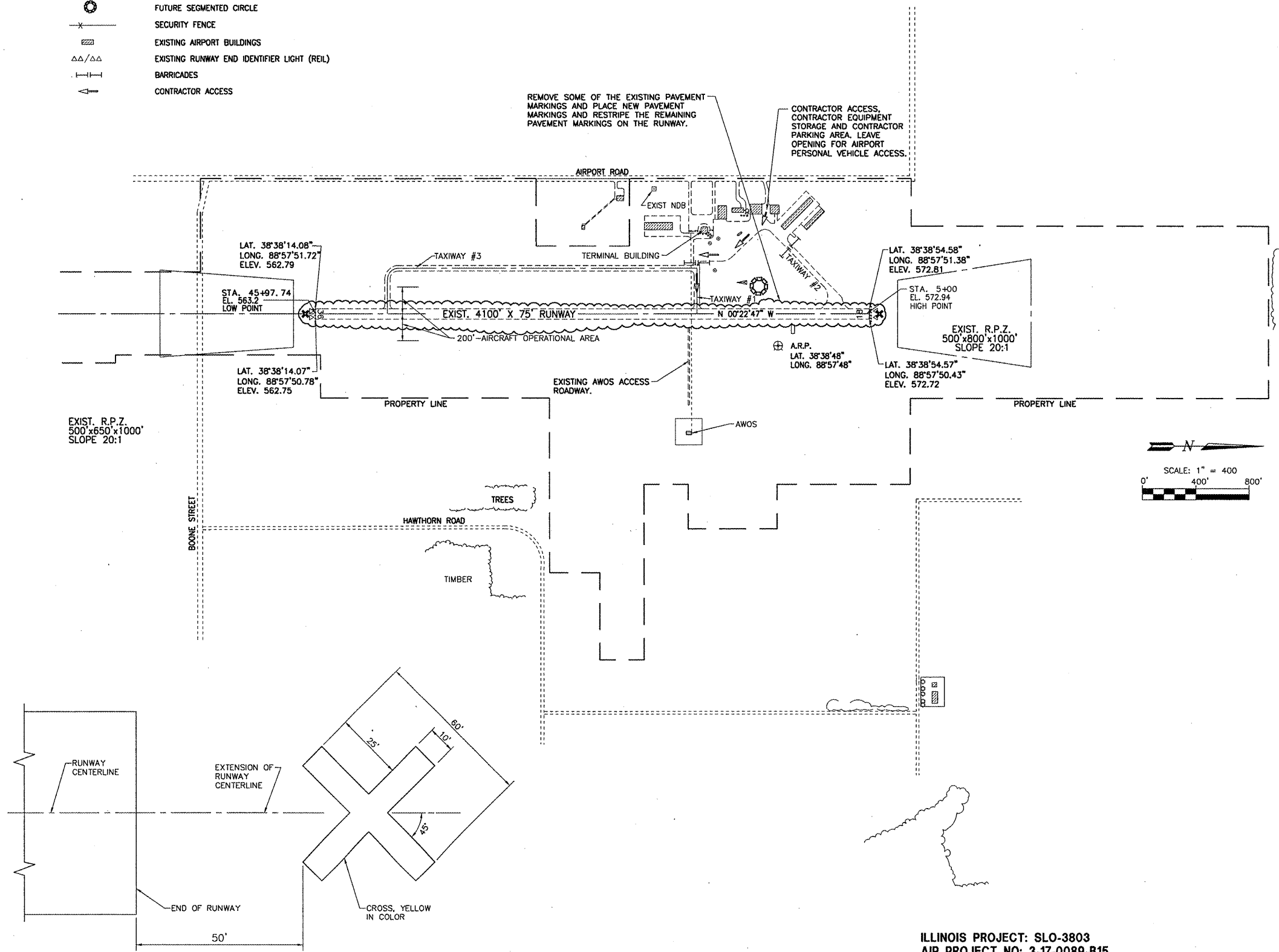


LEGEND

- EXISTING NON-DIRECTIONAL BEACON (NDB)
- EXISTING AIRPORT BOUNDARY
- EXISTING ROTATING BEACON
- EXISTING INTERNALLY LIGHTED WIND CONE
- EXISTING THRESHOLD LIGHT - GREEN
- EXISTING RUNWAY PROTECTION ZONE (RPZ)
- FUTURE SEGMENTED CIRCLE
- SECURITY FENCE
- EXISTING AIRPORT BUILDINGS
- EXISTING RUNWAY END IDENTIFIER LIGHT (REIL)
- BARRICADES
- CONTRACTOR ACCESS



CONTRACTOR'S RESPONSIBILITIES

1. THE CONTRACTOR'S EMPLOYEES WILL PARK IN THE EQUIPMENT PARKING AREA.
2. THE CONTRACTOR WILL FURNISH ALL EMPLOYEES WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THEM AS BEING PART OF THE CONSTRUCTION CREW.
3. WHEN THE CONTRACTOR'S VEHICLES ARE ON THE AIRPORT SITE THEY WILL BE PROPERLY MARKED WITH THE INTERNATIONAL ORANGE AND WHITE FLAG DISPLAYED IN FULL VIEW ABOVE THE VEHICLES.
4. CONTRACTOR MUST HAVE AERONAUTIC RADIO FOR EMERGENCY USE (122.8 MHZ)
5. THE CONTRACTOR WILL NOT BE ALLOWED ACCESS TO THE AIRPORT TERMINAL BUILDING & WILL NEED TO SUPPLY PORTABLE RESTROOMS ON SITE FOR CONSTRUCTION CREWS.
6. CONTRACTOR PERSONNEL SHALL BE LIMITED TO AREA REQUIRED TO CONSTRUCT IMPROVEMENTS.

AIRCRAFT OPERATIONAL AREA

THE RUNWAY SHALL BE CLOSED FOR THE DURATION OF THE PROJECT. THE CONTRACTOR OR HIS EMPLOYEES WILL NOT PROCEED INTO THE AIRCRAFT OPERATIONAL AREA WITHOUT FIRST CLOSING THE AFFECTED RUNWAY. THE CONTRACTOR WILL NOTIFY THE AIRPORT MANAGER TWO DAYS IN ADVANCE OF A RUNWAY CLOSING. ANY RUNWAY CLOSURE MUST BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO ANY CONSTRUCTION. WHEN CONSTRUCTION ACTIVITIES ARE WITHIN 200' OF THE RUNWAY CENTERLINE, THE RUNWAY WILL BE CLOSED BY PLACING YELLOW CROSSES 50' PAST THE END OF THE RUNWAY. COST OF CONSTRUCTING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

MISCELLANEOUS NOTES

THE LOCATIONS OF THE UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAVE BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER, IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED.

EROSION CONTROL

WORK WILL DISTURB LESS THAN 1 ACRE OF LAND. THEREFORE NO N.P.D.E.S. PERMIT WILL BE REQUIRED.

SCOPE OF WORK

THIS PROJECT SHALL CONSIST OF REMOVING SOME OF THE EXISTING PAVEMENT MARKINGS AND PLACING NEW PAVEMENT MARKINGS ALONG WITH RESTRIPE THE REMAINING PAVEMENT MARKINGS ON THE RUNWAY.

HAUL ROAD AND EQUIPMENT PARKING AREA

CONTRACTOR WILL USE THE DESIGNATED EQUIPMENT PARKING AREA AS SHOWN ON THIS SHEET. IT WILL BE THEIR RESPONSIBILITY TO MAINTAIN THE AIRPORT SECURITY AT THE HAUL ROUTE ENTRANCE, AND RESTORE THESE AREAS TO THEIR ORIGINAL CONDITION AFTER CONSTRUCTION IS COMPLETED. ANY AREAS DAMAGED OUTSIDE THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE. THE COST OF THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

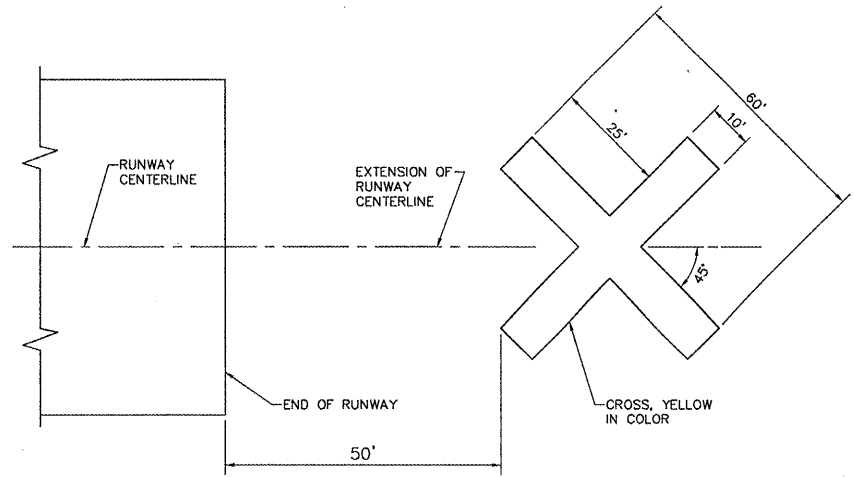
EXISTING CABLE LOCATIONS

THE CONTRACTOR SHALL CONTACT J.U.L.I.E. AND HAVE ALL UTILITIES LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. 811 OR 1-800-892-0123.

IT WILL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ALL UNDERGROUND UTILITIES.

CONSTRUCTION EQUIPMENT HEIGHT

MAXIMUM HEIGHT OF CONSTRUCTION EQUIPMENT SHALL NOT EXCEED 15'.



DETAIL OF CROSS FOR CLOSED RUNWAY

ILLINOIS PROJECT: SLO-3803
AIP PROJECT NO: 3-17-0089-B15

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MARK RUNWAY 18-36
SALEM LECKRONE AIRPORT
SALEM, ILLINOIS

SAFETY PLAN

SURVEY	JAS	CHECKED	DATE	SHEET 2 OF 3
DESIGN	MRQ	APPROVED	06/27/08	
DRAWN	BLT	REVISED	JOB NO. 200802123	

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