

CLEAN & SEAL BITUMINOUS CRACKS (AR201661)

A PAVEMENT CONDITION AND CRACK SURVEY OF THE EXISTING TAXIWAY PAVEMENT WAS PERFORMED IN THE SPRING OF 2008, AND THE PLAN QUANTITY IS BASED UPON THE CONDITIONS PRESENT AT THE TIME OF THE SURVEY, AND THE ANTICIPATED CONDITIONS FOLLOWING THE MILLING OPERATIONS. THE EXACT AMOUNT OF CRACKS TO BE CLEANED AND SEALED WILL BE THE NUMBER OF LINEAR FEET OF CRACKS MARKED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION, AND FOLLOWING THE MILLING OPERATIONS.

ALL CRACKS DESIGNATED BY THE RESIDENT ENGINEER FOR CLEANING AND SEALING WILL BE DONE SO AS STATED IN THE SPECIAL PROVISIONS.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:
AR201661 CLEAN & SEAL BITUMINOUS CRACKS - PER L.F.

BITUMINOUS CRACK REPAIR (AR201660)

SEE SHEET 7 FOR NOTES REGARDING THIS ITEM.

REMOVE & REPLACE BIT. PAVEMENT (AR401910)

ITEM AR401910 REMOVE AND REPLACE BITUMINOUS PAVEMENT WILL BE UTILIZED TO PERFORM LARGE CRACK REPAIR AND SHALL BE COMPLETED BEFORE THE EXISTING SURFACE IS MILLED.

THE QUANTITY OF CRACKS TO BE REPAIRED WAS ESTABLISHED BY THE COMPLETION OF A PAVEMENT CONDITION AND CRACK SURVEY PERFORMED IN THE SPRING OF 2008, AND CONSISTS OF THOSE CRACKS FOUND TO BE GREATER THAN ONE INCH IN WIDTH AT THE TIME OF THE SURVEY, AND EXHIBITING ROLLED EDGES. ALL CRACKS TO RECEIVE REPAIR SHALL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

THE CRACKS SHALL BE REMOVED FROM THE PAVEMENT BY ONE OF TWO METHODS, OR A COMBINATION OF THE TWO. THE FIRST ALTERNATIVE IS TO SAWCUT THE WIDTH OF THE AREA TO BE REPAIRED BY USING A WHEEL SAW AND REMOVE THE PAVEMENT TO THE SPECIFIED DEPTH USING A BACKHOE OR OTHER MECHANICAL MEANS. THE SECOND ALTERNATIVE IS TO USE A SKID STEER (OR EQUIVALENT) MOUNTED MILLING HEAD TO REMOVE THE PAVEMENT TO THE REQUIRED DEPTH. REGARDLESS OF THE METHOD USED TO REMOVE THE PAVEMENT, THE EDGE OF THE TRENCH FORMED WILL HAVE A VERTICAL FACE PRIOR TO THE PLACEMENT OF THE BITUMINOUS MATERIAL. THE WIDTH OF REPAIR WILL BE 3' WIDE, CENTERED ON THE CRACK. THE DEPTH OF REPAIR WILL BE THE FULL DEPTH OF THE EXISTING PAVEMENT. SEE THE TYPICAL SECTIONS, SHEET 4, FOR THE EXISTING PAVEMENT DEPTHS (BASED ON RECORD AS-BUILT CONSTRUCTION PLANS).

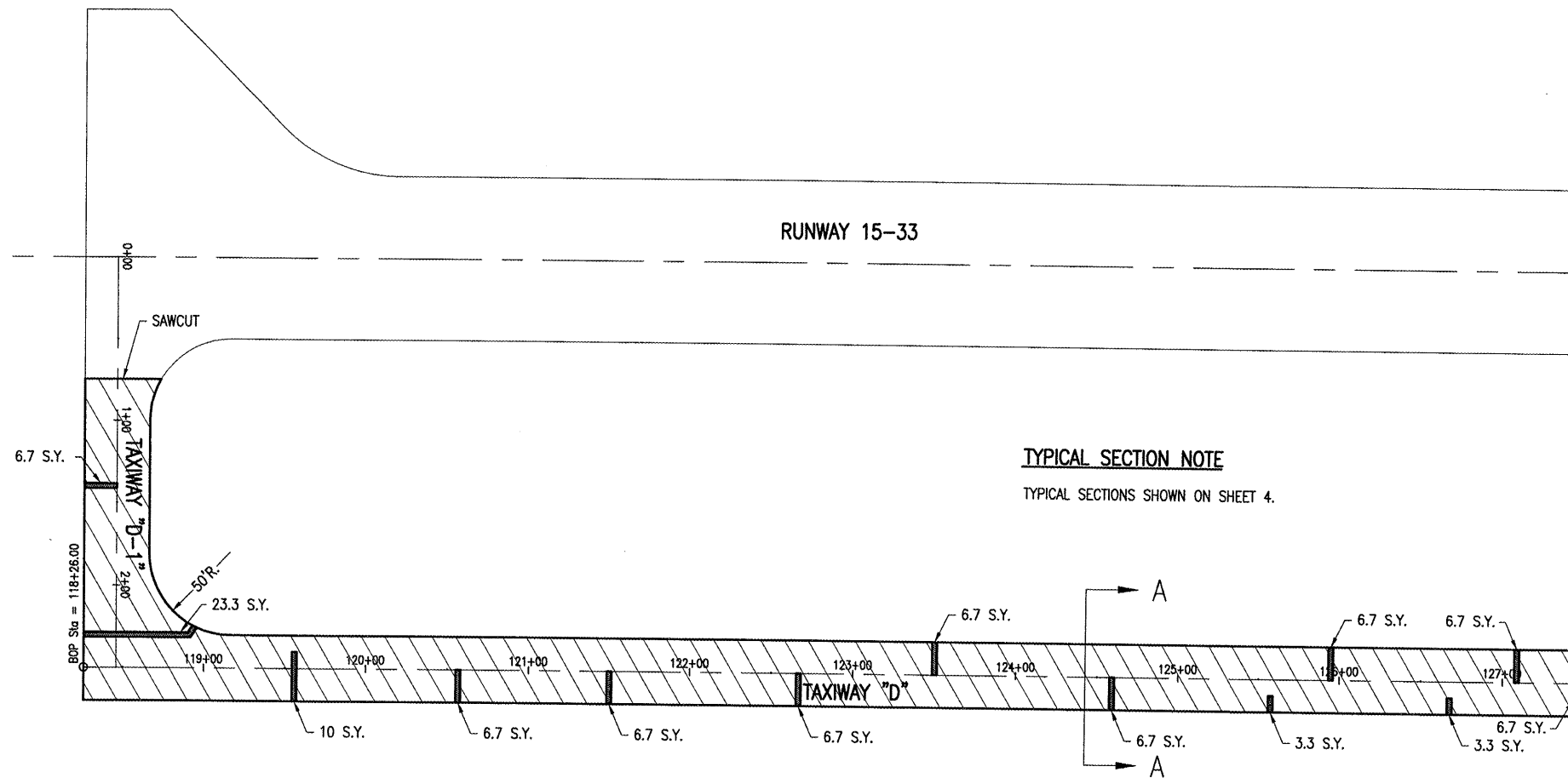
THE CONTRACTOR WILL DISPOSE OF THE EXCAVATED MATERIAL OFF THE AIRPORT SITE AT NO ADDITIONAL COST TO THE CONTRACT.

THE BOTTOM OF THE TRENCH WILL BE CLEANED AND COMPACTED TO PREVENT FUTURE SETTLEMENT, AND ACCEPTED BY THE RESIDENT ENGINEER. ONCE THE TRENCH IS CLEANED, THE BASE AGGREGATE COMPACTED, AND ACCEPTED, AN APPLICATION OF BITUMINOUS TACK MATERIAL WILL BE APPLIED TO THE VERTICAL BITUMINOUS FACE AND THE AGGREGATE BASE. THE REPAIR TRENCH WILL BE BACKFILLED WITH BITUMINOUS SURFACE COURSE MATERIAL (401), IN LIFTS NOT TO EXCEED 3 INCHES IN DEPTH. THE FINAL LIFT SHALL BE FLUSH WITH THE SURFACE OF THE EXISTING BITUMINOUS PAVEMENT. EACH LIFT SHALL BE COMPACTED AND ACCEPTED BY THE RESIDENT ENGINEER.

ANY DAMAGE TO THE EXISTING ADJACENT PAVEMENT WILL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE. THE QUANTITY OF PAVEMENT REMOVAL, MILLING AND/OR SAWCUTS, IF UTILIZED, TACK MATERIAL, AND BITUMINOUS SURFACE COURSE MATERIAL (401) NECESSARY TO COMPLETE THIS TASK WILL BE CONSIDERED INCIDENTAL TO THE TASK, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE QUANTITY OF BITUMINOUS CRACK REPAIR TO BE PAID FOR SHALL BE THE NUMBER OF SQUARE YARDS OF PAVEMENT REMOVED AND REPLACED, IN ACCORDANCE WITH THE SPECIAL PROVISIONS, CONSTRUCTION DRAWINGS, AND ACCEPTED BY THE ENGINEER.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:
AR401910 REMOVE AND REPLACE BITUMINOUS PAVEMENT - PER S.Y.



TYPICAL SECTION NOTE

TYPICAL SECTIONS SHOWN ON SHEET 4.

BITUMINOUS PAVEMENT MILLING (AR401650)

THE AREAS DESIGNATED AS [Hatched Pattern] ON THIS SHEET SHALL BE REMOVED USING A ROTO-MILLING EQUIPMENT. THE TOLERANCE OF THE MILLING WILL BE AS STATED IN THE STANDARD SPECIFICATIONS.

IF A SQUARE STRAIGHT EDGE IS NOT OBTAINED FROM THE MILLING OPERATIONS, THE EXISTING PAVEMENT WILL BE SAWED AS SHOWN ON THIS SHEET. SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT MILLING AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL MILLED MATERIAL WILL BE DISPOSED OF ON THE AIRPORT SITE. SEE SHEET 3, PROPOSED SAFETY PLAN, FOR LOCATION OF WASTE AREA.

PRIOR TO APPLYING THE BITUMINOUS OVERLAY ALL MILLED MATERIAL WILL BE BROOMED AND BLOWN CLEAN AND A BITUMINOUS TACK COAT APPLIED. THE VERTICAL FACE OF ALL SAW CUTS WILL BE PAINTED WITH A LIQUID ASPHALT.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

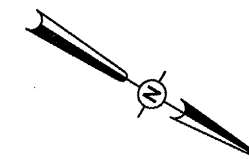
ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

ALL WORK PROPOSED WITHIN ITEM AR401910 REMOVE & REPLACE BIT. PAVEMENT SHALL BE ACCOMPLISHED BEFORE ANY MILLING OPERATIONS ARE CONDUCTED.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:
AR401650 BITUMINOUS PAVEMENT MILLING - PER S.Y.

LEGEND

- [Solid Line] EXISTING PAVEMENT
- [Hatched Pattern] PROPOSED REMOVE & REPLACE BIT. PAVEMENT
- [Diagonal Lines] PROPOSED MILLING



0' 25' 50' 100'
FULL SIZE SCALE: 1" = 50'
HALF SIZE SCALE: 1" = 100'

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| HANSON Professional Services, Inc. 1425 South Sixth Street Springfield, Illinois 62703-2886 Offices Nationwide | LAYOUT MLH 02/15/08 DRAWN MLH 02/26/08 REVIEWED RAW 06/24/08 | REHABILITATE TAXIWAYS "C" AND "D" INSTALL ELECTRICAL EQUIPMENT PROPOSED PAVEMENT PREPARATION PLAN STA. 118+26 TO STA. 127+50 | | 5 | | | |
| | | 5 of 30 sheets | | | | | |