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITIES	
			AS AWARDED	AS BUILT
AR108158	1/C #8 5 KV UG CABLE IN UD	L.F.	10,100	
AR108258	2/C #8 5 KV UG CABLE IN UD	L.F.	3,980	
AR109210	VAULT MODIFICATIONS	L.S.	1	
AR109341	20 KW REGULATOR, STYLE 1	EACH	1	
AR109361	30 KW REGULATOR, STYLE 1	EACH	2	
AR110014	4" DIRECTIONAL BORE	L.F.	100	
AR110501	1-WAY CONC. ENCASED DUCT	L.F.	515	
AR110502	2-WAY CONCRETE ENCASED DUCT	L.F.	255	
AR110710	ELECTRICAL MANHOLE	EACH	2	
AR110907	REMOVE ELECTRICAL MANHOLE	EACH	1	
AR125415	MITL-BASE MOUNTED	EACH	39	
AR125420	TAXIWAY LIGHT INPAVEMENT	EACH	1	
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	4	
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	3	
AR125447	TAXI GUIDANCE SIGN, 7 CHARACTER	EACH	1	
AR125510	MIRL, BASE MOUNTED	EACH	24	
AR125560	RUNWAY DISTANCE REMAINING SIGN	EACH	1	
AR125565	SPLICE CAN	EACH	2	
AR125942	ADJUST BASE MOUNTED LIGHT	EACH	2	
AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	12	
AR125964	RELOCATE TAXI GUIDANCE SIGN	EACH	3	
AR125984	REFURBISH TAXI GUIDANCE SIGN	EACH	5	
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150530	TRAFFIC MAINTENANCE	L.S.	1	
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	31,050	
AR156500	TEMPORARY EROSION CONTROL	L.S.	1	
AR156540	RIPRAP	S.Y.	250	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	15,900	
AR209600	GEOTEXTILE FABRIC	S.Y.	27,850	
AR401610	BITUMINOUS SURFACE COURSE	TON	550	
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	16	
AR501512	12" PCC PAVEMENT	S.Y.	25,650	
AR501530	PCC TEST BATCH	EACH	1	
AR501540	PCC PAVEMENT GROOVING	S.Y.	21,900	
AR501900	REMOVE PCC PAVEMENT	S.Y.	110	
AR602510	BITUMINOUS PRIME COAT	GAL.	320	
AR603510	BITUMINOUS TACK COAT	GAL.	160	
AR620510	PAVEMENT MARKING	S.F.	16,850	
AR620900	PAVEMENT MARKING REMOVAL	S.F.	120	
AR701518	18" RCP, CLASS IV	L.F.	185	
AR701900	REMOVE PIPE	L.F.	725	
AR705506	6" PERFORATED UNDERDRAIN	L.F.	5,600	
AR705508	8" PERFORATED UNDERDRAIN	L.F.	35	
AR751540	MANHOLE 4'	EACH	4	
AR751560	MANHOLE 6'	EACH	2	
AR751570	MANHOLE - SPECIAL	EACH	12	
AR751900	REMOVE INLET	EACH	2	
AR751903	REMOVE MANHOLE	EACH	3	
AR751943	ADJUST MANHOLE	EACH	1	
AR752412	PRECAST REINFORCED CONC. FES 12"	EACH	1	
AR752418	PRECAST REINFORCED CONC. FES 18"	EACH	2	
AR801605	REPLACE TAXI GUIDANCE SIGN PANEL	EACH	22	
AR801614	SUPPLY TAXI GUIDANCE SIGN PANEL	EACH	20	
AR901510	SEEDING	ACRE	19	
AR908513	MULCHING - METHOD 3	ACRE	19	
AR908520	EXCELSIOR BLANKET	S.Y.	1,500	

GENERAL NOTES:

1. MAXIMUM PAY WIDTH FOR 209510 CRUSHED AGGREGATE BASE COURSE SHALL BE 12 INCHES BEYOND THE EDGE OF PAVEMENT. IF THE CONTRACTOR REQUIRES ADDITIONAL WIDTH FOR PAVEMENT INSTALLATION, THE ADDITIONAL MATERIALS SHALL MEET THE SAME SPECIFICATIONS, BUT WILL BE CONSIDERED INCIDENTAL.
2. THE CONTRACTOR SHALL SALVAGE EXISTING AIRFIELD LIGHTING EQUIPMENT, AS DETAILED IN THE CONSTRUCTION PLANS AND SPECIAL PROVISIONS, PRIOR TO THE START OF EARTHWORK AND/OR PAVING ACTIVITIES. SALVAGED EQUIPMENT SHALL BE CLEANED AND REUSED OR DELIVERED TO THE METROPOLITAN AIRPORT AUTHORITY.
3. CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS APPROVED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
4. THE CONTRACTOR SHALL EXCAVATE TEMPORARY EROSION CONTROL DRAINAGE SWALES, AS REQUIRED BY THE RESIDENT ENGINEER, TO CONTROL STORM WATER RUN-OFF.
5. THE CONTRACT AR152410-UNCLASSIFIED EXCAVATION PAY ITEM SHALL INCLUDE ALL COSTS ASSOCIATED WITH EXCAVATION OF SOILS, HAULING OF SOILS, STOCKPILING SOILS, INSTALLATION OF SOILS, COMPACTING OF SOILS, GRADING OF SOILS, INSTALLATION AND REMOVAL OF HAUL ROADS OR ROUTES, RESTORATION OF HAUL ROADS OR ROUTES, DISPOSAL OF WASTE SOILS, CLEANING OF PAVEMENTS, AND ALL OTHER ITEMS THAT ARE REQUIRED TO COMPLETE THE EARTHWORK. THIS ITEM SHALL BE PAID FOR BASED UPON THE CUBIC YARDS OF MATERIALS EXCAVATED AS ACCEPTED BY THE RESIDENT ENGINEER. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
6. AFTER CONSTRUCTION HAS BEEN COMPLETED, THE CONTRACTOR SHALL SEED AND HYDRAULIC MULCH ALL DISTURBED AREAS PER SPECIAL PROVISIONS 901 AND 908. ONLY SEED AND MULCHING AREAS WITHIN THE LIMITS OF CONSTRUCTION/SEEDING WILL BE ELIGIBLE FOR PAYMENT UNDER THESE CONTRACT PAY ITEMS. DISTURBED AREAS OUTSIDE OF THE LIMITS OF CONSTRUCTION/SEEDING SHALL BE SEEDED AND MULCHED BY THE CONTRACTOR PER SPECIAL PROVISION 901/908, BUT SHALL NOT BE MEASURED FOR PAYMENT.
7. ITEM 908513 MULCHING - METHOD 3 SHALL BE ACCOMPLISHED FOLLOWING THE METHODS AND PROCEDURES OUTLINED IN THE IDOT-DOA SUPPLEMENTAL SPECIFICATIONS FOR HYDRAULIC MULCHING AND IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 251, METHOD 3 (HYDRAULIC MULCH).
8. ITEM 908520 EXCELSIOR BLANKET SHALL BE ACCOMPLISHED FOLLOWING THE METHODS AND PROCEDURES OUTLINED IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 251, EROSION CONTROL BLANKET (EXCELSIOR BLANKET). THE LOCATION OF THE PROPOSED EXCELSIOR BLANKET SHALL BE DETERMINED BY THE RESIDENT ENGINEER, IN THE FIELD, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

THIS PROJECT SHALL NOT START UNTIL THE PROPOSED (BY OTHERS) SOIL SURCHARGE HAS CONSOLIDATED THE EXISTING GROUND SOILS AS DETERMINED BY THE RESIDENT ENGINEER. IT IS ANTICIPATED THAT THE CONSTRUCTION OF THIS PROJECT SHALL NOT OCCUR UNTIL THE 2010 CONSTRUCTION SEASON.

THIS CONTRACTOR SHALL NOT START WORK UNTIL THE MLI-3855 CONTRACTOR (OTHERS) HAS REMOVED THE SOIL SURCHARGE AND STOCKPILED SAND AS SHOWN ON SHEETS 8, 10, 14 AND 16 OF THIS SET OF CONSTRUCTION PLANS. THE MLI-3855 CONTRACTOR (OTHERS) SHALL BE REQUIRED TO REMOVE THE SOIL SURCHARGE AND STOCKPILE SAND WITHIN 30 WORKING DAYS AFTER HIS CONTRACT (MLI-3855) NOTICE TO PROCEED IS ISSUED.

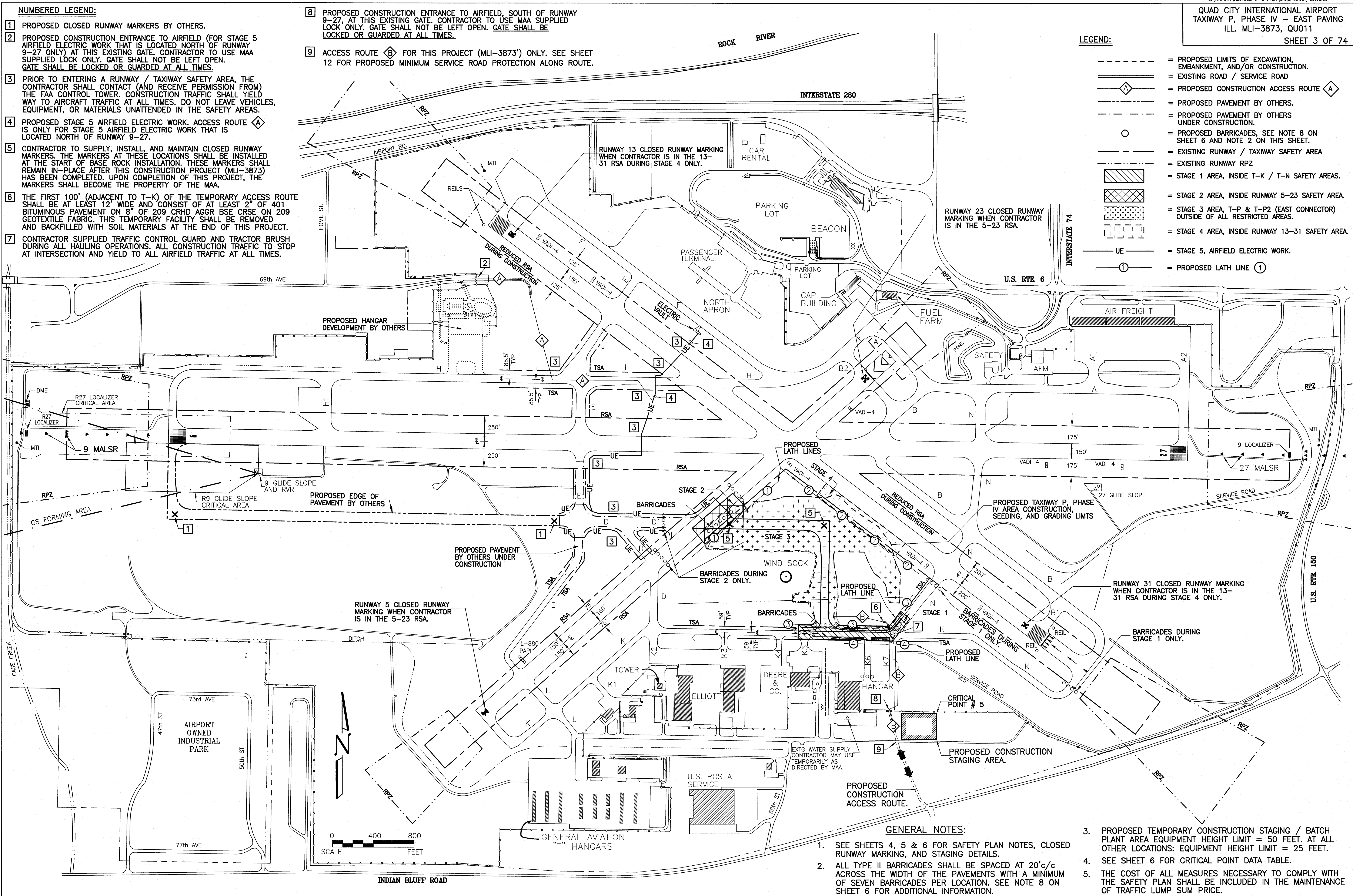
NUMBERED LEGEND:

- 1 PROPOSED CLOSED RUNWAY MARKERS BY OTHERS.
- 2 PROPOSED CONSTRUCTION ENTRANCE TO AIRFIELD (FOR STAGE 5 AIRFIELD ELECTRIC WORK THAT IS LOCATED NORTH OF RUNWAY 9-27 ONLY) AT THIS EXISTING GATE. CONTRACTOR TO USE MAA SUPPLIED LOCK ONLY. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE LOCKED OR GUARDED AT ALL TIMES.
- 3 PRIOR TO ENTERING A RUNWAY / TAXIWAY SAFETY AREA, THE CONTRACTOR SHALL CONTACT (AND RECEIVE PERMISSION FROM) THE FAA CONTROL TOWER. CONSTRUCTION TRAFFIC SHALL YIELD WAY TO AIRCRAFT TRAFFIC AT ALL TIMES. DO NOT LEAVE VEHICLES, EQUIPMENT, OR MATERIALS UNATTENDED IN THE SAFETY AREAS.
- 4 PROPOSED STAGE 5 AIRFIELD ELECTRIC WORK. ACCESS ROUTE IS ONLY FOR STAGE 5 AIRFIELD ELECTRIC WORK THAT IS LOCATED NORTH OF RUNWAY 9-27.
- 5 CONTRACTOR TO SUPPLY, INSTALL, AND MAINTAIN CLOSED RUNWAY MARKERS. THE MARKERS AT THESE LOCATIONS SHALL BE INSTALLED AT THE START OF BASE ROCK INSTALLATION. THESE MARKERS SHALL REMAIN IN-PLACE AFTER THIS CONSTRUCTION PROJECT (MLI-3873) HAS BEEN COMPLETED. UPON COMPLETION OF THIS PROJECT, THE MARKERS SHALL BECOME THE PROPERTY OF THE MAA.
- 6 THE FIRST 100' (ADJACENT TO T-K) OF THE TEMPORARY ACCESS ROUTE SHALL BE AT LEAST 12' WIDE AND CONSIST OF AT LEAST 2" OF 401 BITUMINOUS PAVEMENT ON 8" OF 209 CRHD AGGR BSE CRSE ON 209 GEOTEXTILE FABRIC. THIS TEMPORARY FACILITY SHALL BE REMOVED AND BACKFILLED WITH SOIL MATERIALS AT THE END OF THIS PROJECT.
- 7 CONTRACTOR SUPPLIED TRAFFIC CONTROL GUARD AND TRACTOR BRUSH DURING ALL HAULING OPERATIONS. ALL CONSTRUCTION TRAFFIC TO STOP AT INTERSECTION AND YIELD TO ALL AIRFIELD TRAFFIC AT ALL TIMES.

- 8 PROPOSED CONSTRUCTION ENTRANCE TO AIRFIELD, SOUTH OF RUNWAY 9-27, AT THIS EXISTING GATE. CONTRACTOR TO USE MAA SUPPLIED LOCK ONLY. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE LOCKED OR GUARDED AT ALL TIMES.
- 9 ACCESS ROUTE (A) FOR THIS PROJECT (MLI-3873) ONLY. SEE SHEET 12 FOR PROPOSED MINIMUM SERVICE ROAD PROTECTION ALONG ROUTE.

LEGEND:

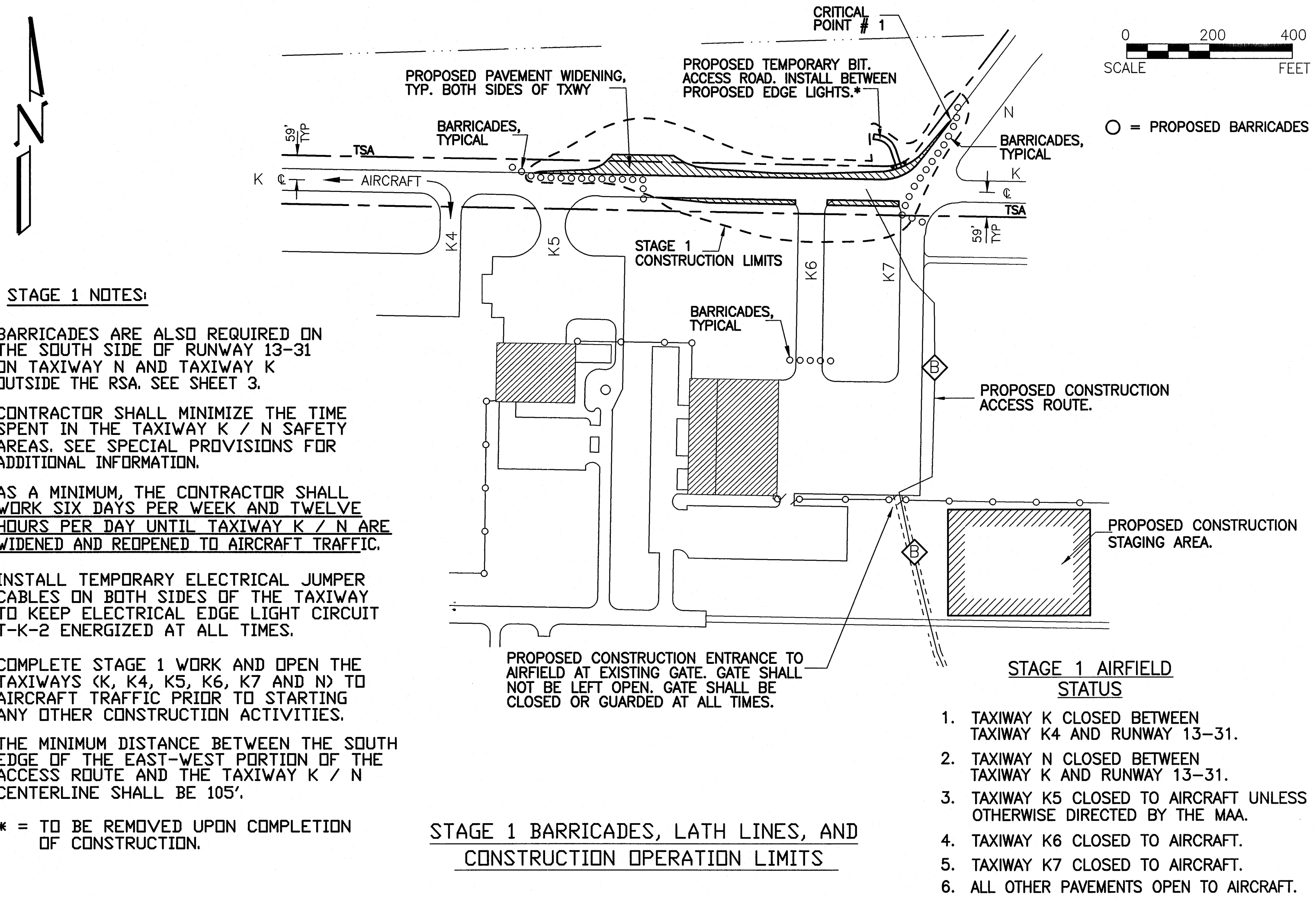
- - - - - = PROPOSED LIMITS OF EXCAVATION, EMBANKMENT, AND/OR CONSTRUCTION.
- — — — — = EXISTING ROAD / SERVICE ROAD
- (A) = PROPOSED CONSTRUCTION ACCESS ROUTE (A)
- - - - - = PROPOSED PAVEMENT BY OTHERS.
- - - - - = PROPOSED PAVEMENT BY OTHERS UNDER CONSTRUCTION.
- = PROPOSED BARRICADES, SEE NOTE 8 ON SHEET 6 AND NOTE 2 ON THIS SHEET.
- - - - - = EXISTING RUNWAY / TAXIWAY SAFETY AREA
- ▨ = EXISTING RUNWAY RPZ
- ▩ = STAGE 1 AREA, INSIDE T-K / T-N SAFETY AREAS.
- ▧ = STAGE 2 AREA, INSIDE RUNWAY 5-23 SAFETY AREA.
- ▦ = STAGE 3 AREA, T-P & T-P2 (EAST CONNECTOR) OUTSIDE OF ALL RESTRICTED AREAS.
- ▤ = STAGE 4 AREA, INSIDE RUNWAY 13-31 SAFETY AREA.
- UE = STAGE 5, AIRFIELD ELECTRIC WORK.
- ① = PROPOSED LATH LINE ①



GENERAL NOTES:

- 1. SEE SHEETS 4, 5 & 6 FOR SAFETY PLAN NOTES, CLOSED RUNWAY MARKING, AND STAGING DETAILS.
- 2. ALL TYPE II BARRICADES SHALL BE SPACED AT 20'C/C ACROSS THE WIDTH OF THE PAVEMENTS WITH A MINIMUM OF SEVEN BARRICADES PER LOCATION. SEE NOTE 8 ON SHEET 6 FOR ADDITIONAL INFORMATION.
- 3. PROPOSED TEMPORARY CONSTRUCTION STAGING / BATCH PLANT AREA EQUIPMENT HEIGHT LIMIT = 50 FEET. AT ALL OTHER LOCATIONS: EQUIPMENT HEIGHT LIMIT = 25 FEET.
- 4. SEE SHEET 6 FOR CRITICAL POINT DATA TABLE.
- 5. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.

G:\Airport\A09T002 TP E P\MT\Safety.dwg, 7/8/2009 10:12:09 AM, jefm



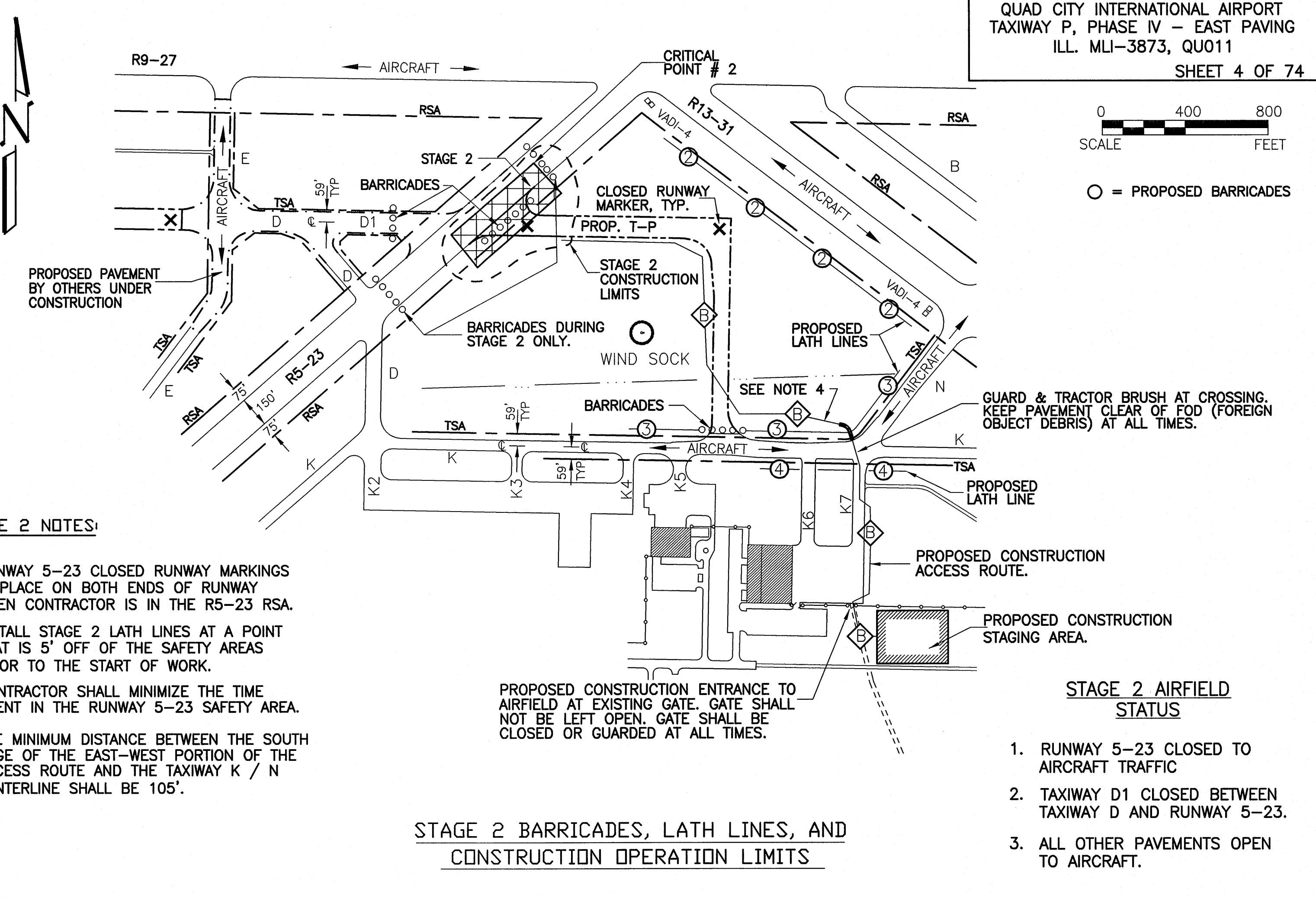
STAGE 1 NOTES:

1. BARRICADES ARE ALSO REQUIRED ON THE SOUTH SIDE OF RUNWAY 13-31 ON TAXIWAY N AND TAXIWAY K OUTSIDE THE RSA. SEE SHEET 3.
2. CONTRACTOR SHALL MINIMIZE THE TIME SPENT IN THE TAXIWAY K / N SAFETY AREAS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
3. AS A MINIMUM, THE CONTRACTOR SHALL WORK SIX DAYS PER WEEK AND TWELVE HOURS PER DAY UNTIL TAXIWAY K / N ARE WIDENED AND REOPENED TO AIRCRAFT TRAFFIC.
4. INSTALL TEMPORARY ELECTRICAL JUMPER CABLES ON BOTH SIDES OF THE TAXIWAY TO KEEP ELECTRICAL EDGE LIGHT CIRCUIT T-K-2 ENERGIZED AT ALL TIMES.
5. COMPLETE STAGE 1 WORK AND OPEN THE TAXIWAYS (K, K4, K5, K6, K7 AND N) TO AIRCRAFT TRAFFIC PRIOR TO STARTING ANY OTHER CONSTRUCTION ACTIVITIES.
6. THE MINIMUM DISTANCE BETWEEN THE SOUTH EDGE OF THE EAST-WEST PORTION OF THE ACCESS ROUTE AND THE TAXIWAY K / N CENTERLINE SHALL BE 105'.
7. * = TO BE REMOVED UPON COMPLETION OF CONSTRUCTION.

STAGE 1 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

STAGE 1 AIRFIELD STATUS

1. TAXIWAY K CLOSED BETWEEN TAXIWAY K4 AND RUNWAY 13-31.
2. TAXIWAY N CLOSED BETWEEN TAXIWAY K AND RUNWAY 13-31.
3. TAXIWAY K5 CLOSED TO AIRCRAFT UNLESS OTHERWISE DIRECTED BY THE MAA.
4. TAXIWAY K6 CLOSED TO AIRCRAFT.
5. TAXIWAY K7 CLOSED TO AIRCRAFT.
6. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT.



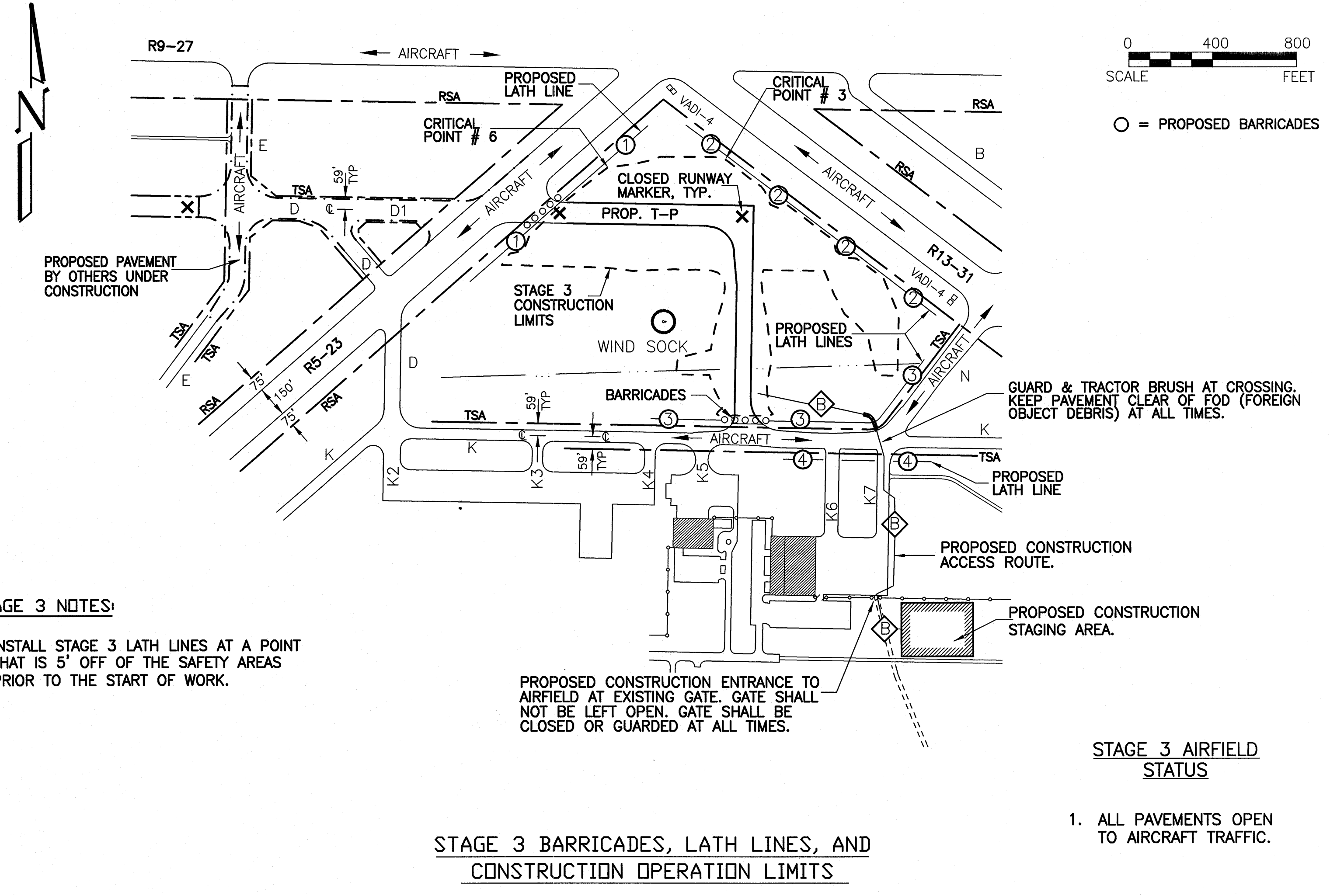
STAGE 2 NOTES:

1. RUNWAY 5-23 CLOSED RUNWAY MARKINGS IN PLACE ON BOTH ENDS OF RUNWAY WHEN CONTRACTOR IS IN THE R5-23 RSA.
2. INSTALL STAGE 2 LATH LINES AT A POINT THAT IS 5' OFF OF THE SAFETY AREAS PRIOR TO THE START OF WORK.
3. CONTRACTOR SHALL MINIMIZE THE TIME SPENT IN THE RUNWAY 5-23 SAFETY AREA.
4. THE MINIMUM DISTANCE BETWEEN THE SOUTH EDGE OF THE EAST-WEST PORTION OF THE ACCESS ROUTE AND THE TAXIWAY K / N CENTERLINE SHALL BE 105'.

STAGE 2 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

STAGE 2 AIRFIELD STATUS

1. RUNWAY 5-23 CLOSED TO AIRCRAFT TRAFFIC
2. TAXIWAY D1 CLOSED BETWEEN TAXIWAY D AND RUNWAY 5-23.
3. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT.



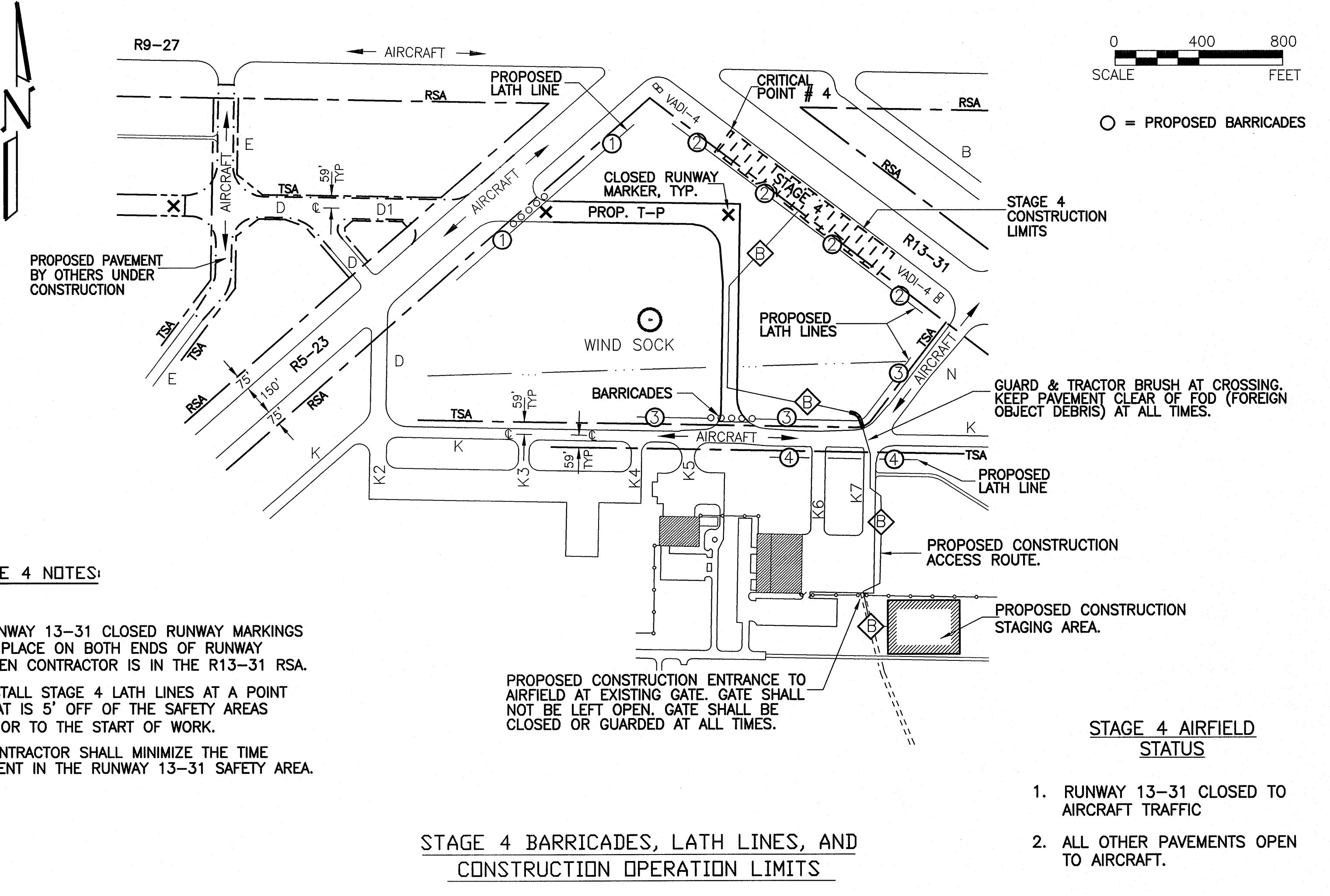
STAGE 3 NOTES:

1. INSTALL STAGE 3 LATH LINES AT A POINT THAT IS 5' OFF OF THE SAFETY AREAS PRIOR TO THE START OF WORK.

STAGE 3 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

STAGE 3 AIRFIELD STATUS

1. ALL PAVEMENTS OPEN TO AIRCRAFT TRAFFIC.



STAGE 4 NOTES:

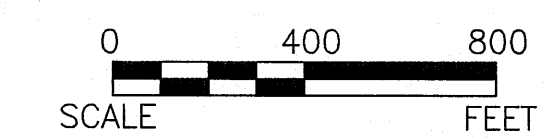
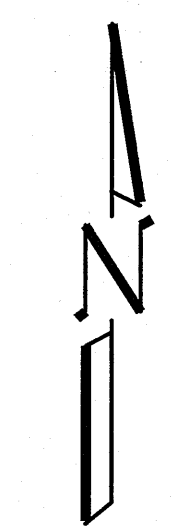
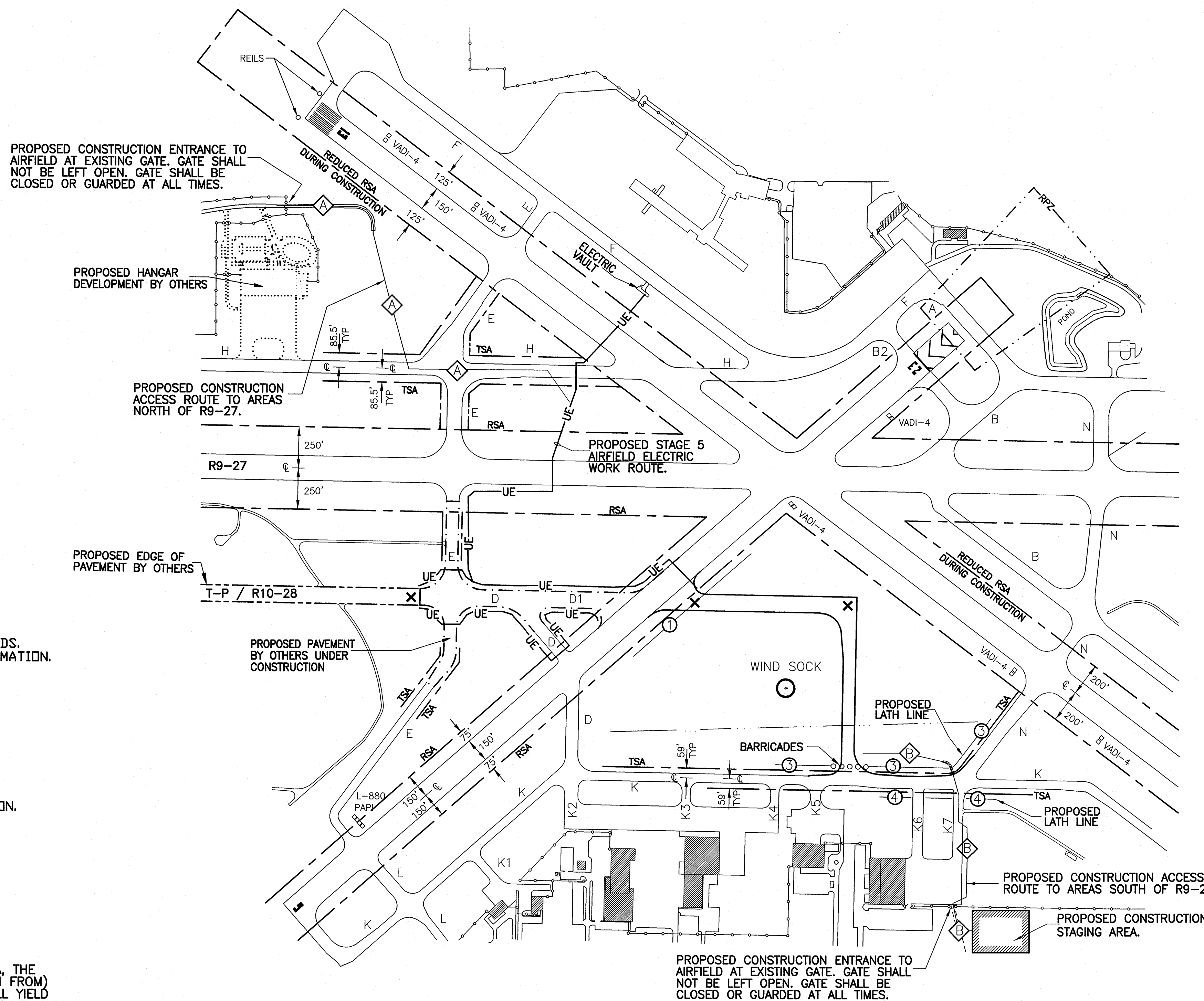
1. RUNWAY 13-31 CLOSED RUNWAY MARKINGS IN PLACE ON BOTH ENDS OF RUNWAY WHEN CONTRACTOR IS IN THE R13-31 RSA.
2. INSTALL STAGE 4 LATH LINES AT A POINT THAT IS 5' OFF OF THE SAFETY AREAS PRIOR TO THE START OF WORK.
3. CONTRACTOR SHALL MINIMIZE THE TIME SPENT IN THE RUNWAY 13-31 SAFETY AREA.

STAGE 4 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

STAGE 4 AIRFIELD STATUS

1. RUNWAY 13-31 CLOSED TO AIRCRAFT TRAFFIC
2. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT.

G:\airport\A09T002 TP E PVMT SAFETY.dwg, 7/8/2009 10:12:18 AM, jefm



○ = PROPOSED BARRICADES

STAGE 5 NOTES:

1. PROPOSED WORK IN THE RUNWAY / TAXIWAY SAFETY AREAS SHALL BE BROKEN DOWN INTO UNITS OF WORK WHICH CAN BE ACCOMPLISHED IN SEPARATE DAY TIME WORK PERIODS. THE MAA RESERVES AND SHALL HAVE THE RIGHT TO STOP WORK AND REDPEN THE RUNWAYS / TAXIWAYS AT ANY TIME DURING THE WORK PERIODS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. AT THE END OF EVERY WORK PERIOD, THE CONTRACTOR SHALL REMOVE ALL ITEMS FROM THE SAFETY / CRITICAL AREAS, BACKFILL ALL EXCAVATIONS / TRENCHES, REGRADE THE SAFETY / CRITICAL AREAS AND REDPEN THE PAVEMENTS TO AIRCRAFT TRAFFIC. THE RUNWAYS AND TAXIWAYS SHALL BE OPEN TO AIRCRAFT TRAFFIC AT ALL TIMES WHEN THE CONTRACTOR IS NOT IN THE SAFETY / CRITICAL AREAS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
3. CONTRACTOR SHALL MINIMIZE THE TIME SPENT IN THE RUNWAY / TAXIWAY SAFETY AREAS.
4. RUNWAY 13-31 SHALL BE OPEN TO AIRCRAFT WHEN RUNWAY 9-27 IS CLOSED.
5. RUNWAY 9-27 SHALL BE OPEN TO AIRCRAFT WHEN RUNWAY 13-31 IS CLOSED.
6. PRIOR TO ENTERING A RUNWAY / TAXIWAY SAFETY AREA, THE CONTRACTOR SHALL CONTACT (AND RECEIVE PERMISSION FROM) THE FAA CONTROL TOWER. CONSTRUCTION TRAFFIC SHALL YIELD WAY TO AIRCRAFT TRAFFIC AT ALL TIMES. DO NOT LEAVE VEHICLES, EQUIPMENT, OR MATERIALS UNATTENDED IN THE SAFETY AREAS.
6. THE CONTRACTOR SHALL BE IN CONTACT WITH THE FAA CONTROL TOWER AT ALL TIMES. IF ISSUED INSTRUCTIONS BY THE FAA CONTROL TOWER, THE CONTRACTOR SHALL FOLLOW THE INSTRUCTIONS WITHOUT HESITATION OR DELAY.
7. AT THE END OF EVERY WORK PERIOD, THE CONTRACTOR SHALL NOT LEAVE THE AIRFIELD UNTIL ALL PAVEMENTS SCHEDULED TO BE REOPENED ARE AVAILABLE TO AIRCRAFT AND PERMISSION TO LEAVE HAS BEEN GIVEN BY THE RESIDENT ENGINEER.
8. THE CONTRACTOR SHALL NOT CROSS RUNWAY 9-27 UNLESS RUNWAY 9-27 HAS BEEN CLOSED BY THE MAA. WHEN RUNWAY 9-27 IS OPEN TO AIRCRAFT TRAFFIC, THE CONTRACTOR SHALL USE THE MILAN BELTWAY / INDIAN BLUFF ROAD TO DRIVE AROUND THE AIRFIELD.

STAGE 5 AIRFIELD STATUS

1. RUNWAY 9-27, RUNWAY 13-31, RUNWAY 5-23, TAXIWAY D (BETWEEN T-E & R5-23), TAXIWAY D1, TAXIWAY E (BETWEEN R9-27 & R5-23), AND TAXIWAY H (BETWEEN T-E & R5-23) :

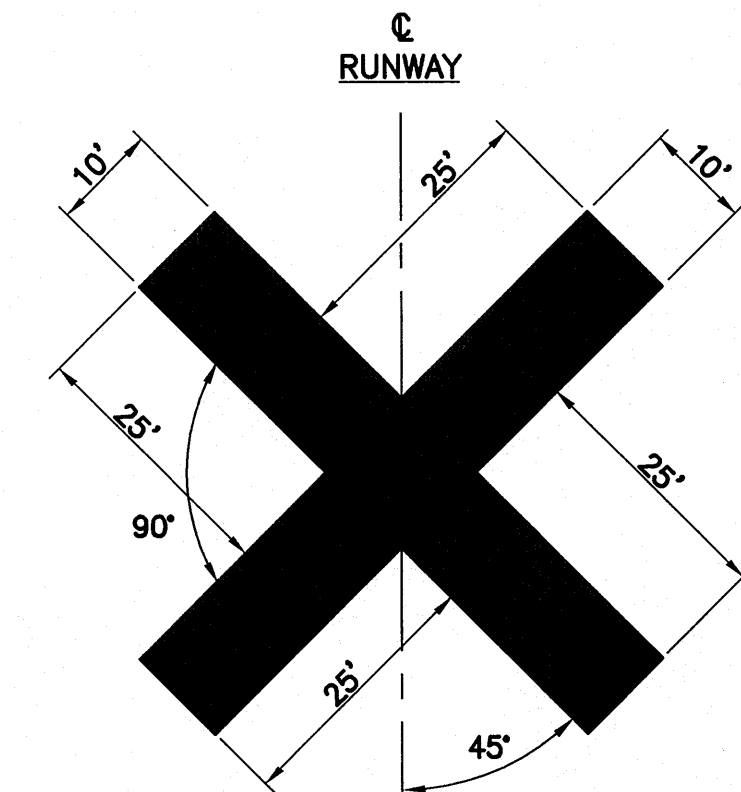
THESE PAVEMENTS WILL BE CLOSED (ONE AT A TIME) TO AIRCRAFT TRAFFIC BY THE MAA DURING DAY TIME CONSTRUCTION WORK PERIODS ONLY. THE WORK PERIODS WILL BE SELECTED BY THE MAA BASED ON WEATHER AND AIR TRAFFIC CONDITIONS. THESE PAVEMENTS SHALL BE OPENED TO AIRCRAFT TRAFFIC BETWEEN THE DAY TIME WORK PERIODS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

2. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT TRAFFIC.

STAGE 5 CONSTRUCTION OPERATION LIMITS

SAFETY PLAN NOTES:

- THE METROPOLITAN AIRPORT AUTHORITY (MAA) SHALL DETERMINE WHEN AND WHERE THE CONTRACTOR WILL BE ALLOWED TO WORK. THE CONTRACTOR SHALL BE PREPARED TO EXIT THE RESTRICTED AREAS, SAFETY AREAS AND/OR AIR FIELD AT ALL TIMES WHEN DIRECTED TO DO SO BY THE MAA. ALL PAVEMENTS SHALL BE OPEN TO AIRCRAFT TRAFFIC UNLESS "NOTAMED" OTHERWISE DURING THE CONSTRUCTION OF THIS PROJECT. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION ON WORKING RESTRICTIONS AND CONDITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF ALL BARRICADES AND LATH LINES AS SHOWN, PRIOR TO ANY EQUIPMENT ENTERING THE AIRFIELD OUTSIDE OF THE CONSTRUCTION STAGING AREA.
- SOLID CLOSED RUNWAY MARKING LINES CROSSES ARE REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES. SEE SPECIAL PROVISIONS FOR DETAILS OF CROSSES.
- THE CONTRACTOR SHALL NOT TRAVEL IN OR THROUGH THE RESTRICTED AREAS AND/OR SAFETY AREAS UNLESS PERMISSION IS RECEIVED AND CONTACT HAS BEEN MADE WITH THE FAA CONTROL TOWER.
- THE CONTRACTOR'S EMPLOYEES SHALL PARK IN THE PROPOSED CONSTRUCTION STAGING AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO THE PROPOSED CONSTRUCTION SITE. ONLY CONTRACTOR'S MARKED VEHICLES AND EQUIPMENT SHALL BE ALLOWED ON THE AIRFIELD. ALL CONSTRUCTION VEHICLES AND EQUIPMENT, EXCEPT THE PAVING TRAIN, SHALL BE PARKED IN THE CONSTRUCTION STAGING AREA DURING ALL NON-WORKING HOUR. THE PAVING TRAIN MAY BE PARKED ON THE AIRFIELD OUTSIDE ALL RESTRICTED AREAS IN A LOCATION AUTHORIZED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL LOCATE HIS TRAILER, THE ENGINEERS FIELD OFFICE, AND ALL OTHER NECESSARY FACILITIES AND MATERIALS IN THE PROPOSED CONSTRUCTION STAGING AREA. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONSTRUCT WHATEVER ACCESS ROAD HE DEEMS NECESSARY BETWEEN THE EXISTING ROADS AND THE CONSTRUCTION AREAS. OVERNIGHT PARKING OF EMPLOYEE VEHICLES IN THE CONSTRUCTION STAGING AREA WILL NOT BE ALLOWED UNLESS AUTHORIZED BY THE MAA.
- THE CONTRACTOR SHALL PROCURE ENOUGH QUAD CITY AIRPORT SECURITY/IDENTIFICATION BADGES FOR HIS EMPLOYEES AND SUBCONTRACTOR EMPLOYEES FROM THE AIRPORT AUTHORITY TO GUARANTEE AT LEAST ONE MEMBER OF EACH CONSTRUCTION CREW WILL HAVE A BADGE. ANY CONSTRUCTION CREW WITHOUT A BADGED MEMBER SHALL NOT BE ALLOWED ON THE AIRFIELD SITE. ALL INDIVIDUALS WHO ARE ISSUED SECURITY BADGES MUST CORRECTLY WEAR THEIR OWN BADGE WHILE ON THE AIRFIELD. BADGES MAY BE OBTAINED, AFTER MEETING SECURITY REQUIREMENTS, FROM THE SECURITY OFFICE AT THE QCI AIRPORT. A FIFTY DOLLAR (\$50.00) REFUNDABLE DEPOSIT IS REQUIRED FOR EACH BADGE.
- THE CONTRACTOR SHALL USE THE DESIGNATED HAUL ROUTES, CONSTRUCTION STAGING AREA, AND ENTRANCE TO THE AIRFIELD AS SHOWN ON THE SAFETY PLAN SHEET. NO CONSTRUCTION TRAFFIC SHALL BE ALLOWED ON THE AIRFIELD OUTSIDE THE HAUL ROUTES AND WORK AREAS UNLESS OTHERWISE AUTHORIZED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL SUPPLY AND INSTALL TEMPORARY LOCKS ON EXISTING GATES AT THE PROPOSED CONSTRUCTION ENTRANCE. THE CONTRACTOR SHALL BE RESPONSIBLE TO KEEP THE CONSTRUCTION ENTRANCE GATE CLOSED AT ALL TIMES. VIOLATIONS ARE SUBJECT TO FINES/PENALTIES AND THE CONTRACTOR SHALL PAY ANY FINES INCURRED, INCLUDING FINES INCURRED BY THE RESIDENT ENGINEER AND/OR MAA DUE TO THE CONTRACTOR'S NEGLIGENCE. ALL VEHICULAR TRAFFIC SHALL BE KEPT TO A MINIMUM. ALL VEHICLES ON THE APRONS, RAMPS, TAXIWAYS, OR RUNWAYS REQUIRE THE APPROVAL OF THE RESIDENT ENGINEER / MAA.
- THE CONTRACTOR AND EACH SUBCONTRACTOR SHALL DESIGNATE, PRIOR TO BEGINNING CONSTRUCTION, A PERSON OR PERSONS WHO CAN BE CONTACTED IN AN EMERGENCY INVOLVING THEIR WORK OR EQUIPMENT. THESE DESIGNATED PEOPLE SHALL BE AVAILABLE ON A 24-HOUR / 7 DAYS PER WEEK BASIS.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO INSTALL TYPE II BARRICADES EQUIPPED WITH FLASHING RED LIGHTS AND 20" X 20" ORANGE FLAGS AS SHOWN ON THE SAFETY PLAN OR AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR WILL FURNISH, MAINTAIN, AND MOVE THE BARRICADES AS REQUIRED BY THE RESIDENT ENGINEER. THE BARRICADES SHALL BE SUFFICIENTLY WEIGHTED WITH SANDBAGS OR OTHER APPROPRIATE METHOD TO WITHSTAND HIGH WINDS AND/OR JET BLAST WITHOUT DISLOCATION. BARRICADES SHALL BE CHECKED DAILY BY THE CONTRACTOR FOR PROPER PLACEMENT, ADEQUATE BALLAST, PROPER LIGHTING, PROPER FLAGGING AND WORKING BATTERIES. ALL DISCREPANCIES SHALL BE CORRECTED IMMEDIATELY. BARRICADES SHALL BE RELOCATED UPON THE COMPLETION OF EACH STAGE OF CONSTRUCTION. COST TO BE INCLUDED IN MAINTENANCE OF TRAFFIC LUMP SUM PRICE.
- WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRFIELD, THEY SHALL BE PROPERLY MARKED. THE MARKING SHALL CONSIST OF A THREE FOOT (3') SQUARE FLAG WITH A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES OF NOT LESS THAN ONE FOOT (1') ON EACH SIDE, DISPLAYED IN FULL VIEW ABOVE THE VEHICLE OR EQUIPMENT. EACH VEHICLE SHALL HAVE A FLASHING YELLOW LIGHT MOUNTED ON TOP OF THE ROOF.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO RESTORE THE CONSTRUCTION STAGING AREA, SERVICE ROADS, ACCESS ROADS, AND HAUL ROADS TO THEIR ORIGINAL CONDITIONS FOLLOWING COMPLETION OF CONSTRUCTION. THIS MAY INCLUDE, BUT NOT BE LIMITED TO, REGRADING, FERTILIZING, SEEDING AND MULCHING OF EARTH SURFACES AND/OR REGRADING, GRAVELING & SEAL COATING OF TREATED SURFACES, AS REQUIRED, TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL GROUND SURFACES, GRAVEL ROADS, PAVEMENTS, AND OTHER FACILITIES DAMAGED BY THE CONTRACTOR WHILE COMPLETING THE PROPOSED WORK SHALL BE REPAIRED OR RETURNED TO ITS ORIGINAL STATE. COST TO BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.
- THE CONTRACTOR SHALL IMMEDIATELY SWEEP OR PICK UP ANY SOIL, DEBRIS, AGGREGATE CHIPS OR ROCK, OR LOOSE MATERIALS WHICH HAS BEEN DROPPED ONTO AIRPORT ROADS, RUNWAYS, TAXIWAYS, OR SODDED AREAS.
- THE DISPOSAL OF ALL MATERIALS NOT TO BE INCORPORATED IN EMBANKMENTS ON THE PROJECT SHALL BE ACCOMPLISHED BY THE CONTRACTOR AT A LOCATION OFF AIRPORT PROPERTY.
- THE SEQUENCE OF CONSTRUCTION OPERATIONS AND DESCRIPTION OF CONDITIONS ARE OUTLINED IN THE PROJECT SPECIAL PROVISIONS. THE CONTRACTOR SHALL GIVE THE MAA A 96 HOUR NOTICE PRIOR TO THE START OF ANY WORK REQUIRING THE CLOSING OF ANY PAVEMENTS TO AIRCRAFT TRAFFIC SO THAT A NOTAM CAN BE ISSUED.
- WORKING HOURS OF THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL CONFORM TO ALL APPLICABLE LOCAL LAWS, INCLUDING ANY NOISE CONTROL.
- NO MOUNDS OF DIRT OR IRREGULARITIES GREATER THAN 3" WHICH, IN THE OPINION OF THE RESIDENT ENGINEER, COULD INTERFERE WITH ANY AIRFIELD OPERATIONS WILL BE PERMITTED ON THE AIRFIELD. NO EXPOSED FACES IN EXCESS OF ONE AND ONE-HALF (1-1/2) INCHES IN HEIGHT AND 2:1 SLOPES ON ANY EXCAVATION WILL BE PERMITTED WITHIN THE RESTRICTED AREAS.
- DUST ABATEMENT MEASURES WILL BE REQUIRED, WHEN IN THE OPINION OF THE RESIDENT ENGINEER, A HAZARD TO AIR TRAFFIC, LOCAL RESIDENCES, OR CONSTRUCTION PROJECT PERSONNEL EXISTS. PREVENTIVE MEASURES TO BE ACCOMPLISHED BY THE CONTRACTOR SHALL INCLUDE, BUT NOT BE LIMITED TO, WATERING AND TREATMENT WITH CALCIUM CHLORIDE.
- BY THE END OF EACH WORK DAY AND PRIOR TO LEAVING THE AIRFIELD, THE CONTRACTOR SHALL HAVE THOROUGHLY SWEEPED THE AIR TRAFFIC CORRIDORS ADJACENT TO THE WORK AREAS TO REMOVE DUST AND DEBRIS. IN ADDITION, ALL AIR TRAFFIC AREAS USED BY CONSTRUCTION PERSONNEL AND EQUIPMENT MUST BE CONTINUOUSLY SWEEPED AND MAINTAINED FREE OF DEBRIS. SWEEPERS SHALL BE PROVIDED BY THE CONTRACTOR FOR THE ENTIRE LENGTH OF THE CONTRACT AND SHALL BE OF A TYPE CAPABLE OF REMOVING ALL DUST AND DEBRIS TO THE SATISFACTION OF THE MAA. SWEEPERS MUST BE COMMERCIAL QUALITY AND APPROVED BY THE RESIDENT ENGINEER AND MAA PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL INSTALL AND MAINTAIN LATH LINES DURING THE LENGTH OF THE PROJECT AS SHOWN OR DIRECTED BY THE RESIDENT ENGINEER. SEE PROJECT SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
- NO CHANGES SHALL BE MADE IN ANY PROVISIONS OF THIS SAFETY PLAN UNLESS APPROVED IN WRITING BY THE METROPOLITAN AIRPORT AUTHORITY, THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF AERONAUTICS, AND THE FEDERAL AVIATION ADMINISTRATION. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.



CLOSED RUNWAY MARKING
DETAIL

CLOSED RUNWAY MARKINGS NOTES:

- SOLID CROSS FOR CLOSED RUNWAY IS REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES AND SHALL BE PLACED DIRECTLY OVER THE RUNWAY IDENTIFICATION NUMERALS.
- THE CONTRACTOR SHALL INSTALL, REMOVE AND REINSTALL THE CROSSES AS REQUIRED BY WORKING CONDITIONS AND AS APPROVED BY THE RESIDENT ENGINEER.
- COLOR OF ALL CROSSES SHALL BE AVIATION YELLOW.
- SOLID CROSSES ARE TO BE CONSTRUCTED OF PLYWOOD, CANVAS, OR ANY OTHER APPROVED SOLID MATERIALS AND SHALL BE ADHERED TO THE RUNWAY IN A MANNER ACCEPTABLE TO THE RESIDENT ENGINEER. THE CONTRACTOR SHALL MAINTAIN THE CROSSES IN EXCELLENT CONDITION.

CRITICAL POINT DATA

NUMBER	LOCATION	LATITUDE	LONGITUDE	GROUND ELEVATION	
				EXISTING	PROPOSED
1	NE CORNER OF STAGE 1 AREA	41D 26' 39.42"	90D 30' 02.92"	574'	574'
2	NW CORNER OF STAGE 2 AREA	41D 26' 51.31"	90D 30' 24.60"	583'	583'
3	NE CORNER OF STAGE 3 AREA	41D 26' 50.89"	90D 30' 14.01"	577'	577'
4	NE CORNER OF STAGE 4 AREA	41D 26' 52.22"	90D 30' 12.56"	579.6'	579.6'
5	NE CORNER OF BATCH PLANT STAGING AREA	41D 26' 29.84"	90D 29' 58.47"	579'	579'
6	NW CORNER OF STAGE 3 AREA	41D 26' 50.44"	90D 30' 22.36"	580.5'	580.5'

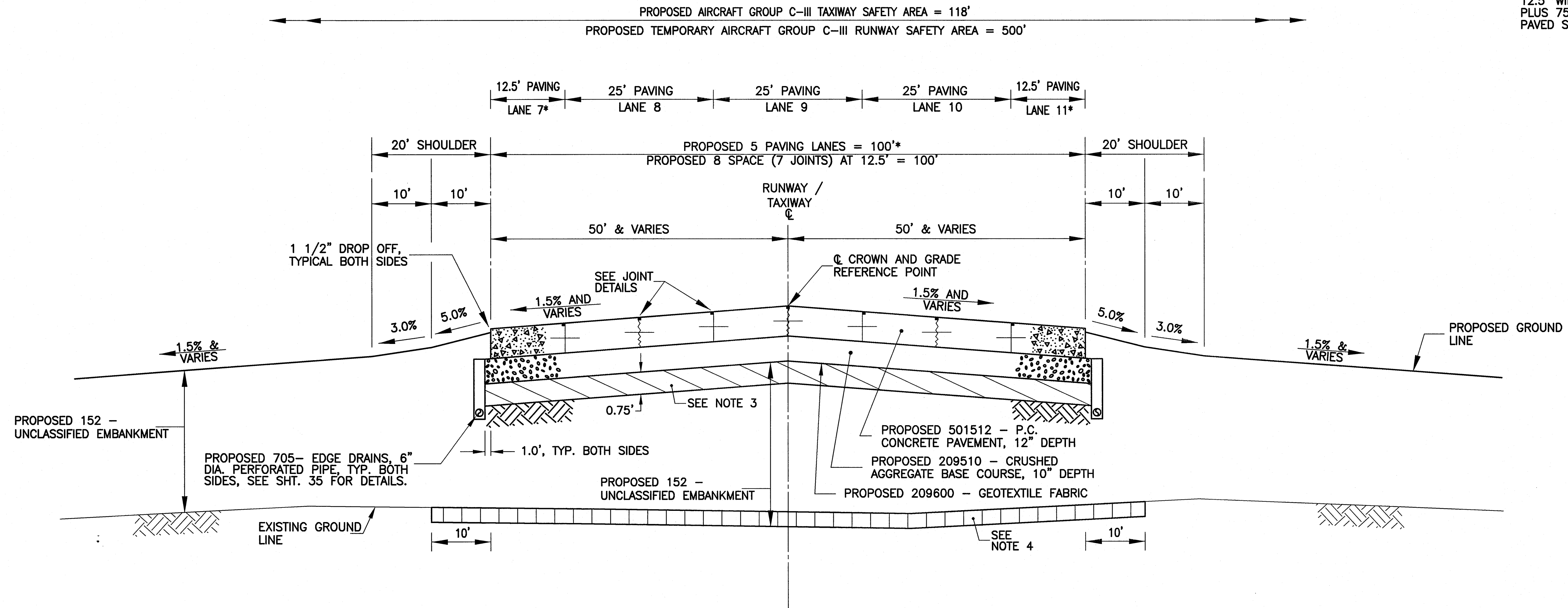
PROPOSED TYPICAL SECTION - PCC PAVING

TAXIWAY P / TEMPORARY RUNWAY 10-28, STA. 642+36.56 (EAST EDGE OF RUNWAY 5-23) TO STA. 647+20

AIRCRAFT APPROACH CATEGORY C
AIRPLANE DESIGN GROUP III
NON-PRECISION RUNWAY

PROPOSED TEMPORARY RUNWAY 10-28
WIDTH = 100' PAVED SURFACE.

PROPOSED TAXIWAY P = 2 EACH
12.5' WIDE PAVED SHOULDERS
PLUS 75' WIDE TAXIWAY = 100'
PAVED SURFACE.

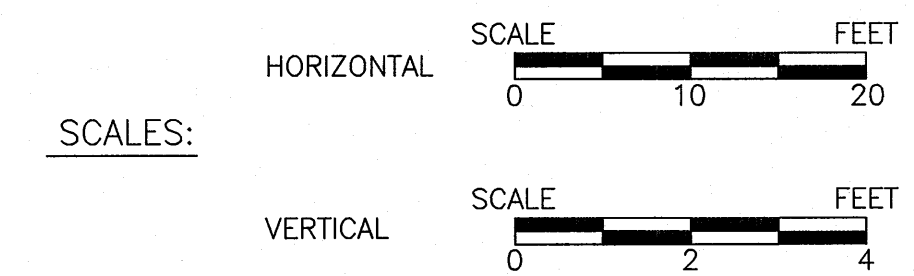


NOTES:

- SEE SHEET 9 FOR CONTINUATION OF EARTHWORK OUTSIDE OF THE PAVING AREA.
- * = IF REQUIRED BY THE FAA, A FUTURE PROJECT COULD REMOVE THE OUTSIDE 12.5' LANES (7 AND 11) TO CREATE A TAXIWAY PAVEMENT WIDTH OF 75'.
- STA. 642+44 (EAST SIDE OF R5) TO STA. 647+20 ONLY: INSTALL COMPACTED SAND FILL MATERIALS UNDER (AND WITHIN 1.0' OF) THE PAVEMENT FROM A POINT 1.83' BELOW THE PAVEMENT SURFACE TO A POINT 2.58' BELOW THE PAVEMENT SURFACE.
- THE EXISTING GROUND MATERIALS UNDER THE PAVEMENT SURFACE SHALL BE REMOVED TO A DEPTH OF 6" FROM THE EXISTING SURFACE PRIOR TO PLACEMENT OF EMBANKMENT MATERIALS. THE SURFACE MATERIALS SHALL BE INCORPORATED INTO THE SHOULDER FILL AREAS. THE COST OF GROUND STRIPPING MATERIALS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

SOIL MATERIAL LEGEND:

- COMPACTED SAND FILL MATERIALS
- COMPACTED SAND OR CLAY FILL MATERIALS
- GROUND STRIPPING MATERIALS

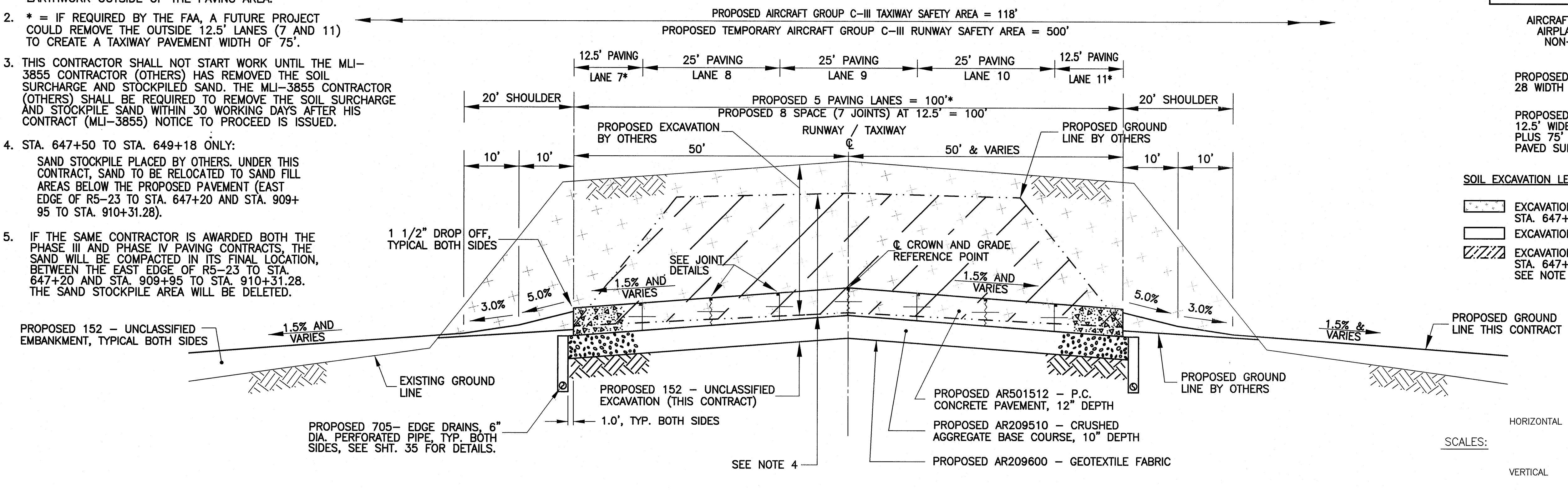


TYPICAL SECTIONS

NOTES:

- SEE SHEET 9 FOR CONTINUATION OF EARTHWORK OUTSIDE OF THE PAVING AREA.
- * = IF REQUIRED BY THE FAA, A FUTURE PROJECT COULD REMOVE THE OUTSIDE 12.5' LANES (7 AND 11) TO CREATE A TAXIWAY PAVEMENT WIDTH OF 75'.
- THIS CONTRACTOR SHALL NOT START WORK UNTIL THE MLI-3855 CONTRACTOR (OTHERS) HAS REMOVED THE SOIL SURCHARGE AND STOCKPILED SAND. THE MLI-3855 CONTRACTOR (OTHERS) SHALL BE REQUIRED TO REMOVE THE SOIL SURCHARGE AND STOCKPILE SAND WITHIN 30 WORKING DAYS AFTER HIS CONTRACT (MLI-3855) NOTICE TO PROCEED IS ISSUED.
- STA. 647+50 TO STA. 649+18 ONLY:
SAND STOCKPILE PLACED BY OTHERS. UNDER THIS CONTRACT, SAND TO BE RELOCATED TO SAND FILL AREAS BELOW THE PROPOSED PAVEMENT (EAST EDGE OF R5-23 TO STA. 647+20 AND STA. 909+95 TO STA. 910+31.28).
- IF THE SAME CONTRACTOR IS AWARDED BOTH THE PHASE III AND PHASE IV PAVING CONTRACTS, THE SAND WILL BE COMPACTED IN ITS FINAL LOCATION, BETWEEN THE EAST EDGE OF R5-23 TO STA. 647+20 AND STA. 909+95 TO STA. 910+31.28. THE SAND STOCKPILE AREA WILL BE DELETED.

PROPOSED TYPICAL SECTION - PCC PAVING
TAXIWAY P / TEMPORARY RUNWAY 10-28, STA. 647+20 TO STA. 653+51



QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE IV - EAST PAVING
ILL. MLI-3873, QU011
SHEET 8 OF 74

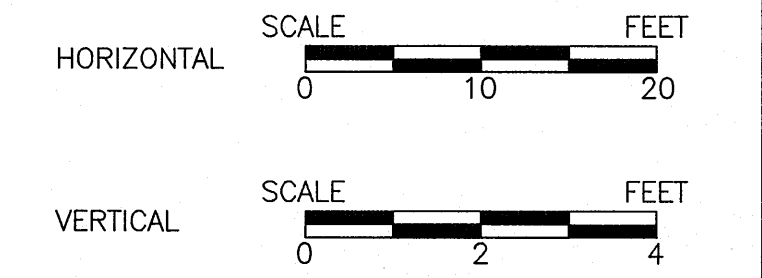
AIRCRAFT APPROACH CATEGORY C
AIRPLANE DESIGN GROUP III
NON-PRECISION RUNWAY

PROPOSED TEMPORARY RUNWAY 10-28 WIDTH = 100' PAVED SURFACE.

PROPOSED TAXIWAY P = 2 EACH
12.5' WIDE PAVED SHOULDERS
PLUS 75' WIDE TAXIWAY = 100' PAVED SURFACE.

SOIL EXCAVATION LEGEND:

- EXCAVATION BY OTHERS (MLI-3855), STA. 647+22 TO 654+29.6 ONLY.
- EXCAVATION BY THIS CONTRACTOR.
- EXCAVATION BY THIS CONTRACTOR, STA. 647+50 TO 649+18 ONLY, SEE NOTE 4.



AIRCRAFT APPROACH CATEGORY C
AIRPLANE DESIGN GROUP III
NON-PRECISION RUNWAY

PROPOSED TYPICAL SECTION - BITUMINOUS PAVING
TAXIWAY P / TEMPORARY RUNWAY 10-28, STA. 653+51 TO STA. 654+00

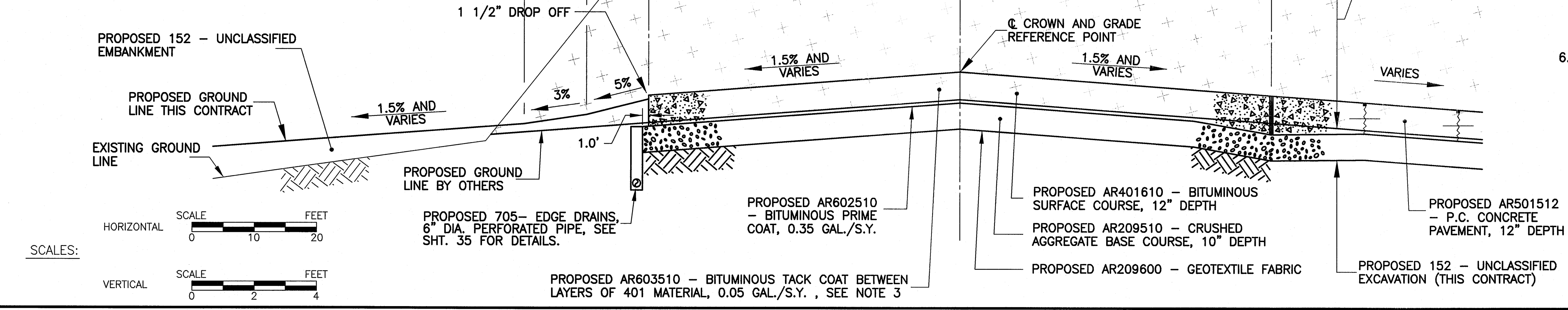
NOTES:

PROPOSED TEMPORARY RUNWAY 10-28 WIDTH = 100' PAVED SURFACE.

PROPOSED TAXIWAY P = 2 EACH
12.5' WIDE PAVED SHOULDERS
PLUS 75' WIDE TAXIWAY = 100' PAVED SURFACE.

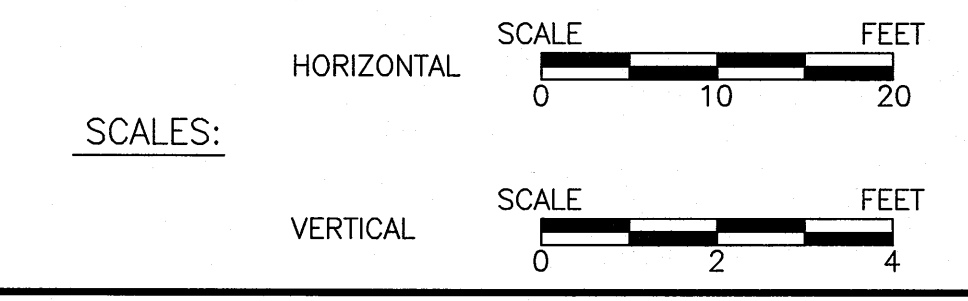
SOIL EXCAVATION LEGEND:

- EXCAVATION BY OTHERS (MLI-3855), STA. 647+25 TO 654+29.6 ONLY.
- EXCAVATION BY THIS CONTRACTOR.



- SEE SHEET 9 FOR CONTINUATION OF EARTHWORK OUTSIDE OF THE PAVING AREA.
- * = IF REQUIRED BY THE FAA, A FUTURE PROJECT COULD REMOVE THE OUTSIDE 12.5' LANE (7) TO CREATE A TAXIWAY PAVEMENT WIDTH OF 75'.
- CONTRACTOR SHALL SPRAY A LIGHT COAT OF 603-BITUMINOUS TACK COAT MATERIALS BETWEEN ALL LAYERS OF BITUMINOUS AS DIRECTED BY THE RESIDENT ENGINEER.
- THIS BITUMINOUS PAVEMENT IS TEMPORARY AND WILL BE REMOVED DURING A FUTURE PROJECT TO CONVERT PROPOSED TEMPORARY RUNWAY 10-28 INTO PERMANENT TAXIWAY P. TO A LANDING AIRCRAFT PILOT, A ROUNDED END OF PAVEMENT LOOKS LIKE A TAXIWAY. A SQUARE END OF PAVEMENT COULD BE MISTAKEN FOR A RUNWAY THRESHOLD.
- THIS CONTRACTOR SHALL NOT START WORK UNTIL THE MLI-3855 CONTRACTOR (OTHERS) HAS REMOVED THE SOIL SURCHARGE. THE MLI-3855 CONTRACTOR (OTHERS) SHALL BE REQUIRED TO REMOVE THE SOIL SURCHARGE WITHIN 30 WORKING DAYS AFTER HIS CONTRACT (MLI-3855) NOTICE TO PROCEED IS ISSUED.
- ** = THE LOCATION OF THE BITUMINOUS TO PCC PAVEMENT JOINT IS UNDER REVIEW. FINAL JOINT LOCATION SHALL BE DETERMINED BY THE RESIDENT ENGINEER IN THE FIELD AT THE TIME OF CONSTRUCTION.

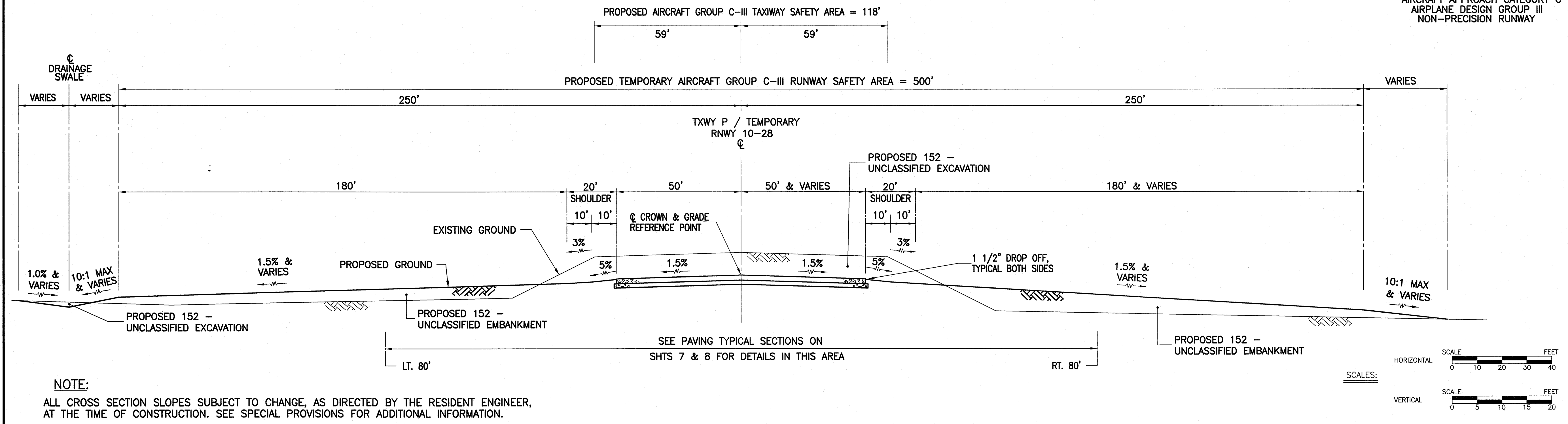
END OF P.C. CONCRETE PAVEMENT / START OF BITUMINOUS PAVEMENT**	
PAVING LANE	BITUMINOUS PAVEMENT LOCATION
7	STA. 652+83 - STA. 654+00
8	STA. 653+24.5 - STA. 654+00
9	STA. 653+51 - STA. 654+00
10	STA. 653+66.5 - STA. 654+00
11	STA. 653+79 - STA. 654+00



PROPOSED TYPICAL SECTION - SAFETY AREA EARTHWORK
 TAXIWAY P / TEMPORARY RUNWAY 10-28, STA. 642+36.56 (EAST EDGE OF RUNWAY 5-23) TO STA. 654+00

QUAD CITY INTERNATIONAL AIRPORT
 TAXIWAY P, PHASE IV - EAST PAVING
 ILL. MLI-3873, QU011
 SHEET 9 OF 74

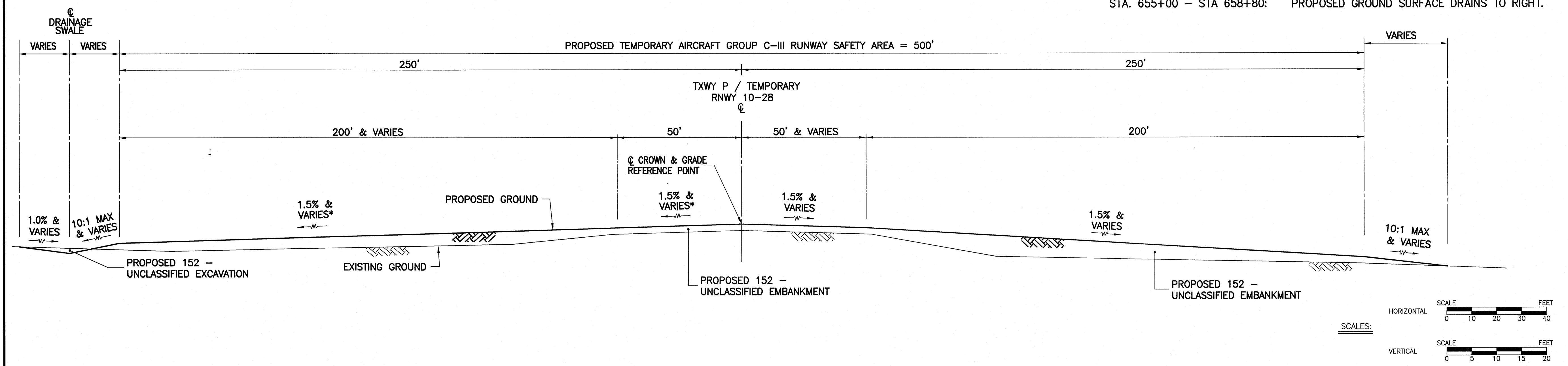
AIRCRAFT APPROACH CATEGORY C
 AIRPLANE DESIGN GROUP III
 NON-PRECISION RUNWAY



PROPOSED TYPICAL SECTION - SAFETY AREA EARTHWORK
 TAXIWAY P / TEMPORARY RUNWAY 10-28, STA. 654+00 TO STA. 658+80

NOTES:

1. ALL CROSS SECTION SLOPES SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. * = LEFT OF CENTERLINE EARTHWORK:
 STA. 654+00 - STA 655+00: PROPOSED GROUND SURFACE DRAINS TO LEFT.
 STA. 655+00 - STA 658+80: PROPOSED GROUND SURFACE DRAINS TO RIGHT.

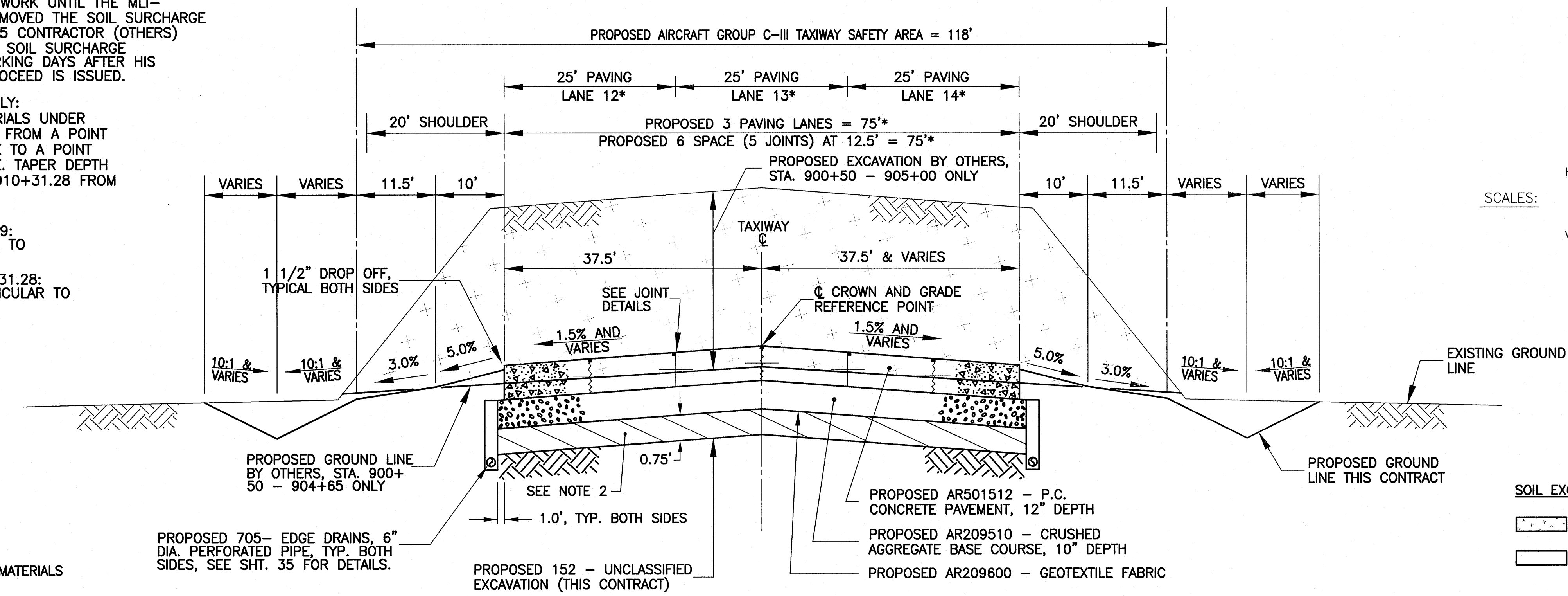


TYPICAL SECTIONS

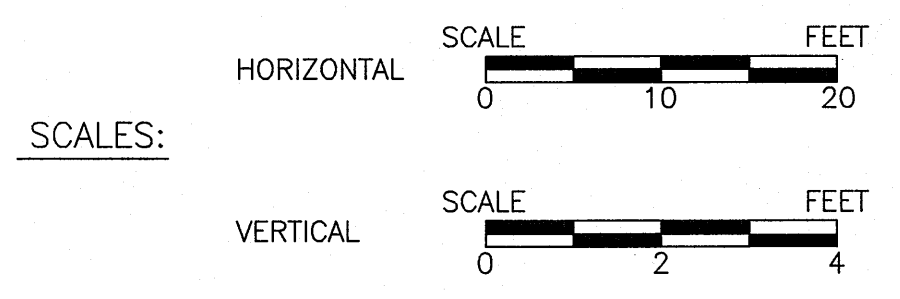
NOTES:

1. STA. 900+50 TO STA. 905+00 ONLY:
THIS CONTRACTOR SHALL NOT START WORK UNTIL THE MLI-3855 CONTRACTOR (OTHERS) HAS REMOVED THE SOIL SURCHARGE AND STOCKPILED SAND. THE MLI-3855 CONTRACTOR (OTHERS) SHALL BE REQUIRED TO REMOVE THE SOIL SURCHARGE AND STOCKPILE SAND WITHIN 30 WORKING DAYS AFTER HIS CONTRACT (MLI-3855) NOTICE TO PROCEED IS ISSUED.
2. STA. 909+95 TO STA. 910+31.28 ONLY:
INSTALL COMPACTED SAND FILL MATERIALS UNDER (AND WITHIN 1.0' OF) THE PAVEMENT FROM A POINT 1.83' BELOW THE PAVEMENT SURFACE TO A POINT 2.58' BELOW THE PAVEMENT SURFACE. TAPER DEPTH OF SAND BETWEEN STA. 910+19 & 910+31.28 FROM 0.75' TO 0.00'.
3. * = STA. 900+50 TO STA. 909+81.29:
INSTALL PAVING LANES PARALLEL TO T-P CENTER LINE.
** = STA. 909+81.29 TO STA. 910+31.28:
INSTALL PAVING LANES PERPENDICULAR TO T-P CENTER LINE.
SEE JOINT PLAN FOR DETAILS.

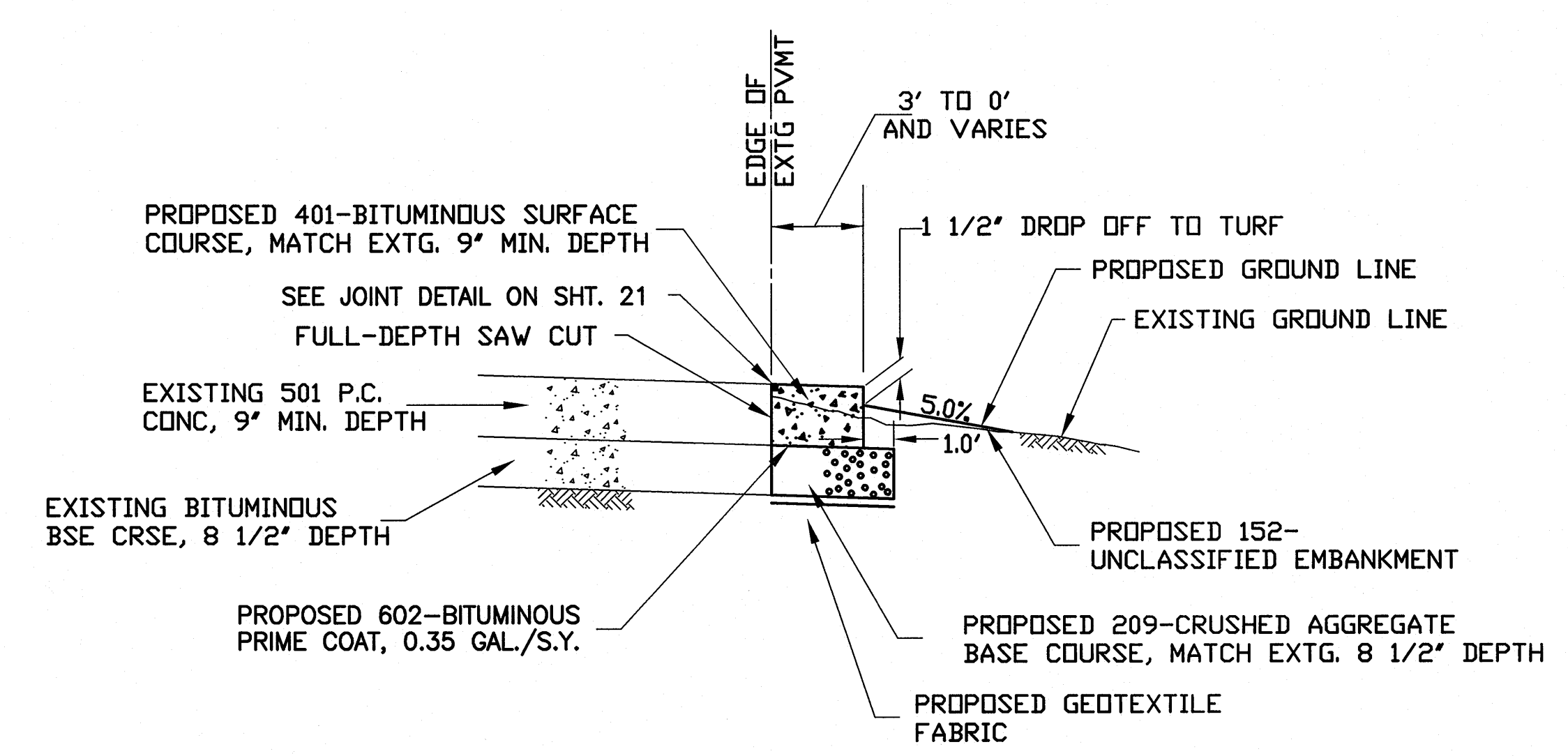
PROPOSED TYPICAL SECTION - PCC PAVING
SURVEY LINE P2 (EAST CONNECTOR), STA. 900+50 TO STA. 910+31.28



SOIL MATERIAL LEGEND:
 COMPACTED SAND FILL MATERIALS
 COMPACTED SAND OR CLAY FILL MATERIALS

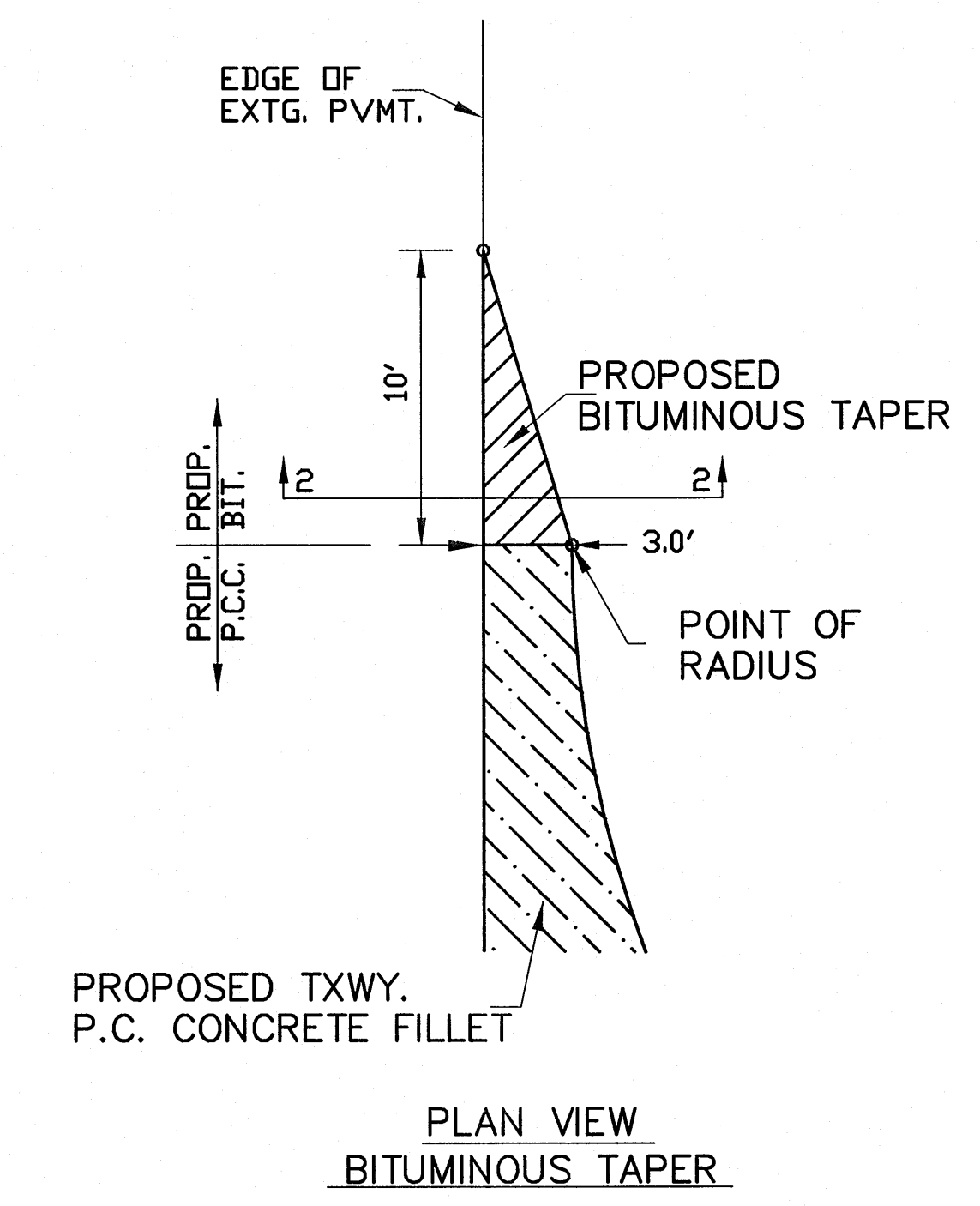


SOIL EXCAVATION LEGEND:
 EXCAVATION BY OTHERS (MLI-3855), STA. 900+50 TO 904+65 ONLY.
 EXCAVATION BY THIS CONTRACTOR.



SECTION 2-2

TYPICAL SECTION
BITUMINOUS TAPER AT EDGE OF EXISTING PAVEMENT :
TAXIWAY K, STA. 949+75, LT. & STA. 952+75, RT.



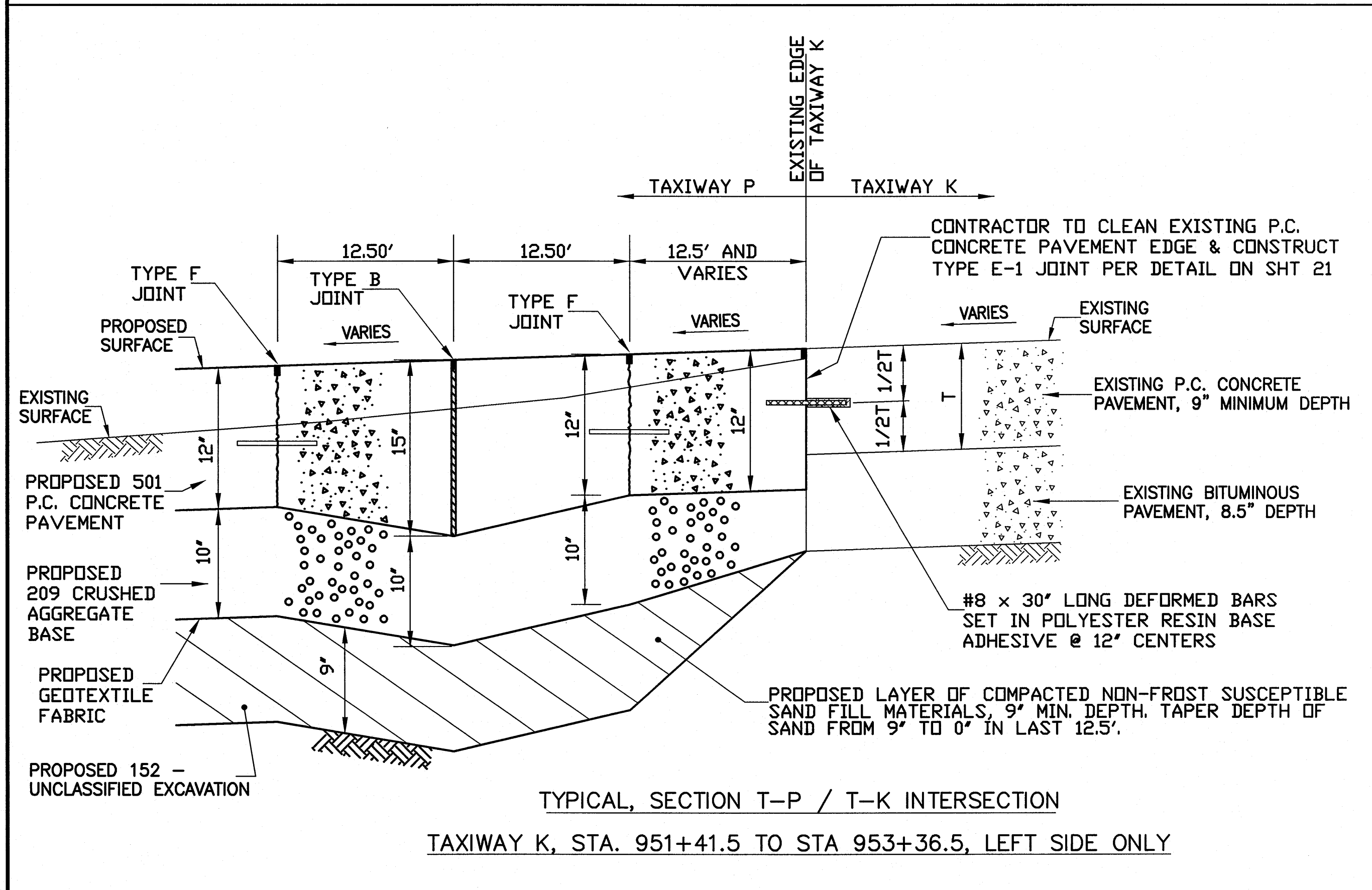
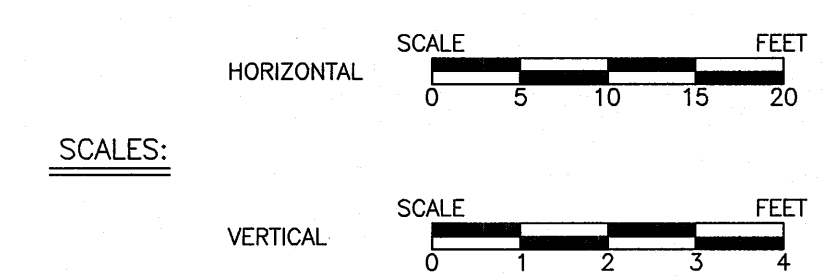
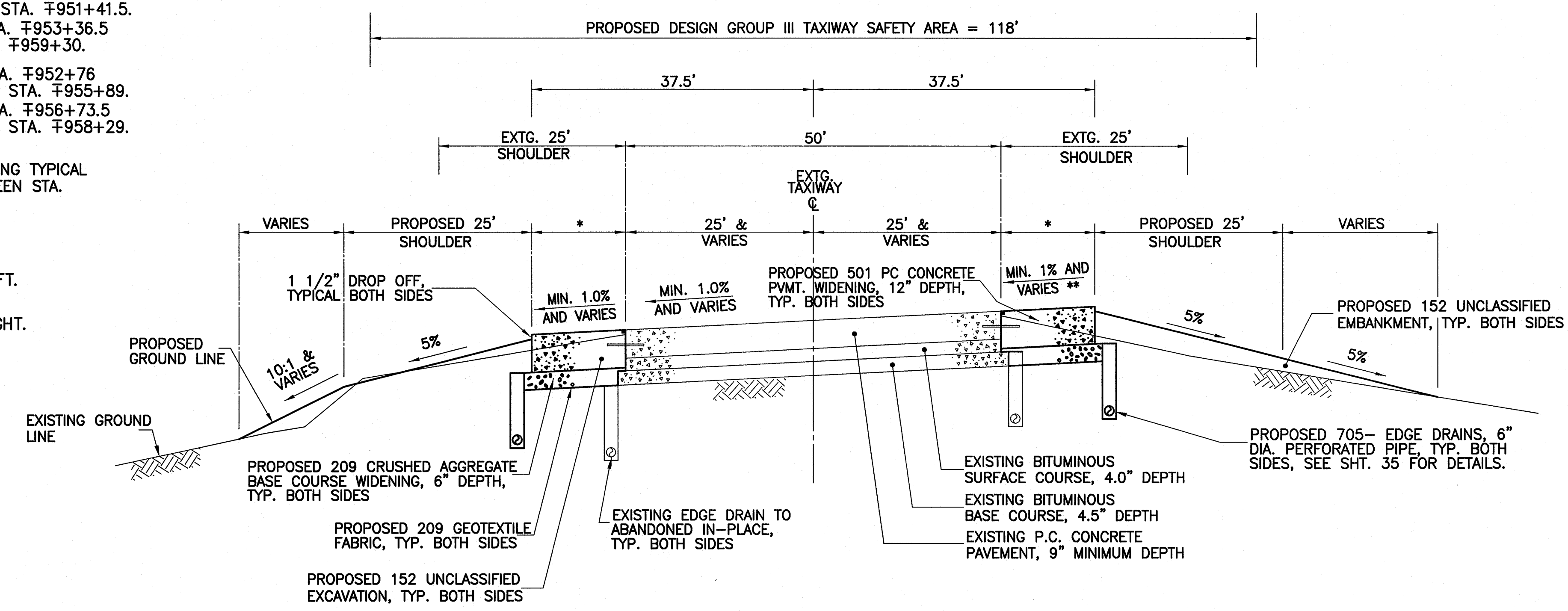
PLAN VIEW
BITUMINOUS TAPER

AIRCRAFT APPROACH CATEGORY C
AIRPLANE DESIGN GROUP III

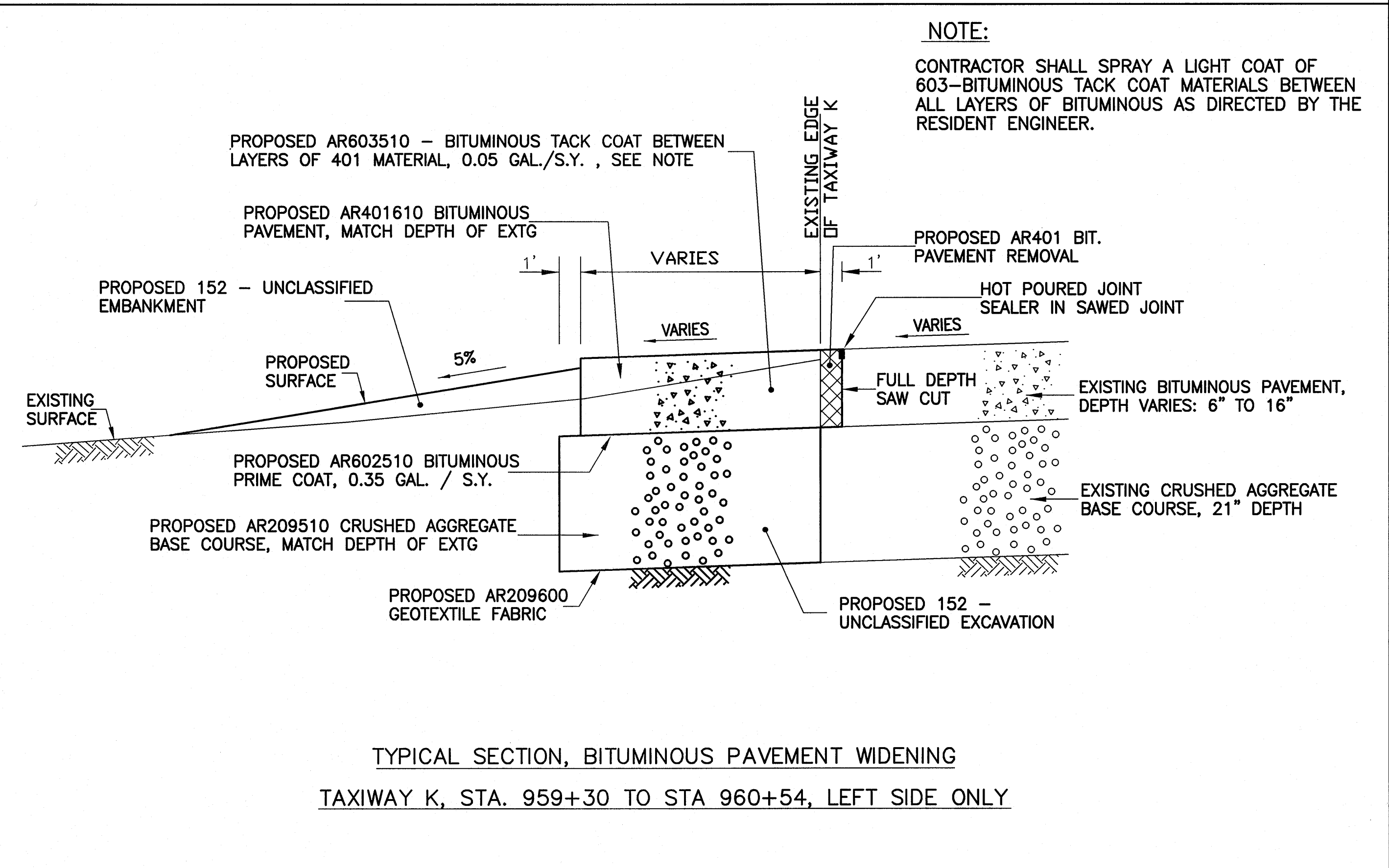
NOTES:

1. * = PROPOSED PCC PVMT WIDENING:
 LT: CLOSEST EXTG. JOINT TO STA. ±949+76.42
 TO CLOSEST EXTG. JOINT @ STA. ±951+41.5.
 RT: CLOSEST EXTG. JOINT TO STA. ±953+36.5
 TO EXTG. PCC JOINT @ STA. ±959+30.
 RT: CLOSEST EXTG. JOINT TO STA. ±952+76
 TO CLOSEST EXTG. JOINT TO STA. ±955+89.
 RT: CLOSEST EXTG. JOINT TO STA. ±956+73.5
 TO CLOSEST EXTG. JOINT TO STA. ±958+29.
2. * = SEE BELOW FOR PAVEMENT WIDENING TYPICAL
 SECTION ON THE LEFT SIDE BETWEEN STA.
 951+41.5 AND STA 953+36.5.
3. ** = RIGHT SIDE PVMT WIDENING:
 STA. 952+76 - STA 955+15:
 PROP PVMT DRAINS TO LEFT.
 STA. 955+15 - STA 958+29:
 PROP PVMT DRAINS TO RIGHT.

**PROPOSED TYPICAL SECTION
TAXIWAY K WIDENING, SEE NOTE 1 FOR LOCATION**



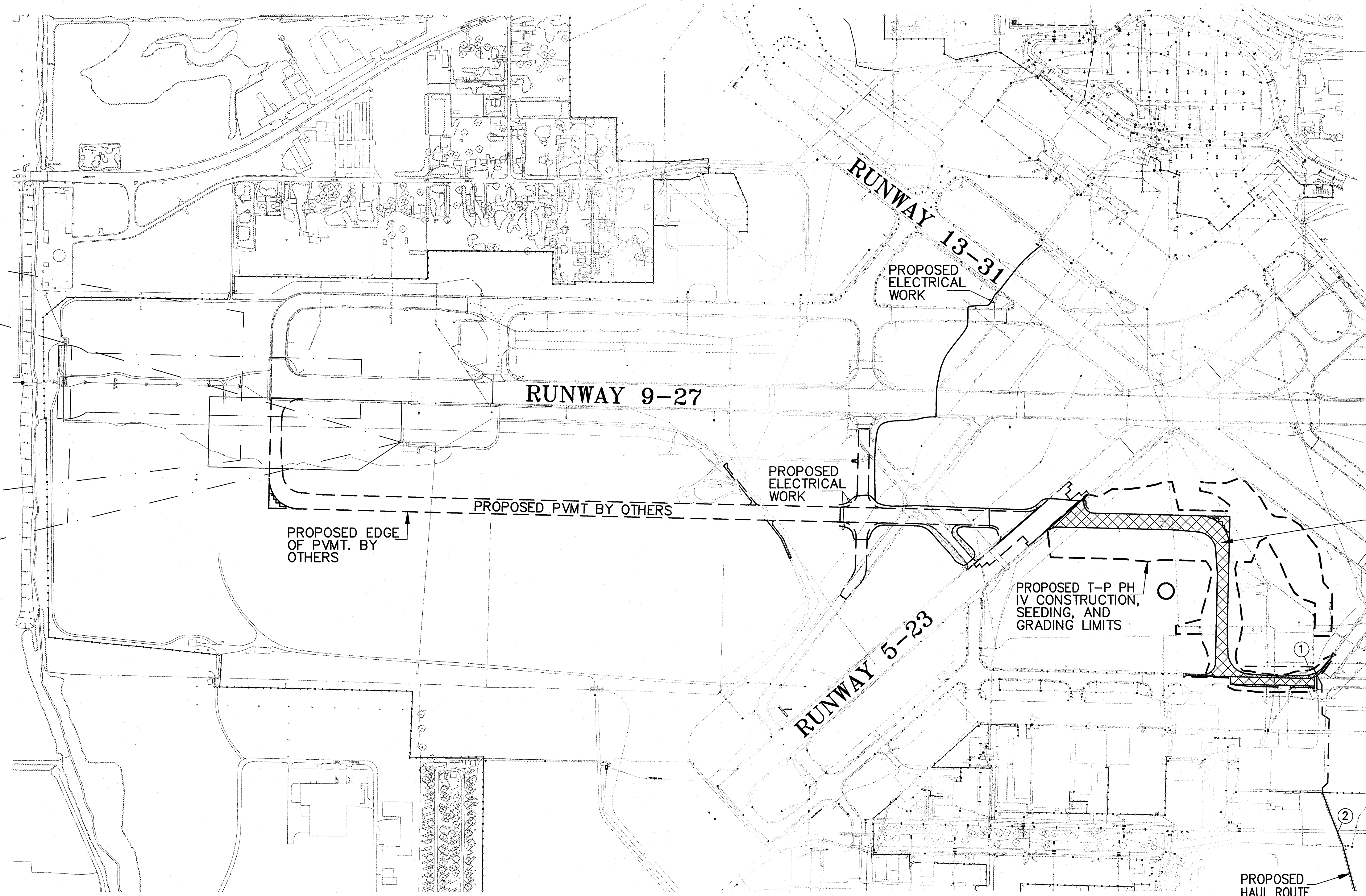
TYPICAL SECTION T-P / T-K INTERSECTION
TAXIWAY K, STA. 951+41.5 TO STA 953+36.5, LEFT SIDE ONLY



TYPICAL SECTION, BITUMINOUS PAVEMENT WIDENING
TAXIWAY K, STA. 959+30 TO STA 960+54, LEFT SIDE ONLY

NOTE:
CONTRACTOR SHALL SPRAY A LIGHT COAT OF
603-BITUMINOUS TACK COAT MATERIALS BETWEEN
ALL LAYERS OF BITUMINOUS AS DIRECTED BY THE
RESIDENT ENGINEER.

QUAD CITY INTERNATIONAL AIRPORT
 TAXIWAY P, PHASE IV - EAST PAVING
 ILL. MLI-3873, QU011
 SHEET 12 OF 74



PROPOSED MINIMUM UTILITY PROTECTION*		
LOCATION NUMBER	UTILITY DESCRIPTION	MINIMUM PROTECTION
①	TEMPORARY FLUSH BITUMINOUS ACCESS ROUTE PER NOTE 6 ON SHEET 3	2" 401 BIT. ON 8" 209 ROCK ON 209 GEOTECH FAB FOR 100' OFF OF T-K
②	BITUMINOUS SERVICE ROAD	STEEL PLATES W/ 2" SOIL COVER**

* = MINIMUM THICKNESS OF STEEL PLATES = 1" (ONE INCH).
 ** = 10:1 SIDE SLOPE TAPERS REQUIRED FROM HAUL ROUTE ONTO SERVICE ROAD FOR AIRFIELD TRAFFIC ON SERVICE ROAD CROSSING THE HAUL ROAD.

LEGEND:

- EDGE LIGHT
- GUIDANCE SIGN
- ELECTRICAL CABLE
- STORM SEWER OR UNDERDRAIN
- INLET
- OUTLET
- STORM MANHOLE
- SANITARY MANHOLE
- DRAINAGE DITCH
- CONDUIT OR DUCT BANK
- PROPOSED PAVEMENT BY OTHERS
- PROPOSED CONSTRUCTION SEEDING, AND GRADING LIMITS
- PROPOSED HAUL ROUTE
- ① PROPOSED UTILITY PROTECTION LOCATION NUMBER.
- PROPOSED PAVEMENT AREA
- PROPOSED ELECTRICAL WORK OUTSIDE OF PAVING AREA.

TEMPORARY BENCH MARKS		
B.M.	ELEV.	DESCRIPTION
TBM 200	587.10	HUB AT SE COR OF TXY H BORROW AREA, N1742722.2538, E2199623.7550, STATION 107+06.74, LT. 739.58
TBM K	578.34	TOP OF RCP, S SIDE OF SERVICE RD, N1741508.8290, E2201629.7690, STATION 127+42.16, RT. 424.08
TBM R	581.52	NE COR CONC BASE ELECT TRAN, S SIDE 9-27, BETWEEN SIGNS [2] - [2], N1741569.4966, E2200420.1701, STATION 115+31.40, RT. 393.12
TBM T	576.35	CHIS. □ SE COR CONC PAD W SIDE GS BLDG, N1741651.7212, E2198675.6748, STATION 97+85.40, RT. 354.03
TBM U	583.18	CHIS. □ HOLD SIGN BASE, NE COR, S SIDE R9, N1741775.92035, E2203365.21359, STATION 144+70.44, RT. 113.99

PROJECT CONTROL POINTS			
NUMBER	LOCATION	ELEV.	REMARK
5	STA. 108+94.45, RT. 339.57 N1741638.7660, E2199784.7412	580.05	
7	STA. 121+74.76, RT. 449.70 N1741496.8955, E2201061.9416	577.67	
8	STA. 127+73.19, RT. 413.69 N1741518.2450, E2201661.0745	579.45	
13	STA. 143+25.87, RT. 1163.46 N1740729.9858, E2203194.7470	574.40	1/2" REBAR
2000	STA. 107+69.21, LT. 736.44 N1742717.7720, E2199686.1370	587.72	SPK

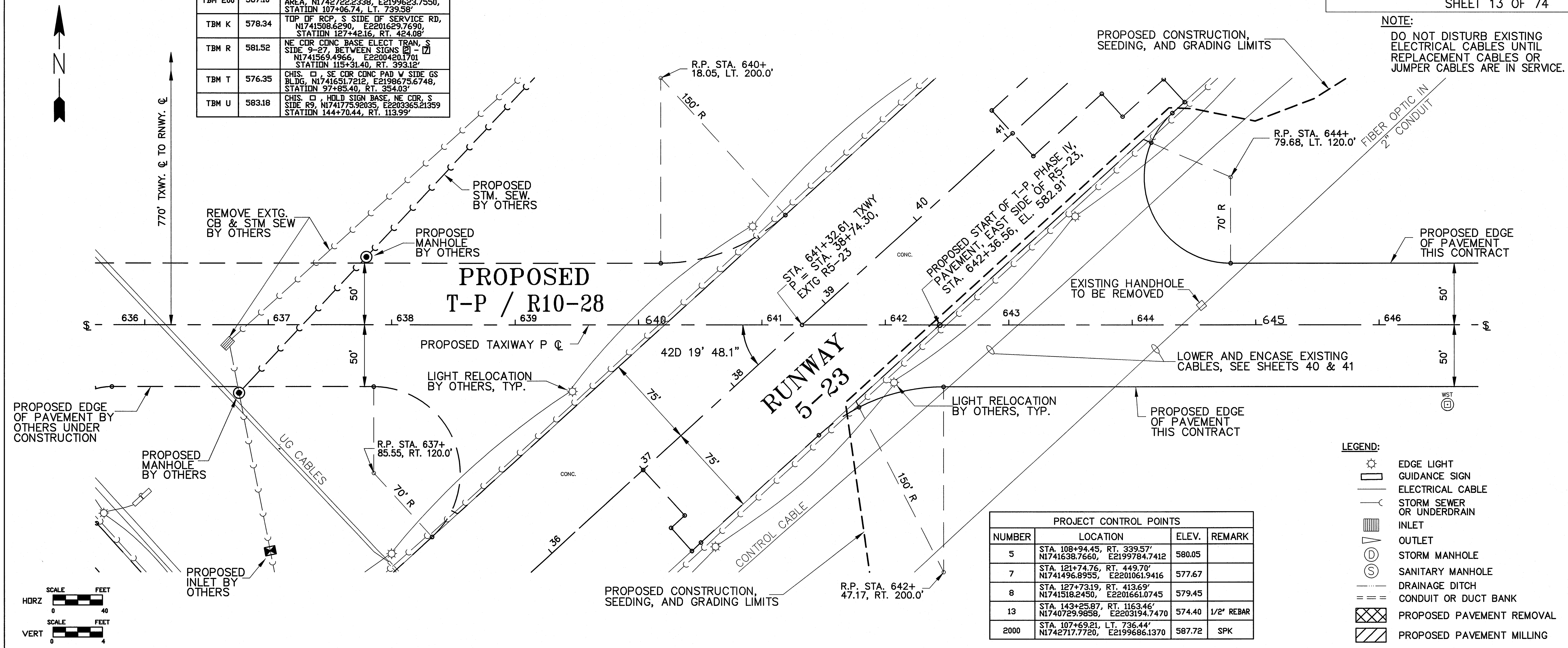
NOTES:

1. SEE PLAN & PROFILE SHEETS FOR LOCATION OF EXISTING UNDERGROUND ELECTRICAL AND FIELD TILES.
2. CONTRACTOR SHALL REPAIR OR REPLACE ANY IMPROVEMENTS DAMAGED DURING CONSTRUCTION AT HIS OWN EXPENSE.

G:\AIRPORT\A09T002 TP EAST PVMT\AREA.DWG, 7/8/2009 10:14:02 AM, jeffm

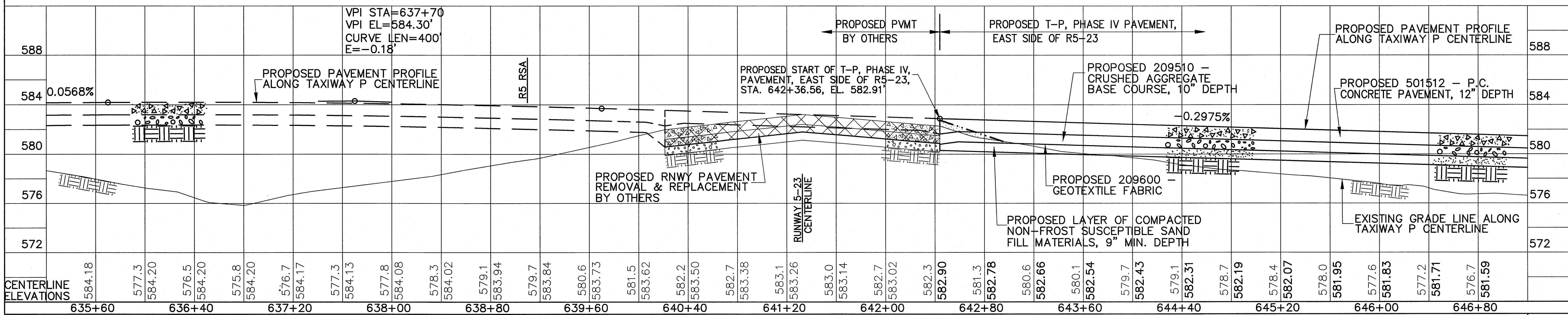
TEMPORARY BENCH MARKS		
B.M.	ELEV.	DESCRIPTION
TBM 200	587.10	HUB AT SE COR OF TXY H BORROW AREA, N1742722.2338, E2199623.7550, STATION 107+06.74, LT. 739.58'
TBM K	578.34	TOP OF RCP, S SIDE OF SERVICE RD, N1741508.6290, E2201629.7690, STATION 127+42.16, RT. 424.08'
TBM R	581.52	NE COR CONC BASE ELECT TRAN, S SIDE 9-27 BETWEEN SIGNS [2] - [7] N1741569.4966, E2200420.1701, STATION 115+31.40, RT. 393.12'
TBM T	576.35	CHIS. □, SE COR CONC PAD V SIDE GS BLDG, N1741651.7212, E2198675.6748, STATION 97+85.40, RT. 354.03'
TBM U	583.18	CHIS. □, HOLD SIGN BASE, NE COR, S SIDE R9, N174175.92035, E220365.21359, STATION 144+70.44, RT. 113.99'

NOTE:
DO NOT DISTURB EXISTING ELECTRICAL CABLES UNTIL REPLACEMENT CABLES OR JUMPER CABLES ARE IN SERVICE.



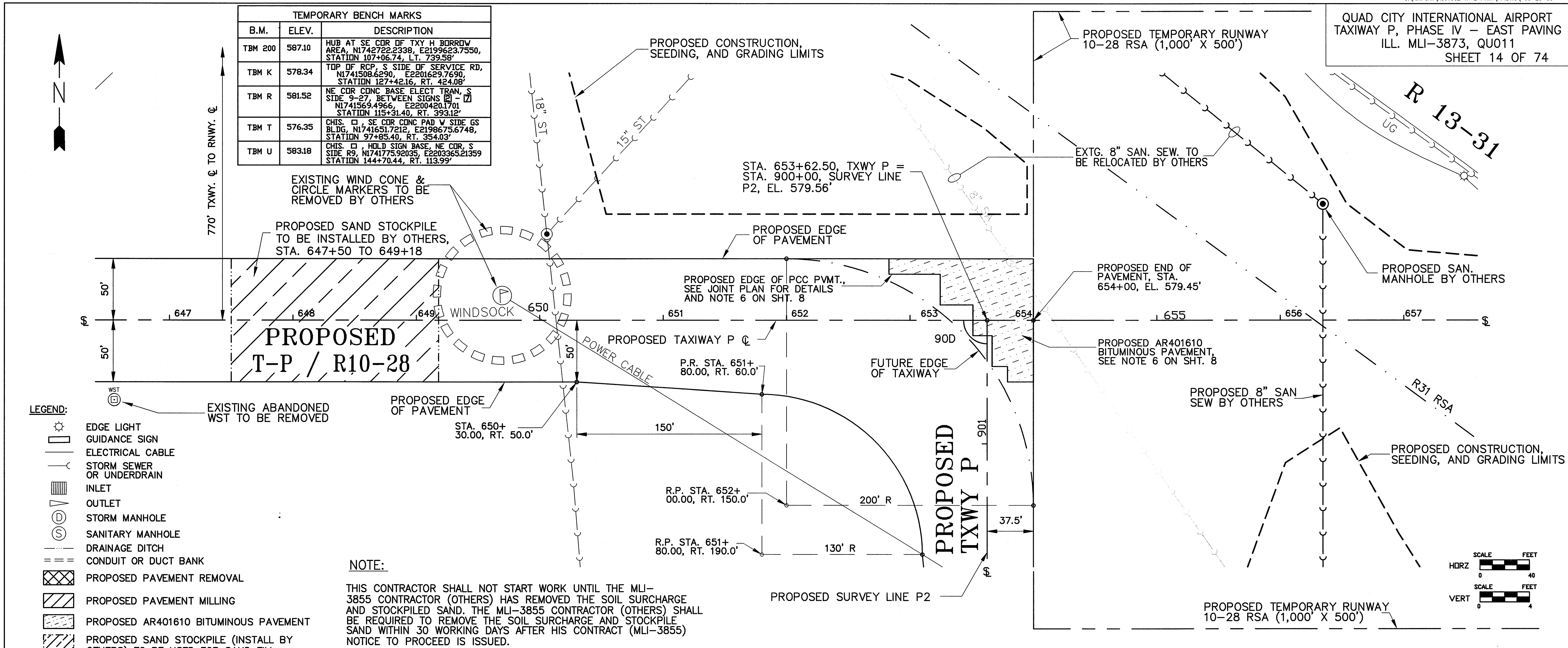
PROJECT CONTROL POINTS			
NUMBER	LOCATION	ELEV.	REMARK
5	STA. 108+94.45, RT. 339.57' N1741638.7660, E2199784.7412	580.05	
7	STA. 121+74.76, RT. 449.70' N1741496.8955, E2201061.9416	577.67	
8	STA. 127+73.19, RT. 413.69' N1741518.2450, E2201661.0745	579.45	
13	STA. 143+25.87, RT. 1163.46' N1740729.9858, E2203194.7470	574.40	1/2" REBAR
2000	STA. 107+69.21, LT. 736.44' N1742717.7720, E2199686.1370	587.72	SPK

- LEGEND:
- EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER OR UNDERDRAIN
 - INLET
 - OUTLET
 - STORM MANHOLE
 - SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - PROPOSED PAVEMENT REMOVAL
 - PROPOSED PAVEMENT MILLING



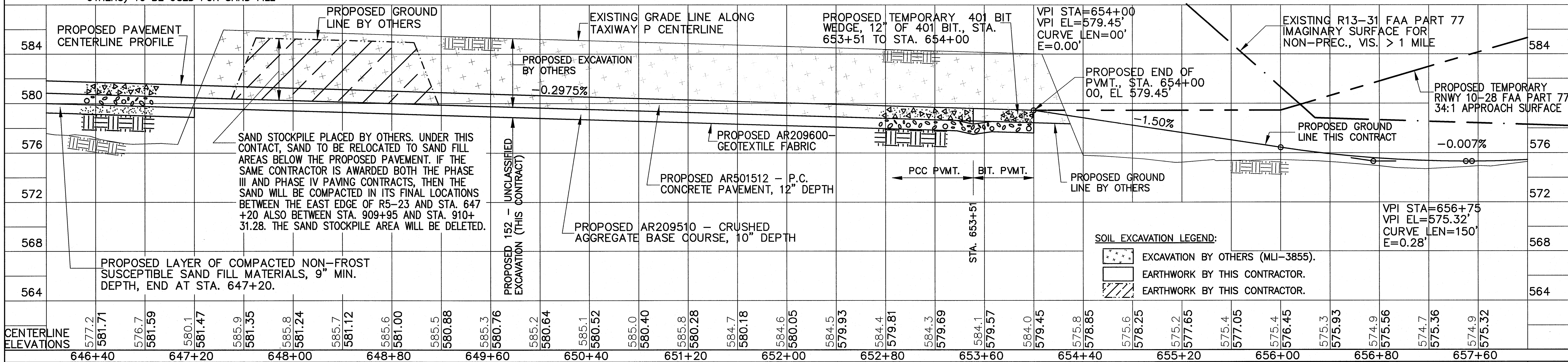
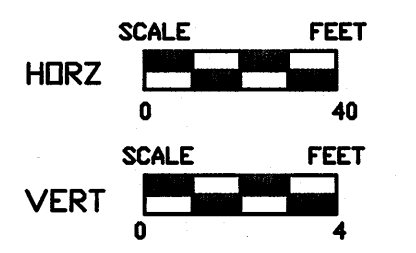
TAXIWAY P PLAN & PROFILE, STA 635+80 TO STA 646+60 13/74

TEMPORARY BENCH MARKS		
B.M.	ELEV.	DESCRIPTION
TBM 200	587.10	HUB AT SE COR OF TXY H BORROW AREA, N1742722.2338, E2199623.7550, STATION 107+06.74, LT. 739.58'
TBM K	578.34	TOP OF RCP S SIDE OF SERVICE RD, N1741508.6290, E201629.7690, STATION 127+42.16, RT. 424.08'
TBM R	581.52	NE COR CONC BASE ELECT TRAN, S SIDE 9-27, BETWEEN SIGNS (2) - (7) N1741569.4966, E2200420.1701, STATION 115+31.40, RT. 393.12'
TBM T	576.35	CHIS. □ SE COR CONC PAD V. SIDE GS BLDG, N1741651.7212, E219875.6748, STATION 97+85.40, RT. 354.03'
TBM U	583.18	CHIS. □ HOLD SIGN BASE, NE COR, S SIDE R9, N1741775.92035, E2203365.21359, STATION 144+70.44, RT. 113.99'



- LEGEND:**
- EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER OR UNDERDRAIN
 - INLET
 - OUTLET
 - STORM MANHOLE
 - SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - PROPOSED PAVEMENT REMOVAL
 - PROPOSED PAVEMENT MILLING
 - PROPOSED AR401610 BITUMINOUS PAVEMENT
 - PROPOSED SAND STOCKPILE (INSTALL BY OTHERS) TO BE USED FOR SAND FILL

NOTE:
THIS CONTRACTOR SHALL NOT START WORK UNTIL THE MLI-3855 CONTRACTOR (OTHERS) HAS REMOVED THE SOIL SURCHARGE AND STOCKPILED SAND. THE MLI-3855 CONTRACTOR (OTHERS) SHALL BE REQUIRED TO REMOVE THE SOIL SURCHARGE AND STOCKPILE SAND WITHIN 30 WORKING DAYS AFTER HIS CONTRACT (MLI-3855) NOTICE TO PROCEED IS ISSUED.

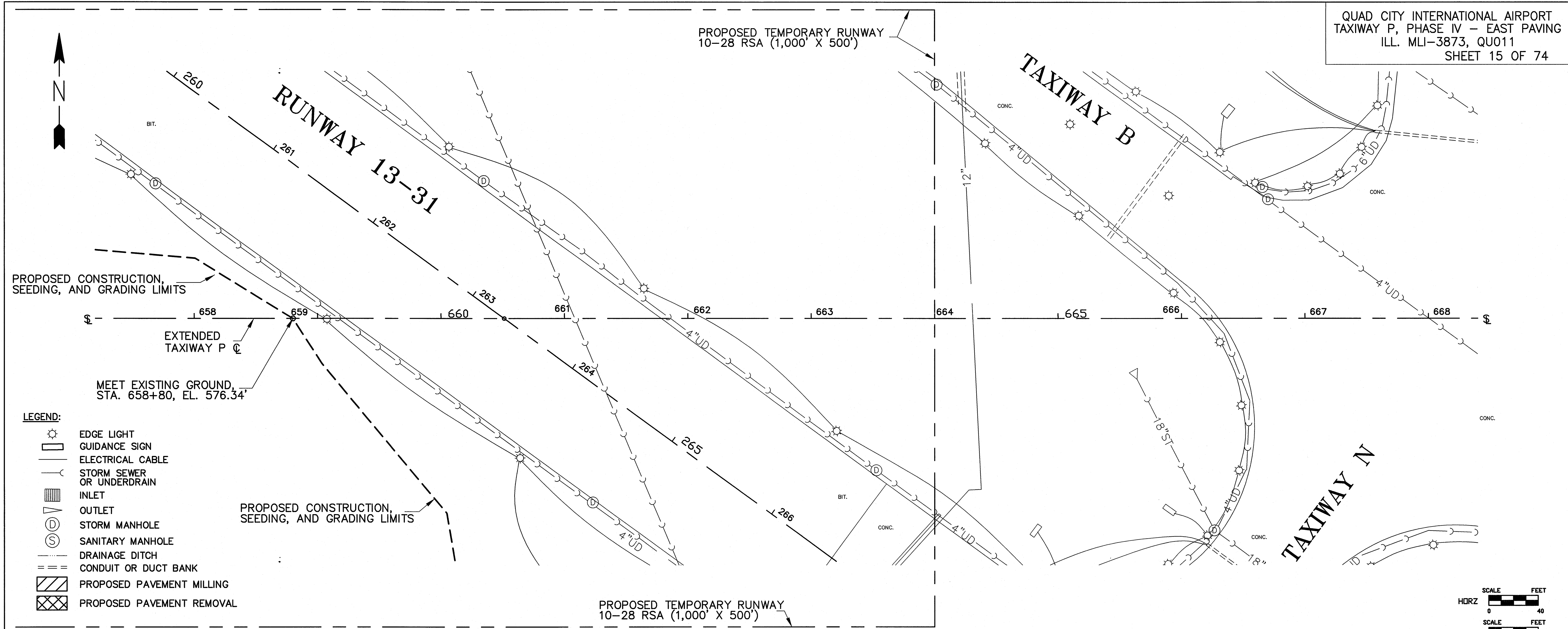


- SOIL EXCAVATION LEGEND:**
- EXCAVATION BY OTHERS (MLI-3855).
 - EARTHWORK BY THIS CONTRACTOR.
 - EARTHWORK BY THIS CONTRACTOR.

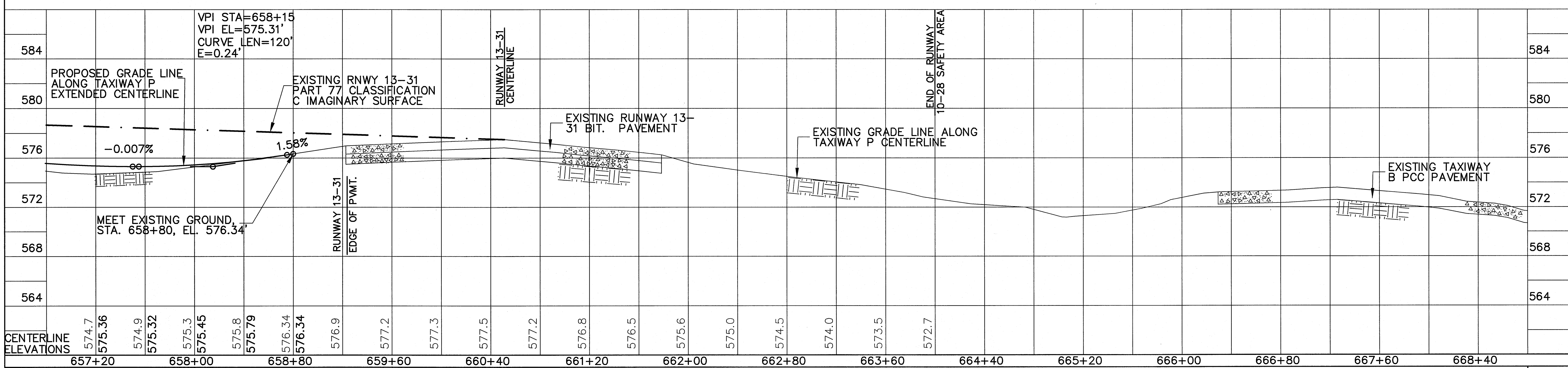
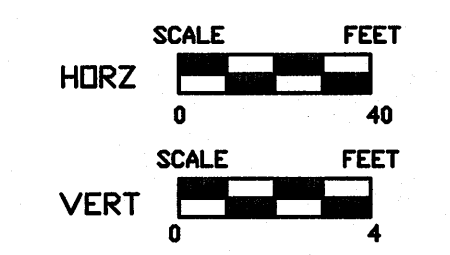
TAXIWAY P PLAN & PROFILE, STA 646+60 TO STA 657+40

G:\AIRPORT\A09T002 TP E PVMT\PP.DWG, 7/8/2009 10:14:43 AM, jefm

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE IV - EAST PAVING
ILL. MLI-3873, QU011
SHEET 15 OF 74

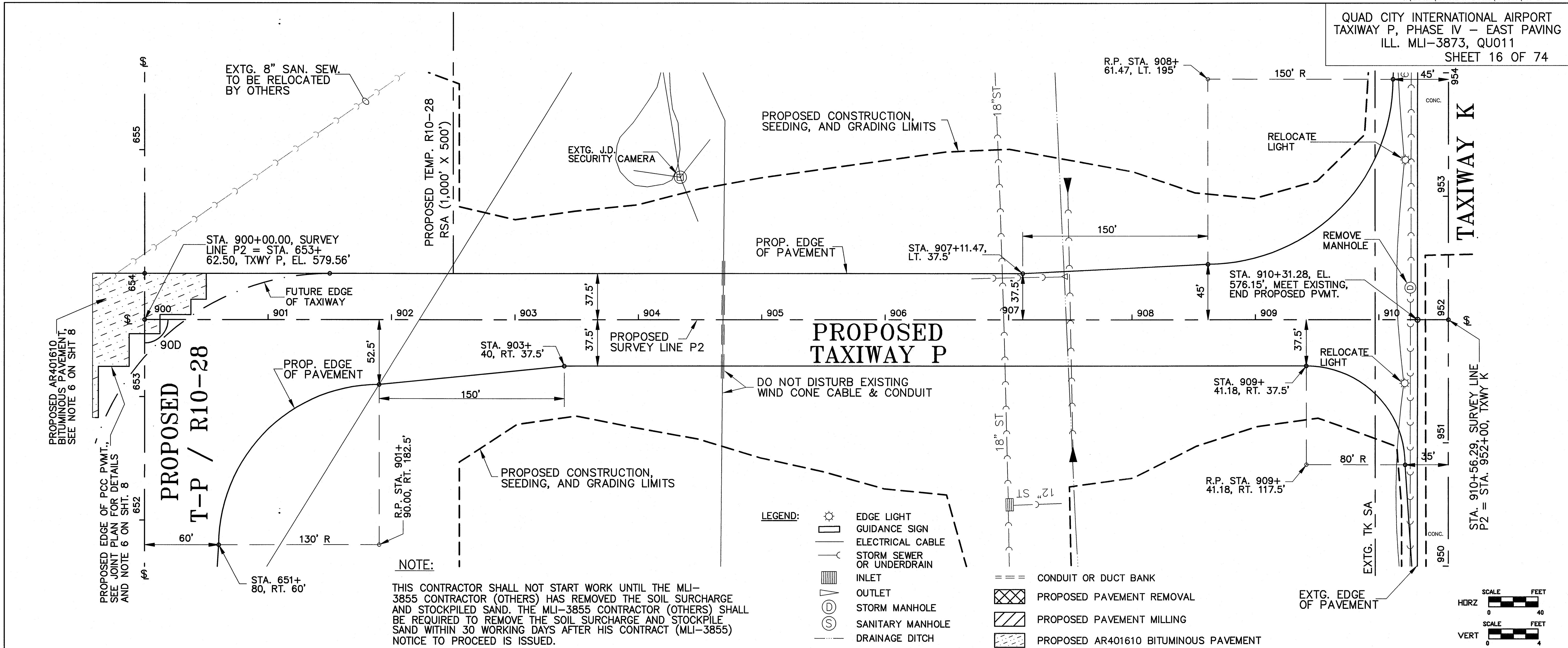


- LEGEND:**
- EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER OR UNDERDRAIN
 - INLET
 - OUTLET
 - STORM MANHOLE
 - SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - PROPOSED PAVEMENT MILLING
 - PROPOSED PAVEMENT REMOVAL

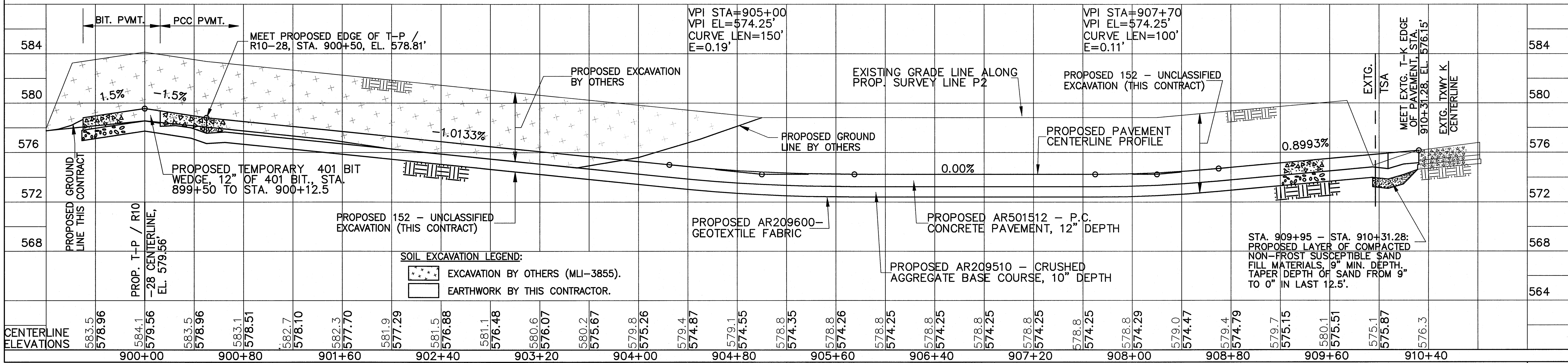


TAXIWAY P PLAN & PROFILE, STA 657+40 TO STA 668+20

G:\AIRPORT\A09T002 TP E PVMT\PP.DWG, 7/8/2009 10:14:56 AM, jeffm



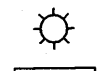
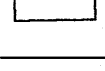
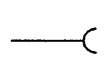

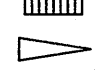


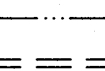
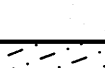
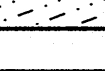


NOTE:
THIS CONTRACTOR SHALL NOT START WORK UNTIL THE MLI-3855 CONTRACTOR (OTHERS) HAS REMOVED THE SOIL SURCHARGE AND STOCKPILED SAND. THE MLI-3855 CONTRACTOR (OTHERS) SHALL BE REQUIRED TO REMOVE THE SOIL SURCHARGE AND STOCKPILE SAND WITHIN 30 WORKING DAYS AFTER HIS CONTRACT (MLI-3855) NOTICE TO PROCEED IS ISSUED.



SURVEY LINE P2 PLAN & PROFILE, STA 900+00 TO STA 910+40

G:\AIRPORT\A09T002 TP E P\MT\PP1.DWG, 7/8/2009 10:15:23 AM, jefm

LEGEND:

-  EDGE LIGHT
-  GUIDANCE SIGN
-  ELECTRICAL CABLE
-  STORM SEWER OR UNDERDRAIN
-  INLET
-  OUTLET
-  STORM MANHOLE
-  SANITARY MANHOLE
-  DRAINAGE DITCH
-  CONDUIT OR DUCT BANK
-  PROPOSED AR401 BITUMINOUS PAVEMENT WIDENING
-  PROPOSED PAVEMENT REMOVAL



PROPOSED PERMANENT PAVEMENT WIDENING, VARIABLE DEPTH 401 BIT. SURF. CRSE. ON 21" OF 209 CRHD. AGGR. ON 209 GOETEXTILE FABRIC. LT. STA 959+28.69 - 960+54

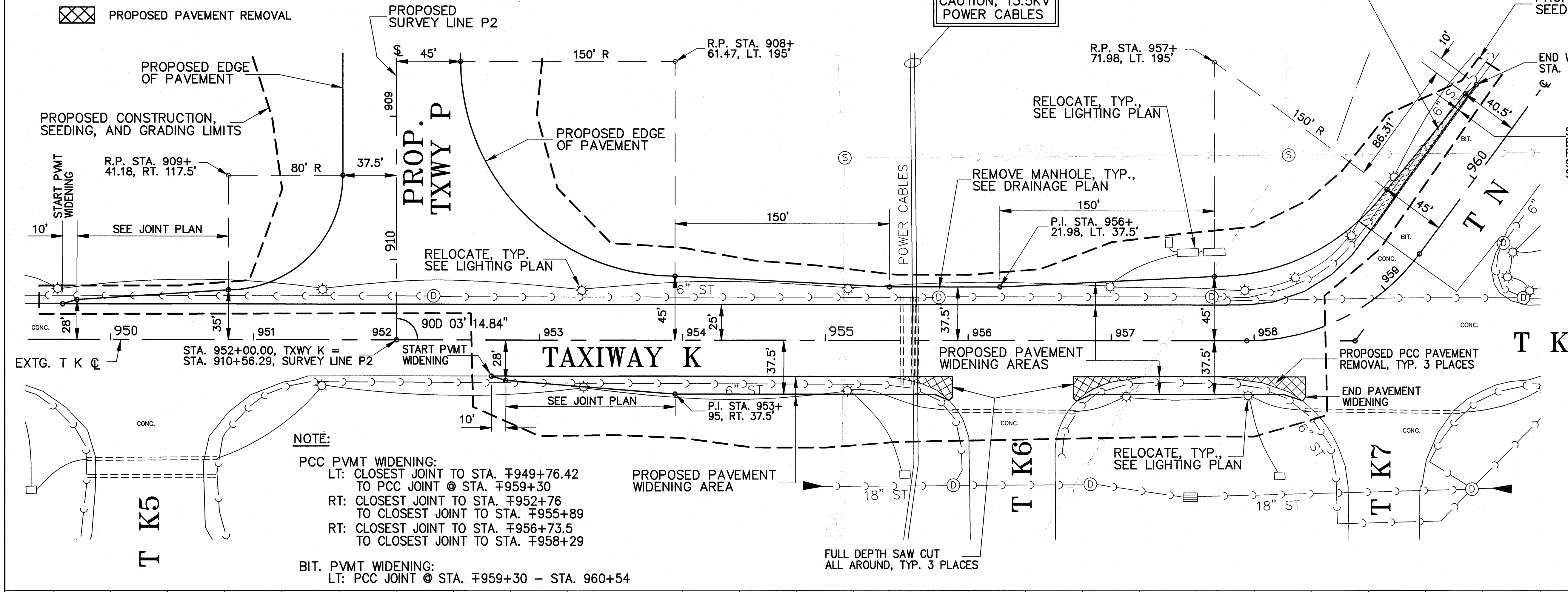
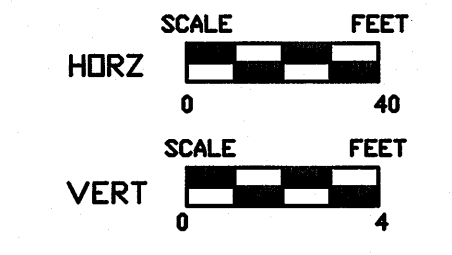
CAUTION, 13.5KV POWER CABLES

PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS

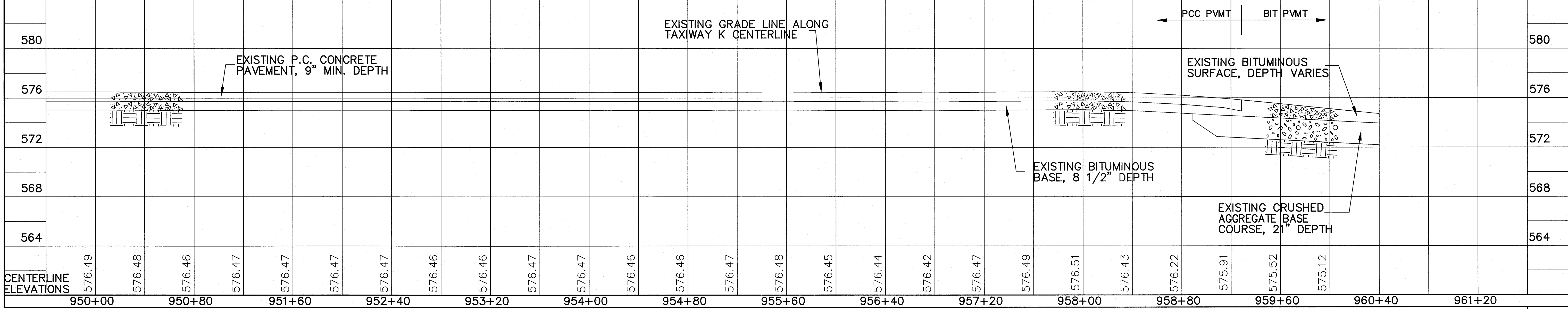
END WIDENING AT STA. 960+54

SAW CUT AND REMOVE 12" OF EXISTING PAVEMENT IN BITUMINOUS PAVEMENT WIDENING AREA, LT. STA. 959+28.69 TO STA. 960+54, SEE SHT. 11 FOR DETAIL.

⊙ CURVE DATA:
I = 53D 34' 39.48"
D = 38D 11' 49.92"
T = 75.73'
L = 140.27'
R = 150.00'
E = 18.03'
P.C. = 957+94.70
P.I. = 958+70.43
P.T. = 959+34.97

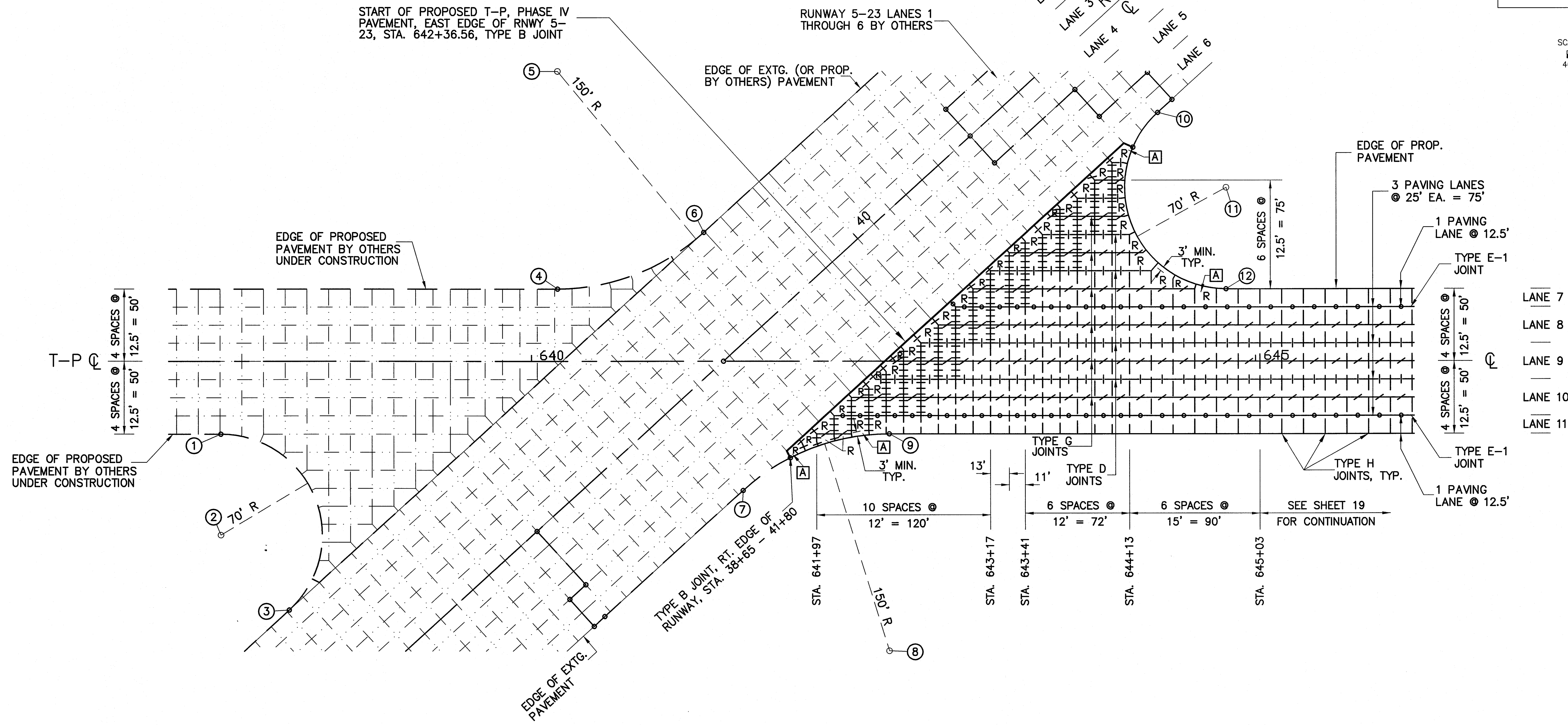
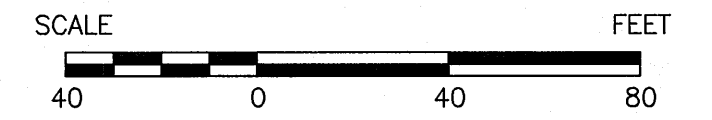


NOTE:
PCC PVMT WIDENING:
LT: CLOSEST JOINT TO STA. ±949+76.42 TO PCC JOINT @ STA. ±959+30
RT: CLOSEST JOINT TO STA. ±952+76 TO CLOSEST JOINT TO STA. ±955+89
RT: CLOSEST JOINT TO STA. ±956+73.5 TO CLOSEST JOINT TO STA. ±958+29
BIT. PVMT WIDENING:
LT: PCC JOINT @ STA. ±959+30 - STA. 960+54



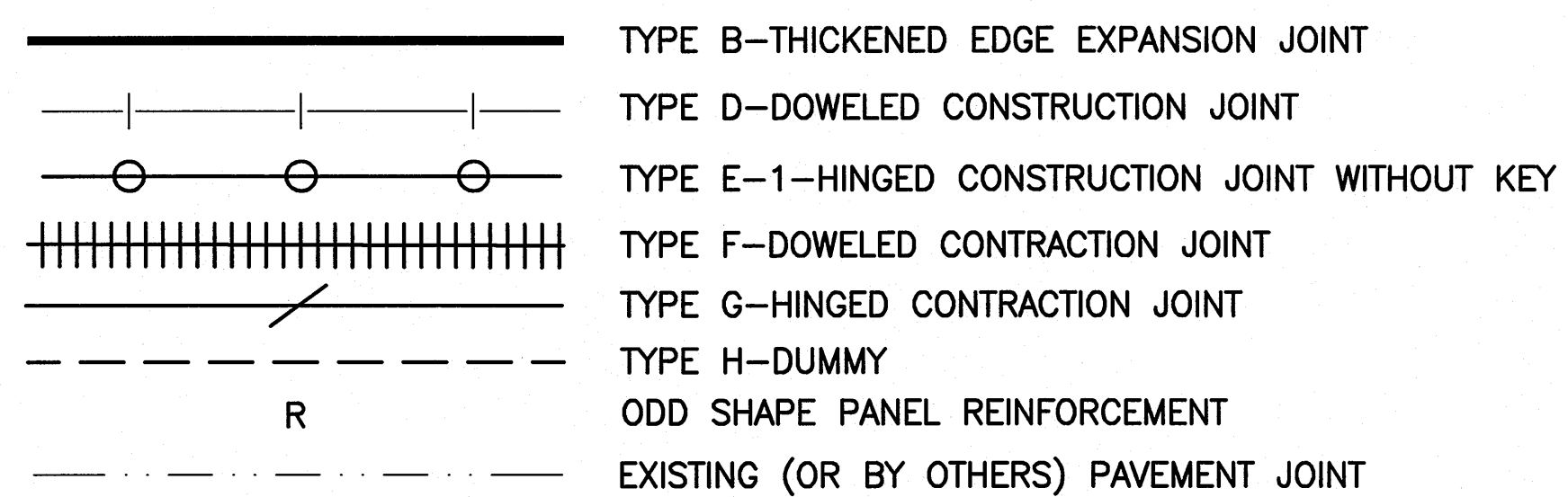
TAXIWAY K PLAN & PROFILE, STA 949+20 TO STA 960+80

G:\AIRPORT\A09T002 TP E PVMT\PP.DWG, 7/8/2009 10:15:54 AM, jeffm



TAXIWAY P / RUNWAY 5 INTERSECTION

JOINT SYMBOL LEGEND



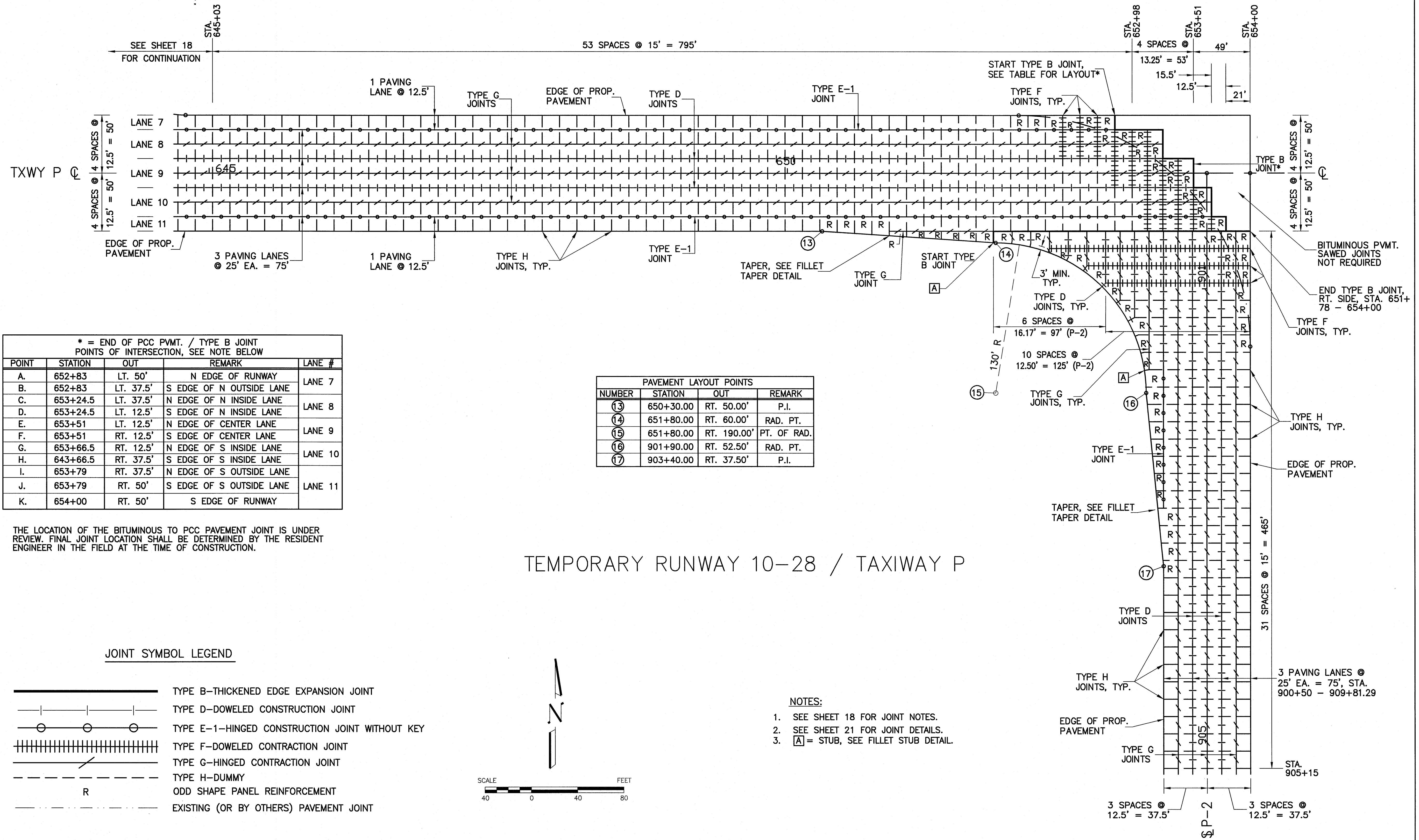
PAVEMENT LAYOUT POINTS			
NUMBER	STATION	OUT	REMARK
①	637+85.55	RT. 50.00'	RAD. PT.
②	637+85.55	RT. 120.00'	PT. OF RAD.
③	35+36.92	LT. 75.00'	RAD. PT.
④	640+18.05	RT. 50.00'	RAD. PT.
⑤	640+18.05	LT. 200.00'	PT. OF RAD.
⑥	39+24.29	LT. 75.00'	RAD. PT.
⑦	39+24.31	RT. 75.00'	RAD. PT.
⑧	642+47.17	LT. 200.00'	PT. OF RAD.
⑨	642+47.17	LT. 50.00'	RAD. PT.
⑩	42+11.69	RT. 75.00'	RAD. PT.
⑪	644+79.68	LT. 120.00'	PT. OF RAD.
⑫	644+79.68	LT. 50.00'	RAD. PT.

TYPE F TRANSVERSE JOINT LOCATIONS*	
LOCATION	START - END
LANE NUMBER 7	640+28 TO 643+41
LANE NUMBER 8	640+04 TO 643+17
LANE NUMBER 9	639+80 TO 642+93
LANE NUMBER 10	639+44 TO 642+69
LANE NUMBER 11	639+20 TO 642+33
T-P RT. 50' - RT. 62.5'	639+20 TO 642+33
T-P RT. 62.5' - RT. 87.5'	638+90 TO EDGE OF R/WY
T-P RT. 87.5' - RT. 137.5'	638+60 TO EDGE OF R/WY
T-P LT. 50' - LT. 62.5'	640+28 TO 643+41
T-P LT. 62.5' - LT. 87.5'	EDGE OF R/WY TO 643+77
T-P LT. 87.5' - LT. 137.5'	EDGE OF R/WY TO 644+01

* = RUNWAY 5-23 LANES 1 THROUGH 6 BY OTHERS

JOINT NOTES:

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
- ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH AASHTO M137.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO DIMENSIONS OF THE SECOND SAWCUT WILL BE ALLOWED.
- SEE SHEET 21 FOR JOINT DETAILS, FILLET STUB DETAILS, FILLET TAPER DETAILS, AND ODD SHAPE PANEL REINFORCEMENT DETAILS.
- RUNWAY 5-23 / TAXIWAY K / TAXIWAY N EXISTING JOINT LOCATIONS AND SPACING BASED ON ORIGINAL PLANS AND/OR AERIAL PHOTOS. ACTUAL EXISTING JOINT LOCATIONS AND SPACING MAY VARY.
- [A] = STUB, SEE FILLET STUB DETAIL.



* = END OF PCC PVMT. / TYPE B JOINT POINTS OF INTERSECTION, SEE NOTE BELOW

POINT	STATION	OUT	REMARK	LANE #
A.	652+83	LT. 50'	N EDGE OF RUNWAY	LANE 7
B.	652+83	LT. 37.5'	S EDGE OF N OUTSIDE LANE	LANE 7
C.	653+24.5	LT. 37.5'	N EDGE OF N INSIDE LANE	LANE 8
D.	653+24.5	LT. 12.5'	S EDGE OF N INSIDE LANE	LANE 8
E.	653+51	LT. 12.5'	N EDGE OF CENTER LANE	LANE 9
F.	653+51	RT. 12.5'	S EDGE OF CENTER LANE	LANE 9
G.	653+66.5	RT. 12.5'	N EDGE OF S INSIDE LANE	LANE 10
H.	643+66.5	RT. 37.5'	S EDGE OF S INSIDE LANE	LANE 10
I.	653+79	RT. 37.5'	N EDGE OF S OUTSIDE LANE	LANE 11
J.	653+79	RT. 50'	S EDGE OF S OUTSIDE LANE	LANE 11
K.	654+00	RT. 50'	S EDGE OF RUNWAY	LANE 11

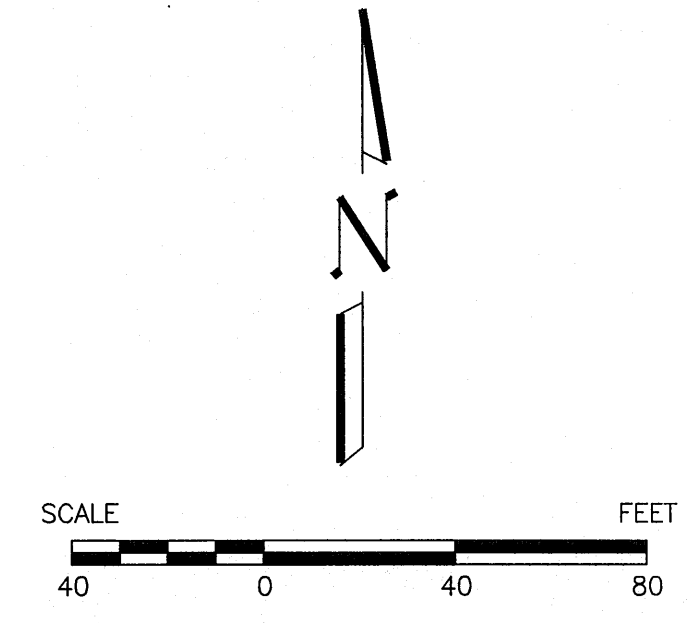
PAVEMENT LAYOUT POINTS			
NUMBER	STATION	OUT	REMARK
13	650+30.00	RT. 50.00'	P.I.
14	651+80.00	RT. 60.00'	RAD. PT.
15	651+80.00	RT. 190.00'	PT. OF RAD.
16	901+90.00	RT. 52.50'	RAD. PT.
17	903+40.00	RT. 37.50'	P.I.

THE LOCATION OF THE BITUMINOUS TO PCC PAVEMENT JOINT IS UNDER REVIEW. FINAL JOINT LOCATION SHALL BE DETERMINED BY THE RESIDENT ENGINEER IN THE FIELD AT THE TIME OF CONSTRUCTION.

TEMPORARY RUNWAY 10-28 / TAXIWAY P

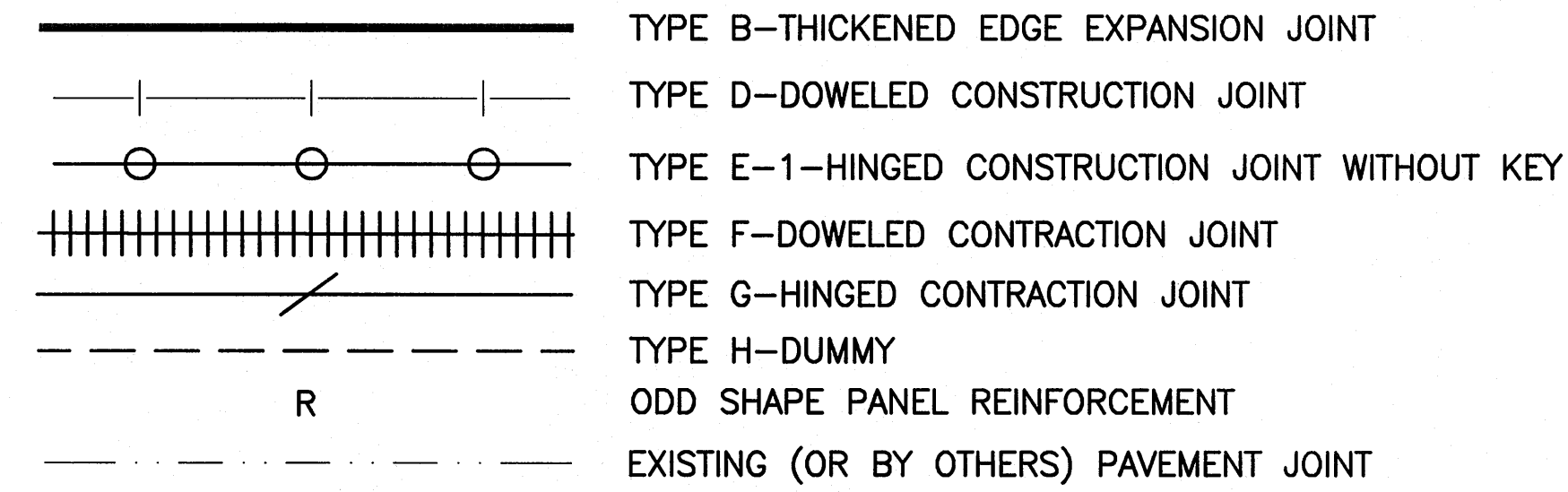
JOINT SYMBOL LEGEND

- TYPE B-THICKENED EDGE EXPANSION JOINT
- TYPE D-DOWELED CONSTRUCTION JOINT
- TYPE E-1-HINGED CONSTRUCTION JOINT WITHOUT KEY
- TYPE F-DOWELED CONTRACTION JOINT
- TYPE G-HINGED CONTRACTION JOINT
- TYPE H-DUMMY
- ODD SHAPE PANEL REINFORCEMENT
- EXISTING (OR BY OTHERS) PAVEMENT JOINT



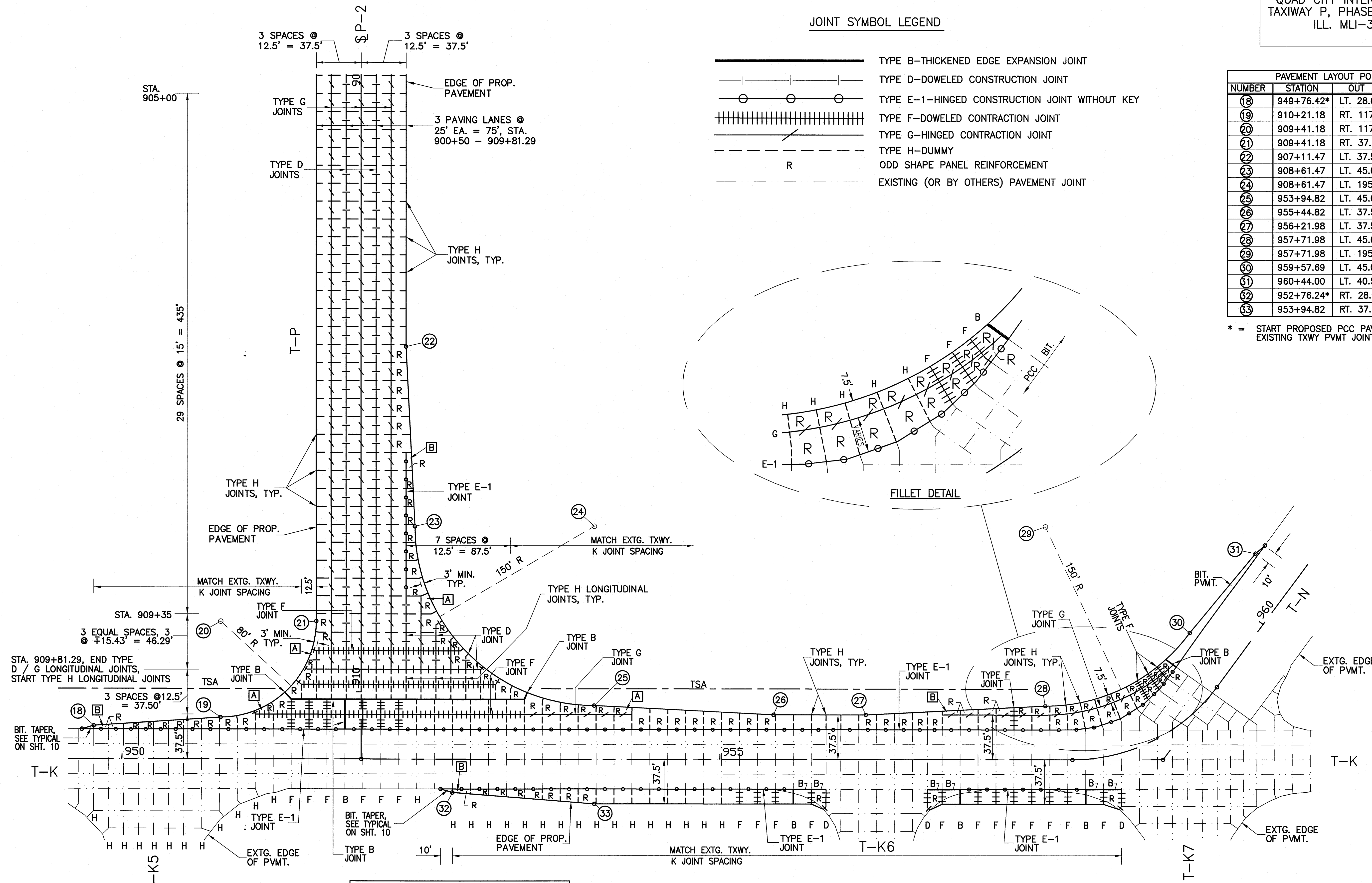
- NOTES:
- SEE SHEET 18 FOR JOINT NOTES.
 - SEE SHEET 21 FOR JOINT DETAILS.
 - [A] = STUB, SEE FILLET STUB DETAIL.

JOINT SYMBOL LEGEND



PAVEMENT LAYOUT POINTS			
NUMBER	STATION	OUT	REMARK
18	949+76.42*	LT. 28.00'	P.I.
19	910+21.18	RT. 117.50'	RAD. PT.
20	909+41.18	RT. 117.50'	PT. OF RAD.
21	909+41.18	RT. 37.50'	RAD. PT.
22	907+11.47	LT. 37.50'	P.I.
23	908+61.47	LT. 45.00'	RAD. PT.
24	908+61.47	LT. 195.00'	PT. OF RAD.
25	953+94.82	LT. 45.00'	RAD. PT.
26	955+44.82	LT. 37.50'	P.I.
27	956+21.98	LT. 37.50'	P.I.
28	957+71.98	LT. 45.00'	RAD. PT.
29	957+71.98	LT. 195.00'	PT. OF RAD.
30	959+57.69	LT. 45.00'	RAD. PT.
31	960+44.00	LT. 40.50'	P.I.
32	952+76.24*	RT. 28.00'	P.I.
33	953+94.82	RT. 37.50'	P.I.

* = START PROPOSED PCC PAVEMENT AT CLOSEST EXISTING TAXIWAY PVMT JOINT TO THIS STATION.

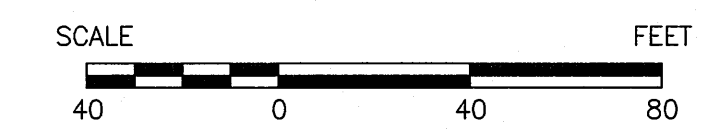


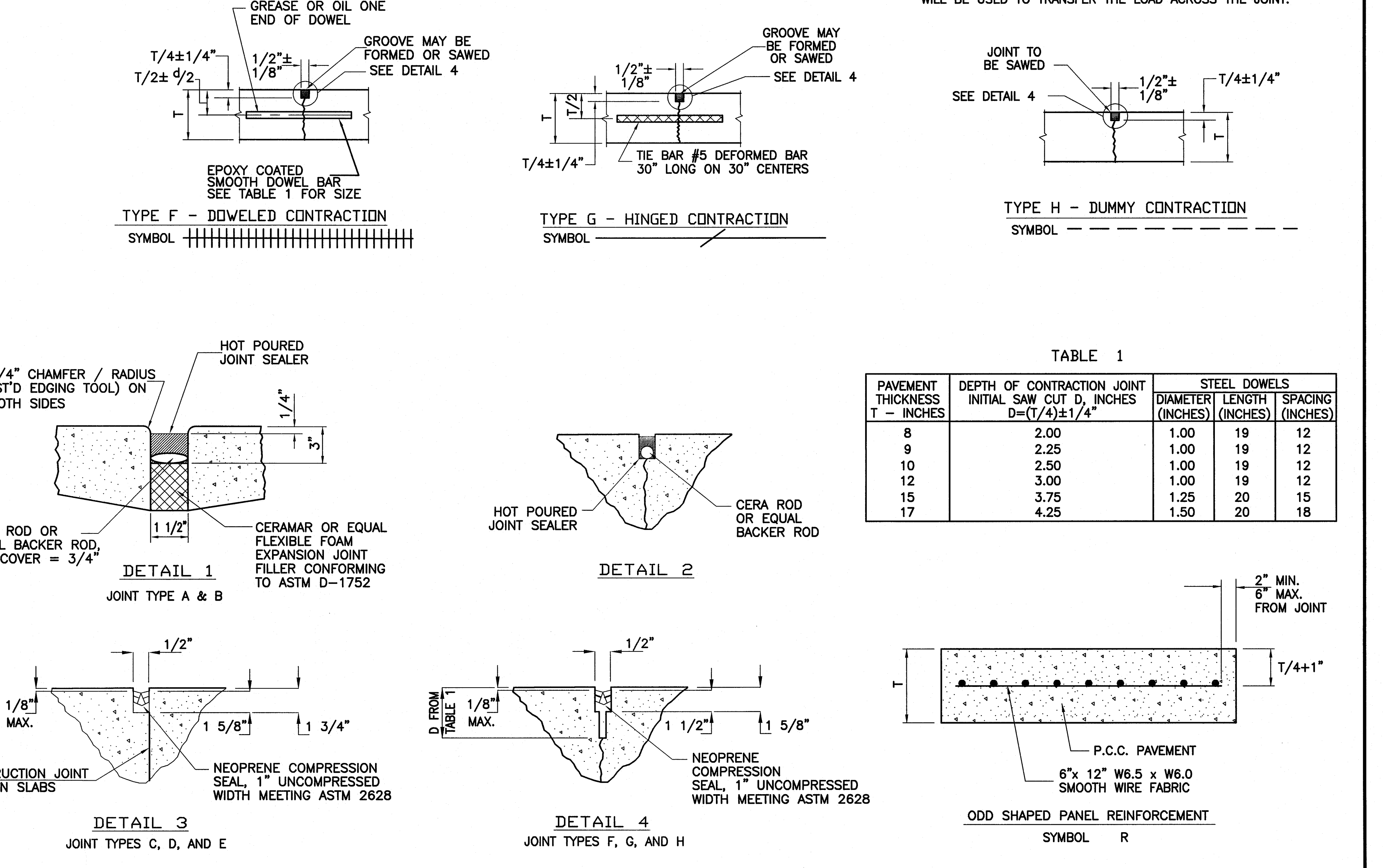
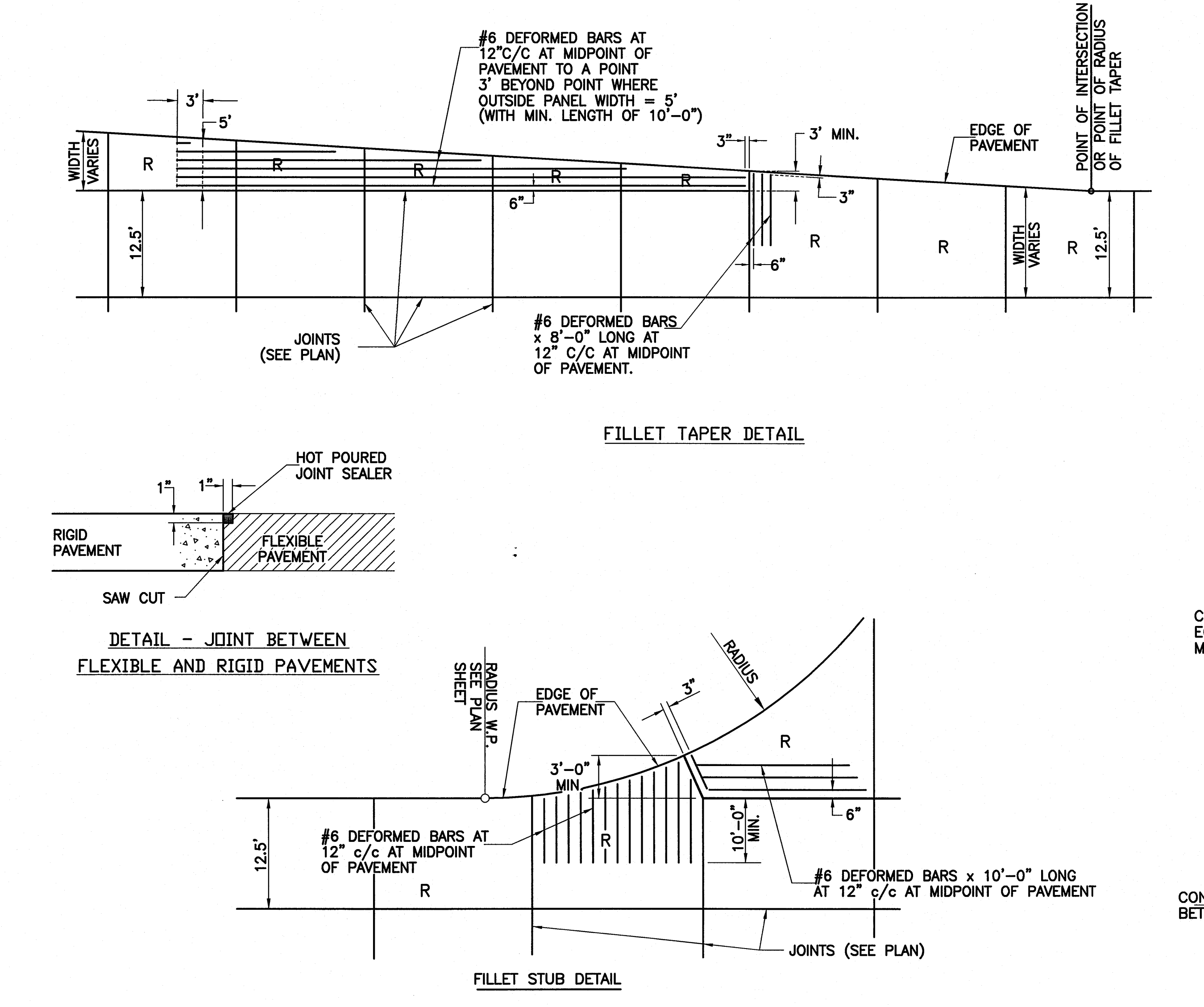
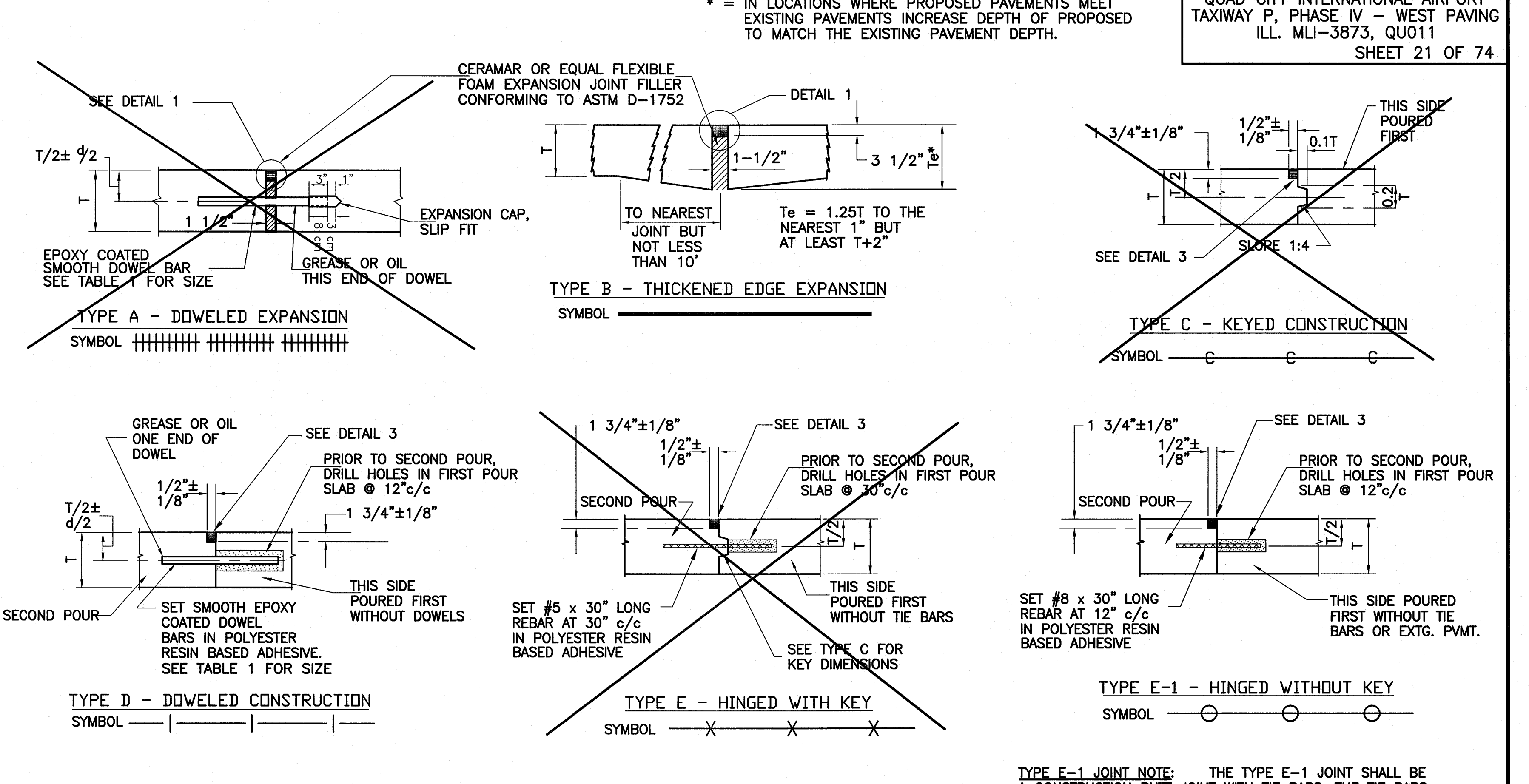
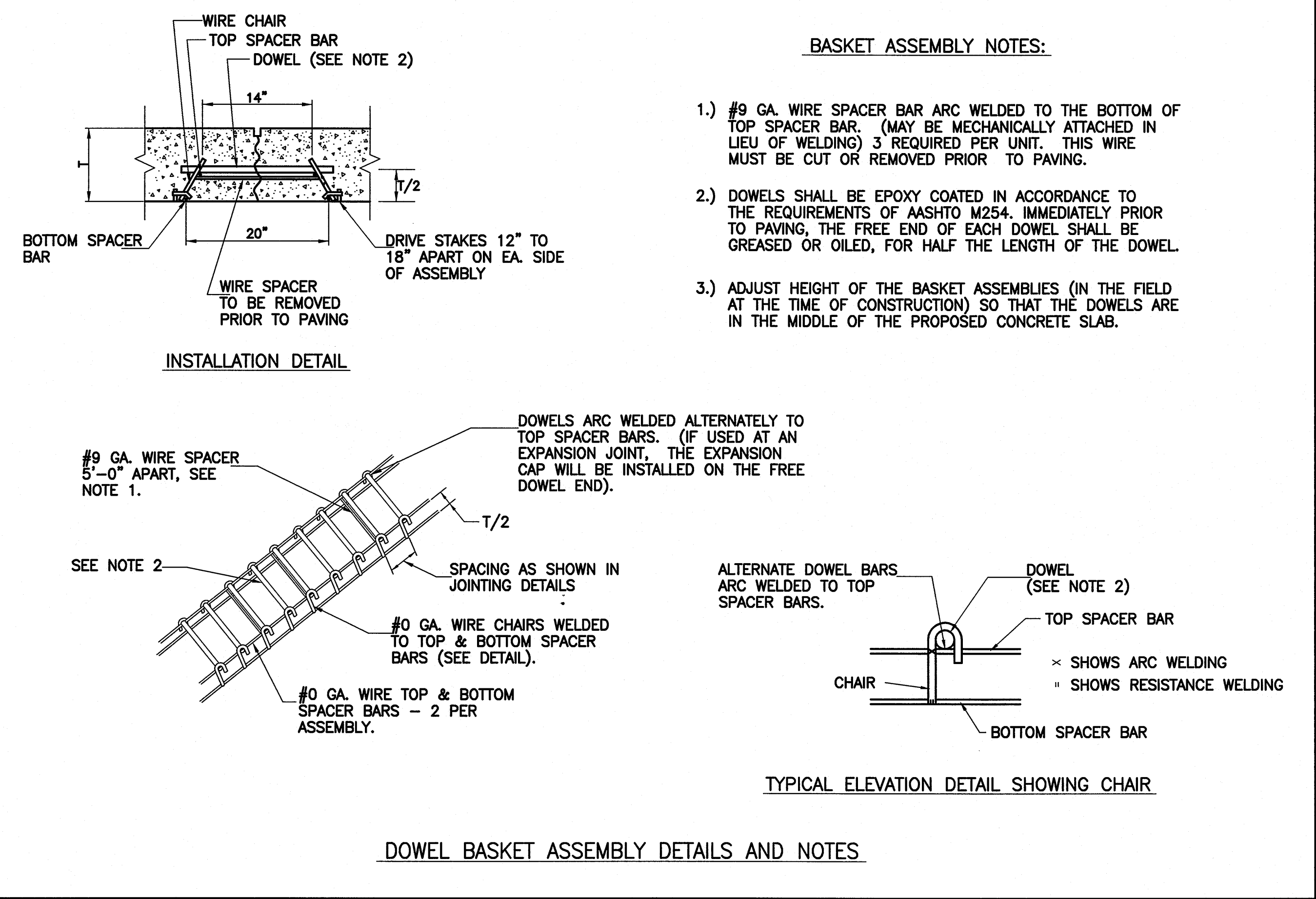
- NOTES:
- SEE SHEET 18 FOR JOINT NOTES.
 - SEE SHEET 21 FOR JOINT DETAILS.
 - [A] = STUB, SEE FILLET STUB DETAIL.
 - [B] = TAPER, SEE FILLET TAPER DETAIL.

STA. 900+50 TO STA. 909+81.29:
INSTALL PAVING LANES PARALLEL
TO T-P CENTER LINE.

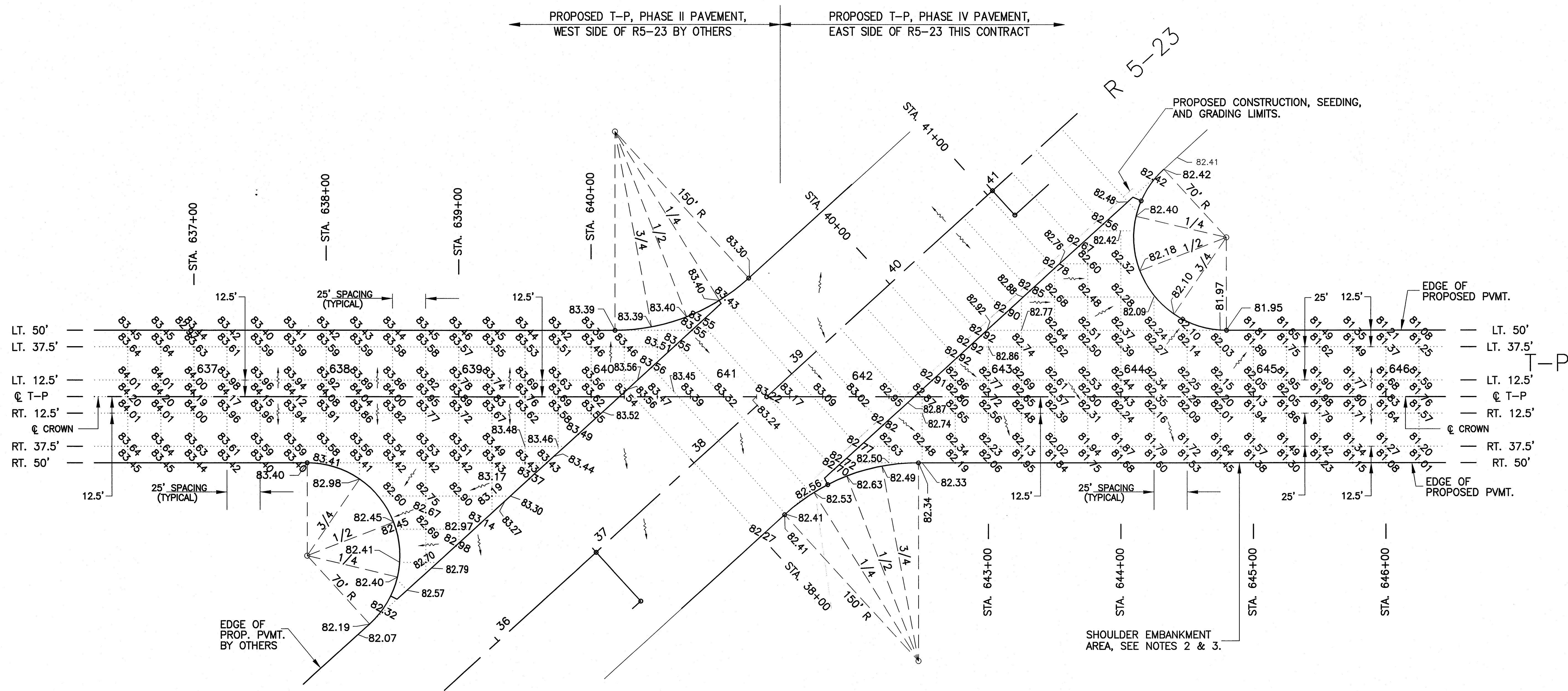
STA. 909+81.29 TO STA. 910+31.28:
INSTALL PAVING LANES PERPENDICULAR
TO T-P CENTER LINE.

TAXIWAY P / TAXIWAY K





G:\AIRPORT\A09T002 TP E PVMT\JOINTS1.dwg, 7/8/2009 10:17:10 AM, jeffm

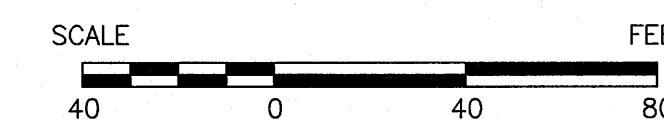


NOTES:

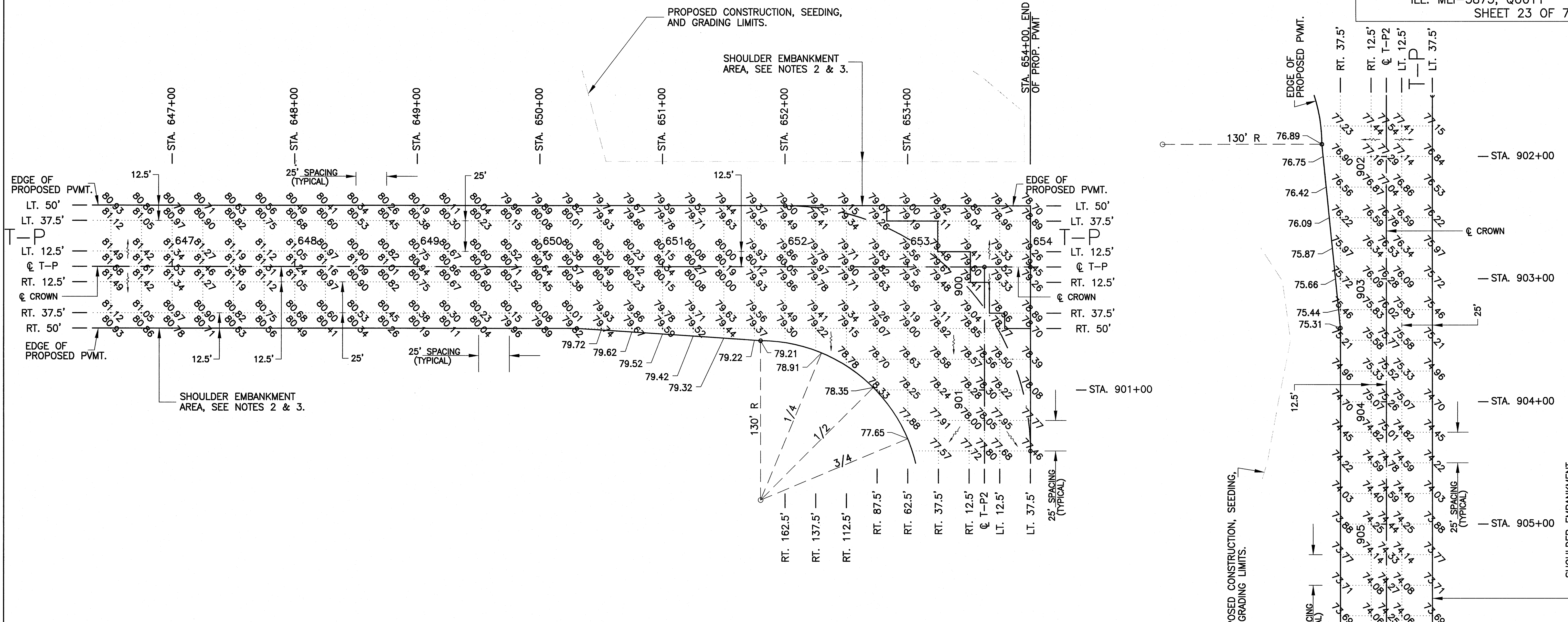
- 1.) CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
- 2.) GRADES SHOWN ON THIS SHEET AT EDGE OF PAVEMENT ARE PROPOSED SURFACE ELEVATIONS OF PAVEMENT. PROPOSED EARTH SHOULDER FINAL GRADE AT EDGE OF PVMT. IS 0.125' BELOW EDGE OF PROPOSED PAVEMENT GRADE.
- 3.) SEE CROSS SECTIONS FOR SURFACE ELEVATIONS OF THE PROPOSED GROUND IN THE SHOULDER AREA.
- 4.) 76.80 = PROPOSED ELEVATION 576.80.
- 5.) 76.60 = EXISTING ELEVATION 576.60.

TAXIWAY P / RUNWAY 5-23 INTERSECTION

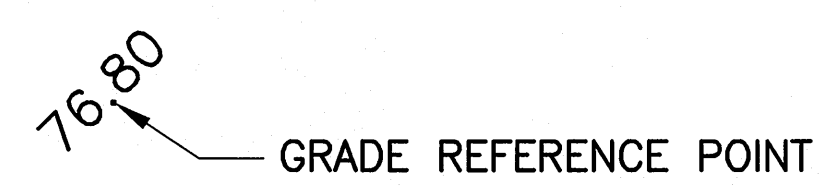
76.80 — GRADE REFERENCE POINT



QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE IV - EAST PAVING
ILL. MLI-3873, QU011
SHEET 23 OF 74

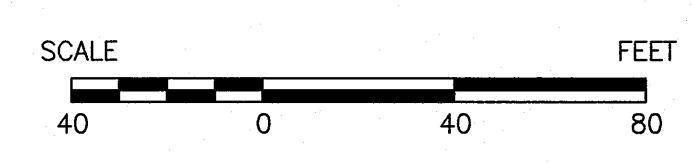


TAXIWAY P / TEMPORARY RUNWAY 10-28



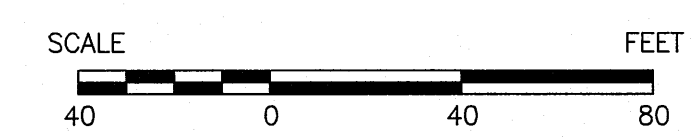
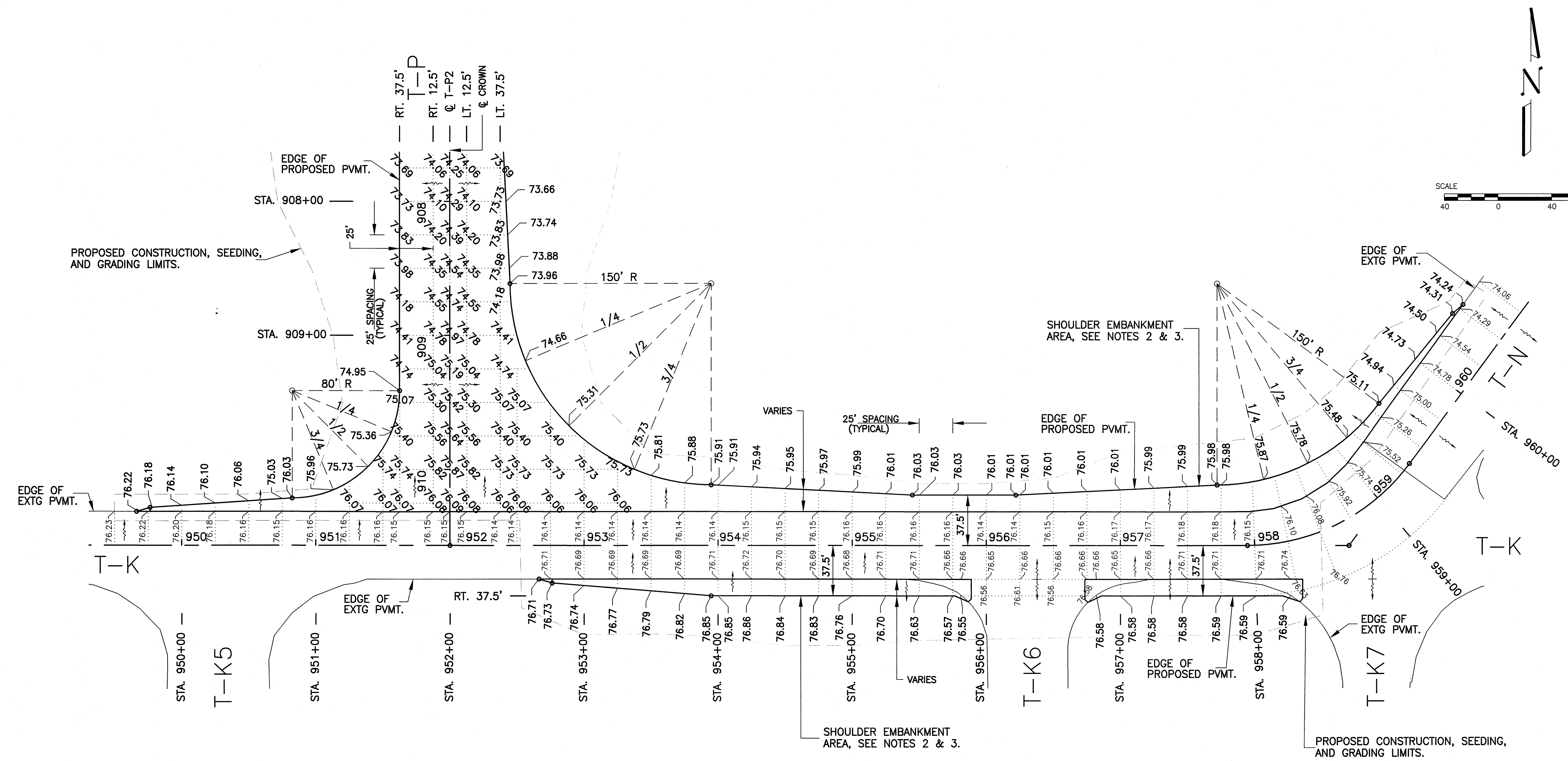
NOTES:

- 1.) CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
- 2.) GRADES SHOWN ON THIS SHEET AT EDGE OF PAVEMENT ARE PROPOSED SURFACE ELEVATIONS OF PAVEMENT. PROPOSED EARTH SHOULDER FINAL GRADE AT EDGE OF PVMT. IS 0.125' BELOW EDGE OF PROPOSED PAVEMENT GRADE.
- 3.) SEE CROSS SECTIONS FOR SURFACE ELEVATIONS OF THE PROPOSED GROUND IN THE SHOULDER AREA.
- 4.) 76.80 = PROPOSED ELEVATION 576.80.
- 5.) 76.60 = EXISTING ELEVATION 576.60.



TAXIWAY P
TAXIWAY P STAKING PLAN

G:\AIRPORT\A09T002 TP E PVMT\STAKING.dwg, 7/8/2009 10:17:57 AM, jefrm



TAXIWAY P / TAXIWAY K / TAXIWAY N

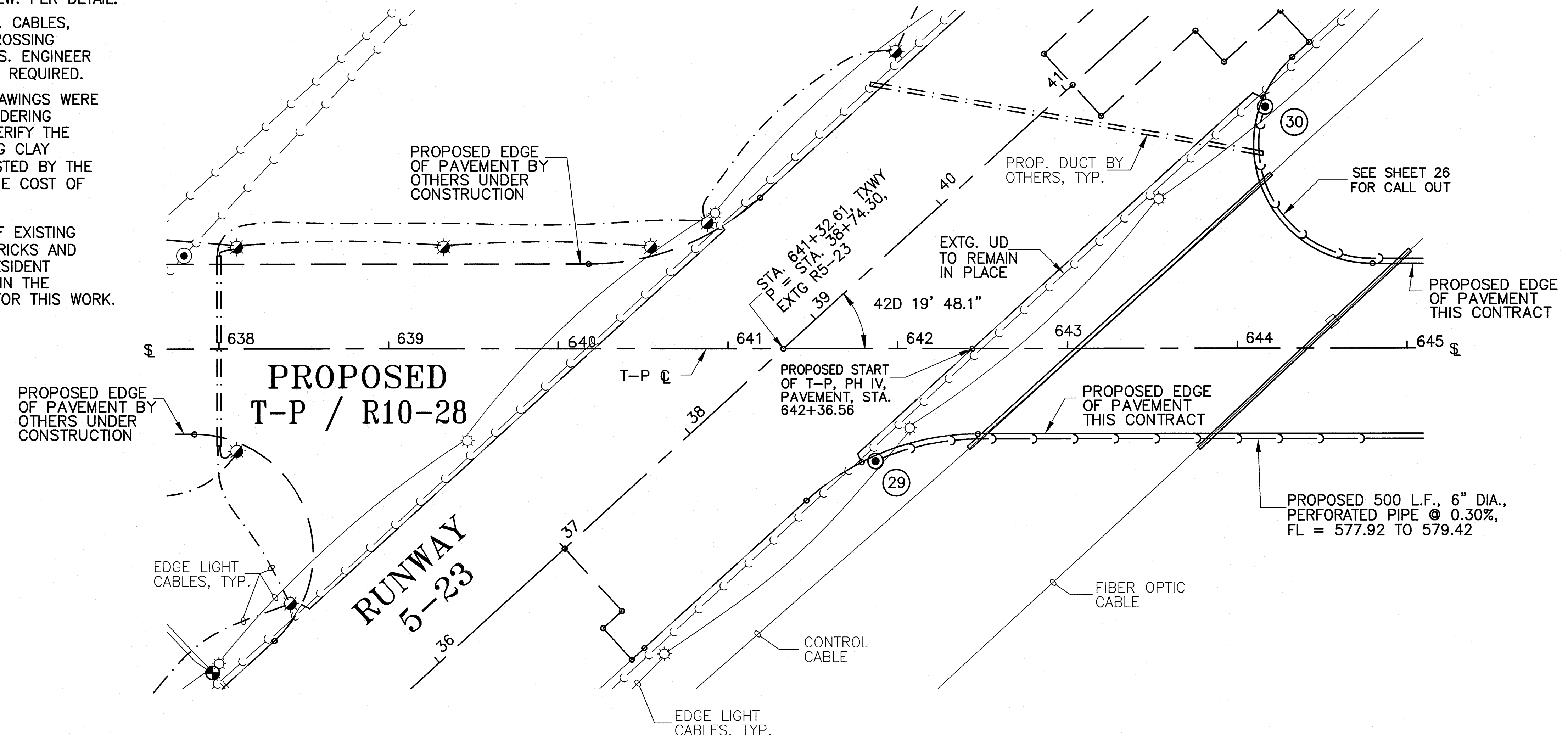
NOTES:

- 1.) CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
- 2.) GRADES SHOWN ON THIS SHEET AT EDGE OF PAVEMENT ARE PROPOSED SURFACE ELEVATIONS OF PAVEMENT. PROPOSED EARTH SHOULDER FINAL GRADE AT EDGE OF PVMT. IS 0.125' BELOW EDGE OF PROPOSED PAVEMENT GRADE.
- 3.) SEE CROSS SECTIONS FOR SURFACE ELEVATIONS OF THE PROPOSED GROUND IN THE SHOULDER AREA.
- 4.) 76.80 = PROPOSED ELEVATION 576.80.
- 5.) 76.60 = EXISTING ELEVATION 576.60.

76.80
GRADE REFERENCE POINT

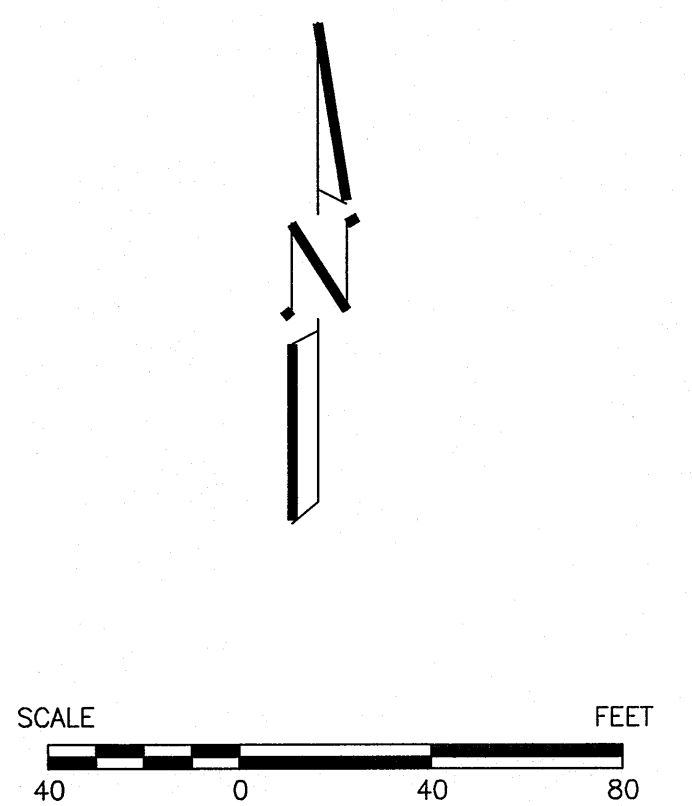
GENERAL DRAINAGE NOTES:

1. SEE PLAN & PROFILE SHEETS FOR THE LOCATION OF EXISTING UNDERGROUND UTILITIES. CONTRACTOR SHALL HAND DIG AROUND ALL EXISTING UNDERGROUND UTILITIES. CONTRACTOR SHALL BE RESPONSIBLE FOR COST OF REPAIRING ALL DAMAGE UTILITIES.
2. OUTLET PROPOSED UNDER DRAIN INTO EXISTING / PROPOSED STM. SEW. PER DETAIL.
3. CONTRACTOR TO EXCAVATE AND EXPOSE EXISTING STORM PIPES, ELEC. CABLES, AND ELEC. DUCT BANKS AT PROPOSED MANHOLE, INLET, AND PIPE CROSSING LOCATIONS TO FIELD VERIFY VERTICAL LOCATIONS OF EXISTING UTILITIES. ENGINEER MAY ADJUST SLOPES AND TIE-INS AT THE TIME OF CONSTRUCTION AS REQUIRED.
4. THE LOCATIONS OF THE EXISTING UNDERDRAINS SHOWN ON THESE DRAWINGS WERE TAKEN FROM AIRPORT RECORDS AND ARE APPROXIMATE. PRIOR TO ORDERING MATERIALS, THE CONTRACTOR SHALL EXCAVATE, EXPOSE, AND FIELD VERIFY THE HORIZONTAL LOCATION, VERTICAL LOCATION, AND SIZE OF THE EXISTING CLAY UNDERDRAINS. ELEVATIONS, SLOPES, AND PIPE SIZES SHALL BE ADJUSTED BY THE RESIDENT ENGINEER IN THE FIELD AT THE TIME OF CONSTRUCTION. THE COST OF THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT RATES.
5. PLUG EXISTING PIPES TO BE ABANDONED IN PLACE. THE PLUGGING OF EXISTING PIPES TO BE ABANDONED IN PLACE SHALL BE ACCOMPLISHED WITH BRICKS AND GROUT (3,500 LBS. @ 28 DAY MIX) TO THE SATISFACTION OF THE RESIDENT ENGINEER. COST FOR PLUGGING EXISTING PIPES SHALL BE INCLUDED IN THE CONTRACT 701 UNIT PRICES. NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK.



LEGEND:

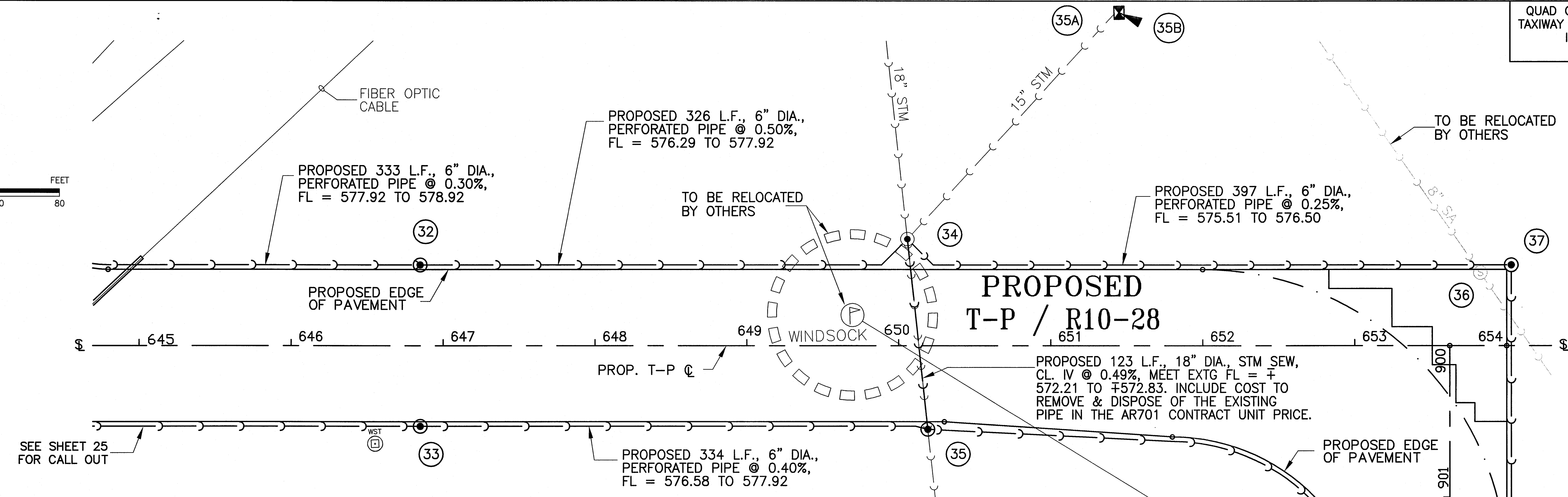
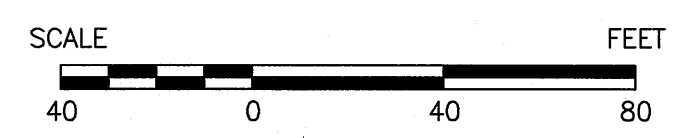
- EXTG EDGE LIGHT
- EXTG GUIDANCE SIGN
- EXTG ELECTRICAL CABLE
- EXTG STORM SEWER, FIELD TILE OR UNDERDRAIN
- EXTG INLET
- EXTG OUTLET
- EXTG STORM MANHOLE
- EXTG SANITARY MANHOLE
- EXTG DRAINAGE DITCH
- EXTG CONDUIT OR DUCT BANK
- PROPOSED MANHOLE
- PROPOSED INLET
- PROPOSED FLARED END SECTION
- PROPOSED STORM SEWER, SANITARY SEWER OR UNDERDRAIN
- PROPOSED / EXISTING STRUCTURE NUMBER
- PROPOSED ELECTRICAL CABLE OR DUCT BY OTHERS
- OR PROPOSED EDGE LIGHT BY OTHERS



STRUCTURE SCHEDULE

NUMBER	LOCATION	TYPE	℄ ELEV.	LID OR GRATE ELEV.	REMARK
29	38+70, RT 85.28'	MANHOLE, SPL., 2' DIA.	579.42'	582.25'	AR751570, EXTG. UN INTO PROP. MH
30	41+80, RT 85.97'	MANHOLE, SPL., 2' DIA.	578.92'	582.12'	AR751570, EXTG. UN INTO PROP. MH

G:\Airport\A09T002 TP E P\MT\DRAINAGE.dwg, 7/8/2009 10:18:50 AM, jefrn



LEGEND:

- EXTG EDGE LIGHT
- EXTG GUIDANCE SIGN
- EXTG ELECTRICAL CABLE
- EXTG STORM SEWER, FIELD TILE OR UNDERDRAIN
- EXTG INLET
- EXTG OUTLET
- EXTG STORM MANHOLE
- EXTG SANITARY MANHOLE
- EXTG DRAINAGE DITCH
- EXTG CONDUIT OR DUCT BANK
- PROPOSED MANHOLE
- PROPOSED INLET
- PROPOSED FLARED END SECTION
- PROPOSED STORM SEWER, SANITARY SEWER OR UNDERDRAIN
- PROPOSED / EXISTING STRUCTURE NUMBER

NOTE:
 1. SEE SHEET 25 FOR GENERAL DRAINAGE.

STRUCTURE SCHEDULE

NUMBER	LOCATION	TYPE	℄ ELEV.	LID OR GRATE ELEV.	REMARK
32	646+85, LT 53'	MANHOLE, SPL., 2' DIA.	577.92'	580.55'	AR751570
33	646+85, RT 53'	MANHOLE, SPL., 2' DIA.	577.92'	580.55'	AR751570
34	650+05.76, LT 70'	MANHOLE, 4' DIA. WITH FLAT SLAB TOP	EXISTING FLOWLINE, ±572.83'	578.95'	BY OTHERS, ADJUST EL. IF REQD. AR751943
35	650+19.15, RT 55'	MANHOLE, 4' DIA. WITH FLAT SLAB TOP	MEET EXTG FLOWLINE, ±572.21'	579.46'	NEENAH R-3493-A FR & SOLID LID, AR751540
35A	651+92, LT 270'	EXTG. SINGLE INLET	EXTG = 573.37'	EXTG = 575.50'	TO REMAIN IN PLACE
35B*	651+97.5, LT 267.4'	PRC FLARED END SECTION, 12" DIA.*	573.50'	---	AR752412
36	653+82.65, LT 47.91'	EXTG. SANITARY SEWER MANHOLE	EXTG = 571.76'	EXTG = 576.31'	TO BE REMOVED BY OTHERS
37	654+03, LT 53'	MANHOLE, SPL., 2' DIA.	575.51'	578.55'	AR751570
38	902+64, RT 48.10'	MANHOLE, SPL., 2' DIA.	573.05'	575.69'	AR751570
39	903+15, LT 40.5'	MANHOLE, SPL., 2' DIA.	572.31'	574.94'	AR751570

* = ITEM 35B (FES, 12" DIA.) IS A TEMPORARY ITEM. THIS TEMPORARY ITEM WILL BE REMOVED DURING A FUTURE PROJECT WHEN THE PERMANENT RUNWAY 13-31 SAFETY AREA GRADING CAN BE COMPLETED. THE PERMANENT RUNWAY 13-31 SAFETY AREA GRADING CAN NOT BE COMPLETED WHEN TEMPORARY RUNWAY 10-28 IS IN OPERATION.

PROPOSED 486 L.F., 6" DIA., PERFORATED PIPE @ 0.90%, FL = 568.68 TO 573.05

PROPOSED 433 L.F., 6" DIA., PERFORATED PIPE @ 0.85%, FL = 568.63 TO 572.31

PROP. POWER CABLE & DUCT BY OTHERS, TYP.

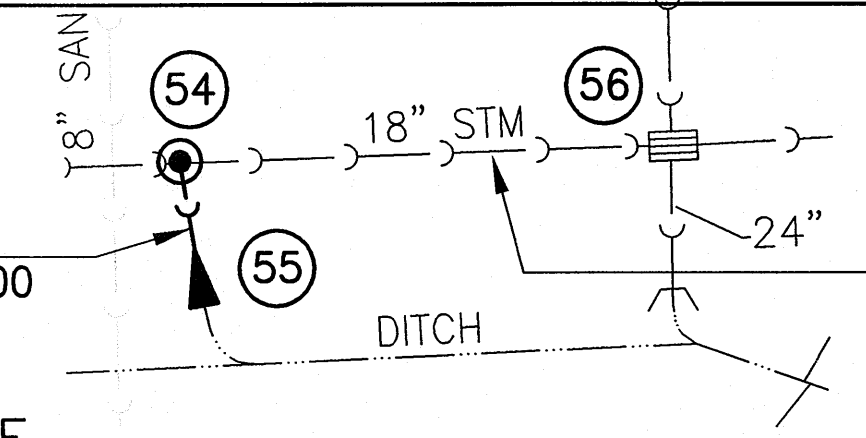
PROPOSED SURVEY LINE P2

G:\AIRPORT\A09T002 TP E P\MT\DRAINAGE.dwg, 7/8/2009 10:19:07 AM, jefm

- LEGEND:**
- EXTG EDGE LIGHT
 - EXTG GUIDANCE SIGN
 - EXTG ELECTRICAL CABLE
 - EXTG STORM SEWER, FIELD TILE OR UNDERDRAIN
 - EXTG INLET
 - EXTG OUTLET
 - EXTG STORM MANHOLE
 - EXTG SANITARY MANHOLE
 - EXTG DRAINAGE DITCH
 - EXTG CONDUIT OR DUCT BANK
 - PROPOSED MANHOLE
 - PROPOSED INLET
 - PROPOSED FLARED END SECTION
 - PROPOSED STORM SEWER, SANITARY SEWER OR UNDERDRAIN
 - PROPOSED / EXISTING STRUCTURE NUMBER

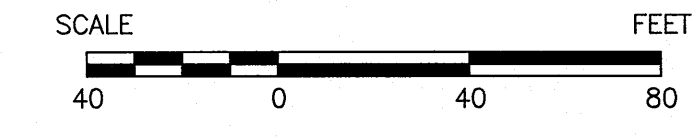
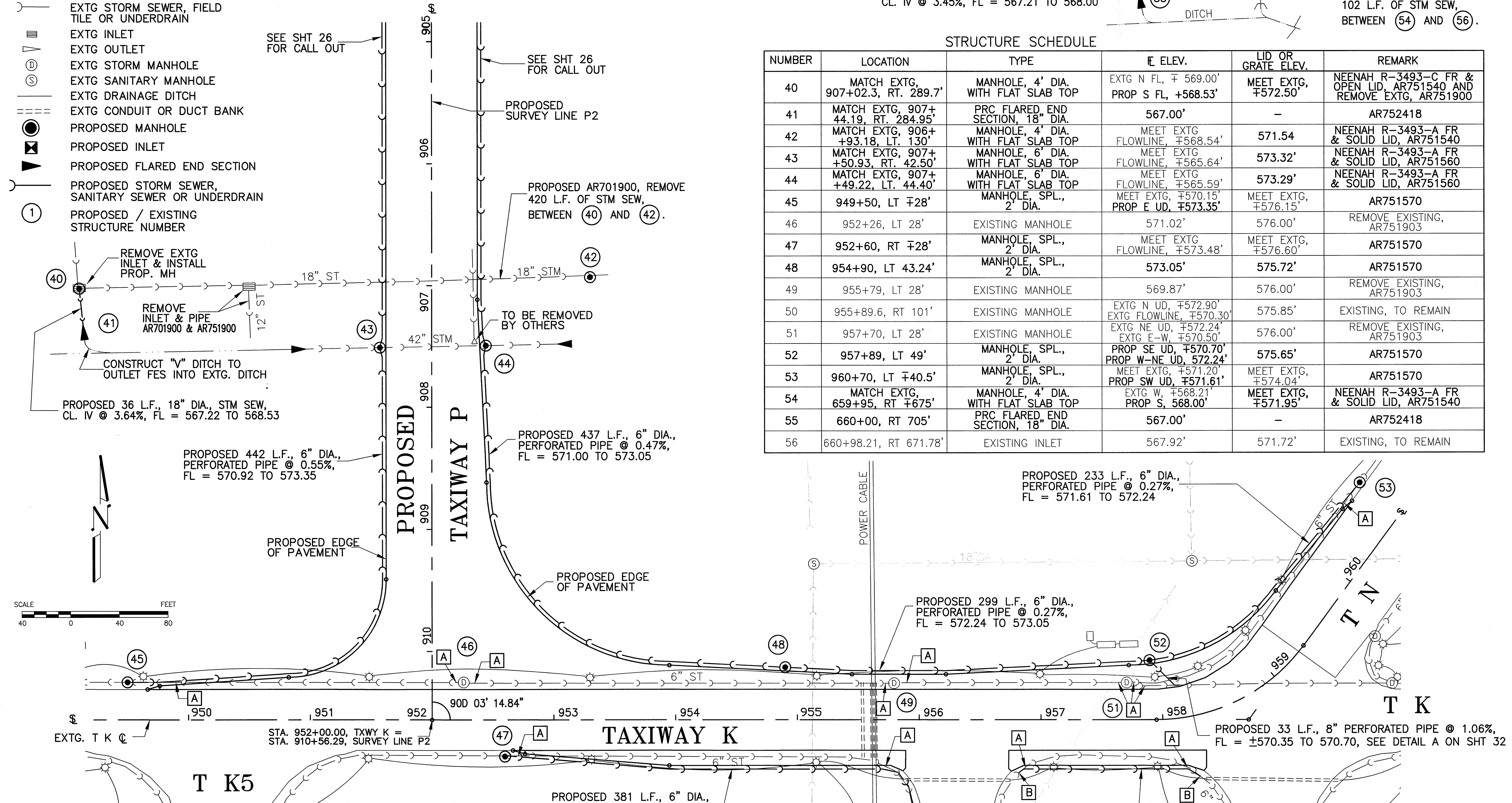
STRUCTURE SCHEDULE

NUMBER	LOCATION	TYPE	E ELEV.	LID OR GRATE ELEV.	REMARK
40	MATCH EXTG, 907+02.3, RT. 289.7'	MANHOLE, 4' DIA. WITH FLAT SLAB TOP	EXTG N FL, ± 569.00' PROP S FL, +568.53'	MEET EXTG, ±572.50'	NEENAH R-3493-C FR & OPEN LID, AR751540 AND REMOVE EXTG, AR751900
41	MATCH EXTG, 907+44.19, RT. 284.95'	PRC FLARED, END SECTION, 18" DIA.	567.00'	-	AR752418
42	MATCH EXTG, 906+93.18, LT. 130'	MANHOLE, 4' DIA. WITH FLAT SLAB TOP	MEET EXTG FLOWLINE, ±568.54'	571.54	NEENAH R-3493-A FR & SOLID LID, AR751540
43	MATCH EXTG, 907+50.93, RT. 42.50'	MANHOLE, 6' DIA. WITH FLAT SLAB TOP	MEET EXTG FLOWLINE, ±565.64'	573.32'	NEENAH R-3493-A FR & SOLID LID, AR751560
44	MATCH EXTG, 907+49.22, LT. 44.40'	MANHOLE, 6' DIA. WITH FLAT SLAB TOP	MEET EXTG FLOWLINE, ±565.59'	573.29'	NEENAH R-3493-A FR & SOLID LID, AR751560
45	949+50, LT ±28'	MANHOLE, SPL., 2' DIA.	MEET EXTG, ±570.15' PROP E UD, ±573.35'	MEET EXTG, ±576.15'	AR751570
46	952+26, LT 28'	EXISTING MANHOLE	571.02'	576.00'	REMOVE EXISTING, AR751903
47	952+60, RT ±28'	MANHOLE, SPL., 2' DIA.	MEET EXTG FLOWLINE, ±573.48'	MEET EXTG, ±576.60'	AR751570
48	954+90, LT 43.24'	MANHOLE, SPL., 2' DIA.	573.05'	575.72'	AR751570
49	955+79, LT 28'	EXISTING MANHOLE	569.87'	576.00'	REMOVE EXISTING, AR751903
50	955+89.6, RT 101'	EXISTING MANHOLE	EXTG N UD, ±572.90' EXTG FLOWLINE, ±570.30'	575.85'	EXISTING, TO REMAIN
51	957+70, LT 28'	EXISTING MANHOLE	EXTG NE UD, ±572.24' EXTG E-W, ±570.50'	576.00'	REMOVE EXISTING, AR751903
52	957+89, LT 49'	MANHOLE, SPL., 2' DIA.	PROP SE UD, ±570.70' PROP W-NE UD, 572.24'	575.65'	AR751570
53	960+70, LT ±40.5'	MANHOLE, SPL., 2' DIA.	MEET EXTG, ±571.20' PROP SW UD, ±571.61'	MEET EXTG, ±574.04'	AR751570
54	MATCH EXTG, 659+95, RT ±675'	MANHOLE, 4' DIA. WITH FLAT SLAB TOP	EXTG W, ±568.21' PROP S, 568.00'	MEET EXTG, ±571.95'	NEENAH R-3493-A FR & SOLID LID, AR751540
55	660+00, RT 705'	PRC FLARED, END SECTION, 18" DIA.	567.00'	-	AR752418
56	660+98.21, RT 671.78'	EXISTING INLET	567.92'	571.72'	EXISTING, TO REMAIN



PROPOSED 23 L.F., 18" DIA., STM SEW. CL. IV @ 3.45%, FL = 567.21 TO 568.00

PROPOSED AR701900, REMOVE 102 L.F. OF STM SEW, BETWEEN (40) AND (42).

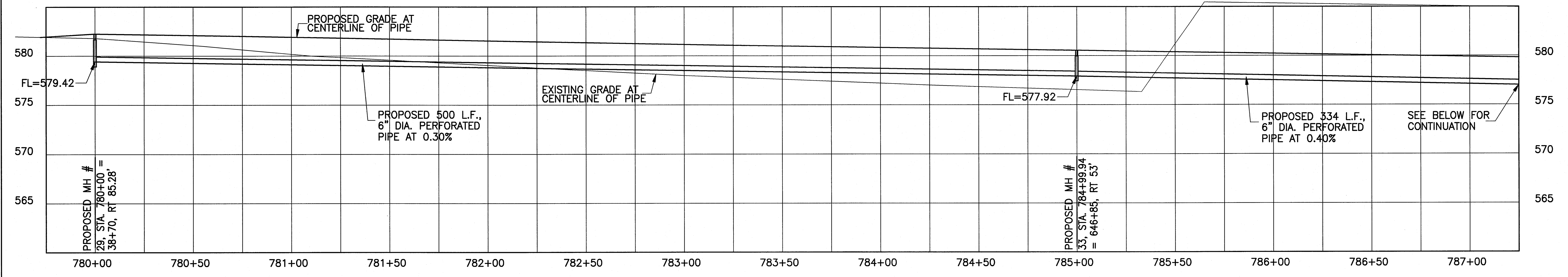


- NOTES:**
- SEE SHEET 25 FOR GENERAL DRAINAGE.
 - [A] = PLUG EXISTING PIPE PER GENERAL NOTE 5.
 - [B] = PROPOSED CONCRETE GROUT COLLAR PER DETAIL "A" ON SHEET 32. SEE GENERAL NOTES 3 & 4.

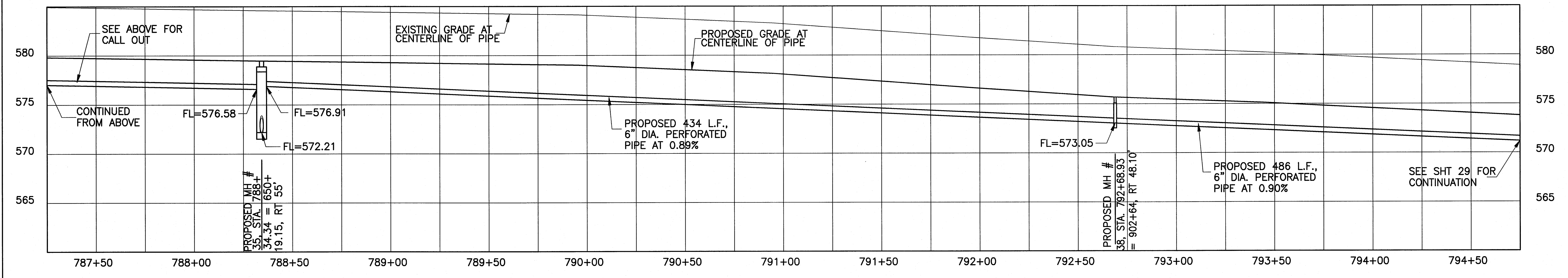
DRAINAGE PLAN, STA 950+50 TO STA 960+50

G:\Airports\A09T002 TP E P\MT\DRAINAGE.dwg, 7/8/2009 10:19:32 AM, jefm

RUNWAY 5-23, STA. 38+70 RIGHT TO TAXIWAY K STA. 946+50 LEFT UNDERDRAIN CENTERLINE PROFILE

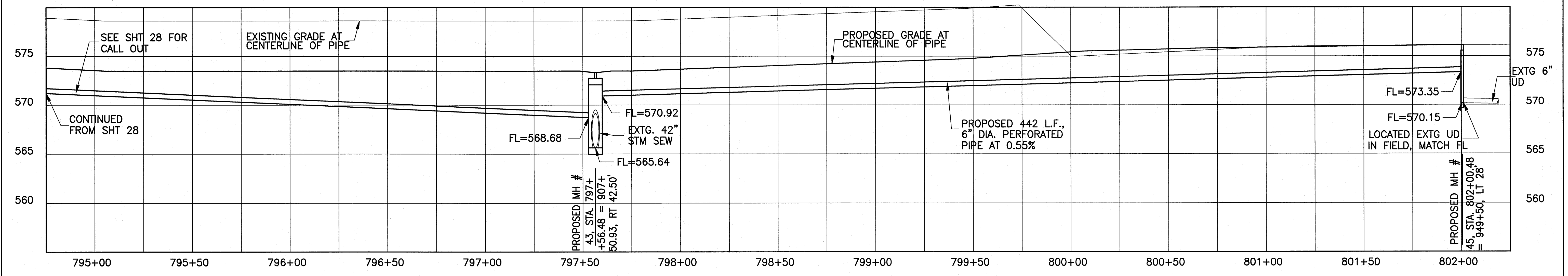


RUNWAY 5-23, STA. 38+70 RIGHT TO TAXIWAY K STA. 946+50 LEFT UNDERDRAIN CENTERLINE PROFILE, CONTINUED

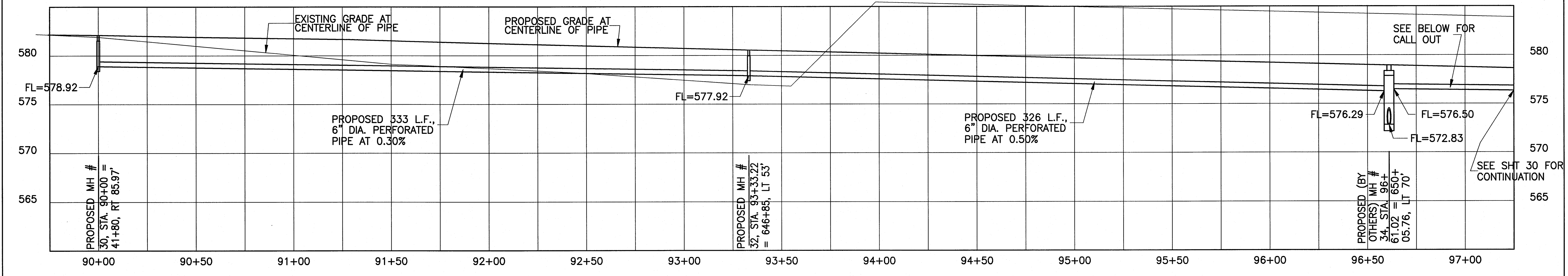


G:\airport\A09T002 TP E PVMT\DRAINAGE1.dwg, 7/8/2009 10:20:05 AM, jeffm

RUNWAY 5-23, STA. 38+70 RIGHT TO TAXIWAY K STA. 946+50 LEFT UNDERDRAIN CENTERLINE PROFILE, CONTINUED

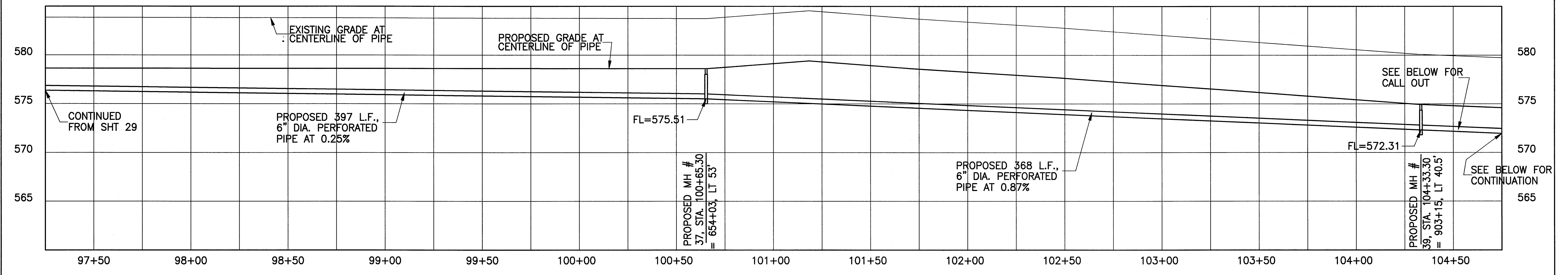


RUNWAY 5-23, STA. 41+80 RIGHT TO TAXIWAY N STA. 960+70 LEFT UNDERDRAIN CENTERLINE PROFILE

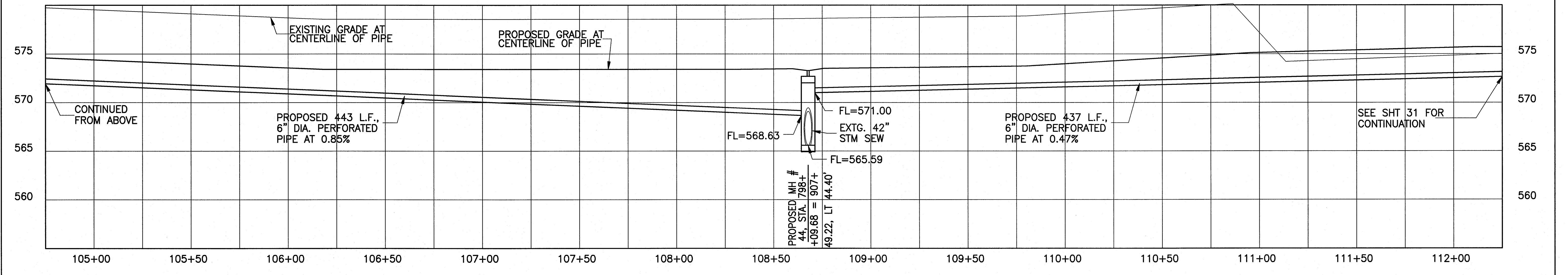


G:\airport\A09T002 TP E PVMT\RAINAGE1.dwg, 7/8/2009 10:20:12 AM, jeffm

RUNWAY 5-23, STA. 41+80 RIGHT TO TAXIWAY N STA. 960+70 LEFT UNDERDRAIN CENTERLINE PROFILE, CONTINUED

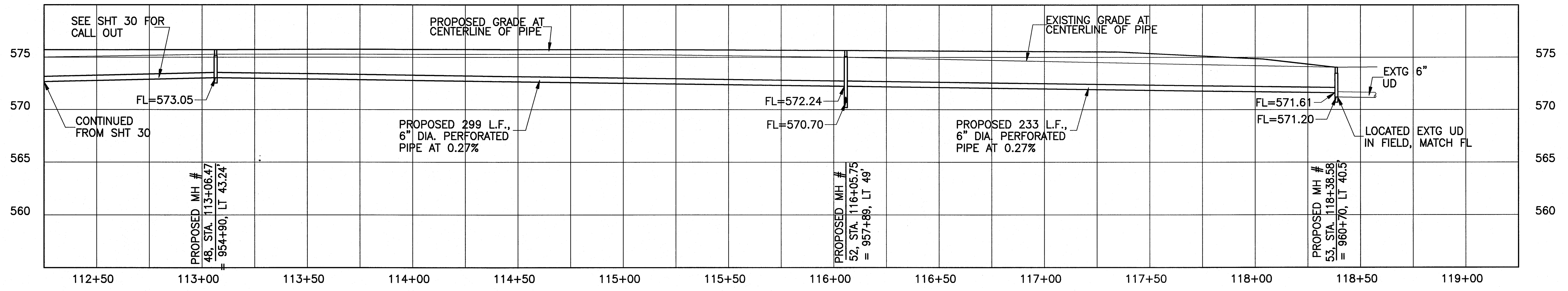


RUNWAY 5-23, STA. 41+80 RIGHT TO TAXIWAY N STA. 960+70 LEFT UNDERDRAIN CENTERLINE PROFILE, CONTINUED

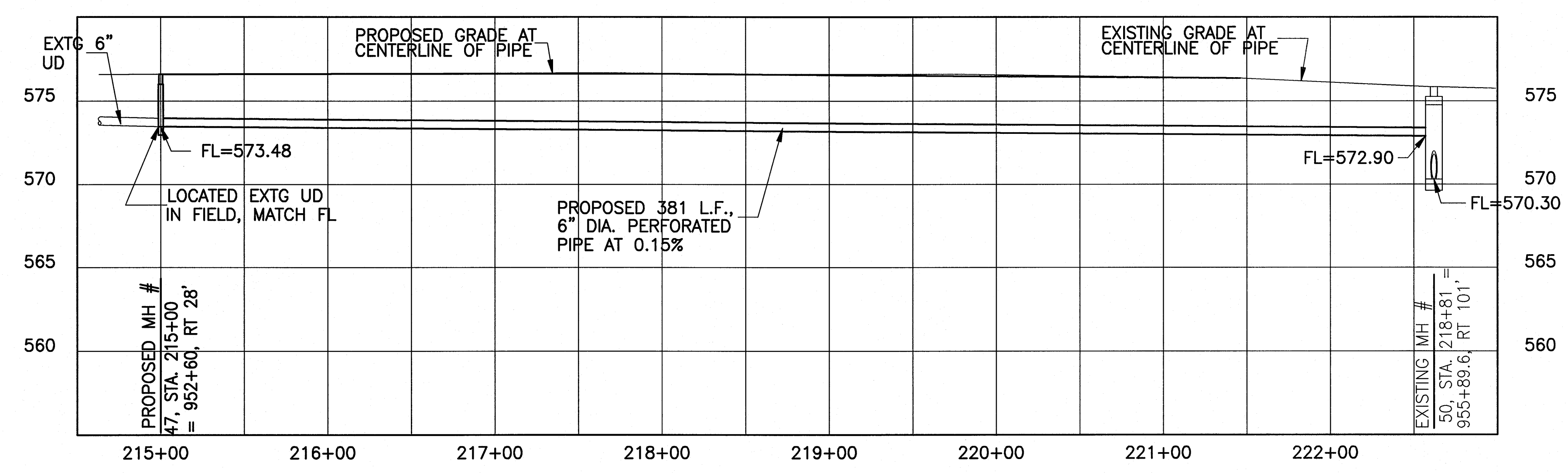


G:\airport\A09T002 TP E PVMT\DRAINAGE1.dwg, 7/8/2009 10:20:18 AM, jefm

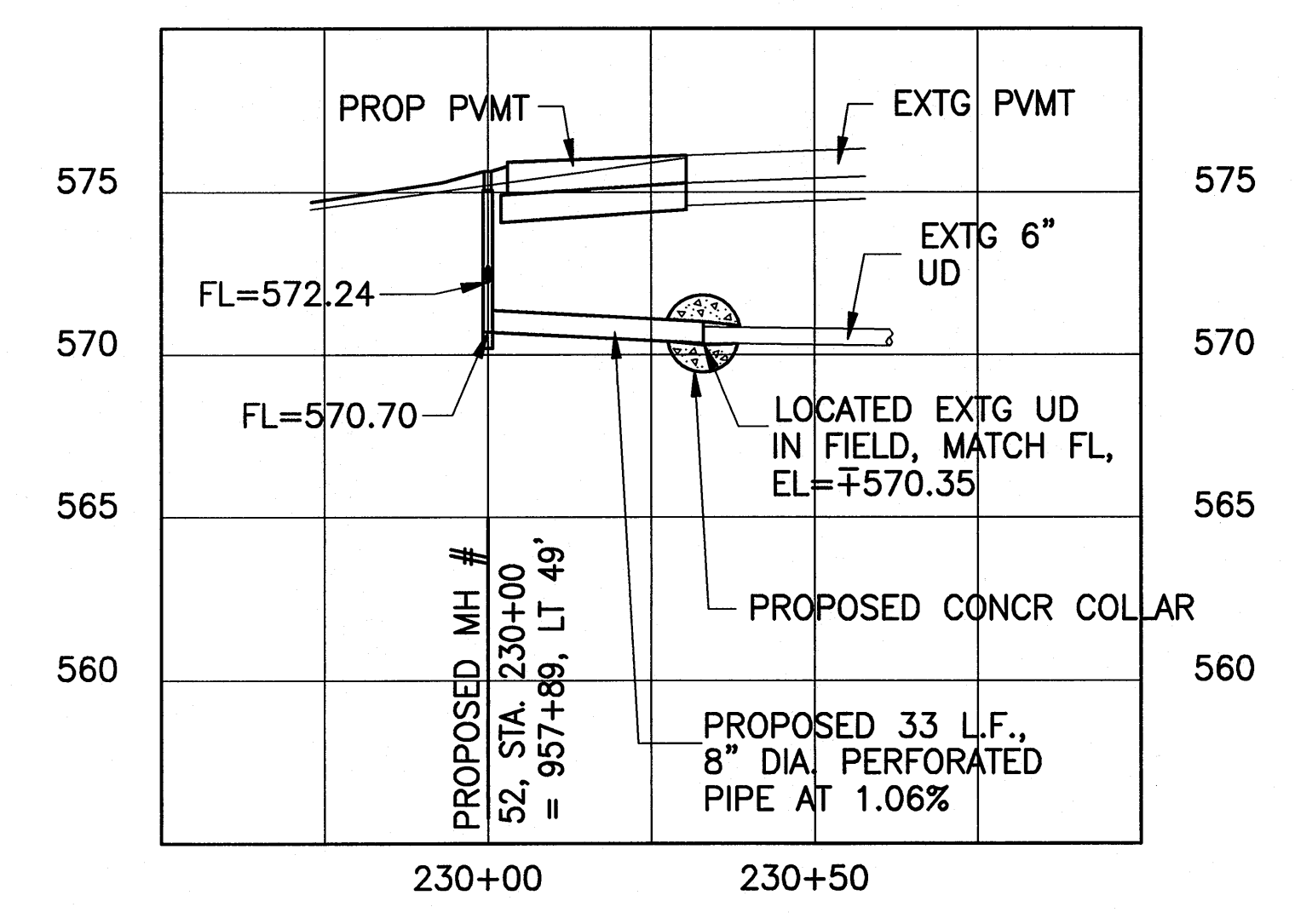
RUNWAY 5-23, STA. 41+80 RIGHT TO TAXIWAY N STA. 960+70 LEFT UNDERDRAIN CENTERLINE PROFILE, CONTINUED



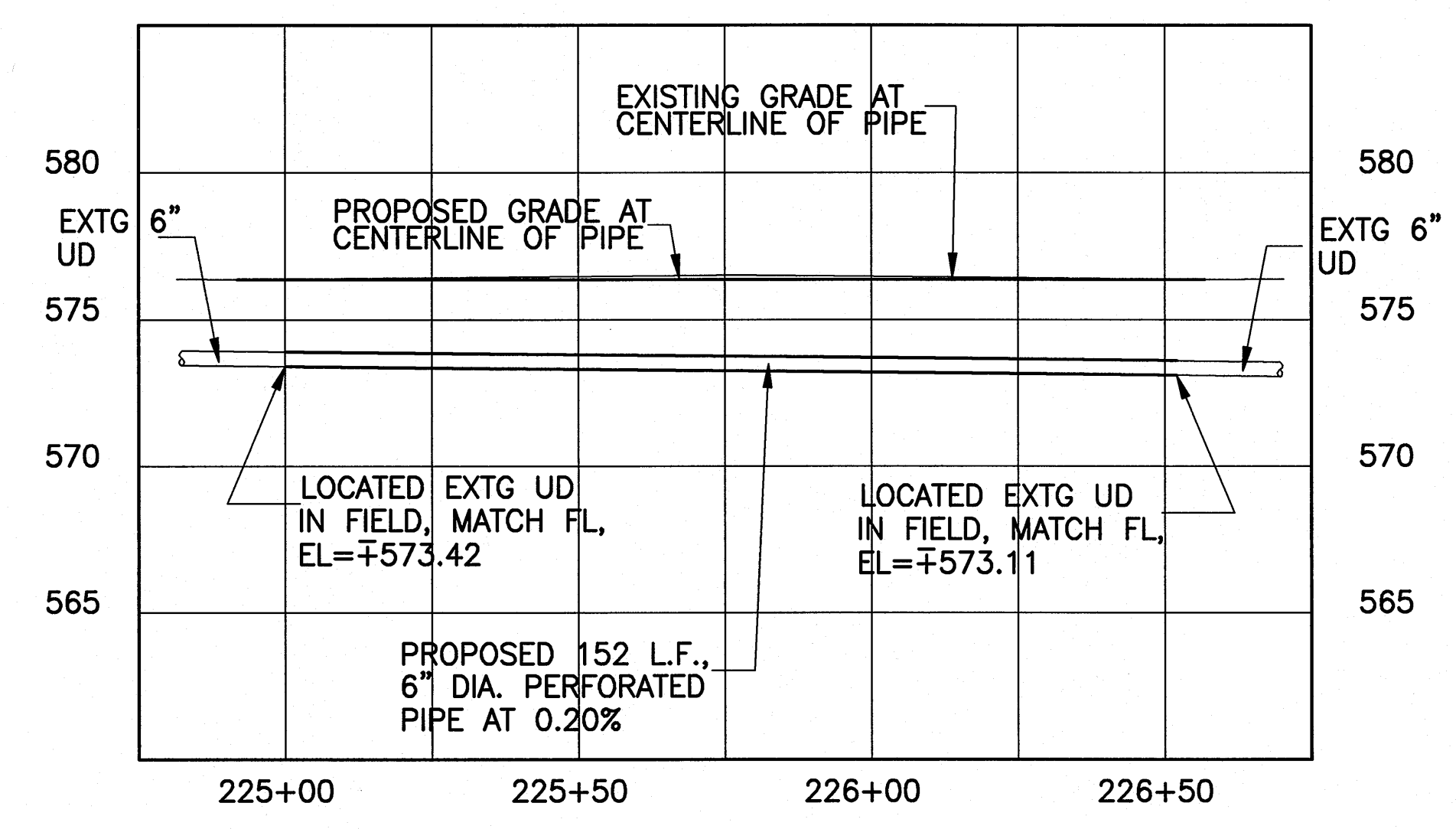
TAXIWAY K, STA. 952+60 TO STA. 955+89.6 RIGHT UNDERDRAIN CENTERLINE PROFILE



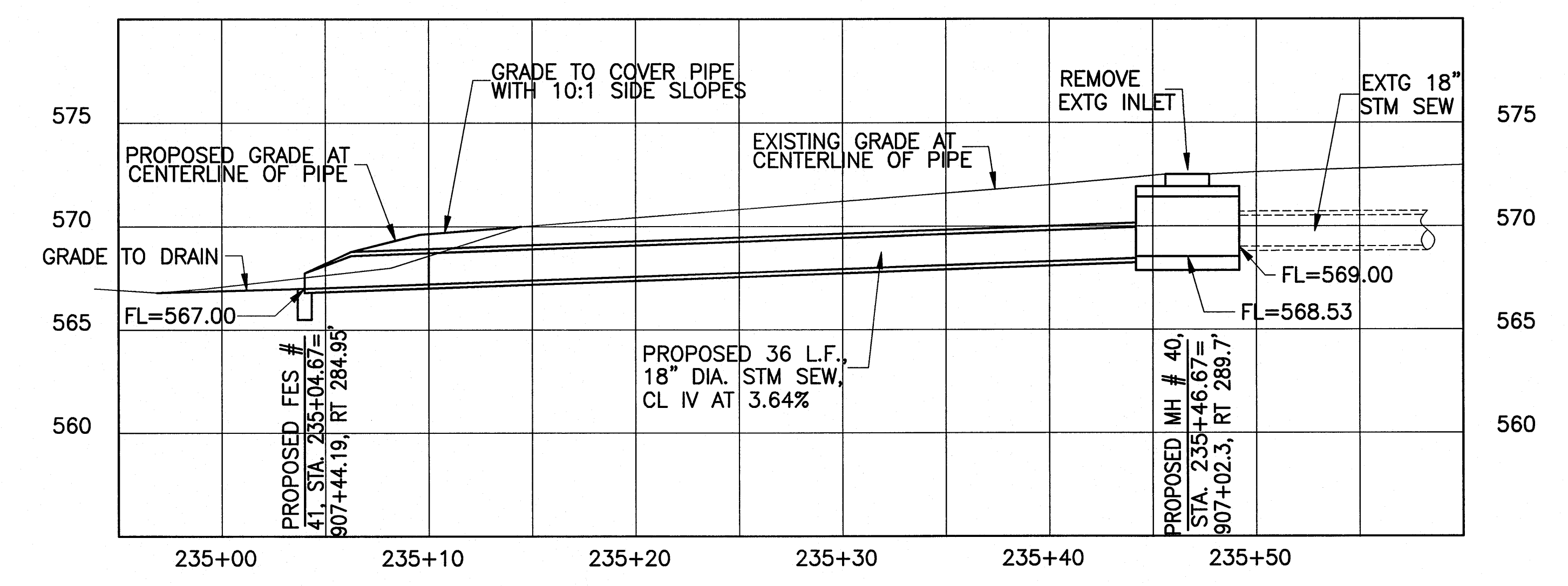
TAXIWAY K, STA. 957+89 TO STA. 958+22 LEFT UNDERDRAIN CENTERLINE PROFILE



TAXIWAY K, STA. 956+82 TO STA. 958+22 RIGHT UNDERDRAIN CENTERLINE PROFILE



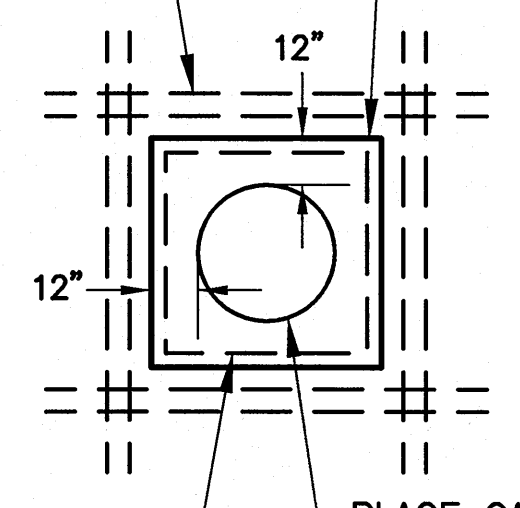
TAXIWAY P, STA. 907+02.3 TO STA. 907+44.2 RIGHT STORM SEWER CENTERLINE PROFILE



G:\AIRPORT\A09T002 TP E PVMT\DRAINAGE1.dwg, 7/8/2009 10:20:27 AM, jefm

2 - #6 REBARS (8 TOTAL) PLACED AT MID-DEPTH OF PAVEMENT 3" FROM EXPANSION JOINT ON 6" C TO C. EXTEND BARS 2'-0" BEYOND EXPANSION CORNERS. DISCONTINUE BARS 3" EITHER SIDE OF INTERSECTING EXPANSION JOINTS.

3/4" THICK PREFORMED EXPANSION JOINT MATERIAL PLACED FULL DEPTH AROUND THE PERIMETER OF THE OPENING

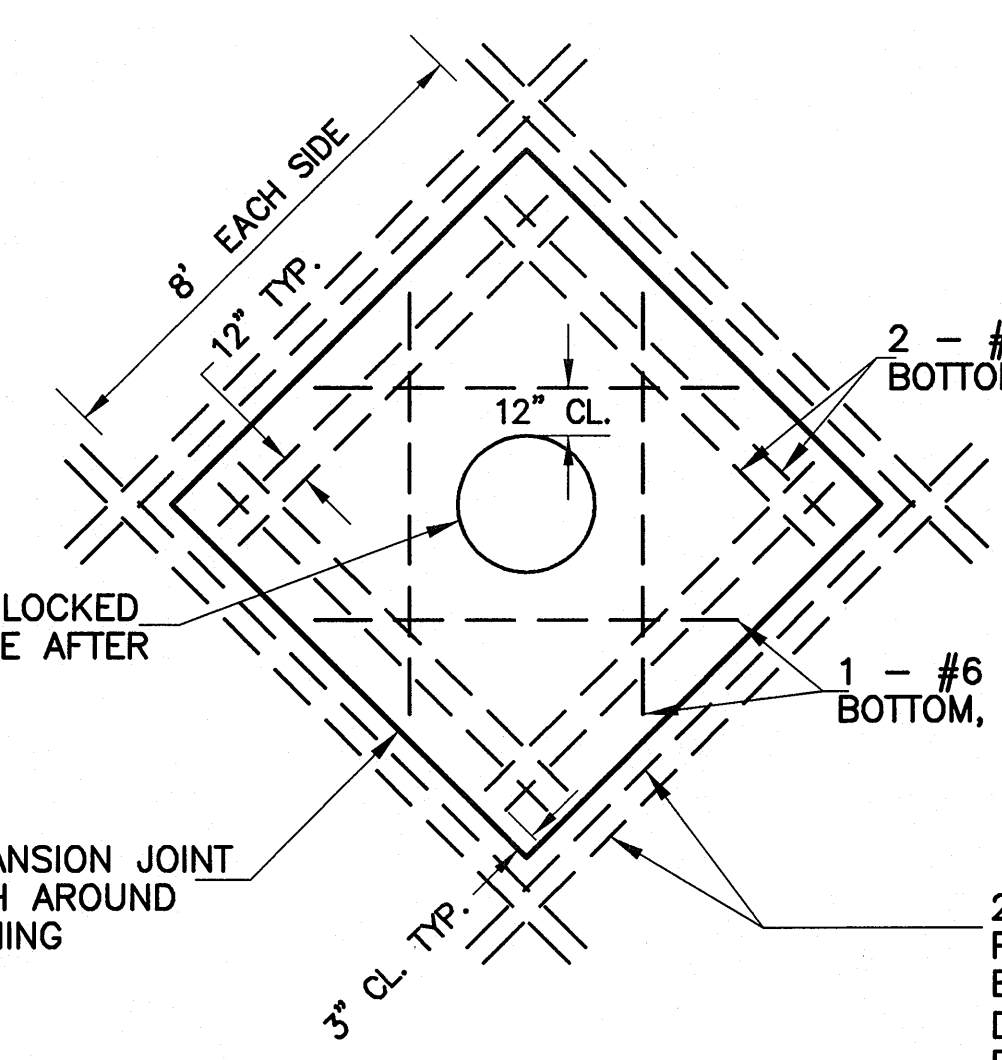


#6 REBARS (4 TOTAL) PLACED AT MID-DEPTH OF PAVEMENT 3" FROM EXPANSION JOINT.

PLACE CASTING TO GRADE AND FILL BLOCKED OUT AREA WITH FULL DEPTH CONCRETE AFTER PAVING INSTALLED

PAVEMENT SQUARE BLOCKOUT
FOR CIRCULAR CASTING
NO SCALE

PLACE CASTING TO GRADE AND FILL BLOCKED OUT AREA WITH FULL DEPTH CONCRETE AFTER PAVING INSTALLED



3/4" THICK PREFORMED EXPANSION JOINT MATERIAL PLACED FULL DEPTH AROUND THE PERIMETER OF THE OPENING

2 - #6 REBAR, TOP & BOTTOM, TYP.

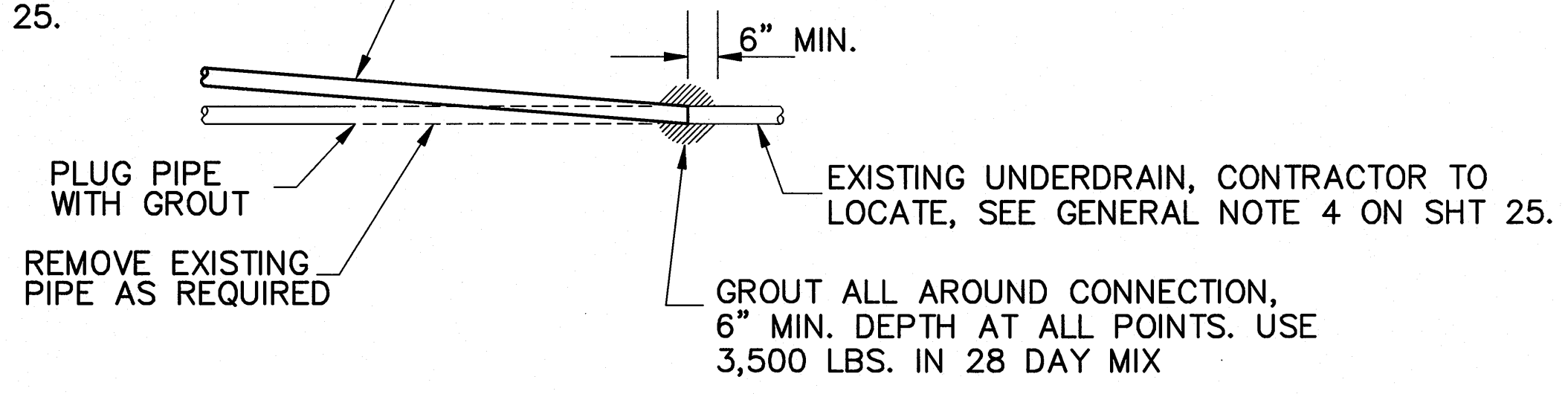
1 - #6 REBAR, TOP & BOTTOM, TYP.

2 - #6 REBARS (8 TOTAL) PLACED AT MID-DEPTH OF PAVEMENT 3" FROM EXPANSION JOINT ON 6" C TO C. EXTEND BARS 2'-0" BEYOND EXPANSION CORNERS. DISCONTINUE BARS 3" EITHER SIDE OF INTERSECTING EXPANSION JOINTS.

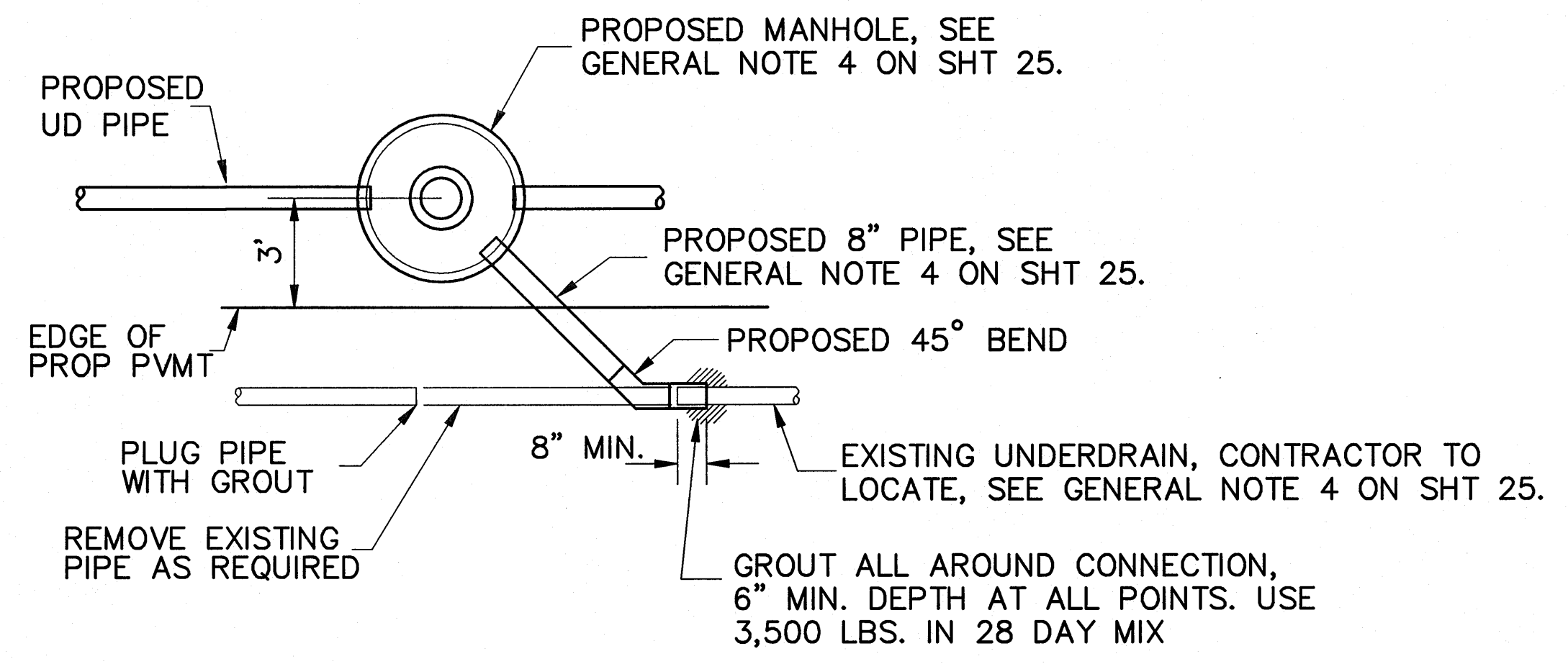
DIAMOND BLOCKOUT
FOR CIRCULAR CASTING
NO SCALE

PROPOSED 6" PERFORATED PIPE, SEE GENERAL NOTES 3 & 4 ON SHT 25.

6" MIN.

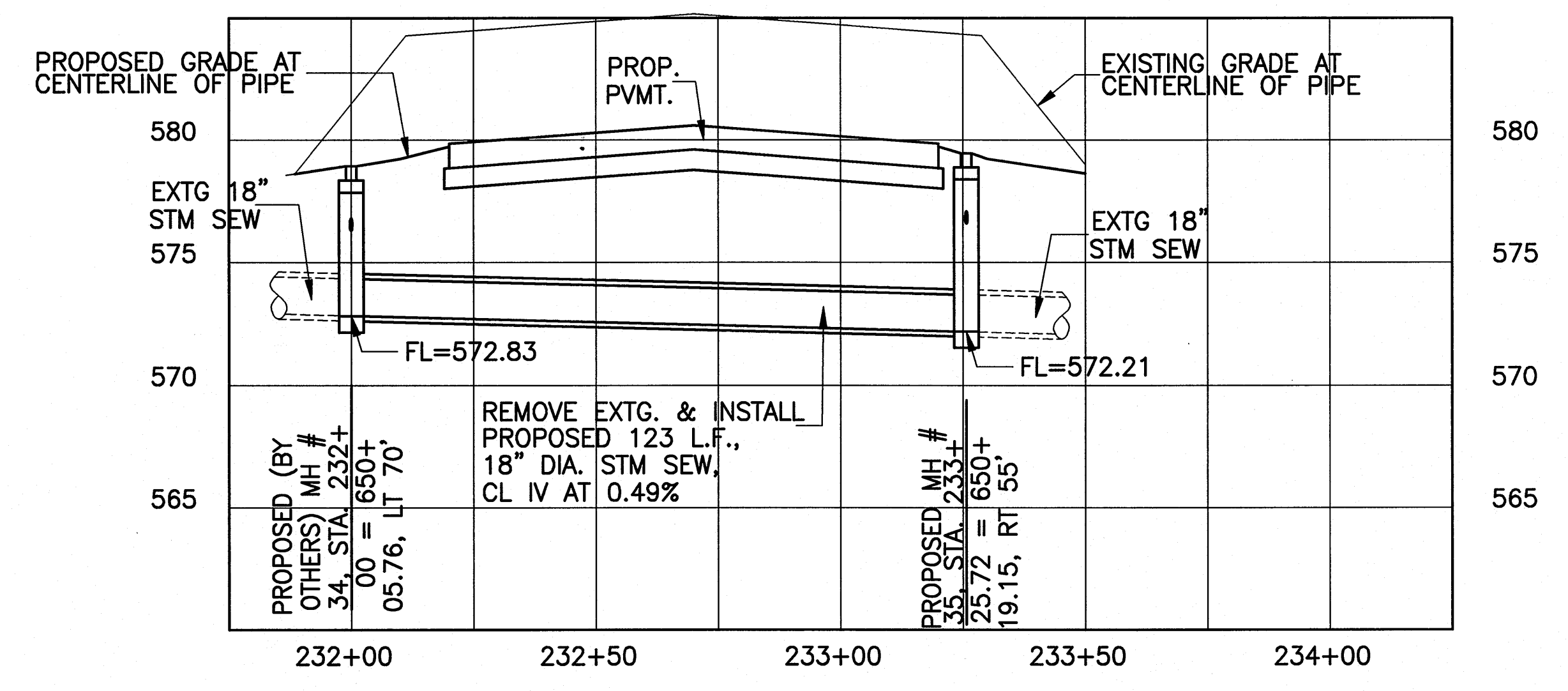


TYPICAL DETAIL - CONCRETE COLLAR
PROPOSED CONNECTION, EXISTING UNDERDRAIN TO PROPOSED UNDERDRAIN, TYPICAL

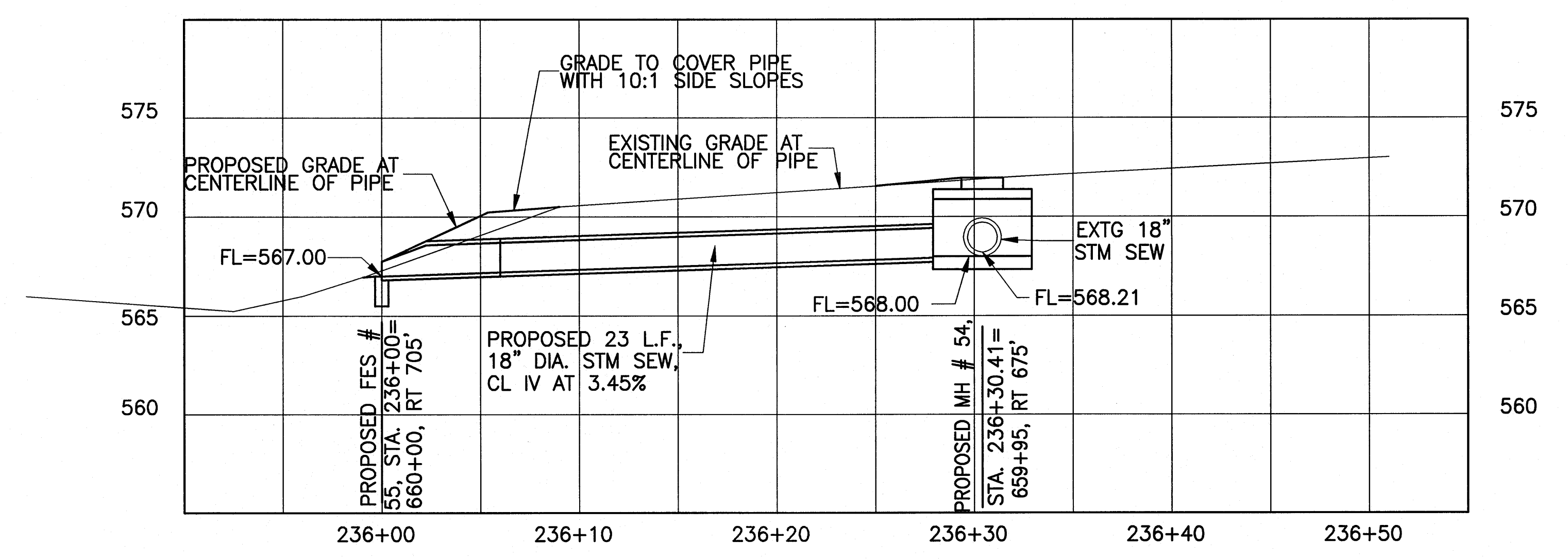


DETAIL A
PROPOSED CONNECTION, EXISTING UNDERDRAIN TO PROPOSED MANHOLE OR UNDERDRAIN, TYPICAL

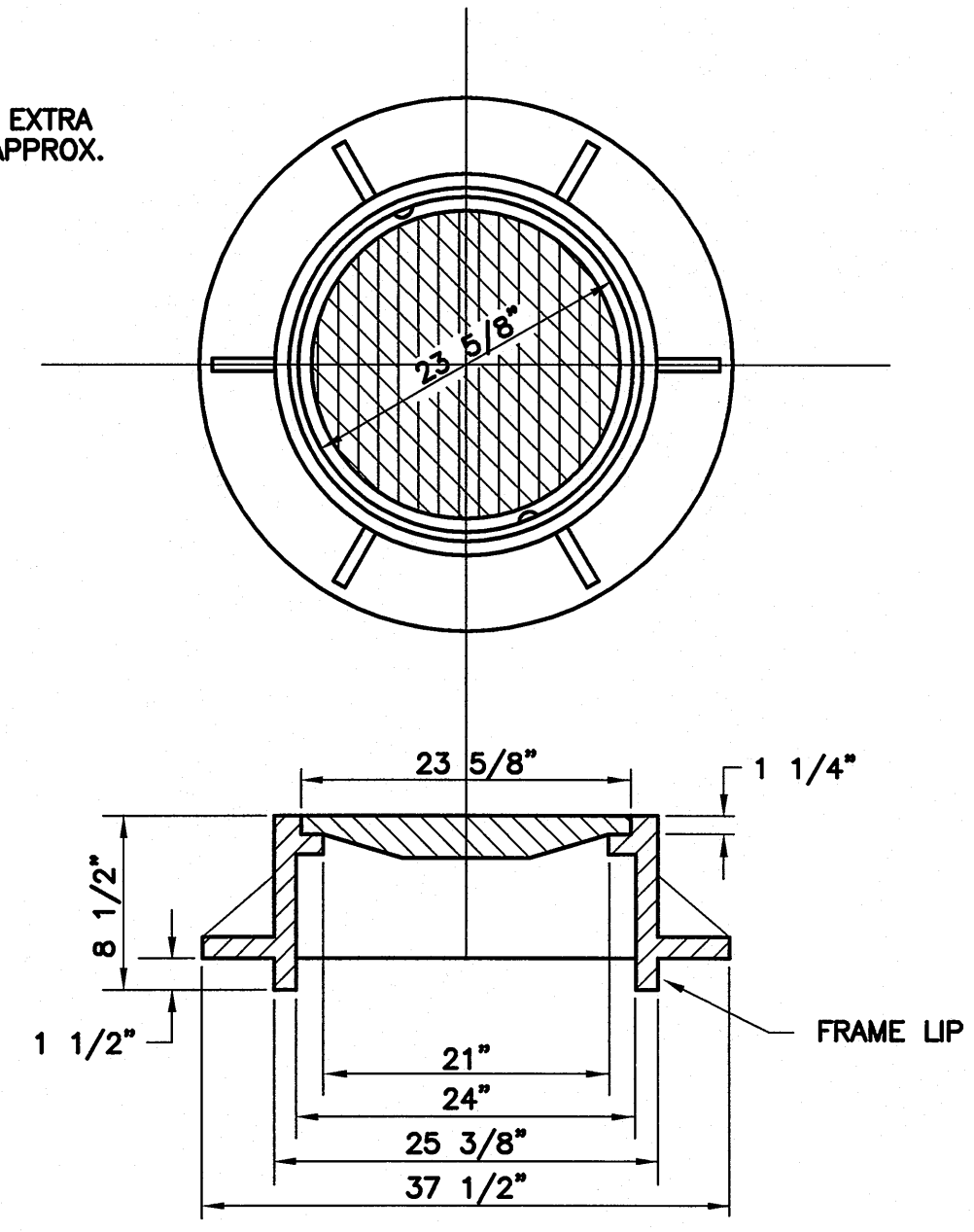
TAXIWAY P, STA. 650+05.76 LEFT TO STA. 650+19.15 RIGHT STORM SEWER CENTERLINE PROFILE



TAXIWAY P, STA. 659+95, RIGHT 675' TO STA. 660+00 RIGHT 705' STORM SEWER CENTERLINE PROFILE

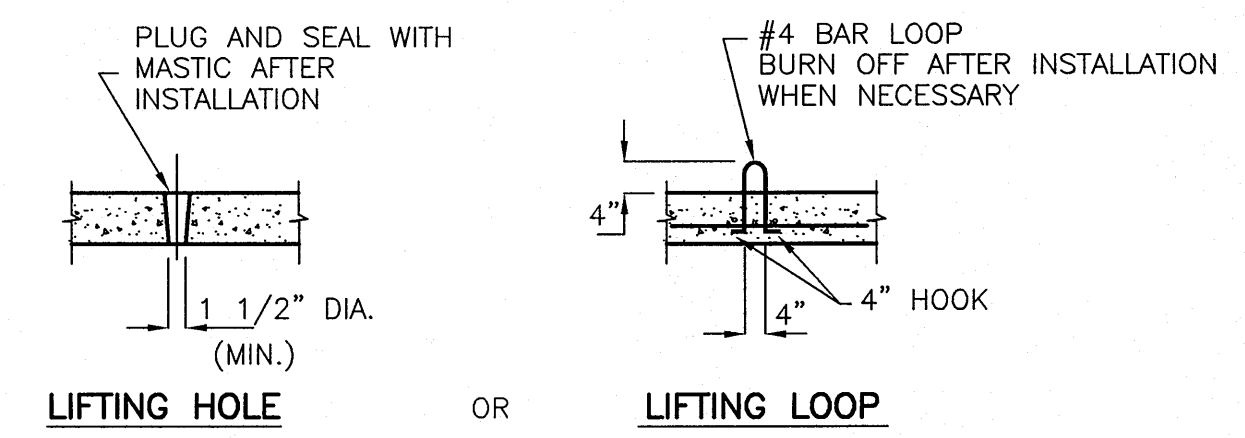
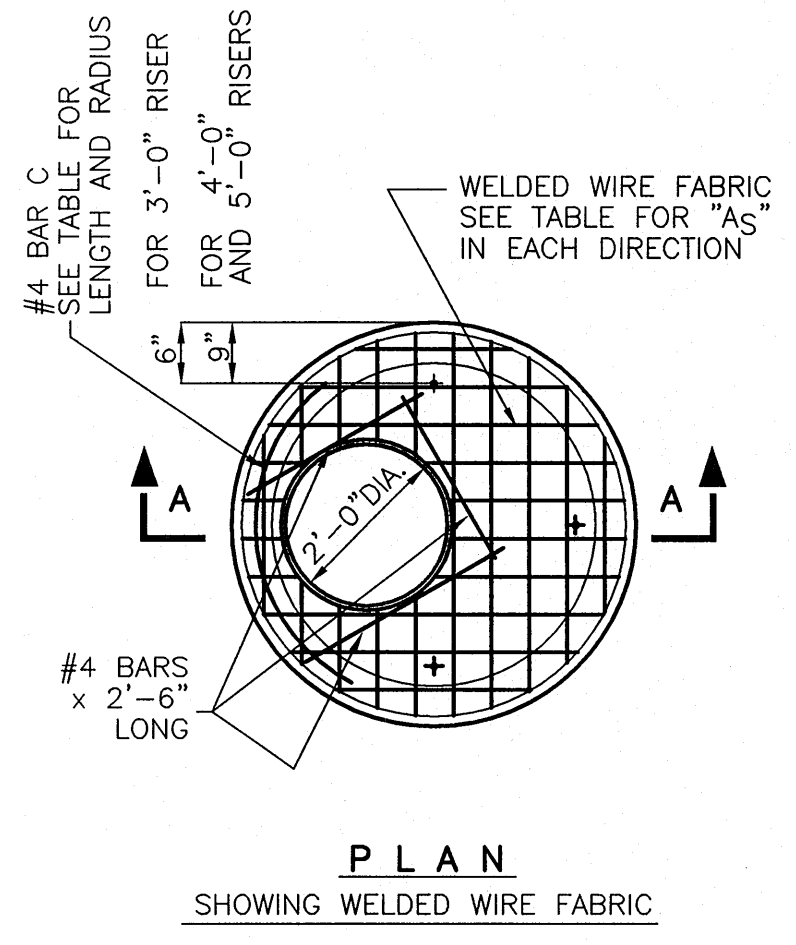


NEENAH R-3493-A OR EQUAL EXTRA
HEAVY DUTY FRAME AND LID. APPROX.
WEIGHT 440 POUNDS.

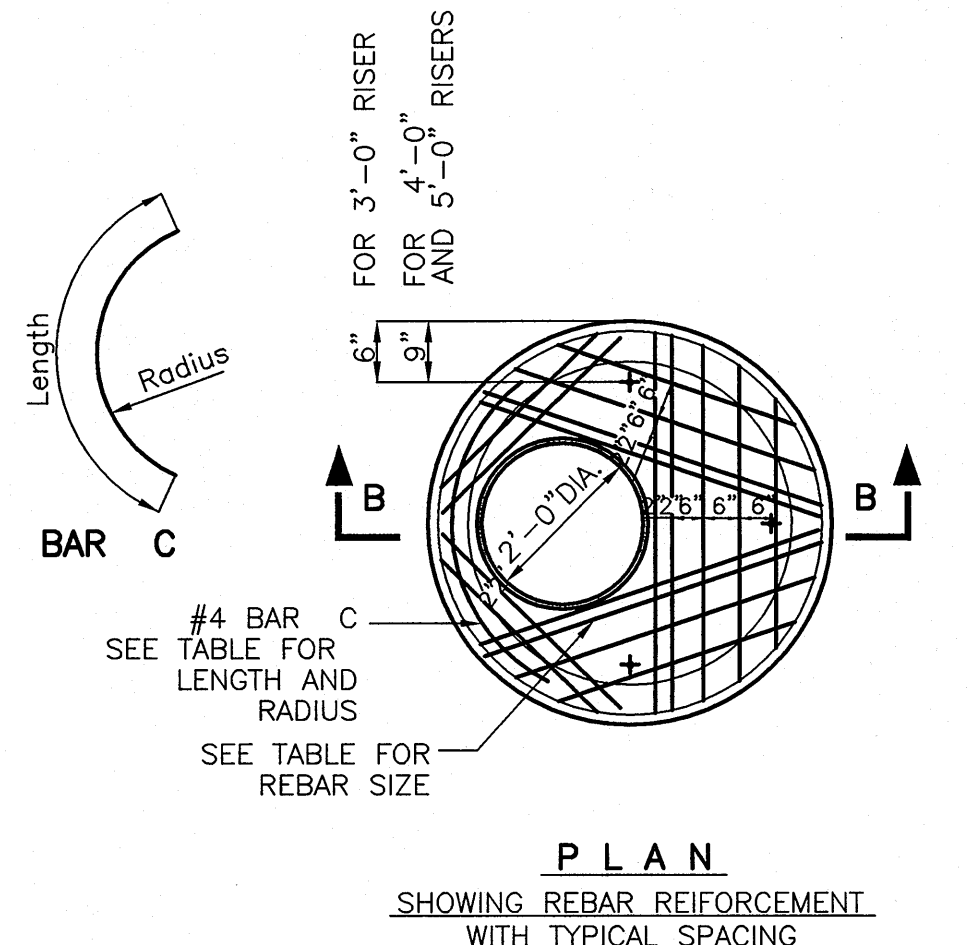


- NOTES:
1. USE FOR ALL MANHOLES (UNLESS NOTED OTHERWISE ON THE PLAN SHEETS).
 2. REMOVE BOLTS THAT HOLD THE GRATE TO THE FRAME AND DO NOT REINSTALL.

MANHOLE FRAME AND LID



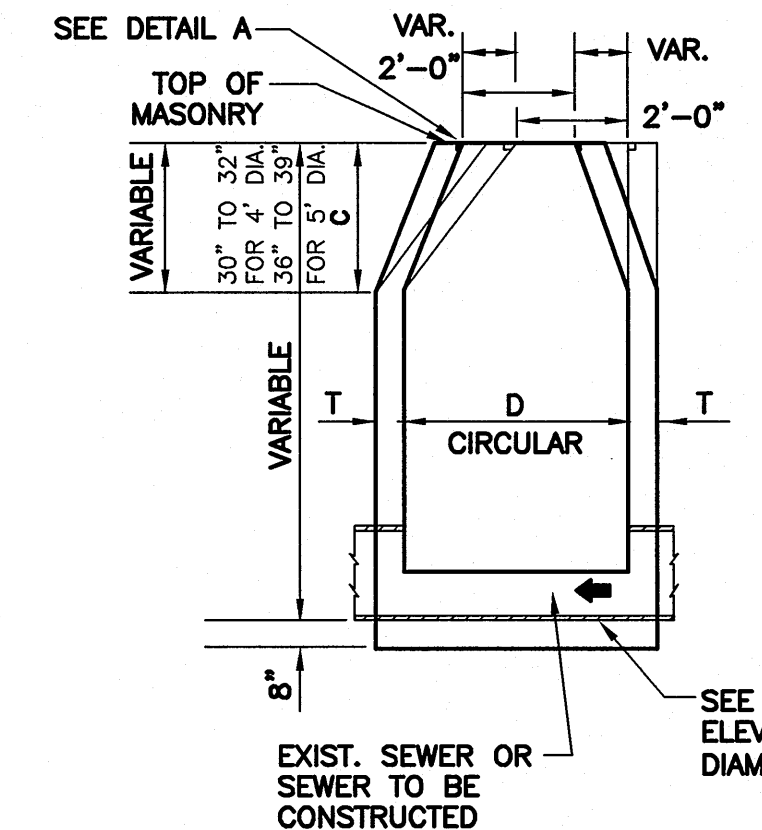
TYPICAL
(3 REQUIRED PER SLAB)



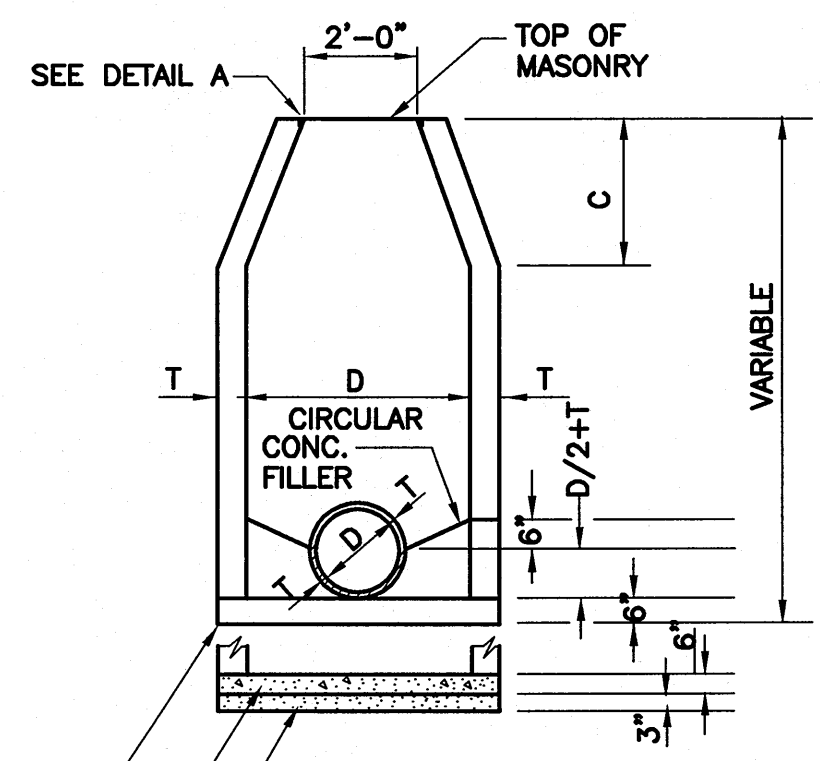
D	T	D ₀ (MIN)	f	REINFORCEMENT	
				*As * W.W.F. OR BAR SIZE EACH DIRECTION	#4 BAR C LENGTH RADIUS
3'-0"	See Standards 1511 and 1566	D + 2T	6"	.20 sq.in./lin.ft.	#4 4'-0" 1'-7"
4'-0"			6"	.35 sq.in./lin.ft.	#5 4'-6" 2'-2"
5'-0"			8"	.35 sq.in./lin.ft.	#5 5'-0" 2'-8"

ALT. MATERIAL FOR WALLS	D	C	T
PRECAST REINFORCED CONCRETE RINGS	4'	2'-6"	5"
	5'	3'-0"	6"
MONOLITHIC CONCRETE	4'	2'-6"	6"
	5'	3'-0"	8"

FOR 6" DIAMETER MANHOLES, SEE IDOT STANDARD 602406. MANHOLE FRAME LIP NOTCH OUT DETAIL AND ADDITIONAL BEDDING REQUIREMENTS THAT ARE SHOWN ON THIS SHEET SHALL BE APPLIED TO IDOT STANDARD 602406.



- NOTES:
- PRECAST REINFORCED CONCRETE RINGS AND MANHOLE FRAMES SHALL BE LAID IN FULL MORTAR BEDS WITH FLUSH JOINTS.
 - MORTAR MIX SHALL CONSIST OF 1 PART PORTLAND CEMENT & 3 PARTS SAND.
 - ALL CONCRETE SHALL HAVE A MIN. STRENGTH AT 28 DAYS OF 3500 P.S.I.
 - THE CONTRACT UNIT PRICE FOR MANHOLE SHALL INCLUDE INSTALLATION OF THE FRAME AND LID.



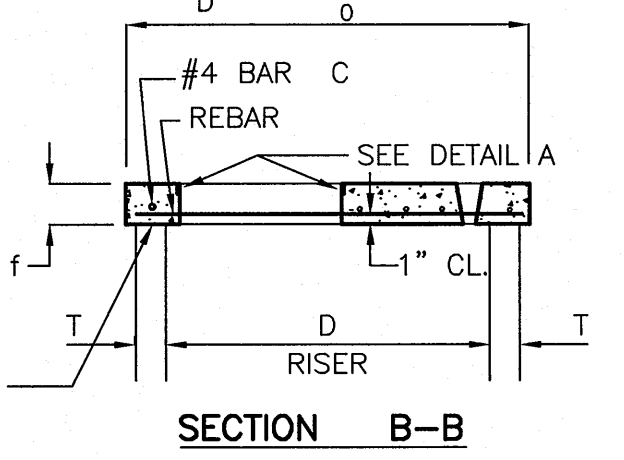
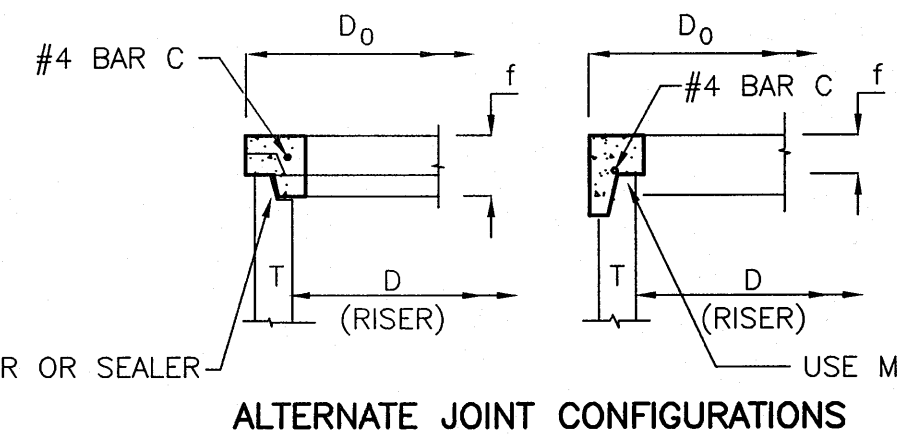
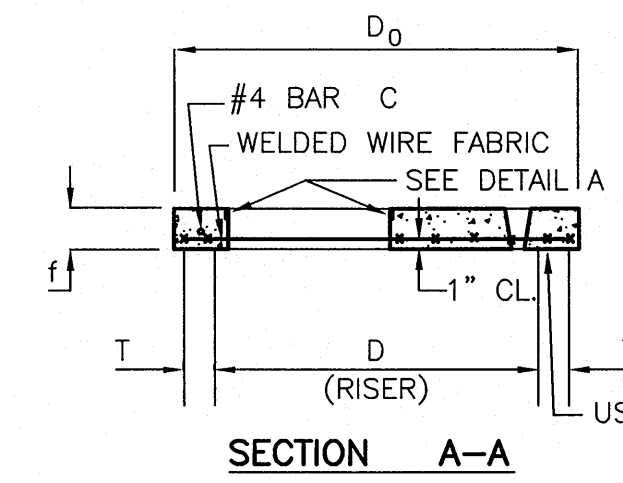
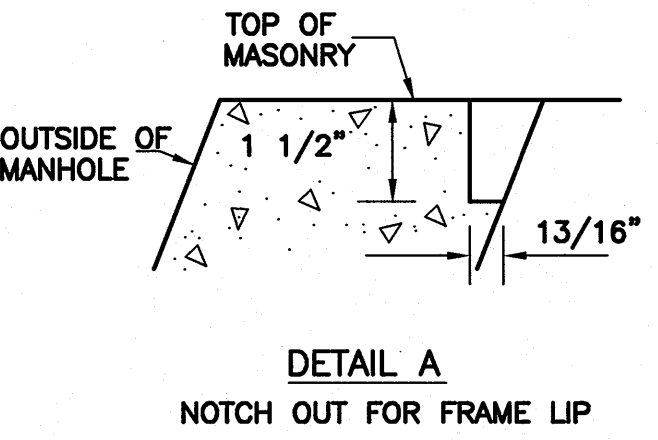
SEE PLAN SHEETS FOR ELEVATION OF INVERT AND DIAMETER OF SEWER

BOTTOM TO BE EITHER PLAIN CONCRETE OR PRECAST REINFORCED CONCRETE SLAB IDOT CA-14 OR -16

FURNISHING AND INSTALLING SAND CUSHION, FRAME, AND LID TO BE INCLUDED IN THE CONTRACT UNIT PRICE

NOTE:
THE CONE OF THE MANHOLE SHALL BE CONSTRUCTED AS SHOWN BY THE DOTTED LINES ONLY WHEN THERE IS INTERFERENCE WITH UNDERGROUND CONDITIONS AND THESE CONDITIONS CAN NOT BE ALTERED.

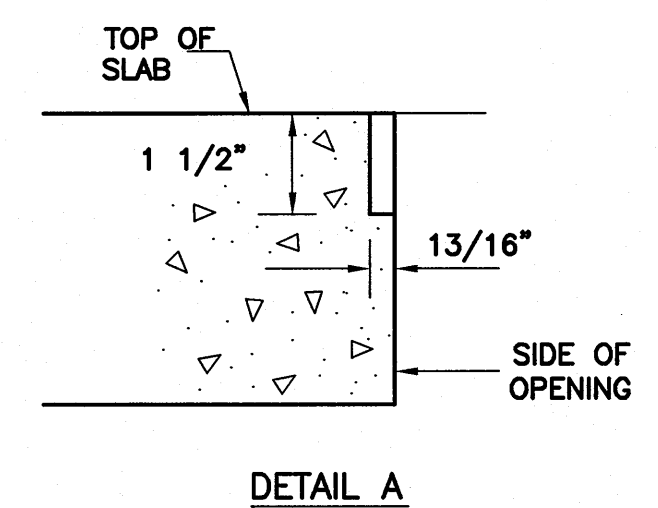
DETAIL OF STORM MANHOLE
ITEMS AR751540, AR751550, & AR751560



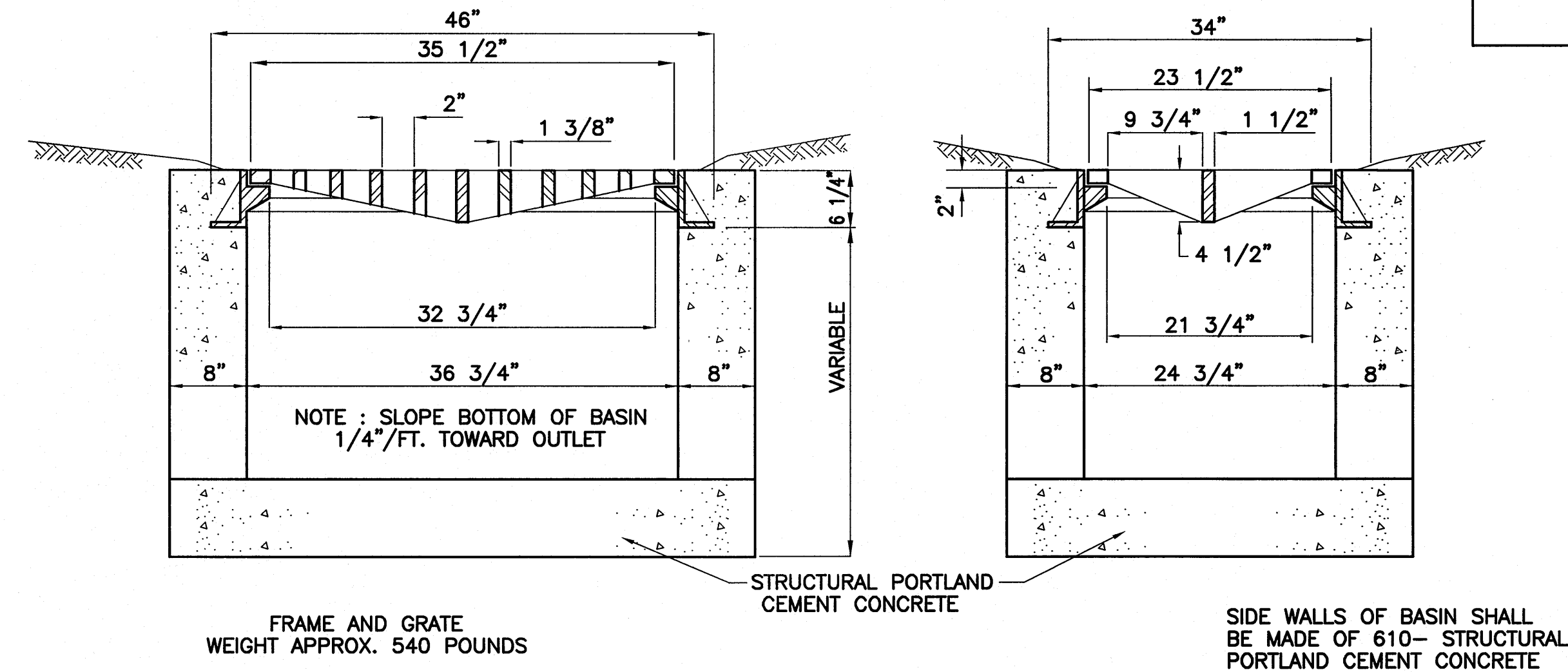
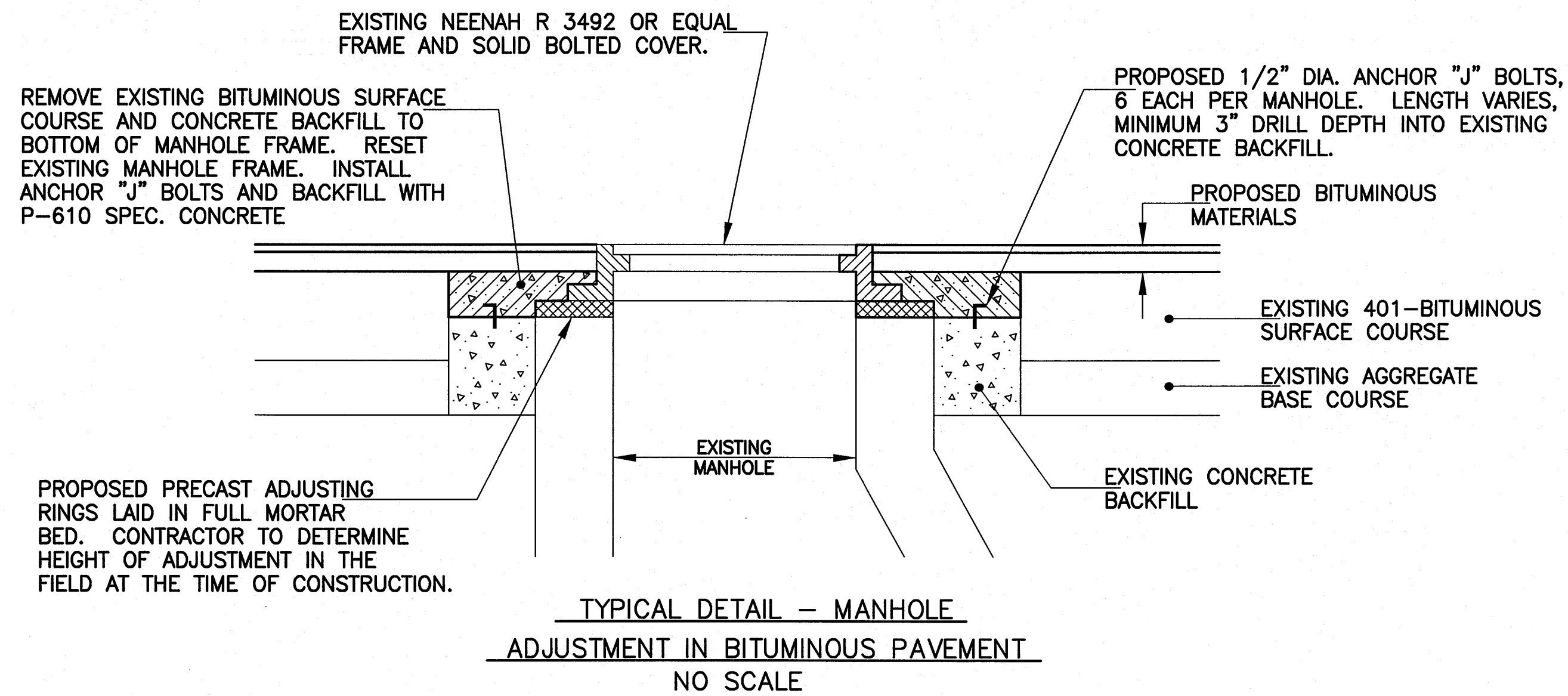
PRECAST REINFORCED CONCRETE
FLAT SLAB TOP
FOR MANHOLES, CATCH BASINS
AND VALVE VAULTS

MODIFIED I.D.O.T.
STANDARD 602601

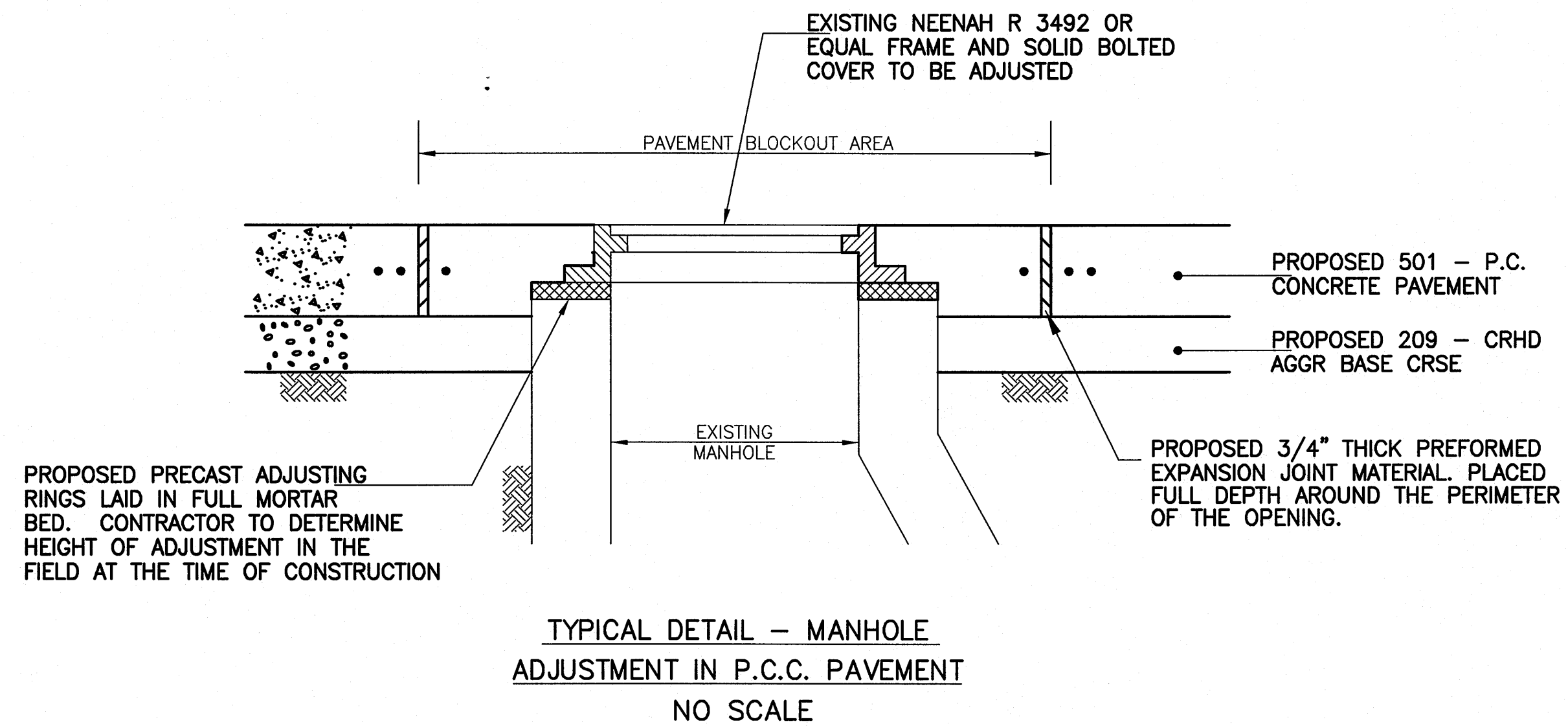
- NOTES:
1. PRECAST FLAT SLAB TOPS SHALL CONFORM TO SECTION 602 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
 2. REINFORCEMENT BARS OR WELDED WIRE FABRIC SHALL BE IN ACCORDANCE WITH ARTICLE 1006.04 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
 3. JOINT CONFIGURATION AND DIMENSIONS SHALL MATCH AND FIT THE RISER JOINT DETAIL.
 4. LIFTING DEVICES OTHER THAN SHOWN MAY BE USED SUBJECT TO APPROVAL BY THE ENGINEER.
 5. THE FLAT SLAB TOP MAY BE USED IN LIEU OF THE TAPERED TOPS SHOWN ON STANDARDS 602001, 602011, 602306, 602401, OR 602501 AT THE OPTION OF THE CONTRACTOR OR WHEN FIELD CONDITIONS PROHIBIT THE USE OF TAPERED TOPS.
 6. THE COST OF FURNISHING AND INSTALLING THE FLAT SLAB TOP SHALL BE INCLUDED IN THE UNIT PRICE FOR CATCH BASINS, MANHOLES, OR VALVE VAULTS.



FOR 6" DIAMETER MANHOLES, SEE IDOT STANDARD 602406. MANHOLE FRAME LIP NOTCH OUT DETAIL AND ADDITIONAL BEDDING REQUIREMENTS THAT ARE SHOWN ON THIS SHEET SHALL BE APPLIED TO IDOT STANDARD 602406.

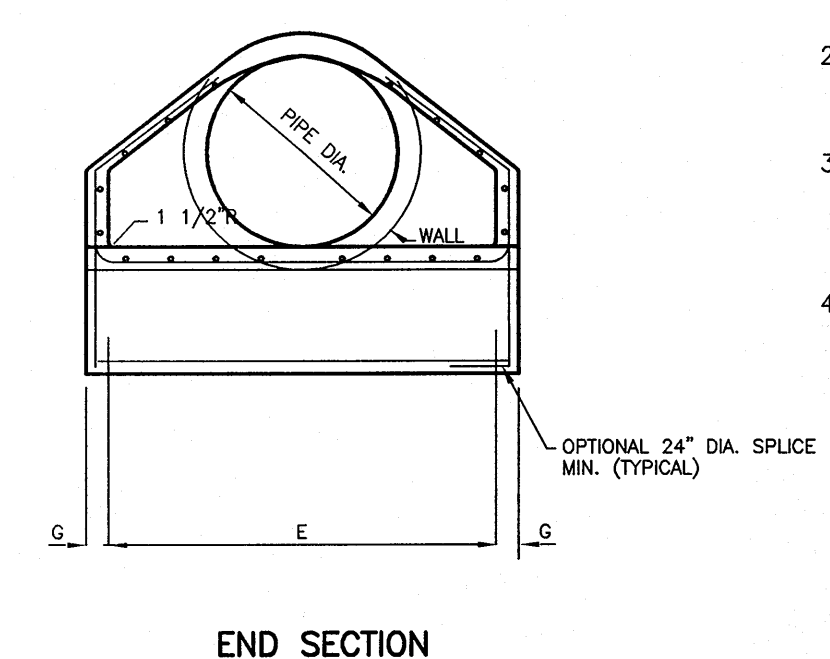
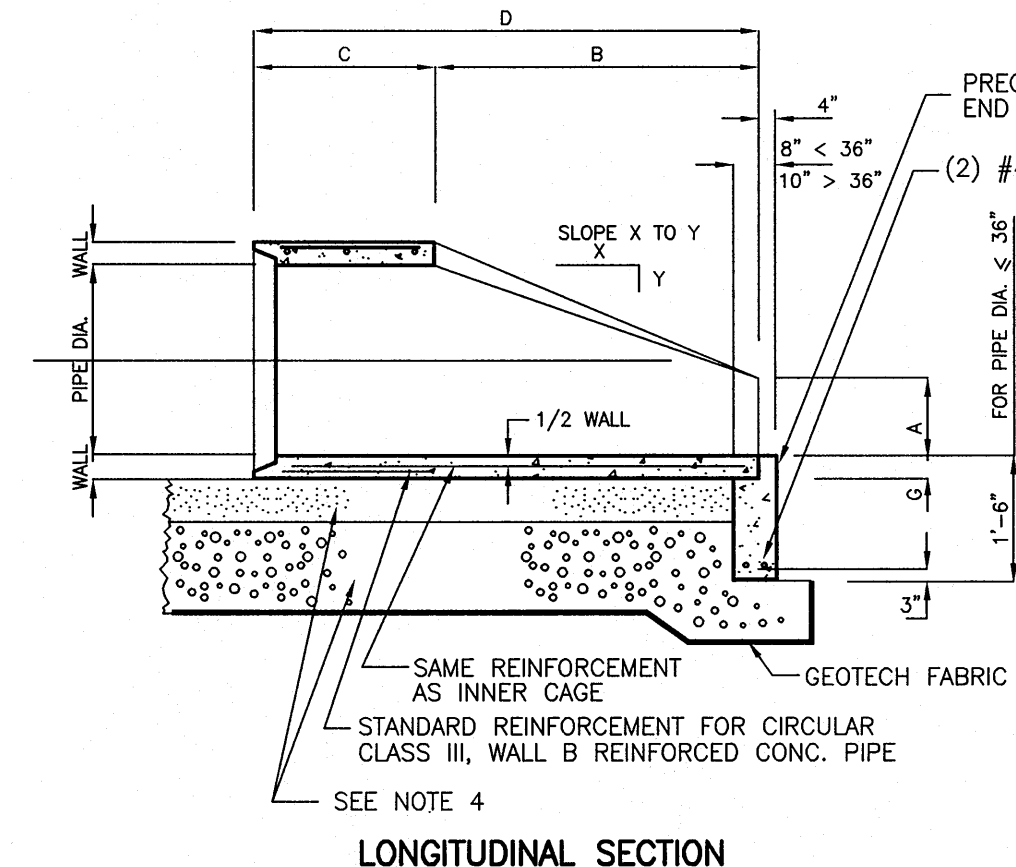
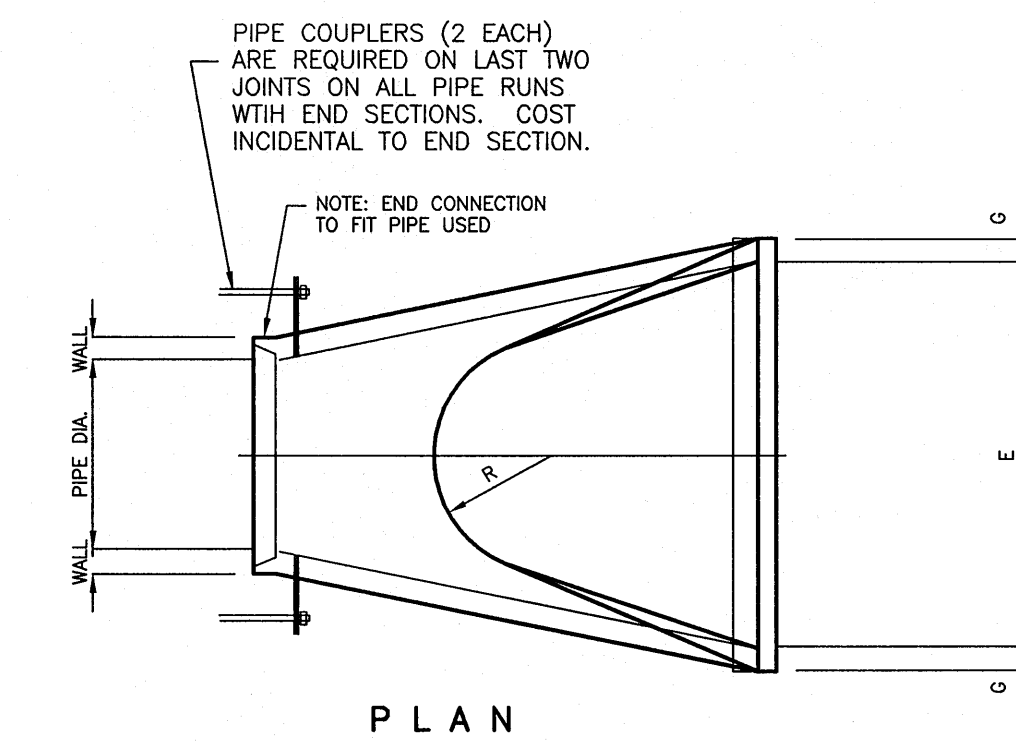


- NOTES :
1. INLET SHALL INCLUDE FRAME WITH SINGLE GRATE OF TYPE SIMILAR AND EQUAL TO NO. R 3475 AS SHOWN BY CATALOG "R" ELEVENTH EDITION OF NEENAH FOUNDRY CO.
 2. REMOVE BOLTS THAT HOLD THE GRATE TO THE FRAME AND DO NOT REINSTALL.

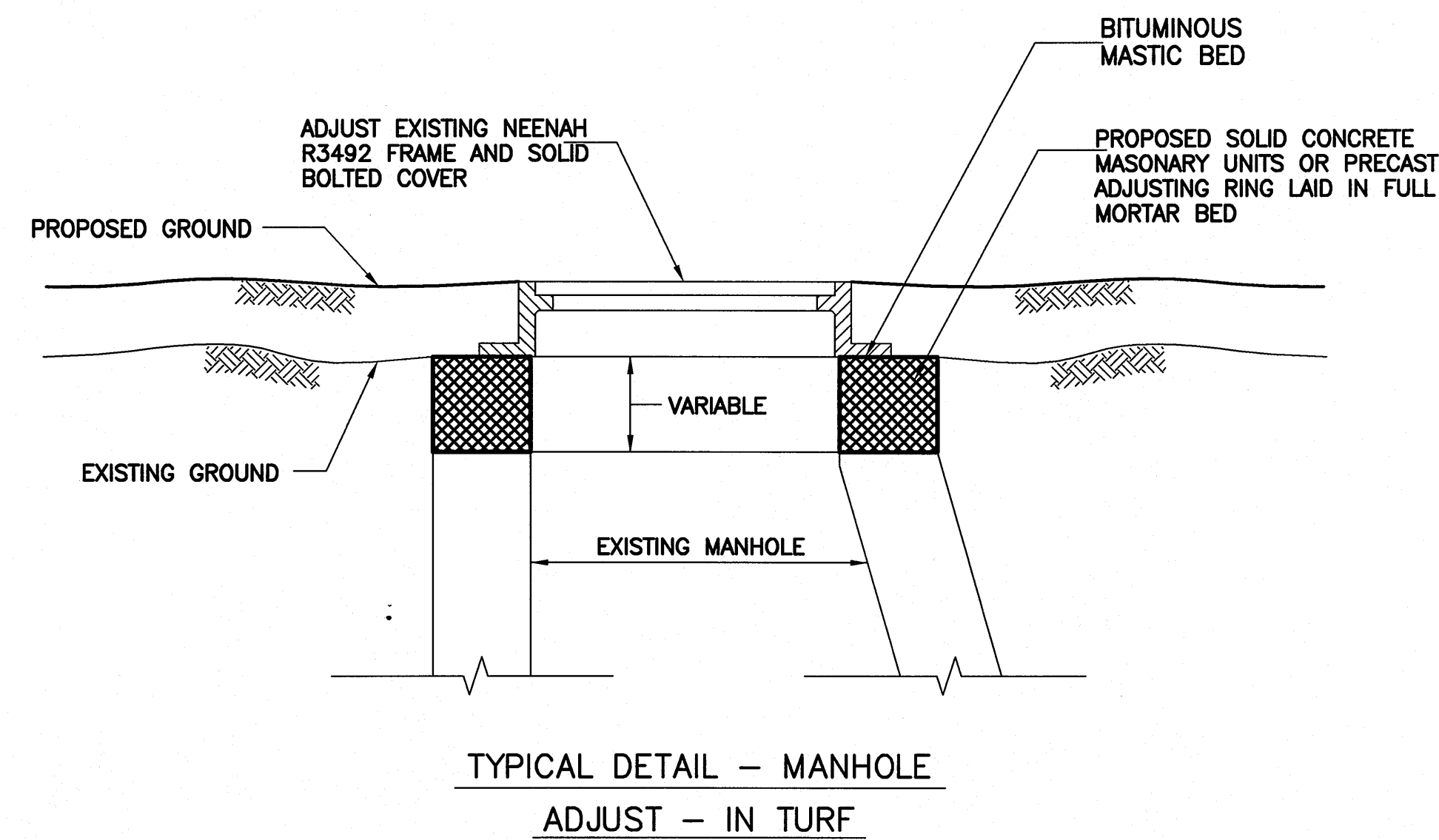


PRECAST REINFORCED CONCRETE FLARED END SECTION

PIPE DIA.	APPRX. WT. (LBS.)	WALL	A	B	C	D	E	G	R	SLOPE
12"	530	2"	4'	2'-0"	4'-0-7/8"	6'-0-7/8"	2'-0"	2'	9"	3:1
15"	740	2-1/4"	6'	2'-3"	3'-10"	6'-1"	2'-6"	2-1/4"	11"	3:1
18"	990	2-1/2"	9'	2'-3"	3'-10"	6'-1"	3'-0"	2-1/2"	12"	3:1
21"	1280	2-3/4"	9'	2'-11"	3'-2"	6'-1"	3'-6"	2-3/4"	13"	3:1
24"	1520	3"	9-1/2"	3'-7-1/2"	2'-6"	6'-1-1/2"	4'-0"	3"	14"	3:1
27"	1930	3-1/4"	10-1/2"	4'-0"	2'-1-1/2"	6'-1-1/2"	4'-6"	3-1/4"	14-1/2"	3:1
30"	2190	3-1/2"	1'-0"	4'-6"	1'-7-3/4"	6'-1-3/4"	5'-0"	3-1/2"	15"	3:1
33"	3200	3-3/4"	1'-1-1/2"	4'-10-1/2"	3'-3-1/4"	8'-1-3/4"	5'-6"	3-3/4"	17-1/2"	3:1
36"	4100	4"	1'-3"	5'-3"	2'-10-3/4"	8'-1-3/4"	6'-0"	4"	20"	3:1
42"	5380	4-1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	4-1/2"	22"	3:1
48"	6550	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	5"	22"	3:1
54"	8240	5-1/2"	2'-3"	5'-5"	2'-11"	8'-4"	7'-6"	5-1/2"	24"	2: 4:1
60"	8730	6"	2'-11"	5'-0"	3'-3"	8'-3"	8'-0"	5"	*	2: 4:1
66"	10710	6-1/2"	2'-6"	6'-0"	2'-3"	8'-3"	8'-6"	5-1/2"	*	2: 4:1
72"	12520	7"	3'-0"	6'-6"	1'-9"	8'-3"	9'-0"	6"	*	1.82: 1
78"	14770	7-1/2"	3'-0"	7'-6"	1'-9"	9'-3"	9'-6"	6-1/2"	*	1.82: 1
84"	18160	8"	3'-0"	7'-6-1/2"	1'-9"	9-3-1/2"	10'-0"	6-1/2"	*	1.5: 1



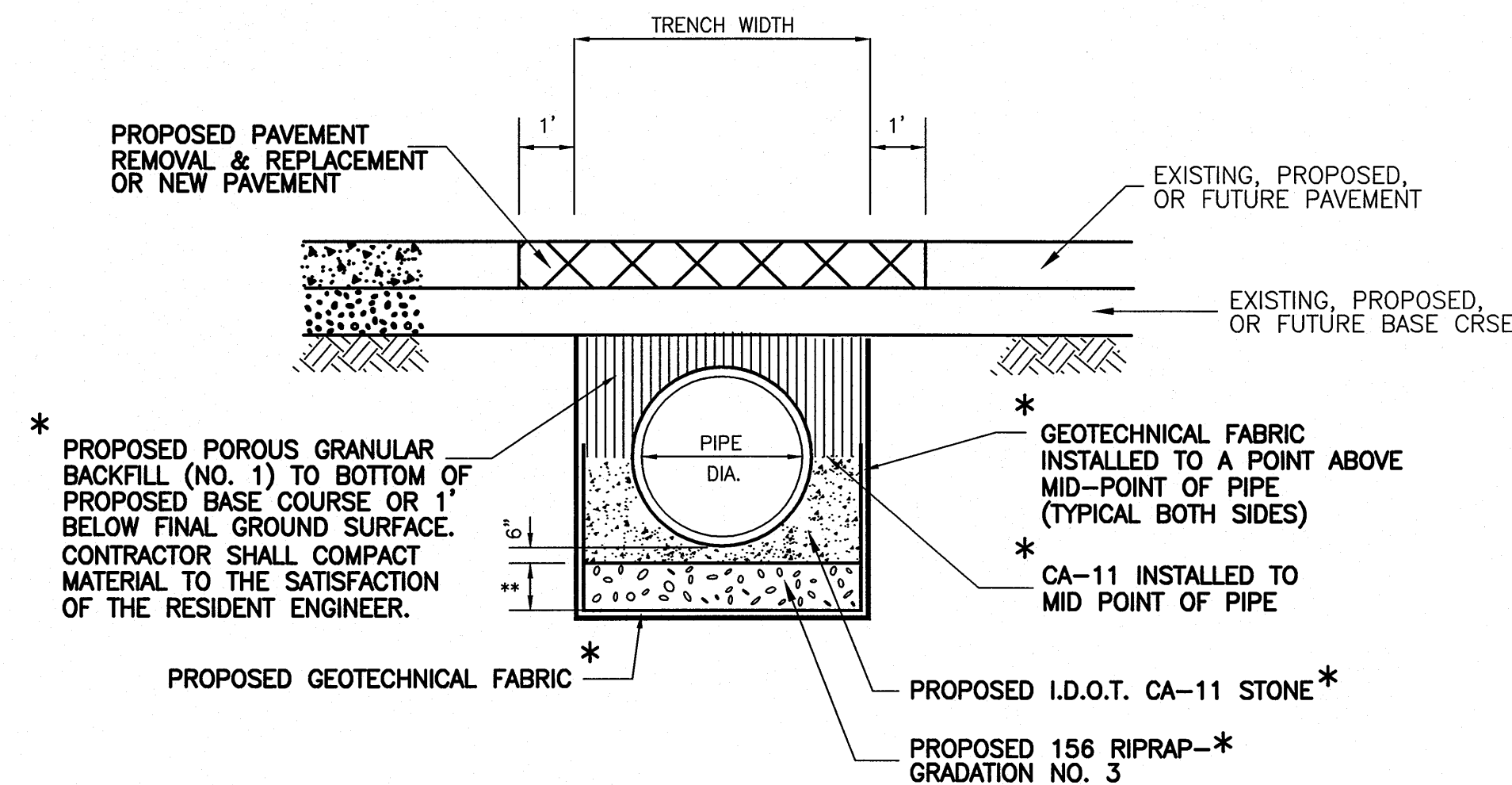
- NOTES:
1. PRECAST CONCRETE FLARED END SECTIONS SHALL CONFORM TO THE APPLICABLE REQUIREMENTS OF AASHTO M-170 CLASS III, WALL B REINFORCED CONCRETE PIPE.
 2. PRECAST CONCRETE FLARED END SECTION FOR PIPE DIAMETER REQUIRED SHALL BE AS INDICATED ON DETAIL PLAN FOR EACH INDIVIDUAL INSTALLATION.
 3. THE END BLOCK SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE FLARED END SECTION. THE END BLOCK SHALL BE BACKFILLED IN ACCORDANCE WITH ART. 701-3.7 OF THE STANDARD SPECIFICATIONS, COST INCIDENTAL TO END SECTION.
 4. BEDDING MATERIALS AND GEOTECH FABRIC PER 701 PIPE SPECIFICATION IN THE SPECIAL PROVISIONS AS SHOWN IN THIS DETAIL SHALL BE INCLUDED IN THE 752 PIPE END SECTION CONTRACT UNIT PRICE.



FOR EQUIV ELLIPTICAL PRC FES, SEE IDOT STANDARD 542306.
ADDITIONAL BEDDING REQUIREMENTS THAT ARE SHOWN ON THIS SHEET SHALL BE APPLIED TO IDOT STANDARD 542306.

MODIFIED I.D.O.T.
STANDARD 542301

PROPOSED TYPICAL SECTION - PIPE TRENCH
(ALL PROPOSED PIPES)



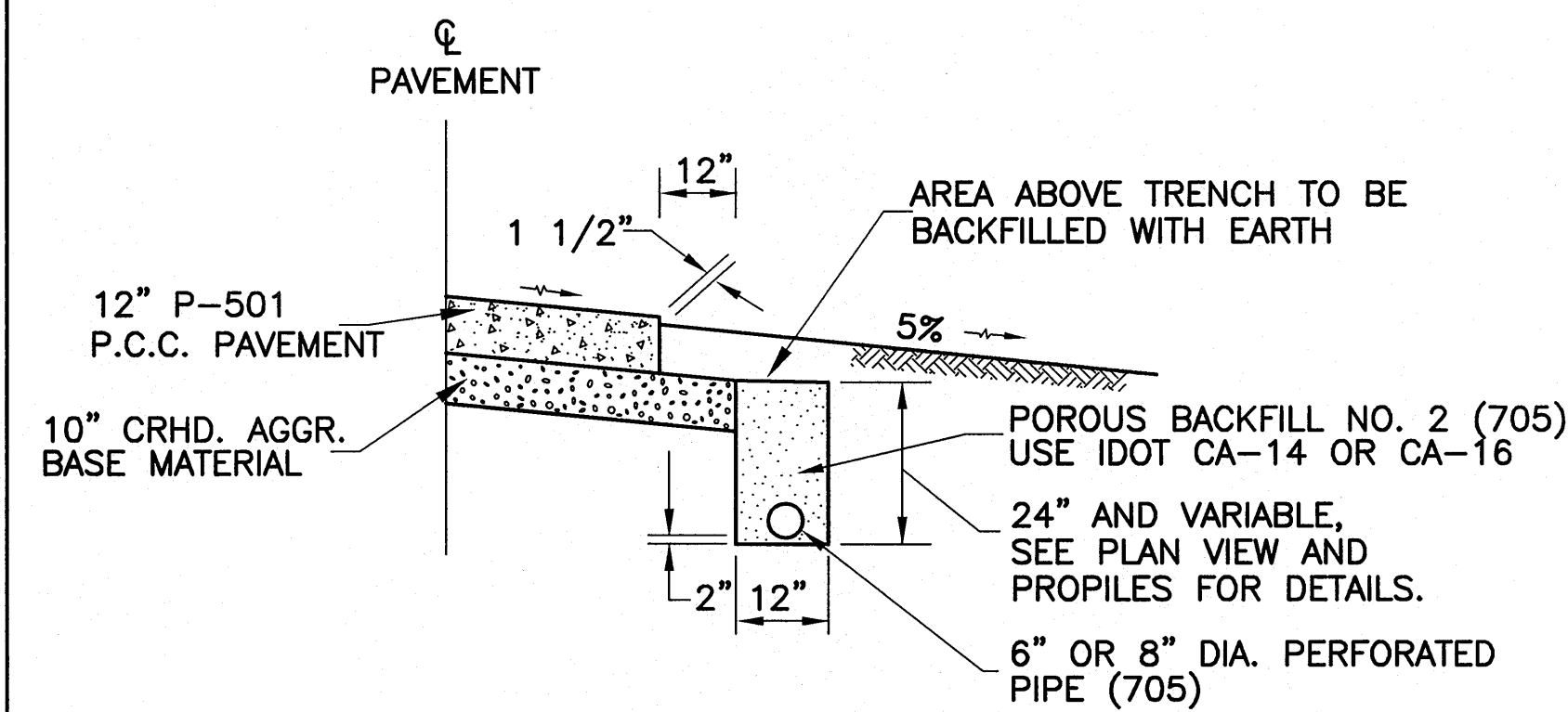
* PROPOSED POROUS GRANULAR BACKFILL (NO. 1) TO BOTTOM OF PROPOSED BASE COURSE OR 1' BELOW FINAL GROUND SURFACE. CONTRACTOR SHALL COMPACT MATERIAL TO THE SATISFACTION OF THE RESIDENT ENGINEER.

** = COST OF THESE ITEMS SHALL BE INCLUDED IN THE 701 PIPE CONTRACT UNIT PRICES

GENERAL PIPE NOTES:

- GROUND WATER IS EXPECTED. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DEWATERING, TO THE SATISFACTION OF THE ENGINEER, TO INSURE PROPER INSTALLATION OF PIPES. NO ADDITIONAL COMPENSATION BEYOND THE CONTRACT UNIT PRICES WILL BE ALLOWED FOR DEWATERING COSTS.
- COMPACTED BEDDING AND BACKFILL MATERIALS SHALL BE REQUIRED AS SHOWN IN DETAIL. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

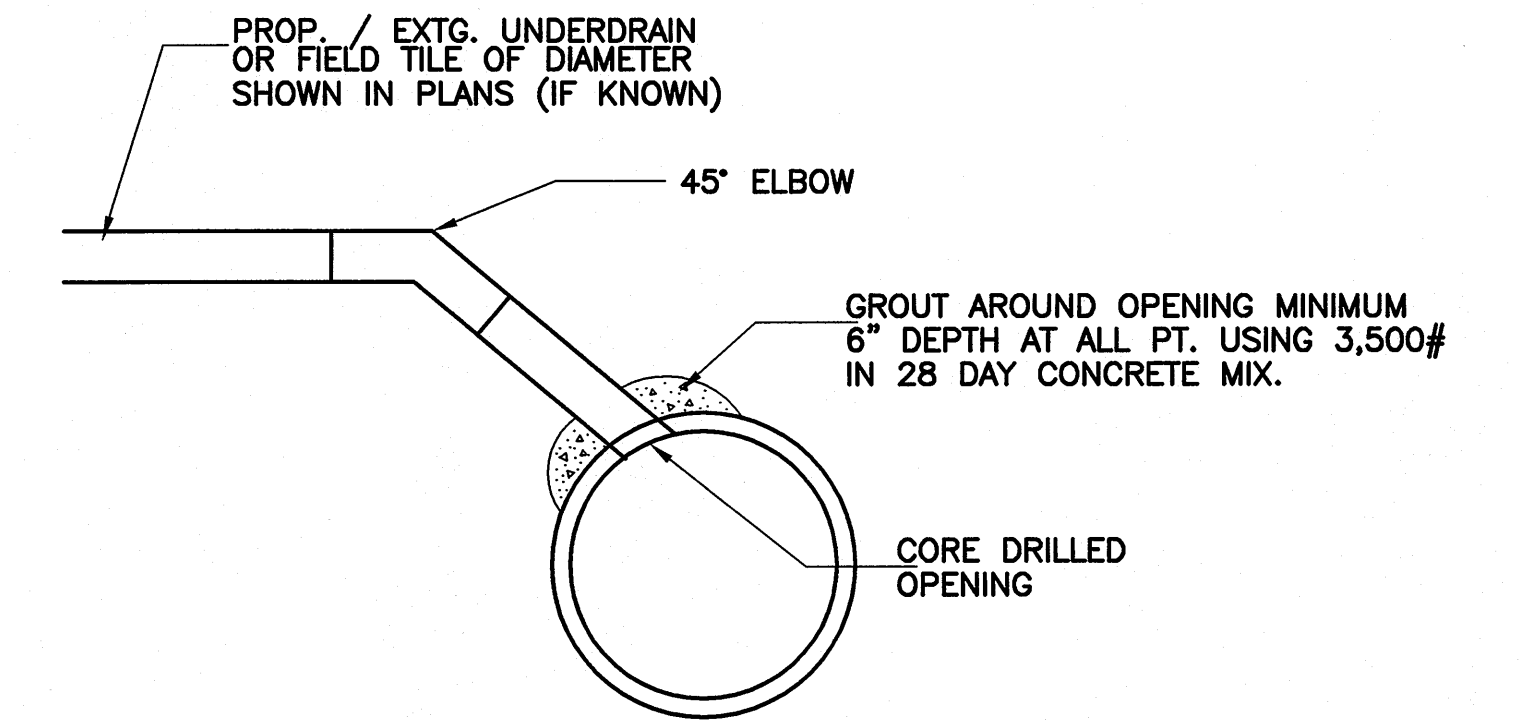
PIPE DIA.	TRENCH WIDTH	PVMT. RMVL. WIDTH	** FOUNDATION DEPTH
8"	3.75'	6'	10"
12"	4.17'	6.5'	10"
18"	4.75'	7'	10"
ELLIPTICAL, 18" EQUIV.	5.21'	7.5'	10"
24"	5.33'	7.5'	18"
30"	5.92'	8'	18"
42"	7.08'	9.5'	18"



NOTES :

- PIPE UNDERDRAIN MATERIAL SHALL MEET THE MATERIAL REQUIREMENTS OF ITEM 705 AND THE SPECIAL PROVISIONS.
- PIPE UNDERDRAIN TO BE INSTALLED BEFORE PLACEMENT OF PAVEMENT & CRUSHED AGGR.
- COST OF POROUS BACKFILL NO. 2 (CA-14 OR CA-16), BENDS AND FITTINGS TO BE INCLUDED IN THE UNIT PRICE FOR UNDERDRAINS.
- NO ADDITIONAL COMPENSATION SHALL BE MADE FOR CHANGES IN ELEVATIONS MADE BY THE RESIDENT ENGINEER.
- PIPE UNDERDRAIN TO BE INSTALLED ON BOTH SIDES OF PAVEMENT.

PERFORATED PIPE UNDERDRAIN DETAIL

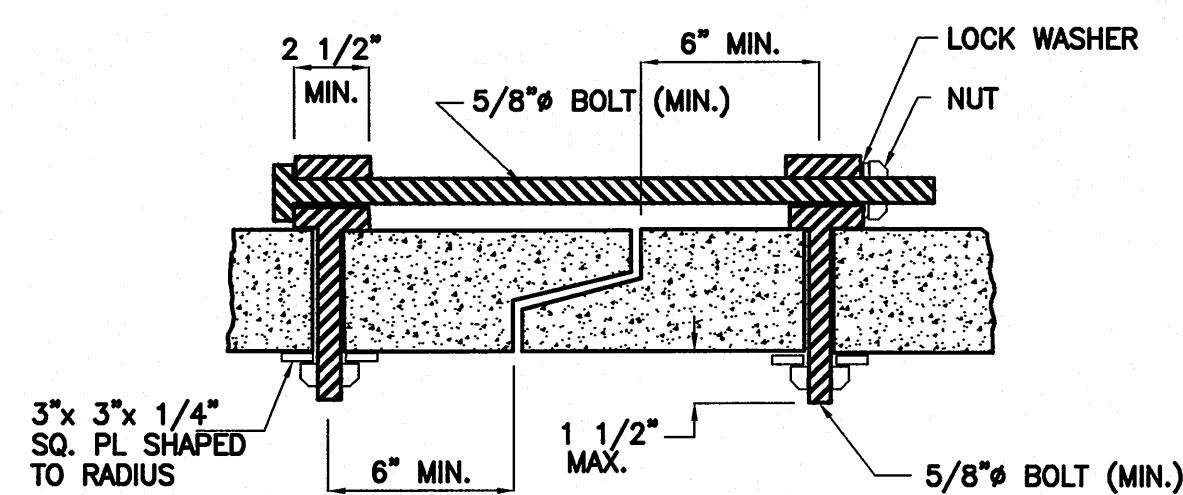
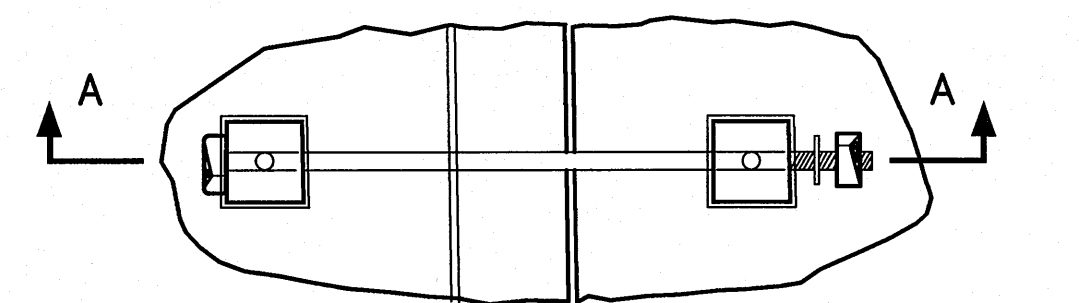


DETAIL OF PIPE UNDERDRAIN / FIELD TILE OUTLET INTO STORM SEWER OR PIPE CULVERT

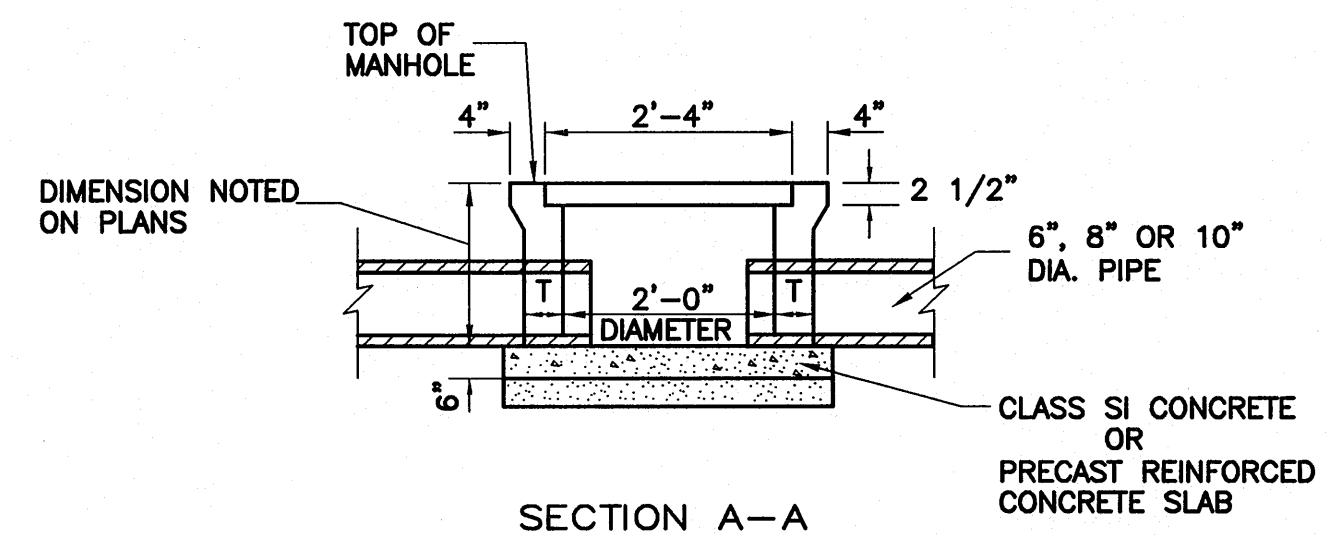
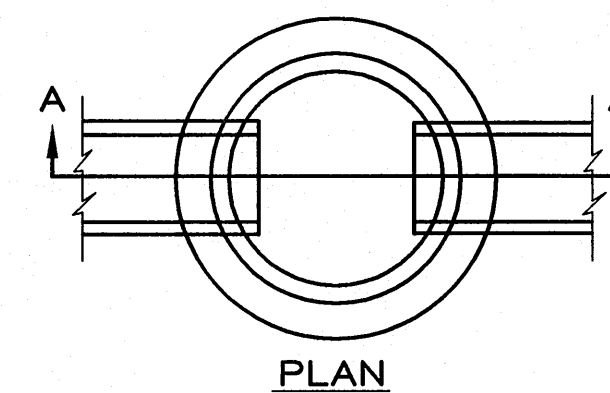
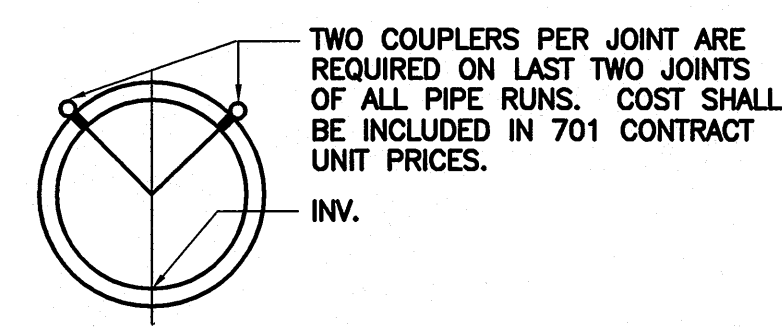
NOTES :

COST OF NEW PIPE, ELBOW AND GROUTING TO BE INCLUDED IN CONTRACT UNIT PRICES. ANY DAMAGE TO EXISTING STORM SEWER, TILE, OR PIPE CULVERT SHALL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT ENGINEER AT THE CONTRACTOR'S EXPENSE.

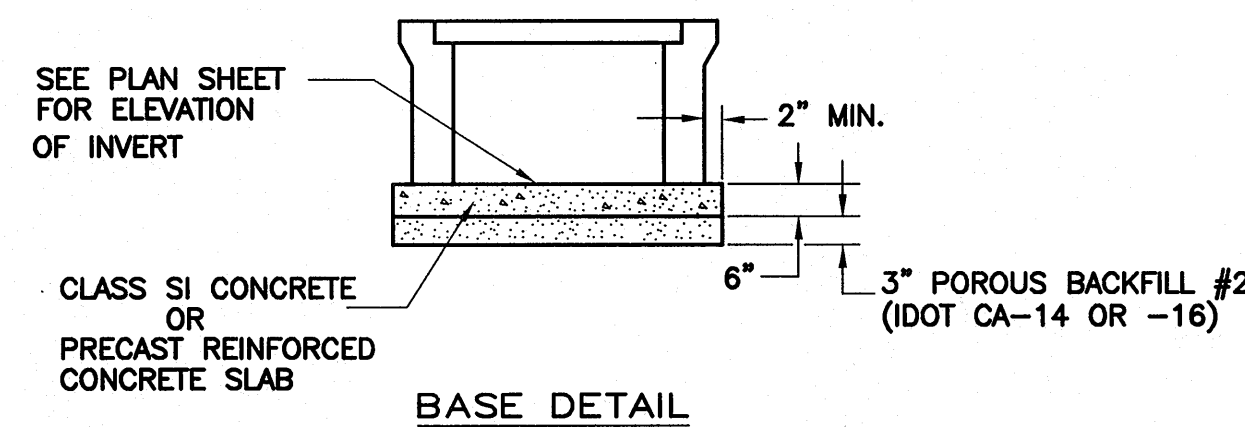
PROPOSED PIPE COUPLERS



SECTION A-A



SECTION A-A



BASE DETAIL

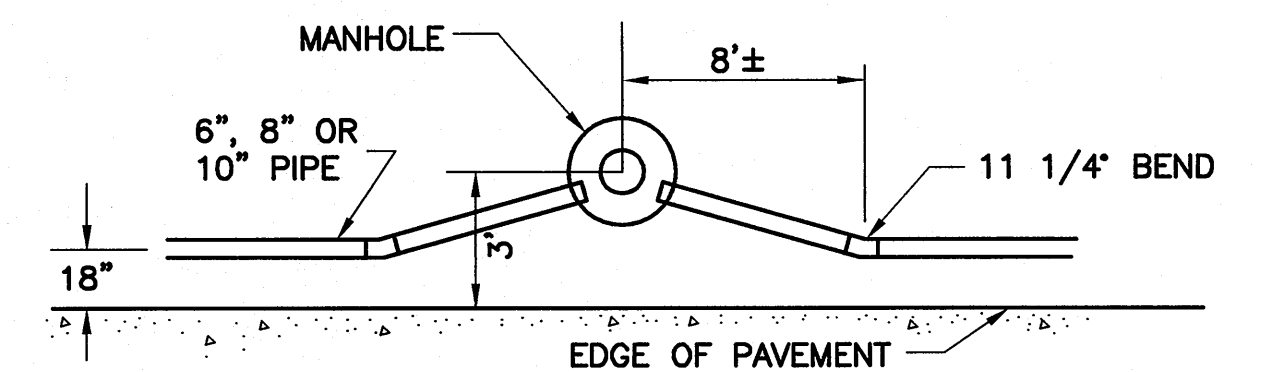
DETAIL OF MANHOLE SPECIAL ITEM 751570

ALTERNATE MATERIALS FOR WALLS	T
PRECAST REINFORCED CONCRETE RINGS	5"
CAST IN PLACE CONCRETE	6"

NOTES :

- PRECAST REINFORCED CONCRETE RINGS AND MANHOLE FRAMES SHALL BE LAID IN FULL MORTAR BEDS WITH FLUSH JOINTS.
- MORTAR MIX SHALL CONSIST OF 1 PART PORTLAND CEMENT AND 3 PARTS SAND.
- ALL CONCRETE SHALL HAVE A MIN. STRENGTH AT 28 DAYS OF 3500 P.S.I.
- THE CONTRACT UNIT PRICE FOR MANHOLE SPECIAL SHALL INCLUDE FURNISHING AND INSTALLING THE FRAME AND LID, THE SAND CUSHION, AND COMPACTING THE BACKFILL MATERIAL. SEE PLAN SHEETS FOR FRAME AND LID TYPE.

MODIFIED IDOT STANDARD 602301

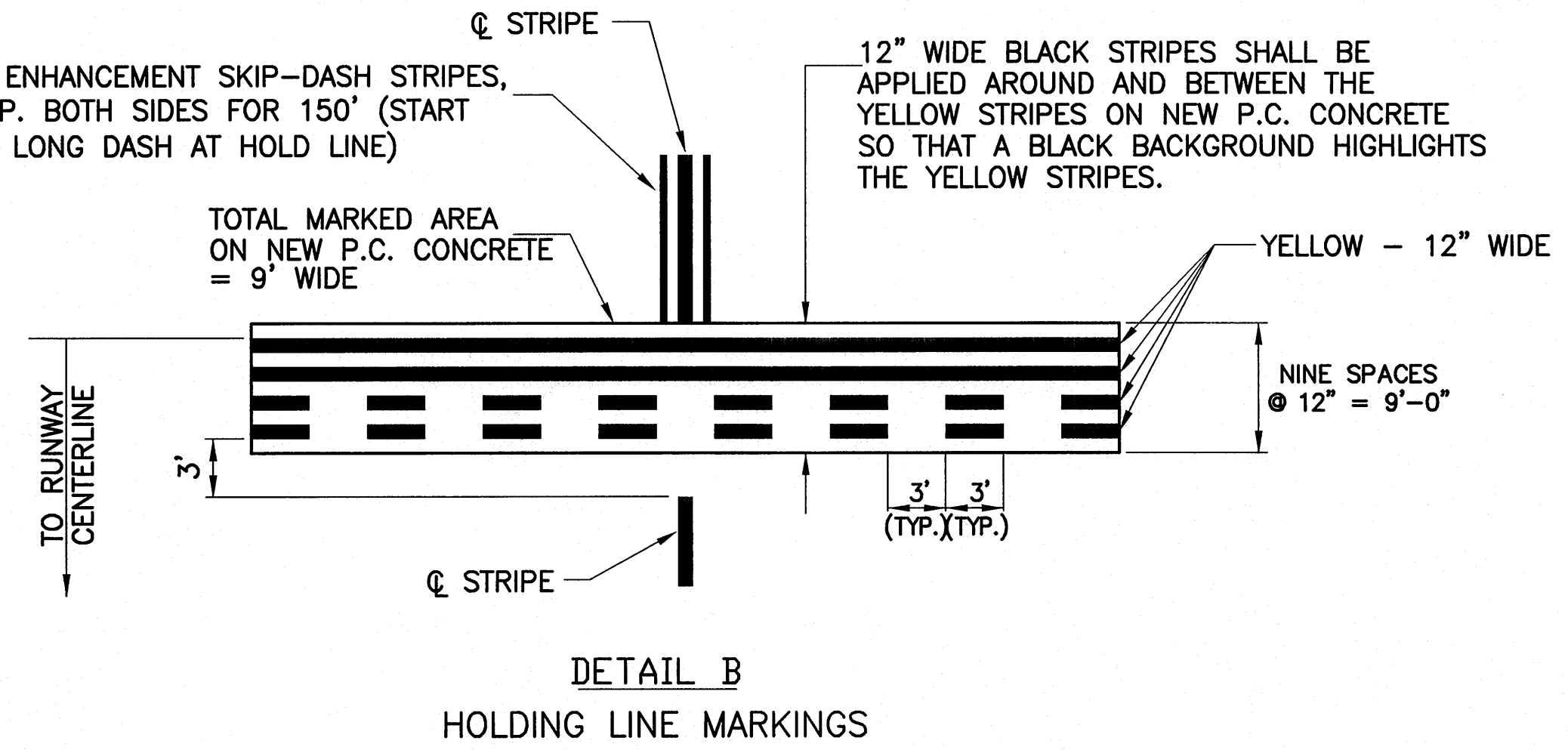
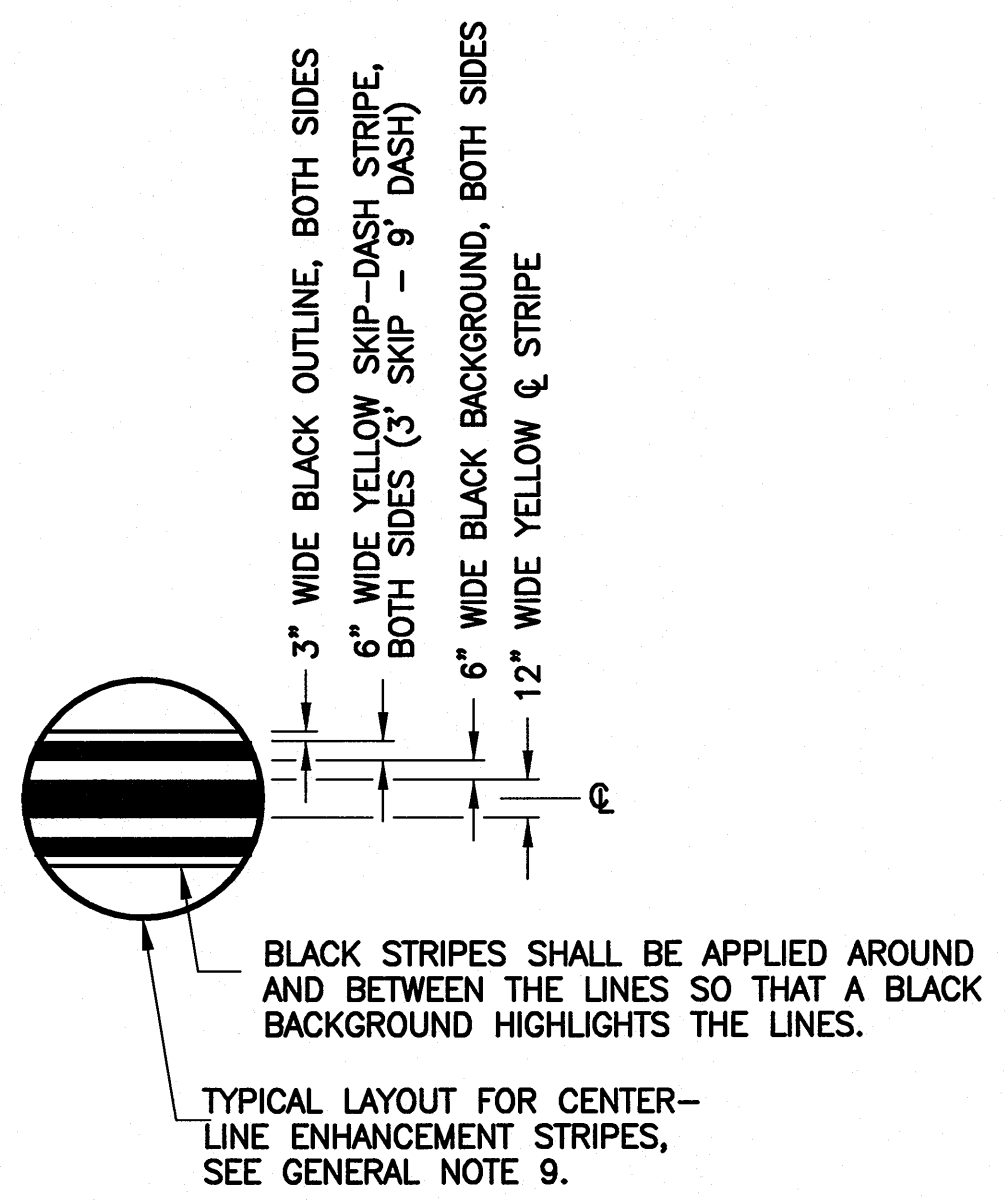
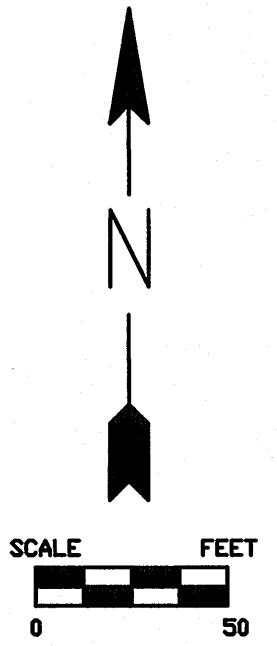
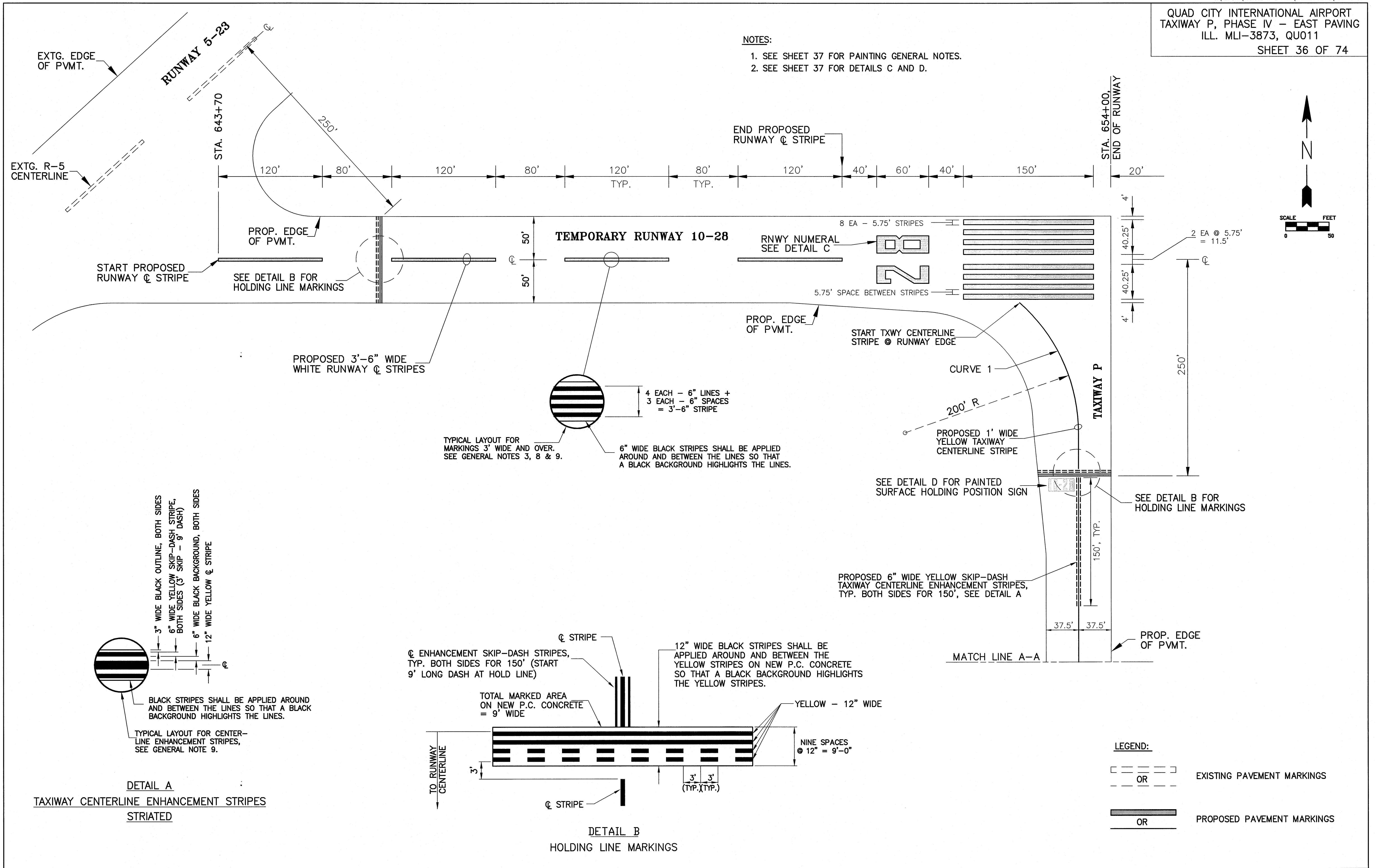


COST OF FITTINGS TO BE INCLUDED IN THE UNIT PRICE FOR UNDERDRAINS.

PLAN VIEW OF MANHOLE SPECIAL ADJACENT TO EDGE OF PVMT.

NOTES:

- 1. SEE SHEET 37 FOR PAINTING GENERAL NOTES.
- 2. SEE SHEET 37 FOR DETAILS C AND D.



LEGEND:

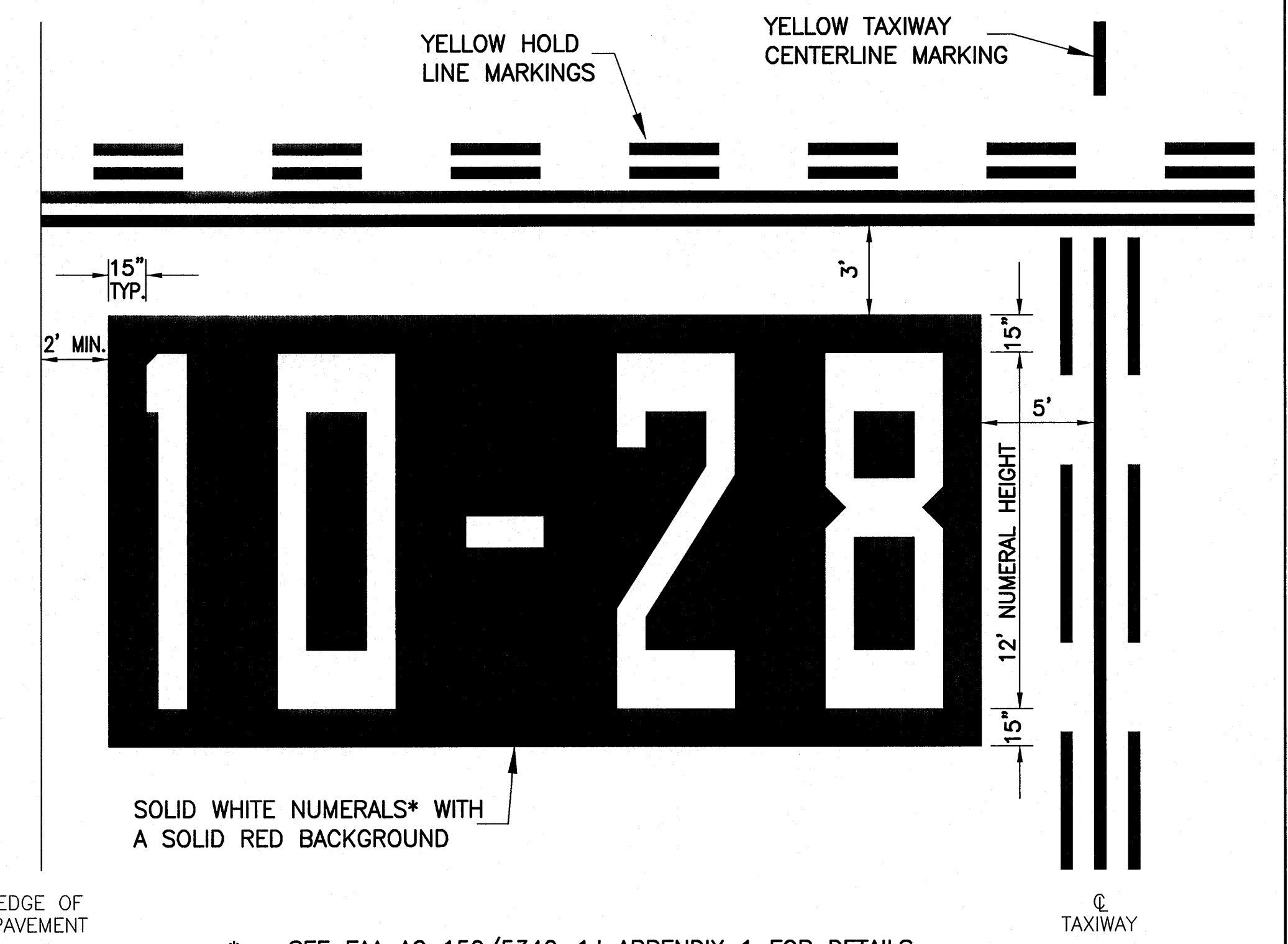
	OR	EXISTING PAVEMENT MARKINGS
	OR	PROPOSED PAVEMENT MARKINGS

G:\airport\A09T002 TP E PVMT\PVMTMRKG.dwg, 7/8/2009 10:21:32 AM, jefm

TAXIWAY CENTERLINE CURVE DATA				
ITEM	CURVE NUMBER			
	1	2	3	4
Δ	48°35'25.36"	90°03'14.84"	89°56'45.16"	53°34'39.48"
D	28°38'52.44"	57°17'44.88"	38°11'49.92"	38°11'49.92"
T	90.28'	100.09'	149.86'	75.73'
L	169.61'	157.17'	235.48'	140.27'
R	200.00'	100.00'	150.00'	150.00'
P.C. STA.	900+50, RT 67.71'	909+56.20	909+06.43	957+94.70
P.I. STA.	901+09.72	910+56.29	910+56.29	958+70.43
P.T. STA.	902+00.00	950+99.91	953+49.86	959+34.97
P.R. STA.	902+00, RT 200'	909+56.20, RT 100'	909+06.43, LT 150'	957+94.70, LT 150'

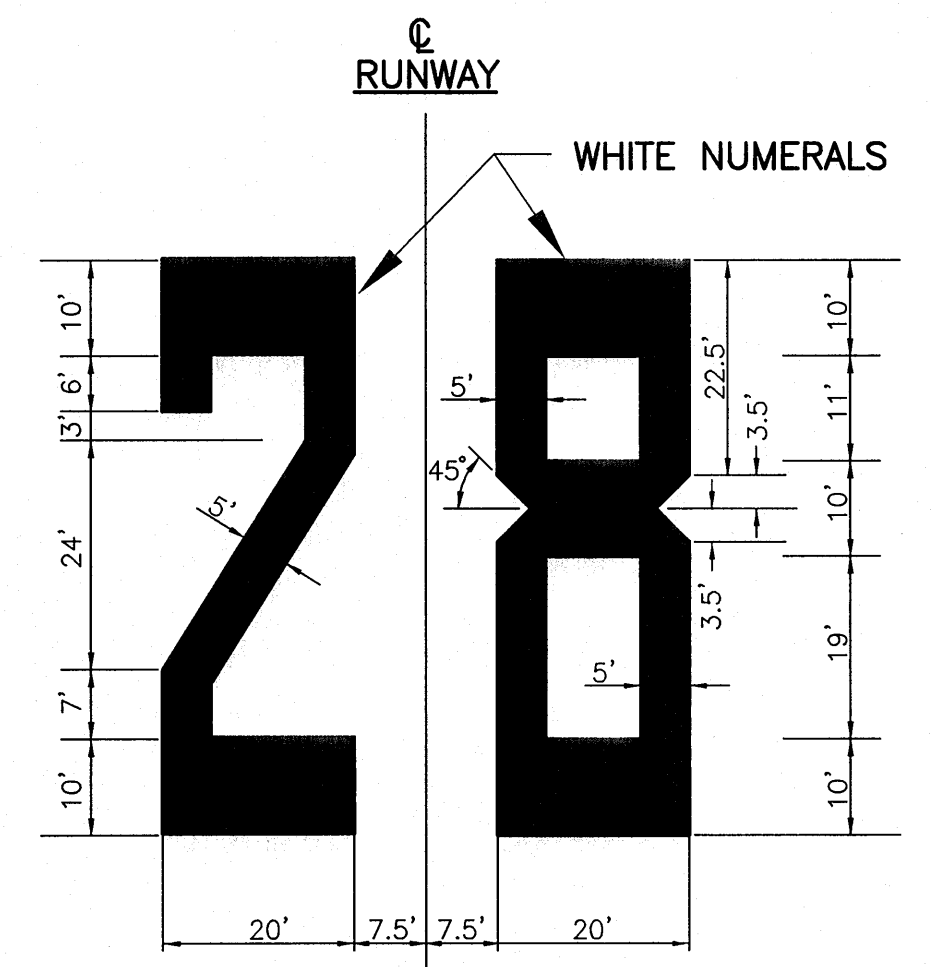
GENERAL NOTES - PAINTING:

1. ALL RUNWAY MARKINGS SHALL BE WHITE UNLESS NOTED OTHERWISE.
2. ALL TAXIWAY MARKINGS SHALL BE YELLOW UNLESS NOTED OTHERWISE.
3. RUNWAY MARKINGS 3' FEET AND OVER SHALL CONSIST OF A SERIES OF LONGITUDINAL STRIPES 6" WIDE WITH EQUAL WIDTH SPACING BETWEEN THE STRIPES (STRIATED MARKINGS.) TAXIWAY PAINTED SURFACE HOLDING POSITION SIGNS ARE NOT STRIATED.
4. ALL MARKINGS SHALL HAVE A REFLECTIVE MEDIA APPLIED IN ACCORDANCE WITH THE SPECIAL PROVISIONS.
5. TAXIWAY EDGE STRIPES AND TAXIWAY CENTERLINE STRIPES SHALL BE SOLID TO MATCH THE EXISTING TAXIWAY MARKINGS. RUNWAY EDGE STRIPES SHALL BE STRIATED TO MATCH THE EXISTING RUNWAY MARKINGS.
6. EXISTING TAXIWAY AND RUNWAY MARKINGS DAMAGED BY CONSTRUCTION OF THE PROJECT SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
7. MARKING LAYOUT BY CONTRACTOR, SEE SPECIAL PROVISIONS.
8. ALL NEW PAVEMENT MARKINGS SHALL BE OUTLINED WITH A SIX INCH (6") WIDE BLACK BORDER. FOR HOLD LINES, THE BLACK BORDER WIDTH SHALL BE INCREASE TO TWELVE INCHES (12").
9. BLACK BACKGROUND STRIPES SHALL BE APPLIED BETWEEN THE YELLOW / WHITE PAVEMENT STRIPES ON STRIATED MARKINGS.
10. THE MAA RESERVES AND SHALL HAVE THE RIGHT TO DELETE THE PAVEMENT MARKING PAY ITEMS FROM THE CONTRACT WORK. THE SPONSOR MAY OPT TO COMPLETE THIS WORK WITH THEIR OWN WORK FORCE.
11. SEE SHEET 36 FOR PAVEMENT MARKING DETAILS A & B.

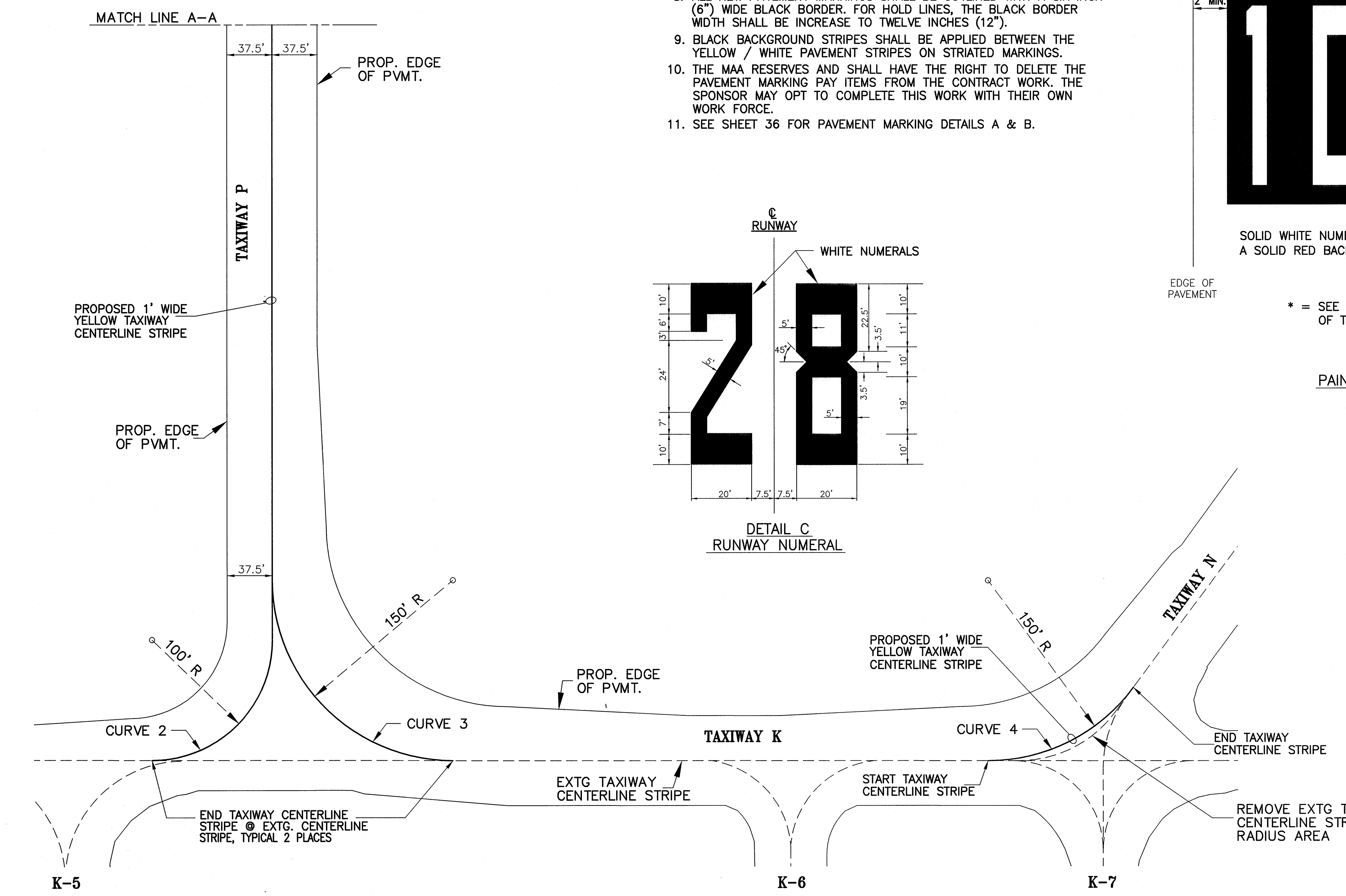


* = SEE FAA AC 150/5340-1J APPENDIX 1 FOR DETAILS OF THE WHITE NUMERAL INSCRIPTION.

DETAIL D
PAINTED SURFACE HOLDING POSITION SIGN



DETAIL C
RUNWAY NUMERAL

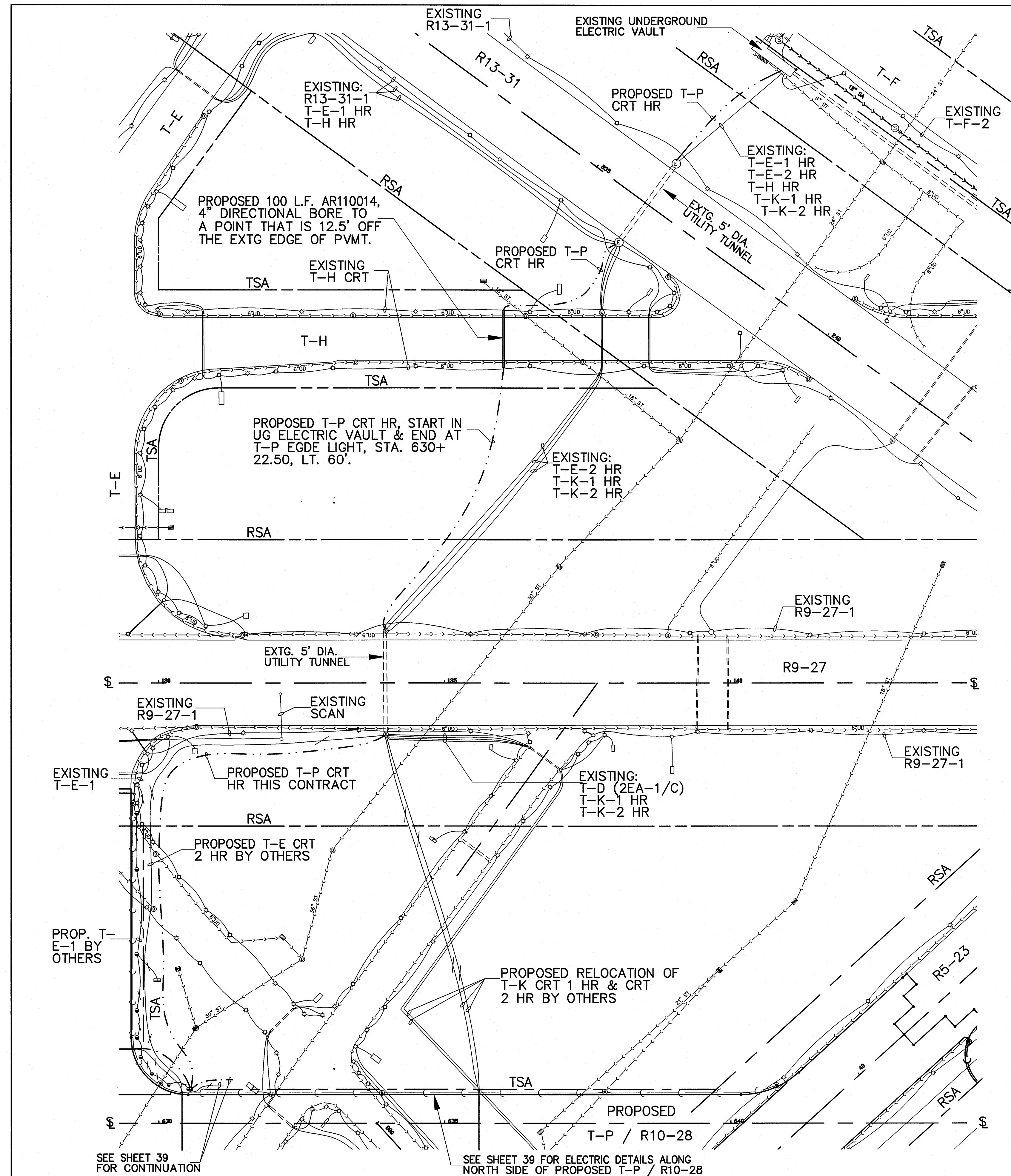


LEGEND:

- EXISTING PAVEMENT MARKINGS
- PROPOSED PAVEMENT MARKINGS

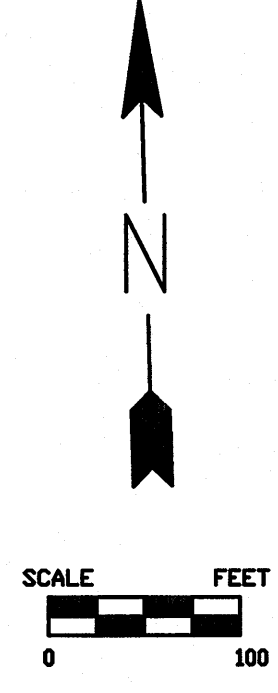
PROPOSED PAVEMENT MARKINGS

G:\AIRPORT\A09T002 TP E PVMT\PVMTMRKG.dwg, 7/8/2009 10:21:47 AM, jeffm



GENERAL NOTES:

1. SEE SHEET 46 FOR ELECTRICAL GENERAL NOTES.
2. SEE SHEETS 46-49 FOR ELECTRICAL DETAILS
3. SEE SHEET 47 FOR SIGN DETAILS.
4. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAYED FOR.



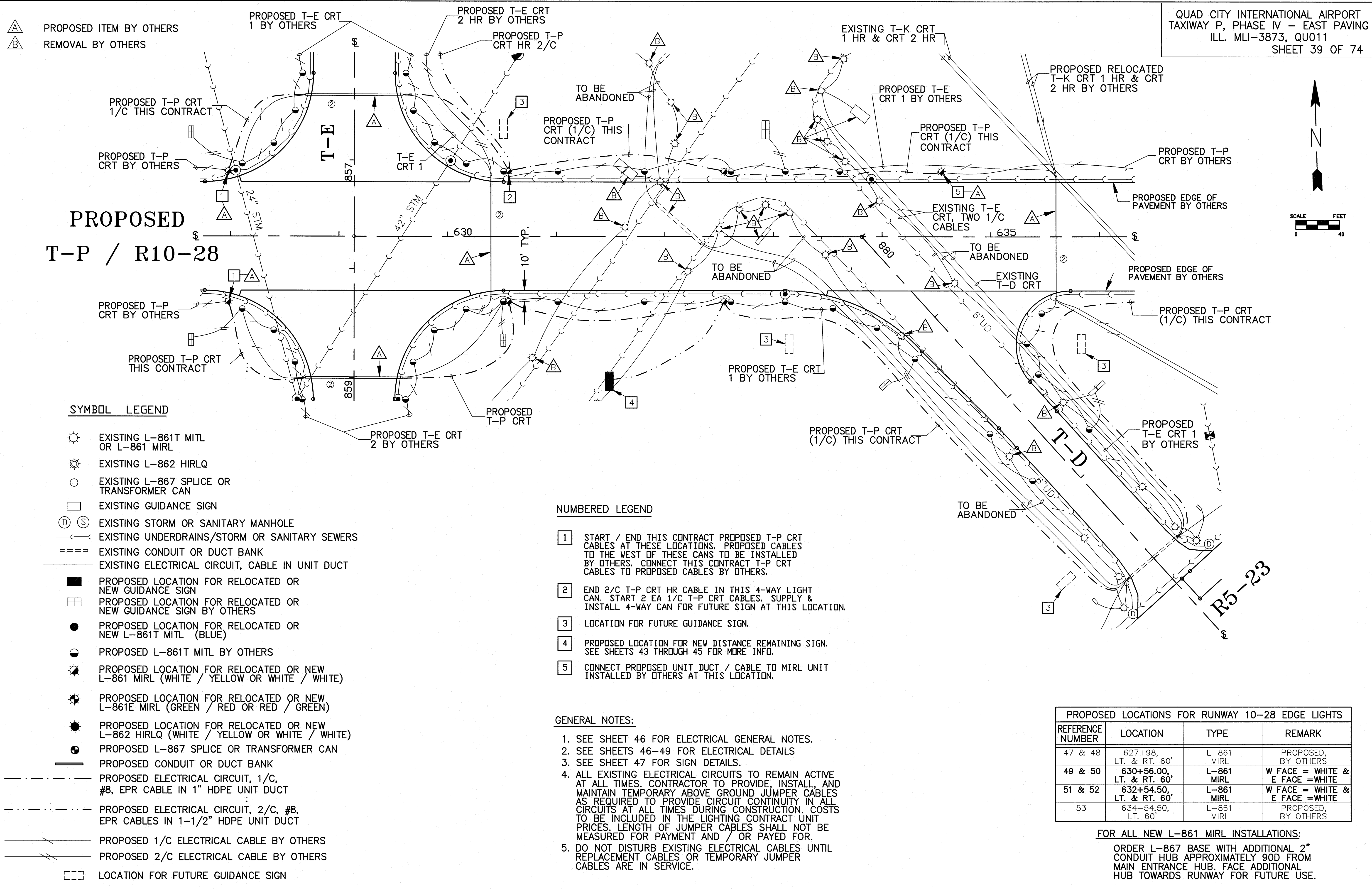
SYMBOL LEGEND

- EXISTING EDGE LIGHT
- EXISTING L-867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- EXISTING STORM OR SANITARY MANHOLE
- EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN
- PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN BY OTHERS
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861T MITL (BLUE)
- PROPOSED L-861T MITL BY OTHERS
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861 MIRL (WHITE / YELLOW OR WHITE / WHITE)
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861E MIRL (GREEN / RED OR RED / GREEN)
- PROPOSED LOCATION FOR RELOCATED OR NEW L-862 HIRLQ (WHITE / YELLOW OR WHITE / WHITE)
- PROPOSED L-867 SPLICE OR TRANSFORMER CAN
- PROPOSED CONDUIT OR DUCT BANK
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1\"/>

SEE SHEET 39 FOR CONTINUATION

SEE SHEET 39 FOR ELECTRIC DETAILS ALONG NORTH SIDE OF PROPOSED T-P / R10-28

G:\AIRPORT\A09T002 TP E PVMT\LIGHTING.dwg, 7/8/2009 10:22:17 AM, jeffm



**PROPOSED
T-P / R10-28**

SYMBOL LEGEND

- ☼ EXISTING L-861T MITL OR L-861 MIRL
- ☼ EXISTING L-862 HIRLQ
- EXISTING L-867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- ④ ⑤ EXISTING STORM OR SANITARY MANHOLE
- EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- ==== EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN
- ▣ PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN BY OTHERS
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861T MITL (BLUE)
- PROPOSED L-861T MITL BY OTHERS
- ☼ PROPOSED LOCATION FOR RELOCATED OR NEW L-861 MIRL (WHITE / YELLOW OR WHITE / WHITE)
- ☼ PROPOSED LOCATION FOR RELOCATED OR NEW L-861E MIRL (GREEN / RED OR RED / GREEN)
- ☼ PROPOSED LOCATION FOR RELOCATED OR NEW L-862 HIRLQ (WHITE / YELLOW OR WHITE / WHITE)
- PROPOSED L-867 SPLICE OR TRANSFORMER CAN
- PROPOSED CONDUIT OR DUCT BANK
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1" HDPE UNIT DUCT
- PROPOSED ELECTRICAL CIRCUIT, 2/C, #8, EPR CABLES IN 1-1/2" HDPE UNIT DUCT
- PROPOSED 1/C ELECTRICAL CABLE BY OTHERS
- PROPOSED 2/C ELECTRICAL CABLE BY OTHERS
- LOCATION FOR FUTURE GUIDANCE SIGN

NUMBERED LEGEND

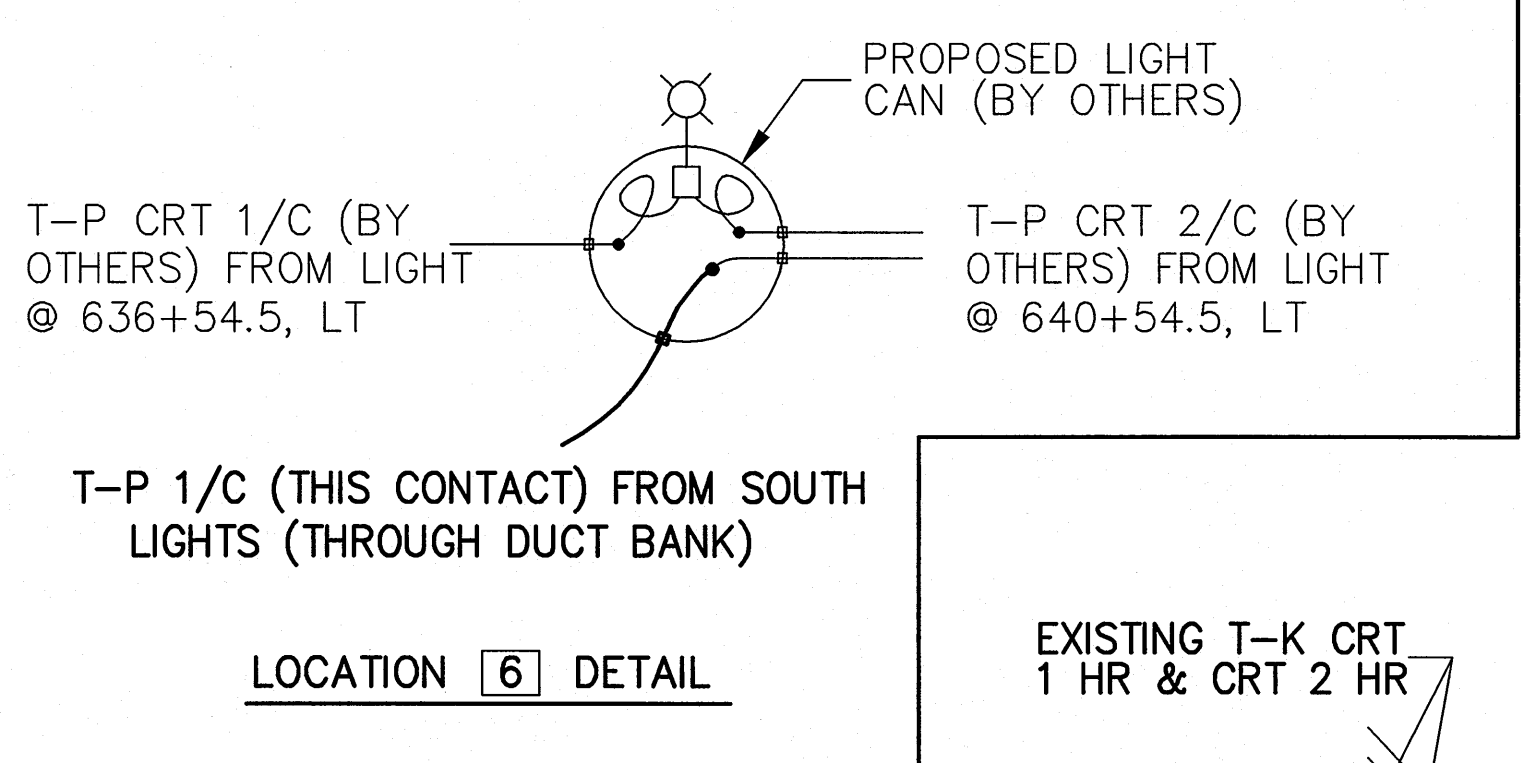
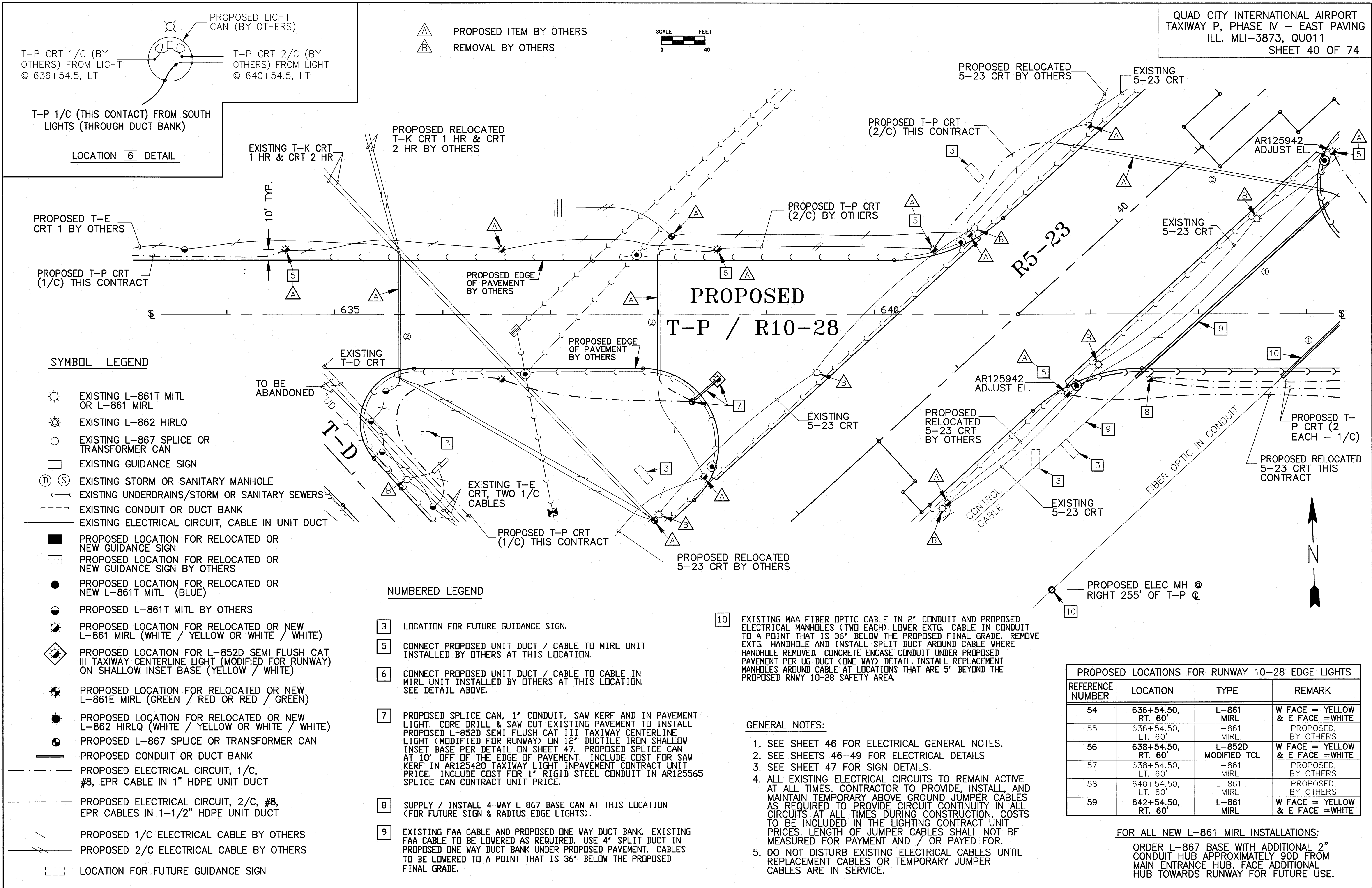
- 1 START / END THIS CONTRACT PROPOSED T-P CRT CABLES AT THESE LOCATIONS. PROPOSED CABLES TO THE WEST OF THESE CANS TO BE INSTALLED BY OTHERS. CONNECT THIS CONTRACT T-P CRT CABLES TO PROPOSED CABLES BY OTHERS.
- 2 END 2/C T-P CRT HR CABLE IN THIS 4-WAY LIGHT CAN. START 2 EA 1/C T-P CRT CABLES. SUPPLY & INSTALL 4-WAY CAN FOR FUTURE SIGN AT THIS LOCATION.
- 3 LOCATION FOR FUTURE GUIDANCE SIGN.
- 4 PROPOSED LOCATION FOR NEW DISTANCE REMAINING SIGN. SEE SHEETS 43 THROUGH 45 FOR MORE INFO.
- 5 CONNECT PROPOSED UNIT DUCT / CABLE TO MIRL UNIT INSTALLED BY OTHERS AT THIS LOCATION.

GENERAL NOTES:

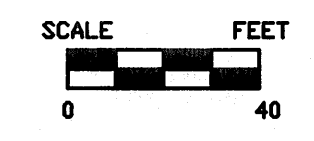
1. SEE SHEET 46 FOR ELECTRICAL GENERAL NOTES.
2. SEE SHEETS 46-49 FOR ELECTRICAL DETAILS
3. SEE SHEET 47 FOR SIGN DETAILS.
4. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAYED FOR.
5. DO NOT DISTURB EXISTING ELECTRICAL CABLES UNTIL REPLACEMENT CABLES OR TEMPORARY JUMPER CABLES ARE IN SERVICE.

PROPOSED LOCATIONS FOR RUNWAY 10-28 EDGE LIGHTS			
REFERENCE NUMBER	LOCATION	TYPE	REMARK
47 & 48	627+98, LT. & RT. 60'	L-861 MIRL	PROPOSED, BY OTHERS
49 & 50	630+56.00, LT. & RT. 60'	L-861 MIRL	W FACE = WHITE & E FACE = WHITE
51 & 52	632+54.50, LT. & RT. 60'	L-861 MIRL	W FACE = WHITE & E FACE = WHITE
53	634+54.50, LT. 60'	L-861 MIRL	PROPOSED, BY OTHERS

FOR ALL NEW L-861 MIRL INSTALLATIONS:
ORDER L-867 BASE WITH ADDITIONAL 2" CONDUIT HUB APPROXIMATELY 90D FROM MAIN ENTRANCE HUB. FACE ADDITIONAL HUB TOWARDS RUNWAY FOR FUTURE USE.



▲ PROPOSED ITEM BY OTHERS
△ REMOVAL BY OTHERS



SYMBOL LEGEND

- ☼ EXISTING L-861T MITL OR L-861 MIRL
- ☼ EXISTING L-862 HIRLQ
- EXISTING L-867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- Ⓧ Ⓨ EXISTING STORM OR SANITARY MANHOLE
- EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- ==== EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN
- ▣ PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN BY OTHERS
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861T MITL (BLUE)
- PROPOSED L-861T MITL BY OTHERS
- ☼ PROPOSED LOCATION FOR RELOCATED OR NEW L-861 MIRL (WHITE / YELLOW OR WHITE / WHITE)
- ☼ PROPOSED LOCATION FOR RELOCATED OR NEW L-862 HIRLQ (WHITE / YELLOW OR WHITE / WHITE)
- PROPOSED L-867 SPLICE OR TRANSFORMER CAN
- PROPOSED CONDUIT OR DUCT BANK
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1" HDPE UNIT DUCT
- PROPOSED ELECTRICAL CIRCUIT, 2/C, #8, EPR CABLES IN 1-1/2" HDPE UNIT DUCT
- PROPOSED 1/C ELECTRICAL CABLE BY OTHERS
- PROPOSED 2/C ELECTRICAL CABLE BY OTHERS
- LOCATION FOR FUTURE GUIDANCE SIGN

NUMBERED LEGEND

- 3 LOCATION FOR FUTURE GUIDANCE SIGN.
- 5 CONNECT PROPOSED UNIT DUCT / CABLE TO MIRL UNIT INSTALLED BY OTHERS AT THIS LOCATION.
- 6 CONNECT PROPOSED UNIT DUCT / CABLE TO CABLE IN MIRL UNIT INSTALLED BY OTHERS AT THIS LOCATION. SEE DETAIL ABOVE.
- 7 PROPOSED SPLICE CAN, 1' CONDUIT, SAW KERF AND IN PAVEMENT LIGHT. CORE DRILL & SAW CUT EXISTING PAVEMENT TO INSTALL PROPOSED L-852D SEMI FLUSH CAT III TAXIWAY CENTERLINE LIGHT (MODIFIED FOR RUNWAY) ON 12" DUCTILE IRON SHALLOW INSET BASE PER DETAIL ON SHEET 47. PROPOSED SPLICE CAN AT 10' OFF OF THE EDGE OF PAVEMENT. INCLUDE COST FOR SAW KERF IN AR125420 TAXIWAY LIGHT IMPAVEMENT CONTRACT UNIT PRICE. INCLUDE COST FOR 1" RIGID STEEL CONDUIT IN AR125565 SPLICE CAN CONTRACT UNIT PRICE.
- 8 SUPPLY / INSTALL 4-WAY L-867 BASE CAN AT THIS LOCATION (FOR FUTURE SIGN & RADIUS EDGE LIGHTS).
- 9 EXISTING FAA CABLE AND PROPOSED ONE WAY DUCT BANK. EXISTING FAA CABLE TO BE LOWERED AS REQUIRED. USE 4" SPLIT DUCT IN PROPOSED ONE WAY DUCT BANK UNDER PROPOSED PAVEMENT. CABLES TO BE LOWERED TO A POINT THAT IS 36" BELOW THE PROPOSED FINAL GRADE.

10 EXISTING MAA FIBER OPTIC CABLE IN 2" CONDUIT AND PROPOSED ELECTRICAL MANHOLES (TWO EACH). LOWER EXTG. CABLE IN CONDUIT TO A POINT THAT IS 36" BELOW THE PROPOSED FINAL GRADE. REMOVE EXTG. HANDHOLE AND INSTALL SPLIT DUCT AROUND CABLE WHERE HANDHOLE REMOVED. CONCRETE ENCASE CONDUIT UNDER PROPOSED PAVEMENT PER UG DUCT (ONE WAY) DETAIL. INSTALL REPLACEMENT MANHOLES AROUND CABLE AT LOCATIONS THAT ARE 5' BEYOND THE PROPOSED RNVY 10-28 SAFETY AREA.

GENERAL NOTES:

1. SEE SHEET 46 FOR ELECTRICAL GENERAL NOTES.
2. SEE SHEETS 46-49 FOR ELECTRICAL DETAILS
3. SEE SHEET 47 FOR SIGN DETAILS.
4. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAYED FOR.
5. DO NOT DISTURB EXISTING ELECTRICAL CABLES UNTIL REPLACEMENT CABLES OR TEMPORARY JUMPER CABLES ARE IN SERVICE.

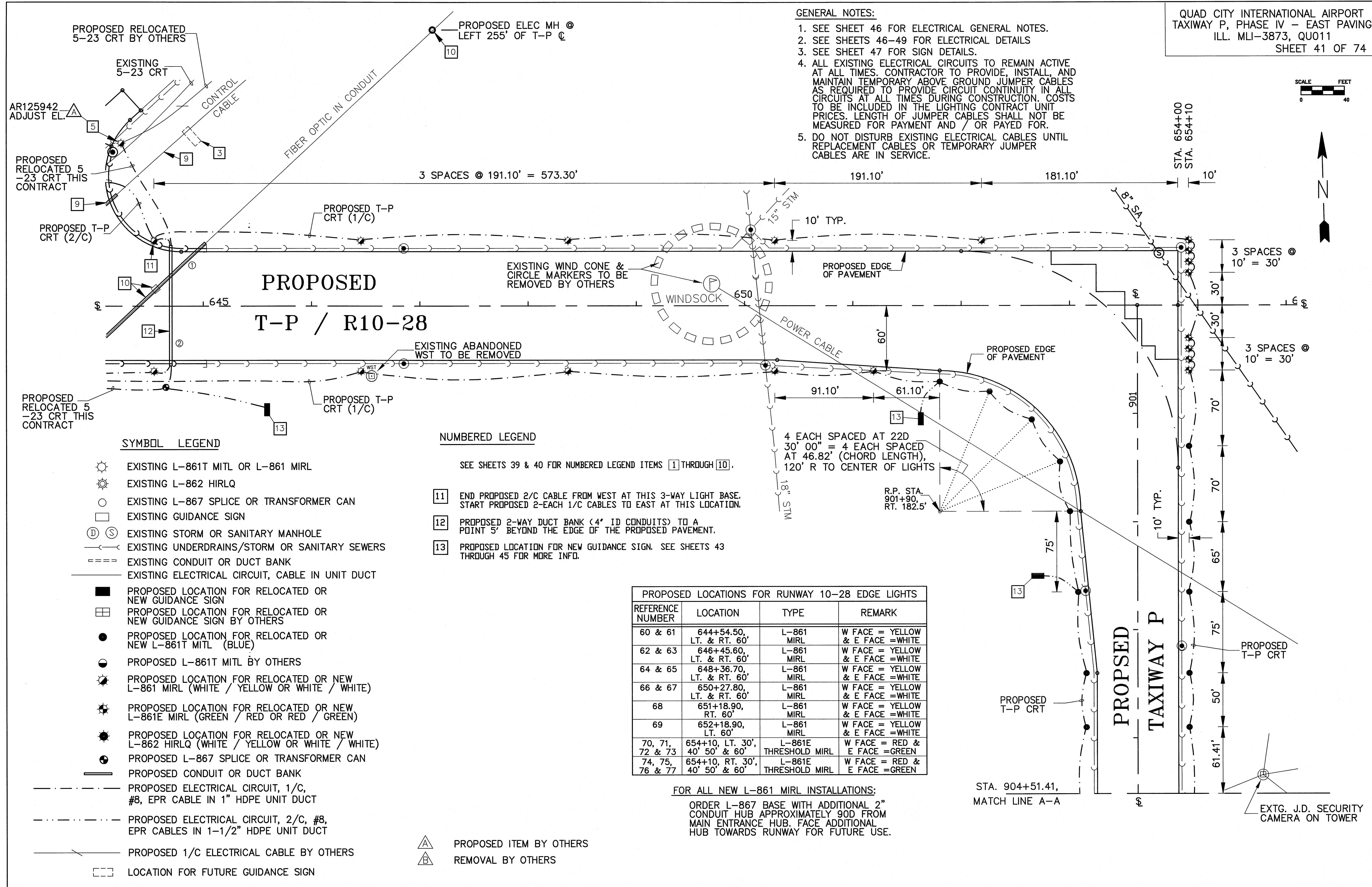
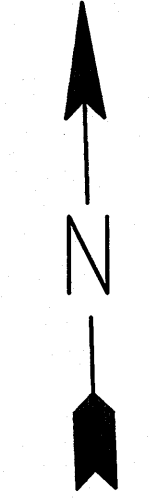
REFERENCE NUMBER	LOCATION	TYPE	REMARK
54	636+54.50, RT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE
55	636+54.50, LT. 60'	L-861 MIRL	PROPOSED, BY OTHERS
56	638+54.50, RT. 60'	L-852D MODIFIED TCL	W FACE = YELLOW & E FACE = WHITE
57	638+54.50, LT. 60'	L-861 MIRL	PROPOSED BY OTHERS
58	640+54.50, LT. 60'	L-861 MIRL	PROPOSED, BY OTHERS
59	642+54.50, RT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE

FOR ALL NEW L-861 MIRL INSTALLATIONS:
ORDER L-867 BASE WITH ADDITIONAL 2" CONDUIT HUB APPROXIMATELY 90D FROM MAIN ENTRANCE HUB. FACE ADDITIONAL HUB TOWARDS RUNWAY FOR FUTURE USE.

G:\AIRPORT\A09T002 TP E P\M\LIGHTING.DWG, 7/8/2009 10:22:57 AM, jeffm

GENERAL NOTES:

1. SEE SHEET 46 FOR ELECTRICAL GENERAL NOTES.
2. SEE SHEETS 46-49 FOR ELECTRICAL DETAILS
3. SEE SHEET 47 FOR SIGN DETAILS.
4. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAYED FOR.
5. DO NOT DISTURB EXISTING ELECTRICAL CABLES UNTIL REPLACEMENT CABLES OR TEMPORARY JUMPER CABLES ARE IN SERVICE.



SYMBOL LEGEND

- EXISTING L-861T MITL OR L-861 MIRL
- EXISTING L-862 HIRLQ
- EXISTING L-867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- EXISTING STORM OR SANITARY MANHOLE
- EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN
- PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN BY OTHERS
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861T MITL (BLUE)
- PROPOSED L-861T MITL BY OTHERS
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861E MIRL (GREEN / RED OR RED / GREEN)
- PROPOSED LOCATION FOR RELOCATED OR NEW L-862 HIRLQ (WHITE / YELLOW OR WHITE / WHITE)
- PROPOSED L-867 SPLICE OR TRANSFORMER CAN
- PROPOSED CONDUIT OR DUCT BANK
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1\"/>

NUMBERED LEGEND

- SEE SHEETS 39 & 40 FOR NUMBERED LEGEND ITEMS 1 THROUGH 10.
- 11 END PROPOSED 2/C CABLE FROM WEST AT THIS 3-WAY LIGHT BASE. START PROPOSED 2-EACH 1/C CABLES TO EAST AT THIS LOCATION.
 - 12 PROPOSED 2-WAY DUCT BANK (4\"/>

REFERENCE NUMBER	LOCATION	TYPE	REMARK
60 & 61	644+54.50, LT. & RT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE
62 & 63	646+45.60, LT. & RT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE
64 & 65	648+36.70, LT. & RT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE
66 & 67	650+27.80, LT. & RT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE
68	651+18.90, RT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE
69	652+18.90, LT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE
70, 71, 72 & 73	654+10, LT. 30', 40' 50\"/>		
74, 75, 76 & 77	654+10, RT. 30', 40' 50\"/>		

FOR ALL NEW L-861 MIRL INSTALLATIONS:

ORDER L-867 BASE WITH ADDITIONAL 2\"/>

GENERAL NOTES:

1. SEE SHEET 46 FOR ELECTRICAL GENERAL NOTES.
2. SEE SHEETS 46-49 FOR ELECTRICAL DETAILS
3. SEE SHEET 47 FOR SIGN DETAILS.
4. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAYED FOR.
5. DO NOT DISTURB EXISTING ELECTRICAL CABLES UNTIL REPLACEMENT CABLES OR TEMPORARY JUMPER CABLES ARE IN SERVICE.

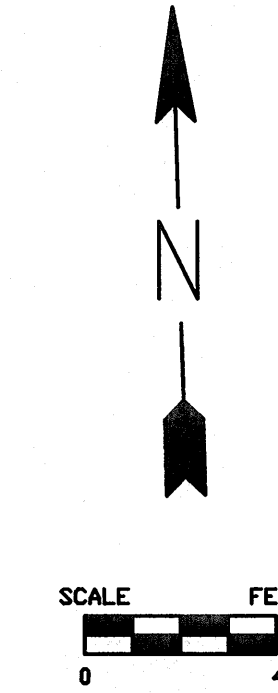
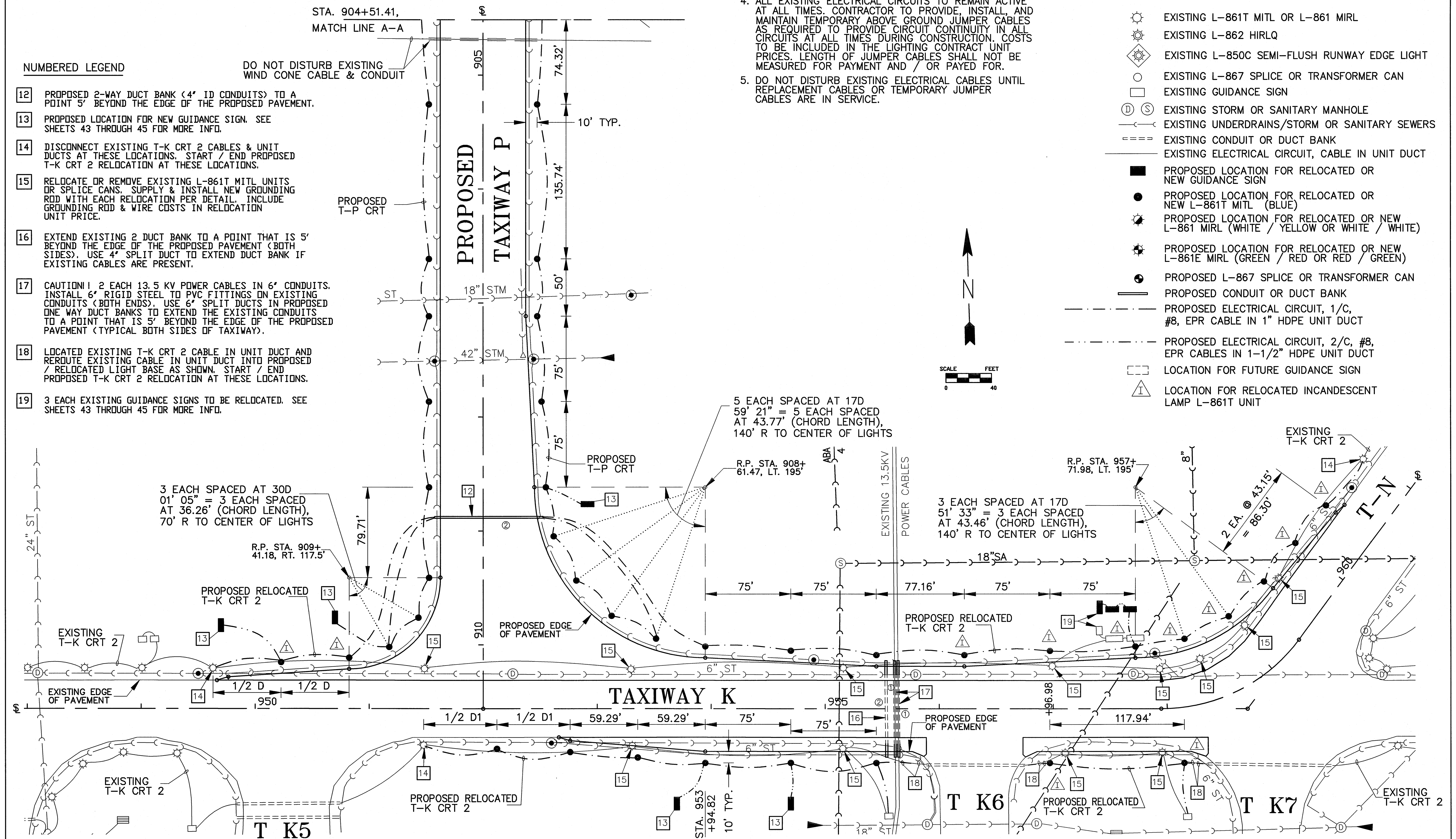
SYMBOL LEGEND

- EXISTING L-861T MITL OR L-861 MIRL
- EXISTING L-862 HIRLQ
- EXISTING L-850C SEMI-FLUSH RUNWAY EDGE LIGHT
- EXISTING L-867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- EXISTING STORM OR SANITARY MANHOLE
- EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861T MITL (BLUE)
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861 MIRL (WHITE / YELLOW OR WHITE / WHITE)
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861E MIRL (GREEN / RED OR RED / GREEN)
- PROPOSED L-867 SPLICE OR TRANSFORMER CAN
- PROPOSED CONDUIT OR DUCT BANK
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1" HDPE UNIT DUCT
- PROPOSED ELECTRICAL CIRCUIT, 2/C, #8, EPR CABLES IN 1-1/2" HDPE UNIT DUCT
- LOCATION FOR FUTURE GUIDANCE SIGN
- LOCATION FOR RELOCATED INCANDESCENT LAMP L-861T UNIT

NUMBERED LEGEND

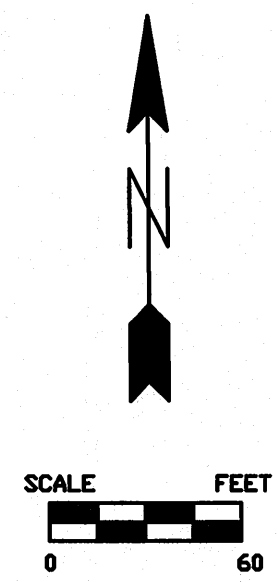
12. PROPOSED 2-WAY DUCT BANK (4" ID CONDUITS) TO A POINT 5' BEYOND THE EDGE OF THE PROPOSED PAVEMENT.
13. PROPOSED LOCATION FOR NEW GUIDANCE SIGN. SEE SHEETS 43 THROUGH 45 FOR MORE INFO.
14. DISCONNECT EXISTING T-K CRT 2 CABLES & UNIT DUCTS AT THESE LOCATIONS. START / END PROPOSED T-K CRT 2 RELOCATION AT THESE LOCATIONS.
15. RELOCATE OR REMOVE EXISTING L-861T MITL UNITS OR SPLICE CANS. SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER DETAIL. INCLUDE GROUNDING ROD & WIRE COSTS IN RELOCATION UNIT PRICE.
16. EXTEND EXISTING 2 DUCT BANK TO A POINT THAT IS 5' BEYOND THE EDGE OF THE PROPOSED PAVEMENT (BOTH SIDES). USE 4" SPLIT DUCT TO EXTEND DUCT BANK IF EXISTING CABLES ARE PRESENT.
17. CAUTION! 2 EACH 13.5 KV POWER CABLES IN 6" CONDUITS. INSTALL 6" RIGID STEEL TO PVC FITTINGS ON EXISTING CONDUITS (BOTH ENDS). USE 6" SPLIT DUCTS IN PROPOSED ONE WAY DUCT BANKS TO EXTEND THE EXISTING CONDUITS TO A POINT THAT IS 5' BEYOND THE EDGE OF THE PROPOSED PAVEMENT (TYPICAL BOTH SIDES OF TAXIWAY).
18. LOCATED EXISTING T-K CRT 2 CABLE IN UNIT DUCT AND REROUTE EXISTING CABLE IN UNIT DUCT INTO PROPOSED / RELOCATED LIGHT BASE AS SHOWN. START / END PROPOSED T-K CRT 2 RELOCATION AT THESE LOCATIONS.
19. 3 EACH EXISTING GUIDANCE SIGNS TO BE RELOCATED. SEE SHEETS 43 THROUGH 45 FOR MORE INFO.

DO NOT DISTURB EXISTING WIND CONE CABLE & CONDUIT



NUMBER OF MODULES	TRANSFORMER WATTAGE			
	STYLE 2, 4.8A-6.6A		STYLE 3, 2.8A-6.6A	
	TRADITIONAL	LED SYSTEM	TRADITIONAL	LED SYSTEM
1	100	200	200	200
2	300	200	300	300
3	500	300	500	500
4	500	300	500	500
DISTANCE REMAINING	300	200	300	300

TRANSFORMERS SHALL BE 6.6/6.6 AMP.
* = OR AS REQUIRED BY SIGN MANUFACTURER.
** = WITH A SIAMESE PIGTAIL ADAPTER AND TWO TRANSFORMERS.



NOTE:
1) SEE SHEET 44 FOR GUIDANCE / DISTANCE REMAINING SIGNS GENERAL NOTES.

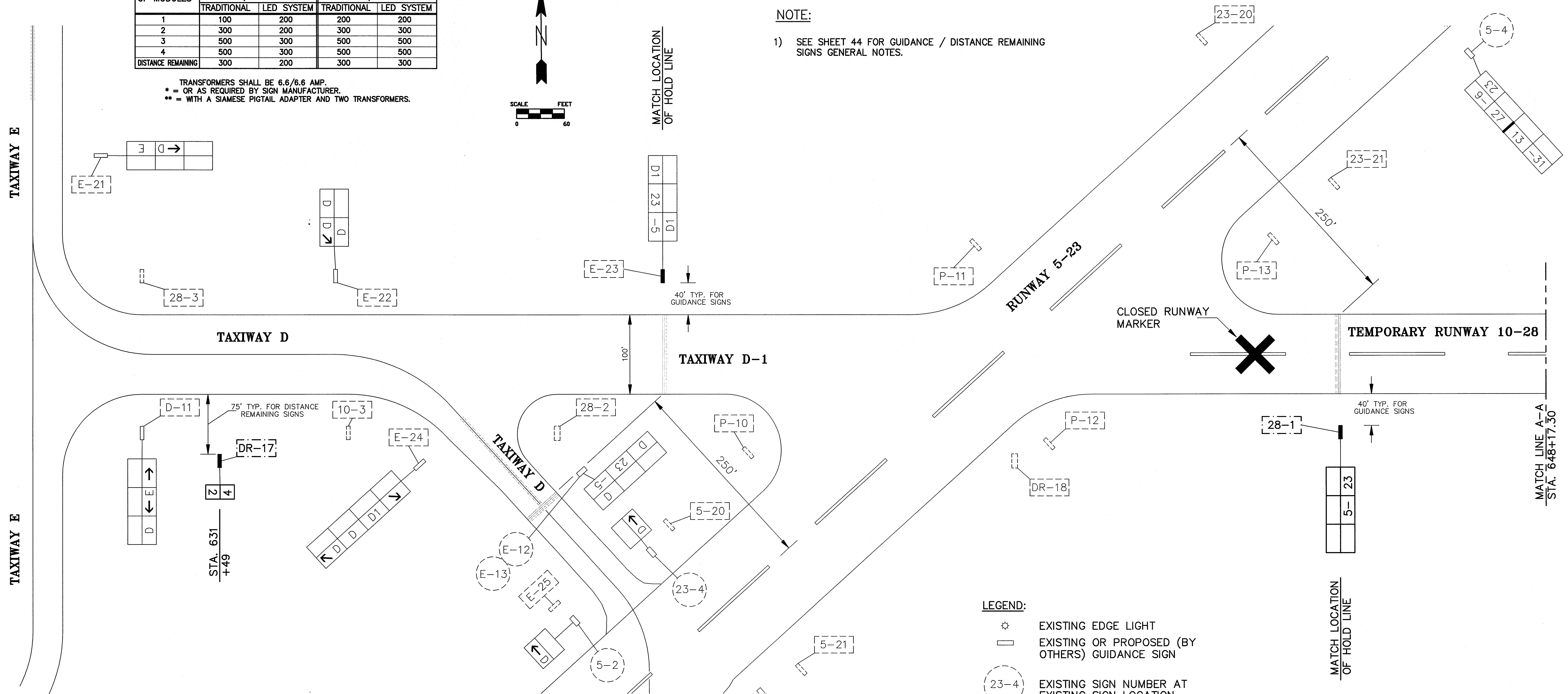
TAXIWAY E

TAXIWAY E

MATCH LOCATION OF HOLD LINE

MATCH LOCATION OF HOLD LINE

MATCH LINE A-A
STA. 648+17.30



- LEGEND:
- ⊛ EXISTING EDGE LIGHT
 - ▭ EXISTING OR PROPOSED (BY OTHERS) GUIDANCE SIGN
 - ⊙ EXISTING SIGN NUMBER AT EXISTING SIGN LOCATION
 - ⊙ EXISTING SIGN NUMBER AT RELOCATED OR NEW SIGN LOCATION
 - ▬ PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN
 - [H-20] PROPOSED NEW SIGN NUMBER
 - [E-20] PROPOSED (BY OTHERS) / FUTURE (BY OTHERS) NEW SIGN NUMBER
 - ⊞ FUTURE (BY OTHERS) GUIDANCE SIGN

PROPOSED TEMPORARY RUNWAY 10-28 DISTANCE REMAINING SIGN SCHEDULE						
SIZE 4, STYLE 3 (WITH LED LIGHTING SYSTEM INCLUDING LED LAMPS), CLASS 2, TYPE L-858B						
SIGN NUMBER	FACE	PROPOSED LEGEND	LETTERS BACKGROUND	CIRCUIT	PAY ITEMS	REMARK
DR-17	W	2	W B	T-P	AR125560	PROPOSED DISTANCE REMAINING SIGN
	E	4	W B			

N = NORTH
S = SOUTH
E = EAST
W = WEST

NW = NORTHWEST
SE = SOUTHEAST
NE = NORTHEAST
SW = SOUTHWEST

B/Y = BLACK LETTERS ON YELLOW BACKGROUND (TYPE L-858Y)
Y/B = YELLOW LETTERS ON BLACK BACKGROUND (TYPE L-858L)
B/B = BLACK BLANK PANEL
Y/Y = YELLOW BLANK PANEL
W/R = WHITE LETTERS ON RED BACKGROUND (TYPE L-858R)
W/B = WHITE LETTERS ON BLACK BACKGROUND (TYPE L-858B)

G:\AIRPORT\A09T002 TP E P\MT\SIGNS.DWG, 7/8/2009 10:24:20 AM, jefrn

GUIDANCE / DISTANCE REMAINING SIGN GENERAL NOTES:

- "LEGEND" COLUMN IN TABLES (FOUND ON SHTS 43, 44 & 45) INDICATES NUMBER OF PANELS (MODULES), SPECIFIC CHARACTERS PER PANEL AND BLANK PANELS. SIGNS ARE TO BE MANUFACTURED AND SUPPLIED AS SHOWN IN THE LEGEND COLUMN UNLESS OTHERWISE APPROVED BY THE MAA.
- ALL SIGNS SHALL COMPLY WITH THE LATEST VERSION OF FAA ADVISORY CIRCULAR 150/5345-44 AND 150/5340-18.
- CONTRACTOR SHALL VERIFY ALL BASE DIMENSIONS WITH MANUFACTURER PRIOR TO INSTALLATION OF ANCHOR BOLTS.
- THE CONTRACTOR SHALL DISASSEMBLE EXISTING GUIDANCE SIGNS AS REQUIRED TO INSTALL PROPOSED NEW SIGN PANELS AND REUSE EXISTING PANELS AS INDICATED IN TABLE. EXISTING PANELS REMOVED MAY ONLY BE REUSED, IF THE CONDITION OF THE PANEL IS ACCEPTABLE TO THE MAA AND THE RESIDENT ENGINEER. THE CONTRACTOR SHALL REPLACE ALL DAMAGED EXISTING PANELS AND EQUIPMENT AS REQUIRED BY THE RESIDENT ENGINEER. ONCE THE SIGNS HAVE BEEN REBUILT, THE CONTRACTOR SHALL SEAL SIGNS AND RETURN THE EXISTING SIGNS TO THEIR ORIGINAL CONDITION. EXISTING PANELS REMOVED AND NOT REUSED SHALL BE DELIVERED BY THE CONTRACTOR TO THE OWNER (MAA).
- SIGNS SHALL BE DOUBLE FACED AS INDICATED IN TABLE (TYPE L-858Y, L-858R, L-858L, OR L-858B). SIGNS AND REPLACEMENT PANELS SHALL BE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND WITH THE EXISTING SIGNS CURRENTLY IN PLACE AT THE QUAD CITY INTERNATIONAL AIRPORT. SIGNS AND REPLACEMENT PANELS SHALL BE LUMACURVE, OR APPROVED EQUAL.
- "FACE" COLUMN INDICATES DIRECTION OF SIGN FACE.
- SEE SHEET 46 - 49 FOR ELECTRICAL GENERAL NOTES AND ELECTRICAL DETAILS.
- SEE SHEET 47 FOR SIGN DETAILS.
- WHERE PROPOSED SIGNS ARE INSTALLED BESIDE EXISTING SIGNS PROVIDE 1' SPACE BETWEEN SIGNS.

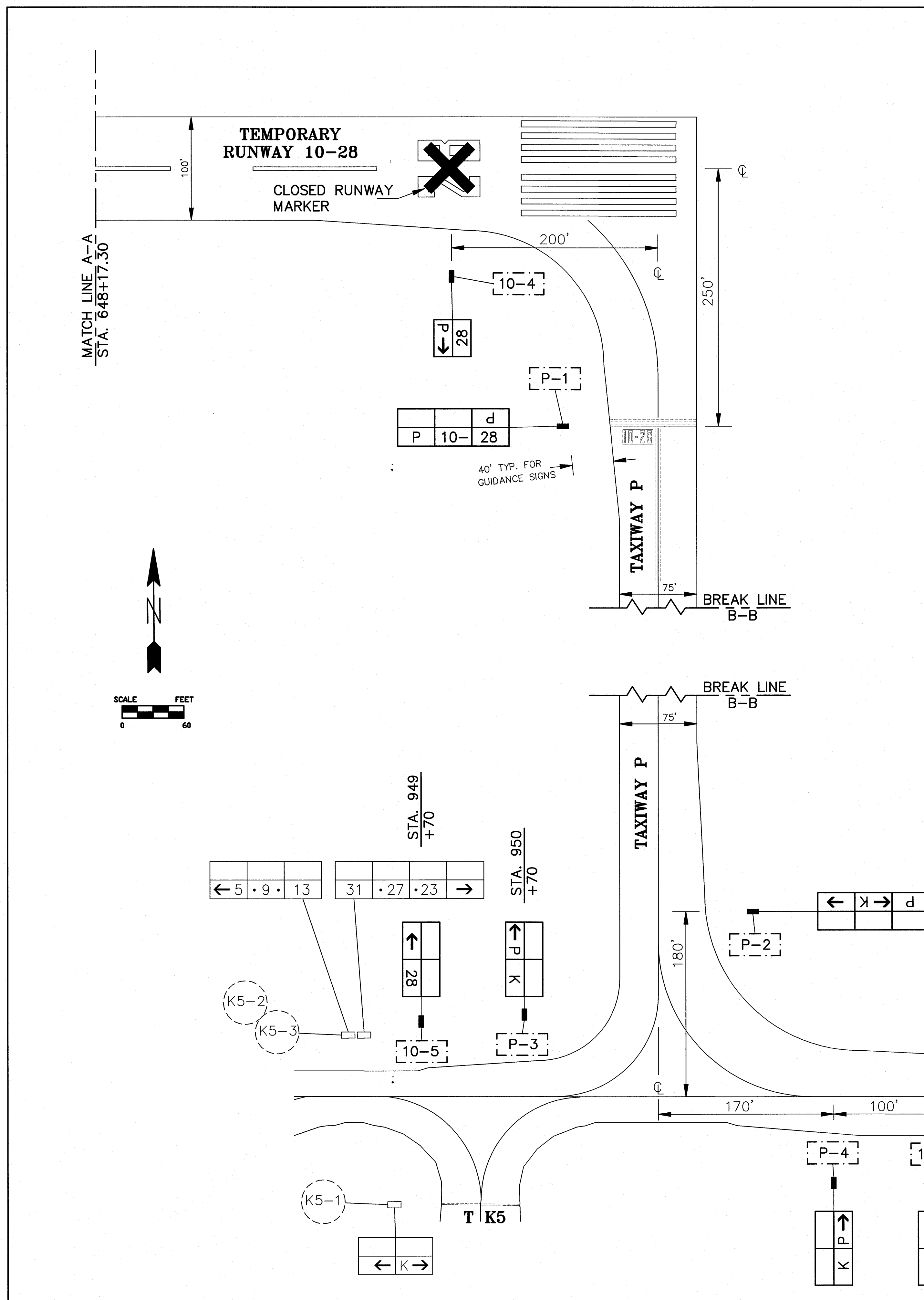
SCHEDULE OF EXISTING GUIDANCE SIGNS TO BE REFURBISHED
EXISTING TRADITIONAL INCANDESCENT SIZE 3, STYLE 2**, CLASS 2 SIGNS TO LED LIGHTING SYSTEM SIGNS WITH LED LAMPS

SIGN NUMBER	FACE	EXISTING LEGEND	LETTERS		CIRCUIT	PAY ITEMS*	REMARK
			BACKGROUND				
D-11	W	[] [] []	B	B	T-E-2	AR125984	RETROFIT EXISTING 3 MODULE SIGN
	E	[D ← E →]	Y	Y			
E-21	N	[← D E]	B	B	T-E-1	AR125984	RETROFIT EXISTING 3 MODULE SIGN
	S	[] [] []	B	B			
E-22	W	[D D ↗]	Y	Y	T-E-1	AR125984	RETROFIT EXISTING 2 MODULE SIGN
	E	[D]	Y	Y			
E-23**	N	[D1 23 - 5]	Y	W	R5-23**	AR125984	RETROFIT EXISTING 3 MODULE SIGN**
	E	[D1]	Y	Y			
E-24	NW	[] [] []	B	B	T-E-1	AR125984	RETROFIT EXISTING 4 MODULE SIGN
	SE	[↖ D D D1 ↘]	B	Y			

* = THE AR125984 - REFURBISH TAXI GUIDANCE SIGN CONTRACT UNIT PRICE SHALL INCLUDE ALL COSTS TO SUPPLY & INSTALL NEW LED LIGHTING SYSTEMS (WITH LED LAMPS AND ISOLATION TRANSFORMERS AS REQUIRED BY THE MANUFACTURER) IN EXISTING TAXI GUIDANCE SIGNS.
** = E-23 IS A STYLE 3 (5 STEP) UNIT. ALL OTHERS SIGNS LISTED ARE STYLE 2 (3 STEP) UNITS.

LEGEND:

- ⊛ EXISTING EDGE LIGHT
- [] EXISTING OR PROPOSED (BY OTHERS) GUIDANCE SIGN
- (23-4) EXISTING SIGN NUMBER AT EXISTING SIGN LOCATION
- ⊙(23-4) EXISTING SIGN NUMBER AT RELOCATED OR NEW SIGN LOCATION
- PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN
- [H-20] PROPOSED NEW SIGN NUMBER
- [E-20] PROPOSED (BY OTHERS) / FUTURE (BY OTHERS) NEW SIGN NUMBER
- [] FUTURE (BY OTHERS) GUIDANCE SIGN



PROPOSED TEMPORARY RUNWAY 10-28 GUIDANCE SIGN SCHEDULE SIZE 3, STYLE 2 (WITH LED LIGHTING SYSTEM INCLUDING LED LAMPS), CLASS 2								
SIGN NUMBER	FACE	EXISTING LEGEND	LETTERS BACKGROUND	PROPOSED LEGEND	LETTERS BACKGROUND	CIRCUIT	PAY ITEMS	REMARK
K-10	W	← N K K7 →	B/Y B/Y B/Y	← N K K7 →	B/Y B/Y B/Y	T-K-2	AR125964 & AR801605	RELOCATE EXTG. 4 MODULE SIGN AND REPLACE 8 PANELS
	E		B/B B/B B/B		B/B B/B B/B			
K7-2	N		B/B B/B B/B		B/B B/B B/B	T-K-2	AR125964 & AR801605	RELOCATE EXTG. 3 MODULE SIGN AND REPLACE 6 PANELS
	S	← 5 • 9 • 13	B/Y B/Y B/Y	← 5 • 9 • 13	B/Y B/Y B/Y			
K7-3	N		B/B B/B B/B		B/B B/B B/B	T-K-2	AR125964 & AR801605	RELOCATE EXTG. 4 MODULE SIGN AND REPLACE 8 PANELS
	S	31 • 27 • 23 →	B/Y B/Y B/Y	31 • 27 • 23 →	B/Y B/Y B/Y			
P-1	N			P	Y/B B/B B/B	T-P	AR125447	PROPOSED NEW 7 CHARACTER SIGN
	S			P 10- 28	Y/B W/R W/R			
P-2	N			P ← K →	Y/B B/Y B/Y	T-P	AR125444	PROPOSED NEW 4 CHARACTER SIGN
	S				B/B B/B B/B			
P-3	W			← P K	B/Y Y/B B/B	T-P	AR125443	PROPOSED NEW 3 CHARACTER SIGN
	E				B/B B/B B/B			
P-4	W				B/B B/B B/B	T-K-2	AR125443	PROPOSED NEW 3 CHARACTER SIGN
	E			K P →	Y/B B/B B/Y			
10-4	W			P →	B/Y Y/B	T-P	AR125444	PROPOSED NEW 4 CHARACTER SIGN
	E			28	Y/B B/B			
10-5	W			← 28	B/Y B/Y B/Y	T-K-2	AR125443	PROPOSED NEW 3 CHARACTER SIGN
	E				B/B B/B B/B			
10-6	W				B/B B/B B/B	T-K-2	AR125443	PROPOSED NEW 3 CHARACTER SIGN
	E			28 →	B/Y B/Y B/Y			

PROPOSED TEMPORARY RUNWAY 10-28 GUIDANCE SIGN SCHEDULE SIZE 3, STYLE 3 (WITH LED LIGHTING SYSTEM INCLUDING LED LAMPS), CLASS 2								
SIGN NUMBER	FACE	EXISTING LEGEND	LETTERS BACKGROUND	PROPOSED LEGEND	LETTERS BACKGROUND	CIRCUIT	PAY ITEMS	REMARK
28-1	W				B/B B/B B/B	R5-23	AR125444	PROPOSED NEW 4 CHARACTER SIGN
	E			5- 23	B/B W/R W/R			

NOTES:

- IF THE CONDITION OF THE EXISTING SIGN EQUIPMENT TO BE RELOCATED IS ACCEPTABLE TO THE ENGINEER, THE CONTRACTOR MAY REUSE THE EXISTING TRANSFORMERS, TRANSFORMER CANS, MOUNTING PLATES, AND SIGN FRAMES AT THE PROPOSED RELOCATED SIGN LOCATIONS. IF THE EXISTING SIGN EQUIPMENT IS REUSED, THEN THE CONTRACTOR SHALL SUPPLY AND INSTALL 10" CRHD. AGG. BEDDING, 10" CONCRETE PAD, CONDUITS, WIRING, SIGN PANELS, GASKETS, RETAP BASE HOLES AS REQD., NEW STAINLESS STEEL BOLTS, NUTS, & WASHERS, AND NEW LAMPS AS REQUIRED. CONTRACTOR SHALL REPLACE ANY DAMAGED EQUIPMENT AS DIRECTED BY THE ENGINEER. CONTRACTOR SHALL SUPPLY AND INSTALL NEW SIGN EQUIPMENT AS REQUIRED.

N = NORTH
S = SOUTH
E = EAST
W = WEST

NW = NORTHWEST
SE = SOUTHEAST
NE = NORTHEAST
SW = SOUTHWEST

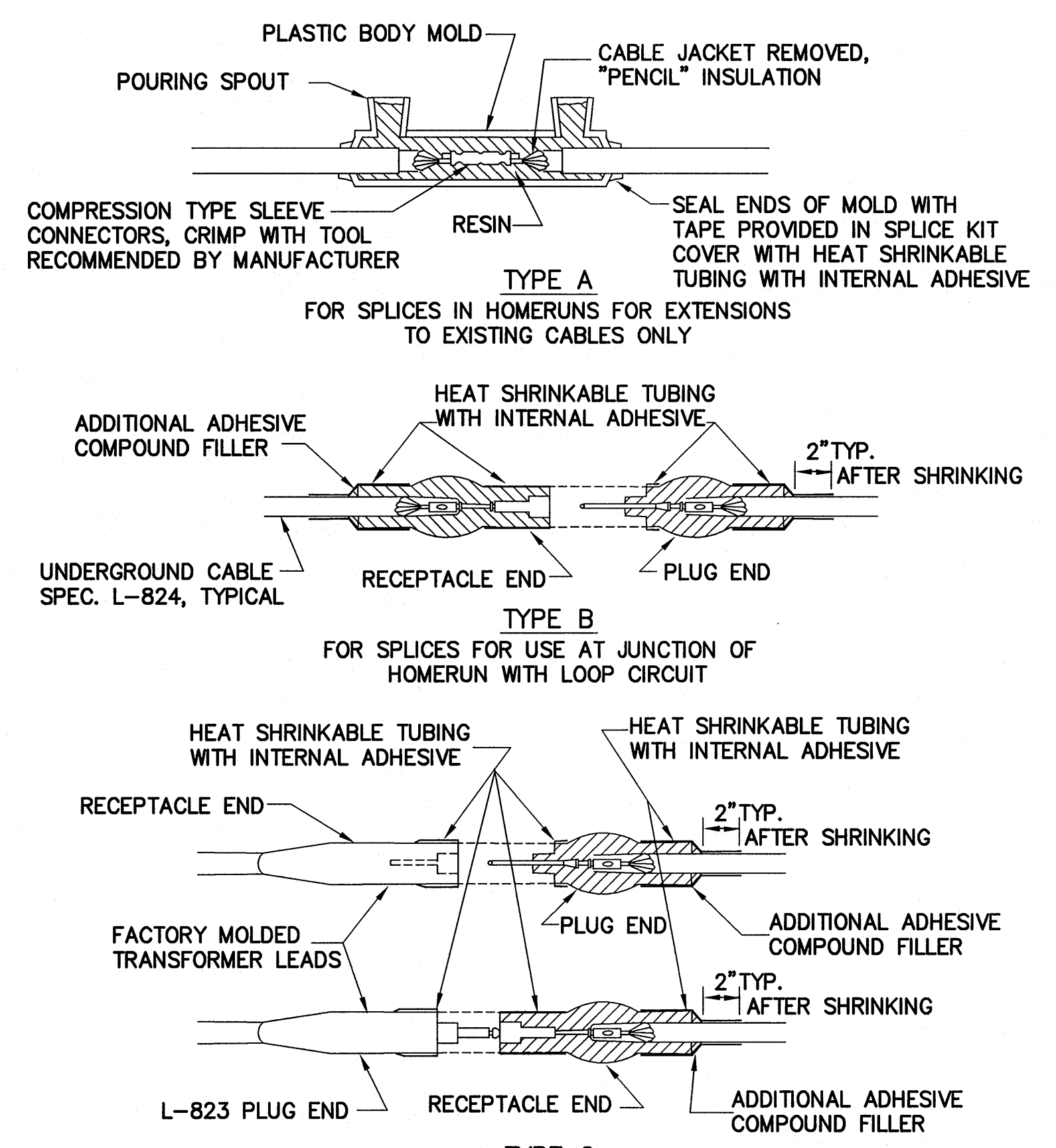
B/Y = BLACK LETTERS ON YELLOW BACKGROUND (TYPE L-858Y)
Y/B = YELLOW LETTERS ON BLACK BACKGROUND (TYPE L-858L)
B/B = BLACK BLANK PANEL
Y/Y = YELLOW BLANK PANEL
W/R = WHITE LETTERS ON RED BACKGROUND (TYPE L-858R)
W/B = WHITE LETTERS ON BLACK BACKGROUND (TYPE L-858B)

NUMBER OF MODULES	SIZE 3 TAXI GUIDANCE / SIZE 4 DIST. REMAIN. SIGNS ISOLATION TRANSFORMER DATA*			
	TRANSFORMER WATTAGE			
	STYLE 2, 4.8A-6.6A		STYLE 3, 2.8A-6.6A	
	TRADITIONAL	LED SYSTEM	TRADITIONAL	LED SYSTEM
1	100	200	200	200
2	300	200	300	300
3	500	300	500	500
4	500	300	500	500
DISTANCE REMAINING	300	200	300	300

TRANSFORMERS SHALL BE 6.6/6.6 AMP.
* = OR AS REQUIRED BY SIGN MANUFACTURER.
** = WITH A SIAMESE PIGTAIL ADAPTER AND TWO TRANSFORMERS.

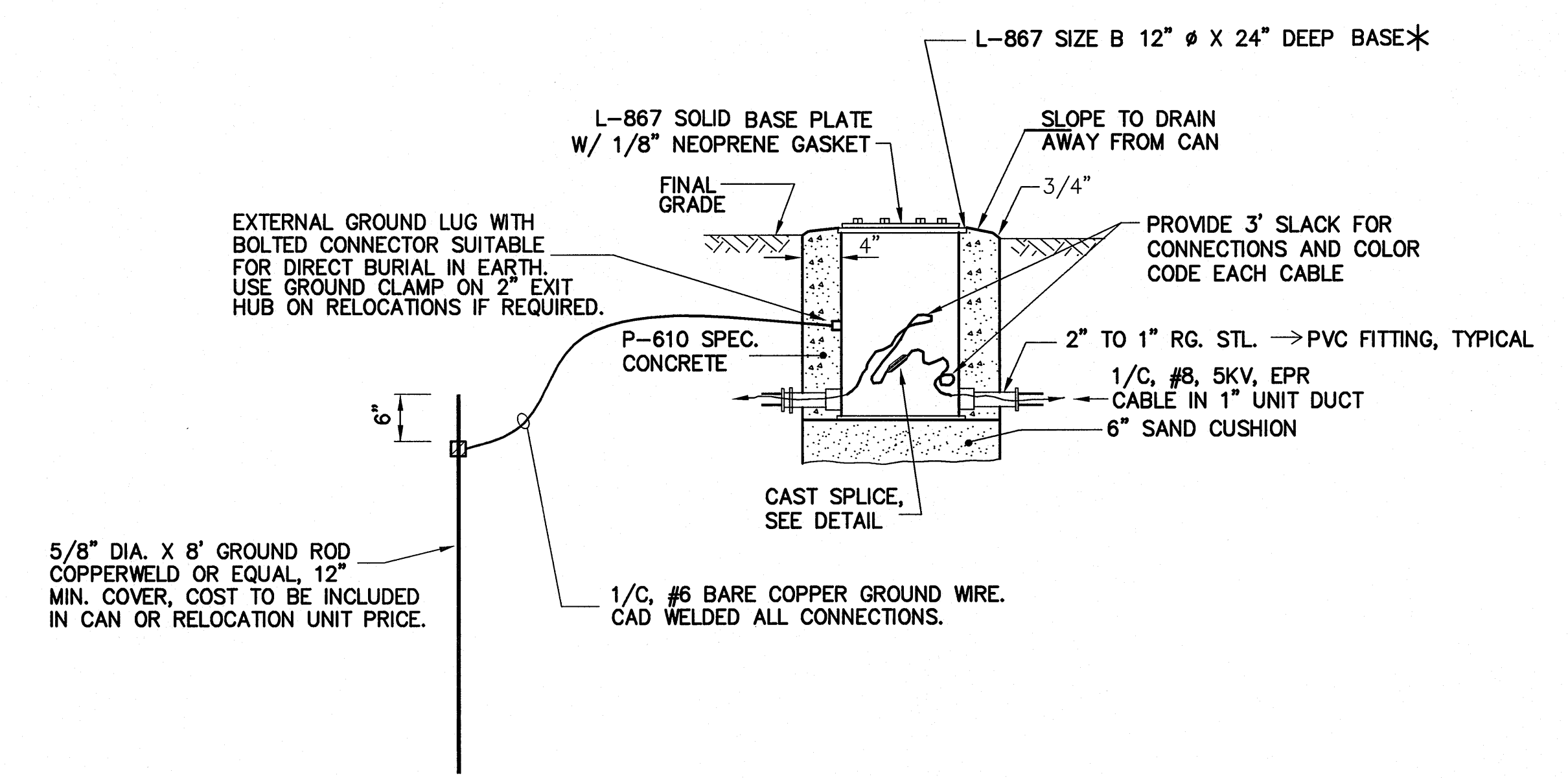
GENERAL ELECTRICAL NOTES:

- ALL EXCAVATION SHALL BE DONE VERY CAREFULLY. EXCAVATION BY HAND DIGGING SHALL BE REQUIRED AROUND ALL EXISTING DUCT BANKS, SPLICE CANS, MANHOLES, AND EXISTING CABLES. MANY EXISTING ACTIVE UNDERGROUND CABLES, WHOSE EXACT LOCATIONS CANNOT BE DETERMINED, ARE FOUND IN THE PROJECT AREA. IN ORDER TO AVOID EXISTING UNDERGROUND CABLES, THE CONTRACTOR SHALL CONNECT A THUMPER TO ALL EXISTING CIRCUITS AFTER WHICH THEY SHALL BE STAKED IN ALL AREAS REQUIRING TRENCHING OR EXCAVATION. CONTRACTOR SHALL ALSO NOTE THAT LOW VOLTAGE, FAA CABLES ALSO RUN UNDERGROUND THROUGHOUT THESE AREAS. ANY CABLE DAMAGED SHALL BE REPAIRED OR REPLACED TO ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
- A MINIMUM OF 3 FEET OF SLACK SHALL BE PROVIDED IN THE CABLES AT EACH TRANSFORMER, CONNECTOR, OR SPLICE POINT. ALL CABLE SPLICES SHALL OCCUR IN MANHOLES, LIGHT WELLS OR SPLICE CANS, UNLESS NOTED OTHERWISE.
- THE ELECTRICAL INSTALLATION, AS A MINIMUM, SHALL MEET THE NATIONAL ELECTRICAL CODE AND LOCAL REGULATIONS.
- ALL MANUFACTURERS FOR SUPPLYING AIRPORT LIGHTING EQUIPMENT SHALL APPEAR ON THE CURRENT FAA "APPROVED AIRPORT EQUIPMENT" LIST FOUND IN AC 150/5345-53C. THE EQUIPMENT SHALL COMPLY WITH THE APPLICABLE CURRENT FAA ADVISORY CIRCULAR LISTED IN THE FAA "APPROVED AIRPORT EQUIPMENT" LIST FOUND IN AC 150/5345-2 (AIRPORTS ELECTRONIC BULLETIN BOARD NUMBER 14).
- THE CONTRACTOR SHALL ASCERTAIN THAT ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY HIM (INCLUDING FAA APPROVED EQUIPMENT) ARE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND THE REMAINDER OF THE NEW/EXISTING SYSTEM. ANY NONCOMPATIBLE COMPONENTS FURNISHED BY THIS CONTRACTOR SHALL BE REPLACED BY HIM AT NO ADDITIONAL COST TO THE AIRPORT SPONSOR WITH A SIMILAR UNIT, APPROVED BY THE ENGINEER (DIFFERENT MODEL OR DIFFERENT MANUFACTURER) THAT IS COMPATIBLE WITH THE REMAINDER OF THE AIRPORT LIGHTING SYSTEM.
- IF THE CONTRACTOR SELECTS TO FURNISH AND INSTALL AIRPORT LIGHTING EQUIPMENT WHICH REQUIRES ADDITIONAL WIRING, TRANSFORMERS, ADAPTERS, MOUNTINGS, ETC., BEYOND THAT SHOWN ON THE DRAWINGS AND/OR LISTED IN THE SPECIFICATIONS, THEN THE COST FOR THE ADDITIONAL ITEMS SHALL BE INCLUDED TO THE CONTRACT UNIT PRICES.
- THE CONTRACTOR INSTALLED EQUIPMENT (INCLUDING FAA APPROVED) SHALL NOT GENERATE ANY ELECTROMAGNETIC INTERFERENCE IN THE EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AIR NAVIGATION, AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST WITH THE EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
- WHEN A SPECIFIC TYPE, STYLE, CLASS, ETC. OF FAA APPROVED EQUIPMENT IS SPECIFIED ONLY THAT TYPE, STYLE, CLASS, WILL BE ACCEPTABLE, EVEN THOUGH EQUIPMENT OF OTHER TYPES, STYLES, CLASSES, ETC. MAY BE FAA APPROVED.
- ALL CONCRETE FOR ELECTRICAL EQUIPMENT SHALL COMPLY WITH SPECIFICATION 610-STRUCTURAL PC CONCRETE 3500 PSI AT 28 DAYS, AIR ENTRAINED CONCRETE MIX SHALL BE USED.
- BASE MOUNTED BREAKABLE COUPLINGS SHALL NOT HAVE WEEP HOLES TO THE OUTSIDE. PLUGGED UP HOLES SHALL NOT BE ACCEPTABLE. IT SHALL HAVE A 1/4" DIAMETER OR EQUIVALENT OPENING FOR DRAINAGE FROM THE SPACE AROUND THE CONNECTOR INTO THE BASE.
- THE ELEVATION OF THE BREAKABLE COUPLING GROOVE SHALL NOT EXCEED 1 1/2" ABOVE THE EDGE OF THE COVER IN THE CASE OF A BASE MOUNTED COUPLING.
- ALL PERMANENT CABLE SPLICES SHALL OCCUR IN MANHOLES, LIGHT WELLS, OR SPLICE CANS, UNLESS NOTED OTHERWISE.
- MIMIC PANEL COLORS : RUNWAY R9-27 CIRCUIT 1 = WHITE, RUNWAY R5-23 CIRCUIT = WHITE, TAXIWAY D CIRCUIT = ORANGE, TAXIWAY E CIRCUIT = YELLOW, TAXIWAY F-2 CIRCUIT = LIME, SIGN CIRCUIT = WHITE AND TAXIWAY P = CYAN.

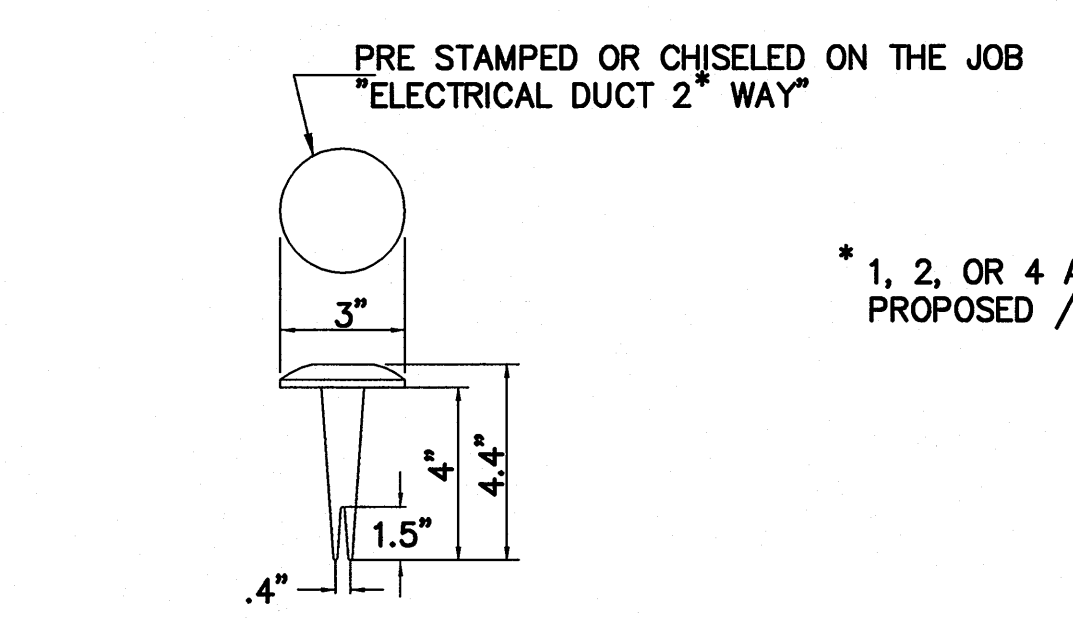


- NOTES :**
- SEE LIGHTING LAYOUT SHEET(S) FOR SPLICE TYPE
 - INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE

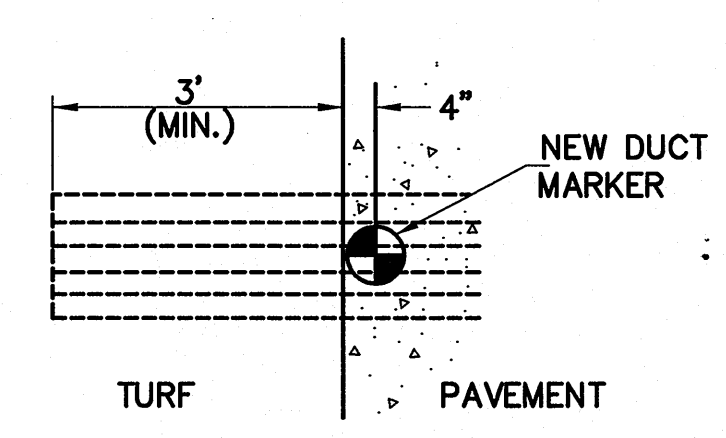
CABLE SPLICES
(NOT TO SCALE)



- NOTES :**
- * ORDER L-867 BASE WITH ADDITIONAL 2" CONDUIT HUB APPROXIMATELY 90" FROM MAIN ENTRANCE HUB WHERE SHOWN ON PLAN SHEETS.
 - SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER THIS DETAIL. INCLUDE GROUNDING ROD & WIRE COSTS IN RELOCATION UNIT PRICE.

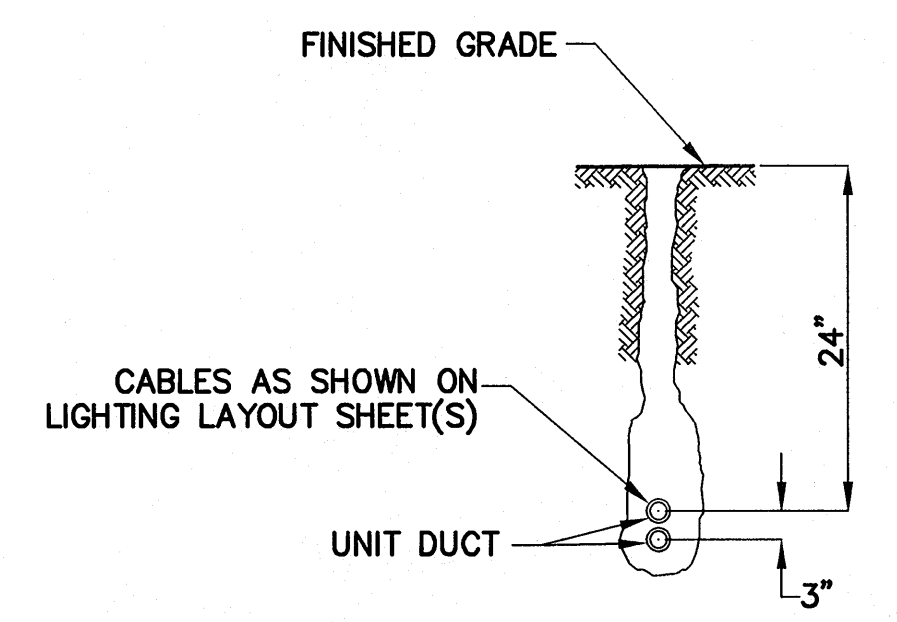


* 1, 2, OR 4 AS APPROPRIATE FOR PROPOSED / EXISTING DUCT BANK

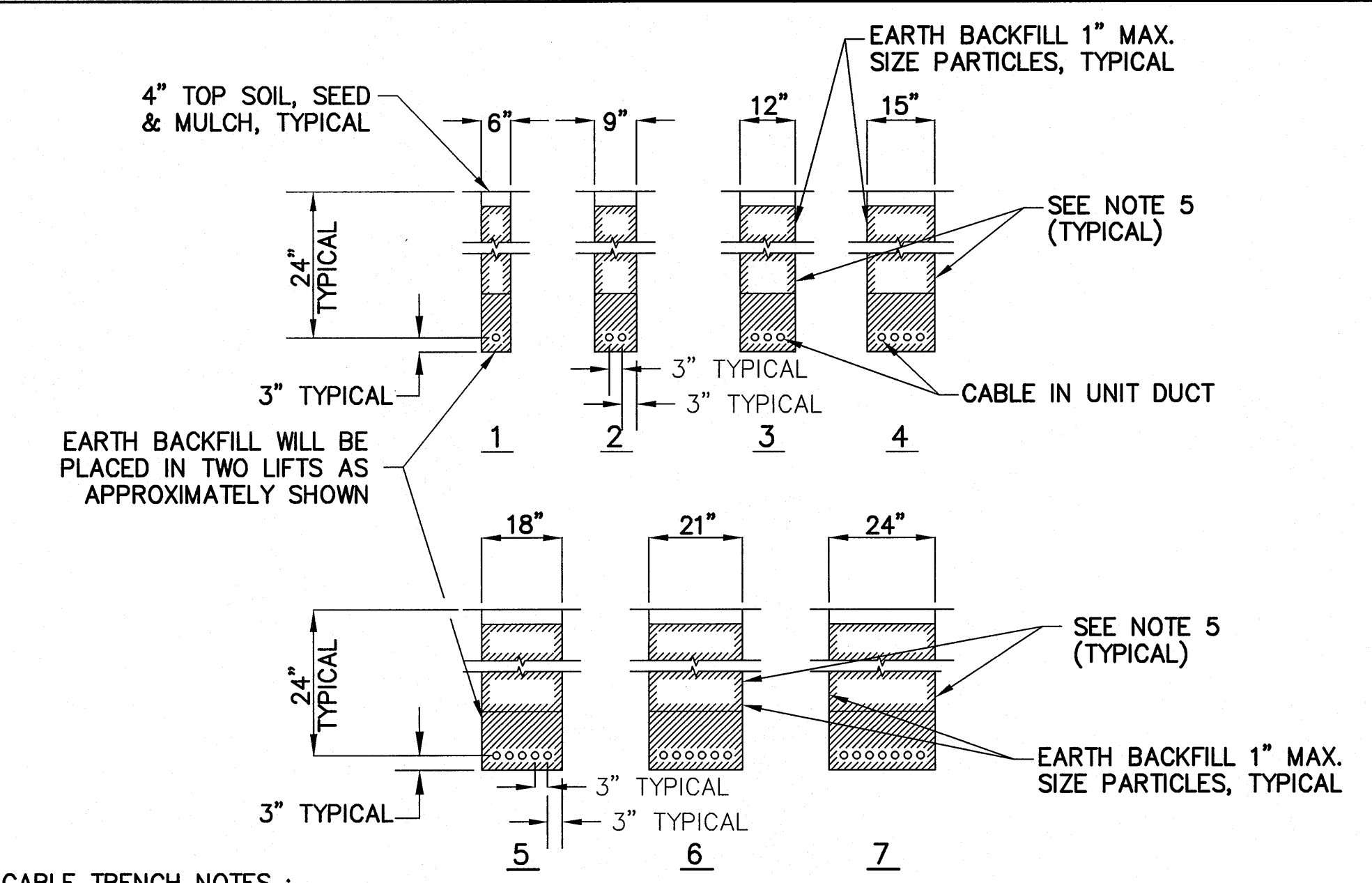


IN-PAVEMENT BRASS DUCT MARKER DETAIL

NOTE: PAVING CONTRACTOR SHALL INSTALL NEW BRASS DUCT MARKERS IN THE PROPOSED PAVEMENT AT ALL LOCATIONS WHERE THE PROPOSED PAVEMENT CROSSES EXISTING OR PROPOSED ELECTRICAL DUCTS. COST OF DUCT MARKERS SHALL BE INCLUDED IN THE 401 AND / OR 501 CONTRACT UNIT PRICES.

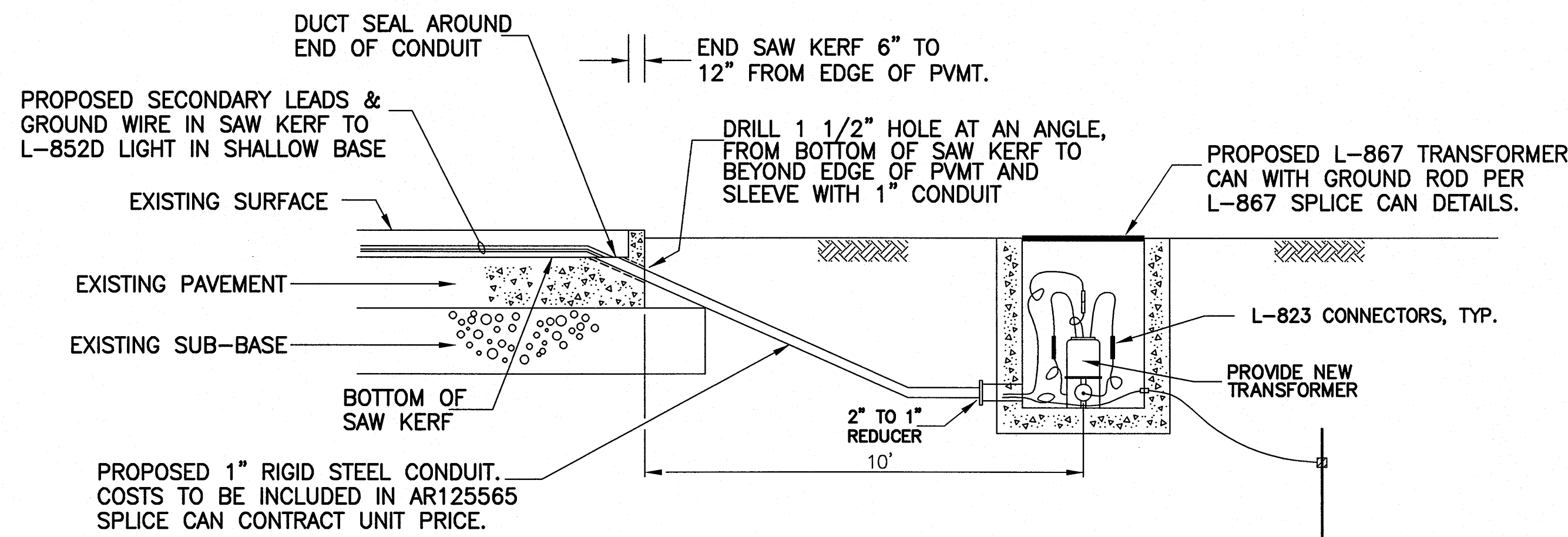


PLOWED CABLE
(NOT TO SCALE)

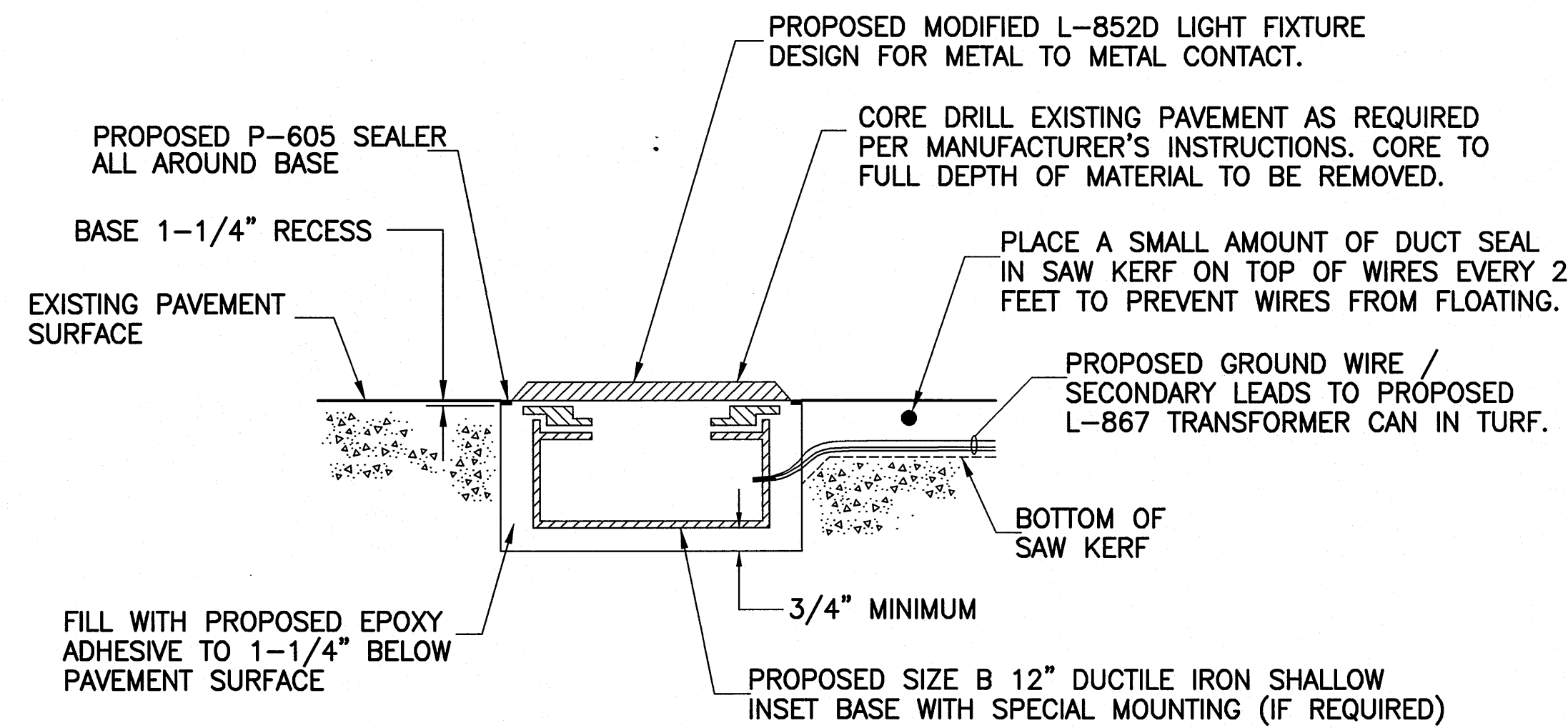


- CABLE TRENCH NOTES :**
- DETAIL NUMBERS INDICATE NO. OF CABLES.
 - TRENCHES WITH MORE THAN 7 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE; IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
 - DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
 - ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH. RETURNING MATERIALS AND RATES MAY BE SHOWN ON THE PLANS.
 - INSTALL YELLOW PLASTIC WARNING RIBBON IN TRENCH 9" ABOVE CABLES (TYPICAL ALL TRENCHES).

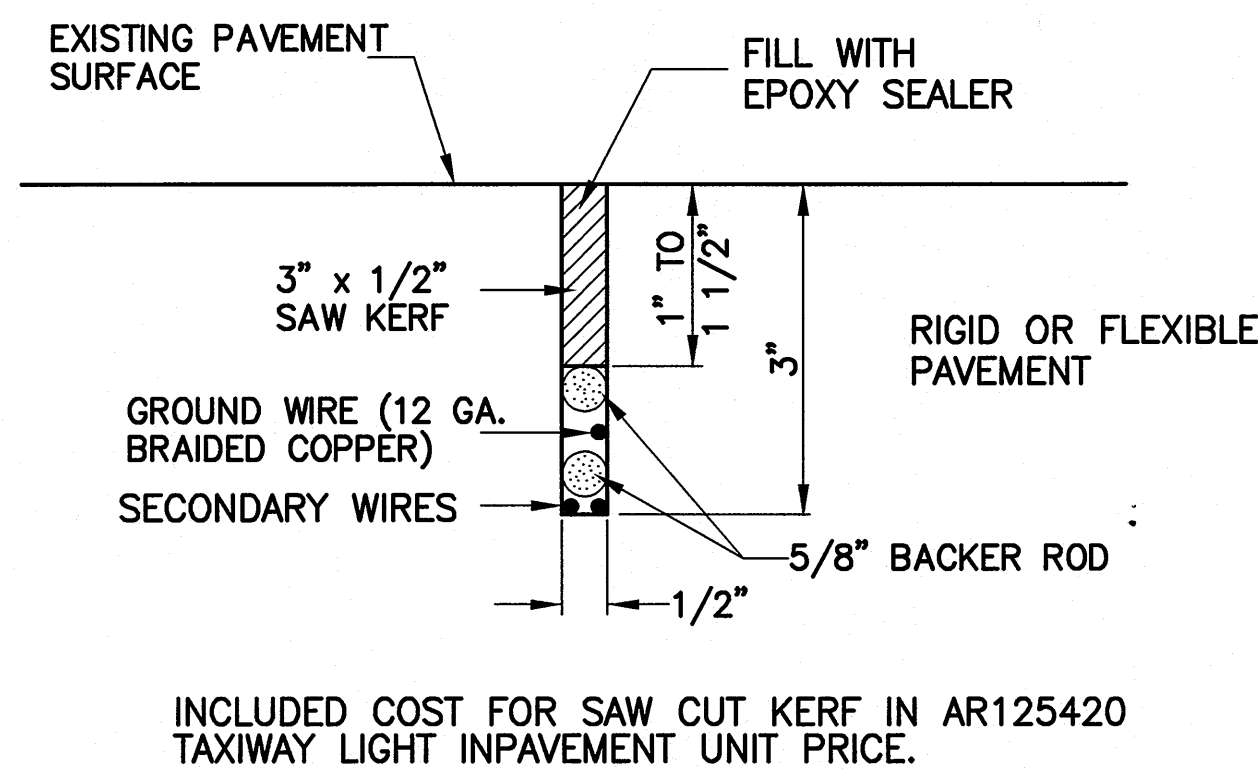
CABLE TRENCHES
(NOT TO SCALE)



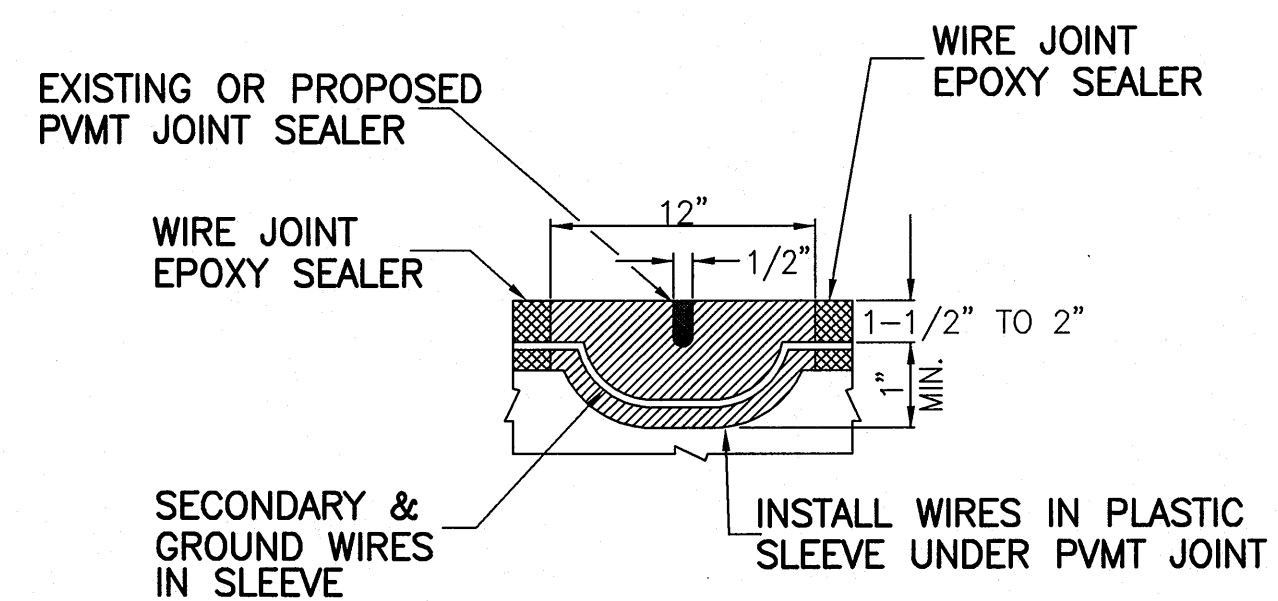
PROPOSED SECONDARY WIRING DETAIL FOR
L-852D LIGHT IN SHALLOW INSET BASE
STA. 638+30.8 ±, RT. R10-28



TYPICAL INSTALLATION OF L-852D IN PAVEMENT TAXIWAY
CENTERLINE LIGHT (MODIFIED FOR RUNWAY) ON 12" DUCTILE IRON
SHALLOW INSET BASE, STA. 638+54.5, RT. 60', R10-28



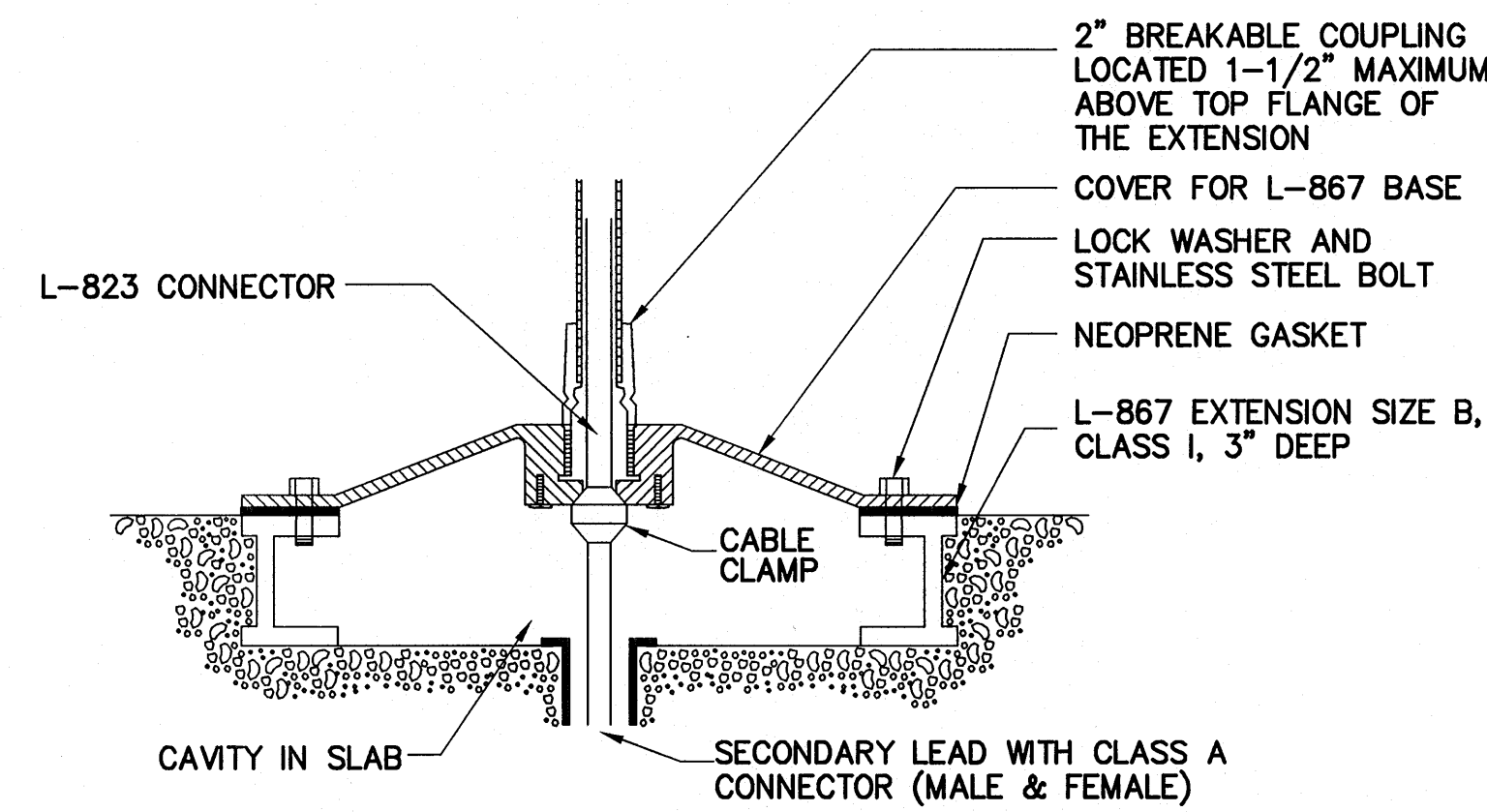
SAW KERF CROSS SECTION



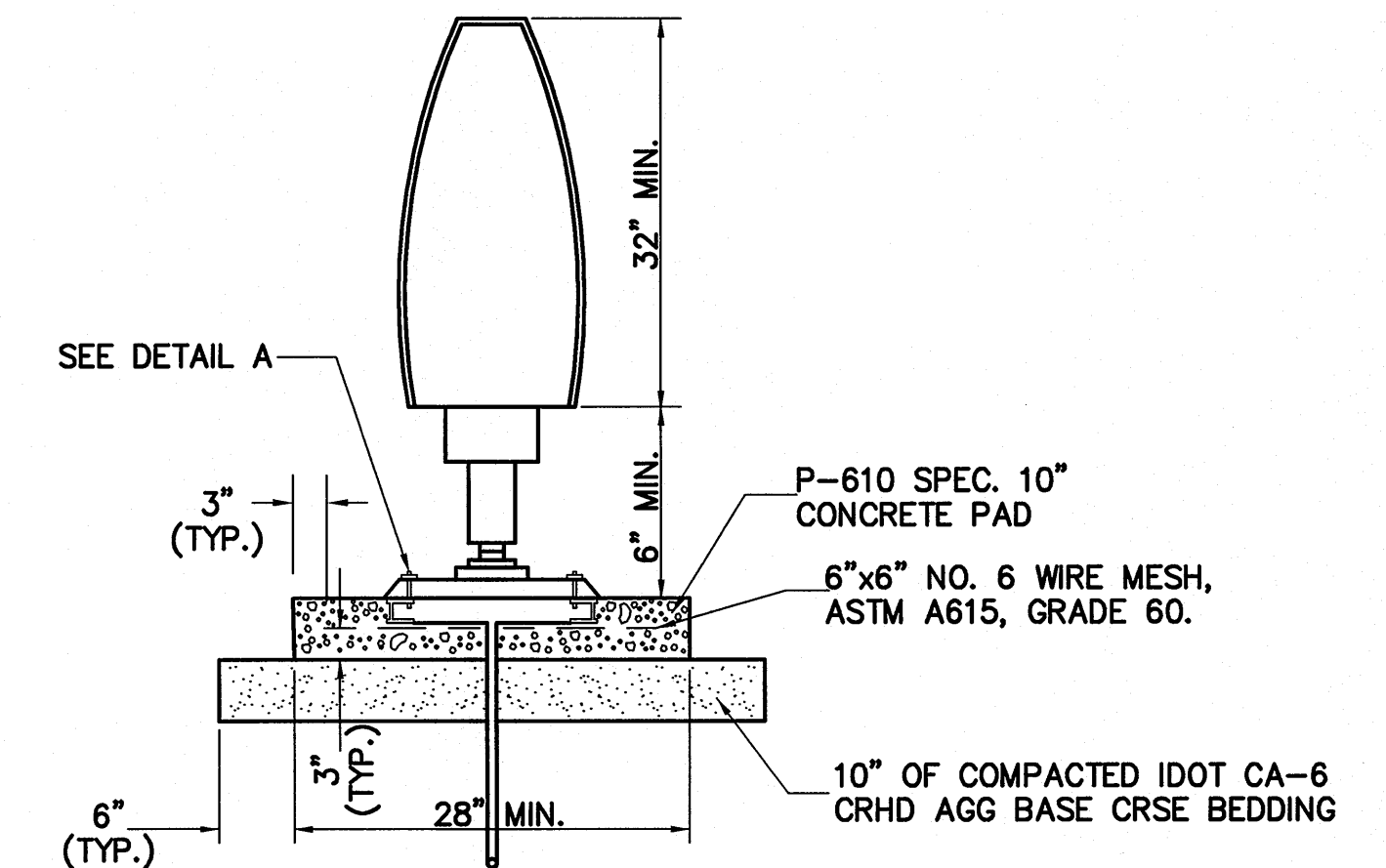
SAW KERF AT PAVEMENT JOINT INTERSECTION

NUMBER OF MODULES	ISOLATION TRANSFORMER DATA*			
	STYLE 2, 4.8A-6.6A		STYLE 3, 2.8A-6.6A	
	TRADITIONAL	LED SYSTEM	TRADITIONAL	LED SYSTEM
1	100	200	200	200
2	300	200	300	300
3	500	300	500	500
4	500	300	500	500
DISTANCE REMAINING	300	200	300	300

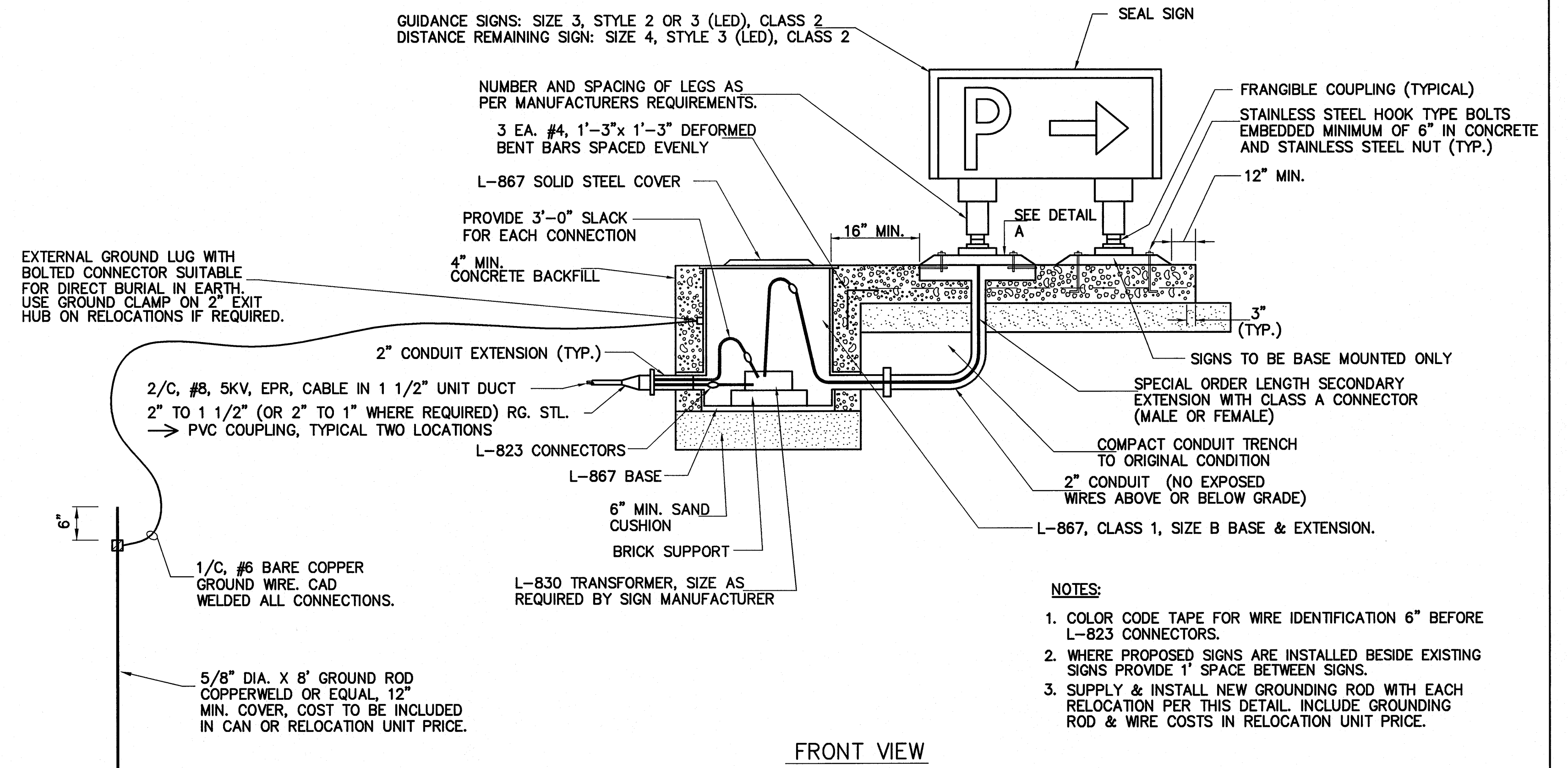
TRANSFORMERS SHALL BE 6.6/6.6 AMP.
* = OR AS REQUIRED BY SIGN MANUFACTURER.
** = WITH A SIAMESE PIGTAIL ADAPTER AND TWO TRANSFORMERS.



DETAIL A



SIDE VIEW

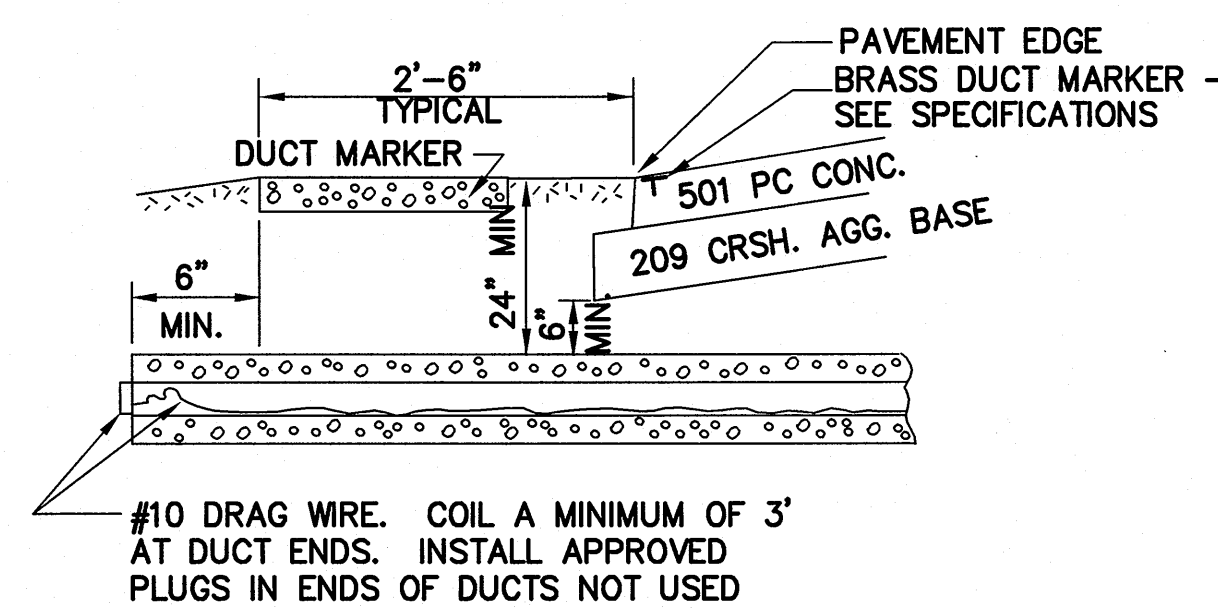
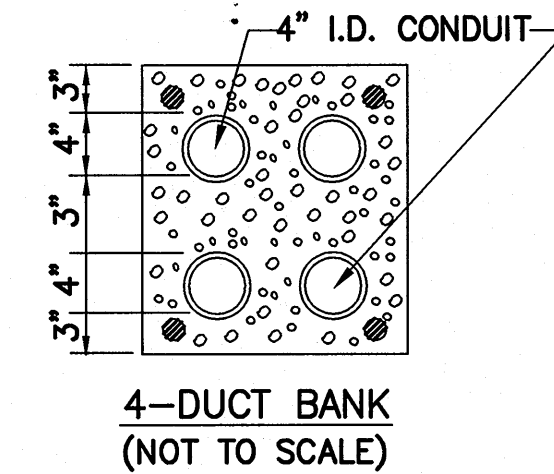
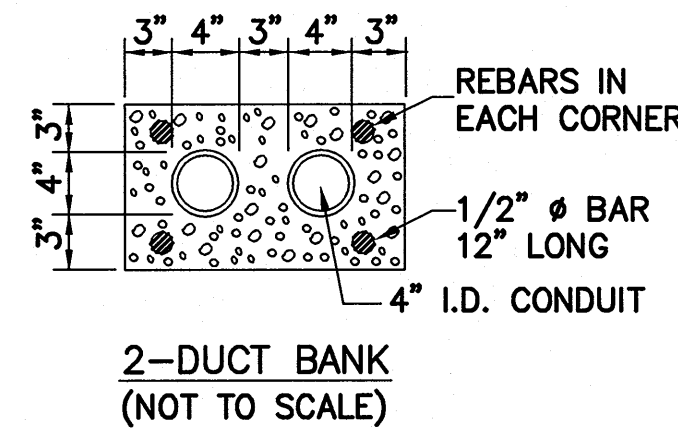
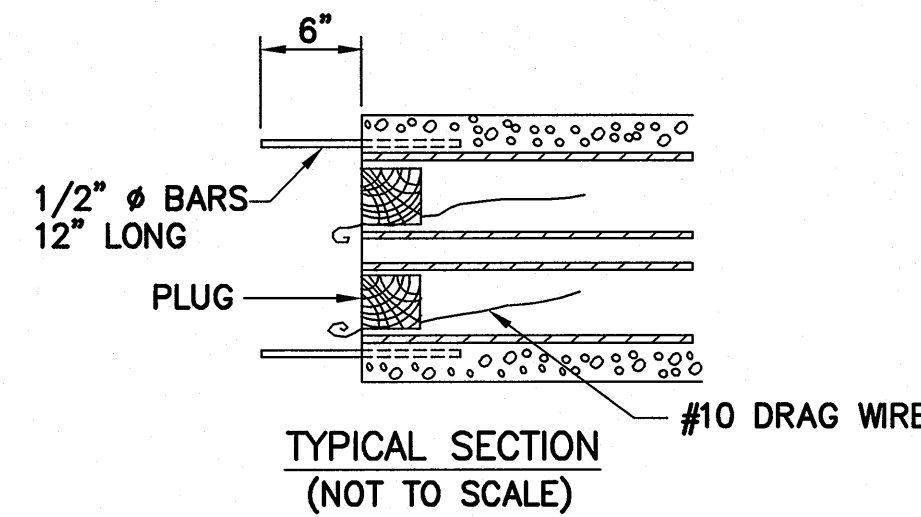


FRONT VIEW

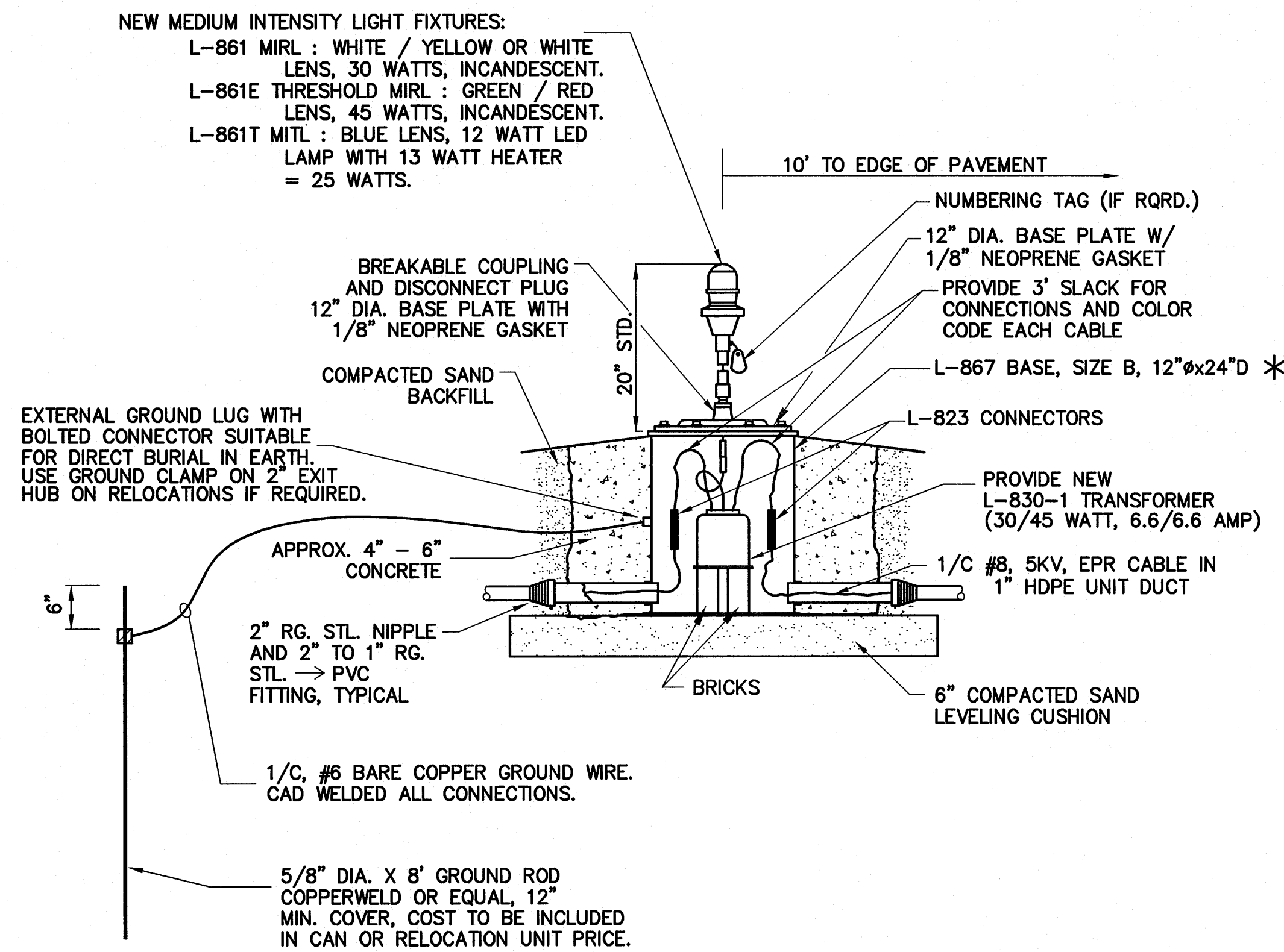
NOTES:

- COLOR CODE TAPE FOR WIRE IDENTIFICATION 6" BEFORE L-823 CONNECTORS.
- WHERE PROPOSED SIGNS ARE INSTALLED BESIDE EXISTING SIGNS PROVIDE 1' SPACE BETWEEN SIGNS.
- SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER THIS DETAIL. INCLUDE GROUNDING ROD & WIRE COSTS IN RELOCATION UNIT PRICE.

- NOTES :
- DIMENSIONS SHOWN ARE MINIMUM.
 - TOP OF CONCRETE ENCASEMENT TO BE NOT LESS THAN 24" BELOW FINISHED SUBGRADE.
 - DUCT CONCRETE SHALL BE 610 STRUCTURAL P.C. CONC.
 - PLASTIC DUCT (PVC) SHALL BE TYPE 1 CONFORMING TO FEDERAL SPEC. W-C-1094.
 - ALL DUCT SHALL BE 4" INSIDE DIA.
 - WHERE EDGE DRAINS ARE USED, THE LENGTH OF THE DUCT SHALL BE SUCH THAT THE ENDS OF THE DUCTS WILL NOT BE LESS THAN TWO FEET FROM THE OUTSIDE EDGE OF ANY POROUS GRANULAR BACKFILL MATERIAL.
 - WHERE EDGE DRAINS ARE NOT USED, THE LENGTH OF THE DUCT SHALL BE SUCH THAT THE ENDS OF THE DUCTS WILL NOT BE LESS THAN THREE FEET FROM THE EDGE OF ANY PAVED SURFACE.
 - CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE HOMERUN (TURF CABLE MARKER)
 - USE SPLIT DUCT IN DUCT BANKS AS REQUIRED WHERE EXISTING CABLES ARE PRESENT. COST OF SPLIT DUCT TO BE INCLUDED IN THE UNIT PRICE FOR DUCT BANK.



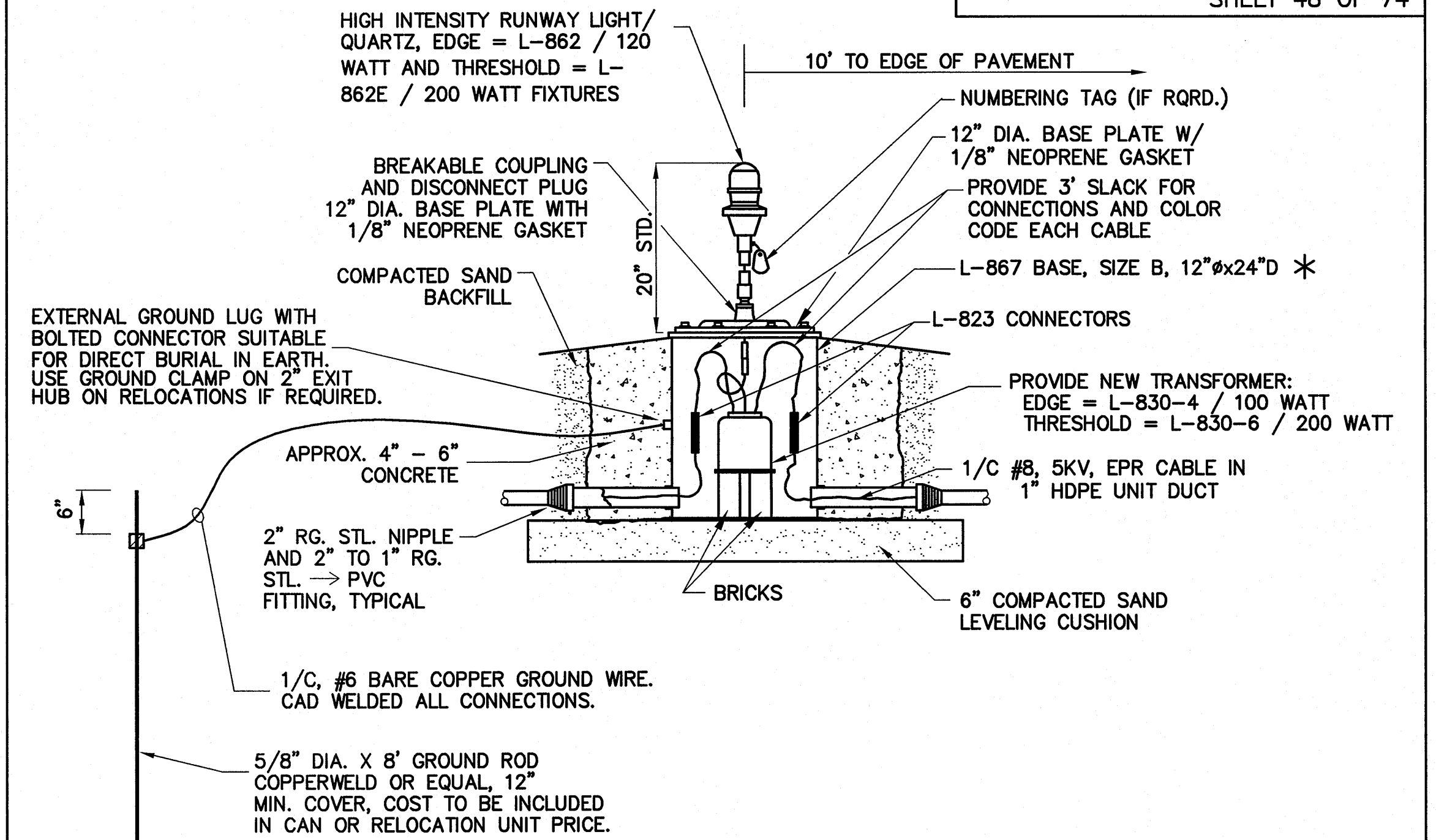
UNDERGROUND ELECTRICAL DUCT
(NOT TO SCALE)



L-861 EDGE LIGHT INSTALLATION / RELOCATION DETAILS
MEDIUM INTENSITY RUNWAY LIGHT (MIRL)
MEDIUM INTENSITY TAXIWAY LIGHT (MITL)

BASE MOUNTED, 6.6 AMP SERIES CIRCUIT
NOT TO SCALE

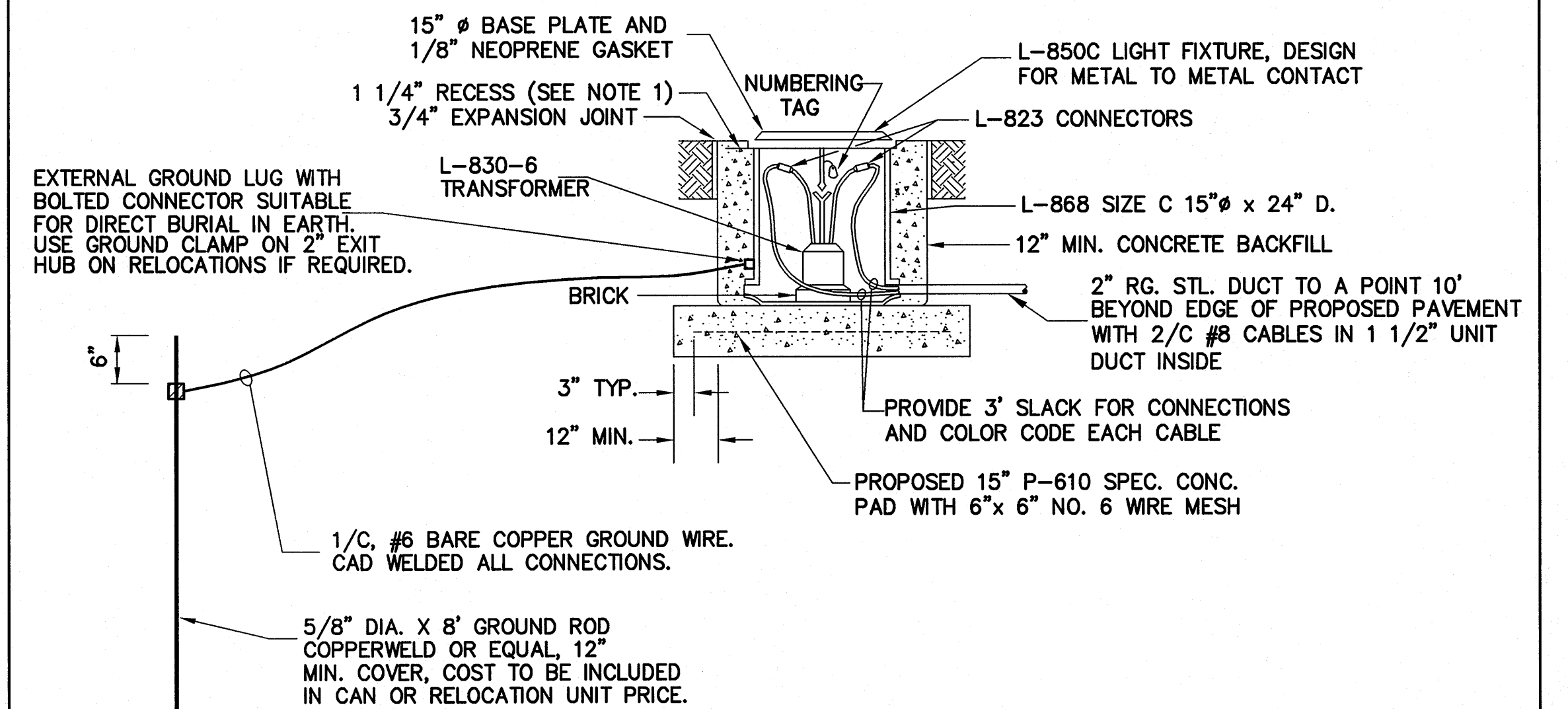
- NOTES :
- BREAKING GROOVE OF BREAKABLE COUPLING SHALL BE 3" TO 3 1/2" ABOVE FINISHED GRADE.
 - * FOR ALL NEW EDGE LIGHTS: ORDER L-867 BASE WITH ADDITIONAL 2" CONDUIT HUB APPROXIMATELY 90' FROM MAIN ENTRANCE HUB.
 - IF THE CONDITION OF THE EXISTING LIGHTING EQUIPMENT TO BE REMOVED IS ACCEPTABLE TO THE RESIDENT ENGINEER, THE CONTRACTOR MAY REUSE THE REMOVED LIGHT FIXTURES, BASE PLATES, CANS, AND TRANSFORMERS AT THE PROPOSED LIGHT LOCATIONS. IF THE EXISTING EDGE LIGHT EQUIPMENT IS REUSED, THEN THE CONTRACTOR SHALL SUPPLY AND INSTALL 6" COMPACTED SAND LEVELING CUSHION, EXTEND THE EXISTING STEEL CONDUIT TO A POINT OUTSIDE OF THE PROPOSED CONCRETE BACKFILL, ADDITIONAL P.C. CONCRETE BACKFILL ALL AROUND (MIN. 4" THICKNESS), COMPACTED SAND BACKFILL, NEW 1/8" NEOPRENE GASKETS, RETAP BASE HOLES AS REQD., NEW STAINLESS STEEL BOLTS, NUTS, & WASHERS, AND NEW LENS & LAMPS AS REQUIRED. CONTRACTOR SHALL REPLACE ANY DAMAGED EQUIPMENT AS DIRECTED BY THE RESIDENT ENGINEER. CONTRACTOR SHALL SUPPLY AND INSTALL NEW EDGE LIGHT EQUIPMENT AS REQUIRED.
 - SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER THIS DETAIL. INCLUDE GROUNDING ROD & WIRE COSTS IN RELOCATION UNIT PRICE.



L-862 EDGE LIGHT INSTALLATION / RELOCATION DETAILS
HIGH INTENSITY RUNWAY LIGHT / QUARTZ (HIRLQ)

BASE MOUNTED, SERIES CIRCUIT
NOT TO SCALE

NOTE : SEE NOTES UNDER L-861T TAXIWAY EDGE LIGHT DETAIL. THESE NOTES ALSO APPLIES TO TYPE L-862 RUNWAY EDGE LIGHTS. SUPPLY 3-WAY BASE CANS ON ALL NEW INSTALLATIONS.

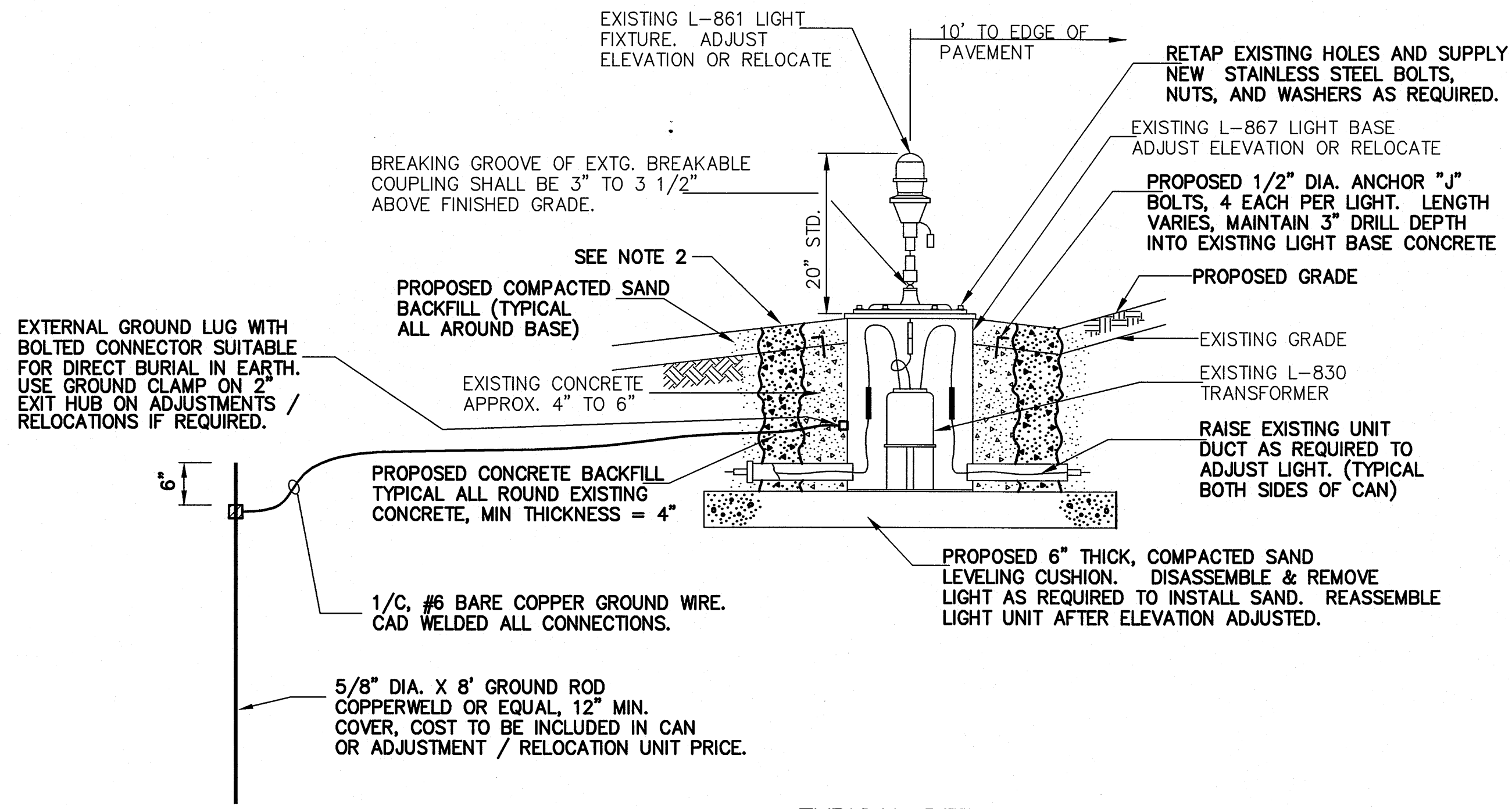


NOTES :

- IF INSTALLATION IS IN BITUMINOUS PAVEMENT, LEAVE CONCRETE BACKFILL 3-4 INCHES LOW FOR BACKFILL WITH BITUMINOUS MATERIAL.
- SEE NOTES 3 AND 4 UNDER L-861T TAXIWAY EDGE LIGHT DETAIL. THESE NOTES ALSO APPLIES TO TYPE L-850C RUNWAY EDGE LIGHTS.

TYPICAL INSTALLATION OF L-850C HIRLQ
EDGE LIGHT IN NEW PAVEMENT
ON L-868 BASE CAN

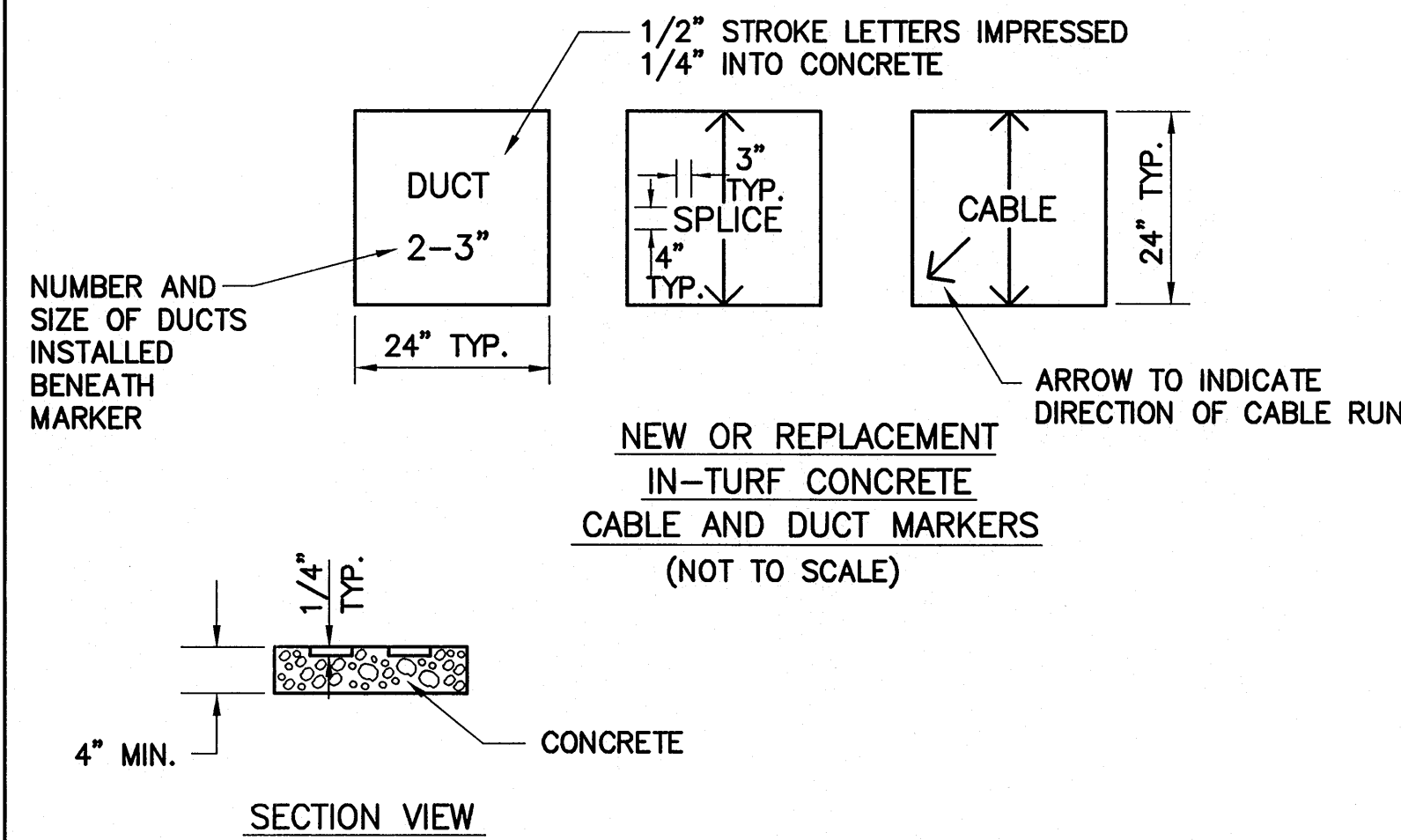
NOT TO SCALE



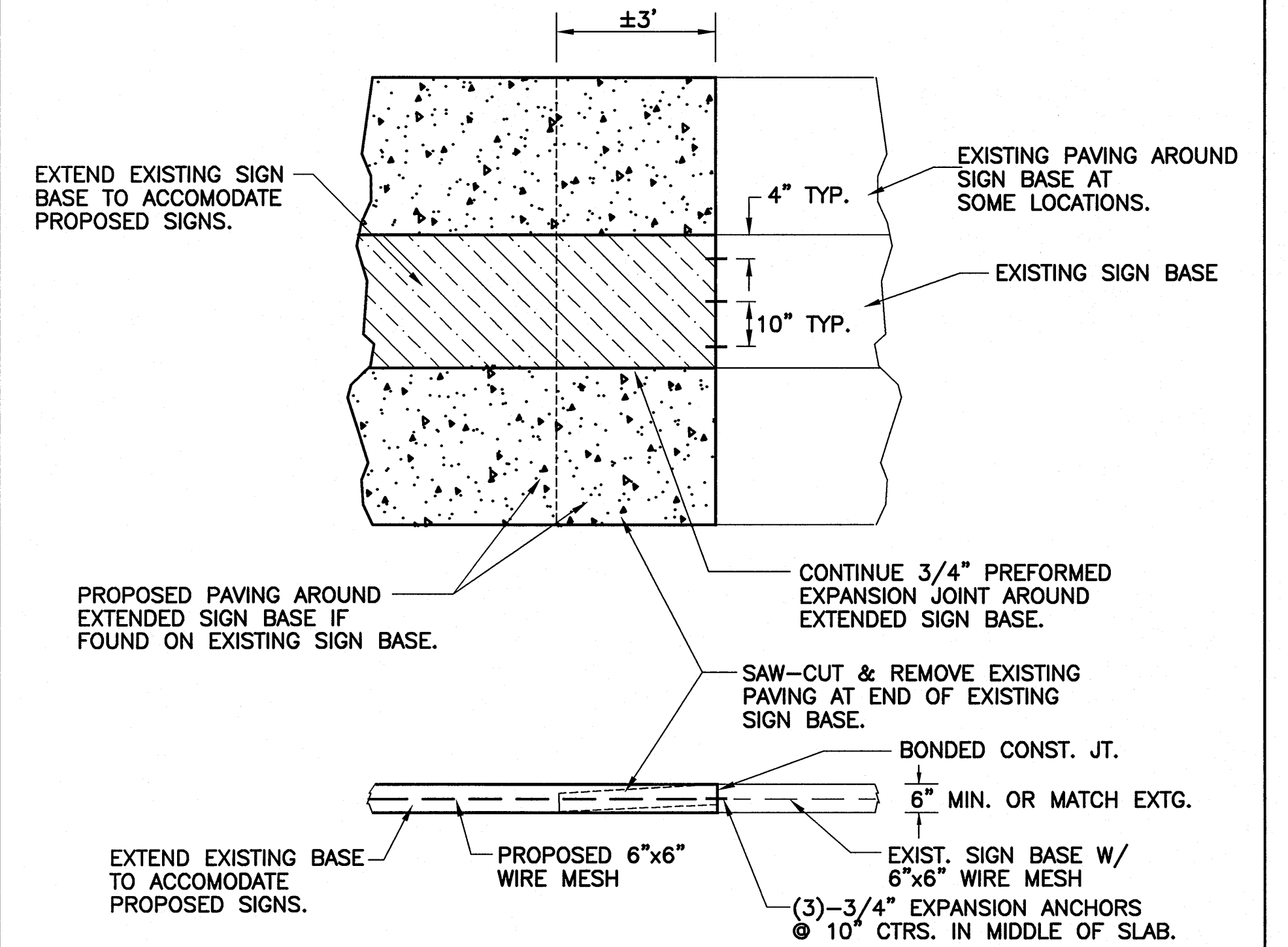
TYPICAL DETAIL L-861

EDGE LIGHTS ADJUSTMENT OR RELOCATION - IN TURF

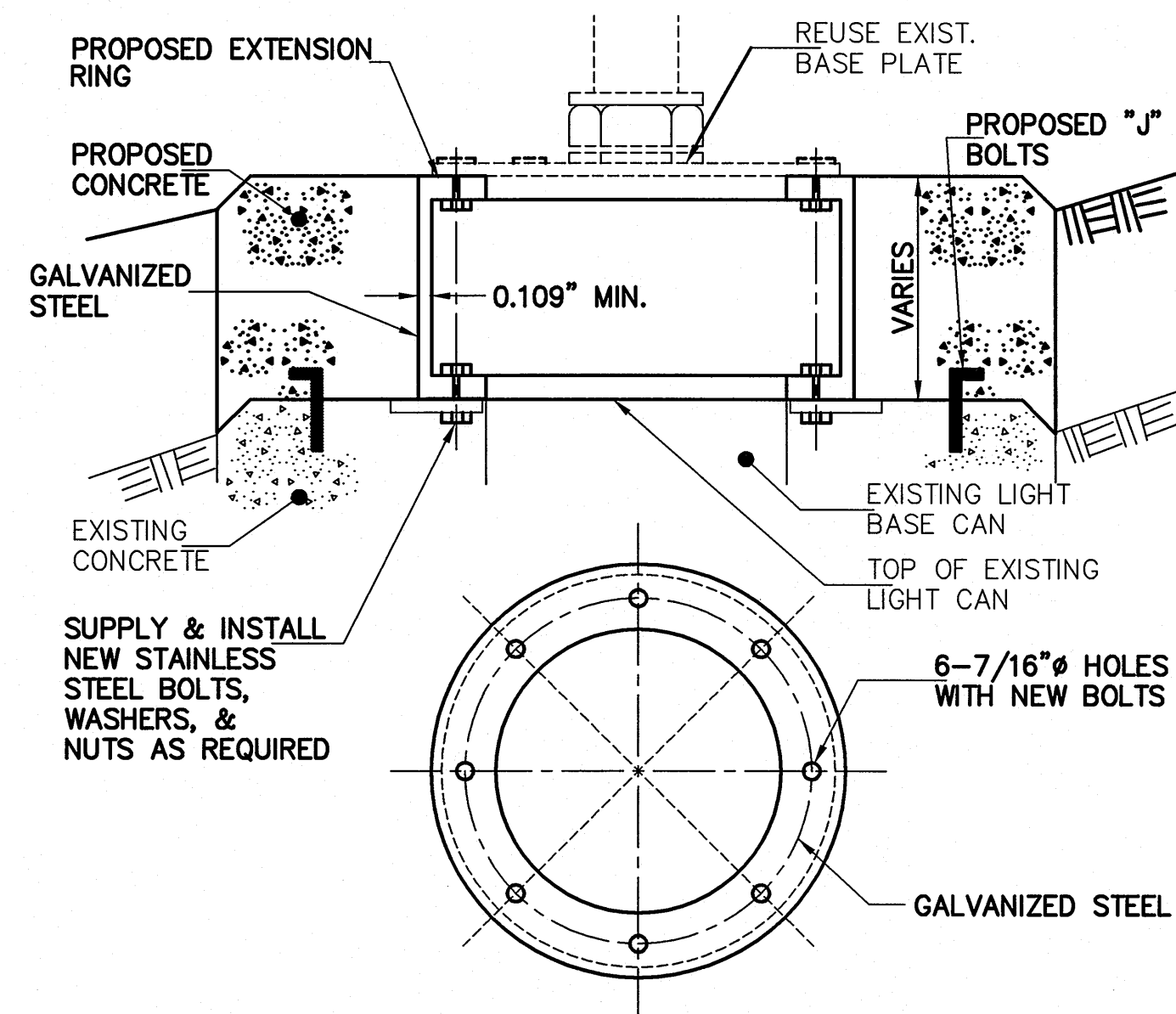
- NOTES:
1. AT THE CONTRACTOR'S OPTION, LIGHT BASE CAN EXTENSION RINGS MAY BE USED TO ADJUST LIGHT FIXTURE. CONTRACTOR TO EXTEND EXISTING CONCRETE APRON AROUND LIGHT BASE TO PROPOSED GRADE ELEVATION IF BASE CAN EXTENSION RINGS ARE USED. CONTRACTOR TO DETERMINE HEIGHT OF BASE CAN EXTENSION RINGS IN THE FIELD AT THE TIME OF CONSTRUCTION.
 2. GROUND SURFACE DRAINAGE TO FLOW AWAY FROM EDGE OF PAVEMENT AND LIGHT BASE CANS. NO WATER PONDING OR LOW SPOTS SHALL BE PERMITTED AROUND LIGHT BASE CANS.
 3. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAID FOR.
 4. SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH ADJUSTMENT / RELOCATION PER THIS DETAIL. INCLUDE GROUNDING ROD & WIRE COSTS IN ADJUSTMENT / RELOCATION UNIT PRICE.



- NOTES:
1. NEW MARKERS ARE REQUIRED ON ALL FAA CABLES. CABLE MARKERS SHALL BE INSTALLED BY THE CONTRACTOR AT ALL LOCATIONS SELECTED BY THE RESIDENT ENGINEER AND / OR THE FAA. THE CONTRACTOR SHALL BE REQUIRED TO ADJUST THE ELEVATION OF EXISTING MARKERS AND / OR REPLACE EXISTING MARKERS DAMAGED DURING CONSTRUCTION.
 2. COST OF CONCRETE MARKERS IS INCIDENTAL TO THE ASSOCIATED ITEMS OF DUCT OR CABLE.
 3. EDGE EXPOSED CONCRETE WITH A 1/4" RADIUS TOOL.
 4. WHERE ADDITIONAL SPACE TO FIT THE LEGEND IS REQUIRED, SOME OF THE FOLLOWING METHODS SHALL BE EMPLOYED.
 - A. REDUCE LETTER SIZE TO 3" HIGH, 2" WIDE.
 - B. INCREASE THE MARKER SIZE TO 30" X 30" MAX.
 - C. PROVIDE ADDITIONAL MARKERS PLACED SIDE BY SIDE.

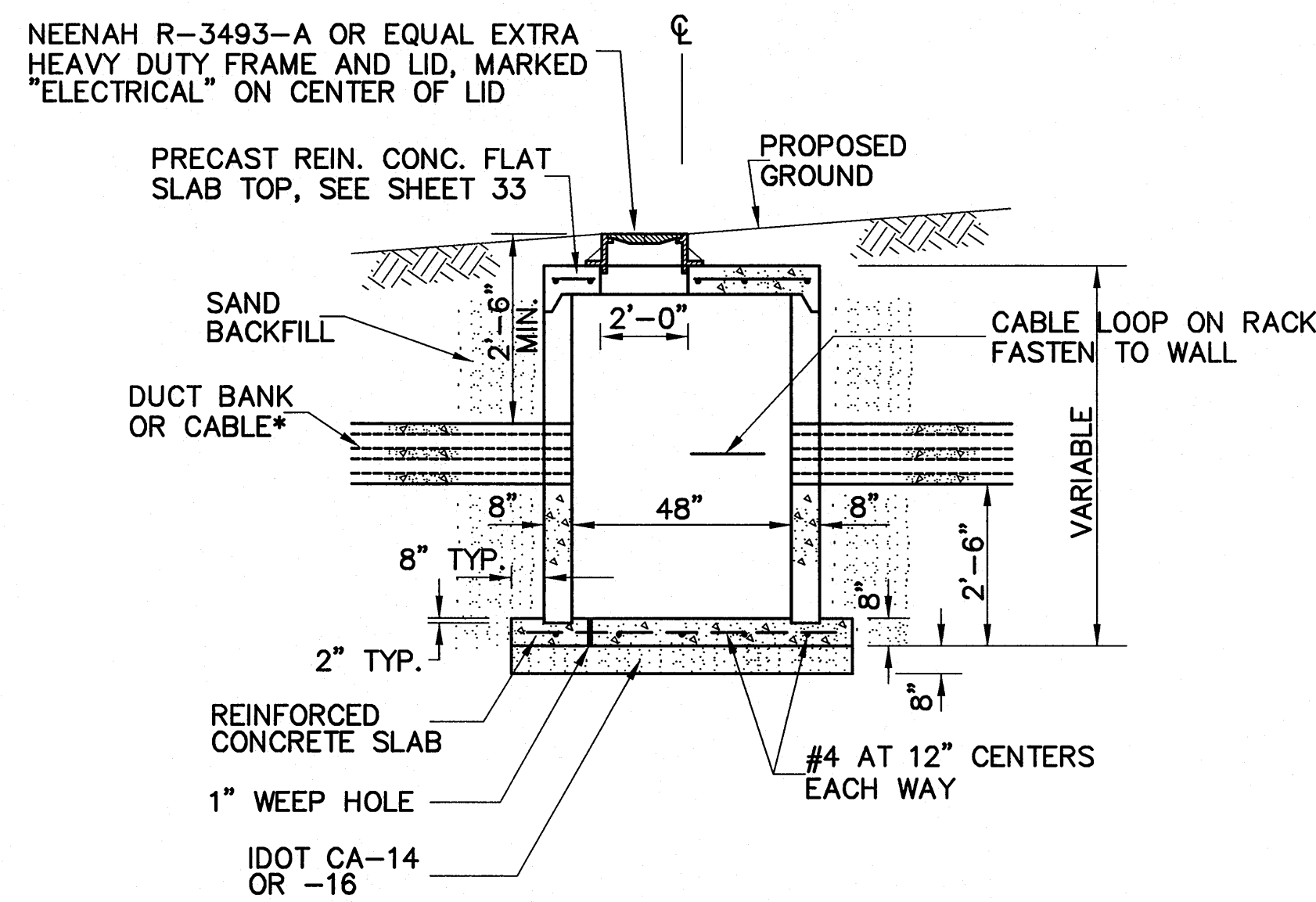


EXISTING SIGN BASE
EXTENSION DETAIL



NOTE: THE CONTRACTOR SHALL FIELD VERIFY EXISTING LIGHT BASE TYPES (MOST ARE 12" L-867) AND REQUIRED HEIGHT ADJUSTMENT DIMENSIONS. NO ADDITIONAL PAYMENT WILL BE MADE FOR ALTERNATE LIGHT CAN TYPES. REDRILL AND RETAP EXISTING HOLES AS REQUIRED. COSTS SHALL BE INCLUDED IN THE UNIT PRICE FOR LIGHT ADJUSTMENT.

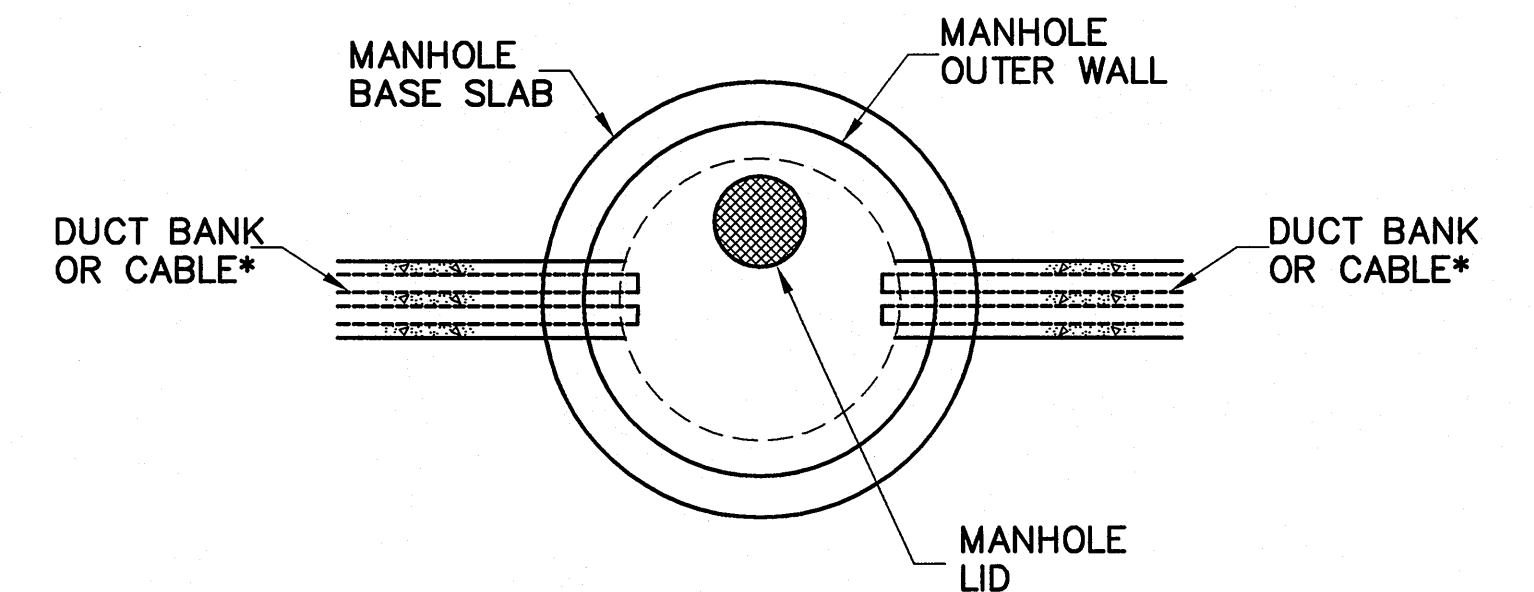
OPTIONAL EXTENSION RING DETAIL
N.T.S.



* = FOR CABLE: INSTALL 2" RG. STL. CONDUIT SLEEVES THROUGH MANHOLE WALL. INSTALL WATERPROOF BUSHINGS ON ENDS OF CONDUITS.

FURNISHING AND INSTALLING SAND CUSHION, CONCR. BASE SLAB, SAND BACKFILL, FRAME & LID, CABLE RACK AND FLAT SLAB TOP TO BE INCLUDED IN THE CONTRACT UNIT PRICE.

PROFILE VIEW

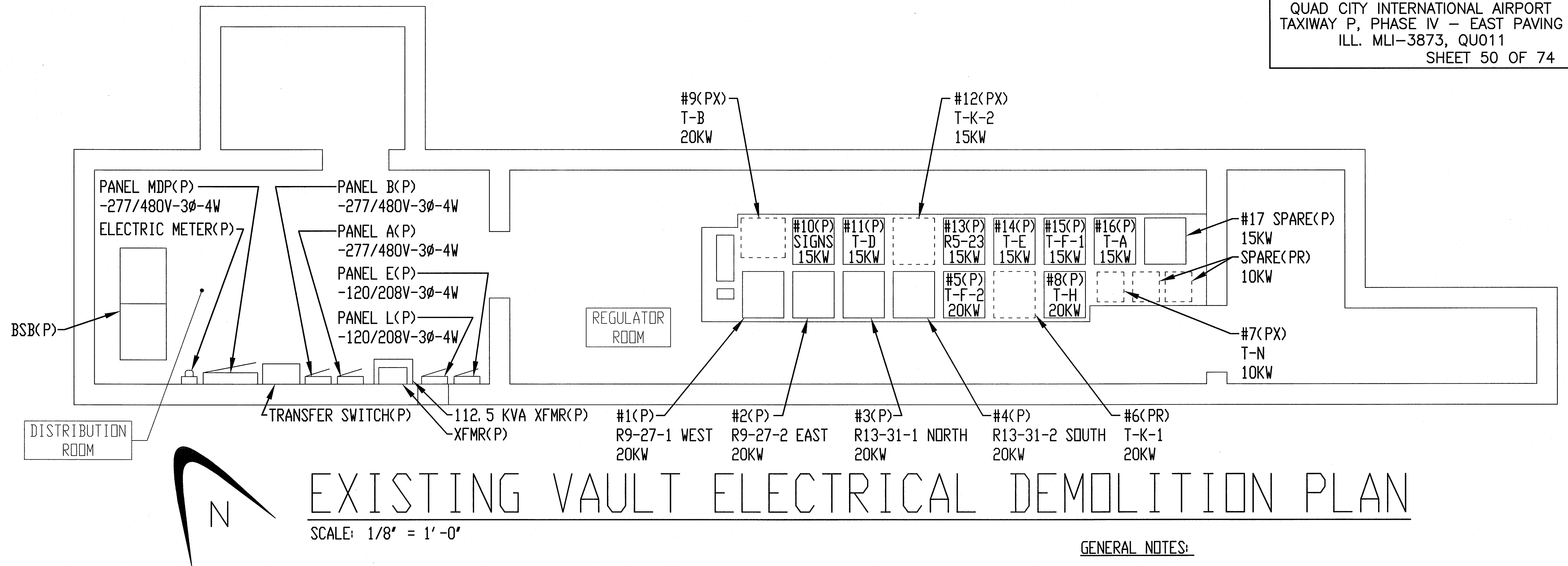


PLAN VIEW

DETAIL OF ELECTRICAL MANHOLE

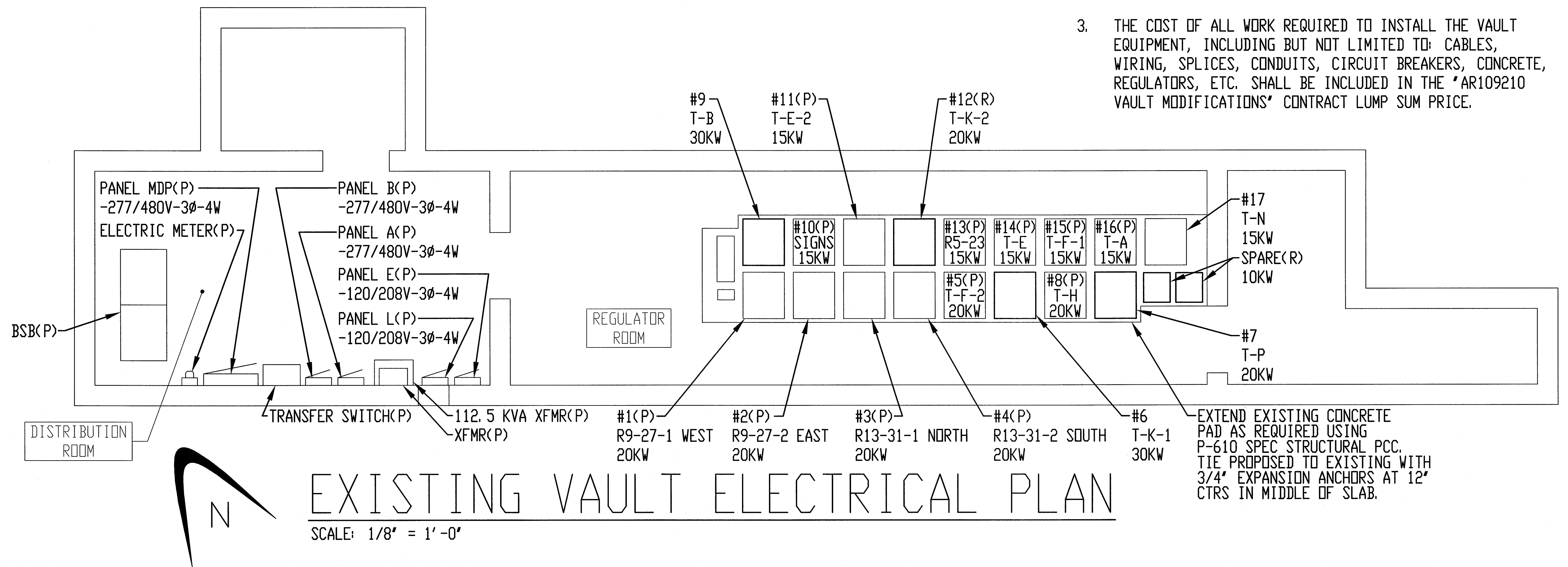
VAULT ELECTRICAL DEMOLITION PLAN NOTES:

1. REGULATOR #9 (20KW) SHALL BE REMOVED AND BECOME A SPARE.
2. REGULATOR #12 (15KW) SHALL BE REMOVED AND BECOME A SPARE.
3. REGULATOR #7 (10KW) SHALL BE REMOVED AND BECOME A SPARE.
4. REGULATOR #6 (20KW) SHALL BE REMOVED, RELOCATED, AND BECOME REGULATOR #12.
5. RELOCATE (SHIFT EAST) TWO SPARE REGULATORS (10KW).



VAULT ELECTRICAL PLAN NOTES:

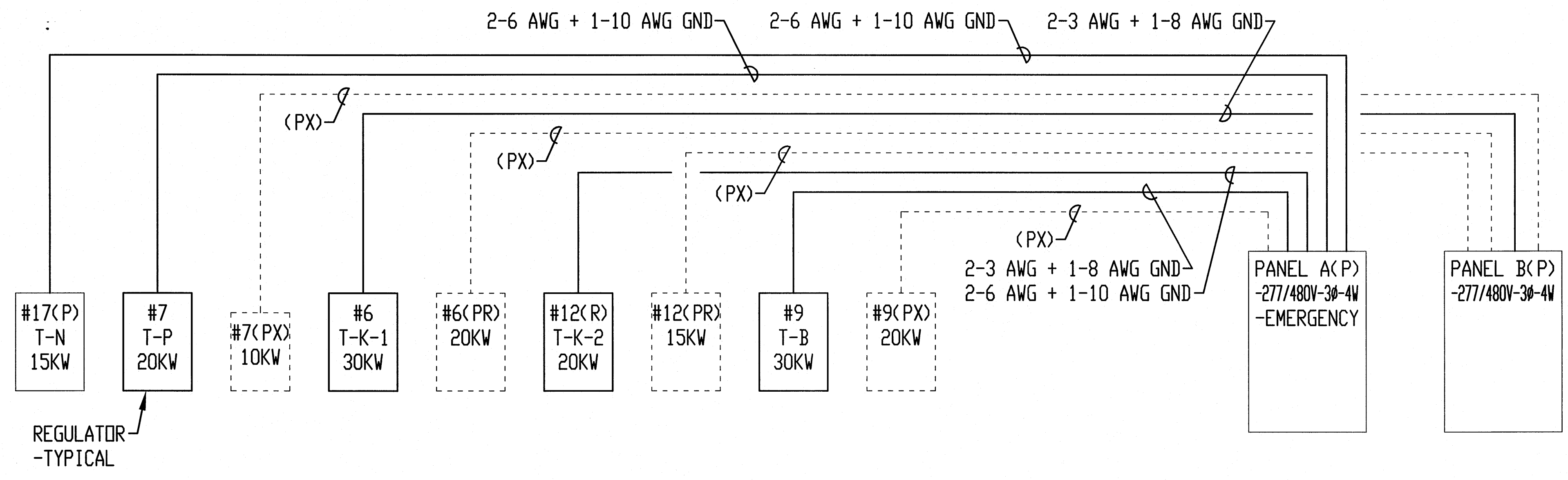
1. REGULATOR #17 (15KW) SHALL BE CONNECTED TO SERVE T-N.
2. RE-LABEL REGULATOR #11 AS 'T-E-2'.
3. REGULATOR #12 (20KW) SHALL BE RELOCATED FROM REGULATOR #6.
4. REGULATOR #6 (30KW), REGULATOR #7 (20KW), AND REGULATOR #9 (30KW) SHALL BE NEW. THE PROPOSED NEW REGULATORS SHALL BE 'THYRISTOR BASED' TYPE REGULATORS WITH MULTI-OUTPUT TAPPING CAPABILITY (MINIMUM INCREMENTS OF 25%). THE NEW REGULATORS SHALL BE COMPATIBLE WITH 'LED' SIGNS / 'LED' LIGHTS ON THE SAME CIRCUIT WITH INCANDESCENT SIGNS / INCANDESCENT LIGHTS. THE REGULATORS SHALL ALSO BE COMPATIBLE AND ABLE TO OPERATE EFFICIENTLY WITH AN ALL 'LED' SIGN / LIGHT CIRCUIT. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.



GENERAL NOTES:

1. ANY EXISTING CABLES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.
2. THE ELECTRICAL INSTALLATION AS A MINIMUM SHALL MEET THE NATIONAL ELECTRICAL CODE AND LOCAL REGULATIONS.
3. THE COST OF ALL WORK REQUIRED TO INSTALL THE VAULT EQUIPMENT, INCLUDING BUT NOT LIMITED TO: CABLES, WIRING, SPLICES, CONDUITS, CIRCUIT BREAKERS, CONCRETE, REGULATORS, ETC. SHALL BE INCLUDED IN THE 'AR109210 VAULT MODIFICATIONS' CONTRACT LUMP SUM PRICE.

EXTEND EXISTING CONCRETE PAD AS REQUIRED USING P-610 SPEC STRUCTURAL PCC. TIE PROPOSED TO EXISTING WITH 3/4" EXPANSION ANCHORS AT 12" CTRS IN MIDDLE OF SLAB.



ONE-LINE RISER DIAGRAM
 SCALE: NONE

NOTES:

- REGULATOR #6 (20KW) SHALL BE RELOCATED AND BECOME REGULATOR #12.
- REGULATOR #12 (15KW) SHALL BE REMOVED AND BECOME A SPARE.
- REGULATOR #6 (30KW), REGULATOR #7 (20KW), AND REGULATOR #9 (30KW) SHALL BE NEW. THE PROPOSED NEW REGULATORS SHALL BE "THYRISTOR BASED" TYPE REGULATORS WITH MULTI-OUTPUT TAPPING CAPABILITY (MINIMUM INCREMENTS OF 25%). THE NEW REGULATORS SHALL BE COMPATIBLE WITH "LED" SIGNS / "LED" LIGHTS ON THE SAME CIRCUIT WITH INCANDESCENT SIGNS / INCANDESCENT LIGHTS. THE REGULATORS SHALL ALSO BE COMPATIBLE AND ABLE TO OPERATE EFFICIENTLY WITH AN ALL "LED" SIGN / LIGHT CIRCUIT. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
- ROUTE CABLES IN CABLE TRAY SIMILAR TO EXISTING CABLING.
- SPARE REGULATOR #17 REMAINS AND SHALL BE CONNECTED TO SERVE T-N.
- REGULATOR #7 (10KW) SHALL BE REMOVED AND BECOME A SPARE.
- REGULATOR #9 (20KW) SHALL BE REMOVED AND BECOME A SPARE.

GENERAL NOTES:

- GROUNDS AND NEUTRALS ARE NOT SHOWN FOR CLARITY, BUT ARE REQUIRED PER N. E. C.
- ALL WIRE TO BE 600V INSULATED, TYPE THWN UNLESS OTHERWISE NOTED.
- THE COST OF ALL WORK REQUIRED TO INSTALL THE VAULT EQUIPMENT, SHALL BE INCLUDED IN THE "AR109210 VAULT MODIFICATIONS" CONTRACT LUMP SUM PRICE.
- PROVIDE MONITORING AND CONTROL, CONNECT TO EXISTING PLC CONTROL SYSTEM.

ELECTRICAL ABBREVIATIONS

ABBREVIATION	DESCRIPTION
BSB	BUILDING SWITCHBOARD
GND	GROUND
KVA	KILO-VOLT AMPERES
KW	KILOWATT
MDP	MAIN DISTRIBUTION PANEL
PH OR ∅	PHASE
V	VOLTS
XFMR	TRANSFORMER
(P)	PRESENT
(PR)	PRESENT TO BE RELOCATED
(PX)	PRESENT TO BE REMOVED
(R)	RELOCATED

G:\AIRPORT\A091002 TP E P\MT\REFM\09-116-51.DWG, 7/8/2009 10:25:34 AM, jefm

PANEL: <u>A(EXISTING)</u> (EMERGENCY)		VOLTAGE: <u>277/480V-3PH-4W</u>			
MOUNTING: <u>SURFACE</u>		MAINS: <u>400A MLO</u>			
CKT. NO.	BKR.	DESCRIPTION	DESCRIPTION	BKR.	CKT. NO.
1	50/2	15 KW - T-A(REG#16)	15 KW - SIGN CKT(REG#10)	50/2	2
3	---	---	---	---	4
5	70/2	20 KW - T-H(REG#8)	SPACE	---	6
7	---	---	SPACE	---	8
9	---	SPACE	20 KW - R-9-27-2(REG#2)	70/2	10
11	---	SPACE	---	---	12
13	70/2	20 KW - R-9-27-1 W(REG#1)	SPACE	---	14
15	---	---	SPACE	---	16
17	---	SPACE	20 KW - R-13-31-1 N(REG#3)	70/2	18
19	---	SPACE	---	---	20
21	70/2	20 KW - T-F-2(REG#5)	SPACE	---	22
23	---	---	SPACE	---	24
25	---	SPACE	20 KW - R-13-31-2 S(REG#4)	70/2	26
27	---	SPACE	---	---	28
29	---	SPACE	15 KW - T-E(REG#14)	50/2	30
31	25/3	15 KVA XFMR	---	---	32
33	---	---	15 KW - T-F-1(REG#15)	50/2	34
35	---	---	---	---	36
37	70/2	20 KW - T-B(REG#9)	SPACE	---	38
39	---	---	SPACE	---	40
41	---	SPACE	SPACE	---	42

PANEL: <u>B(EXISTING)</u>		VOLTAGE: <u>277/480V-3PH-4W</u>			
MOUNTING: <u>SURFACE</u>		MAINS: <u>225A MLO</u>			
CKT. NO.	BKR.	DESCRIPTION	DESCRIPTION	BKR.	CKT. NO.
1	50/2	15 KW - T-D(REG#11)	10 KW - T-N(REG#7)	50/2	2
3	---	---	---	---	4
5	---	SPACE	20 KW - T-K-1(REG#6)	70/2	6
7	---	SPACE	---	---	8
9	50/2	15 KW - T-K-2(REG#12)	SPACE	---	10
11	---	---	SPACE	---	12
13	50/2	15 KW - R-5-23(REG#13)	SPACE	---	14
15	---	---	SPACE	---	16
17	---	SPACE	SPACE	---	18
19	---	SPACE	SPACE	---	20
21	---	SPACE	SPACE	---	22
23	---	SPACE	SPACE	---	24
25	---	SPACE	SPACE	---	26
27	---	SPACE	SPACE	---	28
29	---	SPACE	SPACE	---	30
31	---	SPACE	SPACE	---	32
33	---	SPACE	SPACE	---	34
35	---	SPACE	SPACE	---	36
37	---	SPACE	SPACE	---	38
39	---	SPACE	SPACE	---	40
41	---	SPACE	SPACE	---	42

PANEL: <u>A(REVISED)</u> (EMERGENCY)		VOLTAGE: <u>277/480V-3PH-4W</u>			
MOUNTING: <u>SURFACE</u>		MAINS: <u>400A MLO</u>			
CKT. NO.	BKR.	DESCRIPTION	DESCRIPTION	BKR.	CKT. NO.
1	50/2	15 KW - T-A(REG#16)	15 KW - SIGN CKT(REG#10)	50/2	2
3	---	---	---	---	4
5	70/2	20 KW - T-H(REG#8)	SPACE	---	6
7	---	---	SPACE	---	8
9	60/2	20 KW T-P(REG#7)	20 KW - R-9-27-2(REG#2)	70/2	10
11	---	---	---	---	12
13	70/2	20 KW - R-9-27-1 W(REG#1)	20 KW T-K-2(REG#12)	60/2	14
15	---	---	---	---	16
17	---	SPACE	20 KW - R-13-31-1 N(REG#3)	70/2	18
19	---	SPACE	---	---	20
21	70/2	20 KW - T-F-2(REG#5)	---	---	22
23	---	---	SPACE	---	24
25	---	SPACE	20 KW - R-13-31-2 S(REG#4)	70/2	26
27	---	SPACE	---	---	28
29	25/3	15 KVA XFMR	15 KW - T-E-1(REG#14)	50/2	30
31	---	---	---	---	32
33	---	---	15 KW - T-F-1(REG#15)	50/2	34
35	90/2	30 KW - T-B(REG#9)	---	---	36
37	---	---	SPACE	---	38
39	---	SPACE	15 KW T-N(REG#17)	50/2	40
41	---	SPACE	---	---	42

PANEL: <u>B(REVISED)</u>		VOLTAGE: <u>277/480V-3PH-4W</u>			
MOUNTING: <u>SURFACE</u>		MAINS: <u>225A MLO</u>			
CKT. NO.	BKR.	DESCRIPTION	DESCRIPTION	BKR.	CKT. NO.
1	50/2	15 KW - T-E-2(REG#11)	SPARE	50/2	2
3	---	---	---	---	4
5	---	SPACE	30 KW - T-K-1(REG#6)	90/2	6
7	---	SPACE	---	---	8
9	50/2	SPARE	SPACE	---	10
11	---	---	SPACE	---	12
13	50/2	15 KW - R-5-23(REG#13)	SPACE	---	14
15	---	---	SPACE	---	16
17	---	SPACE	SPACE	---	18
19	---	SPACE	SPACE	---	20
21	---	SPACE	SPACE	---	22
23	---	SPACE	SPACE	---	24
25	---	SPACE	SPACE	---	26
27	---	SPACE	SPACE	---	28
29	---	SPACE	SPACE	---	30
31	---	SPACE	SPACE	---	32
33	---	SPACE	SPACE	---	34
35	---	SPACE	SPACE	---	36
37	---	SPACE	SPACE	---	38
39	---	SPACE	SPACE	---	40
41	---	SPACE	SPACE	---	42

PANELS GENERAL NOTES:

1. REMOVE AND RELOCATE CIRCUIT BREAKERS AS REQUIRED TO SERVE LOADS AS INDICATED IN REVISED PANEL SCHEDULES.
2. PROVIDE NEW CIRCUIT BREAKERS AS REQUIRED TO SERVE NEW/REVISED LOADS AS INDICATED IN REVISED PANEL SCHEDULES.
3. PROVIDE BLANK FILLERS IN ALL UN-USED OPENINGS.
4. VERIFY LOADS ON EACH CIRCUIT AND RE-BALANCE LOADS AS REQUIRED. RECORD PRIMARY AND SECONDARY VOLTAGE AND CURRENT FOR EACH REGULATOR AT MAXIMUM OUTPUT CURRENT LEVEL.

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE IV - EAST PAVING
ILL. MLI-3873, QU011
SHEET 53 OF 74

GENERAL NOTES:

- COLOR CODE ALL PHASE WIRING BY USE OF COLORED TAPE (TAXIWAY P = CYAN).
- ALL BRANCH CIRCUIT CONDUCTORS CONNECTED TO A PARTICULAR PHASE SHALL BE IDENTIFIED WITH THE SAME COLOR. THE COLOR CODING SHALL BE EXTENDED TO THE POINT OF UTILIZATION.
- IN CONTROL WIRING THE SAME COLOR SHALL BE USED THROUGHOUT THE SYSTEM FOR THE SAME FUNCTION, SUCH AS 10%, 30%, 100% BRIGHTNESS CONTROL, ETC.
- ALL POWER AND CONTROL CIRCUIT CONDUCTORS SHALL BE COPPER, ALUMINUM IS NOT ACCEPTABLE. THIS INCLUDES WIRE, CABLE, BUSSES, TERMINALS, SWITCH/PANEL COMPONENTS, ETC.
- LOW VOLTAGE (600V) AND HIGH VOLTAGE (5000V) CONDUCTORS SHALL BE INSTALLED IN SEPERATE WIREWAYS.

GENERAL NOTES:

- NEATLY LACE WIRING IN DISTRIBUTION PANELS, WIREWAYS, SWITCHES AND JUNCTION/PULL BOXES.
- EQUIPMENT CABINETS SHALL NOT BE USED AS PULL/JUNCTION BOXES. ONLY WIRING TERMINATING AT THE EQUIPMENT SHALL BE BROUGHT INTO THESE ENCLOSURES.
- SPLICES AND JUNCTION POINTS SHALL BE PERMITTED ONLY IN JUNCTION BOXES, DUCTS EQUIPPED WITH REMOVABLE COVERS, AND AT EASILY ACCESSIBLE LOCATIONS.
- CIRCUIT BREAKERS IN POWER DISTRIBUTION PANEL(S) SHALL BE THERMAL-MAGNETIC, MOLDED CASE, PERMENENT TRIP WITH 100 AMPERE, MINIMUM, FRAME.
- ALL WALL AND CEILING MOUNTED EQUIPMENT, ENCLOSURES AND CONDUITS SHALL BE MOUNTED ON STRUT FRAMING TO ALLOW A MINIMUM OF 1/2" AIR SPACE BETWEEN WALL AND EQUIPMENT.

GENERAL NOTES:

- RIGID STEEL CONDUIT SHALL BE USED THROUGHOUT THE INSTALLATION UNLESS OTHERWISE SPECIFIED. THE MINIMUM TRADE SIZE SHALL BE 3/4 INCH.
- ALL RIGID CONDUIT SHALL BE TERMINATED AT CONSTANT CURRENT REGULATORS WITH A SECTION (24" MINIMUM) OF FLEXIBLE CONDUIT.
- UNLESS OTHERWISE SHOWN ALL EXPOSED CONDUITS SHALL BE RUN PARALLEL TO OR AT RIGHT ANGLES WITH THE LINES OF THE STRUCTURE.
- ALL STEEL CONDUITS, FITTINGS, NUTS, BOLTS, ETC., SHALL BE GALVANIZED.
- USE CONDUIT BUSHINGS AT EACH CONDUIT TERMINATION. WHERE #4 AWG OR LARGER UNGROUNDED WIRE IS INSTALLED, USE INSULATED BUSHINGS.
- USE DOUBLE LOCK NUTS AT EACH CONDUIT TERMINATION.

GENERAL NOTES:

- WRAP ALL PRIMARY AND SECONDARY POWER TRANSFORMER CONNECTIONS WITH SUFFICIENT LAYERS OF INSULATING TAPE AND COVER WITH INSULATING VARNISH FOR FULL VALUE OF CABLE INSULATION VOLTAGE.
- UNLESS OTHERWISE NOTED, ALL INDOOR SINGLE CONDUCTOR CONTROL WIRING SHALL BE #12 AWG.
- BOTH ENDS OF EACH CONTROL CONDUCTOR SHALL BE TERMINATED AT A TERMINAL BLOCK. THE TERMINAL BLOCK SHALL BE OF PROPER RATING AND SIZE FOR THE FUNCTION INTENDED AND THEY SHALL BE LOCATED IN EQUIPMENT ENCLOSURES.
- ALL CONTROL CONDUCTOR TERMINATIONS SHALL BE OF THE OPEN-EYE CONNECTOR/SCREW TYPE. SOLDERED, CLOSED-EYE TERMINATIONS, OR TERMINATIONS WITOUT CONNECTORS ARE NOT ACCEPTABLE.
- BOTH ENDS OF ALL CONTROL CONDUCTORS SHALL BE IDENTIFIED AS TO THE CIRCUIT, TERMINAL BLOCK, AND TERMINAL NUMBER. ONLY STICK-ON LABELS SHALL BE USED.
- ANY AND ALL MATERIALS REMOVED AND NOT REUSED SHALL REMAIN THE PROPERTY OF THE QCIA. THIS INCLUDES, BUT IS NOT LIMITED TO, WIRE, CABLE, CONDUIT, ELECTRICAL EQUIPMENT, REGULATORS, ETC.

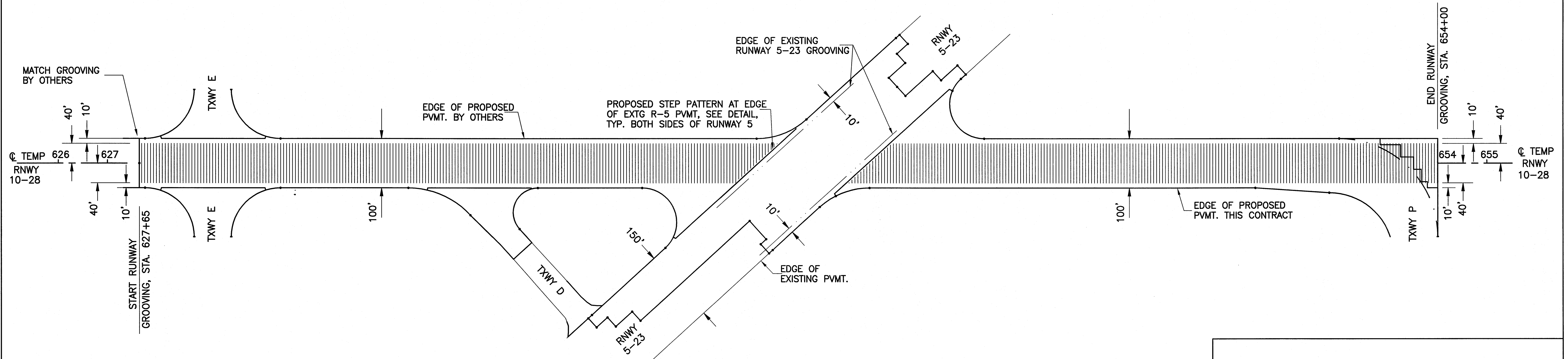
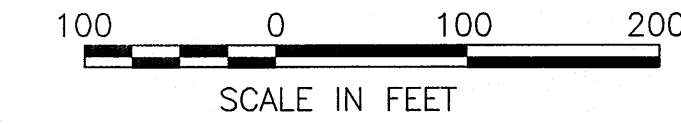
EXISTING CONSTANT CURRENT REGULATOR SCHEDULE

REG #	REG SIZE (KW)	PRIMARY VOLTAGE	MEASURED CKT LOAD (VA)	PANEL CKT (OCP)	CIRCUIT BEING FED
1	20	480	13,373	A-13 (70/2)	R-9-27-1 WEST
2	20	480	8,784	A-10(70/2)	R-9-27-2 EAST
3	20	480	8,827	A-18(70/2)	R-13-31-1 NORTH
4	20	480	10,800	A-26(70/2)	R-13-31-2 SOUTH
5	20	480	12,336	A-21(70/2)	T-F-2
6	20	480	19,200	B-6(70/2)	T-K-1
7	10	480	11,040	B-2(50/2)	T-N
8	20	480	11,808	A-5(70/2)	T-H
9	20	480	24,480	A-37(70/2)	T-B
10	15	480	8,904	A-2(50/2)	SIGNS
11	15	480	7,752	B-1(50/2)	T-D
12	15	480	11,808	B-9(50/2)	T-K-2
13	15	480	-----	B-13(50/2)	R-5-23
14	15	480	10,416	A-30(50/2)	T-E
15	15	480	7,440	A-34(50/2)	T-F-1
16	15	480	11,040	A-1(50/2)	T-A
17	15	480			SPARE
18					

REVISED CONSTANT CURRENT REGULATOR SCHEDULE

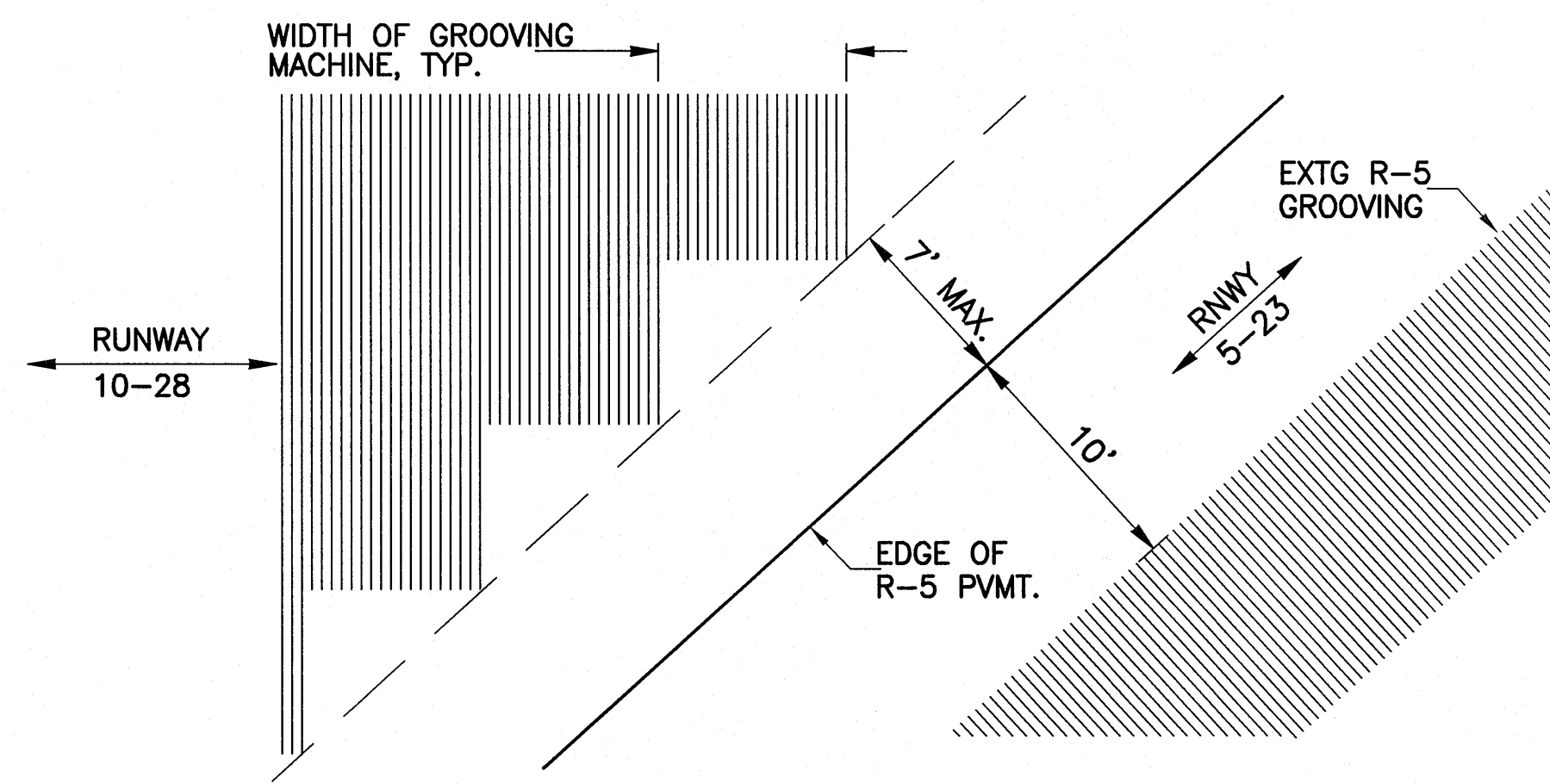
REG #	REG SIZE (KW)	PRIMARY VOLTAGE	MEASURED CKT LOAD (VA) /CALCULATED (WATTS)	PANEL CKT (OCP)	CIRCUIT BEING FED
1	20	480	13373 / -	A-13 (70/2)	R-9-27-1 WEST
2	20	480	8784 / -	A-10(70/2)	R-9-27-2 EAST
3	20	480	8827 / -	A-18(70/2)	R-13-31-1 NORTH
4	20	480	10800 / -	A-26(70/2)	R-13-31-2 SOUTH
5	20	480	12336 / -	A-21(70/2)	T-F-2
6	30	480	19,200 / 19,472	B-6(90/2)	T-K-1
7	20	480	- / 15,565	A-9(60/2)	T-P
8	20	480	11808 / -	A-5(70/2)	T-H
9	30	480	24480 / -	A-35(90/2)	T-B
10	15	480	8904 / -	A-2(50/2)	SIGNS
11	15	480	7,752 / 5,820	B-1(50/2)	T-E-2
12	20	480	11,808 / 15,336	A-14(60/2)	T-K-2
13	15	480	-----	B-13(50/2)	R-5-23
14	15	480	10,416 / 13,225	A-30(50/2)	T-E-1
15	15	480	7440 / -	A-34(50/2)	T-F-1
16	15	480	11040 / -	A-1(50/2)	T-A
17	15	480	11,040 / 11,706	A-40(50/2)	T-N
18					

GROOVING PLAN

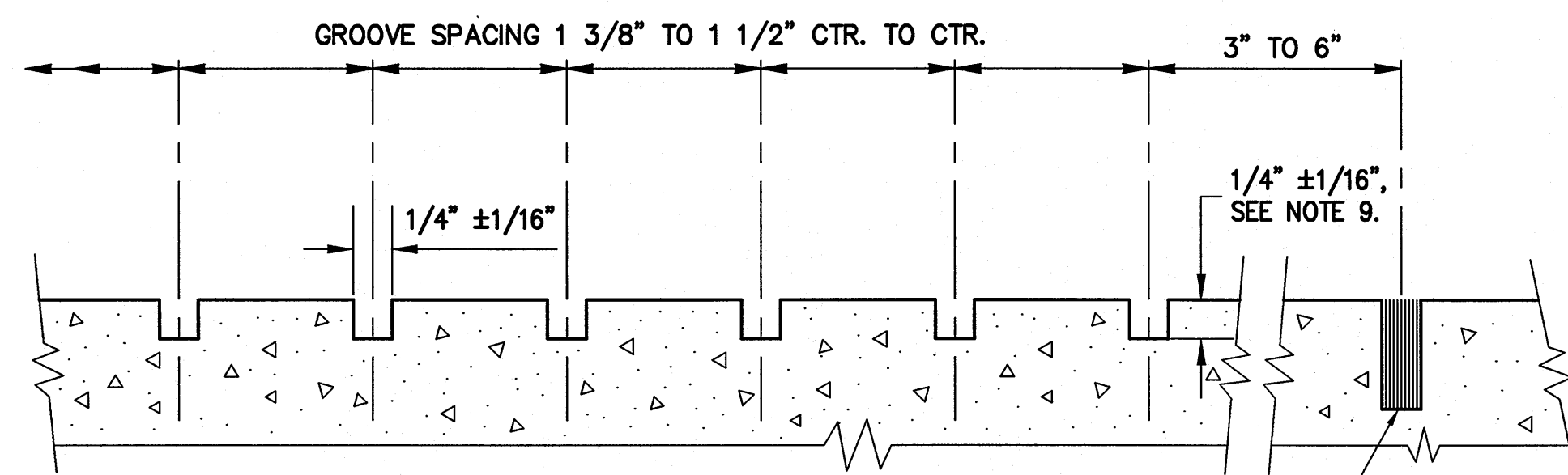


NOTES:

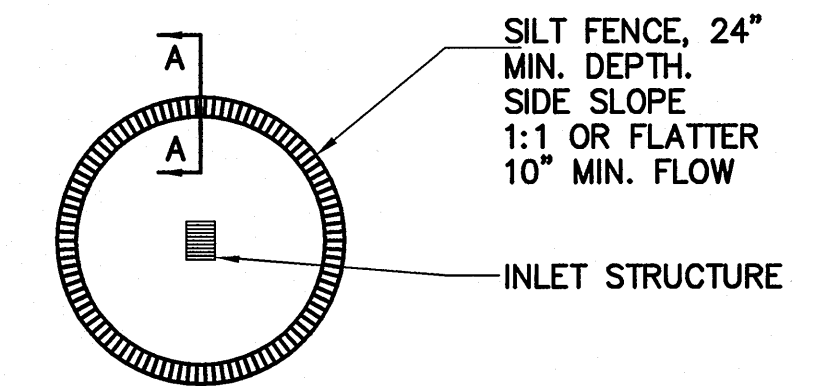
- GROOVING EXTENDS OUTWARD 40' FROM C OF TEMPORARY RUNWAY 10-28 UNLESS OTHERWISE NOTED.
- SUCCESSIVE PASSES OF THE GROOVING MACHINE SHALL NOT OVERLAP.
- CONTRACTOR SHALL NOT ALLOW ACCUMULATION OF CUTTINGS IN TURFED AREAS. THE GROOVING MACHINE SHALL BE EQUIPPED WITH AUTOMATIC CUTTING PICK-UP FEATURES TO PREVENT ACCUMULATION OF CUTTINGS IN THE TURFED AREAS.
- GROOVING SHALL BE COMPLETED PRIOR TO THE INSTALLATION OF NEOPRENE COMPRESSION SEALS IN THE LONGITUDINAL JOINTS.
- CONTRACTOR SHALL REPLACE AND / OR RE-SEAL ALL EXISTING JOINTS THAT ARE DAMAGED BY THE PAVEMENT GROOVING OPERATIONS. COSTS TO BE INCLUDED IN THE CONTRACT UNIT PRICE FOR PAVEMENT.
- GROOVES SHALL BE SAWED NO LESS THAN 6 INCHES AND NO MORE THAN 18 INCHES FROM PROPOSED OR FUTURE IN-PAVEMENT LIGHT FIXTURES.
- THE RUNWAY 10-28 GROOVES SHALL NOT EXTEND ACROSS RUNWAY 5-23. AT BOTH EDGES OF RUNWAY 5, THE RUNWAY 10-28 GROOVES SHALL BE CUT IN A STEP PATTERN AS SHOWN.
- BOTH PCC PAVEMENT GROOVING AND BITUMINOUS PAVEMENT GROOVING SHALL BE MEASURED AND PAID FOR UNDER CONTRACT ITEM 501540.
- AT LEAST FORTY PERCENT (40%) OF THE GROOVES SHALL HAVE A DEPTH THAT MEASURES MORE THAN ONE-FORTH OF AN INCH (1/4").



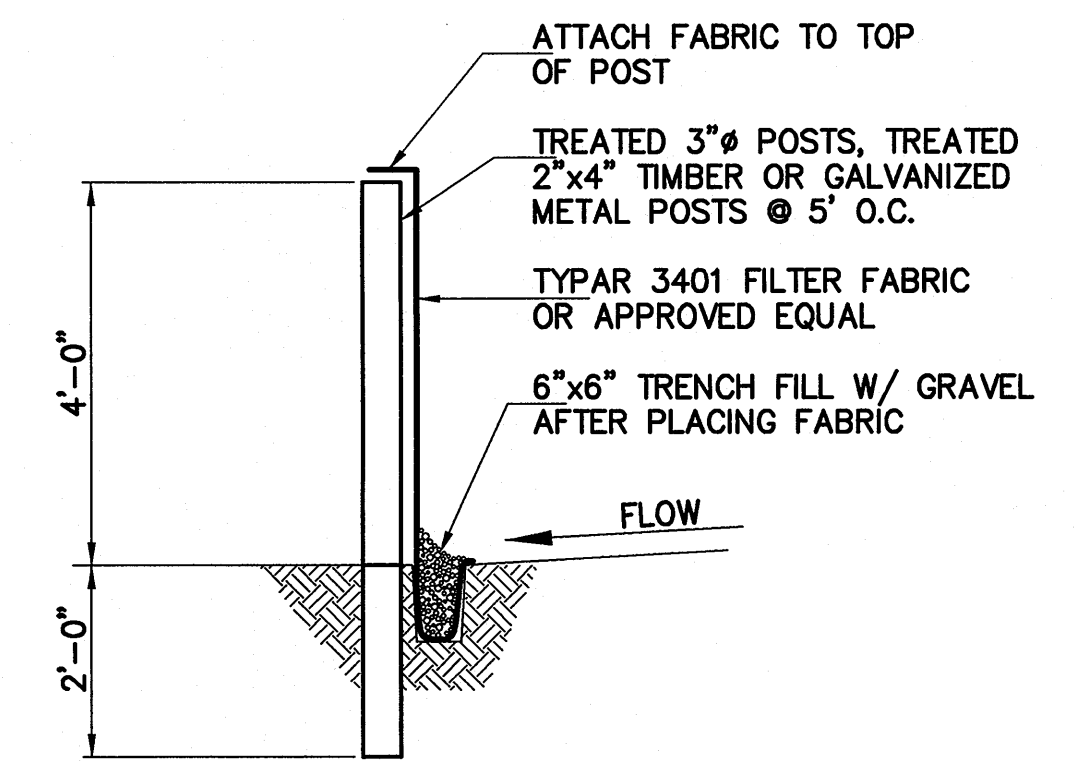
STEP PATTERN DETAIL
(TYPICAL, BOTH SIDES OF RWNY 5)



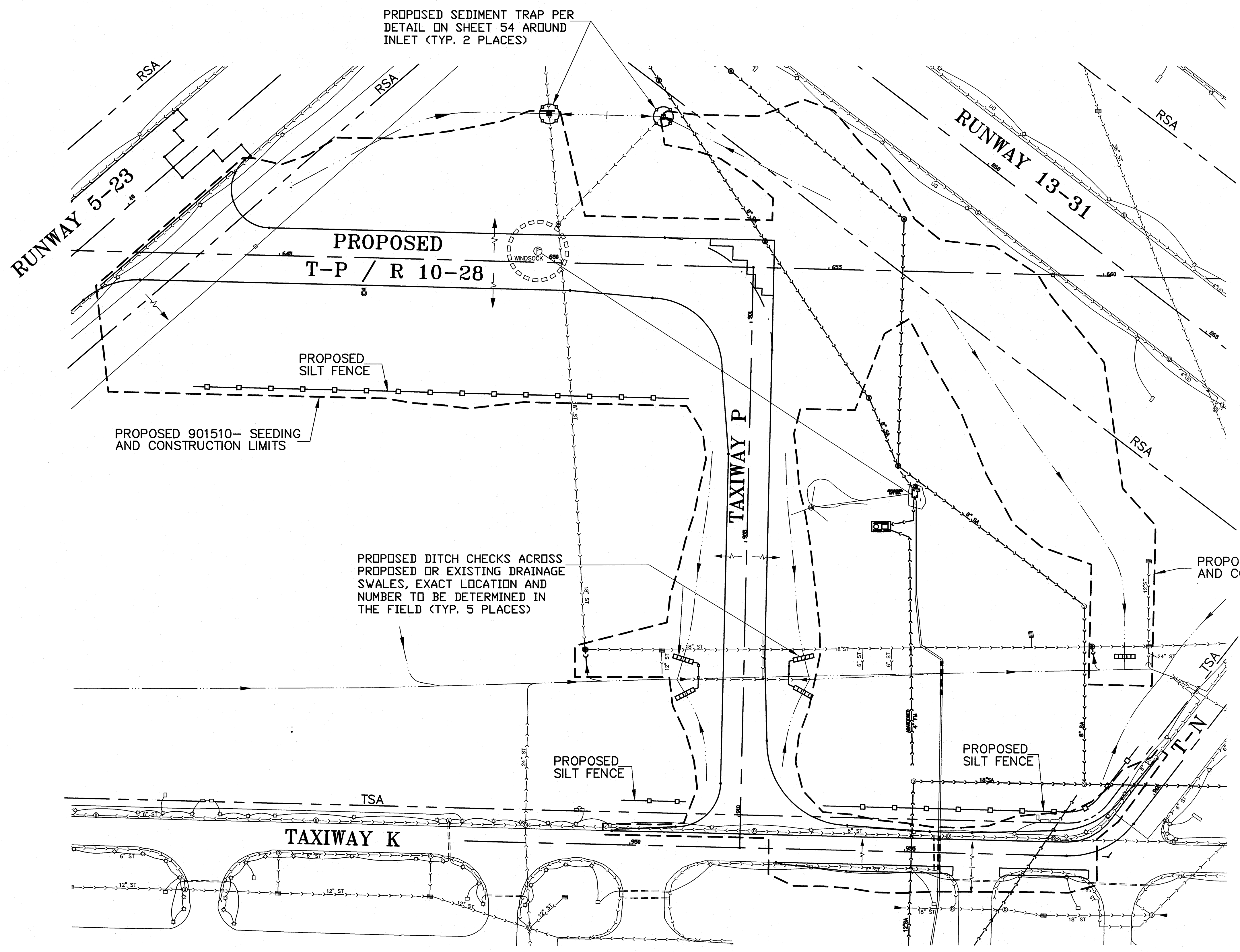
GROOVE DIMENSIONS
N.T.S.



TEMPORARY SEDIMENT TRAP
AT ALL INLET STRUCTURES

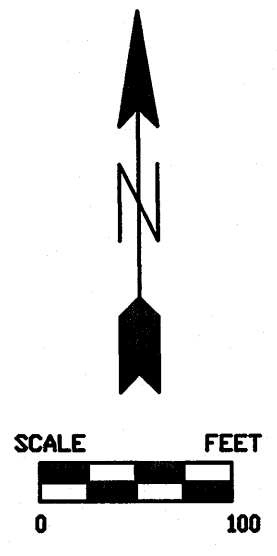


SECTION A-A
SILT FENCE DETAIL
NOT TO SCALE



LEGEND:

- EXTG. EDGE LIGHT
- EXTG. GUIDANCE SIGN
- EXTG. ELECTRICAL CABLE
- EXTG. STORM SEWER OR UNDERDRAIN
- EXTG. INLET
- EXTG. OUTLET
- EXTG. MANHOLE
- EXTG. DRAINAGE DITCH
- PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
- EXISTING FENCE
- EXISTING OR PROPOSED DRAINAGE SWALE
- PROPOSED SILT FENCE
- PROPOSED TEMPORARY STRAW BALE DITCH CHECK
- PROPOSED SEDIMENT TRAP



NOTE:

SEE SHEET 56 FOR EROSION & SEDIMENT CONTROL MEASURES TABLE, STORM WATER POLLUTION PREVENTION GENERAL NOTES, STORM WATER MANAGERS SIGNATURE TABLE AND ANTICIPATED PROBABLE CONSTRUCTION ACTIVES SCHEDULE.

TAXIWAY P / R10-28 EAST AREA PLAN

G:\airport\A09T002 TP E P\MT\SWPP.dwg, 7/8/2009 10:26:57 AM, jefm

EROSION AND SEDIMENT CONTROL MEASURES		
ITEM	LOCATION	SPECIAL PROVISION/ PAY ITEM
TEMPORARY DITCH CHECKS	TAXIWAY P, STA. 906+90, LT. 695'; STA. 907+15, RT.; STA. 907+15, LT.; STA. 907+80, RT; AND STA. 907+80, LT.	156500
TEMPORARY SILT FENCE	AT ALL SEDIMENT TRAP & DITCH CHECK LOCATIONS AND T-P, STA. 643+50 TO STA. 652+50 RT.; T-P, STA. 906 +90 TO STA. 907+15, RT.; T-P STA. 906+90 TO STA. 907+15, LT.; T-K STA. 949+80 TO STA. 951+00, LT.; AND T-K, STA. 953+50 TO T-N STA. 960+50, LT.	156500
TEMPORARY SEDIMENT TRAPS	TAXIWAY P, STA. 649+84, LT. 270' AND STA. 651+92, LT. 270'.	156500

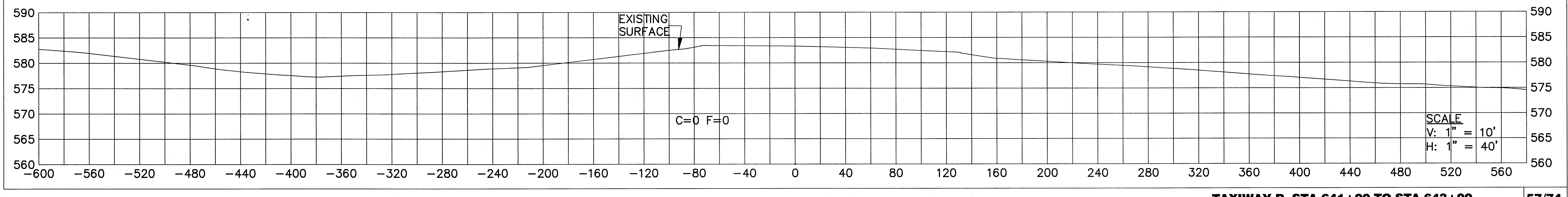
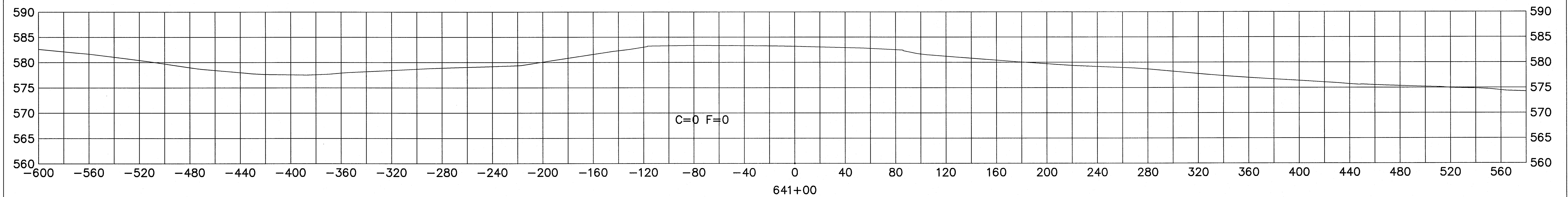
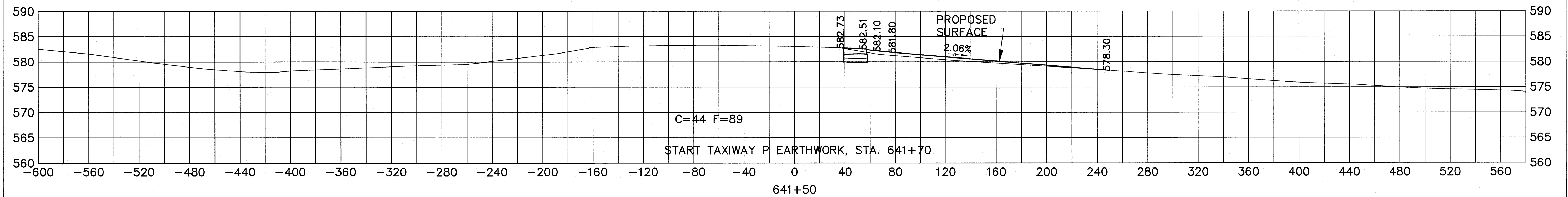
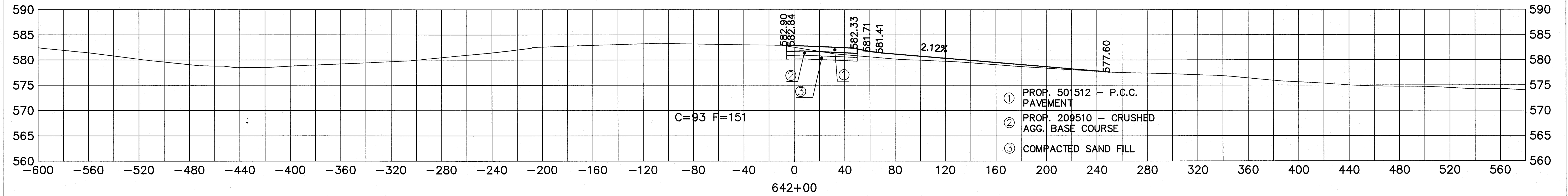
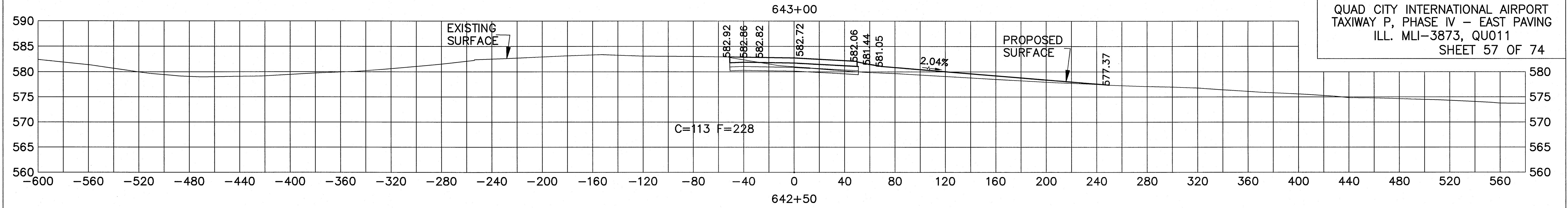
ANTICIPATED PROBABLE CONSTRUCTION ACTIVITIES SCHEDULE																					
NO.	ITEM DESCRIPTION	WEEK																			
		1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20
1	INSTALL SEDIMENT/EROSION CONTROLS	■																			
2	CLEARING AND GRADING		■	■																	
3	EXCAVATION AND EMBANKMENT			■	■	■	■	■	■	■											
4	TURFING																			■	■
5	MAINTAIN SEDIMENT/EROSION CONTROL																				
6	PAVING																				
7	CLEAN-UP																				

STORM WATER MANAGERS					
	NAME	ADDRESS	TELEPHONE NUMBER		SIGNATURE
			WORK	HOME	
CONTRACTOR					
SUBCONTRACTOR					
SUBCONTRACTOR					

GENERAL NOTES:

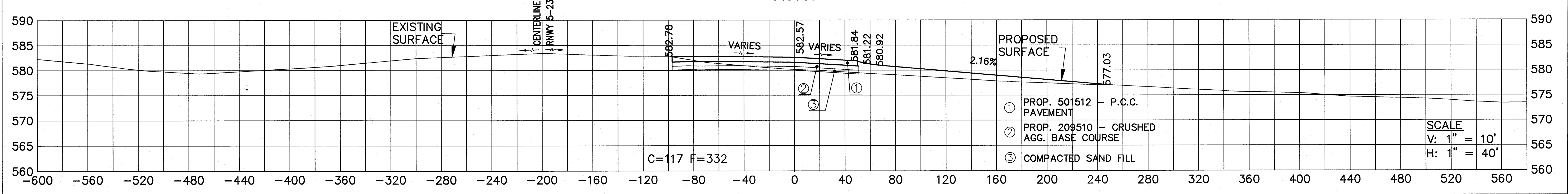
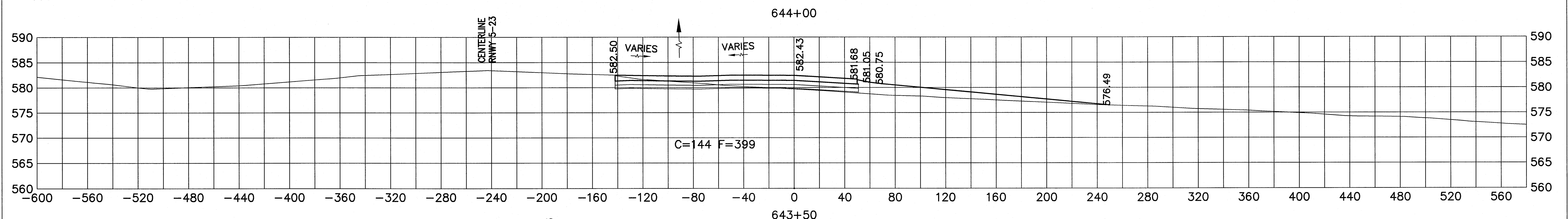
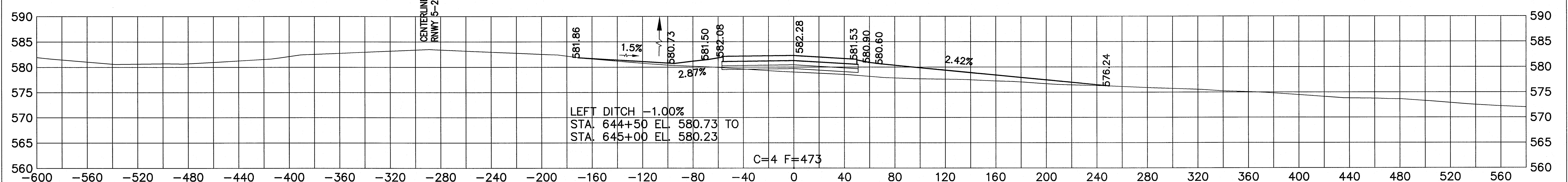
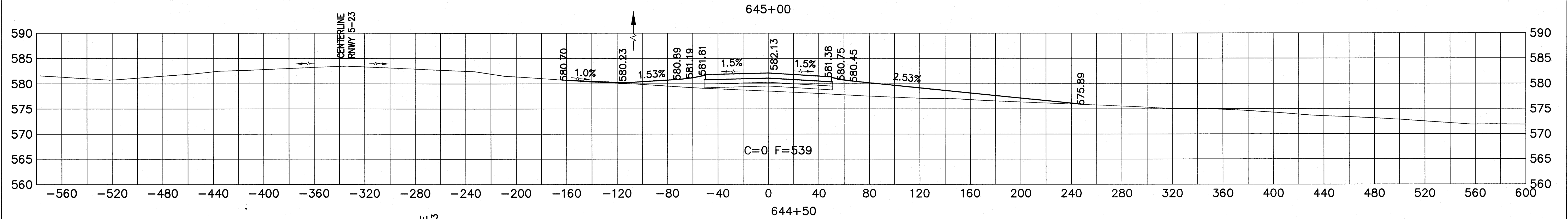
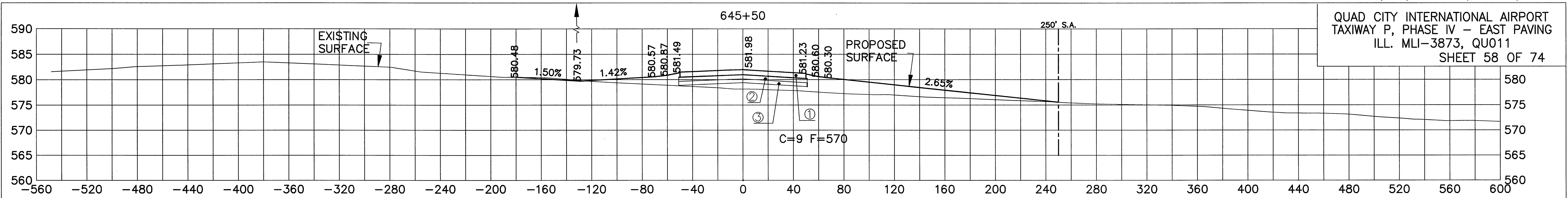
- TEMPORARY DITCH CHECKS - TWO BALES HIGH WITH SILT FENCING SHALL BE REQUIRED PER IDOT STANDARD 280001 - TEMPORARY EROSION SITE CONTROL SYSTEM DRAWING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING, MAINTAINING, AND REMOVING DITCH CHECKS, SILT FENCE, AND SEDIMENT TRAPS TO THE SATISFACTION OF THE RESIDENT ENGINEER. THIS INCLUDES, BUT IS NOT LIMITED TO, CLEANING EROSION SOILS AS REQUIRED.
- LOCATION OF THE DITCH CHECKS, SILT FENCES, AND SEDIMENT TRAPS SHOWN ARE APPROXIMATE. ACTUAL LOCATIONS TO BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION.
- ONCE CONSTRUCTION HAS BEEN COMPLETED, OR TEMPORARILY SUSPENDED FOR LONGER THAN 21 DAYS (SUCH AS A WINTER SHUTDOWN), THE CONTRACTOR SHALL SEED ALL AREAS DISTURBED IN ACCORDANCE WITH ITEM 901510 WITHIN 14 DAYS OF THE LAST DISTURBANCE. DITCH CHECKS, SILT FENCES, AND SEDIMENT TRAPS SHALL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE CONTRACTOR ESTABLISHES A GOOD STAND OF GRASS OF UNIFORM COLOR AND DENSITY TO THE SATISFACTION OF THE ENGINEER.
- THE CONTRACTOR AND EACH SUBCONTRACTOR RESPONSIBLE FOR WATER POLLUTION CONTROL SHALL DESIGNATE, PRIOR TO BEGINNING CONSTRUCTION, A PERSON OR PERSONS WHO CAN BE CONTACTED IN AN EMERGENCY INVOLVING THEIR WATER POLLUTION CONTROL ITEMS. THESE DESIGNATED PEOPLE SHALL BE AVAILABLE TO REPAIR AND MAINTAIN WATER POLLUTION CONTROL DEVICES ON A 24-HOUR / 7 DAYS PER WEEK BASIS.
- CONTRACTOR TO EXCAVATE TEMPORARY EROSION CONTROL DRAINAGE SWALE AS REQUIRED TO PREVENT RAIN WATER PONDING AND TO CONTROL STORM WATER RUN-OFF.
- CONTRACTOR SHALL ADHERE TO THE CITY OF MOLINE'S EROSION AND SEDIMENT CONTROL REGULATIONS AND THE ILLINOIS MANUAL ON EROSION AND SEDIMENT CONTROL.
- SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY CONSTRUCTION THAT DISTURBS EXISTING STORM WATER RUN-OFF CONDITIONS AND/OR GROUND VEGETATION.
- EROSION CONTROL MEASURES SHALL BE INSPECTED WEEKLY AND AFTER EACH PRECIPITATION EVENT AND REPLACED OR REPAIRED AS NECESSARY.
- RESIDENT ENGINEER SHALL CHECK THAT ALL FILL AREAS ARE TO A MINIMUM COMPACTION OF 95% OF THE MATERIALS STANDARD PROCTOR MAXIMUM DRY DENSITY.
- SILT FENCE, SEDIMENT TRAPS, AND HAY BALES SHALL BE CLEANED OR REPLACED WHEN SILT BUILDS UP TO WITHIN ONE FOOT OF THE TOP OF THE SILT FENCE OR HAY BALES.
- ADDITIONAL EROSION CONTROL DEVICES SHALL BE USED AS REQUIRED. THE COSTS OF ALL MEASURES NECESSARY TO COMPLY WITH THIS STORM WATER POLLUTION PREVENTION PLAN SHALL BE INCLUDED IN THE ITEM 156500 - TEMPORARY EROSION CONTROL LUMP SUM PRICES.

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE IV - EAST PAVING
ILL. MLI-3873, QU011
SHEET 57 OF 74



TAXIWAY P STA 641+00 TO STA 643+00

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE IV - EAST PAVING
ILL. MLI-3873, QU011
SHEET 58 OF 74

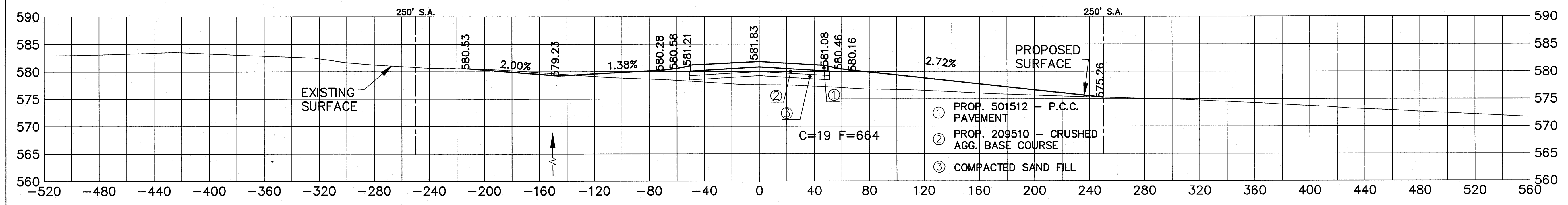
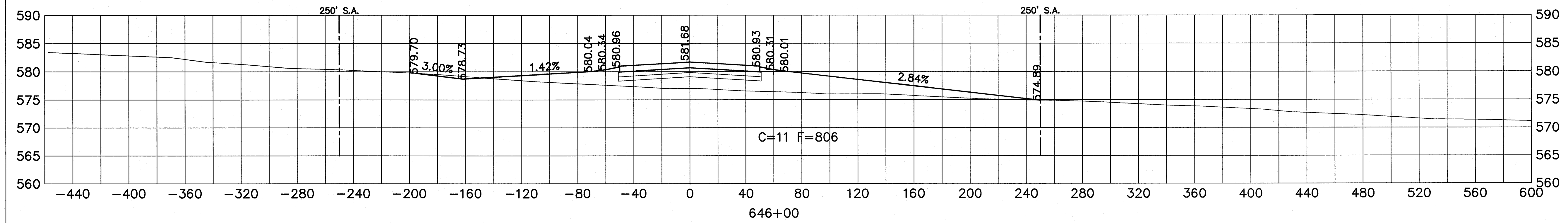
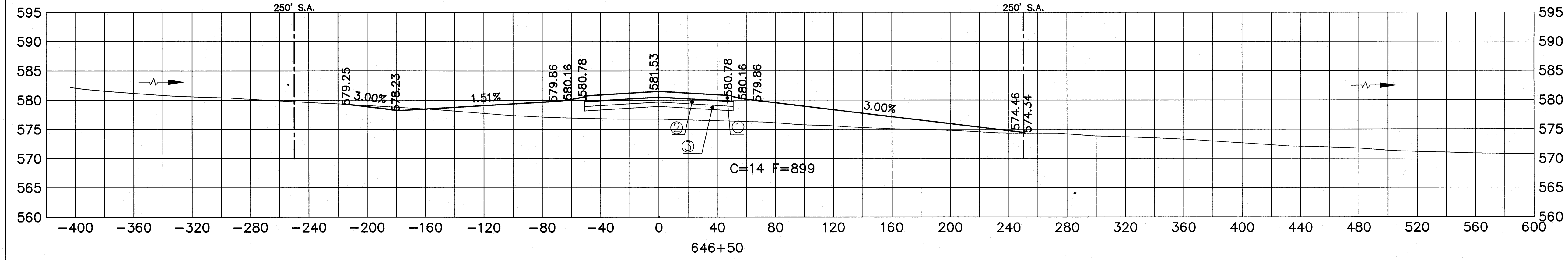
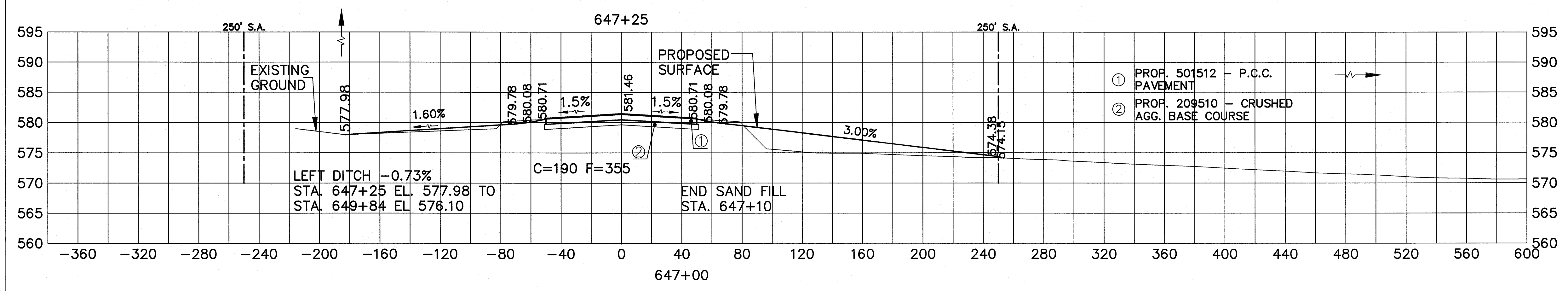


TAXIWAY P, STA 643+50 TO STA 645+00

58/74

G:\Airport\A09T002 TP E P\MT\Sections P.dwg, 7/8/2009 10:27:33 AM, jeffm

QUAD CITY INTERNATIONAL AIRPORT
 TAXIWAY P, PHASE IV - EAST PAVING
 ILL. MLI-3873, QU011
 SHEET 59 OF 74

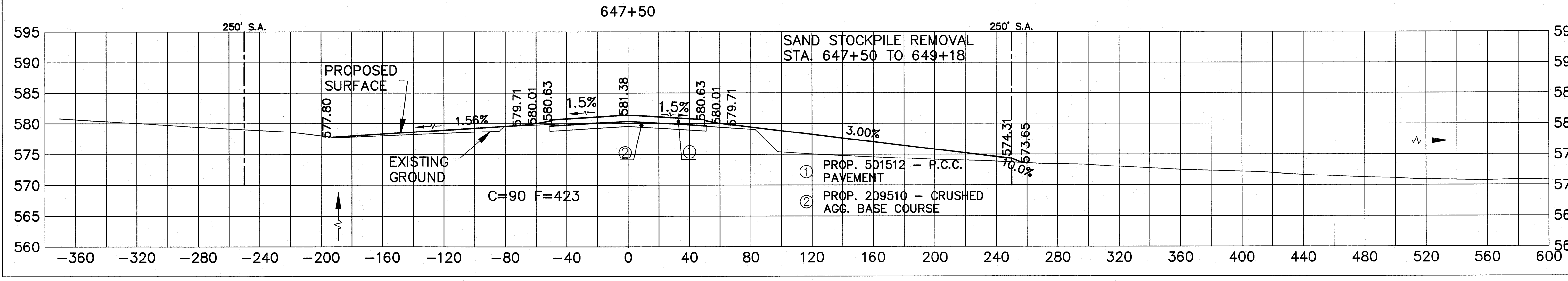
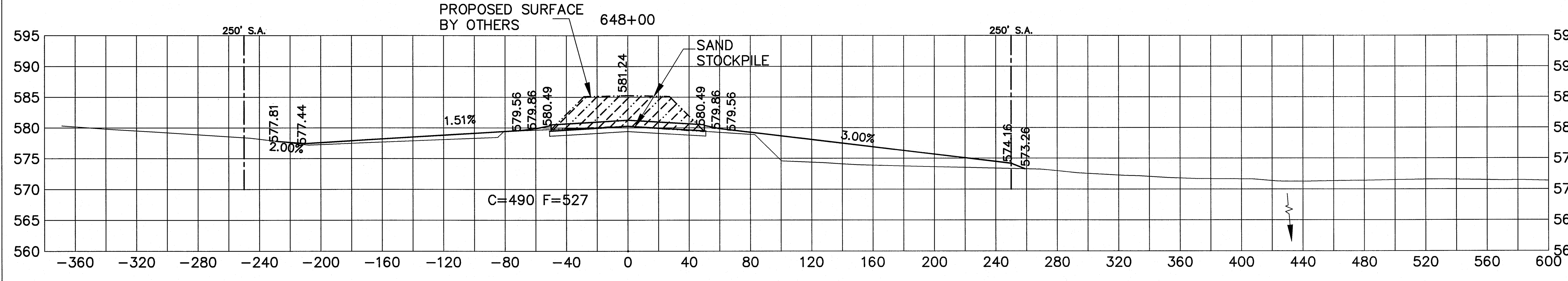
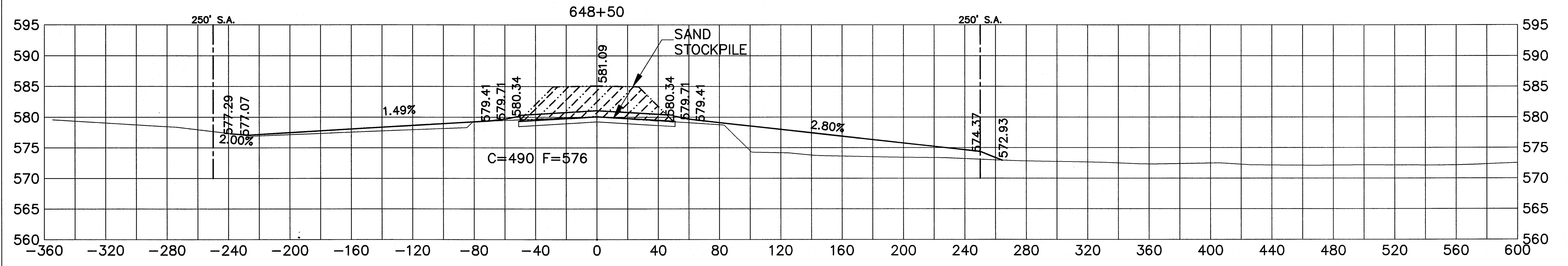
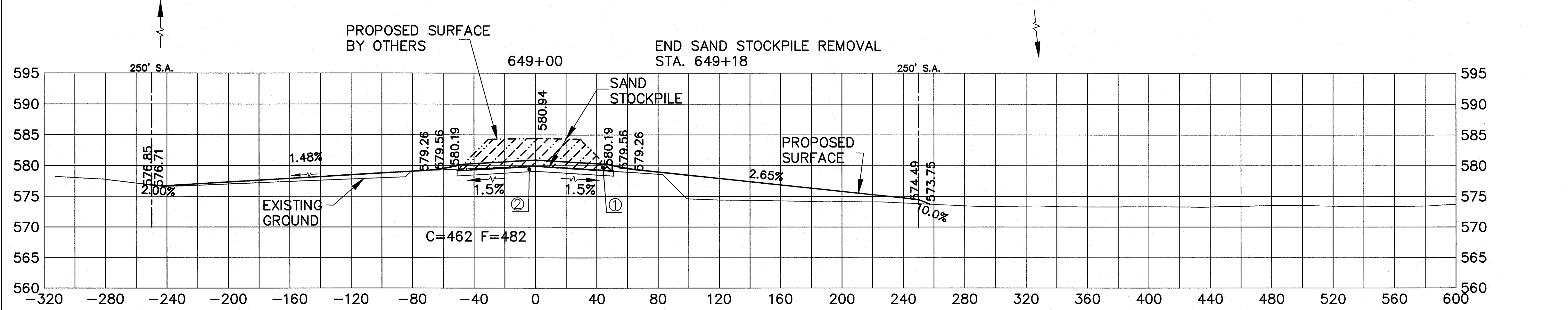


SCALE
 V: 1" = 10'
 H: 1" = 40'

TAXIWAY P, STA 646+00 TO STA 647+25

G:\AIRPORT\A09T002 TP E P\MT\Sections P.dwg, 7/8/2009 10:27:43 AM, jeffm

QUAD CITY INTERNATIONAL AIRPORT
 TAXIWAY P, PHASE IV - EAST PAVING
 ILL. MLI-3873, QU011
 SHEET 60 OF 74



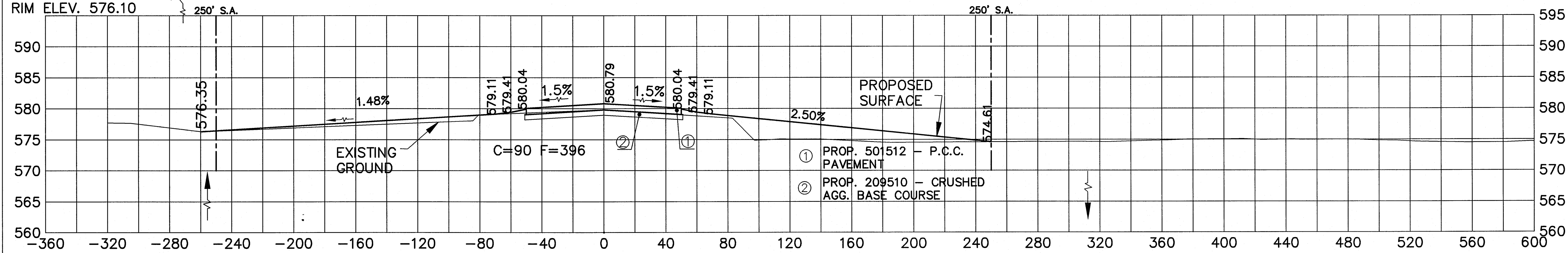
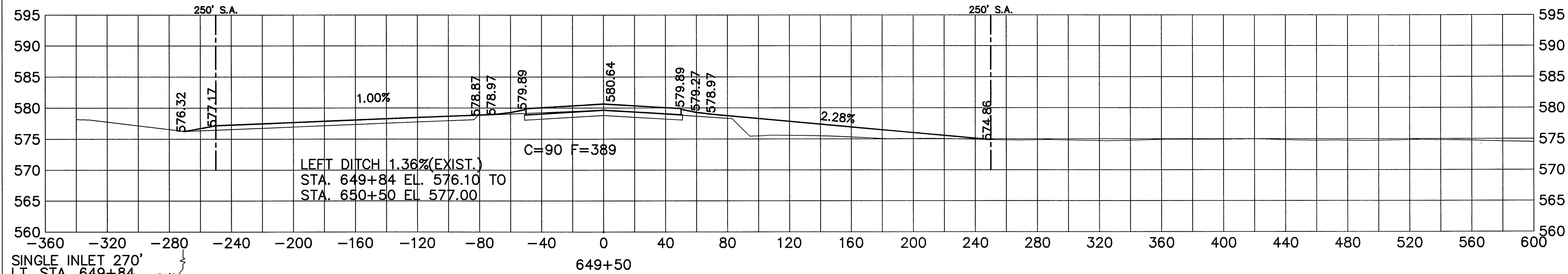
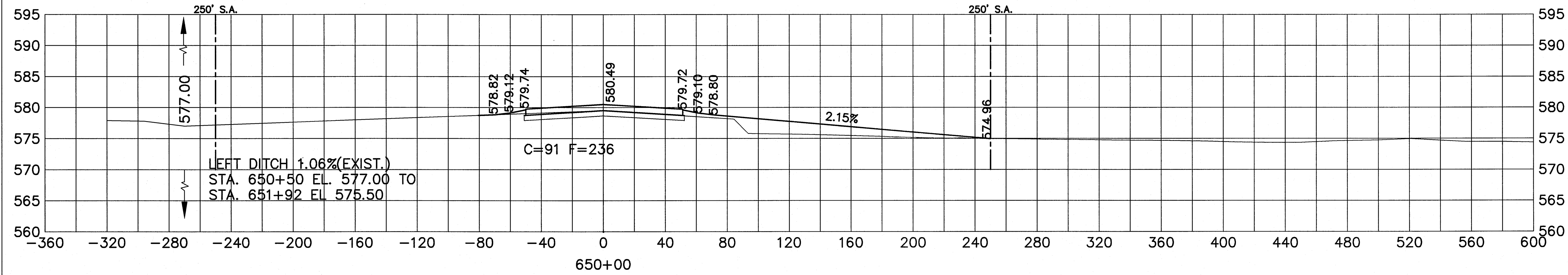
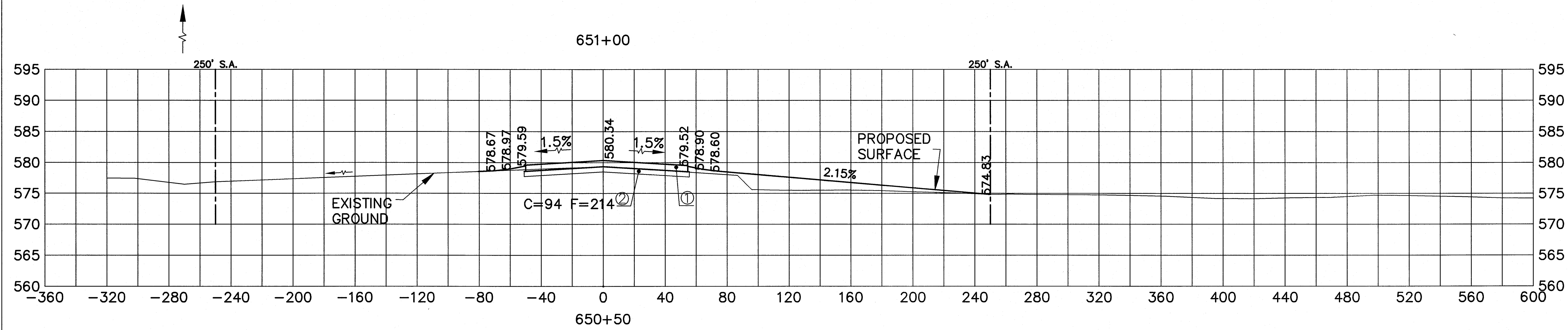
NOTE: CROSS SECTIONS ON THIS SHEET DO NOT SHOW SURCHARGE THAT WILL BE REMOVED BY THE WEST PAVING CONTRACTOR.

SCALE
 V: 1" = 10'
 H: 1" = 40'

KEY FOR EARTHWORK
 = SAND STOCKPILE REMOVAL BY THIS CONTRACTOR (INSTALLATION BY OTHERS)

G:\airport\A09T002 TP E P\MT\Sections P.dwg, 7/8/2009 10:27:53 AM, jeffm

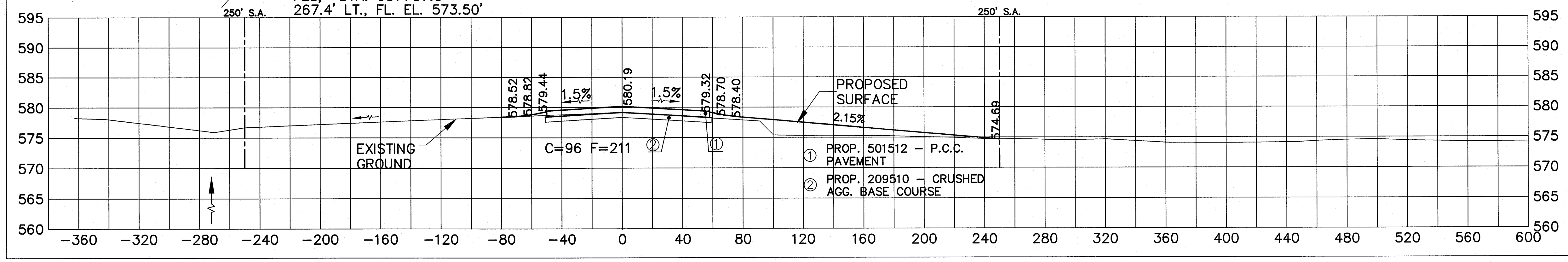
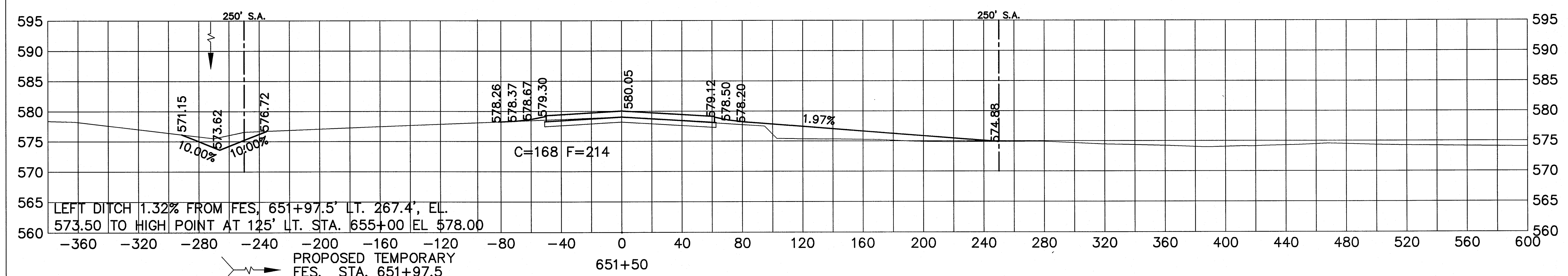
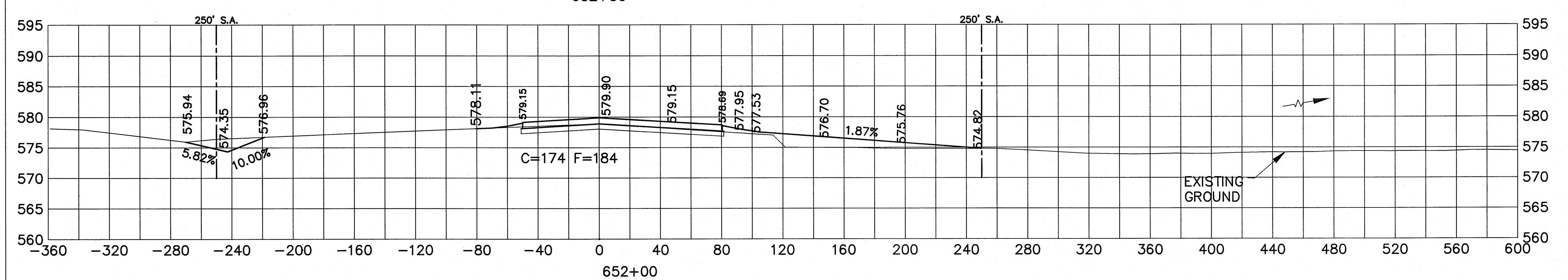
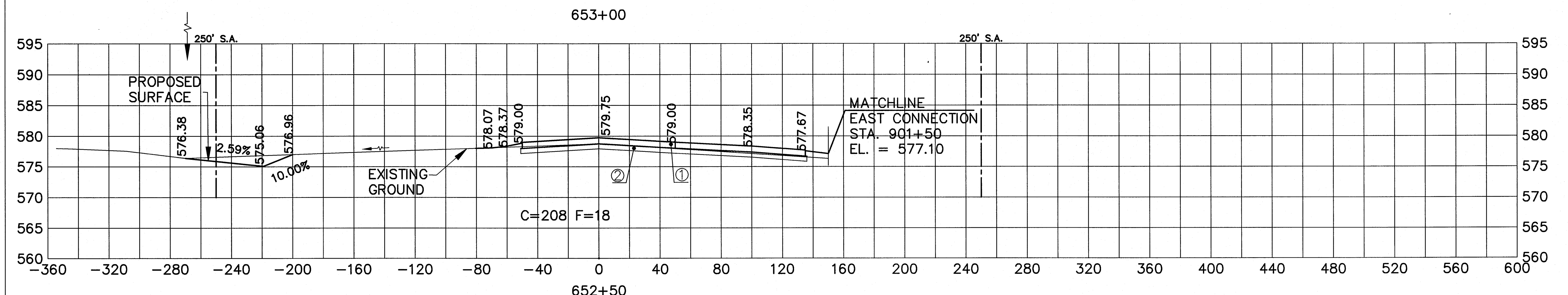
QUAD CITY INTERNATIONAL AIRPORT
 TAXIWAY P, PHASE IV - EAST PAVING
 ILL. MLI-3873, QU011
 SHEET 61 OF 74



NOTE: CROSS SECTIONS ON THIS SHEET DO NOT SHOW SURCHARGE THAT WILL BE REMOVED BY THE WEST PAVING CONTRACTOR.

SCALE
 V: 1" = 10'
 H: 1" = 40'

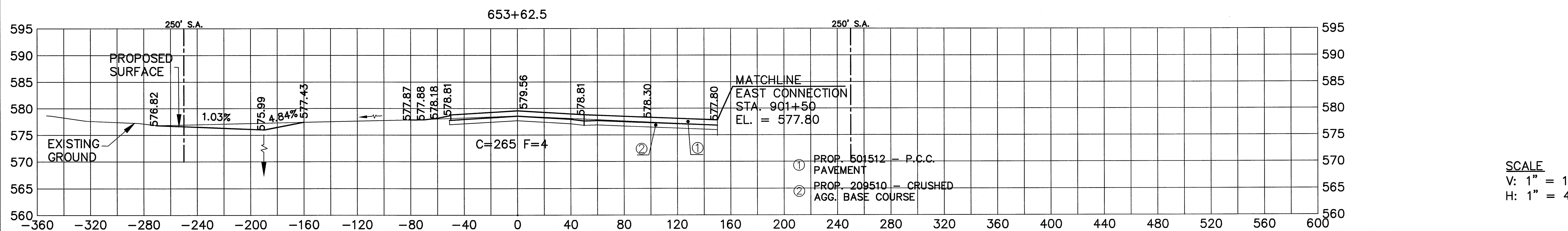
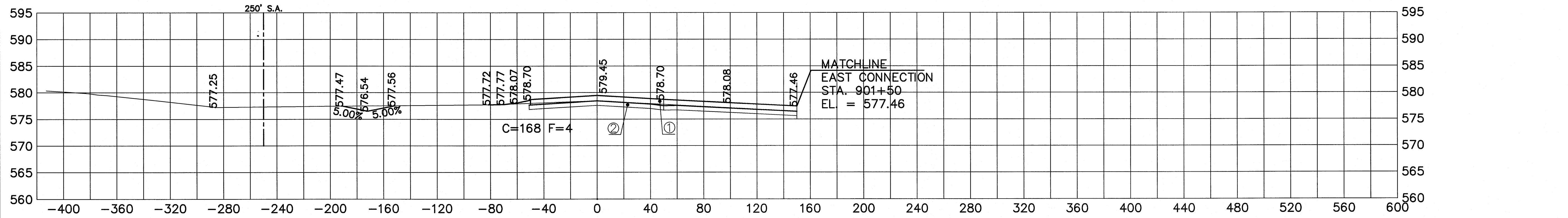
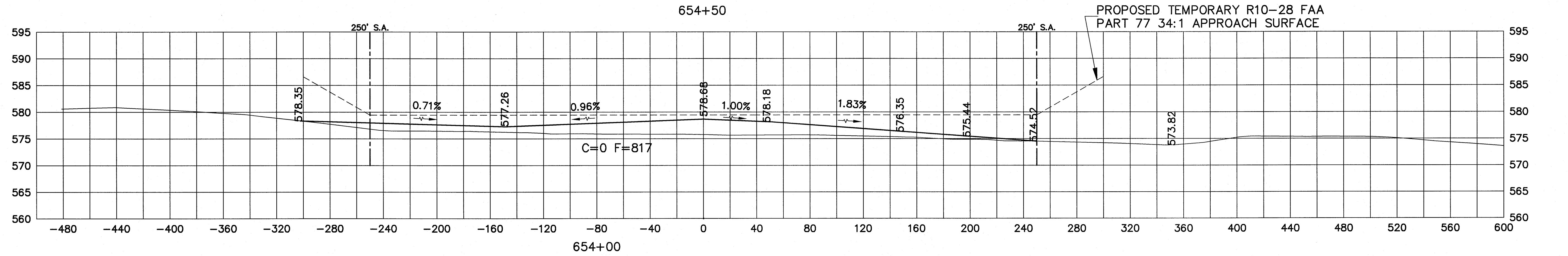
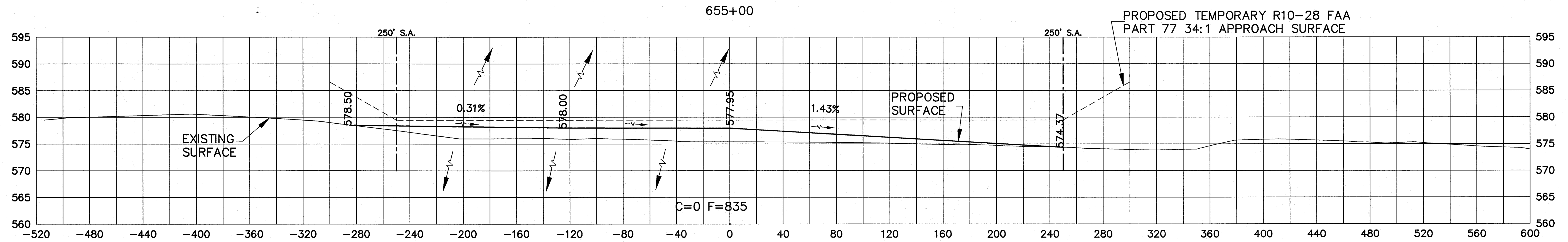
QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE IV - EAST PAVING
ILL. MLI-3873, QU011
SHEET 62 OF 74



NOTE: CROSS SECTIONS ON THIS SHEET DO NOT SHOW SURCHARGE THAT WILL BE REMOVED BY THE WEST PAVING CONTRACTOR.

SCALE
V: 1" = 10'
H: 1" = 40'

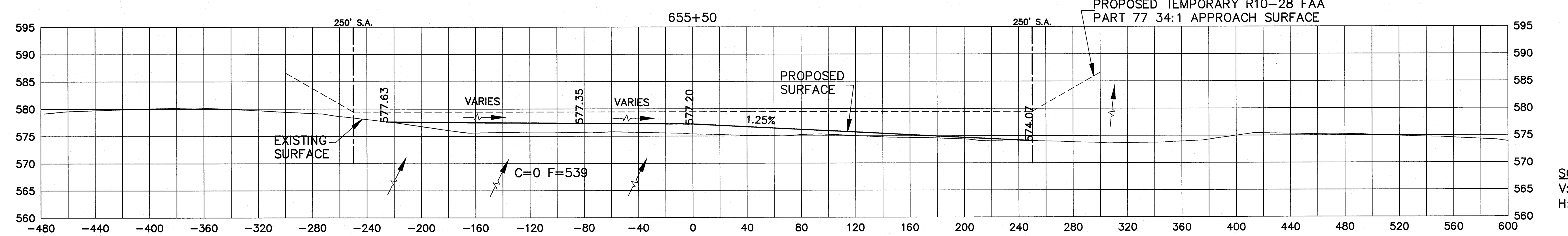
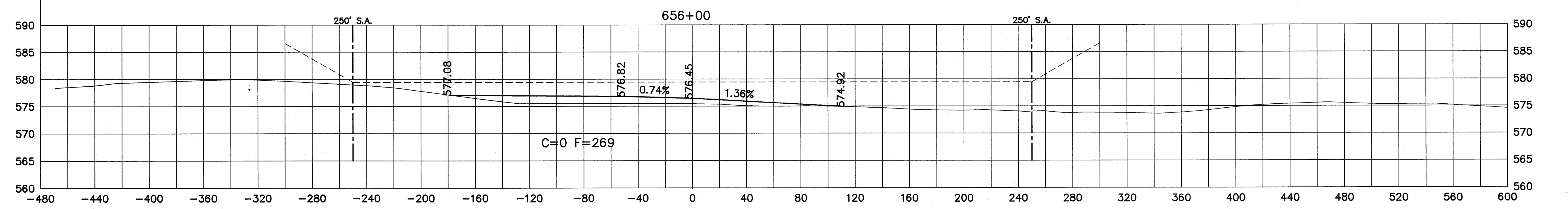
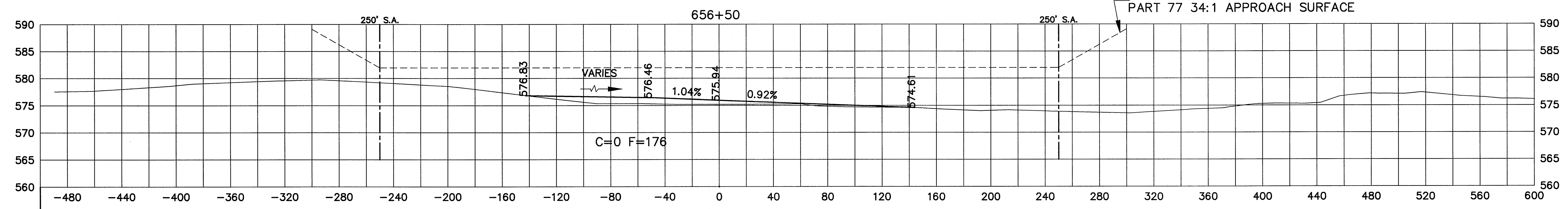
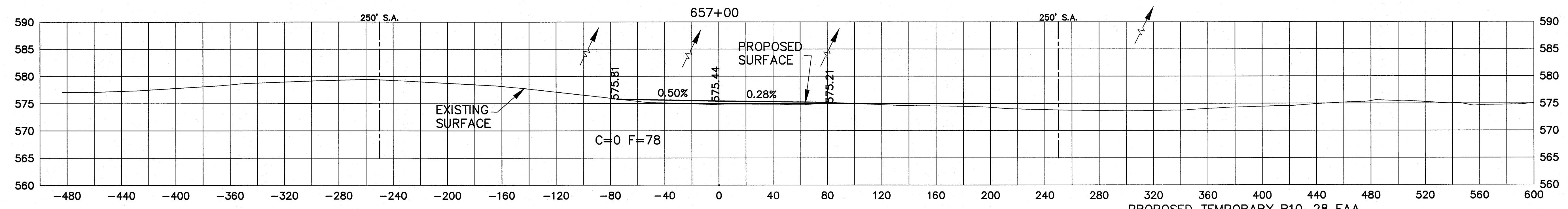
G:\AIRPORT\A09T002 TP E P\MT\Sections P.dwg, 7/8/2009 10:28:08 AM, jeffm



- ① PROP. 501512 - P.C.C. PAVEMENT
- ② PROP. 209510 - CRUSHED AGG. BASE COURSE

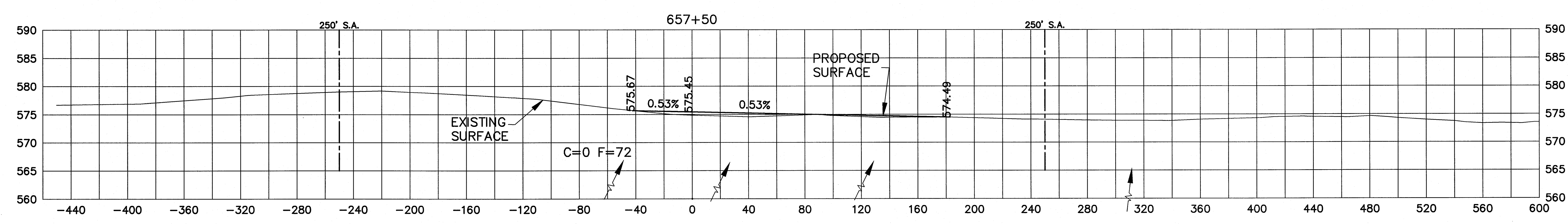
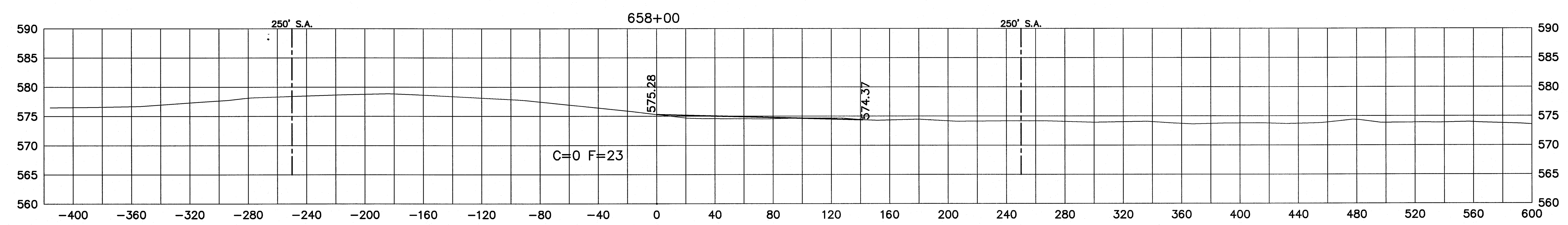
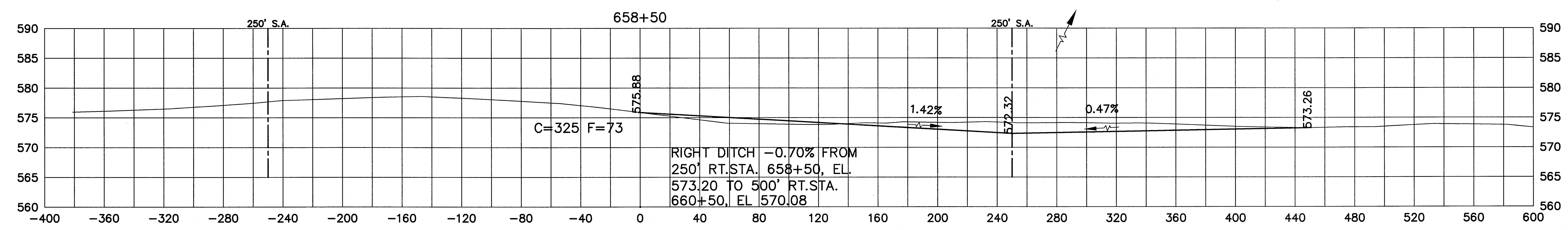
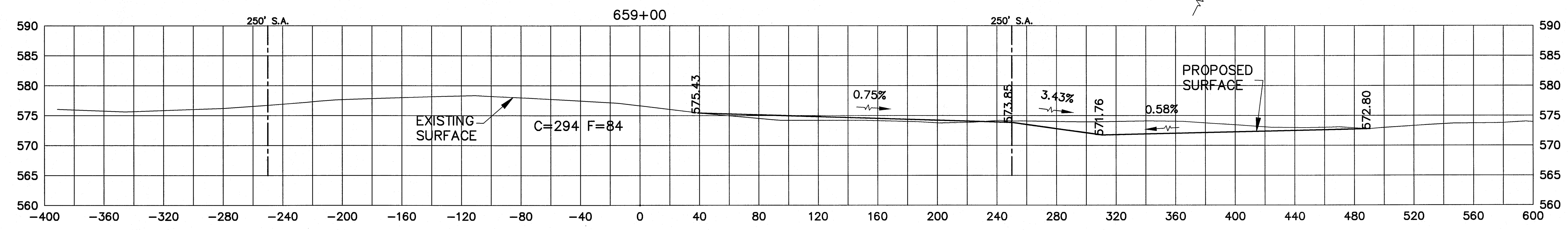
SCALE
V: 1" = 10'
H: 1" = 40'

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE IV - EAST PAVING
ILL. MLI-3873, QU011
SHEET 64 OF 74



SCALE
V: 1" = 10'
H: 1" = 40'

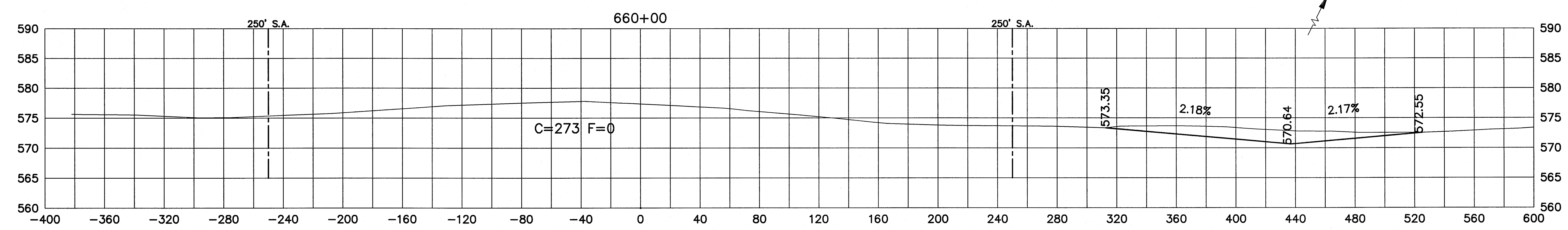
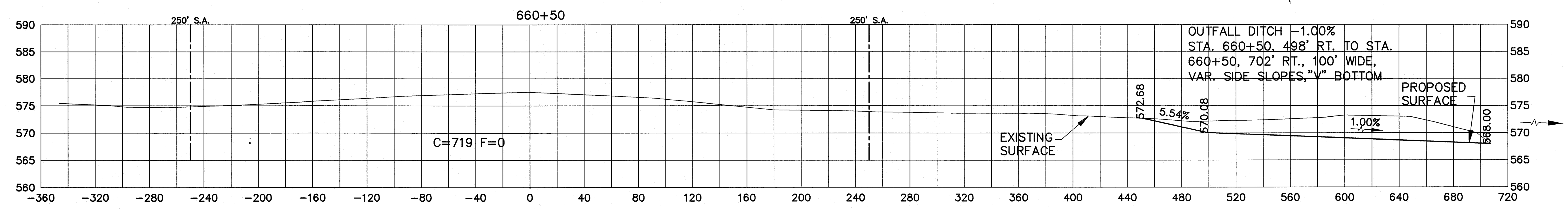
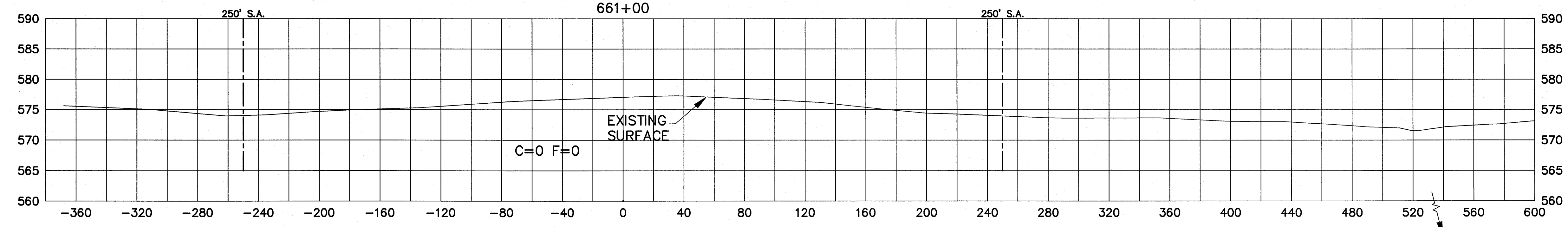
QUAD CITY INTERNATIONAL AIRPORT
 TAXIWAY P, PHASE IV - EAST PAVING
 ILL. MLI-3873, QU011
 SHEET 65 OF 74



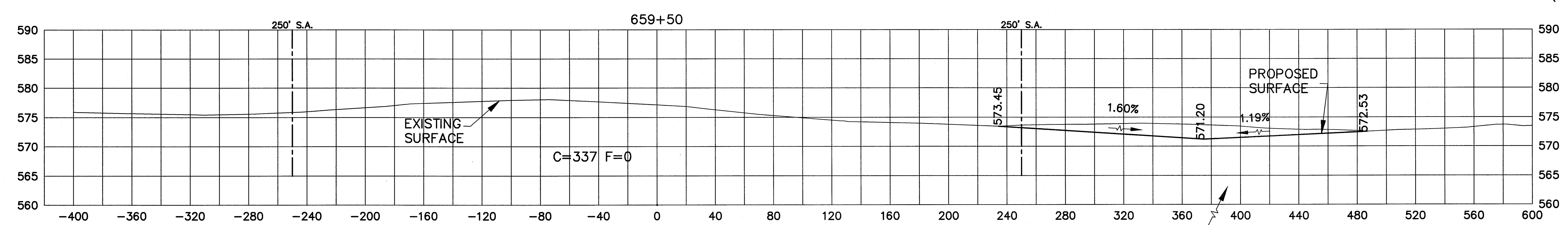
SCALE
 V: 1" = 10'
 H: 1" = 40'

G:\airport\A09T002 TP E P\MT\Sections P.dwg, 7/8/2009 10:28:39 AM, jeffm

END TAXIWAY P EARTHWORK, STA. 661+00

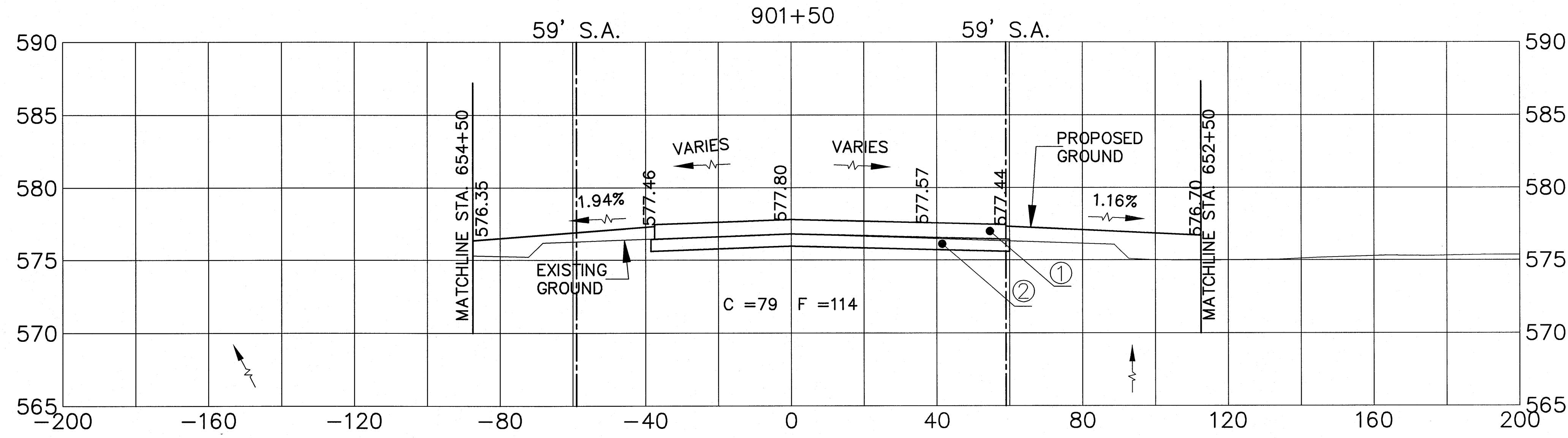
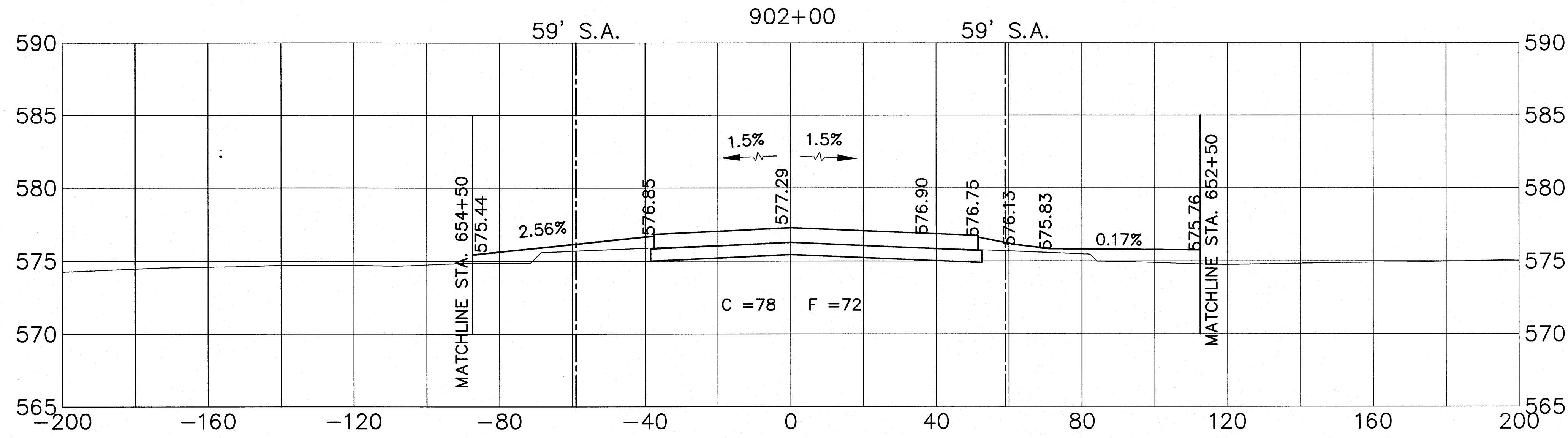
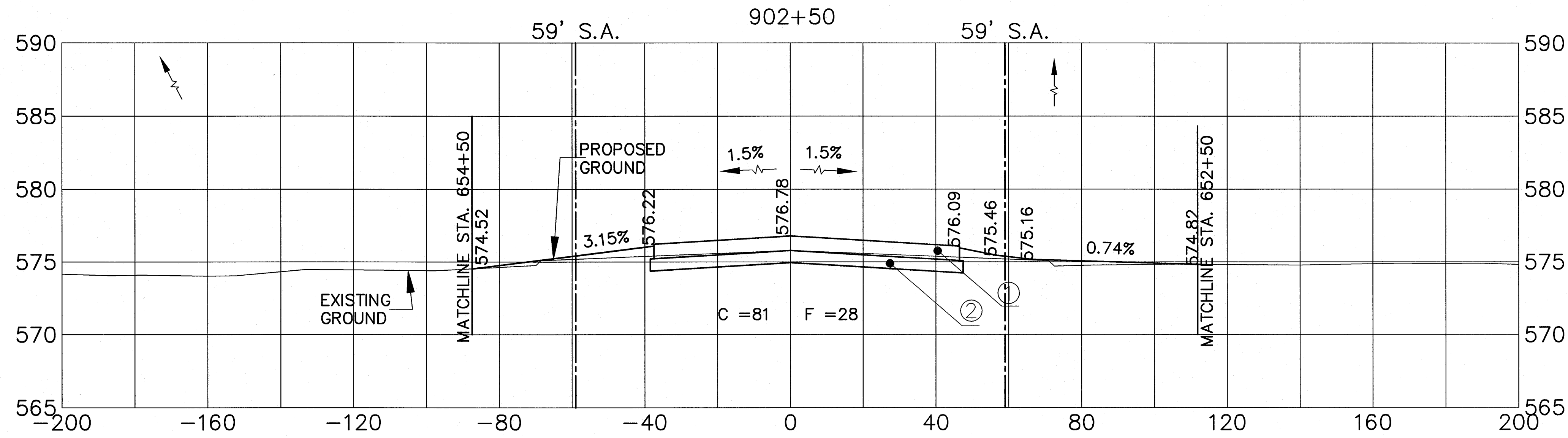


FILL OVER PROPOSED PIPE
STA. 659+95, 675' RT. TO STA.
660+00, 705' RT., = 3 C.Y.
(SEE DRAINAGE SHEET 32)



SCALE
V: 1" = 10'
H: 1" = 40'

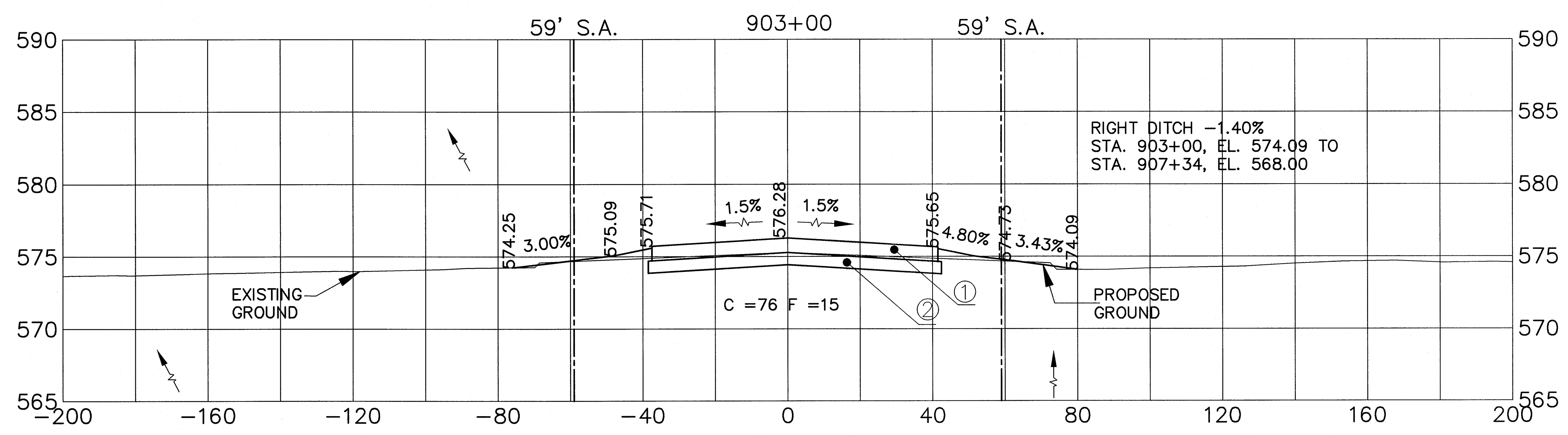
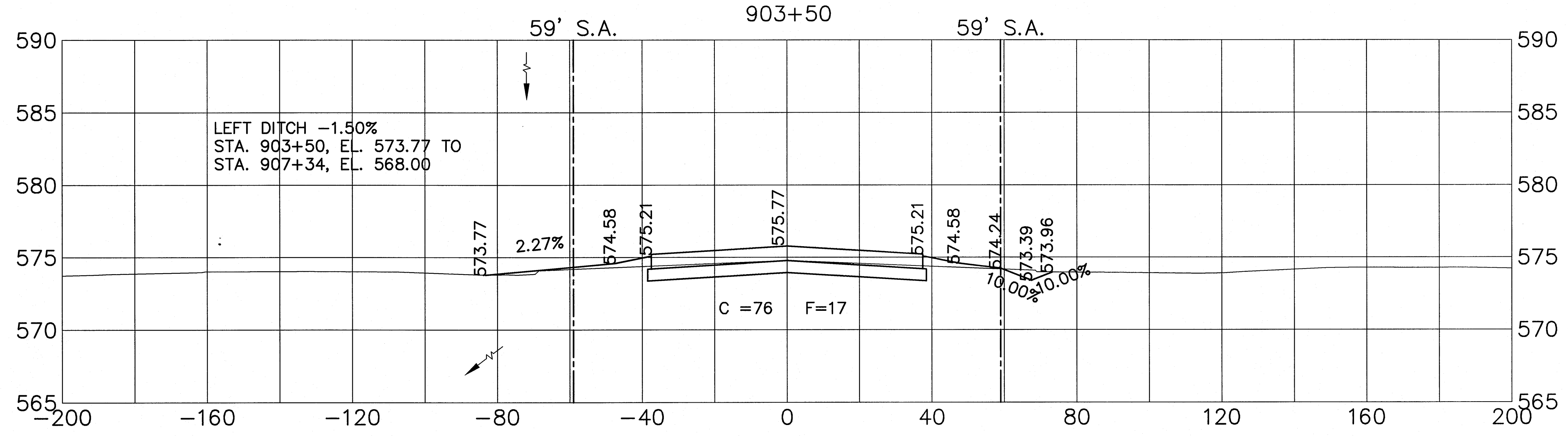
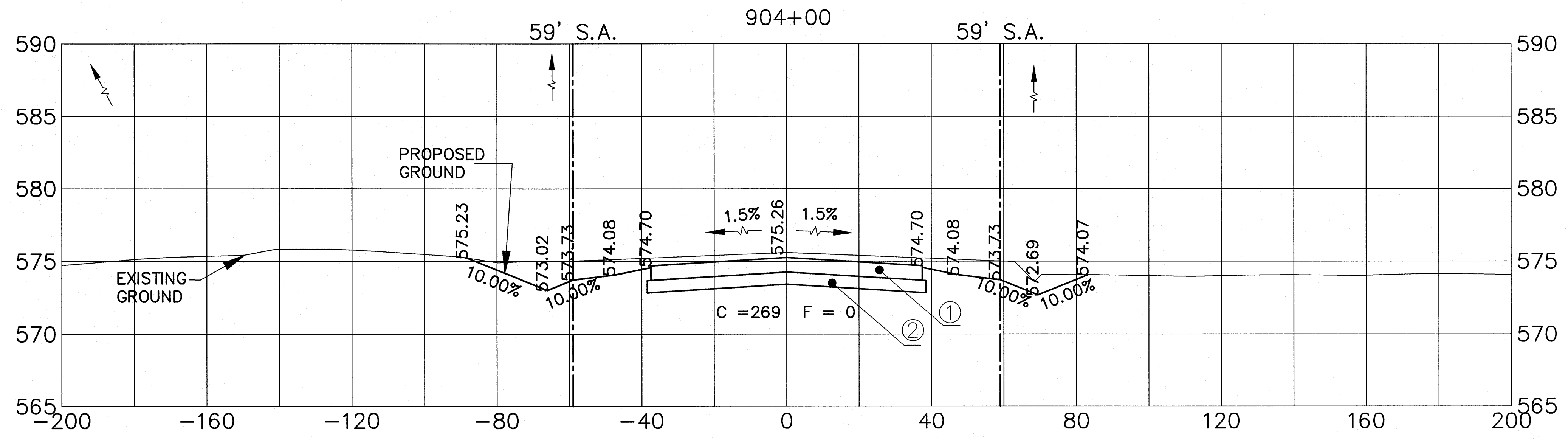
G:\airport\A09T002 TP E P\MT\Sections P.dwg, 7/8/2009 10:28:48 AM, jefm



NOTE: CROSS SECTIONS ON THIS SHEET DO NOT SHOW SURCHARGE THAT WILL BE REMOVED BY THE WEST PAVING CONTRACTOR.

- SCALE
 V: 1"=5'
 H: 1"=20'
- ① PROP. 501512 - P.C.C. PAVEMENT
 - ② PROP. 209510 - CRUSHED AGG. BASE COURSE

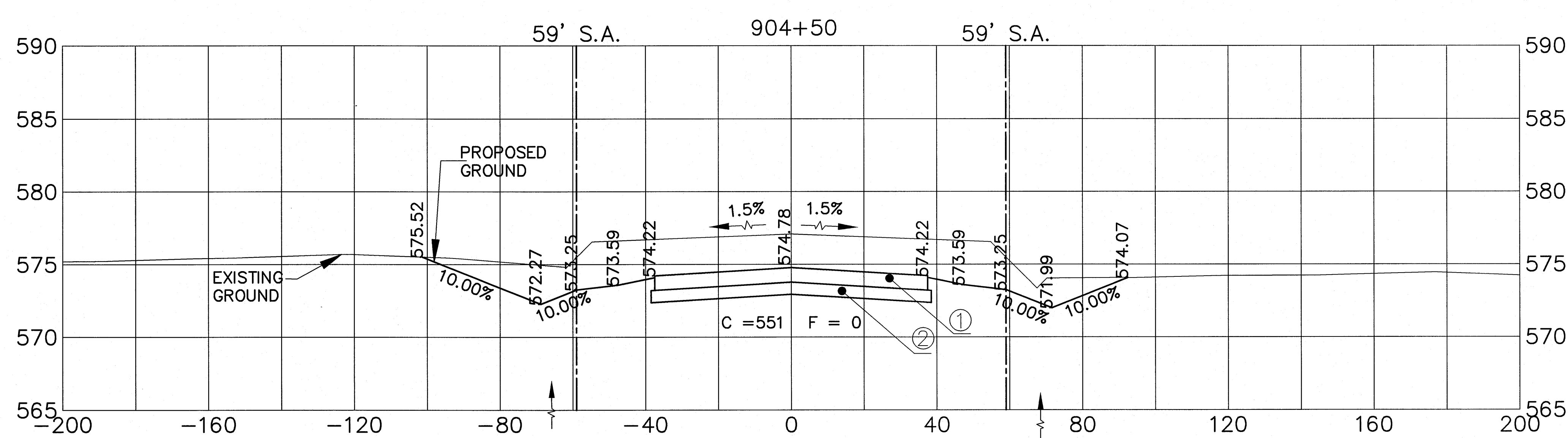
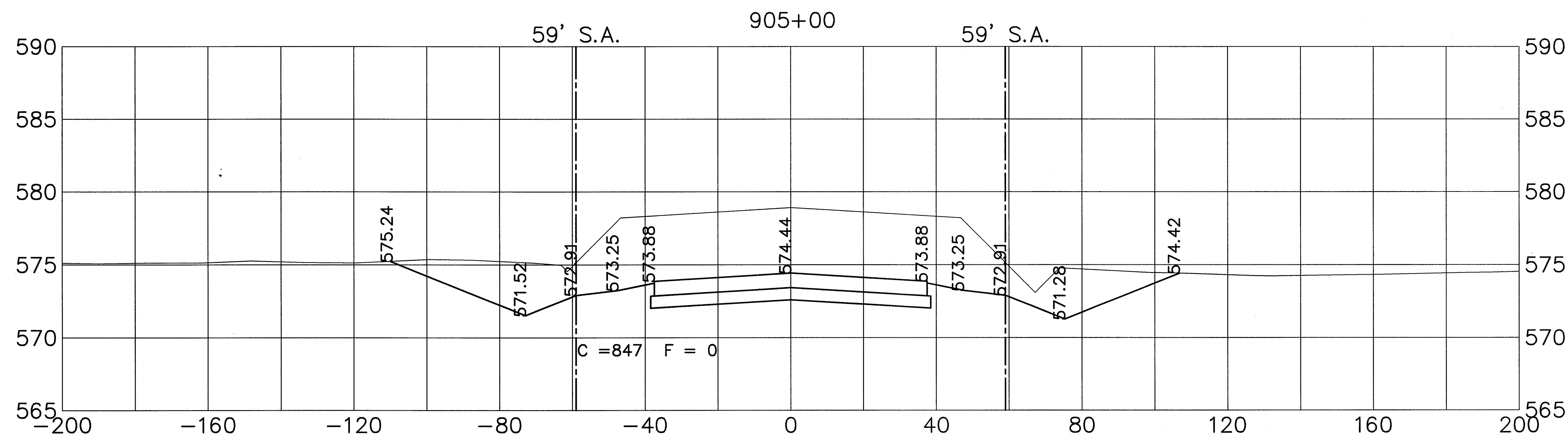
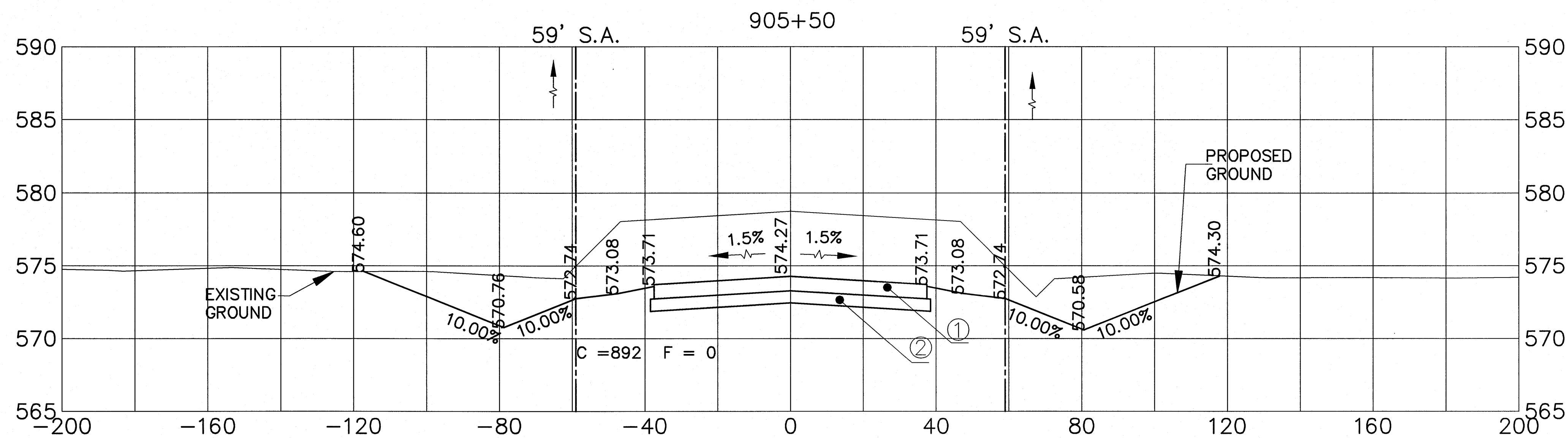
G:\Airport\A09T002 TP E PVMT\Sections P2.dwg, 7/8/2009 10:29:00 AM, jefm



NOTE: CROSS SECTIONS ON THIS SHEET DO NOT SHOW SURCHARGE THAT WILL BE REMOVED BY THE WEST PAVING CONTRACTOR.

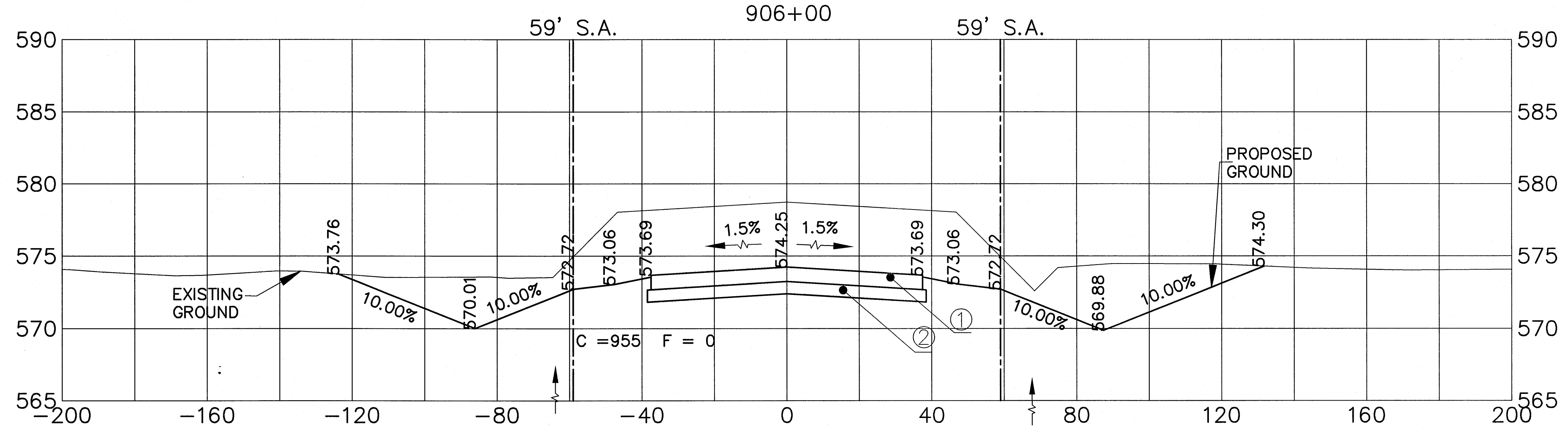
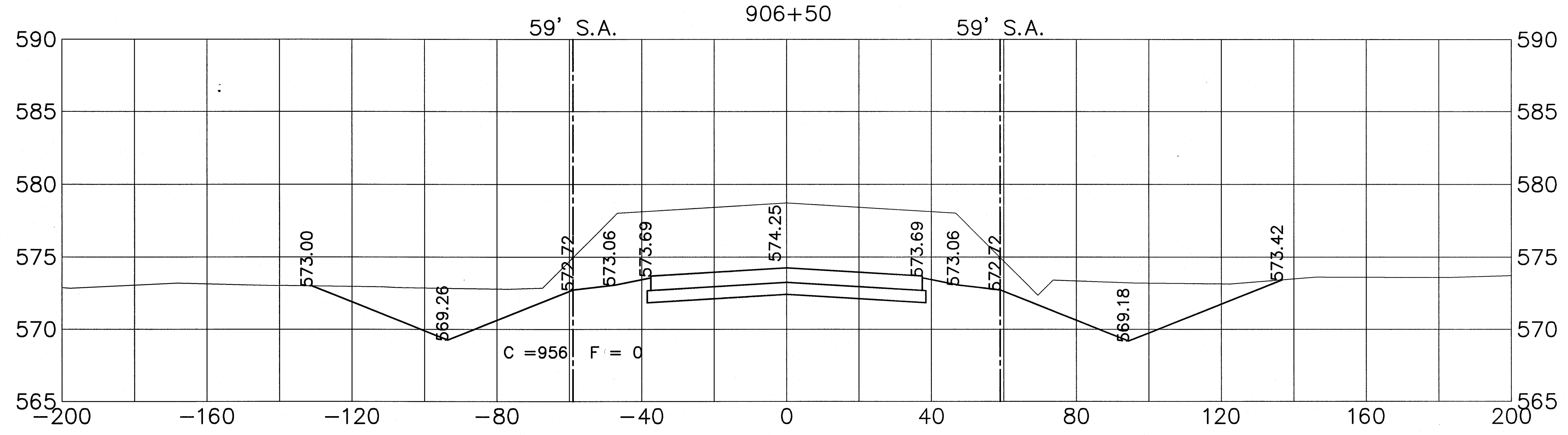
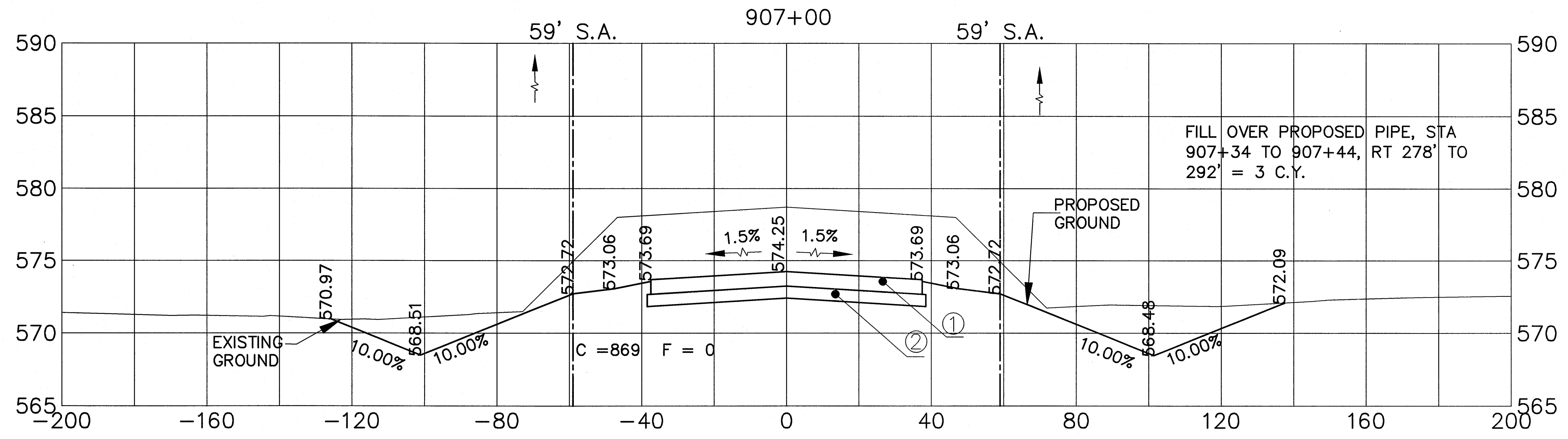
- SCALE
 V: 1"=5'
 H: 1"=20'
- ① PROP. 501512 - P.C.C. PAVEMENT
 - ② PROP. 209510 - CRUSHED AGG. BASE COURSE

G:\airport\A09T002 TP E PVMT\Sections P2.dwg, 7/8/2009 10:29:07 AM, jefrn



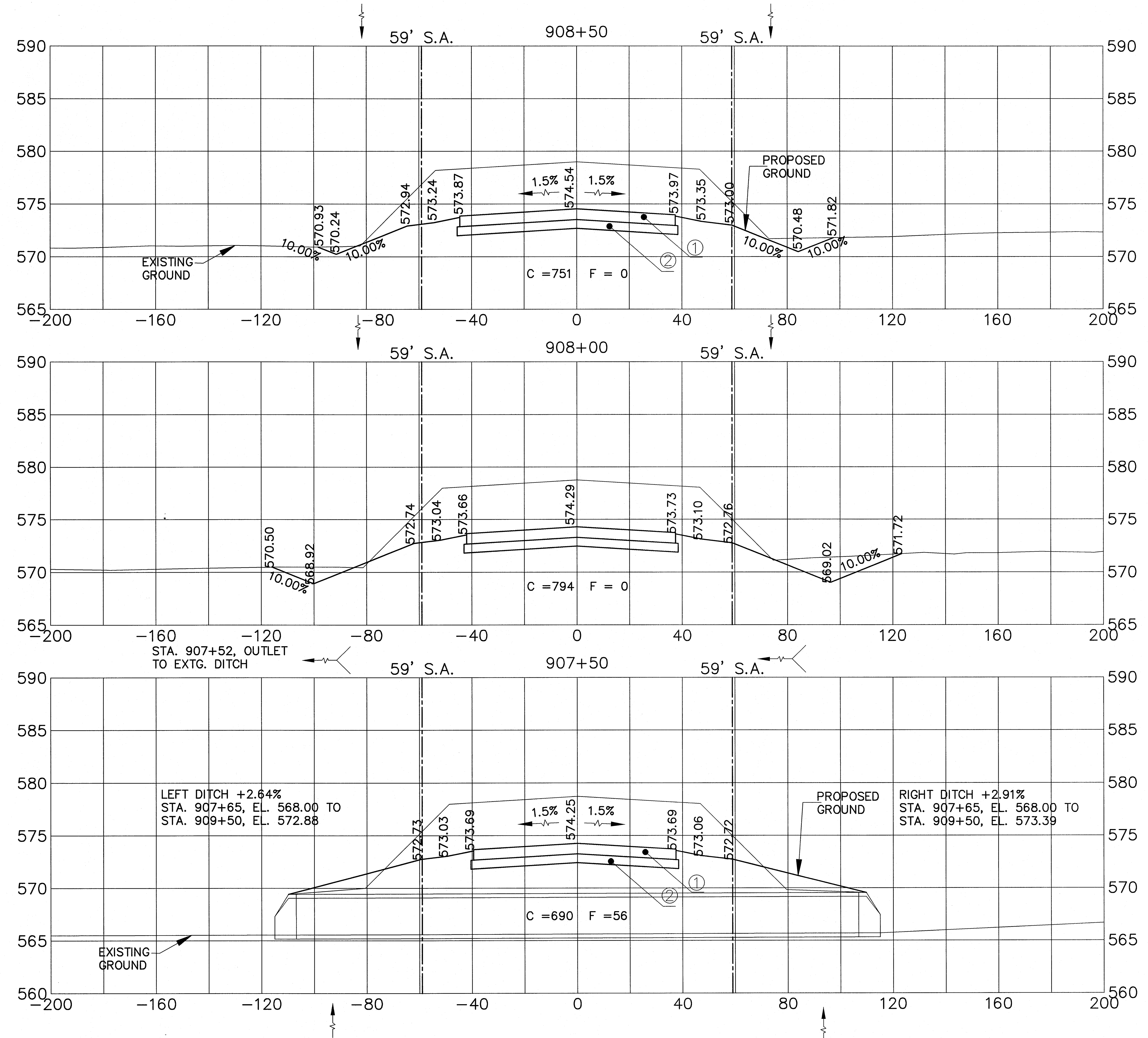
SCALE
 V: 1"=5'
 H: 1"=20'

- ① PROP. 501512 - P.C.C. PAVEMENT
- ② PROP. 209510 - CRUSHED AGG. BASE COURSE



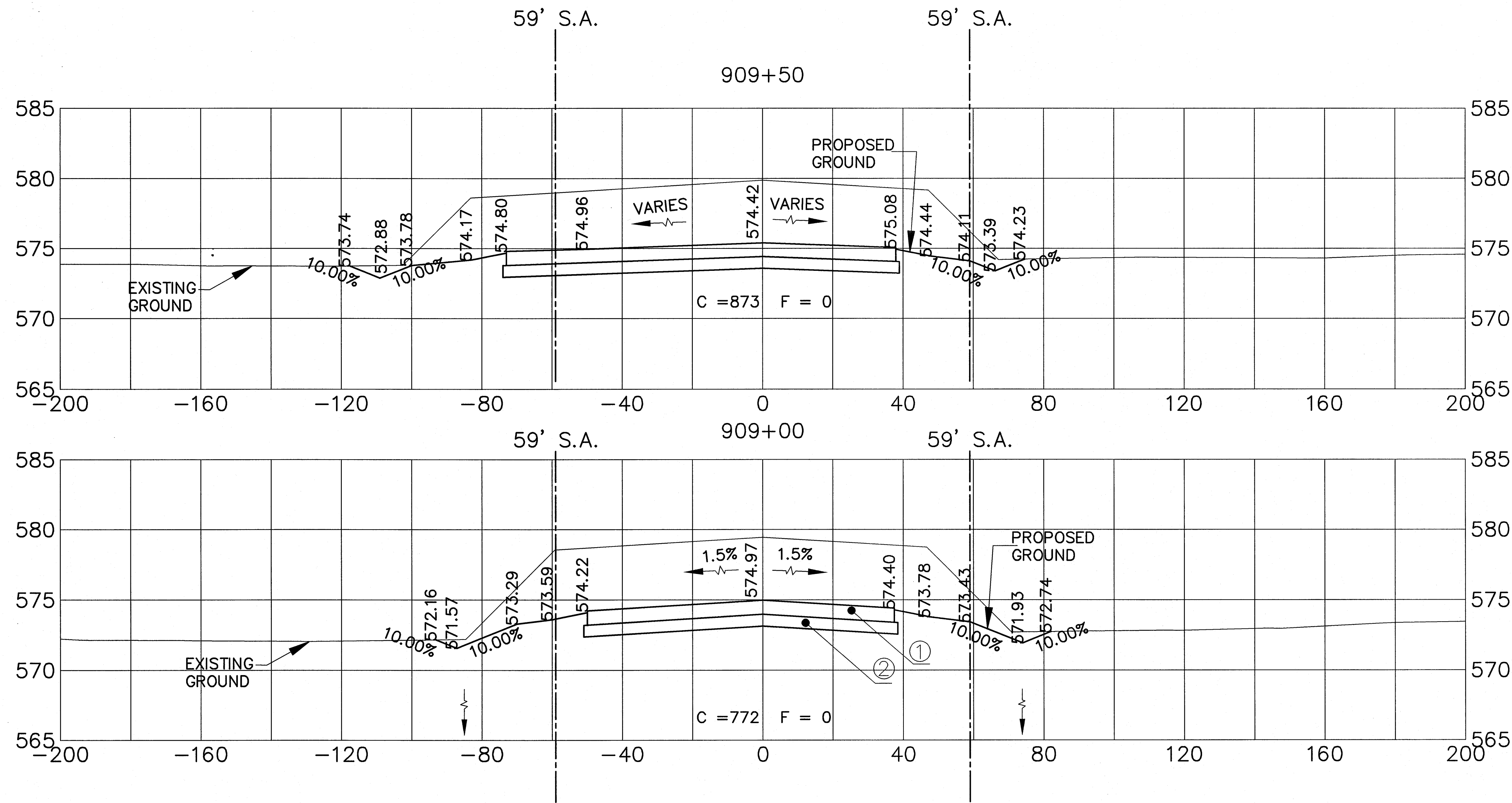
- SCALE
 V: 1"=5'
 H: 1"=20'
- ① PROP. 501512 - P.C.C. PAVEMENT
 - ② PROP. 209510 - CRUSHED AGG. BASE COURSE

G:\Airport\A09T002 TP E PVMT\Sections P2.dwg, 7/8/2009 10:29:27 AM, jeffm



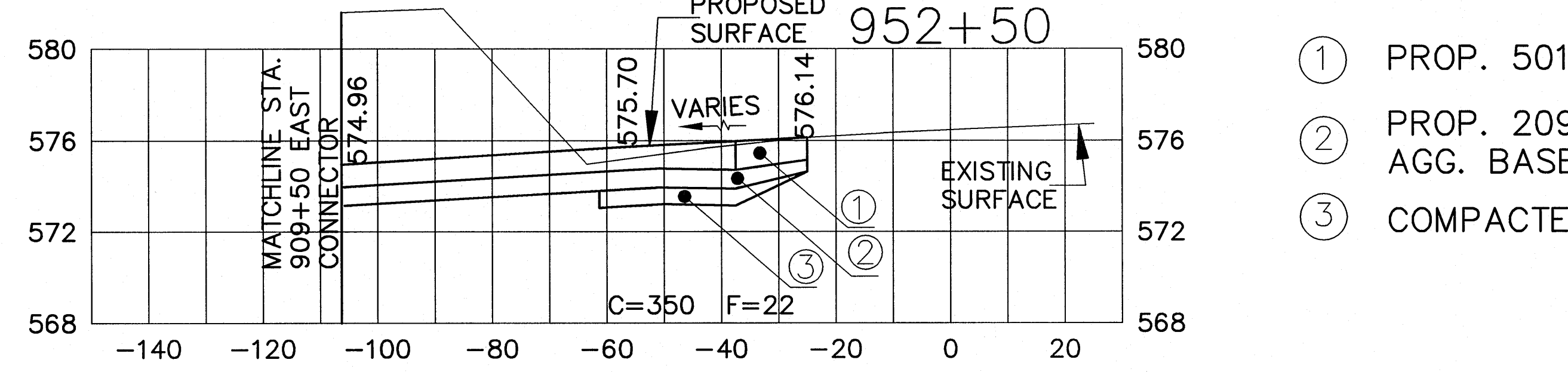
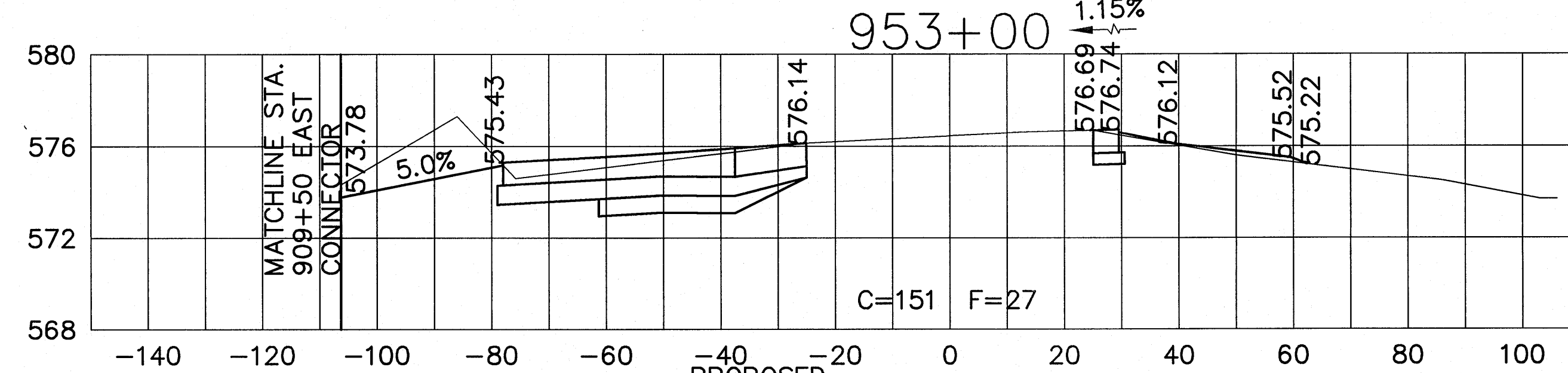
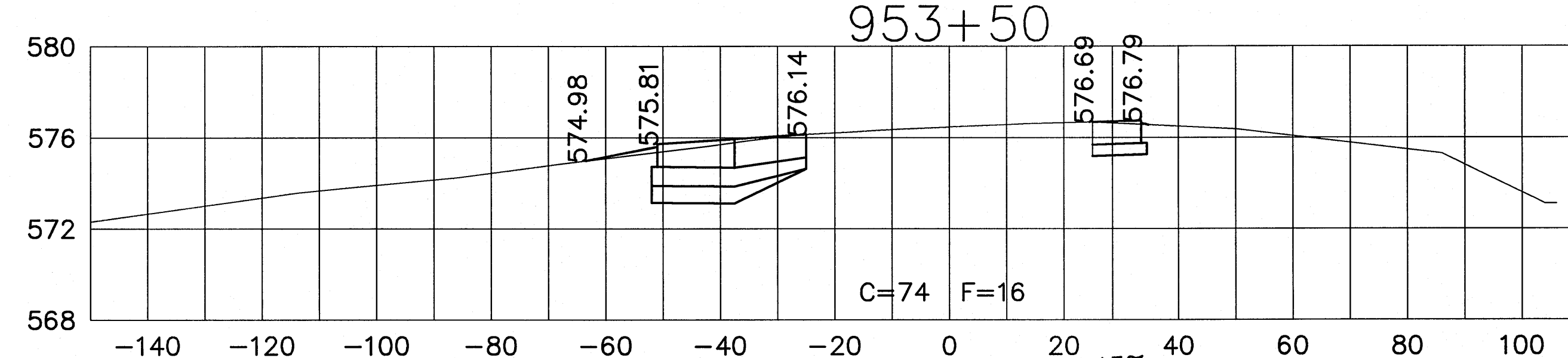
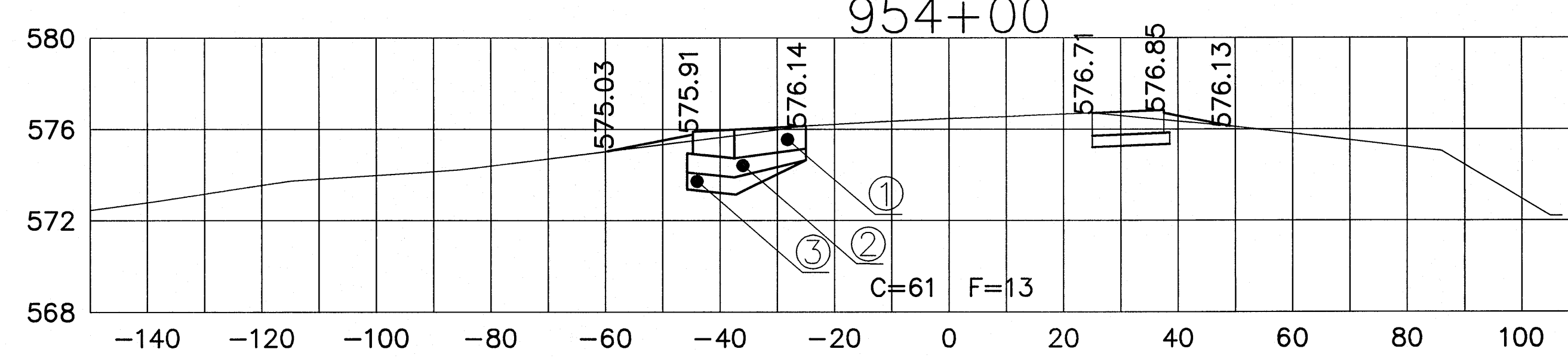
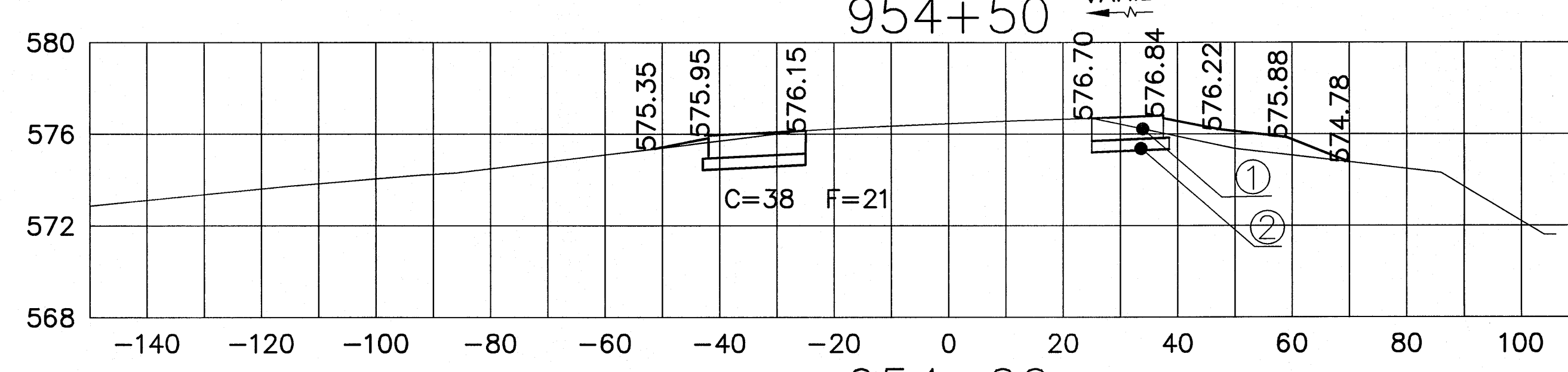
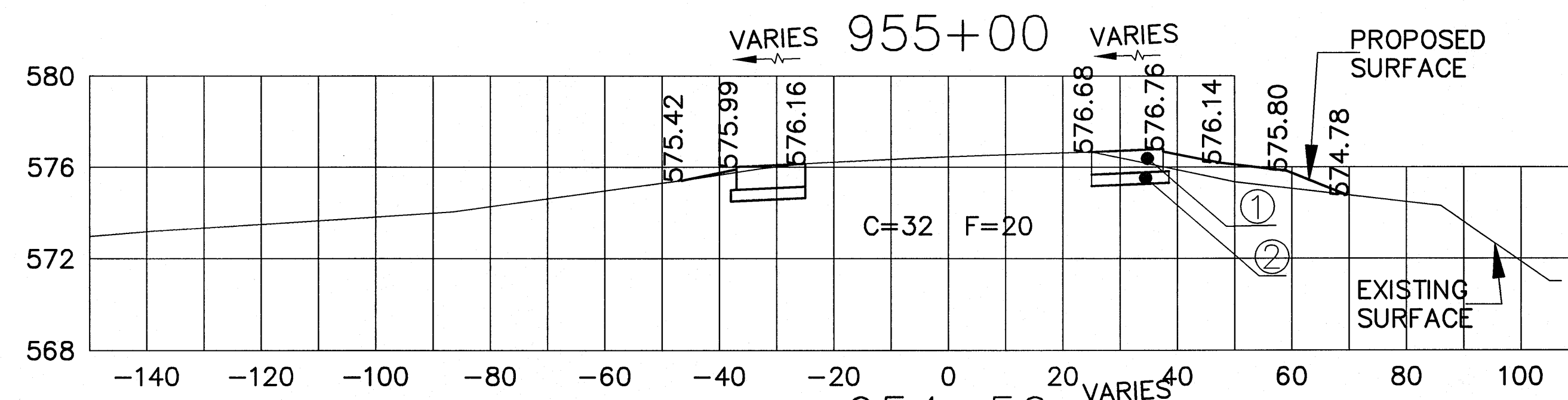
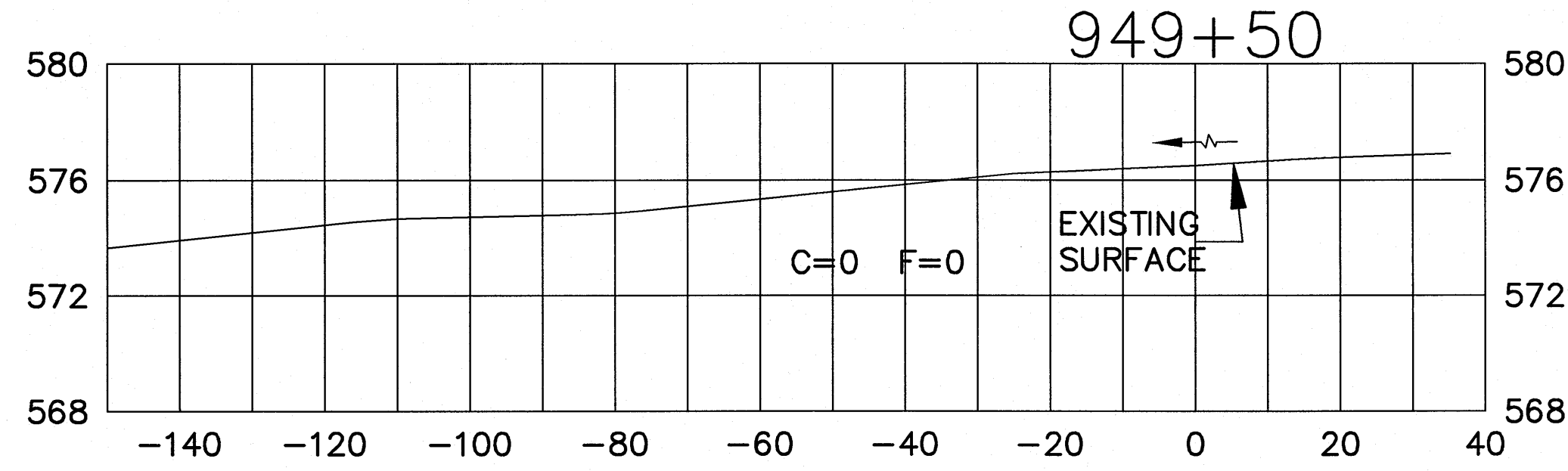
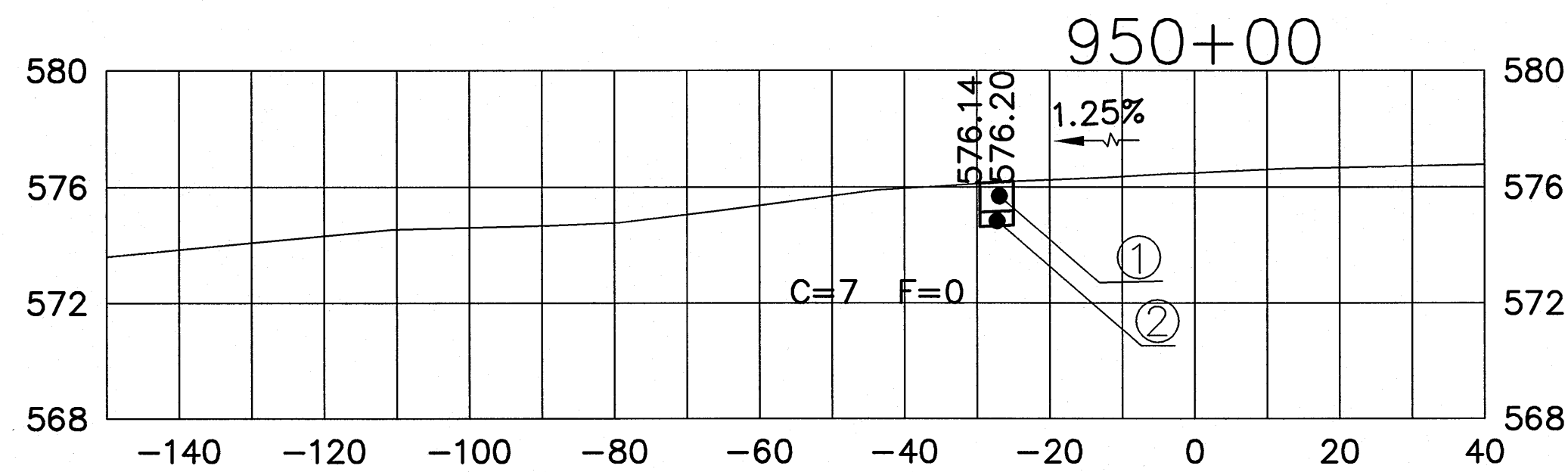
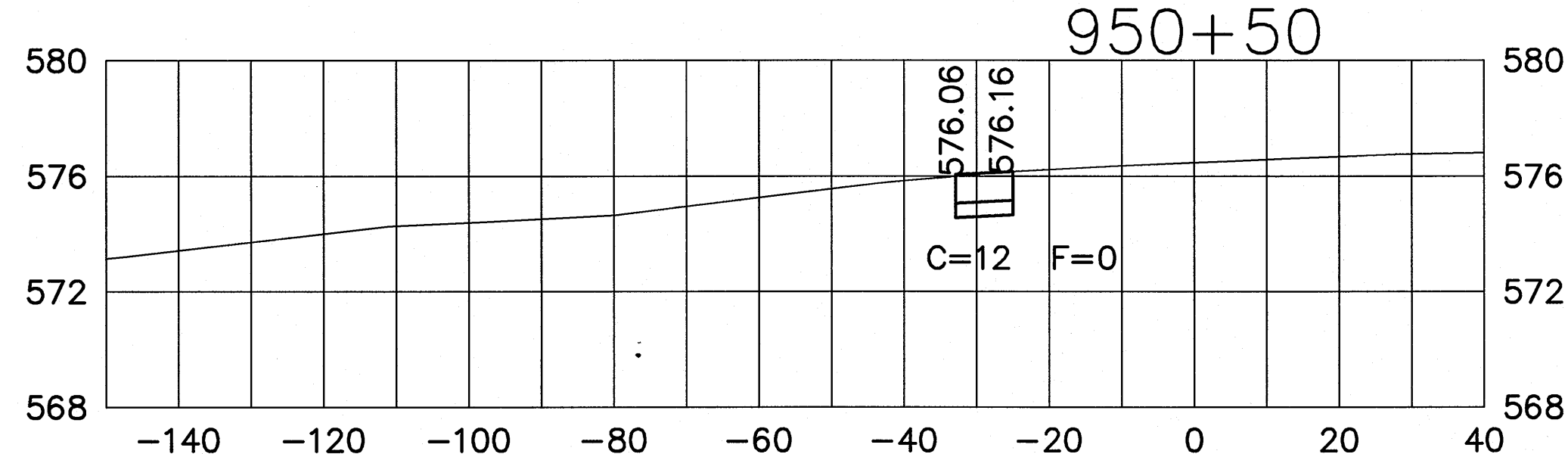
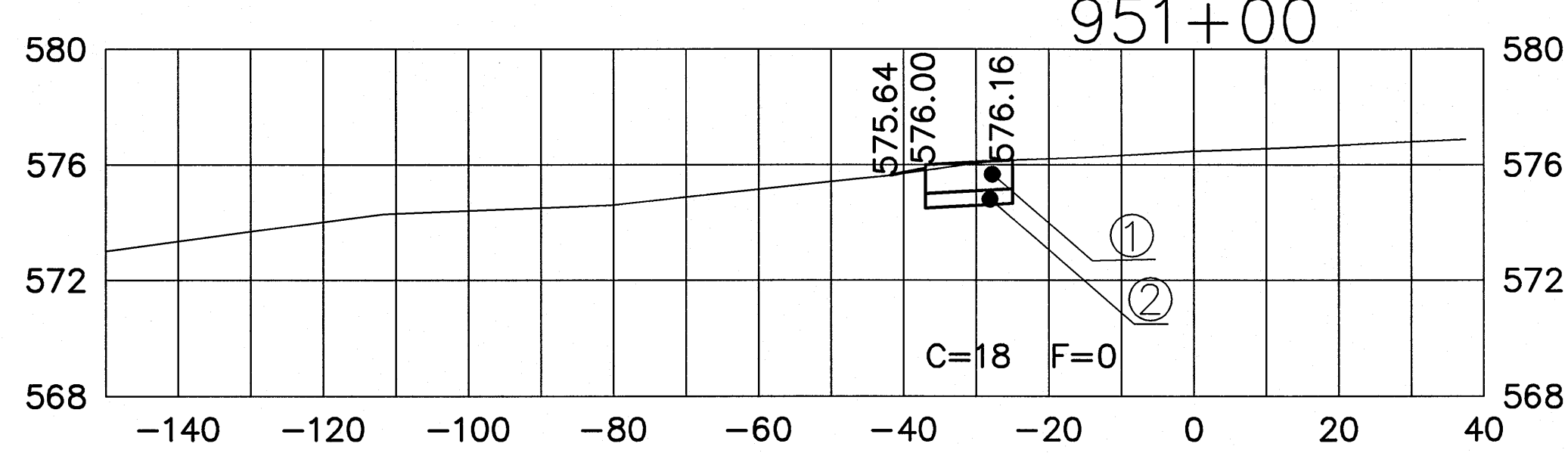
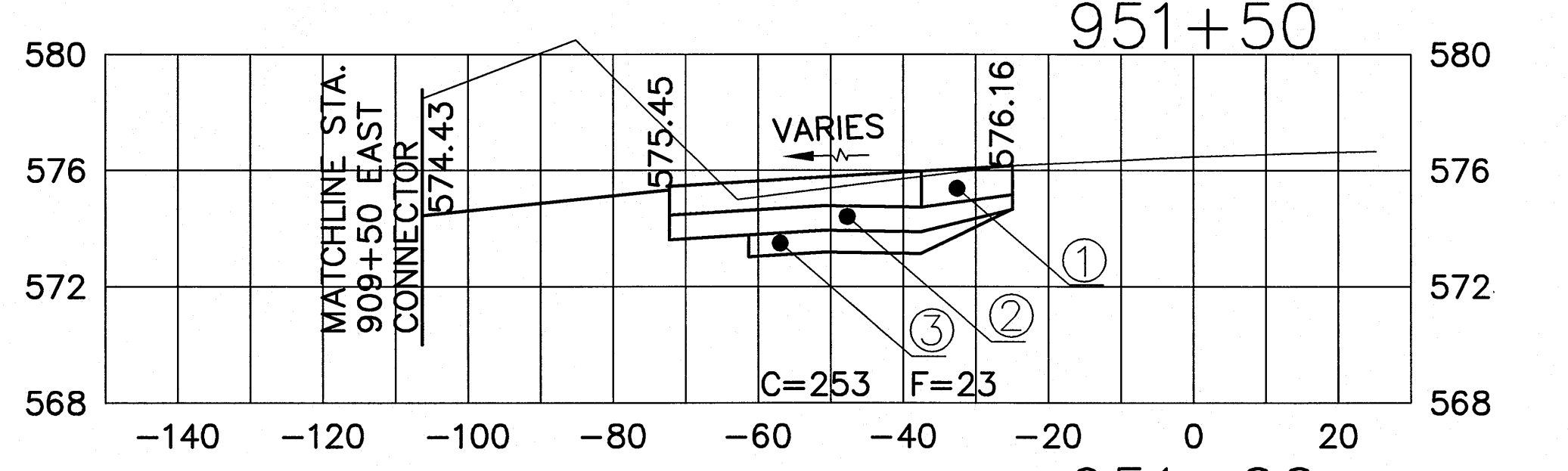
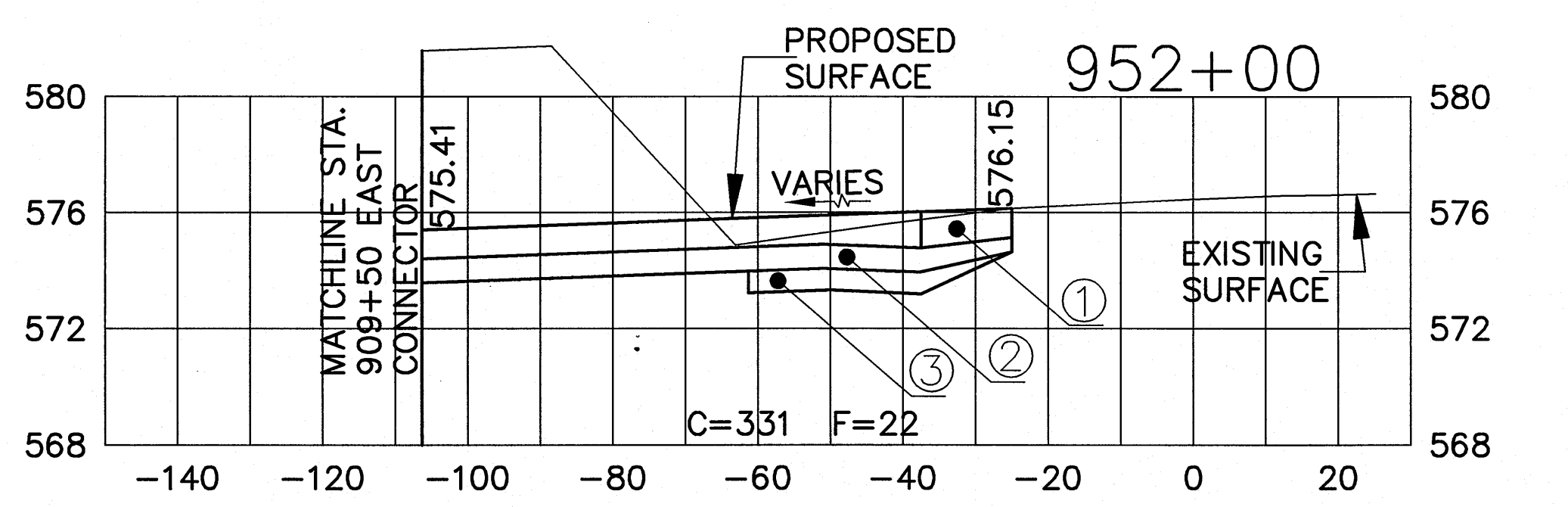
- SCALE**
 V: 1"=5'
 H: 1"=20'
- ① PROP. 501512 - P.C.C. PAVEMENT
 - ② PROP. 209510 - CRUSHED AGG. BASE COURSE

G:\AIRPORT\A09T002 TP E PVMT\Sections P2.dwg, 7/8/2009 10:29:32 AM, jefrm



SCALE
 V: 1"=5'
 H: 1"=20'

- ① PROP. 501512 - P.C.C. PAVEMENT
- ② PROP. 209510 - CRUSHED AGG. BASE COURSE



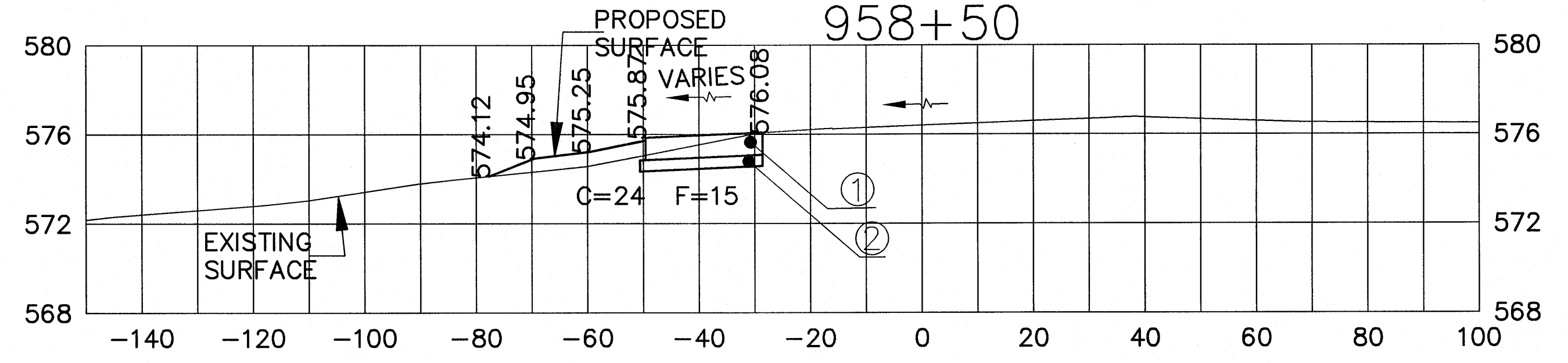
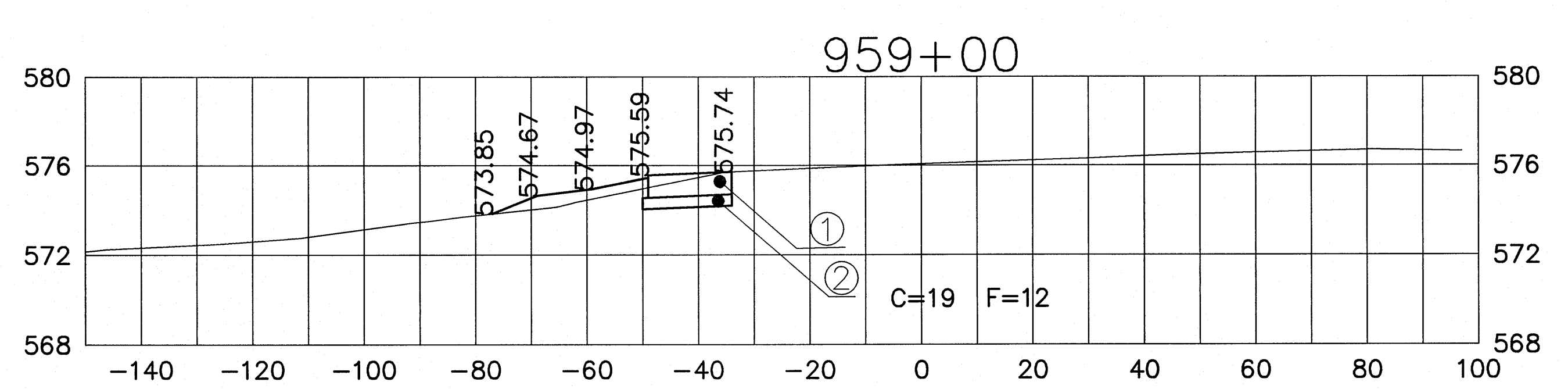
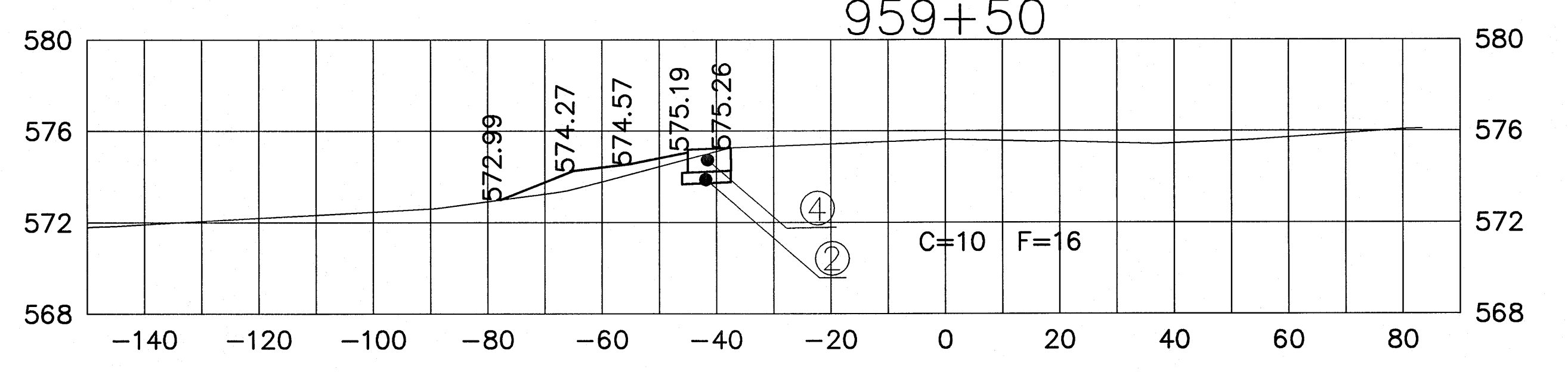
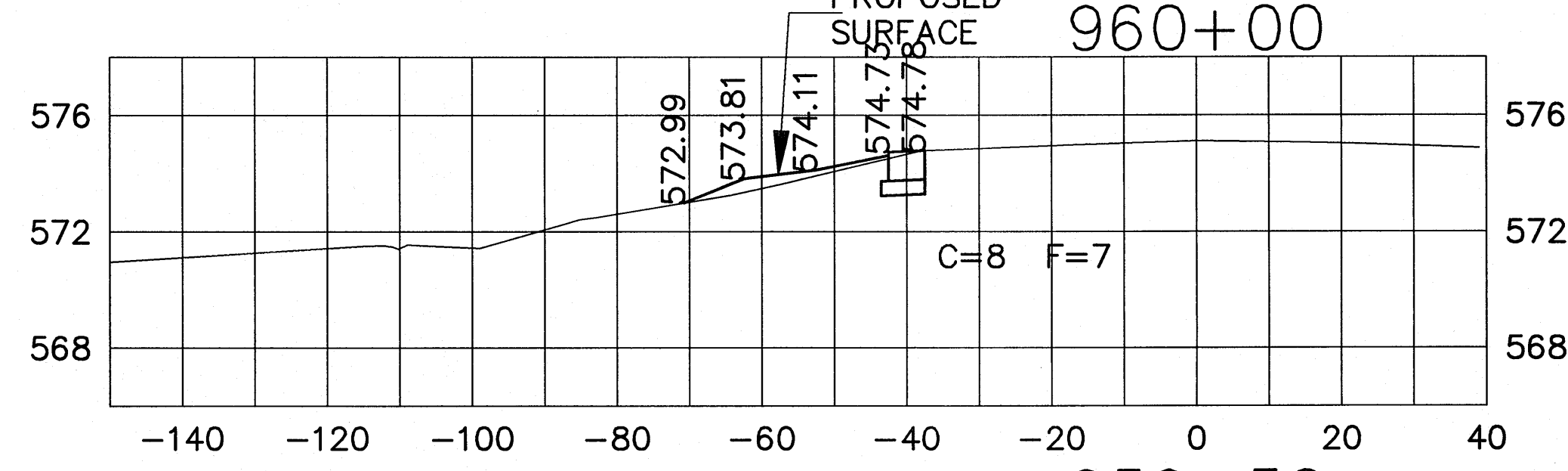
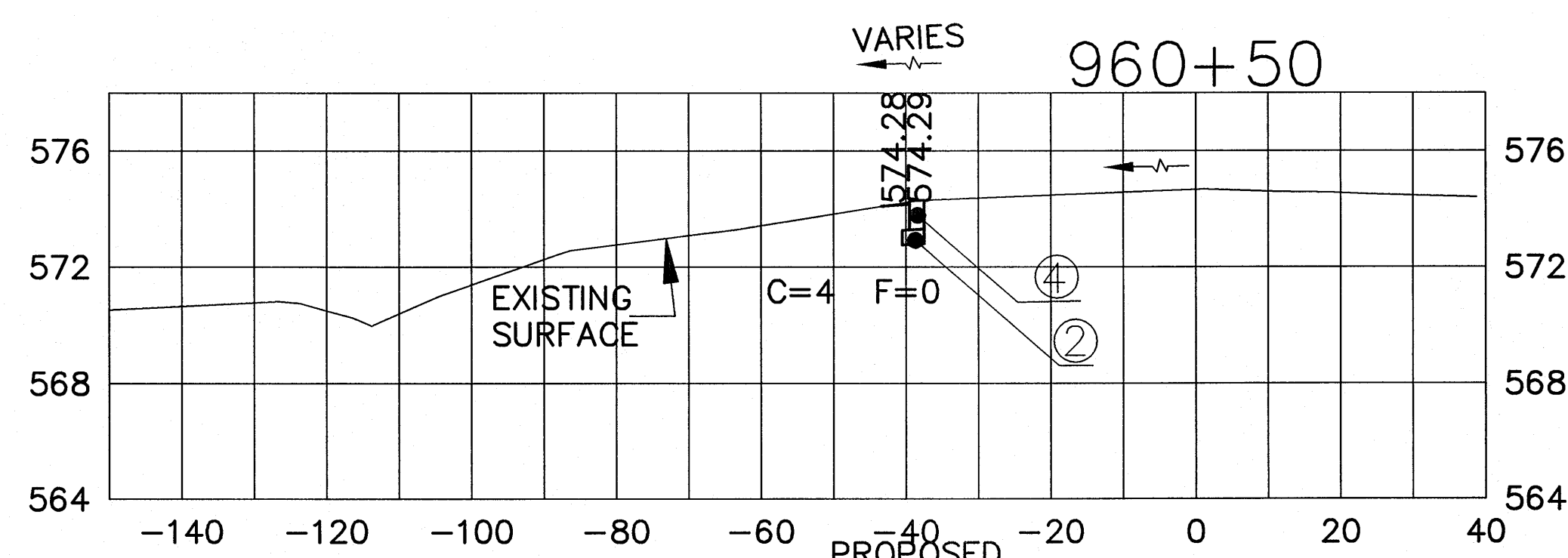
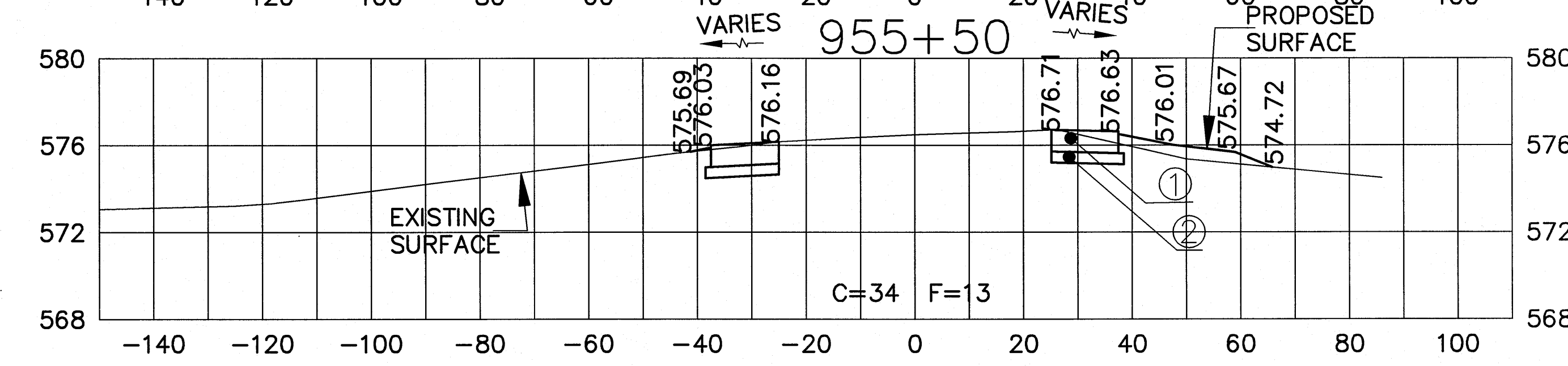
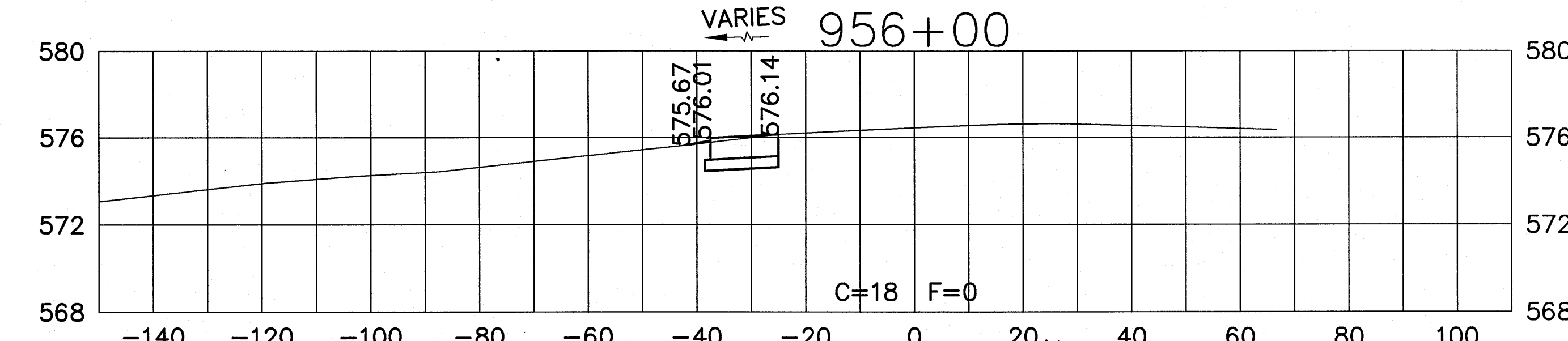
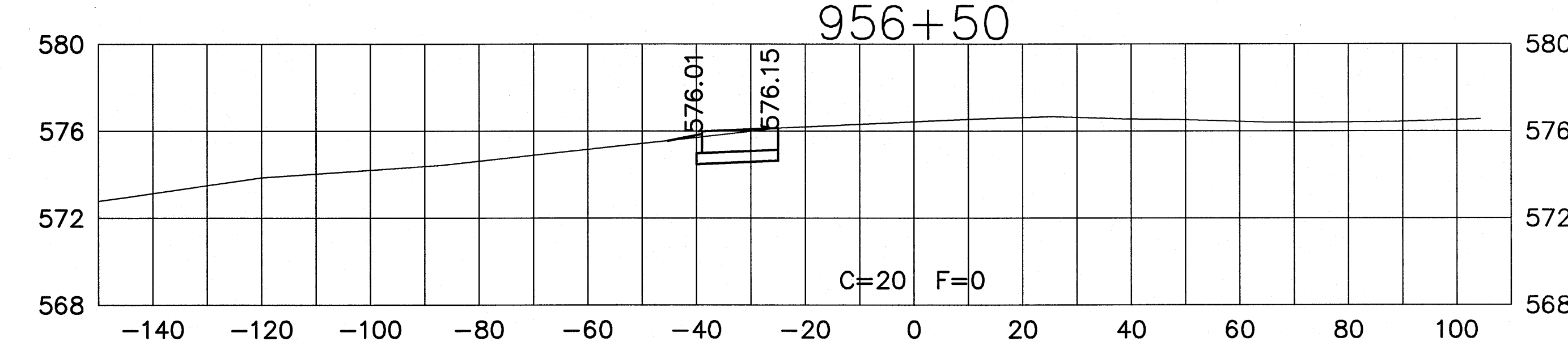
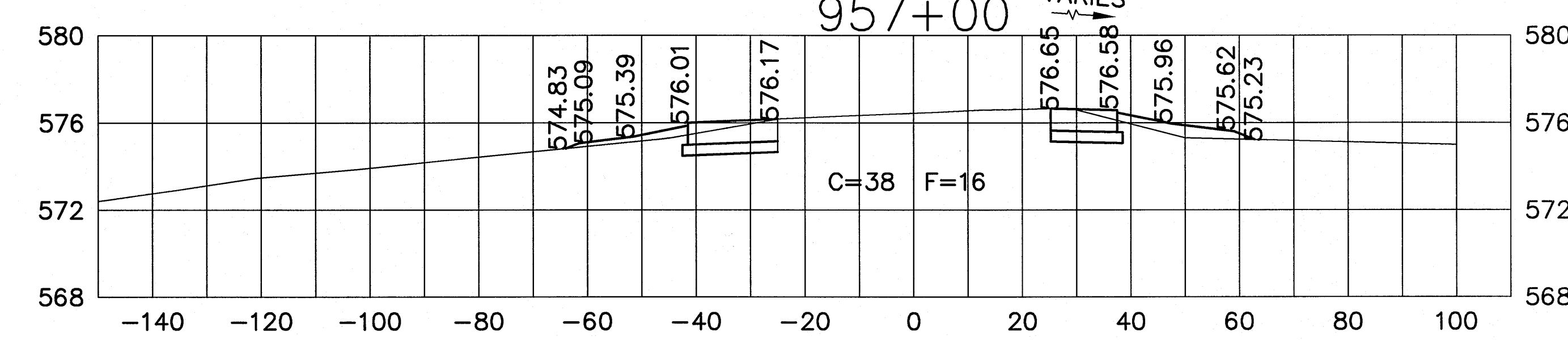
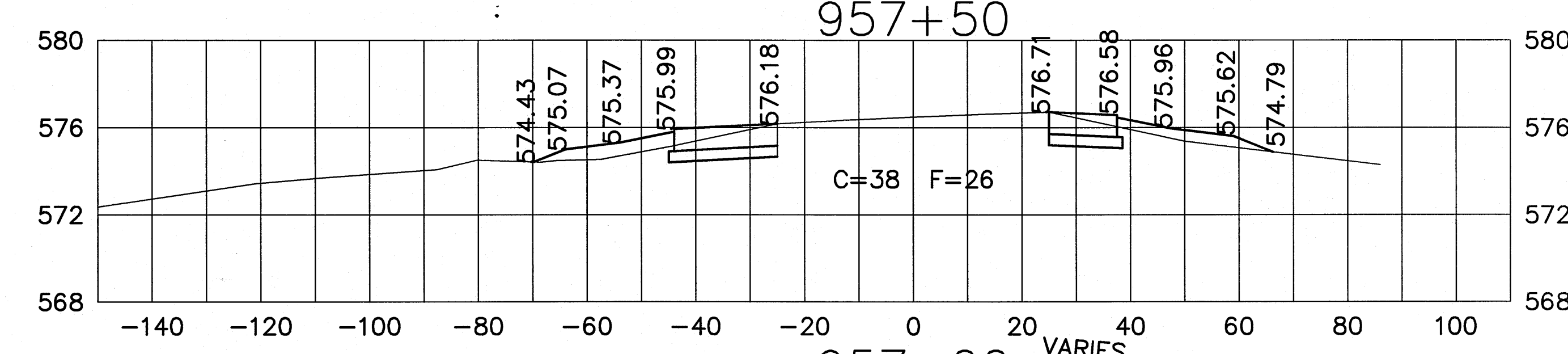
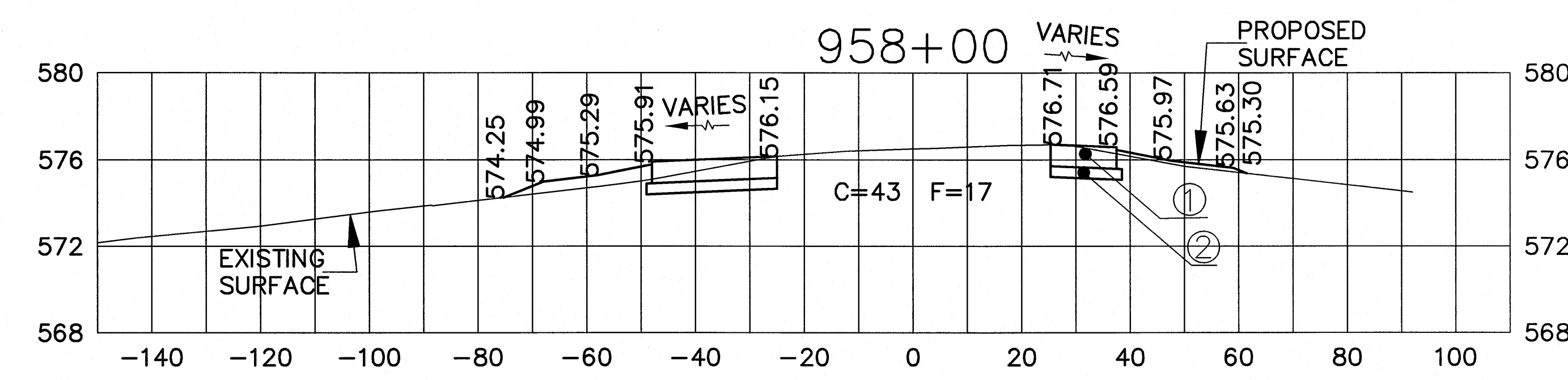
- ① PROP. 501512 - P.C.C. PAVEMENT
- ② PROP. 209510 - CRUSHED AGG. BASE COURSE
- ③ COMPACTED SAND FILL

SCALE
V: 1"=5'
H: 1"=20'

G:\AIRPORT\A09T002_TP_E_P\MT\Sections K.dwg, 7/8/2009 10:29:51 AM, jefm

- ① PROP. 501512 - P.C.C. PAVEMENT
- ② PROP. 209510 - CRUSHED AGG. BASE COURSE
- ③ COMPACTED SAND FILL
- ④ PROP. 4016610 - BIT. PAVEMENT

SCALE
V: 1"=5'
H: 1"=20'



G:\AIRPORT\A09T002_TP_E_PVMT\Sections K.dwg, 7/8/2009 10:29:57 AM, jefim