

# DUPAGE AIRPORT AUTHORITY WEST CHICAGO, ILLINOIS

## CONSTRUCTION PLANS FOR DUPAGE AIRPORT

### REHABILITATE TAXIWAYS A AND C

AIP PROJECT: 3-17-0017-B19  
ILLINOIS PROJECT: DPA-3454

JUNE 9, 2006

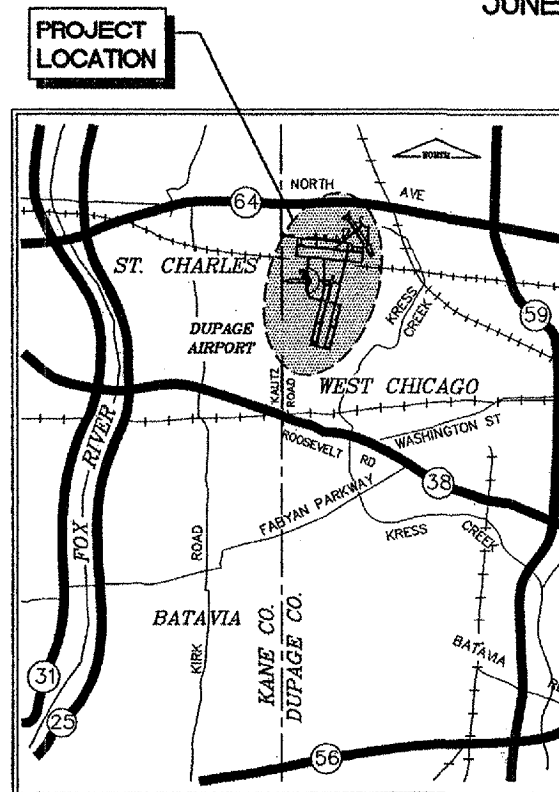
**DUPAGE AIRPORT**  
  
TOWNSHIP: 40 NORTH      WAYNE TOWNSHIP  
RANGE: 9 EAST          (SECTIONS: 31)  
DUPAGE COUNTY

**TAXIWAYS A AND C**  
DESIGN AIRCRAFT APPROACH CATEGORY D  
DESIGN AIRCRAFT GROUP III

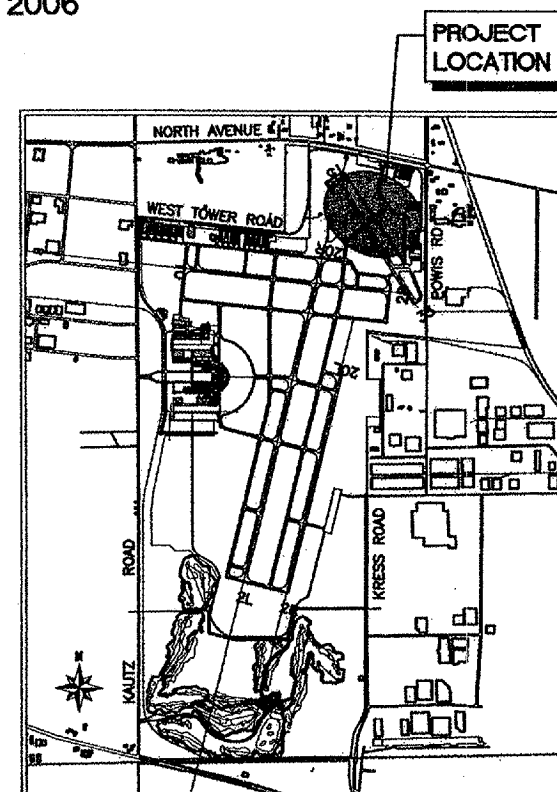
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CONSULTING ENGINEERS  
600 N. COMMONS DRIVE  
SUITE 107  
ALPHORA, IL 60504  
PHONE (630) 820-1922  
FAX (630) 820-0950  
SUBMITTED BY: *[Signature]*  
DATE: *6/9/06*  
DANIEL L. PAPE, P.E.

**DDA**  
**DuPage Airport**  
2700 INTERNATIONAL DRIVE  
SUITE 200  
WEST CHICAGO, IL 60185  
APPROVED BY: *[Signature]*  
DAVID BIRD - EXECUTIVE DIRECTOR  
DATE: *6/9/06*



LOCATION MAP



SITE PLAN

**INDEX TO SHEETS**

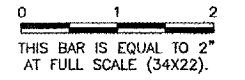
- 1 COVER SHEET
- 2 SUMMARY OF QUANTITIES
- 3 SITE PLAN / HORIZONTAL AND VERTICAL CONTROL
- 4 SEQUENCE OF CONSTRUCTION
- 5 SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS PER AC 150/5370-2E
- 6 STORM WATER POLLUTION PREVENTION PLAN
- 7 STORM WATER POLLUTION PREVENTION PLAN NOTES
- 8 STORM WATER POLLUTION PREVENTION PLAN DETAILS
- 9 TYPICAL SECTIONS
- 10 EXISTING CONDITIONS / PROPOSED REMOVALS - TAXIWAY C
- 11 EXISTING CONDITIONS / PROPOSED REMOVALS - TAXIWAY A
- 12 GRADING PLAN - TAXIWAY C
- 13 GRADING PLAN - TAXIWAY A
- 14 OVERLAY DEPTH CHART
- 15 PAVEMENT MARKING PLAN / LIGHTING PLAN TAXIWAY C
- 16 PAVEMENT MARKING PLAN / LIGHTING PLAN TAXIWAY A
- 17 LIGHTING PLAN TAXIWAY B NORTH
- 18 ELECTRICAL DETAILS SHEET 1
- 19 ELECTRICAL DETAILS SHEET 2
- 20 CROSS SECTIONS TAXIWAY C

**SUMMARY OF QUANTITIES**

ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR108158	1/C #8 5 KV UG CABLE IN UD	LF	14,485	
AR125415	MITL-BASE MOUNTED	EACH	29	
AR125441	TAXI GUIDANCE SIGN, 1 CHARACTER	EACH	1	
AR125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EACH	2	
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	1	
AR125446	TAXI GUIDANCE SIGN, 6 CHARACTER	EACH	1	
AR125901	REMOVE STAKE MOUNTED LIGHT	EACH	6	
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	12	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	3	
AR125906	REMOVE SPLICE CAN	EACH	1	
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR156520	INLET PROTECTION	EACH	10	
AR201663	SAND MIX CRACK REPAIR	LF	1,200	
AR201670	CRACK CONTROL FABRIC	SY	24,500	
AR401610	BITUMINOUS SURFACE COURSE	TON	2,860	
AR401620	BIT. SURFACE COURSE - LEVELING	TON	2,530	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1	
AR401650	BITUMINOUS PAVEMENT MILLING	SY	5,800	
AR401655	BUTT JOINT CONSTRUCTION	SY	2,280	
AR401900	REMOVE BITUMINOUS PAVEMENT	SY	2,900	
AR401915	REM AND REP BIT PAVEMENT - TYPE A	SY	1,140	
AR401916	REM AND REP BIT PAVEMENT - TYPE B	SY	500	
AR510900	REMOVE TIE DOWN	EACH	6	
AR603510	BITUMINOUS TACK COAT	GAL	3,900	
AR620520	PAVEMENT MARKING - WATERBOURNE	SF	7,030	
AR620525	PAVEMENT MARKING - BLACK BORDER	SF	7,490	
AR705900	REMOVE UNDERDRAIN	LF	215	
AR705904	REMOVE UNDERDRAIN CLEANOUT	EACH	2	
AR800053	SOIL GUARD	SY	12,000	
AR800104	SHOULDER FILL	CY	800	
AR800894	RETROFIT EXISTING MITL OR TAXIWAY SIGN	EACH	120	
AR901510	SEEDING	ACRE	2.50	
AR905530	TOPSOILING	SY	8,800	

REVISIONS

NUMBER	BY	DATE



**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 REHABILITATE TAXIWAYS ALPHA AND CHARLIE**

**SUMMARY OF QUANTITIES**

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DESIGN BY:	JRL
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APPROVED BY:	DLP
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JOB No:	04257-05

ILLINOIS PROJECT: DPA-3454  
 A.I.P. PROJECT: 3-17-0017-B19

HORIZONTAL CONTROL

NO.	DESCRIPTION	NORTHING	EASTING
1	TAXIWAY E AND TAXIWAY C INTERSECTION	1911203.08	524069.57
2	TAXIWAY C CURVE	1911659.06	524117.21
3	TAXIWAY C AND RUNWAY 15/33 INTERSECTION	1912023.47	524558.55
4	TAXIWAY C (WEST) AND TAXIWAY B INTERSECTION	1912184.28	524753.32
5	TAXIWAY C AND TAXIWAY A INTERSECTION	1912453.39	525079.26
6	TAXIWAY A BEGIN IMPROVEMENTS	1911235.38	525438.81
7	TAXIWAY A CURVE	1912267.11	525448.58
8	TAXIWAY A END IMPROVEMENTS	1913021.76	524888.71

REVISIONS

NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 REHABILITATE TAXIWAYS ALPHA AND CHARLIE

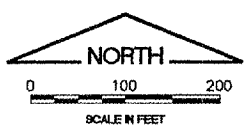
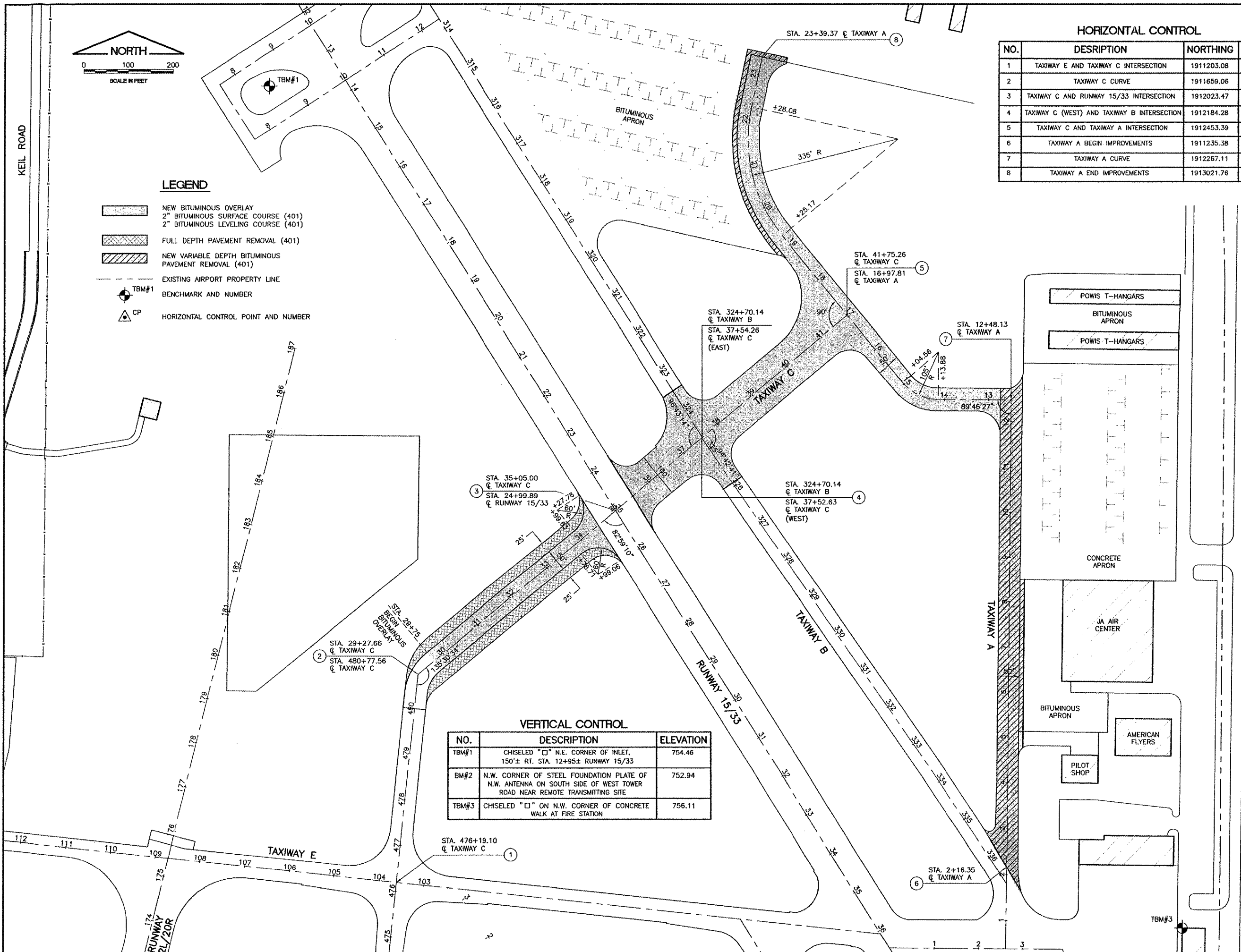
SITE PLAN /  
 HORIZONTAL AND VERTICAL CONTROL

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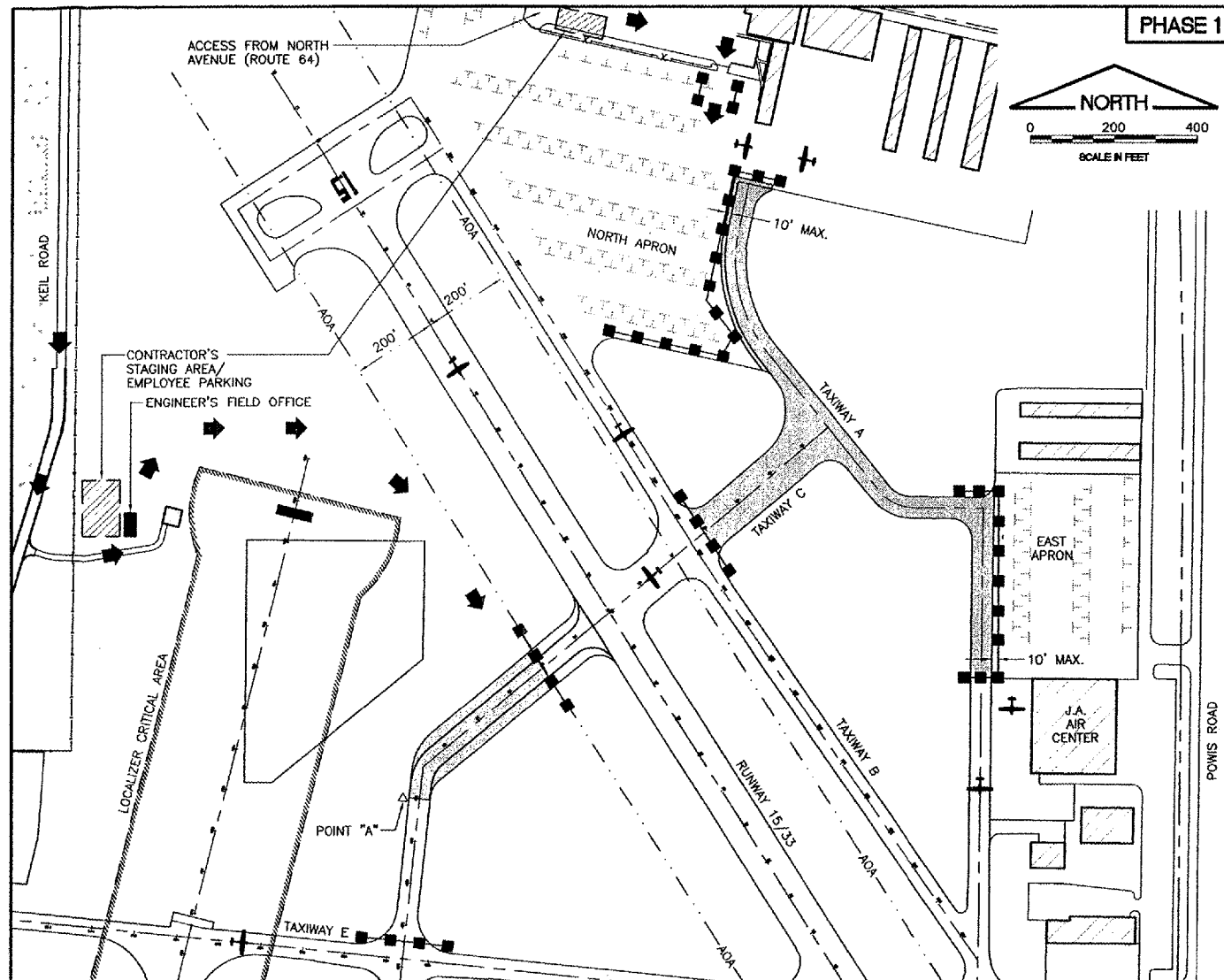


LEGEND

- NEW BITUMINOUS OVERLAY  
 2" BITUMINOUS SURFACE COURSE (401)  
 2" BITUMINOUS LEVELING COURSE (401)
- FULL DEPTH PAVEMENT REMOVAL (401)
- NEW VARIABLE DEPTH BITUMINOUS PAVEMENT REMOVAL (401)
- EXISTING AIRPORT PROPERTY LINE
- TBM#1 BENCHMARK AND NUMBER
- CP HORIZONTAL CONTROL POINT AND NUMBER

VERTICAL CONTROL

NO.	DESCRIPTION	ELEVATION
TBM#1	CHISELED "□" N.E. CORNER OF INLET, 150'± RT. STA. 12+95± RUNWAY 15/33	754.46
BM#2	N.W. CORNER OF STEEL FOUNDATION PLATE OF N.W. ANTENNA ON SOUTH SIDE OF WEST TOWER ROAD NEAR REMOTE TRANSMITTING SITE	752.94
TBM#3	CHISELED "□" ON N.W. CORNER OF CONCRETE WALK AT FIRE STATION	756.11



**PHASE 1**

**PHASE 1  
SUGGESTED SEQUENCE OF CONSTRUCTION**

- PLACE LATHE AND RIBBON AT AIR OPERATIONS AREA (A.O.A.) LIMITS OF RUNWAY 15/33 AND RUNWAY 2L/20R LOCALIZER CRITICAL AREA.
- CLOSE PORTIONS OF TAXIWAY ALPHA AND CHARLIE AND PLACE BARRICADES AS SHOWN ON THE PLANS.
- PLACE EROSION CONTROL DEVICES.
- COMPLETE FULL DEPTH PAVEMENT PATCHING, CRACK SEALING AND BUTT JOINT CONSTRUCTION.
- COMPLETE REFLECTIVE CRACK CONTROL, TAXIWAY OVERLAY AND SHOULDER FILL.
- INSTALL LIGHTING, LANDSCAPING AND PAVEMENT MARKING.
- REMOVE BARRICADES AND OPEN PORTIONS OF TAXIWAY ALPHA AND CHARLIE FOR AIRPORT TRAFFIC.

**PHASE 3  
SUGGESTED SEQUENCE OF CONSTRUCTION**

- CLOSE PORTIONS OF TAXIWAY ALPHA AND PLACE BARRICADES AS SHOWN ON THE PLANS.
- COMPLETE FULL DEPTH PAVEMENT PATCHING, CRACK SEALING AND BUTT JOINT CONSTRUCTION.
- COMPLETE REFLECTIVE CRACK CONTROL, TAXIWAY OVERLAY AND SHOULDER FILL.
- INSTALL LIGHTING, LANDSCAPING AND PAVEMENT MARKING.
- REMOVE BARRICADES AND OPEN TAXIWAY ALPHA FOR AIRPORT TRAFFIC.

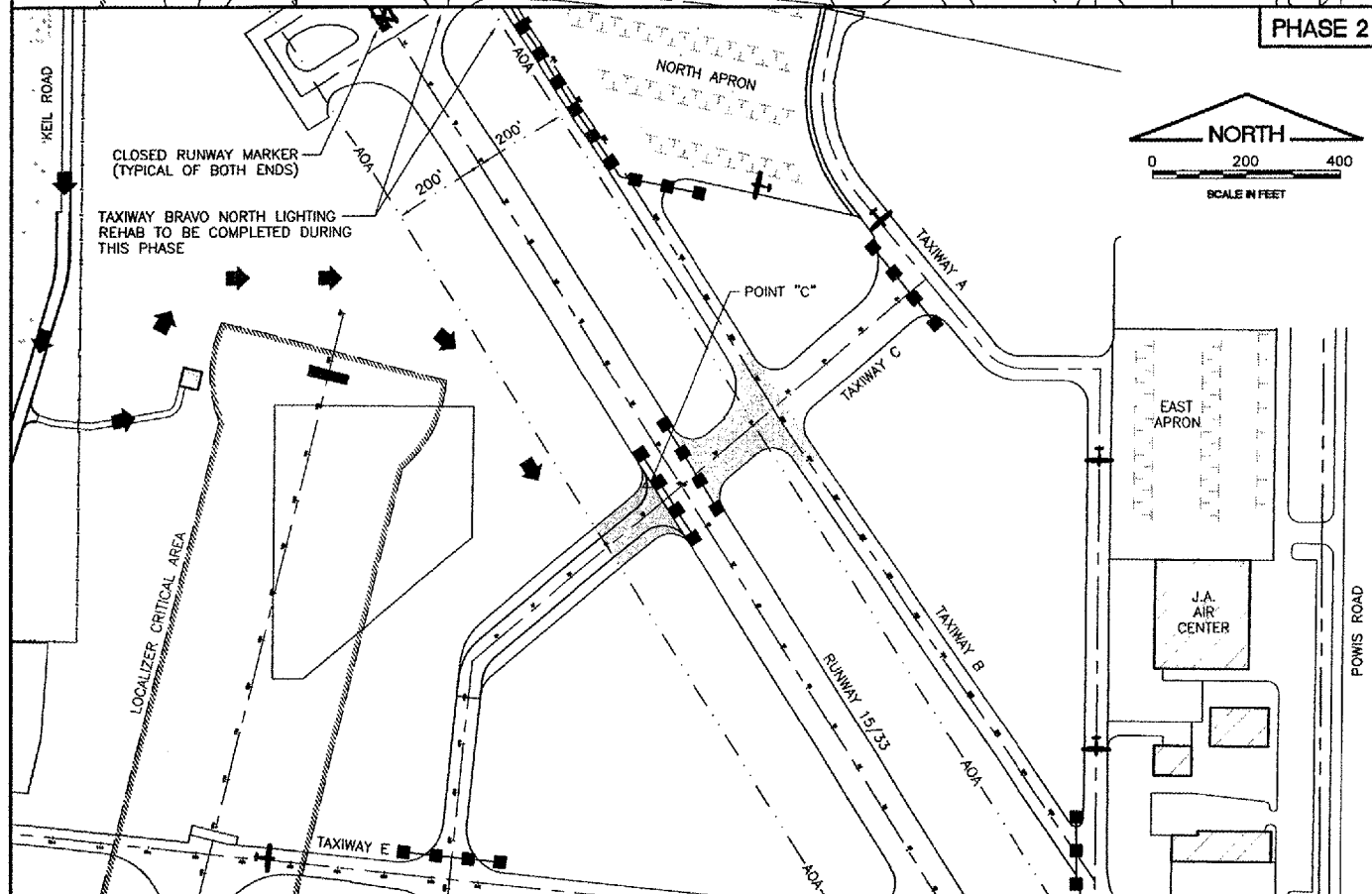
**PHASE 2  
SUGGESTED SEQUENCE OF CONSTRUCTION**

- PHASE 2 SHALL BE COMPLETED WITHIN 15 CALENDAR DAYS.
- CLOSE RUNWAY 15/33 AND PLACE RUNWAY CLOSURE MARKERS.
- CLOSE TAXIWAY CHARLIE AND BRAVO AND PLACE BARRICADES AS SHOWN ON THE PLANS.
- COMPLETE FULL DEPTH PAVEMENT PATCHING, CRACK SEALING AND BUTT JOINT CONSTRUCTION.
- COMPLETE REFLECTIVE CRACK CONTROL, TAXIWAY OVERLAY AND SHOULDER FILL.
- INSTALL LIGHTING, LANDSCAPING AND PAVEMENT MARKING.
- REMOVE BARRICADES AND OPEN ALL TAXIWAYS AND RUNWAYS.

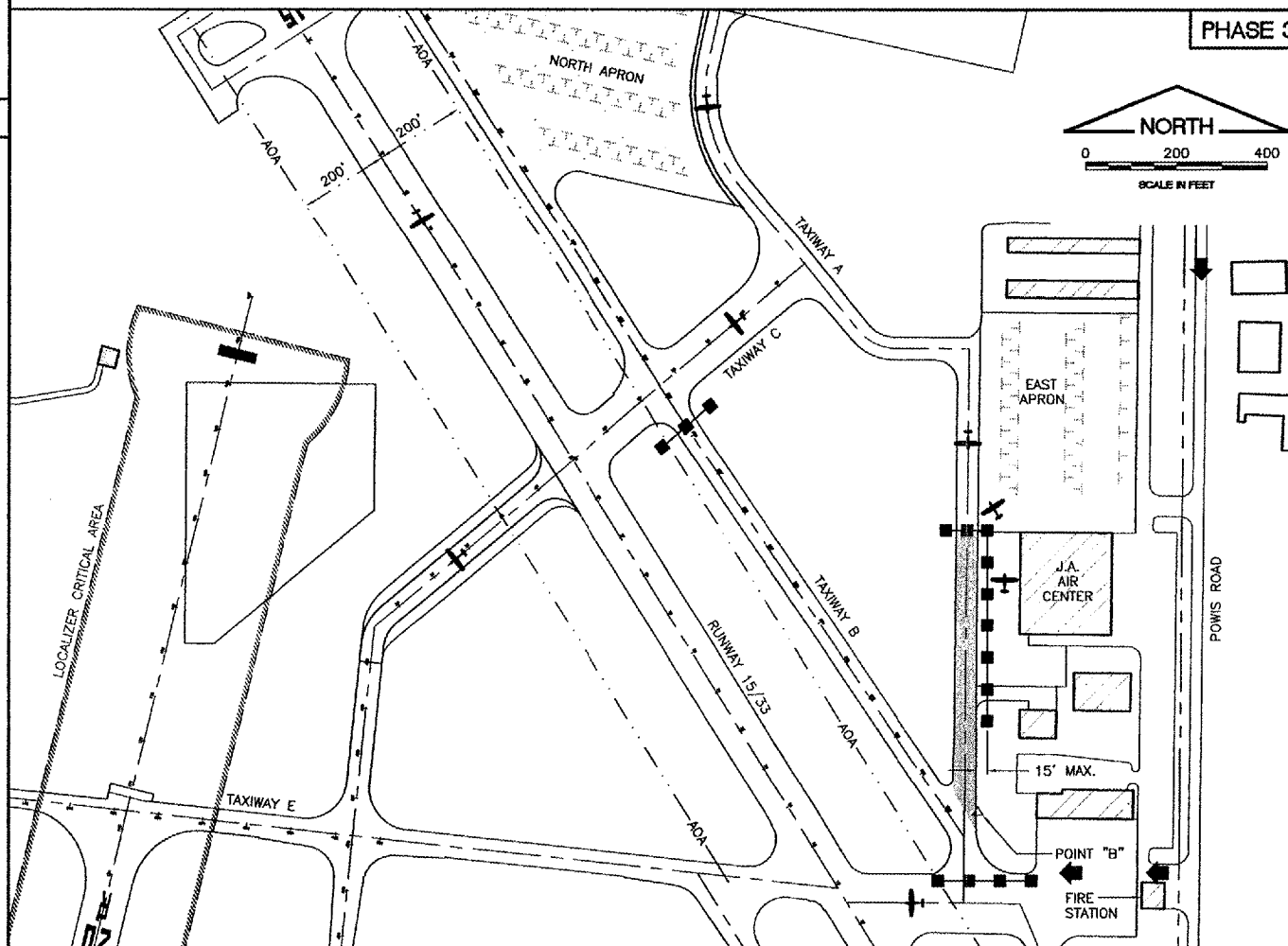
**LEGEND**

- ➔ CONTRACTOR'S ACCESS TO SITE
- ✈ AIRCRAFT MOVEMENT AREA
- A.O.A. AIRCRAFT OPERATIONS AREA (A.O.A.)
- BARRICADES WITH FLASHING RED LIGHTS
- - - AIRPORT PROPERTY LINE
- ▨ LIMITS OF WORK (EACH PHASE)
- X CLOSED RUNWAY MARKER

PHASE	WORK AREA	ALLOWABLE WORK PERIODS	OPERATIONAL RESTRICTIONS
1	TAXIWAY CHARLIE OUTSIDE OF RUNWAY 15/33 A.O.A., EAST OF TAXIWAY BRAVO AND NORTH SECTIONS OF ALPHA	NO RESTRICTIONS	TAXIWAY CHARLIE AND ALPHA CLOSED IN WORK AREAS RUNWAY 15/33, TAXIWAY BRAVO AND SOUTH SECTION OF TAXIWAY ALPHA OPEN
2	TAXIWAY CHARLIE WITHIN A.O.A. AND TAXIWAY BRAVO LIGHTING REHABILITATION WORK	WITHIN RUNWAY 15/33 A.O.A. MONDAY 7:30 AM TO FRIDAY 2:30 PM 15 CALENDAR DAYS	TAXIWAY CHARLIE CLOSED IN WORK AREAS TAXIWAY ALPHA OPEN RUNWAY 15/33 CLOSED DURING THE WEEK FROM MONDAY 7:30 AM TO FRIDAY 2:30 PM
3	SOUTH SECTION OF TAXIWAY ALPHA	NO RESTRICTIONS	SOUTH SECTION OF TAXIWAY ALPHA CLOSED IN WORK AREAS RUNWAY 15/33, TAXIWAY BRAVO AND NORTH SECTION OF TAXIWAY ALPHA OPEN



**PHASE 2**



**PHASE 3**

**DU074**

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LAYOUT: Layout1  
UPDATE BY: johse  
SURVEY BOOK #  
DATE: Tue 6/20/06 2:19pm  
XREF DWG: tbcint.dwg  
tb.dwg  
tybase.dwg

**REVISIONS**

NUMBER	BY	DATE

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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DUPAGE AIRPORT  
WEST CHICAGO, ILLINOIS  
REHABILITATE TAXIWAYS ALPHA AND CHARLIE**

**SEQUENCE OF CONSTRUCTION**

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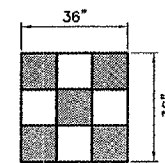
ILLINOIS PROJECT: DPA-3454  
A.I.P. PROJECT: 3-17-0017-B19

**GENERAL NOTES:**

- ALL CONSTRUCTION SEQUENCING AND OPERATIONS SHALL CONFORM TO THE APPLICABLE PROVISIONS OF AC 150/5370-2E OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION (LATEST EDITION). PAYMENT FOR MAINTENANCE OF TRAFFIC SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR MUST STAGE CONSTRUCTION AROUND AIRPORT OPERATIONS. STAGING SHOWN IS SUGGESTED AND IS INTENDED TO PROVIDE THE CONTRACTOR WITH MAJOR WORK AREAS WHILE MINIMIZING DISRUPTIONS TO AIRPORT OPERATIONS. THE CONTRACTOR MAY USE ALTERNATE STAGING PLANS; HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DIRECTOR AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER TWO WORKING DAYS IN ADVANCE OF ANY STAGING CHANGES WHICH WILL REQUIRE CHANGES IN AIRCRAFT MOVEMENT. THE RESIDENT ENGINEER SHALL THEN NOTIFY THE AIRPORT WHO WILL ISSUE APPROPRIATE NOTAMS. THE MOVING AND MAINTENANCE OF ALL TRAFFIC CONTROL SIGNS, BARRICADES, LATHE/RIBBON AT A.O.A. LIMITS ETC. SHALL BE THE CONTRACTOR'S RESPONSIBILITY, AND SHALL BE INCIDENTAL TO THE CONTRACT.
- BARRICADES AT 10' CENTERS SHALL BE PLACED AT THE LOCATIONS SHOWN OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER, HAVE A FLASHING RED LIGHT AND CONFORM TO IDOT STANDARD 702001, TYPE II. ROPE WITH HIGH VISIBILITY ORANGE FLAGGING SHALL BE INSTALLED BETWEEN ALL BARRICADES.
- THE AIRPORT DIRECTOR IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING PAVEMENTS, EXCEPT AS SHOWN OR WITH THE PRIOR APPROVAL OF THE AIRPORT DIRECTOR.
- EXISTING AREAS BEYOND PROJECT LIMITS INCLUDING THE HAUL ROAD(S) DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR TO PRE-CONSTRUCTION CONDITION AT NO ADDITIONAL COST TO THE OWNER AND TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DIRECTOR.
- EACH DAY AT THE COMPLETION OF WORK, OR MORE FREQUENTLY AS DETERMINED BY THE AIRPORT DIRECTOR, THE CONTRACTOR SHALL BE REQUIRED TO USE A PICKUP TYPE SWEEPER IN ALL ACTIVE CONSTRUCTION AIRFIELD PAVEMENT AREAS AND HAUL ROADS. THE COST OF SWEEPING SHALL BE INCIDENTAL TO THE CONTRACT.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- CONTRACTOR'S WORK WITHIN AIRCRAFT OPERATIONS AREA (A.O.A.) SHALL BE EXPEDITED TO MINIMIZE ANY TEMPORARY CLOSURE TIME.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.
- ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND DIRECTOR OF OPERATIONS AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE A DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST OF CROSSING GUARDS, PAVEMENT REPAIR OR ANY NECESSARY ITEMS ASSOCIATED WITH THE SEQUENCE OR STAGING SHALL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- MATERIALS REMOVED FROM THE PROJECT SHALL BE DISPOSED OF OFF OF AIRPORT PROPERTY.
- PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, BARRICADES, SIGNING, RUNWAY CLOSE MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR WILL BE REQUIRED TO PLACE A LINE OF LATHE AND RIBBON ALONG THE AIR OPERATIONS AREA (A.O.A.) LIMIT AND ALONG DESIGNATED LIMITS OF CONSTRUCTION AT LOCATIONS AS SHOWN ON THE PLANS.

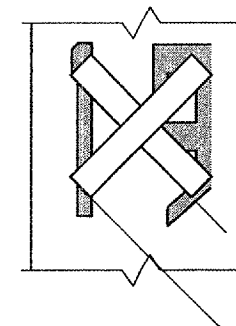
- THE CONTRACTOR SHALL CONTACT THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR A MINIMUM OF FIVE (5) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS.
- ORANGE CONES SHALL BE PLACED AT 25' CENTERS ALONG THE PAVEMENT EDGE DURING PAVING OPERATIONS OF THE CLOSURE LANES TO PREVENT VEHICLES FROM ENTERING.
- VEHICLES AND EQUIPMENT OPERATING ON THE AIRPORT SHALL DISPLAY ORANGE AND WHITE FLAGS (SEE DETAIL THIS SHEET) AND HAVE BEACON LIGHTS.
- ANY PAVEMENT DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR AT NO ADDITIONAL COST TO THE OWNER. THE PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE A DEBRIS-FREE SURFACE DURING HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED AT ALL TIMES UNLESS THE GATE IS MANNED WITH A GUARD (COST INCIDENTAL). THE CONTRACTOR SHALL PROVIDE AN "AUTHORIZED PERSONNEL ONLY" SIGN AT THE ACCESS GATE. THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COSTS RELATING TO THE CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, HE SHALL DO SO AT NO EXTRA COST TO THE CONTRACT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO WORK.
- THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE A STONE/ASPHALT TRUCK, WHICH HAS A MAXIMUM HEIGHT OF 20 FEET IN A FULL DUMP POSITION.
- IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION, THE CONTRACTOR SHALL PLACE "RUNWAY CLOSED" MARKERS OVER THE NUMERALS AS DETAILED IN THE PLANS. OTHERWISE, "RUNWAY CLOSED" MARKERS SHALL BE PLACED IN THE TURF AT THE ENDS OF THE RUNWAY AS DETAILED IN THE PLANS.

**NOTE - ALL PHASES**  
ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.



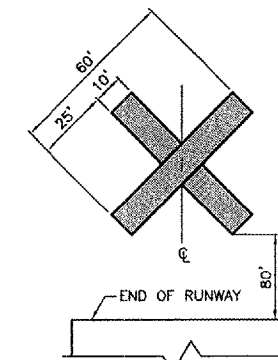
**CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG**

ORANGE/WHITE



- CLOSED RUNWAY MARKERS SHALL BE YELLOW.
- MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER.
- CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
- MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
- COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.

**CLOSED RUNWAY MARKER DETAIL**  
ON PAVEMENT - NO SCALE



- CLOSED RUNWAY MARKERS SHALL BE YELLOW.
- MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER.
- CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
- MARKERS SHALL BE PLACED 80- FEET OFF THE RUNWAY-END OR AS NOTED.
- COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

**CLOSED RUNWAY MARKER DETAIL**  
OFF PAVEMENT - NO SCALE

**DESIGN AIRCRAFT APPROACH CATEGORY: B**  
**DESIGN AIRPORT GROUP: II**

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 20'		
POINT "A" CLOSEST POINT OF CONSTRUCTION TO RUNWAY 2L/20R	POINT "B" CLOSEST POINT OF CONSTRUCTION TO RUNWAY 10/28	POINT "C" CLOSEST POINT OF CONSTRUCTION TO RUNWAY 15/33
STA. ±480+00, ±40' LT. CENTERLINE TAXIWAY C	STA. ±2+00, ±30' RT. CENTERLINE TAXIWAY A	STA. ±34+50, ±50' LT. CENTERLINE TAXIWAY C
LATITUDE: 41°54'54.14" LONGITUDE: 88°14'41.83" EXISTING ELEVATION: 754.2	LATITUDE: 41°54'49.87" LONGITUDE: 88°14'23.28" EXISTING ELEVATION: 754.94	LATITUDE: 41°54'58.83" LONGITUDE: 88°14'36.62" EXISTING ELEVATION: 755.0

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT:

- CONSTRUCT SOUTH FLIGHT CENTER APRON - PHASE 4
- REHABILITATE, OVERLAY, WIDEN AND RELOCATE PORTIONS OF TAXIWAY E.

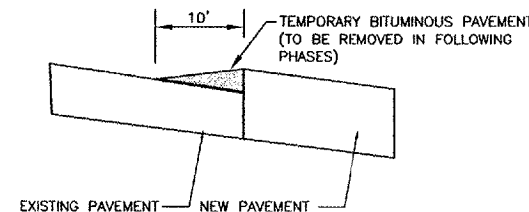
**LIMITATIONS ON CONSTRUCTION WITHIN AIRPORT OPERATIONS AREA (AOA)**

**RUNWAYS:**

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND DIRECTOR OF OPERATIONS TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION PHASE CHANGES. WORK SHALL BE EXPEDITED IN RUNWAY 15 / 33 A.O.A. AND NEAR OPEN TAXIWAYS. RUNWAY 15 / 33 SHALL BE CLOSED FOR THE DURATION OF PHASE 3. THE CONTRACTOR SHALL BE GIVEN 15 CALENDAR DAYS TO COMPLETE WORK WITHIN RUNWAY 15 / 33 A.O.A. . IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT.

**TAXIWAYS:**

CONSTRUCTION WILL BE ALLOWED UP TO THE EDGE OF THE TAXIWAY PAVEMENTS WITHOUT CLOSURE ON A LIMITED BASIS. WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT DIRECTOR TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.



**TEMPORARY BITUMINOUS RAMPS**  
NO SCALE

- RAMPS SHALL BE CONSTRUCTED TO ACCOMADATE CHANGE IN PHASES AND AS DIRECTED BY THE ENGINEER. THE COST OF TEMPORARY RAMPS WILL BE PAID UNDER BUTT JOINT (401655) CONSTRUCTION AND BITUMINOUS PAVEMENT (AR401610).

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UPDATE BY: johae  
SURVEY BOOK #  
DATE: Tue 6/20/06 1:37pm  
XREF DWG: tbcint.dwg  
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NUMBER	BY	DATE

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**DUPAGE AIRPORT**  
**WEST CHICAGO, ILLINOIS**  
**REHABILITATE TAXIWAYS ALPHA AND CHARLIE**

**SEQUENCE OF CONSTRUCTION**  
**GENERAL NOTES AND DETAILS**  
**PER AC 150/5370-2E**

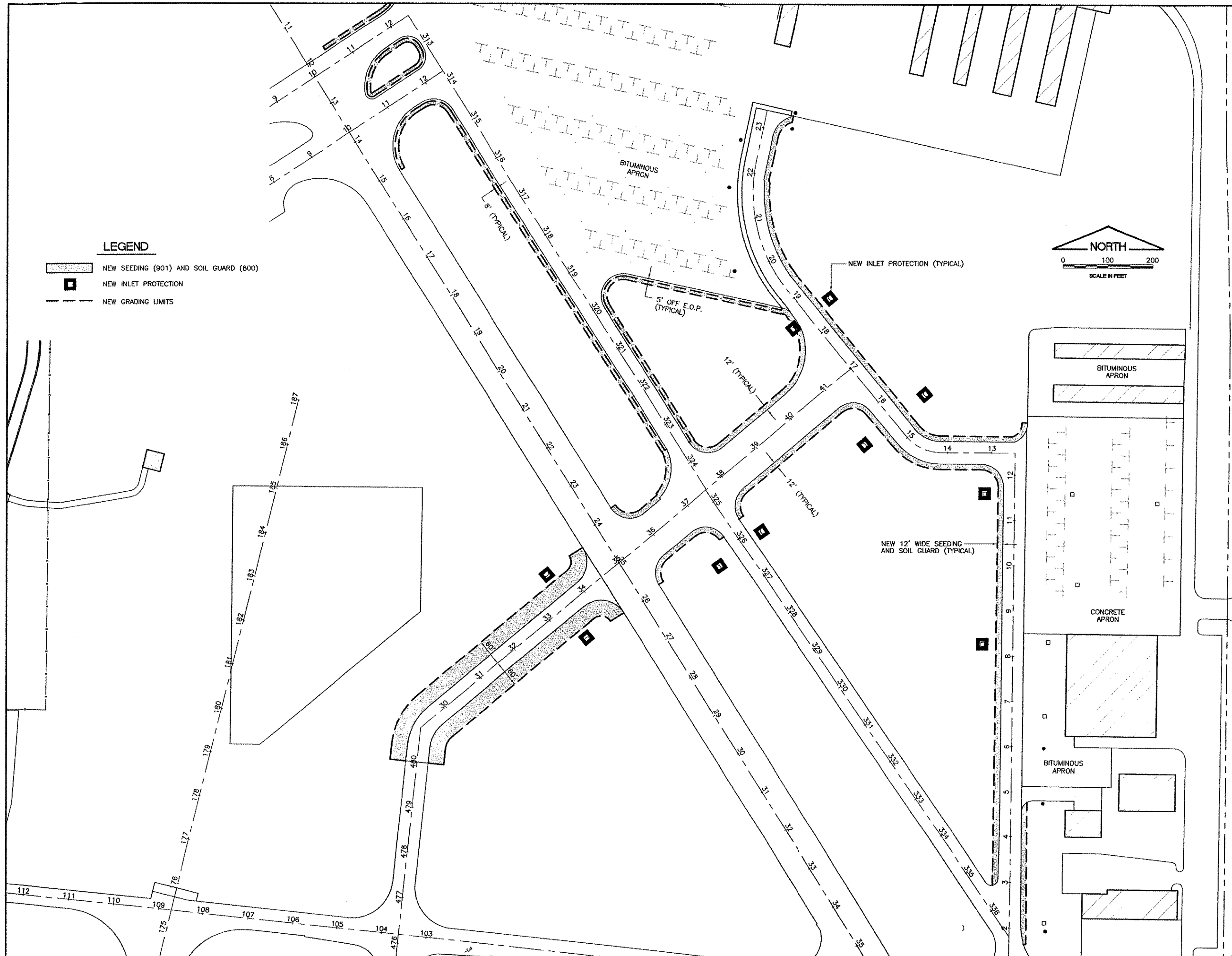
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DESIGN BY: JRL  
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CHECKED BY: CAL  
APPROVED BY: DLP  
DATE: 06/09/06  
JOB No: 04257-05

ILLINOIS PROJECT: DPA-3454  
A.I.P. PROJECT: 3-17-0017-B19





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**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 REHABILITATE TAXIWAYS ALPHA AND CHARLIE  
 STORM WATER POLLUTION  
 PREVENTION PLAN**

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APPROVED BY:	DLP
DATE:	06/09/06
JOB No:	04257-05
ILLINOIS PROJECT: DPA-3454 A.I.P. PROJECT: 3-17-0017-B19	
SHEET 6 OF 20 SHEETS	

# STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

## SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATING TAXIWAYS A AND C WITH A BITUMINOUS OVERLAY AT THE DUPAGE AIRPORT. THE PROJECT INCLUDES VARIOUS PAVEMENT ITEMS, ELECTRICAL IMPROVEMENTS AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

1. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS INLET PROTECTION.
2. REMOVAL OF EXISTING PAVEMENT/PAVEMENT PATCHING.
3. PAVEMENT CONSTRUCTION.
4. ELECTRICAL IMPROVEMENTS.
5. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
6. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCHING.

## AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 7.0 ACRES OF WHICH 1.6 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

## OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

## DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE KRESS CREEK THROUGH A STORM SEWER SYSTEM.

## CONTROLS-EROSION CONTROLS AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION

1. THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER 1LR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

## DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN THREE DAYS. STOCKPILES SHALL NOT BE LOCATED IN SPECIAL MANAGEMENT AREAS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
  - A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
  - B. EXCAVATED AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
  - C. ANY WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION SHALL BE FILTERED.

4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
5. THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2 INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD.
6. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.
7. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

## DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

## MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

## CONTRACTORS

1. THE STORM WATER POLLUTION PREVENTION PLAN MUST CLEARLY IDENTIFY FOR EACH MEASURE IDENTIFIED IN THE PLAN, THE CONTRACTOR(S) OR SUBCONTRACTOR(S) THAT WILL IMPLEMENT THE MEASURE. ALL CONTRACTORS AND SUBCONTRACTORS IDENTIFIED IN THE PLAN MUST SIGN A COPY OF THE CERTIFICATION STATEMENT IN PARAGRAPH 2 BELOW IN ACCORDANCE WITH PART VI.G (SIGNATORY REQUIREMENTS) OF THIS PERMIT. ALL CERTIFICATIONS MUST BE INCLUDED IN THE STORM WATER POLLUTION PREVENTION PLAN EXCEPT FOR OWNERS THAT ARE ACTING AS CONTRACTOR.
2. CERTIFICATION STATEMENT. ALL CONTRACTORS AND SUBCONTRACTORS IDENTIFIED IN A STORM WATER POLLUTION PREVENTION PLAN IN ACCORDANCE WITH PARAGRAPH 1 ABOVE SHALL SIGN A COPY OF THE FOLLOWING CERTIFICATION STATEMENT BEFORE CONDUCTING ANY PROFESSIONAL SERVICE AT THE SITE IDENTIFIED IN THE STORM WATER POLLUTION PREVENTION PLAN:

"I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (1LR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION."

THE CERTIFICATION MUST INCLUDE THE NAME AND TITLE OF THE PERSON PROVIDING THE SIGNATURE IN ACCORDANCE WITH PART VI.G OF THIS PERMIT: THE NAME, ADDRESS AND TELEPHONE NUMBER OF THE CONTRACTING FIRM; THE ADDRESS (OR OTHER IDENTIFYING DESCRIPTION) OF THE SITE; AND THE DATE THE CERTIFICATION IS MADE.

### CONTRACTOR CERTIFICATION

"I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (1LR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION."

### GENERAL CONTRACTOR

SIGNATURE \_\_\_\_\_ TITLE \_\_\_\_\_ DATE \_\_\_\_\_

COMPANY \_\_\_\_\_



## ILLINOIS ENVIRONMENTAL PROTECTION AGENCY NOTICE OF INTENT (NOI) GENERAL PERMIT TO DISCHARGE STORM SEWER CONSTRUCTION SITE ACTIVITIES

IMPORTANT: FORM MUST BE TYPED TO ENABLE AUTOMATED OPTICAL PROCESSING.  
SUBMIT ORIGINAL - DO NOT SUBMIT PHOTOCOPY

OWNER INFORMATION		OWNER TYPE (SELECT ONE AND TYPE "X")	
NAME: LAST FIRST MI. (SEE INSTRUCTIONS)	DUPAGE AIRPORT AUTHORITY	<input type="checkbox"/> PRIVATE	<input type="checkbox"/> COUNTY <input type="checkbox"/> STATE
MAILING ADDRESS:	2700 INTERNATIONAL DRIVE, SUITE 200	<input type="checkbox"/> CITY	<input checked="" type="checkbox"/> SPECIAL DISTRICT
CITY:	WEST CHICAGO ST. IL ZIP: 60185	<input type="checkbox"/> FEDERAL	
CONTACT PERSON:	BYRON MILLER	TELEPHONE NUMBER:	AREA CODE NUMBER 630 208-6172

CONTRACTOR INFORMATION		TELEPHONE NUMBER		AREA CODE		NUMBER	
NAME: LAST FIRST MI. (SEE INSTRUCTIONS)							
MAILING ADDRESS:		CITY:		ST.:		ZIP:	

CONSTRUCTION SITE INFORMATION		SELECT ONE AND TYPE "X"		CHANGE OF INFORMATION	
<input checked="" type="checkbox"/> EXISTING SITE	<input type="checkbox"/> NEW SITE	<input type="checkbox"/> OTHER REPAIRS	<input type="checkbox"/> PERMIT NUMBERS (IF APPLICABLE)	N/A	
FACILITY NAME:	DUPAGE AIRPORT	TELEPHONE NUMBER:	630 208-6172	AREA CODE:	208-6172
MAILING ADDRESS:	2700 INTERNATIONAL DRIVE, SUITE 200	CITY:	WEST CHICAGO	ST.:	IL
CITY:	WEST CHICAGO	ST.:	IL	ZIP:	60185
COUNTY:	DUPAGE	SECTION:	29, 31 & 32	TOWNSHIP:	40 NORTH
START CONSTRUCTION DATE:		END CONSTRUCTION DATE:		TOTAL SIZE OF CONSTRUCTION SITE IN ACRES:	1.6

TYPE OF CONSTRUCTION	
<input type="checkbox"/> RESIDENTIAL	<input type="checkbox"/> COMMERCIAL
<input type="checkbox"/> INDUSTRIAL	<input type="checkbox"/> RECONSTRUCTION
<input checked="" type="checkbox"/> TRANSPORTATION	<input type="checkbox"/> OTHER

RECEIVING WATER INFORMATION	
DOES YOUR STORM WATER DISCHARGE DIRECTLY TO (SELECT ONE AND TYPE "X")	
<input type="checkbox"/> WATER OF THE STATE	<input checked="" type="checkbox"/> STORM SEWER
NAME OF CLOSEST RECEIVING WATER (IF KNOWN):	KRESS CREEK
DOES THE QUANTITATIVE DATA CURRENTLY EXIST WHICH DESCRIBES THE CONCENTRATION OF POLLUTANTS IN THE STORM WATER DISCHARGES?	
<input type="checkbox"/> YES	<input checked="" type="checkbox"/> NO

"I certify under penalty of law that this document and all attachments were prepared under my direction and supervision in accordance with a system designed to ensure that qualified personnel properly gather and evaluate the information submitted. Based on my knowledge and belief, the information submitted is true, accurate, and complete. I am aware that there are significant penalties for submitting false information, including the possibility of fine and imprisonment." In addition, I certify that the provisions of the permit, including the development and implementation of a Storm Water Pollution Prevention Plan and a Sedimentation Program Plan, will be complied with.

OWNER SIGNATURE:	DATE:	FOR OFFICE USE ONLY	
MAIL COMPLETED FORM TO: (DO NOT SUBMIT ADDITIONAL DOCUMENTATION UNLESS REQUESTED)	ILLINOIS ENVIRONMENTAL PROTECTION AGENCY DIVISION OF WATER POLLUTION CONTROL ATTN: PERMIT SECTION 2000 CHURCHILL ROAD FIRST OFFICE BLDG. 15275 SPRINGFIELD, IL 62794-9278	LOG	PERMIT
		DATE:	

This Agency is authorized to require this information under Illinois Revised Statute, 1991, Chapter 111 1/2, section 1039. Information is required under that Section. Failure to do so may prevent this form from being processed and could result in your application being denied. This form has been approved by the Forms Management Center.

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## REVISIONS

NUMBER	BY	DATE

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THIS BAR IS EQUAL TO 2"  
AT FULL SCALE (34X22).

DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 REHABILITATE TAXIWAYS ALPHA AND CHARLIE  
 STORM WATER POLLUTION  
 PREVENTION PLAN  
 NOTES

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APPROVED BY: DLP

DATE: 06/09/06

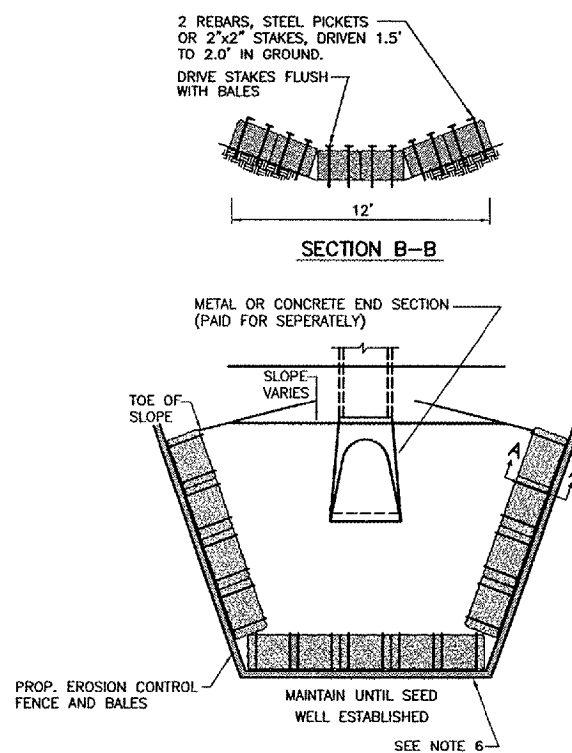
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ILLINOIS PROJECT: DPA-3454  
 A.I.P. PROJECT: 3-17-0017-B19

SHEET 7 OF 20 SHEETS

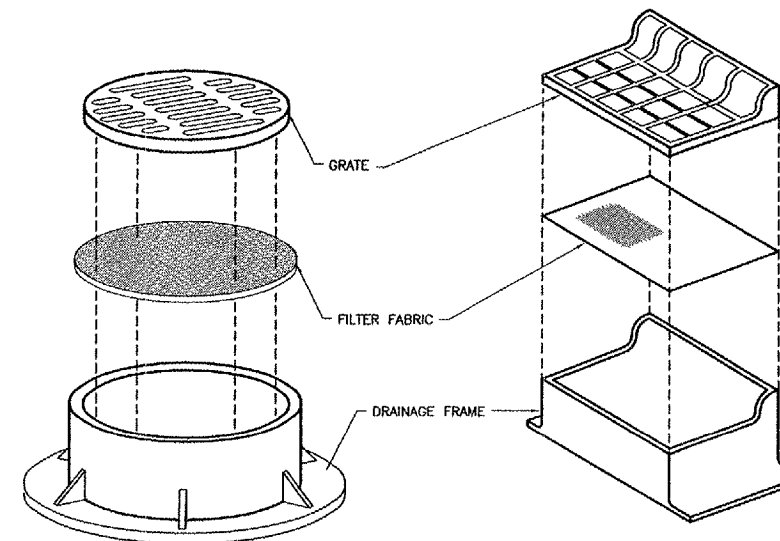
**NOTES**

- BALES SHALL BE PLACED AT THE TOE OF SLOPE OR ON A CONTOUR AND IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.
- EACH BALE SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 4 INCHES, AND PLACED SO THE BINDINGS ARE HORIZONTAL.
- BALES SHALL BE SECURELY ANCHORED IN PLACE BY EITHER TWO STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST STAKE IN EACH BALE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE AT AN ANGLE TO FORCE THE BALES TOGETHER. STAKES SHALL BE DRIVEN FLUSH WITH THE BALE.
- INSPECTION SHALL BE FREQUENT AND REPAIR / REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
- BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. COST OF REMOVAL / REPLACEMENT TO BE INCLUDED IN UNIT PRICE FOR INLET PROTECTION.
- AFTER FINAL APPROVAL OF THE ENGINEER, STRAW BALES MAY BE REMOVED. CONTRACTOR SHALL PLACE SOD, EXCELSIOR BLANKET WITH SEED OR KNITTED STRAW MAT WITH SEED OVER THE DISTURBED AREAS. COST INCIDENTAL TO INLET PROTECTION.



**INLET PROTECTION (END SECTION)**

N.T.S.



**NOTES:**

- FILTER WRAP TO BE PLACED IN ALL SLOPE BOXES, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
- FABRIC SHALL BE IN CONFORMANCE WITH MATERIALS SPECIFIED FOR FABRIC FENCE.
- FABRIC SHALL OVERLAY FRAME BY 2-INCH (MINIMUM).
- CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
- FABRIC SHALL REMAIN IN PLACE UNTIL TURFED AREAS HAVE DEVELOPED A MINIMUM OF 80% OF COVERAGE.
- COST OF FILTER WRAP SHALL BE CONSIDERED INCIDENTAL TO INLET PROTECTION.

**DRAINAGE STRUCTURE FILTER WRAP**

N.T.S.

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UPDATE BY: johse  
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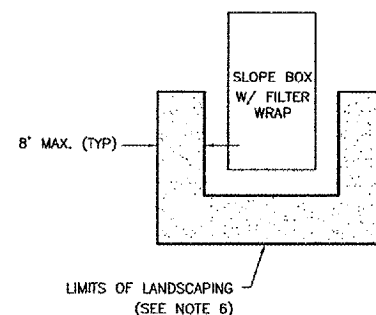
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AT FULL SCALE (34X22).

DUPAGE AIRPORT  
WEST CHICAGO, ILLINOIS  
REHABILITATE TAXIWAYS ALPHA AND CHARLIE

STORM WATER POLLUTION  
PREVENTION PLAN  
DETAILS

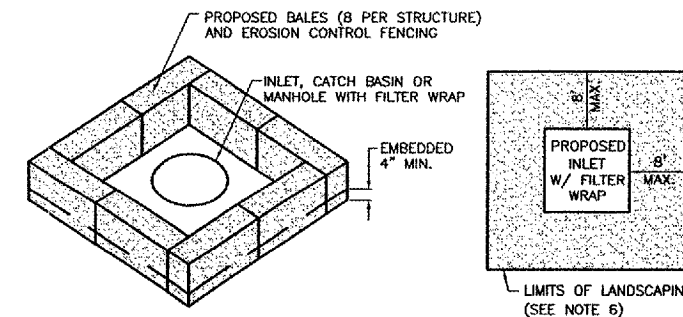
**NOTES**

- BALES SHALL BE PLACED AT THE TOE OF SLOPE OR ON A CONTOUR AND IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.
- EACH BALE SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 4 INCHES, AND PLACED SO THE BINDINGS ARE HORIZONTAL.
- BALES SHALL BE SECURELY ANCHORED IN PLACE BY EITHER TWO STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST STAKE IN EACH BALE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE AT AN ANGLE TO FORCE THE BALES TOGETHER. STAKES SHALL BE DRIVEN FLUSH WITH THE BALE.
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- BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. COST OF REMOVAL / REPLACEMENT TO BE INCLUDED IN UNIT PRICE FOR INLET PROTECTION.
- AFTER FINAL APPROVAL OF THE ENGINEER, STRAW BALES MAY BE REMOVED. CONTRACTOR SHALL PLACE SOD, EXCELSIOR BLANKET WITH SEED OR KNITTED STRAW MAT WITH SEED OVER THE DISTURBED AREAS. COST INCIDENTAL TO INLET PROTECTION.



**INLET PROTECTION (SLOPE BOX)**

N.T.S.

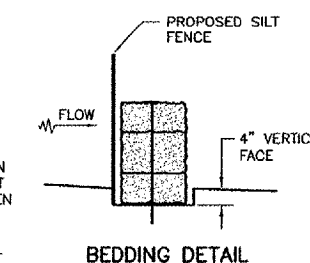


**INLET PLACEMENT**

**INLET PLACEMENT**

**NOTES**

- BALES SHALL BE PLACED AT THE TOE OF SLOPE OR ON A CONTOUR AND IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.
- EACH BALE SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 4 INCHES, AND PLACED SO THE BINDINGS ARE HORIZONTAL.
- BALES SHALL BE SECURELY ANCHORED IN PLACE BY EITHER TWO STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST STAKE IN EACH BALE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE AT AN ANGLE TO FORCE THE BALES TOGETHER. STAKES SHALL BE DRIVEN FLUSH WITH THE BALE.
- INSPECTION SHALL BE FREQUENT AND REPAIR / REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
- BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. COST OF REMOVAL / REPLACEMENT TO BE INCLUDED IN UNIT PRICE FOR INLET PROTECTION.
- AFTER FINAL APPROVAL OF THE ENGINEER, STRAW BALES MAY BE REMOVED. CONTRACTOR SHALL PLACE SOD AND MULCH OVER THE DISTURBED AREAS. COST INCIDENTAL TO INLET PROTECTION.



**BEDDING DETAIL**

**INLET PROTECTION (INLET/MANHOLES)**

N.T.S.

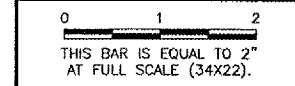
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**DDMI**  
Dupage Airport

DESIGN BY:	JRL
DRAWN BY:	JRO
CHECKED BY:	
APPROVED BY:	
DATE:	06/09/06
JOB No:	04257-05
ILLINOIS PROJECT:	DPA-3454
A.I.P. PROJECT:	3-17-0017-B19
SHEET	8 OF 20 SHEETS



REVISIONS		
NUMBER	BY	DATE

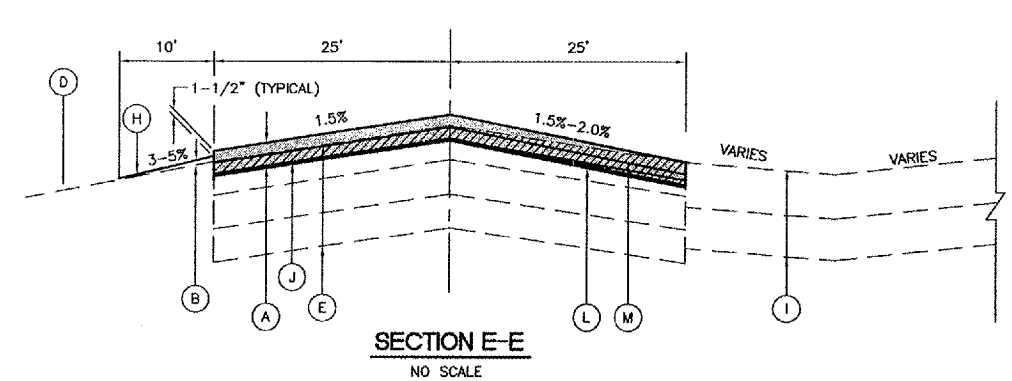
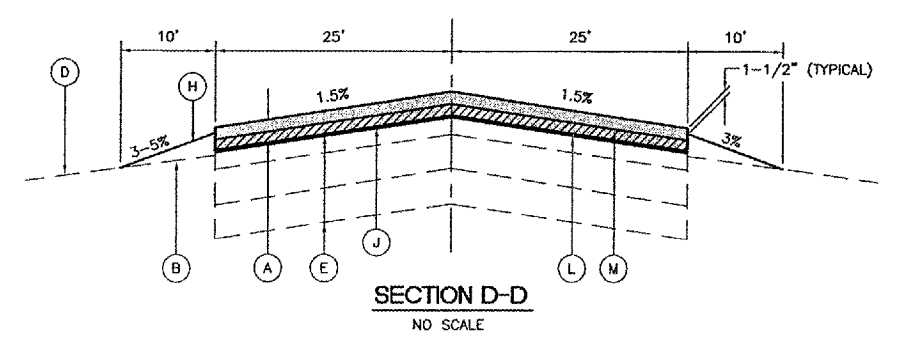
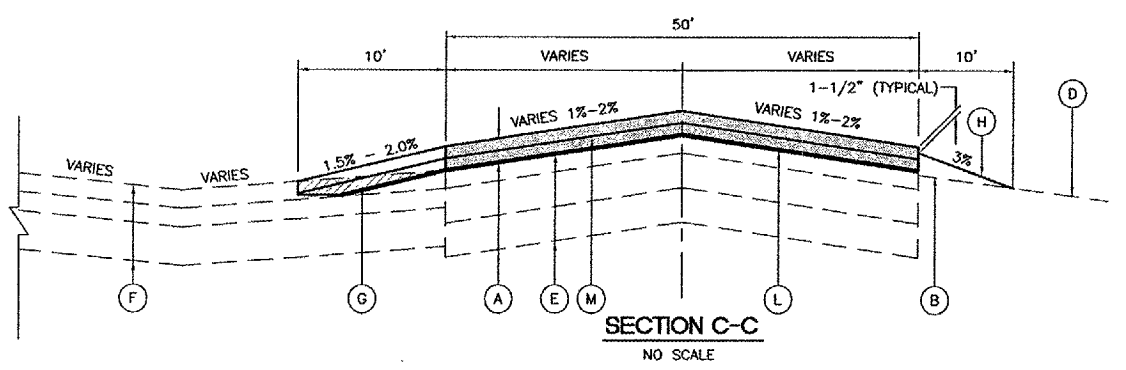
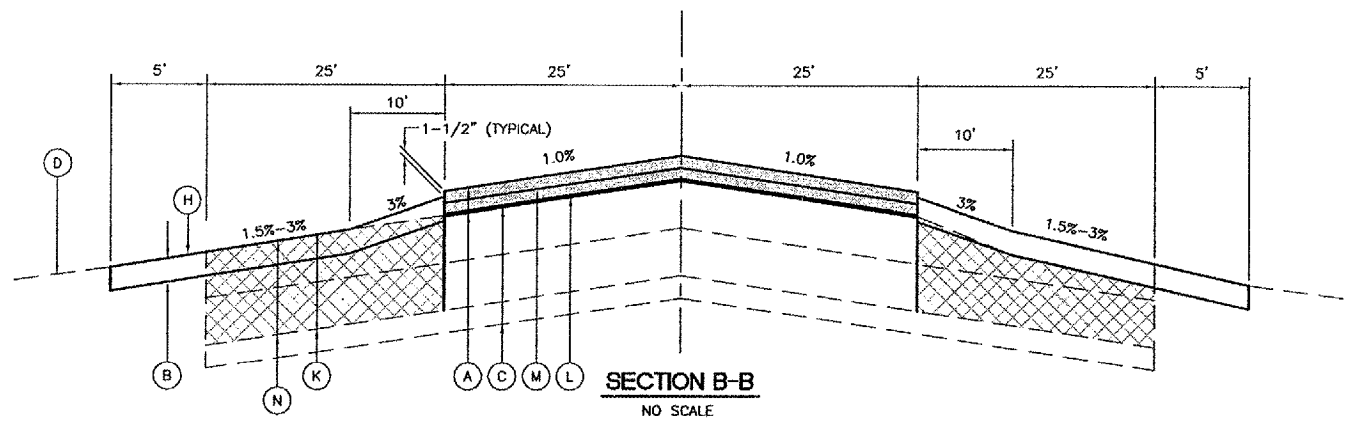
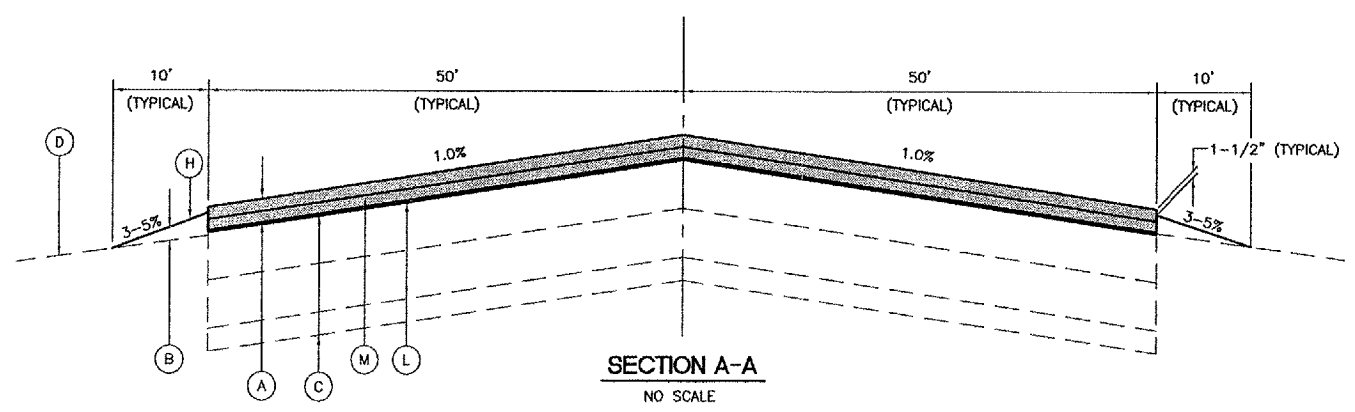


**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 REHABILITATE TAXIWAYS ALPHA AND CHARLIE  
 TYPICAL SECTIONS**

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DESIGN BY:	JRL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DLP
DATE:	06/09/06
JOB No:	04257-05

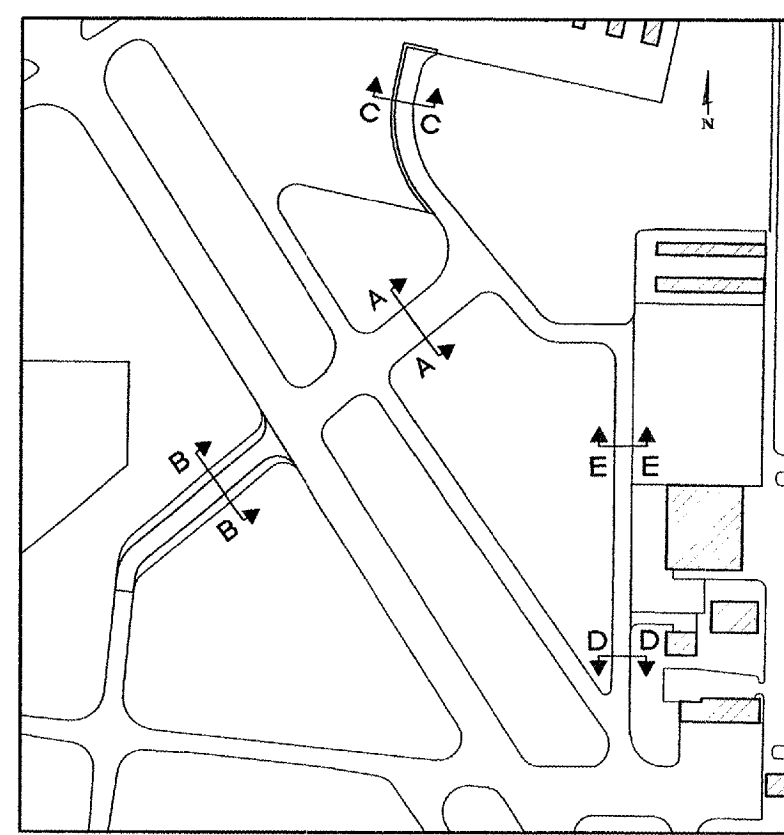
ILLINOIS PROJECT: DPA-3454  
 A.I.P. PROJECT: 3-17-0017-B19



**LEGEND**

- (A) NEW 2" BITUMINOUS SURFACE COURSE OVERLAY (401)  
NEW 2" AVERAGE BITUMINOUS SURFACE LEVELING COURSE (401)
- (B) NEW TOPSOIL PLACEMENT (FROM OFF-SITE)(905)
- (C) EXISTING TAXIWAY C PAVEMENT  
6" BITUMINOUS PAVEMENT  
6" P.C.C. PAVEMENT  
AGGREGATE BASE COURSE
- (D) EXISTING GROUNDLINE
- (E) EXISTING TAXIWAY A PAVEMENT  
3" BITUMINOUS PAVEMENT  
6" PCC PAVEMENT  
AGGREGATE BASE COURSE
- (F) EXISTING BITUMINOUS APRON PAVEMENT
- (G) NEW BUTT JOINT CONSTRUCTION (401)
- (H) NEW SEED (901) AND SOIL GUARD (800)
- (I) EXISTING CONCRETE APRON
- (J) VARIABLE DEPTH (0"-3") BITUMINOUS PAVEMENT MILLING (401)
- (K) NEW FULL DEPTH PAVEMENT REMOVAL  
(PAID FOR AS AR401900 REMOVE BITUMINOUS PAVEMENT)
- (L) CRACK CONTROL FABRIC (201)
- (M) BITUMINOUS TACK COAT (603)
- (N) NEW SHOULDER FILL (800)

**KEY MAP**



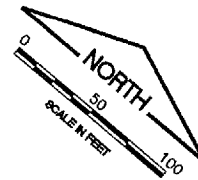
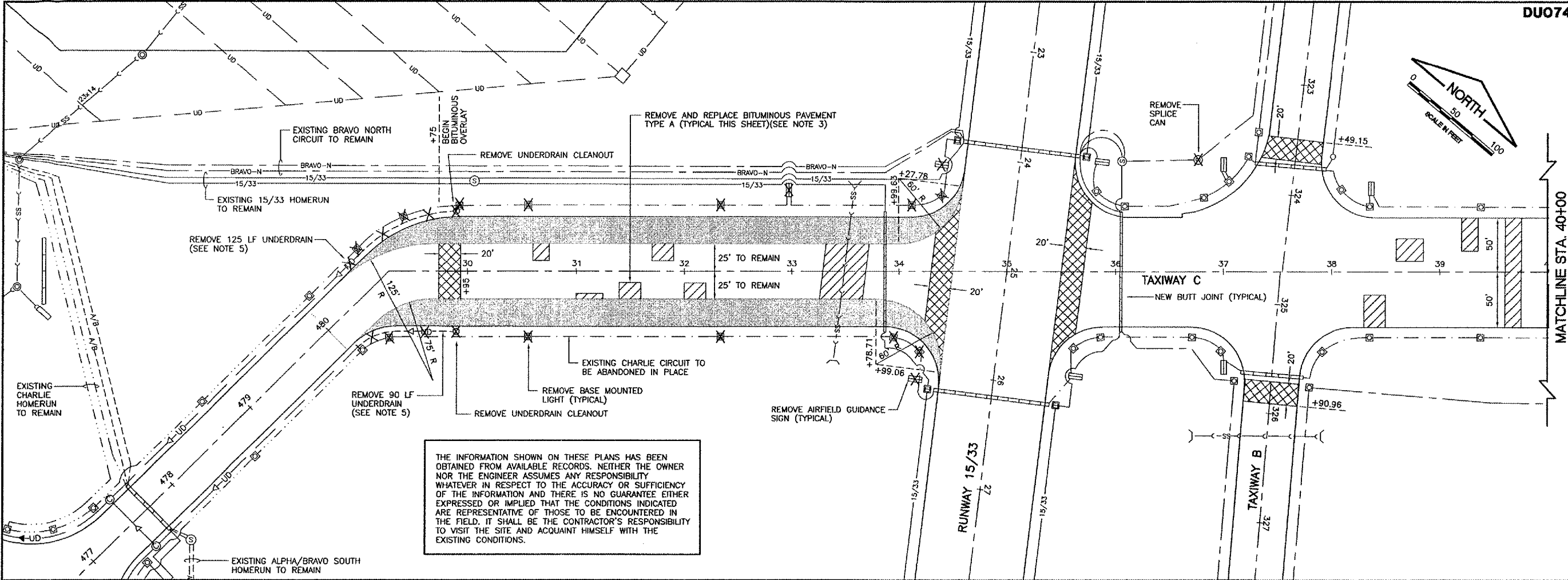


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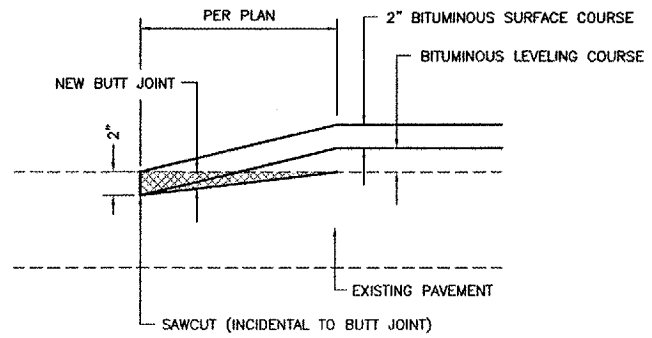
THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY...

NOTES

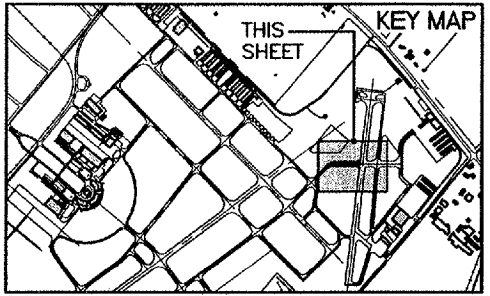
- 1. FOR ALL PAVEMENT REMOVAL ITEMS THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND THE PERIMETER OF THE REMOVAL LIMITS.
2. THE TAXIWAY LIGHTS AND TRANSFORMERS TO BE REMOVED SHALL BE TURNED OVER TO THE AIRPORT.
3. PAVEMENT REMOVAL AND REPLACEMENT LIMITS SHOWN ARE APPROXIMATE.
4. ALL LIGHTING CIRCUITS SHALL BE OPERATIONAL AT THE END OF EACH WORKING DAY.
5. CAP END OF UNDERDRAIN TO REMAIN.

LEGEND

- REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A
REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B
NEW BUTT JOINT CONSTRUCTION
REMOVE BITUMINOUS PAVEMENT
PAVEMENT MILLING
EXISTING BASE MOUNTED TAXI LIGHT
EXISTING BASE MOUNTED RUNWAY LIGHT
EXISTING STAKE MOUNTED TAXI LIGHT
EXISTING AIRFIELD GUIDANCE SIGN
EXISTING DUCT BANK
EXISTING SPLICE CAN
EXISTING 15/33 LIGHTING CIRCUIT
EXISTING BRAVO NORTH LIGHTING CIRCUIT
EXISTING CHARLIE LIGHTING CIRCUIT
EXISTING ALPHA/BRAVO SOUTH CIRCUIT
EXISTING STORM SEWER
EXISTING HEADWALL
EXISTING UNDERDRAIN
EXISTING UNDERDRAIN CLEANOUT
ITEM TO BE REMOVED



BUTT JOINT DETAIL
NOT TO SCALE



DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
REHABILITATE TAXIWAYS ALPHA AND CHARLIE
EXISTING CONDITIONS/PROPOSED REMOVALS - TAXIWAY C

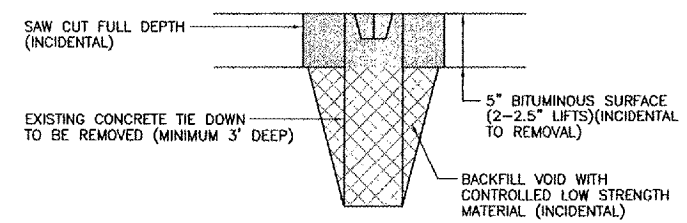
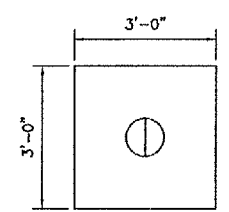
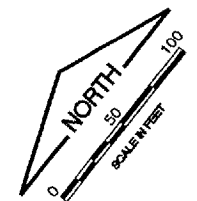
CMT CONSULTING ENGINEERS logo and contact information.

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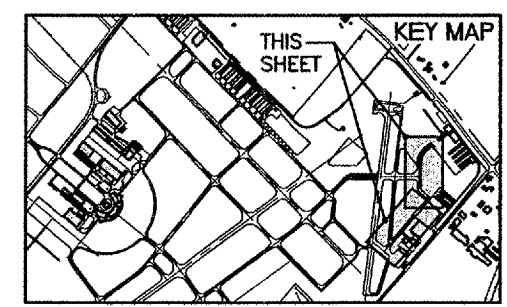
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UPDATE BY: johse  
SURVEY BOOK #  
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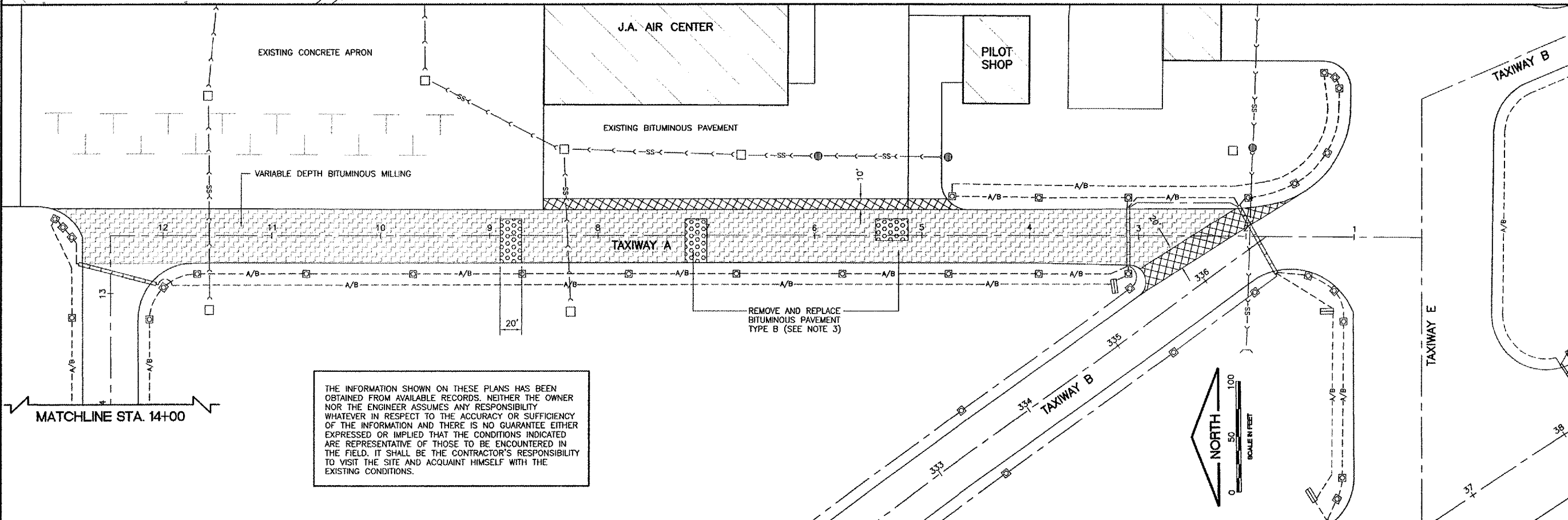
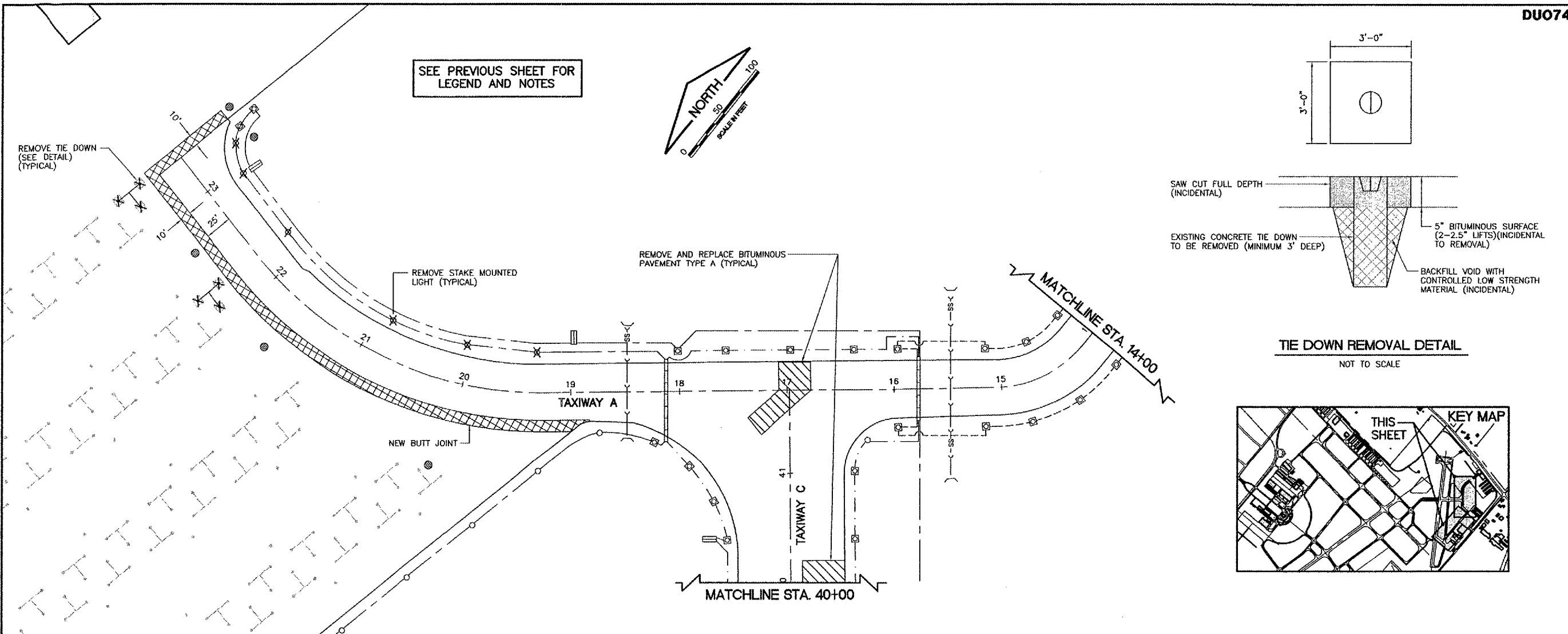
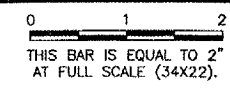
SEE PREVIOUS SHEET FOR  
LEGEND AND NOTES



TIE DOWN REMOVAL DETAIL  
NOT TO SCALE



REVISIONS		
NUMBER	BY	DATE



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**DUPAGE AIRPORT  
WEST CHICAGO, ILLINOIS  
REHABILITATE TAXIWAYS ALPHA AND CHARLIE  
EXISTING CONDITIONS/PROPOSED  
REMOVALS - TAXIWAY A**

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



**DPA**  
DuPage Airport

DESIGN BY:	JRL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DLP
DATE:	06/09/06
JOB No:	04257-05
ILLINOIS PROJECT: DPA-3454 A.I.P. PROJECT: 3-17-0017-B19	
SHEET 11 OF 20 SHEETS	

DU074

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UPDATE BY: johse  
SURVEY BOOK #  
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txybase.dwg

**LEGEND**

-  NEW ELEVATIONS (5.29)
-  EXISTING ELEVATION (5.07)
-  EXISTING STORM STRUCTURE
-  EXISTING CONTOUR

**NOTES**

1. ADD 750 TO ELEVATION SHOWN FOR U.S.G.S. DATUM.



**REVISIONS**

NUMBER	BY	DATE



**DUPAGE AIRPORT  
WEST CHICAGO, ILLINOIS  
REHABILITATE TAXIWAYS ALPHA AND CHARLIE**

**GRADING PLAN - TAXIWAY C**

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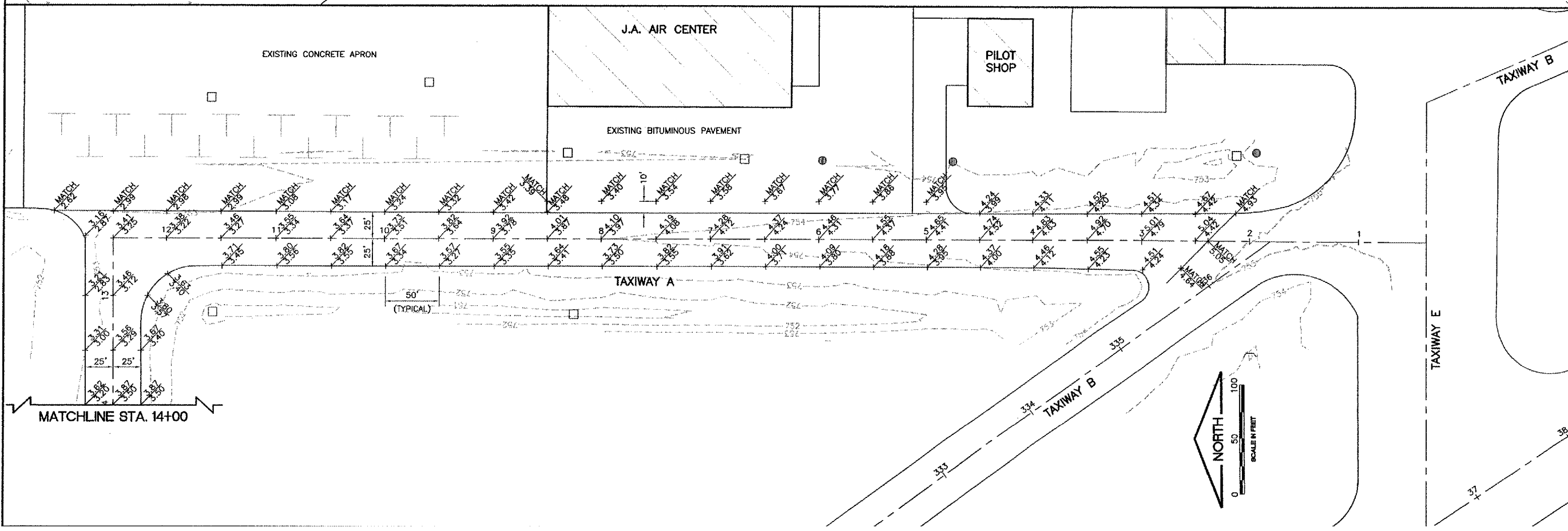
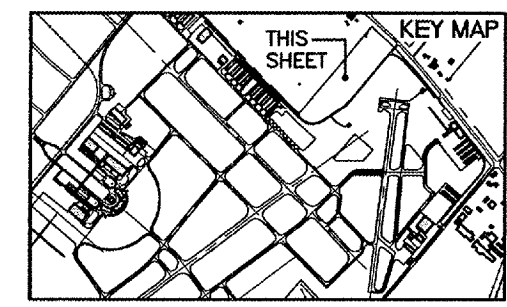
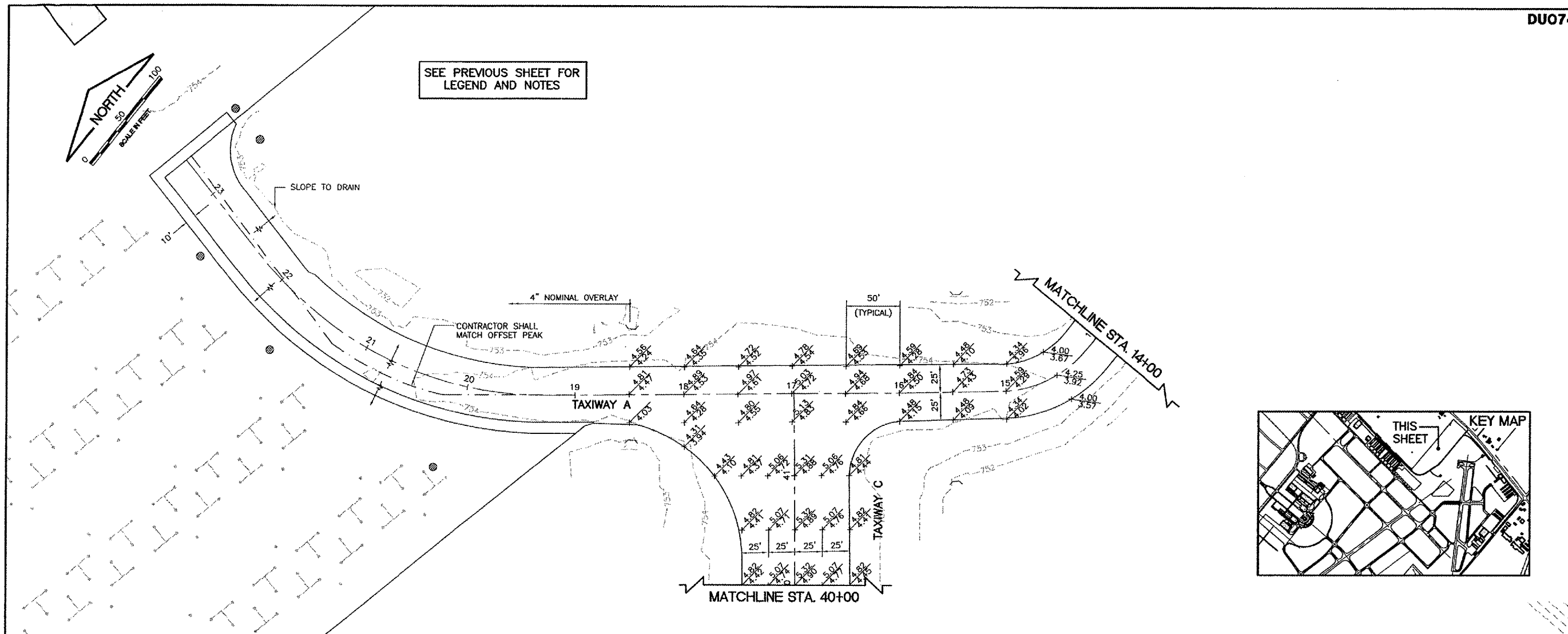
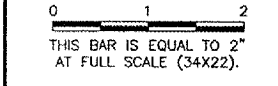
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CHECKED BY: CAL  
APPROVED BY: DLP  
DATE: 06/09/06  
JOB No: 04257-05

ILLINOIS PROJECT: DPA-3454  
A.I.P. PROJECT: 3-17-0017-B19

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 UPDATE BY: johse  
 SURVEY BOOK #  
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 tb.dwg  
 txybase.dwg

SEE PREVIOUS SHEET FOR  
 LEGEND AND NOTES

REVISIONS		
NUMBER	BY	DATE



**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 REHABILITATE TAXIWAYS ALPHA AND CHARLIE  
 GRADING PLAN - TAXIWAY A**

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**DDMI**  
 DuPage Airport

DESIGN BY:	JRL
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DLP
DATE:	06/09/06
JOB No:	04257-05
ILLINOIS PROJECT: DPA-3454 A.I.P. PROJECT: 3-17-0017-B19	
SHEET 13 OF 20 SHEETS	



**TAXIWAY C**

STATION	50 LT. CL			25 LT. CL			CENTERLINE			25 RT. CL			50 RT. CL		
	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH
29+75				4.86	4.89*	0.17	4.95	4.78*	0.17	4.75	4.58*	0.17			
30+00				5.06	4.81	0.25	5.31	4.95	0.36	5.06	4.85	0.21			
30+50				5.11	4.82	0.29	5.36	5.02	0.34	5.11	4.84	0.27			
31+00				5.16	4.84	0.32	5.41	5.08	0.33	5.16	4.87	0.29			
31+50				5.20	4.84	0.36	5.45	5.10	0.35	5.20	4.87	0.33			
32+00				5.18	4.83	0.35	5.43	5.05	0.38	5.18	4.85	0.33			
32+50				5.14	4.80	0.34	5.39	5.01	0.38	5.14	4.85	0.29			
33+00				5.12	4.82	0.30	5.37	5.00	0.37	5.12	4.90	0.22			
33+50				5.16	4.83	0.33	5.41	5.00	0.41	5.16	4.92	0.24			
34+00				5.21	4.98	0.23	5.46	5.10	0.36	5.29	5.07	0.22			
34+50				5.47	5.30*	0.17	5.55	5.38*	0.17	5.51	5.34*	0.17			
AVG. OVERLAY THICK						0.28	0.33						0.25		
AVG. LEVELING THICK						0.12	0.16						0.08		

STATION	50 LT. CL			25 LT. CL			CENTERLINE			25 RT. CL			50 RT. CL		
	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH
35+50	5.47	5.30*	0.17	5.47	5.30*	0.17	5.48	5.31*	0.17	5.36	5.20*	0.16	5.28	5.11*	0.17
36+00	4.98	4.82	0.16	5.23	5.04	0.19	5.48	5.22	0.26	5.23	4.93	0.30	4.98	4.73	0.25
36+50	4.98	4.67	0.31	5.23	5.00	0.23	5.48	5.17	0.31	5.23	4.99	0.24	4.98	4.67	0.31
37+00	4.87	4.53	0.34	5.12	4.73	0.39	5.37	4.97	0.40	5.12	4.78	0.34	4.87	4.62	0.25
37+50	4.73	4.52	0.21	5.04	4.81	0.23	5.35	4.94	0.41	5.20	4.77	0.43	5.05	4.59	0.46
38+00	4.85	4.36	0.49	5.10	4.81	0.29	5.35	4.91	0.44	5.10	4.84	0.26	4.85	4.62	0.23
38+50	4.84	4.44	0.40	5.09	4.78	0.31	5.34	4.90	0.44	5.09	4.82	0.27	4.84	4.55	0.29
39+00	4.84	4.44	0.40	5.09	4.78	0.31	5.34	4.91	0.43	5.09	4.81	0.28	4.84	4.49	0.35
39+50	4.83	4.45	0.38	5.08	4.77	0.31	5.33	4.91	0.42	5.08	4.78	0.30	4.83	4.48	0.35
40+00	4.82	4.42	0.40	5.07	4.74	0.33	5.32	4.90	0.42	5.07	4.77	0.30	4.82	4.45	0.37
40+50	4.82	4.41	0.41	5.07	4.71	0.36	5.32	4.89	0.43	5.07	4.76	0.31	4.82	4.41	0.41
41+00	4.81	4.39	0.42	5.06	4.72	0.34	5.31	4.88	0.43	5.06	4.76	0.30	4.81	4.44	0.37
AVG. OVERLAY THICK						0.34	0.38						0.29		
AVG. LEVELING THICK						0.17	0.21						0.12		

**TAXIWAY A**

STATION	35 LT. CL			25 LT. CL			CENTERLINE			25 RT. CL			35 RT. CL		
	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH
02+25							5.05	4.88*	0.17	4.65	4.48*	0.17			
02+50							5.04	4.71*	0.33	4.67	4.34*	0.33			
03+00				4.51	4.18*	0.33	5.01	4.68*	0.33	4.51	4.18*	0.33			
03+50				4.55	4.22*	0.33	4.92	4.59*	0.33	4.52	4.19*	0.33			
04+00				4.46	4.13*	0.33	4.83	4.50*	0.33	4.33	4.00*	0.33			
04+50				4.37	4.04*	0.33	4.74	4.41*	0.33	4.24	3.91*	0.33			
05+00				4.28	3.95*	0.33	4.65	4.32*	0.33	4.15	3.82*	0.33	3.95	3.78*	0.17
05+50				4.18	3.85*	0.33	4.55	4.22*	0.33	4.05	3.72*	0.33	3.86	3.69*	0.17
06+00				4.09	3.76*	0.33	4.46	4.13*	0.33	3.96	3.63*	0.33	3.77	3.60*	0.17
06+50				4.00	3.67*	0.33	4.37	4.04*	0.33	3.87	3.54*	0.33	3.67	3.50*	0.17
07+00				3.91	3.58*	0.33	4.28	3.95*	0.33	3.78	3.45*	0.33	3.58	3.41*	0.17
07+50				3.82	3.49*	0.33	4.19	3.86*	0.33	3.69	3.36*	0.33	3.54	3.37*	0.17
08+00				3.73	3.40*	0.33	4.10	3.77*	0.33	3.60	3.27*	0.33	3.40	3.23*	0.17
08+50				3.64	3.31*	0.33	4.01	3.68*	0.33	3.48	3.15*	0.33	3.39	3.22*	0.17
09+00				3.55	3.22*	0.33	3.92	3.59*	0.33	3.42	3.17*	0.25			
09+50				3.57	3.24*	0.33	3.82	3.49*	0.33	3.32	3.07*	0.25			
10+00				3.67	3.34*	0.33	3.73	3.40*	0.33	3.24	2.99*	0.25			
10+50				3.82	3.49*	0.33	3.64	3.31*	0.33	3.17	2.92*	0.25			
11+00				3.80	3.47*	0.33	3.55	3.22*	0.33	3.08	2.83*	0.25			
11+50				3.71	3.38*	0.33	3.46	3.13*	0.33	2.99	2.74*	0.25			
12+00				3.63	3.30*	0.33	3.38	3.05*	0.33	2.98	2.73*	0.25			
12+50				3.44	3.11*	0.33	3.41	3.08*	0.33	2.99	2.74*	0.25			
AVG. OVERLAY THICK						0.33	0.32						0.29		
AVG. LEVELING THICK						0.16	0.16						0.13		

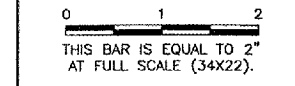
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	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH	PROP.	EXIST	DEPTH
13+00				3.80	3.57	0.23	3.46	3.12	0.34	3.21	2.83	0.38			
13+50				3.67	3.40	0.27	3.56	3.29	0.27	3.31	3.00	0.31			
14+00				3.87	3.50	0.37	3.87	3.50	0.37	3.62	3.20	0.42			
14+50				4.00	3.57	0.43	4.25	3.92	0.33	4.00	3.67	0.33			
15+00				4.34	4.02	0.32	4.59	4.29	0.30	4.34	3.96	0.38			
15+50				4.48	4.09	0.39	4.73	4.43	0.30	4.48	4.10	0.38			
16+00				4.48	4.15	0.33	4.84	4.50	0.34	4.59	4.28	0.31			
16+50				4.84	4.66	0.18	4.94	4.68	0.26	4.69	4.53	0.16			
17+00				5.13	4.83	0.30	5.03	4.82	0.21	4.78	4.54	0.24			
17+50				4.80	4.55	0.25	4.97	4.61	0.36	4.72	4.52	0.20			
18+00				4.64	4.28	0.36	4.89	4.53	0.36	4.64	4.35	0.29			
18+50				4.56	4.03	0.53	4.81	4.47	0.34	4.56	4.24	0.32			
AVG. OVERLAY THICK						0.33	0.32						0.31		
AVG. LEVELING THICK						0.16	0.15						0.14		

\* Denotes existing elevation after milling/butt joint has been constructed.

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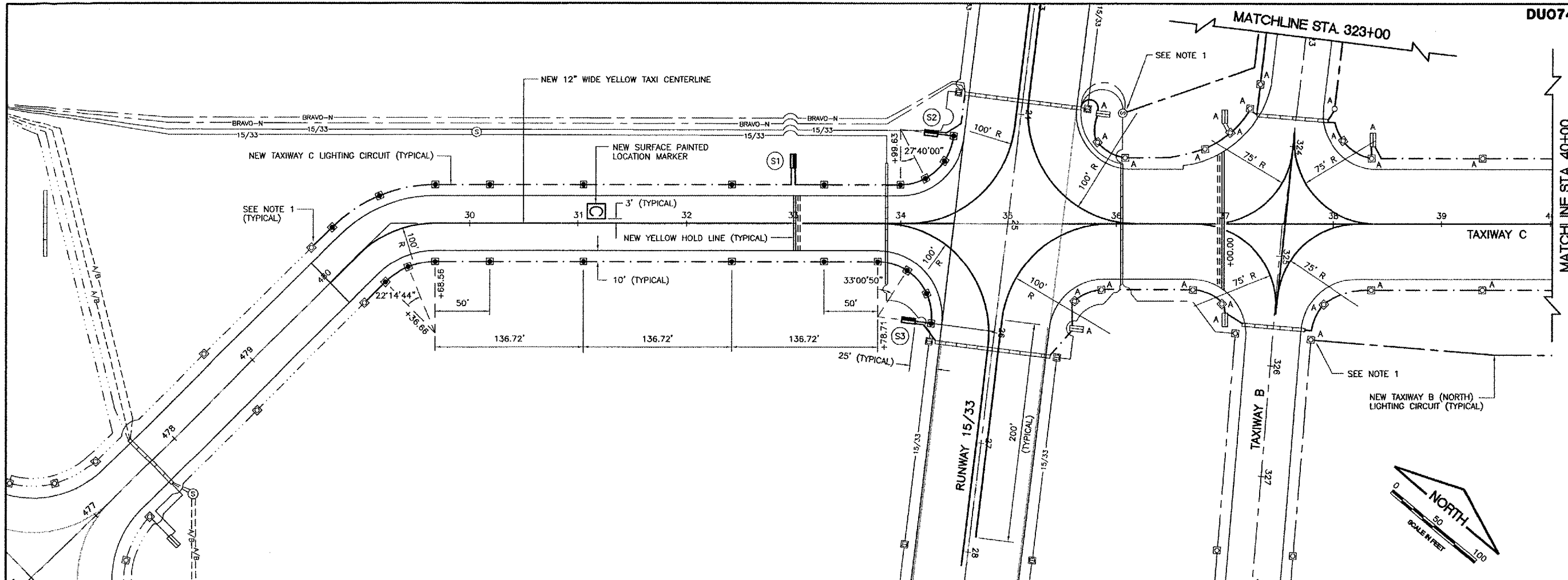


**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 REHABILITATE TAXIWAYS ALPHA AND CHARLIE  
 OVERLAY DEPTH CHART**

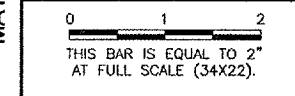
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**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 REHABILITATE TAXIWAYS ALPHA AND CHARLIE  
 PAVEMENT MARKING PLAN/LIGHTING  
 PLAN TAXIWAY C**

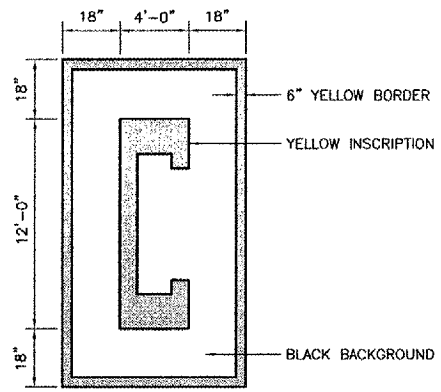
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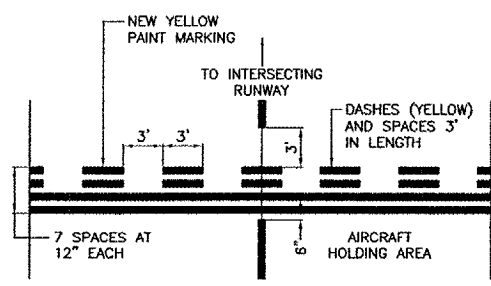
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ILLINOIS PROJECT: DPA-3454 A.I.P. PROJECT: 3-17-0017-B19	
SHEET 15 OF 20 SHEETS	

AIRFIELD SIGNAGE SCHEDULE				
NEW SIGN NUMBER	NEW SIGN FACE	NEW SIGN LEGEND	NEW SIGN TYPE	NEW SIGN LOCATION
S1	E W		1 2,1	STA. 33+00, 50' LT. CENTERLINE TAXIWAY C
S2	N S		3 0	STA. 24+27.78, 75' RT. CENTERLINE RUNWAY 15/33
S3	N S		0 3	STA. 25+99.06, 75' RT. CENTERLINE RUNWAY 15/33
S4	N S		3,1 1	STA. 22+90.81, 42.76' RT. CENTERLINE TAXIWAY A
S5	N S		1 0	STA. 18+96.74, 43.16' RT. CENTERLINE TAXIWAY A

- 0 — BLANK PANEL — BLACK
- 1 — RUNWAY/TAXIWAY HOLDLINE — WHITE LEGEND ON RED BACKGROUND
- 2 — LOCATION SIGN — YELLOW LEGEND ON BLACK BACKGROUND
- 3 — DIRECTION SIGN — BLACK LEGEND ON YELLOW BACKGROUND

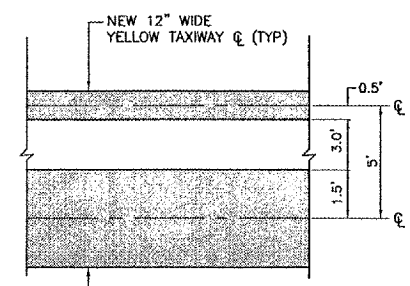


**SURFACE LOCATION MARKER DETAIL**  
 NOT TO SCALE



**RUNWAY HOLDING POSITION MARKER**  
 NO SCALE

NOTE: ALL HOLDING POSITION MARKINGS SHALL HAVE 6" WIDE BLACK BORDER



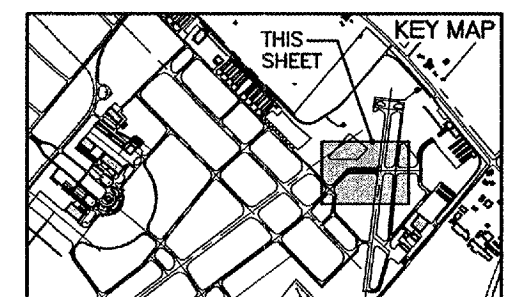
**TAXIWAY TANGENT DETAIL A**  
 NOT TO SCALE

**LIGHTING NOTES**

- CONNECT NEW CABLE TO EXISTING CABLE AT GUIDANCE SIGN, TAXIWAY LIGHT, RUNWAY LIGHT, MANHOLE OR SPLICE CAN. THE COST OF SPLICING SHALL BE INCIDENTAL TO AIRFIELD LIGHTING CABLE INSTALLATION.
- THE CONTRACTOR SHALL VERIFY THAT THE EXISTING RUNWAY/TAXIWAY LIGHTING CIRCUITS ARE OPERATIONAL AT THE END OF EACH WORKING DAY.
- THE ROUTING OF NEW AND EXISTING CABLE SHOWN IS FOR INFORMATION ONLY. THE EXACT ROUTING SHALL BE COORDINATED WITH THE ENGINEER.
- ANY EXISTING CABLE MARKERS THAT ARE DISTURBED ARE TO BE REMOVED AND REINSTALLED AT THE SAME LOCATION. COST SHALL BE INCIDENTAL TO GRADING.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS DIRECTED BY THE ENGINEER. ALL TEMPORARY CABLING AND SPLICING SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.
- AT ANY LOCATION WHERE THE NEW DUCT OR CABLE ROUTE CROSSES AN EXISTING UTILITY, THE CONTRACTOR SHALL HAND DIG AND LOCATE THE EXISTING UTILITY PRIOR TO TRENCHING. COST OF LOCATING ALL EXISTING UTILITIES SHALL BE INCIDENTAL TO THE CONTRACT.
- ALL SPLICES SHALL BE INSTALLED INSIDE HANDHOLE OR LIGHT BASES. NO DIRECT BURIED SPLICE SHALL BE ALLOWED.
- ALL AIRFIELD LIGHTING CIRCUIT CABLE SHALL BE 1/C #8, 5KV, L-824 TYPE C IN 3/4" UNIT DUCT (DIRECT BURIED).
- ALL LIGHTING CIRCUITS SHALL BE OPERATIONAL AT THE END OF EACH WORKING DAY. ANY TEMPORARY CABLING NECESSARY TO MAINTAIN THE CIRCUITS DURING CONSTRUCTION SHALL BE INCIDENTAL TO CONSTRUCTION.

**PAVEMENT MARKING NOTES**

- THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
- WHEN YELLOW TAXIWAY STRIPPING CROSSES ANY WHITE RUNWAY STRIPPING, THE TAXIWAY STRIPE SHALL BE GAPPED 6" EACH SIDE OF RUNWAY STRIPE EXCEPT RUNWAY/RUNWAY HOLDLINE.



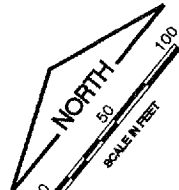
**LEGEND**

- A — EXISTING LIGHT/SIGN TO BE RETROFITTED
- O — EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING/NEW BASE MOUNTED TAXI LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING/NEW AIRFIELD GUIDANCE SIGN
- --- NEW TAXIWAY CHARLIE LIGHTING CIRCUIT
- --- NEW TAXIWAY BRAVO NORTH LIGHTING CIRCUIT
- A/B --- NEW ALPHA/BRAVO SOUTH LIGHTING CIRCUIT
- --- EXISTING DUCT BANK
- O S — EXISTING SPLICE CAN
- 15/33 --- EXISTING 15/33 LIGHTING CIRCUIT
- --- EXISTING BRAVO NORTH LIGHTING CIRCUIT
- --- EXISTING CHARLIE LIGHTING CIRCUIT
- A/B --- EXISTING ALPHA/BRAVO SOUTH CIRCUIT

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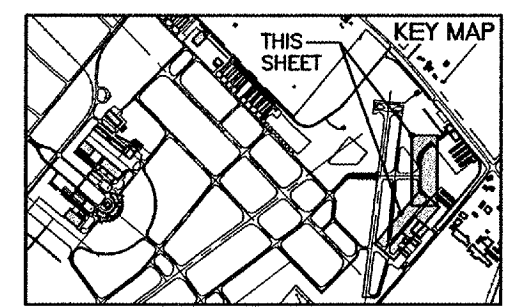
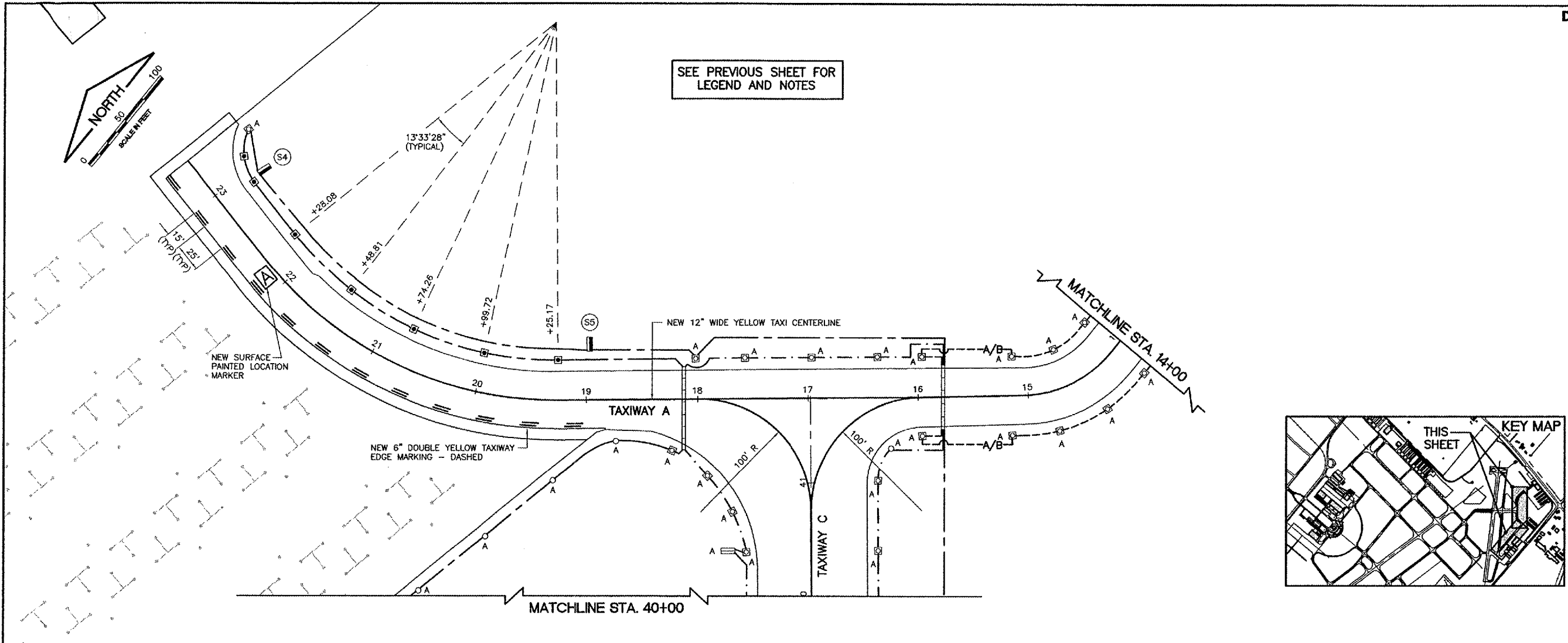
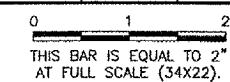
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SEE PREVIOUS SHEET FOR  
LEGEND AND NOTES

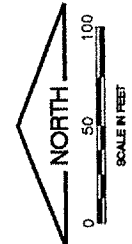
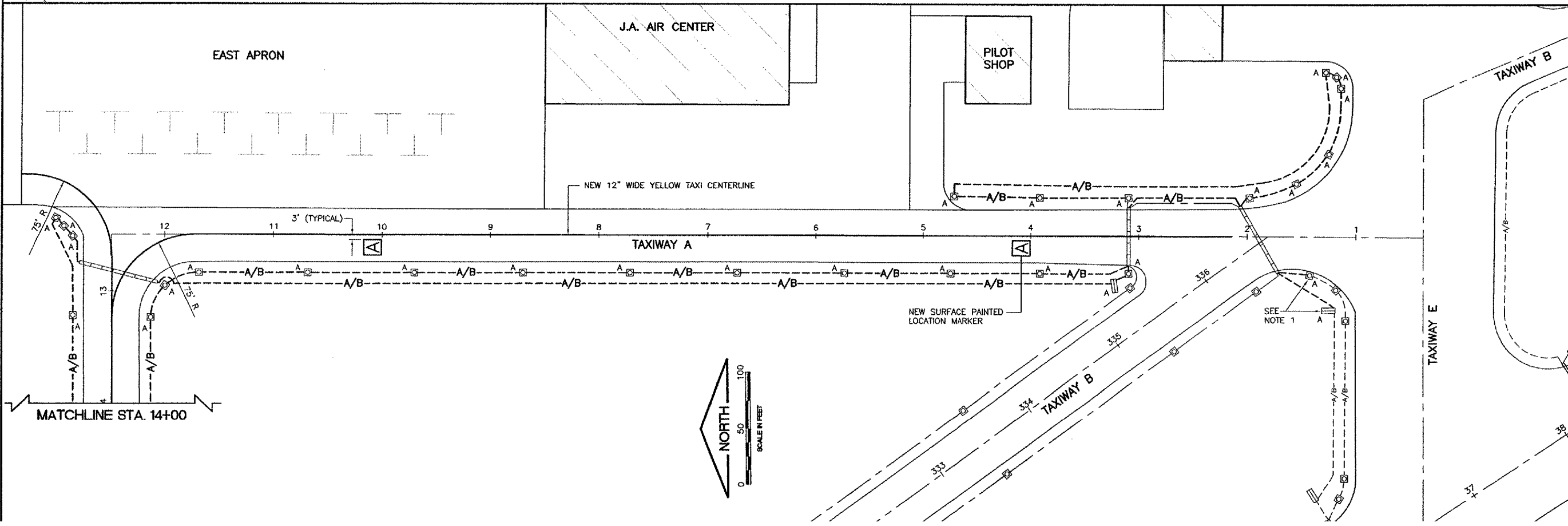


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DUPAGE AIRPORT  
WEST CHICAGO, ILLINOIS  
REHABILITATE TAXIWAYS ALPHA AND CHARLIE  
**PAVEMENT MARKING PLAN/LIGHTING  
PLAN - TAXIWAY A**

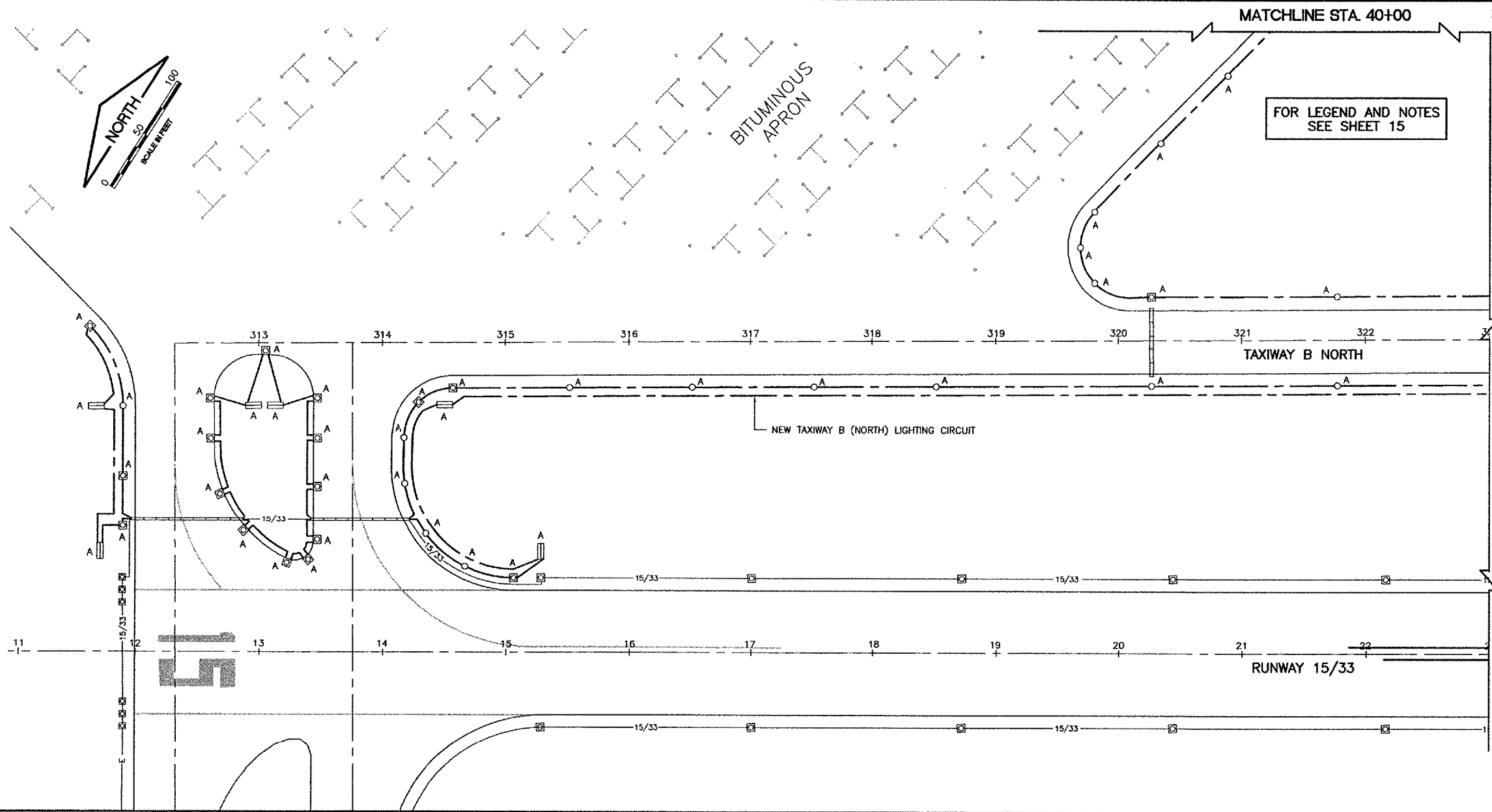


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FOR LEGEND AND NOTES  
SEE SHEET 15

DU074

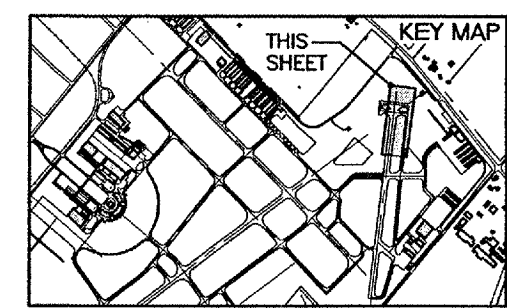
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0 1 2  
THIS BAR IS EQUAL TO 2"  
AT FULL SCALE (34X22).

DUPAGE AIRPORT  
WEST CHICAGO, ILLINOIS  
REHABILITATE TAXIWAYS ALPHA AND CHARLIE

LIGHTING PLAN TAXIWAY B NORTH

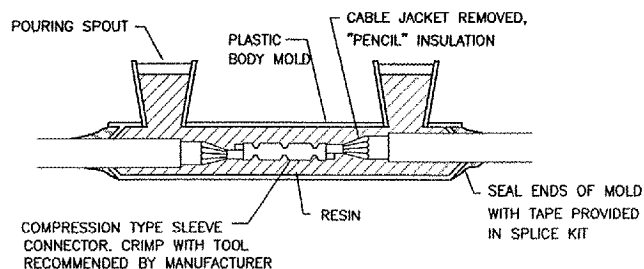


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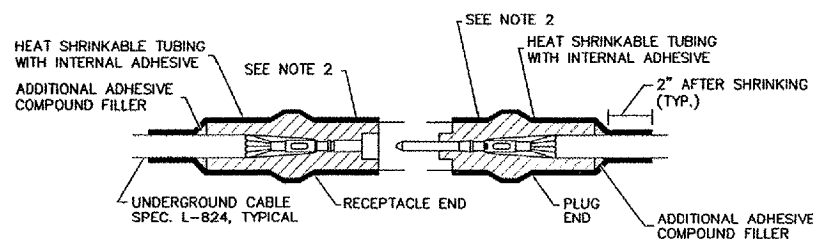
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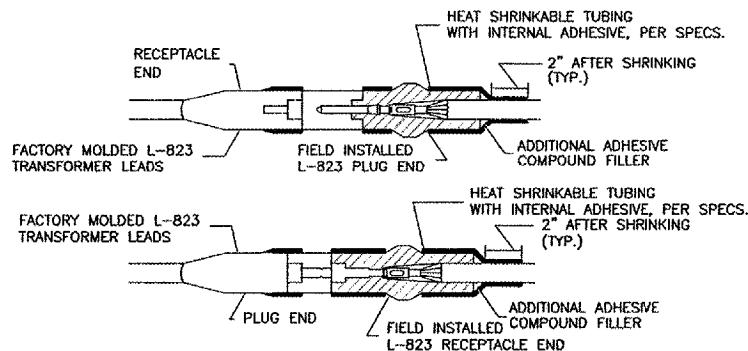
**TYPE A - CABLE SPLICE**

FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY  
 N.T.S.



**TYPE B - CABLE SPLICE**

FOR SPLICES FOR USE AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT  
 NOT TO SCALE

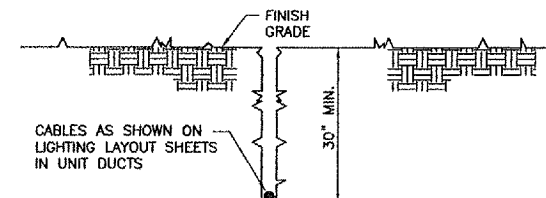


**TYPE C AND D - CABLE SPLICE**

FOR SPLICES AT RUNWAY/TAXIWAY LIGHTS AND SIGNS  
 NOT TO SCALE

**CABLE SPLICE NOTES**

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.



**CABLE IN UNIT-DUCT-PLOWED**

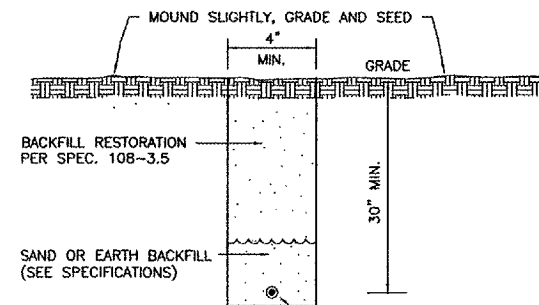
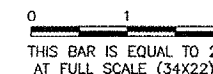
NO SCALE

**NOTE**

- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTERING INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
- DUCT MARKERS SHALL BE INSTALLED AT EVERY NEW DUCT AND AT EVERY EXISTING DUCT USED FOR THIS PROJECT.
- CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.

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**TRENCH DETAIL**

NO SCALE

**NOTE**

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3\"/>
- DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH RETURNING MATERIALS.

NOTE: AT CONTRACTOR'S OPTION, CABLE PLOWING MAY BE USED IN LIEU OF TRENCHING.

DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 REHABILITATE TAXIWAYS ALPHA AND CHARLIE

ELECTRICAL DETAILS  
 SHEET 1

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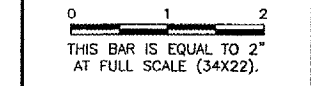
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DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 REHABILITATE TAXIWAYS ALPHA AND CHARLIE  
 ELECTRICAL DETAILS  
 SHEET 2

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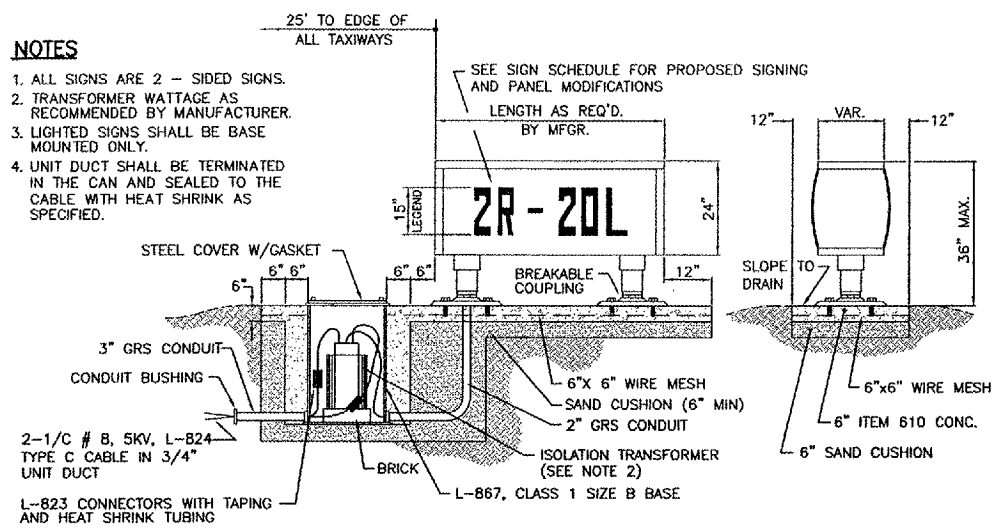
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JOB No:	04257-05

ILLINOIS PROJECT: DPA-3454  
 A.I.P. PROJECT: 3-17-0017-B19

**NOTES**

- ALL SIGNS ARE 2 - SIDED SIGNS.
- TRANSFORMER WATTAGE AS RECOMMENDED BY MANUFACTURER.
- LIGHTED SIGNS SHALL BE BASE MOUNTED ONLY.
- UNIT DUCT SHALL BE TERMINATED IN THE CAN AND SEALED TO THE CABLE WITH HEAT SHRINK AS SPECIFIED.

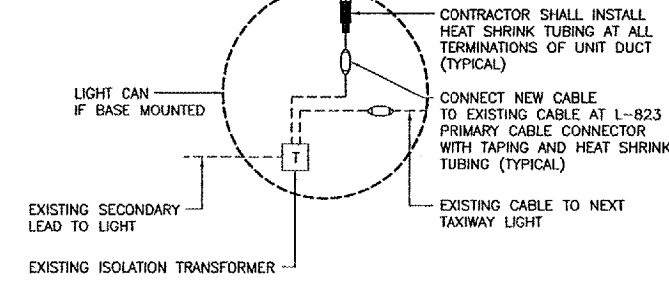


**HOLD LINE/TAXIWAY GUIDANCE SIGN L-858, SIZE 2, STYLE 2, CLASS 2**

NO SCALE

CONTRACTOR SHALL SUBMIT DETAILED SHOP DRAWING INCLUDING SIGN, COLOR, SIZE, PROPOSED LEGEND, IN ENOUGH DETAIL AND DETERMINE PROPOSED SPACING AND OTHER INFORMATION REQUIRED BY SPECIAL PROVISIONS. CONTRACTOR TO VERIFY PROPOSED SIGN LOCATIONS AND ORIENTATIONS WITH RESIDENT ENGINEER PRIOR TO INSTALLATION. SIGN MANUFACTURER SHALL MATCH EXISTING AIRFIELD SIGNS.

NEW 1/2" #8 L-824, TYPE C, 5KV AIRFIELD LIGHTING CABLE IN 3/4" UNIT DUCT OR GRS CONDUIT (TYP.) FROM LAST NEW TAXIWAY LIGHT

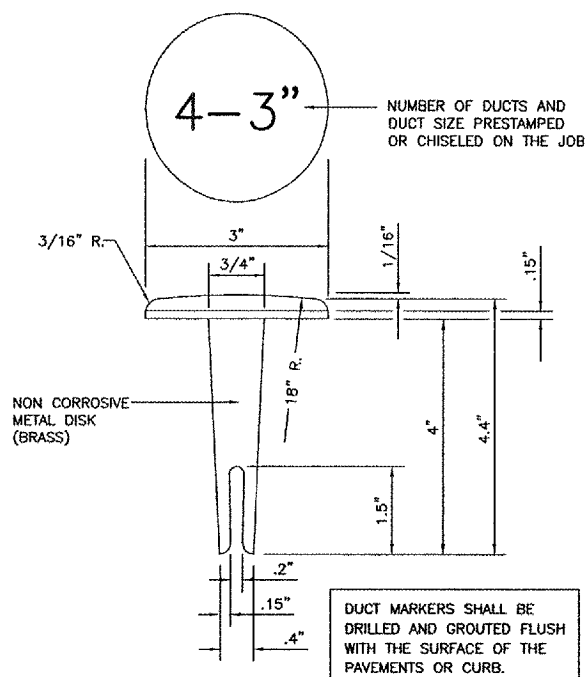


**RUNWAY/TAXIWAY LIGHTING CIRCUIT CONNECTION DETAIL**

NOT TO SCALE

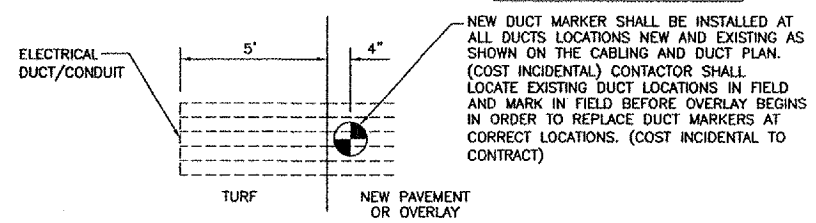
**GENERAL NOTES:**

- THE CONCRETE BASE FOR BASE MTD. LIGHTS AND SIGNS SHALL BE TROWEL FINISHED WITH A 45° BEVELED EDGE. SLOPE TO DRAIN (610).
- TRANSFORMER HOLDER SHALL BE ANY COMMERCIALY AVAILABLE BRICK.
- BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
- ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/2" No. 8, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
- ALL SIGNS, LIGHTS, CABLES AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT DIRECTOR, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE.
- CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.
- ALL RUNWAY/TAXIWAY EDGE LIGHTS SHALL HAVE 2" DIA. COLUMN AND FRANGIBLE COUPLINGS, UNLESS NOTED OTHERWISE.



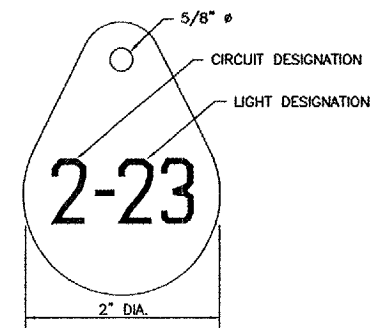
**DUCT/CONDUIT MARKER DETAIL**

NOT TO SCALE



**LIGHT IDENTIFICATION DETAIL**

NOT TO SCALE

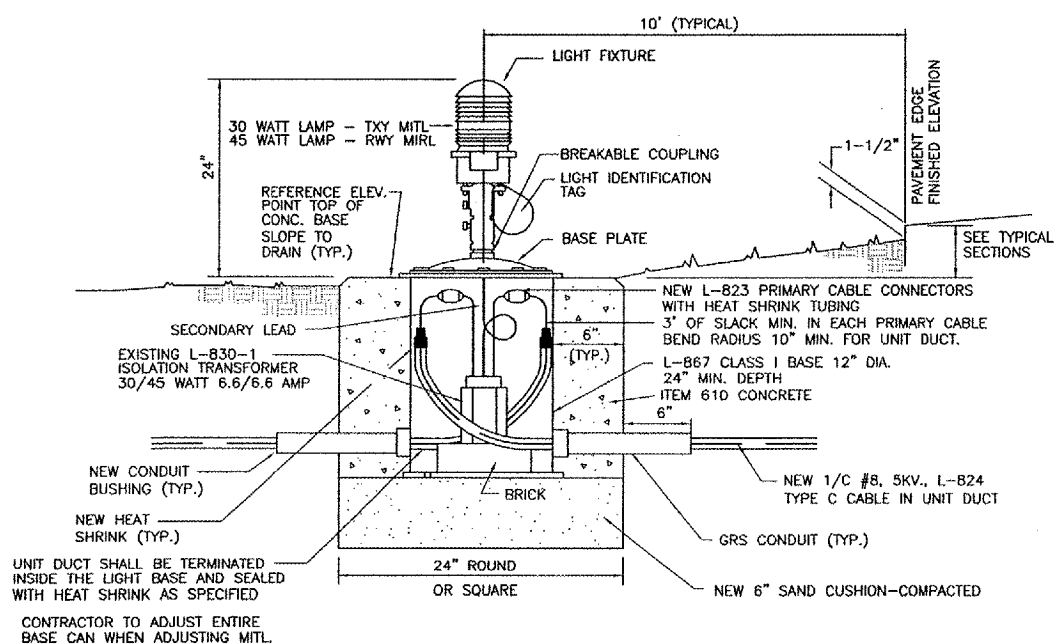
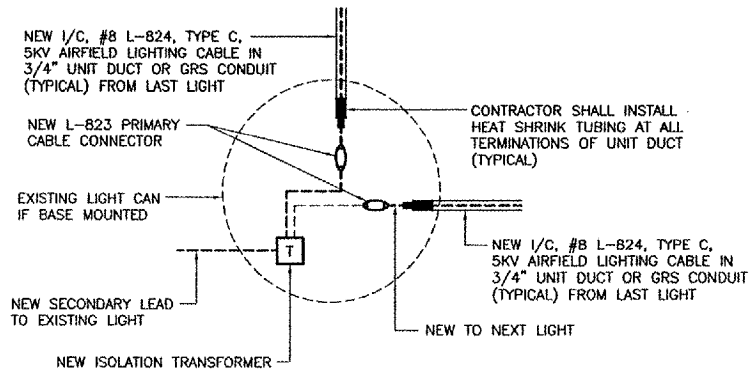


**NOTES:**

- INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
- NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND NEW TAXIWAY LIGHTS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (NEW OR RELOCATED LIGHTS) SHALL BE RETAGGED.
- COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

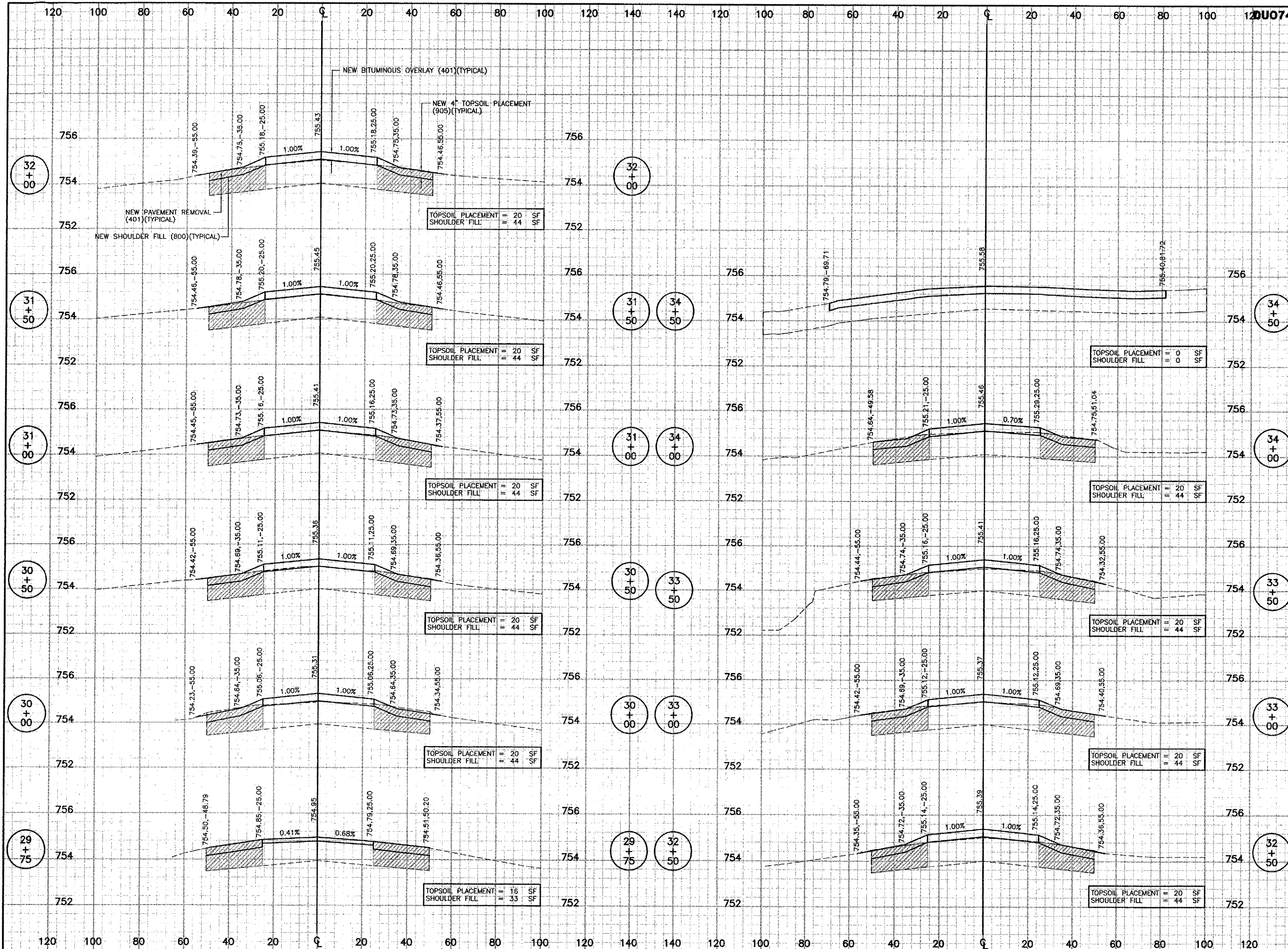
**RETROFITTED RUNWAY/TAXIWAY/SIGN CONNECTION DETAIL**

NO SCALE



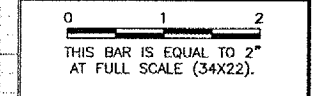
**NEW/ ADJUSTMENT/ RELOCATED BASE MOUNTED MEDIUM INTENSITY LIGHT**

NOT TO SCALE



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 DATE: .

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**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 REHABILITATE TAXIWAYS ALPHA AND CHARLIE  
 CROSS SECTIONS  
 TAXIWAY C**

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