

TIE-DOWN ADJUSTMENT DETAIL
"NOT TO SCALE"

TIE-DOWN ADJUSTMENT NOTE

THE EXISTING TIE-DOWN MOORING CASTINGS WILL BE REMOVED FROM THE EXISTING CONCRETE BASE AND REUSED FOR THE ADJUSTMENT.

REMOVE TO THE BOTTOM OF THE MOORING CASTINGS OR TO A 6" DEPTH WHICH EVER IS GREATER

ALL LOOSE CONCRETE WILL BE REMOVED AND DISPOSED OF OFF SITE.

FOUR (4) NO. 4 TIE-BARS WILL BE DRILLED AND GROUTED INTO THE EXISTING CONCRETE BASE AT EACH CORNER.

THE CONCRETE WILL BE POURED TO THE PROPOSED PAVEMENT ELEVATION.

THIS ITEM OF WORK WILL BE PAID FOR UNDER:
ITEM AR510940 "ADJUST TIE-DOWN" _____ 18 EACH.

603-BITUMINOUS TACK COAT NOTES:

THE BITUMINOUS TACK COAT (603) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR603 "BITUMINOUS TACK COAT" AS STATED ON PAGE 62 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE PROPOSED BITUMINOUS TACK COAT SHALL BE PLACED ON THE EXISTING BITUMINOUS PAVEMENT PRIOR TO THE PLACEMENT OF THE PROPOSED PFC. THE TACK COAT OF BITUMINOUS MATERIAL WILL BE APPLIED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.

THE QUANTITY OF THE PROPOSED BITUMINOUS TACK COAT HAS BEEN CALCULATED AT THE RATE OF 0.25 GALLONS PER SQUARE YARD.

THE PROPOSED BITUMINOUS TACK COAT WILL BE PAID FOR UNDER ITEM: AR603510 BITUMINOUS TACK COAT _____ 5,371 GAL.

BUTT JOINT CONSTRUCTION NOTES

THE PROPOSED BUTT JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ITEM AR401655 "BUTT JOINT CONSTRUCTION" AS STATED ON PAGE 295 AND IN ACCORDANCE WITH ITEM 501550 "PCC PAVEMENT MILLING" AS STATED ON PAGE 368 OF THE SUPPLEMENTAL SPECIFICATION AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

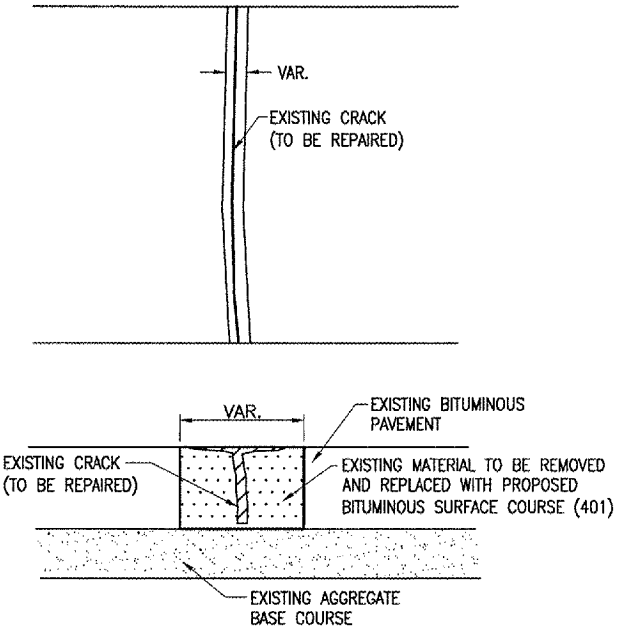
THIS ITEM SHALL CONSIST OF REMOVING THE EXISTING BITUMINOUS AND/OR CONCRETE PAVEMENT SURFACE BY MILLING IN ORDER TO CONSTRUCT A BUTT JOINT FOR THE TRANSITION OF THE PROPOSED POROUS FRICTION COURSE AT THE LOCATIONS AS SHOWN ON THE PLANS.

THE EXISTING PAVEMENT SURFACE WILL BE MILLED TO A DEPTH OF 0.17 FT. AT THE BUTT END AND WILL TAPER TO 0 IN. AT THE OPPOSITE END. THE PLANER MUST BE CAPABLE OF MILLING THE SURFACE TO THE DESIRED ELEVATIONS AND GRADES.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

ALL BITUMINOUS AND CONCRETE PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

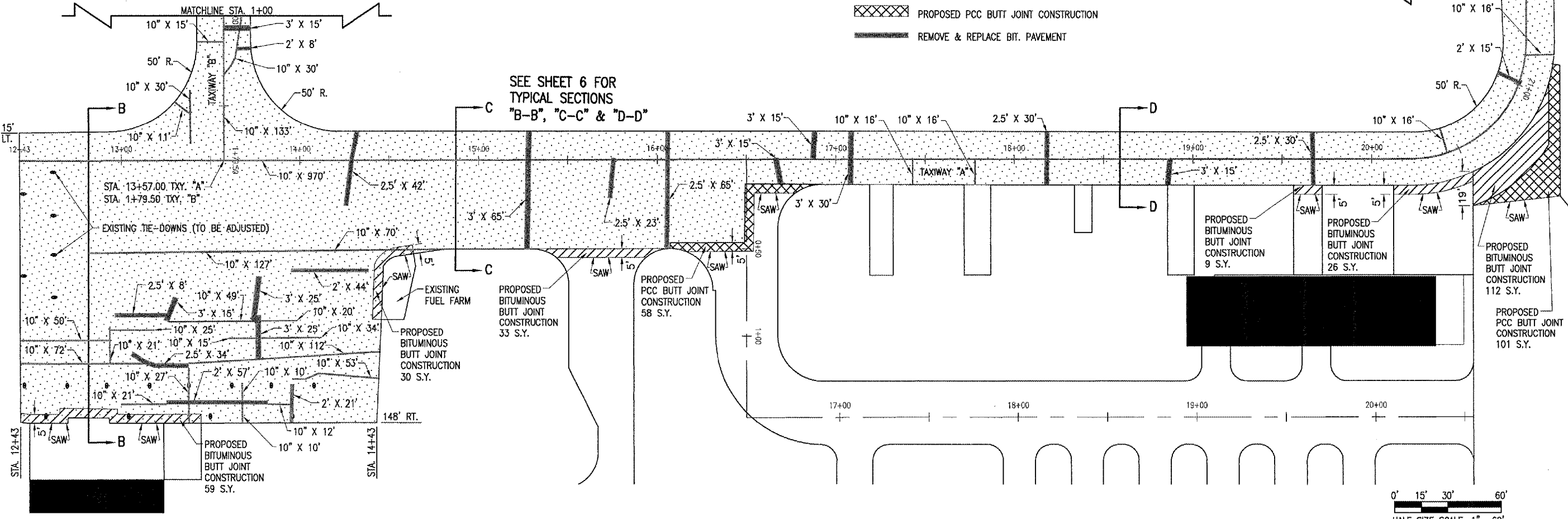
THE PAVEMENT MILLING WILL BE PAID FOR UNDER ITEM:
AR401655 "BUTT JOINT CONSTRUCTION" _____ 328 S.Y.
AR501551 "PCC BUTT JOINT" _____ 159 S.Y.



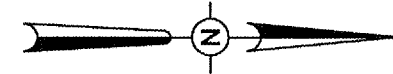
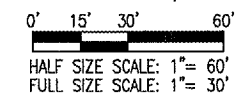
REMOVE & REPLACE BIT. PAVEMENT DETAIL
"NOT TO SCALE"

LEGEND

- EXISTING PAVEMENT
- EXISTING BUILDINGS
- EXISTING TIE-DOWNS (TO BE ADJUSTED)
- PROPOSED IMPROVEMENTS
- PROPOSED BITUMINOUS BUTT JOINT CONSTRUCTION
- PROPOSED PCC BUTT JOINT CONSTRUCTION
- REMOVE & REPLACE BIT. PAVEMENT



SEE SHEET 6 FOR TYPICAL SECTIONS "B-B", "C-C" & "D-D"



DATE	REVISION

FAIRFIELD MUNICIPAL AIRPORT
FAIRFIELD, ILLINOIS

FILE NO. 805-06RWD 0800	DATE 05/04/06
REVISION R-121CON.DWG	DATE 05/04/06
SCALE 1" = 30'	DATE 05/26/06
DESIGNED BY	CHECKED BY
DRAWN BY	APPROVED BY

HANSON
Hanson Professional Services Inc.
1500 North State Street
Springfield, IL 62703-2866
Offices Nationwide

RUNWAY 18-36
P.F.C. OVERLAY
PROPOSED CONSTRUCTION
PLAN FOR TAXIWAY "A"
STA. 12+43 TO STA. 21+50