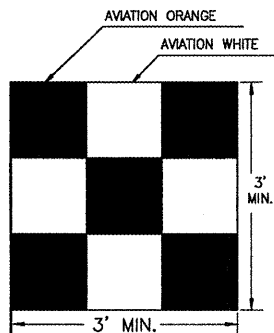
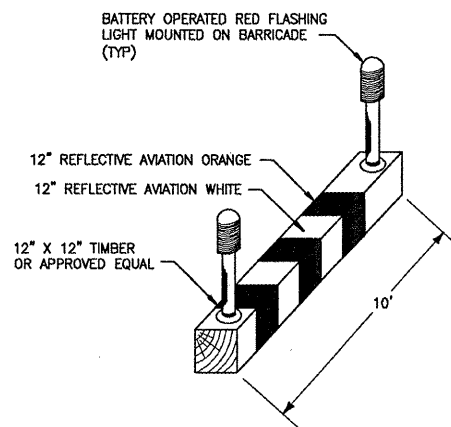


NOTE: SAFETY FLAG SHALL BE PROMINENTLY DISPLAYED ON ALL CONSTRUCTION EQUIPMENT. (SEE NOTE 2 UNDER SAFETY)



CONSTRUCTION SAFETY FLAG

N.T.S.



LOW LEVEL LIGHTED BARRICADES

N.T.S.

NOTES:

1. LOW LEVEL LIGHTED BARRICADES SHALL BE PLACED AROUND ALL OPEN EXCAVATIONS, HOLES, TRENCHES, PAVEMENT DROPOFFS, AND AREAS IDENTIFIED ON THE PHASING PLAN.
2. PLACE BARRICADES @ 20 FT. O.C. UNLESS OTHERWISE NOTED.
3. 20 BARRICADES SHALL BE TURNED OVER TO AND BECOME THE PROPERTY OF THE AIRPORT UPON COMPLETION OF THE PROJECT.

CONTRACTOR'S SAFETY AND SECURITY REQUIREMENTS

SAFETY

1. THE CONTRACTOR SHALL ACQUAINT HIS SUPERVISORS AND EMPLOYEES OF THE AIRPORT ACTIVITY AND OPERATIONS THAT ARE INHERENT TO THIS ACTIVE AIR CARRIER AIRPORT AND SHALL CONDUCT THE CONSTRUCTION ACTIVITIES TO CONFORM TO ALL ROUTINE AND EMERGENCY AIR TRAFFIC REQUIREMENTS AND GUIDELINES ON SAFETY AS SPECIFIED IN PARAGRAPH NO. 11 BELOW.
2. ALL CONTRACTOR VEHICLES THAT ARE AUTHORIZED TO OPERATE ON THE AIRPORT IN THE ACTIVE AIRCRAFT OPERATIONS AREA (AOA) SHALL DISPLAY IN FULL VIEW A FLASHING AMBER (YELLOW) DOME-TYPE LIGHT OR ABOVE THE VEHICLE A 3' X 3' OR LARGER, ORANGE AND WHITE CHECKERBOARD FLAG, EACH CHECKERBOARD COLOR BEING 1-FOOT SQUARE, (SEE CONSTRUCTION SAFETY FLAG DETAIL, THIS SHEET) AND ESCORTED UNDER THE CONTROL OF ONE CONTRACTOR MOBILE (TWO-WAY) RADIO OPERATOR ON THE JOB AT ALL TIMES. ANY VEHICLE OPERATING IN THE ACTIVE AOA DURING THE HOURS OF DARKNESS SHALL BE EQUIPPED WITH A FLASHING AMBER (YELLOW) DOME LIGHT, MOUNTED ON TOP OF THE VEHICLE AND OF SUCH INTENSITY TO CONFORM TO LOCAL CODES FOR MAINTENANCE AND EMERGENCY VEHICLES.
3. ALL NON-RADIO EQUIPPED CONTRACTOR VEHICLES THAT ARE REQUIRED TO OPERATE ON OR ACROSS ACTIVE RUNWAYS, TAXIWAYS, APRONS AND RUNWAY APPROACH AND PROTECTION ZONES SHALL DO SO UNDER THE DIRECT CONTROL OF A RADIO EQUIPPED ESCORT VEHICLE. ESCORT VEHICLES, OPERATORS, AND RADIO EQUIPMENT SHALL BE APPROVED BY THE AIRPORT. ESCORT VEHICLES SHALL BE MARKED AND LIGHTED AS DESCRIBED IN PARAGRAPH NO. 2 ABOVE AND SHALL BE EQUIPPED WITH AN APPROVED AVIATION BAND. RADIO EQUIPPED VEHICLES SHALL CONTINUOUSLY MONITOR GROUND CONTROL FREQUENCY 121.85 MHZ. ALL AIRCRAFT TRAFFIC ON RUNWAYS, TAXIWAYS AND APRONS SHALL HAVE PRIORITY OVER CONTRACTOR'S TRAFFIC.
4. NO RUNWAY, TAXIWAY, APRON OR AIRPORT ROADWAY SHALL BE CLOSED WITHOUT WRITTEN APPROVAL OF AIRPORT OPERATIONS. TO ENABLE NECESSARY "NOTICES TO AIRMEN" (NOTAM) OR ADVISORIES TO AIRPORT SERVICES OR TENANTS, A MINIMUM OF 72 HOURS WRITTEN NOTICE REQUESTING CLOSING SHALL BE DIRECTED TO THE RESIDENT PROJECT REPRESENTATIVE (RPR) AND/OR RESIDENT ENGINEER WHO WILL COORDINATE THE REQUEST WITH AIRPORT OPERATIONS.
5. ANY CONSTRUCTION ACTIVITY WITHIN 175 FEET OF AN ACTIVE RUNWAY EDGE OR 50 FEET FROM AN ACTIVE TAXIWAY EDGE OR OPEN EXCAVATIONS IN EXCESS OF THREE INCHES DEEP WITHIN THE ABOVE AREAS, WILL REQUIRE CLOSURE OF THE AFFECTED RUNWAY OR TAXIWAY, UNLESS OTHERWISE APPROVED BY AIRPORT OPERATIONS. CLOSURE REQUIRES THE SAME PROVISIONS AS PARAGRAPH NO. 4 ABOVE.
6. OPEN FLAME WELDING OR TORCH CUTTING OPERATIONS ARE PROHIBITED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS HAVE BEEN TAKEN AND THE PROCEDURE APPROVED BY THE RPR AND/OR AIRPORT OPERATIONS.
7. STOCKPILED MATERIAL SHALL BE CONSTRAINED IN A MANNER TO PREVENT MOVEMENT RESULTING FROM AIRCRAFT JET BLAST OR WIND CONDITIONS IN EXCESS OF TEN KNOTS. STOCKPILE HEIGHT SHALL BE LESS THAN 15 FEET, AND OUTSIDE THE RUNWAY AND TAXIWAY OBJECT FREE AREAS. IN ADDITION, STOCKPILED MATERIAL SHALL HAVE SILT FENCE LOCATED AROUND THE MATERIAL TO PREVENT FOD FROM MOVING ONTO THE AIRFIELD PAVEMENTS.
8. OPEN TRENCHES, EXCAVATIONS AND STOCKPILED MATERIAL LOCATED IN THE AOA SHALL BE PROMINENTLY MARKED WITH FLAGS AND LIGHTED BY APPROVED LIGHT UNITS DURING HOURS OF RESTRICTED VISIBILITY AND DARKNESS.
9. DEBRIS, WASTE AND LOOSE MATERIAL CAPABLE OF CAUSING DAMAGE TO AIRCRAFT LANDING GEARS, PROPELLERS OR BEING INGESTED IN JET ENGINES SHALL NOT BE ALLOWED ON ACTIVE AIRCRAFT MOVEMENT AREAS. IF THESE MATERIALS ARE OBSERVED TO BE ON ACTIVE AIRCRAFT MOVEMENT AREAS, THEY WILL BE REMOVED IMMEDIATELY AND/OR CONTINUOUSLY DURING CONSTRUCTION.
10. THE RESIDENT ENGINEER WILL ARRANGE WITH AIRPORT OPERATIONS FOR INSPECTION PRIOR TO OPENING FOR AIRCRAFT USE ANY RUNWAY OR TAXIWAY THAT HAS BEEN CLOSED FOR WORK, ON OR ADJACENT THERETO, OR THAT HAS BEEN USED FOR A CROSSING POINT OR HAUL ROUTE BY THE CONTRACTOR.
11. THE CONTRACTOR IS DIRECTED TO COMPLY WITH AND ACQUAINT HIS/HER EMPLOYEES WITH THE FOLLOWING SAFETY GUIDELINES, RELATED MATERIALS AND FAA ADVISORY CIRCULARS:

<p>FAA ORDER 150/5200-18C 150/5210-5B 150/5370-2E 150/5370-4 150/5370-7</p>	<p>"SAFETY REQUIREMENT ON AIRPORTS DURING AGENCY FUNDED CONSTRUCTION AND MAINTENANCE ACTIVITIES" "AIRPORT SAFETY-SELF INSPECTION" "PAINTING, MARKING & LIGHTING OF VEHICLES USED ON AIRPORTS" "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" "APPENDIX 3. SUGGESTED SPECIAL PROVISIONS FOR PROTECTION OF CABLES, CONTROLS, NAVAIDS AND WEATHER BUREAU FACILITIES" "AIRPORT CONSTRUCTION CONTROLS TO PREVENT AIR AND WATER POLLUTION"</p>
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12. CONSTRUCTION DURING THE PROJECT MAY BE HALTED AT ANY TIME BY RPR, ENGINEER, AND/OR AIRPORT OPERATIONS IF IT IS DETERMINED TO BE IN THE BEST INTEREST OF AIRPORT OPERATIONS OR SAFETY. THE CONTRACTOR MAY BE DIRECTED TO REMOVE EQUIPMENT AND/OR EVACUATE THE SITE IN ORDER TO ENABLE AIRCRAFT OPERATIONS. NECESSARY EXTENSIONS IN CONTRACT TIME WILL BE GRANTED OR A STOP WORK ORDER WILL BE ISSUED DUE TO THESE DELAYS. HOWEVER, THERE WILL BE NO ADJUSTMENTS IN CONTRACT PRICE DUE TO THESE DELAYS. ANY DELAY LONGER THAN 8 HOURS WILL RESULT IN AN EXTENSION OF CONTRACT TIME IF THE DELAY IS THROUGH NO FAULT OF THE CONTRACTOR.

IN ADDITION TO THE ABOVE, THE FOLLOWING SPECIAL REQUIREMENTS WILL APPLY FOR NIGHT CONSTRUCTION:
13. WHEN APPLICABLE, A DAILY SAFETY AND PROGRESS MEETING SHALL BE HELD BETWEEN THE RESIDENT ENGINEER AND THE CONTRACTOR'S SUPERINTENDENT TO DISCUSS REQUIREMENTS FOR THE NIGHTTIME WORK PERIODS.
14. WHEN APPLICABLE, THE CONTRACTOR SHALL PREPARE A SAFETY PLAN SPECIFIC TO NIGHTTIME CONSTRUCTION OPERATIONS, AS WELL AS A CONTINGENCY PLAN TO ADDRESS CASES OF ABNORMAL FAILURES OR UNEXPECTED DISASTERS.
15. TRUCK HAUL ROUTES ON THE AIRFIELD SHALL BE DELINEATED WITH LIGHTED BARRICADES. OTHER MEANS TO CLEARLY MARK THE ROUTES TO THE WORK SITE MAY BE APPROVED BY THE RPR, ENGINEER, AND/OR AIRPORT OPERATIONS.
16. ALL COMMUNICATION WITH THE AIR TRAFFIC CONTROL TOWER OR OTHER ELEMENTS OF THE AIRPORT SHALL BE THROUGH THE RESIDENT ENGINEER AND/OR AIRPORT OPERATIONS OR THE CONTRACTOR'S BADGED PERSONNEL. IF THE CONTRACTOR IS REQUIRED TO CROSS ACTIVE MOVEMENT AREAS THE CONTRACTOR'S SUPERINTENDENT AND FOREMAN SHALL BE BADGED BY THE AIRPORT. SEE SECURITY NOTE 6.

SECURITY

1. **GENERAL INTENT:** IT IS INTENDED THAT THE CONTRACTOR SHALL COMPLY WITH ALL REQUIREMENTS OF THE AIRPORT SECURITY PLAN AND WITH THE SECURITY REQUIREMENTS SPECIFIED HEREIN BY AIRPORT OPERATIONS. THE CONTRACTOR SHALL DESIGNATE TO THE RESIDENT ENGINEER AND AIRPORT OPERATIONS, IN WRITING, THE NAME OF HIS "CONTRACTOR SECURITY OFFICER (CSO)." THE CSO SHALL REPRESENT THE CONTRACTOR ON THE SECURITY REQUIREMENTS FOR THE CONTRACT.
2. **CONTRACTOR PERSONNEL SECURITY ORIENTATION:** THE CSO SHALL BE RESPONSIBLE FOR BRIEFING ALL CONTRACTOR PERSONNEL ON SECURITY REQUIREMENTS. ALL NEW CONTRACTOR EMPLOYEES SHALL BE BRIEFED ON SECURITY REQUIREMENTS PRIOR TO WORKING IN THE CONSTRUCTION AREA.
3. **ACCESS TO THE SITE:** CONTRACTOR'S ACCESS TO THE SITE SHALL BE AS SHOWN ON THE PLANS. NO OTHER ACCESS POINTS SHALL BE ALLOWED UNLESS APPROVED BY AIRPORT OPERATIONS. ALL CONTRACTOR TRAFFIC AUTHORIZED TO ENTER THE SITE SHALL BE EXPERIENCED IN THE ROUTE OR GUIDED BY CONTRACTOR PERSONNEL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL TO AND FROM THE VARIOUS CONSTRUCTION AREAS ON THE SITE, AND FOR THE OPERATION AND SECURITY OF THE ACCESS GATE TO THE SITE. A CONTRACTOR'S FLAGMAN OR TRAFFIC CONTROL PERSON SHALL MONITOR AND COORDINATE ALL CONTRACTOR TRAFFIC AT THE ACCESS GATE WITH SECURITY. THE CONTRACTOR SHALL NOT PERMIT ANY UNAUTHORIZED CONSTRUCTION PERSONNEL OR TRAFFIC ON THE SITE. ACCESS GATES TO THE SITE SHALL BE LOCKED AND SECURED AT ALL TIMES WHEN NOT ATTENDED BY THE CONTRACTOR. IF THE CONTRACTOR CHOOSES TO LEAVE ANY ACCESS GATE OPEN, IT SHALL BE ATTENDED BY CONTRACTOR PERSONNEL WHO ARE FAMILIAR WITH THE REQUIREMENTS OF THE AIRPORT OPERATIONS SECURITY PROGRAM. THE CONTRACTOR IS RESPONSIBLE FOR THE IMMEDIATE CLEANUP OF ANY DEBRIS DEPOSITED ALONG THE ACCESS ROUTE AS A RESULT OF HIS CONSTRUCTION TRAFFIC. DIRECTIONAL SIGNING FROM THE ACCESS GATE ALONG THE DELIVERY ROUTE TO THE STORAGE AREA, PLANT SITE OR WORK SITE SHALL BE AS DIRECTED BY AIRPORT OPERATIONS.
4. **MATERIALS DELIVERY TO THE SITE:** ALL CONTRACTOR'S MATERIAL ORDERS FOR DELIVERY TO THE WORK SITE WILL USE AS A DELIVERY ADDRESS, THE STREET NAME ASSIGNED TO THE ACCESS POINT AT THE CONTRACTOR'S STAGING SITE AT THE AIRPORT. THE AIRPORT NAME SHALL NOT BE USED IN THE DELIVERY ADDRESS AT ANY TIME. THIS WILL PRECLUDE DELIVERY TRUCKS FROM ENTERING INTO THE TERMINAL COMPLEX, OR TAKING SHORT CUTS THROUGH THE PERIMETER GATES AND ENTERING INTO AIRCRAFT OPERATIONS AREAS INAPPROPRIATELY.
5. **CONSTRUCTION AREA LIMITS:** THE LIMITS OF CONSTRUCTION, MATERIAL STORAGE AREAS, PLANT SITE, EQUIPMENT STORAGE AREA, PARKING AREA AND OTHER AREAS DEFINED AS REQUIRED FOR THE CONTRACTOR'S EXCLUSIVE USE DURING CONSTRUCTION SHALL BE MARKED BY THE CONTRACTOR. THE CONTRACTOR SHALL ERECT AND MAINTAIN AROUND THE PERIMETER OF THESE AREAS SUITABLE FENCING, MARKING AND/OR WARNING DEVICES VISIBLE FOR DAY/NIGHT USE. TEMPORARY BARRICADES, FLAGGING AND FLASHING WARNING LIGHTS WILL BE REQUIRED AT CRITICAL ACCESS POINTS. TYPE OF MARKING AND WARNING DEVICES SHALL BE APPROVED BY AIRPORT OPERATIONS.
6. **IDENTIFICATION---PERSONNEL:** ALL EMPLOYEES, AGENTS, VENDORS, INVITEES, ETC. OF THE CONTRACTOR OR SUBCONTRACTORS REQUIRING ACCESS TO THE CONSTRUCTION SITE SHALL, IN ACCORDANCE WITH THE AIRPORT OPERATIONS SECURITY PROGRAM, BE REQUIRED TO DISPLAY IDENTIFICATION OR BE UNDER ESCORT BY PROPERLY BADGED PERSONNEL. THESE BADGES WILL BE IDENTIFIED NUMERICALLY AND ISSUED TO INDIVIDUAL EMPLOYEES WITH A PERMANENT RECORD MAINTAINED ON EACH INDIVIDUAL TO WHOM A BADGE IS ISSUED. AT THE COMPLETION OF THE CONTRACT ALL BADGES WILL BE RETURNED TO THE AIRPORT AND A CHARGE OF \$100 PER BADGE WILL BE ASSESSED FOR ALL BADGES NOT RETURNED. NO BADGE WILL BE ISSUED TO ANY PERSON UNTIL A REVIEW OF THE TSA REQUIRED PAPERWORK BY AIRPORT SECURITY AND ALL REQUIREMENTS ARE MET. PAPERWORK SHALL BE SUBMITTED A MINIMUM OF 7 DAYS BEFORE ISSUANCE OF BADGE. IDENTIFIABLE HARD HATS OR OTHER IDENTIFICATION SHALL ALSO BE WORN AT ALL TIMES IF REQUIRED BY AIRPORT OPERATIONS. THE CONTRACTOR'S SUPERINTENDENT AND FOREMAN ARE RESPONSIBLE FOR ATTENDING TRAINING AND COMPLETING SECURITY BADGE APPLICATIONS, WHICH WILL INCLUDE AIR/GROUND RADIO COMMUNICATION, TAXIWAY AND AIRPORT FAMILIARIZATION. ESTIMATED TIME FOR COMPLETION IS 3 HOURS.
7. **IDENTIFICATION---VEHICLES:** THE CONTRACTOR, THROUGH THE CSO, SHALL ESTABLISH AND MAINTAIN A LIST OF CONTRACTOR AND SUBCONTRACTOR VEHICLES AUTHORIZED TO OPERATE ON THE SITE. PERSONAL VEHICLES OF CONTRACTOR EMPLOYEES SHALL BE RESTRICTED TO THE CONTRACTOR'S EMPLOYEE PARKING AREA AND ARE NOT ALLOWED ON OTHER AREAS OF THE AOA AT ANY TIME.
8. **FINES:** PAYMENT OF ALL FINES ASSESSED TO THE AIRPORT DUE TO VIOLATIONS BY THE CONTRACTOR OF TSA/FAA SECURITY OR SAFETY REQUIREMENTS SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. IF THE RESTRICTED AREA GATE IS FOUND TO BE OPEN OR UNLOCKED AND UNATTENDED, AIRPORT SECURITY POLICE MAY ISSUE THE CONTRACTOR A CITATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS AND IMPOSED FINES. IN ADDITION, A \$1,000.00 CHARGE MAY BE LEVIED BY THE AIRPORT FOR EACH VIOLATION SO DOCUMENTED AND UPON FINAL PAYMENT THE TOTAL OF ANY SUCH CHARGES WILL BE DEDUCTED FROM MONEYS DUE THE CONTRACTOR.
9. A MINIMUM OF 48 HOURS IN ADVANCE OF ANY EXCAVATION OR BORINGS, THE CONTRACTOR SHALL CONTACT LOCAL CABLE OWNERS TO VERIFY ALL UNDERGROUND CABLE LOCATIONS IN THE VICINITY OF THE PROPOSED WORK.

SAFETY PLAN NARRATIVE

1. THIS PROJECT INCLUDES APPROXIMATELY 11,600 SY OF NEW CONCRETE APRON PAVEMENT, EXCAVATION, DRAINAGE IMPROVEMENTS, ELECTRICAL IMPROVEMENTS AND SITE RESTORATION.
2. AIRCRAFT OPERATIONS WILL CONTINUE DURING CONSTRUCTION. TYPE OF AIRCRAFT OPERATIONS DURING CONSTRUCTION INCLUDE DESIGN GROUPS I, II, III AND IV AND APPROACH CATEGORY A, B AND C.
3. BASIS FOR TEMPORARY DISPLACED OR RELOCATED THRESHOLDS: NOT APPLICABLE.
4. BASIS FOR DEVIATING FROM STANDARDS: NOT APPLICABLE.
5. TYPE AND HEIGHT OF CONSTRUCTION EQUIPMENT:
 - A. TRUCKS (DUMP, FLATBED, CONCRETE) - 15 FEET
 - B. FRONT END LOADERS - 20 FEET
 - C. BULLDOZERS - 15 FEET
 - D. ROLLERS AND COMPACTORS - 15 FEET
 - E. EXCAVATORS - 25 FEET
6. DATES FOR START AND COMPLETION OF WORK:

ESTIMATED START:	SEPTEMBER 15, 2008
ESTIMATED COMPLETE:	JULY 3, 2009
CONSTRUCTION TIME:	150 CALENDAR DAYS

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IL PROJECT: PIA-3812
AIP PROJECT: 3-17-0080-46

REVISIONS

NO.	DESCRIPTION	DATE

DATE ISSUED: 06/27/08

REVIEWED BY: PCB

DRAWN BY: M.JL

DESIGNED BY: NDO

AEP PROJECT NUMBER

213-6945-000

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SHEET TITLE

**SAFETY AND SECURITY
NOTES AND DETAILS**

SHEET NUMBER

3 OF 43

**ISSUED FOR
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