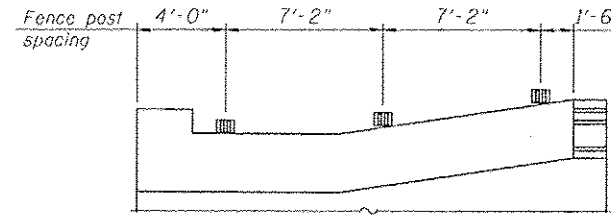
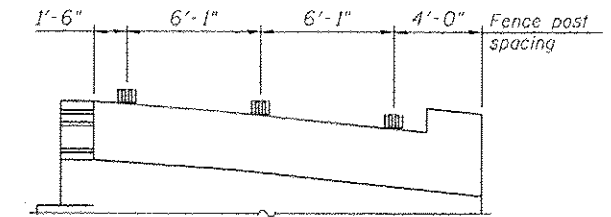


GENERAL NOTES:

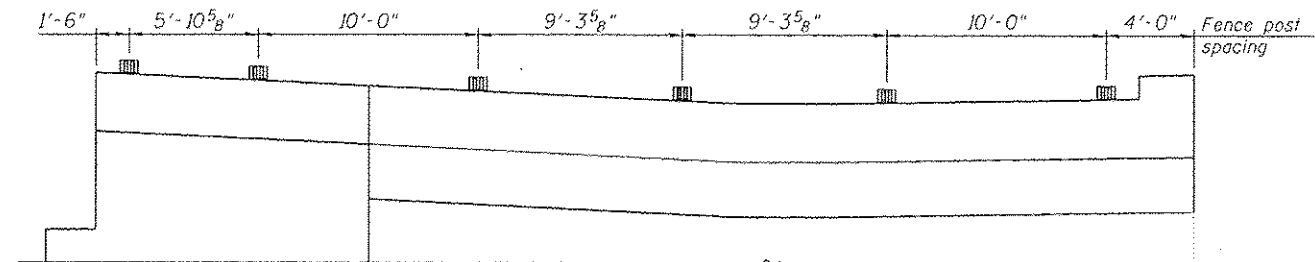
- Fasteners shall be AASHTO M164 Type 1, hot dip galvanized bolts. Bolts $\frac{7}{8}$ in. ϕ , holes $\frac{9}{16}$ in. ϕ , unless otherwise noted.
- Calculated weight of structural steel = 571,060 lbs. M270 Grade 50.
Calculated weight of structural steel = 38,860 lbs. M270 Grade 36.
- All structural steel shall be hot dip galvanized. Cost included with Furnishing & Erecting Structural Steel. See Special Provisions for Hot Dip Galvanizing of Structural Steel.
- No field welding is permitted except as specified in the contract documents.
- Reinforcement bars designated (E) shall be epoxy coated.
- Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- The existing structural steel coating contains lead. The Contractor shall take appropriate precaution to deal with the presence of lead on this project.
- If the Contractor elects to use cantilever forming brackets on the exterior beams or girders, the brackets shall be placed at the same locations as required for the hardwood blocks in Article 503.06(b) of the Standard Specifications. If additional cantilever forming brackets are required, hardwood blocking shall be wedged between the exterior and first interior beam at each of these additional bracket locations.
- Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of $\frac{1}{8}$ in. Adjustment shall be made either by grinding the surface or by shimming the bearings.
- Load carrying components designated "NTR" shall conform to the Impact Testing Requirement, Zone 2.
- Concrete Sealer shall be applied to the designated areas of the Piers, Abutments and Wingwalls.
- For Conduit Attached to Structure quantities and details, see Electrical Plans.
- The Contractor shall exercise extreme caution during construction to make certain that construction activities, live load surcharge and other loads applied to the structures will not have detrimental effects on the adjacent building foundations.
- Driving piles and temporary sheet piling is not allowed.
- For light pole support system, see Sheet S1-18 and Electrical and ITS Plans.
- Existing CTA subpier and foundation were most likely removed or partially removed when existing abutments and approach slab were constructed. Any existing foundation that is within the proposed excavation for the new bridge structure construction shall be included with FOUNDATION REMOVAL. See Special Provisions.
- The Contractor shall exercise extreme caution during construction to make certain that construction activities, live load surcharge, structure excavation, drilling shafts and other loads applied to the pier 1 will not have detrimental effects on the 10'x9' main drain. Any damage to the main drain during construction shall be repaired by the Contractor at his expense and no charge to the Department.
- Abandoned 5' Brick CTA Water Tunnel must be filled prior to the start of North Approach Bent construction. The tunnel must be filled so that north approach bent drilled shafts construction will not be impacted by tunnel. A number of the north approach bent drilled shafts will be placed through this tunnel. Drilling operations must account for the presence of debris, brick material, CLSM and bedding material in addition to soil and other expected materials to be encountered.
- The Contractor shall exercise extreme caution during construction to make certain that construction activities, live load surcharge, structure excavation, drilling shafts and other loads applied to the North Approach Bent and slab will not have detrimental effects on the existing 36" ϕ watermain at the southeast corner of the North Approach Slab. Any damage to the watermain during construction shall be repaired by the Contractor at his expense and no charge to the Department.
- For proposed watermain alignment, see Sheet No. 87.
- Slipforming of parapets is not allowed.
- For the City of Chicago conduits attached to structure and embedded in structure quantities and details, see Civil and Electrical plans.



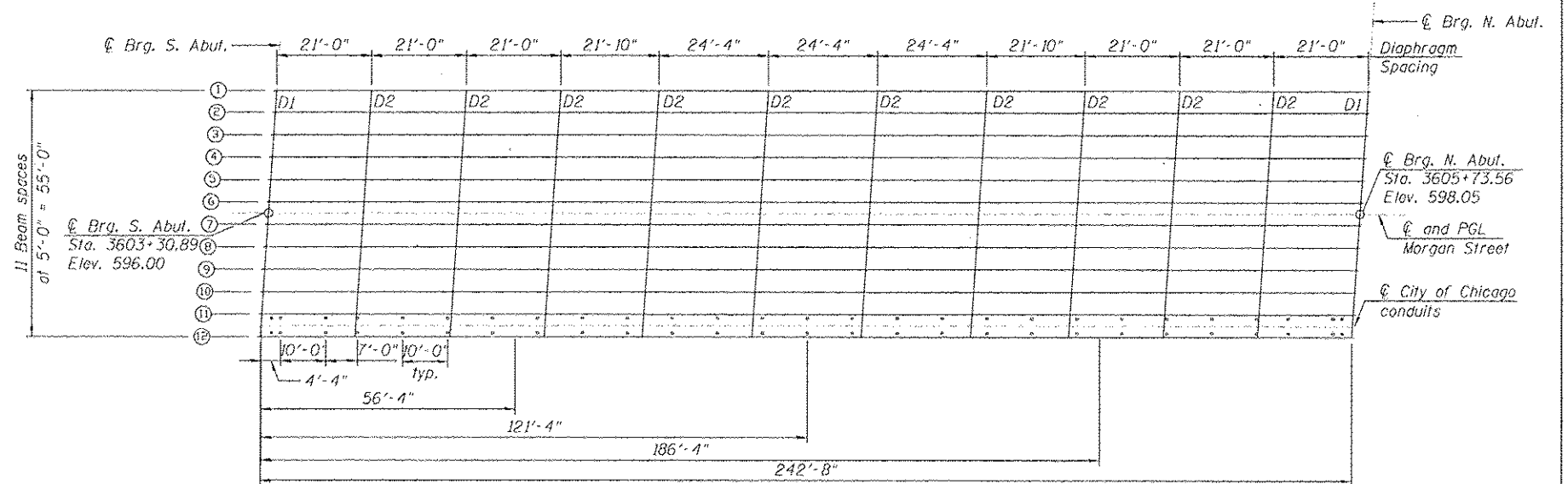
**WEST WINGWALL PARAPET RAILING
SPACING AT SOUTH ABUTMENT**
(Looking at Inside Face of Parapet)



**EAST WINGWALL PARAPET RAILING
SPACING AT SOUTH ABUTMENT**
(Looking at Inside Face of Parapet)



**WEST WINGWALL PARAPET RAILING
SPACING AT NORTH ABUTMENT**
(Looking at Inside Face of Parapet)



FILE PATH: C:\Users\willmardaus\Documents\Projects\The Des Plaines River Bridge\SP2\GenNotes.dwg

Entire sheet revised

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DRAWN - JJS, LAB	REVISION -
CHECKED - MAI, MI, JJS	REVISION -
DATE - 6/11/2013	REVISION -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES
STRUCTURE NO. 016-1709**

SCALE: SHEET S1-03 OF 51 SHEETS STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2013-007R	COUNTY COOK	TOTAL SHEETS 317	SHEET NO. 147
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				