

FED. RFD. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1653	22-00020-00-RS	LAKE	31	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 61K49		

FOR INDEX OF SHEETS, SEE SHEET NO. 2

08-02-2024 LETTING ITEM 002

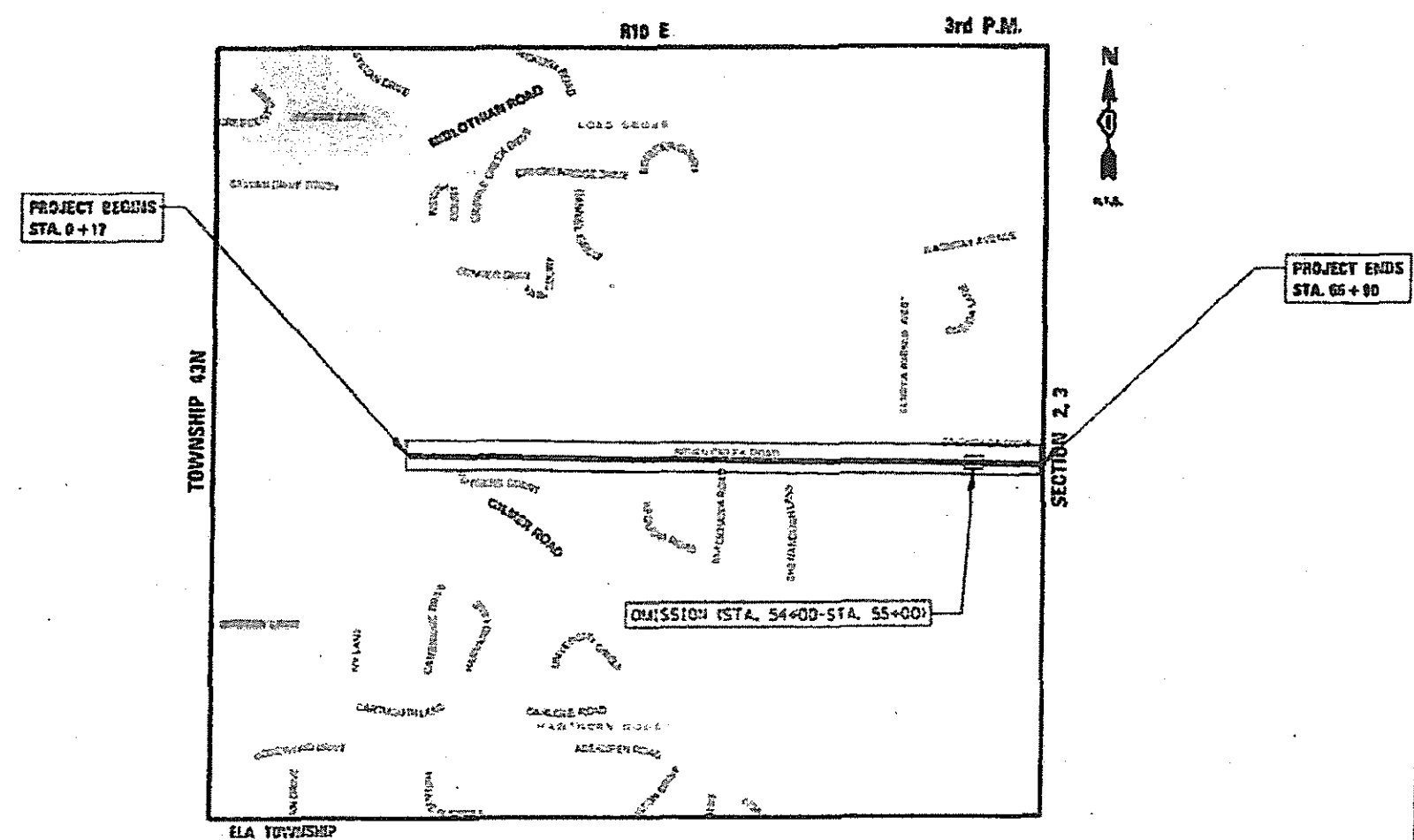
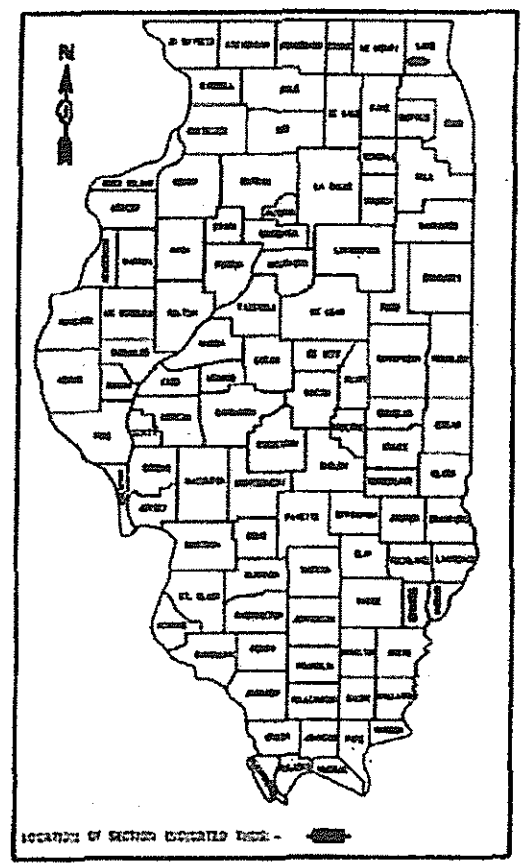
TRAFFIC DATA

INDIAN CREEK ROAD
 ADT (YEAR) = 3,100 (2018)
 POSTED SPEED LIMIT = 30 MPH

DESIGN DESIGNATION:
 MAJOR COLLECTOR

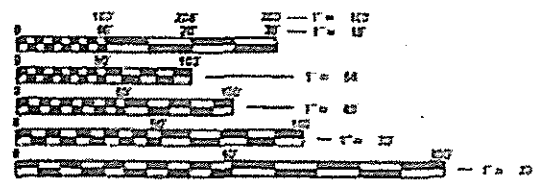
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
**PLANS FOR PROPOSED
 FEDERAL AID HIGHWAY**

FAU ROUTE 1653 (INDIAN CREEK ROAD)
 GILMER ROAD TO E VILLAGE LIMITS
 LAFO RESURFACING
 SECTION NO. 22-00020-00-RS
 PROJECT NO. MW87(638)
 VILLAGE OF HAWTHORN WOODS
 LAKE COUNTY
 C-91-213-24



LOCATION MAP

INDIAN CREEK ROAD
 GROSS LENGTH OF PROJECT = 6,673 LINEAL FEET (1.26 MILES)
 NET LENGTH OF PROJECT = 6,573 LINEAL FEET (1.24 MILES)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

JULIE
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 8-1-1 03 1-800-332-0123

CHRISTOPHER B. BURKE ENGINEERING LTD.
 9575 West Higgins Road, Suite 600
 Rosemont, Illinois 60018 (847) 823-0500

PROFESSIONAL DESIGN FIRM NO. 104-001173
 EXPIRATION DATE: 04/30/25

CONTRACT NO. 61K49

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E., SCHALNBURG, IL

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

APPROVED 06/04/2024
[Signature]
 DIRECTOR OF PUBLIC WORKS (VILLAGE ENGINEER)
 VILLAGE OF HAWTHORN WOODS

APPROVED 6/1/2024
[Signature]
 HIGHWAY SUPERINTENDENT
 ELA TOWNSHIP

PASSED June 5 2024
[Signature]
 DISTRICT ENGINEER (LOCAL ENGINEER)
 DISTRICT 1

RELEASING FOR BID BASED ON LIMITED REVIEW June 6th 2024
[Signature]
 REGIONAL ENGINEER

LEE M. FELL
 ILLINOIS REGISTRATION NO. 062-053703
 EXPIRATION DATE: 11/30/2025

4/5/24
 DATE

**PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

SHEET NO.	DESCRIPTION	GENERAL NOTES
1	COVER SHEET	
2	GENERAL NOTES AND HIGHWAY STANDARDS	
3-4	SUMMARY OF QUANTITIES	
5	TYPICAL SECTIONS	
6-11	EXISTING CONDITIONS AND REMOVAL PLAN	
12-17	PROPOSED PLAN	
18-23	PAVEMENT MARKING PLAN	
24	CONSTRUCTION DETAILS	
25	FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8)	
26	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)	
26A	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)	
27	BUTT JOINT AND HMA TAPER DETAILS (BD-32)	
28	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)	
28A	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11)	
29	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)	
30	SHORT TERM PAVEMENT MARKINGS AND SYMBOLS (TC-16)	
31	ARTERIAL ROAD INFORMATION SIGN (TC-22)	
HIGHWAY STANDARDS		
000001-08	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS	
420001-10	PAVEMENT JOINTS	
483001-06	PCC SHOULDER	
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER	
701006-05	OFF ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE	
701011-04	OFF ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY	
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS	
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS, DAY ONLY	
701501-06	URBAN LANE CLOSURE, 2L, 2W, - UNDIVIDED	
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION	
701901-09	TRAFFIC CONTROL DEVICES	
780001-05	TYPICAL PAVEMENT MARKINGS	
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS	
DISTRICT ONE DETAILS		
BD-08	FRAMES AND LIDS ADJUSTMENT WITH MILLING	
BD-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	
BD-24	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	
BD-32	BUTT JOINT AND HMA TAPER DETAILS	
TC-10	TRAFFIC CONTROL FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	
TC-11	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)	
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS	
TC-16	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS	
TC-22	ARTERIAL ROAD INFORMATION SIGN	

GENERAL NOTES

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2022; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2024; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JUNE 2014 SEVENTH EDITION, THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT HIGHWAY STANDARD.

CODES OF THE IEPA TITLE 35, AND O.S.M.A. SHALL BE ADHERED TO FOR THE CONSTRUCTION OF THIS PROJECT.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

ALL REQUIRED PERMITS FROM THE PROPER GOVERNING AGENCY SHALL BE OBTAINED FOR CONSTRUCTION ALONG OR ACROSS EXISTING STREETS OR HIGHWAYS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHEETING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE AGENCY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SIGNS AND WARNING DEVICES TO INFORM AND PROTECT THE PUBLIC.

UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITY FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.

WATER, STORM SEWER AND SANITARY SEWER

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT THE VILLAGE OF HAWTHORN WOODS (TEL. 847-438-5500) AND AQUA ILLINOIS (TEL. 877-987-2782) FOR THEM TO TURN VALVES OR OPERATE HYDRANTS. ANY UNAUTHORIZED USE OF THIS EQUIPMENT THE OFFENDER WILL BE SUBJECT TO ARREST AND PROSECUTION.

MISCELLANEOUS

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, DRIVEWAY REMOVAL AND REPLACEMENT, AND STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH ARTICLE 107.25 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES.

FRESH OIL SIGNS SHALL BE POSTED AT BOTH ENDS OF THE ROADWAY AND ALL SIDE STREETS AS DIRECTED BY THE ENGINEER. CONSTRUCTION AHEAD SIGNS SHALL BE PLACED AT ALL SIDE STREETS AND BOTH ENDS OF THE ROADWAY WHILE CONSTRUCTION IS IN PROGRESS.

CONTRACTOR SHALL NOT PLACE TOPSOIL & SEEDING UNTIL THE TEMPERATURE IS 80 OR LESS AND THE FORECAST FOR THE NEXT 7 DAYS SHOWS TEMPERATURES OF 80 OR LESS.

NO CONSTRUCTION SHALL BEGIN UNTIL ALL PROPER TEMPORARY SIGNS AND BARRICADES HAVE BEEN INSTALLED.

ALL ROADS MUST HAVE ONLY ONE LONGITUDINAL JOINT WHILE PAVING.

VANDALISM - SPECIAL ATTENTION IS CALLED TO THE SPECIAL PROVISION FOR "INSPECTION" AS WELL AS ARTICLE 107.30 OF THE "STANDARD SPECIFICATIONS." ANY DEFACED WORK AS DETERMINED AND DIRECTED BY THE VILLAGE SHALL BE CORRECTED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THE VILLAGE OF HAWTHORN WOODS WILL COOPERATE WITH THE CONTRACTOR TO MINIMIZE VANDALISM, BUT THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE TO CORRECT ANY DAMAGE. THE VILLAGE WILL NOT BE RESPONSIBLE FOR THE SECURITY OF THE WORK SITE IN THIS REGARD, OTHER THAN NORMAL PATROLLING AND RESPONSE TO EMERGENCIES. SECURITY REQUIRED TO MEET THIS SPECIAL PROVISION SHALL BE INCLUDED.

FLAGGERS SHALL BE PRESENT DURING SURFACE REMOVAL AS WELL AS RESURFACING ACTIVITIES.

FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDIAN CREEK ROAD GENERAL NOTES AND HIGHWAY STANDARDS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\HAWTHORNWOODS\230536\Civil\Not_230536.sht		DRAWN - JEH	REVISED -			1653	22-00020-00-R5	LAKE	31	2	
		CHECKED - LMF	REVISED -			CONTRACT NO. 61K49					
Default	PLOT DATE = 6/12/2024	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO
						ILLINOIS FED. AID PROJECT					

SUMMARY OF QUANTITIES

SPECIALTY ITEM	ITEM NO.	ITEM	UNIT	CONSTRUCTION TYPE CODE 0005 (80% FEDERAL / 20% LOCAL)	CONSTRUCTION TYPE CODE 0042 (80% FEDERAL / 20% LOCAL)
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	19	
	40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	713	
	40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	1425	
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	11368	
	44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	16841	
	44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	40	
	44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	40	
	~ 44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	40	
	44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	40	
	67100100	MOBILIZATION	LSUM	1	
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1	
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	
	70107025	CHANGEABLE MESSAGE SIGN	CAL DA	15	
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	2750	
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1375	
~	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	FOOT	75	

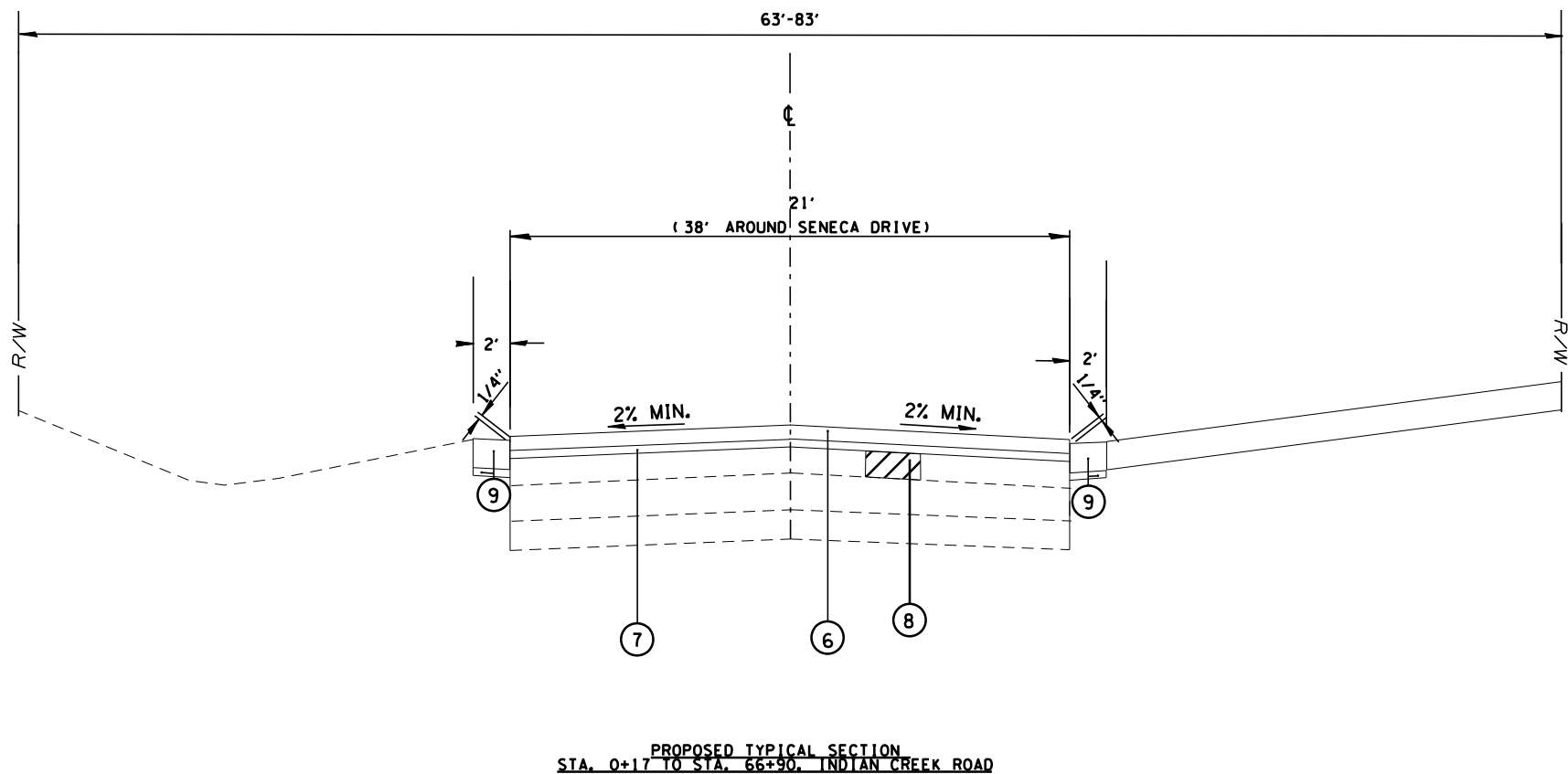
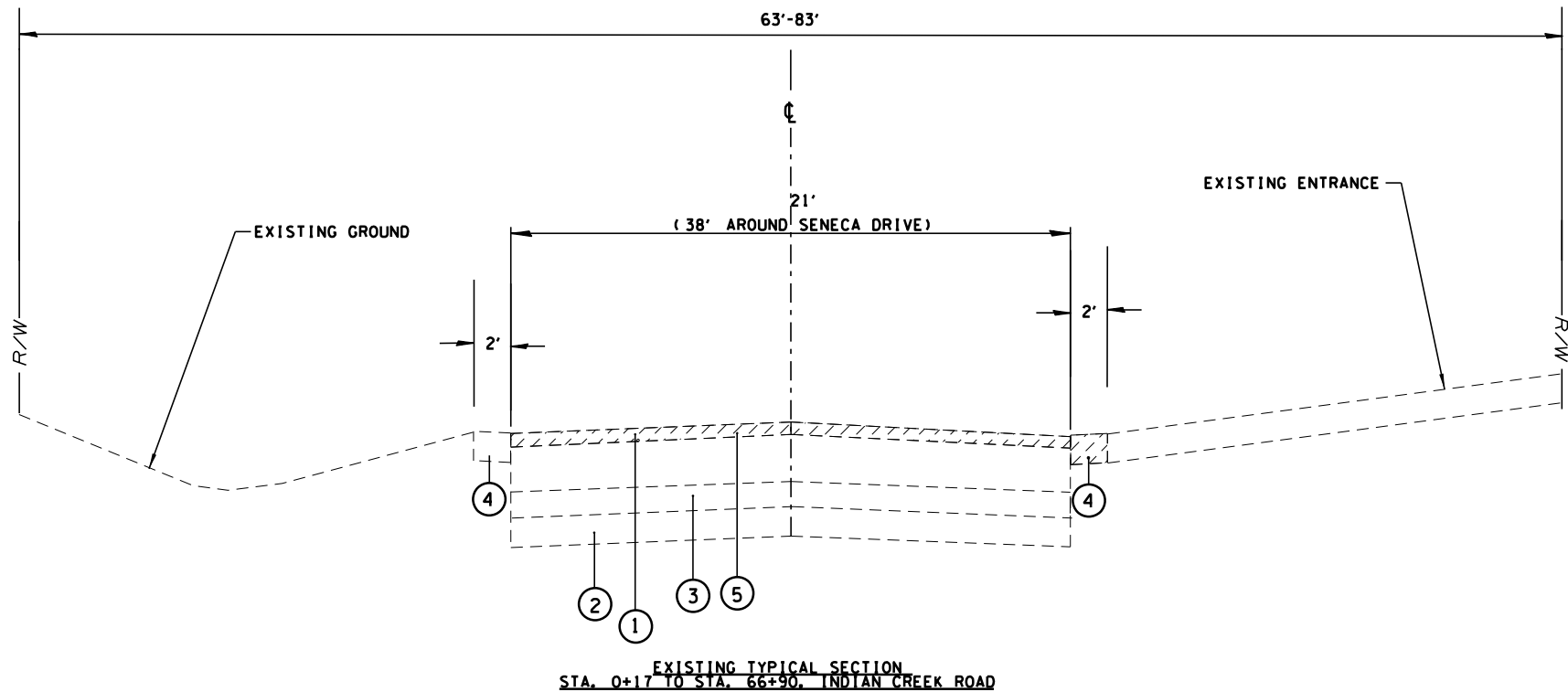
~ INDICATES SPECIALTY ITEM

FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDIAN CREEK ROAD SUMMARY OF QUANTITIES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\HAWTHORNWOODS\238536\Civil\50001.230	36.plt	DRAWN - JEH	REVISED -			1653	22-00020-00-RS	LAKE	31	3	
PLDT SCALE = 3/4"	CHECKED - LMF	REVISED -	REVISED -			CONTRACT NO. 61K49					
PLDT DATE = 5/29/2024	DATE -	REVISED -	REVISED -			SCALE:	SHEET 1 OF 2 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	

SPECIALTY ITEM	ITEM NO.	ITEM	UNIT	CONSTRUCTION TYPE CODE 0005 (80% FEDERAL / 20% LOCAL)	CONSTRUCTION TYPE CODE 0042 (80% FEDERAL / 20% LOCAL)
~	78000200	THERMOPLASTIC PAVEMENT MARKING - 4"	FOOT	12564	
~	78000400	THERMOPLASTIC PAVEMENT MARKING - 6"	FOOT	250	
~	78000650	THERMOPLASTIC PAVEMENT MARKING - 24"	FOOT	55	
~	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	354	
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	354	
	78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	50	
~	K1004595	PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE	LSUM	1	
	X4404264	SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)	FOOT	50	
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	2	
	Z0013798	CONSTRUCTION LAYOUT	LSUM	1	
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	120	

~ INDICATES SPECIALTY ITEM

FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDIAN CREEK ROAD SUMMARY OF QUANTITIES				F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\HAWT-ORHW005\230536\Civil\50002.23	536.sht	DRAWN - JEH	REVISED -		1653	22-00020-00-RS	LAKE	31	4				
Default	PLOT SCALE = 3/8"	CHECKED - LMF	REVISED -		SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.				CONTRACT NO. 61K49				
	PLOT DATE = 6/12/2024	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								



HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
RESURFACING ITEM	AIR Voids @ Nges	OMP
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 1.5"	4% @ 50 GYR.	LR 1030-2
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 0.75"	3.5% @ 50 GYR.	LR 1030-2
PATCHING ITEM		
CLASS D PATCHES, (HMA BINDER IL 19MM) 8 INCH	4% @ 70 GYR.	LR 1030-2
OMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (OC/QA) PER LR 1030-2		

MIXTURE REQUIREMENT NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE IS 112 LBS/SY/IN. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED RECLAIMED MATERIAL SPECIFICATIONS.

NOTES:

1. PAVING OF THE FULL ROADWAY WIDTH SHALL BE COMPLETED AT THE END OF EACH DAY OF PAVING TO PREVENT A LONGITUDINAL COLD JOINT FROM APPEARING WHEN OPPOSITE SIDES OF THE ROAD ARE PAVED ON DIFFERENT DAYS. THE CONTRACTOR SHALL ALSO ENSURE THAT AT THE END OF EACH DAY EACH PASS ENDS AT APPROXIMATELY THE SAME STATION TO PREVENT A COLD JOINT.
2. PAVEMENT PATCHING SHALL BE DONE AFTER ROADWAY MILLING AS PER BD-22 DETAIL.

LEGEND

- ① EXISTING HOT-MIX ASPHALT PAVEMENT (3"-11")
- ② EXISTING AGGREGATE SUBBASE
- ③ EXISTING AGGREGATE BASE
- ④ EXISTING CONCRETE RIBBON/CONCRETE RIBBON REMOVAL (AS DIRECTED BY THE ENGINEER)
- ⑤ HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- ⑥ HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 - 1.5"
- ⑦ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50- 0.75"
- ⑧ CLASS D PATCHES, 8 INCH (TYPE I-IV)
- ⑨ CONCRETE RIBBON (INCLUDING 4" GRANULAR SUBBASE)

FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -
N:\HAWTHORNWOODS\230536\Civil\typ\230536.sht		DRAWN - JEH	REVISED -
	PLOT SCALE = 1/42'	CHECKED - LMF	REVISED -
Default	PLOT DATE = 5/29/2024	DATE -	REVISED -



**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

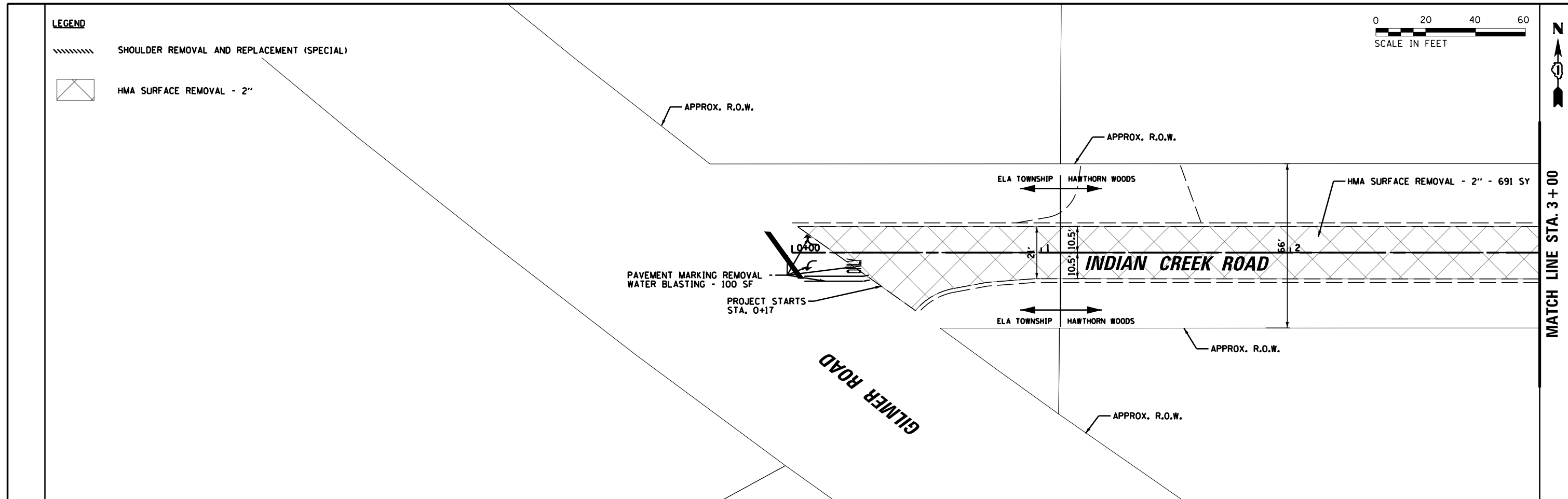
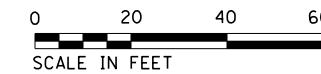
TYPICAL SECTIONS

SCALE: SHEET OF SHEETS STA. TO STA.

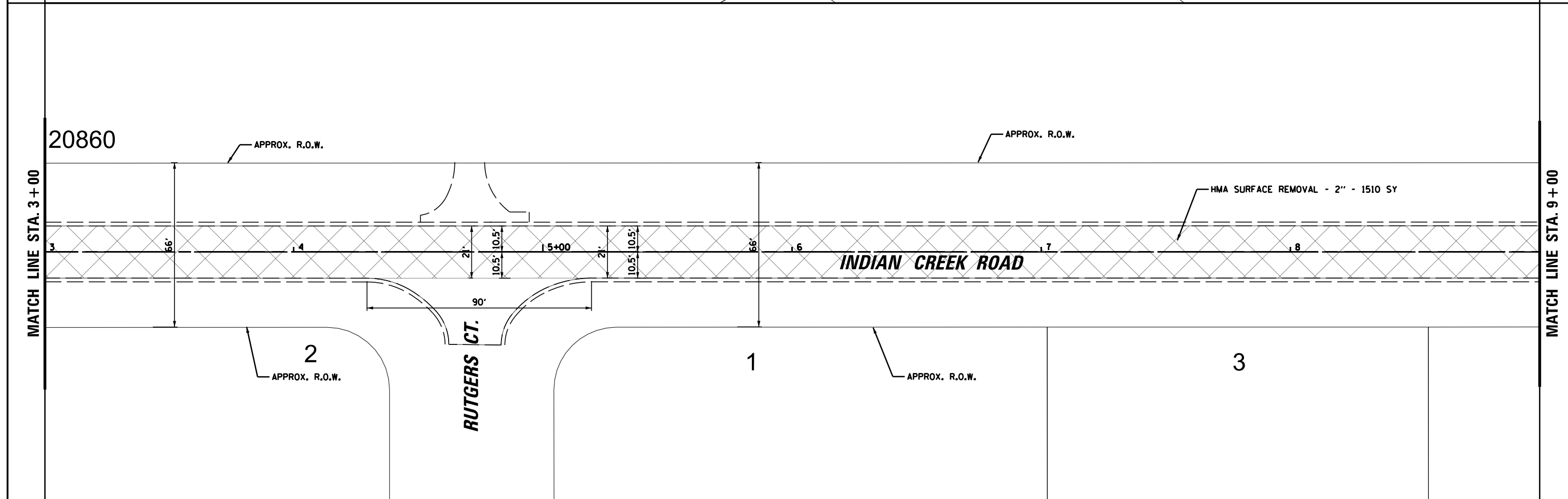
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1653	22-00020-00-R5	LAKE	31	5
CONTRACT NO. 61K49				
ILLINOIS FED. AID PROJECT				

LEGEND

- 
SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)
- 
HMA SURFACE REMOVAL - 2"



MATCH LINE STA. 3 + 00



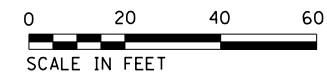
MATCH LINE STA. 9 + 00

FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING CONDITIONS AND REMOVAL PLAN	F.A.U. RTE. 1203	SECTION 22-00020-00-R5	COUNTY LAKE	TOTAL SHEETS 31	SHEET NO. 6		
Default		DRAWN - JEH	REVISED -			SCALE:	SHEET OF SHEETS	STA. TO STA.	CONTRACT NO. 61K49			
		CHECKED - LMF	REVISED -			ILLINOIS FED. AID PROJECT						
		DATE -	REVISED -									

LEGEND

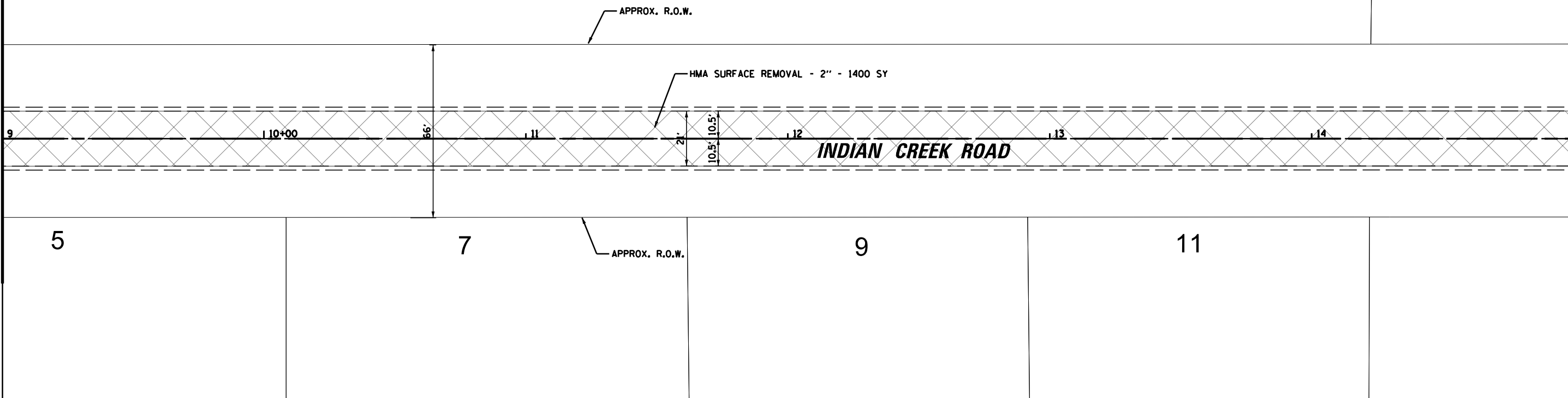
////// SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)

▣ HMA SURFACE REMOVAL - 2"



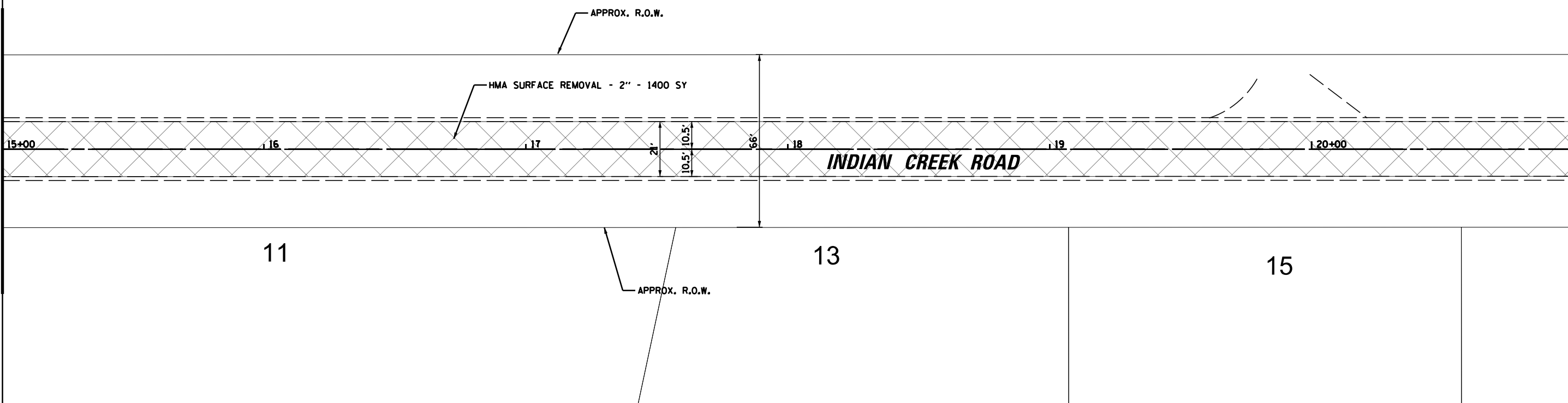
MATCH LINE STA. 9 + 00

MATCH LINE STA. 15 + 00



MATCH LINE STA. 15 + 00

MATCH LINE STA. 21 + 00



FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -
N:\HAWTHORNWOODS\230536\Civi1\REM02.230536.sht		DRAWN - JEH	REVISED -
Default	PLOT SCALE = 48"	CHECKED - LMF	REVISED -
	PLOT DATE = 5/29/2024	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EXISTING CONDITIONS AND REMOVAL PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1203	22-00020-00-R5	LAKE	31	7
CONTRACT NO. 61K49				
ILLINOIS FED. AID PROJECT				

LEGEND

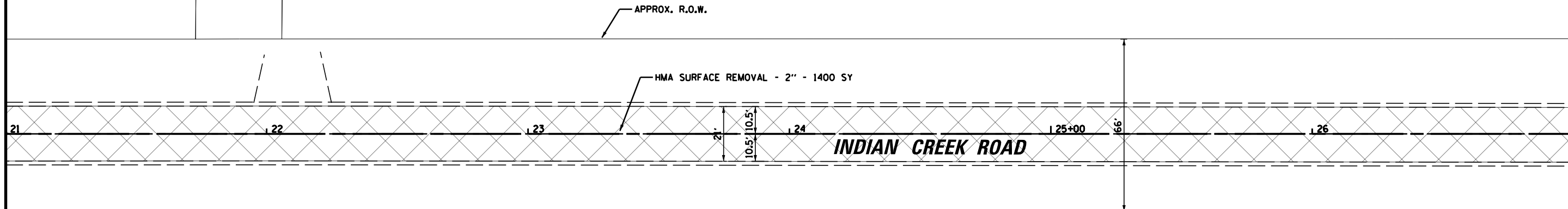
----- SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)

▨ HMA SURFACE REMOVAL - 2"



MATCH LINE STA. 21+00

MATCH LINE STA. 27+00



17

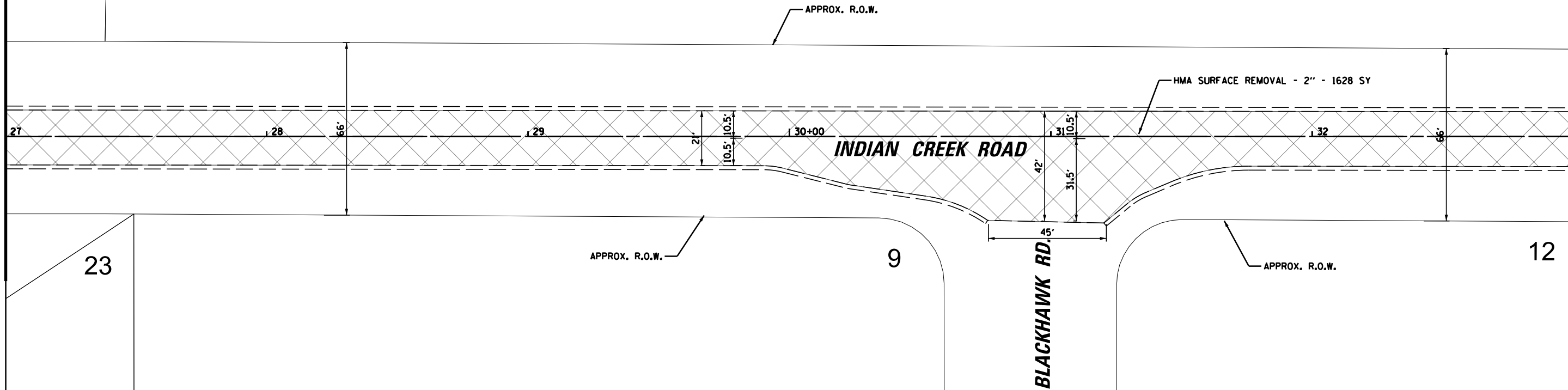
19

21

APPROX. R.O.W.

MATCH LINE STA. 27+00

MATCH LINE STA. 33+00



23

9

12

APPROX. R.O.W.

APPROX. R.O.W.

FILE NAME = N:\HAWTHORNWOODS\230536\Civil\REM03.230536.sht
Default

USER NAME = jhouseh
PLOT SCALE = 48"
PLOT DATE = 5/29/2024

DESIGNED - JEH
DRAWN - JEH
CHECKED - LMF
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING CONDITIONS AND REMOVAL PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1203	22-00020-00-R5	LAKE	31	8
CONTRACT NO. 61K49				
ILLINOIS FED. AID PROJECT				

LEGEND

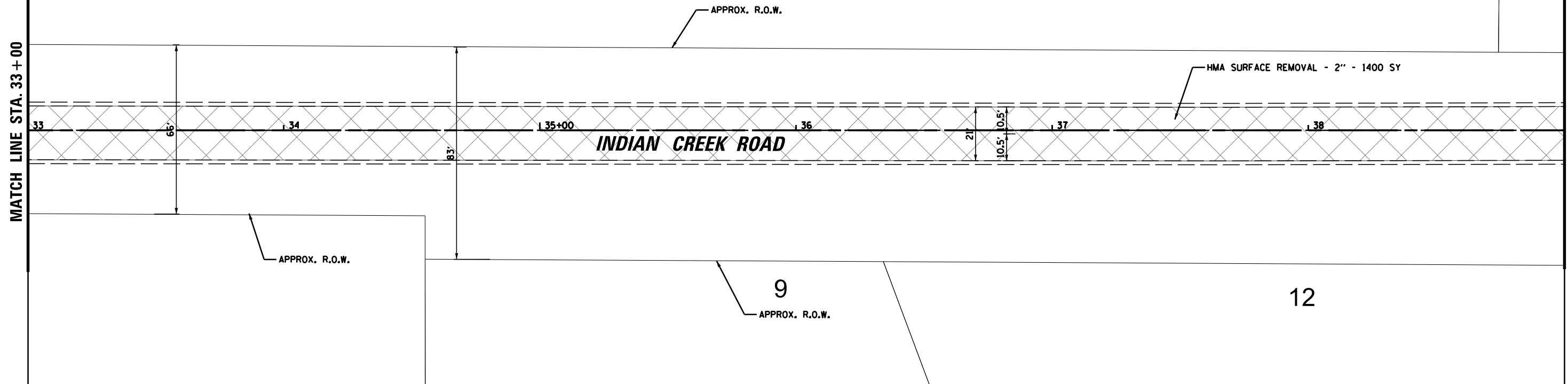
////// SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)

▨ HMA SURFACE REMOVAL - 2"



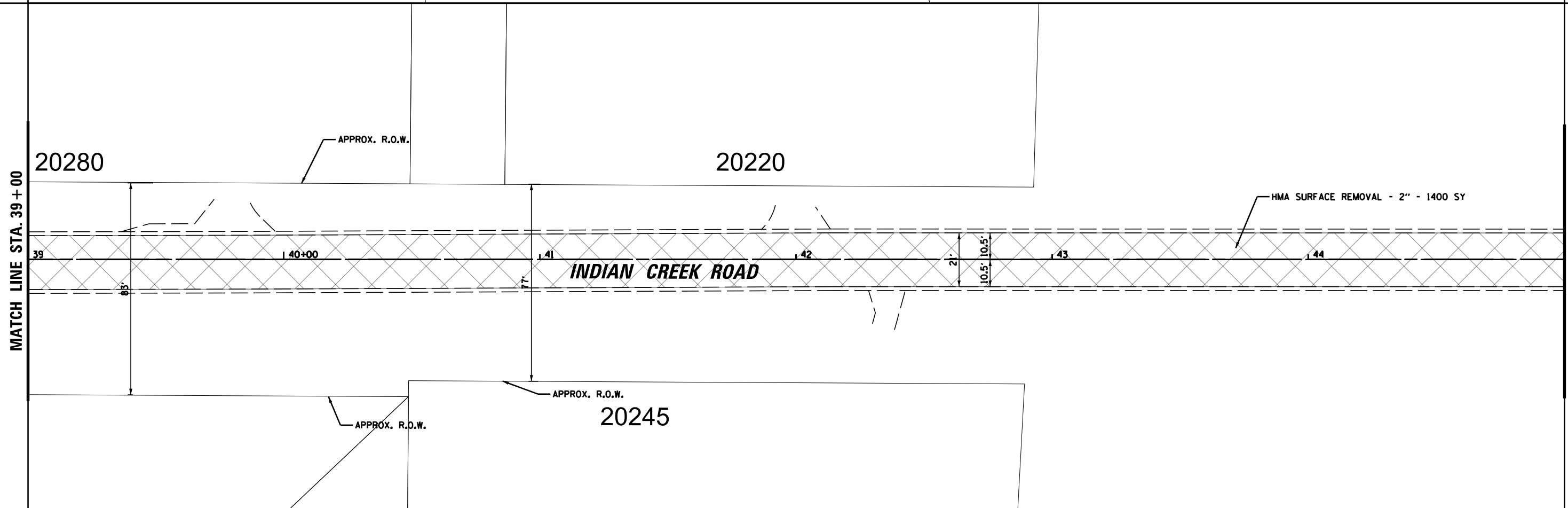
MATCH LINE STA. 33+00

MATCH LINE STA. 39+00





MATCH LINE STA. 39+00

MATCH LINE STA. 45+00



FILE NAME = N:\HAWTHORNWOODS\230536\Civi1\REM04.230536.sht	USER NAME = jhouseh	DESIGNED - JEH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING CONDITIONS AND REMOVAL PLAN			F.A.U. RTE. = 1203	SECTION = 22-00020-00-R5	COUNTY = LAKE	TOTAL SHEETS = 31	SHEET NO. = 9
	PLOT SCALE = 48"	CHECKED - LMF	REVISED -					CONTRACT NO. 61K49				
Default	PLOT DATE = 5/29/2024	DATE -	REVISED -	SCALE: SHEET OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT					

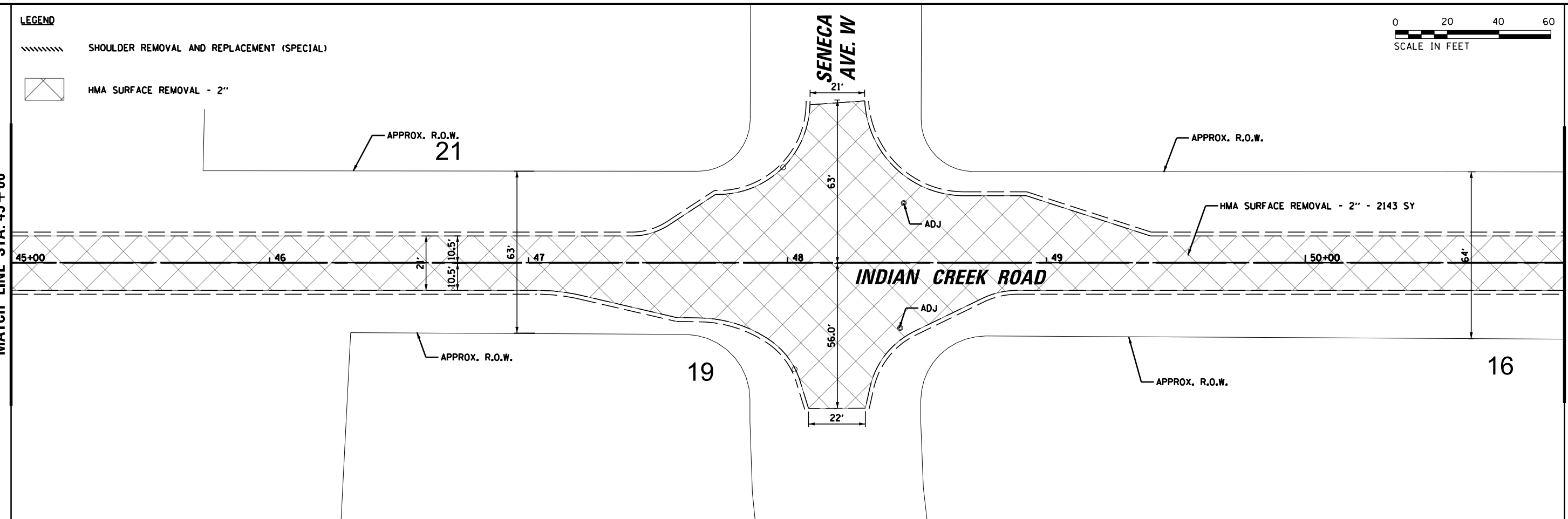
LEGEND

- 
SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)
- 
HMA SURFACE REMOVAL - 2"



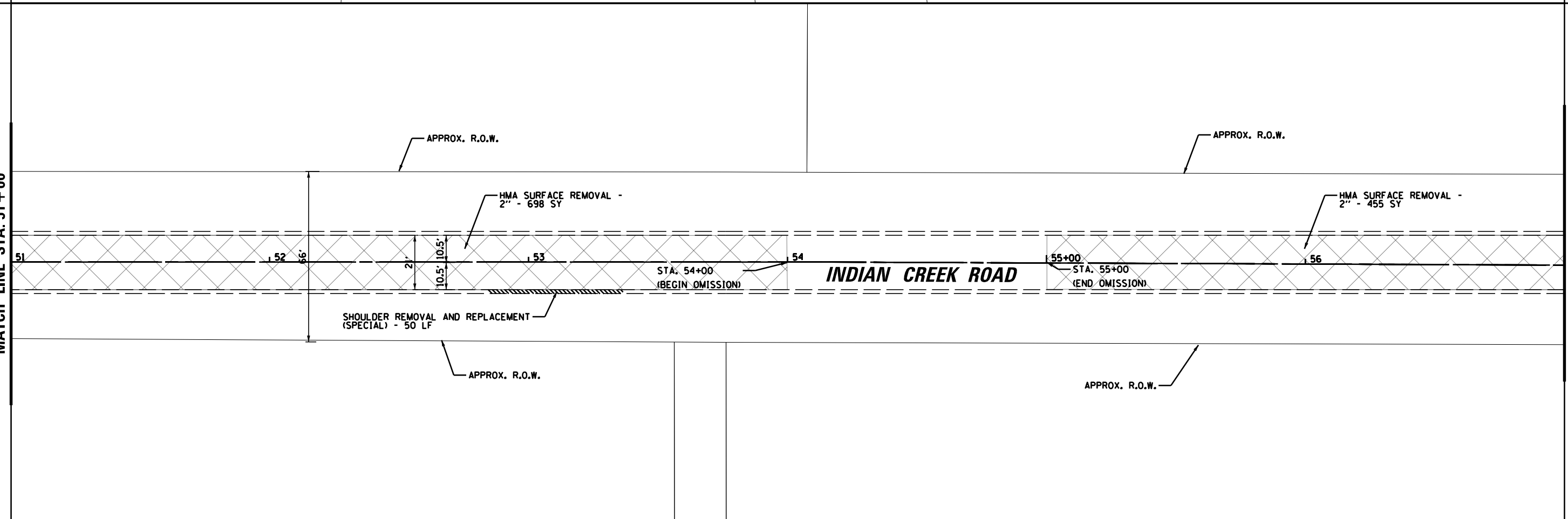
MATCH LINE STA. 45 + 00

MATCH LINE STA. 51 + 00



MATCH LINE STA. 51 + 00

MATCH LINE STA. 57 + 00



FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING CONDITIONS AND REMOVAL PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN - JEH	REVISED -			1203	22-00020-00-R5	LAKE	31	10	
		CHECKED - LMF	REVISED -			CONTRACT NO. 61K49					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
Default	PLOT DATE = 5/29/2024			SCALE:		SHEET OF SHEETS		STA. TO STA.			

LEGEND

////// SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)

▨ HMA SURFACE REMOVAL - 2"



MATCH LINE STA. 57+00

MATCH LINE STA. 63+00

4

6

8

APPROX. R.O.W.

HMA SURFACE REMOVAL - 2" - 1400 SY

57 58 59 60+00 61 62

INDIAN CREEK ROAD

APPROX. R.O.W.

MATCH LINE STA. 63+00

**SENECA
AVE. E**

APPROX. R.O.W.

24

HMA SURFACE REMOVAL - 2" - 1194 SY

PROJECT ENDS
STA. 66+90

63 64 65+00 66

INDIAN CREEK ROAD

APPROX. R.O.W.

FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -
N:\HAWTHORNWOODS\230536\Civil\REM06.230536.sht		DRAWN - JEH	REVISED -
Default	PLOT SCALE = 48"	CHECKED - LMF	REVISED -
	PLOT DATE = 5/29/2024	DATE -	REVISED -



**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

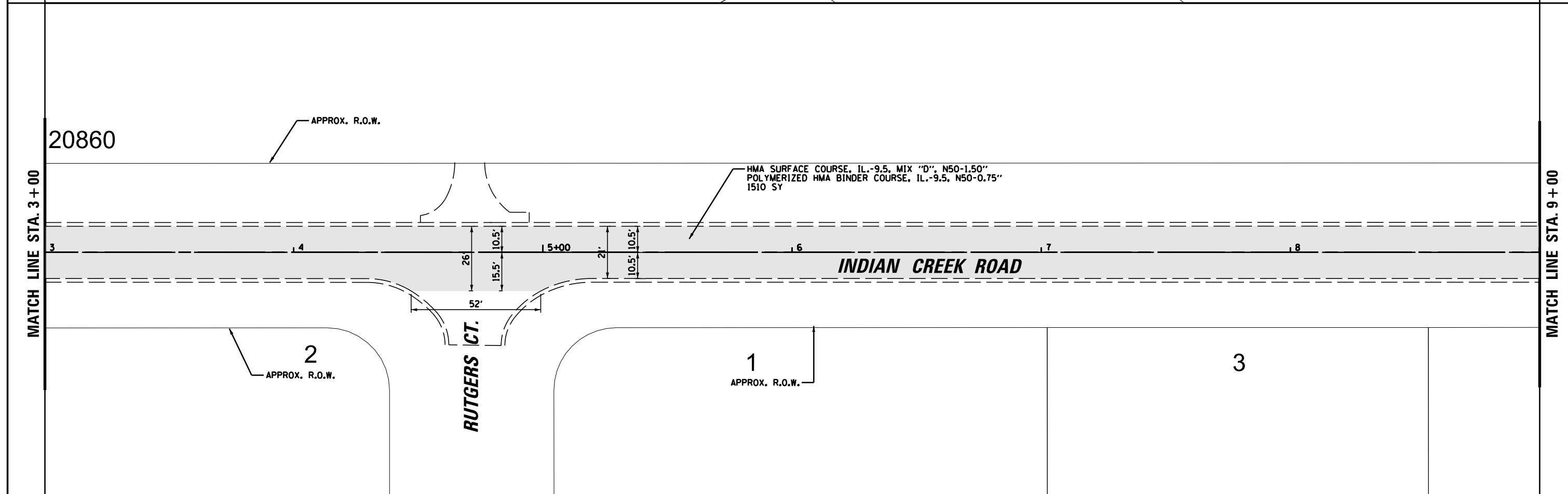
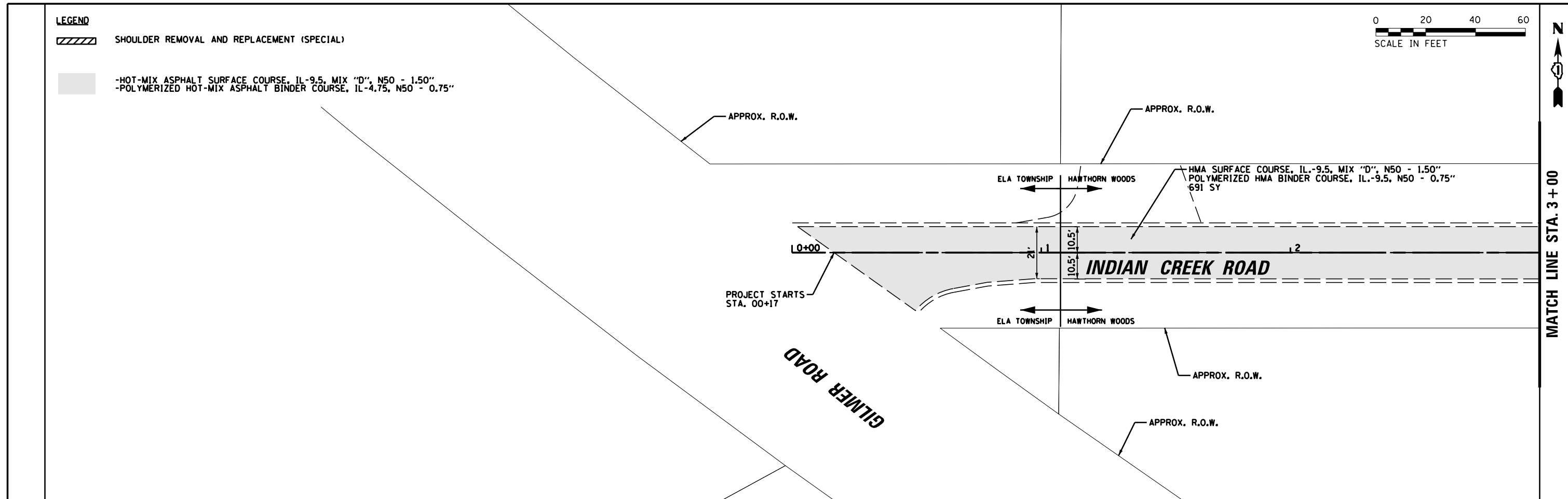
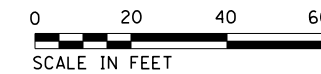
EXISTING CONDITIONS AND REMOVAL PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1203	22-00020-00-R5	LAKE	31	11
CONTRACT NO. 61K49			ILLINOIS FED. AID PROJECT	

LEGEND

-  SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)
-  -HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 - 1.50"
-POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50 - 0.75"



FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN - JEH	REVISED -					1203	22-00020-00-R5	LAKE	31	12	
		CHECKED - LMF	REVISED -										CONTRACT NO. 61K49
		DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT

LEGEND

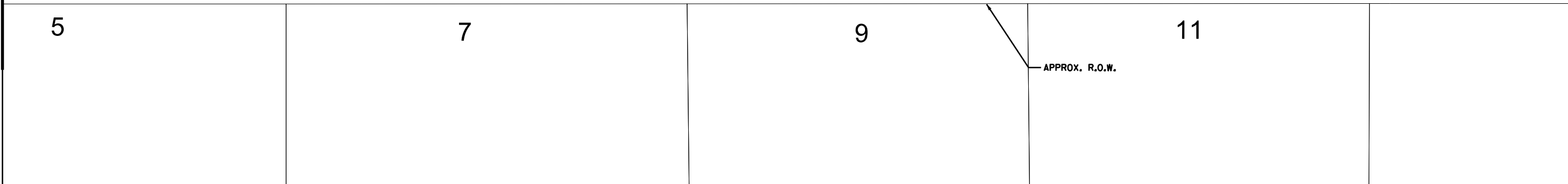
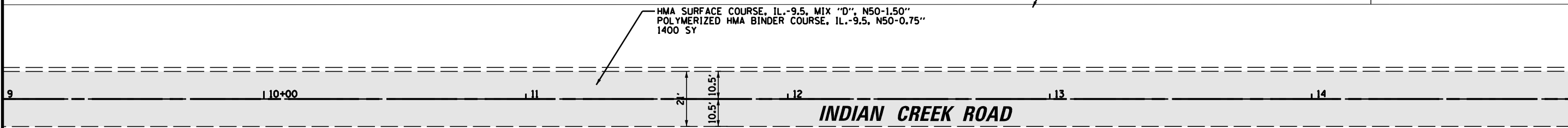
 SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)

 -HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 - 1.50"
 -POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50 - 0.75"



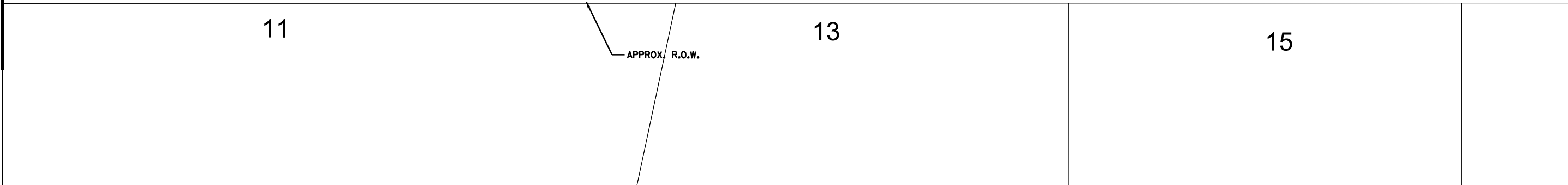
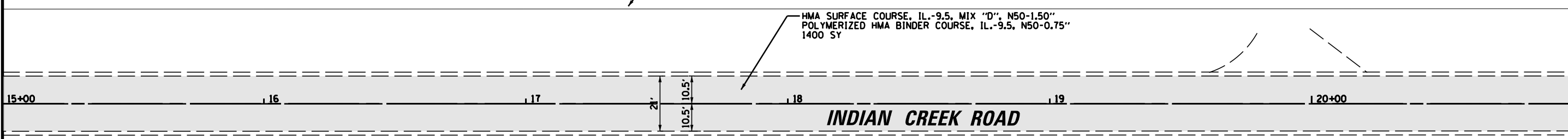
MATCH LINE STA. 9 + 00

MATCH LINE STA. 15 + 00



MATCH LINE STA. 15 + 00

MATCH LINE STA. 21 + 00



FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -
N:\HAWTHORNWOODS\230536\Civil\PRO02.230536.sht		DRAWN - JEH	REVISED -
Default	PLOT SCALE = 48"	CHECKED - LMF	REVISED -
	PLOT DATE = 5/29/2024	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1203	22-00020-00-R5	LAKE	31	13
CONTRACT NO. 61K49			ILLINOIS FED. AID PROJECT	

LEGEND



SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)

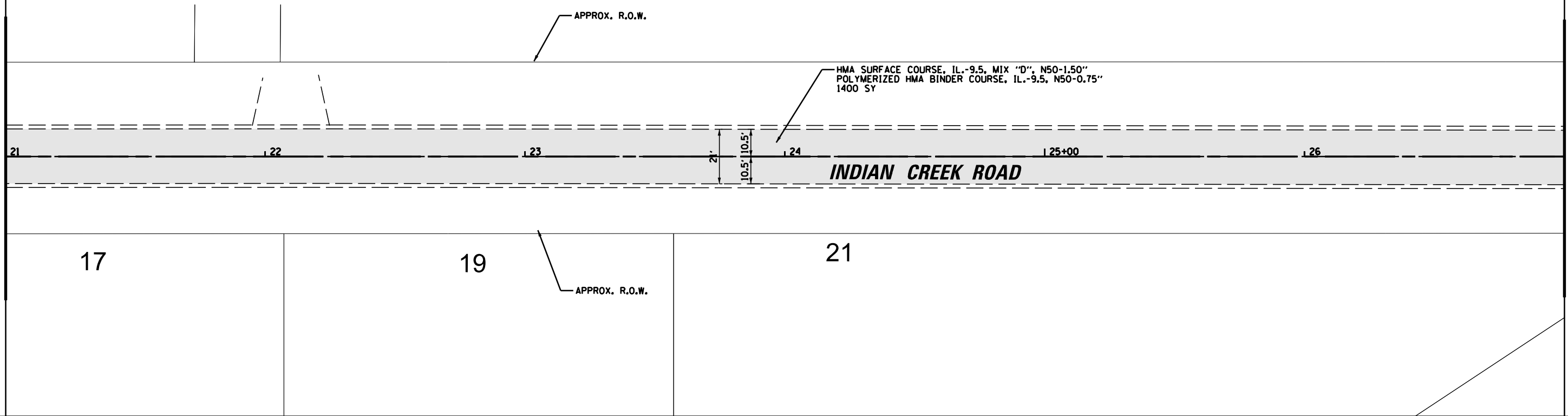


-HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 - 1.50"
 -POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50 - 0.75"



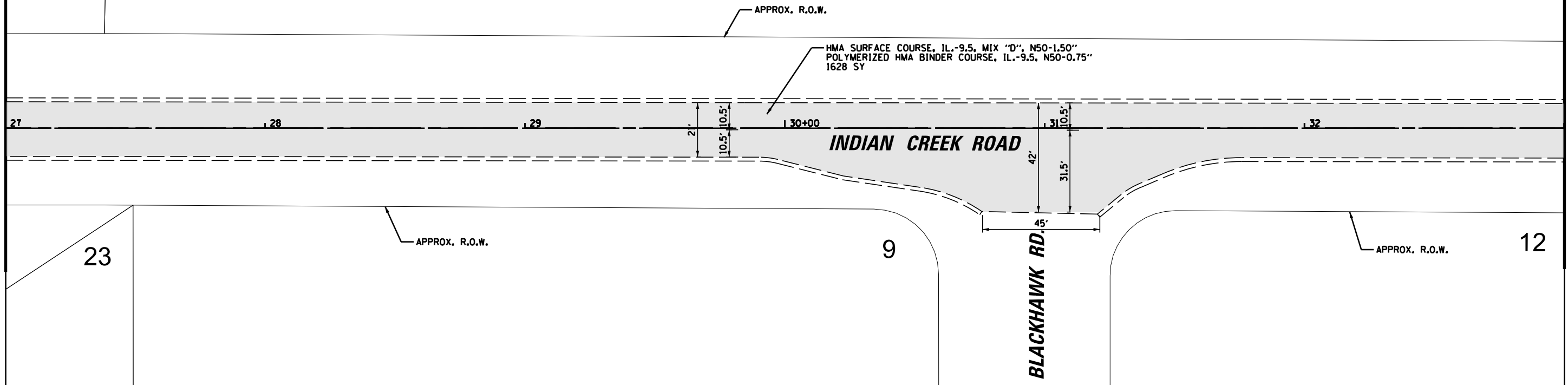
MATCH LINE STA. 21 + 00

MATCH LINE STA. 27 + 00



MATCH LINE STA. 27 + 00

MATCH LINE STA. 33 + 00



FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -
N:\HAWTHORNWOODS\230536\Civil\PRO03.230536.sht		DRAWN - JEH	REVISED -
Default	PLOT SCALE = 48"	CHECKED - LMF	REVISED -
	PLOT DATE = 5/29/2024	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PROPOSED PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1203	22-00020-00-R5	LAKE	31	14
CONTRACT NO. 61K49				
ILLINOIS FED. AID PROJECT				

LEGEND

 SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)

 -HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 - 1.50"
 -POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50 - 0.75"

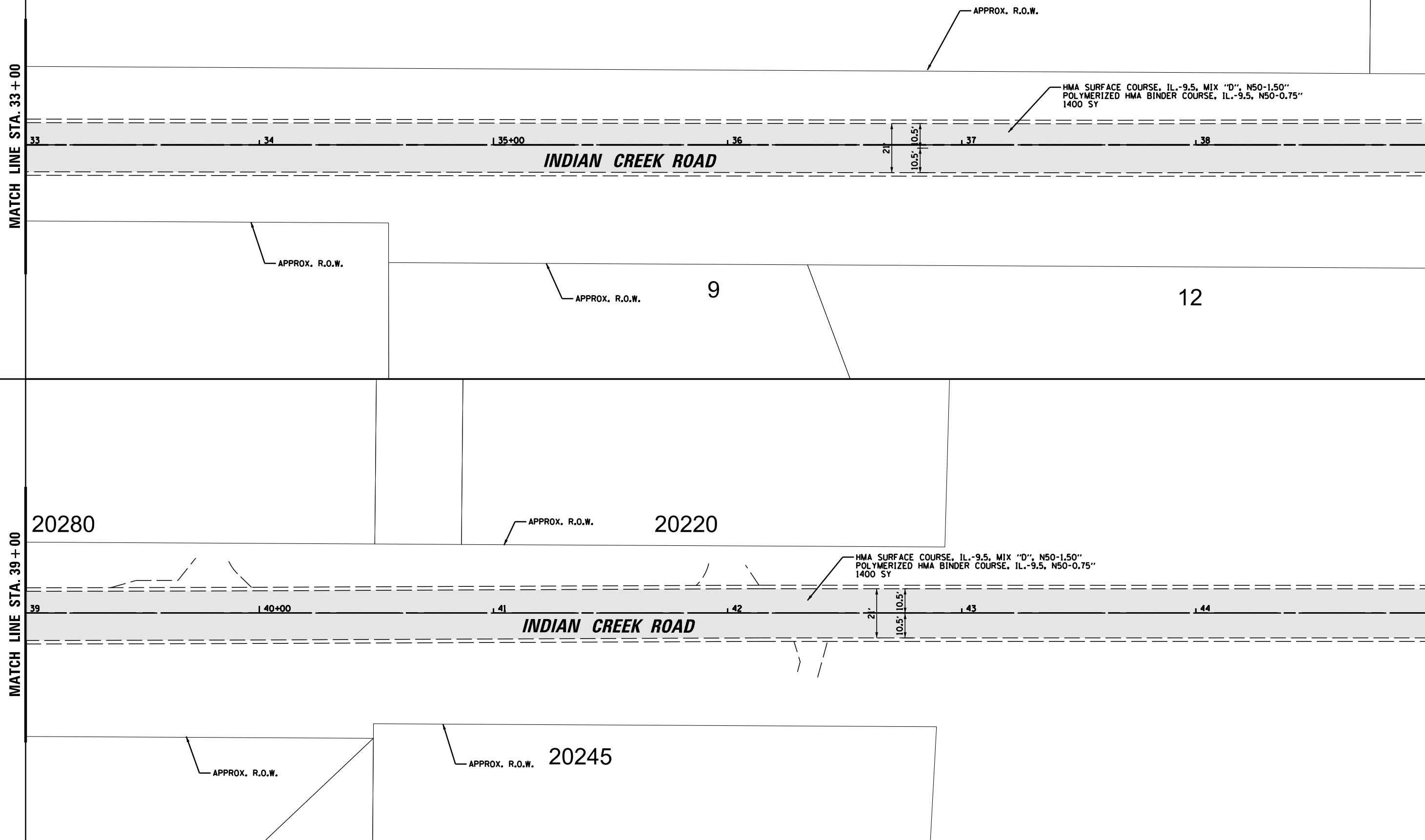


MATCH LINE STA. 33 + 00

MATCH LINE STA. 39 + 00

MATCH LINE STA. 39 + 00

MATCH LINE STA. 45 + 00



FILE NAME = N:\HAWTHORNWOODS\230536\Civil\PRO04.230536.sht	USER NAME = jhouseh	DESIGNED - JEH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	PLOT DATE = 5/29/2024	DRAWN - JEH	REVISED -					1203	22-00020-00-R5	LAKE	31	15
		CHECKED - LMF	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 61K49				
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

LEGEND



SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)

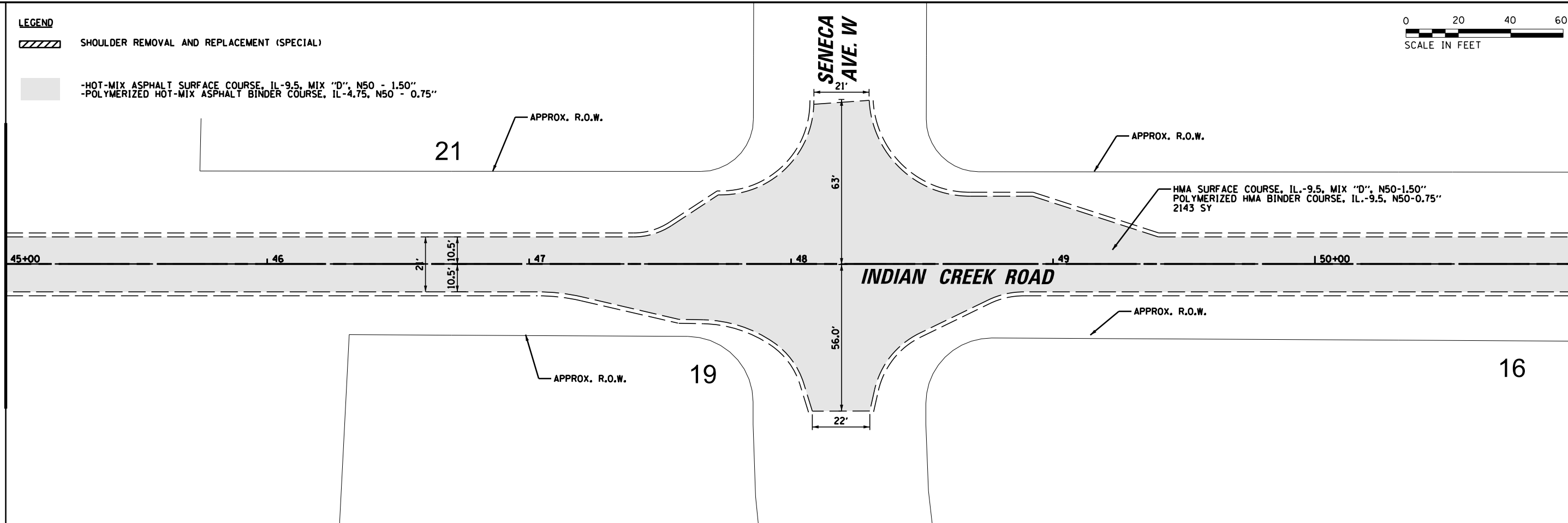


-HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 - 1.50"
 -POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50 - 0.75"



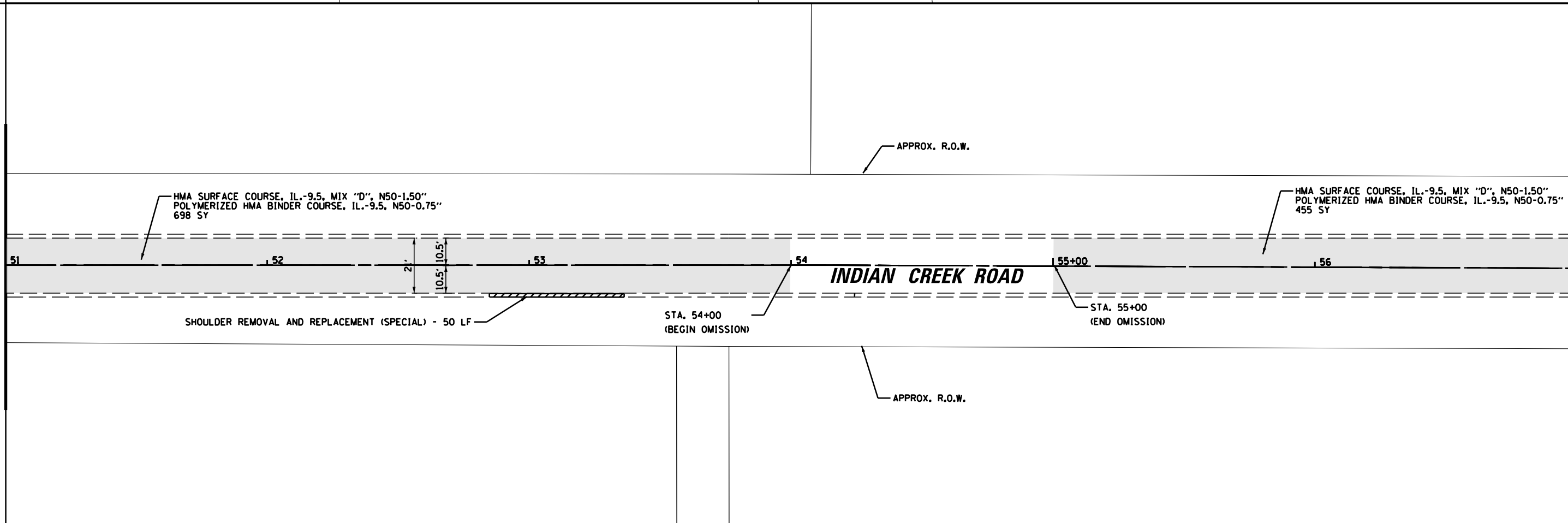
MATCH LINE STA. 45 + 00

MATCH LINE STA. 51 + 00



MATCH LINE STA. 51 + 00

MATCH LINE STA. 57 + 00

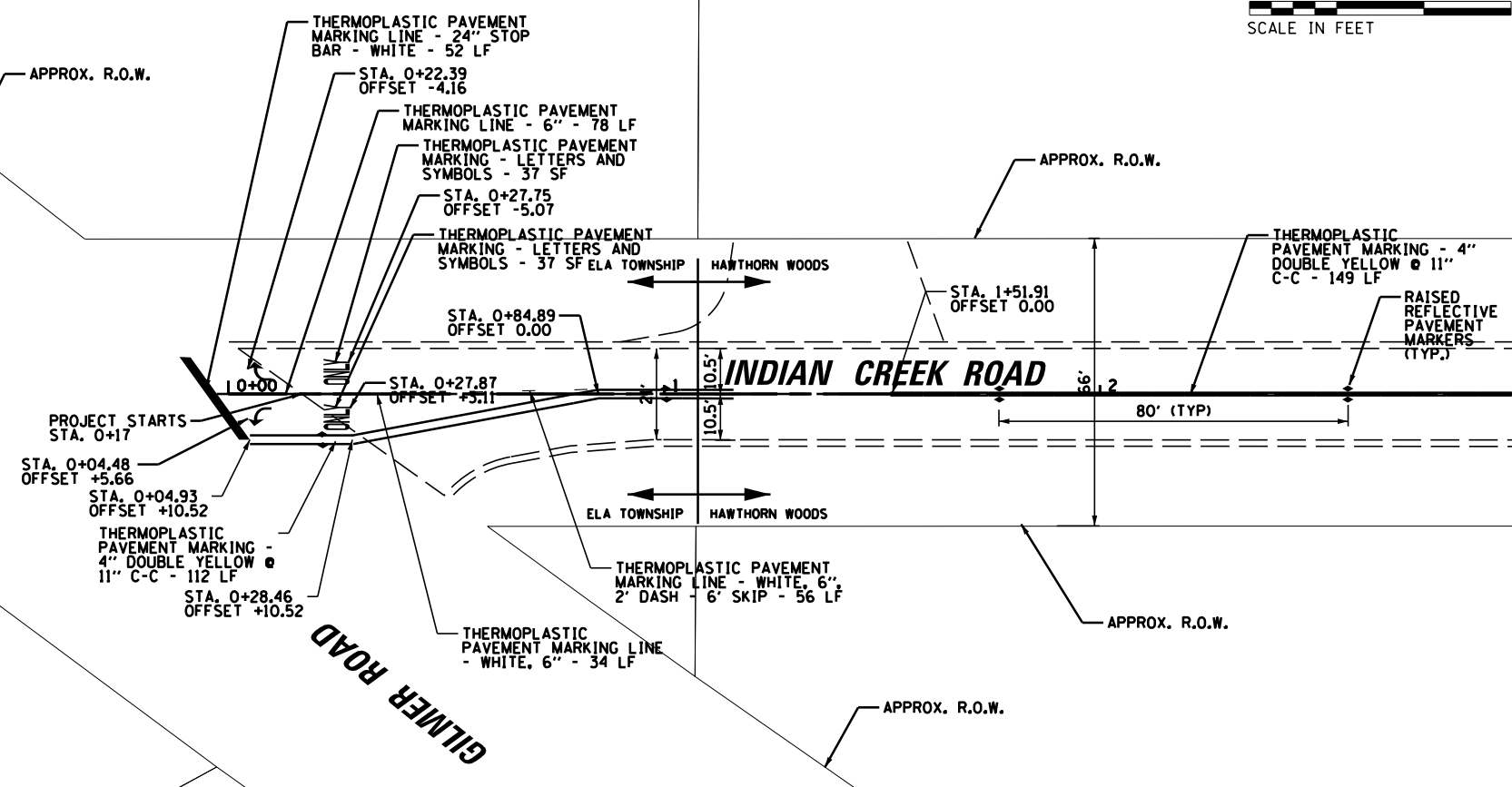


FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\HAWTHORNWOODS\230536\Civil\PRO05.230536.sht	PLOT SCALE = 48"	DRAWN - JEH	REVISED -					1203	22-00020-00-R5	LAKE	31	16
Default	PLOT DATE = 5/29/2024	CHECKED - LMF	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 61K49				
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

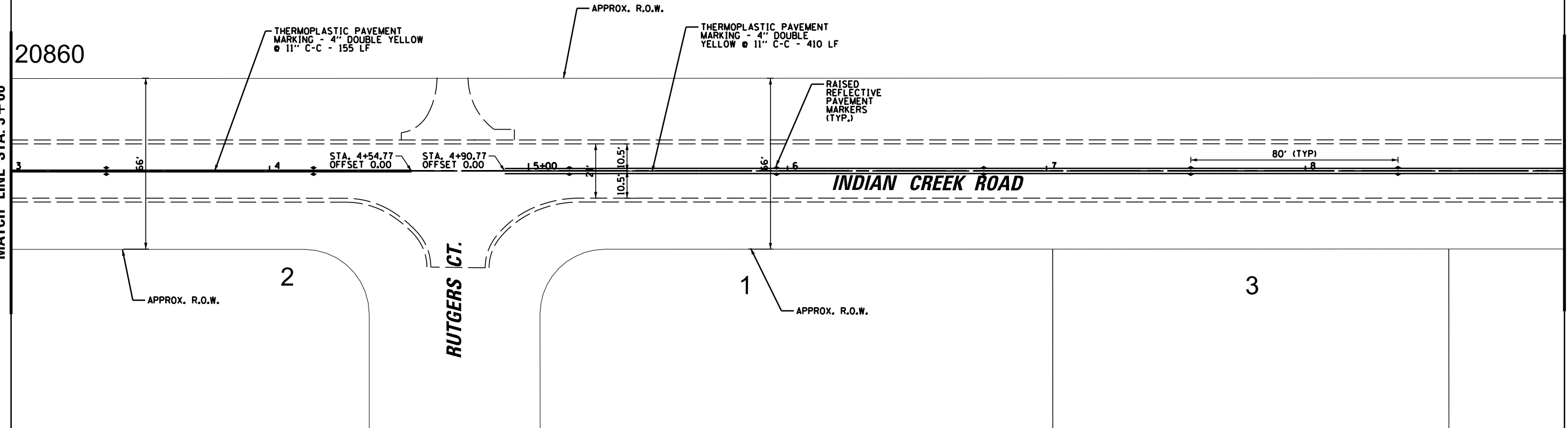


LEGEND

- ◆ RAISED REFLECTIVE PAVEMENT MARKERS (TYP.)
- ==== THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE YELLOW @ 11" C-C
- - - THERMOPLASTIC PAVEMENT MARKING LINE - WHITE, 6", 2' DASH - 6' SKIP
- ↶ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- ▬ THERMOPLASTIC PAVEMENT MARKING LINE - 24" STOP BAR - WHITE



MATCH LINE STA. 3 + 00








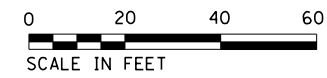
MATCH LINE STA. 3 + 00

MATCH LINE STA. 9 + 00

FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING PLAN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\HAWTHORNWOODS\230536\Civi1\PMK01.230536.sht								SCALE: SHEET OF SHEETS STA. TO STA.			1203	22-00020-00-R5
PLOT SCALE = 48'					CONTRACT NO. 61K49			ILLINOIS FED. AID PROJECT				
PLOT DATE = 5/29/2024												

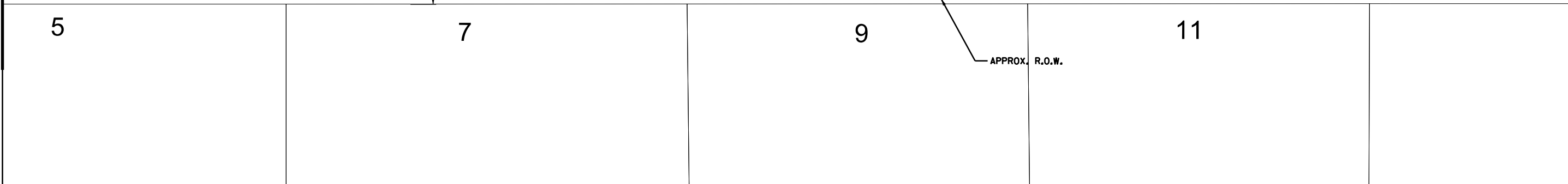
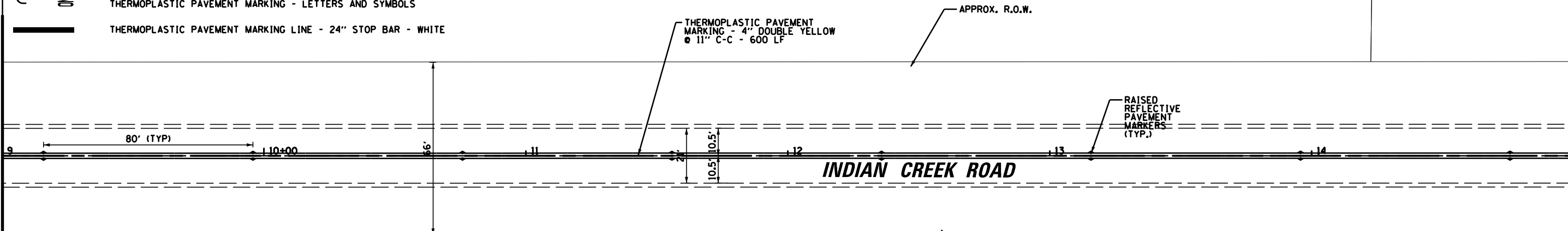
LEGEND

-  RAISED REFLECTIVE PAVEMENT MARKERS (TYP.)
-  THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE YELLOW @ 11" C-C
-  THERMOPLASTIC PAVEMENT MARKING LINE - WHITE, 6", 2' DASH - 6' SKIP
-  THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
-  THERMOPLASTIC PAVEMENT MARKING LINE - 24" STOP BAR - WHITE



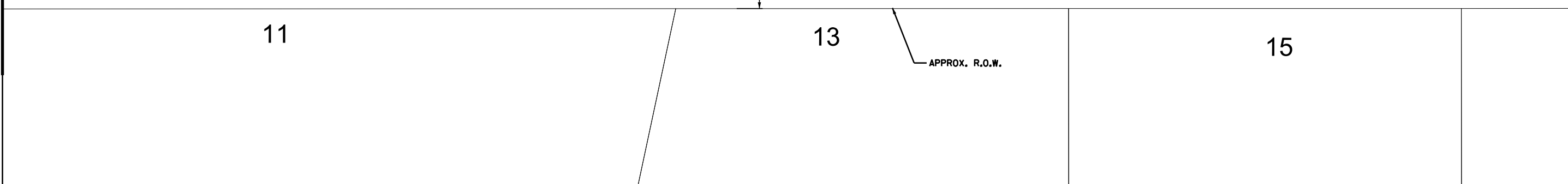
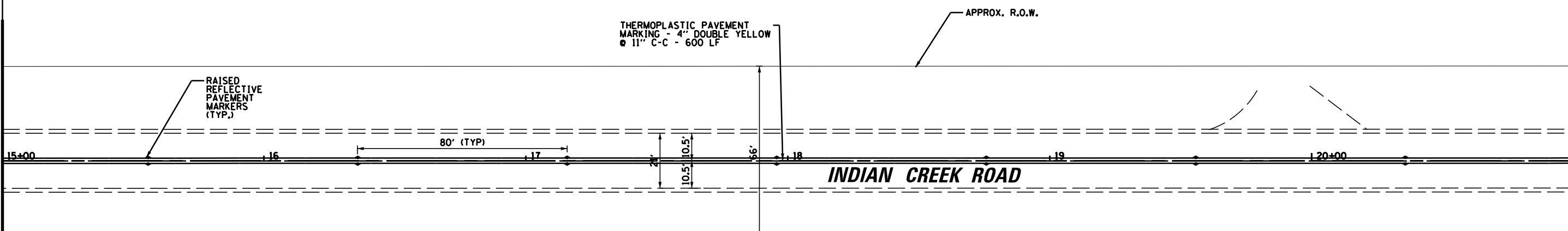
MATCH LINE STA. 9+00

MATCH LINE STA. 15+00








MATCH LINE STA. 15+00

MATCH LINE STA. 21+00



FILE NAME = N:\HAWTHORNWOODS\230536\Civi1\PMK02.230536.sht	USER NAME = jhouseh	DESIGNED - JEH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING PLAN			F.A.U. RTE. 1203	SECTION 22-00020-00-R5	COUNTY LAKE	TOTAL SHEETS 31	SHEET NO. 19
Default	PLOT SCALE = 48"	CHECKED - LMF	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 61K49				
	PLOT DATE = 5/29/2024	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

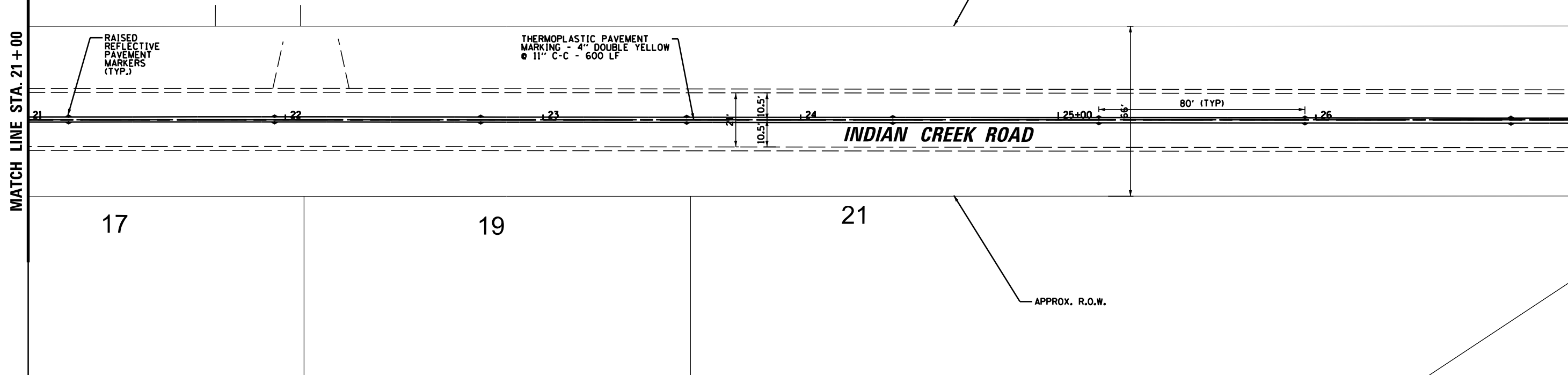
LEGEND

-  RAISED REFLECTIVE PAVEMENT MARKERS (TYP.)
-  THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE YELLOW @ 11" C-C
-  THERMOPLASTIC PAVEMENT MARKING LINE - WHITE, 6", 2' DASH - 6' SKIP
-  THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
-  THERMOPLASTIC PAVEMENT MARKING LINE - 24" STOP BAR - WHITE



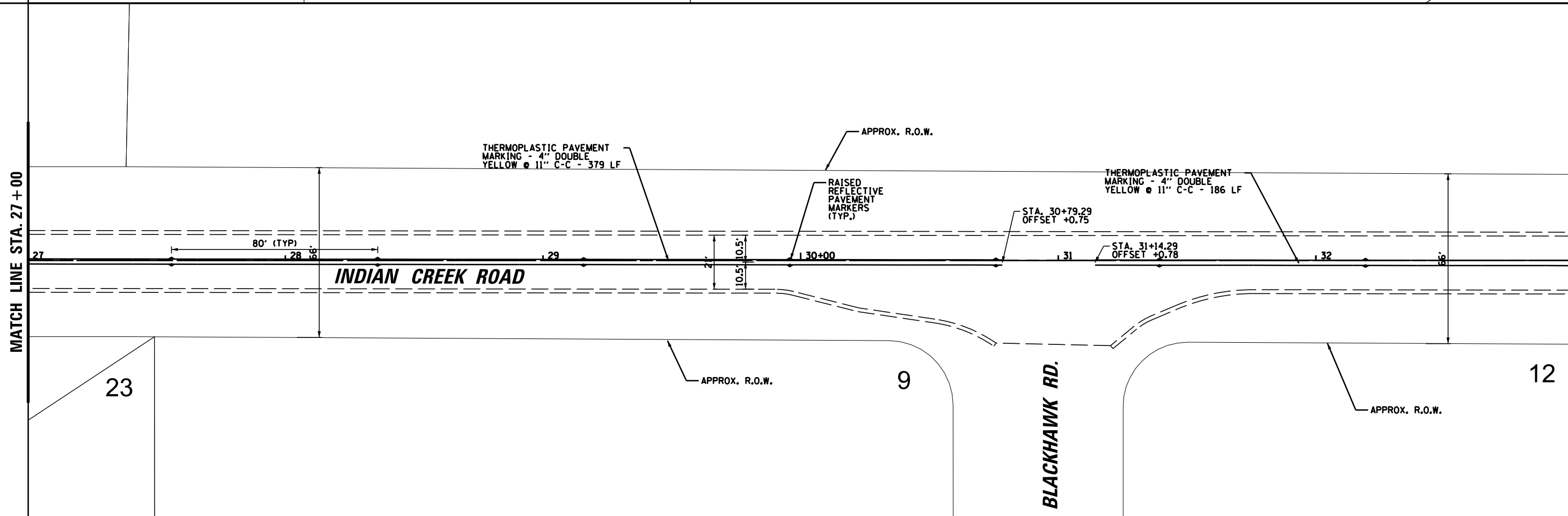
MATCH LINE STA. 21+00

MATCH LINE STA. 27+00



MATCH LINE STA. 27+00

MATCH LINE STA. 33+00



FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -
N:\HAWTHORNWOODS\230536\Civil\PMK03.230536.sht		DRAWN - JEH	REVISED -
Default	PLOT SCALE = 48"	CHECKED - LMF	REVISED -
	PLOT DATE = 5/29/2024	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1203	22-00020-00-R5	LAKE	31	20
CONTRACT NO. 61K49				
ILLINOIS FED. AID PROJECT				

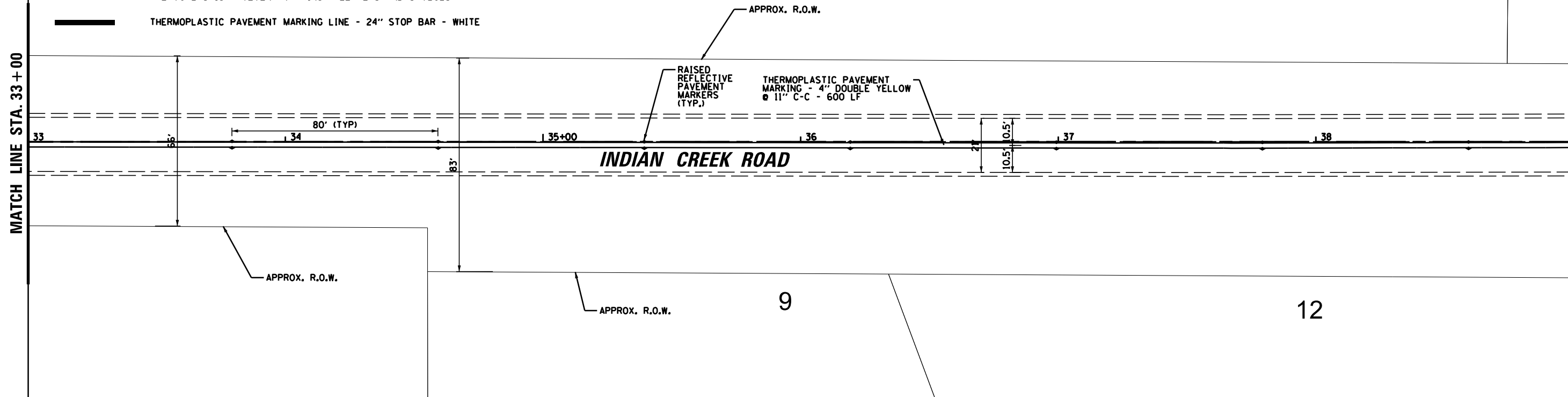
LEGEND

- ◆ RAISED REFLECTIVE PAVEMENT MARKERS (TYP.)
- ==== THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE YELLOW @ 11" C-C
- - - THERMOPLASTIC PAVEMENT MARKING LINE - WHITE, 6", 2' DASH - 6' SKIP
- ↶ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- ▬ THERMOPLASTIC PAVEMENT MARKING LINE - 24" STOP BAR - WHITE



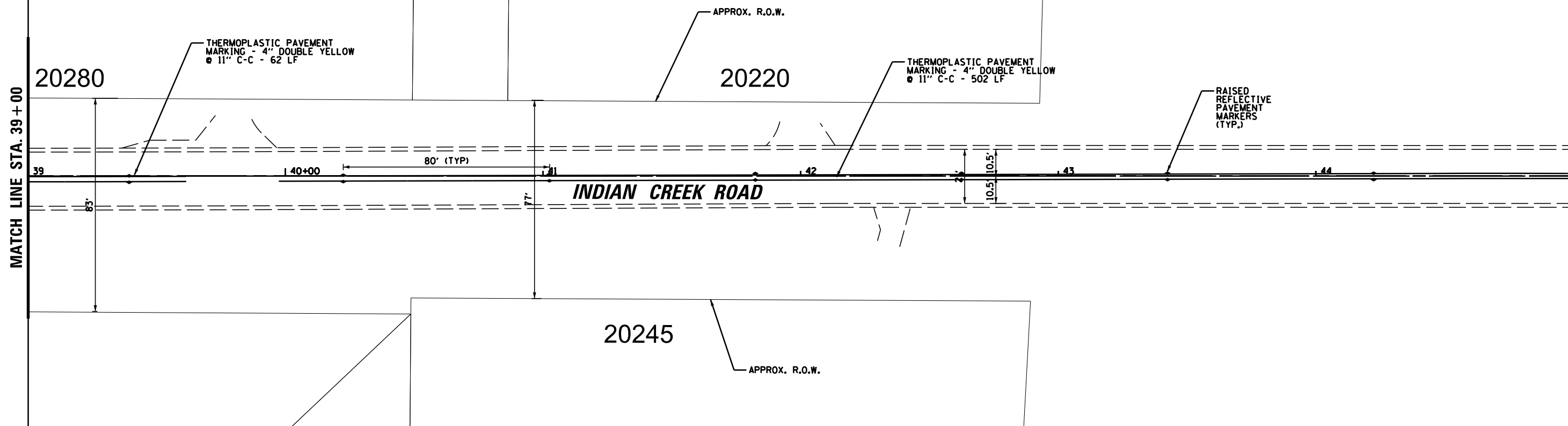
MATCH LINE STA. 33+00

MATCH LINE STA. 39+00



MATCH LINE STA. 39+00

MATCH LINE STA. 45+00








FILE NAME = N:\HAWTHORNWOODS\230536\Civil\PMK04.230536.sht	USER NAME = jhouseh	DESIGNED - JEH	REVISED -
	PLOT SCALE = 48"	DRAWN - JEH	REVISED -
	PLOT DATE = 5/29/2024	CHECKED - LMF	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLAN			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1203	22-00020-00-R5	LAKE	31	21
CONTRACT NO. 61K49				
ILLINOIS FED. AID PROJECT				

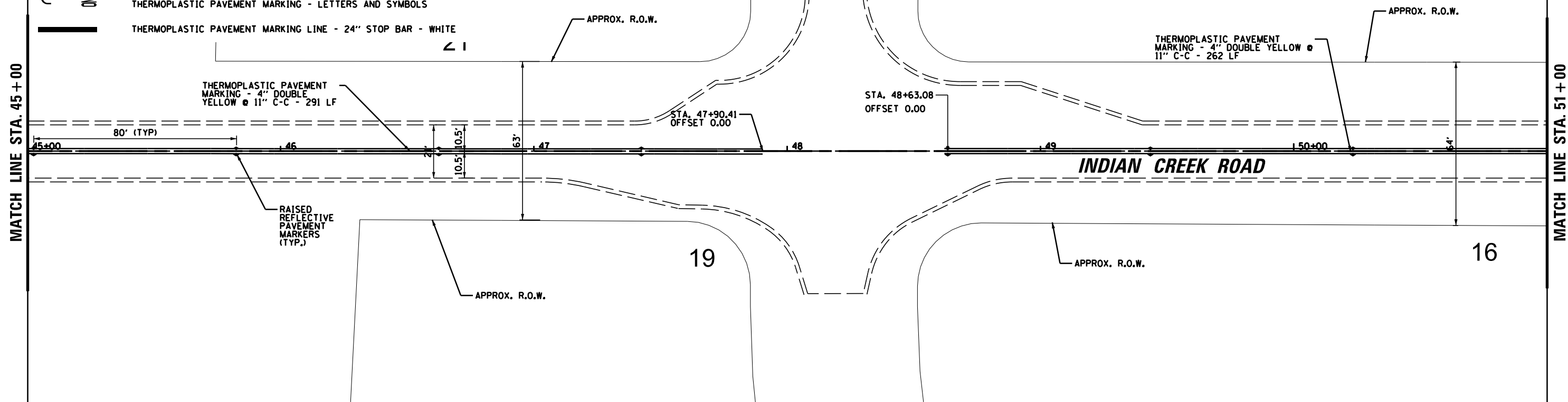
LEGEND

-  RAISED REFLECTIVE PAVEMENT MARKERS (TYP.)
-  THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE YELLOW @ 11" C-C
-  THERMOPLASTIC PAVEMENT MARKING LINE - WHITE, 6", 2" DASH - 6' SKIP
-  THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
-  THERMOPLASTIC PAVEMENT MARKING LINE - 24" STOP BAR - WHITE



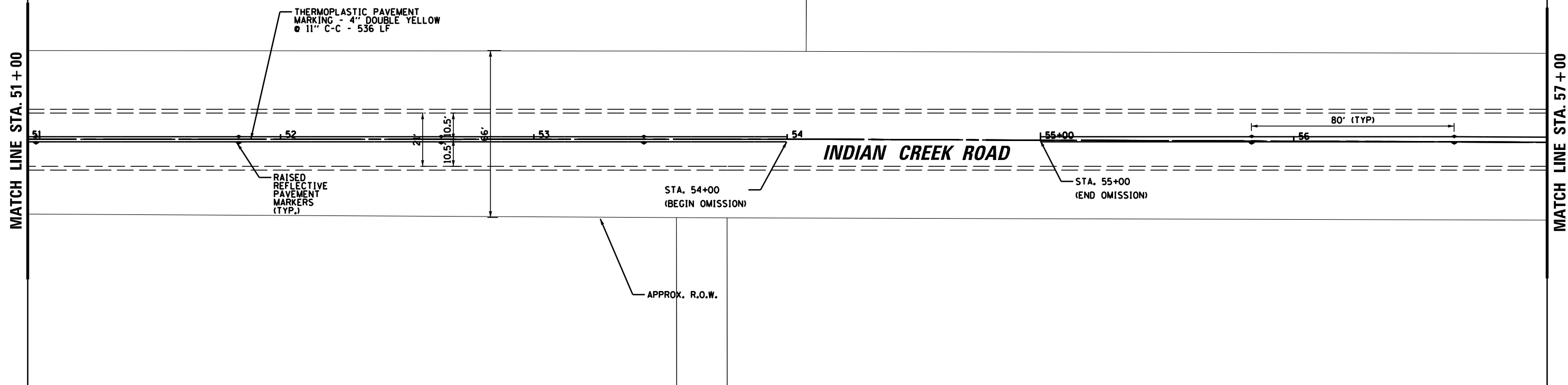
MATCH LINE STA. 45+00

MATCH LINE STA. 51+00



MATCH LINE STA. 51+00

MATCH LINE STA. 57+00








FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -
N:\HAWTHORNWOODS\230536\Civi1\PMK05.230536.sht		DRAWN - JEH	REVISED -
	PLOT SCALE = 48"	CHECKED - LMF	REVISED -
Default	PLOT DATE = 5/29/2024	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLAN			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1203	22-00020-00-R5	LAKE	31	22
CONTRACT NO. 61K49				
ILLINOIS FED. AID PROJECT				

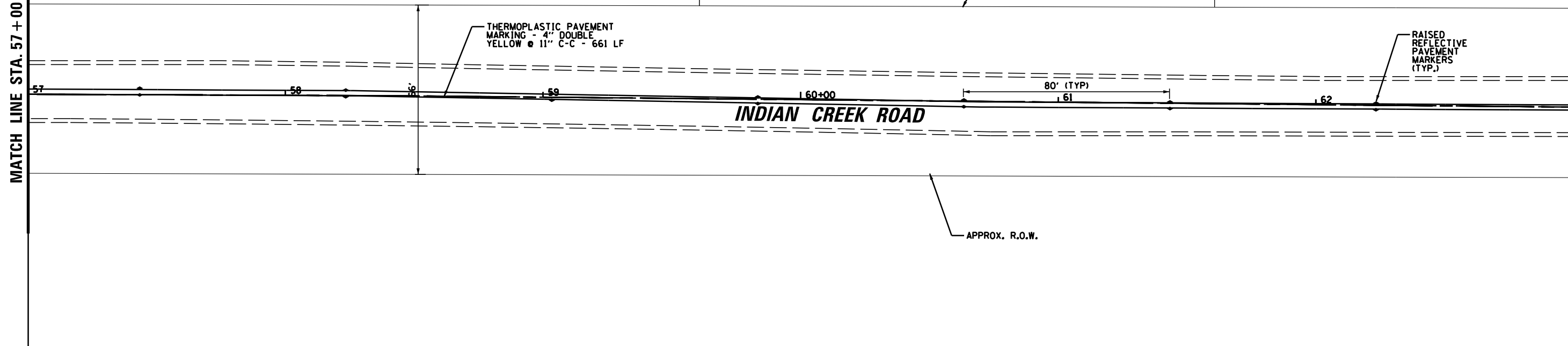
LEGEND

-  RAISED REFLECTIVE PAVEMENT MARKERS (TYP.)
-  THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE YELLOW @ 11" C-C
-  THERMOPLASTIC PAVEMENT MARKING LINE - WHITE, 6", 2" DASH - 6' SKIP
-  THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
-  THERMOPLASTIC PAVEMENT MARKING LINE - 24" STOP BAR - WHITE



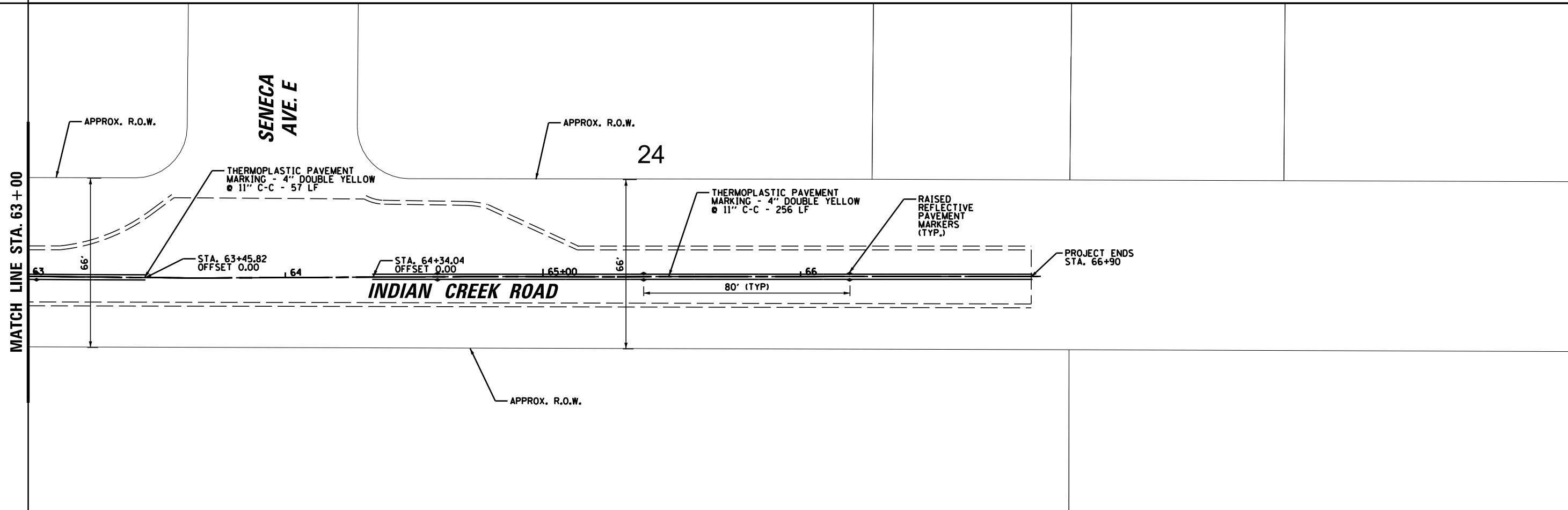
MATCH LINE STA. 57+00

MATCH LINE STA. 63+00



MATCH LINE STA. 63+00

PROJECT ENDS STA. 66+90



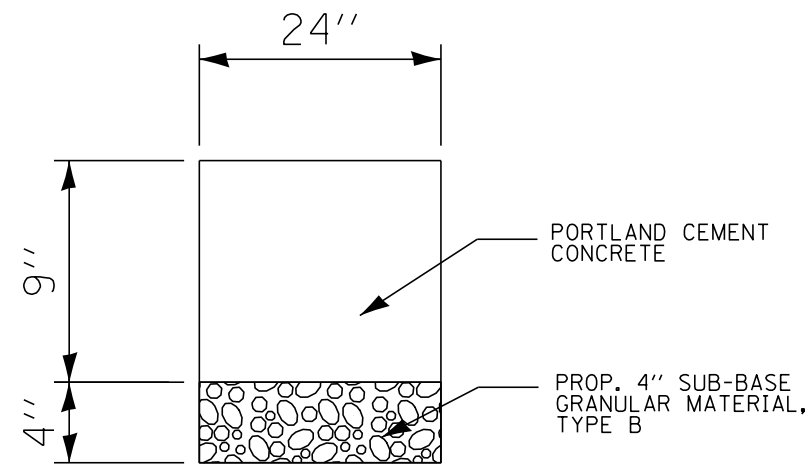
FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -
N:\HAWTHORNWOODS\230536\Civi1\PMK06.230536.sht		DRAWN - JEH	REVISED -
Default	PLOT SCALE = 48'	CHECKED - LMF	REVISED -
	PLOT DATE = 5/29/2024	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

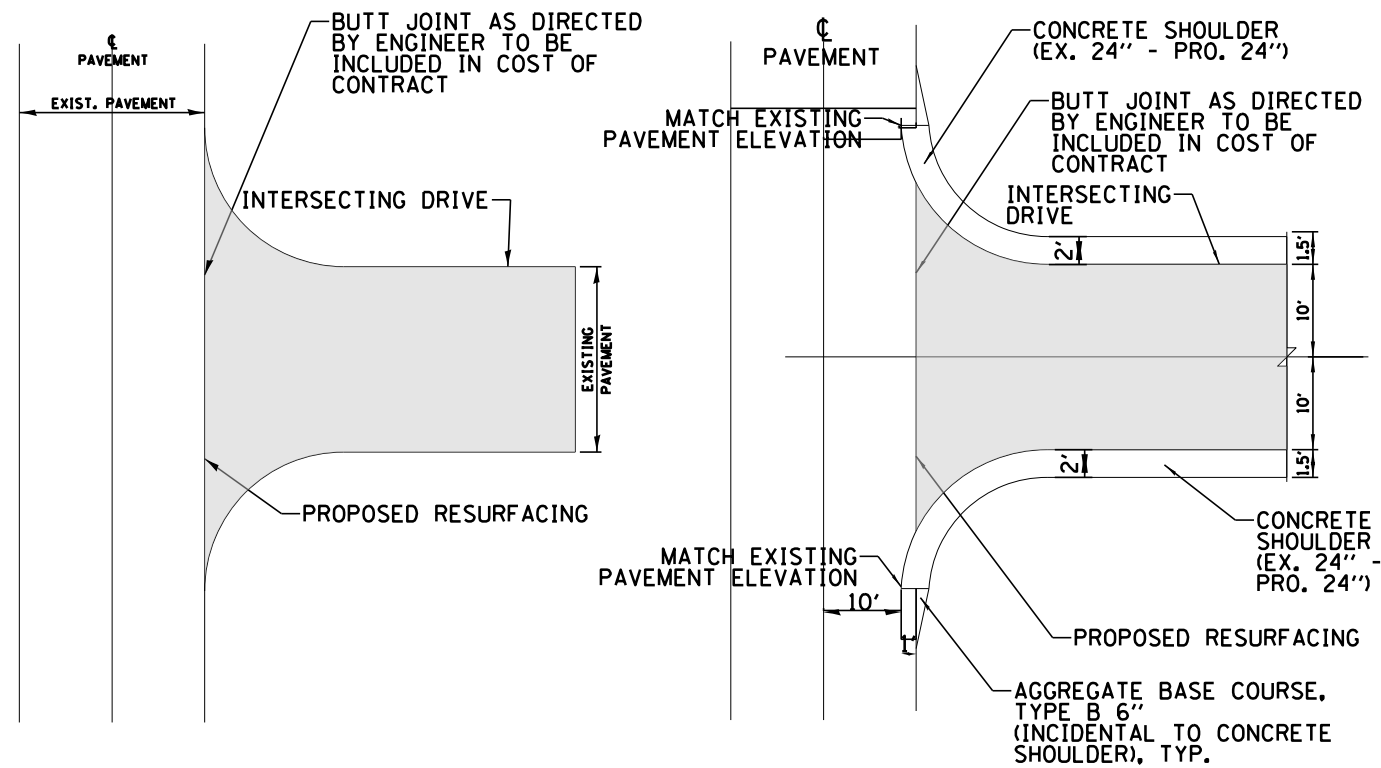
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1203	22-00020-00-R5	LAKE	31	23
CONTRACT NO. 61K49				
ILLINOIS FED. AID PROJECT				



PROPOSED CONCRETE SHOULDER SHALL BE 24" (2') WIDE PCC SHOULDERS AT THE EDGE OF THE EXISTING ASPHALT PAVEMENT. THIS ITEM INCLUDES EXCAVATION, PAVEMENT REMOVAL, 4" GRANULAR SUBBASE, 9" PCC SHOULDER, AND BACKFILLING THE SHOULDER AS DETAILED IN THE SPECIAL PROVISION. THE 4" GRANULAR SUBBASE SHALL BE COMPACTED CA-6. THIS WORK SHALL BE COMPLETED IN ACCORDANCE WITH SECTION 483 OF THE STANDARD SPECIFICATION. THE TYPICAL SECTIONS INCLUDED HEREIN, IDOT STANDARDS 420001 AND 483001 EXCEPT FOR THE MODIFICATIONS SHOWN IN THE SPECIAL PROVISION FOR "SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)".

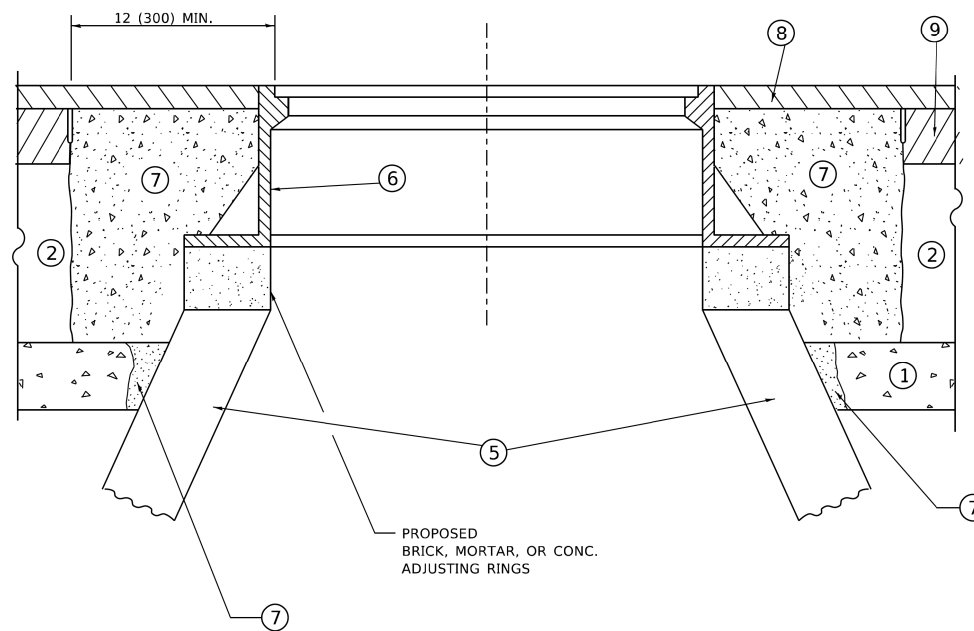
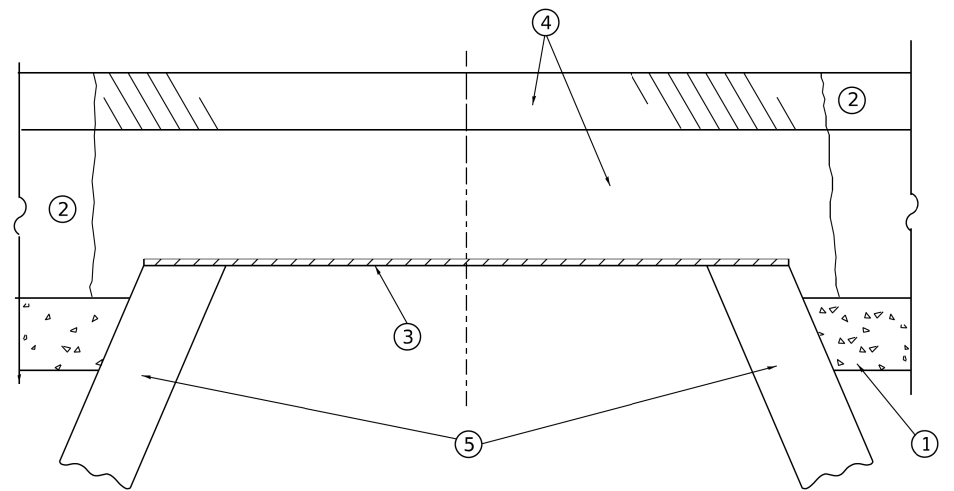
CONCRETE SHOULDER DETAIL

STREET INTERSECTIONS



CONCRETE SHOULDER WILL BE CONSTRUCTED TO MEET THE EXISTING STREET GRADE ON ALL STREETS.

FILE NAME =	USER NAME = jhouseh	DESIGNED - JEH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CONSTRUCTION DETAILS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\HAWTHORNWOODS\230536\Civil\det_230536.sht		DRAWN - JEH	REVISED -					1653	22-00020-00-R5	LAKE	31	24
PLOT SCALE = 56'		CHECKED - LMF	REVISED -		CONTRACT NO. 61K49							
Default	PLOT DATE = 5/29/2024	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT



DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-2* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

MODEL: Default
FILE NAME: W:\1815102\2341bd08.dgn

USER NAME = Lawrence.DeManche	DESIGNED - R. SHAH	REVISED - R. BORO 03-09-11
	DRAWN -	REVISED - R. BORO 12-06-11
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - K. SMITH 11-18-22
PLOT DATE = 9/15/2023	DATE - 10-25-94	REVISED - K. SMITH 09-15-23

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

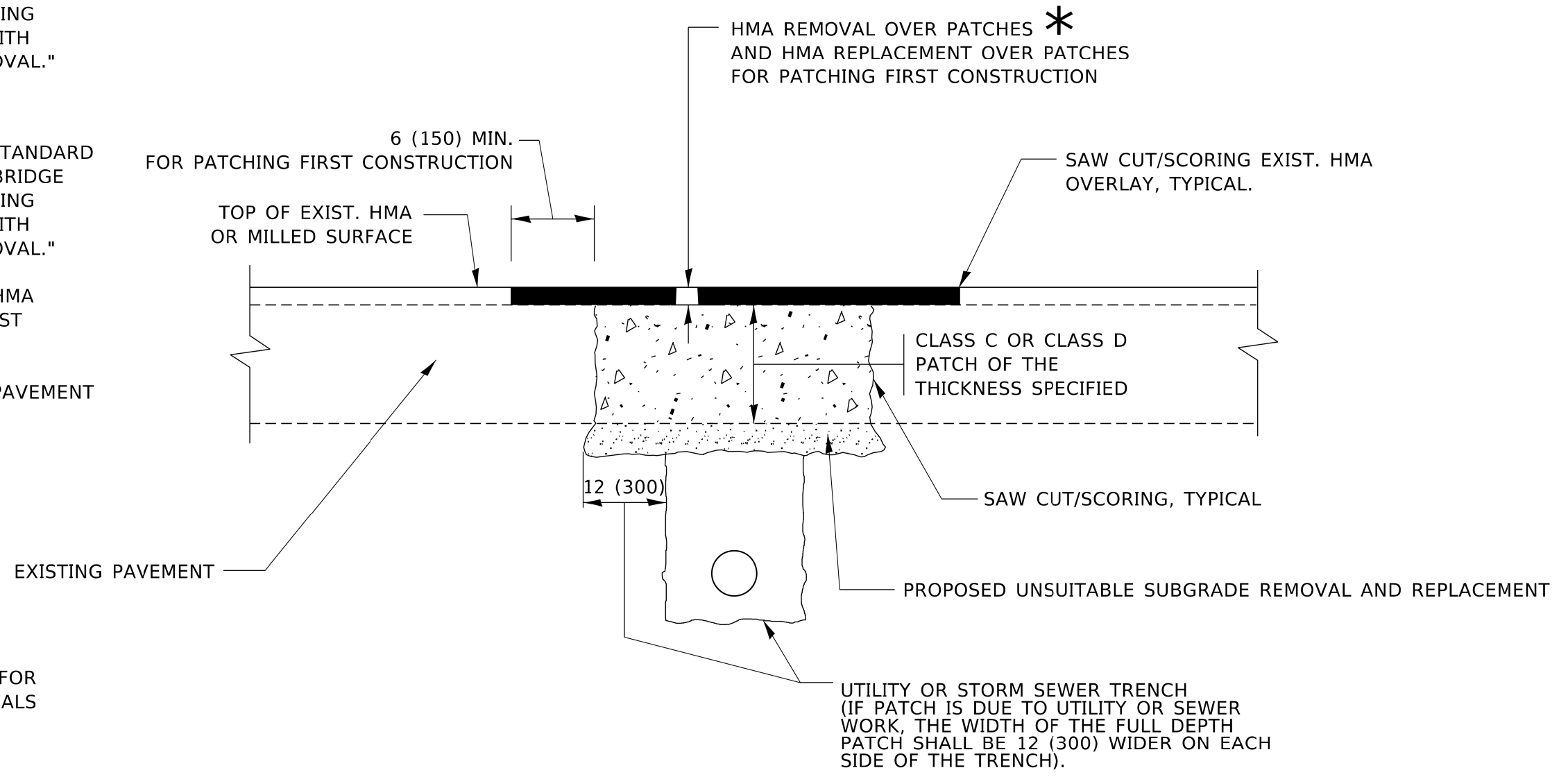
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1653	22-00020-00-RS	LAKE	31	25
BD600-03 (BD-08)			CONTRACT NO. 61K49	
ILLINOIS FED. AID PROJECT				

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
FILE: \\miller-pw-bentley.com\pww\DOT\Documents\DOT_Offices\Bentley_1\Projects\1653\22-00020-00-RS\CADD\DATA\CAD\Sheets\1653\22.dgn

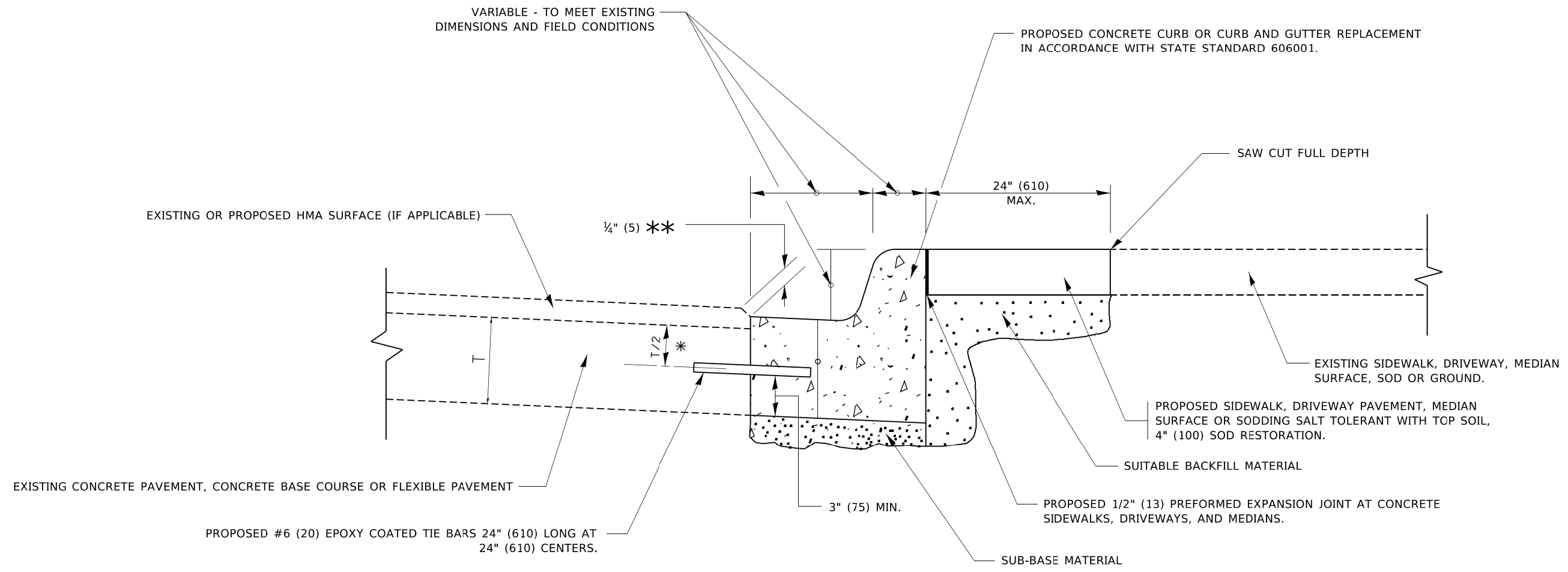
USER NAME = Lawrence.DeManche	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07
	DRAWN -	REVISED - R. BORO 09-04-07
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - K. ENG 10-27-08
PLOT DATE = 11/18/2022	DATE - 10-25-94	REVISED - K. SMITH 11-18-22

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1653	22-00020-00-RS	LAKE	31	26
BD400-04 (BD-22)			CONTRACT NO. 61K49	
ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

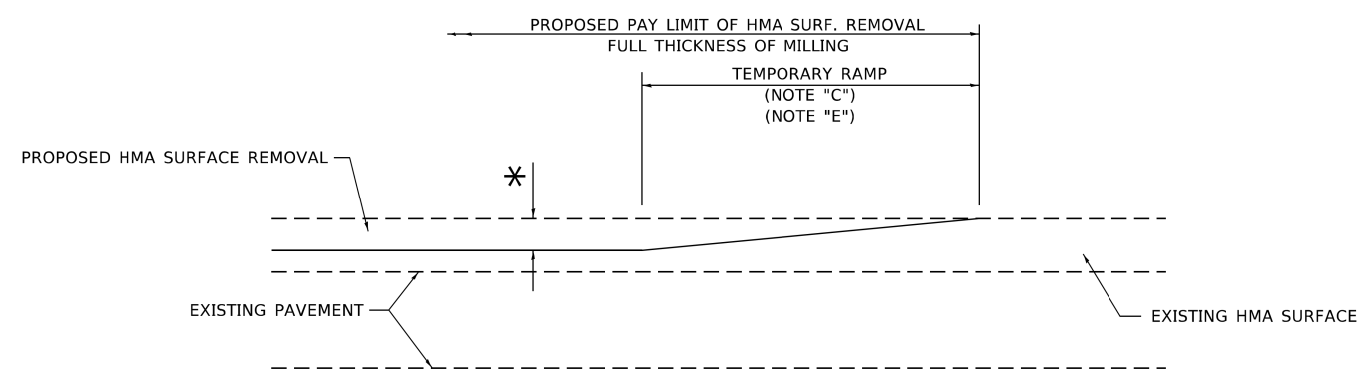
MODEL: Default
FILE: Model: p:\pghl\com.dca.illinois.gov\PI\DOT\Documents\DOT_Offices\Bartlett_1\Projects\1154272-21\CAD\Drawn\CAD\sheet\26A.dgn

USER NAME = footemj	DESIGNED - A. HOUSEH	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 01-22-01
PLOT SCALE = 50.0000' / 1"	CHECKED -	REVISED - R. BORO 12-15-09
PLOT DATE = 7/11/2019	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

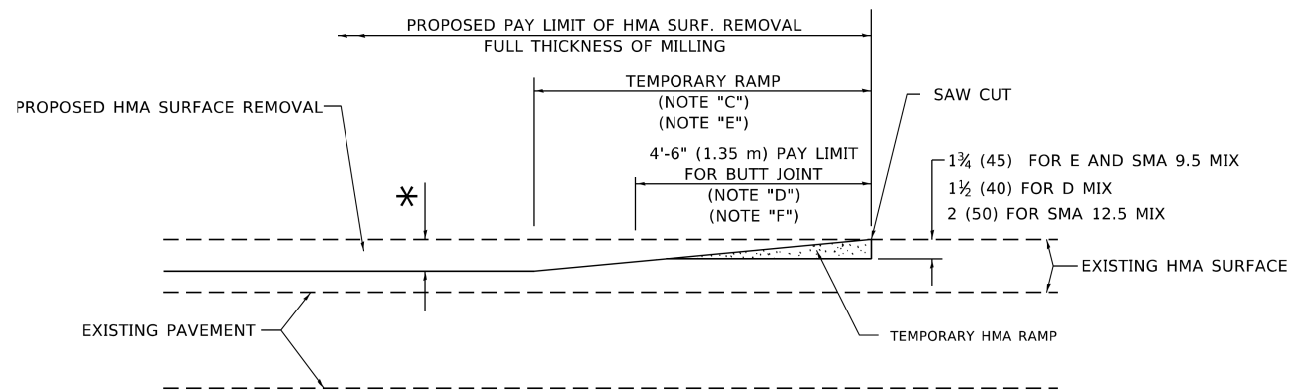
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1653	22-00020-00-RS	LAKE	31	26A
BD600-06 (BD-24)		CONTRACT NO. 61K49		
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

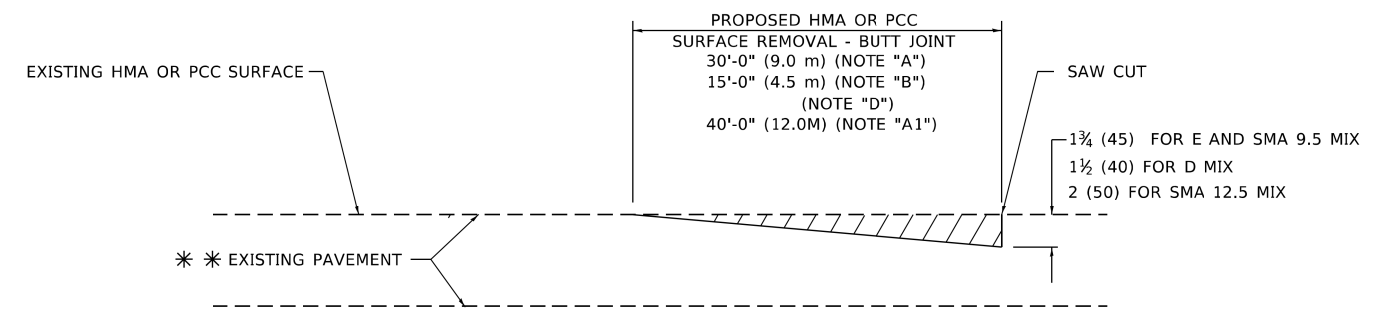
OPTION 1



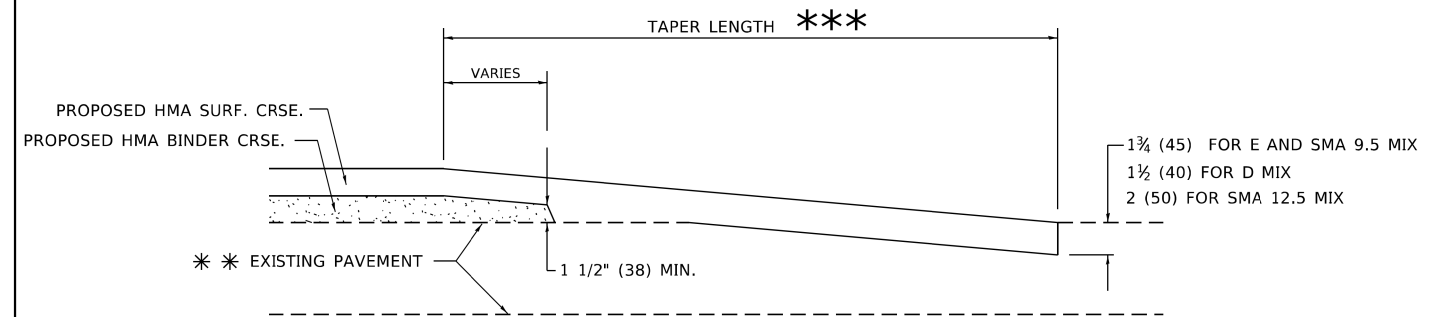
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

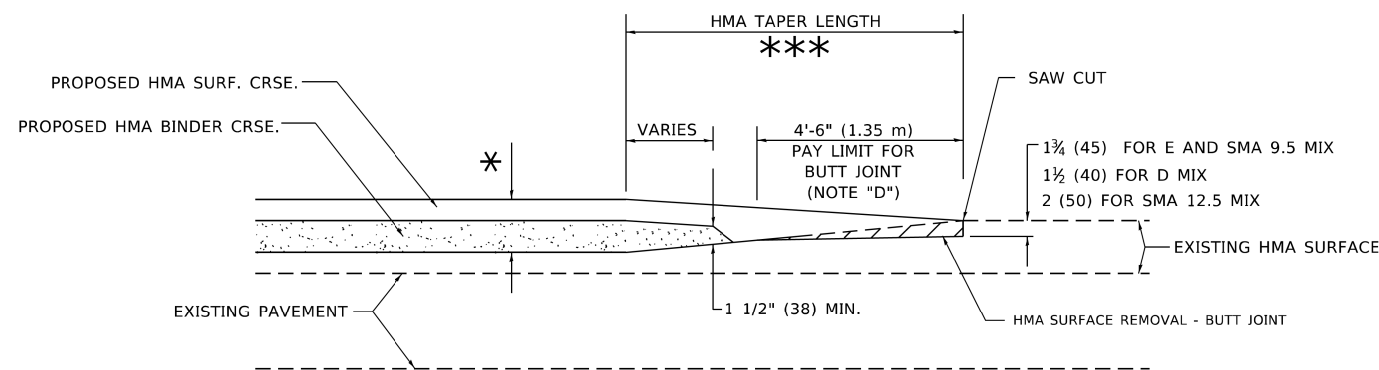
GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

MODEL: Default
FILE: \\miller-cw-bentley.com\pwwd\DOT\Documents\DOT_Offices\Miller_1\Projects\BIS\0213\CAD\DATA\CAD\Sheets\0323.dgn

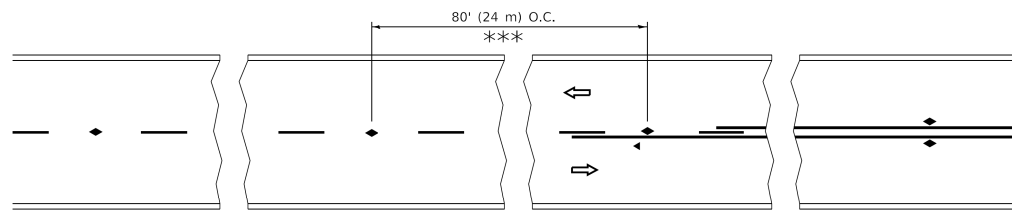
USER NAME = Lawrence.DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

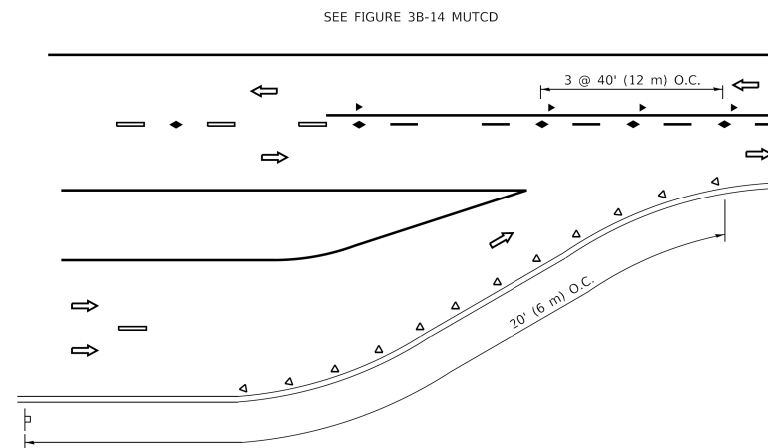
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1653	22-00020-00-RS	LAKE	31	27
BD400-05 BD-32		CONTRACT NO. 61K49		
ILLINOIS FED. AID PROJECT				

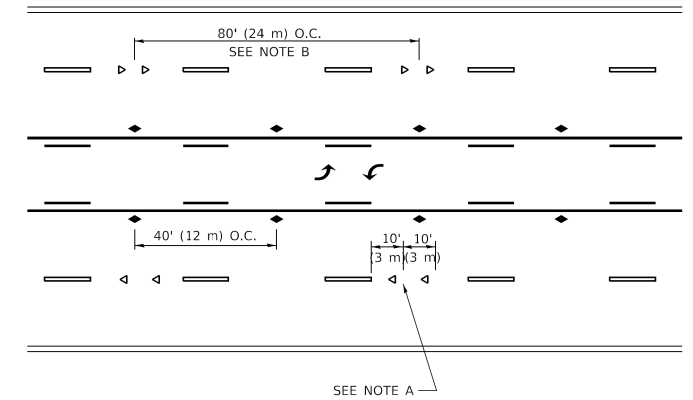


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

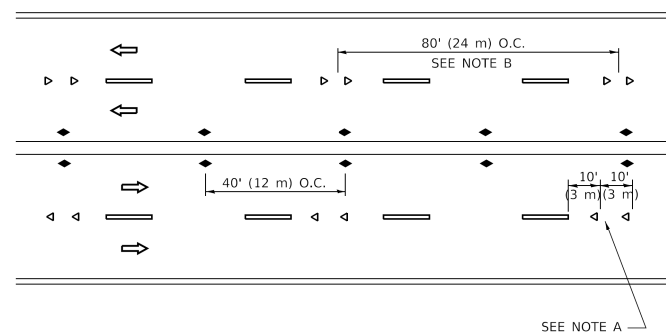
TWO-LANE/TWO-WAY



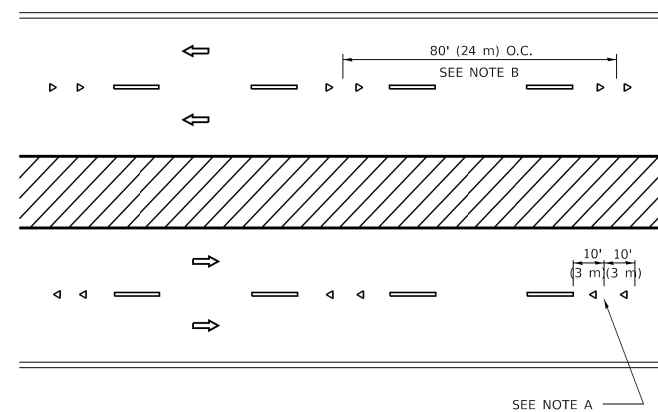
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

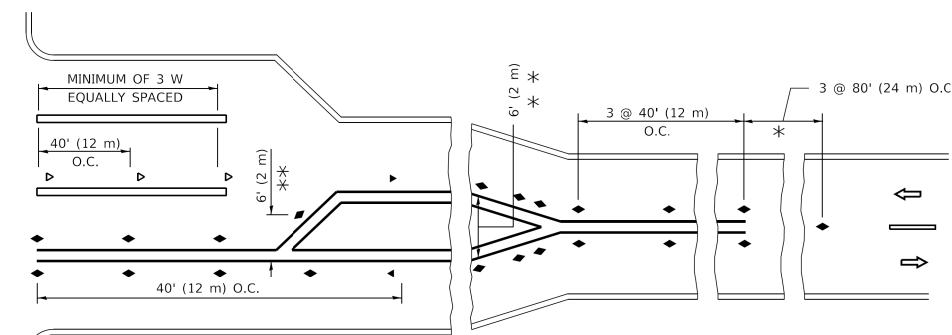
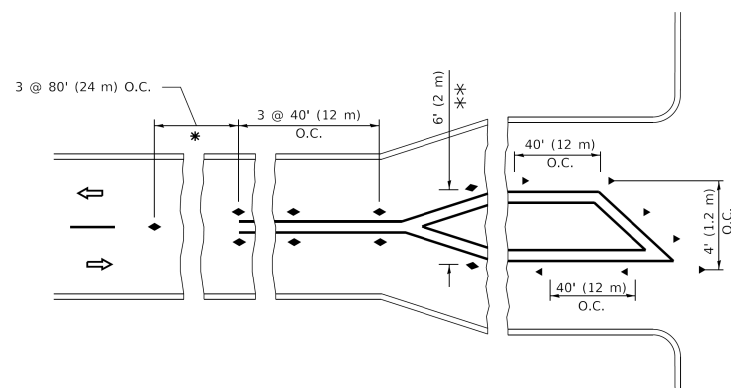
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



TURN LANES

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
 FILE: I:\work\pav\110818EB\INDTEG_IL\links\gov\p\INDOT\Documents\DOT_Offices\District_1\Projects\GIS\422-24\CADD\Draw\CAD\sheet\11.dgn

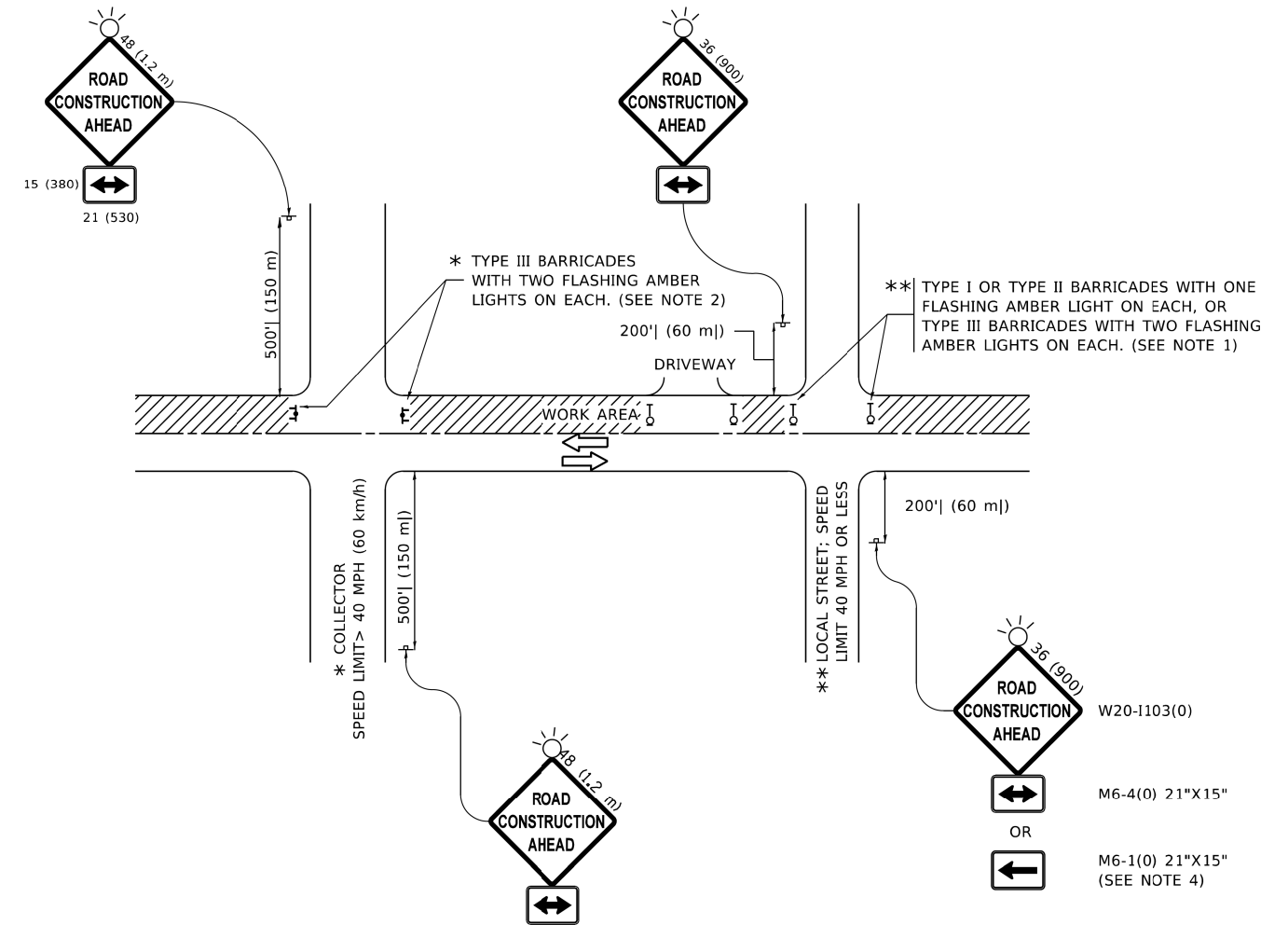
USER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 03-12-99
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 50.0000" / ft.	CHECKED -	REVISED - C. JUCIUS 09-09-09
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 07-01-13

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1653	22-00020-00-RS	LAKE	31	28A
TC-11			CONTRACT NO. 61K49	
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
FILE: \\nrc\pww\lsc-pw-bentley.com\pww\DOT\Documents\DOT_Offices\Di\lsc\1\Projects\Di\H502732\ACAD\DATA\CAD\Sheet\TC10.dgn

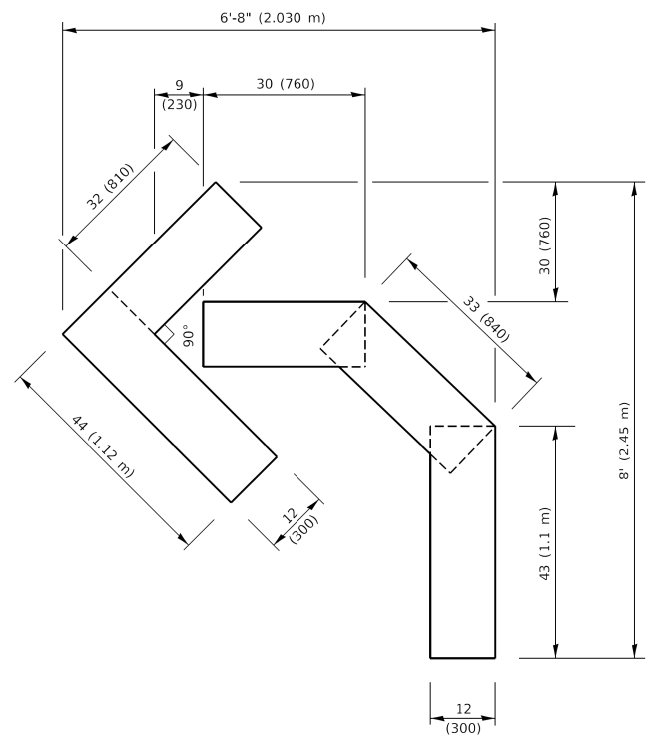
USER NAME = Lawrence,DeManche	DESIGNED - L.H.A.	REVISED - T. RAMMACHER 01-06-00
	DRAWN -	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 5/3/2024	DATE - 06-89	REVISED - D. SENDERAK 05-03-24

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

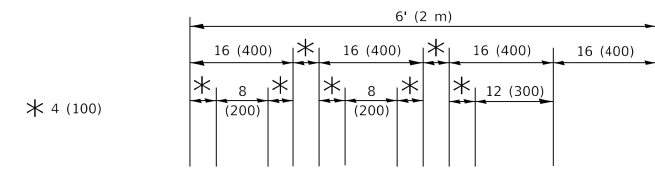
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1653	22-00020-00-RS	LAKE	31	28
TC-10			CONTRACT NO. 61K49	
ILLINOIS FED. AID PROJECT				



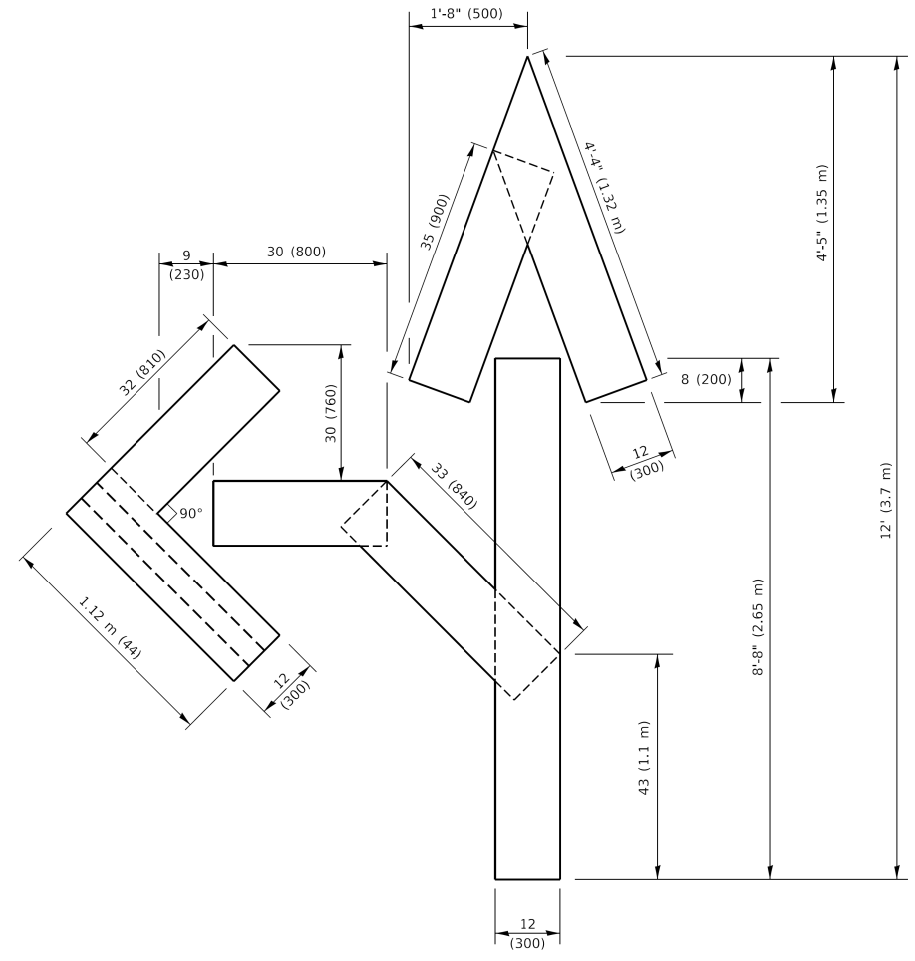
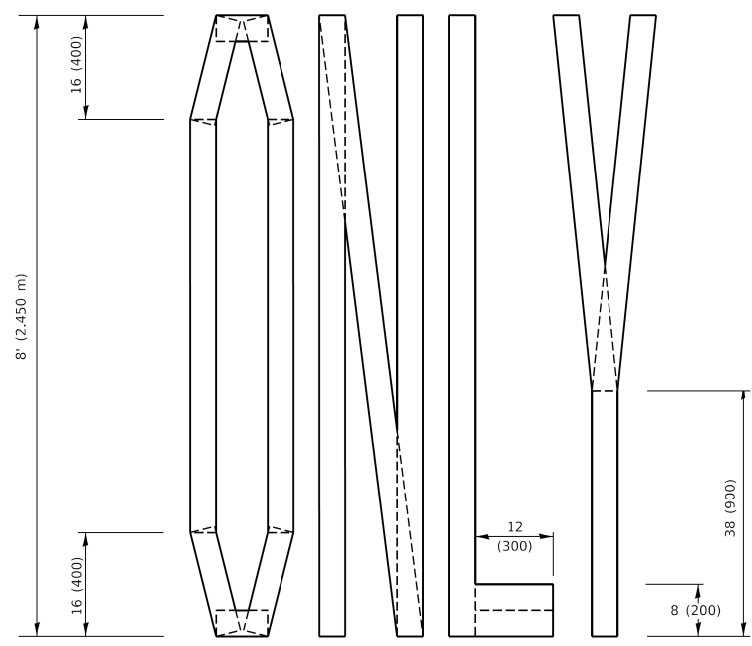
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

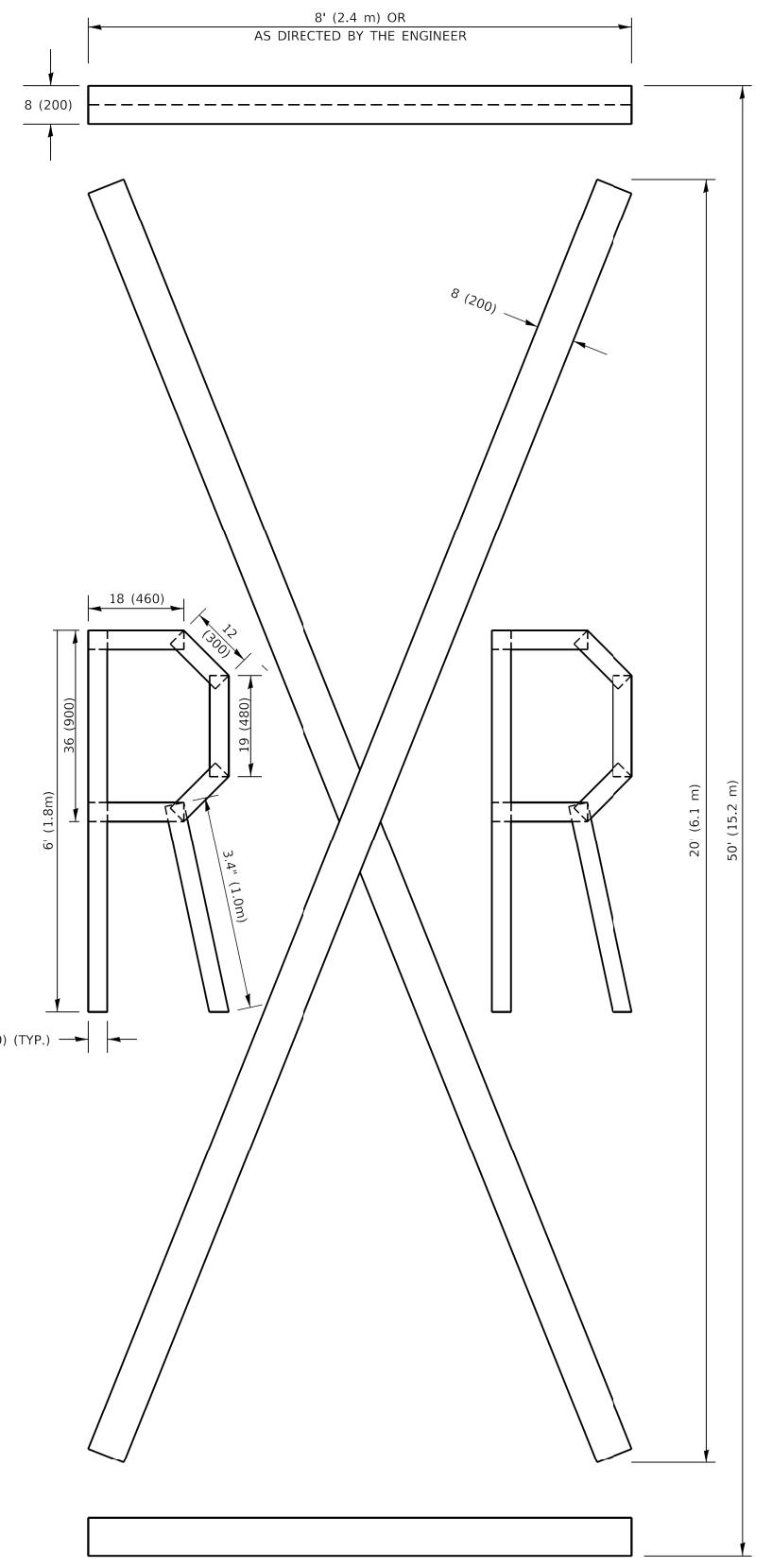


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
FILE NAME: I:\U084EB\DOT\Illinois.gov\PIWDOT\Documents\DOT_Offices\District 1\Projects\Dist5\S22-24\CAD\Drawings\CAD\Sheets\16.dgn

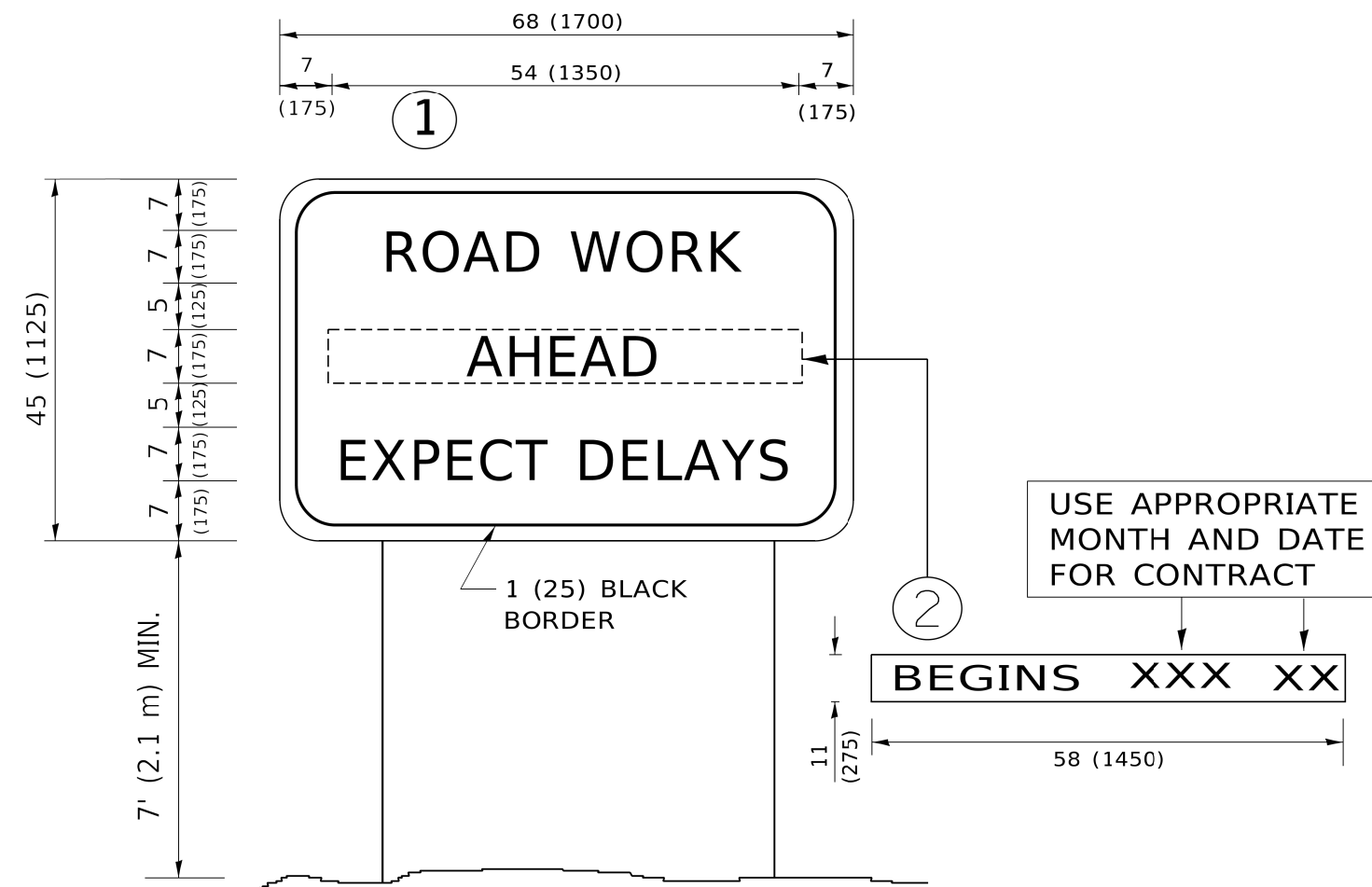
USER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT SCALE = 50.0068" / in.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 3/4/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1653	22-00020-00-RS	LAKE	31	30
TC-16			CONTRACT NO. 61K49	
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
 FILE: \\miller-pw\ill084\FEB\NTEC\Illinois.gov-PWIDOT\Documents\DOT_Offices\District 1\Projects\DUH\S22\2-21\CADD\ba\CAD\ba\sheet1c22.dgn

USER NAME = footemj	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 50.0000 ' / ft.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1653	22-00020-00-RS	LAKE	31	31
TC-22			CONTRACT NO. 61K49	
ILLINOIS FED. AID PROJECT				