

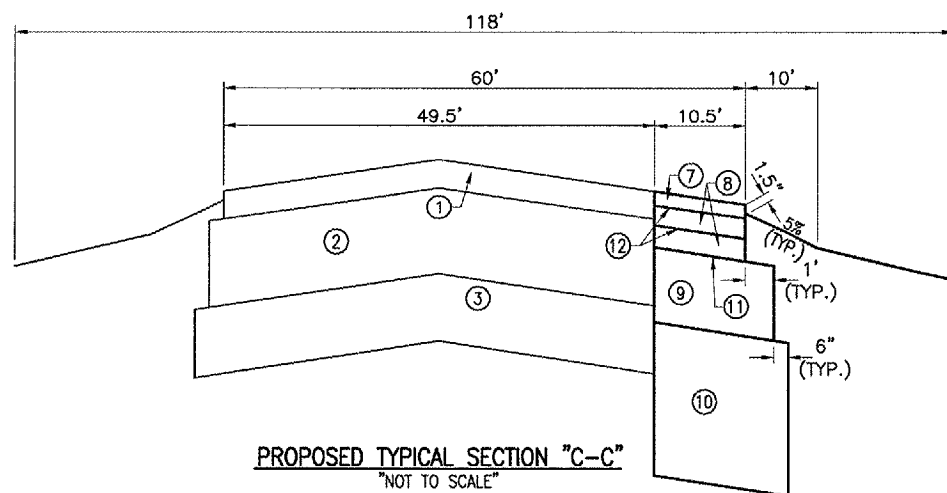
PROPOSED TYPICAL SECTION "A-A"
"NOT TO SCALE"

LEGEND FOR TYPICAL SECTIONS "A-A", "B-B" & "C-C"

- ① EXISTING 401 BITUMINOUS SURFACE COURSE (3" AVG. DEPTH)
- ② EXISTING 501 PCC PAVEMENT (9" DEPTH)
- ③ EXISTING 209 CRUSHED AGGREGATE BASE COURSE (7" DEPTH)
- ④ PROPOSED 501 PCC PAVEMENT (11" DEPTH)
- ⑤ PROPOSED 209 AGGREGATE BASE (5" DEPTH)
- ⑥ PROPOSED 155 LIME MODIFIED SUBGRADE (16" DEPTH)
- ⑦ PROPOSED 401 BITUMINOUS SURFACE COURSE (1-1/2" DEPTH, COMPACTED)
- ⑧ PROPOSED 401 BITUMINOUS BASE COURSE (2-1/4" DEPTH, COMPACTED)
- ⑨ PROPOSED 209 CRUSHED AGGREGATE BASE COURSE (8" DEPTH)
- ⑩ PROPOSED 155 LIME MODIFIED SUBGRADE (16" DEPTH)
- ⑪ PROPOSED 602 BITUMINOUS PRIME COAT (0.35 GAL./S.Y.)
- ⑫ PROPOSED 603 BITUMINOUS TACK COAT (0.05 GAL./S.Y., MIN.)*

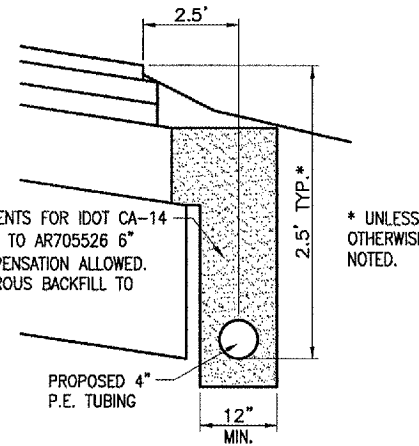
NOTE:

* QUANTITY BASED ON RATE OF 0.15 GAL./SY.



PROPOSED TYPICAL SECTION "C-C"
"NOT TO SCALE"

POROUS BACKFILL SHALL CONFORM TO THE REQUIREMENTS FOR IDOT CA-14 OR IDOT CA-16 AND WILL BE CONSIDERED INCIDENTAL TO AR705526 6" PERF. UNDERDRAIN W/SOCK AND NO ADDITIONAL COMPENSATION ALLOWED. CONTRACTOR SHALL PLACE AND CONSOLIDATE THE POROUS BACKFILL TO THE SATISFACTION OF THE RESIDENT ENGINEER.



UNDERDRAIN DETAIL
"NOT TO SCALE"

SOIL PROCESSING:

THE STANDARD DRY DENSITY OF THE LIME-MODIFIED SOIL SHALL BE DETERMINED BY ASTM D1557 FOR AIRCRAFT 60,000 LBS OR MORE. DETERMINATION OF OPTIMUM MOISTURE CONTENT AND STANDARD DRY DENSITY OF LIME MODIFIED SOIL IS THE CONTRACTOR'S RESPONSIBILITY.

LIME MODIFIED SOIL PROCESSING SHALL CONSIST OF CONSTRUCTING A 16 INCH COURSE OF A MIXTURE OF SOIL, LIME, AND WATER IN ACCORDANCE WITH THE RATES AND METHODS PROVIDED.

THE BY-PRODUCT LIME QUANTITY WAS CALCULATED AT 6% OF THE DRY SOIL WEIGHT (120 LBS/CF) MAXIMUM DENSITY. THE ACTUAL AMOUNT SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE RESIDENT ENGINEER PRIOR TO CONSTRUCTION.

THE SUBGRADE SHALL BE CUT AND WITHIN GRADE TOLERANCE PRIOR TO LIME MODIFICATION. THE CONTRACTOR SHALL PROCESS THE SOIL TO 1-1/2' OUTSIDE THE PROPOSED PAVEMENT SURFACE ON ALL SIDES.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ACCOUNT FOR ANY "FLUFF" OR "SWELL" IN THE MODIFIED SUBGRADE.

ANY SWELL SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. NO ADDITIONAL PAYMENT WILL BE MADE FOR REMOVAL OF SWELL.

ONCE THE SPECIFIED DENSITY OF THE LIME-SOIL MIXTURE IS ACHIEVED, THE CONTRACTOR MAY START FINAL TRIMMING OPERATIONS AND PLACEMENT OF THE OVERLYING COURSE IF THE COMPACTED LIME MODIFIED LAYER IS NOT RUTTED OR DISTORTED BY THE EQUIPMENT.

THE CONTRACTOR SHALL KEEP THE LIME-SOIL MIXTURE MOIST CURED FOR A MINIMUM OF SEVEN (7) DAYS BY WATERING OR BY PLACEMENT OF THE OVERLYING COURSE.

PCC PAVING NOTE:

THE PROPOSED PCC CONCRETE SHALL COMPLY WITH ITEM AR501002 "PORTLAND CEMENT CONCRETE PAVEMENT METHOD II", DESIGNED FOR AIRCRAFT WEIGHT GREATER THAN 60,000 LBS.

AR209-CRUSHED AGGREGATE BASE COURSE NOTES

THE CRUSHED AGGREGATE BASE COURSE (209) SHALL BE PLACED IN ACCORDANCE WITH ITEM 209 "CRUSHED AGGREGATE BASE COURSE" AS STATED ON PAGE 45 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE CRUSHED AGGREGATE BASE COURSE MATERIAL (CA-6) WILL BE USED TO CONSTRUCT A BASE COURSE FOR THE PROPOSED PAVEMENTS. THE CRUSHED AGGREGATE BASE COURSE MATERIAL WILL BE CONSTRUCTED TO THE DEPTHS SHOWN IN THE TYPICAL SECTIONS AND COMPACTED TO NOT LESS THAN 95 PERCENT DENSITY.

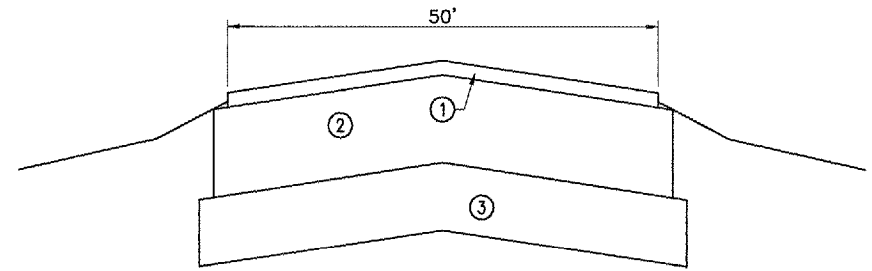
THE PROPOSED AGGREGATE FOR THE BASE COURSE MATERIAL SHALL MEET THE REQUIREMENTS OF (GRADATION "B") IN TABLE 1 OF THE SUPPLEMENTAL SPECIFICATIONS.

PROVISIONS SHALL BE MADE BY THE CONTRACTOR FOR FURNISHING WATER AT THE PLANT AND AT THE SITE OF THE WORK BY EQUIPMENT OF AMPLE CAPACITY AND OF SUCH DESIGN AS TO ASSURE UNIFORM MIXING AND APPLICATION.

THE CONTRACTOR WILL PROVIDE THE RESIDENT ENGINEER A PROCTOR CURVE SHOWING OPTIMUM DENSITY AND MOISTURE FOR THE SUPPLIED BASE COURSE MATERIAL.

THE COMPACTION CONTROL TEST TO BE USED SHALL BE FOR AIRCRAFT GREATER THAN 60,000 LBS.

IF AT THE TIME THE AGGREGATES ARE WEIGHED THEY CONTAIN MORE THAN SIX (6) PERCENT OF ABSORBED AND FREE MOISTURE BY WEIGHT, A DEDUCTION FOR THE MOISTURE IN EXCESS OF THIS AMOUNT SHALL BE MADE IN DETERMINING THE PAY QUANTITY.



EXISTING TYPICAL SECTION "B-B"
"NOT TO SCALE"

STATION	AREA (SQ. FEET)		VOLUME (CUBIC YARDS)		CUMULATIVE COLUMNS (CUBIC YARDS)	
	CUT	FILL	CUT	FILL	CUT	FILL
	145+47.00	0.00	0.00	0.00	0.00	0.00
146+00.00	1.10	0.00	1.08	0.00	1.08	0.00
147+00.00	2.10	0.00	5.93	0.00	7.01	0.00
147+14.95	20.20	0.00	6.17	0.00	13.18	0.00
147+50.00	17.60	0.20	24.53	0.13	37.71	0.13
147+73.00	21.30	0.00	16.57	0.09	54.28	0.21
148+00.00	18.10	0.30	19.70	0.15	73.98	0.36
148+50.00	15.90	0.60	31.48	0.83	105.46	1.20
149+00.00	21.90	0.90	35.00	1.39	140.46	2.59
149+50.00	21.50	1.00	40.19	1.76	180.65	4.35
150+00.00	26.80	4.30	44.72	4.91	225.37	9.25
150+15.00	53.20	3.00	22.22	2.03	247.59	11.28
150+50.00	45.20	23.50	63.78	17.18	311.37	28.46
150+58.00	41.20	52.10	12.80	11.20	324.17	39.66
151+00.00	50.70	60.90	71.48	87.89	395.65	127.55
151+18.13	63.80	59.70	38.44	40.49	434.09	168.04
151+50.00	67.50	51.20	77.49	65.45	511.58	233.49
152+00.00	76.30	57.60	133.15	100.74	644.73	334.23
152+04.00	64.20	48.40	10.41	7.85	655.14	342.08
152+68.13	31.30	40.90	113.42	106.05	768.55	448.13
153+38.13	2.40	51.60	43.69	119.91	812.24	568.04
153+68.13	3.20	48.20	3.11	55.44	815.35	623.48
153+98.13	16.30	24.40	10.83	40.33	826.18	663.82
154+68.13	120.10	13.80	176.81	49.52	1,003.00	713.34
155+00.00	62.30	21.20	107.65	20.66	1,110.65	733.99
155+50.00	72.30	39.30	124.63	56.02	1,235.28	790.01
156+00.00	82.80	59.00	143.61	91.02	1,378.89	881.03
156+18.13	39.20	53.80	40.96	37.87	1,419.85	918.90
156+50.00	8.70	37.10	28.27	53.65	1,448.12	972.55
157+00.00	9.60	30.10	16.94	62.22	1,465.12	1,034.77
157+38.46	13.40	25.60	16.38	39.67	1,481.44	1,074.44
157+50.00	0.60	19.90	2.99	9.72	1,484.44	1,084.17
158+00.00	29.40	35.40	37.78	51.20	1,512.21	1,135.37
158+23.49	60.20	45.80	38.98	35.32	1,551.19	1,170.69
TOTAL:					1,551.19	1,170.69

NOTE:

THE CUT VOLUME SHOWN IS EARTHWORK ONLY AND DOES NOT INCLUDE THE VOLUME OF PAVEMENT AND AGGREGATE BASE REMOVED. THE FILL VOLUME SHOWN INCLUDES THE FILL REQUIRED TO BACKFILL THE VOID CREATED BY THE REMOVAL OF THE EXISTING PAVEMENT AND AGGREGATE BASE.

JUL 11, 2006 9:28 PM RAW I:\AIRPORTS\QUINCY\B2406TCD\AIRPORT\SHEETS\R-12\CON.DWG - DETAILS

REVISION		DATE			
BY					
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS					
ILL. PROJECT NO. 3-17-0085-XX ILL. PROJ. UN-3338					
HANSON Hanson Professional Services Inc. 1400 North Lincoln Springfield, Illinois 62703-2886 Offices Nationwide					
REALIGN AND WIDEN TAXIWAY "C" TYPICAL SECTIONS, PAVING NOTES, AND EARTHWORK DISTRIBUTION					
5					
6 of 24 sheets					