

# GENERAL NOTES

## 1. SUGGESTED SEQUENCE OF CONSTRUCTION:

THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED ONLY AND MAY BE MODIFIED WITH THE APPROVAL OF THE ENGINEER AND AIRPORT MANAGER.

## 2. HAUL ROAD / STAGING AREA RESTORATION:

ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STAGING AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION TO THE SATISFACTION OF THE AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE PAVEMENTS / AREAS SHALL BE INCIDENTAL TO THE CONTRACT.

## 3. AIRPORT APPROVAL OF PHASING:

THE ENGINEER AND AIRPORT MANAGER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT OPERATIONS. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. VEHICULAR TRAFFIC / CONTRACTOR ACTIVITIES SHALL YIELD TO AIRCRAFT OPERATIONS.

## 4. AIRFIELD PAVEMENT / SITE DEBRIS REMOVAL:

THE CONTRACTOR SHALL RESTRICT ACCESS TO ALL TRUCKS, EQUIPMENT AND MATERIALS FROM THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS SHOWN. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEE USE. FACILITIES WITHIN THE HANGARS / AIRPORT BUILDINGS SHALL NOT BE USED.

## 5. PROJECT LIGHTING OUTSIDE OF DAYLIGHT HOURS:

WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND / OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND AIRPORT MANAGER, AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.

## 6. EXISTING UTILITY COORDINATION:

COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED PRIOR TO CONSTRUCTION. SEE SECTION 50 - 17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE AIRPORT NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED, ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY / OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE AIRPORT AND THE ENGINEER. ANY SUCH UTILITIES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE AIRPORT AND ENGINEER.

## 7. TRAFFIC CONTROL PAYMENT:

PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, ETC. SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO ASSOCIATED CONTRACT ITEM. TYPE 2 BARRICADES WITH STEADY BURN RED LIGHTS SHALL BE PLACED ON 15' CENTERS WITH CONSTRUCTION FENCE BETWEEN EACH SET OF BARRICADES. TYPE 2 BARRICADES SHALL BE PLACED AS SHOWN ON THIS PLAN AND AS DIRECTED BY THE ENGINEER FOR WORK ADJACENT TO THE EXPEDITED WORK AREAS. WHEN NOT IN USE, THESE BARRICADES SHALL BE STORED AT THE CONTRACTOR'S STAGING AREA OR OFF SITE. ACCESS TO THE ACTIVE RUNWAY AND TAXIWAY PAVEMENTS SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON THE CLOSEST BARRICADES (2 EACH, RIGHT AND LEFT) AT THE ENTRANCE. IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "ACTIVE AIRFIELD AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE." ALL TYPE II AND TYPE III BARRICADES SHALL CONFORM TO IDOT STANDARD DETAIL 702001. ALL PAVEMENT DROP-OFFS GREATER THAN 24" REQUIRE TYPE II BARRICADES WITH EXTENDED LEGS.

## 8. DUST CONTROL REQUIREMENTS:

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ON SITE DURING ALL CONSTRUCTION OPERATIONS GENERATING SIGNIFICANT DUST DURING WORKING HOURS. PAYMENT FOR DUST CONTROL SHALL BE INCIDENTAL TO THE ASSOCIATED CONTRACT ITEM.

## 9. OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION (AC 150/5370-2(Latest edition):

ALL WORK SHALL CONFORM TO FAA ADVISORY CIRCULAR (AC) AC 150/5370-2(LATEST EDITION) OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION. THIS AC IS AVAILABLE AT, [www.faa.gov/arp/pdf/5370-2x.pdf](http://www.faa.gov/arp/pdf/5370-2x.pdf)

## 10. STAGING AREA:

THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFERS SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.

## 11. AIRFIELD LIGHTING COORDINATION:

THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT MANAGER REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD LIGHTING CIRCUITS AT THE START AND END OF EACH CONSTRUCTION DAY.

## 12. WEEKLY COORDINATION MEETINGS

WEEKLY COORDINATION MEETINGS SHALL BE REQUIRED TO DISCUSS PROJECT PROGRESS. REPRESENTATION BY THE PRIME CONTRACTOR IS MANDATORY.

## 13. AIRFIELD FENCING / GATE SECURITY:

ALL EXISTING AND PROPOSED FENCE LINES, EXCEPT AS OTHERWISE NOTED, SHALL BE MAINTAINED AND SHALL SERVE AS CONSTRUCTION FENCING AROUND THE PERIMETER OF THE PROJECT. ALL EXISTING GATES SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE AIRPORT OWNER'S REPRESENTATIVE. SHOULD THE CONTRACTOR CHOOSE TO KEEP A GATE OPEN FOR CONSTRUCTION OPERATIONS, A COMPETENT SECURITY GUARD SHALL MONITOR THE OPEN GATE. ANY COSTS SHALL NOT BE PAID FOR SEPARATELY, BUT THEY WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

## 14. AIRPORT RADIO COMMUNICATION:

THE CONTRACTOR SHALL SUPPLY AND HAVE IN THEIR POSSESSION AT ALL TIMES AT LEAST ONE AIRPORT RADIO MONITORING AIRPORT TRAFFIC. THE OPERATOR OF THE AIRPORT RADIO SHALL BE FAMILIAR WITH AIRPORT RADIO PROCEDURES AND BE TUNED INTO THE APPLICABLE GROUND CONTROL FREQUENCY.

# PROJECT NOTES

FR037



CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
© Copyright CMT, Inc.



Freeport, Illinois

City of Freeport

AIP: 3-17-0045-B20  
FEP-3129

Airfield Pavement  
Repairs

### Revisions

No.	Date	Description

0 1  
THIS BAR IS EQUAL TO 1"  
AT FULL SCALE (17 X 11).

DESIGN BY:	CMT- RFD
DRAWN BY:	CMT- RFD
CHECKED BY:	CMT- RFD
APPROVED BY:	JGP
DATE:	6.9.2006
JOB No:	06294-02-00

SEQUENCE OF  
CONSTRUCTION  
PLAN, NOTES AND  
DETAILS - 2  
(SEQ2)

5

SHEET 5 OF 9 SHEETS

## 1. UNATTENDED CONSTRUCTION ACCESS:

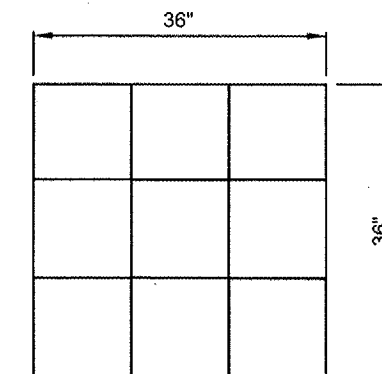
THE OWNER WILL ASSESS THE CONTRACTOR A \$1,000.00 FINE FOR CLOSING AND/OR LOCKING EACH UNATTENDED ACCESS DOOR, GATE OR FENCE WHICH HAS BEEN IDENTIFIED AS A CONTRACTOR RESPONSIBILITY. AN ACCESS DOOR, GATE OR FENCE IS DEFINED AS "UNATTENDED" ANY TIME IT IS OPEN, UNLOCKED OR OTHERWISE RENDERED INEFFECTIVE IN PROVIDING SECURITY AND CONTRACTOR PERSONNEL ARE NOT PHYSICALLY IN A POSITION TO DETECT AND PREVENT UNAUTHORIZED ENTRY THROUGH IT. IN THE EVENT AIRPORT PERSONNEL ARE UNABLE TO SECURE AN UNATTENDED ACCESS DOOR, GATE OR FENCE, AIRPORT SECURITY PERSONNEL WILL BE POSTED AND CONTRACTORS WILL BE NOTIFIED. CONTRACTORS WILL BE CHARGED THE COST OF PROVIDING SECURITY PERSONNEL UNTIL THE AREA IS AGAIN SECURE.

## 2. UNAUTHORIZED ACCESS TO AIRFIELD:

THE CONTRACTOR SHALL RESTRICT ALL ACTIVITIES TO THE CONSTRUCTION AREA DETAILED IN THE PHASING PLAN. ANY UNAUTHORIZED ACCESS, PEDESTRIAN OR VEHICULAR, TO ACTIVE AIRFIELD PAVEMENTS SHOWN SHALL BE CONSIDERED AIRFIELD INCURSIONS. AIRFIELD INCURSIONS, AT THE DISCRETION OF THE AIRPORT DIRECTOR OF OPERATIONS, MAY BE FINED \$1,000.00 PER INCIDENT. INCURSION FINES WILL BE ASSESSED IMMEDIATELY AND TAKEN FROM MONIES DUE THE CONTRACTOR ON THE NEXT CONSTRUCTION PAYMENT.

## 3. CONSTRUCTION SITE ACCESS:

THE CONTRACTOR SHALL UTILIZE THE HAUL ROAD AS SHOWN ON THE CONSTRUCTION PHASING PLAN. THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLE ACCESS AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY.



CONSTRUCTION EQUIPMENT  
AND TRUCK SIGNAL FLAG

NOT TO SCALE

# SEQUENCE OF CONSTRUCTION PLAN, NOTES AND DETAILS - 2