

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	06-00100-00-RS	COOK	26	1
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83953				

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
PLANS FOR

**PROPOSED LOCAL AGENCY IMPROVEMENT  
FEDERAL-AID URBAN PROJECT**

F.A.U. ROUTE 2770 (SHERMER ROAD)  
F.A.U. ROUTE 1324 (DEMPSTER STREET)  
TO F.A.P. ROUTE 339 (GOLF ROAD)  
**STREET RESURFACING**  
SECTION 06-00100-00-RS  
PROJECT NO: M-8003(694)  
**COOK COUNTY**  
C-91-028-07

FOR INDEX OF SHEETS, SEE SHEET NO. 2

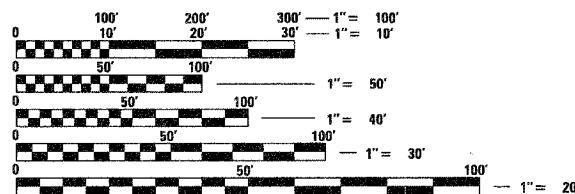
**DESIGN DESIGNATION**

SHERMER ROAD  
DESIGN SPEED = 30 MPH  
POSTED SPEED = 30 MPH

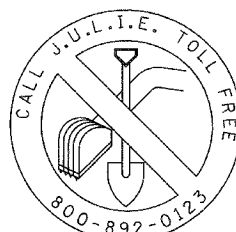
**TRAFFIC DATA**

SHERMER ROAD  
EXISTING ADT = 4,750 (2006)

PROJECT LOCATED IN THE  
VILLAGE OF MORTON GROVE

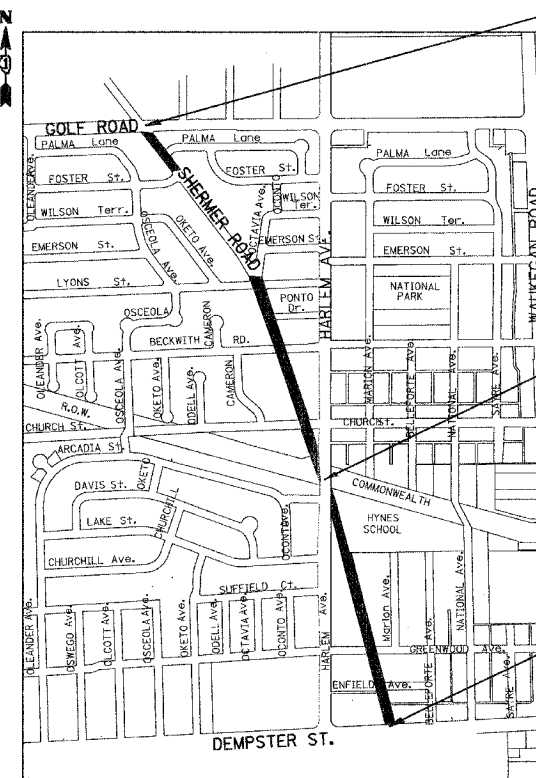


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



CONTRACT NO : 83953

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123



END PROJECT  
STA. 151 + 08

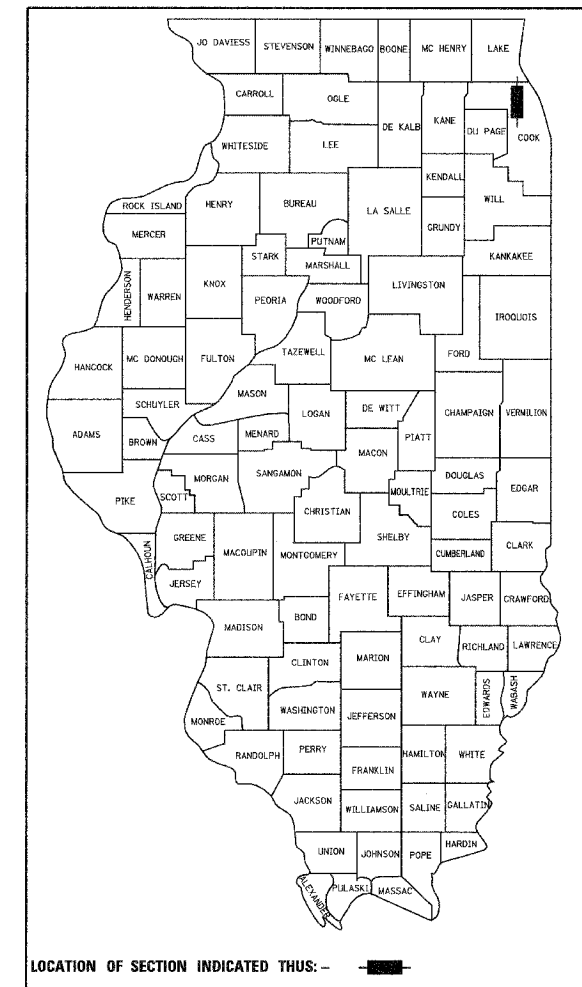
OMISSION  
STA. 116 + 71 TO  
STA. 117 + 99

BEGIN PROJECT  
STA. 95 + 81

LOCATION MAP  
T 41N, R 9E (MAINE TOWNSHIP)  
T 41N, R 13E (HILES TOWNSHIP)  
GROSS LENGTH OF PROJECT = 5527 FT = 1.05 MI  
NET LENGTH OF PROJECT = 5399 FT = 1.02 MI



DATE: 5/25/2007  
SEAL EXPIRES: 11/30/2007



LOCATION OF SECTION INDICATED THIS: -

<b>IDOT DIVISION OF HIGHWAYS</b>	
APPROVED	<i>[Signature]</i> VILLAGE OF MORTON GROVE VILLAGE ENGINEER
PASSED	May 29, 2007 <i>[Signature]</i> DISTRICT 1 ENGINEER OF LOCAL ROAD & STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	May 29, 2007 <i>[Signature]</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

**Ciorba Group, Inc.**  
DESIGN FIRM  
REGISTRATION NUMBER  
184-001016  
CONSULTING ENGINEERS  
SUITE 402, 5507 NORTH CUMBERLAND AVE  
CHICAGO, ILLINOIS 60656 :: (773) 775-4009

PLANS PREPARED BY: CIORBA GROUP  
PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

IDOT FEDERAL AID DESIGN ENGINEER: ABIGAIL L. WILGREEN (847) 705-4233  
CONSULTANT: CIORBA GROUP, INC. (773) 775-4009

GENERAL NOTES

**A. GENERAL**

1. **EARTHWORK AND PAVING SPECIFICATIONS**  
THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", LATEST EDITION, AND ALL ADDENDA THERETO, SHALL GOVERN THE PAVING WORK UNDER THIS CONTRACT, EXCEPT AS MODIFIED BY THESE SPECIFICATIONS.
2. **UNDERGROUND SPECIFICATIONS**  
THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", LATEST EDITION, SHALL GOVERN THE UNDERGROUND WORK UNDER THIS CONTRACT, EXCEPT AS MODIFIED BY THESE SPECIFICATIONS.
3. **WORK**  
WORK SHALL MEAN THE FURNISHING OF ALL LABOR, MATERIAL, EQUIPMENT AND OTHER INCIDENTALS NECESSARY OR CONVENIENT TO THE SUCCESSFUL COMPLETION OF THE IMPROVEMENTS DESIGNED BY THE ENGINEER AND SHOWN ON THIS SET OF PLANS. WORK SHALL ALSO INCLUDE CARRYING OUT OF ALL THE DUTIES AND OBLIGATIONS IMPOSED BY THE AGREEMENT BETWEEN THE OWNER AND THE CONTRACTOR FOR THE PROPER INSTALLATION AND ACCEPTANCE OF THE IMPROVEMENTS DESIGNED BY THE ENGINEER AND SHOWN ON THIS SET OF PLANS.
4. **COMPLIANCE WITH LOCAL ORDINANCES**  
ALL WORK PERFORMED UNDER THIS CONTRACT SHALL BE IN ACCORDANCE WITH THE VILLAGE OF MORTON GROVE'S ORDINANCES AND STANDARDS.
5. **EXISTING UTILITIES**  
WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION AND ELEVATIONS OF UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF THE ENGINEER AS TO LOCATION AND ELEVATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE BIDDERS. THE ENGINEER AND THE OWNER ASSUME NO RESPONSIBILITY WHATEVER IN RESPECT TO THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS RELATIVE TO THE LOCATION AND ELEVATION OF UTILITY FACILITIES, NOR THE MANNER IN WHICH THEY ARE TO BE REMOVED OR ADJUSTED. IT IS EACH CONTRACTOR'S AND SUBCONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND DETERMINE THE ACTUAL LOCATION AND ELEVATION OF ALL UTILITIES. EACH CONTRACTOR AND SUBCONTRACTOR SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION RELATIVE TO THE LOCATION AND ELEVATION OF THEIR FACILITIES AND THE WORKING SCHEDULES OF THE UTILITY COMPANIES FOR REMOVING OR ADJUSTING THEM.  
  
EACH CONTRACTOR AND SUBCONTRACTOR PERFORMING UNDERGROUND WORK SHALL CONTACT J.U.L.L.E. AT 1-800-892-0123 AT LEAST 48 HOURS PRIOR TO START OF WORK.
6. **SAFETY OF WORK**  
EACH CONTRACTOR AND SUBCONTRACTOR SHALL BE RESPONSIBLE FOR INITIATING, MAINTAINING AND SUPERVISING ALL SAFETY PRECAUTIONS AND PROGRAMS IN CONNECTION WITH THE WORK. EACH CONTRACTOR AND SUBCONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE SAFETY OF, AND SHALL PROVIDE THE NECESSARY PROTECTION TO PREVENT DAMAGE, INJURY OR LOSS TO:
  - A) ALL EMPLOYEES ON THE WORK AND OTHER PERSONS AND ORGANIZATIONS WHO MAY BE AFFECTED THEREBY;
  - B) ALL THE WORK AND MATERIALS AND EQUIPMENT TO BE INCORPORATED THEREIN, WHETHER IN STORAGE ON OR OFF THE SITE; AND
  - C) OTHER PROPERTY AT THE SITE OR ADJACENT THERETO, INCLUDING TREES, SHRUBS, LAWNS, WALKS, PAVEMENTS, ROADWAYS, STRUCTURES, UTILITIES AND UNDERGROUND FACILITIES NOT DESIGNATED FOR REMOVAL, RELOCATIONS OR REPLACEMENT IN THE COURSE OF CONSTRUCTION.
  - D) EACH CONTRACTOR AND SUBCONTRACTOR SHALL DESIGNATE A RESPONSIBLE REPRESENTATIVE AT THE SITE WHOSE DUTY SHALL BE THE PREVENTION OF ACCIDENTS. THIS PERSON SHALL BE THE CONTRACTOR'S SUPERINTENDENT UNLESS OTHERWISE DESIGNATED IN WRITING BY THE CONTRACTOR TO OWNER.

7. **EXISTING ELEVATIONS AND LOCATIONS**  
EVERY CONTRACTOR AND SUBCONTRACTOR SHALL VERIFY THE ELEVATIONS AND LOCATIONS OF ALL EXISTING INFORMATION AS SHOWN ON THE PLANS AND NOTIFY THE ENGINEER OF ALL DISCREPANCIES PRIOR TO THE COMMENCEMENT OF THE WORK.
8. **NOTIFICATION**  
THE VILLAGE OF MORTON GROVE'S ENGINEER SHALL BE NOTIFIED BY EVERY CONTRACTOR AND SUBCONTRACTOR AT LEAST TWO (2) WORKING DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION.
9. **COMPLIANCE WITH LAWS AND REGULATIONS**  
EVERY CONTRACTOR SHALL COMPLY WITH ALL LOCAL AND STATE SAFETY LAWS, REGULATIONS AND ORDINANCES, AND FEDERAL SAFETY REGULATIONS AS OUTLINED IN THE LATEST REVISIONS OF THE FEDERAL CONSTRUCTION SAFETY STANDARDS AND WITH ALL PROVISIONS AND REGULATIONS OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) STANDARDS. EACH CONTRACTOR AND SUBCONTRACTOR IS RESPONSIBLE FOR PROVIDING A SAFE WORKING PLACE FOR HIS EMPLOYEES. EACH CONTRACTOR AND SUBCONTRACTOR IS RESPONSIBLE FOR THE SUPERVISION, DIRECTION AND CONDUCT OF THEIR EMPLOYEES, AGENTS, MATERIAL SUPPLIERS AND VENDORS.
10. **PAVEMENT DISTURBED DURING CONSTRUCTION**  
ANY EXISTING ROADWAY PAVEMENT OR SHOULDER DISTURBED DURING CONSTRUCTION SHALL BE RESTORED IN ACCORDANCE WITH I.D.O.T. AND VILLAGE OF MORTON GROVE STANDARDS.
11. **FINAL FRAME ADJUSTMENTS**  
THE PAVING CONTRACTOR SHALL MAKE ALL NECESSARY FINAL ADJUSTMENTS TO EXISTING AND PROPOSED FRAMES, GRATES, LIDS AND STRUCTURES TO MEET FINAL FINISHED GRADES.
12. **RECORD DRAWINGS**  
EACH CONTRACTOR AND SUBCONTRACTOR SHALL MAINTAIN AND KEEP UP TO DATE A SET OF "RECORD DRAWINGS" SHOWING ALL CHANGES FROM THE ORIGINAL PLANS. ALL CONTRACTORS AND SUBCONTRACTORS SHALL DELIVER THE "RECORD DRAWINGS" TO THE ENGINEER AT THE CONCLUSION OF THE PROJECT. THE ENGINEER WILL TRANSFER THE INFORMATION TO THE ORIGINAL PLANS.
13. **CONTRACTOR(S)**  
FOR THE PURPOSES OF THESE SPECIFICATIONS, THE TERMS CONTRACTOR AND SUBCONTRACTOR SHALL MEAN ANY PERSON OR ENTITY THAT PROVIDES WORK FOR THE IMPROVEMENTS AS SHOWN ON THESE PLANS. THE ENGINEER, OWNER AND THE ARCHITECT ARE NOT CONSIDERED CONTRACTORS OR SUBCONTRACTORS.  
  
ANY SIGN LOCATED IN THE PUBLIC RIGHT-OF-WAY WHICH INTERFERES WITH CONSTRUCTION OF THE PROPOSED ROADWAY WORK OR LIGHTING SYSTEM THAT IS INTENDED TO BE MAINTAINED SHALL BE RELOCATED. THIS WORK IS INCIDENTAL TO THIS PROJECT. ALL WORK INVOLVING SIGN REMOVAL SHALL BE GOVERNED BY THE FOLLOWING REQUIREMENTS:  
  
EVERY SIGN RELOCATED MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND BE VISIBLE TO TRAFFIC FOR WHICH IT WAS INTENDED. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF TEMPORARY SETTING. THIS WORK INCLUDES PROVIDING A SIGN POST FOR THE TEMPORARY LOCATION, SHOULD SUCH A POST BE NECESSARY.  
  
ALL SIGNS SHALL BE RE-ERECTED IN THE ORIGINAL LOCATION AS THE IMPROVEMENTS ARE COMPLETED.  
  
THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY SIGNS DAMAGED BY HIS/HER CONSTRUCTION ACTIVITIES AND WILL REPLACE THEM AT NO COST TO THE VILLAGE. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.
15. **GRASS AREAS WITHIN RIGHT-OF-WAY**  
ALL GRASS AREAS WITHIN THE RIGHT-OF-WAY DISTURBED DURING CONSTRUCTION SHALL BE RESTORED BY PLACING A MINIMUM 4" TOPSOIL AND SALT TOLERANT SOD. 4" TOPSOIL AND SALT TOLERANT SOD ARE INCIDENTAL TO THE CONTRACT.

- B. PAVING**
1. THE PAVING CONTRACTOR IS RESPONSIBLE FOR MAINTAINING PROPER DRAINAGE ON THE PROPOSED PAVEMENT. WHERE THE CONTRACTOR IS IN DOUBT ABOUT THE PROPER DIRECTION OF DRAINAGE, HE SHALL REQUEST ASSISTANCE FROM THE ENGINEER.
  2. FULL DEPTH SAW CUTS SHALL BE USED TO REMOVE EXISTING PAVEMENT, CURB AND GUTTER, SIDEWALK, DRIVEWAYS, BUTT JOINTS AND APPURTENANCES FROM MATERIAL TO REMAIN, IN ACCORDANCE WITH SECTION 440 OF THE "STANDARD SPECIFICATIONS". THE COST OF THE SAWING SHALL BE INCLUDED IN THE COST OF THE ITEM REMOVED.
  3. BASE COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY BACKFILLED TO THE SATISFACTION OF THE ENGINEER.
  4. PRIOR TO PLACING HOT MIX ASPHALT ADJACENT TO EXISTING PAVEMENT TO REMAIN, THE EXPOSED EDGE SHALL BE CLEANED OF LOOSE MATERIAL TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE HOT MIX ASPHALT BEING PLACED.
  5. THE CONTRACTOR SHALL NOT CROSS COMPLETED BASE COURSE, OR EXISTING PAVEMENT NOT SCHEDULED TO BE REMOVED WITH TRACK EQUIPMENT.
  6. PRIME COAT - PRIME COAT MUST BE INSTALLED NO EARLIER THAN TWENTY-FOUR (24) HOURS PRIOR TO PLACEMENT OF HOT MIX ASPHALT.
  7. BUTT JOINTS - BUTT JOINT WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
  8. THE LOCATION OF PAVEMENT PATCHING, COMBINATION CONCRETE CURB AND GUTTER, SIDEWALK AND DRIVEWAY REMOVAL AND REPLACEMENT ARE AS SHOWN ON THE PLANS OR AS DETERMINED BY THE ENGINEER. THESE SHALL NOT EXCEED THE QUANTITIES LISTED IN THE SUMMARY OF QUANTITIES.
  9. THE CLASS D PATCH LOCATIONS INDICATED IN THE PLANS ARE APPROXIMATE. ACTUAL CLASS D PATCH LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.

- C. RESTORATION AND TREE PRESERVATION**
1. ALL DISTURBED LAWN AREAS WITHIN THE PUBLIC RIGHT-OF-WAY/EASEMENTS WILL BE RESTORED WITH 4" OF TOPSOIL AND SALT TOLERANT SOD. THE 4" TOPSOIL AND SALT TOLERANT SOD ARE INCIDENTAL TO THE CONTRACT.
  2. SODDING SHALL INCLUDE A MINIMUM OF 4 INCHES OF TOPSOIL AND SHALL BE DONE IN CONFORMANCE WITH SECTION 252 OF I.D.O.T. STANDARDS.
  3. ALL PARKWAY TREES SHALL BE PRESERVED. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY DAMAGE TO SUCH TREES AND SHALL PROVIDE TEMPORARY FENCES AND OTHER MEASURES, AS NECESSARY, WITH ALL SUCH MEASURES CONSIDERED AS INCIDENTAL TO THE CONTRACT.

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STATE HIGHWAY STANDARDS

000001-04	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-04	CURB RAMP FOR SIDEWALKS
442201-02	CLASS C AND D PATCHES
604001-02	FRAME AND LIDS, TYPE 1
604051-02	FRAME AND GRATE TYPE 11
606001-03	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-02	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701501-03	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-04	URBAN LANE CLOSURE MULTILANE INTERSECTION
701801-03	LANE CLOSURE MULTILANE 1W, 2W, CROSSWALK OR SIDEWALK CLOSURE
701606-04	URBAN LANE CLOSURE MULTILANE, 2W, MOUTABLE MEDIAN
702001-06	TRAFFIC CONTROL DEVICES
780001-01	TYPICAL PAVEMENT MARKINGS
781001-02	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
886001	DETECTOR LOOP INSTALLATIONS
886006	TYPICAL LAYOUTS FOR DETECTION LOOPS

REVISIONS	
NAME	DATE

**CG** **Clorba Group, Inc.**  
CONSULTING ENGINEERS  
5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656  
Tel. 773.775.4009 Fax 773.775.4014 Email clorba@clorba.com

VILLAGE OF MORTON GROVE  
  
SHERMER ROAD  
INDEX OF SHEETS AND  
GENERAL NOTES  
  
SCALE: N/A  
DATE: MAY, 2007  
  
DRAWN BY: CLG  
CHECKED BY: MRJ

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	D6-00100-00-RS	COOK	26	3
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 83953				

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY 1000-2A
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	70
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	5200
40600300	AGGREGATE (PRIME COAT)	TON	50
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	20
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	420
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	2250
42001300	PROTECTIVE COAT	SQ YD	950
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	280
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	70
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	800
42400800	DETECTABLE WARNINGS	SQ FT	110
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	26000
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	350
44000600	SIDEWALK REMOVAL	SQ FT	1000
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	2260
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	1000
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	240
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	500
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	4
60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	1
60250200	CATCH BASINS TO BE ADJUSTED	EACH	2
60250400	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1
60251500	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 11 FRAME AND GRATE	EACH	1
60255500	MANHOLES TO BE ADJUSTED	EACH	2
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	23
60500050	REMOVING CATCH BASINS	EACH	5
67100100	MOBILIZATION	L SUM	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	3400
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	380
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS	SQ FT	260
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	12500
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1700
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	500
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	140
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	40
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	40
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	480

X4067107 Polymerized Leveling Binder (machine Method), IL 4.75, N50 TON 1080

\* SPECIALITY ITEM

REVISIONS	
NAME	DATE

**GC** Giorba Group, Inc.  
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Tel. 773.775.4009 Fax 773.775.4014 Email: chicago@giorba.com

VILLAGE OF MORTON GROVE  
  
SHERMER ROAD  
SUMMARY OF QUANTITIES

SCALE: N.T.S. DRAWN BY: CLG  
DATE: MAY, 2007 CHECKED BY: MRJ

DATE: 05/07/07 TIME: 11:11 AM FILE: P:\PROJECTS\2007\20070507\Shermer Road\Summary of Quantities.dwg

**EXISTING CONDITIONS**

- (A) EXISTING BITUMINOUS BASE 8"±
- (B) EXISTING PCC BASE 8"±
- (C) HOT MIX ASPHALT SURFACE REMOVAL (2 1/4" TYP.)
- (D) SIDEWALK REMOVAL (AS DIRECTED BY THE ENGINEER)
- (E) COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

**PROPOSED IMPROVEMENTS**

- ① COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)
- ② CLASS D PATCHES, 10" TYPE II-IV (TYP.)
- ③ BITUMINOUS MATERIALS (PRIME COAT)
- ④ AGGREGATE (PRIME COAT)
- ⑤ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 (3/4")
- ⑥ HOT-MIX ASPHALT SURFACE COURSE, MIX D, N50 (1 1/2")
- ⑦ PORTLAND CEMENT SIDEWALK 5" (AS DIRECTED BY THE ENGINEER)
- ⑧ FULL DEPTH SAWCUT (INCIDENTAL)
- ⑨ EXISTING TIE BAR TO REMAIN IN PLACE (SEE CURB AND GUTTER REMOVAL AND REPLACEMENT DETAIL)

EXISTING TYPICAL SECTION  
STA. 95+81 TO STA. 116+71  
DEMPSTER STREET TO HARLEM AVENUE

EXISTING TYPICAL SECTION  
STA. 117+99 TO STA. 129+70  
HARLEM AVENUE TO BECKWITH ROAD

EXISTING TYPICAL SECTION  
STA. 129+70 TO STA. 151+08  
BECKWITH ROAD TO GOLF ROAD

PROPOSED TYPICAL SECTION  
STA. 95+81 TO STA. 116+71  
STA. 117+99 TO STA. 151+08

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
ITEM	AC TYPE	VOIDS
CLASS D PATCHES, 10" TYPE II TO IV (HMA BINDER, IL-19 mm)	PG 64-22*	4% @ 70 GYR
HOT-MIX ASPHALT SURFACE CSE, MIX "D", N50 (1.5")	PG 64-22	4% @ 50 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50	SBS/SBR PG 76-28/-22	4% @ 50 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN  
\*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

\*\* INTERMITTENT GUTTER OVERLAY AS DIRECTED BY ENGINEER

REVISIONS	
NAME	DATE

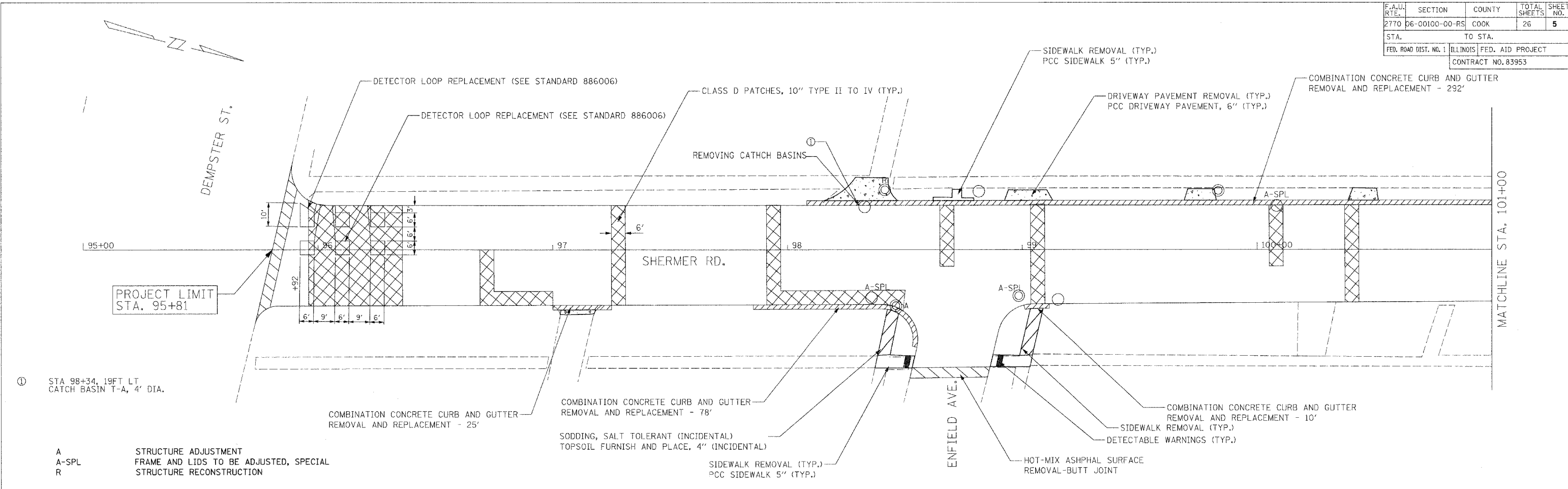
**Ciorba Group, Inc.**  
CONSULTING ENGINEERS  
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VILLAGE OF MORTON GROVE

SHERMER ROAD  
TYPICAL SECTIONS

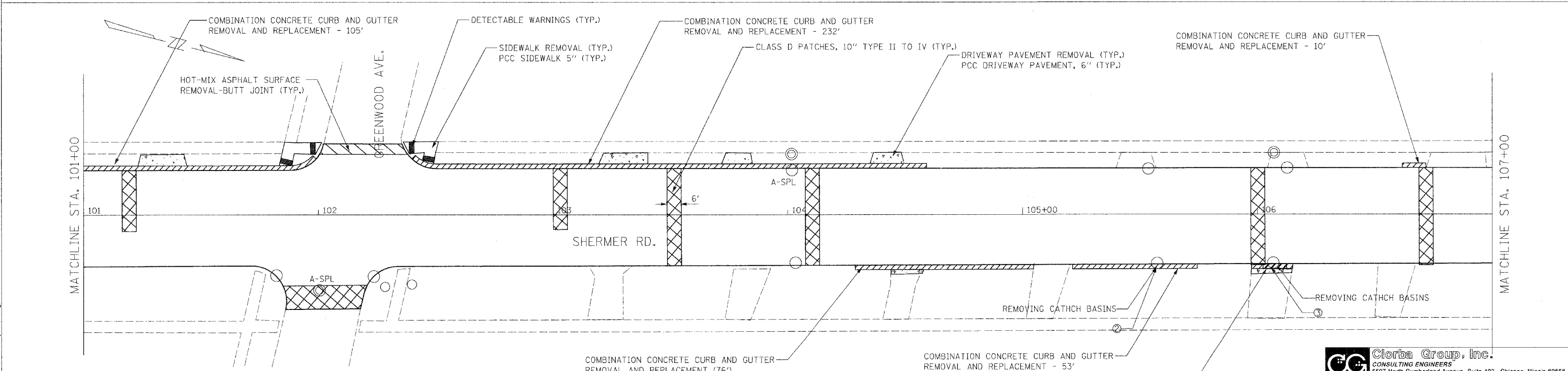
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FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83953				



① STA 98+34, 19FT LT CATCH BASIN T-A, 4' DIA.

A STRUCTURE ADJUSTMENT  
A-SPL FRAME AND LIDS TO BE ADJUSTED, SPECIAL  
R STRUCTURE RECONSTRUCTION



② STA 105+57, 21FT RT CATCH BASIN T-A, 4'DIA.  
③ STA 106+06, 21FT RT CATCH BASIN T-A, 4'DIA.

A STRUCTURE ADJUSTMENT  
A-SPL FRAME AND LIDS TO BE ADJUSTED, SPECIAL  
R STRUCTURE RECONSTRUCTION

REVISIONS	
NAME	DATE

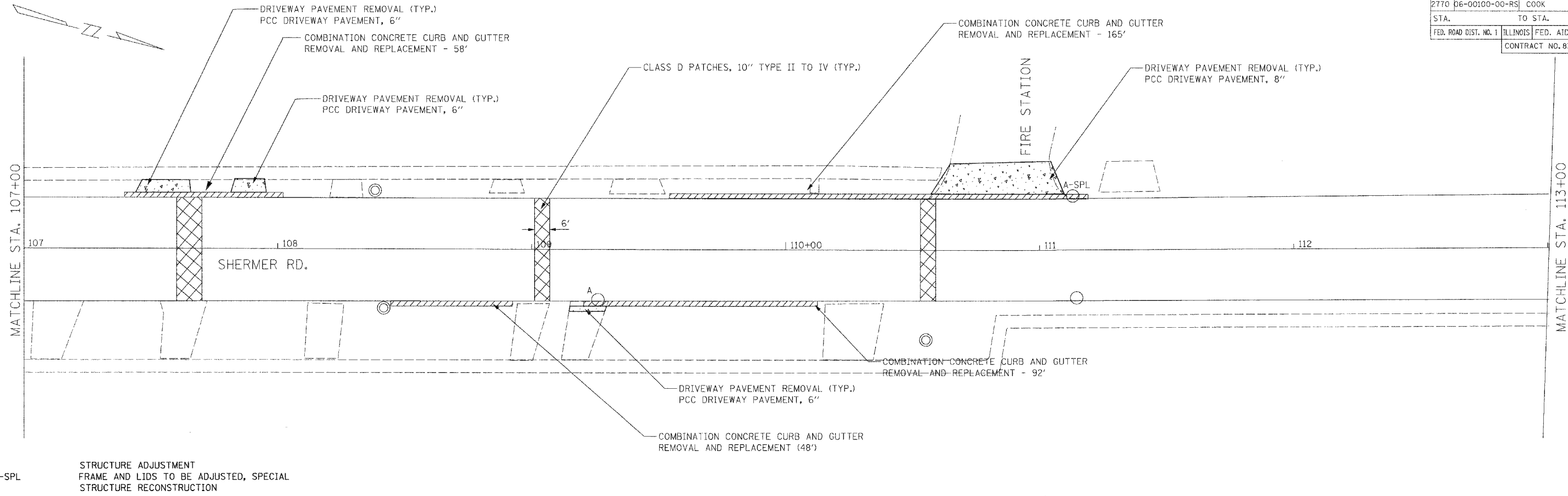
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VILLAGE OF MORTON GROVE  
**SHERMER ROAD  
PROPOSED PLAN  
STA. 95+81 TO STA. 107+00**

SCALE: 1"=20'  
DATE: MAY, 2007  
DRAWN BY: CLG  
CHECKED BY: MRJ

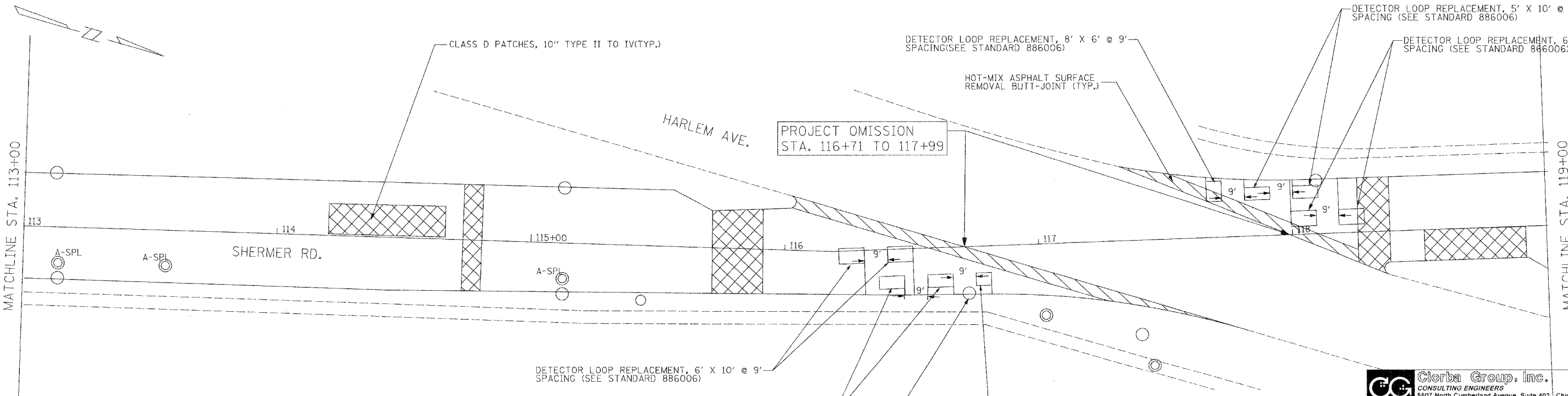
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	D6-00100-00-RS	COOK	26	6
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 83953				



A  
A-SPL  
R

STRUCTURE ADJUSTMENT  
FRAME AND LIDS TO BE ADJUSTED, SPECIAL  
STRUCTURE RECONSTRUCTION



A  
A-SPL  
R

STRUCTURE ADJUSTMENT  
FRAME AND LIDS TO BE ADJUSTED, SPECIAL  
STRUCTURE RECONSTRUCTION

PROJECT OMISSION  
STA. 116+71 TO 117+99

REVISIONS	
NAME	DATE

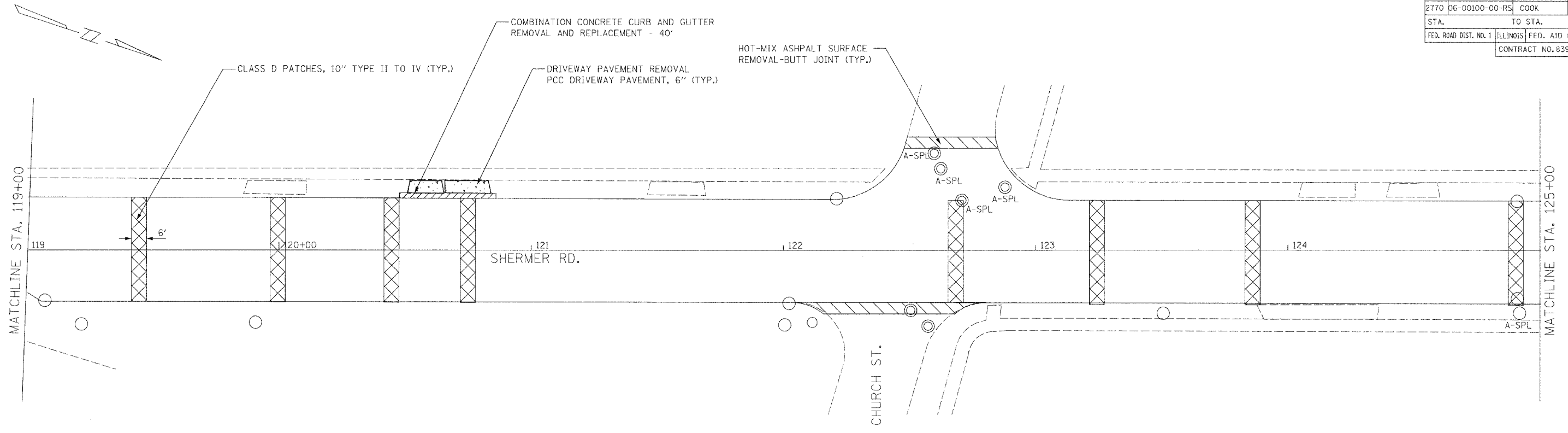
**CG** Giorba Group, Inc.  
CONSULTING ENGINEERS  
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VILLAGE OF MORTON GROVE  
SHERMER ROAD  
PROPOSED PLAN  
STA. 107+00 TO STA. 119+00

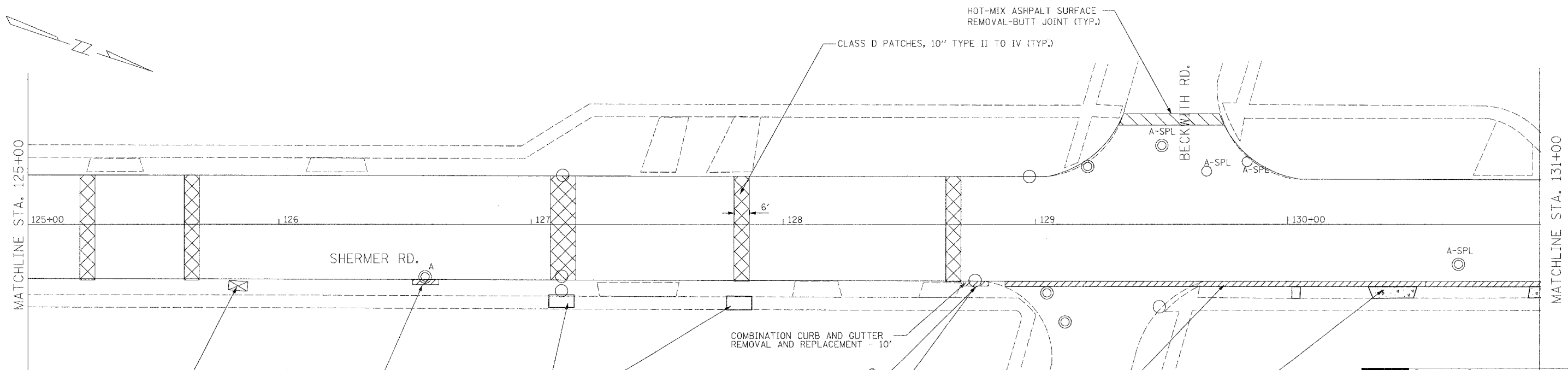
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DATE: 5/24/2007  
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	06-00100-00-RS	COOK	26	7
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 83953				



A STRUCTURE ADJUSTMENT  
A-SPL FRAME AND LIDS TO BE ADJUSTED, SPECIAL  
R STRUCTURE RECONSTRUCTION



① STA 128+75, 21FT RT CATCH BASIN TYP-C, 2' DIA.

A STRUCTURE ADJUSTMENT  
A-SPL FRAME AND LIDS TO BE ADJUSTED, SPECIAL  
R STRUCTURE RECONSTRUCTION

REVISIONS	
NAME	DATE

**CG** Ciorba Group, Inc.  
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VILLAGE OF MORTON GROVE  
SHERMER ROAD  
PROPOSED PLAN  
STA. 119+00 TO STA. 131+00

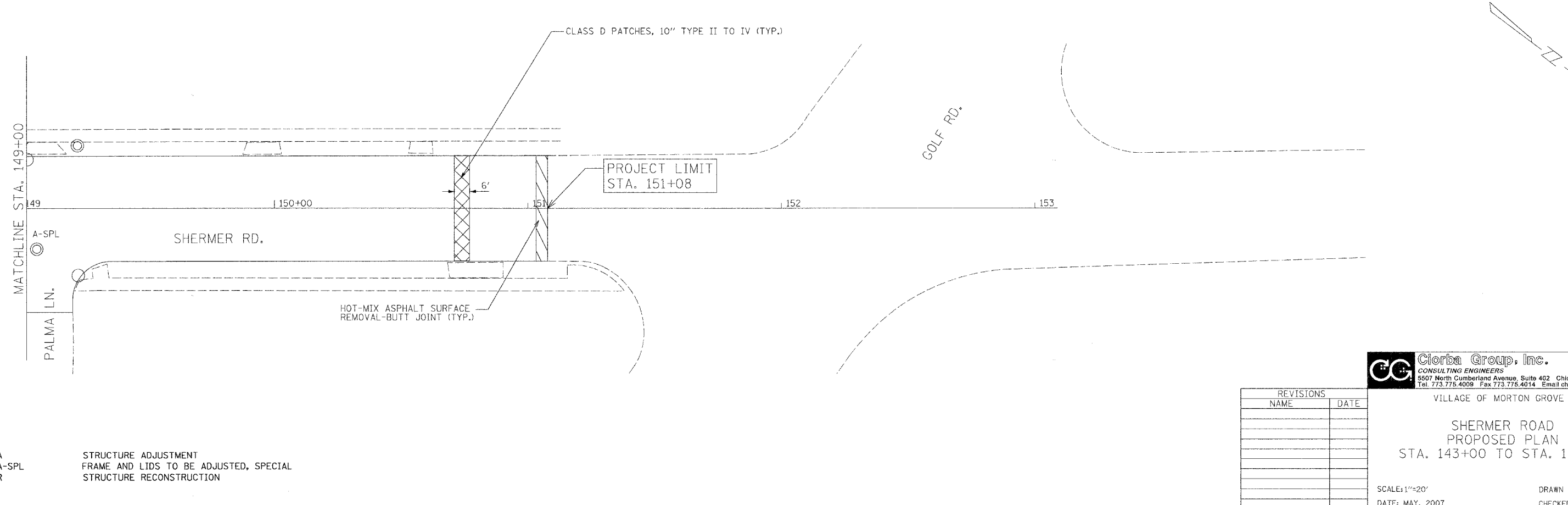
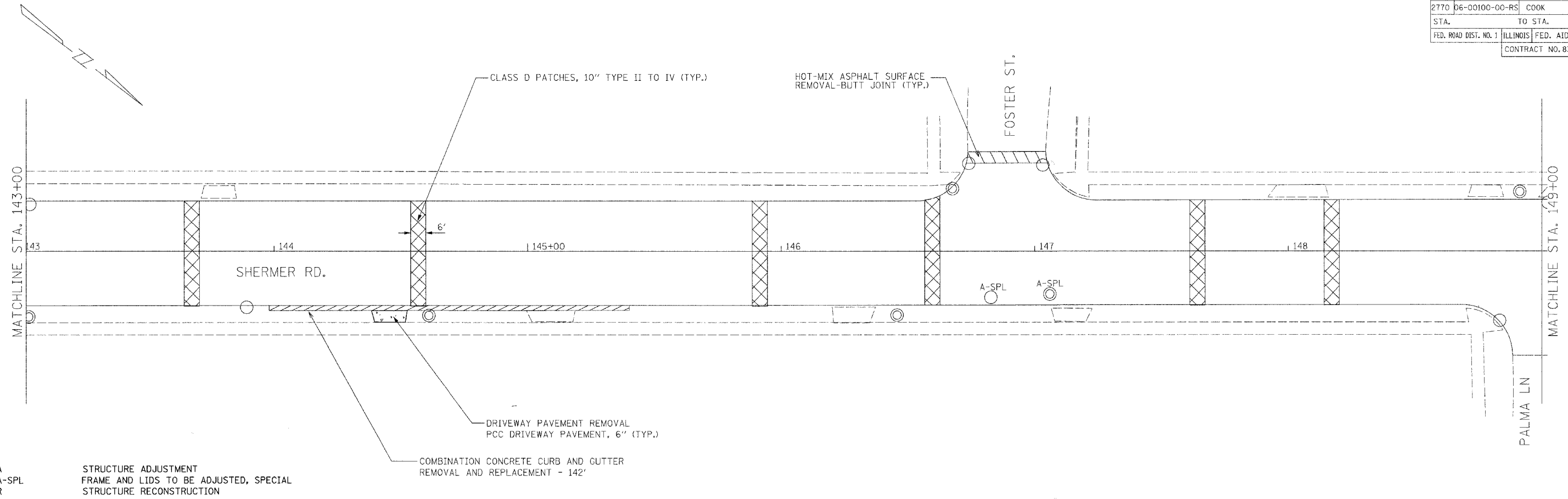
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DATE: MAY, 2007  
DRAWN BY: CLG  
CHECKED BY: MRJ

DATE: 5/24/2007  
FILENAME: N:\PROJ\0635\Design\ProposedPlan\0635plan203.dgn





F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	D6-00100-00-RS	COOK	26	9
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83953				



DATE: 6/24/2007  
FILENAME: N:\PROJ\8395\Design\ProposedPlan\603plan205.dgn

REVISIONS	
NAME	DATE

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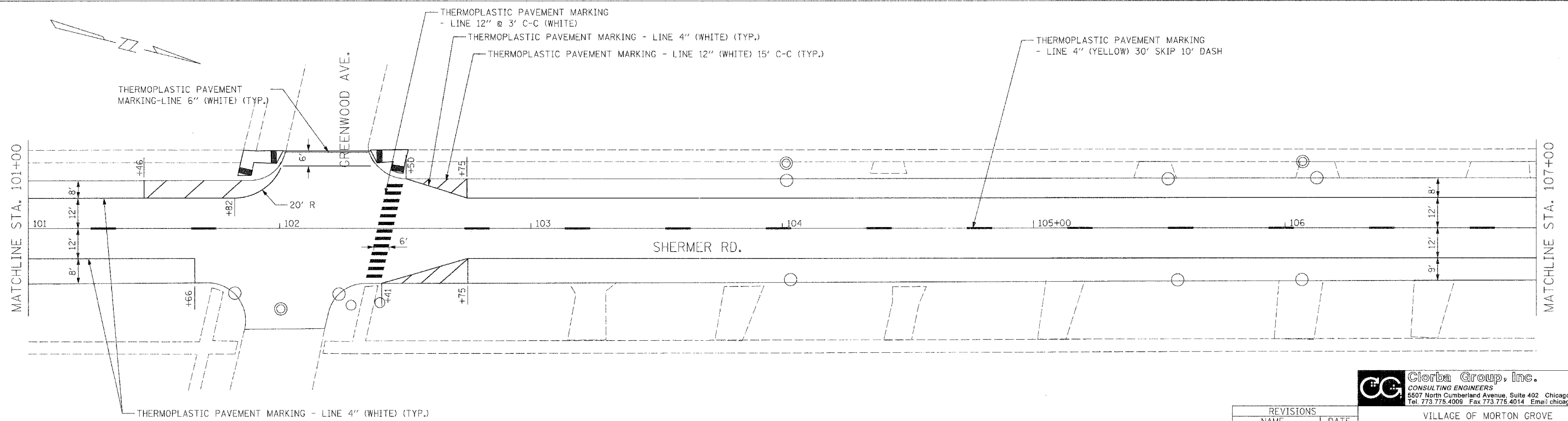
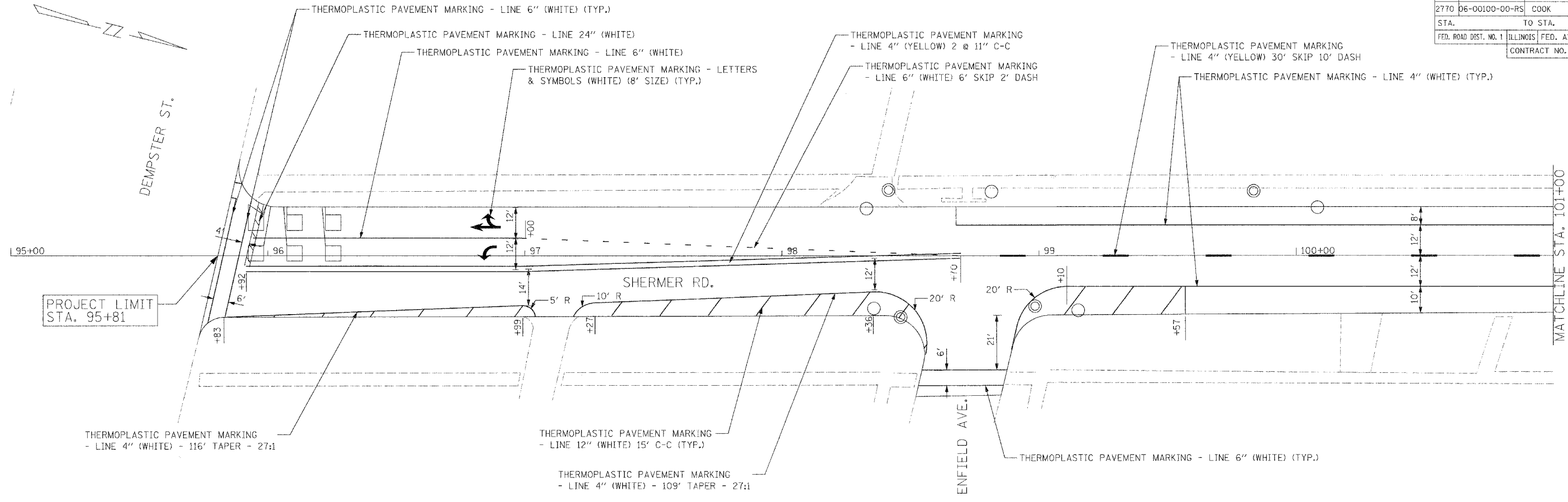
VILLAGE OF MORTON GROVE

SHERMER ROAD  
PROPOSED PLAN  
STA. 143+00 TO STA. 151+08

SCALE: 1"=20'  
DATE: MAY, 2007

DRAWN BY:  
CHECKED BY: MRJ

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	06-00100-00-RS	COOK	26	10
STA. 2770		TO STA. 2770		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 83953				



REVISIONS	
NAME	DATE

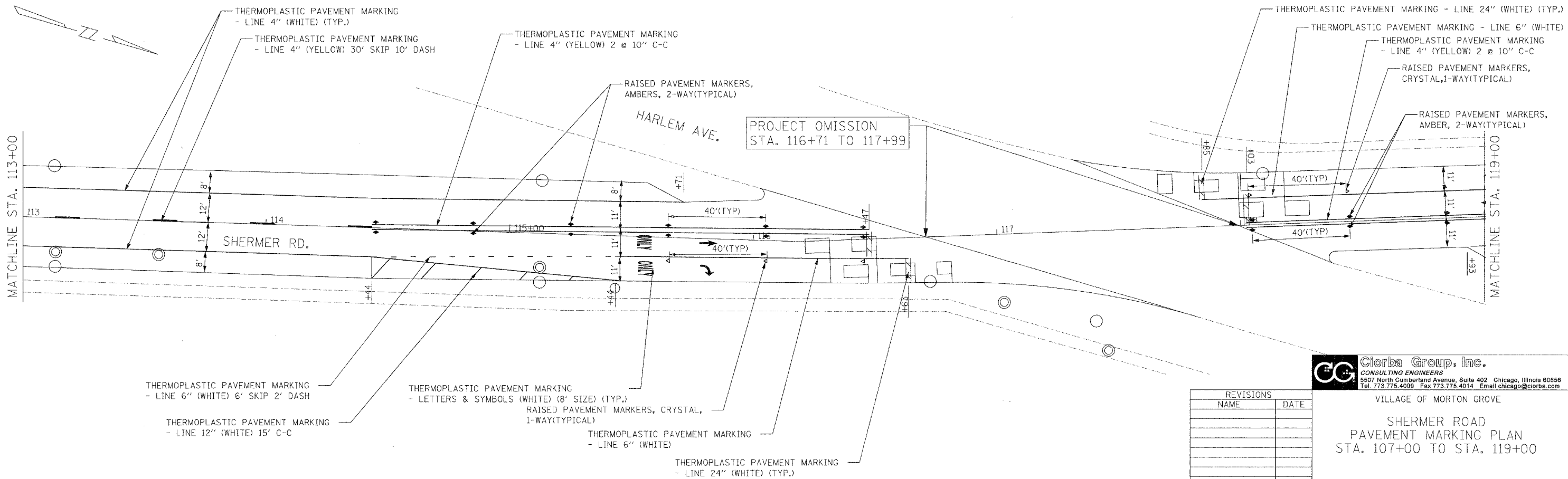
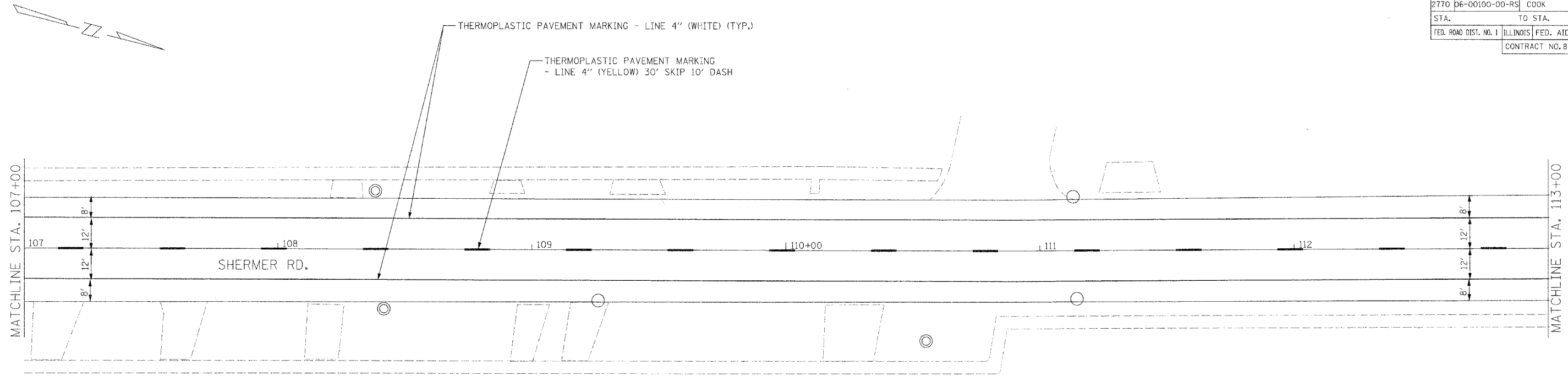
**Clorba Group, Inc.**  
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 Tel. 773.775.4009 Fax 773.775.4014 Email: chicago@clorba.com

VILLAGE OF MORTON GROVE  
 SHERMER ROAD  
 PAVEMENT MARKING PLAN  
 STA. 95+81 TO STA. 107+00

SCALE: 1"=20'  
 DATE: MAY, 2007  
 DRAWN BY: CLG  
 CHECKED BY: MRJ

DATE: 5/10/07  
 FILENAME: R:\PROJECTS\2770\2770-06-00100-00-RS.DWG

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	06-00100-00-RS	COOK	26	11
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83953				



- THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE) 6' SKIP 2' DASH
- THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE) 15' C-C
- THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE) (TYP.)
- THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW) 30' SKIP 10' DASH
- THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW) 2 @ 10" C-C
- RAISED PAVEMENT MARKERS, AMBERS, 2-WAY(TYPICAL)
- THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW) 2 @ 10" C-C
- RAISED PAVEMENT MARKERS, CRYSTAL, 1-WAY(TYPICAL)
- THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE)
- THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE) (TYP.)

REVISIONS	
NAME	DATE

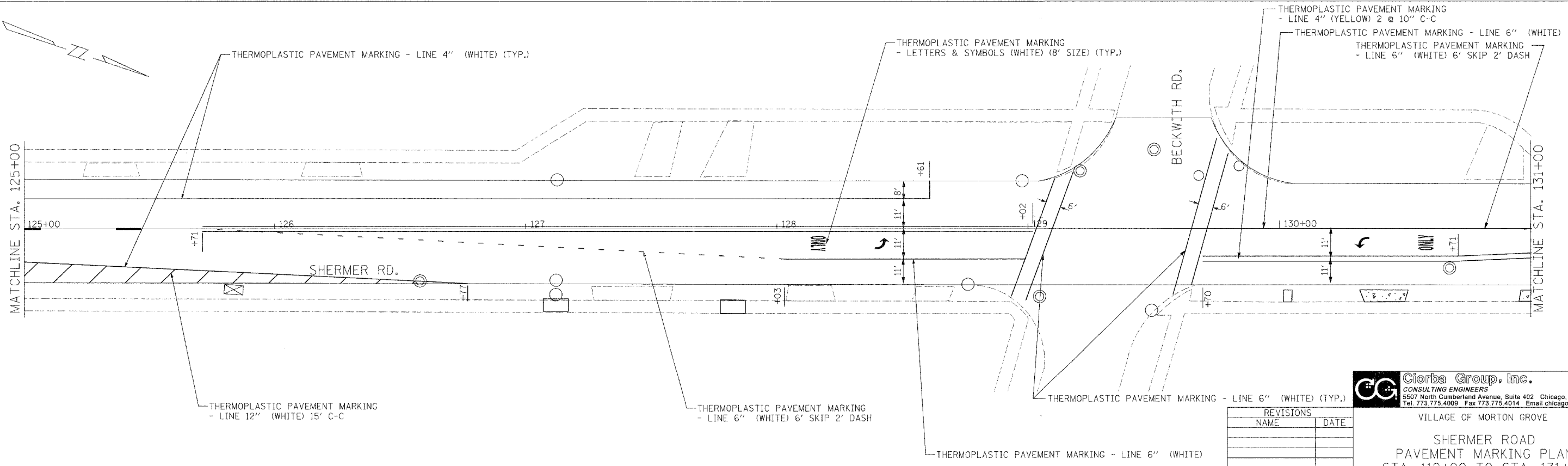
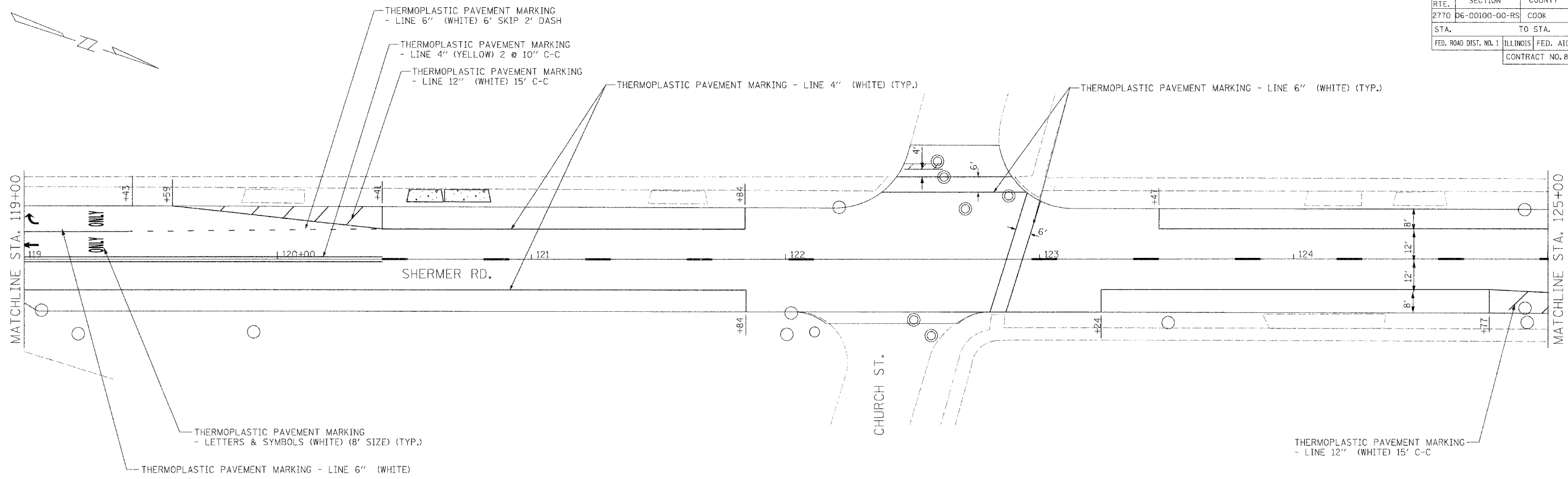
**Giorba Group, Inc.**  
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VILLAGE OF MORTON GROVE  
**SHERMER ROAD  
PAVEMENT MARKING PLAN  
STA. 107+00 TO STA. 119+00**

SCALE: 1"=20'  
DATE: MAY, 2007  
DRAWN BY: CLG  
CHECKED BY: MRJ

DATE: 05/04/07 2:00 PM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	06-00100-00-RS	COOK	26	12
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83953				



REVISIONS	
NAME	DATE

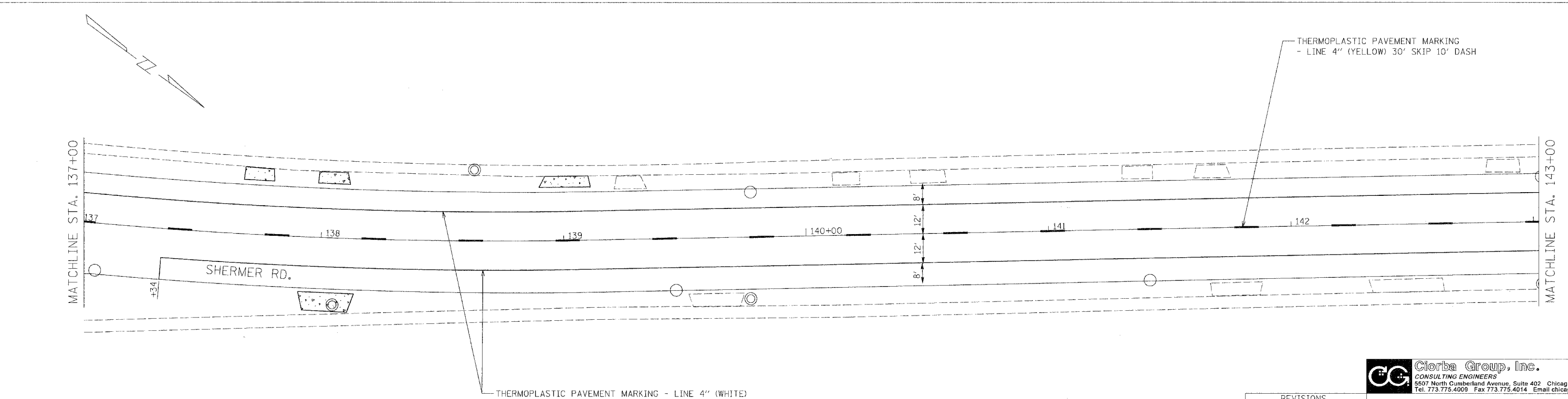
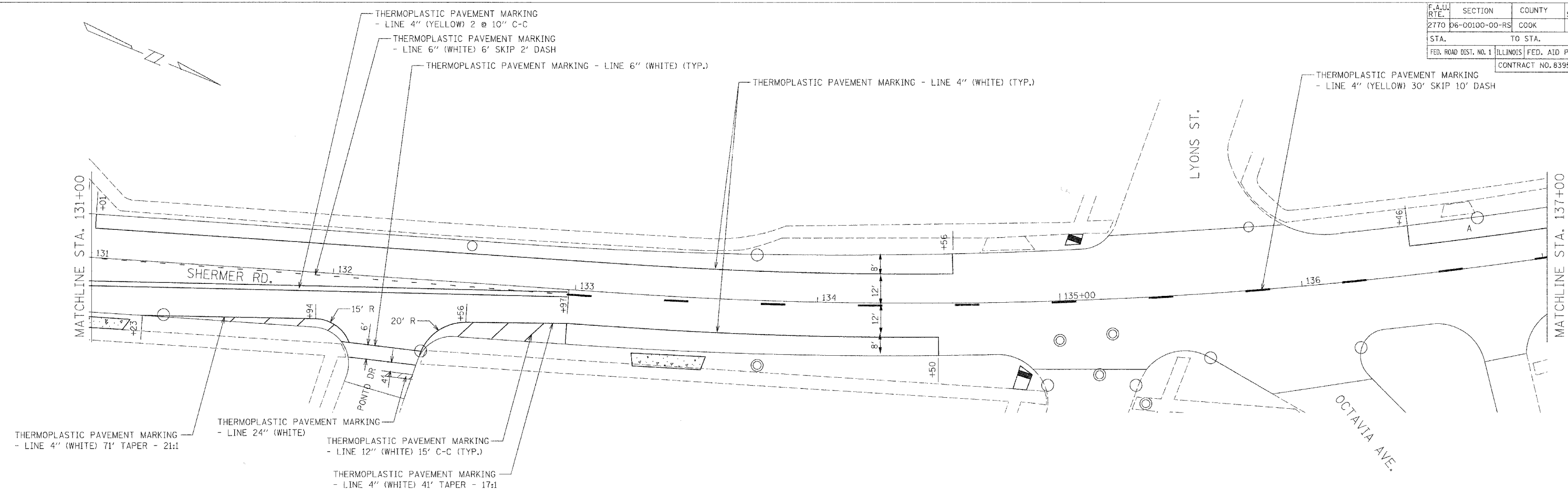
**CG** Ciorba Group, Inc.  
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VILLAGE OF MORTON GROVE  
**SHERMER ROAD  
PAVEMENT MARKING PLAN  
STA. 119+00 TO STA. 131+00**

SCALE: 1"=20'  
DATE: MAY, 2007  
DRAWN BY: CLG  
CHECKED BY: MRJ

DATE: 05/14/07  
FILENAME: MORTON\_GROVE\_SHERMER\_PAVEMENT\_MARKING\_PLAN.dwg

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	D6-00100-00-RS	COOK	26	13
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 83953				



REVISIONS	
NAME	DATE

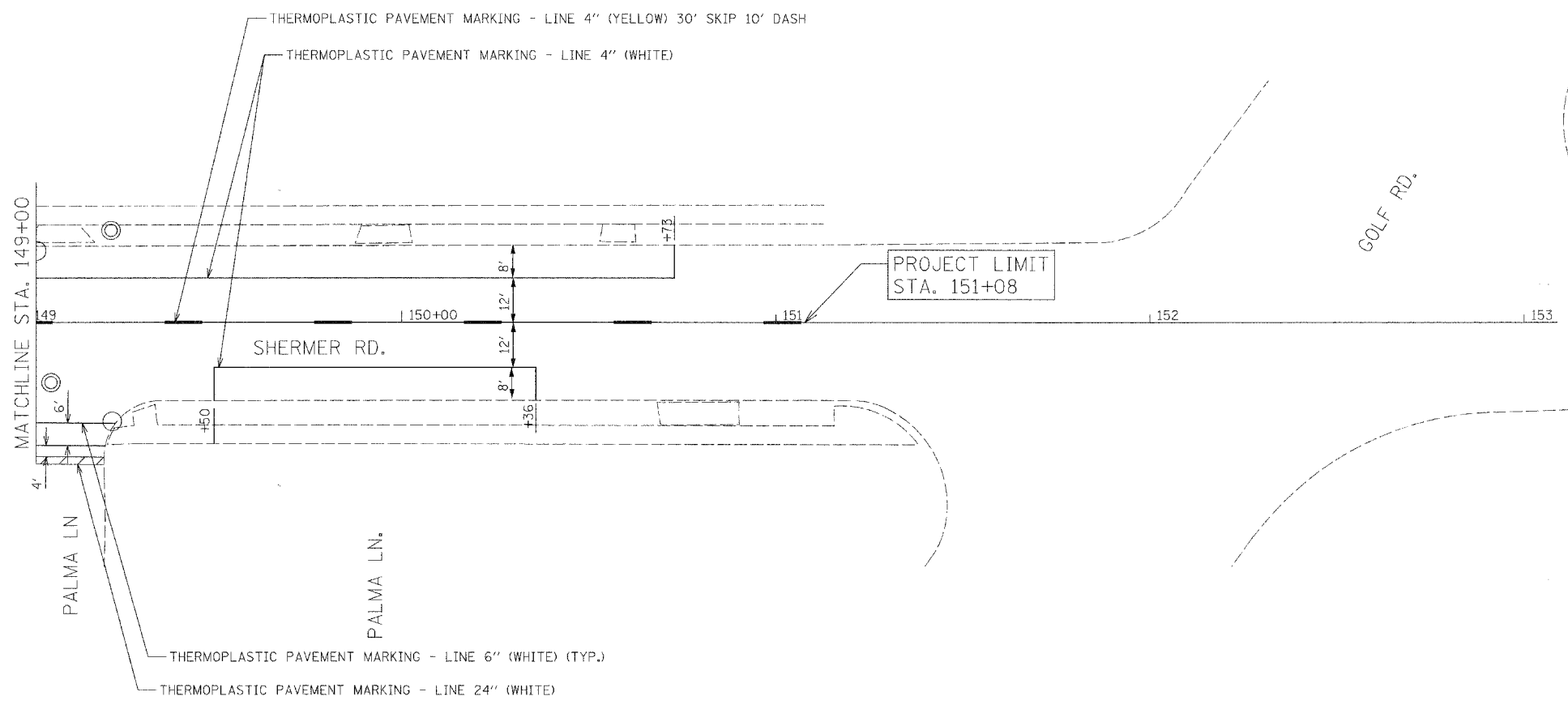
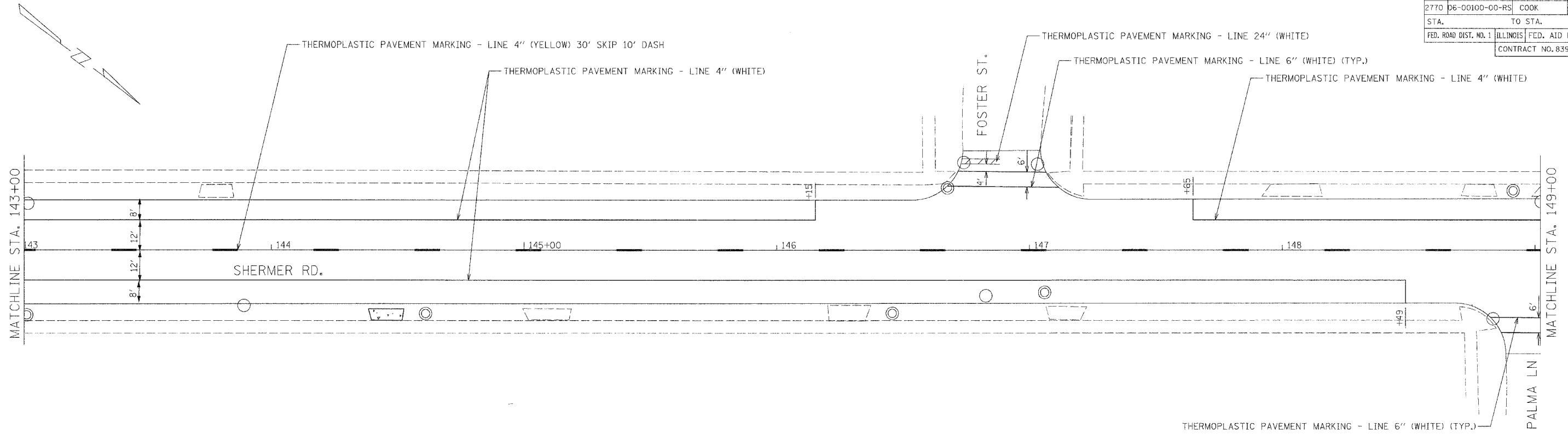
**Giorba Group, Inc.**  
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VILLAGE OF MORTON GROVE  
**SHERMER ROAD  
PAVEMENT MARKING PLAN  
STA. 131+00 TO STA. 143+00**

SCALE: 1"=20'  
DATE: MAY, 2007  
DRAWN BY: CLG  
CHECKED BY: MRJ

DATE: 05/04/07  
DRAWN BY: CLG  
CHECKED BY: MRJ  
SCALE: 1"=20'  
PROJECT: PAVEMENT MARKING PLAN FOR SHERMER RD. STA. 131+00 TO 143+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	D6-00100-00-RS	COOK	26	14
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83953				

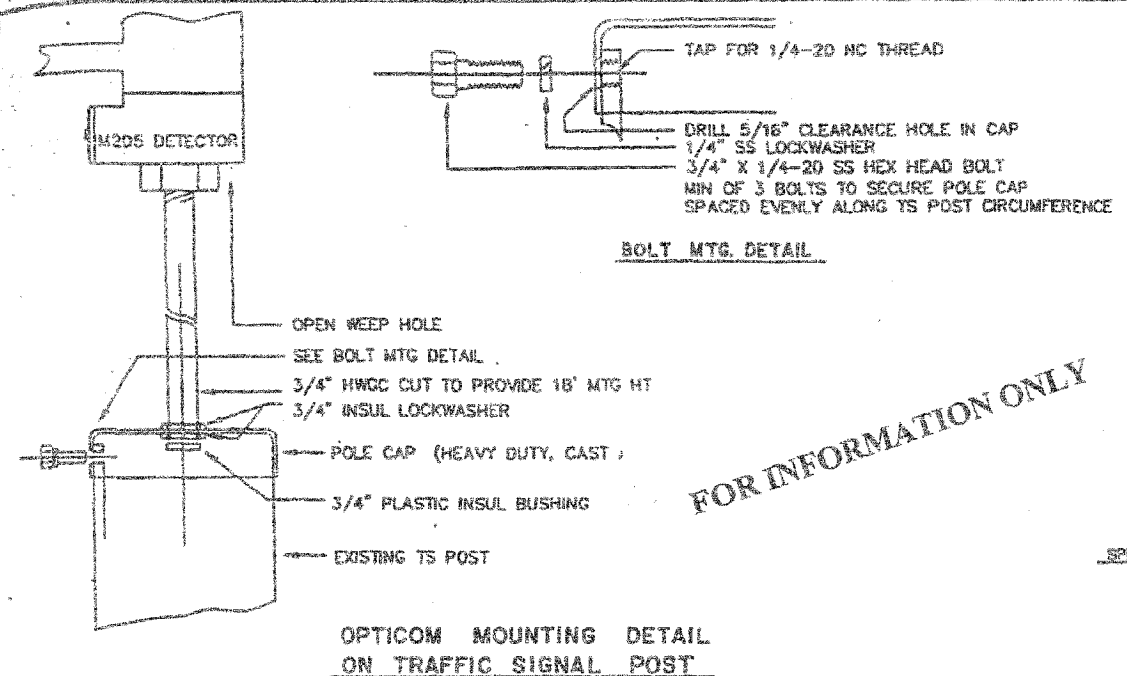


REVISIONS	
NAME	DATE

**CG** Ciorba Group, Inc.  
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Tel. 773.775.4009 Fax 773.775.4014 Email: chicago@ciorba.com

VILLAGE OF MORTON GROVE  
**SHERMER ROAD  
PAVEMENT MARKING PLAN  
STA. 143+00 TO STA. 151+08**

SCALE: 1"=20'  
DATE: MAY, 2007  
DRAWN BY: CLG  
CHECKED BY: MRJ



FOR INFORMATION ONLY

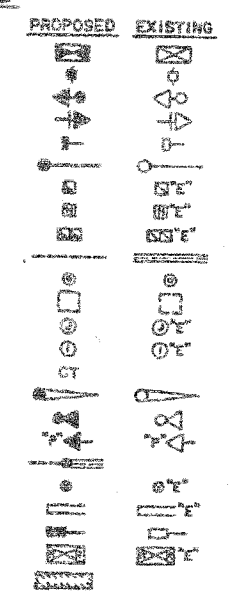
**REMOVAL NOTES**

1. PULL OUT ALL CABLES NOT USED FOR NEW SIGNAL HEADS AND INTERCONNECT CABLE.

2. REMOVE FOUNDATIONS FOR EAST AND WESTBOUND EAST ARM POLES.

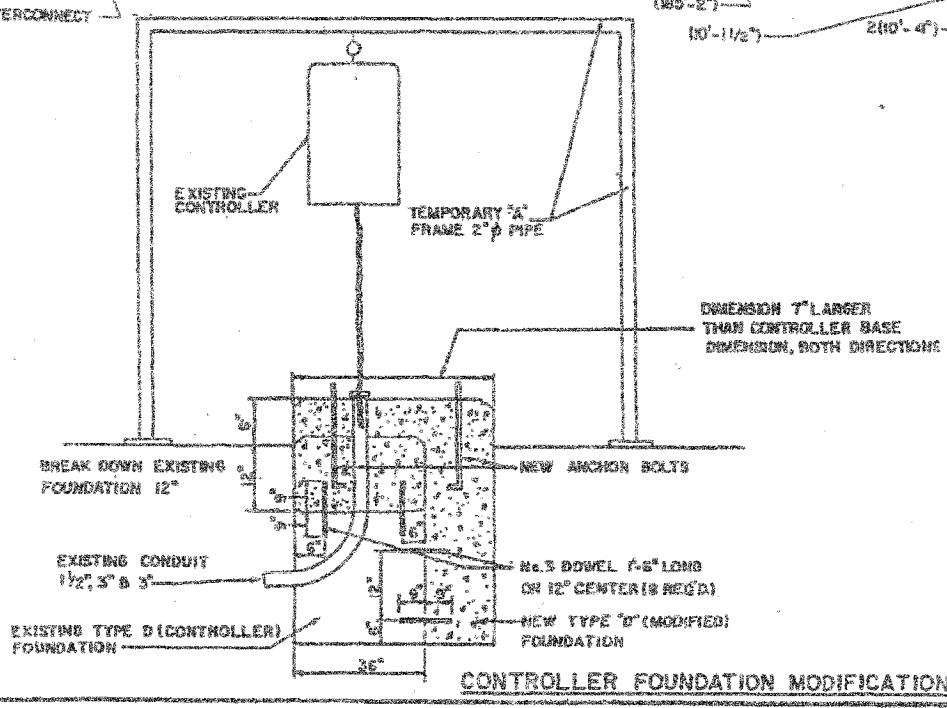
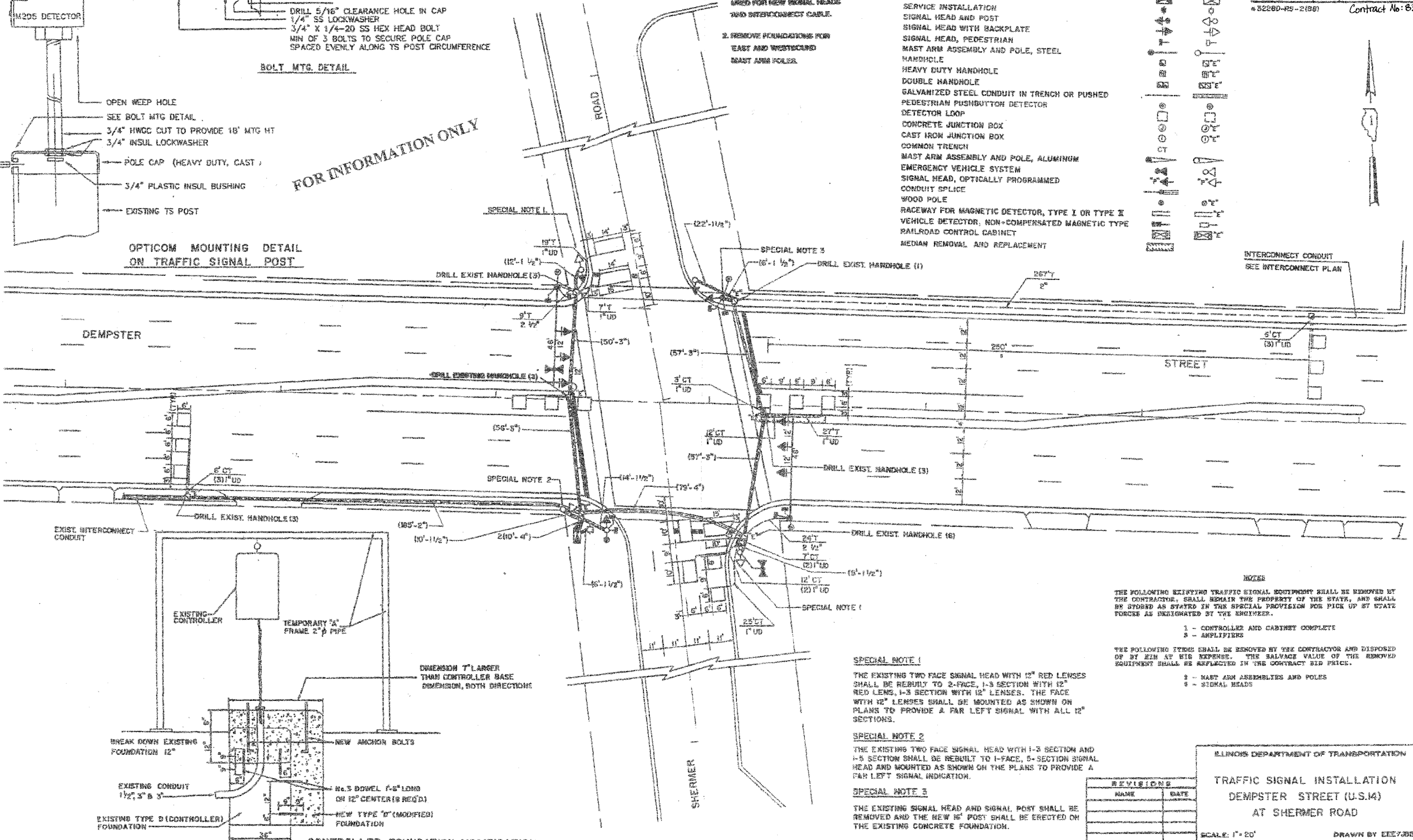
**TRAFFIC SIGNAL LEGEND**

- CONTROLLER  
SERVICE INSTALLATION  
SIGNAL HEAD AND POST  
SIGNAL HEAD WITH BACKPLATE  
SIGNAL HEAD, PEDESTRIAN  
MAST ARM ASSEMBLY AND POLE, STEEL  
HANDPOLE  
HEAVY DUTY HANDHOLE  
DOUBLE HANDHOLE  
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED  
PEDESTRIAN PUSHBUTTON DETECTOR  
DETECTOR LOOP  
CONCRETE JUNCTION BOX  
CAST IRON JUNCTION BOX  
COMMON TRENCH  
MAST ARM ASSEMBLY AND POLE, ALUMINUM  
EMERGENCY VEHICLE SYSTEM  
SIGNAL HEAD, OPTICALLY PROGRAMMED  
CONDUIT SPLICE  
WOOD POLE  
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II  
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE  
RAILROAD CONTROL CABINET  
MEDIAN REMOVAL AND REPLACEMENT



NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1324	6	COOK	26	15
STA.	TO STA.		26	
PER ROAD DIST NO. 1		ALLIANCE	FOR AID PROJECT	
# 52280-25-2 (88)				

Contract No: 83953



**NOTES**

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE STATE, AND SHALL BE STORED AS STATED IN THE SPECIAL PROVISION FOR PICK UP BY STATE POLICE AS DESIGNATED BY THE ENGINEER.

1 - CONTROLLER AND CABINET COMPLETE  
2 - AMPLIFIERS

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND DISPOSED OF BY HIM AT HIS EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

1 - MAST ARM ASSEMBLIES AND POLES  
2 - SIGNAL HEADS

**SPECIAL NOTE 1**

THE EXISTING TWO FACE SIGNAL HEAD WITH 12" RED LENSES SHALL BE REBUILT TO 2-FACE, 1-3 SECTION WITH 12" RED LENS, 1-3 SECTION WITH 12" LENSES. THE FACE WITH 12" LENSES SHALL BE MOUNTED AS SHOWN ON PLANS TO PROVIDE A FAR LEFT SIGNAL WITH ALL 12" SECTIONS.

**SPECIAL NOTE 2**

THE EXISTING TWO FACE SIGNAL HEAD WITH 1-3 SECTION AND 1-5 SECTION SHALL BE REBUILT TO 1-FACE, 5-SECTION SIGNAL HEAD AND MOUNTED AS SHOWN ON THE PLANS TO PROVIDE A FAR LEFT SIGNAL INDICATION.

**SPECIAL NOTE 3**

THE EXISTING SIGNAL HEAD AND SIGNAL POST SHALL BE REMOVED AND THE NEW 6" POST SHALL BE ERRECTED ON THE EXISTING CONCRETE FOUNDATION.

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL INSTALLATION  
DEMPSTER STREET (U.S.14)  
AT SHERMER ROAD

SCALE: 1" = 20'

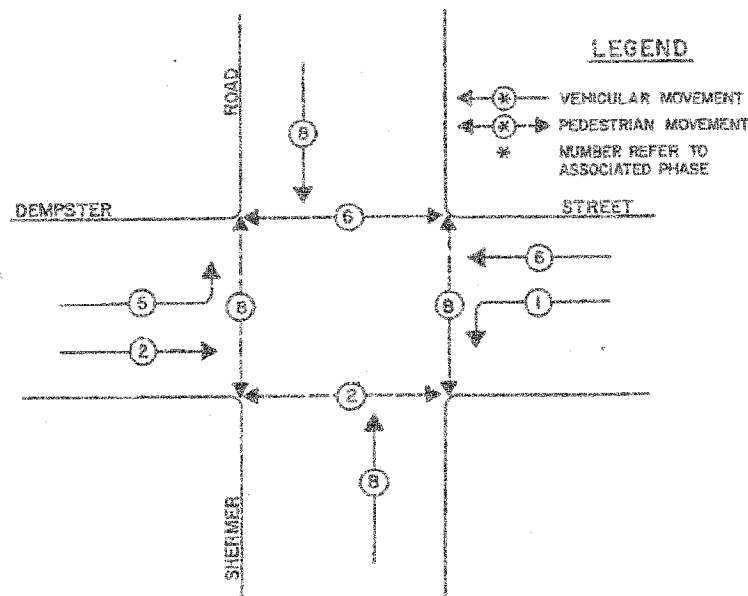
DATE NOV. 17, 1990

DRAWN BY EE7/BB  
CHECKED BY FW6/AG

NO.	NAME	DATE

**CONTROLLER SEQUENCE IV**

REFERRING TO STANDARD 2393-1, THE VEHICULAR AND PEDESTRIAN PHASES USED ARE DESIGNATED BELOW.



**PHASE DESIGNATION DIAGRAM**

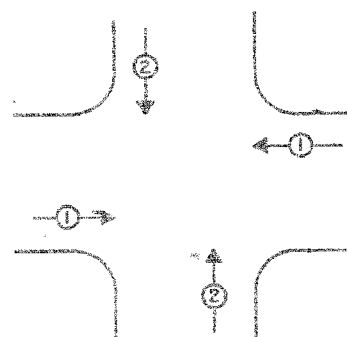
**SCHEDULE OF QUANTITIES**

QUANTITY	UNIT	ITEM
15.5	SQ FT	SIGN PANEL - TYPE I
2	EACH	SIGNAL HEAD, 1-FACE, 2-SECTION, BRACKET MOUNTED
4	EACH	SIGNAL HEAD, 1-FACE, 3-SECTION, MAST ARM MOUNTED
1	EACH	SIGNAL HEAD, 1-FACE, 5-SECTION, BRACKET MOUNTED
2	EACH	SIGNAL HEAD, 1-FACE, 5-SECTION, MAST ARM MOUNTED
4	EACH	PEDESTRIAN SIGNAL HEAD, 3-FACE, BRACKET MOUNTED
6	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 15 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 48 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 48 FT.
1	EACH	FULL-ACTUATED CONTROLLER, STANDARD SEQUENCE, 8 PHASES, IN TYPE IV CABINET
1	EACH	TRANSCEIVER
7	EACH	INDUCTION LOOP DETECTOR AMPLIFIER
696	LIN FT	DETECTOR LOOP, TYPE I
4	EACH	PEDESTRIAN PUSHBUTTON
267	LIN FT	GALVANIZED STEEL CONDUIT IN TRENCH 2"
33	LIN FT	GALVANIZED STEEL CONDUIT IN TRENCH 1 1/2"
167	LIN FT	UNIT DUCT, WITHOUT CABLE, IN TRENCH 1"
567	LIN FT	ELECTRIC CABLE IN CONDUIT NO. 14 2/C
1134	LIN FT	ELECTRIC CABLE IN CONDUIT NO. 14 3/C
580	LIN FT	ELECTRIC CABLE IN CONDUIT NO. 14 5/C
618	LIN FT	ELECTRIC CABLE IN CONDUIT NO. 14 7/C
1334	LIN FT	ELECTRIC CABLE IN CONDUIT NO. 14 2/C TWISTED, SHIELDED
30	LIN FT	CONCRETE FOUNDATION, TYPE K 36-INCH DIAMETER
1	EACH	CONCRETE HANDHOLE
421	LIN FT	TRENCH AND BACKFILL
3	EACH	REBUILD EXISTING SIGNAL HEAD
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
2	EACH	REMOVE EXISTING CONCRETE FOUNDATION
1516	LIN FT	PULL ELECTRIC CABLE FROM CONDUIT
1	EACH	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
1	EACH	PAINT EXISTING TRAFFIC SIGNAL EQUIPMENT
19	EACH	DRILL EXISTING HANDHOLE
2	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
304	LIN FT	EMERGENCY VEHICLE PRIORITY SYSTEM LEAD-IN CABLE IN CONDUIT
1	EACH	MODIFY EXISTING TYPE "D" FOUNDATION
10	LIN FT	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
120	SQ FT	MEDIAN REMOVAL AND REPLACEMENT
50	SQ FT	SIDEWALK REMOVAL AND REPLACEMENT

**NOTES:**

1. TERMINATION OF PHASE 1+6 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR RED.
2. TERMINATION OF PHASES 1 OR 5 ALONE IN PHASE 1+6 OR 2+6 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR GREEN.
3. TERMINATION OF PHASES 2+8 OR 8 SHALL BE WITH A CIRCULAR YELLOW WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED.
4. TERMINATION OF PHASES 2+6 OR 8 SHALL BE WITH A CIRCULAR YELLOW WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED. WHEN PHASES 2+6 OR 8 CIRCULAR GREEN IS TO BE DISPLAYED IN THE PRIORITY LANE INTERVAL, IT SHALL REMAIN GREEN.
5. TERMINATION OF ALL PEDESTRIAN INTERVALS SHALL INCLUDE A FULL FLASHING "DON'T WALK" CLEARANCE INTERVAL.
6. TERMINATION OF ALL PRIORITY INTERVALS SHALL BE WITH A CIRCULAR YELLOW EXCEPT WHEN THE GREEN DISPLAYED DURING THE PRIORITY INTERVAL IS TO REMAIN GREEN WHEN THE NORMAL SEQUENCE OF OPERATING RESUMES CONTROL OF THE INTERSECTION.
7. IF ALL RED CLEARANCE IS USED IN THE NORMAL SEQUENCE OF OPERATIONS, IT MUST BE DISPLAYED AFTER THE YELLOW CLEARANCE INTERVAL WHEN ENTERING OR LEAVING THE PRIORITY SEQUENCE.

**PRIORITY SEQUENCE FOR CONTROLLER SEQUENCE IV**



PROPOSED PRIORITY LANES		
PRIORITY LANE INTERVAL	1	2
MOVEMENT	← →	↓ ↑

ROUTE	SECTION	COUNTY	DATE	SCALE
1524	#	COOK	85	38
DATE:	TO STA.	26	16	

Contract No: 83953

**CABLE PLAN LEGEND**

- 8" TRAFFIC SIGNAL SECTION
- 12" TRAFFIC SIGNAL SECTION
- 12" PEDESTRIAN SIGNAL SECTION
- CONTROLLER CABINET
- SERVICE INSTALLATION (EXISTING)
- VEHICLE DETECTOR, INDUCTION LOOP
- PUSHBUTTON DETECTOR
- DENOTES NUMBER OF CONDUCTORS (NEW)
- ALL LOOP DETECTOR CABLE TO BE SHIELDED
- ALL CABLE NO. 14 EXCEPT AS INDICATED.
- INDICATES EXISTING CABLE
- SIGNAL FACE WITH BACKPLATE
- "P" INDICATES OPTICAL PROGRAMMING
- EXISTING SIGNAL SECTION
- MAGNETIC DETECTOR
- OPTICAL DETECTOR

FOR INFORMATION ONLY

- PUSHBUTTON NOTES**
1. PUSHBUTTONS 'A' & 'D' SHALL PLACE A CALL IN PHASES 2 AND 8.
  2. PUSHBUTTONS 'B' & 'C' SHALL PLACE A CALL IN PHASES 6 AND 8.

REVISIONS	
NAME	DATE

DEPARTMENT OF TRANSPORTATION

**CABLE DIAGRAM**

**SCHEDULE OF QUANTITIES**

**DEMPSTER STREET (U.S. 14)**

**AT SHERMER ROAD**

SCALE: NONE DRAWN BY: EEE/MB

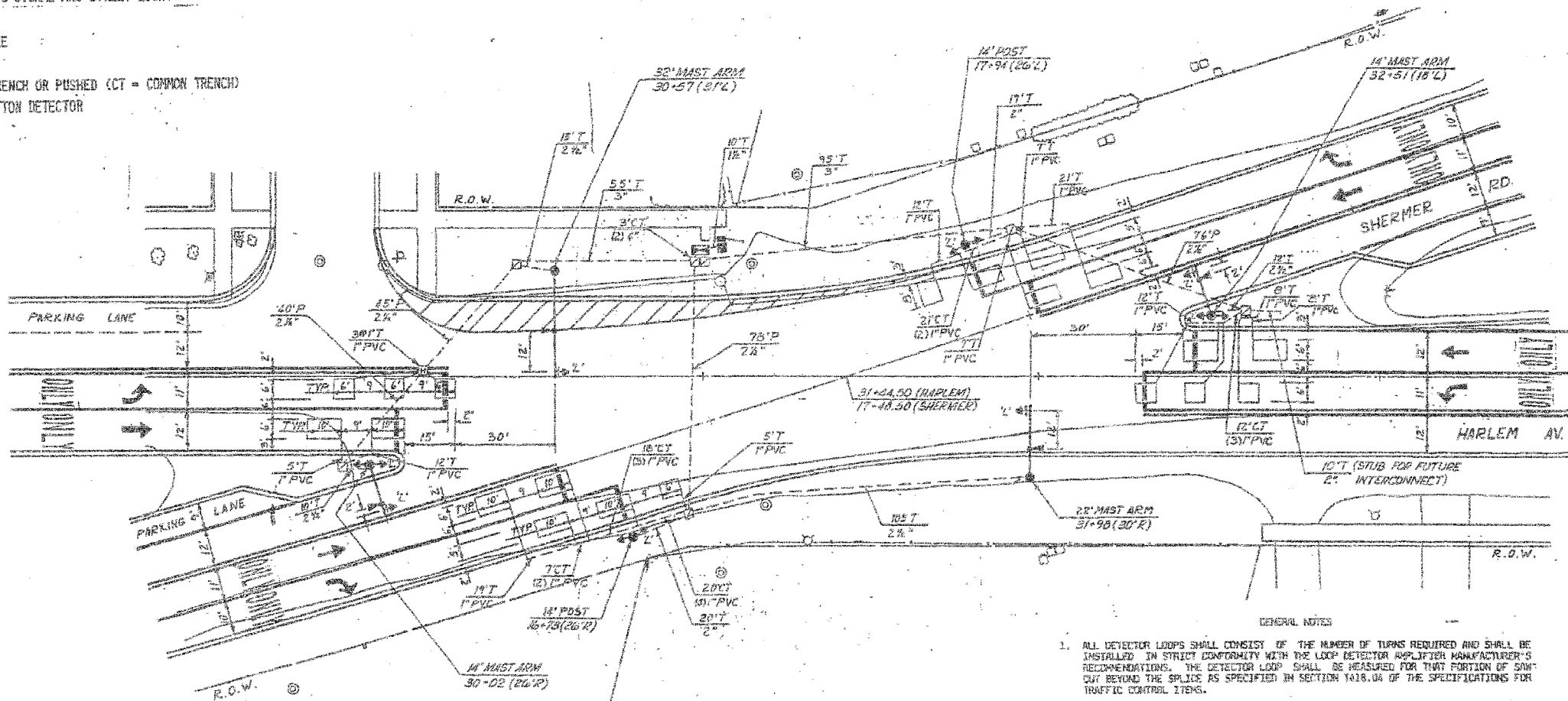
DATE: NOV 20, 1990 CHECKED BY: PKG



TRAFFIC SIGNAL LEGEND

EXISTING	PROPOSED	
		CONTROLLER
		SERVICE INSTALLATION
		SIGNAL HEAD (2" WITH STRAIGHT RAY LOUVER)
		SIGNAL FACE WITH BACKPLATE
		SIGNAL HEAD, PEDESTRIAN
		EMERGENCY VEHICLE DETECTOR, OPTICAL TYPE
		SIGNAL POST
		STREET LIGHT
		MAST ARM ASSEMBLY AND POLE, STEEL
		COMBINATION MAST ARM ASSEMBLY AND STREET LIGHT
		COMBINATION TRAFFIC SIGNAL AND STREET LIGHT
		HANDHOLE
		HEAVY DUTY HANDHOLE
		DOUBLE HANDHOLE
		S.S. CONDUIT IN TRENCH OR PUSHED (CT = COMMON TRENCH)
		PEDESTRIAN PUSHBUTTON DETECTOR
		LOOP DETECTOR
		POWER POLE
		TELEPHONE POLE
		GUY WIRE

NOTES: 1. ALL CONDUIT SHALL BE GALVANIZED STEEL EXCEPT FOR LOOP DETECTOR LEAD-IN CONDUIT WHICH SHALL BE PVC.  
 2. LOOP DETECTOR LEAD-INS SHALL BE SPACED A MINIMUM OF SIX (6) INCHES APART AT THE DRILLED HOLE TO THE LEAD-IN CONDUIT.



GENERAL NOTES

- ALL DETECTOR LOOPS SHALL CONSIST OF THE NUMBER OF TURNS REQUIRED AND SHALL BE INSTALLED IN STRICT CONFORMITY WITH THE LOOP DETECTOR MANUFACTURER'S RECOMMENDATIONS. THE DETECTOR LOOP SHALL BE MEASURED FOR THAT PORTION OF SPAN OUT BEYOND THE SPLICE AS SPECIFIED IN SECTION 1418.04 OF THE SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS.
- LEAD-IN WIRING SHALL BE INSTALLED IN STRICT CONFORMITY WITH THE MANUFACTURER'S RECOMMENDATIONS. THE 2/C SHIELDED CABLE TO BE USED FOR THE DETECTOR LOOP LEAD-IN SHALL BE MEASURED FROM THE SPLICE TO THE CONTROLLER AS SPECIFIED IN SECTION 1421.04 OF THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS. FLAT CABLE WILL NOT BE PERMITTED.
- ALL SIGNAL AND DETECTOR ELECTRIC CABLE THAT IS FURNISHED BY THE CONTRACTOR SHALL BE PROTECTED BY ETHYLENE INSULATION WITH A POLYVINYLCHLORIDE JACKET. SERVICE CABLE MAY HAVE AN NLP JACKET.
- THE REMOVAL AND REPLACEMENT OF SIDEWALK, DRIVEWAY, MEDIAN AND ISLAND SURFACE PAVING AT HANDHOLES, JACKING PITS, INSPECTION OPENINGS AND CONCRETE JUNCTION BOXES SHALL BE SHOWN OUT AROUND THE AREA TO BE REMOVED. THIS SHOWN CUTTING SHALL BE INCIDENTAL TO THE CONTRACT. THE REMOVAL AND REPLACEMENT OF SIDEWALK, DRIVEWAY, MEDIAN AND ISLAND SURFACE PAVING WILL BE PAID FOR SEPARATELY.
- THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE THE INSTALLATION OF ANY COMPONENTS OF THE TRAFFIC SIGNAL SYSTEM. FOR LOCATION OF UTILITIES CALL I.U.L.L.I.E. TOLL FREE NUMBER 1-800-892-0123 AND STATE MAINTAINED TRAFFIC SIGNALS 1-312-378-2800.
- ALL SIGNAL POST AND MAST ARM POLES SHALL BE LOCATED WITH THEIR CENTERLINES A MINIMUM OF FOUR (4) AND SIX (6) FEET RESPECTIVELY FROM THE BACK OF CURB UNLESS NOTED OR DIMENSIONED TO THE CONTRARY ON THE DRAWINGS. IN NON-CURBED AREAS THE MAST ARM POLE SHALL BE LOCATED A MINIMUM OF TEN (10) FEET BEHIND THE EDGE OF PAVEMENT OR TWO (2) FEET BEHIND THE EDGE OF SHOULDER, WHICHEVER DISTANCE IS GREATER. SIGNAL POSTS SHOULD BE PLACED AT A MINIMUM OF TWO (2) FEET BEHIND THE EDGE OF THE SHOULDER.
- PRIOR TO SANICUTTING ANY DETECTOR LOOPS, THE CONTRACTOR SHALL CONTACT THE I.U.D.O.T. TRAFFIC SIGNAL ENGINEER WHO WILL MARK THE PAVEMENT FOR THE CUTTING OF THE LOOPS.

FOR INFORMATION ONLY

DATE	BY	REVISION
7/6	COOK	59
7/6		17

Contract No. B9453  
 1313 (BECKWITH ROAD)  
 2766 (HARLEM AVENUE)  
 2770 (SHERMER ROAD)

STATE: 1984-167 W B RS  
 VILLAGE: 81-00071-01-RS

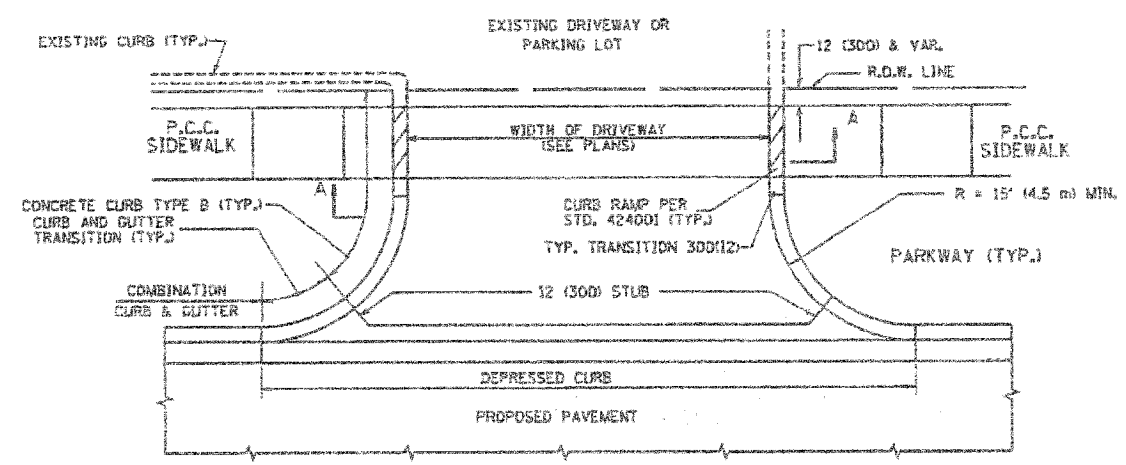
**CC** Ciorba Group, Inc.  
 CONSULTING ENGINEERS  
 1900 EAST GOLF ROAD, SUITE 500 • SCHAMBURG, ILLINOIS 60156

VILLAGE OF MORTON GROVE  
 HARLEM AV. & SHERMER RD.  
 TRAFFIC SIGNAL INSTALLATION

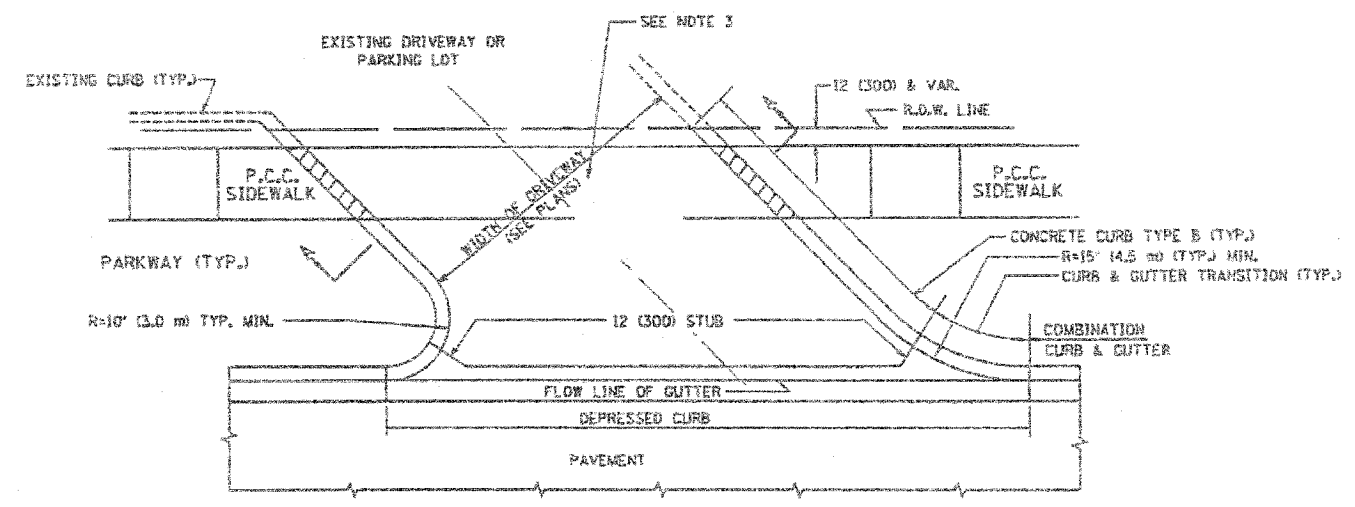
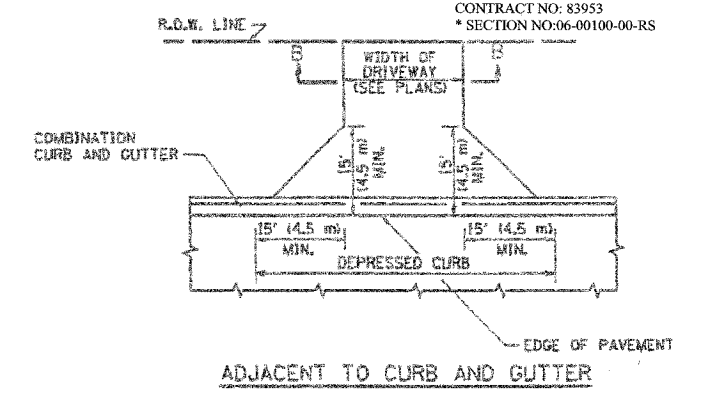
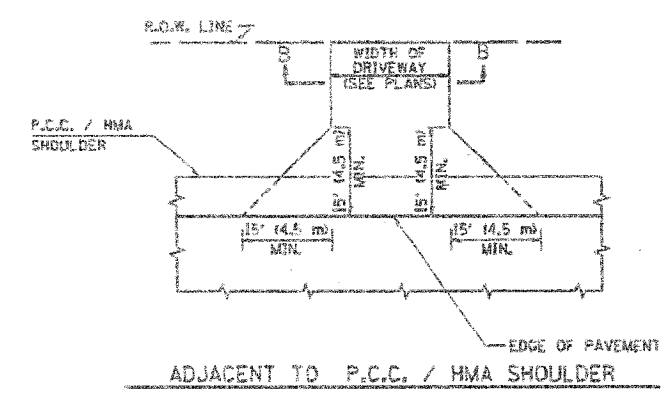
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 CHECKED: J.S.V.  
 DATE: 7-28-84  
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 FILE NO.: 2688

CONTRACT NO.				
F.A. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	*	COOK	26	18
STA.	TO STA.			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

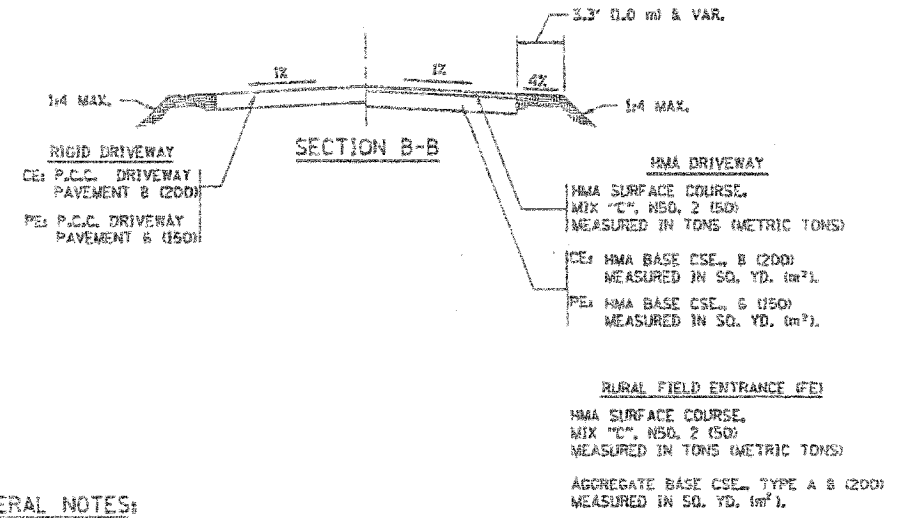
CONTRACT NO. 83953  
SECTION NO. 06-00100-00-RS



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB. CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

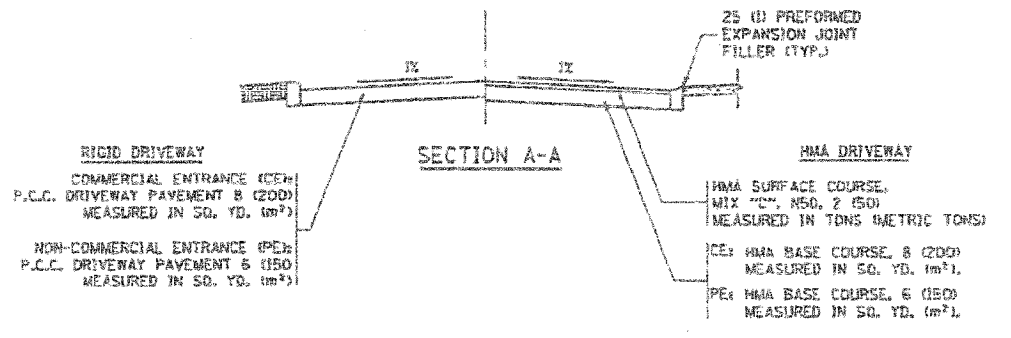
THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS, SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED



REVISIONS	
NAME	DATE
R. SHAY	11-04-95
J. POLLASTRINO	06-12-96
J. POLLASTRINO	12-14-96
A. ABBAS	03-21-97
T. HOLTZ	04-08-97
M. COMEZ	04-06-01
P. LEFLEUR	04-15-03
R. BURD	07-01-07

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DRIVEWAY DETAILS**  
DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

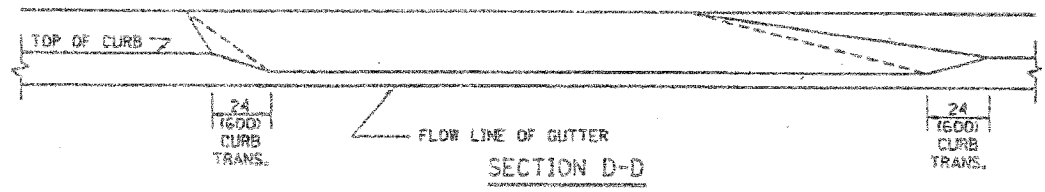
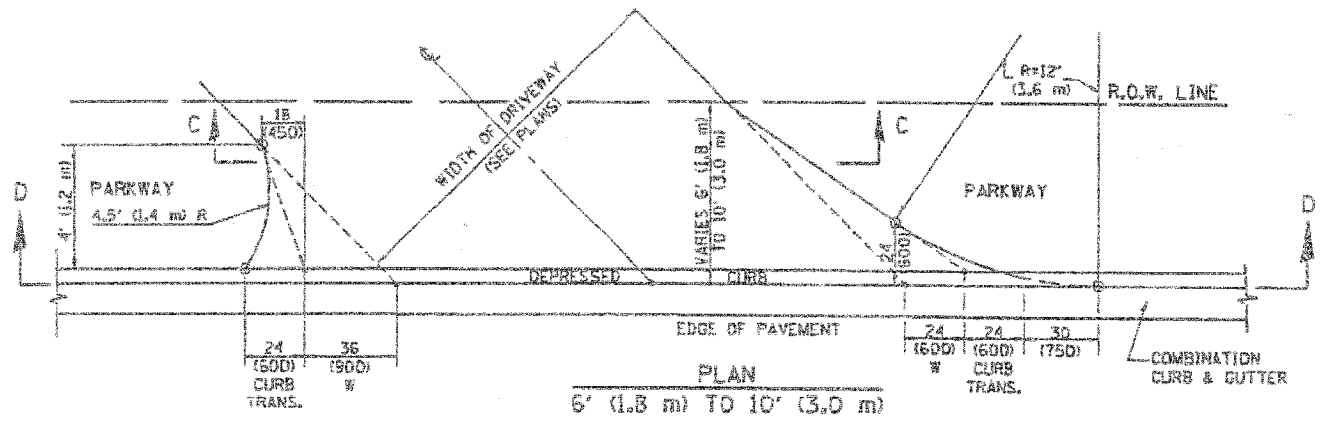
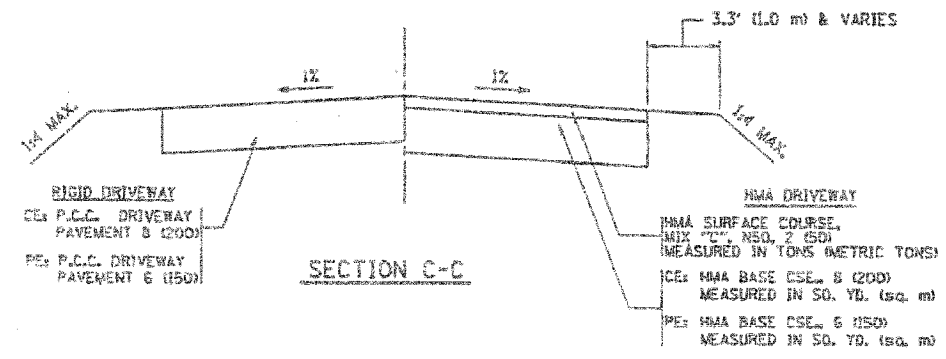
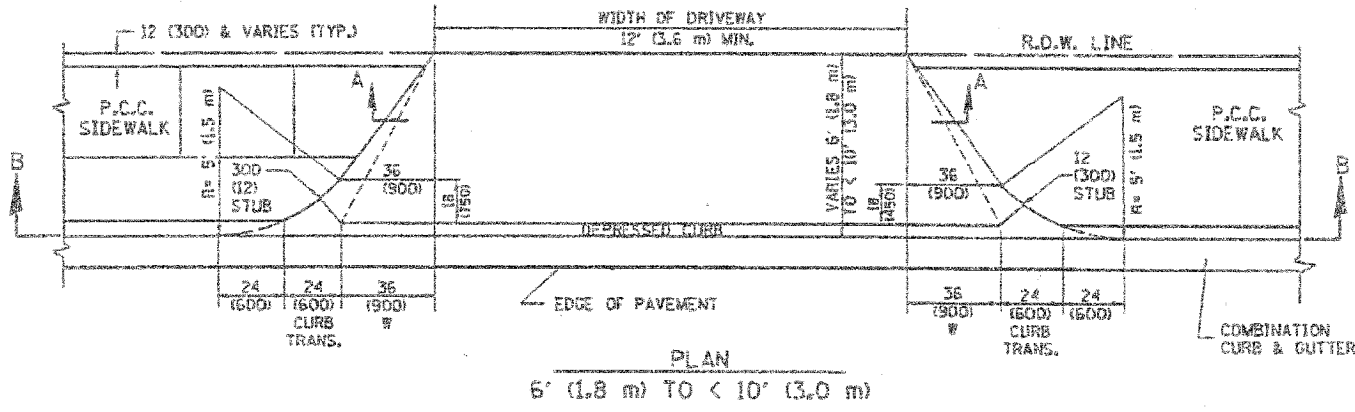
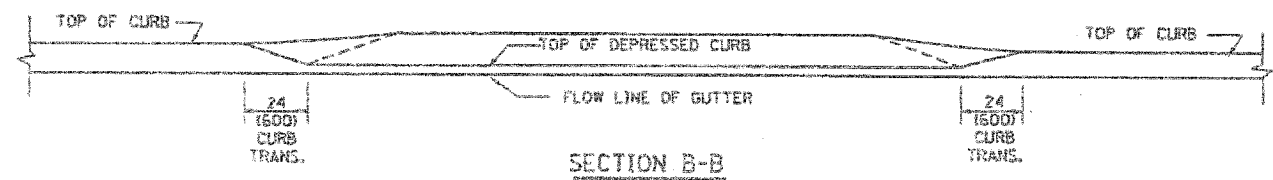
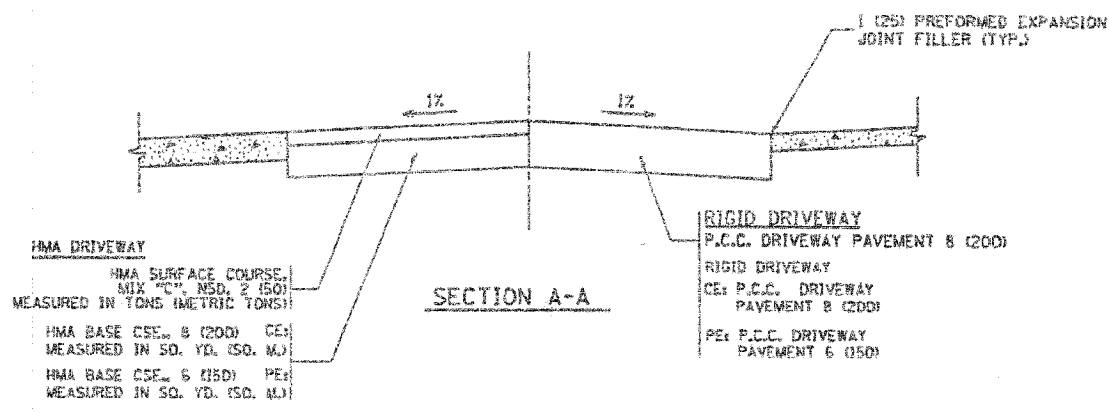
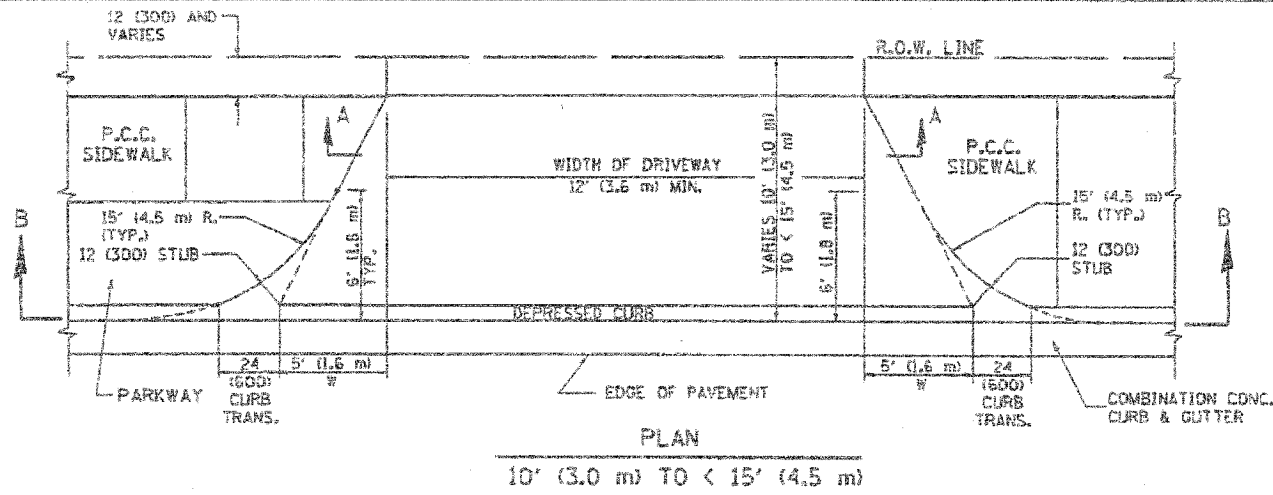
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HORIZ.

DRAWN BY  
CHECKED BY

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USER NAME: bryant@rdg.com

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	X	COOK	26	15
STA.	TO STA.			
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

CONTRACT NO: 83953  
SECTION NO:06-00100-00-RS



**GENERAL NOTES**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

ILLINOIS DEPARTMENT OF TRANSPORTATION

**DRIVEWAY DETAILS**  
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)

REVISIONS	
NAME	DATE
R. SHAR	11/26/95
J. POLLASTRINI	06/12/96
J. POLLASTRINI	12/14/96
A. ABBAS	03/21/97
T. HOLTZ	04/08/97
M. GOMEZ	04/06/97
P. LOFLER	04/15/97
R. BORD	01/01/97

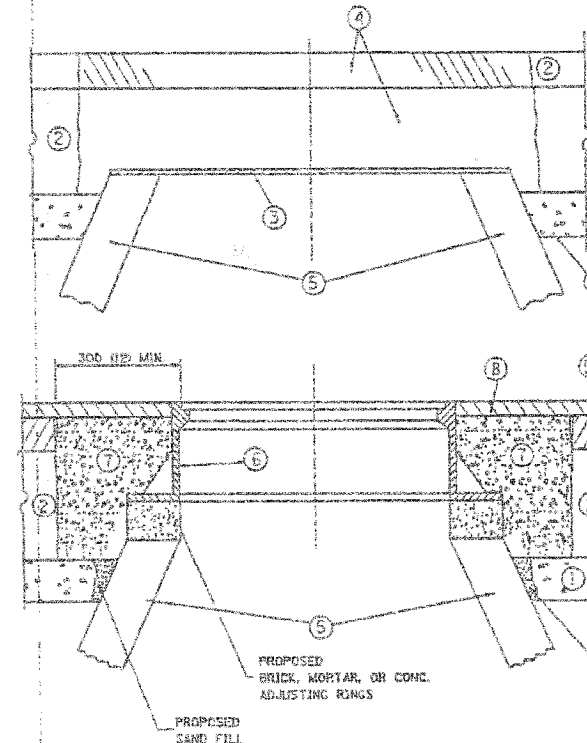
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HORIZ.

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CHECKED BY

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USER: RSHAR

F.A. NO.	SECTION	DESIGN	SHEET NO.	TOTAL SHEETS
2770	ME	LOOK	26	20
STA.	TO STA.			
FED. ROAD DIST. NO.	ROUTE	FED. RD. PROJECT		

CONTRACT NO. 83953  
 \* SECTION NO. 06-00100-00-RS



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 300 (12) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 900 (36) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 40 (1.5) THICK BITUMINOUS MATERIAL APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE BITUMINOUS MATERIAL AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 900 (36) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND BITUMINOUS MATERIAL
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS S1 CONCRETE, BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL
- ⑧ PROPOSED BITUMINOUS CONCRETE SURFACE COURSE
- ⑨ PROPOSED BITUMINOUS CONCRETE BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** FRAMES AND LIDS TO BE ADJUSTED, SPECIAL EACH

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE RECONSTRUCTION PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN

ILLINOIS DEPARTMENT OF TRANSPORTATION

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

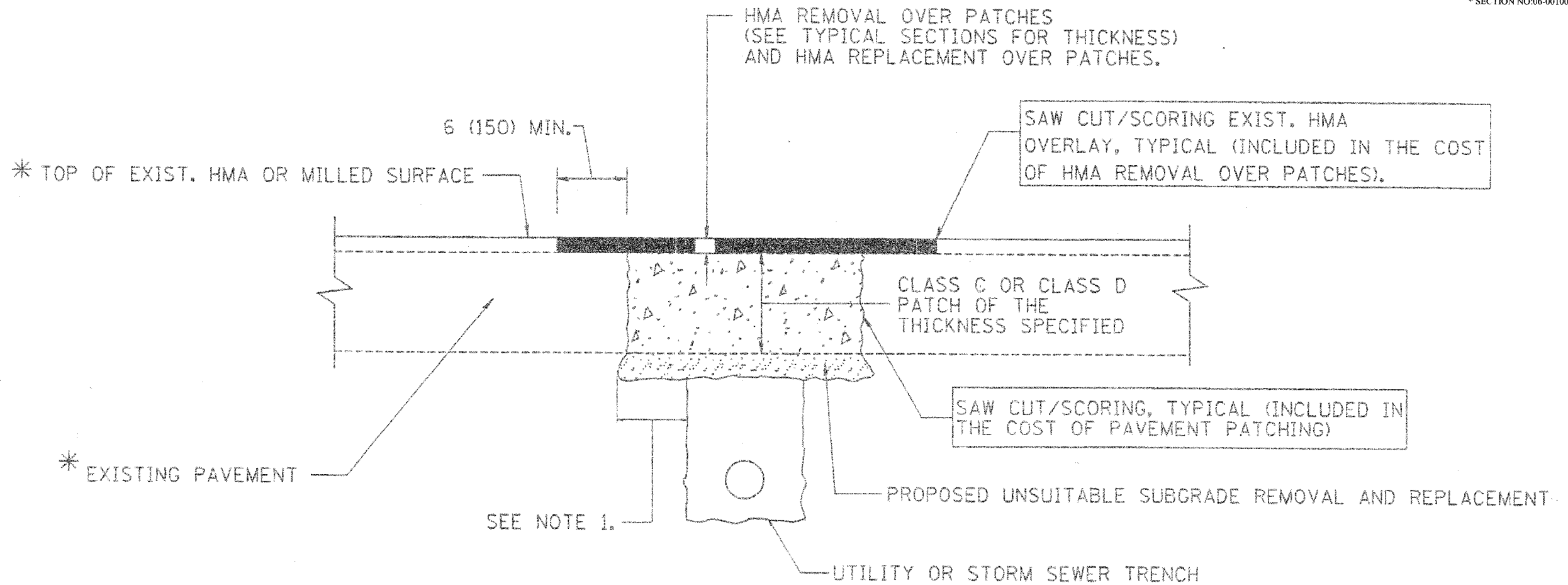
REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	10/30/95
R. SHAH	105/10/95
A. ABBAS	103/21/97

SCALE: NONE  
 DATE: 07/21/2003

DRAWN BY  
 CHECKED BY  
 BD600-03 (BD-B)  
 REVISION DATE: 07/21/98

CONTRACT NO.				
F.A. RITE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	*	COOK	26	21
STA.	TO STA.			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

CONTRACT NO: 83953  
\* SECTION NO:06-00100-00-RS



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
R. SHAH	10/25/94	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
R. SHAH	10/14/95	
R. SHAH	05/23/96	
R. SHAH	04/24/96	
A. HOLSCH	03/15/96	
A. ABBAS	05/21/97	
A. ABBAS	01/20/98	SCALE: VERT. NONE HORIZ. NONE
ART ABBAS	04/27/98	
R. BORD	01/01/07	DRAWN BY

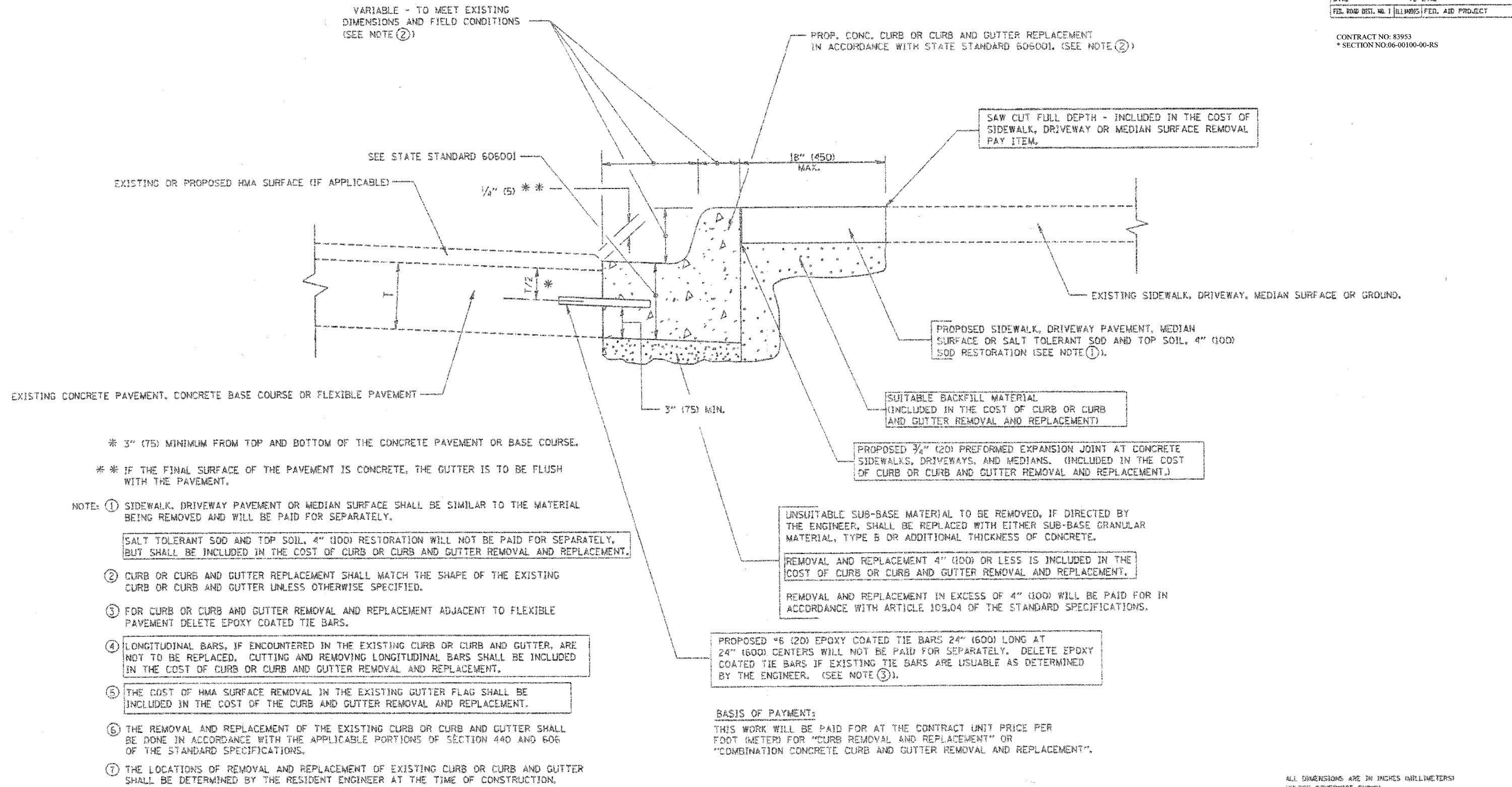
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BD400-04 (SD-22)

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USER: NHA

CONTRACT NO.				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	#	COOK	26	22
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT			

CONTRACT NO: 83953  
\* SECTION NO: 06-00100-00-RS



\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.  
\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
A. MOUSEN	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	05/19/95
R. SHAH	09/12/96
R. SHAH	08/19/96
R. SHAH	10/03/98
A. ABBAS	05/21/97
M. GOMEZ	01/22/01
R. BORD	01/01/01

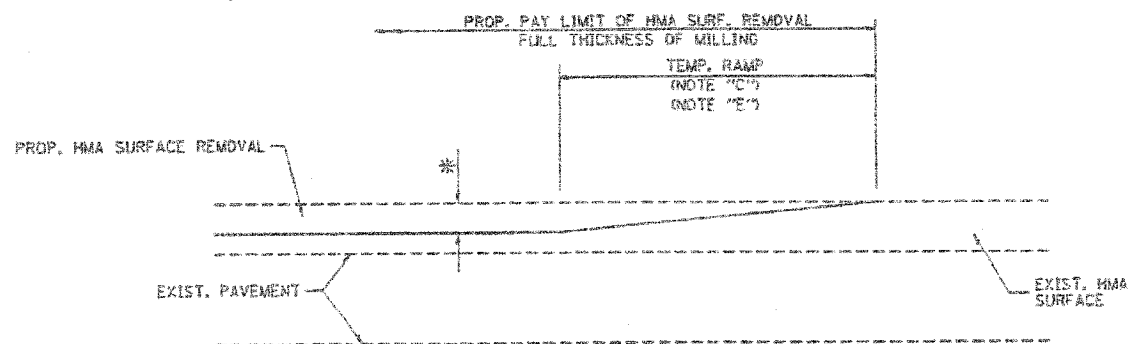
ILLINOIS DEPARTMENT OF TRANSPORTATION  
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: VERT. NONE  
HORIZ.  
DRAWN BY  
CHECKED BY  
BD500-06 (8D-24)

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

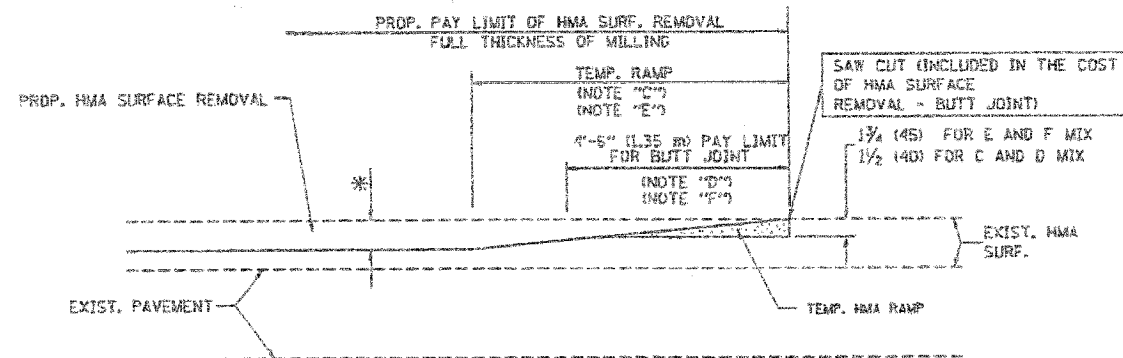
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CONTRACT NO.				
ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	*	COOK	26	23
STA.		TO STA.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
CONTRACT NO: 83953				
* SECTION NO: 06-00100-00-RS				



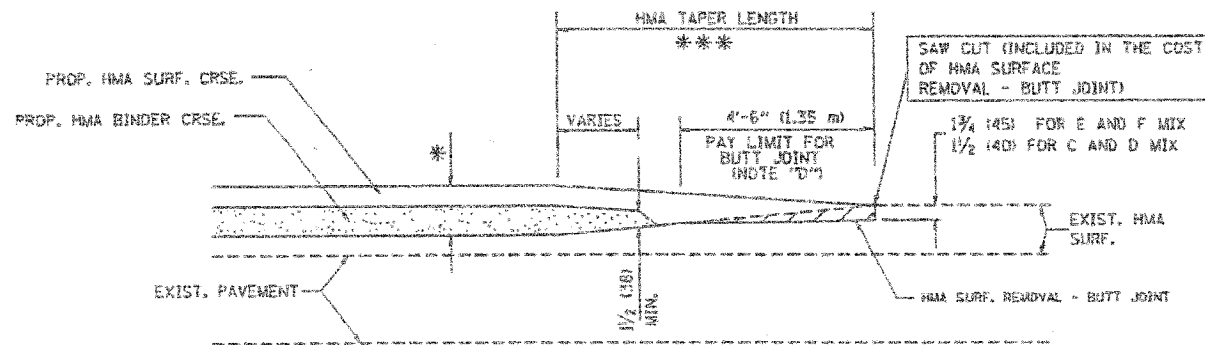
MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1



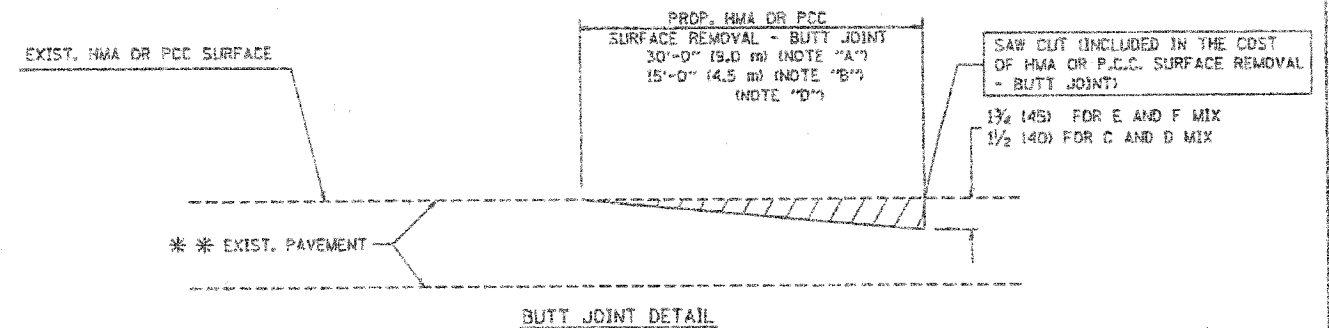
HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2  
TYPICAL TEMPORARY RAMP

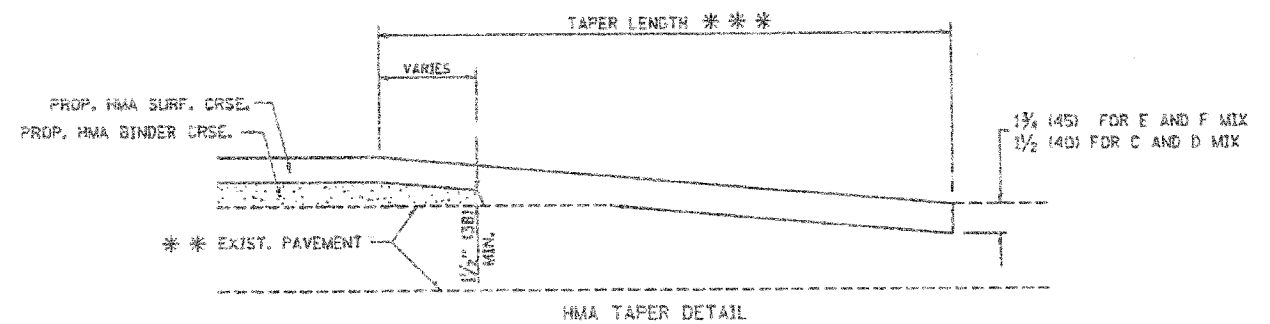


BUTT JOINT AND  
HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.25 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL - BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. ODIERZ	04/06/01
R. BORD	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER  
DETAILS

SCALE: VERT. NONE  
HORIZ. NONE

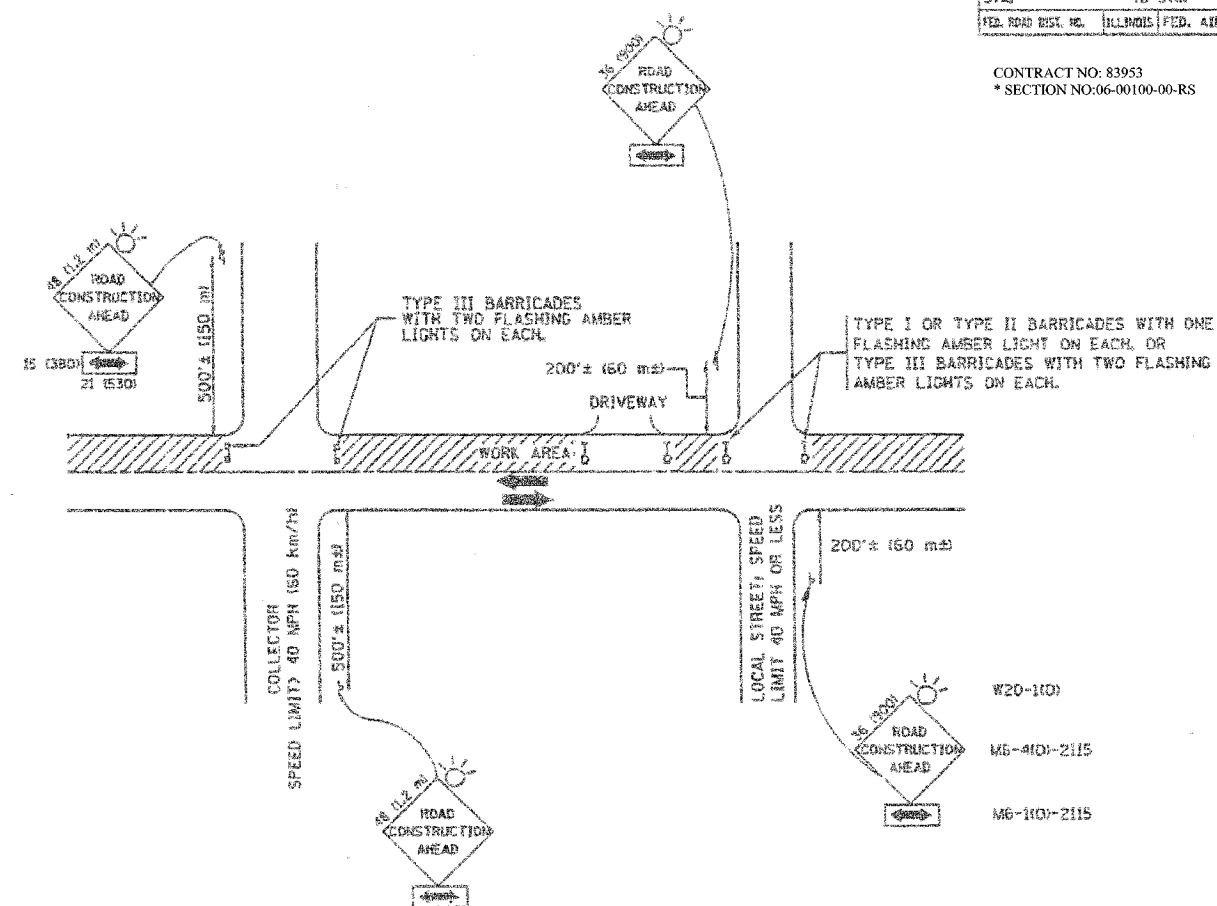
DRAWN BY

CHECKED BY

SD400-05 (VI)-B032

CONTRACT NO.				
F.A. RYE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
277b	76	COOK	26	24
STA. TO STA.		FED. AID PROJECT		
FED. RDIS DIST. NO.		ILLINOIS		

CONTRACT NO: 83953  
\* SECTION NO:06-00100-00-RS



W20-1101  
M6-410-2115  
M6-110-2115

### TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

#### NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-0).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. Y0100L, STD. Y0100R OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	10/06/94
J. OBERLE	10/18/95
A. HOUSEH	10/06/96
T. RAMMACHER	10/15/96
	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
TRAFFIC CONTROL AND PROTECTION  
FOR  
SIDE ROADS, INTERSECTIONS, AND  
DRIVEWAYS

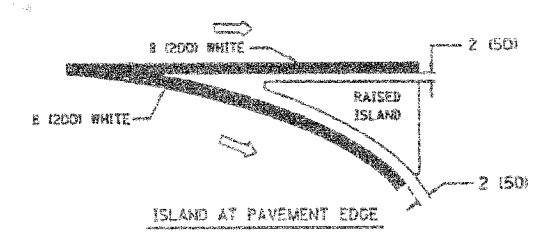
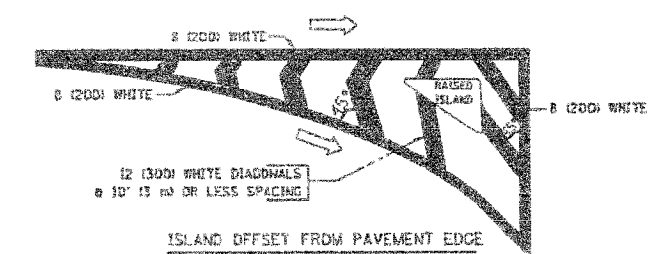
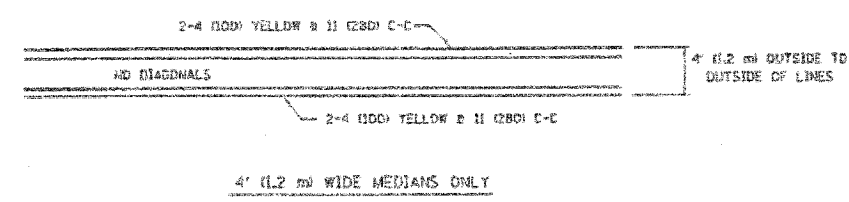
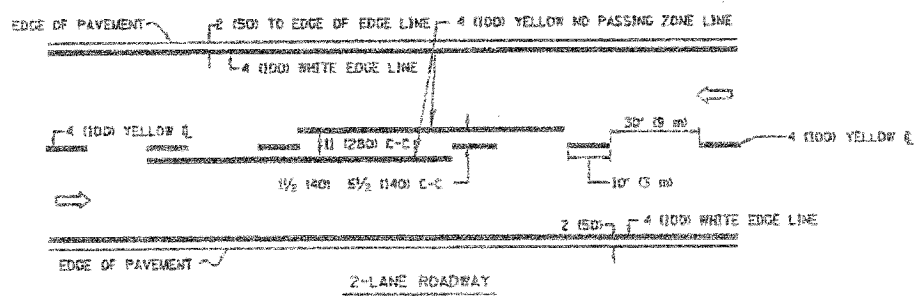
SCALE: NONE

DRAWN BY  
CHECKED BY

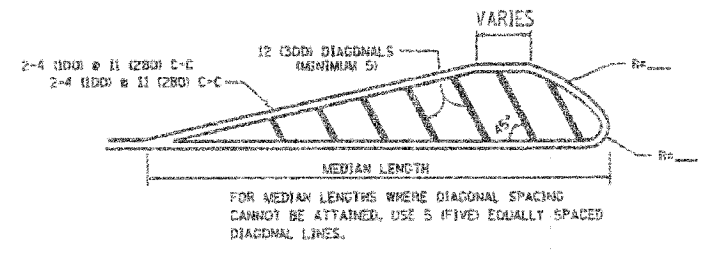
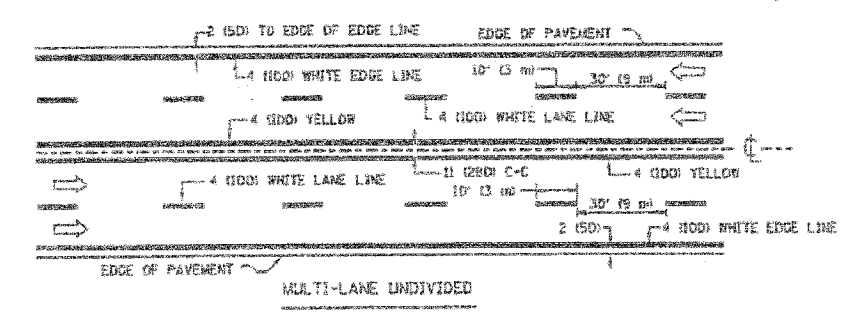


CONTRACT NO.				
STATE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	M	COOK	26	25
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

CONTRACT NO: 83953  
SECTION NO: 06-00100-00-RS

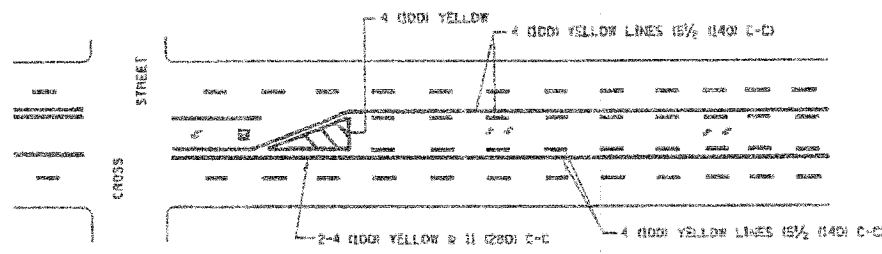


TYPICAL ISLAND MARKING



DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (22 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

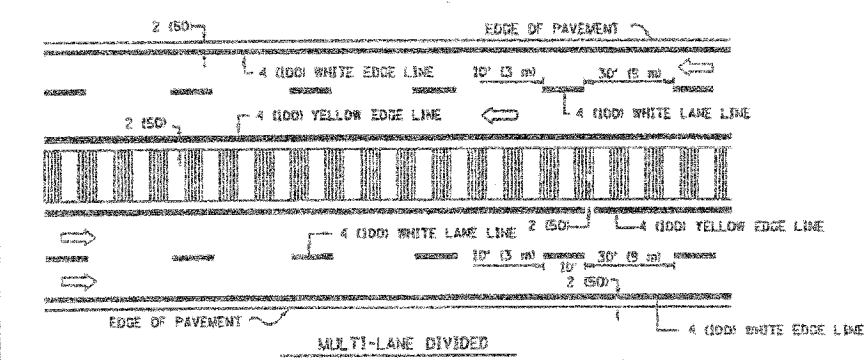


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



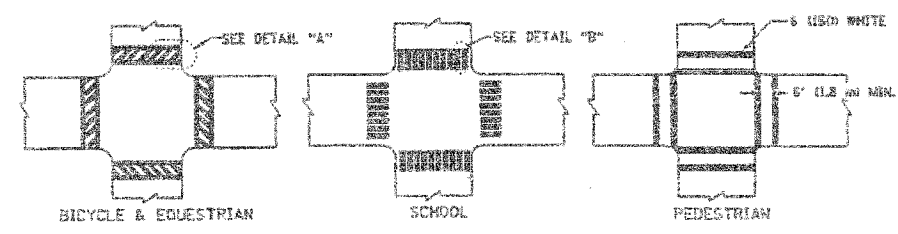
MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

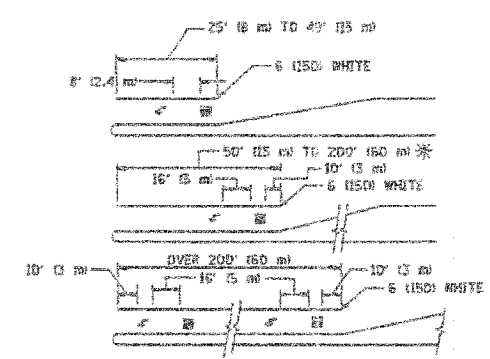


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.  
1/4 AREA = 25.6 SQ. FT. (2.4 m<sup>2</sup>) 1/2 AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROWS - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB.
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS 18" (4.6 m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BICYCLE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 6' (1.8 m) OR ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW WHITE WHITE	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CHINESE MARKING AND CHANNELIZING LINES	6 (150) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES "RR" IS 6" (1.8 m) LETTERS IS 1400 LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD T80001 AREA OF: "X"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (22 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD T80001.

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-15-92
T. RAMMACHER	10-27-94
ALEX MOUSEH	10-09-95
ALEX MOUSEH	10-17-95
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

SCALE: NONE

DRAWN BY: CADJ  
CHECKED BY:

TC-13

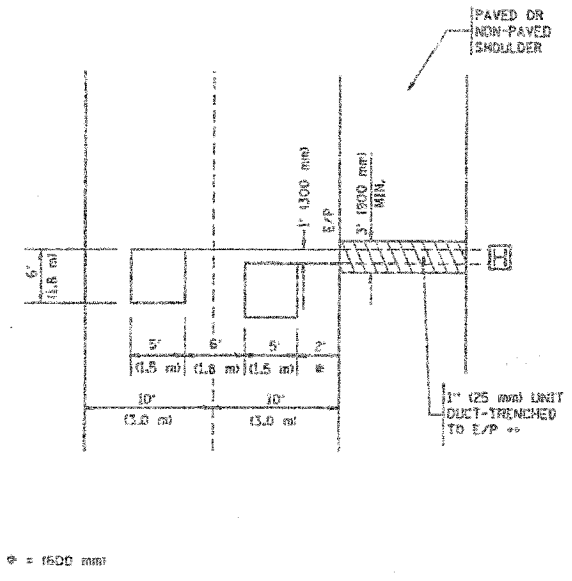
PL01 0015 4/30/2007  
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PLOT SCALE: 1/8"=1'-0"  
USER: HONG

ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2770	*	COOK	26	26
STA. TO STA.		FED. AID PROJECT		
FED. ROAD DIST. NO.		ILLINOIS		

CONTRACT NO: 83953  
SECTION NO: 06-00100-00-RS

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3" (76.2 mm) X WIDTH OF PAVED SHOULDER.

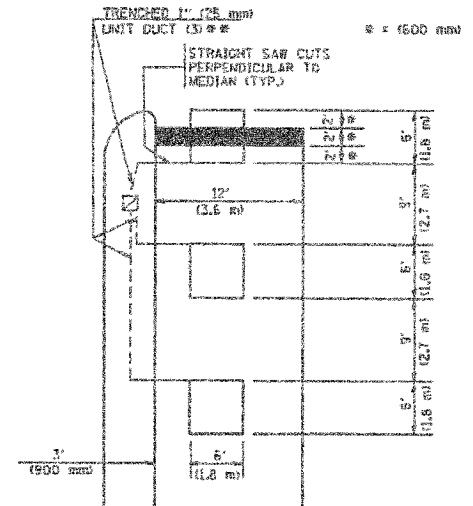


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

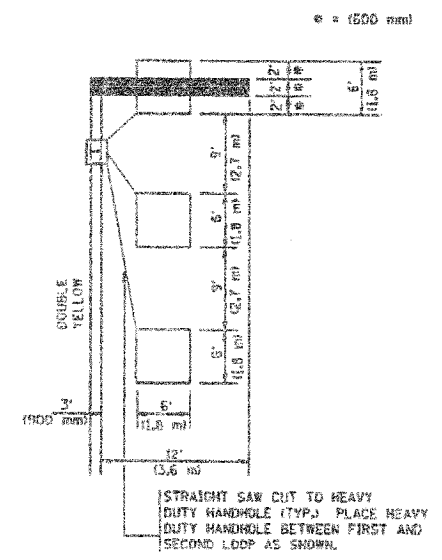
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD (1400) TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

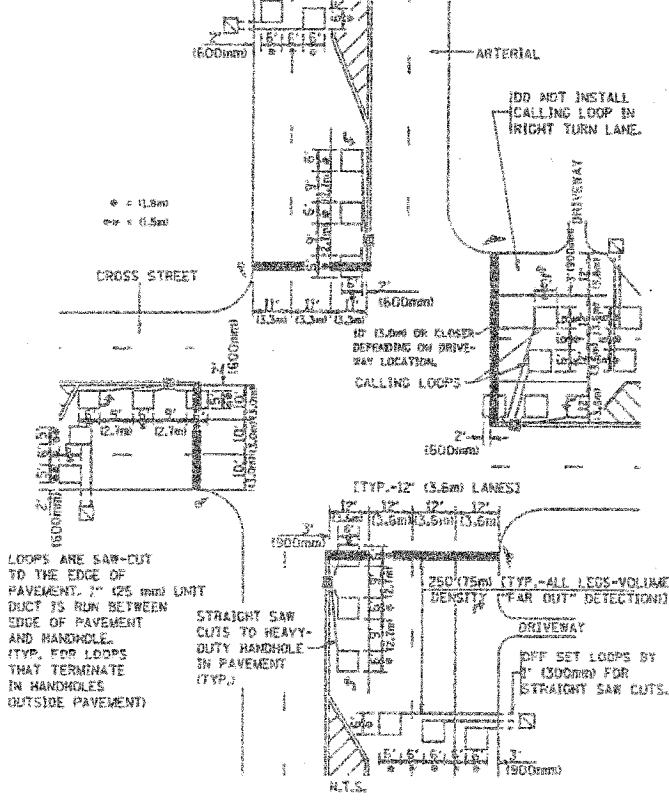
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

REVISIONS	
NAME	DATE

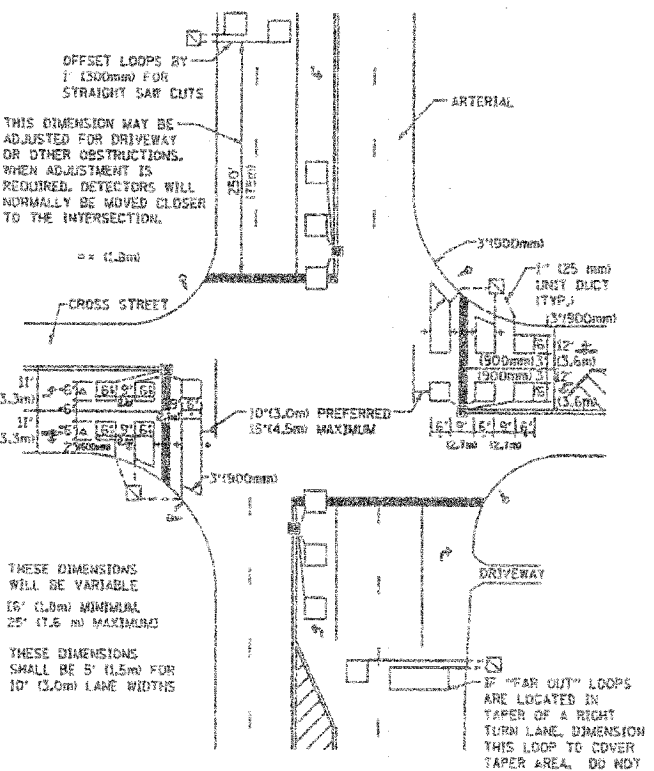
ILLINOIS DEPARTMENT OF TRANSPORTATION  
DISTRICT 1  
DETECTOR LOOP  
INSTALLATION DETAILS  
FOR ROADWAY RESURFACING  
DESIGNED BY  
DRAWN BY CADD  
CHECKED BY R.K.F.  
TS07

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2  
N.T.S.

NOT USE  
FILE NAME  
PLOT STYLE  
USER NAME