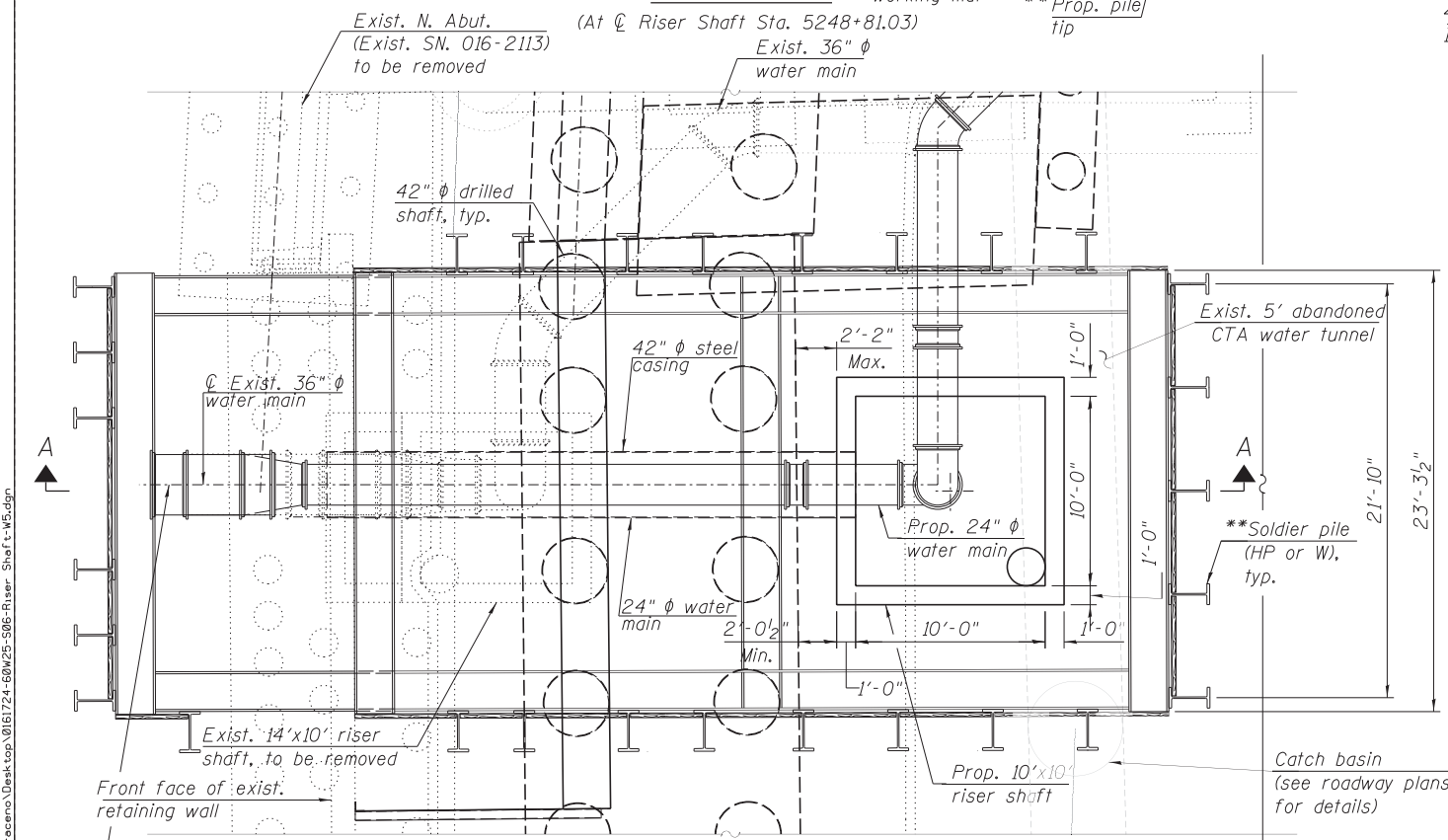


SECTION A-A



RISER SHAFT AND BRACED EXCAVATION PLAN
(Soldier pile encasement and corner bracing not shown for clarity)

NOTES:

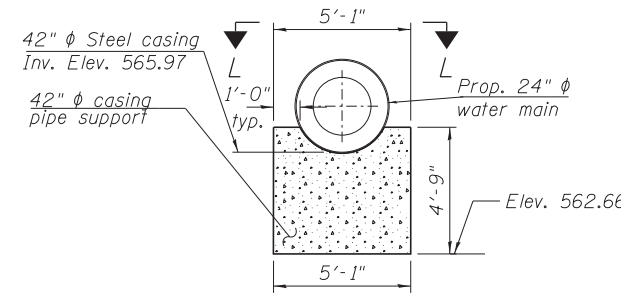
- The braced excavation support system for the riser shaft construction layout and details is a suggested option. The suggested construction sequence enables the Contractor to construct the braced excavation support system for existing riser shaft removal, existing water main connection removal and new riser shaft and new wall construction. The Contractor shall design the braced excavation consisting of drilled soldier piles, wood lagging and structural steel sections (struts and walers) to meet the City of Chicago Office of Underground coordination (OUC) building requirement. The design shall be performed and sealed by a Structural Engineer, licensed and registered in the State of Illinois, and shall be approved by the Engineer.
- The portion of the existing abandoned CTA water tunnel interfering with the new riser shaft construction shall be removed.
- If the 5' abandoned CTA water tunnel is encountered during drilling for the soldier piles for the braced excavation, concrete coring shall be used (if required).

* Cost included with "Pipe Underdrains for Structures, 6".

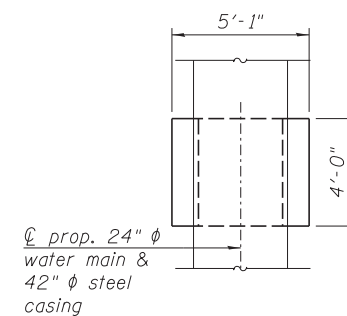
** Pile section, spacing, tip elevation shaft diameter, and limits to be determined during braced excavation design, to be performed by the contractor.

SUGGESTED CONSTRUCTION SEQUENCE

- Shut down the water main to allow for new construction.
- Construct braced excavation support system for existing riser shaft removal, existing water main extension, new riser shaft construction and new wall construction.
- Remove the existing riser shaft and existing water main interfering with the new construction.
- Assemble the 24" ϕ water main horizontal portion and install it into the 42" ϕ casing pipe (at Invert Elevation 565.97).
- Construct the concrete foundation shown in Section B-B and C-C.
- Install the assembled water main and casing, assembled in Item 4, in its final position to bear on the concrete foundation supports.
- Connect the new 24" ϕ water main of Item 4 to the existing 36" ϕ water main.
- Construct the riser shaft bottom slab and walls.
- Backfill the braced excavation up to the bottom of the new wall footing (at Elev. 570.24) around the new riser shaft with approved structural backfill.
- Install the braced excavation required for construction of Retaining Wall 5 (S.N. 016-1724) and the north abutment of Morgan Street Bridge (S.N. 016-1709).
- Remove the wood lagging between the piles interfering with new abutment and wall construction on the west and east walls of the riser shaft braced excavation to allow construction of Retaining Wall 5 and the north abutment of Morgan Street Bridge.
- Install the drilled shafts for Retaining Wall 5 (S.N. 016-1724).
- Construct the new retaining wall.
- Construct the north abutment.
- Re-install the removed timber lagging and backfill behind the north abutment and wall up to the bottom of the proposed sub base material.
- Backfill the braced excavation portion existing to the north of the new wall to Elevation 584.94 (1' below the bottom of the new water main running thru the top of the west wall of the riser shaft).
- Remove the existing retaining wall to the limits shown on the removal plans, cut braced excavation piles, walers and struts to the south of the proposed Retaining Wall 5, patch strut holes in new retaining wall and backfill in front of new wall as required.
- Excavate as required, outside the limits of the braced excavation on Morgan Street, for the installation of the connection to the 30" ϕ water main on Morgan Street and construct thrust restrainers.
- Install the remaining portion of the 24" ϕ water main inside the riser shaft and the connection portion to the 30" ϕ water main on Morgan Street.
- Construct the top slab of the riser shaft.
- Construct the manhole and Chicago standard frame and lid.
- Backfill to the final elevation to allow for the construction of a temporary pavement and subbase material (see civil plans for details).
- Cut off and remove the braced excavation support system to a depth which will not interfere with new construction, or a minimum of 2' below finished grade.
- Construct pavement and curb and gutter within limits of construction. See pavement removal and construction plans.



SECTION B-B



SECTION C-C

LEGEND

- Concrete Structures
- Concrete Removal

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DRAWN - WM	REVISED -
CHECKED - MAI, MI, LAB	REVISED -
DATE - 6/17/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SECTIONS AND DETAILS - WALL AT RISER SHAFT
STRUCTURE NO. 016-1724

SCALE: SHEET S2-06 OF 7 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	201
CONTRACT NO. 60W25			ILLINOIS FED. AID PROJECT	



BORING LOG 2113-B-01

wangeng@wangeng.com
1145 N Main Street
Lombard, IL 60148
Telephone: 630 953-9928
Fax: 630 953-9938

WEI Job No.: 1100-04-01

Client **AECOM - DRAFT -**
Project **Circle Interchange Reconstruction**
Location **Sections 16 and 17, T39N, R14E of 3rd PM**

Datum: NAVD 88
Elevation: 594.97 ft
North: 1898089.14 ft
East: 1169854.00 ft
Station: 3605+74.45
Offset: 08.76 RT

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
594.8	2.75-inch thick ASPHALT --PAVEMENT--														
593.8	11.25-inch thick CONCRETE --PAVEMENT--														
	Loose to very dense, brown SANDY LOAM to SAND, trace gravel														
	--FILL--														
		1	X	1	14 26 33	NP	31			11	X	11	0 1 1	0.25 B	28
		2	X	2	12 10 9	NP	18			12	X	12	1 1 1	0.08 B	28
		3	X	3	9 9 7	NP	25			13	X	13	0 1 2	0.18 B	28
		4	X	4	4 6 6	NP	28			14	X	14	0 1 2	0.33 B	26
		5	X	5	8 6 7	NP	36			15	X	15	0 1 1	0.16 B	25
		6	X	6	3 4 4	NP	43			16	X	16	1 1 1	0.57 B	26
579.6	Very soft to medium stiff, gray CLAY to SILTY CLAY									17	X	17	3 5 7	1.97 B	16
		7	X	7	0 1 1	0.33 B	23			18	X	18	6 9 14	5.82 B	13
		8	X	8	0 0 1	0.16 B	25			19	X	19	6 8 14	2.13 B	22
		9	X	9	0 0 0	0.08 B	26			20	X	20	7 13 15	NP	20
		10	X	10	0 1 1	0.08 B	26			21	X	21	19 32 41	> 4.50 P	12

GENERAL NOTES

Begin Drilling **02-18-2013** Complete Drilling **02-24-2013**
 Drilling Contractor **Wang Testing Services** Drill Rig **B-57 TMR**
 Driller **R&J** Logger **F.Bozga** Checked by **C. Marin**
 Drilling Method **2.25" SSA to 15', Mud Rotary 15' thereafter**

WATER LEVEL DATA

While Drilling ∇ **13.00 ft**
 At Completion of Drilling ∇ **MUD (10')**
 Time After Drilling **NA**
 Depth to Water ∇ **NA**

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.



BORING LOG 2113-B-01

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Fax: 630 953-9938

WEI Job No.: 1100-04-01

Client **AECOM - DRAFT -**
Project **Circle Interchange Reconstruction**
Location **Sections 16 and 17, T39N, R14E of 3rd PM**

Datum: NAVD 88
Elevation: 594.97 ft
North: 1898089.14 ft
East: 1169854.00 ft
Station: 3605+74.45
Offset: 08.76 RT

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
543.0	Stiff to hard, gray SILTY CLAY LOAM, trace gravel														
	--L _t (%)=28, P _t (%)=15-- --%Gravel=2.2-- --%Sand=16.9--55 --%Silt=54.8-- --%Clay=26.1-- --A-6 (8)--														
		17	X	17	3 5 7	1.97 B	16			22	X	22	14 25 37	> 4.50 P	16
		18	X	18	6 9 14	5.82 B	13			23	X	23	17 26 47	> 4.50 P	15
		19	X	19	6 8 14	2.13 B	22			24	X	24	40 50/4	NP	14
		20	X	20	7 13 15	NP	20			25	X	25	50/3	NP	14
528.2	Medium dense, gray SILTY LOAM, trace clay lenses														
	--%Gravel=1.1-- --%Sand=6.7-- --%Silt=77.2--70 --%Clay=15.0-- --A-4 (0)--														
		21	X	21	19 32 41	> 4.50 P	12								
523.2	Hard, gray SILTY CLAY LOAM, trace gravel														
		25	X	25	50/3	NP	14								
508.0	Very dense, gray SANDY LOAM														
		24	X	24	40 50/4	NP	14								
503.0	Very dense, gray GRAVELLY SAND, some dolostone fragments														
		25	X	25	50/3	NP	14								
499.0	--AUGER REFUSAL--														
	Boring terminated at 96.00 ft														

GENERAL NOTES

Begin Drilling **02-18-2013** Complete Drilling **02-24-2013**
 Drilling Contractor **Wang Testing Services** Drill Rig **B-57 TMR**
 Driller **R&J** Logger **F.Bozga** Checked by **C. Marin**
 Drilling Method **2.25" SSA to 15', Mud Rotary 15' thereafter**

WATER LEVEL DATA

While Drilling ∇ **13.00 ft**
 At Completion of Drilling ∇ **MUD (10')**
 Time After Drilling **NA**
 Depth to Water ∇ **NA**

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

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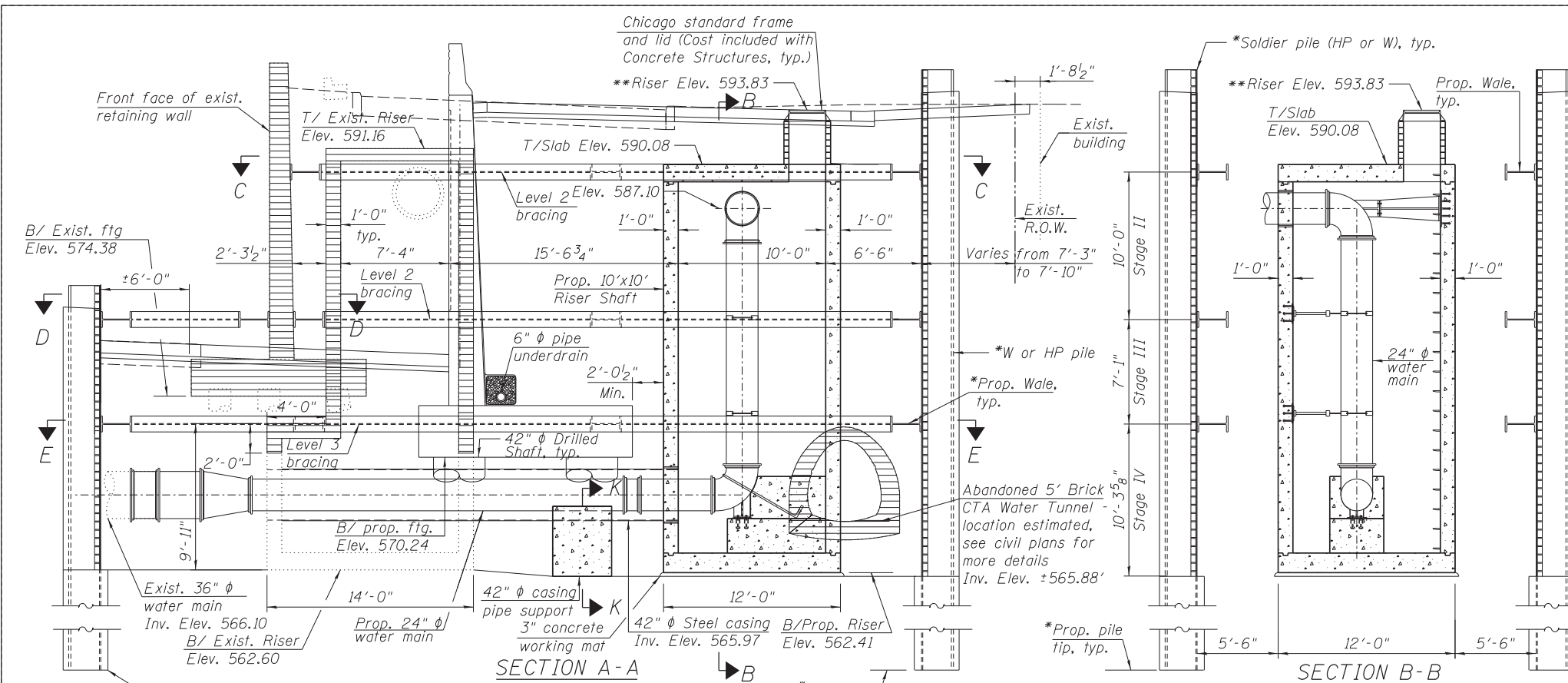
DESIGNED - JJS	REVISED -
DRAWN - JJS	REVISED -
CHECKED - MI, MAF	REVISED -
DATE - 6/17/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BORING LOGS
STRUCTURE NO. 016-1724

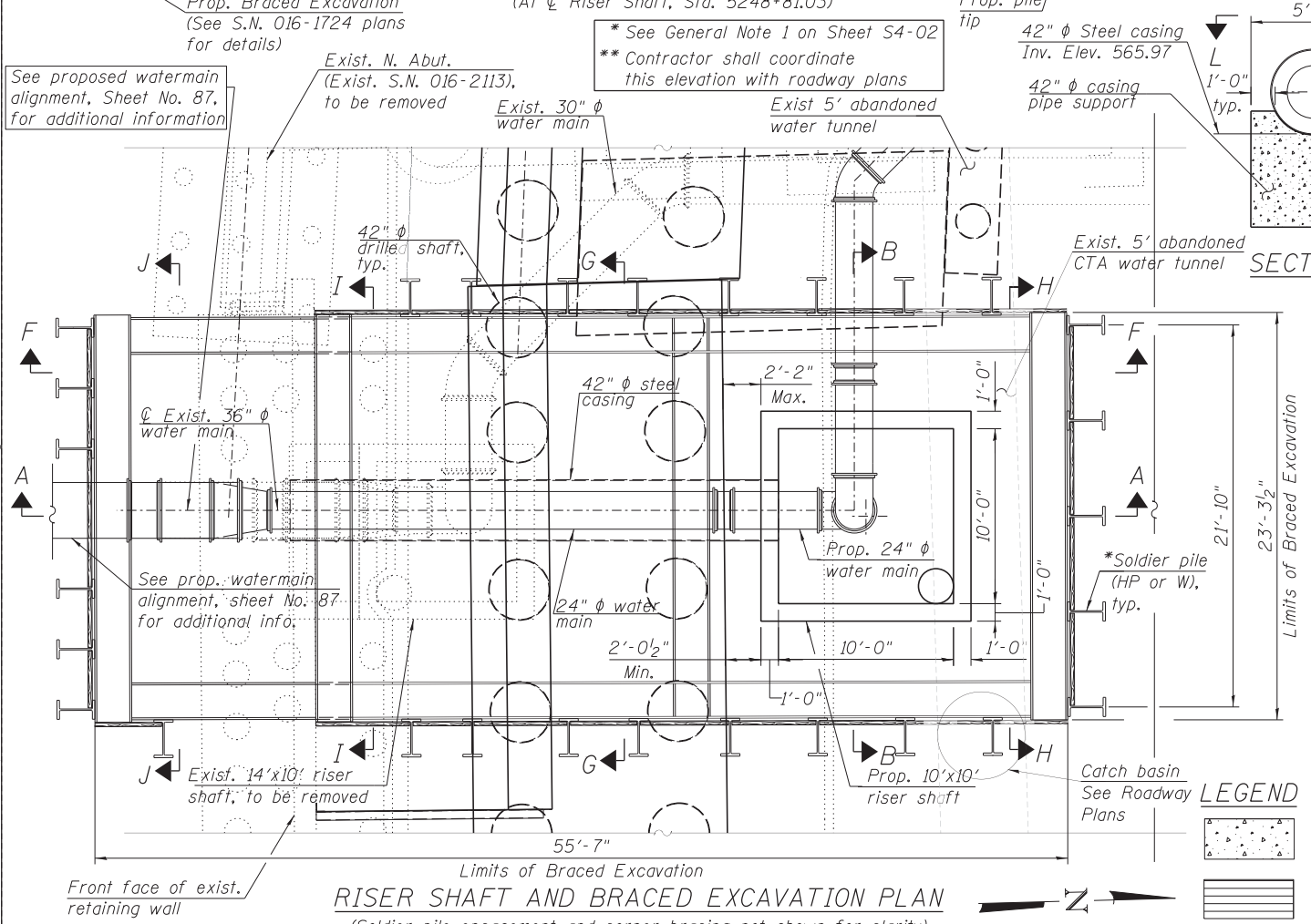
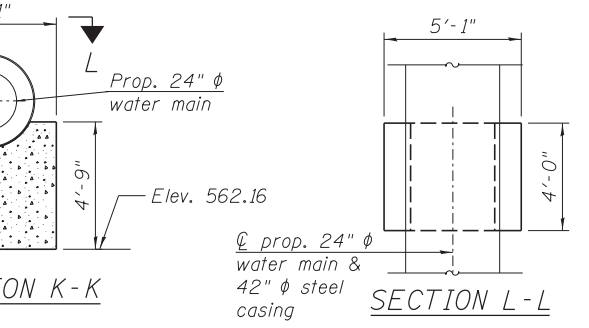
F.A.I. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	202
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET S2-07 OF 7 SHEETS STA. TO STA.



SUGGESTED CONSTRUCTION SEQUENCE

1. Shut down the water main to allow for new construction.
2. Construct braced excavation support system for existing riser shaft removal, existing water main extension, new riser shaft construction and new wall construction.
3. Remove the existing riser shaft and existing water main interfering with the new construction.
4. Assemble the 24" ϕ water main horizontal portion and install it into the 42" ϕ casing pipe. (At Invert Elevation 565.97).
5. Construct the concrete foundation shown in Section K-K and L-L.
6. Install the assembled water main and casing, assembled in Item 4, in its final position to bear on the concrete foundation supports.
7. Connect the new 24" ϕ water main of Item 4 to the existing 36" ϕ water main.
8. Construct the riser shaft bottom slab and walls.
9. Backfill the braced excavation up to the bottom of the new wall footing (at Elev. 570.24) around the new riser shaft with approved structural backfill.
10. Install the braced excavation required for construction of Retaining Wall 5 (S.N. 016-1724) and the north abutment of Morgan Street Bridge (S.N. 016-1709).
11. Remove the wood lagging between the piles interfering with new abutment and wall construction on the west and east walls of the riser shaft braced excavation to allow construction of Retaining Wall 5 and the north abutment of Morgan Street Bridge.
12. Install the drilled shafts for Retaining Wall 5 (S.N. 016-1724).
13. Construct the new retaining wall.
14. Construct the north abutment.
15. Re-install the removed timber lagging and backfill behind the north abutment and wall up to the bottom of the proposed sub-base material.
16. Backfill the braced excavation portion existing to the north of the new wall to Elevation 584.94 (1' below the bottom of the new water main running thru the top of the west wall of the riser shaft).
17. Remove the existing retaining wall to the limits shown on the removal plans, cut braced excavation piles, walers and struts to the south of the proposed Retaining Wall 5, patch strut holes in new retaining wall and backfill in front of new wall as required.
18. Excavate as required, outside the limits of the braced excavation on Morgan Street, for the installation of the connection to the 30" ϕ water main on Morgan Street and construct thrust restrainers.
19. Install the remaining portion of the 24" ϕ water main inside the riser shaft and the connection portion to the 30" ϕ water main on Morgan Street.
20. Construct the top slab of the riser shaft.
21. Construct the manhole and Chicago standard frame and lid.
22. Backfill to the final elevation to allow for the construction of a temporary pavement and subbase material. (See civil plans for details).
23. Cut off and remove the braced excavation support system to a depth which will not interfere with new construction, or a minimum of 2' below finished grade.
24. Construct pavement and curb and gutter within limits of construction. See pavement removal and construction plans.



NOTES

1. For Sections C-C, D-D and E-E, See Sheet S4-06.
2. For Sections F-F, G-G, H-H, I-I, and J-J, See sheet S4-07.
3. For General Notes, See sheet S4-02.
4. For Index of Sheets, See sheet S4-02.

DESIGN STRESSES

FIELD UNITS
 $f'_c = 3,500$ psi (Seal Coat Concrete)
 $f_y = 36,000$ psi (Soldier Piles)
 $f_y = 36,000$ psi (Structural Steel)

LOADING

Equivalent Fluid Soil Pressure = 40 pcf
 2 ft Live Load Surcharge

DESIGN SPECIFICATIONS

2012 AASHTO LRFD Bridge Design Specifications, 6th Edition

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Trench Backfill	CU YD	874
Concrete Removal	CU YD	72.9
Concrete Structures	CU YD	61.5
Reinforcement Bars, Epoxy Coated	POUND	16,970
Drilling and Setting Soldier Piles (in Soil)	CU FT	1,358
Braced Excavation	CU YD	1,221
Furnishing Soldier Piles (HP Section)	FOOT	212

LEGEND

- Concrete Structures
- Concrete Removal



Signed Moussa A. Issa
 Moussa A. Issa, HBM Il. Lic. No. 081-005738
 Expires 11-30-2014
 Date 6/17/2013 For Sheets S4-01 Thru S4-08

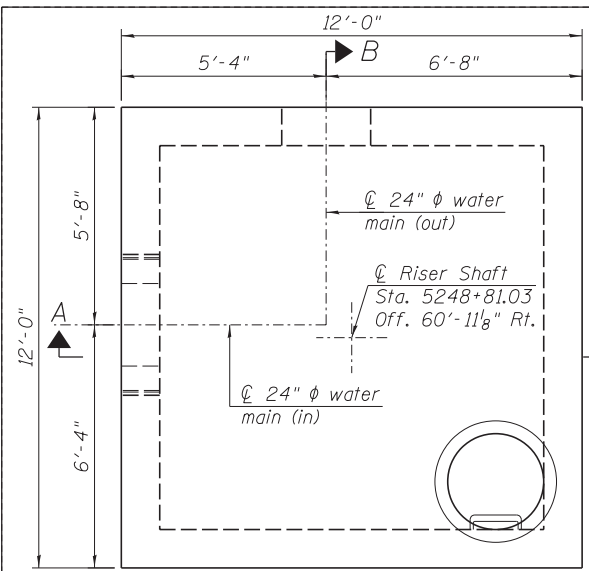
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 RESEARCH & TESTING

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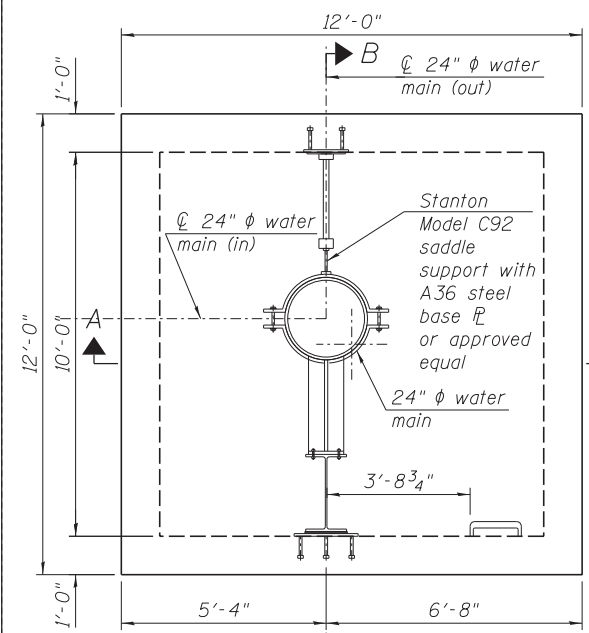
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND SECTIONS
RISER SHAFT AND BRACED EXCAVATION

F.A.I. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	225
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

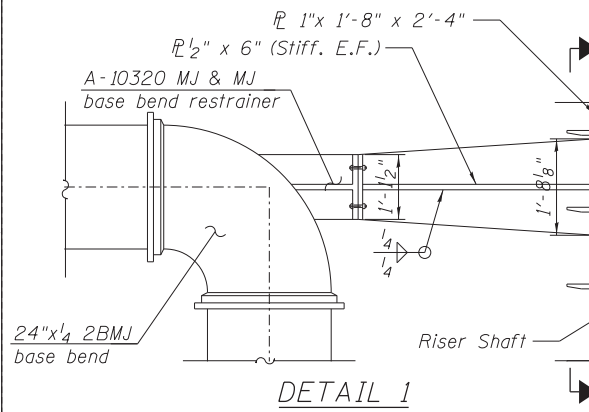


RISER PLAN



SECTIONAL PLAN

(Plan view of lateral pipe supports)



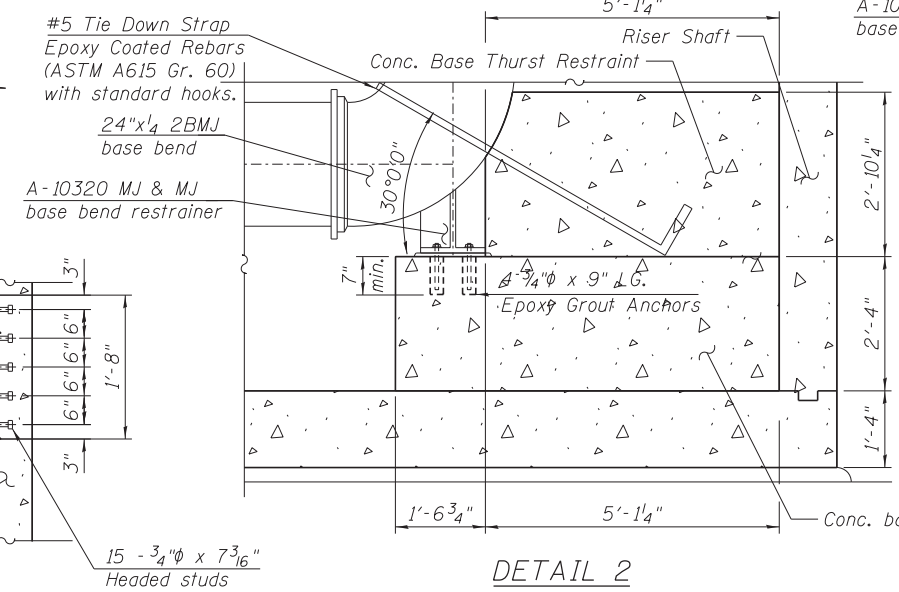
DETAIL 1

GENERAL NOTES:

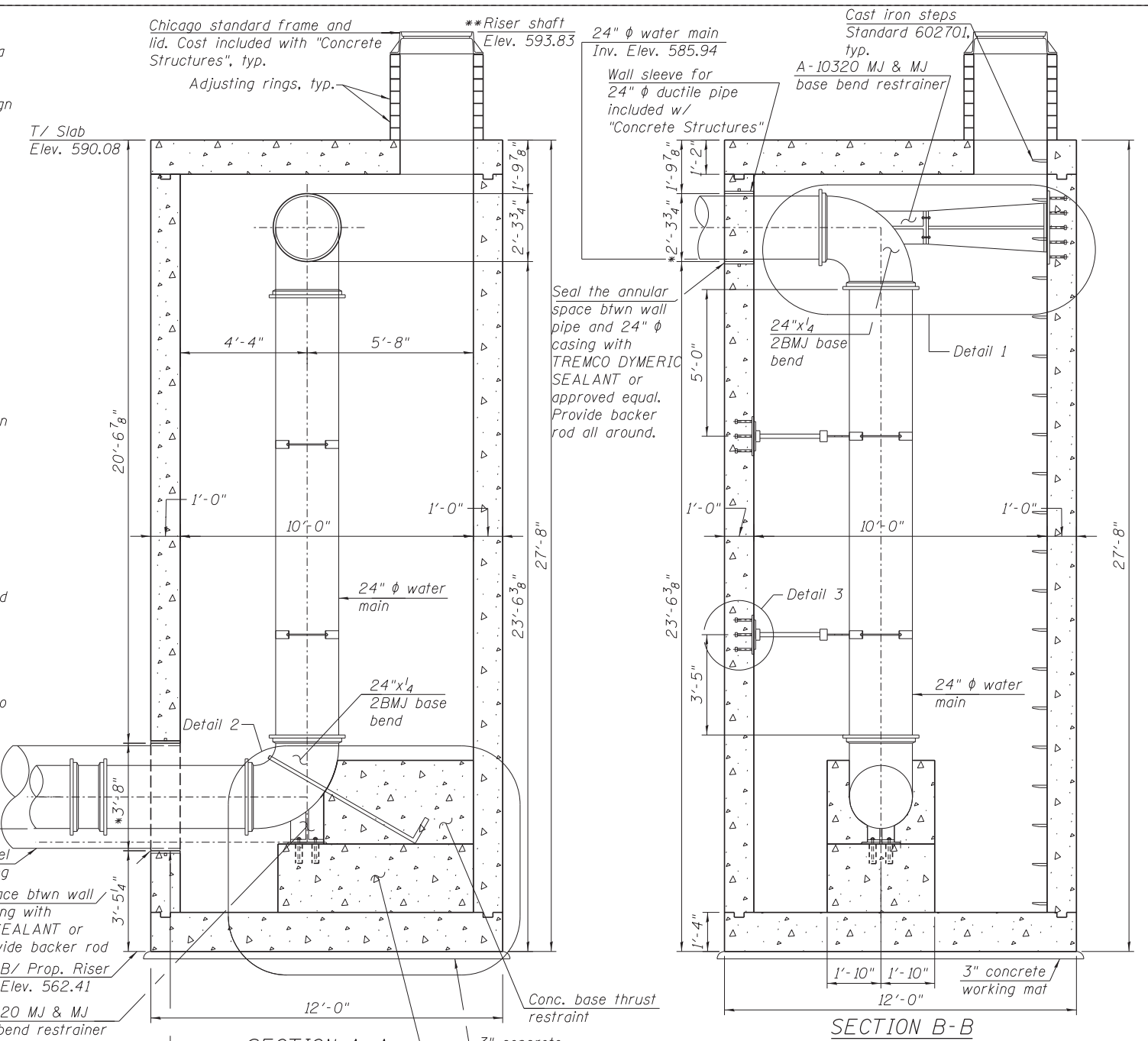
1. The braced excavation support system for the riser shaft construction layout and details is a suggested option. The suggested construction sequence enables the Contractor to construct the braced excavation support system for existing riser shaft removal, existing water main connection removal and new riser shaft and new wall construction. The Contractor shall design the braced excavation consisting of drilled soldier piles, wood lagging and structural steel sections (struts and walers) to meet the City of Chicago Office of Underground Coordination (OUC) for maximum allowed deflection of 1/4". The design shall be performed and sealed by a Structural Engineer, licensed and registered for the State of Illinois, and shall be approved by the Engineer.
2. The portion of the existing abandoned CTA water tunnel interfering with the new riser shaft construction shall be removed.
3. If the 5' abandoned CTA water tunnel is encountered during drilling for the soldier piles for the braced excavation, concrete coring shall be used (if required).
4. For water main location, layout and details, see the water main design plan sheets.
5. The proposed retaining wall foundation locations may require adjustment based upon the field location of the water main and shall be determined by the Engineer.
6. The proposed riser shaft location shall be adjusted as necessary to align with the water main location.
7. Existing CTA foundations were most likely removed or partially removed when the existing retaining wall was originally constructed. Any existing foundation that is within the proposed excavation for the new retaining wall and riser shaft construction shall be paid for as FOUNDATION REMOVAL.
8. Abandoned 5' Brick CTA Water Tunnel to be bulkheaded to the outside of the area of riser shaft construction (see civil sheets). In the area of the riser shaft construction, it will be necessary to remove any brick tunnel remnants. The removal of the existing tunnel is included as part of Braced Excavation as necessary, with proper disposal.
9. Abandoned 5' Brick CTA Water Tunnel must be filled prior to the start of wall construction. The tunnel must be filled so that foundation construction will not be impacted by tunnel. A number of the braced excavation piles may be placed through this tunnel. Drilling operations must account for presence of debris, brick material, CLSM and bedding material in addition to soil and other expected materials to be encountered.

INDEX OF SHEETS

- S4-01 General Plan and Sections
- S4-02 Riser Shaft Sections and Details (Sheet 1 of 3)
- S4-03 Riser Shaft Sections and Details (Sheet 2 of 3)
- S4-04 Riser Shaft Sections and Details (Sheet 3 of 3)
- S4-05 Permanent Thrust Restrainers Plan, Sections and Details
- S4-06 Suggested Braced Excav. Sections and Details (Sheet 1 of 2)
- S4-07 Suggested Braced Excav. Sections and Details (Sheet 2 of 2)
- S4-08 Boring Log

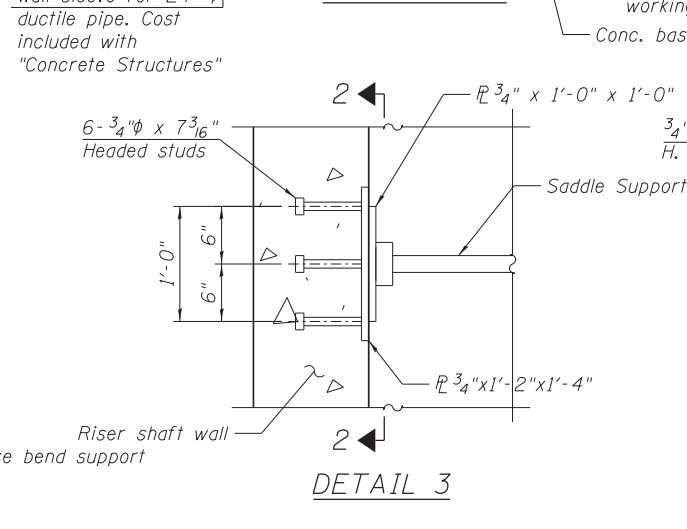


DETAIL 2

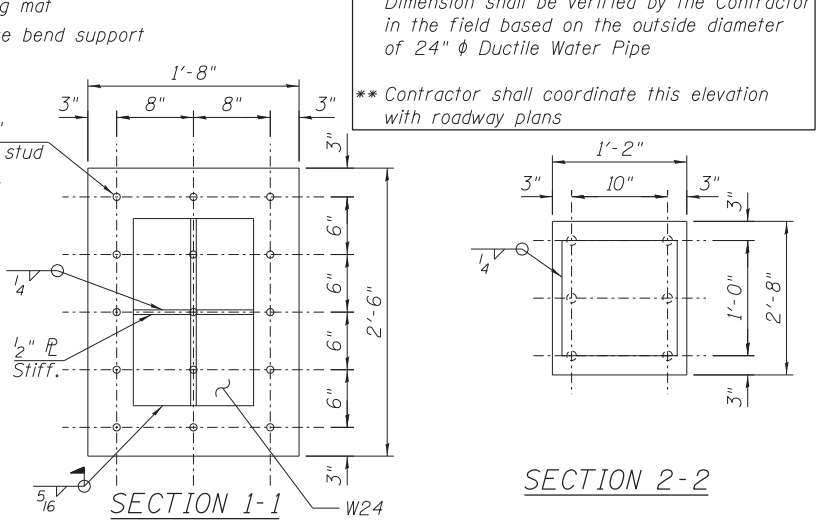


SECTION A-A

SECTION B-B



DETAIL 3



SECTION 1-1

SECTION 2-2

* Dimension shall be verified by the Contractor in the field based on the outside diameter of 24" ϕ Ductile Water Pipe
 ** Contractor shall coordinate this elevation with roadway plans

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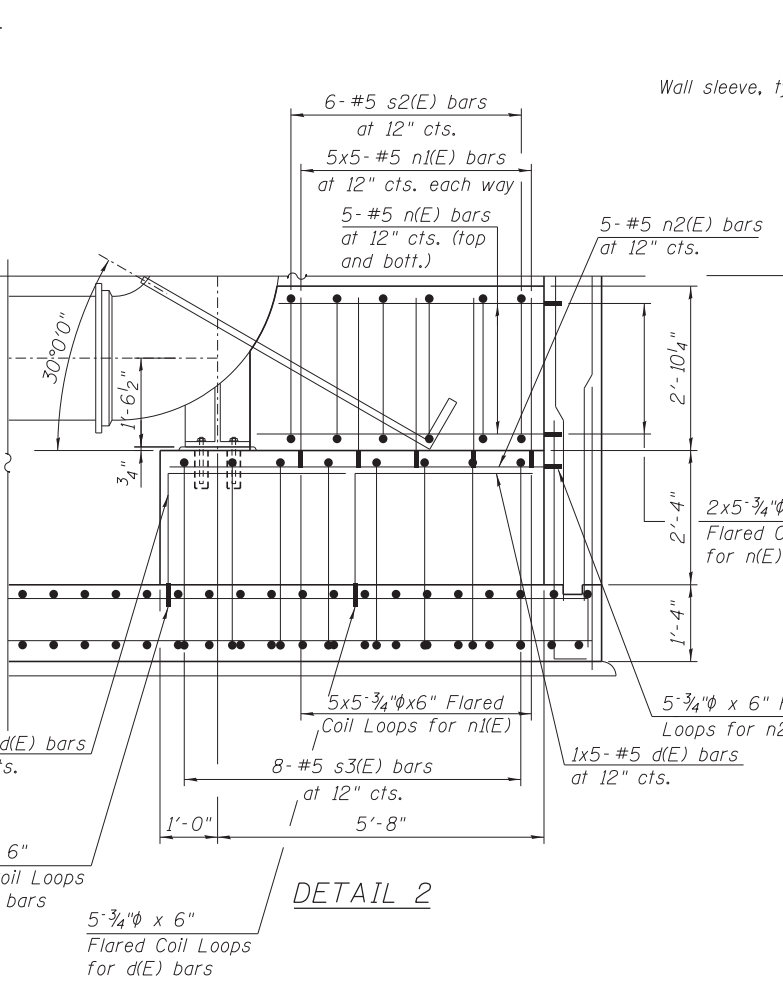
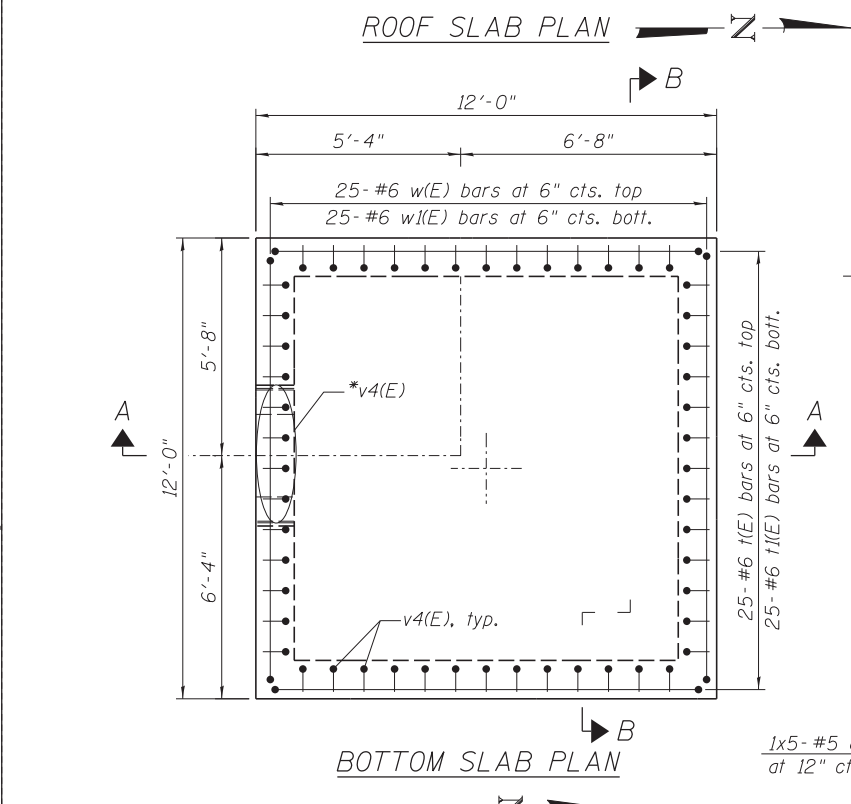
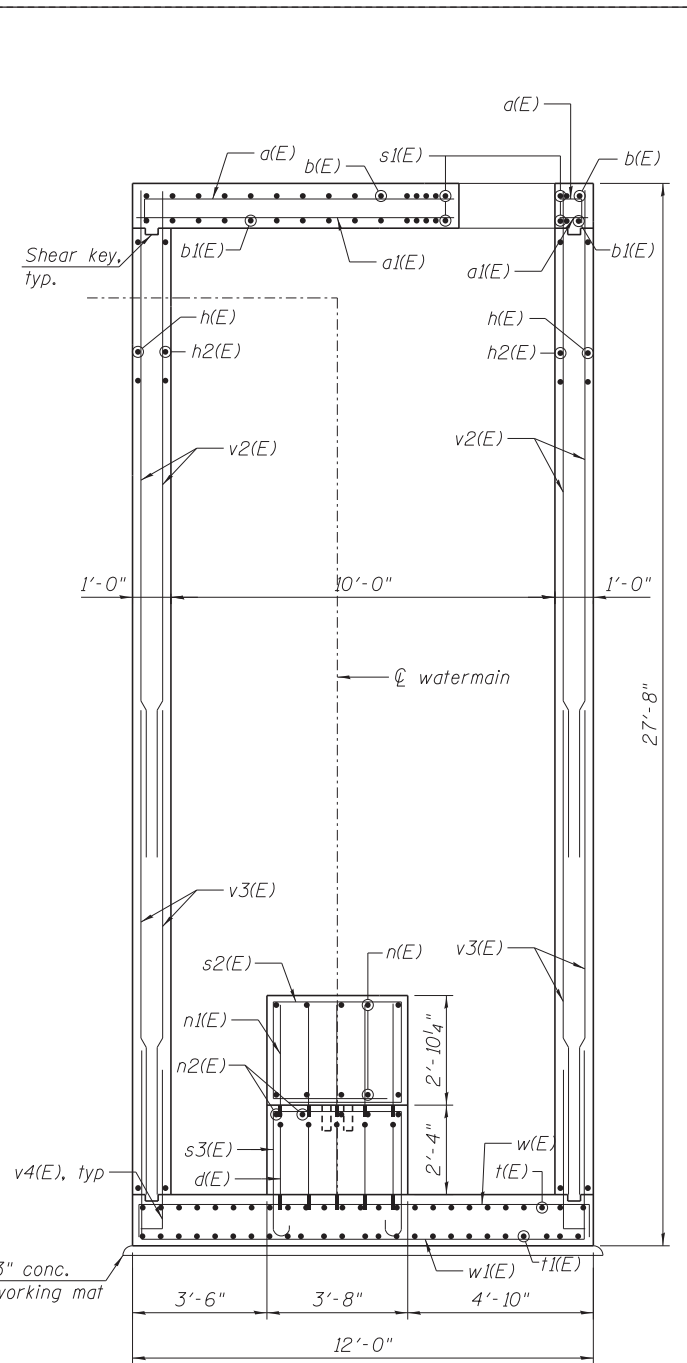
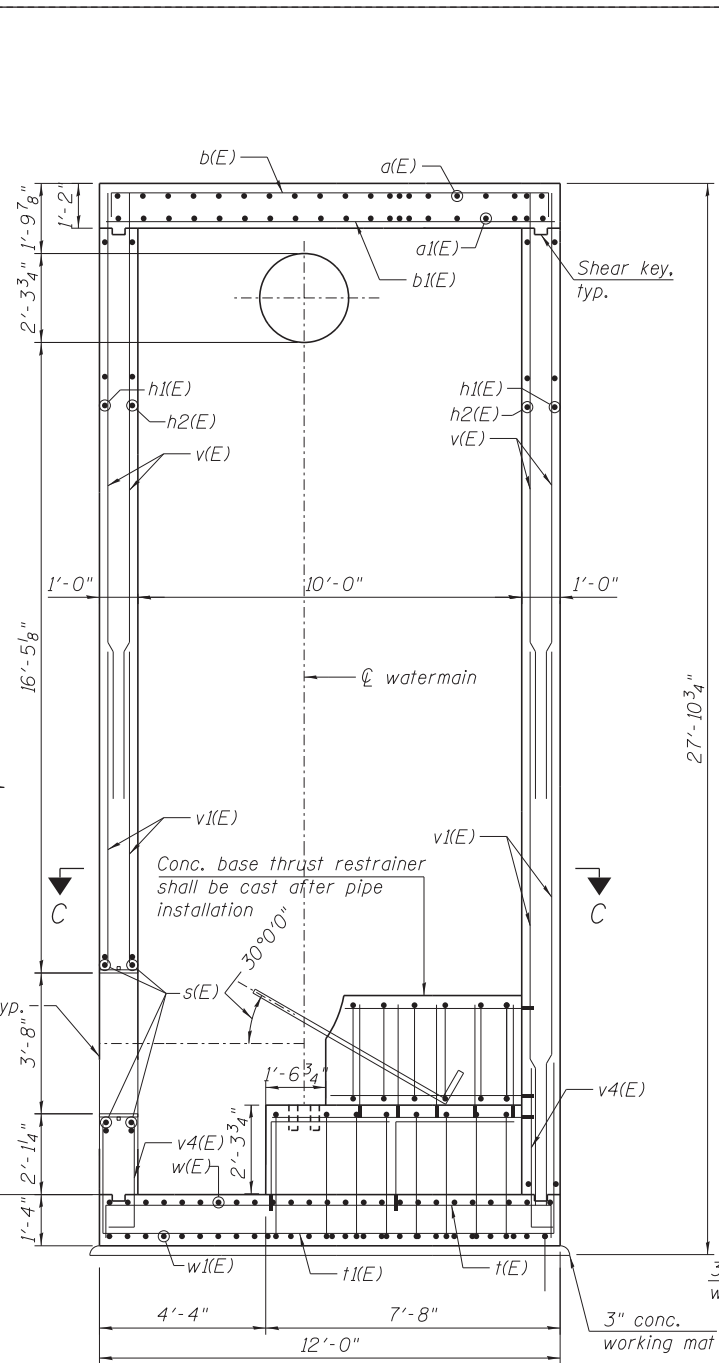
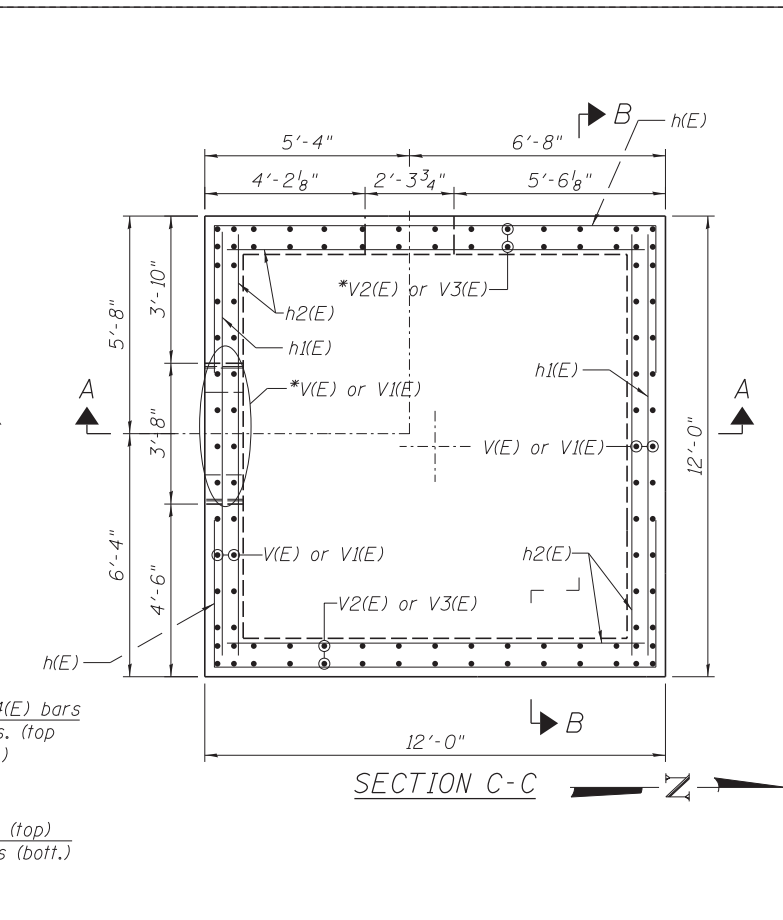
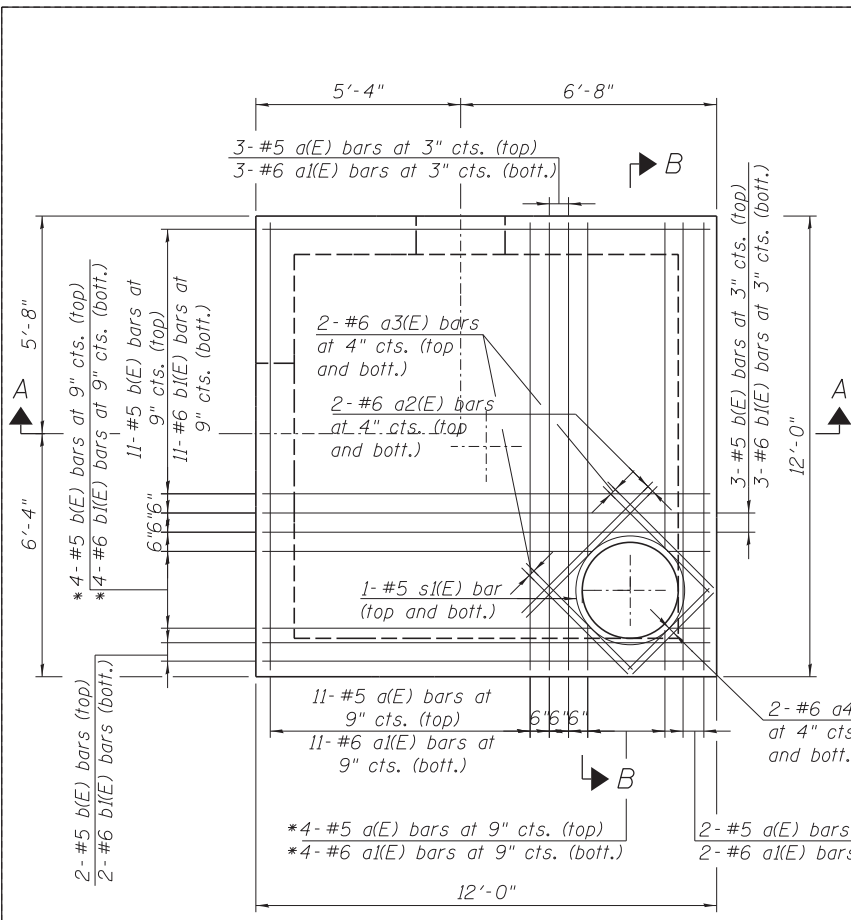
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

RISER SHAFT SECTIONS AND DETAILS (SHEET 1 OF 3)
 RISER SHAFT AND BRACED EXCAVATION

F.A.I. RE. 90/94/290	SECTION 2013-007R	COUNTY COOK	TOTAL SHEETS 317	SHEET NO. 226
CONTRACT NO. 60W25			ILLINOIS FED. AID PROJECT	

SCALE: SHEET S4-02 OF 8 SHEETS STA. TO STA.



SECTION A-A

SECTION B-B

NOTES:

1. For General Notes, see Sheet S4-02.
2. For Bill of Material, bar diagrams and minimum bar laps, see Sheet S4-04.

*Order bars full length and cut in field to fit

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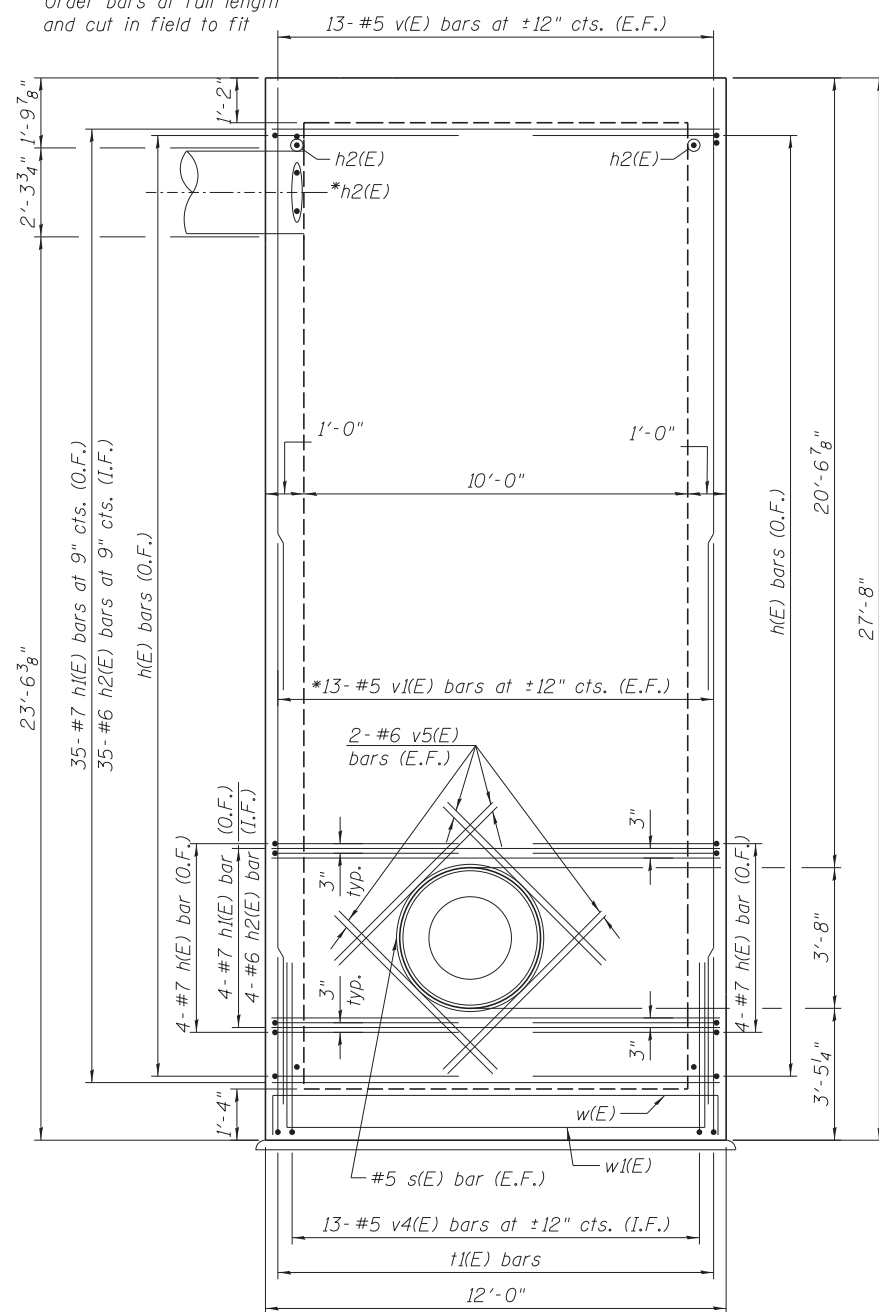
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**RISER SHAFT SECTIONS AND DETAILS (SHEET 2 OF 3)
RISER SHAFT AND BRACED EXCAVATION**

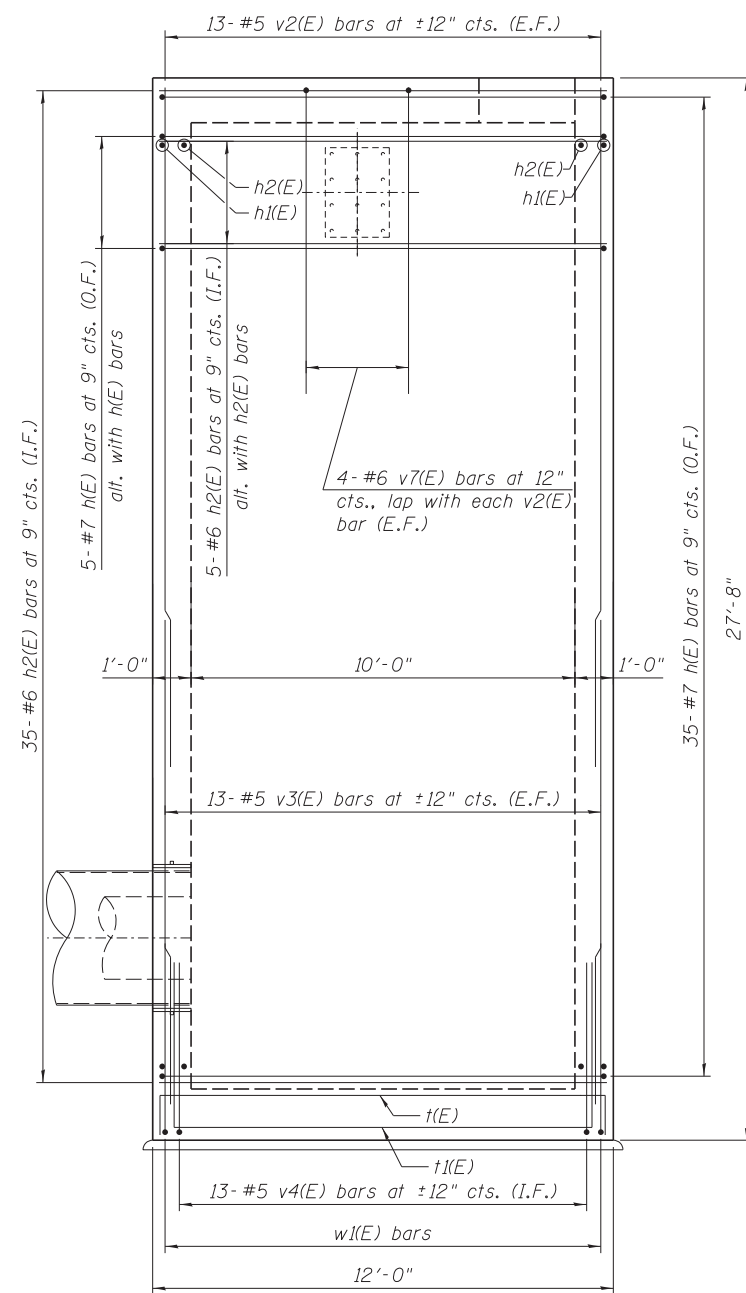
SCALE: SHEET S4-03 OF 8 SHEETS STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2013-007R	COUNTY COOK	TOTAL SHEETS 317	SHEET NO. 227
CONTRACT NO. 60W25			ILLINOIS FED. AID PROJECT	

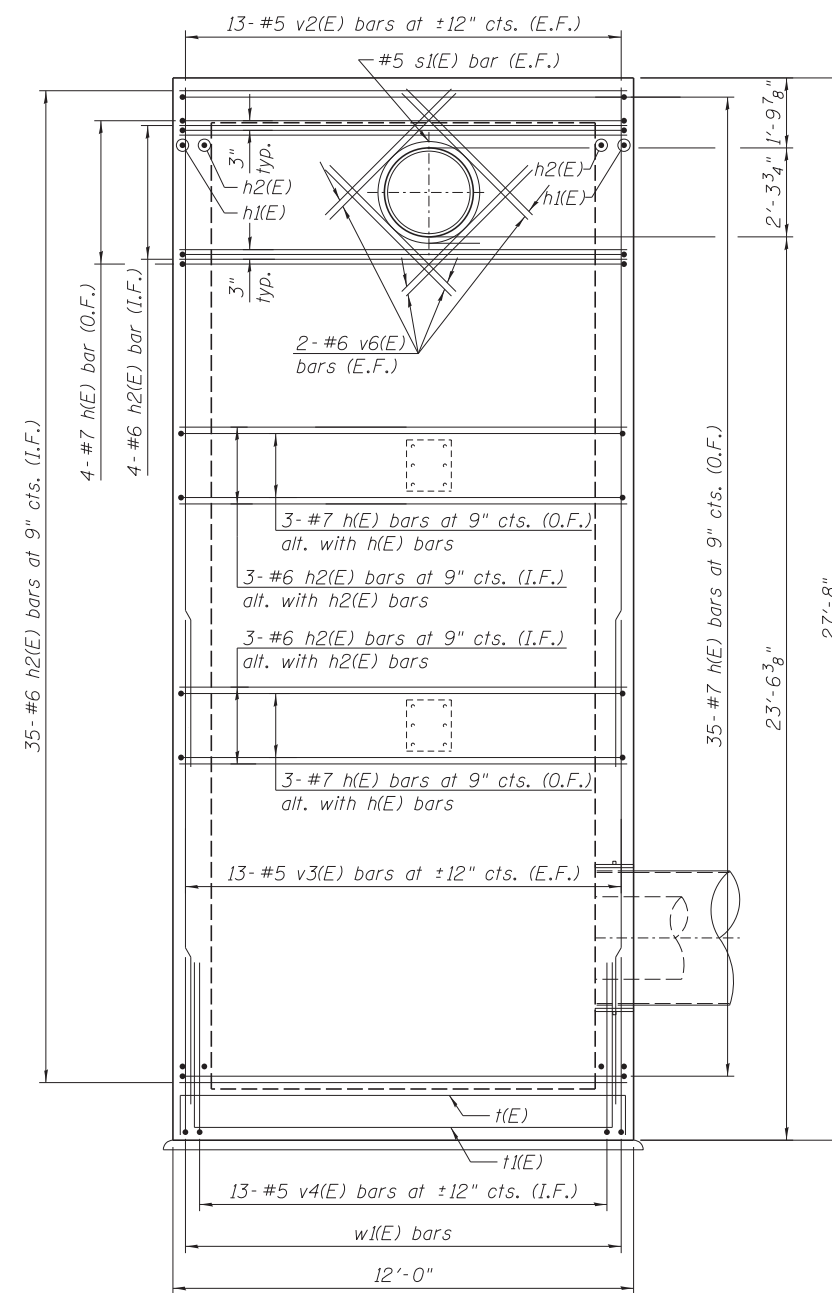
*Order bars at full length and cut in field to fit



SOUTH WALL ELEVATION
(Looking North)



EAST WALL ELEVATION
(Looking West)



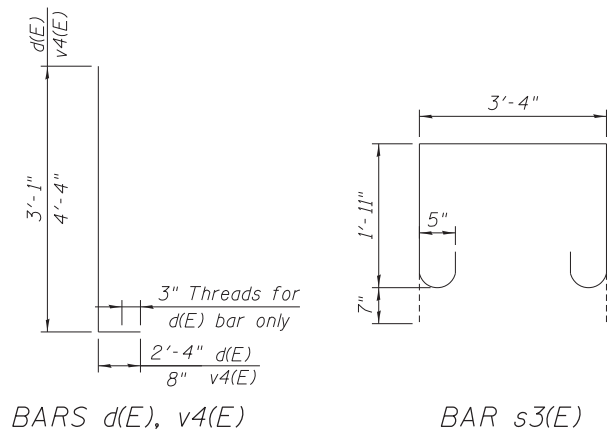
WEST WALL ELEVATION
(Looking East)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	20	#5	12'-10"	□
a1(E)	20	#6	11'-6"	—
a2(E)	4	#6	4'-10"	—
a3(E)	8	#6	3'-10"	—
a4(E)	4	#6	2'-10"	—
b(E)	20	#5	13'-0"	□
b1(E)	20	#6	11'-6"	—
d(E)	10	#5	5'-5"	□
h(E)	93	#7	21'-10"	□
h1(E)	74	#7	11'-6"	—
h2(E)	159	#6	11'-6"	—
n(E)	10	#5	4'-8"	—
n1(E)	25	#5	2'-10"	—
n2(E)	5	#5	6'-8"	—
s(E)	2	#5	16'-5"	○
s1(E)	4	#5	12'-2"	○
s2(E)	6	#5	15'-0"	□
s3(E)	8	#5	8'-4"	□
t(E)	25	#6	13'-0"	□
t1(E)	25	#6	21'-10"	□
v(E)	52	#5	15'-9"	—
v1(E)	52	#5	14'-7"	—
v2(E)	52	#5	17'-9"	—
v3(E)	52	#5	12'-7"	—
v4(E)	52	#5	5'-0"	—
v5(E)	16	#6	6'-2"	—
v6(E)	16	#6	4'-8"	—
v7(E)	8	#6	11'-10"	—
w(E)	25	#6	13'-2"	□
w1(E)	25	#6	21'-10"	□

Concrete Structures Cu Yd 61.5
Reinforcement Bars, Epoxy Coated Pound 16,970
Bars designated (E) shall be epoxy coated

Minimum Bar Laps	
Bar	Lap
#4	2'-7"
#5	3'-3"
#6	3'-10"
#7	5'-2"
#8	6'-9"

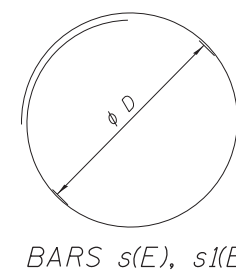


BARS d(E), v4(E)

BAR s3(E)

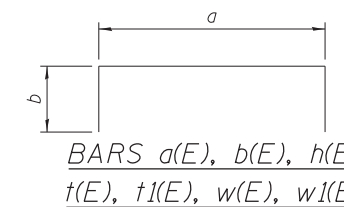
BAR s2(E)

BAR v7(E)



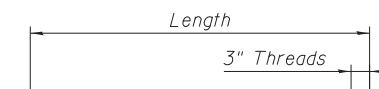
BARS s(E), s1(E)

Bar	phi D
s(E)	4'-2"
s1(E)	2'-10"



BARS a(E), b(E), h(E), t(E), t1(E), w(E), w1(E)

Bar	a	b
a(E)	11'-6"	8"
b(E)	11'-6"	9"
h(E)	11'-6"	5'-2"
t(E)	11'-6"	9"
t1(E)	11'-6"	5'-2"
w(E)	11'-6"	10"
w1(E)	11'-6"	5'-2"



BARS n(E), n1(E), n2(E)

LEGEND:

E.F. - Each Face
I.F. - Inside Face
O.F. - Outside Face

FILE PATH = C:\Users\will.mardous\Desktop\Riser_Shaft\DI60W25-Sht-RS04-Details3.dgn

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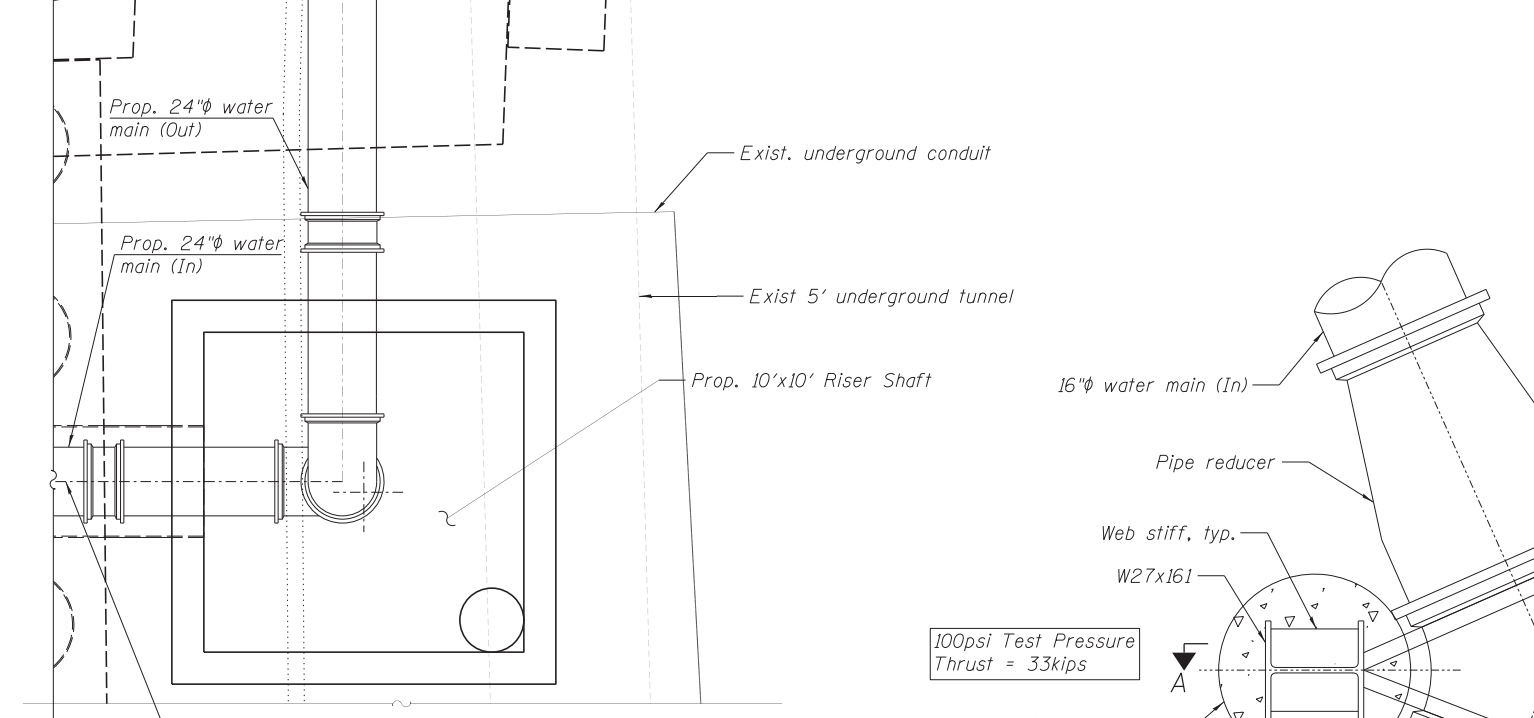
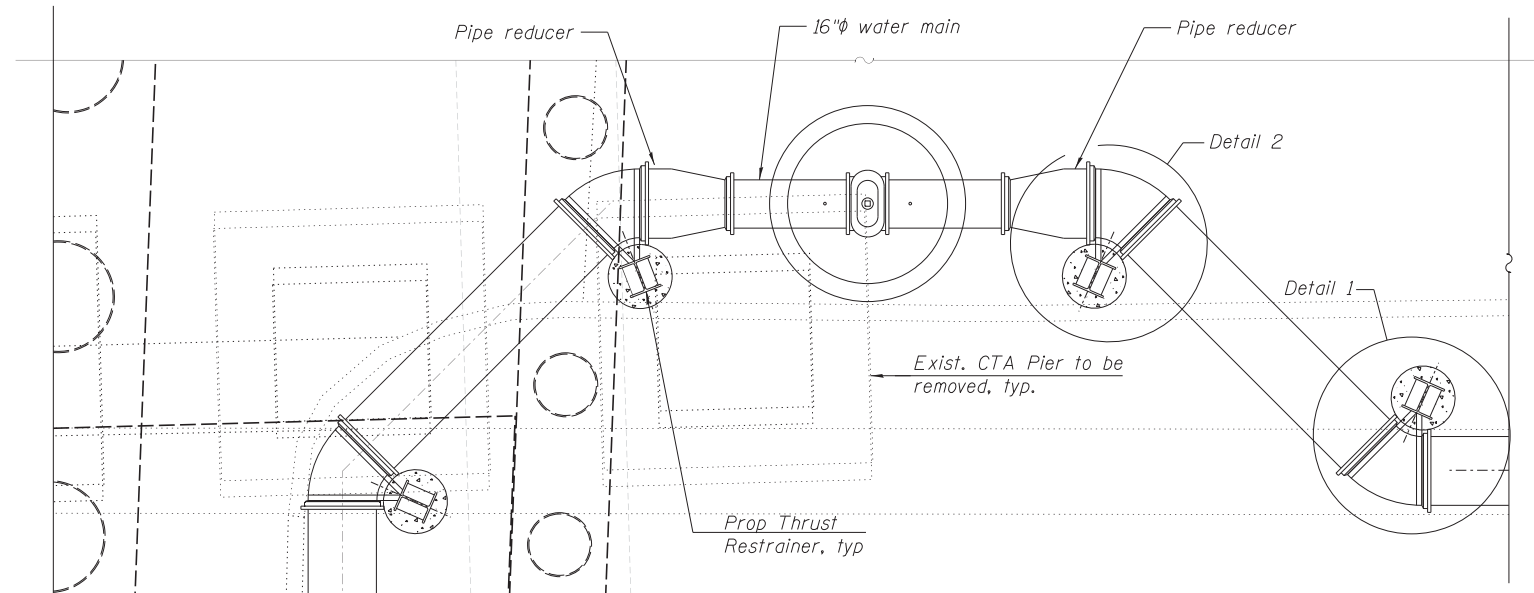
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RISER SHAFT SECTIONS AND DETAILS (SHEET 3 OF 3)
RISER SHAFT AND BRACED EXCAVATION

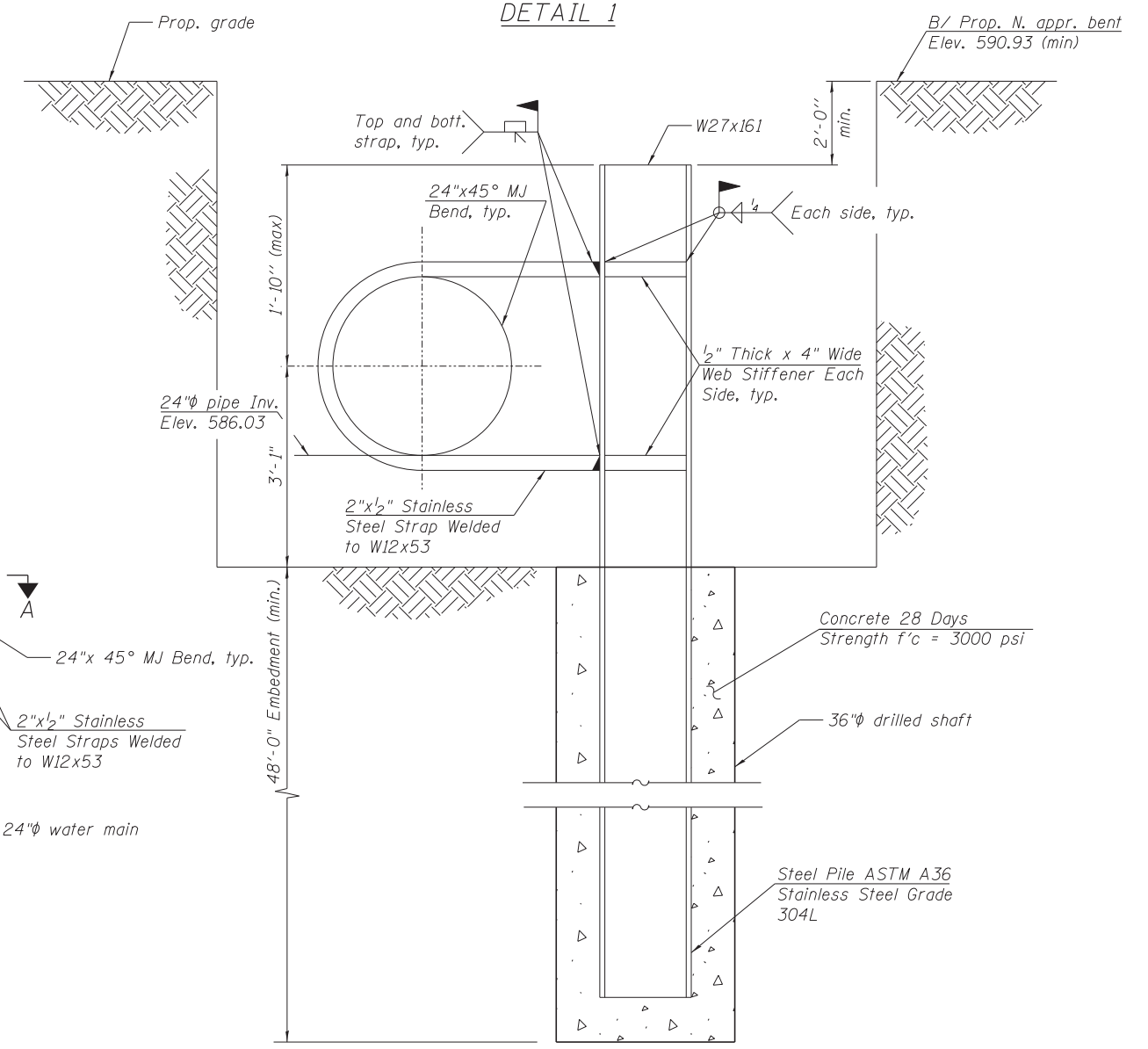
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F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	228
CONTRACT NO. 60W25			ILLINOIS FED. AID PROJECT	



See proposed watermain alignment, Sheet No. 87, for additional information

THRUST RESTRAINER PLAN



DETAIL 2

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Drilling and Setting Soldier Piles (in Soil)	CU FT	1358
Furnishing Soldier Piles (HP Section)	FOOT	212

- NOTES:
1. Permanent Thrust Restraint Structure shown is designed to resist a maximum 47kip horizontal thrust load in pipe longitudinal direction.
 2. Steel Pile and Beams shall be ASTM A-36.
 3. Welds shall be in compliance with AWS D1.1 "Structural Welding Code - Steel", latest edition.

FILE PATH = C:\Users\will.mor-deuss\Desktop\Riser_Shaft\160W25-Sht-RS05-ThrustRestraint.dgn

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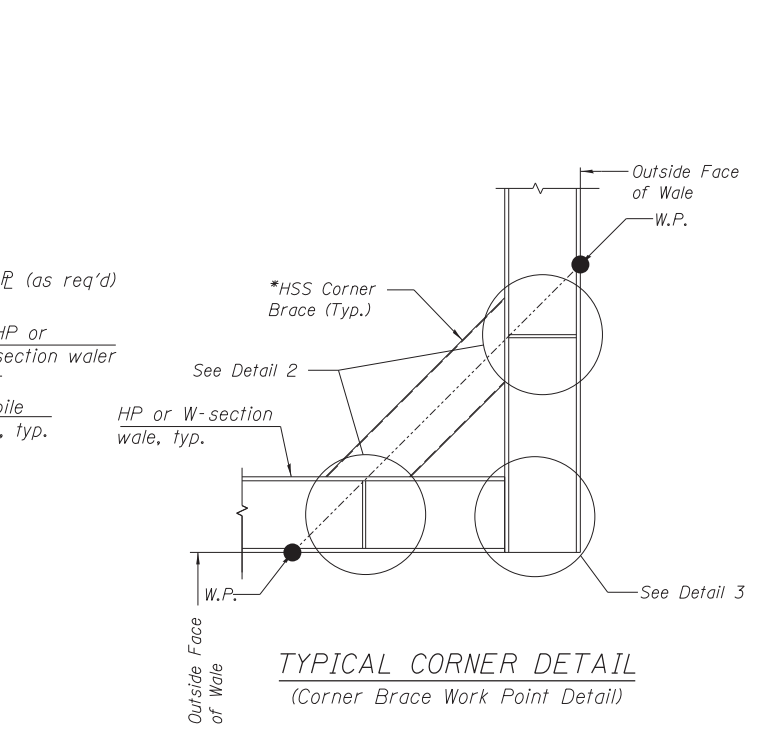
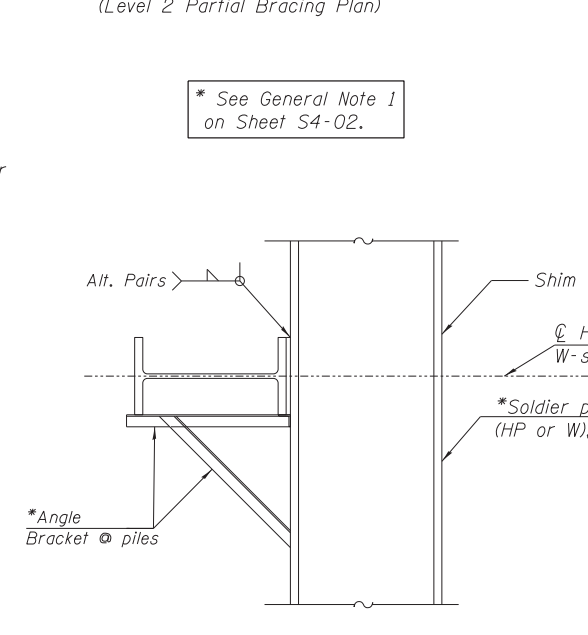
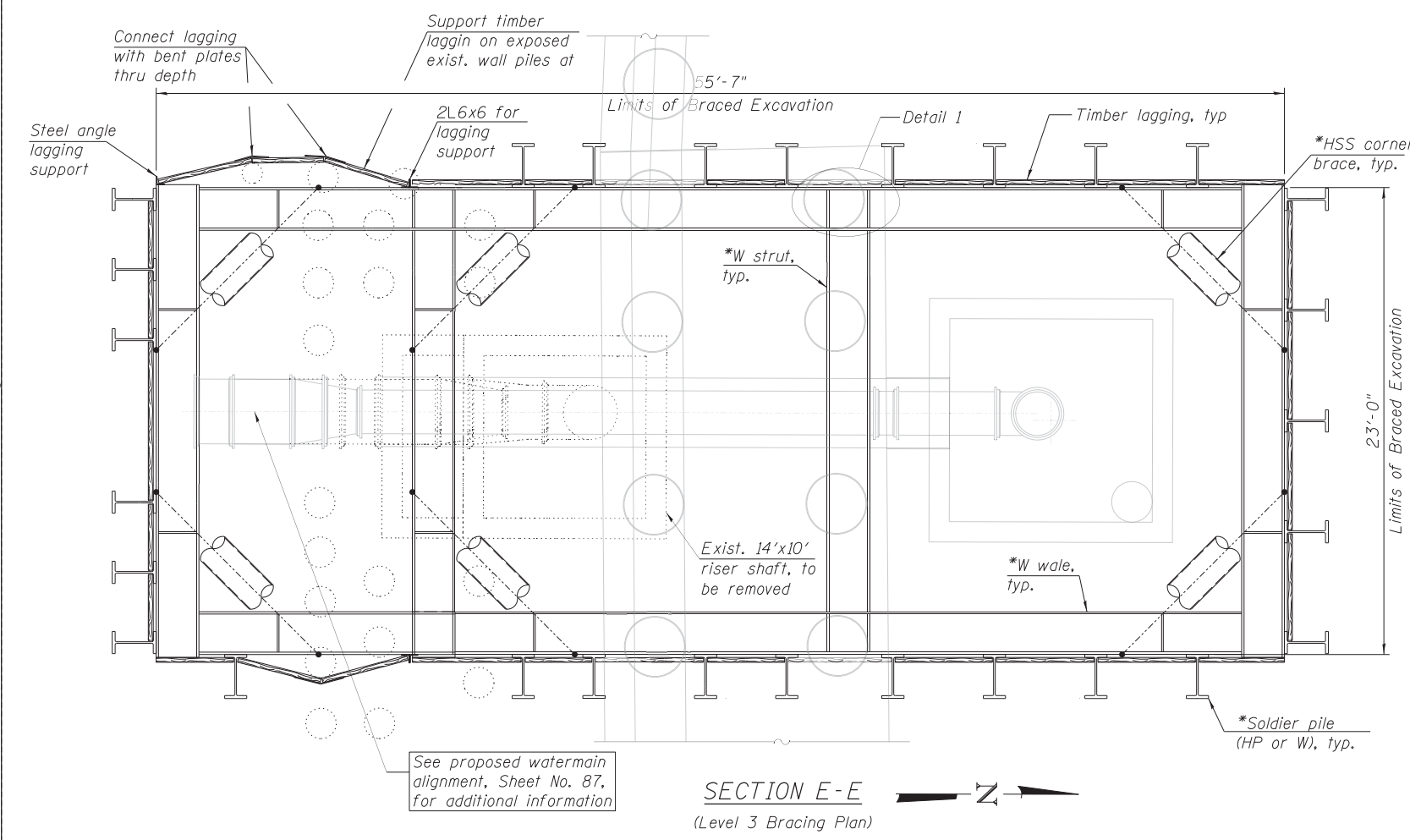
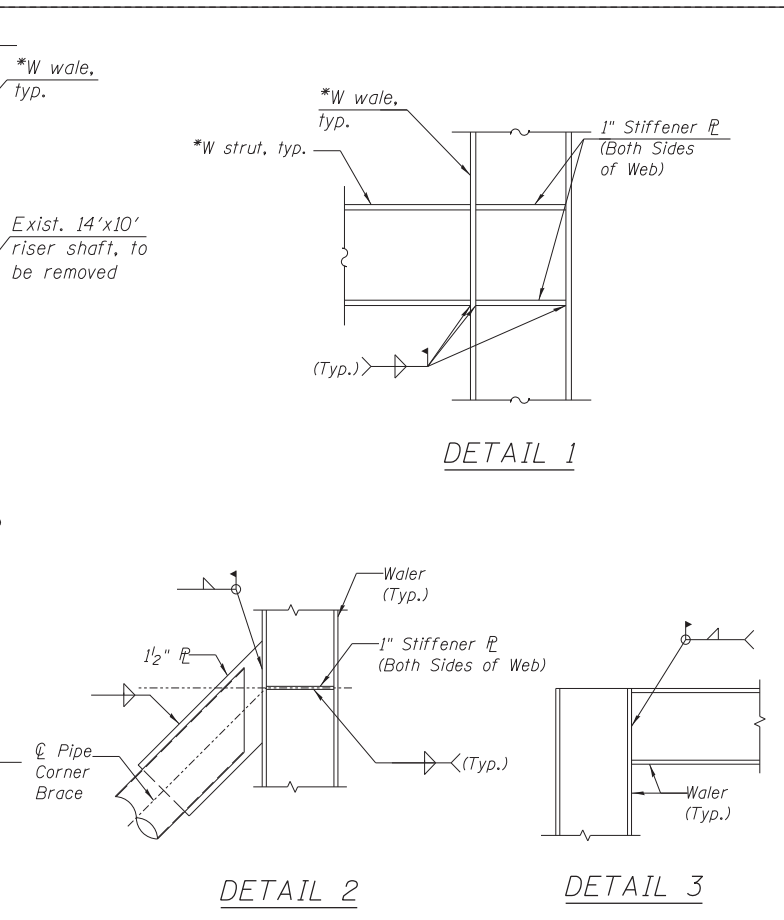
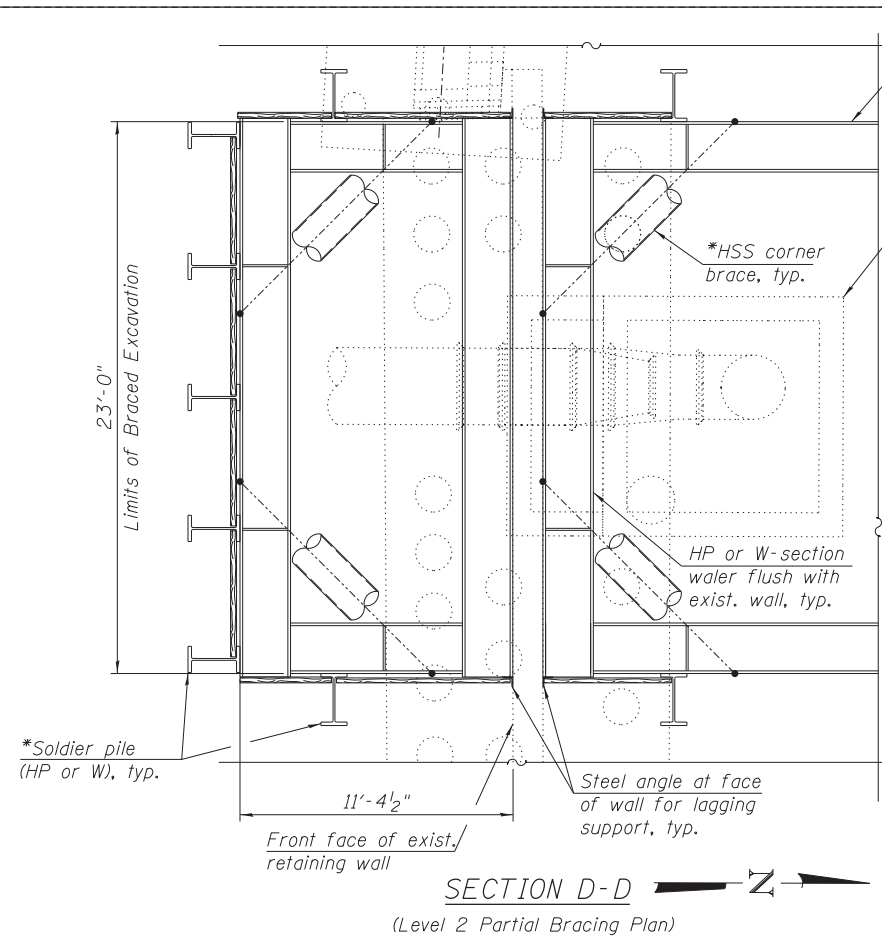
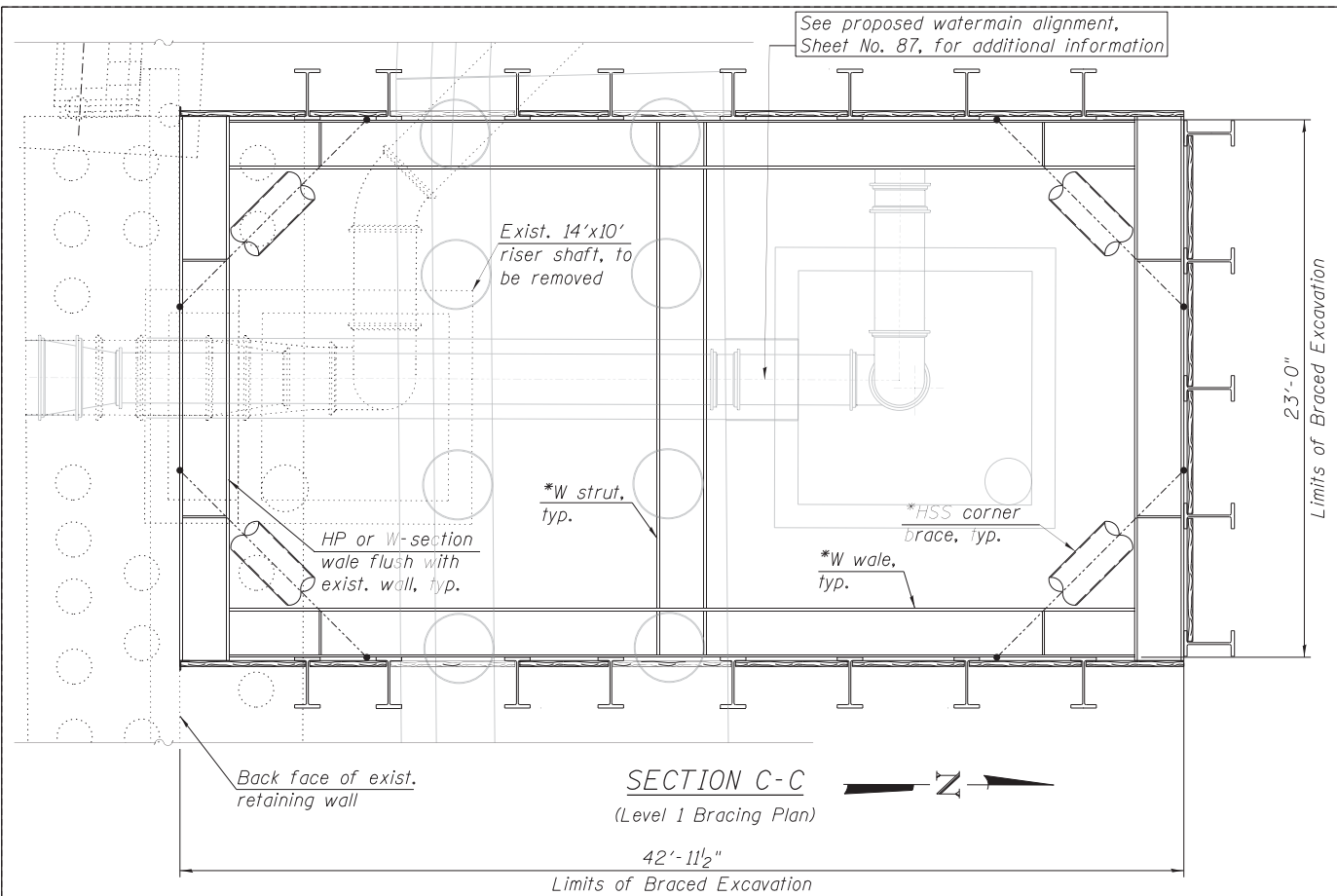
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PERMANENT THRUST RESTRAINERS PLAN, SECTIONS AND DETAILS
 RISER SHAFT AND BRACED EXCAVATION

SCALE: SHEET S4-05 OF 8 SHEETS STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2013-007R	COUNTY COOK	TOTAL SHEETS 317	SHEET NO. 229
CONTRACT NO. 60W25			ILLINOIS FED. AID PROJECT	



- NOTES:**
- Any existing substructure elements interfering with proposed construction shall be removed as required.
 - All sections and details presented on Sheets S4-06 and S4-07 are for information only.

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Braced Excavation	CU YD	1,221

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DI60W25-Sht-RS06-BracedExcavation.dgn
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PLOT DATE = 6/14/2013

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DRAWN - WM
CHECKED - MAI, MI, JJS
DATE - 6/17/2013

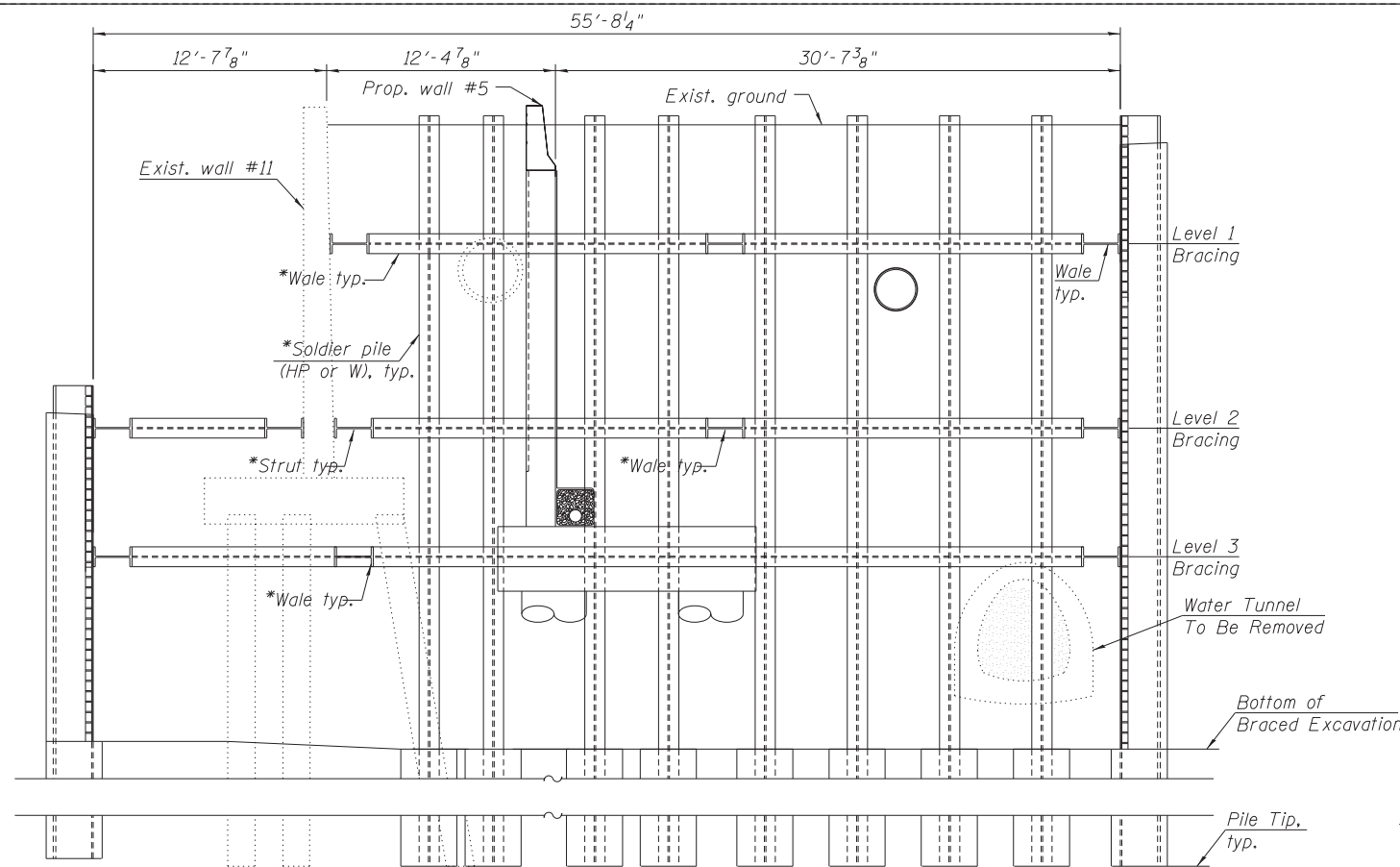
REVISED -
REVISED -
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REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

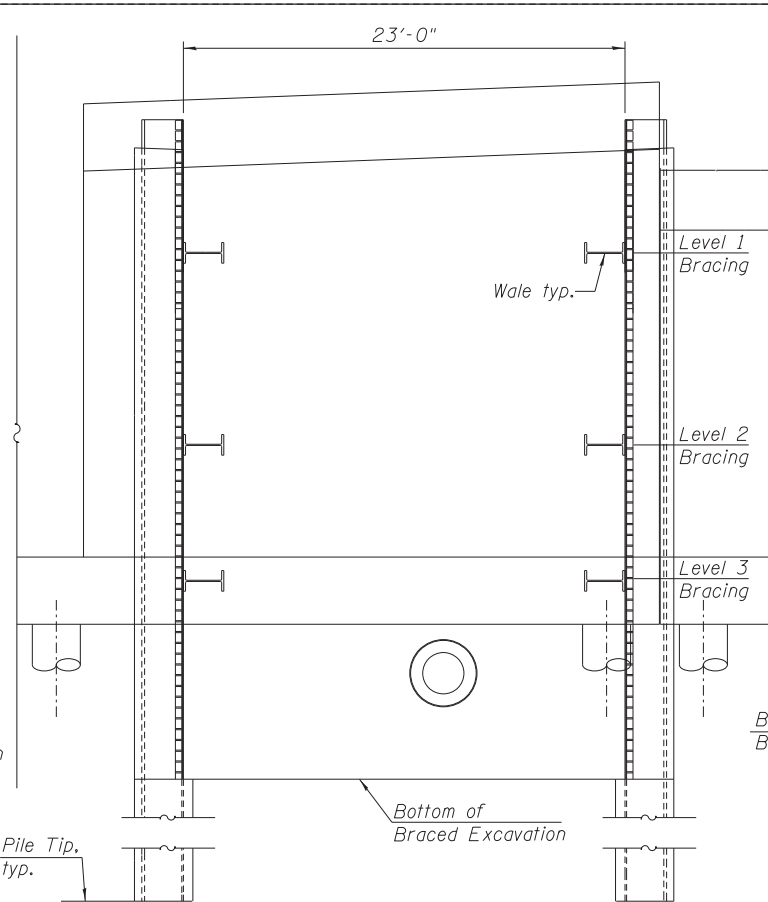
**SUGGESTED BRACED EXCAV. SECTIONS AND DETAILS (SHEET 1 OF 2)
RISER SHAFT AND BRACED EXCAVATION**

SCALE: SHEET S4-06 OF 8 SHEETS STA. TO STA.

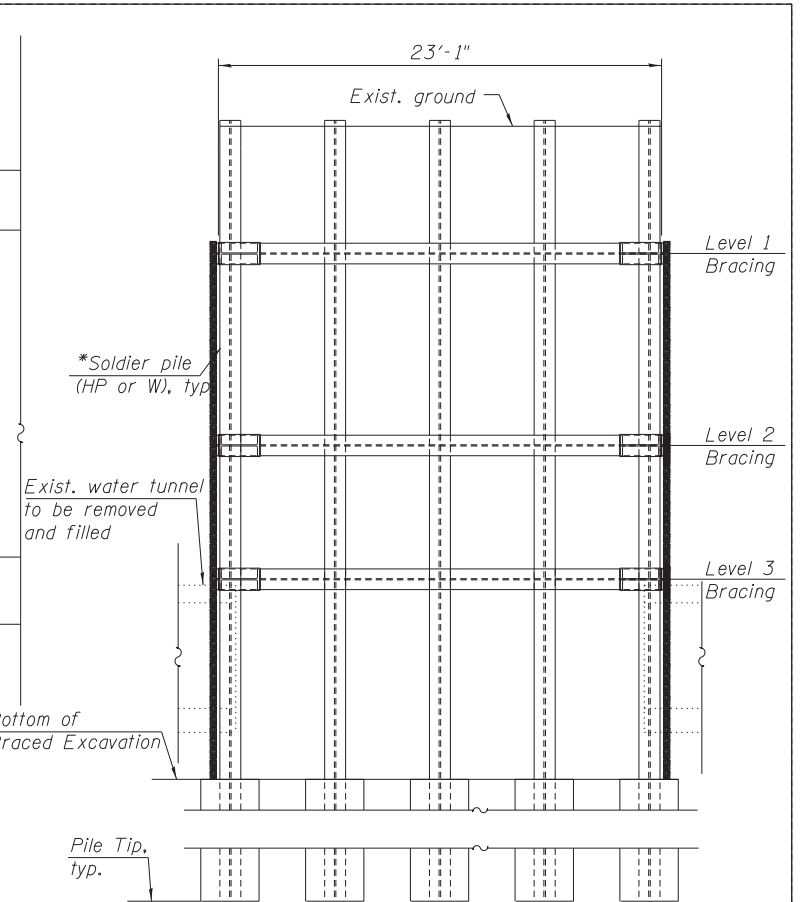
F.A.I. R.E. 90/94/290
SECTION 2013-007R
COUNTY COOK
TOTAL SHEETS 317
SHEET NO. 230
CONTRACT NO. 60W25
ILLINOIS FED. AID PROJECT



SECTION F-F



SECTION G-G

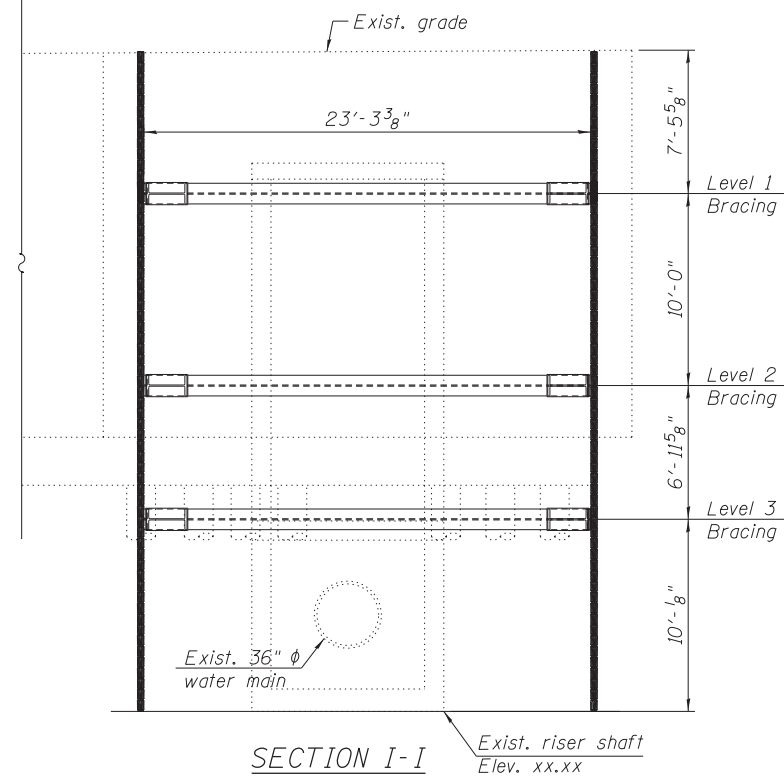


SECTION H-H

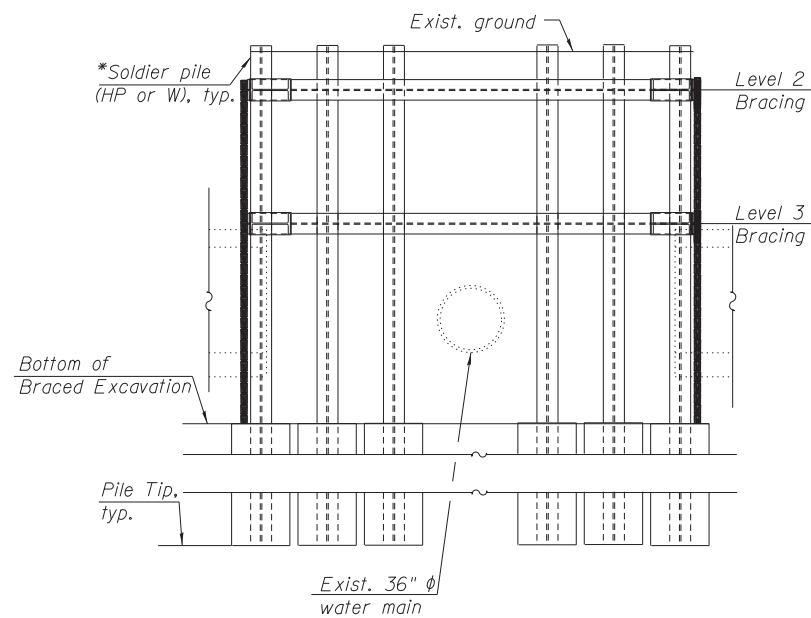
NOTES:

1. All exist. piles interfering with need construction to be removed.

*See General Note 1 on Sheet S4-02.



SECTION I-I



SECTION J-J

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED BRACED EXCAV. SECTIONS AND DETAILS (SHEET 2 OF 2)
RISER SHAFT AND BRACED EXCAVATION**

SCALE: SHEET S4-07 OF 8 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	231
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				



BORING LOG 2113-B-03

wangeng@wangeng.com
1145 N Main Street
Lombard, IL 60148
Telephone: 630 953-9928
Fax: 630 953-9938

WEI Job No.: 1100-04-01
Client: **AECOM**
Project: **CIRCLE INTERCHANGE RECONSTRUCTION**
Location: **Chicago, IL**

Datum: NGVD
Elevation: 594.20 ft
North: 1897826.77 ft
East: 1169837.46 ft
Station: 3603+12.67
Offset: 15.39 LT

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
593.85	8.5-inch thick ASPHALT --PAVEMENT--												
592.9	11-inch thick CONCRETE --PAVEMENT--												
	Very loose to loose, brown, fine SAND, trace gravel and brick fragments		1	2 3 4	NP	18				11	0 0 2	0.08 B	26
	--FILL--		2	2 1 2	NP	16				12	0 1 1	0.08 B	28
			3	1 3 3	NP	26							
586.2	Medium stiff to stiff, gray SILTY CLAY LOAM, trace gravel and brick fragments		4	1 2 3	0.75 P	43				13	0 1 1	0.25 B	26
	--FILL--10		5	1 2 3		21							
			6	0 2 2	0.45 B	38				14	0 0 2	0.33 B	27
581.2	Very soft to soft, gray CLAY to SILTY CLAY		7	2 1 1	0.33 B	28							
			8	0 1 2	0.16 B	27				15	0 1 3	0.41 B	25
			9	0 1 1	0.08 B	27							
			10	0 0 0	0.08 B	29				16	2 3 3	0.33 B	22

--L_L(%)=33, P_L(%)=15--
--%Gravel=1.0--
--%Sand=15.5--35--
--%Silt=49.9--
--%Clay=33.6--
--A-6 (14)--

GENERAL NOTES

Begin Drilling: 02-15-2013 Complete Drilling: 02-18-2013
Drilling Contractor: Wang Testing Services Drill Rig: B-57 TMR
Driller: R&J Logger: F.Bozga Checked by: C. Marin
Drilling Method: 2.25" SSA to 15', Mud Rotary 15' thereafter

WATER LEVEL DATA

While Drilling: 87.00 ft
At Completion of Drilling: MUD (15')
Time After Drilling: NA
Depth to Water: NA

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.



BORING LOG 2113-B-03

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1145 N Main Street
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Telephone: 630 953-9928
Fax: 630 953-9938

WEI Job No.: 1100-04-01
Client: **AECOM**
Project: **CIRCLE INTERCHANGE RECONSTRUCTION**
Location: **Chicago, IL**

Datum: NGVD
Elevation: 594.20 ft
North: 1897826.77 ft
East: 1169837.46 ft
Station: 3603+12.67
Offset: 15.39 LT

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
542.7	Very stiff, gray SILTY CLAY LOAM, trace gravel												
			17	6 7 10	3.28 B	20				22	13 20 30	8.04 B	16
			18	6 7 14	2.55 B	20				23	12 12 19	3.36 B	20
532.2	Medium dense, gray SANDY LOAM		19	9 8 8	NP	15				24	43 50/5	NP	9
			20	6 6 7	1.80 B	27				25	50/1	NP	4
527.2	Stiff to hard, gray SILTY CLAY		21	28 28 50	7.71 S	12							
	--HARD DRILLING--												
	--AUGER REFUSAL--												
	Boring terminated at 94.50 ft												

--L_L(%)=35, P_L(%)=13--
--%Gravel=0.1--
--%Sand=1.0--85--
--%Silt=60.6--
--%Clay=38.2--
--A-6 (21)--

GENERAL NOTES

Begin Drilling: 02-15-2013 Complete Drilling: 02-18-2013
Drilling Contractor: Wang Testing Services Drill Rig: B-57 TMR
Driller: R&J Logger: F.Bozga Checked by: C. Marin
Drilling Method: 2.25" SSA to 15', Mud Rotary 15' thereafter

WATER LEVEL DATA

While Drilling: 87.00 ft
At Completion of Drilling: MUD (15')
Time After Drilling: NA
Depth to Water: NA

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BORING LOG
RISER SHAFT AND BRACED EXCAVATION

F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	232
CONTRACT NO. 60W25			ILLINOIS FED. AID PROJECT	

SCALE: SHEET S4-08 OF 8 SHEETS STA. TO STA.

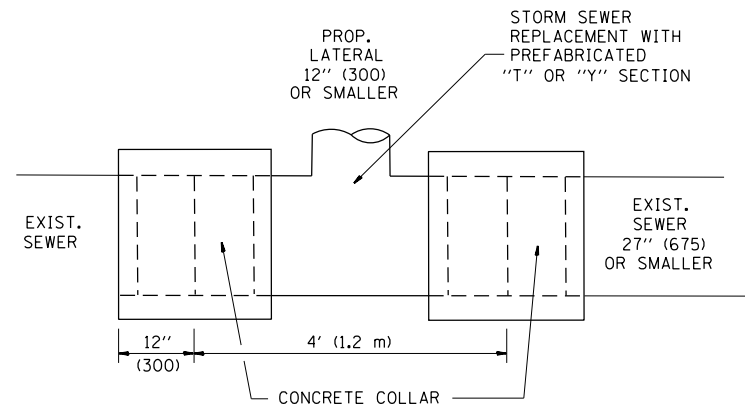
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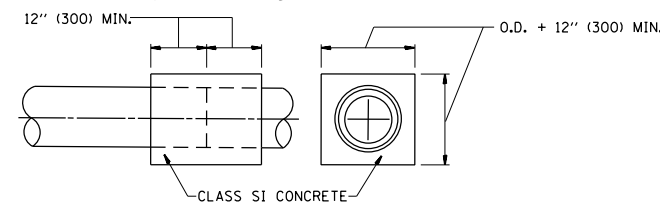
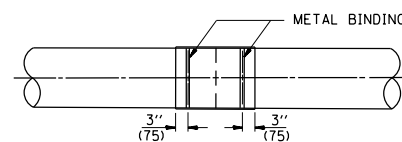
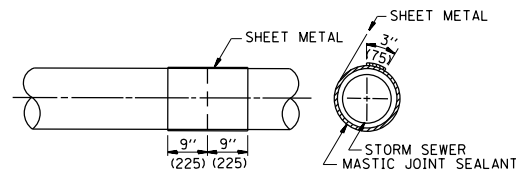
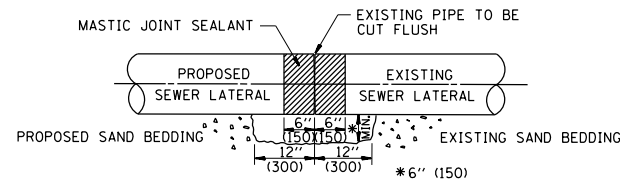
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DRAWN - WM
CHECKED - MAI
DATE - 6/17/2013

REVISED -
REVISED -
REVISED -
REVISED -



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER



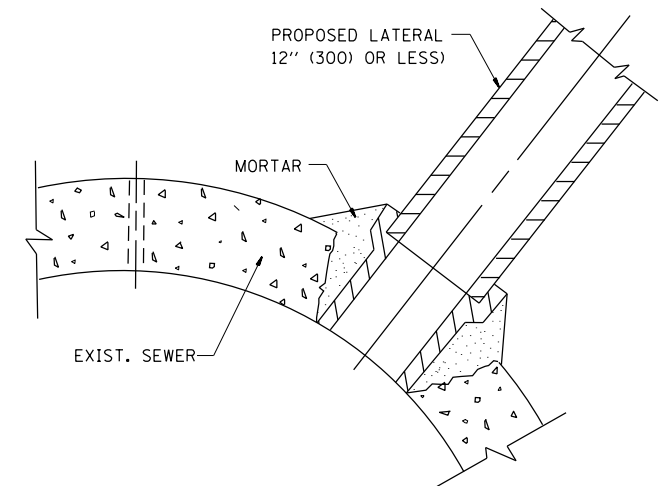
DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

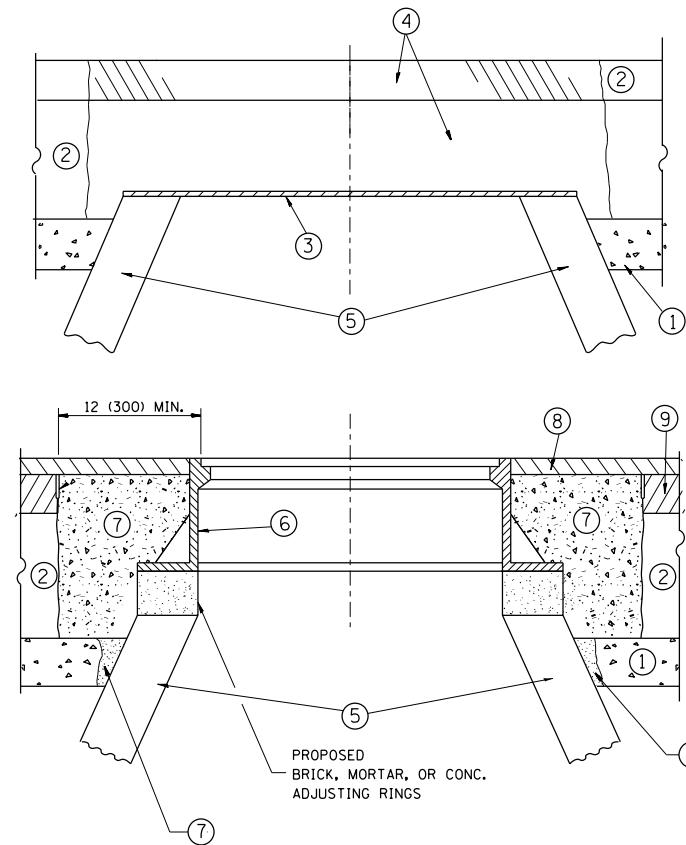
CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

FILE NAME =	USER NAME = pte1p2	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92
pw:\388039-pwintl\ecomonline\local\pwae\000\Documents\01 Americas\Transportation\40000\CAD\006.Road\REVISED\60W25\B.C.SHAH\09-08-94\HT-D1\Detail-01.dgn		DRAWN - B. SHAH 09-08-94	REVISED - B. SHAH 10-25-94
	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - B. SHAH 06-12-96
	PLOT DATE = 5/15/2013	DATE - 07-25-90	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER		
SCALE: NONE	SHEET NO. 1 OF 31 SHEETS	STA. _____ TO STA. _____

F.A.I. RT.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2013-007R	COOK	317	233
BD500-01 (BD-7)		CONTRACT NO. 60W25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

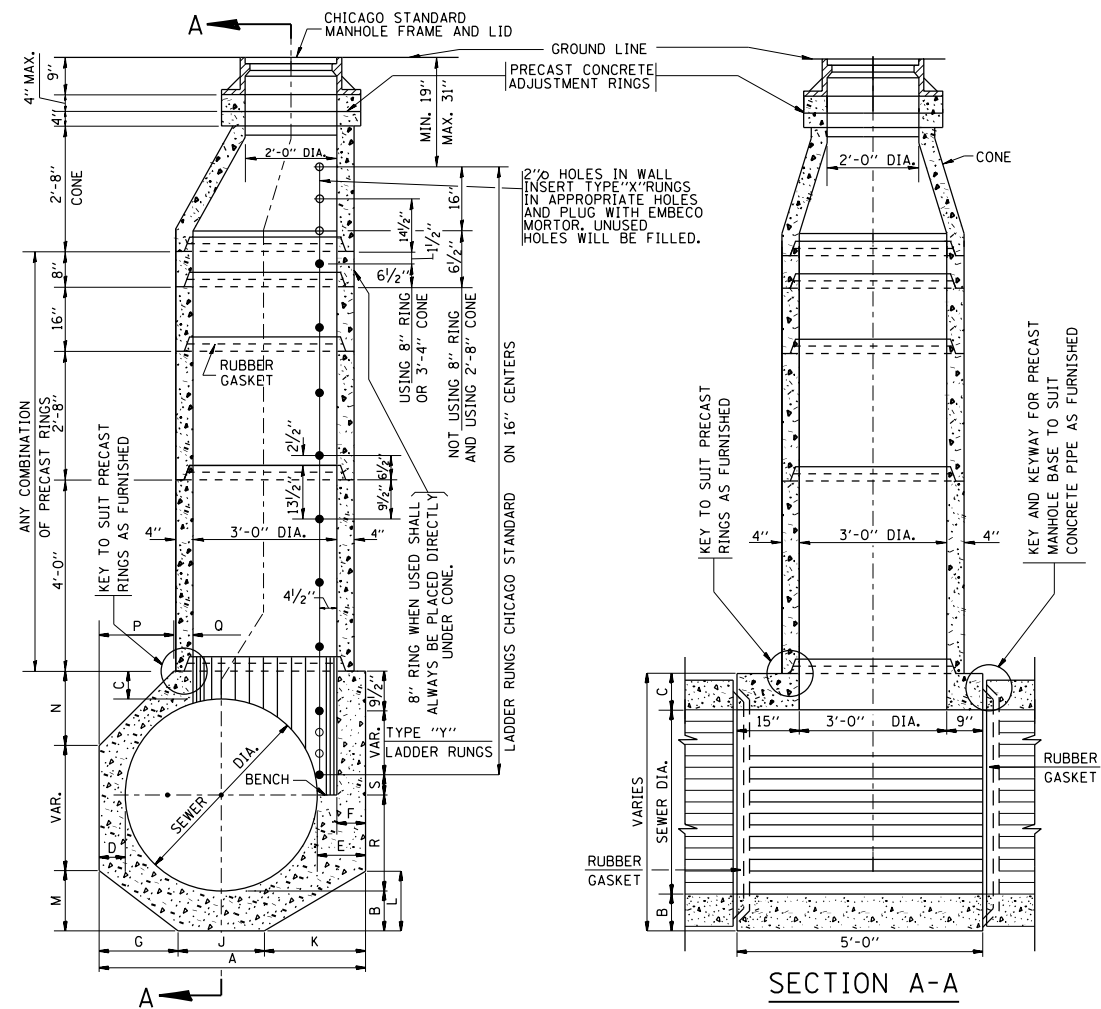
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = pte1p2	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
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	PLOT SCALE = 49.9999' / in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 5/15/2013	DATE - 10-25-94	REVISED - R. BORO 12-06-11

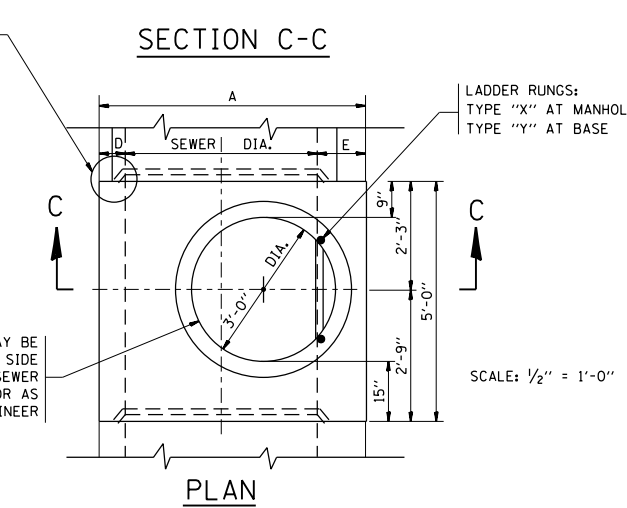
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 2 OF 31 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2013-007R	COOK	317	234
BD600-03 (BD-8)		CONTRACT NO. 60W25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SECTION A-A



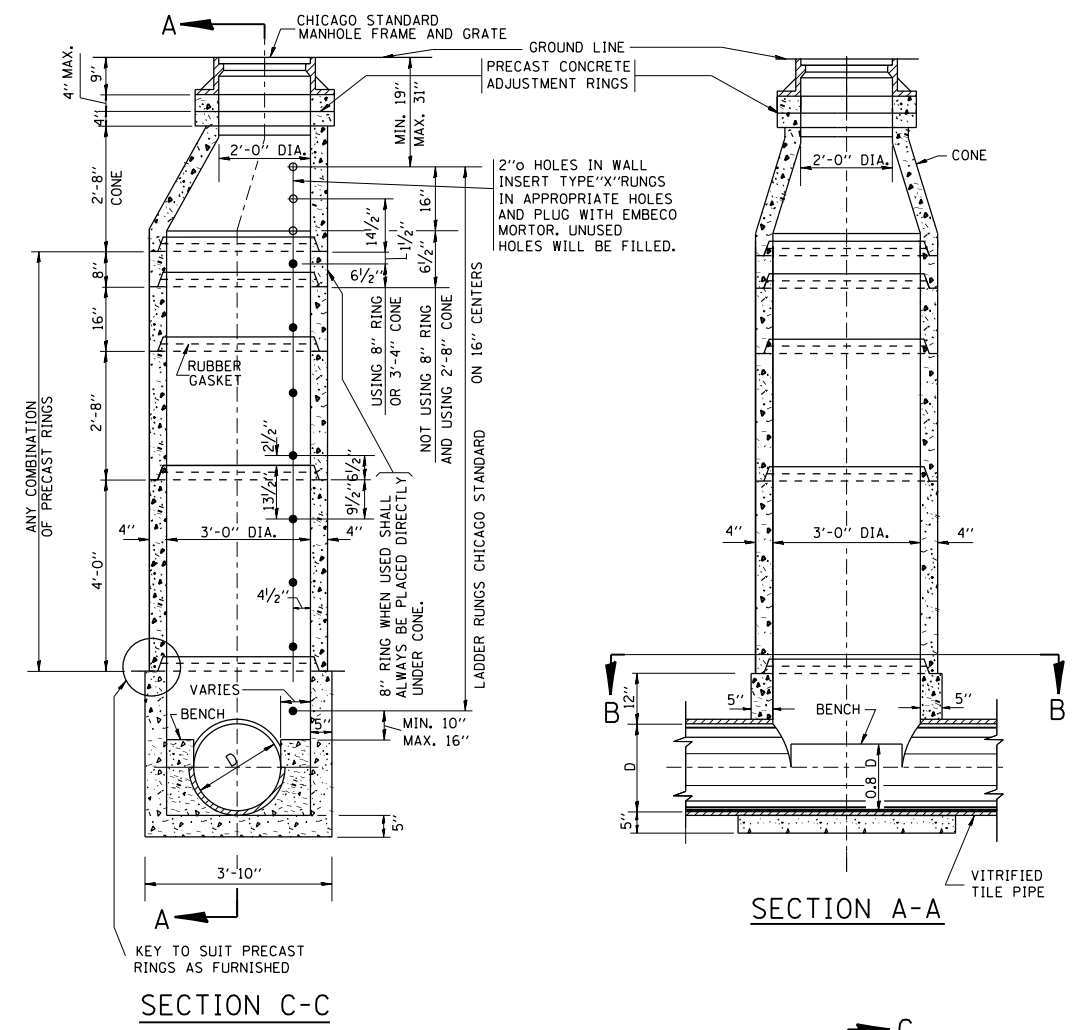
SECTION C-C

PLAN

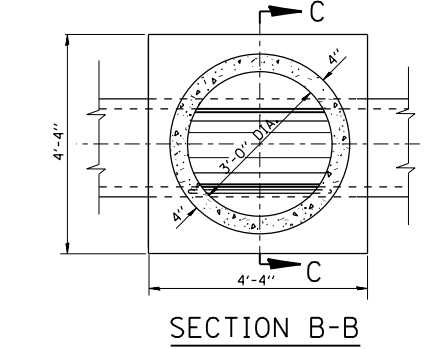
TYPE "A" MANHOLE
FOR SEWERS
24" TO 120" DIAMETER
PRECAST BASES AND RINGS

SEWER DIA.	PART OF ITEM	DIMENSIONS OF PRECAST MANHOLE BASE																NO. "Y" RINGS
		A	B	C	D	E	F	G	J	K	L	M	N	P	Q	R		
120"	----	12"-4 1/2"	12"	12"	12"	16 1/2"	12"	4'-0"	4'-0"	4'-4 1/2"	2'-7 1/2"	2'-5"	3'-7"	3'-7"	4'-8 1/2"	2'-0"	2 1/2"	7
108"	----	11"-4 1/2"	12"	12"	12"	16 1/2"	12"	3'-8"	3'-8"	4'-0 1/2"	2'-5"	2'-2"	3'-4"	3'-4"	4'-0 1/2"	2'-0"	6 1/2"	6
102"	----	10"-10 1/2"	12"	12"	12"	16 1/2"	12"	3'-6"	3'-6"	3'-10 1/2"	2'-4"	2'-1"	3'-2"	3'-2"	3'-8 1/2"	2'-0"	16 1/2"	5
96"	10-A	10"-2 1/2"	11"	11"	11"	15 1/2"	11"	3'-3"	3'-3"	3'-8 1/2"	2'-3"	23"	2'-11"	2'-11"	3'-4 1/2"	2'-0"	9 1/2"	5
90"	10-B	9'-8 1/2"	11"	11"	11"	15 1/2"	11"	3'-1"	3'-1"	3'-6 1/2"	2'-1 1/2"	22"	2'-10"	2'-10"	2'-11 1/2"	2'-0"	3 1/2"	5
84"	10-C	9'-0 1/2"	10"	10"	10"	14 1/2"	10"	2'-11"	2'-11"	3'-2 1/2"	23"	21"	2'-7"	2'-7"	2'-7 1/2"	2'-0"	12 1/2"	4
78"	10-D	8'-6 1/2"	10"	10"	10"	14 1/2"	10"	2'-9"	2'-9"	3'-0 1/2"	22"	20"	2'-6"	2'-6"	2'-2 1/2"	2'-0"	6 1/2"	4
72"	10	7"-10 1/2"	9"	9"	9"	13 1/2"	9"	2'-6"	2'-6"	2'-10 1/2"	21"	18"	2'-3"	2'-3"	22 1/2"	2'-0"	15 1/2"	3
66"	11	7'-4 1/2"	9"	9"	9"	13 1/2"	9"	2'-4"	2'-4"	2'-8 1/2"	19 1/2"	17"	2'-1"	2'-1"	18 1/2"	2'-0"	9 1/2"	3
60"	12	6'-8 1/2"	8"	8"	8"	12 1/2"	8"	2'-1 1/2"	2'-1"	2'-6"	18"	15"	23"	23"	13 1/2"	2'-0"	2 1/2"	3
54"	13	6'-2 1/2"	8"	8"	8"	12 1/2"	8"	23 1/2"	23"	2'-4"	17"	14"	21"	21"	9 1/2"	2'-0"	12 1/2"	2
48"	14	5'-6 1/2"	7"	7"	7"	11 1/2"	7"	20 1/2"	21"	2'-1"	15"	12 1/2"	18 1/2"	18 1/2"	5"	2'-0"	5 1/2"	2
42"	15	5'-0 1/2"	7"	7"	7"	11 1/2"	7"	18 1/2"	19"	23"	14"	11"	---	---	17 1/2"	21"	2 1/2"	2
36"	16	4'-4 1/2"	6"	6"	6"	10 1/2"	6"	16"	16"	20 1/2"	12 1/2"	9 1/2"	---	---	10 1/2"	18"	14 1/2"	1
30"	17	4'-0"	6"	6"	6"	12"	6"	14"	14"	20"	12"	8 1/2"	---	---	6"	15"	11 1/2"	1
24"	18	4'-0"	6"	6"	12"	12"	6"	16"	16"	16"	9 1/2"	9 1/2"	---	---	6"	12"	8 1/2"	1

FOR STATE CONTRACT
ALL DIMENSIONS SHOULD
BE PREPARED IN METRIC
UNITS. SOFT CONVERSION
METHOD SHOULD BE USED.



SECTION C-C

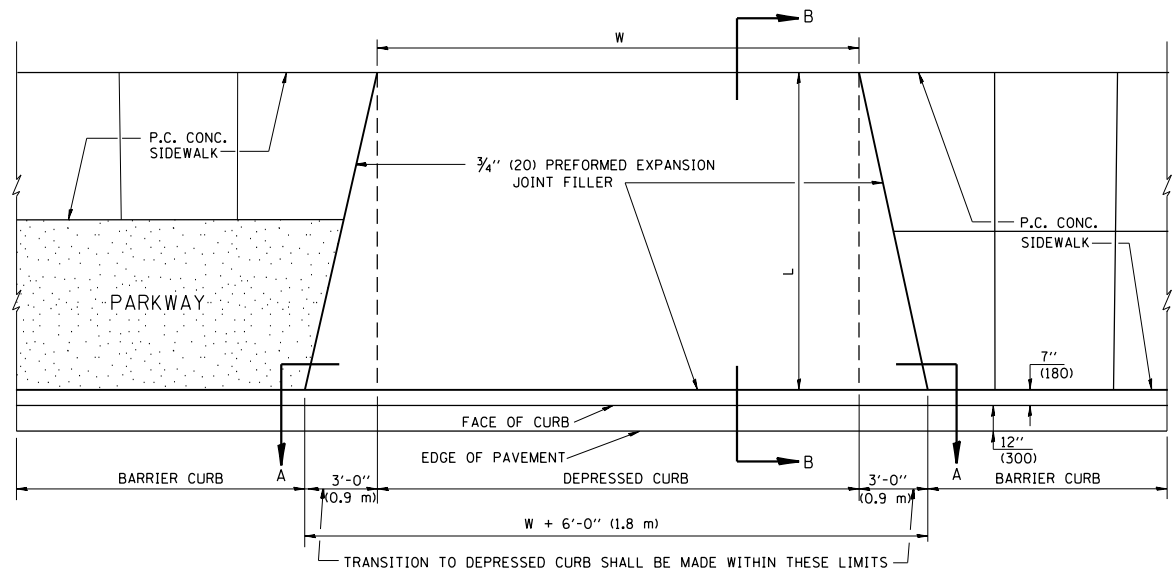


SECTION A-A

SECTION B-B

TYPE "A" MANHOLE
FOR SEWERS
21" DIAMETER AND SMALLER
PRECAST BASES AND RINGS

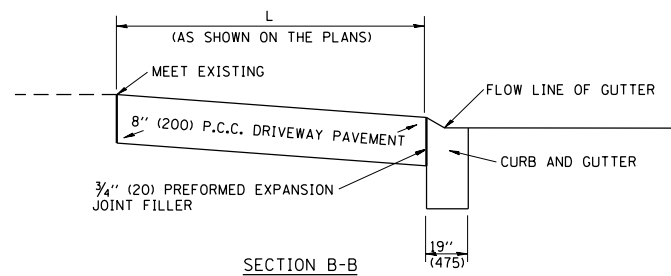
SCALE: 1/2" = 1'-0"



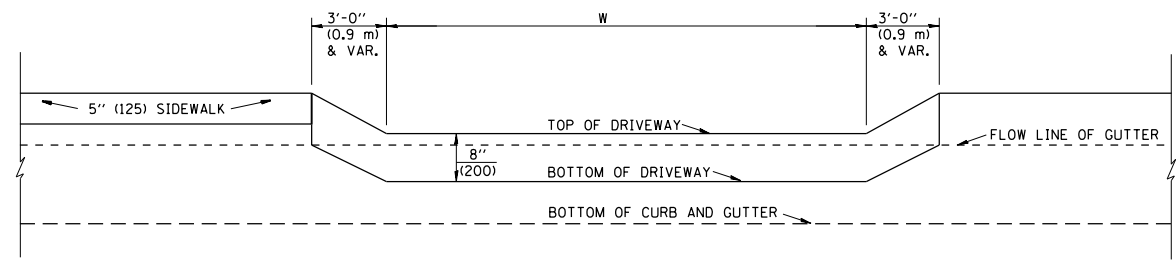
PLAN VIEW

NOTES:

1. EXPANSION JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE DETAILS FOR P.C.C. SIDEWALK.
2. THE CURB BETWEEN ADJACENT DRIVEWAYS SHALL BE FULL HEIGHT FOR A DISTANCE OF AT LEAST FOUR FEET (1.2 METERS)
3. P.C. CONCRETE DRIVEWAYS SHALL BE CONSTRUCTED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
4. 3/4" (20) PREFORMED EXPANSION JOINTS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO P.C.C. DRIVEWAY PAVEMENT 8" (200).
5. COMBINATION CONC. CURB AND GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE TRANSITION CURB AND GUTTER.

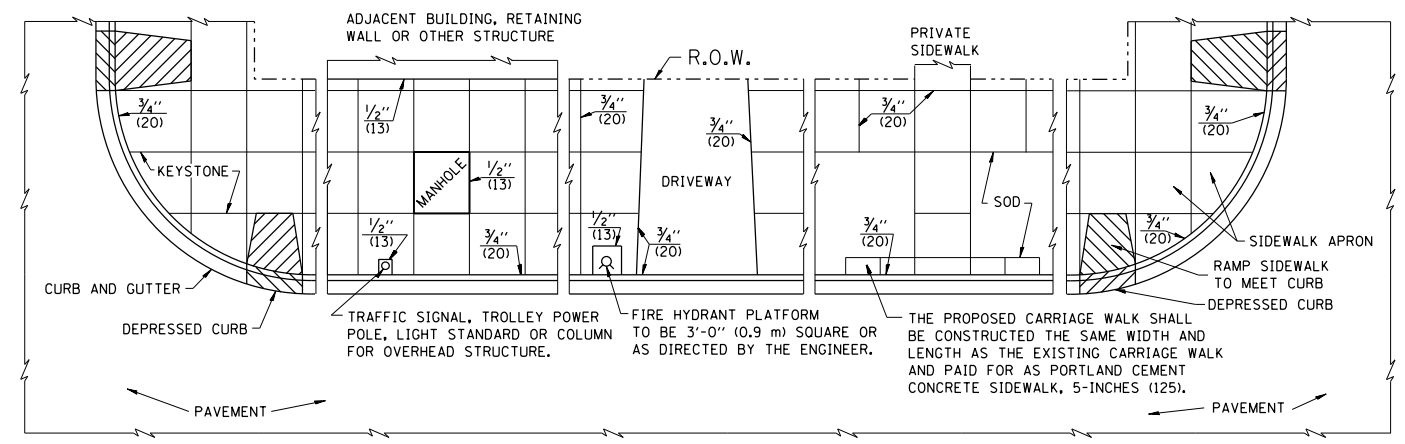


SECTION B-B



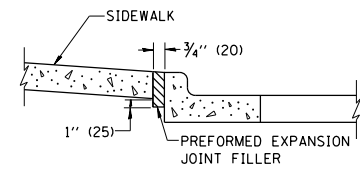
SECTION A-A

P.C.C. DRIVEWAY PAVEMENT DETAIL



NOTES:

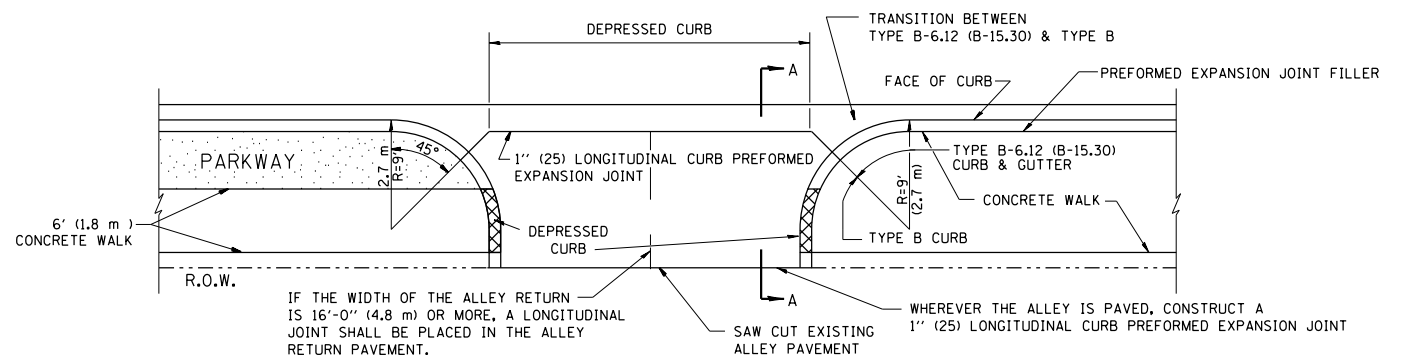
1. ONE-HALF INCH THICK EXPANSION JOINTS SHALL BE PLACED BETWEEN THE SIDEWALK AND ALL STRUCTURES SUCH AS LIGHT STANDARDS, TRAFFIC LIGHT STANDARDS, MANHOLES, WHICH EXTEND THROUGH THE SIDEWALK.
2. 3/4" (20) THICK EXPANSION JOINTS SHALL BE PLACED AT INTERVALS OF NOT MORE THAN 100 FEET (30 METERS) IN THE SIDEWALK, WHERE THE SIDEWALK IS CONSTRUCTED ADJACENT TO PAVEMENT OR CURB HAVING EXPANSION JOINTS, THE EXPANSION JOINTS IN THE SIDEWALK SHALL BE PLACED OPPOSITE THE EXISTING EXPANSION JOINTS AS NEARLY AS PRACTICABLE. EXPANSION JOINTS SHALL ALSO BE PLACED WHERE THE SIDEWALK ABUTS EXISTING SIDEWALKS, BETWEEN DRIVEWAY PAVEMENT AND SIDEWALK, AND BETWEEN SIDEWALK AND CURBS WHERE THE SIDEWALK ABUTS A CURB.



SLOPE FOR SIDEWALK
1" (25) IN 3'-0" (0.9 m) IN CHICAGO

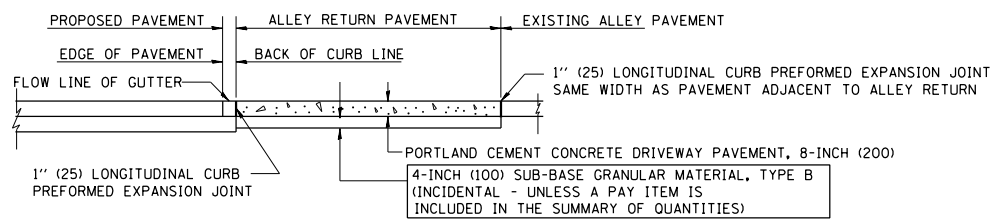
PORTLAND CEMENT CONCRETE SIDEWALK DETAILS

NOTES: NO EXTRA COMPENSATION SHALL BE ALLOWED FOR THE GUTTER FLARE



IF THE WIDTH OF THE ALLEY RETURN IS 16'-0" (4.8 m) OR MORE, A LONGITUDINAL JOINT SHALL BE PLACED IN THE ALLEY RETURN PAVEMENT.

WHEREVER THE ALLEY IS PAVED, CONSTRUCT A 1" (25) LONGITUDINAL CURB PREFORMED EXPANSION JOINT



SECTION A-A

ALLEY RETURN DETAIL

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = pte1p2	DESIGNED - M. DE YONG	REVISED -
p:\388039\p\int\ecom\line\local\PAVE\COM\Documents\01 Americas\Transportation\4000\Drawn\Circle\Phase.II\000.CAD\006.Road\REVISED\60W25.Contract\DI60W25-SHT-D1\Detail-04.dgn			
PLOT SCALE = 49.9999 / in.	CHECKED -	REVISOR -	REVISOR -
PLOT DATE = 5/15/2013	DATE - 06-13-90	REVISOR -	REVISOR -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
DETAILS FOR P.C. CONCRETE DRIVEWAY, ALLEY RETURN AND SIDEWALK
SCALE: NONE SHEET NO. 4 OF 31 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2013-007R	COOK	317	236
	BD400-03 (BD-17)	CONTRACT NO.	60W25	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
* 90/94/290				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

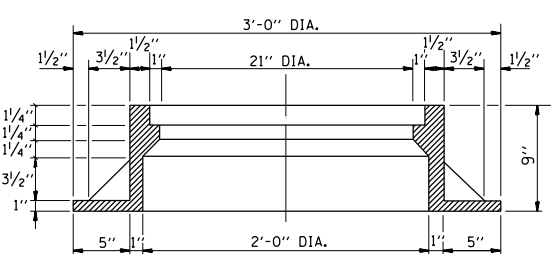
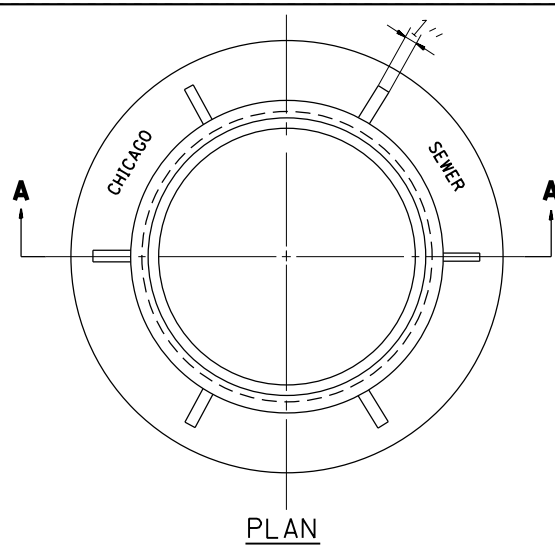
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

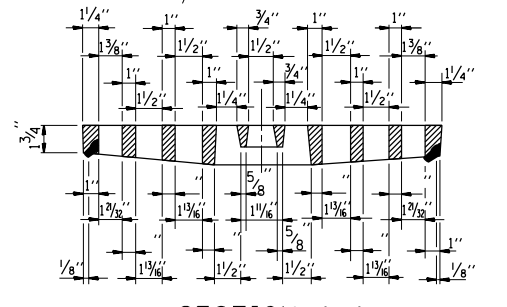
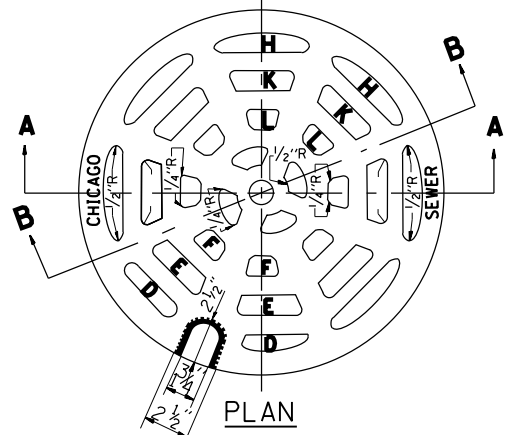
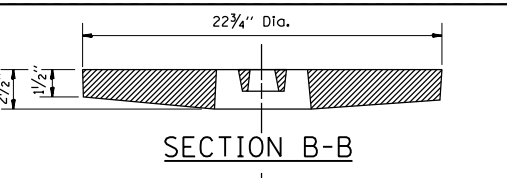
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = pteip2	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\388039-pwintl\ecomonline\local\pwaecom\000\Documents\01 Americas\Transportation\400\Drawn\Circle\Phase.II\000.CAD\006.Road\60W25.CAD\60223197.D\Detail-05.dgn	COMMENTS	CHECKED -	REVISED - M. GOMEZ 01-22-01		SCALE: NONE	SHEET NO. 5 OF 31 SHEETS	STA.	TO STA.	2013-007R	COOK	317	237
	PLOT SCALE = 50.000' / in.	DATE - 03-11-94	REVISED - R. BORO 12-15-09					BD600-06 (BD-24)		CONTRACT NO. 60W25		
	PLOT DATE = 5/15/2013						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

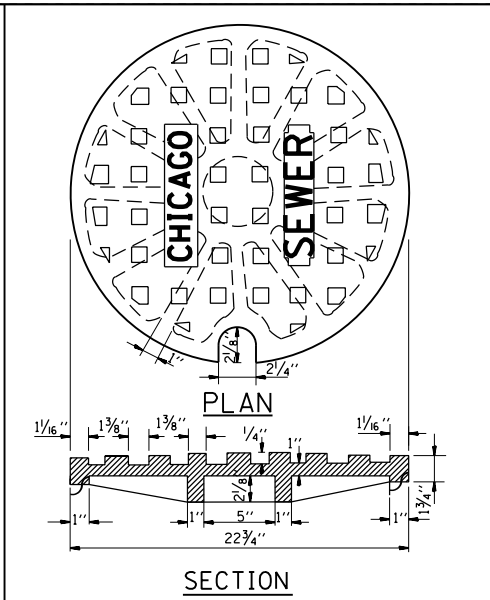
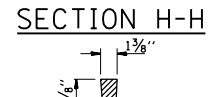
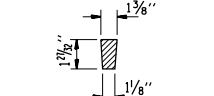
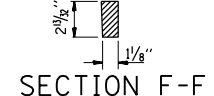
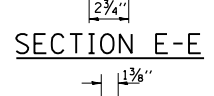
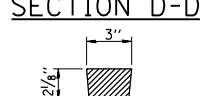
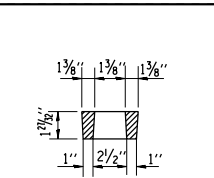


NOTE: METAL PLATES MUST BE FURNISHED FOR PERFORATED LIDS ON MANHOLES

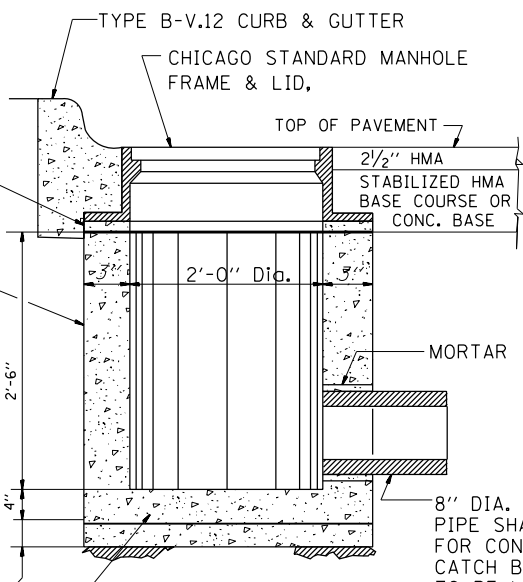
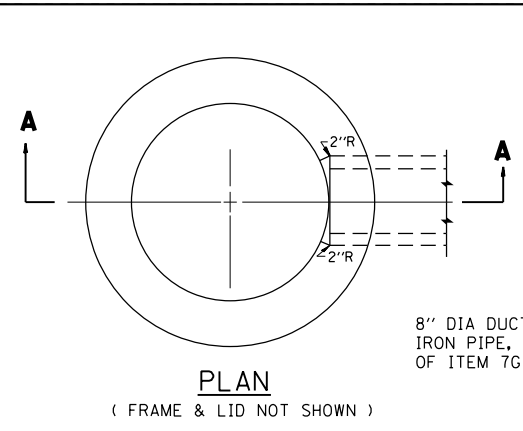
CHICAGO STANDARD MANHOLE FRAME
SCALE: 1 1/2" = 1'-0"
MATERIAL: CAST IRON



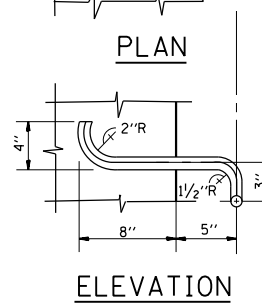
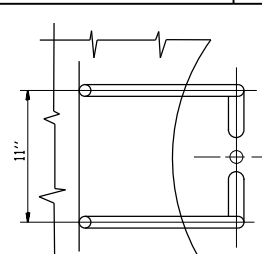
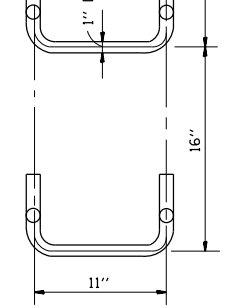
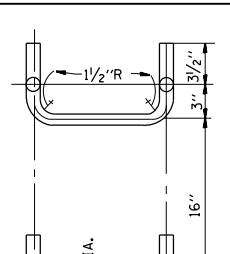
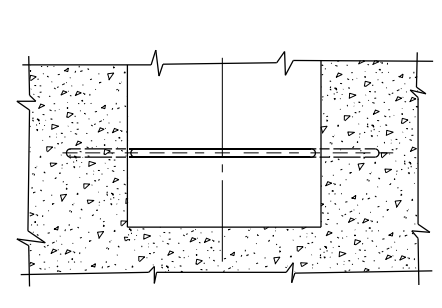
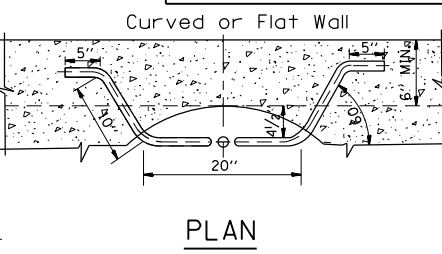
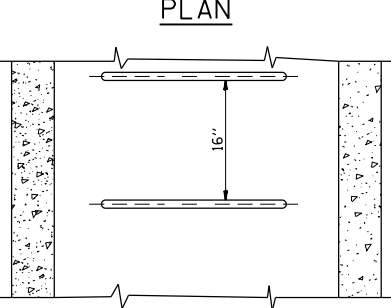
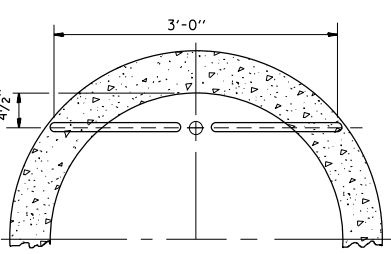
PERFORATED LID FOR CATCH BASINS & MANHOLES
SCALE: 2" = 1'-0"
MATERIAL: CAST IRON



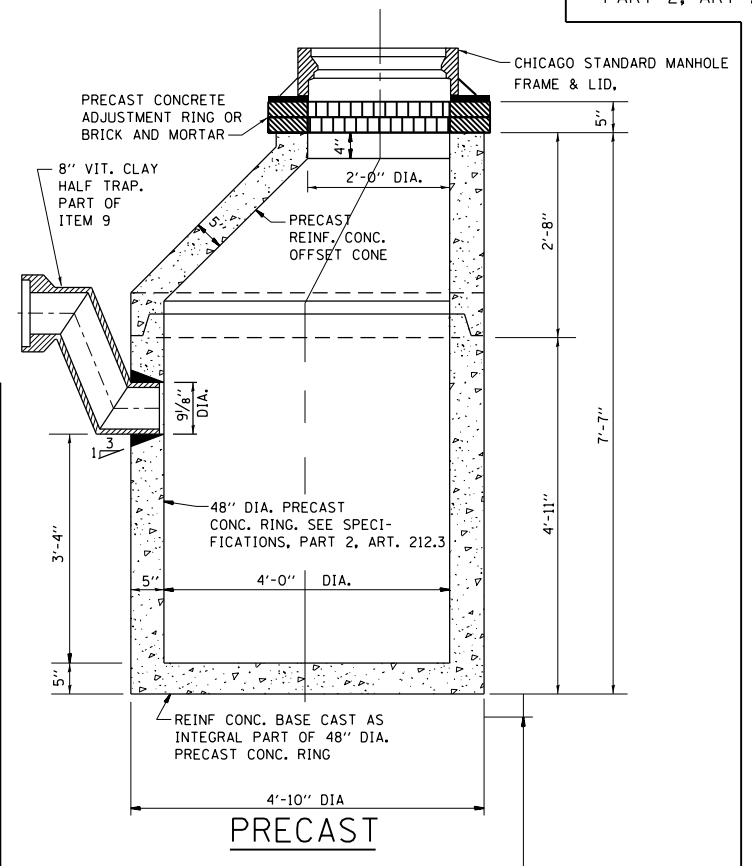
SOLID LID FOR MANHOLES
SCALE: NONE
MATERIAL: CAST IRON



STANDARD INLETS
SCALE 1" = 1'-0"
ITEM 12
THIS INLET DETAIL IS SOMETIMES REFERRED TO AS "CHICAGO STANDARD INLET, TYPE A"



STANDARD LADDER RUNGS
ALL LADDER RUNGS SHALL BE ALUMINUM OR GALVANIZED WROUGHT IRON AS SPECIFIED IN THE SPECIFICATIONS, PART 2, ARTICLE 214.2. RUNGS SHALL BE 1" DIAMETER OR OF A SHAPE HAVING AN EQUIVALENT CROSS-SECTIONAL AREA.



STANDARD CATCH BASINS
SCALE: 3/4" = 1'-0"
ITEM 9

NOTE: INLETS SHALL NOT BE CONSTRUCTED UNLESS IT IS IMPOSSIBLE TO CONSTRUCT A CATCH BASIN. THE CONTRACTOR SHALL HAVE THE DEPARTMENT OF SEWERS APPROVAL BEFORE CONSTRUCTING INLETS.

CITY OF CHICAGO
DEPARTMENT OF SEWERS
ENGINEERING DIVISION

FILE NAME =	USER NAME = ptelep2	DESIGNED - M. GOMEZ	REVISED -
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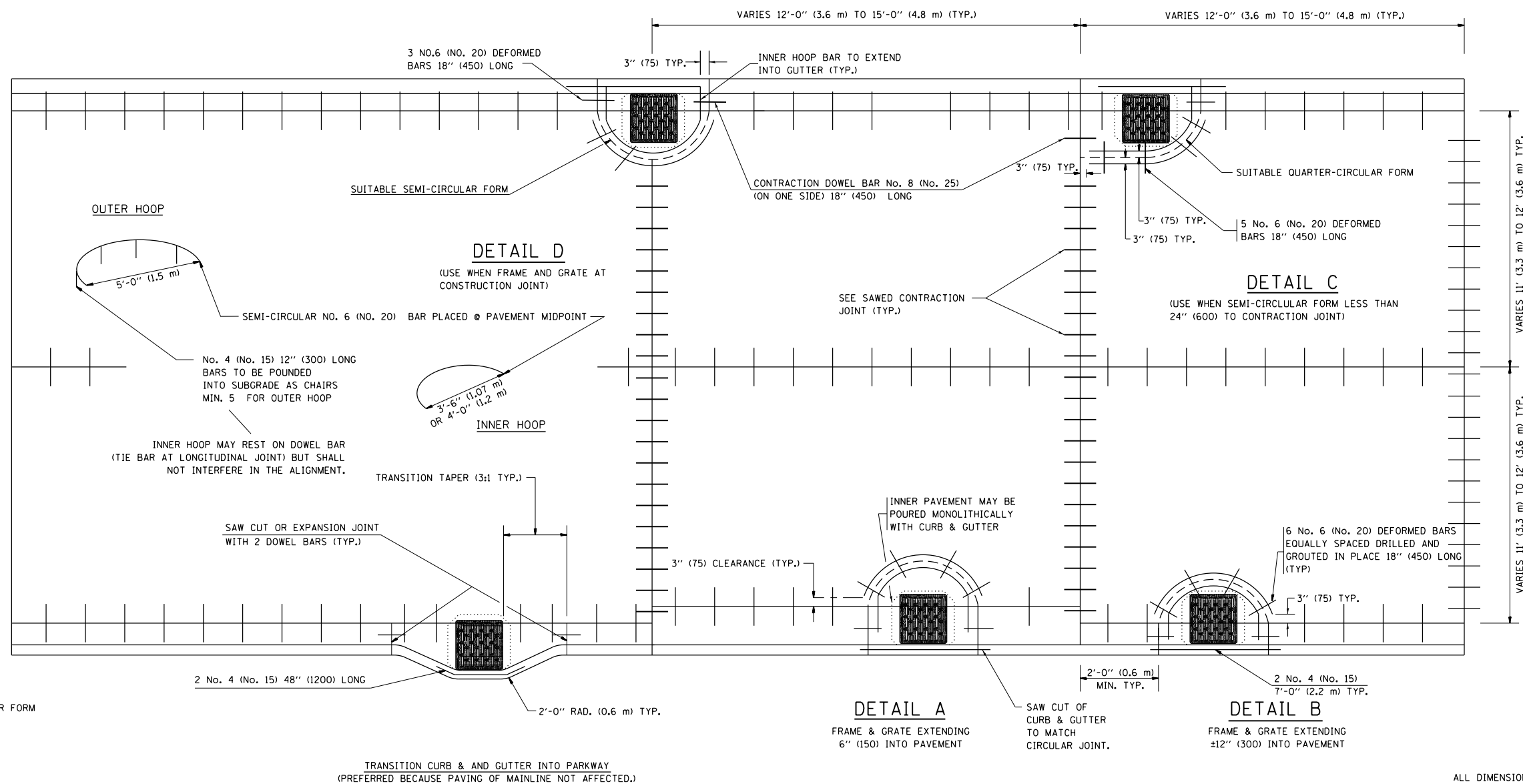
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		CITY OF CHICAGO CATCH BASIN, INLET AND MANHOLE DETAILS		F.A.I. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE		SHEET NO. 6 OF 31 SHEETS		2013-007R		COOK	317	238
		STA. TO STA.		BD600-13 (BD47)		CONTRACT NO. 60W25		
				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
				90/94/290				

FRAME EXTENSION INTO PAVEMENT	INNER HOOP REINFORCEMENT DIAMETER	SEMI CIRCULAR FORM DIAMETER	OUTER HOOP REINFORCEMENT DIAMETER
UP TO 8" (200)	3'-6" (1.1 m)	4'-0" (1.2 m)	5'-0" (1.5 m)
> 8" (200) TO 14" (360)	4'-0" (1.2 m)	4'-6" (1.4 m)	5'-0" (1.5 m)

DESIGNER NOTE:
THIS DETAIL IS TO BE USED
WHEN THE GUTTER FLAG IS
LESS THAN 24"

NOTES :

1. THE ROUNDOUT AND ADDED REINFORCEMENT WILL NOT BE PAID SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PAVEMENT.
2. TRANSVERSE JOINTS MAY BE MOVED TO ACCOMMODATE ROUNDOUT. EDGE OF CIRCULAR JOINT SHALL BE MINIMUM 12" (300) FROM TRANSVERSE JOINT. RELOCATED TRANSVERSE JOINT SHALL BE CONTINUOUS FROM EDGE OF PAVEMENT TO EDGE OF PAVEMENT.
3. SEMI-CIRCULAR FORM SHALL BE REMOVED PRIOR TO DRILL AND GROUT OF TIE BARS.
4. ALL REINFORCED BARS SHALL BE EPOXY COATED.
5. DRILL AND GROUT IS PREFERRED, HOWEVER TIE BARS CAN BE POURED IN PLACE IF CLEARANCE IS PROVIDED TO OUTER EDGE OF FRAME. MINIMUM 2" (50) CLEARANCE.
6. WOOD SHIMS SHALL BE USED TO ADJUST ALL FRAMES. AFTER ADJUSTING MORTAR HAS CURED, THE WOOD SHIMS SHALL BE REMOVED AND THE VOIDS UNDER THE FRAMES FILLED WITH NON SHRINK GROUT.
7. HOOP REINFORCEMENT SHALL BE ONE PIECE CONSTRUCTION.
8. CIRCULAR FRAMES AND GRATES MAY BE SUBSTITUTED.
9. CURB DOWELS MUST BE PLACED LEVEL & TRUE TO ALLOW CONTRACTION MOVEMENT.



LEGEND:

- CASTING
- - - - - SUITABLE SEMI-CIRCULAR FORM

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED

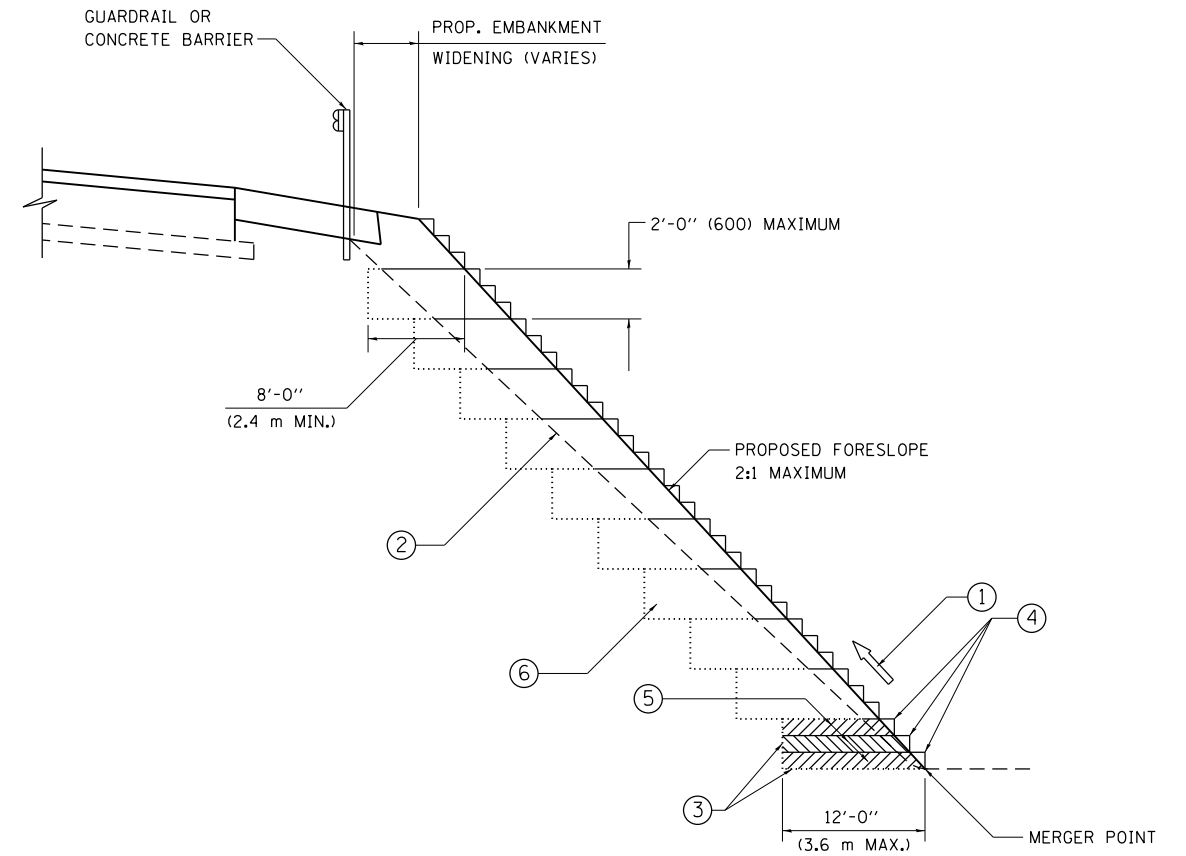
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		DATE - 01-04-99	REVISED - P. LAFLEUR 08-27-02

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PCC PAVEMENT ROUNDOUTS AT
CURB AND GUTTER

SCALE: NONE SHEET NO. 07 OF 31 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2013-007R	COOK	317	239
BD-48		CONTRACT NO. 60W25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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	PLOT DATE = 5/15/2013	DATE - 06-16-04	REVISED -

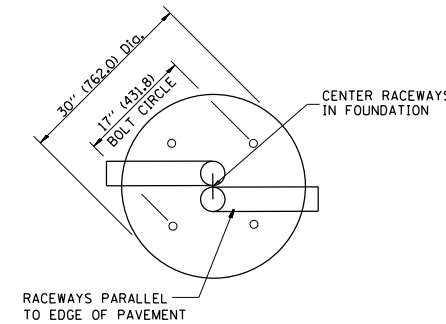
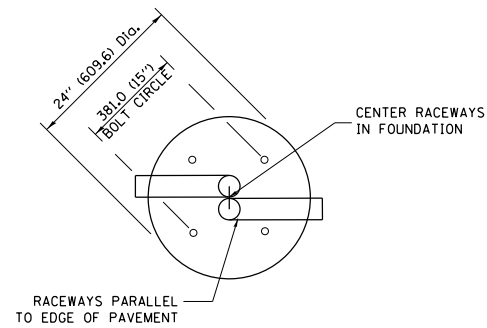
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BENCHING DETAIL FOR EMBANKMENT WIDENING	
SCALE: NONE	SHEET NO. 8 OF 31 SHEETS
STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2013-007R	COOK	317	240
BD-51			CONTRACT NO. 60W25	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LIGHT POLE FOUNDATION DEPTH TABLE
40 FT. (12.192 m) TO 47.5 FT. (14.478 m) MOUNTING HEIGHT

SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY Ou = 0.375 TON/SQ. FT.	13'-0" (3.96 m)	15'-0" (4.57 m)
MEDIUM CLAY Ou = 0.75 TON/SQ.FT	9'-6" (2.93 m)	10'-9" (3.23 m)
STIFF CLAY Ou = 1.50 TON/SQ. FT.	7'-0" (2.13 m)	8'-0" (2.44 m)
LOOSE SAND φ = 34°	9'-0" (2.74 m)	10'-0" (3.05 m)
MEDIUM SAND φ = 37.5°	8'-3" (2.52 m)	9'-0" (2.74 m)
DENSE SAND φ = 40°	7'-9" (2.36 m)	9'-0" (2.74 m)

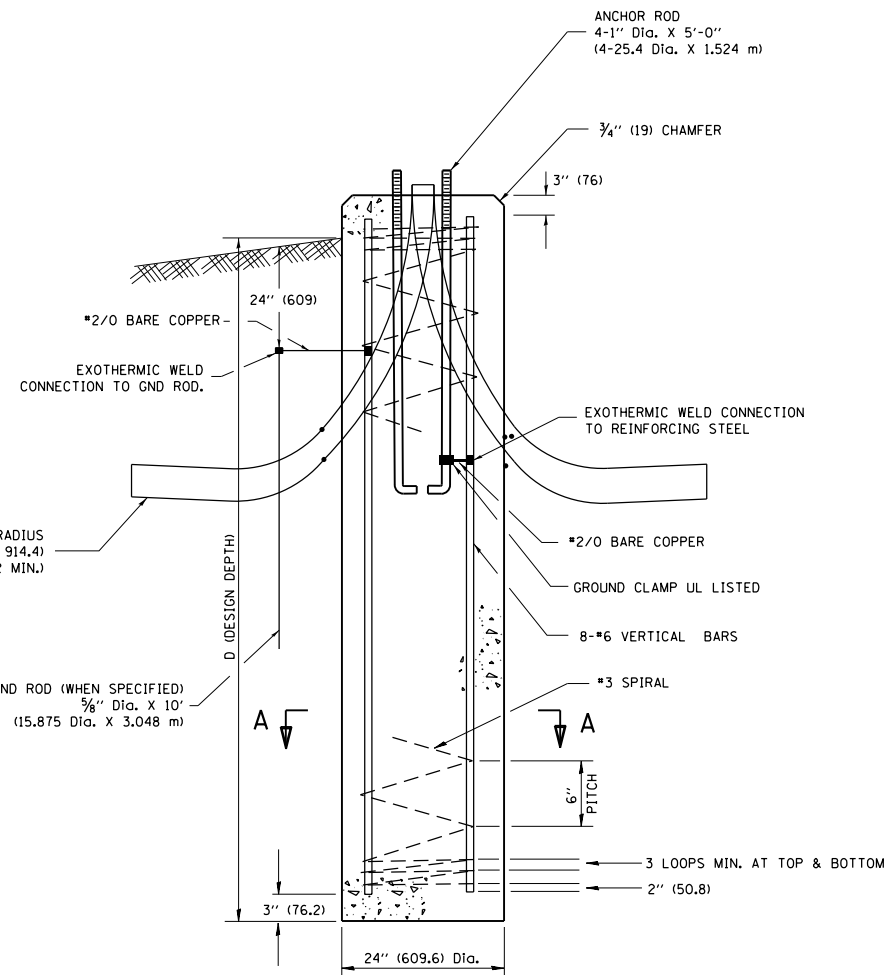


TOP VIEW

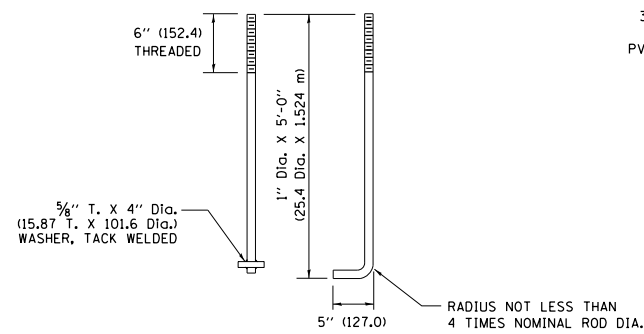
TOP VIEW

NOTES

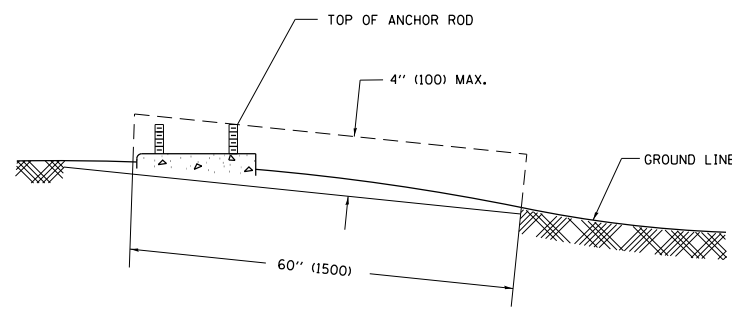
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 100MM (4 IN.) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS SI. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UMG (MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.



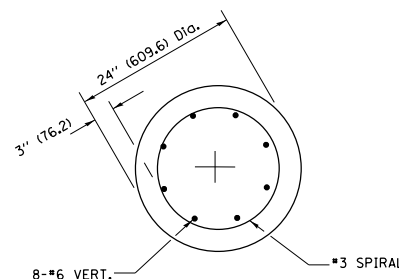
FOUNDATION DETAIL



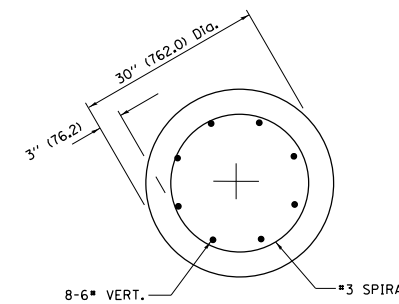
ANCHOR ROD DETAIL



FOUNDATION EXTENSION DETAIL



SECTION A-A



SECTION A-A

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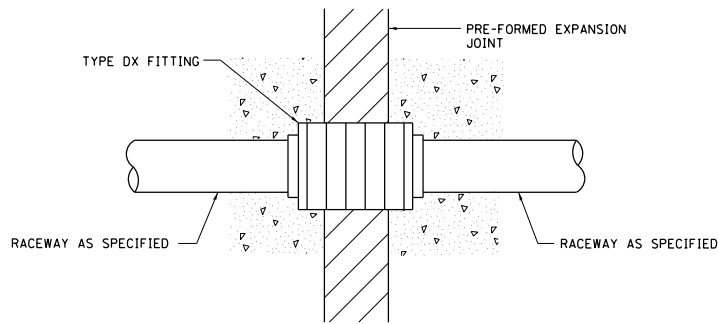
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LIGHT POLE FOUNDATION
40' (12.192 m) TO 47' 1/2' (14.478 m) M.H. 15" (381 mm) BOLT CIRCLE
SCALE: NONE SHEET NO. 9 OF 31 SHEETS STA. TO STA.

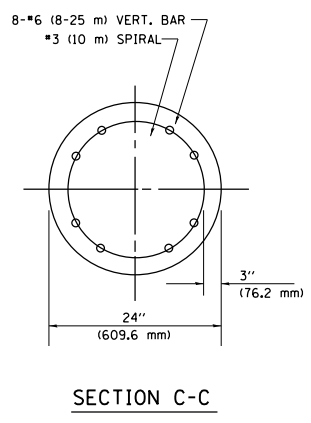
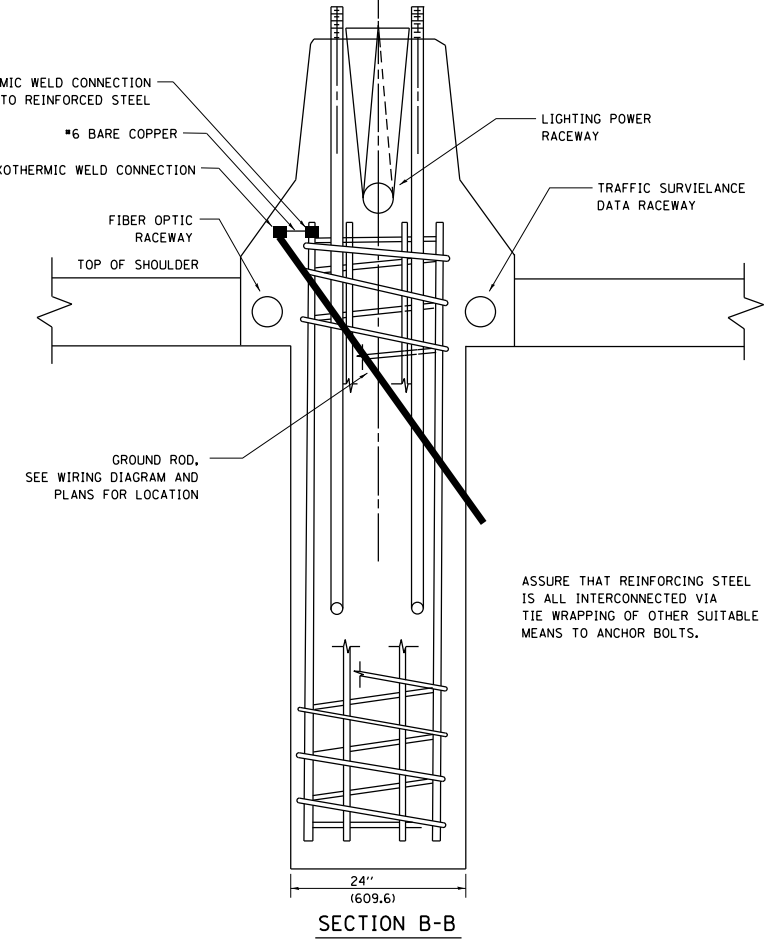
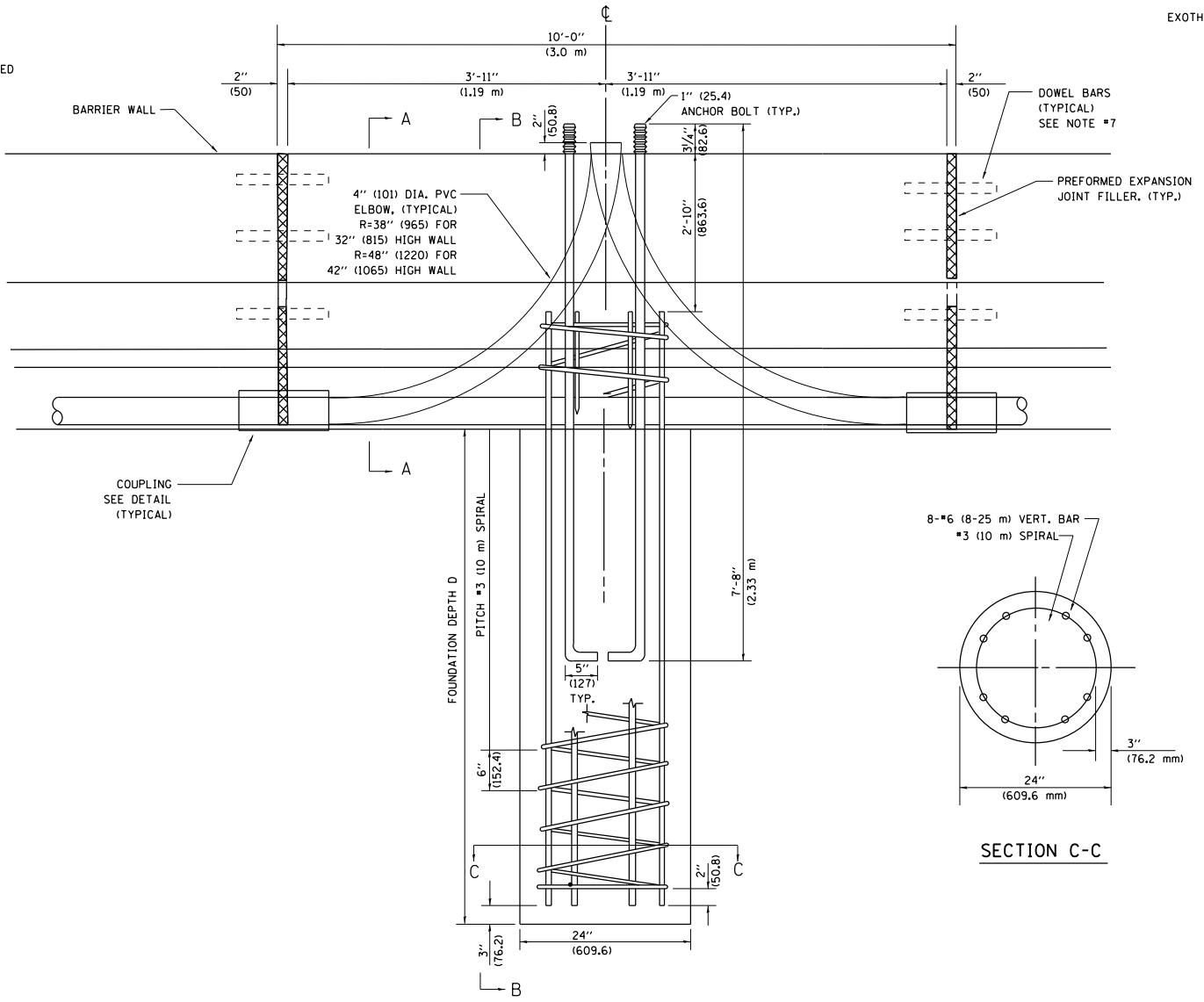
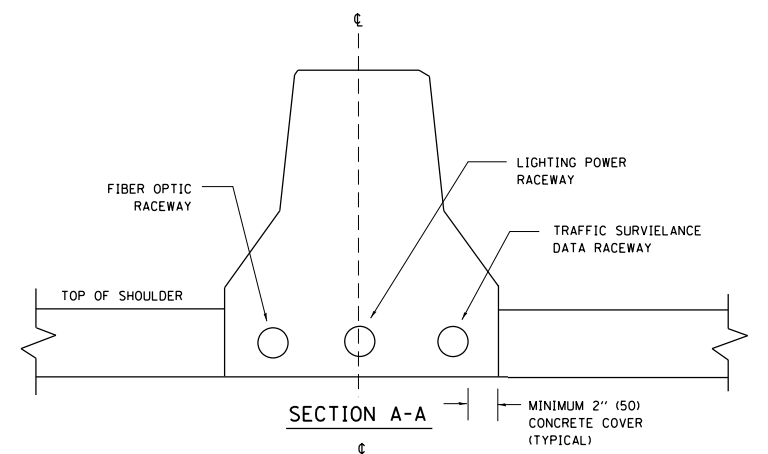
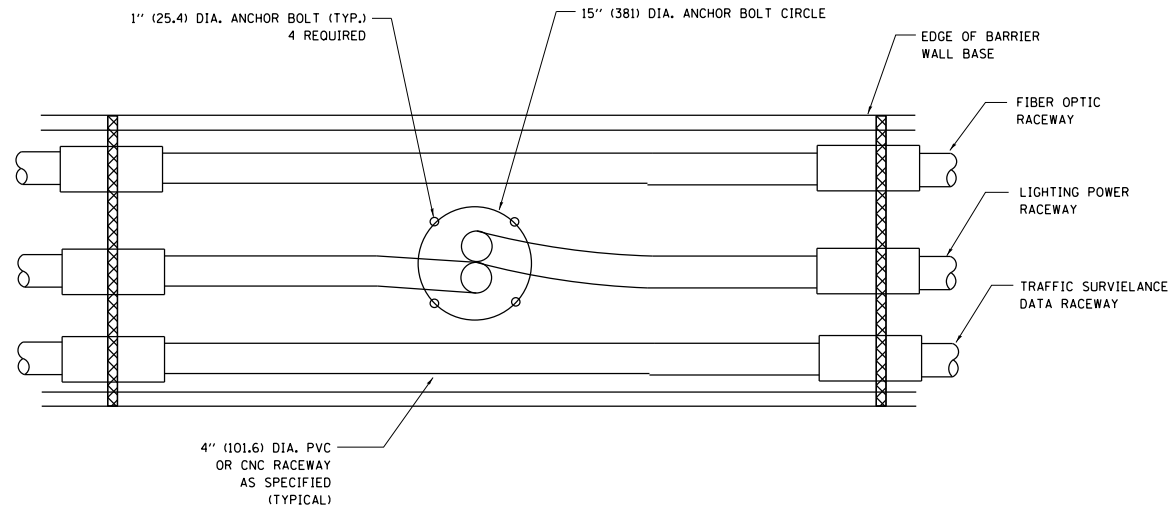
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2013-007R	COOK	317	241
BE-301		CONTRACT NO. 60W25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
90/94/290				

NOTES:

1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
2. THE ANCHOR BOLTS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED IN THE FORMS.
3. THE CONTRACTOR AT HIS OPTION MAY SUBSTITUTE #4 (15 m) TIES AT 12" (304.8 m) CTRS. FOR THE #3 (3 m) SPIRAL, TACKWELDED TYPE BOLT MAY BE SUBSTITUTED FOR THE HOOK TYPE BOLT.
4. COLD BENDING OF THE HOOK BOLT SHALL NOT BE ALLOWED.
5. EXCAVATION FOR THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER 24 INCHES (609.6 m) IN DIAMETER.
6. THE ENGINEER SHALL DETERMINE THE TYPE OF SOIL DURING EXCAVATION AND SELECT THE DESIGN DEPTH OF FOUNDATION FROM THE DESIGN TABLE USING THE DOMINANT CHARACTERISTIC OF THE SOIL ENCOUNTERED.
7. BARRIER WALL EXPANSION AND CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH STANDARD DETAIL 637001-02 AND 637006 AS APPLICABLE.



CONDUIT DEFLECTION FITTING DETAIL



FOUNDATION DEPTH

TYPE OF SOIL	FOUND FEPTH D	REINFORCEMENT IN FOUNDATION	
		VERTICAL BARS	SPIRAL
ROCK OR SOLIDIFIED SLAG	5'-0" (1.52 m)	NONE	NONE
DENSE SAND	7'-9" (2.36 m)	8-#6 x 9'-0" (8-20 m x 2.74 m)	#3 x 90' (3 m x 27.43 m)
MEDIUM SAND	8'-3" (2.51 m)	8-#6 x 9'-5" (8-20 m x 2.87 m)	#3 x 94' (3 m x 28.65 m)
LOOSE SAND	9'-0" (2.74 m)	8-#6 x 10'-2" (8-20 m x 3.09 m)	#3 x 100' (3 m x 30.48 m)
STIFF CLAY	7'-0" (2.13 m)	8-#6 x 10'-8" (8-20 m x 2.48 m)	#3 x 80' (3 m x 24.38 m)
MEDIUM CLAY	9'-6" (2.89 m)	8-#6 x 10'-8" (8-20 m x 3.25 m)	#3 x 104' (3 m x 31.69 m)
SOFT CLAY	13'-0" (3.96 m)	8-#6 x 14'-2" (8-20 m x 4.32 m)	#3 x 144' (3 m x 43.89 m)

DESIGN: 80 MPH AASHTO

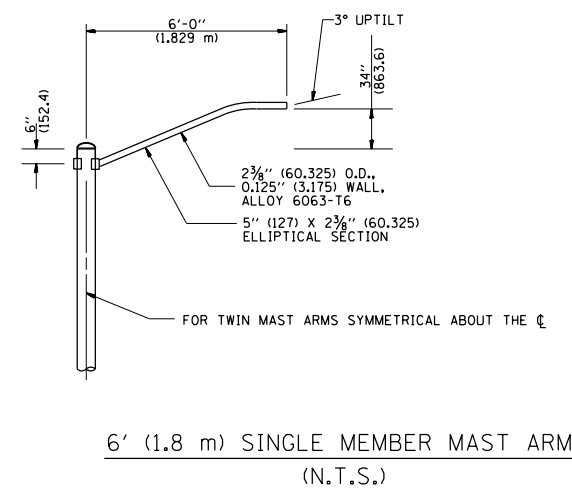
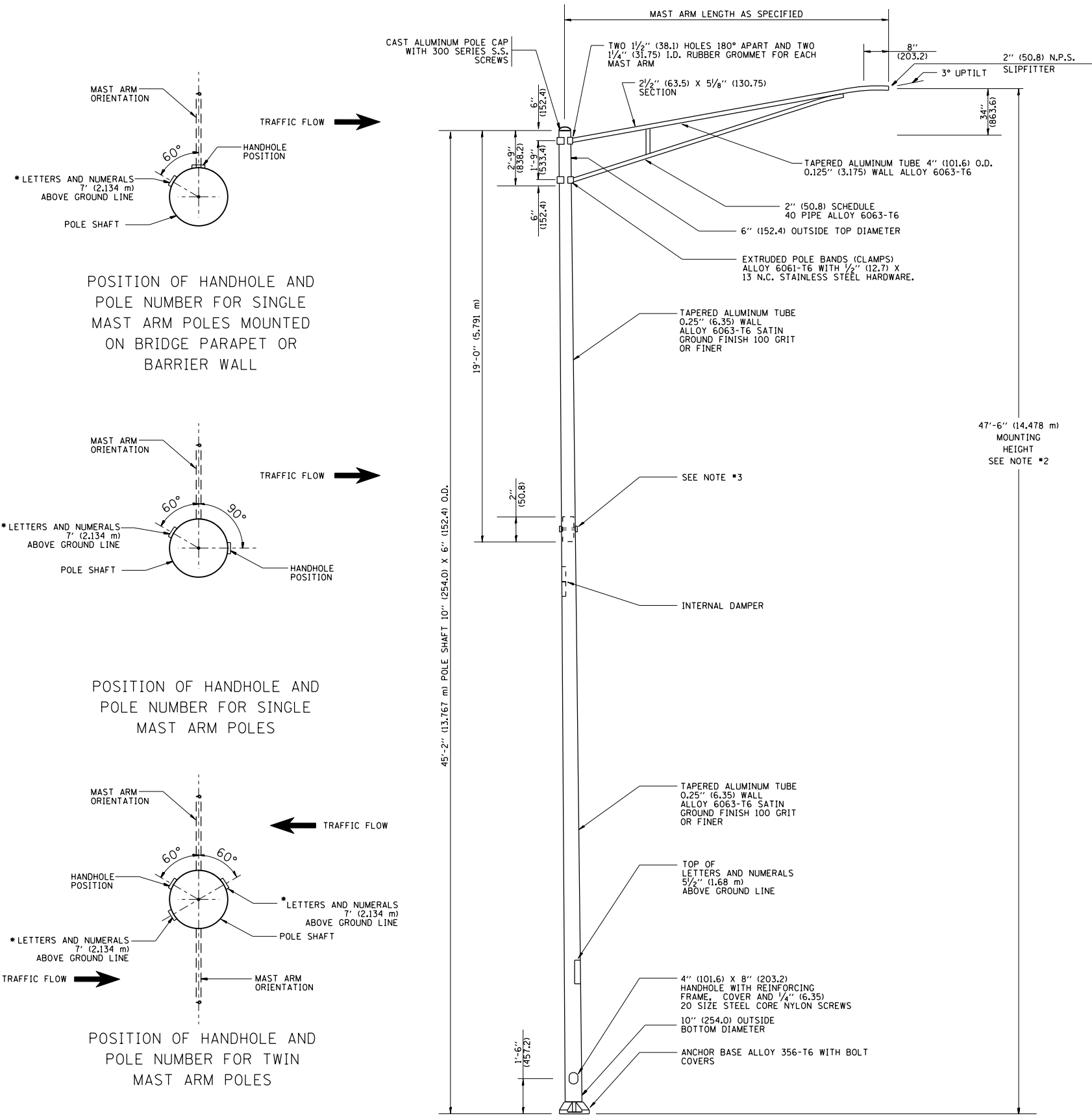
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		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

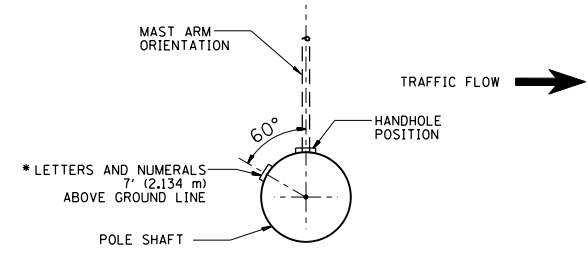
24" (609.6 mm) DIA. LIGHT POLE FOUNDATION
INTEGRAL WITH DOUBLE FACE BARRIER WALL

SCALE: NONE SHEET NO. 10 OF 31 SHEETS STA. TO STA.

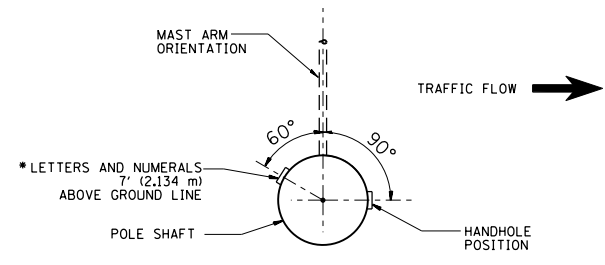
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2013-007R	COOK	317	242
	BE-322	CONTRACT NO.	60W25	



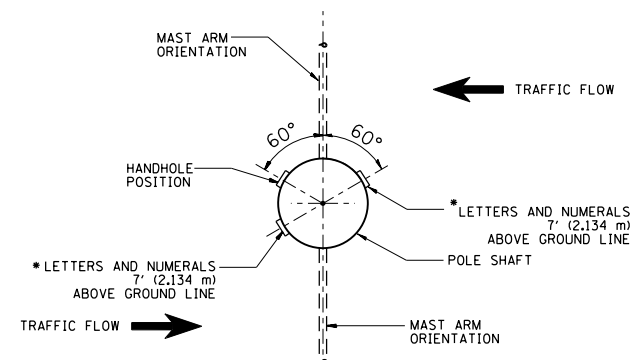
- NOTES:**
1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
 2. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENON TO THE BOTTOM OF THE ANCHOR BASE.
 3. TWO PIECE SHAFT WILL BE MATCHED MARKED AND INTERCHANGEABLE BETWEEN DIFFERENT UNITS. FIELD DRILLING OF THE HOLES WILL NOT BE ALLOWED.
 4. THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
 5. THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GROUNDING CONNECTOR, BURNDY K2C23, T&B SP4DL OR APPROVED EQUAL.
 6. LIGHT POLES WILL NOT BE INSTALLED WITHOUT MAST ARMS AND LUMINAIRES.
 7. LIGHT POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.
 8. LIGHTING UNIT IDENTIFICATION NUMBERS SHALL BE INSTALLED BEFORE THE LIGHTING UNIT IS ENERGIZED.



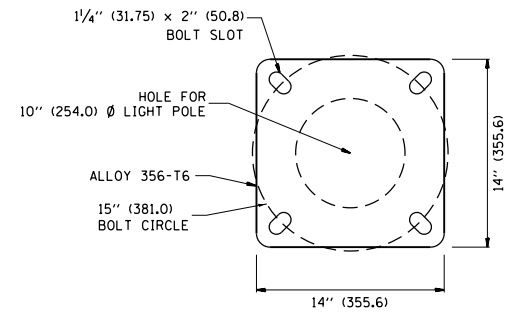
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES MOUNTED ON BRIDGE PARAPET OR BARRIER WALL



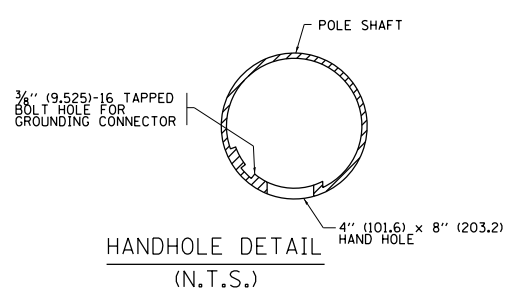
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES

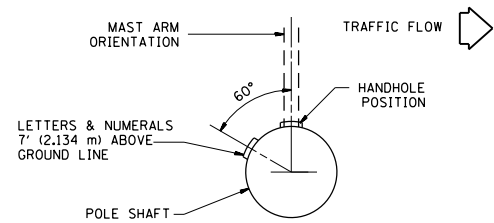


POSITION OF HANDHOLE AND POLE NUMBER FOR TWIN MAST ARM POLES

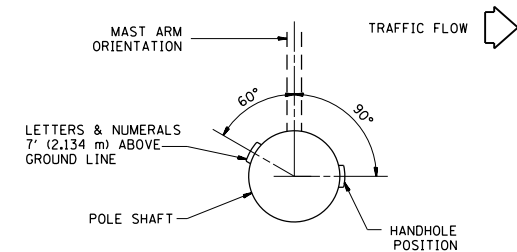


LIGHT POLE BASE PLATE DETAIL
15 INCH (381.0) BOLT CIRCLE

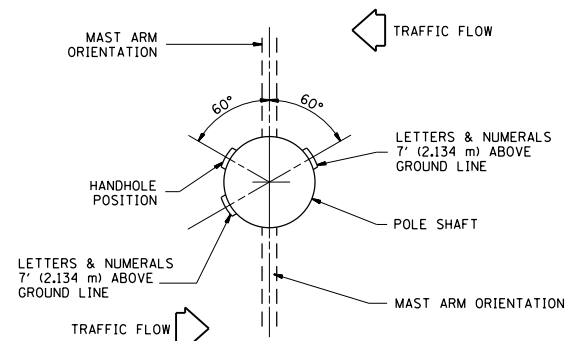




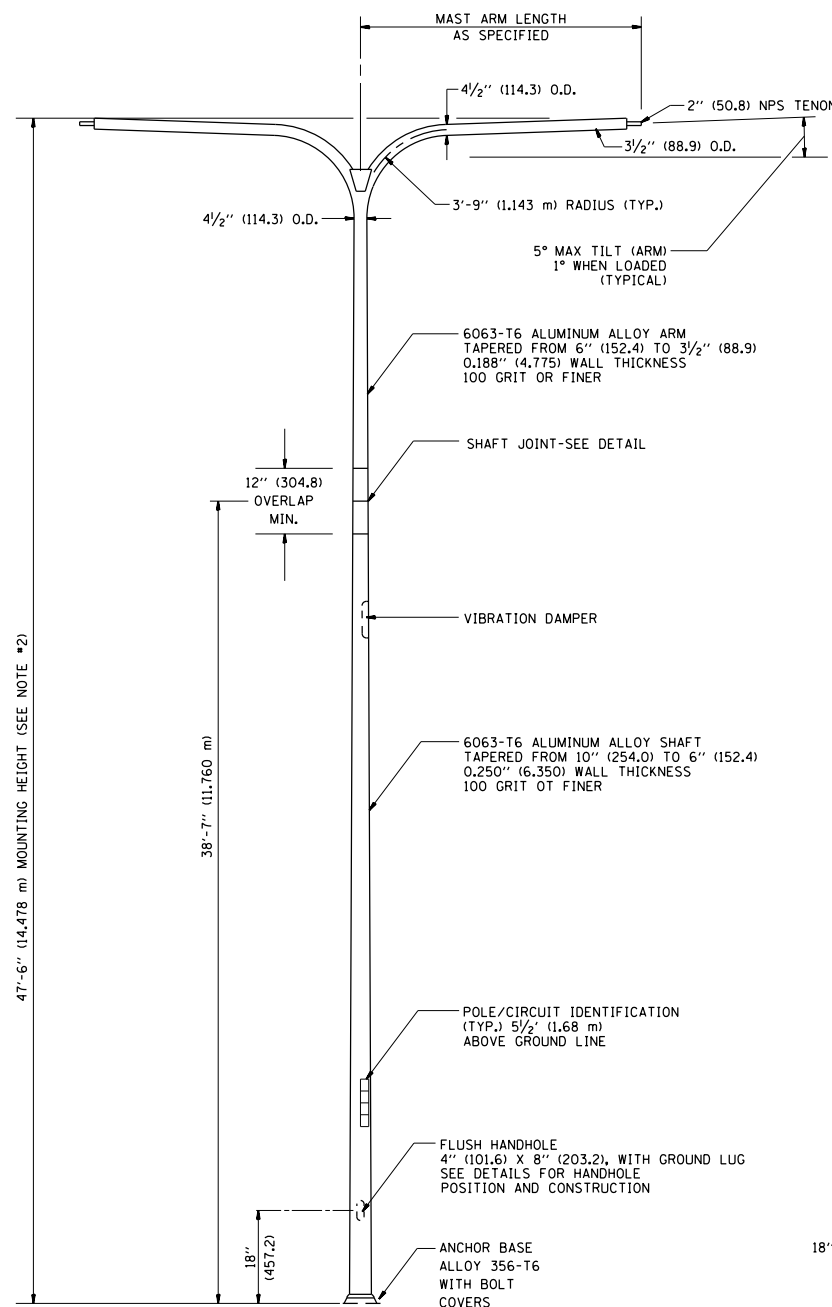
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES MOUNTED ON BRIDGE PARAPET OR BARRIER WALL



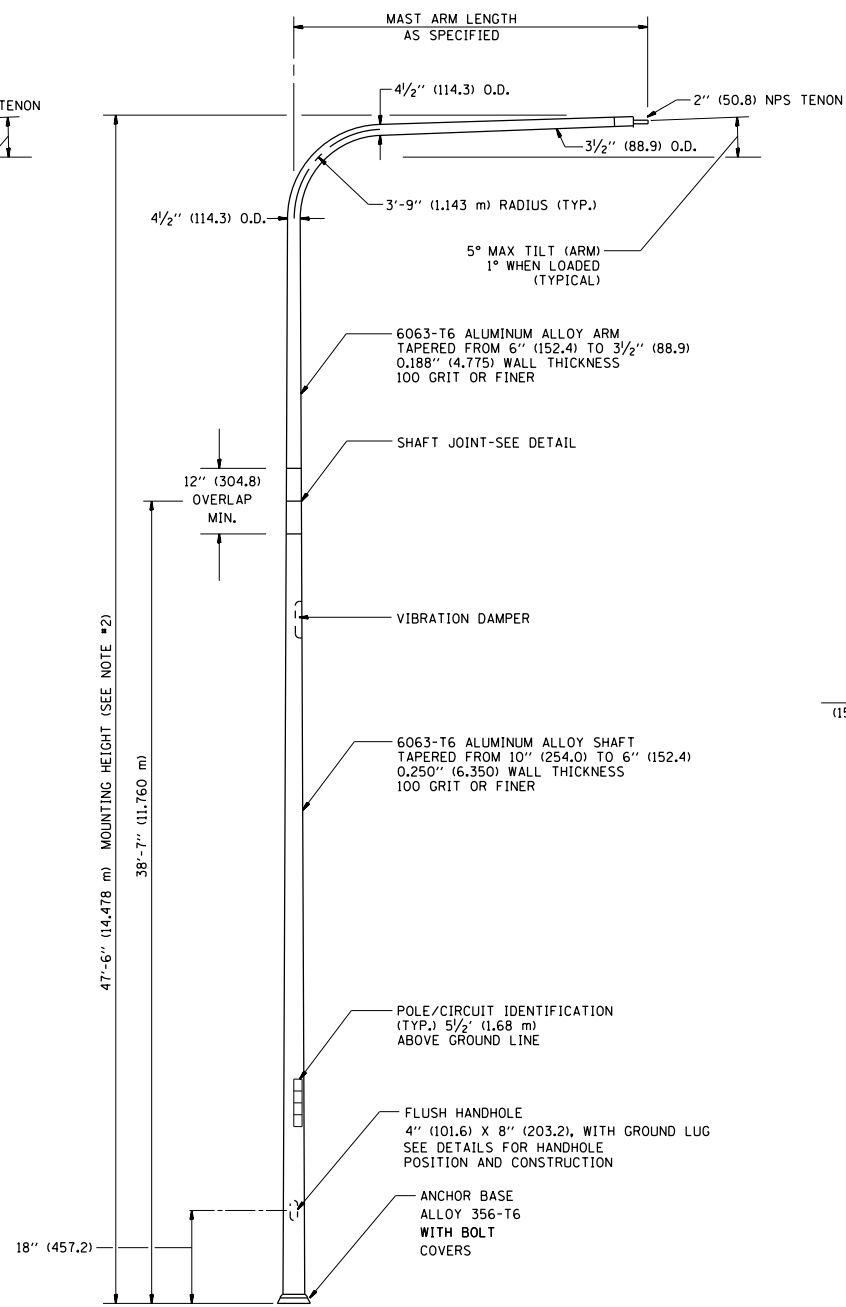
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES



POSITION OF HANDHOLE AND POLE NUMBER FOR TWIN MAST ARM POLES

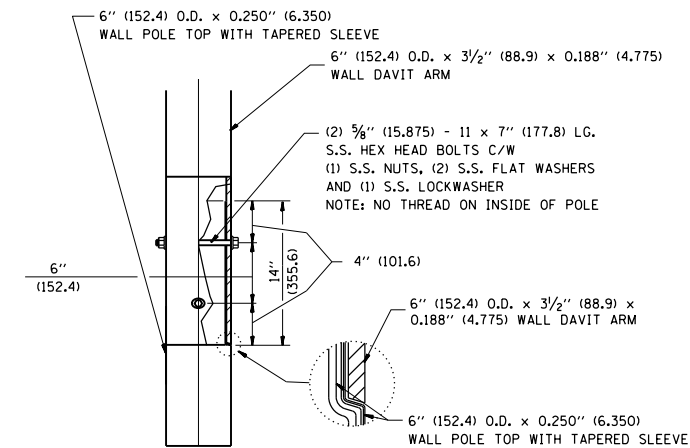


TWIN ARM POLE

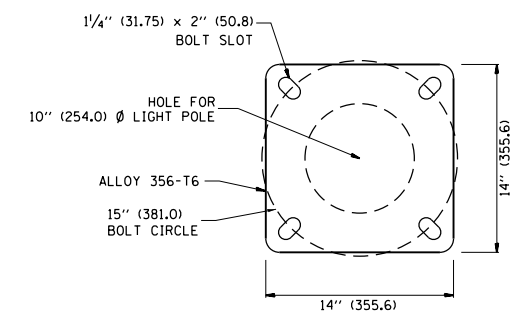


SINGLE ARM POLE

- NOTES:
1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
 2. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENON TO THE BOTTOM OF THE ANCHOR BASE.
 3. TWO PIECE SHAFT WILL BE MATCHED MARKED AND INTERCHANGEABLE BETWEEN DIFFERENT UNITS. FIELD DRILLING OF THE HOLES WILL NOT BE ALLOWED.
 4. THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
 5. THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GROUNDING CONNECTOR, BURNDY K2C23, T&B SP40L OR APPROVED EQUAL.
 6. LIGHT POLES WILL NOT BE INSTALLED WITHOUT MAST ARMS AND LUMINAIRES.
 7. LIGHT POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.
 8. LIGHTING UNIT IDENTIFICATION NUMBERS SHALL BE INSTALLED BEFORE THE LIGHTING UNIT IS ENERGIZED.

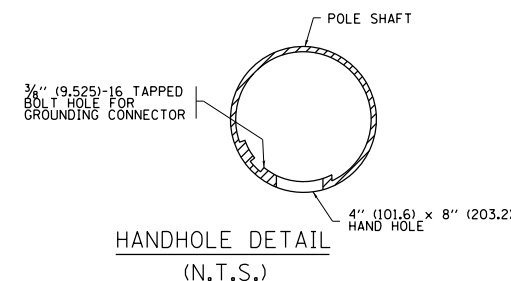


DAVIT ARM CONNECTION
[14" (355.6) OVERLAP SHOWN]



LIGHT POLE BASE PLATE DETAIL

(FOR POLE MOUNTED ON 15 INCH (381.0) BOLT CIRCLE FOUNDATION)

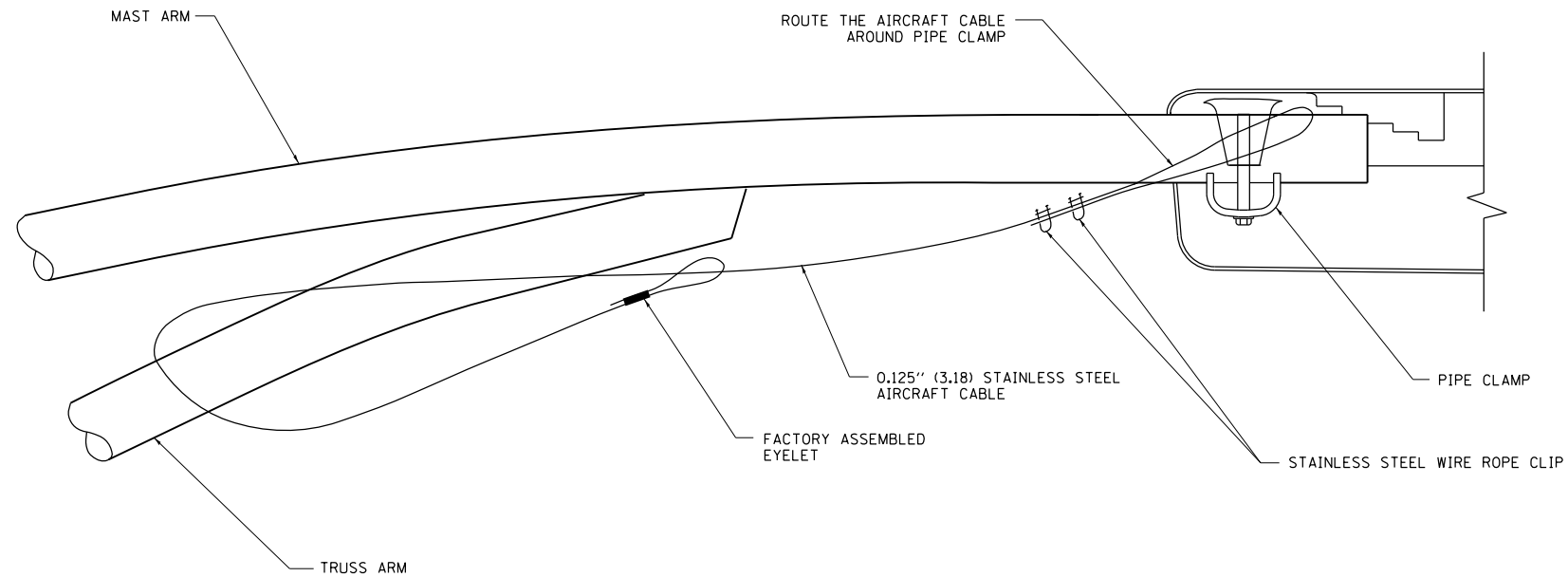


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		DATE -	REVISED - R. TOMSONS 09-02-03

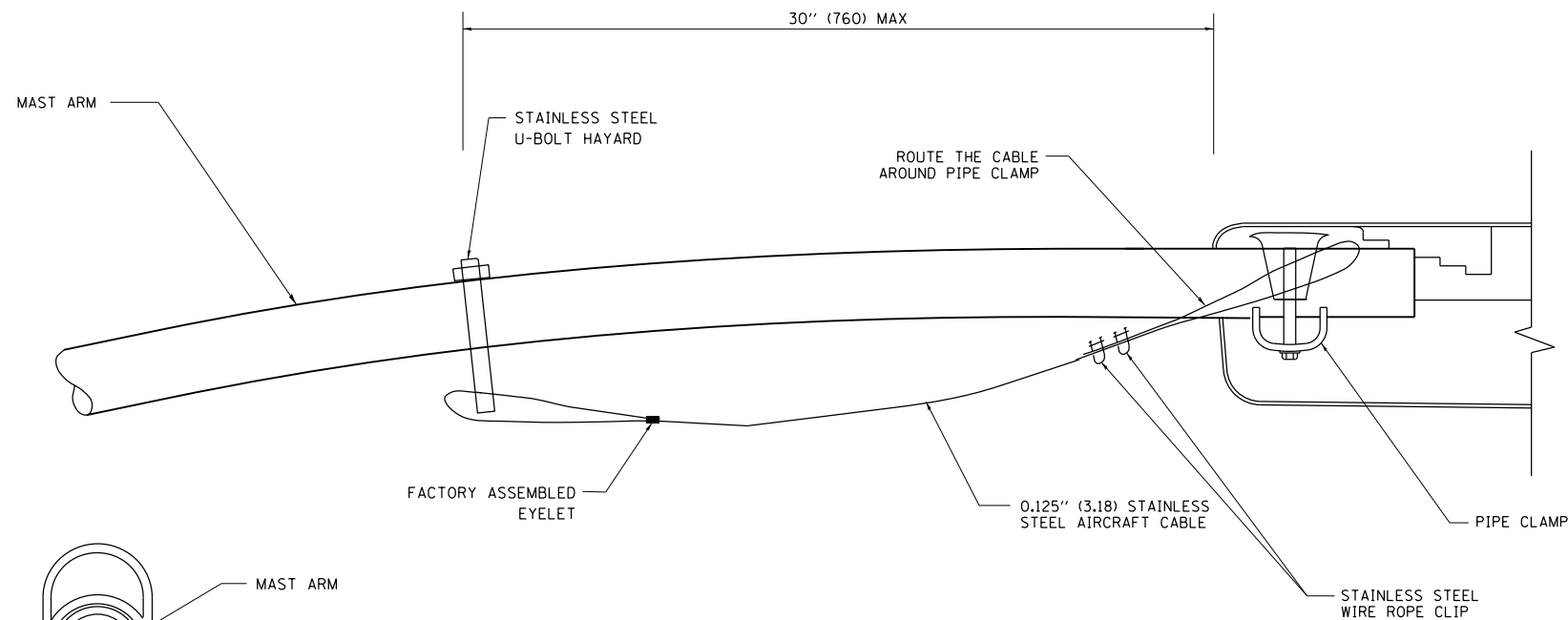
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DAVIT LIGHT POLE			
47'-6" (14.478 m) MOUNTING HEIGHT			
SCALE: NONE	SHEET NO. 12 OF 31 SHEETS	STA.	TO STA.

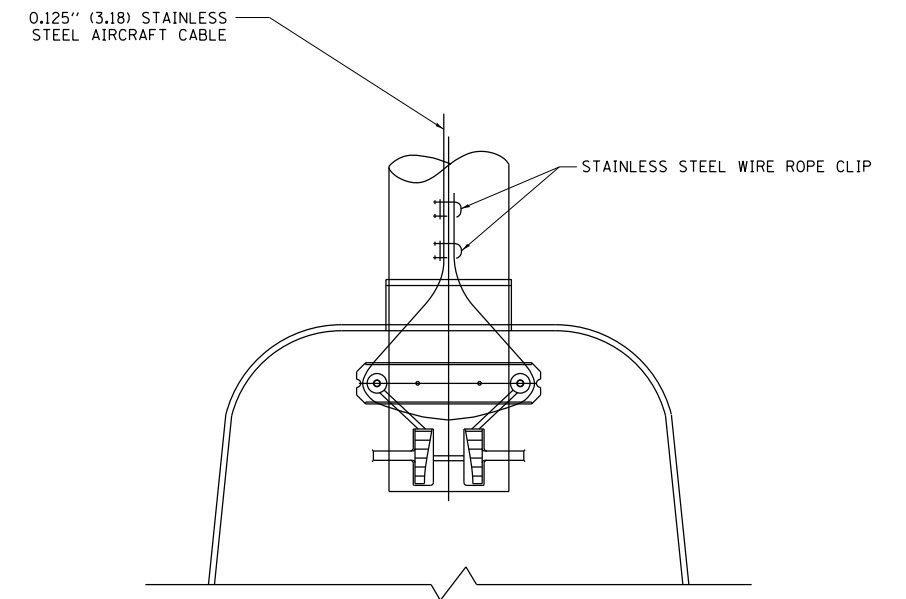
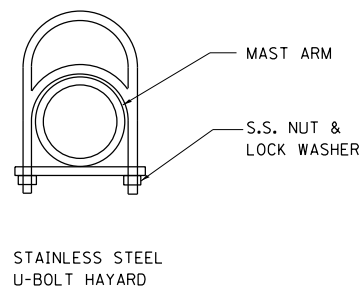
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2013-007R	COOK	317	244
BE-410		CONTRACT NO. 60W25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
9/94/290				



SIDE VIEW (TRUSS ARM)
N.T.S.



SIDE VIEW (SINGLE MEMBER OR DAVIT ARM)
N.T.S.



BOTTOM VIEW
N.T.S.

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.
2. CONTRACTOR SHALL ADJUST THE WIRE CLIP TO ELIMINATE ANY SLACK FROM THE WIRE ROPE.
3. THE 0.125" (3.18) STAINLESS STEEL AIRCRAFT CABLE SHALL REMAIN VISIBLE FROM THE GROUND LEVEL.
4. THE BREAKING STRENGTH OF THE CABLE SHALL BE 1700 LBS. MIN.

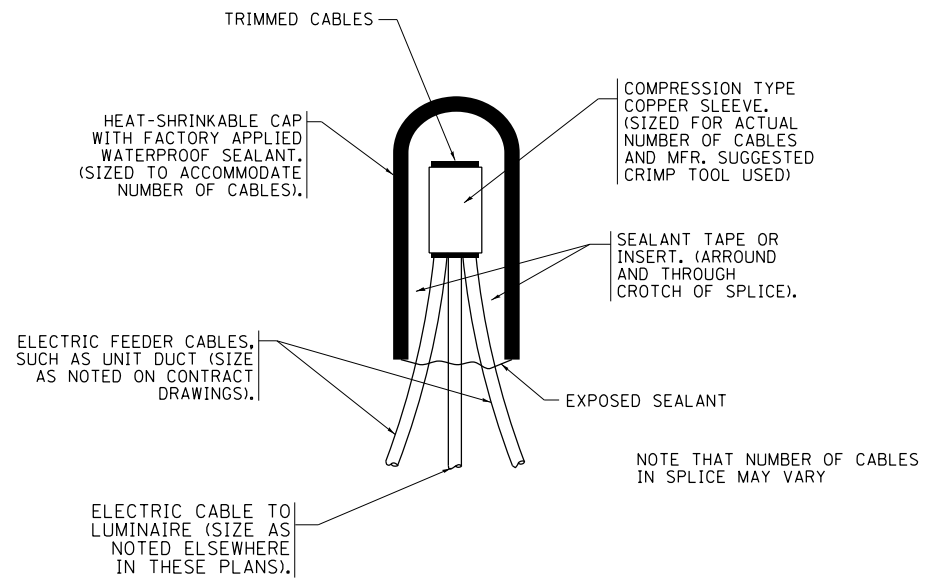
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	PLOT SCALE = 50.000' / in.	DATE -	REVISED -
	PLOT DATE = 5/15/2013		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

LUMINAIRE SAFETY CABLE ASSEMBLY

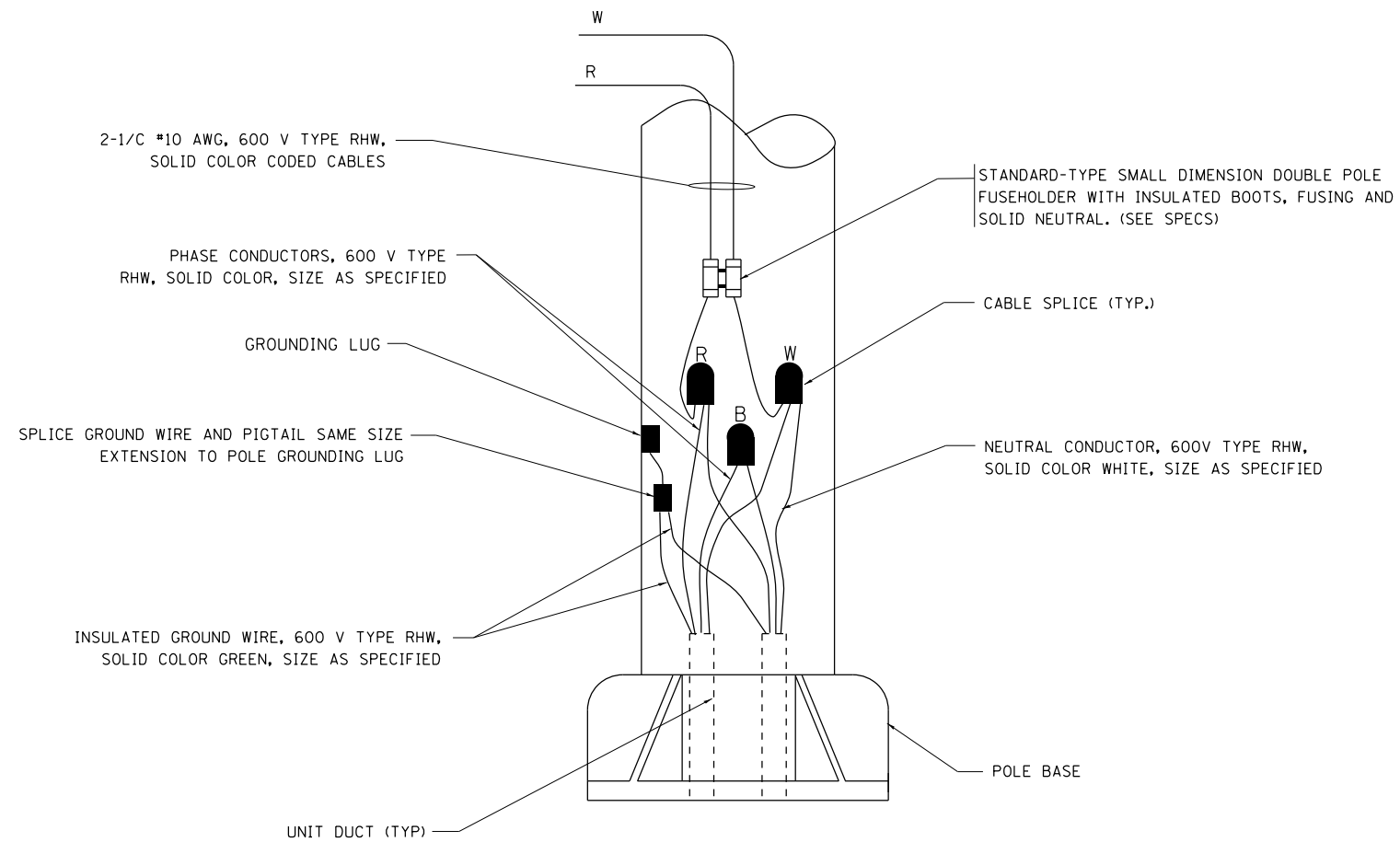
SCALE: NONE SHEET NO. 13 OF 31 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2013-007R	COOK	317	245
BE-701		CONTRACT NO. 60W25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



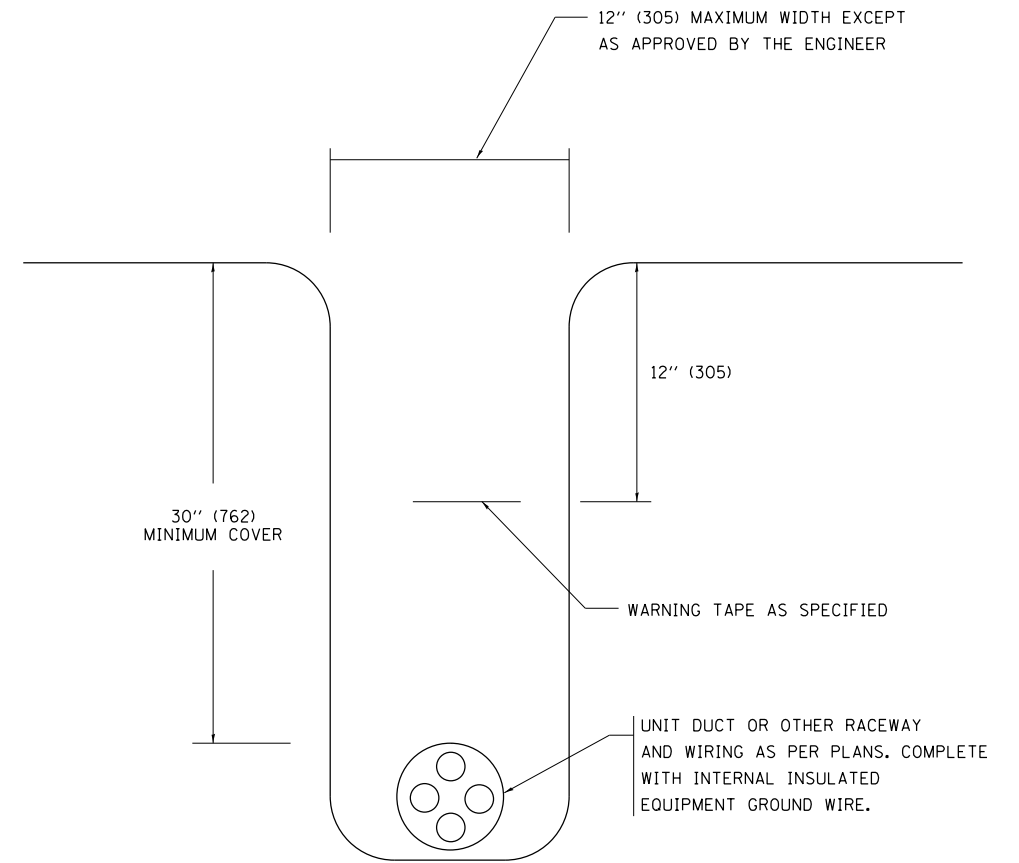
TYPICAL SPLICE DETAIL

N.T.S.



POLE WIRING DETAIL

N.T.S.



TYPICAL WIRING IN TRENCH DETAIL

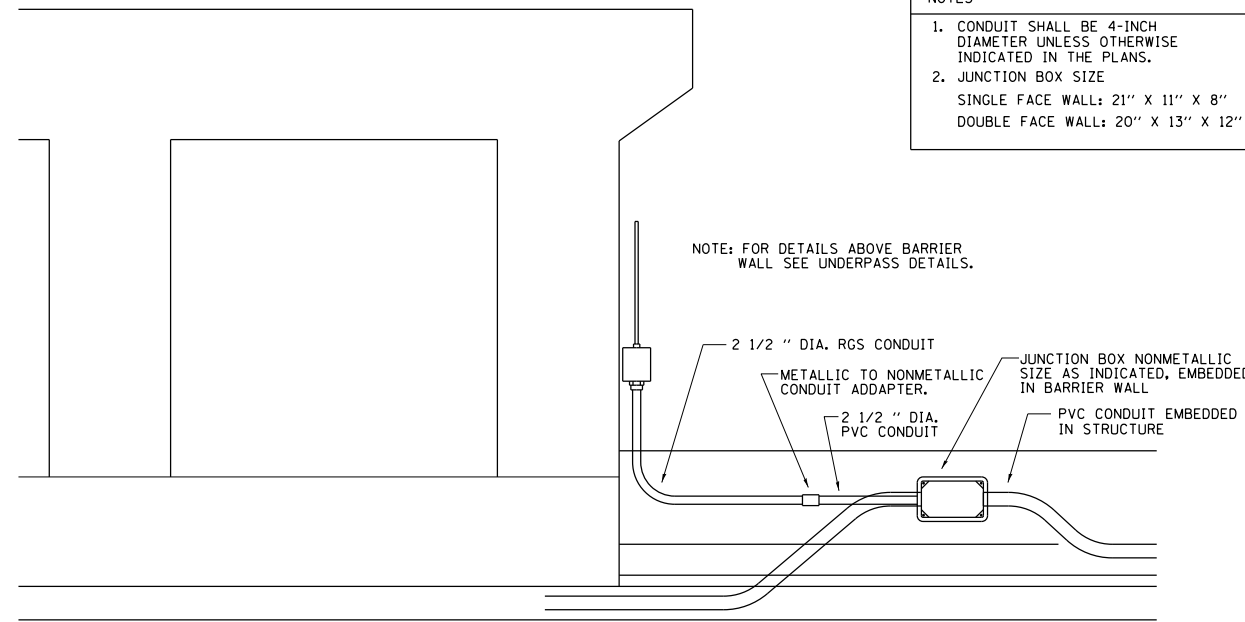
N.T.S.

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PLOT SCALE = 50.000' / in.	CHECKED -	DATE -	REVISED -
PLOT DATE = 5/15/2013	DATE -	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

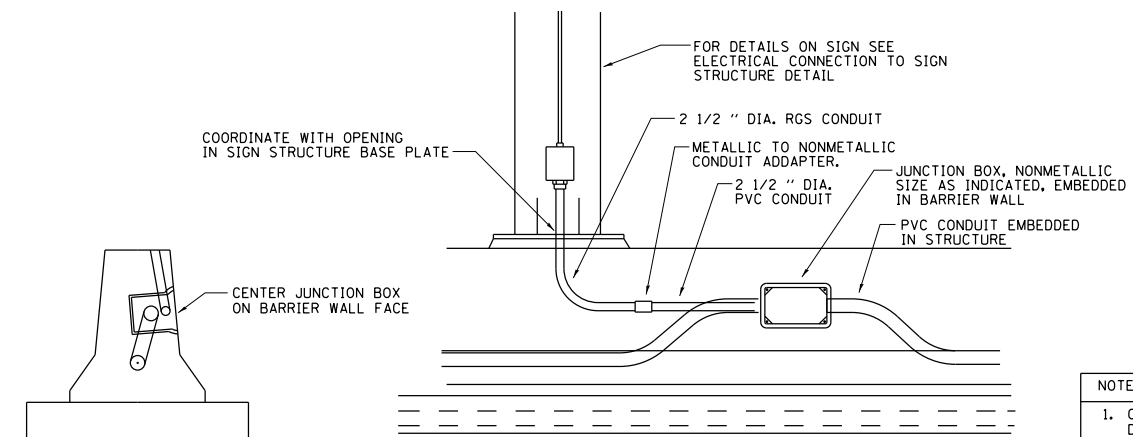
MISC. ELECTRICAL DETAILS			
SHEET A			
SCALE: NONE	SHEET NO. 14 OF 31 SHEETS	STA.	TO STA.

F.A.I. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2013-007R	COOK	317	246
BE-702		CONTRACT NO. 60W25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
• 90/94/290				



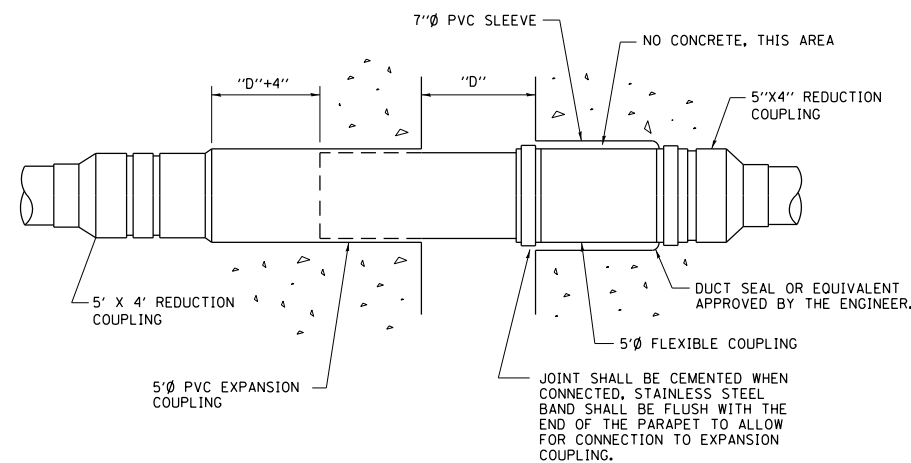
- NOTES
1. CONDUIT SHALL BE 4-INCH DIAMETER UNLESS OTHERWISE INDICATED IN THE PLANS.
 2. JUNCTION BOX SIZE
SINGLE FACE WALL: 21" X 11" X 8"
DOUBLE FACE WALL: 20" X 13" X 12"

ED - BWD
ELECTRIC CONNECTION TO UNDERPASS LIGHTING

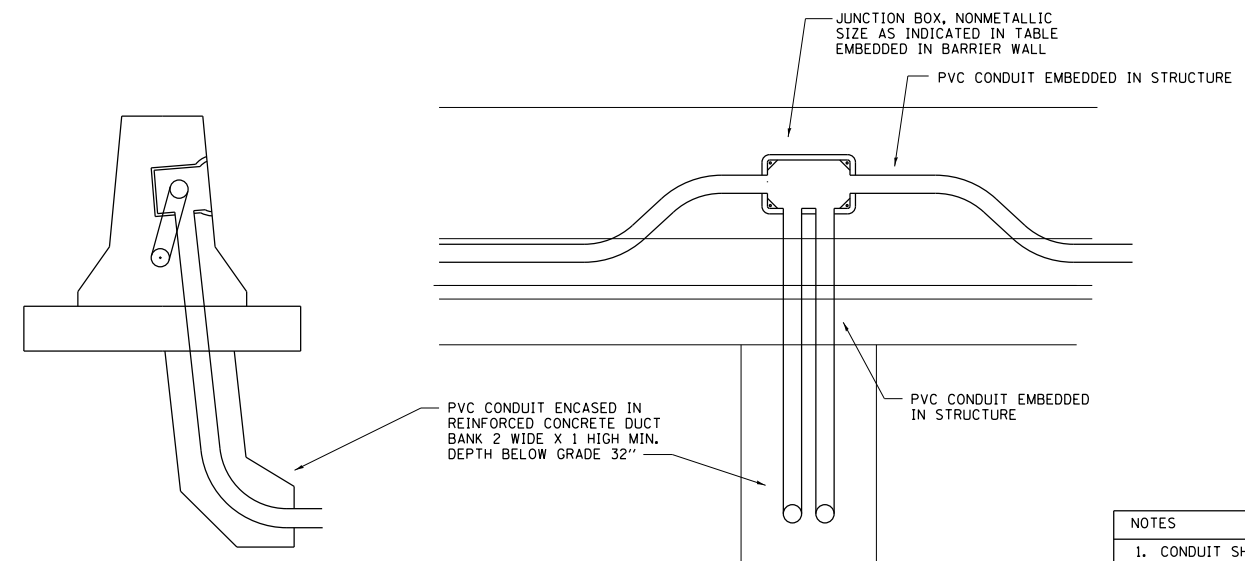


- NOTES
1. CONDUIT SHALL BE 4-INCH DIAMETER UNLESS OTHERWISE INDICATED IN THE PLANS.
 2. JUNCTION BOX SIZE
SINGLE FACE WALL: 21" X 11" X 8"
DOUBLE FACE WALL: 20" X 13" X 12"

ED - SGN
JUNCTION BOX EMBEDDED IN BARRIER WALL FOR SIGN LIGHTING



INSTALLATION OF CONDUIT
IN BRIDGE PARAPET EXPANSION JOINT
(N.T.S.)



- NOTES
1. CONDUIT SHALL BE 4-INCH DIAMETER UNLESS OTHERWISE INDICATED IN THE PLANS.
 2. JUNCTION BOX SIZE
SINGLE FACE WALL: 21" X 11" X 8"
DOUBLE FACE WALL: 20" X 13" X 12"

ED - BW
JUNCTION BOX EMBEDDED IN BARRIER WALL

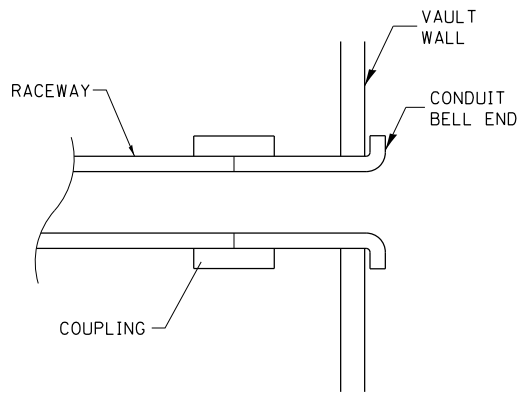
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	PLOT DATE = 5/15/2013	DATE - 01-20-2009	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

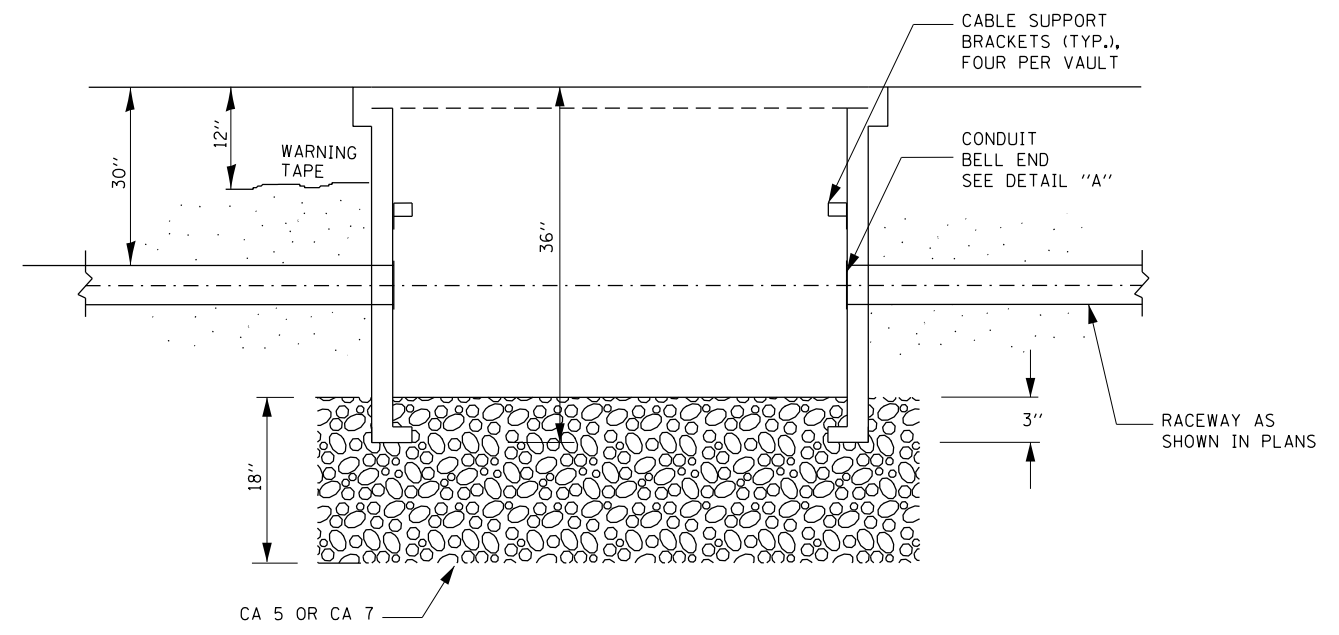
MISCELLANEOUS ELECTRICAL DETAILS, SHEET B			
J BOX EMBEDDED IN BARRIER WALL - INSTALLATION OF CONDUIT IN BRIDGE PARAPET EXPANSION JOINT - ELECTRIC CONNECTION TO UNDERPASS LIGHTING			
SCALE: NONE	SHEET NO. 15 OF 31 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2013-007R	COOK	317	247
BE-703		CONTRACT NO. 60W25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

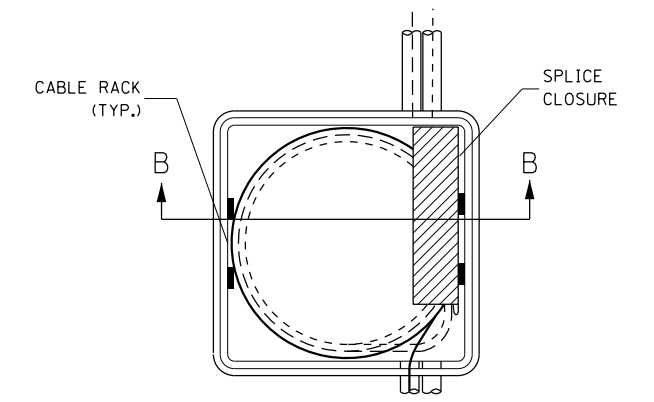
COMMUNICATIONS VAULT LOAD RATINGS			
COMPONENT	ANSI TIER	LOADING	
		DESIGN	TEST
BOX	22	22,500 lbs.	37,750 lbs.
COVER	22	22,500 lbs.	37,750 lbs.



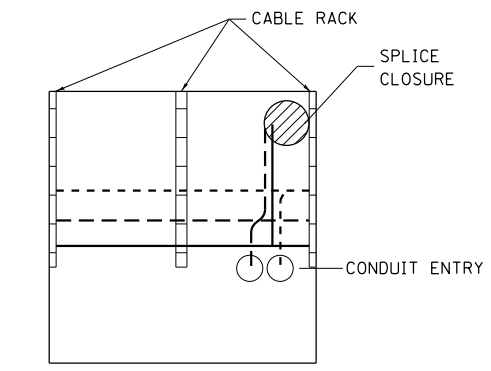
DETAIL A



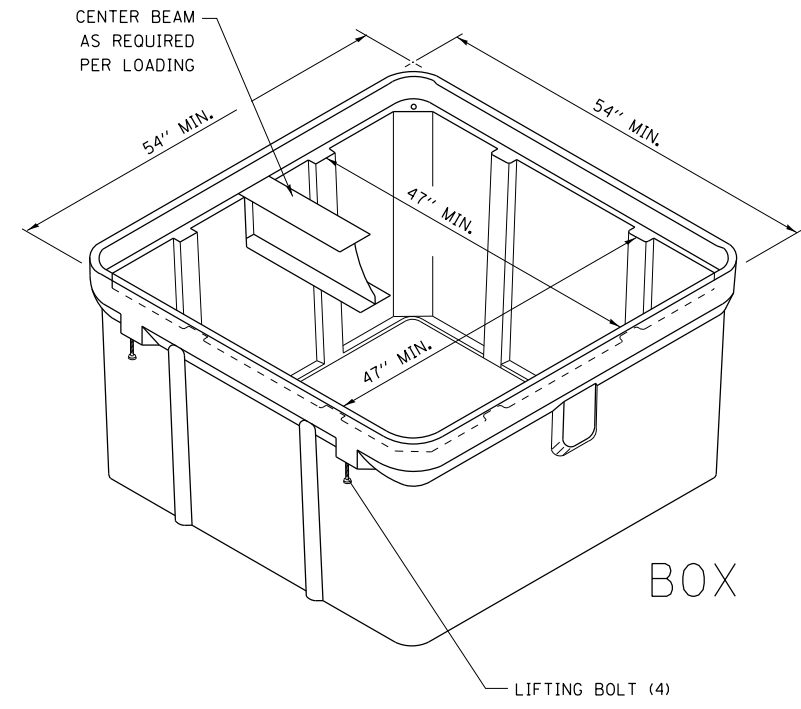
SECTION A-A



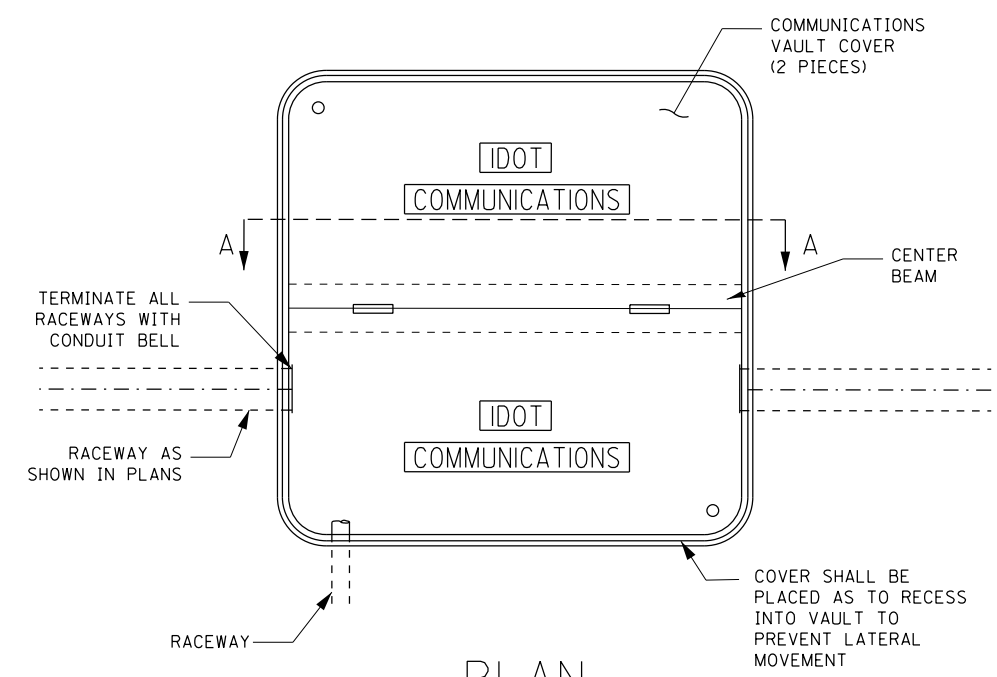
TOP VIEW



SECTION B-B



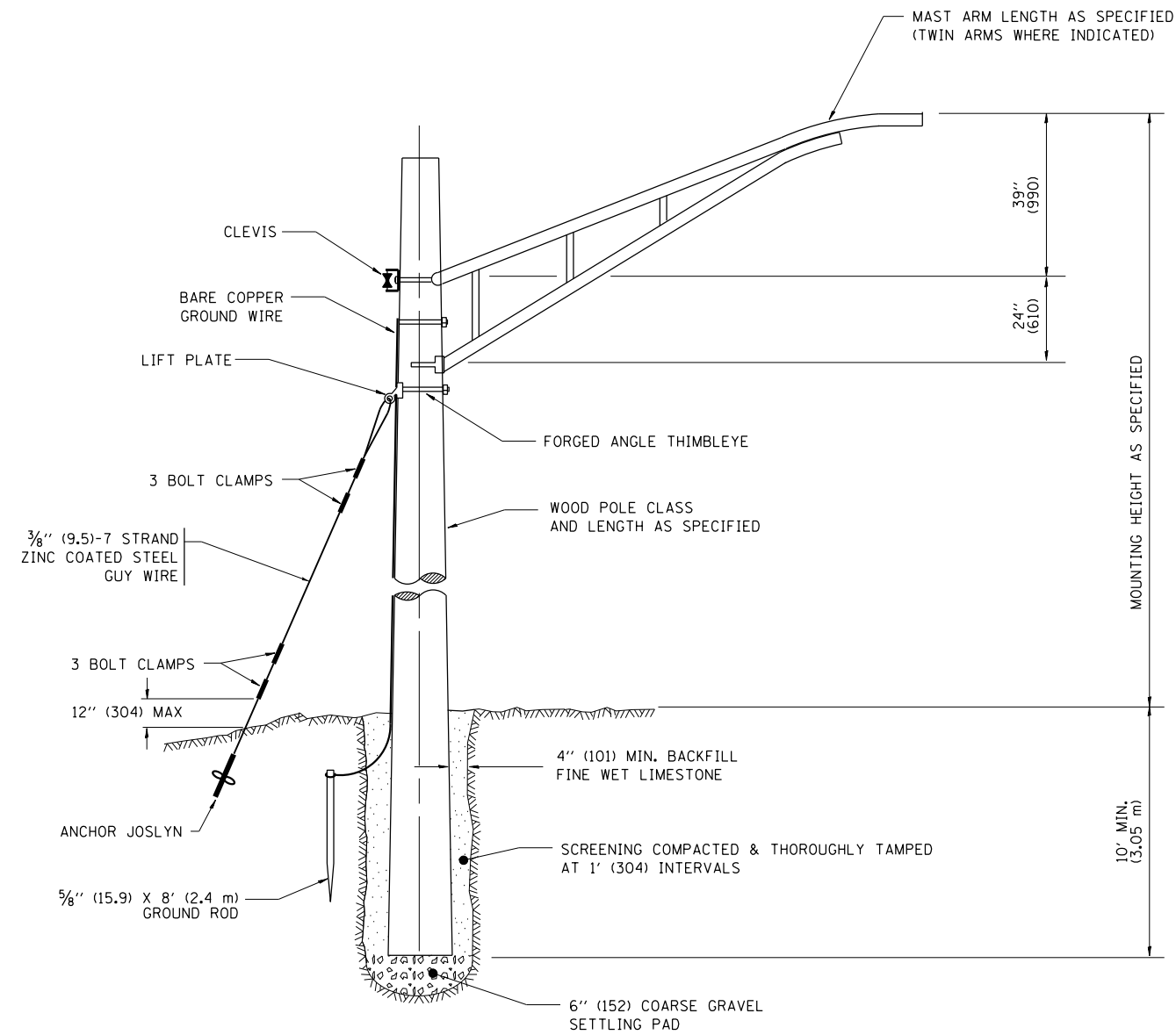
ISOMETRIC



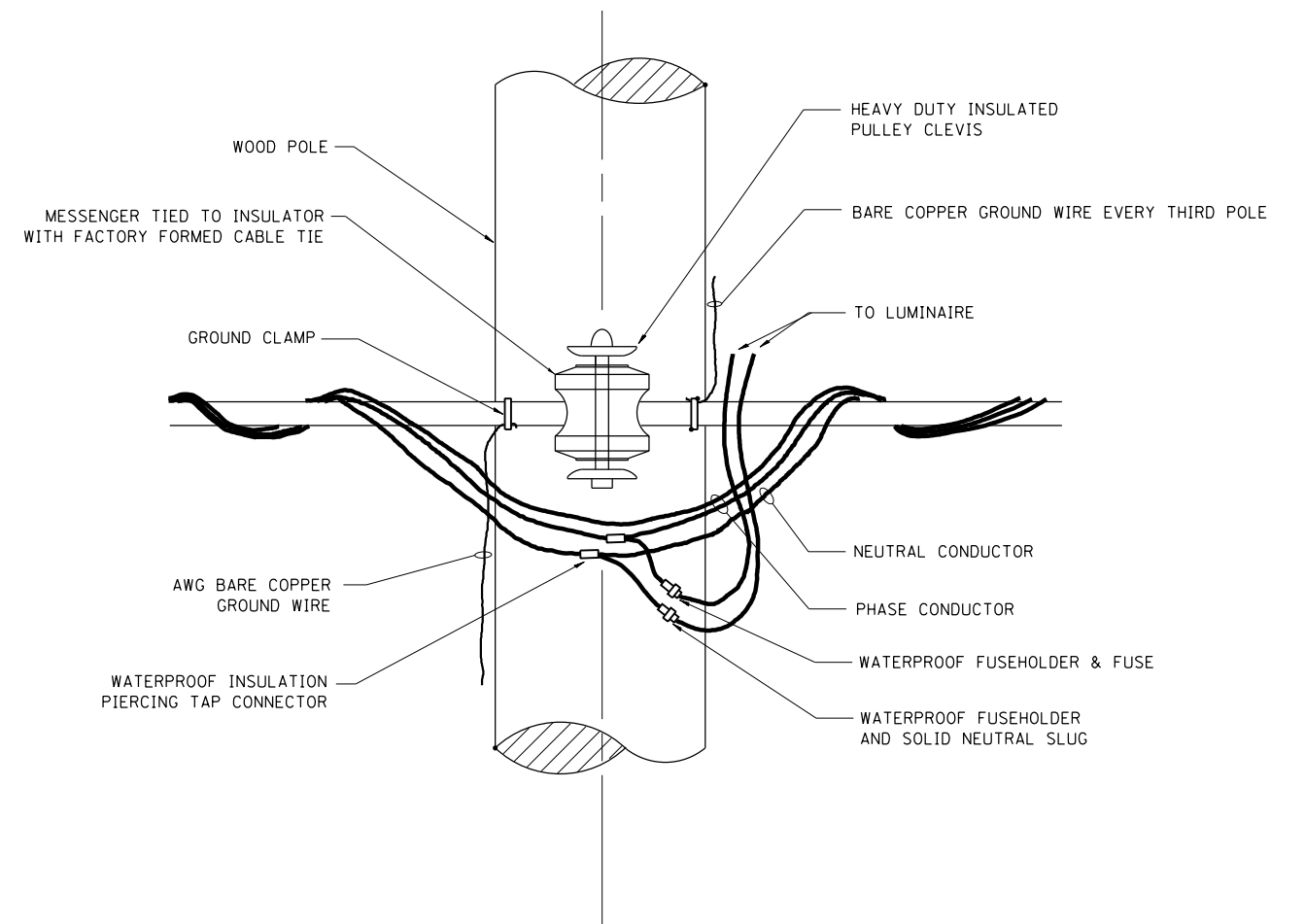
PLAN

NOTES:

1. BOX SHALL HAVE AN OPEN BASE.
2. ALL OPENINGS IN STRUCTURE MUST BE MACHINED AT TIME OF FABRICATION OR PUNCH DRIVEN AT TIME OF PLACEMENT. IN ACCORDANCE WITH MANUFACTURER RECOMMENDATIONS.
3. FIELD PLACEMENT OF COMMUNICATIONS VAULT SHALL BE AS DIRECTED BY THE ENGINEER.
4. ALL DIMENSIONS ARE MINIMUM AND A LARGER SIZE HANDHOLE MAY BE USED, WITH THE APPROVAL OF THE ENGINEER, TO FACILITATE USING A MANUFACTURER'S STANDARD PRODUCT.



TEMPORARY LIGHT POLE DETAIL



TEMPORARY LIGHT POLE ATTACHMENT DETAIL

NOTES:

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED

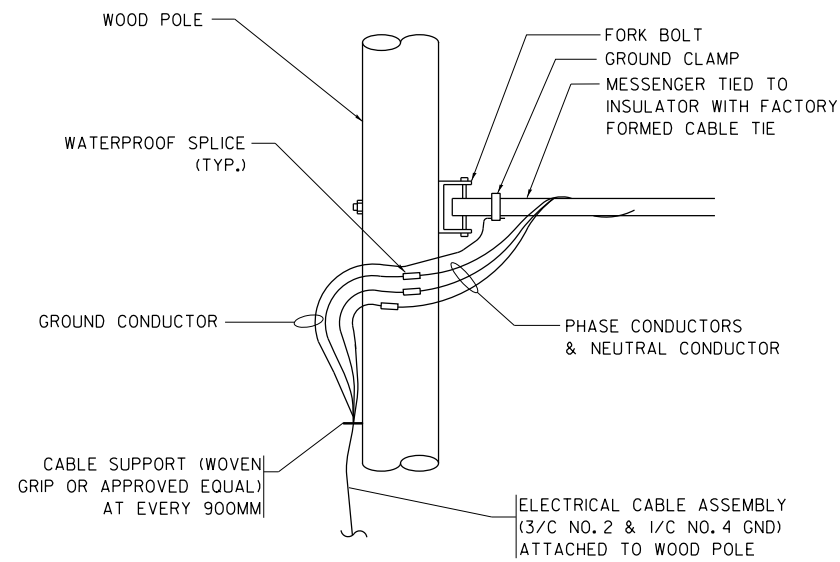
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	PLOT DATE = 5/15/2013	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

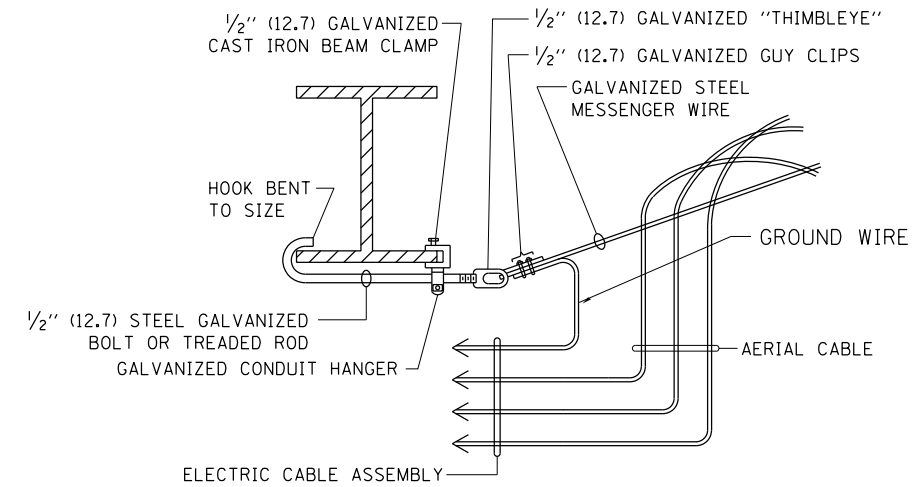
TEMPORARY LIGHT POLE DETAILS

SCALE: NONE SHEET NO. 17 OF 31 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2013-007R	COOK	317	249
BE-800		CONTRACT NO. 60W25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



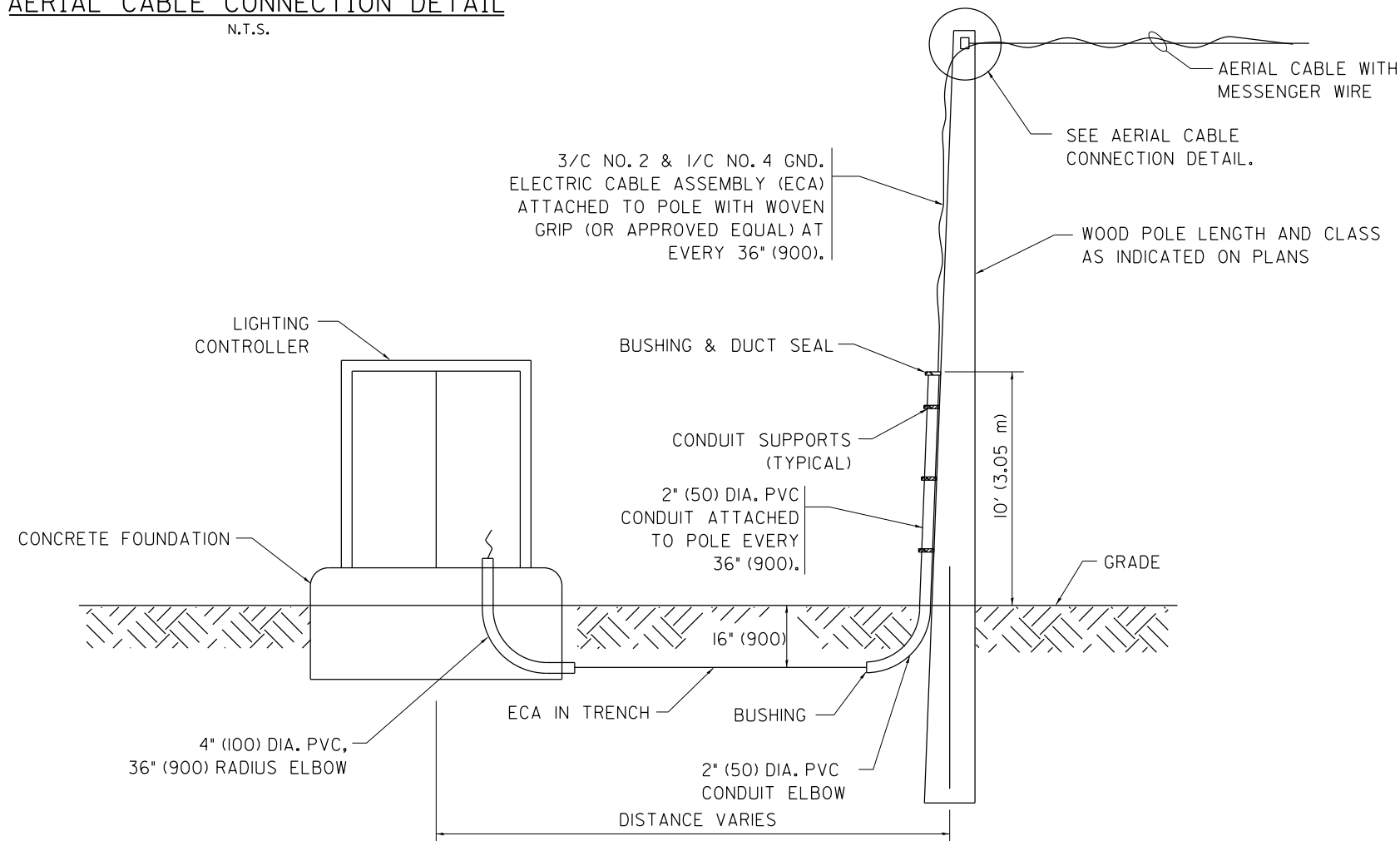
AERIAL CABLE CONNECTION DETAIL
N.T.S.



AERIAL CABLE ATTACHED TO STRUCTURE
NOT TO SCALE

NOTES:

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED.
2. SEE PROPOSED LIGHTING PLAN FOR CONDUIT, CABLE AND ROUTING.
3. THE CONTRACTOR SHALL PROVIDE INTERMEDIATE SUPPORTS TO MAINTAIN MINIMUM CLEARANCES. REFER TO AERIAL AERIAL CABLE ATTACHED TO STRUCTURE DETAIL.
4. COST OF SPLICES AND MOUNTING HARDWARE SHALL BE INCLUDED IN THE UNIT PRICE FOR AERIAL CABLE.



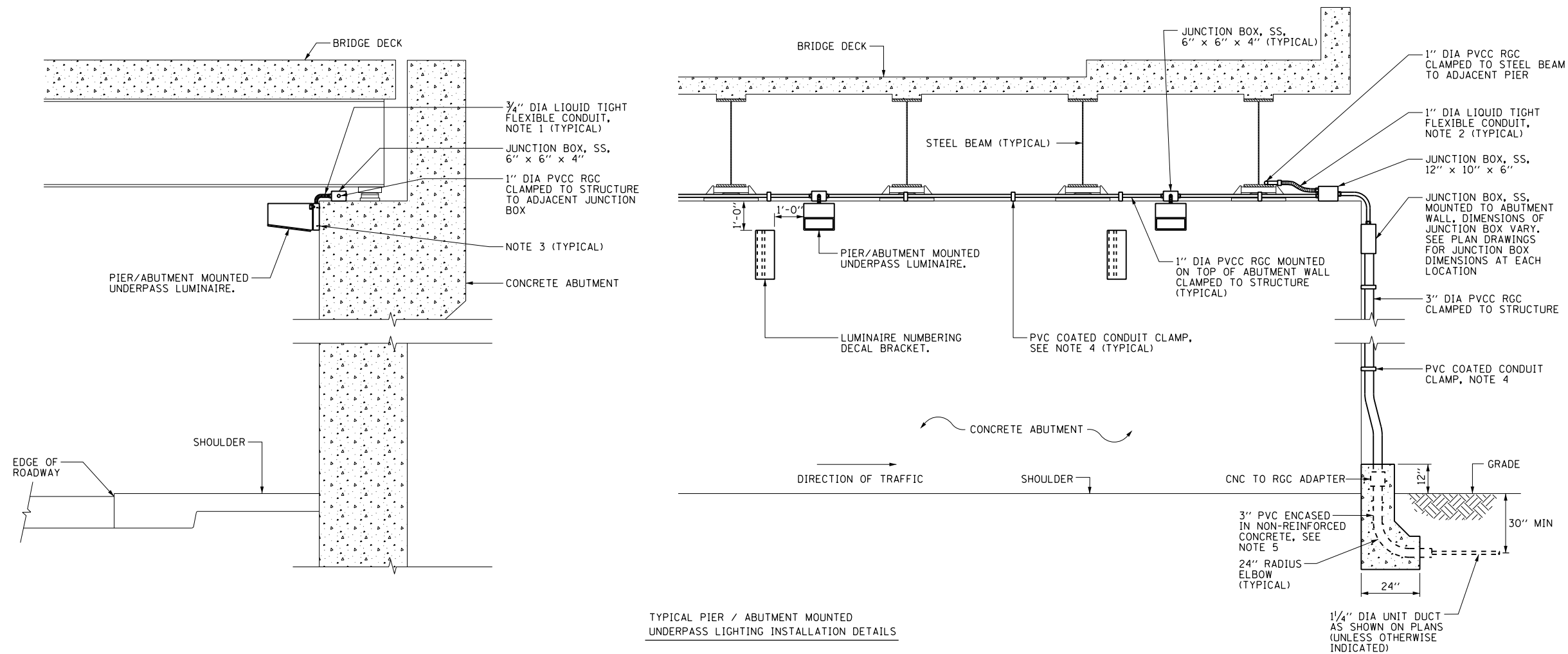
WOOD POLE TO LIGHTING CONTROLLER WIRING CONNECTION DETAIL
N.T.S.

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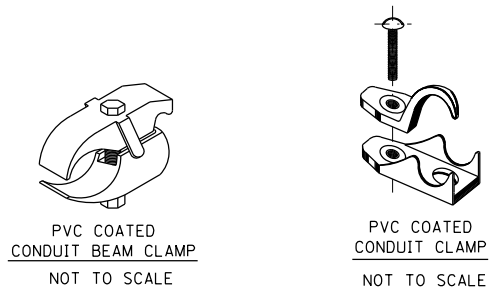
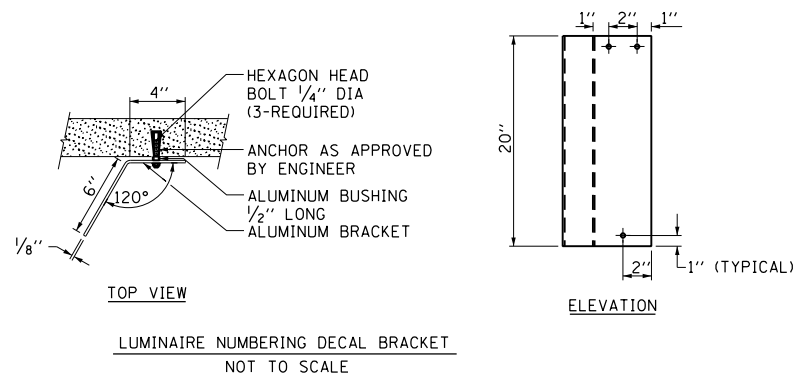
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TEMPORARY AERIAL CABLE INSTALLATION			
SCALE: NONE	SHEET NO. 18 OF 31 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2013-007R	COOK	317	250
BE-801		CONTRACT NO. 60W25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TYPICAL PIER / ABUTMENT MOUNTED UNDERPASS LIGHTING INSTALLATION DETAILS



NOTES:

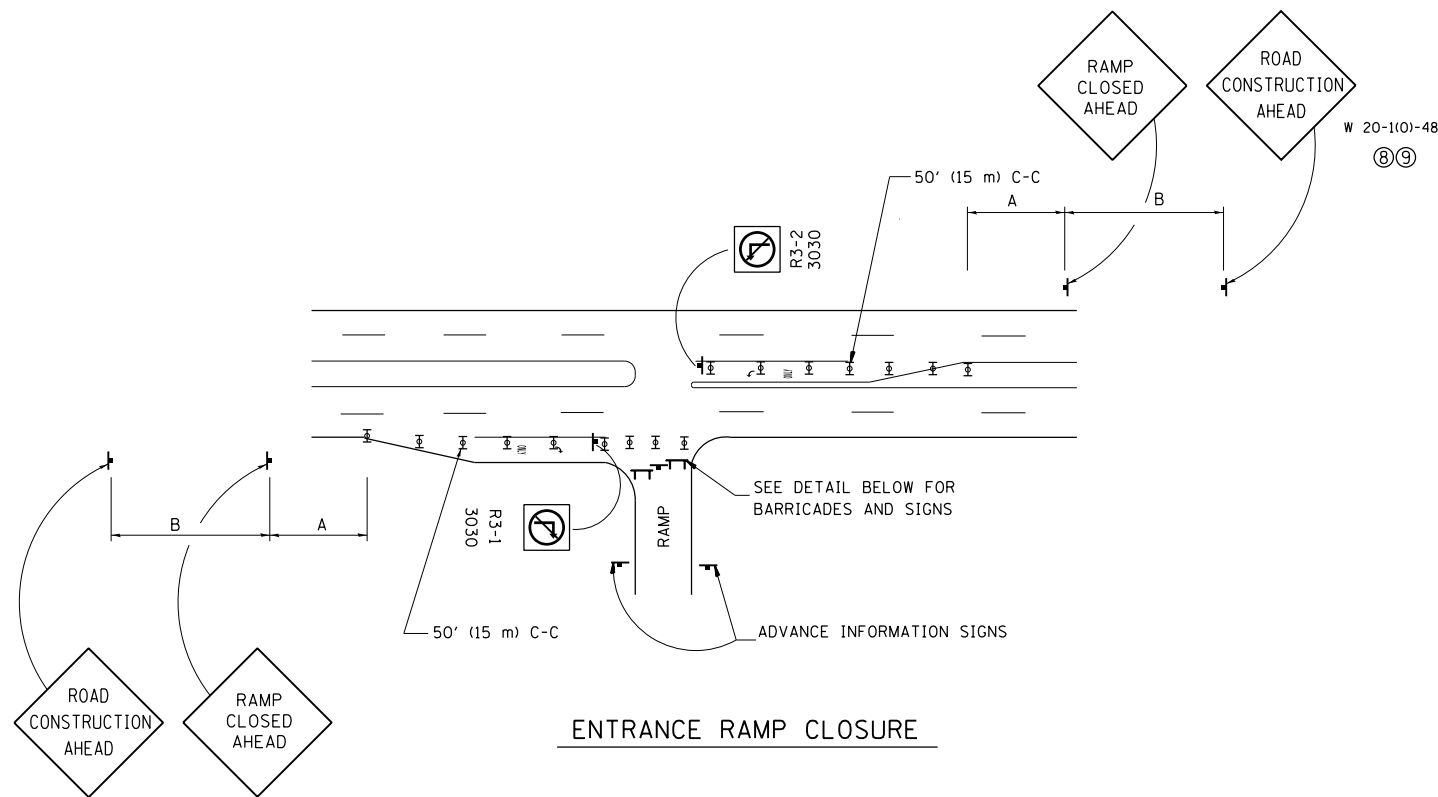
- LIQUID TIGHT FLEXIBLE METAL CONDUIT, MAXIMUM LENGTH 6'-0", TYPICAL FOR EACH INSTANCE AS SHOWN. PROVIDE PVC COATED RIGID GALVANIZED STEEL CONDUIT AS REQUIRED NOT TO EXCEED 6'-0" OF FLEXIBLE LIQUID TIGHT METAL CONDUIT. LIQUID TIGHT FLEXIBLE METAL CONDUIT WILL BE INCLUDED IN THE COST OF THE CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED PAY ITEM EXCEPT THAT THE COST OF THE 3/4" DIA. RIGID STEEL CONDUIT AND 3/4" DIA. FLEXIBLE CONDUIT SHALL BE INCLUDED IN THE LUMINAIRE INSTALLATION.
- UNDERPASS LUMINAIRE MOUNTED TO FACE OF PIER OR ABUTMENT WALL. MOUNTING HEIGHT OF 1" BELOW THE TOP OF PIER OR ABUTMENT WALL TYPICAL FOR ALL PIER/ABUTMENT MOUNTED UNDERPASS LUMINAIRES UNLESS OTHERWISE NOTED.
- EXPANSION ANCHOR, POWDER ACTUATED FASTENERS WILL NOT BE ALLOWED. EXPANSION ANCHOR MUST BE SIZED IN ACCORDANCE WITH MANUFACTURER'S REQUIREMENTS.
- SECURE THE CONDUIT WITH PVC COATED CONDUIT CLAMPS OR CONDUIT BEAM CLAMPS AS SHOWN AT 5'-0" INTERVALS FOR LATERALS AND WITHIN 2'-0" MAXIMUM FROM ANY JUNCTION BOX, FLEXIBLE CONDUIT, OR CHANGE IN DIRECTION. ALL PVC COATED CONDUIT CLAMPS OR BEAM CLAMPS SHALL BE INCLUDED WITH THE COST OF THE "CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED" PAY ITEM.
- THE CONCRETE ENCASED CONDUIT TRANSITION SHALL BE INCLUDED IN THE COST OF THE GALVANIZED RIGID STEEL CONDUIT PAY ITEMS.
- ALL CONDUIT ATTACHED TO STRUCTURE SHALL BE PVC COATED RIGID STEEL CONDUIT (PVCC RGC) TYPICAL.

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PLOT SCALE = 50.000' / in.		DATE -	REVISED -
PLOT DATE = 5/15/2013			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER / ABUTMENT MOUNTED UNDERPASS LUMINAIRE INSTALLATION DETAILS			
SCALE: NONE	SHEET NO. 19 OF 31 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2013-007R	COOK	317	251
BE-902		CONTRACT NO. 60W25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



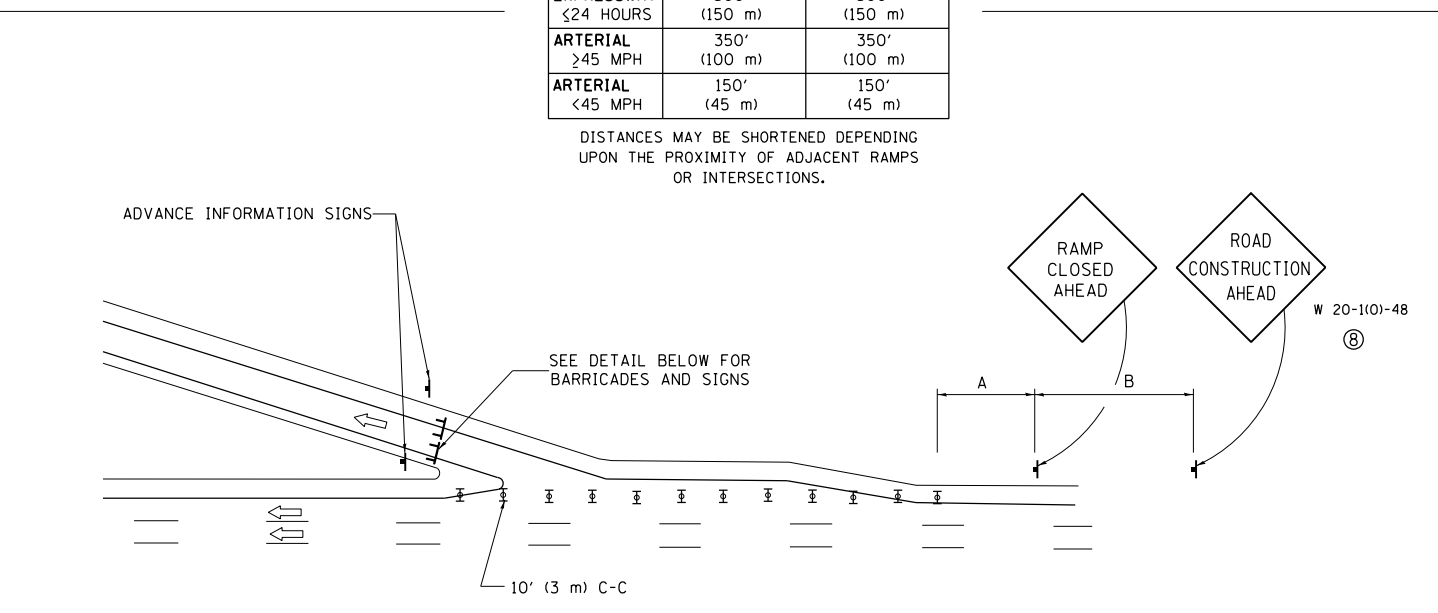
ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL ≥45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	150' (45 m)	150' (45 m)

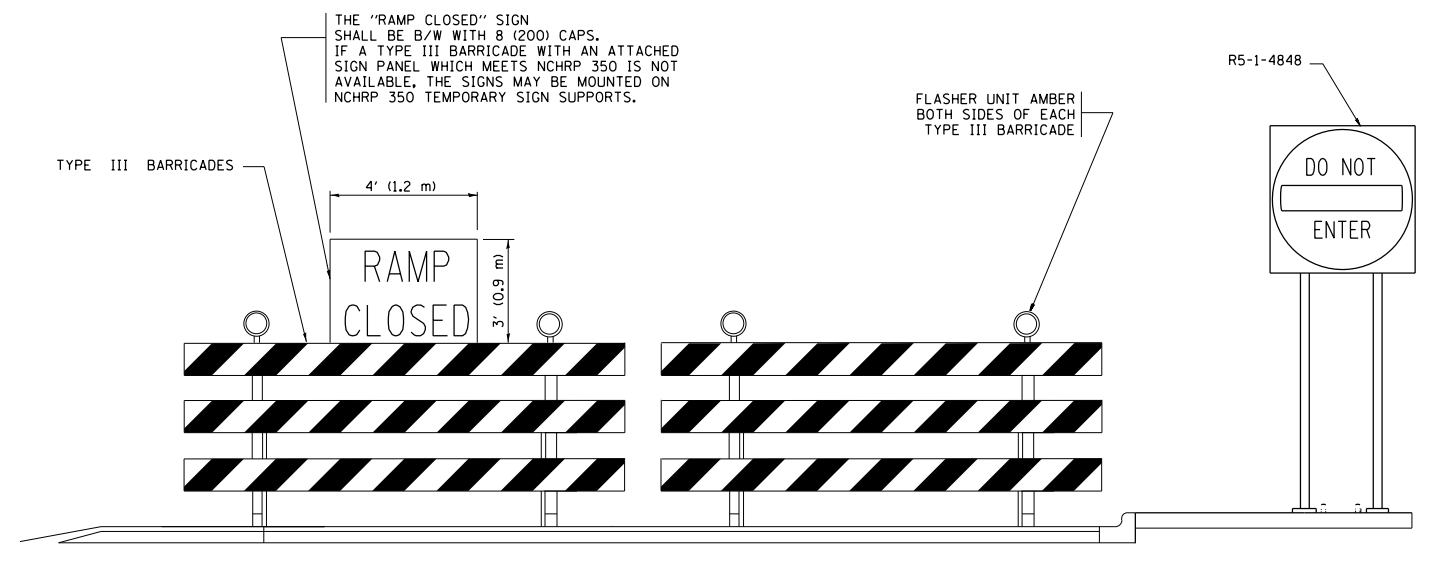
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.

W 20-1(0)-48 (8) (9)

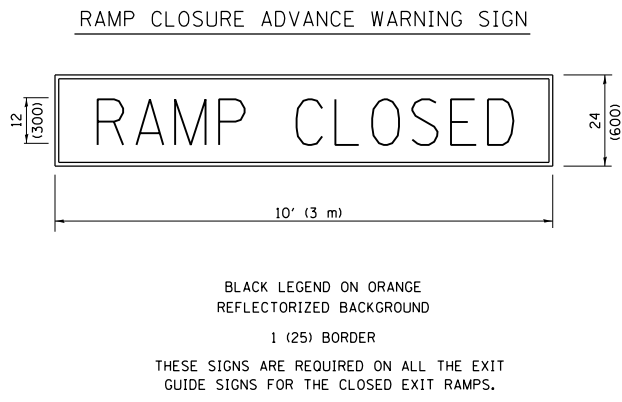


EXIT RAMP CLOSURE

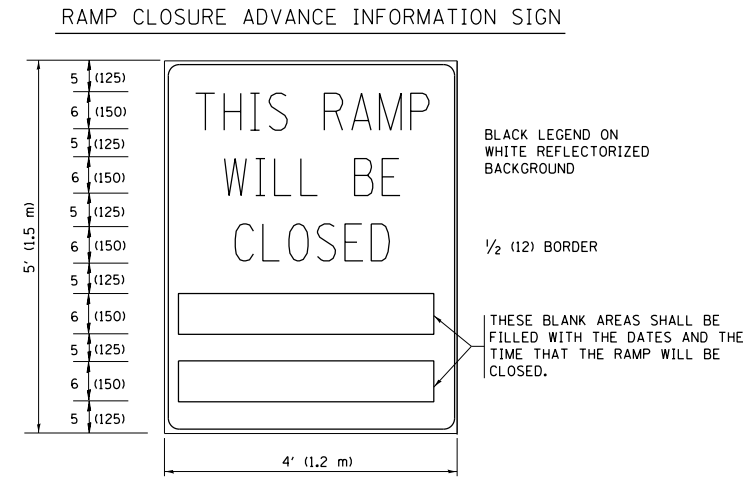
- SYMBOLS**
- ⊥ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
 - ⊓ TYPE III BARRICADE WITH FLASHING LIGHT



DETAIL FOR REQUIRED BARRICADES & SIGNS



BLACK LEGEND ON ORANGE REFLECTORIZED BACKGROUND
1 (25) BORDER
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR THE CLOSED EXIT RAMPS.



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY-FOUR (24) HOURS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED ON CLOSURES LESS THAN 24 HOURS IN DURATION.

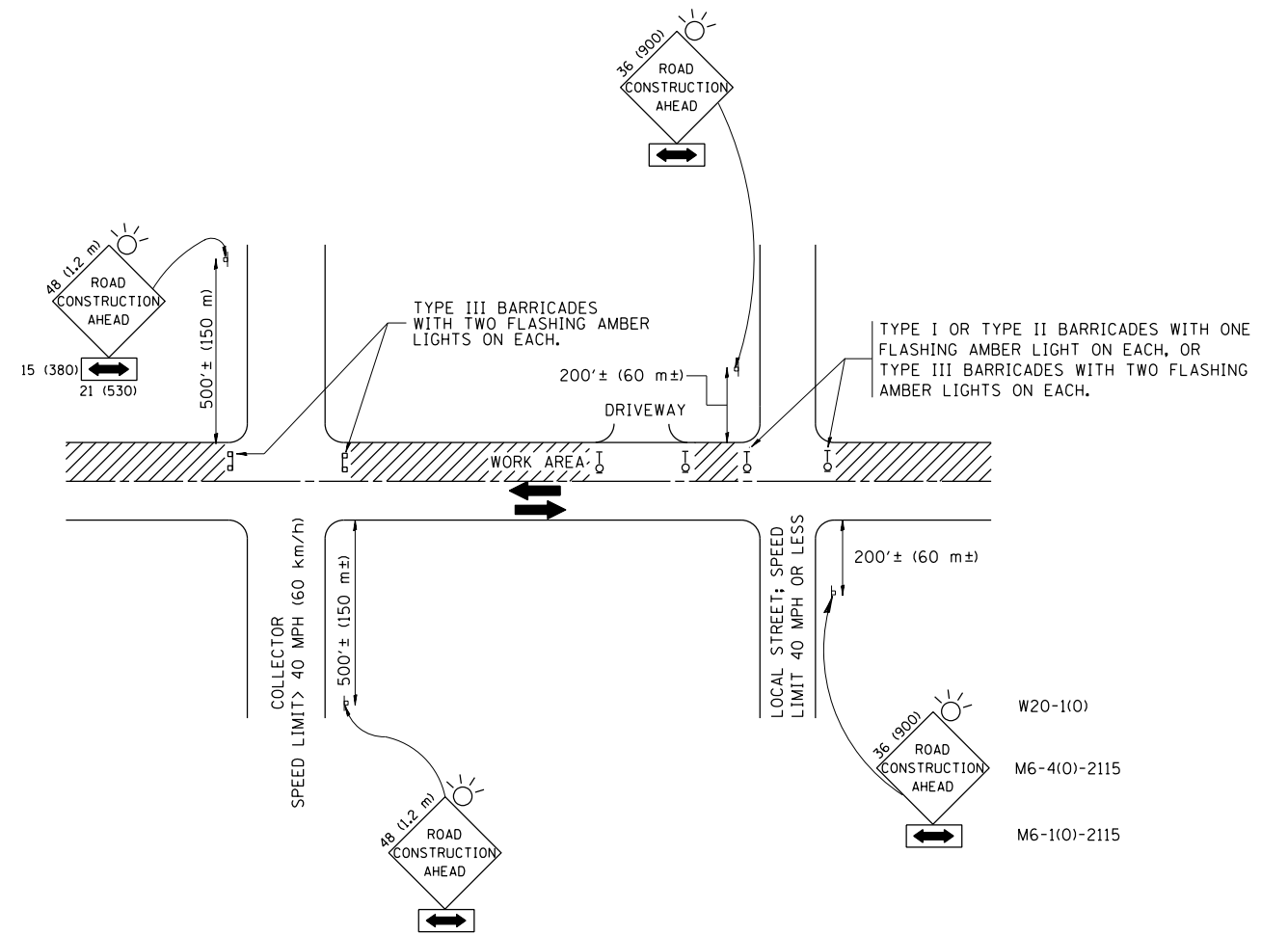
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = pteip2	DESIGNED - DWS	REVISED - DWS/JAF 12-02
p:\388039-pwintl.eacom\lne.local\pwaec\000\Documents\01 Americas\Transportation\000\Drawn Circle\Phase 1\11000.CAD\006.Road\REVISED\60W25.CAD-02-10-09\25-SHT-D1-Detail-20.dgn		CHECKED -	REVISED - SPB 01-07
	PLOT SCALE = 50.000' / in.	DATE - 02-83	REVISED - SPB 12-09
	PLOT DATE = 5/15/2013		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS			
SCALE: NONE	SHEET NO. 20 OF 31 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2013-007R	COOK	317	252
TC-08			CONTRACT NO. 60W25	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
90/94/290				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

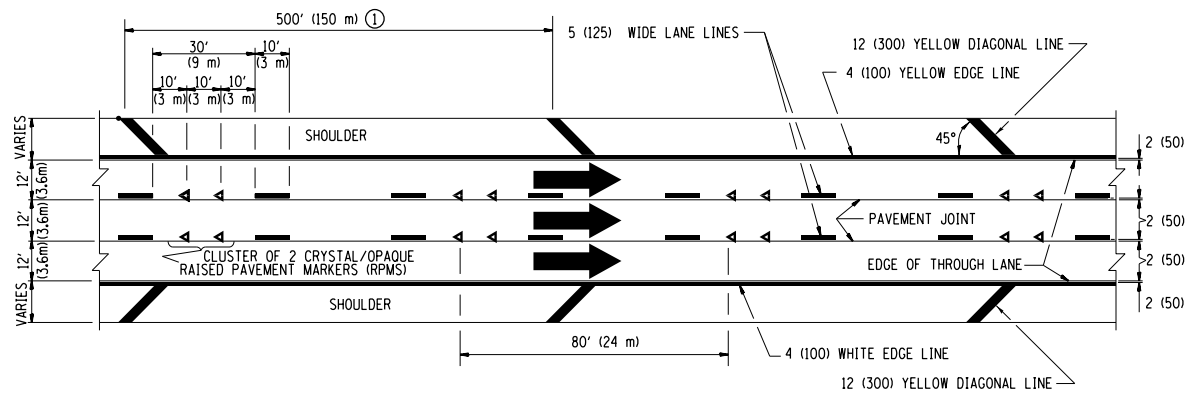
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = pte1p2	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
pw:\388039-pwintl\ecomon\lne.local\PWAE	C:\000\Documents\01 Americas\Transportation\	DRAWN Circle\Phase.II\000.CAD\006.Road	REVISED - A. HOUSEH 10-15-96
	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 5/15/2013	DATE - 06-89	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	
SCALE: NONE	SHEET NO. 21 OF 31 SHEETS
STA.	TO STA.

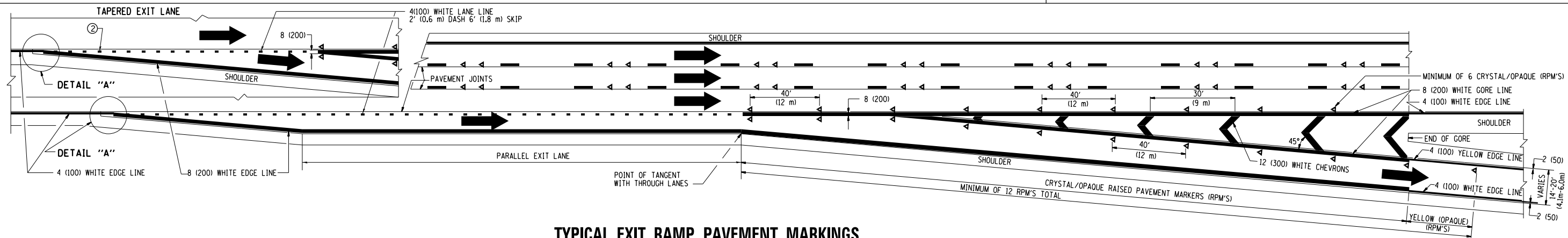
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2013-007R	COOK	317	253
TC-10		CONTRACT NO. 60W25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
* 90/94/290				



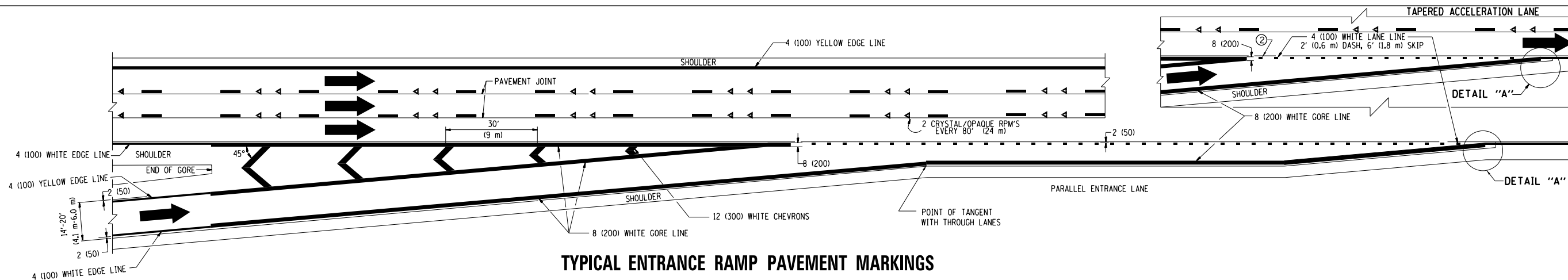
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

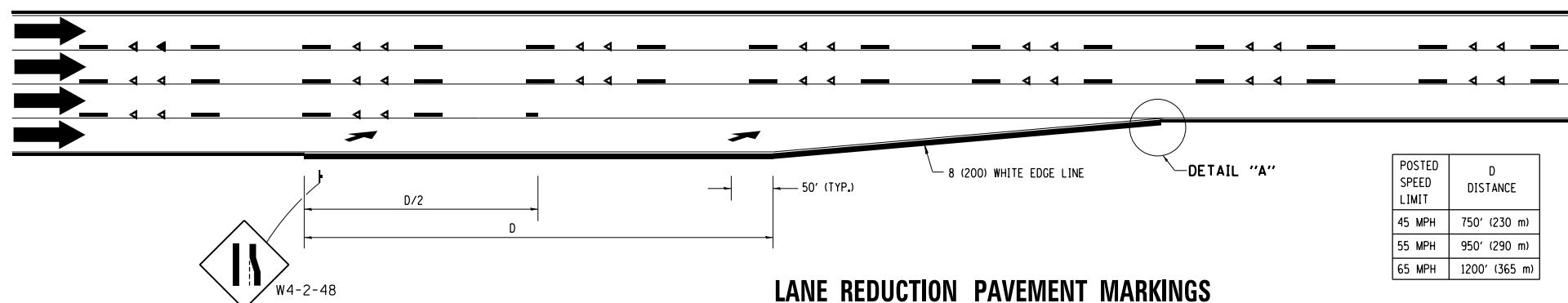
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES ON BITUMINOUS PAVEMENT.
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC.



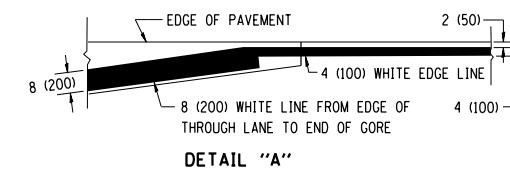
TYPICAL EXIT RAMP PAVEMENT MARKINGS



TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



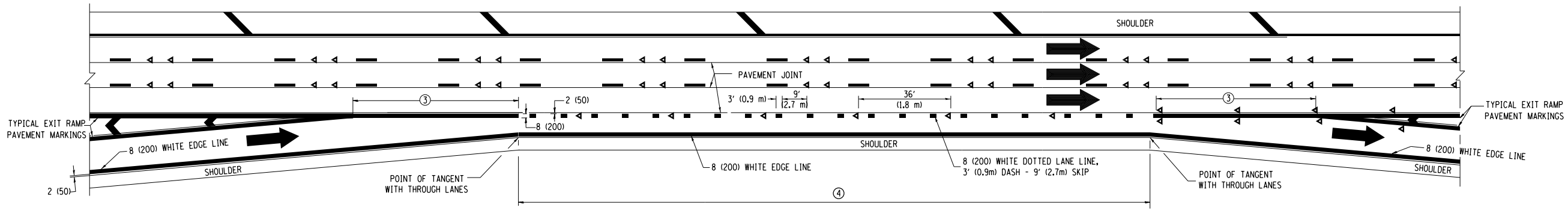
LANE REDUCTION PAVEMENT MARKINGS



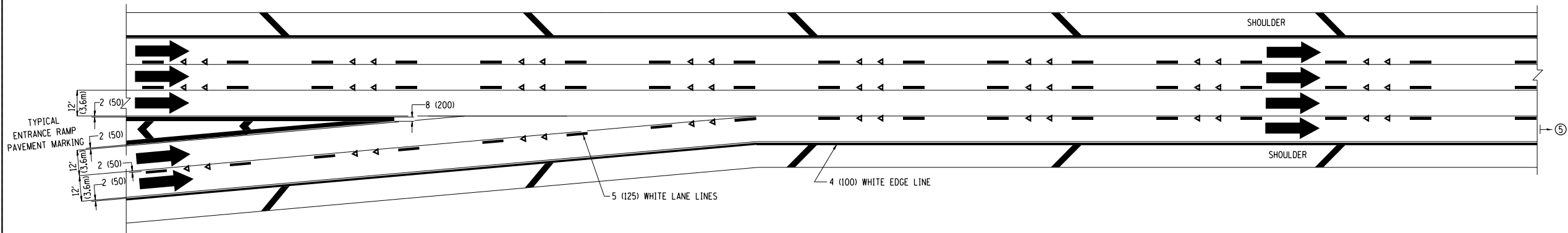
NOTES:

- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
- ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

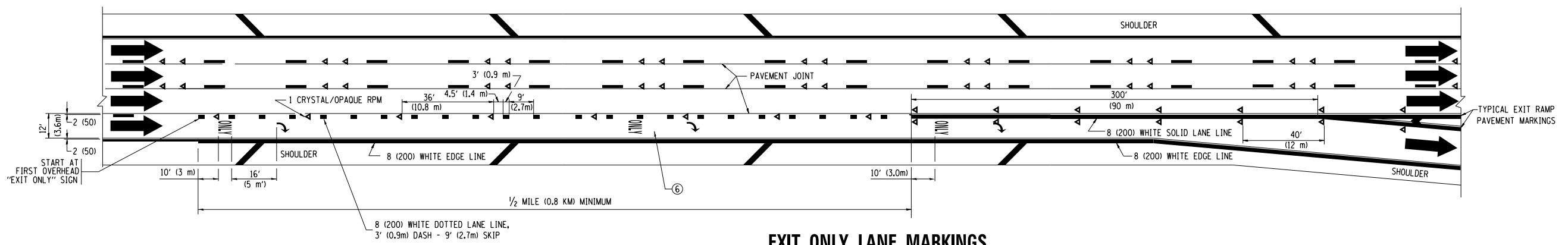
POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



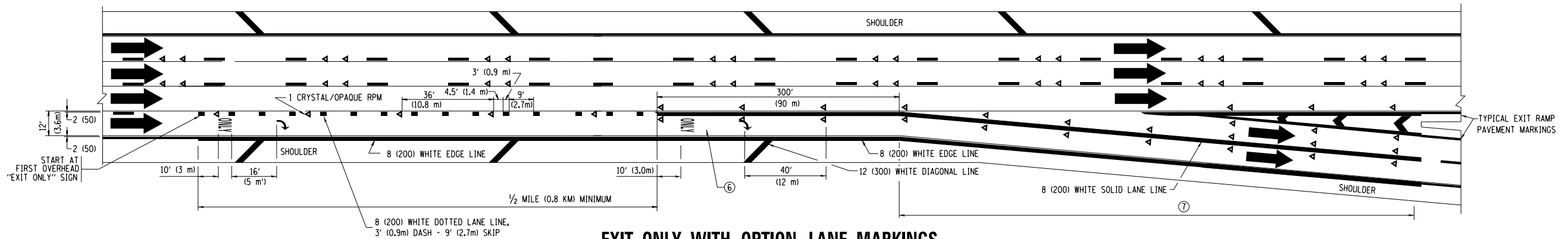
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS




EXIT ONLY WITH OPTION LANE MARKINGS


- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.


FILE NAME =	USER NAME = pte1p2	DESIGNED - D.W.S.	REVISED - D.W.S. 07-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
DRAWN CirclePhase.II.000.CAD\006.Road\REVISED\60W25.Cad\02-006-25-SHT-D1-22-23.dgn					SCALE: NONE	SHEET NO. 23 OF 31 SHEETS	STA.	TO STA.	•	2013-007R	COOK	317	255
PLOT SCALE = 50.000' / in.					CHECKED -	REVISED - S.P.B. 01-07				TC-12		CONTRACT NO. 60W25	
PLOT DATE = 5/15/2013					DATE - 01-90	REVISED - S.P.B. 01-10				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

90/94/290






ROUTE MARKERS

 FOR U.S. ROUTES
MI-40-2424






 FOR ILLINOIS ROUTES
MI-50-2424

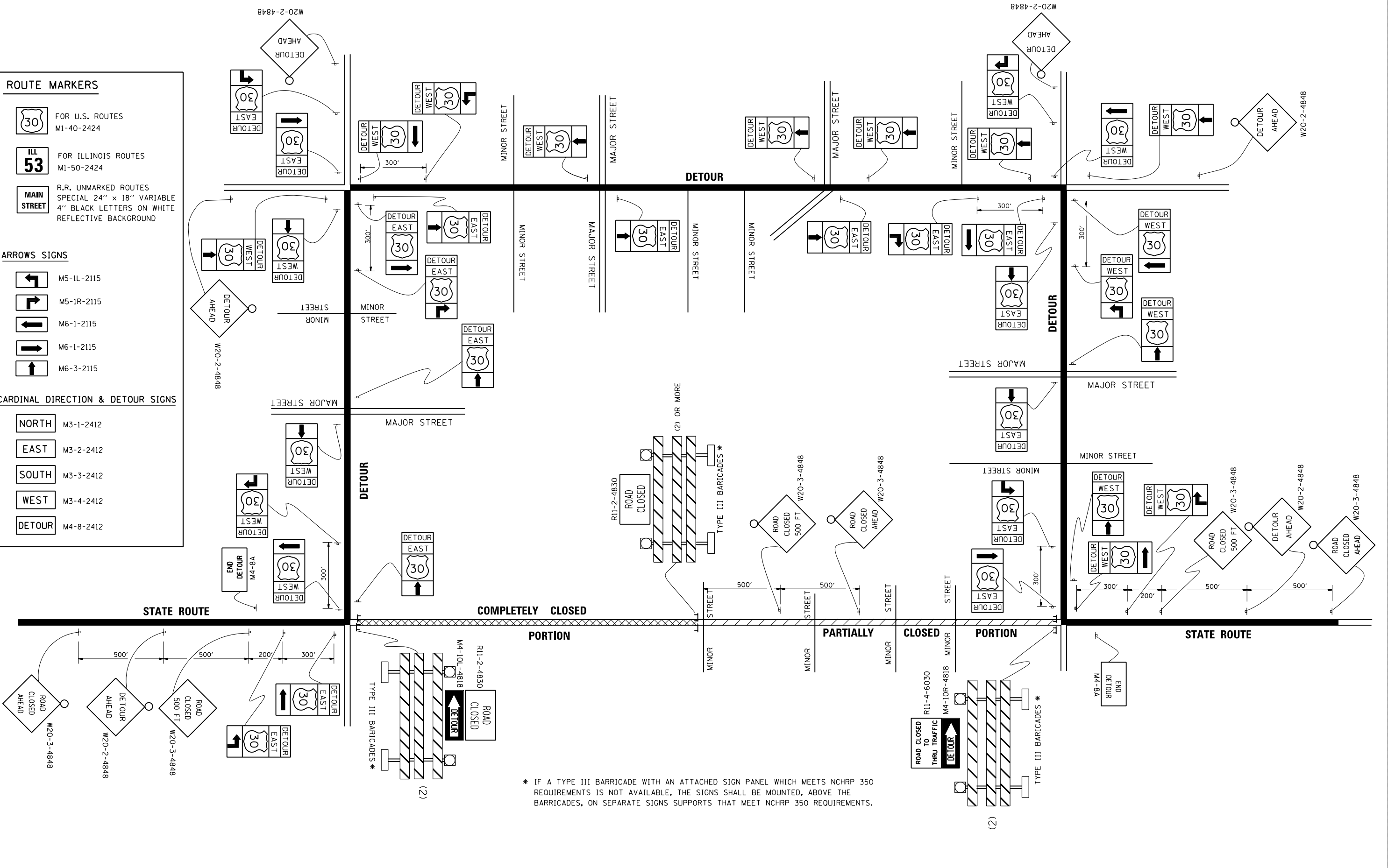
 R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS

-  M5-1L-2115
-  M5-1R-2115
-  M6-1-2115
-  M6-1-2115
-  M6-3-2115

CARDINAL DIRECTION & DETOUR SIGNS

-  NORTH M3-1-2412
-  EAST M3-2-2412
-  SOUTH M3-3-2412
-  WEST M3-4-2412
-  DETOUR M4-8-2412



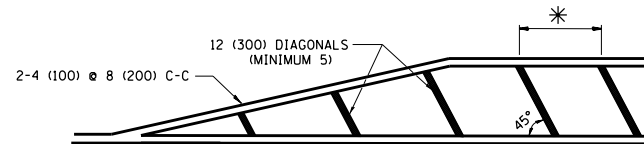
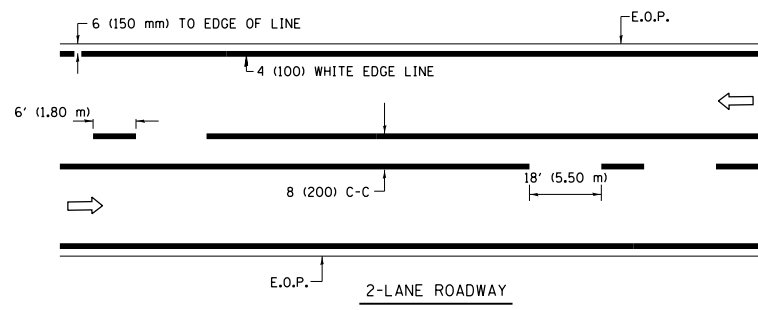
* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

FILE NAME =	USER NAME = pteip2	DESIGNED -	REVISED - 10-18-02
C:\pwworking\Illinois\Projects\2013\007R\007R-001\007R-001-001.dgn		CHECKED -	REVISED -
PLOT SCALE = 49.9999' / in.		DATE -	REVISED -
PLOT DATE = 5/15/2013			

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

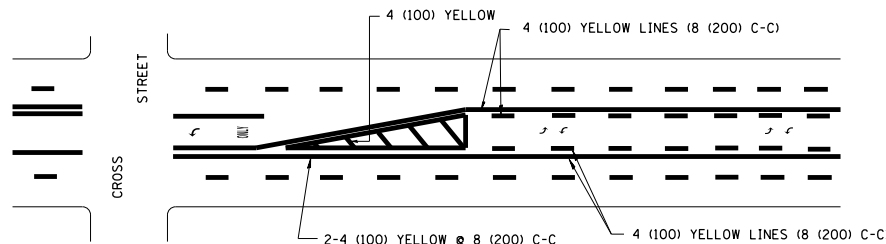
DETOUR SIGNING FOR CLOSING STATE HIGHWAYS			
SCALE: NONE	SHEET NO. 24 OF 31 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2013-007R	COOK	317	256
TC-21		CONTRACT NO. 60W25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

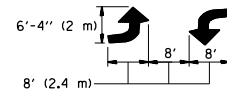


* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 * DIAGONAL LINE SPACING: 20' (6.1 m) C-C

PAINTED MEDIANS

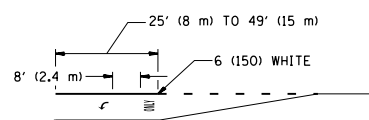
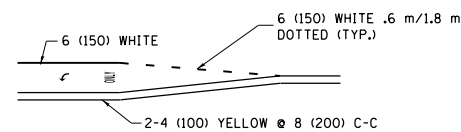


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

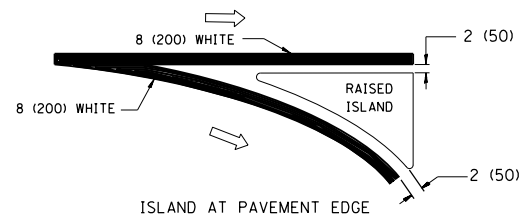
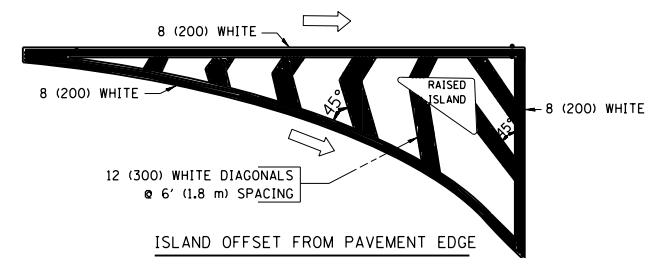
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

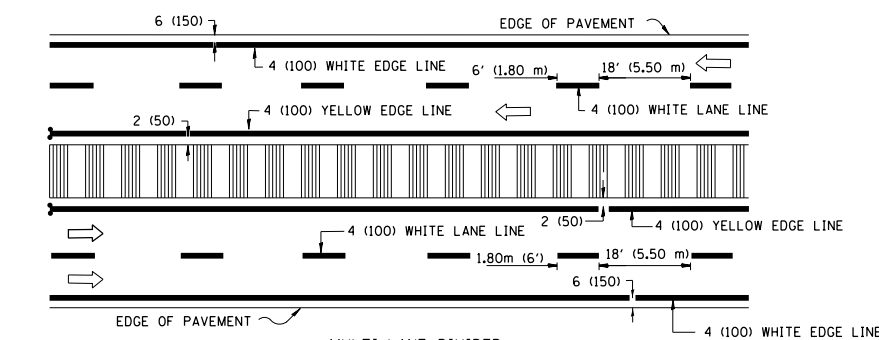
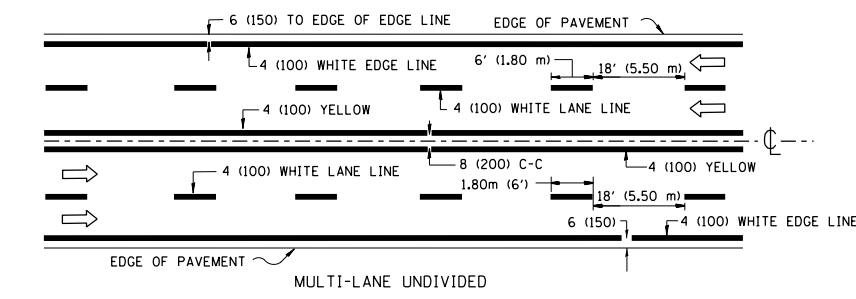


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

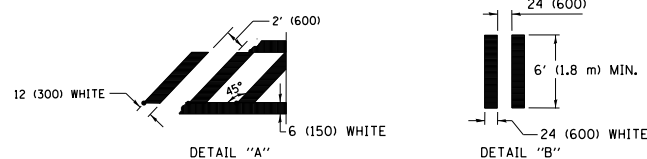
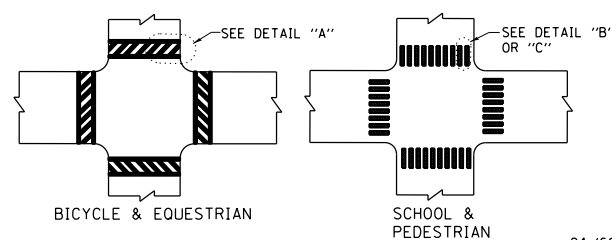
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

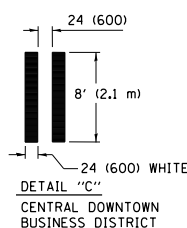


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



DETAIL "C"
CENTRAL DOWNTOWN BUSINESS DISTRICT

FILE NAME =	USER NAME = pte1p2	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
pw:\388039-pwintl\ecomonline\local\PAVE\COM\Documents\01 Americas\Transportation\0000\Drawn\Circle\Phase_11\000_CAD\006_Road\REVISED_60WZL_CEN\02-28-12\Detail-25-27.dgn		CHECKED -	REVISED -
	PLOT SCALE = 50.000' / in.	DATE -	REVISED -
	PLOT DATE = 5/15/2013		

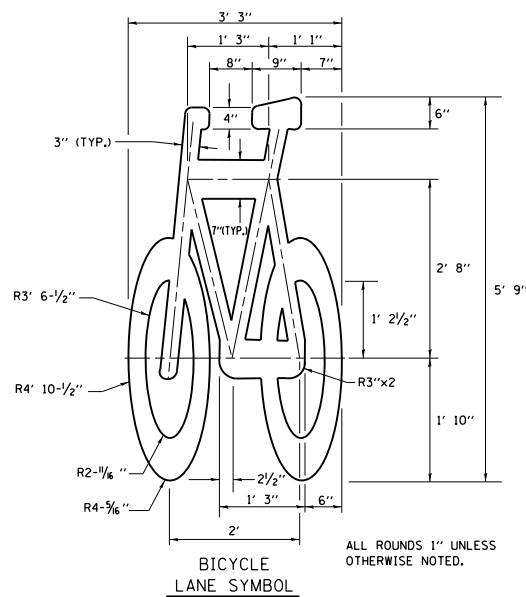
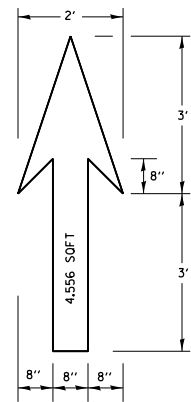
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET NO. 25 OF 31 SHEETS STA. TO STA.

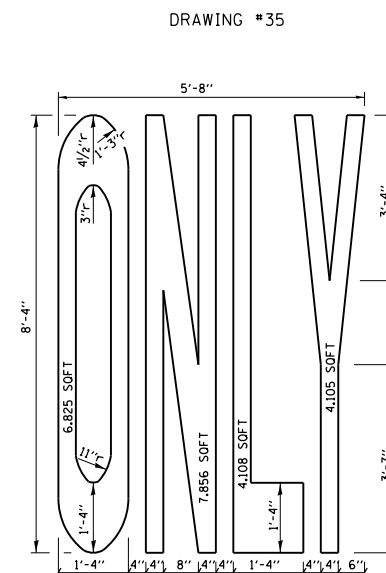
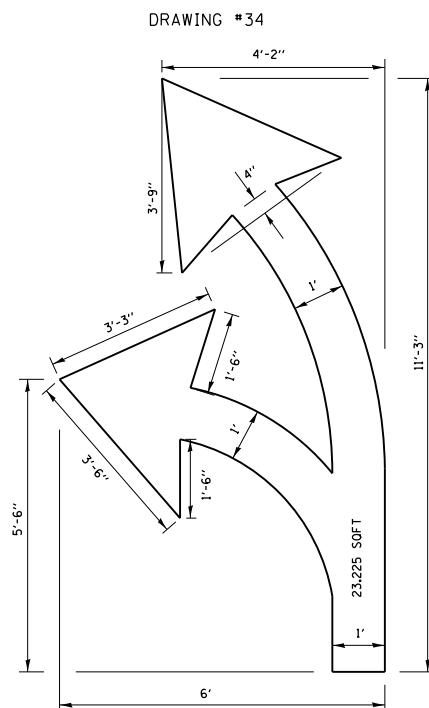
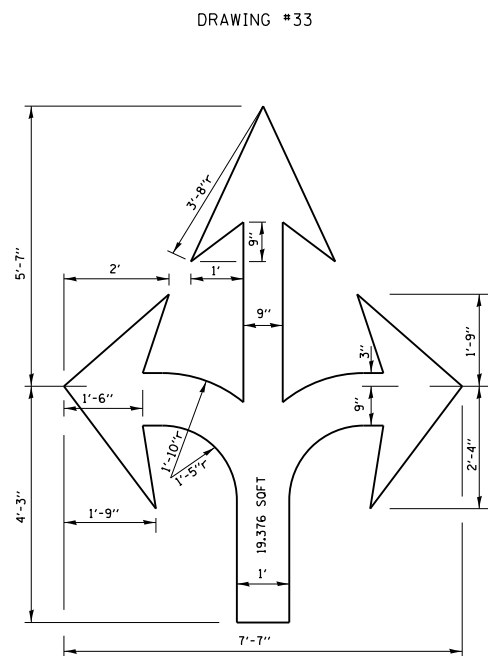
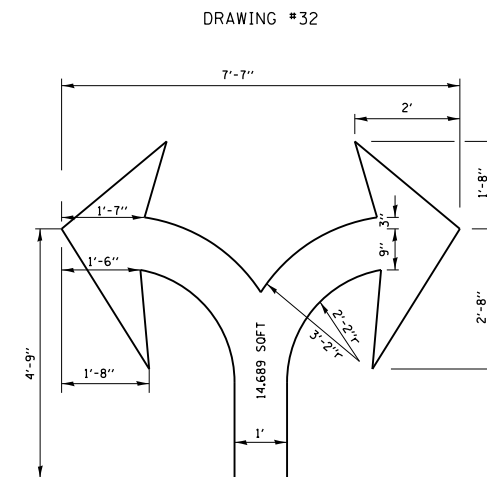
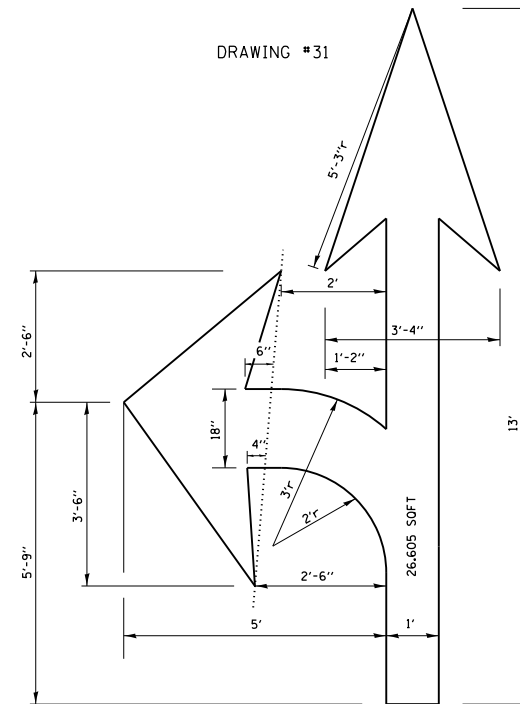
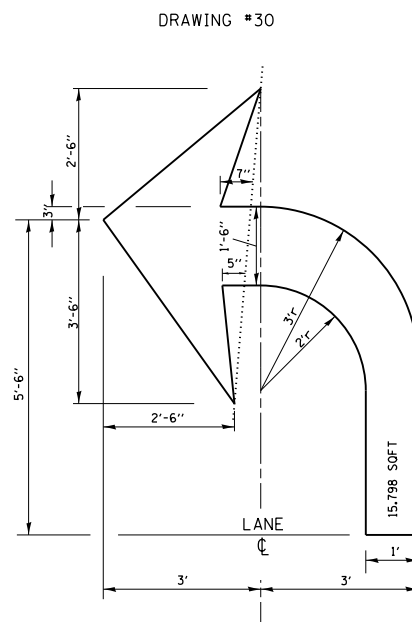
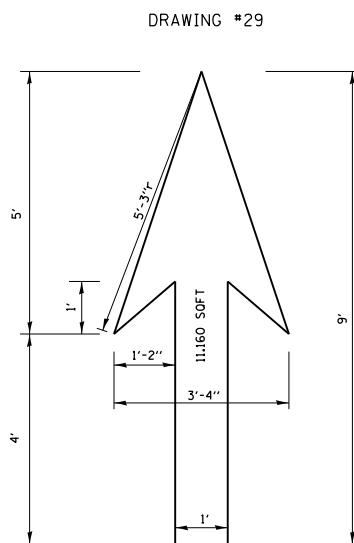
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2013-007R	COOK	317	257
TC-24		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

90/94/290



- NOTE:**
- FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
 - THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28



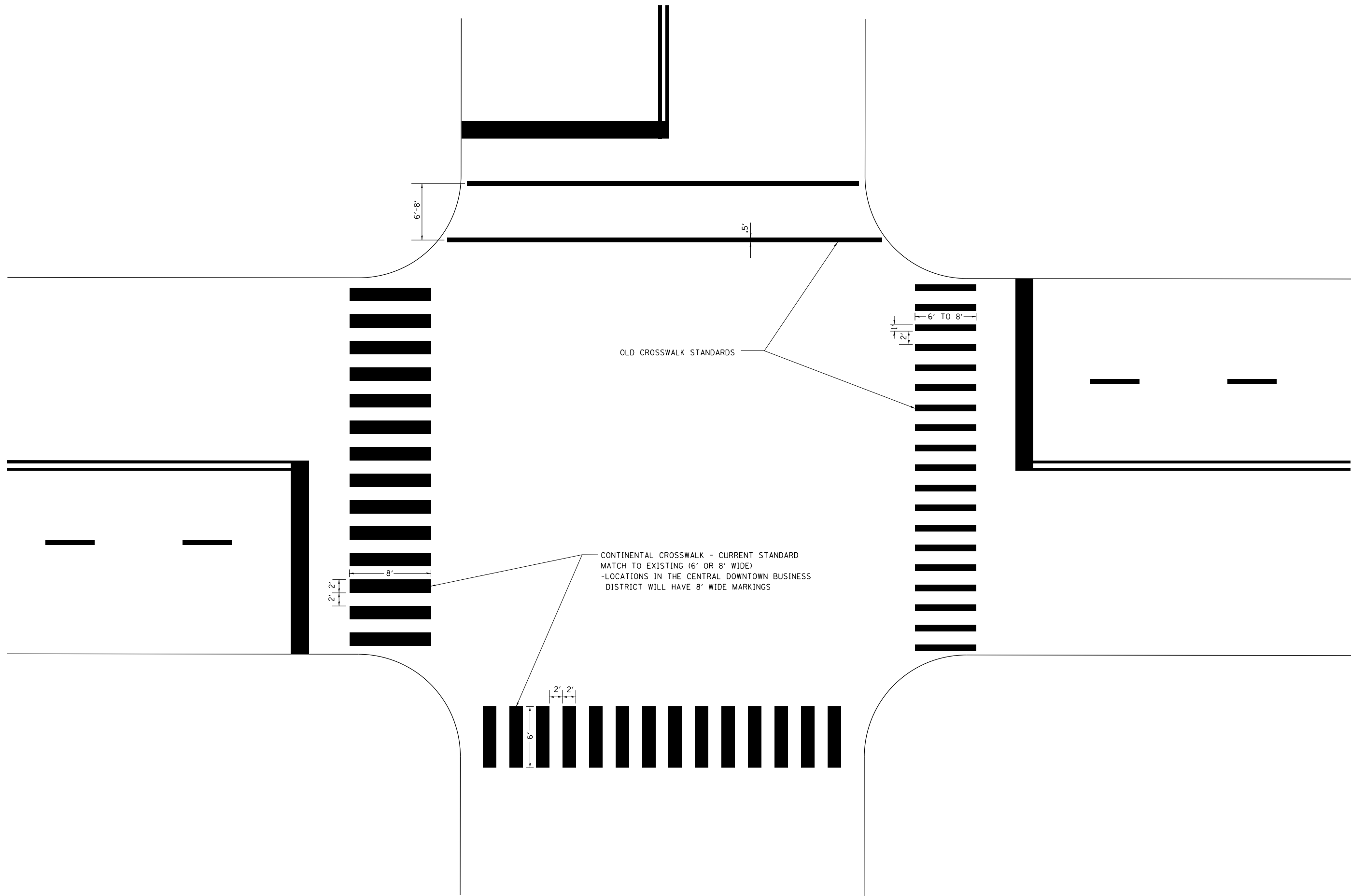
NOTE:
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

FILE NAME =	USER NAME = pte1p2	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
pw:\388039-pwintl.eacom\lne.local\pawc\000\Documents\01 Americas\Transportation\000\Drawn\Circle\Phase 1\11000.CAD\006.Road\REVISED\60W2K.CAD\001\21-32-12	COMMENTS	CHECKED -	REVISED -
PLOT SCALE = 50.000' / in.	DATE = 5/15/2013	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 26 OF 31 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2013-007R	COOK	317	258
TC-24		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
90/94/290				



CONTINENTAL CROSSWALK - CURRENT STANDARD
 MATCH TO EXISTING (6' OR 8' WIDE)
 -LOCATIONS IN THE CENTRAL DOWNTOWN BUSINESS
 DISTRICT WILL HAVE 8' WIDE MARKINGS

OLD CROSSWALK STANDARDS

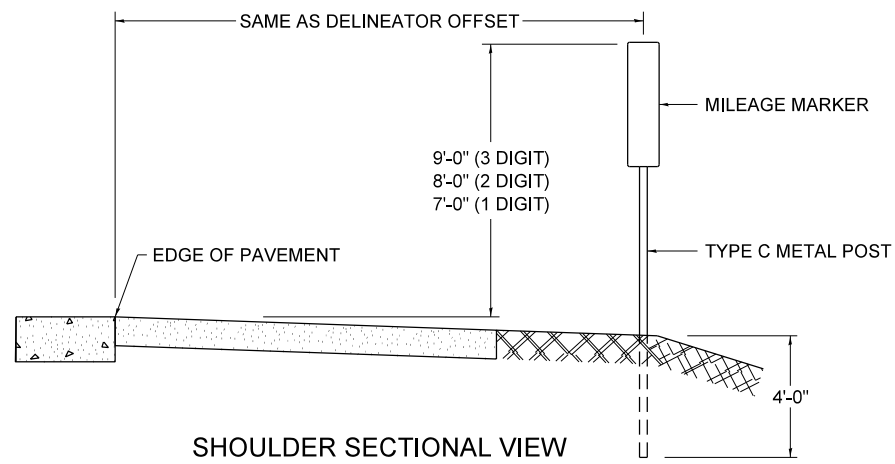
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	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/15/2013	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

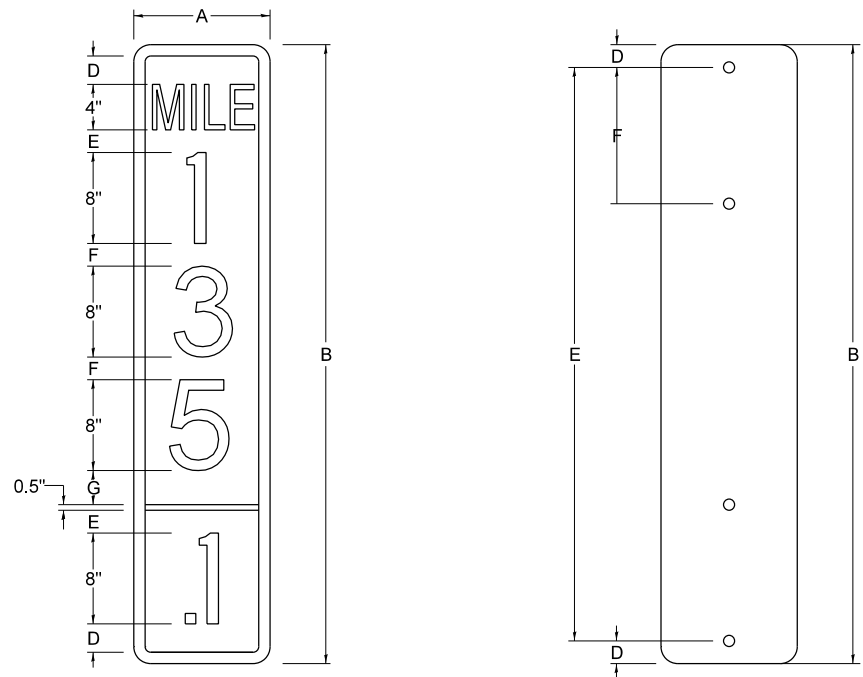
CITY OF CHICAGO			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 27 OF 31 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	2013-007R	COOK	317	259
TC-24		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
• 90/94/290				

STANDARD DESIGN FOR MILE POST



SHOULDER SECTIONAL VIEW

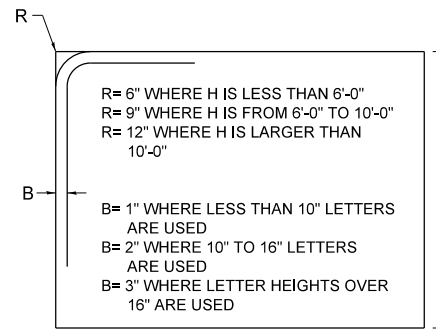


SIGN SIZE	DIMENSIONS							
	A	B	C	D	E	F	G	DIGIT
12 x 24	12.0	24.0	1.5	1.5	1.5	N/A	1.5	1
12 x 36	12.0	36.0	1.5	2.0	2.0	2.0	1.5	2
12 x 48	12.0	48.0	1.5	2.5	2.0	2.0	2.5	3

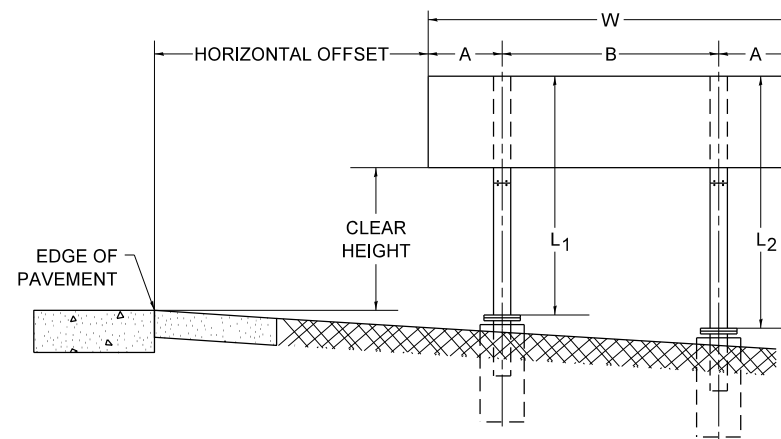
BLANK	A	B	C	D	E	F
B9-1224	12.0	24.0	1.5	2.0	20.0	N/A
B9-1236	12.0	36.0	1.5	2.0	32.0	12.0
B9-1248	12.0	48.0	1.5	2.0	44.0	12.0

SIGN SIZE	SERIES					BLANK STD.	
	LINES						
	1	2	3	4	5	BORDER	
12 x 24	4C	8D	4C	N/A	N/A	0.5	B9-1224
12 x 36	4C	8D	8D	4C	N/A	0.5	B9-1236
12 x 48	4C	8D	8D	8D	4C	0.5	B9-1248

BORDER AND RADIUS LAYOUT



MAJOR GUIDE SIGN LAYOUT

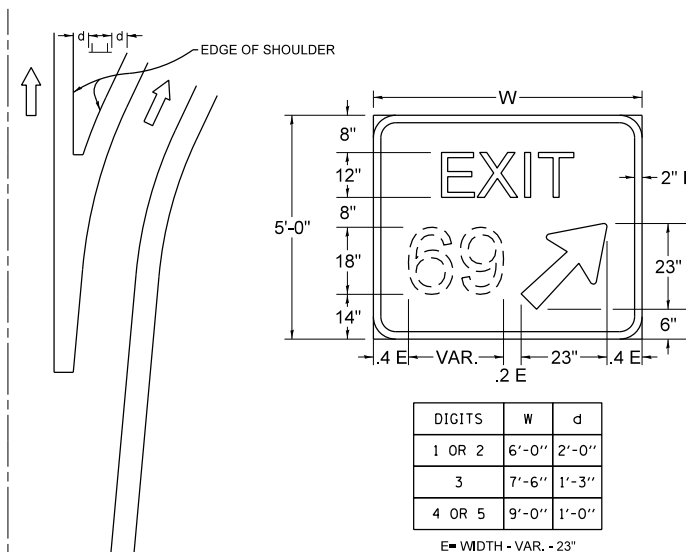


NUMBER OF STEEL SUPPORTS	A	B
2	.2 W	.6 W
3	.15 W	.35 W
4	.125 W	.25 W
5	.1 W	.2 W

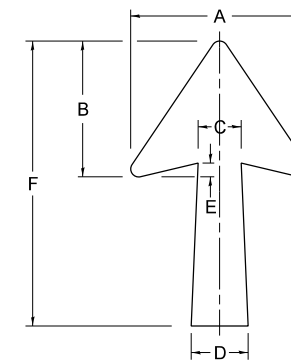
"L1" IS THE LENGTH OF SUPPORT, NOT INCLUDING THE STUB PROJECTION, CLOSEST TO THE EDGE OF THE PAVEMENT.

"A" IS THE DISTANCE FROM THE SIGN EDGE TO THE CENTERLINE OF THE NEAREST SUPPORT. "B" IS THE DISTANCE BETWEEN CENTERLINES OF SUPPORTS.

GORE SIGNS

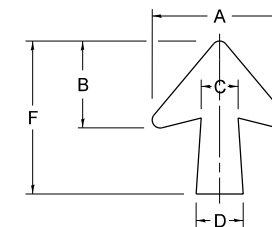


STANDARD ARROWS FOR INTERSTATE GUIDE SIGNS



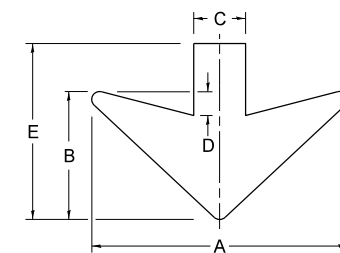
ARROW SYMBOL	A	B	C	D	E	F	R
24 1/4 x 15 1/8	15 1/8	11 1/8	3 3/4	5	1 1/8	24 1/4	1/8
29 1/4 x 18 1/4	18 1/4	14	4 1/2	6	1 1/2	29 1/4	3/4
35 5/8 x 22 1/4	22 1/4	17	5 3/8	7 1/8	1 3/4	35 5/8	1
18 1/4 x 11 1/4	11 1/4	8 3/4	3 1/8	3 3/8		18 1/4	

NOTE: D & F ARE RECOMMENDED DIMENSIONS. TAPER SHOULD BE HELD CONSTANT FOR LONGER OR SHORTER SHAFT LENGTHS



ARROW SYMBOL	A	B	C	D	E	F	R
17 1/4 x 14 1/4	14 1/4	9 1/8	3 3/8	4 1/2	1 5/8	17 1/4	3/4
20 1/4 x 17 1/4	17 1/4	11 3/4	4 3/8	5 5/8	1 1/2	20 1/4	
25 x 21 1/8	21 1/8	14 1/4	5	6 3/4	1 3/4	25	1
9 5/8 x 8 1/8	8 1/8	5 1/8	2 3/8	2 3/8		9 5/8	1/2

DOWN ARROWS



ARROW SYMBOL	A	B	C	D	E	R
16 1/2 x 24	24	12	5	1 1/2	16 1/2	3/4
22 x 32	32	16	6 1/2	3	22	1

FILE NAME = D160W25-SHT-D1-Detail-28.dgn

USER NAME = potelp2
 PLOT SCALE = 50.0000' / in.
 PLOT DATE = 5/15/2013

DESIGNED -
 DRAWN -
 CHECKED -
 DATE - 03-08-1984

REVISED - 02-04-2009
 REVISED -
 REVISED -
 REVISED -

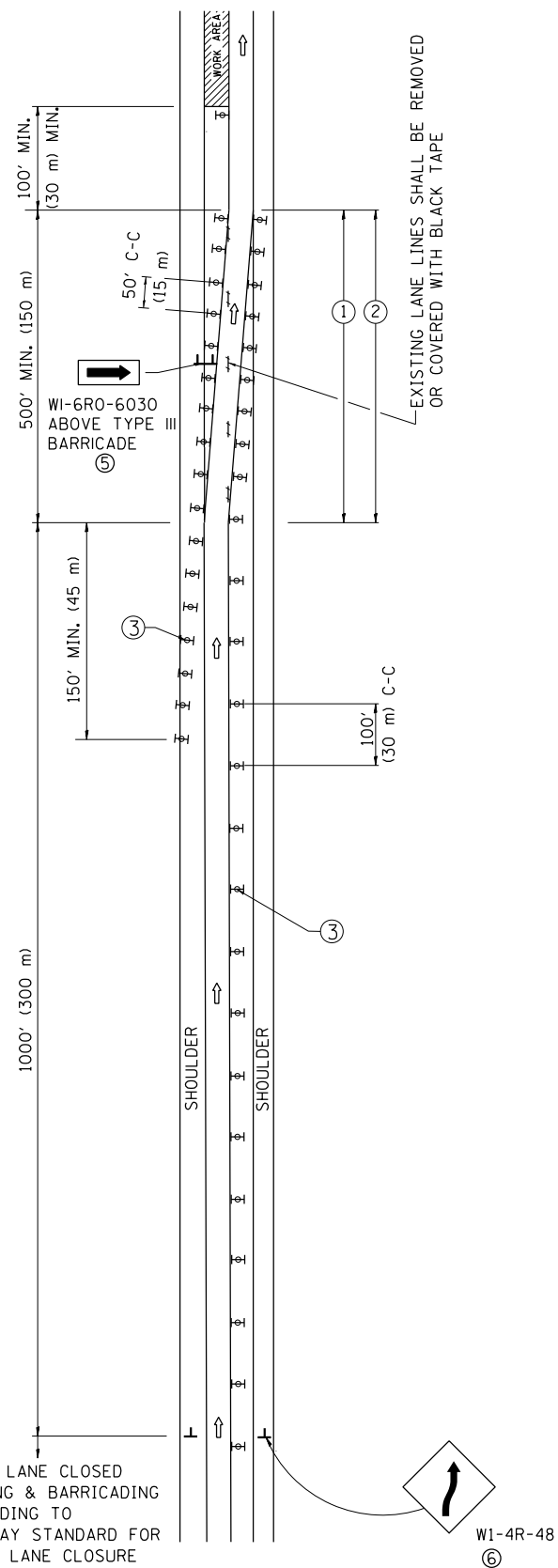
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

MILE POST MARKERS - GORE SIGNS
 MAJOR GUIDE SIGN LAYOUT - ARROWS

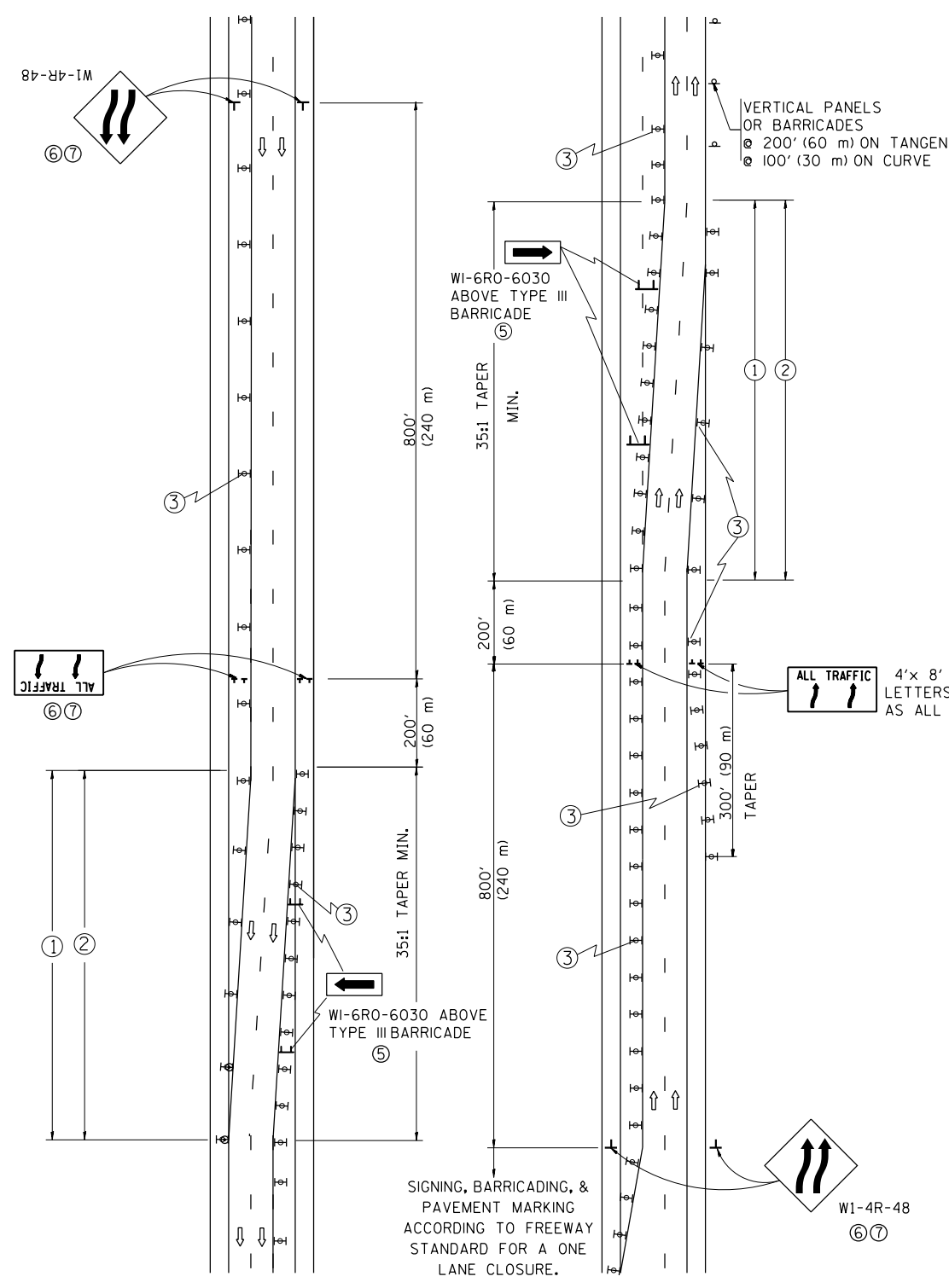
SCALE: NONE SHEET NO. 28 OF 31 SHEETS STA. TO STA.

F.A.I. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
 2013-007R COOK 317 260
 TC-27 (TS-2341-1) CONTRACT NO. 60W25
 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT
 90/94/290

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 24 HOURS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS. TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

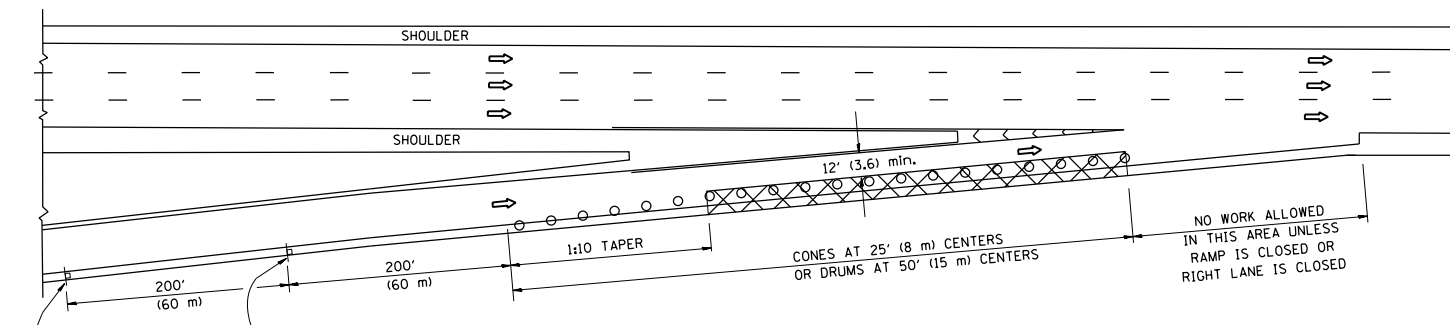
SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- W1-4R-48
- W24-1-48

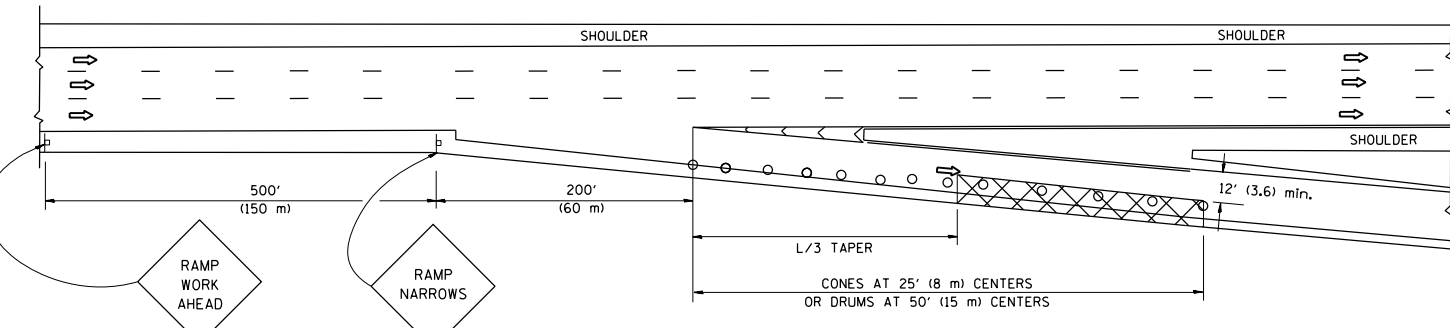
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = pteip2	DESIGNED - DWS	REVISED - JAF 01-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN -					REVISED - SPB 01-07	SCALE: NONE			2013-007R	COOK	317	261A
CHECKED -					REVISED - SPB 12-09	SHEET NO. 30 OF 31 SHEETS			TC-09		CONTRACT NO. 60W25	
PLOT DATE = 5/15/2013					DATE - 02-87	STA. TO STA.			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

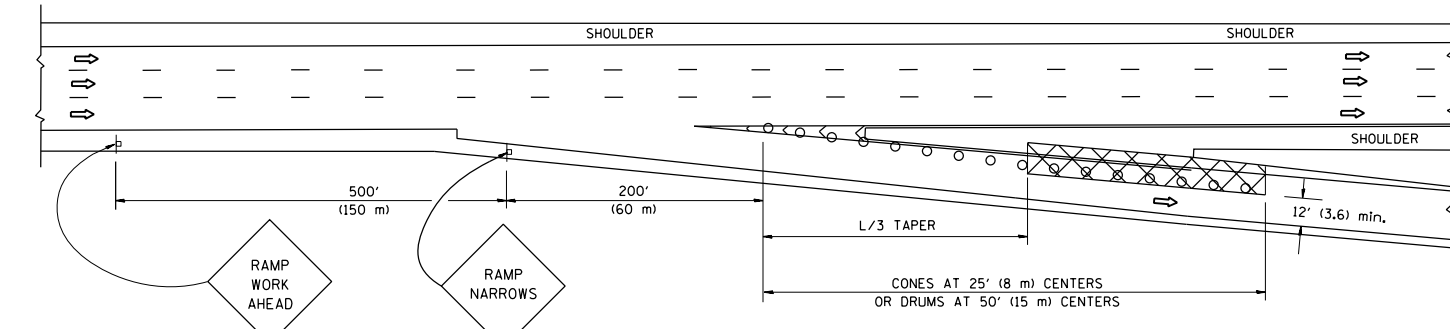
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

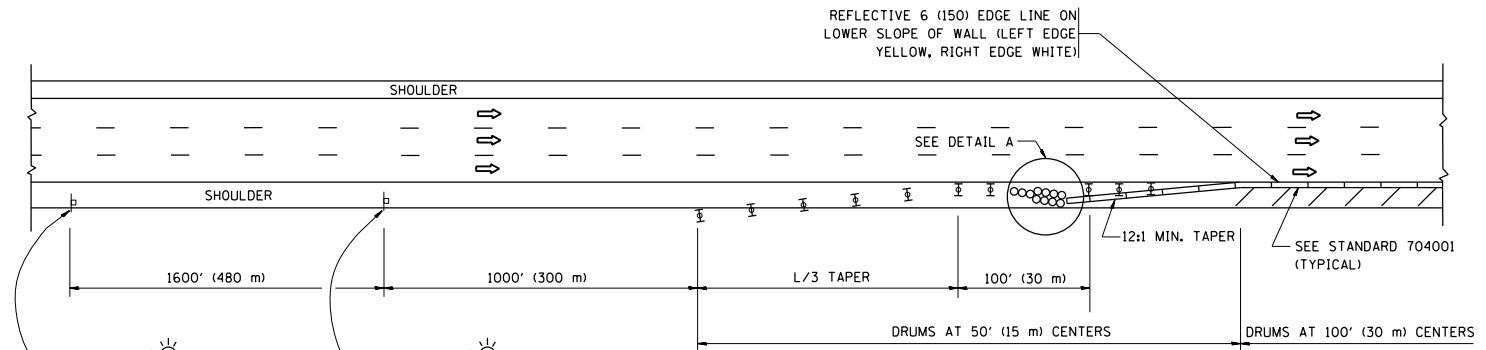
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

GENERAL NOTES

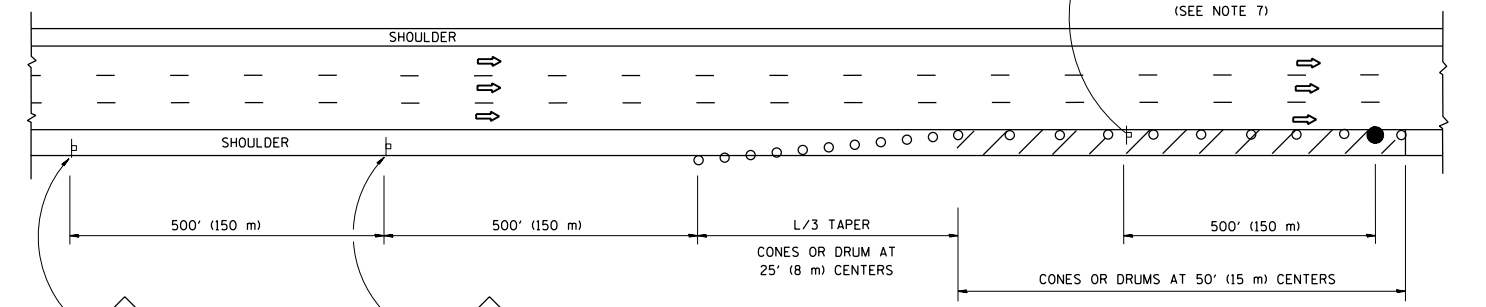
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH
	$L = 0.65(W)(S)$ $L = (W)(S)$
	W = WIDTH OF OFFSET IN FEET (METERS) S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

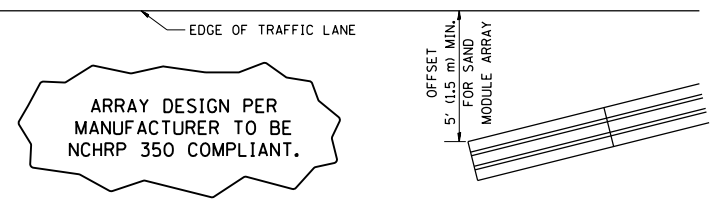


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCRANCH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCRANCHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

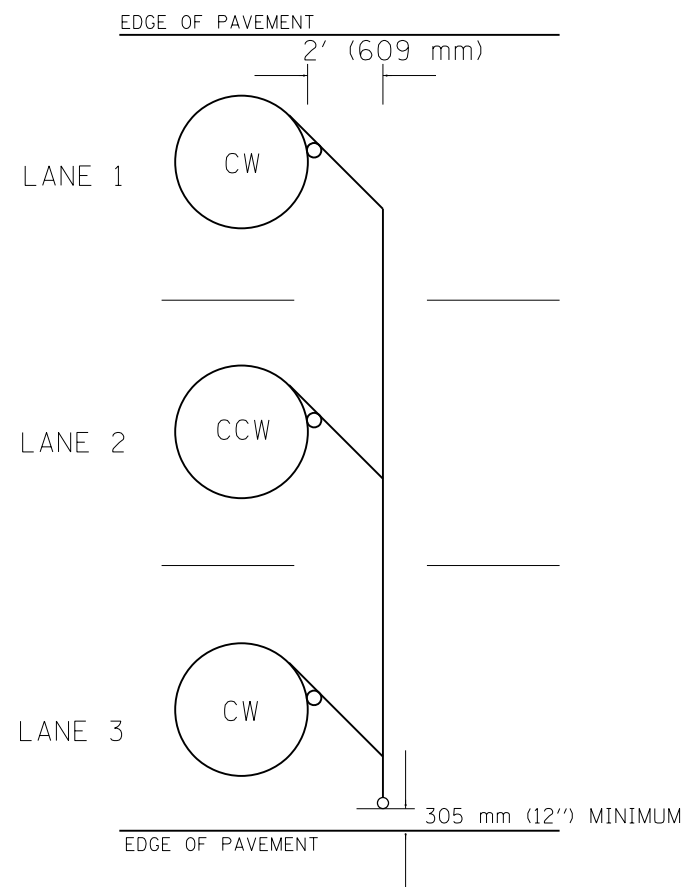
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = pte1p2	DESIGNED -	REVISED - 04-03
\\388039-pwintl.eocomonline.local\pwaec\000\Documents\01 Americas\Transportation\400000\Drawings\1115\000\CAD\006\Road\60W25.Cad\21-000\25-SHT-D1\Detail-31.dgn		CHECKED -	REVISED - S.P.B. 01-07
		DATE - 11-96	REVISED - S.P.B. 12-09

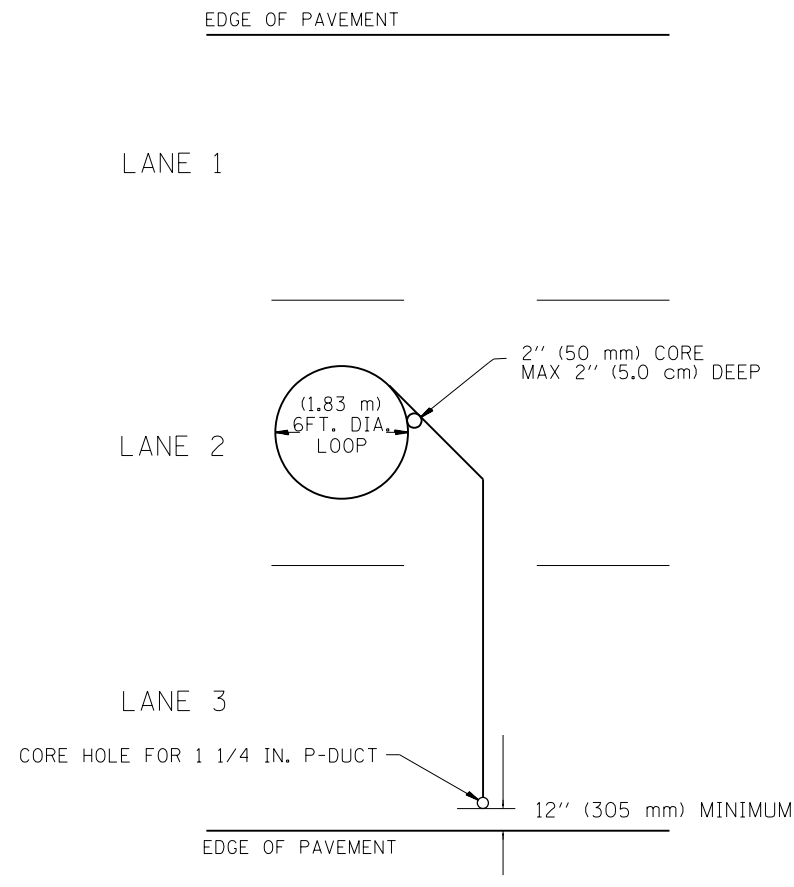
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY			
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES			
SCALE: NONE	SHEET NO. 31 OF 31 SHEETS	STA.	TO STA.

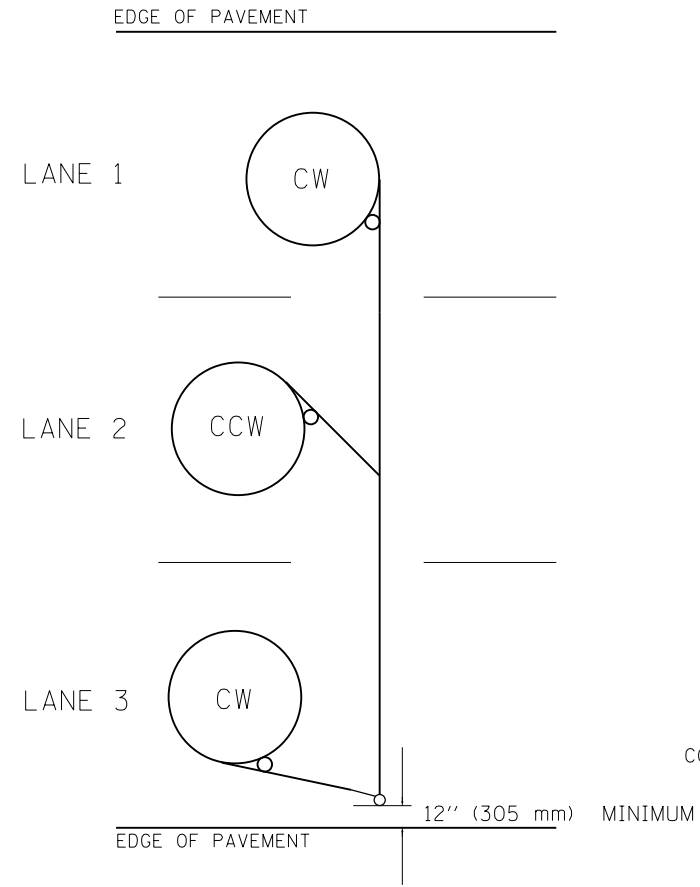
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2013-007R	COOK	317	261B
TC-17		CONTRACT NO. 60W25		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



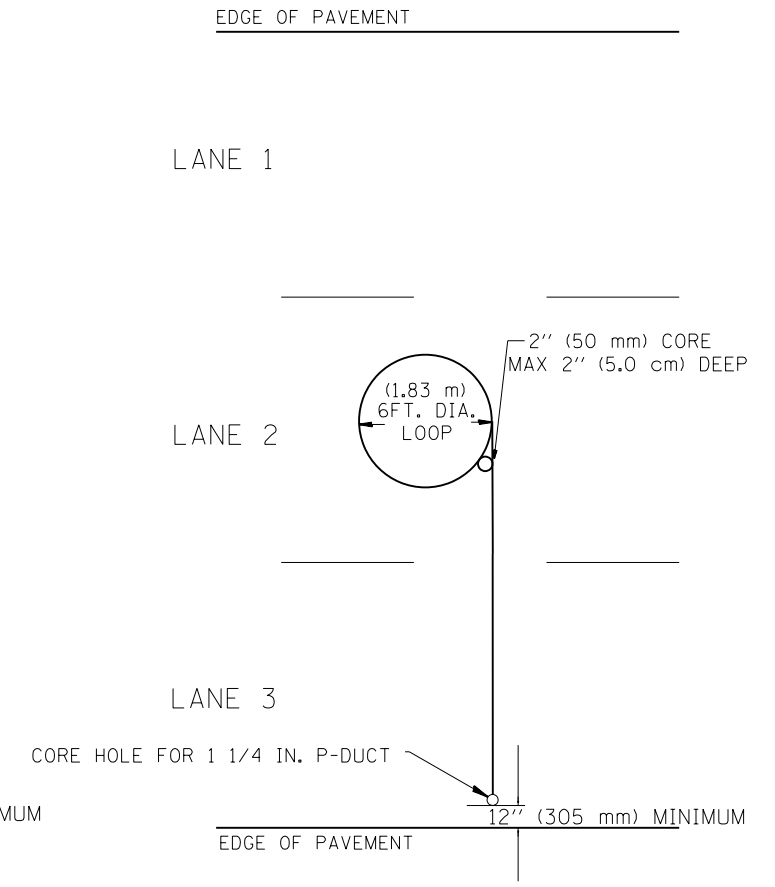
TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



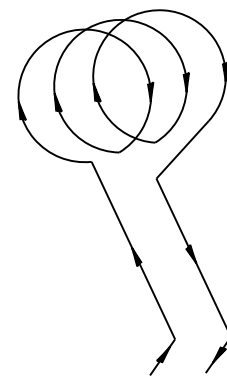
TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP
LAYOUT FOR LANE 2



TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP
LAYOUT FOR LANE 2



WIRING DETAILS

NOTES

1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150' (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

FILE NAME =	USER NAME = lacherj	DESIGNED - R.L.	REVISED - 6/94
c:\pwworking\aeocom\00\lacherj\0204534\0160W25-shr-ITS-05.dgn		DRAWN - G.M.	REVISED - 9/96
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	PLOT DATE = 5/9/2013	DATE - 6-22-94	REVISED -

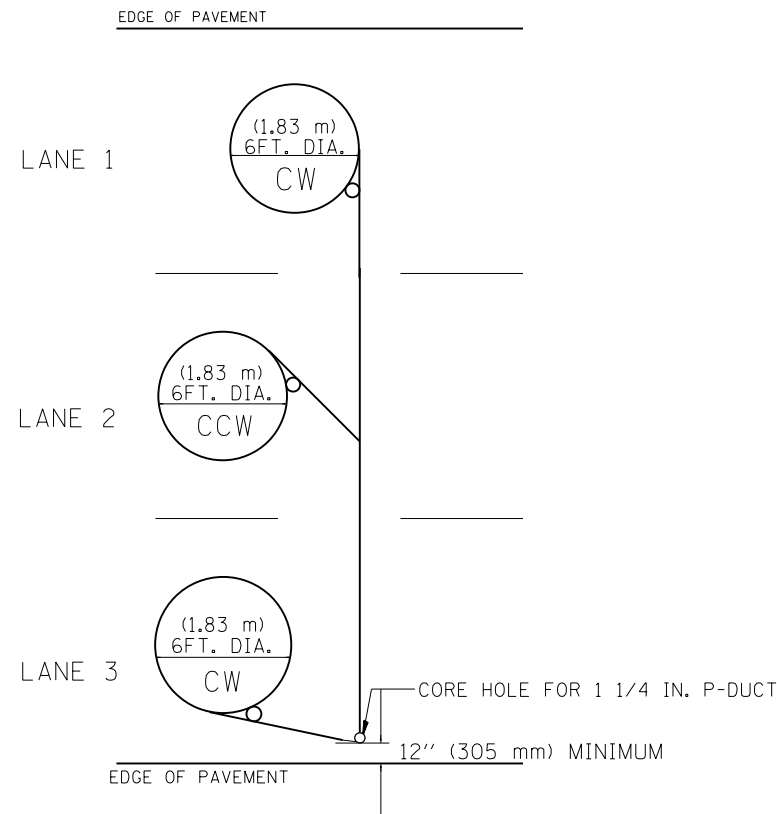
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER

EXISTING ROUND LOOP
INSTALLATION
SCALE: NO SCALE SHEET 1 OF 5 SHEETS STA. TO STA.

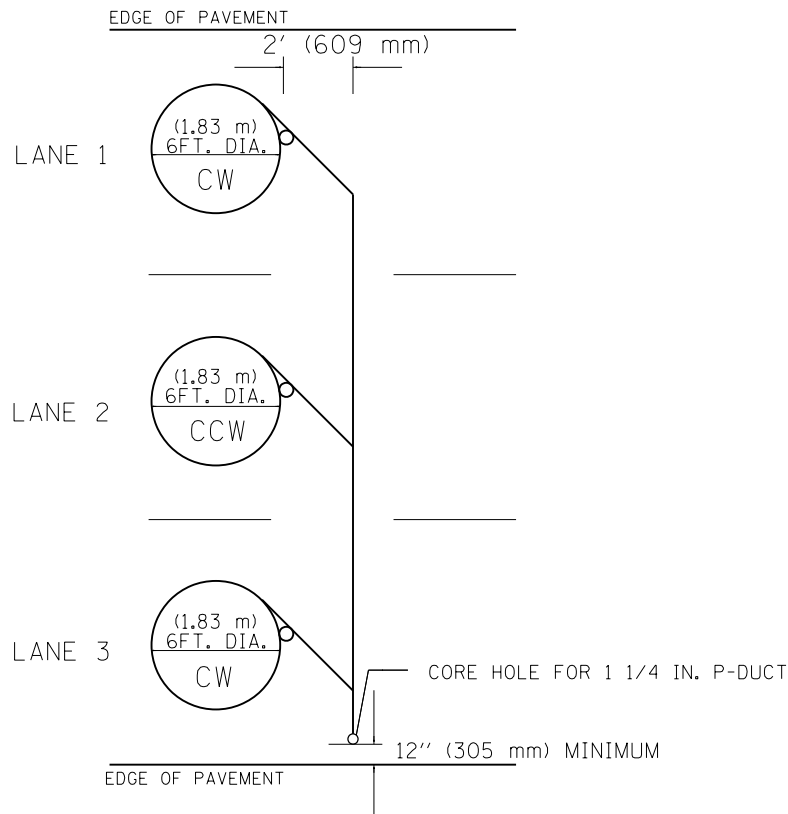
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	262
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

TRAFFIC SYSTEMS CENTER (TY-1TSC-418#1)

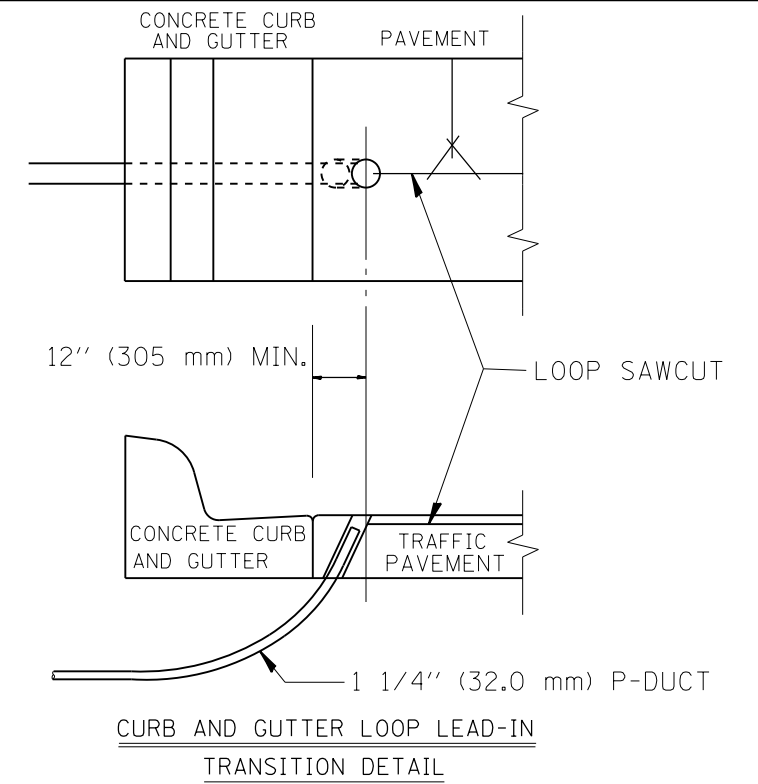
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TYPICAL 6FT. (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



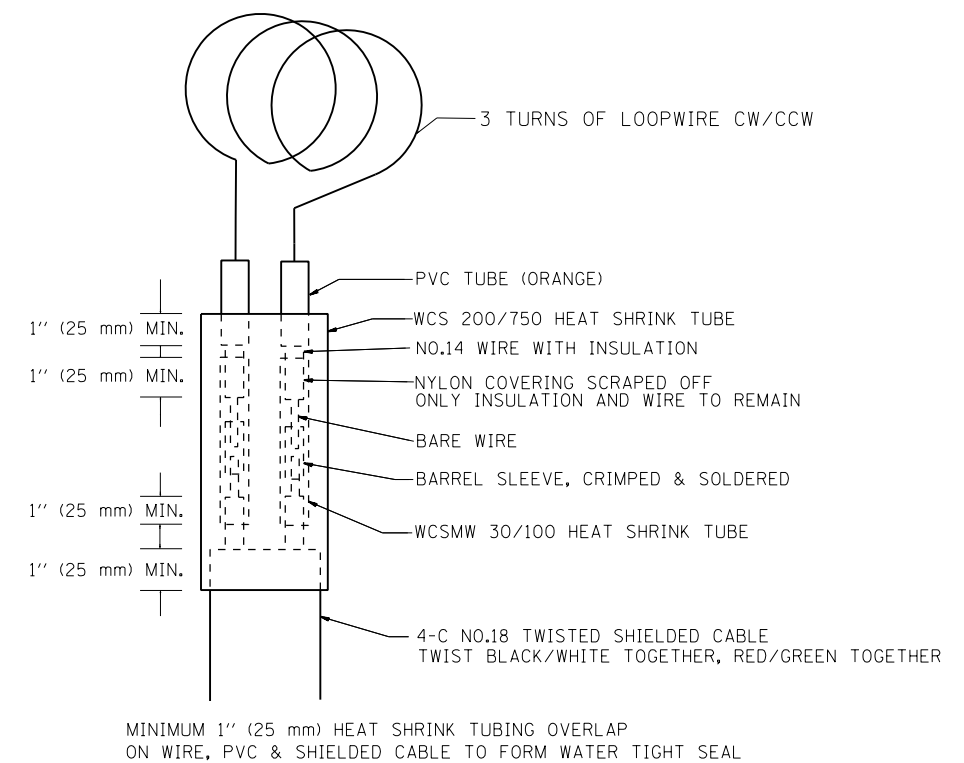
TYPICAL 6FT. (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



CURB AND GUTTER LOOP LEAD-IN
TRANSITION DETAIL

NOTES

1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150FT. (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.



LOOP SPLICING REQUIREMENTS

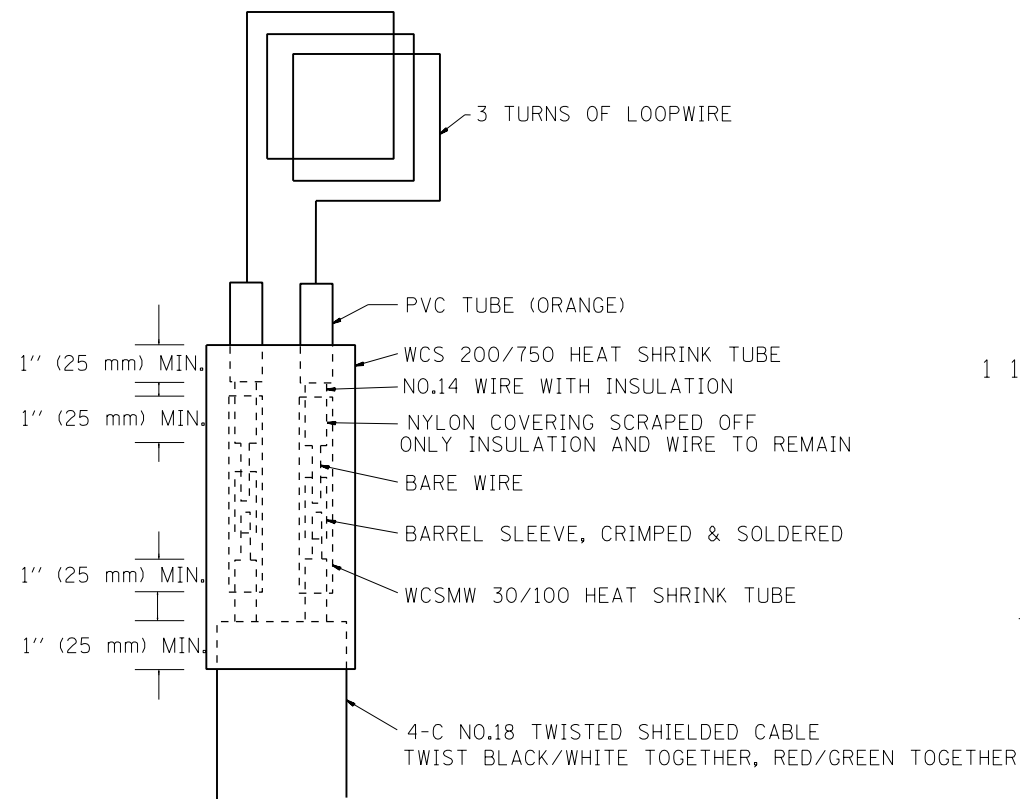
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	PLOT SCALE = 50.0000' / in.	CHECKED - R.L.	REVISED - R.L. 03/2011
	PLOT DATE = 5/9/2013	DATE - 6-22-94	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER

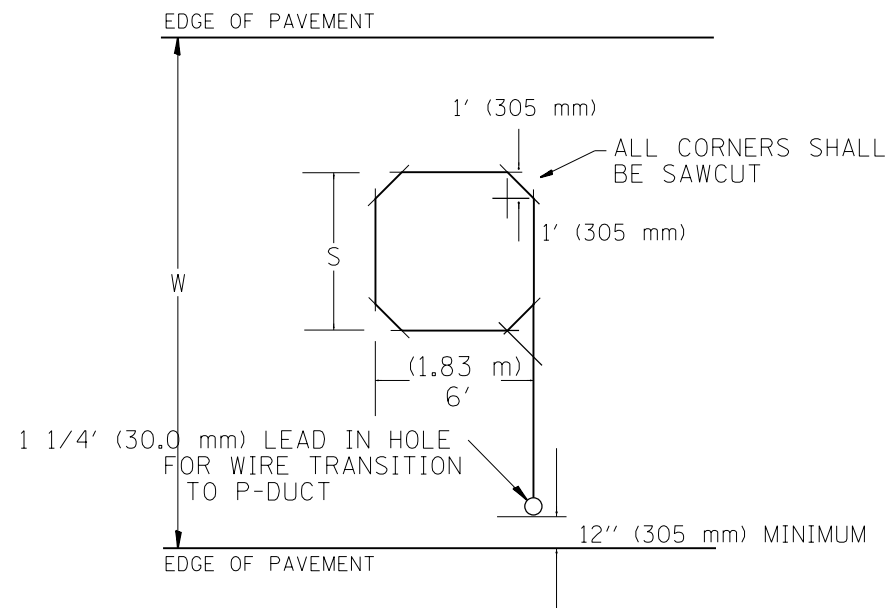
EXISTING ROUND INDUCTION LOOP TYPICALS		F.A.I. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NO SCALE SHEET 2 OF 5 SHEETS STA. TO STA.		90/94/290	2013-007R	COOK	317	263
					CONTRACT NO. 60W25	
ILLINOIS FED. AID PROJECT						

TRAFFIC SYSTEMS CENTER (TY-1TSC-418#2)

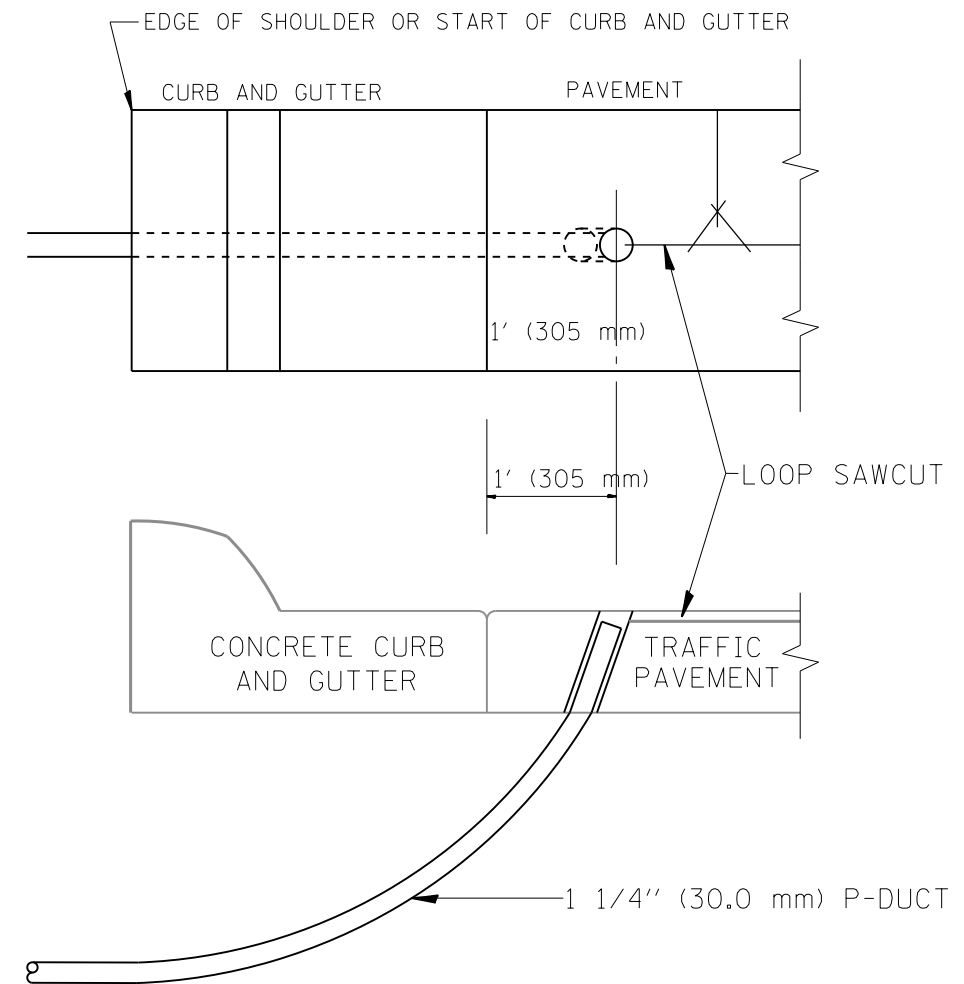


LOOP SPLICING REQUIREMENTS

MINIMUM 1" (25 mm) HEAT SHRINK TUBING OVERLAP ON WIRE, PVC & SHIELDED CABLE TO FORM WATER TIGHT SEAL



TYPICAL "S" FT. BY 6' (1.83 m) INDUCTION LOOP SAWCUT LAYOUT FOR RAMPS



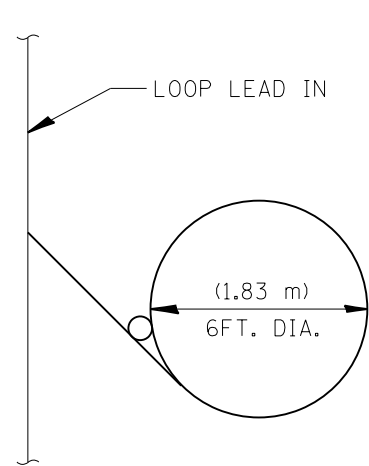
CURB AND GUTTER LOOP LEAD-IN TRANSITION DETAIL

WIDTH (W)	WIDTH (S)
12' (3.7 m)	8' (2.5 m)
13' (4.0 m)	9' (2.8 m)
14' (4.3 m)	10' (3.1 m)
15' (4.6 m)	11' (3.4 m)
16' (4.9 m)	12' (3.7 m)
17' (5.2 m)	13' (4.0 m)
18' (5.5 m)	14' (4.3 m)
19' (5.8 m)	15' (4.6 m)
20' (6.1 m)	18' (4.9 m)
21' (6.4 m)	17' (5.2 m)
22' (6.7 m)	18' (5.5 m)
23' (7.0 m)	19' (5.8 m)
24' (7.3 m)	20' (6.1 m)
25' (7.6 m)	21' (6.4 m)

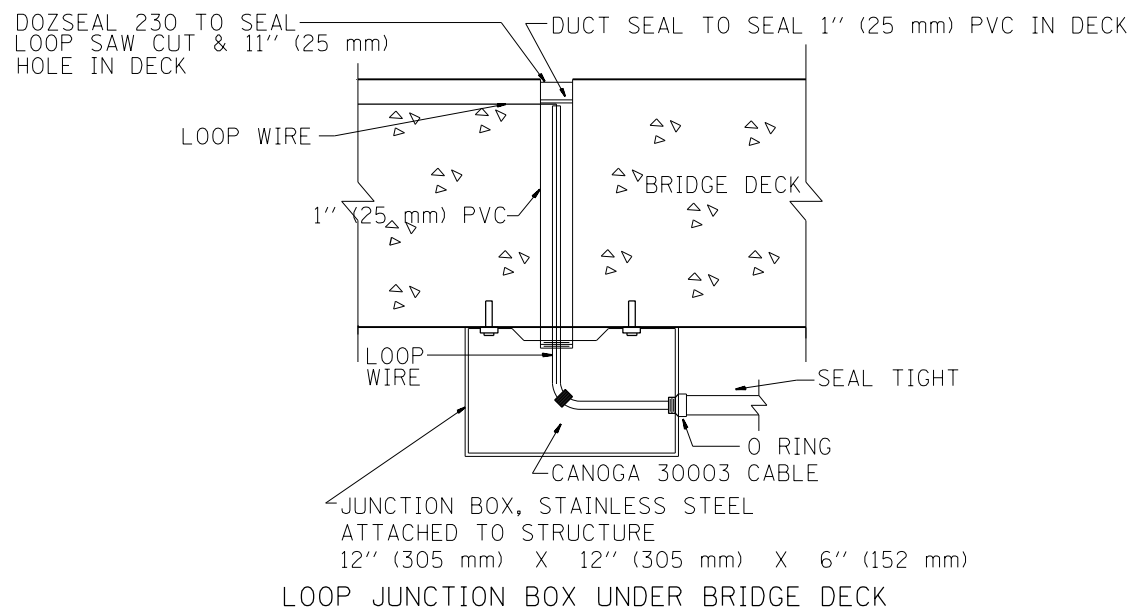
NOTES

1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150' (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

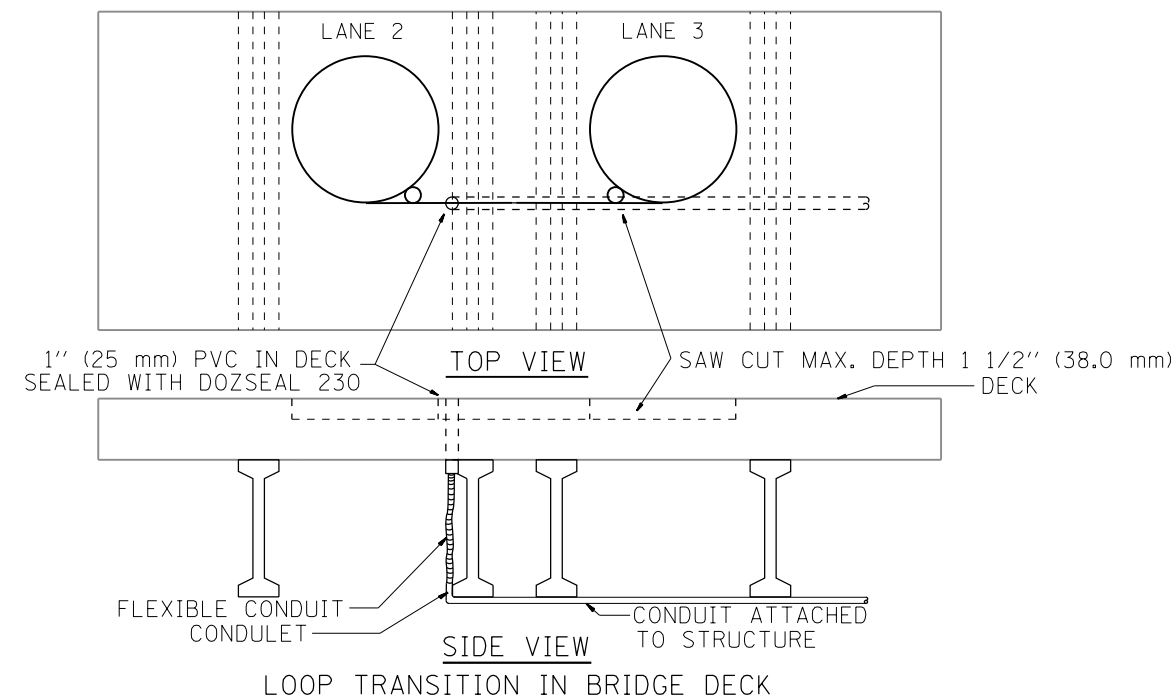
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	PLOT SCALE = 50.0000' / in.	CHECKED - R.L.	REVISED - 05/96		SCALE: NO SCALE	SHEET 3 OF 5 SHEETS	STA. TO STA.	CONTRACT NO. 60W25				
	PLOT DATE = 5/9/2013	DATE - 6-22-94	REVISED - 10/96		ILLINOIS FED. AID PROJECT							
TRAFFIC SYSTEMS CENTER (TY-1TSC-418#3)												



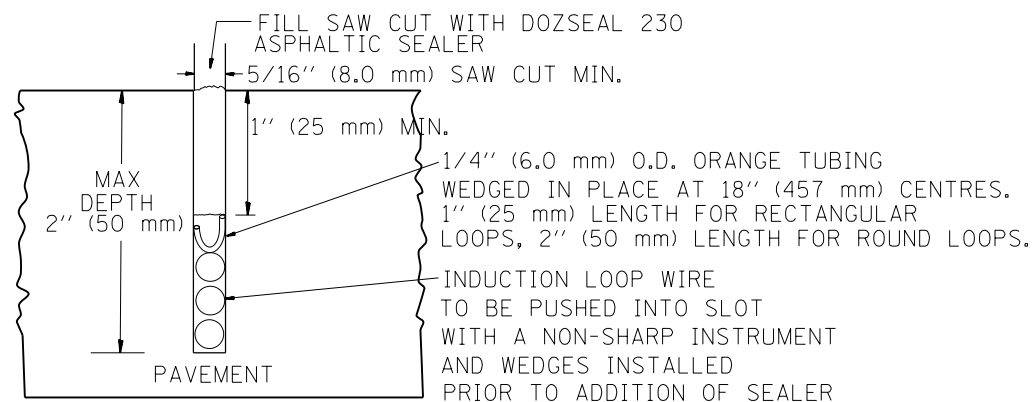
TYPICAL LOOP SAWCUT LAYOUT



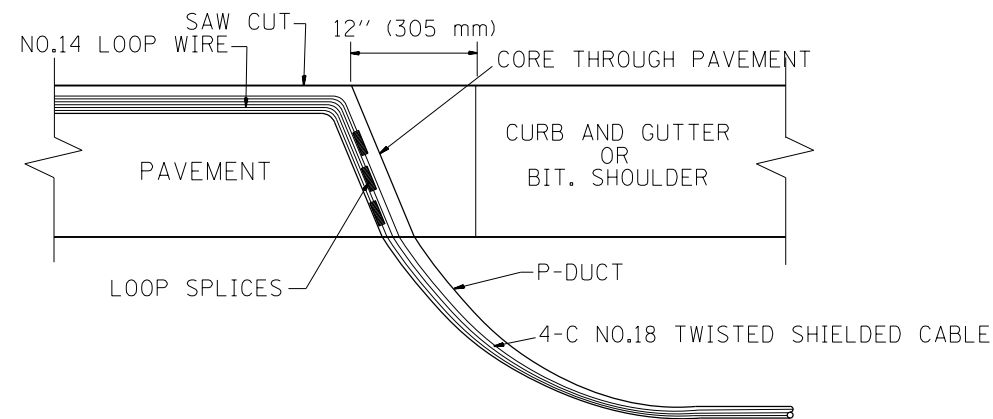
LOOP JUNCTION BOX UNDER BRIDGE DECK



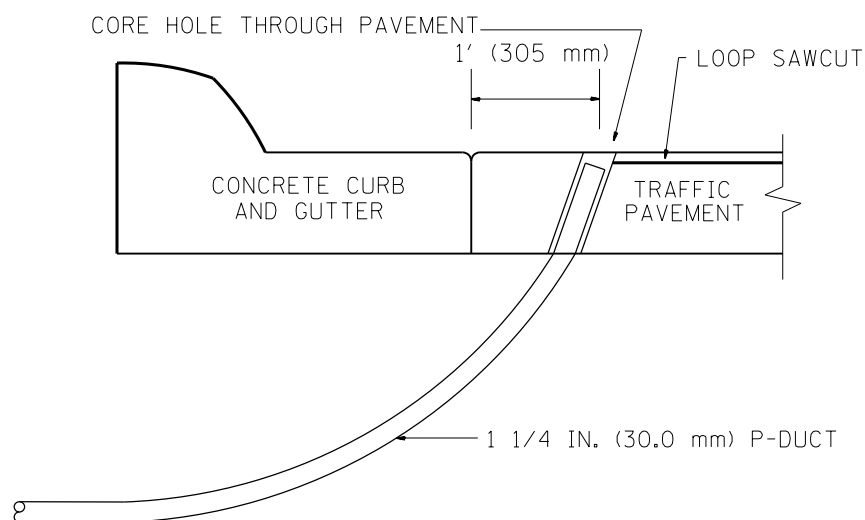
LOOP TRANSITION IN BRIDGE DECK



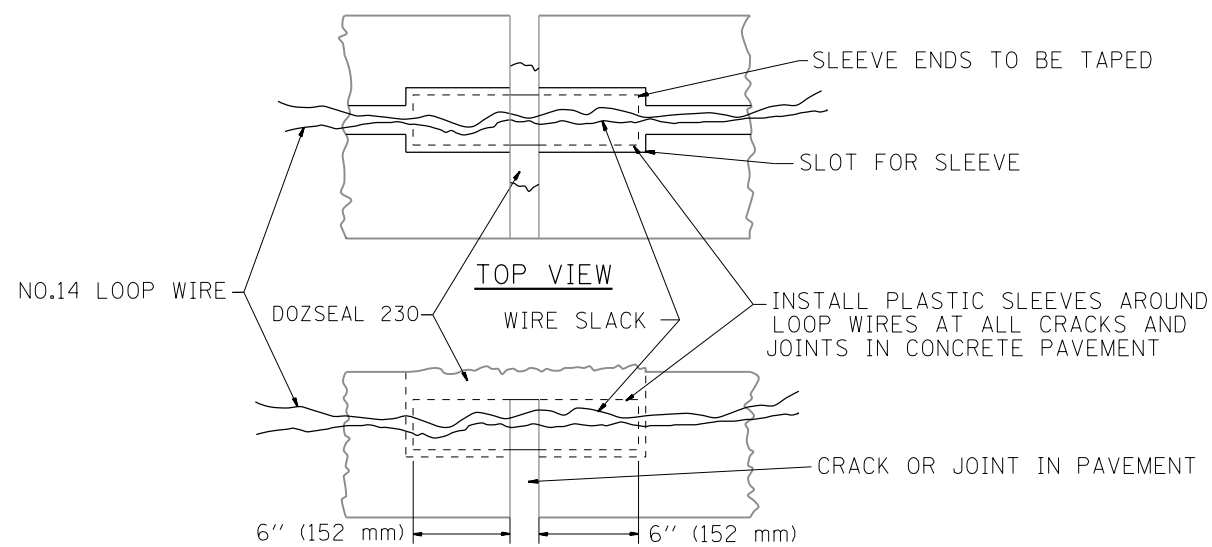
LOOP CROSS SECTION IN PAVEMENT



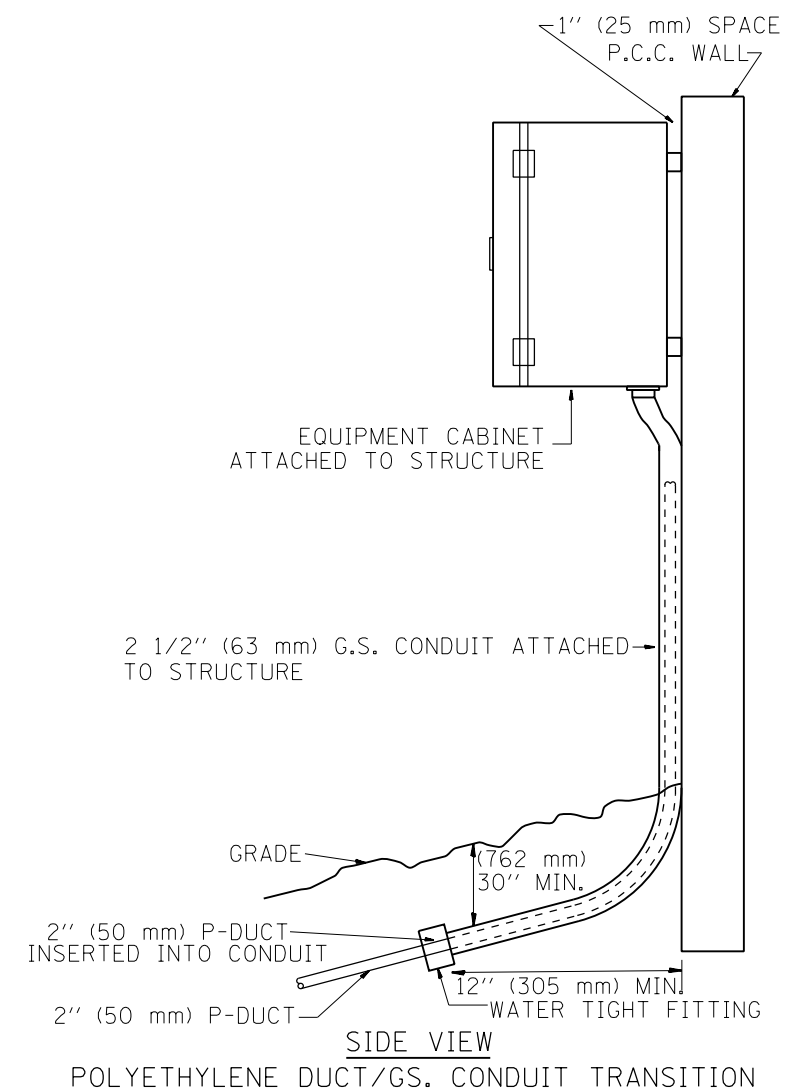
MULTIPLE LOOP SPLICING



SIDE SECTION LOOP LEAD-IN TRANSITION DETAIL



PAVEMENT CRACK TRANSVERSE SLEEVE



POLYETHYLENE DUCT/GS. CONDUIT TRANSITION

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER

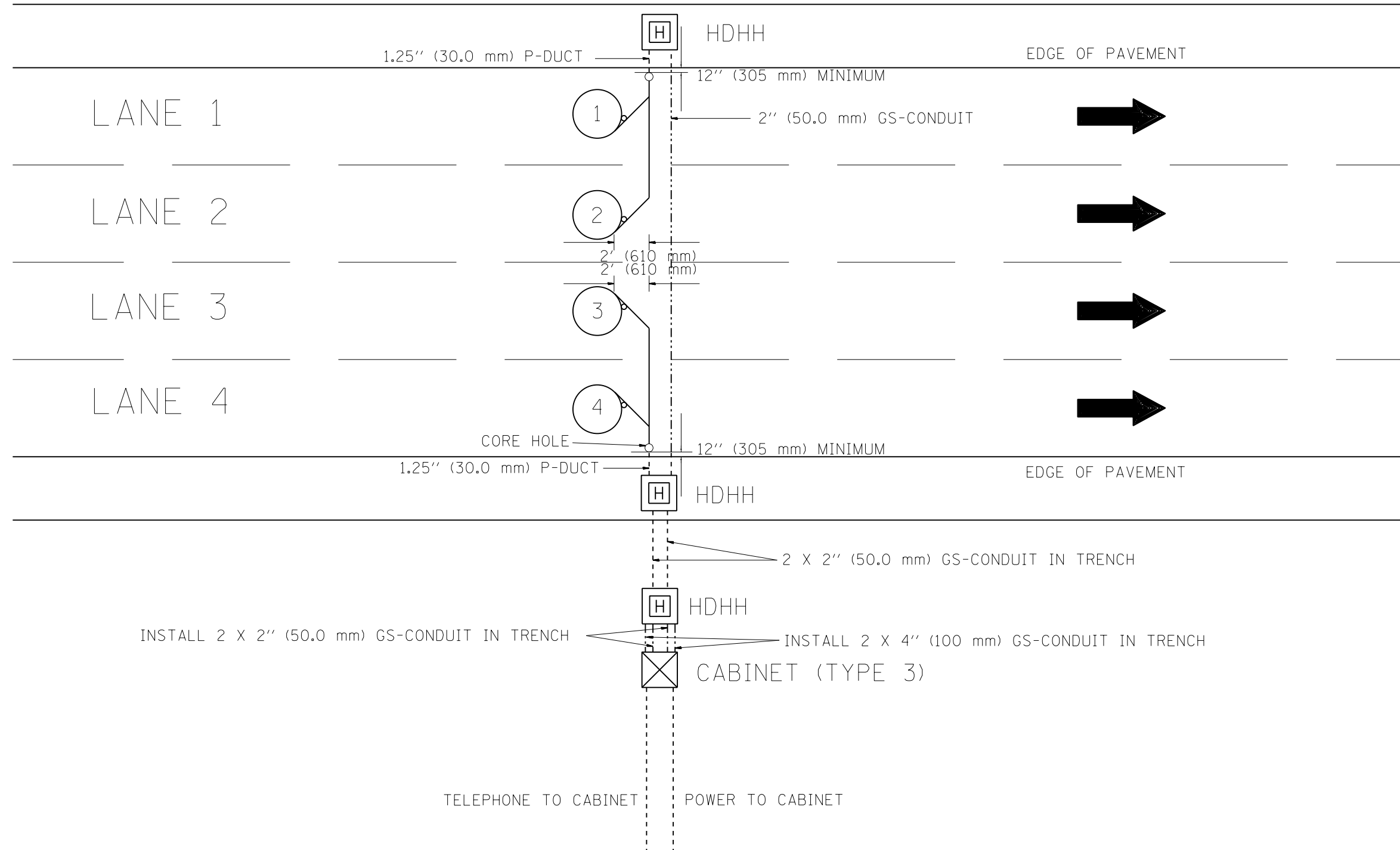
LOOP, CONDUIT & DUCT
INSTALLATION DETAILS

SCALE: NO SCALE SHEET 4 OF 5 SHEETS STA. TO STA.

F.A.I. R.T.E. 90/94/290	SECTION 2013-007R	COUNTY COOK	TOTAL SHEETS 317	SHEET NO. 265
CONTRACT NO. 60W25			ILLINOIS FED. AID PROJECT	

TRAFFIC SYSTEMS CENTER (TY-1TSC-418#4)

NOTE: ALL MAIN LINE LOOPS ARE ROUND 6' DIA. (1.83 m)



NOTES

1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150' (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

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	PLOT DATE = 5/9/2013	DATE - 6-27-94	REVISED -

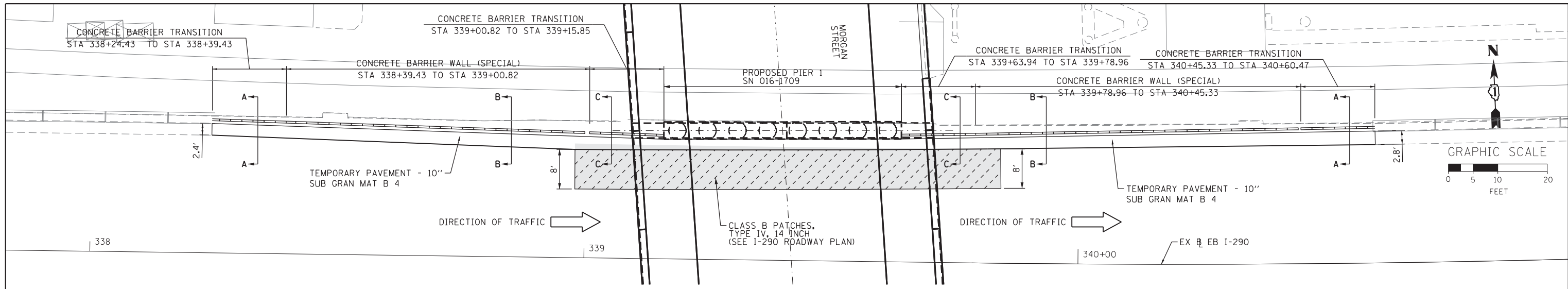
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER

NEW CONSTRUCTION ROUND INDUCTION LOOP
TYPICAL INSTALLATION
SCALE: NO SCALE SHEET 5 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	266
CONTRACT NO. 60W25			ILLINOIS FED. AID PROJECT	

TRAFFIC SYSTEMS CENTER (TY-1TSC-418#6)

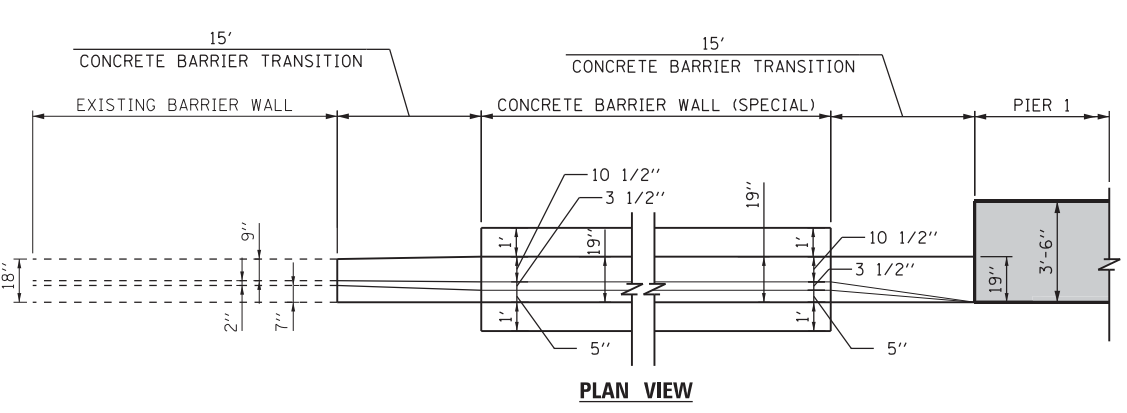
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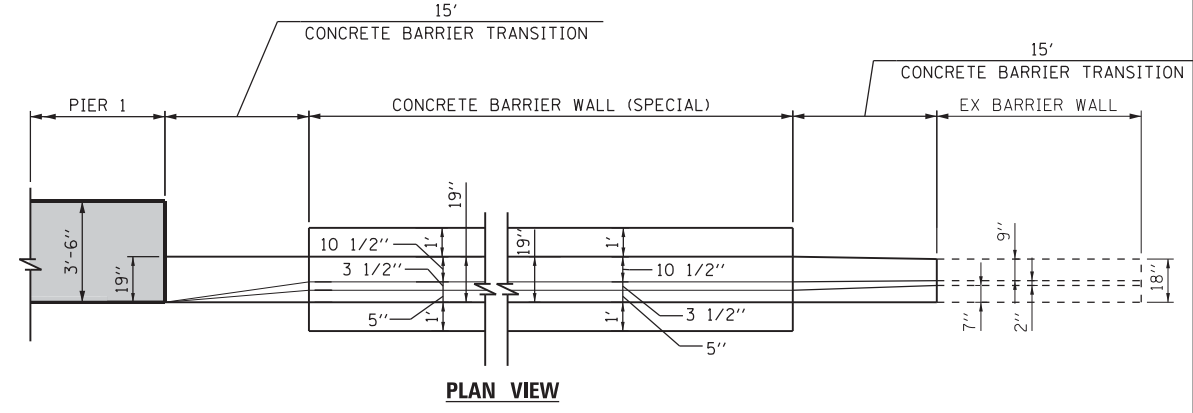
PLAN VIEW
CONCRETE BARRIER WALL (SPECIAL)
TO PROPOSED PIER 1 (SN 016-1709)

NOTES:

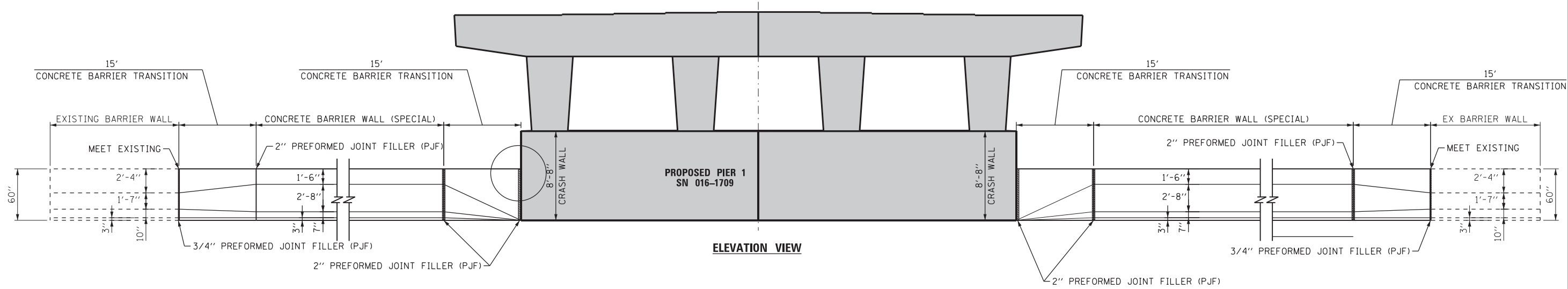
1. EXPANSION JOINTS SHOWN ON THIS DRAWING SHALL BE PREFORMED JOINT MATERIAL (BITUMINOUS TYPE) FILLER SHALL MEET AASHTO DESIGNATION M-33.
2. ALL WORK DETAILED HEREIN SHALL BE INCLUDED IN THE COST OF THE VARIOUS CONCRETE BARRIER PAY ITEMS UNLESS OTHERWISE NOTED.
3. PREFORMED JOINT FILLER SHALL BE INCLUDED IN THE COST OF THE CONCRETE BARRIER TRANSITION.
4. JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH ARTICLE 637.08 OF THE STANDARD SPECIFICATIONS
5. TWO VERTICAL EPOXY COATED, NO. 8 TIE BARS, 10" LONG, SHALL BE PLACED AT EQUIDISTANT SPACING ALONG THE CONCRETE BARRIER TRANSITION. TIE BARS SHALL BE INCLUDED IN THE COST OF CONCRETE BARRIER TRANSITION.
6. HORIZONTAL TIE BARS SHALL BE NO. 6 EPOXY COATED, 24" LONG, 24" C-C AND SHALL BE INCLUDED IN THE COST OF THE BARRIER BASE.
7. SEE SHEET ROADWAY DETAILS SHEET 3 OF 9 FOR ADDITIONAL DETAILS



PLAN VIEW



PLAN VIEW



ELEVATION VIEW

FILE PATH = p:\388035-p\m\l\escom\l\l\local\p\AEC\000\Documents\01_Americas\Transportation\60269938_Circle\Phase_1\000_CAD\005_Roadway\Sheets\60W25-Contract\0160W25-sht-Detail-01.dgn



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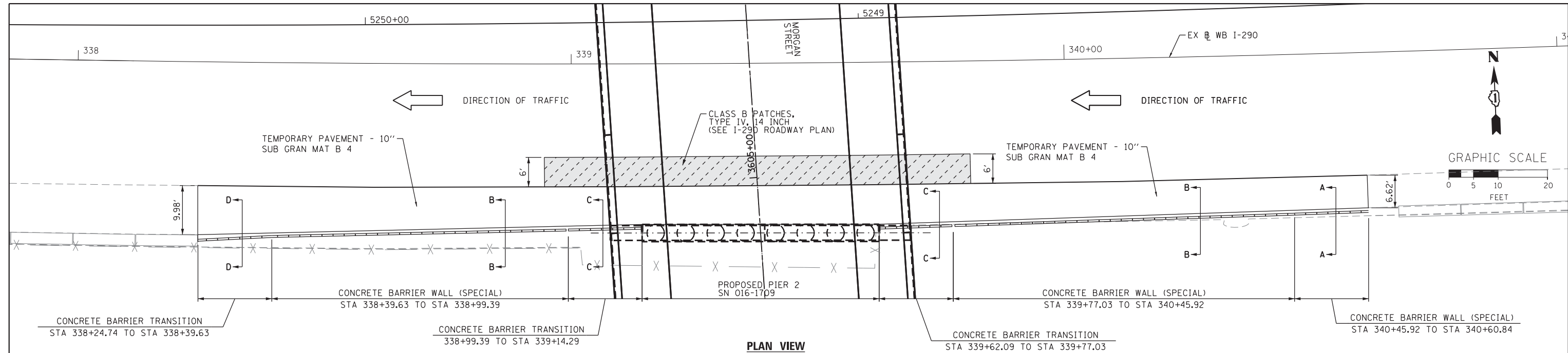
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DRAWN - JWM/OPS	REVISED -
CHECKED - DBM	REVISED -
DATE - 6/17/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY DETAILS

SCALE: N.T.S. SHEET 1 OF 9 SHEETS STA. TO STA.

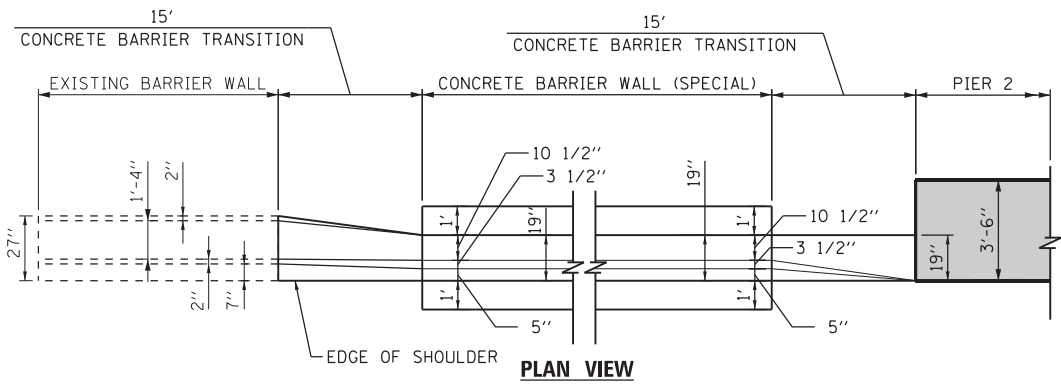
F.A.I. RTE. 90/94/290	SECTION 2013-007R	COUNTY COOK	TOTAL SHEETS 317	SHEET NO. 267
CONTRACT NO. 60W25			ILLINOIS FED. AID PROJECT	



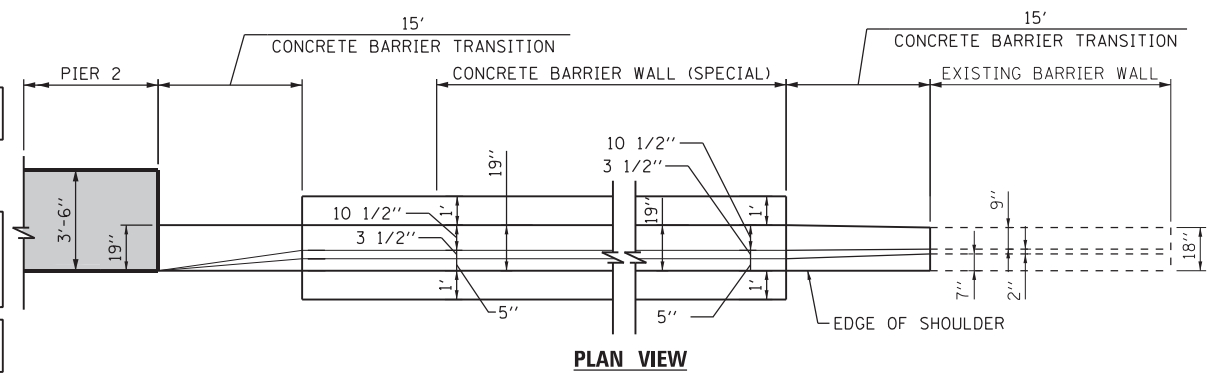
PLAN VIEW
CONCRETE BARRIER WALL (SPECIAL) TRANSITION
TO PROPOSED PIER 2 (SN 016-1709)

NOTES:

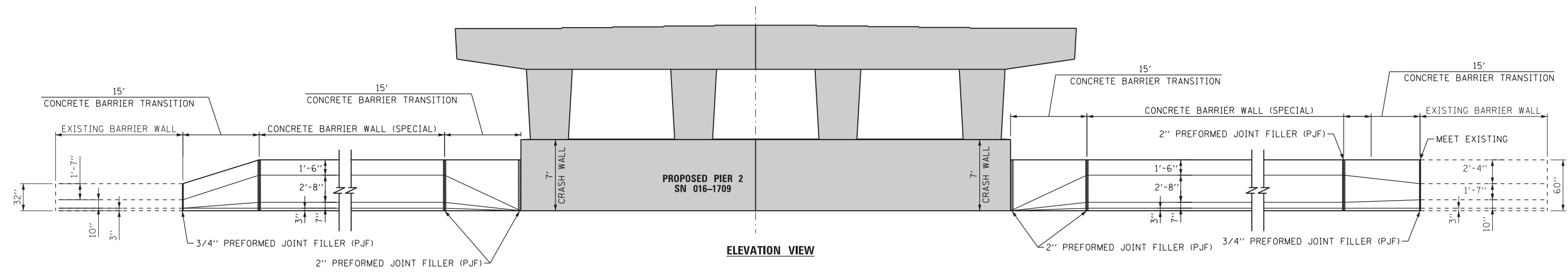
1. EXPANSION JOINTS SHOWN ON THIS DRAWING SHALL BE PREFORMED JOINT MATERIAL (BITUMINOUS TYPE) FILLER SHALL MEET AASHTO DESIGNATION M-33.
2. ALL WORK DETAILED HEREIN SHALL BE INCLUDED IN THE COST OF THE VARIOUS CONCRETE BARRIER PAY ITEMS UNLESS OTHERWISE NOTED.
3. PREFORMED JOINT FILLER SHALL BE INCLUDED IN THE COST OF THE CONCRETE BARRIER TRANSITION.
4. JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH ARTICLE 637.08 OF THE STANDARD SPECIFICATIONS
5. TWO VERTICAL EPOXY COATED, NO. 8 TIE BARS, 10" LONG, SHALL BE PLACED AT EQUIDISTANT SPACING ALONG THE CONCRETE BARRIER TRANSITION. TIE BARS SHALL BE INCLUDED IN THE COST OF CONCRETE BARRIER TRANSITION.
6. HORIZONTAL TIE BARS SHALL BE NO. 6 EPOXY COATED, 24" LONG, 24" C-C AND SHALL BE INCLUDED IN THE COST OF THE BARRIER BASE.
7. SEE SHEET ROADWAY DETAILS SHEET 3 OF 9 FOR ADDITIONAL DETAILS



PLAN VIEW



PLAN VIEW



ELEVATION VIEW

FILE PATH = p:\388035-p\m\l\escomon\lme\local\p\w\ec\000\Documents\01_Americas\Transportation\60269938_Circle\Phase_1\000_CAD\005_Roadway\Sheets\60W25-Contract\0160W25-sht-Detail-02



D160W25-sht-Detail-02
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 PLOT SCALE = 10.0000" / 1"
 PLOT DATE = 6/14/2013

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DRAWN - JWM/OPS	REVISED -
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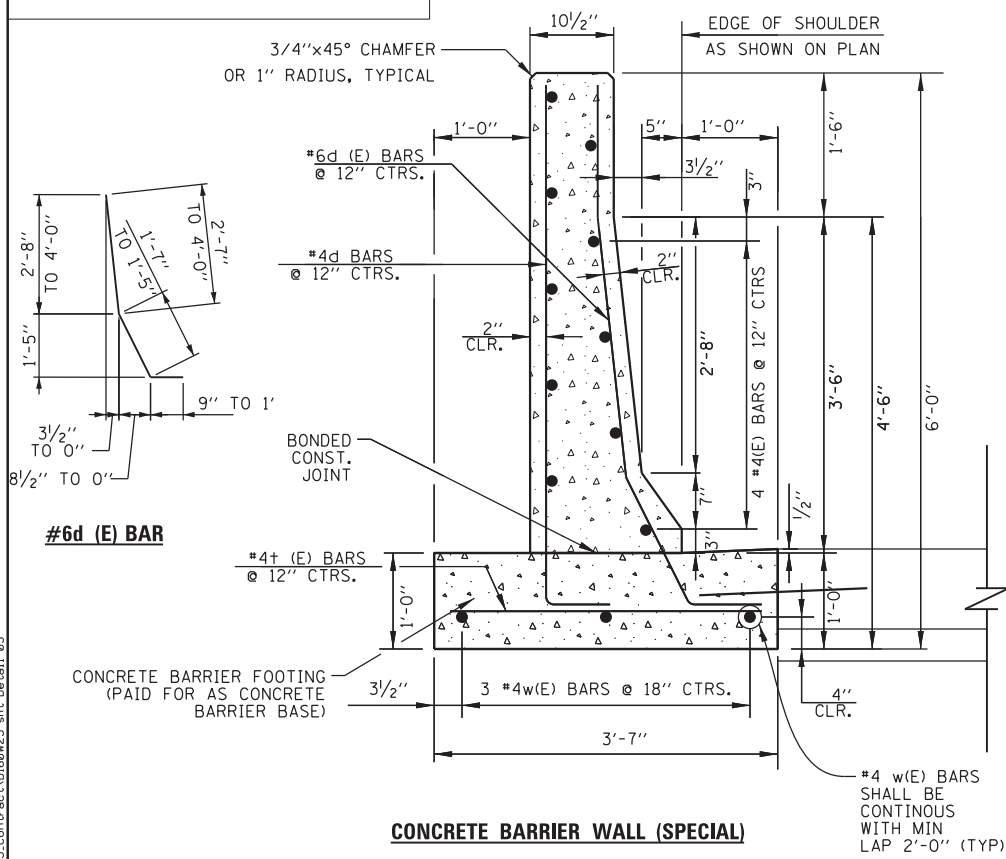
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY DETAILS

SCALE: N.T.S. SHEET 2 OF 9 SHEETS STA. TO STA.

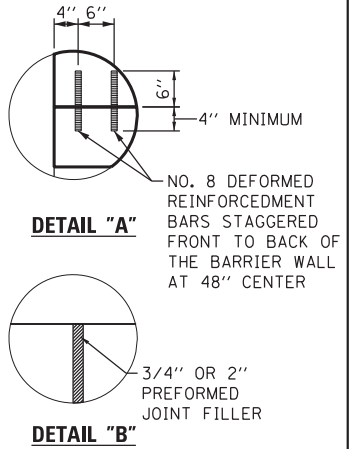
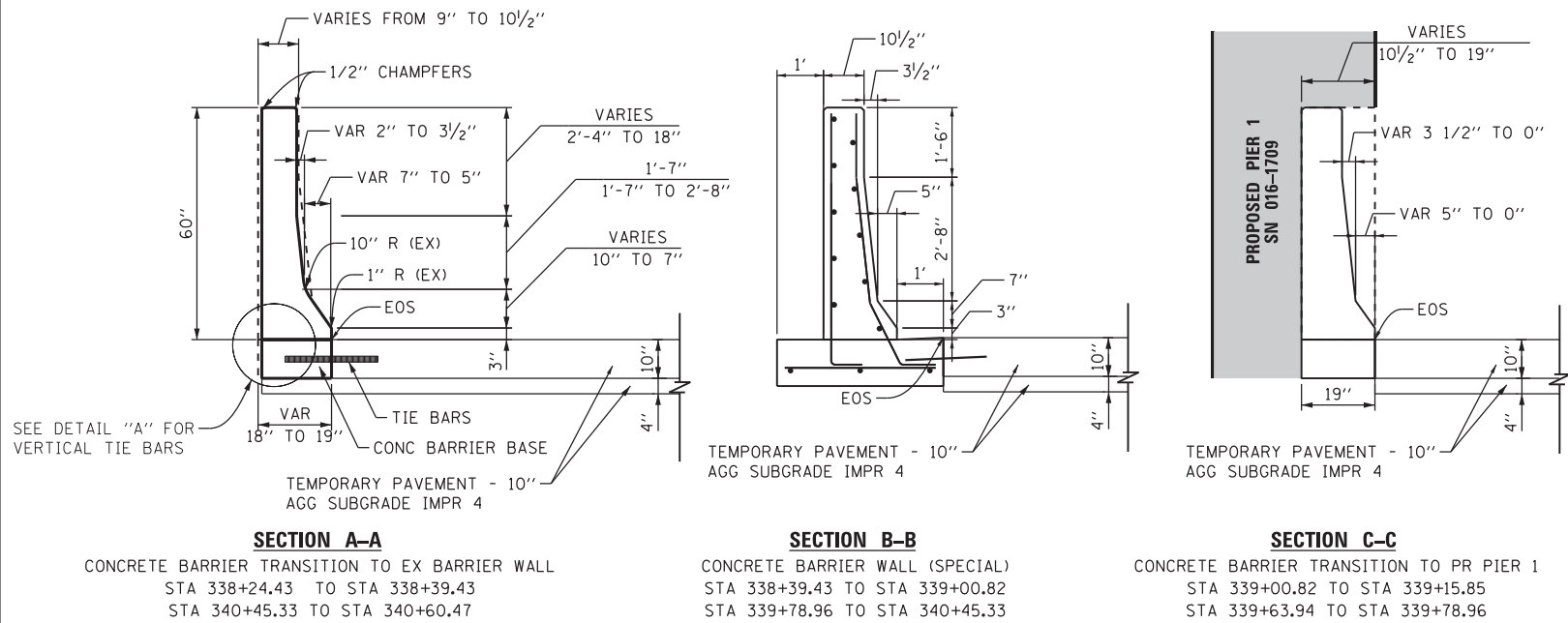
F.A.I. RTE. 90/94/290	SECTION 2013-007R	COUNTY COOK	TOTAL SHEETS 317	SHEET NO. 268
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

CONCRETE BARRIER WALL (SPECIAL) DETAILS



CONCRETE BARRIER WALL (SPECIAL)

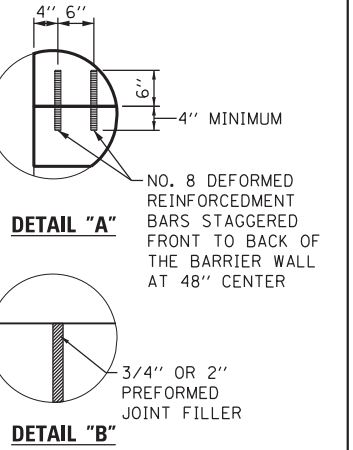
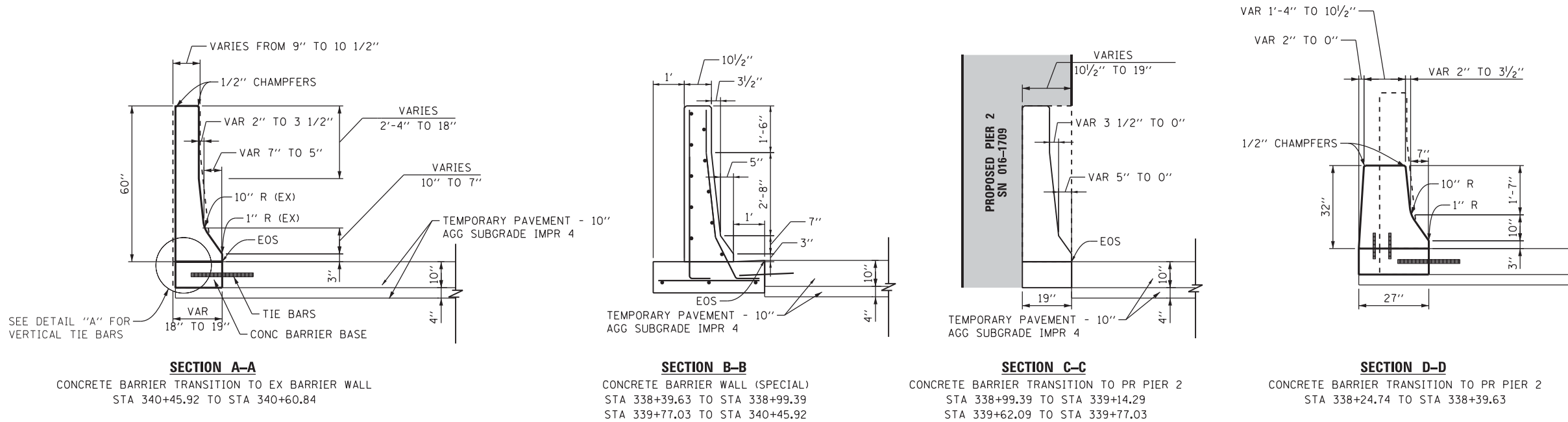
CONCRETE BARRIER WALL (SPECIAL) AND CONCRETE BARRIER TRANSITION AT PIER 1



DETAIL "A"
NO. 8 DEFORMED REINFORCEMENT BARS STAGGERED FRONT TO BACK OF THE BARRIER WALL AT 48" CENTER

DETAIL "B"
3/4" OR 2" PREFORMED JOINT FILLER

CONCRETE BARRIER WALL (SPECIAL) AND CONCRETE BARRIER TRANSITION AT PIER 2



DETAIL "A"
NO. 8 DEFORMED REINFORCEMENT BARS STAGGERED FRONT TO BACK OF THE BARRIER WALL AT 48" CENTER

DETAIL "B"
3/4" OR 2" PREFORMED JOINT FILLER

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DATE - 6/17/2013

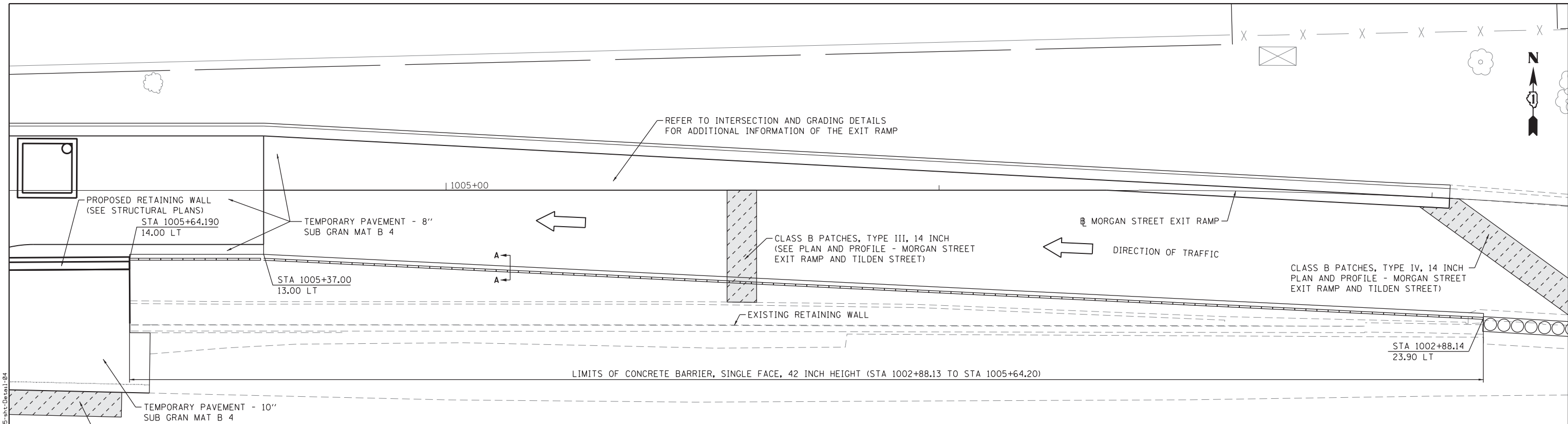
REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

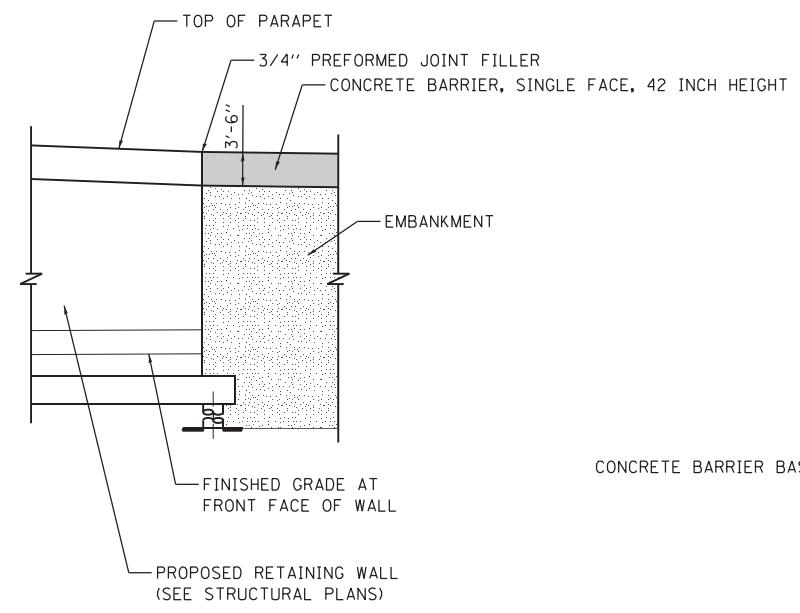
ROADWAY DETAILS

SCALE: N.T.S. SHEET 3 OF 9 SHEETS STA. TO STA.

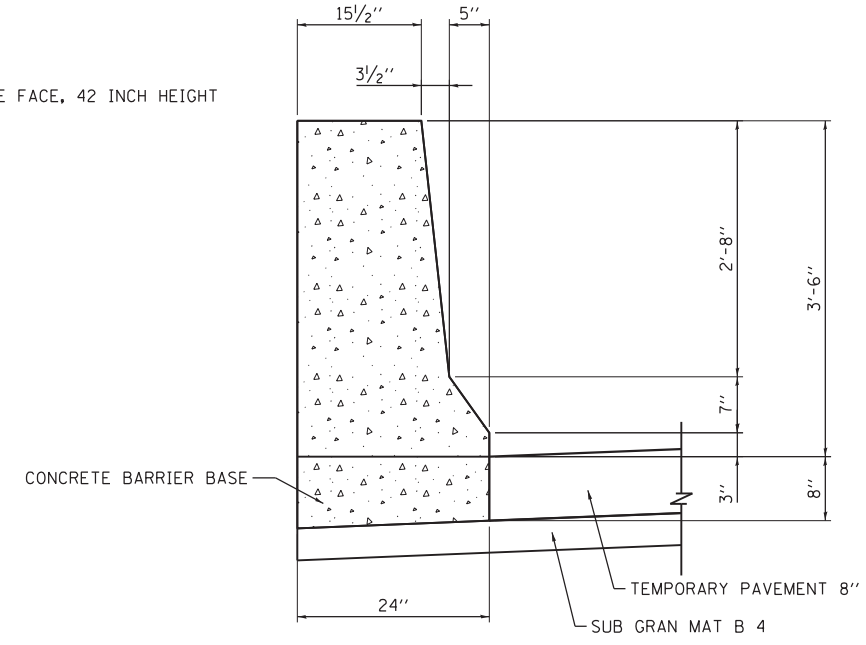
F.A.I. RTE. 90/94/290	SECTION 2013-007R	COUNTY COOK	TOTAL SHEETS 317	SHEET NO. 269
CONTRACT NO. 60W25			ILLINOIS FED. AID PROJECT	



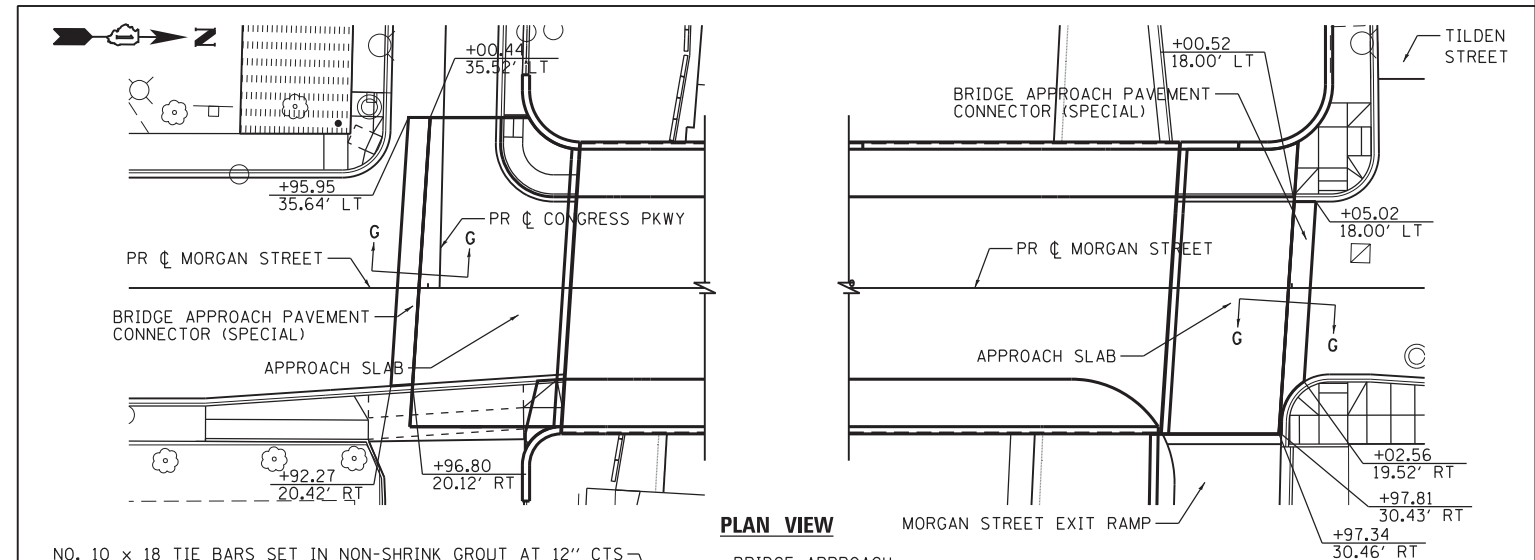
PLAN VIEW



ELEVATION VIEW



SECTION A-A
CONCRETE BARRIER, SINGLE FACE
42 INCH HEIGHT



PLAN VIEW

SECTION G-G

PAVEMENT CONNECTOR (SPECIAL) DETAILS

NOTES:

1. PREFORMED JOINT FILLER SHALL BE INCLUDED IN THE COST OF THE CONCRETE BARRIER, SINGLE FACE 42 INCH HEIGHT.
2. JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH ARTICLE 637.08 OF THE STANDARD SPECIFICATIONS.

CONCRETE BARRIER, SINGLE FACE
42 INCH HEIGHT AT EXIT RAMP
DETAILS



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USER NAME = chiuw	DRAWN - AFC	REVISED -
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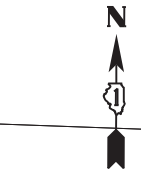
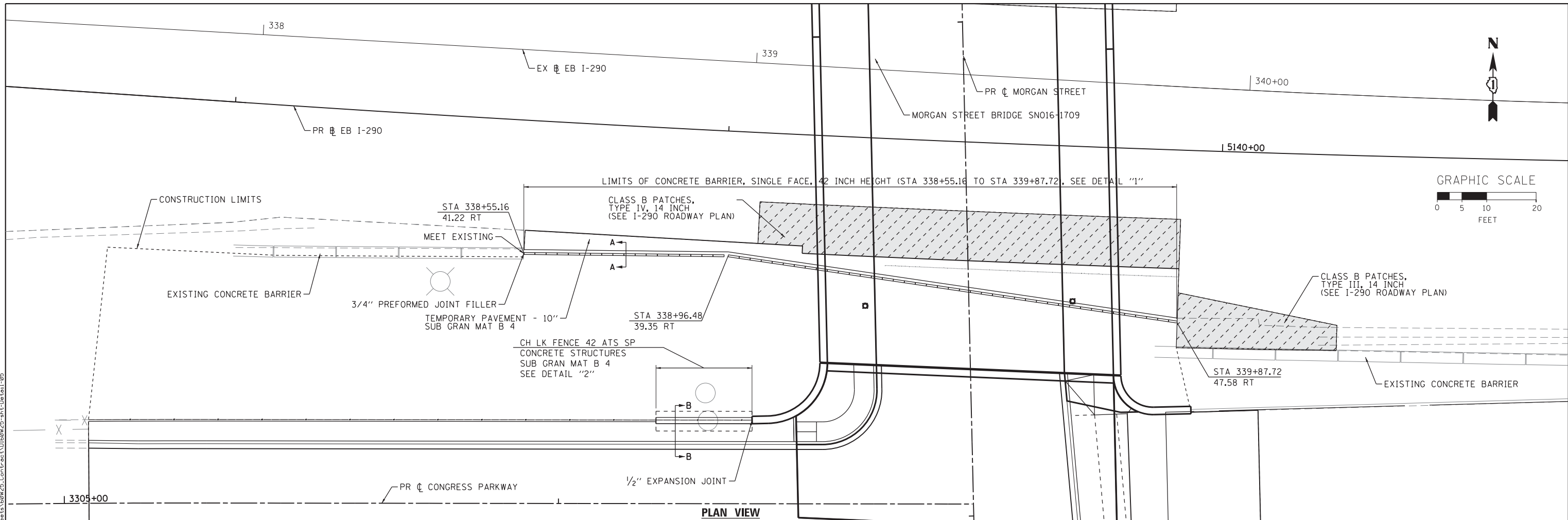
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY DETAILS

SCALE: NTS SHEET 4 OF 9 SHEETS STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2013-007R	COUNTY COOK	TOTAL SHEETS 317	SHEET NO. 269A
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

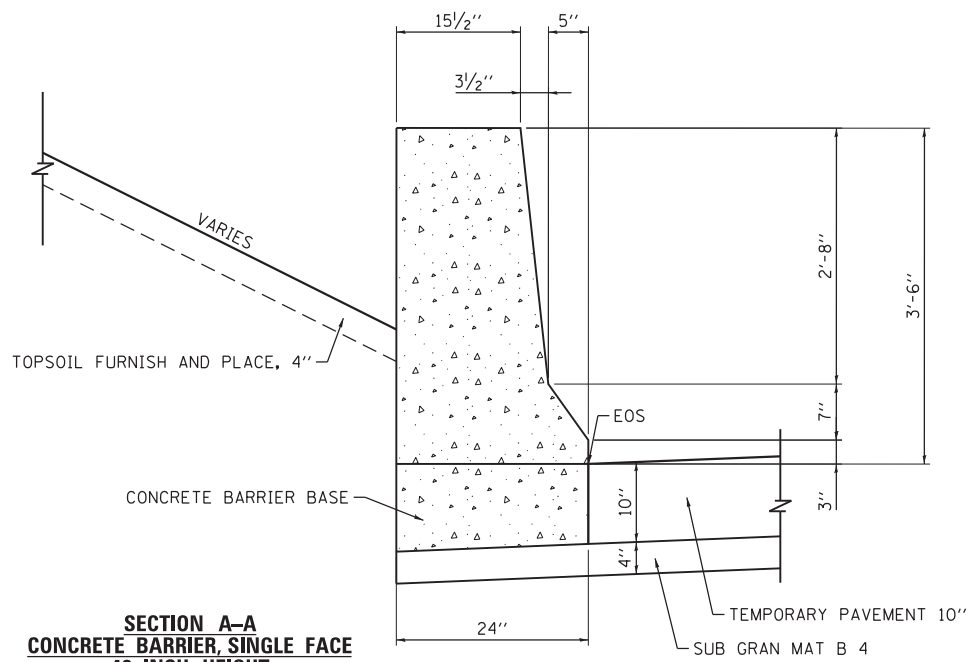
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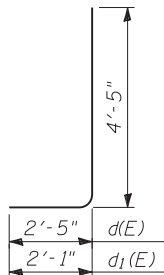
PLAN VIEW

NOTES:

- PREFORMED JOINT FILLER SHALL BE INCLUDED IN THE COST OF THE VARIOUS CONCRETE BARRIER PAY ITEMS UNLESS OTHERWISE NOTED.
- JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH ARTICLE 637.08 OF THE STANDARD SPECIFICATIONS.



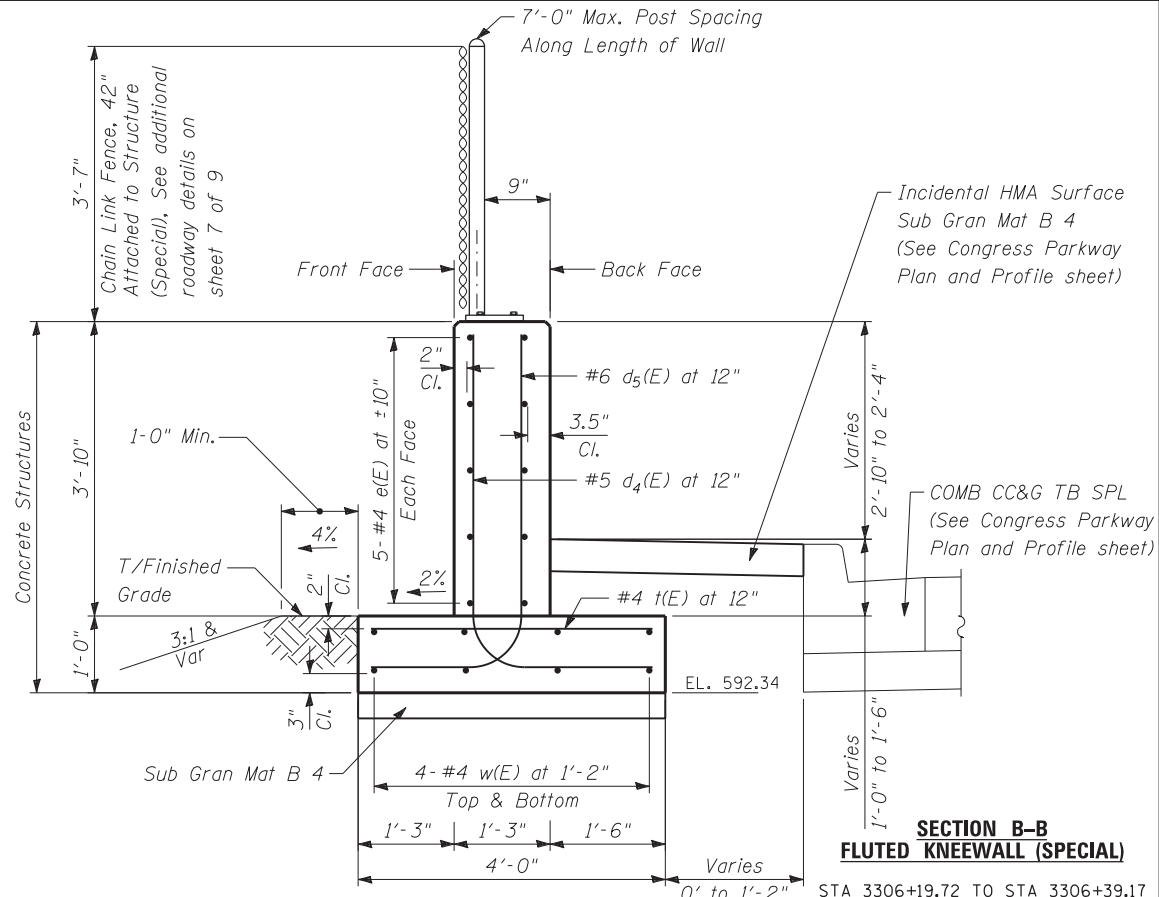
SECTION A-A
CONCRETE BARRIER, SINGLE FACE
42 INCH HEIGHT
STA 338+55.16 TO STA 339+87.72



BARS $d_4(E)$ & $d_5(E)$

BILL OF MATERIAL

ITEM	UNIT	TOTAL
CONC STRUCT	CU YD	6.5
PROTECTIVE COAT	SQ YD	20
REINF BARS, EPOXY CTD	POUND	620
CH LK FENCE 42 ATS SP	FOOT	20
SUB GRAN MAT B 4	SQ YD	78



SECTION B-B
FLUTED KNEEWALL (SPECIAL)
STA 3306+19.72 TO STA 3306+39.17



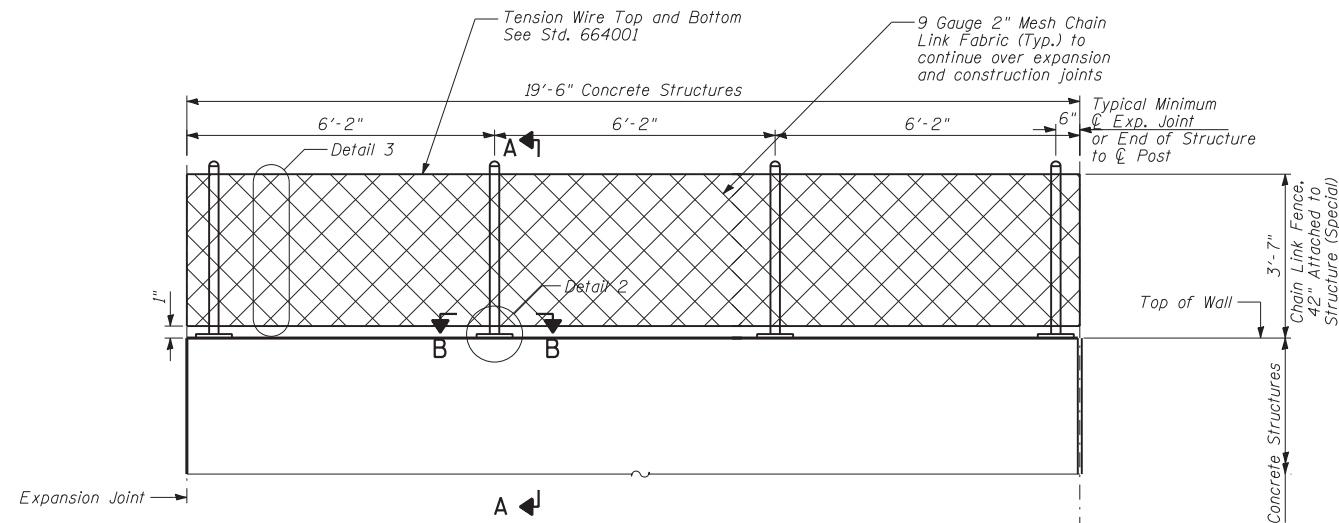
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

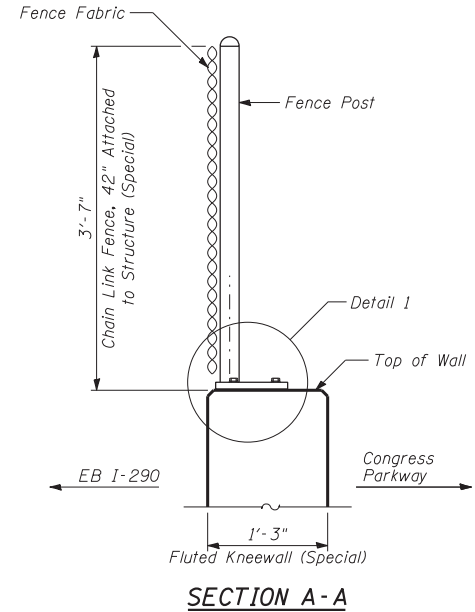
ROADWAY DETAILS
CONCRETE BARRIER / FLUTED KNEEWALL (SPECIAL)

SCALE: 1"=10' SHEET 5 OF 9 SHEETS STA. TO STA.

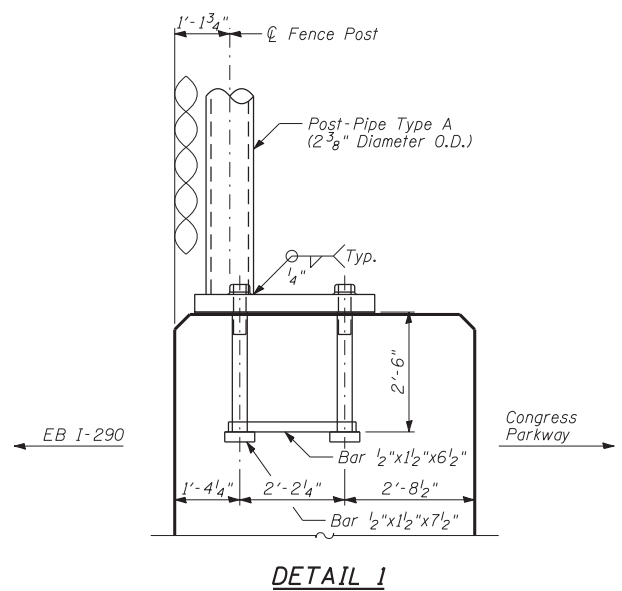
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	269B
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				



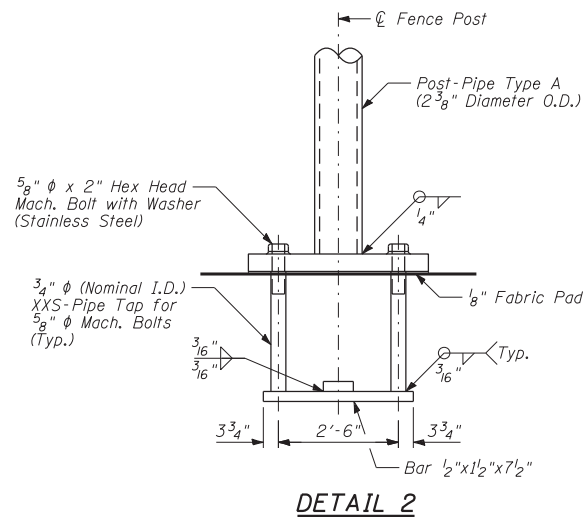
FRONT FACE ELEVATION - TOP OF CONCRETE STRUCTURES CHAIN LINK FENCE



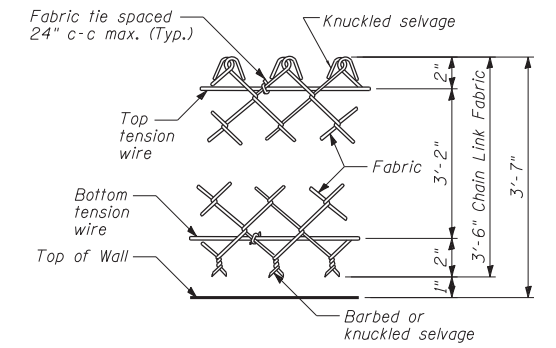
SECTION A-A



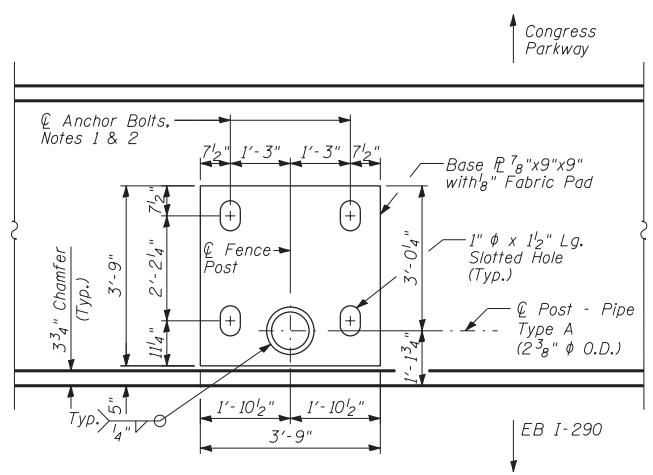
DETAIL 1



DETAIL 2



DETAIL 3



**SECTION B-B
BASE PLATE PLAN**

NOTES:

- In lieu of the cast-in-place anchor bolt assembly shown, the Contractor has the option of drilling and epoxy grouting 3/8" diameter anchor rods with 1/4" diameter washers. The Contractor shall use the capsule or the adhesive cartridge type anchor rods that have been previously tested and given prior approval by the Department. The Contractor shall install these anchor rods in pre-drilled holes according to the manufacturer's recommendations and procedures. The capsule or adhesive cartridge shall be sealed with pre-measured amounts of adhesive chemical. Anchor rod threading to be peened after nuts are installed.
- For additional chain link details, see Standard 664001.

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DI60W25-sht-Detail-05A
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / 1"
 PLOT DATE = 6/14/2013

DESIGNED - KAM
 DRAWN - KAM
 CHECKED - AFC
 DATE - 6/17/2013

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY DETAILS
 CHAIN LINK FENCE ATTACHED TO STRUCTURE**

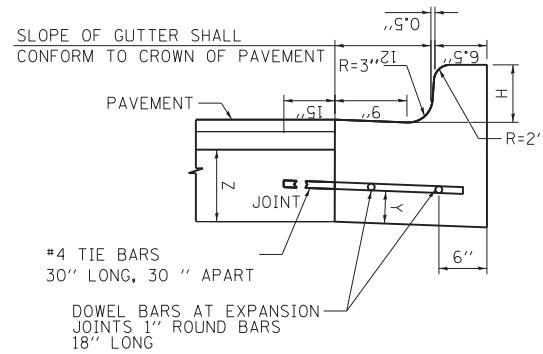
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	269C
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

SCALE: NTS SHEET 6 OF 9 SHEETS STA. TO STA.

DETAILS OF CONCRETE CURB, TYPE B AND COMBINATION CONCRETE CURB AND GUTTER, TYPE B (SPECIAL)

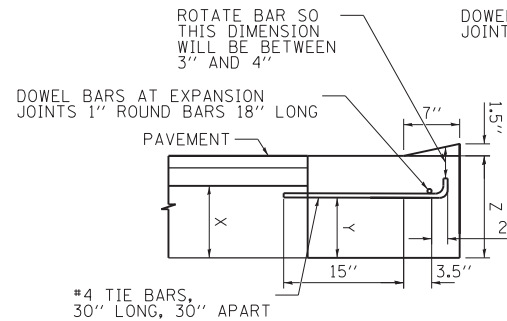
NOTE:

H = VARIABLE, MINIMUM 3" AND NOT TO EXCEED 9" (SEE PLANS)
 X = THICKNESS OF PAVEMENT
 Y = ONE HALF THE THICKNESS OF CONCRETE PAVEMENT OR CONCRETE BASE
 Z = 10" OR THICKNESS OF PAVEMENT - WHICHEVER IS GREATER



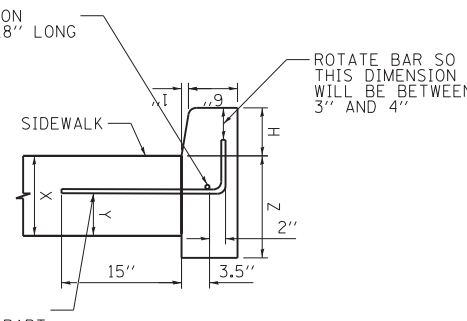
COMBINATION CONCRETE CURB AND GUTTER, TYPE B (SPECIAL)

(USED AT MORGAN ST, CONGRESS PKWY, AND TILDEN ST)



DEPRESSED CURB

(USED AT DRIVEWAY AND ADA RAMPS)



BARRIER CURB

(USED IN FRONT OF FENCE OF UIC LOT #9
AND NEXT TO ADA RAMPS WHEN NEEDED)

CONCRETE CURB, TYPE B

DEPRESSED CURB & GUTTER

DEPRESSED CURB AND GUTTER AND TRANSITIONS BETWEEN BARRIER CURB WILL BE PAID FOR UNDER THE ADJACENT CURB ITEM. DEPRESSED CURB AND MOUNTABLE GUTTER MUST MEET CDOT ADA STANDARDS.

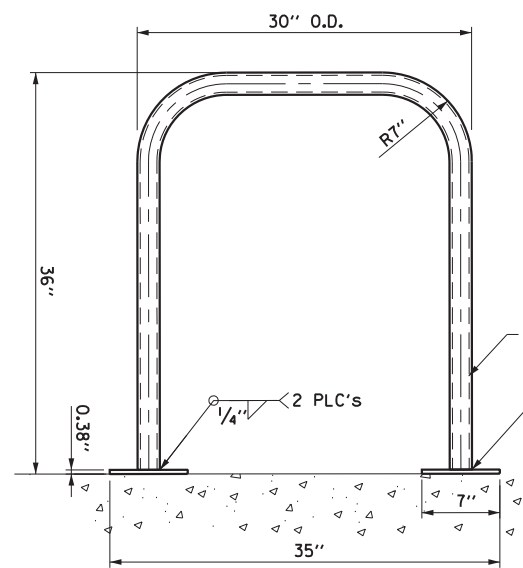
JOINTS IN CURB AND COMBINATION CURB & GUTTER

FOR FULL PAVEMENT RECONSTRUCTION, TRANSVERSE JOINTS OF A TYPE SIMILAR TO THAT USED IN THE ADJACENT PAVEMENT SHALL BE INSTALLED IN CURB AND COMBINATION CURB AND GUTTER IN PROLONGATION WITH THE JOINTS IN THE PAVEMENT. THE DETAILS OF THE TRANSVERSE JOINTS IN THE CURB AND COMBINATION CURB AND GUTTER SHALL BE APPROVED BY THE ENGINEER. WHERE CURB OR COMBINATION CURB AND GUTTER IS CONSTRUCTED ADJACENT TO A FLEXIBLE BASE PAVEMENT OR CLASS C PATCH, 5/8" THICK EXPANSION JOINTS COMPOSED OF PREFORMED JOINT FILLER AND SEALANT SHALL BE INSTALLED IN THE CURB AND/OR COMBINATION CURB AND GUTTER AT POINTS OF CURVATURE AND AT CONSTRUCTION JOINTS AT A DISTANCE NOT EXCEEDING 50'. CONTRACTION JOINTS SHALL ALSO BE PLACED BETWEEN THESE EXPANSION JOINTS AT A DISTANCE NOT EXCEEDING 10'. CONTRACTION AND EXPANSION JOINTS SHALL LINE UP WITH THE PROPOSED SIDEWALK SAWCUT JOINTS (EXCEPT FOR AT POINTS OF CURVATURE). ALL TIE BARS SHALL BE CORRUGATED; ALL DOWEL BARS SHALL BE SMOOTH.

BASIS OF PAYMENT

THE COST OF ALL JOINTS, INCLUDING LABOR, FURNISH AND PLACING OF STEEL, JOINT FILLER, SEALANT, AND ALL OTHER INCIDENTALS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR CONCRETE CURB, TYPE B, AND COMBINATION CONCRETE CURB AND GUTTER ITEMS. SAWCUTTING AND FURNISHING AND INSTALLING CURB ANCHORS, DOWELS, AND TIE BARS SHALL ALSO BE INCIDENTAL TO THESE ITEMS.

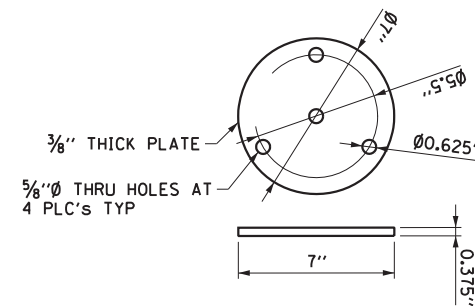
BICYCLE RACKS



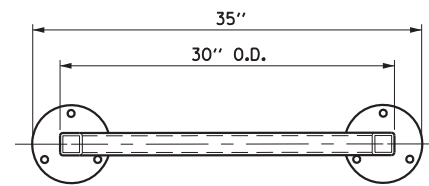
ELEVATION

NOTE:

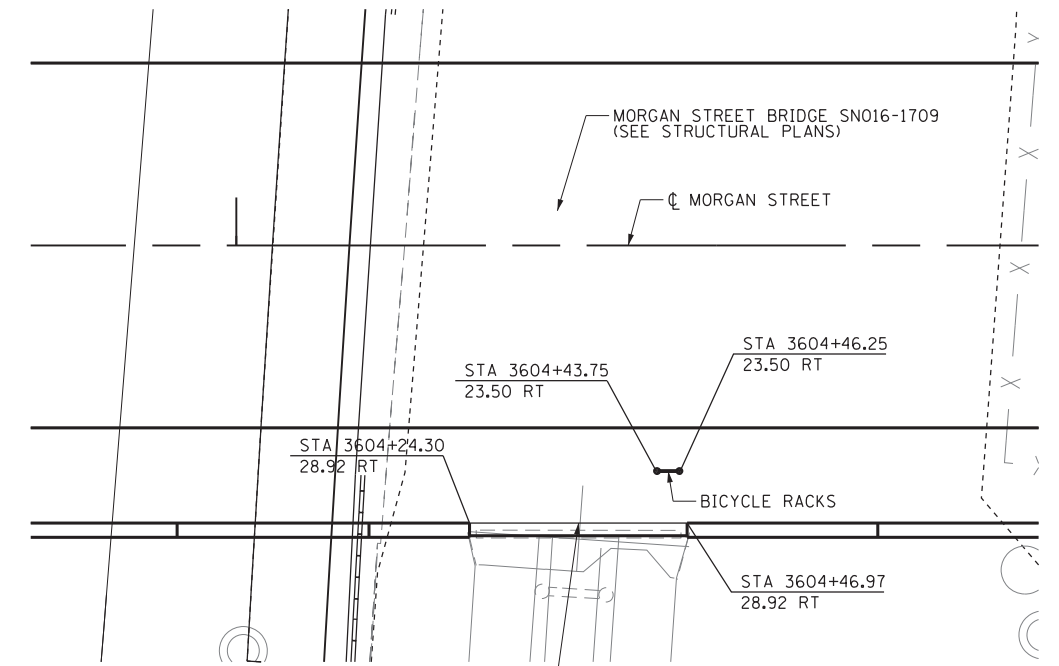
1. SQUARE FLANGE MOUNTING PLATE MAY BE SUBSTITUTES FOR ROUND FLANGED.



FLANGE DETAIL
NOT TO SCALE



DETAIL PLAN



PLAN

FILE PATH = p:\388039\pmt\escomon\me\local\p\AECOM\Documents\01_Americas\Transportation\60269938_Circle\Phase_1\1600_CAD\006_Roadway\Sheets\60W25-sht-Detail-06



D160W25-sht-Detail-06
 USER NAME = pimsarno
 PLOT SCALE = 10.0000 / 1 in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC
 DRAWN - AFC
 CHECKED - DBM
 DATE - 6/17/2013

REVISED -
 REVISED -
 REVISED -
 REVISED -

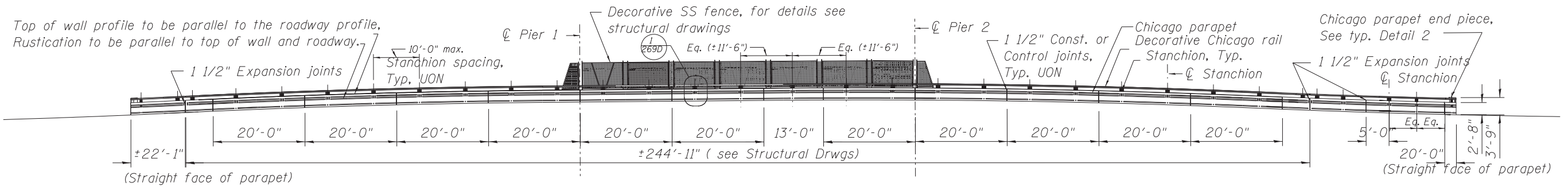
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY DETAILS
CURB DETAIL / BICYCLE RACKS**

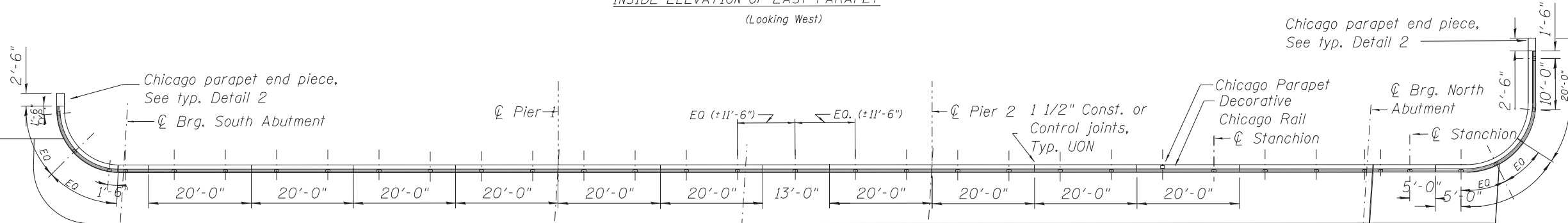
SCALE: NTS SHEET 7 OF 9 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	269D
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

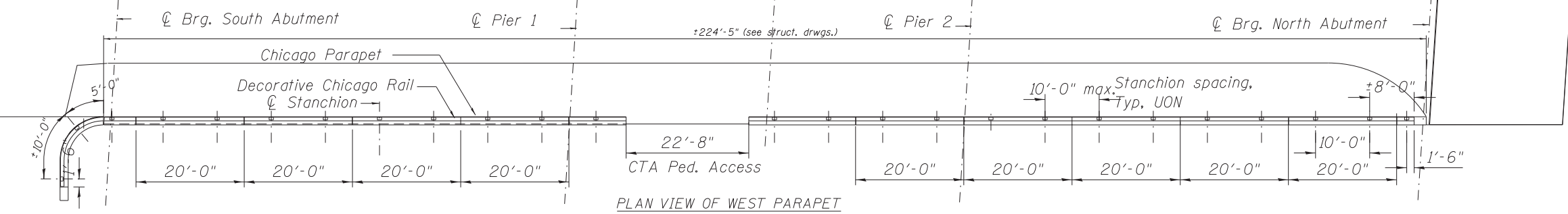
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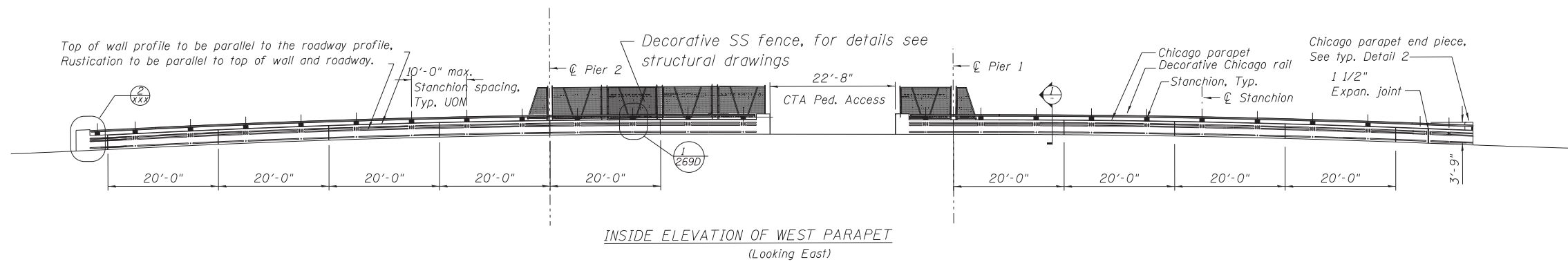
INSIDE ELEVATION OF EAST PARAPET
(Looking West)



PLAN VIEW OF EAST PARAPET



PLAN VIEW OF WEST PARAPET



INSIDE ELEVATION OF WEST PARAPET
(Looking East)



D160W25-sht-Parapet-Detail.dgn
 USER NAME = r1stovskem
 PLOT SCALE = 1:8000 / 1" = 80'
 PLOT DATE = 6/14/2013

DESIGNED - MR	REVISED -
DRAWN - MR	REVISED -
CHECKED -	REVISED -
DATE - 5/21/2013	REVISED -

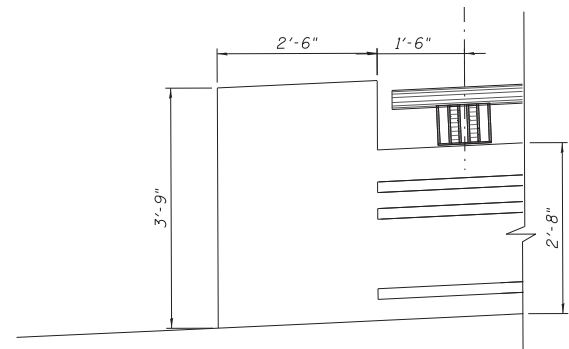
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ROADWAY DETAILS
 BRIDGE PARAPET

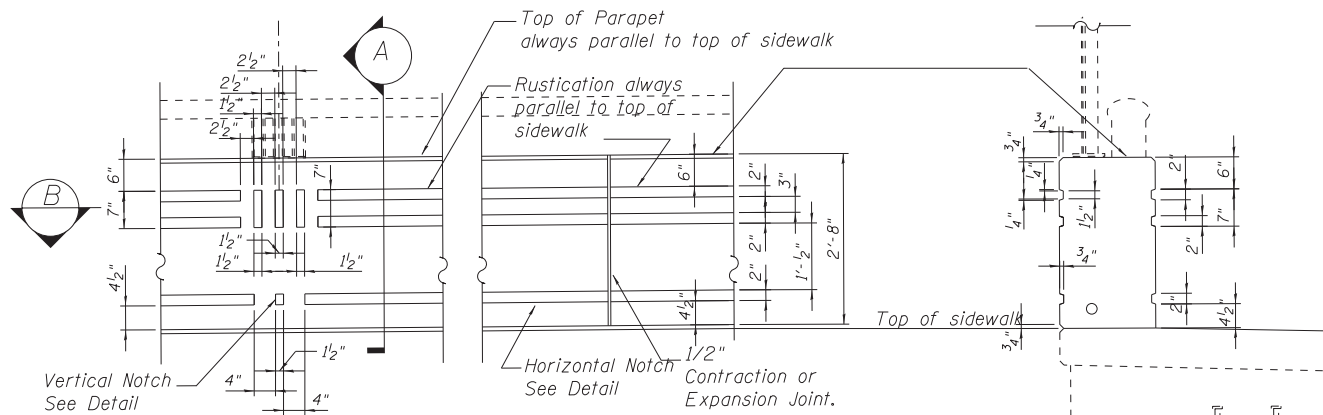
SCALE: SHEET 8 OF 9 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	269E
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

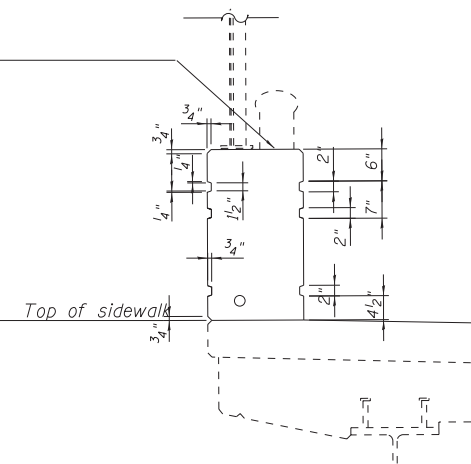
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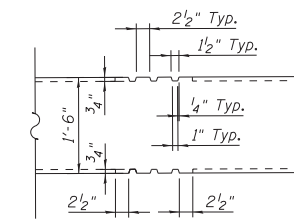
DETAIL 2



DETAIL 1



SECTION A



SECTION B



DI60W25-sht_Parapet_Detail_02.dgn
 USER NAME = ristolovskem
 PLOT SCALE = 1.0000' / 1" =
 PLOT DATE = 6/14/2013

DESIGNED - MR
 DRAWN - MR
 CHECKED -
 DATE - 5/21/2013

REVISED -
 REVISED -
 REVISED -
 REVISED -

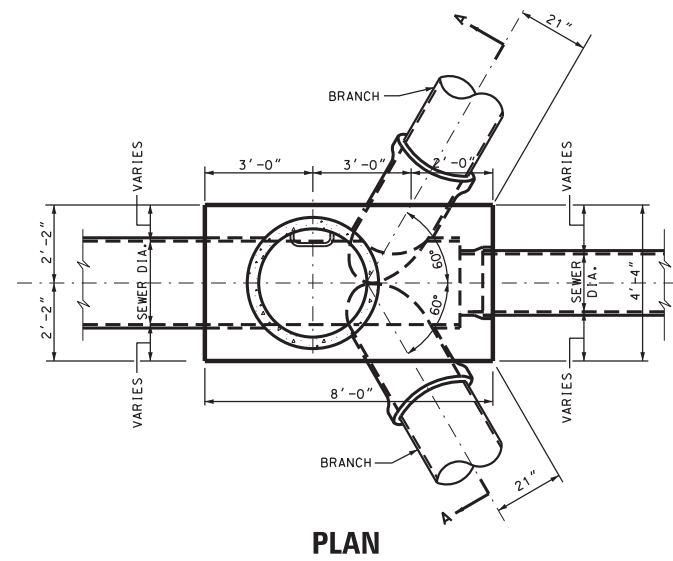
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ROADWAY DETAILS
 BRIDGE PARAPET

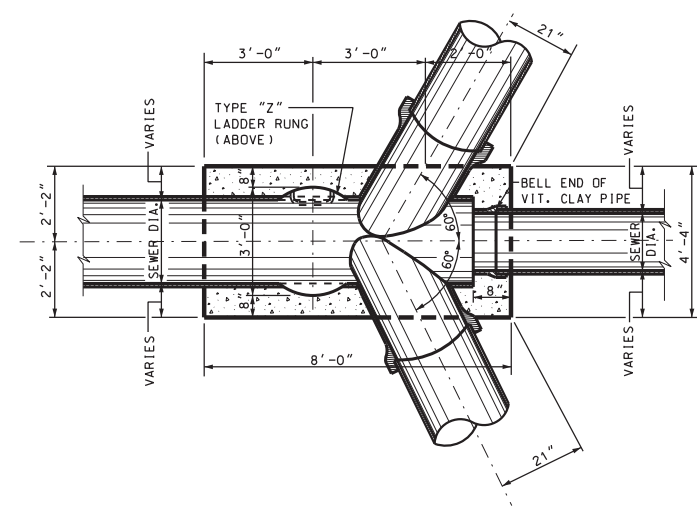
SCALE: SHEET 9 OF 9 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	269F
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

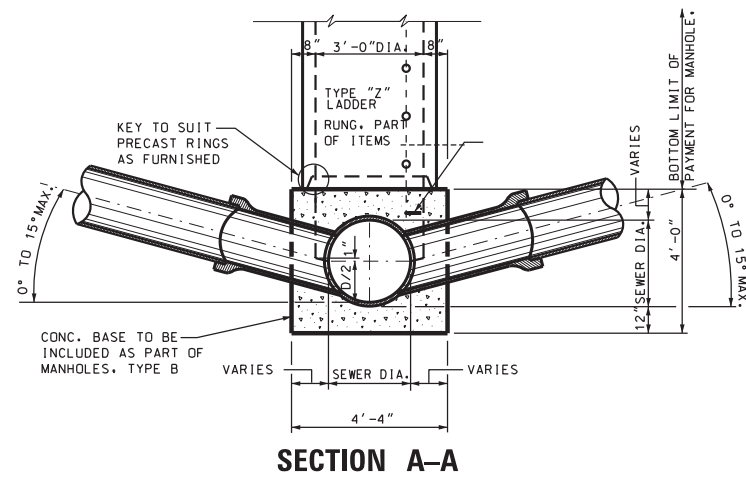
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PLAN

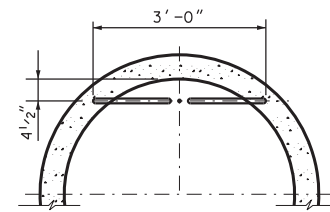


SECTIONAL PLAN

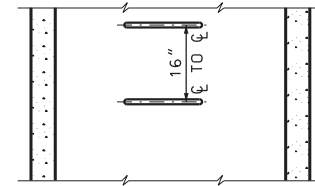


SECTION A-A

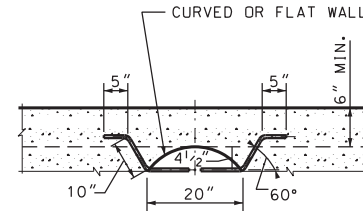
TYPE "B" MANHOLE
FOR SEWERS 24" DIA. & SMALLER



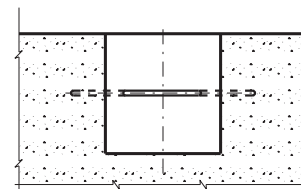
PLAN



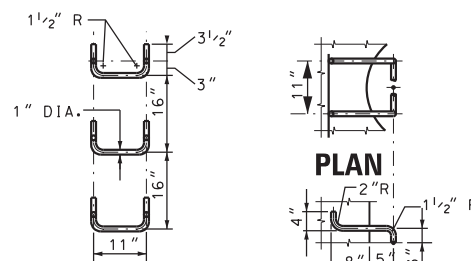
ELEVATION
TYPE X



PLAN



ELEVATION
TYPE Y

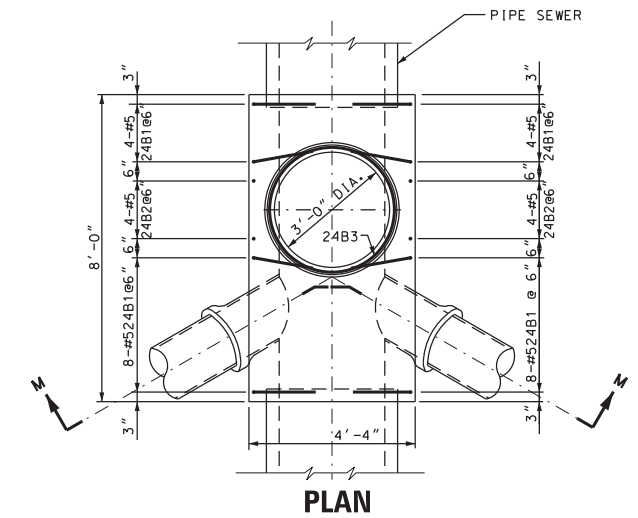


SPACING

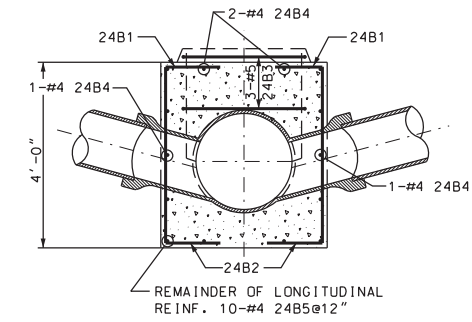
ELEVATION

HANDHOLD-TYPE Z RUNG

STANDARD LADDER RUNGS



PLAN



SECTION M-M

REINFORCEMENT FOR TYPE "B" MANHOLE BASES
FOR SEWERS 24" DIA & SMALLER



D160W25-sht-Drain-Detail-03
USER NAME = pimserno
PLOT SCALE = 1:8000 / 1/8" = 1'-0"
PLOT DATE = 6/14/2013

DESIGNED -
DRAWN - AFC
CHECKED - EY
DATE - 6/17/2013

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

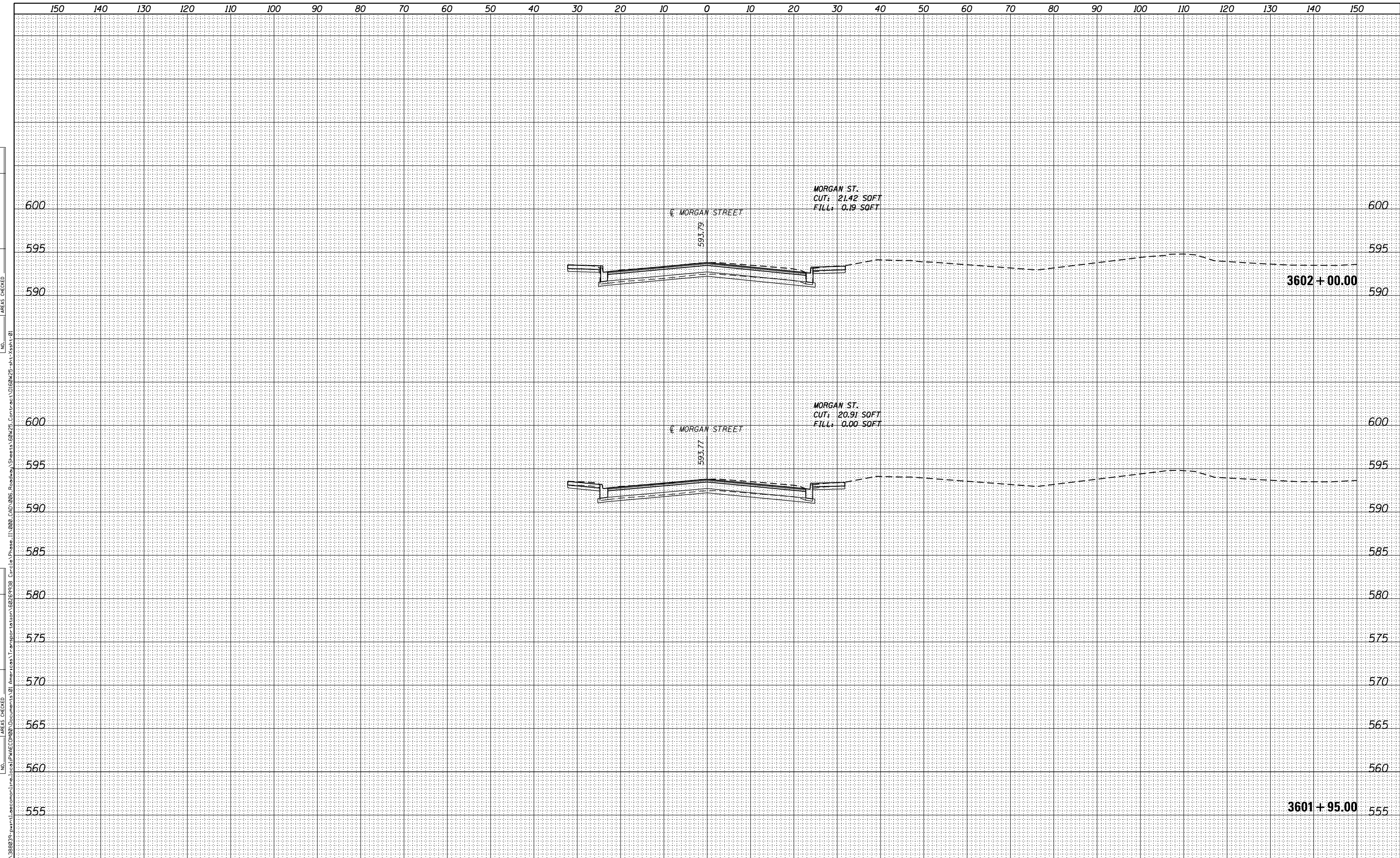
DRAINAGE DETAILS
CITY OF CHICAGO, TYPE "B" MANHOLES

SCALE: NTS SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2013-007R	COUNTY COOK	TOTAL SHEETS 317	SHEET NO. 272
CONTRACT NO. 60W25			ILLINOIS FED. AID PROJECT	

BY	DATE

ORIGINAL SURVEY	SURVEYED	DATE
NOTE BOOK	PLOTTED	
	TEMPLATE	
	AREAS CHECKED	



AECOM
 303 EAST WACKER DRIVE, SUITE 1400
 CHICAGO, IL 60601-5275
 PHONE: (312) 373-7700 FAX: (312) 373-6800

DI60W25-sht-Xsht-01	DESIGNED - AFC	REVISED -
USER NAME = moronsk	DRAWN - AFC	REVISED -
PLOT SCALE = 10.0000' / 1"	CHECKED - DBM	REVISED -
PLOT DATE = 5/15/2013	DATE - 5/14/2013	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

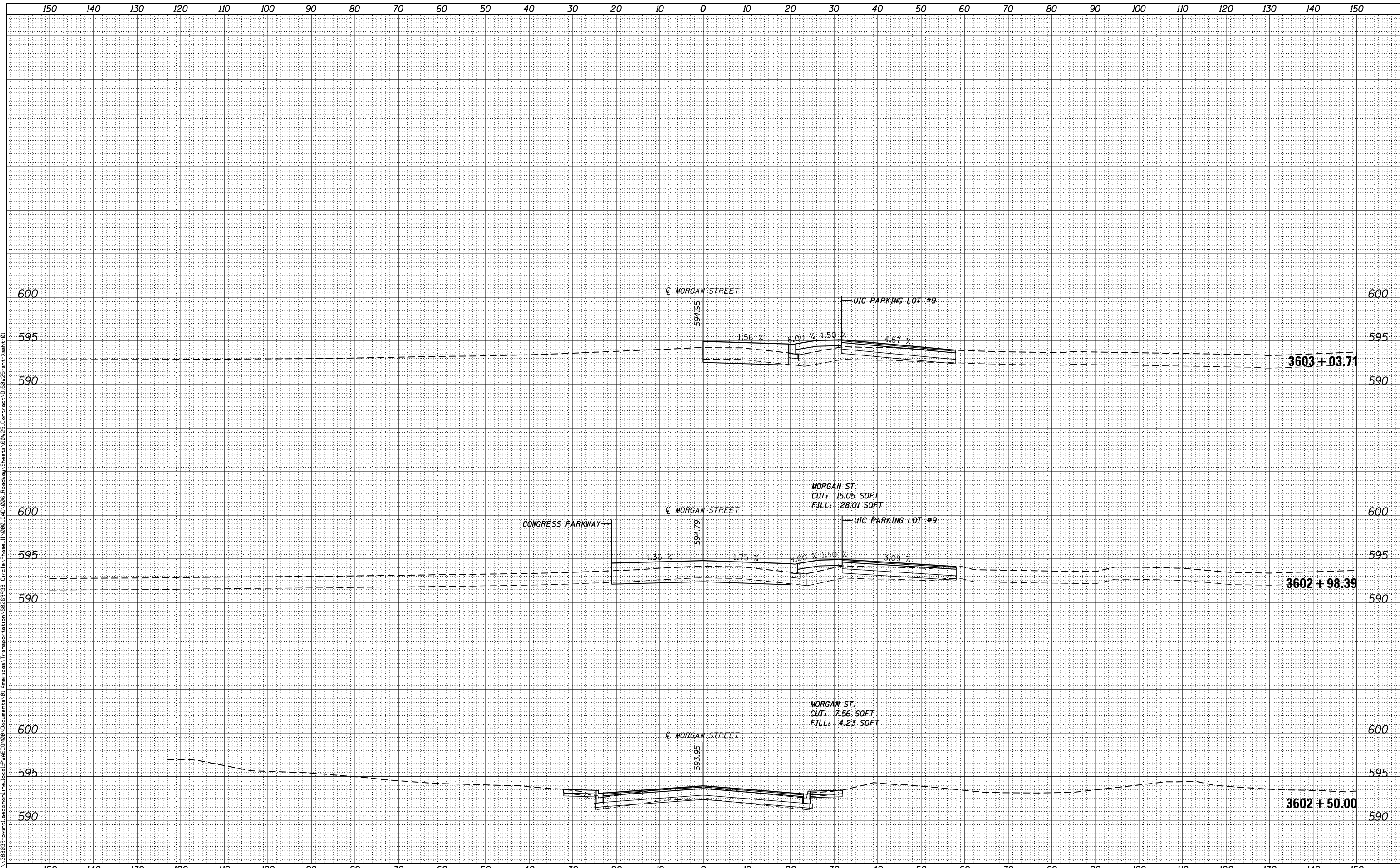
MORGAN STREET CROSS SECTION		
SCALE: 1:5V, 1:10H	SHEET 1 OF 45 SHEETS	STA. 3601+95.00 TO STA. 3602+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	273
CONTRACT NO. 60W25				ILLINOIS FED. AID PROJECT

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED

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D160W25-shr-Xsht-01
 USER NAME = moronik
 PLOT SCALE = 10.0000' / 1" = 100.0000'
 PLOT DATE = 5/15/2013

DESIGNED - AFC	REvised -
DRAWN - AFC	REvised -
CHECKED - DBM	REvised -
DATE - 5/14/2013	REvised -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

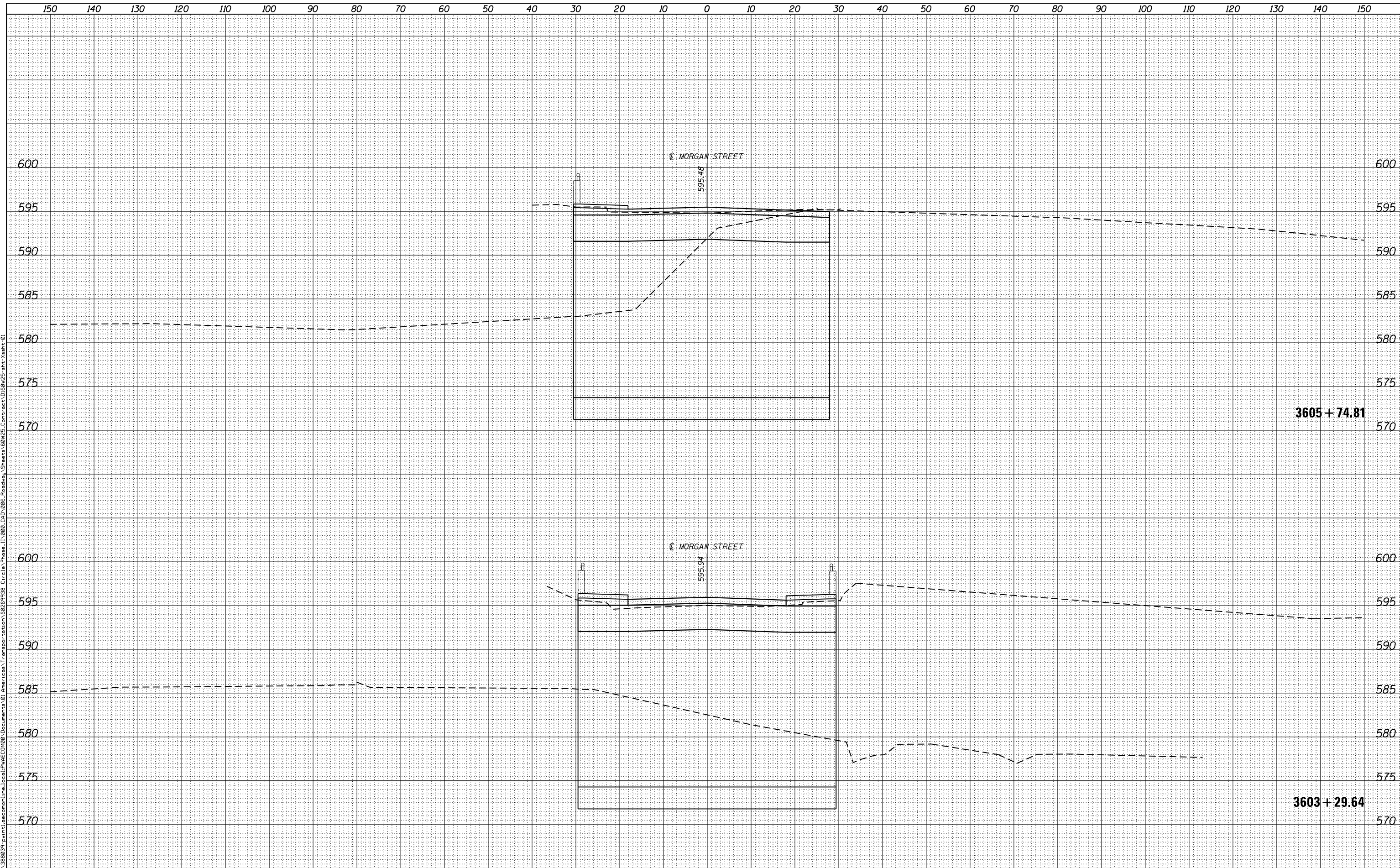
MORGAN STREET CROSS SECTION

SCALE: 1:5V, 1:10H SHEET 2 OF 45 SHEETS STA. 3602+50.00 TO STA. 3603+03.71

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	274
				CONTRACT NO. 60W25
ILLINOIS FED. AID PROJECT				

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	



D160W25-sht-Xssht-01	DESIGNED - AFC	REVISED -
USER NAME = moransk	DRAWN - AFC	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - DBM	REVISED -
PLOT DATE = 5/15/2013	DATE - 5/14/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

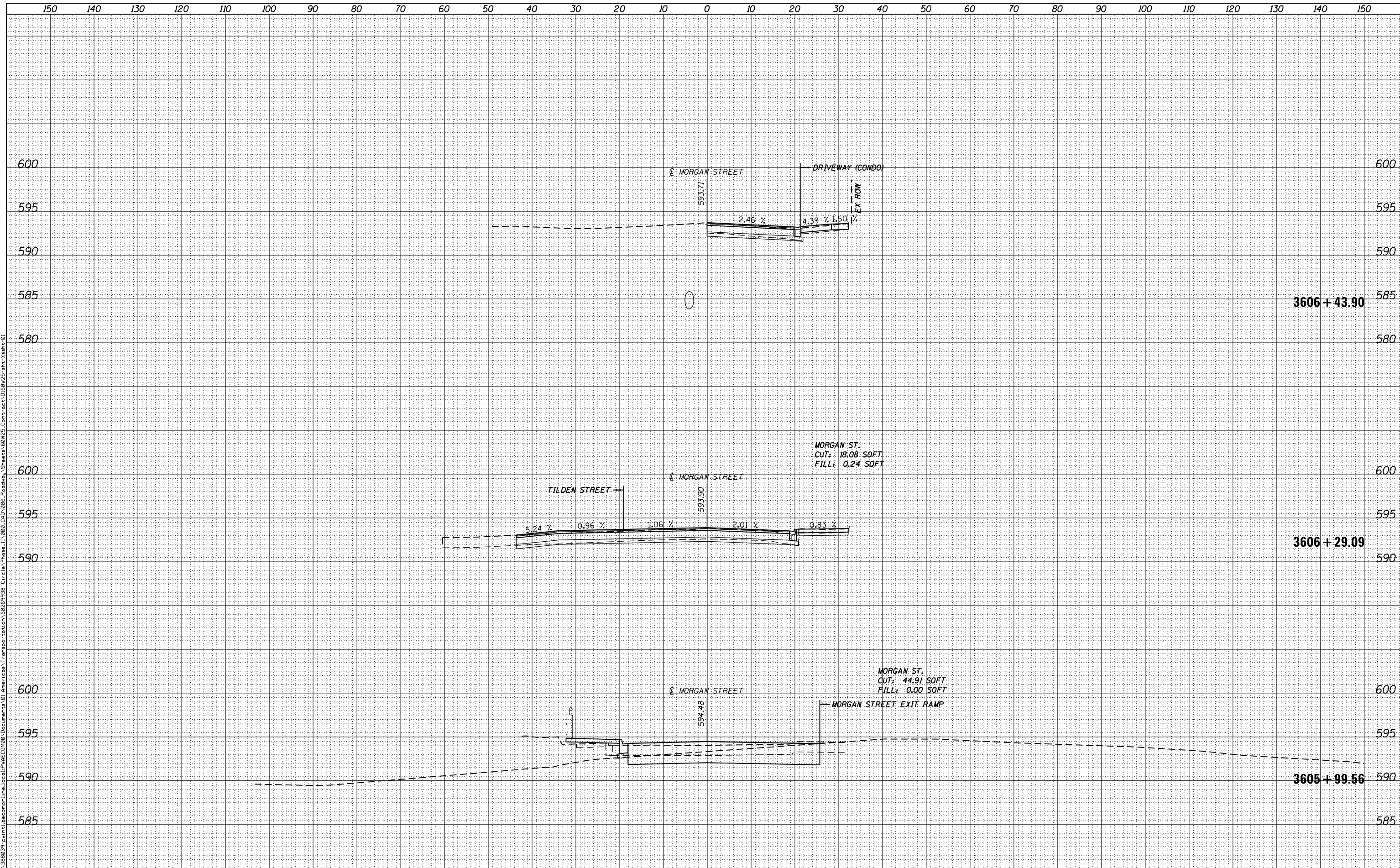
MORGAN STREET CROSS SECTION

SCALE: 1:5V,1:10H SHEET 3 OF 45 SHEETS STA. 3603+29.64 TO STA. 3605+74.81

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	275
				CONTRACT NO. 60W25
ILLINOIS FED. AID PROJECT				

DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	
FINAL SURVEY	
NOTE BOOK	
AREAS	
CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	
FINAL SURVEY	
NOTE BOOK	
AREAS	
CHECKED	
NO.	



DI60W25-sht-Xssht-01	DESIGNED - AFC	REVISED -
USER NAME = moronik	DRAWN - AFC	REVISED -
PLOT SCALE = 10.0000' / 1"	CHECKED - DBM	REVISED -
PLOT DATE = 5/15/2013	DATE - 5/14/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

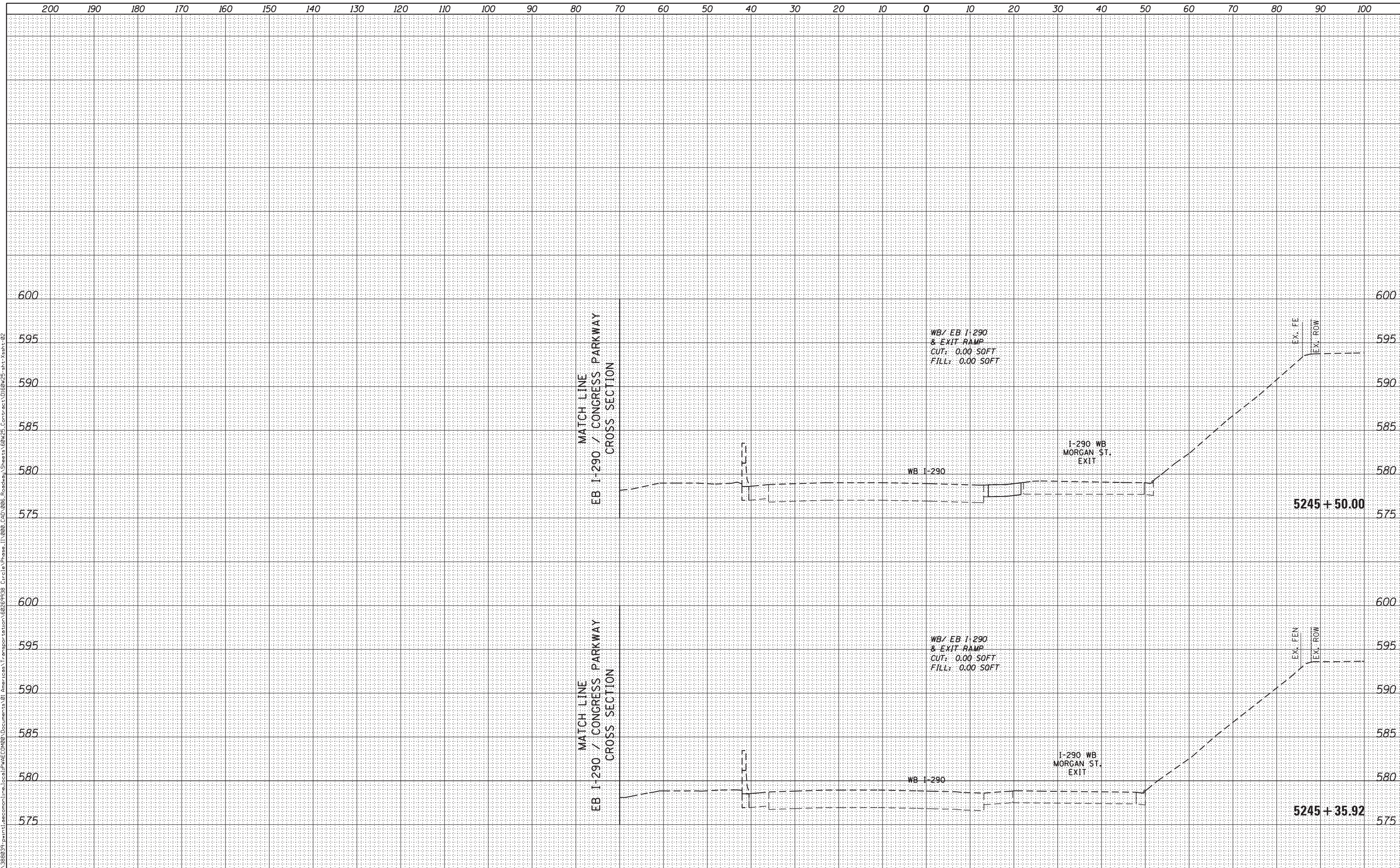
MORGAN STREET CROSS SECTION

SCALE: 1:5V,1:10H SHEET 4 OF 45 SHEETS STA. 3605+99.56 TO STA. 3606+43.90

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	276
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



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D160W25-sht-Xsht-02
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

WB I-290 / EXIT RAMP TO MORGAN STREET CROSS SECTION

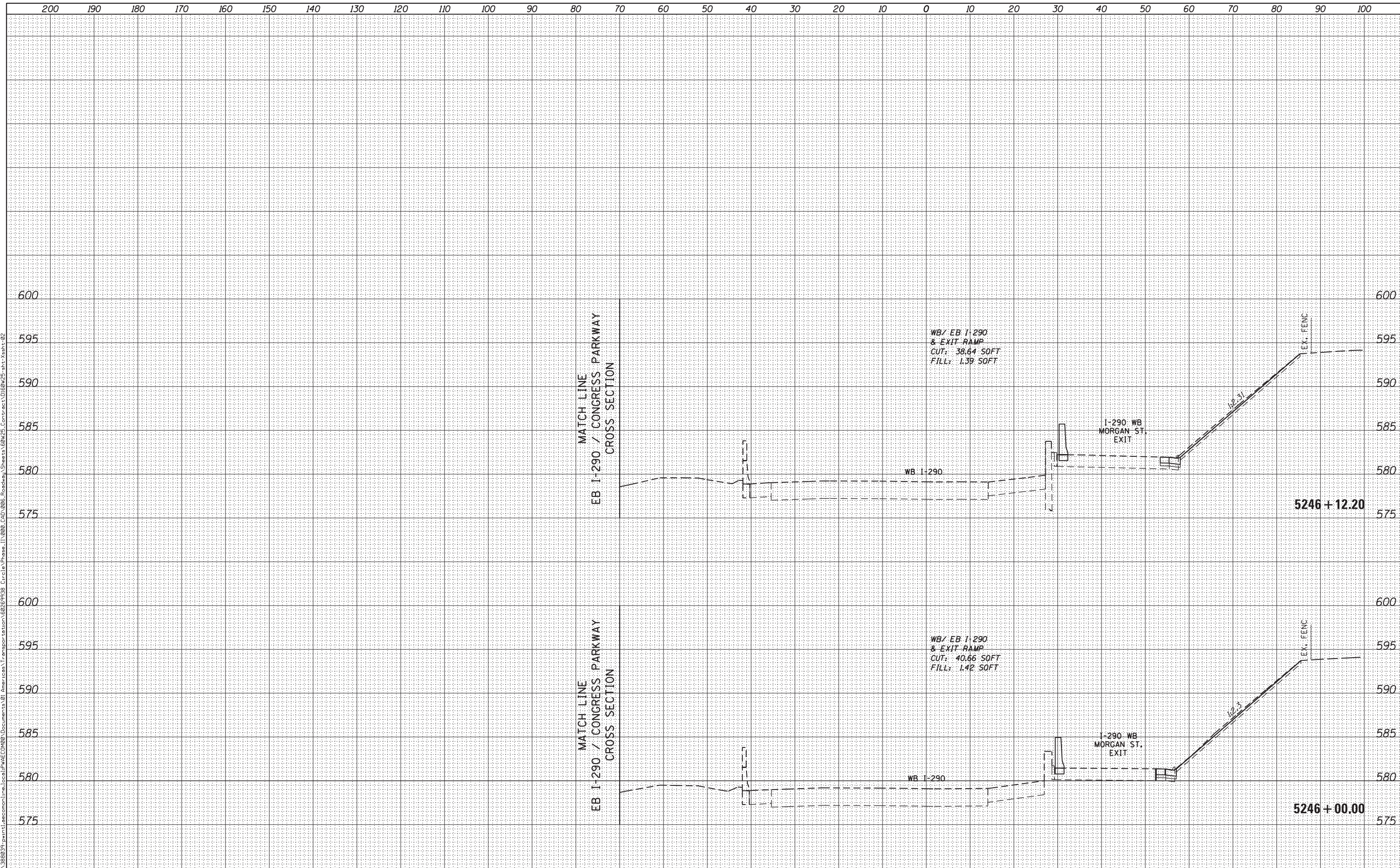
SCALE: 1:5V, 1:10H SHEET 6 OF 33 SHEETS STA. 5245+35.92 TO STA. 5245+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	278
CONTRACT NO. 60W25				

ILLINOIS FED. AID PROJECT

DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



FILE PATH = p:\388039\print\atcomon\line\local\p\AEC\000\Documents\01\Americas\Transportation\60269938_Circle\Phase_1\000_CAD\006_Roadway\Sheets\60W25_Contract\0160W25-sht-Xsht-02



D160W25-sht-Xsht-02
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

REVISED -
 REVISED -
 REVISED -
 REVISED -

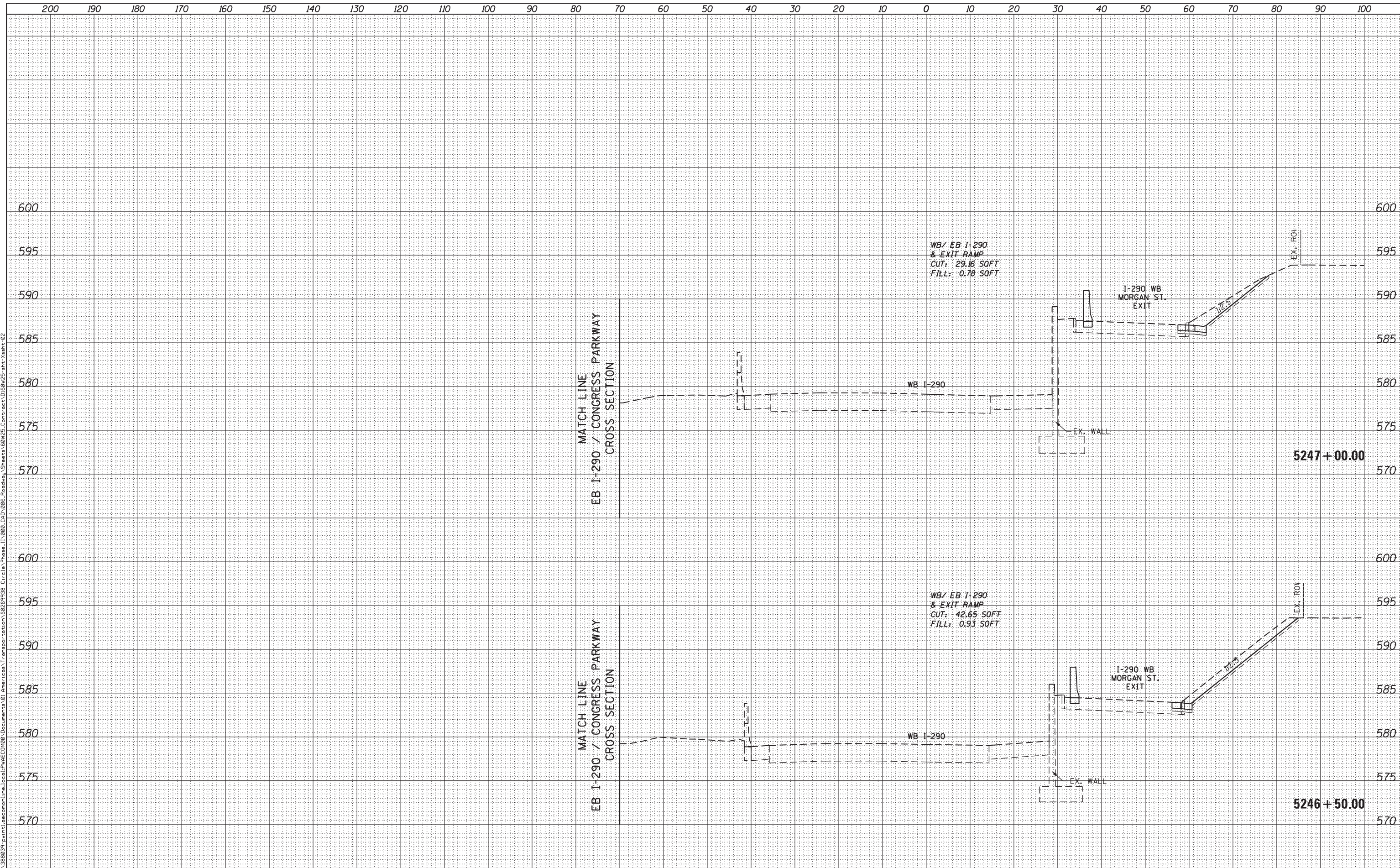
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

WB I-290 /EXIT RAMP TO MORGAN STREET CROSS SECTION
 SCALE: 1:5V,1:10H SHEET 7 OF 33 SHEETS STA. 5246+00.00 TO STA. 5246+12.20

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	279
CONTRACT NO. 60W25			ILLINOIS FED. AID PROJECT	

BY	DATE
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
	TEMPLATE
	AREAS CHECKED

ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
	TEMPLATE
	AREAS CHECKED



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D160W25-sht-Xsht-02	DESIGNED - AFC / KAM	REVISED -
USER NAME = pimsarno	DRAWN - AFC / KAM	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - DBM	REVISED -
PLOT DATE = 6/14/2013	DATE - 6/17/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

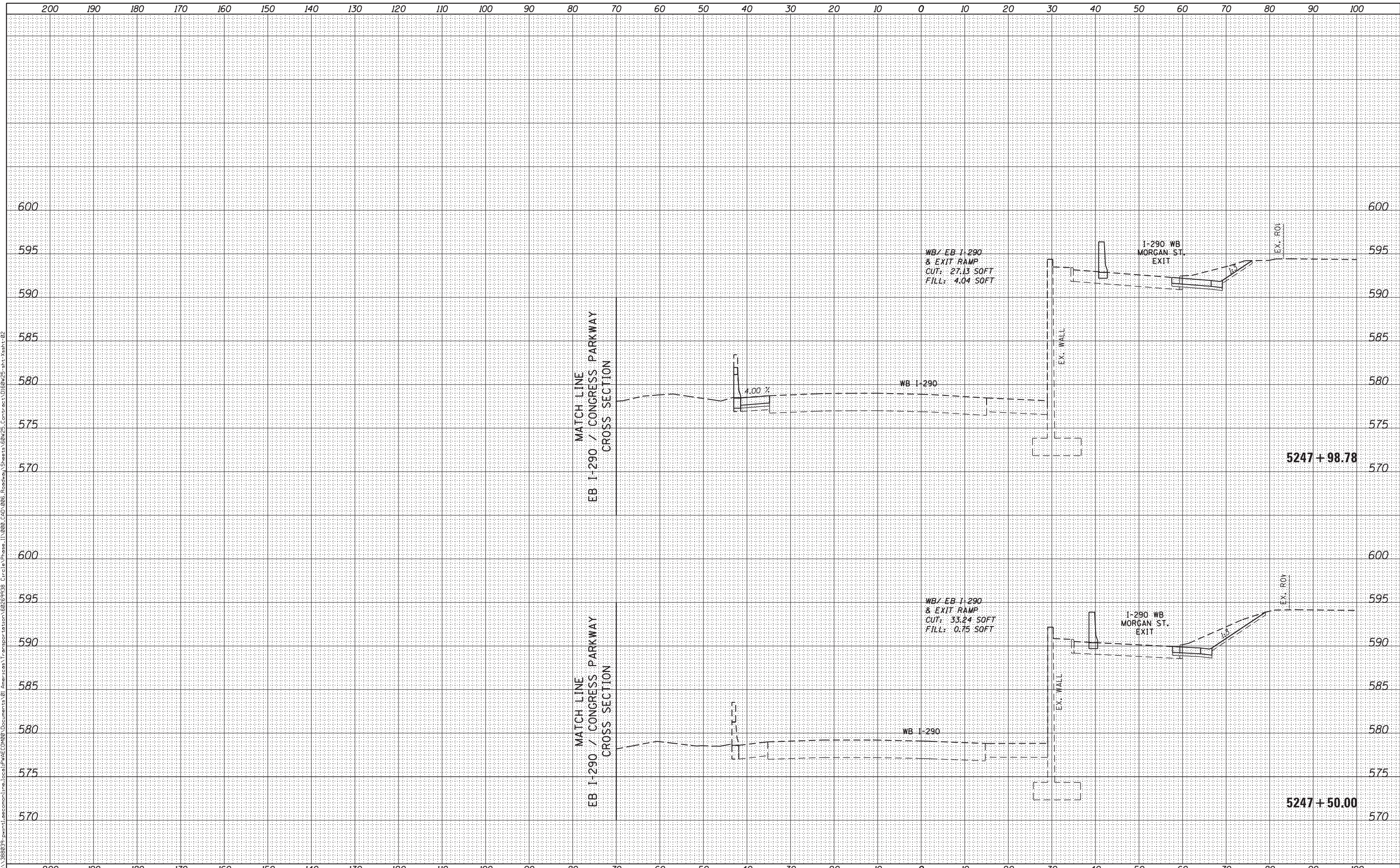
WB I-290 /EXIT RAMP TO MORGAN STREET CROSS SECTION

SCALE: 1:5V,1:10H SHEET 8 OF 33 SHEETS STA. 5246+50.00 TO STA. 5247+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	280
				CONTRACT NO. 60W25
ILLINOIS FED. AID PROJECT				

BY	DATE
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
	TEMPLATE
	AREAS CHECKED
NO.	AREAS CHECKED

BY	DATE
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
	TEMPLATE
	AREAS CHECKED
NO.	AREAS CHECKED



DI60W25-sht-Xsht-02	DESIGNED - AFC / KAM	REVISED -
USER NAME = pimsarno	DRAWN - AFC / KAM	REVISED -
PLOT SCALE = 10.0000 ' / in.	CHECKED - DBM	REVISED -
PLOT DATE = 6/14/2013	DATE - 6/17/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WB I-290 / EXIT RAMP TO MORGAN STREET CROSS SECTION

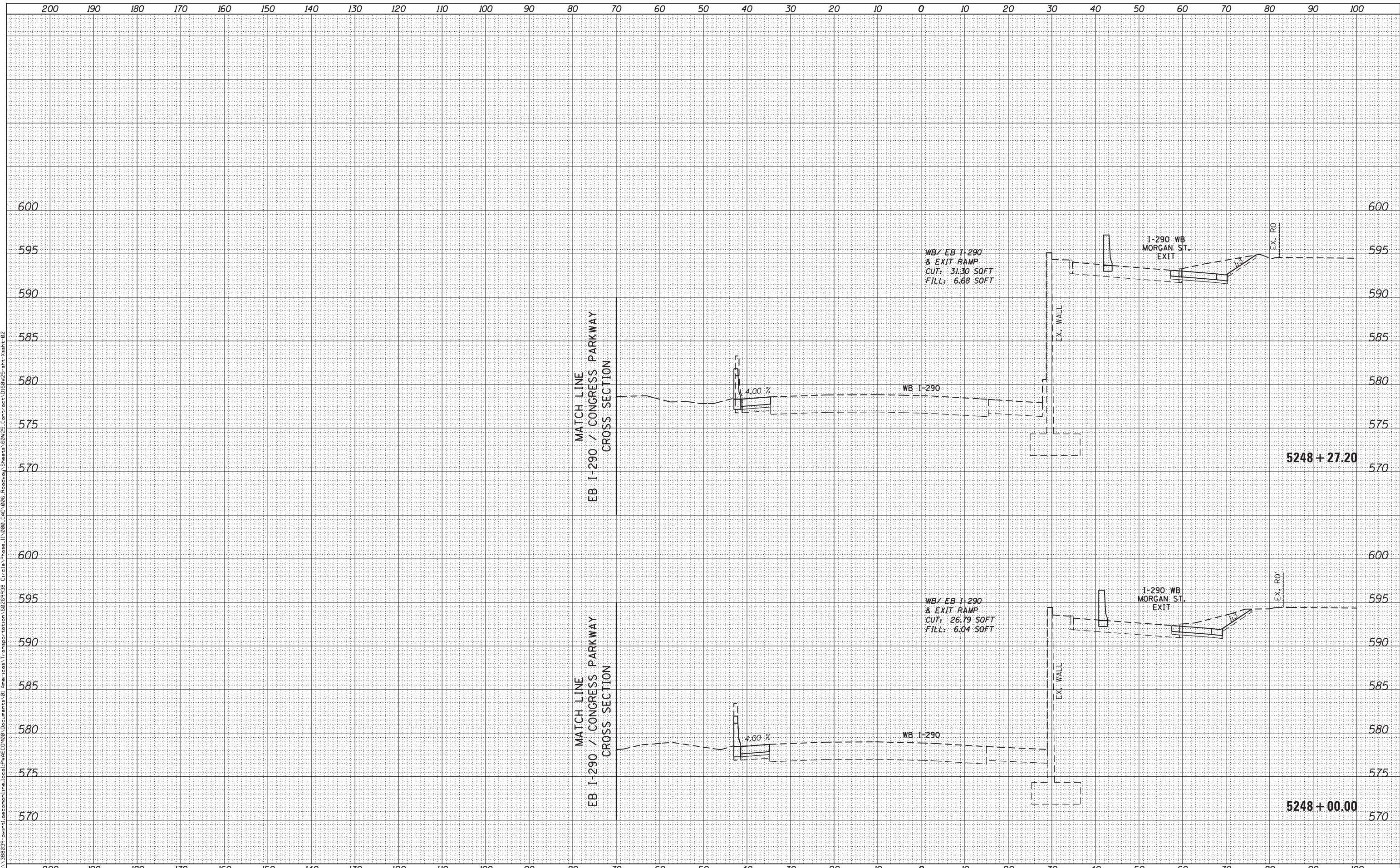
SCALE: 1:5V, 1:10H SHEET 9 OF 33 SHEETS STA. 5247+50.00 TO STA. 5247+98.78

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	281
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
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DATE	
BY	
ORIGINAL SURVEY	
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NOTE BOOK	
AREAS CHECKED	
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D160W25-sht-Xsht-02
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

WB I-290 / EXIT RAMP TO MORGAN STREET CROSS SECTION

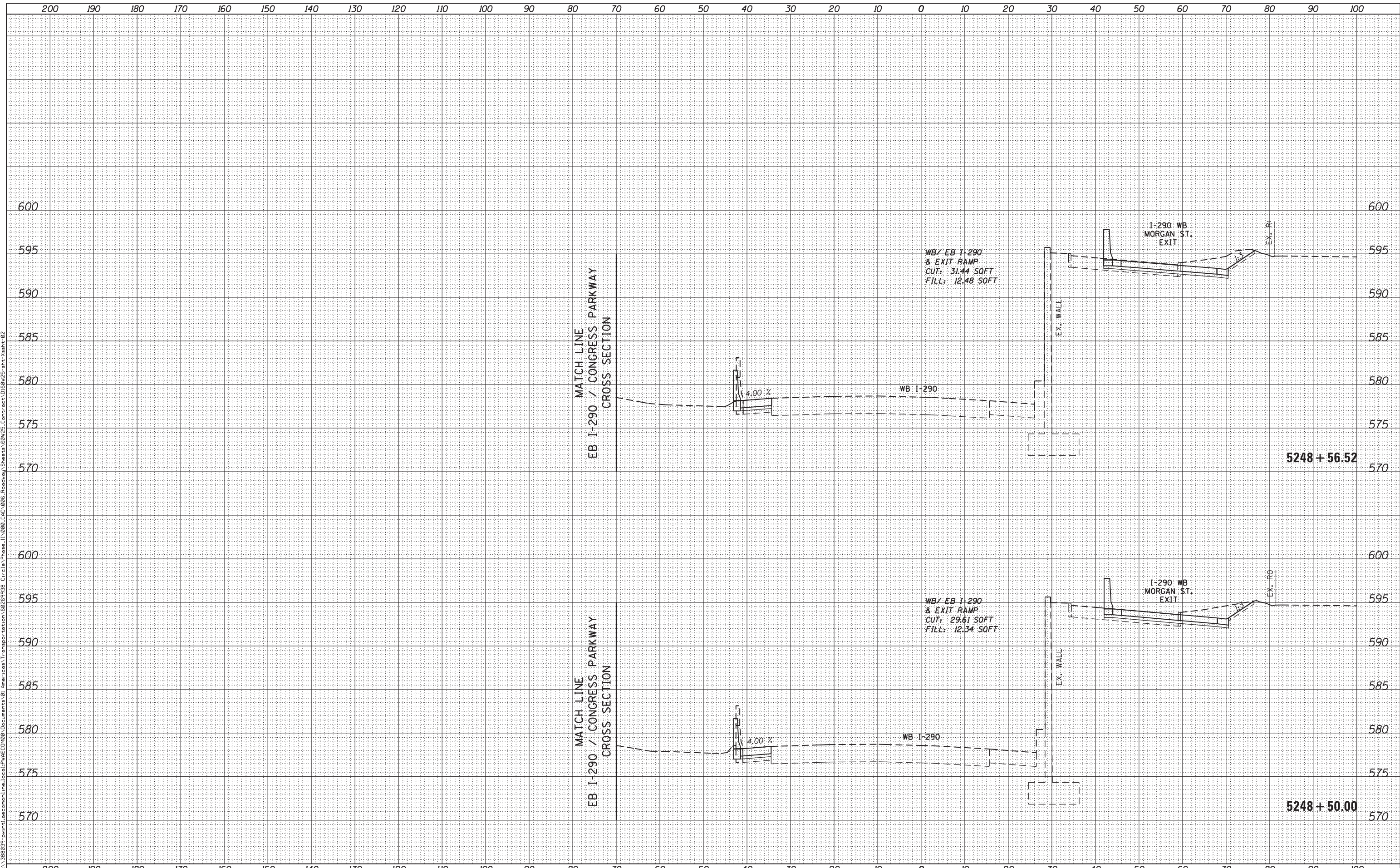
SCALE: 1:5V, 1:10H SHEET 10 OF 33 SHEETS STA. 5248+00.00 TO STA. 5248+27.20

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	282
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
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ORIGINAL SURVEY	
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D160W25-sht-Xsht-02
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

WB I-290 / EXIT RAMP TO MORGAN STREET CROSS SECTION

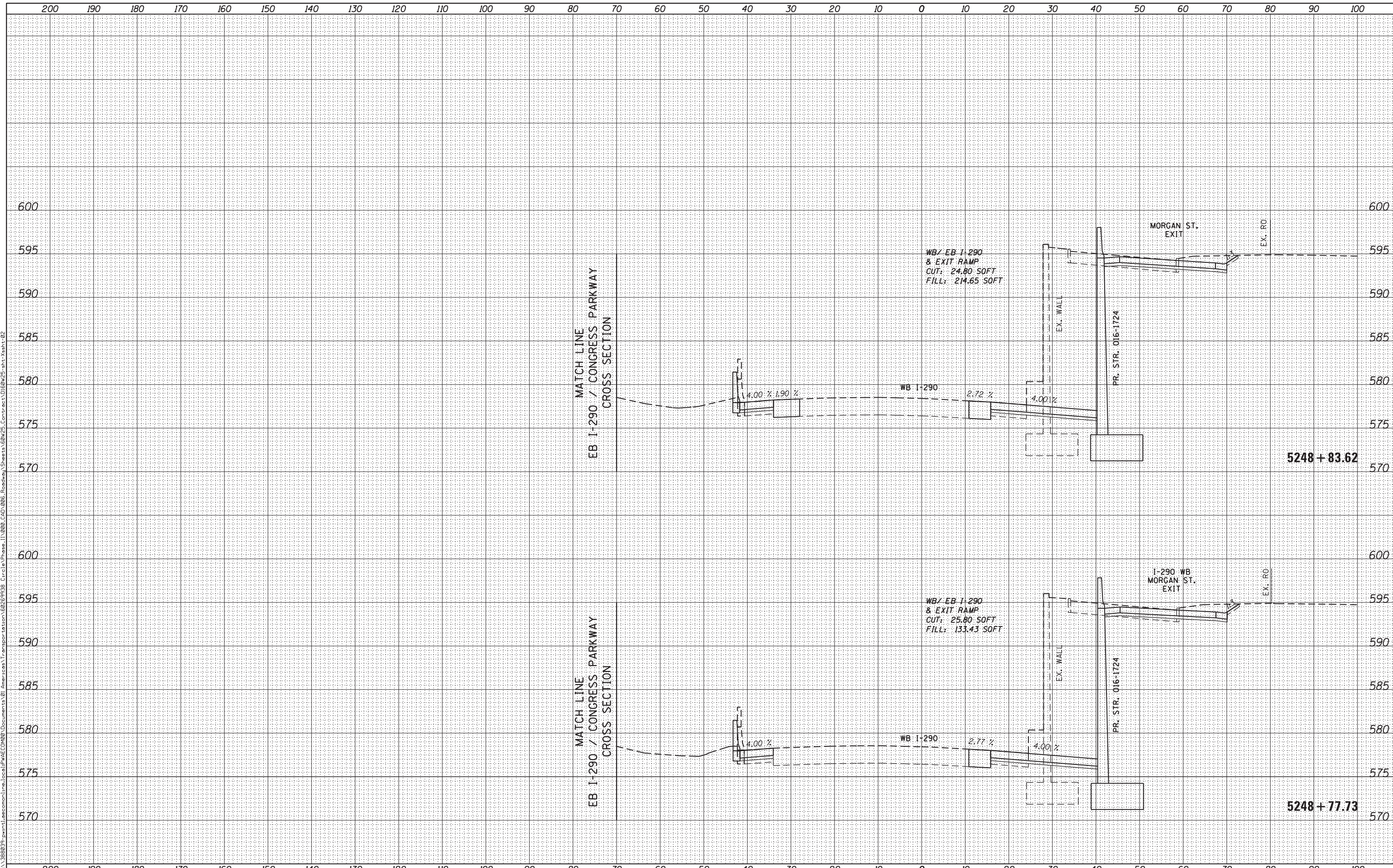
SCALE: 1:5V, 1:10H SHEET 11 OF 33 SHEETS STA. 5248+50.00 TO STA. 5248+56.52

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	283
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
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ORIGINAL SURVEY	
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D160W25-shr-Xsht-02
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

WB I-290 /EXIT RAMP TO MORGAN STREET CROSS SECTION

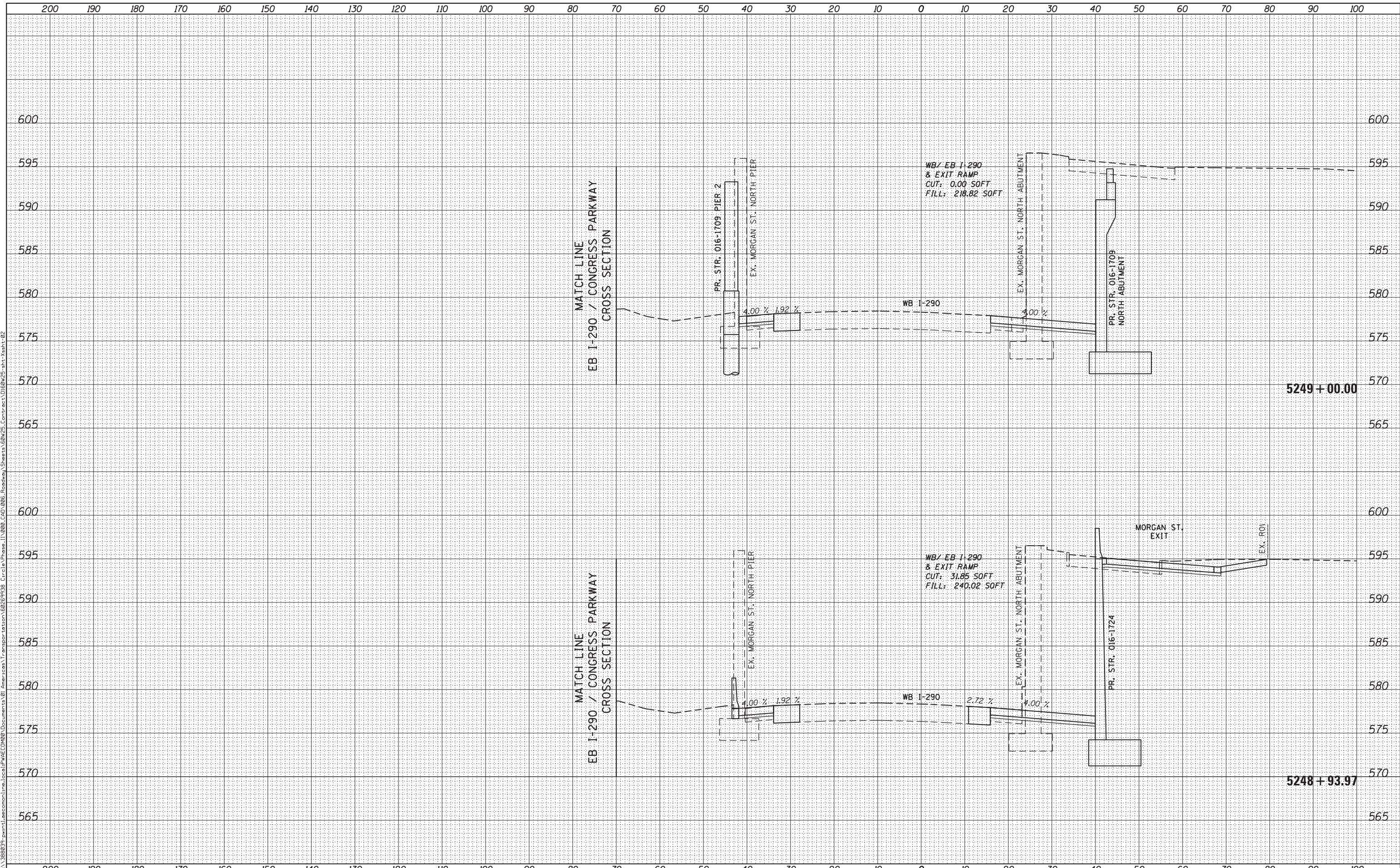
SCALE: 1:5V,1:10H SHEET 12 OF 33 SHEETS STA. 5248+77.73 TO STA. 5248+83.62

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	284
				CONTRACT NO. 60W25
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINL SURVEY	SURVEYED
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	AREAS CHECKED

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ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

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D160W25-sht-Xsht-02
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

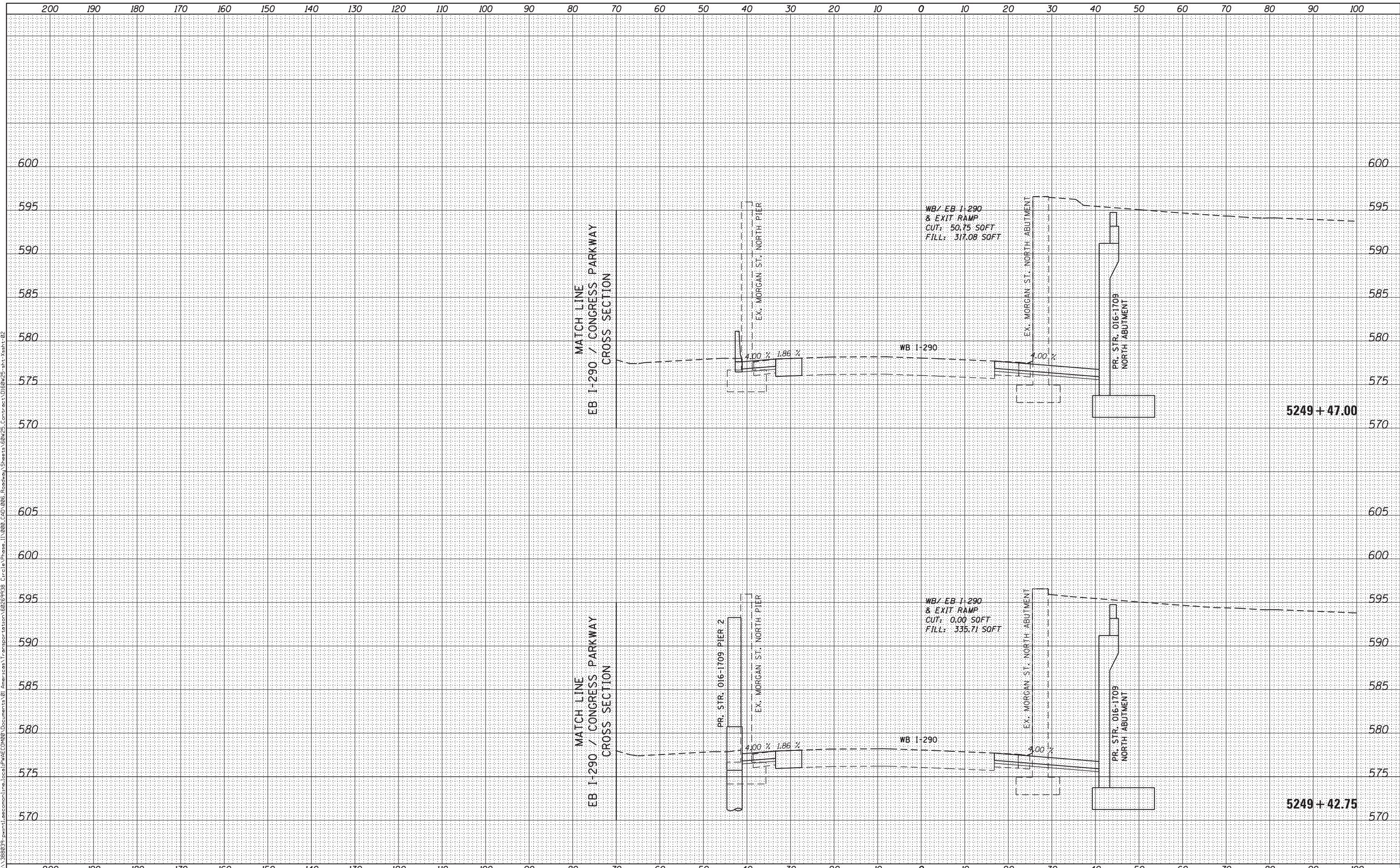
WB I-290 / EXIT RAMP TO MORGAN STREET CROSS SECTION
 SCALE: 1:5V,1:10H SHEET 13 OF 33 SHEETS STA. 5248+93.97 TO STA. 5249+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	285
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
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D160W25-sht-Xsht-02
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

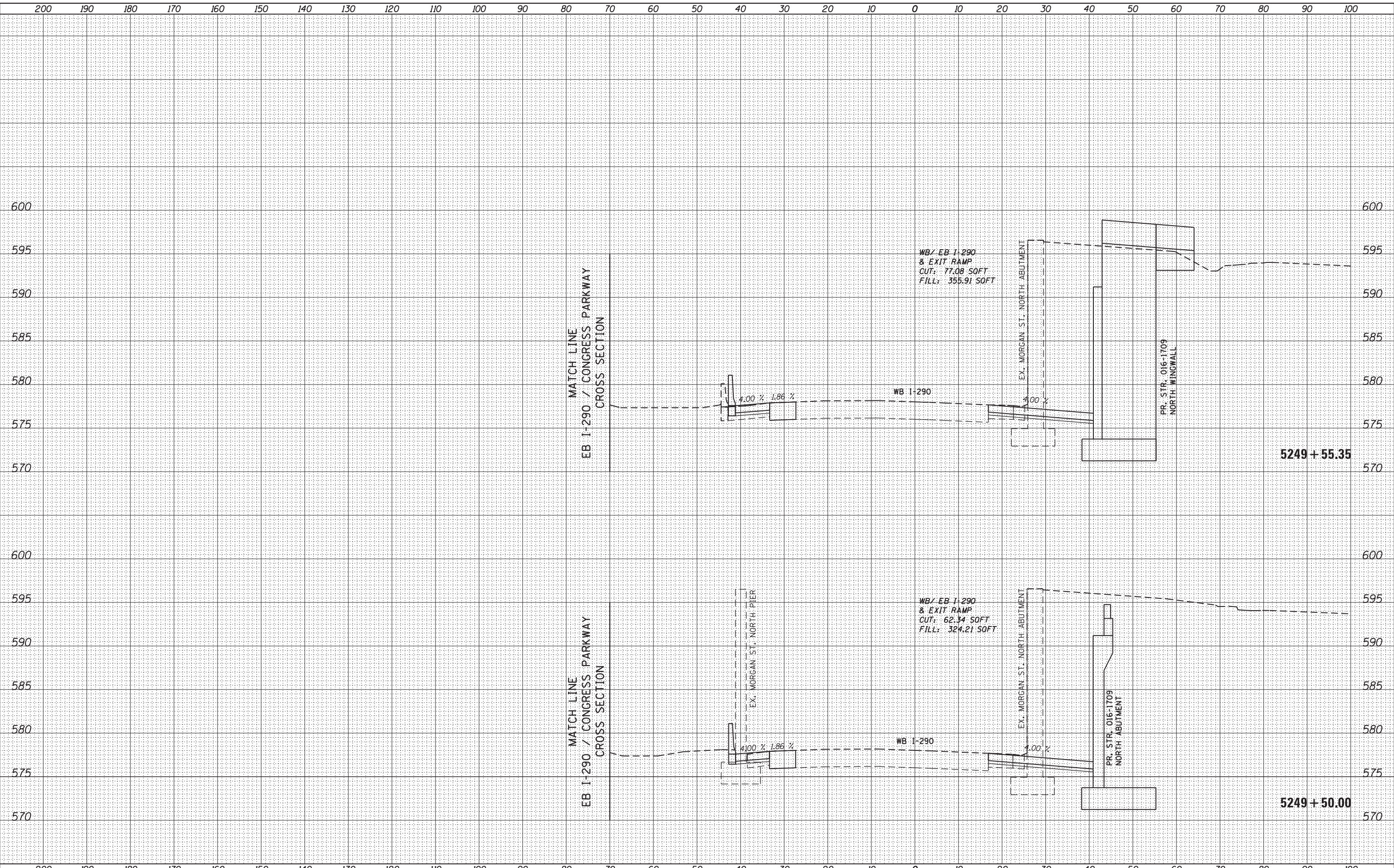
WB I-290 / EXIT RAMP TO MORGAN STREET CROSS SECTION
 SCALE: 1:5V, 1:10H SHEET 14 OF 33 SHEETS STA. 5249+42.75 TO STA. 5249+47.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	286
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

BY	DATE
FINL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
	TEMPLATE
	AREAS CHECKED

BY	DATE
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
	TEMPLATE
	AREAS CHECKED

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DI60W25-sht-Xsht-02
 USER NAME = pimsarno
 PLOT SCALE = 10.0000 "/>

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

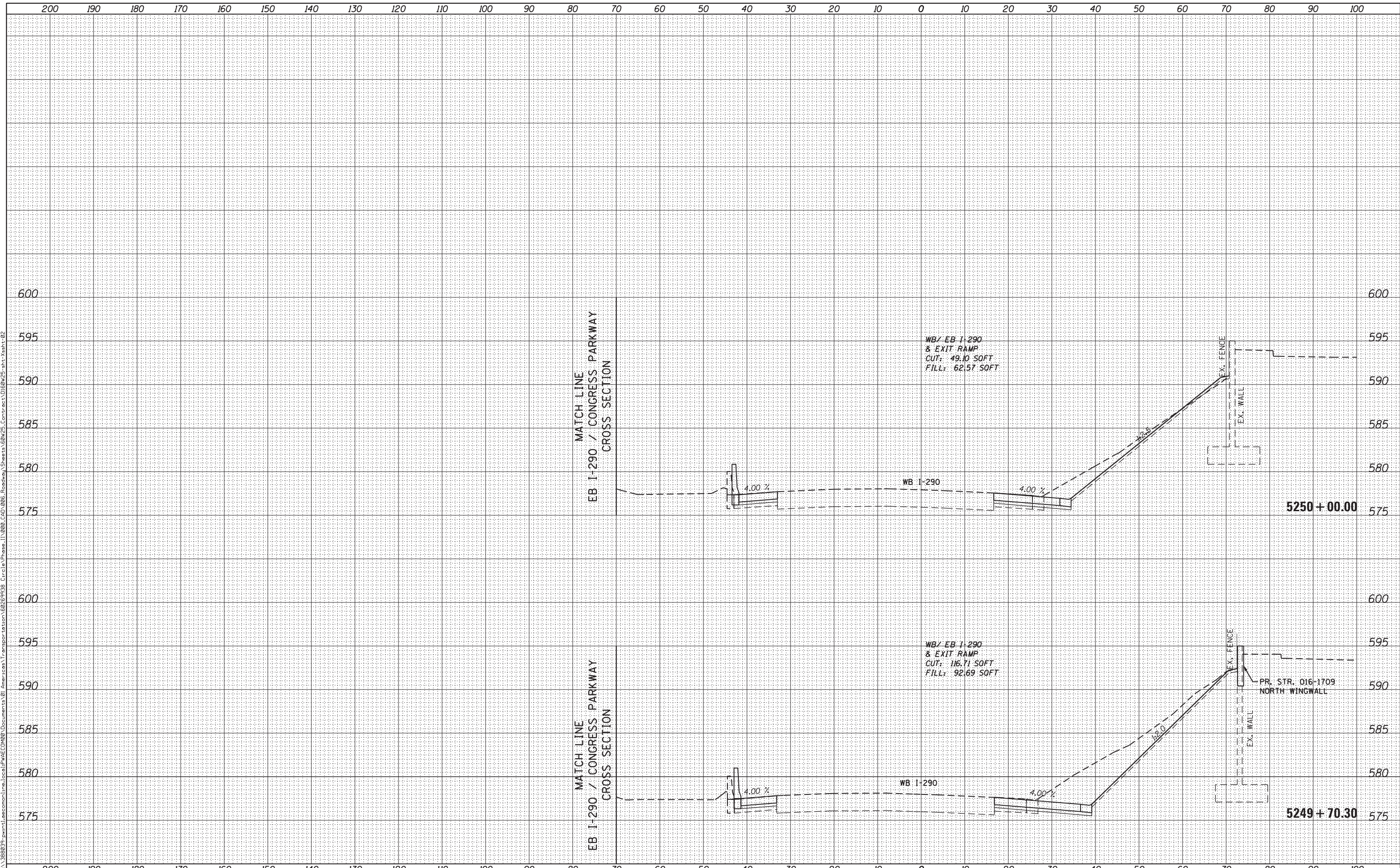
WB I-290 / EXIT RAMP TO MORGAN STREET CROSS SECTION

SCALE: 1:5V, 1:10H SHEET 15 OF 33 SHEETS STA. 5249+50.00 TO STA. 5249+55.35

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	287
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

DATE	
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DATE	
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TEMPLATE	
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WB/ EB I-290
& EXIT RAMP
CUT: 49.10 SOFT
FILL: 62.57 SOFT

WB/ EB I-290
& EXIT RAMP
CUT: 116.71 SOFT
FILL: 92.69 SOFT

MATCH LINE
EB I-290 / CONGRESS PARKWAY
CROSS SECTION

MATCH LINE
EB I-290 / CONGRESS PARKWAY
CROSS SECTION

5250 + 00.00

5249 + 70.30

PR. STR. 016-1709
NORTH WINGWALL



D160W25-sht-Xsht-02
USER NAME = pimsarno
PLOT SCALE = 10.0000' / in.
PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
DRAWN - AFC / KAM
CHECKED - DBM
DATE - 6/17/2013

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WB I-290 /EXIT RAMP TO MORGAN STREET CROSS SECTION

SCALE: 1:5V,1:10H SHEET 16 OF 33 SHEETS STA. 5249+70.30 TO STA. 5250+00.00

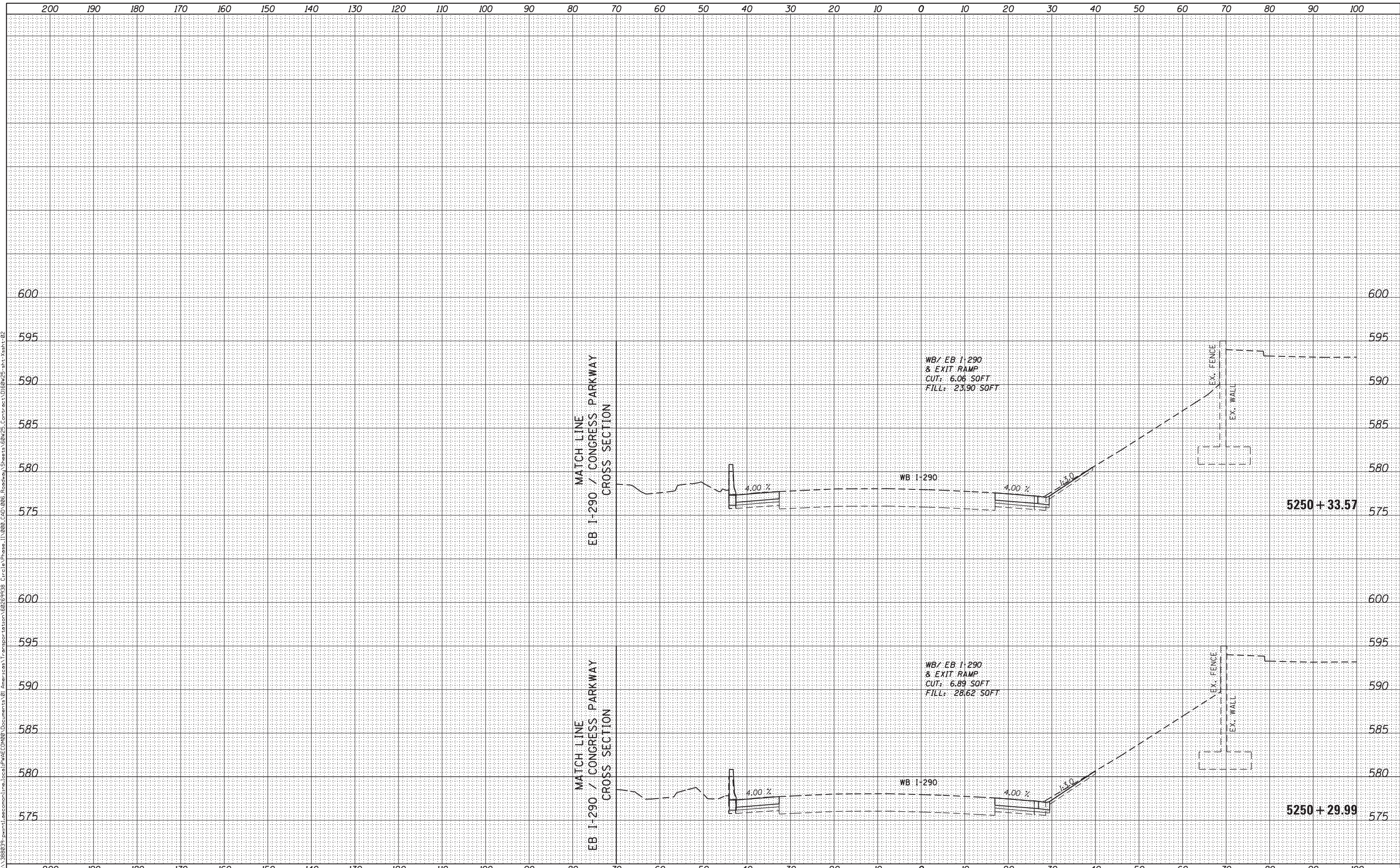
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	288
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

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FINISH SURVEY	
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D160W25-sht-Xsht-02
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

WB I-290 /EXIT RAMP TO MORGAN STREET CROSS SECTION

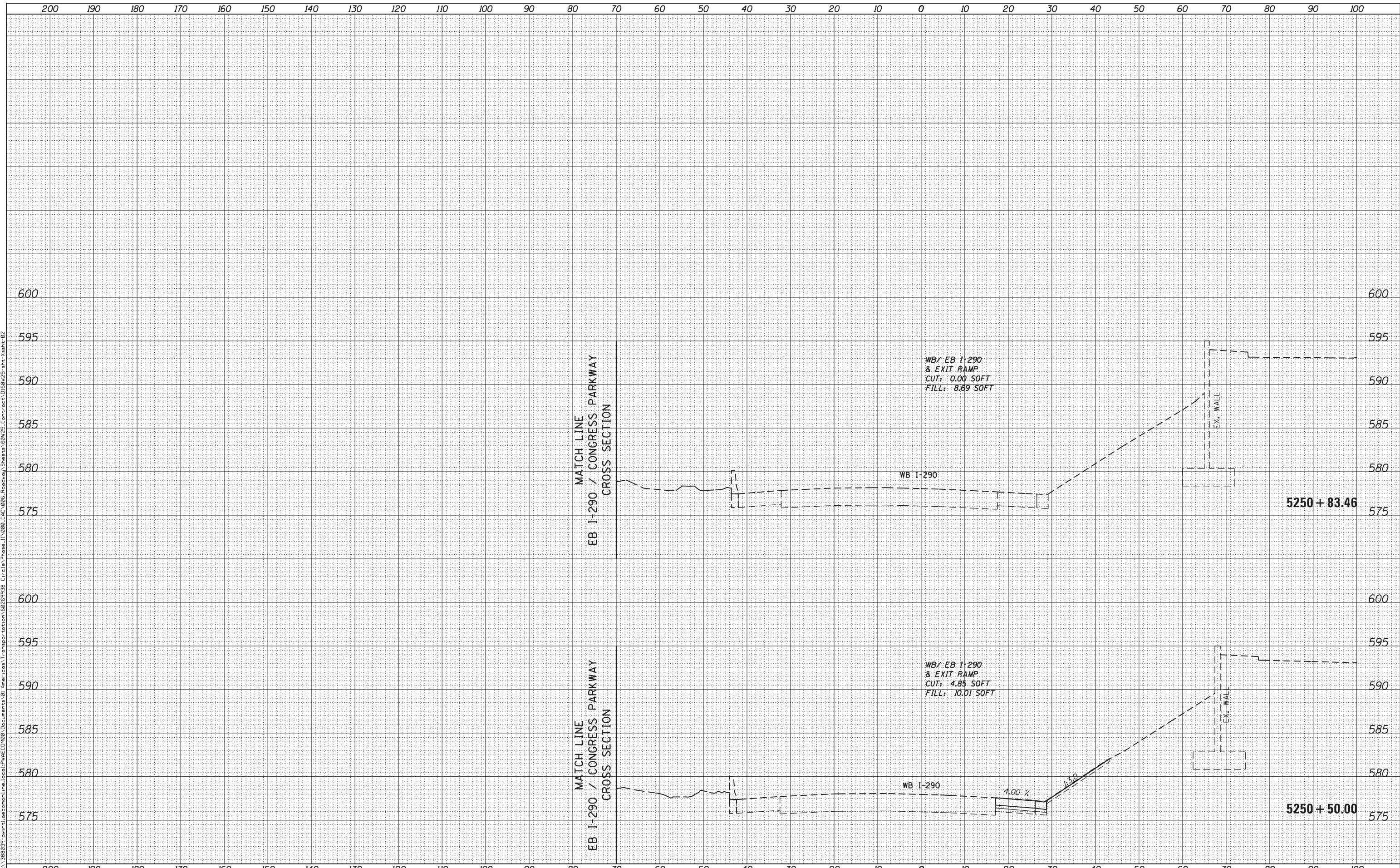
SCALE: 1:5V,1:10H SHEET 17 OF 33 SHEETS STA. 5250+29.99 TO STA. 5250+33.57

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	289
CONTRACT NO. 60W25			ILLINOIS FED. AID PROJECT	

DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
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D160W25-sht-Xsht-02
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

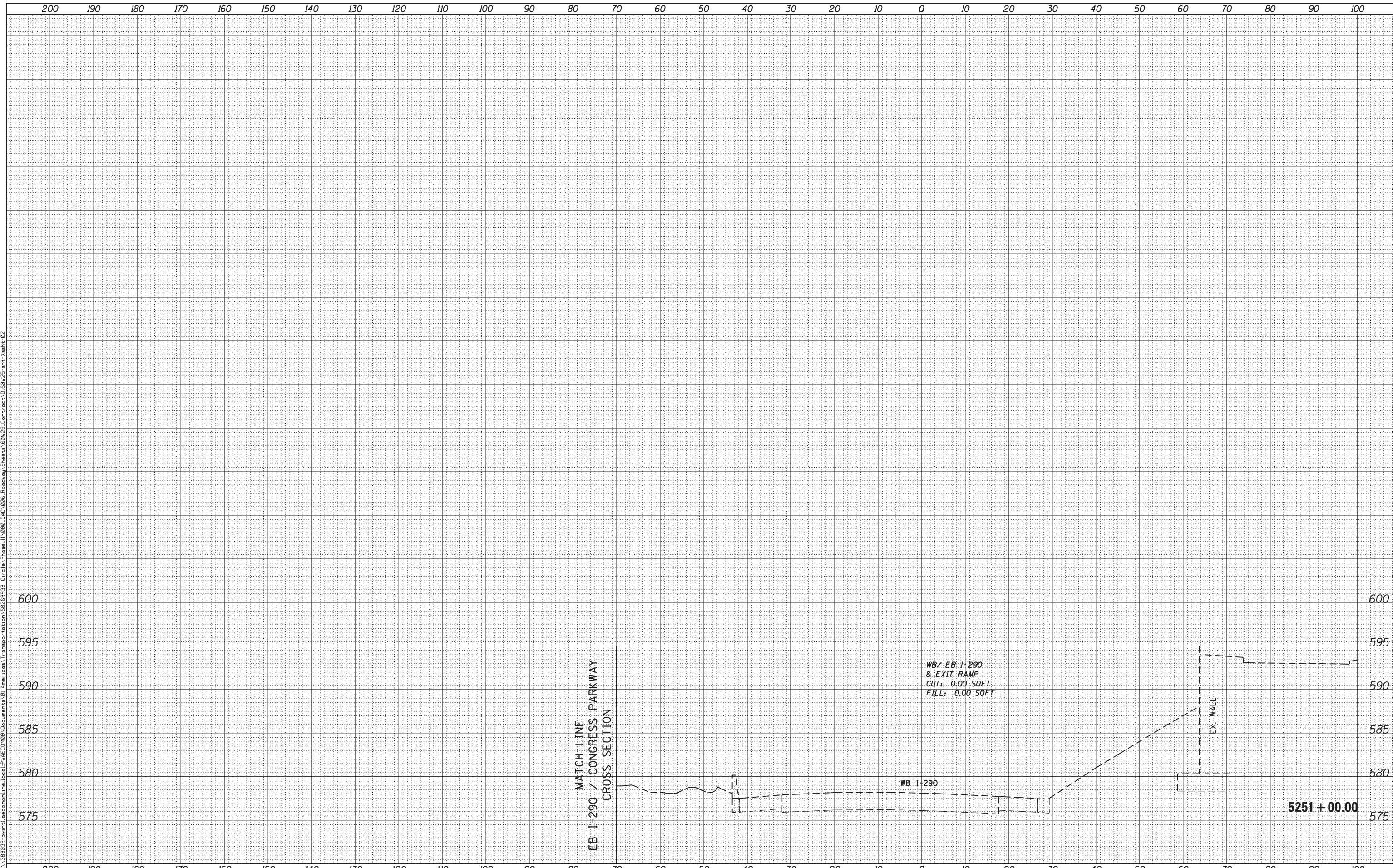
DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

WB I-290 / EXIT RAMP TO MORGAN STREET CROSS SECTION
 SCALE: 1:5V, 1:10H SHEET 18 OF 33 SHEETS STA. 5250+50.00 TO STA. 5250+83.46

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	290
CONTRACT NO. 60W25			ILLINOIS FED. AID PROJECT	



BY	DATE

BY	DATE



DESIGNED - AFC / KAM	REVISED -
DRAWN - AFC / KAM	REVISED -
CHECKED - DBM	REVISED -
DATE - 6/17/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

WB I-290 /EXIT RAMP TO MORGAN STREET CROSS SECTION

SCALE: 1:5V,1:10H SHEET 19 OF 33 SHEETS STA. 5251+00.00 TO STA. 5251+00.00

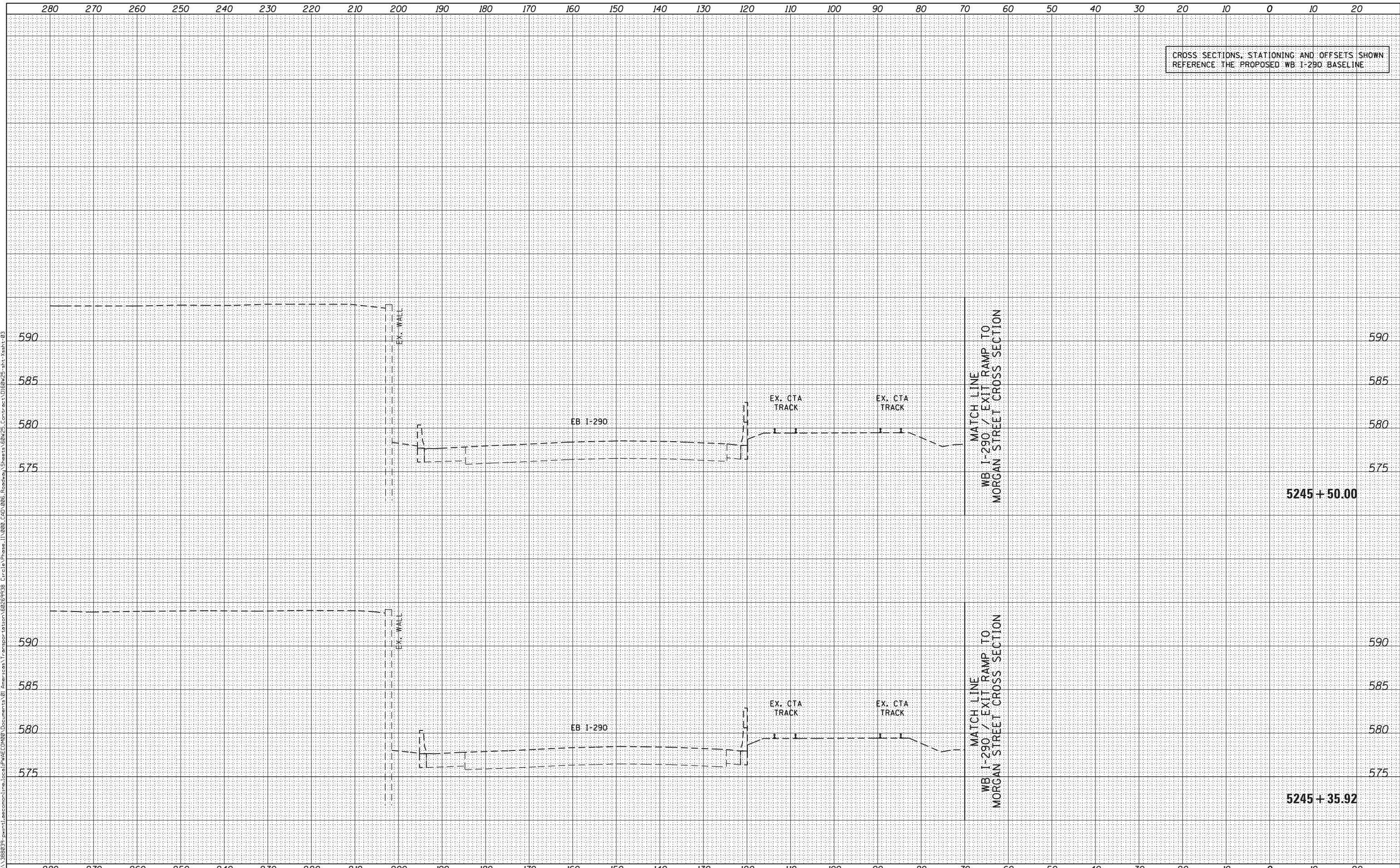
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	291
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

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CROSS SECTIONS, STATIONING AND OFFSETS SHOWN
REFERENCE THE PROPOSED WB I-290 BASELINE

DATE	
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DI60W25-sht-Xsht-03
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / 1"
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EB I-290 / CONGRESS PARKWAY CROSS SECTION

SCALE: 1:5V, 1:10H SHEET 20 OF 33 SHEETS STA. 5245+35.92 TO STA. 5245+50.00

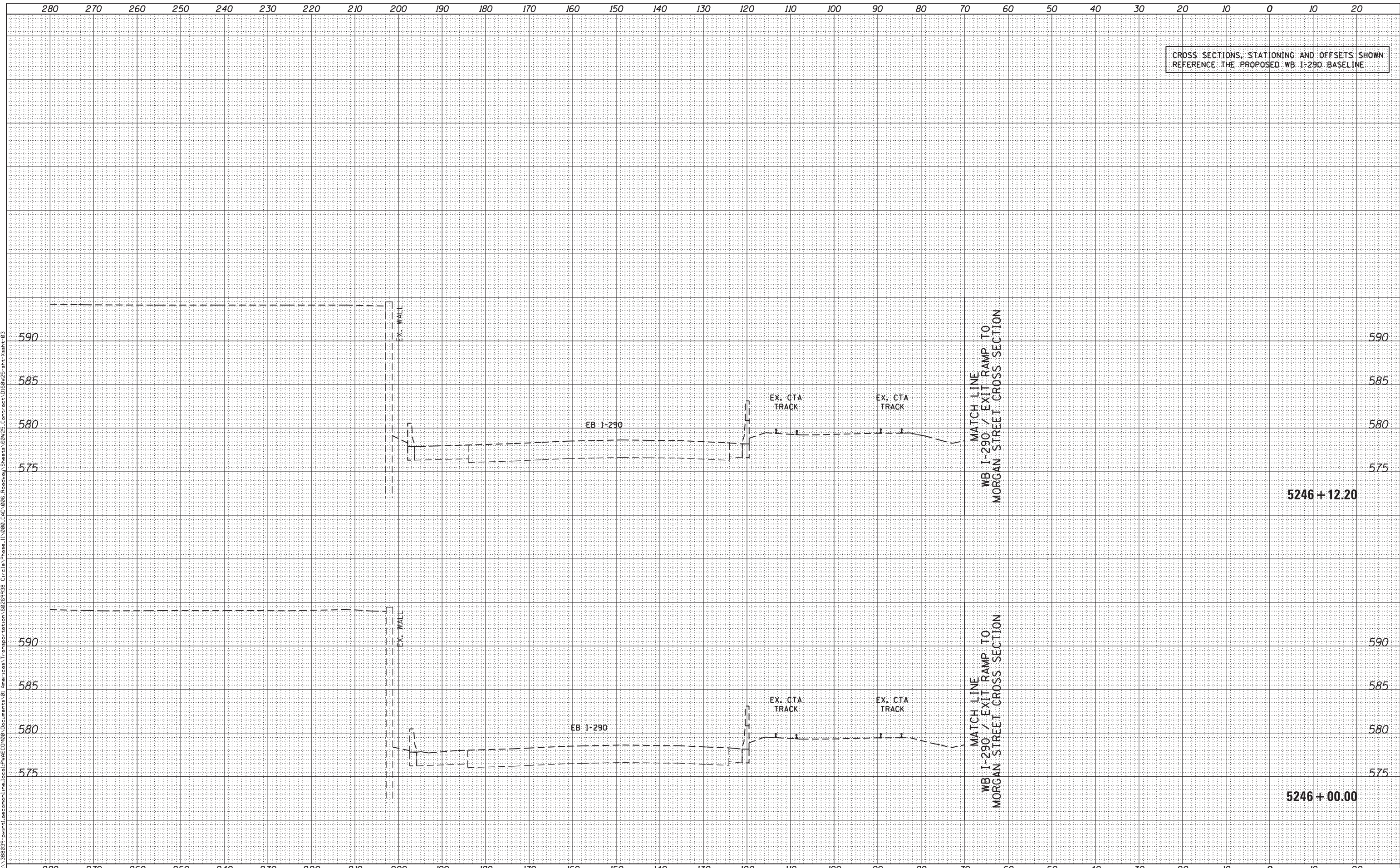
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	292
CONTRACT NO. 60W25				

ILLINOIS FED. AID PROJECT

CROSS SECTIONS, STATIONING AND OFFSETS SHOWN
REFERENCE THE PROPOSED WB I-290 BASELINE

DATE	
BY	
SURVEYED	
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TEMPLATE	
NOTE BOOK	
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D160W25-sht-Xsht-03
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EB I-290 / CONGRESS PARKWAY CROSS SECTION

SCALE: 1:5V, 1:10H SHEET 21 OF 33 SHEETS STA. 5246+00.00 TO STA. 5246+12.20

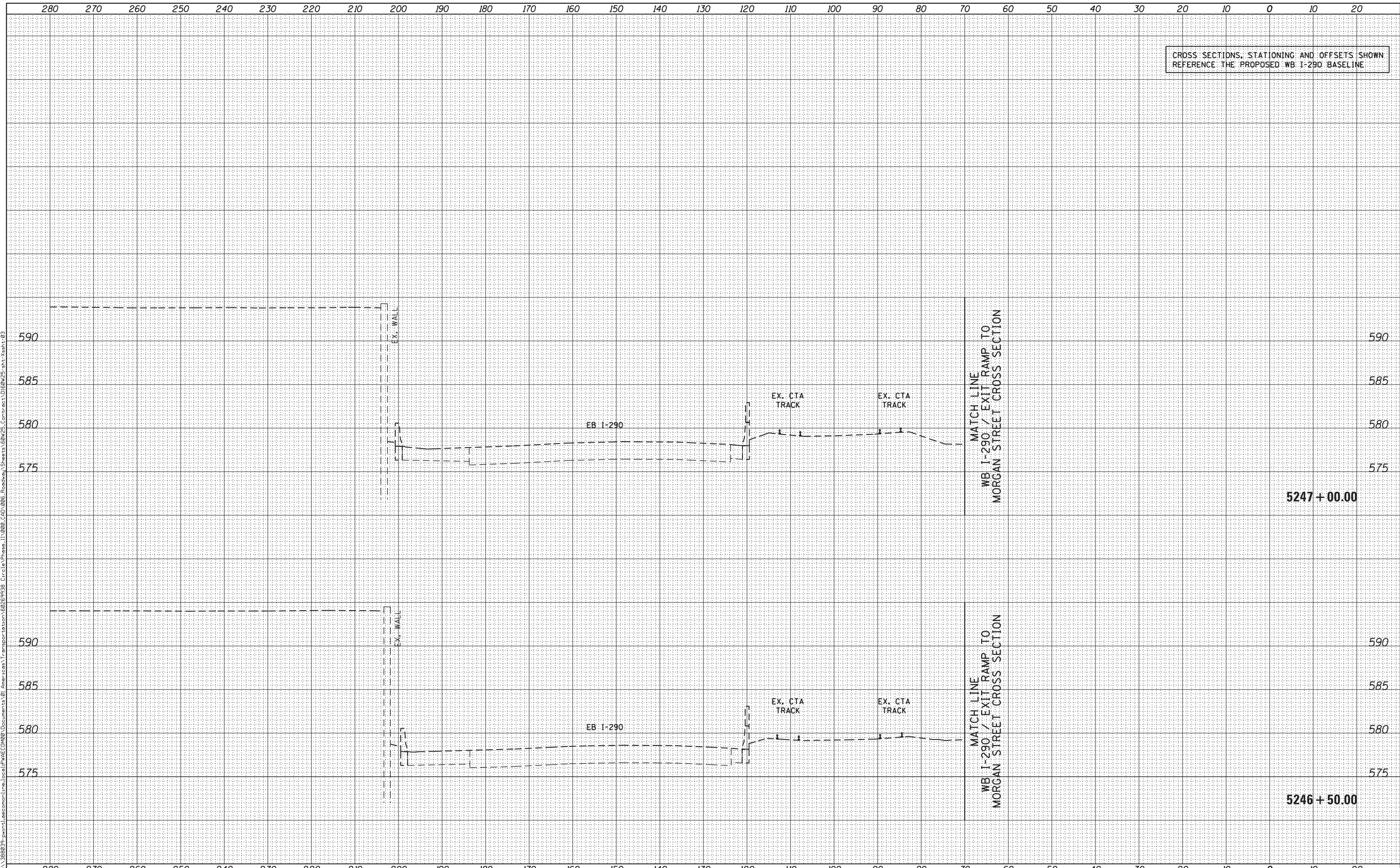
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	293
CONTRACT NO. 60W25				

ILLINOIS FED. AID PROJECT

CROSS SECTIONS, STATIONING AND OFFSETS SHOWN
REFERENCE THE PROPOSED WB I-290 BASELINE

DATE	
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FINISHED SURVEY	
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DI60W25-sht-Xsht-03
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EB I-290 / CONGRESS PARKWAY CROSS SECTION

SCALE: 1:5V, 1:10H SHEET 22 OF 33 SHEETS STA. 5246+50.00 TO STA. 5247+00.00

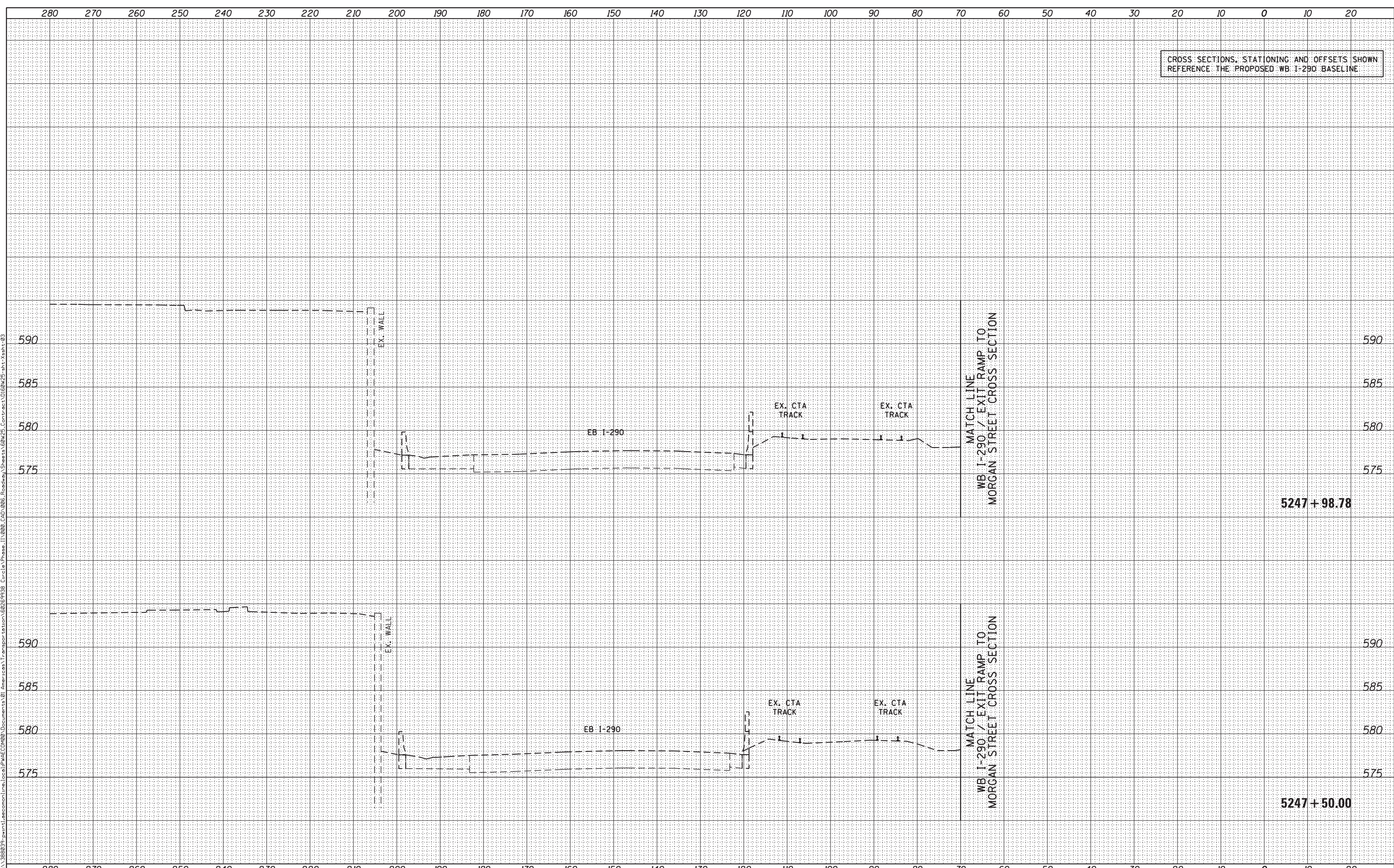
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	294
CONTRACT NO. 60W25				

ILLINOIS FED. AID PROJECT

CROSS SECTIONS, STATIONING AND OFFSETS SHOWN
REFERENCE THE PROPOSED WB I-290 BASELINE

DATE	
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D160W25-sht-Xsht-03
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / 1"
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EB I-290 / CONGRESS PARKWAY CROSS SECTION

SCALE: 1:5V, 1:10H SHEET 23 OF 33 SHEETS STA. 5247+50.00 TO STA. 5247+98.78

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	295
CONTRACT NO. 60W25				

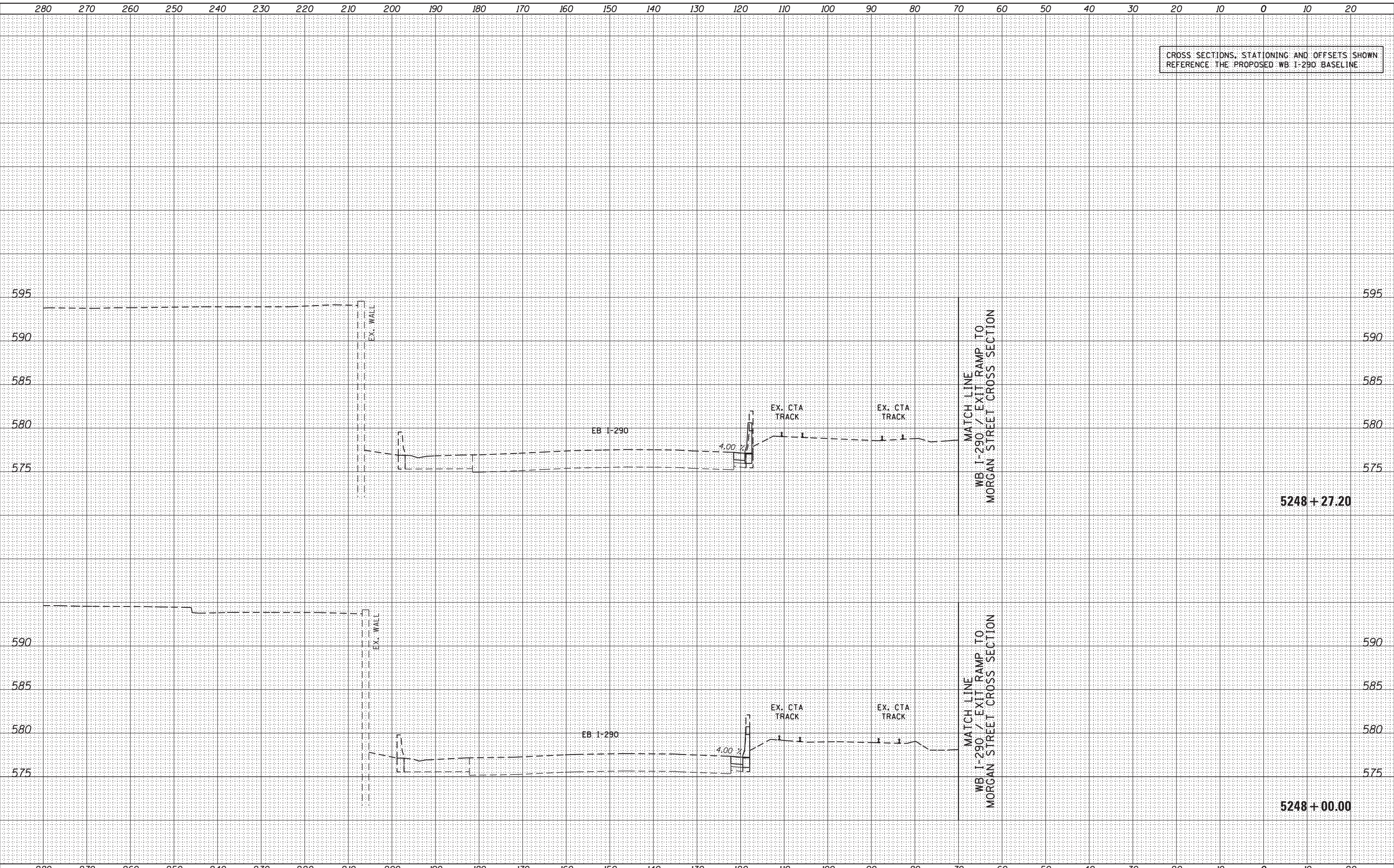
ILLINOIS FED. AID PROJECT

CROSS SECTIONS, STATIONING AND OFFSETS SHOWN
REFERENCE THE PROPOSED WB I-290 BASELINE

DATE	BY	SURVEYED	PLOTTED	TEMPLATE	AREAS CHECKED
		FINL	SURVEY	NOTE BOOK	AREAS CHECKED

DATE	BY	SURVEYED	PLOTTED	TEMPLATE	AREAS CHECKED
		ORIG	SURVEY	NOTE BOOK	AREAS CHECKED

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D160W25-shr-Xsht-03
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EB I-290 / CONGRESS PARKWAY CROSS SECTION

SCALE: 1:5V,1:10H SHEET 24 OF 33 SHEETS STA. 5248+00.00 TO STA. 5248+27.20

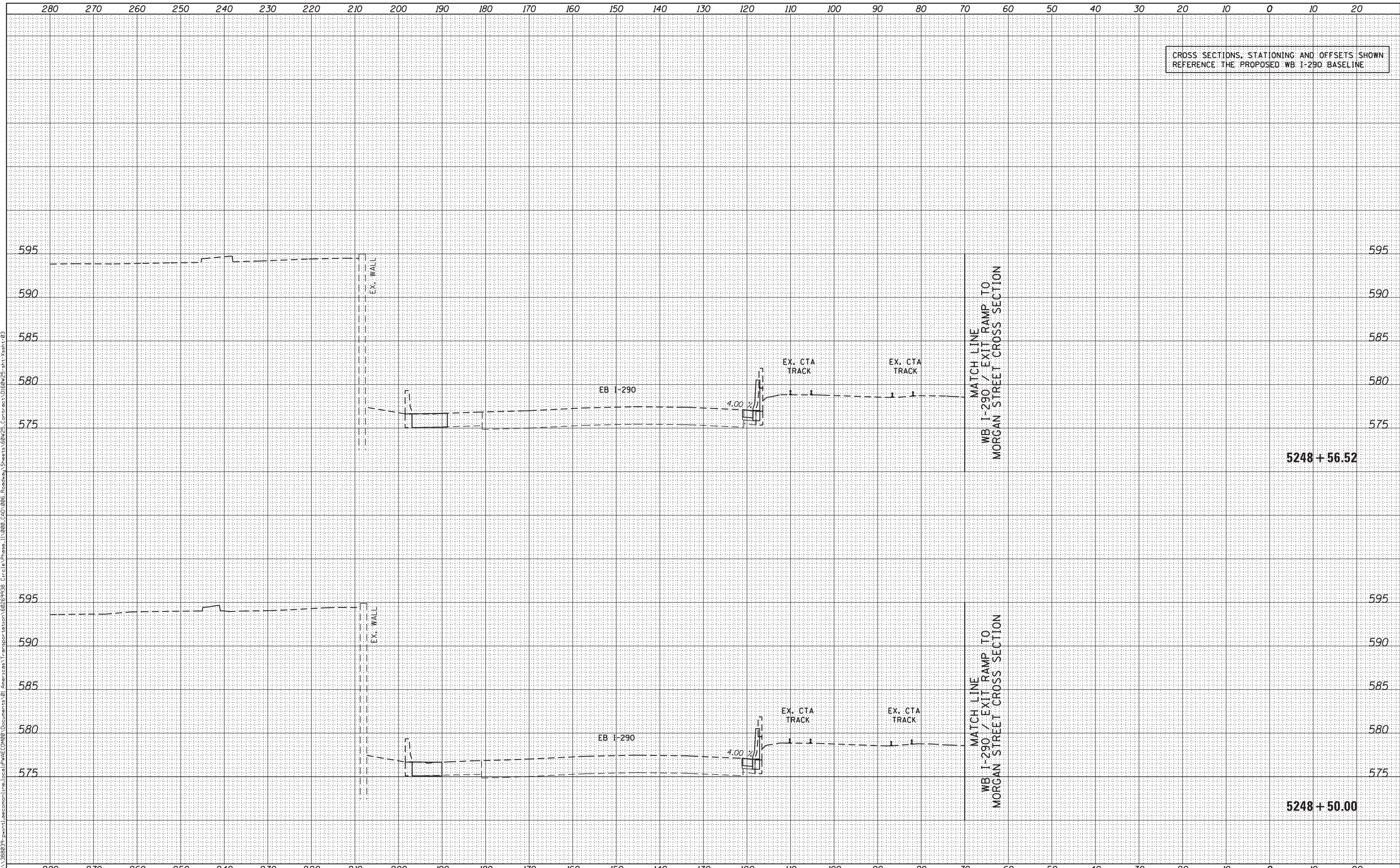
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	296
CONTRACT NO. 60W25				

ILLINOIS FED. AID PROJECT

CROSS SECTIONS, STATIONING AND OFFSETS SHOWN
REFERENCE THE PROPOSED WB I-290 BASELINE

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FINISH SURVEY	
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FINISH SURVEY	
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AREAS CHECKED	
NO.	



D160W25-sht-Xsht-03	DESIGNED - AFC / KAM	REVISED -
USER NAME = pimsarno	DRAWN - AFC / KAM	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - DBM	REVISED -
PLOT DATE = 6/14/2013	DATE - 6/17/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EB I-290 / CONGRESS PARKWAY CROSS SECTION

SCALE: 1:5V, 1:10H SHEET 25 OF 33 SHEETS STA. 5248+50.00 TO STA. 5248+56.52

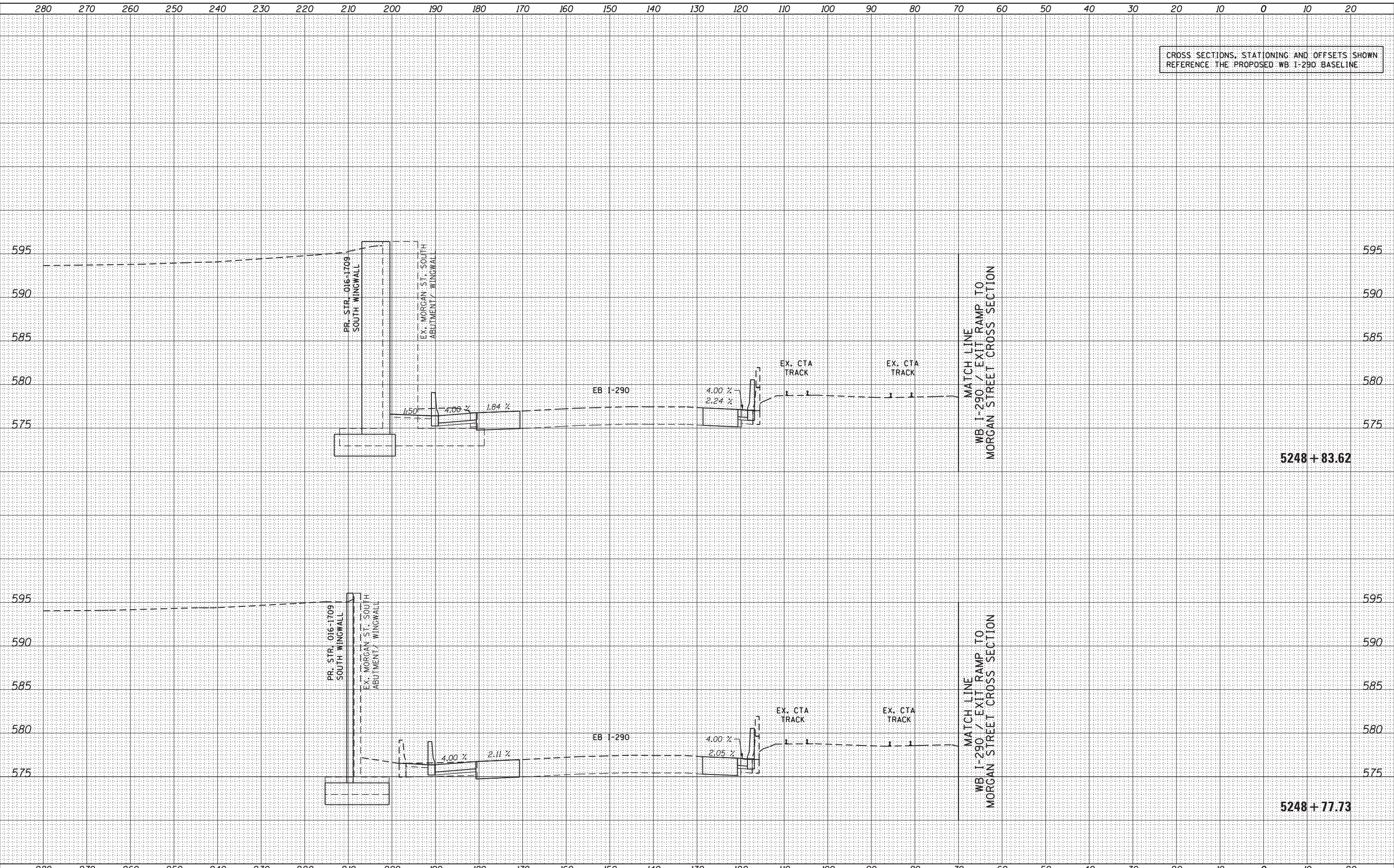
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	297
				CONTRACT NO. 60W25
ILLINOIS FED. AID PROJECT				

CROSS SECTIONS, STATIONING AND OFFSETS SHOWN
REFERENCE THE PROPOSED WB I-290 BASELINE

DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
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DATE	
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ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
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D160W25-sht-Xsht-03
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EB I-290 / CONGRESS PARKWAY CROSS SECTION

SCALE: 1:5V, 1:10H SHEET 26 OF 33 SHEETS STA. 5248+77.73 TO STA. 5248+83.62

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	298
CONTRACT NO. 60W25				

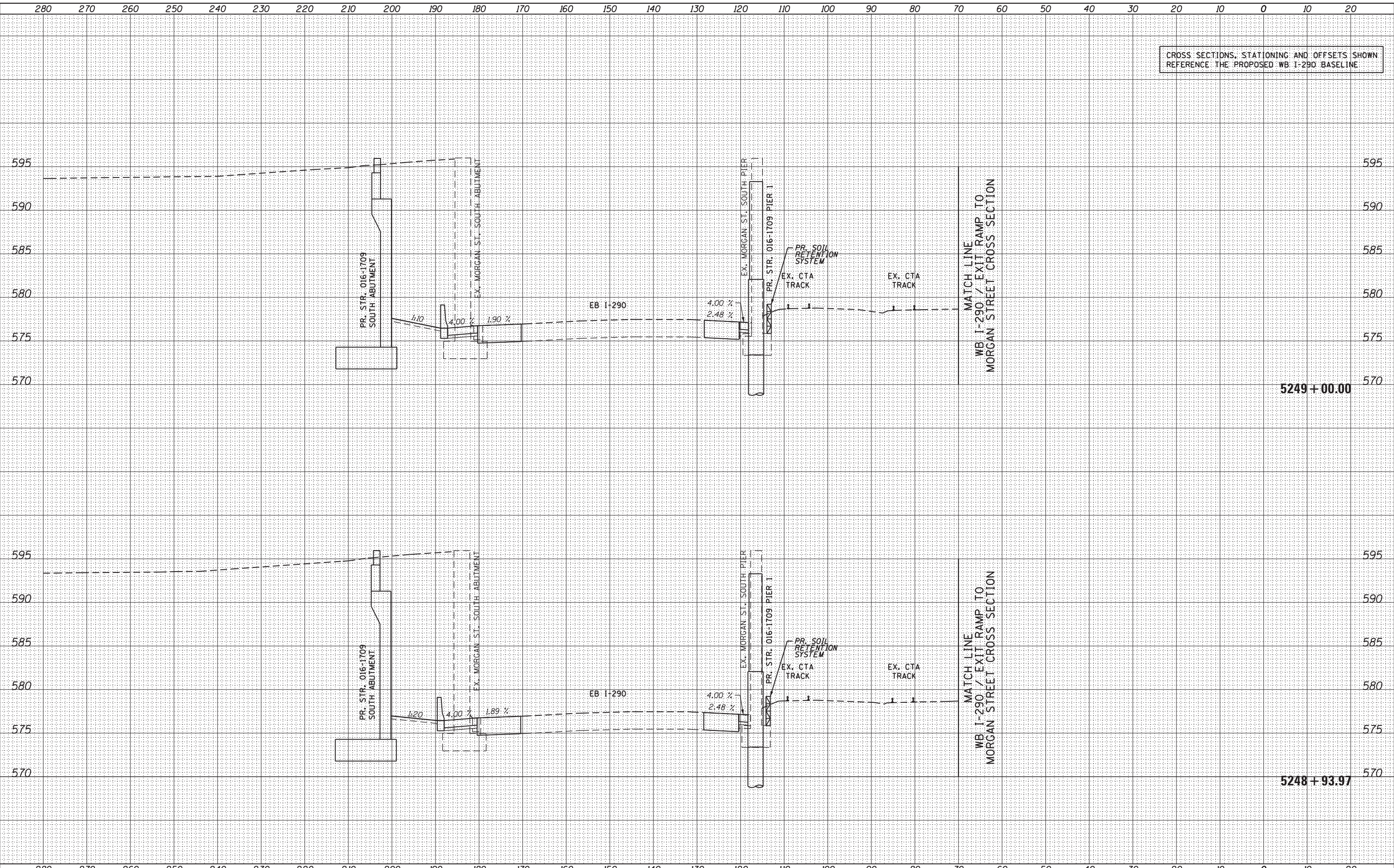
ILLINOIS FED. AID PROJECT

CROSS SECTIONS, STATIONING AND OFFSETS SHOWN
REFERENCE THE PROPOSED WB I-290 BASELINE

DATE	
BY	
FINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
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ORIGINAL SURVEY	
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NOTE BOOK	
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D160W25-shr-Xsht-03
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EB I-290 / CONGRESS PARKWAY CROSS SECTION

SCALE: 1:5V, 1:10H SHEET 27 OF 33 SHEETS STA. 5248+93.97 TO STA. 5249+00.00

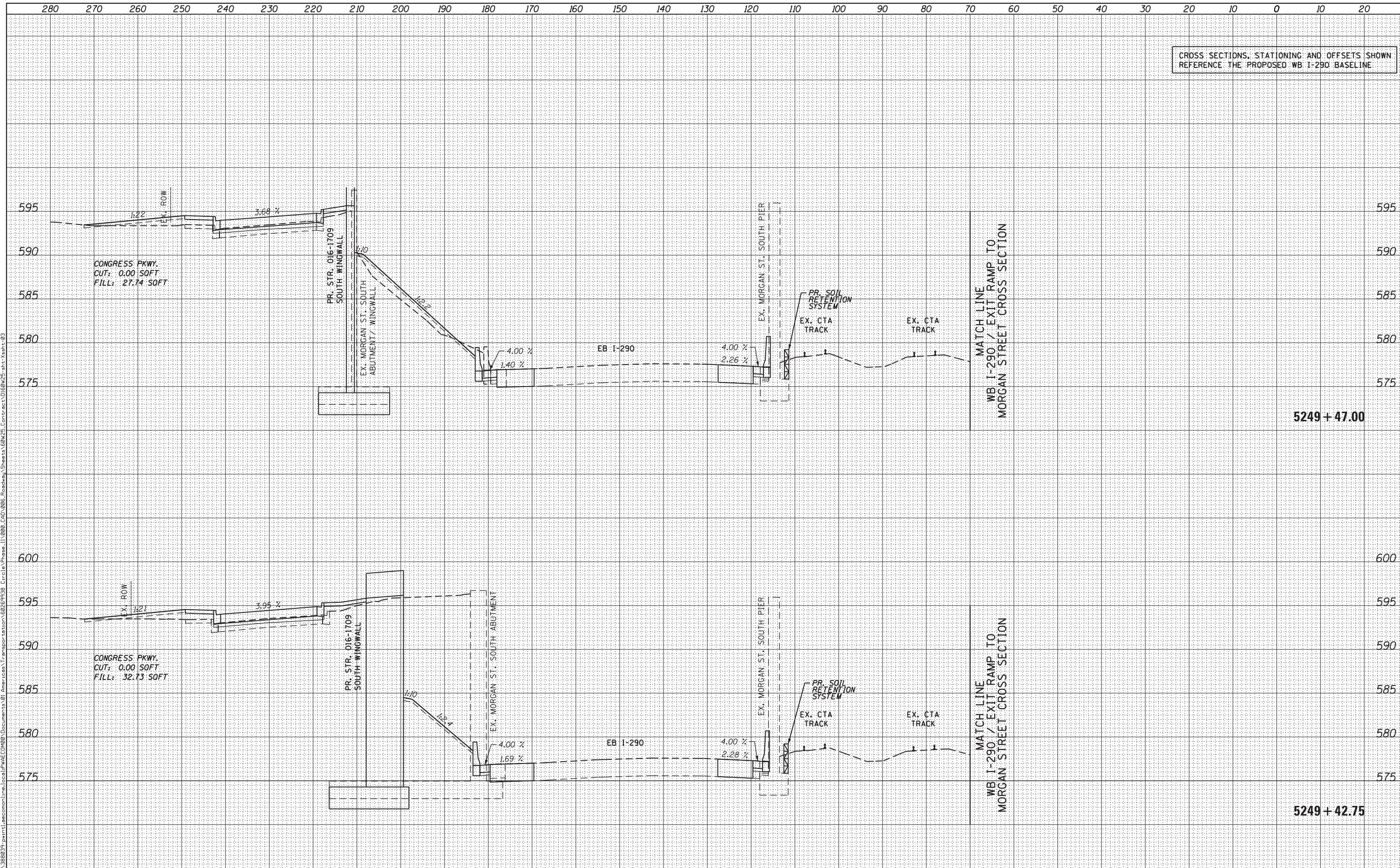
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	299
CONTRACT NO. 60W25				

ILLINOIS FED. AID PROJECT

CROSS SECTIONS, STATIONING AND OFFSETS SHOWN
REFERENCE THE PROPOSED WB I-290 BASELINE

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BY	DATE



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D16W25-sht-Xsht-03
USER NAME = pimsarno
PLOT SCALE = 10.0000' / in.
PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
DRAWN - AFC / KAM
CHECKED - DBM
DATE - 6/17/2013

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EB I-290 / CONGRESS PARKWAY CROSS SECTION

SCALE: 1:5V,1:10H SHEET 28 OF 33 SHEETS STA. 5249+42.75 TO STA. 5249+47.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	300
CONTRACT NO. 60W25				

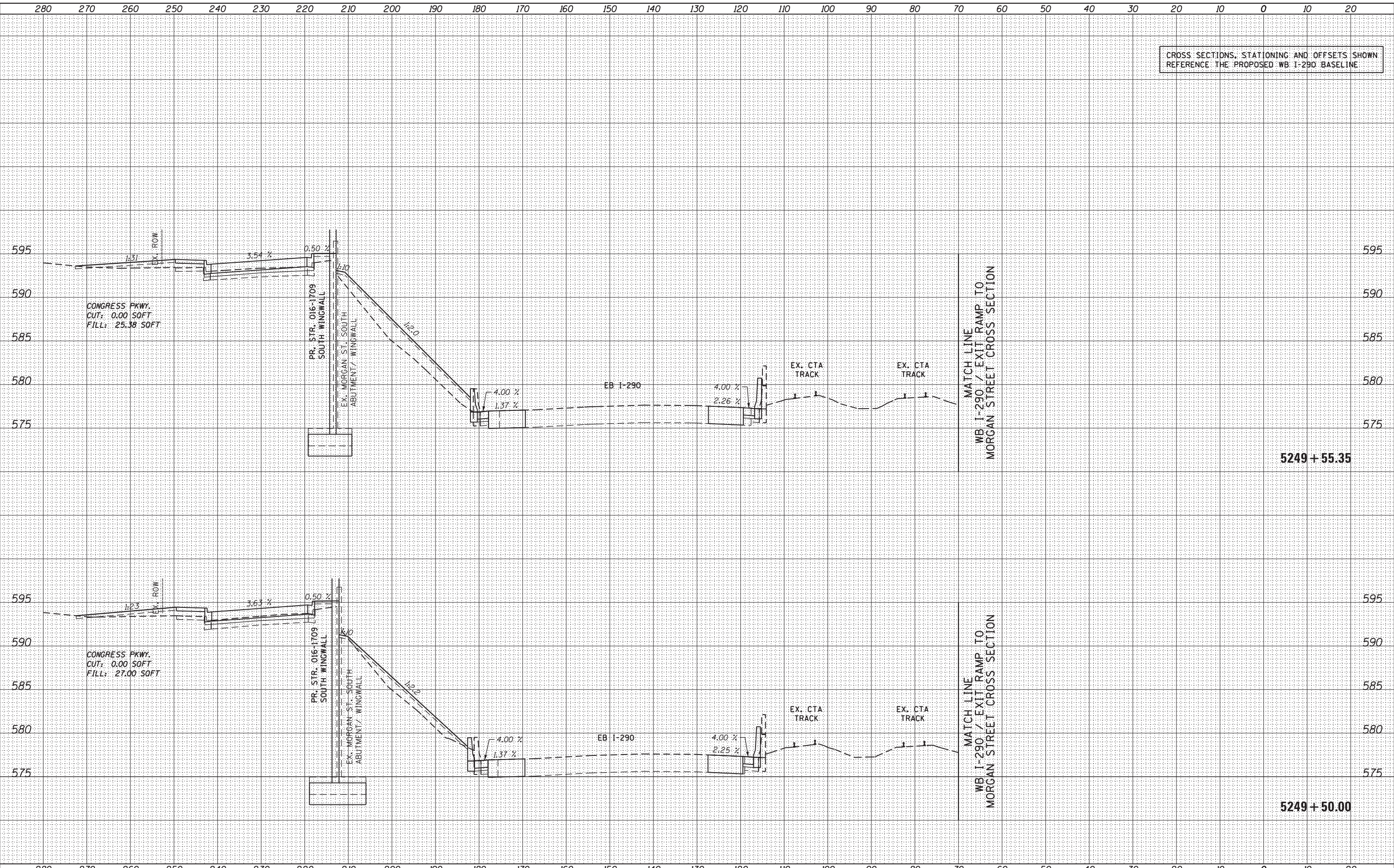
ILLINOIS FED. AID PROJECT

CROSS SECTIONS, STATIONING AND OFFSETS SHOWN
REFERENCE THE PROPOSED WB I-290 BASELINE

DATE	
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D160W25-sht-Xsht-03
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EB I-290 / CONGRESS PARKWAY CROSS SECTION

SCALE: 1:5V, 1:10H SHEET 29 OF 33 SHEETS STA. 5249+50.00 TO STA. 5249+55.35

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	301
CONTRACT NO. 60W25				

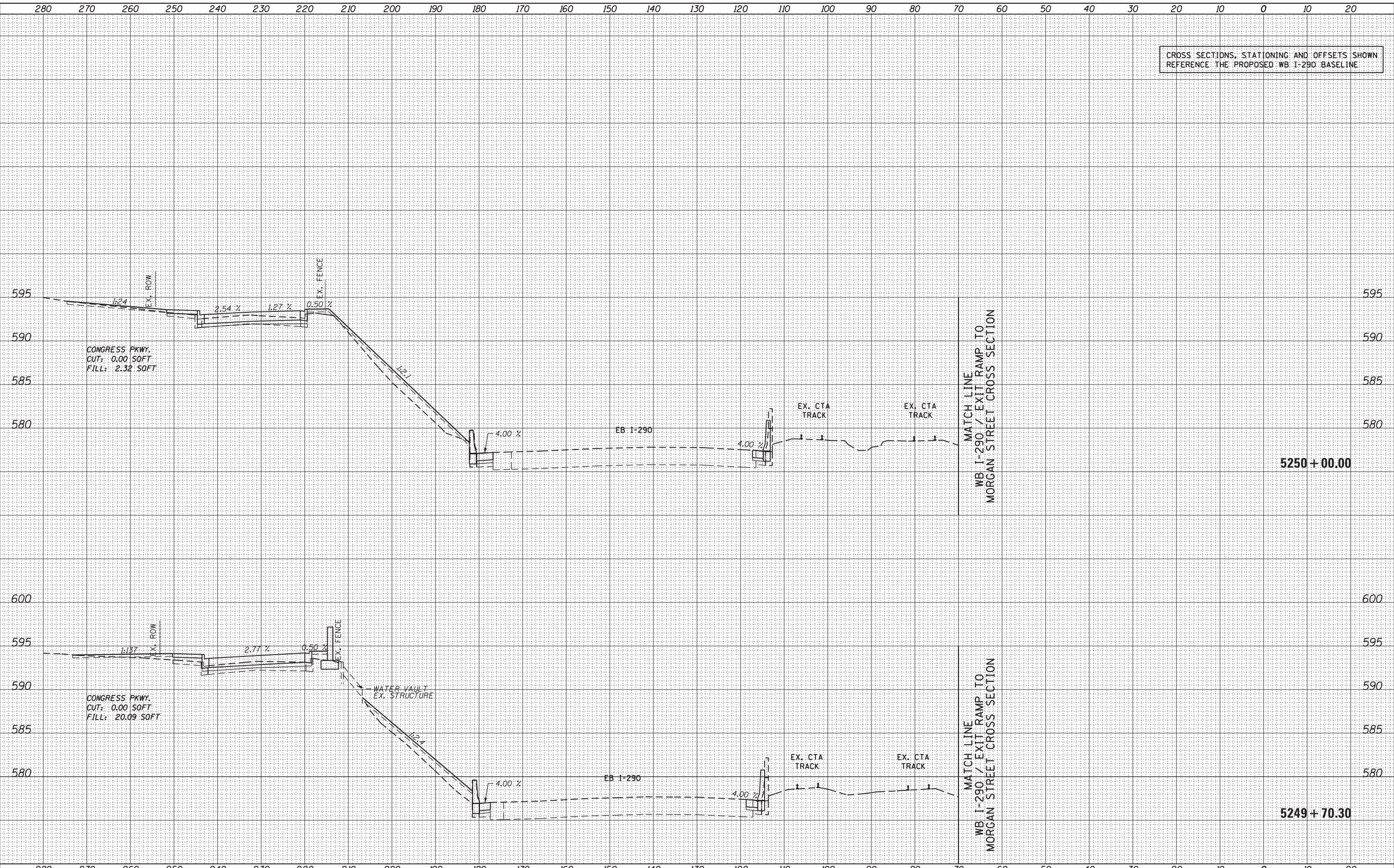
ILLINOIS FED. AID PROJECT

CROSS SECTIONS, STATIONING AND OFFSETS SHOWN
REFERENCE THE PROPOSED WB I-290 BASELINE

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D160W25-sht-Xsht-03
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EB I-290 / CONGRESS PARKWAY CROSS SECTION

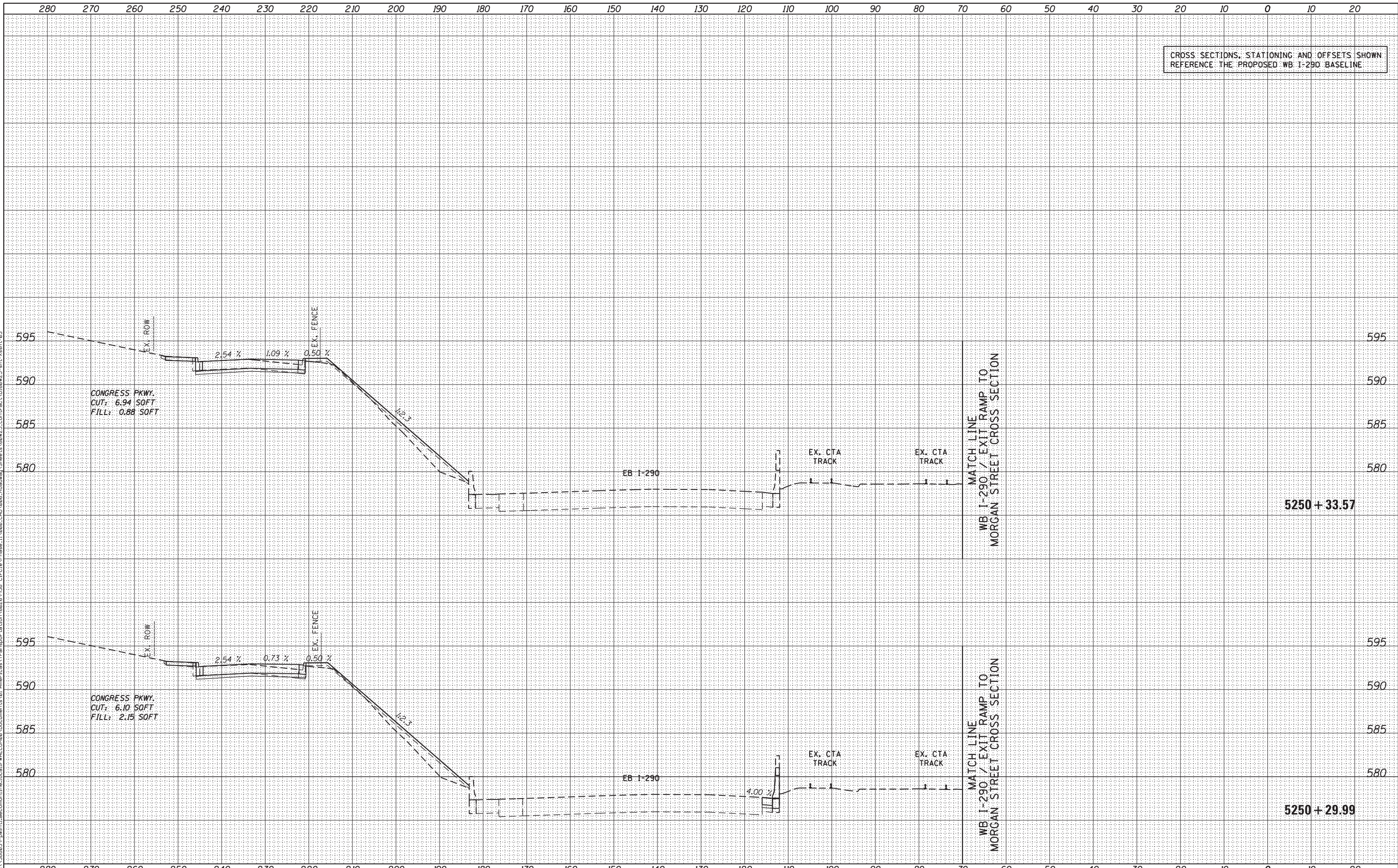
SCALE: 1:5V, 1:10H SHEET 30 OF 33 SHEETS STA. 5249+70.30 TO STA. 5250+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	302
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

CROSS SECTIONS, STATIONING AND OFFSETS SHOWN
REFERENCE THE PROPOSED WB I-290 BASELINE

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ORIGINAL SURVEY	
NOTE BOOK	
AREAS CHECKED	



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D160W25-sht-Xsht-03
 USER NAME = pimserno
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EB I-290 / CONGRESS PARKWAY CROSS SECTION
 SCALE: 1:5V, 1:10H SHEET 31 OF 33 SHEETS STA. 5250+29.99 TO STA. 5250+33.57

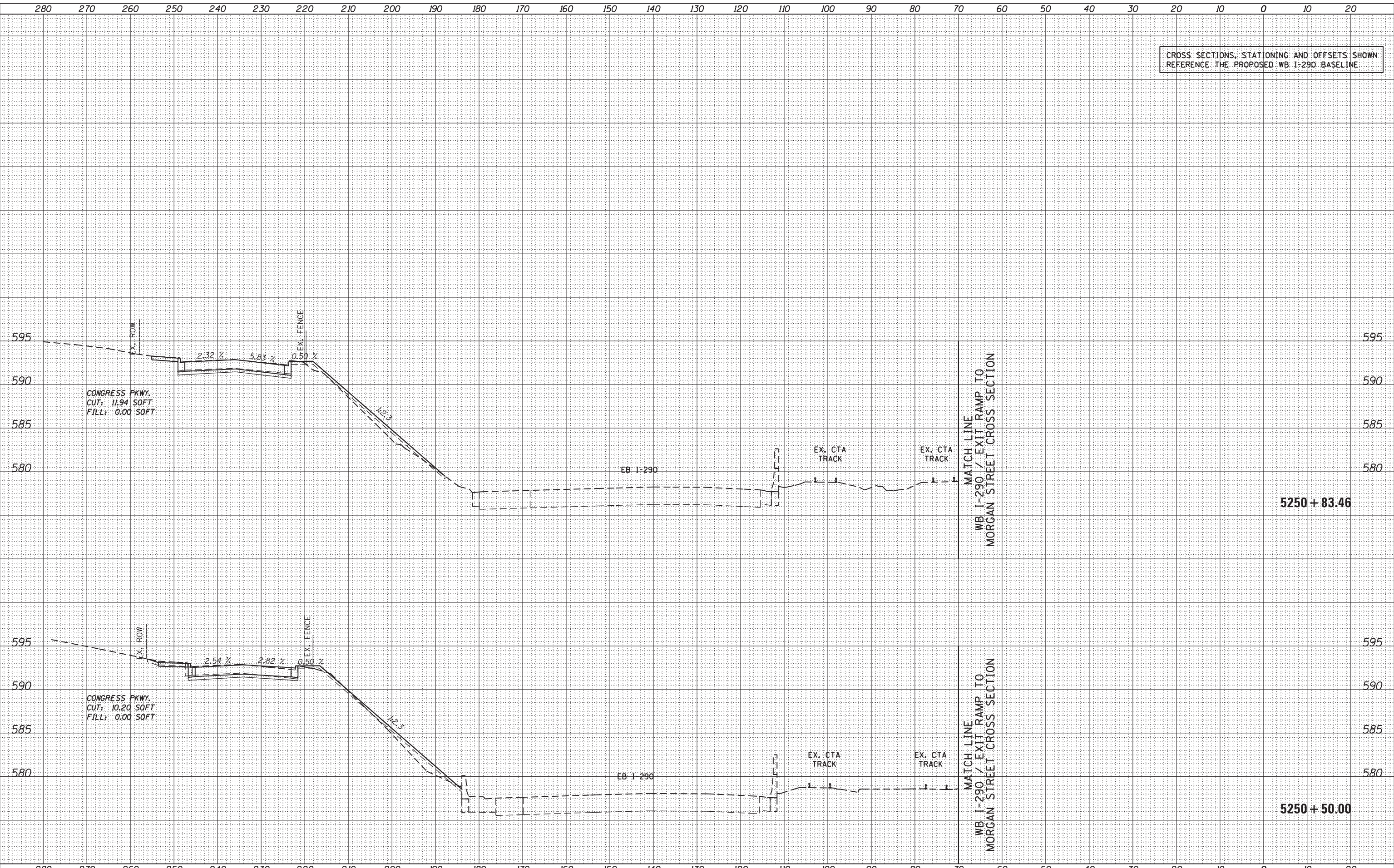
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	303
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				

CROSS SECTIONS, STATIONING AND OFFSETS SHOWN
REFERENCE THE PROPOSED WB I-290 BASELINE

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D160W25-shr-Xsht-03
 USER NAME = pimsarno
 PLOT SCALE = 10.0000' / in.
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DESIGNED - AFC / KAM
 DRAWN - AFC / KAM
 CHECKED - DBM
 DATE - 6/17/2013

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EB I-290 / CONGRESS PARKWAY CROSS SECTION

SCALE: 1:5V, 1:10H SHEET 32 OF 33 SHEETS STA. 5250+50.00 TO STA. 5250+83.46

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	304
CONTRACT NO. 60W25				

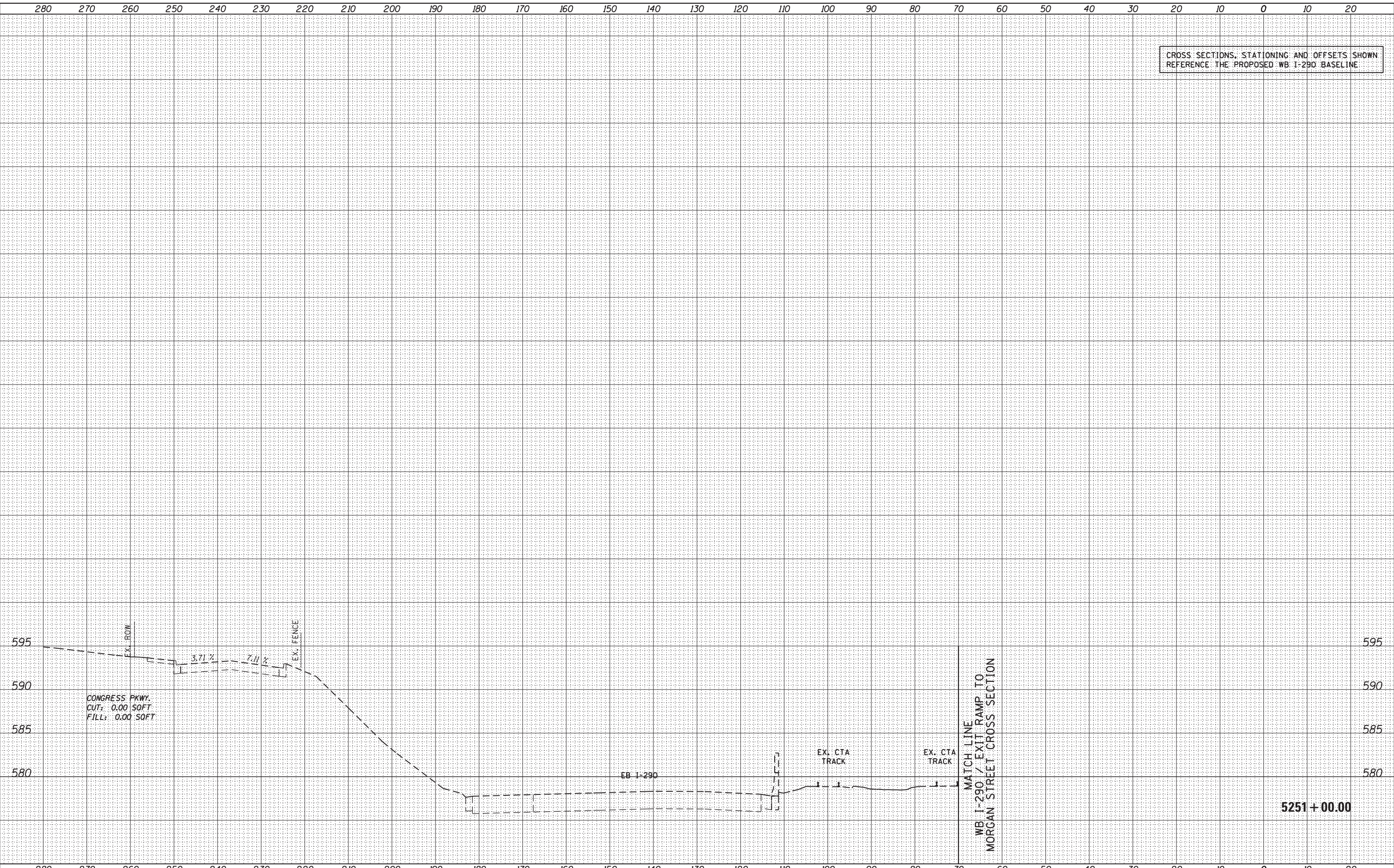
ILLINOIS FED. AID PROJECT

CROSS SECTIONS, STATIONING AND OFFSETS SHOWN
REFERENCE THE PROPOSED WB I-290 BASELINE

DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
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NOTE BOOK	
AREAS CHECKED	

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DI60W25-sht-Xsht-03
USER NAME = pimsarno
PLOT SCALE = 10.0000' / 1"
PLOT DATE = 6/14/2013

DESIGNED - AFC / KAM
DRAWN - AFC / KAM
CHECKED - DBM
DATE - 6/17/2013

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EB I-290 / CONGRESS PARKWAY CROSS SECTION

SCALE: 1:5V, 1:10H SHEET 33 OF 33 SHEETS STA. 5251+00.00 TO STA. 5251+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	305
CONTRACT NO. 60W25				

ILLINOIS FED. AID PROJECT