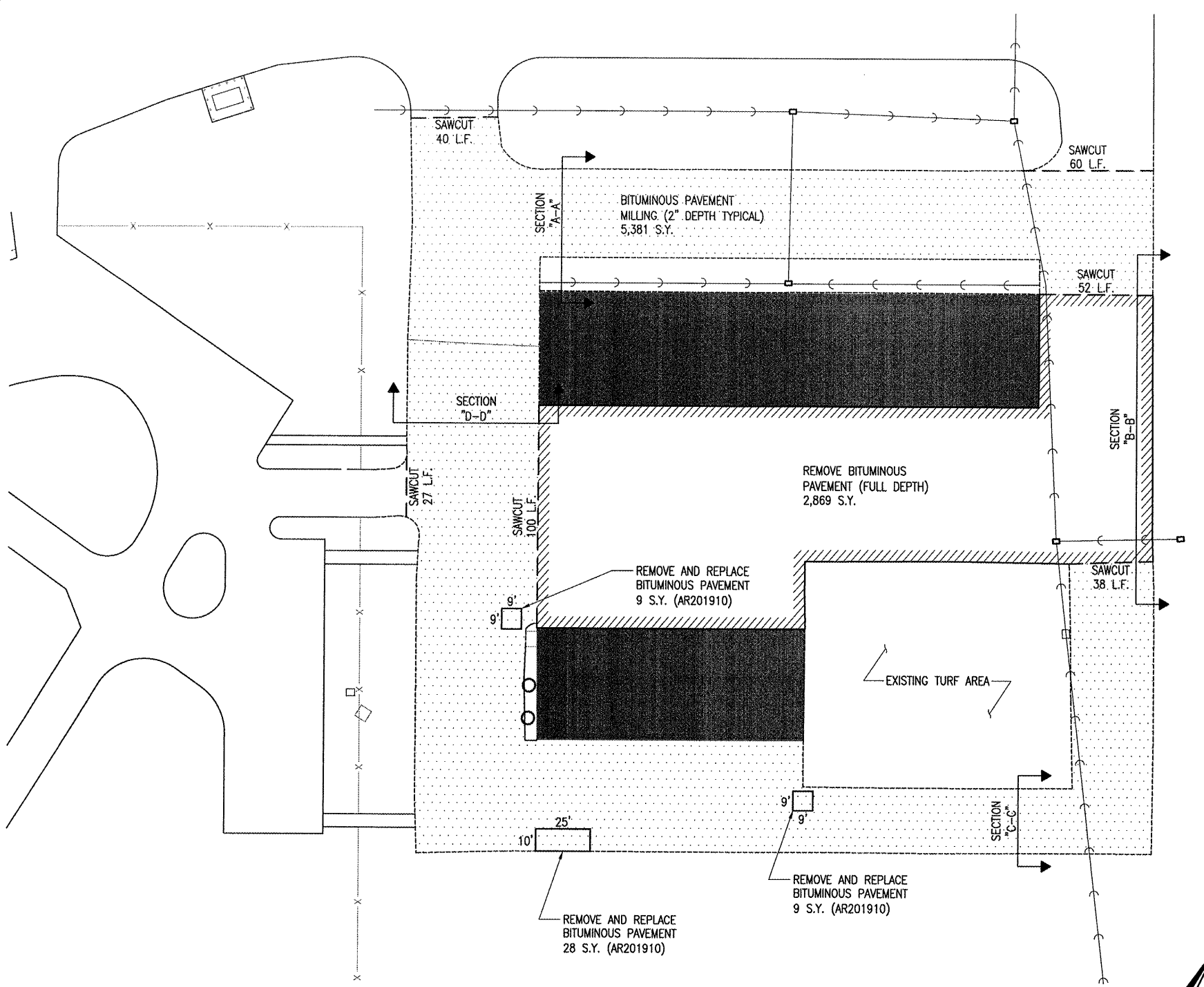
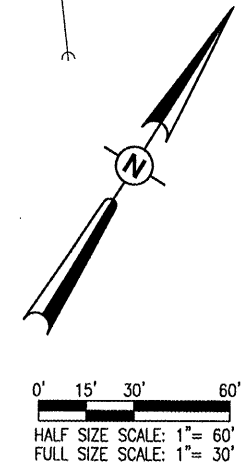


JUN 30, 2008 4:13 PM C:\HW000795
 E:\AIRPORTS\SHELBY\827-03THG\DESIGN\CAD\SHEETS\R-102-PREP.DWG - Layout1



- LEGEND**
- BUILDINGS (HANGAR)
 - DRAINAGE PIPE
 - INLET
 - FENCE
 - REMOVE AND REPLACE BITUMINOUS PAVEMENT
 - PAVEMENT MILLING



SE003

BITUMINOUS PAVEMENT MILLING (AR401650)

APRON - THE DESIGNATED MILLING AREAS ON THIS SHEET WILL BE CUT OR TRIMMED. THE TOP 2" (INCHES) OF THE EXISTING BITUMINOUS SURFACE/BASE COURSE MATERIAL WILL BE REMOVED BY ROTO-MILLING. THE TOLERANCE OF THE MILLING WILL BE AS STATED IN THE SPECIAL PROVISIONS FOR BITUMINOUS SURFACE COURSE, 401-4.14 SURFACE TEST OF STANDARD SPECIFICATIONS.

WHERE THE BITUMINOUS SURFACING MATCHES EXISTING PAVEMENT TO REMAIN, THE EXISTING PAVEMENT WILL BE SAWED IF A VERTICAL FACE IS NOT ACHIEVED BY THE MILLING OPERATIONS. SAWING WILL BE PAID UNDER ITEM AR401665 - BITUMINOUS PAVEMENT SAWING - PER LINEAR FOOT.

ALL MILLED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE IN A LEGAL MANNER.

AFTER THE MILLING OPERATION IS COMPLETE, THE PAVEMENT SHALL BE INSPECTED AND ANY REMAINING CRACKS THAT REQUIRE ATTENTION IN THE OPINION OF THE RESIDENT ENGINEER WILL BE REPAIRED IN ACCORDANCE WITH THESE PLANS AND THE SPECIAL PROVISIONS. CRACK REPAIR WILL BE PAID SEPARATELY PER LINEAR FOOT. AR201660.

PRIOR TO APPLYING THE BITUMINOUS OVERLAY ON THE APRON, ALL MILLED MATERIAL WILL BE BROOMED AND BLOWN CLEAN AND A BITUMINOUS TACK COAT APPLIED. THE VERTICAL FACE OF ALL SAW CUTS WILL BE PAINTED WITH A LIQUID ASPHALT.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE. ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

REMOVE BITUMINOUS PAVEMENT (AR401900)

APRON RECONSTRUCTION - THE CONTRACTOR WILL REMOVE THE EXISTING BITUMINOUS MATERIAL TO THE EXISTING EARTH BASE. THE CONTRACTOR SHALL THEN BE REQUIRED TO EXCAVATE, REWORK, AND RECOMPACT THE SUBGRADE PRIOR TO PAVING IN ACCORDANCE WITH ITEM 152 OF THE SPECIFICATIONS AND TO THE SATISFACTION OF THE RESIDENT ENGINEER. EXCAVATION, REWORK, AND COMPACTION OF THE EXISTING EARTH SUBGRADE PRIOR TO PAVING SHALL BE CONSIDERED INCIDENTAL TO AR209510 CRUSHED AGGREGATE BASE COURSE AND NO ADDITIONAL COMPENSATION ALLOWED.

AREAS FOUND TO BE SOFT OR OTHERWISE UNSUITABLE FOR PAVING AND UNABLE TO REACH COMPACTION REQUIREMENTS SHALL BE UNDERCUT AND BACKFILLED WITH APPROVED MATERIAL TO THE SATISFACTION OF THE RESIDENT ENGINEER.

CRUSHED AGGREGATE BASE COURSE SHALL BE PLACED AND COMPACTED IN ISOLATED AREAS WHERE NECESSARY PRIOR TO PAVING. THE AGGREGATE IS INTENDED TO BE AVAILABLE FOR REPAIR AREAS OR FILLING HOLES IN THE AGGREGATE BASE AND SHALL BE PAID FOR UNDER ITEM AR209510 CRUSHED AGGREGATE BASE COURSE PER TON.

ONCE THE SUBGRADE IS ACCEPTED BY THE RESIDENT ENGINEER, THE CONTRACTOR SHALL RECONSTRUCT THE APRON IN ACCORDANCE WITH THESE CONSTRUCTION PLANS, THE SPECIFICATIONS, AND THE SPECIAL PROVISIONS.

BITUMINOUS CRACK REPAIR (AR201660)

THE ACTUAL LENGTH AND NUMBER OF CRACKS TO BE REPAIRED AS PART OF THIS PROJECT WILL BE DESIGNATED BY THE RESIDENT ENGINEER AFTER THE SURFACE MILLING IS COMPLETE. THE PROJECT QUANTITY IS BASED ON THE AMOUNT OF CRACKS IDENTIFIED ON A SURVEY AND ARE EXPECTED TO BE DEEPER THAN 2 INCHES.

THE CRACKS SHALL BE CUT OPEN USING AN "EARTH SAW" OR "VERMEER TRENCHER" AT A WIDTH OF 10" PLUS OR MINUS 1", AND A DEPTH TO THE AGGREGATE BASE COURSE 6"-8".

THE EXCAVATED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE BY THE CONTRACTOR.

AN APPLICATION OF BITUMINOUS TACK MATERIAL WILL BE APPLIED TO THE VERTICAL BITUMINOUS FACE AND THE AGGREGATE BASE. THE REPAIR TRENCH WILL BE BACKFILLED WITH BITUMINOUS SURFACE COURSE MATERIAL (401), IN LIFTS OF THICKNESS NO GREATER THAN 3". THE FINAL LIFT SHALL BE FLUSH WITH THE SURFACE OF THE EXISTING BITUMINOUS PAVEMENT OR THE CONTRACTOR WILL BE REQUIRED TO MILL THE FINAL LIFT FLUSH AT HIS OWN EXPENSE. EACH LIFT SHALL BE COMPACTED TO THE SATISFACTION OF THE RESIDENT ENGINEER AND ACCEPTED. NO NUCLEAR DENSITY TESTING SHALL BE REQUIRED FOR THIS TASK.

ANY DAMAGE TO THE EXISTING ADJACENT PAVEMENT WILL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE. THE QUANTITY OF PAVEMENT REMOVAL, MILLING AND/OR SAWCUTS, IF UTILIZED, TACK MATERIAL, AND BITUMINOUS SURFACE COURSE MATERIAL (401) NECESSARY TO COMPLETE THIS TASK WILL BE CONSIDERED INCIDENTAL TO THE TASK, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE BITUMINOUS CRACK REPAIR WILL BE PAID FOR UNDER ITEM: AR201660 "BITUMINOUS CRACK REPAIR" _____ 650 L.F.

REMOVE AND REPLACE BITUMINOUS PAVEMENT (AR201910)

THE AREAS REQUIRING THE REMOVAL AND REPLACEMENT OF BITUMINOUS PAVEMENT (AR201910) SHALL BE MARKED IN THE FIELD BY THE RESIDENT ENGINEER.

GENERAL NOTE

THE CONTRACTOR SHALL COMPLETE PAVEMENT MILLING, BITUMINOUS CRACK REPAIR, REMOVE AND REPLACE BITUMINOUS PAVEMENT (PATCHING), AND THE RECONSTRUCTION OF THE APRON UP THROUGH BITUMINOUS BASE PRIOR TO THE FINAL LIFT OF BITUMINOUS SURFACE COURSE. THE FINAL LIFT OF 401 SHALL THEN BE COMPLETED FOR UNIFORM SURFACE.

DATE	REVISION	BY

SHELBY COUNTY AIRPORT
 SHELBYVILLE, IL

IL PROJ.: 2H0-3781 A.I.P. PROJ.: 3-17-0093-B8

Revision No. 827-03THG	MLH	06/09/08
Element: R-102-PREP.DWG	ESC	06/18/08
Scale: 1"=30'	JDW	06/24/08
Date: 06/20/08		
LAYOUT		
DRAWN		
REVIEWED		



REHABILITATE
 T-HANGAR PAVEMENTS
 PROPOSED PAVEMENT
 PREPARATION PLAN