

KANKAKEE VALLEY AIRPORT AUTHORITY

KANKAKEE, ILLINOIS

INDEX TO SHEETS

1. COVER SHEET
2. SITE PLAN
3. SEQUENCE OF CONSTRUCTION 1
4. SEQUENCE OF CONSTRUCTION 2
5. SEQUENCE OF CONSTRUCTION NOTES
6. SEQUENCE OF CONSTRUCTION DETAILS
7. STORMWATER POLLUTION PREVENTION PLAN
8. STORMWATER POLLUTION PREVENTION PLAN DETAILS
9. FENCING PLAN 1
10. FENCING PLAN 2
11. FENCING PLAN 3
12. FENCING PLAN 4
13. FENCING DETAILS 1
14. FENCING DETAILS 2
15. CANTILEVER GATE DETAILS
16. DRAINAGE IMPROVEMENTS 1
17. DRAINAGE IMPROVEMENTS 2
18. DRAINAGE IMPROVEMENTS 3
19. DRAINAGE DETAILS

IDOT STANDARDS

701101-05 701106-02 701901-08

CONSTRUCTION PLANS FOR GREATER KANKAKEE AIRPORT

PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING

ILLINOIS PROJECT: IKK-4707
S.B.G. PROJECT: 3-17-SBGP-TBD

JUNE 7, 2019

SUMMARY OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR150520	MOBILIZATION	LS	1	
AR150540	HAUL ROUTE	LS	1	
AR151412	TREE CLEARING	ACRE	0.8	
AR151420	CLEARING TREES 0-2.5' BUTT. DIA.	EACH	13	
AR151430	CLEARING TREES 2.5'-5' BUTT DIA.	EACH	1	
AR151450	CLEARING AND GRUBBING	ACRE	2.2	
AR152410	UNCLASSIFIED EXCAVATION	CY	2,550	
AR152541	STABILIZATION GEOGRID	SY	1,350	
AR156510	SILT FENCE	LF	4,300	
AR156513	SEPARATION FABRIC	SY	1,350	
AR156530	TEMPORARY SEEDING	ACRE	3.0	
AR156531	EROSION CONTROL BLANKET	SY	3,100	
AR156543	RIPRAP - GRADATION NO. 3	SY	320	
AR156544	RIPRAP - GRADATION NO. 4	SY	100	
AR161900	REMOVE CLASS C FENCE	LF	2,250	
AR162210	CLASS E MANUAL SLIDE GATE - 10'	EACH	2	
AR162216	CLASS E MANUAL SLIDE GATE - 16'	EACH	4	
AR162220	CLASS E MANUAL SLIDE GATE - 20'	EACH	3	
AR162222	CLASS E MANUAL SLIDE GATE - 22'	EACH	1	
AR162224	CLASS E MANUAL SLIDE GATE - 24'	EACH	2	
AR162530	WALKWAY GATES, CLASS E (5')	EACH	1	
AR162810	CLASS E FENCE 10' W/ 2' BURY	LF	9,100	
AR162900	REMOVE CLASS E FENCE	LF	670	
AR162910	REMOVE CLASS E GATE	EACH	6	
AR208515	POROUS GRANULAR EMBANKMENT	CY	1,660	
AR208606	6" AGGREGATE BASE COURSE	SY	930	
AR701012	12" PVC STORM SEWER	LF	70	
AR701173	PRECAST CONC. BOX CULVERT 7'X3'	LF	40	
AR701183	PRECAST CONC. BOX CULVERT 8'X3'	LF	40	
AR701342	42" RCP, CLASS II	LF	80	
AR752501	GRATING	SF	195	
AR752600	CONCRETE HEADWALL	EACH	2	
AR754910	REMOVE PAVED DITCH	LF	150	
AR800173	WILDLIFE DETERRENT BARRIER - PAD	SF	450	
AR901510	SEEDING	ACRE	7.8	
AR908514	LIGHT-DUTY HYDRAULIC MJLCH	ACRE	7.1	

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J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS
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THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

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ANTONIO R. MARIN
062-057994
LICENSED PROFESSIONAL ENGINEER OF ILLINOIS

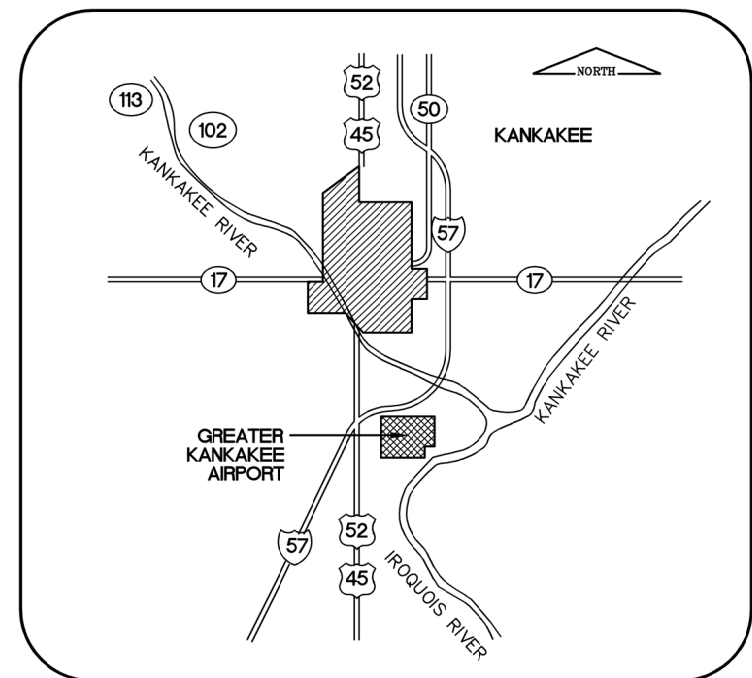
SUBMITTED BY: *Antonio R. Marin*
ANTONIO R. MARIN, P.E.

DATE: *June 14, 2019*

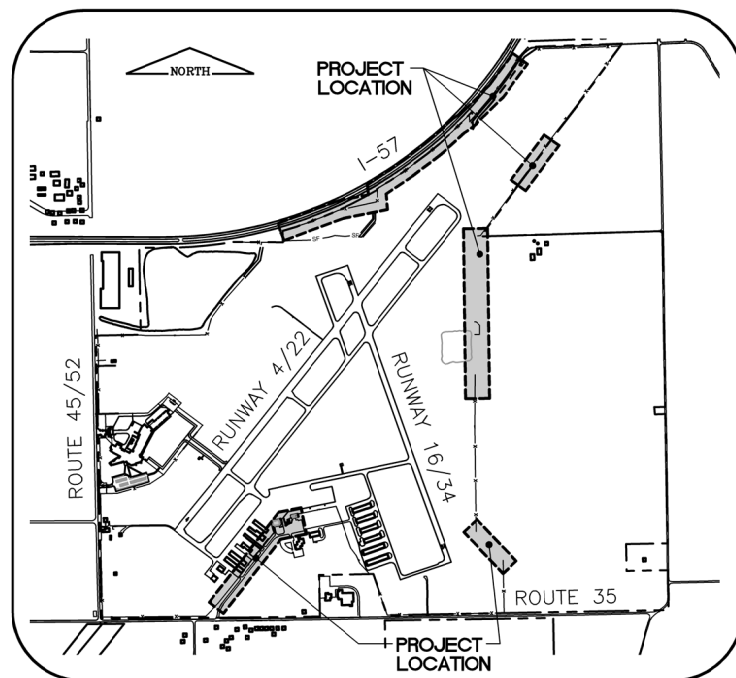
KANKAKEE VALLEY AIRPORT AUTHORITY

APPROVED: *Jeff Benoit*
JEFF BENOIT, AIRPORT MANAGER

DATE: *6-6* 20*19*



SITE PLAN



LOCATION MAP

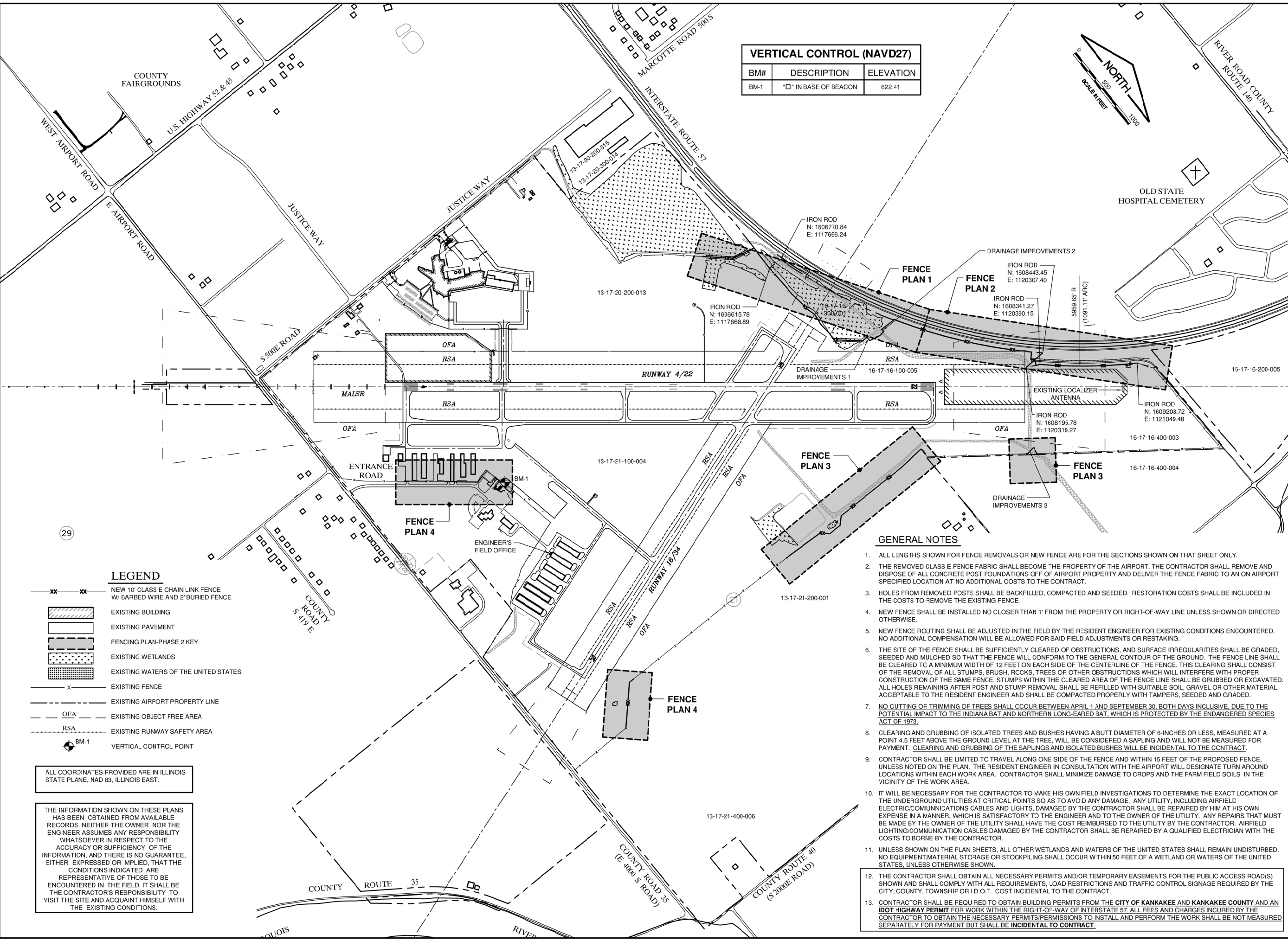
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DESIGN AIRCRAFT APPROACH CATEGORY D
DESIGN AIRCRAFT GROUP II

KANKAKEE VALLEY AIRPORT AUTHORITY
GREATER KANKAKEE AIRPORT

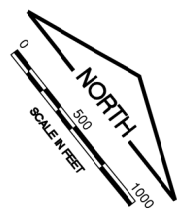
SECTION: 16, 20 AND 21 COUNTY: KANKAKEE
RANGE: 13 W TOWNSHIP: KANKAKEE AND OTTO
TOWNSHIP: 30 N

UNICOM RADIO FREQUENCY - 123.0

DATE: Tuesday, June 18, 2019 11:56:10 AM
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 UPDATE BY: Jim Ohse
 LAYOUT: Layout1
 IMAGE FILES: Logo.jpg
 REF: DING: k\Fencing Base.dwg



VERTICAL CONTROL (NAVD27)		
BM#	DESCRIPTION	ELEVATION
BM-1	"O" IN BASE OF BEACON	622.41



IL CONTRACT: KA049		
IL LETTING ITEM: 02A		
IL PROJECT: IKK-4707		
S.B.G. PROJECT: 3-17-SBGP-TBD		
SURVEY BOOK #	BOOK #	
REVISIONS		
NUMBER	BY	DATE
0 1 2 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).		

LEGEND

	NEW 10' CLASS E CHAIN LINK FENCE W/ BARBED WIRE AND 2' BURIED FENCE
	EXISTING BUILDING
	EXISTING PAVEMENT
	FENCING PLAN-PHASE 2 KEY
	EXISTING WETLANDS
	EXISTING WATERS OF THE UNITED STATES
	EXISTING FENCE
	EXISTING AIRPORT PROPERTY LINE
	EXISTING OBJECT FREE AREA
	EXISTING RUNWAY SAFETY AREA
	VERTICAL CONTROL POINT

ALL COORDINATES PROVIDED ARE IN ILLINOIS STATE PLANE, NAD 83, ILLINOIS EAST.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION, AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

GENERAL NOTES

- ALL LENGTHS SHOWN FOR FENCE REMOVALS OR NEW FENCE ARE FOR THE SECTIONS SHOWN ON THAT SHEET ONLY.
- THE REMOVED CLASS E FENCE FABRIC SHALL BECOME THE PROPERTY OF THE AIRPORT. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL CONCRETE POST FOUNDATIONS OFF OF AIRPORT PROPERTY AND DELIVER THE FENCE FABRIC TO AN ON AIRPORT SPECIFIED LOCATION AT NO ADDITIONAL COSTS TO THE CONTRACT.
- HOLES FROM REMOVED POSTS SHALL BE BACKFILLED, COMPACTED AND SEEDED. RESTORATION COSTS SHALL BE INCLUDED IN THE COSTS TO REMOVE THE EXISTING FENCE.
- NEW FENCE SHALL BE INSTALLED NO CLOSER THAN 1' FROM THE PROPERTY OR RIGHT-OF-WAY LINE UNLESS SHOWN OR DIRECTED OTHERWISE.
- NEW FENCE ROUTING SHALL BE ADJUSTED IN THE FIELD BY THE RESIDENT ENGINEER FOR EXISTING CONDITIONS ENCOUNTERED. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR SAID FIELD ADJUSTMENTS OR RESTAKING.
- THE SITE OF THE FENCE SHALL BE SUFFICIENTLY CLEARED OF OBSTRUCTIONS, AND SURFACE IRREGULARITIES SHALL BE GRADED, SEEDED AND MULCHED SO THAT THE FENCE WILL CONFORM TO THE GENERAL CONTOUR OF THE GROUND. THE FENCE LINE SHALL BE CLEARED TO A MINIMUM WIDTH OF 12 FEET ON EACH SIDE OF THE CENTERLINE OF THE FENCE. THIS CLEARING SHALL CONSIST OF THE REMOVAL OF ALL STUMPS, BRUSH, ROCKS, TREES OR OTHER OBSTRUCTIONS WHICH WILL INTERFERE WITH PROPER CONSTRUCTION OF THE SAME FENCE. STUMPS WITHIN THE CLEARED AREA OF THE FENCE LINE SHALL BE GRUBBED OR EXCAVATED. ALL HOLES REMAINING AFTER POST AND STUMP REMOVAL SHALL BE REFILLED WITH SUITABLE SOIL, GRAVEL OR OTHER MATERIAL ACCEPTABLE TO THE RESIDENT ENGINEER AND SHALL BE COMPACTED PROPERLY WITH TAMPERS, SEEDED AND GRADED.
- NO CUTTING OR TRIMMING OF TREES SHALL OCCUR BETWEEN APRIL 1 AND SEPTEMBER 30, BOTH DAYS INCLUSIVE, DUE TO THE POTENTIAL IMPACT TO THE INDIANA BAT AND NORTHERN LONG-EARED BAT, WHICH IS PROTECTED BY THE ENDANGERED SPECIES ACT OF 1973.
- CLEARING AND GRUBBING OF ISOLATED TREES AND BUSHES HAVING A BUTT DIAMETER OF 6-INCHES OR LESS, MEASURED AT A POINT 4.5 FEET ABOVE THE GROUND LEVEL AT THE TREE, WILL BE CONSIDERED A SAPLING AND WILL NOT BE MEASURED FOR PAYMENT. CLEARING AND GRUBBING OF THE SAPLINGS AND ISOLATED BUSHES WILL BE INCIDENTAL TO THE CONTRACT.
- CONTRACTOR SHALL BE LIMITED TO TRAVEL ALONG ONE SIDE OF THE FENCE AND WITHIN 15 FEET OF THE PROPOSED FENCE, UNLESS NOTED ON THE PLAN. THE RESIDENT ENGINEER IN CONSULTATION WITH THE AIRPORT WILL DESIGNATE TURN AROUND LOCATIONS WITHIN EACH WORK AREA. CONTRACTOR SHALL MINIMIZE DAMAGE TO CROPS AND THE FARM FIELD SOILS IN THE VICINITY OF THE WORK AREA.
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATIONS TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRIC/COMMUNICATIONS CABLES AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER, WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING/COMMUNICATION CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BORNE BY THE CONTRACTOR.
- UNLESS SHOWN ON THE PLAN SHEETS, ALL OTHER WETLANDS AND WATERS OF THE UNITED STATES SHALL REMAIN UNDISTURBED. NO EQUIPMENT/MATERIAL STORAGE OR STOCKPILING SHALL OCCUR WITHIN 50 FEET OF A WETLAND OR WATERS OF THE UNITED STATES, UNLESS OTHERWISE SHOWN.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND/OR TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS AND TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP OR I.D.O.T. COST INCIDENTAL TO THE CONTRACT.
- CONTRACTOR SHALL BE REQUIRED TO OBTAIN BUILDING PERMITS FROM THE CITY OF KANKAKEE AND KANKAKEE COUNTY AND AN IDOT HIGHWAY PERMIT FOR WORK WITHIN THE RIGHT-OF-WAY OF INTERSTATE 57. ALL FEES AND CHARGES INCURRED BY THE CONTRACTOR TO OBTAIN THE NECESSARY PERMITS/PERMISSIONS TO INSTALL AND PERFORM THE WORK SHALL BE NOT MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE INCIDENTAL TO CONTRACT.

KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING

SITE PLAN

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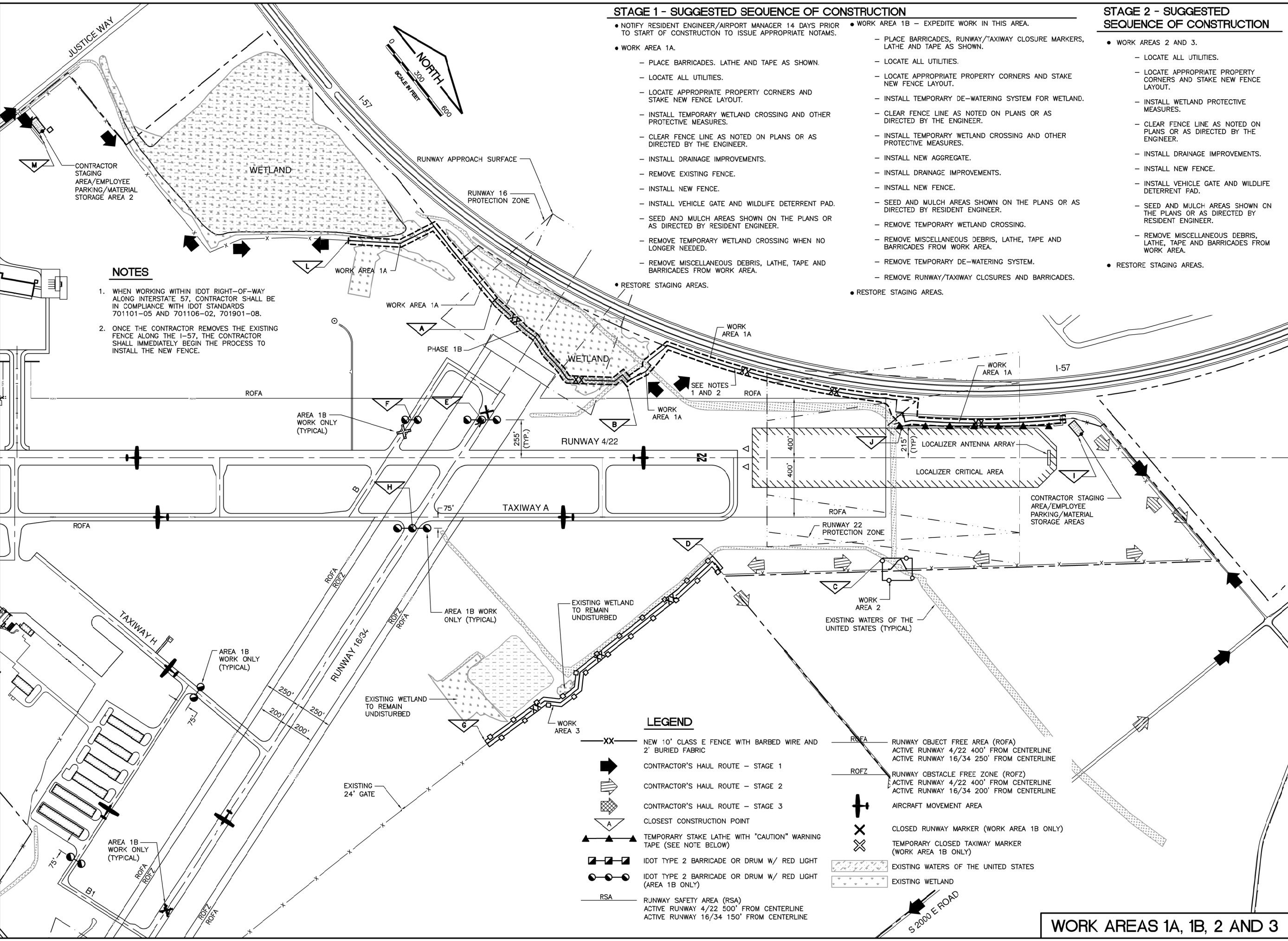
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DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	ARM
DATE:	6/7/19
JOB No:	160075-02

FINAL

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 IMAGE FILES: Logo.jpg
 REF DWG: 16075-02.dwg
 9/27/2015 CONTINUED



NOTES

1. WHEN WORKING WITHIN IDOT RIGHT-OF-WAY ALONG INTERSTATE 57, CONTRACTOR SHALL BE IN COMPLIANCE WITH IDOT STANDARDS 701101-05 AND 701106-02, 701901-08.
2. ONCE THE CONTRACTOR REMOVES THE EXISTING FENCE ALONG THE I-57, THE CONTRACTOR SHALL IMMEDIATELY BEGIN THE PROCESS TO INSTALL THE NEW FENCE.

STAGE 1 - SUGGESTED SEQUENCE OF CONSTRUCTION

- NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER 14 DAYS PRIOR TO START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
- WORK AREA 1A.
 - PLACE BARRICADES. LATHE AND TAPE AS SHOWN.
 - LOCATE ALL UTILITIES.
 - LOCATE APPROPRIATE PROPERTY CORNERS AND STAKE NEW FENCE LAYOUT.
 - INSTALL TEMPORARY WETLAND CROSSING AND OTHER PROTECTIVE MEASURES.
 - CLEAR FENCE LINE AS NOTED ON PLANS OR AS DIRECTED BY THE ENGINEER.
 - INSTALL DRAINAGE IMPROVEMENTS.
 - REMOVE EXISTING FENCE.
 - INSTALL NEW FENCE.
 - INSTALL VEHICLE GATE AND WILDLIFE DETERRENT PAD.
 - SEED AND MULCH AREAS SHOWN ON THE PLANS OR AS DIRECTED BY RESIDENT ENGINEER.
 - REMOVE TEMPORARY WETLAND CROSSING WHEN NO LONGER NEEDED.
 - REMOVE MISCELLANEOUS DEBRIS, LATHE, TAPE AND BARRICADES FROM WORK AREA.
- RESTORE STAGING AREAS.

STAGE 2 - SUGGESTED SEQUENCE OF CONSTRUCTION

- WORK AREA 1B - EXPEDITE WORK IN THIS AREA.
 - PLACE BARRICADES, RUNWAY/TAXIWAY CLOSURE MARKERS, LATHE AND TAPE AS SHOWN.
 - LOCATE ALL UTILITIES.
 - LOCATE APPROPRIATE PROPERTY CORNERS AND STAKE NEW FENCE LAYOUT.
 - INSTALL TEMPORARY DE-WATERING SYSTEM FOR WETLAND.
 - CLEAR FENCE LINE AS NOTED ON PLANS OR AS DIRECTED BY THE ENGINEER.
 - INSTALL TEMPORARY WETLAND CROSSING AND OTHER PROTECTIVE MEASURES.
 - INSTALL NEW AGGREGATE.
 - INSTALL DRAINAGE IMPROVEMENTS.
 - INSTALL NEW FENCE.
 - SEED AND MULCH AREAS SHOWN ON THE PLANS OR AS DIRECTED BY RESIDENT ENGINEER.
 - REMOVE TEMPORARY WETLAND CROSSING.
 - REMOVE MISCELLANEOUS DEBRIS, LATHE, TAPE AND BARRICADES FROM WORK AREA.
 - REMOVE TEMPORARY DE-WATERING SYSTEM.
 - REMOVE RUNWAY/TAXIWAY CLOSURES AND BARRICADES.
- RESTORE STAGING AREAS.

STAGE 2 - SUGGESTED SEQUENCE OF CONSTRUCTION

- WORK AREAS 2 AND 3.
 - LOCATE ALL UTILITIES.
 - LOCATE APPROPRIATE PROPERTY CORNERS AND STAKE NEW FENCE LAYOUT.
 - INSTALL WETLAND PROTECTIVE MEASURES.
 - CLEAR FENCE LINE AS NOTED ON PLANS OR AS DIRECTED BY THE ENGINEER.
 - INSTALL DRAINAGE IMPROVEMENTS.
 - INSTALL NEW FENCE.
 - INSTALL VEHICLE GATE AND WILDLIFE DETERRENT PAD.
 - SEED AND MULCH AREAS SHOWN ON THE PLANS OR AS DIRECTED BY RESIDENT ENGINEER.
 - REMOVE MISCELLANEOUS DEBRIS, LATHE, TAPE AND BARRICADES FROM WORK AREA.
- RESTORE STAGING AREAS.

LEGEND

- XX NEW 10' CLASS E FENCE WITH BARBED WIRE AND 2' BURIED FABRIC
- ➔ CONTRACTOR'S HAUL ROUTE - STAGE 1
- ➔ CONTRACTOR'S HAUL ROUTE - STAGE 2
- ➔ CONTRACTOR'S HAUL ROUTE - STAGE 3
- A CLOSEST CONSTRUCTION POINT
- ▲▲▲ TEMPORARY STAKE LATHE WITH "CAUTION" WARNING TAPE (SEE NOTE BELOW)
- IDOT TYPE 2 BARRICADE OR DRUM W/ RED LIGHT
- IDOT TYPE 2 BARRICADE OR DRUM W/ RED LIGHT (AREA 1B ONLY)
- RSA RUNWAY SAFETY AREA (RSA)
ACTIVE RUNWAY 4/22 500' FROM CENTERLINE
ACTIVE RUNWAY 16/34 150' FROM CENTERLINE
- ROFA RUNWAY OBJECT FREE AREA (ROFA)
ACTIVE RUNWAY 4/22 400' FROM CENTERLINE
ACTIVE RUNWAY 16/34 250' FROM CENTERLINE
- ROFZ RUNWAY OBSTACLE FREE ZONE (ROFZ)
ACTIVE RUNWAY 4/22 400' FROM CENTERLINE
ACTIVE RUNWAY 16/34 200' FROM CENTERLINE
- ✈ AIRCRAFT MOVEMENT AREA
- ✕ CLOSED RUNWAY MARKER (WORK AREA 1B ONLY)
- ✕ TEMPORARY CLOSED TAXIWAY MARKER (WORK AREA 1B ONLY)
- ▨ EXISTING WATERS OF THE UNITED STATES
- ▨ EXISTING WETLAND

IL. CONTRACT: **KA049**
 IL. LETTING ITEM: **02A**
 IL. PROJECT: **IKK-4707**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).

KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING
SEQUENCE OF CONSTRUCTION 1

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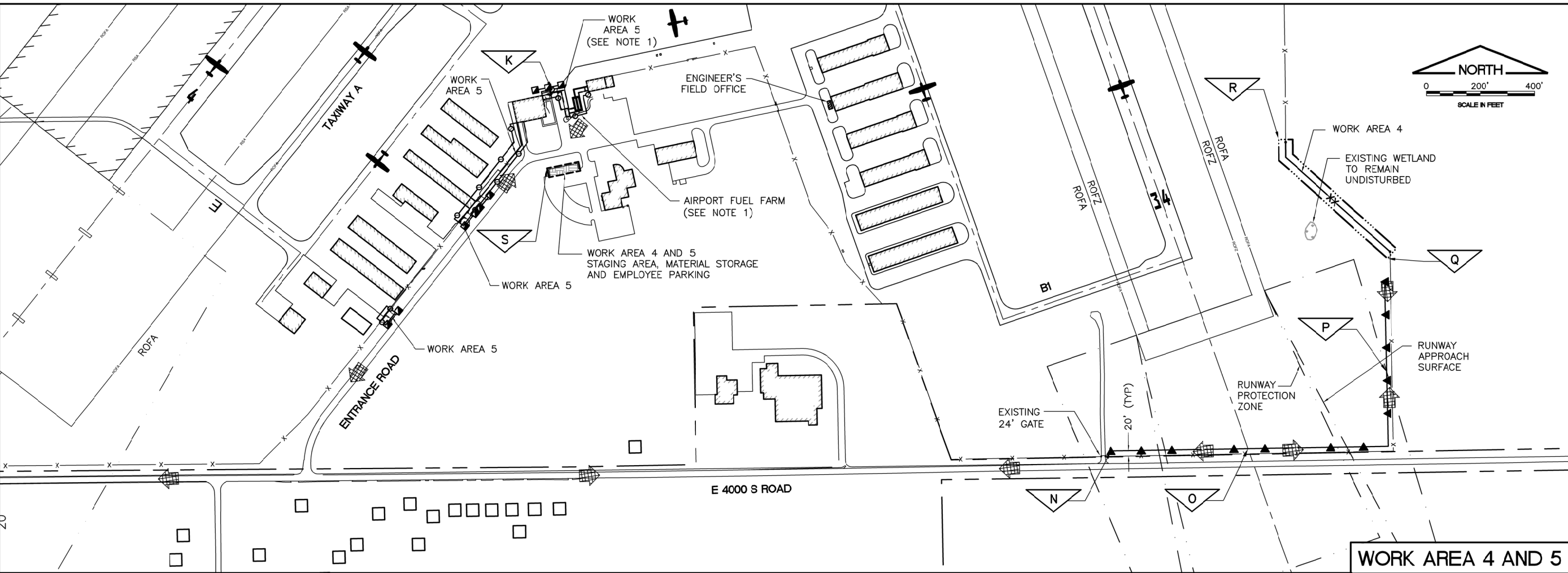
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DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	ARM
DATE:	6/7/19
JOB No:	160075-02

WORK AREAS 1A, 1B, 2 AND 3

DATE: Tuesday, June 18, 2019 11:57:50 AM
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 KKK-11-11-11



I.L. CONTRACT: **KA049**
 I.L. LETTING ITEM: **02A**
 I.L. PROJECT: **IKK-4707**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # **BOOK #**

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING
SEQUENCE OF CONSTRUCTION 2

LEGEND

- NEW 10' CLASS E FENCE WITH BARBED WIRE AND 2' BURIED FABRIC
- CONTRACTOR'S HAUL ROUTE - STAGE 1
- CONTRACTOR'S HAUL ROUTE - STAGE 2
- CONTRACTOR'S HAUL ROUTE - STAGE 3
- CLOSEST CONSTRUCTION POINT
- TEMPORARY STAKE LATHE WITH "CAUTION" WARNING TAPE (SEE NOTE BELOW)
- IDOT TYPE 2 BARRICADE OR DRUM W/ RED LIGHT
- IDOT TYPE 2 BARRICADE OR DRUM W/ RED LIGHT (AREA 1B ONLY)
- RSA - RUNWAY SAFETY AREA (RSA)
ACTIVE RUNWAY 4/22 500' FROM CENTERLINE
ACTIVE RUNWAY 16/34 150' FROM CENTERLINE
- ROFA - RUNWAY OBJECT FREE AREA (ROFA)
ACTIVE RUNWAY 4/22 400' FROM CENTERLINE
ACTIVE RUNWAY 16/34 250' FROM CENTERLINE
- ROFZ - RUNWAY OBSTACLE FREE ZONE (ROFZ)
ACTIVE RUNWAY 4/22 400' FROM CENTERLINE
ACTIVE RUNWAY 16/34 200' FROM CENTERLINE
- AIRCRAFT MOVEMENT AREA
- CLOSED RUNWAY MARKER (WORK AREA 1B ONLY)
- TEMPORARY CLOSED TAXIWAY MARKER (WORK AREA 1B ONLY)
- EXISTING WATERS OF THE UNITED STATES
- EXISTING WETLAND

STAGE	WORK AREA	ALLOWABLE WORK PERIODS	AIRFIELD OPERATIONAL STATUS/RESTRICTION
1	1A	TREE REMOVAL TO OCCUR BETWEEN OCTOBER 1 THRU MARCH 31; ALL OTHER WORK NO RESTRICTIONS	ALL RUNWAYS/TAXIWAYS OPEN
	1B	TREE REMOVAL TO OCCUR BETWEEN OCTOBER 1 THRU MARCH 31; ALL OTHER WORK NO RESTRICTIONS; EXPEDITE ALL WORK THIS AREA CONTRACTOR ALLOWED TO USE AREA 1B AS HAUL ROUTE BETWEEN WORK AREA 1A SECTIONS, WHEN IN RADIO CONTACT WITH ACTIVE AIRFIELD. SEE NOTE 32 ON SEQUENCE OF CONSTRUCTION NOTES SHEET. NO AIRFIELD BARRICADES/RUNWAY CLOSED MARKERS ARE REQUIRED TO BE PLACED.	RUNWAY 16/34 CLOSED RUNWAY 4/22 OPEN TAXIWAY B1 CLOSED PARTIAL CLOSURE OF TAXIWAYS H AND B ALL OTHER TAXIWAYS OPEN
2	2	NO RESTRICTIONS	ALL RUNWAYS/TAXIWAYS OPEN
	3	NO RESTRICTIONS	ALL RUNWAYS/TAXIWAYS OPEN
3	4	NO RESTRICTIONS	ALL RUNWAYS/TAXIWAYS OPEN
	5	NO RESTRICTIONS	ALL RUNWAYS/TAXIWAYS OPEN

STAGE 3 - SUGGESTED SEQUENCE OF CONSTRUCTION

- NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER 14 DAYS PRIOR TO START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
- WORK AREAS 4 AND 5.
 - PLACE BARRICADES, LATHE AND TAPE AS SHOWN.
 - LOCATE ALL UTILITIES.
 - LOCATE APPROPRIATE PROPERTY CORNERS AND STAKE NEW FENCE LAYOUT.
 - CLEAR FENCE LINE AS NOTED ON PLANS OR AS DIRECTED BY THE ENGINEER.
 - REMOVE EXISTING FENCE/GATES (SEE NOTES 2 AND 3).
 - INSTALL NEW FENCE.
 - INSTALL VEHICLE DRIVE GATES (SEE NOTES 2 AND 3).
 - SEED AND MULCH AREAS SHOWN ON THE PLANS.
 - REMOVE MISCELLANEOUS DEBRIS, LATHE, TAPE AND BARRICADES FROM WORK AREA.
- RESTORE STAGING AREAS.

FENCE/GATE REMOVAL NOTES

1. FOR WORK AREA 5, THE CONTRACTOR SHALL LEAVE THE EXISTING FENCE IN PLACE OR INSTALL TEMPORARY 4' POLYETHYLENE FENCE IF THE CONTRACTOR ELECTS TO REMOVE THE EXISTING FENCE BEFORE THE NEW FENCE IS INSTALLED. IF THE CONTRACTOR ELECTS TO LEAVE THE EXISTING FENCE IN PLACE THE LOCATION OF THE NEW FENCE SHALL BE ADJUSTED AS APPROVED BY THE OWNER. ONCE THE NEW FENCE IS INSTALLED THE EXISTING FENCE OR TEMPORARY FENCE SHALL BE REMOVED. THE COST OF THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCIDENTAL TO THE CONTRACT UNLESS THE PROJECT SPECIFICATIONS INCLUDE PAYMENT METHOD.
2. AT LOCATIONS WHERE GATES ARE TO BE REPLACED, THE CONTRACTOR SHALL PROVIDE TEMPORARY BARRIER MEASURES AS DIRECTED BY THE AIRPORT MANAGER OR RESIDENT ENGINEER. UNTIL NEW GATE IS INSTALLED. CONTRACTOR SHALL IMMEDIATELY INSTALL NEW GATE. TEMPORARY BARRIER MEASURES MAY CONSIST OF BARRICADES, CABLING, TEMPORARY GATE, ETC. TO PREVENT UNAUTHORIZED ACCESS TO THE AIRPORT PROPERTY AS APPROVED BY THE AIRPORT MANAGER. COST INCIDENTAL TO THE CONTRACT.
3. IN ORDER TO MAINTAIN ACCESS TO MAIN RAMP AND WEST HANGARS, THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN ONE CHAIN DRIVEN GATE IN OPERATION AT ALL TIMES. COST INCIDENTAL TO THE CONTRACT.

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DESIGN BY: **ARM**
 DRAWN BY: **JRO**
 CHECKED BY: **DKP**
 APPROVED BY: **ARM**
 DATE: **6/7/19**
 JOB No: **160075-02**

FINAL

SHEET 4 OF 19 SHEETS

DATE: Tuesday, June 18, 2019 11:58:58 AM
FILE: K:\Kankakee\16075-02_Fencing\16075-02_Fencing\16075-02_Sheet5.dwg
UPDATE BY: Jim Ohse
LAYOUT: 05_Seg_Notes
XREF DWG: 16075-02.dwg
IMAGE FILES: Logo.jpg

GENERAL NOTES

1. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
2. ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2 (LATEST EDITION) **OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION**.
3. CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
4. THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
5. ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
6. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER.
7. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
8. THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
9. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
11. PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES WITH ONE ORANGE FLAG (20" x 20") ON EACH BARRICADE SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A STEADY BURN OR FLASHING RED LIGHT. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL SUPPLY AND USE AS DIRECTED BY THE AIRPORT, REFLECTIVE LOW PROFILE TYPE BARRICADES. ALL BARRICADES SHALL BE PLACED OUTSIDE OF ACTIVE SAFETY AREAS.
12. THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER FOURTEEN (14) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
14. CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
15. IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
16. DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
17. THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE A READY MIX CONCRETE TRUCK, WHICH HAS A MAXIMUM HEIGHT OF 15 FEET.
18. IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
19. THE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
20. APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
21. MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.

BARRICADE NOTES

1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OR SOLAR POWER OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 10' SPACING CENTER TO CENTER BETWEEN RED LIGHTS ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.

DESIGN AIRCRAFT APPROACH CATEGORY: B AND D

DESIGN AIRPORT GROUP: II
RUNWAY 4/22 SAFETY AREA WIDTH: 500'
RUNWAY 16/34 SAFETY AREA WIDTH: 150'
RUNWAY 4/22 OBJECT FREE AREA = 800'
RUNWAY 16/34 OBJECT FREE AREA = 500'
TAXIWAY SAFETY AREA = 79'
TAXIWAY OBJECT FREE AREA = 131'
TAXIWAY CENTERLINE TO OBJECT SEPARATION: 65.5'
TAXILANE CENTERLINE TO OBJECT SEPARATION: 57.5'

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND ACCESS TO THE SITE DUE TO ADJACENT CONSTRUCTION.

- PHASE 1B - PERIMETER SAFETY/WILDLIFE FENCE.
- REHABILITATE SW QUADRANT TAXIWAY AND AIRFIELD PAVEMENT REPAIRS.

MAXIMUM ANTICIPATED HEIGHT OF EQUIPMENT - 40'

IN THE EVENT THE CONTRACTOR PROPOSES TO UTILIZE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN WHAT IS LISTED, THE CONTRACTOR WILL BE RESPONSIBLE TO SUBMIT FAA FORM 7460 FOR AIRSPACE APPROVAL. THE RESIDENT ENGINEER WILL PROVIDE BASE AIRPORT INFORMATION FOR THE CONTRACTOR'S USE.

CLOSEST CONSTRUCTION POINT TABLE

POINT	NEAREST ACTIVE RUNWAY	LATITUDE	LONGITUDE	ANTICIPATED EQUIPMENT HEIGHT	SITE ELEVATION
A	RUNWAY 4/22	N41°04'36.80"	W87°50'53.24"	15'	619.2
B	RUNWAY 4/22	N41°04'40.38"	W87°50'40.06"	40'	619.6
C	RUNWAY 4/22	N41°04'46.64"	W87°50'14.29"	40'	620.9
D	RUNWAY 4/22	N41°04'37.88"	W87°50'23.70"	15'	622.0
E	RUNWAY 4/22	N41°04'31.44"	W87°50'45.69"	15'	621.9
F	RUNWAY 4/22	N41°04'27.75"	W87°50'49.54"	15'	621.6
G	RUNWAY 4/22	N41°04'18.35"	W87°50'23.38"	15'	619.0
H	RUNWAY 4/22	N41°04'23.33"	W87°50'42.02"	15'	618.4
I	RUNWAY 4/22	N41°05'01.86"	W87°50'12.03"	15'	617.2
J	RUNWAY 4/22	N41°04'53.10"	W87°50'22.34"	15'	614.7
K	RUNWAY 4/22	N41°03'58.84"	W87°50'58.35"	15'	622.9
L	RUNWAY 4/22	N41°04'31.71"	W87°51'05.11"	15'	615.0
M	RUNWAY 4/22	N41°04'20.16"	W87°51'29.60"	15'	617.5
N	RUNWAY 16/34	N41°03'45.55"	W87°50'31.54"	15'	616.0
O	RUNWAY 16/34	N41°03'45.60"	W87°50'24.93"	15'	612.5
P	RUNWAY 16/34	N41°03'48.38"	W87°50'18.25"	15'	611.0
Q	RUNWAY 16/34	N41°03'52.50"	W87°50'18.18"	15'	612.0
R	RUNWAY 16/34	N41°03'56.87"	W87°50'23.36"	15'	612.1
S	RUNWAY 4/22	N41°03'55.97"	W87°50'58.59"	15'	622.0

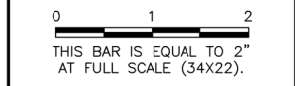
CONTRACTOR CROSSING RUNWAY/TAXIWAY/TAXILANE/APRON AIR OPERATIONS AREA (A.O.A.)

32. ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS OR RUNWAY APPROACH FOR ACCESS TO AND FROM THE WORK ZONE, A CROSSING GUARD OR ESCORT IN RADIO CONTACT WITH AIR TRAFFIC SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURRENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
33. ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
34. WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE MEN AND EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

IL. CONTRACT: **KA049**
IL. LETTING ITEM: **02A**
IL. PROJECT: **IKK-4707**
S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

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NUMBER	BY	DATE



KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING
SEQUENCE OF CONSTRUCTION NOTES

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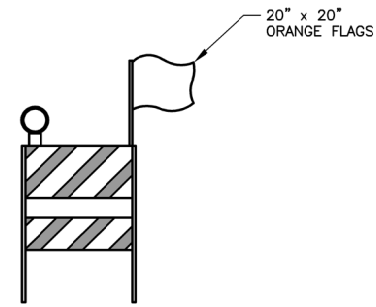
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APPROVED BY: ARM
DATE: 6/7/19
JOB No: 160075-02

FINAL
SHEET 5 OF 19 SHEETS

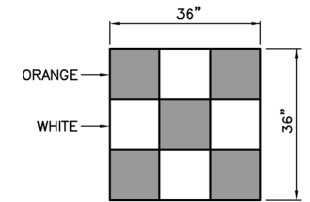
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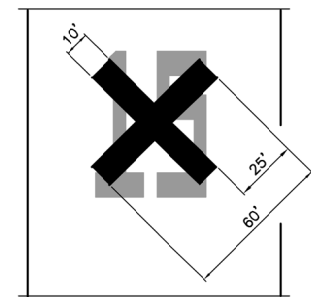
FLASHER BARRICADE DETAIL
NOT TO SCALE

NOTES

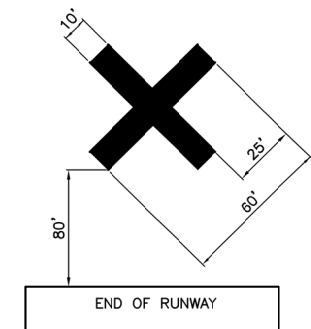
1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES. ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT TEN (10') INTERVALS.
5. ONE 20"x20" ORANGE FLAG TO BE INSTALLED ON EACH BARRICADE.
6. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING BARRICADES AT ALL TIMES TO THE SATISFACTION OF THE AIRPORT MANAGER.



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
NOT TO SCALE



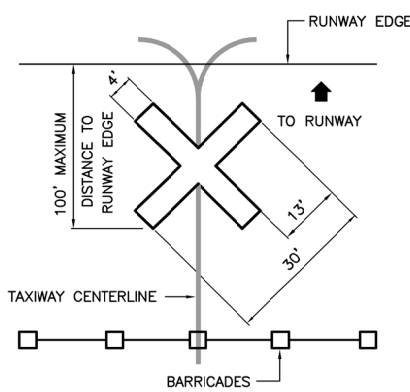
ON PAVEMENT TEMPORARY CLOSED RUNWAY MARKER DETAIL
NOT TO SCALE



OFF PAVEMENT TEMPORARY CLOSED RUNWAY MARKER DETAIL
NOT TO SCALE

CLOSED RUNWAY MARKER DETAIL NOTES

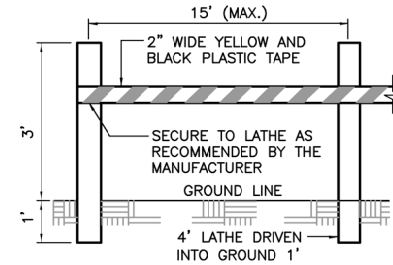
1. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
2. MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
4. MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
5. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.



CLOSED TAXIWAY MARKER DETAIL
NOT TO SCALE

CLOSED TAXIWAY MARKER DETAIL NOTES

1. CLOSED TAXIWAY MARKERS SHALL BE PAINTED WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OBJECT FREE AREA CRITERIA AND RUNWAY SAFETY AREA CRITERIA PER ADVISORY CIRCULAR 150/5300-13A (LATEST EDITION) AND ARE APPROVED BY THE AIRPORT.
2. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
3. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. PLACE MARKERS OVER TAXIWAY CENTERLINE.
5. MARKERS SHALL BE ADEQUATELY SECURED TO PREVENT MOVEMENT BY PROPELLER WASH, JET BLAST OR OTHER WIND CURRENTS.
6. MARKERS ARE ONLY REQUIRED FOR TAXIWAYS CLOSED THREE (3) CONSECUTIVE DAYS OR MORE.



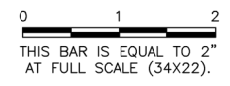
LATHING AND WARNING TAPE DETAIL
NOT TO SCALE

MATERIALS ARE TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION. COST OF MATERIALS, INSTALLATION, RELOCATION AND MAINTENANCE OF LATHING AND WARNING TAPE SHALL BE INCIDENTAL TO THE CONTRACT.

IL. CONTRACT: **KA049**
 IL. LETTING ITEM: **02A**
 IL. PROJECT: **IKK-4707**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING

SEQUENCE OF CONSTRUCTION DETAILS

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APPROVED BY:	ARM
DATE:	6/7/19
JOB No:	160075-02

FINAL

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STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTING A NEW 10' SECURITY FENCE WITH A 2' BURIED WILDLIFE FENCING, DRAINAGE IMPROVEMENTS AND AGGREGATE PATH AT THE GREATER KANKAKEE AIRPORT.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

1. INSTALL AND MAINTAIN TEMPORARY EROSION CONTROL MEASURES.
2. COMPLETE CLEARING AND GRUBBING.
3. INSTALL DRAINAGE IMPROVEMENTS.
4. REMOVE EXISTING FENCE.
5. INSTALL THE NEW FENCE POSTS AND DETERRENT FENCE EXCAVATION.
6. INSTALL NEW FENCE IMPROVEMENTS.
7. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
8. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND BLANKET.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 8.5 ACRES OF WHICH 7.8 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE UNNAMED TRIBUTARIES THAT OUTLET INTO THE KANKAKEE RIVER.

SEDIMENTATION AND EROSION CONTROL NOTES

THE SOIL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE INSPECTED WEEKLY AND AFTER 1/2 INCH OF RAIN OR MORE BY THE RESIDENT ENGINEER.

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL, UNLESS OTHERWISE STATED.

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
 - A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
 - B. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

5. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.
6. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

CONTRACTORS

1. THE STORM WATER POLLUTION PREVENTION PLAN MUST CLEARLY IDENTIFY FOR EACH MEASURE IDENTIFIED IN THE PLAN, THE CONTRACTOR(S) OR SUBCONTRACTOR(S) THAT WILL IMPLEMENT THE MEASURE. ALL CONTRACTORS AND SUBCONTRACTORS IDENTIFIED IN THE PLAN MUST SIGN A COPY OF THE CERTIFICATION STATEMENT IN PARAGRAPH 2 BELOW IN ACCORDANCE WITH PART VI.G (SIGNATORY REQUIREMENTS) OF THIS PERMIT. ALL CERTIFICATIONS MUST BE INCLUDED IN THE STORM WATER POLLUTION PREVENTION PLAN EXCEPT FOR OWNERS THAT ARE ACTING AS CONTRACTOR.
2. CERTIFICATION STATEMENT. ALL CONTRACTORS AND SUBCONTRACTORS IDENTIFIED IN A STORM WATER POLLUTION PREVENTION PLAN IN ACCORDANCE WITH PARAGRAPH 1 ABOVE SHALL SIGN A COPY OF THE FOLLOWING CERTIFICATION STATEMENT BEFORE CONDUCTING ANY PROFESSIONAL SERVICE AT THE SITE IDENTIFIED IN THE STORM WATER POLLUTION PREVENTION PLAN:

"I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION."

THE CERTIFICATION MUST INCLUDE THE NAME AND TITLE OF THE PERSON PROVIDING THE SIGNATURE IN ACCORDANCE WITH PART VI.G OF THIS PERMIT; THE NAME, ADDRESS AND TELEPHONE NUMBER OF THE CONTRACTING FIRM; THE ADDRESS (OR OTHER IDENTIFYING DESCRIPTION) OF THE SITE; AND THE DATE THE CERTIFICATION IS MADE.

CONTRACTOR CERTIFICATION STATEMENT

THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PROJECT INFORMATION:

ROUTE: GREATER KANKAKEE AIRPORT MARKED: PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING
 SECTION: 16, 20 & 21 PROJECT NUMBER: IKK-4707
 COUNTY: KANKAKEE CONTRACT NUMBER: 3-17-SBGP-TBD

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SIGNATURE: _____ DATE: _____

PRINTED NAME: _____ TITLE: _____

NAME OF FIRM: _____

STREET ADDRESS: _____

CITY, STATE, ZIP: _____

PHONE NUMBER: _____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.

ADDITIONAL NOTES

1. PRIOR TO COMMENCING DEWATERING ACTIVITIES AND LAND DISTURBING ACTIVITIES IN AREAS OTHER THAN INDICATED ON THESE PLANS (INCLUDING BUT NOT LIMITED TO, ADDITIONAL PHASES OF DEVELOPMENT AND OFF-SITE BORROW OR WASTE AREAS) A SUPPLEMENTARY EROSION CONTROL PLAN SHALL BE SUBMITTED TO THE OWNER FOR REVIEW AND INCLUSION INTO SWPPP.
2. DURING DEWATERING OPERATION, WATER SHALL BE PUMPED FROM A SUMP PIT INTO SEDIMENT BASINS OR SILT TRAPS OR OTHER APPROVED BMP. COSTS OF SUMP PITS, SEDIMENT BASINS AND SILT TRAPS AND OTHER BMP ARE INCIDENTAL TO THE DEWATERING PAY ITEM. DEWATERING DIRECTLY INTO FIELD TILES OR STORMWATER STRUCTURES IS PROHIBITED.
3. UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE CONSTRUCTED ACCORDING TO MINIMUM STANDARDS AND SPECIFICATIONS IN THE ILLINOIS URBAN MANUAL CURRENT EDITION FOUND AT ILLINOISMANUAL.ORG.
4. WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND OR PERMANENT VEGETATIVE COVER FOR PROPER EROSION AND SEDIMENT CONTROL. TEMPORARY SEEDING (156) SHALL BE PAID FOR ONLY WHEN NEEDED FOR WINTERIZATION OF THE SITE AT THE LOCATIONS NOTED BY THE ENGINEER. ALL OTHER APPLICATIONS OF TEMPORARY SEED NEEDED FOR COMPLIANCE FOR THE NPDES PERMIT SHALL BE INCIDENTAL TO THE CONTRACT.

IL. CONTRACT: **KA049**
 IL. LETTING ITEM: **02A**
 IL. PROJECT: **IKK-4707**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS

NUMBER	BY	DATE

0 1 2
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 AT FULL SCALE (34x22).

KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING

STORMWATER POLLUTION PREVENTION PLAN

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KANKAKEE VALLEY
AIRPORT AUTHORITY

DESIGN BY: ARM

DRAWN BY: JRO

CHECKED BY: DKP

APPROVED BY: AKM

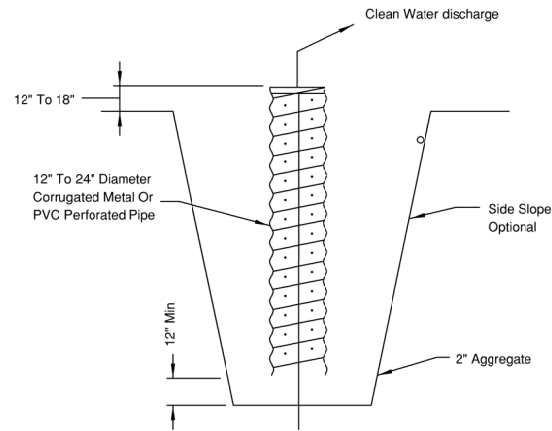
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JOB No: 160075-02

FINAL

SHEET 7 OF 19 SHEETS

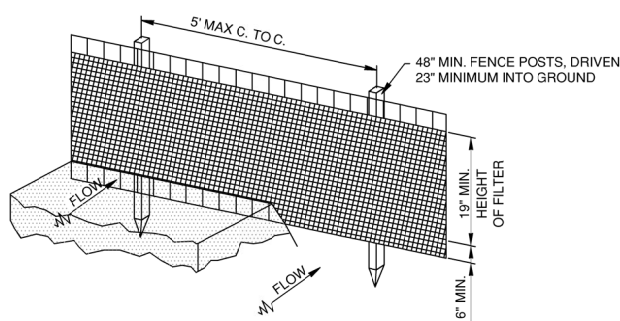
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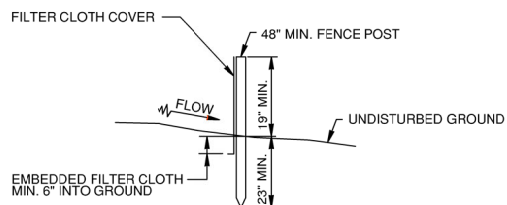
SECTION
SUMP PIT PLAN
N.T.S.

NOTES:

- Pit dimensions are optional.
- The standpipe will be constructed by perforating a 12"-24" diameter corrugated metal or PVC pipe.
- A base of 2" aggregate will be placed in the pit to a minimum depth of 12". After installing the standpipe, the pit surrounding the standpipe will then be backfilled with 2" aggregate.
- The standpipe will extend 12" to 18" above the lip of the pit.
- If discharge will be pumped directly to a storm drainage system, the standpipe will be wrapped with filter fabric before installation.
- If desired, 1/4"-1/2" hardware cloth may be placed around the standpipe prior to attaching the filter fabric. This will increase the rate of water seepage into the pipe.



PERSPECTIVE VIEW

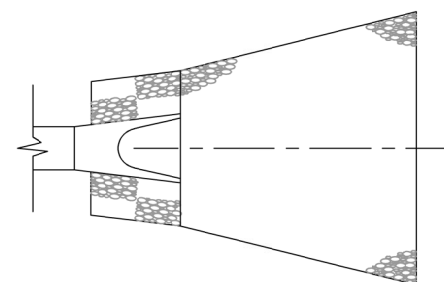
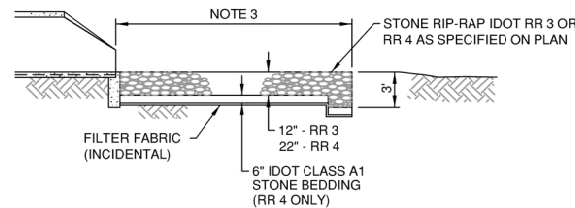


SECTION

SILT FENCE DETAIL
N.T.S.

POSTS: STEEL EITHER T OR U TYPE OR 2" HARDWOOD

CONTRACTOR SHALL HAVE THE OPTION TO INSTALL SILT FILTER FENCE, ROLLED EXCELSIOR OR URETHANE FOAM/GEOTEXTILE AS SPECIFIED IN SECTION 156-2.1 OF THE SPECIAL PROVISIONS.



RIP RAP AT CULVERT ENDS
N.T.S.

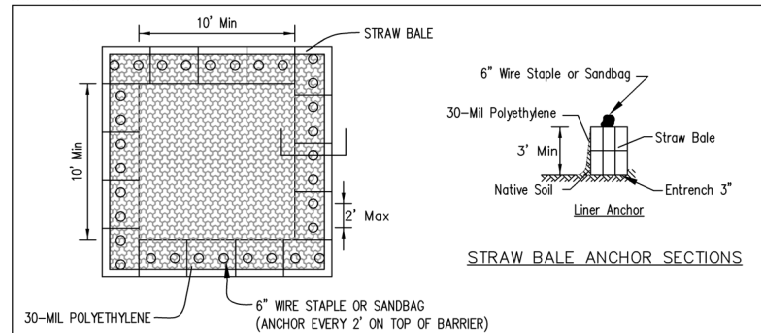
MAINTENANCE NOTES:

- INSPECT RIP RAP AFTER STORM EVENTS FOR STONE DISPLACEMENT AND FOR EROSION AT THE SIDES AND ENDS OF THE APRON.
- TAKE NEEDED REPAIRS IMMEDIATELY; USE APPROPRIATE SIZE STONE, AND DO NOT PLACE THEM ABOVE FINISHED GRADE.
- THE ENGINEER SHALL DETERMINE THE FINAL RIP-RAP CONFIGURATION IN THE FIELD.
- FILTER FABRIC INCIDENTAL TO RIP RAP.

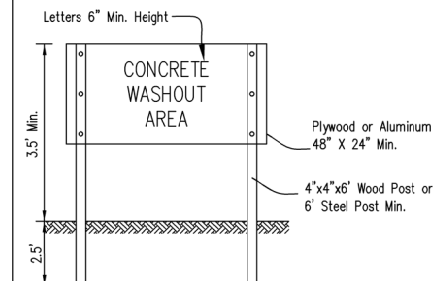
NOTES:

- FILTER CLOTH TO BE FASTENED SECURELY TO POSTS.
- WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER, THEY SHALL BE OVERLAPPED BY 6" MIN. AND FOLDED.
- MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.

AUTOCAD2006



PLAN VIEW



SIGN DETAIL

TEMPORARY CONCRETE WASHOUT FACILITY - STRAW BALE

NOTES:

- Maintaining temporary concrete washout facilities shall include removing and disposing of hardened concrete and/or slurry and returning the facilities to a functional condition.
- Facility shall be cleaned or reconstructed in a new area once washout becomes two-thirds full.
- The contractor may propose alternate configurations/materials or a pre-fabricated washout container for approval by the resident engineer at no additional cost to the project.
- The construction, maintenance, materials, removals, labor and all other incidentals necessary to construct the washout facility will not be measured for payment but will be considered incidental to the contract.

IL CONTRACT: **KA049**
 IL LETTING ITEM: **02A**
 IL PROJECT: **IKK-4707**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

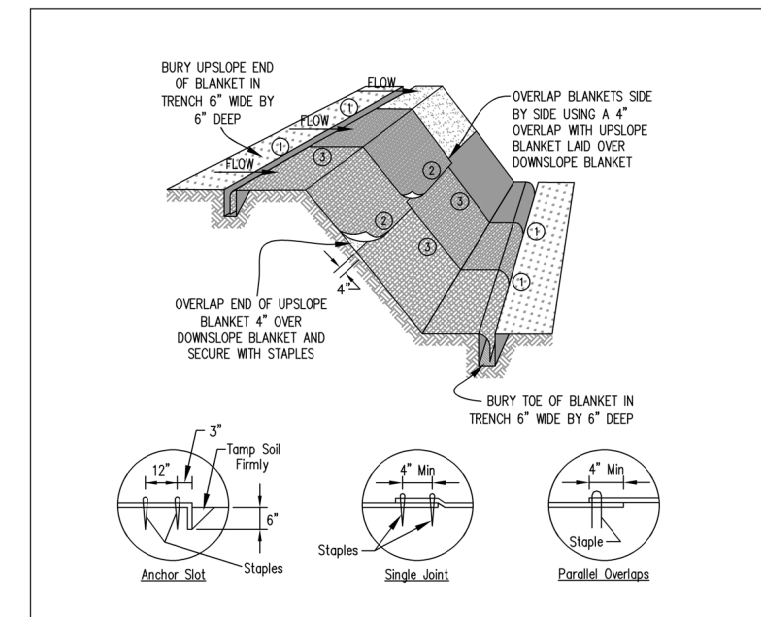
SURVEY BOOK # BOOK #

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**KANKAKEE VALLEY AIRPORT AUTHORITY
 KANKAKEE, ILLINOIS
 PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING
 STORM WATER POLLUTION PREVENTION PLAN
 DETAILS**



DETAIL 1

DETAIL 2

DETAIL 3

NOTES:

- Wood stakes shall be placed in a diamond pattern at 2 per s.y. for stiched blankets. Non-stiched shall use 4 stakes per s.y. of material. This equates to 200 stakes with stiched blanket and 400 stakes with non-stiched blanket per 100 s.y. of material.
- Wood stake lengths shall be selected based on soil type and conditions. (minimum stake length is 7")
- Erosion control material shall be placed in contact with the soil over a prepared seedbed.
- All anchor slots shall be stapled at approximately 12" intervals.

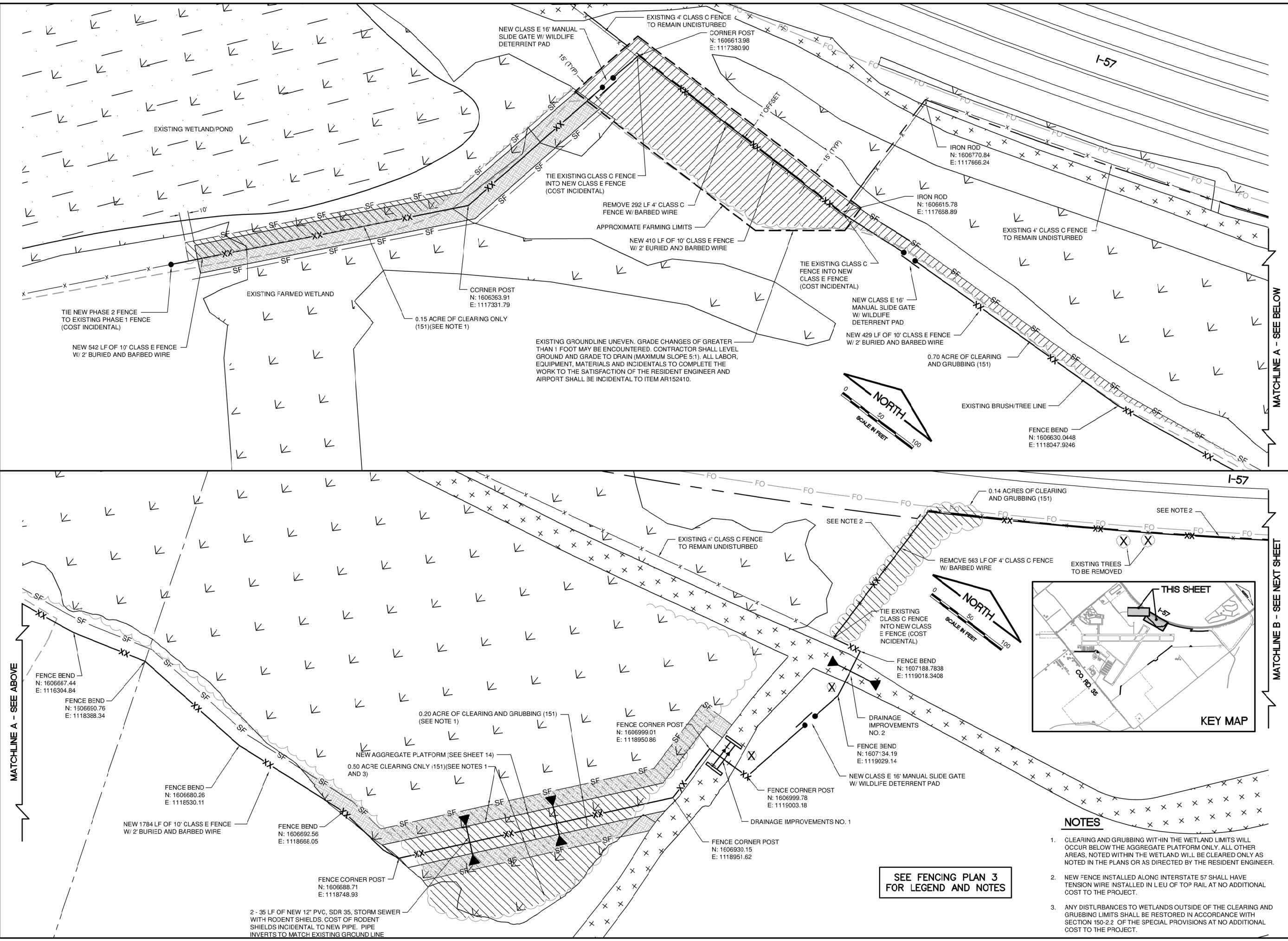
EROSION CONTROL BLANKET INSTALLATION DETAILS

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DESIGN BY: ARM
 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: AKM
 DATE: 6/7/19
 JOB No: 160075-02

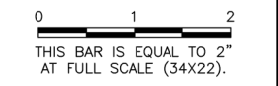
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 LAYOUT: FENCING PLAN 1
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 REF: DWG: 16075-02



IL CONTRACT: **KA049**
 IL LETTING ITEM: **02A**
 IL PROJECT: **IKK-4707**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

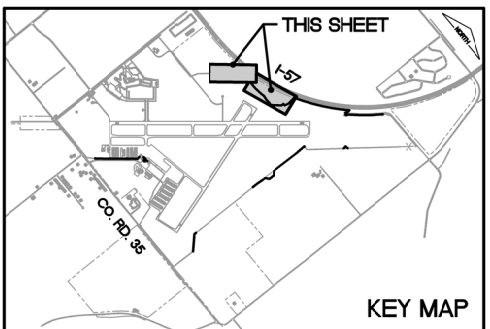
REVISIONS		
NUMBER	BY	DATE



KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING
FENCING PLAN 1



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DRAWN BY:	JRO
CHECKED BY:	ARM
APPROVED BY:	DKP
DATE:	6/7/19
JOB No:	16075-02



- NOTES**
- CLEARING AND GRUBBING WITHIN THE WETLAND LIMITS WILL OCCUR BELOW THE AGGREGATE PLATFORM ONLY. ALL OTHER AREAS, NOTED WITHIN THE WETLAND WILL BE CLEARED ONLY AS NOTED IN THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER.
 - NEW FENCE INSTALLED ALONG INTERSTATE 57 SHALL HAVE TENSION WIRE INSTALLED IN LEU OF TOP RAIL AT NO ADDITIONAL COST TO THE PROJECT.
 - ANY DISTURBANCES TO WETLANDS OUTSIDE OF THE CLEARING AND GRUBBING LIMITS SHALL BE RESTORED IN ACCORDANCE WITH SECTION 150-2.2 OF THE SPECIAL PROVISIONS AT NO ADDITIONAL COST TO THE PROJECT.

SEE FENCING PLAN 3 FOR LEGEND AND NOTES

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UPDATE BY: Jim O'hea
LAYOUT: FENCING PLAN 2

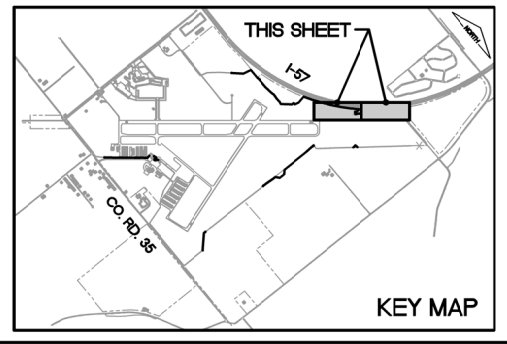
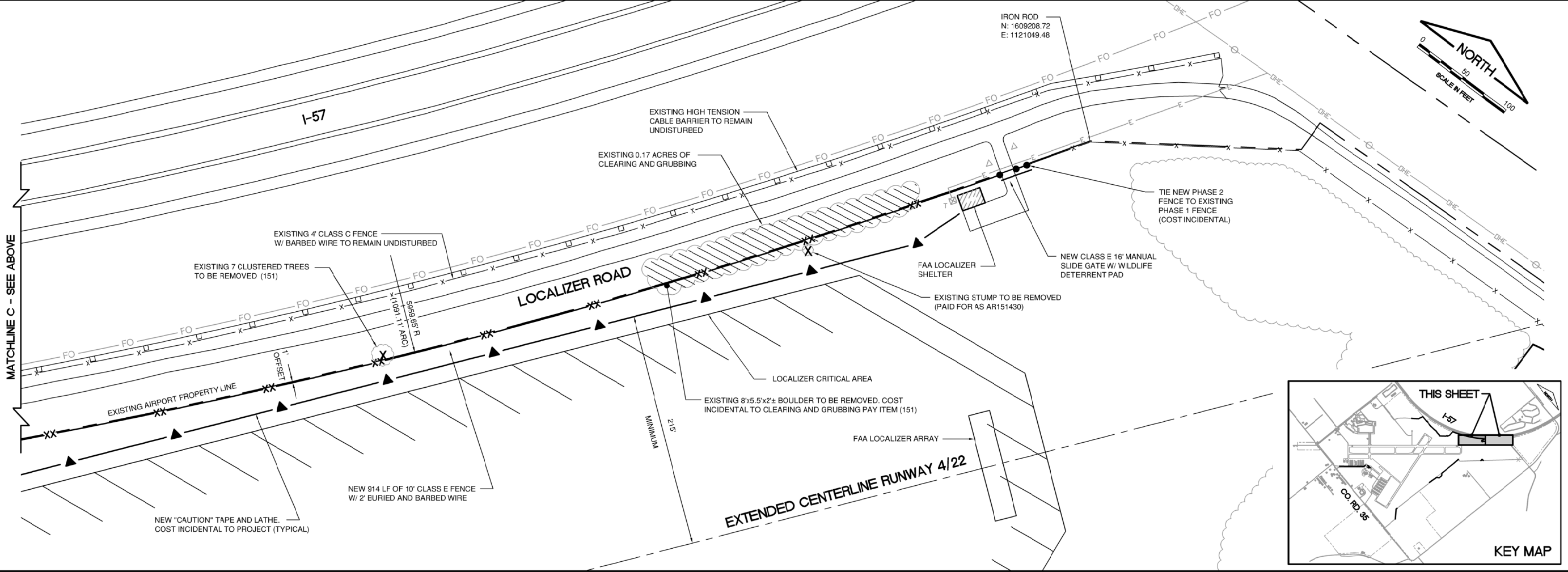
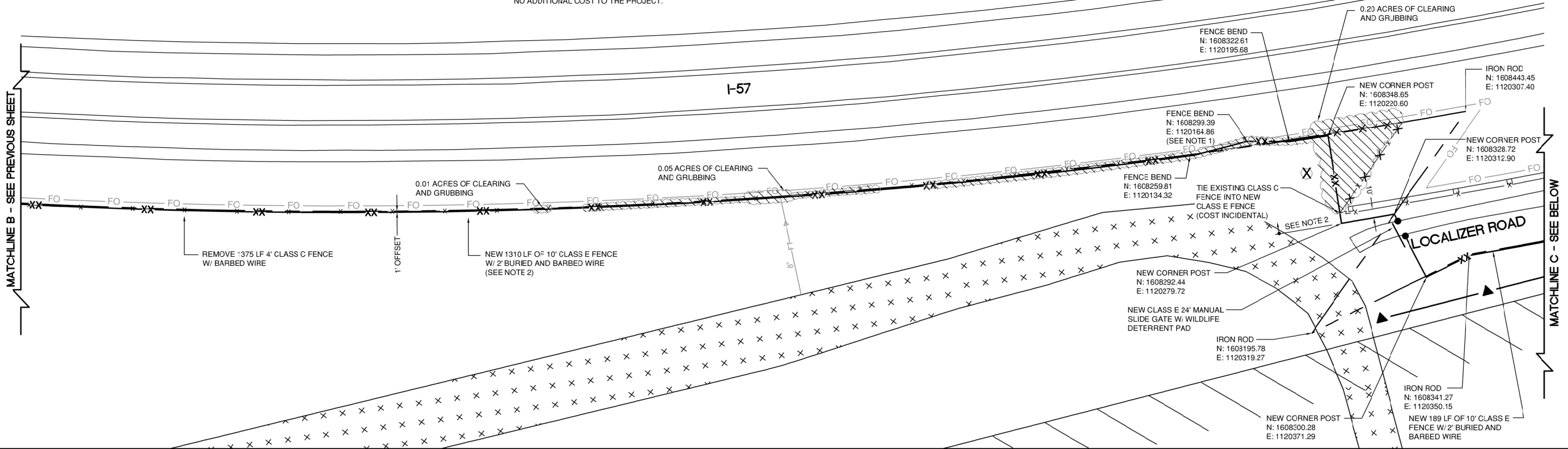
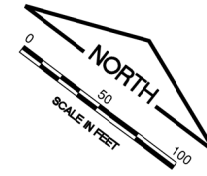
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NOTES

1. NEW FENCE POST MUST BE INSTALLED AT THE LOCATION NOTED TO OBTAIN THE NECESSARY CLEARANCE TO COMPLY WITH RUNWAY 4/22 OBJECT FREE AREA.
2. NEW FENCE INSTALLED ALONG INTERSTATE 57 SHALL HAVE TENSION WIRE INSTALLED IN LIEU OF TOP RAIL AT NO ADDITIONAL COST TO THE PROJECT.

SEE FENCING PLAN 3 FOR LEGEND AND NOTES



IL CONTRACT: **KA049**
 IL LETTING ITEM: **02A**
 IL PROJECT: **IKK-4707**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
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**KANKAKEE VALLEY AIRPORT AUTHORITY
 KANKAKEE, ILLINOIS
 PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING
 FENCING PLAN 2**

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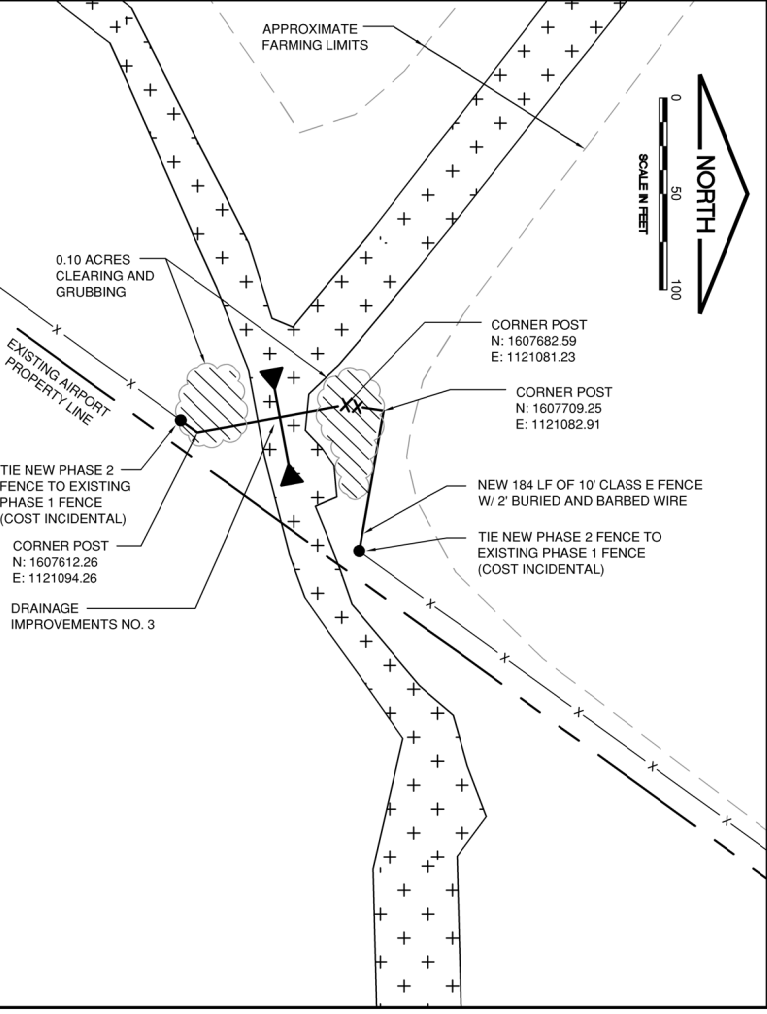
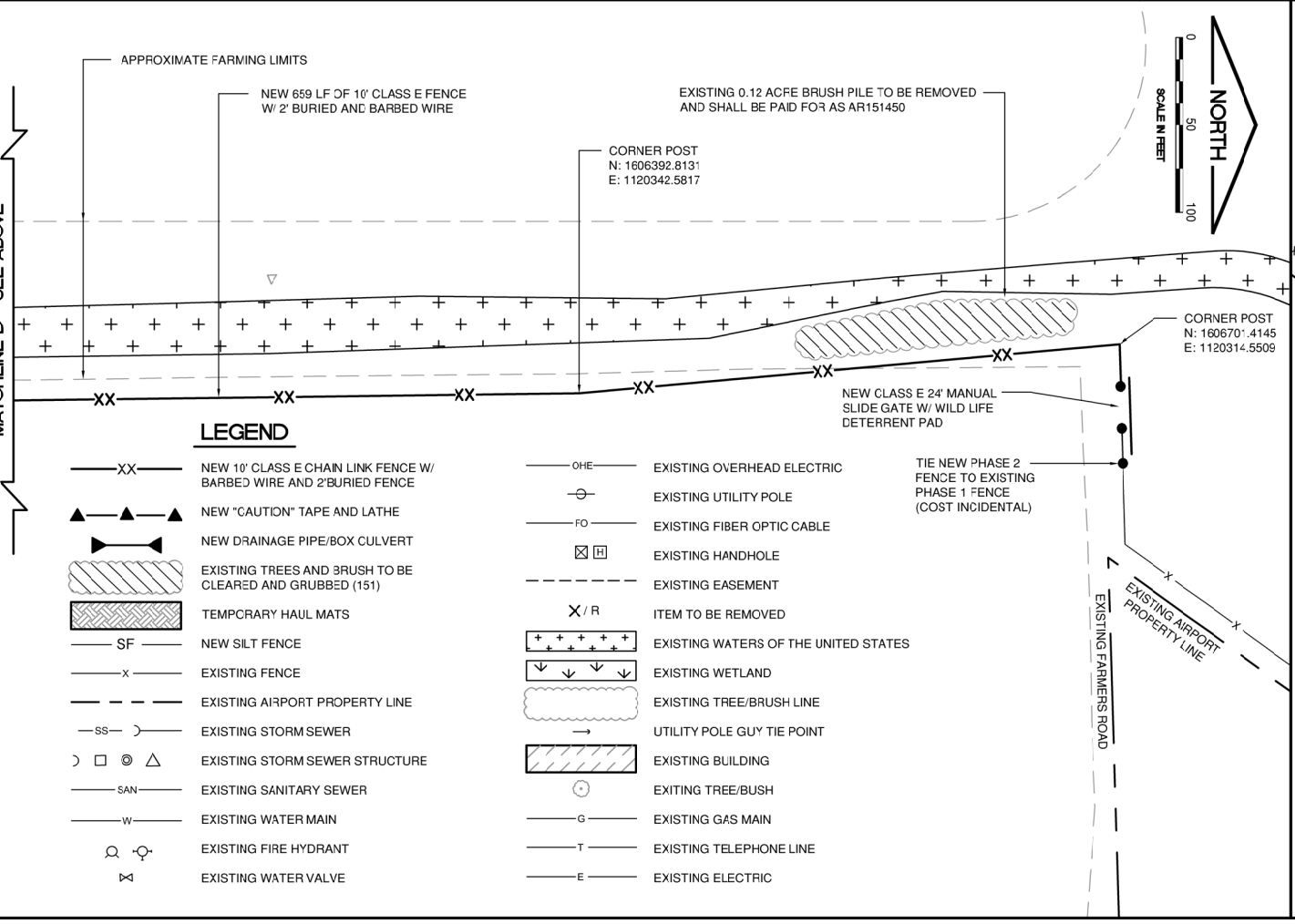
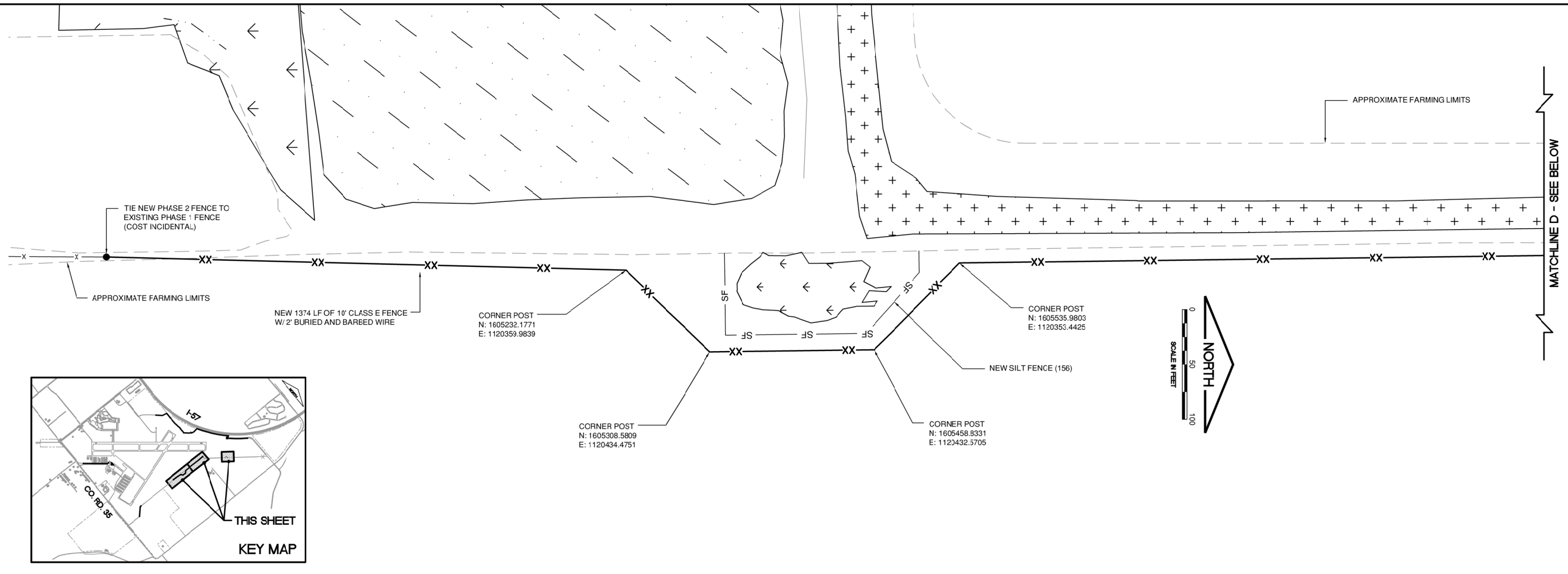
KANKAKEE VALLEY AIRPORT AUTHORITY

DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	ARM
APPROVED BY:	DKP
DATE:	6/7/19
JOB No:	160375-02

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SHEET 10 OF 19 SHEETS

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 IMAGE FILES: Logo.jpg
 Bmp: base base.dwg



- ### GENERAL NOTES
- ALL LENGTHS SHOWN FOR FENCE REMOVALS OR NEW FENCE ARE FOR THE SECTIONS SHOWN ON THAT SHEET ONLY.
 - CONTRACTOR SHALL INSTALL "CAUTION DO NOT ENTER" TAPE AND LATHE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. COST INCIDENTAL TO THE CONTRACT.
 - AREAS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER TO BE CLEARED AND GRUBBED SHALL BE SMOOTHLY GRADED TO DRAIN MEETING THE SATISFACTION OF THE ENGINEER AND AIRPORT. GRADING SHALL BE INCIDENTAL TO THE SEEDING PAY ITEM. THE AREAS DISTURBED BY CLEARING AND GRUBBING OPERATIONS SHALL BE SEEDED (901) AND MULCHED (908).
 - GATE LOCATIONS TO BE COORDINATED WITH THE AIRPORT.
 - UPON REQUEST BY THE CONTRACTOR, A COPY OF THE AIRPORT'S BOUNDARY SURVEY IS AVAILABLE FOR USE BY THE CONTRACTOR.
 - WHEN THE NEW FENCE IS LOCATED WITHIN OR ADJACENT TO A FARM FIELD, THE CONTRACTOR SHALL SEED (901) AND MULCH (908) THE FARM FIELD WITHIN 10' OF THE FENCE OR AS DIRECTED BY THE ENGINEER.
 - FARM FIELD TILES MUST BE PROTECTED BY THE CONTRACTOR. FARM FIELD TILES ENCOUNTERED DURING CONSTRUCTION, IF DAMAGED MUST BE REPLACED IN KIND AT THE CONTRACTOR'S COST.
 - ALL PROJECT AREAS, INCLUDING STOCKPILES, ABANDONED HAUL ROADS AND STAGING AREAS, AS SHOWN ON THE PLANS, SHALL HAVE 4 INCHES OF TOPSOIL PLACED AND BE SEEDED AND MULCHED IN ACCORDANCE WITH THE SPECIFICATIONS. AREAS DISTURBED OUTSIDE THE PROJECT LIMITS WILL BE SEEDED AND MULCHED BY THE CONTRACTOR AT HIS COST AND RESTORED TO ORIGINAL CONDITIONS.
 - ANY FIELD DRAIN TILES THAT ARE DAMAGED IN ANY WAY SHALL BE IMMEDIATELY REPAIRED ACCORDING TO THE PLANS, SPECIFICATIONS AND SHALL BE PROTECTED FROM SEDIMENT-LADEN WATER.
 - WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND OR PERMANENT VEGETATIVE COVER FOR PROPER EROSION AND SEDIMENT CONTROL.
 - TEMPORARY SEEDING (156) SHALL BE PAID FOR ONLY WHEN NEEDED FOR WINTERIZATION OF THE SITE AT THE LOCATIONS NOTED BY THE ENGINEER. ALL OTHER APPLICATIONS OF TEMPORARY SEED NEEDED FOR COMPLIANCE FOR THE NPDES PERMIT SHALL BE INCIDENTAL TO THE CONTRACT.

IL CONTRACT: **KA049**
 IL LETTING ITEM: **02A**
 IL PROJECT: **IKK-4707**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #
 REVISIONS

NUMBER	BY	DATE

0 1 2
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KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING
FENCING PLAN 3

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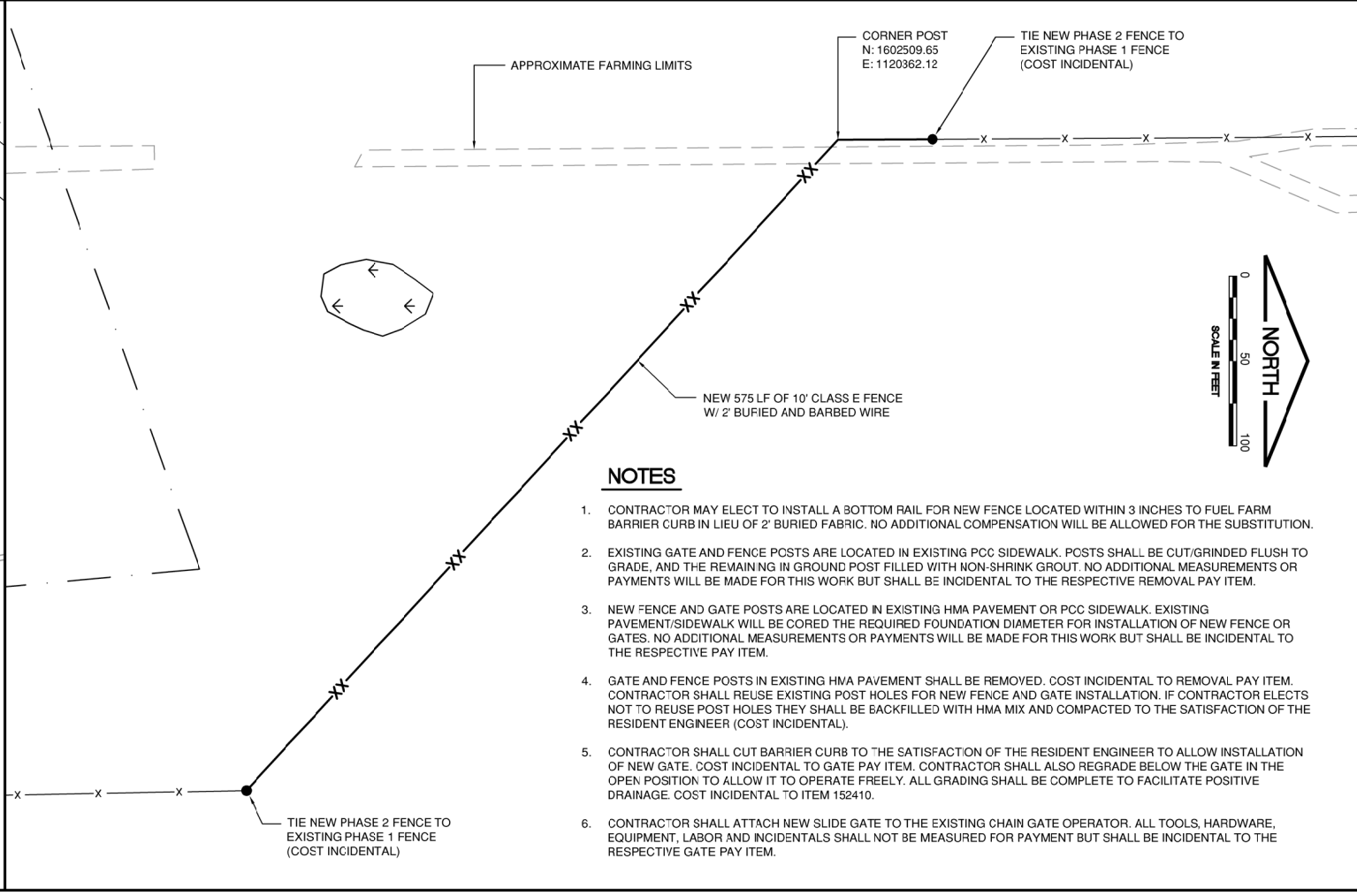
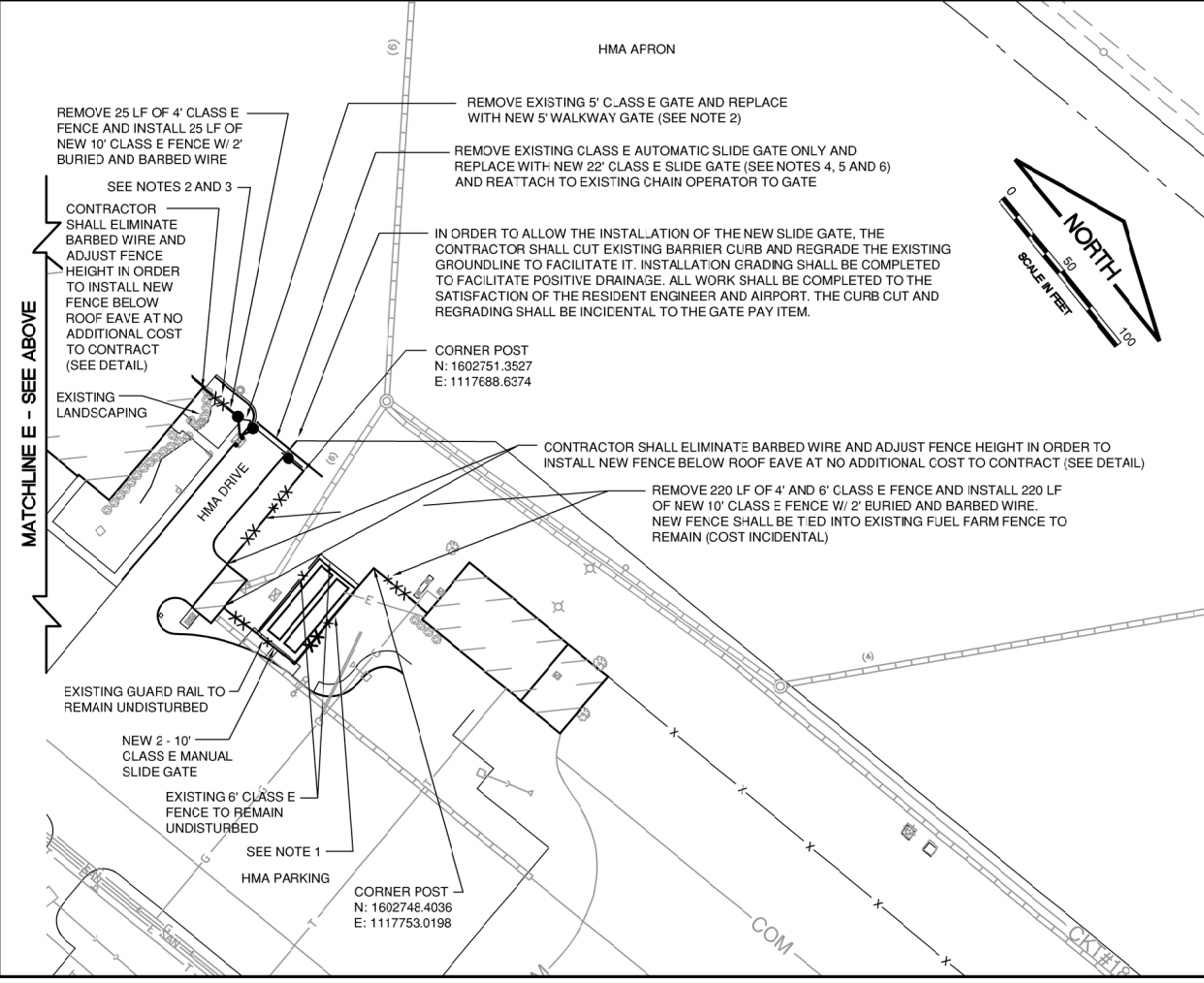
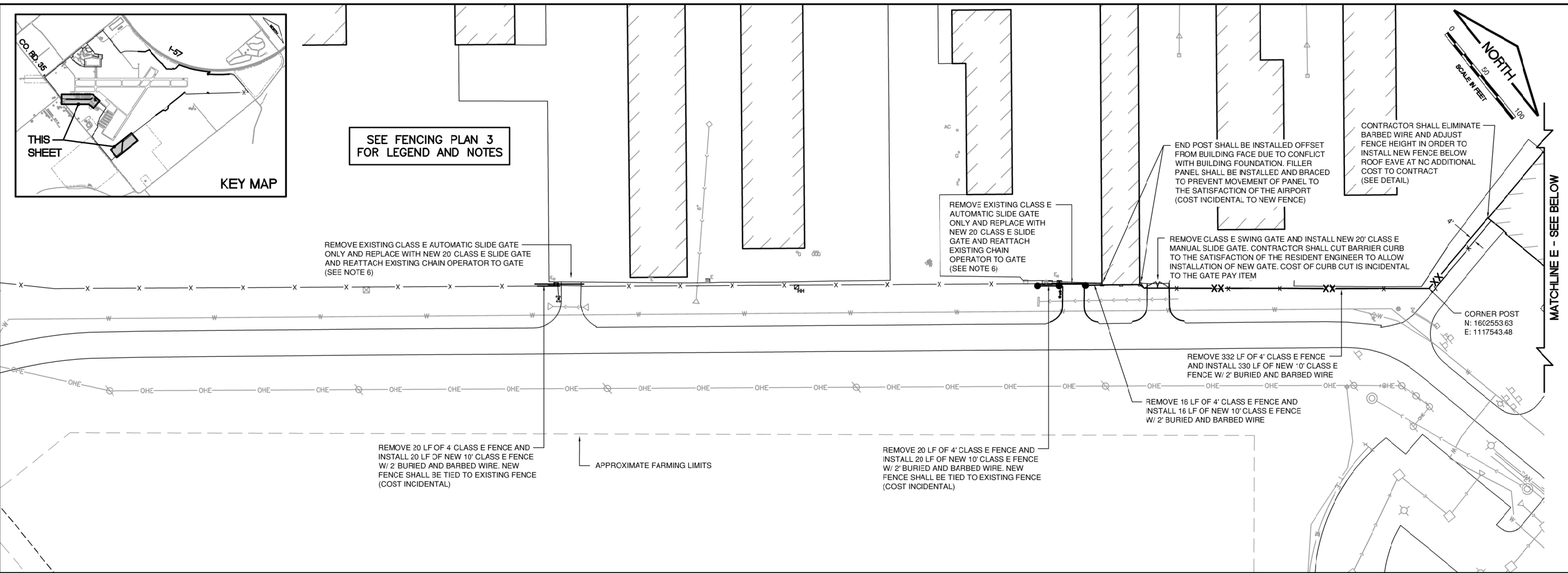
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APPROVED BY:	DKP
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JOB No:	16075-02

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SHEET 11 OF 19 SHEETS

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NOTES

1. CONTRACTOR MAY ELECT TO INSTALL A BOTTOM RAIL FOR NEW FENCE LOCATED WITHIN 3 INCHES TO FUEL FARM BARRIER CURB IN LIEU OF 2' BURIED FABRIC. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE SUBSTITUTION.
2. EXISTING GATE AND FENCE POSTS ARE LOCATED IN EXISTING PCC SIDEWALK. POSTS SHALL BE CUT/GRINDED FLUSH TO GRADE, AND THE REMAINING IN GROUND POST FILLED WITH NON-SHRINK GROUT. NO ADDITIONAL MEASUREMENTS OR PAYMENTS WILL BE MADE FOR THIS WORK BUT SHALL BE INCIDENTAL TO THE RESPECTIVE REMOVAL PAY ITEM.
3. NEW FENCE AND GATE POSTS ARE LOCATED IN EXISTING HMA PAVEMENT OR PCC SIDEWALK. EXISTING PAVEMENT/SIDEWALK WILL BE CORED THE REQUIRED FOUNDATION DIAMETER FOR INSTALLATION OF NEW FENCE OR GATES. NO ADDITIONAL MEASUREMENTS OR PAYMENTS WILL BE MADE FOR THIS WORK BUT SHALL BE INCIDENTAL TO THE RESPECTIVE PAY ITEM.
4. GATE AND FENCE POSTS IN EXISTING HMA PAVEMENT SHALL BE REMOVED. COST INCIDENTAL TO REMOVAL PAY ITEM. CONTRACTOR SHALL REUSE EXISTING POST HOLES FOR NEW FENCE AND GATE INSTALLATION. IF CONTRACTOR ELECTS NOT TO REUSE POST HOLES THEY SHALL BE BACKFILLED WITH HMA MIX AND COMPACTED TO THE SATISFACTION OF THE RESIDENT ENGINEER (COST INCIDENTAL).
5. CONTRACTOR SHALL CUT BARRIER CURB TO THE SATISFACTION OF THE RESIDENT ENGINEER TO ALLOW INSTALLATION OF NEW GATE. COST INCIDENTAL TO GATE PAY ITEM. CONTRACTOR SHALL ALSO REGRADE BELOW THE GATE IN THE OPEN POSITION TO ALLOW IT TO OPERATE FREELY. ALL GRADING SHALL BE COMPLETE TO FACILITATE POSITIVE DRAINAGE. COST INCIDENTAL TO ITEM 152410.
6. CONTRACTOR SHALL ATTACH NEW SLIDE GATE TO THE EXISTING CHAIN GATE OPERATOR. ALL TOOLS, HARDWARE, EQUIPMENT, LABOR AND INCIDENTALS SHALL NOT BE MEASURED FOR PAYMENT BUT SHALL BE INCIDENTAL TO THE RESPECTIVE GATE PAY ITEM.

IL CONTRACT: **KA049**
 IL LETTING ITEM: **02A**
 IL PROJECT: **IKK-4707**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PHASE 2 - INSTALL SECURITY/LIFE 10' FENCING
FENCING PLAN 4

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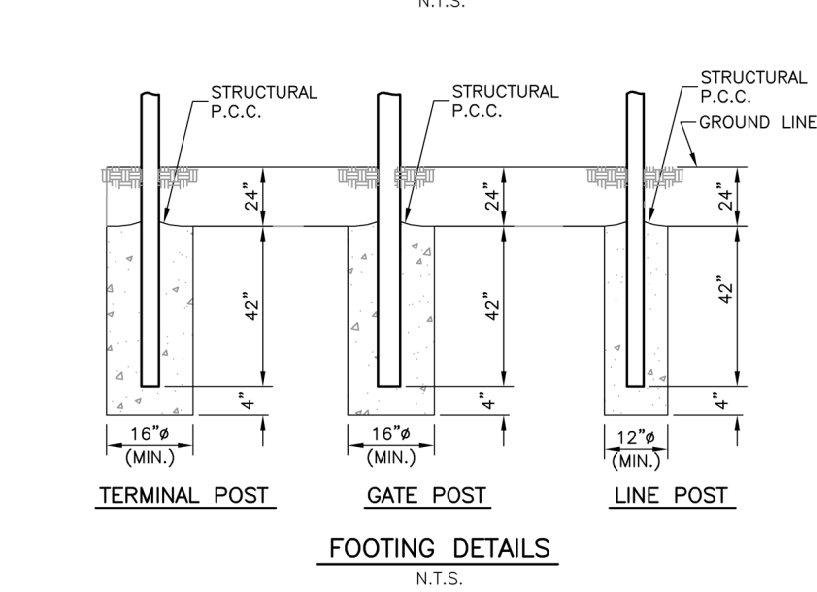
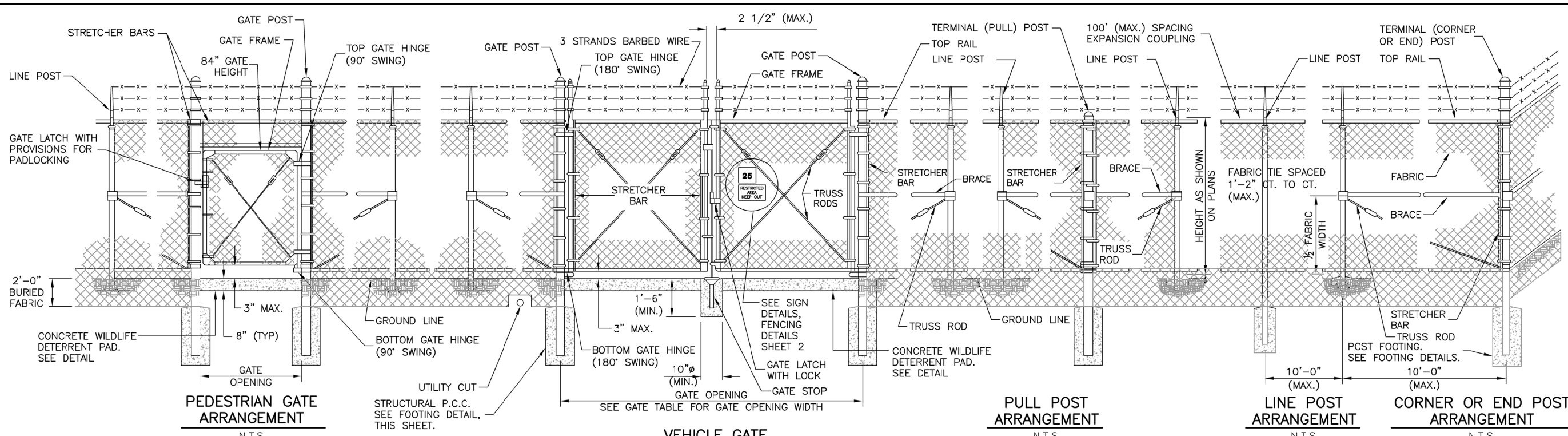
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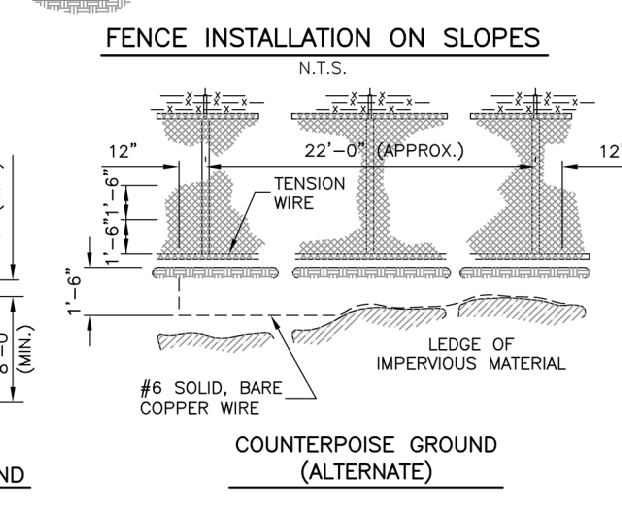
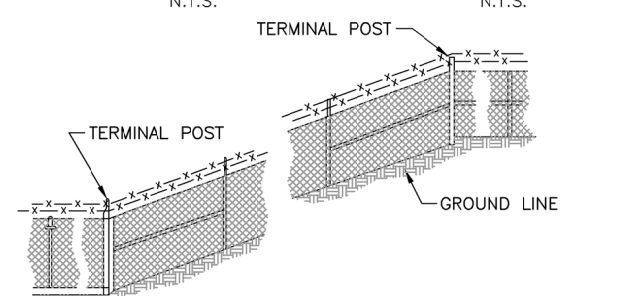
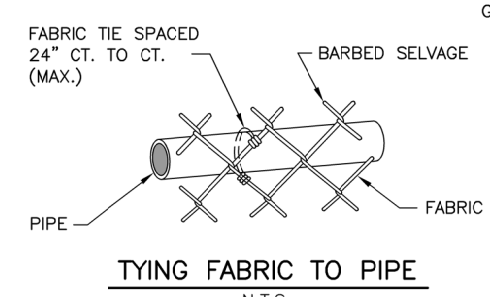
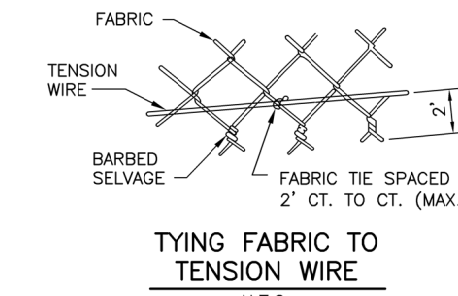
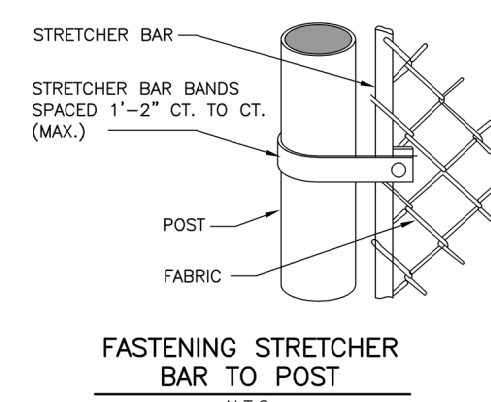
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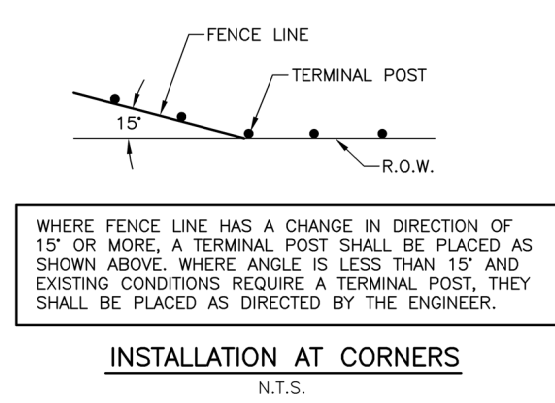
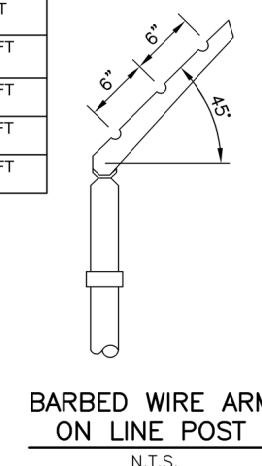
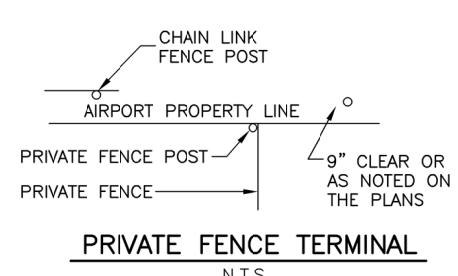
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 REF: DWG: 16075-02
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 bdf: 06/18/2019



TENSION WIRE SHALL BE INSTALLED IN LIEU OF TOP RAIL ALONG INTERSTATE 57 AT NO ADDITIONAL COST TO THE PROJECT.



FENCE POST TABLE				
POST TYPE	ASTM F 1083		ASTM F 1043	
	DIAMETER	WEIGHT	DIAMETER	WEIGHT
CORNER, END, PULL	3.5" O.D.	7.58 LB/FT	3.5" O.D.	5.71 LB/FT
LINE POST	2.875" O.D.	5.80 LB/FT	2.875" O.D.	4.64 LB/FT
TOP RAIL	1.66" O.D.	2.27 LB/FT	1.66" O.D.	1.84 LB/FT
GATE POST	4" O.D.	9.12 LB/FT	4" O.D.	6.56 LB/FT



- NOTES**
- CONTINUOUS FENCE SHALL BE GROUNDED AT INTERVALS NOT EXCEEDING 500 FT. EXCEPT THERE SHALL BE A GROUND NOT EXCEEDING 100 FT. FROM GATE IN EACH SECTION OF THE FENCE ADJACENT TO THE GATE.
 - FENCE UNDER POWER LINES SHALL BE GROUNDED BY 3 GROUNDS, ONE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE 25 TO 50 FT. AWAY. A SINGLE GROUND SHALL BE LOCATED DIRECTLY UNDER EACH TELEPHONE WIRE OR CABLE CROSSINGS.
 - THE COUNTERPOISE SHALL BE USED ONLY WHERE IT IS IMPOSSIBLE TO DRIVE A GROUND ROD BECAUSE OF IMPERVIOUS EARTH STRUCTURES.
 - THE GROUND WIRE SHALL BE CONNECTED TO THE FABRIC AND THE GROUND ROD BY A MECHANICAL CLAMP OF CAST BRONZE BODY AND BRONZE OR STAINLESS STEEL BOLTS AND WASHERS. WHEN A TENSION WIRE IS REQUIRED, THE BOTTOM CONNECTION OF THE GROUND WIRE SHALL BE MADE TO THE TENSION WIRE.

I.L. CONTRACT: **KA049**
 I.L. LETTING ITEM: **02A**
 I.L. PROJECT: **IKK-4707**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
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KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING
FENCING DETAILS 1

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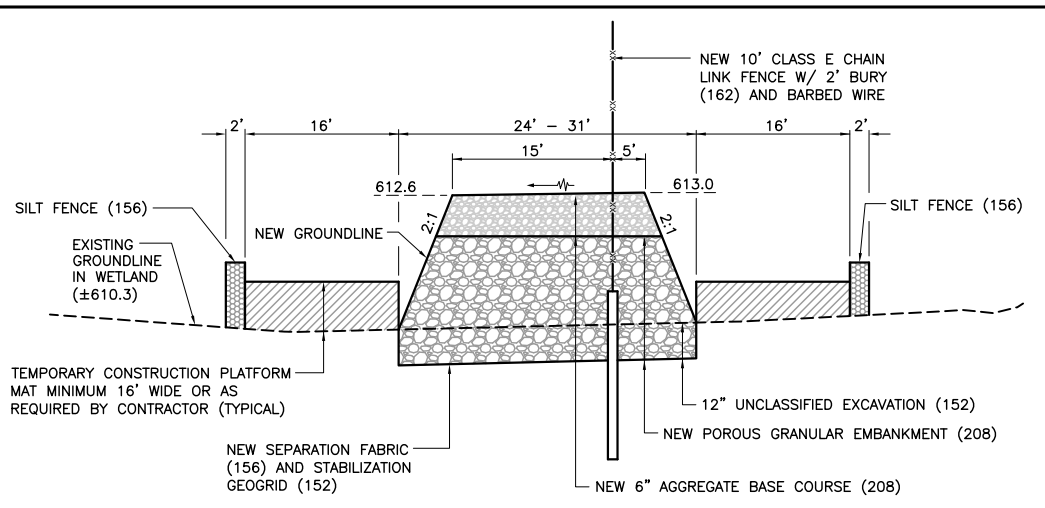
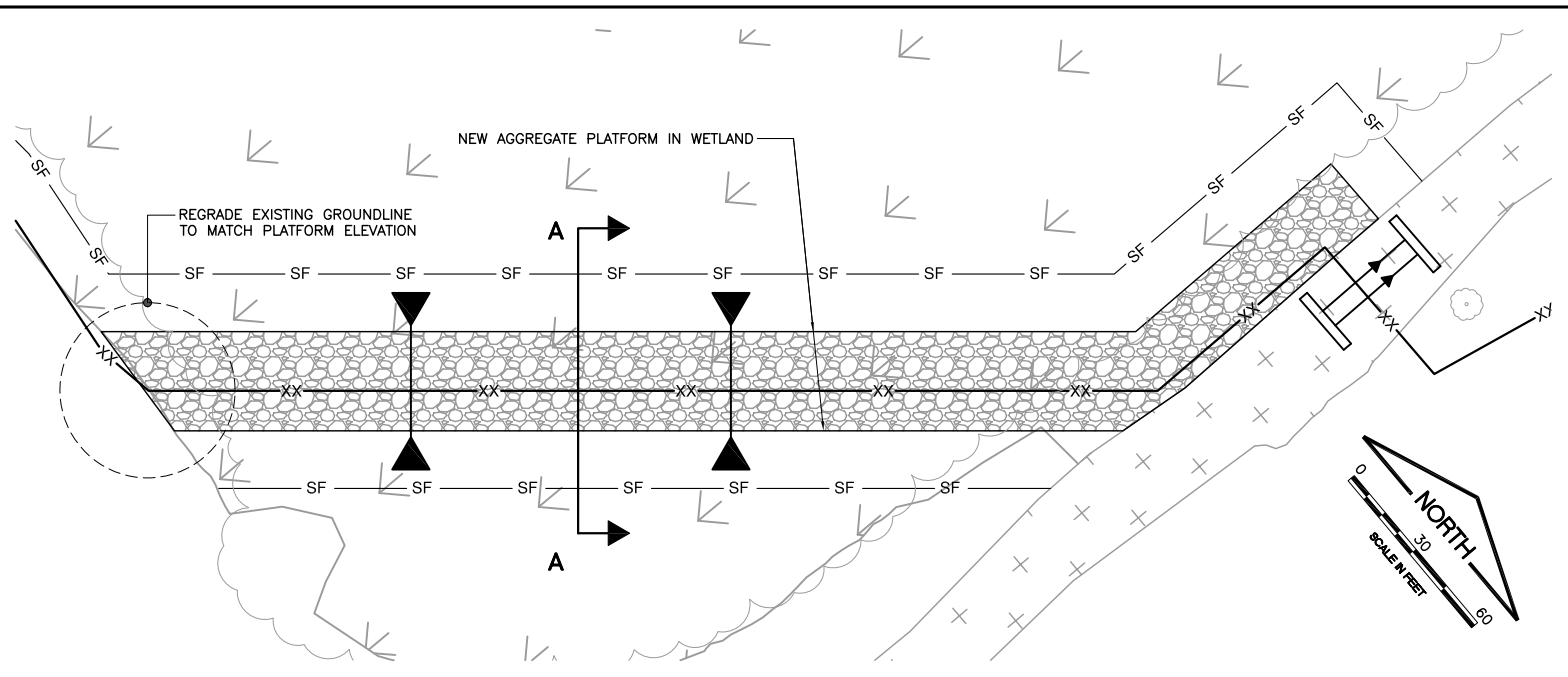
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CHECKED BY:	ARM
APPROVED BY:	DKP
DATE:	6/7/19
JOB No:	160075-02

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SHEET 13 OF 19 SHEETS

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SECTION A-A
NEW FENCE AND AGGREGATE PLATFORM IN WETLAND
 NOT TO SCALE

- NOTES:**
- TEMPORARY CONSTRUCTION MATS SHALL BE INCIDENTAL TO THE HAUL ROUTE PAY ITEM. NO ADDITIONAL MEASUREMENT OR PAYMENTS WILL BE MADE TO THE CONTRACTOR IF ADDITIONAL MAT WIDTH OR LENGTH IS CONSTRUCTED.
 - WETLAND MAY CONTAIN STANDING WATER, REQUIRING DE-WATERING PRIOR TO INSTALLATION OF AGGREGATE PLATFORM AND FENCE. DE-WATERING SHALL BE IN COMPLIANCE WITH SECTION 701-3.1 OF THE SPECIAL PROVISIONS.

I.L. CONTRACT: **KA049**
 I.L. LETTING ITEM: **02A**
 I.L. PROJECT: **IKK-4707**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

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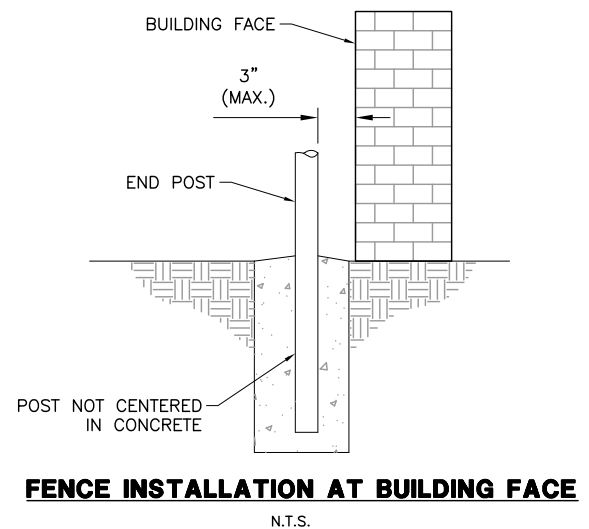
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KANKAKEE, ILLINOIS
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FENCING DETAILS 2

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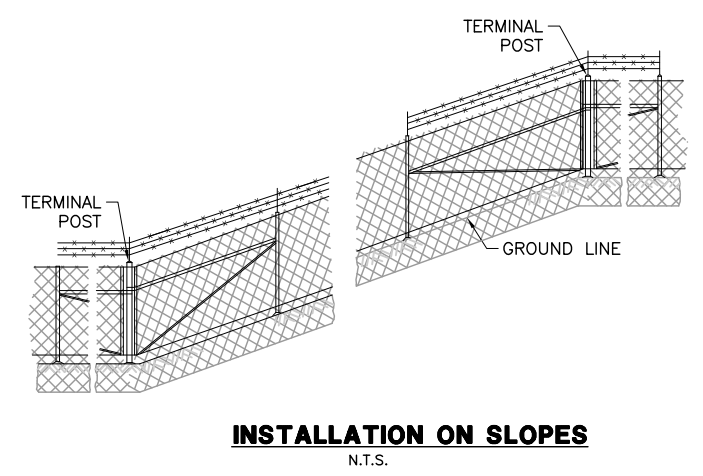
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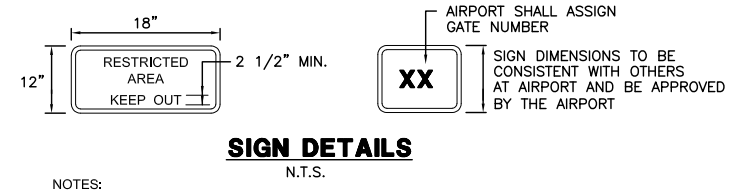
SHEET 14 OF 19 SHEETS



FENCE INSTALLATION AT BUILDING FACE
 N.T.S.



INSTALLATION ON SLOPES
 N.T.S.



SIGN DETAILS
 N.T.S.

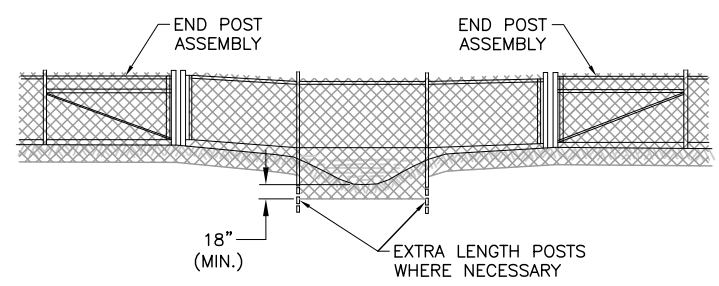
- NOTES:**
- EACH PEDESTRIAN OR VEHICLE SWING GATE SHALL REQUIRE ONE "RESTRICTED" SIGN AND ONE "NUMBER" SIGN. AIRPORT SHALL PROVIDE GATE NUMBER. EVERY 100' OF FENCE SHALL REQUIRE ONE "RESTRICTED" SIGN. COST INCIDENTAL TO FENCE.
 - 0.08 GA ALUMINUM ALLOY SHEET. LETTERING SHALL BE RED ON A WHITE BACKGROUND.
 - CANTILEVERED SLIDE GATES SHALL INCLUDE TWO ADDITIONAL SIGNS AS DETAILED ON THE CANTILEVER GATE DETAIL SHEETS.

SIGNS					
CONTRACTOR SHALL FURNISH AND INSTALL SIGNS AS INDICATED IN TABLE BELOW					
SIGN	DIMENSIONS WIDTH x HEIGHT	BACKGROUND COLOR	LETTERING		TEXT
			COLOR	HEIGHT	
1	GATE NUMBER - SEE DETAIL ON FENCING DETAILS SHEET 2				
2	24" x 18"	WHITE	RED	1.5"	FAA REGULATIONS REQUIRE THAT AFTER ENTERING OR EXITING THROUGH THIS SECURITY GATE YOU ARE REQUIRED TO WAIT UNTIL THE GATE CLOSES BEFORE PROCEEDING. FINES OR PENALTIES WILL BE IMPOSED.
3	18" x 12"	WHITE	RED	2-1/2"	RESTRICTED AREA KEEP OUT
4	WARNING SIGN - SEE DETAIL ON CANTILEVER GATE DETAILS SHEET				

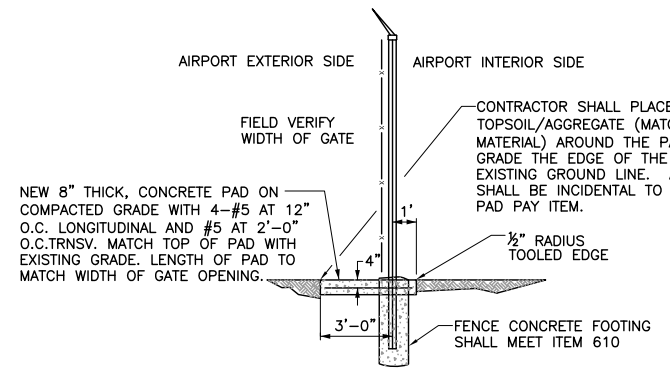
- NOTES:**
- SIGNS #1, #2 AND #4 SHALL BE INSTALLED ON BOTH SIDES OF GATE. SIGN #3 SHALL BE INSTALLED ON "LAND" SIDE OF GATE.
 - ALL CANTILEVERED SLIDE GATES, BOTH MANUAL AND ELECTRIC OPERATION, SHALL HAVE ALL FOUR SIGNS INSTALLED, INCIDENTAL TO GATE INSTALLATION.
 - ALL SWING GATES SHALL HAVE SIGNS #1 AND #3 INSTALLED, INCIDENTAL TO GATE INSTALLATION.

GENERAL NOTES - GATE SIGNS

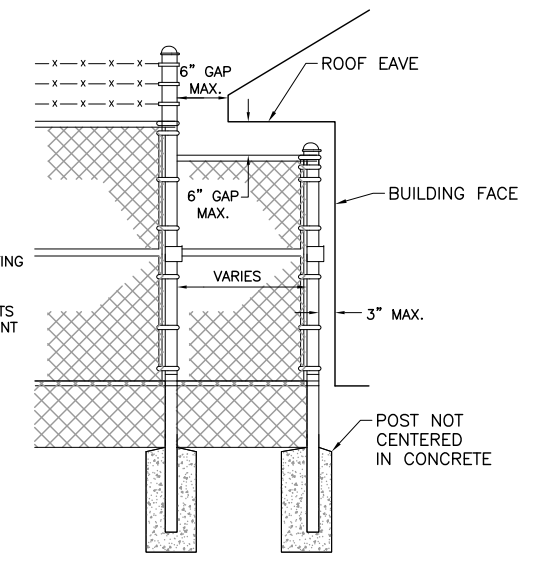
- SIGNS OTHER THAN THOSE SHOWN ABOVE THAT ARE AFFIXED TO EXISTING GATES SHALL BE REMOVED AND TRANSFERRED TO THE NEW GATE AT THE COMPLETION OF THE GATE INSTALLATION. COST INCIDENTAL TO NEW GATE.



ELEVATION
FENCE INSTALLATION OVER STREAM OR SWALES
 N.T.S.

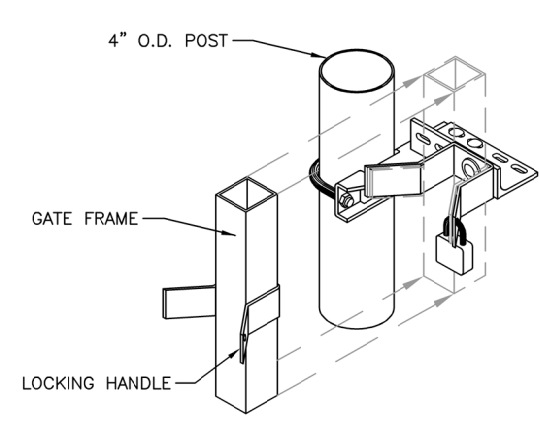


TYPICAL WILDLIFE DETERRENT
CONCRETE PAD AT GATES
 N.T.S.

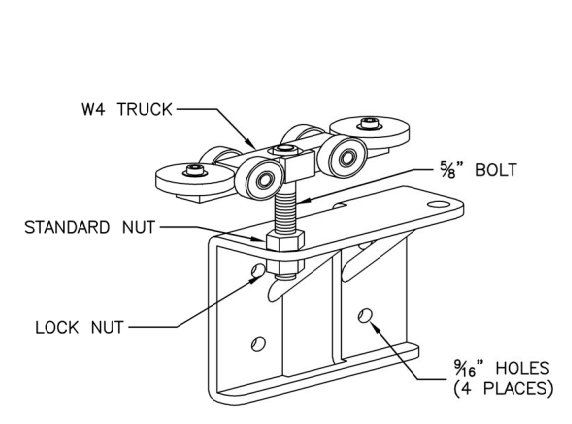


FENCE INSTALLATION AT BUILDING FACE
WITH LOW ROOF EAVE
 N.T.S.

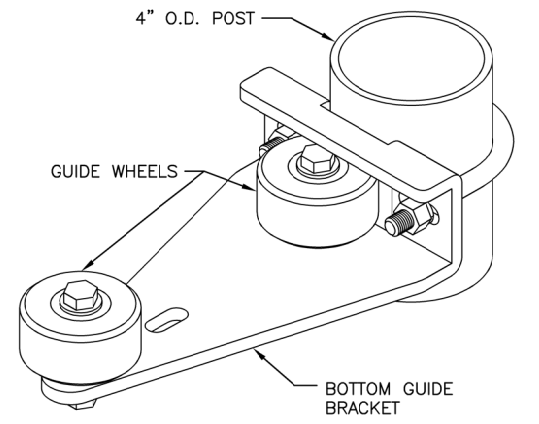
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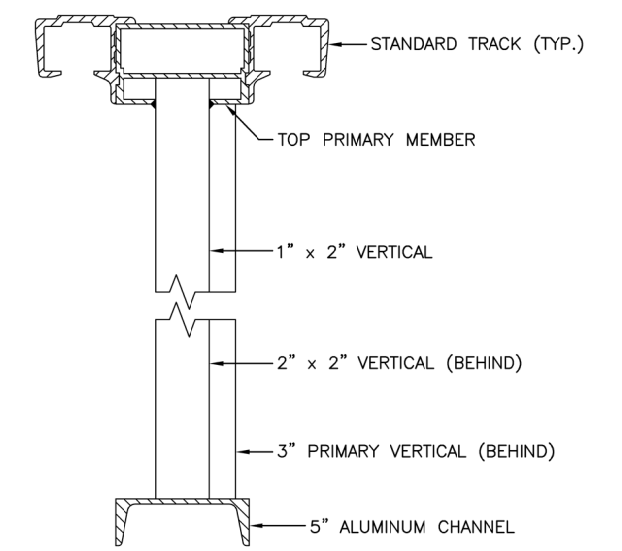
CATCH ASSEMBLY DETAIL
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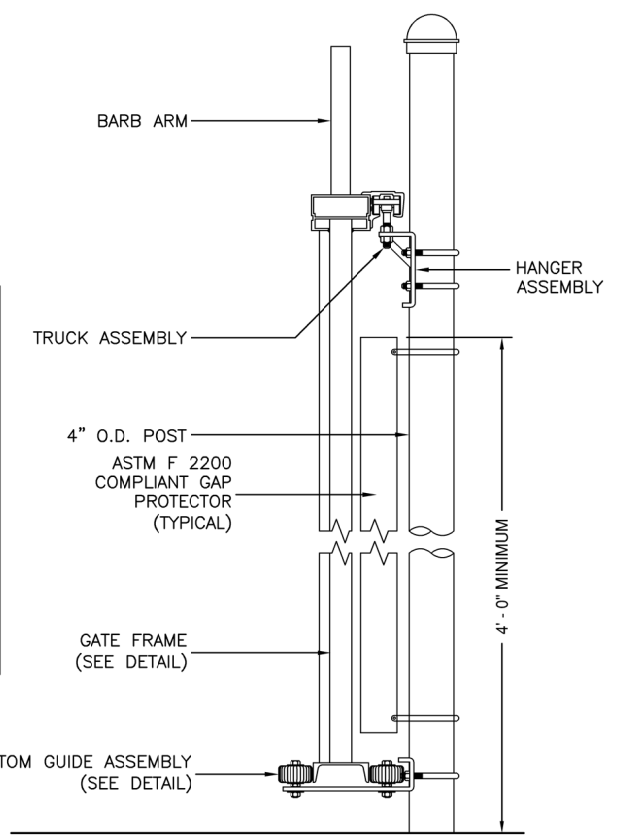
GATE HANGER ASSEMBLY
N.T.S.



STANDARD BOTTOM GUIDE ASSEMBLY
N.T.S.

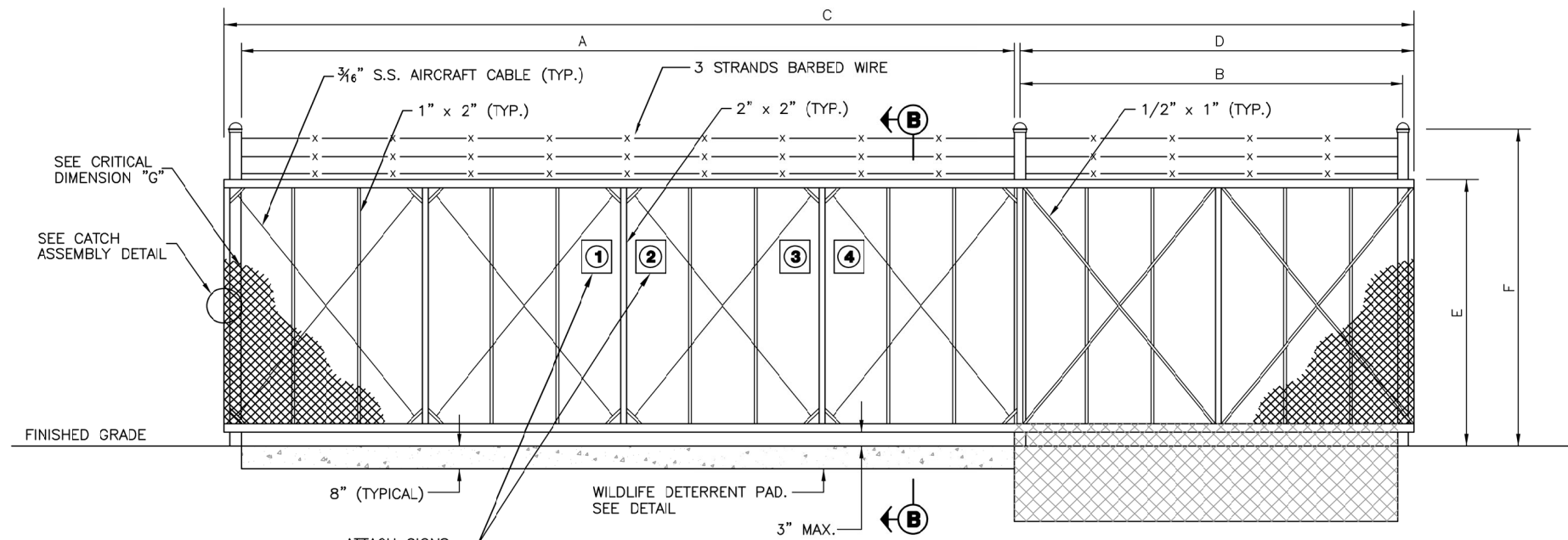


GATE FRAME SECTION
N.T.S.



NOTE:
EXTRUSIONS SHALL BE OVERSIZED FOR EXTRA RIGIDITY OVER "STANDARD" GATE DESIGN.

SECTION B-B
(TYPICAL ALL GATES)



ELEVATION

CANTILEVER SLIDE GATE
N.T.S.

LOCATIONS, DETAILS AND CHARACTER OF EQUIPMENT SHOWN ON THIS SHEET ARE GENERIC. EQUIPMENT LOCATIONS SHALL BE AS RECOMMENDED BY THE EQUIPMENT MANUFACTURER.

MANUAL SLIDE GATE NOTES:

- CANTILEVERED GATE SHALL BE SUFFICIENTLY RIGID TO WITHSTAND FLEXING OR BENDING DURING WINDY CONDITIONS. CONTRACTOR SHALL PROVIDE STIFFENERS, STRUCTURAL SHAPES IN EXCESS OF THE MINIMUM SPECIFIED DIMENSIONS OR ADDITIONAL ROLLERS AND POSTS SUFFICIENT TO PREVENT DISPLACEMENT OF THE GATE BY WIND OR BY UNAUTHORIZED PERSONNEL.
- CONTRACTOR SHALL PROVIDE AND INSTALL GATE AS A COMPLETE WORKING UNIT. THE GATE WORK SHALL INCLUDE, BUT NOT BE LIMITED TO: GATE, POSTS, AND ALL CONNECTIONS, SIGNS, LABOR AND MATERIALS NECESSARY FOR COMPLETE OPERATION.
- THE FABRIC TYPE & FINISH OF THE GATE SHALL MATCH THE PROPOSED FENCE OR BE AS DIRECTED BY THE ENGINEER.
- ALL SLIDING GATES SHALL HAVE ALL ROLLERS ENCLOSED IN STEEL OR PLASTIC SHROUDS TO PREVENT ACCIDENTAL INJURY.
- 3 STRANDS OF BARBED WIRE ON TOP OF GATE NOT SHOWN TO IMPROVE CLARITY.
- SEE FENCING DETAILS SHEET 2 FOR SIGN DETAILS.

CRITICAL DIMENSIONS						
A	CLEAR OPENING	10'-0"	16'-0"	20'-0"	22'-0"	24'-0"
B	COUNTER BALANCE POST SPACING, CENTER TO CENTER	4'-1"	7'-1"	9'-1"	10'-1"	11'-1"
C	OVERALL GATE LENGTH	15'-0"	24'-0"	30'-0"	33'-0"	36'-0"
D	COUNTERBALANCE LENGTH	5'-0"	8'-0"	10'-0"	11'-0"	12'-0"
E	NOMINAL GATE HEIGHT	10'-0"	10'-0"	10'-0"	10'-0"	10'-0"
F	POST HEIGHT	11'-6"	11'-6"	11'-6"	11'-6"	11'-6"
G	FABRIC HEIGHT	9'-0"	9'-0"	9'-0"	9'-0"	9'-0"

UL 235 COMPLIANCE NOTES

GATE INSTALLATION SHALL COMPLY WITH ALL REQUIREMENTS OF UL 235, INCLUDING, BUT NOT LIMITED TO:

- ALL OPENINGS OF THE SLIDE GATE ARE GUARDED OR SCREENED FROM THE BOTTOM OF THE GATE TO A MINIMUM OF 4 FEET ABOVE GROUND TO PREVENT A 2-1/4" DIAMETER SPHERE FROM PASSING THROUGH THE OPENINGS ANYWHERE IN THE GATE, AND IN THAT PORTION OF THE ADJACENT FENCE THAT THE GATE COVERS IN THE OPEN POSITION.
- ALL EXPOSED PINCH POINTS ARE ELIMINATED OR GUARDED AND GUARDING IS SUPPLIED FOR ALL EXPOSED ROLLERS.
- A WARNING SIGN (SEE DETAIL, THIS SHEET) MUST BE AFFIXED TO BOTH SIDES OF THE SLIDE GATE, BOTH MANUAL AND ELECTRIC.

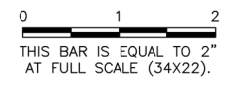


WARNING SIGN DETAIL
N.T.S.

IL. CONTRACT: **KA049**
IL. LETTING ITEM: **02A**
IL. PROJECT: **IKK-4707**
S.B.G. PROJECT: **3-17-SBGP-TBD**

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KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING

CANTILEVER GATE DETAILS

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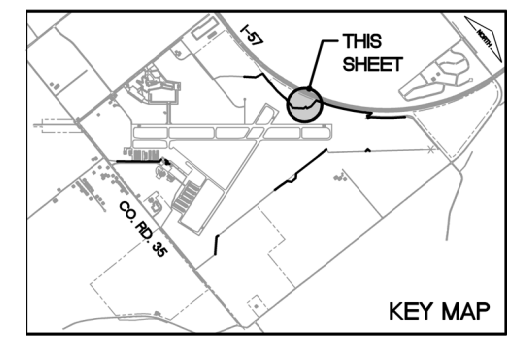
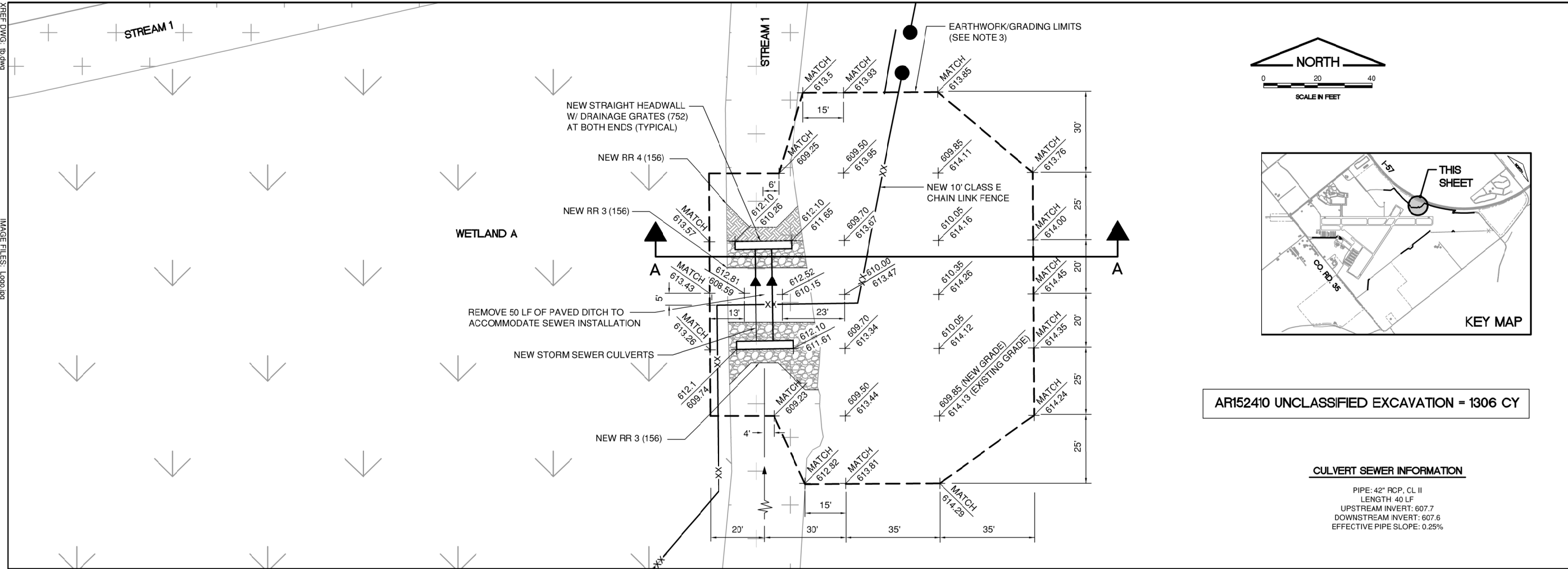
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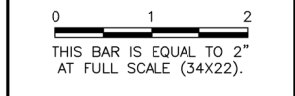
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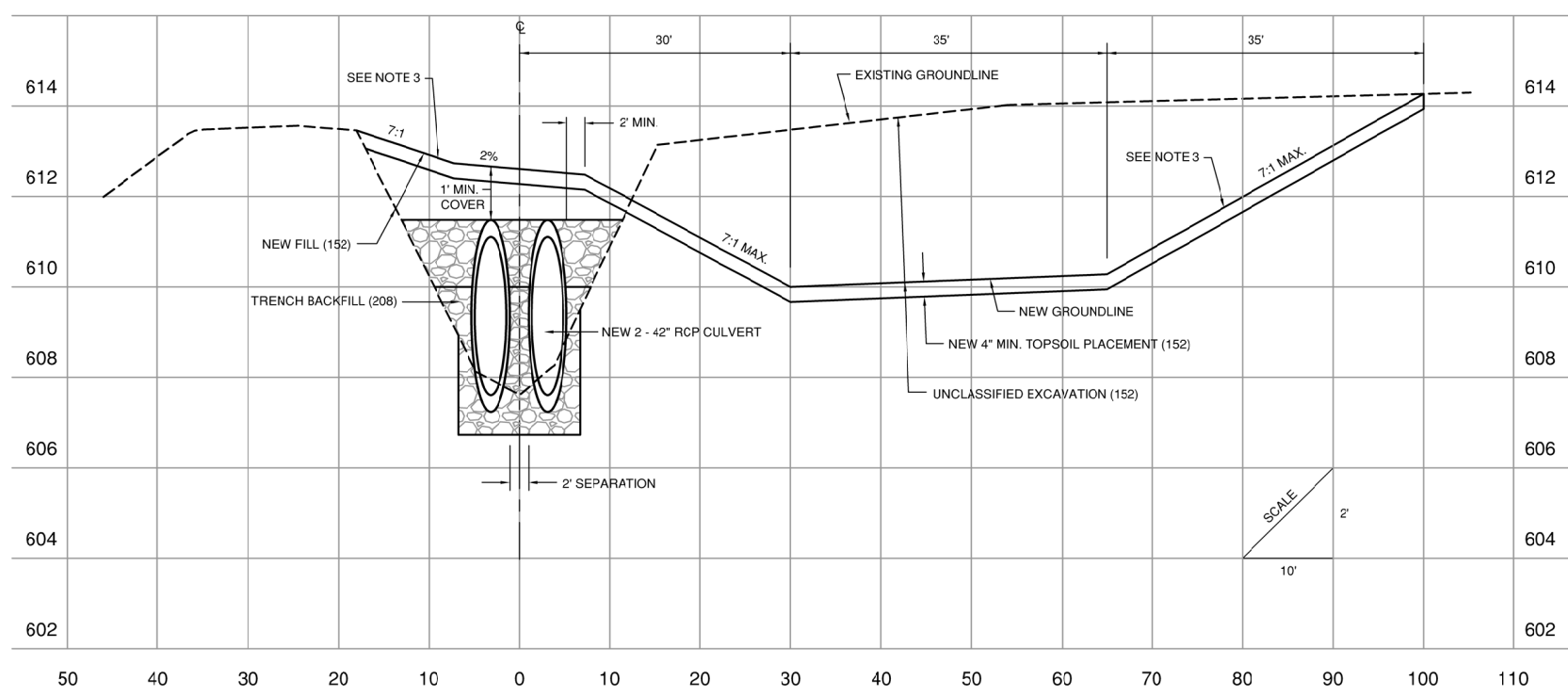
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 IL PROJECT: **IKK-4707**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

NUMBER	BY	DATE



GENERAL NOTES

- ALL CONSTRUCTION WITHIN THE WATERWAY SHALL BE CONDUCTED DURING ZERO OR LOW FLOW CONDITIONS.
- ALL EXCESS EXCAVATION SHALL BE DISPOSED OF OFF AIRPORT PROPERTY. COST INCIDENTAL TO THE UNCLASSIFIED EXCAVATION PAY ITEM.
- THE LIMITS OF THE NEW GROUNDLINE SHALL HAVE A MINIMUM OF 4 INCHES OF TOPSOIL (152) FE-SPREAD (COST INCIDENTAL TO UNCLASSIFIED EXCAVATION PAY ITEM), THE TOPSOIL SHALL BE SEEDED (901) AND BLANKETED (156).
- CONTRACTOR'S METHOD'S OF DE-WATERING, DIVERTING OR BLOCKING OF WATER FLOW SHALL BE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT PRIOR TO IMPLEMENTATION. CONTRACTOR SHALL INITIATE BMP'S IN COMPLIANCE WITH THE ILLINOIS URBAN MANUAL. ALL COSTS INCIDENTAL TO STORM SEWER PAY ITEM.
- UNCLASSIFIED EXCAVATION AND TOPSOIL STRIPPING QUANTITIES IS CALCULATED BASED ON THE MATERIAL IN ITS INITIAL POSITION AND QUANTIFIED BY METHOD OF AVERAGE END AREAS. NO CHANGES IN PAYMENT WILL BE MADE FOR ANY VARIATIONS IN MATERIAL VOLUME AFTER EXCAVATION DUE TO MATERIAL SHRINKAGE OR SWELLING.
- TOPSOIL STRIPPING SHALL BE PAID FOR AS UNCLASSIFIED EXCAVATION.
- TOPSOIL PLACEMENT AND FILL IS INCIDENTAL TO UNCLASSIFIED EXCAVATION.
- ALL FILL MATERIAL SHALL BE COMPACTED TO THE SATISFACTION OF THE RESIDENT ENGINEER.



**KANKAKEE VALLEY AIRPORT AUTHORITY
 KANKAKEE, ILLINOIS
 PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING
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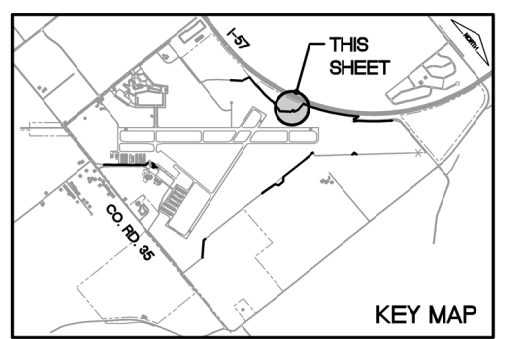
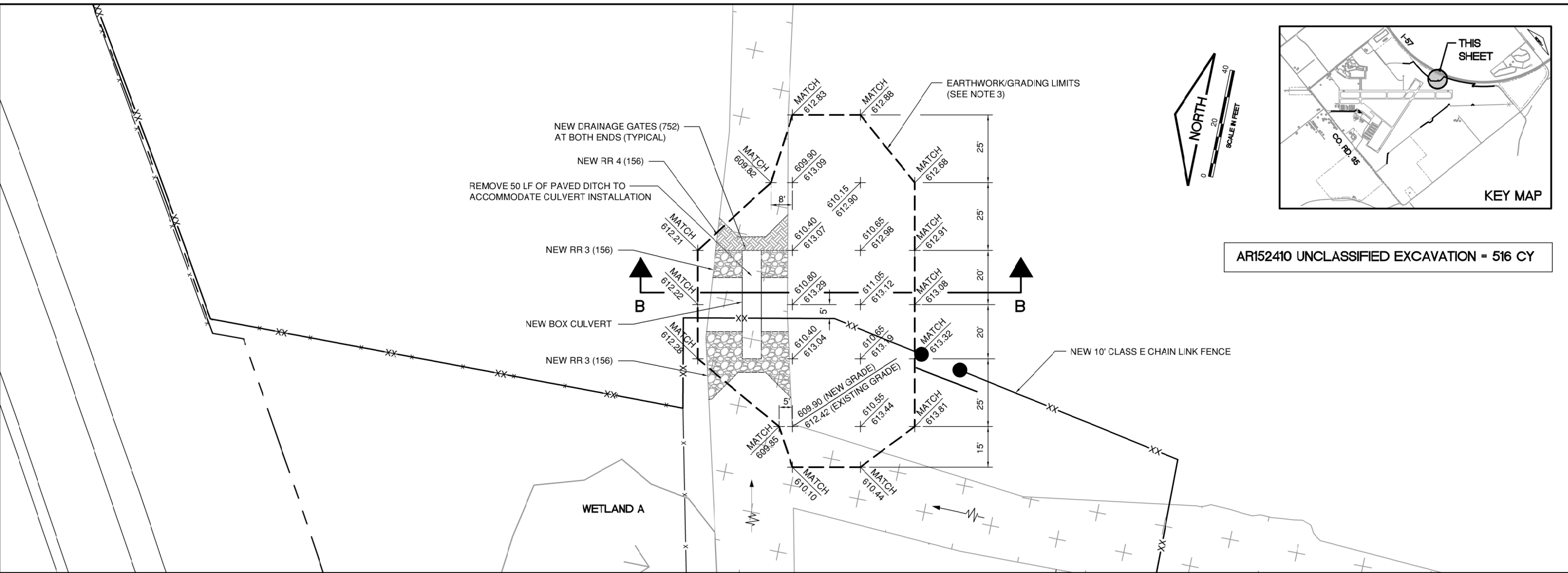
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UPDATE BY: Jim Chee
LAYOUT: 1
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ARI52410 UNCLASSIFIED EXCAVATION = 516 CY

IL CONTRACT: KA049
IL LETTING ITEM: 02A
IL PROJECT: IKK-4707
S.B.G. PROJECT: 3-17-SBGP-TBD

REVISIONS		
NUMBER	BY	DATE

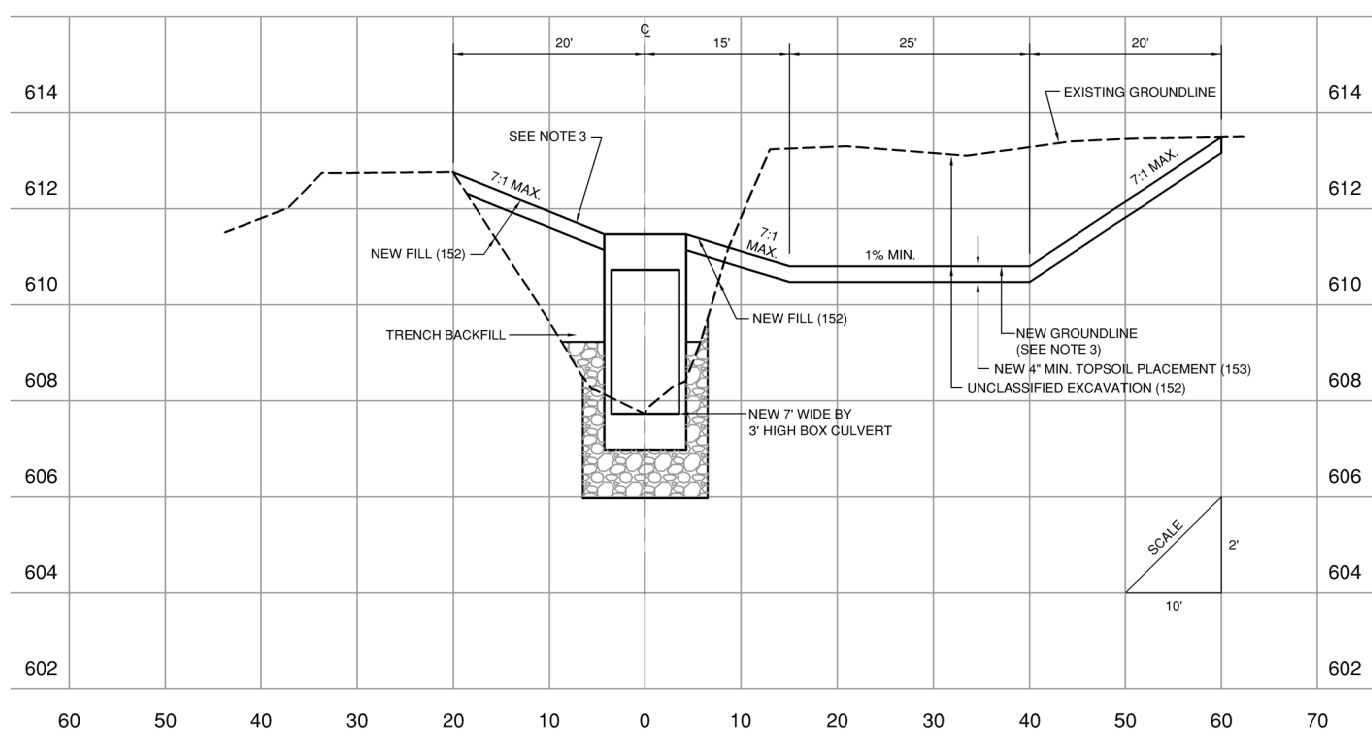
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THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING

DRAINAGE IMPROVEMENTS 2

GENERAL NOTES

1. ALL CONSTRUCTION WITHIN THE WATERWAY SHALL BE CONDUCTED DURING ZERO OR LOW FLOW CONDITIONS.
2. ALL EXCESS EXCAVATION SHALL BE DISPOSED OF OFF AIRPORT PROPERTY. COST INCIDENTAL TO THE UNCLASSIFIED EXCAVATION PAY ITEM.
3. THE LIMITS OF THE NEW GROUNDLINE SHALL HAVE A MINIMUM OF 4 INCHES OF TOPSOIL (152) RE-SPREAD (COST INCIDENTAL TO UNCLASSIFIED EXCAVATION PAY ITEM). THE TOPSOIL SHALL BE SEEDED (901) AND BLANKETED (156).
4. CONTRACTOR'S METHOD'S OF DE-WATERING, DIVERTING OR BLOCKING OF WATER FLOW SHALL BE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT PRIOR TO IMPLEMENTATION. CONTRACTOR SHALL INITIATE BMPs IN COMPLIANCE WITH THE ILLINOIS URBAN MANUAL. ALL COSTS INCIDENTAL TO BOX CULVERT PAY ITEM.
5. SEE NOTES 5 - 8 ON DRAINAGE IMPROVEMENTS 1 SHEET.



CULVERT SEWER INFORMATION

BOX CULVERT: 7' x 3'
LENGTH: 40 LF
UPSTREAM INVERT: 607.65
DOWNSTREAM INVERT: 607.55
EFFECTIVE BOX SLOPE: 0.25%
CULVERT LOADING: H20
CULVERT COVER: 0'

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KVA
KANKAKEE VALLEY AIRPORT AUTHORITY

DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	ARM
DATE:	6/7/19
JOB No:	16075-02

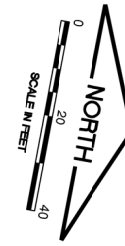
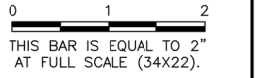
FINAL

SHEET 17 OF 19 SHEETS

SURVEY BOOK # BOOK #

REVISIONS

NUMBER	BY	DATE

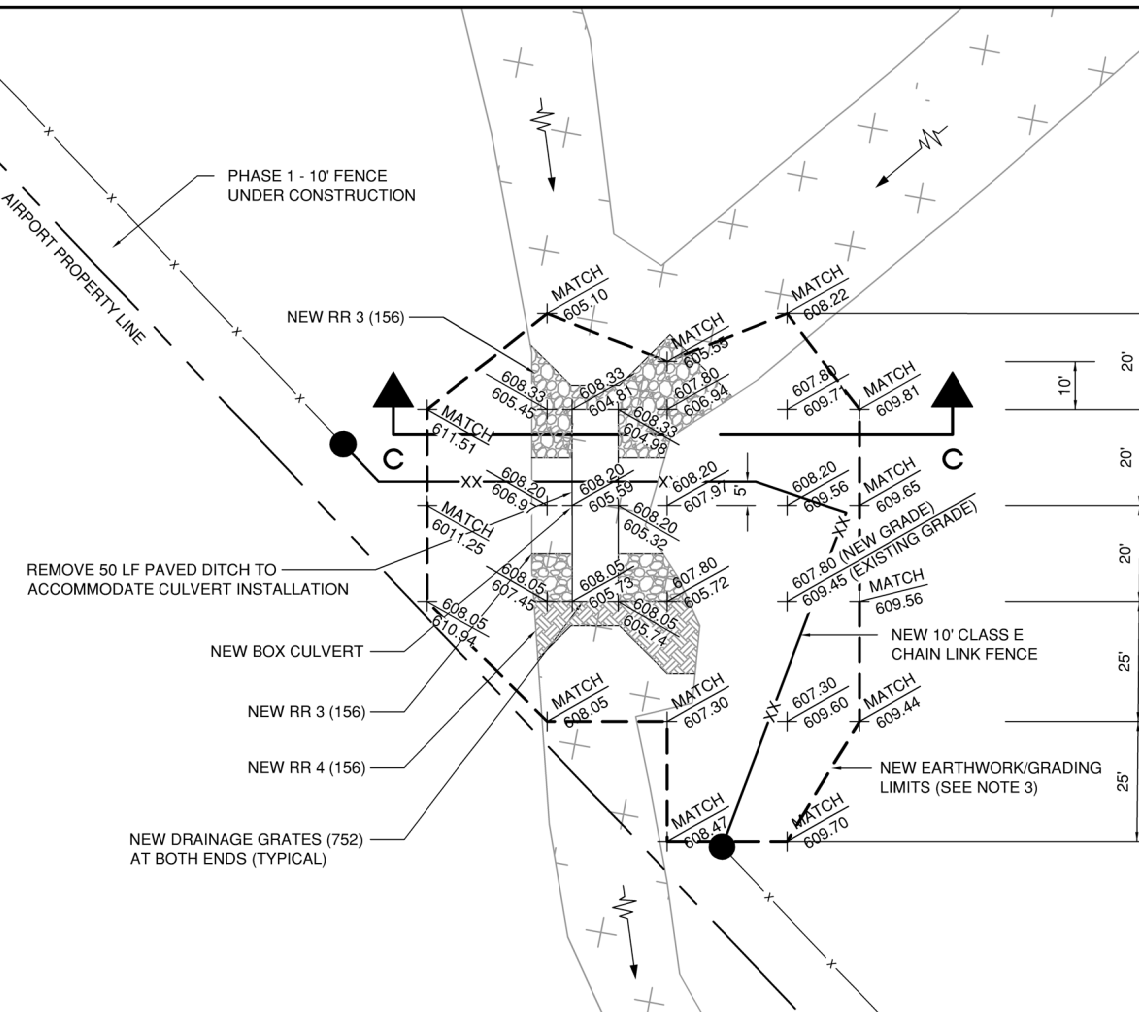
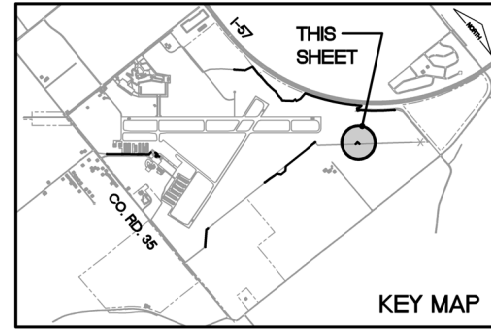


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IMAGE FILES: Logo.jpg

UPDATE BY: Jim Chae
LAYOUT: 3

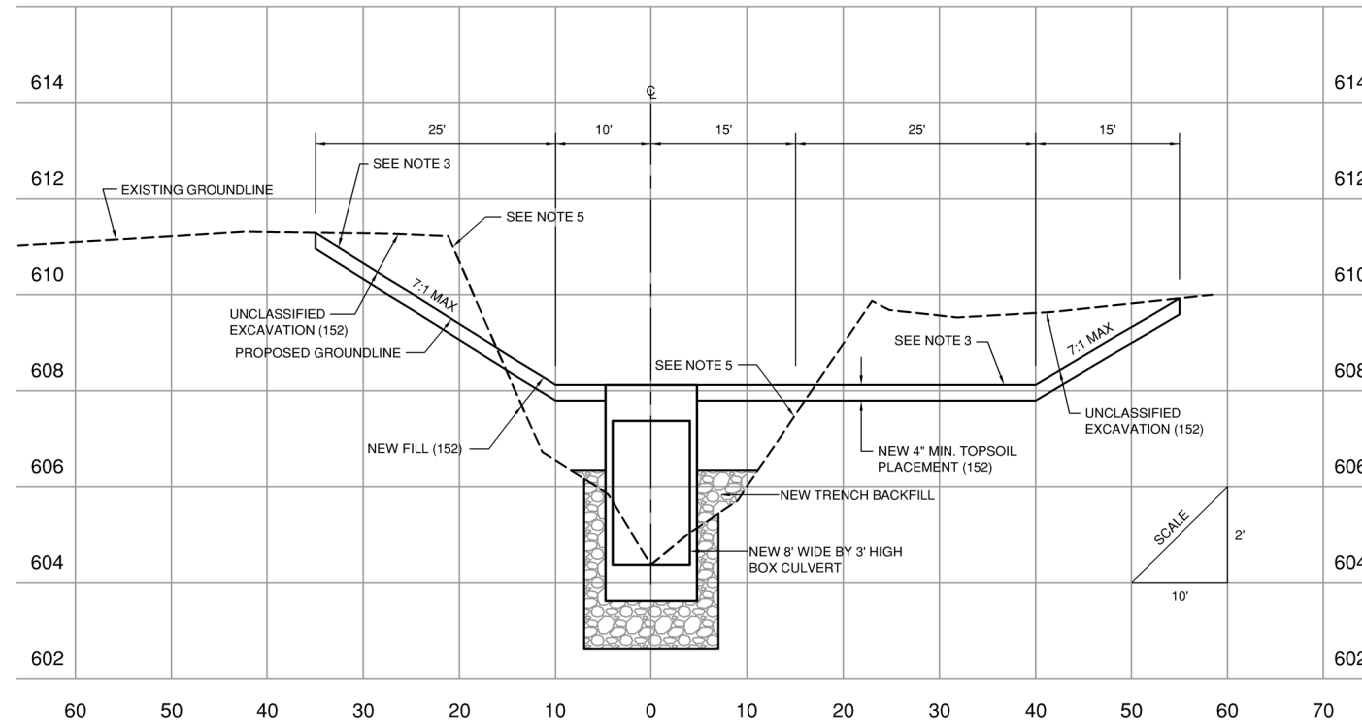
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AR152410 UNCLASSIFIED EXCAVATION = 249 CY

GENERAL NOTES

1. ALL CONSTRUCTION WITHIN THE WATERWAY SHALL BE CONDUCTED DURING ZERO OR LOW FLOW CONDITIONS.
2. ALL EXCESS EXCAVATION SHALL BE DISPOSED OF OFF AIRPORT PROPERTY. COST INCIDENTAL TO THE UNCLASSIFIED EXCAVATION PAY ITEM.
3. THE LIMITS OF THE NEW GROUNDLINE SHALL HAVE A MINIMUM OF 4 INCHES OF TOPSOIL (152) RE-SPREAD. (COST INCIDENTAL TO UNCLASSIFIED EXCAVATION PAY ITEM. THE TOPSOIL SHALL BE SEEDED (901) AND BLANKETED (156).
4. CONTRACTOR'S METHODS OF DE-WATERING, DIVERTING OR BLOCKING OF WATER FLOW SHALL BE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT PRIOR TO IMPLEMENTATION. CONTRACTOR SHALL INITIATE BMPs IN COMPLIANCE WITH THE ILLINOIS URBAN MANUAL. ALL COSTS INCIDENTAL TO BOX CULVERT PAY ITEM.
5. EXISTING DITCH BANKS ARE LINED WITH RIP RAP AND OVER GROWN WITH GFASSES. NO ADDITIONAL SEPARATE MEASUREMENTS WILL BE MADE FOR EXCAVATION AND DISPOSAL. ALL COSTS SHALL BE INCIDENTAL TO THE UNCLASSIFIED EXCAVATION PAY ITEM.
6. SEE NOTES 5 - 8 ON DRAINAGE IMPROVEMENTS 1 SHEET.



TYPICAL - CROSS SECTION C

CULVERT SEWER INFORMATION

BOX CULVERT: 8' x 3'
 LENGTH: 40 LF
 UPSTREAM INVERT: 604.4
 DOWNSTREAM INVERT: 604.2
 EFFECTIVE BOX SLOPE: 0.50%
 CULVERT LOADING: H20
 CULVERT COVER: 0'

KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS

PHASE 2 - INSTALL SECURITY/WILDLIFE 10' FENCING

DRAINAGE IMPROVEMENTS 3

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DESIGN BY: ARM

DRAWN BY: JRO

CHECKED BY: DKP

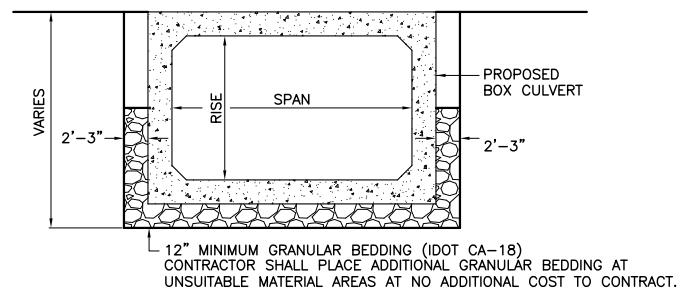
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DATE: 6/7/19

JOB No: 160375-02

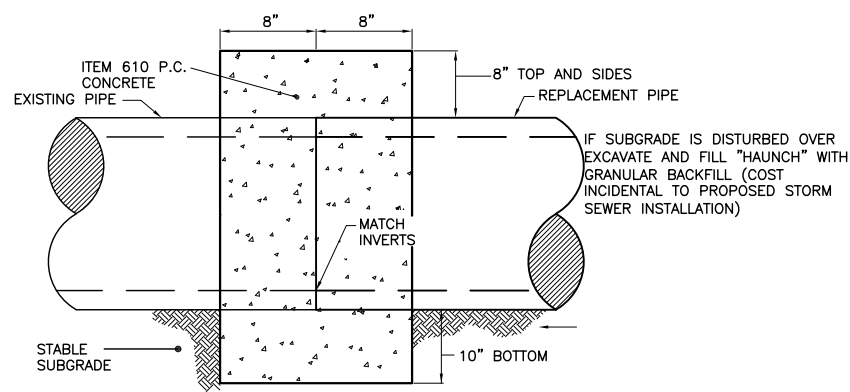
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BOX CULVERT TRENCH DETAIL

NOT TO SCALE

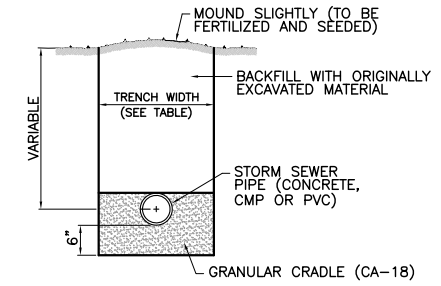


CONCRETE COLLAR - FIELD TILE

NOT TO SCALE

NOTE: COST INCIDENTAL TO CONTRACT.

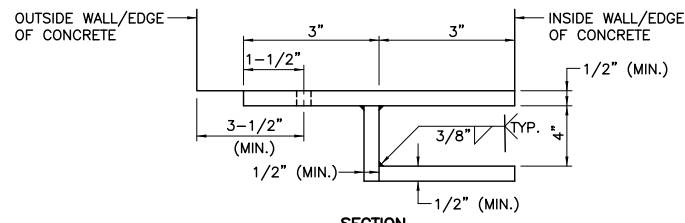
INSIDE DIAMETER OF STORM SEWER (INCHES)	MAXIMUM TRENCH WIDTH	MAXIMUM PAVEMENT REMOVAL WIDTH
6	3'-7"	5'-7"
8	3'-9"	5'-9"
12	4'-2"	6'-2"
15	4'-6"	6'-6"
18	4'-9"	6'-9"
21	5'-0"	7'-0"
24	5'-4"	7'-4"
27	5'-7"	7'-7"
30	5'-11"	7'-11"
36	6'-6"	8'-6"
42	7'-1"	9'-1"
48	7'-8"	9'-8"
54	8'-3"	10'-3"
60	8'-10"	10'-10"
66	9'-5"	11'-5"
72	10'-0"	12'-0"
78	10'-7"	12'-7"
84	11'-2"	13'-2"
90	11'-9"	13'-9"
96	12'-4"	14'-4"
102	12'-11"	14'-11"
108	13'-6"	15'-6"



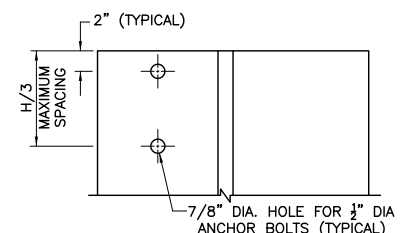
NON-PAVED AREAS

TRENCH DETAILS

NOT TO SCALE



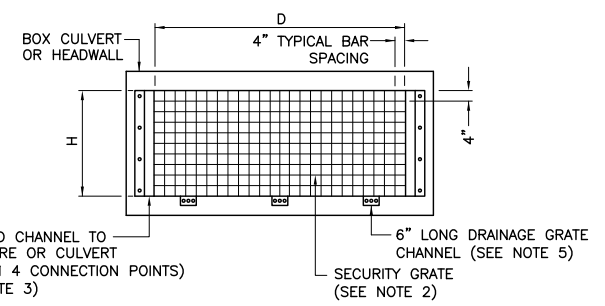
SECTION



PLAN

DRAINAGE GRATE CHANNEL

NOT TO SCALE



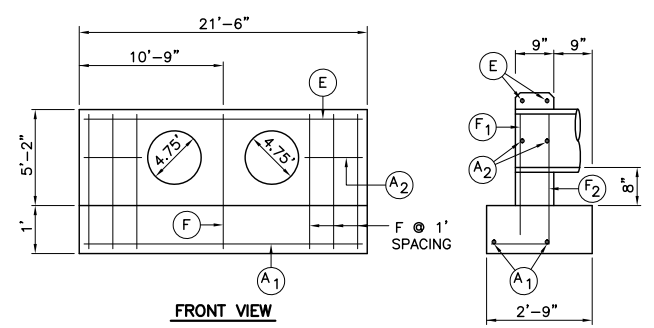
DRAINAGE GRATES

NOT TO SCALE

NOTES

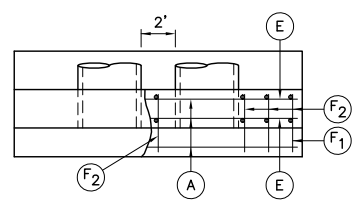
- CHANNEL PLATES SHALL BE GALVANIZED AND IN COMPLIANCE WITH ASTM A36.
- SECURITY GRATE BARS ARE #5 DEFORMED BARS MEETING ASTM A706, GR. 60. GRATE BARS SHALL BE WELDED TO EACH OTHER TO FORM THE SECURITY GRATE AND GALVANIZED. TYPICAL BAR SPACING IS 4". OUTSIDE SPACE TO BE ADJUSTED FOR CHANNEL SPACING.
- ANCHOR BOLTS SHALL BE 1/2" x 3-3/4" STAINLESS STEEL IN ACCORDANCE WITH ASTM F 1554, GR. 36.
- GRATES SHALL BE INSTALLED AT THE BOTH ENDS OF THE PIPE/CULVERT.
- BOX CULVERT SHALL HAVE A MINIMUM OF 3 - 6" LONG CHANNELS AT BOTTOM OF CULVERT TO HOLD GRATE IN PLACE. A MINIMUM OF 1 SHALL BE INSTALLED FOR EACH RCP.

CULVERT	D	H	NO. OF GRATES
42" RCP	52"	52"	4
7' x 3'	92"	44"	2
8' x 3'	104"	44"	2



FRONT VIEW

SIDE VIEW



BAR F2

CONCRETE HEADWALL

NOT TO SCALE

NOTES

- GRADE 60 REINFORCED STEEL.
- 610 CONCRETE.
- 2" MINIMUM CONCRETE COVER.
- HEADWALL SHALL HAVE A MINIMUM 12" GRANULAR BEDDING (IDOT CA-16)(COST INCIDENTAL HEADWALL PAY ITEM).

TABLE OF REINFORCING STEEL

BAR	SIZE	SPA.	NO. 2
A1	#5	-	2
A2	#5	1'-6"	-
E	#5	-	2
F	#5	1'-0"	-

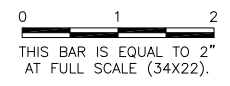
BOX CULVERT NOTES

- ALL PRECAST BOX CULVERTS SHALL BE DESIGNED IN ACCORDANCE WITH ASTM C1433 OR ASTM C1577, HS-20 LOADING, WITH 0- FEET OF COVER..
- PRIOR TO BACKFILLING, FILL SOLID THE 3" JOINT BETWEEN BARRELS WITH CONCRETE (610) USING A MAXIMUM SIZE AGGREGATE OF 3/8 INCH.
- GROUT ALL LIFTING HOLES BEFORE BACKFILLING, ALLOW GROUT TO ACHIEVE MINIMUM STRENGTH BEFORE BACKFILLING.
- DURING BACKFILL PLACEMENT, DO NOT PERMIT A DIFFERENCE IN FILL ELEVATION ON THE WALLS OF THE CULVERT IN EXCESS OF 2 FEET. DURING COMPACTION, DO NOT ALLOW WHEELS OF ROLLERS TO COME CLOSER THAN ONE FOOT TO THE FACE OF THE STRUCTURE.
- JOINT MATERIAL BETWEEN PRECAST SECTIONS SHALL CONFORM TO SECTION 1055 OR SECTION 1056 OF IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED APRIL 1, 2016.
- ALL BOX CULVERT END SECTIONS SHALL HAVE A PRECAST OR CAST-IN-PLACE TOEWALL.
- ANY ACCUMULATED DEBRIS WITHIN THE PROJECT AS A RESULT OF THE IMPROVEMENT SHALL BE REMOVED AND DISPOSED OF SATISFACTORILY BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. THE COST OF THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF DRAINAGE AT ALL TIMES DURING THE CONSTRUCTION OF THE CULVERT. THE METHODS USED BY THE CONTRACTOR SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER. THE COST FOR THIS REQUIREMENT WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE JOINTS SHALL BE EXTERNALLY SEALED ON ALL FOUR SIDES USING A 13 INCH WIDE EXTERNAL SEALING BANDS CONFORMING TO ASTM C877. PRIOR TO APPLYING THE SEALING BAND, THE CONCRETE SURFACE SHALL BE CLEANED TO REMOVE DIRT OR LAITANCE AND ALLOWED TO DRY. THE SEAL SHALL BE CENTERED OVER THE JOINT AND SECURED TO REMAIN IN PLACE DURING THE BACKFILLING OPERATION.

IL. CONTRACT: **KA049**
 IL. LETTING ITEM: **02A**
 IL. PROJECT: **IKK-4707**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS
PHASE 2 - INSTALL SECURITY/WDLIFE 10' FENCING

DRAINAGE DETAILS

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DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	ARM
DATE:	6/7/19
JOB No:	160075-02

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