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STATE OF ILLINOIS

**DEPARTMENT OF TRANSPORTATION** 

**DIVISION OF HIGHWAYS** 

FOR INDEX OF SHEETS, SEE SHEET NO. 2

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PROPOSED HIGHWAY PLANS

F.A.U. ROUTE 1411 RANDOLPH STREET
LATHROP AVE. TO IL 43 (HARLEM AVE.)
SECTION: 2010–103–RS
RESURFACING (3P)
COOK COUNTY
C-91–093–11

IMPROVEMENT IS LOCATED IN THE VILLAGE OF FOREST PARK

R 12 E

IMPROVEMENT BEGINS
STA. 10+36

RESURFACING OMISSION
STA. 22+95 TO STA. 24+05

PARK

RESURFACING OMISSION
STA. 22+95 TO STA. 24+05

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION 1-800-892-0123
OR 811

PROJECT ENGINEER: DAN WILGREEN (847) 705–4240 PROJECT MANAGER: KEN ENG

CONTRACT NO. 60M26

GROSS LENGTH OF IMPROVEMENT = 2,510 FEET (0.48 MILES)
NET LENGTH OF IMPROVEMENT = 2,400 FEET (0.45 MILES)

D-91-093-11

| Day Continue | Day Co

LOCATION OF SECTION INDICATED THUS: - -

2010-103-RS

COOK /4 1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED

DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

DIVIDAD. BOSTONIA DE LA CONTROLLE DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

# STATE STANDARDS

	INDEX OF SHEETS
SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS. AND GENERAL NOTES
3-5	SUMMARY OF QUANTITIES
6	TYPICAL SECTIONS
7	ROADWAY & PAVEMENT MARKING PLANS
8	DETAILS FOR FRAMES AND LIDS WITH MILLING (BD-08)
9	PAVEMENT PATCHING FOR HOT-MIX ASPHALT SURFACED PAVEMENT (BD-22)
10	CUR8 OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
11	BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS (BD-32)
12	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11)
13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
14	ARTERIAL ROAD INFORMATION SIGNING (TC-22)

000001-06STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-03 PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424001-07 DIAGONAL CURB RAMPS FOR SIDEWALKS

424006-01 CORNER PARALLEL CURB RAMPS FOR SIDEWALKS

424011 - O/ MID-BLOCK CURB RAMPS FOR SIDEWALKS

424016 - 01 DEPRESSED CORNER FOR SIDEWALK

424021-02 CLASS C AND D PATCHES

604001-03 FRAME AND LIDS TYPE 1

701011-04 OFF-ROAD MOVING OPERATIONS 2L, 2W DAY ONLY

701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS

701501-06 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED

701311-03 LANE CLOSURE, 2L, 2W. MOVING DAY ONLY OPERATIONS

701801-05 SIDEWALK, CORNER OR CROSSWALK CLOSURE

701901-03 TRAFFIC CONTROL DEVICES

# GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. ( 48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE FOREST PARK.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H), WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

10 FEET (3 METER) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTERS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.

ALL PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.

TWO WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS, CONTACT JOE ECKERT, AREA TRAFFIC FIELD ENGINEER AT (224) 217-8632.

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO THE DISTRICT STANDARDS AS NOTED IN THE DETAIL.

THE UNIT WEIGHT (CONVERSION FACTOR) DUDTED IS FOR THE ESTIMATING PLAN QUANTITIES ONLY. ACTUAL QUANTITIES TO FULFILL CONTRACT REQUIREMENTS WILL BE DETERMINED BASED ON UNIT WEIGHT OF APPROVED MIX DESIGN, PLAN DIMENSIONS, AND DENSITY LIMITATIONS. MAXIMUM PAYMENT WILL BE COMPUTED BASED ON WEIGHT AVERAGE DENSITIES OF THE IN-PLACE MIXTURE.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

ALL SIDEWALK RAMPS WITHIN THE LIMITS OF THE PROJECT SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY ENGINEER.

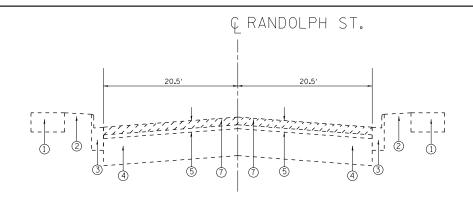
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40600400	MIXTURE FOR CRACKS, JOINTS,	TON	17	17						44000600	SIDEWALK REMOVAL	SO FT	3177	3177				
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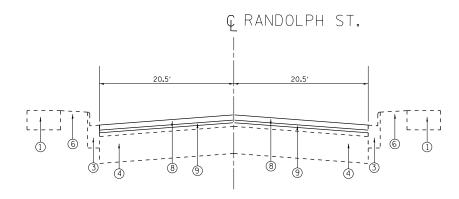
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70300260	TEMPORARY PAVEMENT MARKING	FOOT	678	678				***************************************		X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	200	200					
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EXISTING TYPICAL SECTION
RANDOLPH STREET
STA. 10+36 TO STA. 22+95
STA. 24+05 TO STA. 35+46
(LANTHROP AVE. TO HARLEM AVE.)



PROPOSED TYPICAL SECTION RANDOLPH STREET STA. 10+36 TO STA. 22+95 STA. 24+05 TO STA. 35+46 (LANTHROP AVE. TO HARLEM AVE.)

# LEGEND:

- 1) EXISTING CONCRETE SIDEWALK
- 2 EXISTING SODDING
- 3 EXISTING CURB & GUTTER B 6.12
- 4 EXISTING PCC PAVEMENT ±7"
- (5) EXISTING HMA OVERLAY ±3"
- (6) PROPOSED 4" TOPSOIL & SODDING (ADJACENT TO CURB REMOVALS)
- 7 PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- (8) PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm); 2"
- 9 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 1"

# \* NOTE: CONTRACTOR SHALL PATCH FIRST ACCORDING TO STANDARD DETAIL BD-22

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ N des	QUALITY MANAGEMENT PROGRAM (QMP)
RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm), 2"	4% @ 70 GYR	QCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"	3.5% @ 50 GYR	QC/QA
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm), 7"	4% @ 70 GYR	QC/QA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm), 3"	4% @ 70 GYR	QC/QA
OMP DESIGNATION: QUALITY CONTROL/QUALTTY ASSURANCE (QC/QA); QUAL	TIY CONTROL FOR PERFORM	IANCE (QCP)

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LB/SQ YD-IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

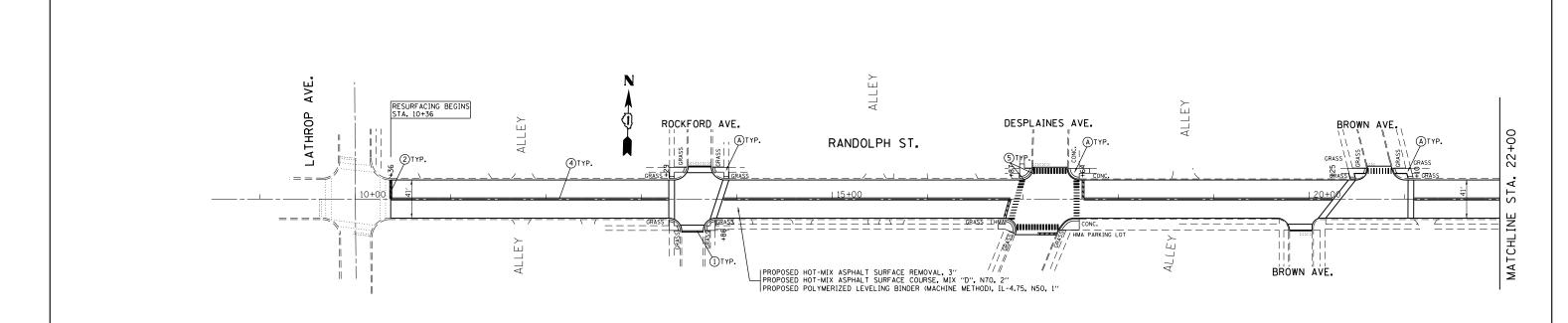
FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

FILE NAME =	USER NAME = abreuah	DESIGNED -	REVISED -	
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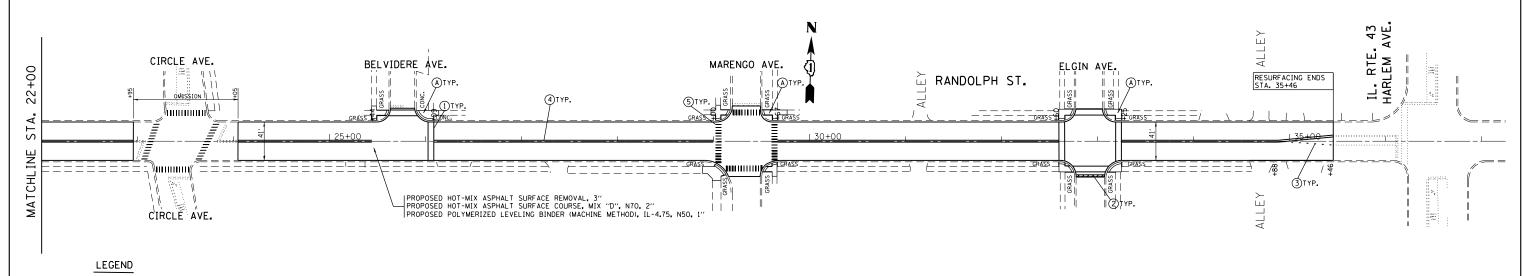
# STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

T	RANDOLPH ST. (FROM LANTHROP AVE. TO IL 43 / HARLEM AVE.)								SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		•	TVDI	CAL SECT	TONC		•	1411	2010-103-RS	COOK	14	6
L			ITPI	CAL SECT	ION2					CONTRACT	NO. 6	0м26
L	SCALE: NTS	SHEET NO.	OF	SHEETS	STA. X	TO STA.	X	FED. RC	AD DIST. NO. 1 ILLINOIS FED. A	D PROJECT		



# ADA RAMP LEGEND

A PROP. SIDEWALK REMOVAL PROP. PCC SIDEWALK 5" (SEE HWY STDS. 424001, 424006, 424011, 424016, 424021) PROP. DETECTABLE WARNINGS PROP. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL • SEE GENERAL NOTES



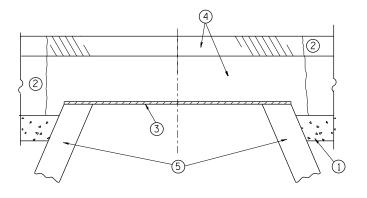
- 1 THERMOPLASTIC PAVEMENT MARKING 6" SOLID WHITE LINE
- 2 THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR
- 24" WHITE STOP BAR

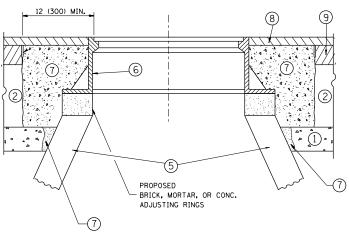
  3 THERMOPLASTIC PAVEMENT MARKING
  6" WHITE DOTTED EXTENSION
- 4 THERMOPLASTIC PAVEMENT MARKING 4" SOLID DOUBLE YELLOW
- (5) THERMOPLASTIC PAVEMENT MARKING 12" SOLID WHITE LINE

# <u>NOTES</u>

- 1. ALL FINAL PAYEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAYEMENT MARKINGS" DETAIL STANDARD TC-13.
- 2.ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)" STANDARD TC-11.
- 3. THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.

FILE NAME =	USER NAME = abreuah	DESIGNED -	REVISED -		RANI	IDOLPH STREET (LATHROP AVE. TO IL 43 /HARLEM AVE.)	F.A.U. RTF.	SECTION	COUNTY	TOTAL SHEET SHEETS NO.
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	PLOT SCALE = 100.0000 '/ in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	ROADWAY & PAVEMENT MARKING PLAN				CONTRACT	T NO. 60M26
	PLOT DATE = 5/29/2014	DATE -	REVISED -		SCALE:	SHEET NO. OF SHEETS STA. TO STA.		ILLINOIS FED. A	ID PROJECT	





#### NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109,04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED. THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

#### CONSTRUCTION PROCEDURES

#### STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.

  D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 11/2 (40)
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 11/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

## STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\*
  CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING
  BASE COURSE OR THE BINDER COURSE.
- \* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE FNGINFER."

## LEGEND

- 1 SUB-BASE GRANULAR MATERIAL
- 6) FRAME AND LID (SEE NOTES)
- 2 EXISTING PAVEMENT

(5) EXISTING STRUCTURE

- (7) CLASS PP-1\* CONCRETE
- 3 36 (900) DIAMETER METAL PLATE
- (8) PROPOSED HMA SURFACE COURSE
- PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- (9) PROPOSED HMA BINDER COURSE

# LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

## BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

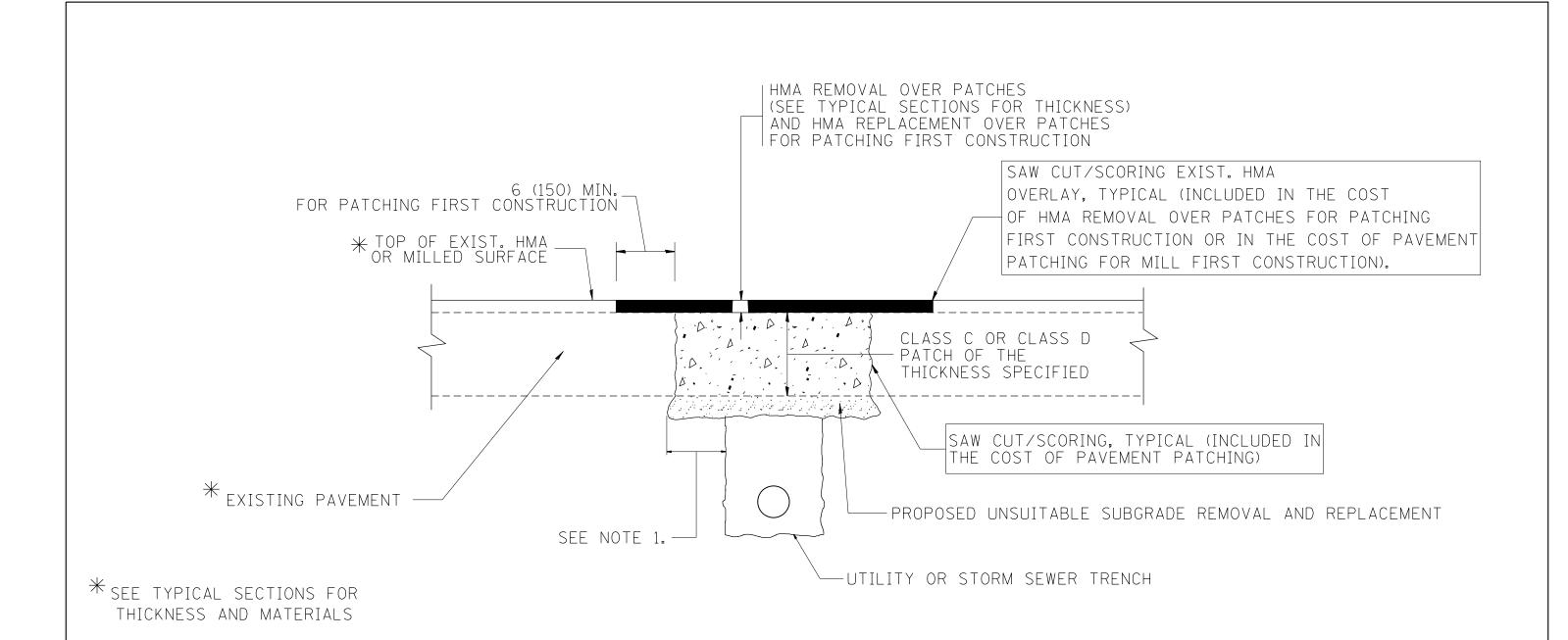
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

# DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

LE NAME = USER NAME = abreuah		DESIGNED	-	R. SHAH	REVISED	-	R. WIEDEM	AN 05-14-04	
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	DETAILS FO	R	F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	FRAMES AND LIDS ADJUSTM	ENT WITH MILLING	1411	2010-103-RS	соок	14	8
	FRANCES AND LIDS ADJUSTIN	EINI WIIN WILLING		BD600-03 (BD-8)	CONTRACT	NO. 6	ОМ26
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO ST	A. FED.	ROAD DIST, NO. 1 ILLINOIS FED. A	ID PROJECT		



# NOTES:

- 1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
- 2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

# SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

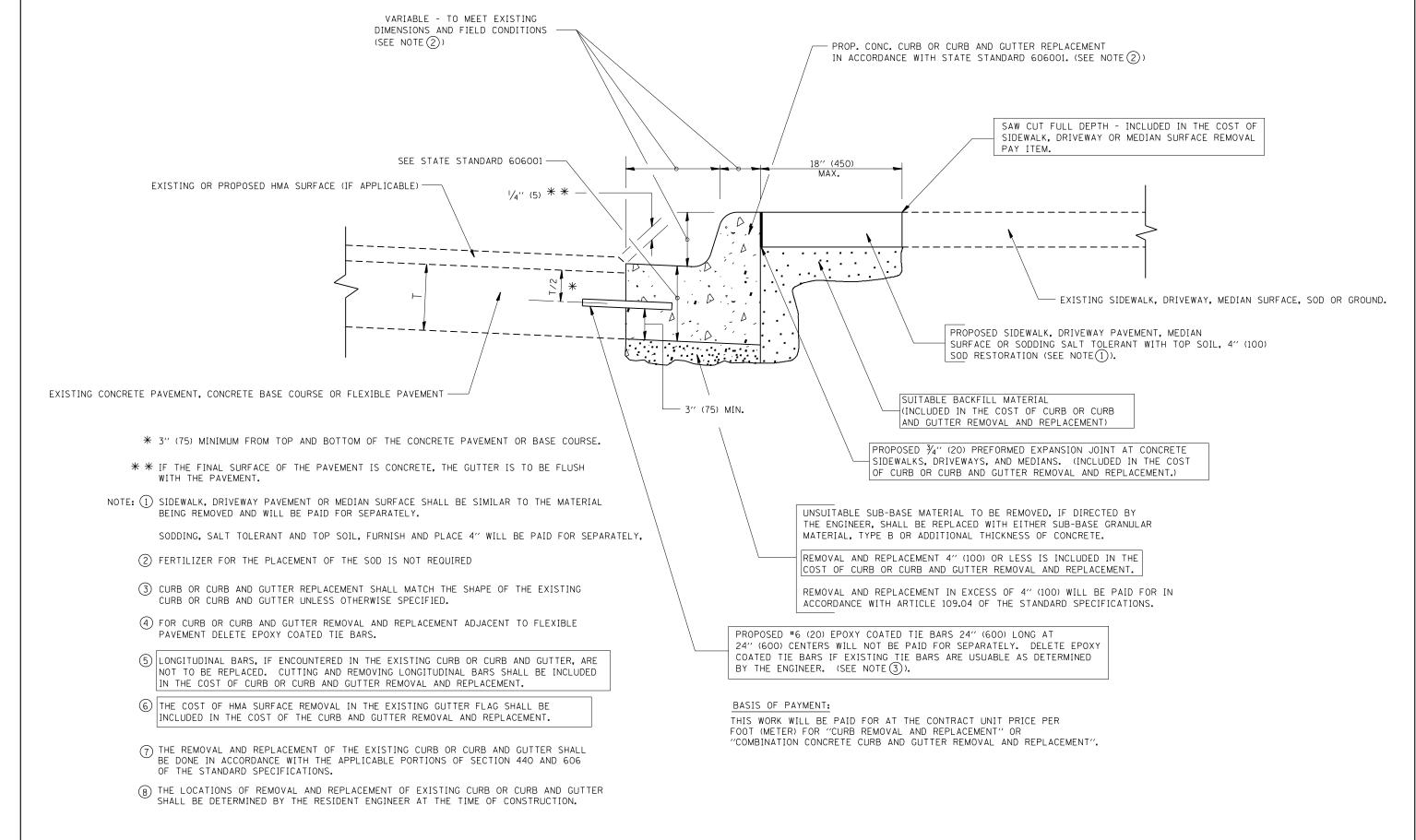
- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

# SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 41/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

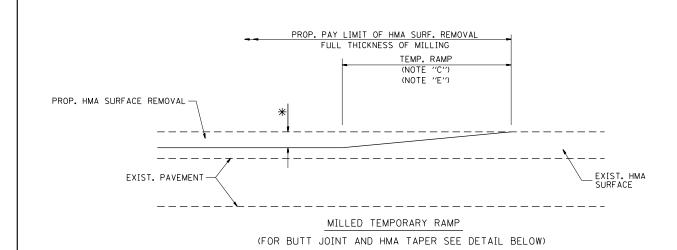
Γ	FILE NAME =	USER NAME = abreuah	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98		PAVEMENT PATCHING FOR	F.A.U. SECTION	COUNTY TOTAL SHEET
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		PLOT SCALE = 100.0000 '/ in.	CHECKED -	REVISED - R. BORO 09-04-07	DEPARTMENT OF TRANSPORTATION	HMA SURFACED PAVEMENT	BD400-04 (BD-22)	CONTRACT NO. 60M26
		PLOT DATE = 5/29/2014	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST, NO. 1   ILLINOIS FED. AI	



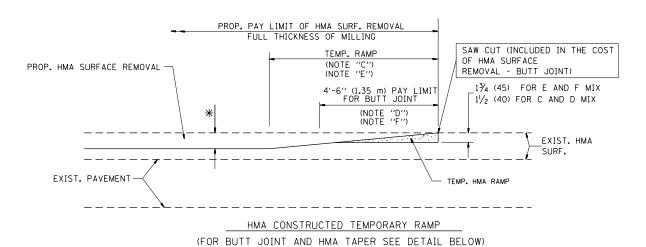
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

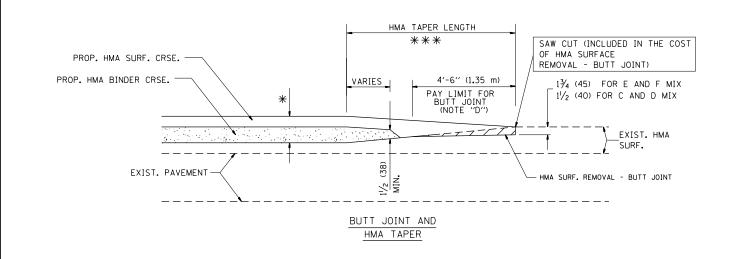
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FILE NAME =	USER NAME = abreuah	DESIGNED - A. HOUSEH	REVISED -	R. SHAH 10-03-96			CURB OR CURB AND GUTTER		F.A.U.	SECTION	COUNTY	SHEETS	NO.



# OPTION 1



# OPTION 2 TYPICAL TEMPORARY RAMP



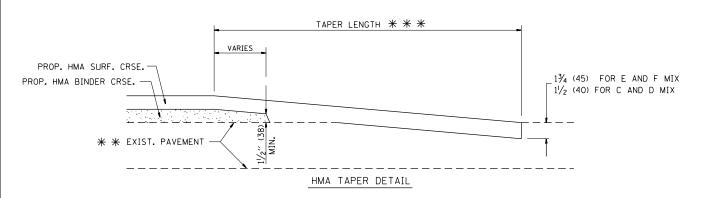
# TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS

OTHERWISE SHOWN.

# PROP. HMA OR PCC SURFACE REMOVAL - BUTT JOINT 30'-0" (9.0 m) (NOTE "A") 15'-0" (4.5 m) (NOTE "B") (NOTE "D") \*\* \* EXIST. PAVEMENT BUTT JOINT DETAIL



# TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

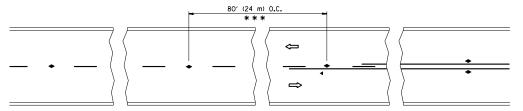
\* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

# NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- : MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

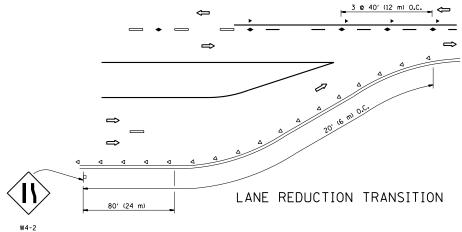
# BASIS OF PAYMENT:

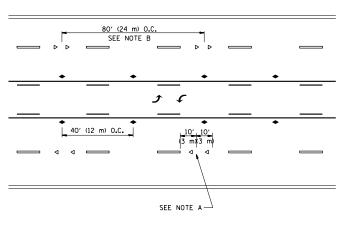
THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".



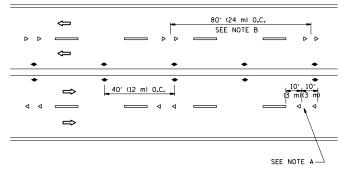
\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO-LANE/TWO-WAY

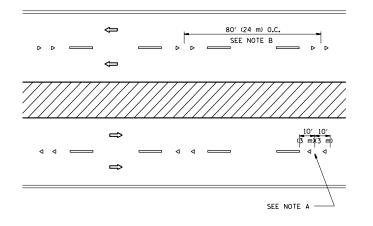




TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

# GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- 2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- 3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

# LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

# SYMBOLS

---- YELLOW STRIPE

---- WHITE STRIPE

- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (₩/O)
- ◆ TWO-WAY AMBER MARKER

# DESIGN NOTES

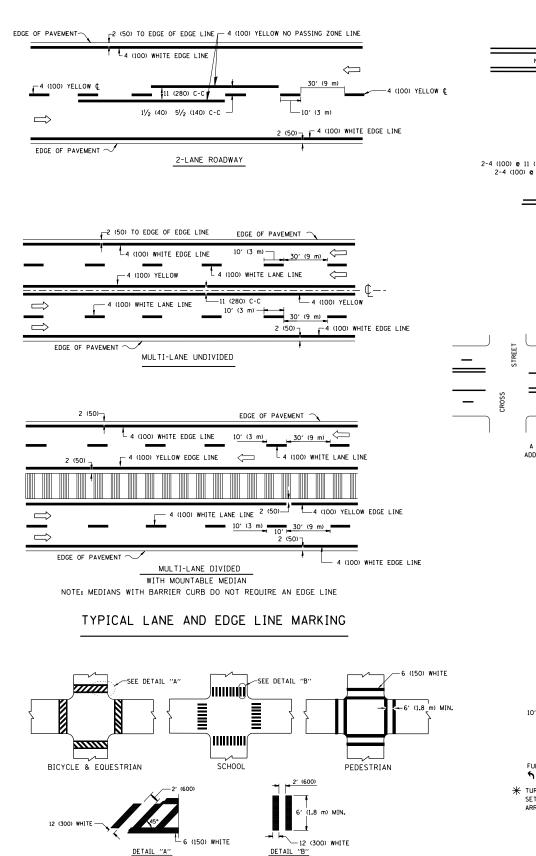
- 1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- 2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- 3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- 4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

# 

LEFT TURN

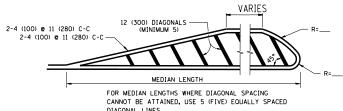
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT DATE = 5/29/2014	DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD	DIST. NO. 1 ILLINOIS FE	D. AID PROJECT	



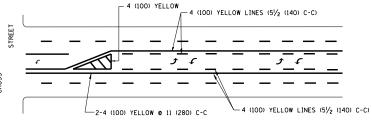
4' (1.2 m) OUTSIDE TO OUTSIDE OF LINES NO DIAGONALS \_\_ 2-4 (100) YELLOW @ 11 (280) C-C

## 4' (1.2 m) WIDE MEDIANS ONLY

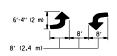


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

## MEDIANS OVER 4' (1.2 m) WIDE

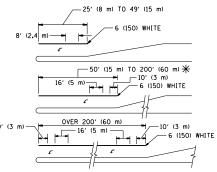


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

# TYPICAL PAINTED MEDIAN MARKING

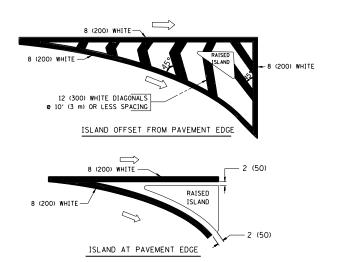


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  $\P$  AREA = 15.6 SO. FT. (1.5 m² )  $\P$  AREA = 20.8 SO. FT. (1.9 m²)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

## TYPICAL TURN LANE MARKING



# TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE			CDACING / DEMARKS
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 1280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW: EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 © 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH: 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART 5EE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1,2 m) N ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIACONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SO. FT. (0.33 m²) EACH "X"=54.0 SO. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) <b>@</b> 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (0VER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

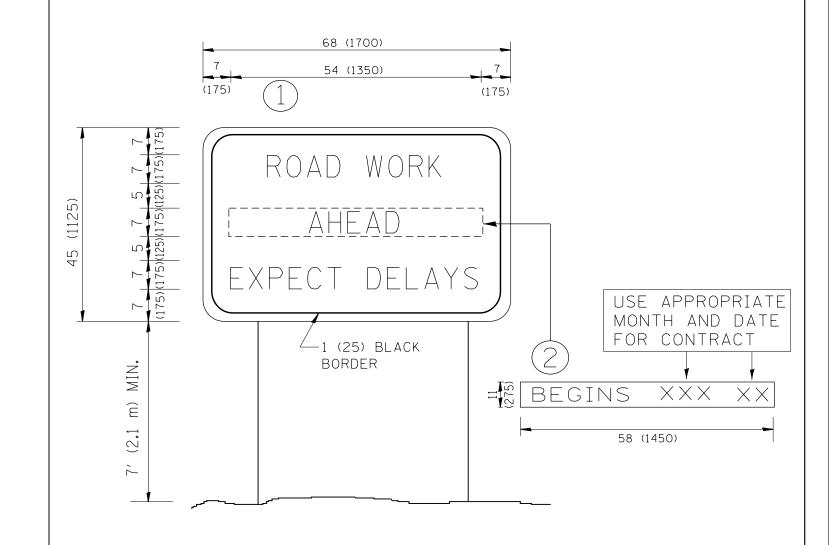
FILE NAME =	USER NAME = abreuah	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
c:\pw_work\pwidot\abreuah\d0246778\Dist	td.dgn	DRAWN -	REVISED -C. JUCIUS 09-09-09
	PLOT SCALE = 100.0000 '/ in.	CHECKED -	REVISED -
	PLOT DATE = 5/29/2014	DATE - 03-19-90	REVISED -

TYPICAL CROSSWALK MARKING

DETAIL "B"

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

	DISTRICT	ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
	TYPICAL PAVEMEN	MARKINGS		1411	2010-103-RS	соок	14	13		
ITFICAL FAVEWEINT WARKINGS					TC-13 CONTRACT NO.					
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. RO	DAD DIST. NO. 1   ILLINOIS FED. AI					



# NOTES:

- 1. USE BLACK LETTERING ON ORANGE BACKGROUND.
- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL 2 SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

Γ	FILE NAME =	USER NAME = abrevah	DESIGNED -	REVISED - R. MIRS 09-15-97			ARTERIAL ROAD		F.A.U. RTF	SECTION	COUNTY T	TOTAL SHEET
	c:\pw_work\pwidot\abreuah\d0246778\Dist	td.dgn	DRAWN -	REVISED - R. MIRS 12-11-97	STATE OF ILLINOIS				1411	2010-103-RS	соок	14 14
		PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -T. RAMMACHER 02-02-99			INFORMATION SIGN			TC-22	CONTRACT N	NO. 60M26
		PLOT DATE = 5/29/2014	DATE -	REVISED - C. JUCIUS 01-31-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA.	TO STA.	FED. ROAD D			