

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAP 305: US ROUTE 14
SECTION: 27R-2

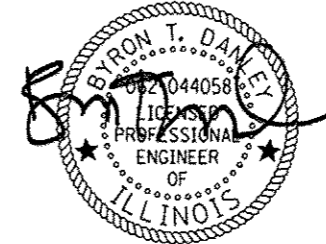
**RECONSTRUCTION & ADD LANE
WEST LAKE SHORE DRIVE TO LUCAS ROAD**

**MCHENRY COUNTY
C-91-293-01**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	1
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 62268	

*673 + 15 = 688

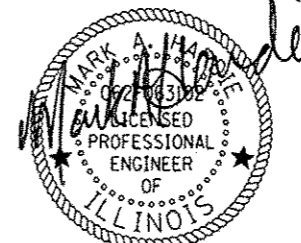
D-91-293-01



BYRON T. DANLEY, P.E.
IL REG NO. 062-044058
EXP U.S. SERVICES INC.
EXPIRES: 11-30-2015

DATE: 6/6/14

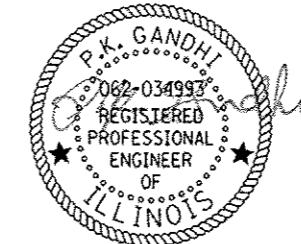
FOR SHEETS : 1 TO 285
: 382 TO 422
: 489 TO 673



MARK A. HARDIE, P.E.
IL REG NO. 062-063102
HR GREEN, INC.
EXPIRES: 11-30-2015

DATE: 6/5/14

FOR SHEETS : 458 TO 471



P.K. GANDHI, P.E.
IL REG NO. 062-034993
GANDHI AND ASSOCIATES INC.
EXPIRES: 11-30-2015

DATE: 6/5/2014

FOR SHEETS : 423 TO 457

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED June 6 2014

John Frutman
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

June 27 2014
John D. Baranzelli, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

June 27 2014
Omer Osman, P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

SUBSURFACE UTILITY ENGINEERING (S.U.E.)
UTILIZED ON THIS PROJECT

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE CITIES OF
WOODSTOCK AND CRYSTAL LAKE

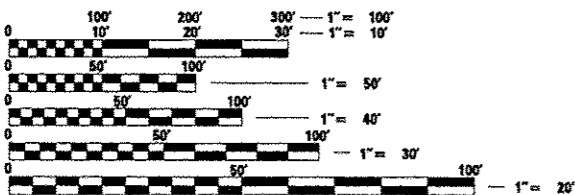
U.S. ROUTE 14
DESIGN DESIGNATION - 3360(22) ARTERIAL 11.47 (PCC-20)
ADT = 18,900 (2005) 32,000 (2030)
DESIGN SPEED = 50 MPH
POSTED SPEED = 45 MPH

LILY POND ROAD
ADT = 1,300 (2005) 2,000 (2030)
DESIGN SPEED = 35 MPH
POSTED SPEED = 30 MPH (UNPOSTED)

RIDGEFIELD ROAD NORTH
ADT = 1,050 (2005) 2,000 (2030)
DESIGN SPEED = 50 MPH
POSTED SPEED = 45 MPH

DOTY ROAD
ADT = 3,000 (2005) 6,000 (2030)
DESIGN SPEED = 45 MPH
POSTED SPEED = 40 MPH

LAKE SHORE DRIVE
ADT = 3,500 (2005) 5,000 (2030)
DESIGN SPEED = 35 MPH
POSTED SPEED = 30 MPH (UNPOSTED)



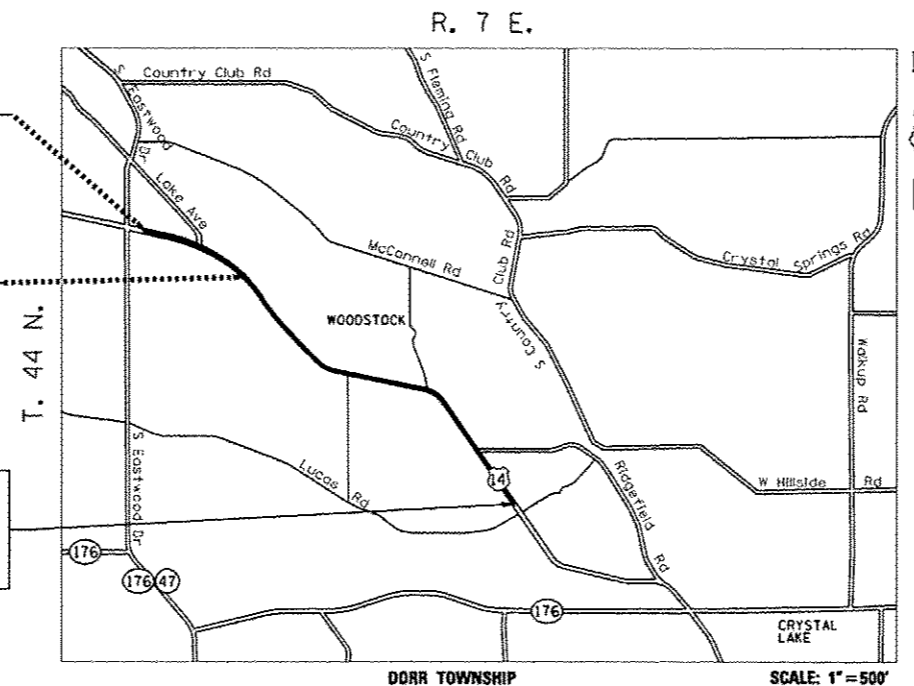
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT BEGINS
U.S. ROUTE 14
STA. 129 + 22 (EX)

STATION EQUATION
U.S. ROUTE 14
STA. 169 + 61.76 BK =
STA. 325 + 74.26 AH

PROJECT ENDS
U.S. ROUTE 14
STA. 493 + 00.00



GROSS & NET LENGTH OF PROJECT: 20,765.50 FEET = 3.933 MILES

PROJECT ENGINEER: CRAIG BAUER (847) 705-4265
PROJECT MANAGER: LONG TRAN (847) 705-4232

CONTRACT NO. 62268

exp U.S. Services Inc.
Chicago, IL
BUILDINGS-EARTH & ENVIRONMENT-ENERGY
INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY



GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 306
CHICAGO, ILLINOIS 60631 TEL. (773) 774-5910



**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS & STATE STANDARDS
3	GENERAL NOTES / COMMITMENTS
4 - 14	SUMMARY OF QUANTITIES
15 - 27	TYPICAL SECTIONS
28 - 40	SCHEDULE OF QUANTITIES
41 - 43	ALIGNMENT, TIES, AND BENCHMARKS
44 - 54	EXISTING ROADWAY AND REMOVAL PLANS
55 - 73	PROPOSED ROADWAY PLANS AND PROFILES
74 - 80	ROADWAY DETAILS
81 - 144	STAGING AND TRAFFIC CONTROL PLANS
145 - 207	STAGING AND TRAFFIC CONTROL CROSS SECTIONS
208 - 249	EROSION CONTROL AND SEDIMENT CONTROL PLANS
250 - 269	DRAINAGE AND UTILITY PLANS AND PROFILES
270 - 280	DRAINAGE SCHEDULE
281 - 285	DRAINAGE DETAILS
286 - 293	UTILITY PLANS BY SUE (SUBSURFACE UTILITY EXPLORATION)
294 - 381	PLAT OF HIGHWAYS
382 - 386	INTERSECTION DESIGN
387 - 412	PAVEMENT MARKING AND SIGNING PLANS
413 - 422	LANDSCAPING PLANS
423 - 457	TRAFFIC SIGNAL PLANS
458 - 471	CITY OF WOODSTOCK EXISTING AND PROPOSED UTILITY PLANS
472 - 488	DISTRICT ONE STANDARD DETAILS
489 - 673	CROSS SECTIONS

LIST OF HIGHWAY STANDARDS

REV. NO.	STD. NO.	DESCRIPTION
-06	000001	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
-07	280001	TEMPORARY EROSION CONTROL SYSTEMS
-07	420001	PAVEMENT JOINTS
-04	420101	24' (7.2 m) JOINTED PCC PAVEMENT
-04	420106	36' (10.8 m) JOINTED PCC PAVEMENT
-03	420111	PCC PAVEMENT ROUNDOUTS
-07	424001	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
-01	424031	MEDIAN PEDESTRIAN CROSSINGS
-03	442201	CLASS C AND D PATCHES
-02	482001	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
-03	482006	HMA SHOULDER ADJACENT TO RIGID PAVEMENT
-04	483001	PCC SHOULDER
-03	542301	PRECAST REINFORCED CONCRETE FLARED END SECTION
-02	542306	PRECAST REINFORCED CONCRETE ELLIPTICAL FLARED END SECTION
-04	601001	SUB-SURFACE DRAINS
-01	601101	CONCRETE HEADWALL FOR PIPE DRAIN
-02	602001	CATCH BASIN TYPE A
-02	602011	CATCH BASIN TYPE C
-04	602301	INLET-TYPE A
-03	602306	INLET-TYPE B
-03	602401	MANHOLE TYPE A
-06	602406	MANHOLE TYPE A 6' (1.8 m) DIAMETER
-04	602416	MANHOLE TYPE A 8' (2.4 m) DIAMETER
-02	602501	VALVE VAULT TYPE A
-03	602601	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
-02	602701	MANHOLE STEPS
-03	604001	FRAME AND LIDS TYPE 1
-04	604006	FRAME AND GRATE TYPE 3
-02	604036	GRATE TYPE 8
-02	604091	FRAME AND GRATE TYPE 24
-05	606001	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
-02	606006	OUTLETS FOR CONCRETE CURB AND GUTTER TYPE B-6.24 (B-15.60)
-04	606301	PC CONCRETE ISLANDS AND MEDIANS
-03	606306	CORRUGATED PC CONCRETE MEDIANS
-01	606401	PAVED DITCH
-01	635011	REFLECTOR MARKER AND MOUNTING DETAILS
-01	642006	SHOULDER RUMBLE STRIPS, 8 IN.
-02	701001	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 m) AWAY
-05	701006	OFF-RD OPERATIONS, 2L, 2W, (15') 4.5 M TO 24" (600mm) FROM PAVEMENT EDGE
-04	701011	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
-04	701101	OFF-RD OPERATIONS, MULTILANE, 15' (4.5m) TO 24" (600 mm) FROM PAVEMENT EDGE
-02	701106	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' (4.5m) AWAY
-04	701201	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS >= 45 MPH
-03	701206	LANE CLOSURE, 2L, 2W, NIGHT ONLY FOR SPEEDS >= 45 MPH
-04	701301	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
-03	701306	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45 MPH
-03	701311	LANE CLOSURE 2L, 2W MOVING OPERATIONS-DAY ONLY
-04	701326	LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS >= 45 MPH
-04	701331	LANE CLOSURE, 2L, 2W, WITH RUN-AROUND, FOR SPEEDS >= 45 MPH
-06	701336	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS >= 45 MPH
-06	701421	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY FOR SPEEDS >= 45 MPH TO 55 MPH
-06	701422	LANE CLOSURE, MULTILANE, FOR SPEEDS >= 45 MPH TO 55 MPH
-06	701426	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS >= 45 MPH
-06	701501	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
-09	701701	URBAN LANE CLOSURE, MULTILANE INTERSECTION
-03	701901	TRAFFIC CONTROL DEVICES
-07	704001	TEMPORARY CONCRETE BARRIER
-01	720001	SIGN PANEL MOUNTING DETAILS
-04	720006	SIGN PANEL ERECTION DETAILS
-03	720016	MAST ARM MOUNTED STREET NAME SIGNS
-01	728001	TELESCOPING STEEL SIGN SUPPORT
-01	731001	BASE FOR TELESCOPING STEEL SIGN SUPPORT
-04	780001	TYPICAL PAVEMENT MARKINGS
-01	805001	ELECTRICAL SERVICE INSTALLATION DETAILS
-02	814001	HANDHOLES
-02	814006	DOUBLE HANDHOLES
-01	857001	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
-01	862001	UNINTERRUPTABLE POWER SUPPLY (UPS)
-02	873001	TRAFFIC SIGNAL GROUNDING & BONDING
-05	877001	STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
-02	877002	SEETL MAST ARM ASSEMBLY AND POLE 56' THROUGH 75'
-09	878001	CONCRETE FOUNDATION DETAILS
-01	880001	SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION
-01	880006	TRAFFIC SIGNAL MOUNTING DETAILS
-01	886001	DETECTOR LOOP INSTALLATIONS

LIST OF DISTRICT ONE DETAILS

DETAIL NO.	DESCRIPTION
B001	DRIVEWAY DETAILS - DISTANCE BETWEEN ROW AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)
B002	DRIVEWAY DETAILS - DISTANCE BETWEEN ROW AND FACE OF CURB & EDGE OF SHOULDER < 15' (4.5 m)
B003	OUTLET FOR CONCRETE CURB AND GUTTER
B007	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER
B008	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
B032	BUTT JOINT AND HMA TAPER DETAILS
B036	FIRE HYDRANT TO BE MOVED
BD46	PCC PAVEMENT ROUNDOUTS AT CURB & GUTTER
B051	BENCHING DETAIL FOR EMBANKMENT WIDENING
B052	DETAIL OF PAVEMENT SEPARATION JOINT FOR JOINTED PCC PAVEMENTS AT INTERSECTIONS
TC10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC11	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
TC13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC14	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
TC16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
TC22	ARTERIAL ROAD INFORMATION SIGN
TC26	DRIVEWAY ENTRANCE SIGNING
TS05	STANDARD TRAFFIC SIGNAL DESIGN DETAILS

FILE NAME - T:\DOCUMENT\1134\9601\CT11\WORK\1134\02.H1.DGN
 RECHTBR
 11/07/2013 10:11:51 AM

FILE NAME - #FILE# exp U.S. Services Inc. Design & BUILDINGS - EARTH & ENVIRONMENT - ENERGY INDUSTRIAL INFRASTRUCTURE - SUSTAINABILITY	USER NAME - RECHTBR PLOT SCALE - #SCALE# PLOT DATE - #DATE#	DESIGNED - DRAWN - RJS CHECKED - BRH DATE - 11/01/13	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	INDEX OF SHEETS & HIGHWAY STANDARDS SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.	F.A.P. RTE. 305 SECTION 27R-2 COUNTY MCHENRY TOTAL SHEETS 673 SHEET NO. 2 CONTRACT NO. 62268 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT
---	---	---	--	--	--	--

Rev.

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.I.L.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRICAL, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
2. 10 FT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB & GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE CITY OF WOODSTOCK AND THE CITY OF CRYSTAL LAKE AND LOCAL AGENCIES.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5. THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING COOLING TIME FOR VARIOUS HMA LIFTS.
6. BARRICADES: TYPE I, TYPE II, AND TYPE III BARRICADES SHALL BE WEIGHTED IN A MANNER APPROVED BY THE MANUFACTURER SO THEY ARE NOT MOVED BY TRAFFIC.
7. WHERE ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
8. RESIDENT ENGINEER SHALL CONTACT DEBBIE HANLON (AREA TRAFFIC FIELD ENGINEER) 847-438-2300, TWO WEEKS PRIOR TO INSTALLING ANY PERMANENT PAVEMENT MARKINGS.
9. AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE STANDARD SPECIFICATIONS AND THE GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS ENCOUNTERED, THE SOIL SHALL BE REMOVED AND REPLACED WITH PGES OR EMBANKMENT AS DETERMINED BY THE GEOTECHNICAL ENGINEER. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE THE CONTRACTOR.
10. RADIUS OF CURB AND GUTTER IS SHOWN AT THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
11. THE REMOVAL OF EXISTING RIGHT-OF-WAY MARKERS THAT ARE WITHIN PROPOSED RIGHT-OF-WAY WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN CLEARING.
12. ON STATE STANDARDS 482001 AND 483001 AGGREGATE SUBGRADE IMPROVEMENT 12" (300 MM) SHALL BE USED AS THE IMPROVED SUBGRADE. THE ADDITIONAL THICKNESS OF AGGREGATE SUBGRADE UNDER THE SHOULDER SHALL BE INCLUDED IN THE COST PER 50 YARD (50 M) OF AGGREGATE SUBGRADE IMPROVEMENT 12".
13. CONTRACT 62517 IS EXPECTED TO RUN SIMULTANEOUSLY. TRAFFIC CONTROL AND OTHER CONSTRUCTION OPERATIONS BETWEEN THESE TWO CONTRACTS SHALL BE COORDINATED THROUGHOUT THE DURATION OF THE PROJECT IN ORDER TO PROVIDE A SAFE AND EFFICIENT WORK ENVIRONMENT. THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "TRAFFIC CONTROL AND PROTECTION (SPECIAL)."
14. THE MULTI-USE PATH AGGREGATE BASE COURSE TYPE B SHALL MEET ALL THE REQUIREMENTS FOR AGGREGATE BASE COURSE TYPE A EXCEPT THAT ADDING WATER TO THE AGGREGATE (BY USE OF A CONTROLLED AGGREGATE MIXING SYSTEM) BEFORE IT IS DEPOSITED ON THE SUBGRADE WILL NOT BE REQUIRED.
15. THE WIDTH SHOWN ON THE PROPOSED TYPICAL SECTIONS FOR AGGREGATE SHOULDERS, TYPE B SHALL BE USED FOR THE WIDTH OF MEASUREMENT.
16. SIX (6) INCHES OF TOPSOIL REMOVAL HAS BEEN SHOWN ON THE CROSS SECTIONS AND HAS BEEN INCLUDED IN TOPSOIL EXCAVATION AND PLACEMENT.

THE QUANTITY FOR TOPSOIL EXCAVATION SHOWN ON THE EARTHWORK SCHEDULE IS ESTIMATED TO HAVE A SURPLUS. SURPLUS TOPSOIL EXCAVATION SHALL BE DISPOSED OF ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS. REMOVAL AND DISPOSAL OF SURPLUS TOPSOIL EXCAVATION WILL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL ACCORDING TO SECTION 202 OF THE STANDARD SPECIFICATIONS. THE AREAS OF SURPLUS TOPSOIL EXCAVATION SHALL BE APPROVED BY THE ENGINEER.
17. THE REMOVAL AND DISPOSAL OF ALL FENCES, EXCEPT AT LOCATIONS DESIGNATED ON THE PLANS AS CHAIN LINK FENCE REMOVAL OR WOODEN FENCE REMOVAL, IS INCLUDED IN THE WORK FOR CLEARING.
18. ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND DISPOSED OF ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.
19. EXISTING AGGREGATE SHOULDERS AND AGGREGATE DRIVEWAYS ARE CLASSIFIED AS EARTH EXCAVATION.
20. THE CONTRACTOR SHALL VERIFY THE EXISTING ELEVATIONS SHOWN ON THE PLANS WHERE THE PROPOSED PROFILE MEETS THE EXISTING PAVEMENT. THE ENGINEER SHALL BE NOTIFIED OF DISCREPANCIES BETWEEN THE EXISTING AND PROPOSED PAVEMENT ELEVATIONS IN ADVANCE OF FINAL GRADING OPERATIONS. TO ALLOW APPROPRIATE ADJUSTMENTS TO BE MADE TO THE PROFILE, THE ELEVATIONS SHOWN ON THE PLANS ARE DERIVED FROM DIGITAL TERRAIN MODELING AND MAY REQUIRE ADJUSTMENT TO ENSURE A SMOOTH TRANSITION WHERE THE PROFILE MEETS THE EXISTING PAVEMENT.
21. BUTT JOINTS SHALL BE INSTALLED AT THE END OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

22. THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

GRANULAR MATERIALS	2.05	TONS/ CU YD
BITUMINOUS PRIME COAT	0.0004	TONS/ SQ YD
AGGREGATE PRIME COAT	0.002	TONS/ SQ YD
HMA RESURFACING	112	LBS/ SQ YD/ IN
SHORT TERM PAVEMENT MARKING	10	FT/ 100 FT OF APPLICATIONS
MIX FOR CRACK, JTS & FLGWYS	.0003	TONS/ SQ YD
SUPPLEMENTAL WATERING	3	GAL/ SQ YD/ APPLICATION
TEMPORARY DITCH CHECKS	10	FEET/ EACH

23. TRAFFIC CONTROL AND PROTECTION FOR CONSTRUCTION STAGING AT INTERSECTIONS, STREET RETURNS, DRIVEWAYS OR OTHER ISOLATED LOCATIONS NOT SPECIFICALLY SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS, BUT NECESSARY FOR CONSTRUCTION, SHALL BE INCLUDED IN THE CONTRACT UNIT LUMP SUM PRICE FOR "TRAFFIC CONTROL AND PROTECTION (SPECIAL)."

24. SHOULDER RUMBLE STRIPS, WHERE SHOWN ON THE PLANS, SHALL BE CONSTRUCTED ON THE RESURFACING PORTION OF U.S. ROUTE 14.

25. THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT 847-705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

26. THE MULTI-USE PATH (SHARED USE PATH) SHALL BE CONSTRUCTED ACCORDING TO THE ADAAG, THE ILLINOIS ACCESSIBILITY CODE, AND AS SHOWN ON THE PLANS.

THE MULTI-USE PATH (SHARED USE PATH) SHALL BE CONSTRUCTED WITH A RUNNING SLOPE THAT DOES NOT EXCEED 5%.

THE MULTI-USE PATH (SHARED USE PATH) SHALL BE CONSTRUCTED WITH A CROSS SLOPE THAT DOES NOT EXCEED 2 %, THE CROSS SLOPE SHALL BE A MINIMUM OF 1.5 % UNLESS OTHERWISE SHOWN ON THE PLANS.

27. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL PRIVATE AND COMMERCIAL ENTRANCES FOR PROPERTY ABUTTING THE HIGHWAY OR STREET BEING IMPROVED AT ALL TIMES DURING CONSTRUCTION. WHEN A PROPERTY HAS ONLY A SINGLE ENTRANCE, THE CONTRACTOR SHALL CONSTRUCT THE PROPOSED ENTRANCE ONE HALF AT A TIME IN ORDER TO MAINTAIN ACCESS. WHEN A PROPERTY HAS MULTIPLE ENTRANCES, ONE OF THE ENTRANCES SHALL REMAIN OPEN AT ALL TIMES.

28. SUBBASE GRANULAR MATERIAL, TYPE B 8' HAS BEEN PROVIDED FOR USE AT TEMPORARY PAVEMENT LOCATIONS FOR SUBGRADE SOILS THAT HAVE AN IBR LESS THAN 2.5. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH SUBBASE GRANULAR MATERIAL, TYPE B 8' TO PROVIDE A TOTAL 12' SUBBASE GRANULAR MATERIAL THICKNESS WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. IF SUBGRADE SOILS THAT HAVE AN IBR LESS THAN 2.5 ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATIONS WILL BE DUE TO THE CONTRACTOR.

29. THE CONTRACT PLANS SHOW THE GENERAL LOCATION OF THE PROPOSED RIGHT-OF-WAY AND TEMPORARY EASEMENTS. THE FINAL LOCATIONS OF PROPOSED RIGHT-OF-WAY AND TEMPORARY EASEMENTS ARE SHOWN ON THE SIGNED AND SEALED PLATS OF HIGHWAYS.

30. FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLANS.

31. CITY OF WOODSTOCK STANDARDS AND DETAILS INCLUDED IN THE PLANS ONLY PERTAIN TO CITY OF WOODSTOCK UTILITY RELOCATIONS.

32. THE LOCATIONS OF EXISTING WATER MAINS, GAS MAINS, SEWERS, ELECTRIC POWER LINES AND OTHER UTILITIES SHOWN ON THE PLANS ARE BASED ON FIELD INVESTIGATIONS AND THE BEST INFORMATION AVAILABLE, BUT THEY ARE NOT GUARANTEED. IT SHALL BE THE CONTRACTORS' RESPONSIBILITY TO ASCERTAIN THEIR EXACT LOCATIONS FROM THE UTILITY COMPANIES AND BY FIELD INSPECTION.

33. WET REFLECTIVE TEMPORARY TAPE TYPE III SHALL BE USED FOR TEMPORARY PAVEMENT MARKING ON THE FINAL WEARING SURFACE EXCEPT THAT EPOXY PAVEMENT MARKING SHALL BE USED FOR TEMPORARY PAVEMENT MARKING WHEN CONSTRUCTION STAGING REQUIRES THE TEMPORARY PAVEMENT MARKING TO BE IN PLACE OVER THE WINTER PERIOD.

WET REFLECTIVE TEMPORARY TAPE TYPE III SHOWN ON THE PLANS FOR ANY CONSTRUCTION STAGE THAT THE CONTRACTOR PROPOSES TO EXTEND OVER THE WINTER PERIOD SHALL MEAN TEMPORARY PAVEMENT MARKING. WET REFLECTIVE TEMPORARY TAPE TYPE III AND TEMPORARY PAVEMENT MARKING PAY ITEMS WILL BE PAID AT THEIR RESPECTIVE CONTRACT UNIT PRICES AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR CHANGES IN QUANTITIES TO COMPLY WITH THIS REQUIREMENT.

EPOXY PAVEMENT MARKING MAY BE USED IN PLACE OF WET REFLECTIVE TEMPORARY TAPE TYPE III, WHEN AUTHORIZED BY THE ENGINEER, FOR LATE SEASON APPLICATIONS WHERE TAPE ADHESION WOULD BE A PROBLEM.

REMOVAL OF TEMPORARY (EPOXY) PAVEMENT MARKING PLACED FOR THE WINTER PERIOD ON THE FINAL WEARING SURFACE WILL BE PAID FOR AS PAVEMENT MARKING REMOVAL.

34. A QUANTITY OF 9000 CU YD OF FURNISHED EXCAVATION HAS BEEN INCLUDED IN THE CONTRACT TO BALANCE THE LOSS OF THE NON-SPECIAL WASTE DISPOSAL QUANTITY. IF TESTING DETERMINES THAT SOME OF THE NON-SPECIAL WASTE MATERIAL CAN BE USED ON SITE AS EMBANKMENT, THE EXCESS FURNISHED EXCAVATION SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR. THE CONTRACTOR SHALL OBTAIN THE APPROVAL OF THE ENGINEER PRIOR TO PLACING FURNISHED EXCAVATION.

35. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

36. BEFORE BEGINNING OF THE PAVEMENT AND SHOULDER MILLING WORK ON US ROUTE 14 FROM STA. 129+22± (EX) TO STA. 169+61.76± (EX), THE CONTRACTOR SHALL RECORD AND RETAIN FOR FUTURE REFERENCE ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THEIR LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER. THE WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR HOT-MIX ASPHALT SURFACE REMOVAL OF THE THICKNESS SPECIFIED.

COMMITMENTS

1. COORDINATE WITH THE MCHENRY COUNTY CONSERVATION DISTRICT TO ENSURE PROPER SCHEDULING AND FUNDING OF THE PROPOSED 10-FOOT WIDE MULTI-USE TRAIL.
2. SCHOOL DISTRICTS 47, 155 AND 200 SHOULD BE NOTIFIED PRIOR TO THE START OF CONSTRUCTION IN ORDER TO COORDINATE THEIR BUS SCHEDULES.
3. THE MCHENRY COUNTY CONSERVATION DISTRICT SHALL BE NOTIFIED OF CHANGES TO THE PROJECT FUNDING. PLEASE CONTACT THE FOLLOWING:

ELIZABETH KESSLER
EXECUTIVE DIRECTOR
MCHENRY COUNTY CONSERVATION DISTRICT
18410 U.S. HIGHWAY 14
WOODSTOCK, IL 60098
PHONE: 815-338-MCCD (6223)

DRAINAGE NOTES

1. ALL OFFSETS AND RIM ELEVATIONS OF DRAINAGE STRUCTURES ARE TO THE CENTER OF FRAMES AND CRATES OR FRAMES AND LIDS, IF NOT SHOWN ON DRAWING 281. DRAINAGE DETAILS - I.

2. THE COST OF MAKING SEWER AND UNDERDRAIN CONNECTIONS TO DRAINAGE STRUCTURES AND BREAKING NEW HOLES INTO EXISTING OR PROPOSED DRAINAGE STRUCTURES SHALL BE CONSIDERED INCLUDED IN THE COST FOR DRAINAGE WORK.

3. BEFORE FINAL ACCEPTANCE OF THE PROJECT, ALL PROPOSED STORM SEWER LINES AND STRUCTURES SHALL BE CLEANED AS DIRECTED BY THE ENGINEER. THE COST FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICES FOR THE VARIOUS STORM SEWERS.

4. ALL STORM SEWER CONNECTIONS WITH PIPES 27 INCHES (675 MM) DIAMETER AND SMALLER SHALL BE MADE WITH PRECAST "TEE" OR "WYE" PIPES. FOR PROPOSED STORM SEWER PIPES LARGER THAN 27 INCHES (675 MM) DIAMETER, OPENINGS OF THE SPECIFIED DIAMETER SHALL BE MADE IN THE PIPE AT THE TIME IT IS MANUFACTURED. PRECAST "TEE" OR "WYE" PIPE CONNECTIONS FOR PROPOSED STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST FOR THE STORM SEWERS.

5. THERE ARE TWO LOCATIONS WHERE GROUND WATER TABLE IS HIGH AND MAY COME INTO CONFLICT WITH PROPOSED STORM SEWER INSTALLATION. THESE LOCATIONS ARE 366+00 TO 370+50 AND 421+50 TO 428+50. THE PRESENCE OF WATER DURING STORM SEWER INSTALLATION COULD CAUSE TRENCH INSTABILITY. THE CONTRACTOR SHALL TAKE SPECIAL PRECAUTIONS, SUCH AS DEWATERING OR TRENCH BRACING, DURING STORM SEWER INSTALLATION. THE CONTRACTOR SHALL NOTE THAT THE FLUCTUATIONS IN THE WATER TABLE CAN BE ANTICIPATED UPON VARIATIONS IN PRECIPITATION AND SURFACE RUNOFF.

6. THE CONTRACTOR SHALL INSTALL AND MAINTAIN TEMPORARY DRAINAGE AND TEMPORARY EROSION CONTROL THROUGHOUT STAGED CONSTRUCTION FOR THE DURATION OF THE CONTRACT. SEE EROSION CONTROL AND SEDIMENT CONTROL PLANS FOR STAGED CONSTRUCTION OF PROPOSED DRAINAGE AND TEMPORARY DRAINAGE.

7. FLARED END SECTIONS WILL BE INCLUDED IN THE LENGTH OF MEASUREMENT FOR PIPE CULVERT REMOVAL. THE REMOVAL OF GRATING FOR FLARED END SECTIONS WILL NOT BE MEASURED FOR PAYMENT, BUT THE COST OF THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR PIPE CULVERT REMOVAL.

8. AS SHOWN ON THE PLANS, SOME OF THE PROPOSED DRAINAGE STRUCTURES WILL BE STAGE CONSTRUCTED VERTICALLY TO FACILITATE CONSTRUCTION OPERATIONS. IF THE TOP SLAB OF THE STRUCTURE MUST BE PLACED AT AN INTERIM ELEVATION, THE TOP SLAB WILL BE REMOVED AND THE REMAINING PORTION OF THE STRUCTURE ADDED AND THE TOP SLAB RE-INSTALLED IN A LATER STAGE. THE COST OF STAGE CONSTRUCTING PROPOSED DRAINAGE STRUCTURES VERTICALLY WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER EACH FOR THE DRAINAGE STRUCTURE OF THE TYPE AND SIZE SPECIFIED ON THE PLANS.

9. FOR STORM SEWER CONSTRUCTED UNDER THE ROADWAY, BACKFILLING METHODS TWO AND THREE AUTHORIZED UNDER THE PROVISIONS OF ARTICLE 550.07 OF THE STANDARD SPECIFICATIONS WILL NOT BE ALLOWED.

FILE NAME	USER NAME	DESIGNED	REVISED	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				GENERAL NOTES			
exp U.S. Services Inc. Chicago, IL BUILDINGS - EARTH & ENVIRONMENT - ENERGY INDUSTRIAL - INFRASTRUCTURE - SUSTAINABILITY	HECHTBR	RJS		U.S. ROUTE 14				CONTRACT NO. 62268			
	PLOT SCALE	CHECKED	DATE	SCALE: N.T.S.				SHEET NO. OF SHEETS STA. TO STA.			
		BRH	11/01/13	SECTION				COUNTY			
				305				27R-2			
				F.A.P. RTE.				TOTAL SHEETS			
				305				673			
				FED. ROAD DIST. NO. 1				ILLINOIS (FED. AID PROJECT)			

URBAN

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE												
				100% STATE	0003	0021	0021	0021	0021	0021	0021	0021	0028	0043	0043	
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	598	598												
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	140,592	140,592												
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	6,167	6,167												
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	5,064	5,064												
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1,844	1,844												
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	691	691												
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	102,453	102,453												
70400100	TEMPORARY CONCRETE BARRIER	FOOT	3,038	3,038												
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1,913	1,913												
70600250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	8	8												
70600350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	6	6												
*72000100	SIGN PANEL - TYPE 1	SQ FT	994	830			20	45				15	84			
*72000200	SIGN PANEL - TYPE 2	SQ FT	62				40					22				
*72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	48	48												
*72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	35	35												

* SPECIALTY ITEM

C:\Projects\US141\SO-LucasRd-LongTran\080Station Design Files\Sheets\SU0208HW.DGN
 User: amb
 Date: 11/01/13
 Plot Scale: 1/8" = 1'-0"
 Plot Date: 6/18/2014

FILE NAME :	USER NAME :	DESIGNED :	REVISED :
C:\Projects\US141\SO-LucasRd-LongTran\080Station Design Files\Sheets\SU0208HW.DGN	amb	DRAWN - AMB	REVISED -
exp U.S. Services Inc Chicago IL	PLOT SCALE : 1/8" = 1'-0"	CHECKED - BRH	REVISED -
DEPT OF TRANSPORTATION INDUSTRIAL INFRASTRUCTURE SUSTAINABILITY	PLOT DATE : 6/18/2014	DATE - 11/01/13	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
U.S. ROUTE 14**

SUMMARY OF QUANTITIES - I			
SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	44
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				CONTRACT NO. 62268

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	URBAN										
				ROADWAY	FIBER OPTIC CONDUIT	TRAFFIC SIGNALS					MULTI-USE PATH	WATER MAIN AND SANITARY SEWER		
				100% STATE	100% MCHENRY COUNTY	US RTE 14 & DOTY ROAD 93.3% STATE 6.7% WOODSTOCK	US RTE 14 & LAKE SHORE DR 95% STATE 5% WOODSTOCK	EMERGENCY VEHICLE PREEMPTION 100% WOODSTOCK	INTERCONNECT 100% STATE	US RTE 14 & LAKE SHORE DR 90% STATE 10% WOODSTOCK	80% STATE 20% MCHENRY	100% WOODSTOCK	100% STATE	
				CONSTRUCTION CODE										
				0003	0021	0021	0021	0021	0021	0021	0021	0021	0021	0021
*72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	4	4										
*72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	7	7										
*72400710	RELOCATE SIGN PANEL - TYPE 1	SO FT	5	5										
*72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	1,433	1,433										
*73000100	WOOD SIGN SUPPORT	FOOT	212	212										
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	270	270										
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	9,230	9,230										
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	631	631										
*78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	942	942										
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	488	488										
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	152	152										
*78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	16,140									16,140		
*78005180	EPOXY PAVEMENT MARKING - LINE 24"	FOOT	34									34		
*78008200	POLYUREA PAVEMENT MARKING TYPE I - LETTERS AND SYMBOLS	SO FT	2,041	2,041										
*78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	32,974	32,974										

* SPECIALTY ITEM

C:\Projects\US14\LD-LucasRd-LongTran\AutoStation Design Files\Sheet\SU0000DW.DGN
 exp U.S. Services Inc.
 305
 2014
 05/18/2014

FILE NAME :	USER NAME : **user**	DESIGNED -	REVISED -
C:\Projects\US14\LD-LucasRd-LongTran\AutoStation Design Files\Sheet\SU0000DW.DGN	PROJ DATE : 05/18/2014	DRAWN - AMB	REVISED -
exp U.S. Services Inc.	PLOT SCALE : 100.0000 "/in	CHECKED - BRH	REVISED -
305	PLOT DATE : 05/18/2014	DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

SUMMARY OF QUANTITIES - 1				
SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	41
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62268	

CODE NO.	ITEM	UNIT	URBAN		TRAFFIC SIGNALS							MULTI-USE PATH		WATER MAIN AND SANITARY SEWER	
			TOTAL QUANTITY	100% STATE	ROADWAY 100% STATE	FIBER OPTIC CONDUIT 100% MCHENRY COUNTY	US RTE 14 & DOTY ROAD 93.3% STATE 6.7% WOODSTOCK	US RTE 14 & LAKE SHORE DR 95% STATE 5% WOODSTOCK	EMERGENCY VEHICLE PREEMPTION 100% WOODSTOCK	INTERCONNECT 100% STATE	US RTE 14 & LAKE SHORE DR 90% STATE 10% WOODSTOCK	80% STATE 20% MCCD	100% WOODSTOCK	100% STATE	
			100% STATE	CONSTRUCTION CODE	0003	0021	0021	0021	0021	0021	0021	0028	0043	0043	
44200120	PAVEMENT PATCHING, TYPE II, 10 INCH	SO YD	15										15		
44200124	PAVEMENT PATCHING, TYPE III, 10 INCH	SO YD	131	102										29	
44200126	PAVEMENT PATCHING, TYPE IV, 10 INCH	SO YD	290	290											
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	640	640											
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	144	144											
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	78	78											
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	1,494	1,494											
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SO YD	2,563	2,563											
48101600	AGGREGATE SHOULDERS, TYPE B 8"	SO YD	449	449											
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	236	236											
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SO YD	1,341	1,341											
48300415	PORTLAND CEMENT CONCRETE SHOULDERS 9 3/4"	SO YD	19,911	19,911											
50105220	PIPE CULVERT REMOVAL	FOOT	1,679	1,679											
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	13	13											
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	18	18											

* SPECIALTY ITEM

C:\Projects\US14\LSO-LucasRd-LongTran\Station Design Files\Sheet\SU0008HW.DGN
 exp U.S. Services Inc.
 CHINA
 exp
 DESIGN, EARTH & ENVIRONMENT, ENERGY
 INDUSTRIAL INFRASTRUCTURE SUSTAINABILITY

FILE NAME :	USER NAME : **user**	DESIGNED -	REVISED -
C:\Projects\US14\LSO-LucasRd-LongTran\Station Design Files\Sheet\SU0008HW.DGN	DRAWN - AMB	REVISIONS	
exp U.S. Services Inc.	CHECKED - BRH	DATE - 11/01/13	REVISED -
CHINA	PLOT SCALE * 100.0000 ** / in.		
exp	PLOT DATE * 6/19/2014		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

SUMMARY OF QUANTITIES - I			
SCALE: N.T.S.	SHEET NO. OF	SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	40
CONTRACT NO. 62268			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY 100% STATE	URBAN															
				ROADWAY	FIBER OPTIC CONDUIT	TRAFFIC SIGNALS					MULTI-USE PATH	WATER MAIN AND SANITARY SEWER							
				100% STATE	100% MCHENRY COUNTY	US RTE 14 & DOTY ROAD 93.3% STATE 6.7% WOODSTOCK	US RTE 14 & LAKE SHORE DR 95% STATE 5% WOODSTOCK	EMERGENCY VEHICLE PREEMPTION 100% WOODSTOCK	INTERCONNECT 100% STATE	US RTE 14 & LAKE SHORE DR 90% STATE 10% WOODSTOCK	80% STATE 20% MCCD	100% WOODSTOCK	100% STATE						
CONSTRUCTION CODE										0003	0021	0021	0021	0021	0021	002B	0043	0043	
60240301	INLETS, TYPE B, TYPE B GRATE	EACH	15	15															
60240328	INLETS, TYPE B, TYPE 24 FRAME AND GRATE	EACH	56	56															
*60248700	VALVE VAULTS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2															2	
*60248900	VALVE VAULTS, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3															3	
60250400	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	2	2															
60250500	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1	1															
60255700	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	1															
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1	1															
*60500405	FILLING VALVE VAULTS	EACH	1																1
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	8	8															
60600605	CONCRETE CURB, TYPE B	FOOT	195	195															
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	2,536	2,536															
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	48,282	48,282															
60608552	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06	FOOT	140	140															
60608582	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24	FOOT	200	200															

* SPECIALTY ITEM

C:\Projects\US14150-LucasRd-LongTran\MapStation Design Files\Sheet\SUDD0001.DGN
 exp U.S. Services Inc.
 01/18/2014
 11/01/13

FILE NAME :	USER NAME : #user#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	SUMMARY OF QUANTITIES -- 1				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
C:\Projects\US14150-LucasRd-LongTran\MapStation Design Files\Sheet\SUDD0001.DGN	exp U.S. Services Inc.	DRAWN - AMB	REVISED -		305	27R-2	MCHENRY	673	41.				
PLOT SCALE * 100.0000 ' / in	CHECKED - BRH	REVISIED -			SCALE: N.T.S.	SHEET NO. OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT	CONTRACT NO. 62268		
PLOT DATE * 6/18/2014	DATE - 11/01/13	REVISED -											

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	URBAN									
				ROADWAY	FIBER OPTIC CONDUIT	TRAFFIC SIGNALS					MULTI-USE PATH	WATER MAIN AND SANITARY SEWER	
				100% STATE	100% MCHENRY COUNTY	US RTE 14 & DOTY ROAD 93.3% STATE 6.7% WOODSTOCK	US RTE 14 & LAKE SHORE DR 95% STATE 5% WOODSTOCK	EMERGENCY VEHICLE PREEMPTION 100% WOODSTOCK	INTERCONNECT 100% STATE	US RTE 14 & LAKE SHORE DR 90% STATE 10% WOODSTOCK	80% STATE 20% MCHENRY	100% WOODSTOCK	100% STATE
CONSTRUCTION CODE													
100% STATE													
0003 0021 0021 0021 0021 0021 0021 0021 0021 0021 0021 0021													
•88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	19			3	10				6		
•88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	7			4	3						
•88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	4								4		
•88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	7			3					4		
•88030210	SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	3				3						
•88030220	SIGNAL HEAD, LED, 2-FACE, 5-SECTION, BRACKET MOUNTED	EACH	1			1							
•88030240	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	1			1							
•88102710	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED	EACH	4				2				2		
•88102740	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED	EACH	1				1						
•88200510	TRAFFIC SIGNAL BACKPLATE, RETROREFLECTIVE	EACH	26			6	10				10		
•88500100	INDUCTIVE LOOP DETECTOR	EACH	25			7	9				9		
•88600100	DETECTOR LOOP, TYPE I	FOOT	1,785	1,101			684						
•88600700	PERFORMED DETECTOR LOOP	FOOT	1,592			671	117				804		
•88700200	LIGHT DETECTOR	EACH	2								2		
•88700300	LIGHT DETECTOR AMPLIFIER	EACH	1								1		

* SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	URBAN																	
				CONSTRUCTION CODE																	
				100% STATE	0003	0021	0021	0021	0021	0021	0021	0021	0021	0021							
*A2002816	TREE, CATALPA SPECIOSA (NORTHERN CATALPA), 2" CALIPER, BALLED AND BURLAPPED	EACH	15	15																	
*A2002920	TREE, CELTIS OCCIDENTALIS (COMMON HACKBERRY), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	3	3																	
*A2004610	TREE, GLEDITSIA TRIACANTHOS INERMIS PERFECTION (PERFECTION THORNLESS HONEYLOCUST), 2" CALIPER, BALLED AND BURLAPPED	EACH	13	13																	
*A2004716	TREE, GLEDITSIA TRIACANTHOS INERMIS SHADEMASTER (SHADEMASTER THORNLESS COMMON HONEYLOCUST), 2" CALIPER, BALLED AND BURLAPPED	EACH	10	10																	
*A2005015	TREE, GYMNOCLADUS DIOICUS (KENTUCKY COFFEETREE), 8' HEIGHT, MULTI-STEM FORM, BALLED AND BURLAPPED	EACH	10	10																	
*A2005020	TREE, GYMNOCLADUS DIOICUS (KENTUCKY COFFEETREE), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	9	9																	
*A2005036	TREE, GYMNOCLADUS DIOICUS EXPRESSO (EXPRESSO KENTUCKY COFFEETREE), 1-3/4" CALIPER, BALLED AND BURLAPPED	EACH	4	4																	
*A2005516	TREE, NYSSA SYLVATICA (BLACK TUPELO), 2" CALIPER, BALLED AND BURLAPPED	EACH	16	16																	
*A2006416	TREE, QUERCUS ALBA (WHITE OAK), 2" CALIPER, BALLED AND BURLAPPED	EACH	8	8																	
*A2006520	TREE, QUERCUS BICOLOR (SWAMP WHITE OAK), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	7	7																	
*A2006574	TREE, QUERCUS BICOLOR (SWAMP WHITE OAK), 10' HEIGHT, CLUMP FORM, BALLED AND BURLAPPED	EACH	3	3																	
*A2006720	TREE, QUERCUS MACROCARPA (BUR OAK), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	23	23																	
*A2006820	TREE, QUERCUS MUEHLENBERGII (CHINKAPIN OAK), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	8	8																	
*A2007210	TREE, QUERCUS X MACDANIELLI CLEMONS (HERITAGE OAK), 2" CALIPER, BALLED AND BURLAPPED	EACH	6	6																	
*A2008519	TREE, ULMUS MORTON GLOSSY (TRIUMPH ELM), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	9	9																	

* SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																
				URBAN																
				ROADWAY	FIBER OPTIC CONDUIT	TRAFFIC SIGNALS				MULTI-USE PATH	WATER MAIN AND SANITARY SEWER									
100% STATE	100% MCHENRY COUNTY	US RTE 14 & DOTY ROAD 93.3% STATE 6.7% WOODSTOCK	US RTE 14 & LAKE SHORE DR 95% STATE 5% WOODSTOCK	EMERGENCY VEHICLE PREEMPTION 100% WOODSTOCK	INTERCONNECT 100% STATE	US RTE 14 & LAKE SHORE DR 90% STATE 10% WOODSTOCK	80% STATE 20% MCHENRY	100% WOODSTOCK	100% STATE											
•A2012116	TREE, AESCULUS X AUTUMN SPLENDOR (AUTUMN SPLENDOR BUCKEYE), 2" CALIPER, BALLED AND BURLAPPED	EACH	5	5																
•A2016616	TREE, QUERCUS ELLIPSOIDALIS (HILL'S OAK), 2" CALIPER, BALLED AND BURLAPPED	EACH	8	8																
•A2016818	TREE, QUERCUS SCHUETTI (SWAMP BUR OAK), 1-3/4" CALIPER, BALLED AND BURLAPPED	EACH	6	6																
•A2018720	TREE, ULMUS CARPINIFOLIA MORTON, (ACCOLADE ELM), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	6	6																
•A2064012	TREE, QUERCUS ALBA X ROBUR CRIMSCHMIDT (CRIMSON SPIRE OAK), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	5	5																
•B2001616	TREE, CRATAEGUS CRUSGALLI INERMIS (THORN LESS COCKSPUR HAWTHORN), 2" CALIPER, TREE FORM, BALLED AND BURLAPPED	EACH	8	8																
•B2001666	TREE, CRATAEGUS CRUSGALLI INERMIS (THORNLESS COCKSPUR HAWTHORN), 6' HEIGHT, SHRUB FORM, BALLED AND BURLAPPED	EACH	8	8																
•B2004566	TREE, MALUS RED JEWEL (RED JEWEL CRAB APPLE), 6' HEIGHT, CLUMP FORM, BALLED AND BURLAPPED	EACH	6	6																
•B2006220	TREE, SYRINGA RETICULATA (JAPANESE TREE LILAC), 2-1/2" CALIPER, TREE FORM, BALLED AND BURLAPPED	EACH	18	18																
•C2012024	SHRUB, VIBURNUM DENTATUM SYNNESTVEDT (CHICAGO LUSTRE ARROWWOOD VIBURNUM), 2' HEIGHT, BALLED AND BURLAPPED	EACH	44	44																
•D2003776	EVERGREEN, THUJA OCCIDENTALIS SMARAGD (EMERALD GREEN AMERICAN ARBORVITAE), 5' HEIGHT, BALLED AND BURLAPPED	EACH	65	65																
•K0012990	PERENNIAL PLANTS, ORNAMENTAL TYPE, GALLON POT	UNIT	5	5																
•K0013030	PERENNIAL PLANTS, WETLAND TYPE, 2" DIAMETER BY 4" DEEP PLUG	UNIT	20	20																
•K0026850	PERENNIAL PLANT CARE	SD YD	492	492																
•K0029632	WEED CONTROL, NON-SELECTIVE AND NON-RESIDUAL	GALLON	5	5																

• SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY 100% STATE	TRAFFIC SIGNALS												
				ROADWAY	FIBER OPTIC CONDUIT	US RTE 14 & DOTY ROAD		US RTE 14 & LAKE SHORE DR		EMERGENCY VEHICLE PREEMPTION	INTERCONNECT 100% STATE	US RTE 14 & LAKE SHORE DR		MULTI-USE PATH	WATER MAIN AND SANITARY SEWER	
				100% STATE	100% MCHENRY COUNTY	93.3% STATE 6.7% WOODSTOCK	95% STATE 5% WOODSTOCK	100% WOODSTOCK	100% STATE	90% STATE 10% WOODSTOCK	80% STATE 20% MCHENRY	100% WOODSTOCK	100% STATE			
CONSTRUCTION CODE																
				0003	0021	0021	0021	0021	0021	0021	0028	0043	0043			
*K0029634	WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE	POUND	10	10												
*K1001970	CA-7 WASHED GRAVEL FOR RAIN GARDEN	CU YD	328	328												
K1005465	SELECTIVE MOWING STAKES	EACH	1	1												
X0300266	WOODEN FENCE REMOVAL	FOOT	260	260												
X0322464	ABANDON AND FILL EXISTING SANITARY MANHOLE	EACH	2											2		
X0322791	FILL EXISTING SANITARY SEWERS	CU YD	151										31	120		
X0322938	TEMPORARY END SECTION	EACH	9	9												
X0323814	SANITARY SEWER REMOVAL, 18"	FOOT	193										193			
X0324079	EXISTING FIELD TILE REMOVAL	FOOT	201	201												
*X0324085	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	1,711						1,711							
X0324085	MANHOLES, TYPE A, SANITARY, 4' - DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	7											7		
X0326713	SANITARY SEWER CONNECTION	EACH	3										2	1		
X0326802	UTILITY STRUCTURE REMOVAL	EACH	8										3	5		
X0326981	ENGINEERED SOIL FURNISH AND PLACE (SPECIAL)	CU YD	328	328												
X0487800	SANITARY SEWER REMOVAL 12"	FOOT	70											70		

* SPECIALTY ITEM

***ref: .itl...
 ***ref: .itl...
 ***ref: .itl...

FILE NAME =	USER NAME =	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	SUMMARY OF QUANTITIES - VI				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
C:\Projects\US14\USD-LucasRd-LongTran\MaproStation Design Files\Sheet\SU000-13.DGN	exp U.S. Services Inc.	DRAWN - AMB	REVISED -						305	27R-2	MCHENRY	673	9
PLT SCALE = 100.0000' / 1"	CHIEF: U.S. SERVICES INC.	CHECKED - BRH	REVISED -		CONTRACT NO. 62268				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
PLT DATE = 6/10/2014	INDUSTRIAL INFRASTRUCTURE SUSTAINABILITY	DATE - 11/01/13	REVISED -		SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA.	TO STA.				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY 100% STATE	URBAN											
				ROADWAY		FIBER OPTIC CONDUIT		TRAFFIC SIGNALS				MULTI-USE PATH		WATER MAIN AND SANITARY SEWER	
				100% STATE	100% MCHENRY COUNTY	US RTE 14 & DOTY ROAD 93.3% STATE 6.7% WOODSTOCK	US RTE 14 & LAKE SHORE DR 95% STATE 5% WOODSTOCK	EMERGENCY VEHICLE PREEMPTION 100% WOODSTOCK	INTERCONNECT 100% STATE	US RTE 14 & LAKE SHORE DR 90% STATE 10% WOODSTOCK	80% STATE 20% MCCD	100% WOODSTOCK	100% STATE		
CONSTRUCTION CODE															
				0003	0021	0021	0021	0021	0021	0021	0028	0043	0043		
X0840000	SANITARY SEWER REMOVAL 8"	FOOT	70									50	20		
X4020900	AGGREGATE SURFACE COURSE, TYPE B SPECIAL	SO YD	251	251											
X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	23	23											
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	7	7											
X4024100	TEMPORARY ACCESS (WINTERIZE)	SQ YD	261	261											
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SO YD	892	892											
X4402020	CONCRETE MEDIAN SURFACE REMOVAL	SO FT	3,121	3,121											
X4402800	ISLAND PAVEMENT REMOVAL	SO YD	158	158											
X4811700	AGGREGATE SHOULDERS (SPECIAL)	CU YD	742	742											
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	272	272											
X5537900	STORM SEWERS TO BE CLEANED 15"	FOOT	29	29											
X5538000	STORM SEWERS TO BE CLEANED 18"	FOOT	162	162											
X5538200	STORM SEWERS TO BE CLEANED 24"	FOOT	71	71											
X5610651	ABANDON EXISTING WATER MAIN, FILL WITH CLSM	FOOT	958										958		
X5610700	WATER MAIN REMOVAL	FOOT	1,024									558	466		

* SPECIALTY ITEM

C:\Projects\US14\LSO-LucasRd-LongTran\MacroStation Design Files\Sheet\SU000.W3.DGN
 exp U.S. Services Inc.
 CHECKED - BRH
 DATE - 11/01/13
 DESIGNED - AMB
 REVISIONS -
 PLOT SCALE = 100.0000' / 1"

FILE NAME :	USER NAME : ##user##	DESIGNED -	REVISIONS -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	SUMMARY OF QUANTITIES - VII				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
C:\Projects\US14\LSO-LucasRd-LongTran\MacroStation Design Files\Sheet\SU000.W3.DGN	DRAWN - AMB	CHECKED - BRH	DATE - 11/01/13		SCALE: N.T.S.	SHEET NO.	OF	SHEETS	STA.	TO STA.	305	27R-2	MCHENRY	673	10
exp U.S. Services Inc.															
CHECKED - BRH															

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT CONTRACT NO. 62268

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	URBAN											
				ROADWAY		FIBER OPTIC CONDUIT		TRAFFIC SIGNALS				MULTI-USE PATH		WATER MAIN AND SANITARY SEWER	
				100% STATE	100% MCHENRY COUNTY	US RTE 14 & DOTY ROAD 93.3% STATE 6.7% WOODSTOCK	US RTE 14 & LAKE SHORE DR 95% STATE 5% WOODSTOCK	EMERGENCY VEHICLE PREEMPTION 100% WOODSTOCK	INTERCONNECT 100% STATE	US RTE 14 & LAKE SHORE DR 90% STATE 10% WOODSTOCK	80% STATE 20% MCHENRY	100% WOODSTOCK	100% STATE	100% STATE	
CONSTRUCTION CODE															
				0003	0021	0021	0021	0021	0021	0021	0028	0043	0043		
X5630010	CUT AND CAP EXISTING 10" WATER MAIN	EACH	3									3			
X5630012	CUT AND CAP EXISTING 12" WATER MAIN	EACH	1										1		
X5630708	CONNECTION TO EXISTING WATER MAIN 8"	EACH	3									1	2		
X6020096	MANHOLES, TYPE A, 6'-DIAMETER, WITH 2 TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE	EACH	8	8											
X6061100	CONCRETE MEDIAN, TYPE SB (SPECIAL)	SO FT	17,397	17,397											
X6061815	COMBINATION CONCRETE CURB AND GUTTER, TYPE M (SPECIAL)	FOOT	95	95											
X6640300	CHAIN LINK FENCE REMOVAL	FOOT	1,056	1,056											
X6700410	ENGINEER'S FIELD OFFICE, TYPE A (SPECIAL)	CAL MO	18	18											
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1											
X7030025	WET REFLECTIVE TEMPORARY TAPE, TYPE III - LETTERS AND SYMBOLS	SO FT	873	873											
X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH	FOOT	120,324	120,324											
X7030040	WET REFLECTIVE TEMPORARY TAPE TYPE III, 6 INCH	FOOT	2,689	2,689											
X7030045	WET REFLECTIVE TEMPORARY TAPE TYPE III, 8 INCH	FOOT	606	606											
X7030050	WET REFLECTIVE TEMPORARY TAPE TYPE III, 12 INCH	FOOT	641	641											
X7030055	WET REFLECTIVE TEMPORARY TAPE TYPE III, 24 INCH	FOOT	595	595											

* SPECIALTY ITEM

C:\Projects\US14150-LucasRd-LongTran\AutoStation Design Files\Sheet\SU000.RD.DGN
 exp U.S. Services Inc.
 11/01/13
 6/18/2014

FILE NAME =	USER NAME =	DESIGNED -	REVISED -
C:\Projects\US14150-LucasRd-LongTran\AutoStation Design Files\Sheet\SU000.RD.DGN	exp U.S. Services Inc.	DRAWN - AMB	REVISED -
	PLOT SCALE = 100.0000' / 1"	CHECKED - BRH	REVISED -
	PLOT DATE = 6/18/2014	DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

SUMMARY OF QUANTITIES - VIII			
SCALE: N.T.S.	SHEET NO. OF	SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	11
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT			CONTRACT NO. 62268	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	TRAFFIC SIGNALS																			
				URBAN		ROADWAY	FIBER OPTIC CONDUIT	US RTE 14 & DOTY ROAD		US RTE 14 & LAKE SHORE DR		EMERGENCY VEHICLE PREEMPTION	INTERCONNECT	US RTE 14 & LAKE SHORE DR		MULTI-USE PATH	WATER MAIN AND SANITARY SEWER						
				100% STATE	100% MCHENRY COUNTY	100% STATE	100% MCHENRY COUNTY	93.3% STATE 6.7% WOODSTOCK	95% STATE 5% WOODSTOCK	100% WOODSTOCK	100% STATE	90% STATE 10% WOODSTOCK	80% STATE 20% MCD	100% WOODSTOCK	100% STATE								
CONSTRUCTION CODE													0003	0021	0021	0021	0021	0021	0021	0021	0021	0043	0043
51400100	HANDHOLE	EACH	3																				
X0327766	HDPE DUCT	FOOT	4800																				
X0327767	RECONSTRUCT WET WELL	EACH	1																			1	
X0327768	SANITARY FORCE MAIN 8"	FOOT	648																			648	
Ø 70076604	TRAINEES - TRAINING PROGRAM GRADUATE	HR	1500	1500																			
* 06900200	NON-SPECIAL WASTE DISPOSAL	CU YD	5900	5900																			
* 06900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1																			
* 06900530	SOIL DISPOSAL ANALYSIS	EACH	10	10																			

7
* SPECIALTY ITEM
Ø 0042

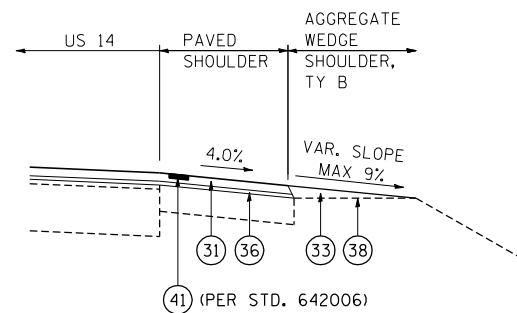
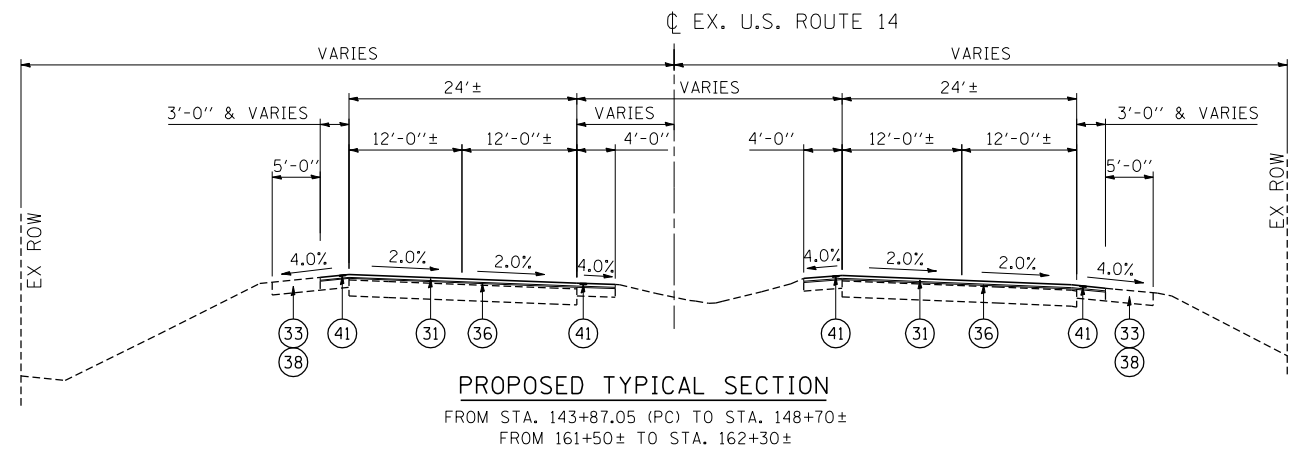
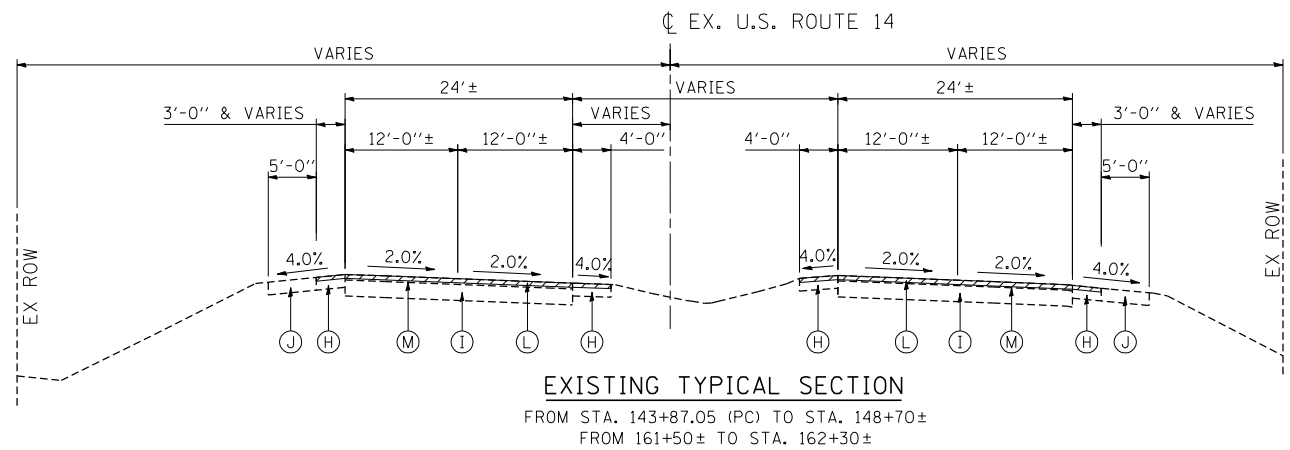
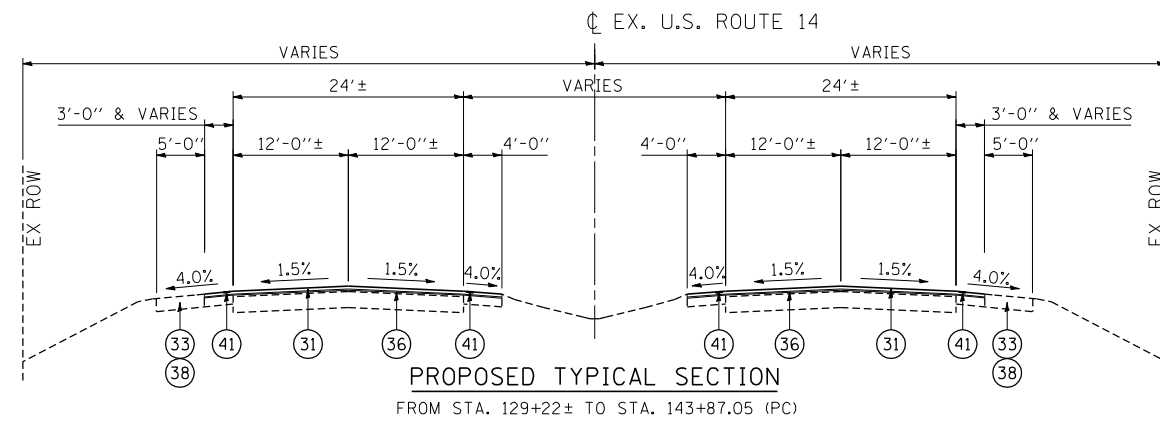
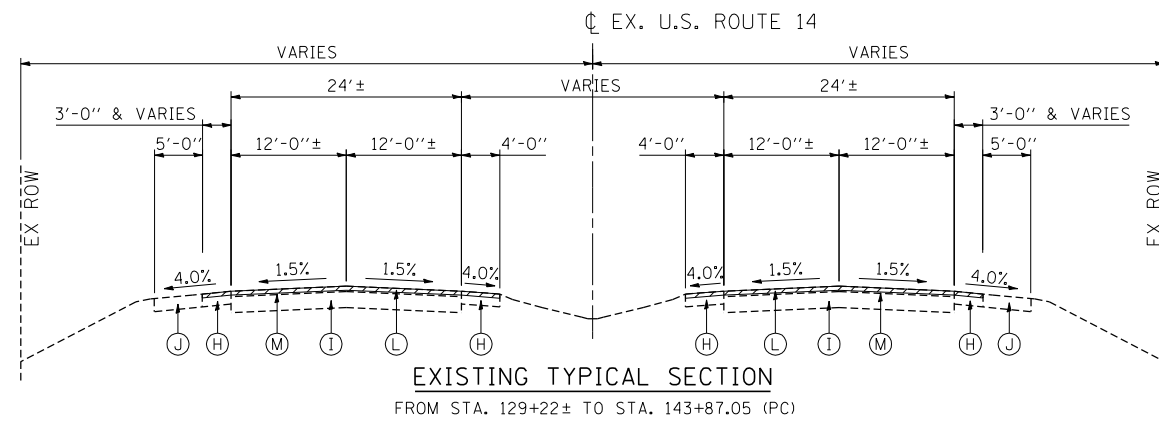
FILE NAME : C:\Projects\US14\SD-LucasRd-LongTran\Station Design Files\Sheet\SU008.16.DGN	USER NAME : *****	DESIGNED - DRAWN - AMB	REVISED - REVISED -
exp U.S. Services Inc. CHICAGO, IL SUBSIDIARY OF EARTH & ENVIRONMENT-ENERGY INDUSTRIAL INFRASTRUCTURE SUSTAINABILITY	PLT SCALE : 100.0000 / in	CHECKED - BRH	REVISED - REVISED -
PLT DATE : 8/18/2014		DATE - 11/01/13	REVISED - REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**
U.S. ROUTE 14

SUMMARY OF QUANTITIES - XI			
SCALE: N.T.S.	SHEET NO. OF SHEETS	STA. TO STA.	

F.A.P. RTE. 305	SECTION 27R-2	COUNTY MCHENRY	TOTAL SHEETS 673	SHEET NO. 14
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

Rev.



DETAIL P

SHOULDER RESURFACING TREATMENT

NOTES:

- EXISTING DIMENSIONS SHOWN ARE FROM RECORD PLANS. ACTUAL DIMENSIONS MAY VARY.
- EXISTING THICKNESSES SHOWN ARE FROM RECORD PLANS. ACTUAL THICKNESSES MAY VARY.
- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SO.YD./IN.

EXISTING LEGEND

- (A) HMA PAVEMENT (VARIES 8" TO 12")
- (B) PCC PAVEMENT (VARIES 6 1/2" TO 8 1/4")
- (C) HMA PAVEMENT (4 1/2" AND VARIES)
- (D) HMA PAVEMENT (5" AND VARIES)
- (E) HMA PAVEMENT (6" AND VARIES)
- (F) CONCRETE MEDIAN SURFACE
- (G) CONCRETE CURB & GUTTER
- (H) HMA SHOULDER (8" & VARIES)
- (I) PCC BASE COURSE (10")
- (J) AGGREGATE SHOULDER
- (K) HMA SURFACE REMOVAL (VARIABLE DEPTH)
- (L) HMA SURFACE REMOVAL (2 1/4" MIN - COLD MILL)
- (M) HMA BINDER AND SURFACE CSE (3 1/2" & VARIES)

PROPOSED LEGEND

- (1) P.C.C. PAVEMENT 9 3/4" JOINTED
- (2) STABILIZED SUBBASE (HMA) 4 1/2"
- (3) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- (4) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- (5) COMB CC&G TB6.12
- (6) COMB CC&G TB6.24
- (7) PCC SHOULDERS, 9 3/4"
- (8) COURSE AGGREGATE FILL
- (9) HMA SHOULDERS, 8"
- (10) CONCRETE MEDIAN, TYPE SB (SPECIAL)
- (11) CONCRETE MEDIAN SURFACE 4"
- (12) TOPSOIL 6"
- (13) AGGREGATE SHOULDER, TYPE B 6"
- (14) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS @ 30" CTS
- (15) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS @ 24" CTS
- (16) NO. 6 TIE BARS @ 24" CTS
- (17) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (18) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (19) HOT MIX-ASHPALT BASE COURSE WIDENING 6 1/4"
- (20) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (21) HOT-MIX ASPHALT BASE COURSE WIDENING 7"
- (22) AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 SHEET 3)
- (23) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8 1/4" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 6 1/4"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DOWEL BAR @ 24" CTS

- (25) HMA DRIVEWAY (SEE DISTRICT DETAIL BD-01)
- (26) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 7"
- (27) SODDING OR SEEDING (SEE LANDSCAPING PLANS)
- (28) TOPSOIL 18"
- (29) AGGREGATE SHOULDER, TYPE B 8"
- (30) CORRUGATED MEDIAN
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
- (32) P.C.C. BASE COURSE WIDENING 10"
- (33) AGGREGATE WEDGE SHOULDER, TYPE B
- (34) COMB CC&G TM4.06
- (35) COMB CC&G TM4.24

- (36) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (3/4" & VARIES)
- (37) TOPSOIL 24"
- (38) GRADING AND SHAPING SHOULDERS
- (39) AGGREGATE SURFACE COURSE, TYPE B SPECIAL 2"
- (40) AGGREGATE BASE COURSE, TYPE B 6"
- (41) SHOULDER RUMBLE STRIPS, 8 INCH

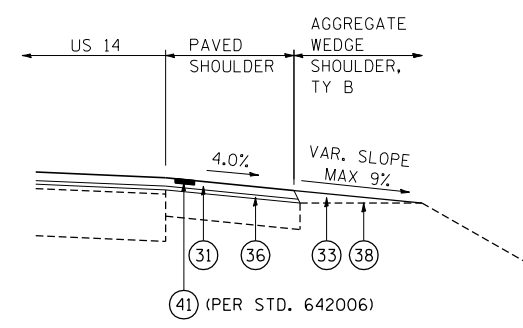
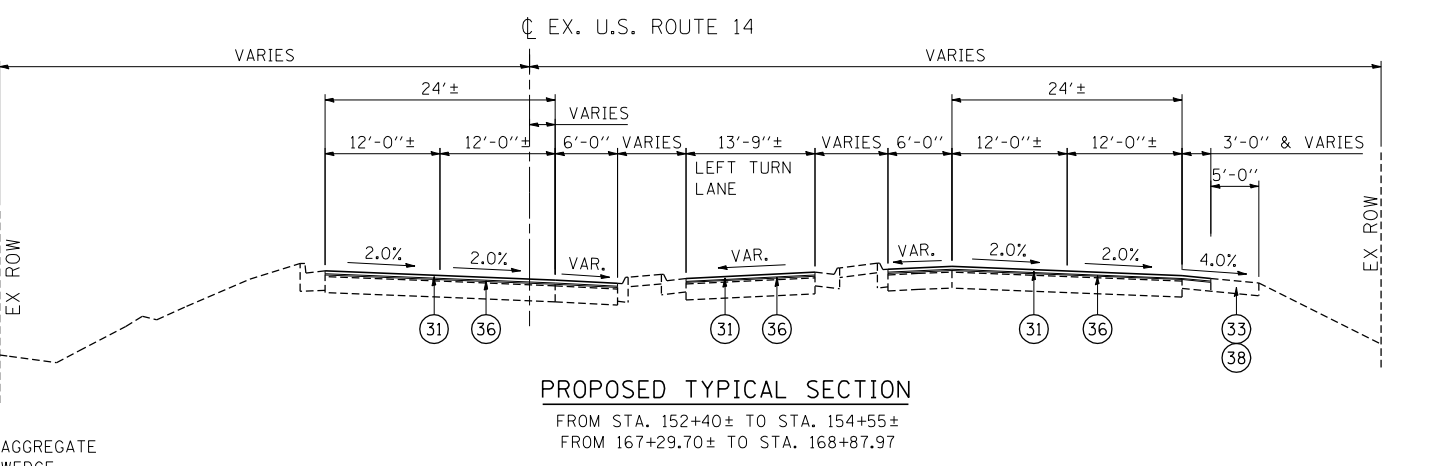
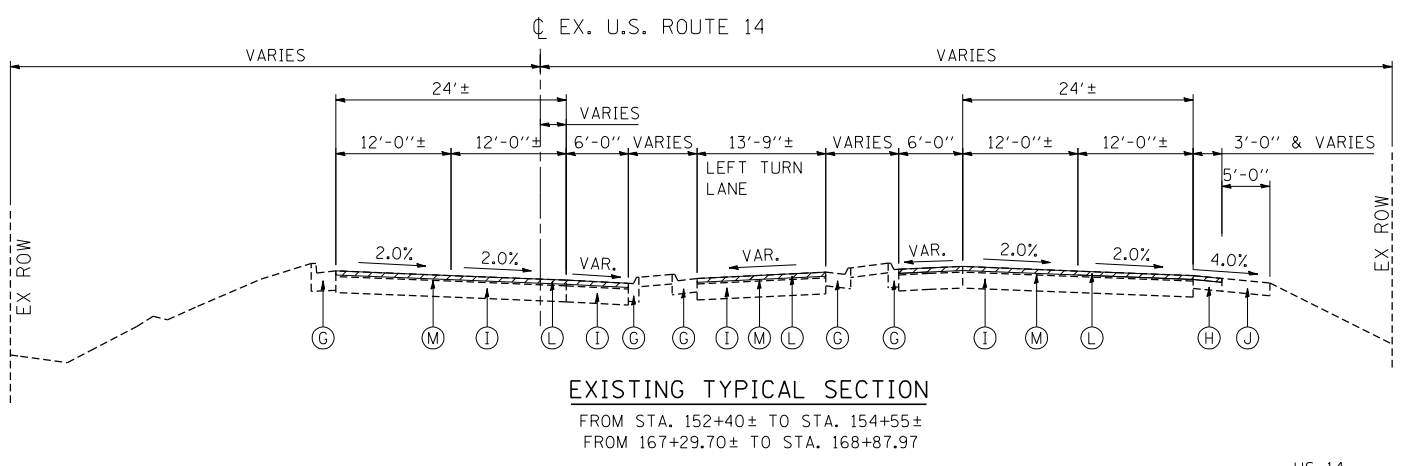
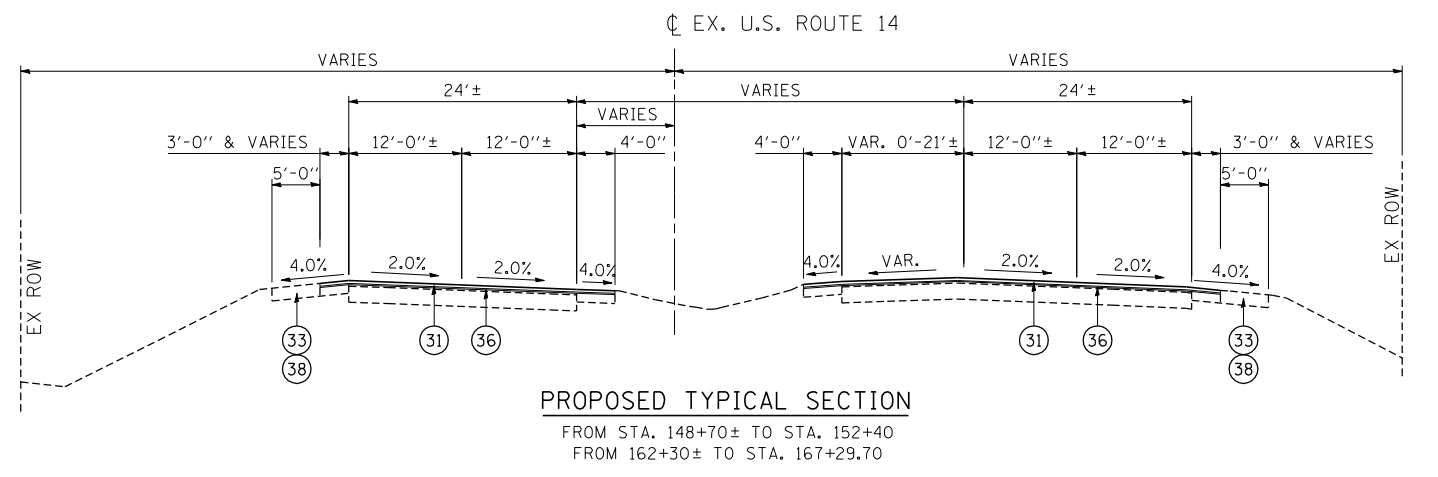
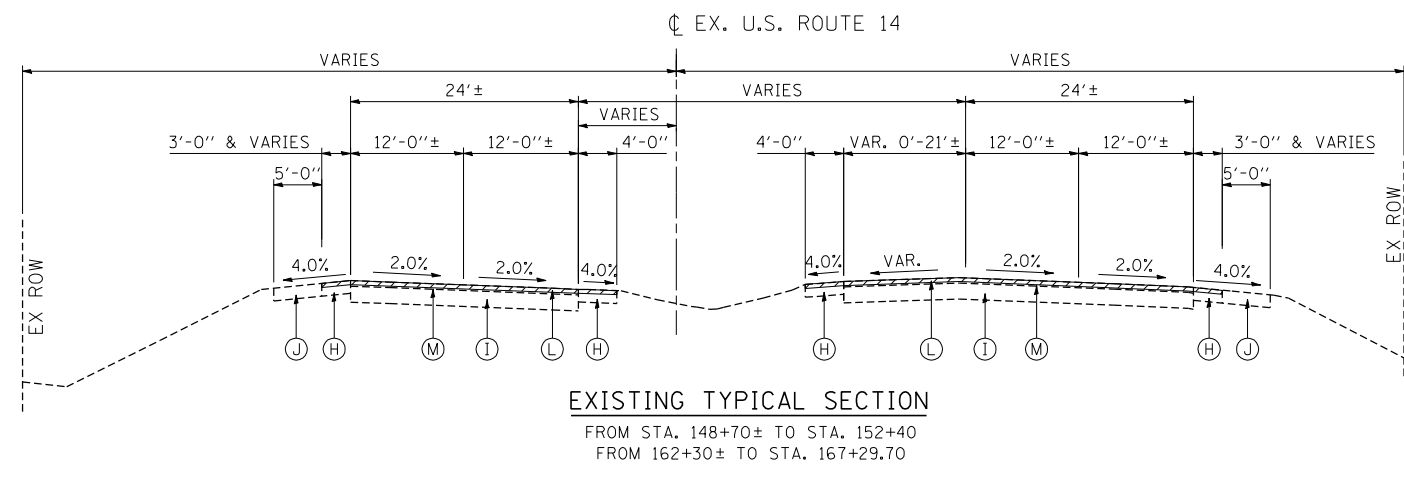
ATT:TB08RJ.DGN, .GEM08RJ.DGN, .NTSC08.DGN, .HECHTBR T:\DOCUMENT\19349901\CIVIL\DOCS\TSC08_22.DGN
 11-07-2013, 16:18:19

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
#FILE#		DRAWN - MRK	REVISED -
exp U.S. Services Inc. Chicago, IL	PLOT SCALE = *SCALE*	CHECKED - BRH	REVISED -
BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT DATE = *DATE*	DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

EXISTING AND PROPOSED TYPICAL SECTIONS			
U.S. ROUTE 14 - I			
SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	15
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62268	



- NOTES:**
- EXISTING DIMENSIONS SHOWN ARE FROM RECORD PLANS ACTUAL DIMENSIONS MAY VARY.
 - EXISTING THICKNESSES SHOWN ARE FROM RECORD PLANS AND PAVEMENT CORES. ACTUAL THICKNESSES MAY VARY.
 - THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ.YD./IN.

- EXISTING LEGEND**
- (A) HMA PAVEMENT (VARIES 8" TO 12")
 - (B) PCC PAVEMENT (VARIES 6 1/2" TO 8 1/4")
 - (C) HMA PAVEMENT (4 1/2" AND VARIES)
 - (D) HMA PAVEMENT (5" AND VARIES)
 - (E) HMA PAVEMENT (6" AND VARIES)
 - (F) CONCRETE MEDIAN SURFACE
 - (G) CONCRETE CURB & GUTTER
 - (H) HMA SHOULDER (8" & VARIES)
 - (I) PCC BASE COURSE (10")
 - (J) AGGREGATE SHOULDER
 - (K) HMA SURFACE REMOVAL (VARIABLE DEPTH)
 - (L) HMA SURFACE REMOVAL (2 1/4" MIN - COLD MILL)
 - (M) HMA BINDER AND SURFACE CSE (3 1/2" & VARIES)

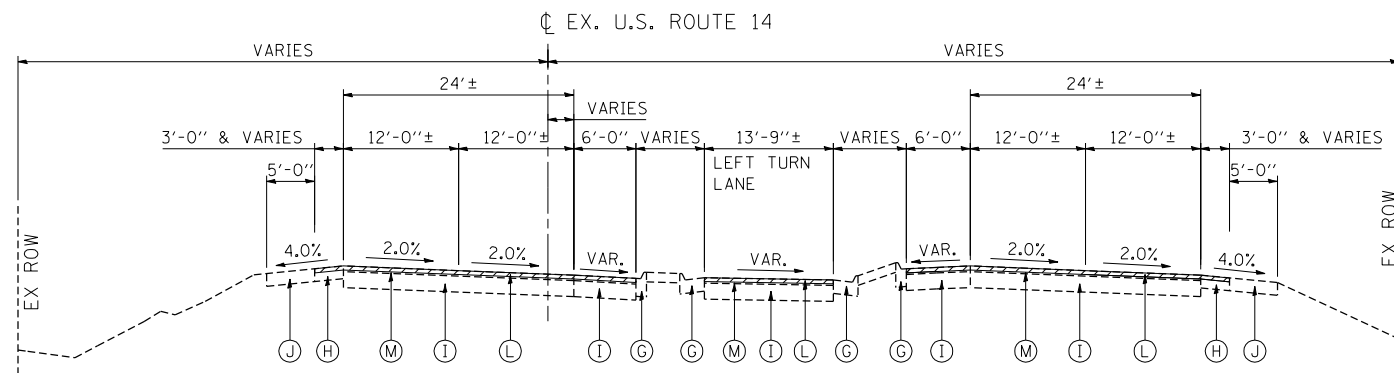
- PROPOSED LEGEND**
- (1) P.C.C. PAVEMENT 9 3/4" JOINTED
 - (2) STABILIZED SUBBASE (HMA) 4 1/2"
 - (3) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
 - (4) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
 - (5) COMB CC&G TB6.12
 - (6) COMB CC&G TB6.24
 - (7) PCC SHOULDERS, 9 3/4"
 - (8) COURSE AGGREGATE FILL
 - (9) HMA SHOULDERS, 8"
 - (10) CONCRETE MEDIAN, TYPE SB (SPECIAL)
 - (11) CONCRETE MEDIAN SURFACE 4"
 - (12) TOPSOIL 6"
 - (13) AGGREGATE SHOULDER, TYPE B 6"

- (14) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS @ 30" CTS
- (15) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS @ 24" CTS
- (16) NO. 6 TIE BARS @ 24" CTS
- (17) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (18) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (19) HOT MIX-ASHPALT BASE COURSE WIDENING 6 1/4"
- (20) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (21) HOT-MIX ASPHALT BASE COURSE WIDENING 7"
- (22) AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 SHEET 3)
- (23) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8 1/4" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 6 1/4"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DOWEL BAR @ 24" CTS

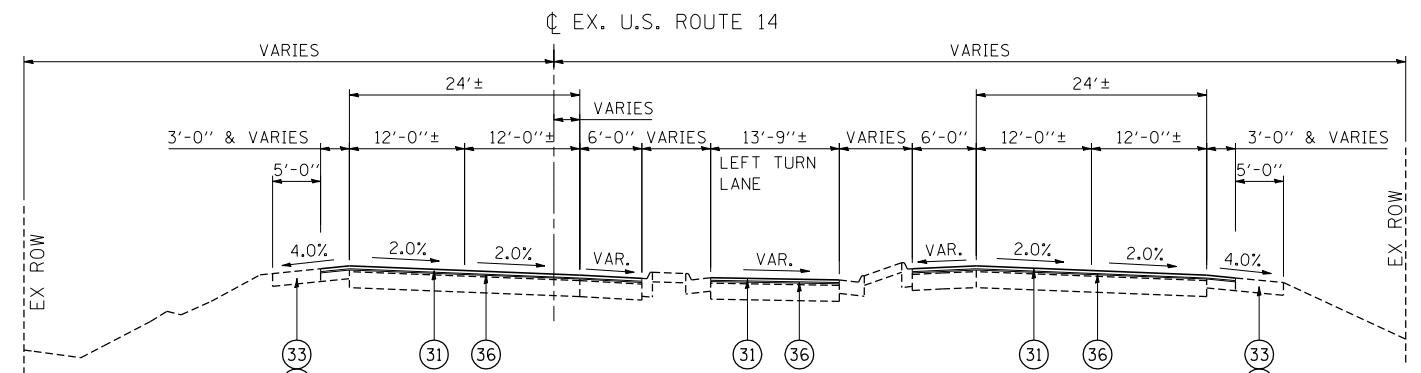
- (25) HMA DRIVEWAY (SEE DISTRICT DETAIL BD-01)
- (26) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 7"
- (27) SODDING OR SEEDING (SEE LANDSCAPING PLANS)
- (28) TOPSOIL 18"
- (29) AGGREGATE SHOULDER, TYPE B 8"
- (30) CORRUGATED MEDIAN
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
- (32) P.C.C. BASE COURSE WIDENING 10"
- (33) AGGREGATE WEDGE SHOULDER, TYPE B
- (34) COMB CC&G TM4.06
- (35) COMB CC&G TM4.24

- (36) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (3/4" & VARIES)
- (37) TOPSOIL 24"
- (38) GRADING AND SHAPING SHOULDERS
- (39) AGGREGATE SURFACE COURSE, TYPE B SPECIAL 2"
- (40) AGGREGATE BASE COURSE, TYPE B 6"
- (41) SHOULDER RUMBLE STRIPS, 8 INCH

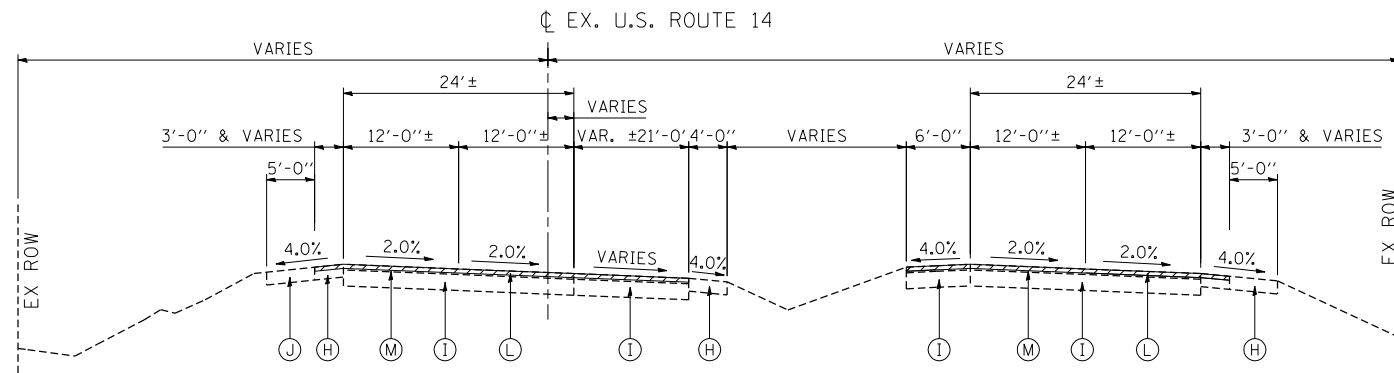
FILE NAME = ... USER NAME = HECHTBR ... DESIGNED - ... REVISED - ... DRAWN - MRK ... REVISED - ... CHECKED - BRH ... REVISED - ... DATE - 11/01/13 ... REVISED - ...



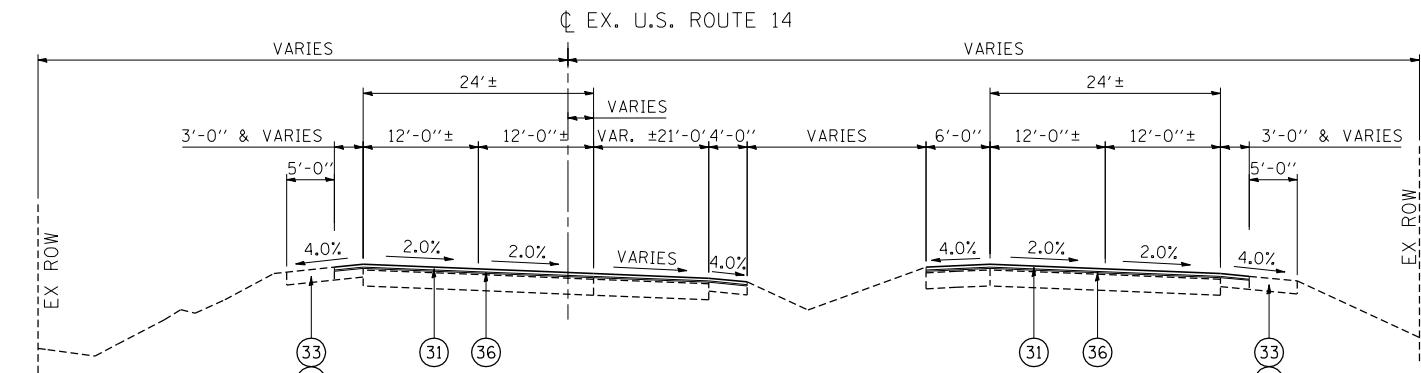
EXISTING TYPICAL SECTION
FROM STA. 154+55± TO STA. 158+97±



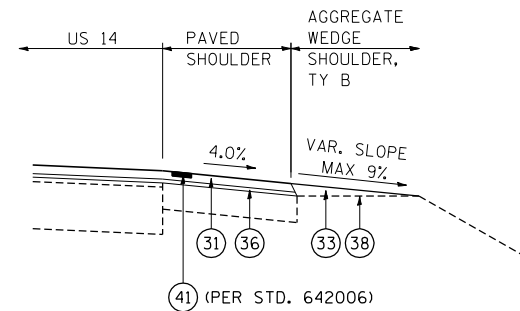
PROPOSED TYPICAL SECTION
FROM STA. 154+55± TO STA. 158+97±



EXISTING TYPICAL SECTION
FROM STA. 158+97± TO STA. 161+50±



PROPOSED TYPICAL SECTION
FROM STA. 158+97± TO STA. 161+50±



DETAIL P

SHOULDER RESURFACING TREATMENT

NOTES:

- EXISTING DIMENSIONS SHOWN ARE FROM RECORD PLANS. ACTUAL DIMENSIONS MAY VARY.
- EXISTING THICKNESSES SHOWN ARE FROM RECORD PLANS AND PAVEMENT CORES. ACTUAL THICKNESSES MAY VARY.
- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ.YD./IN.

EXISTING LEGEND

- (A) HMA PAVEMENT (VARIES 8" TO 12")
- (B) PCC PAVEMENT (VARIES 6 1/2" TO 8 1/4")
- (C) HMA PAVEMENT (4 1/2" AND VARIES)
- (D) HMA PAVEMENT (5" AND VARIES)
- (E) HMA PAVEMENT (6" AND VARIES)
- (F) CONCRETE MEDIAN SURFACE
- (G) CONCRETE CURB & GUTTER
- (H) HMA SHOULDER (8" & VARIES)
- (I) PCC BASE COURSE (10")
- (J) AGGREGATE SHOULDER
- (K) HMA SURFACE REMOVAL (VARIABLE DEPTH)
- (L) HMA SURFACE REMOVAL (2 1/4" MIN - COLD MILL)
- (M) HMA BINDER AND SURFACE CSE (3 1/2" & VARIES)

PROPOSED LEGEND

- (1) P.C.C. PAVEMENT 9 3/4" JOINTED
- (2) STABILIZED SUBBASE (HMA) 4 1/2"
- (3) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- (4) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- (5) COMB CC&G TB6.12
- (6) COMB CC&G TB6.24
- (7) PCC SHOULDERS, 9 3/4"
- (8) COURSE AGGREGATE FILL
- (9) HMA SHOULDERS, 8"
- (10) CONCRETE MEDIAN, TYPE SB (SPECIAL)
- (11) CONCRETE MEDIAN SURFACE 4"
- (12) TOPSOIL 6"
- (13) AGGREGATE SHOULDER, TYPE B 6"
- (14) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS @ 30" CTS
- (15) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS @ 24" CTS
- (16) NO. 6 TIE BARS @ 24" CTS
- (17) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (18) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (19) HOT MIX-ASHPALT BASE COURSE WIDENING 6 1/4"
- (20) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (21) HOT-MIX ASPHALT BASE COURSE WIDENING 7"
- (22) AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 SHEET 3)
- (23) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8 1/4"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19.0, N70, 6 1/4"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DOWEL BAR @ 24" CTS

- (25) HMA DRIVEWAY (SEE DISTRICT DETAIL BD-01)
- (26) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19.0, N70, 7"
- (27) SODDING OR SEEDING (SEE LANDSCAPING PLANS)
- (28) TOPSOIL 18"
- (29) AGGREGATE SHOULDER, TYPE B 8"
- (30) CORRUGATED MEDIAN
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
- (32) P.C.C. BASE COURSE WIDENING 10"
- (33) AGGREGATE WEDGE SHOULDER, TYPE B
- (34) COMB CC&G TM4.06
- (35) COMB CC&G TM4.24
- (36) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (3/4" & VARIES)
- (37) TOPSOIL 24"
- (38) GRADING AND SHAPING SHOULDERS
- (39) AGGREGATE SURFACE COURSE, TYPE B SPECIAL 2"
- (40) AGGREGATE BASE COURSE, TYPE B 6"
- (41) SHOULDER RUMBLE STRIPS, 8 INCH

FILE NAME = USER NAME = HECHTBR
 #FILE#
 exp U.S. Services Inc.
 BUILDING- EARTH & ENVIRONMENT-ENERGY
 INDUSTRIAL- INFRASTRUCTURE-SUSTAINABILITY

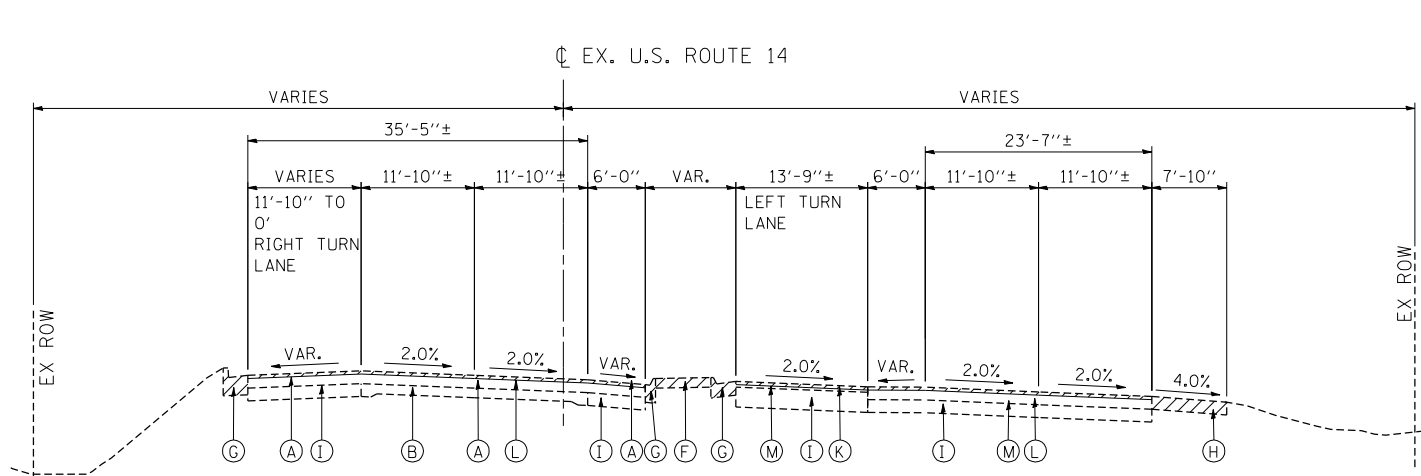
DESIGNED -	REVISED -
DRAWN - MRK	REVISED -
CHECKED - BRH	REVISED -
DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
U.S. ROUTE 14

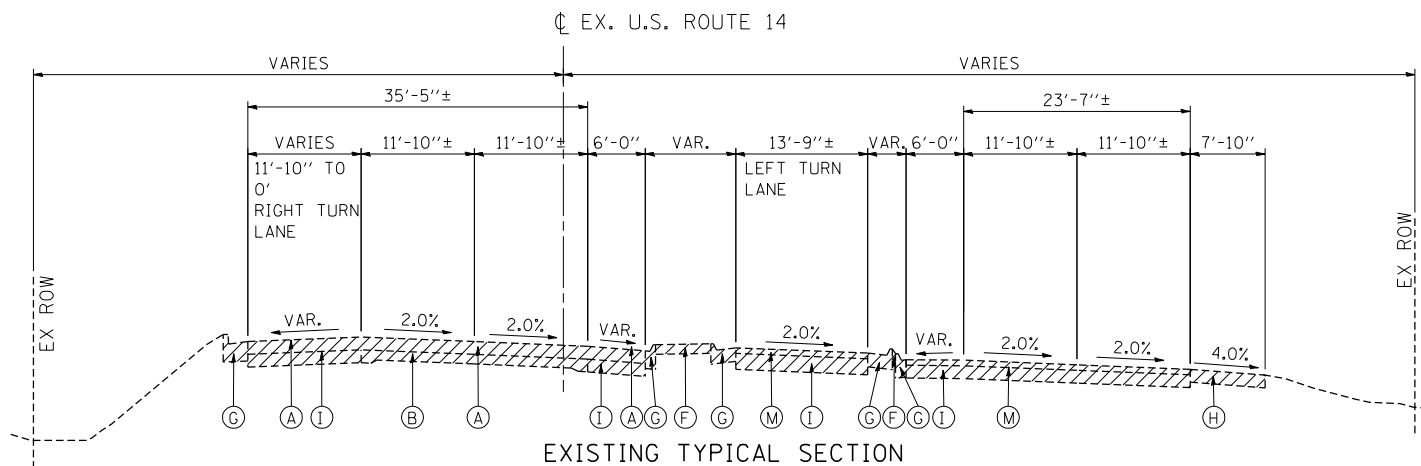
EXISTING AND PROPOSED TYPICAL SECTIONS
U.S. ROUTE 14 - III

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

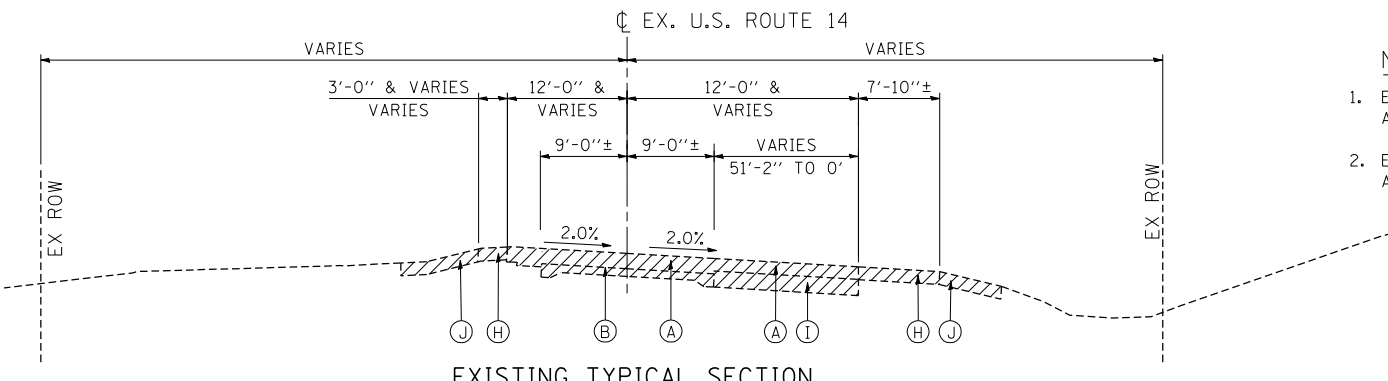
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	17
CONTRACT NO. 62268			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



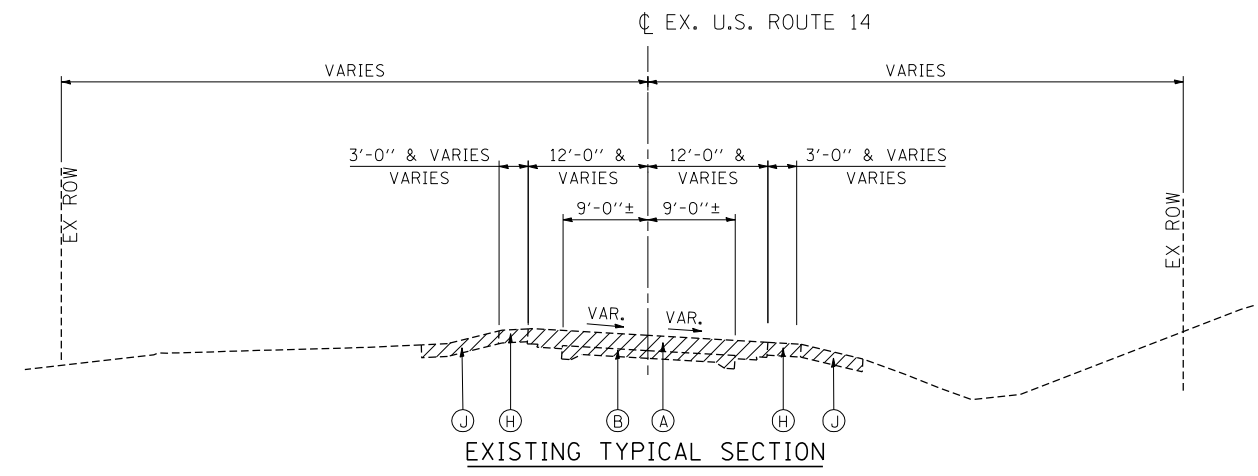
EXISTING TYPICAL SECTION
FROM STA. 168+87.97 TO STA. 169+61.76



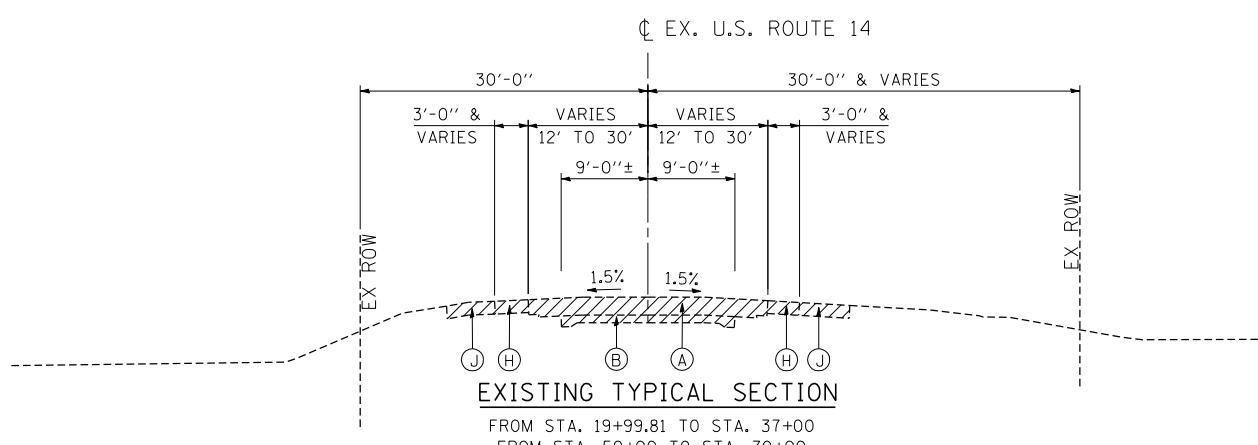
EXISTING TYPICAL SECTION
FROM STA. 169+61.76 TO STA. 175+54



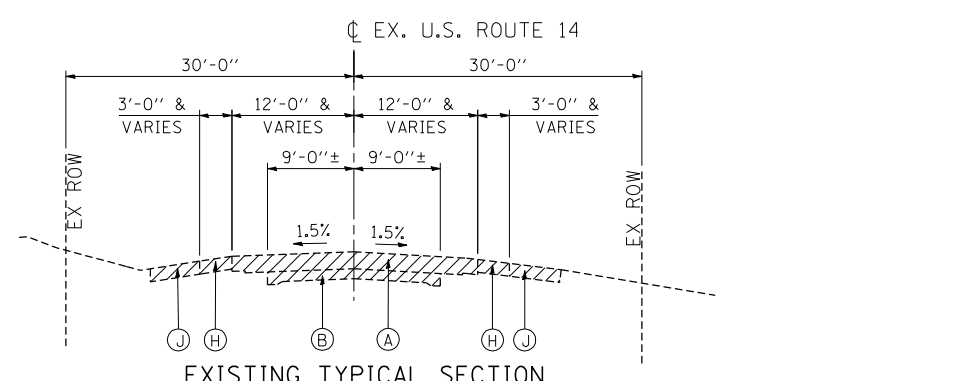
EXISTING TYPICAL SECTION
FROM STA. 175+54 TO STA. 184+00



EXISTING TYPICAL SECTION
FROM STA. 184+00 TO STA. 200+00
EQUATION: STA. 200+00.00 BK = STA. 19+99.81 AH



EXISTING TYPICAL SECTION
FROM STA. 19+99.81 TO STA. 37+00
FROM STA. 50+00 TO STA. 70+00



EXISTING TYPICAL SECTION
FROM STA. 37+00 TO STA. 50+00
FROM STA. 70+00 TO STA. 157+10

- NOTES:
- EXISTING DIMENSIONS SHOWN ARE FROM RECORD PLANS. ACTUAL DIMENSIONS MAY VARY.
 - EXISTING THICKNESSES SHOWN ARE FROM RECORD PLANS AND PAVEMENT CORES. ACTUAL THICKNESSES MAY VARY.

- EXISTING LEGEND**
- (A) HMA PAVEMENT (VARIES 8" TO 12")
 - (B) PCC PAVEMENT (VARIES 6 1/2" TO 8 1/4")
 - (C) HMA PAVEMENT (4 1/2" AND VARIES)
 - (D) HMA PAVEMENT (5" AND VARIES)
 - (E) HMA PAVEMENT (6" AND VARIES)
 - (F) CONCRETE MEDIAN SURFACE
 - (G) CONCRETE CURB & GUTTER
 - (H) HMA SHOULDER (8" & VARIES)
 - (I) PCC BASE COURSE (10")
 - (J) AGGREGATE SHOULDER
 - (K) HMA SURFACE REMOVAL (VARIABLE DEPTH)
 - (L) HMA SURFACE REMOVAL (2 1/4" MIN - COLD MILL)
 - (M) HMA BINDER AND SURFACE CSE (3 1/2" & VARIES)

- PROPOSED LEGEND**
- (1) P.C.C. PAVEMENT 9 3/4" JOINTED
 - (2) STABILIZED SUBBASE (HMA) 4 1/2"
 - (3) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
 - (4) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
 - (5) COMB CC&G TB6.12
 - (6) COMB CC&G TB6.24
 - (7) PCC SHOULDERS, 9 3/4"
 - (8) COURSE AGGREGATE FILL
 - (9) HMA SHOULDERS, 8"
 - (10) CONCRETE MEDIAN, TYPE SB (SPECIAL)
 - (11) CONCRETE MEDIAN SURFACE 4"
 - (12) TOPSOIL 6"
 - (13) AGGREGATE SHOULDER, TYPE B 6"

- (14) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS @ 30" CTS
- (15) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS @ 24" CTS
- (16) NO. 6 TIE BARS @ 24" CTS
- (17) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (18) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (19) HOT MIX-ASHPALT BASE COURSE WIDENING 6 1/4"
- (20) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (21) HOT-MIX ASPHALT BASE COURSE WIDENING 7"
- (22) AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 SHEET 3)
- (23) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8 1/4"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19.0, N70, 6 1/4"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DOWEL BAR @ 24" CTS

- (25) HMA DRIVEWAY (SEE DISTRICT DETAIL BD-01)
- (26) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19.0, N70, 7"
- (27) SODDING OR SEEDING (SEE LANDSCAPING PLANS)
- (28) TOPSOIL 18"
- (29) AGGREGATE SHOULDER, TYPE B 8"
- (30) CORRUGATED MEDIAN
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
- (32) P.C.C. BASE COURSE WIDENING 10"
- (33) AGGREGATE WEDGE SHOULDER, TYPE B
- (34) COMB CC&G TM4.06
- (35) COMB CC&G TM4.24

- (36) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (3/4" & VARIES)
- (37) TOPSOIL 24"
- (38) GRADING AND SHAPING SHOULDERS
- (39) AGGREGATE SURFACE COURSE, TYPE B SPECIAL 2"
- (40) AGGREGATE BASE COURSE, TYPE B 6"
- (41) SHOULDER RUMBLE STRIPS, 8 INCH

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**
U.S. ROUTE 14

**EXISTING TYPICAL SECTIONS
U.S. ROUTE 14 - IV**

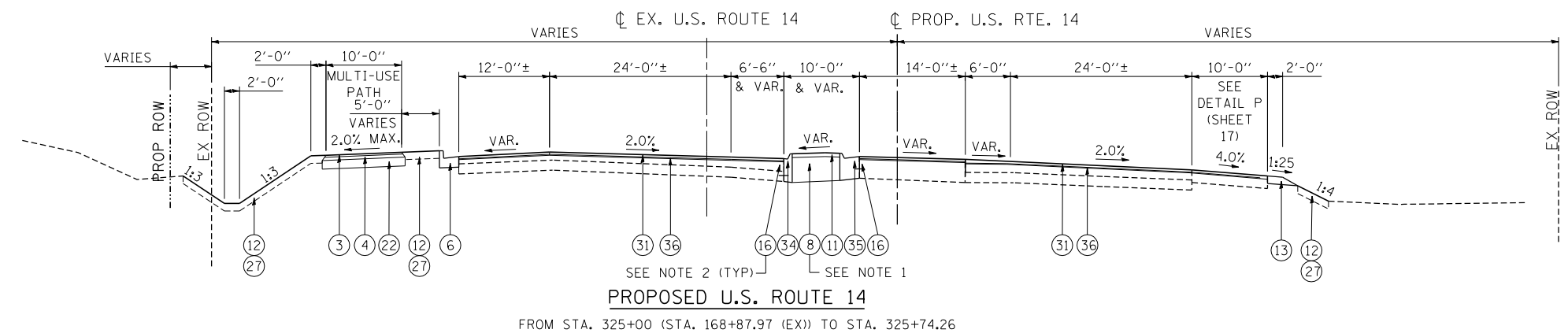
FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
#FILEL#		DRAWN - MRK	REVISED -
	PLOT SCALE = #SCALE#	CHECKED - BRH	REVISED -
	PLOT DATE = #DATE#	DATE - 11/01/13	REVISED -

SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA.	TO STA.
---------------	-----------	-----------	------	---------

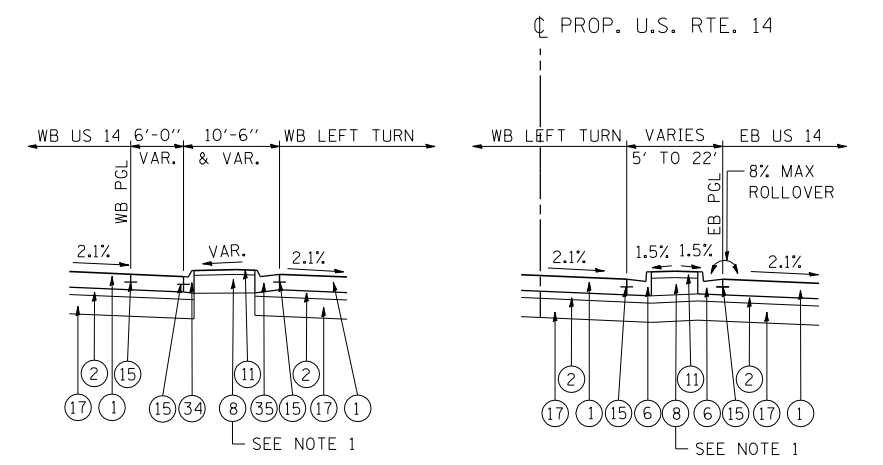
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	18
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

ATT:TB08RJJ.DGN, G:\E\080817.DGN, \NTSC000_0C.DGN
 11-07-2013, 16:18:26
 HECHTBR T:\DOCUMENT\19349901\CIVIL\DGN\TSC00008.KH.DGN

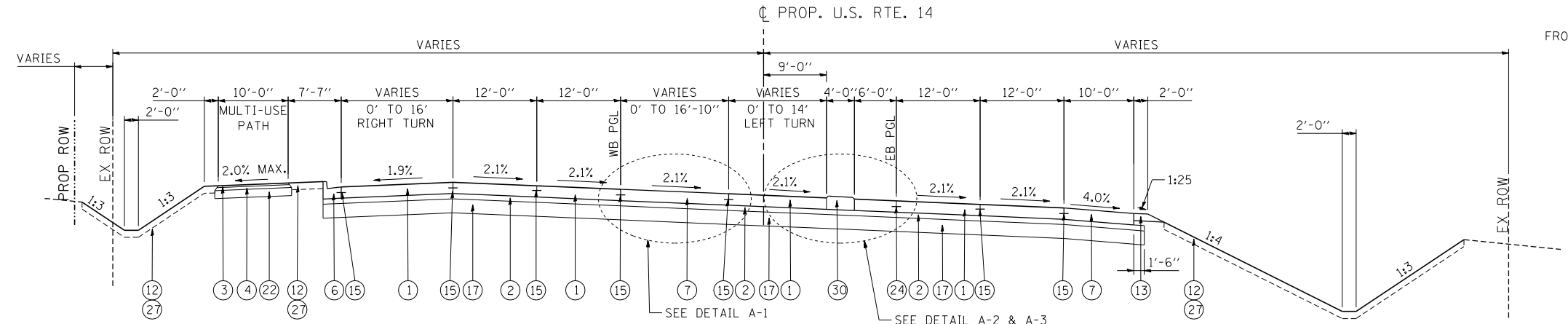




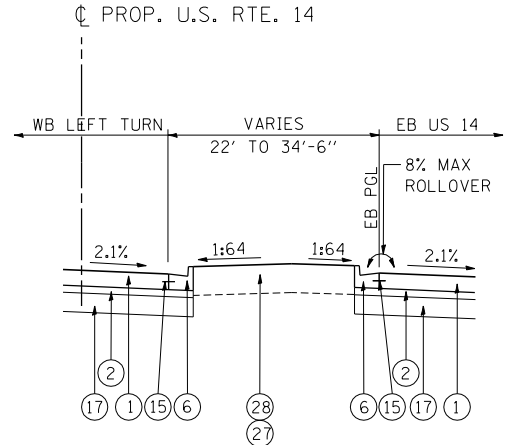
PROPOSED U.S. ROUTE 14
FROM STA. 325+00 (STA. 168+87.97 (EX)) TO STA. 325+74.26



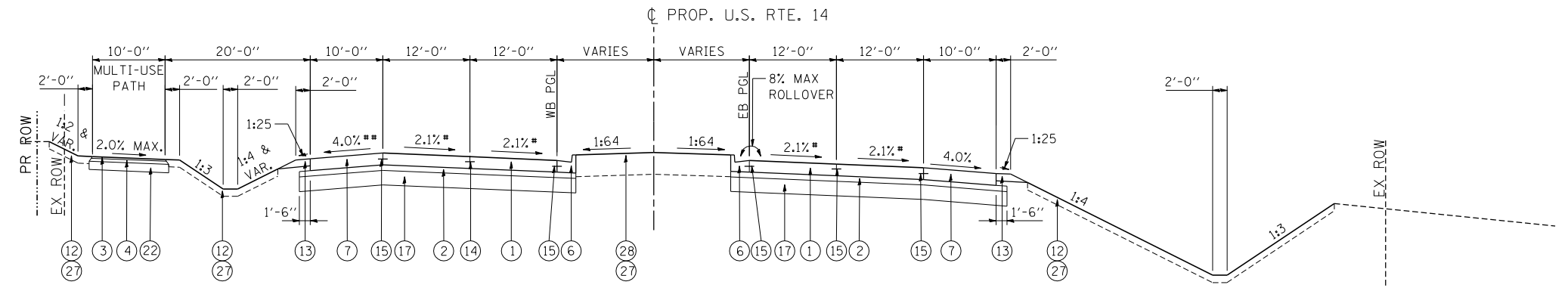
DETAIL A-1 FROM STA. 325+74.26 TO STA. 326+64.78
DETAIL A-2 FROM STA. 328+07.98 TO STA. 329+67.19



PROPOSED U.S. ROUTE 14
FROM STA. 325+74.26 TO STA. 330+99.09



DETAIL A-3 FROM STA. 329+67.19 TO STA. 330+99.09



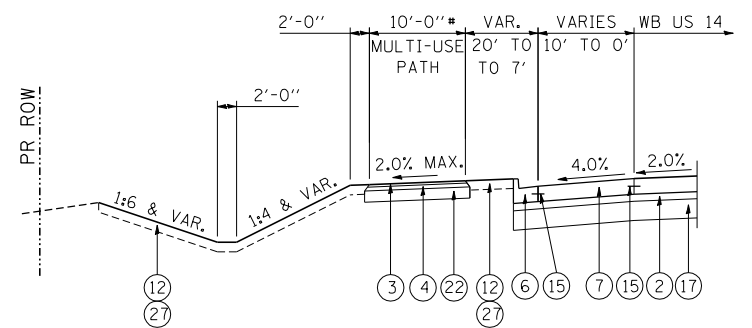
PROPOSED U.S. ROUTE 14
FROM STA. 330+99.09 TO STA. 345+66.27 (PT)
* PAVEMENT CROSS SLOPE VARIES THRU SUPERELEVATION TRANSITION (SEE SHEET 76)
** SHOULDER CROSS SLOPE VARIES -1.9% TO -4.0% FROM STA. 331+00 TO STA. 331+42

- NOTES**
- COARSE AGGREGATE FILL SHALL BE CONSTRUCTED IN ACCORDANCE WITH ART. 606.09 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR CONCRETE MEDIAN SURFACE 4".
 - TIE BARS BETWEEN NEW PORTLAND CEMENT CONCRETE APPURTENANCES AND EXISTING PORTLAND CEMENT CONCRETE PAVEMENT/ BASE COURSE, INCLUDING ALL LABOR AND MATERIALS REQUIRED FOR INSTALLATION AND TESTING, WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICE FOR THE PORTLAND CEMENT CONCRETE ITEM INVOLVED.
- TIE BARS SHALL BE INSTALLED IN DRILLED HOLES IN EXISTING PORTLAND CEMENT CONCRETE PAVEMENT/ BASE COURSE IN ACCORDANCE WITH ARTICLE 420.05(b) OF THE STANDARD SPECIFICATIONS.

- EXISTING LEGEND**
- (A) HMA PAVEMENT (VARIES 8" TO 12")
 - (B) PCC PAVEMENT (VARIES 6 1/2" TO 8 1/4")
 - (C) HMA PAVEMENT (4 1/2" AND VARIES)
 - (D) HMA PAVEMENT (5" AND VARIES)
 - (E) HMA PAVEMENT (6" AND VARIES)
 - (F) CONCRETE MEDIAN SURFACE
 - (G) CONCRETE CURB & GUTTER
 - (H) HMA SHOULDER (8" & VARIES)
 - (I) PCC BASE COURSE (10")
 - (J) AGGREGATE SHOULDER
 - (K) HMA SURFACE REMOVAL (VARIABLE DEPTH)
 - (L) HMA SURFACE REMOVAL (2 1/4" MIN - COLD MILL)
 - (M) HMA BINDER AND SURFACE CSE (3 1/2" & VARIES)
- PROPOSED LEGEND**
- (1) P.C.C. PAVEMENT 9 3/4" JOINTED
 - (2) STABILIZED SUBBASE (HMA) 4 1/2"
 - (3) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
 - (4) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
 - (5) COMB CC&G TB6.12
 - (6) COMB CC&G TB6.24
 - (7) PCC SHOULDERS, 9 3/4"
 - (8) COURSE AGGREGATE FILL
 - (9) HMA SHOULDERS, 8"
 - (10) CONCRETE MEDIAN, TYPE SB (SPECIAL)
 - (11) CONCRETE MEDIAN SURFACE 4"
 - (12) TOPSOIL 6"
 - (13) AGGREGATE SHOULDER, TYPE B 6"
 - (14) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS @ 30" CTS
 - (15) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS @ 24" CTS
 - (16) NO. 6 TIE BARS @ 24" CTS
 - (17) AGGREGATE SUBGRADE IMPROVEMENT, 12"
 - (18) STRIP REFLECTIVE CRACK CONTROL TREATMENT
 - (19) HOT MIX-ASHPALT BASE COURSE WIDENING 6 1/4"
 - (20) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
 - (21) HOT-MIX ASPHALT BASE COURSE WIDENING 7"
 - (22) AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 SHEET 3)
 - (23) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8 1/4" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 6 1/4"
 - (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DOWEL BAR @ 24" CTS
 - (25) HMA DRIVEWAY (SEE DISTRICT DETAIL BD-01)
 - (26) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 7"
 - (27) SODDING OR SEEDING (SEE LANDSCAPING PLANS)
 - (28) TOPSOIL 18"
 - (29) AGGREGATE SHOULDER, TYPE B 8"
 - (30) CORRUGATED MEDIAN
 - (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
 - (32) P.C.C. BASE COURSE WIDENING 10"
 - (33) AGGREGATE WEDGE SHOULDER, TYPE B
 - (34) COMB CC&G TM4.06
 - (35) COMB CC&G TM4.24
 - (36) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (3/4" & VARIES)
 - (37) TOPSOIL 24"
 - (38) GRADING AND SHAPING SHOULDERS
 - (39) AGGREGATE SURFACE COURSE, TYPE B SPECIAL 2"
 - (40) AGGREGATE BASE COURSE, TYPE B 6"
 - (41) SHOULDER RUMBLE STRIPS, 8 INCH

FILE NAME = \\NTSC00\J1\DON... \GEM008\J7\DON... \NTSC00_06.DGN
 11-07-2013, 16:18:29
 HECHTBR
 T:\DOCUMENT\1934\9901\CIVIL\DON\NTSC00_01.DGN
 exp U.S. Services Inc. BUILDING-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY

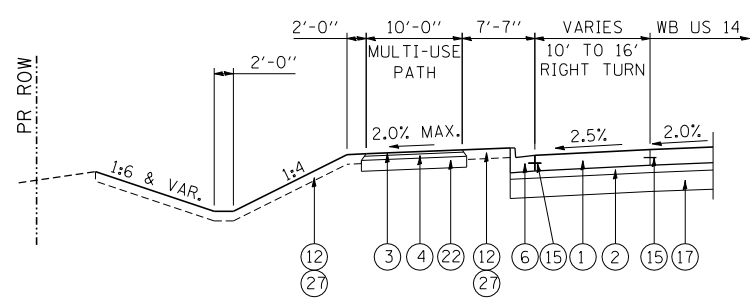
FILE NAME = \\NTSC00\J1\DON... \GEM008\J7\DON... \NTSC00_06.DGN	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	PROPOSED TYPICAL SECTIONS U.S. ROUTE 14 STA. 326+00.00 TO STA. 345+66.27				F.A.P. RTE. 305	SECTION 27R-2	COUNTY MCHENRY	TOTAL SHEETS 673	SHEET NO. 19
#FILEL#	PLOT SCALE = #SCALE#	DRAWN - MRK	REVISED -		SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 62268			
PLOT DATE = #DATE#	DATE - 11/01/13	CHECKED - BRH	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								



DETAIL B-1A

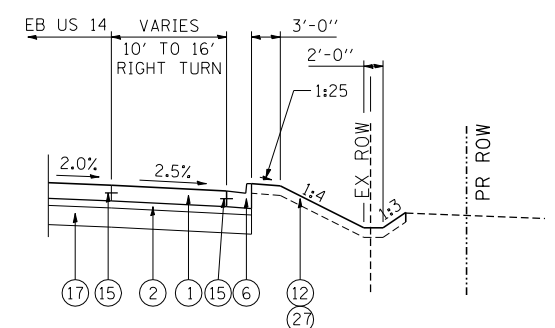
FROM STA. 361+39.85 TO STA. 364+83.68

MULTI-USE PATH IS 8' WIDE BETWEEN STA. 364+80.68 AND STA. 365+17.14



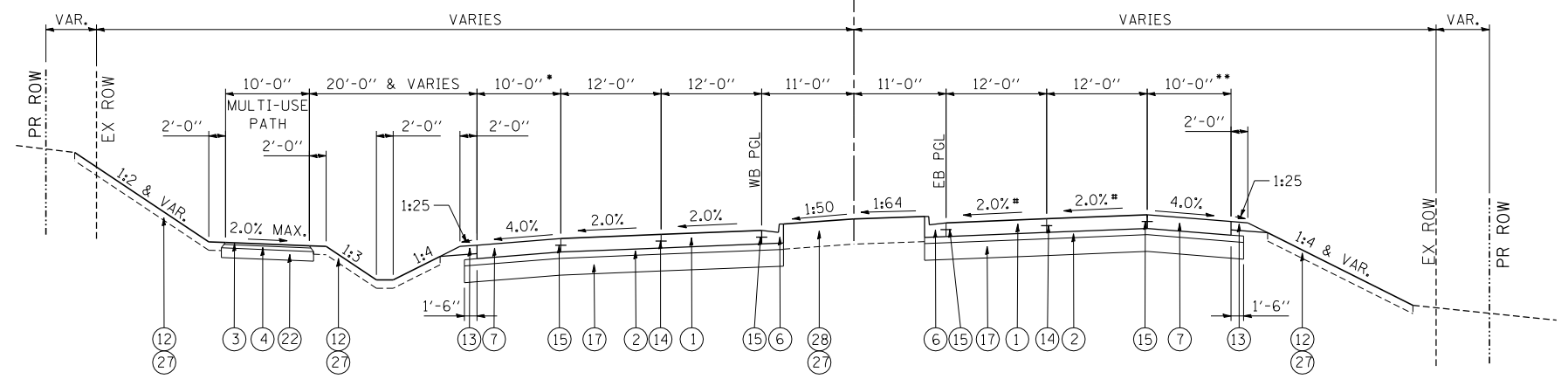
DETAIL B-1B

FROM STA. 366+16.29 TO STA. 370+46.85



DETAIL B-2

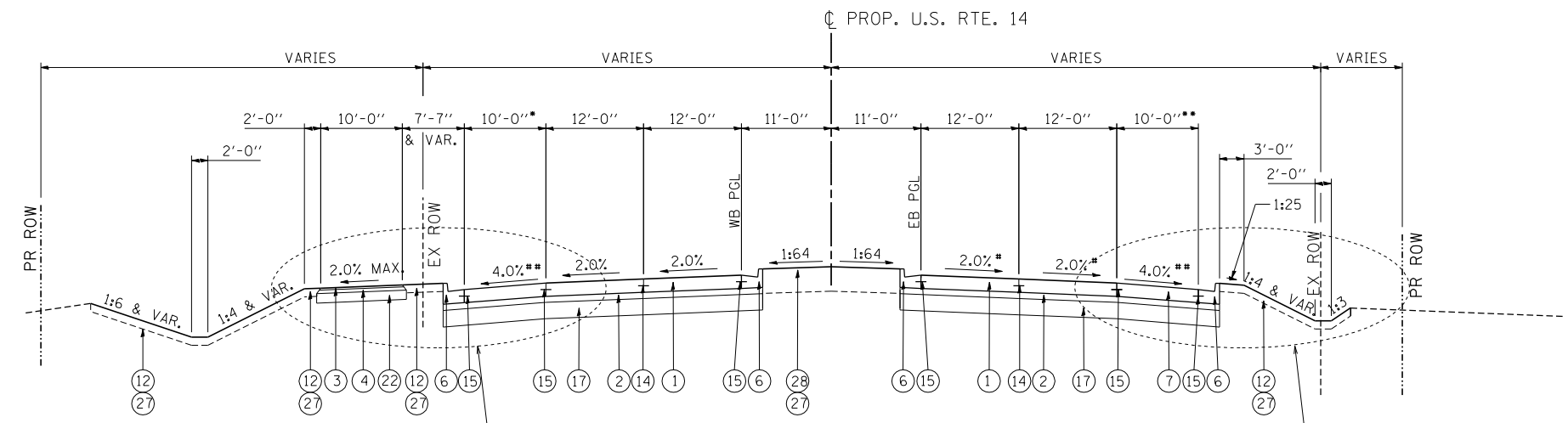
FROM STA. 360+59.49 TO STA. 365+02.14



PROPOSED U.S. ROUTE 14

FROM STA. 345+66.27 (PT) TO STA. 357+72.10 (PT)

- # PAVEMENT CROSS SLOPE VARIES THRU SUPERELEVATION TRANSITION (SEE SHEET 77)
- * SHOULDER 12' & VARIES FROM STA. 349+15.72 TO STA. 351+47.08
- ** SHOULDER 12' & VARIES FROM STA. 350+86.36 TO STA. 353+05.64



PROPOSED U.S. ROUTE 14

FROM STA. 357+72.10 (PT) TO STA. 380+71.55 (PC)

- # PAVEMENT CROSS SLOPE VARIES THRU SUPERELEVATION TRANSITION (SEE SHEET 78)
- ** SHOULDER CROSS SLOPES VARY:
 - 4.0% TO -2.5% FROM STA. 360+30 TO STA. 360+60 (RT)
 - 2.5% TO -4.0% FROM STA. 370+45 TO STA. 370+75 (LT)
 - 4.0% TO -2.0% FROM STA. 372+90 TO STA. 373+30 (RT)
 - 2.0% TO -4.0% FROM STA. 374+30 TO STA. 374+70 (RT)
- * SHOULDER 12' & VARIES FROM STA. 363+35 TO STA. 365+16
- ** SHOULDER 12' & VARIES FROM STA. 365+93 TO STA. 367+70

NOTES

1. SEE SHEET 24 FOR LEFT TURN LANE DETAILS.

EXISTING LEGEND

- (A) HMA PAVEMENT (VARIES 8" TO 12")
- (B) PCC PAVEMENT (VARIES 6 1/2" TO 8 1/4")
- (C) HMA PAVEMENT (4 1/2" AND VARIES)
- (D) HMA PAVEMENT (5" AND VARIES)
- (E) HMA PAVEMENT (6" AND VARIES)
- (F) CONCRETE MEDIAN SURFACE
- (G) CONCRETE CURB & GUTTER
- (H) HMA SHOULDER (8" & VARIES)
- (I) PCC BASE COURSE (10")
- (J) AGGREGATE SHOULDER
- (K) HMA SURFACE REMOVAL (VARIABLE DEPTH)
- (L) HMA SURFACE REMOVAL (2 1/4" MIN - COLD MILL)
- (M) HMA BINDER AND SURFACE CSE (3 1/2" & VARIES)

PROPOSED LEGEND

- (1) P.C.C. PAVEMENT 9 3/4" JOINTED
- (2) STABILIZED SUBBASE (HMA) 4 1/2"
- (3) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- (4) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- (5) COMB CC&G TB6.12
- (6) COMB CC&G TB6.24
- (7) PCC SHOULDERS, 9 3/4"
- (8) COURSE AGGREGATE FILL
- (9) HMA SHOULDERS, 8"
- (10) CONCRETE MEDIAN, TYPE SB (SPECIAL)
- (11) CONCRETE MEDIAN SURFACE 4"
- (12) TOPSOIL 6"
- (13) AGGREGATE SHOULDER, TYPE B 6"

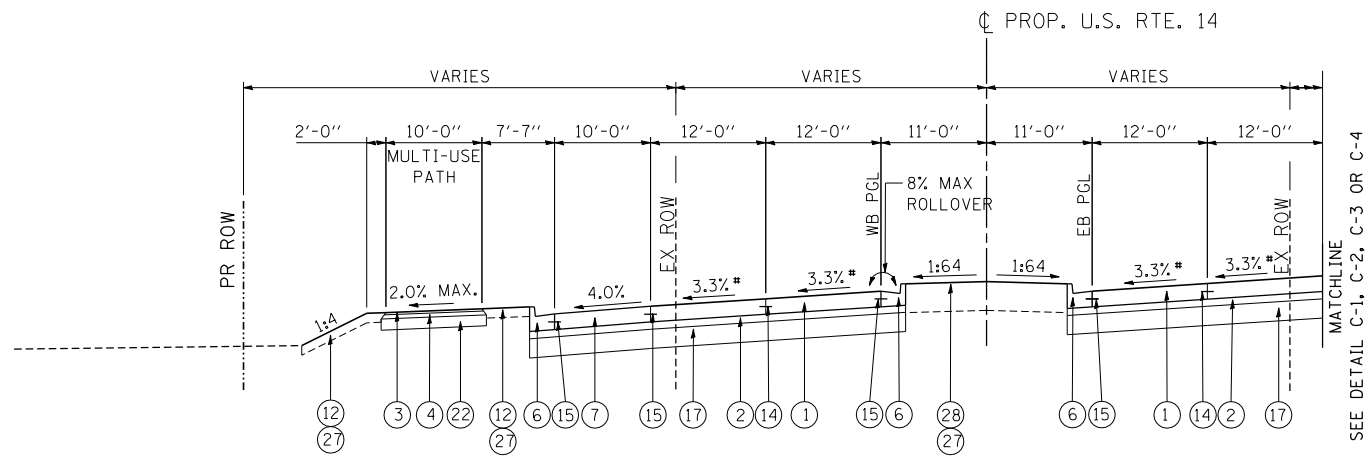
- (14) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS @ 30" CTS
- (15) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS @ 24" CTS
- (16) NO. 6 TIE BARS @ 24" CTS
- (17) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (18) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (19) HOT MIX-ASHPALT BASE COURSE WIDENING 6 1/4"
- (20) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (21) HOT-MIX ASPHALT BASE COURSE WIDENING 7"
- (22) AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 SHEET 3)
- (23) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8 1/4" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 6 1/4"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DOWEL BAR @ 24" CTS

- (25) HMA DRIVEWAY (SEE DISTRICT DETAIL BD-01)
- (26) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 7"
- (27) SODDING OR SEEDING (SEE LANDSCAPING PLANS)
- (28) TOPSOIL 18"
- (29) AGGREGATE SHOULDER, TYPE B 8"
- (30) CORRUGATED MEDIAN
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
- (32) P.C.C. BASE COURSE WIDENING 10"
- (33) AGGREGATE WEDGE SHOULDER, TYPE B
- (34) COMB CC&G TM4.06
- (35) COMB CC&G TM4.24

- (36) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (3/4" & VARIES)
- (37) TOPSOIL 24"
- (38) GRADING AND SHAPING SHOULDERS
- (39) AGGREGATE SURFACE COURSE, TYPE B SPECIAL 2"
- (40) AGGREGATE BASE COURSE, TYPE B 6"
- (41) SHOULDER RUMBLE STRIPS, 8 INCH

ATT:BD08J1.DGN, G:\E:\080817.DGN, \TSC000_06.DGN
 11-07-2013, 16:18:31
 HECHTBR
 T:\DOCUMENT\1934\9901\CIVIL\DOCS\TSC000_06.DGN

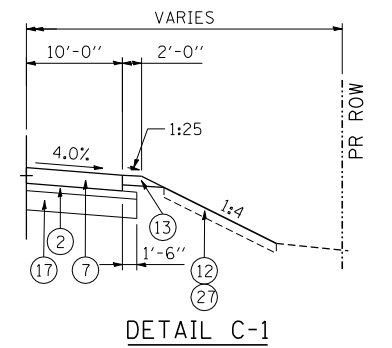
FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14				PROPOSED TYPICAL SECTIONS U.S. ROUTE 14 STA. 345 + 66.27 TO STA. 380 + 71.55				F.A.P. RTE. = 305	SECTION = 27R-2	COUNTY = MCHENRY	TOTAL SHEETS = 673	SHEET NO. = 20
#FILEL#	PLOT SCALE = #SCALE#	DRAWN - MRK	REVISED -									SCALE: N.T.S.	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT
exp U.S. Services Inc. Chicago, IL	PLOT DATE = #DATE#	CHECKED - BRH	REVISED -									CONTRACT NO. 62268				
BUILDING-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	DATE = 11/01/13	DATE = 11/01/13	REVISED -													



PROPOSED U.S. ROUTE 14

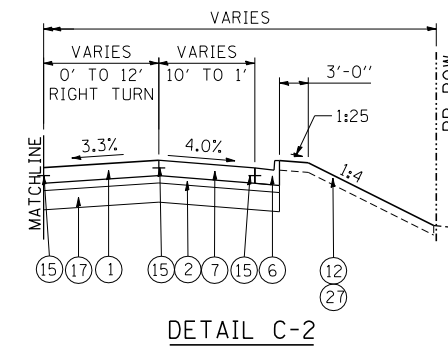
FROM STA. 380+71.55 (PC) TO STA. 393+14.99 (PT)

* PAVEMENT CROSS SLOPE VARIES THRU SUPERELEVATION TRANSITION (SEE SHEET 78)



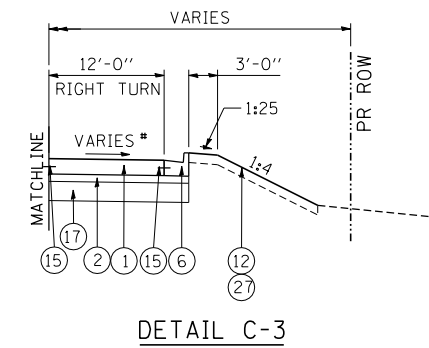
DETAIL C-1

FROM STA. 380+71.55 TO STA. 383+69.02



DETAIL C-2

FROM STA. 383+69.02 TO STA. 385+85

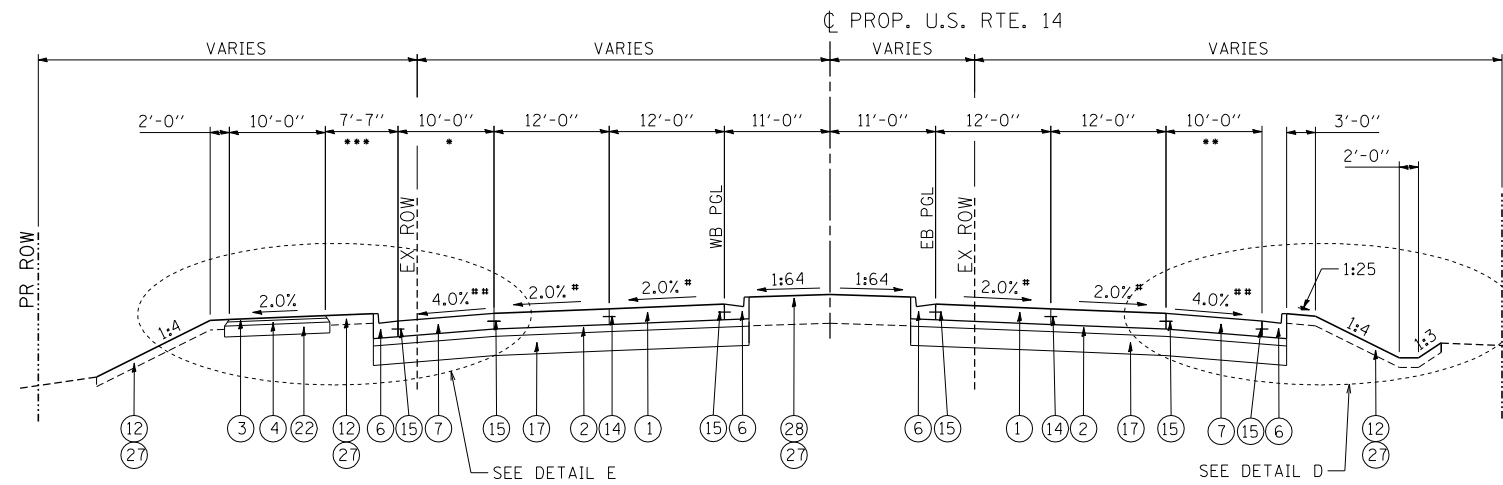


DETAIL C-3

FROM STA. 385+85 TO STA. 388+42
* CROSS SLOPE TRANSITIONS FROM 3.3% TO -1.5%
FROM STA. 385+67 TO STA. 387+01

NOTES

1. SEE SHEET 24 FOR LEFT TURN LANE DETAILS.

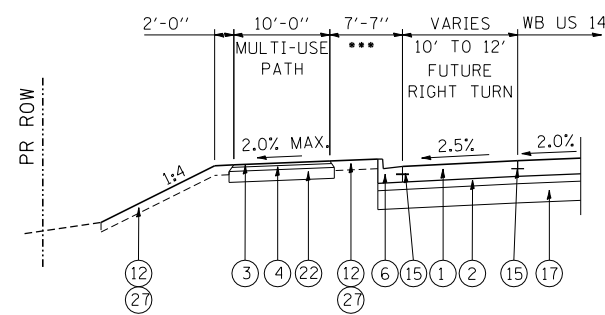


PROPOSED U.S. ROUTE 14

FROM STA. 393+14.99 (PT) TO STA. 402+30 (LT)

FROM STA. 393+14.99 (PT) TO STA. 402+75 (RT)

- * PAVEMENT CROSS SLOPE VARIES THRU SUPERELEVATION TRANSITION (SEE SHEET 78)
- ** SHOULDER CROSS SLOPES VARY:
 - 4.0% TO -2.5% FROM STA. 392+45 TO STA. 392+75 (RT)
 - 4.0% TO -2.5% FROM STA. 395+19 TO STA. 395+79 (LT)
 - 2.5% TO -4.0% FROM STA. 400+00 TO STA. 400+37.50 (LT)
- * SHOULDER 12' & VARIES FROM STA. 395+13.81 TO STA. 396+73.81
- ** SHOULDER 12' & VARIES FROM STA. 398+08.89 TO STA. 399+55.33
- *** VARIES 7'-7" TO 20'-0" FROM STA. 400+54 TO STA. 402+00



DETAIL E

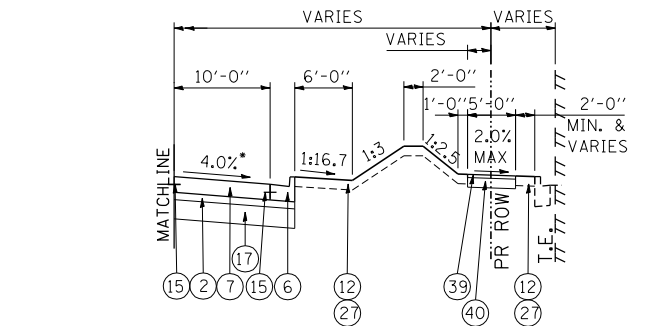
FROM STA. 396+73.81 TO STA. 402+43.58

EXISTING LEGEND

- (A) HMA PAVEMENT (VARIES 8" TO 12")
- (B) PCC PAVEMENT (VARIES 6 1/2" TO 8 1/4")
- (C) HMA PAVEMENT (4 1/2" AND VARIES)
- (D) HMA PAVEMENT (5" AND VARIES)
- (E) HMA PAVEMENT (6" AND VARIES)
- (F) CONCRETE MEDIAN SURFACE
- (G) CONCRETE CURB & GUTTER
- (H) HMA SHOULDER (8" & VARIES)
- (I) PCC BASE COURSE (10")
- (J) AGGREGATE SHOULDER
- (K) HMA SURFACE REMOVAL (VARIABLE DEPTH)
- (L) HMA SURFACE REMOVAL (2 1/4" MIN - COLD MILL)
- (M) HMA BINDER AND SURFACE CSE (3 1/2" & VARIES)

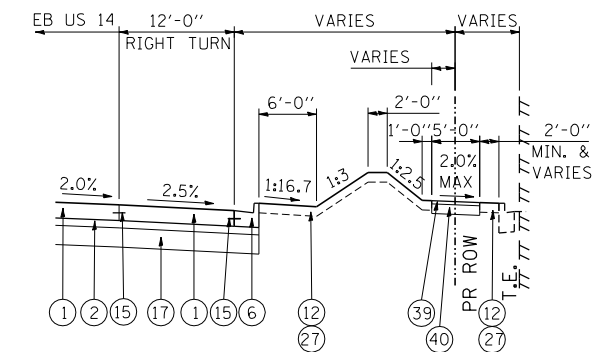
PROPOSED LEGEND

- (1) P.C.C. PAVEMENT 9 3/4" JOINTED
- (2) STABILIZED SUBBASE (HMA) 4 1/2"
- (3) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- (4) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- (5) COMB CC&G TB6.12
- (6) COMB CC&G TB6.24
- (7) PCC SHOULDERS, 9 3/4"
- (8) COURSE AGGREGATE FILL
- (9) HMA SHOULDERS, 8"
- (10) CONCRETE MEDIAN, TYPE SB (SPECIAL)
- (11) CONCRETE MEDIAN SURFACE 4"
- (12) TOPSOIL 6"
- (13) AGGREGATE SHOULDER, TYPE B 6"
- (14) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS @ 30" CTS
- (15) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS @ 24" CTS
- (16) NO. 6 TIE BARS @ 24" CTS
- (17) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (18) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (19) HOT MIX-ASHPALT BASE COURSE WIDENING 6 1/4"
- (20) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (21) HOT-MIX ASPHALT BASE COURSE WIDENING 7"
- (22) AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 SHEET 3)
- (23) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8 1/4"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19.0, N70, 6 1/4"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DOWEL BAR @ 24" CTS



DETAIL C-4

FROM STA. 388+42 TO STA. 392+34.70
* SHOULDER TRANSITIONS FROM -1.5% TO -4.0%
FROM STA. 388+42 TO STA. 388+92

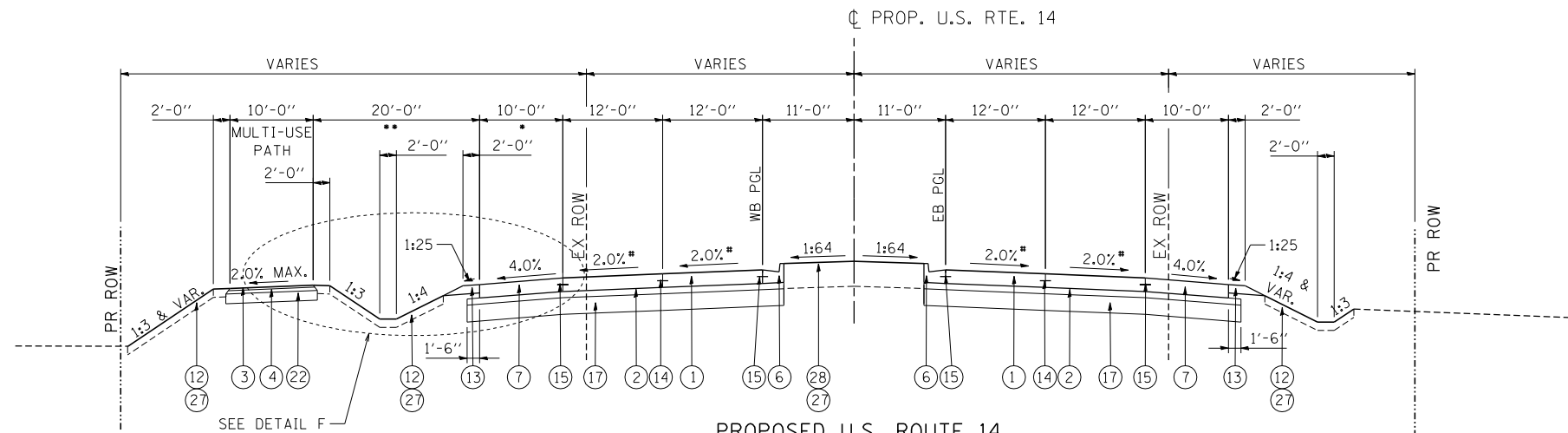


DETAIL D

FROM STA. 392+34.70 TO STA. 396+68

ATT:BD08\J.DON...GEM08\J7.DON...NTSC00_0C.DGN
 11-07-2013, 16:18:34
 HECHTBR T:\DOCUMENT\1934\9901\CIVIL\DON\TSC00_0C.DGN

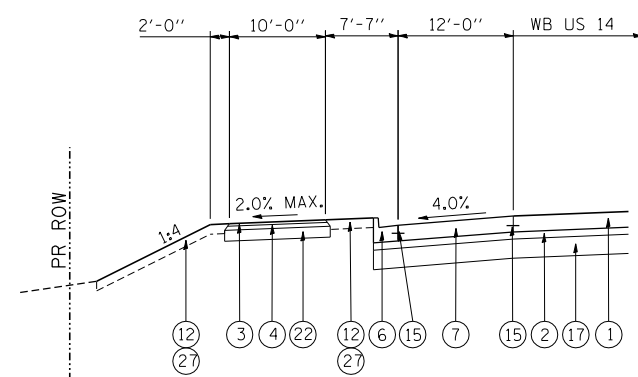
FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				PROPOSED TYPICAL SECTIONS U.S. ROUTE 14 STA. 380+71.55 TO STA. 402+40 (LT)				F.A.P. RTE. = 305	SECTION = 27R-2	COUNTY = MCHENRY	TOTAL SHEETS = 673	SHEET NO. = 21
#FILEL#	PLOT SCALE = *SCALE*	DRAWN - MRK	REVISED -									SCALE: N.T.S.	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS	FED. AID PROJECT
exp U.S. Services Inc. Chicago, IL BUILDING-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY		CHECKED - BRH	REVISED -	U.S. ROUTE 14				CONTRACT NO. 62268								
PLOT DATE = \$DATE*		DATE - 11/01/13	REVISED -													



PROPOSED U.S. ROUTE 14

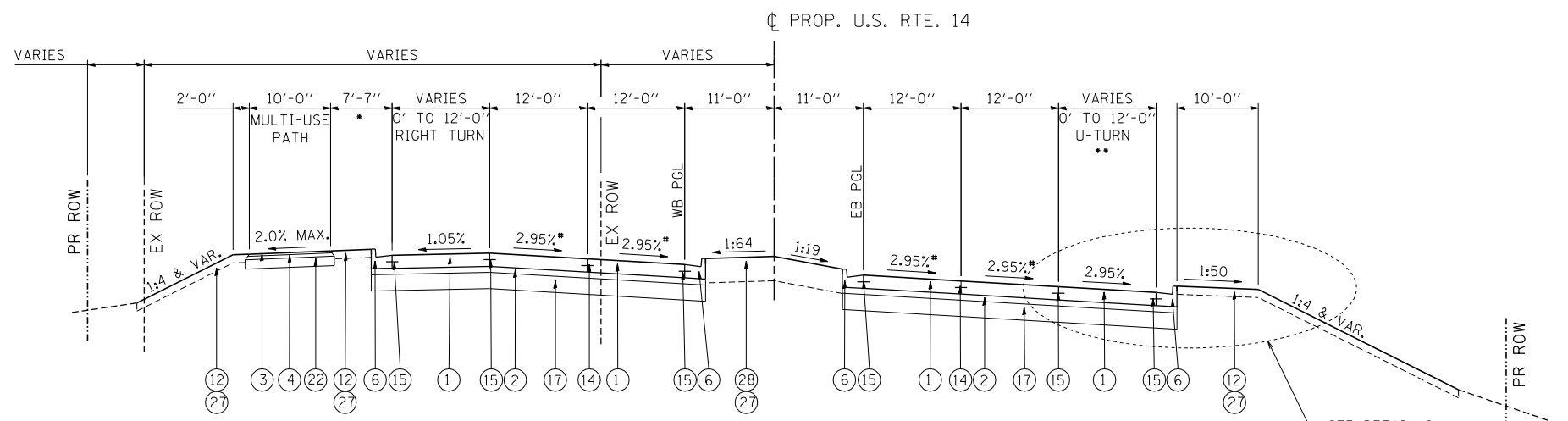
FROM STA. 402+40 TO STA. 424+90.41 (LT)
FROM STA. 402+86 TO STA. 424+88.52 (PC) (RT)

- * PAVEMENT CROSS SLOPE VARIES THRU SUPERELEVATION TRANSITION (SEE SHEET 77)
- SHOULDER 12' & VARIES FROM STA. 428+30.41 TO STA 430+50± (LT)



DETAIL F

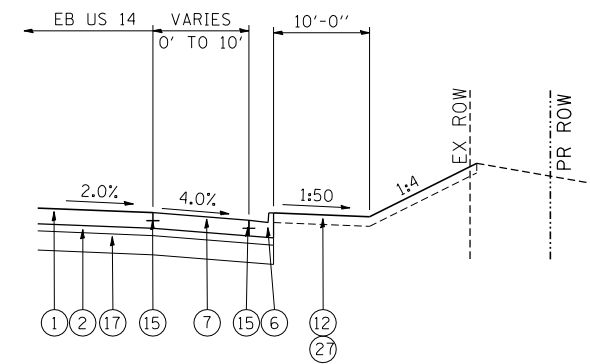
FROM STA. 428+90.42 TO STA 430+50±



PROPOSED U.S. ROUTE 14

FROM STA. 424+88.52 (PC) TO STA. 444+14.83 (PT)

- * PAVEMENT CROSS SLOPE VARIES THRU SUPERELEVATION TRANSITION (SEE SHEET 77)
- VARIES 20' TO 7'-7" FROM STA. 428+60± TO STA. 429+20±
- ** PAVEMENT VARIES 0' TO 12' FROM STA. 429+91.01 TO STA. 434+31.24



DETAIL G

FROM STA. 424+88.52 TO STA. 427+10.43

NOTES

1. SEE SHEET 24 FOR LEFT TURN LANE DETAILS.

EXISTING LEGEND

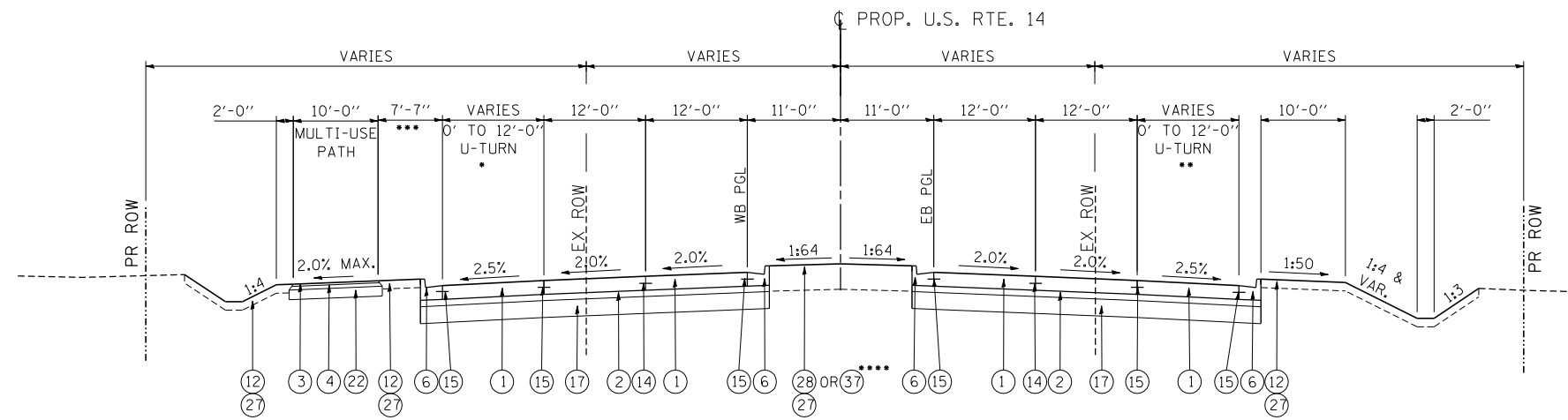
- (A) HMA PAVEMENT (VARIES 8" TO 12")
- (B) PCC PAVEMENT (VARIES 6 1/2" TO 8 1/4")
- (C) HMA PAVEMENT (4 1/2" AND VARIES)
- (D) HMA PAVEMENT (5" AND VARIES)
- (E) HMA PAVEMENT (6" AND VARIES)
- (F) CONCRETE MEDIAN SURFACE
- (G) CONCRETE CURB & GUTTER
- (H) HMA SHOULDER (8" & VARIES)
- (I) PCC BASE COURSE (10")
- (J) AGGREGATE SHOULDER
- (K) HMA SURFACE REMOVAL (VARIABLE DEPTH)
- (L) HMA SURFACE REMOVAL (2 1/4" MIN - COLD MILL)
- (M) HMA BINDER AND SURFACE CSE (3 1/2" & VARIES)

PROPOSED LEGEND

- (1) P.C.C. PAVEMENT 9 3/4" JOINTED
- (2) STABILIZED SUBBASE (HMA) 4 1/2"
- (3) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- (4) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- (5) COMB CC&G TB6.12
- (6) COMB CC&G TB6.24
- (7) PCC SHOULDERS, 9 3/4"
- (8) COURSE AGGREGATE FILL
- (9) HMA SHOULDERS, 8"
- (10) CONCRETE MEDIAN, TYPE SB (SPECIAL)
- (11) CONCRETE MEDIAN SURFACE 4"
- (12) TOPSOIL 6"
- (13) AGGREGATE SHOULDER, TYPE B 6"
- (14) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS @ 30" CTS
- (15) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS @ 24" CTS
- (16) NO. 6 TIE BARS @ 24" CTS
- (17) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (18) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (19) HOT MIX-ASHPALT BASE COURSE WIDENING 6 1/4"
- (20) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (21) HOT-MIX ASPHALT BASE COURSE WIDENING 7"
- (22) AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 SHEET 3)
- (23) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8 1/4" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 6 1/4"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DOWEL BAR @ 24" CTS
- (25) HMA DRIVEWAY (SEE DISTRICT DETAIL BD-01)
- (26) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 7"
- (27) SODDING OR SEEDING (SEE LANDSCAPING PLANS)
- (28) TOPSOIL 18"
- (29) AGGREGATE SHOULDER, TYPE B 8"
- (30) CORRUGATED MEDIAN
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
- (32) P.C.C. BASE COURSE WIDENING 10"
- (33) AGGREGATE WEDGE SHOULDER, TYPE B
- (34) COMB CC&G TM4.06
- (35) COMB CC&G TM4.24
- (36) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (3/4" & VARIES)
- (37) TOPSOIL 24"
- (38) GRADING AND SHAPING SHOULDERS
- (39) AGGREGATE SURFACE COURSE, TYPE B SPECIAL 2"
- (40) AGGREGATE BASE COURSE, TYPE B 6"
- (41) SHOULDER RUMBLE STRIPS, 8 INCH

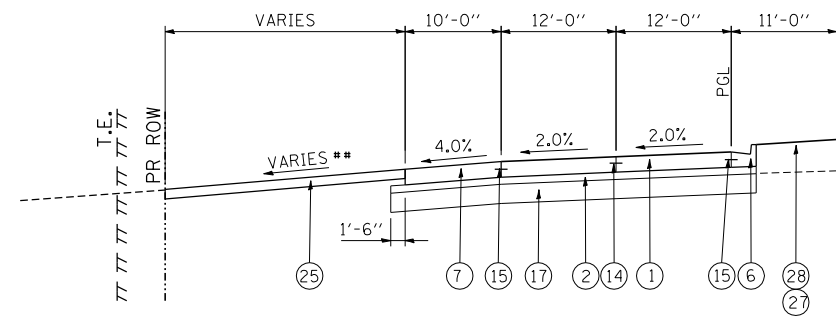
ATT:BD08J.DGN, G:\E:\080817.DGN, \TSC000_0C.DGN
 11-07-2013, 16:18:36
 HECHTBR T:\DOCUMENT\19349901\CIVIL\DCNTSC000_0M.DGN

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	PROPOSED TYPICAL SECTIONS U.S. ROUTE 14 STA. 402+40 TO STA. 444+14 (LT)			F.A.P. RTE. 305	SECTION 27R-2	COUNTY MCHENRY	TOTAL SHEETS 673	SHEET NO. 22
#FILEL#	PLOT SCALE = *SCALE*	DRAWN - MRK	REVISED -		SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 62268		
exp U.S. Services Inc. Chicago, IL	PLOT DATE = *DATE*	CHECKED - BRH	REVISED -									
INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY		DATE - 11/01/13	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



PROPOSED U.S. ROUTE 14

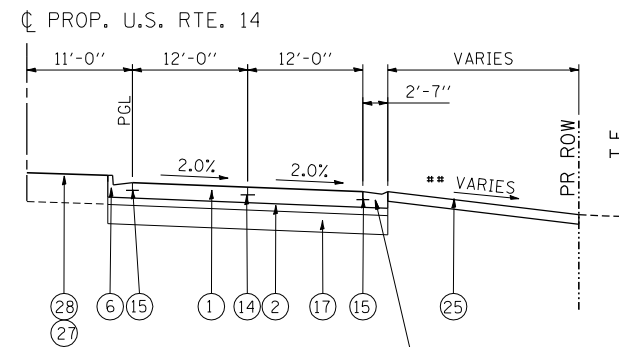
- FROM STA. 444+14.83 (PT) TO STA. 493+00
- * PAVEMENT CROSS SLOPE VARIES THRU SUPERELEVATION TRANSITION (SEE SHEET 79)
- PAVEMENT VARIES 0' TO 12' FROM STA. 447+80 TO STA. 451+95
- FROM STA. 464+46.31 TO STA. 467+70±
- PAVEMENT VARIES 0' TO 12' FROM STA. 450+70 TO STA. 454+85
- FROM STA. 466+84.49 TO STA. 471+33.38
- VARIES 7'-7" TO 20'-0" STA. 468+55.98 TO STA. 470+50±
- TOPSOIL FURNISH AND PLACE, 24" FROM STA. 473+48.38 TO STA. 493+00



PROPOSED DRIVEWAY OPENINGS AT SHOULDER

TYPICAL DRIVEWAY OPENINGS

** SEE CROSS SECTIONS FOR GRADES WHERE MULTI-USE PATH CROSSES DRIVEWAY



PROPOSED DRIVEWAY OPENINGS AT CURB & GUTTER

TYPICAL DRIVEWAY OPENINGS

STRUCTURAL DESIGN TRAFFIC:		YEAR <u>2022</u>
PV = <u>30,583</u>	SU = <u>1,512</u>	MU = <u>1,512</u>
ROAD/STREET CLASSIFICATION: CLASS <u>I</u>		
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:		
P = <u>32%</u>	S = <u>45%</u>	M = <u>45%</u>
TRAFFIC FACTOR	ACTUAL TF = <u>11.47</u>	
	MINIMUM TF = <u>6.03</u>	
SUBGRADE SUPPORT RATING:		
SSR = <u>POOR</u> (STA 326+00 TO 493+00) US-14		

NOTES

- SEE SHEET 24 FOR LEFT TURN LANE DETAILS.

- | | |
|--|---------|
| EXISTING LEGEND | REMOVAL |
| (A) HMA PAVEMENT (VARIES 8" TO 12") | |
| (B) PCC PAVEMENT (VARIES 6 1/2" TO 8 1/4") | |
| (C) HMA PAVEMENT (4 1/2" AND VARIES) | |
| (D) HMA PAVEMENT (5" AND VARIES) | |
| (E) HMA PAVEMENT (6" AND VARIES) | |
| (F) CONCRETE MEDIAN SURFACE | |
| (G) CONCRETE CURB & GUTTER | |
| (H) HMA SHOULDER (8" & VARIES) | |
| (I) PCC BASE COURSE (10") | |
| (J) AGGREGATE SHOULDER | |
| (K) HMA SURFACE REMOVAL (VARIABLE DEPTH) | |
| (L) HMA SURFACE REMOVAL (2 1/4" MIN - COLD MILL) | |
| (M) HMA BINDER AND SURFACE CSE (3 1/2" & VARIES) | |

PROPOSED LEGEND

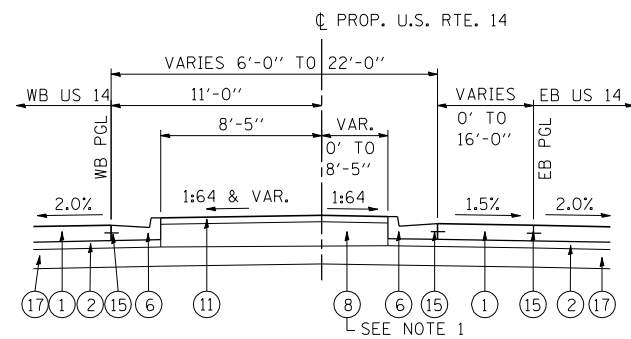
- | | |
|--|---|
| (1) P.C.C. PAVEMENT 9 3/4" JOINTED | (14) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS @ 30" CTS |
| (2) STABILIZED SUBBASE (HMA) 4 1/2" | (15) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS @ 24" CTS |
| (3) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2" | (16) NO. 6 TIE BARS @ 24" CTS |
| (4) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2" | (17) AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| (5) COMB CC&G TB6.12 | (18) STRIP REFLECTIVE CRACK CONTROL TREATMENT |
| (6) COMB CC&G TB6.24 | (19) HOT MIX-ASHPALT BASE COURSE WIDENING 6 1/4" |
| (7) PCC SHOULDERS, 9 3/4" | (20) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2" |
| (8) COURSE AGGREGATE FILL | (21) HOT-MIX ASPHALT BASE COURSE WIDENING 7" |
| (9) HMA SHOULDERS, 8" | (22) AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 SHEET 3) |
| (10) CONCRETE MEDIAN, TYPE SB (SPECIAL) | (23) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8 1/4" |
| (11) CONCRETE MEDIAN SURFACE 4" | HMA SURFACE COURSE, MIX "D", N70, 2" |
| (12) TOPSOIL 6" | HMA BINDER COURSE, IL-19.0, N70, 6 1/4" |
| (13) AGGREGATE SHOULDER, TYPE B 6" | (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DOWEL BAR @ 24" CTS |

MIXTURE TYPE	AIR VOIDS @ NDES
PAVEMENT RESURFACING (US 14)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm) (1 1/2")	4% @ 70 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 (3/4" - 1" PER LIFT)	3.5% @ 50 GYR.
FULL-DEPTH PAVEMENT RECONSTRUCTION (LILY POND RD)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) (2")	4% @ 70 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (6 1/4") (2 LIFTS)	4% @ 70 GYR.
FULL-DEPTH PAVEMENT RECONSTRUCTION (RIDGEFIELD ROAD NORTH)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) (2")	4% @ 70 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (7") (2 LIFTS)	4% @ 70 GYR.
PAVEMENT RESURFACING (RIDGEFIELD ROAD NORTH)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) (2")	4% @ 70 GYR.
LEVELING BINDER (MACHINE METHOD), N70 (IL-9.5 mm) (3/4" - 2 1/4" PER LIFT)	4% @ 70 GYR.
PAVEMENT RESURFACING (LILY POND RD)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) (2")	4% @ 70 GYR.
LEVELING BINDER (MACHINE METHOD), N70 (IL-9.5 mm) (3/4" - 2 1/4" PER LIFT)	4% @ 70 GYR.
PAVEMENT WIDENING (LILY POND RD)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) (2")	4% @ 70 GYR.
LEVELING BINDER (MACHINE METHOD), N70 (IL-9.5 mm) (3/4" - 2 1/4" PER LIFT)	4% @ 70 GYR.
HOT-MIX ASPHALT BASE COURSE WIDENING (HMA BINDER IL-19 mm), 6 1/4" (2 LIFTS)	4% @ 70 GYR.
PAVEMENT WIDENING (RIDGEFIELD ROAD NORTH)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) (2")	4% @ 70 GYR.
LEVELING BINDER (MACHINE METHOD), N70 (IL-9.5 mm) (3/4" - 2 1/4" PER LIFT)	4% @ 70 GYR.
HOT-MIX ASPHALT BASE COURSE WIDENING (HMA BINDER IL-19 mm), 7" (2 LIFTS)	4% @ 70 GYR.
SHOULDER RECONSTRUCTION (RIDGEFIELD ROAD NORTH, LILY POND RD)	
HMA SHOULDER (HMA BINDER IL-19mm) (2 LIFTS)	4% @ 50 GYR.
PAVEMENT PATCHING (US 14, LAKE SHORE DR, DOTY RD)	
CLASS D PATCH (HMA BINDER IL-19mm) (10")	4% @ 70 GYR.
TEMPORARY PAVEMENT (SEE NOTE 3)	
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm) (2")	4% @ 50 GYR.
TEMP PAVEMENT (HMA BINDER IL-19mm) (8 1/4")	4% @ 50 GYR.
TEMPORARY HMA JOINT REPAIR	
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm) (2")	4% @ 50 GYR.
DRIVEWAYS (PRIVATE/ COMMERCIAL)	
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm) (2")	4% @ 50 GYR.
HMA BASE COURSE (HMA BINDER IL-19 mm) PE-6", CE-8"	4% @ 50 GYR.
DRIVEWAYS (FIELD ENTRANCE)	
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm) (2")	4% @ 50 GYR.
WINTERIZED TEMPORARY ACCESS	
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm) (2")	4% @ 50 GYR.
STABILIZED SUBBASE (US 14, LAKE SHORE DR, DOTY RD)	
STABILIZED SUBBASE-HOT MIX ASPHALT (HMA BINDER IL-19mm) (4 1/2")	3% @ 50 GYR.
MULTI-USE PATH	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm) (1 1/2")	4% @ 50 GYR.
HOT-MIX ASPHALT BINDER COURSE, (HMA BINDER IL-19 mm), N50 (2 1/2")	4% @ 50 GYR.

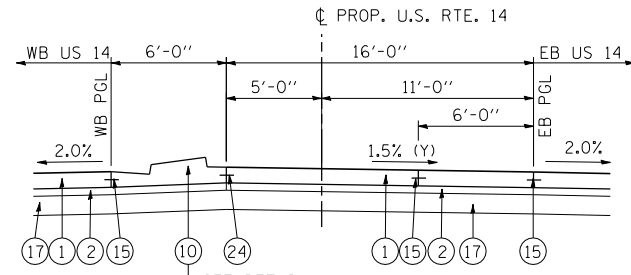
- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQY YD/IN.
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
- CONTRACTOR OPTION: PC CONCRETE TEMPORARY PAVEMENT, SHALL CONSIST OF CLASS PV CONCRETE MEETING THE REQUIREMENTS OF ART. 1020 OF THE STANDARD SPECIFICATIONS. THICKNESS SHALL BE 8-1/4".

- | | |
|---|--|
| (25) HMA DRIVEWAY (SEE DISTRICT DETAIL BD-01) | (36) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (3/4" & VARIES) |
| (26) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 7" | (37) TOPSOIL 24" |
| (27) SODDING OR SEEDING (SEE LANDSCAPING PLANS) | (38) GRADING AND SHAPING SHOULDERS |
| (28) TOPSOIL 18" | (39) AGGREGATE SURFACE COURSE, TYPE B SPECIAL 2" |
| (29) AGGREGATE SHOULDER, TYPE B 8" | (40) AGGREGATE BASE COURSE, TYPE B 6" |
| (30) CORRUGATED MEDIAN | (41) SHOULDER RUMBLE STRIPS, 8 INCH |
| (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2" | |
| (32) P.C.C. BASE COURSE WIDENING 10" | |
| (33) AGGREGATE WEDGE SHOULDER, TYPE B | |
| (34) COMB CC&G TM4.06 | |
| (35) COMB CC&G TM4.24 | |

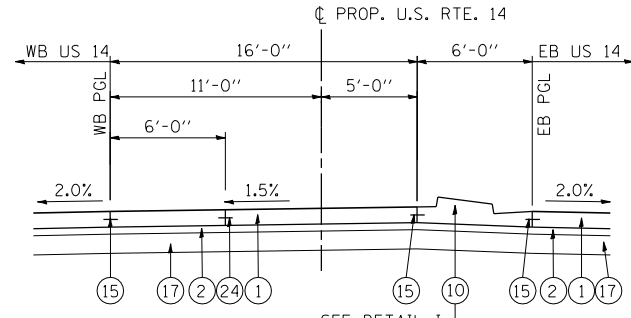
ATT:TB008J1.DGN...G:\E\00817.DGN...TSC000_0C.DGN
11-07-2013, 16:18:38
HECHTBR T:\DOCUMENT\1934\9801\CIVIL\DGN\TSC000_0C.DGN



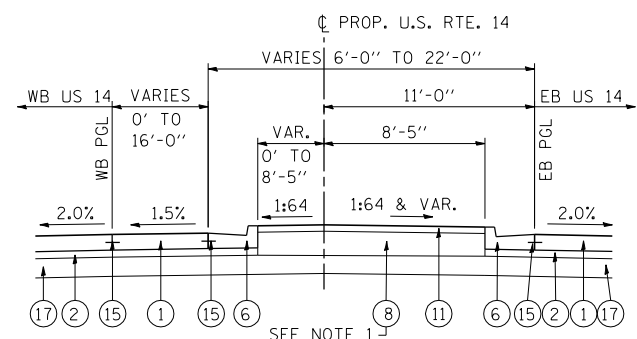
FROM STA. 346+41 TO STA. 348+61
 FROM STA. 360+60 TO STA. 362+80
 FROM STA. 446+65 TO STA. 448+85
 FROM STA. 462+86 TO STA. 465+06



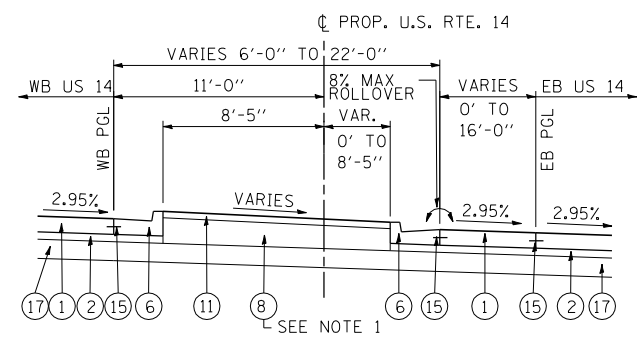
FROM STA. 362+80 TO STA. 364+95
 FROM STA. 393+58 TO STA. 396+73 (SEE TABLE 1)
 FROM STA. 448+85 TO STA. 451+00
 FROM STA. 465+06 TO STA. 467+21



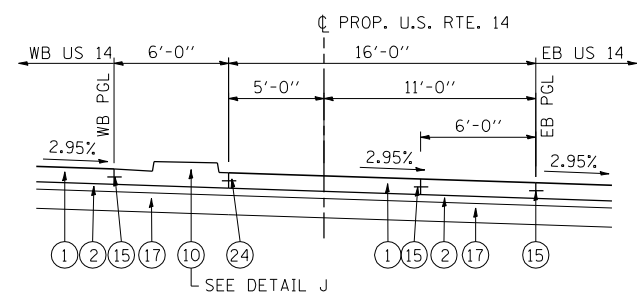
FROM STA. 366+10 TO STA. 368+25
 FROM STA. 397+95 TO STA. 400+10
 FROM STA. 451+65 TO STA. 453+80
 FROM STA. 468+13 TO STA. 470+28



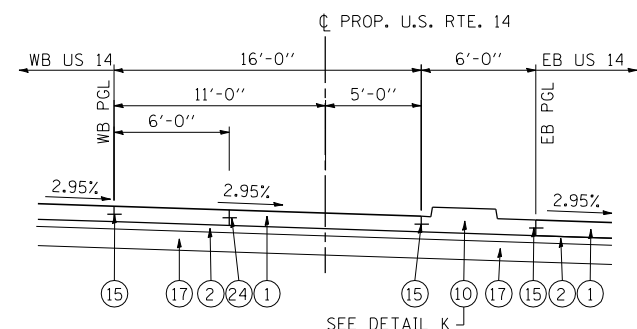
FROM STA. 368+25 TO STA. 370+45
 FROM STA. 400+10 TO STA. 402+30
 FROM STA. 453+80 TO STA. 456+00
 FROM STA. 470+28 TO STA. 472+48



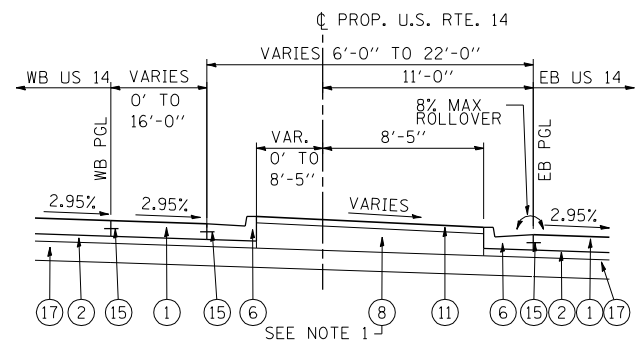
FROM STA. 425+91 TO STA. 427+06



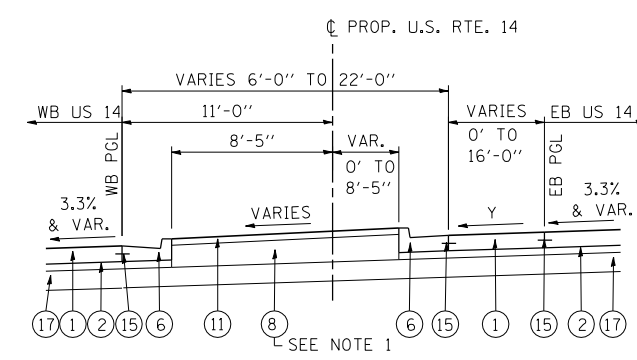
FROM STA. 427+06 TO STA. 430+08



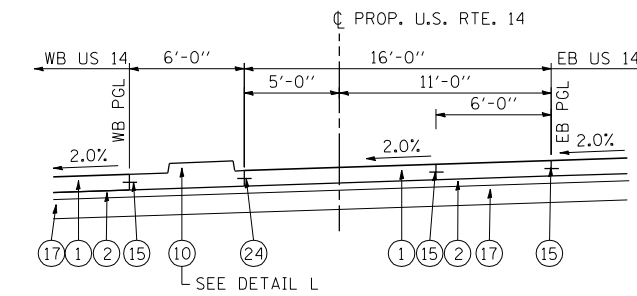
FROM STA. 431+05 TO STA. 434+13



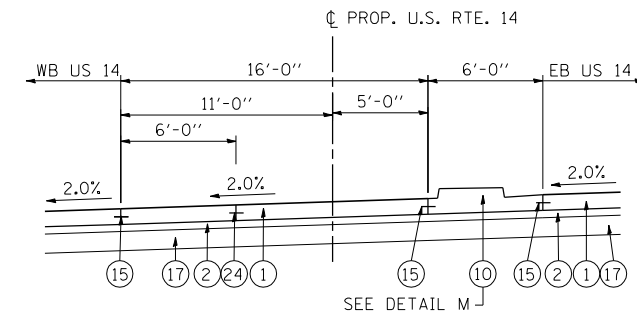
FROM STA. 434+13 TO STA. 435+43



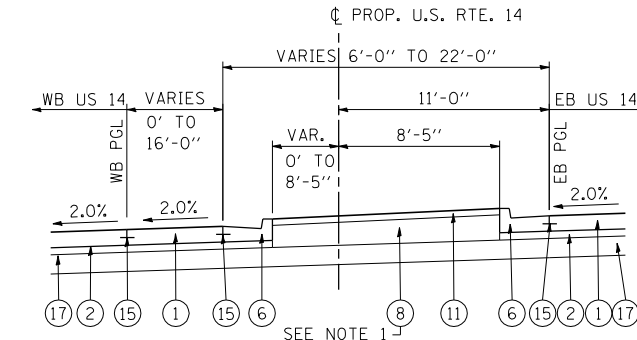
FROM STA. 392+10 TO STA. 393+58 (SEE TABLE 1)



FROM STA. 348+61 TO STA. 350+77



FROM STA. 351+47 TO STA. 353+63

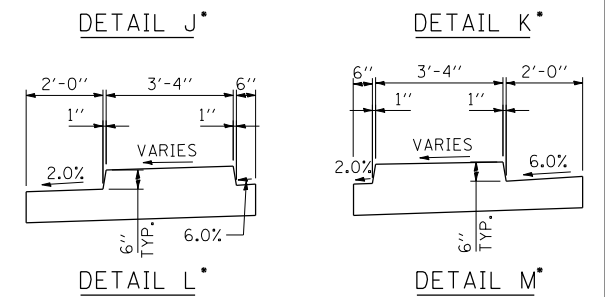
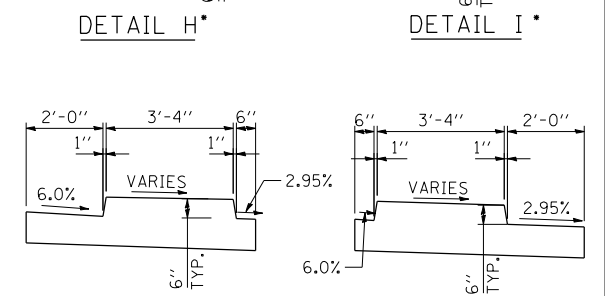
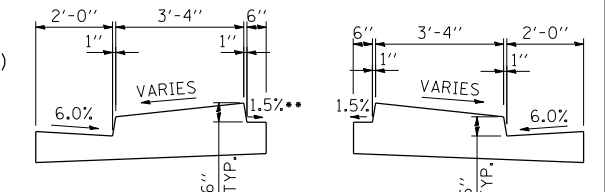


FROM STA. 353+63 TO STA. 355+83

FROM	TO	Y
STA. 392+10	STA. 392+75	-3.30
STA. 392+75	STA. 393+58	-3.30% TO -0.97%
STA. 393+58	STA. 393+94	-0.97% TO 0%
STA. 393+94	STA. 394+48	0% TO 1.5%
STA. 394+48	STA. 396+73	1.5%

TABLE 1

PAVEMENT SLOPES ARE SHOWN WITH RESPECT TO THE EB PGL



- * SEE HIGHWAY STANDARD 606301 FOR ADDITIONAL DETAILS AND RAMP NOSE LENGTHS
- ** GUTTER SLOPE TRANSITIONS FROM -6% TO 1.5% FROM STA. 393+94 TO STA. 394+48.

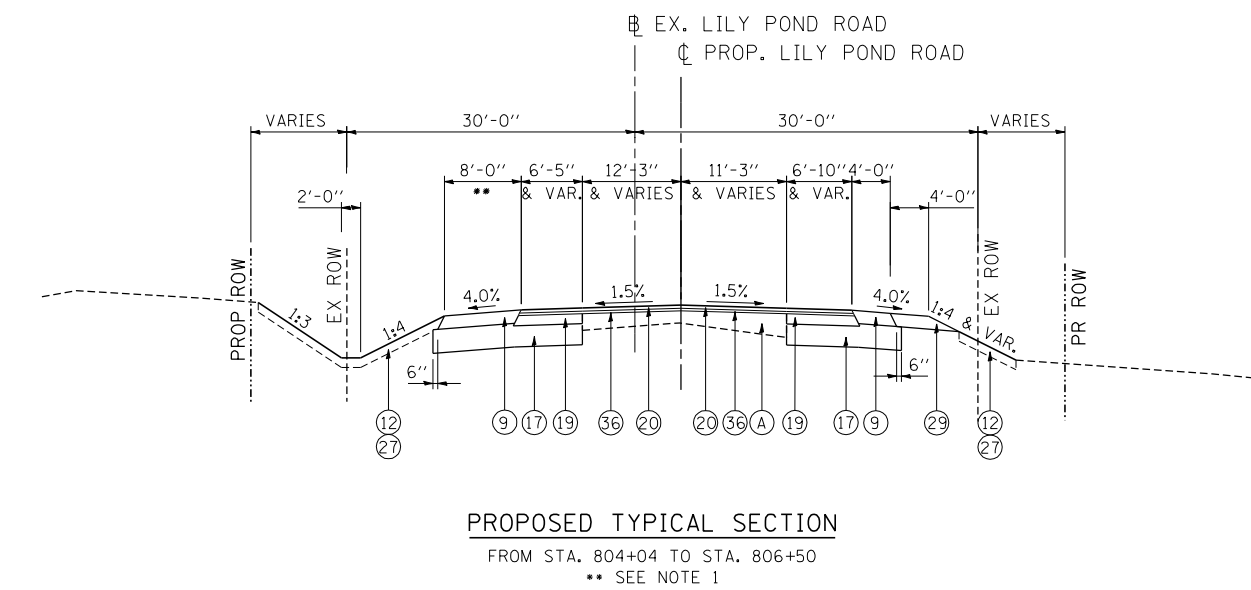
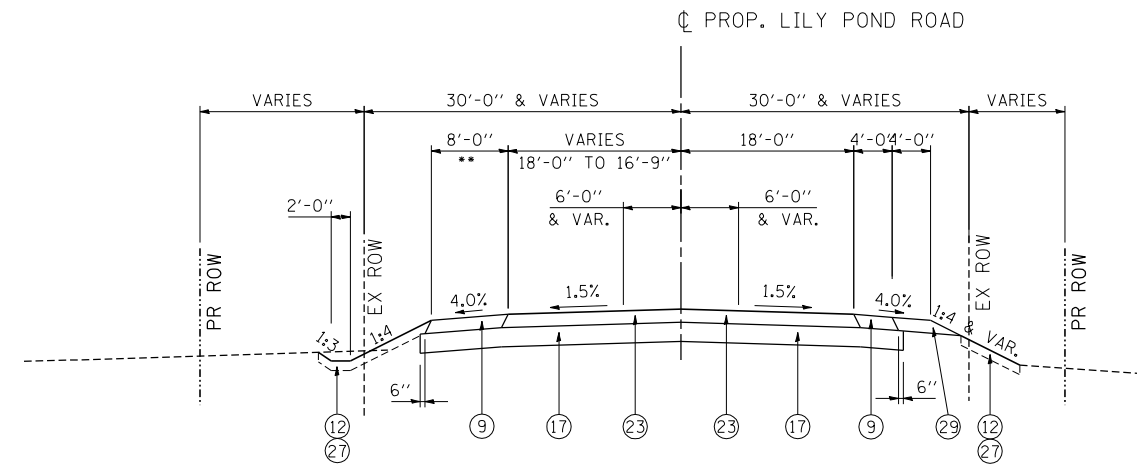
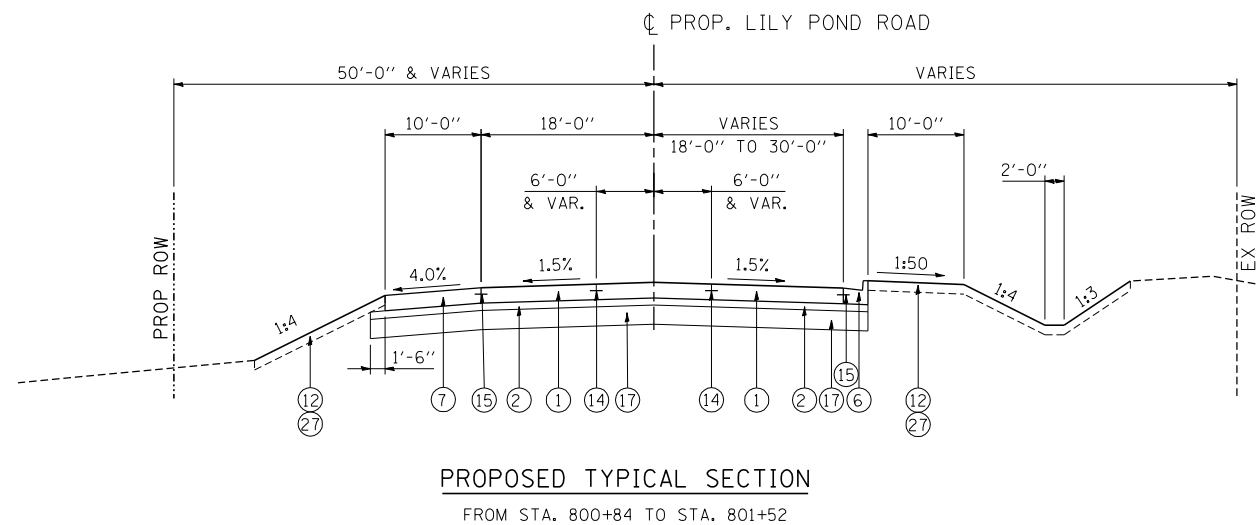
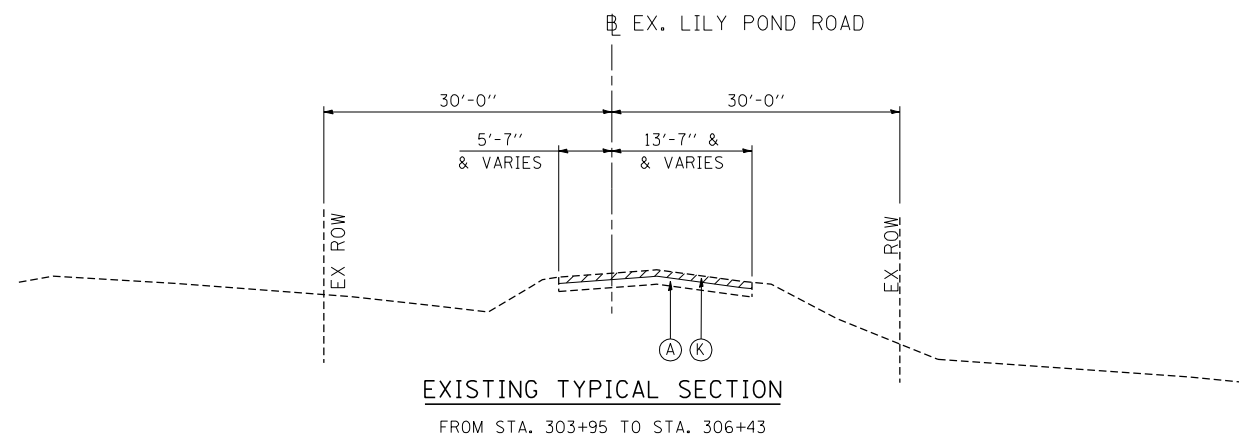
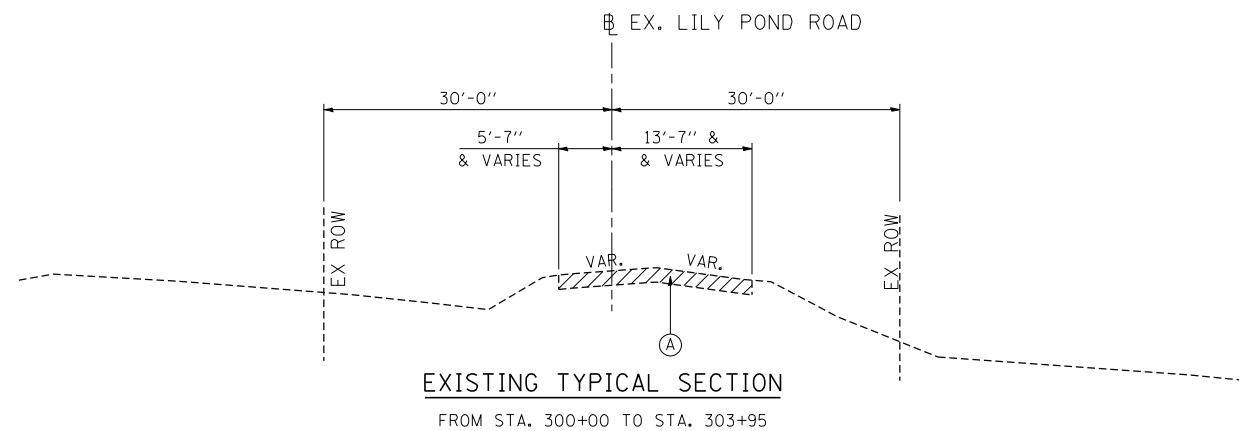
NOTES

1. COARSE AGGREGATE FILL SHALL BE CONSTRUCTED IN ACCORDANCE WITH ART. 606.09 OF THE STANDARD SPECIFICATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR CONCRETE MEDIAN SURFACE 4".
2. SEE SHEETS 76 TO 79 FOR EB AND WB FOR U.S. ROUTE 14 SUPERELEVATION TRANSITIONS.

PROPOSED LEGEND

- | | | | |
|--|---|---|--|
| 1 P.C.C. PAVEMENT 9 3/4" JOINTED | 14 LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS @ 30" CTS | 25 HMA DRIVEWAY (SEE DISTRICT DETAIL BD-01) | 36 POLYMERIZED LEVELING BINDER (MACHIN IL-4.75, N50, (3/4" & VARIES) |
| 2 STABILIZED SUBBASE (HMA) 4 1/2" | 15 LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS @ 24" CTS | 26 HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 7" | 37 TOPSOIL 24" |
| 3 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2" | 16 NO. 6 TIE BARS @ 24" CTS | 27 SODDING OR SEEDING (SEE LANDSCAPING PLANS) | 38 GRADING AND SHAPING SHOULDERS |
| 4 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2" | 17 AGGREGATE SUBGRADE IMPROVEMENT, 12" | 28 TOPSOIL 18" | 39 AGGREGATE SURFACE COURSE, TYPE B 5" |
| 5 COMB CC&G TB6.12 | 18 STRIP REFLECTIVE CRACK CONTROL TREATMENT | 29 AGGREGATE SHOULDER, TYPE B 8" | 40 AGGREGATE BASE COURSE, TYPE B 6" |
| 6 COMB CC&G TB6.24 | 19 HOT MIX-ASHPALT BASE COURSE WIDENING 6 1/4" | 30 CORRUGATED MEDIAN | 41 SHOULDER RUMBLE STRIPS, 8 INCH |
| 7 PCC SHOULDERS, 9 3/4" | 20 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2" | 31 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2" | |
| 8 COURSE AGGREGATE FILL | 21 HOT-MIX ASPHALT BASE COURSE WIDENING 7" | 32 P.C.C. BASE COURSE WIDENING 10" | |
| 9 HMA SHOULDERS, 8" | 22 AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 SHEET 3) | 33 AGGREGATE WEDGE SHOULDER, TYPE B | |
| 10 CONCRETE MEDIAN, TYPE SB (SPECIAL) | 23 HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8 1/4" | 34 COMB CC&G TM4.06 | |
| 11 CONCRETE MEDIAN SURFACE 4" | 24 LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DOWEL BAR @ 24" CTS | 35 COMB CC&G TM4.24 | |
| 12 TOPSOIL 6" | | | |
| 13 AGGREGATE SHOULDER, TYPE B 6" | | | |

ATT:TB08RJ.DGN, G:\M08\17.DGN, T:\SC00\0C.DGN, HECHTBR T:\DOCUMENT\1934\9901\CIVIL\DGN\TSC00_146.DGN
 11-07-2013, 16:18:41



NOTE

1. HMA SHOULDER AND AGGREGATE SUBGRADE SHALL BE CONSTRUCTED WITH ADDITIONAL WIDTH TO MAINTAIN TRAFFIC SHOWN IN STAGE 2.1 SUBSTAGE A-3. HMA SHOULDER SHALL BE CONSTRUCTED 1' WIDER THAN EDGE OF TRAVELED LANE SHOWN ON SHT 139. ONCE LILY POND RD IS CONSTRUCTED AND TRAFFIC MOVED TO PERMANENT LOCATION WIDEN PORTION OF HMA SHOULDER SHALL BE FULL DEPTH SAW CUT AND REMOVED TO WIDTH SHOWN ON THE PLANS AND TYPICAL SECTION. REMOVAL OF THIS PORTION OF HMA SHOULDER WILL BE PAID FOR AS (INCLUDING FULL DEPTH SAW CUT) PAVED SHOULDER REMOVAL.

EXISTING LEGEND

- (A) HMA PAVEMENT (VARIES 8" TO 12")
- (B) PCC PAVEMENT (VARIES 6 1/2" TO 8 1/4")
- (C) HMA PAVEMENT (4 1/2" AND VARIES)
- (D) HMA PAVEMENT (5" AND VARIES)
- (E) HMA PAVEMENT (6" AND VARIES)
- (F) CONCRETE MEDIAN SURFACE
- (G) CONCRETE CURB & GUTTER
- (H) HMA SHOULDER (8" & VARIES)
- (I) PCC BASE COURSE (10")
- (J) AGGREGATE SHOULDER
- (K) HMA SURFACE REMOVAL (VARIABLE DEPTH)
- (L) HMA SURFACE REMOVAL (2 1/4" MIN - COLD MILL)
- (M) HMA BINDER AND SURFACE CSE (3 1/2" & VARIES)

PROPOSED LEGEND

- (1) P.C.C. PAVEMENT 9 3/4" JOINTED
- (2) STABILIZED SUBBASE (HMA) 4 1/2"
- (3) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- (4) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- (5) COMB CC&G TB6.12
- (6) COMB CC&G TB6.24
- (7) PCC SHOULDERS, 9 3/4"
- (8) COURSE AGGREGATE FILL
- (9) HMA SHOULDERS, 8"
- (10) CONCRETE MEDIAN, TYPE SB (SPECIAL)
- (11) CONCRETE MEDIAN SURFACE 4"
- (12) TOPSOIL 6"
- (13) AGGREGATE SHOULDER, TYPE B 6"
- (14) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS @ 30" CTS
- (15) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS @ 24" CTS
- (16) NO. 6 TIE BARS @ 24" CTS
- (17) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (18) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (19) HOT MIX-ASHPALT BASE COURSE WIDENING 6 1/4"
- (20) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (21) HOT-MIX ASPHALT BASE COURSE WIDENING 7"
- (22) AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 SHEET 3)
- (23) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8 1/4" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 6 1/4"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DOWEL BAR @ 24" CTS

- (25) HMA DRIVEWAY (SEE DISTRICT DETAIL BD-01)
- (26) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 7"
- (27) SODDING OR SEEDING (SEE LANDSCAPING PLANS)
- (28) TOPSOIL 18"
- (29) AGGREGATE SHOULDER, TYPE B 8"
- (30) CORRUGATED MEDIAN
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
- (32) P.C.C. BASE COURSE WIDENING 10"
- (33) AGGREGATE WEDGE SHOULDER, TYPE B
- (34) COMB CC&G TM4.06
- (35) COMB CC&G TM4.24
- (36) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (3/4" & VARIES)
- (37) TOPSOIL 24"
- (38) GRADING AND SHAPING SHOULDERS
- (39) AGGREGATE SURFACE COURSE, TYPE B SPECIAL 2"
- (40) AGGREGATE BASE COURSE, TYPE B 6"
- (41) SHOULDER RUMBLE STRIPS, 8 INCH

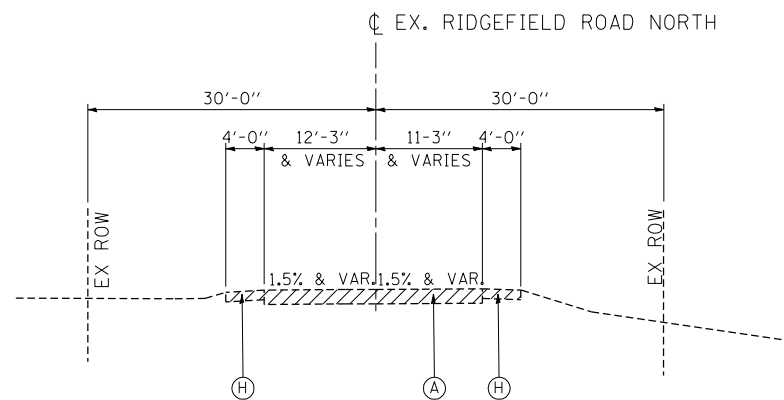
ATT:TB08RJJ.DGN, .GEM08RJJ.DGN, .NTSC08R.DGN
 11-07-2013, 16:18:43
 HECHTBR T:\DOCUMENT\1934\9901\CIVIL\DOCS\TSC08R.DGN

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
#FILEL#		DRAWN - MRK	REVISED -
	PLOT SCALE = *SCALE*	CHECKED - BRH	REVISED -
	PLOT DATE = *DATE*	DATE - 11/01/13	REVISED -

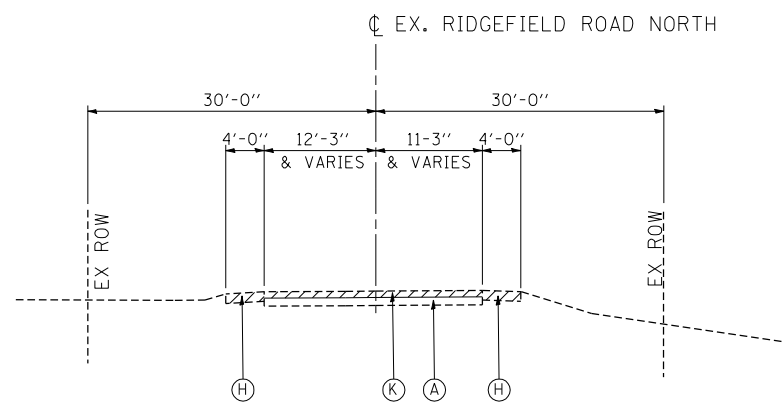
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

TYPICAL SECTIONS			
LILY POND ROAD			
SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA. TO STA.

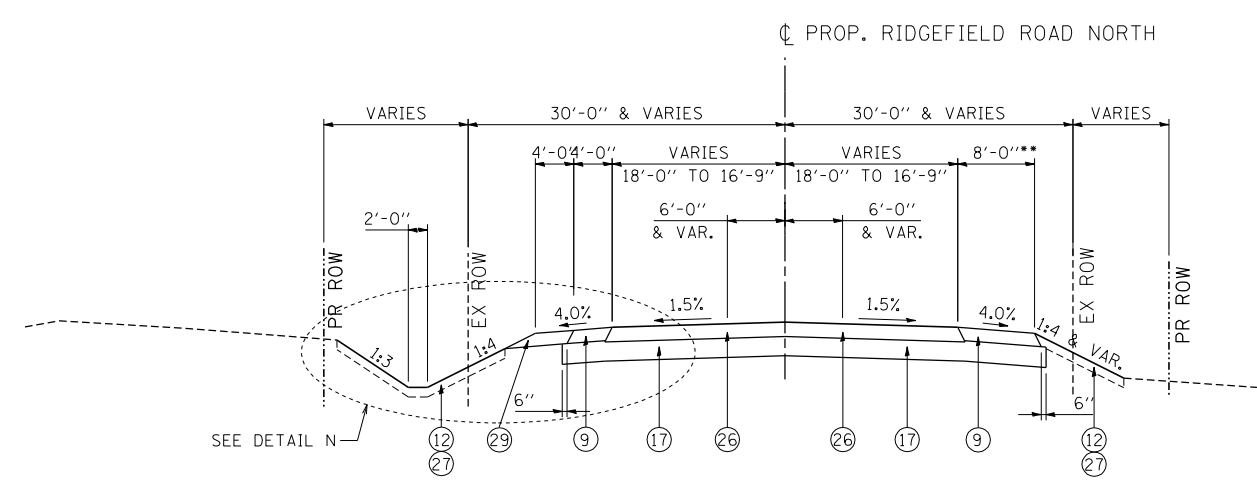
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	25
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



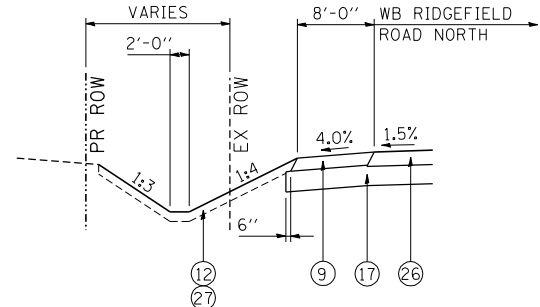
EXISTING TYPICAL SECTION
FROM STA. 431+61 (EX) TO STA. 405+06 (EX)



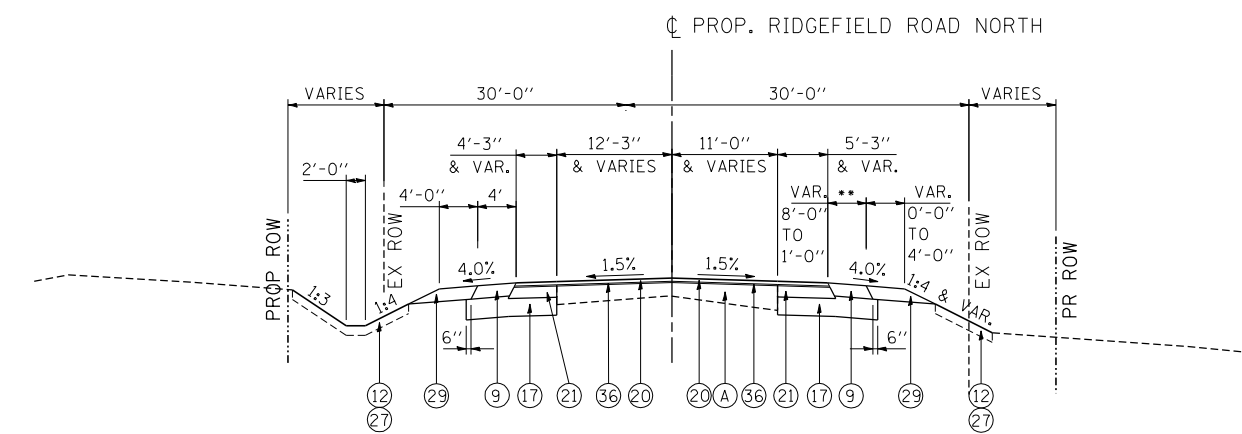
EXISTING TYPICAL SECTION
FROM STA. 405+06 (EX) TO STA. 407+59 (EX)



PROPOSED TYPICAL SECTION
FROM STA. 851+00 TO STA. 854+50
** SEE NOTE 1



DETAIL N
FROM STA. 851+00 TO STA. 852+14



PROPOSED TYPICAL SECTION
FROM STA. 854+50 TO STA. 407+59
(STATION EQUATION: STA. 856+43.40 BK = STA. 407+00 AH)
** SEE NOTE 1

NOTE

1. HMA SHOULDER AND AGGREGATE SUBGRADE SHALL BE CONSTRUCTED WITH ADDITIONAL WIDTH TO MAINTAIN TRAFFIC SHOWN IN STAGE 2.1 SUBSTAGE B-3. HMA SHOULDER SHALL BE CONSTRUCTED 1' WIDER THAN EDGE OF TRAVELED LANE SHOWN ON SHT 141. ONCE RIDGEFIELD RD NORTH IS CONSTRUCTED AND TRAFFIC MOVED TO PERMANENT LOCATION WIDEN PORTION OF HMA SHOULDER SHALL BE FULL DEPTH SAW CUT AND REMOVED TO WIDTH SHOWN ON THE PLANS AND TYPICAL SECTION. REMOVAL OF THIS PORTION OF HMA SHOULDER WILL BE PAID FOR AS (INCLUDING FULL DEPTH SAW CUT) PAVED SHOULDER REMOVAL.

EXISTING LEGEND

- (A) HMA PAVEMENT (VARIES 8" TO 12")
- (B) PCC PAVEMENT (VARIES 6 1/2" TO 8 1/4")
- (C) HMA PAVEMENT (4 1/2" AND VARIES)
- (D) HMA PAVEMENT (5" AND VARIES)
- (E) HMA PAVEMENT (6" AND VARIES)
- (F) CONCRETE MEDIAN SURFACE
- (G) CONCRETE CURB & GUTTER
- (H) HMA SHOULDER (8" & VARIES)
- (I) PCC BASE COURSE (10")
- (J) AGGREGATE SHOULDER
- (K) HMA SURFACE REMOVAL (VARIABLE DEPTH)
- (L) HMA SURFACE REMOVAL (2 1/4" MIN - COLD MILL)
- (M) HMA BINDER AND SURFACE CSE (3 1/2" & VARIES)

PROPOSED LEGEND

- (1) P.C.C. PAVEMENT 9 3/4" JOINTED
- (2) STABILIZED SUBBASE (HMA) 4 1/2"
- (3) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- (4) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- (5) COMB CC&G TB6.12
- (6) COMB CC&G TB6.24
- (7) PCC SHOULDERS, 9 3/4"
- (8) COURSE AGGREGATE FILL
- (9) HMA SHOULDERS, 8"
- (10) CONCRETE MEDIAN, TYPE SB (SPECIAL)
- (11) CONCRETE MEDIAN SURFACE 4"
- (12) TOPSOIL 6"
- (13) AGGREGATE SHOULDER, TYPE B 6"

- (14) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS @ 30" CTS
- (15) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS @ 24" CTS
- (16) NO. 6 TIE BARS @ 24" CTS
- (17) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (18) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (19) HOT MIX-ASHPALT BASE COURSE WIDENING 6 1/4"
- (20) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (21) HOT-MIX ASPHALT BASE COURSE WIDENING 7"
- (22) AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 SHEET 3)
- (23) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8 1/4" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 6 1/4"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DOWEL BAR @ 24" CTS

- (25) HMA DRIVEWAY (SEE DISTRICT DETAIL BD-01)
- (26) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 7"
- (27) SODDING OR SEEDING (SEE LANDSCAPING PLANS)
- (28) TOPSOIL 18"
- (29) AGGREGATE SHOULDER, TYPE B 8"
- (30) CORRUGATED MEDIAN
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
- (32) P.C.C. BASE COURSE WIDENING 10"
- (33) AGGREGATE WEDGE SHOULDER, TYPE B
- (34) COMB CC&G TM4.06
- (35) COMB CC&G TM4.24

- (36) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (3/4" & VARIES)
- (37) TOPSOIL 24"
- (38) GRADING AND SHAPING SHOULDERS
- (39) AGGREGATE SURFACE COURSE, TYPE B SPECIAL 2"
- (40) AGGREGATE BASE COURSE, TYPE B 6"
- (41) SHOULDER RUMBLE STRIPS, 8 INCH

ATT:BD08J.DGN, .GEM008J7.DGN, .NTSC000.OC.DGN, .HECHTBR T:\DOCUMENT\1934\9801\CIVIL\DGN\TSC000.HS.DGN 11-07-2013, 16:18:46

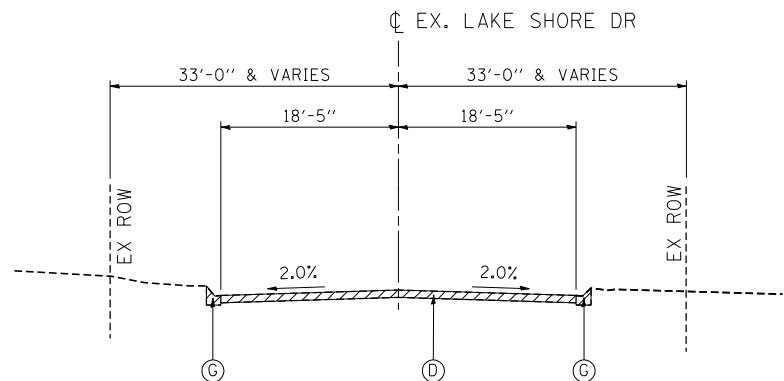
FILE NAME = USER NAME = HECHTBR
 #FILEL#
 exp U.S. Services Inc. Chicago, IL
 BUILDINGS-EARTH & ENVIRONMENT-ENERGY
 INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY

DESIGNED -	REVISED -
DRAWN - MRK	REVISED -
CHECKED - BRH	REVISED -
DATE - 11/01/13	REVISED -

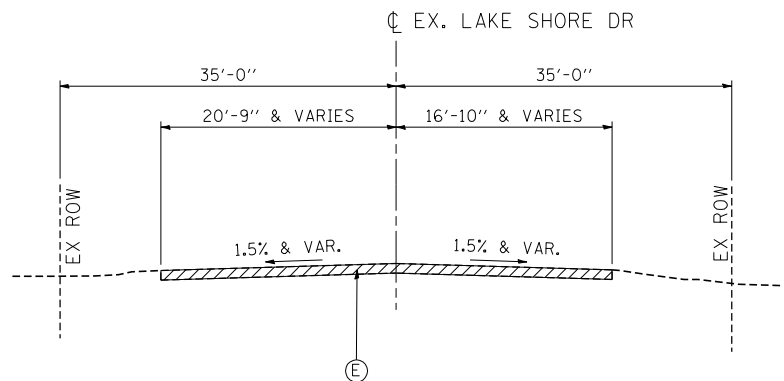
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

TYPICAL SECTIONS			
RIDGEFIELD ROAD NORTH			
SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA. TO STA.

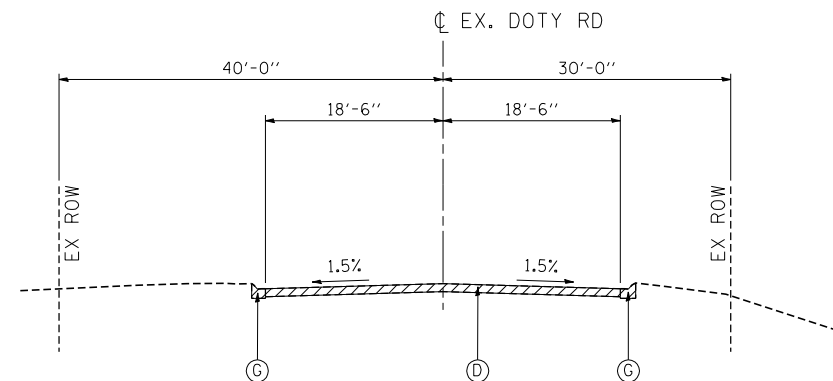
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	26
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



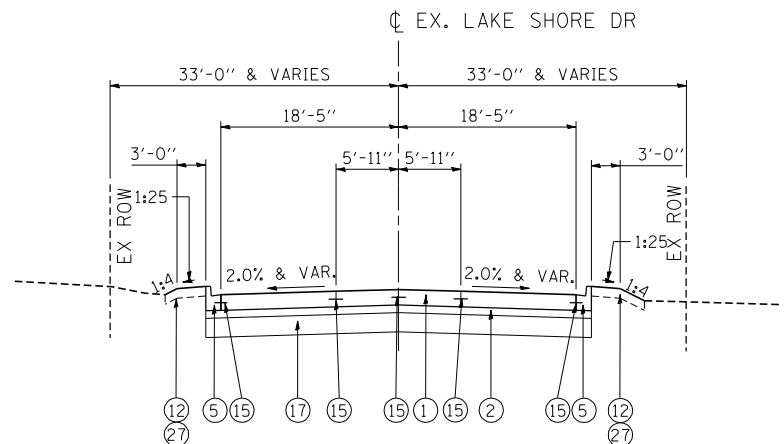
EXISTING LAKE SHORE DRIVE TYPICAL SECTION
FROM STA. 43+00 TO 44+88



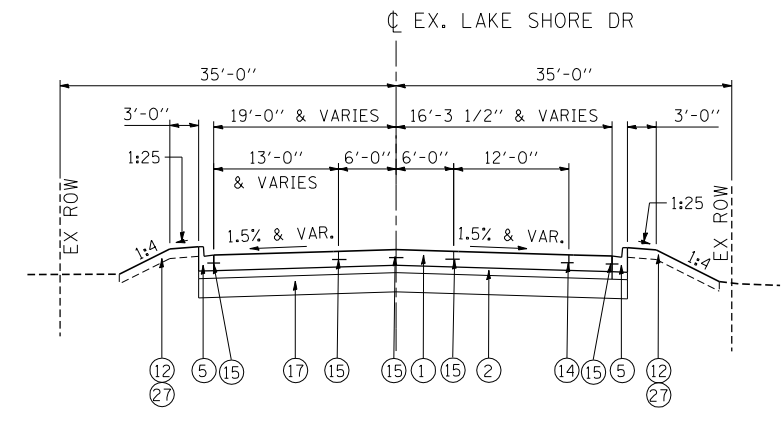
EXISTING LAKE SHORE DRIVE TYPICAL SECTION
FROM STA. 45+51 TO STA. 47+13



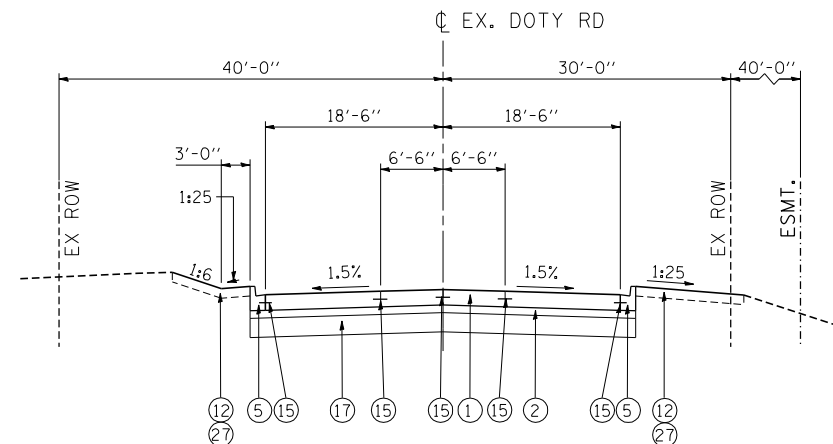
EXISTING DOTY ROAD TYPICAL SECTION
FROM STA. 0+14 TO STA. 1+67
(SEE NOTE 2)



PROPOSED LAKE SHORE DRIVE TYPICAL SECTION
FROM STA. 43+00 TO 44+88



PROPOSED LAKE SHORE DRIVE TYPICAL SECTION
FROM STA. 45+51 TO STA. 47+13



PROPOSED DOTY ROAD TYPICAL SECTION
FROM STA. 0+14 TO STA. 1+67

NOTES

- EXISTING THICKNESSES SHOWN ARE FROM PAVEMENT CORES. ACTUAL THICKNESSES MAY VARY.

EXISTING LEGEND REMOVAL

- (A) HMA PAVEMENT (VARIES 8" TO 12")
- (B) PCC PAVEMENT (VARIES 6 1/2" TO 8 1/4")
- (C) HMA PAVEMENT (4 1/2" AND VARIES)
- (D) HMA PAVEMENT (5" AND VARIES)
- (E) HMA PAVEMENT (6" AND VARIES)
- (F) CONCRETE MEDIAN SURFACE
- (G) CONCRETE CURB & GUTTER
- (H) HMA SHOULDER (8" & VARIES)
- (I) PCC BASE COURSE (10")
- (J) AGGREGATE SHOULDER
- (K) HMA SURFACE REMOVAL (VARIABLE DEPTH)
- (L) HMA SURFACE REMOVAL (2 1/4" MIN - COLD MILL)
- (M) HMA BINDER AND SURFACE CSE (3 1/2" & VARIES)

PROPOSED LEGEND

- (1) P.C.C. PAVEMENT 9 3/4" JOINTED
- (2) STABILIZED SUBBASE (HMA) 4 1/2"
- (3) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"
- (4) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- (5) COMB CC&G TB6.12
- (6) COMB CC&G TB6.24
- (7) PCC SHOULDERS, 9 3/4"
- (8) COURSE AGGREGATE FILL
- (9) HMA SHOULDERS, 8"
- (10) CONCRETE MEDIAN, TYPE SB (SPECIAL)
- (11) CONCRETE MEDIAN SURFACE 4"
- (12) TOPSOIL 6"
- (13) AGGREGATE SHOULDER, TYPE B 6"

- (14) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS @ 30" CTS
- (15) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS @ 24" CTS
- (16) NO. 6 TIE BARS @ 24" CTS
- (17) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (18) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (19) HOT MIX-ASHPALT BASE COURSE WIDENING 6 1/4"
- (20) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (21) HOT-MIX ASPHALT BASE COURSE WIDENING 7"
- (22) AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 SHEET 3)
- (23) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8 1/4" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 6 1/4"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 8 DOWEL BAR @ 24" CTS

- (25) HMA DRIVEWAY (SEE DISTRICT DETAIL BD-01)
- (26) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19.0, N70, 7"
- (27) SODDING OR SEEDING (SEE LANDSCAPING PLANS)
- (28) TOPSOIL 18"
- (29) AGGREGATE SHOULDER, TYPE B 8"
- (30) CORRUGATED MEDIAN
- (31) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
- (32) P.C.C. BASE COURSE WIDENING 10"
- (33) AGGREGATE WEDGE SHOULDER, TYPE B
- (34) COMB CC&G TM4.06
- (35) COMB CC&G TM4.24

- (36) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (3/4" & VARIES)
- (37) TOPSOIL 24"
- (38) GRADING AND SHAPING SHOULDERS
- (39) AGGREGATE SURFACE COURSE, TYPE B SPECIAL 2"
- (40) AGGREGATE BASE COURSE, TYPE B 6"
- (41) SHOULDER RUMBLE STRIPS, 8 INCH

ATT:BD08J.DGN, GE:00817.DGN, TSC000,0C.DGN
 11-07-2013, 16:18:48
 HECHTBR T:\DOCUMENT\19349901\CIVIL\DCN\TSC000_0C.DGN

FILE NAME = #FILEL#	USER NAME = HECHTBR	DESIGNED - DRAWN - MRK	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	TYPICAL SECTIONS LAKE SHORE DRIVE & DOTY ROAD			F.A.P. RTE. 305	SECTION 27R-2	COUNTY MCHENRY	TOTAL SHEETS 673	SHEET NO. 27
exp U.S. Services Inc. BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT SCALE = #SCALE#	CHECKED - BRH	REVISED -		SCALE: N.T.S.	SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO. 62268				
	PLOT DATE = #DATE#	DATE - 11/01/13	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

Table with columns: LOCATION, STATION, SUBTOTALS, LOCATION, STATION, SUBTOTALS. Data for 20100110 TREE REMOVAL (6 TO 15 UNITS DIAMETER) TOTAL 2,364 UNIT.

Table with columns: LOCATION, STATION, SUBTOTALS, LOCATION, STATION, SUBTOTALS. Data for 20100110 TREE REMOVAL (6 TO 15 UNITS DIAMETER) TOTAL 2,364 UNIT.

Table with columns: LOCATION, STATION, SUBTOTALS, LOCATION, STATION, SUBTOTALS. Data for 20100210 TREE REMOVAL (OVER 15 UNITS DIAMETER) TOTAL 2,610 UNIT.

Vertical text on the left edge of the page.

Header information block including: FILE NAME, USER NAME, DESIGNED, REVISED, STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION, SCHEDULE OF QUANTITIES - I, F.A.P. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO., CONTRACT NO., SCALE, SHEET NO. OF SHEETS, STA. TO STA., FED. ROAD DIST. NO. 1, ILLINOIS FED. AID PROJECT.

31200502	STABILIZED SUBBASE - HOT-MIX ASPHALT, 4 1/2"			TOTAL	149,265	SQ YD
LOCATION	FROM STATION	TO STATION	AREA (SQ FT)	SUBTOTALS		
US 14	325+74	334+00	79829.70	8874.7		
US 14	334+00	348+00	110026.30	12220.5		
US 14	348+00	362+00	123415.28	13712.8		
US 14	362+00	376+00	136426.19	15158.5		
US 14	376+00	390+00	110051.73	12228.0		
US 14	390+00	404+00	133729.70	14858.9		
US 14	404+00	418+00	106624.00	11847.1		
US 14	418+00	432+50	126646.78	14071.9		
US 14	432+50	446+00	87613.65	9734.9		
US 14	446+00	460+00	104244.54	11582.7		
US 14	460+00	474+00	109240.27	12137.8		
US 14	474+00	488+00	81648.00	9072.0		
US 14	488+00	493+00	29160.00	3240.0		
LILY POND RD	801+50	801+52.24	112.24	12.5		
LAKE SHORE DR	42+85	43+00	600.81	66.8		
DOTY RD	1+50	2+50	4016.08	446.2		

35102000	AGGREGATE BASE COURSE, TYPE B 8"			TOTAL	439	SQ YD
LOCATION	STATION	ENTRANCE TYPE	LT/RT	SQ FT	SUBTOTALS	
US RTE 14	381+45.89	FIELD	RT	445.75	50	
US RTE 14	414+86.67	FIELD	RT	505.02	56	
US RTE 14	415+71.60	FIELD	RT	684.29	76	
US RTE 14	436+15.81	FIELD	RT	1103.16	123	
US RTE 14	447+77.39	FIELD	RT	575.42	64	
US RTE 14	486+95.24	FIELD	RT	635.34	71	

35102200	AGGREGATE BASE COURSE, TYPE B 10"			TOTAL	19,269	SQ YD
LOCATION	FROM STATION	TO STATION	BASE WIDTH (FT)	BASE AREA (SQ FT)	SUBTOTALS	
MULTI-USE PATH	325+75.00	334+00.00	10.67	8902.51	989.2	
MULTI-USE PATH	334+00.00	348+00.00	10.67	15129.01	1681.0	
MULTI-USE PATH	348+00.00	362+00.00	10.67	14801.45	1644.6	
MULTI-USE PATH	362+00.00	364+50.00	10.67	2687.24	298.6	
MULTI-USE PATH	364+50.00	364+80.68	9.67	289.89	32.2	
MULTI-USE PATH	364+80.68	364+97.94	8.67	149.67	16.6	
MULTI-USE PATH	366+20.46	376+00.00	10.67	10451.72	1161.3	
MULTI-USE PATH	376+00.00	382+07.53	10.67	6503.95	722.7	
MULTI-USE PATH	382+49.50	384+10.20	10.67	1706.22	189.6	
MULTI-USE PATH	384+31.46	390+00.00	10.67	5908.20	656.5	
MULTI-USE PATH	390+00.00	404+00.00	10.67	14086.91	1565.2	
MULTI-USE PATH	404+00.00	418+00.00	10.67	14727.43	1636.4	
MULTI-USE PATH	418+00.00	430+06.35	10.67	13032.12	1448.0	
MULTI-USE PATH	431+23.87	432+50.00	10.67	1379.10	153.2	
MULTI-USE PATH	432+50.00	446+00.00	10.67	13848.59	1538.7	
MULTI-USE PATH	446+00.00	460+00.00	10.67	14614.65	1623.8	
MULTI-USE PATH	460+00.00	467+04.81	10.67	7524.70	836.1	
MULTI-USE PATH	468+23.28	474+00.00	10.67	6153.50	683.7	
MULTI-USE PATH	474+00.00	488+00.00	10.67	14705.00	1633.9	
MULTI-USE PATH	488+00.00	493+00.00	10.67	5335.00	592.8	
ADA SIDEWALK RAMPS (SEE 42400300 PORTLAND CEMENT CONCRETE SIDEWALK)						
WEST LAKE SHORE DRIVE					61.8	
LAKE SHORE DRIVE					32.4	
LILY POND ROAD					36.1	
RIDGEFIELD ROAD NORTH					35.0	

35501308	HOT-MIX ASPHALT BASE COURSE, 6"			TOTAL	1,583	SQ YD
LOCATION	STATION	ENTRANCE TYPE	LT/RT	SQ FT	SUBTOTALS	
US RTE 14	374+16.87	PRIVATE	RT	1153.30	128	
US RTE 14	382+02.42	PRIVATE	LT	899.53	100	
US RTE 14	384+21.84	PRIVATE	LT	383.72	43	
US RTE 14	402+17.14	PRIVATE	LT	539.08	60	
US RTE 14	402+64.72	PRIVATE	RT	449.30	50	
US RTE 14	404+40.33	PRIVATE	LT	811.30	90	
US RTE 14	411+96.01	PRIVATE	RT	982.11	109	
US RTE 14	433+64.97	PRIVATE	LT	880.69	98	
US RTE 14	438+27.28	PRIVATE	LT	1498.72	167	
US RTE 14	448+06.50	PRIVATE	LT	1239.16	138	
US RTE 14	450+00.00	PRIVATE	LT	640.98	71	
US RTE 14	461+00.00	PRIVATE	LT	672.97	75	
US RTE 14	461+27.77	PRIVATE	RT	769.58	86	
US RTE 14	463+38.02	PRIVATE	RT	736.59	82	
US RTE 14	476+66.62	PRIVATE	RT	654.30	73	
US RTE 14	486+90.34	PRIVATE	LT	765.43	85	
LILY POND	805+28.40	PRIVATE	RT	548.50	61	
RIDGEFIELD	853+37.00	PRIVATE	LT	350.47	39	
RIDGEFIELD	854+05.21	PRIVATE	LT	274.30	30	

35501316	HOT-MIX ASPHALT BASE COURSE, 8"			TOTAL	1,186	SQ YD
LOCATION	STATION	ENTRANCE TYPE	LT/RT	SQ FT	SUBTOTALS	
US RTE 14	351+10.12	COMMERCIAL	RT	790.80	88	
US RTE 14	388+30.00	COMMERCIAL	RT	291.03	32	
US RTE 14	399+78.32	COMMERCIAL	LT	4855.74	540	
LAKE SHORE DR	44+02.27	COMMERCIAL	LT	556.98	62	
LAKE SHORE DR	46+03.66	COMMERCIAL	LT	624.49	69	
LILY POND	801+79.69	COMMERCIAL	RT	1852.23	206	
RIDGEFIELD	851+80.00	COMMERCIAL	LT	1705.51	190	

35501309	HOT-MIX ASPHALT BASE COURSE WIDENING, 6 1/4"			TOTAL	195	SQ YD
LOCATION	FROM STATION	TO STATION	LENGTH (FT)	WIDTH (FT)	AREA (SQ FT)	SUBTOTALS
LILY POND WIDEN AND RESURFACE	804+04.34	806+50.00	245.66	VARY	1512.90	168.1
LILY POND ADDITIONAL	804+04.34	806+50.00	245.66	0.979	240.54	26.7

35600704	HOT-MIX ASPHALT BASE COURSE WIDENING, 7"			TOTAL	135	SQ YD
LOCATION	FROM STATION	TO STATION	LENGTH (FT)	WIDTH (FT)	AREA (SQ FT)	SUBTOTALS
RIDGEFIELD RD	854+50.02	407+58.97	252.37	VARY	992.18	110.2
RIDGEFIELD RD ADDITIONAL (RT side)	854+50.02	407+58.97	252.37	0.521	131.44	14.6
RIDGEFIELD RD ADDITIONAL (LT side)	854+50.02	856+32.80	182.78	0.521	95.20	10.6

40600200	BITUMINOUS MATERIALS (PRIME COAT)		TOTAL	29.6	TON
APPLICATION RATE 0.0004 TONS/ SQ YD					
LOCATION	DESCRIPTION	AREA (SQ YD)	SUBTOTALS		
US RTE 14	RESURFACE CL MILLING	36,291.46	14.5		
LILY POND RD	RESURFACE	758.56	0.3		
RIDGEFIELD RD	RESURFACE	765.43	0.3		
Note: Total HMA areas, including shoulders					

40600300	AGGREGATE (PRIME COAT)		TOTAL	148	TON
APPLICATION RATE 0.002 TONS/SQ YD					
LOCATION	DESCRIPTION	AREA (SQ YD)	SUBTOTALS		
US RTE 14	RESURFACE CL MILLING	36291.46	72.6		
LILY POND RD	RESURFACE	758.56	1.5		
RIDGEFIELD RD	RESURFACE	765.43	1.5		

40600635	LEVELING BINDER (MACHINE METHOD), N70		TOTAL	126	TON
LOCATION	STATION	STATION	WIDTH (FT)	AREA (SQ YD)	SUBTOTALS
LILY POND	804+04.34	806+50.00	VARIES	758.56	94
RIDGEFIELD	854+50.00	407+58.97	VARIES	765.43	32

40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50		TOTAL	1497	TON
LOCATION	STATION	STATION	AREA (SQ YD)	THICKNESS (IN)	SUBTOTALS
US RTE 14	129+22.00	169+61.76	36291.5	36291.5	1497

T:\DOCUMENTS\1934\981\CIVIL\DRAWING\SC0800.PT.DGN
 11-07-2013 16:18:52
 HECHTBR

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	SCHEDULE OF QUANTITIES - II			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FILEL		DRAWN - BRH	REVISED -					305	27R-2	MCHENRY	673	29
exp U.S. Services Inc. Chicago, IL	PLOT SCALE = *SCALE*	CHECKED - TMH	REVISED -					CONTRACT NO. 62268				
BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT DATE = *DATE*	DATE - 11/01/13	REVISED -					SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA.	TO STA.

LOCATION	STATION	DIRECTION	WIDTH (FT)	AREA (SQ YD)	SUBTOTALS
US RTE 14	129+22.00	EB	4.50	108.00	12
US RTE 14	169+61.76	EB	4.50	216.00	24
US RTE 14	129+67.00	WB	4.50	108.00	12
US RTE 14	169+61.76	WB	4.50	207.00	23
W LSD	201+15.00		4.50	220.50	25
FRONTAGE RD	102+30.00		4.50	90.00	10
LILY POND	806+50.00	EB	4.50	94.50	11
RIDGEFIELD	407+58.97	EB	4.50	105.75	12
LAKE AVE	NORTH END		4.50	391.50	44
LAKE AVE	SOUTH END		4.50	301.50	34

LOCATION	FROM STATION	TO STATION	THICKNESS (IN)	WIDTH (FT)	AREA (SQ YD)	SUBTOTALS
MULTI-USE PATH	325+75.00	334+00.00	2.5	10	927.06	129.8
MULTI-USE PATH	334+00.00	348+00.00	2.5	10	1575.45	220.6
MULTI-USE PATH	348+00.00	362+00.00	2.5	10	1541.34	215.8
MULTI-USE PATH	362+00.00	364+50.00	2.5	10	279.83	39.2
MULTI-USE PATH	364+50.00	364+80.68	2.5	9	29.98	4.2
MULTI-USE PATH	364+80.68	364+97.94	2.5	8	15.34	2.1
MULTI-USE PATH	366+20.46	376+00.00	2.5	10	1088.38	152.4
MULTI-USE PATH	376+00.00	382+07.53	2.5	10	677.28	94.8
MULTI-USE PATH	382+07.53	384+10.20	2.5	10	177.68	24.9
MULTI-USE PATH	384+10.20	390+00.00	2.5	10	615.25	86.1
MULTI-USE PATH	390+00.00	404+00.00	2.5	10	1466.93	205.4
MULTI-USE PATH	404+00.00	418+00.00	2.5	10	1533.63	214.7
MULTI-USE PATH	418+00.00	430+06.35	2.5	10	1357.09	190.0
MULTI-USE PATH	431+23.87	432+50.00	2.5	10	143.61	20.1
MULTI-USE PATH	432+50.00	446+00.00	2.5	10	1442.11	201.9
MULTI-USE PATH	446+00.00	460+00.00	2.5	10	1521.88	213.1
MULTI-USE PATH	460+00.00	467+04.69	2.5	10	783.58	109.7
MULTI-USE PATH	468+23.28	474+00.00	2.5	10	640.79	89.7
MULTI-USE PATH	474+00.00	488+00.00	2.5	10	1531.29	214.4
MULTI-USE PATH	488+00.00	493+00.00	2.5	10	555.56	77.8

LOCATION	FROM STATION	TO STATION	THICKNESS (IN)	WIDTH (FT)	AREA (SQ YD)	SUBTOTALS
RIDGEFIELD	854+50.00	407+58.97	2.0	VARY	765.43	85.73
LILY POND	804+04.34	806+50.00	2.0	VARY	758.56	84.96
WB US RTE 14	129+67.00	136+00.00	1.5	VARY	2,179.63	183.09
EB US RTE 14	129+22.00	136+00.00	1.5	VARY	2,334.58	196.10
WB US RTE 14	136+00.00	150+00.00	1.5	VARY	4,839.11	406.49
EB US RTE 14	136+00.00	150+00.00	1.5	VARY	4,860.09	408.25
US RTE 14	150+00.00	164+00.00	1.5	VARY	15,750.07	1,323.01
US RTE 14	164+00.00	169+61.76	1.5	VARY	6,327.96	531.55

LOCATION	FROM STATION	TO STATION	THICKNESS (IN)	WIDTH (FT)	AREA (SQ YD)	SUBTOTALS
MULTI-USE PATH	325+75.00	334+00.00	1.5	10	927.06	77.9
MULTI-USE PATH	334+00.00	348+00.00	1.5	10	1575.45	132.3
MULTI-USE PATH	348+00.00	362+00.00	1.5	10	1541.34	129.5
MULTI-USE PATH	362+00.00	364+50.00	1.5	10	279.83	23.5
MULTI-USE PATH	364+50.00	364+80.68	1.5	9	29.98	2.5
MULTI-USE PATH	364+80.68	364+97.94	1.5	8	15.34	1.3
MULTI-USE PATH	366+20.46	376+00.00	1.5	10	1088.38	91.4
MULTI-USE PATH	376+00.00	382+07.53	1.5	10	677.28	56.9
MULTI-USE PATH	382+07.53	384+10.20	1.5	10	177.68	14.9
MULTI-USE PATH	384+10.20	390+00.00	1.5	10	615.25	51.7
MULTI-USE PATH	390+00.00	404+00.00	1.5	10	1466.93	123.2
MULTI-USE PATH	404+00.00	418+00.00	1.5	10	1533.63	128.8
MULTI-USE PATH	418+00.00	430+06.35	1.5	10	1357.09	114.0
MULTI-USE PATH	431+23.87	432+50.00	1.5	10	143.61	12.1
MULTI-USE PATH	432+50.00	446+00.00	1.5	10	1442.11	121.1
MULTI-USE PATH	446+00.00	460+00.00	1.5	10	1521.88	127.8
MULTI-USE PATH	460+00.00	467+04.81	1.5	10	783.58	65.8
MULTI-USE PATH	468+23.28	474+00.00	1.5	10	640.79	53.8
MULTI-USE PATH	474+00.00	488+00.00	1.5	10	1531.29	128.6
MULTI-USE PATH	488+00.00	493+00.00	1.5	10	555.56	46.7

LOCATION	FROM STATION	TO STATION	THICKNESS (IN)	WIDTH (FT)	AREA (SQ YD)	SUBTOTALS
US RTE 14	331+00	345+00	2	4	622.22	69.7
US RTE 14	378+00	385+00	2	4	311.11	34.8
US RTE 14	390+00	400+00	2	4	444.44	49.8
US RTE 14	430+00	431+55	2	4	68.89	7.7
US RTE 14	440+00	442+50	2	4	111.11	12.4
US RTE 14	488+50	491+00	2	4	111.11	12.4

LOCATION	FROM STATION	TO STATION	THICKNESS (IN)	WIDTH (FT)	AREA (SQ YD)	SUBTOTALS
US RTE 14	351+10.12	RT	2		87.87	9.8
US RTE 14	374+16.87	RT	2		128.14	14.4
US RTE 14	381+45.89	RT	2		49.53	5.5
US RTE 14	382+02.42	LT	2		99.95	11.2
US RTE 14	384+21.84	LT	2		42.64	4.8
US RTE 14	388+30.00	RT	2		32.34	3.6
US RTE 14	399+78.32	LT	2		539.53	60.4
US RTE 14	402+17.14	LT	2		59.90	6.7
US RTE 14	402+64.72	RT	2		49.92	5.6
US RTE 14	404+40.33	LT	2		90.14	10.1
US RTE 14	411+96.01	RT	2		109.12	12.2
US RTE 14	414+86.67	RT	2		56.11	6.9
US RTE 14	415+71.60	RT	2		56.11	6.3
US RTE 14	433+64.97	LT	2		97.85	11.0
US RTE 14	436+15.81	RT	2		122.57	13.7
US RTE 14	438+27.28	LT	2		166.52	18.7
US RTE 14	447+77.39	RT	2		63.94	7.2
US RTE 14	448+06.50	LT	2		137.68	15.4
US RTE 15	450+00.00	LT	2		71.22	8.0
US RTE 13	461+00.00	LT	2		74.77	8.4
US RTE 14	461+27.77	RT	2		85.51	9.6
US RTE 14	463+38.02	RT	2		81.84	9.2
US RTE 14	476+66.62	RT	2		72.70	8.1
US RTE 14	486+90.34	LT	2		85.05	9.5
US RTE 14	486+95.24	RT	2		70.59	7.9

LOCATION	FROM STATION	TO STATION	THICKNESS (IN)	WIDTH (FT)	AREA (SQ YD)	SUBTOTALS
LAKE SHORE DR	44+02.27	LT	2		61.89	6.9
LAKE SHORE DR	46+03.66	LT	2		69.39	7.8
LILY POND RD	801+79.69	RT	2		205.80	23.0
LILY POND RD	805+28.40	RT	2		60.94	6.8
RIDGEFIELD	851+80.00	LT	2		189.50	21.2
RIDGEFIELD	853+37.00	LT	2		38.94	4.4
RIDGEFIELD	854+05.21	LT	2		30.48	3.4

LOCATION	FROM STATION	TO STATION	LENGTH (FT)	WIDTH (FT)	AREA (SQ FT)	SUBTOTALS
LILY POND	801+52.24	804+04.04	251.8	VARIES	9054.73	1006.08

LOCATION	FROM STATION	TO STATION	LENGTH (FT)	WIDTH (FT)	AREA (SQ FT)	SUBTOTALS
RIDGEFIELD RD	851+07.00	851+50.00	43.00	VARIES	1667.13	185.24
RIDGEFIELD RD	851+50.00	854+50.02	300.02	VARIES	10731.99	1192.44

LOCATION	FROM STATION	TO STATION	AREA (SQ YD)	SUBTOTALS
US 14 LT	325+74	334+00	31716.4	3524.0
US 14 RT			23952.19	2661.4
US 14 LT	334+00	348+00	33743.70	3749.3
US 14 RT			34354.90	3817.2
US 14 LT	348+00	362+00	39307.85	4367.5
US 14 RT			40853.02	4539.2
US 14 LT	362+00	376+00	51722.33	5746.9
US 14 RT			52981.02	5886.8
US 14 LT	376+00	390+00	33326.70	3703.0
US 14 RT			38769.80	4307.8
US 14 LT	390+00	404+00	46080.80	5120.1
US 14 RT			53123.00	5902.6
US 14 LT	404+00	418+00	33600.00	3733.3
US 14 RT			33600.00	3733.3
US 14 LT	418+00	432+50	47059.24	5228.8
US 14 RT			43191.78	4799.1
US 14 LT	432+50	446+00	39017.80	4335.3
US 14 RT			33359.70	3706.6
US 14 LT	446+00	460+00	42903.15	4767.0
US 14 RT			42903.15	4767.0
US 14 LT	460+00	474+00	47213.25	5245.9
US 14 RT			43564.21	4840.5
US 14 LT	474+00	488+00	33600.00	3733.3
US 14 RT			33600.00	3733.3
US 14 LT	488+00	493+00	12000.00	1333.3
US 14 RT			12000.00	1333.3
LAKE SHORE DR	42+80	43+00	553.40	61.5
DOTY RD	1+50	2+50	3700.08	411.1
LILY POND	801+50	801+52.24	80.70	9.0

CODE	ITEM	SUBTOTALS
42000416	PCC PAVEMENT, 9 3/4" (JOINTED)	109,098
42400300	PCC SIDEWALK 6 INCH	195
48300415	PCC SHOULDERS 9 3/4"	19,911
60600605	CONCRETE CURB, TYPE B	32
60600095	CLASS SI CONCRETE (OUTLET)	20
60603800	COMB CC&G TB6.12	564
60605000	COMB CC&G TB6.24	16,094
60615400	PAVED DITCH, TYPE A-15	99
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	2,662
60621600	CONCRETE MEDIAN, TPYE SM	17
60624600	CORRUGATED MEDIAN	100
X6061100	CONCRETE MEDIAN, TYPE SB (SPECIAL)	1,933
X6063600	COMB CC&G TM4.24	57
X6064201	COMB CC&G TM4.06	17

T:\DOCUMENTS\1934\9801\CIVIL\DCM\SC000_PU.DGN
 11-07-2013 16:18:54
 FILE NAME =
 USER NAME = HECHTBR
 DESIGNED -
 REVISED -
 DRAWN - BRH
 REVISED -
 CHECKED - TMH
 REVISED -
 DATE - 11/01/13
 REVISED -
 PLOT SCALE = *SCALE*
 PLOT DATE = *DATE*
 exp U.S. Services Inc.
 CHICAGO, IL
 BUILDINGS-EARTH & ENVIRONMENT-ENERGY
 INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

SCHEDULE OF QUANTITIES - III

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	30
				CONTRACT NO. 62268
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

42400300	PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH	TOTAL	1,751.5 SQ FT
INTERSECTION SUBTOTALS			
WEST LAKE SHORE DRIVE		556.6	
LAKE SHORE DRIVE		554.4	
LILY POND ROAD		325.3	
RIDGEFIELD ROAD NORTH		315.4	

42400800	DETECTABLE WARNINGS	TOTAL	296 SQ FT
INTERSECTION SUBTOTALS			
WEST LAKE SHORE DRIVE		115.4	
LAKE SHORE DRIVE		54.9	
LILY POND ROAD		63.3	
RIDGEFIELD ROAD NORTH		61.9	

44000100	PAVEMENT REMOVAL	TOTAL	104,015 SQ YD	
LOCATION	FROM STATION	TO STATION	AREA (SQ FT)	SUBTOTALS
US RTE 14	325+74	334+00	57311.30	6368
US RTE 14	334+00	348+00	41563.39	4618
US RTE 14	348+00	362+00	38222.41	4247
US RTE 14	362+00	376+00	75440.12	8382
US RTE 14	376+00	390+00	36189.53	4021
US RTE 14	390+00	404+00	60490.89	6721
US RTE 14	404+00	418+00	35341.03	3927
US RTE 14	418+00	432+50	43493.17	4833
US RTE 14	432+50	446+00	37265.96	4141
US RTE 14	446+00	460+00	33879.82	3764
US RTE 14	460+00	474+00	42007.89	4668
US RTE 14	474+00	488+00	34049.57	3783
US RTE 14	488+00	493+00	13099.04	1455
LILY POND RD	801+50	804+04.34	5396.88	600
RIDGEFIELD RD	851+50	854+50.02	7277.26	809
TEMPORARY PAVEMENT PLACED DURING THE MOT STAGING				39739
TEMPORARY PAVEMENT (VARIABLE DEPTH) MOT STAGING				1939

44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	TOTAL	1669 SQ YD		
LOCATION	FROM STATION	TO STATION	WIDTH (FT)	AREA (SQ FT)	SUBTOTALS
US RTE 14	331+00	345+00	4	5600	622
US RTE 14	378+00	385+00	4	2800	311
US RTE 14	390+00	400+00	4	4000	444
US RTE 14	430+00	431+55	4	620	69
US RTE 14	440+00	442+50	4	1000	111
US RTE 14	488+50	491+00	4	1000	111
MAINTENANCE OF TRAFFIC - CENTERLINE MILLING - PER ENGINEERS DIRECTION					

44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	TOTAL	36291 SQ YD
LOCATION	FROM STATION	TO STATION	SUBTOTALS
US RTE 14	129+22.00	169+61.76	36,291

44000500	COMBINATION CURB AND GUTTER REMOVAL	TOTAL	6,270 FOOT	
LOCATION	FROM STATION	TO STATION	OFFSET	SUBTOTALS
US RTE 14	168+16	169+59	LT	95
US RTE 14	325+48	330+21	LT	490
US RTE 14	325+19	325+52	LT	138
US RTE 14	325+23	326+70	LT	285
US RTE 14	326+16	331+65	RT	1,089
LAKE SHORE DR	43+00	44+92	LT	240
LAKE SHORE DR	43+00	44+92	RT	241
US RTE 14	364+68	365+32	LT	84
US RTE 14	365+88	366+37	LT	68
US RTE 14	388+06	388+53	RT	99
DOTY RD	0+06	1+50	RT	182
DOTY RD	0+23	1+50	LT	152
US RTE 14	399+16	399+73	LT	64
US RTE 14	400+01	401+15	LT	121
US RTE 14	414+86	418+00	LT	315
US RTE 14	415+00	418+00	RT	300
US RTE 14	418+00	419+60	LT	163
US RTE 14	418+00	418+70	RT	71
US RTE 14	431+20	300+42	LT	81
US RTE 14	300+86	432+50	LT	121
US RTE 14	432+50	435+31	LT	286
LAKE SHORE DR	42+23	43+00	RT	76
DOTY RD	1+50	1+67	RT	17
DOTY RD	1+50	3+89	LT	239
TEMPORARY TRANSITION C&G (5') PLACED IN STAGING (20 LOCATIONS)				100
LAKE SHORE DRIVE	44+29	44+67	LT	58
LAKE SHORE DRIVE	43+00	44+61	RT	184
LAKE SHORE DRIVE	45+67	47+25	RT	168
DOTY RD	00+32	01+50	LT	152
LAKE SHORE DRIVE	42+24	43+00	RT	77
LAKE SHORE DRIVE	47+25	47+97	RT	80
DOTY RD	01+50	03+89	LT	240
RIDGEFIELD RD	432+25	434+20	LT	196

44000300	CURB REMOVAL	TOTAL	94 FOOT	
LOCATION	FROM STATION	TO STATION	OFFSET	SUBTOTALS
US RTE 14	350+98	351+23	RT	42
LAKE SHORE DR	43+79	44+25	LT	52

44003100	MEDIAN REMOVAL	TOTAL	141 FOOT
LOCATION	FROM STATION	TO STATION	SUBTOTALS
US RTE 14	325+75	326+16	140.92

44004000	PAVED DITCH REMOVAL	TOTAL	133 FOOT	
LOCATION	FROM STATION	TO STATION	OFFSET	SUBTOTALS
US RTE 14	336+73	336+82	RT	133

44004250	PAVED SHOULDER REMOVAL	TOTAL	13,588 SQ YD		
LOCATION	FROM STATION	TO STATION	OFFSET	AREA (SQ FT)	SUBTOTALS
US RTE 14	165+76	168+46	RT	871.91	96.9
US RTE 14	330+21	334+00	LT	1272.28	141.4
US RTE 14	334+00	348+00	LT	4259.23	473.2
US RTE 14	325+22	334+00	RT	6994.40	777.2
US RTE 14	334+00	348+00	RT	7739.75	860.0
US RTE 14	348+00	362+00	LT	4899.88	544.4
US RTE 14	348+00	362+00	RT	5014.60	557.2
US RTE 14	362+00	364+27	LT	932.96	103.7
US RTE 14	362+00	364+70	RT	986.11	109.6
US RTE 14	366+37	376+00	LT	3381.39	375.7
US RTE 14	366+48	373+72	RT	2423.94	269.3
US RTE 14	373+72	376+00	RT	568.46	63.2
US RTE 14	376+00	390+00	LT	6197.94	688.7
US RTE 14	376+00	390+00	RT	6667.50	740.8
US RTE 14	390+00	399+60	LT	3293.98	366.0
US RTE 14	390+00	396+61	RT	2477.72	275.3
US RTE 14	400+10	402+05	LT	824.99	91.7
US RTE 14	397+99	402+50	RT	1527.78	169.8
US RTE 14	402+31	404+00	LT	602.01	66.9
US RTE 14	402+75	404+00	RT	448.70	49.9
US RTE 14	404+00	404+25	LT	75.72	8.4
US RTE 14	404+00	411+75	RT	2584.08	287.1
US RTE 14	404+57	418+00	LT	5402.28	600.3
US RTE 14	412+13	418+00	RT	2607.14	289.7
US RTE 14	418+00	431+20	LT	4558.41	506.5
US RTE 14	418+00	432+50	RT	5375.29	597.3
US RTE 14	435+40	446+00	LT	3549.64	394.4
US RTE 14	432+50	446+00	RT	4324.36	480.5
US RTE 14	446+00	460+00	LT	5028.74	558.7
US RTE 14	446+00	460+00	RT	4670.88	519.0
US RTE 14	460+00	465+23	LT	1744.07	193.8
US RTE 14	460+00	460+96	RT	330.19	36.7
US RTE 14	461+40	474+00	RT	4202.36	466.9
US RTE 14	466+98	474+00	LT	2427.11	269.7
US RTE 14	474+00	486+86	LT	4533.45	503.7
US RTE 14	474+00	476+39	RT	793.40	88.2
US RTE 14	476+93	488+00	RT	3723.77	413.8
US RTE 14	487+29	488+00	LT	228.78	25.4
US RTE 14	488+00	493+00	LT	1719.64	191.1
US RTE 14	488+00	493+00	RT	1696.77	188.5

ADDITIONAL PAVED SHOULDER CONSTRUCTED FOR MOT STAGE 2.1 SUBSTAGE A-3
LILY POND RD 802+27 804+91 LT 859.20 95.5

ADDITIONAL PAVED SHOULDER CONSTRUCTED FOR MOT STAGE 2.1 SUBSTAGE B-3
RIDGEFIELD RD 852+16 855+02 RT 468.70 52.1

44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	TOTAL	1,494 FOOT	
LOCATION	FROM STATION	TO STATION	LANES/EOP	SUBTOTALS
LILY POND RD	804+04.34	806+50.00	3	737.0
RIDGEFIELD RD NORTH	854+50.02	856+43.38	3	580.1
RIDGEFIELD RD NORTH	407+00.00	407+59.00	3	177.0

T:\DOCUMENTS\1934\9901\CIVIL\DCN\SC0800.PV.DGN
11-07-2013 16:18:56
exp U.S. Services Inc.
BUILDING-EARTH & ENVIRONMENT-ENERGY
INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY

48101500 AGGREGATE SHOULDERS, TYPE B 6" TOTAL 2,563 SQ YD						
LOCATION	FROM STATION	TO STATION	OFFSET	WIDTH (FT)	AREA (SQ FT)	SUBTOTALS
US RTE 14	325+20.00	334+00.00	RT	2.00	1747.00	194.1
US RTE 14	331+06.17	334+00.00	LT	2.00	594.00	66.0
US RTE 14	334+00.00	334+50.00	RT	2.00	98.80	11.0
US RTE 14	341+50.00	348+00.00	RT	2.00	1291.60	143.5
US RTE 14	334+00.00	348+00.00	LT	2.00	2823.60	313.7
US RTE 14	348+00.00	350+86.36	RT	2.00	578.80	64.3
US RTE 14	351+31.93	357+00.00	RT	2.00	1149.00	127.7
US RTE 14	348+00.00	361+39.85	LT	2.00	2663.20	295.9
US RTE 14	374+24.00	376+00.00	RT	2.00	352.00	39.1
US RTE 14	376+00.00	381+38.00	RT	2.00	1078.80	119.9
US RTE 14	381+56.00	383+61.40	RT	2.00	417.60	46.4
US RTE 14	402+86.00	404+00.00	RT	2.00	228.00	25.3
US RTE 14	402+43.00	404+00.00	LT	2.00	312.80	34.8
US RTE 14	404+00.00	411+78.86	RT	2.00	1557.80	173.1
US RTE 14	412+07.17	414+76.75	RT	2.00	539.20	59.9
US RTE 14	414+96.76	415+61.60	RT	2.00	129.60	14.4
US RTE 14	415+81.60	418+00.00	RT	2.00	436.80	48.5
US RTE 14	404+00.00	404+25.50	LT	2.00	51.00	5.7
US RTE 14	404+53.05	418+00.00	LT	2.00	2693.80	299.3
US RTE 14	418+00.00	424+88.52	RT	2.00	1377.00	153.0
US RTE 14	418+00.00	428+90.41	LT	2.00	2196.20	244.0
LAKE SHORE	47+29.69	48+58.91	RT	4.00	551.16	61.2
LAKE SHORE	47+32.10	47+86.47	LT	4.00	198.87	22.1

48101600 AGGREGATE SHOULDERS, TYPE B 8" TOTAL 449 SQ YD						
LOCATION	FROM STATION	TO STATION	OFFSET	WIDTH (FT)	AREA (SQ FT)	SUBTOTALS
LILY POND RD	805+55.38	806+50.00	LT	4	291.30	32.4
LILY POND RD	802+51.72	805+11.96	RT	4	1070.31	118.9
LILY POND RD	805+45.90	806+50.00	RT	4	391.01	43.4
RIDGEFIELD RD	852+14.78	853+21.71	LT	4	442.80	49.2
RIDGEFIELD RD	853+50.67	853+90.02	LT	4	166.40	18.5
RIDGEFIELD RD	854+31.04	407+59.00	LT	4	1048.70	116.5
RIDGEFIELD RD	855+45.27	407+59.00	RT	4	631.77	70.2

48102100 AGGREGATE WEDGE SHOULDER, TYPE B TOTAL 236 TON						
LOCATION	FROM STATION	TO STATION		WIDTH (FT)	AREA (SQ FT)	SUBTOTALS
WB US RTE 14	129+67.00	152+43.73		5'	11405.64	72.17
WB US RTE 14	155+59.27	167+26.57		5'	5987.12	37.9
EB US RTE 14	129+22.00	153+44.28		5'	11939.23	75.5
EB US RTE 14	156+18.05	168+63.94		5'	7899.34	50.0

48203029 HOT-MIX ASPHALT SHOULDERS, 8" TOTAL 1,341 SQ YD						
LOCATION	FROM STATION	TO STATION	OFFSET	AREA (SQ FT)		SUBTOTALS
RIDGEFIELD RD	851+50	407+59	LT	2199.10		244.3
RIDGEFIELD RD	851+50	407+59	RT	3333.80		370.4
RIDGEFIELD RD	850+83	851+50	LT	499.20		55.5
RIDGEFIELD RD	851+24	851+50	RT	149.89		16.7
LILY POND RD	801+53	806+50	LT	3404.40		378.3
LILY POND RD	802+43	806+50	RT	1151.61		128.0
ADDITIONAL PAVED SHOULDER CONSTRUCTED FOR MOT STAGE 2.1 SUBSTAGE A-3						
LILY POND RD	802+27	804+91	LT	859.20		95.5
ADDITIONAL PAVED SHOULDER CONSTRUCTED FOR MOT STAGE 2.1 SUBSTAGE B-3						
RIDGEFIELD RD	852+16	855+02	RT	468.70		52.1

48300415 PORTLAND CEMENT CONCRETE SHOULDERS 9 3/4" TOTAL 19,911 SQ YD						
LOCATION	FROM STATION	TO STATION	OFFSET	WIDTH (FT)	AREA (SQ FT)	SUBTOTALS
US RTE 14	325+74.26	331+06.16	RT	10	5,288.00	588
US RTE 14	325+74.26	329+61.57	LT	vor	3,941.14	438
US RTE 14	331+06.06	334+00.00	LT/RT	20	5,875.60	653
US RTE 14	334+00.00	348+00.00	LT/RT	20	27,994.80	3,111
US RTE 14	348+00.00	349+15.72	LT/RT	20	2,314.40	257
US RTE 14	349+15.72	349+76.25	LT	11	664.10	74
US RTE 14	349+15.72	350+86.36	RT	10	1,720.30	191
US RTE 14	350+86.36	351+31.93	RT	11	503.90	56
US RTE 14	351+31.93	352+46.12	RT	12	1,381.20	153
US RTE 14	349+76.25	351+39.01	LT	12	1,936.80	215
US RTE 14	351+39.01	351+47.08	LT	11	88.77	10
US RTE 14	352+46.12	353+05.64	RT	11	656.40	73
US RTE 14	351+47.08	353+05.64	LT	10	1,572.60	175
US RTE 14	353+05.64	360+60.74	LT/RT	20	15,077.40	1,675
US RTE 14	360+60.74	361+39.85	LT	10	803.50	89
US RTE 14	361+39.85	362+00.00	LT	8.355	502.60	56
US RTE 14	362+00.00	363+04.85	LT	3.86	404.70	45
US RTE 14	365+93.64	367+10.31	RT	12	1,285.10	143
US RTE 14	367+10.31	367+70.31	RT	11	660.00	73
US RTE 14	367+70.31	370+35.59	RT	10	2,652.80	295
US RTE 14	368+66.87	370+35.59	LT	5.5	927.96	103
US RTE 14	370+35.59	376+00.00	LT/RT	20	11,288.20	1,254
US RTE 14	376+00.00	383+86.47	LT/RT	20	15,729.40	1,748
US RTE 14	383+86.47	385+66.06	RT	5.5	958.40	106
US RTE 14	383+86.47	388+42.00	LT	10	4,458.00	495
US RTE 14	388+42.00	390+00.00	LT/RT	20	3,160.00	351
US RTE 14	390+00.00	392+34.70	LT/RT	20	4,694.00	522
US RTE 14	392+34.70	395+13.81	LT	10	2,773.90	308
US RTE 14	395+13.81	395+73.80	LT	11	659.89	73
US RTE 14	395+73.80	396+73.81	LT	12	1,200.12	133
US RTE 14	397+80.27	398+95.33	RT	12	1,274.31	142
US RTE 14	398+95.33	399+55.33	RT	11	660.00	73
US RTE 14	399+55.33	402+43.58	RT	10	2,882.50	320
US RTE 14	402+43.58	404+00.00	LT/RT	20	3,128.40	348
US RTE 14	404+00.00	418+00.00	LT/RT	20	28,000.00	3,111
US RTE 14	418+00.00	424+88.52	LT/RT	20	13,770.40	1,530
US RTE 14	424+88.52	426+88.33	RT	6	1,435.60	160
US RTE 14	424+88.52	428+30.41	LT	10	3,475.30	386
US RTE 14	428+30.41	428+90.41	LT	11	663.25	74
US RTE 14	428+90.41	429+95.12	LT	12	1,278.10	142
US RTE 14	429+95.12	430+43.42	LT	11	1,431.47	159
LILY POND	801+50.00	801+52.24	LT	10	22.40	2

60600605 CONCRETE CURB, TYPE B TOTAL 194.5 FOOT			
LOCATION	STATION		SUBTOTALS
LAKE SHORE DR	44+00		42.9
US 14	399+75		125.6
US 14	351+10		26.17

60603800 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 TOTAL 2,536.0 FOOT				
LOCATION	FROM STATION	TO STATION	OFFSET	SUBTOTALS
EX US RTE 14	168+16.53	168+46.50	LT	36.6
LAKE SHORE DR	45+58.78	47+12.73	LT	165.8
LAKE SHORE DR	45+70.58	47+12.73	RT	166.8
LAKE SHORE DR	42+85.00	43+00.00	LT	15.0
LAKE SHORE DR	43+00.00	44+67.61	LT	179.0
LAKE SHORE DR	42+23.71	43+00.00	RT	76.3
LAKE SHORE DR	43+00.00	44+74.05	RT	174.8
DOTY RD	0+58.15	1+50.00	LT	96.8
DOTY RD	1+50.00	3+89.19	LT	239.2
DOTY RD	0+37.11	1+50.00	RT	128.9
DOTY RD	1+50.00	2+50.00	RT	100.0
PLACED IN PRE-STAGE				
LAKE SHORE DRIVE	44+29	44+67	LT	57.7
LAKE SHORE DRIVE	43+00	44+61	RT	184.3
LAKE SHORE DRIVE	45+67	47+25	RT	167.5
DOTY RD	00+32	01+50	LT	151.7
LAKE SHORE DRIVE	42+24	43+00	RT	76.6
LAKE SHORE DRIVE	47+25	47+97	RT	80.0
DOTY RD	01+50	03+89	LT	243.3
PLACED IN STAGE 2.1B				
RIDGEFIELD RD	432+25	434+20	LT	195.8

T:\DOCUMENTS\1934\9901\CIVIL\DCN\SC0800.PW.DGN
 11-07-2013 16:18:58
 exp U.S. Services Inc.
 CHICAGO, IL
 BUILDINGS-EARTH & ENVIRONMENT-ENERGY
 INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES - V				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN - BRH	REVISED -						305	27R-2	MCHENRY	673	32
PLOT SCALE = *SCALE*		CHECKED - TMH	REVISED -						CONTRACT NO. 62268				
PLOT DATE = *DATE*		DATE - 11/01/13	REVISED -						SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA.	TO STA.



60605000 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 TOTAL 48,281.5 FOOT				
LOCATION	FROM STATION	TO STATION	OFFSET	SUBTOTALS
US RTE 14	325+44.60	331+06.18	LT	574.8
US RTE 14	328+07.98	334+00.00	LT/RT	593.9
US RTE 14	328+80.44	334+00.00	RT	517.9
US RTE 14	334+00.00	348+00.00	LT	1402.4
US RTE 14	334+00.00	348+00.00	RT	1397.6
US RTE 14	334+50.00	341+50.00	RT	692.3
US RTE 14	348+00.00	348+61.19	LT	61.2
US RTE 14	348+00.00	348+61.19	LT/RT	61.4
US RTE 14	353+63.24	362+00.00	LT/RT	837.2
US RTE 14	353+63.24	362+00.00	RT	837.9
US RTE 14	357+00.00	362+00.00	RT	500.7
US RTE 14	361+39.85	362+00.00	LT	60.1
US RTE 14	362+00.00	365+05.58	LT	293.7
US RTE 14	362+00.00	362+80.74	LT	80.7
US RTE 14	362+00.00	362+80.74	LT/RT	81
US RTE 14	362+00.00	365+01.88	RT	303.9
US RTE 14	365+93.12	374+24.00	RT	830.9
US RTE 14	366+26.01	376+00.00	LT	974.05
US RTE 14	368+25.60	376+00.00	LT/RT	775
US RTE 14	368+25.60	376+00.00	RT	774.4
US RTE 14	376+00.00	390+00.00	LT	1376.9
US RTE 14	376+00.00	390+00.00	LT	1395.6
US RTE 14	376+00.00	390+00.00	RT	1404.4
US RTE 14	383+61.40	388+36.62	RT	502.7
US RTE 14	388+51.47	390+00.00	RT	197.1
US RTE 14	390+00.00	402+43.58	LT	1235.8
US RTE 14	390+00.00	393+58.81	LT	357.4
US RTE 14	390+00.00	393+58.81	LT/RT	360.7
US RTE 14	390+00.00	396+78.35	RT	686.5
US RTE 14	397+80.08	402+86.00	RT	506
US RTE 14	400+10.33	404+00.00	LT/RT	390.3
US RTE 14	400+10.33	404+00.00	RT	389.7
US RTE 14	404+00.00	418+00.00	LT	1400
US RTE 14	404+00.00	418+00.00	RT	1400
US RTE 14	418+00.00	427+06.45	LT	907.3
US RTE 14	418+00.00	427+06.45	RT	907.2
US RTE 14	424+88.52	432+50.00	RT	749.8
US RTE 14	428+90.41	430+31.93	LT	178.6
US RTE 14	430+79.72	432+50.00	LT	244.3
US RTE 14	432+50.00	446+00.00	LT	1369.6
US RTE 14	433+12.90	446+00.00	LT/RT	1191.1
US RTE 14	432+12.90	446+00.00	RT	1183.4
US RTE 14	432+50.00	446+00.00	RT	1331.9
US RTE 14	446+00.00	460+00.00	LT	1401.8
US RTE 14	446+00.00	448+85.00	LT	285
US RTE 14	446+00.00	448+85.00	LT/RT	285.4
US RTE 14	446+00.00	460+00.00	RT	1401.8
US RTE 14	453+80.00	460+00.00	LT/RT	620.6
US RTE 14	453+80.00	460+00.00	RT	620
US RTE 14	460+00.00	467+43.73	LT	754.7
US RTE 14	460+00.00	465+06.31	LT	506.3
US RTE 14	460+00.00	465+06.31	LT/RT	506.7
US RTE 14	460+00.00	474+00.00	RT	1401.7
US RTE 14	467+88.47	474+00.00	LT	665.6
US RTE 14	470+28.38	474+00.00	LT/RT	372.2
US RTE 14	470+28.38	474+00.00	RT	371.6
US RTE 14	474+00.00	488+00.00	LT	1400
US RTE 14	474+00.00	488+00.00	LT	1400
US RTE 14	474+00.00	488+00.00	RT	1400
US RTE 14	488+00.00	493+00.00	LT	500
US RTE 14	488+00.00	493+00.00	LT	500
US RTE 14	488+00.00	493+00.00	RT	500
US RTE 14	488+00.00	493+00.00	RT	500
LILY POND	801+56.43	802+19.46	RT	70.52
TEMPORARY CURB RAMP TRANSITIONS DURING STAGING (20 LOCATIONS)				100

60618300 CONCRETE MEDIAN SURFACE, 4 INCH TOTAL 23,956 SQ FT				
LOCATION	FROM STATION	TO STATION	OFFSET	SUBTOTALS
US RTE 14	325+19.47	325+47.75		347.0
US RTE 14	325+26.85	326+58.78		918.3
US RTE 14	328+07.98	329+67.19		1592.2
US RTE 14	346+41.19	348+00.00		1756.1
US RTE 14	348+00.00	348+61.19		186.6
US RTE 14	353+63.24	355+83.26		2128.9
US RTE 14	360+60.74	362+00.00		1639.9
US RTE 14	362+00.00	362+80.74		304.9
US RTE 14	368+25.60	370+45.60		1944.8
US RTE 14	392+09.94	393+58.81		1195.3
US RTE 14	400+10.33	402+30.42		1944.9
US RTE 14	425+91.30	427+06.45		1071.6
US RTE 14	434+12.90	435+42.90		1145.5
US RTE 14	446+65.00	448+85.00		1944.9
US RTE 14	453+80.00	456+00.00		1944.9
US RTE 14	462+86.31	465+06.31		1944.9
US RTE 14	470+28.38	472+48.38		1944.8

60622320 CONCRETE MEDIAN, TYPE SM-4.24 TOTAL 149 SQ FT				
LOCATION	FROM STATION	TO STATION	OFFSET	SUBTOTALS
US RTE 14	326+56.46	326+64.78	LT	75.5
US RTE 14	327+95.00	328+07.98	RT	73.6

60624600 CORRUGATED MEDIAN TOTAL 898 SQ FT				
LOCATION	FROM STATION	TO STATION	OFFSET	SUBTOTALS
US RTE 14	325+74.69	327+95.00		897.8

63200310 GUARDRAIL REMOVAL TOTAL 375 FOOT				
LOCATION	FROM STATION	TO STATION	OFFSET	SUBTOTALS
US RTE 14	430+87	432+50	RT	165
US RTE 14	432+50	434+58	RT	210

64200108 SHOULDER RUMBLE STRIPS, 8 INCH TOTAL 12219 FOOT				
LOCATION	FROM STATION	TO STATION	OFFSET	SUBTOTALS
US RTE 14	129+67.00	152+24.00	WB RT	2,257.00
US RTE 14	129+67.00	152+08.00	WB LT	2,241.00
US RTE 14	129+22.00	153+35.00	EB RT	2,413.00
US RTE 14	129+22.00	148+68.00	EB LT	1,946.00
US RTE 14	155+79.00	167+27.00	WB RT	1,148.00
US RTE 14	161+52.00	166+85.00	WB LT	533.00
US RTE 14	158+98.00	163+51.00	EB LT	453.00
US RTE 14	156+18.00	168+46.00	EB RT	1,228.00

78100100 RAISED REFLECTIVE PAVEMENT MARKER TOTAL 1,419 EACH							
LOCATION	FROM STATION	TO STATION	SPACING	NO. REFLECTORS	OFFSET	SUBTOTALS	
US RTE 14	129+22	168+28	80	2	RT		98
US RTE 14	129+67	168+28	80	2	RT		98
US RTE 14	150+47	152+65	40	1	RT		6
US RTE 14	150+47	154+06	40	1	RT		9
US RTE 14	152+08	153+74	40	1	LT		5
US RTE 14	155+31	158+98	40	1	RT		10
US RTE 14	154+97	160+27	40	1	LT		14
US RTE 14	157+68	160+27	40	1	LT		7
US RTE 14	155+53	157+72	40	1	LT		6
US RTE 14	166+20	168+60	40	1	RT		6
US RTE 14	166+20	167+55	40	1	RT		4
US RTE 14	166+85	168+35	40	1	RT		4
US RTE 14	325+45	334+00	80	2	RT		22
US RTE 14	334+00	348+00	80	2	RT		36
US RTE 14	348+00	362+00	80	2	RT		36
US RTE 14	348+61	350+77	40	1	RT		6
US RTE 14	362+00	364+95	80	2	RT		8
US RTE 14	362+80	364+95	40	1	RT		6
US RTE 14	362+80	364+95	40	1	RT		6
US RTE 14	366+10	376+00	80	2	RT		26
US RTE 14	376+00	390+00	80	2	RT		36
US RTE 14	385+85	388+32	40	1	RT		7
US RTE 14	390+00	396+75	80	2	RT		18
US RTE 14	394+58	396+75	40	1	RT		6
US RTE 14	394+58	396+75	40	1	RT		6
US RTE 14	397+85	404+00	80	2	RT		16
US RTE 14	404+00	418+00	80	2	RT		36
US RTE 14	418+00	430+22	80	2	RT		32
US RTE 14	427+06	430+22	40	1	RT		8
US RTE 14	431+08	432+50	80	2	RT		4
US RTE 14	432+50	446+00	80	2	RT		34
US RTE 14	446+00	460+00	80	2	RT		36
US RTE 14	448+85	451+00	40	1	RT		6
US RTE 14	460+00	467+21	80	2	RT		20
US RTE 14	465+06	467+21	40	1	RT		6
US RTE 14	468+13	474+00	80	2	RT		16
US RTE 14	474+00	488+00	80	2	RT		36
US RTE 14	488+00	493+00	80	2	RT		14
US RTE 14	325+45	334+00	80	2	LT		22
US RTE 14	325+73	329+73	40	1	LT		10
US RTE 14	325+45	329+73	40	2	LT		22
US RTE 14	326+60	329+73	40	2	LT		16
US RTE 14	334+00	348+00	80	2	LT		36
US RTE 14	348+00	362+00	80	2	LT		36
US RTE 14	351+47	353+63	40	1	LT		6
US RTE 14	362+00	365+00	80	2	LT		8
US RTE 14	366+10	376+00	80	2	LT		26
US RTE 14	366+10	368+25	40	1	LT		6
US RTE 14	366+10	368+25	40	1	LT		6
US RTE 14	376+00	390+00	80	2	LT		36
US RTE 14	390+00	396+75	80	2	LT		18
US RTE 14	397+95	404+00	80	2	LT		16
US RTE 14	397+95	400+10	40	1	LT		6
US RTE 14	404+00	418+00	80	2	LT		36
US RTE 14	418+00	430+22	80	2	LT		32
US RTE 14	431+08	432+50	80	2	LT		4
US RTE 14	431+08	432+50	40	1	LT		4
US RTE 14	431+08	432+50	40	1	LT		4
US RTE 14	432+50	446+00	80	2	LT		34
US RTE 14	432+50	434+45	40	1	LT		5
US RTE 14	432+50	434+13	40	1	LT		5
US RTE 14	446+00	460+00	80	2	LT		36
US RTE 14	451+65	453+80	40	1	LT		6
US RTE 14	460+00	467+21	80	2	LT		20
US RTE 14	468+13	474+00	80	2	LT		16
US RTE 14	468+13	470+28	40	1	LT		6
US RTE 14	474+00	488+00	80	2	LT		36
US RTE 14	488+00	493+00	80	2	LT		14
LAKE SHORE DR	42+60	43+81	40	2			8
LAKE SHORE DR	43+81	44+64	40	2			6
LAKE SHORE DR	43+81	44+64	40	1	RT		3
LAKE SHORE DR	45+85	46+65	40	1	LT		2
LAKE SHORE DR	45+85	46+65	40	2	RT		4
LAKE SHORE DR	46+65	48+28	40	2			10
DOTY RD	0+71	4+60	40	1	LT		10
DOTY RD	0+71	4+60	40	2	LT		20
LILY POND RD	800+70	802+99	40	1	LT		6
LILY POND RD	800+70	802+99	40	2	LT		12
LILY POND RD	802+99	806+50	40	2			18
RIDGEFIELD RD	850+67	852+85	40	1	LT		6
RIDGEFIELD RD	850+68	407+59	40	2			32
LAKE AVE (NORTH SIDE ONLY)			80	2			2
LAKE AVE (NORTH SIDE ONLY)			80	2			2

78100300	REPLACEMENT REFLECTOR	TOTAL	24	EACH
LOCATION	FROM STATION	TO STATION	AVE. SPACING	SUBTOTALS
US RTE 14	156+00	165+40	80	24

X0300266	WOODEN FENCE REMOVAL	TOTAL	260	FOOT
LOCATION	FROM STATION	TO STATION	SUBTOTALS	
LILY POND RD	802+53.30	804+75.41	260	

X6061100	CONCRETE MEDIAN, TYPE SB (SPECIAL)	TOTAL	17,397	SQ FT
LOCATION	FROM STATION	TO STATION	OFFSET	SUBTOTALS
US RTE 14	348+61.19	350+80.23	LT	1308.3
US RTE 14	351+44.14	353+63.24	RT	1312.6
US RTE 14	362+80.74	364+98.74	LT	1304.1
US RTE 14	366+07.60	368+25.60	RT	1304.1
US RTE 14	393+58.81	396+76.81	LT	1904.1
US RTE 14	397+92.33	400+10.33	RT	1304.1
US RTE 14	427+06.45	430+23.43	LT	1892.9
US RTE 14	431+02.87	432+50.00	RT	876.0
US RTE 14	432+50.00	434+12.90	RT	974.15
US RTE 14	448+85.00	451+03.00	LT	1304.1
US RTE 14	451+62.00	453+80.00	RT	1304.1
US RTE 14	465+06.31	467+24.31	LT	1304.1
US RTE 14	468+10.40	470+28.38	RT	1304.1

78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	TOTAL	829	EACH
LOCATION	FROM STATION	TO STATION	AVE. SPACING	SUBTOTALS
US RTE 14	129+22	168+28	80	98
US RTE 14	129+67	168+28	80	98
US RTE 14	150+47	152+65	40	6
US RTE 14	150+47	154+06	40	10
US RTE 14	152+08	153+74	40	5
US RTE 14	155+31	158+98	40	9
US RTE 14	154+97	160+27	40	14
US RTE 14	157+68	160+27	40	7
US RTE 14	155+53	157+72	40	6
US RTE 14	166+20	168+60	40	6
US RTE 14	166+20	167+55	40	4
US RTE 14	166+85	168+35	40	4
US RTE 14	325+15	327+85	40	8
US RTE 14	325+40	331+65	80	16
US RTE 14	325+25	328+75	40	10
US RTE 14	326+65	328+75	40	6
US RTE 14	325+45	331+65	40	16
US RTE 14	325+40	331+65	80	16
US RTE 14	331+65	340+05	40	42
US RTE 14	340+05	344+25	80	10
US RTE 14	344+25	353+15	80	12
US RTE 14	353+15	357+40	80	10
US RTE 14	357+40	365+00	40	38
US RTE 14	366+15	373+70	40	38
US RTE 14	373+70	387+75	80	36
US RTE 14	387+75	396+60	40	44
US RTE 14	397+80	405+65	40	40
US RTE 14	405+65	431+50	80	64
US RTE 14	432+60	447+40	80	38
US RTE 14	447+40	456+15	80	12
US RTE 14	456+15	464+90	80	22
US RTE 14	466+90	475+35	80	22
US RTE 14	475+35	491+25	80	20
US RTE 14	491+25	493+00	40	8
US RTE 14	362+95	365+00	40	6
US RTE 14	362+95	365+00	40	6
US RTE 14	366+15	367+15	40	4
US RTE 14	366+15	368+10	40	6
US RTE 14	394+65	396+50	40	6
US RTE 14	397+80	400+15	40	6

X4402020	CONCRETE MEDIAN SURFACE REMOVAL	TOTAL	3,121	SQ FT
LOCATION	FROM STATION	TO STATION	AREA (SQ FT)	SUBTOTALS
US RTE 14	168+16	169+59	306.15	306
US RTE 14	326+16	331+65	2814.79	2814.79

X4402800	ISLAND PAVEMENT REMOVAL	TOTAL	158	SQ YD
LOCATION	FROM STATION	TO STATION	AREA (SQ FT)	SUBTOTALS
US RTE 14	325+19	325+52	613.66	68
US RTE 14	325+28	326+70	805.2	89

60608582	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24	TOTAL	200	FOOT
LOCATION	FROM STATION	TO STATION	OFFSET	SUBTOTALS
US RTE 14	325+29.00	326+56.46	LT	127
US RTE 14	328+07.98	328+80.44	RT	72

60608552	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06	TOTAL	140	FOOT
LOCATION	FROM STATION	TO STATION	OFFSET	SUBTOTALS
US RTE 14	325+25.00	326+56.46	LT	140.0

T:\DOCUMENTS\1934\9901\CIVIL\DCN\SC000.PY.DGN
 T:\DOCUMENTS\1934\9901\CIVIL\DCN\SC000.PY.DGN
 11-07-2013 16:19:01
 exp U.S. Services Inc.
 BUILDINGS-EARTH & ENVIRONMENT-ENERGY
 INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
#FILEL#		DRAWN - BRH	REVISED -
exp U.S. Services Inc.		CHECKED - TMH	REVISED -
	PLOT SCALE = *SCALE*	DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

SCHEDULE OF QUANTITIES - VII

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	34
CONTRACT NO. 62268			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

X6640300 CHAIN LINK FENCE REMOVAL				TOTAL 1,056 FOOT
LOCATION	FROM STATION	TO STATION	OFFSET	SUBTOTALS
US RTE 14	430+17	432+50	RT	231
US RTE 14	432+50	438+02	RT	551
US RTE 14	461+24	462+78	RT	274

X7830050 RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL					TOTAL 24 EACH
LOCATION	FROM STATION	TO STATION	AVERAGE SPACING	SUBTOTALS	
US RTE 14	156+00	165+40	80	24	

Z0073345 SLEEPER SLAB		TOTAL 205 FOOT
LOCATION		SUBTOTALS
US RTE 14 & LAKE SHORE DR (S)		95
US RTE 14 & LAKE SHORE DR (N)		109.81

Z0062456 TEMPORARY PAVEMENT						TOTAL 39,739 SQ YD
LOCATION	FROM STATION	TO STATION	OFFSET	AREA (SQ FT)	SUBTOTALS	
PRE-STAGE						
US RTE 14	165+76	168+53	RT	1608.93	178.8	
US RTE 14	325+17	325+57	LT	877.45	97.5	
US RTE 14	325+21	334+00	RT	7643.17	849.2	
US RTE 14	334+00	348+00	RT	15425.66	1714.0	
US RTE 14	348+00	362+00	RT	25900.30	2877.8	
US RTE 14	362+00	365+38	RT	7498.47	833.2	
US RTE 14	365+79	376+00	RT	24753.51	2750.4	
LAKE SHORE DR	45+67	47+25	RT	1283.57	142.6	
US RTE 14	376+00	383+04	RT/LT	7510.69	834.5	
US RTE 14	379+70	390+00	LT	18256.99	2028.6	
US RTE 14	390+00	404+00	LT	20647.78	2294.2	
DOTY ROAD	0+16	1+50	LT	2441.62	271.3	
US RTE 14	404+00	418+00	LT	36284.72	4031.6	
US RTE 14	418+00	431+12	LT	32219.81	3580.0	
US RTE 14	440+20	446+00	LT	7908.56	878.7	
US RTE 14	446+00	460+00	LT	24956.44	2772.9	
US RTE 14	460+00	465+65	LT	9568.85	1063.2	
US RTE 14	466+68	474+00	LT	12699.07	1411.0	
US RTE 14	474+00	488+00	LT	24406.25	2711.8	
US RTE 14	488+00	489+72	LT	1684.48	187.2	
US RTE 14	488+00	493+00	RT	9728.68	1081.0	
LAKE SHORE DR	47+25	488+59	RT	888.68	98.7	
LAKE SHORE DR	42+48	43+00	RT	182.73	20.3	
DOTY ROAD	1+50	3+05	LT	1614.66	179.4	
STAGE 1.1						
US 14	331+83	334+75	LT	1651.87	183.5	
US 14	352+80	355+60	LT/RT	4009.4	445.5	
US 14	362+10	365+41	LT	3906.97	434.1	
US 14	377+50	379+70	LT	5435.38	603.9	
US 14	396+71	397+42	LT/RT	1103.57	122.6	
US 14	430+45	432+50	LT/RT	3322.43	369.2	
US 14	432+50	432+78	LT/RT	216.11	24.0	
US 14	485+00	488+00	RT	6756.51	750.7	
STAGE 1.1 A-3						
US RTE 14	325+74	335+00	LT/RT	9855.90	1095.1	
STAGE 1.1 C-2						
US RTE 14	397+42	397+66	LT/RT	251.23	27.9	
STAGE 1.2						
US RTE 14	369+07	373+53	LT/RT	4287.41	476.4	
US RTE 14	478+67	483+14	LT	4287.44	476.4	
STAGE 2.1						
US RTE 14	429+18	431+34	LT/RT	2392.34	265.8	
US RTE 14	466+09	468+63	LT/RT	3179.86	353.3	
STAGE 2.1 A-1						
LILY POND RD	302+12	306+53		4545.62	505.1	
STAGE 2.1 B-1						
RIDGEFIELD RD	432+47	407+61		6455.00	717.2	

T:\DOCUMENT\1934\9901\CIVIL\DCN\SC000\FZ.DGN
 11-07-2013 16:19:03
 exp U.S. Services Inc.
 CHICAGO, IL
 BUILDINGS-EARTH & ENVIRONMENT-ENERGY
 INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	SCHEDULE OF QUANTITIES - VIII			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FILEL		DRAWN - BRH	REVISED -					305	27R-2	MCHENRY	673	35
exp U.S. Services Inc.	PLOT SCALE = *SCALE*	CHECKED - TMH	REVISED -		SCALE: N.T.S.			SHEET NO. OF SHEETS STA. TO STA.				
INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT DATE = *DATE*	DATE - 11/01/13	REVISED -					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
											CONTRACT NO. 62268	

EARTHWORK SCHEDULE

Table with columns: LOCATION (FROM STATION, TO STATION), EARTH EXCAVATION, TOPSOIL EXCAVATION, TOPSOIL PLACEMENT, REMOVAL & DISPOSAL OF UNSUITABLE MATERIALS, AGGREGATE SUBGRADE IMPROVEMENT, EXCAVATION TO BE USED IN EMBANKMENT, EMBANKMENT, and EARTHWORK BALANCE WASTE OR SHORTAGE.

- * TOTAL DOES NOT INCLUDE A SHRINKAGE FACTOR
** SEE EARTHWORK SCHEDULE REQUIRED FOR MULTI-USE PATH ON SHEET 11.
*** THE PLACEMENT OF THE TWO 2' WIDE TOPSOIL SHELF'S ADJACENT TO THE MULTI-USE PATH IS QUANTIFIED AS PART OF THE MULTI-USE PATH COLUMN.

EARTHWORK SCHEDULE CONTINUED ON NEXT SHEET

FILE NAME = ... HECHTBR

Project information including: STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION, SCHEDULE OF QUANTITIES - IX, U.S. ROUTE 14, CONTRACT NO. 62268, and various scales and dates.

EARTHWORK SCHEDULE

LOCATION		EARTH EXCAVATION**				TOPSOIL EXCAVATION (SEE GENERAL NOTE 16)				TOPSOIL PLACEMENT	REMOVAL & DISPOSAL OF UNSUITABLE MATERIALS				AGGREGATE SUBGRADE IMPROVEMENT PLACEMENT				EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)				EMBANKMENT				EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)				
FROM STATION	TO STATION	PRE- STAGE CU YD	STAGE 1 CU YD	STAGE 2 CU YD	STAGE 3 CU YD	PRE- STAGE CU YD	STAGE 1 CU YD	STAGE 2 CU YD	STAGE 3 CU YD	TOTAL*	PRE- STAGE CU YD	STAGE 1 CU YD	STAGE 2 CU YD	STAGE 3 CU YD	PRE- STAGE CU YD	STAGE 1 CU YD	STAGE 2 CU YD	STAGE 3 CU YD	PRE- STAGE CU YD	STAGE 1 CU YD	STAGE 2 CU YD	STAGE 3 CU YD	PRE- STAGE CU YD	STAGE 1 CU YD	STAGE 2 CU YD	STAGE 3 CU YD	PRE- STAGE CU YD	STAGE 1 CU YD	STAGE 2 CU YD	STAGE 3 CU YD	
US 14 (CONTINUED)																															
385+00.00	386+00.00	338.1	243.1	141.9	11.9	75.0	70.7	20.4	0.0	132.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	287.4	206.7	120.6	10.1	1.5	54.6	61.3	19.4	285.9	152.0	59.3	-9.4
386+00.00	387+00.00	444.4	250.2	41.1	5.2	90.0	58.9	13.5	0.0	135.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	377.8	212.7	34.9	4.4	1.5	51.1	127.4	34.1	376.3	161.5	-92.5	-29.7
387+00.00	388+00.00	463.5	355.6	36.5	6.7	95.2	56.7	10.0	0.0	140.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	394.0	302.2	31.0	5.7	1.5	26.1	122.0	21.9	392.5	276.1	-91.0	-16.2
388+00.00	389+00.00	358.1	375.9	40.4	16.3	89.4	67.8	11.5	0.0	144.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	304.4	319.5	34.3	13.9	1.3	35.2	98.9	9.3	303.1	284.4	-64.6	4.6
389+00.00	390+00.00	224.1	418.5	60.6	25.6	72.4	86.5	17.0	0.0	148.7	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	190.5	355.7	51.5	21.7	1.1	37.0	94.3	3.3	189.4	318.7	-42.8	18.4
390+00.00	391+00.00	91.7	519.5	92.0	15.7	51.1	114.6	22.6	0.2	160.6	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	77.9	441.6	78.2	13.4	8.0	6.9	119.3	10.9	70.0	434.8	-41.0	2.5
391+00.00	392+00.00	24.8	316.2	103.0	3.1	35.0	131.1	29.6	6.7	169.8	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	21.1	268.7	87.5	2.7	15.4	10.6	131.5	35.7	5.7	258.2	-44.0	-33.1
392+00.00	393+00.00	19.4	64.6	99.6	28.3	31.3	124.4	30.6	11.7	120.0	0.0	19.1	36.2	17.8	0.0	37.1	36.3	18.0	16.5	54.9	84.7	24.1	23.0	50.9	112.9	25.2	-6.4	4.0	-28.2	-1.1	
393+00.00	394+00.00	16.9	34.8	144.1	50.6	33.1	125.0	27.8	5.2	72.6	0.0	53.7	72.5	33.4	0.0	75.2	72.5	33.6	14.3	29.6	122.5	43.0	31.5	59.1	91.9	0.0	-17.2	-29.5	30.6	43.0	
394+00.00	395+00.00	14.8	111.1	227.6	57.6	32.6	115.6	29.1	0.0	69.8	0.0	76.4	70.1	29.9	0.0	79.9	70.1	29.9	12.6	94.4	193.5	49.0	34.6	62.2	70.5	0.0	-22.0	32.2	123.0	49.0	
395+00.00	396+00.00	15.4	170.4	291.5	77.6	34.3	103.3	31.7	0.0	67.4	0.0	83.6	69.6	28.6	0.0	83.6	69.6	28.6	13.1	144.8	247.8	66.0	44.6	90.2	69.6	0.0	-31.6	54.6	178.1	66.0	
396+00.00	397+00.00	15.0	235.4	344.8	43.0	38.7	89.1	33.9	0.0	55.9	0.0	94.1	87.9	14.3	0.0	94.1	87.9	14.3	12.8	200.1	293.1	36.5	57.6	48.5	98.6	0.0	-44.8	151.5	194.5	36.5	
397+00.00	398+00.00	23.0	244.3	268.3	22.0	51.9	80.2	39.3	0.0	53.0	0.0	52.3	52.2	0.0	0.0	52.3	52.2	0.0	19.5	207.6	228.1	18.7	58.1	10.6	156.0	0.0	-38.6	197.1	72.1	18.7	
398+00.00	399+00.00	21.1	168.0	98.1	29.1	51.7	100.9	46.3	0.0	73.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	17.9	142.8	83.4	24.7	51.7	26.3	223.3	10.2	-33.7	116.5	-139.9	14.5	
399+00.00	400+00.00	28.1	71.9	24.3	14.6	20.7	114.8	54.1	0.0	76.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	23.9	61.1	20.6	12.4	26.1	143.7	252.6	30.2	-2.2	-82.6	-232.0	-17.8	
400+00.00	401+00.00	62.0	218.5	46.7	13.7	15.9	118.5	52.6	0.0	77.8	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	52.7	185.7	39.7	11.6	2.0	131.3	130.2	24.6	50.7	54.4	-90.5	-13.0	
401+00.00	402+00.00	51.9	512.6	130.0	42.4	27.2	121.7	59.6	0.0	93.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	44.1	435.7	110.5	36.0	1.9	12.4	29.4	4.6	42.2	423.3	81.1	31.4	
402+00.00	403+00.00	31.7	608.0	307.4	58.1	33.1	112.8	62.8	0.0	134.6	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	26.9	516.8	261.3	49.4	3.1	8.9	25.0	0.0	23.8	507.9	236.3	49.4	
403+00.00	404+00.00	62.0	751.5	547.8	56.9	46.9	109.1	55.2	0.0	176.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	52.7	638.8	465.6	48.3	2.4	7.4	0.2	0.0	50.3	631.4	465.4	48.3	
404+00.00	405+00.00	70.6	1029.1	669.1	74.1	45.4	113.5	65.0	0.0	182.8	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	60.0	874.7	568.7	63.0	0.6	7.2	0.0	0.0	59.4	867.5	568.7	63.0	
405+00.00	406+00.00	44.8	1184.8	657.2	83.5	45.2	116.1	69.6	0.0	184.5	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	38.1	1007.1	558.7	71.0	0.0	7.0	0.0	0.0	38.1	1000.1	558.7	71.0	
406+00.00	407+00.00	19.1	938.9	564.5	88.3	73.3	106.3	61.3	0.0	192.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	16.2	798.1	479.8	75.1	157.6	6.9	133.1	0.0	-141.4	791.2	346.6	75.1	
407+00.00	408+00.00	3.1	676.5	471.9	89.3	115.4	96.3	48.3	0.0	211.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	2.7	575.0	401.1	75.9	764.4	6.7	473.9	0.0	-761.8	568.3	-72.8	75.9	
408+00.00	409+00.00	2.8	746.7	391.9	78.5	126.5	98.0	47.4	0.0	222.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	2.4	634.7	333.1	66.7	1018.9	6.7	661.7	0.0	-1016.5	628.0	-328.6	66.7	
409+00.00	410+00.00	1.7	678.5	288.2	40.7	118.5	96.3	53.7	0.0	218.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	1.4	576.7	245.0	34.6	856.9	6.7	658.0	0.6	-855.4	570.1	-413.0	34.1	
410+00.00	411+00.00	0.0	412.2	203.0	8.3	124.3	94.4	53.5	0.0	222.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	350.4	172.6	7.1	1030.9	28.3	781.1	35.0	-1030.9	322.1	-608.6	-27.9	
411+00.00	412+00.00	0.0	159.8	125.0	1.5	136.3	48.1	54.4	0.0	207.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	135.8	106.3	1.3	1289.6	115.9	1003.3	78.9	-1289.6	19.9	-897.1	-77.6	
412+00.00	413+00.00	1.5	56.1	61.5	1.5	117.6	40.7	76.1	0.0	202.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	1.3	47.7	52.3	1.3	807.8	231.9	879.8	94.3	-806.5	-184.2	-827.5	-93.0	
413+00.00	414+00.00	79.8	204.1	42.4	1.5	87.8	85.4	87.2	4.8	213.6	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	67.8	173.5	36.0	1.3	105.6	153.9	431.6	83.1	-37.7	19.6	-395.6	-81.9	
414+00.00	415+00.00	281.7	466.3	107.4	10.4	95.0	85.6	59.4	4.8	195.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	239.4	396.4	91.3	8.8	3.5	13.9	119.1	49.3	235.9	383.4	-27.8	-40.4	
415+00.00	416+00.00	991.5	951.1	395.4	20.7	120.6	80.4	40.0	0.0	200.6	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	842.8	808.4	336.1	17.6	4.4	0.0	16.1	29.4	838.3	808.4	320.0	-11.8	
416+00.00	417+00.00	1641.3	1308.1	686.9	22.0	138.1	78.9	40.7	0.0	224.7	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	1395.1	1111.9	583.8	18.7	4.4	0.0	16.3	26.9	1390.7	1111.9	567.5	-8.1	
417+00.00	418+00.00	1550.4	1069.1	666.5	20.0	139.1	71.7	41.3	0.0	219.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	1317.8	908.7	566.5	17.0	3.1	0.0	16.3	28.7	1314.7	908.7	550.2	-11.7	
418+00.00	419+00.00	860.9	480.2	435.9	9.8	123.0	69.3	41.5	1.5	198.7	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	731.8	408.2	370.5	8.3	2.0	86.3	15.0	48.9	729.8	321.9	355.6	-40.5	
419+00.00	420+00.00	163.7	72.0	198.0	1.5	123.5	81.1	51.3	1.5	212.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	139.1	61.2	168.3	1.3	581.5	425.2	381.6	79.3	-442.3	-364.0	-213.4	-78.0	
420+00.00	421+00.00	3.9	0.6	138.3	1.5	133.5	75.9	61.5	0.0	221.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	3.3	0.5	117.6	1.3	968.5	550.4	609.1	84.6	-965.2	-549.9	-491.5	-83.4	
421+00.00	422+00.00	25.6	18.7	183.3	1.5	115.4	65.2	60.2	1.1	193.7	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	21.7	15.9	155.8	1.3	395.0	247.6	259.2	73.1	-373.3	-231.7	-103.4	-71.9	
422+00.00	423+00.00	261.7	176.3	322.1	4.3	107.2	60.4	51.7	3.1	176.7	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	222.4	149.9	273.7	3.6	8.0	38.1	29.8	51.1	214.5	111.7	244.0	-47.5	
423+00.00	424+00.00	558.9	391.1	426.9	10.6	117.2	48.1	45.6	2.0	167.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	475.1	332.4	362.8	9.0	2.2	2.0	10.0	34.6	472.8	330.4	352.9	-25.7	
424+00.00	425+00.00	685.6	500.4																												

EARTHWORK SCHEDULE

LOCATION		EARTH EXCAVATION**				TOPSOIL EXCAVATION (SEE GENERAL NOTE 16)				TOPSOIL PLACEMENT	REMOVAL & DISPOSAL OF UNSUITABLE MATERIALS				AGGREGATE SUBGRADE IMPROVEMENT PLACEMENT				EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)				EMBANKMENT				EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)						
FROM STATION	TO STATION	PRE- STAGE CU YD	STAGE 1 CU YD	STAGE 2 CU YD	STAGE 3 CU YD	PRE- STAGE CU YD	STAGE 1 CU YD	STAGE 2 CU YD	STAGE 3 CU YD	TOTAL*	PRE- STAGE CU YD	STAGE 1 CU YD	STAGE 2 CU YD	STAGE 3 CU YD	PRE- STAGE CU YD	STAGE 1 CU YD	STAGE 2 CU YD	STAGE 3 CU YD	PRE- STAGE CU YD	STAGE 1 CU YD	STAGE 2 CU YD	STAGE 3 CU YD	PRE- STAGE CU YD	STAGE 1 CU YD	STAGE 2 CU YD	STAGE 3 CU YD	PRE- STAGE CU YD	STAGE 1 CU YD	STAGE 2 CU YD	STAGE 3 CU YD			
US 14 (CONTINUED)																																	
445+00.00	446+00.00	16.7	13.3	0.0	3.0	60.4	95.2	63.9	0.0	211.5	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	14.2	11.3	0.0	2.5	98.0	368.7	818.5	126.7	-83.8	-357.4	-818.5	-124.1			
446+00.00	447+00.00	19.6	9.4	0.0	9.8	45.9	95.6	67.4	0.0	154.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	16.7	8.0	0.0	8.3	60.4	424.3	716.1	108.9	-43.7	-416.2	-716.1	-100.5			
447+00.00	448+00.00	59.4	7.0	10.9	16.7	15.9	91.9	33.7	0.0	70.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	50.5	6.0	9.3	14.2	3.0	440.2	403.0	89.3	47.6	-434.2	-393.7	-75.1			
448+00.00	449+00.00	85.0	0.0	11.1	16.7	25.2	86.7	21.3	0.0	54.8	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	72.3	0.0	9.4	14.2	0.7	480.9	279.3	103.3	71.5	-480.9	-269.8	-89.2			
449+00.00	450+00.00	64.3	0.0	0.6	16.7	50.2	87.2	55.4	0.0	55.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	54.6	0.0	0.5	14.2	1.5	477.4	267.9	107.4	53.1	-477.4	-267.4	-93.2			
450+00.00	451+00.00	40.0	0.0	0.4	16.7	48.3	94.6	68.6	0.0	64.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	34.0	0.0	0.3	14.2	5.0	487.4	340.1	92.4	29.0	-487.4	-339.8	-78.2			
451+00.00	452+00.00	22.4	0.0	0.0	16.1	46.7	106.1	70.0	0.0	93.9	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	19.0	0.0	0.0	13.7	21.5	508.5	468.5	72.8	-2.4	-508.5	-468.5	-59.1			
452+00.00	453+00.00	18.9	0.0	1.1	15.2	45.4	108.7	63.5	0.0	95.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	16.1	0.0	0.9	12.9	44.8	465.0	443.9	53.5	-28.8	-465.0	-442.9	-40.6			
453+00.00	454+00.00	16.1	1.7	3.7	13.7	38.3	101.7	56.9	0.0	81.5	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	13.7	1.4	3.1	11.6	35.0	380.6	298.9	36.9	-21.3	-379.1	-295.7	-25.2			
454+00.00	455+00.00	25.7	5.0	12.2	12.8	38.3	89.8	46.9	0.0	69.6	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	21.9	4.3	10.4	10.9	9.1	272.4	155.2	23.3	12.8	-268.2	-144.8	-12.5			
455+00.00	456+00.00	47.6	12.4	33.5	7.8	46.9	83.9	38.0	0.0	114.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	40.5	10.5	28.5	6.6	2.2	171.9	67.8	38.3	38.2	-161.3	-39.3	-31.7			
456+00.00	457+00.00	55.9	23.3	52.0	2.6	49.6	86.5	40.4	0.0	169.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	47.5	19.8	44.2	2.2	1.3	131.1	64.6	56.1	46.2	-111.3	-20.4	-53.9			
457+00.00	458+00.00	35.6	38.5	59.8	2.8	48.5	90.0	41.9	0.0	173.9	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	30.2	32.7	50.8	2.4	4.1	114.6	93.3	50.0	26.1	-81.9	-42.5	-47.6			
458+00.00	459+00.00	25.2	45.9	66.1	3.1	49.1	92.0	42.6	0.0	177.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	21.4	39.0	56.2	2.7	7.4	112.2	110.9	45.0	14.0	-73.2	-54.7	-42.3			
459+00.00	460+00.00	50.0	75.2	79.1	3.1	53.1	95.7	42.4	0.0	184.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	42.5	63.9	67.2	2.7	5.0	100.9	90.7	45.7	37.5	-37.0	-23.5	-43.1			
460+00.00	461+00.00	85.2	97.0	75.3	3.0	57.8	80.9	40.9	0.0	147.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	72.4	82.5	64.0	2.5	1.7	63.5	73.8	47.4	70.7	19.0	-9.7	-44.9			
461+00.00	462+00.00	94.3	135.9	64.0	3.0	60.2	85.6	34.4	0.0	147.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	80.1	115.5	54.4	2.5	2.6	45.6	71.7	48.1	77.5	70.0	-17.3	-45.6			
462+00.00	463+00.00	100.2	147.8	58.7	11.1	60.6	84.1	21.9	0.0	103.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	85.2	125.6	49.9	9.4	3.0	37.8	72.2	24.8	82.2	87.8	-22.3	-15.4			
463+00.00	464+00.00	110.4	143.7	46.5	15.2	61.3	85.6	13.7	0.0	50.7	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	93.8	122.1	39.5	12.9	1.9	48.1	78.5	2.2	92.0	74.0	-39.0	10.7			
464+00.00	465+00.00	121.5	155.9	37.8	16.3	60.7	109.3	17.0	0.0	82.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	103.3	132.5	32.1	13.9	1.5	75.6	62.6	16.3	101.8	57.0	-30.5	-2.4			
465+00.00	466+00.00	68.3	144.4	70.4	10.7	29.8	104.8	40.6	0.0	94.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	58.1	122.8	59.8	9.1	0.7	50.9	89.3	37.6	57.3	71.9	-29.4	-28.5			
466+00.00	467+00.00	78.7	234.1	387.4	8.3	36.3	104.3	80.0	0.0	128.9	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	66.9	199.0	329.3	7.1	1.1	61.1	141.3	62.0	65.8	137.9	188.0	-55.0			
467+00.00	468+00.00	165.4	190.9	360.7	16.9	68.9	114.4	78.1	0.0	109.3	0.0	8.8	42.1	0.0	0.0	0.0	0.0	0.0	140.6	162.3	306.6	14.3	1.9	251.9	187.4	88.9	138.7	-89.6	119.2	-74.6			
468+00.00	469+00.00	97.0	33.7	32.2	16.5	59.4	124.4	63.3	0.0	98.1	0.0	8.8	42.1	0.0	0.0	0.0	0.0	0.0	82.5	28.6	27.4	14.0	20.9	680.7	328.5	103.0	61.6	-652.1	-301.1	-89.0			
469+00.00	470+00.00	44.3	0.0	66.1	15.9	60.2	121.5	63.5	0.0	125.6	0.0	1.2	0.0	0.0	0.0	0.0	0.0	0.0	235.3	162.8	18.0	37.6	0.0	56.2	13.5	55.2	853.1	340.0	74.9	-17.6	-853.1	-283.8	-61.4
470+00.00	471+00.00	102.6	17.4	168.1	21.9	66.9	104.1	51.5	0.0	112.2	0.0	29.8	23.7	18.0	0.0	0.0	0.0	0.0	235.0	68.1	36.0	87.2	14.8	142.9	18.6	39.8	415.7	163.9	22.3	47.4	-400.9	-21.0	-3.7
471+00.00	472+00.00	103.7	93.0	235.9	67.6	63.1	82.4	42.0	0.0	85.0	0.0	55.6	48.3	36.0	0.0	0.0	0.0	0.0	113.6	49.2	36.0	88.1	79.0	200.5	57.5	10.6	75.7	54.1	0.0	77.6	3.3	146.5	57.5
472+00.00	473+00.00	81.3	183.9	319.1	98.0	59.6	71.3	36.9	0.0	129.6	0.0	27.0	24.6	18.0	0.0	0.0	0.0	0.0	27.0	24.6	18.0	69.1	156.3	271.2	83.3	9.6	55.2	22.6	0.0	59.5	101.1	248.6	83.3
473+00.00	474+00.00	96.7	176.1	383.7	83.1	59.6	78.5	36.5	0.0	199.1	0.0	25.7	27.0	18.0	0.0	0.0	0.0	0.0	82.2	149.7	326.1	70.7	7.8	128.3	15.0	0.0	74.4	21.4	311.1	70.7			
474+00.00	475+00.00	100.2	135.9	361.5	66.3	58.5	83.9	38.7	0.0	205.7	0.0	52.7	54.0	0.0	0.0	0.0	0.0	0.0	54.0	54.0	0.0	85.2	115.5	307.3	56.4	4.8	174.4	15.2	0.0	80.3	-58.9	292.1	56.4
475+00.00	476+00.00	100.2	136.5	321.5	47.2	57.2	76.1	40.6	0.0	198.3	0.0	54.0	54.0	0.0	0.0	0.0	0.0	0.0	54.0	54.0	0.0	85.2	116.0	273.3	40.1	1.9	111.7	16.1	0.0	83.3	4.3	257.1	40.1
476+00.00	477+00.00	154.3	310.4	343.9	33.5	60.4	85.2	40.7	0.0	210.6	0.0	54.0	54.0	0.0	0.0	0.0	0.0	0.0	54.0	54.0	0.0	131.1	263.8	292.3	28.5	1.7	53.5	17.0	0.0	129.5	210.3	275.3	28.5
477+00.00	478+00.00	200.9	459.3	347.4	22.4	62.4	97.6	41.1	0.0	225.0	0.0	54.0	54.0	0.0	0.0	0.0	0.0	0.0	54.0	54.0	0.0	170.8	390.4	295.3	19.0	1.5	20.7	16.7	3.9	169.3	369.6	278.6	15.2
478+00.00	479+00.00	171.3	336.1	156.9	18.9	58.0	114.1	20.9	0.0	216.7	0.0	27.0	27.0	0.0	0.0	0.0	0.0	0.0	27.0	27.0	0.0	145.6	285.7	133.3	16.1	1.5	60.6	8.0	17.4	144.1	225.1	125.4	-1.4
479+00.00	480+00.00	86.9	136.3	0.0	33.1	49.4	151.1	0.0	0.0	224.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	73.8	115.9	0.0	28.2	15.2	521.7	0.0	16.9	58.6	-405.8	0.0	11.3			
480+00.00	481+00.00	19.3	25.4	0.0	44.8	48.0	176.1	0.0	0.0	248.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	16.4	21.6	0.0	38.1	118.9	1232.0	0.0	3.3	-102.5	-1210.5	0.0	34.8			
481+00.00	482+00.00	14.4	34.6	0.0	41.3	54.4	169.4	0.0	0.0	247.6	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	12.3	29.4	0.0	35.1	192.0	1187.2	0.0	7.0	-179.8	-1157.8	0.0	28.1			
482+00.00	483+00.00	16.7	182.4	0.0	28.7	47.0	171.7	0.0	0.0	241.9	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	14.2	155.0	0.0	24.4	104.6	550.6	0.0	13.5	-90.5	-395.5	0.0	10.9			
483+00.00	484+00.00	39.6	435.9	0.0	23.1	45.6	173.5	0.0	0.0	242.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	33.7	370.5	0.0	19.7	17.8	149.1	0.0	6.5	15.9	221.5	0.0	13.2			
484+00.00	485+00.00	59.1	464.1	102.6	20.9	53.3	147.8	8.9	0.0	234.1	0.0																						

EARTHWORK SCHEDULE

Table with columns for LOCATION (FROM STATION, TO STATION), EARTH EXCAVATION (PRE-STAGE, STAGE 1-3), TOPSOIL EXCAVATION (PRE-STAGE, STAGE 1-3), TOPSOIL PLACEMENT, REMOVAL & DISPOSAL OF UNSUITABLE MATERIALS (PRE-STAGE, STAGE 1-3), AGGREGATE SUBGRADE IMPROVEMENT (PRE-STAGE, STAGE 1-3), EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE) (PRE-STAGE, STAGE 1-3), EMBANKMENT (PRE-STAGE, STAGE 1-3), and EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (PRE-STAGE, STAGE 1-3).

- TOTAL DOES NOT INCLUDE A SHRINKAGE FACTOR
•• SEE EARTHWORK SCHEDULE REQUIRED FOR MULTI-USE PATH ON SHEET 11.
••• THE PLACEMENT OF THE TWO 2' WIDE TOPSOIL SHELF'S ADJACENT TO THE MULTI-USE PATH IS QUANTIFIED AS PART OF THE MULTI-USE PATH COLUMN.

EARTHWORK SCHEDULE CONTINUED ON NEXT SHEET

ATTB008JJ.DGN...SC0000.RZ.DGN...HECHTBR

FILE NAME =
USER NAME = HECHTBR
DESIGNED -
DRAWN - RJS
PLOT SCALE = *SCALE*
CHECKED - AG
DATE - 11/01/13
REVISED -

exp U.S. Services Inc.
EXPLORE - EARTH & ENVIRONMENT-ENERGY
INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
U.S. ROUTE 14

SCHEDULE OF QUANTITIES - XII

Table with columns: F.A.P. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO., SCALE: N.T.S., SHEET NO. OF SHEETS, STA. TO STA., FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT, CONTRACT NO. 62268

TOPSOIL EXCAVATION = 36198.9 CU YD
 - TOPSOIL PLACEMENT = 26658.1 CU YD
 TOPSOIL SURPLUS = 9540.8 CU YD

EARTHWORK SCHEDULE (CONT)

LOCATION		EARTH EXCAVATION**				TOPSOIL EXCAVATION (SEE GENERAL NOTE 16)				TOPSOIL PLACEMENT	REMOVAL & DISPOSAL OF UNSUITABLE MATERIALS				AGGREGATE SUBGRADE IMPROVEMENT PLACEMENT				EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)				EMBANKMENT				EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)			
FROM STATION	TO STATION	PRE-STAGE	STAGE 1	STAGE 2	STAGE 3	PRE-STAGE	STAGE 1	STAGE 2	STAGE 3	TOTAL*	PRE-STAGE	STAGE 1	STAGE 2	STAGE 3	PRE-STAGE	STAGE 1	STAGE 2	STAGE 3	PRE-STAGE	STAGE 1	STAGE 2	STAGE 3	PRE-STAGE	STAGE 1	STAGE 2	STAGE 3	PRE-STAGE	STAGE 1	STAGE 2	STAGE 3
		CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD
EX N RIDGEFIELD																														
431+37.00	432+00.00	0.0	0.0	3.9	0.0	0.0	0.0	10.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	3.3	0.0	0.0	0.0	3.5	0.0	0.0	0.0	-0.2	0.0
432+00.00	432+50.00	0.0	0.0	3.7	0.0	0.0	0.0	7.7	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	3.1	0.0	0.0	0.0	2.5	0.0	0.0	0.0	0.6	0.0
432+50.00	433+00.00	0.0	0.0	1.5	0.0	0.0	0.0	7.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	1.3	0.0	0.0	0.0	5.3	0.0	0.0	0.0	-4.0	0.0
433+00.00	433+50.00	0.0	0.0	3.0	0.0	0.0	0.0	13.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	2.5	0.0	0.0	0.0	9.5	0.0	0.0	0.0	-7.0	0.0
433+50.00	434+00.00	0.0	0.0	4.4	0.0	0.0	0.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	3.7	0.0	0.0	0.0	9.1	0.0	0.0	0.0	-5.4	0.0
434+00.00	434+50.00	0.0	0.0	13.3	0.0	0.0	0.0	20.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	11.3	0.0	0.0	0.0	4.0	0.0	0.0	0.0	7.4	0.0
434+50.00	435+00.00	0.0	0.0	26.3	0.0	0.0	0.0	12.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	22.4	0.0	0.0	0.0	0.3	0.0	0.0	0.0	22.1	0.0
435+00.00	435+50.00	0.0	0.0	26.5	0.0	0.0	0.0	12.5	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	22.5	0.0	0.0	0.0	0.5	0.0	0.0	0.0	22.0	0.0
435+50.00	436+00.00	0.0	0.0	22.3	0.0	0.0	0.0	24.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	19.0	0.0	0.0	0.0	1.4	0.0	0.0	0.0	17.6	0.0
436+00.00	436+50.00	0.0	0.0	19.9	0.0	0.0	0.0	23.9	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	16.9	0.0	0.0	0.0	5.7	0.0	0.0	0.0	11.2	0.0
436+50.00	437+00.00	0.0	0.0	15.3	0.0	0.0	0.0	22.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	13.0	0.0	0.0	0.0	10.8	0.0	0.0	0.0	2.2	0.0
437+00.00	437+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
437+00.00	437+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
406+74.16	407+08.10	0.0	0.0	5.3	0.0	0.0	0.0	11.6	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	4.5	0.0	0.0	0.0	7.5	0.0	0.0	0.0	-3.1	0.0
407+08.10	407+61.83	0.0	0.0	4.2	0.0	0.0	0.0	15.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	3.6	0.0	0.0	0.0	10.9	0.0	0.0	0.0	-7.4	0.0
407+61.83	END	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
SUBTOTAL		0.0	0.0	149.4	0.0	0.0	0.0	196.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	127.0	0.0	0.0	0.0	71.1	0.0	0.0	0.0	55.9	0.0
DITCH AT STA. 336+90 RT				111.7				26.7					0.0								94.9				0.0				94.9	
TOTAL		18673.5	46512.2	30856.8	5002.8	9227.7	17984.0	8630.5	356.7	26658.1	0.0	2198.9	1799.7	566.5	0.0	4483.6	2544.0	702.6	15872.5	39535.4	26228.3	4252.4	12653.5	32591.2	27488.5	4848.9	3219.0	6944.2	-1260.2	-596.5

* TOTAL DOES NOT INCLUDE A SHRINKAGE FACTOR

** SEE EARTHWORK SCHEDULE BELOW FOR REQUIRED FOR EARTH EXCAVATION FOR MULTI-USE.

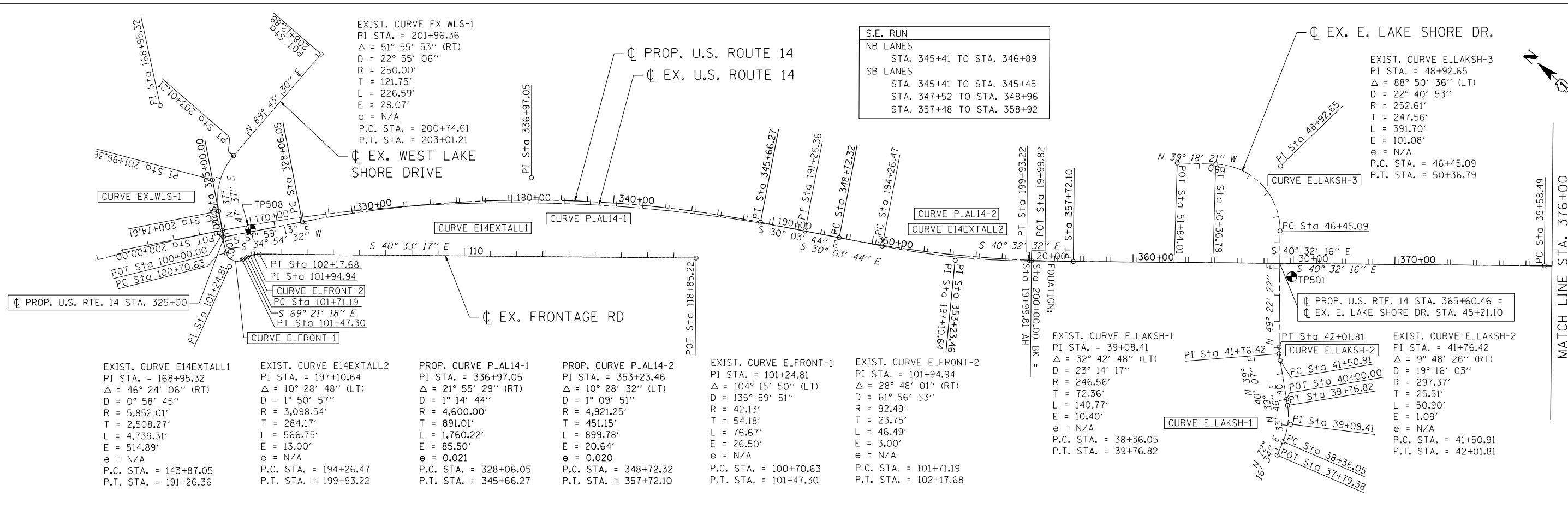
*** THE PLACEMENT OF THE TWO 2' WIDE TOPSOIL SHELF'S ADJACENT TO THE MULTI-USE PATH IS QUANTIFIED AS PART OF THE MULTI-USE PATH COLUMN.

LOCATION		EARTH EXCAVATION	
FROM STATION	TO STATION	STAGE 1	STAGE 2
		CU YD	CU YD
PR US 14 - MULTI-USE PATH 80% IDOT 20% MCCD			
325+74.26	326+00.00	0.0	0.0
326+00.00	327+00.00	0.0	0.0
327+00.00	328+00.00	0.0	0.0
328+00.00	329+00.00	0.0	0.0
329+00.00	330+00.00	0.0	0.0
330+00.00	331+00.00	0.0	0.0
331+00.00	332+00.00	21.1	0.0
332+00.00	333+00.00	34.1	0.0
333+00.00	334+00.00	13.0	0.0
334+00.00	335+00.00	0.2	0.0
335+00.00	336+00.00	33.1	0.0
336+00.00	337+00.00	48.1	0.0
337+00.00	338+00.00	56.1	0.0
338+00.00	338+50.00	27.8	0.0
338+50.00	339+00.00	11.5	0.0
339+00.00	340+00.00	16.7	0.0
340+00.00	341+00.00	25.0	0.0
341+00.00	342+00.00	45.7	0.0
342+00.00	343+00.00	33.0	0.0
343+00.00	344+00.00	3.9	0.0
344+00.00	345+00.00	0.0	0.0
345+00.00	346+00.00	0.0	0.0
346+00.00	347+00.00	0.0	0.0
347+00.00	348+00.00	0.0	0.0
348+00.00	349+00.00	40.4	0.0
349+00.00	350+00.00	77.0	0.0
350+00.00	351+00.00	104.3	0.0
351+00.00	352+00.00	174.4	0.0
352+00.00	353+00.00	251.1	0.0
353+00.00	354+00.00	222.6	0.0
354+00.00	355+00.00	116.5	0.0

LOCATION		EARTH EXCAVATION	
FROM STATION	TO STATION	STAGE 1	STAGE 2
		CU YD	CU YD
PR US 14 - MULTI-USE PATH 80% IDOT 20% MCCD			
355+00.00	356+00.00	110.9	0.0
356+00.00	357+00.00	206.9	0.0
357+00.00	358+00.00	263.9	0.0
358+00.00	359+00.00	238.3	0.0
359+00.00	360+00.00	204.4	0.0
360+00.00	361+00.00	163.1	0.0
361+00.00	362+00.00	68.3	0.0
362+00.00	363+00.00	1.1	0.0
363+00.00	364+00.00	0.0	0.0
364+00.00	365+00.00	0.0	0.0
365+00.00	366+00.00	0.0	0.0
366+00.00	367+00.00	0.0	0.0
367+00.00	368+00.00	0.0	0.0
368+00.00	369+00.00	0.0	0.0
369+00.00	370+00.00	0.0	0.0
370+00.00	371+00.00	0.0	0.0
371+00.00	372+00.00	0.0	0.0
372+00.00	373+00.00	9.4	0.0
373+00.00	374+00.00	35.6	0.0
374+00.00	375+00.00	52.2	0.0
375+00.00	376+00.00	26.5	0.0
376+00.00	377+00.00	0.4	0.0
377+00.00	378+00.00	0.0	0.0
378+00.00	379+00.00	0.0	0.0
379+00.00	380+00.00	0.0	0.0
380+00.00	381+00.00	0.0	0.0
381+00.00	382+00.00	0.0	11.7
382+00.00	383+00.00	0.0	20.2
383+00.00	384+00.00	0.0	33.3
384+00.00	385+00.00	0.0	85.6

LOCATION		EARTH EXCAVATION	
FROM STATION	TO STATION	STAGE 1	STAGE 2
		CU YD	CU YD
PR US 14 - MULTI-USE PATH 80% IDOT 20% MCCD			
385+00.00	386+00.00	0.0	81.9
386+00.00	387+00.00	0.0	39.1
387+00.00	388+00.00	0.0	27.8
388+00.00	389+00.00	0.0	11.5
389+00.00	390+00.00	0.0	1.7
390+00.00	391+00.00	0.0	0.0
391+00.00	392+00.00	0.0	1.9
392+00.00	393+00.00	0.0	2.6
393+00.00	394+00.00	0.0	2.0
394+00.00	395+00.00	0.0	1.5
395+00.00	396+00.00	0.0	0.2
396+00.00	397+00.00	0.0	0.0
397+00.00	398+00.00	0.0	0.0
398+00.00	399+00.00	0.0	0.0
399+00.00	400+00.00	0.0	0.0
400+00.00	401+00.00	0.0	5.9
401+00.00	402+00.00	0.0	6.5
402+00.00	403+00.00	0.0	20.2
403+00.00	404+00.00	0.0	63.9
404+00.00	405+00.00	0.0	79.4
405+00.00	406+00.00	0.0	60.9
406+00.00	407+00.00	0.0	25.7
407+00.00	408+00.00	0.0	0.0
408+00.00	409+00.00	0.0	0.0
409+00.00	410+00.00	0.0	0.0
410+00.00	411+00.00	0.0	0.0
411+00.00	412+00.00	0.0	0.0
412+00.00	413+00.00	0.0	0.0
413+00.00	414+00.00	0.0	0.0
414+00.00	415+00.00	0.0	17.8

LOCATION		EARTH EXCAVATION	
FROM STATION	TO STATION	STAGE 1	STAGE 2
		CU YD	CU YD
PR US 14 - MULTI-USE PATH 80% IDOT 20% MCCD			
415+00.00	416+00.00	0.0	160.6
416+00.00	417+00.00	0.0	318.7
417+00.00	418+00.00	0.0	302.4
418+00.00	419+00.00	0.0	166.9
419+00.00	420+00.00	0.0	40.4
420+00.00	421+00.00	0.0	0.0
421+00.00	422+00.00	0.0	0.0
422+00.00	423+00.00	0.0	69.1
423+00.00	424+00.00	0.0	135.7
424+00.00	425+00.00	0.0	114.1
425+00.00	426+00.00	0.0	101.7
426+00.00	427+00.00	0.0	54.3
427+00.00	428+00.00	0.0	0.0
428+00.00	429+00.00	0.0	0.0
429+00.00	430+00.00	0.0	0.0
430+00.00	431+00.00	0.0	0.0
431+00.00	432+00.00	0.0	6.9
432+00.00	433+00.00	0.0	11.3



EXIST. CURVE EX.WLS-1
 PI STA. = 201+96.36
 $\Delta = 51^\circ 55' 53''$ (RT)
 $R = 22^\circ 55' 06''$
 $R = 250.00'$
 $T = 121.75'$
 $L = 226.59'$
 $E = 28.07'$
 $e = N/A$
 P.C. STA. = 200+74.61
 P.T. STA. = 203+01.21

S.E. RUN
 NB LANES
 STA. 345+41 TO STA. 346+89
 SB LANES
 STA. 345+41 TO STA. 345+45
 STA. 347+52 TO STA. 348+96
 STA. 357+48 TO STA. 358+92

EXIST. CURVE E.LAKSH-3
 PI STA. = 48+92.65
 $\Delta = 88^\circ 50' 36''$ (LT)
 $D = 22^\circ 40' 53''$
 $R = 252.61'$
 $T = 247.56'$
 $L = 391.70'$
 $E = 101.08'$
 $e = N/A$
 P.C. STA. = 46+45.09
 P.T. STA. = 50+36.79

EXIST. CURVE E14EXTALL1
 PI STA. = 168+95.32
 $\Delta = 46^\circ 24' 06''$ (RT)
 $D = 0^\circ 58' 45''$
 $R = 5,852.01'$
 $T = 2,508.27'$
 $L = 4,739.31'$
 $E = 514.89'$
 $e = N/A$
 P.C. STA. = 143+87.05
 P.T. STA. = 191+26.36

EXIST. CURVE E14EXTALL2
 PI STA. = 197+10.64
 $\Delta = 10^\circ 28' 48''$ (LT)
 $D = 1^\circ 50' 57''$
 $R = 3,098.54'$
 $T = 284.17'$
 $L = 566.75'$
 $E = 13.00'$
 $e = N/A$
 P.C. STA. = 194+26.47
 P.T. STA. = 199+93.22

PROP. CURVE P.AL14-1
 PI STA. = 336+97.05
 $\Delta = 21^\circ 55' 29''$ (RT)
 $D = 1^\circ 14' 44''$
 $R = 4,600.00'$
 $T = 891.01'$
 $L = 1,760.22'$
 $E = 85.50'$
 $e = 0.021$
 P.C. STA. = 328+06.05
 P.T. STA. = 345+66.27

PROP. CURVE P.AL14-2
 PI STA. = 353+23.46
 $\Delta = 10^\circ 28' 32''$ (LT)
 $D = 1^\circ 09' 51''$
 $R = 4,921.25'$
 $T = 451.15'$
 $L = 899.78'$
 $E = 20.64'$
 $e = 0.020$
 P.C. STA. = 348+72.32
 P.T. STA. = 357+72.10

EXIST. CURVE E.FRONT-1
 PI STA. = 101+24.81
 $\Delta = 104^\circ 15' 50''$ (LT)
 $D = 135^\circ 59' 51''$
 $R = 42.13'$
 $T = 54.18'$
 $L = 76.67'$
 $E = 26.50'$
 $e = N/A$
 P.C. STA. = 100+70.63
 P.T. STA. = 101+47.30

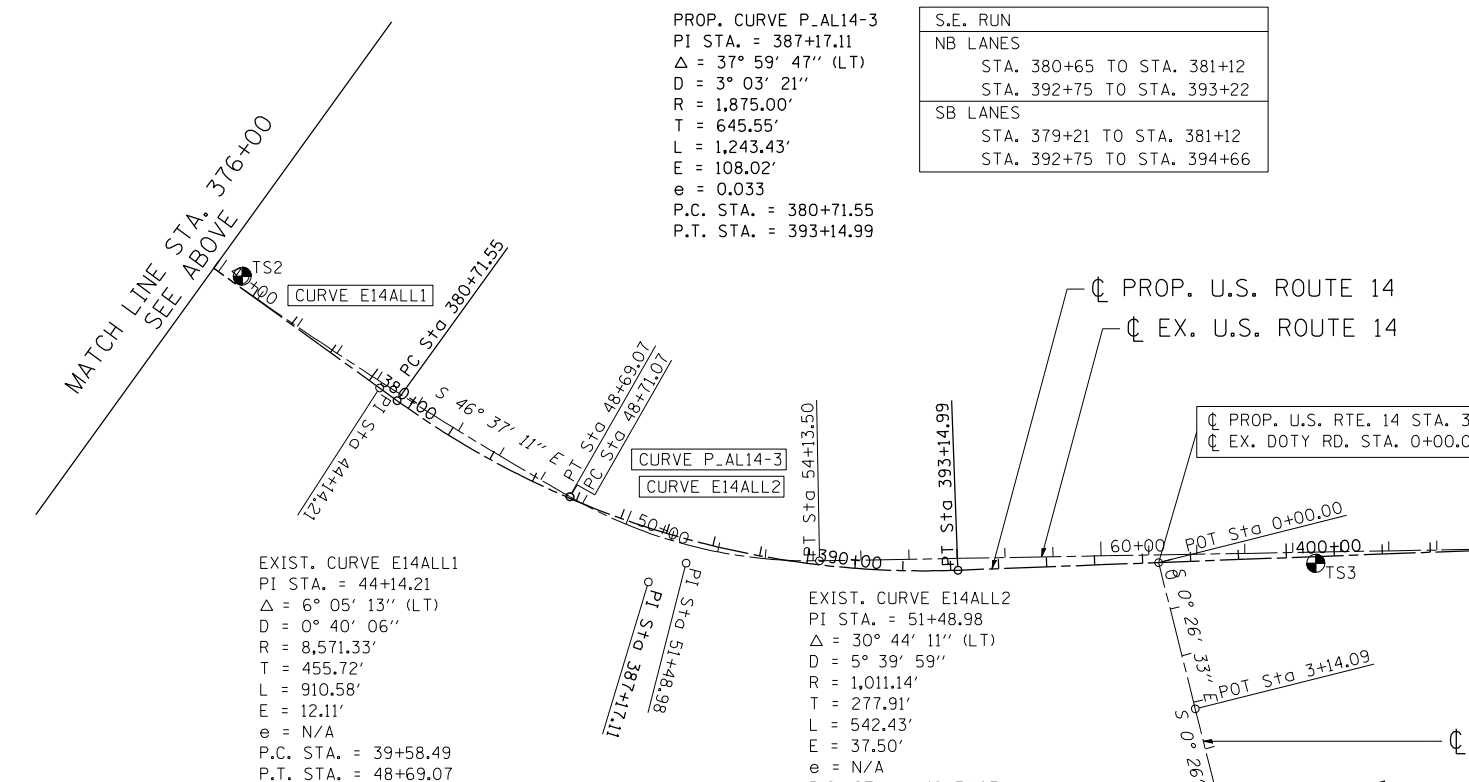
EXIST. CURVE E.FRONT-2
 PI STA. = 101+94.94
 $\Delta = 28^\circ 48' 01''$ (RT)
 $D = 61^\circ 56' 53''$
 $R = 92.49'$
 $T = 23.75'$
 $L = 46.49'$
 $E = 3.00'$
 $e = N/A$
 P.C. STA. = 101+71.19
 P.T. STA. = 102+17.68

EXIST. CURVE E.LAKSH-1
 PI STA. = 39+08.41
 $\Delta = 32^\circ 42' 48''$ (LT)
 $D = 23^\circ 14' 17''$
 $R = 246.56'$
 $T = 72.36'$
 $L = 140.77'$
 $E = 10.40'$
 $e = N/A$
 P.C. STA. = 38+36.05
 P.T. STA. = 39+76.82

EXIST. CURVE E.LAKSH-2
 PI STA. = 41+76.42
 $\Delta = 9^\circ 48' 26''$ (RT)
 $D = 19^\circ 16' 03''$
 $R = 297.37'$
 $T = 25.51'$
 $L = 50.90'$
 $E = 1.09'$
 $e = N/A$
 P.C. STA. = 41+50.91
 P.T. STA. = 42+01.81

EXIST. CURVE E.LAKSH-1
 PI STA. = 39+08.41
 $\Delta = 32^\circ 42' 48''$ (LT)
 $D = 23^\circ 14' 17''$
 $R = 246.56'$
 $T = 72.36'$
 $L = 140.77'$
 $E = 10.40'$
 $e = N/A$
 P.C. STA. = 38+36.05
 P.T. STA. = 39+76.82

DESCRIPTION	STATION	NORTHING	EASTING
EX. U.S. ROUTE 14			
P.C. E14EXTALL1	143+87.05	2,049,616.1747	959,251.1662
P.I. E14EXTALL1	168+95.32	2,049,029.0904	961,689.7622
P.T. E14EXTALL1	191+26.36	2,046,858.2280	962,946.2553
P.C. E14EXTALL2	194+26.47	2,046,598.4942	963,096.5890
P.I. E14EXTALL2	197+10.64	2,046,352.5502	963,238.9412
P.T. E14EXTALL2	199+93.22	2,046,136.6017	963,423.6540
P.O.T. AE014	19+99.81	2,046,131.4500	963,428.0605
P.O.T. AEUSEXT4	19+99.82	2,046,131.4475	963,428.0626
P.C. E14ALL1	39+58.49	2,044,642.9012	964,701.1045
P.I. E14ALL1	44+14.21	2,044,296.5395	964,997.2704
P.T. E14ALL1	48+69.07	2,043,983.5347	965,328.4935
P.C. E14ALL2	48+71.07	2,043,982.1634	965,329.9447
P.I. E14ALL2	51+48.98	2,043,791.2849	965,531.9332
P.T. E14ALL2	54+13.50	2,043,730.4533	965,803.1038
EX. WEST LAKE SHORE DRIVE			
P.O.B.	325+00.00	2,048,516.3430	961,470.1062
P.C. P.AL14-1	328+06.05	2,048,327.8672	961,711.2300
P.I. P.AL14-1	336+97.05	2,047,779.1457	962,413.2291
P.T. P.AL14-1	345+66.27	2,047,007.9930	962,859.5715
P.C. P.AL14-2	348+72.32	2,046,743.1108	963,012.8851
P.I. P.AL14-2	353+23.46	2,046,352.6519	963,238.8823
P.T. P.AL14-2	357+72.10	2,046,009.7915	963,532.1052
P.C. P.AL14-3	380+71.55	2,044,262.2574	965,026.6407
P.I. P.AL14-3	387+17.11	2,043,771.6543	965,446.2170
P.T. P.AL14-3	393+14.99	2,043,643.3323	966,078.8853
EX. LAKE SHORE DR.			
P.O.T. A500	37+79.38	2,044,937.3757	963,486.4676
P.C. E.LAKSH-1	38+36.05	2,044,954.6281	963,540.4486
P.I. E.LAKSH-1	39+08.41	2,044,976.6580	963,609.3779
P.T. E.LAKSH-1	39+76.82	2,045,032.4455	963,655.4682
P.O.T. A501	40+00.00	2,045,050.3149	963,670.2314
P.C. E.LAKSH-2	41+50.91	2,045,166.4763	963,766.5630
P.I. E.LAKSH-2	41+76.42	2,045,186.1440	963,782.8136
P.T. E.LAKSH-2	42+01.81	2,045,202.7562	963,802.1769
P.C. E.LAKSH-3	46+45.09	2,045,491.3884	964,138.6074
P.I. E.LAKSH-3	48+92.65	2,045,652.5838	964,326.4973
P.T. E.LAKSH-3	50+36.79	2,045,843.6893	964,169.1275
P.O.T. A502	51+84.01	2,045,957.6109	964,075.8646



EXIST. CURVE E14ALL1
 PI STA. = 44+14.21
 $\Delta = 6^\circ 05' 13''$ (LT)
 $D = 0^\circ 40' 06''$
 $R = 8,571.33'$
 $T = 455.72'$
 $L = 910.58'$
 $E = 12.11'$
 $e = N/A$
 P.C. STA. = 39+58.49
 P.T. STA. = 48+69.07

EXIST. CURVE E14ALL2
 PI STA. = 51+48.98
 $\Delta = 30^\circ 44' 11''$ (LT)
 $D = 5^\circ 39' 59''$
 $R = 1,011.14'$
 $T = 277.91'$
 $L = 542.43'$
 $E = 37.50'$
 $e = N/A$
 P.C. STA. = 48+71.07
 P.T. STA. = 54+13.50

DESCRIPTION	STATION	NORTHING	EASTING
EX. DOTY RD.			
P.O.T. A20	0+00.00	2,043,560.1736	966,488.8843
P.O.T. A21	3+14.09	2,043,246.0918	966,491.3095
P.O.T. A22	5+87.93	2,042,972.2610	966,493.4240

S.E. RUN
 NB LANES
 STA. 380+65 TO STA. 381+12
 STA. 392+75 TO STA. 393+22
 SB LANES
 STA. 379+21 TO STA. 381+12
 STA. 392+75 TO STA. 394+66

DESCRIPTION	STATION	NORTHING	EASTING
EX. WEST LAKE SHORE DRIVE			
P.O.T. W250	200+00.00	2,048,535.9591	961,487.2136
P.C. EX.WLS-1	200+74.61	2,048,594.9211	961,532.9388
P.I. EX.WLS-1	201+96.36	2,048,691.1292	961,607.5484
P.T. EX.WLS-1	203+01.21	2,048,691.7133	961,729.2950
P.O.T. W251	208+12.88	2,048,694.1682	962,240.9620
EX. FRONTAGE ROAD			
P.O.T. AFRNT1	100+00.00	2,048,527.1662	961,471.9039
P.C. E.FRONT-1	100+70.63	2,048,469.2428	961,431.4825
P.I. E.FRONT-1	101+24.81	2,048,424.8112	961,400.4763
P.T. E.FRONT-1	101+47.30	2,048,405.7083	961,451.1777
P.C. E.FRONT-2	101+71.19	2,048,397.2854	961,473.5333
P.I. E.FRONT-2	101+94.94	2,048,388.9125	961,495.7559
P.T. E.FRONT-2	102+17.68	2,048,370.8694	961,511.1960
P.O.T. AFRNT2	118+85.22	2,047,103.9002	962,595.3841

LEGEND
 SURVEY CONTROL POINT (SEE SHEET 43 FOR SURVEY TIES)
 LABEL

FILE NAME = ...
 USER NAME = HECHTBR
 DESIGNED -
 DRAWN - AG
 CHECKED - BRH
 DATE - 11/01/13

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

ALIGNMENTS, TIES &
 BENCHMARKS - I

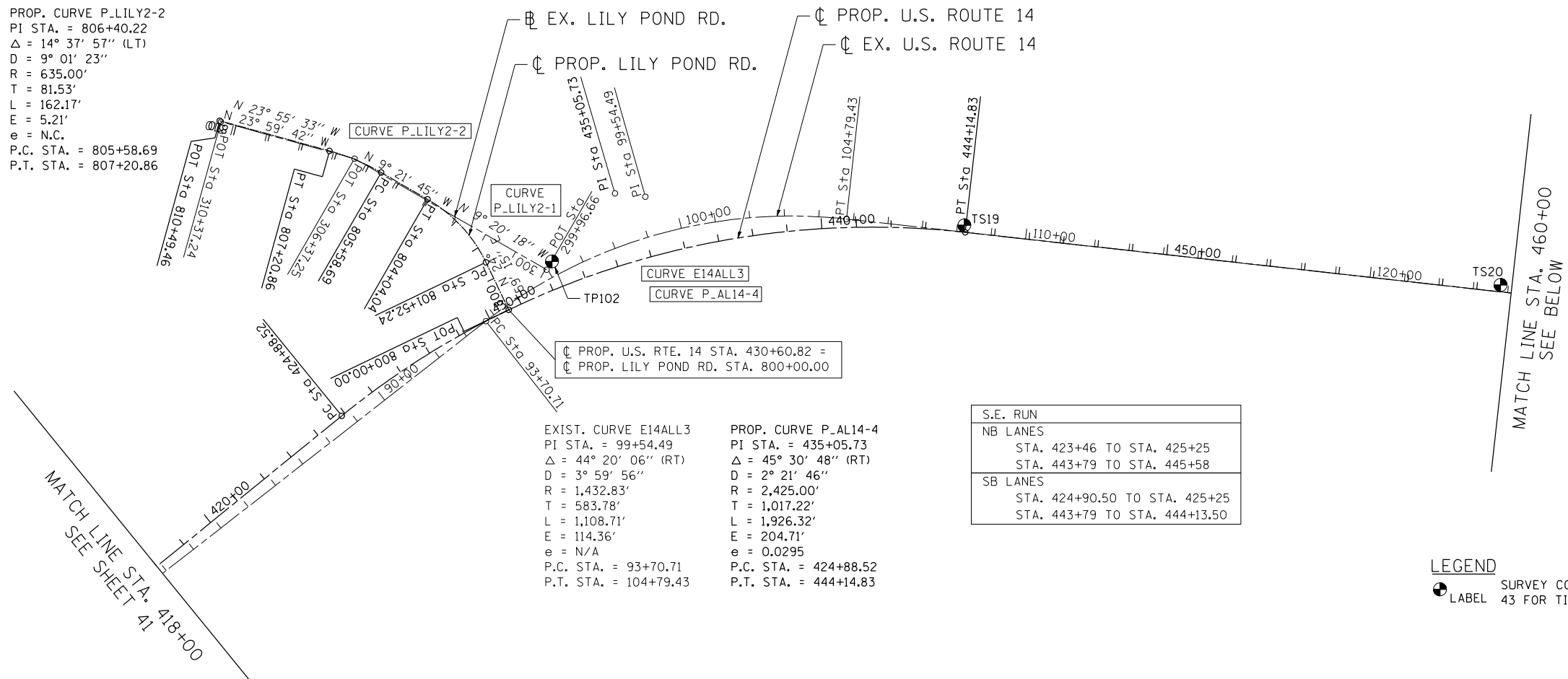
SCALE: 1"=200'
 SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	41

CONTRACT NO. 62268
 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

PROP. CURVE P.LILY2-1
 PI STA. = 802+82.05
 $\Delta = 34^\circ 21' 00''$ (LT)
 $D = 13^\circ 38' 31''$
 $R = 420.00'$
 $T = 129.81'$
 $L = 251.80'$
 $E = 19.60'$
 $e = \text{N.C.}$
 P.C. STA. = 801+52.24
 P.T. STA. = 804+04.04

PROP. CURVE P.LILY2-2
 PI STA. = 806+40.22
 $\Delta = 14^\circ 37' 57''$ (LT)
 $D = 9^\circ 01' 23''$
 $R = 635.00'$
 $T = 81.53'$
 $L = 162.17'$
 $E = 5.21'$
 $e = \text{N.C.}$
 P.C. STA. = 805+58.69
 P.T. STA. = 807+20.86



EXIST. CURVE E14ALL3
 PI STA. = 99+54.49
 $\Delta = 44^\circ 20' 06''$ (RT)
 $D = 3^\circ 59' 56''$
 $R = 1,432.83'$
 $T = 583.78'$
 $L = 1,108.71'$
 $E = 114.36'$
 $e = \text{N/A}$
 P.C. STA. = 93+70.71
 P.T. STA. = 104+79.43

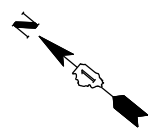
PROP. CURVE P.AL14-4
 PI STA. = 435+05.73
 $\Delta = 45^\circ 30' 48''$ (RT)
 $D = 2^\circ 21' 46''$
 $R = 2,425.00'$
 $T = 1,017.22'$
 $L = 1,926.32'$
 $E = 204.71'$
 $e = 0.0295$
 P.C. STA. = 424+88.52
 P.T. STA. = 444+14.83

S.E. RUN	
NB LANES	
STA. 423+46 TO STA. 425+25	
STA. 443+79 TO STA. 445+58	
SB LANES	
STA. 424+90.50 TO STA. 425+25	
STA. 443+79 TO STA. 444+13.50	

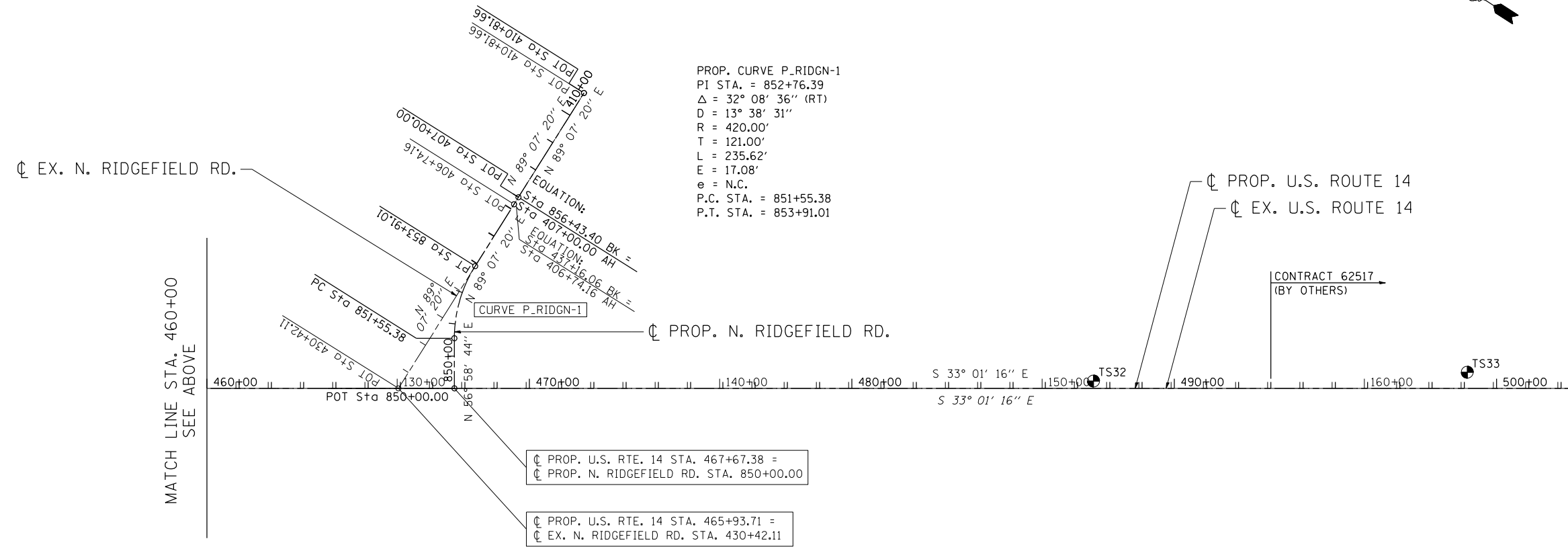
LEGEND
 SURVEY CONTROL POINT (SEE SHEET 43 FOR TIES)
 LABEL

MATCH LINE STA. 418+00
 SEE SHEET 41

MATCH LINE STA. 460+00
 SEE BELOW

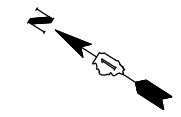


FILE NAME = ...
 USER NAME = HECHTBR
 DESIGNED -
 DRAWN - AG
 CHECKED - BRH
 DATE - 11/01/13



PROP. CURVE P.RIDGN-1
 PI STA. = 852+76.39
 $\Delta = 32^\circ 08' 36''$ (RT)
 $D = 13^\circ 38' 31''$
 $R = 420.00'$
 $T = 121.00'$
 $L = 235.62'$
 $E = 17.08'$
 $e = \text{N.C.}$
 P.C. STA. = 851+55.38
 P.T. STA. = 853+91.01

DESCRIPTION	STATION	NORTHING	EASTING
EX. U.S. ROUTE 14			
P.C. E14ALL3	93+70.71	2,042,864.2604	969,664.3578
P.I. E14ALL3	99+54.49	2,042,736.4771	970,233.9809
P.T. E14ALL3	104+79.43	2,042,246.9952	970,552.1106
P.C. E14ALL4	186+19.96	2,035,421.4054	974,988.2768
P.I. E14ALL4	191+16.09	2,035,005.4154	975,258.6419
P.T. E14ALL4	195+66.14	2,034,881.9824	975,739.1721
PROP. U.S. ROUTE 14			
P.C. P.AL14-4	424+88.52	2,043,012.5016	969,189.0822
P.I. P.AL14-4	435+05.73	2,042,810.2999	970,186.0012
P.T. P.AL14-4	444+14.83	2,041,957.3930	970,740.3322
P.O.T.	493+00.00	2,037,777.4795	973,456.9900
EX. LILY POND RD.			
P.O.T. A300	299+96.66	2,042,823.0450	969,890.3882
P.O.T. A301	306+37.25	2,043,455.1409	969,786.4437
P.O.T. A302	310+37.24	2,043,820.7601	969,624.2255
PROP. LILY POND RD.			
P.O.T. LILLY1000	800+00.00	2,042,833.9139	969,731.4130
P.C. P.LILY2-1	801+52.24	2,042,971.9052	969,795.7228
P.I. P.LILY2-1	802+82.05	2,043,089.5662	969,850.5578
P.T. P.LILY2-1	804+04.04	2,043,217.6483	969,829.4398
P.C. P.LILY2-2	805+58.69	2,043,370.2425	969,804.2803
P.I. P.LILY2-2	806+40.22	2,043,450.6842	969,791.0172
P.T. P.LILY2-2	807+20.86	2,043,525.1665	969,757.8633
P.O.T. LILLY1001	810+49.46	2,043,825.3620	969,624.2390
EX. RIDGEFIELD RD.			
P.O.T. A6	430+42.11	2,040,130.4691	971,927.7078
P.O.T. A7	406+74.16	2,040,140.7942	972,601.5789
P.O.T. A8	410+81.66	2,040,147.0375	973,009.0332
PROP. RIDGEFIELD RD.			
P.O.T. A850	850+00.00	2,039,984.8541	972,022.3471
P.C. P.RIDGN-1	851+55.38	2,040,069.5303	972,152.6318
P.I. P.RIDGN-1	852+76.39	2,040,135.4698	972,254.0880
P.T. P.RIDGN-1	853+91.01	2,040,137.3236	972,375.0753
P.O.T. A851	407+00.00	2,040,141.1901	972,627.4193
P.O.T. A852	410+81.66	2,040,147.0375	973,009.0332



MATCH LINE STA. 460+00
 SEE ABOVE

CONTRACT 62517
 (BY OTHERS)

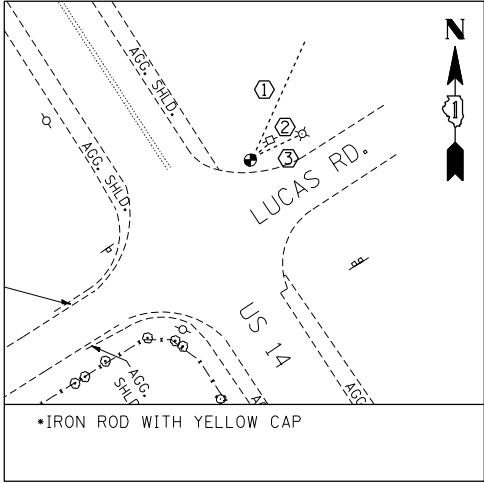
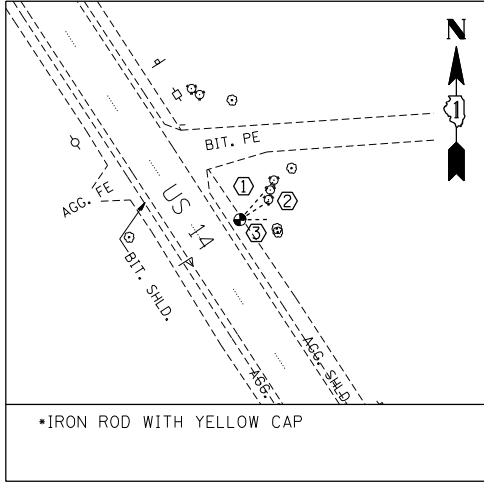
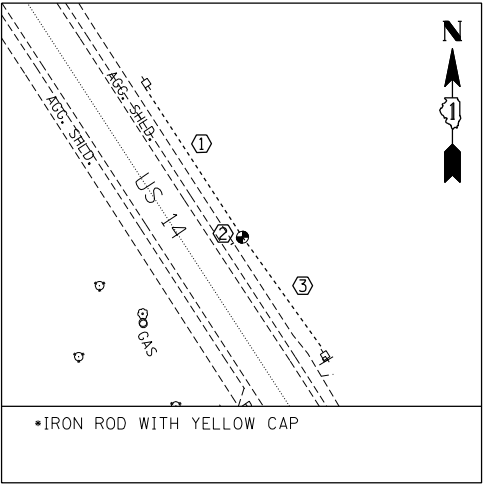
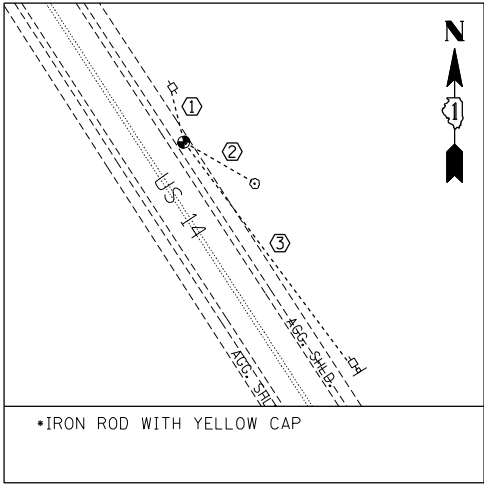
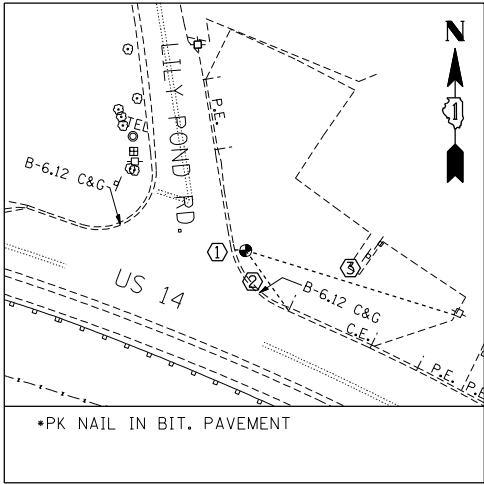
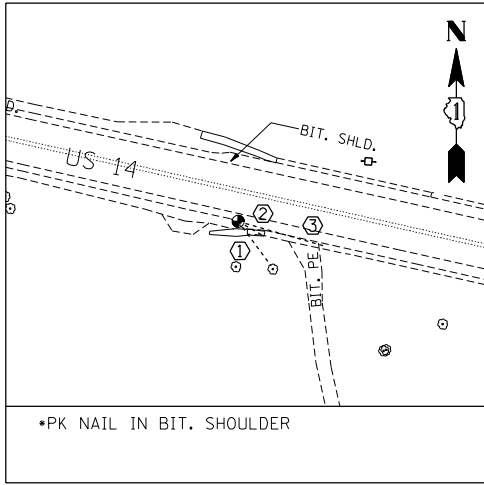
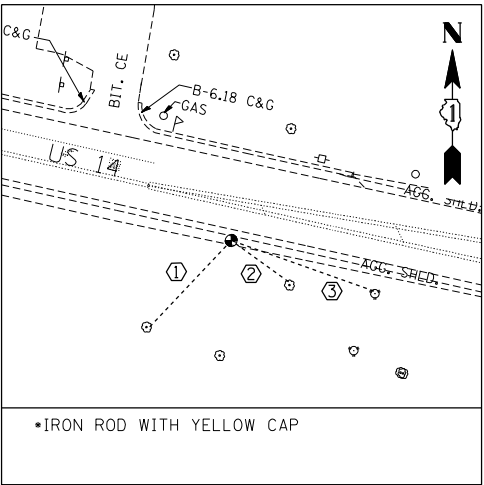
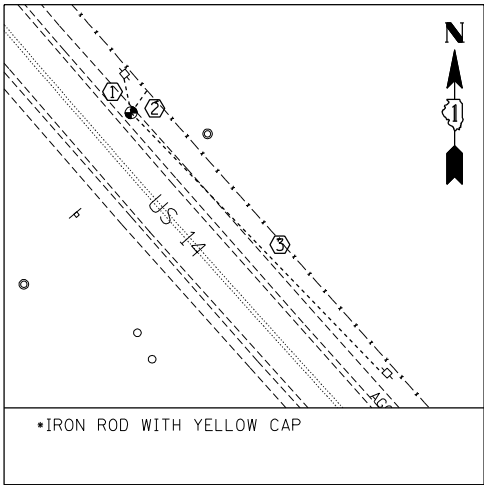
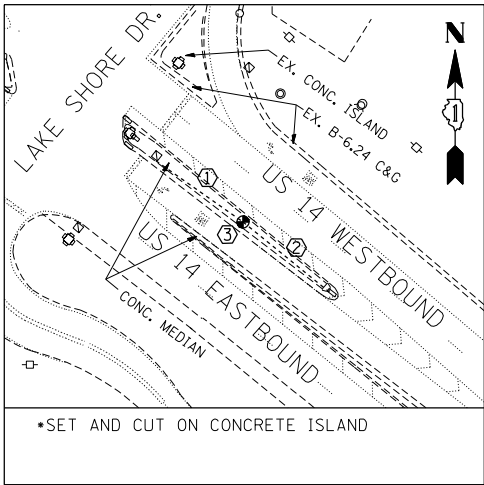
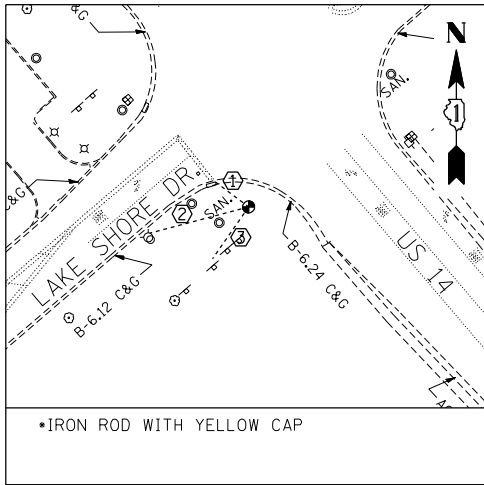
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

ALIGNMENTS, TIES & BENCHMARKS - II

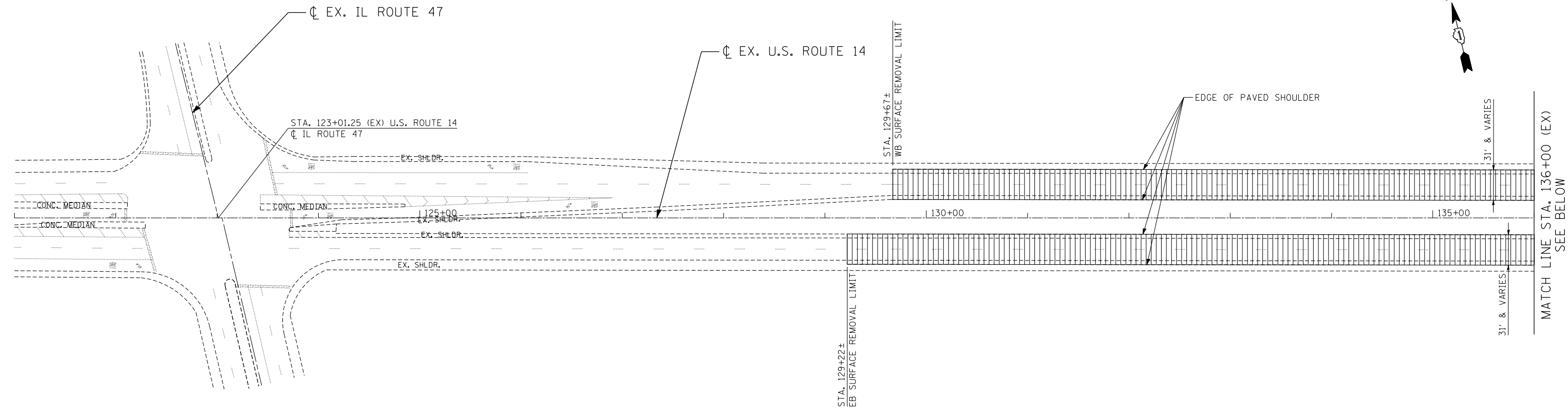
SCALE: 1"=200' SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	42
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

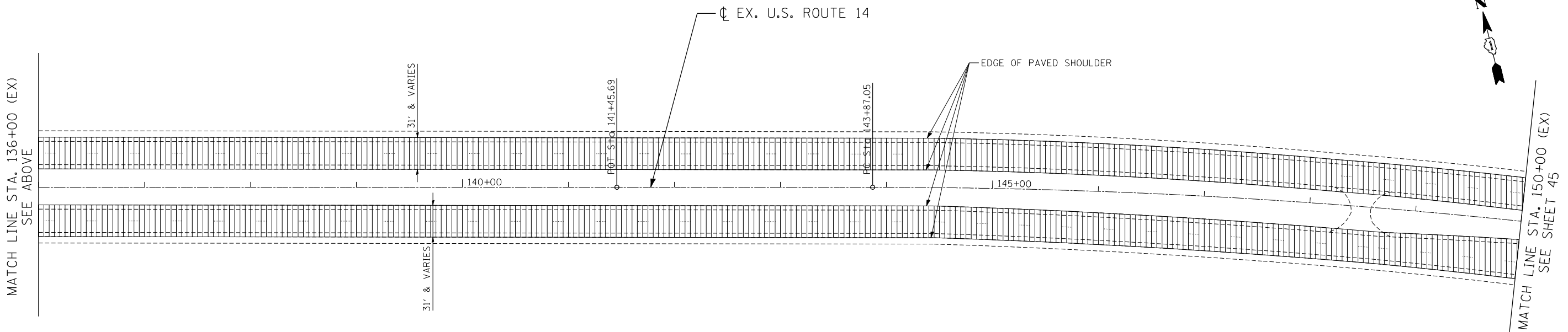
FILE NAME = T:\DOCUMENT\19349901\CIVIL\DRAWING\100_1A.DGN
 USER NAME = HECHTBR
 DESIGNED -
 DRAWN - AG
 CHECKED - BRH
 DATE - 11/01/13
 REVISED -
 REVISED -
 REVISED -
 REVISED -
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14
 ALIGNMENTS, TIES & BENCHMARKS - III
 SCALE: 1"=200'
 SHEET NO. OF SHEETS STA. TO STA.
 F.A.P. R.T.E. 305 SECTION 27R-2 COUNTY MCHENRY TOTAL SHEETS 673 SHEET NO. 43 CONTRACT NO. 62268
 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



FILE NAME = *FILEL* 	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	ALIGNMENTS, TIES & BENCHMARKS - III	F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = *SCALE*	CHECKED - BRH	REVISIONS -			305	27R-2	MCHENRY	673	43
	PLOT DATE = *DATE*	DATE - 11/01/13	REVISIONS -			CONTRACT NO. 62268				
						SCALE: 1"=200'		SHEET NO. OF SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



MATCH LINE STA. 136+00 (EX)
SEE BELOW



MATCH LINE STA. 136+00 (EX)
SEE ABOVE

MATCH LINE STA. 150+00 (EX)
SEE SHEET 45

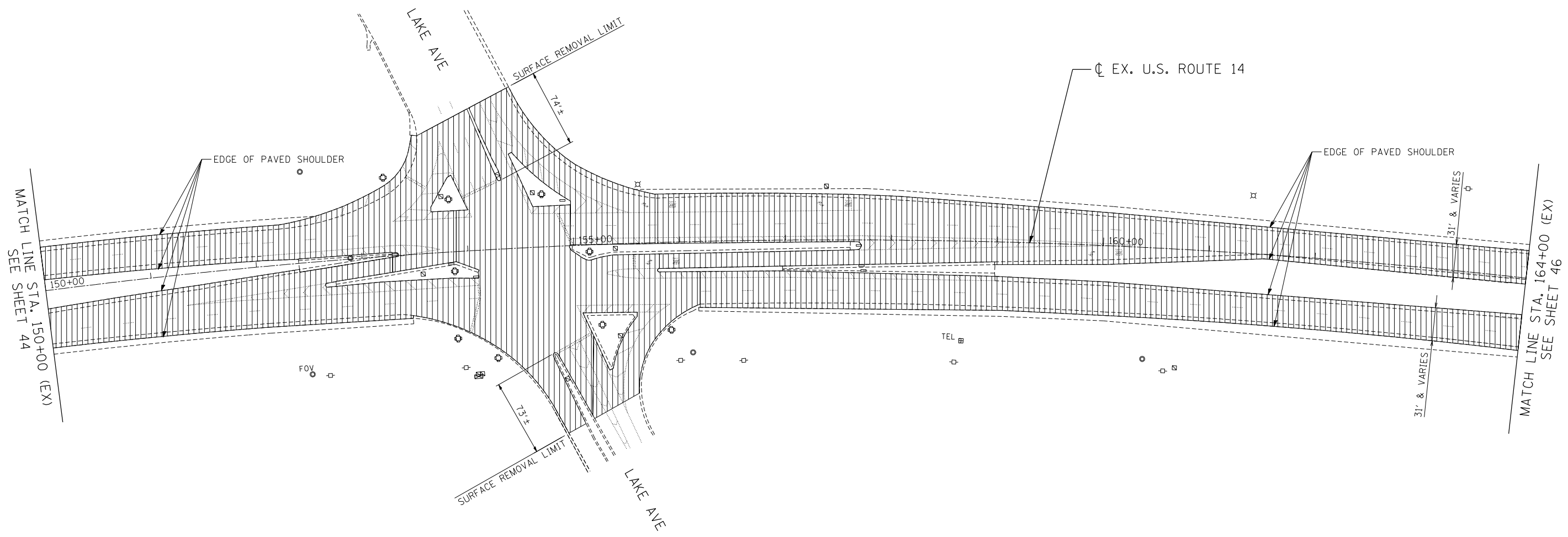
REMOVAL LEGEND

	PAVEMENT REMOVAL		HMA SURFACE REMOVAL (VAR. DEPTH)		LINEAR REMOVALS
	PAVED SHOULDER REMOVAL		HMA SURFACE REMOVAL (2 1/4")		STORM SEWER TO BE REMOVED
	ISLAND PAVEMENT REMOVAL		MEDIAN REMOVAL		PIPE CULVERT REMOVAL (INCLUDING FLARED END SECTION)
	CONCRETE MEDIAN SURFACE REMOVAL		TREE REMOVAL (WITH DIA. INCHES)		DRAINAGE STRUCTURE TO BE REMOVED
	DRIVEWAY PAVEMENT REMOVAL		BUSH / SHRUB REMOVAL (REMOVAL INCLUDED IN COST OF EARTHWORK)		DRAINAGE STRUCTURE TO BE ADJUSTED

NOTES:
1. SEE GENERAL NOTE 35 FROM SHEET 3.

FILE NAME = ...
 USER NAME = HECHTBR
 DESIGNED -
 DRAWN - JP
 CHECKED - KA
 DATE - 11/01/13
 REVISED -
 REVISED -
 REVISED -
 REVISED -
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14
 EXISTING ROADWAY & REMOVAL PLANS
 STA. 121+00 (EX) TO STA. 150+00 (EX)
 SCALE: 1"=50'
 SHEET NO. OF SHEETS STA. TO STA.
 F.A.P. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
 305 27R-2 MCHENRY 673 44
 CONTRACT NO. 62268
 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	EXISTING ROADWAY & REMOVAL PLANS STA. 121+00 (EX) TO STA. 150+00 (EX)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
FILEL		DRAWN - JP	REVISED -			305	27R-2	MCHENRY	673	44	
exp U.S. Services Inc. Chicago, IL	PLOT SCALE = *SCALE*	CHECKED - KA	REVISED -			CONTRACT NO. 62268					
11-11-2013 14:18:10	PLOT DATE = *DATE*	DATE - 11/01/13	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



REMOVAL LEGEND

	PAVEMENT REMOVAL		HMA SURFACE REMOVAL (VAR. DEPTH)		LINEAR REMOVALS
	PAVED SHOULDER REMOVAL		HMA SURFACE REMOVAL (2 1/4")		STORM SEWER TO BE REMOVED
	ISLAND PAVEMENT REMOVAL		MEDIAN REMOVAL		PIPE CULVERT REMOVAL (INCLUDING FLARED END SECTION)
	CONCRETE MEDIAN SURFACE REMOVAL		TREE REMOVAL (WITH DIA. INCHES)		DRAINAGE STRUCTURE TO BE REMOVED
	DRIVEWAY PAVEMENT REMOVAL		BUSH / SHRUB REMOVAL (REMOVAL INCLUDED IN COST OF EARTHWORK)		DRAINAGE STRUCTURE TO BE ADJUSTED

NOTES:
1. SEE GENERAL NOTE 35 FROM SHEET 3.

\\PDR008\15.DGN, \\T1B008\11.DGN, \\GEN008\77.DGN, \\REM008\20.DGN, \\11-11-2013, 14:18:15
 HECHTBR T:\DOCUMENT\1934\981\CIVIL\DCN\REM008_ID.DGN

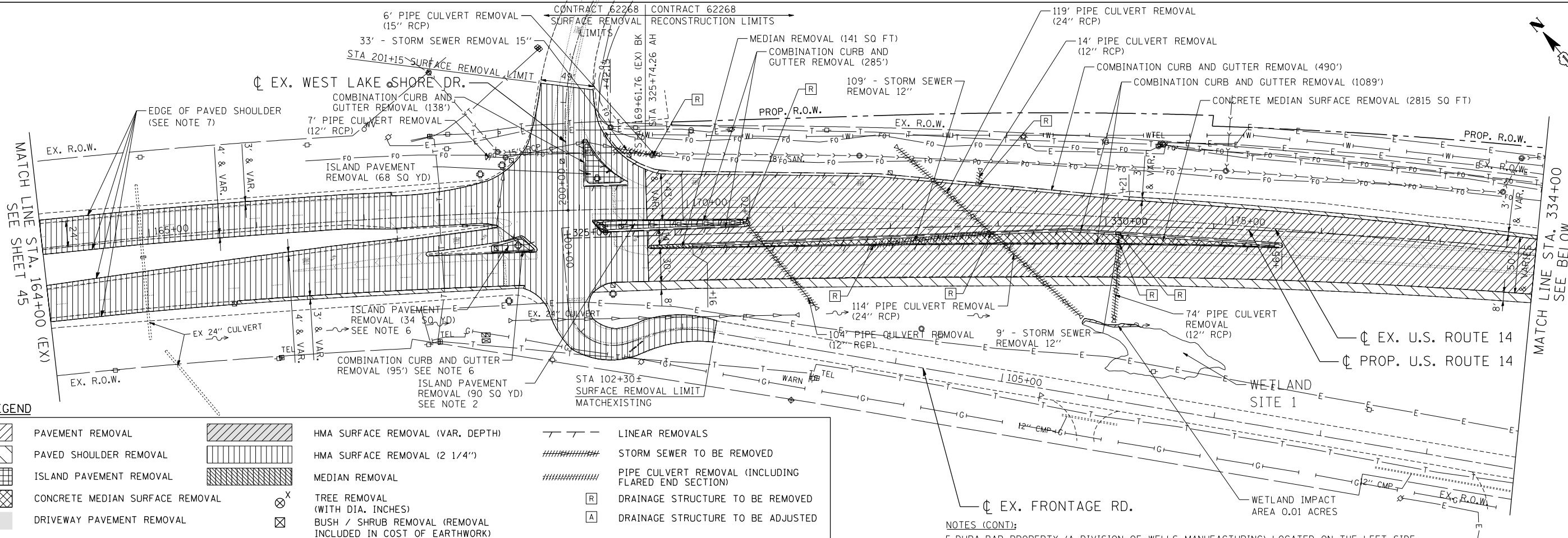
FILE NAME =
 FILEL
 exp U.S. Services Inc.
 BUILDINGS-EARTH & ENVIRONMENT-ENERGY
 INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY

USER NAME = HECHTBR	DESIGNED -	REVISED -
PLOT SCALE = *SCALE*	DRAWN - JP	REVISED -
PLOT DATE = *DATE*	CHECKED - KA	REVISED -
	DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

EXISTING ROADWAY & REMOVAL PLANS STA. 150+00 (EX) TO STA. 164+00 (EX)			
SCALE: 1"=50'	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	45
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62268	



REMOVAL LEGEND

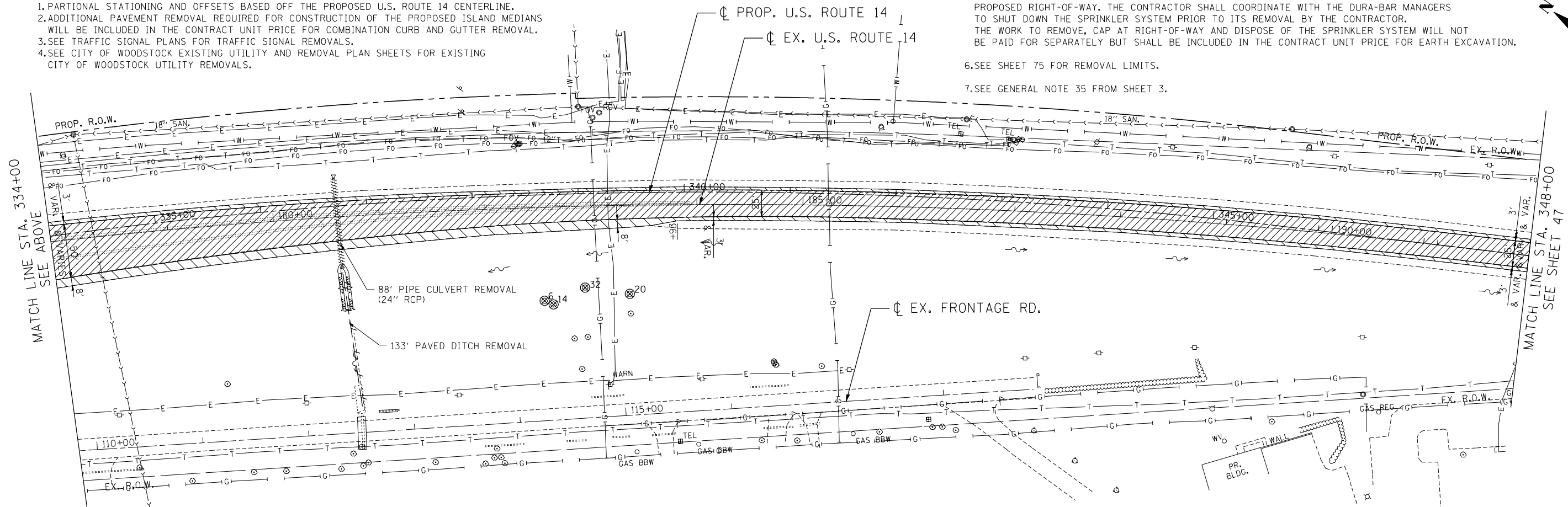
	PAVEMENT REMOVAL		HMA SURFACE REMOVAL (VAR. DEPTH)		LINEAR REMOVALS
	PAVED SHOULDER REMOVAL		HMA SURFACE REMOVAL (2 1/4")		STORM SEWER TO BE REMOVED
	ISLAND PAVEMENT REMOVAL		MEDIAN REMOVAL		PIPE CULVERT REMOVAL (INCLUDING FLARED END SECTION)
	CONCRETE MEDIAN SURFACE REMOVAL		TREE REMOVAL (WITH DIA. INCHES)		DRAINAGE STRUCTURE TO BE REMOVED
	DRIVEWAY PAVEMENT REMOVAL		BUSH / SHRUB REMOVAL (REMOVAL INCLUDED IN COST OF EARTHWORK)		DRAINAGE STRUCTURE TO BE ADJUSTED

NOTES:

- PARTIAL STATIONING AND OFFSETS BASED OFF THE PROPOSED U.S. ROUTE 14 CENTERLINE.
- ADDITIONAL PAVEMENT REMOVAL REQUIRED FOR CONSTRUCTION OF THE PROPOSED ISLAND MEDIANS WILL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR COMBINATION CURB AND GUTTER REMOVAL.
- SEE TRAFFIC SIGNAL PLANS FOR TRAFFIC SIGNAL REMOVALS.
- SEE CITY OF WOODSTOCK EXISTING UTILITY AND REMOVAL PLAN SHEETS FOR EXISTING CITY OF WOODSTOCK UTILITY REMOVALS.

NOTES (CONT):

- DURA-BAR PROPERTY (A DIVISION OF WELLS MANUFACTURING) LOCATED ON THE LEFT SIDE FROM STA. 331+12 TO 342+78 HAS AN EXISTING SPRINKLER SYSTEM LOCATED WITHIN THE PROPOSED RIGHT-OF-WAY. THE CONTRACTOR SHALL COORDINATE WITH THE DURA-BAR MANAGERS TO SHUT DOWN THE SPRINKLER SYSTEM PRIOR TO ITS REMOVAL BY THE CONTRACTOR. THE WORK TO REMOVE, CAP AT RIGHT-OF-WAY AND DISPOSE OF THE SPRINKLER SYSTEM WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR EARTH EXCAVATION.
- SEE SHEET 75 FOR REMOVAL LIMITS.
- SEE GENERAL NOTE 35 FROM SHEET 3.



FILE NAME = USER NAME = HECHTBR DESIGNED - REVISED -
 #FILEL# DRAWN - RJS REVISED -
 exp U.S. Services Inc. CHECKED - BRH REVISED -
 BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY DATE - 11/01/13 REVISED -
 11-07-2013, 16:20:55

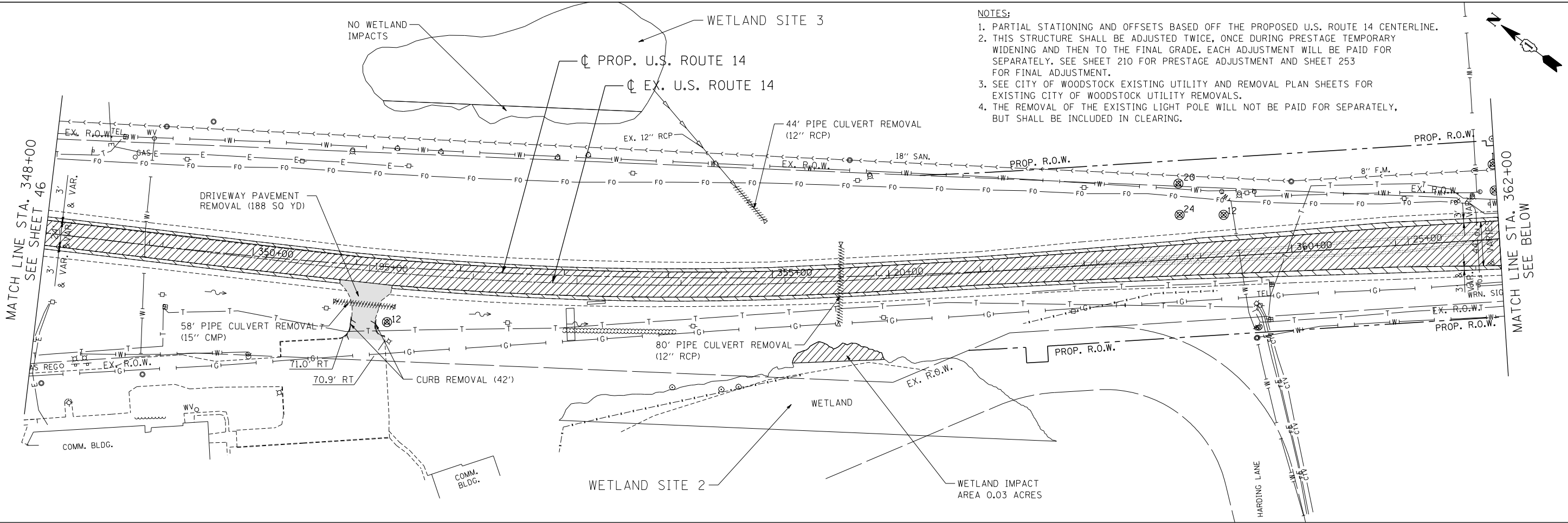
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
U.S. ROUTE 14**

**EXISTING ROADWAY & REMOVAL PLANS
STA. 164+00 (EX) TO STA. 348+00**

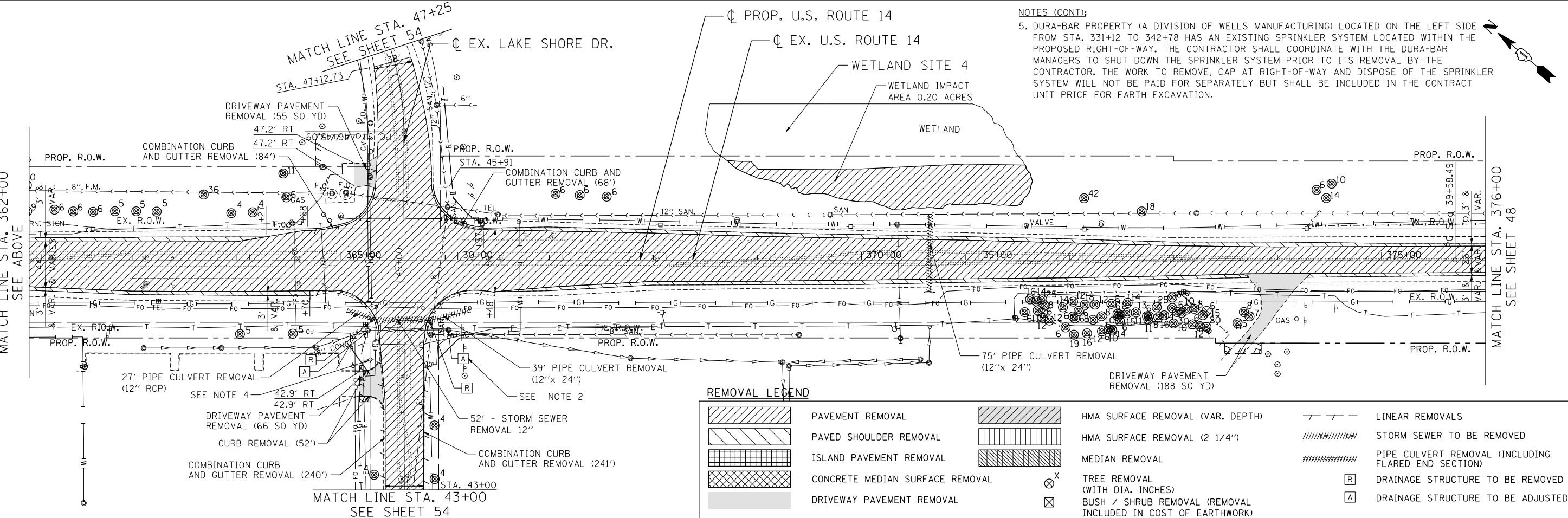
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	46
CONTRACT NO. 62268				

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



- NOTES:
- PARTIAL STATIONING AND OFFSETS BASED OFF THE PROPOSED U.S. ROUTE 14 CENTERLINE.
 - THIS STRUCTURE SHALL BE ADJUSTED TWICE, ONCE DURING PRESTAGE TEMPORARY WIDENING AND THEN TO THE FINAL GRADE. EACH ADJUSTMENT WILL BE PAID FOR SEPARATELY. SEE SHEET 210 FOR PRESTAGE ADJUSTMENT AND SHEET 253 FOR FINAL ADJUSTMENT.
 - SEE CITY OF WOODSTOCK EXISTING UTILITY AND REMOVAL PLAN SHEETS FOR EXISTING CITY OF WOODSTOCK UTILITY REMOVALS.
 - THE REMOVAL OF THE EXISTING LIGHT POLE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN CLEARING.



- NOTES (CONT):
- DURA-BAR PROPERTY (A DIVISION OF WELLS MANUFACTURING) LOCATED ON THE LEFT SIDE FROM STA. 331+12 TO 342+78 HAS AN EXISTING SPRINKLER SYSTEM LOCATED WITHIN THE PROPOSED RIGHT-OF-WAY. THE CONTRACTOR SHALL COORDINATE WITH THE DURA-BAR MANAGERS TO SHUT DOWN THE SPRINKLER SYSTEM PRIOR TO ITS REMOVAL BY THE CONTRACTOR. THE WORK TO REMOVE, CAP AT RIGHT-OF-WAY AND DISPOSE OF THE SPRINKLER SYSTEM WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR EARTH EXCAVATION.

REMOVAL LEGEND

	PAVEMENT REMOVAL		HMA SURFACE REMOVAL (VAR. DEPTH)		LINEAR REMOVALS
	PAVED SHOULDER REMOVAL		HMA SURFACE REMOVAL (2 1/4")		STORM SEWER TO BE REMOVED
	ISLAND PAVEMENT REMOVAL		MEDIAN REMOVAL		PIPE CULVERT TO BE REMOVED (INCLUDING FLARED END SECTION)
	CONCRETE MEDIAN SURFACE REMOVAL		TREE REMOVAL (WITH DIA. INCHES)		DRAINAGE STRUCTURE TO BE REMOVED
	DRIVEWAY PAVEMENT REMOVAL		BUSH / SHRUB REMOVAL (REMOVAL INCLUDED IN COST OF EARTHWORK)		DRAINAGE STRUCTURE TO BE ADJUSTED

FILE NAME = ...
 USER NAME = HECHTBR
 DESIGNED -
 DRAWN - RJS
 CHECKED - BRH
 DATE - 11/01/13
 REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
U.S. ROUTE 14**

**EXISTING ROADWAY &
REMOVAL PLANS
STA. 348+00 TO STA. 376+00**

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

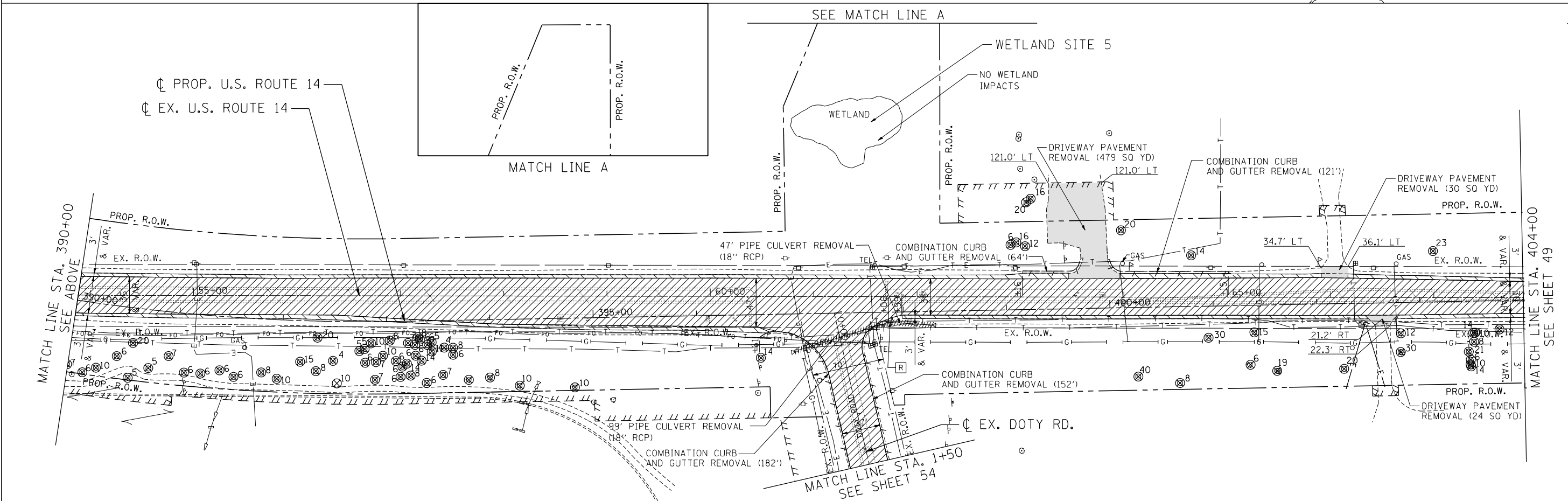
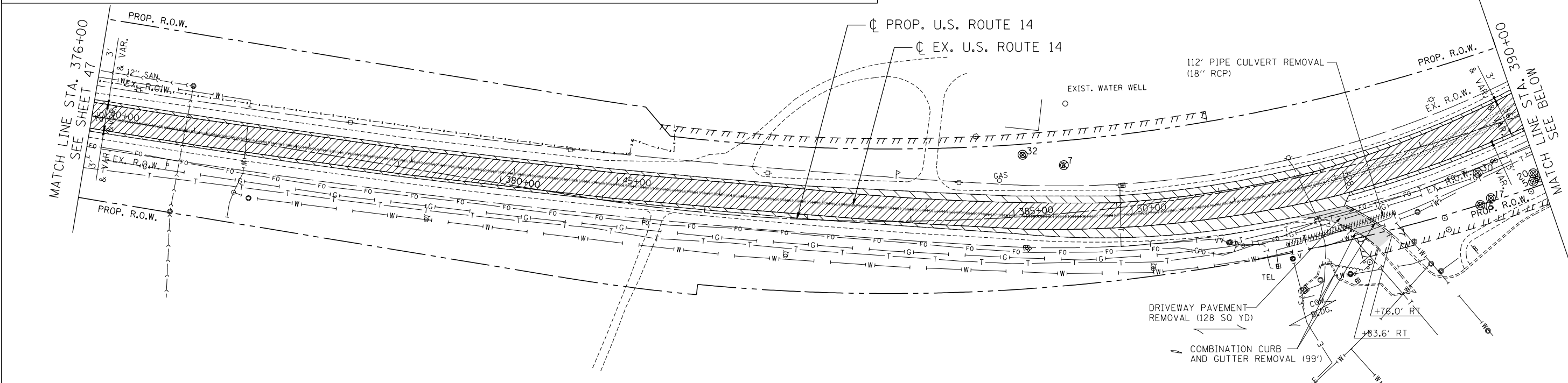
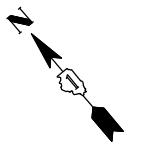
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	47
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62268	

REMOVAL LEGEND

	PAVEMENT REMOVAL		HMA SURFACE REMOVAL (VAR. DEPTH)		LINEAR REMOVALS
	PAVED SHOULDER REMOVAL		HMA SURFACE REMOVAL (2 1/4")		STORM SEWER TO BE REMOVED
	ISLAND PAVEMENT REMOVAL		MEDIAN REMOVAL		PIPE CULVERT REMOVAL (INCLUDING FLARED END SECTION)
	CONCRETE MEDIAN SURFACE REMOVAL		TREE REMOVAL (WITH DIA. INCHES)		DRAINAGE STRUCTURE TO BE REMOVED
	DRIVEWAY PAVEMENT REMOVAL		BUSH / SHRUB REMOVAL (REMOVAL INCLUDED IN COST OF EARTHWORK)		DRAINAGE STRUCTURE TO BE ADJUSTED

NOTES:

- PARTIAL STATIONING AND OFFSETS BASED OFF THE PROPOSED U.S. ROUTE 14 CENTERLINE.
- SEE TRAFFIC SIGNAL PLANS FOR TRAFFIC SIGNAL REMOVALS.
- SEE CITY OF WOODSTOCK EXISTING UTILITY AND REMOVAL PLAN SHEETS FOR EXISTING CITY OF WOODSTOCK UTILITY REMOVALS.



FILE NAME = ... USER NAME = HECHTBR ... DESIGNED - ... REVISED - ... DRAWN - RJS ... REVISED - ... CHECKED - BRH ... REVISED - ... DATE - 11/01/13 ... REVISED - ...

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

EXISTING ROADWAY & REMOVAL PLANS
STA. 376+00 TO STA. 404+00

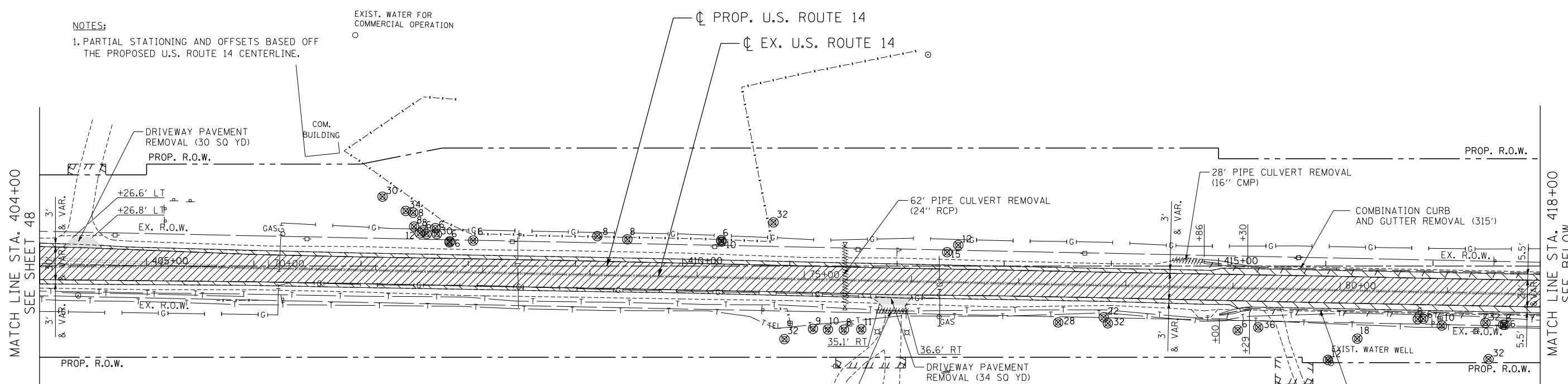
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	48
CONTRACT NO. 62268				

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

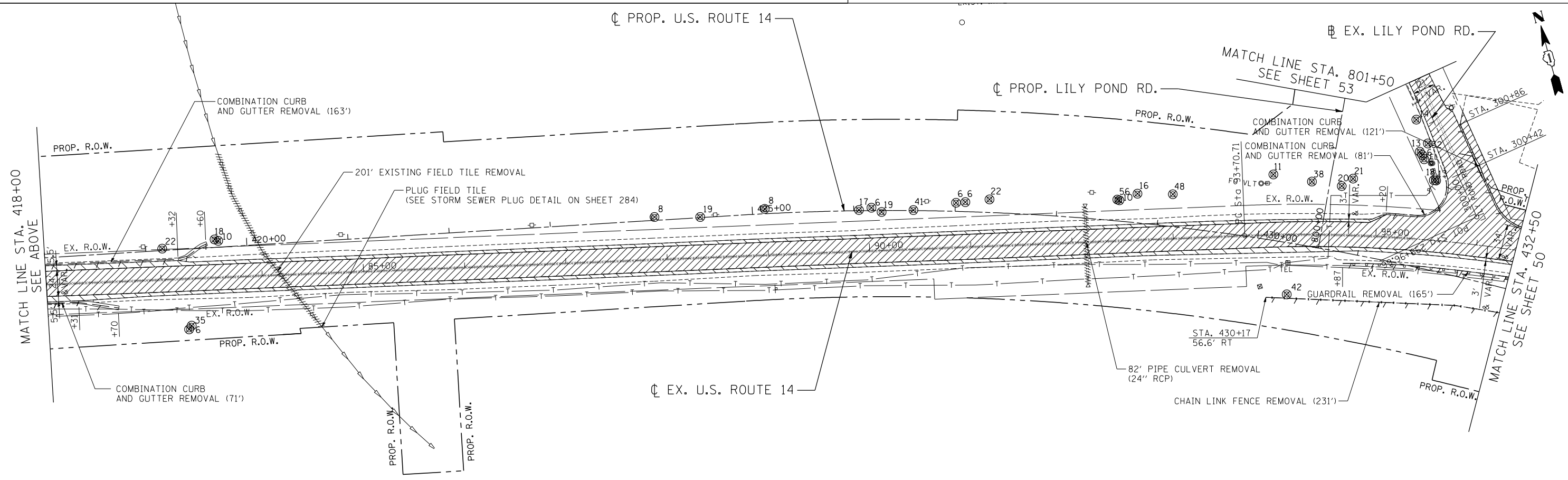
NOTES:
1. PARTIAL STATIONING AND OFFSETS BASED OFF THE PROPOSED U.S. ROUTE 14 CENTERLINE.

EXIST. WATER FOR COMMERCIAL OPERATION



REMOVAL LEGEND

	PAVEMENT REMOVAL		HMA SURFACE REMOVAL (VAR. DEPTH)		LINEAR REMOVALS
	PAVED SHOULDER REMOVAL		HMA SURFACE REMOVAL (2 1/4")		STORM SEWER TO BE REMOVED
	ISLAND PAVEMENT REMOVAL		MEDIAN REMOVAL		PIPE CULVERT REMOVAL (INCLUDING FLARED END SECTION)
	CONCRETE MEDIAN SURFACE REMOVAL		TREE REMOVAL (WITH DIA. INCHES)		DRAINAGE STRUCTURE TO BE REMOVED
	DRIVEWAY PAVEMENT REMOVAL		BUSH / SHRUB REMOVAL (REMOVAL INCLUDED IN COST OF EARTHWORK)		DRAINAGE STRUCTURE TO BE ADJUSTED



FILE NAME = ... USER NAME = HECHTBR ... DESIGNED - ... REVISED - ... DRAWN - RJS ... REVISED - ... CHECKED - BRH ... REVISED - ... DATE - 11/01/13 ... REVISED - ...

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

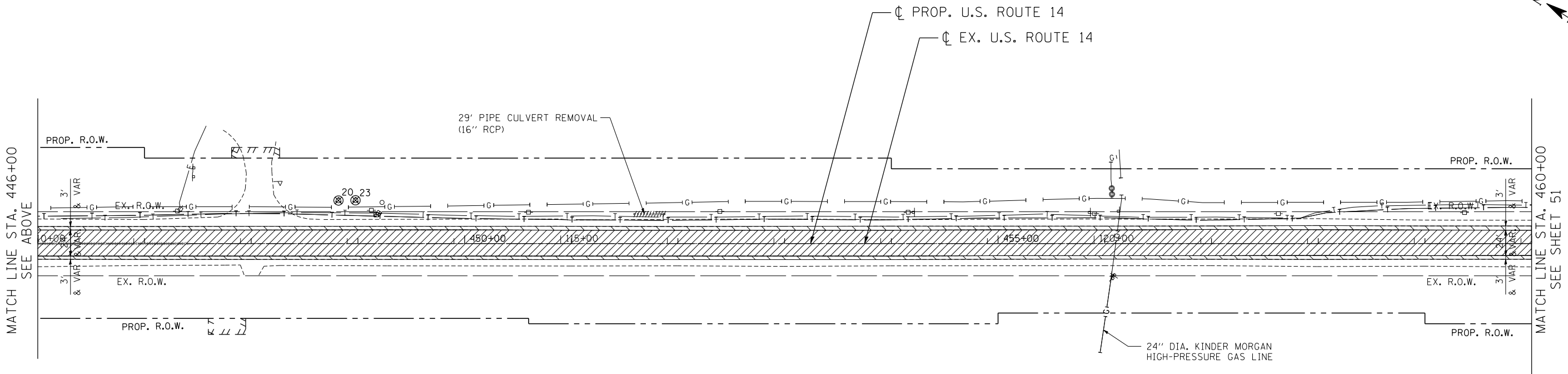
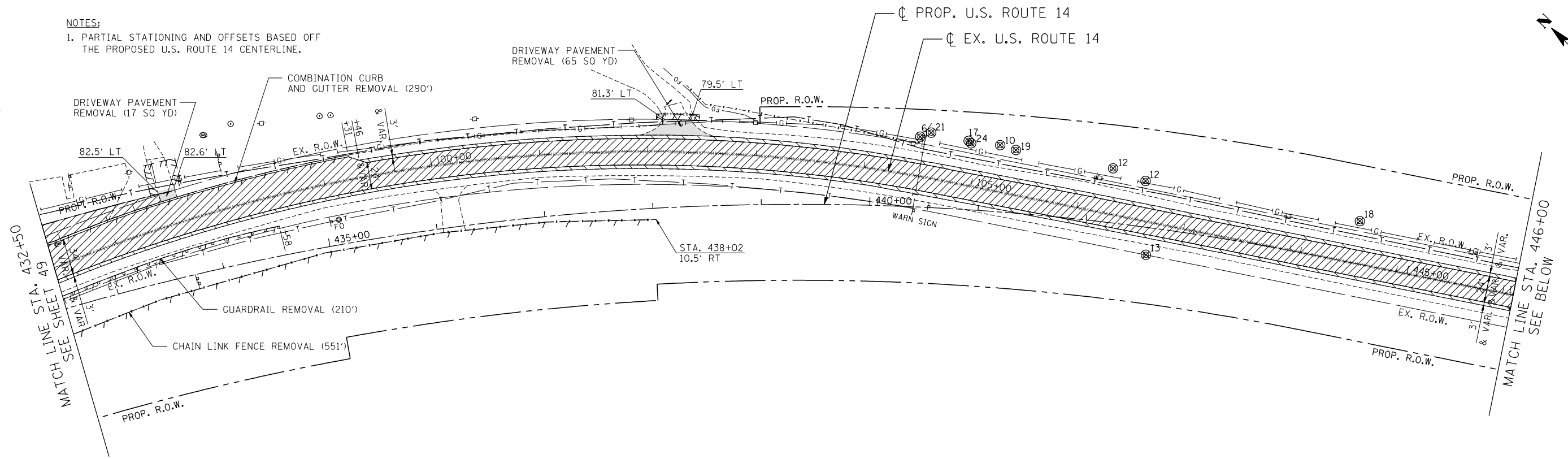
EXISTING ROADWAY & REMOVAL PLANS
STA. 404+00 TO STA. 432+50

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	49
CONTRACT NO. 62268				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

NOTES:
1. PARTIAL STATIONING AND OFFSETS BASED OFF THE PROPOSED U.S. ROUTE 14 CENTERLINE.



REMOVAL LEGEND

	PAVEMENT REMOVAL		HMA SURFACE REMOVAL (VAR. DEPTH)		LINEAR REMOVALS
	PAVED SHOULDER REMOVAL		HMA SURFACE REMOVAL (2 1/4\")		STORM SEWER TO BE REMOVED
	ISLAND PAVEMENT REMOVAL		MEDIAN REMOVAL		PIPE CULVERT REMOVAL (INCLUDING FLARED END SECTION)
	CONCRETE MEDIAN SURFACE REMOVAL		TREE REMOVAL (WITH DIA. INCHES)		DRAINAGE STRUCTURE TO BE REMOVED
	DRIVEWAY PAVEMENT REMOVAL		BUSH / SHRUB REMOVAL (REMOVAL INCLUDED IN COST OF EARTHWORK)		DRAINAGE STRUCTURE TO BE ADJUSTED

\P\RD\00815.DGN, \GEN\00817.DGN, \RD\00815.DGN, \TTB\00817.DGN, \T:\DOCUMENT\1934\9901\CIVIL\DCN\REM\00817.DGN
 11-07-2013, 16:21:44
 HECHTBR

FILE NAME = #FILEL#	USER NAME = HECHTBR	DESIGNED -	REVISED -
exp U.S. Services Inc. Chicago, IL BUILDING-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY		DRAWN - RJS	REVISED -
	PLOT SCALE = #SCALE#	CHECKED - BRH	REVISED -
	PLOT DATE = #DATE#	DATE - 11/01/13	REVISED -

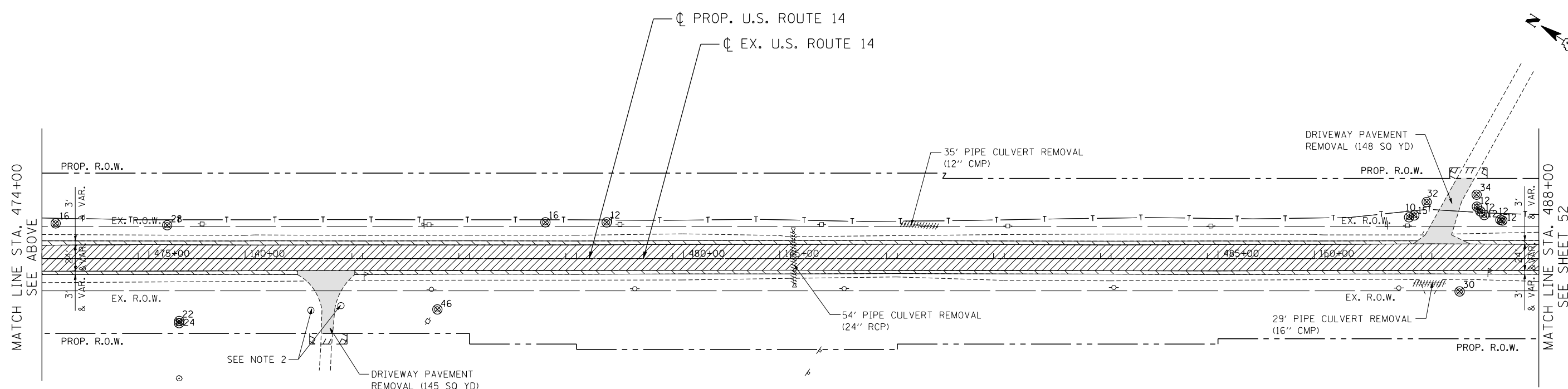
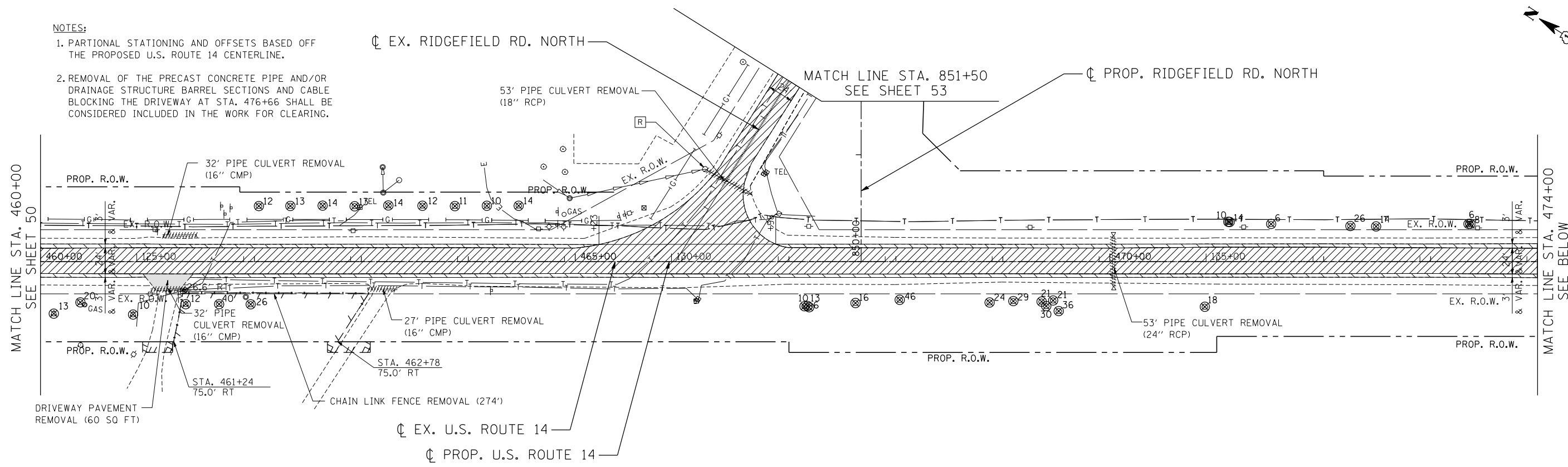
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

EXISTING ROADWAY & REMOVAL PLANS			
STA. 432+50 TO STA. 460+00			
SCALE: 1"=50'	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	50
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

NOTES:

- PARTIAL STATIONING AND OFFSETS BASED OFF THE PROPOSED U.S. ROUTE 14 CENTERLINE.
- REMOVAL OF THE PRECAST CONCRETE PIPE AND/OR DRAINAGE STRUCTURE BARREL SECTIONS AND CABLE BLOCKING THE DRIVEWAY AT STA. 476+66 SHALL BE CONSIDERED INCLUDED IN THE WORK FOR CLEARING.



REMOVAL LEGEND

	PAVEMENT REMOVAL		HMA SURFACE REMOVAL (VAR. DEPTH)		LINEAR REMOVALS
	PAVED SHOULDER REMOVAL		HMA SURFACE REMOVAL (2 1/4")		STORM SEWER TO BE REMOVED
	ISLAND PAVEMENT REMOVAL		MEDIAN REMOVAL		PIPE CULVERT REMOVAL (INCLUDING FLARED END SECTION)
	CONCRETE MEDIAN SURFACE REMOVAL		TREE REMOVAL (WITH DIA. INCHES)		DRAINAGE STRUCTURE TO BE REMOVED
	DRIVEWAY PAVEMENT REMOVAL		BUSH / SHRUB REMOVAL (REMOVAL INCLUDED IN COST OF EARTHWORK)		DRAINAGE STRUCTURE TO BE ADJUSTED

\P\RD\00815.DGN, \GEN\00815.DGN, \RD\00815.DGN, \TTB\00815.DGN, \T\00815.DGN, \CIVIL\00815.DGN, \R\00815.DGN
 11-07-2013, 16:21:55
 HECHTBR

FILE NAME =
 #FILE#
 exp U.S. Services Inc.
 BUILDING-EARTH & ENVIRONMENT-ENERGY
 INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY

USER NAME = HECHTBR	DESIGNED -	REVISED -
PLOT SCALE = #SCALE#	DRAWN - RJS	REVISED -
PLOT DATE = #DATE#	CHECKED - BRH	REVISED -
	DATE - 11/01/13	REVISED -

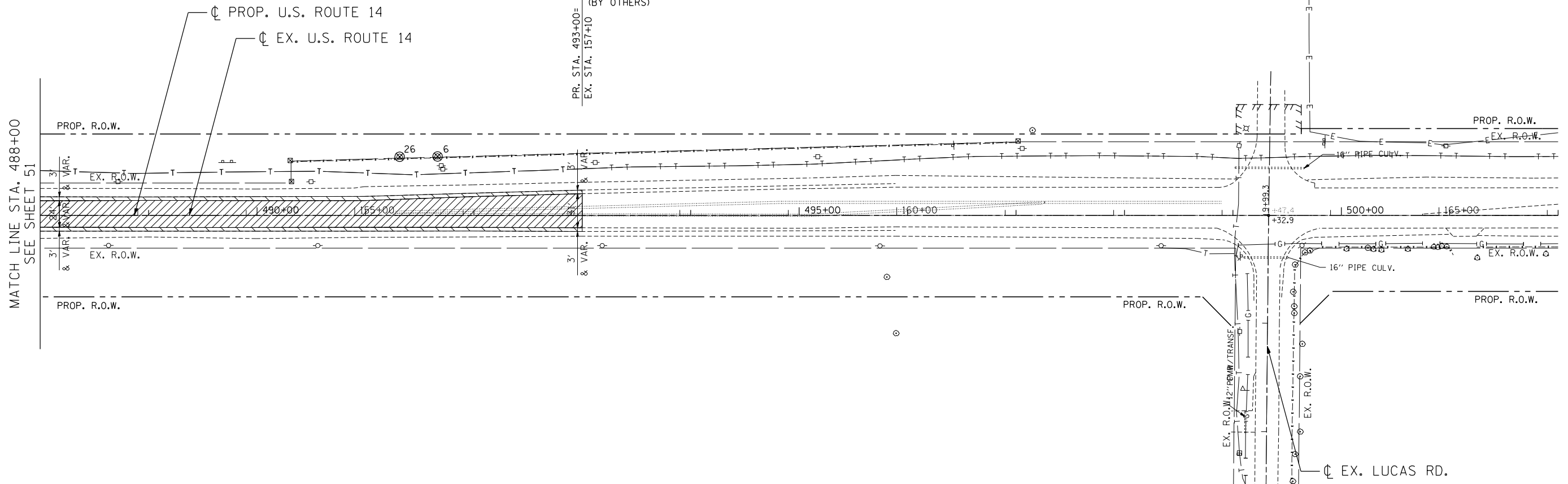
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

EXISTING ROADWAY & REMOVAL PLANS
STA. 460+00 TO STA. 488+00
 SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	51
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

NOTES:

1. PARTIAL STATIONING AND OFFSETS BASED OFF THE PROPOSED U.S. ROUTE 14 CENTERLINE.



REMOVAL LEGEND

	PAVEMENT REMOVAL		HMA SURFACE REMOVAL (VAR. DEPTH)		LINEAR REMOVALS
	PAVED SHOULDER REMOVAL		HMA SURFACE REMOVAL (2 1/4")		STORM SEWER TO BE REMOVED
	ISLAND PAVEMENT REMOVAL		MEDIAN REMOVAL		PIPE CULVERT REMOVAL (INCLUDING FLARED END SECTION)
	CONCRETE MEDIAN SURFACE REMOVAL		TREE REMOVAL (WITH DIA. INCHES)		DRAINAGE STRUCTURE TO BE REMOVED
	DRIVEWAY PAVEMENT REMOVAL		BUSH / SHRUB REMOVAL (REMOVAL INCLUDED IN COST OF EARTHWORK)		DRAINAGE STRUCTURE TO BE ADJUSTED

\\PDR008\15.DGN, \\GEM008\17.DGN, \\TTB008\J.DGN, \\HECHTBR\1\DOCUMENT\1934\9901\CIVIL\DCN\REM\2008\KS.DGN
 11-07-2013, 16:22:02

FILE NAME = #FILE#	USER NAME = HECHTBR	DESIGNED -	REVISED -
exp U.S. Services Inc. Chicago, IL		DRAWN - RJS	REVISED -
BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY		CHECKED - BRH	REVISED -
	PLOT SCALE = #SCALE#	DATE - 11/01/13	REVISED -
	PLOT DATE = #DATE#		

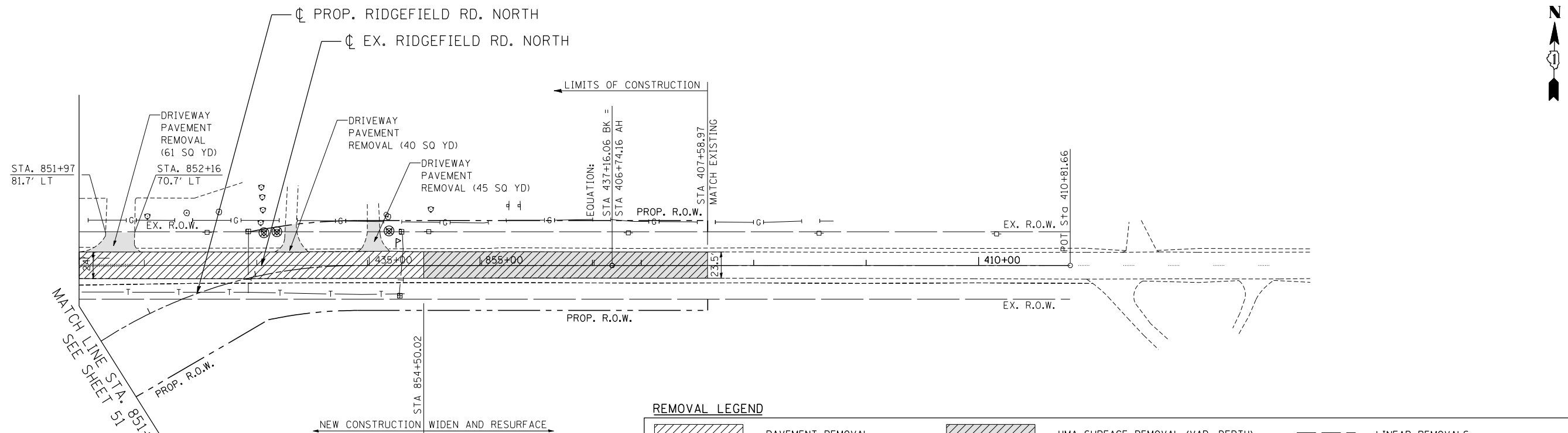
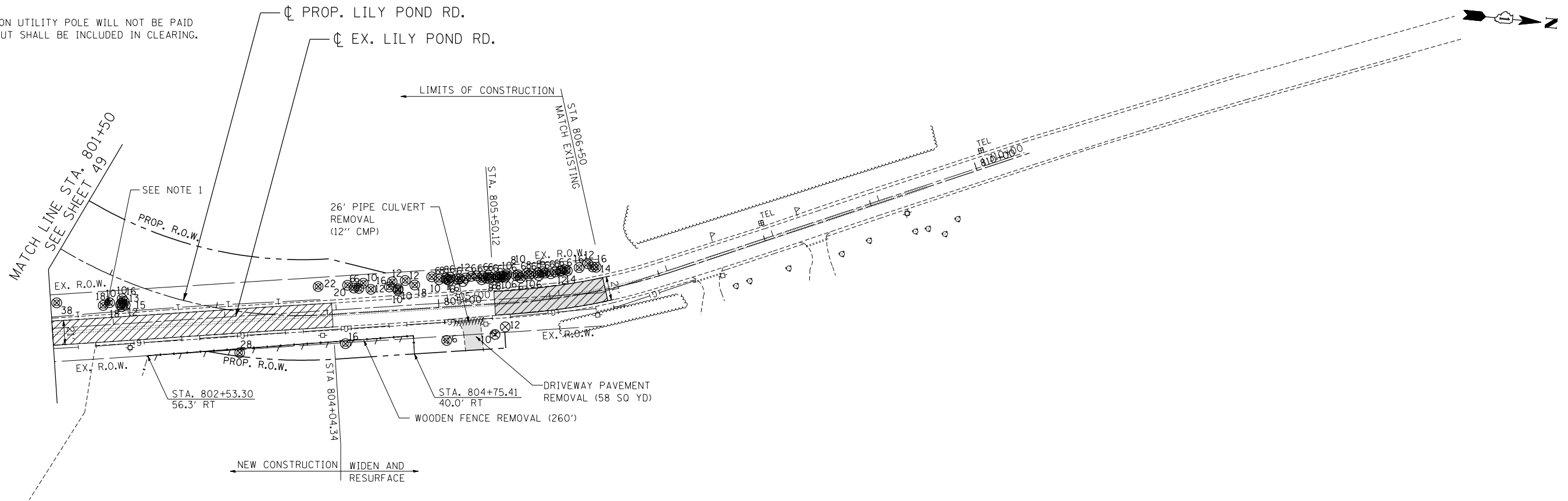
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

EXISTING ROADWAY & REMOVAL PLANS			
STA. 488+00 TO STA. 502+00			
SCALE: 1"=50'	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	52
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

NOTES:

1. REMOVAL OF ABANDON UTILITY POLE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN CLEARING.

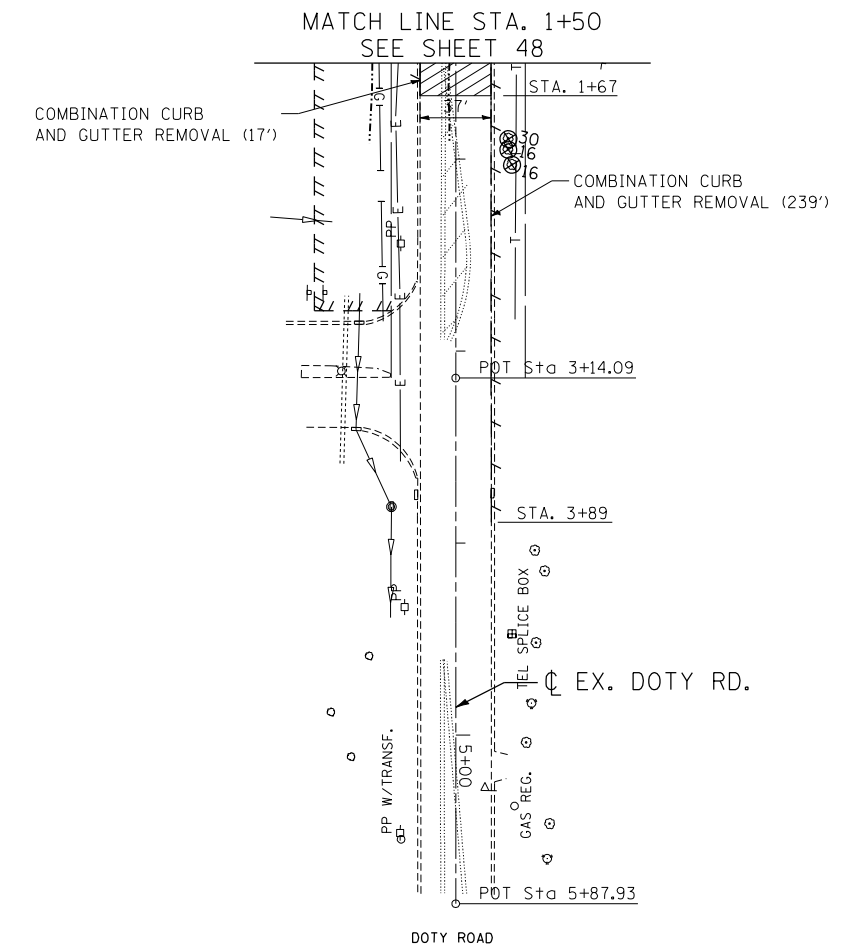
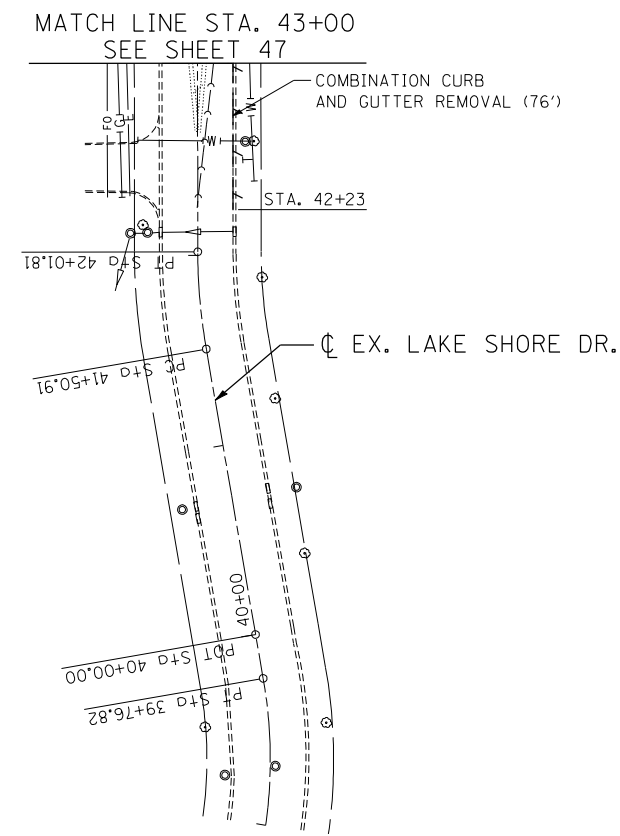
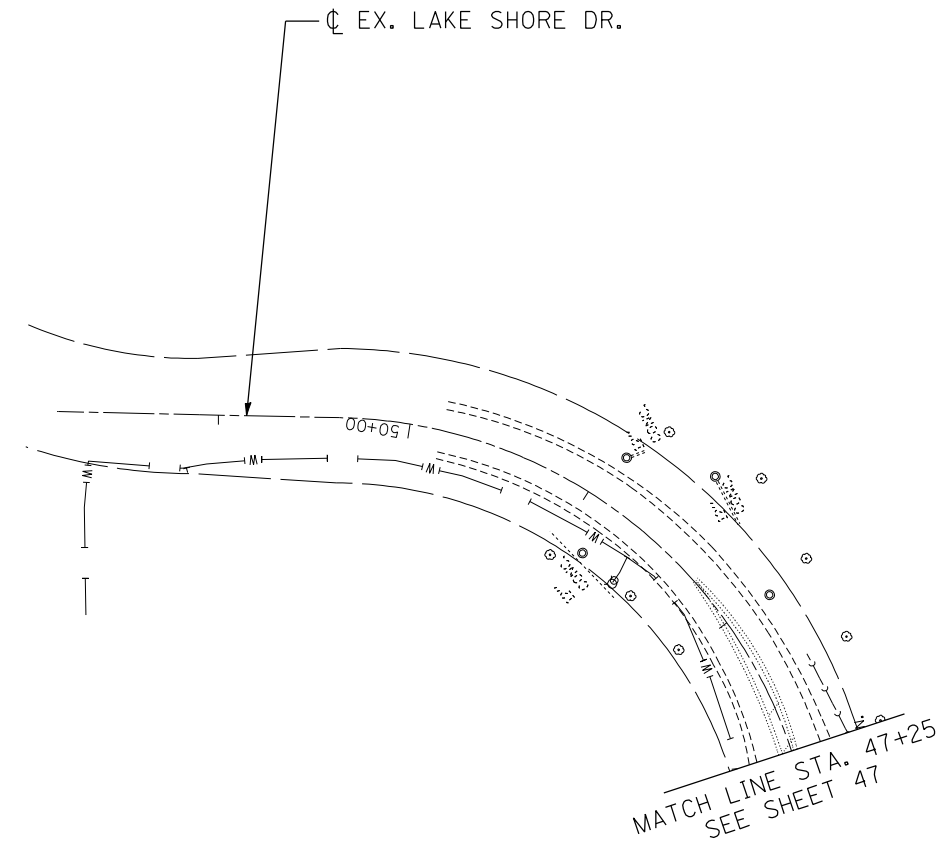
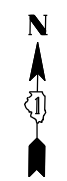
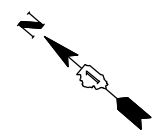
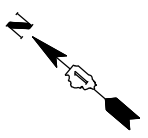


REMOVAL LEGEND

	PAVEMENT REMOVAL		HMA SURFACE REMOVAL (VAR. DEPTH)		LINEAR REMOVALS
	PAVED SHOULDER REMOVAL		HMA SURFACE REMOVAL (2 1/4")		STORM SEWER TO BE REMOVED
	ISLAND PAVEMENT REMOVAL		MEDIAN REMOVAL		PIPE CULVERT REMOVAL (INCLUDING FLARED END SECTION)
	CONCRETE MEDIAN SURFACE REMOVAL		TREE REMOVAL (WITH DIA. INCHES)		DRAINAGE STRUCTURE TO BE REMOVED
	DRIVEWAY PAVEMENT REMOVAL		BUSH / SHRUB REMOVAL (REMOVAL INCLUDED IN COST OF EARTHWORK)		DRAINAGE STRUCTURE TO BE ADJUSTED

FILE NAME = ... USER NAME = HECHTBR ... DESIGNED - ... REVISED - ... DRAWN - RJS ... REVISED - ... CHECKED - BRH ... REVISED - ... DATE - 11/01/13 ... REVISED - ...

		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14		EXISTING ROADWAY & REMOVAL PLANS LILY POND & RIDGEFIELD ROAD		F.A.P. RTE. 305	SECTION 27R-2	COUNTY MCHENRY	TOTAL SHEETS 673	SHEET NO. 53
SCALE: 1"=50'		SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 62268		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



REMOVAL LEGEND

	PAVEMENT REMOVAL		HMA SURFACE REMOVAL (VAR. DEPTH)		LINEAR REMOVALS
	PAVED SHOULDER REMOVAL		HMA SURFACE REMOVAL (2 1/4")		STORM SEWER TO BE REMOVED
	ISLAND PAVEMENT REMOVAL		MEDIAN REMOVAL		PIPE CULVERT REMOVAL (INCLUDING FLARED END SECTION)
	CONCRETE MEDIAN SURFACE REMOVAL		TREE REMOVAL (WITH DIA. INCHES)		DRAINAGE STRUCTURE TO BE REMOVED
	DRIVEWAY PAVEMENT REMOVAL		BUSH / SHRUB REMOVAL (REMOVAL INCLUDED IN COST OF EARTHWORK)		DRAINAGE STRUCTURE TO BE ADJUSTED

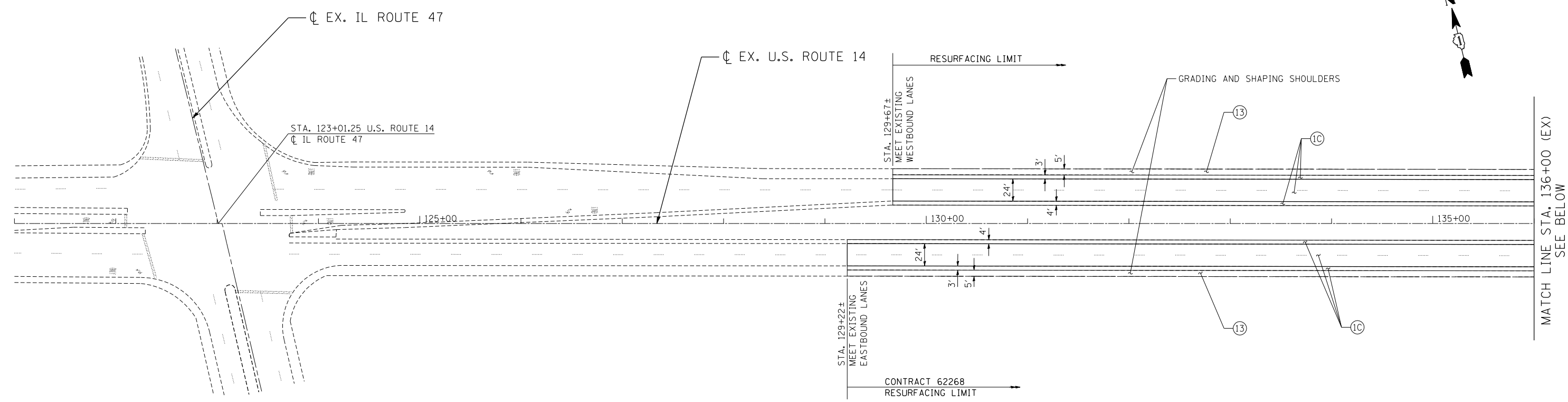
FILE NAME = ... USER NAME = HECHTBR ... DESIGNED - ... REVISED - ...
 DRAWN - RJS ... CHECKED - BRH ... DATE - 11/01/13 ...
 PLOT SCALE = *SCALE* ... PLOT DATE = *DATE* ...
 exp U.S. Services Inc. Chicago, IL BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

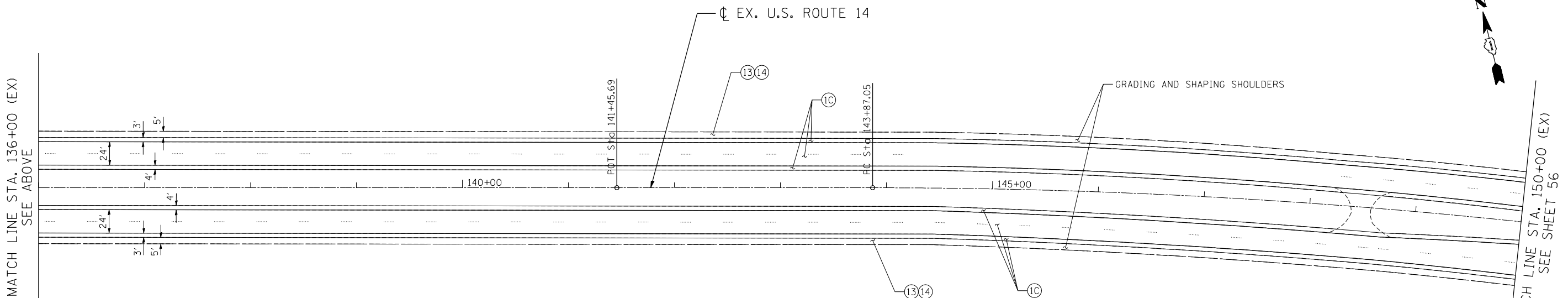
EXISTING ROADWAY & REMOVAL PLANS
LAKE SHORE DRIVE & DOTY ROAD

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	54
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 136+00 (EX)
SEE BELOW



MATCH LINE STA. 136+00 (EX)
SEE ABOVE

MATCH LINE STA. 150+00 (EX)
SEE SHEET 56

PROPOSED LEGEND • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)

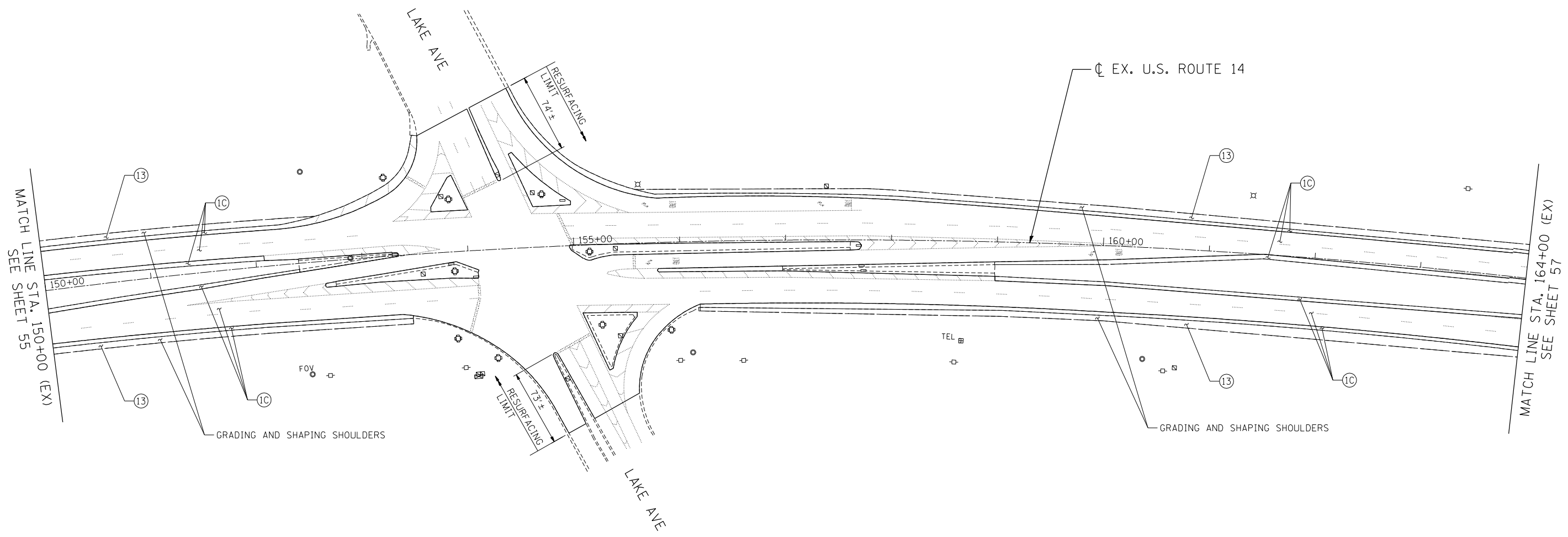
- | | | | |
|--|--|---|--|
| <p>① US ROUTE 14*
P.C.C. PAVEMENT 9 3/4" JOINTED
STABILIZED SUBBASE (HMA) 4 1/2"</p> <p>①A HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 6 1/4"</p> <p>①B HMA ASPHALT PAVT (FULL DEPTH), 9"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 7"</p> <p>①C US ROUTE 14 RESURFACING
HMA SURFACE COURSE, MIX "D", N70 1 3/4"
POLY LEV BIND MM IL-4.75 N50 3/4"</p> | <p>② MULTI-USE PATH
HMA SURFACE COURSE, MIX "D", N50, 1 1/2"
HMA BINDER COURSE, IL-19, N50, 2 1/2"
AGGREGATE BASE COURSE, TYPE B 10"
(SEE NOTE 14 & 26 SHEET 3)</p> <p>③ N. RIDGEFIELD RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES</p> <p>③A LILY POND RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES</p> | <p>④ LILY POND WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 6 1/4"
N. RIDGEFIELD WIDENING*</p> <p>④A HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 7"</p> <p>⑤ PRIVATE ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 6"</p> <p>⑥ COMMERCIAL ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 8"</p> | <p>⑥A FIELD ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
AGGREGATE BASE COURSE, TYPE B 8"</p> <p>⑦ HMA SHOULDERS, 8"</p> <p>⑧ PCC SHOULDERS, 9 3/4"</p> <p>⑨ CONCRETE MEDIAN SURFACE 4"</p> <p>⑩ AGGREGATE SHOULDERS, TYPE B 6"</p> <p>⑪ LANDSCAPE MEDIAN (SEE LANSCAPING PLANS)</p> <p>⑫ AGGREGATE SHOULDERS, TYPE B 8"</p> <p>⑬ AGGREGATE WEDGE SHOULDER, TYPE B</p> |
|--|--|---|--|

NOTES:

1. SEE SHEET 33 FOR RUMBLE STRIPS, 8 INCH LOCATIONS.

FILE NAME = ... USER NAME = HECHTBR ... DESIGNED - ... REVISED - ...
 11-11-2013, 14:18:25

exp U.S. Services Inc. BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14		PROPOSED ROADWAY U.S. ROUTE 14 PLAN STA. 121+00 (EX) TO STA. 150+00 (EX)		F.A.P. RTE. 305 SECTION 27R-2 COUNTY MCHENRY TOTAL SHEETS 673 SHEET NO. 55	
SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 62268		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



PROPOSED LEGEND • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)

- | | | | |
|---|--|--|--|
| <p>① US ROUTE 14*
P.C.C. PAVEMENT 9 3/4" JOINTED
STABILIZED SUBBASE (HMA) 4 1/2"</p> <p>LILY POND*</p> <p>①A HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 6 1/4"</p> <p>N. RIDGEFIELD*</p> <p>①B HMA ASPHALT PAVT (FULL DEPTH), 9"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 7"</p> <p>US ROUTE 14 RESURFACING</p> <p>①C HMA SURFACE COURSE, MIX "D", N70 1 3/4"
POLY LEV BIND MM IL-4.75 N50 3/4"</p> | <p>② MULTI-USE PATH
HMA SURFACE COURSE, MIX "D", N50, 1 1/2"
HMA BINDER COURSE, IL-19, N50, 2 1/2"
AGGREGATE BASE COURSE, TYPE B 10"
(SEE NOTE 14 & 26 SHEET 3)</p> <p>③ N. RIDGEFIELD RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES</p> <p>③A LILY POND RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES</p> | <p>④ LILY POND WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 6 1/4"</p> <p>N. RIDGEFIELD WIDENING*</p> <p>④A HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 7"</p> <p>PRIVATE ENTRANCE DRIVEWAY</p> <p>⑤ HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 6"</p> <p>COMMERCIAL ENTRANCE DRIVEWAY</p> <p>⑥ HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 8"</p> | <p>FIELD ENTRANCE DRIVEWAY</p> <p>⑥A HMA SURFACE COURSE, MIX "D", N50, 2"
AGGREGATE BASE COURSE, TYPE B 8"</p> <p>⑦ HMA SHOULDERS, 8"</p> <p>⑧ PCC SHOULDERS, 9 3/4"</p> <p>⑨ CONCRETE MEDIAN SURFACE 4"</p> <p>⑩ AGGREGATE SHOULDERS, TYPE B 6"</p> <p>⑪ LANDSCAPE MEDIAN (SEE LANDSCAPING PLANS)</p> <p>⑫ AGGREGATE SHOULDERS, TYPE B 8"</p> <p>⑬ AGGREGATE WEDGE SHOULDER, TYPE B</p> |
|---|--|--|--|

NOTES:

1. SEE SHEET 33 FOR RUMBLE STRIPS, 8 INCH LOCATIONS.

FILE NAME = T:\DOCUMENTS\1934\9901\CIVIL\DRAWING\PROJ00_ZA.DGN
 USER NAME = HECHTBR
 DESIGNED -
 REVISED -
 DRAWN - JP
 REVISED -
 CHECKED - KA
 REVISED -
 DATE - 11/01/13
 REVISED -
 PLOT SCALE = *SCALE*
 PLOT DATE = *DATE*
 exp U.S. Services Inc. Chicago, IL
 BUILDINGS-EARTH & ENVIRONMENT-ENERGY
 INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY
 11-11-2013, 14:18:30

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

PROPOSED ROADWAY
U.S. ROUTE 14 PLAN
STA. 150+00 (EX) TO STA. 164+00 (EX)

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

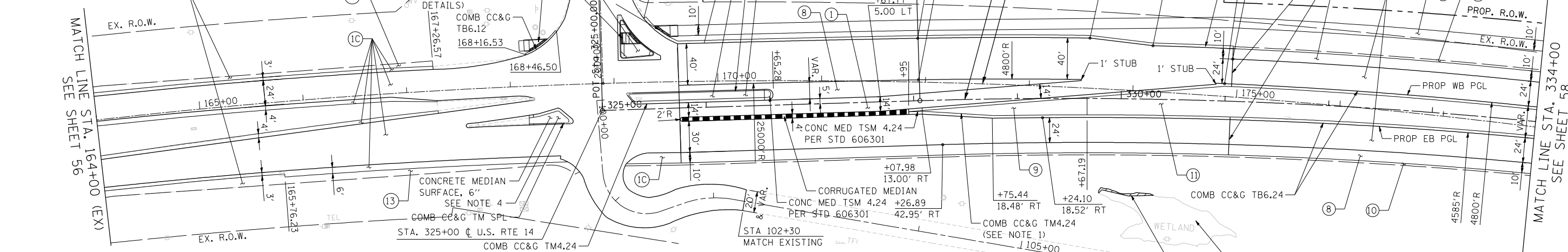
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	56
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

NOTES:

- CURB AND GUTTER SHALL TRANSITION FROM TM4.24 TO TB6.24 FROM STA. 328+75.44 TO STA. 328+80.44.
- SEE SHEET 74 FOR INTERMEDIATE ISLAND DETAILS.
- EXPANSION JOINT WILL NOT BE PAID FOR SEPARATELY. LOCATION TO BE DETERMINED BY THE ENGINEER.
- SEE SHEET 75 FOR MEDIAN ISLAND DETAILS.
- SEE SHEET 33 FOR SHOULDER RUMBLE STRIPS, 8 INCH LOCATIONS.

PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. _____	

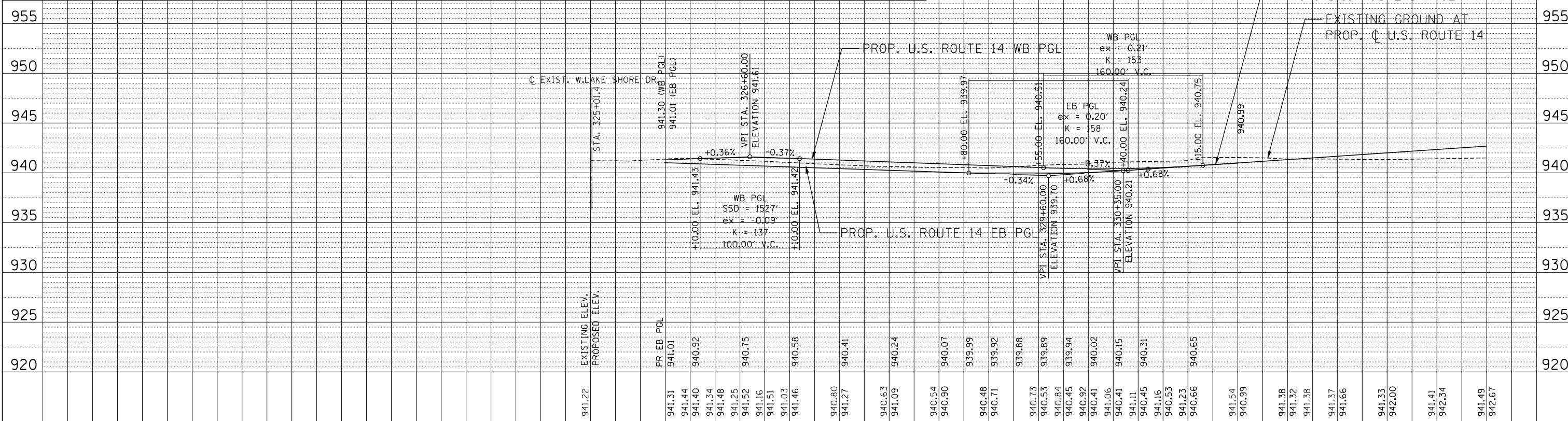
GRADING AND SHAPING SHOULDERS



PROPOSED LEGEND - PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)

- | | | |
|--|--|--|
| <p>① US ROUTE 14*
P.C.C. PAVEMENT 9 3/4" JOINTED STABILIZED SUBBASE (HMA) 4 1/2"</p> <p>② MULTI-USE PATH
HMA SURFACE COURSE, MIX "D", N50, 1 1/2" HMA BINDER COURSE, IL-19, N50, 2 1/2" AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 & 26 SHEET 3)</p> <p>③ N. RIDGEFIELD RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES</p> <p>④ LILY POND WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 6 1/4" N. RIDGEFIELD WIDENING*</p> <p>⑤ N. RIDGEFIELD RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES</p> <p>⑥ LILY POND RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES</p> <p>⑦ US ROUTE 14 RESURFACING
HMA SURFACE COURSE, MIX "D", N70 1 3/4" POLY LEV BIND MM IL-4.75 N50 3/4"</p> | <p>⑧ HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 6 1/4" N. RIDGEFIELD WIDENING*</p> <p>⑨ HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 7" PRIVATE ENTRANCE DRIVEWAY</p> <p>⑩ HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 6" COMMERCIAL ENTRANCE DRIVEWAY</p> <p>⑪ HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 8"</p> | <p>⑫ HMA SURFACE COURSE, MIX "D", N50, 2" AGGREGATE BASE COURSE, TYPE B 8"</p> <p>⑬ HMA SHOULDERS, 8"</p> <p>⑭ PCC SHOULDERS, 9 3/4"</p> <p>⑮ CONCRETE MEDIAN SURFACE 4"</p> <p>⑯ AGGREGATE SHOULDERS, TYPE B 6"</p> <p>⑰ LANDSCAPE MEDIAN (SEE LANDSCAPING PLANS)</p> <p>⑱ AGGREGATE SHOULDERS, TYPE B 8"</p> <p>⑲ AGGREGATE WEDGE SHOULDER, TYPE B</p> |
|--|--|--|

PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. _____	



**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
U.S. ROUTE 14**

**PROPOSED ROADWAY
U.S. ROUTE 14 PLAN & PROFILE
STA. 164+00 (EX) TO STA. 334+00**

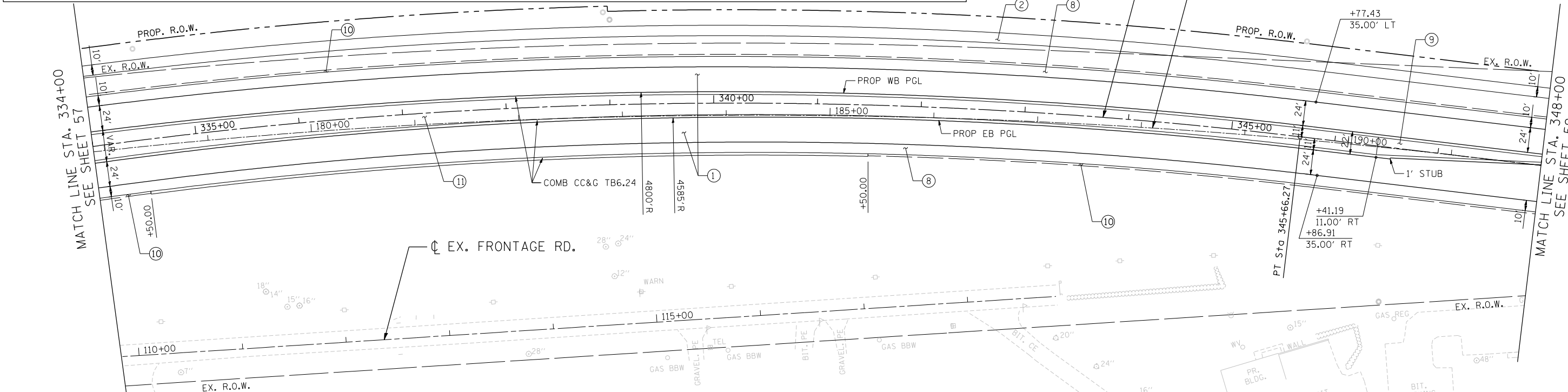
FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -	SCALE: HORIZ. 1"=50'	VERT. 1"=5'	STA. TO STA.	F.A.P. RTE. 305	SECTION 27R-2	COUNTY MCHENRY	TOTAL SHEETS 673	SHEET NO. 57
FILE.EL		DRAWN - RJS	REVISED -				CONTRACT NO. 62268				
		CHECKED - BRH	REVISED -				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
		DATE - 11/01/13	REVISED -								

11-07-2013 16:22:45 HECHTBR
 T:\DOCUMENT\15134298\15134298\CIVIL\GDN\PROJ\08\J.R.DON

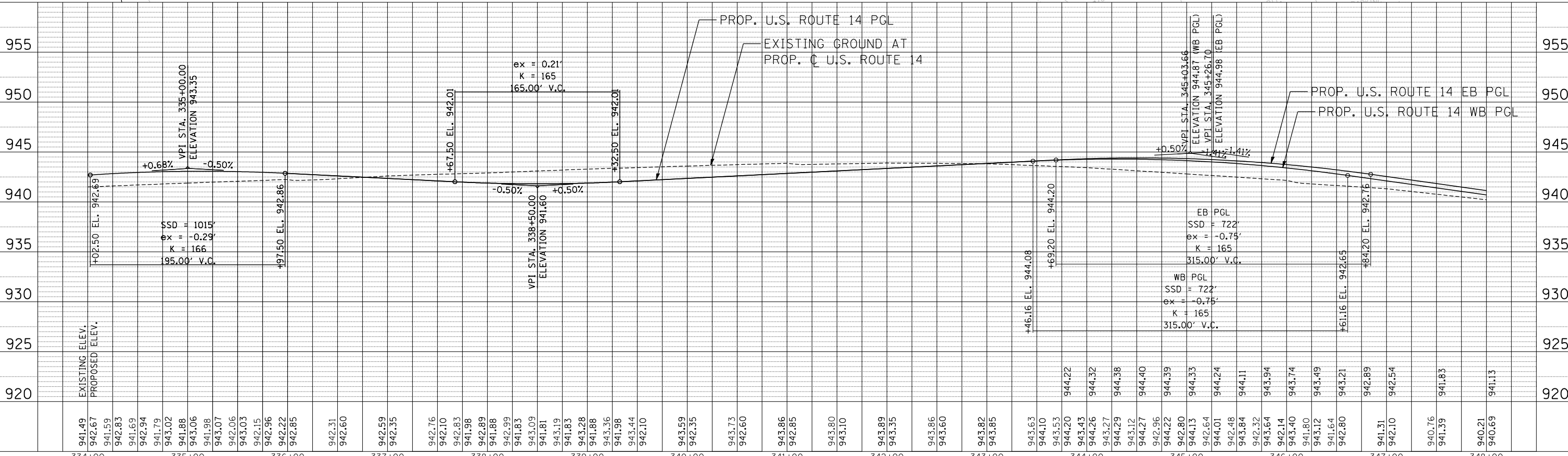
PROPOSED LEGEND • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)

- | | | | |
|---|--|--|--|
| <p>① US ROUTE 14*
P.C.C. PAVEMENT 9 3/4" JOINTED
STABILIZED SUBBASE (HMA) 4 1/2"</p> <p>①A HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 6 1/4"
N. RIDGEFIELD*</p> <p>①B HMA ASPHALT PAVT (FULL DEPTH), 9"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 7"
US ROUTE 14 RESURFACING</p> <p>①C HMA SURFACE COURSE, MIX "D", N70 1 3/4"
POLY LEV BIND MM IL-4.75 N50 3/4"</p> | <p>② MULTI-USE PATH
HMA SURFACE COURSE, MIX "D", N50, 1 1/2"
HMA BINDER COURSE, IL-19, N50, 2 1/2"
AGGREGATE BASE COURSE, TYPE B 10"
(SEE NOTE 14 & 26 SHEET 3)</p> <p>③ N. RIDGEFIELD RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES</p> <p>③A LILY POND RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES</p> | <p>④ LILY POND WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 6 1/4"
N. RIDGEFIELD WIDENING*</p> <p>④A HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 7"
PRIVATE ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 6"</p> <p>⑤ COMMERCIAL ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 8"</p> | <p>⑥A FIELD ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
AGGREGATE BASE COURSE, TYPE B 8"</p> <p>⑦ HMA SHOULDERS, 8"</p> <p>⑧ PCC SHOULDERS, 9 3/4"</p> <p>⑨ CONCRETE MEDIAN SURFACE 4"</p> <p>⑩ AGGREGATE SHOULDERS, TYPE B 6"</p> <p>⑪ LANDSCAPE MEDIAN (SEE LANSCAPING PLANS)</p> <p>⑫ AGGREGATE SHOULDERS, TYPE B 8"</p> <p>⑬ AGGREGATE WEDGE SHOULDER, TYPE B</p> |
|---|--|--|--|

PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	CHECKED	
	AT	
	MAX	
	FILE	
	NAME	



PROFILE	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	CHECKED	
	AT	
	MAX	
	FILE	
	NAME	



11-07-2013 16:22:58 HECHTBR
 T:\DOCUMENT\1134398\1134398\CIVIL\UDR\PROP08\15.DGN

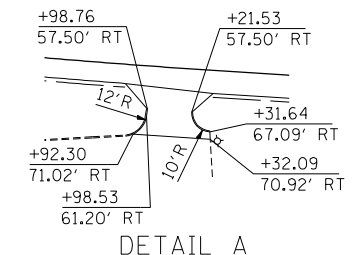
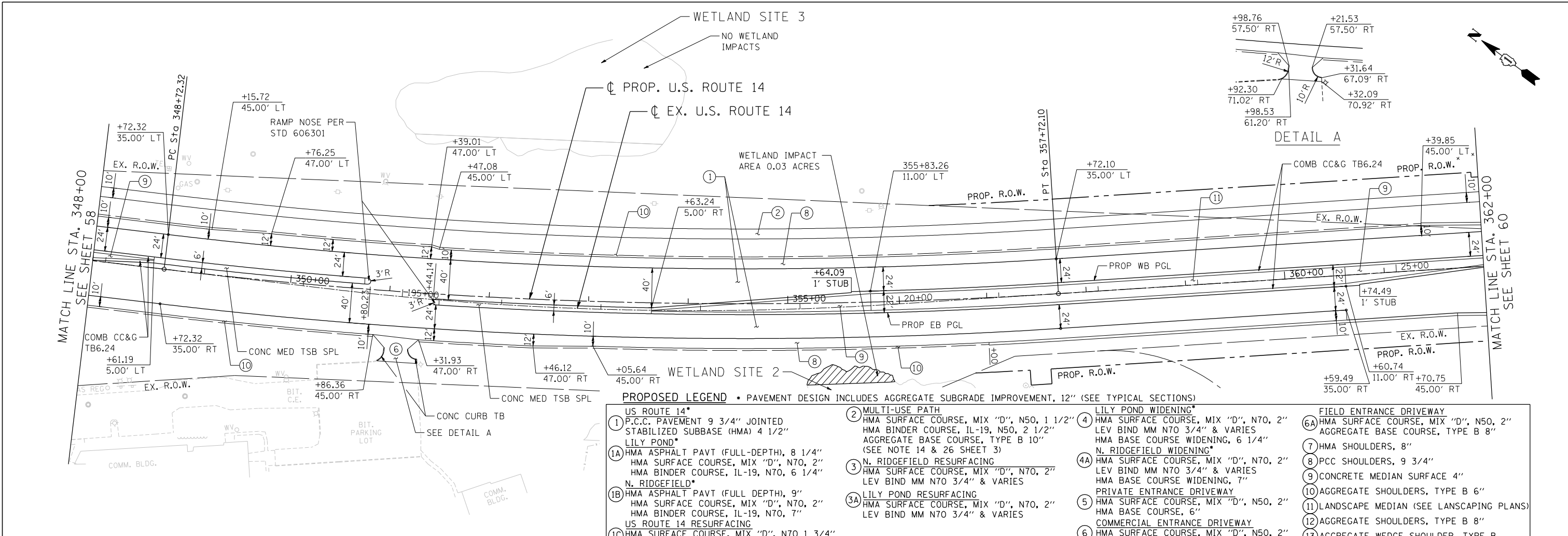
FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -	<p align="center">STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14</p>	<p align="center">PROPOSED ROADWAY U.S. ROUTE 14 PLAN & PROFILE STA. 334+00 TO STA. 348+00</p>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
FILE.EL		DRAWN - RJS	REVISED -			305	27R-2	MCHENRY	673	58	
PLOT SCALE = *SCALE*		CHECKED - BRH	REVISED -			CONTRACT NO. 62268					
PLOT DATE = *DATE*		DATE - 11/01/13	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA.

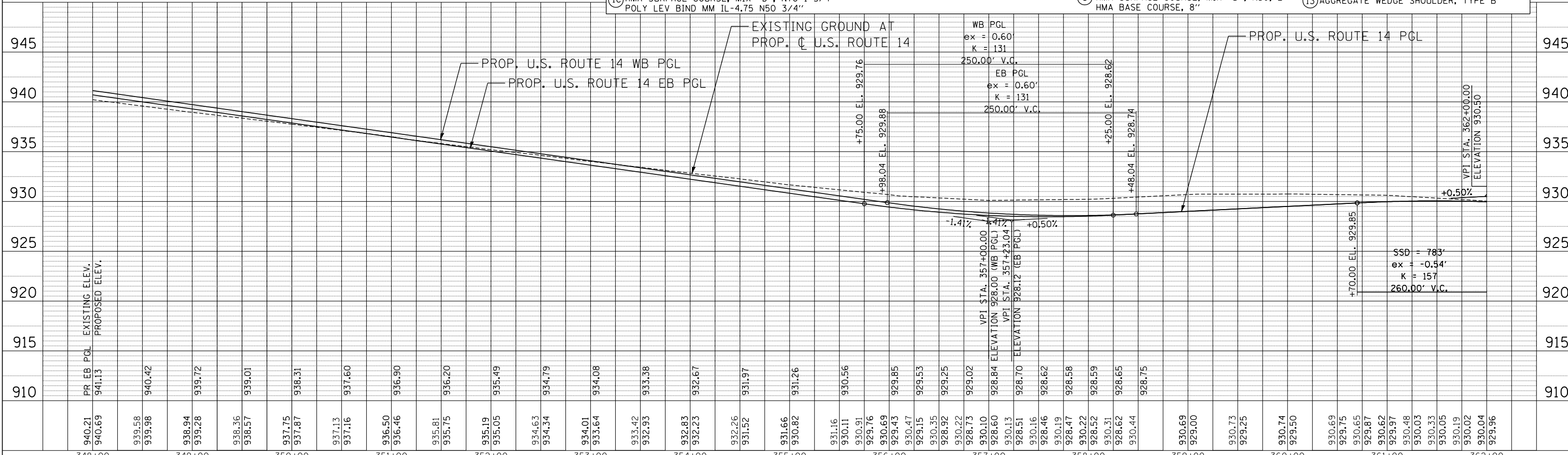
PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	CHECKED	
	AT	
	WORK	
	MAX	
	FILE	
	NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	CHECKED	
	AT	
	WORK	
	MAX	
	FILE	
	NAME	

11-07-2013 16:23:05 HECHTBR
 T:\DOCUMENT\13243980\CIVIL\UDN\PH0811.DGN



- PROPOSED LEGEND** • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)
- US ROUTE 14*
 - 1 P.C.C. PAVEMENT 9 3/4" JOINTED STABILIZED SUBBASE (HMA) 4 1/2" LILY POND*
 - 1A HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19, N70, 6 1/4" N. RIDGEFIELD*
 - 1B HMA ASPHALT PAVT (FULL DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19, N70, 7" US ROUTE 14 RESURFACING
 - 1C HMA SURFACE COURSE, MIX "D", N70 1 3/4" POLY LEV BIND MM IL-4.75 N50 3/4"
 - MULTI-USE PATH
 - 2 HMA SURFACE COURSE, MIX "D", N50, 1 1/2" HMA BINDER COURSE, IL-19, N50, 2 1/2" AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 & 26 SHEET 3)
 - N. RIDGEFIELD RESURFACING
 - 3 HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES
 - LILY POND RESURFACING
 - 3A HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES
 - LILY POND WIDENING*
 - 4 HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 6 1/4" N. RIDGEFIELD WIDENING*
 - 4A HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 7" PRIVATE ENTRANCE DRIVEWAY
 - 5 HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 6"
 - 6 HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 8"
 - FIELD ENTRANCE DRIVEWAY
 - 6A HMA SURFACE COURSE, MIX "D", N50, 2" AGGREGATE BASE COURSE, TYPE B 8"
 - 7 HMA SHOULDERS, 8"
 - 8 PCC SHOULDERS, 9 3/4"
 - 9 CONCRETE MEDIAN SURFACE 4"
 - 10 AGGREGATE SHOULDERS, TYPE B 6"
 - 11 LANDSCAPE MEDIAN (SEE LANSCAPING PLANS)
 - 12 AGGREGATE SHOULDERS, TYPE B 8"
 - 13 AGGREGATE WEDGE SHOULDER, TYPE B

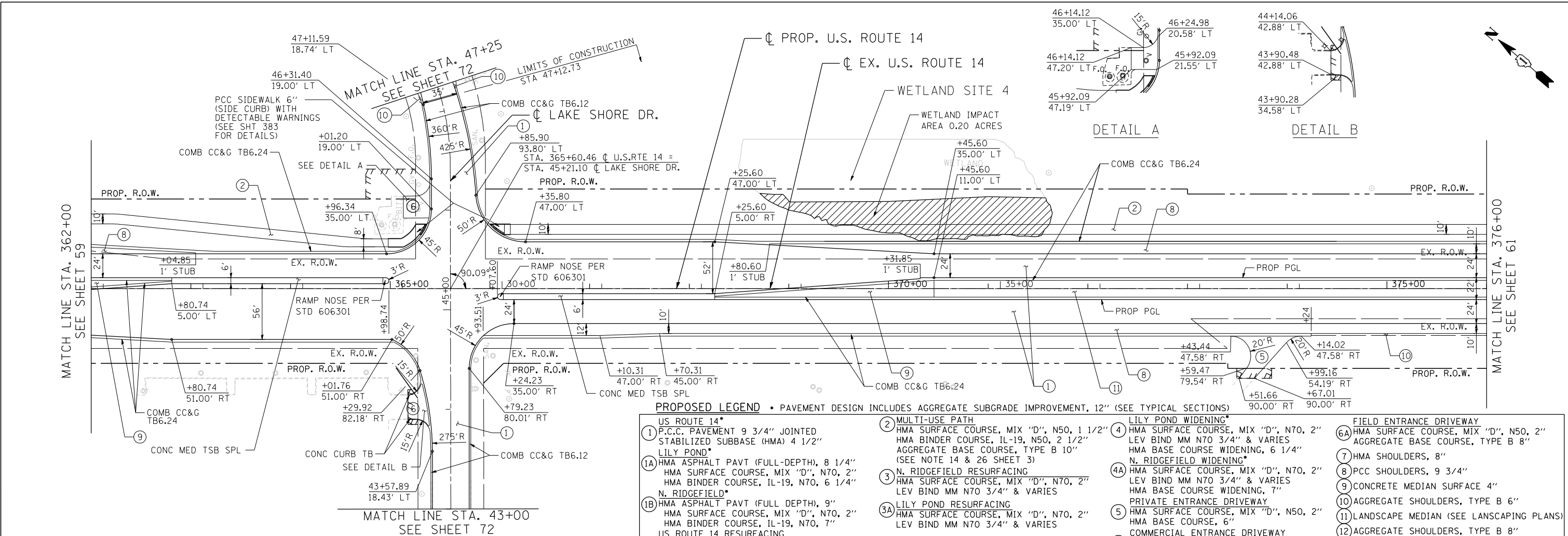


FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	PROPOSED ROADWAY U.S. ROUTE 14 PLAN & PROFILE STA. 348+00 TO STA. 362+00	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILEL#		DRAWN - RJS	REVISED -			305	27R-2	MCHENRY	673	59	
PLOT SCALE = *SCALE*		CHECKED - BRH	REVISED -			CONTRACT NO. 62268					
PLOT DATE = *DATE*		DATE - 11/01/13	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

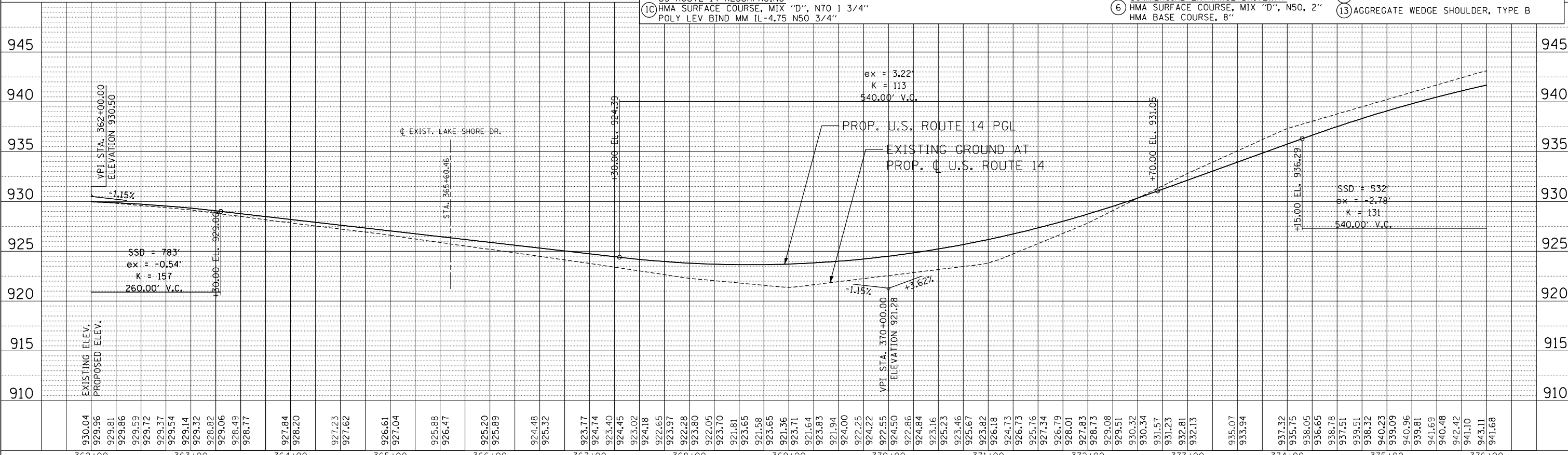
PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	AT		
	FILE NAME		
	NO.		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	AT		
	FILE NAME		
	NO.		

11-07-2013 16:23:13 HECHTER
 T:\DOCUMENT\13134390\CIVIL\UDR\PH0813\J.DCN



- PROPOSED LEGEND** • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)
- ① US ROUTE 14* P.C.C. PAVEMENT 9 3/4" JOINTED STABILIZED SUBBASE (HMA) 4 1/2"
 - ② MULTI-USE PATH HMA SURFACE COURSE, MIX "D", N50, 1 1/2" HMA BINDER COURSE, IL-19, N50, 2 1/2" AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 & 26 SHEET 3)
 - ③ N. RIDGEFIELD RESURFACING HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES
 - ④ LILY POND WIDENING* HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 6 1/4" N. RIDGEFIELD WIDENING* HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 7" PRIVATE ENTRANCE DRIVEWAY HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 6" COMMERCIAL ENTRANCE DRIVEWAY HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 8"
 - ⑤ LILY POND RESURFACING HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES
 - ⑥ HMA ASPHALT PAVT (FULL DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19, N70, 7" US ROUTE 14 RESURFACING HMA SURFACE COURSE, MIX "D", N70 1 3/4" POLY LEV BIND MM IL-4.75 N50 3/4"
 - ⑦ HMA SHOULDERS, 8"
 - ⑧ PCC SHOULDERS, 9 3/4"
 - ⑨ CONCRETE MEDIAN SURFACE 4"
 - ⑩ AGGREGATE SHOULDERS, TYPE B 6"
 - ⑪ LANDSCAPE MEDIAN (SEE LANSAPPE PLANS)
 - ⑫ AGGREGATE SHOULDERS, TYPE B 8"
 - ⑬ AGGREGATE WEDGE SHOULDER, TYPE B



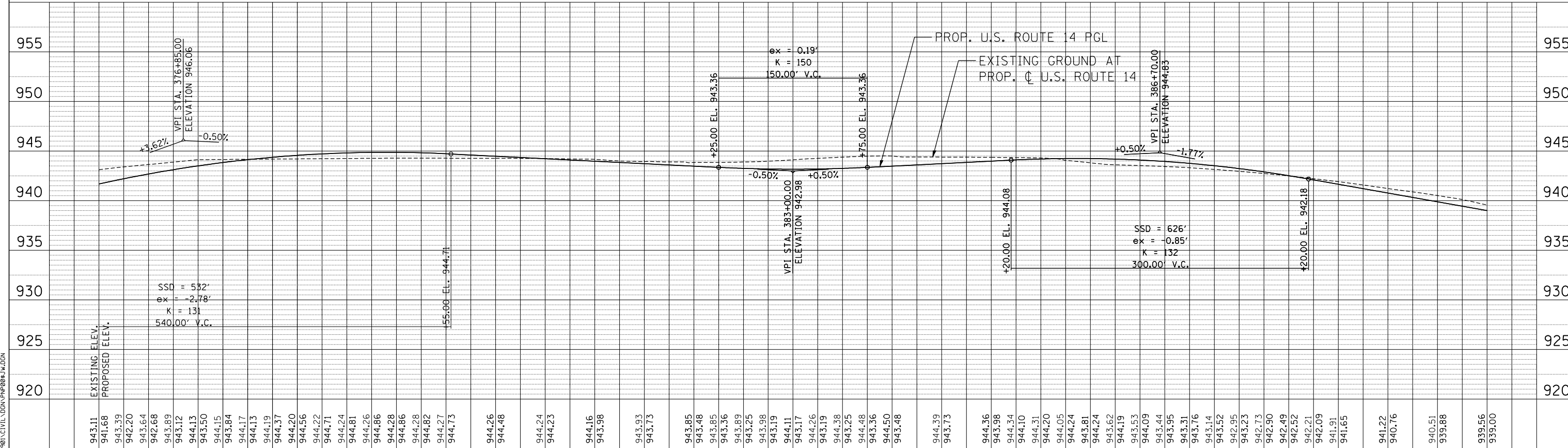
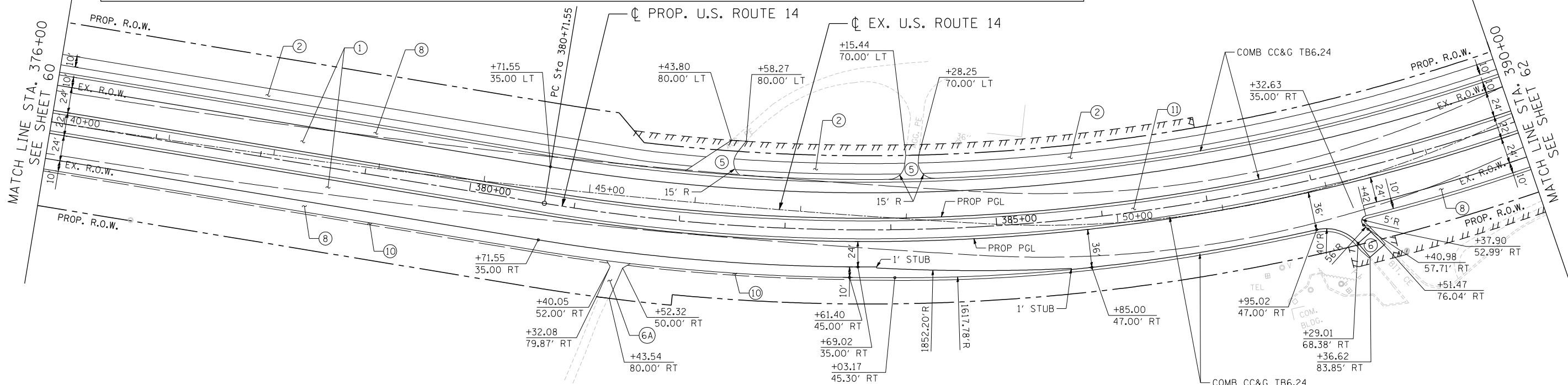
FILE NAME =	USER NAME = HECHTR	DESIGNED -	REVISED -
#FILE#		DRAWN - RJS	REVISED -
PLOT SCALE = *SCALE*		CHECKED - BRH	REVISED -
PLOT DATE = *DATE*		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS		PROPOSED ROADWAY	
DEPARTMENT OF TRANSPORTATION		U.S. ROUTE 14 PLAN & PROFILE	
U.S. ROUTE 14		STA. 362+00 TO STA. 376+00	
SCALE: HORIZ. 1"=50'	VERT. 1"=5'	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	60
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62268	

PROPOSED LEGEND • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)

- | | | | |
|---|--|---|--|
| ① US ROUTE 14*
P.C.C. PAVEMENT 9 3/4" JOINTED
STABILIZED SUBBASE (HMA) 4 1/2" | ② MULTI-USE PATH
HMA SURFACE COURSE, MIX "D", N50, 1 1/2"
HMA BINDER COURSE, IL-19, N50, 2 1/2"
AGGREGATE BASE COURSE, TYPE B 10"
(SEE NOTE 14 & 26 SHEET 3) | ④ LILY POND WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 6 1/4" | ⑥A FIELD ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
AGGREGATE BASE COURSE, TYPE B 8" |
| ①A LILY POND*
HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 6 1/4"
N. RIDGEFIELD* | ③ N. RIDGEFIELD RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES | ④A N. RIDGEFIELD WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 7" | ⑦ HMA SHOULDERS, 8" |
| ①B HMA ASPHALT PAVT (FULL DEPTH), 9"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 7" | ③A LILY POND RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES | ⑤ PRIVATE ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 6" | ⑧ PCC SHOULDERS, 9 3/4" |
| ①C US ROUTE 14 RESURFACING
HMA SURFACE COURSE, MIX "D", N70 1 3/4"
POLY LEV BIND MM IL-4.75 N50 3/4" | | ⑥ COMMERCIAL ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 8" | ⑨ CONCRETE MEDIAN SURFACE 4" |
| | | | ⑩ AGGREGATE SHOULDERS, TYPE B 6" |
| | | | ⑪ LANDSCAPE MEDIAN (SEE LANDSCAPING PLANS) |
| | | | ⑫ AGGREGATE SHOULDERS, TYPE B 8" |
| | | | ⑬ AGGREGATE WEDGE SHOULDER, TYPE B |



PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	CHECKED	
	AT	
	WORK	
	FILE	
	NAME	
	CARD	
	FILE	

PROFILE	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	CHECKED	
	AT	
	WORK	
	FILE	
	NAME	
	CARD	
	FILE	

11-07-2013 16:23:19 HECHTBR
 TA:DOCUMENT\134398\CIVIL\UDN\134398\08\14.DGN

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	PROPOSED ROADWAY U.S. ROUTE 14 PLAN & PROFILE STA. 376+00 TO STA. 390+00	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILEL#		DRAWN - RJS	REVISED -			305	27R-2	MCHENRY	673	61	
PLOT SCALE = *SCALE*		CHECKED - BRH	REVISED -			CONTRACT NO. 62268					
PLOT DATE = *DATE*		DATE - 11/01/13	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

PROPOSED LEGEND • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)

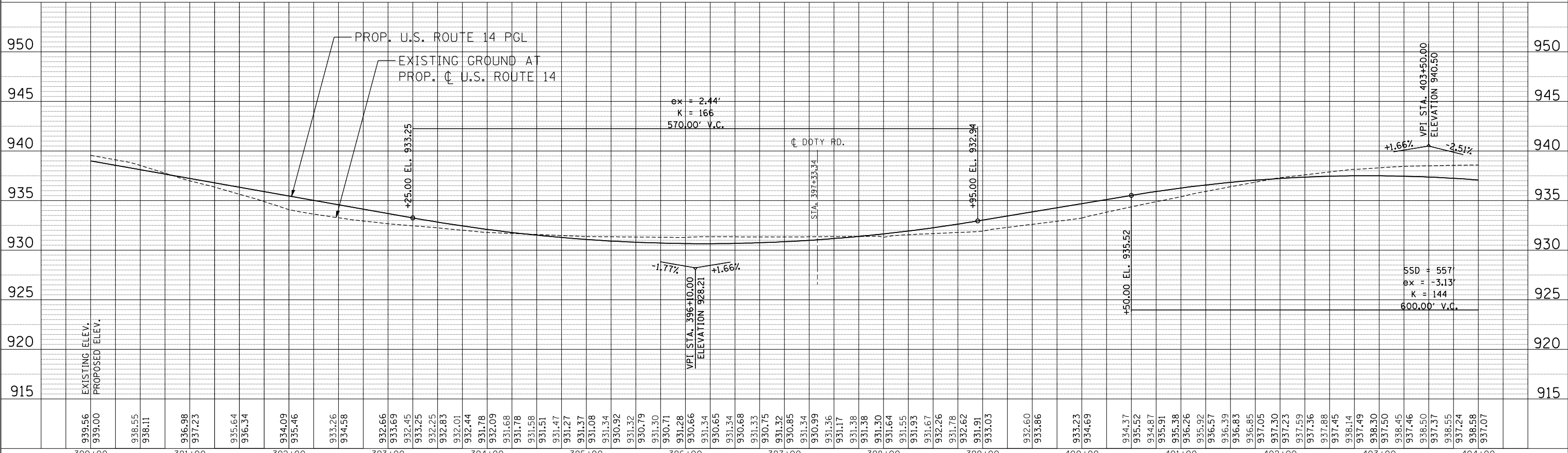
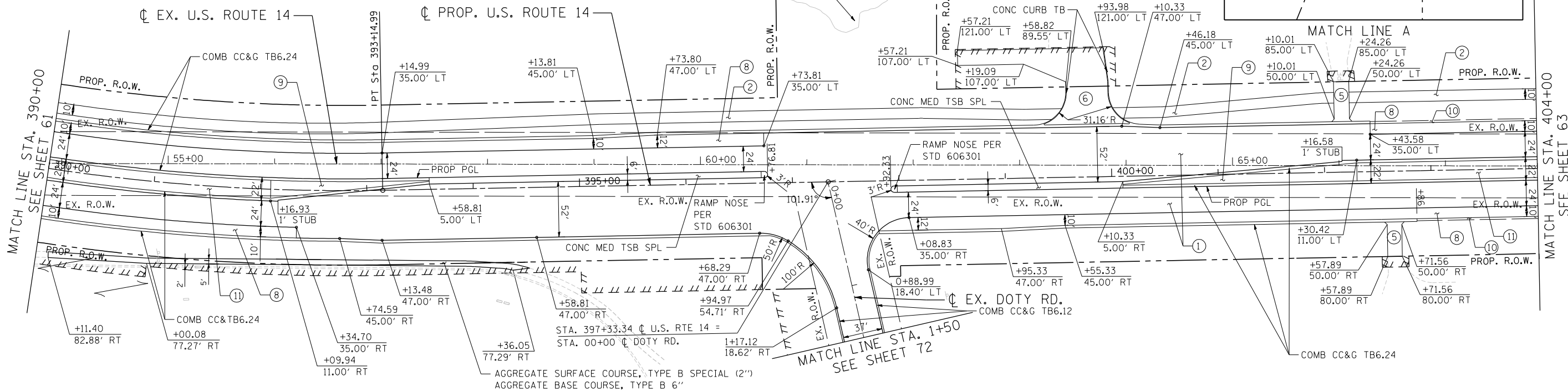
- ① US ROUTE 14*
P.C.C. PAVEMENT 9 3/4" JOINTED
STABILIZED SUBBASE (HMA) 4 1/2"
- ② MULTI-USE PATH
HMA SURFACE COURSE, MIX "D", N50, 1 1/2"
HMA BINDER COURSE, IL-19, N50, 2 1/2"
AGGREGATE BASE COURSE, TYPE B 10"
(SEE NOTE 14 & 26 SHEET 3)
- ③ N. RIDGEFIELD RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
- ④ LILY POND WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 6 1/4"
- ④A N. RIDGEFIELD WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 7"
- ⑤ PRIVATE ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 6"
- ⑥ COMMERCIAL ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 8"
- ⑦ LILY POND WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 6 1/4"
- ⑧ N. RIDGEFIELD WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 7"
- ⑨ PRIVATE ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 6"
- ⑩ COMMERCIAL ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 8"

PROPOSED ROADWAY LEGEND (CONT)

- ⑥A FIELD ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
AGGREGATE BASE COURSE, TYPE B 8"
- ⑦ HMA SHOULDERS, 8"
- ⑧ PCC SHOULDERS, 9 3/4"
- ⑨ CONCRETE MEDIAN SURFACE 4"
- ⑩ AGGREGATE SHOULDERS, TYPE B 6"
- ⑪ LANDSCAPE MEDIAN (SEE LANSCAPING PLANS)
- ⑫ AGGREGATE SHOULDERS, TYPE B 8"
- ⑬ AGGREGATE WEDGE SHOULDER, TYPE B

PLAN	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHKD	
	NO. _____	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHKD	
	NO. _____	



11-07-2013 16:23:28 HECHTBR 1:343980\CIVIL\UDN\PR0814.DGN

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
#FILE#		DRAWN - RJS	REVISED -
PLOT SCALE = *SCALE*		CHECKED - BRH	REVISED -
PLOT DATE = *DATE*		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
U.S. ROUTE 14

PROPOSED ROADWAY
U.S. ROUTE 14 PLAN & PROFILE
STA. 390+00 TO STA. 404+00

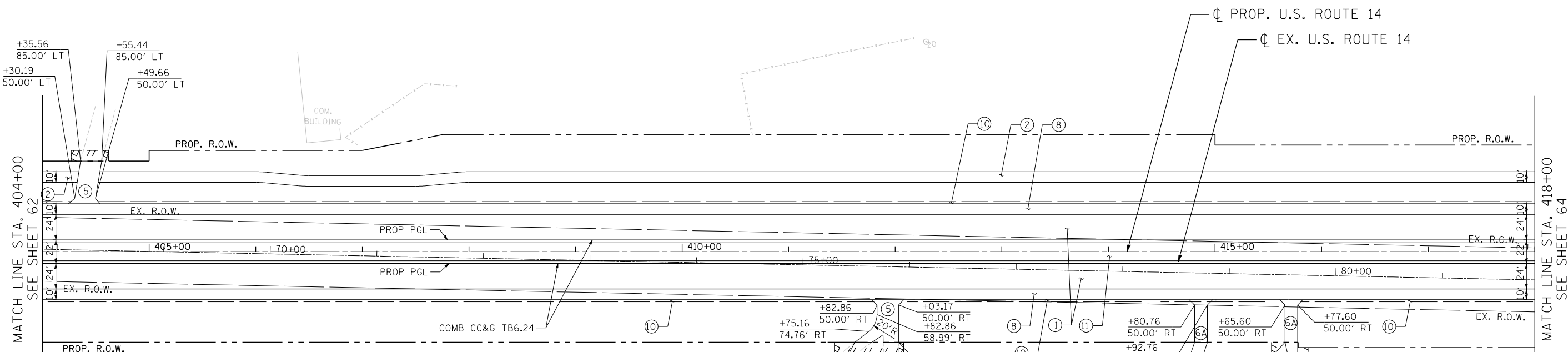
SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA. FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	62
CONTRACT NO. 62268				

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	AT		
	NO. _____		
	FILE NAME		

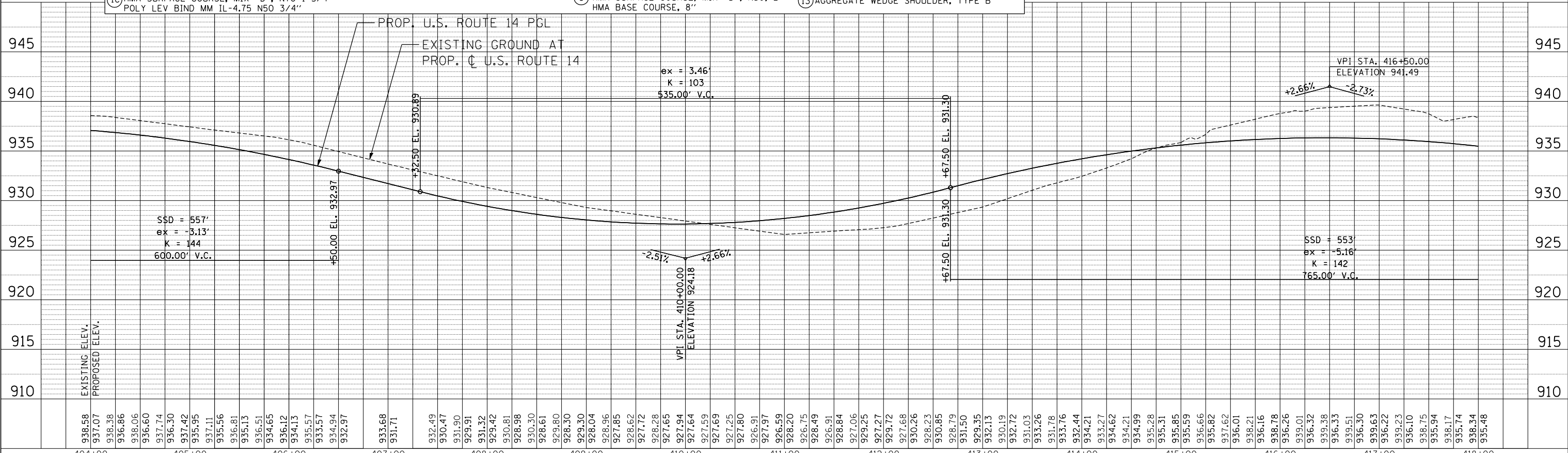
PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	AT		
	NO. _____		
	FILE NAME		

11-07-2013 16:23:07 HECHTBR
 T:\DOCUMENT\13134390\CIVIL\UDR\PROP\0813.DGN



- PROPOSED LEGEND** • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)
- | | | | |
|---|---|--|--|
| <p>US ROUTE 14*</p> <p>① P.C.C. PAVEMENT 9 3/4" JOINTED STABILIZED SUBBASE (HMA) 4 1/2" LILY POND*</p> <p>①A HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19, N70, 6 1/4" N. RIDGEFIELD*</p> <p>①B HMA ASPHALT PAVT (FULL DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19, N70, 7" US ROUTE 14 RESURFACING</p> <p>①C HMA SURFACE COURSE, MIX "D", N70 1 3/4" POLY LEV BIND MM IL-4.75 N50 3/4"</p> | <p>② MULTI-USE PATH</p> <p>HMA SURFACE COURSE, MIX "D", N50, 1 1/2" HMA BINDER COURSE, IL-19, N50, 2 1/2" AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 & 26 SHEET 3)</p> <p>③ N. RIDGEFIELD RESURFACING</p> <p>HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES</p> <p>③A LILY POND RESURFACING</p> <p>HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES</p> | <p>LILY POND WIDENING*</p> <p>④ HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 6 1/4" N. RIDGEFIELD WIDENING*</p> <p>④A HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 7" PRIVATE ENTRANCE DRIVEWAY</p> <p>⑤ HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 6" COMMERCIAL ENTRANCE DRIVEWAY</p> <p>⑥ HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 8"</p> | <p>FIELD ENTRANCE DRIVEWAY</p> <p>⑥A HMA SURFACE COURSE, MIX "D", N50, 2" AGGREGATE BASE COURSE, TYPE B 8"</p> <p>⑦ HMA SHOULDERS, 8"</p> <p>⑧ PCC SHOULDERS, 9 3/4"</p> <p>⑨ CONCRETE MEDIAN SURFACE 4"</p> <p>⑩ AGGREGATE SHOULDERS, TYPE B 6"</p> <p>⑪ LANDSCAPE MEDIAN (SEE LANDSCAPING PLANS)</p> <p>⑫ AGGREGATE SHOULDERS, TYPE B 8"</p> <p>⑬ AGGREGATE WEDGE SHOULDER, TYPE B</p> |
|---|---|--|--|

NOTE
 1. THE PORTION OF THE PROPOSED FIELD ENTRANCE BEYOND THE PROPOSED R.O.W. SHALL BE CONSTRUCTED AS AGGREGATE BASE COURSE, TYPE B 8" (50 YD).



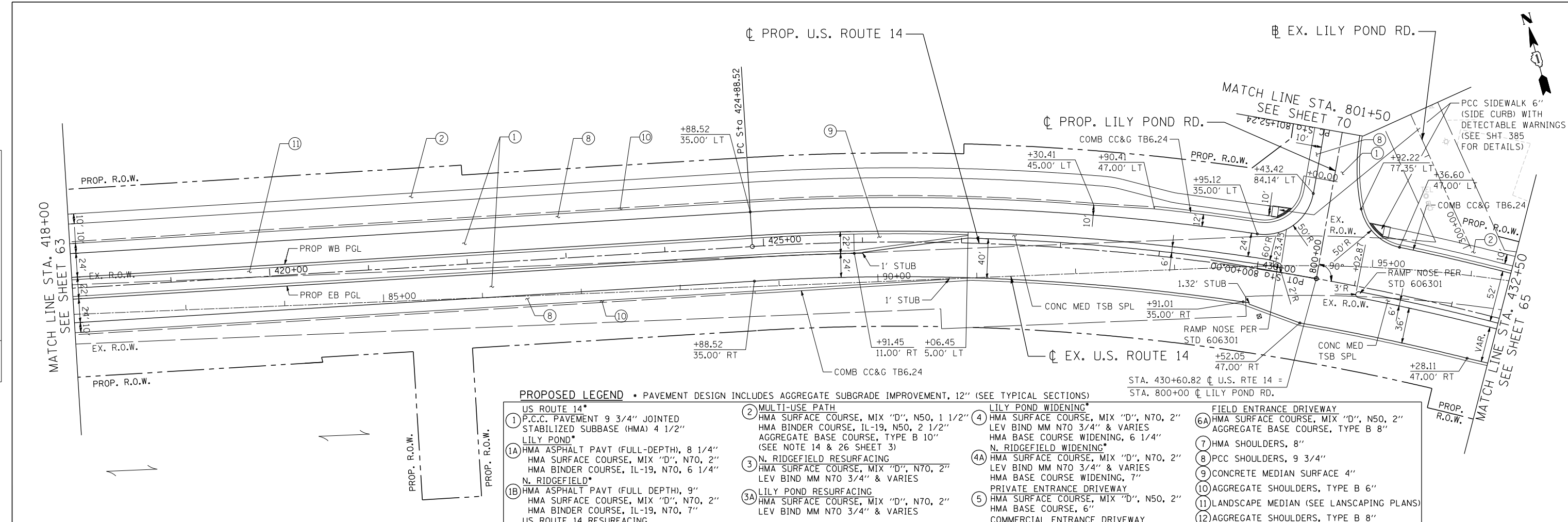
FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
#FILEL#		DRAWN - RJS	REVISED -
		CHECKED - BRH	REVISED -
		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS		PROPOSED ROADWAY	
DEPARTMENT OF TRANSPORTATION		U.S. ROUTE 14 PLAN & PROFILE	
U.S. ROUTE 14		STA. 404+00 TO STA. 418+00	
SCALE: HORIZ. 1"=50'	VERT. 1"=5'	STA.	TO STA.

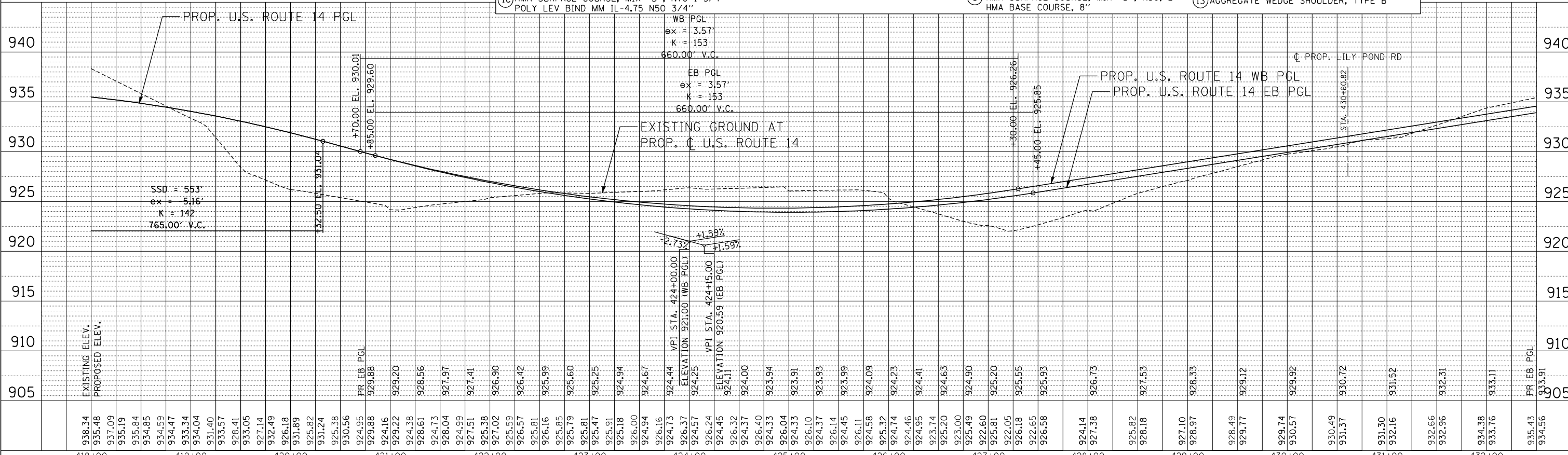
F.A.P. RTE. 305	SECTION 27R-2	COUNTY MCHENRY	TOTAL SHEETS 63	SHEET NO. 63
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	AT		
	ALIGNED		
	MAX		
	CADD		
	FILE		
	NAME		
	NO.		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES		
	CHECKED		
	STRUCTURE		
	NOTATIONS		
	CHFD		
	NO.		



- PROPOSED LEGEND** • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)
- ① US ROUTE 14*
 - P.C.C. PAVEMENT 9 3/4" JOINTED STABILIZED SUBBASE (HMA) 4 1/2"
 - LILY POND*
 - (A) HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19, N70, 6 1/4" N. RIDGEFIELD*
 - (B) HMA ASPHALT PAVT (FULL DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19, N70, 7" N. RIDGEFIELD*
 - (C) US ROUTE 14 RESURFACING HMA SURFACE COURSE, MIX "D", N70 1 3/4" POLY LEV BIND MM IL-4.75 N50 3/4"
 - ② MULTI-USE PATH
 - HMA SURFACE COURSE, MIX "D", N50, 1 1/2"
 - HMA BINDER COURSE, IL-19, N50, 2 1/2"
 - AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 & 26 SHEET 3)
 - ③ N. RIDGEFIELD RESURFACING
 - HMA SURFACE COURSE, MIX "D", N70, 2"
 - LEV BIND MM N70 3/4" & VARIES
 - ③A LILY POND RESURFACING
 - HMA SURFACE COURSE, MIX "D", N70, 2"
 - LEV BIND MM N70 3/4" & VARIES
 - ④ LILY POND WIDENING*
 - HMA SURFACE COURSE, MIX "D", N70, 2"
 - LEV BIND MM N70 3/4" & VARIES
 - HMA BASE COURSE WIDENING, 6 1/4" N. RIDGEFIELD WIDENING*
 - ④A N. RIDGEFIELD WIDENING*
 - HMA SURFACE COURSE, MIX "D", N70, 2"
 - LEV BIND MM N70 3/4" & VARIES
 - HMA BASE COURSE WIDENING, 7"
 - ⑤ PRIVATE ENTRANCE DRIVEWAY
 - HMA SURFACE COURSE, MIX "D", N50, 2"
 - HMA BASE COURSE, 6"
 - ⑥ COMMERCIAL ENTRANCE DRIVEWAY
 - HMA SURFACE COURSE, MIX "D", N50, 2"
 - HMA BASE COURSE, 8"
 - ⑥A FIELD ENTRANCE DRIVEWAY
 - HMA SURFACE COURSE, MIX "D", N50, 2"
 - AGGREGATE BASE COURSE, TYPE B 8"
 - ⑦ HMA SHOULDERS, 8"
 - ⑧ PCC SHOULDERS, 9 3/4"
 - ⑨ CONCRETE MEDIAN SURFACE 4"
 - ⑩ AGGREGATE SHOULDERS, TYPE B 6"
 - ⑪ LANDSCAPE MEDIAN (SEE LANSCAPING PLANS)
 - ⑫ AGGREGATE SHOULDERS, TYPE B 8"
 - ⑬ AGGREGATE WEDGE SHOULDER, TYPE B



FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
#FILE#		DRAWN - RJS	REVISED -
		CHECKED - BRH	REVISED -
		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS		PROPOSED ROADWAY	
DEPARTMENT OF TRANSPORTATION		U.S. ROUTE 14 PLAN & PROFILE	
U.S. ROUTE 14		STA. 418+00 TO STA. 432+50	
SCALE: HORIZ. 1"=50'	VERT. 1"=5'	STA.	TO STA.

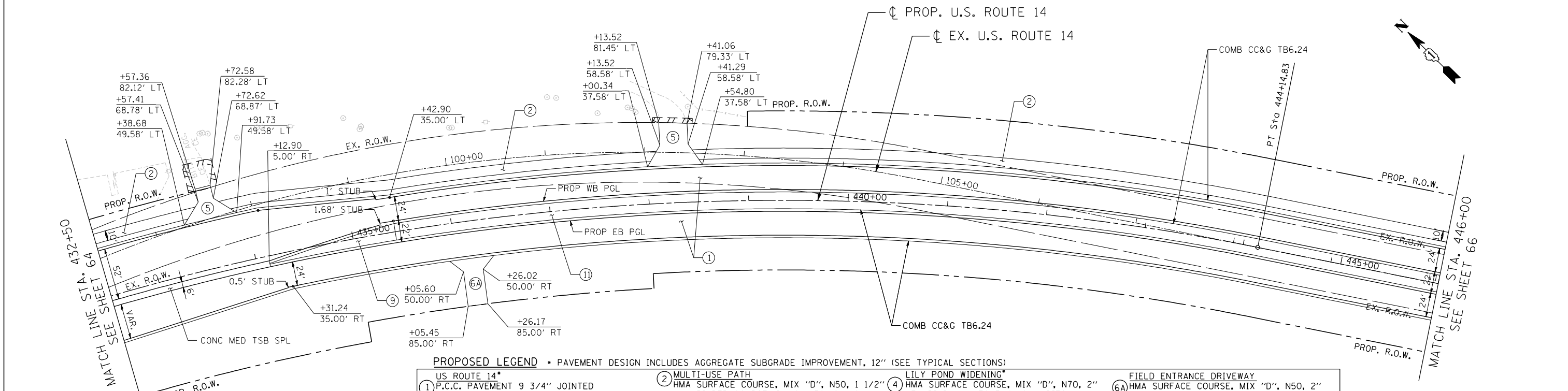
F.A.P. RTE. 305	SECTION 27R-2	COUNTY MCHENRY	TOTAL SHEETS 673	SHEET NO. 64
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

11-07-2013 16:29:43 HECHTBR
 T:\DOCUMENT\13134390\CIVIL\UDR\IN\0817.DGN

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	AT		
	FILE NAME		
	NO.		

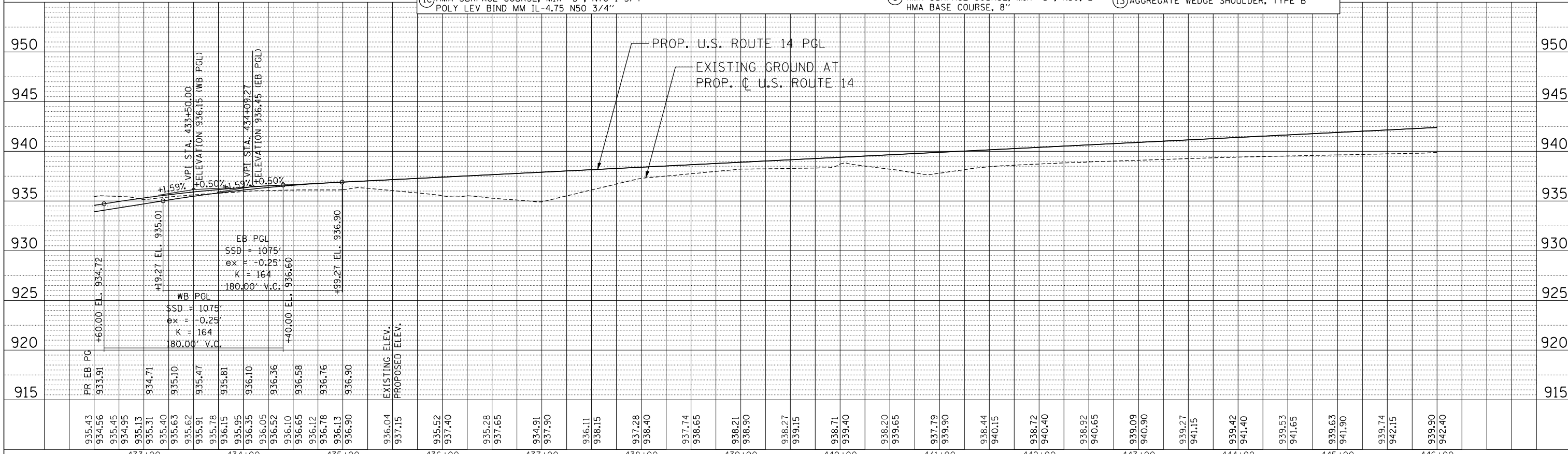
PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	AT		
	FILE NAME		
	NO.		

11-07-2013 16:23:59 HECHTBR
 T:\DOCUMENT\13134590\CIVIL\UDN\13134590.DGN



PROPOSED LEGEND • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)

① US ROUTE 14* P.C.C. PAVEMENT 9 3/4" JOINTED STABILIZED SUBBASE (HMA) 4 1/2"	② MULTI-USE PATH HMA SURFACE COURSE, MIX "D", N50, 1 1/2" HMA BINDER COURSE, IL-19, N50, 2 1/2" AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 & 26 SHEET 3)	④ LILY POND WIDENING* HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 6 1/4"	⑥A FIELD ENTRANCE DRIVEWAY HMA SURFACE COURSE, MIX "D", N50, 2" AGGREGATE BASE COURSE, TYPE B 8"
①A HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19, N70, 6 1/4" N. RIDGEFIELD*	③ N. RIDGEFIELD RESURFACING HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES	④A HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 7"	⑦ HMA SHOULDERS, 8"
①B HMA ASPHALT PAVT (FULL DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19, N70, 7"	③A LILY POND RESURFACING HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES	⑤ PRIVATE ENTRANCE DRIVEWAY HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 6"	⑧ PCC SHOULDERS, 9 3/4"
①C US ROUTE 14 RESURFACING HMA SURFACE COURSE, MIX "D", N70 1 3/4" POLY LEV BIND MM IL-4.75 N50 3/4"		⑥ COMMERCIAL ENTRANCE DRIVEWAY HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 8"	⑨ CONCRETE MEDIAN SURFACE 4"
			⑩ AGGREGATE SHOULDERS, TYPE B 6"
			⑪ LANDSCAPE MEDIAN (SEE LANSAPING PLANS)
			⑫ AGGREGATE SHOULDERS, TYPE B 8"
			⑬ AGGREGATE WEDGE SHOULDER, TYPE B



FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
#FILE#		DRAWN - RJS	REVISED -
PLOT SCALE = *SCALE*		CHECKED - BRH	REVISED -
PLOT DATE = *DATE*		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

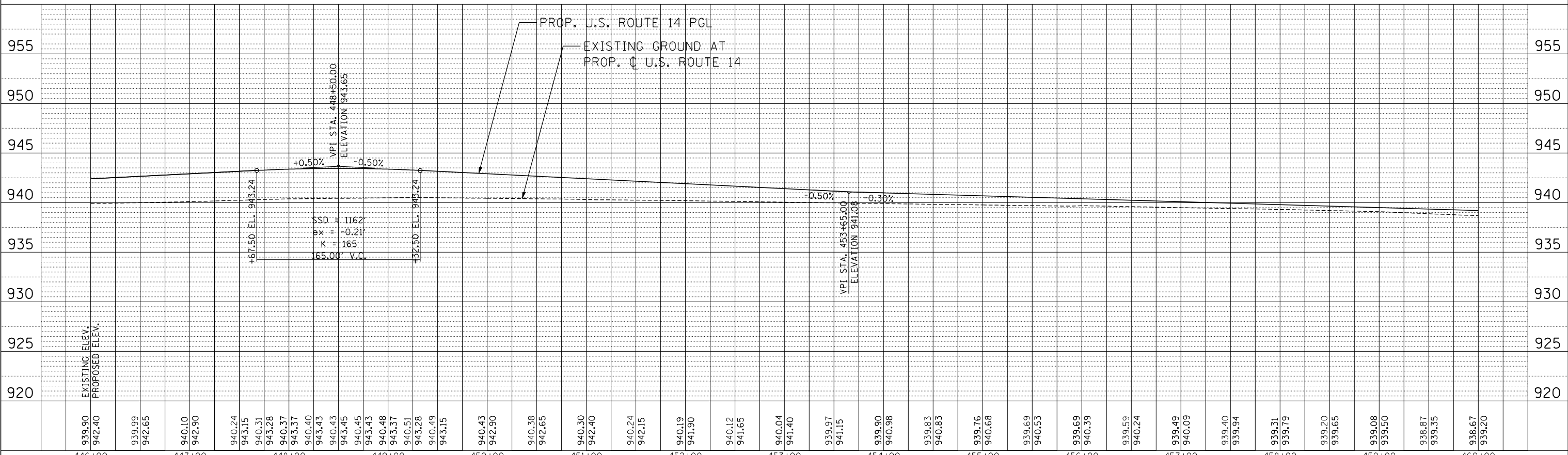
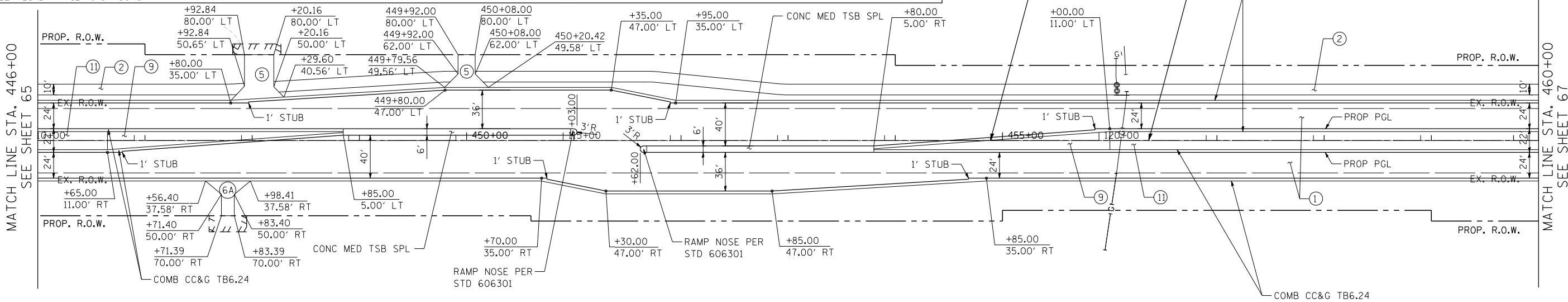
PROPOSED ROADWAY
U.S. ROUTE 14 PLAN & PROFILE
STA. 432+50 TO STA. 446+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	65
CONTRACT NO. 62268				

SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA. FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

PROPOSED LEGEND • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)

- ① US ROUTE 14*
P.C.C. PAVEMENT 9 3/4" JOINTED
STABILIZED SUBBASE (HMA) 4 1/2"
- ② MULTI-USE PATH
HMA SURFACE COURSE, MIX "D", N50, 1 1/2"
HMA BINDER COURSE, IL-19, N50, 2 1/2"
AGGREGATE BASE COURSE, TYPE B 10"
(SEE NOTE 14 & 26 SHEET 3)
- ③ N. RIDGEFIELD RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
- ④ LILY POND WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 6 1/4"
N. RIDGEFIELD WIDENING*
- ⑤ FIELD ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
AGGREGATE BASE COURSE, TYPE B 8"
- ⑥A HMA SHOULDERS, 8"
- ⑦ HMA SHOULDERS, 8"
- ⑧ PCC SHOULDERS, 9 3/4"
- ⑨ CONCRETE MEDIAN SURFACE 4"
- ⑩ AGGREGATE SHOULDERS, TYPE B 6"
- ⑪ LANDSCAPE MEDIAN (SEE LANSCAPING PLANS)
- ⑫ AGGREGATE SHOULDERS, TYPE B 8"
- ⑬ AGGREGATE WEDGE SHOULDER, TYPE B
- ①A HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 6 1/4"
- ①B N. RIDGEFIELD*
HMA ASPHALT PAVT (FULL DEPTH), 9"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 7"
- ③A LILY POND RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
- ⑤ US ROUTE 14 RESURFACING
HMA SURFACE COURSE, MIX "D", N70 1 3/4"
POLY LEV BIND MM IL-4.75 N50 3/4"



PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	AT		
	FILE NAME		
	NO.		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	AT		
	FILE NAME		
	NO.		

11-07-2013 16:29:57 HECHTBR
 T:\DOCUMENT\13134290\CIVIL\UDN\PR0813...DGN

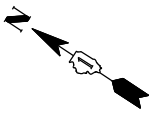
FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
#FILE#		DRAWN - RJS	REVISED -
		CHECKED - BRH	REVISED -
		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

PROPOSED ROADWAY
 U.S. ROUTE 14 PLAN & PROFILE
 STA. 446+00 TO STA. 460+00

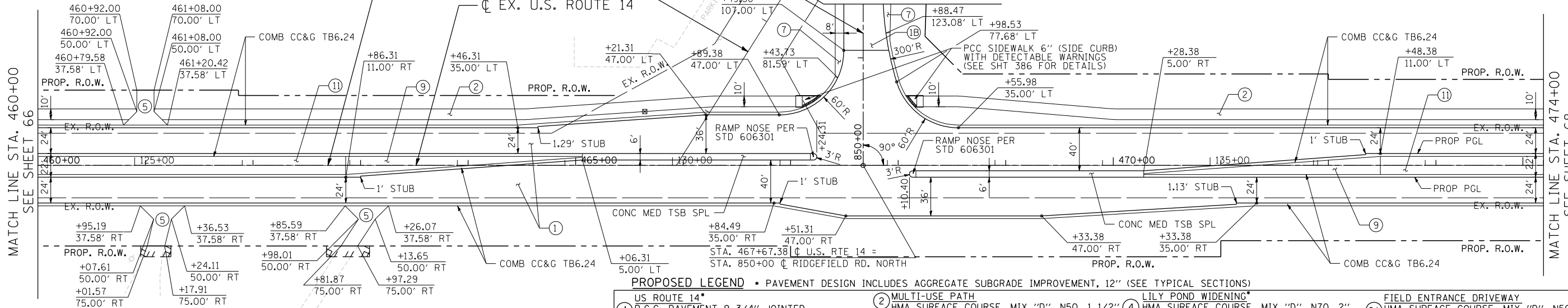
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	66
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62268	

SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA.



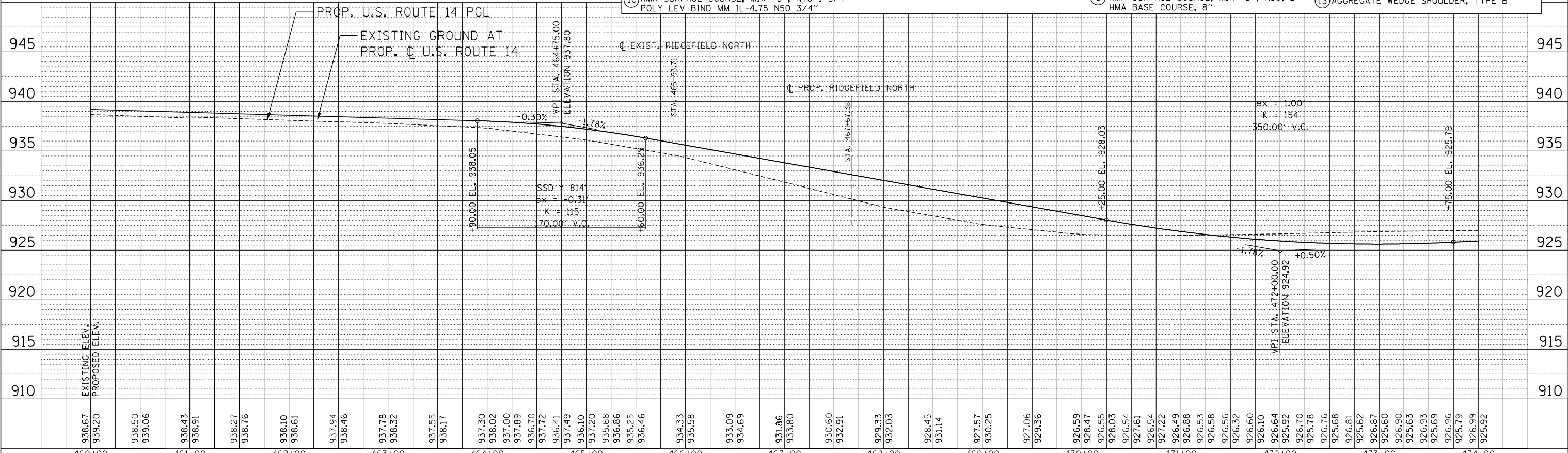
DATE	
BY	
PLAN	SURVEYED
	NOTED
	CHECKED
	AT
	FILE NAME
	NO.

DATE	
BY	
PROFILE	SURVEYED
	NOTED
	CHECKED
	AT
	FILE NAME
	NO.



PROPOSED LEGEND - PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)

<p>US ROUTE 14*</p> <p>① P.C.C. PAVEMENT 9 3/4" JOINTED STABILIZED SUBBASE (HMA) 4 1/2" LILY POND*</p> <p>①A HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19, N70, 6 1/4" N. RIDGEFIELD*</p> <p>①B HMA ASPHALT PAVT (FULL DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19, N70, 7" US ROUTE 14 RESURFACING</p> <p>①C HMA SURFACE COURSE, MIX "D", N70 1 3/4" POLY LEV BIND MM IL-4.75 N50 3/4"</p>	<p>② MULTI-USE PATH</p> <p>HMA SURFACE COURSE, MIX "D", N50, 1 1/2" HMA BINDER COURSE, IL-19, N50, 2 1/2" AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 & 26 SHEET 3)</p> <p>③ N. RIDGEFIELD RESURFACING</p> <p>HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES</p> <p>③A LILY POND RESURFACING</p> <p>HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES</p>	<p>LILY POND WIDENING*</p> <p>④ HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 6 1/4" N. RIDGEFIELD WIDENING*</p> <p>④A HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 7" PRIVATE ENTRANCE DRIVEWAY</p> <p>⑤ HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 6" COMMERCIAL ENTRANCE DRIVEWAY</p> <p>⑥ HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 8"</p>	<p>FIELD ENTRANCE DRIVEWAY</p> <p>⑥A HMA SURFACE COURSE, MIX "D", N50, 2" AGGREGATE BASE COURSE, TYPE B 8"</p> <p>⑦ HMA SHOULDERS, 8"</p> <p>⑧ PCC SHOULDERS, 9 3/4"</p> <p>⑨ CONCRETE MEDIAN SURFACE 4"</p> <p>⑩ AGGREGATE SHOULDERS, TYPE B 6"</p> <p>⑪ LANDSCAPE MEDIAN (SEE LANSCAPING PLANS)</p> <p>⑫ AGGREGATE SHOULDERS, TYPE B 8"</p> <p>⑬ AGGREGATE WEDGE SHOULDER, TYPE B</p>
---	---	--	---



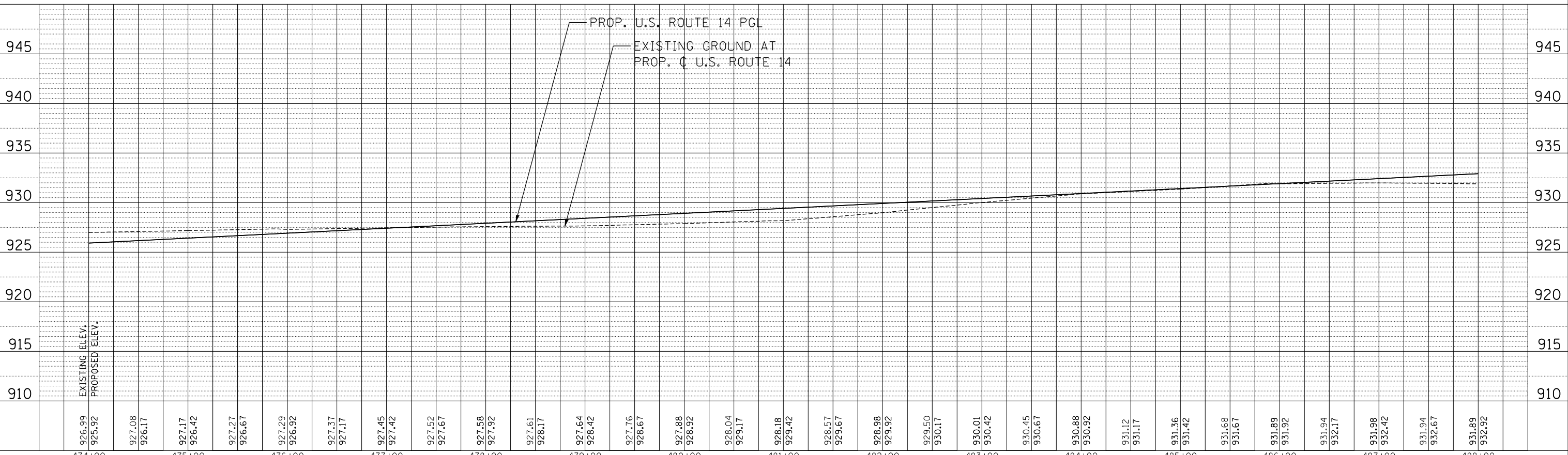
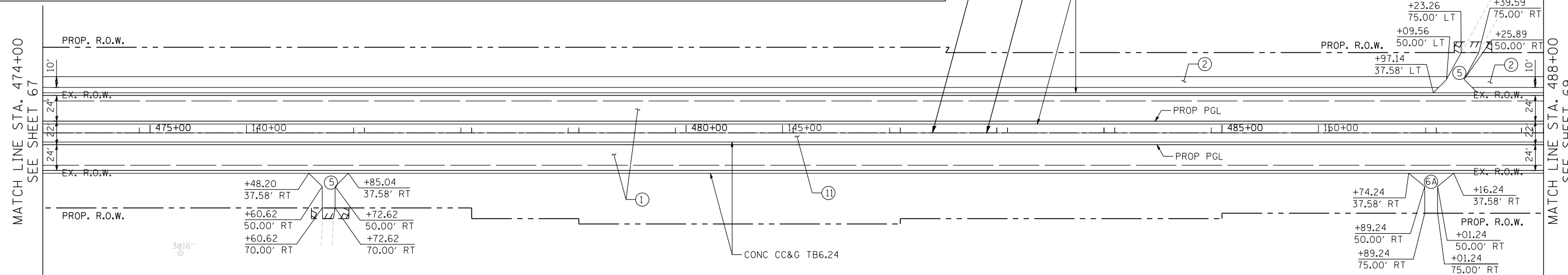
11-07-2013 16:24:07 HECHTBR T:\DOCUMENTS\1343498\1\CIVIL\UDR\1343498.dgn

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	PROPOSED ROADWAY U.S. ROUTE 14 PLAN & PROFILE STA. 460+00 TO STA. 474+00			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*FILE.L.S		DRAWN - RJS	REVISED -		305	27R-2	MCHENRY	673	67			
PLOT SCALE = *SCALE*		CHECKED - BRH	REVISED -		CONTRACT NO. 62268							
PLOT DATE = *DATE*		DATE - 11/01/13	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA.

PROPOSED LEGEND • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)

- | | | | |
|---|--|---|--|
| ① US ROUTE 14*
P.C.C. PAVEMENT 9 3/4" JOINTED
STABILIZED SUBBASE (HMA) 4 1/2"
LILY POND* | ② MULTI-USE PATH
HMA SURFACE COURSE, MIX "D", N50, 1 1/2"
HMA BINDER COURSE, IL-19, N50, 2 1/2"
AGGREGATE BASE COURSE, TYPE B 10"
(SEE NOTE 14 & 26 SHEET 3) | ④ LILY POND WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 6 1/4"
N. RIDGEFIELD WIDENING* | ⑥A FIELD ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
AGGREGATE BASE COURSE, TYPE B 8" |
| ①A HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 6 1/4"
N. RIDGEFIELD* | ③ N. RIDGEFIELD RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES | ④A HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 7" | ⑦ HMA SHOULDERS, 8" |
| ①B HMA ASPHALT PAVT (FULL DEPTH), 9"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 7" | ③A LILY POND RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES | ⑤ PRIVATE ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 6" | ⑧ PCC SHOULDERS, 9 3/4" |
| ①C US ROUTE 14 RESURFACING
HMA SURFACE COURSE, MIX "D", N70 1 3/4"
POLY LEV BIND MM IL-4.75 N50 3/4" | | ⑥ COMMERCIAL ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 8" | ⑨ CONCRETE MEDIAN SURFACE 4" |
| | | | ⑩ AGGREGATE SHOULDERS, TYPE B 6" |
| | | | ⑪ LANDSCAPE MEDIAN (SEE LANSCAPING PLANS) |
| | | | ⑫ AGGREGATE SHOULDERS, TYPE B 8" |
| | | | ⑬ AGGREGATE WEDGE SHOULDER, TYPE B |



FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
#FILE#		DRAWN - RJS	REVISED -
exp U.S. Services Inc.		CHECKED - BRH	REVISED -
11-07-2013 16:24:18		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
U.S. ROUTE 14

PROPOSED ROADWAY
U.S. ROUTE 14 PLAN & PROFILE
STA. 474+00 TO STA. 488+00

SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA. F.A.P. RTE. 305 SECTION 27R-2 COUNTY MCHENRY TOTAL SHEETS 673 SHEET NO. 68 CONTRACT NO. 62268 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	CHECKED	
	AT	
	FILE NAME	
	CARD	

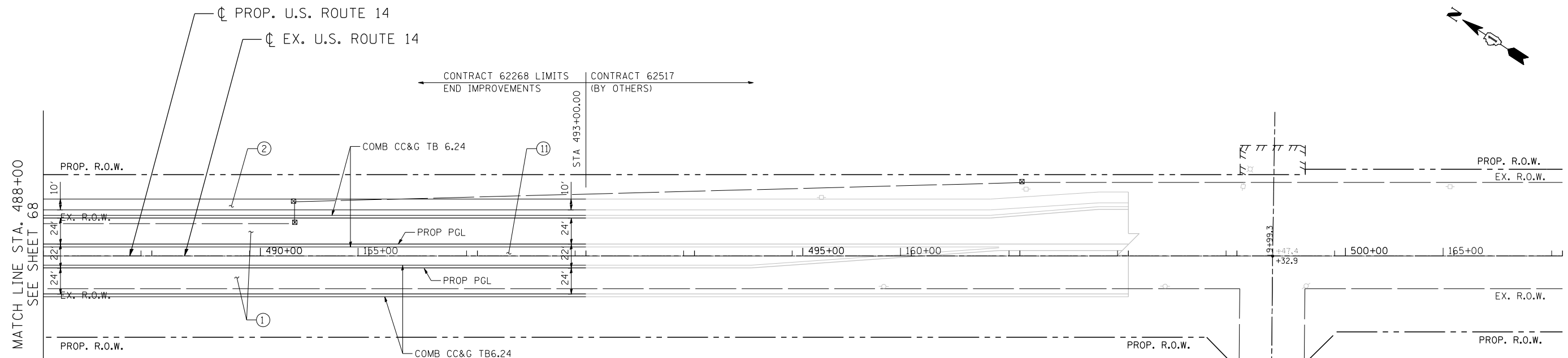
PROFILE	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	CHECKED	
	AT	
	FILE NAME	
	CARD	

11-07-2013 16:24:18 HECHTBR
T:\DOCUMENT\13154598\1\CIVIL\UDN\13154598\084K6.DGN

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	AT		
	FILE NAME		
	NO.		

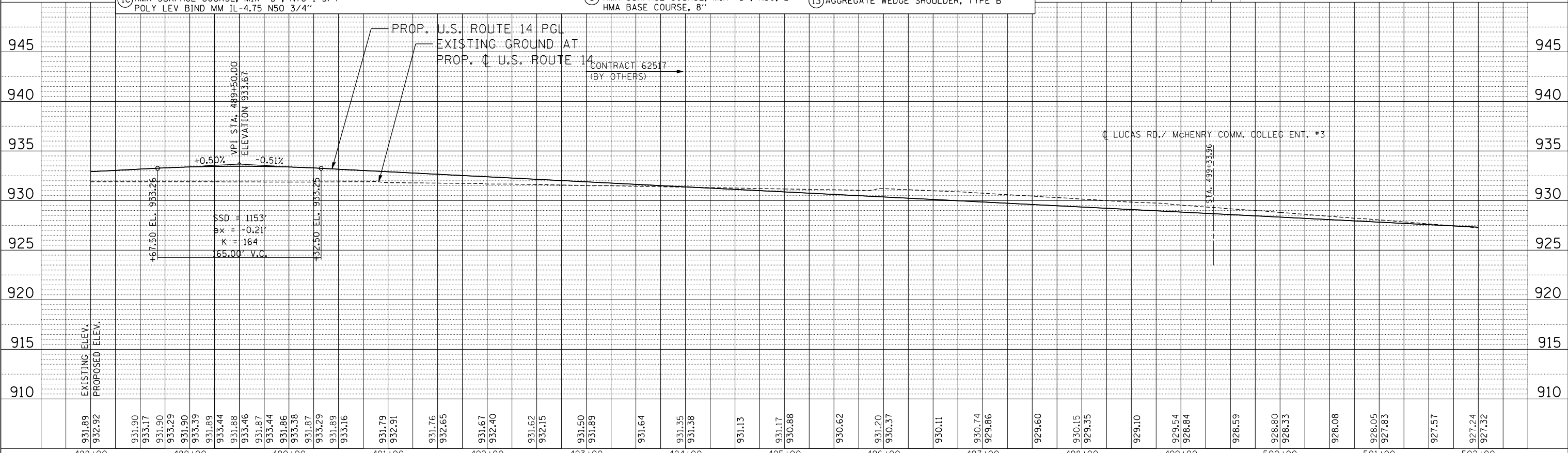
PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	AT		
	FILE NAME		
	NO.		

11-07-2013 16:24:17 HECHTBR
 TA:DOCUMENT\13143790\CIVIL\UDN\PR08\KLD.DGN



PROPOSED LEGEND • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)

<p>US ROUTE 14*</p> <p>① P.C.C. PAVEMENT 9 3/4" JOINTED STABILIZED SUBBASE (HMA) 4 1/2" LILY POND*</p> <p>①A HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19, N70, 6 1/4" N. RIDGEFIELD*</p> <p>①B HMA ASPHALT PAVT (FULL DEPTH), 9" HMA SURFACE COURSE, MIX "D", N70, 2" HMA BINDER COURSE, IL-19, N70, 7" US ROUTE 14 RESURFACING</p> <p>①C HMA SURFACE COURSE, MIX "D", N70 1 3/4" POLY LEV BIND MM IL-4.75 N50 3/4"</p>	<p>MULTI-USE PATH</p> <p>② HMA SURFACE COURSE, MIX "D", N50, 1 1/2" HMA BINDER COURSE, IL-19, N50, 2 1/2" AGGREGATE BASE COURSE, TYPE B 10" (SEE NOTE 14 & 26 SHEET 3)</p> <p>N. RIDGEFIELD RESURFACING</p> <p>③ HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES</p> <p>LILY POND RESURFACING</p> <p>③A HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES</p>	<p>LILY POND WIDENING*</p> <p>④ HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 6 1/4" N. RIDGEFIELD WIDENING*</p> <p>④A HMA SURFACE COURSE, MIX "D", N70, 2" LEV BIND MM N70 3/4" & VARIES HMA BASE COURSE WIDENING, 7" PRIVATE ENTRANCE DRIVEWAY</p> <p>⑤ HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 6" COMMERCIAL ENTRANCE DRIVEWAY</p> <p>⑥ HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 8"</p>	<p>FIELD ENTRANCE DRIVEWAY</p> <p>⑥A HMA SURFACE COURSE, MIX "D", N50, 2" AGGREGATE BASE COURSE, TYPE B 8"</p> <p>⑦ HMA SHOULDERS, 8"</p> <p>⑧ PCC SHOULDERS, 9 3/4"</p> <p>⑨ CONCRETE MEDIAN SURFACE 4"</p> <p>⑩ AGGREGATE SHOULDERS, TYPE B 6"</p> <p>⑪ LANDSCAPE MEDIAN (SEE LANSCAPING PLANS)</p> <p>⑫ AGGREGATE SHOULDERS, TYPE B 8"</p> <p>⑬ AGGREGATE WEDGE SHOULDER, TYPE B</p>
---	---	--	---



FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
#FILEL#		DRAWN - RJS	REVISED -
PLOT SCALE = *SCALE*		CHECKED - BRH	REVISED -
PLOT DATE = *DATE*		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

PROPOSED ROADWAY
U.S. ROUTE 14 PLAN & PROFILE
STA. 488+00 TO STA. 502+00

SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA.

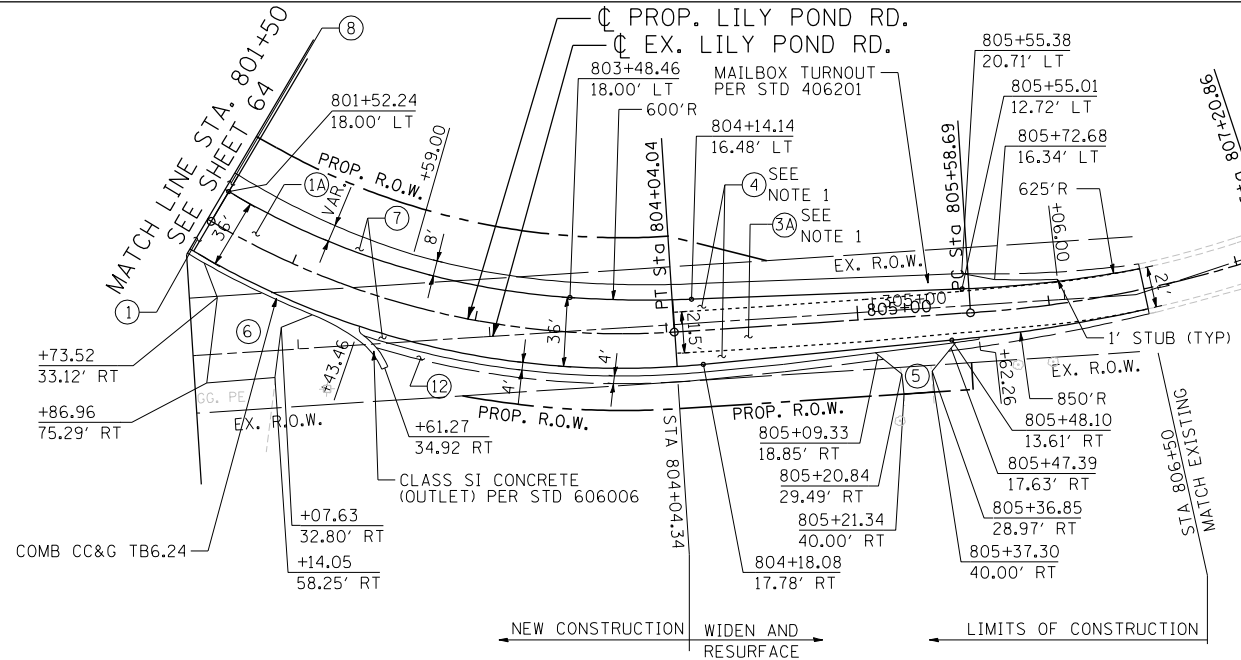
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	69
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62268	

PROPOSED LEGEND • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)

- | | | | |
|--|--|--|---|
| <p>① US ROUTE 14*
P.C.C. PAVEMENT 9 3/4" JOINTED
STABILIZED SUBBASE (HMA) 4 1/2"</p> <p>①A LILY POND*
HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 6 1/4"
N. RIDGEFIELD*</p> <p>①B HMA ASPHALT PAVT (FULL DEPTH), 9"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 7"
US ROUTE 14 RESURFACING
HMA SURFACE COURSE, MIX "D", N70 1 3/4"
POLY LEV BIND MM IL-4.75 N50 3/4"</p> | <p>② MULTI-USE PATH
HMA SURFACE COURSE, MIX "D", N50, 1 1/2"
HMA BINDER COURSE, IL-19, N50, 2 1/2"
AGGREGATE BASE COURSE, TYPE B 10"
(SEE NOTE 14 & 26 SHEET 3)</p> <p>③ N. RIDGEFIELD RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES</p> <p>③A LILY POND RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES</p> | <p>④ LILY POND WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 6 1/4"
N. RIDGEFIELD WIDENING*</p> <p>④A HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 7"
PRIVATE ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 6"</p> <p>⑤ COMMERCIAL ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 8"</p> | <p>⑥A FIELD ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
AGGREGATE BASE COURSE, TYPE B 8"</p> <p>⑦ HMA SHOULDERS, 8"</p> <p>⑧ PCC SHOULDERS, 9 3/4"</p> <p>⑨ CONCRETE MEDIAN SURFACE 4"</p> <p>⑩ AGGREGATE SHOULDERS, TYPE B 6"</p> <p>⑪ LANDSCAPE MEDIAN (SEE LANSAPING PLANS)</p> <p>⑫ AGGREGATE SHOULDERS, TYPE B 8"</p> <p>⑬ AGGREGATE WEDGE SHOULDER, TYPE B</p> |
|--|--|--|---|

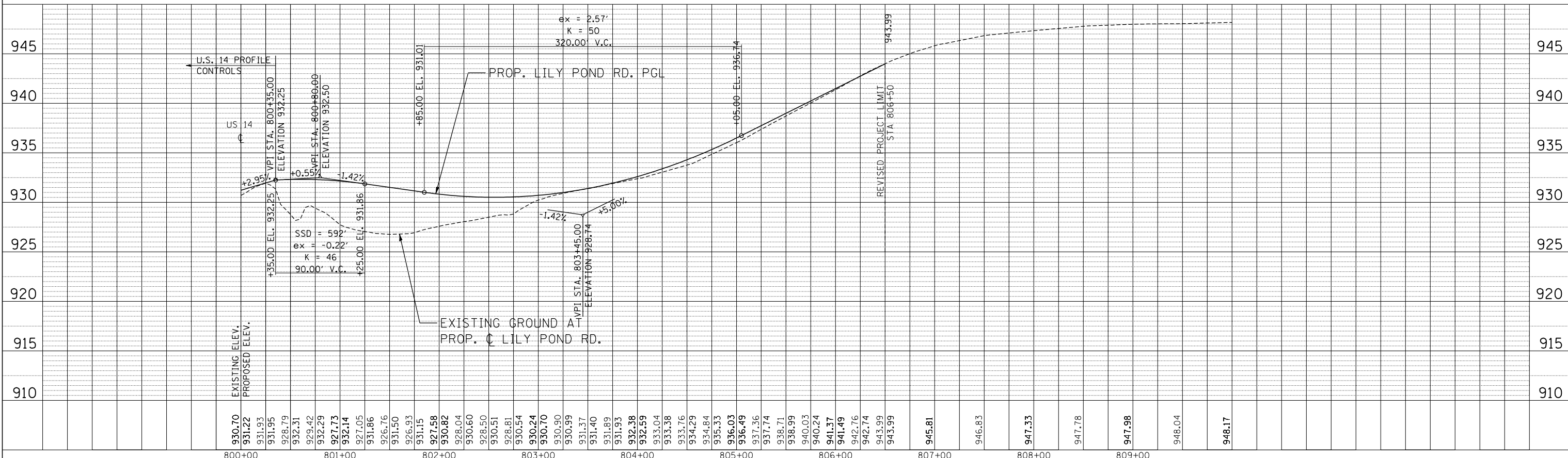
PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	CHECKED	
	AT	
	FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	CHECKED	
	AT	
	FILE NAME	



NOTE

1. LEVELING BINDER (MACHINE METHOD) THICKNESS SHALL VARY BASED ON PROPOSED PROFILE. THICKNESS SHALL BE A MINIMUM 3/4".



11-07-2013 16:24:23 HECHTER
T:\DOCUMENT\151343790\CIVIL\UDN\1P\0808K2.DGN

FILE NAME =	USER NAME =	DESIGNED -	REVISED -
#FILEL#	HECHTR	DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

PLOT SCALE = *SCALE*	CHECKED -	REVISED -
PLOT DATE = *DATE*	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
U.S. ROUTE 14

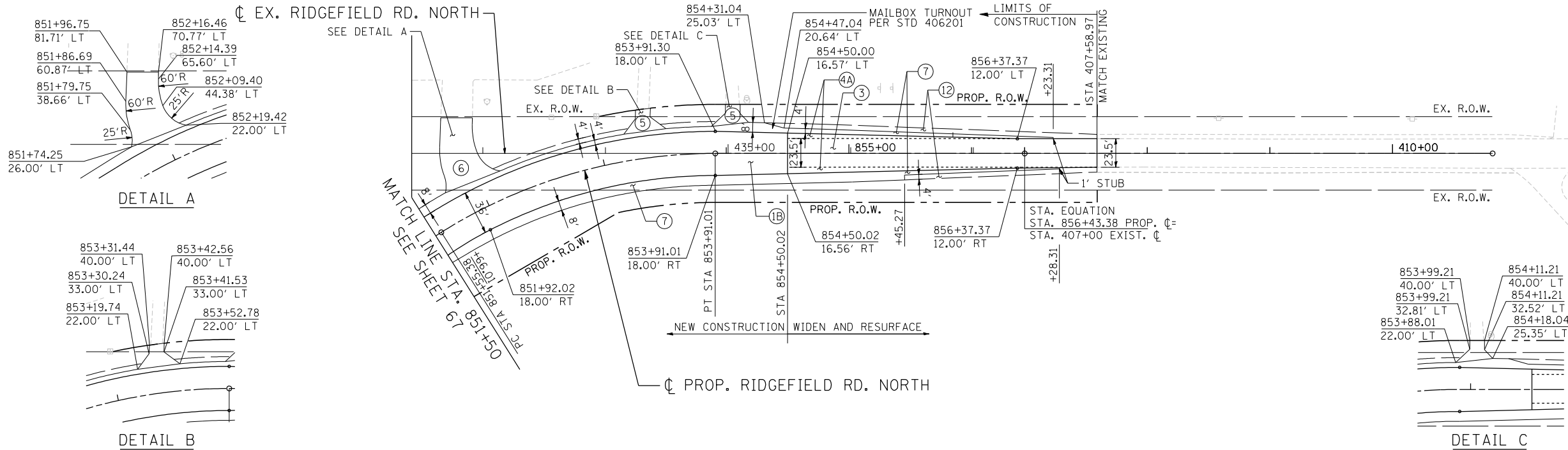
PROPOSED ROADWAY		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
LILY POND ROAD PLAN & PROFILE		305	27R-2	MCHENRY	673	70
STA. 801+50 TO STA. 806+50		CONTRACT NO. 62268				
SCALE: HORIZ. 1"=50'	VERT. 1"=5'	STA.	TO STA.			

FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT	
-----------------------	--	---------------------------	--

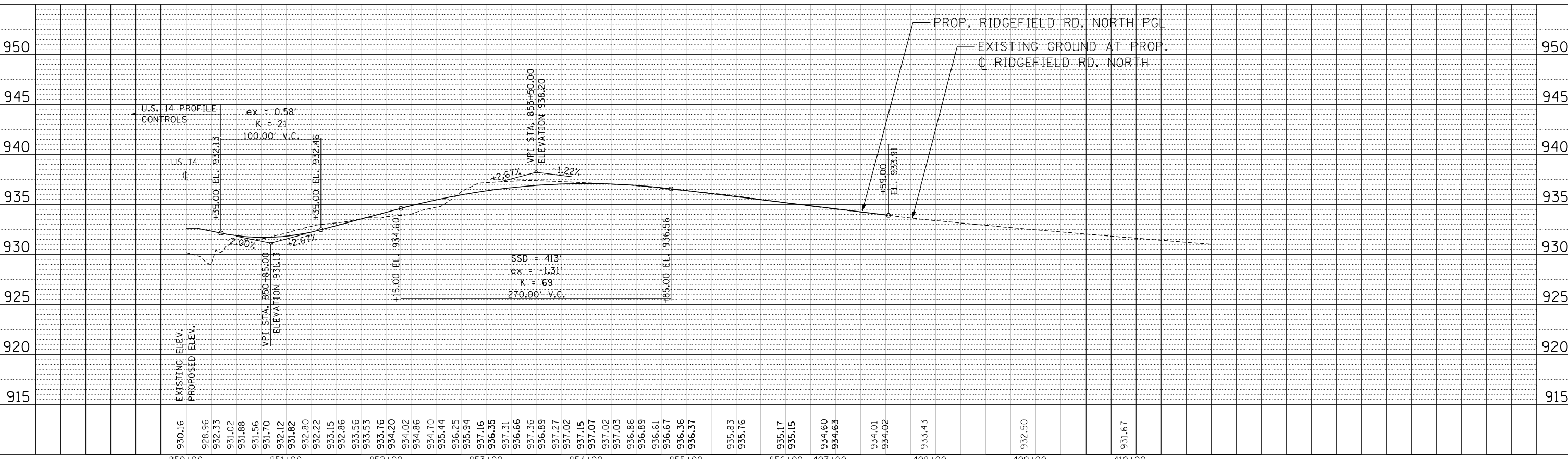
PROPOSED LEGEND • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)

- | | | | |
|--|--|---|---|
| <p>① US ROUTE 14*
P.C.C. PAVEMENT 9 3/4" JOINTED
STABILIZED SUBBASE (HMA) 4 1/2"</p> <p>①A LILY POND*
HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 6 1/4"
N. RIDGEFIELD*</p> <p>①B HMA ASPHALT PAVT (FULL DEPTH), 9"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 7"
US ROUTE 14 RESURFACING</p> <p>①C HMA SURFACE COURSE, MIX "D", N70 1 3/4"
POLY LEV BIND MM IL-4.75 N50 3/4"</p> | <p>② MULTI-USE PATH
HMA SURFACE COURSE, MIX "D", N50, 1 1/2"
HMA BINDER COURSE, IL-19, N50, 2 1/2"
AGGREGATE BASE COURSE, TYPE B 10"
(SEE NOTE 14 & 26 SHEET 3)</p> <p>③ N. RIDGEFIELD RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES</p> <p>③A LILY POND RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES</p> | <p>④ LILY POND WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 6 1/4"
N. RIDGEFIELD WIDENING*</p> <p>④A HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 7"
PRIVATE ENTRANCE DRIVEWAY</p> <p>⑤ HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 6"
COMMERCIAL ENTRANCE DRIVEWAY</p> <p>⑥ HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 8"</p> | <p>FIELD ENTRANCE DRIVEWAY
⑥A HMA SURFACE COURSE, MIX "D", N50, 2"
AGGREGATE BASE COURSE, TYPE B 8"</p> <p>⑦ HMA SHOULDERS, 8"</p> <p>⑧ PCC SHOULDERS, 9 3/4"</p> <p>⑨ CONCRETE MEDIAN SURFACE 4"</p> <p>⑩ AGGREGATE SHOULDERS, TYPE B 6"</p> <p>⑪ LANDSCAPE MEDIAN (SEE LANSAPING PLANS)</p> <p>⑫ AGGREGATE SHOULDERS, TYPE B 8"</p> <p>⑬ AGGREGATE WEDGE SHOULDER, TYPE B</p> |
|--|--|---|---|

PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	
NO.	CHECKED	
	AT	
	FILE NAME	
	NO.	



PROFILE	SURVEYED	DATE
NOTE BOOK	PLOTTED	
NO.	CHECKED	
	AT	
	FILE NAME	
	NO.	



11-07-2013 16:24:41 HECHTBR T:\DOCUMENT\15134390\CIVIL\UDN\15134390\08R08\08R08.dgn

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
#FILE#		DRAWN - RJS	REVISED -
PLOT SCALE = *SCALE*		CHECKED - BRH	REVISED -
PLOT DATE = *DATE*		DATE - 11/01/13	REVISED -

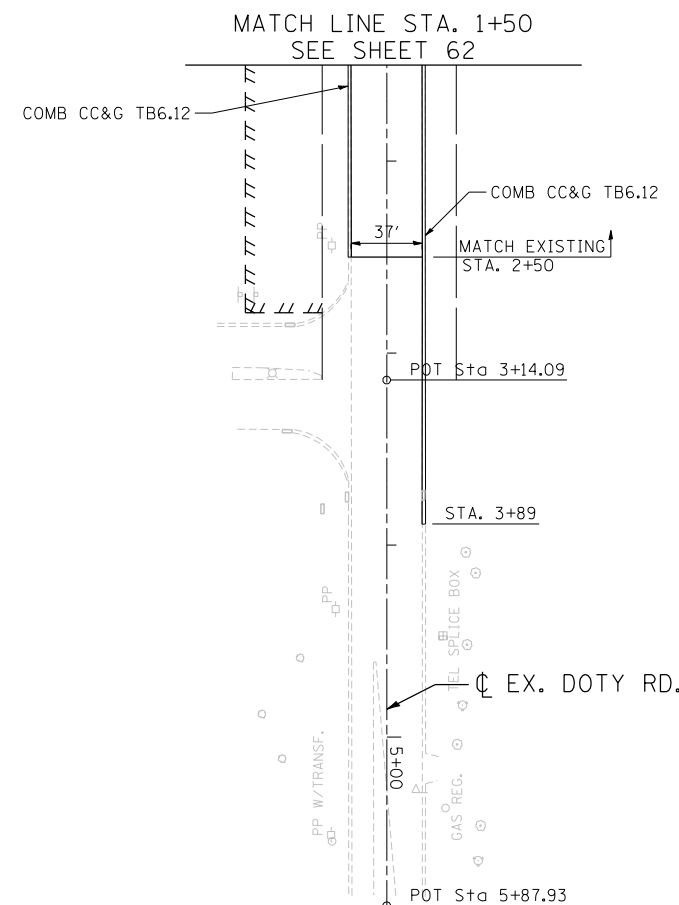
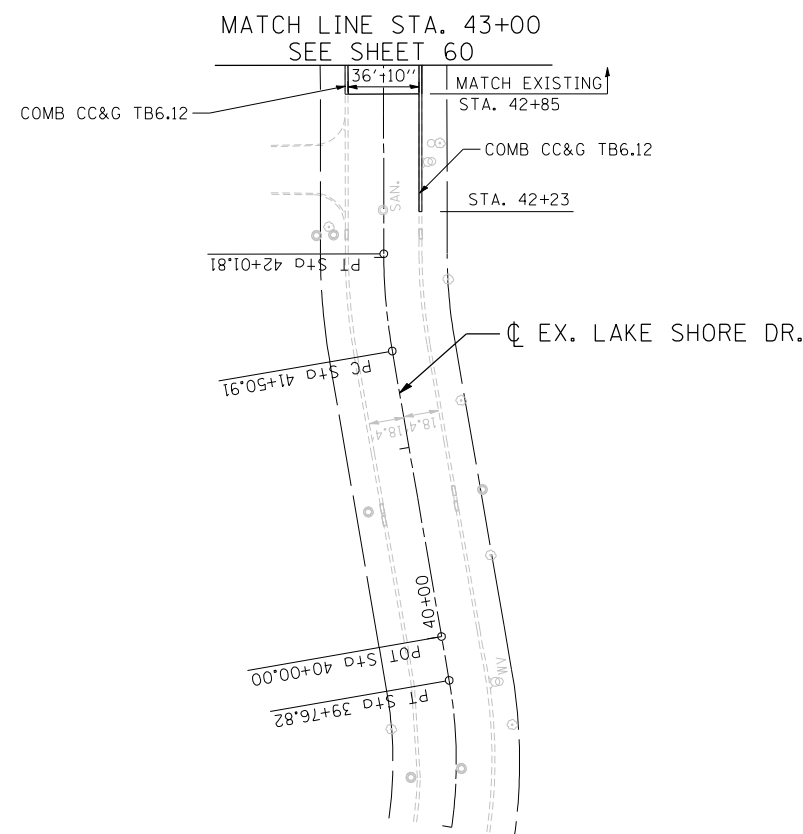
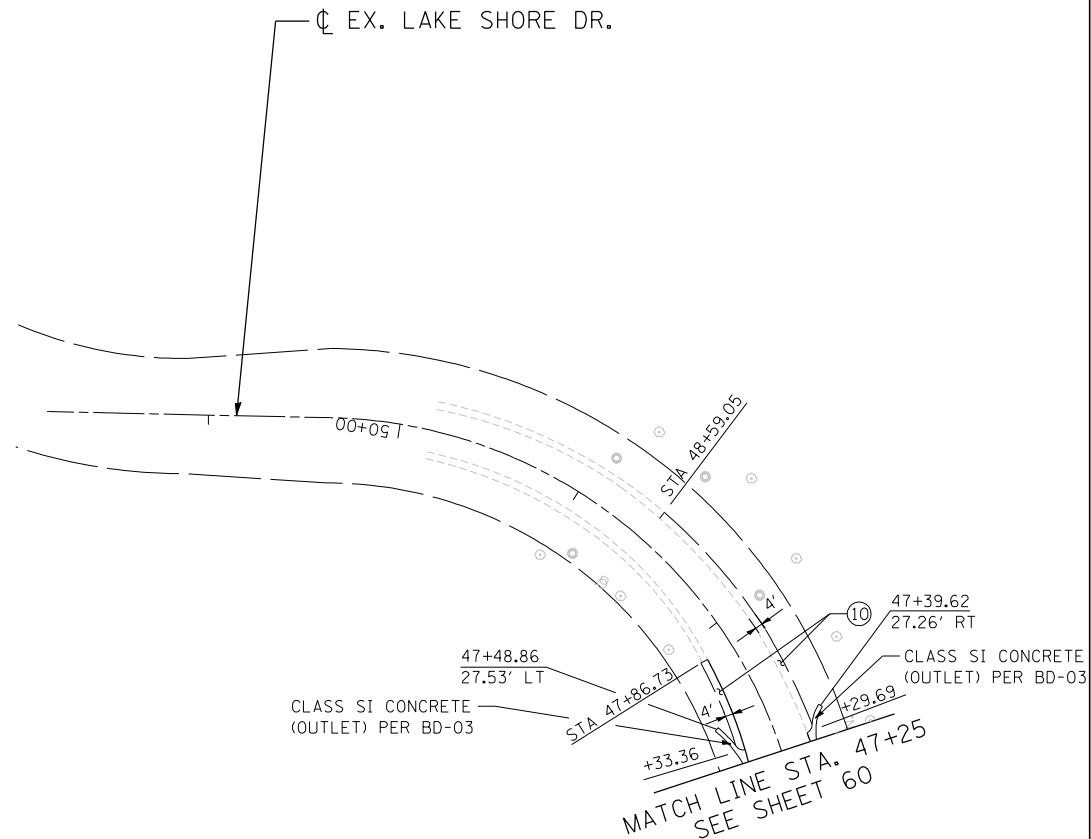
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
U.S. ROUTE 14

PROPOSED ROADWAY
RIDGEFIELD ROAD NORTH PLAN & PROFILE
STA. 851 + 50 TO STA. 857 + 00

SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	71
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62268	





PROPOSED LEGEND • PAVEMENT DESIGN INCLUDES AGGREGATE SUBGRADE IMPROVEMENT, 12" (SEE TYPICAL SECTIONS)

- | | | | |
|---|--|---|---|
| <p>① US ROUTE 14*
P.C.C. PAVEMENT 9 3/4" JOINTED
STABILIZED SUBBASE (HMA) 4 1/2"
LILY POND*</p> <p>①A HMA ASPHALT PAVT (FULL-DEPTH), 8 1/4"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 6 1/4"</p> <p>①B HMA ASPHALT PAVT (FULL DEPTH), 9"
HMA SURFACE COURSE, MIX "D", N70, 2"
HMA BINDER COURSE, IL-19, N70, 7"</p> <p>①C US ROUTE 14 RESURFACING
HMA SURFACE COURSE, MIX "D", N70 1 3/4"
POLY LEV BIND MM IL-4.75 N50 3/4"</p> | <p>② MULTI-USE PATH
HMA SURFACE COURSE, MIX "D", N50, 1 1/2"
HMA BINDER COURSE, IL-19, N50, 2 1/2"
AGGREGATE BASE COURSE, TYPE B 10"
(SEE NOTE 14 & 26 SHEET 3)</p> <p>③ N. RIDGEFIELD RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES</p> <p>③A LILY POND RESURFACING
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES</p> | <p>④ LILY POND WIDENING*
HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 6 1/4"
N. RIDGEFIELD WIDENING*</p> <p>④A HMA SURFACE COURSE, MIX "D", N70, 2"
LEV BIND MM N70 3/4" & VARIES
HMA BASE COURSE WIDENING, 7"</p> <p>⑤ PRIVATE ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 6"</p> <p>⑥ COMMERCIAL ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 8"</p> | <p>⑥A FIELD ENTRANCE DRIVEWAY
HMA SURFACE COURSE, MIX "D", N50, 2"
AGGREGATE BASE COURSE, TYPE B 8"</p> <p>⑦ HMA SHOULDERS, 8"</p> <p>⑧ PCC SHOULDERS, 9 3/4"</p> <p>⑨ CONCRETE MEDIAN SURFACE 4"</p> <p>⑩ AGGREGATE SHOULDERS, TYPE B 6"</p> <p>⑪ LANDSCAPE MEDIAN (SEE LANSCLAPING PLANS)</p> <p>⑫ AGGREGATE SHOULDERS, TYPE B 8"</p> <p>⑬ AGGREGATE WEDGE SHOULDER, TYPE B</p> |
|---|--|---|---|

\P\RD\00815.DGN, \T\TB00815.DGN, \R\RD\00815.DGN, \G\GEN\00817.DGN
 11-07-2013, 16:24:55
 HECHTBR
 T:\DOCUMENT\1934\9901\CIVIL\00815.DGN\PP00.MB.DGN

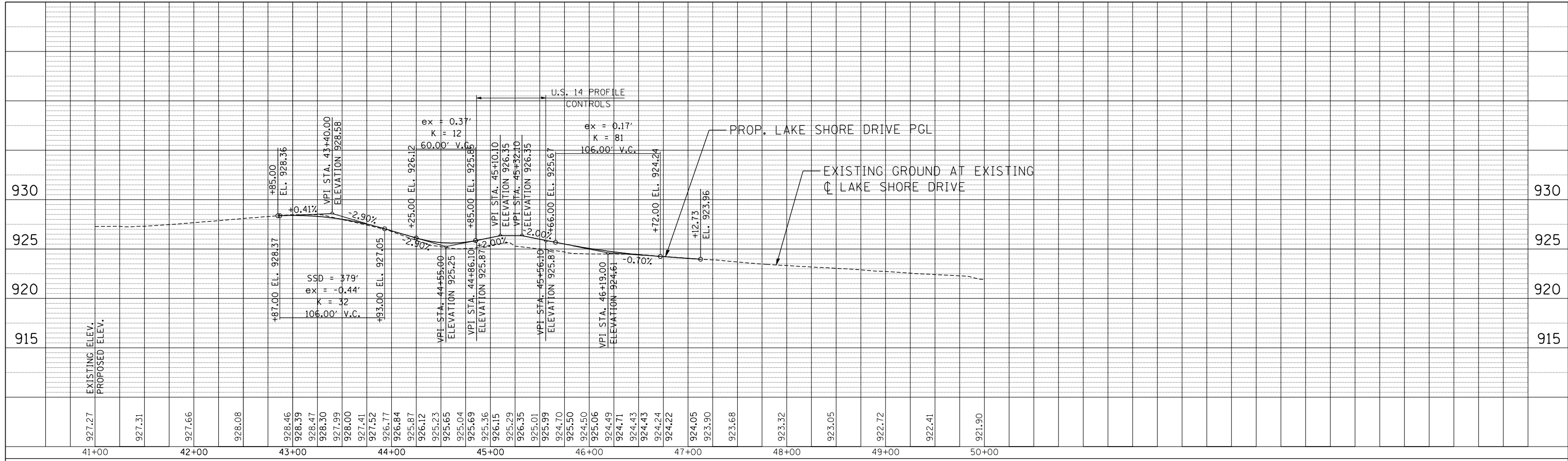
FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
%FILEL#		DRAWN - RJS	REVISED -
exp U.S. Services Inc. Chicago, IL BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT SCALE = %SCALE#	CHECKED - BRH	REVISED -
	PLOT DATE = %DATE#	DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

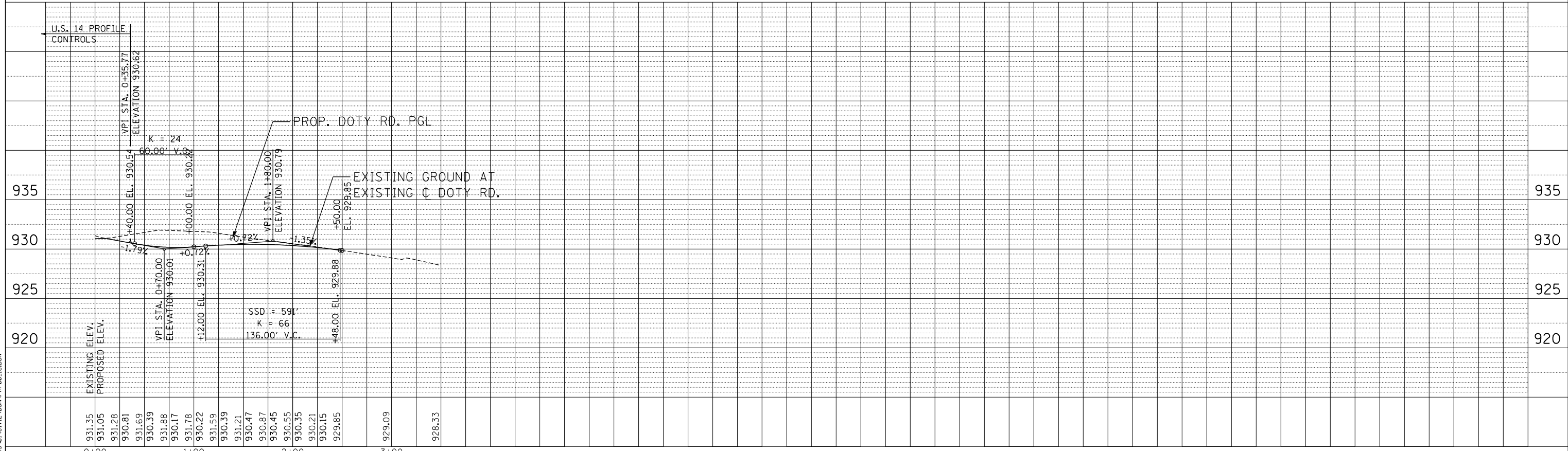
PROPOSED ROADWAY			
U.S. ROUTE 14 PLAN			
LAKE SHORE DRIVE & DOTY ROAD			
SCALE: 1"=50'	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	72
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62268	

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	NOTE BOOK NO.		
	CARD FILE NAME		



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	NOTE BOOK NO.		
	FILE NAME		



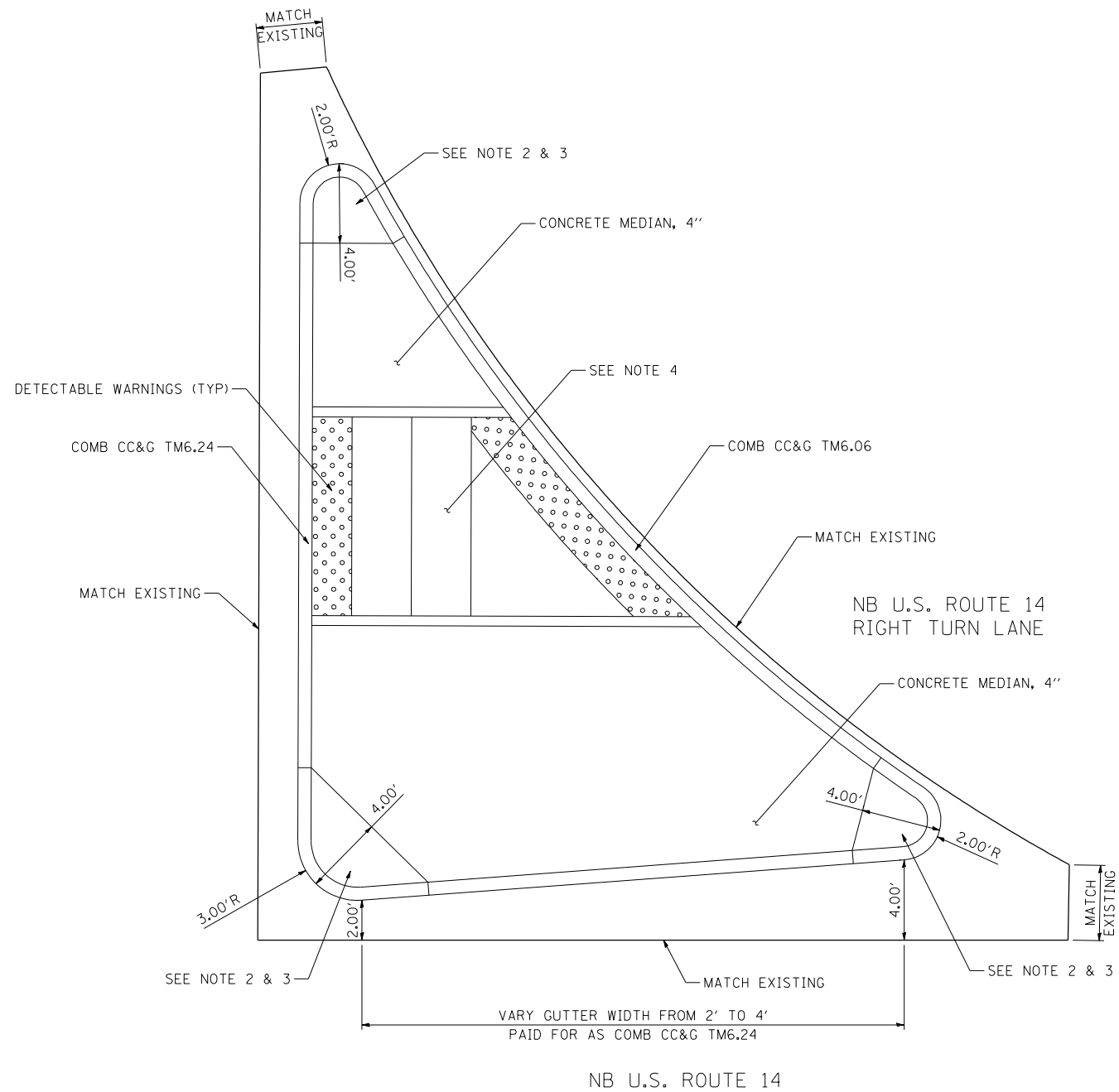
11-07-2013 16:24:45 HECHTBR
 TA:DOCUMENT\134390\CIVIL\UDN\PRF00_IC.DGN

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
FILE.L		DRAWN - RJS	REVISED -
		CHECKED - BRH	REVISED -
		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

PROPOSED PROFILES	
LAKE SHORE DRIVE & DOTY ROAD	
SCALE: HORIZ. 1"=50'	VERT. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	73
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



DETAIL OF INTERMEDIATE ISLAND AT W. LAKE SHORE DRIVE AND U.S. ROUTE 14

N.T.S.

NOTES:

1. SEE IDOT STANDARD 606301 FOR THE DETAILS (INTERMEDIATE ISLAND) NOT SHOWN HERE.
2. NOSE OF CURBED CORNER ISLAND SHALL NOT BE RAMPED.
3. THIS PORTION OF THE ISLAND IS CONSTRUCTED AS INTEGRAL COMB CC&G TM6.24 AND THE COST SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF COMB CC&G TM6.24.
4. SEE SHEET 382 FOR ADDITIONAL RAMP DETAILS.

T:\DOCUMENTS\1934\9901\CIVIL\DCN\DET100.K7.DGN
 HECHTBR
 11-07-2013 16:25:01

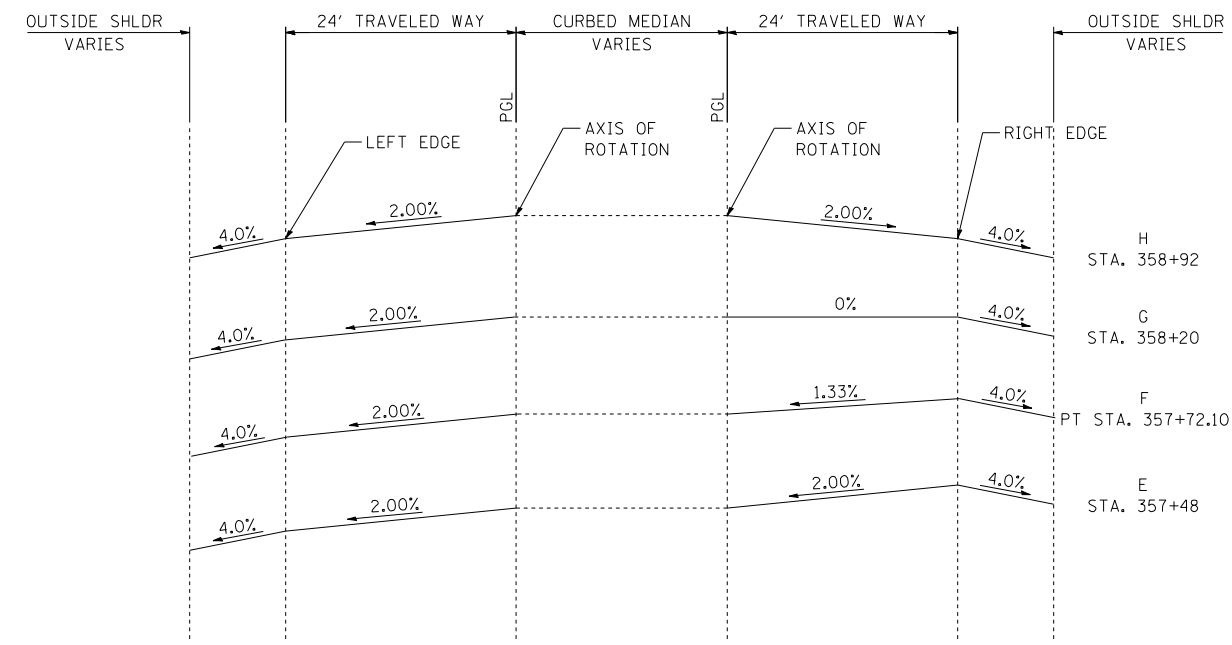
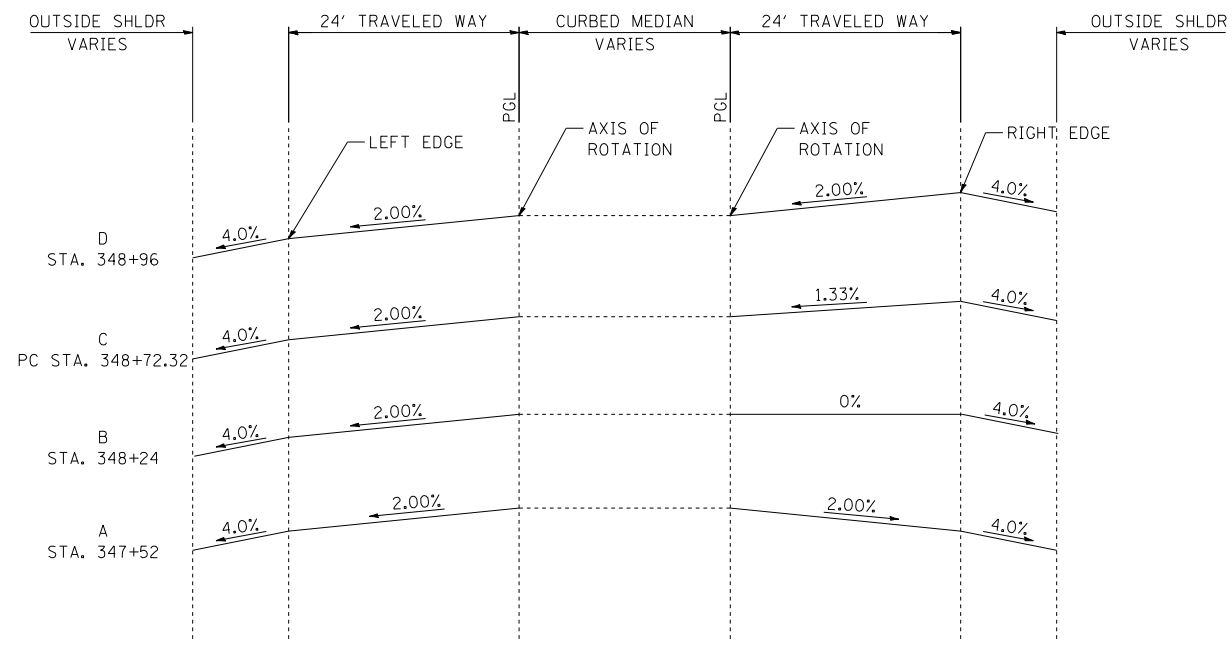
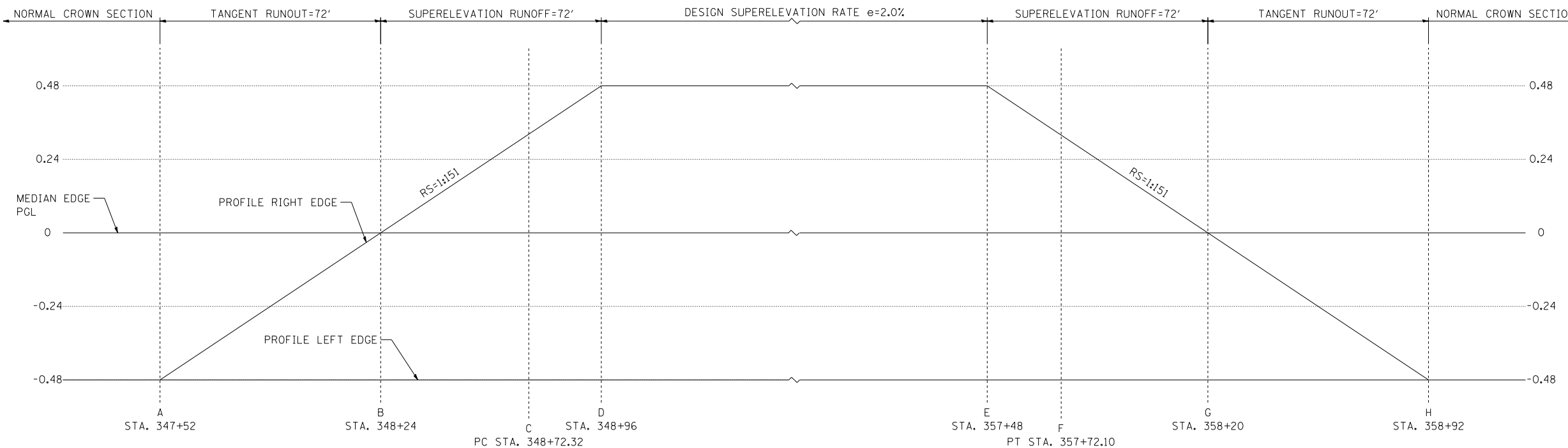
FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
FILEL		DRAWN - AG	REVISED -
exp U.S. Services Inc. Chicago, IL		CHECKED - BRH	REVISED -
BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY		DATE - 11/01/13	REVISED -
PLOT SCALE = *SCALE*			
PLOT DATE = *DATE*			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

ROADWAY DETAIL - I

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	74
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



U.S. ROUTE 14 SUPERELEVATION DETAILS

T:\DOCUMENTS\1934\9901\CIVIL\DCN\DET100_HP.DGN
 HECHTBR
 11-07-2013 16:25:08

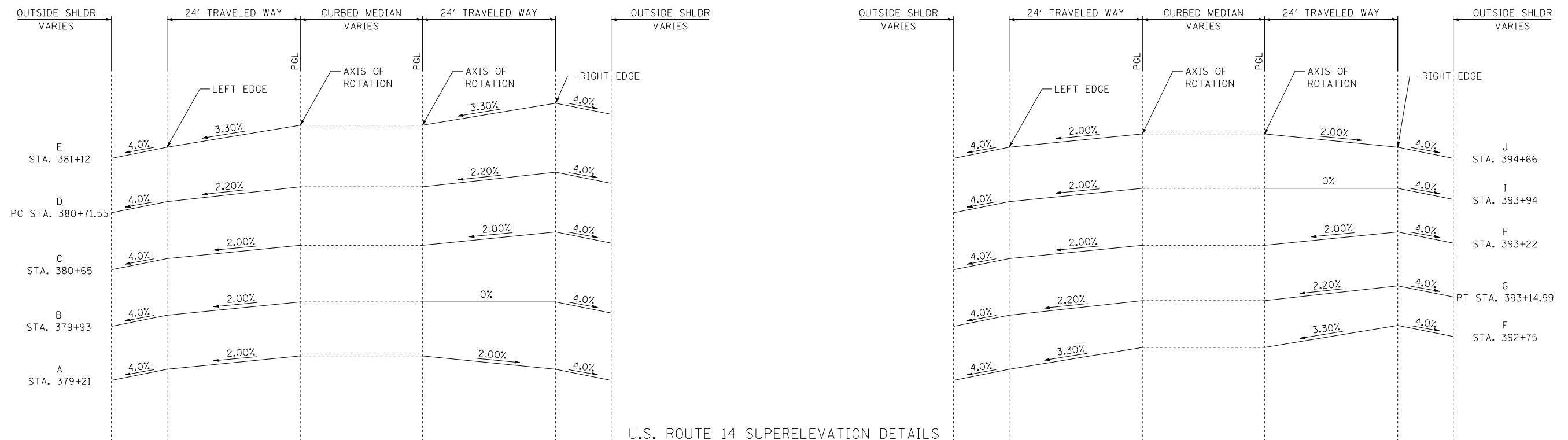
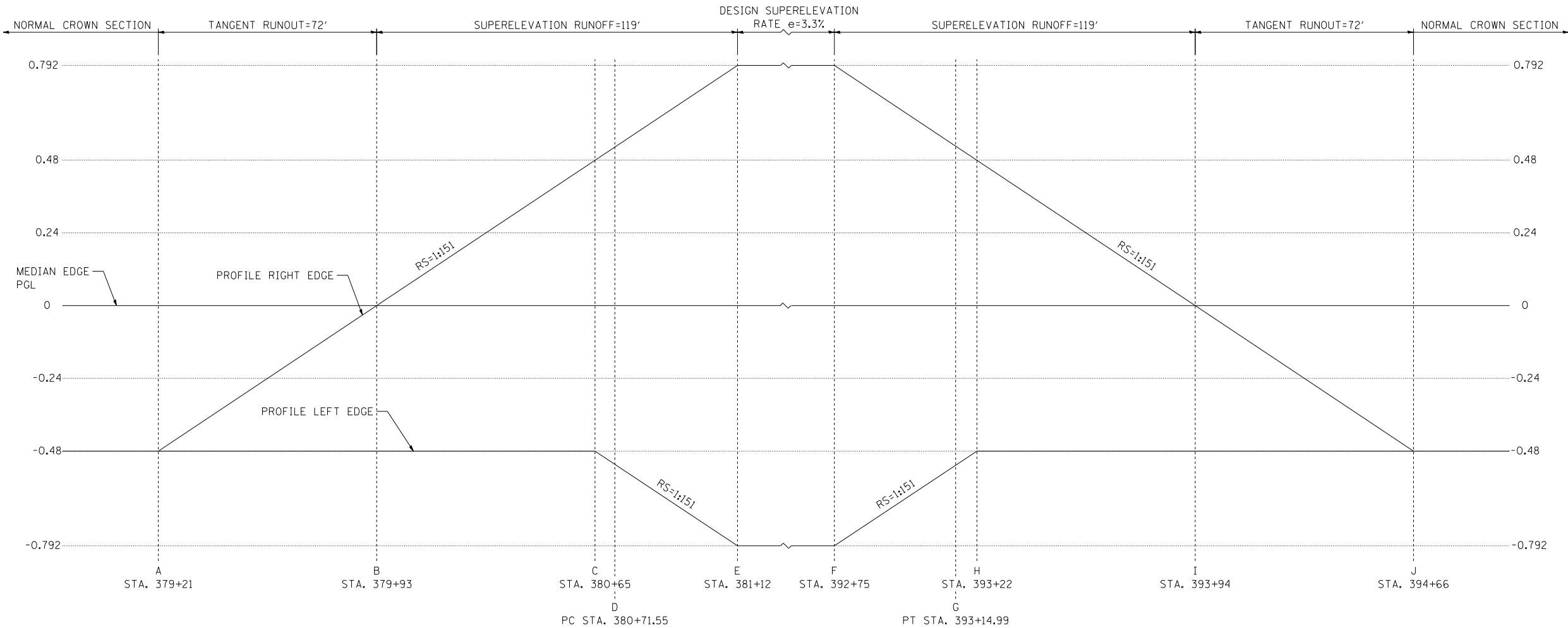
FILE NAME = *FILEL*	USER NAME = HECHTBR	DESIGNED -	REVISED -
		DRAWN - RJS	REVISED -
		CHECKED - BRH	REVISED -
		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

SUPERELEVATION DETAILS - II			
U.S. ROUTE 14			
SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA. TO STA.

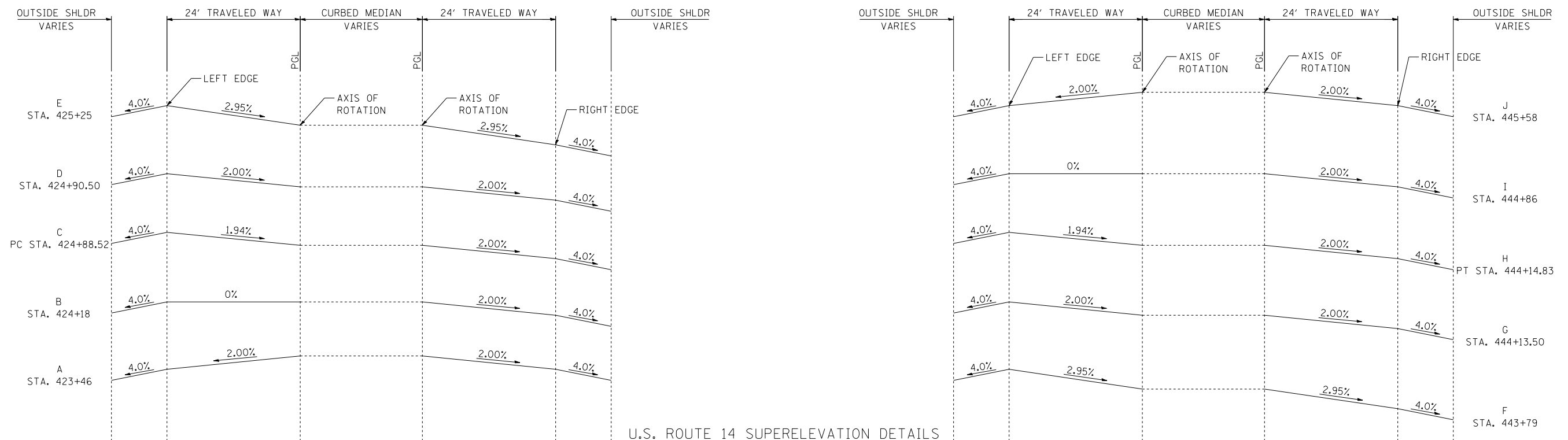
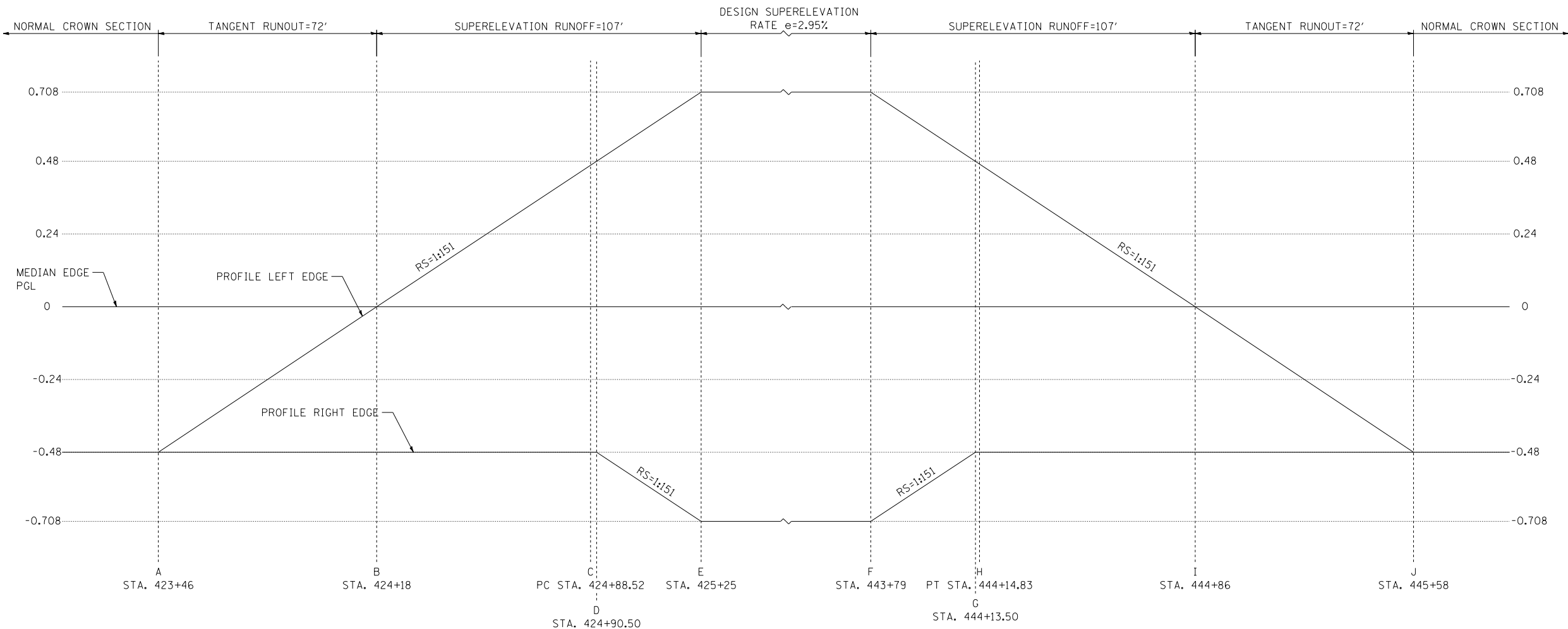
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	77
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





T:\DOCUMENTS\1934\981\CIVIL\DRAWING\00100.H0.DGN
 11-07-2013 16:25:10
 HECHTBR

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14				SUPERELEVATION DETAILS - III U.S. ROUTE 14				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN - RJS	REVISED -									305	27R-2	MCHENRY	673	78
exp U.S. Services Inc. BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT SCALE = *SCALE*	CHECKED - BRH	REVISED -	SCALE: N.T.S.				SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 62268				
	PLOT DATE = *DATE*	DATE - 11/01/13	REVISED -									FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



U.S. ROUTE 14 SUPERELEVATION DETAILS

T:\DOCUMENTS\19349901\CIVIL\DRAWING\DET100_HR.DGN
 11-07-2013 16:25:42
 HECHTBR

FILE NAME = *FILEL*	USER NAME = HECHTBR	DESIGNED -	REVISED -
		DRAWN - RJS	REVISED -
	PLOT SCALE = *SCALE*	CHECKED - BRH	REVISED -
	PLOT DATE = *DATE*	DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

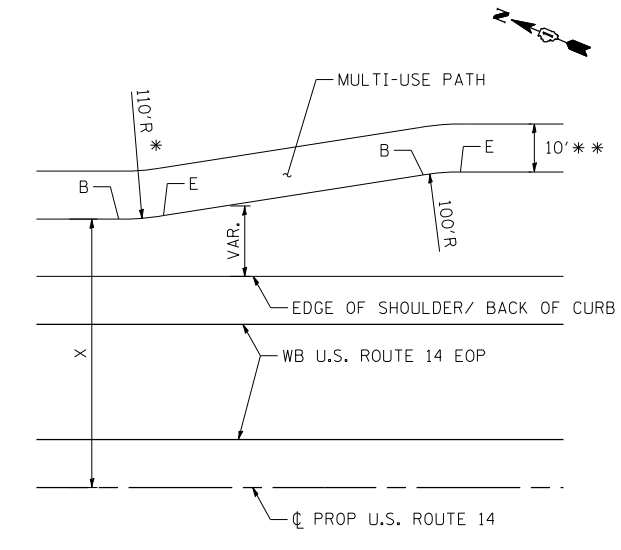
SUPERELEVATION DETAILS - IV			
U.S. ROUTE 14			
SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE. 305	SECTION 27R-2	COUNTY MCHENRY	TOTAL SHEETS 673	SHEET NO. 79
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



MULTI-USE PATH LAYOUT

U.S. ROUTE 14 STATION	OFFSET TO MULTI-USE PATH (X) (FEET)	U.S. ROUTE 14 STATION	OFFSET TO MULTI-USE PATH (X) (FEET)	U.S. ROUTE 14 STATION	OFFSET TO MULTI-USE PATH (X) (FEET)	U.S. ROUTE 14 STATION	OFFSET TO MULTI-USE PATH (X) (FEET)	U.S. ROUTE 14 STATION	OFFSET TO MULTI-USE PATH (X) (FEET)	U.S. ROUTE 14 STATION	OFFSET TO MULTI-USE PATH (X) (FEET)	U.S. ROUTE 14 STATION	OFFSET TO MULTI-USE PATH (X) (FEET)	U.S. ROUTE 14 STATION	OFFSET TO MULTI-USE PATH (X) (FEET)	U.S. ROUTE 14 STATION	OFFSET TO MULTI-USE PATH (X) (FEET)
326+00.00	-69.41	356+50.00	-65.00	386+00.00	-52.58	409+50.00	-65.00	437+00.00	-42.58	457+00.00	-42.58	484+50.00	-42.58				
326+50.00	-69.15	357+00.00	-65.00	386+50.00	-52.58	410+00.00	-65.00	437+50.00	-42.58	457+50.00	-42.58	485+00.00	-42.58				
327+00.00	-68.79	357+50.00	-65.00	387+00.00	-52.58	410+50.00	-65.00	438+00.00	-42.58	458+00.00	-42.58	485+50.00	-42.58				
327+50.00	-68.34	358+00.00	-65.00	387+50.00	-52.58	411+00.00	-65.00	438+50.00	-42.58	458+50.00	-42.58	486+00.00	-42.58				
328+00.00	-67.78	358+50.00	-65.00	388+00.00	-52.58	411+50.00	-65.00	439+00.00	-42.58	459+00.00	-42.58	486+50.00	-42.58				
328+50.00	-66.96	359+00.00	-65.00	388+50.00	-52.58	412+00.00	-65.00	439+50.00	-42.58	459+50.00	-42.58	487+00.00	-42.58				
329+00.00	-66.62	359+50.00	-65.00	389+00.00	-52.58	412+50.00	-65.00	440+00.00	-42.58	460+00.00	-42.58	487+50.00	-42.58				
329+50.00	-66.84	360+00.00	-65.00	389+50.00	-52.58	413+00.00	-65.00	440+50.00	-42.58	460+50.00	-42.58	488+00.00	-42.58				
330+00.00	-67.61	360+50.00	-65.00	390+00.00	-52.58	413+50.00	-65.00	441+00.00	-42.58	461+00.00	-42.58	488+50.00	-42.58				
330+50.00	-68.92	361+00.00	-65.00	390+50.00	-52.58	414+00.00	-65.00	441+50.00	-42.58	461+50.00	-42.58	489+00.00	-42.58				
B 330+97.04	-70.67	361+50.00	-65.00	391+00.00	-52.58	414+50.00	-65.00	442+00.00	-42.58	462+00.00	-42.58	489+50.00	-42.58				
E 331+00.00	-70.75	362+00.00	-65.00	391+50.00	-52.58	415+00.00	-65.00	442+50.00	-42.58	462+50.00	-42.58	490+00.00	-42.58				
E 331+02.24	-70.75	B 362+20.21	-65.00	392+00.00	-52.69	415+50.00	-65.00	443+00.00	-42.58	463+00.00	-42.58	490+50.00	-42.58				
331+50.00	-70.28	E 362+30.16	-65.50	392+50.00	-53.07	416+00.00	-65.00	443+50.00	-42.58	463+50.00	-42.58	491+00.00	-42.58				
332+00.00	-69.80	362+50.00	-65.52	393+00.00	-53.73	416+50.00	-65.00	444+00.00	-42.58	464+00.00	-42.58	491+50.00	-42.58				
332+50.00	-69.35	363+00.00	-57.52	393+50.00	-54.68	417+00.00	-65.00	444+50.00	-42.58	464+50.00	-42.58	492+00.00	-42.58				
333+00.00	-68.91	363+50.00	-52.52	394+00.00	-55.68	417+50.00	-65.00	445+00.00	-42.58	B 464+50.76	-42.58	492+50.00	-42.58				
333+50.00	-68.50	364+00.00	-47.52	394+50.00	-56.68	418+00.00	-65.00	445+50.00	-42.58	E 464+58.29	-42.84	493+00.00	-42.58				
334+00.00	-68.10	364+50.00	-42.52	395+00.00	-57.69	418+50.00	-65.00	446+00.00	-42.58	465+00.00	-45.70						
334+50.00	-67.73	B 364.50.23	-42.49	395+50.00	-58.08	419+00.00	-65.00	446+50.00	-42.58	465+50.00	-49.13						
335+00.00	-67.38	E 364+60.18	-42.00	396+00.00	-58.08	419+50.00	-65.00	446+50.00	-42.58	B 466+00.00	-52.56						
335+50.00	-67.05	365+00.00	-42.00	396+50.00	-58.08	420+00.00	-65.00	447+00.00	-42.58	E 466+26.11	-54.35						
336+00.00	-66.74	366+00.00	-54.58	397+00.00	-58.08	420+50.00	-65.00	447+00.00	-42.58	466+32.95	-54.58						
336+50.00	-66.46	367+00.00	-54.58	397+50.00	-58.08	421+00.00	-65.00	447+50.00	-42.58	466+50.00	-54.58						
337+00.00	-66.19	367+50.00	-54.58	398+00.00	-58.08	421+50.00	-65.00	447+50.00	-42.58	467+00.00	-54.58						
337+50.00	-65.95	368+00.00	-54.58	B 398+19.99	-58.08	422+00.00	-65.00	B 447+76.48	-42.58	467+50.00	-54.50						
338+00.00	-65.72	368+50.00	-54.58	E 398+23.59	-58.02	422+50.00	-65.00	E 447+83.06	-42.78	468+00.00	-54.50						
338+50.00	-65.52	369+00.00	-54.58	398+50.00	-57.06	423+00.00	-65.00	448+00.00	-43.79	468+50.00	-54.50						
339+00.00	-65.34	369+50.00	-54.58	399+00.00	-55.26	423+50.00	-65.00	448+00.00	-43.79	B 468+53.65	-54.50						
339+50.00	-65.18	370+00.00	-54.58	B 399+16.73	-54.65	424+00.00	-65.00	448+50.00	-46.79	E 468+62.01	-54.15						
340+00.00	-65.04	370+50.00	-54.58	E 399+20.70	-54.58	424+50.00	-65.00	448+50.00	-46.79	469+00.00	-50.96						
340+50.00	-64.93	371+00.00	-54.58	399+50.00	-54.58	425+00.00	-65.00	449+00.00	-49.79	469+50.00	-46.77						
341+00.00	-64.83	371+50.00	-54.58	400+00.00	-54.58	425+50.00	-65.00	449+00.00	-49.79	B 469+95.41	-42.96						
341+50.00	-64.76	B 372+00.00	-54.58	B 400+46.96	-54.58	426+00.00	-65.00	449+50.00	-52.79	470+00.00	-42.68						
342+00.00	-64.71	372+50.00	-54.58	400+50.00	-54.63	426+50.00	-65.00	449+50.00	-52.79	E 470+04.60	-42.58						
342+50.00	-64.68	E 373+00.00	-54.58	E 400+54.75	-54.86	427+00.00	-65.00	B 449+76.78	-54.40	470+50.00	-42.58						
343+00.00	-64.67	373+50.00	-54.58	401+00.00	-58.07	427+50.00	-65.00	E 449+82.77	-54.58	471+00.00	-42.58						
343+50.00	-64.68	374+00.00	-54.58	401+50.00	-61.62	428+00.00	-65.00	E 450+00.00	-54.58	471+50.00	-42.58						
344+00.00	-64.72	374+50.00	-54.58	B 401+93.98	-64.75	B 428+38.26	-65.00	450+00.00	-54.58	472+00.00	-42.58						
344+50.00	-64.77	375+00.00	-54.58	E 402+00.00	-64.99	428+50.00	-64.30	450+50.00	-54.58	472+50.00	-42.58						
345+00.00	-64.85	375+50.00	-54.58	E 402+01.06	-65.00	E 428+54.60	-63.64	450+50.00	-54.58	473+00.00	-42.58						
345+50.00	-64.95	376+00.00	-54.58	402+50.00	-65.00	E 429+00.00	-56.50	451+00.00	-54.58	473+50.00	-42.58						
346+00.00	-65.00	376+50.00	-54.58	403+00.00	-65.00	B 429+02.27	-56.16	451+00.00	-54.58	474+00.00	-42.58						
346+50.00	-65.00	377+00.00	-54.58	403+50.00	-65.00	E 429+14.71	-55.13	451+50.00	-54.58	474+50.00	-42.58						
347+00.00	-65.00	377+50.00	-54.58	404+00.00	-65.00	429+50.00	-54.64	451+50.00	-54.58	475+00.00	-42.58						
347+50.00	-65.00	378+00.00	-54.58	404+50.00	-65.00	430+00.00	-54.85	B 451+68.54	-54.58	475+50.00	-42.58						
348+00.00	-65.00	378+50.00	-54.58	405+00.00	-65.00	430+50.00	-55.90	E 451+78.41	-54.09	476+00.00	-42.58						
348+50.00	-65.00	379+00.00	-54.58	405+50.00	-65.00	431+00.00	-54.58	452+00.00	-51.95	476+50.00	-42.58						
349+00.00	-65.00	379+50.00	-54.58	406+00.00	-65.00	431+50.00	-54.58	452+00.00	-51.95	477+00.00	-42.58						
349+50.00	-65.00	380+00.00	-54.58	B 406+00.51	-65.00	432+00.00	-54.58	452+50.00	-46.99	477+50.00	-42.58						
350+00.00	-65.00	380+50.00	-54.58	E 406+09.47	-64.60	432+50.00	-54.58	452+50.00	-46.99	478+00.00	-42.58						
350+50.00	-65.00	381+00.00	-54.37	B 406+40.08	-61.84	433+00.00	-54.58	B 452+89.09	-43.12	478+50.00	-42.58						
351+00.00	-65.00	B 381+46.11	-53.14	E 406+49.94	-61.40	433+50.00	-54.58	E 452+99.95	-42.58	479+00.00	-42.58						
351+50.00	-65.00	381+50.00	-53.00	406+50.00	-61.40	434+00.00	-54.58	453+00.00	-42.58	479+50.00	-42.58						
352+00.00	-65.00	E 381+75.08	-52.58	407+00.00	-61.40	B 434+07.61	-54.58	453+00.00	-42.58	480+00.00	-42.58						
352+50.00	-65.00	382+00.00	-52.58	407+50.00	-61.40	E 434+18.67	-53.96	453+50.00	-42.58	480+50.00	-42.58						
353+00.00	-65.00	382+50.00	-52.58	B 407+50.06	-61.40	434+50.00	-50.63	453+50.00	-42.58	481+00.00	-42.58						
353+50.00	-65.00	E 383+00.00	-52.58	E 407+59.92	-61.84	435+00.00	-45.96	454+00.00	-42.58	481+50.00	-42.58						
354+00.00	-65.00	383+50.00	-52.58	B 407+90.53	-64.60	B 435+39.38	-42.85	454+50.00	-42.58	482+00.00	-42.58						
354+50.00	-65.00	E 384+00.00	-52.58	E 407+99.49	-65.00	E 435+46.75	-42.58	455+00.00	-42.58	482+50.00	-42.58						
355+00.00	-65.00	384+50.00	-52.58	408+00.00	-65.00	435+50.00	-42.58	455+50.00	-42.58	483+00.00	-42.58						
355+50.00	-65.00	385+00.00	-52.58	408+50.00	-65.00	436+00.00	-42.58	456+00.00	-42.58	483+50.00	-42.58						
356+00.00	-65.00	385+50.00	-52.58	409+00.00	-65.00	436+50.00	-42.58	456+50.00	-42.58	484+00.00	-42.58						



MULTI-USE PATH LAYOUT PLAN
N.T.S.

* THE RADIUS BETWEEN STA. 381+46.11 AND STA. 381+75.08 IS 510'.
** MULTI-USE PATH IS 10' WIDE UNLESS OTHERWISE SHOWN ON THE PLANS.

B = BEGIN CURVE
E = END CURVE

FILE NAME = ...
 USER NAME = HECHTBR
 DESIGNED - ...
 REVISED - ...
 DRAWN - BRH
 REVISED - ...
 CHECKED - AG
 REVISED - ...
 DATE - 11/01/13
 REVISED - ...
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14
 SCALE: N.T.S.
 SHEET NO. OF SHEETS STA. TO STA.
 F.A.P. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
 305 27R-2 MCHENRY 673 80
 CONTRACT NO. 62268
 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

MAINTENANCE OF TRAFFIC GENERAL NOTES

1. THE WORK REQUIRED TO COMPLY WITH THE MAINTENANCE OF TRAFFIC GENERAL NOTES SHALL BE INCLUDED IN THE CONTRACT LUMP SUM FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL), EXCEPT FOR PAY ITEMS WHICH HAVE BEEN ESTABLISHED IN THE CONTRACT AND ARE SHOWN ON THE PLANS.
2. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC SUPERVISOR AT 847-705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
3. ALL TRAFFIC CONTROL SIGNS AND DEVICES, WORK ZONE PAVEMENT MARKING, TEMPORARY CONCRETE BARRIERS AND ATTENUATORS NECESSARY FOR THE MAINTENANCE OF TRAFFIC, AS SHOWN ON THE PLANS, SHALL BE IN PLACE BEFORE THE START OF ANY CONSTRUCTION STAGE.
4. THE CONTRACTOR SHALL PLACE A CHANGEABLE MESSAGE SIGN AT THE NORTH END OF THE PROJECT LIMITS ALONG US ROUTE 14 TO INFORM MOTORISTS OF UPCOMING CONSTRUCTION ACTIVITIES. THE LOCATION SHALL BE AS DIRECTED BY THE ENGINEER. THE MESSAGE SIGN SHALL BE IN PLACE TWO WEEKS PRIOR TO START OF CONSTRUCTION ACTIVITIES. THIS WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR MONTH FOR "CHANGEABLE MESSAGE SIGN."
5. SIDE ROAD, INTERSECTION, AND DRIVEWAY TRAFFIC CONTROL SHALL BE AS SHOWN ON THE PLANS, AND IN ACCORDANCE WITH DISTRICT DETAIL TC-10, AND TYPICAL ENTRANCE SIGNING DETAIL (SHT 94).
6. EXISTING PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS THAT CONFLICT WITH REVISED TRAFFIC PATTERNS SHALL BE REMOVED.
7. ACCESS SHALL BE PROVIDED AND MAINTAINED TO ALL DRIVEWAYS, HOUSES, BUILDINGS, FIELD ENTRANCES OR OTHER PROPERTY ABUTTING THE HIGHWAY OR STREET BEING IMPROVED FOR THE ENTIRE DURATION OF THE CONTRACT.
8. TEMPORARY ACCESS TO FIELD ENTRANCES SHALL BE PROVIDED IN ACCORDANCE WITH THE SPECIAL PROVISION FOR "AGGREGATE SURFACE COURSE FOR TEMPORARY ACCESS." THE AGGREGATE SURFACE SHALL BE CONSTRUCTED AS A PRIVATE ENTRANCE AND PAID AS TEMPORARY ACCESS (PRIVATE ENTRANCE).
9. TEMPORARY TRAFFIC SIGNALS SHALL BE IN PLACE AT THE INTERSECTIONS NOTED ON THE PLANS BEFORE THE START OF STAGE 1.1 CONSTRUCTION
10. TEMPORARY TRAFFIC SIGNALS SHALL BE ADJUSTED TO ACCOMMODATE THE VARIOUS STAGES OF CONSTRUCTION SHOWN ON THE PLANS.
11. A MINIMUM OF ONE (1) LANE (11 FEET) IN EACH DIRECTION SHALL BE KEPT OPEN TO TRAFFIC ALONG US 14 DURING NON-WORKING HOURS. A MINIMUM OF ONE (1) LANE (10 FEET) IN EACH DIRECTION SHALL BE KEPT OPEN TO TRAFFIC ALONG ALL SIDE ROADS DURING NON-WORKING HOURS.
12. PAVEMENT MARKING TAPE TYPE III SHALL BE USED ON THE FINAL WEARING SURFACE OR ON THE EXISTING PAVEMENT TO REMAIN WHEN THE TEMPORARY PAVEMENT MARKING WILL CONFLICT WITH THE PERMANENT PAVEMENT MARKING SUCH AS ON TAPERS AND LANE SHIFTS.
13. WET REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE USED AT ALL TRAFFIC STAGING CROSS OVER LOCATIONS.
14. TEMPORARY INFORMATION SIGNS ON TEMPORARY SUPPORTS SHALL BE PROVIDED FOR ALL COMMERCIAL DRIVEWAYS. THESE SIGNS SHALL BE WHITE ON GREEN IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THIS WORK WILL BE PAID FOR PER DISTRICT ONE DETAIL TC-26. THESE SIGNS SHALL BE RELOCATED AS REQUIRED FOR EACH CONSTRUCTION STAGE AND SHALL BE PLACED AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR TEMPORARY INFORMATION SIGNING.
15. MAINTENANCE OF TRAFFIC STAGING SHALL BE COORDINATED WITH THE CONTRACTOR ON THE ADJACENT CONTRACT NO. 62517.
16. ALL CONSTRUCTION WARNING SIGNS SHALL BE BLACK LEGEND ON ORANGE BACKGROUND.
17. TREE REMOVALS ARE NOT SHOWN ON THE MAINTENANCE OF TRAFFIC STAGING PLANS.
18. ALL CONSTRUCTION WARNING SIGNS SHALL BE 48" x 48" UNLESS OTHERWISE SHOWN ON THE PLANS.
19. ALL "ROAD CONSTRUCTION AHEAD" WARNING SIGNS SHALL BE EQUIPPED WITH A TYPE B MONO-DIRECTIONAL FLASHING LIGHT.
20. NOT USED.
21. NOT USED.
22. PRISMATIC BARRIER WALL REFLECTORS SHALL BE INSTALLED ON BOTH THE FACE OF THE TEMPORARY CONCRETE BARRIER WALL NEXT TO TRAFFIC, AND THE TOP OF SECTIONS OF THE TEMPORARY CONCRETE BARRIER WALL AS SHOWN IN STANDARD 704001. THE COLOR OF THESE REFLECTORS SHALL MATCH THE COLOR OF THE EDGELINES (YELLOW ON THE LEFT AND CRYSTAL OR WHITE ON THE RIGHT). IF THE BASE OF THE TEMPORARY CONCRETE BARRIER WALL IS 12 INCHES OR LESS FROM THE TRAVEL LANE, THEN THE LOWER SLOPE OF THE WALL SHALL ALSO HAVE A 6 INCH WIDE TEMPORARY PAVEMENT MARKING EDGELINE (YELLOW ON THE LEFT AND WHITE ON THE RIGHT). TEMPORARY PAVEMENT MARKING ON THE LOWER SLOPE OF THE TEMPORARY CONCRETE BARRIER WALL WILL BE MEASURED AND PAID FOR AS TEMPORARY PAVEMENT MARKING 6".
23. EXISTING PAVEMENT HMA JOINT LOCATIONS, THAT WILL BE WITHIN THE LIMITS OF THE PAVEMENT USED FOR TRAFFIC STAGING AND ARE DETERMINED BY THE ENGINEER TO NEED REPAIR, SHALL BE MILLED 2" AND RESURFACED WITH HMA SURFACE COURSE, MIX "D", NSO (1L 9.5 mm) (2") DURING THE PRE-STAGE.
24. EXISTING RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE REMOVED IMMEDIATELY AFTER SHIFTING TRAFFIC INTO STAGE 1.1.
25. TRAFFIC CONTROL AND PROTECTION SHALL BE AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH THE APPLICABLE HIGHWAY STANDARD OR DISTRICT DETAIL FOR THE PROPOSED WORK. WORKERS SIGN SHALL BE USED ONLY WHEN WORKERS ARE PRESENT. FLAGGER SIGN SHALL BE USED ONLY WHEN FLAGGERS ARE PRESENT.
26. TRAFFIC CONTROL FOR THE STAGE CONSTRUCTION OF INTERSECTIONS, STREET RETURNS, DRIVEWAYS AND OTHER ISOLATED AREAS THAT IS NOT SPECIFICALLY SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS BUT WHICH IS NECESSARY FOR CONSTRUCTION WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL).

T:\DOCUMENTS\934\9301\CIVIL\DCN\MO100.k6.dgn
 HECHTBR
 11-11-2013 8:34:32

FILE NAME = #FILEL\$	USER NAME = HECHTBR	DESIGNED - DRAWN - JP	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	MAINTENANCE OF TRAFFIC GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
exp U.S. Services Inc. Chicago, IL BUILDING-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT SCALE = #SCALE#	CHECKED - KA	REVISED -			305	27R-2	MCHENRY	673	81
PLOT DATE = #DATE#	DATE - 11/01/13	REVISED -				CONTRACT NO. 62268				
						SCALE: 1"=50'	SHEET NO.	OF	SHEETS	STA.

MAINTENANCE OF TRAFFIC STAGES

WESTBOUND TRAFFIC: DENOTES TRAVEL FROM LUCAS ROAD TO WEST LAKE SHORE DRIVE.

EASTBOUND TRAFFIC: DENOTES TRAVEL FROM WEST LAKE SHORE DRIVE TO LUCAS ROAD.

PRE-STAGE

CONSTRUCTION:

- CONSTRUCT PROPOSED CULVERT OR STORM SEWER CROSSING AND PAVEMENT PATCHES AS SHOWN ON THE PLANS PRIOR TO CONSTRUCTING THE TEMPORARY PAVEMENT AT CULVERT OR STORM SEWER CROSSING LOCATIONS.
- CONSTRUCT TEMPORARY PAVEMENT WIDENING AND SHOULDERS AS SHOWN ON THE PLANS.
- CONSTRUCT EMBANKMENT AND TEMPORARY RUNAROUND AND SHOULDERS AS SHOWN ON THE PLANS.
- CONSTRUCT TEMPORARY DITCHES, SWALES, PIPE CULVERTS AND STORM SEWERS AS REQUIRED TO MAINTAIN DRAINAGE.
- CONSTRUCT TEMPORARY TRAFFIC SIGNALS AT US 14 AND WEST LAKE SHORE DRIVE AND US ROUTE 14 AND DOTY ROAD.
- REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT AS SHOWN ON THE PLANS.
- HMA JOINT REPAIRS AS DETERMINED BY THE ENGINEER.

TRAFFIC CONTROL:

- PROVIDE TRAFFIC CONTROL AND PROTECTION IN ACCORDANCE WITH THE APPLICABLE HIGHWAY STANDARDS FOR TEMPORARY LANE CLOSURES DURING THE ALLOWABLE CLOSURE HOURS SPECIFIED IN THE SPECIAL PROVISIONS FOR KEEPING ARTERIAL ROADWAYS OPEN TO TRAFFIC.
- MAINTAIN TRAFFIC ON EXISTING PAVEMENT.

STAGE 1.1

CONSTRUCTION:

- CONSTRUCT WESTBOUND DRAINAGE, PAVEMENT, CURB AND GUTTER AND SHOULDERS BETWEEN APPROXIMATELY STA. 331+06 LT AND APPROXIMATELY STA. 379+70 LT.
- CONSTRUCT EASTBOUND DRAINAGE, PAVEMENT, CURB AND GUTTER AND SHOULDERS BETWEEN APPROXIMATELY STA. 385+00 RT AND APPROXIMATELY STA. 485+00 RT.
- CONSTRUCT TEMPORARY PAVEMENT WIDENING AND SHOULDERS FROM THE END OF EASTBOUND PAVEMENT CONSTRUCTION AT APPROXIMATELY STA. 485+00 TO MEET PREVIOUSLY CONSTRUCTED TEMPORARY PAVEMENT WIDENING AND SHOULDERS AT STA. 488+00 OR AS DETERMINED BY THE CONTRACTOR.

TRAFFIC CONTROL:

- PROVIDE TRAFFIC CONTROL AND PROTECTION AS SHOWN ON THE PLANS.
- MAINTAIN WESTBOUND & EASTBOUND TRAFFIC IN A NEW ALIGNMENT ON EXISTING PAVEMENT BETWEEN STA. 326+00 AND APPROXIMATELY STA. 335+00
- MAINTAIN WESTBOUND & EASTBOUND TRAFFIC ON A PORTION OF SOUTHERLY EXISTING PAVEMENT AND ADJACENT TEMPORARY PAVEMENT WIDENING BETWEEN APPROXIMATELY STA. 335+00 RT AND APPROXIMATELY STA. 377+00 RT.
- SHIFT WESTBOUND AND EASTBOUND TRAFFIC FROM THE SOUTHERLY SIDE TO THE NORTHERLY SIDE OF EXISTING PAVEMENT BETWEEN APPROXIMATELY STA. 377+00 RT AND APPROXIMATELY STA. 384+00 LT.
- MAINTAIN TRAFFIC ON A PORTION OF NORTHERLY EXISTING PAVEMENT AND ADJACENT TEMPORARY PAVEMENT WIDENING BETWEEN APPROXIMATELY STA. 384+00 LT AND STA. 406+61 LT.
- MAINTAIN TRAFFIC ON TEMPORARY RUNAROUND BETWEEN STA. 406+61 LT AND APPROXIMATELY STA. 431+00 LT.
- MAINTAIN TRAFFIC ON EXISTING PAVEMENT BETWEEN APPROXIMATELY STA. 431+00 LT AND APPROXIMATELY STA. 440+00 LT.
- MAINTAIN TRAFFIC ON A PORTION OF NORTHERLY EXISTING PAVEMENT AND ADJACENT TEMPORARY PAVEMENT WIDENING BETWEEN APPROXIMATELY STA. 440+00 LT AND APPROXIMATELY STA. 487+50 LT.
- SHIFT TRAFFIC FROM THE NORTHERLY SIDE TO THE SOUTHERLY SIDE OF EXISTING PAVEMENT BETWEEN APPROXIMATELY STA. 487+50 LT AND STA. 493+00 RT TO COORDINATE WITH ADJACENT CONTRACT 62517 STAGE 1 MAINTENANCE OF TRAFFIC.

STAGE 1.1 SUBSTAGE A-1 (INDEPENDENT OF SUBSTAGE B AND C)

CONSTRUCTION:

- CONSTRUCT WESTBOUND PAVEMENT BETWEEN STA. 326+00 LT AND STA. 331+06 LT.

TRAFFIC CONTROL:

- PROVIDE TRAFFIC CONTROL AND PROTECTION AS SHOWN ON THE PLANS.
- MAINTAIN WESTBOUND TRAFFIC IN A NEW ALIGNMENT ON EXISTING PAVEMENT BETWEEN STA. 326+00 AND APPROXIMATELY STA. 330+00 AND IN THE EXISTING STAGE 1.1 CONFIGURATION BETWEEN APPROXIMATELY STA. 330+00 AND STA 334+00.
- MAINTAIN EASTBOUND TRAFFIC IN THE EXISTING STAGE 1.1 CONFIGURATION BETWEEN STA. 326+00 AND STA. 334+00.

STAGE 1.1 SUBSTAGE A-2 (INDEPENDENT OF SUBSTAGE B AND C)

CONSTRUCTION:

- CONSTRUCT PORTIONS OF WESTBOUND PAVEMENT BETWEEN STA. 326+00 AND APPROXIMATELY STA. 330+00 AS SHOWN ON THE PLANS.

TRAFFIC CONTROL:

- PROVIDE TRAFFIC CONTROL AND PROTECTION AS SHOWN ON THE PLANS.
- MAINTAIN WESTBOUND TRAFFIC IN A NEW ALIGNMENT ON NEW WESTBOUND PAVEMENT BETWEEN STA. 326+00 LT AND APPROXIMATELY STA. 351+50 LT.
- SHIFT WESTBOUND TRAFFIC FROM THE SOUTHERLY SIDE OF EXISTING PAVEMENT AND TEMPORARY PAVEMENT WIDENING TO THE NORTHERLY SIDE AND NEW PAVEMENT BETWEEN APPROXIMATELY STA. 351+50 AND STA. 356+50.
- SHIFT EASTBOUND TRAFFIC TOWARDS THE SOUTH AND MAINTAIN EASTBOUND TRAFFIC ON EXISTING PAVEMENT AND TEMPORARY PAVEMENT WIDENING BETWEEN STA. 163+65 AND APPROXIMATELY STA. 341+00 AND MAINTAIN EASTBOUND TRAFFIC IN THE STAGE 1.1 CONFIGURATION BETWEEN APPROXIMATELY STA. 341+00 AND STA. 356+50.
- MAINTAIN WESTBOUND AND EASTBOUND TRAFFIC IN THE STAGE 1.1 CONFIGURATION BETWEEN STA. 356+50 AND STA. 493+00.

STAGE 1.1 SUBSTAGE A-3 (INDEPENDENT OF SUBSTAGE B AND C)

CONSTRUCTION:

- CONSTRUCT PORTIONS OF EASTBOUND PAVEMENT AND WESTBOUND PAVEMENT TURN LANE BETWEEN STA. 326+00 AND STA. 332+00.
- CONSTRUCT TEMPORARY PAVEMENT BETWEEN STA. 326+00 AND STA. 335+00 AS SHOWN ON THE PLANS.

TRAFFIC CONTROL:

- PROVIDE TRAFFIC CONTROL AND PROTECTION AS SHOWN ON THE PLANS.
- MAINTAIN TRAFFIC IN THE SUB-STAGE A-2 CONFIGURATION AND SHIFT LOCATION OF WESTBOUND LEFT TURN LANE AT WEST LAKE SHORE DR AS SHOWN ON PLANS.

STAGE 1.1 SUBSTAGE B-1 - B-3 (INDEPENDENT OF SUBSTAGE A AND C)

CONSTRUCTION:

- CONSTRUCT NORTH LEG OF LAKE SHORE DRIVE INTERSECTION.

TRAFFIC CONTROL:

- PROVIDE TRAFFIC CONTROL AND PROTECTION AS SHOWN ON THE PLANS.
- MAINTAIN WESTBOUND AND EASTBOUND US ROUTE 14 TRAFFIC IN STAGE 1.1 CONFIGURATION AS SHOWN ON THE PLANS
- SHIFT LAKE SHORE DRIVE TRAFFIC PER SUBSTAGES AS SHOWN ON THE PLANS.

STAGE 1.1 SUBSTAGE C-1 - C-3 (INDEPENDENT OF SUBSTAGE A AND B)

CONSTRUCTION:

- CONSTRUCT DOTY ROAD.

TRAFFIC CONTROL:

- PROVIDE TRAFFIC CONTROL AND PROTECTION AS SHOWN ON THE PLANS.
- MAINTAIN WESTBOUND AND EASTBOUND US ROUTE 14 TRAFFIC IN STAGE 1.1 CONFIGURATION AS SHOWN ON THE PLANS.
- SHIFT DOTY ROAD TRAFFIC PER SUBSTAGES AS SHOWN ON THE PLANS.

STAGE 1.2

CONSTRUCTION:

- CONSTRUCT EASTBOUND PAVEMENT BETWEEN APPROXIMATELY STA. 364+40 AND APPROXIMATELY STA. 385+00 AND BETWEEN STA. 474+00 AND STA. 485+00.
- CONSTRUCT WESTBOUND PAVEMENT BETWEEN STA. 479+00 AND STA. 493+00.

TRAFFIC CONTROL:

- PROVIDE TRAFFIC CONTROL AND PROTECTION AS SHOWN ON THE PLANS.
- MAINTAIN WESTBOUND US ROUTE 14 TRAFFIC ON NEW WESTBOUND PAVEMENT AND SHIFT EASTBOUND US ROUTE 14 TRAFFIC TO A NEW CONFIGURATION ON NEW WESTBOUND PAVEMENT BETWEEN STA. 326+00 LT AND APPROXIMATELY STA. 351+50.
- SHIFT WESTBOUND AND EASTBOUND US ROUTE 14 TRAFFIC TO A NEW CONFIGURATION ON NEW WESTBOUND PAVEMENT BETWEEN APPROXIMATELY STA. 351+50 AND STA. 377+50.
- SHIFT WESTBOUND AND EASTBOUND US ROUTE 14 TRAFFIC FROM THE NORTHERLY SIDE OF EXISTING PAVEMENT AND TEMPORARY PAVEMENT WIDENING TO THE SOUTHERLY SIDE NEW PAVEMENT AND TEMPORARY PAVEMENT BETWEEN STA. 475+00 AND STA. 493+00.
- MAINTAIN WESTBOUND AND EASTBOUND US ROUTE 14 TRAFFIC ON A PORTION OF EXISTING PAVEMENT AND TEMPORARY PAVEMENT WIDENING BETWEEN APPROXIMATELY STA. 377+50 AND APPROXIMATELY STA. 385+00.
- MAINTAIN WESTBOUND AND EASTBOUND US ROUTE 14 TRAFFIC IN THE STAGE 1.1 CONFIGURATION BETWEEN APPROXIMATELY STA. 385+00 AND STA. 475+00.

STAGE 1.2 SUBSTAGE A-1 - A-3

CONSTRUCTION:

- CONSTRUCT SOUTH LEG OF LAKE SHORE DRIVE INTERSECTION.

TRAFFIC CONTROL:

- PROVIDE TRAFFIC CONTROL AND PROTECTION AS SHOWN ON THE PLANS.
- MAINTAIN WESTBOUND AND EASTBOUND US ROUTE 14 TRAFFIC IN STAGE 1.2 CONFIGURATION AS SHOWN ON THE PLANS.
- SHIFT LAKE SHORE DRIVE TRAFFIC PER SUBSTAGE AS SHOWN ON THE PLANS.

STAGE 2.1

CONSTRUCTION:

- CONSTRUCT EASTBOUND PAVEMENT BETWEEN STA. 326+00 AND APPROXIMATELY STA. 364+40 AND STA. 485+00 AND STA. 493+00
- CONSTRUCT WESTBOUND PAVEMENT BETWEEN APPROXIMATELY STA. 377+50 AND STA. 479+00 WITH GAPS IN PAVEMENT CONSTRUCTION AT LILY POND RD. AND RIDGEFIELD RD.

TRAFFIC CONTROL:

- PROVIDE TRAFFIC CONTROL AND PROTECTION AS SHOWN ON THE PLANS.
- MAINTAIN WESTBOUND AND EASTBOUND TRAFFIC ON THE NEW WESTBOUND PAVEMENT IN THE STAGE 1.2 CONFIGURATION BETWEEN STA. 163+65 AND APPROXIMATELY STA. 368+00.
- SHIFT WESTBOUND AND EASTBOUND TRAFFIC FROM THE NEW WESTBOUND PAVEMENT TO THE NEW EASTBOUND PAVEMENT BETWEEN APPROXIMATELY STA. 368+00 AND APPROXIMATELY STA. 373+00.
- MAINTAIN WESTBOUND AND EASTBOUND TRAFFIC ON THE NEW EASTBOUND PAVEMENT BETWEEN APPROXIMATELY STA. 373+00 AND APPROXIMATELY STA. 479+00.
- SHIFT WESTBOUND AND EASTBOUND US ROUTE 14 TRAFFIC FROM THE NEW EASTBOUND PAVEMENT TO THE NEW WESTBOUND PAVEMENT BETWEEN APPROXIMATELY STA. 479+00 AND APPROXIMATELY STA. 483+00 AND MAINTAIN WESTBOUND AND EASTBOUND US ROUTE 14 TRAFFIC ON THE NEW WESTBOUND PAVEMENT BETWEEN STA. 483+00 AND STA. 493+00
- MAINTAIN TRAFFIC ON LILY POND RD. AND RIDGEFIELD RD.

STAGE 2.1 SUBSTAGE A-1 - A-3 (INDEPENDENT OF SUBSTAGE B)

CONSTRUCTION:

- CONSTRUCT LILY POND ROAD.

TRAFFIC CONTROL:

- PROVIDE TRAFFIC CONTROL AND PROTECTION AS SHOWN ON THE PLANS
- MAINTAIN WESTBOUND AND EASTBOUND US ROUTE 14 TRAFFIC IN STAGE 2.1 CONFIGURATION AS SHOWN ON THE PLANS.
- SHIFT LILY POND RD TRAFFIC PER SUBSTAGE AS SHOWN ON THE PLANS.

STAGE 2.1 SUBSTAGE B-1 - B-3 (INDEPENDENT OF SUBSTAGE A)

CONSTRUCTION:

- CONSTRUCT RIDGEFIELD ROAD NORTH.

TRAFFIC CONTROL:

- MAINTAIN WESTBOUND AND EASTBOUND US ROUTE 14 TRAFFIC IN STAGE 2.1 CONFIGURATION AS SHOWN ON THE PLANS
- SHIFT RIDGEFIELD ROAD NORTH TRAFFIC PER SUBSTAGE AS SHOWN ON THE PLANS.

STAGE 3.1

CONSTRUCTION:

- CONSTRUCT MEDIAN CURB AND GUTTER AT CROSSOVER LOCATIONS.
- CONSTRUCT LEFT TURN LANE / U-TURN LANE PAVEMENT AND ADJACENT CURB AND GUTTER AND SOLID MEDIAN AT LILY POND RD., RIDGEFIELD RD. AND U-TURN LOCATIONS.
- CONSTRUCT REMAINING MEDIAN FILL AND TOPSOIL BETWEEN STA. 326+00 AND TO STA. 493+00.

TRAFFIC CONTROL:

- PROVIDE TRAFFIC CONTROL AND PROTECTION IN ACCORDANCE WITH THE APPLICABLE HIGHWAY STANDARDS FOR LANE CLOSURES AND APPROACH TO LANE CLOSURES.
- SHIFT WESTBOUND TRAFFIC TO THE NEW WESTBOUND PAVEMENT OUTSIDE LANE (LANE 2) AND SHIFT EASTBOUND TRAFFIC TO THE NEW EASTBOUND PAVEMENT OUTSIDE LANE (LANE 2) AT ALL LOCATIONS BETWEEN STA. 326+00 AND STA. 493+00.
- PLACE TYPE II BARRICADES OR DRUMS (WITH TYPE C STEADY BURN MONO-DIRECTIONAL LIGHTS) AT 50 FT CENTERS IN THE CLOSED INSIDE LANE (LANE 1) BETWEEN STA. 326+00 AND STA. 493+00.
- ESTABLISH TEMPORARY LEFT TURN BAYS ON NEW EASTBOUND PAVEMENT AT LAKE SHORE DRIVE, DOTY ROAD, LILY POND RD AND RIDGEFIELD NORTH RD
- MAINTAIN TRAFFIC ON THE OUTSIDE LANE (LANE 2) BETWEEN STA. 326+00 AND STA. 493+00.

STAGE 4.1

CONSTRUCTION:

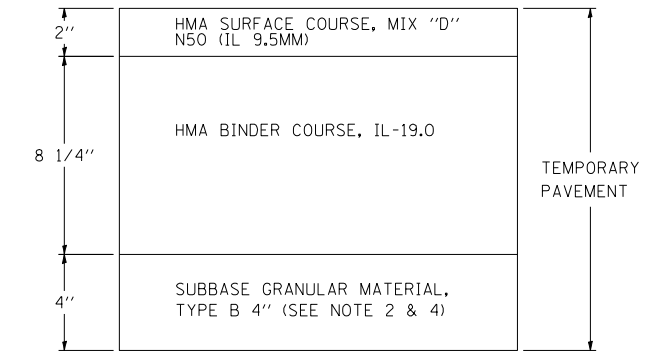
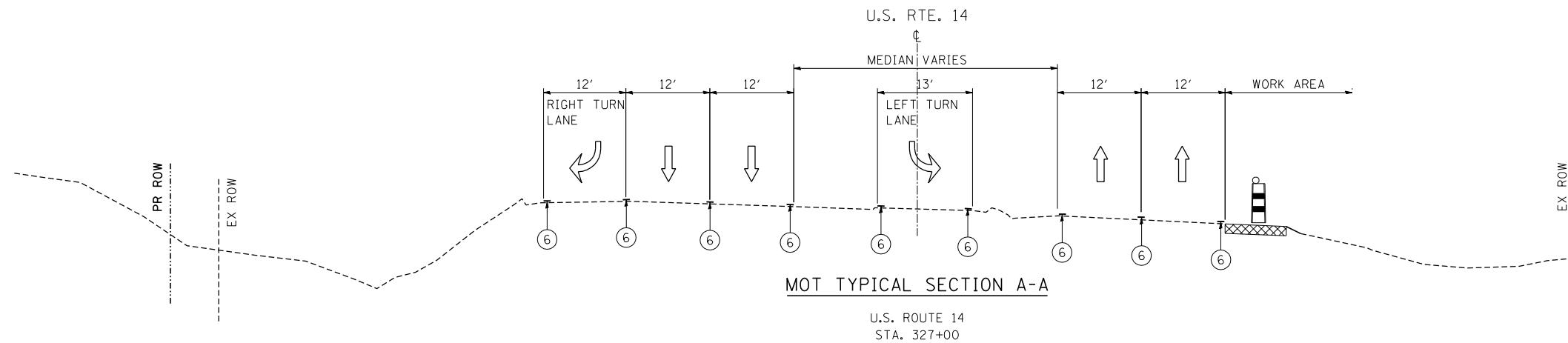
- CONSTRUCT OUTSIDE CURB AND GUTTER ALONG NORTHEAST CORNER OF LAKE SHORE DRIVE .
- MILL AND RESURFACE WEST LAKE SHORE DRIVE INTERSECTION.
- PLACE PERMANENT PAVEMENT STRIPING BETWEEN STA. 165+40 (EX) AND TO STA. 493+00.

TRAFFIC CONTROL:

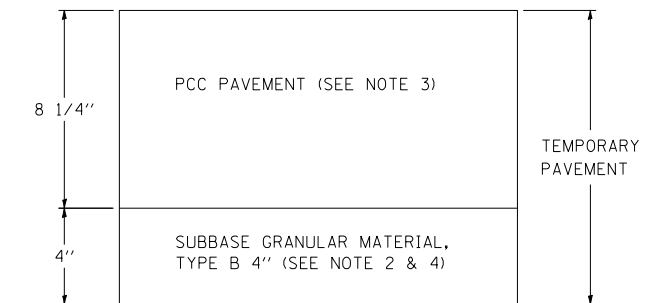
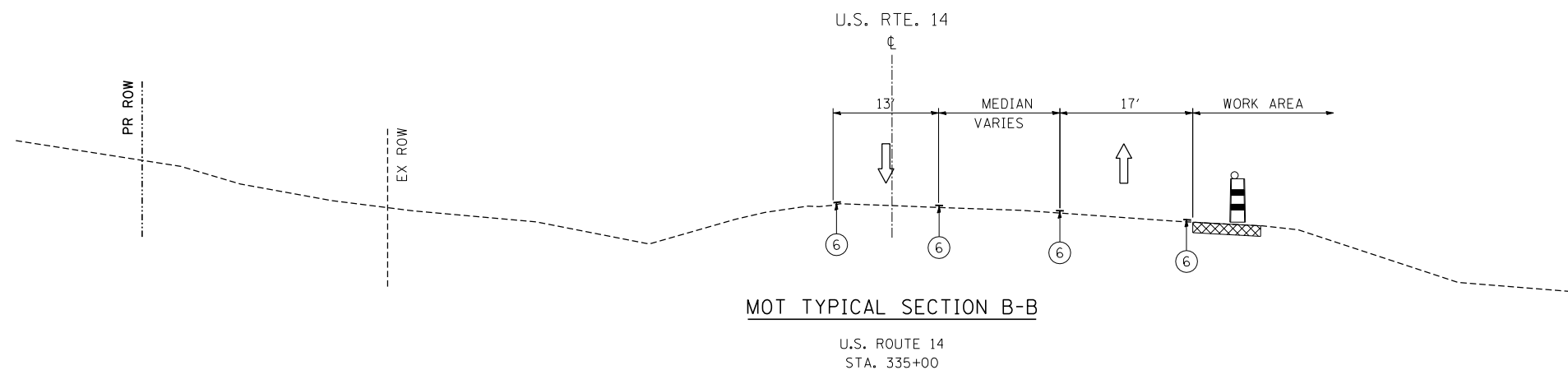
- PROVIDE TRAFFIC CONTROL AND PROTECTION IN ACCORDANCE WITH THE APPLICABLE HIGHWAY STANDARDS FOR LANE CLOSURES AND APPROACH TO LANE CLOSURES.

ATTB008JJ.DGN 11-07-2013 16:25:24 HECHTBR T:\DOCUMENT\934\9341\CIVIL\DCN\MO100_02.DGN

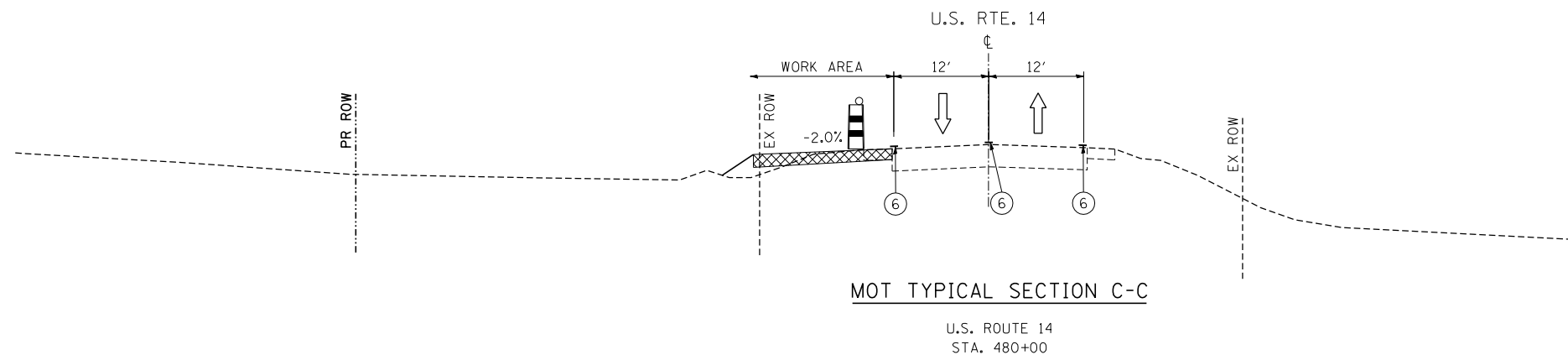
FILE NAME = #FILEL#	USER NAME = HECHTBR	DESIGNED - DRAWN - BRH	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	MAINTENANCE OF TRAFFIC SUGGESTED STAGES OF TRAFFIC	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
 exp U.S. Services Inc. Chicago, IL BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT SCALE = #SCALE#	CHECKED - KA	REVISED -			305	27R-2	MCHENRY	673	82	
PLOT DATE = #DATE#	DATE - 11/01/13	REVISED -				CONTRACT NO. 62268					
						SCALE: N.T.S.	SHEET NO.	OF	SHEETS	STA.	TO STA.



FLEXIBLE DESIGN



RIGID DESIGN



TEMPORARY PAVEMENT SECTION

NOTES

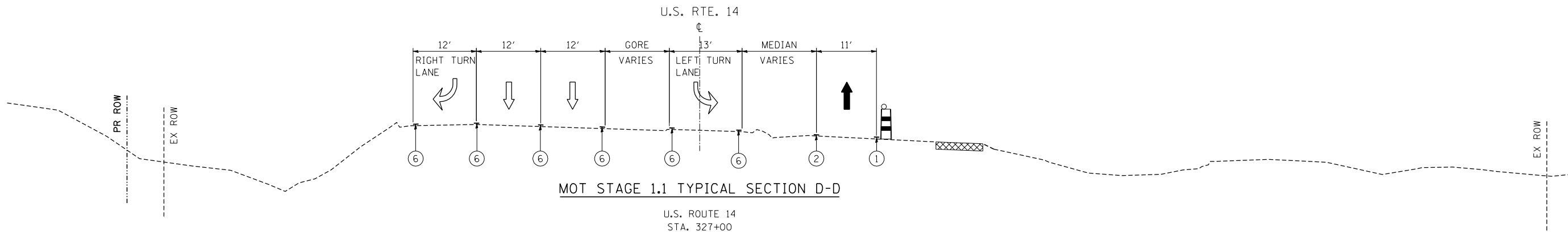
- TEMPORARY PAVEMENT SHALL BE DEFINED AS PORTLAND CEMENT CONCRETE OR HMA PAVEMENT AND SHALL INCLUDED SUBBASE GRANULAR MATERIAL, TYPE B 4".
- THE THICKNESS OF THE SUBBASE GRANULAR MATERIAL TYPE B SHALL BE INCREASED TO 12" IN AREAS OF SUBGRADE SOILS WITH AN IBR <2.5. THE INCREASED THICKNESS OF SUBBASE GRANULAR MATERIAL WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR SUBBASE GRANULAR MATERIAL, TYPE B 8".
- IF CONTRACTOR CHOOSES TO USE PC CONCRETE TEMPORARY PAVEMENT, THE PAVEMENT SHALL CONSIST OF CLASS PV CONCRETE MEETING THE REQUIREMENTS OF ART. 1020 OF THE STANDARD SPECIFICATIONS.
- THE SUBBASE GRANULAR MATERIAL, TYPE B 4" WILL NOT BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQ YD FOR PAVEMENT REMOVAL. THE REMOVAL OF SUBBASE GRANULAR MATERIAL, TYPE B REQUIRED BY THE PROPOSED CROSS SECTION, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CU YD FOR EARTH EXCAVATION.

MAINTENANCE OF TRAFFIC LEGEND

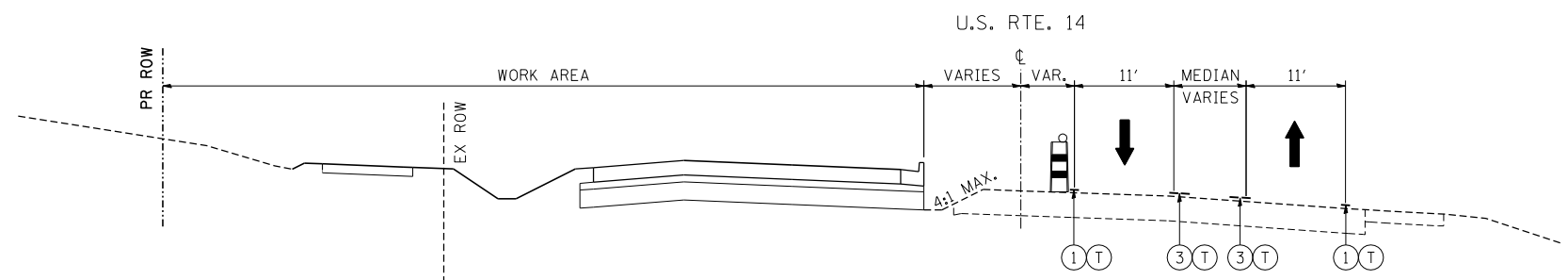
- TEMPORARY PAVEMENT (SEE SHT 83 FOR SECTION)
- TEMPORARY CONCRETE BARRIER
- TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
- DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
- EXISTING TRAFFIC DIRECTION
- PROPOSED TRAFFIC DIRECTION
- ① PVT MK - LINE 4" WHITE
- ② PVT MK - LINE 4" YELLOW
- ③ PVT MK - DBL 4" YELLOW
- ④ PVT MK - LINE 6" WHITE
- ⑤ PVT MK - LINE 8" WHITE
- ⑥ EXISTING PAVEMENT MARKING
- Ⓣ TEMP PVT MK
- Ⓜ WET REF TEM TAPE T3

FILE NAME = ...
 USER NAME = HECHTBR
 DESIGNED -
 DRAWN - JP
 CHECKED - BRH
 DATE - 11/01/13
 REVISED -
 REVISED -
 REVISED -
 REVISED -
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14
 MOT TYPICAL SECTIONS
 U.S. ROUTE 14
 PRESTAGE
 SCALE: N.T.S.
 SHEET NO. OF SHEETS STA. TO STA.
 F.A.P. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
 305 27R-2 MCHENRY 673 83
 CONTRACT NO. 62268
 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	MOT TYPICAL SECTIONS U.S. ROUTE 14 PRESTAGE	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
*FILEL#		DRAWN - JP	REVISED -			305	27R-2	MCHENRY	673	83	
exp U.S. Services Inc. Chicago, IL	PLOT SCALE = *SCALE*	CHECKED - BRH	REVISED -			CONTRACT NO. 62268					
BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT DATE = *DATE*	DATE - 11/01/13	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

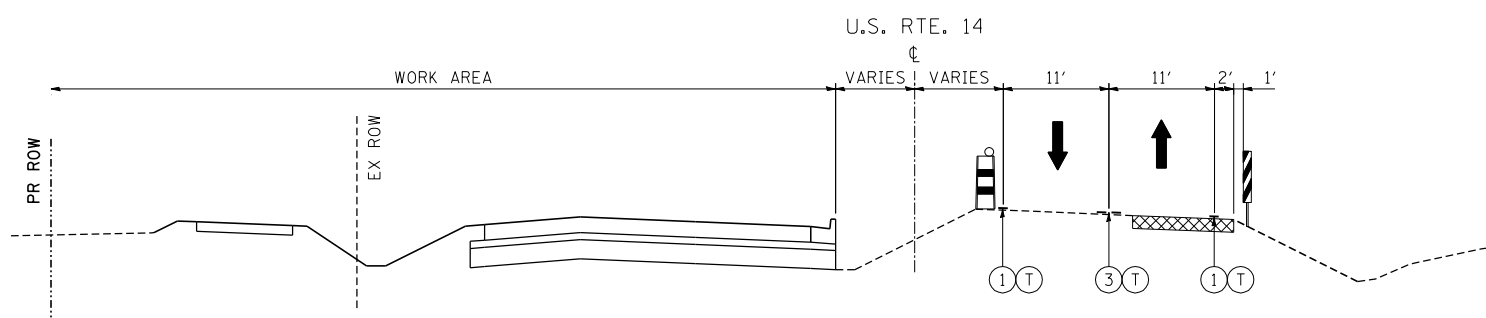


U.S. ROUTE 14
STA. 327+00



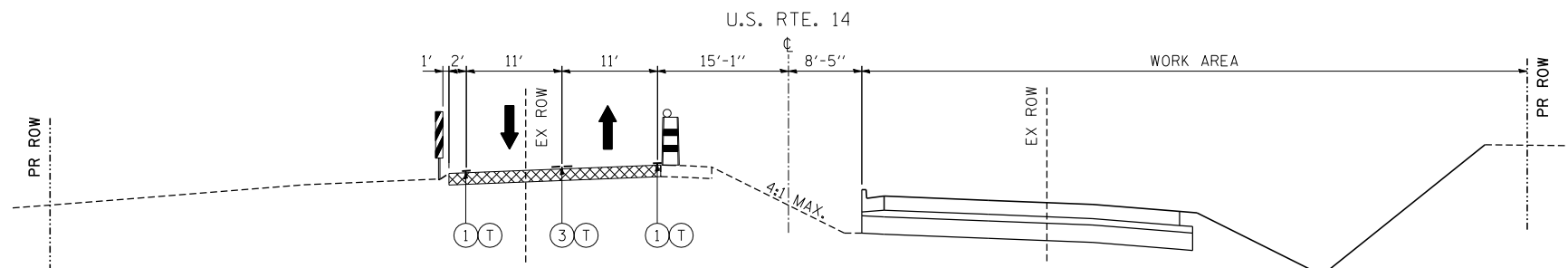
MOT STAGE 1.1 TYPICAL SECTION E-E

U.S. ROUTE 14
STA. 335+00



MOT STAGE 1.1 TYPICAL SECTION F-F

U.S. ROUTE 14
STA. 341+00



MOT STAGE 1.1 TYPICAL SECTION G-G

U.S. ROUTE 14
STA. 405+00

MAINTENANCE OF TRAFFIC LEGEND

- TEMPORARY PAVEMENT (SEE SHT 83 FOR SECTION)
- TEMPORARY CONCRETE BARRIER
- TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
- DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
- EXISTING TRAFFIC DIRECTION
- PROPOSED TRAFFIC DIRECTION
- ① PVT MK - LINE 4" WHITE
- ② PVT MK - LINE 4" YELLOW
- ③ PVT MK - DBL 4" YELLOW
- ④ PVT MK - LINE 6" WHITE
- ⑤ PVT MK - LINE 8" WHITE
- ⑥ EXISTING PAVEMENT MARKING
- (T) TEMP PVT MK
- (W) WET REF TEM TAPE T3

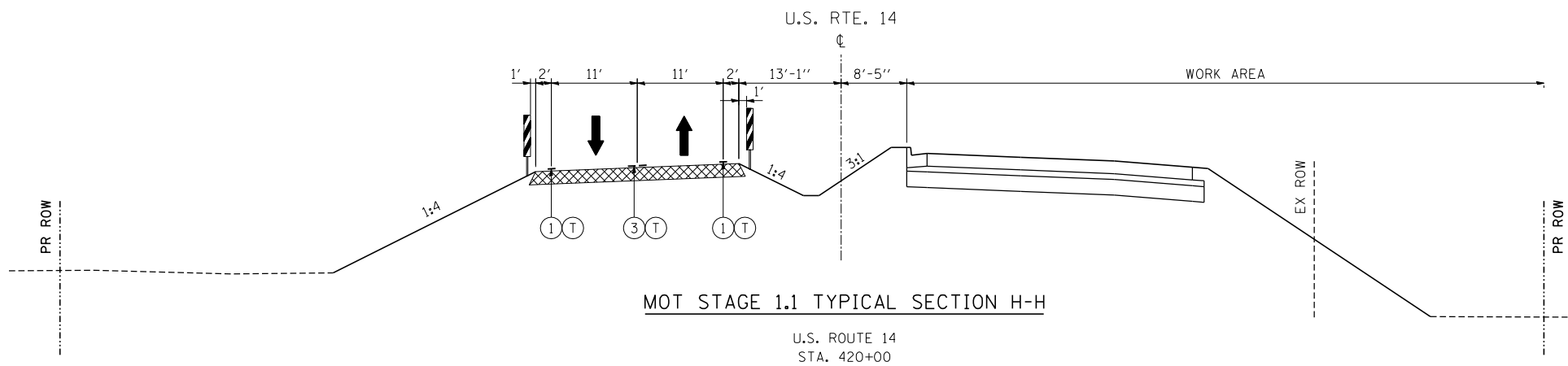
\NTSC00\31\DON\NTTB006\JJDON\GEN00477.DGN
 11-07-2013 16:25:29
 HECHTBR T:\DOCUMENT\1934\9901\CIVIL\DON\TSC00_38.DGN

FILE NAME = *FILEL*	USER NAME = HECHTBR	DESIGNED -	REVISED -
		DRAWN - JP	REVISED -
		CHECKED - BRH	REVISED -
		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

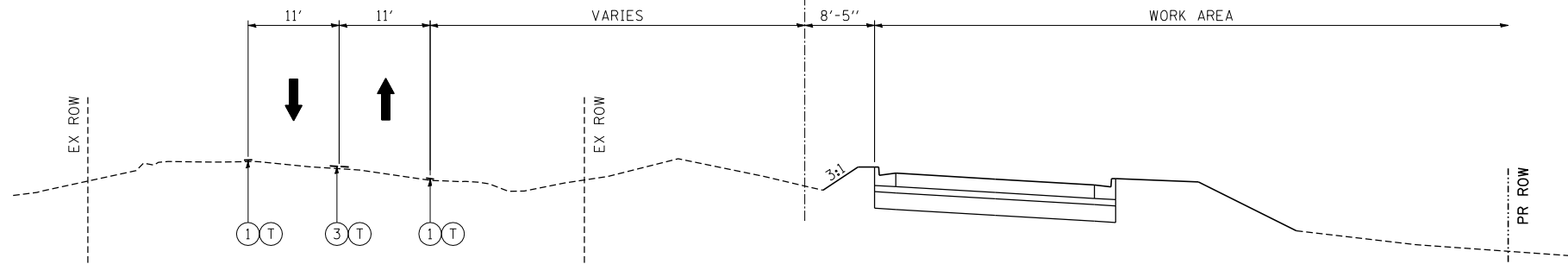
MOT TYPICAL SECTIONS			
U.S. ROUTE 14			
STAGE 1.1 - SHEET I			
SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	84
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



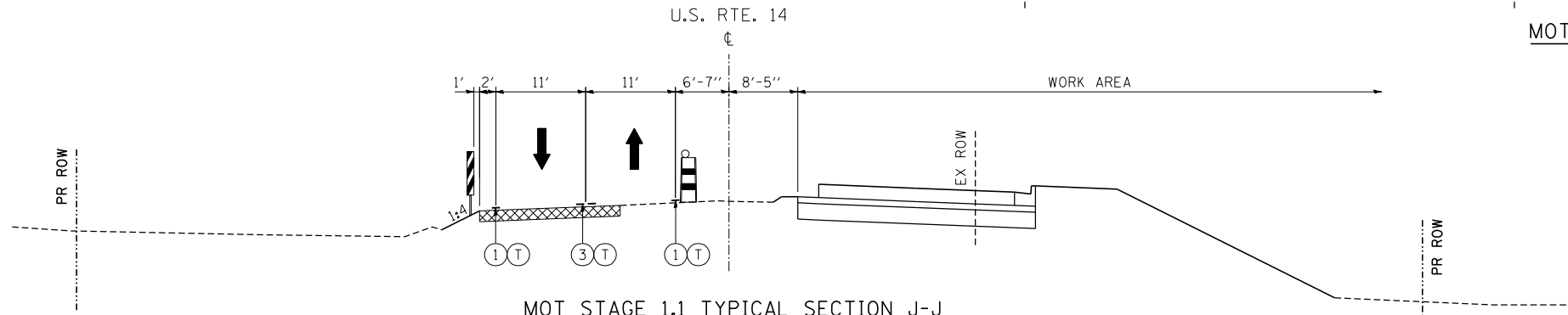
MOT STAGE 1.1 TYPICAL SECTION H-H

U.S. ROUTE 14
STA. 420+00



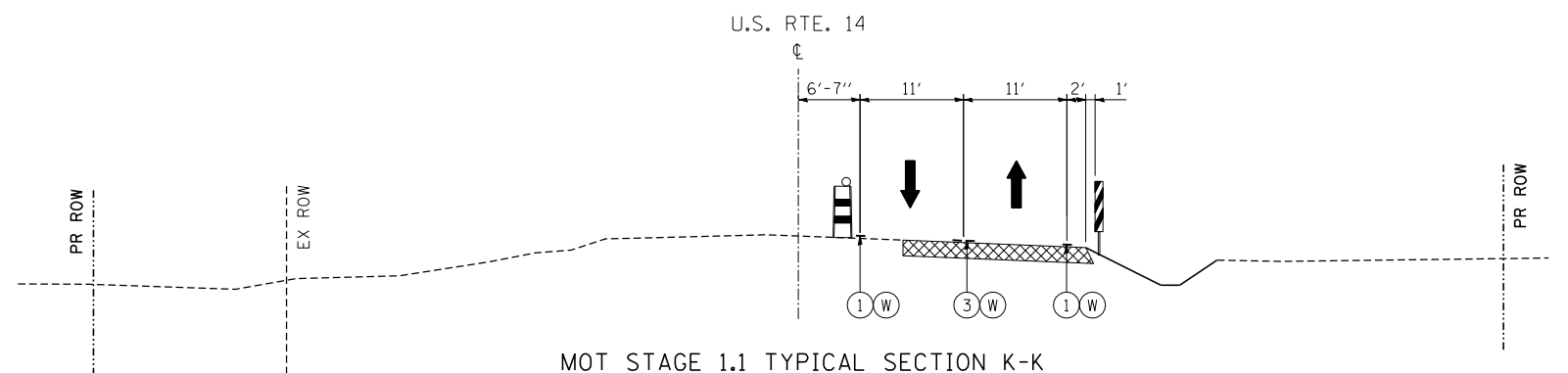
MOT STAGE 1.1 TYPICAL SECTION I-I

U.S. ROUTE 14
STA. 435+00



MOT STAGE 1.1 TYPICAL SECTION J-J

U.S. ROUTE 14
STA. 480+00



MOT STAGE 1.1 TYPICAL SECTION K-K

U.S. ROUTE 14
STA. 492+50

MAINTENANCE OF TRAFFIC LEGEND

- TEMPORARY PAVEMENT (SEE SHT 83 FOR SECTION)
- DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
- (T) TEMP PVT MK
- TEMPORARY CONCRETE BARRIER
- EXISTING TRAFFIC DIRECTION
- TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
- PROPOSED TRAFFIC DIRECTION
- (1) PVT MK - LINE 4" WHITE
- (2) PVT MK - LINE 4" YELLOW
- (3) PVT MK - DBL 4" YELLOW
- (4) PVT MK - LINE 6" WHITE
- (5) PVT MK - LINE 8" WHITE
- (6) EXISTING PAVEMENT MARKING
- (W) WET REF TEM TAPE T3

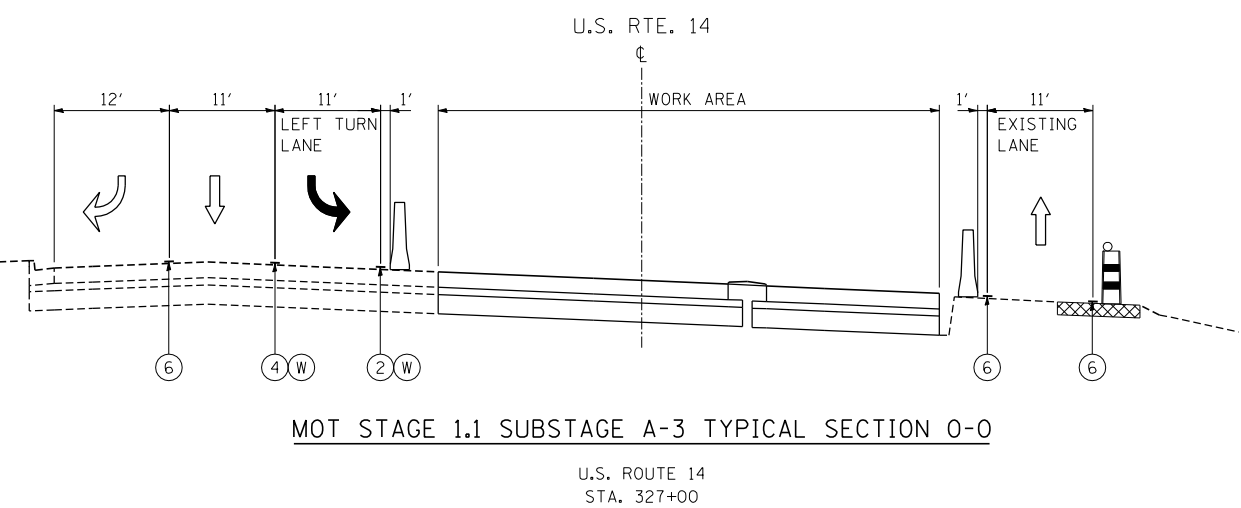
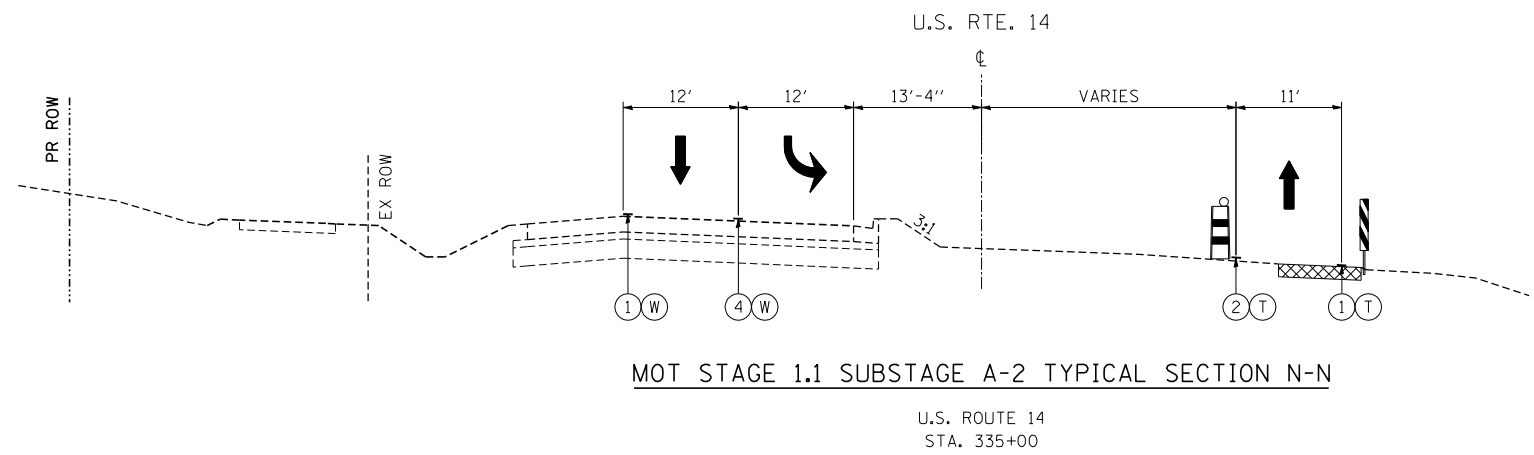
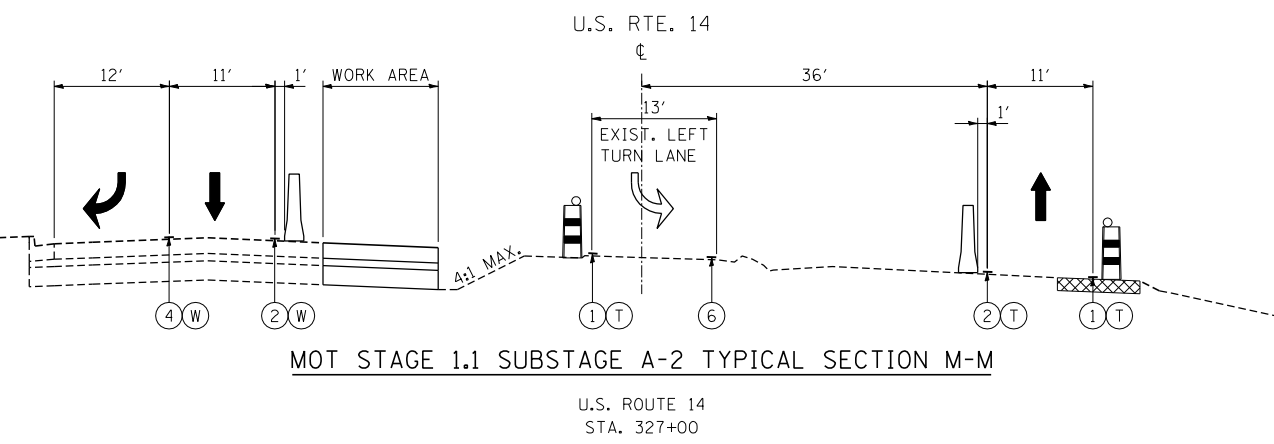
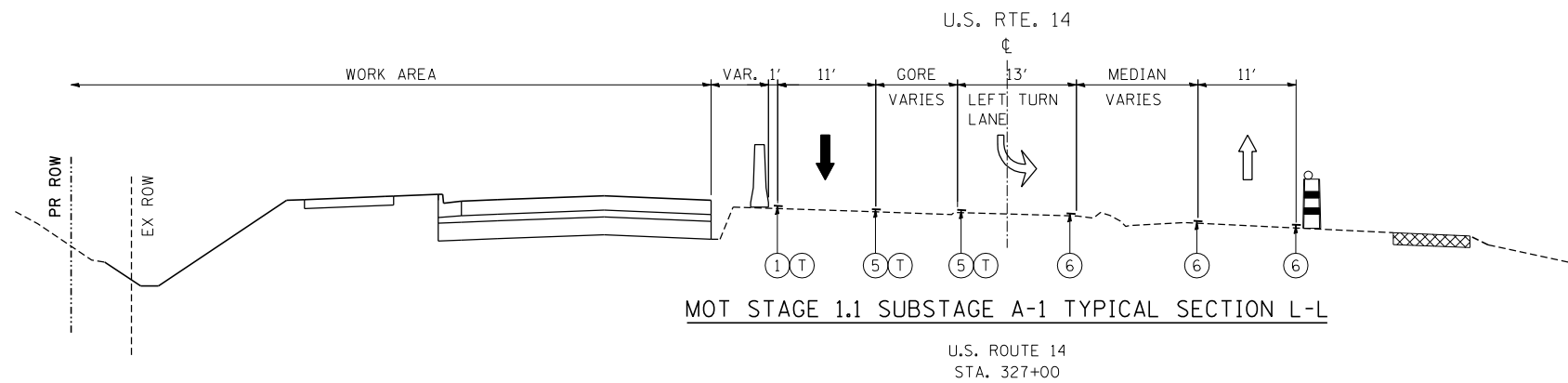
\NTSC00_31.DGN, \NTTB006.LJ.DGN, \GEN00477.DGN, \HECHTBR T:\DOCUMENT\19349901\CIVIL\DOGTSC00_39.DGN
 11-07-2013, 16:25:31

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
FILEL		DRAWN - JP	REVISED -
exp U.S. Services Inc. Chicago, IL	PLOT SCALE = *SCALE*	CHECKED - BRH	REVISED -
BUILDING-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT DATE = *DATE*	DATE - 11/01/13	REVISED -















STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

MOT TYPICAL SECTIONS			
U.S. ROUTE 14			
STAGE 1.1 - SHEET II			
SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	85
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



MAINTENANCE OF TRAFFIC LEGEND

-  TEMPORARY PAVEMENT (SEE SHT 83 FOR SECTION)
-  DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
-  PVT MK - LINE 4" WHITE
-  TEMP PVT MK
-  TEMPORARY CONCRETE BARRIER
-  EXISTING TRAFFIC DIRECTION
-  PVT MK - LINE 4" YELLOW
-  PVT MK - DBL 4" YELLOW
-  PVT MK - LINE 6" WHITE
-  PVT MK - LINE 8" WHITE
-  EXISTING PAVEMENT MARKING
-  TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
-  PROPOSED TRAFFIC DIRECTION
-  WET REF TEM TAPE T3

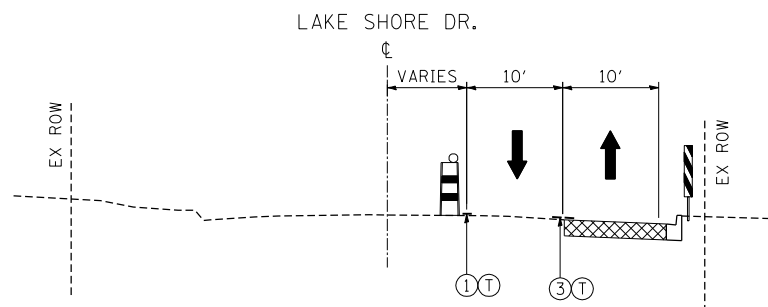
\NTSC00_31.DGN, \NTTB006.LJ.DGN, \GEN00477.DGN, \HECHTBR T:\DOCUMENT\19349901\CIVIL\DGN\TSC00_3A.DGN
 11-07-2013, 16:25:33

FILE NAME = #FILEL#	USER NAME = HECHTBR	DESIGNED -	REVISED -
exp U.S. Services Inc. Chicago, IL BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT SCALE = #SCALE#	DRAWN - JP	REVISED -
PLOT DATE = #DATE#	DATE - 11/01/13	CHECKED - BRH	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

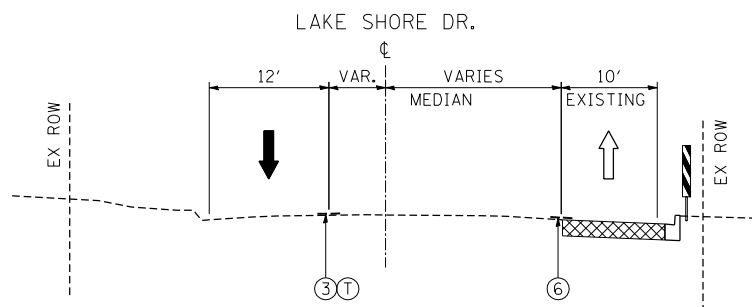
MOT TYPICAL SECTIONS			
U.S. ROUTE 14			
STAGE 1.1 SUBSTAGE A-1 TO A-3			
SCALE: N.T.S.	SHEET NO. OF SHEETS	STA. TO STA.	

F.A.P. RTE. 305	SECTION 27R-2	COUNTY MCHENRY	TOTAL SHEETS 673	SHEET NO. 86
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



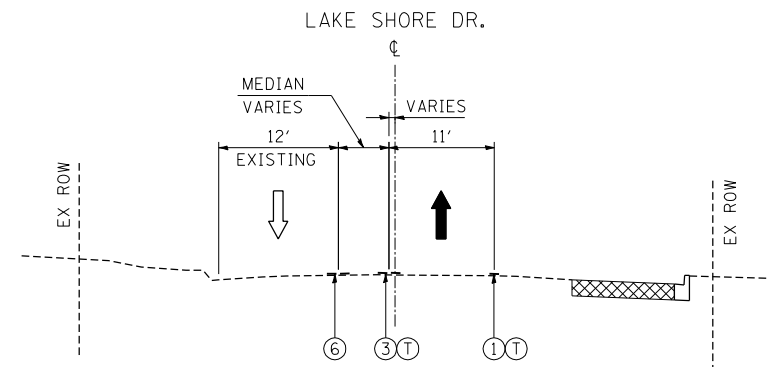
MOT STAGE 1.1 SUBSTAGE B-1 TYPICAL SECTION P-P

LAKE SHORE DRIVE
STA. 43+50



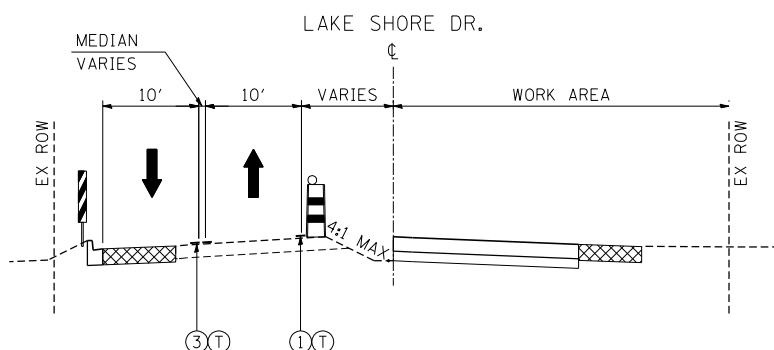
MOT STAGE 1.1 SUBSTAGE B-2 TYPICAL SECTION R-R

LAKE SHORE DRIVE
STA. 43+50



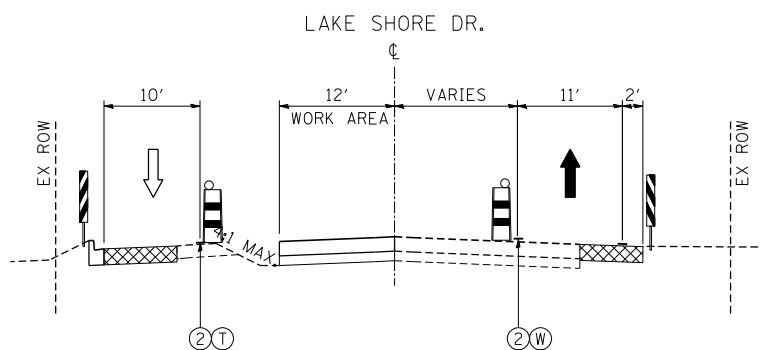
MOT STAGE 1.1 SUBSTAGE B-3 TYPICAL SECTION T-T

LAKE SHORE DRIVE
STA. 43+50



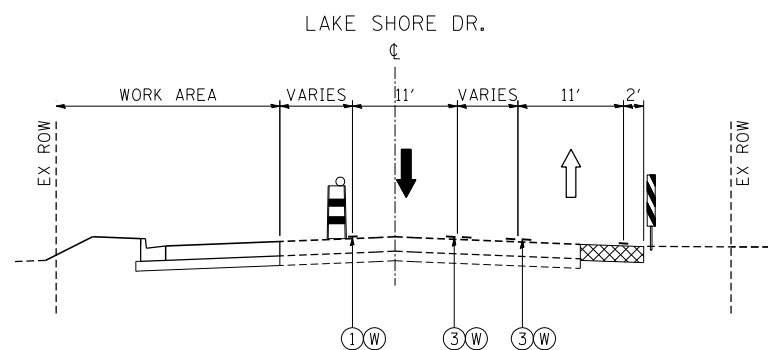
MOT STAGE 1.1 SUBSTAGE B-1 TYPICAL SECTION Q-Q

LAKE SHORE DRIVE
STA. 46+50



MOT STAGE 1.1 SUBSTAGE B-2 TYPICAL SECTION S-S

LAKE SHORE DRIVE
STA. 46+50



MOT STAGE 1.1 SUBSTAGE B-3 TYPICAL SECTION U-U

LAKE SHORE DRIVE
STA. 46+50

MAINTENANCE OF TRAFFIC LEGEND

- TEMPORARY PAVEMENT (SEE SHT 83 FOR SECTION)
- DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
- TEMPORARY CONCRETE BARRIER
- TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
- EXISTING TRAFFIC DIRECTION
- PROPOSED TRAFFIC DIRECTION

- ① PVT MK - LINE 4" WHITE
- ② PVT MK - LINE 4" YELLOW
- ③ PVT MK - DBL 4" YELLOW
- ④ PVT MK - LINE 6" WHITE
- ⑤ PVT MK - LINE 8" WHITE
- ⑥ EXISTING PAVEMENT MARKING
- Ⓣ TEMP PVT MK
- Ⓜ WET REF TEM TAPE T3

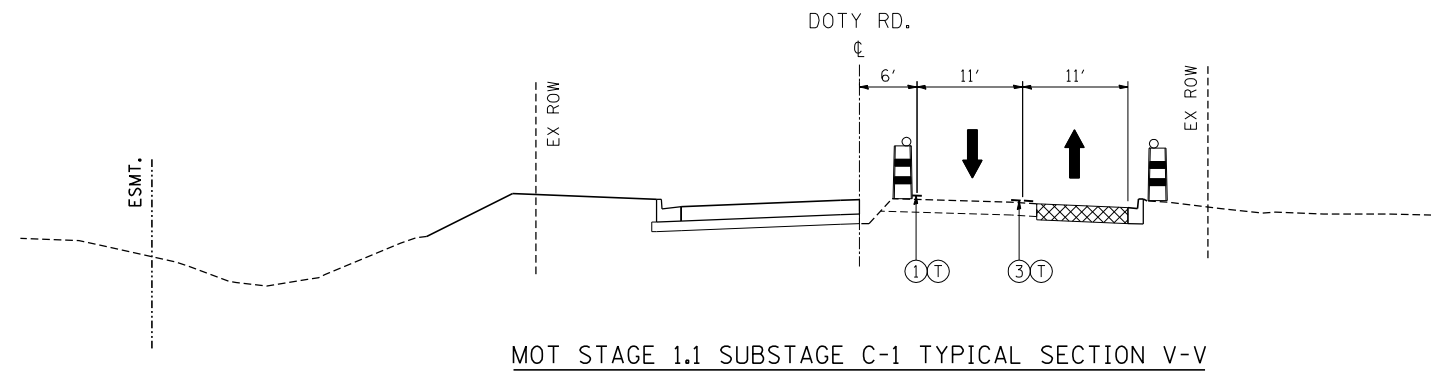
\NTSC00_31.DGN, \NTTB006.LJ.DGN, \GEN00477.DGN, \HECHTBR T:\DOCUMENT\19349901\CIVIL\DGN\TSC00_38.DGN
 11-07-2013, 16:25:35

FILE NAME = *FILEL*	USER NAME = HECHTBR	DESIGNED -	REVISED -
exp U.S. Services Inc. Chicago, IL BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT SCALE = *SCALE*	DRAWN - JP	REVISED -
	PLOT DATE = *DATE*	CHECKED - BRH	REVISED -
		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

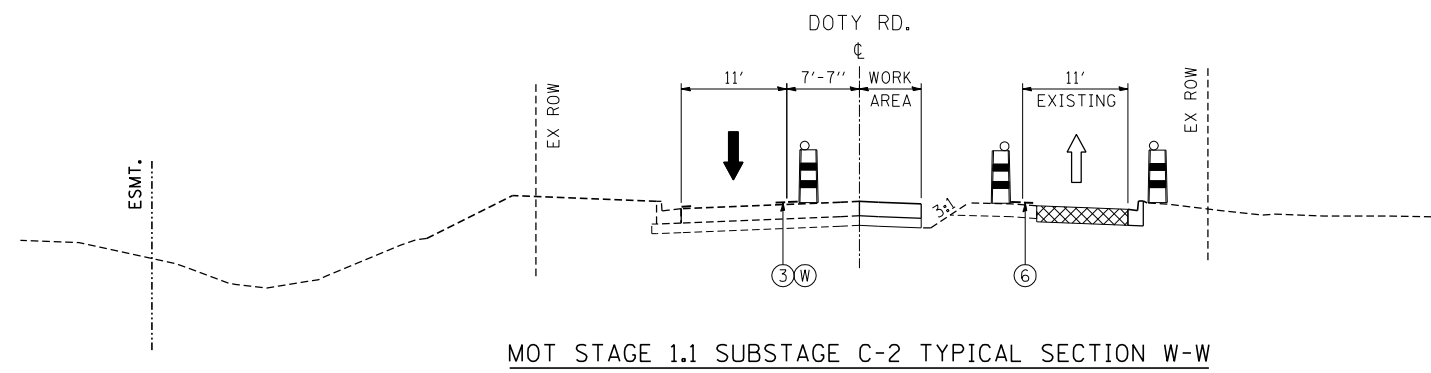
MOT TYPICAL SECTIONS			
LAKE SHORE DRIVE			
STAGE 1.1 SUBSTAGE B-1 TO B-3			
SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	87
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



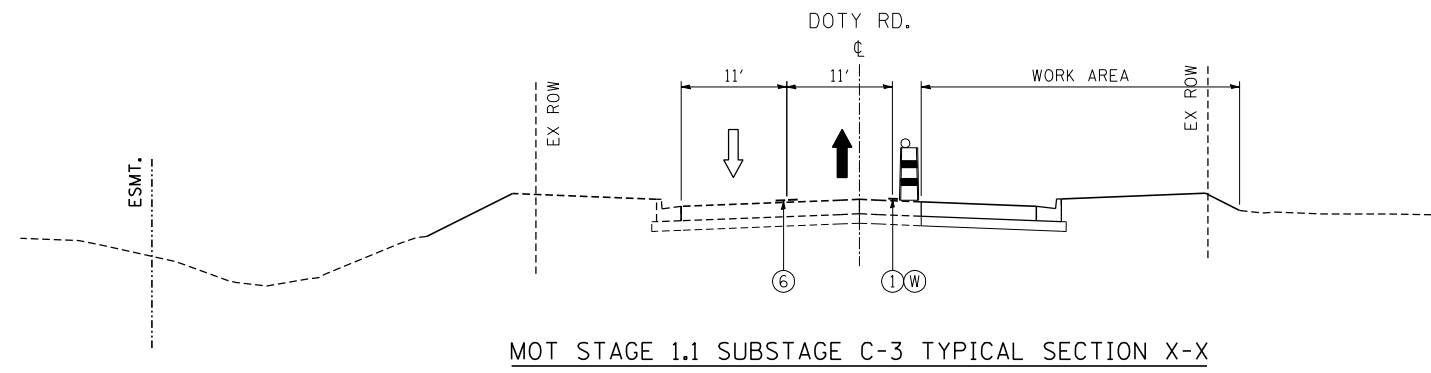
MOT STAGE 1.1 SUBSTAGE C-1 TYPICAL SECTION V-V

DOTY ROAD
STA. 1+50



MOT STAGE 1.1 SUBSTAGE C-2 TYPICAL SECTION W-W

DOTY ROAD
STA. 1+50



MOT STAGE 1.1 SUBSTAGE C-3 TYPICAL SECTION X-X

DOTY ROAD
STA. 1+50

MAINTENANCE OF TRAFFIC LEGEND

- | | | | | | | | |
|--|--|--|--|---|---------------------------|---|---------------------|
| | TEMPORARY PAVEMENT
(SEE SHT 83 FOR SECTION) | | DOUBLE VERTICAL PANEL
WITH TYPE C STEADY
BURN BI-DIRECTIONAL LIGHT | ① | PVT MK - LINE 4" WHITE | Ⓣ | TEMP PVT MK |
| | TEMPORARY CONCRETE BARRIER | | EXISTING TRAFFIC DIRECTION | ② | PVT MK - LINE 4" YELLOW | Ⓜ | WET REF TEM TAPE T3 |
| | TYPE II BARRICADE OR DRUM
W/ TYPE C STEADY BURN
BI-DIRECTIONAL LIGHT | | PROPOSED TRAFFIC DIRECTION | ③ | PVT MK - DBL 4" YELLOW | | |
| | | | | ④ | PVT MK - LINE 6" WHITE | | |
| | | | | ⑤ | PVT MK - LINE 8" WHITE | | |
| | | | | ⑥ | EXISTING PAVEMENT MARKING | | |

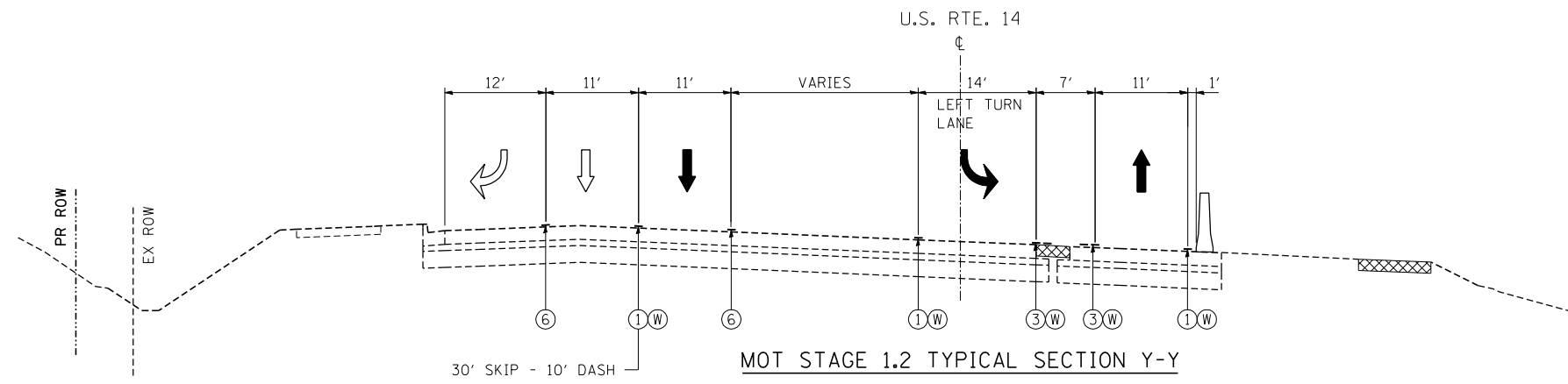
T:\DOCUMENTS\1934\9901\CIVIL\DOTY\DOTY\SC00.PM.DGN
 11-07-2013 16:25:37
 HECHTBR
 exp U.S. Services Inc.
 CHICAGO, IL
 BUILDING-EARTH & ENVIRONMENT-ENERGY
 INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY

FILE NAME = *FILEL*	USER NAME = HECHTBR	DESIGNED -	REVISED -
		DRAWN - JP	REVISED -
		CHECKED - BRH	REVISED -
		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

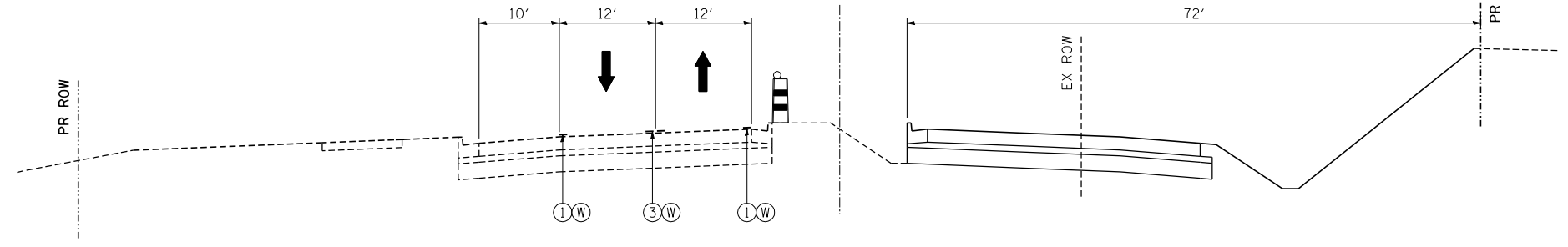
MOT TYPICAL SECTIONS			
DOTY ROAD			
STAGE 1.1 SUBSTAGE C-1 TO C-3			
SCALE: N.T.S.	SHEET NO. OF SHEETS	STA. TO STA.	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	88
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



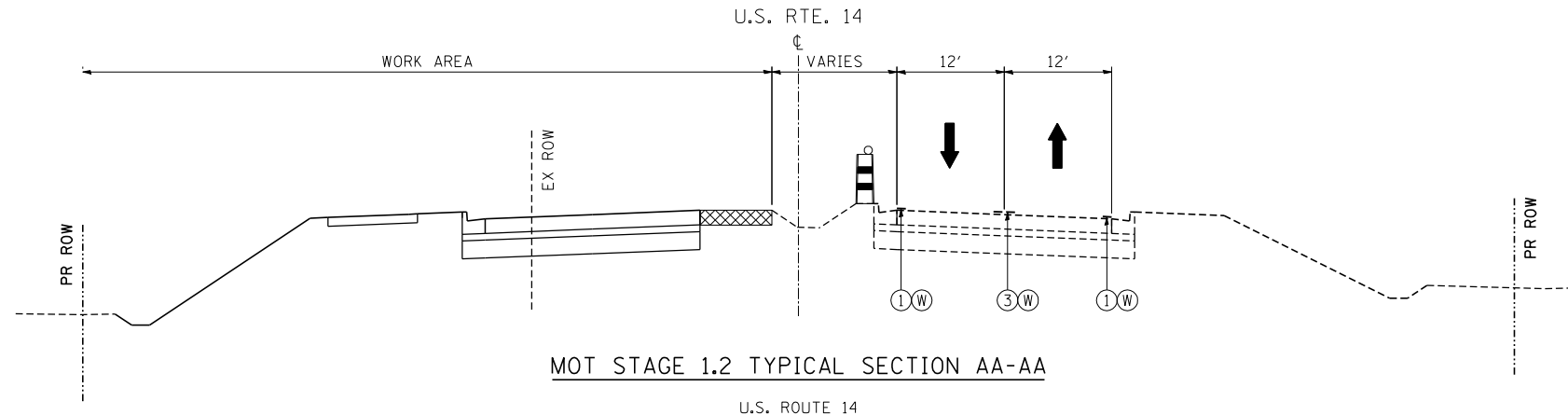
MOT STAGE 1.2 TYPICAL SECTION Y-Y

U.S. ROUTE 14
STA. 327+00



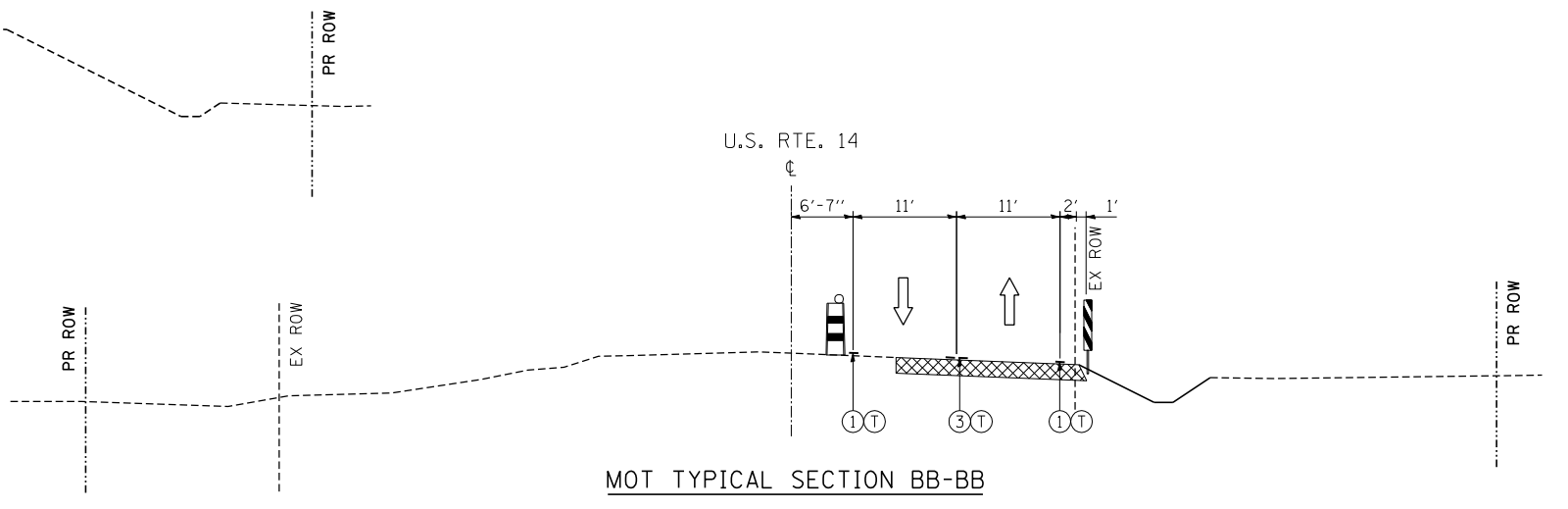
MOT STAGE 1.2 TYPICAL SECTION Z-Z

U.S. ROUTE 14
STA. 375+00



MOT STAGE 1.2 TYPICAL SECTION AA-AA

U.S. ROUTE 14
STA. 482+00



MOT TYPICAL SECTION BB-BB

U.S. ROUTE 14
STA. 492+50

MAINTENANCE OF TRAFFIC LEGEND

- TEMPORARY PAVEMENT (SEE SHT 83 FOR SECTION)
- DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
- PVT MK - LINE 4" WHITE
- TEMP PVT MK
- TEMPORARY CONCRETE BARRIER
- PVT MK - LINE 4" YELLOW
- WET REF TEM TAPE T3
- EXISTING TRAFFIC DIRECTION
- PVT MK - DBL 4" YELLOW
- PVT MK - LINE 6" WHITE
- PVT MK - LINE 8" WHITE
- EXISTING PAVEMENT MARKING
- TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
- PROPOSED TRAFFIC DIRECTION

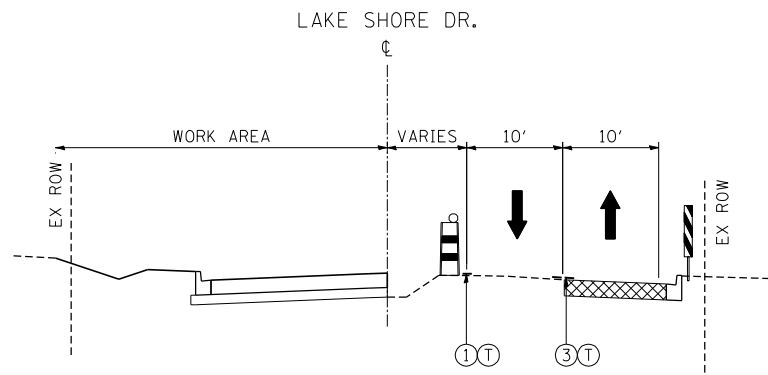
T:\DOCUMENTS\1934\9901\CIVIL\DOGN\TSC00.FIN.DGN
 11-07-2013 16:25:40
 HECHTBR

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
FILEL		DRAWN - JP	REVISED -
		CHECKED - BRH	REVISED -
		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

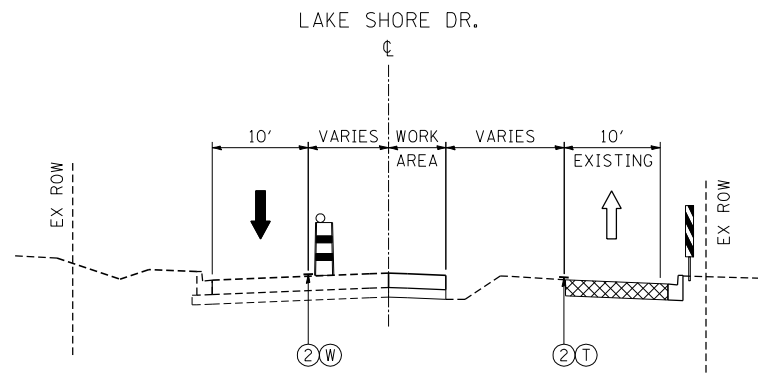
MOT TYPICAL SECTIONS			
U.S. ROUTE 14			
STAGE 1.2			
SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	89
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



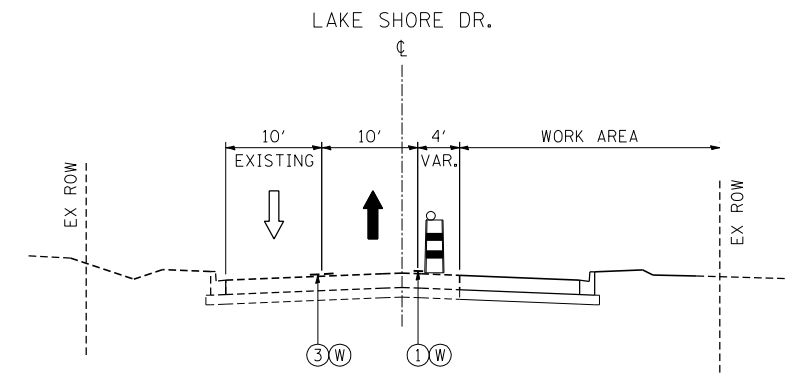
MOT STAGE 1.2 SUBSTAGE A-1 TYPICAL SECTION CC-CC

LAKE SHORE DRIVE
STA. 43+50



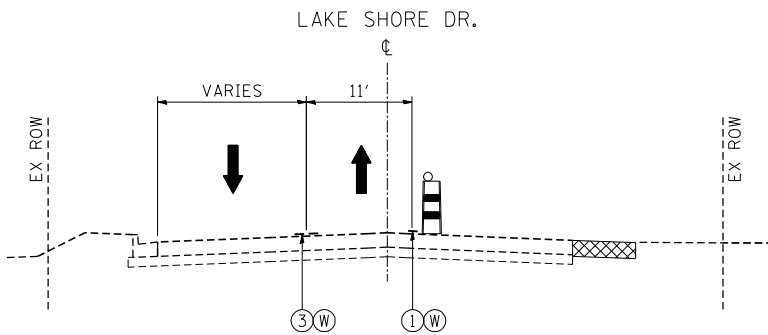
MOT STAGE 1.2 SUBSTAGE A-2 TYPICAL SECTION EE-EE

LAKE SHORE DRIVE
STA. 43+50



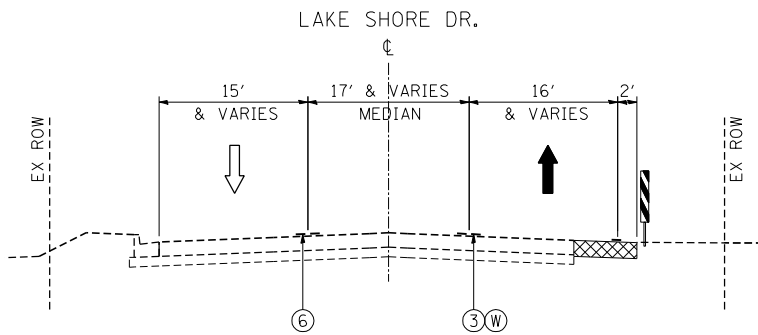
MOT STAGE 1.2 SUBSTAGE A-3 TYPICAL SECTION GG-GG

LAKE SHORE DRIVE
STA. 43+50



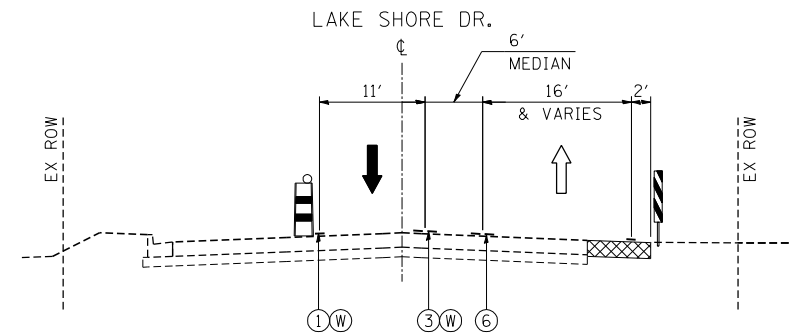
MOT STAGE 1.2 SUBSTAGE A-1 TYPICAL SECTION DD-DD

LAKE SHORE DRIVE
STA. 46+50



MOT STAGE 1.2 SUBSTAGE A-2 TYPICAL SECTION FF-FF







LAKE SHORE DRIVE
STA. 46+50



MOT STAGE 1.2 SUBSTAGE A-3 TYPICAL SECTION HH-HH

LAKE SHORE DRIVE
STA. 46+50

MAINTENANCE OF TRAFFIC LEGEND

-  TEMPORARY PAVEMENT (SEE SHT 83 FOR SECTION)
-  DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
-  TEMPORARY CONCRETE BARRIER
-  EXISTING TRAFFIC DIRECTION
-  TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
-  PROPOSED TRAFFIC DIRECTION

- ① PVT MK - LINE 4" WHITE
- ② PVT MK - LINE 4" YELLOW
- ③ PVT MK - DBL 4" YELLOW
- ④ PVT MK - LINE 6" WHITE
- ⑤ PVT MK - LINE 8" WHITE
- ⑥ EXISTING PAVEMENT MARKING
- Ⓣ TEMP PVT MK
- Ⓜ WET REF TEM TAPE T3

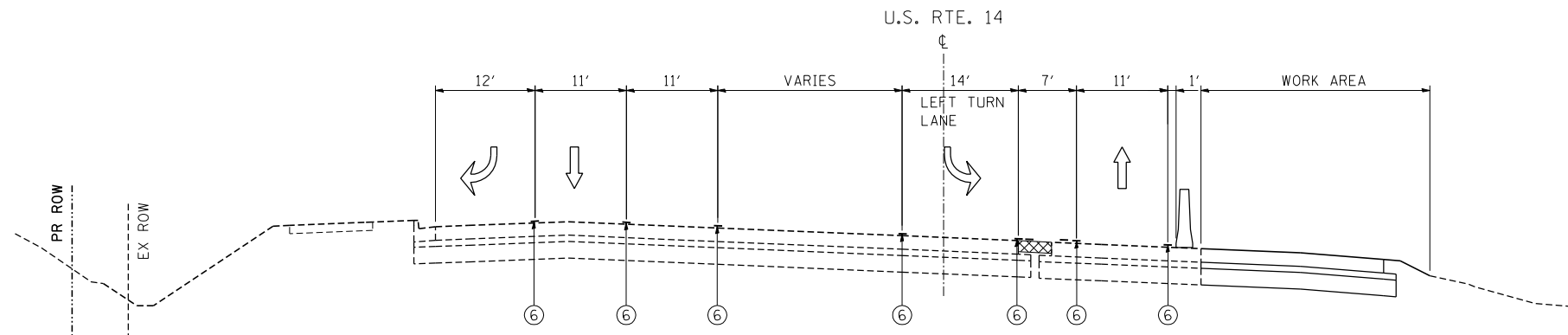
\\NTSC0031.DGN, \\NTTB006.LJ.DGN, \\GEN00477.DGN, \\HECHTBR T:\DOCUMENT\19349901\CIVIL\DGN\TSC00.FO.DGN
 11-07-2013, 16:25:42

FILE NAME = *FILEL*	USER NAME = HECHTBR	DESIGNED -	REVISED -
exp U.S. Services Inc. Chicago, IL BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT SCALE = *SCALE*	DRAWN - JP	REVISED -
	PLOT DATE = *DATE*	CHECKED - BRH	REVISED -
		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

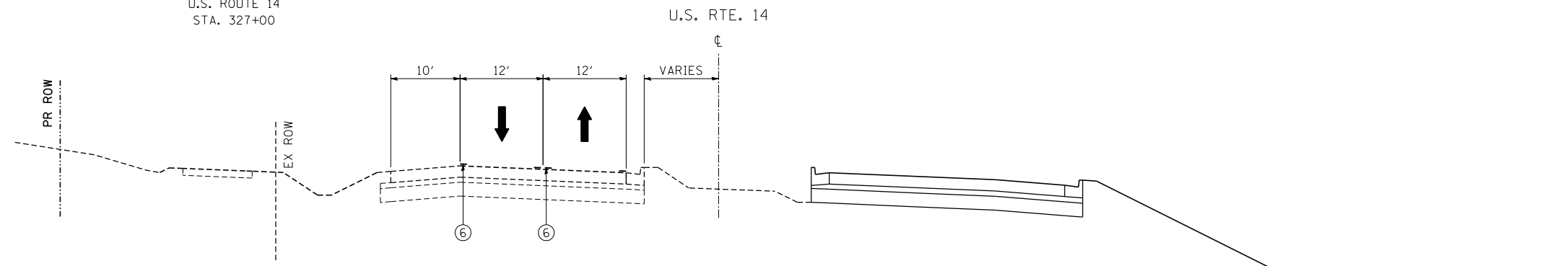
MOT TYPICAL SECTIONS			
U.S. ROUTE 14			
STAGE 1.2 SUBSTAGE A-1 TO A-3			
SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	90
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62268	



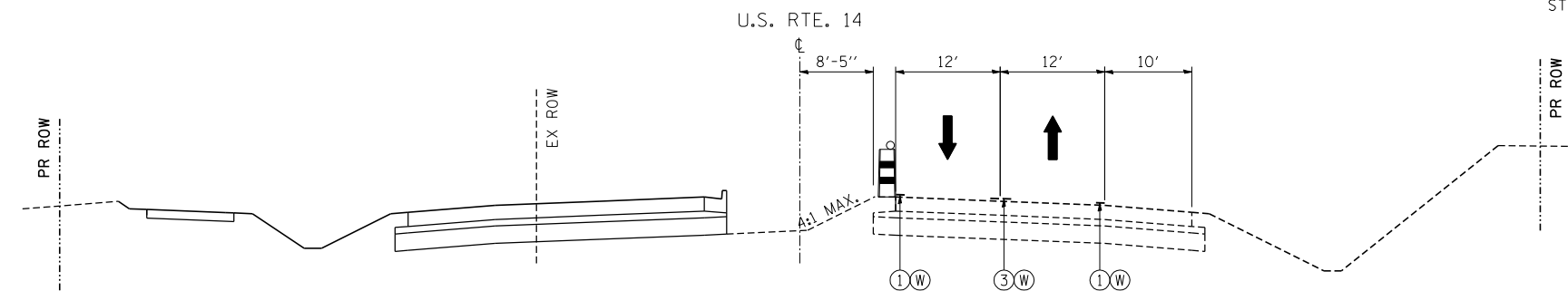
MOT STAGE 2.1 TYPICAL SECTION II-II

U.S. ROUTE 14
STA. 327+00



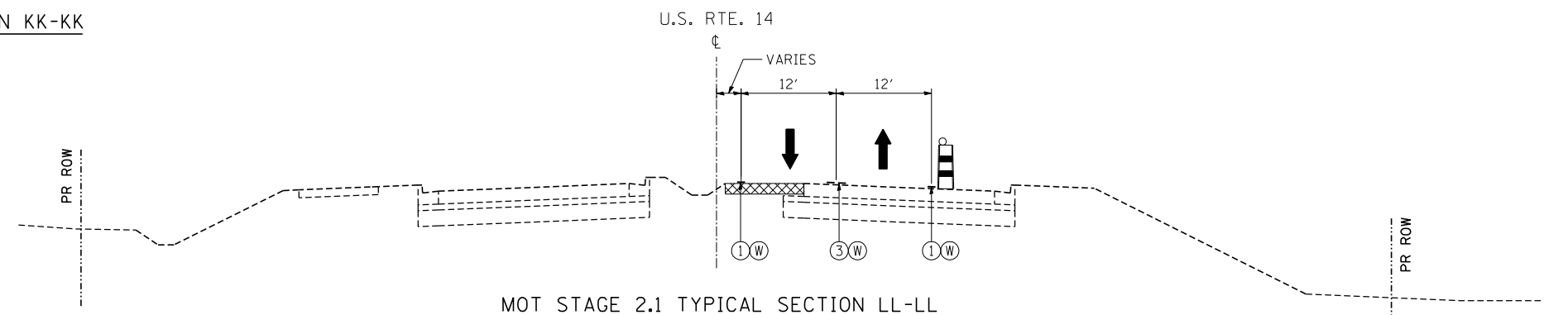
MOT STAGE 2.1 TYPICAL SECTION JJ-JJ

U.S. ROUTE 14
STA. 335+00



MOT STAGE 2.1 TYPICAL SECTION KK-KK















U.S. ROUTE 14
STA. 405+00



MOT STAGE 2.1 TYPICAL SECTION LL-LL

U.S. ROUTE 14
STA. 480+00

MAINTENANCE OF TRAFFIC LEGEND

-  TEMPORARY PAVEMENT (SEE SHT 83 FOR SECTION)
-  DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
-  ① PVT MK - LINE 4" WHITE
-  (T) TEMP PVT MK
-  TEMPORARY CONCRETE BARRIER
-  ② PVT MK - LINE 4" YELLOW
-  (W) WET REF TEM TAPE T3
-  EXISTING TRAFFIC DIRECTION
-  ③ PVT MK - DBL 4" YELLOW
-  ④ PVT MK - LINE 6" WHITE
-  ⑤ PVT MK - LINE 8" WHITE
-  ⑥ EXISTING PAVEMENT MARKING
-  TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
-  PROPOSED TRAFFIC DIRECTION

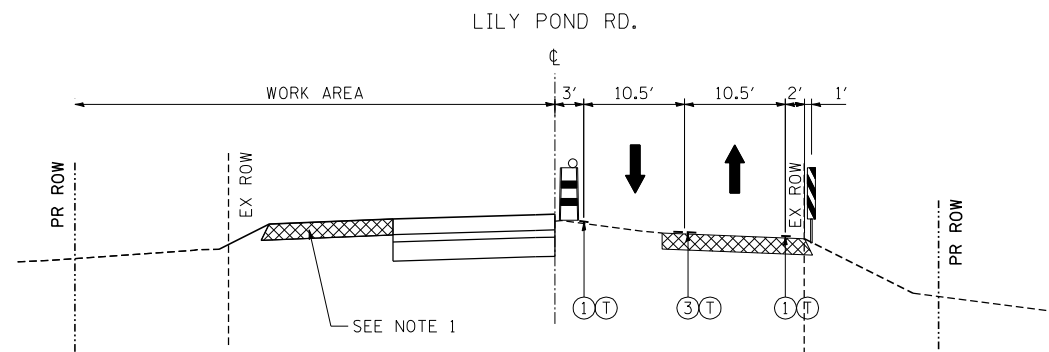
\NTSC00\31.DGN, \NTTB006\JJ.DGN, \GEN004\77.DGN, \HECHTBR T:\DOCUMENT\1934\9901\CIVIL\DGN\TSC00_PP.DGN
 11-07-2013, 16:25:44

FILE NAME = *FILEL*	USER NAME = HECHTBR	DESIGNED -	REVISED -
		DRAWN - JP	REVISED -
		CHECKED - BRH	REVISED -
		DATE - 11/01/13	REVISED -
	PLOT SCALE = *SCALE*		
	PLOT DATE = *DATE*		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

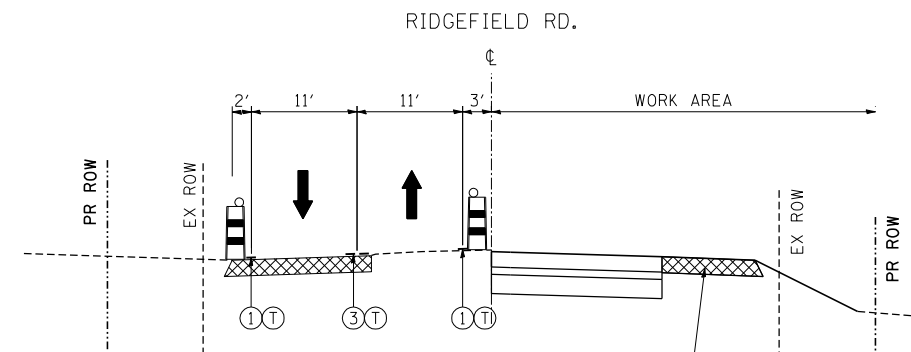
MOT TYPICAL SECTIONS			
U.S. ROUTE 14			
STAGE 2.1			
SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	91
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



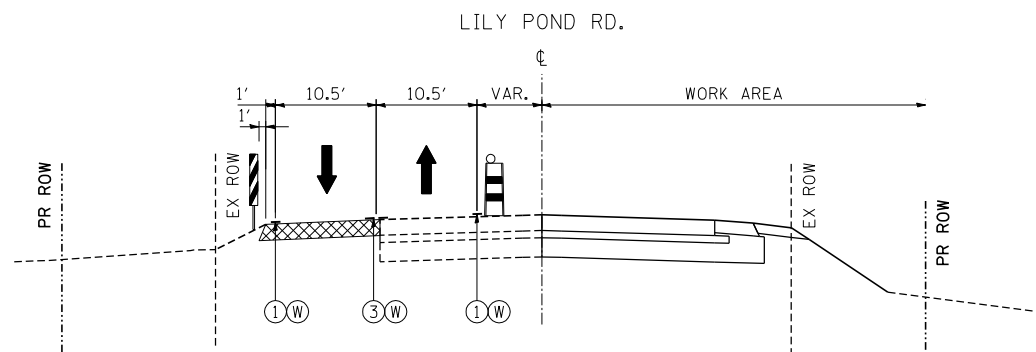
MOT STAGE 2.1 SUBSTAGE A-2 TYPICAL SECTION MM-MM

LILY POND RD.
STA. 804+00



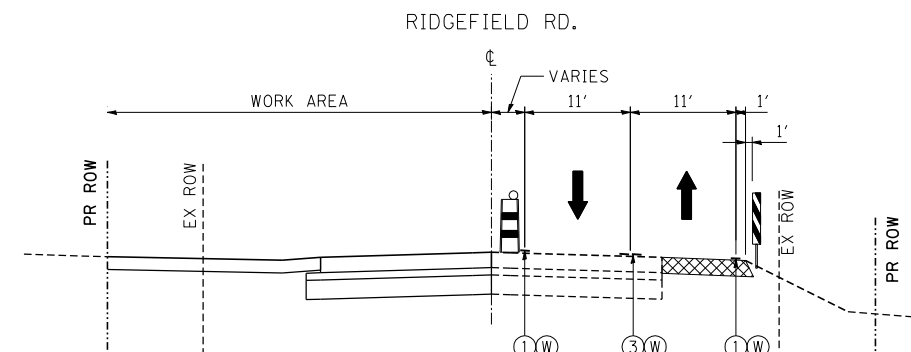
MOT STAGE 2.1 SUBSTAGE B-2 TYPICAL SECTION 00-00

RIDGEFIELD RD. NORTH
STA. 854+00



MOT STAGE 2.1 SUBSTAGE A-3 TYPICAL SECTION NN-NN

LILY POND RD.
STA. 804+00



MOT STAGE 2.1 SUBSTAGE B-3 TYPICAL SECTION PP-PP

RIDGEFIELD RD. NORTH
STA. 854+00

MAINTENANCE OF TRAFFIC LEGEND

- TEMPORARY PAVEMENT (SEE SHT 83 FOR SECTION)
- TEMPORARY CONCRETE BARRIER
- TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
- DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN BI-DIRECTIONAL LIGHT
- EXISTING TRAFFIC DIRECTION
- PROPOSED TRAFFIC DIRECTION
- 1 PVT MK - LINE 4" WHITE
- 2 PVT MK - LINE 4" YELLOW
- 3 PVT MK - DBL 4" YELLOW
- 4 PVT MK - LINE 6" WHITE
- 5 PVT MK - LINE 8" WHITE
- 6 EXISTING PAVEMENT MARKING
- T TEMP PVT MK
- W WET REF TEM TAPE T3

NOTE:

1. TEMPORARY PAVEMENT CONSTRUCTED DURING STAGE 2.1 SUBSTAGE A-2 AND B-2 SHALL BE CONSTRUCTED AS PERMANENT SHOULDER ALONG LILY POND RD AND RIDGEFIELD ROAD NORTH.

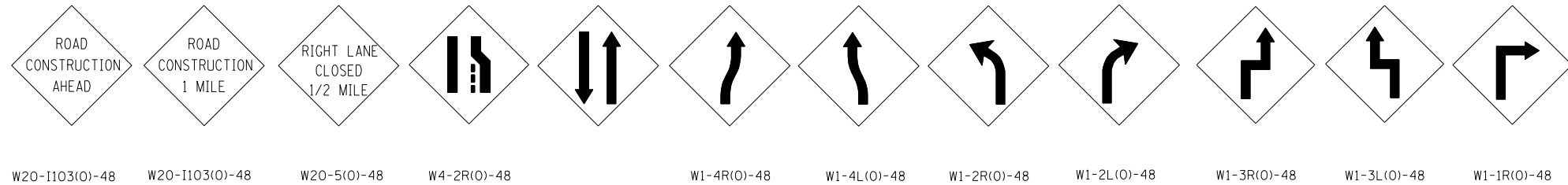
\NTSC00\31.DGN, \NTTB006.LJ.DGN, \GEN00477.DGN, \HECHTBR T:\DOCUMENT\934\9901\CIVIL\DGN\TSC00.F0.DGN
 11-07-2013, 16:25:46

FILE NAME = #FILEL#	USER NAME = HECHTBR	DESIGNED -	REVISED -
		DRAWN - JP	REVISED -
		CHECKED - BRH	REVISED -
		DATE - 11/01/13	REVISED -

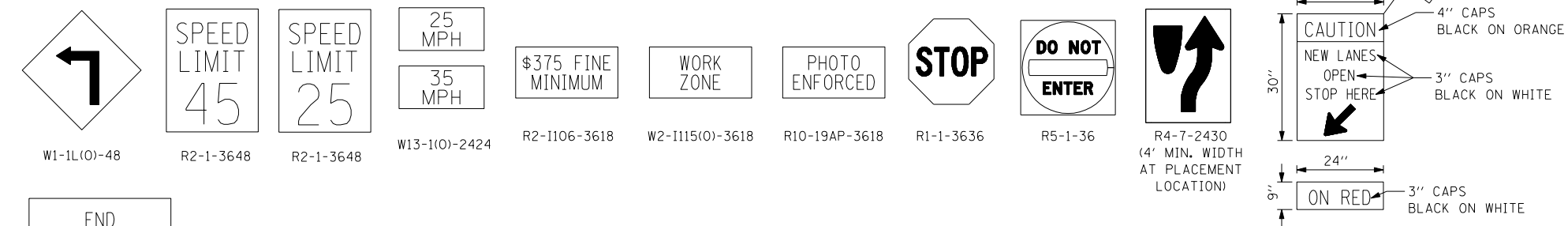
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

MOT TYPICAL SECTIONS			
LILY POND ROAD & RIDGEFIELD ROAD NORTH			
STAGE 2.1 SUBSTAGE A-2, A-3, B-2 & B-3			
SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	92
CONTRACT NO. 62268				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



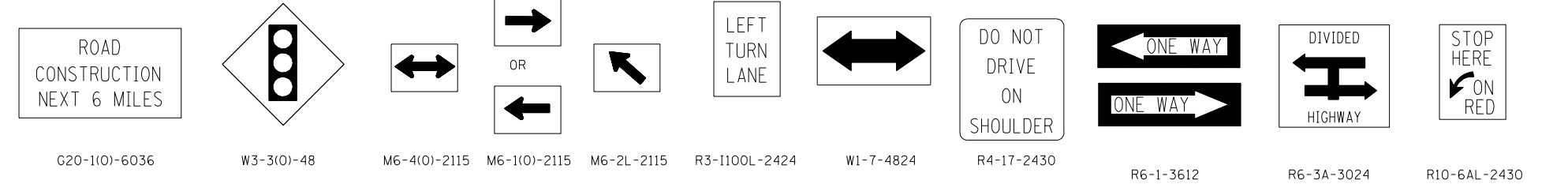
W20-1103(O)-48 W20-1103(O)-48 W20-5(O)-48 W4-2R(O)-48 W1-4R(O)-48 W1-4L(O)-48 W1-2R(O)-48 W1-2L(O)-48 W1-3R(O)-48 W1-3L(O)-48 W1-1R(O)-48



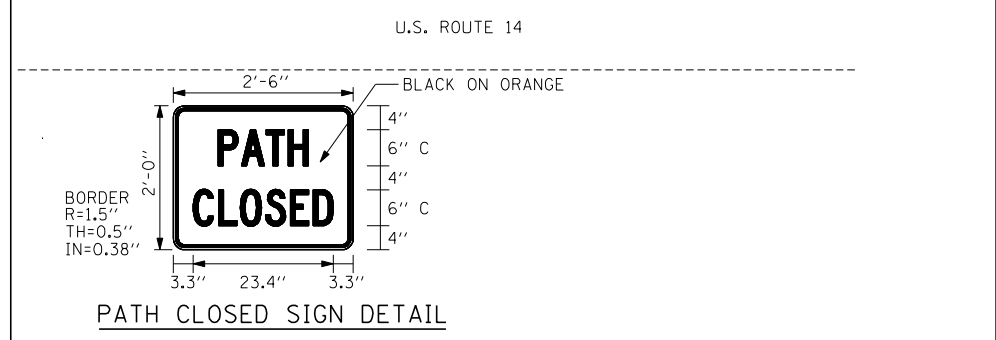
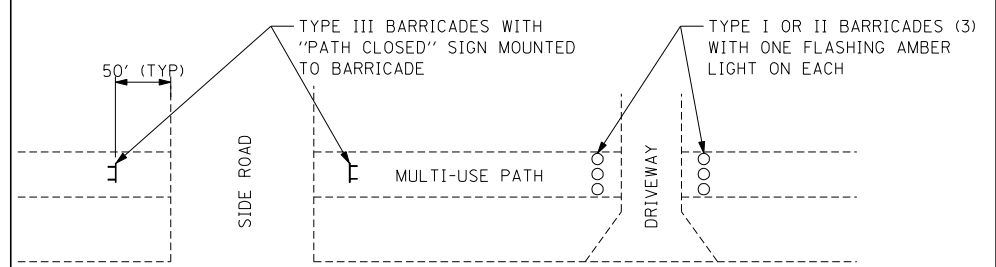
W1-1L(O)-48 R2-1-3648 R2-1-3648 W13-1(O)-2424 R2-1106-3618 W2-1115(O)-3618 R10-19AP-3618 R1-1-3636 R5-1-36 R4-7-2430 (4' MIN. WIDTH AT PLACEMENT LOCATION) 18"x18" FLAG 4" CAPS BLACK ON ORANGE 3" CAPS BLACK ON WHITE 3" CAPS BLACK ON WHITE



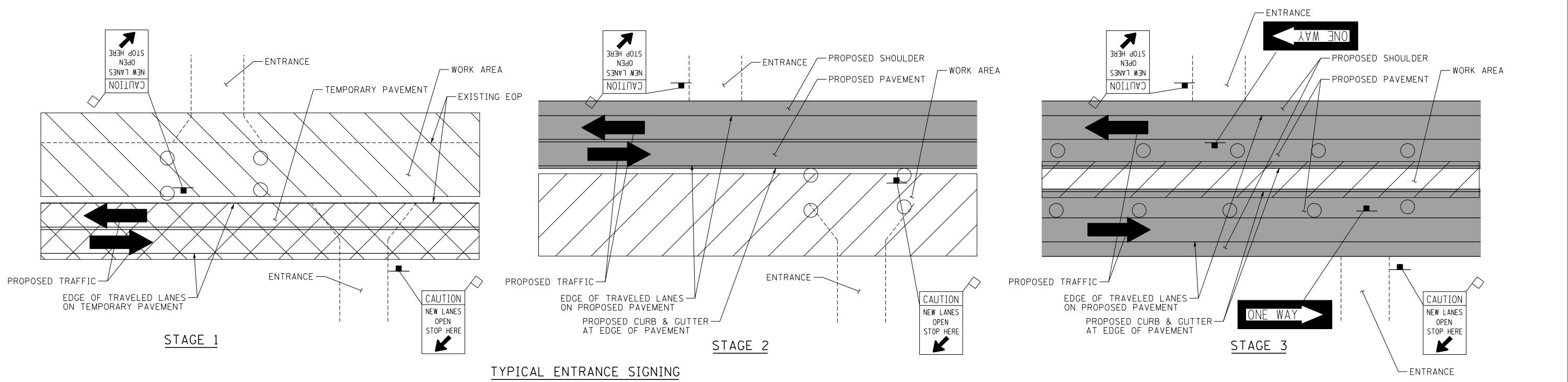
G20-2a(O)-6024



G20-1(O)-6036 W3-3(O)-48 M6-4(O)-2115 M6-1(O)-2115 M6-2L-2115 R3-1100L-2424 W1-7-4824 R4-17-2430 R6-1-3612 R6-3A-3024 R10-6AL-2430



- MULTI-USE PATH CONSTRUCTION SIGNING**
- "PATH CLOSED" SIGN AND BARRICADES SHALL BE ERECTED IMMEDIATELY AFTER CONSTRUCTION OF MULTI-USE PATH HAS BEGUN AND SHALL REMAIN IN PLACE UNTIL PATH IS OPENED TO USE.
 - "PATH CLOSED" SIGN SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.
 - TYPE III BARRICADE WITH ATTACHED SIGN PANEL SHALL MEET NCHRP 350.



TYPICAL ENTRANCE SIGNING

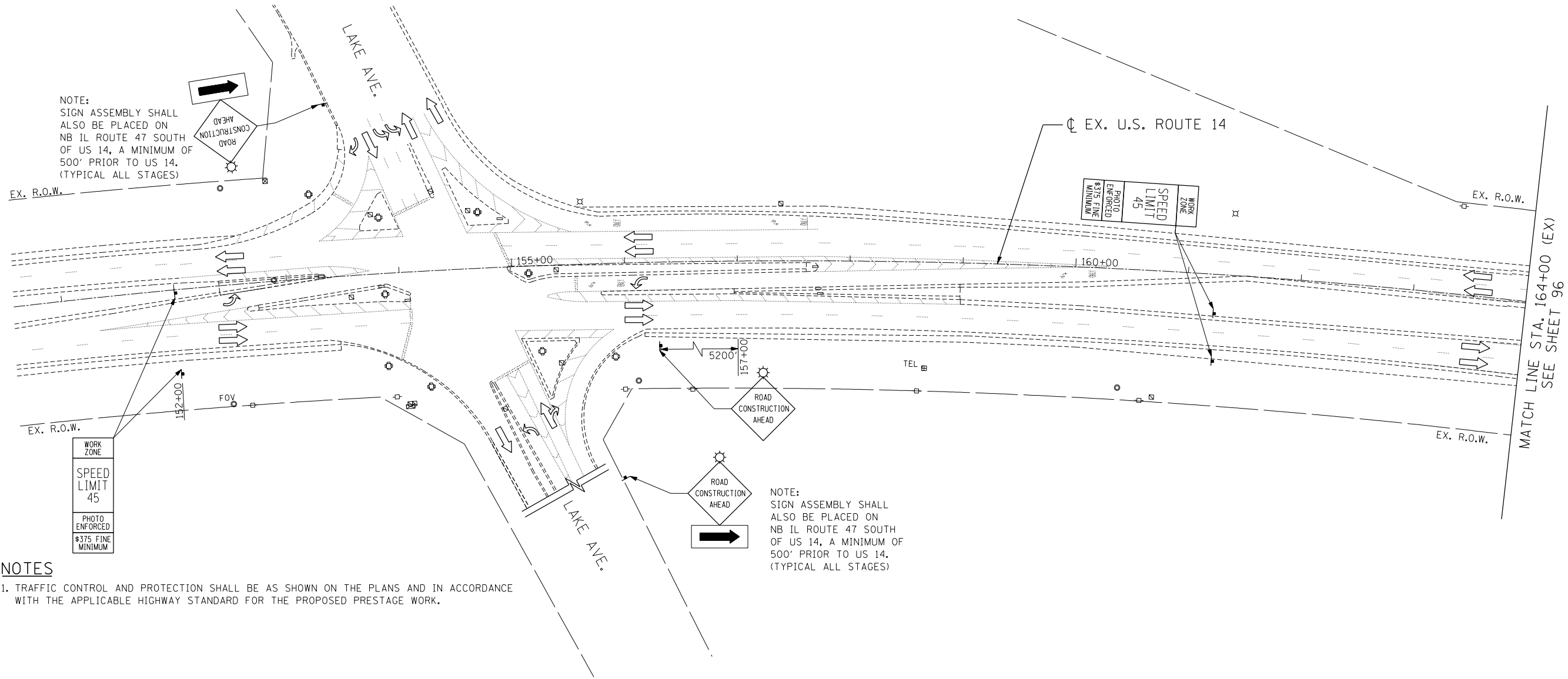
"CAUTION NEW LANES OPEN STOP HERE" SIGNS SHALL BE PLACED AT EACH ENTRANCE WITHIN THE CONTRACT LIMITS DURING EACH STAGE OF CONSTRUCTION. THE LOCATION SHALL BE 12' MINIMUM FROM THE EDGE OF THE TRAVELED LANE OR 6' MINIMUM FROM THE EDGE OF THE HMA/PCC SHOULDER AND AS DIRECTED BY THE ENGINEER.

FILE NAME = USER NAME = HECHTBR DESIGNED - REVISED -
 #FILEL# DRAWN - AMB REVISED -
 exp U.S. Services Inc. CHECKED - KA REVISED -
 BUILDINGS-EARTH & ENVIRONMENT-ENERGY DATE - 11/01/13 REVISED -
 INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14		MAINTENANCE OF TRAFFIC SIGN DESCRIPTIONS AND DETAILS		F.A.P. RTE. 305	SECTION 27R-2	COUNTY MCHENRY	TOTAL SHEETS 673	SHEET NO. 94
SCALE: N.T.S.				SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 62268		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								

MAINTENANCE OF TRAFFIC LEGEND

	WORK AREA		EXISTING TRAFFIC DIRECTION		ARROW BOARD		PVT MK - LINE 4" WHITE		PVT MK - LINE 24" WHITE
	TEMPORARY PAVEMENT (CURRENT STAGE)		PROPOSED TRAFFIC DIRECTION		DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN LIGHT		PVT MK - LINE 4" YELLOW		PVT MK - LINE 8" WHITE
	TEMPORARY PAVEMENT (PREVIOUS STAGES)		BARRICADE, TYPE III, W/ TYPE A FLASHING LIGHTS		PROPOSED SIGN		PVT MK - DBL 4" YELLOW		TEMP PVT MK
	CONSTRUCTED PAVEMENT		TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT		TYPE II BARRICADE OR DRUM W/ TYPE A FLASHING LIGHT		PVT MK - LINE 6" WHITE		WET REF TEM TAPE T3
			TYPE B MONO-DIRECTIONAL FLASHING LIGHT				PVT MK - LINE 12"		
							PVT MK - LTRS & SYMB		



NOTE:
SIGN ASSEMBLY SHALL ALSO BE PLACED ON NB IL ROUTE 47 SOUTH OF US 14, A MINIMUM OF 500' PRIOR TO US 14. (TYPICAL ALL STAGES)

NOTE:
SIGN ASSEMBLY SHALL ALSO BE PLACED ON NB IL ROUTE 47 SOUTH OF US 14, A MINIMUM OF 500' PRIOR TO US 14. (TYPICAL ALL STAGES)

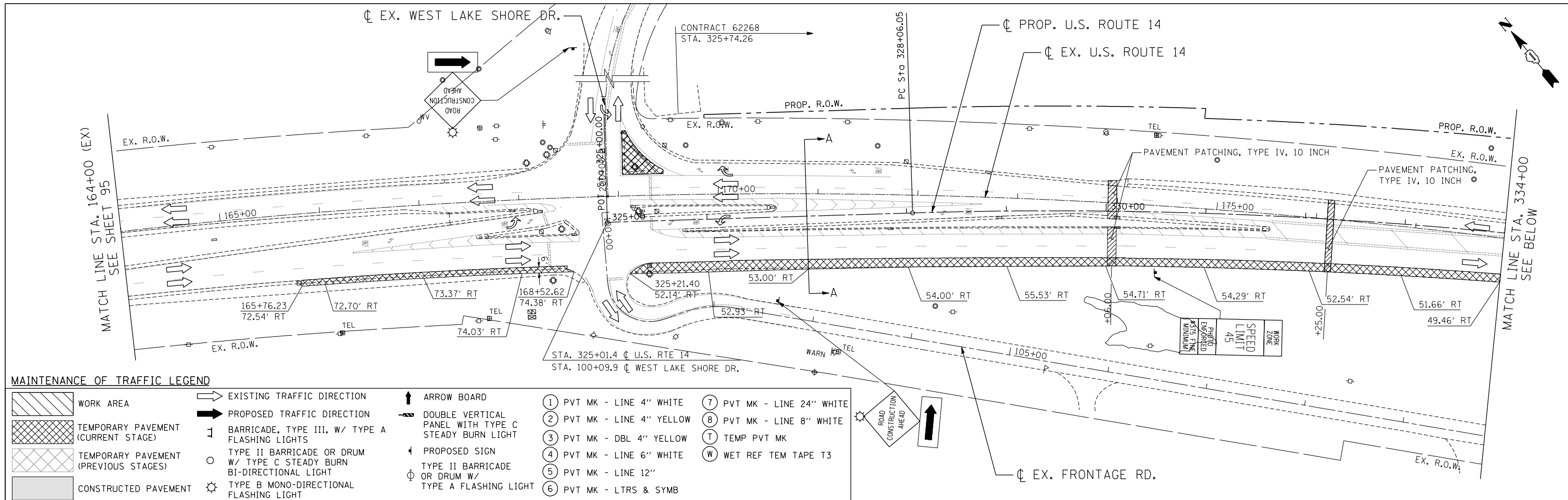
NOTES

1. TRAFFIC CONTROL AND PROTECTION SHALL BE AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH THE APPLICABLE HIGHWAY STANDARD FOR THE PROPOSED PRESTAGE WORK.

T:\DOCUMENT\1934\9901\CIVIL\DCN\MO100.11.DGN
 HECHTBR
 11-07-2013 16:25:59
 exp U.S. Services Inc. Chicago, IL
 BUILDINGS-EARTH & ENVIRONMENT-ENERGY
 INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY

FILE NAME = #FILEL#	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	MAINTENANCE OF TRAFFIC PRESTAGE STA. 152+00 (EX) TO STA. 164+00 (EX)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
exp U.S. Services Inc. Chicago, IL BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT SCALE = #SCALE#	DRAWN - JP	REVISED -			305	27R-2	MCHENRY	673	95	
PLOT DATE = #DATE#	CHECKED - KA	DATE - 11/01/13	REVISED -			CONTRACT NO. 62268					
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

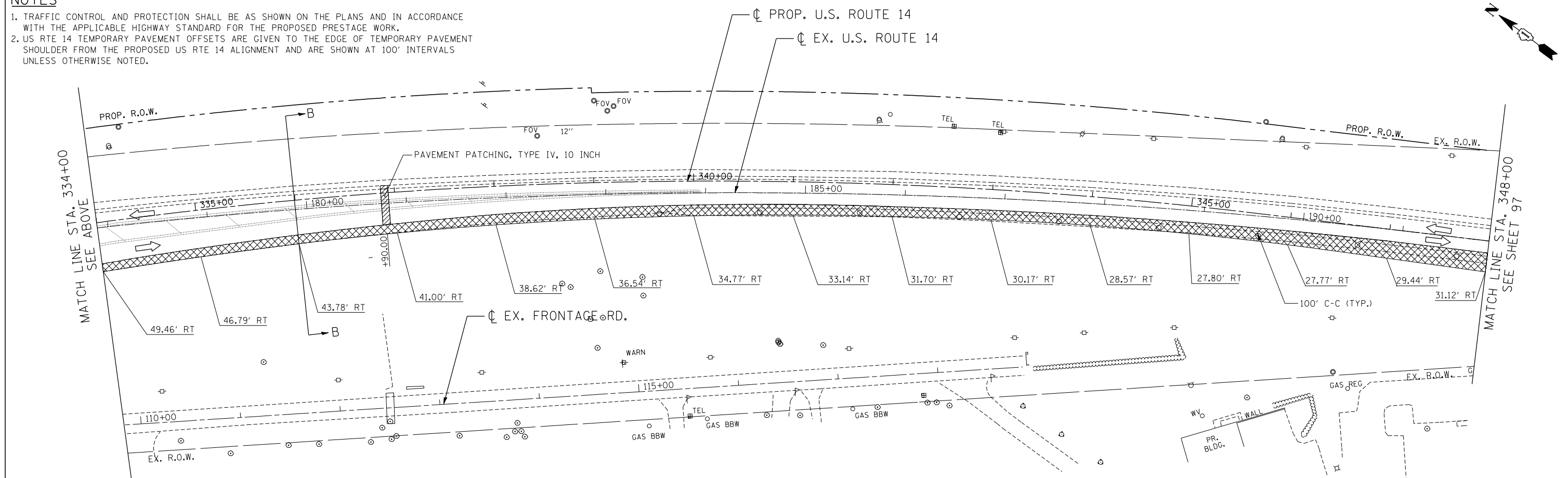


MAINTENANCE OF TRAFFIC LEGEND

	WORK AREA		EXISTING TRAFFIC DIRECTION		ARROW BOARD		PVT MK - LINE 4" WHITE		PVT MK - LINE 24" WHITE
	TEMPORARY PAVEMENT (CURRENT STAGE)		BARRICADE, TYPE III, W/ TYPE A FLASHING LIGHTS		DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN LIGHT		PVT MK - LINE 4" YELLOW		PVT MK - LINE 8" WHITE
	TEMPORARY PAVEMENT (PREVIOUS STAGES)		TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT		PROPOSED SIGN		TEMP PVT MK		WET REF TEM TAPE T3
	CONSTRUCTED PAVEMENT		TYPE B MONO-DIRECTIONAL FLASHING LIGHT		TYPE II BARRICADE OR DRUM W/ TYPE A FLASHING LIGHT		PVT MK - LINE 6" WHITE		PVT MK - LINE 12"
			TYPE A FLASHING LIGHT		PVT MK - LTRS & SYMB		PVT MK - LINE 12"		

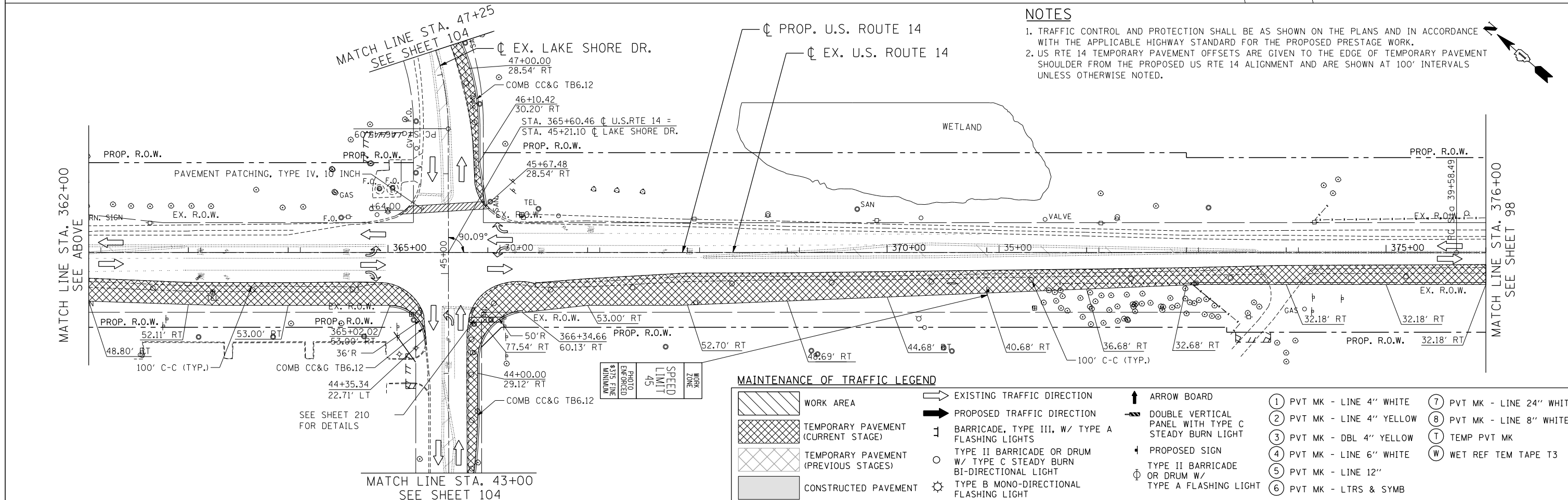
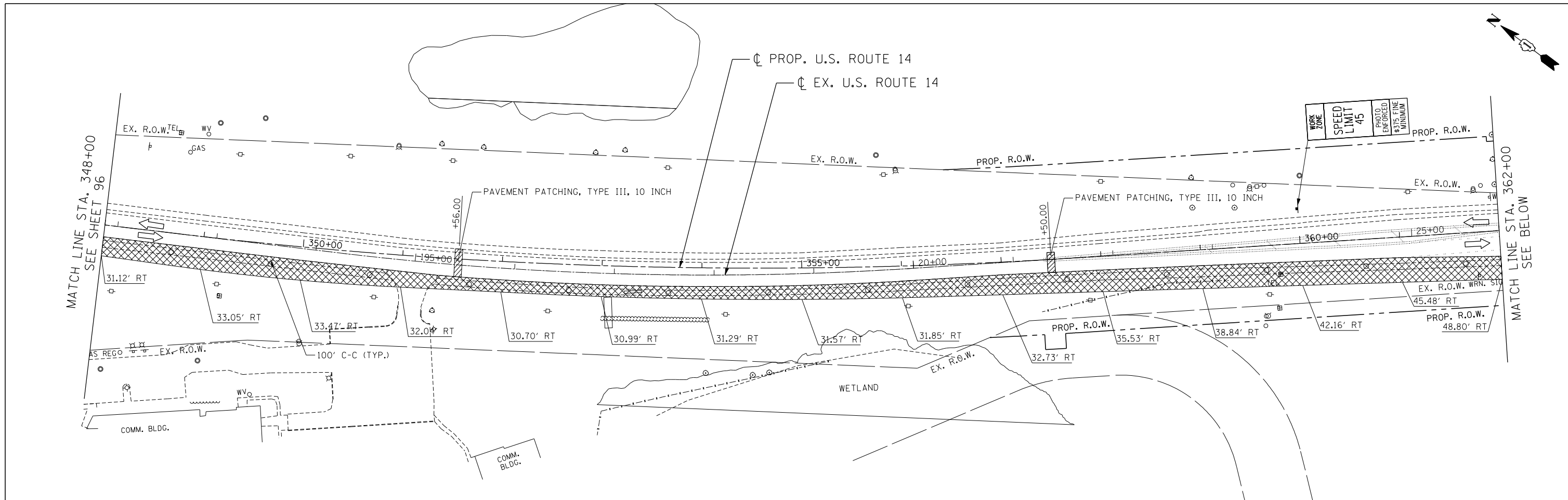
NOTES

1. TRAFFIC CONTROL AND PROTECTION SHALL BE AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH THE APPLICABLE HIGHWAY STANDARD FOR THE PROPOSED PRESTAGE WORK.
2. US RTE 14 TEMPORARY PAVEMENT OFFSETS ARE GIVEN TO THE EDGE OF TEMPORARY PAVEMENT SHOULDER FROM THE PROPOSED US RTE 14 ALIGNMENT AND ARE SHOWN AT 100' INTERVALS UNLESS OTHERWISE NOTED.



FILE NAME = #FILE#	USER NAME = HECHTBR	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14	MAINTENANCE OF TRAFFIC PRESTAGE STA. 164 + 00 (EX) TO STA. 348 + 00			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
exp U.S. Services Inc. Chicago, IL BUILDINGS-EARTH & ENVIRONMENT-ENERGY INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY	PLOT SCALE = #SCALE#	DRAWN - JP	REVISED -		SCALE: 1"=50'	SHEET NO.	OF SHEETS	STA.	TO STA.	305	27R-2	MCHENRY	673	96
	PLOT DATE = #DATE#	CHECKED - KA	REVISED -											
		DATE - 11/01/13	REVISED -											
										CONTRACT NO. 62268			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

I:\PROJECTS\11-07-2013_1626210\HECHTBR\11-07-2013_1626210.DGN
 I:\PROJECTS\11-07-2013_1626210\HECHTBR\11-07-2013_1626210.DGN
 I:\PROJECTS\11-07-2013_1626210\HECHTBR\11-07-2013_1626210.DGN



NOTES

1. TRAFFIC CONTROL AND PROTECTION SHALL BE AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH THE APPLICABLE HIGHWAY STANDARD FOR THE PROPOSED PRESTAGE WORK.
2. US RTE 14 TEMPORARY PAVEMENT OFFSETS ARE GIVEN TO THE EDGE OF TEMPORARY PAVEMENT SHOULDER FROM THE PROPOSED US RTE 14 ALIGNMENT AND ARE SHOWN AT 100' INTERVALS UNLESS OTHERWISE NOTED.

MAINTENANCE OF TRAFFIC LEGEND

WORK AREA	EXISTING TRAFFIC DIRECTION	ARROW BOARD	1 PVT MK - LINE 4" WHITE	7 PVT MK - LINE 24" WHITE
TEMPORARY PAVEMENT (CURRENT STAGE)	PROPOSED TRAFFIC DIRECTION	DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN LIGHT	2 PVT MK - LINE 4" YELLOW	8 PVT MK - LINE 8" WHITE
TEMPORARY PAVEMENT (PREVIOUS STAGES)	BARRICADE, TYPE III, W/ TYPE A FLASHING LIGHTS	PROPOSED SIGN	3 PVT MK - DBL 4" YELLOW	T TEMP PVT MK
CONSTRUCTED PAVEMENT	TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT	TYPE II BARRICADE OR DRUM W/ TYPE A FLASHING LIGHT	4 PVT MK - LINE 6" WHITE	W WET REF TEM TAPE T3
	TYPE B MONO-DIRECTIONAL FLASHING LIGHT		5 PVT MK - LINE 12"	W WET REF TEM TAPE T3
			6 PVT MK - LTRS & SYMB	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

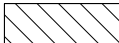






















MAINTENANCE OF TRAFFIC
PRESTAGE
STA. 348+00 TO STA. 376+00

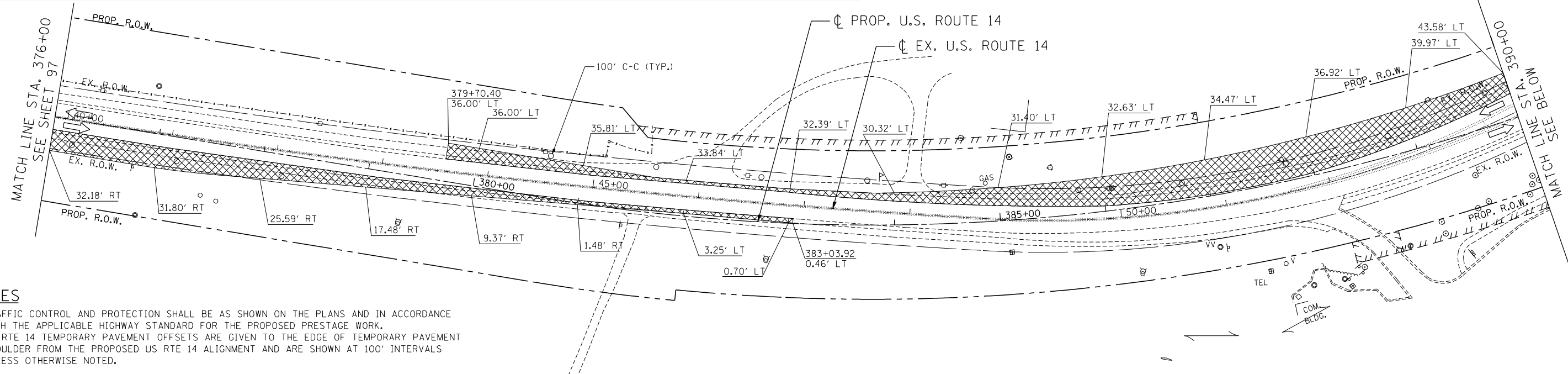
FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
#FILEL#		DRAWN - JP	REVISED -
		CHECKED - KA	REVISED -
		DATE - 11/01/13	REVISED -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	97
CONTRACT NO. 62268				
SCALE: 1"=50'				
SHEET NO. OF SHEETS STA. TO STA.				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE NAME = ...
 USER NAME = HECHTBR
 DESIGNED - ...
 DRAWN - JP
 CHECKED - KA
 DATE - 11/01/13

MAINTENANCE OF TRAFFIC LEGEND

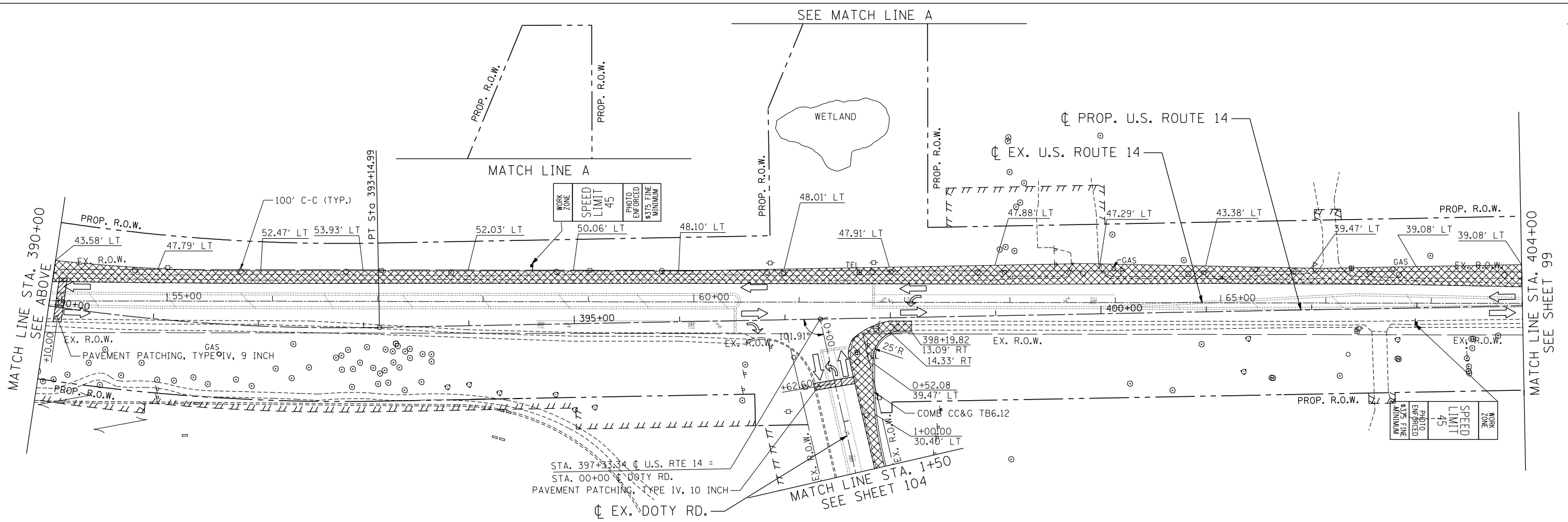
 WORK AREA	 EXISTING TRAFFIC DIRECTION	 ARROW BOARD	 ① PVT MK - LINE 4" WHITE	 ⑦ PVT MK - LINE 24" WHITE
 TEMPORARY PAVEMENT (CURRENT STAGE)	 PROPOSED TRAFFIC DIRECTION	 DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN LIGHT	 ② PVT MK - LINE 4" YELLOW	 ⑧ PVT MK - LINE 8" WHITE
 TEMPORARY PAVEMENT (PREVIOUS STAGES)	 BARRICADE, TYPE III, W/ TYPE A FLASHING LIGHTS	 PROPOSED SIGN	 ③ PVT MK - DBL 4" YELLOW	 ⑩ TEMP PVT MK
 CONSTRUCTED PAVEMENT	 TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT	 TYPE II BARRICADE OR DRUM W/ TYPE A FLASHING LIGHT	 ④ PVT MK - LINE 6" WHITE	 ⑪ WET REF TEM TAPE T3
	 TYPE B MONO-DIRECTIONAL FLASHING LIGHT		 ⑤ PVT MK - LINE 12"	
			 ⑥ PVT MK - LTRS & SYMB	



NOTES

1. TRAFFIC CONTROL AND PROTECTION SHALL BE AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH THE APPLICABLE HIGHWAY STANDARD FOR THE PROPOSED PRESTAGE WORK.
2. US RTE 14 TEMPORARY PAVEMENT OFFSETS ARE GIVEN TO THE EDGE OF TEMPORARY PAVEMENT SHOULDER FROM THE PROPOSED US RTE 14 ALIGNMENT AND ARE SHOWN AT 100' INTERVALS UNLESS OTHERWISE NOTED.

FILE NAME = ... USER NAME = HECHTBR ... DESIGNED - ... REVISED - ... DRAWN - JP ... CHECKED - KA ... DATE - 11/01/13 ...



**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
U.S. ROUTE 14**

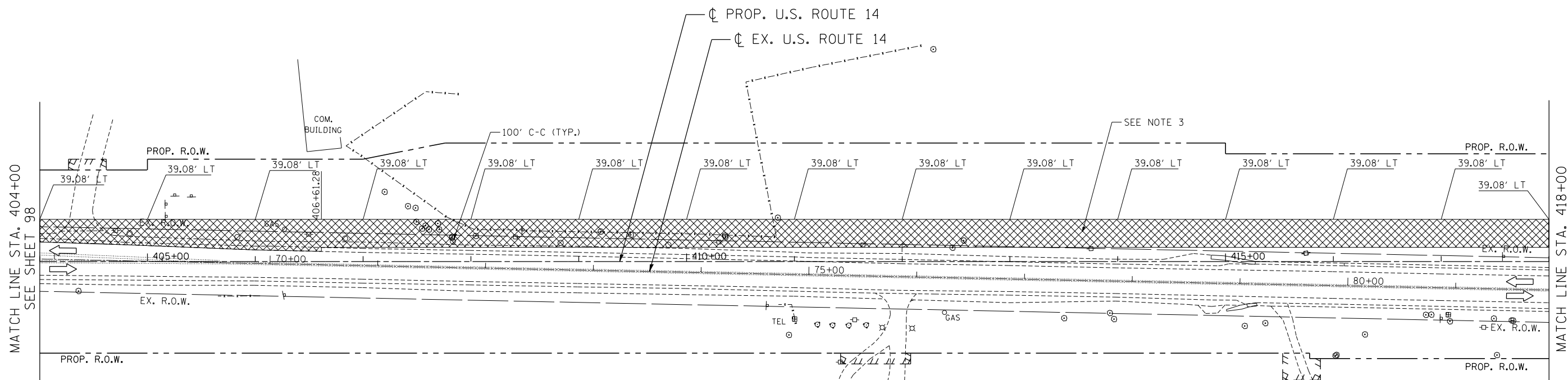
**MAINTENANCE OF TRAFFIC
PRESTAGE
STA. 376+00 TO STA. 404+00**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	98
CONTRACT NO. 62268				

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
FILEL		DRAWN - JP	REVISED -
exp U.S. Services Inc.	PLOT SCALE = *SCALE*	CHECKED - KA	REVISED -
BUILDING-EARTH & ENVIRONMENT-ENERGY	PLOT DATE = *DATE*	DATE - 11/01/13	REVISED -
INDUSTRIAL-INFRASTRUCTURE-SUSTAINABILITY			

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

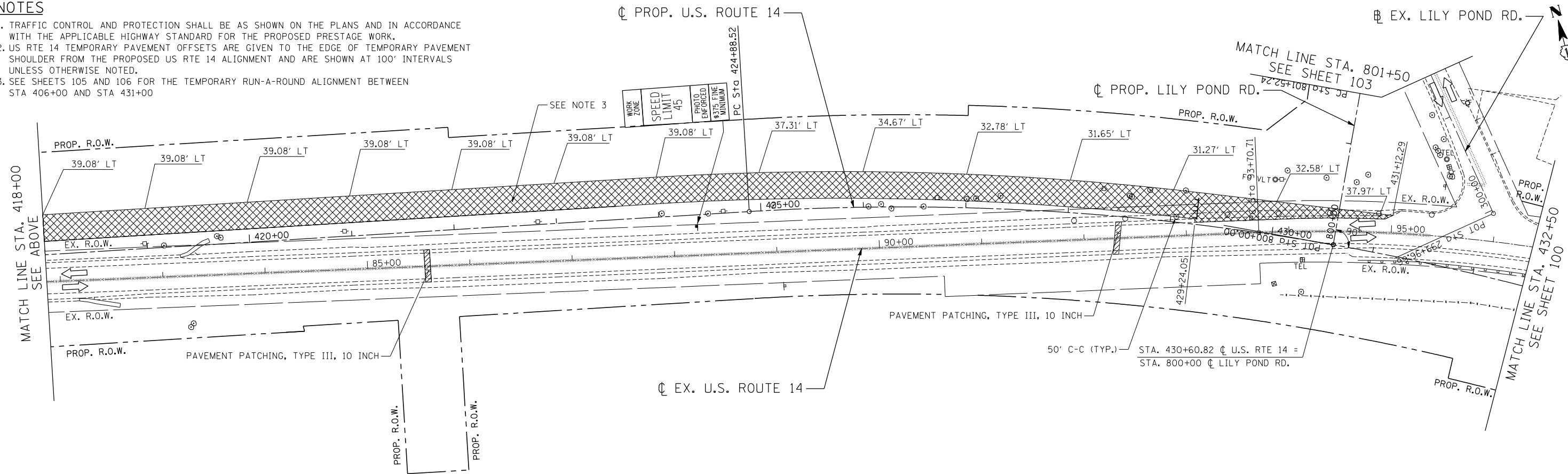


MAINTENANCE OF TRAFFIC LEGEND

	WORK AREA		EXISTING TRAFFIC DIRECTION		ARROW BOARD		PVT MK - LINE 4" WHITE		PVT MK - LINE 24" WHITE
	TEMPORARY PAVEMENT (CURRENT STAGE)		PROPOSED TRAFFIC DIRECTION		DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN LIGHT		PVT MK - LINE 4" YELLOW		PVT MK - LINE 8" WHITE
	TEMPORARY PAVEMENT (PREVIOUS STAGES)		BARRICADE, TYPE III, W/ TYPE A FLASHING LIGHTS		PROPOSED SIGN		PVT MK - DBL 4" YELLOW		TEMP PVT MK
	CONSTRUCTED PAVEMENT		TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT		TYPE II BARRICADE OR DRUM W/ TYPE A FLASHING LIGHT		PVT MK - LINE 6" WHITE		WET REF TEM TAPE T3
			TYPE B MONO-DIRECTIONAL FLASHING LIGHT		TYPE II BARRICADE OR DRUM W/ TYPE A FLASHING LIGHT		PVT MK - LINE 12"		PVT MK - LTRS & SYMB

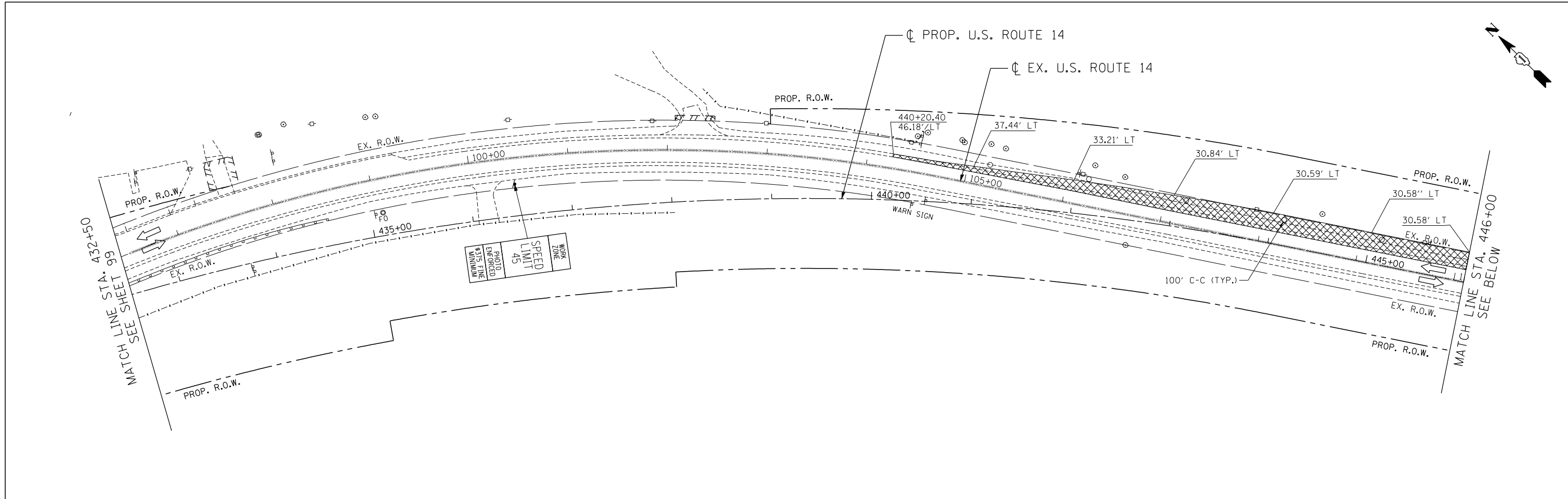
NOTES

1. TRAFFIC CONTROL AND PROTECTION SHALL BE AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH THE APPLICABLE HIGHWAY STANDARD FOR THE PROPOSED PRESTAGE WORK.
2. US RTE 14 TEMPORARY PAVEMENT OFFSETS ARE GIVEN TO THE EDGE OF TEMPORARY PAVEMENT SHOULDER FROM THE PROPOSED US RTE 14 ALIGNMENT AND ARE SHOWN AT 100' INTERVALS UNLESS OTHERWISE NOTED.
3. SEE SHEETS 105 AND 106 FOR THE TEMPORARY RUN-A-ROUND ALIGNMENT BETWEEN STA 406+00 AND STA 431+00



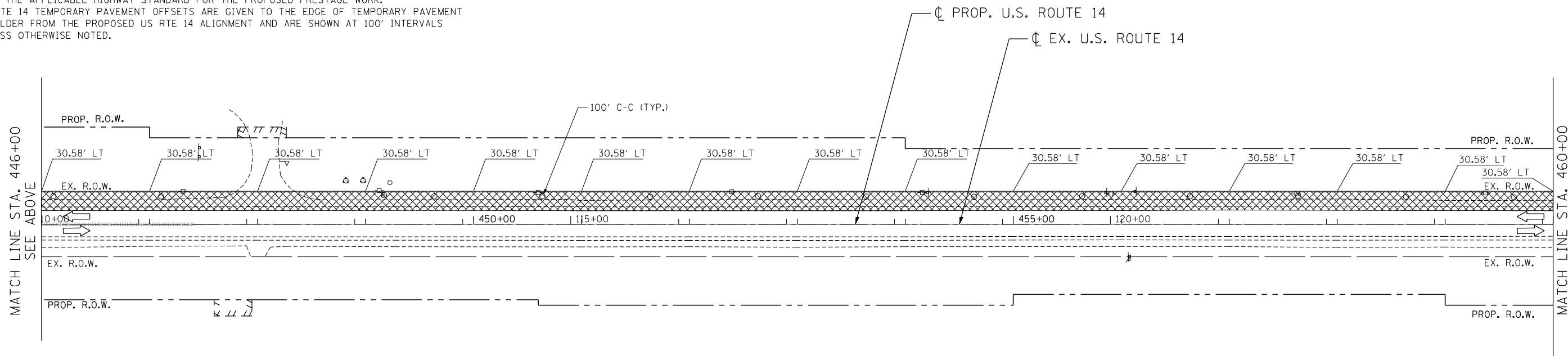
FILE NAME = ... USER NAME = HECHTBR ... DESIGNED - ... REVISED - ... DRAWN - JP ... CHECKED - KA ... DATE - 11/01/13 ... PLOT SCALE = *SCALE* ... PLOT DATE = *DATE*

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION U.S. ROUTE 14		MAINTENANCE OF TRAFFIC PRESTAGE STA. 404+00 TO STA. 432+50		F.A.P. RTE. 305	SECTION 27R-2	COUNTY MCHENRY	TOTAL SHEETS 673	SHEET NO. 99
SCALE: 1"=50'				SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 62268		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								



NOTES

1. TRAFFIC CONTROL AND PROTECTION SHALL BE AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH THE APPLICABLE HIGHWAY STANDARD FOR THE PROPOSED PRESTAGE WORK.
2. US RTE 14 TEMPORARY PAVEMENT OFFSETS ARE GIVEN TO THE EDGE OF TEMPORARY PAVEMENT SHOULDER FROM THE PROPOSED US RTE 14 ALIGNMENT AND ARE SHOWN AT 100' INTERVALS UNLESS OTHERWISE NOTED.



MAINTENANCE OF TRAFFIC LEGEND

WORK AREA	EXISTING TRAFFIC DIRECTION	ARROW BOARD	1 PVT MK - LINE 4" WHITE	7 PVT MK - LINE 24" WHITE
TEMPORARY PAVEMENT (CURRENT STAGE)	PROPOSED TRAFFIC DIRECTION	DOUBLE VERTICAL PANEL WITH TYPE C STEADY BURN LIGHT	2 PVT MK - LINE 4" YELLOW	8 PVT MK - LINE 8" WHITE
TEMPORARY PAVEMENT (PREVIOUS STAGES)	BARRICADE, TYPE III, W/ TYPE A FLASHING LIGHTS	PROPOSED SIGN	3 PVT MK - DBL 4" YELLOW	T TEMP PVT MK
CONSTRUCTED PAVEMENT	TYPE II BARRICADE OR DRUM W/ TYPE C STEADY BURN BI-DIRECTIONAL LIGHT	TYPE II BARRICADE OR DRUM W/ TYPE A FLASHING LIGHT	4 PVT MK - LINE 6" WHITE	W WET REF TEM TAPE T3
	TYPE B MONO-DIRECTIONAL FLASHING LIGHT		5 PVT MK - LINE 12"	6 PVT MK - LTRS & SYMB

FILE NAME =	USER NAME = HECHTBR	DESIGNED -	REVISED -
FILEL		DRAWN - JP	REVISED -
		CHECKED - KA	REVISED -
		DATE - 11/01/13	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 14

MAINTENANCE OF TRAFFIC
PRESTAGE
STA. 432 + 50 TO STA. 460 + 00

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	27R-2	MCHENRY	673	100
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62268	

11-07-2013 16:26:55 HECHTBR T:\DOCUMENT\19349901\CIVIL\DCN\M01008\KY.DGN
 11-07-2013 16:26:55 HECHTBR T:\DOCUMENT\19349901\CIVIL\DCN\M01008\KY.DGN