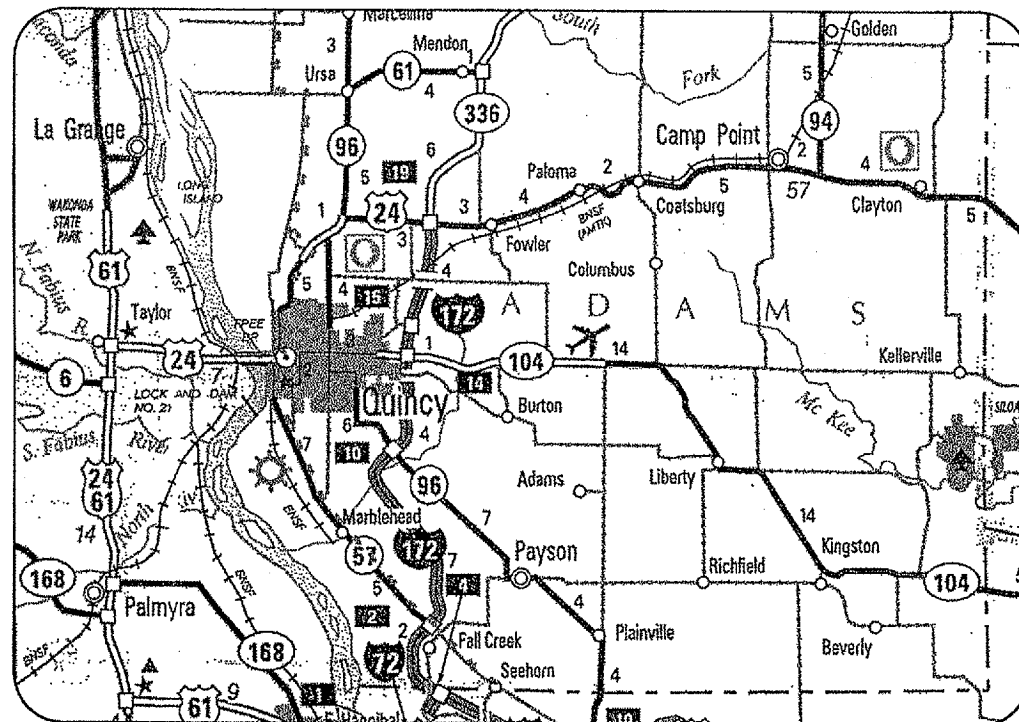
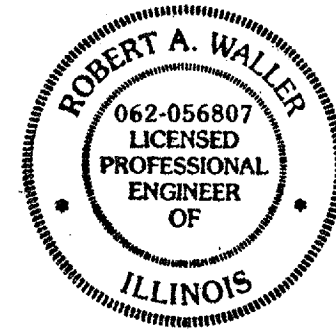


CONSTRUCTION PLANS FOR QUINCY REGIONAL AIRPORT - BALDWIN FIELD QUINCY, ADAMS COUNTY, ILLINOIS REALIGN AND WIDEN TAXIWAY "C"

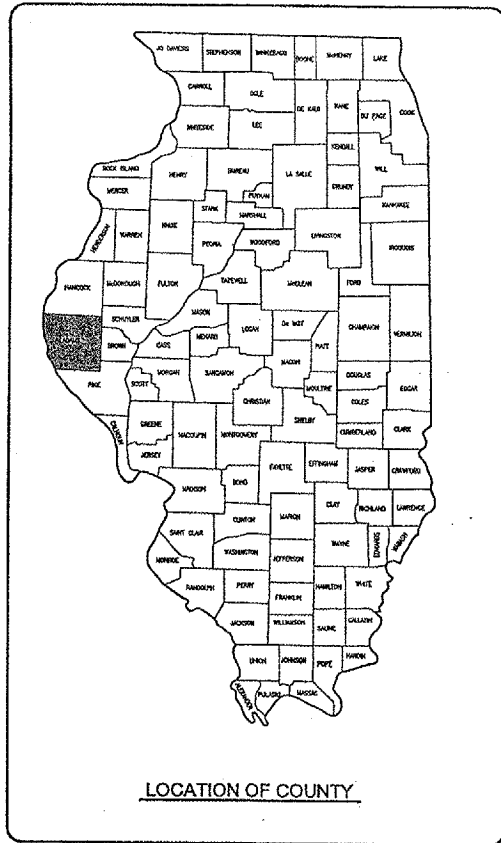
SCOPE OF WORK

THE PROJECT SCOPE CONSISTS OF REALIGNING AND WIDENING TAXIWAY "C" FROM A WIDTH OF 50 FT TO A WIDTH OF 60 FT. THE REALIGNMENT WILL UTILIZE 11" PORTLAND CEMENT CONCRETE (PCC) PAVEMENT, THE WIDENING COMPLETED USING BITUMINOUS PAVEMENT. ASSOCIATED WORK WILL INCLUDE PAVEMENT REMOVAL, EXCAVATION, DRAINAGE, CABLING AND LIGHTING, PAVEMENT MARKING, SEEDING, MULCHING, AND THE PLACEMENT OF EROSION CONTROL BLANKET



LOCATION

ILL. PROJ.:	UIN-3338
A.I.P. PROJ.:	3-17-0085-XX
LATITUDE:	39° 56' 33"
LONGITUDE:	91° 11' 40"
ELEVATION:	769.0' M.S.L.
DATE:	JULY 12, 2006



HANSON
Hanson Professional Services Inc.

Submitted by: *Robert A. Waller* ENG'R

Date Submitted: 7/12/06

Lics. Exp. Date: 11/30/07

CITY OF QUINCY

Approved: *Philip A. Greig* MAYOR.

Date: 6/26/06

Approved: *H. Steinberg* CITY ENGINEER

Date: 6/26/06

Approved: *Mark J. Hanna* AIRPORT DIRECTOR

Date: 6/26/06

DATE	REVISION	BY

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS

ILL. PROJ.: UIN-3338 A.I.P. PROJ.: 3-17-0085-XX

HEL Project No. 82406TYCD_0240 Filename: R-001CVR.DWG Scale: N/A Date: 06/19/06	LAYOUT: RAW 06/19/06 DRAWN: BAK 06/19/06 REVIEWED: RAW 07/12/06
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HANSON
Hanson Professional Services Inc.
1525 South Sixth Street
Springfield, Illinois 62703-2885
Offices Nationwide

REALIGN AND WIDEN TAXIWAY "C"

COVER SHEET

1

1 of 24 sheets

DATE	REVISION	BY


SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITIES	AS BUILT QUANTITIES
AR108158	1/C #8 5 KV UG CABLE IN UD	L.F.	2,800	
AR110502	2-WAY CONCRETE ENCASED DUCT	L.F.	70	
AR125410	MITL-STAKE MOUNTED	EA.	25	
AR125415	MITL-BASE MOUNTED	EA.	3	
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EA.	1	
AR125565	SPLICE CAN	EA.	1	
AR125901	REMOVE STAKE MOUNTED LIGHT	EA.	27	
AR125902	REMOVE BASE MOUNTED LIGHT	EA.	4	
AR125904	REMOVE TAXI GUIDANCE SIGN	EA.	1	
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150540	HAUL ROUTE	L.S.	1	
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	1,552	
AR155540	BY-PRODUCT LIME	TON	293	
AR155616	SOIL PROCESSING - 16"	S.Y.	6,760	
AR156511	DITCH CHECK	EA.	3	
AR156520	INLET PROTECTION	EA.	5	
AR156531	EROSION CONTROL BLANKET	S.Y.	1,758	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	2,000	
AR401610	BITUMINOUS SURFACE COURSE	TON	236	
AR401655	BUTT JOINT CONSTRUCTION	S.Y.	323	
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	3,395	
AR501511	11" PCC PAVEMENT	S.Y.	6,029	
AR501530	PCC TEST BATCH	EA.	1	
AR602510	BITUMINOUS PRIME COAT	GAL.	145	
AR603510	BITUMINOUS TACK COAT	GAL.	212	
AR620520	PAVEMENT MARKING - WATERBORNE	S.F.	1,947	
AR620525	PAVEMENT MARKING - BLACK BORDER	S.F.	2,003	
AR620900	PAVEMENT MARKING REMOVAL	S.F.	1,112	
AR701512	12" RCP, CLASS IV	L.F.	246	
AR705526	6" PERFORATED UNDERDRAIN W/SOCK	L.F.	1,882	
AR705546	6" NON PERFORATED UNDERDRAIN	L.F.	50	
AR705630	UNDERDRAIN INSPECTION HOLE	EA.	6	
AR705640	UNDERDRAIN CLEANOUT	EA.	2	
AR705900	REMOVE UNDERDRAIN	L.F.	1,653	
AR751410	INLET	EA.	1	
AR800449	WEED CONTROL LIGHT RING	EA.	28	
AR901510	SEEDING	AC.	3.2	
AR908510	MULCHING	AC.	3.2	

INDEX TO SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	SUMMARY OF QUANTITIES AND INDEX TO SHEETS
3	PROPOSED SAFETY PLAN
4	PROPOSED CONSTRUCTION PLAN
5	TYPICAL SECTIONS, PAVING NOTES, AND EARTHWORK DISTRIBUTION
6	PROPOSED STORMWATER POLLUTION PREVENTION PLAN
7	PROPOSED PLAN AND PROFILE
8	PROPOSED STAKING PLAN
9	PROPOSED DRAINAGE PLAN
10	PROPOSED DRAINAGE DETAILS
11	PROPOSED JOINTING PLAN
12	PROPOSED JOINTING DETAILS
13	EXISTING ELECTRICAL PLAN
14	PROPOSED ELECTRICAL PLAN
15	PROPOSED ELECTRICAL DETAILS
16	PROPOSED ELECTRICAL DETAILS
17	PROPOSED ELECTRICAL NOTES
18	PROPOSED ELECTRICAL NOTES
19	PROPOSED MARKING PLAN
20	PROPOSED CROSS-SECTIONS STA. 145+47 TO STA. 148+50
21	PROPOSED CROSS-SECTIONS STA. 149+00 TO STA. 150+58
22	PROPOSED CROSS-SECTIONS STA. 151+00 TO STA. 153+38.13
23	PROPOSED CROSS-SECTIONS STA. 153+68.13 TO STA. 156+00
24	PROPOSED CROSS-SECTIONS STA. 156+18.13 TO STA. 158+23.49

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS



IL PROJ.: UIN-3338 A.I.P. PROJ.: 3-17-0085-XX

NET Project No.	82408TYCD_0240	LAYOUT	RAW	06/19/06
Drawn	R-002FLP.DWG	DRAWN	BAK	06/19/06
Scale	N/A	REVIEWED	RAW	07/12/06
Date	06/19/06			



REALIGN AND WIDEN TAXIWAY "C"
SUMMARY OF QUANTITIES AND INDEX TO SHEETS

SCOPE OF WORK

THE PROJECT SCOPE CONSISTS OF REALIGNING AND WIDENING TAXIWAY "C" FROM A WIDTH OF 50 FT TO A WIDTH OF 60 FT. THE REALIGNMENT WILL UTILIZE 11" PORTLAND CEMENT CONCRETE (PCC) PAVEMENT, THE WIDENING COMPLETED USING BITUMINOUS PAVEMENT. ASSOCIATED WORK WILL INCLUDE PAVEMENT REMOVAL, EXCAVATION, DRAINAGE, CABLING AND LIGHTING, PAVEMENT MARKING, SEEDING, MULCHING, AND THE PLACEMENT OF EROSION CONTROL BLANKET

AIRPORT SECURITY NOTE

THE AIRPORT SPONSOR MAY PROVIDE ADDITIONAL GUIDANCE AND TRAINING FOR AIRPORT SECURITY MEASURES. AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL CLOSE AND LOCK THE EXISTING GATE IN THE HAUL ROUTE AT THE END OF EACH WORKING DAY. ANY ACCESS GATES WILL BE CLOSED AT ALL TIMES WHEN NOT IN USE. IF CONTINUOUS HAULING IS REQUIRED, THEN THE GATE ACCESS WILL BE MONITORED TO PREVENT A BREACH FROM OUTSIDE, NON-CONSTRUCTION RELATED TRAFFIC.

UTILITY NOTE

THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES AND AGENCIES WHICH HAVE LINES OR CONDUITS IN THE PROPOSED WORK AREA. ALL LINES AND CONDUITS SHALL BE LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. THE CONTRACTOR WILL CALL J.U.L.I.E. (1-800-892-0123) TO ACCOMPLISH THE ABOVE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO IDENTIFY ALL UNDERGROUND NON-JULIE UTILITIES LOCATED WITHIN THE PROPOSED CONSTRUCTION LIMITS. THESE UNDERGROUND IMPROVEMENTS WILL BE LOCATED AT THE CONTRACTOR'S OWN EXPENSE PRIOR TO THE START OF CONSTRUCTION ACTIVITIES.

HEIGHT OF CONSTRUCTION EQUIPMENT

THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A SEMI TRUCK WITH TRAILER IN THE UP POSITION.

HAUL ROUTE AND VEHICLE PARKING

THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND PARKING AREA AS SHOWN ON THIS SHEET AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS. THE PROPOSED PARKING AREA WILL BE APPROXIMATELY 250' X 250', POSITIONED AS SHOWN. THE HAUL ROUTE WILL BE LOCATED AS SHOWN. A SWING GATE (10' HT.) SHALL BE INSTALLED AT THE LOCATION SHOWN, AND WILL REMAIN FOLLOWING THE CONCLUSION OF THE PROJECT. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND PARKING AREA THROUGHOUT THE COURSE OF THE PROJECT. ANY AREAS DAMAGED OUTSIDE OF THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AND AT THE CONTRACTOR'S OWN EXPENSE. AT THE CONCLUSION OF THE PROJECT THE CONTRACTOR WILL GRADE, FERTILIZE, SEED AND MULCH THE HAUL ROUTE AND PARKING AREA AS NEEDED TO RESTORE IT TO ITS ORIGINAL STATE. CONSTRUCTION, MAINTENANCE, AND ANY REQUIRED RESTORATION OF THE HAUL ROUTE AND PARKING AREA, INCLUDING LABOR, MATERIALS, AND EQUIPMENT TO INSTALL THE PROPOSED SWING GATE, PIPE CULVERT, PERMIT FEES, AND OTHER INCIDENTAL ITEMS ASSOCIATED WITH ESTABLISHING THE TEMPORARY ACCESS TO IL ROUTE 104 WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

CONTRACTOR RESPONSIBILITIES

THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREA WILL BE AS SHOWN ON THIS SHEET. THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THIS AREA. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE THIS AREA.

THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.

THE CONTRACTOR SHALL KEEP RUNWAY 4-22 OPEN AT ALL TIMES AND MAINTAIN CONTINUOUS ACCESS TO ALL HANGARS AND ADMINISTRATIVE AREAS.

ALL WORK PERFORMED SHALL BE DONE IN A ORDERLY AND EFFECTIVE MANNER TO MINIMIZE ALL RUNWAY CLOSURES.

TRENCHES AND/OR HOLES THAT REMAIN OPEN OVERNIGHT WILL REQUIRE BARRICADES AND/OR CONES TO INDICATE THEIR LOCATION AND PREVENT ACCIDENTAL ENTRANCE, WITHIN THE PROJECT AREA AND OUTSIDE ANY ACTIVE SAFETY AREAS.

BARRICADES AND TRAFFIC CONES

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND TRAFFIC CONES AS DIRECTED BY THE AIRPORT MANAGER OR HIS DESIGNATED REPRESENTATIVE. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING OR RED STEADY-BURN LIGHTS AND 20" SQUARE ORANGE FLAGS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

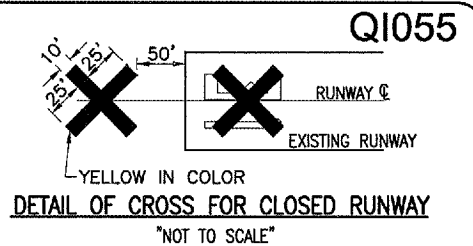
CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123.

BENCHMARK DATA

NO.	DESCRIPTION	NORTHING	EASTING	ELEV.
1	CP #61 STAINLESS STEEL ROD IN SLEEVE ±200' SE OF WINDSOCK POST	1,192,998.5670	2,009,012.5220	760.63
2	CB-24 CHISELED "□" NE CORNER CONCRETE AROUND DROP INLET	-	-	757.77
3	CB-25 CHISELED "□" NE CORNER CONCRETE AROUND DROP INLET	-	-	759.85
4	PK NAIL AT RUNWAY END 31	1,191,936.0250	2,010,470.4040	-

CRITICAL POINT DATA

POINT NO.	DESCRIPTION	LATITUDE	LONGITUDE	ELEVATION
1	AIRCRAFT OPERATION LINE @ C OF TXY C	39°56'05.87"	91°11'15.26"	767.20'



NOTE:

COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. ALL RUNWAY CLOSURES WILL BE IN ACCORDANCE WITH ALL FAA ADVISORY CIRCULARS THAT RELATE TO RUNWAY CLOSURES DURING CONSTRUCTION OPERATIONS. THE RUNWAY CLOSURE PROCEDURES SHALL BE REVIEWED BY THE AIRPORT MANAGER AND COORDINATED WITH THE RESIDENT ENGINEER. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE RESIDENT ENGINEER AND REVIEWED BY THE AIRPORT MANAGER. THE CROSSES WILL BE PLACED OVER THE NUMERALS AND SECURED IN A MANNER APPROVED BY THE AIRPORT MANAGER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

J.U.L.I.E. INFORMATION

COUNTY: ADAMS
 CITY: QUINCY
 TOWNSHIP: GLIMMER
 SECTION NO.: 34
 ADDRESS: QUINCY REGIONAL AIRPORT - BALDWIN FIELD
 1645 HIGHWAY 104
 QUINCY, ILLINOIS 62305
 217-885-3285

PROPOSED SAFETY PLAN

GENERAL - THE QUINCY REGIONAL AIRPORT IS COMPRISED OF THREE RUNWAYS. RUNWAY 4-22 WILL REMAIN OPEN AT ALL TIMES. THE PROPOSED CONSTRUCTION WILL NECESSITATE CLOSING RUNWAY 13-31 ANY TIME THE CONTRACTOR IS WORKING WITHIN 200' OF THE RUNWAY CENTERLINE. PRIOR TO OPENING RUNWAY 13-31 FOR USE, THE CONTRACTOR WILL SMOOTH GRADE ALL AREAS WITHIN THE SAFETY AREA TO THE SATISFACTION OF THE RESIDENT ENGINEER, ENSURE RUNWAY LIGHTING IS OPERABLE, AND REMOVE ALL TRAFFIC CONES, BARRICADES, AND CLOSURE CROSSES. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE AIRPORT MANAGER, OR HIS DESIGNATED REPRESENTATIVE, WILL ISSUE ALL NOTICES TO AIRMAN (NOTAM) RELATED TO OPENING AND CLOSING PAVEMENTS THROUGHOUT THE PROJECT.

IDENTIFICATION - WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CONTRACTOR WILL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.

RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (123.00 MHz) WITH THE AIRPORT UNICOM. THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE QUINCY REGIONAL AIRPORT AND ENABLE THE AIRPORT TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

150-ENGINEER'S FIELD OFFICE NOTES

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE FURNISHED, MAINTAINED, AND REMOVED IN ACCORDANCE WITH ITEM AR150510 "ENGINEER'S FIELD OFFICE" AS STATED ON PAGE 168 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE LOCATION OF THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.

THE ENGINEERING FIRM WILL MAKE PAYMENT FOR ALL LONG DISTANCE TELEPHONE CALLS IN EXCESS OF ONE HUNDRED DOLLARS (\$100.00) PER MONTH.

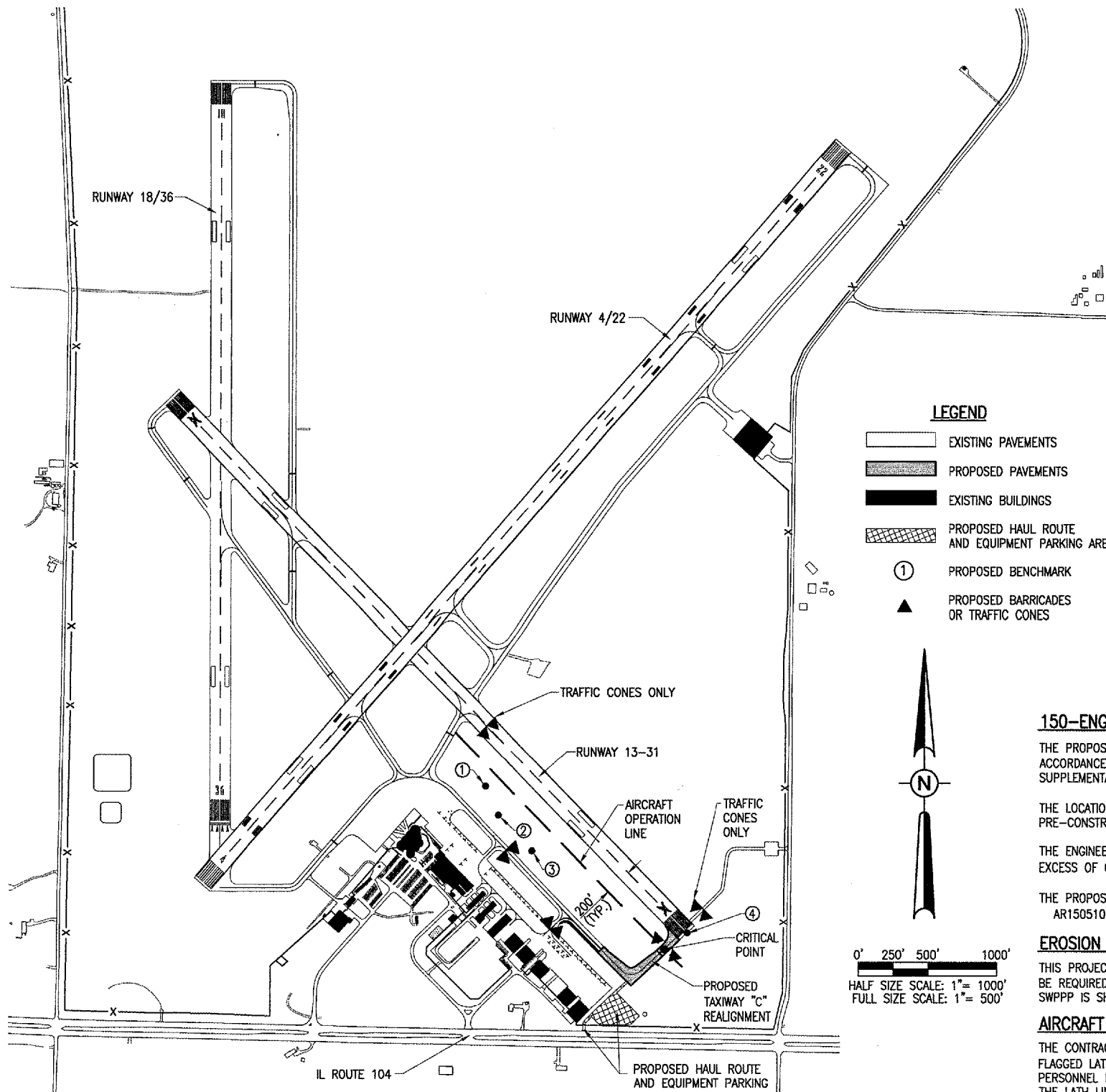
THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE PAID FOR UNDER ITEMS:
 AR150510 ENGINEER'S FIELD OFFICE ____ 1 L.S.

EROSION CONTROL

THIS PROJECT WILL DISTURB MORE THAN 1 ACRE OF LAND, THEREFORE A N.P.D.E.S. PERMIT WILL BE REQUIRED AND A STORMWATER POLLUTION PREVENTION PLAN (SWPPP) IMPLEMENTED. THE SWPPP IS SHOWN ON SHEET 6.

AIRCRAFT OPERATION LINE

THE CONTRACTOR WILL LOCATE THIS LINE AT THE START OF CONSTRUCTION AND WILL PLACE FLAGGED LATH EVERY 150' ALONG IT. THIS LINE WILL BE THE LIMITS THAT ALL CONTRACTOR PERSONNEL MAY VENTURE WHEN A RUNWAY IS NOT CLOSED. THE CONTRACTOR WILL MAINTAIN THE LATH LINE FOR RUNWAYS THROUGHOUT THE PROJECT.



BY: _____ DATE: _____

REVISION: _____

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS

PROJECT: 3-17-0085-XX
I.L. PROJ.: UIN-3338

FILE: Project No. 82406TCD_0240
Filename: R_003SEY.DWG
Scale: 1"=500'
Date: 06/19/06

LAYOUT	RAW	06/19/06
DRAWN	BAK	06/19/06
REVIEWED	RAW	07/12/06

HANSON
 Hanson Professional Services Inc.
 1525 South State Street
 Springfield, Illinois 62703-2988
 Offices Nationwide

REALIGN AND WIDEN TAXIWAY "C"
 PROPOSED SAFETY PLAN

3 of 24 sheets

JUL 14, 2006 10:16 AM RAW
 I:\AIRPORTS\QUINCY\82406TCD\AIRPORT\PROJECTSHEETS\R-003SEY.DWG - SAFETY

EROSION CONTROL BLANKET NOTES

AN EROSION CONTROL MATERIAL (EXCELSIOR BLANKET) WILL BE INSTALLED AT THE LOCATIONS SHOWN ON THIS SHEET AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM AR156531 "EROSION CONTROL BLANKET" _____ 1,758 S.Y.

COMPACTION NOTES

THE SUBGRADE COMPACTION REQUIREMENT FOR PROPOSED TAXIWAY "C" WILL ADHERE TO ITEM AR152 FOR AIRCRAFT GREATER THAN 60,000 LBS.

THE ENTIRE THICKNESS OF THE TREATED SUBGRADE SHALL BE COMPACTED TO A DENSITY OF NOT LESS THAN 95% OF THE STANDARD DRY DENSITY. THE STANDARD DRY DENSITY OF THE LIME-MODIFIED SOIL SHALL BE DETERMINED BY ASTM D1557 FOR AIRCRAFT 60,000 LBS OR MORE. DETERMINATION OF OPTIMUM MOISTURE CONTENT AND STANDARD DRY DENSITY OF LIME MODIFIED SOIL IS THE CONTRACTOR'S RESPONSIBILITY.

PAVEMENT REMOVAL NOTES

THE AREA DESIGNATED AS [Hatched Pattern] ON THIS SHEET WILL HAVE THE EXISTING PAVEMENT AND AGGREGATE BASE REMOVED (FULL DEPTH) TO A SUITABLE SUBGRADE. ALL REMOVED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE IN A LEGAL MANNER.

WHERE THE PROPOSED REMOVAL AREA ABUTS THE EXISTING PAVEMENT, THE PAVEMENT WILL BE SAWS AS SHOWN ON THIS SHEET. THE SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ANY ADJACENT PAVEMENT DAMAGED BY THE PAVEMENT REMOVAL OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

THE PAVEMENT REMOVAL AREA WILL CONSIST OF 3" (AVERAGE) OF BITUMINOUS SURFACE COURSE ON THE MAJORITY OF TAXIWAY C, AND POTENTIALLY MORE AT THE INTERSECTION OF EXISTING TAXIWAY C AND RUNWAY 13-31 DUE TO PAST BUTT JOINT CONSTRUCTION FOR OVERLAYS OF RUNWAY 13-31, 9" OF CONCRETE, AND 7" OF AGGREGATE BASE.

6" OF THE EXISTING EDGE OF PAVEMENT IN THE AREA OF THE BITUMINOUS WIDENING WILL BE REMOVED TO FORM A UNIFORM PAVING SURFACE.

SEE SHEET 9 FOR UNDERDRAIN REMOVAL.

THE REMOVAL OF PAVEMENT WILL BE PAID FOR UNDER ITEM: AR401900 "REMOVE BITUMINOUS PAVEMENT" _____ 3,395 S.Y.

AR401655 BUTT JOINT CONSTRUCTION NOTES

THE PROPOSED BUTT JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ITEM AR401655 "BUTT JOINT CONSTRUCTION" AS STATED ON PAGE 295 OF THE SUPPLEMENTAL SPECIFICATION AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THIS ITEM SHALL CONSIST OF REMOVING THE EXISTING BITUMINOUS PAVEMENT SURFACE BY MILLING IN ORDER TO CONSTRUCT A BUTT JOINT FOR THE TRANSITION OF THE PROPOSED BITUMINOUS OVERLAY AT THE LOCATIONS AS SHOWN ON THE PLANS.

THE PAVEMENT SURFACE WILL BE MILLED TO A DEPTH OF 1-1/2" AT THE BITUMINOUS BUTT END AND WILL TAPER IN THE DIRECTION OF THE PCC UNTIL A MINIMUM DEPTH OF PROPOSED BITUMINOUS PAVEMENT IS ACHIEVABLE. THE BUTT JOINT AT THE PCC END WILL MATCH THE PROPOSED GRADE OF THE PCC PAVEMENT. THE PLANER MUST BE CAPABLE OF MILLING THE SURFACE TO THE DESIRED ELEVATIONS AND GRADES.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

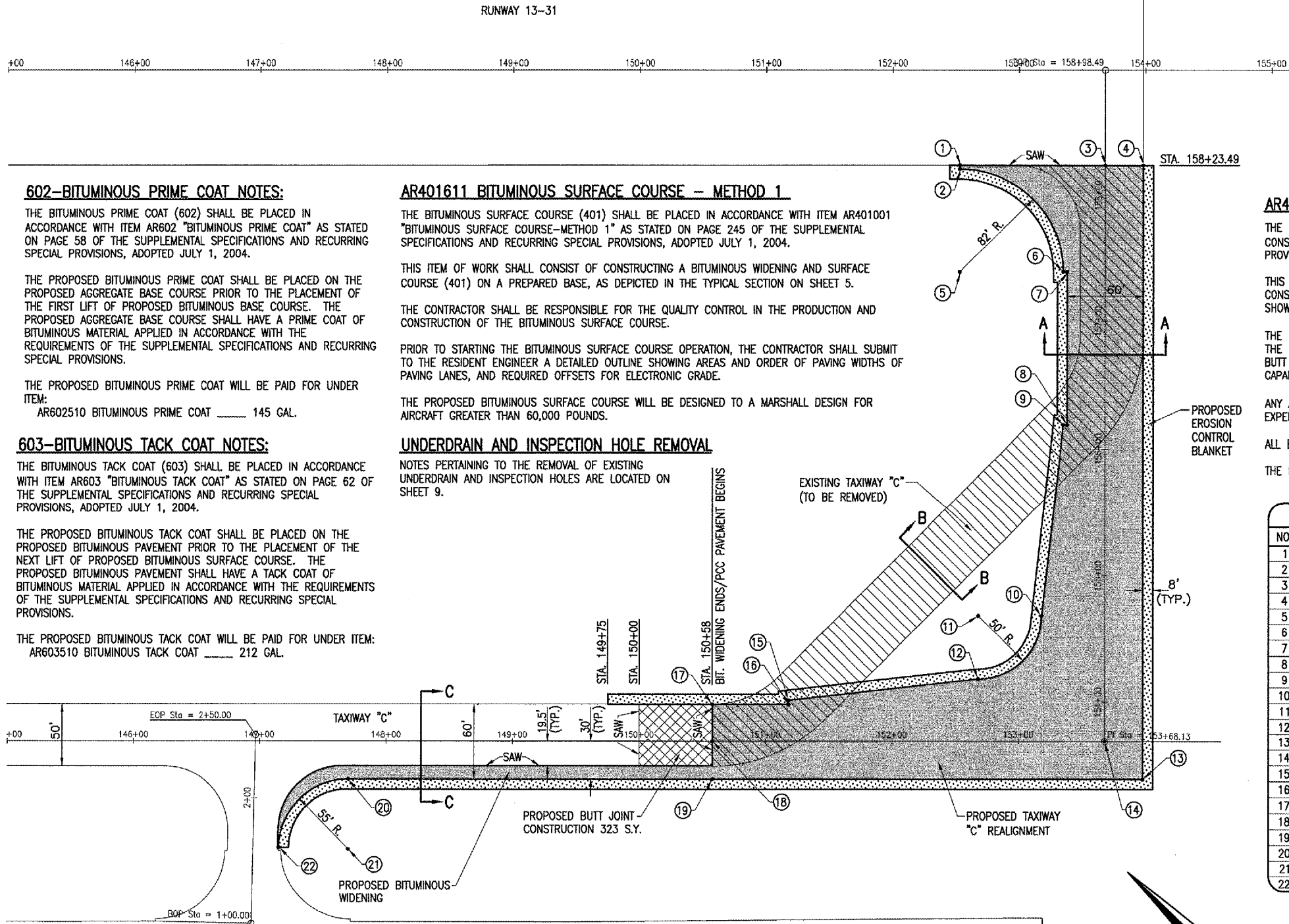
ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

THE PAVEMENT MILLING WILL BE PAID FOR UNDER ITEM: AR401655 "BUTT JOINT CONSTRUCTION" _____ 323 S.Y.

POINT DATA TABLE					
NO.	DESCRIPTION	STATION	OFFSET	NORTHING	EASTING
1	PAVEMENT EDGE	158+23.49	115' LT.	1,191,988.1047	2,010,315.7536
2	RADIUS RETURN	158+20.49	115' LT.	1,191,986.0197	2,010,313.5966
3	CENTERLINE OF TAXIWAY "C"	158+23.49	0'	1,191,905.4016	2,010,395.6947
4	PAVEMENT EDGE	158+23.49	30' RT.	1,191,883.8313	2,010,416.5447
5	CENTER OF RADIUS	157+38.46	115' LT.	1,191,929.0298	2,010,254.6377
6	RADIUS RETURN	157+38.46	33' LT.	1,191,870.0552	2,010,311.6114
7	PAVEMENT EDGE	157+38.46	30' LT.	1,191,867.8976	2,010,313.6958
8	PAVEMENT EDGE	156+18.13	30' LT.	1,191,784.2921	2,010,227.1541
9	PAVEMENT EDGE	156+18.13	33' LT.	1,191,786.4497	2,010,225.0697
10	RADIUS RETURN	154+68.13	50' LT.	1,191,694.4559	2,010,105.3778
11	CENTER OF RADIUS	154+68.13	100' LT.	1,191,730.4160	2,010,070.6378
12	RADIUS RETURN	152+68.13	50' LT.	1,191,695.6760	2,010,034.6777
13	PAVEMENT EDGE	-	30' RT.	1,191,546.5956	2,010,067.4657
14	CENTERLINE OF TAXIWAY "C"	153+68.13	0'	1,191,589.0157	2,010,068.1977
15	PAVEMENT EDGE	151+18.13	33' LT.	1,191,791.7446	2,009,918.2311
16	PAVEMENT EDGE	151+18.13	30' LT.	1,191,789.6602	2,009,916.0735
17	PAVEMENT EDGE	150+58.00	30' LT.	1,191,832.9049	2,009,874.2960
18	CENTERLINE OF TAXIWAY "C"	150+58.00	0'	1,191,812.0809	2,009,852.7199
19	PAVEMENT EDGE	150+58.00	30' RT.	1,191,791.1400	2,009,831.2181
20	RADIUS RETURN	147+69.93	30' RT.	1,191,998.3961	2,009,630.9937
21	CENTER OF RADIUS	147+69.93	85' RT.	1,191,960.1820	2,009,591.4376
22	RADIUS RETURN	147+14.96	84' RT.	1,192,000.5689	2,009,554.1217

LEGEND

- [Solid Line] EXISTING PAVEMENT
- [Hatched Pattern] EXISTING PAVEMENT (TO BE REMOVED)
- [Dotted Pattern] PROPOSED IMPROVEMENTS
- [Cross-hatched Pattern] PROPOSED EROSION CONTROL BLANKET
- [X-hatched Pattern] PROPOSED BUTT JOINT CONSTRUCTION



602-BITUMINOUS PRIME COAT NOTES:

THE BITUMINOUS PRIME COAT (602) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR602 "BITUMINOUS PRIME COAT" AS STATED ON PAGE 58 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE PROPOSED BITUMINOUS PRIME COAT SHALL BE PLACED ON THE PROPOSED AGGREGATE BASE COURSE PRIOR TO THE PLACEMENT OF THE FIRST LIFT OF PROPOSED BITUMINOUS BASE COURSE. THE PROPOSED AGGREGATE BASE COURSE SHALL HAVE A PRIME COAT OF BITUMINOUS MATERIAL APPLIED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.

THE PROPOSED BITUMINOUS PRIME COAT WILL BE PAID FOR UNDER ITEM: AR602510 BITUMINOUS PRIME COAT _____ 145 GAL.

603-BITUMINOUS TACK COAT NOTES:

THE BITUMINOUS TACK COAT (603) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR603 "BITUMINOUS TACK COAT" AS STATED ON PAGE 62 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE PROPOSED BITUMINOUS TACK COAT SHALL BE PLACED ON THE PROPOSED BITUMINOUS PAVEMENT PRIOR TO THE PLACEMENT OF THE NEXT LIFT OF PROPOSED BITUMINOUS SURFACE COURSE. THE PROPOSED BITUMINOUS PAVEMENT SHALL HAVE A TACK COAT OF BITUMINOUS MATERIAL APPLIED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.

THE PROPOSED BITUMINOUS TACK COAT WILL BE PAID FOR UNDER ITEM: AR603510 BITUMINOUS TACK COAT _____ 212 GAL.

AR401611 BITUMINOUS SURFACE COURSE - METHOD 1

THE BITUMINOUS SURFACE COURSE (401) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR401001 "BITUMINOUS SURFACE COURSE-METHOD 1" AS STATED ON PAGE 245 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THIS ITEM OF WORK SHALL CONSIST OF CONSTRUCTING A BITUMINOUS WIDENING AND SURFACE COURSE (401) ON A PREPARED BASE, AS DEPICTED IN THE TYPICAL SECTION ON SHEET 5.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE QUALITY CONTROL IN THE PRODUCTION AND CONSTRUCTION OF THE BITUMINOUS SURFACE COURSE.

PRIOR TO STARTING THE BITUMINOUS SURFACE COURSE OPERATION, THE CONTRACTOR SHALL SUBMIT TO THE RESIDENT ENGINEER A DETAILED OUTLINE SHOWING AREAS AND ORDER OF PAVING WIDTHS OF PAVING LANES, AND REQUIRED OFFSETS FOR ELECTRONIC GRADE.

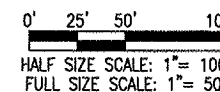
THE PROPOSED BITUMINOUS SURFACE COURSE WILL BE DESIGNED TO A MARSHALL DESIGN FOR AIRCRAFT GREATER THAN 60,000 POUNDS.

UNDERDRAIN AND INSPECTION HOLE REMOVAL

NOTES PERTAINING TO THE REMOVAL OF EXISTING UNDERDRAIN AND INSPECTION HOLES ARE LOCATED ON SHEET 9.

EXCAVATION NOTES

APPROXIMATELY 1,552 CY OF CUT MATERIAL IS ANTICIPATED TO BE GENERATED FOLLOWING THE EXCAVATION OF TAXIWAY C. SEE SHEET 5 FOR AN EARTHWORK DISTRIBUTION TABLE. THIS AMOUNT DOES NOT INCLUDE THE VOLUME OF MATERIAL REMOVED AS PART OF AR401900 - BITUMINOUS PAVEMENT REMOVAL. THE VOLUME OF PAVEMENT AND AGGREGATE BASE TO BE REMOVED IS ASSUMED TO BE THE DEPTH OF THE MATERIAL OVER THE SURFACE AREA TO BE REMOVED AND MAY SLIGHTLY EXCEED THE QUANTITY ASSUMED DEPENDING ON THE PAVEMENT STRUCTURE ENCOUNTERED DURING THE REMOVAL. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR GREATER REMOVAL VOLUMES DUE TO WIDTHS OF SUBSURFACE MATERIAL BEING WIDER THAN THOSE ASSUMED. CUT MATERIAL WILL BE USED TO BACKFILL THOSE AREAS WHERE PAVEMENT REMOVAL OCCURS AND IS ANTICIPATED TO BALANCE FOLLOWING THE SPECIFIED COMPACTION, BASED ON THE PREVIOUSLY STATED ASSUMPTION. ANY SURPLUS CUT MATERIAL WILL BE UNIFORMLY SPREAD AND GRADED TO DRAIN WITHIN THE PROPOSED GRADING LIMITS AND OUTSIDE THE PROPOSED SWALE FLOWLINES. NO OFFSITE HAUL OF MATERIAL, OTHER THAN THE PAVEMENT TO BE REMOVED, IS ANTICIPATED.



Q1055

REVISION	DATE

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS

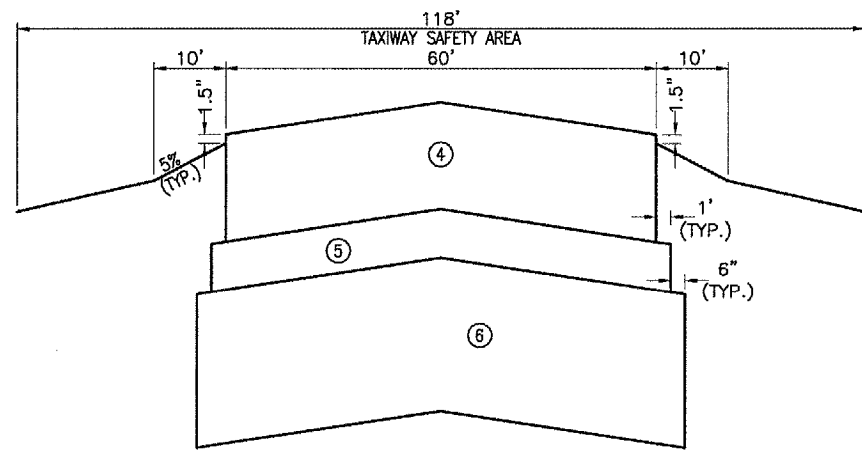
IL PROJ.: UIN-3338
A.I.P. PROJ.: 3-17-0085-XX

DATE	BY	REVIEWED
06/19/06	RAW	06/19/06
06/19/06	BAK	06/19/06
07/12/06	RAW	07/12/06

HANSON
Hanson Professional Services Inc.
1525 South Sixth Street
Springfield, Illinois 62705-2688
Chicago, Illinois

REALIGN AND WIDEN TAXIWAY "C"
PROPOSED CONSTRUCTION PLAN

JUL 14 2006 10:21 AM RAW I:\AIRPORTS\QUINCY\82406TCD\AIRPORT\PROJECTS\R-12\CON.DWG - CONSTRUCTION



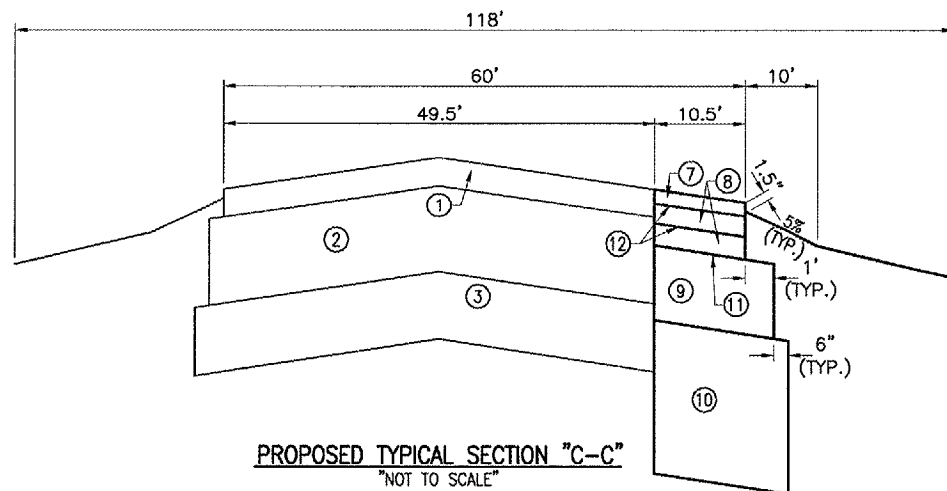
PROPOSED TYPICAL SECTION "A-A"
"NOT TO SCALE"

LEGEND FOR TYPICAL SECTIONS "A-A", "B-B" & "C-C"

- ① EXISTING 401 BITUMINOUS SURFACE COURSE (3" AVG. DEPTH)
- ② EXISTING 501 PCC PAVEMENT (9" DEPTH)
- ③ EXISTING 209 CRUSHED AGGREGATE BASE COURSE (7" DEPTH)
- ④ PROPOSED 501 PCC PAVEMENT (11" DEPTH)
- ⑤ PROPOSED 209 AGGREGATE BASE (5" DEPTH)
- ⑥ PROPOSED 155 LIME MODIFIED SUBGRADE (16" DEPTH)
- ⑦ PROPOSED 401 BITUMINOUS SURFACE COURSE (1-1/2" DEPTH, COMPACTED)
- ⑧ PROPOSED 401 BITUMINOUS BASE COURSE (2-1/4" DEPTH, COMPACTED)
- ⑨ PROPOSED 209 CRUSHED AGGREGATE BASE COURSE (8" DEPTH)
- ⑩ PROPOSED 155 LIME MODIFIED SUBGRADE (16" DEPTH)
- ⑪ PROPOSED 602 BITUMINOUS PRIME COAT (0.35 GAL./S.Y.)
- ⑫ PROPOSED 603 BITUMINOUS TACK COAT (0.05 GAL./S.Y., MIN.)*

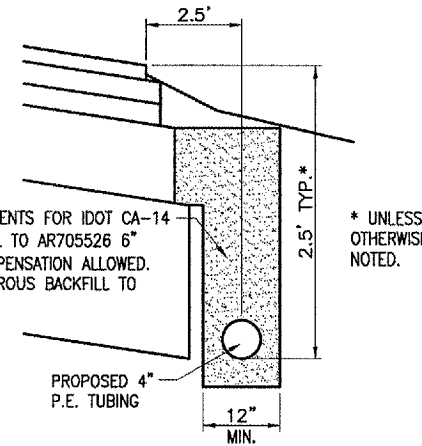
NOTE:

* QUANTITY BASED ON RATE OF 0.15 GAL./SY.



PROPOSED TYPICAL SECTION "C-C"
"NOT TO SCALE"

POROUS BACKFILL SHALL CONFORM TO THE REQUIREMENTS FOR IDOT CA-14 OR IDOT CA-16 AND WILL BE CONSIDERED INCIDENTAL TO AR705526 6" PERF. UNDERDRAIN W/SOCK AND NO ADDITIONAL COMPENSATION ALLOWED. CONTRACTOR SHALL PLACE AND CONSOLIDATE THE POROUS BACKFILL TO THE SATISFACTION OF THE RESIDENT ENGINEER.



UNDERDRAIN DETAIL
"NOT TO SCALE"

SOIL PROCESSING:

THE STANDARD DRY DENSITY OF THE LIME-MODIFIED SOIL SHALL BE DETERMINED BY ASTM D1557 FOR AIRCRAFT 60,000 LBS OR MORE. DETERMINATION OF OPTIMUM MOISTURE CONTENT AND STANDARD DRY DENSITY OF LIME MODIFIED SOIL IS THE CONTRACTOR'S RESPONSIBILITY.

LIME MODIFIED SOIL PROCESSING SHALL CONSIST OF CONSTRUCTING A 16 INCH COURSE OF A MIXTURE OF SOIL, LIME, AND WATER IN ACCORDANCE WITH THE RATES AND METHODS PROVIDED.

THE BY-PRODUCT LIME QUANTITY WAS CALCULATED AT 6% OF THE DRY SOIL WEIGHT (120 LBS/CF) MAXIMUM DENSITY. THE ACTUAL AMOUNT SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE RESIDENT ENGINEER PRIOR TO CONSTRUCTION.

THE SUBGRADE SHALL BE CUT AND WITHIN GRADE TOLERANCE PRIOR TO LIME MODIFICATION. THE CONTRACTOR SHALL PROCESS THE SOIL TO 1-1/2' OUTSIDE THE PROPOSED PAVEMENT SURFACE ON ALL SIDES.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ACCOUNT FOR ANY "FLUFF" OR "SWELL" IN THE MODIFIED SUBGRADE.

ANY SWELL SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. NO ADDITIONAL PAYMENT WILL BE MADE FOR REMOVAL OF SWELL.

ONCE THE SPECIFIED DENSITY OF THE LIME-SOIL MIXTURE IS ACHIEVED, THE CONTRACTOR MAY START FINAL TRIMMING OPERATIONS AND PLACEMENT OF THE OVERLYING COURSE IF THE COMPACTED LIME MODIFIED LAYER IS NOT RUTTED OR DISTORTED BY THE EQUIPMENT.

THE CONTRACTOR SHALL KEEP THE LIME-SOIL MIXTURE MOIST CURED FOR A MINIMUM OF SEVEN (7) DAYS BY WATERING OR BY PLACEMENT OF THE OVERLYING COURSE.

PCC PAVING NOTE:

THE PROPOSED PCC CONCRETE SHALL COMPLY WITH ITEM AR501002 "PORTLAND CEMENT CONCRETE PAVEMENT METHOD II", DESIGNED FOR AIRCRAFT WEIGHT GREATER THAN 60,000 LBS.

AR209-CRUSHED AGGREGATE BASE COURSE NOTES

THE CRUSHED AGGREGATE BASE COURSE (209) SHALL BE PLACED IN ACCORDANCE WITH ITEM 209 "CRUSHED AGGREGATE BASE COURSE" AS STATED ON PAGE 45 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE CRUSHED AGGREGATE BASE COURSE MATERIAL (CA-6) WILL BE USED TO CONSTRUCT A BASE COURSE FOR THE PROPOSED PAVEMENTS. THE CRUSHED AGGREGATE BASE COURSE MATERIAL WILL BE CONSTRUCTED TO THE DEPTHS SHOWN IN THE TYPICAL SECTIONS AND COMPACTED TO NOT LESS THAN 95 PERCENT DENSITY.

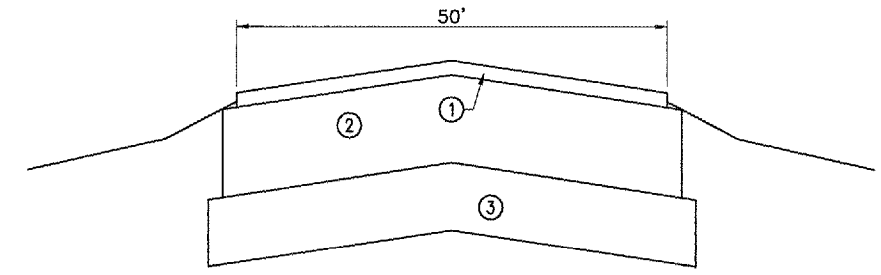
THE PROPOSED AGGREGATE FOR THE BASE COURSE MATERIAL SHALL MEET THE REQUIREMENTS OF (GRADATION "B") IN TABLE 1 OF THE SUPPLEMENTAL SPECIFICATIONS.

PROVISIONS SHALL BE MADE BY THE CONTRACTOR FOR FURNISHING WATER AT THE PLANT AND AT THE SITE OF THE WORK BY EQUIPMENT OF AMPLE CAPACITY AND OF SUCH DESIGN AS TO ASSURE UNIFORM MIXING AND APPLICATION.

THE CONTRACTOR WILL PROVIDE THE RESIDENT ENGINEER A PROCTOR CURVE SHOWING OPTIMUM DENSITY AND MOISTURE FOR THE SUPPLIED BASE COURSE MATERIAL.

THE COMPACTION CONTROL TEST TO BE USED SHALL BE FOR AIRCRAFT GREATER THAN 60,000 LBS.

IF AT THE TIME THE AGGREGATES ARE WEIGHED THEY CONTAIN MORE THAN SIX (6) PERCENT OF ABSORBED AND FREE MOISTURE BY WEIGHT, A DEDUCTION FOR THE MOISTURE IN EXCESS OF THIS AMOUNT SHALL BE MADE IN DETERMINING THE PAY QUANTITY.



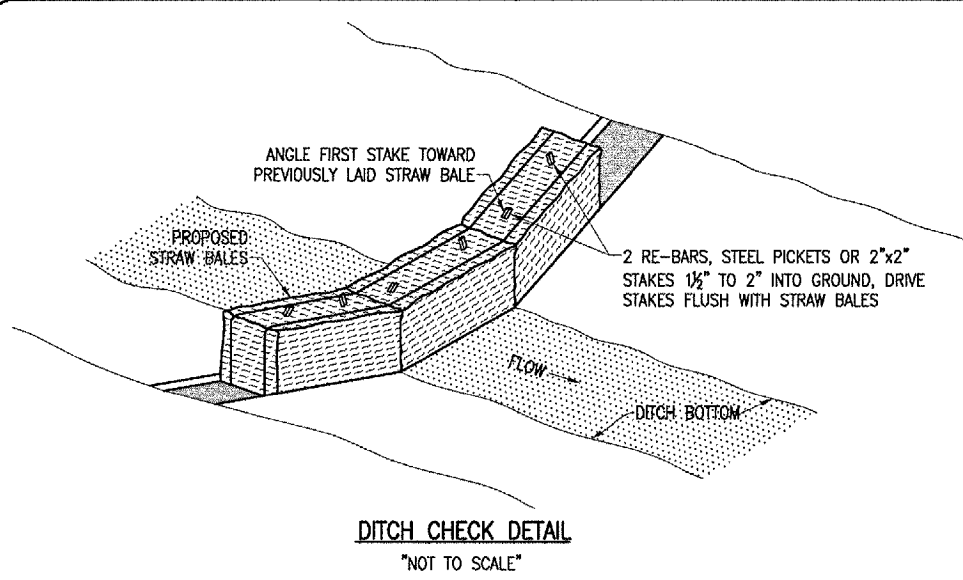
EXISTING TYPICAL SECTION "B-B"
"NOT TO SCALE"

STATION	AREA (SQ. FEET)		VOLUME (CUBIC YARDS)		CUMULATIVE COLUMNS (CUBIC YARDS)	
	CUT	FILL	CUT	FILL	CUT	FILL
145+47.00	0.00	0.00	0.00	0.00	0.00	0.00
146+00.00	1.10	0.00	1.08	0.00	1.08	0.00
147+00.00	2.10	0.00	5.93	0.00	7.01	0.00
147+14.95	20.20	0.00	6.17	0.00	13.18	0.00
147+50.00	17.60	0.20	24.53	0.13	37.71	0.13
147+73.00	21.30	0.00	16.57	0.09	54.28	0.21
148+00.00	18.10	0.30	19.70	0.15	73.98	0.36
148+50.00	15.90	0.60	31.48	0.83	105.46	1.20
149+00.00	21.90	0.90	35.00	1.39	140.46	2.59
149+50.00	21.50	1.00	40.19	1.76	180.65	4.35
150+00.00	26.80	4.30	44.72	4.91	225.37	9.25
150+15.00	53.20	3.00	22.22	2.03	247.59	11.28
150+50.00	45.20	23.50	63.78	17.18	311.37	28.46
150+58.00	41.20	52.10	12.80	11.20	324.17	39.66
151+00.00	50.70	60.90	71.48	87.89	395.65	127.55
151+18.13	63.80	59.70	38.44	40.49	434.09	168.04
151+50.00	67.50	51.20	77.49	65.45	511.58	233.49
152+00.00	76.30	57.60	133.15	100.74	644.73	334.23
152+04.00	64.20	48.40	10.41	7.85	655.14	342.08
152+68.13	31.30	40.90	113.42	106.05	768.55	448.13
153+38.13	2.40	51.60	43.69	119.91	812.24	568.04
153+68.13	3.20	48.20	3.11	55.44	815.35	623.48
153+98.13	16.30	24.40	10.83	40.33	826.18	663.82
154+68.13	120.10	13.80	176.81	49.52	1,003.00	713.34
155+00.00	62.30	21.20	107.65	20.66	1,110.65	733.99
155+50.00	72.30	39.30	124.63	56.02	1,235.28	790.01
156+00.00	82.80	59.00	143.61	91.02	1,378.89	881.03
156+18.13	39.20	53.80	40.96	37.87	1,419.85	918.90
156+50.00	8.70	37.10	28.27	53.65	1,448.12	972.55
157+00.00	9.60	30.10	16.94	62.22	1,465.12	1,034.77
157+38.46	13.40	25.60	16.38	39.67	1,481.44	1,074.44
157+50.00	0.60	19.90	2.99	9.72	1,484.44	1,084.17
158+00.00	29.40	35.40	37.78	51.20	1,512.21	1,135.37
158+23.49	60.20	45.80	38.98	35.32	1,551.19	1,170.69
TOTAL:					1,551.19	1,170.69

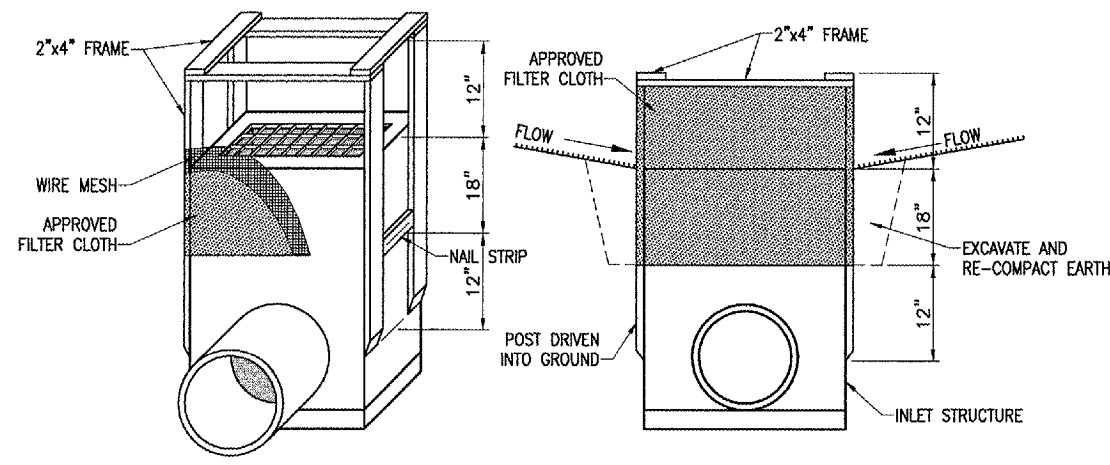
NOTE:

THE CUT VOLUME SHOWN IS EARTHWORK ONLY AND DOES NOT INCLUDE THE VOLUME OF PAVEMENT AND AGGREGATE BASE REMOVED. THE FILL VOLUME SHOWN INCLUDES THE FILL REQUIRED TO BACKFILL THE VOID CREATED BY THE REMOVAL OF THE EXISTING PAVEMENT AND AGGREGATE BASE.

BY		REVISION		DATE	
<p style="text-align: center; margin: 0;">QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS</p>					
<p style="text-align: center; margin: 0;">Hanson Professional Services Inc. 1400 S. Main Street Springfield, Illinois 62703-2886 Offices Nationwide</p>					
<p style="margin: 0;">REALIGN AND WIDEN TAXIWAY "C"</p> <p style="margin: 0;">TYPICAL SECTIONS, PAVING NOTES, AND EARTHWORK DISTRIBUTION</p>					
5					
6 of 24 sheets					



DITCH CHECK DETAIL
"NOT TO SCALE"



INLET PROTECTION DETAIL
"NOT TO SCALE"

RUNWAY 13-31

EROSION CONTROL NOTES

ALL EROSION CONTROL MEASURES WILL BE PLACED PRIOR TO ANY DISRUPTION OF THE EXISTING SOD AREAS OR THE PLACEMENT OF TOPSOIL FROM OFF SITE. THE CONTRACTOR WILL REMOVE THE INLET PROTECTION AND DITCH CHECKS ONCE A STAND OF GRASS IS ESTABLISHED AND ACCEPTED BY THE RESIDENT ENGINEER. REMOVAL OF THE EROSION CONTROL MEASURES WILL BE INCIDENTAL TO THEIR RESPECTIVE PAY ITEMS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL PROPOSED EROSION CONTROL MEASURES SHALL BE COMPLETED AS DETAILED ON THIS EROSION CONTROL PLAN AND IN ACCORDANCE WITH THE SPECIFICATIONS.

EROSION CONTROL MEASURES ARE GOVERNED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY'S STANDARDS AND SPECIFICATIONS FOR SOIL EROSIONS AND SPECIFICATION FOR SOIL EROSIONS AND SEDIMENT CONTROL.

SILT FENCE WILL BE USED FOR INLET PROTECTION AS SHOWN ON THIS SHEET.

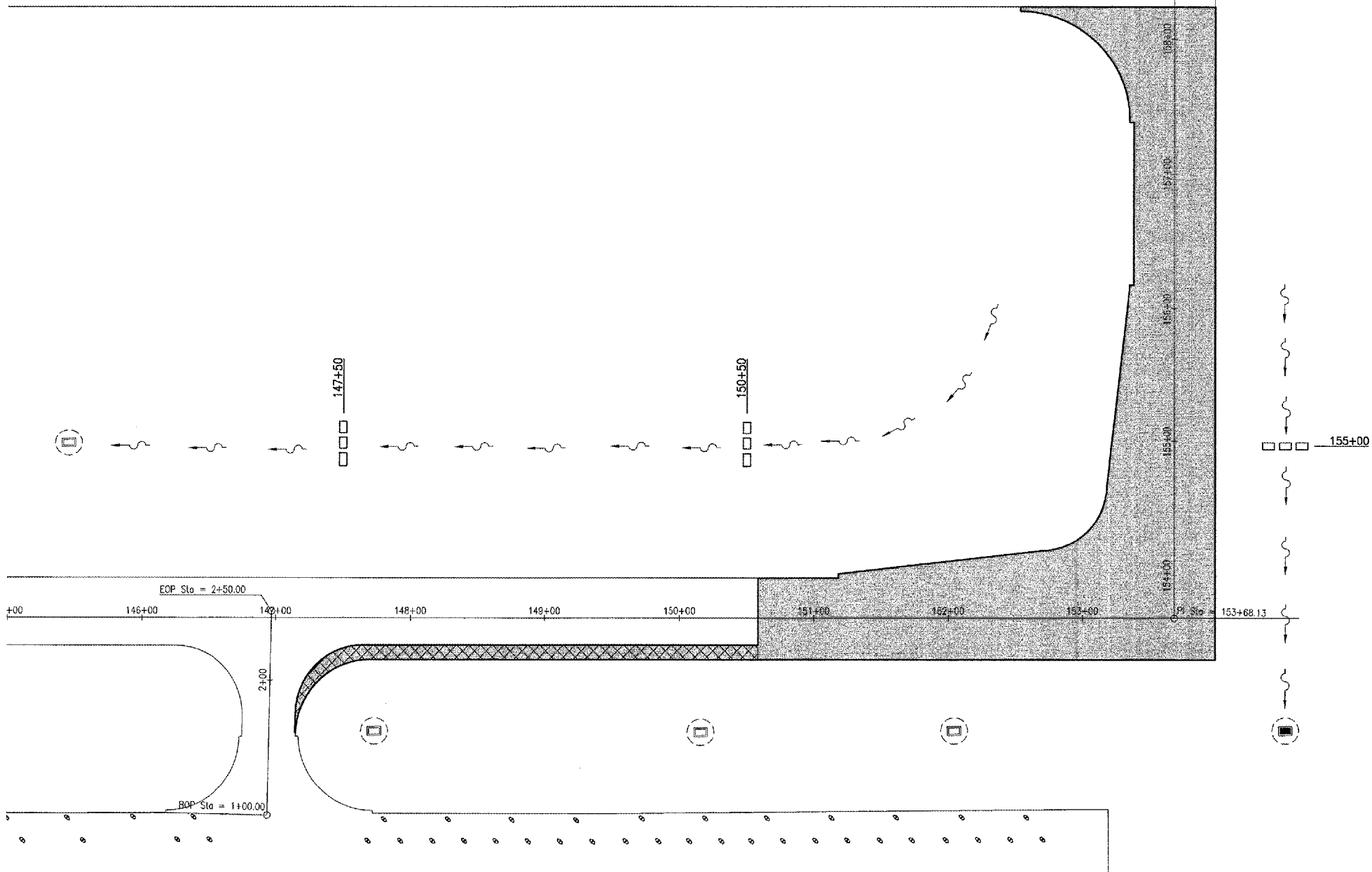
THE PROPOSED INLET PROTECTION WILL BE PLACED AT THE LOCATIONS SHOWN ON THIS SHEET.

INSPECTION OF THE INLET PROTECTION SHALL BE FREQUENT AND REPAIR/REPLACEMENT SHALL BE MADE PROMPTLY, AS NEEDED. ANY REPAIR/REPLACEMENT MADE WILL BE CONSIDERED INCIDENTAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

INLET PROTECTION IS TO INCLUDE ALL INCIDENTALS NECESSARY TO CONSTRUCT THESE ITEMS AND WILL BE PAID FOR UNDER ITEM:
AR156520 "INLET PROTECTION" - PER EACH

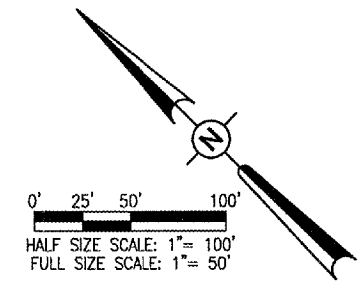
THE PROPOSED DITCH CHECKS WILL BE CONSTRUCTED AS SHOWN ON THIS SHEET AND SHALL CONSIST OF A MINIMUM OF 3 BALES PER LOCATION. INSPECTION OF THE DITCH CHECKS SHALL BE FREQUENT AND REPAIR/REPLACEMENT SHALL BE MADE PROMPTLY, AS NEEDED. ANY REPAIR/REPLACEMENT MADE WILL BE CONSIDERED INCIDENTAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE DITCH CHECKS WILL INCLUDE ALL INCIDENTALS NECESSARY TO COMPLETE THE ITEM AND WILL BE PAID FOR UNDER ITEM:
AR156511 "DITCH CHECK" - PER EACH



EROSION CONTROL QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	QUANTITY	AS BUILT QUANTITY
AR156511	DITCH CHECK	EA.	3	
AR156520	INLET PROTECTION	EA.	5	

- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED PCC IMPROVEMENTS
 - PROPOSED BITUMINOUS WIDENING
 - PROPOSED INLET PROTECTION
 - PROPOSED INLET
 - EXISTING INLET
 - DITCH CHECK
 - SWALE FLOWLINE

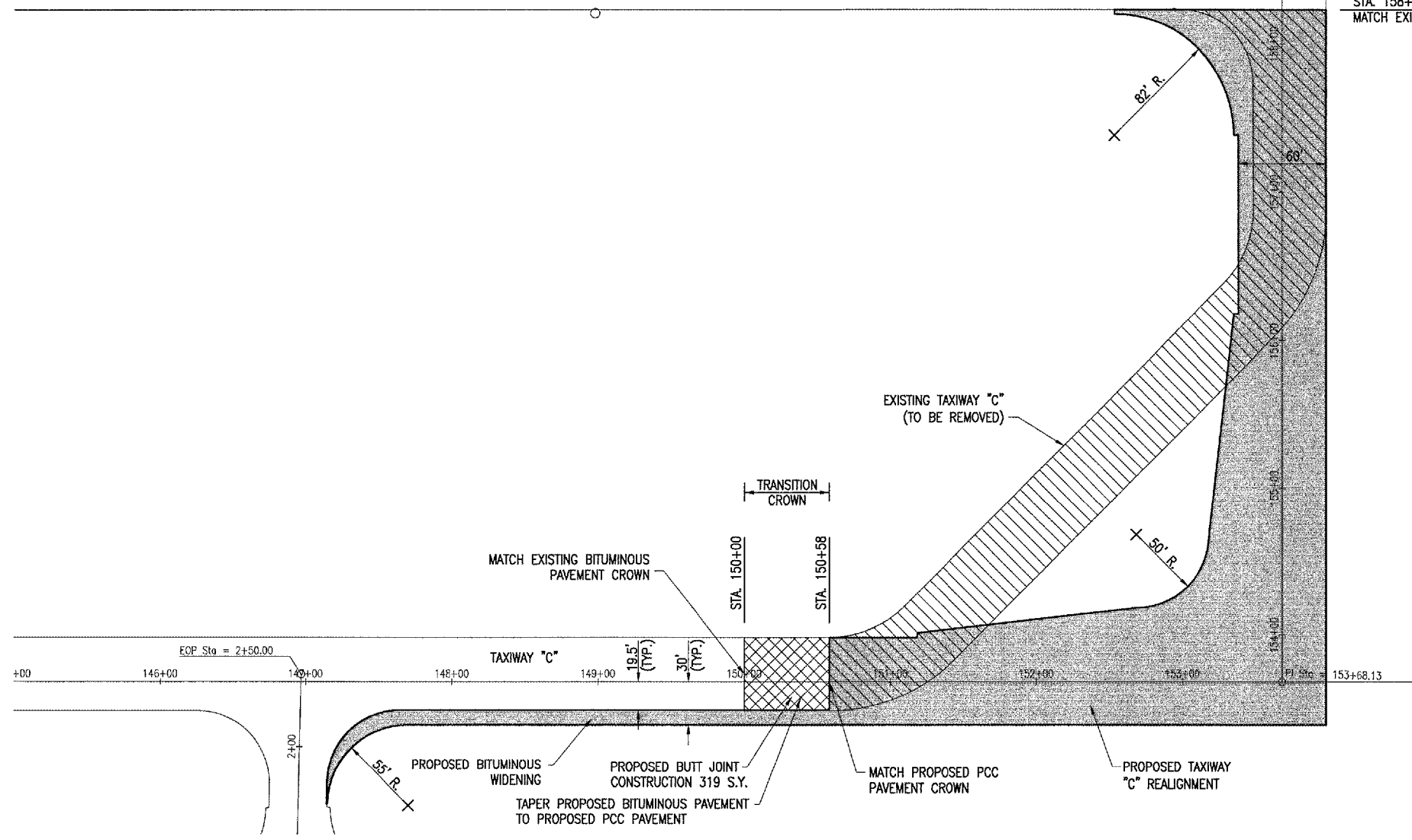


REVISION DATE					
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS					
A.I.P. PROJ.: 3-17-0085-XX IL PROJ.: UIN-3338					
REL Project No.: 82406TYCD_0240 Drawing: R-181SWP.DWG Scale: 1" = 50' Date: 06/19/06					
LAYOUT		RAW		06/19/06	
DRAWN		BAK		06/19/06	
REVIEWED		RAW		07/12/06	
Hanson Professional Services Inc. 1026 South State Street Springfield, Illinois 62768					
REALIGN AND WIDEN TAXIWAY "C"			PROPOSED STORMWATER POLLUTION PREVENTION PLAN		
6 of 24 sheets					

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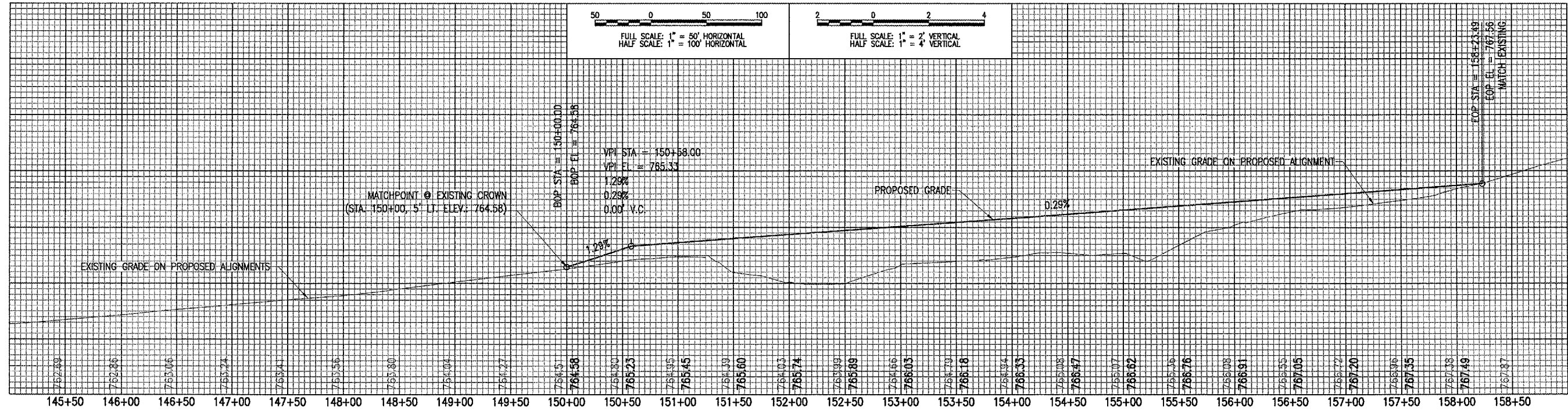
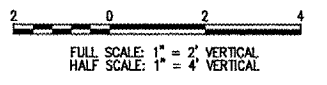
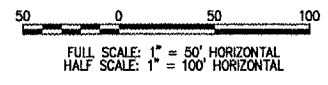
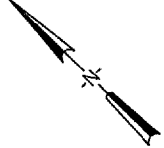
RUNWAY 13-31

STA 158+23.49
MATCH EXISTING



LEGEND

- EXISTING PAVEMENT
- EXISTING PAVEMENT (TO BE REMOVED)
- PROPOSED IMPROVEMENTS
- PROPOSED BUTT JOINT CONSTRUCTION



DATE	REVISION	BY

**QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS**

QUINCY Regional Airport
1700 E. Adams St.
Quincy, IL 62450

IL PROJ.: UIN-3338 A.I.P. PROJ.: 3-17-0085-XX

FILE NO.	DATE	LAYOUT	RAW	BAK	RAW
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R-701RNP-DWG	06/19/06	RAW	06/19/06	06/19/06	06/19/06
Scale: 1" = 50'H & 1" = 2'V	06/19/06	RAW	06/19/06	06/19/06	06/19/06
Date: 06/19/06	06/19/06	RAW	06/19/06	06/19/06	06/19/06



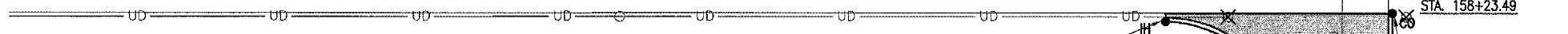
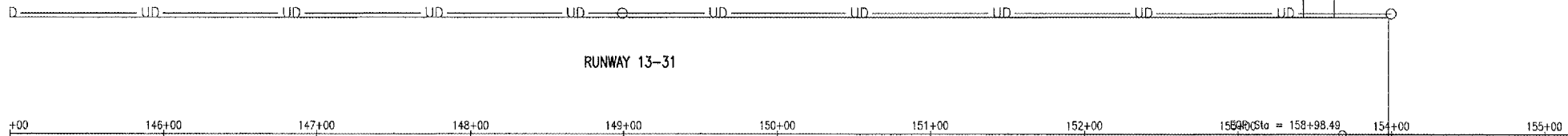
REALIGN AND WIDEN TAXIWAY "C"

PROPOSED
PLAN AND PROFILE

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FOR UNDERDRAIN DETAILS
SEE SHEETS 5 AND 10.

Q1055



REMOVE UNDERDRAIN AND INSPECTION HOLE NOTE

EXISTING UNDERDRAIN AND INSPECTION HOLES EXIST ADJACENT TO THE PAVEMENT PROPOSED TO BE REMOVED. THE UNDERDRAIN AND INSPECTION HOLES, IN THEIR ENTIRETY, WILL BE REMOVED AND DISPOSED OF OFF SITE BY THE CONTRACTOR IN A LEGAL MANNER. CARE SHOULD BE TAKEN NOT TO DAMAGE THE EXISTING UNDERDRAIN (TO BE REUSED) WHEN REMOVING THE UNDERDRAIN AND INSPECTION HOLES. ANY DAMAGE TO EXISTING UNDERDRAIN TO BE REUSED WILL BE REPLACED AT THE COST OF THE CONTRACTOR.

THIS ITEM WILL BE PAID FOR AT THE CONTRACT UNIT PRICE BID PER LINEAL FOOT OF UNDERDRAIN REMOVED. THE REMOVAL OF THE INSPECTION HOLES WILL BE CONSIDERED INCIDENTAL TO THE REMOVAL OF THE UNDERDRAIN AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THIS PRICE WILL CONSTITUTE FULL COMPENSATION FOR THE REMOVAL AND DISPOSAL OF THE UNDERDRAIN AND INSPECTION HOLES AND EXCAVATING AND BACKFILLING FOR THE REMOVAL IN ACCORDANCE TO ITEM 705.

THE REMOVAL OF THE UNDERDRAIN AND INSPECTION HOLES WILL BE PAID FOR UNDER ITEM:
AR705900 - REMOVE UNDERDRAIN, PER L.F.

LEGEND

- EXISTING PAVEMENT
- PROPOSED IMPROVEMENTS
- EXISTING STORM SEWER
- EXISTING UNDERDRAINS
- EXISTING INLET
- EXISTING UNDERDRAIN CLEANOUT
- EXISTING UNDERDRAIN CLEANOUT (TO BE REMOVED)
- PROPOSED STORM SEWER
- PROPOSED PERFORATED UNDERDRAIN
- PROPOSED INLET
- PROPOSED UNDERDRAIN CLEANOUT
- PROPOSED INSPECTION HOLE

PROPOSED INSPECTION HOLE #1
TYPE A
TXY "C" STA. 158+18, 115' LT.
TOP ELEV. 767.10
FLOWLINE ELEV. 764.65
TIE TO EXISTING - INCIDENTAL TO
ITEM AR705630 - UNDERDRAIN INSPECTION
HOLE

PROPOSED CLEANOUT #1
TYPE B
TXY "C" STA. 158+23.49, 32.5' RT.
TOP ELEV. 767.52
FLOWLINE ELEV. 765.07

PROPOSED INSPECTION HOLE #2
TYPE A
TXY "C" STA. 156+28, 32.5' LT.
TOP ELEV. 766.49
FLOWLINE ELEV. 764.04

PROPOSED INSPECTION HOLE #4
TYPE A
TXY "C" STA. 150+58, 32.5' LT.
TOP ELEV. 764.83
FLOWLINE ELEV. 780.98 (MATCH EXISTING)

PROPOSED CLEANOUT #2
TYPE B
TXY "C" STA. 146+25, 22.5' RT.
TOP ELEV. 762.64
EXISTING INVERT

PROPOSED INSPECTION HOLE #6
TYPE A
TXY "C" STA. 150+58, 32.5' RT.
TOP ELEV. 764.83
FLOWLINE ELEV. 762.38

PROPOSED INSPECTION HOLE #3
TYPE A
RWY. STA. 154+00.35, 563' RT.
TOP ELEV. 766.17
FLOWLINE ELEV. 763.72

PROPOSED INLET TYPE A
RWY. STA. 154+50, 612' RT.
TOP ELEV. 763.54
FLOWLINE ELEV. 760.54
BOX 24" X 36" WITH NEENAH R-3475
FRAME AND GRATE, OR EQUAL

PROPOSED INSPECTION HOLE #5 (TIE INTO 15" RCP--SEE DETAIL SHEET 10)
TYPE B
TXY "C" STA. 147+19.50, 85.75' RT.
TOP ELEV. 763.19
FLOWLINE ELEV. 760.74
15" INV. ELEV. 757.48

705-UNDERDRAIN NOTES:

THE PROPOSED UNDERDRAIN PIPE WILL BE CONSTRUCTED IN ACCORDANCE WITH ITEM 705 "PIPE UNDERDRAINS FOR AIRPORTS" AS STATED ON PAGE 109 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THIS ITEM OF WORK SHALL CONSIST OF FURNISHING AND INSTALLING 6" PERFORATED P.E. TUBING (WRAPPED) AND UNDERDRAIN INSPECTION HOLES AT THE LOCATIONS AND TO THE GRADES SHOWN ON THE CONSTRUCTION PLANS.

PIPE DRAINS SHALL BE INSTALLED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. THE PIPE SHALL BE BEDDED IN THE UNDERLYING MATERIAL TO A DEPTH NOT LESS THAN 10 PERCENT OF THE EXTERNAL DIAMETER OF THE PIPE, AND WHERE TRENCHING IS REQUIRED, THE TRENCH SHALL HAVE A WIDTH OF NOT LESS 12 IN. THE BOTTOM OF THE TRENCH SHALL BE COMPACTED IN A MANNER MEETING THE APPROVAL OF THE RESIDENT ENGINEER.

JOINTS AND FITTINGS MAY BE ASSEMBLED WITHOUT GASKETS OR SOLVENT CEMENT IF THE JOINT IS SAND TIGHT AND THE SPIGOT ENTERS THE SOCKET NOT LESS THAN 1/3 OF THE SOCKET DEPTH FOR SOLVENT CEMENT JOINTS AND FULL-DEPTH FOR ELASTOMERIC GASKET JOINTS.

NO PIPE SHALL BE PLACED IN THE TRENCH UNTIL IT AND THE PREPARED FOUNDATION HAVE BEEN APPROVED BY THE RESIDENT ENGINEER. THE PIPE SHALL BE LAID SO THAT THE FLOWLINE WILL BE AT THE GRADE SHOWN ON THE PLANS OR ESTABLISHED BY THE RESIDENT ENGINEER. THE PERMISSIBLE MINIMUM COVER OVER A PIPE SHALL BE 6 IN.

LAYING OF PIPES SHALL COMMENCE AT THE OUTLET END AND PROCEED TOWARD THE INLET END WITH THE PIPES TRUE TO LINE AND GRADE.

THE ENDS OF THE PIPE SHALL BE CAREFULLY CLEANED BEFORE THEY ARE PLACED, AND SHALL BE PLACED TO AVOID UNNECESSARY HANDLING ON THE FOUNDATION. AS EACH LENGTH OF PIPE IS LAID, THE ENDS OF THE PIPE SHALL BE PROTECTED TO PREVENT THE ENTRANCE OF ANY MATERIAL.

LONGITUDINAL LAPS SHALL BE PLACED AT THE SIDES AND SEPARATE SECTIONS OF PIPE SHALL BE JOINED WITH TIGHTLY-DRAWN, APPROVED CONNECTING BANDS.

THE TRENCH SHALL BE BACKFILLED WITH CA-14 OR CA-16, IN ACCORDANCE WITH ITEM 705, AND CONSOLIDATED TO THE RESIDENT ENGINEER'S SATISFACTION. THE POROUS BACKFILL WILL BE CONSIDERED INCIDENTAL TO ITEM AR70526 - 6" PERFORATED UNDERDRAIN W/ SOCK, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

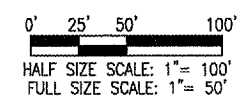
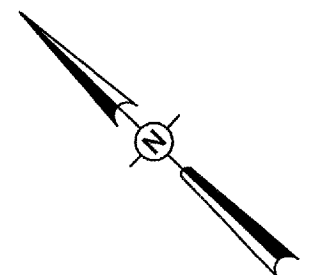
50 L.F. OF 6" NON-PERFORATED UNDERDRAIN IS INCLUDED FOR UNFORESEEN REPAIRS, IF NECESSARY.

THE PROPOSED UNDERDRAIN PIPE WILL BE PAID FOR UNDER ITEMS:

AR70526 6" PERFORATED UNDERDRAIN W/ SOCK	1,882 L.F.
AR705630 UNDERDRAIN INSPECTION HOLE	6 EACH
AR705546 6" NON PERFORATED UNDERDRAIN	50 L.F.
AR705640 UNDERDRAIN CLEANOUT	2 EACH
AR705900 REMOVE UNDERDRAIN	1653 L.F.

PIPE CONNECTION NOTE

NOTE: THE CONTRACTOR WILL TIE THE PROPOSED 12" RCP TO THE EXISTING INLET AT THE PROPOSED LOCATION AND INVERT SHOWN, IN ACCORDANCE TO ITEMS 701 AND 751. THE INLET WAS RECENTLY INSTALLED, IS IN GOOD CONDITION, AND WILL NOT REQUIRE REPLACEMENT DUE TO THE PLACEMENT OF THE PROPOSED PIPE. IF, DURING THE COURSE OF THE INSTALLATION, THE CONTRACTOR DAMAGES THE INLET BEYOND REPAIR, TO THE RESIDENT ENGINEER'S SATISFACTION, THE CONTRACTOR WILL REPLACE THE INLET AND ALL NECESSARY ITEMS AT HIS EXPENSE. THE COST OF CONNECTING THE PIPE TO THE EXISTING INLET WILL BE PAID FOR UNDER ITEM AR701512 - 12" RCP, CLASS IV, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE COST WILL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS TO COMPLETE THIS TASK.



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DATE	REVISION

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS

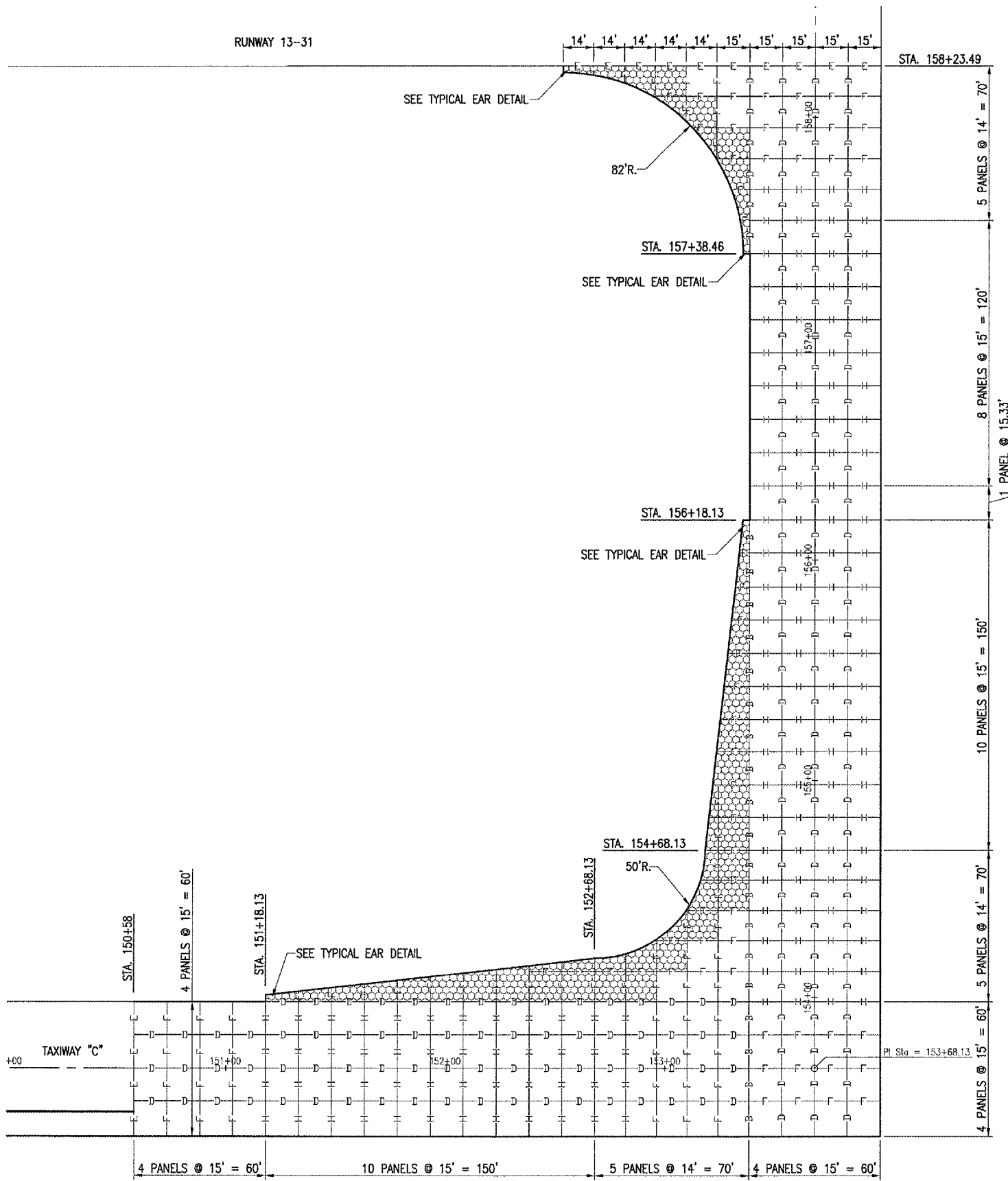
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ILLINOIS
A.I.P. PROJ.: 3-17-0085-XX
ILL. PROJ.: UIN-3338

PROJECT No. 82406TCD 0240	DATE 06/19/06
ENGINEER R-131DRN.DWG	DATE 06/19/06
SCALE 1" = 50'	DATE 06/19/06
DATE 06/19/06	DATE 07/12/06
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REVIEWED	RAW

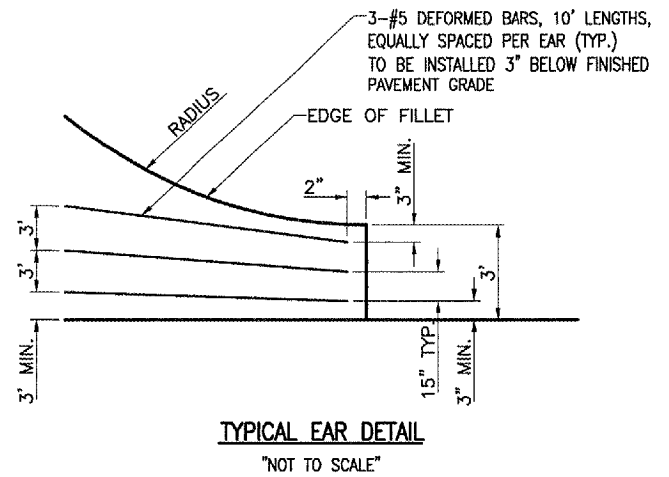
HANSON
Hanson Professional Services Inc.
1505 S. Main Street
Springfield, Illinois 62703-2886
Offices Nationwide

REALIGN AND
WIDEN TAXIWAY "C"
PROPOSED
DRAINAGE
PLAN

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- LEGEND**
- PROPOSED CONCRETE PAVEMENT
 - EXISTING PAVEMENT
 - PAVEMENT AREA TO BE REINFORCED WITH WIRE MESH 6"x6" (W5xW5)
 - CONSTRUCTION JOINT WITH DOWELS - TYPE D
 - LONGITUDINAL CONTRACTION JOINT WITH TIE BARS - TYPE G HINGED
 - TRANSVERSE CONTRACTION JOINT WITH DOWELS - TYPE F
 - TRANSVERSE CONTRACTION JOINT WITHOUT DOWELS - TYPE H
 - THICKENED EDGE EXPANSION JOINT - TYPE B
 - THICKENED EDGE EXPANSION JOINT - TYPE E



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DATE	REVISION	BY

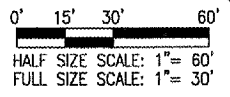
QUINCY REGIONAL AIRPORT
 BALDWIN FIELD
 ADAMS COUNTY, ILLINOIS

IL PROJ.: UN-3338 A.I.P. PROJ.: 3-17-0085-XX

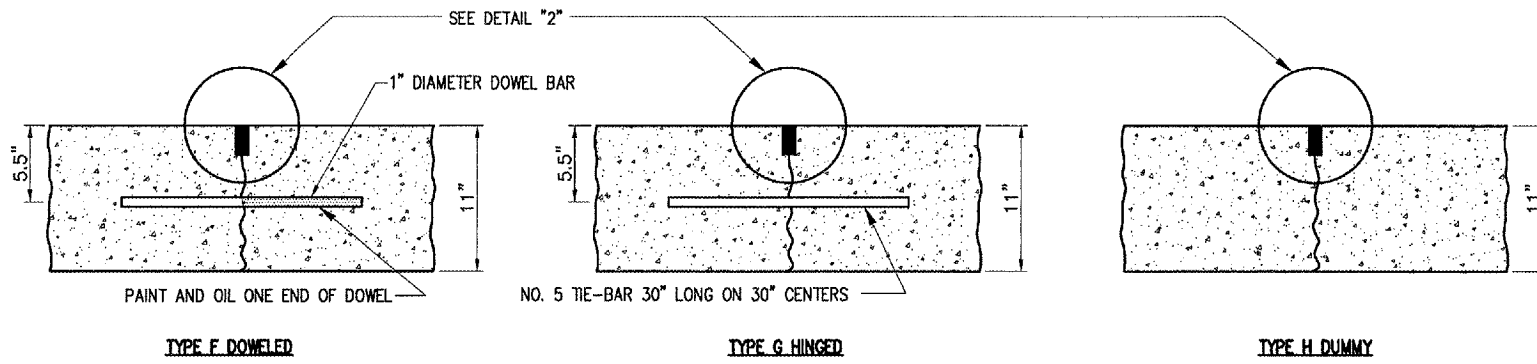
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		DATE: 07/12/06



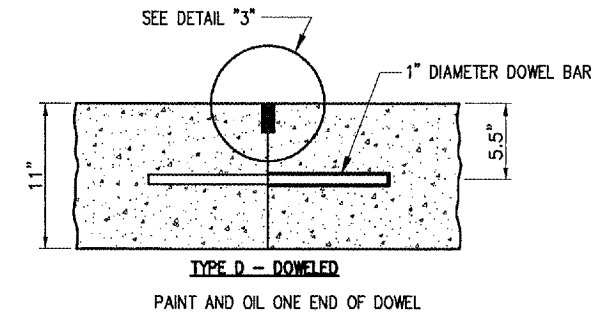
REALIGN AND
 WIDEN TAXIWAY "C"
 PROPOSED
 JOINTING
 PLAN



CONTRACTION JOINTS



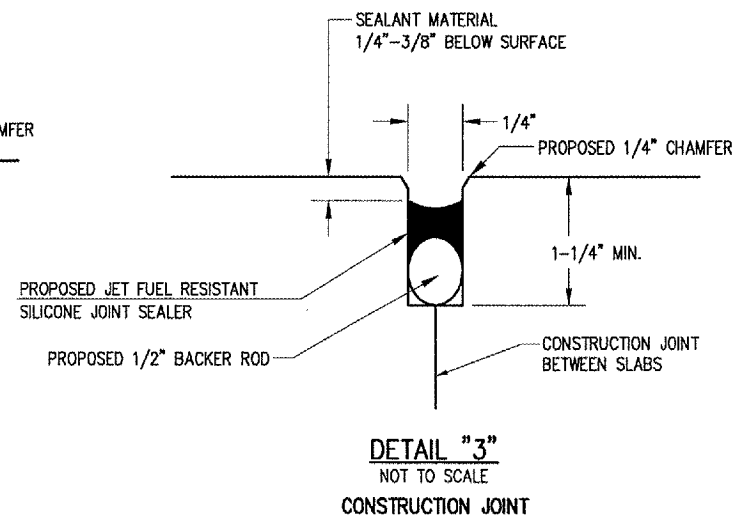
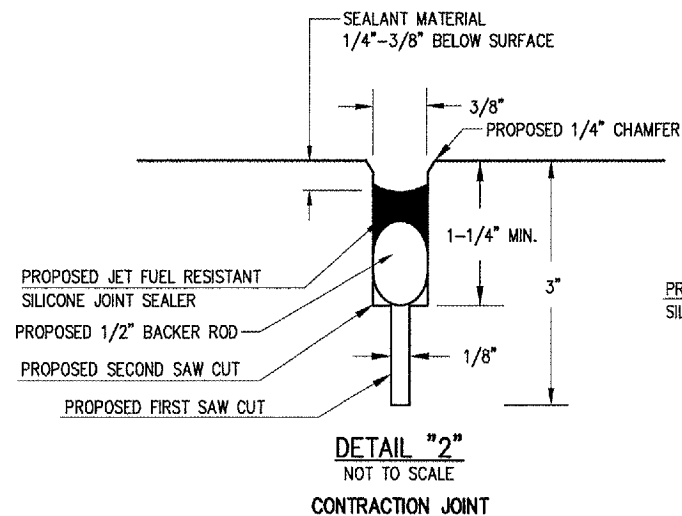
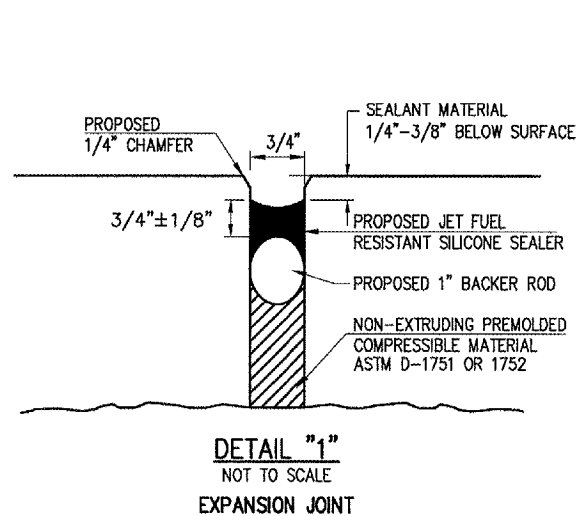
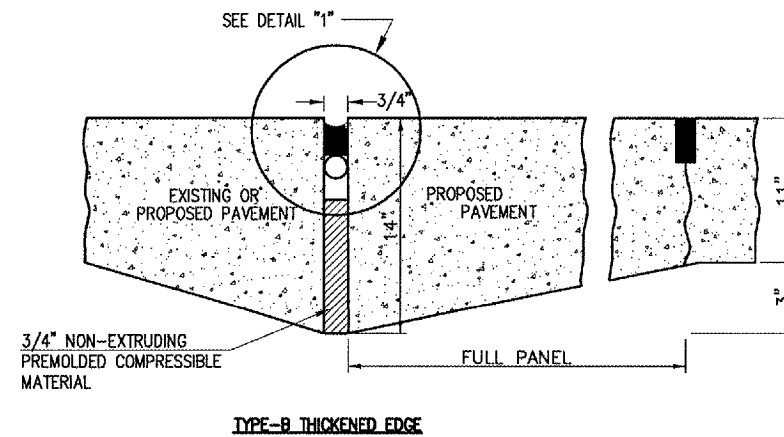
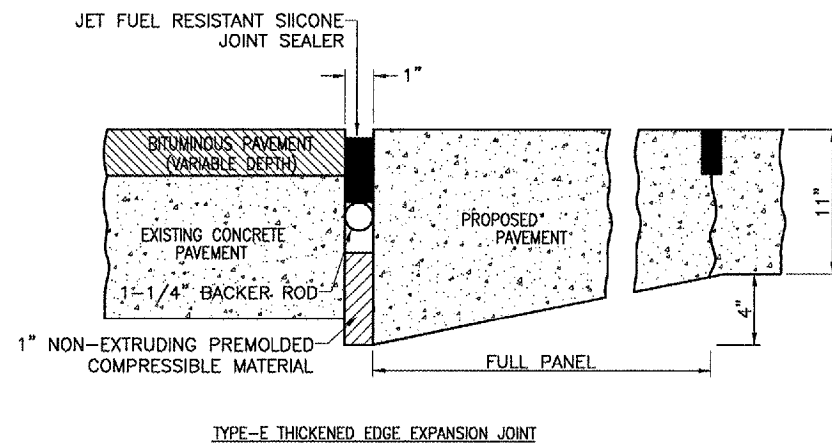
CONSTRUCTION JOINTS



JOINTING NOTES

- ALL EXPOSED JOINT EDGES SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/4".
- ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE SURFACE OF THE PAVEMENT AND TO THE CENTERLINES OF THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION. DOWELS FOR THE 11" PAVEMENT SHALL BE 1" DIA., 19" LENGTH AND 12" SPACING CENTER TO CENTER.
- ALL TIE-BARS SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT. SUPPORT PINS SHALL BE OF SUFFICIENT LENGTH TO PENETRATE AT LEAST 6" INTO THE SUBGRADE.
- ALL TIE-BARS SHALL BE PLACED AT A POINT NOT EXCEEDING 15" OR CLOSER THAN 6" FROM A TRANSVERSE, CONTRACTION, EXPANSION, OR CONSTRUCTION JOINT AND SPACED 30" ON CENTERS AND SHALL BE NO. 5 DEFORMED BARS, 30" IN LENGTH.
- DOWELS IN TRANSVERSE CONTRACTION AND LONGITUDINAL CONSTRUCTION JOINTS SHALL BE COATED WITH A RUSTPROOFING COMPOUND AND HALF THE LENGTH GREASED WITH A HEAVY GREASE.
- ALLOWABLE TOLERANCES FOR GROOVE DEPTH WILL BE +1/8" FOR CONSTRUCTION JOINTS AND + 1/4" FOR CONTRACTION JOINTS.
- THE CONTRACTOR IS REQUIRED TO DRILL AND EPOXY THE PROPOSED DOWELS IN ACCORDANCE WITH DETAILS AND THE SPECIAL PROVISIONS. THE EPOXY MATERIAL MUST BE APPROVED BY THE ILLINOIS DIVISION OF AERONAUTICS.
- THE COST OF ALL DOWEL BARS, TIE-BARS, WIRE MESH, SAWING AND SEALING SHALL BE INCLUDED IN THE COST OF THE P.C.C. PAVEMENT.
- PRIOR TO PLACING ADJACENT PAVEMENT SECTIONS, THE VERTICAL EDGE SHALL BE CHECKED FOR TRUENESS IF THE FACE IS BURRED OR IRREGULAR, THE CONTRACTOR SHALL GRIND, STONE OR SAW THE FACE A MINIMUM OF 2" IN DEPTH TO PRODUCE A SMOOTH AND STRAIGHT EDGE.
- WHEN CONSTRUCTING "FILL-IN" PAVEMENT LANES THE CONTRACTOR SHALL USE BELTING OR OTHER PROTECTIVE MATERIAL FOR THE PAVING MACHINE TO TRAVEL ON AND WILL ROPE THE TRANSVERSE JOINTS.
- JOINT SEALANT SHALL BE AS SPECIFIED IN THE RECURRING SPECIAL PROVISIONS FOR ITEM 605
- CURING COMPOUND WILL BE AS SPECIFIED IN THE RECURRING SPECIAL PROVISIONS FOR ITEM 501 AND SHALL BE APPROVED PRIOR TO THE PAVING OPERATION BY THE ENGINEER.
- ALL NON-ALIGNED EDGES WILL BE SAWED FULL DEPTH.

EXPANSION JOINTS



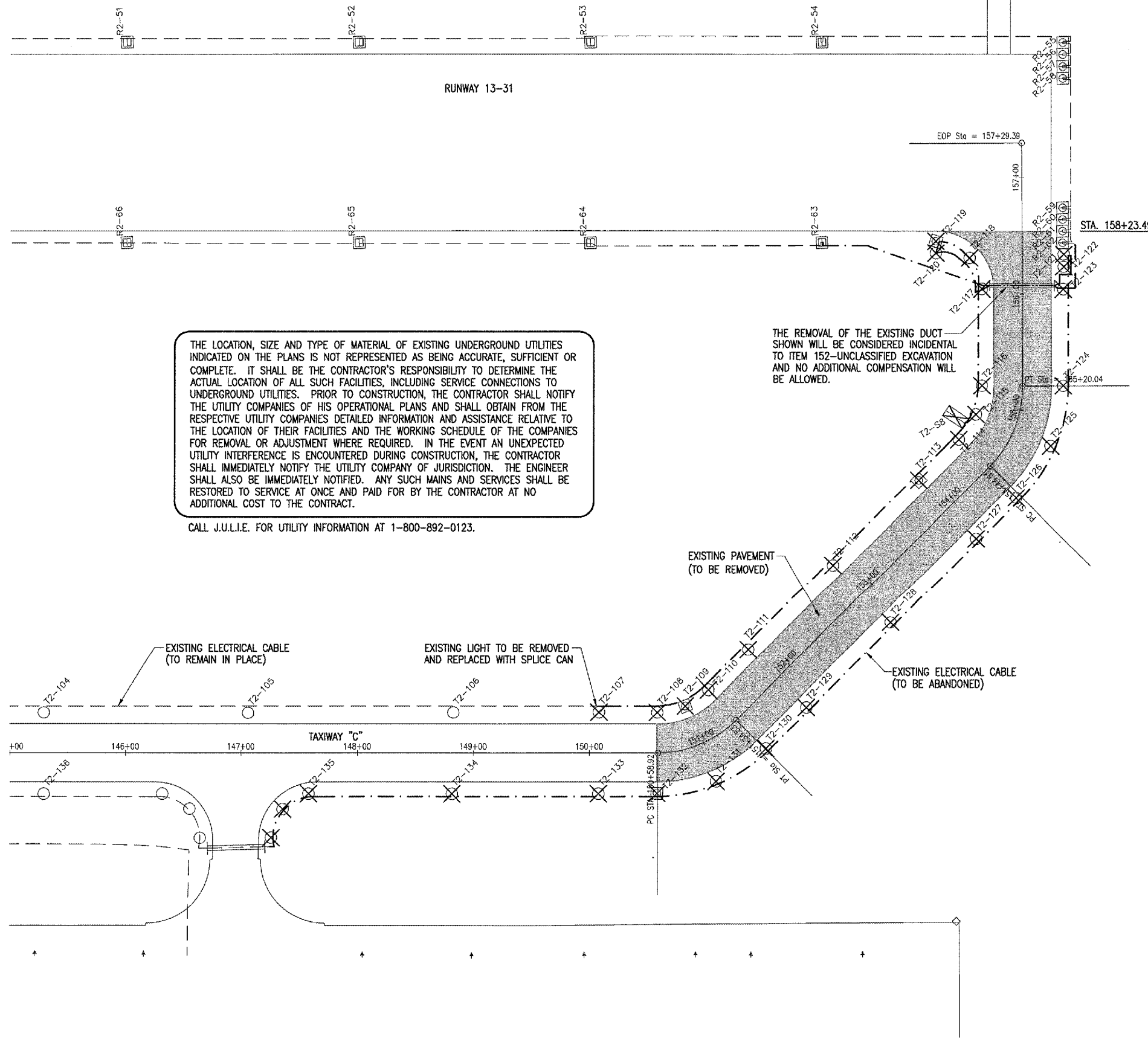
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BY	
REVISION	
DATE	
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS A.I.P. PROJ.: 3-17-0085-XX ILL. PROJ.: UIN-3338	

FILE PROJECT No.	82406TYCD 0240
FILE PROJECT	R-561UNT.DWG
SCALE	N/A
DATE	06/19/06
LAYOUT	RAW 06/19/06
DRAWN	BAK 06/19/06
REVIEWED	RAW 07/12/06

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 1500 S. Main Street
 Springfield, IL 62703-2888
 Offices Nationwide

REALIGN AND
WIDEN TAXIWAY "C"
PROPOSED
JOINTING
DETAILS



THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123.

THE REMOVAL OF THE EXISTING DUCT SHOWN WILL BE CONSIDERED INCIDENTAL TO ITEM 152-UNCLASSIFIED EXCAVATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

LEGEND

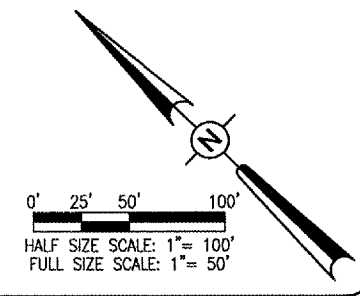
- EXISTING PAVEMENT
- EXISTING PAVEMENT (TO BE REMOVED)
- EXISTING ELECTRICAL DUCT
- EXISTING ELECTRICAL CABLE (TO REMAIN IN PLACE)
- EXISTING ELECTRICAL CABLE (TO BE ABANDONED)
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING BASE MOUNTED THRESHOLD LIGHT
- EXISTING STAKE MOUNTED TAXIWAY LIGHT (TO BE REMOVED)
- EXISTING BASE MOUNTED TAXIWAY LIGHT (TO BE REMOVED)
- EXISTING TAXI GUIDANCE SIGN (TO BE REMOVED)

EXISTING ELECTRICAL INFORMATION NOTE
 THE EXISTING ELECTRICAL INFORMATION SHOWN WAS OBTAINED FROM RECORD DRAWINGS AND IS PROVIDED FOR REFERENCE. PLEASE REFER TO THE PROPOSED ELECTRICAL PLANS FOR IMPROVEMENTS REQUIRED IN THIS PROJECT.

CABLING CONFLICT NOTE
 IN AREAS WHERE THE PROPOSED CABLES ARE TO BE PLACED ADJACENT TO THE EXISTING CABLES TO BE ABANDONED, THE PROPOSED CABLES WILL BE PLACED AT 15' OFF THE PROPOSED EDGE OF PAVEMENT TO AVOID CONFLICT WITH ABANDONED CABLES.

EQUIPMENT REMOVAL NOTE
 THE CONTRACTOR SHALL REMOVE THE LIGHTS AND TAXI GUIDANCE SIGN SHOWN TO BE REMOVED IN THEIR ENTIRETY, TO INCLUDE TRANSFORMERS, BASES, STAKES, AND ASSOCIATED MATERIAL. THE LIGHTS AND SIGNS SHALL BE TURNED OVER TO THE AIRPORT. ALL REMAINING MATERIALS REMOVED WILL BE DISPOSED OF OFF THE AIRPORT IN A LEGAL MANNER. ALL HOLES REMAINING AFTER EACH PROPOSED REMOVAL WILL BE BACKFILLED IN ACCORDANCE TO ITEM 108, TO THE SATISFACTION OF THE RESIDENT ENGINEER, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE COST OF THE PROPOSED REMOVAL OF THE LIGHTS AND SIGNS WILL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, AND DISPOSAL, AND WILL BE PAID FOR UNDER EACH RESPECTIVE REMOVAL PAY ITEM.

TEMPORARY CABLE NOTE
 THE CONTRACTOR SHALL PROVIDE TEMPORARY CABLE, AS NECESSARY, TO KEEP THE LIGHTS USEABLE WHILE THE EXISTING DUCT SHOWN IS REMOVED. THE TEMPORARY CABLE WILL NOT BE NECESSARY IF THE PROPOSED DUCT SHOWN ON SHEET 14 IS IN PLACE PRIOR TO THE REMOVAL OF THE EXISTING DUCT. THE COST OF THE TEMPORARY CABLE WILL INCLUDE ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO COMPLETE THIS TASK, AND WILL BE CONSIDERED INCIDENTAL TO ITEM AR110502 - 2-WAY CONCRETE ENCASED DUCT, WITH NO ADDITIONAL COMPENSATION ALLOWED.



DATE	REVISION	BY

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS

Quincy Regional Airport
4th Avenue
Quincy, IL 62450

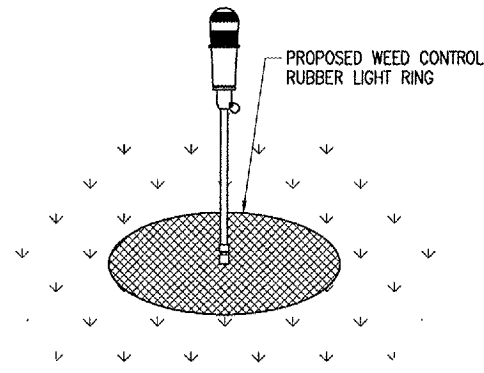
ILL. PROJ.: UIN-3338
A.I.P. PROJ.: 3-17-0085-XX

ILL. Drawn No. S2406TDCD-0240	DATE	06/19/06
Checked R-141ELE.DWG	DATE	06/19/06
Scale 1" = 50'	DATE	06/19/06
Drawn 06/19/06	DATE	06/19/06
Reviewed	DATE	07/12/06

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 Hanson Professional Services Inc.
 1400 E. St.
 Springfield, Illinois 62703-2886
 Offices Nationwide

REALIGN AND
WIDEN TAXIWAY "C"
EXISTING
ELECTRICAL
PLAN

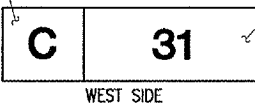
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WEED CONTROL LIGHT RING DETAIL
"NOT TO SCALE"

WEED CONTROL LIGHT RING NOTE
ALL PROPOSED LIGHTS SHALL HAVE A WEED CONTROL LIGHT RING PLACED AT ITS BASE. SEE SPECIAL PROVISIONS FOR ITEM AR800449.

YELLOW LETTERS ON BLACK WITH YELLOW BORDER



WEST SIDE

WHITE LETTERS W/BLACK BORDER ON RED



EAST SIDE

BLANK PANEL

YELLOW LETTERS ON BLACK WITH YELLOW BORDER

TAXIWAY GUIDANCE SIGN T2-S8 DETAIL
"NOT TO SCALE"

NUMBERING TAG NOTE

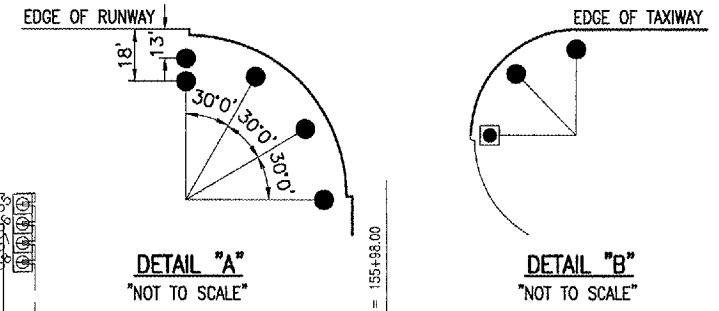
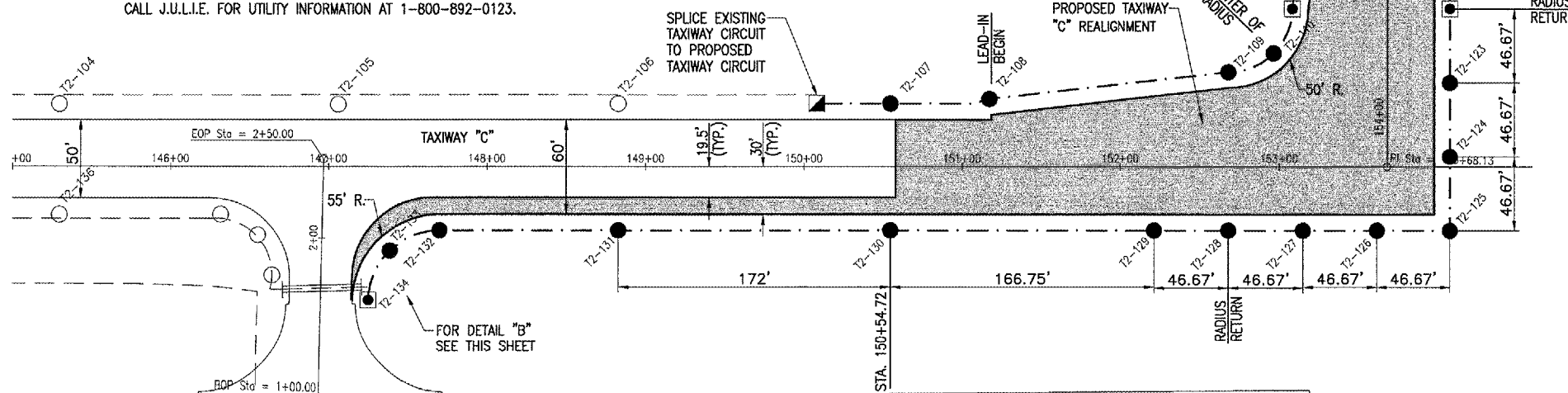
ALL OF THE PROPOSED LIGHTING WILL REQUIRE NUMBERING TAGS TO BE ATTACHED IN THE SEQUENCE SHOWN. DUE TO THE CHANGE IN QUANTITY FROM WHAT CURRENTLY EXISTS, NEW TAGS WILL BE SUPPLIED BY THE CONTRACTOR FOR THE LIGHTS PAST THE FINAL LIGHT SHOWN AS PROPOSED (T2-134). THE TOTAL QUANTITY REQUIRED IS 47, NUMBERED FROM T2-107 TO T2-153. THE COST OF RETAGGING THE EXISTING LIGHTS ON THE EXISTING CIRCUIT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123.

LEGEND

- EXISTING PAVEMENT
- PROPOSED IMPROVEMENTS
- EXISTING ELECTRICAL CABLE
- EXISTING ELECTRICAL DUCT
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING BASE MOUNTED THRESHOLD LIGHT
- PROPOSED 1/2", #8 5KV CABLE IN UNIT DUCT
- PROPOSED 2-WAY CONCRETE ENCASED DUCT
- PROPOSED STAKE MOUNTED TAXIWAY LIGHT
- PROPOSED BASE MOUNTED TAXIWAY LIGHT
- PROPOSED TAXI GUIDANCE SIGN
- PROPOSED SPLICE CAN



DETAIL "A"
"NOT TO SCALE"

DETAIL "B"
"NOT TO SCALE"

ELECTRICAL NOTES

CABLES:
ALL PROPOSED CABLE WILL BE PLACED 12' FROM THE PAVEMENT EDGE UNLESS THEY CONFLICT WITH THE EXISTING CABLES TO BE ABANDONED, WHERE THEY WILL BE PLACED AT 15' OFF OF PROPOSED EDGE OF PAVEMENT.

ALL PROPOSED CABLE WILL BE PLACED 18" BELOW THE PROPOSED GRADE.

THE PROPOSED ELECTRICAL CABLE WILL BE NO. 8, 5000 V., 1/C, TYPE C UNDERGROUND CABLE IN UNIT DUCT.

IN AREAS WHERE THERE IS A CONGESTION OF CABLES OR WHERE THE PROPOSED CABLE CROSSES AN EXISTING CABLE, THE PROPOSED CABLE WILL BE HAND DUG INTO PLACE. AT ALL OTHER LOCATIONS, THE PROPOSED CABLE IN UNIT DUCT MAY BE EITHER TRENCHED OR PLOWED INTO PLACE. THE TRENCHING AND/OR PLOWING WILL BE CONSIDERED INCIDENTAL TO THE PROPOSED CABLE AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL CABLE SHOWN BY A (---) DASHED LINE ARE EXISTING CIRCUITS THAT ARE NOT PART OF THIS CONTRACT. THE LOCATION OF THESE CABLES ARE APPROXIMATE AND FOR INFORMATIONAL PURPOSES ONLY. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE AND IDENTIFY THESE EXISTING CIRCUITS PRIOR TO THE INSTALLATION OF THE PROPOSED CABLE. ANY DAMAGE TO THE EXISTING CIRCUITS SHALL BE REPAIRED IMMEDIATELY TO THE SATISFACTION OF THE RESIDENT ENGINEER AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

LIGHTS:
ALL PROPOSED TAXIWAY LIGHTS WILL BE PLACED 10' FROM THE PAVEMENT EDGE UNLESS SHOWN OTHERWISE.

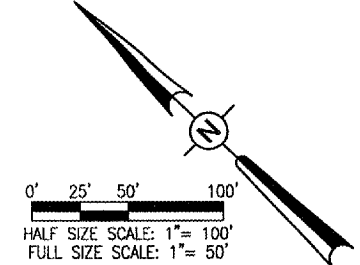
ALL PROPOSED TAXIWAY LIGHTS WILL BE CONSTRUCTED AT THE LOCATION SHOWN ON THIS SHEET AND IN ACCORDANCE WITH THE DETAILS SHOWN ON SHEET 16 AND THE SPECIFICATIONS.

ALL PROPOSED TAXIWAY LIGHTS WILL USE 360° BLUE LENSES.

DUCT:
ALL PROPOSED 2-WAY CONCRETE DUCT WILL BE CONSTRUCTED AT THE LOCATION SHOWN ON THIS SHEET AND IN ACCORDANCE WITH THE DETAIL ON SHEET NO. 15 AND THE SPECIFICATIONS.

THE TOP OF THE PROPOSED 2-WAY DUCTS WILL BE LOCATED 36" BELOW THE FINISHED PAVEMENT GRADE.

DURING CONSTRUCTION, THE EXISTING ACTIVE RUNWAY AND TAXIWAY LIGHTING CIRCUITS MAY BE INOPERABLE DURING THE DAY. HOWEVER, THE CIRCUIT WILL BE FUNCTIONING AT THE END OF THE CONSTRUCTION DAY. THE CONTRACTOR WILL PROVIDE TEMPORARY CABLE, FIXTURES, OR DO WHATEVER IS NECESSARY TO HAVE THE CIRCUITS ACTIVE AT THE END OF THE CONSTRUCTION DAY. THE WORK WILL BE CONSIDERED AS PART OF THE LIGHTING INSTALLATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



DATE	REVISION	BY

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS

IL PROJ.: UIN-3338
A.I.P. PROJ.: 3-17-0085-XX

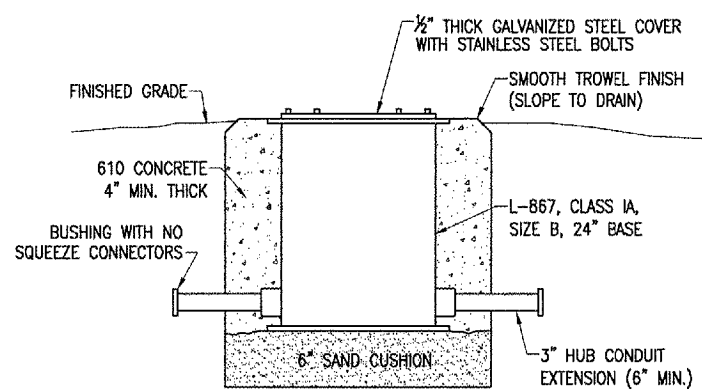
Project No. 82406TCD-0240	DATE 06/19/06
Revision R-142ELE.DWG	DATE 06/19/06
Scale 1"=50'	DATE 06/19/06
DATE 06/19/06	DATE 07/12/06
LAYOUT	RAW
DRAWN	BAK
REVIEWED	RAW

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REALIGN AND WIDEN TAXIWAY "C"
PROPOSED ELECTRICAL PLAN

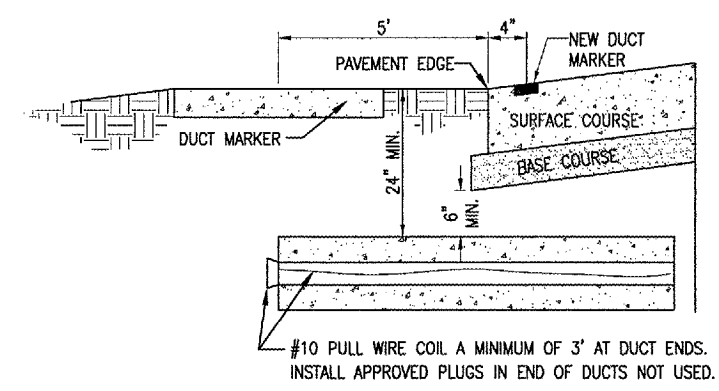
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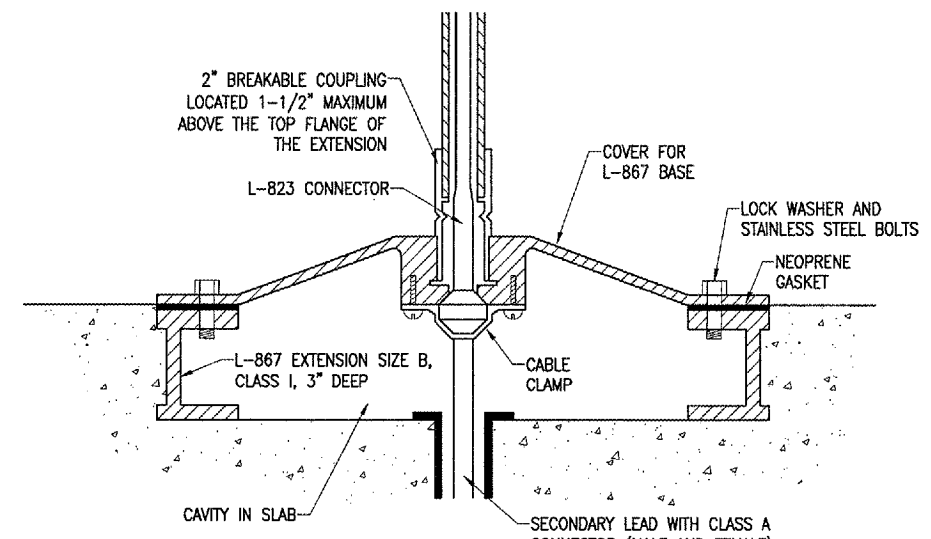


SPLICE CAN DETAIL
(NOT TO SCALE)

INCLUDE INTERNAL AND EXTERNAL GROUND LUGS



UNDERGROUND ELECTRICAL DUCT
(NOT TO SCALE)



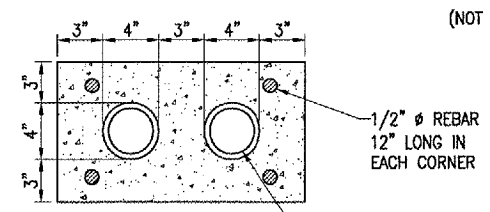
DETAIL "A"
(NOT TO SCALE)

NOTE:
THE COST OF ALL TURF AND PAVEMENT DUCT MARKERS SHALL BE INCIDENTAL TO THE DUCT. THE COST OF ALL CABLE MARKERS SHALL BE INCIDENTAL TO THE CABLE.

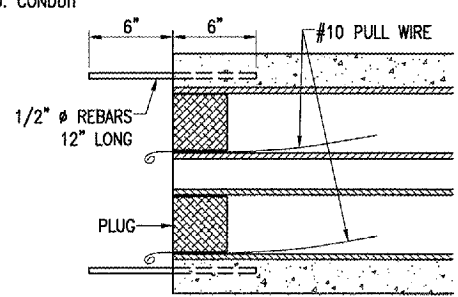
BITUMINOUS PAVEMENT DUCT MARKER AND CONCRETE DUCT MARKER TO BE PROVIDED AT EACH END OF EACH DUCT AS SHOWN ON THE LOCATION PLAN. FOR CONCRETE PAVEMENT, THE LETTER "D" SHALL BE IMPRESSED IN THE PAVEMENT INSTEAD OF THE MARKER. THE LETTER SHALL BE INFORMED AS DESCRIBED IN NOTE 4.

CABLE MARKERS SHALL BE PLACED AT CHANGES OF DIRECTION AND APPROXIMATELY EVERY 200' ALONG CABLE RUNS.

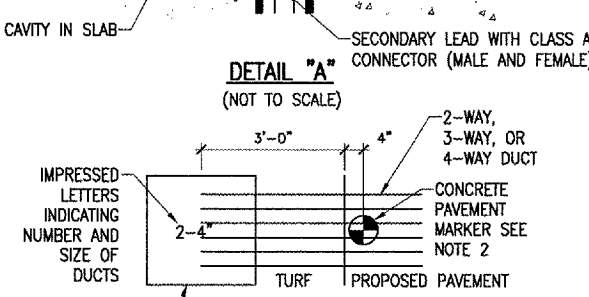
LETTERS 4" HIGH, 3" WIDE WITH WIDTH OF STROKE 1/2" AND 1/4" DEEP. ALL LETTERS, NUMBERS AND ARROWS TO BE IMPRESSED.



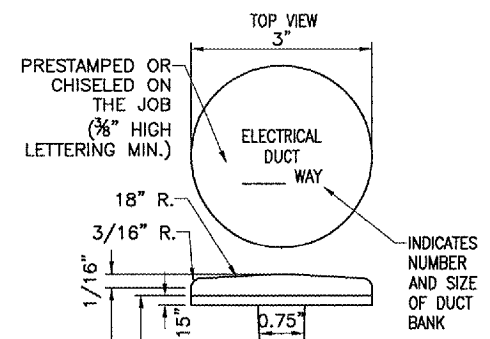
2-DUCT BANK
(NOT TO SCALE)



TYPICAL SECTION
(NOT TO SCALE)



DUCT MARKER DETAIL
"NOT TO SCALE"

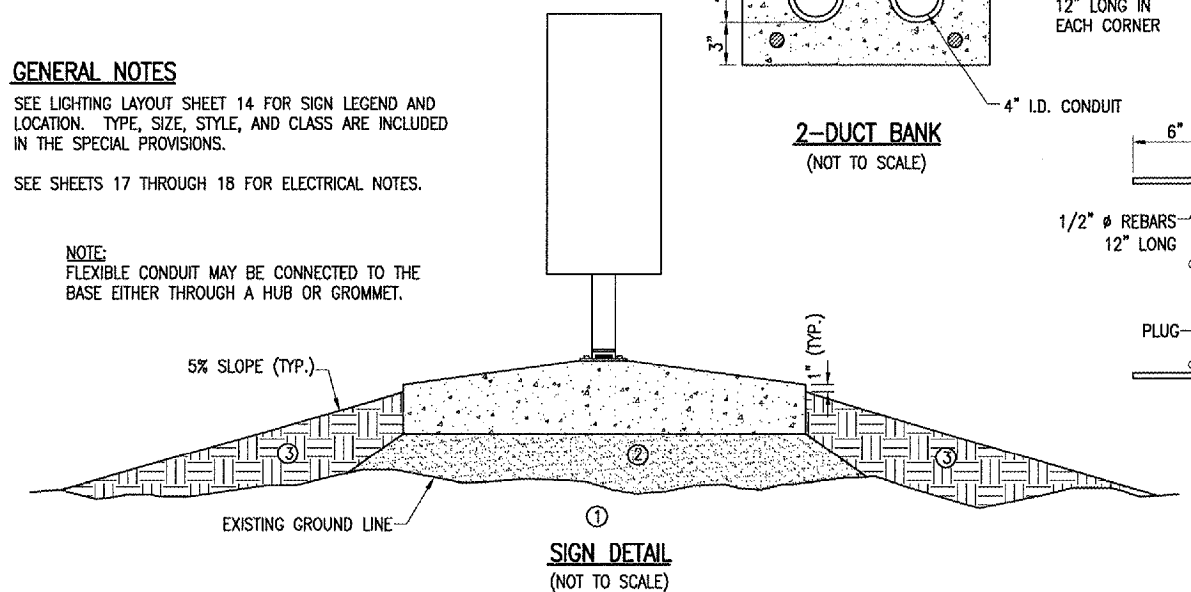


BITUMINOUS PAVEMENT DUCT MARKERS
"NOT TO SCALE"

GENERAL NOTES
SEE LIGHTING LAYOUT SHEET 14 FOR SIGN LEGEND AND LOCATION. TYPE, SIZE, STYLE, AND CLASS ARE INCLUDED IN THE SPECIAL PROVISIONS.

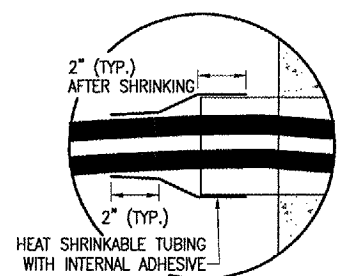
SEE SHEETS 17 THROUGH 18 FOR ELECTRICAL NOTES.

NOTE:
FLEXIBLE CONDUIT MAY BE CONNECTED TO THE BASE EITHER THROUGH A HUB OR GROMMET.



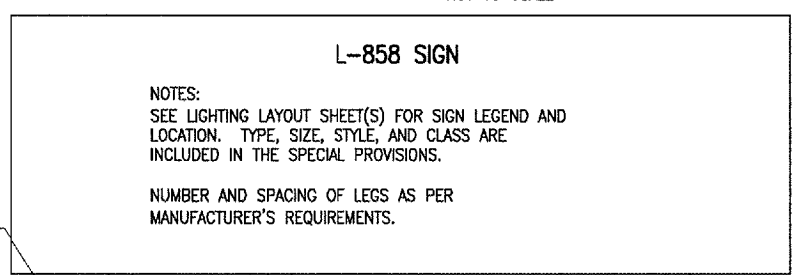
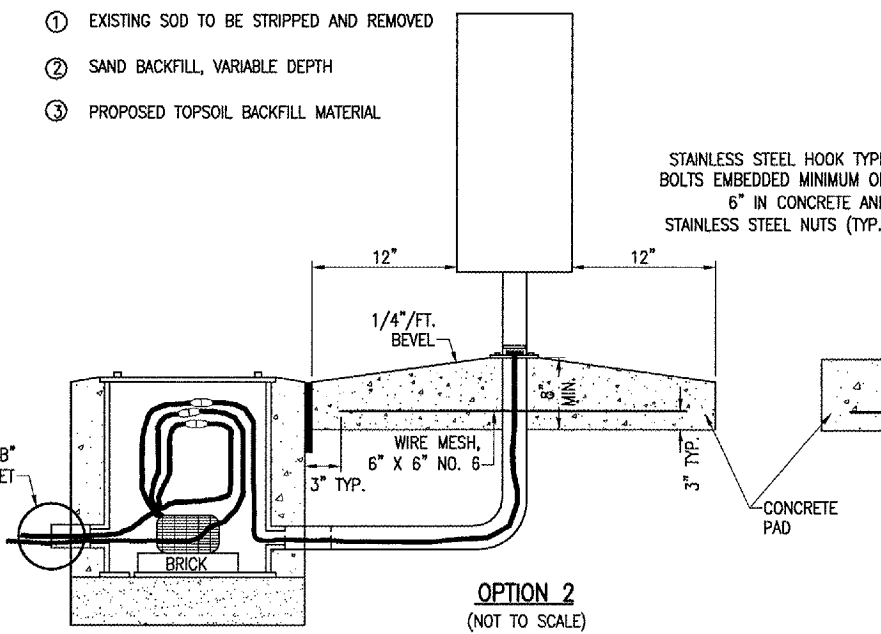
SIGN DETAIL
(NOT TO SCALE)

- ① EXISTING SOD TO BE STRIPPED AND REMOVED
- ② SAND BACKFILL, VARIABLE DEPTH
- ③ PROPOSED TOPSOIL BACKFILL MATERIAL



DETAIL "B"
(NOT TO SCALE)

SEE DETAIL "B" ON THIS SHEET



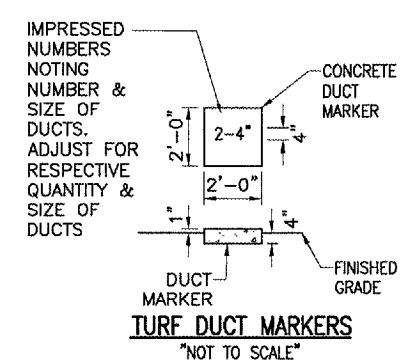
L-858 SIGN

NOTES:
SEE LIGHTING LAYOUT SHEET(S) FOR SIGN LEGEND AND LOCATION. TYPE, SIZE, STYLE, AND CLASS ARE INCLUDED IN THE SPECIAL PROVISIONS.

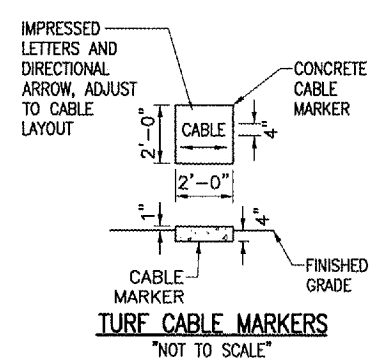
NUMBER AND SPACING OF LEGS AS PER MANUFACTURER'S REQUIREMENTS.

OPTION 1
(NOT TO SCALE)

OPTION 2
(NOT TO SCALE)

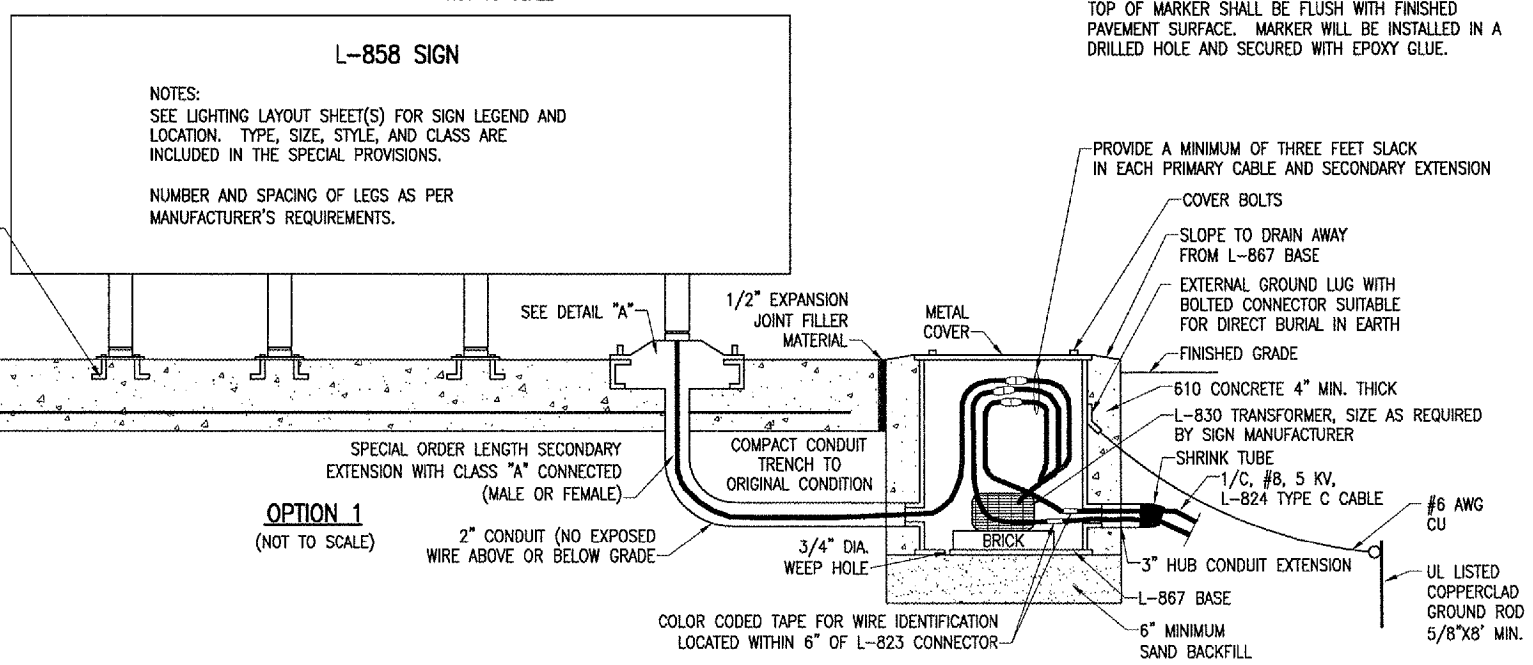


TURF DUCT MARKERS
"NOT TO SCALE"



TURF CABLE MARKERS
"NOT TO SCALE"

NOTE:
TOP OF MARKER SHALL BE FLUSH WITH FINISHED PAVEMENT SURFACE. MARKER WILL BE INSTALLED IN A DRILLED HOLE AND SECURED WITH EPOXY GLUE.



REVISION	DATE	BY

QUINCY REGIONAL AIRPORT
 BALDWIN FIELD
 ADAMS COUNTY, ILLINOIS
 A.I.P. PROJ.: 3-17-0085-XX
 I.L. PROJ.: UIN-3338

FILE	PROJECT NO.	DATE	SCALE	LAYOUT	DRAWN	BAK	REVIEWED
R-54/ELEC.DWG	82406TYCD 0240	06/19/06	N/A	RAW	RAW	RAW	RAW
		06/19/06					

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 Springfield, IL 62768
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REALIGN AND
 WIDEN TAXIWAY "C"
 PROPOSED
 ELECTRICAL
 DETAILS

BY	
REVISION	
DATE	

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS

QUINCY Regional Airport
ILLINOIS

A.I.P. PROJ.: 3-17-0085-XX
ILL. PROJ.: UN-3338

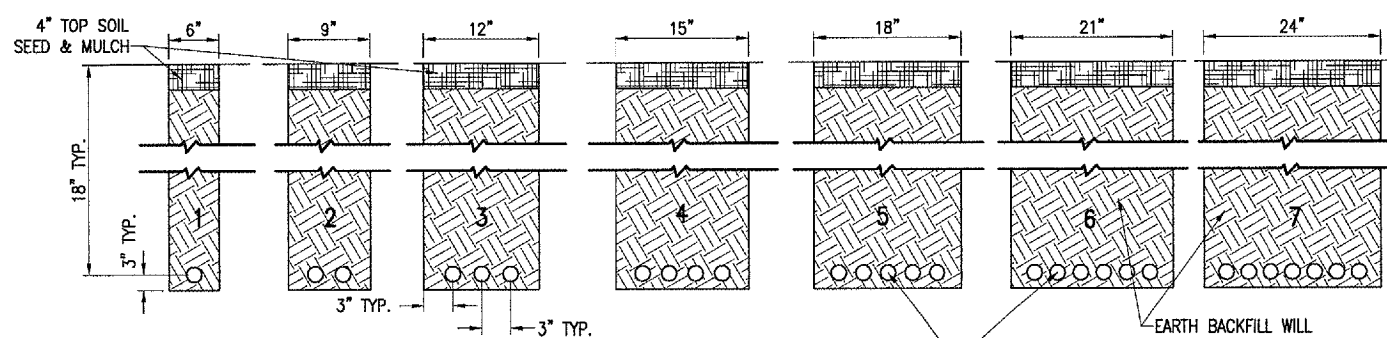
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File Name	R-542ELEC.DWG
Scale	N/A
Date	06/19/06
LAYOUT	RAW
DRAWN	BAK
REVIEWED	RAW
	06/19/06
	06/19/06
	07/12/06

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Springfield, Illinois 62703-2886
Offices Nationwide

REALIGN AND
WIDEN TAXIWAY "C"

PROPOSED
ELECTRICAL
DETAILS

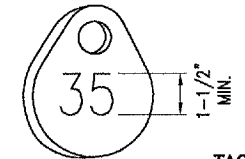
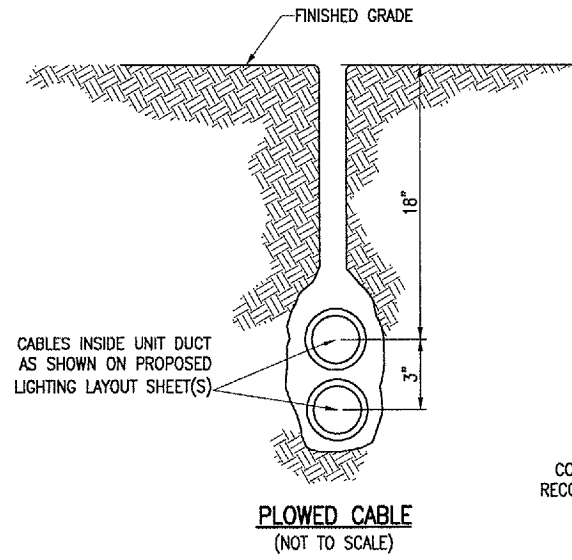


NOTES:
DETAIL NUMBERS INDICATE NO. OF CABLES.

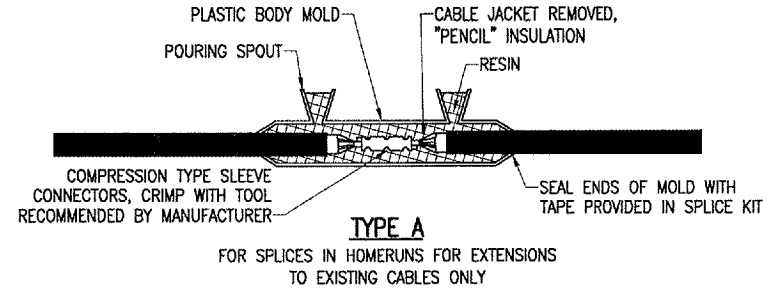
TRENCHES WITH MORE THAN SEVEN CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE; IF SPECIFIED ON PLANS TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.

DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.

ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH.

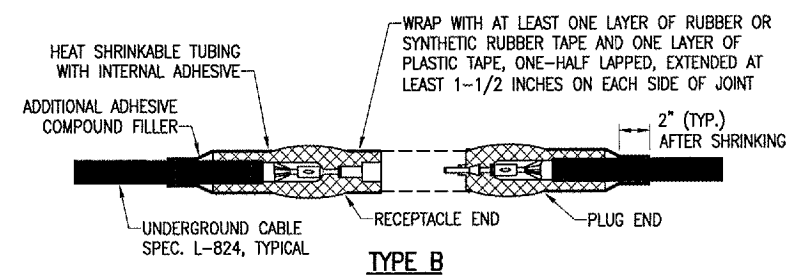


TAG DETAIL (NOT TO SCALE)



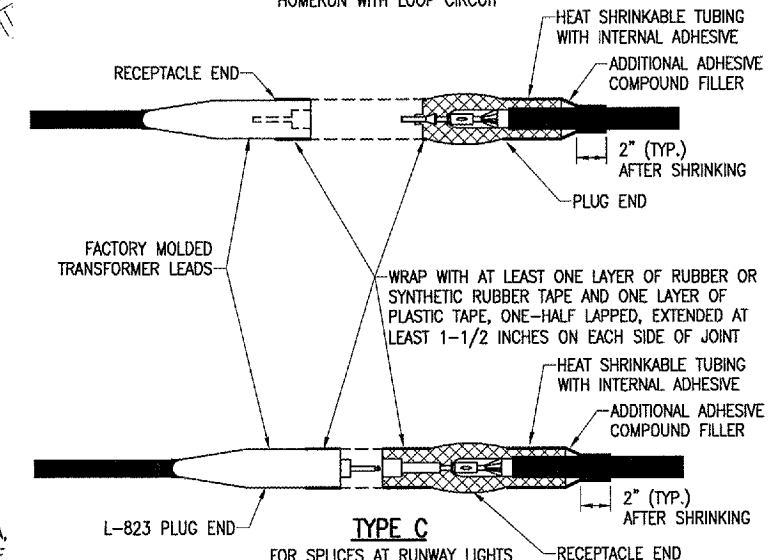
TYPE A

FOR SPLICES IN HOMERUNS FOR EXTENSIONS TO EXISTING CABLES ONLY



TYPE B

FOR SPLICES AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT



TYPE C

FOR SPLICES AT RUNWAY LIGHTS

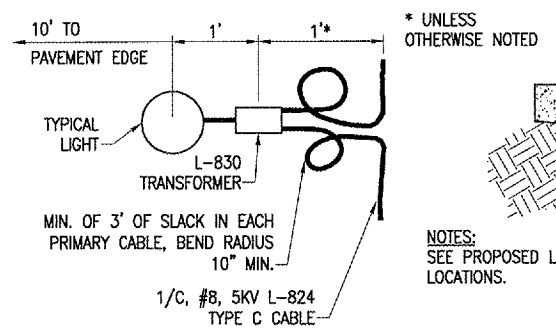
NOTES:
SEE PROPOSED LIGHTING LAYOUT SHEET(S) FOR SPLICE TYPE.

INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.

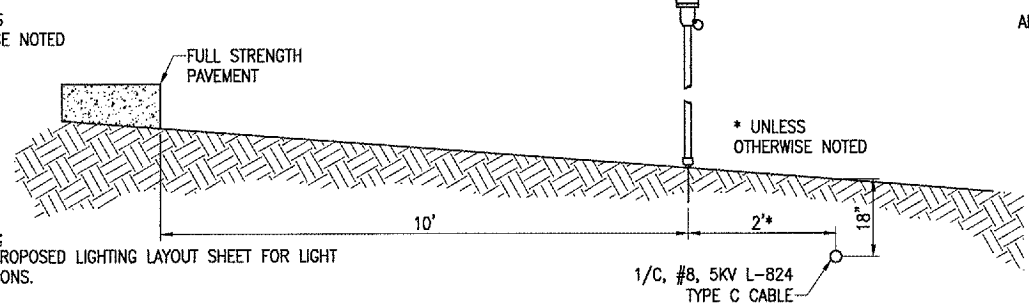
CABLE SPLICES (NOT TO SCALE)

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER, IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

CABLE TRENCHES (NOT TO SCALE)

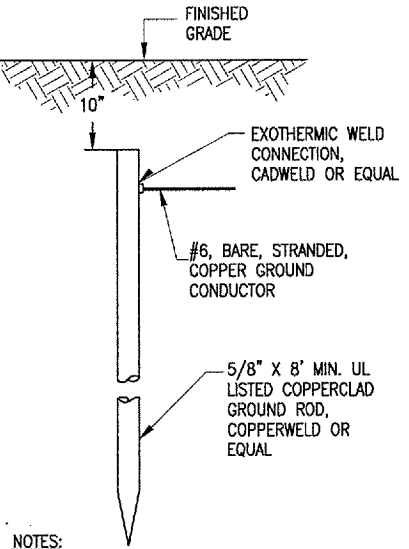


PLAN VIEW



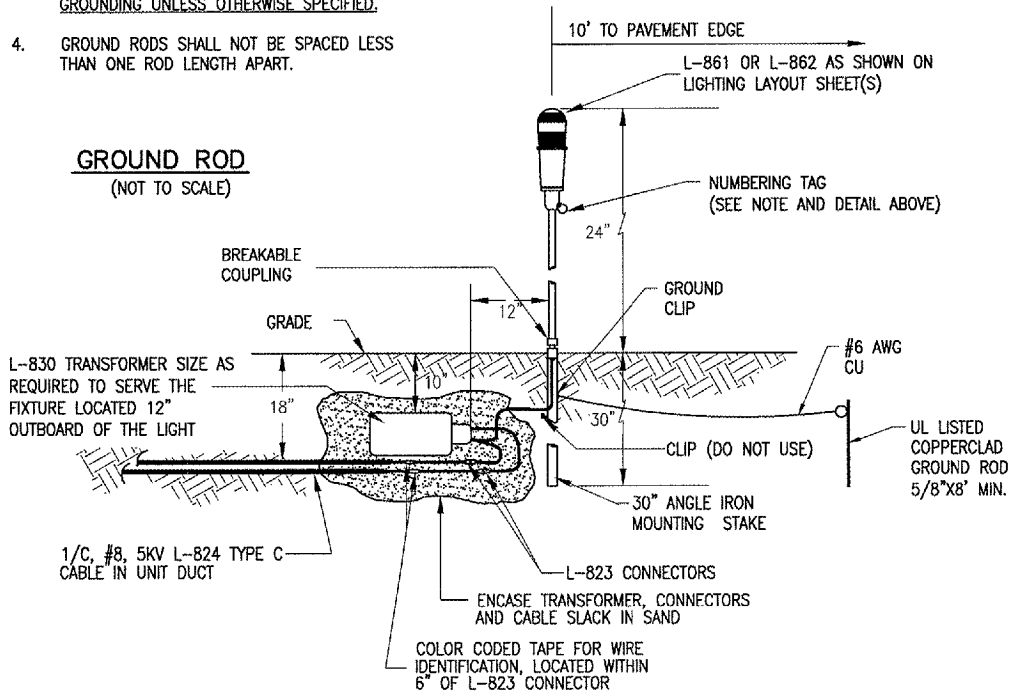
PROFILE VIEW

LIGHT AND CABLE INSTALLATION DETAIL (NOT TO SCALE)

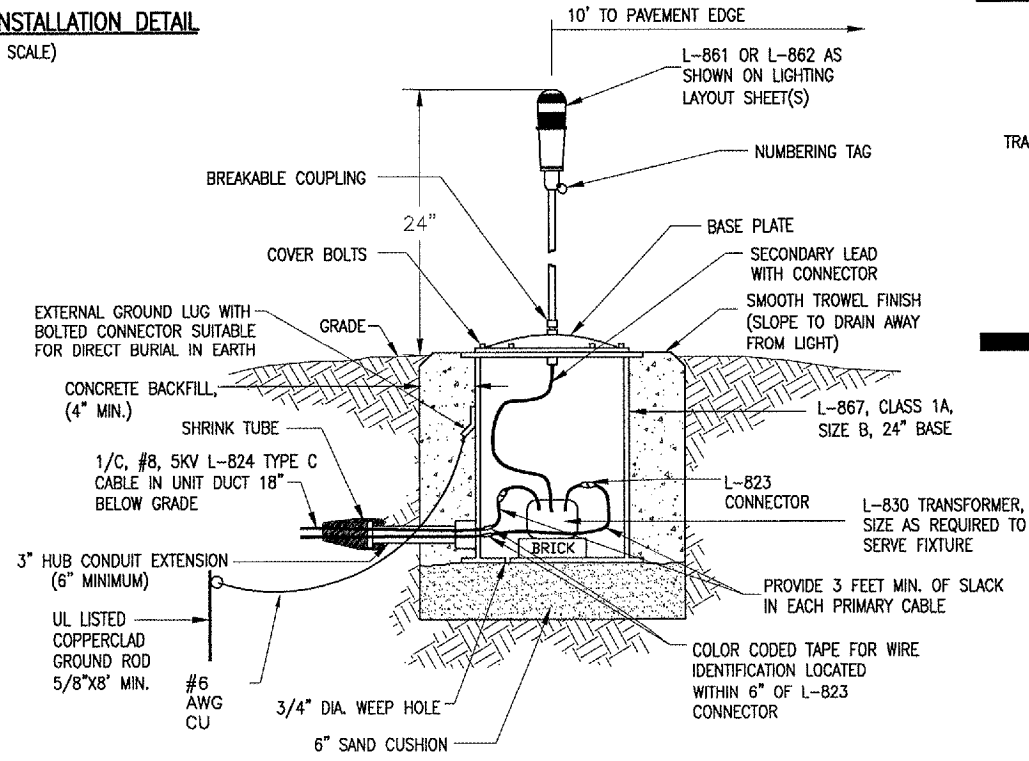


GROUND ROD (NOT TO SCALE)

- NOTES:**
- TYPE AND MINIMUM NUMBER OF GROUND RODS SHALL BE AS SPECIFIED ON THE PLAN.
 - THE RESISTANCE TO GROUND OF THE GROUNDING SYSTEM SHALL NOT EXCEED 25 OHMS.
 - COST OF GROUND RODS IS INCIDENTAL TO THE ASSOCIATED ITEMS REQUIRING GROUNDING UNLESS OTHERWISE SPECIFIED.
 - GROUND RODS SHALL NOT BE SPACED LESS THAN ONE ROD LENGTH APART.



MEDIUM INTENSITY LIGHT - STAKE MOUNTED (NOT TO SCALE)



MEDIUM/HIGH INTENSITY LIGHT - BASE MOUNTED (NOT TO SCALE)

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GENERAL

1. THE ELECTRICAL INSTALLATION, AS A MINIMUM, SHALL MEET THE NATIONAL ELECTRICAL CODE (LATEST RECOGNIZED VERSION) AND LOCAL REGULATIONS.
2. THE CONTRACTOR SHALL ASCERTAIN THAT ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY HIM, INCLUDING FAA APPROVED EQUIPMENT, ARE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND THE REMAINDER OF THE NEW/EXISTING SYSTEM. ANY NONCOMPATIBLE COMPONENTS FURNISHED BY THIS CONTRACTOR SHALL BE REPLACED BY HIM AT NO ADDITIONAL COST TO THE AIRPORT SPONSOR WITH A SIMILAR UNIT, APPROVED BY THE ENGINEER (DIFFERENT MODEL OR DIFFERENT MANUFACTURER) THAT IS COMPATIBLE WITH THE REMAINDER OF THE AIRPORT LIGHTING SYSTEM.
3. IN CASE THE CONTRACTOR ELECTS TO FURNISH AND INSTALL AIRPORT LIGHTING EQUIPMENT REQUIRING ADDITIONAL WIRING, TRANSFORMERS, ADAPTORS, MOUNTINGS, ETC., TO THOSE SHOWN ON THE DRAWINGS AND/OR LISTED IN THE SPECIFICATION, ANY COST FOR THESE ITEMS SHALL BE INCIDENTAL TO THE EQUIPMENT COST.
4. THE CONTRACTOR INSTALLED EQUIPMENT (INCLUDING FAA APPROVED) SHALL NOT GENERATE ANY ELECTROMAGNETIC INTERFERENCE IN THE EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AIR NAVIGATION, AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST WITH THE EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
5. WHEN A SPECIFIC TYPE, STYLE, CLASS, ETC. OF FAA APPROVED EQUIPMENT IS SPECIFIED ONLY THAT TYPE, STYLE, CLASS, WILL BE ACCEPTABLE, EVEN THOUGH EQUIPMENT OF OTHER TYPES, STYLES, CLASSES, ETC. MAY BE APPROVED.
6. ANY AND ALL INSTRUCTIONS FROM THE ENGINEER TO THE CONTRACTOR REGARDING CHANGES IN OR DEVIATIONS FROM THE PLANS AND SPECIFICATIONS SHALL BE IN WRITING WITH COPIES SENT TO THE AIRPORT SPONSOR AND THE FAA FIELD OFFICE (ADO/AFO). THE CONTRACTOR SHALL NOT ACCEPT ANY VERBAL INSTRUCTIONS FROM THE RESIDENT ENGINEER REGARDING ANY CHANGES FROM THE PLANS AND SPECIFICATIONS.
7. A MINIMUM OF THREE COPIES OF THE INSTRUCTION BOOK SHALL BE SUPPLIED WITH EACH DIFFERENT TYPE OF EQUIPMENT. THE BOOKS DESCRIBING A MORE SOPHISTICATED TYPE OF EQUIPMENT, SUCH AS REGULATORS, PAPI, REIL, ETC. AS A MINIMUM SHALL CONTAIN THE FOLLOWING:
 - A. A DETAILED DESCRIPTION OF THE OVERALL EQUIPMENT AND ITS INDIVIDUAL COMPONENTS.
 - B. THEORY OF OPERATION INCLUDING THE FUNCTION OF EACH COMPONENT.
 - C. INSTALLATION INSTRUCTIONS.
 - D. START-UP INSTRUCTIONS.
 - E. PREVENTATIVE MAINTENANCE REQUIREMENTS.
 - F. CHART FOR TROUBLE-SHOOTING.
 - G. COMPLETE POWER AND CONTROL DETAILED WIRING DIAGRAM(S), SHOWING EACH CONDUCTOR/CONNECTION/COMPONENT - "BLACK" BOXES ARE NOT ACCEPTABLE. THE DIAGRAM OF THE NARRATIVE SHALL SHOW VOLTAGE/CURRENTS/WAVE SHAPES AT STRATEGIC LOCATIONS TO BE USED WHEN CHECKING AND/OR TROUBLE-SHOOTING THE EQUIPMENT. WHEN THE EQUIPMENT HAS SEVERAL MODES OF OPERATION, SUCH AS SEVERAL BRIGHTNESS STEPS, THESE PARAMETERS SHALL BE INDICATED FOR ALL DIFFERENT MODES.
 - H. PARTS LIST WHICH WILL INCLUDE ALL MAJOR AND MINOR COMPONENTS SUCH AS RESISTORS, DIODES, ETC. IT SHALL INCLUDE A COMPLETE NOMENCLATURE OF EACH COMPONENT AND, IF APPLICABLE, THE NAME OF ITS MANUFACTURER AND THE CATALOG NUMBER.
 - I. SAFETY INSTRUCTIONS.

POWER AND CONTROL

1. STENCIL ALL ELECTRICAL EQUIPMENT TO IDENTIFY FUNCTION, CIRCUIT VOLTAGE AND PHASE. WHERE THE EQUIPMENT CONTAINS FUSES, ALSO STENCIL THE FUSE OR FUSE LINK AMPERE RATING. WHERE THE EQUIPMENT DOES NOT HAVE SUFFICIENT STENCILING AREA, THE STENCILING SHALL BE DONE ON THE WALL NEXT TO THE UNIT. THE LETTERS SHALL BE ONE INCH HIGH AND PAINTED IN WHITE OR BLACK TO PROVIDE THE HIGHEST CONTRAST WITH THE BACKGROUND.
2. COLOR CODE ALL PHASE WIRING BY THE USE OF COLORED WIRE INSULATION AND/OR COLORED TAPE. WHERE TAPE IS USED, THE WIRE INSULATION SHALL BE BLACK, BLACK AND RED SHALL BE USED FOR SINGLE-PHASE, THREE WIRE SYSTEMS AND BLACK, RED AND BLUE SHALL BE USED FOR THREE-PHASE SYSTEMS. NEUTRAL CONDUCTORS, SIZE NO. 6 AWG OR SMALLER, SHALL BE IDENTIFIED BY A CONTINUOUS WHITE OR NATURAL GRAY OUTER FINISH ALONG ITS ENTIRE LENGTH. NEUTRAL CONDUCTORS LARGER THAN NO. 6 AWG SHALL BE IDENTIFIED EITHER BY A CONTINUOUS WHITE OR NATURAL GRAY OUTER FINISH ALONG ITS ENTIRE LENGTH OR BY THE USE OF WHITE TAPE AT ITS TERMINATIONS AND INSIDE ACCESSIBLE WIREWAYS.
3. ALL BRANCH CIRCUIT CONDUCTORS CONNECTED TO A PARTICULAR PHASE SHALL BE IDENTIFIED WITH THE SAME COLOR. THE COLOR CODING SHALL BE EXTENDED TO THE POINT OF UTILIZATION.
4. IN CONTROL WIRING THE SAME COLOR SHALL BE USED THROUGHOUT THE SYSTEM FOR THE SAME FUNCTION, SUCH AS 10%, 30%, 100% BRIGHTNESS CONTROL, ETC.
5. LOW VOLTAGE (600 V.) AND HIGH VOLTAGE (5000 V.) CONDUCTORS SHALL BE INSTALLED IN SEPARATE WIREWAYS.
6. NEATLY LACE WIRING IN DISTRIBUTION PANELS, WIREWAYS, SWITCHES AND JUNCTION/PULL BOXES.
7. THE MINIMUM SIZE OF PULL/JUNCTION BOXES, REGARDLESS OF THE QUANTITY AND SIZE OF THE CONDUCTORS SHOWN, SHALL BE AS FOLLOWS:
 - A. IN STRAIGHT PULLS THE LENGTH OF THE BOX SHALL NOT BE LESS THAN EIGHT TIMES THE TRADE DIAMETER OF THE LARGER CONDUIT. THE TOTAL AREA (INCLUDING THE CONDUIT CROSS-SECTIONAL AREA) OF A BOX END SHALL BE AT LEAST 3 TIMES GREATER THAN THE TOTAL TRADE CROSS-SECTIONAL AREA OF THE CONDUITS TERMINATING AT THE END.
 - B. IN ANGLE PULLS OR 'U' PULLS THE DISTANCE BETWEEN EACH CONDUIT ENTRY INSIDE THE BOX AND THE OPPOSITE WALL OF THE BOX SHALL NOT BE LESS THAN SIX (6) TIMES THE TRADE DIAMETER OF THE LARGEST CONDUIT. THIS DISTANCE SHALL BE INCREASED FOR ADDITIONAL ENTRIES BY THE AMOUNT OF THE SUM OF THE DIAMETERS OF ALL OTHER CONDUIT ENTRIES ON THE SAME WALL AS THE BOX. THE DISTANCE BETWEEN CONDUIT ENTRIES ENCLOSING THE SAME CONDUCTOR SHALL NOT BE LESS THAN SIX TIMES THE TRADE DIAMETER OF THE LARGEST CONDUIT.
8. A RUN OF CONDUIT BETWEEN TERMINATIONS AT EQUIPMENT ENCLOSURES, SQUARE DUCTS AND PULL/JUNCTION BOXES, SHALL NOT CONTAIN MORE THAN THE EQUIVALENT OF FOUR QUARTER BENDS (360 DEGREES TOTAL), INCLUDING THOSE BENDS LOCATED IMMEDIATELY AT THE TERMINATIONS, CAST, CONDUIT TYPE OUTLETS SHALL NOT BE TREATED AS PULL/JUNCTION BOXES.
9. EQUIPMENT CABINETS SHALL NOT BE USED AS PULL/JUNCTION BOXES. ONLY WIRING TERMINATING AT THE EQUIPMENT SHALL BE BROUGHT INTO THESE ENCLOSURES.
10. SPLICES AND JUNCTION POINTS SHALL BE PERMITTED ONLY IN JUNCTION BOXES, DUCTS EQUIPPED WITH REMOVABLE COVERS, AND AT EASILY ACCESSIBLE LOCATIONS.
11. CIRCUIT BREAKERS IN POWER DISTRIBUTION PANEL(S) SHALL BE THERMAL-MAGNETIC MOLDED CASE, PERMANENT TRIP WITH 100 AMPERE, MINIMUM FRAME.
12. DUAL LUGS SHALL BE USED WHERE TWO (2) WIRES, SIZE NO. 6 OR LARGER, ARE TO BE CONNECTED TO THE SAME TERMINAL.
13. ALL WALL MOUNTED EQUIPMENT ENCLOSURES SHALL BE MOUNTED ON WOODEN MOUNTING BOARDS.
14. WOODEN EQUIPMENT MOUNTING BOARDS SHALL BE PLYWOOD, EXTERIOR TYPE, 3/4 INCH, MINIMUM, THICKNESS, BOTH SIDES PAINTED WITH ONE COAT OF PRIMER AND TWO COATS OF GRAY OIL-BASED PAINT.
15. RIGID STEEL CONDUIT SHALL BE USED THROUGHOUT THE INSTALLATION UNLESS OTHERWISE SPECIFIED. THE MINIMUM TRADE SIZE SHALL BE 3/4 INCH.
16. ALL RIGID CONDUIT SHALL BE TERMINATED AT CONSTANT CURRENT REGULATORS WITH A SECTION (10" MINIMUM) OF FLEXIBLE CONDUIT.
17. UNLESS OTHERWISE SHOWN, ALL EXPOSED CONDUITS SHALL BE RUN PARALLEL TO OR AT RIGHT ANGLES WITH THE LINES OF THE STRUCTURE.
18. ALL STEEL CONDUITS, FITTINGS, NUTS, BOLTS, ETC. SHALL BE GALVANIZED.
19. USE CONDUIT BUSHINGS AT EACH CONDUIT TERMINATION. WHERE NO. 4 AWG OR LARGER UNDERGROUND WIRE IS INSTALLED, USE INSULATED BUSHINGS.
20. USE DOUBLE LOCK NUTS AT EACH CONDUIT TERMINATION.
21. WRAP ALL PRIMARY AND SECONDARY POWER TRANSFORMER CONNECTIONS WITH SUFFICIENT LAYERS OF INSULATING TAPE AND COVER WITH INSULATING VARNISH FOR FULL VALUE OF CABLE INSULATION VOLTAGE.
22. UNLESS OTHERWISE NOTED, ALL INDOOR SINGLE CONDUCTOR CONTROL WIRING SHALL BE NO. 12 AWG. MINIMUM.
23. THE FOLLOWING SHALL APPLY TO RELAY/CONTACTOR PANELS/ENCLOSURES:
 - A. ALL COMPONENTS SHALL BE MOUNTED IN DUST PROOF ENCLOSURE(S) WITH VERTICALLY HINGED COVERS.
 - B. THE ENCLOSURE(S) SHALL HAVE AMPLE SPACE FOR THE CIRCUIT COMPONENTS, TERMINAL BLOCKS AND INCOMING AND INTERNAL WIRING.
 - C. ALL CONTROL CONDUCTOR TERMINATIONS SHALL BE OF THE OPEN-EYE CONNECTOR/SCREW TYPE. SOLDERED CLOSED-EYE TERMINATIONS, OR TERMINATIONS WITHOUT CONNECTORS ARE NOT ACCEPTABLE.
 - D. WHEN THE ENCLOSURE COVER IS OPENED, ALL CIRCUIT COMPONENTS, WIRING AND TERMINALS SHALL BE EXPOSED AND ACCESSIBLE WITHOUT REMOVAL OF ANY PANELS, COVERS, ETC., EXCEPT THOSE COVERING HIGH VOLTAGE COMPONENTS.
 - E. ACCESS TO, OR REMOVAL OF A CIRCUIT COMPONENT OR TERMINAL BLOCK WILL NOT REQUIRE THE REMOVAL OF ANY OTHER CIRCUIT COMPONENT OR TERMINAL BLOCK.
 - F. EACH CIRCUIT COMPONENT SHALL BE CLEARLY IDENTIFIED INDICATING ITS CORRESPONDING NUMBER SHOWN ON THE DRAWINGS AND ITS FUNCTION.
 - G. A COMPLETE WIRING DIAGRAM (NOT A SCHEMATIC DIAGRAM) SHALL BE MOUNTED ON THE INSIDE OF THE COVER. THE DIAGRAM SHALL REPRESENT EACH CONDUCTOR BY A SEPARATE LINE.
 - H. THE DIAGRAM SHALL IDENTIFY EACH CIRCUIT COMPONENT AN NUMBERING AND COLOR OF EACH TERMINAL CONDUCTOR AND TERMINAL.
 - I. ALL WIRING SHALL BE NEATLY TRAINED AND LACED.
 - J. MINIMUM WIRE SIZE SHALL BE NO. 12 AWG.

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BY									
REVISION									
DATE									
 QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS									
ILL. PROJ.: QUN-3338 A.I.P. PROJ.: 3-17-0085-XK									
IEL Project No.: B2406TYCD_0240 Filename: R-5A3ELECD.WG Sheet: N/A Date: 06/19/06	LAYOUT: RAW DRAWN: BAK REVIEWED: RAW	06/19/06 06/19/06 07/12/06	 HANSON Hanson Professional Services Inc. 1626 South Sixth Street Springfield, Illinois 62762-2686 Offices Nationwide						
REALIGN AND WIDEN TAXIWAY "C" PROPOSED ELECTRICAL NOTES									
17 17 of 24 sheets									

FIELD LIGHTING NOTES

1. UNLESS OTHERWISE NOTED, ALL UNDERGROUND FIELD POWER MULTIPLE AND SERIES CIRCUIT CONDUCTORS WHETHER DEB OR IN DUCT/CONDUIT SHALL BE FAA APPROVED L-824 TYPE. INSULATION VOLTAGE AND SIZE SHALL BE AS SPECIFIED.
2. NO COMPONENTS OF PRIMARY CIRCUIT SUCH AS CABLE, CONNECTORS AND TRANSFORMERS SHALL BE BROUGHT ABOVE GROUND AT EDGE LIGHTS, SIGNS, REIL, PAPI, ETC.
3. THERE SHALL BE NO EXPOSED POWER/CONTROL CABLES BETWEEN THE POINT WHERE THEY LEAVE THE UNDERGROUND (DEB OR L-867 BASES) AND WHERE THEY ENTER THE EQUIPMENT (SUCH AS TAXIWAY SIGNS, PAPI, REIL, ETC.) ENCLOSURES. THESE CABLES SHALL BE ENCLOSED IN RIGID CONDUIT OR IN FLEXIBLE, WATERTIGHT CONDUIT WITH BREAKABLE COUPLING(S) AT THE GRADE OR THE HOUSING COVER, AS SHOWN IN APPLICABLE DETAILS.
4. THE JOINTS OF THE L-823 PRIMARY CONNECTORS SHALL BE WRAPPED WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF THE JOINT, AS SHOWN ON SHEET NO. 16.
5. THE CABLE ENTRANCE INTO THE FIELD-ATTACHED L-823 CONNECTORS SHALL BE ENCLOSED BY A HEAT-SHRINKABLE TUBING WITH CONTINUOUS INTERNAL ADHESIVE, AS SHOWN ON SHEET NO. 16.
6. L-823 TYPE II, TWO-CONDUCTOR SECONDARY CONNECTORS SHALL BE CLASS "A" (FACTORY MOLDED).
7. THERE SHALL BE NO SPLICES IN THE SECONDARY CABLE(S) WITHIN THE STEMS OF A RUNWAY/TAXIWAY EDGE/THRESHOLD LIGHTING FIXTURE AND THE WIREWAYS LEADING TO TAXIWAY SIGNS AND PAPI/REIL EQUIPMENT.
8. ELECTRICAL INSULATING GREASE SHALL BE APPLIED WITHIN THE L-823, SECONDARY, TWO CONDUCTOR CONNECTORS TO PREVENT WATER ENTRANCE. THESE CONNECTORS SHALL NOT BE TAPED.
9. DEB ISOLATION TRANSFORMERS SHALL BE BURIED AT A DEPTH OF TEN (10") INCHES ON A LINE CROSSING THE LIGHT AND PERPENDICULAR TO THE RUNWAY/TAXIWAY CENTERLINE AT A LOCATION TWELVE (12") INCHES FROM THE LIGHT OPPOSITE FROM THE RUNWAY/TAXIWAY.
10. A SLACK OF THREE (3') FEET, MINIMUM, SHALL BE PROVIDED IN THE PRIMARY CABLE AT EACH TRANSFORMER/CONNECTOR TERMINATION. AT STAKE-MOUNTED LIGHTS, THE SLACK SHALL BE LOOSELY COILED IMMEDIATELY BELOW THE ISOLATION TRANSFORMER.
11. DIRECTION OF PRIMARY CABLES SHALL BE IDENTIFIED BY COLOR CODING AS FOLLOWS: WHEN FACING LIGHT WITH BACK TO PAVEMENT, CABLE TO THE LEFT IS CODED RED AND CABLE TO RIGHT IS CODED BLUE. THIS APPLIES TO STAKE MOUNTED LIGHTS AND BASE MOUNTED LIGHTS WHERE THE BASE HAS ONLY ONE ENTRANCE.
12. L-867 BASES SHALL BE SIZE B, 24" DEEP, CLASS I, UNLESS OTHERWISE NOTED.
13. BASE MOUNTED BREAKABLE COUPLINGS SHALL NOT HAVE WEEP HOLES TO THE OUTSIDE. PLUGGED UP HOLES SHALL NOT BE ACCEPTABLE. IT SHALL BE A 1/4" DIAMETER, MINIMUM, OR EQUIVALENT OPENING FOR DRAINAGE FROM THE SPACE AROUND THE SECONDARY CONNECTOR INTO THE L-867 BASE.
14. THE ELEVATION OF THE BREAKABLE COUPLING GROOVE SHALL NOT EXCEED 1-1/2" ABOVE THE EDGE OF THE COVER IN CASE OF BASE MOUNTED COUPLINGS, OR THE TOP OF THE STAKE IN CASE OF STAKE MOUNTED COUPLINGS.

15. WHERE THE BREAKABLE COUPLING IS NOT AN INTEGRAL PART OF THE LIGHT FIXTURE STEM OR MOUNTING LEG, A BEAD OF SILICON SEAL SHALL BE APPLIED COMPLETELY AROUND LIGHT STEM OR WIREWAY AT BREAKABLE COUPLING TO PROVIDE A WATERTIGHT SEAL.
16. TOPS OF THE STAKES SUPPORTING LIGHT FIXTURES SHALL BE FLUSH WITH THE SURROUNDING GRADE.
17. PLASTIC LIGHTING FIXTURE COMPONENTS, SUCH AS LAMP HEADS, STEMS, BREAKABLE COUPLINGS, BASE COVERS, BRACKETS, STAKES, SHALL NOT BE ACCEPTABLE.
18. THE TOLERANCE FOR THE HEIGHT OF RUNWAY/TAXIWAY EDGE LIGHTS SHALL BE: ONE (1) INCH. IN CASE OF STAKE MOUNTED LIGHTS, THE SPECIFIED LIGHTING FIXTURE HEIGHT SHALL BE MEASURED BETWEEN THE TOP OF THE STAKE AND THE TOP OF THE LENS. IN CASE OF BASE MOUNTED LIGHTS, THE SPECIFIED LIGHTING FIXTURE HEIGHT SHALL BE MEASURED BETWEEN THE TOP OF THE BASE FLANGE AND THE TOP OF THE LENS, THUS INCLUDING THE BASE COVER, THE FRANGIBLE COUPLING, THE STEM, THE LAMP HOUSING AND THE LENS.
19. THE TOLERANCE FOR THE LATERAL SPACING (LIGHT LANE TO RUNWAY/TAXIWAY CENTERLINE) OF RUNWAY/TAXIWAY EDGE LIGHTS SHALL BE ONE (1) INCH. THIS ALSO APPLIES AT INTERSECTIONS TO LATERAL SPACING BETWEEN LIGHTS OF A RUNWAY/TAXIWAY AND THE INTERSECTING RUNWAY/TAXIWAY.
20. ENTRANCES INTO L-867 BASES SHALL BE SEALED WITH HEAT SHRINK AS SHOWN IN DETAIL "B" ON SHEET NO. 15.
21. GALVANIZED/PAINTED EQUIPMENT/COMPONENT SURFACES SHALL NOT BE DAMAGED BY DRILLING, FILING, ETC. DRAIN HOLES IN METAL TRANSFORMER HOUSINGS SHALL BE MADE BEFORE GALVANIZING.
22. EDGE LIGHT NUMBERING TAGS SHALL BE FACING THE PAVEMENT.
23. CABLE/SPLICE/DUCT MARKERS SHALL BE PRECAST CONCRETE OF THE SIZE SHOWN. LETTERS/NUMBERS/ARROWS FOR THE LEGEND TO BE IMPRESSED INTO THE TOPS OF THE MARKERS SHALL BE PRE-ASSEMBLED AND SECURED IN THE MOLD BEFORE THE CONCRETE IS POURED. LEGEND INSCRIBED BY HAND IN WET CONCRETE SHALL NOT BE ACCEPTABLE.
24. ALL UNDERGROUND CABLE RUNS SHALL BE IDENTIFIED BY CABLE MARKERS AT 200 FEET MAXIMUM SPACING, WITH AN ADDITIONAL MARKER AT EACH CHANGE OF DIRECTION OF THE CABLE RUN. CABLE MARKERS SHALL BE INSTALLED IMMEDIATELY ABOVE THE CABLES.
25. THERE SHALL BE NO SPLICES BETWEEN THE ISOLATION TRANSFORMERS. L-823 CONNECTORS ARE ALLOWED AT TRANSFORMER CONNECTIONS ONLY, UNLESS OTHERWISE SHOWN.
26. APPLY AN OXIDE INHIBITING, ANTI-SEIZING COMPOUND TO ALL SCREWS, NUTS AND BREAKAGE COUPLING THREADS.
27. LOCATIONS OF ENDS OF ALL UNDERGROUND DUCTS SHALL BE IDENTIFIED BY DUCT MARKERS.
28. WHERE A PARALLEL, CONSTANT VOLTAGE PAPI SYSTEM IS PROVIDED, THE "T" SPLICES SHALL BE OF THE CAST TYPE.
29. CONCRETE USED FOR SLABS, FOOTINGS, BACKFILL AROUND TRANSFORMER HOUSINGS, MARKINGS, ETC. SHALL BE 3000 PSI, AIR-ENTRAINED.
30. ALL POWER AND CONTROL CABLES IN MAN/HAND HOLES SHALL BE TAGGED. USE EMBOSSED COPPER STRIPS TO BE ATTACHED AT BOTH ENDS TO THE CABLE BY THE USE OF PLASTIC STRAPS. MINIMUM OF TWO TAGS SHALL BE PROVIDED ON EACH CABLE IN A MAN/HAND HOLE-ONE AT THE CABLE ENTRANCE AND ONE AT THE CABLE EXIT.

GROUNDING NOTES

1. ALL GROUND CONNECTIONS TO GROUND RODS, BUSES, PANELS, ETC. SHALL BE MADE WITH PRESSURE TYPE SOLDERLESS LUGS AND GROUND CLAMPS SOLDERED OR BOLT AND WASHER TYPE CONNECTIONS ARE NOT ACCEPTABLE. CLEAN ALL METAL SURFACES BEFORE MAKING GROUND CONNECTIONS. CONNECTIONS TO GROUND RODS SHALL BE EXOTHERMIC WELD WHERE SPECIFIED HEREIN.
2. TOP OF GROUND RODS SHALL BE TEN (10) INCHES BELOW GRADE, UNLESS SPECIFIED OTHERWISE HEREIN, FOR RESPECTIVE APPLICATIONS.
3. THE RESISTANCE TO GROUND OF THE VAULT GROUNDING SYSTEM WITH THE COMMERCIAL POWER LINE NEUTRAL DISCONNECTED SHALL NOT EXCEED 10 OHMS.

JUL 11, 2006 11:20 PM RAW
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DATE	REVISION	BY

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS

QUINCY Regional Airport
 Adams County, Illinois

A.I.P. PROJ.: 3-17-0085-XX
 I.L. PROJ.: UIN-3338

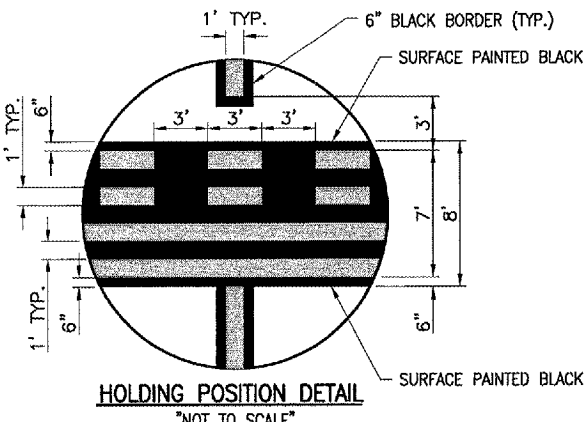
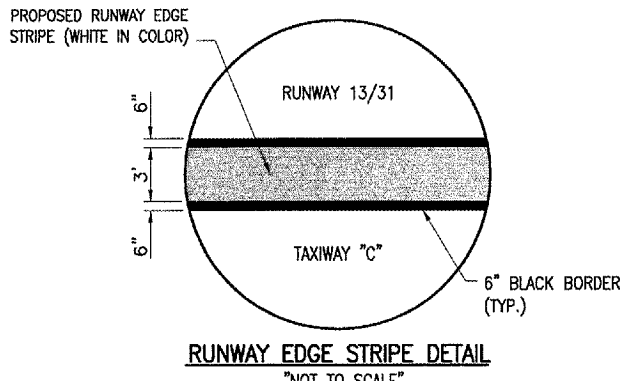
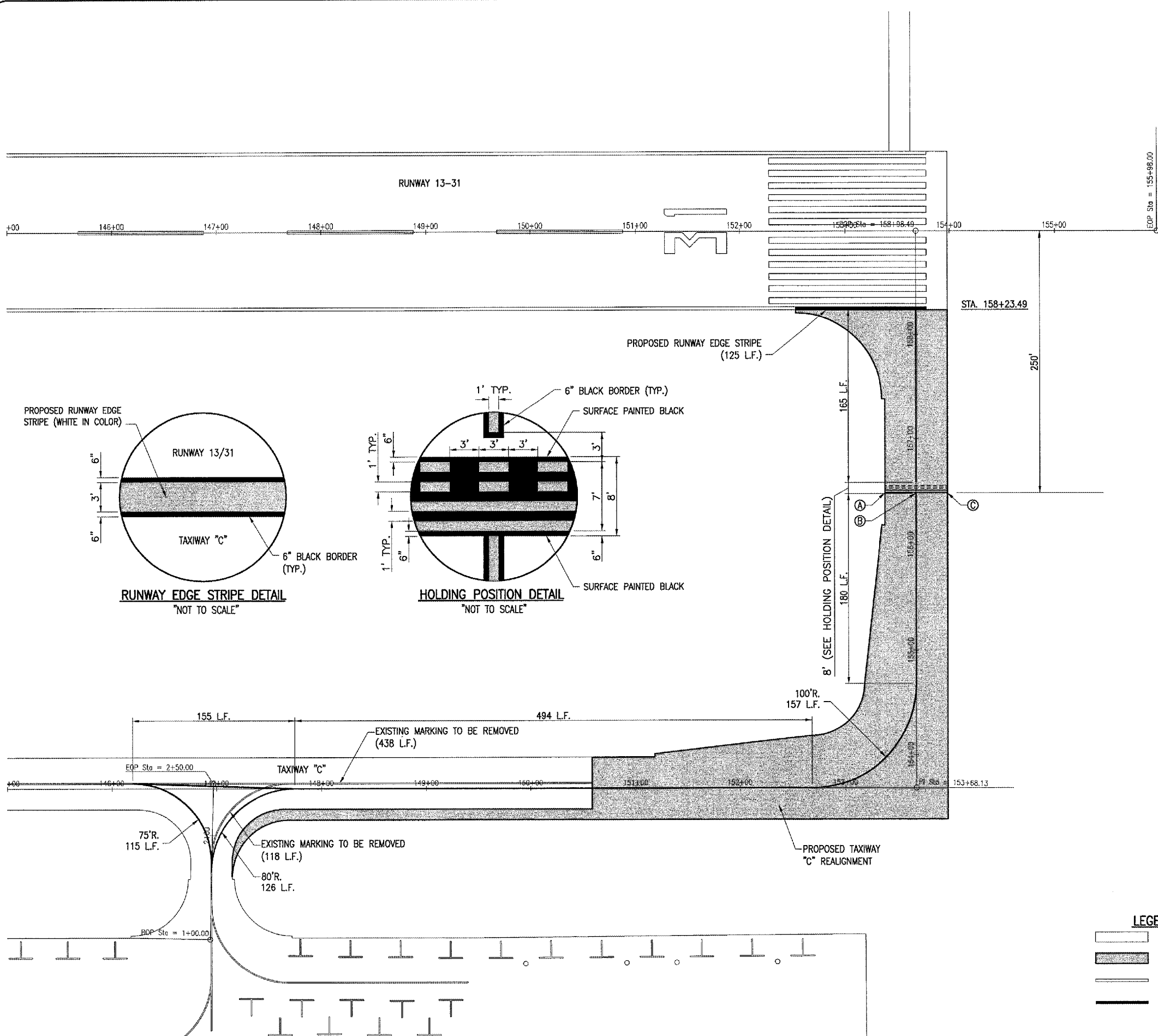
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Filename R-544ELEC.DWG	Scale N/A
Date 06/19/06	Drawn BAK
	Reviewed RAW
	Date 07/12/06



REALIGN AND WIDEN TAXIWAY "C"

PROPOSED ELECTRICAL NOTES

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MARKING NOTES

ALL RUNWAY MARKING WILL BE SOLID AND WHITE IN COLOR. THE RUNWAY EDGE STRIPE WILL BE 3 FT IN WIDTH.

THE TAXIWAY CENTERLINE STRIPE AND HOLDING POSITION MARKING WILL BE SOLID AND YELLOW IN COLOR. THE TAXIWAY CENTERLINE WILL BE 1 FT IN WIDTH AND REQUIRE TWO APPLICATIONS. THE HOLDING POSITION MARKINGS WILL BE AS SPECIFIED IN THE DETAILS SHOWN ON THIS SHEET.

ALL PROPOSED MARKING WILL BE LOCATED AS SHOWN ON THIS SHEET.

ALL YELLOW & WHITE PROPOSED MARKING WILL HAVE A REFLECTIVE MEDIA APPLIED IN ACCORDANCE WITH THE SPECIFICATIONS.

BLACK MARKING WILL NOT REQUIRE REFLECTIVE MEDIA.

CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

ALL PROPOSED MARKING ON EXISTING AND PROPOSED PAVEMENT SHALL HAVE A 6 INCH WIDE (MIN.) BLACK OUTLINE.

ALL PROPOSED MARKING ITEMS WILL BE PAID FOR AS FOLLOWS:
 AR620520 "PAVEMENT MARKING-WATERBORNE" - PER SQ. FT.
 AR620525 "PAVEMENT MARKING-BLACK BORDER" - PER SQ. FT.

WHERE CALLED FOR, THE CENTERLINE TAXIWAY WILL BE REMOVED BY SAND BLASTING, OR WATER BLASTING, OR OTHER APPROVED METHODS.

ALL PERMANENT MARKING REMOVAL ITEMS WILL BE PAID FOR AS FOLLOWS:
 AR620900 "PAVEMENT MARKING REMOVAL" - PER SQ. FT.

COORDINATE DATA

POINT ID.	STATION	OFFSET	NORTHING	EASTING
A	156+48.49	30' LT.	1191805.3875	2010248.9904
B	156+48.49	C	1191783.8114	2010269.8344
C	156+48.49	30' RT.	1191762.2353	2010290.6784

MARKING QUANTITIES - AR620520

DESCRIPTION	TOTAL (S.F.)
TAXIWAY CENTERLINE STRIPING - YELLOW	1,392
TAXIWAY HOLDING POSITION - YELLOW	180
TOTAL YELLOW:	1,572
RUNWAY EDGE STRIPE - WHITE	375
TOTAL WHITE:	375
TOTAL MARKING:	1,947

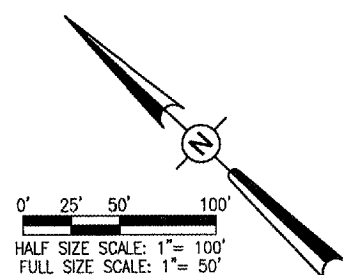
MARKING QUANTITIES - AR620525

DESCRIPTION	TOTAL (S.F.)
TAXIWAY CENTERLINE OUTLINE - BLACK	1,396
TAXIWAY HOLDING POSITION OUTLINE - BLACK	480
RUNWAY EDGE STRIPE - BLACK	127
TOTAL BLACK:	2,003

MARKING REMOVAL QUANTITIES - AR620900

DESCRIPTION	TOTAL (S.F.)
TAXIWAY CENTERLINE STRIPING - YELLOW	556
TAXIWAY CENTERLINE OUTLINE - BLACK	556
TOTAL REMOVAL:	1,112

- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED IMPROVEMENTS
 - EXISTING MARKING
 - PROPOSED MARKING



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QUINCY Regional Airport
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 A.I.P. PROJ.: 3-17-0085-XX

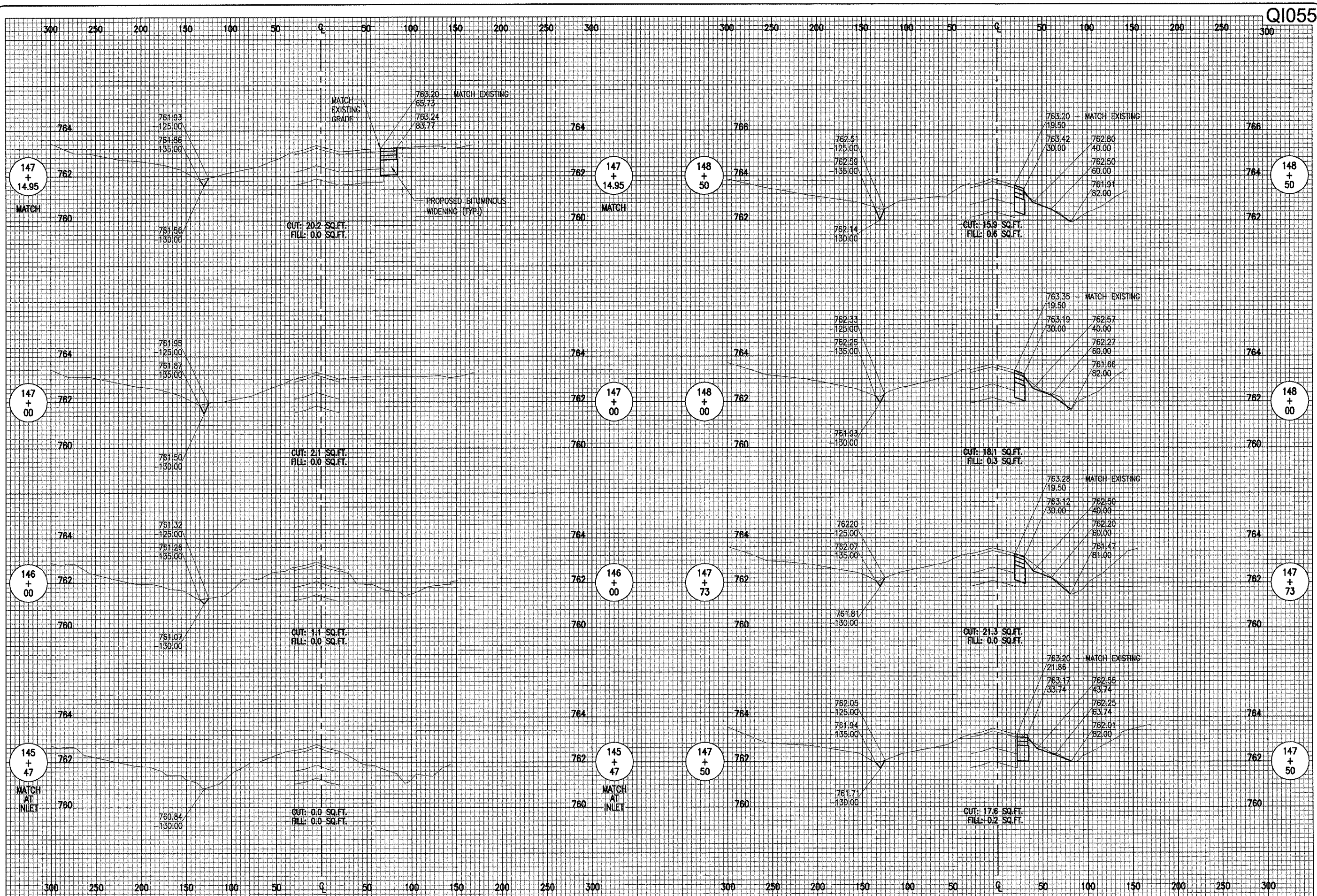
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 Date: 06/19/06

LAYOUT	RAW	06/19/06
DRAWN	BAK	06/19/06
REVIEWED	RAW	07/12/06

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REALIGN AND
 WIDEN TAXIWAY "C"
 PROPOSED
 MARKING
 PLAN

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QUINCY Regional Airport
 Adams County, Illinois

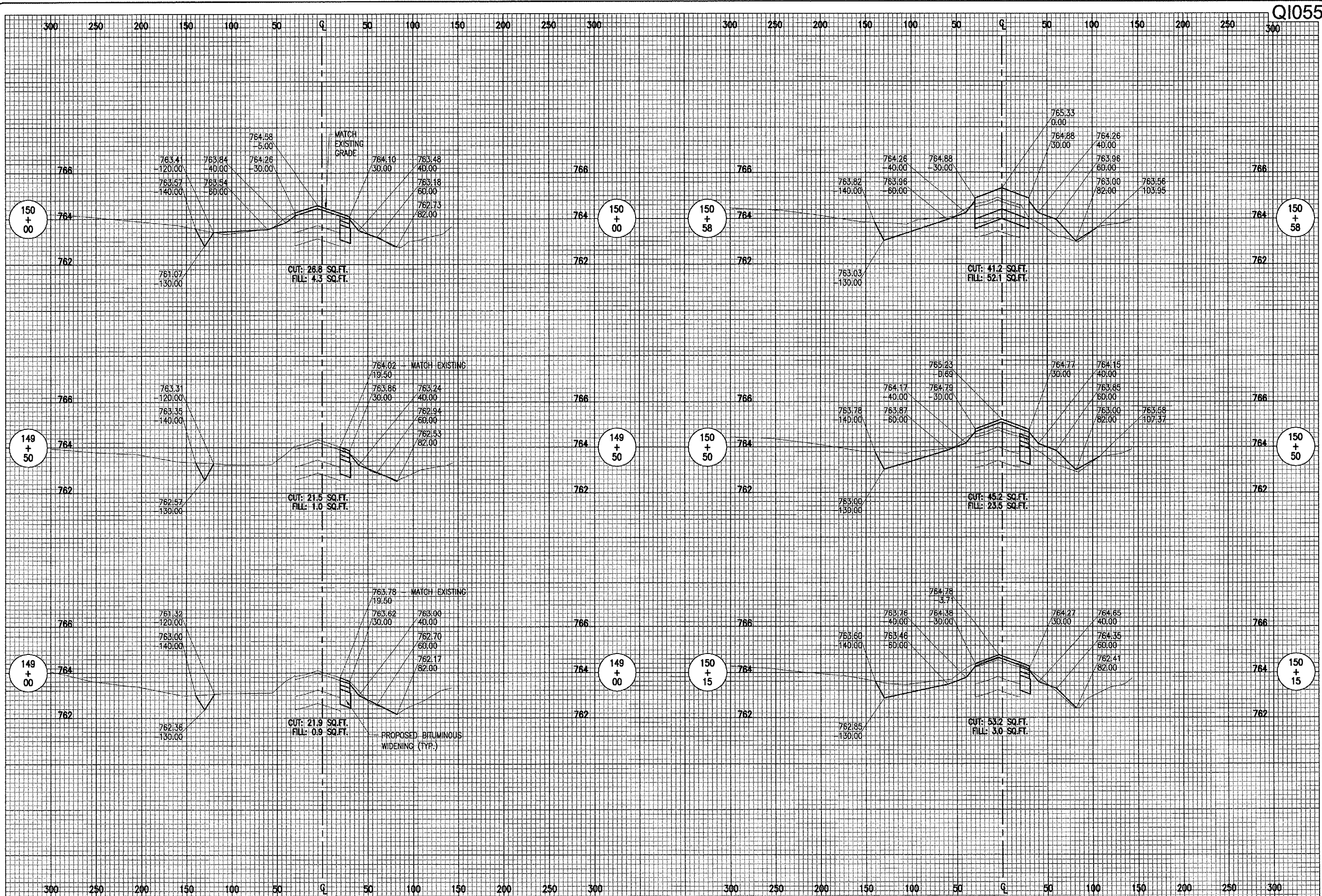
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Scale 1" = 50'H. & 2'V.	RAW	07/12/06
Date 06/21/06		



REALIGN AND
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 PROPOSED
 CROSS-SECTIONS
 STA. 145+47 TO STA. 148+50

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REVISION	
DATE	

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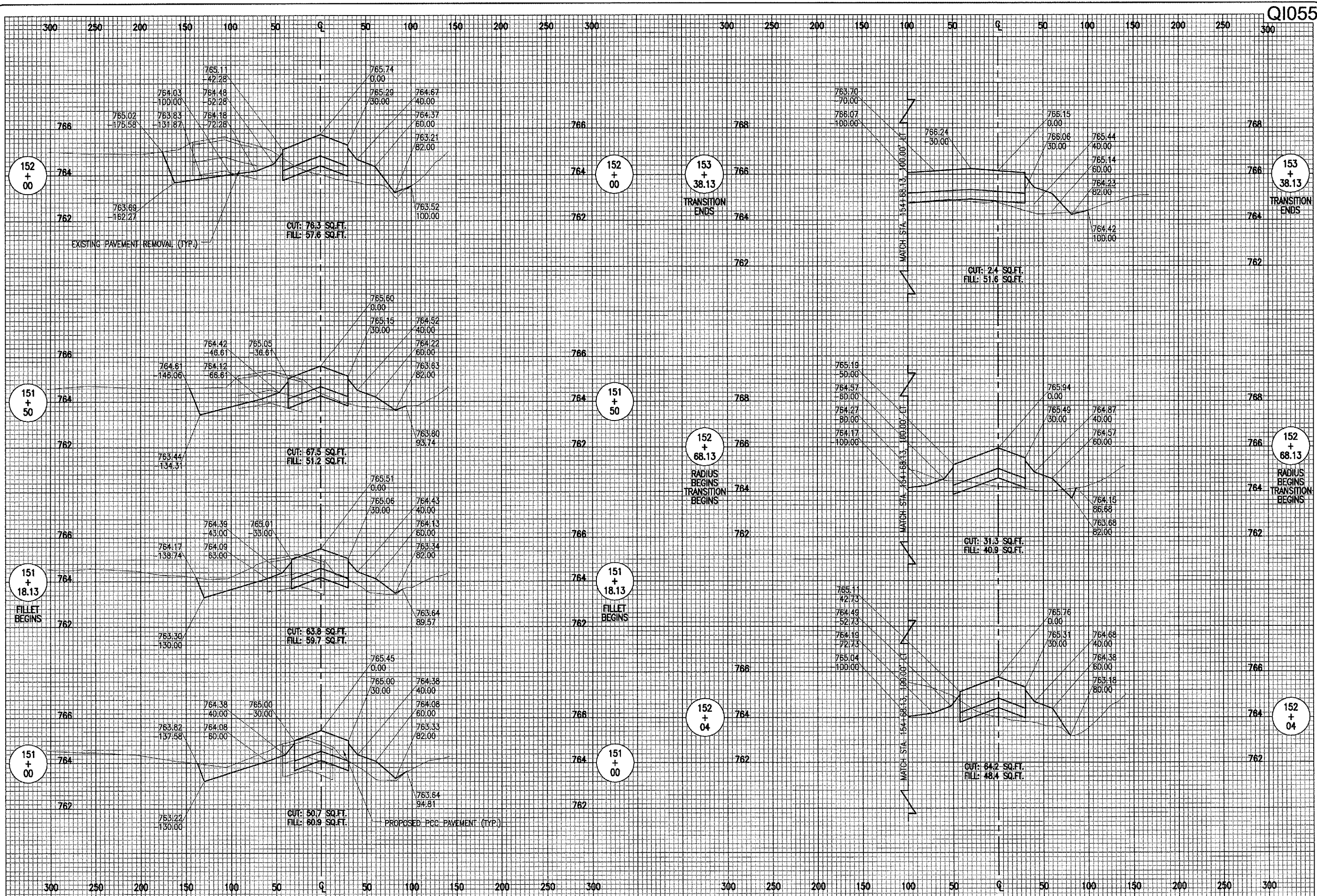
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DRAWN	CCC	06/21/06
REVIEWED	RAW	07/12/06

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 STA. 149+00 TO STA. 150+58**

21
 21 of 24 sheets

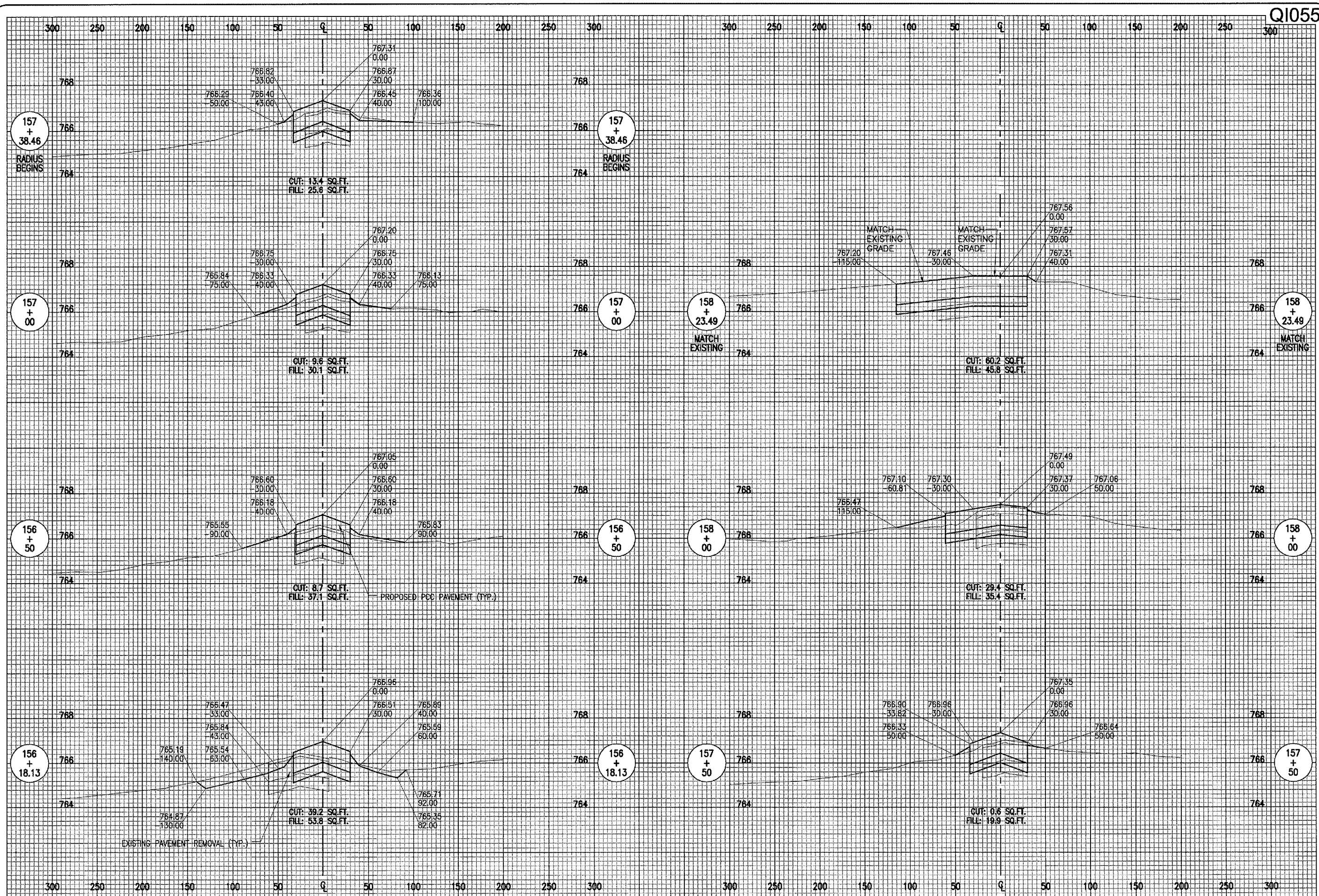
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DATE	
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS	
IL PROJ.: UIN-3338 A.I.P. PROJ.: 3-17-0085-XX	
IET Project No. 82406TCD Filename R-303XS.DWG Scale 1" = 50'H. & 2" Date 06/21/06	LAYOUT RAW 06/19/06 DRAWN CCC 06/21/06 REVIEWED RAW 07/12/06
Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 Offices Nationwide	
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22 22 of 24 sheets	

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QUINCY REGIONAL AIRPORT
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 ADAMS COUNTY, ILLINOIS

QUINCY Regional Airport
 1100 W. PARK

IL PROJ.: UIN-3338 A.I.P. PROJ.: 3-17-0085-XX

FILE PROJECT NO.	DATE	BY
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R-305XS.DWG	06/21/06	CCC
1" = 50'H. & 2".	07/12/06	RAW

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 CROSS-SECTIONS
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