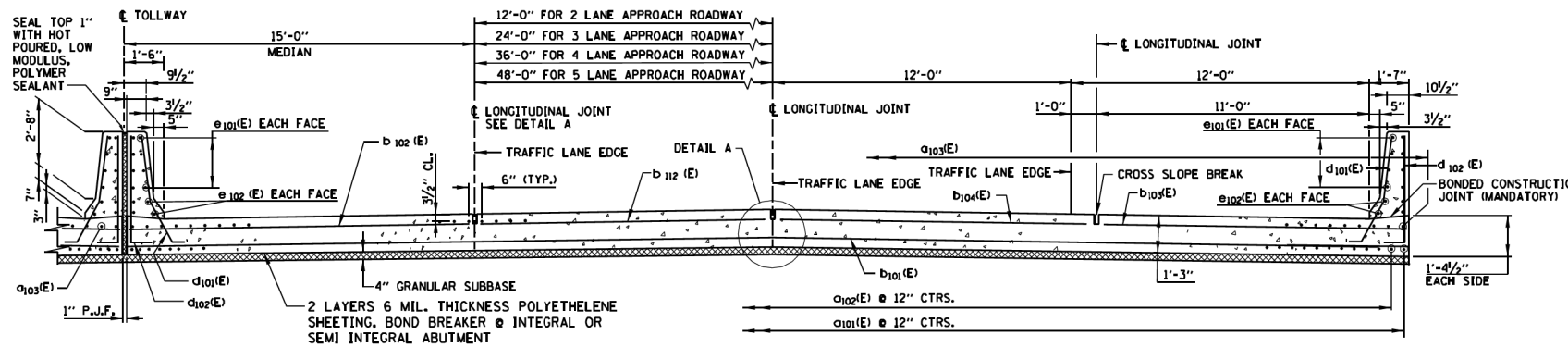
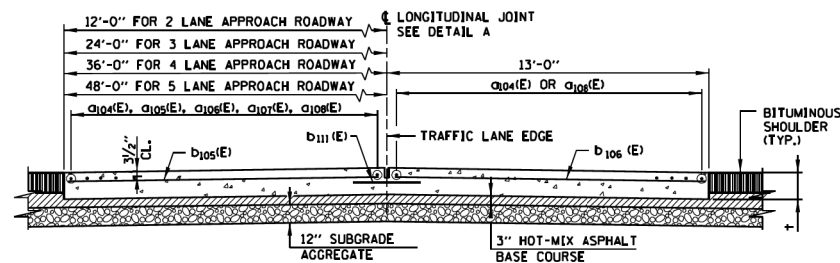


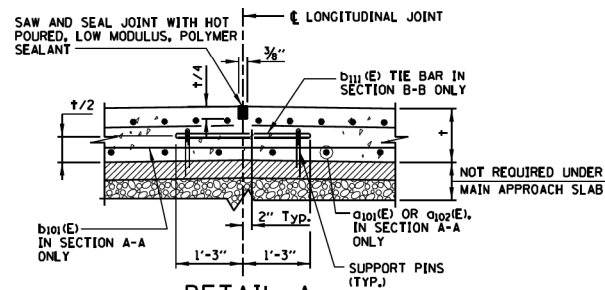
LONGITUDINAL CROSS SECTION



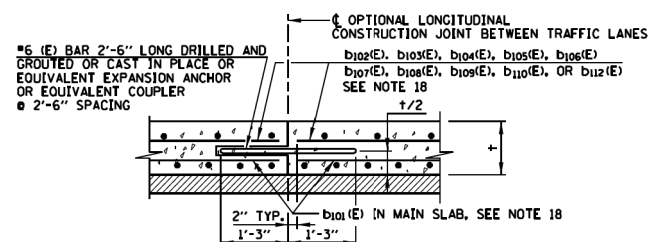
SECTION A-A



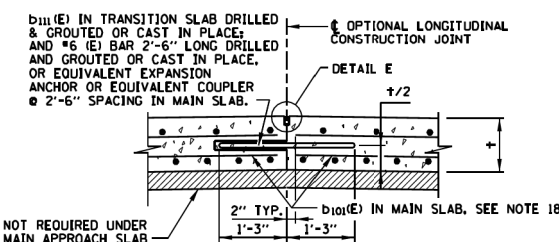
SECTION B-B



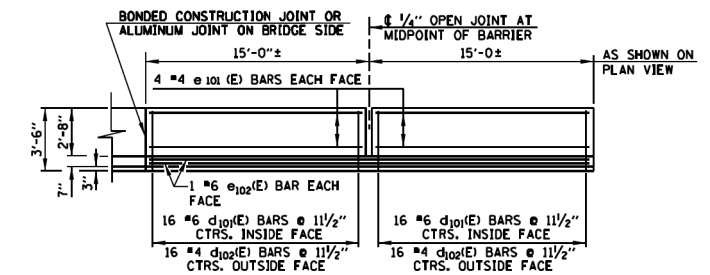
DETAIL A  
TYPICAL LONGITUDINAL JOINT



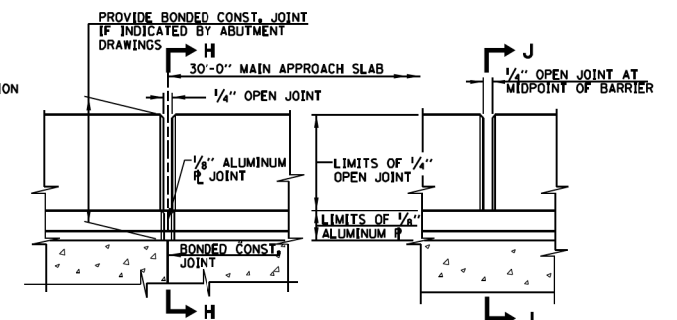
CROSS SECTION THRU OPTIONAL LONGITUDINAL CONSTRUCTION JOINT BETWEEN TRAFFIC LANES



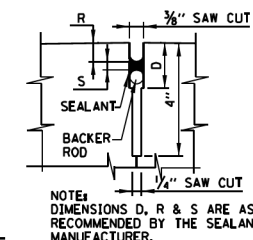
CROSS SECTION THRU LONGITUDINAL JOINT WITH OPTIONAL CONSTRUCTION JOINT



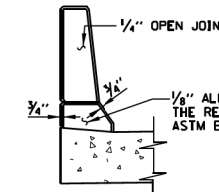
BARRIER ELEVATION



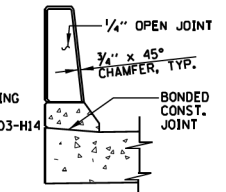
ELEVATION DETAIL OF BARRIER JOINTS



DETAIL E



SECTION H-H



SECTION J-J

NOTES:

- SEE SHEET 1 (OF 3) OF THIS SERIES FOR NOTES ON THIS SHEET.
- THE DIMENSION  $\pm$  IS THE THICKNESS OF THE MAIN APPROACH SLAB (1'-3") OR THE TRANSITION APPROACH SLAB AS DEFINED IN THE DESIGN PLANS.

SHEET 2 OF 3

**Illinois Tollway**  
Open Roads for a Faster Future

APPROACH SLAB TO J.P.C. PAVEMENT, MAINLINE

STANDARD G3-02

*Paul Kovacs*  
APPROVED CHIEF ENGINEER DATE 2-28-2008

BOWMAN, BARRETT & ASSOCIATES INC.  
CONSULTING ENGINEERS  
Chicago, Illinois  
312.228.0100  
www.bbandainc.com

FILE NAME = \$FILES\$	USER NAME = default	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TOLLWAY STANDARD DRAWING	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED -			94	49-1-R-1	LAKE	677	664B	
PLOT SCALE = \$SCALE\$		CHECKED - RGR	REVISED -			CONTRACT NO. 60L77					
PLOT DATE = 6/20/2012		DATE - 6/20/2012	REVISED -			ILLINOIS FED. AID PROJECT					
SCALE:						SHEET NO. N/A OF N/A SHEETS	STA.	TO STA.			

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