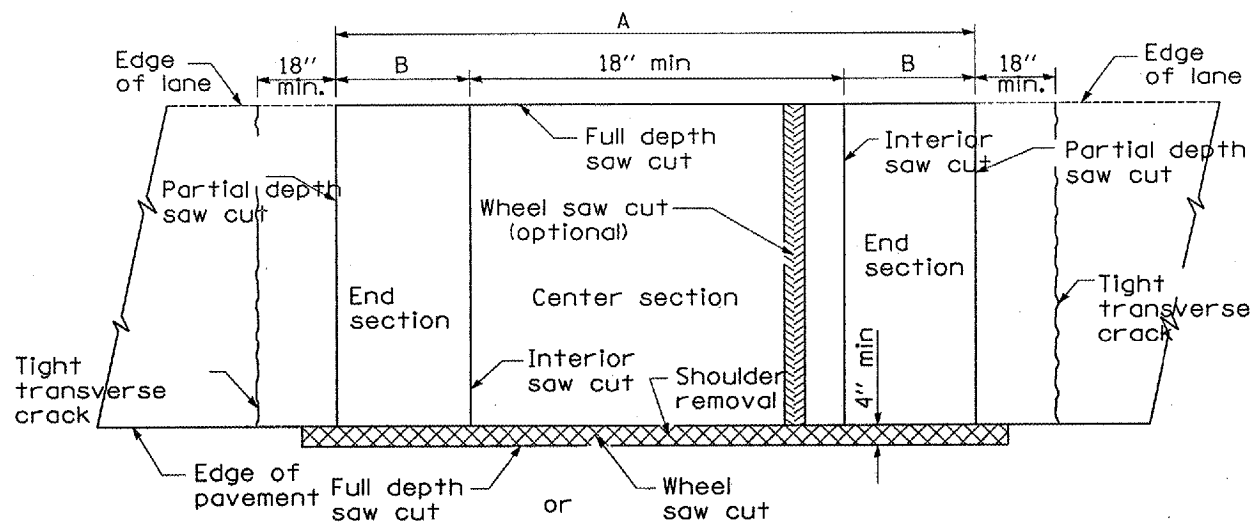
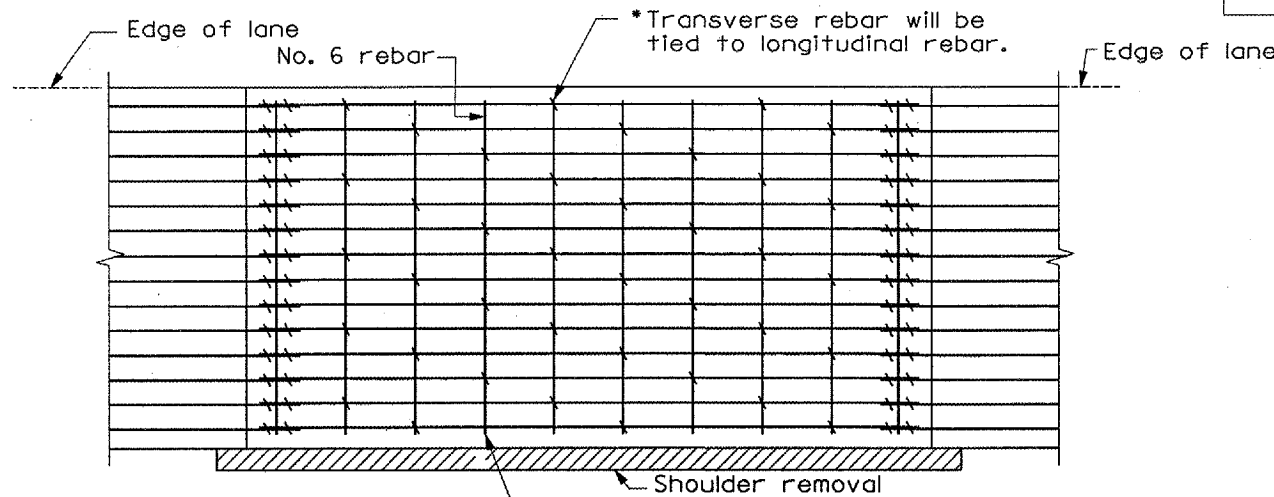


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(27-38)I-1 & (27-1)I-2	FORD	14	11
ILLINOIS				



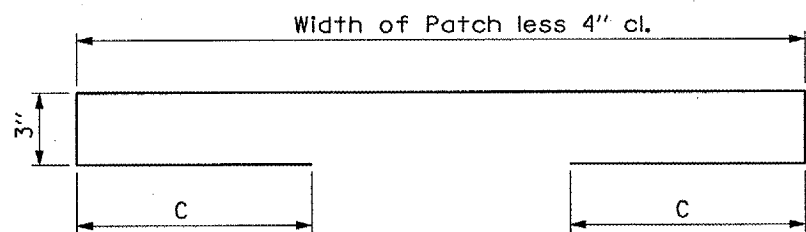
**PAVEMENT SAWING DETAIL**  
(BITUMINOUS SHOULDER)



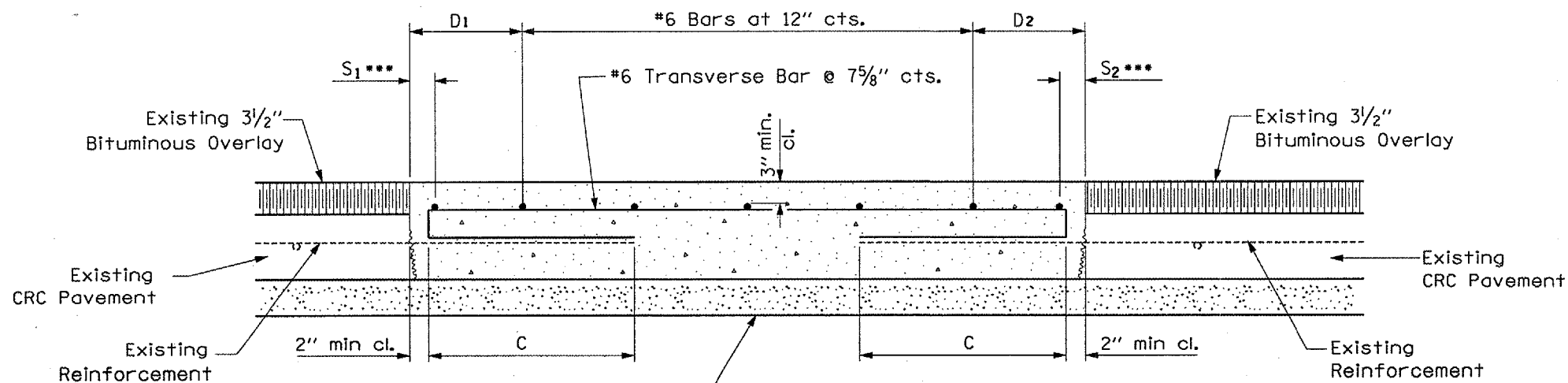
**PAVEMENT REINFORCEMENT DETAIL**

Transverse rebar will extend to outer longitudinal rebar while providing a minimum 3" clearance from existing pavement edge.

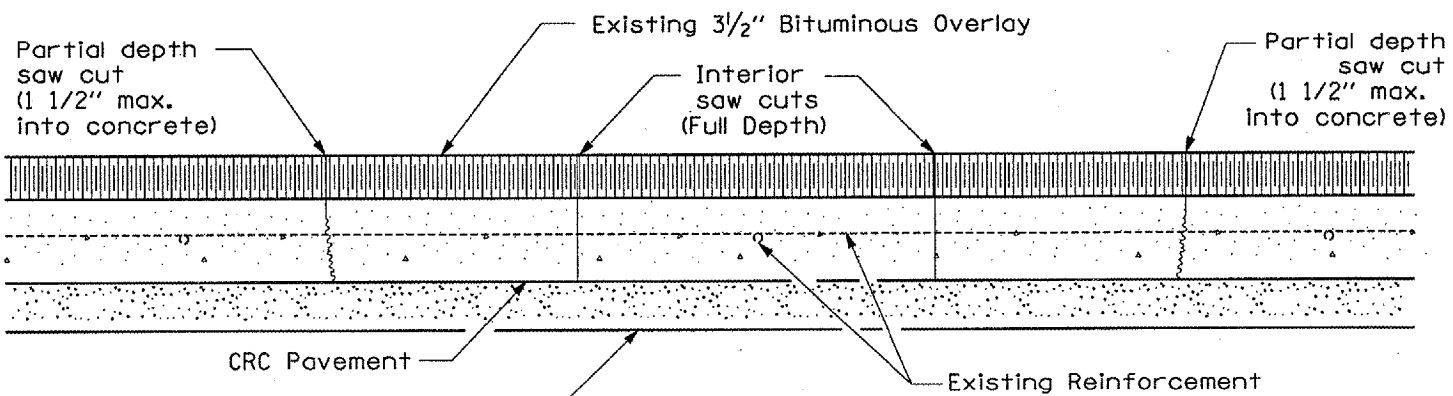
EXISTING LONGITUDINAL REINFORCEMENT BARS	A (min.)	B (min.)	C (min.)
No. 6	6'-0"	21"	22"



**#6 TRANSVERSE BAR**



**PROPOSED CLASS A PATCHING DETAIL**



**SAW CUT DETAIL**

- Every 3rd intersection must be tied.
- Lap new reinforcement with existing longitudinal reinforcement
- Variable: Where  $S_1$  and  $S_2$  are  $2\frac{1}{2}$ " min. and 12" max.  $D_1 = 2(S_1)$  and  $D_2 = 2(S_2)$ .

**GENERAL NOTES**

When patching two adjacent lanes in one operation, the longitudinal joint shall be a sawed longitudinal joint as detailed on Standard 420001; however, the groove shall be either preformed or sawed. Tie bars shall only be included for patches 6.0 m (20') or longer.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CLASS A PATCHING DETAILS**  
FAI 57  
SEC: (27-38)I-1 & (27-1)I-2  
FORD COUNTY