

ITEM 18A

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF AERONAUTICS
 CONSTRUCTION PLANS

FOR

QUAD-CITY
 INTERNATIONAL
 AIRPORT

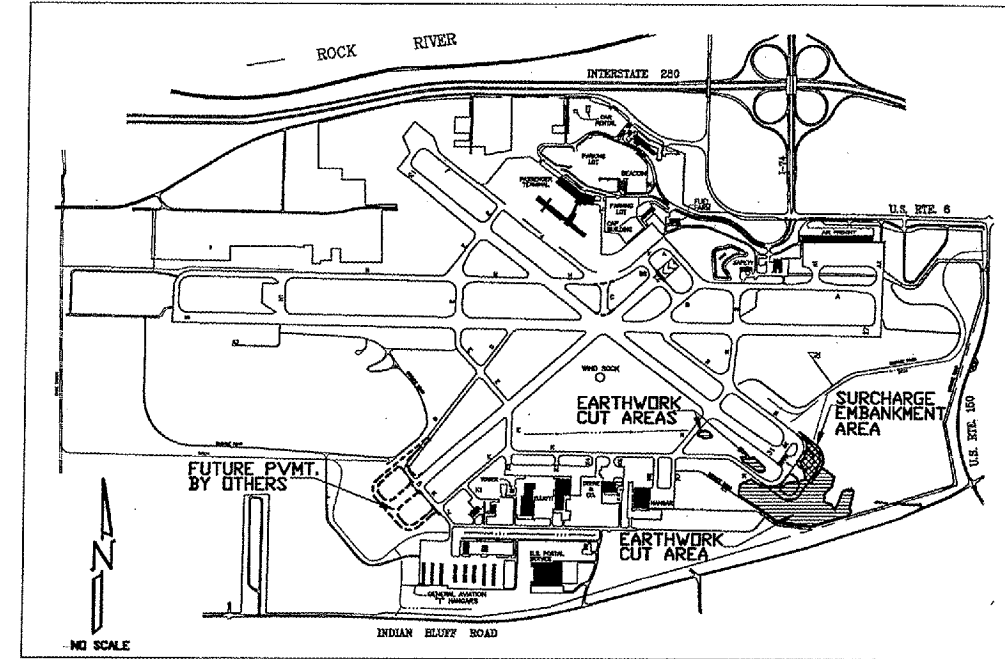
ROCK ISLAND COUNTY, ILLINOIS

RUNWAY 31 EXTENSION - EARTHWORK

EXCAVATION AND CONSTRUCTION OF 8' SURCHARGE EMBANKMENT (TAXIWAY B ONLY) FOR FUTURE EXTENSIONS TO RUNWAY 31 (300' X 150'), TAXIWAY B (650' X 90') AND TAXIWAY K (575' X 75') ALONG WITH ASSOCIATED UTILITY ADJUSTMENTS AND TURFING.

ILLINOIS PROJECT MLI-3564
 A.I.P. PROJECT NO. 3-17-0068-XX
 AIRPORT CLASSIFICATION - AIR CARRIER

AIRCRAFT APPROACH CATEGORY - C
 AIRPLANE DESIGN GROUP - IV
 LATITUDE 41°26'52.4", LONGITUDE 90°30'33.9", ELEVATION 589'M.S.L.

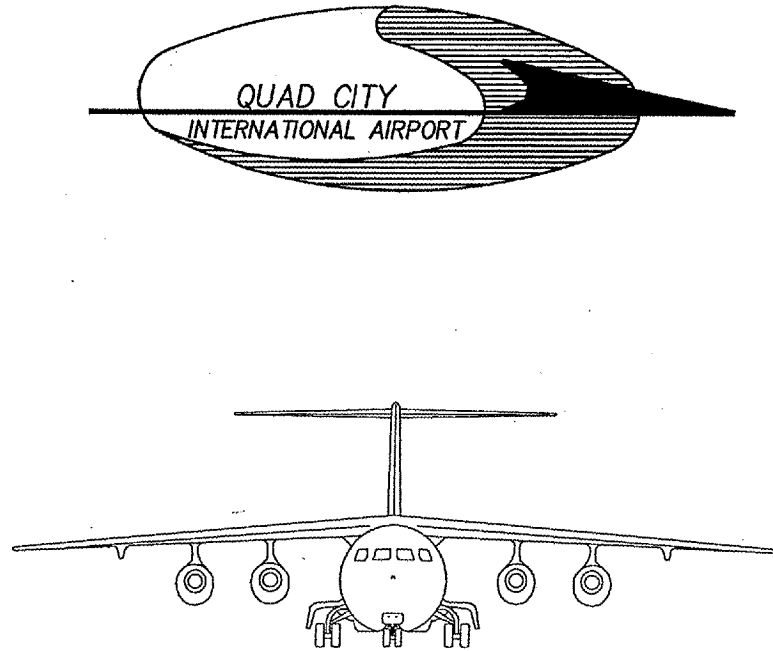


PROJECT LOCATION MAP

WARNING



CALL BEFORE YOU DIG



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CONSULTING ENGINEERS MISSMAN, STANLEY & ASSOC. ENGINEER'S SIGNATURE <i>Dennis R. Martin</i> SEALED & SIGNED 6/27/06 DATE OF LICENSE EXPIRATION	METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND COUNTY, ILLINOIS <i>Bruce Carter</i> 06/28/06 DATE APPROVED

JUNE 30, 2006 OFFICIAL DATE OF PLANS	PREPARED BY MISSMAN, STANLEY & ASSOCIATES Consulting Civil Engineers ROCK ISLAND, ILLINOIS
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SUMMARY OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITIES	
			AS AWARDED	AS BUILT
AR107408	L-806 WIND CONE - 8' LIGHTED	EACH	6	
AR108258	2/C #8 5KV UG CABLE IN UD	L.F.	990	
AR125565	SPLICE CAN	EACH	6	
AR125944	ADJUST TAXI GUIDANCE SIGN	EACH	1	
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150530	TRAFFIC MAINTENANCE	L.S.	1	
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	44,250	
AR156500	TEMPORARY EROSION CONTROL	L.S.	1	
AR156543	RIPRAP - GRADATION NO. 3	S.Y.	3,415	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	2,480	
AR209511	CRUSHED AGGREGATE BASE (CA-1)	TON	100	
AR209600	GEOTEXTILE FABRIC	S.Y.	3,415	
AR701512	12" RCP, CLASS IV	L.F.	343	
AR701536	36" RCP, CLASS IV	L.F.	3	
AR701710	RCEP SPAN 23 RISE 14	L.F.	243	
AR751415	INLET - SPECIAL	EACH	3	
AR751567	MANHOLE 7'	EACH	2	
AR751943	ADJUST MANHOLE	EACH	1	
AR751980	RECONSTRUCT INLET	EACH	1	
AR751983	RECONSTRUCT MANHOLE	EACH	1	
AR751992	RECONSTRUCT SPECIAL STRUCTURE	EACH	1	
AR752412	PRECAST REINFORCED CONC. FES 12"	EACH	2	
AR752850	SPECIAL STRUCTURE	EACH	2	
AR801604	SETTLEMENT PLATES	EACH	5	
AR901510	SEEDING	ACRE	25	
AR908513	MULCHING - METHOD 3	ACRE	25	
AR908520	EXCELSIOR BLANKET	S.Y.	1,575	

GENERAL NOTES:

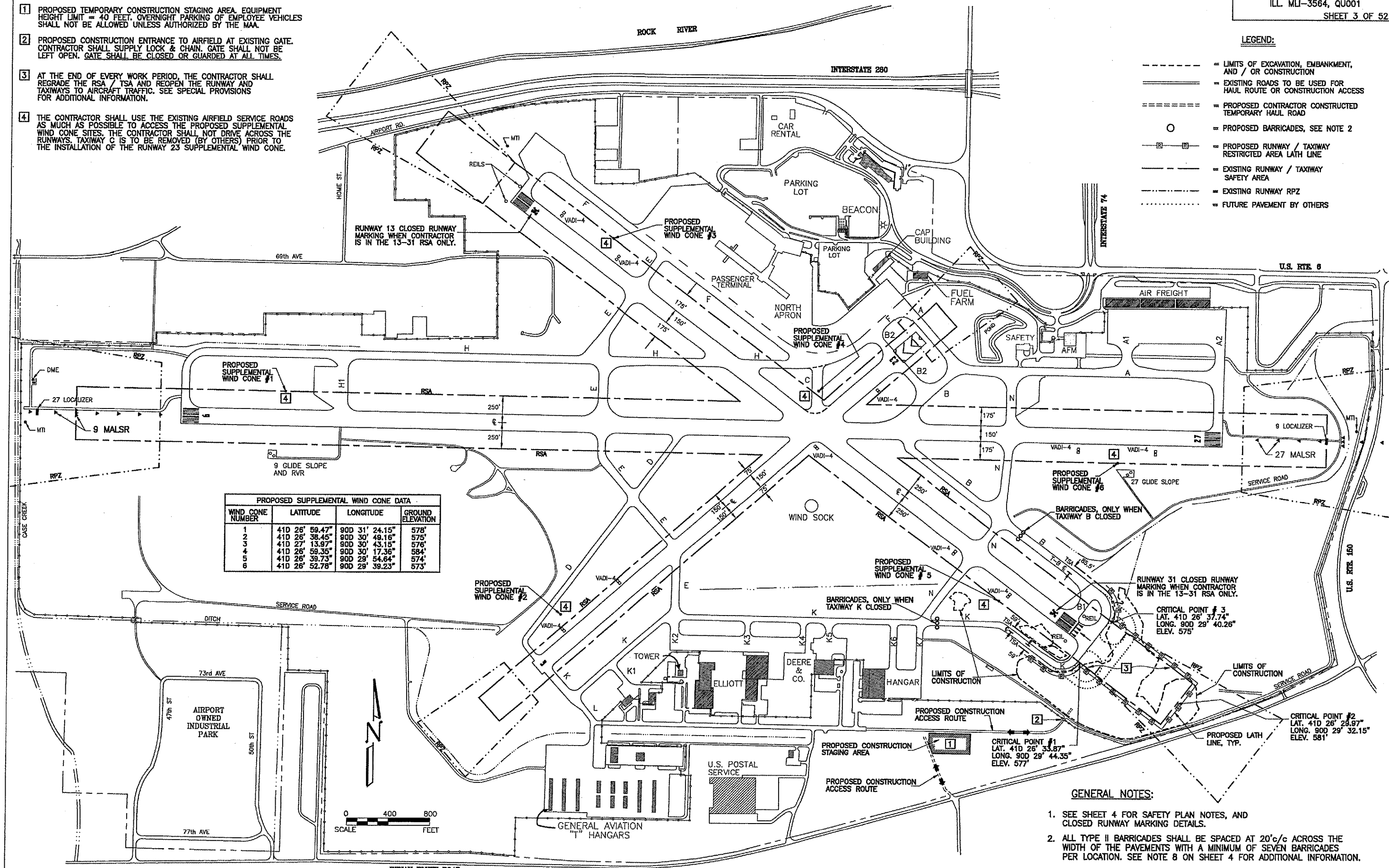
1. THE CONTRACTOR SHALL SALVAGE EXISTING AIRFIELD LIGHTING EQUIPMENT AS DETAILED IN THE CONSTRUCTION PLANS AND SPECIAL PROVISIONS PRIOR TO THE START OF EARTHWORK AND/OR PAVING ACTIVITIES. SALVAGED EQUIPMENT SHALL BE CLEANED AND REUSED ON THE AIRPORT OR DELIVERED TO THE METROPOLITAN AIRPORT AUTHORITY.
2. CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS APPROVED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
3. THE CONTRACTOR SHALL EXCAVATE TEMPORARY EROSION CONTROL DRAINAGE SWALES, AS REQUIRED BY THE RESIDENT ENGINEER, TO CONTROL STORM WATER RUN-OFF.
4. THE CONTRACT ITEM 152 - UNCLASSIFIED EXCAVATION SHALL INCLUDE ALL COSTS ASSOCIATED WITH EXCAVATION OF SOILS, HAULING OF SOILS, STOCKPILING SOILS, INSTALLATION OF SOILS, COMPACTION OF SOILS, GRADING OF SOILS, INSTALLATION AND REMOVAL OF HAUL ROADS OR ROUTES, RESTORATION OF HAUL ROADS OR ROUTES, DISPOSAL OF WASTE SOILS, CLEANING OF PAVEMENTS, AND ALL OTHER ITEMS THAT ARE REQUIRED TO COMPLETE THE EARTHWORK. THIS ITEM SHALL BE PAID FOR BASED UPON THE CUBIC YARDS OF MATERIALS REMOVED AS ACCEPTED BY THE ENGINEER. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
5. AFTER CONSTRUCTION HAS BEEN COMPLETED, THE CONTRACTOR SHALL SEED AND HYDRAULIC MULCH ALL DISTURBED AREAS PER SPECIAL PROVISIONS 901 AND 908. ONLY SEEDING AND MULCHING AREAS WITHIN THE LIMITS OF CONSTRUCTION/SEEDING WILL BE ELIGIBLE FOR PAYMENT UNDER THESE CONTRACT PAY ITEMS. AREAS OUTSIDE OF THE LIMITS OF CONSTRUCTION/SEEDING SHALL BE SEEDED AND MULCHED BY THE CONTRACTOR PER SPECIAL PROVISION 901/908, BUT SHALL NOT BE MEASURED FOR PAYMENT.
6. ITEM AR908513 MULCHING - METHOD 3 SHALL BE ACCOMPLISHED FOLLOWING THE METHODS AND PROCEDURES OUTLINED IN THE IDOT-DOA SUPPLEMENTAL SPECIFICATIONS AND IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 251, METHOD 3 (HYDRAULIC MULCH).
7. ITEM AR908520 EXCELSIOR BLANKET SHALL BE ACCOMPLISHED FOLLOWING THE METHODS AND PROCEDURES OUTLINED IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 251, EROSION CONTROL BLANKET (EXCELSIOR BLANKET). THE LOCATION OF THE PROPOSED EXCELSIOR BLANKET SHALL BE DETERMINED BY THE RESIDENT ENGINEER, IN THE FIELD, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

NUMBERED LEGEND:

- 1 PROPOSED TEMPORARY CONSTRUCTION STAGING AREA. EQUIPMENT HEIGHT LIMIT = 40 FEET. OVERNIGHT PARKING OF EMPLOYEE VEHICLES SHALL NOT BE ALLOWED UNLESS AUTHORIZED BY THE MAA.
- 2 PROPOSED CONSTRUCTION ENTRANCE TO AIRFIELD AT EXISTING GATE. CONTRACTOR SHALL SUPPLY LOCK & CHAIN. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE CLOSED OR GUARDED AT ALL TIMES.
- 3 AT THE END OF EVERY WORK PERIOD, THE CONTRACTOR SHALL REGRADE THE RSA / TSA AND REOPEN THE RUNWAY AND TAXIWAYS TO AIRCRAFT TRAFFIC. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
- 4 THE CONTRACTOR SHALL USE THE EXISTING AIRFIELD SERVICE ROADS AS MUCH AS POSSIBLE TO ACCESS THE PROPOSED SUPPLEMENTAL WIND CONE SITES. THE CONTRACTOR SHALL NOT DRIVE ACROSS THE RUNWAYS. TAXIWAY C IS TO BE REMOVED (BY OTHERS) PRIOR TO THE INSTALLATION OF THE RUNWAY 23 SUPPLEMENTAL WIND CONE.

LEGEND:

- - - - - LIMITS OF EXCAVATION, EMBANKMENT, AND / OR CONSTRUCTION
- =====
===== EXISTING ROADS TO BE USED FOR HAUL ROUTE OR CONSTRUCTION ACCESS
- ===== PROPOSED CONTRACTOR CONSTRUCTED TEMPORARY HAUL ROAD
- PROPOSED BARRICADES, SEE NOTE 2
- ▣ PROPOSED RUNWAY / TAXIWAY RESTRICTED AREA LATH LINE
- — — — — EXISTING RUNWAY / TAXIWAY SAFETY AREA
- — — — — EXISTING RUNWAY RPZ
- FUTURE PAVEMENT BY OTHERS



PROPOSED SUPPLEMENTAL WIND CONE DATA

WIND CONE NUMBER	LATITUDE	LONGITUDE	GROUND ELEVATION
1	41D 26' 59.47"	90D 31' 24.15"	578'
2	41D 26' 38.45"	90D 30' 49.16"	575'
3	41D 27' 13.97"	90D 30' 43.15"	576'
4	41D 26' 59.35"	90D 30' 17.36"	584'
5	41D 26' 39.73"	90D 29' 54.64"	574'
6	41D 26' 52.78"	90D 29' 39.23"	573'

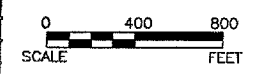
CRITICAL POINT # 3
 LAT. 41D 26' 37.74"
 LONG. 90D 29' 40.26"
 ELEV. 575'

CRITICAL POINT # 2
 LAT. 41D 26' 29.57"
 LONG. 90D 29' 32.15"
 ELEV. 581'

CRITICAL POINT # 1
 LAT. 41D 26' 33.87"
 LONG. 90D 29' 44.35"
 ELEV. 577'

GENERAL NOTES:

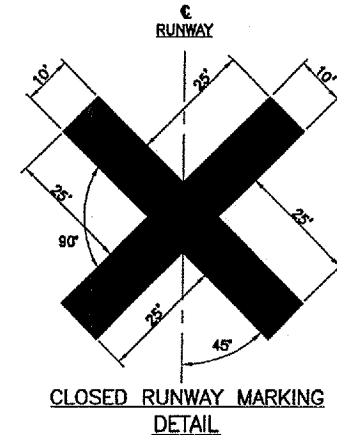
1. SEE SHEET 4 FOR SAFETY PLAN NOTES, AND CLOSED RUNWAY MARKING DETAILS.
2. ALL TYPE II BARRICADES SHALL BE SPACED AT 20'c/c ACROSS THE WIDTH OF THE PAVEMENTS WITH A MINIMUM OF SEVEN BARRICADES PER LOCATION. SEE NOTE 8 ON SHEET 4 FOR ADDITIONAL INFORMATION.
3. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.



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SAFETY PLAN NOTES:

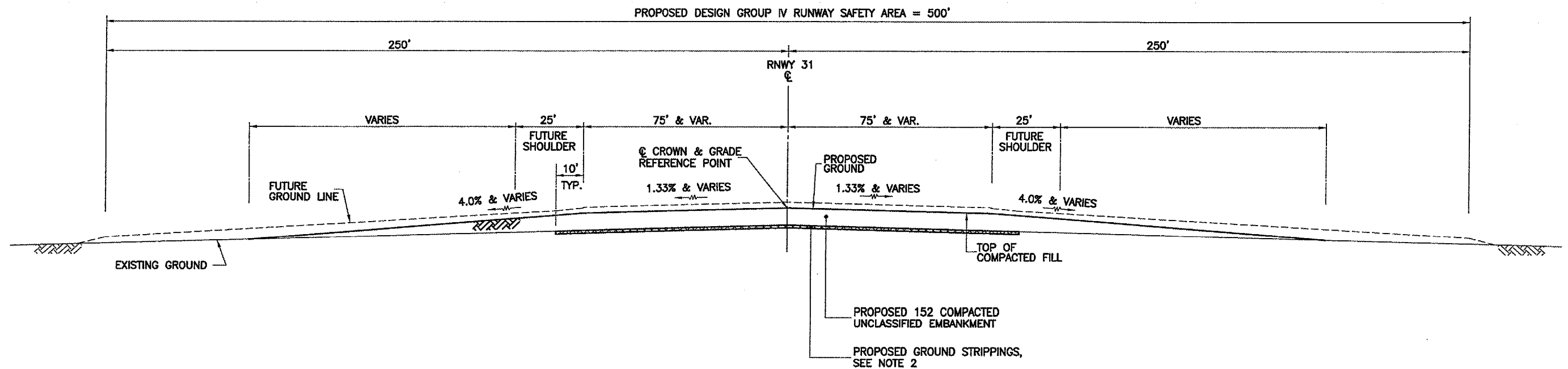
1. THE METROPOLITAN AIRPORT AUTHORITY (MAA) SHALL DETERMINE WHEN AND WHERE THE CONTRACTOR WILL BE ALLOWED TO WORK. THE CONTRACTOR SHALL BE PREPARED TO EXIT THE RESTRICTED AREAS, SAFETY AREAS AND/OR AIR FIELD AT ALL TIMES WHEN DIRECTED TO DO SO BY THE MAA. ALL PAVEMENTS SHALL BE OPEN TO AIRCRAFT TRAFFIC UNLESS "NOTAMED" OTHERWISE DURING THE CONSTRUCTION OF THIS PROJECT. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION ON WORKING RESTRICTIONS AND CONDITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF ALL BARRICADES AND LATH LINES AS SHOWN, PRIOR TO ANY EQUIPMENT ENTERING THE AIRFIELD OUTSIDE OF THE CONSTRUCTION STAGING AREA.
2. SOLID CLOSED RUNWAY MARKING CROSSES ARE REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES. SEE SPECIAL PROVISIONS FOR DETAILS OF CROSSES.
3. THE CONTRACTOR SHALL NOT TRAVEL IN OR THROUGH THE RESTRICTED AREAS AND/OR SAFETY AREAS UNLESS PERMISSION IS RECEIVED AND CONTACT HAS BEEN MADE WITH THE FAA CONTROL TOWER.
4. THE CONTRACTOR'S EMPLOYEES SHALL PARK IN THE PROPOSED CONSTRUCTION STAGING AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO THE PROPOSED CONSTRUCTION SITE. ONLY CONTRACTOR'S MARKED VEHICLES AND EQUIPMENT SHALL BE ALLOWED ON THE AIRFIELD. ALL CONSTRUCTION VEHICLES AND EQUIPMENT, EXCEPT THE PAVING TRAIN, SHALL BE PARKED IN THE CONSTRUCTION STAGING AREA DURING ALL NON-WORKING HOUR. THE PAVING TRAIN MAY BE PARKED ON THE AIRFIELD OUTSIDE ALL RESTRICTED AREAS IN A LOCATION AUTHORIZED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL LOCATE HIS TRAILER, THE ENGINEERS FIELD OFFICE, AND ALL OTHER NECESSARY FACILITIES AND MATERIALS IN THE PROPOSED CONSTRUCTION STAGING AREA. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONSTRUCT WHATEVER ACCESS ROAD HE DEEMS NECESSARY BETWEEN THE EXISTING ROADS AND THE CONSTRUCTION AREAS. OVERNIGHT PARKING OF EMPLOYEE VEHICLES IN THE CONSTRUCTION STAGING AREA WILL NOT BE ALLOWED UNLESS AUTHORIZED BY THE MAA.
5. THE CONTRACTOR SHALL PROCURE ENOUGH QUAD CITY AIRPORT SECURITY/IDENTIFICATION BADGES FOR HIS EMPLOYEES AND SUBCONTRACTOR EMPLOYEES FROM THE AIRPORT AUTHORITY TO GUARANTEE AT LEAST ONE MEMBER OF EACH CONSTRUCTION CREW WILL HAVE A BADGE. ANY CONSTRUCTION CREW WITHOUT A BADGED MEMBER SHALL NOT BE ALLOWED ON THE AIRFIELD SITE. ALL INDIVIDUALS WHO ARE ISSUED SECURITY BADGES MUST CORRECTLY WEAR THEIR OWN BADGE WHILE ON THE AIRFIELD. BADGES MAY BE OBTAINED, AFTER MEETING SECURITY REQUIREMENTS, FROM THE SECURITY OFFICE AT THE QCI AIRPORT. A FIFTY DOLLAR (\$50.00) REFUNDABLE DEPOSIT IS REQUIRED FOR EACH BADGE.
6. THE CONTRACTOR SHALL USE THE DESIGNATED HAUL ROUTES, CONSTRUCTION STAGING AREA, AND ENTRANCE TO THE AIRFIELD AS SHOWN ON THE SAFETY PLAN SHEET. NO CONSTRUCTION TRAFFIC SHALL BE ALLOWED ON THE AIRFIELD OUTSIDE THE HAUL ROUTES AND WORK AREAS UNLESS OTHERWISE AUTHORIZED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL SUPPLY AND INSTALL TEMPORARY LOCKS ON EXISTING GATES AT THE PROPOSED CONSTRUCTION ENTRANCE. THE CONTRACTOR SHALL BE RESPONSIBLE TO KEEP THE CONSTRUCTION ENTRANCE GATE CLOSED AT ALL TIMES. VIOLATIONS ARE SUBJECT TO FINES/PENALTIES AND THE CONTRACTOR SHALL PAY ANY FINES INCURRED, INCLUDING FINES INCURRED BY THE RESIDENT ENGINEER AND/OR MAA DUE TO THE CONTRACTOR'S NEGLIGENCE. ALL VEHICULAR TRAFFIC SHALL BE KEPT TO A MINIMUM. ALL VEHICLES ON THE APRONS, RAMPS, TAXIWAYS, OR RUNWAYS REQUIRE THE APPROVAL OF THE RESIDENT ENGINEER / MAA.
7. THE CONTRACTOR AND EACH SUBCONTRACTOR SHALL DESIGNATE, PRIOR TO BEGINNING CONSTRUCTION, A PERSON OR PERSONS WHO CAN BE CONTACTED IN AN EMERGENCY INVOLVING THEIR WORK OR EQUIPMENT. THESE DESIGNATED PEOPLE SHALL BE AVAILABLE ON A 24-HOUR / 7 DAYS PER WEEK BASIS.
8. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO INSTALL TYPE II BARRICADES EQUIPPED WITH FLASHING RED LIGHTS AND 20" X 20" ORANGE FLAGS AS SHOWN ON THE SAFETY PLAN OR AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR WILL FURNISH, MAINTAIN, AND MOVE THE BARRICADES AS REQUIRED BY THE RESIDENT ENGINEER. THE BARRICADES SHALL BE SUFFICIENTLY WEIGHTED WITH SANDBAGS OR OTHER APPROPRIATE METHOD TO WITHSTAND HIGH WINDS AND/OR JET BLAST WITHOUT DISLOCATION. BARRICADES SHALL BE CHECKED DAILY BY THE CONTRACTOR FOR PROPER PLACEMENT, ADEQUATE BALLAST, PROPER LIGHTING, PROPER FLAGGING AND WORKING BATTERIES. ALL DISCREPANCIES SHALL BE CORRECTED IMMEDIATELY. BARRICADES SHALL BE RELOCATED UPON THE COMPLETION OF EACH STAGE OF CONSTRUCTION. COST TO BE INCLUDED IN MAINTENANCE OF TRAFFIC LUMP SUM PRICE.
9. WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRFIELD, THEY SHALL BE PROPERLY MARKED. THE MARKING SHALL CONSIST OF A THREE FOOT (3') SQUARE FLAG WITH A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES OF NOT LESS THAN ONE FOOT (1') ON EACH SIDE, DISPLAYED IN FULL VIEW ABOVE THE VEHICLE OR EQUIPMENT. EACH VEHICLE SHALL HAVE A FLASHING YELLOW LIGHT MOUNTED ON TOP OF THE ROOF.
10. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO RESTORE THE CONSTRUCTION STAGING AREA, SERVICE ROADS, ACCESS ROADS, AND HAUL ROADS TO THEIR ORIGINAL CONDITIONS FOLLOWING COMPLETION OF CONSTRUCTION. THIS MAY INCLUDE, BUT NOT BE LIMITED TO, REGRADING, FERTILIZING, SEEDING AND MULCHING OF EARTH SURFACES AND/OR REGRADING, GRAVELING & SEAL COATING OF TREATED SURFACES, AS REQUIRED, TO THE SATISFACTION OF THE ENGINEER. ALL GROUND SURFACES, GRAVEL ROADS, PAVEMENTS, AND OTHER FACILITIES DAMAGED BY THE CONTRACTOR WHILE COMPLETING THE PROPOSED WORK SHALL BE REPAIRED OR RETURNED TO ITS ORIGINAL STATE. COST TO BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.
11. THE CONTRACTOR SHALL IMMEDIATELY SWEEP OR PICK UP ANY SOIL, DEBRIS, AGGREGATE CHIPS OR ROCK, OR LOOSE MATERIALS WHICH HAS BEEN DROPPED ONTO AIRPORT ROADS, RUNWAYS, TAXIWAYS, OR SODDED AREAS.
12. THE DISPOSAL OF ALL MATERIALS NOT TO BE INCORPORATED IN EMBANKMENTS ON THE PROJECT SHALL BE ACCOMPLISHED BY THE CONTRACTOR AT A LOCATION OFF AIRPORT PROPERTY.
13. THE SEQUENCE OF CONSTRUCTION OPERATIONS AND DESCRIPTION OF CONDITIONS ARE OUTLINED IN THE PROJECT SPECIAL PROVISIONS. THE CONTRACTOR SHALL GIVE THE MAA A 96 HOUR NOTICE PRIOR TO THE START OF ANY WORK REQUIRING THE CLOSING OF ANY PAVEMENTS TO AIRCRAFT TRAFFIC SO THAT A NOTAM CAN BE ISSUED.
14. WORKING HOURS OF THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL CONFORM TO ALL APPLICABLE LOCAL LAWS, INCLUDING ANY NOISE CONTROL.
15. NO MOUNDS OF DIRT OR IRREGULARITIES GREATER THAN 3" WHICH, IN THE OPINION OF THE RESIDENT ENGINEER, COULD INTERFERE WITH ANY AIRFIELD OPERATIONS WILL BE PERMITTED ON THE AIRFIELD. NO EXPOSED FACES IN EXCESS OF ONE AND ONE-HALF (1-1/2) INCHES IN HEIGHT AND 2:1 SLOPES ON ANY EXCAVATION WILL BE PERMITTED WITHIN THE RESTRICTED AREAS.
16. DUST ABATEMENT MEASURES WILL BE REQUIRED, WHEN IN THE OPINION OF THE RESIDENT ENGINEER, A HAZARD TO AIR TRAFFIC, LOCAL RESIDENCES, OR CONSTRUCTION PROJECT PERSONNEL EXISTS. PREVENTIVE MEASURES TO BE ACCOMPLISHED BY THE CONTRACTOR SHALL INCLUDE, BUT NOT BE LIMITED TO, WATERING AND TREATMENT WITH CALCIUM CHLORIDE.
17. BY THE END OF EACH WORK DAY AND PRIOR TO LEAVING THE AIRFIELD, THE CONTRACTOR SHALL HAVE THOROUGHLY SWEEPED THE AIR TRAFFIC CORRIDORS ADJACENT TO THE WORK AREAS TO REMOVE DUST AND DEBRIS. IN ADDITION, ALL AIR TRAFFIC AREAS USED BY CONSTRUCTION PERSONNEL AND EQUIPMENT MUST BE CONTINUOUSLY SWEEPED AND MAINTAINED FREE OF DEBRIS. SWEEPERS SHALL BE PROVIDED BY THE CONTRACTOR FOR THE ENTIRE LENGTH OF THE CONTRACT AND SHALL BE OF A TYPE CAPABLE OF REMOVING ALL DUST AND DEBRIS TO THE SATISFACTION OF THE MAA. SWEEPERS MUST BE COMMERCIAL QUALITY AND APPROVED BY THE RESIDENT ENGINEER AND MAA PRIOR TO THE START OF CONSTRUCTION.
18. THE CONTRACTOR SHALL INSTALL AND MAINTAIN LATH LINES DURING THE LENGTH OF THE PROJECT AS SHOWN OR DIRECTED BY THE RESIDENT ENGINEER. SEE PROJECT SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
19. NO CHANGES SHALL BE MADE IN ANY PROVISIONS OF THIS SAFETY PLAN UNLESS APPROVED IN WRITING BY THE METROPOLITAN AIRPORT AUTHORITY, THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF AERONAUTICS, AND THE FEDERAL AVIATION ADMINISTRATION. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.






CLOSED RUNWAY MARKINGS NOTES:

1. SOLID CROSS FOR CLOSED RUNWAY IS REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES AND SHALL BE PLACED DIRECTLY OVER THE RUNWAY IDENTIFICATION NUMERALS.
2. THE CONTRACTOR SHALL INSTALL, REMOVE AND REINSTALL THE CROSSES AS REQUIRED BY WORKING CONDITIONS AND AS APPROVED BY THE RESIDENT ENGINEER.
3. COLOR OF ALL CROSSES SHALL BE AVIATION YELLOW.
4. SOLID CROSSES ARE TO BE CONSTRUCTED OF PLYWOOD, CANVAS, OR ANY OTHER APPROVED SOLID MATERIALS AND SHALL BE ADHERED TO THE RUNWAY IN A MANNER ACCEPTABLE TO THE RESIDENT ENGINEER. THE CONTRACTOR SHALL MAINTAIN THE CROSSES IN EXCELLENT CONDITION.

PROPOSED TYPICAL SECTION
 RUNWAY 13-31, STA. 85+50 TO 89+33

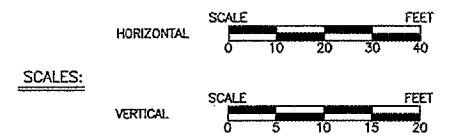


SOIL MATERIAL LEGEND:

	UNCOMPACTED FILL MATERIALS
	COMPACTED FILL MATERIALS
	GROUND STRIPPING

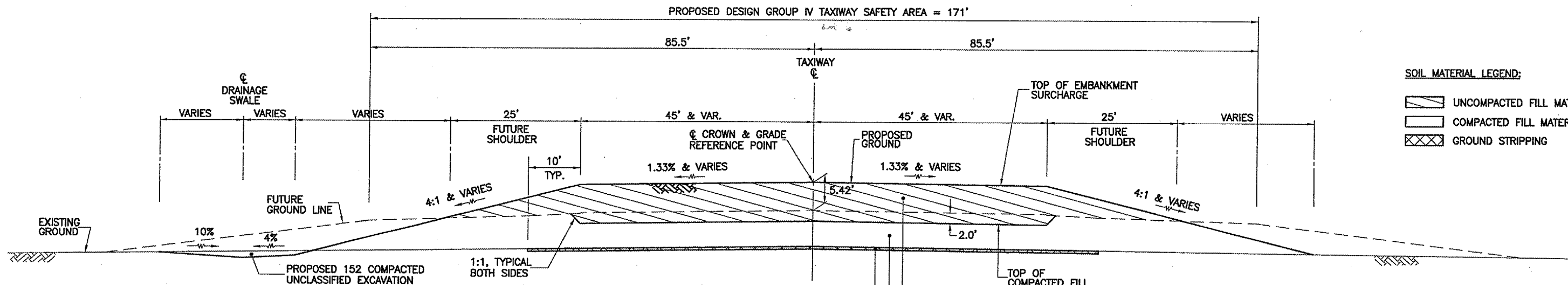
NOTES:

1. ALL CROSS SECTION SLOPES SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. THE EXISTING GROUND MATERIALS UNDER THE FUTURE PAVEMENT SURFACES SHALL BE REMOVED TO A DEPTH OF 6" FROM THE EXISTING SURFACE PRIOR TO PLACEMENT OF EMBANKMENT MATERIALS. THE SURFACE MATERIALS SHALL BE INCORPORATED INTO THE SHOULDER FILL AREAS. THE COST OF GROUND STRIPPING SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.



AIRCRAFT APPROACH CATEGORY C
 AIRPLANE DESIGN GROUP IV

PROPOSED TYPICAL SECTION
 TAXIWAY B, STA. 182+75 TO 188+44.66

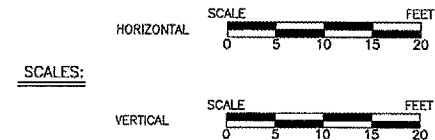


SOIL MATERIAL LEGEND:

 UNCOMPACTED FILL MATERIALS
 COMPACTED FILL MATERIALS
 GROUND STRIPPING

NOTES:

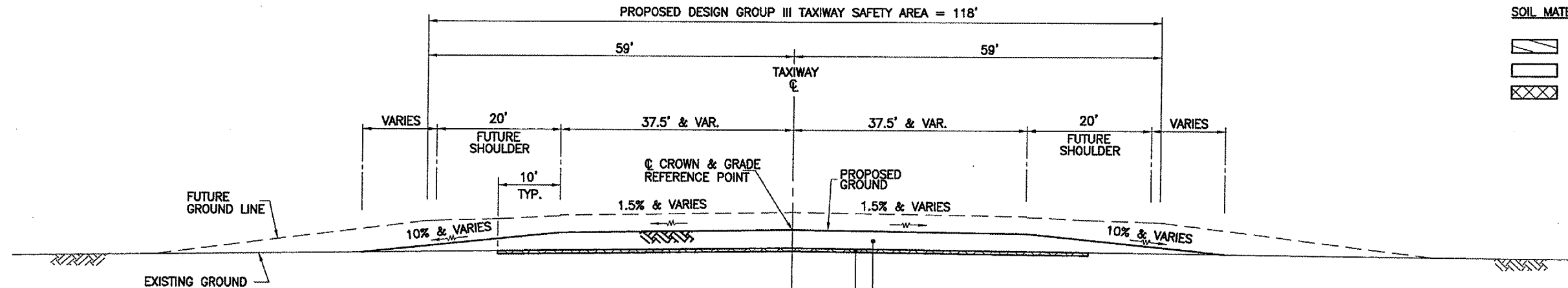
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3. IN LOCATIONS WHERE POOR SURFACE SOILS EXIST (AS DETERMINED BY THE RESIDENT ENGINEER) BETWEEN STATIONS 183+80 AND 186+60, THE CONTRACTOR SHALL REMOVE 12" OF THE EXISTING SOIL MATERIALS AND INSTALL 12" OF PROPOSED 209 CRUSHED AGGREGATE BASE COURSE ON 12" OF 156 RIPRAP - GRADATION NO. 3 ON 209 GEOTEXTILE FABRIC PRIOR TO STARTING EMBANKMENT INSTALLATION. THE PROPOSED MATERIALS SHALL BE INSTALLED UNDER AND WITHIN 10' OF THE FUTURE PAVED AREA.



PROPOSED TYPICAL SECTION

TAXIWAY K, STA. 383+91.37 TO 386+09.76

AIRCRAFT APPROACH CATEGORY C
 AIRPLANE DESIGN GROUP III



SOIL MATERIAL LEGEND:

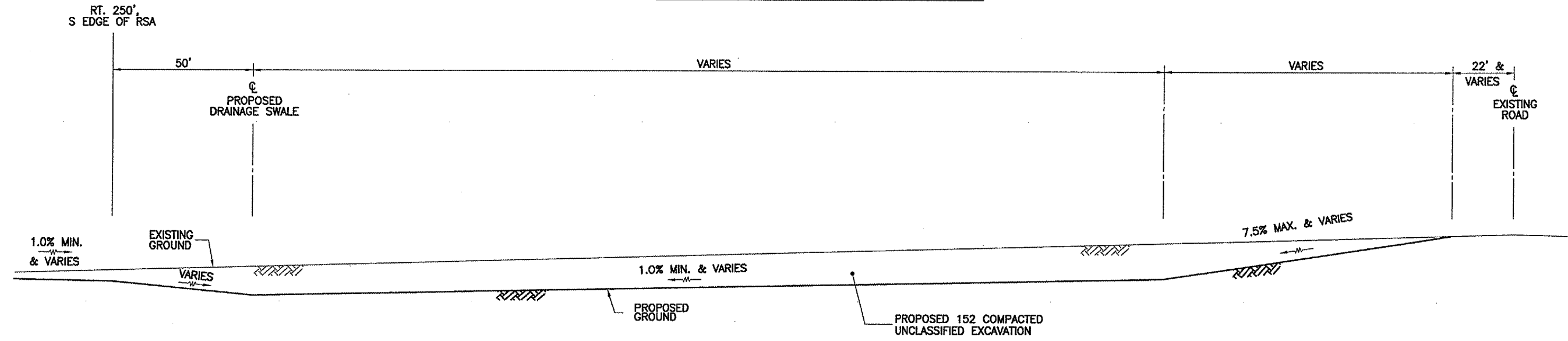
 UNCOMPACTED FILL MATERIALS
 COMPACTED FILL MATERIALS
 GROUND STRIPPING

NOTES:

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2. THE EXISTING GROUND MATERIALS UNDER THE FUTURE PAVEMENT SURFACES SHALL BE REMOVED TO A DEPTH OF 6" FROM THE EXISTING SURFACE PRIOR TO PLACEMENT OF EMBANKMENT MATERIALS. THE SURFACE MATERIALS SHALL BE INCORPORATED INTO THE SHOULDER FILL AREAS. THE COST OF GROUND STRIPPING SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

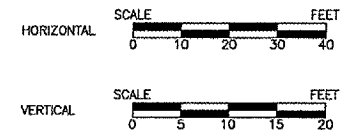


PROPOSED TYPICAL SECTION
CUT AREA:
RUNWAY 13-31, STA. 88+00 TO 93+50, RIGHT

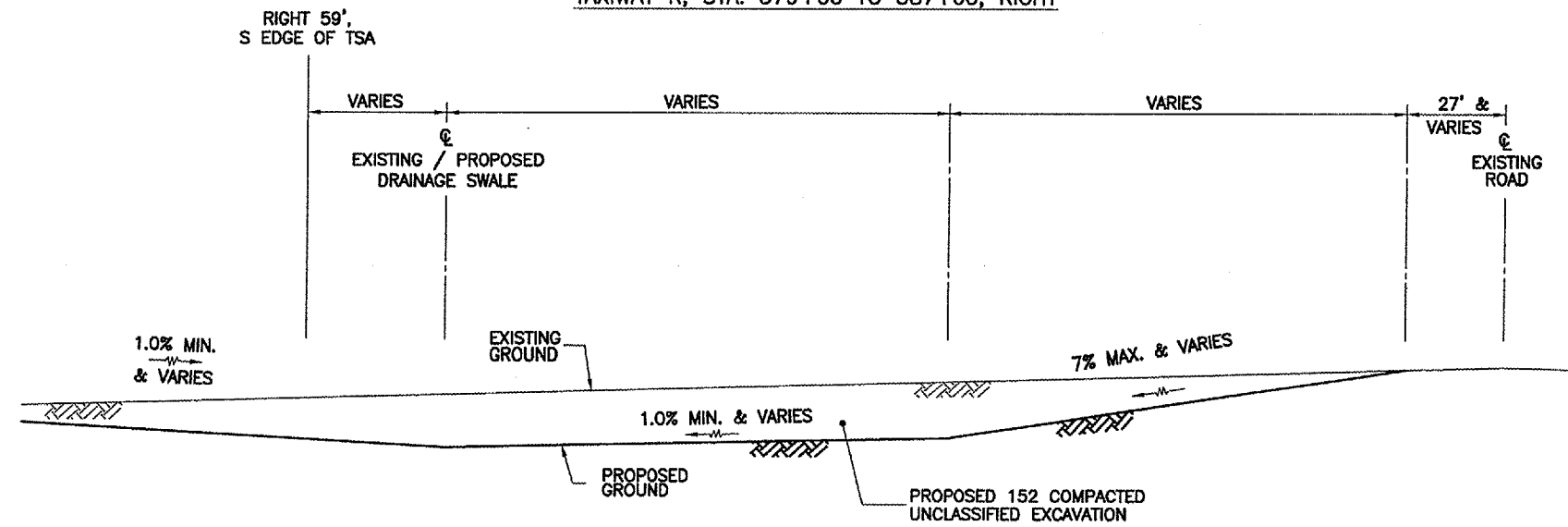


NOTE:

ALL CROSS SECTION SLOPES SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

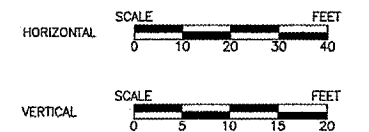


PROPOSED TYPICAL SECTION
CUT AREA:
TAXIWAY K, STA. 379+00 TO 387+00, RIGHT
















NOTE:

ALL CROSS SECTION SLOPES SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.



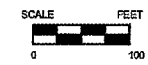
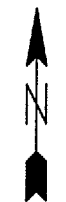
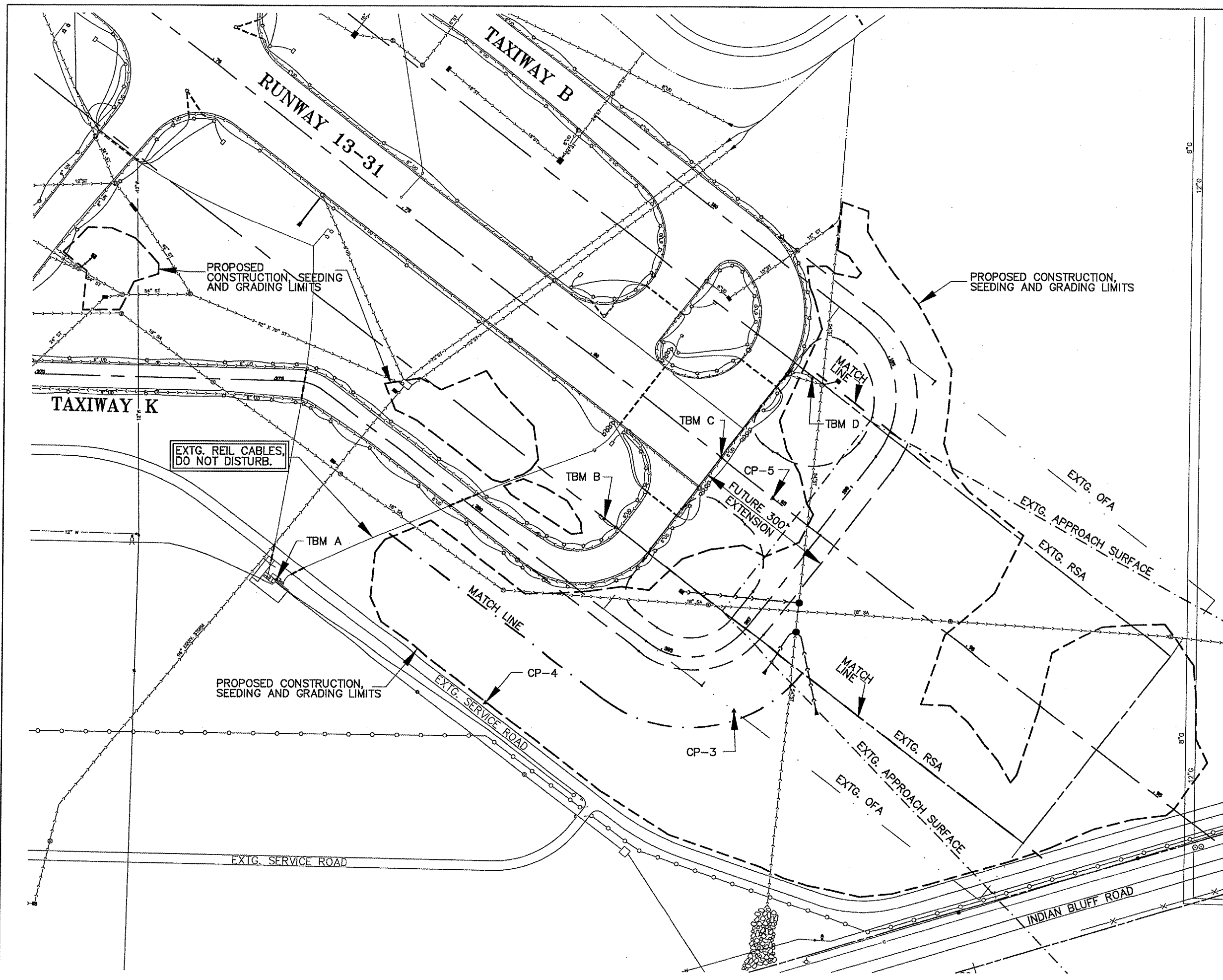
QUAD CITY INTERNATIONAL AIRPORT
 RUNWAY 31 EXTENSION - EARTHWORK
 ILL. MLI-3564, QU001
 SHEET 8 OF 52

LEGEND:

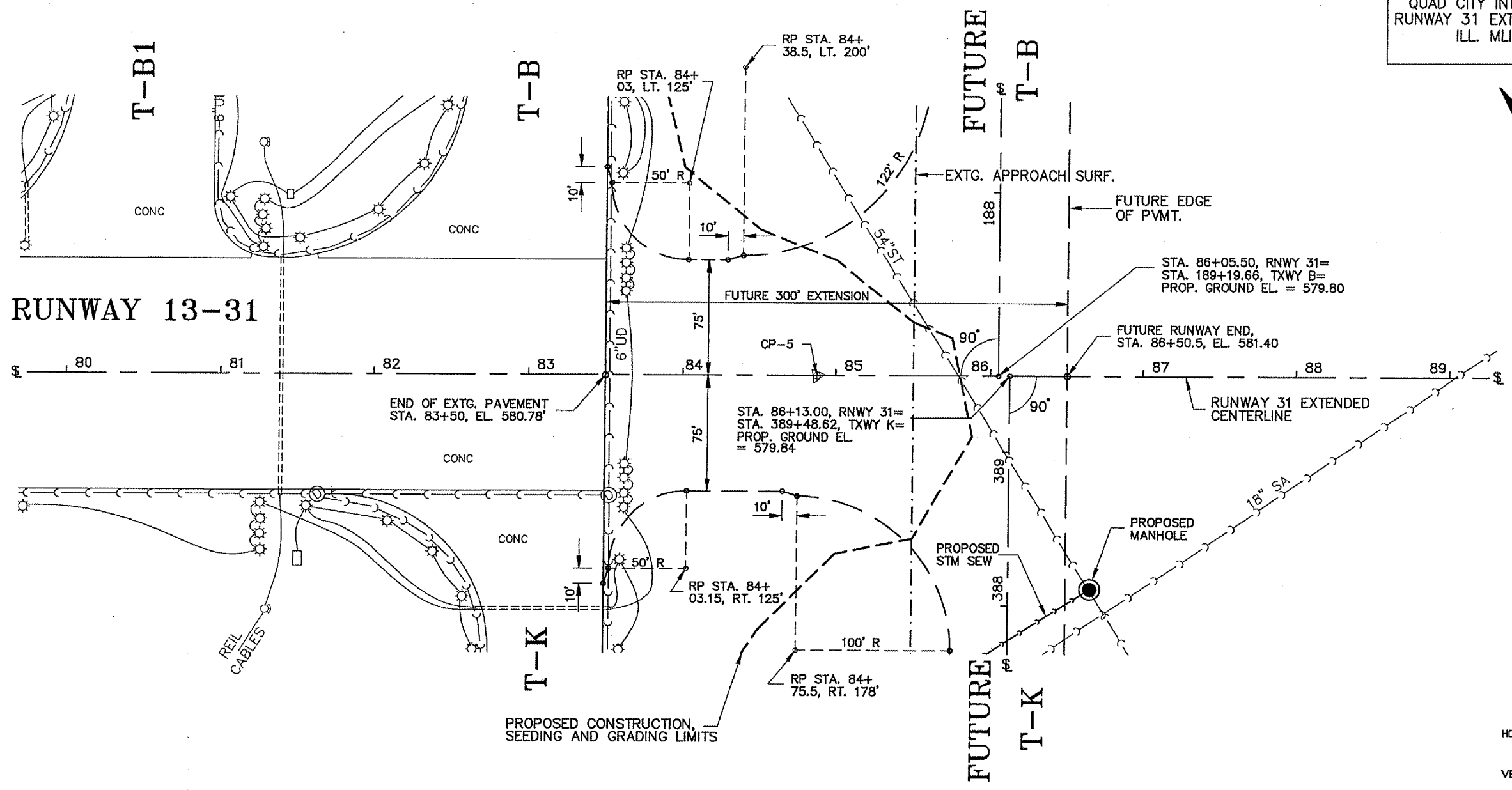
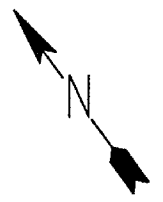
-  EDGE LIGHT
-  GUIDANCE SIGN
-  ELECTRICAL CABLE
-  STORM SEWER OR UNDERDRAIN
-  INLET
-  OUTLET
-  STORM MANHOLE
-  SANITARY MANHOLE
-  DRAINAGE DITCH
-  CONDUIT OR DUCT BANK
-  FUTURE PAVEMENT
-  PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
-  EARTHWORK MATCH LINES

PROJECT CONTROL POINTS			
NUMBER	LOCATION	ELEV.	REMARK
CP-3	STA. 86+99.84, RT. 400.23' N1739318.5560, E2206561.5788	578.39	SPK
CP-4	STA. 82+77.01, RT. 706.75' N1739333.6190, E2206039.6030	580.12	SPK
CP-5	STA. 84+87.59, RT. 0.12' N1739765.0785, E2206637.3701	579.77	

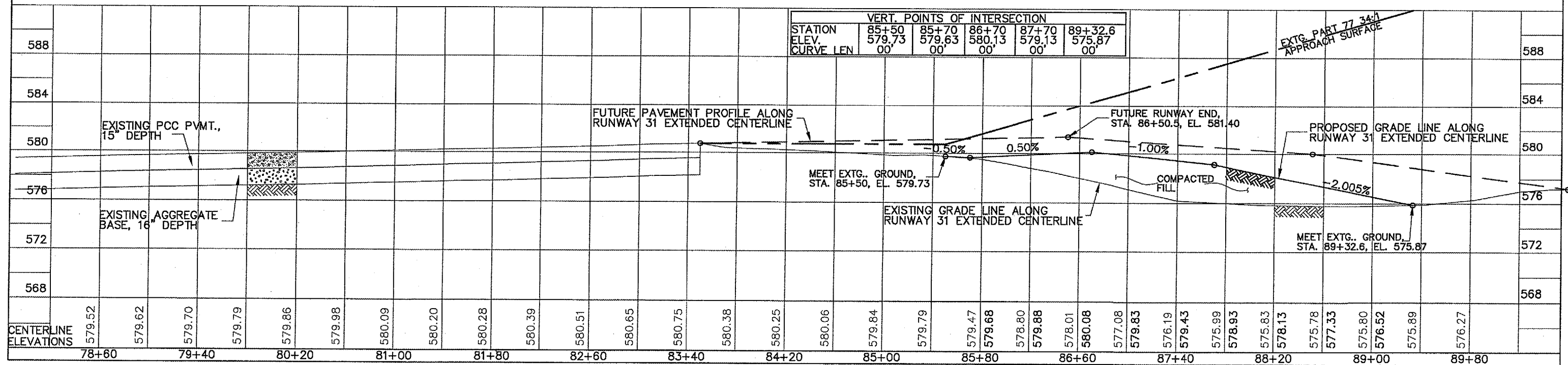
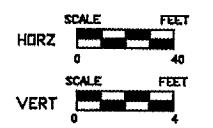
TEMPORARY BENCH MARKS		
T.B.M.	ELEV.	DESCRIPTION
TBM-A	578.57	CHIS. □ ON CONC TRANSFORMER PAD, TXWY K, STA. 377+71.67, RT. 369.06'
TBM-B	576.95	CHIS. □ ON NE COR OF CONC SIGN BASE, RNWY 31, STA. 82+34.96, RT. 250.41'
TBM-C	580.78	ALUMINUM DISC IN CONC ○ RNWY 31 END, RNWY 31, STA. 83+49.85, LT. 0.00'
TBM-D	575.61	CHIS. □ ON NE COR OF CONC SIGN BASE, RNWY 31, STA. 83+98.14, RT. 248.07'



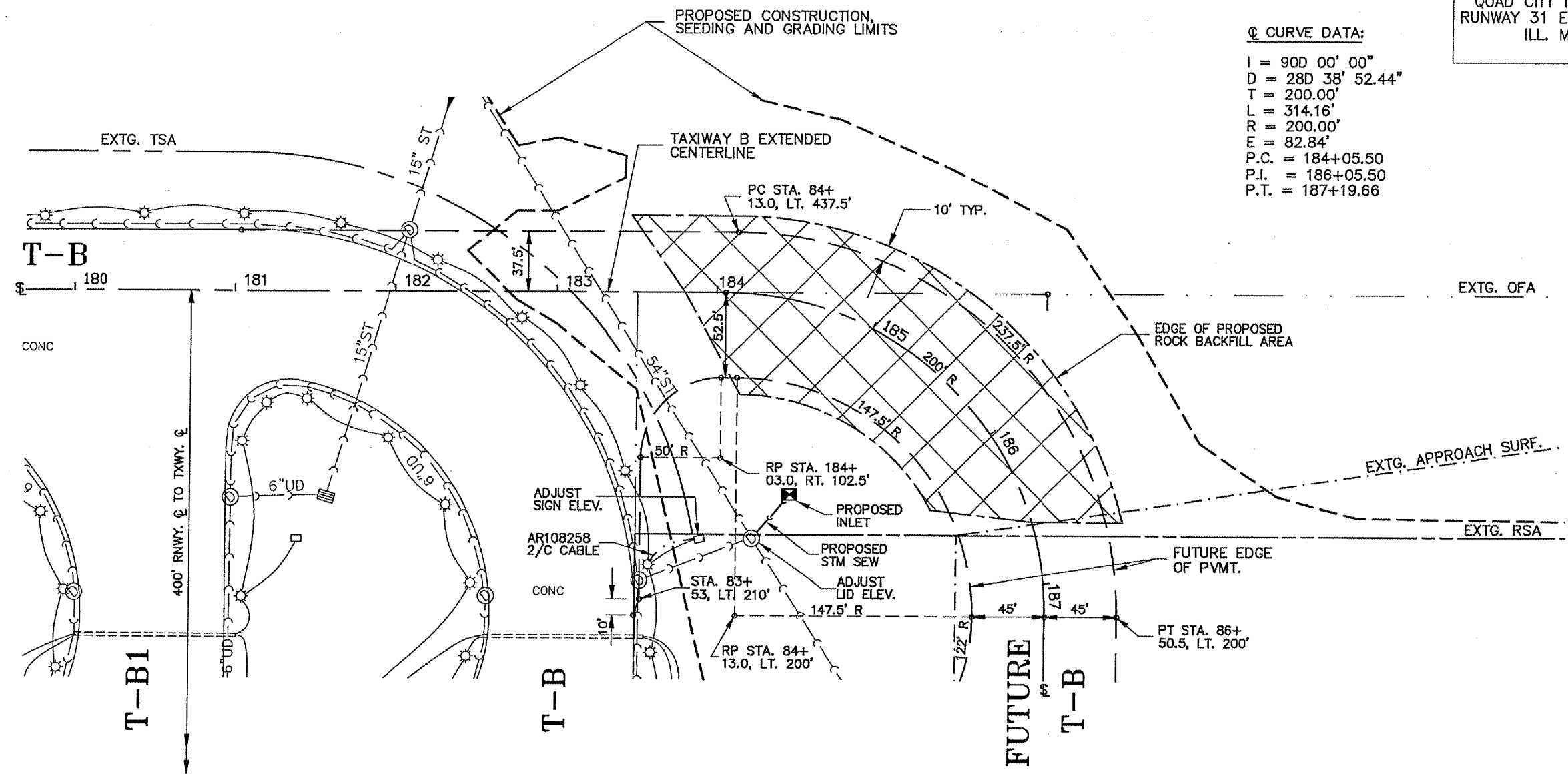
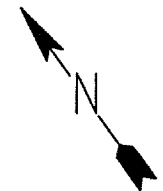
QUAD CITY INTERNATIONAL AIRPORT
 RUNWAY 31 EXTENSION - EARTHWORK
 ILL. MLI-3564, QU001
 SHEET 9 OF 52



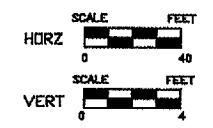
- LEGEND:**
- EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER, TILE, OR UNDERDRAIN
 - INLET
 - OUTLET
 - STORM MANHOLE
 - SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - UNCOMPACTED FILL MATERIALS



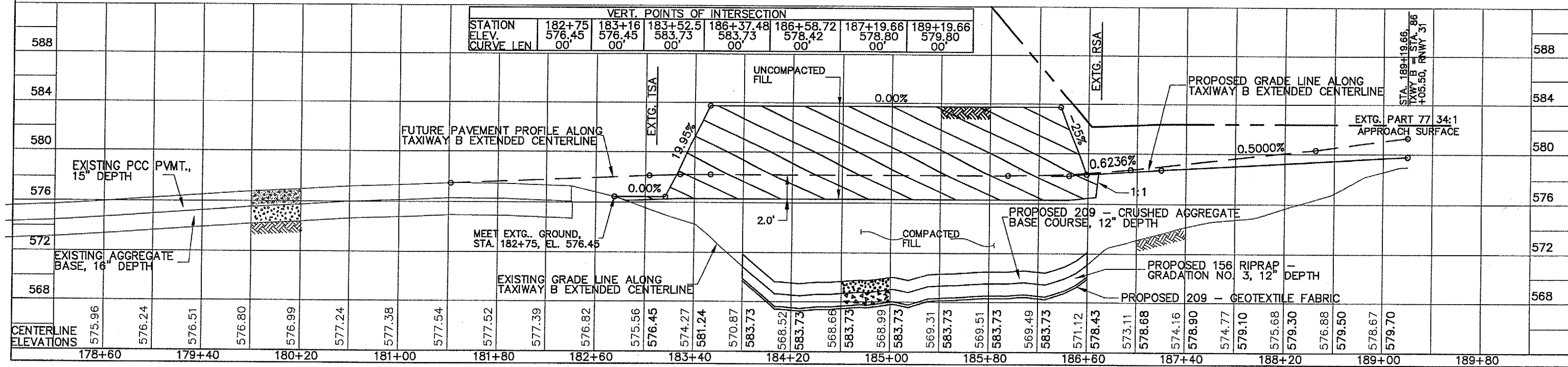
☉ CURVE DATA:
 I = 90D 00' 00"
 D = 28D 38' 52.44"
 T = 200.00'
 L = 314.16'
 R = 200.00'
 E = 82.84'
 P.C. = 184+05.50
 P.I. = 186+05.50
 P.T. = 187+19.66



- LEGEND:**
- EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER, TILE, OR UNDERDRAIN
 - INLET
 - OUTLET
 - STORM MANHOLE
 - SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - UNCOMPACTED FILL MATERIALS
 - PROPOSED ROCK BACKFILL AREA



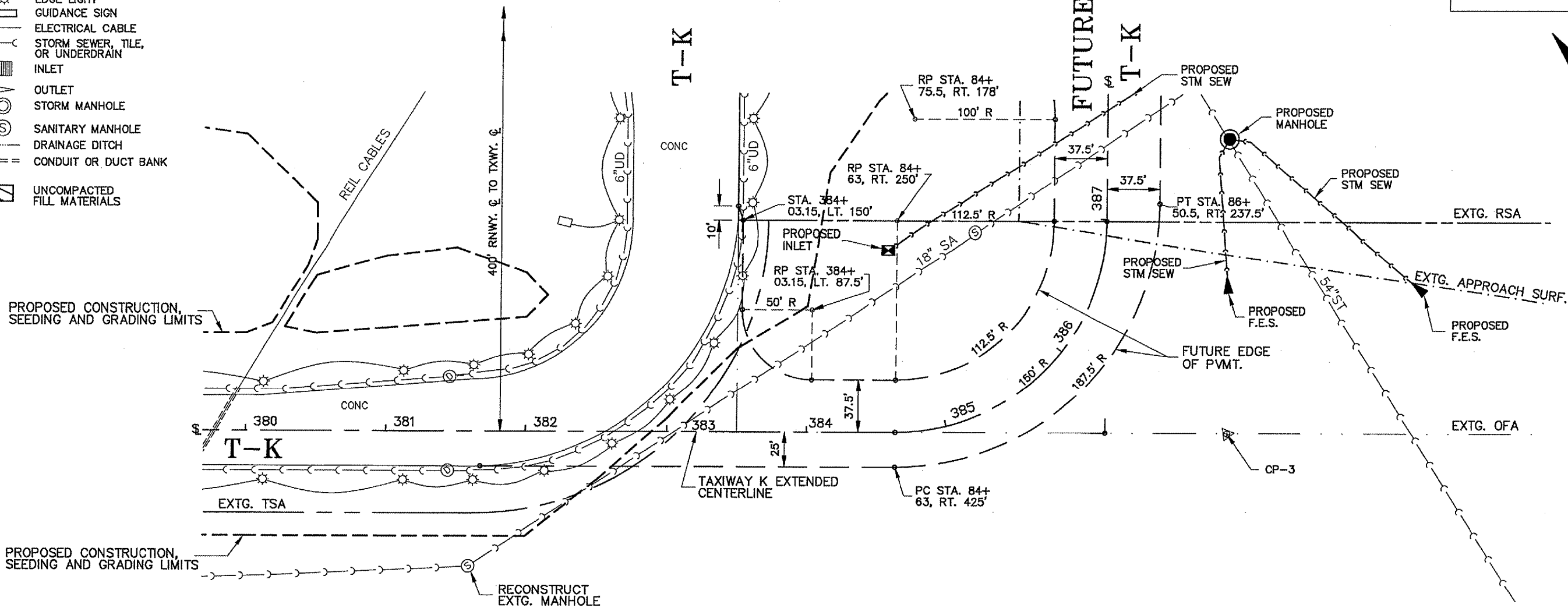
VERT. POINTS OF INTERSECTION							
STATION	182+75	183+16	183+52.5	186+37.48	186+58.72	187+19.66	189+19.66
ELEV.	576.45	576.45	583.73	583.73	578.42	578.80	579.80
CURVE LEN.	00'	00'	00'	00'	00'	00'	00'



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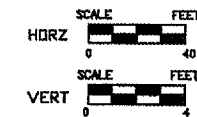
QUAD CITY INTERNATIONAL AIRPORT
 RUNWAY 31 EXTENSION - EARTHWORK
 ILL. MLI-3564, QU001
 SHEET 11 OF 52

- LEGEND:**
- ⊙ EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER, TILE, OR UNDERDRAIN
 - ▣ INLET
 - ▽ OUTLET
 - ⊙ STORM MANHOLE
 - ⊙ SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - ▨ UNCOMPACTED FILL MATERIALS



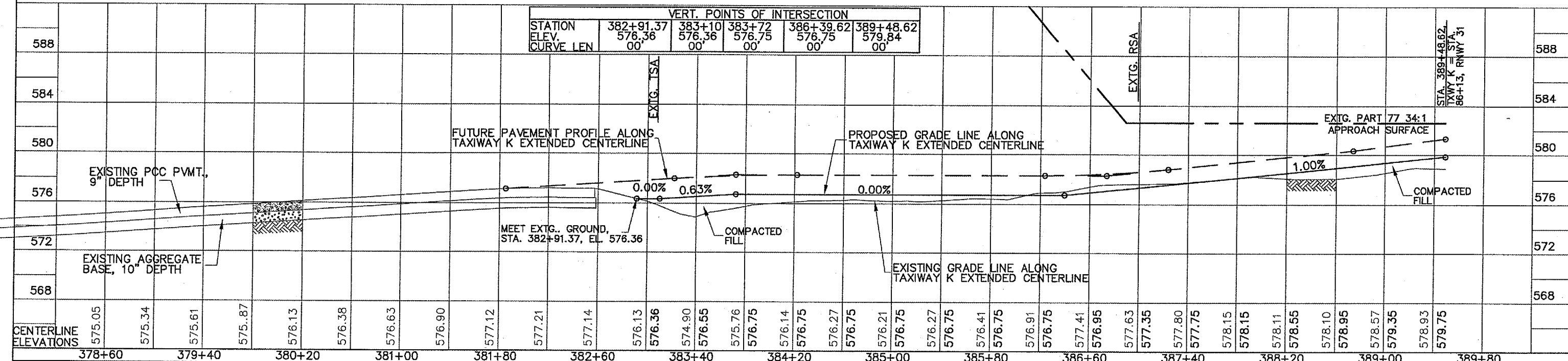
⊙ CURVE DATA:

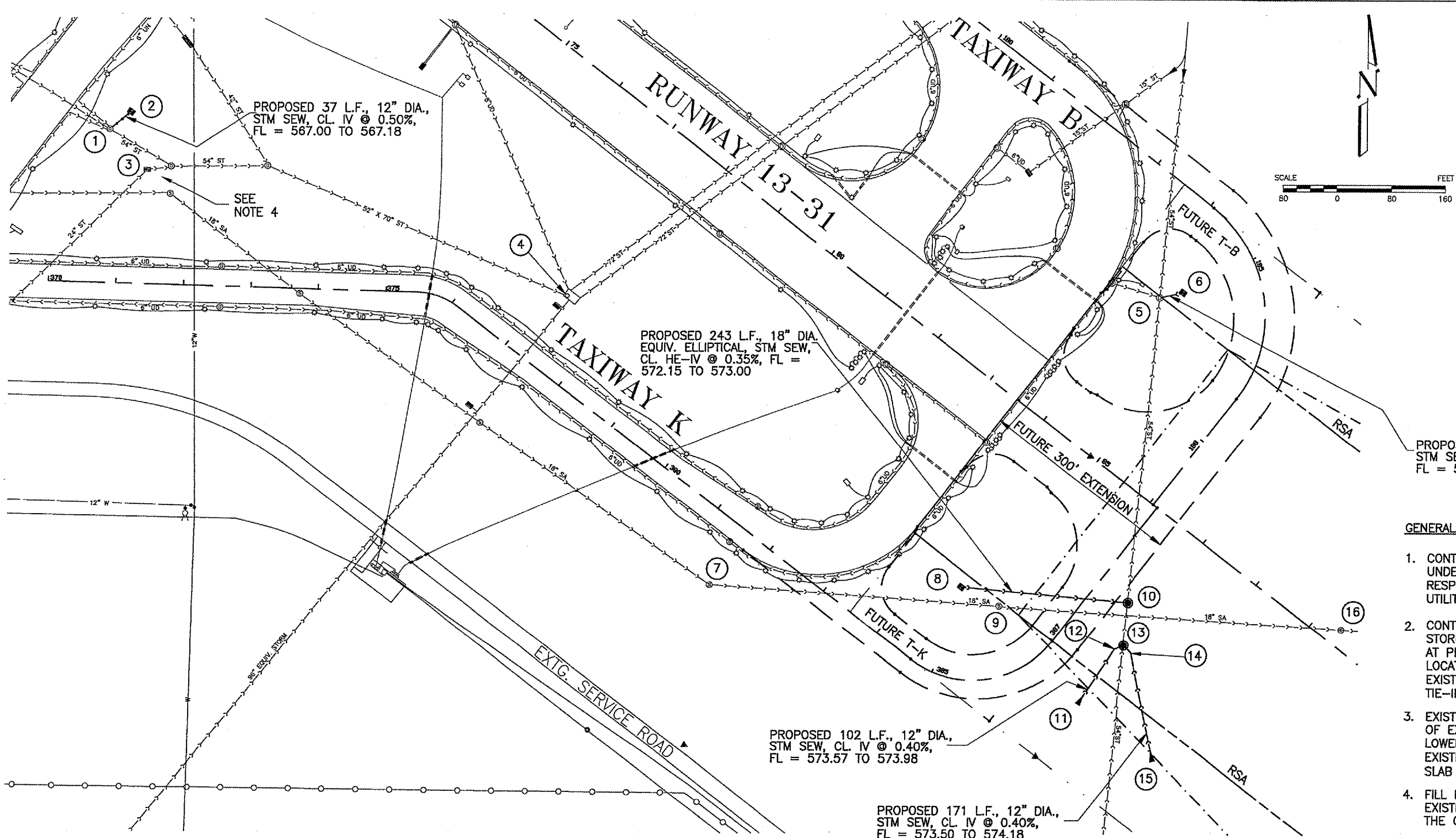
I	= 90D 00' 00"
D	= 38D 11' 49.92"
T	= 150.00'
L	= 235.62'
R	= 150.00'
E	= 62.13'
P.C.	= 384+63.00
P.I.	= 386+13.00
P.T.	= 386+98.62



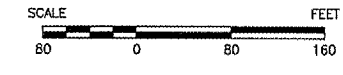
VERT. POINTS OF INTERSECTION

STATION	382+91.37	383+10	383+72	386+39.62	389+48.62
ELEV.	576.36	576.36	576.75	576.75	579.84
CURVE LEN.	00'	00'	00'	00'	00'





- LEGEND:**
- ☉ EXTG EDGE LIGHT
 - ▭ EXTG GUIDANCE SIGN
 - EXTG ELECTRICAL CABLE
 - EXTG STORM SEWER, FIELD TILE OR UNDERDRAIN
 - ▣ EXTG INLET
 - ▽ EXTG OUTLET
 - ⊕ EXTG STORM MANHOLE
 - ⊙ EXTG SANITARY MANHOLE
 - EXTG DRAINAGE DITCH
 - EXTG CONDUIT OR DUCT BANK
 - PROPOSED MANHOLE
 - ⊠ PROPOSED INLET
 - ▴ PROPOSED FLARED END SECTION
 - PROPOSED STORM SEWER, SANITARY SEWER OR UNDERDRAIN
 - ① PROPOSED / EXISTING STRUCTURE NUMBER



- GENERAL DRAINAGE NOTES:**
- CONTRACTOR SHALL HAND DIG AROUND ALL EXISTING UNDERGROUND UTILITIES. CONTRACTOR SHALL BE RESPONSIBLE FOR COST OF REPAIRING ALL DAMAGED UTILITIES.
 - CONTRACTOR TO EXCAVATE AND EXPOSE EXISTING STORM PIPES, ELEC. CABLES, AND ELEC. DUCT BANKS AT PROPOSED MANHOLE, INLET, AND PIPE CROSSING LOCATIONS TO FIELD VERIFY VERTICAL LOCATIONS OF EXISTING UTILITIES. ENGINEER MAY ADJUST SLOPES AND TIE-INS AT THE TIME OF CONSTRUCTION AS REQUIRED.
 - EXISTING MANHOLE CONE TRANSITION SECTION ON TOP OF EXISTING 23' X 15' X 11' DRAINAGE STRUCTURE. LOWER EXISTING MANHOLE FRAME & LID BY REMOVING EXISTING MH CONE SECTION AND INSTALLING NEW FLAT SLAB TOP. BOTTOM OF CONE SECTION = 4' DIA.
 - FILL IN / REGRADE EXISTING LOW AREA AROUND EXISTING DRAINAGE STRUCTURE NUMBER 3 SO THAT THE GROUND DRAINS INTO THE RECONSTRUCTED INLET.

STRUCTURE SCHEDULE

NUMBER	LOCATION	TYPE	E ELEV.	LID OR GRATE ELEV.	REMARK
1	370+83, LT. 224'	EXTG. 6' DIA. MANHOLE	E=W: MEET EXTG., ±565.30', N : 567.00'	±572.10'	NO CHANGE
2	371+14, LT. 250'	INLET, SPECIAL	567.18'	569.50'	NEENAH R-3475, AR751415
3	371+30, LT. 154'	EXTG. INLET *	±564.94'	EXTG =568.66', PROP = 569.26'	RECONSTRUCT INLET, AR751980 *
4	377+24, LT. 114'	SEE NOTE 3	±562.00'	EXTG =574.88', PROP = 573.13'	RECONSTRUCT SPL STRUCTURE, AR751992
5	84+23, LT. 248'	EXTG. 54" X 36" R.C. RISER TEE	N=S: MEET EXTG., ±567.45', E : 569.48'	EXTG =573.57', PROP = 575.75'	ADJUST FRAME & LID, AR751943 & AR701536**
6	84+45.5, LT. 275'	INLET, SPECIAL	569.80'	574.00'	NEENAH R-3475, AR751415
7	381+59.5, RT. 96.5'	EXTG. SANITARY SEWER MANHOLE	±558.19'	EXTG =575.20', PROP = 573.85'	RECONSTRUCT MH, AR751983
8	84+57, RT. 270'	INLET, SPECIAL	573.00'	575.00'	NEENAH R-3475, AR751415

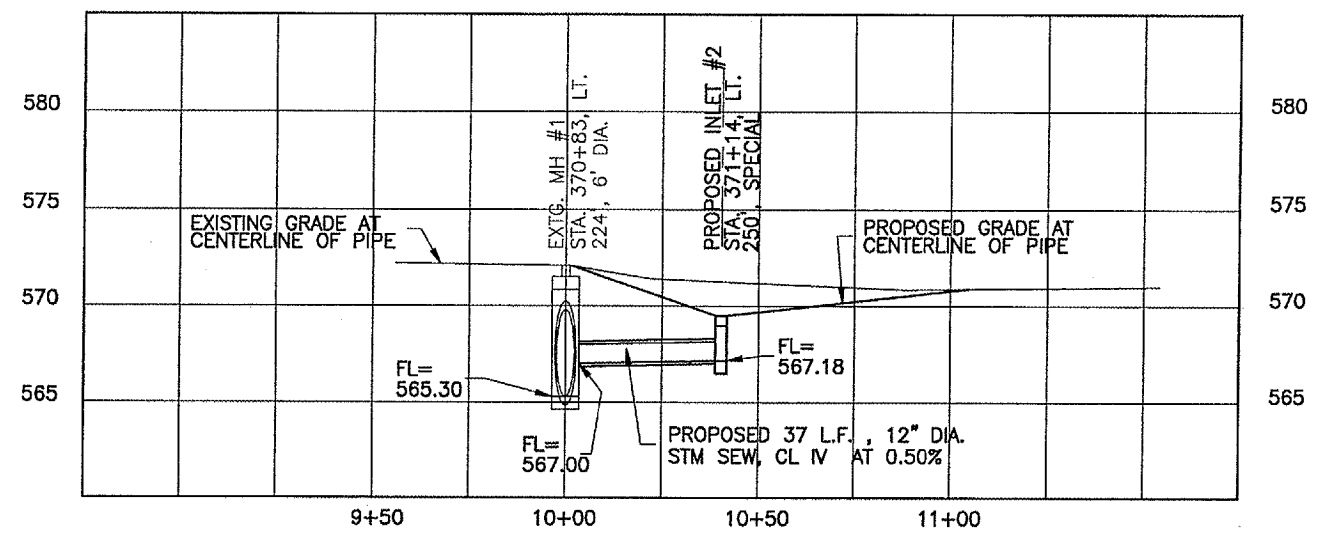
STRUCTURE SCHEDULE

NUMBER	LOCATION	TYPE	E ELEV.	LID OR GRATE ELEV.	REMARK
9	85+18.9, RT. 257.9'	EXTG. SANITARY SEWER MANHOLE	± 557.88'	576.14'	NO CHANGE
10	86+66, RT. 137.8'	MANHOLE, 7' DIA. WITH FLAT SLAB TOP	N=S: MEET EXTG., ±571.19', W : 572.15'	578.68'	NEENAH R-3493-A FR & SOLID LID, AR751567
11	87+00, RT. 300'	PRC FLARED END SECTION, 12" DIA.	574.00'	-	AR752412
12	±86+92.6, RT. ±204'	REIN. CONCR. PIPE ELBOW, 12" D, 30D	573.62'	-	AR752850
13	87+00, RT. 191.3'	MANHOLE, 7' DIA. WITH FLAT SLAB TOP	N=S: MEET EXTG., ±571.71', SW: 573.57', SE: 573.50'	578.30'	NEENAH R-3493-A FR & SOLID LID, AR751567
14	±87+15, RT. ±193.4'	REIN. CONCR. PIPE ELBOW, 12" D, 30D	573.55'	-	AR752850
15	88+40, RT. 300'	PRC FLARED END SECTION, 12" DIA.	574.20'	-	AR752412
16	89+40.4, LT. 23.7'	EXTG. SANITARY SEWER MANHOLE	± 557.02'	575.57'	NO CHANGE

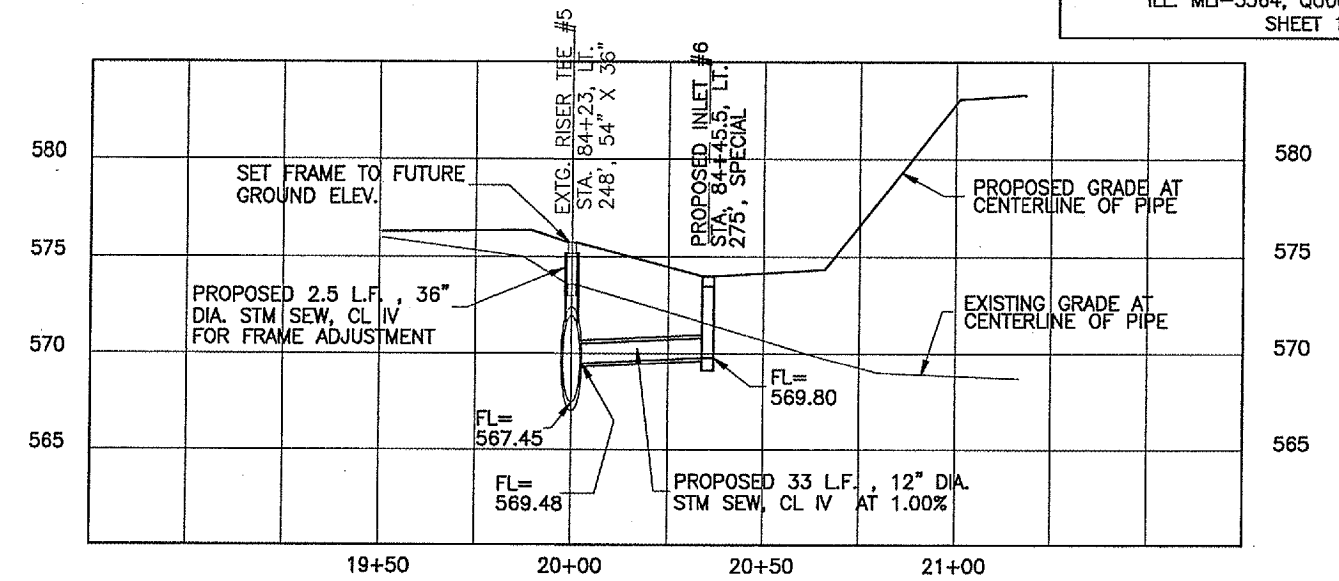
* = SEE NOTE 4.
 ** = SUPPLY & INSTALL 2.5 L.F. OF 36" DIA. STM SEW, CL. IV TO ADJUST RISER TEE.

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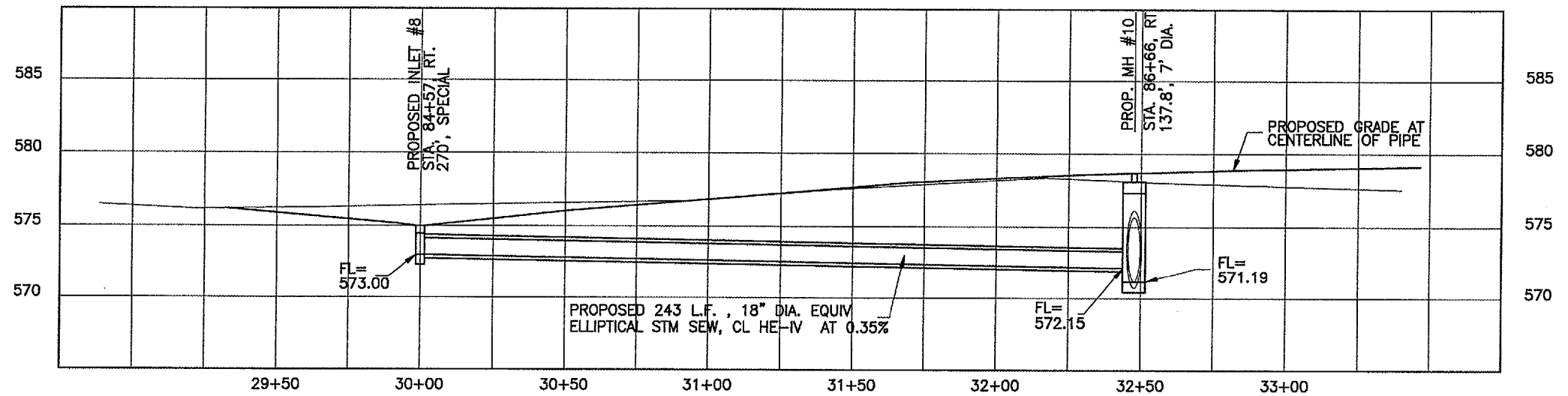
TAXIWAY K, STA. 371+00 LEFT, 12" STM SEW CENTERLINE PROFILE



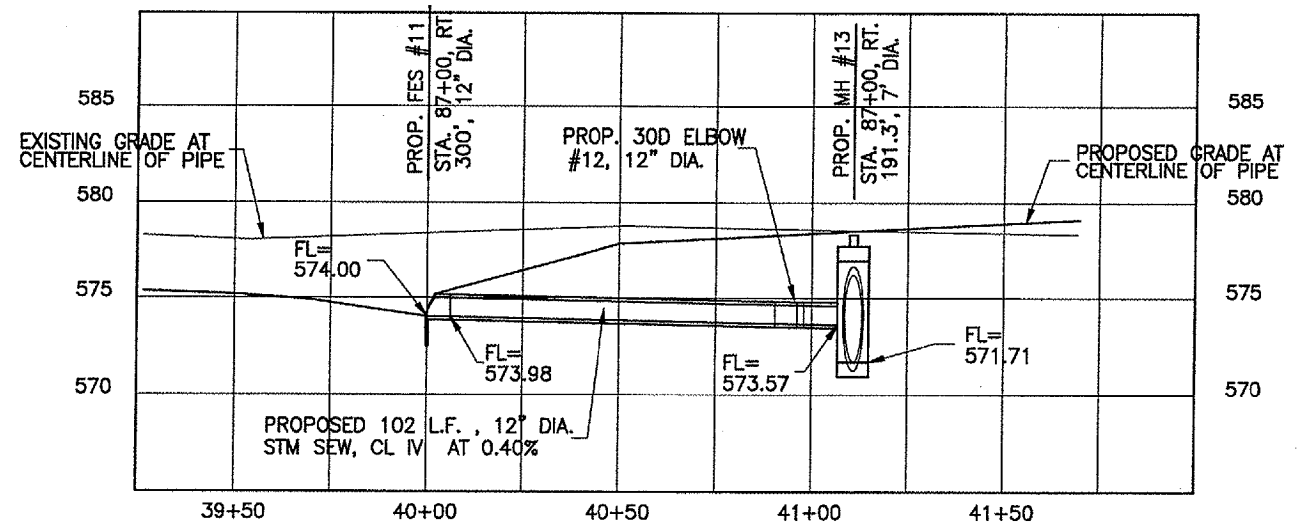
RUNWAY 31, STA. 84+30 LEFT, 12" STM SEW CENTERLINE PROFILE



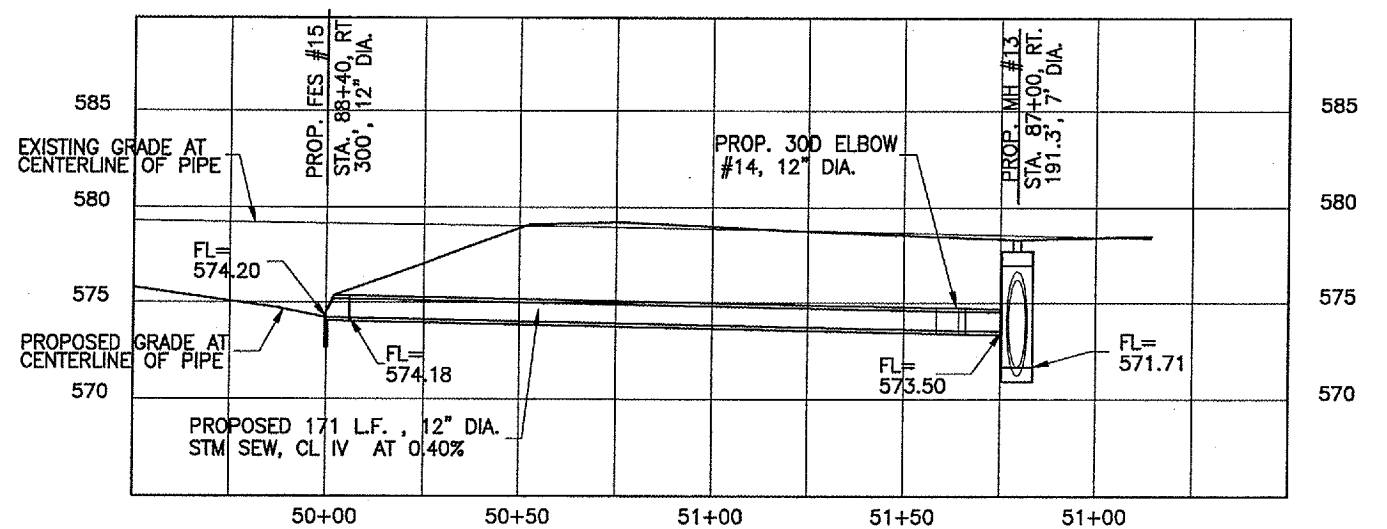
TAXIWAY K INFIELD AREA 18" EQUIV. ELLIPTICAL STM SEW CENTERLINE PROFILE



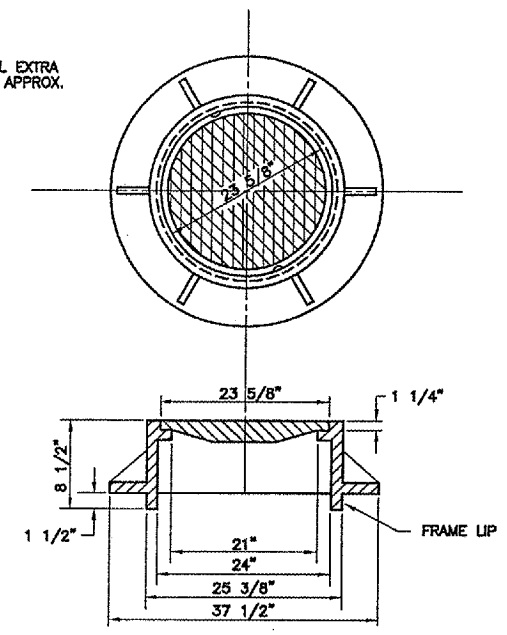
RUNWAY 31, STA. 87+00 RIGHT, 12" STM SEW CENTERLINE PROFILE



RUNWAY 31, STA. 88+40 RIGHT, 12" STM SEW CENTERLINE PROFILE

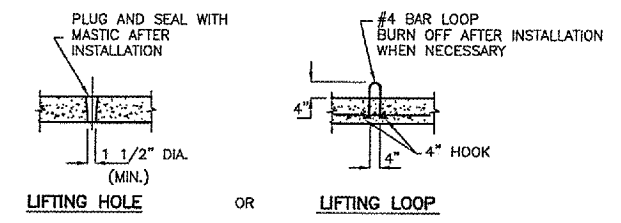
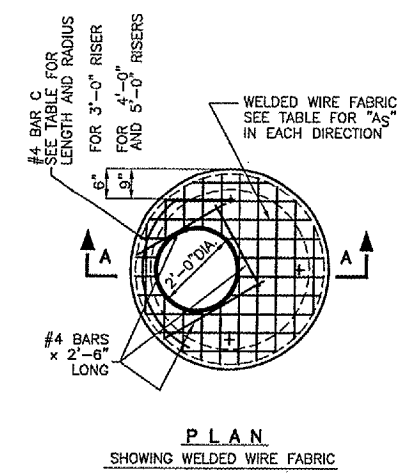


NEENAH R-3493-A OR EQUAL EXTRA
 HEAVY DUTY FRAME AND LID. APPROX.
 WEIGHT 440 POUNDS.

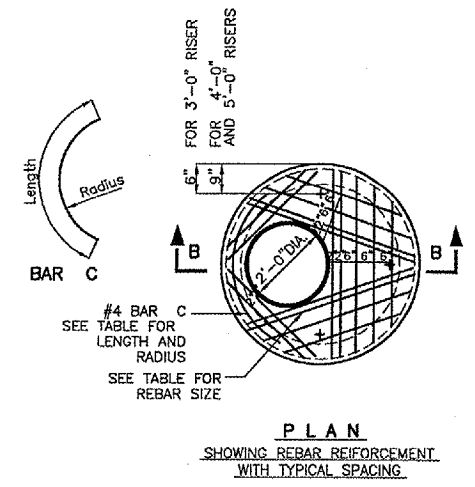


- NOTES:
1. USE FOR ALL MANHOLES (UNLESS NOTED OTHERWISE ON THE PLAN SHEETS).
 2. REMOVE BOLTS THAT HOLD THE GRATE TO THE FRAME AND DO NOT REINSTALL.

MANHOLE FRAME AND LID



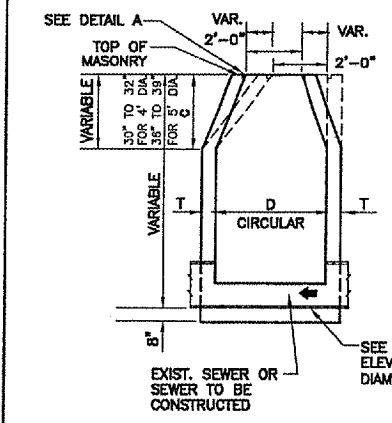
TYPICAL
 (3 REQUIRED PER SLAB)



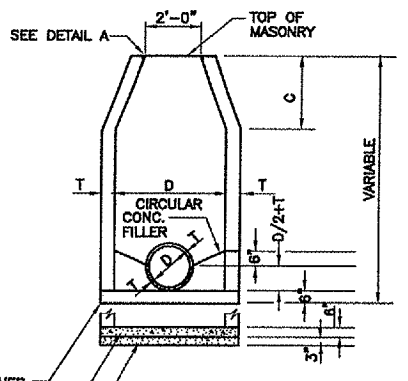
D	T	D ₀ (MIN)	f	REINFORCEMENT		#4 BAR C	
				"AS" W.W.F. EACH DIRECTION	OR BAR SIZE	LENGTH	RADIUS
3'-0"	8" 3" RISES 10" 3" RISES 15" 3" RISES AND REBAR	D + 2T	6"	.20 sq.in./lin.ft.	#4	4'-0"	1'-7"
4'-0"				.35 sq.in./lin.ft.	#5	4'-6"	2'-2"
5'-0"				.35 sq.in./lin.ft.	#5	5'-0"	2'-8"

ALT. MATERIAL FOR WALLS	D	C	T
PRECAST REINFORCED CONCRETE RINGS	4'-2'-6"	5'-3'-0"	6"
MONOLITHIC CONCRETE	4'-2'-6"	5'-3'-0"	6"

FOR 7" DIAMETER MANHOLES, SEE IDOT STANDARD 602411. MANHOLE FRAME LIP NOTCH OUT DETAIL AND ADDITIONAL BEDDING REQUIREMENTS THAT ARE SHOWN ON THIS SHEET SHALL BE APPLIED TO IDOT STANDARD 602411.



- NOTES:
- PRECAST REINFORCED CONCRETE RINGS AND MANHOLE FRAMES SHALL BE LAID IN FULL MORTAR BEDS WITH FLUSH JOINTS.
 - MORTAR MIX SHALL CONSIST OF 1 PART PORTLAND CEMENT & 3 PARTS SAND.
 - ALL CONCRETE SHALL HAVE A MIN. STRENGTH AT 28 DAYS OF 3500 P.S.I.
 - THE CONTRACT UNIT PRICE FOR MANHOLE SHALL INCLUDE INSTALLATION OF THE FRAME AND LID.



SEE PLAN SHEETS FOR ELEVATION OF INVERT AND DIAMETER OF SEWER

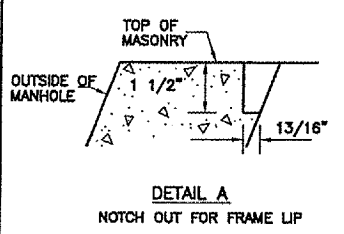
BOTTOM TO BE EITHER PLAIN CONCRETE OR PRECAST REINFORCED CONCRETE SLAB IDOT CA-14 OR -16

FURNISHING AND INSTALLING SAND CUSHION, FRAME, AND LID TO BE INCLUDED IN THE CONTRACT UNIT PRICE

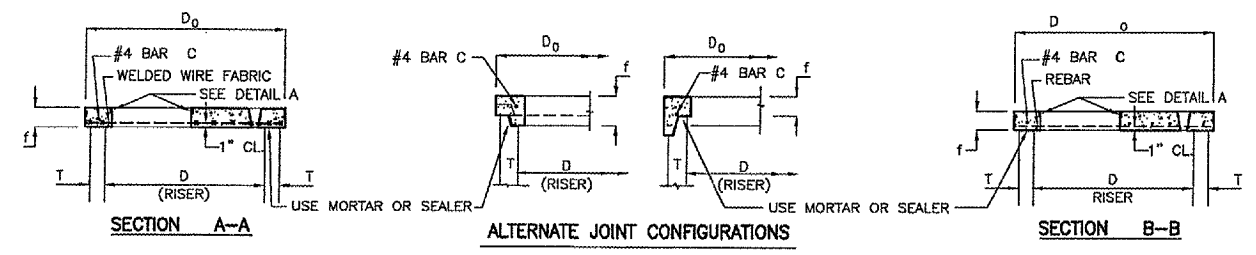
NOTE:
 THE CONE OF THE MANHOLE SHALL BE CONSTRUCTED AS SHOWN BY THE DOTTED LINES ONLY WHEN THERE IS INTERFERENCE WITH UNDERGROUND CONDITIONS AND THESE CONDITIONS CAN NOT BE ALTERED.

MODIFIED IDOT STANDARD 602401

DETAIL OF MANHOLE



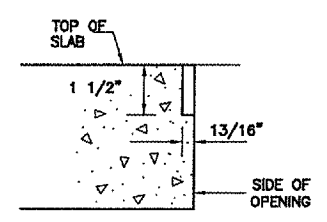
DETAIL A
 NOTCH OUT FOR FRAME LIP



PRECAST REINFORCED CONCRETE
 FLAT SLAB TOP
 FOR MANHOLES, CATCH BASINS
 AND VALVE VAULTS

MODIFIED I.D.O.T. STANDARD 602601

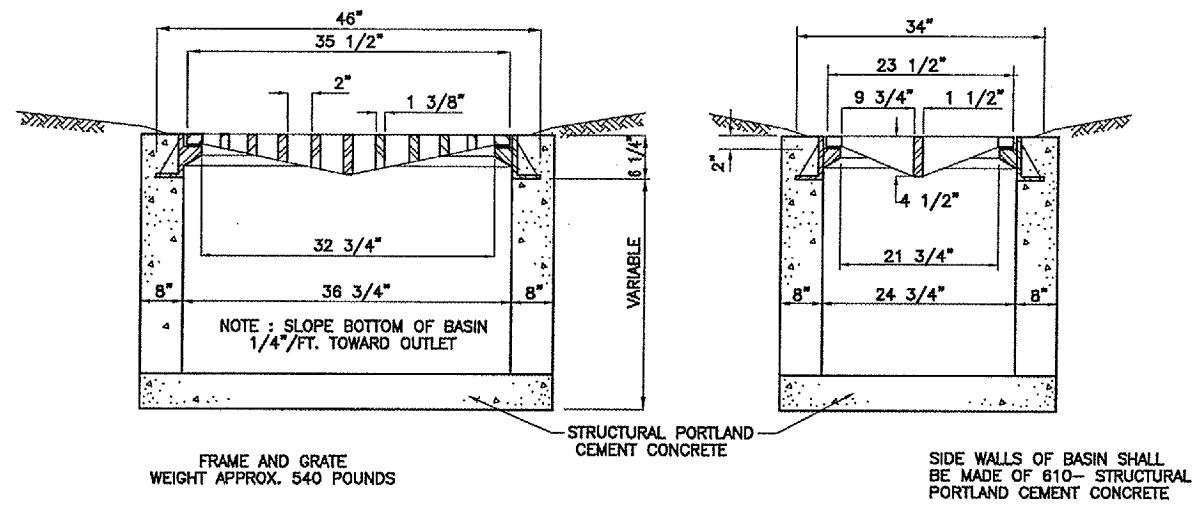
- NOTES:
1. PRECAST FLAT SLAB TOPS SHALL CONFORM TO SECTION 602 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
 2. REINFORCEMENT BARS OR WELDED WIRE FABRIC SHALL BE IN ACCORDANCE WITH ARTICLE 1006.04 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
 3. JOINT CONFIGURATION AND DIMENSIONS SHALL MATCH AND FIT THE RISER JOINT DETAIL.
 4. LIFTING DEVICES OTHER THAN SHOWN MAY BE USED SUBJECT TO APPROVAL BY THE ENGINEER.
 5. THE FLAT SLAB TOP MAY BE USED IN LIEU OF THE TAPERED TOPS SHOWN ON STANDARDS 602001, 602011, 602306, 602401, OR 602501 AT THE OPTION OF THE CONTRACTOR OR WHEN FIELD CONDITIONS PROHIBIT THE USE OF TAPERED TOPS.
 6. THE COST OF FURNISHING AND INSTALLING THE FLAT SLAB TOP SHALL BE INCLUDED IN THE UNIT PRICE FOR CATCH BASINS, MANHOLES, OR VALVE VAULTS.



DETAIL A

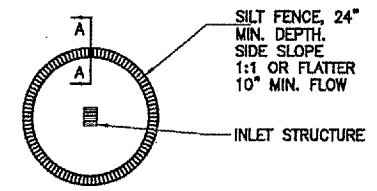
FOR 7" DIAMETER MANHOLES, SEE IDOT STANDARD 602411. MANHOLE FRAME LIP NOTCH OUT DETAIL AND ADDITIONAL BEDDING REQUIREMENTS THAT ARE SHOWN ON THIS SHEET SHALL BE APPLIED TO IDOT STANDARD 602411.

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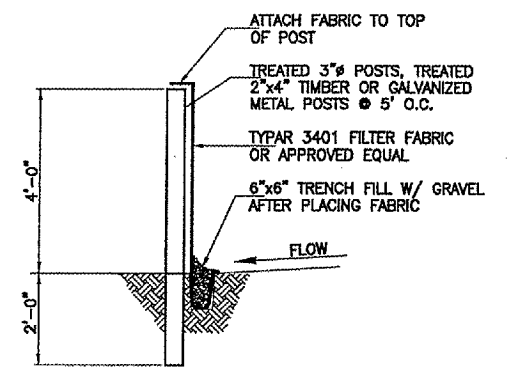


INLET SPECIAL, ITEM 751415

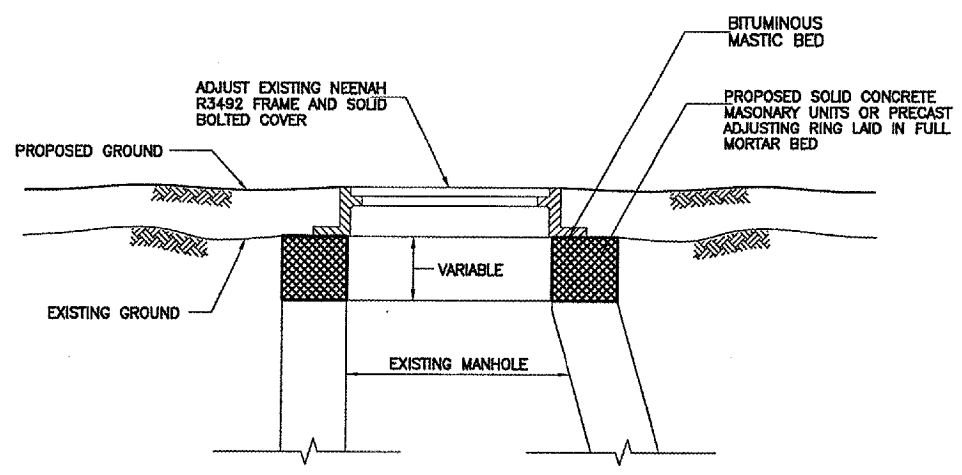
- NOTES :
1. INLET SPECIAL SHALL INCLUDE FRAME WITH SINGLE GRATE OF TYPE SIMILAR AND EQUAL TO NO. R 3475 AS SHOWN BY CATALOG "R" ELEVENTH EDITION OF NEENAH FOUNDRY CO.
 2. REMOVE BOLTS THAT HOLD THE GRATE TO THE FRAME AND DO NOT REINSTALL.



TEMPORARY SEDIMENT TRAP AT ALL INLET STRUCTURES



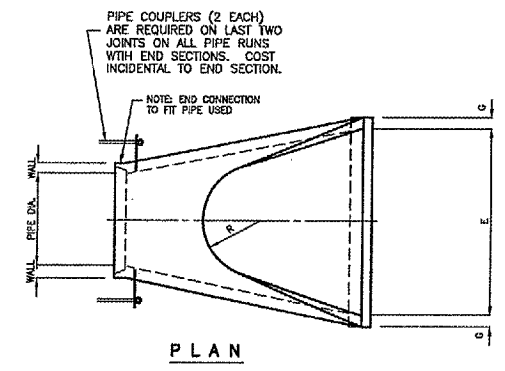
SECTION A-A SILT FENCE DETAIL NOT TO SCALE



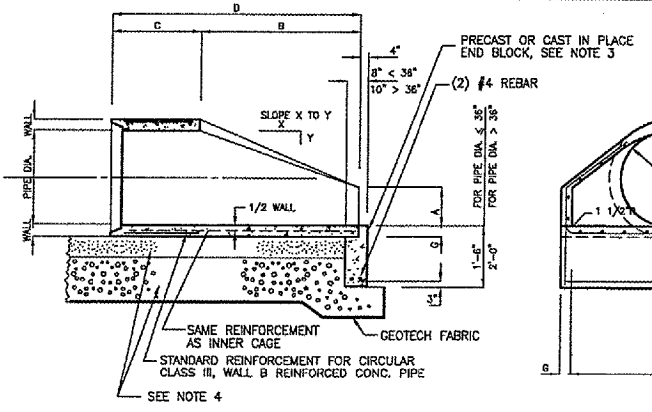
TYPICAL DETAIL - MANHOLE ADJUST - IN TURF

PRECAST REINFORCED CONCRETE FLARED END SECTION

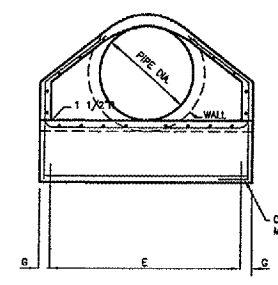
PIPE DIA.	APPRX. WT. (LBS.)	WALL	A	B	C	D	E	G	R	SLOPE
12"	530	2"	4'	2'-0"	4'-0-7/8"	6'-0-7/8"	2'-0"	2"	9'	3:1
15"	740	2-1/4"	6'	2'-3"	3'-10"	6'-1"	2'-6"	2-1/4"	11'	3:1
18"	990	2-1/2"	9'	2'-3"	3'-10"	6'-1"	3'-0"	2-1/2"	12'	3:1
21"	1280	2-3/4"	9'	2'-11"	3'-2"	6'-1"	3'-6"	2-3/4"	13'	3:1
24"	1520	3"	9-1/2"	3'-7-1/2"	2'-6"	6'-1-1/2"	4'-0"	3"	14'	3:1
27"	1930	3-1/4"	10-1/2"	4'-0"	2'-1-1/2"	6'-1-1/2"	4'-6"	3-1/4"	14-1/2"	3:1
30"	2190	3-1/2"	1'-0"	4'-6"	1'-7-3/4"	6'-1-3/4"	5'-0"	3-1/2"	15'	3:1
33"	3200	3-3/4"	1'-1-1/2"	4'-10-1/2"	3'-3-1/4"	6'-1-3/4"	5'-6"	3-3/4"	17-1/2"	3:1
36"	4100	4"	1'-3"	5'-3"	2'-10-3/4"	6'-1-3/4"	6'-0"	4"	20'	3:1
42"	5380	4-1/2"	1'-9"	5'-3"	2'-11"	6'-2"	6'-6"	4-1/2"	22'	3:1
48"	6550	5"	2'-0"	6'-0"	2'-2"	6'-2"	7'-0"	5"	22'	3:1
54"	8240	5-1/2"	2'-3"	5'-5"	2'-11"	7'-6"	7'-6"	5-1/2"	24'	2.4:1
60"	8730	6"	2'-11"	5'-0"	3'-3"	6'-3"	8'-0"	5"	24'	2:1
66"	10710	6-1/2"	2'-6"	6'-0"	2'-3"	6'-3"	8'-3"	5-1/2"	24'	2:1
72"	12520	7"	3'-0"	6'-6"	1'-9"	6'-6"	9'-0"	6"	24'	1.85:1
78"	14770	7-1/2"	3'-0"	7'-6"	1'-9"	6'-6"	9'-3"	6-1/2"	24'	1.82:1
84"	18160	8"	3'-0"	7'-6-1/2"	1'-9"	6'-6"	9'-3-1/2"	6-1/2"	24'	1.5:1



PLAN



LONGITUDINAL SECTION



END SECTION

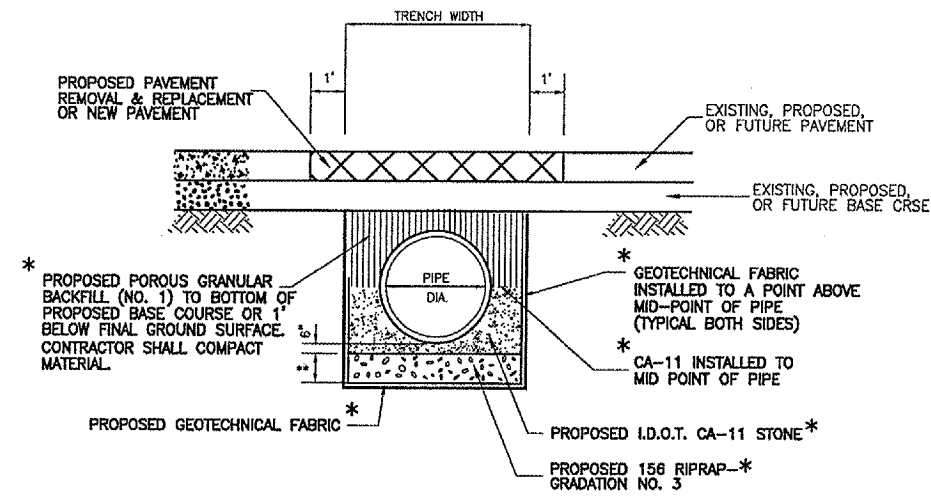
*RADIUS AS FURNISHED BY MANUFACTURER

- NOTES:
1. PRECAST CONCRETE FLARED END SECTIONS SHALL CONFORM TO THE APPLICABLE REQUIREMENTS OF AASHTO M-170 CLASS III, WALL B REINFORCED CONCRETE PIPE.
 2. PRECAST CONCRETE FLARED END SECTION FOR PIPE DIAMETER REQUIRED SHALL BE AS INDICATED ON DETAIL PLAN FOR EACH INDIVIDUAL INSTALLATION.
 3. THE END BLOCK SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE FLARED END SECTION. THE END BLOCK SHALL BE BACKFILLED IN ACCORDANCE WITH ART. 701-3.7 OF THE STANDARD SPECIFICATIONS, COST INCIDENTAL TO END SECTION.
 4. BEDDING MATERIALS AND GEOTECH FABRIC PER 701 PIPE SPECIFICATION IN THE SPECIAL PROVISIONS AS SHOWN IN THIS DETAIL SHALL BE INCLUDED IN THE 752 PIPE END SECTION CONTRACT UNIT PRICE.

MODIFIED I.D.O.T. STANDARD 542301

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PROPOSED TYPICAL SECTION - PIPE TRENCH
 (ALL PROPOSED PIPES)

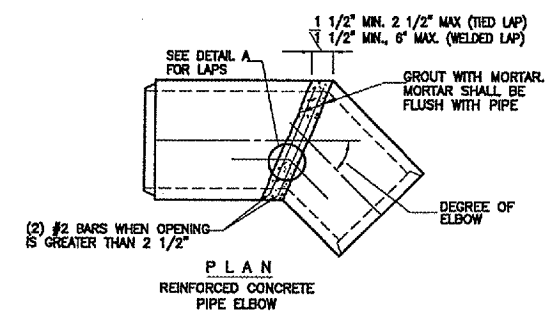
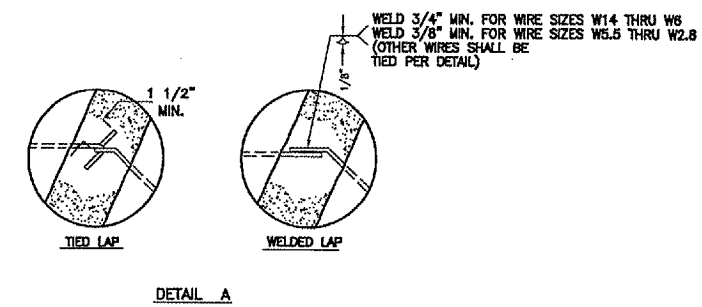
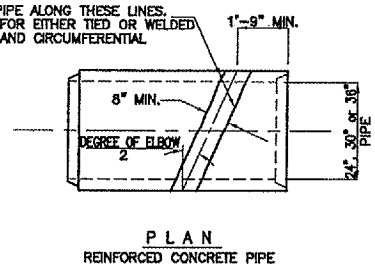
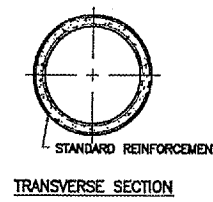


* = COST OF THESE ITEMS SHALL BE INCLUDED IN THE 701 PIPE CONTRACT UNIT PRICES

GENERAL PIPE NOTES:

- GROUND WATER IS EXPECTED. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DEWATERING, TO THE SATISFACTION OF THE ENGINEER, TO INSURE PROPER INSTALLATION OF PIPES. NO ADDITIONAL COMPENSATION BEYOND THE CONTRACT UNIT PRICES WILL BE ALLOWED FOR DEWATERING COSTS.
- COMPACTED BEDDING AND BACKFILL MATERIALS SHALL BE REQUIRED AS SHOWN IN DETAIL. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

PIPE DIA.	TRENCH WIDTH	PVMT. FMT. WIDTH	FOUNDATION DEPTH
6"	3.58'	6'	8"
12"	4.17'	6.5'	8"
18"	4.75'	7'	8"
ELLIPTICAL 18" EQUIV	5.25'	7.5'	8"
24"	5.33'	7.5'	8"



REINFORCED CONCRETE PIPE ELBOW

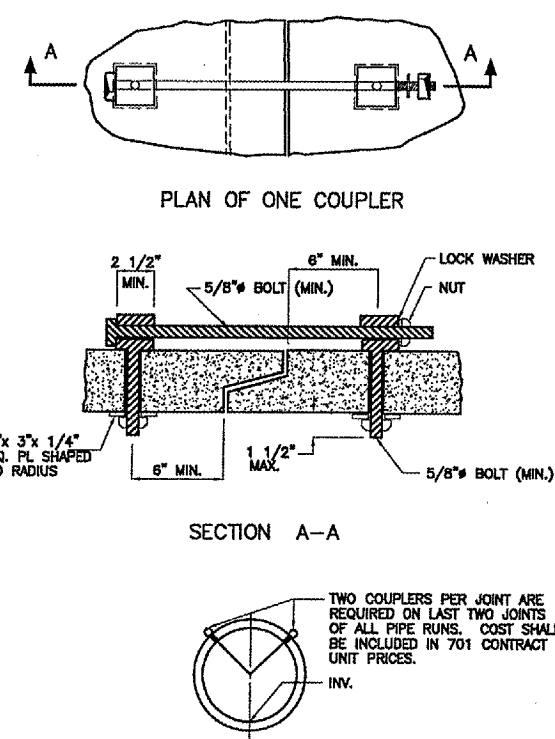
NOTES:

REINFORCED CONCRETE PIPE SHALL CONFORM TO AASHTO M-170
 ADDITIONAL REINFORCEMENT SHALL CONFORM TO AASHTO M-31 or M-53
 DEGREE OF ELBOW AND PIPE SIZE REQUIRED SHALL BE AS INDICATED ON DETAIL PLAN FOR EACH INDIVIDUAL INSTALLATION.
 CEMENT MORTAR WITH BONDING AGENT SHALL BE APPROVED BY THE ENGINEER.
 BASIS OF PAYMENT - REINFORCED CONCRETE PIPE ELBOW SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE "EACH" COMPLETE IN PLACE FOR THE PIPE SIZE SPECIFIED. THE SECTION OF PIPE WHICH INCLUDES THE ELBOW WILL NOT BE INCLUDED IN THE LINEAL FOOT MEASUREMENT OF THE PIPE RUN IN WHICH IT IS INCLUDED.
 GRANULAR BACKFILL, BEDDING MATERIALS, AND GEOTECH FABRIC PER 701 PIPE SPECIFICATIONS IN SPECIAL PROVISIONS SHALL BE INCLUDED IN CONTRACT UNIT PRICE.

MODIFIED I.D.O.T. STANDARD 542601

ITEM AR752850

PROPOSED PIPE COUPLERS

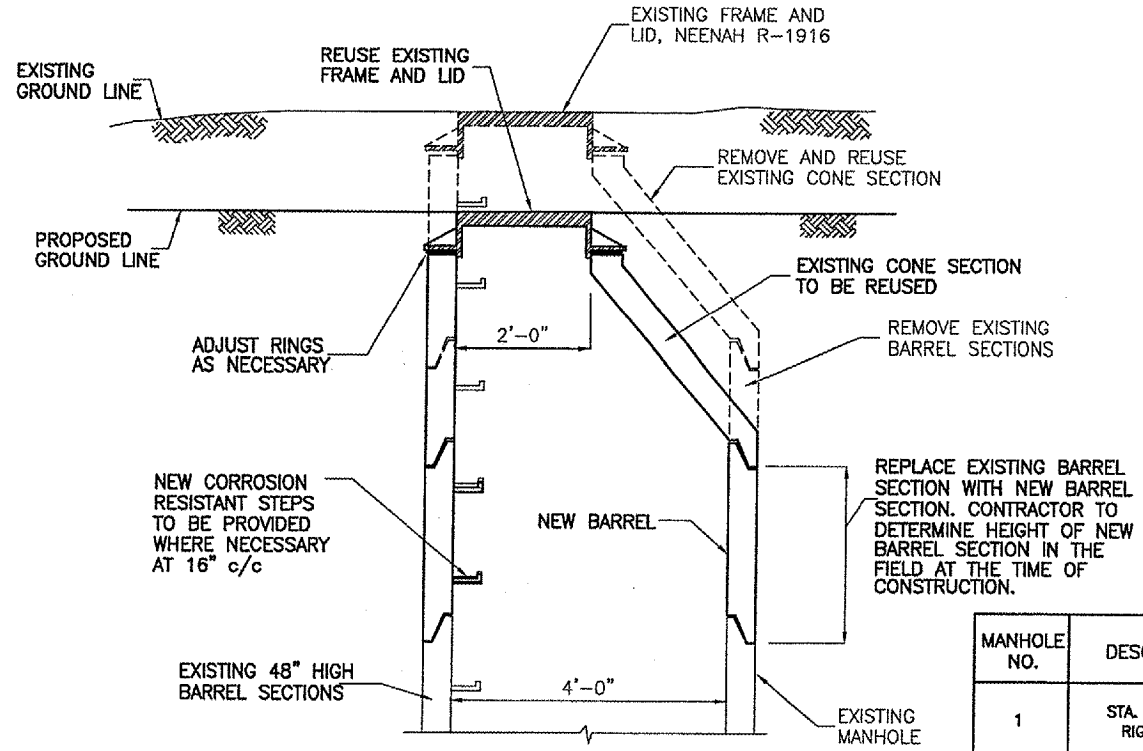


RECONSTRUCT EXISTING SANITARY SEWER MANHOLE

TXWY. K, STA. 381+59.5, RIGHT 96.5'
 ITEM AR751983

GENERAL NOTES

- CONTRACTOR SHALL FIELD VERIFY MEASUREMENTS AND SUPPLY ALL LABOR AND MATERIALS REQUIRED TO RECONSTRUCT MANHOLE TO THE ELEVATION SHOWN BELOW.
- REUSE EXISTING MANHOLE FRAME AND LID. SUPPLY NEW GASKET SEAL AND STAINLESS STEEL BOLTS FOR EXTG. FRAME AND LID. THIS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE FOR RECONSTRUCT MANHOLE.
- BOLT HOLES SHALL BE CLEANED FREE OF RUST AND LID BOLTED DOWN WATER TIGHT. COST TO BE INCLUDED IN RECONSTRUCT MANHOLE UNIT PRICE.
- ALL MATERIALS REMOVED AND NOT REUSED WILL BECOME THE PROPERTY OF THE METROPOLITAN AIRPORT AUTHORITY AND SHALL BE HAULED TO THE OLD LAKE AREA AS DIRECTED BY THE ENGINEER.
- ALL WORK DESCRIBED IN THIS DETAIL WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT PRICE FOR RECONSTRUCT MANHOLE.



MANHOLE NO.	DESCRIPTION	EXISTING FL ELEV.	EXISTING LID ELEV.	PROPOSED LID ELEV.	REMARKS
1	STA. 381+59.5, RIGHT 96.5'	558.19±	575.20±	573.85±*	REUSE EXTG. FRAME & LID

* = MATCH PROPOSED GROUND

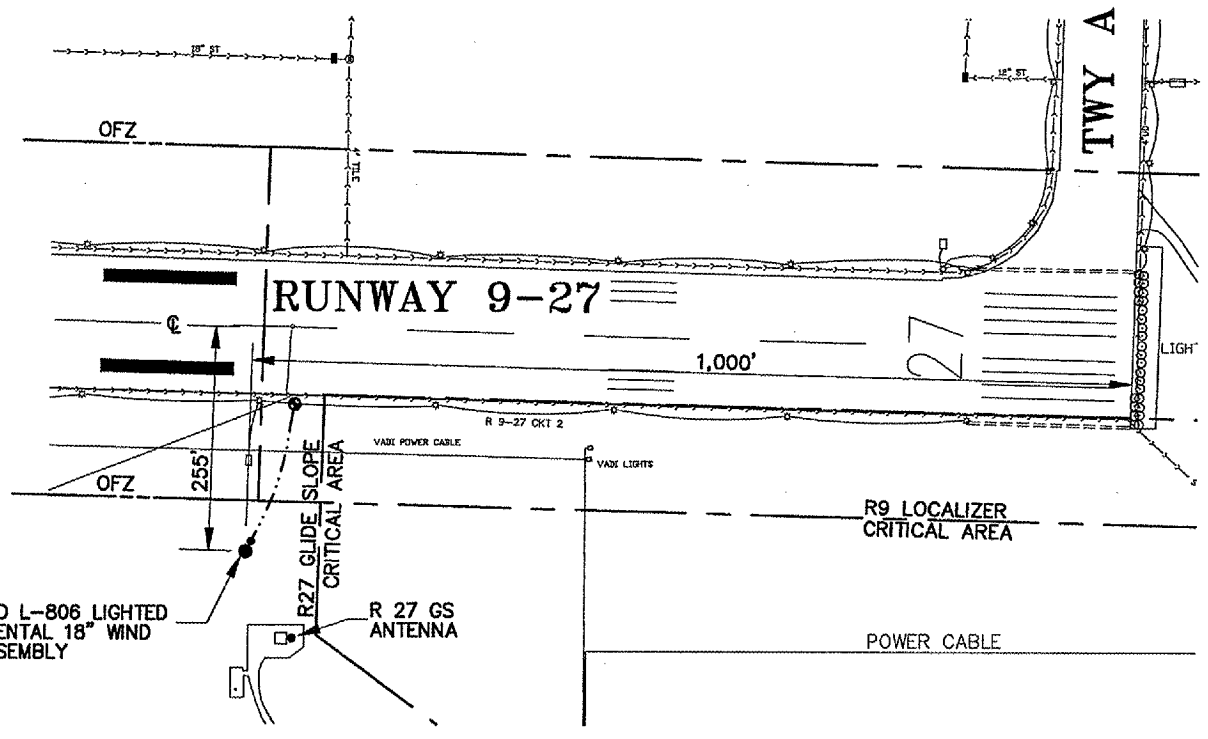
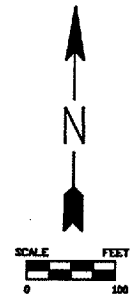
QUAD CITY INTERNATIONAL AIRPORT
 RUNWAY 31 EXTENSION - EARTHWORK
 ILL. MLI-3564, QU001
 SHEET 17 OF 52

SYMBOL LEGEND

- ☼ EXISTING RUWAY / TAXIWAY EDGE LIGHT
- EXISTING L-867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- Ⓢ EXISTING STORM OR SANITARY MANHOLE
- ← EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- ==== EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- PROPOSED L-867 SPLICE CAN OR TRANSFORMER CAN
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1" HDPE UNIT DUCT
- - - PROPOSED ELECTRICAL CIRCUIT, 2/C, #8, EPR CABLES IN 1-1/2" HDPE UNIT DUCT
- PROPOSED L-806 LIGHTED SUPPLEMENTAL 18" WIND CONE ASSEMBLY

GENERAL NOTES:

1. SEE SHEET 20 FOR ELECTRICAL GENERAL NOTES.
2. SEE SHEETS 19-20 FOR ELECTRICAL DETAILS
3. SEE SHEET 19 FOR SUPPLEMENTAL WIND CONE DETAILS.
4. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAYED FOR.

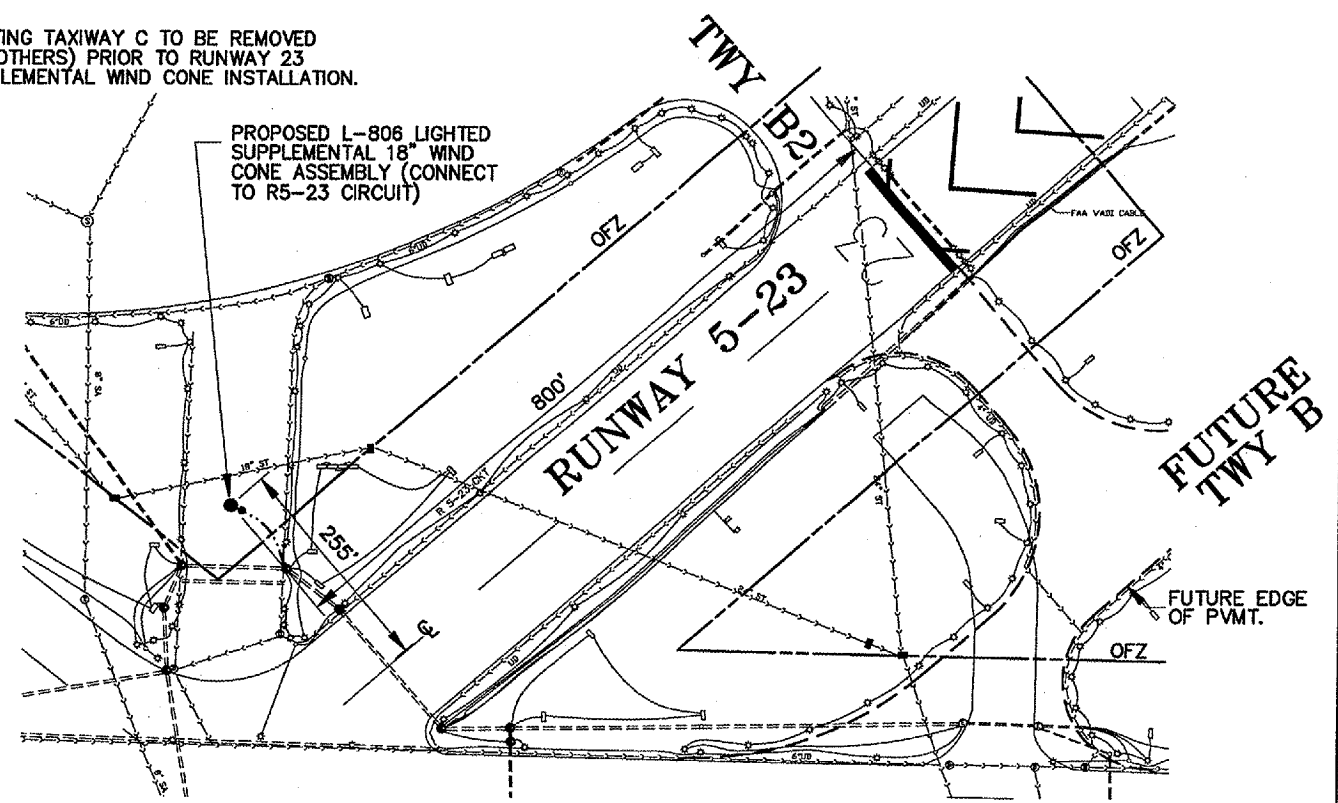


PROPOSED L-806 LIGHTED SUPPLEMENTAL 18" WIND CONE ASSEMBLY

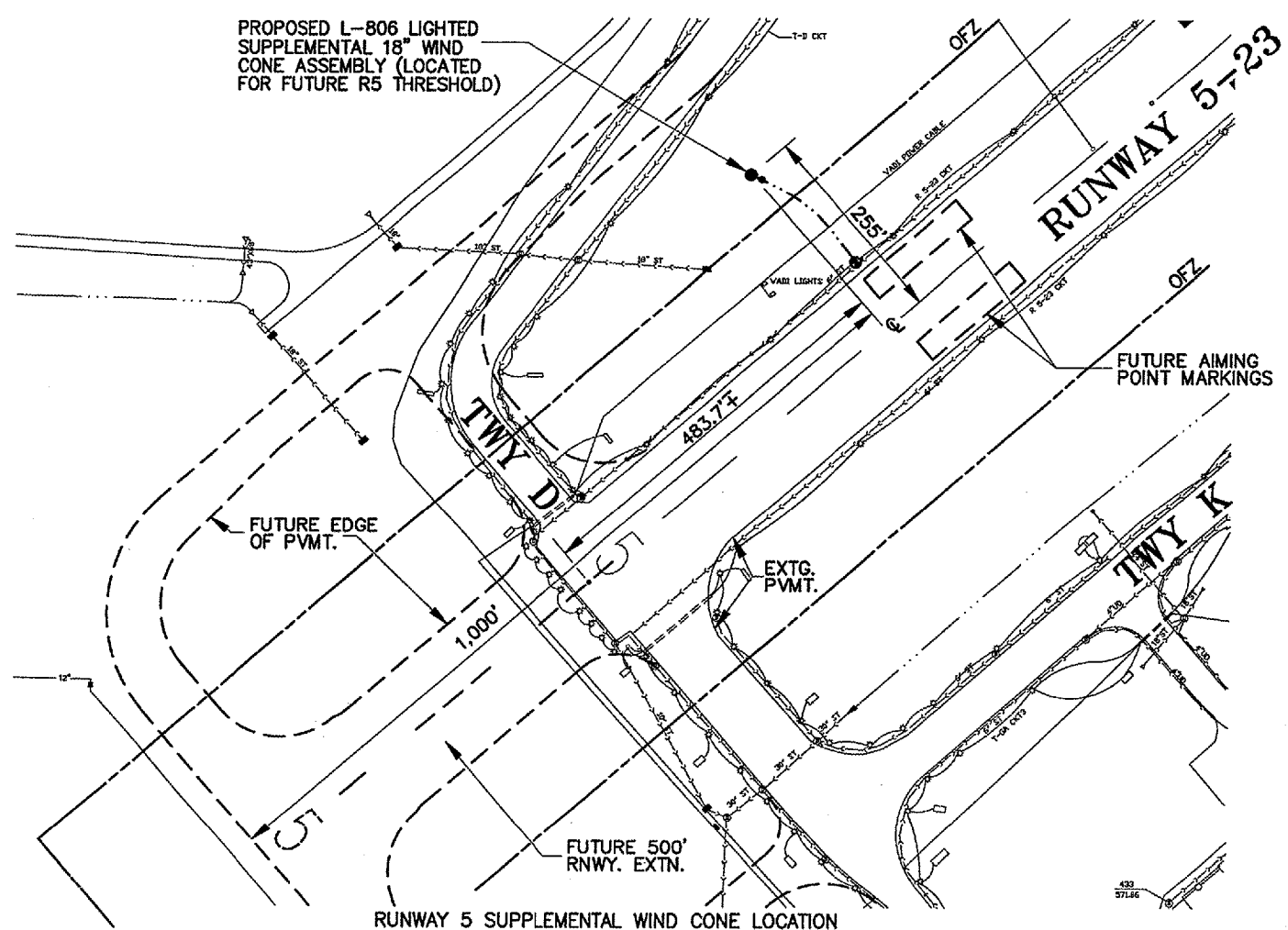
RUNWAY 27 SUPPLEMENTAL WIND CONE LOCATION

NOTE:

EXISTING TAXIWAY C TO BE REMOVED (BY OTHERS) PRIOR TO RUNWAY 23 SUPPLEMENTAL WIND CONE INSTALLATION.



RUNWAY 23 SUPPLEMENTAL WIND CONE LOCATION



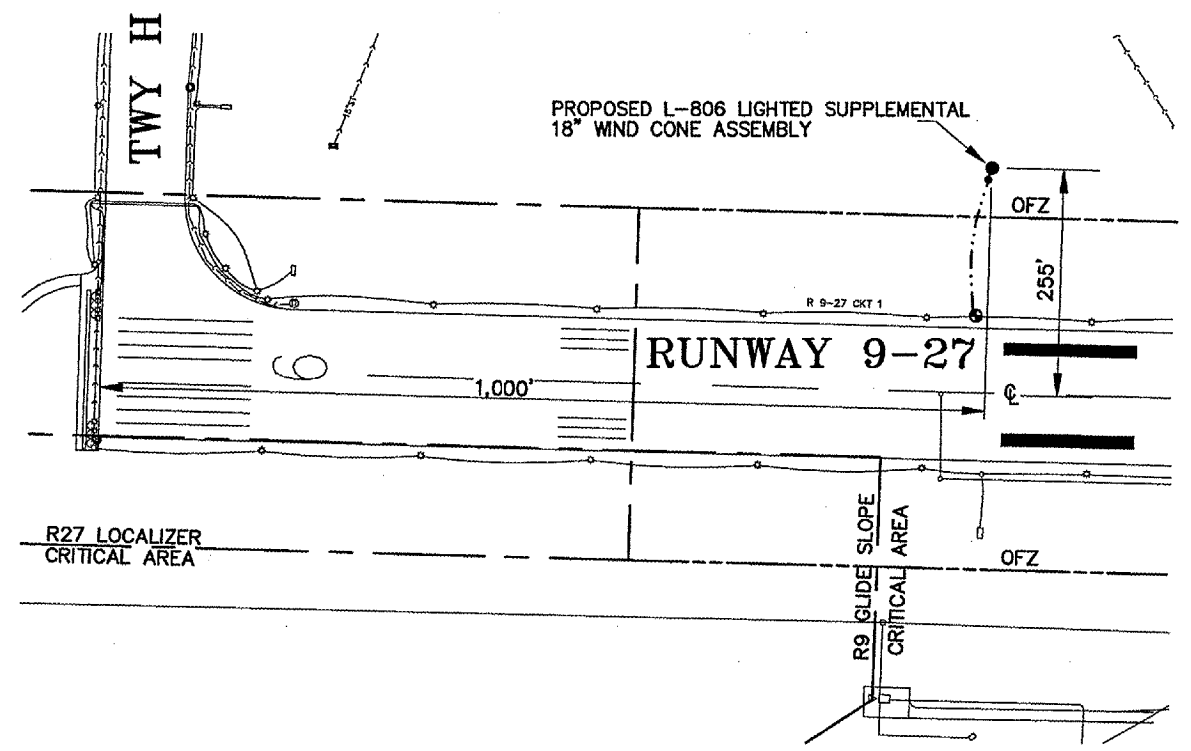
RUNWAY 5 SUPPLEMENTAL WIND CONE LOCATION

GENERAL NOTES:

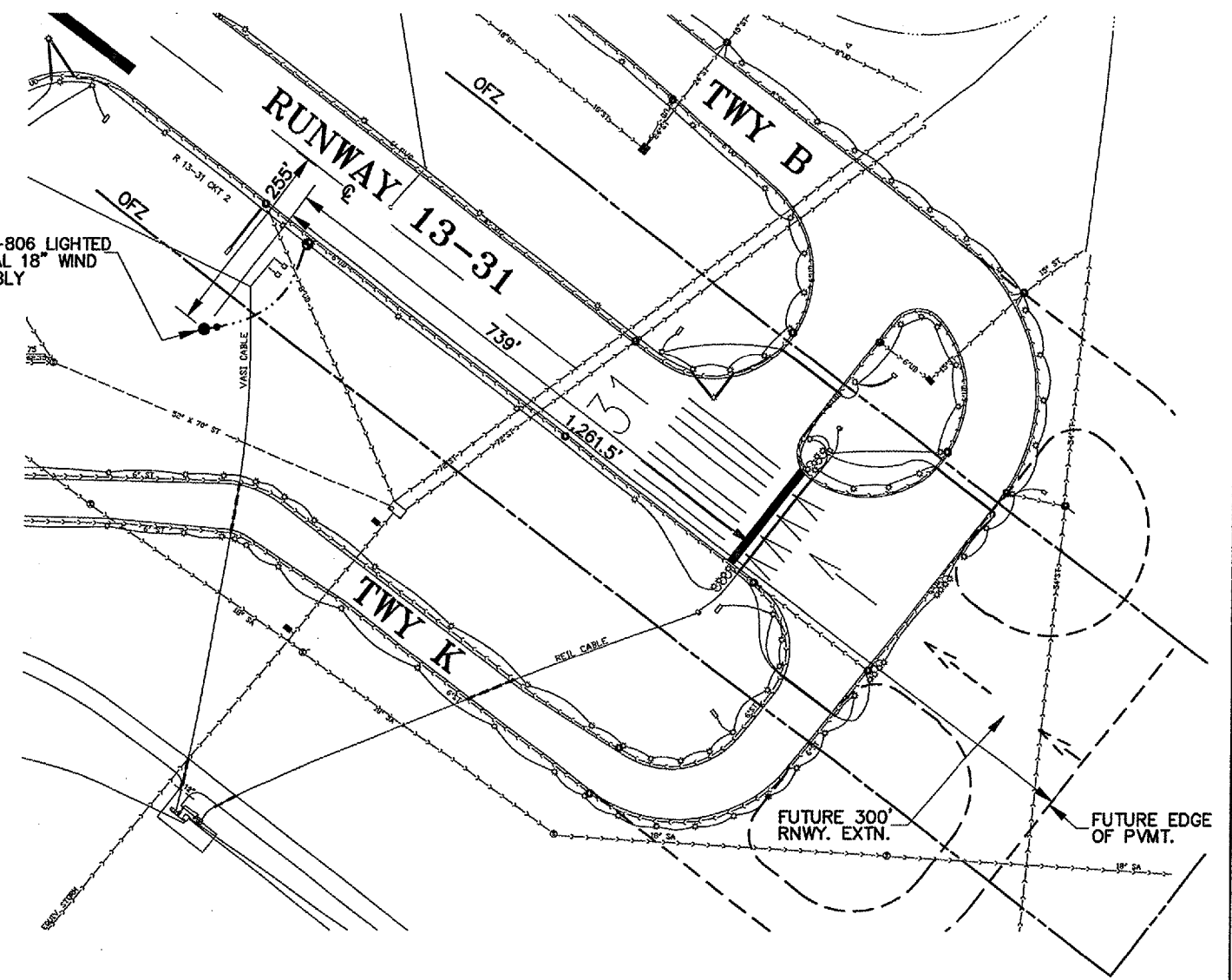
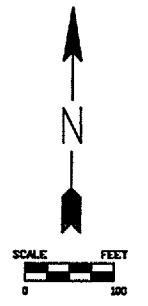
1. SEE SHEET 20 FOR ELECTRICAL GENERAL NOTES.
2. SEE SHEETS 19-20 FOR ELECTRICAL DETAILS
3. SEE SHEET 19 FOR SUPPLEMENTAL WIND CONE DETAILS.
4. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAYED FOR.

SYMBOL LEGEND

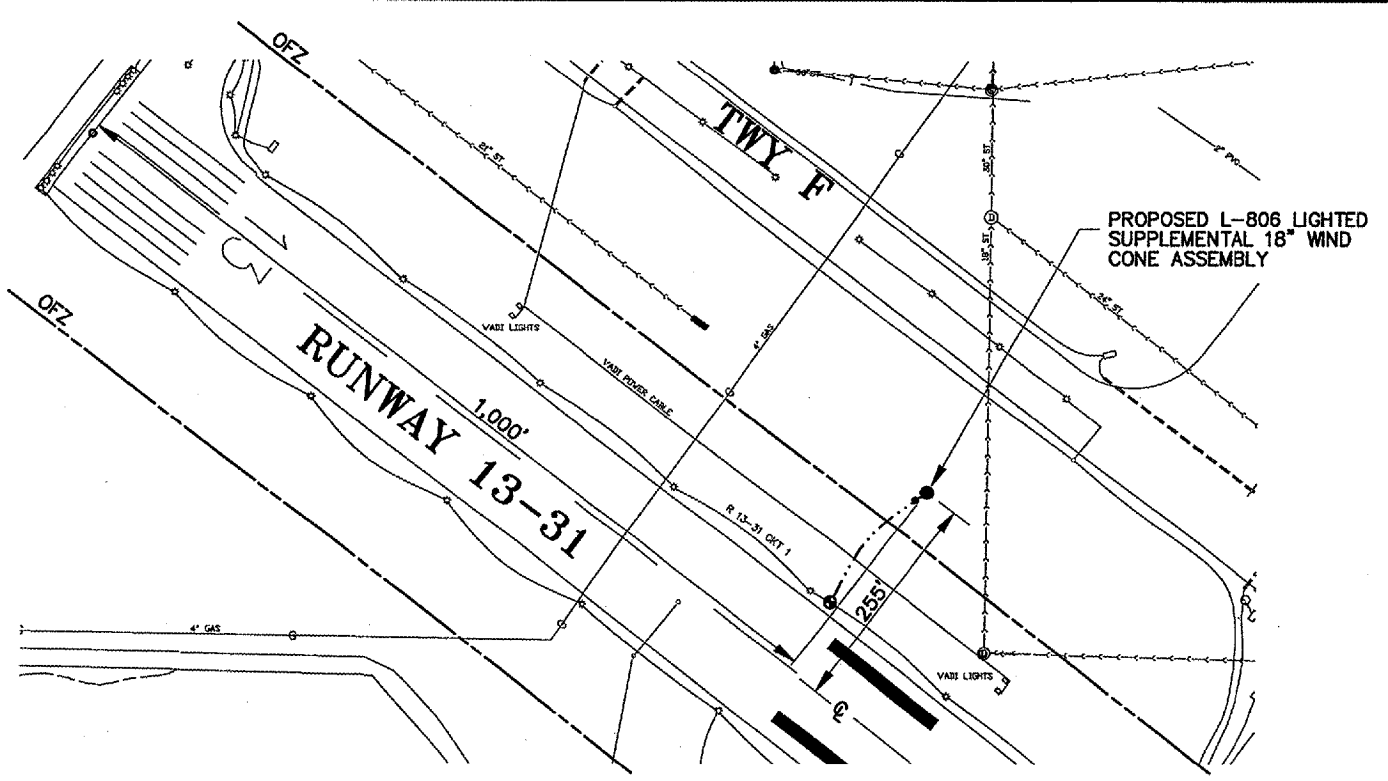
- ☉ EXISTING RWY / TAXIWAY EDGE LIGHT
- EXISTING L-867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- Ⓧ EXISTING STORM OR SANITARY MANHOLE
- EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- ==== EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- ⊕ PROPOSED L-867 SPLICE CAN OR TRANSFORMER CAN
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1" HDPE UNIT DUCT
- PROPOSED ELECTRICAL CIRCUIT, 2/C, #8, EPR CABLES IN 1-1/2" HDPE UNIT DUCT
- PROPOSED L-806 LIGHTED SUPPLEMENTAL 18" WIND CONE ASSEMBLY



RUNWAY 9 SUPPLEMENTAL WIND CONE LOCATION

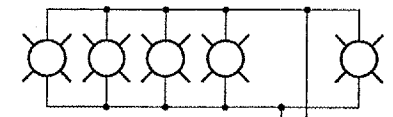


RUNWAY 31 SUPPLEMENTAL WIND CONE LOCATION



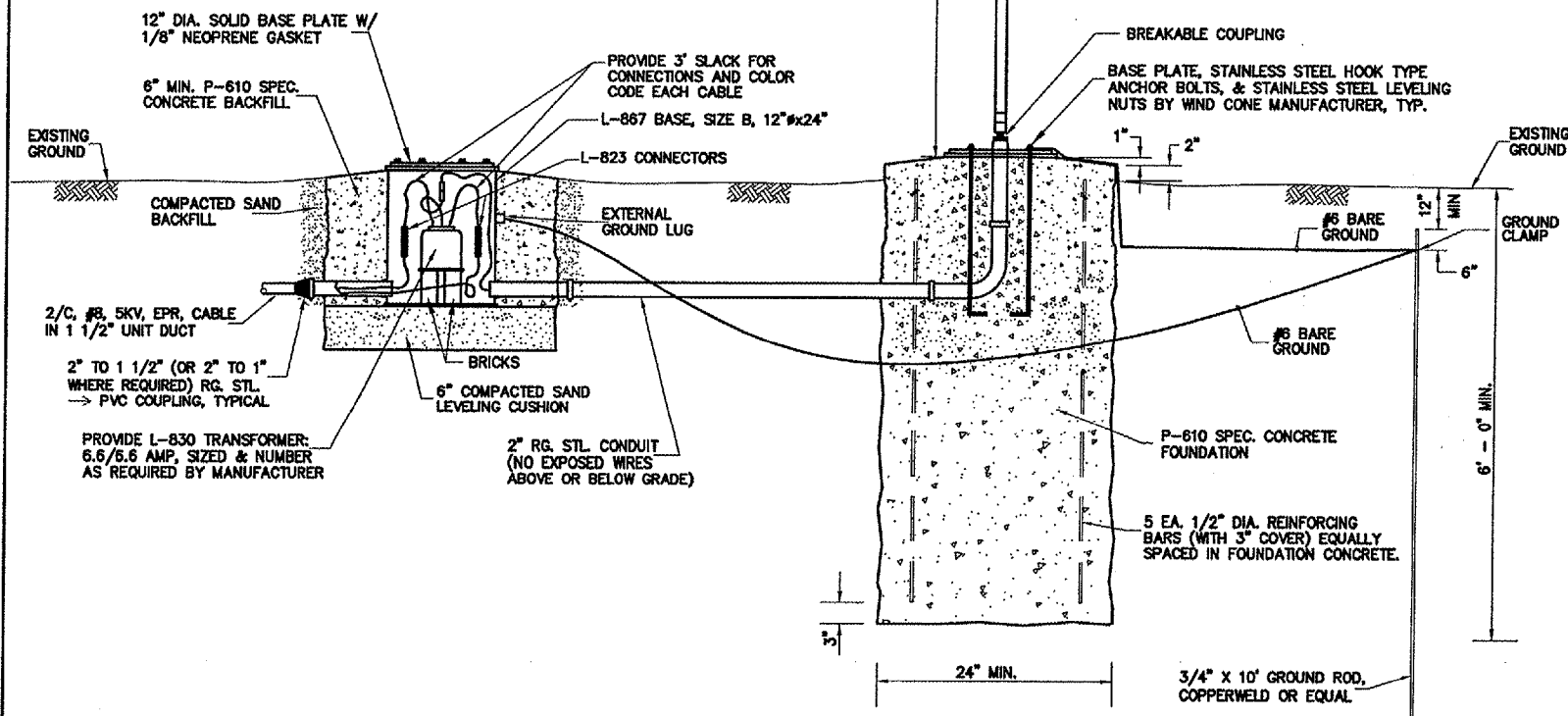
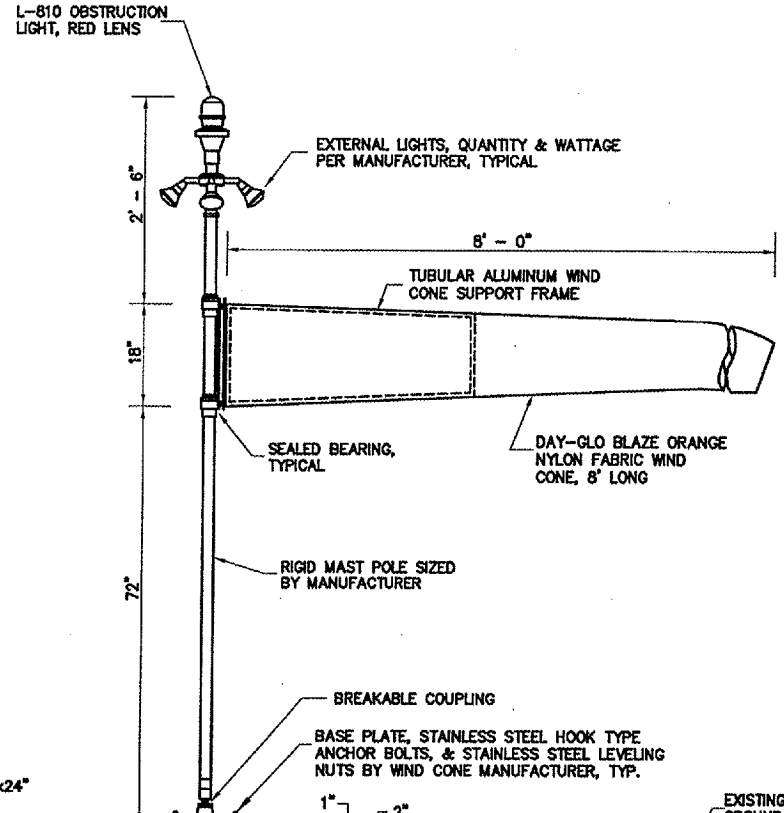
RUNWAY 13 SUPPLEMENTAL WIND CONE LOCATION

L-810 OBSTRUCTION LIGHT, WATTAGE PER MANUFACTURER



EXTERNAL LIGHTS, QUANTITY AND WATTAGE PER MANUFACTURER
 6.6 AMP RUNWAY SERIES CIRCUIT

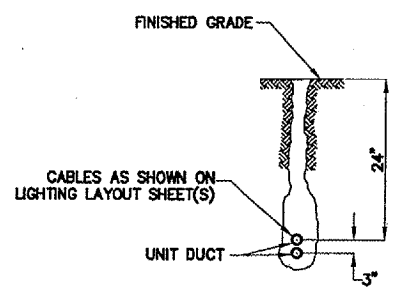
WIRING DIAGRAM



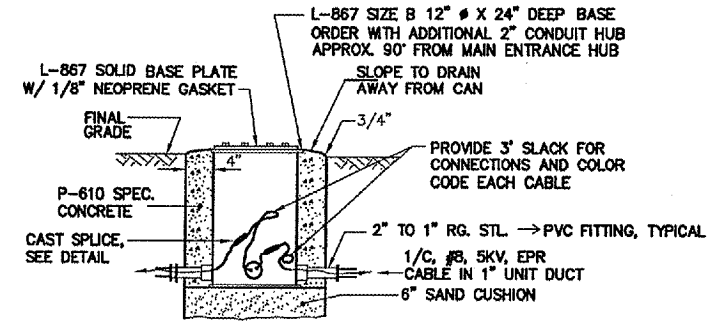
- NOTES:
- BREAKING GROOVE OF BREAKABLE COUPLING SHALL BE 3" TO 3 1/2" ABOVE FINISHED GRADE.

PROFILE VIEW

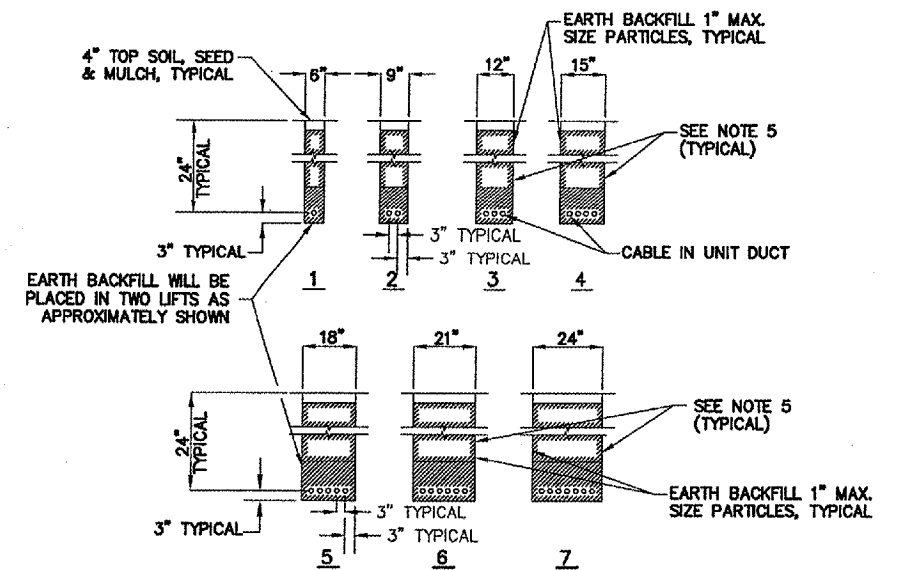
L-806 LIGHTED SUPPLEMENTAL 18" WIND CONE ASSEMBLY DETAILS
 FRANGIBLE - BASE MOUNTED
 6.6 AMP RUNWAY SERIES CIRCUIT
 NOT TO SCALE



PLOWED CABLE (NOT TO SCALE)



L-867 SPLICE CAN DETAIL (NOT TO SCALE)

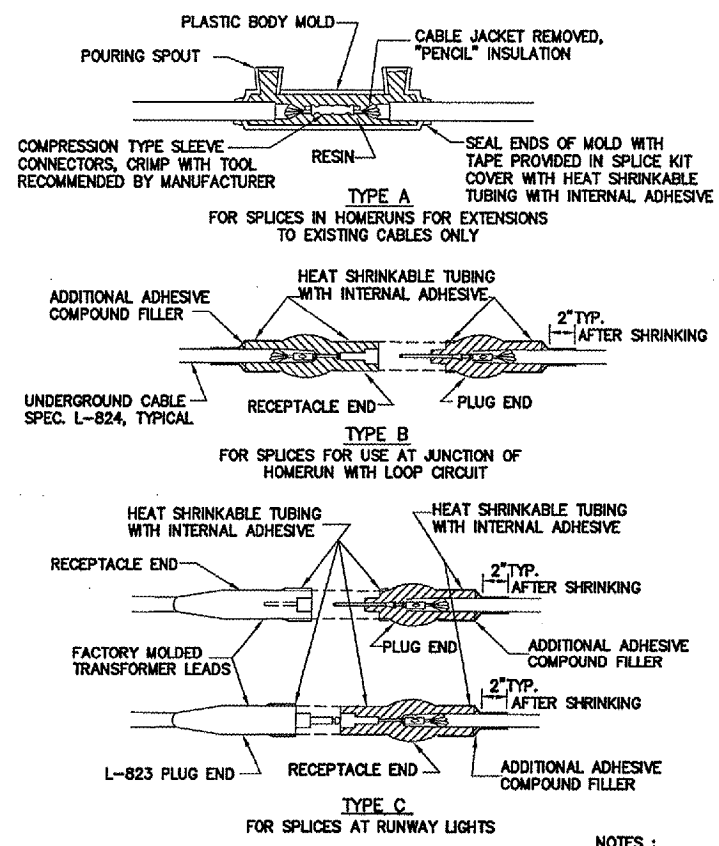


- CABLE TRENCH NOTES:
- DETAIL NUMBERS INDICATE NO. OF CABLES.
 - TRENCHES WITH MORE THAN 7 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE; IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
 - DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
 - ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH. RETURNING MATERIALS AND RATES MAY BE SHOWN ON THE PLANS.
 - INSTALL YELLOW PLASTIC WARNING RIBBON IN TRENCH 9" ABOVE CABLES (TYPICAL ALL TRENCHES).

CABLE TRENCHES (NOT TO SCALE)

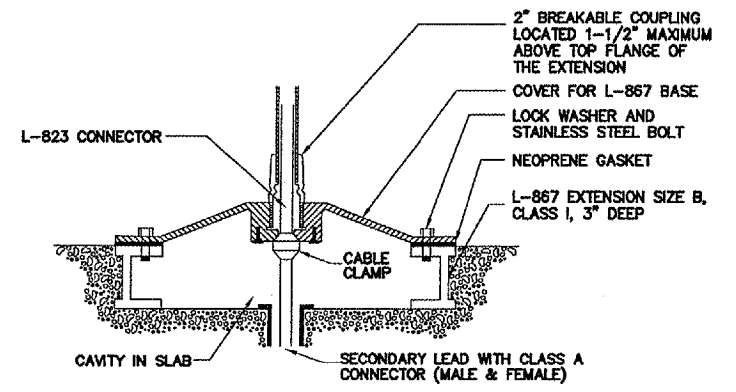
GENERAL ELECTRICAL NOTES:

1. ALL EXCAVATION SHALL BE DONE VERY CAREFULLY. EXCAVATION BY HAND DIGGING SHALL BE REQUIRED AROUND ALL EXISTING DUCT BANKS, SPLICE CANS, MANHOLES, AND EXISTING CABLES. MANY EXISTING ACTIVE UNDERGROUND CABLES, WHOSE EXACT LOCATIONS CANNOT BE DETERMINED, ARE FOUND IN THE PROJECT AREA. IN ORDER TO AVOID EXISTING UNDERGROUND CABLES, THE CONTRACTOR SHALL CONNECT A THUMPER TO ALL EXISTING CIRCUITS AFTER WHICH THEY SHALL BE STAKED IN ALL AREAS REQUIRING TRENCHING OR EXCAVATION. CONTRACTOR SHALL ALSO NOTE THAT LOW VOLTAGE, FAA CABLES ALSO RUN UNDERGROUND THROUGHOUT THESE AREAS. ANY CABLE DAMAGED SHALL BE REPAIRED OR REPLACED TO ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
2. A MINIMUM OF 3 FEET OF SLACK SHALL BE PROVIDED IN THE CABLES AT EACH TRANSFORMER, CONNECTOR, OR SPLICE POINT. ALL CABLE SPLICES SHALL OCCUR IN MANHOLES, LIGHT WELLS OR SPLICE CANS, UNLESS NOTED OTHERWISE.
3. THE ELECTRICAL INSTALLATION, AS A MINIMUM, SHALL MEET THE NATIONAL ELECTRICAL CODE AND LOCAL REGULATIONS.
4. ALL MANUFACTURERS FOR SUPPLYING AIRPORT LIGHTING EQUIPMENT SHALL APPEAR ON THE CURRENT FAA "APPROVED AIRPORT EQUIPMENT" LIST FOUND IN AC 150/5345-53B. THE EQUIPMENT SHALL COMPLY WITH THE APPLICABLE CURRENT FAA ADVISORY CIRCULAR LISTED IN THE FAA "APPROVED AIRPORT EQUIPMENT" LIST FOUND IN AC 150/5345-2 (AIRPORTS ELECTRONIC BULLETIN BOARD NUMBER 14).
5. THE CONTRACTOR SHALL ASCERTAIN THAT ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY HIM (INCLUDING FAA APPROVED EQUIPMENT) ARE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND THE REMAINDER OF THE NEW/EXISTING SYSTEM. ANY NONCOMPATIBLE COMPONENTS FURNISHED BY THIS CONTRACTOR SHALL BE REPLACED BY HIM AT NO ADDITIONAL COST TO THE AIRPORT SPONSOR WITH A SIMILAR UNIT, APPROVED BY THE ENGINEER (DIFFERENT MODEL OR DIFFERENT MANUFACTURER) THAT IS COMPATIBLE WITH THE REMAINDER OF THE AIRPORT LIGHTING SYSTEM.
6. IF THE CONTRACTOR SELECTS TO FURNISH AND INSTALL AIRPORT LIGHTING EQUIPMENT WHICH REQUIRES ADDITIONAL WIRING, TRANSFORMERS, ADAPTERS, MOUNTINGS, ETC., BEYOND THAT SHOWN ON THE DRAWINGS AND/OR LISTED IN THE SPECIFICATIONS, THEN THE COST FOR THE ADDITIONAL ITEMS SHALL BE INCLUDED TO THE CONTRACT UNIT PRICES.
7. THE CONTRACTOR INSTALLED EQUIPMENT (INCLUDING FAA APPROVED) SHALL NOT GENERATE ANY ELECTROMAGNETIC INTERFERENCE IN THE EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AIR NAVIGATION, AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST WITH THE EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
8. WHEN A SPECIFIC TYPE, STYLE, CLASS, ETC. OF FAA APPROVED EQUIPMENT IS SPECIFIED ONLY THAT TYPE, STYLE, CLASS, WILL BE ACCEPTABLE, EVEN THOUGH EQUIPMENT OF OTHER TYPES, STYLES, CLASSES, ETC. MAY BE FAA APPROVED.
9. ALL CONCRETE FOR ELECTRICAL EQUIPMENT SHALL COMPLY WITH SPECIFICATION 610-STRUCTURAL PC CONCRETE 3500 PSI AT 28 DAYS, AIR ENTRAINED CONCRETE MIX SHALL BE USED.
10. BASE MOUNTED BREAKABLE COUPLINGS SHALL NOT HAVE WEEP HOLES TO THE OUTSIDE. PLUGGED UP HOLES SHALL NOT BE ACCEPTABLE. IT SHALL HAVE A 1/4" DIAMETER OR EQUIVALENT OPENING FOR DRAINAGE FROM THE SPACE AROUND THE CONNECTOR INTO THE BASE.
11. THE ELEVATION OF THE BREAKABLE COUPLING GROOVE SHALL NOT EXCEED 1 1/2" ABOVE THE EDGE OF THE COVER IN THE CASE OF A BASE MOUNTED COUPLING.
12. ALL PERMANENT CABLE SPLICES SHALL OCCUR IN MANHOLES, LIGHT WELLS, OR SPLICE CANS, UNLESS NOTED OTHERWISE.
13. MIMIC PANEL COLORS : RUNWAY R9-27 CIRCUIT = WHITE, RUNWAY R13-31 CIRCUIT = WHITE, RUNWAY R5-23 CIRCUIT = WHITE AND TAXWAY B CIRCUIT = PINK.



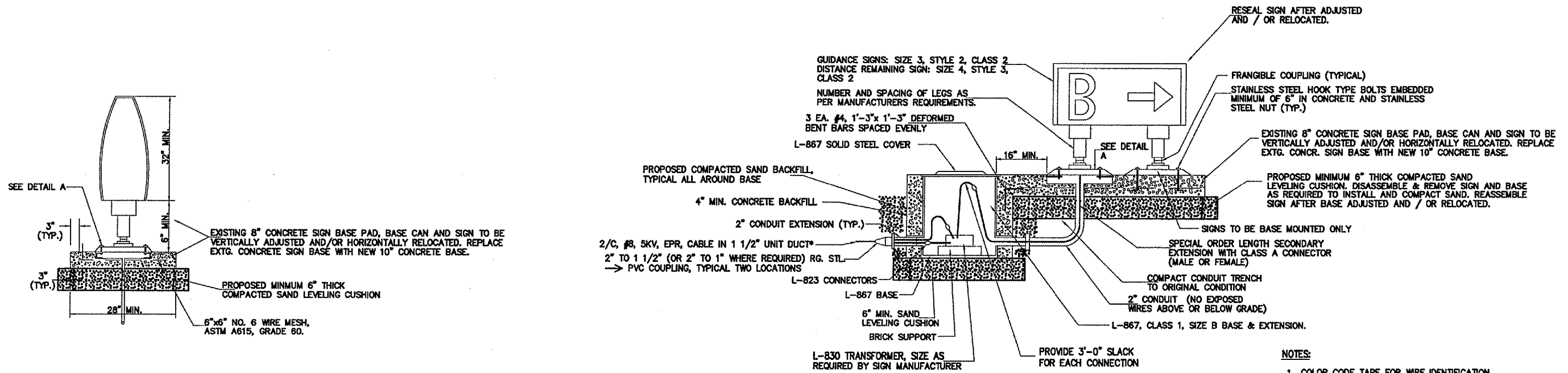
CABLE SPLICES
 (NOT TO SCALE)

- NOTES :
1. SEE LIGHTING LAYOUT SHEET(S) FOR SPLICE TYPE
 2. INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE



DETAIL A

ADJUSTMENT / RELOCATION OF EXISTING GUIDANCE SIGNS



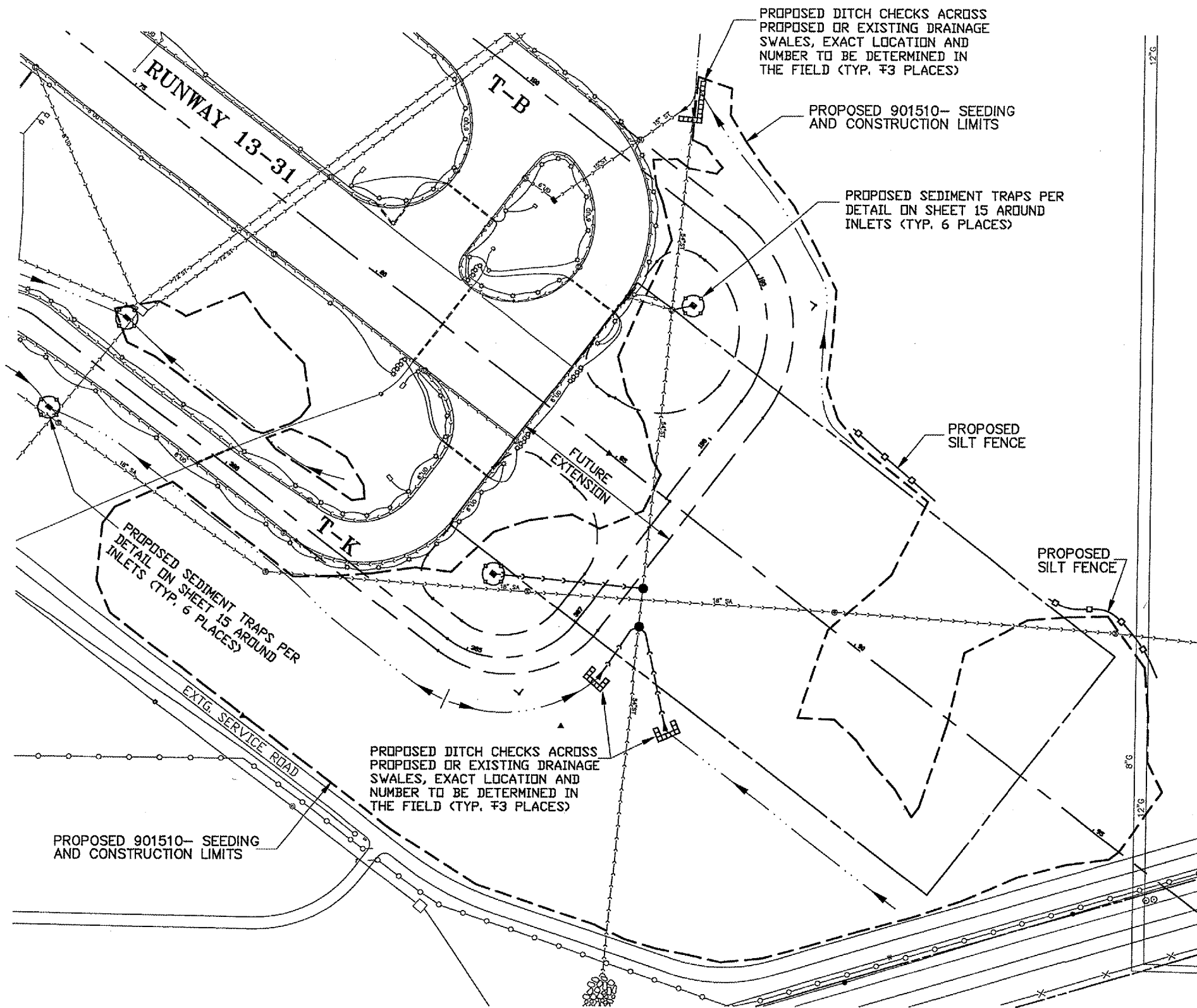
- NOTES:
1. COLOR CODE TAPE FOR WIRE IDENTIFICATION 6" BEFORE L-823 CONNECTORS.
 2. CONTRACTOR MAY REUSE EXISTING MATERIALS AND EQUIPMENT. CONTRACTOR SHALL SUPPLY AND INSTALL NEW EQUIPMENT AND MATERIALS AS REQUIRED TO COMPLETE SIGN ADJUSTMENT / RELOCATION.
 3. DRAINAGE TO FLOW AWAY FROM SIGN. NO WATER PONDING AROUND SIGN BASE PERMITTED.

* = FOR SIGN ELEVATION ADJUSTMENTS, RAISE EXISTING UNIT DUCT AS REQUIRED TO ADJUST ELEVATION OF TRANSFORMER CAN.

SIDE VIEW

FRONT VIEW

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LEGEND:

- EXTG. EDGE LIGHT
- EXTG. GUIDANCE SIGN
- EXTG. ELECTRICAL CABLE
- EXTG. STORM SEWER OR UNDERDRAIN
- EXTG. INLET
- EXTG. OUTLET
- EXTG. MANHOLE
- EXTG. DRAINAGE DITCH
- PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
- EXISTING FENCE
- EXISTING OR PROPOSED DRAINAGE SWALE
- PROPOSED SILT FENCE
- PROPOSED TEMPORARY STRAW BALE DITCH CHECK
- PROPOSED SEDIMENT TRAP

SCALE 0 100 FEET

N

GENERAL NOTES:

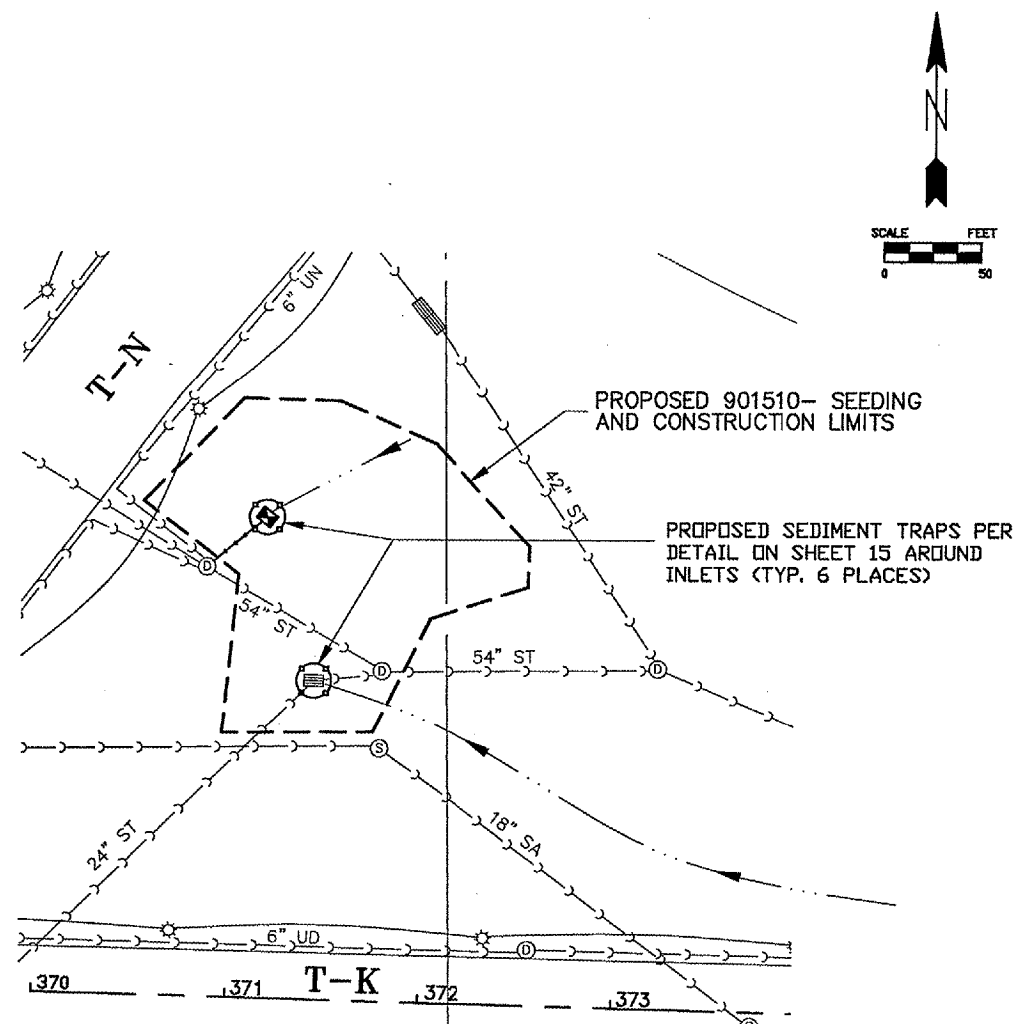
1. TEMPORARY DITCH CHECKS - TWO BALES HIGH WITH SILT FENCING SHALL BE REQUIRED PER IDOT STANDARD 280001 - TEMPORARY EROSION SITE CONTROL SYSTEM DRAWING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING, MAINTAINING, AND REMOVING DITCH CHECKS, SILT FENCE, AND SEDIMENT TRAPS TO THE SATISFACTION OF THE RESIDENT ENGINEER. THIS INCLUDES, BUT IS NOT LIMITED TO, CLEANING EROSION SOILS AS REQUIRED.
2. LOCATION OF THE DITCH CHECKS, SILT FENCES, AND SEDIMENT TRAPS SHOWN ARE APPROXIMATE. ACTUAL LOCATIONS TO BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION.
3. ONCE CONSTRUCTION HAS BEEN COMPLETED, OR TEMPORARILY SUSPENDED FOR LONGER THAN 21 DAYS (SUCH AS A WINTER SHUTDOWN), THE CONTRACTOR SHALL SEED ALL AREAS DISTURBED IN ACCORDANCE WITH ITEM 901510 WITHIN 14 DAYS OF THE LAST DISTURBANCE. DITCH CHECKS, SILT FENCES, AND SEDIMENT TRAPS SHALL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE CONTRACTOR ESTABLISHES A GOOD STAND OF GRASS OF UNIFORM COLOR AND DENSITY TO THE SATISFACTION OF THE ENGINEER.
4. THE CONTRACTOR AND EACH SUBCONTRACTOR RESPONSIBLE FOR WATER POLLUTION CONTROL SHALL DESIGNATE, PRIOR TO BEGINNING CONSTRUCTION, A PERSON OR PERSONS WHO CAN BE CONTACTED IN AN EMERGENCY INVOLVING THEIR WATER POLLUTION CONTROL ITEMS. THESE DESIGNATED PEOPLE SHALL BE AVAILABLE TO REPAIR AND MAINTAIN WATER POLLUTION CONTROL DEVICES ON A 24-HOUR / 7 DAYS PER WEEK BASIS.
5. CONTRACTOR TO EXCAVATE TEMPORARY EROSION CONTROL DRAINAGE SWALE AS REQUIRED TO PREVENT RAIN WATER PONDING AND TO CONTROL STORM WATER RUN-OFF.
6. CONTRACTOR SHALL ADHERE TO THE CITY OF MOLINE'S EROSION AND SEDIMENT CONTROL REGULATIONS AND THE ILLINOIS MANUAL ON EROSION AND SEDIMENT CONTROL.
7. SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY CONSTRUCTION THAT DISTURBS EXISTING STORM WATER RUN-OFF CONDITIONS AND/OR GROUND VEGETATION.
8. EROSION CONTROL MEASURES SHALL BE INSPECTED WEEKLY AND AFTER EACH PRECIPITATION EVENT AND REPLACED OR REPAIRED AS NECESSARY.
9. RESIDENT ENGINEER SHALL CHECK THAT ALL FILL AREAS ARE TO A MINIMUM COMPACTION OF 95% OF THE MATERIALS STANDARD PROCTOR MAXIMUM DRY DENSITY.
10. SILT FENCE, SEDIMENT TRAPS, AND HAY BALES SHALL BE CLEANED OR REPLACED WHEN SILT BUILDS UP TO WITHIN ONE FOOT OF THE TOP OF THE SILT FENCE OR HAY BALES.
11. ADDITIONAL EROSION CONTROL DEVICES SHALL BE USED AS REQUIRED. THE COSTS OF ALL MEASURES NECESSARY TO COMPLY WITH THIS STORM WATER POLLUTION PREVENTION PLAN SHALL BE INCLUDED IN THE ITEM 156500 - TEMPORARY EROSION CONTROL LUMP SUM PRICES.
12. SEE SHEET 22 FOR STORM WATER MANAGERS SIGNATURE CHART, ANTICIPATED PROBABLE CONSTRUCTION ACTIVITIES SCHEDULE, AND EROSION / SEDIMENT CONTROL MEASURES LIST.

RUNWAY 31 AREA PLAN

ANTICIPATED PROBABLE CONSTRUCTION ACTIVITIES SCHEDULE		WEEK											
NO.	ITEM DESCRIPTION	1	2	3	4	5	6	7	8	9	10	11	12
1	INSTALL SEDIMENT/EROSION CONTROLS	█											
2	CLEARING AND GRADING		█	█	█	█	█	█	█				
3	EXCAVATION AND EMBANKMENT			█	█	█	█	█	█	█			
4	TURFING											█	█
5	MAINTAIN SEDIMENT/EROSION CONTROL	█	█	█	█	█	█	█	█	█	█	█	█
6	CLEAN-UP												█

STORM WATER MANAGERS					
	NAME	ADDRESS	TELEPHONE NUMBER		SIGNATURE
			WORK	HOME	
CONTRACTOR					
SUBCONTRACTOR					
SUBCONTRACTOR					

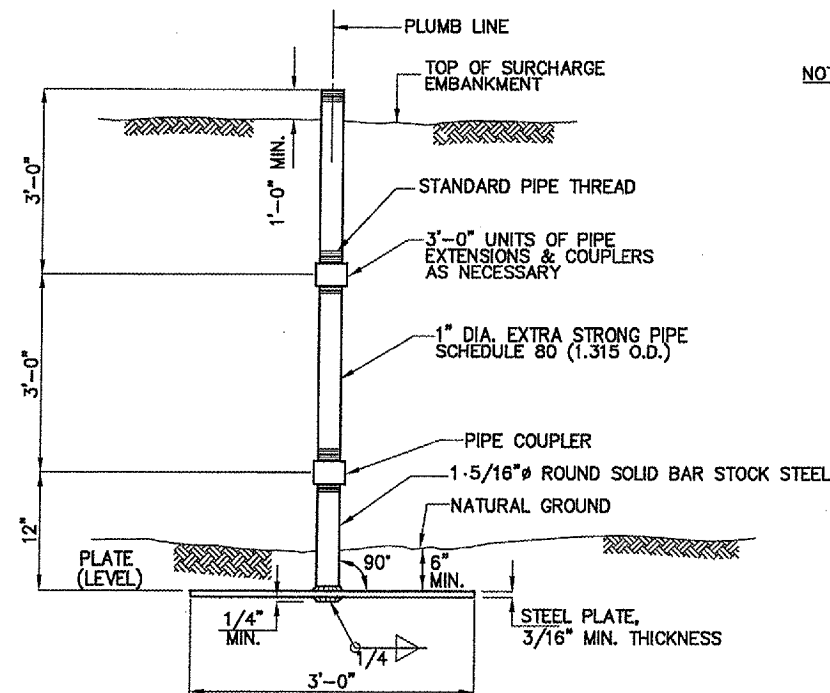
EROSION AND SEDIMENT CONTROL MEASURES		
ITEM	LOCATION	SPECIAL PROVISION/ PAY ITEM
TEMPORARY DITCH CHECKS	RUNWAY 31, STA. 87+00, RT; RUNWAY 31, STA. 88+40, RT. AND TAXIWAY B, STA. 182+60, LT.	156500
TEMPORARY SILT FENCE	AT ALL SEDIMENT TRAP & DITCH CHECK LOCATIONS. ALSO AT RUNWAY 31 LEFT, STA. 87+80 TO STA. 89+60 AND RUNWAY 31 LEFT, STA. 92+05 TO STA. 94+25	156500
TEMPORARY SEDIMENT TRAPS	RNWX. 31, STA. 84+45.5, LT.; RNWX. 31, STA. 84+57, RT.; TXWY. K, STA. 371+14, LT.; TXWY. K, STA. 371+30, LT.; TXWY. K STA. 377+24, RT. AND TXWY. K STA. 377+24, LT.	156500
EXCELSIOR BLANKET	TO BE DETERMINED BY THE RESIDENT ENGINEER IN THE FIELD AT THE TIME OF CONSTRUCTION.	908520



WEST TAXIWAY K AREA PLAN

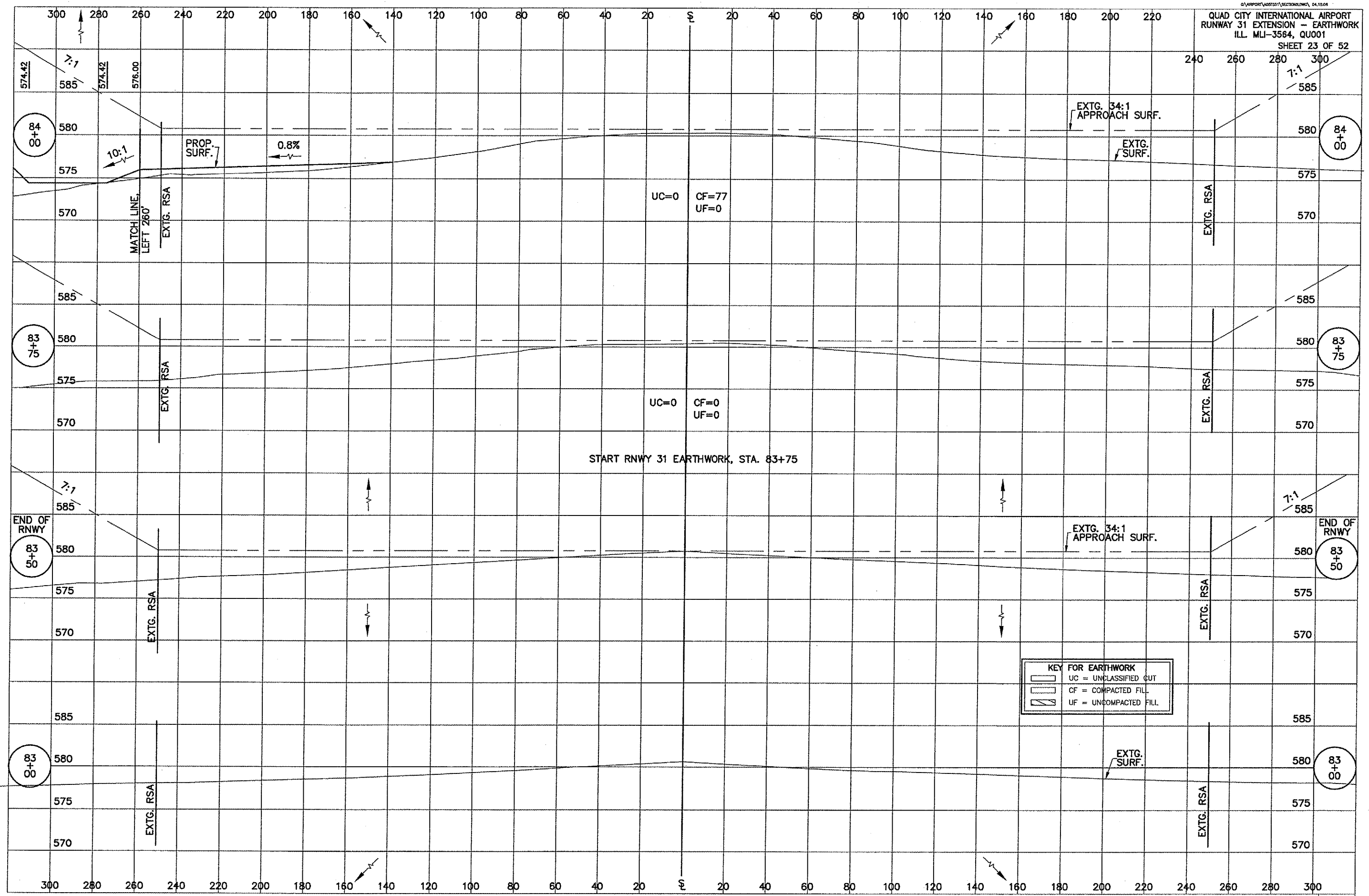
NOTE:
 SEE SHEET 21 FOR STORM WATER POLLUTION PREVENTION GENERAL NOTES.

- LEGEND:
- EXTG. EDGE LIGHT
 - EXTG. GUIDANCE SIGN
 - EXTG. ELECTRICAL CABLE
 - EXTG. STORM SEWER OR UNDERDRAIN
 - EXTG. INLET
 - EXTG. OUTLET
 - EXTG. MANHOLE
 - EXTG. DRAINAGE DITCH
 - PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
 - EXISTING FENCE
 - EXISTING OR PROPOSED DRAINAGE SWALE
 - PROPOSED SILT FENCE
 - PROPOSED TEMPORARY STRAW BALE DITCH CHECK
 - PROPOSED SEDIMENT TRAP



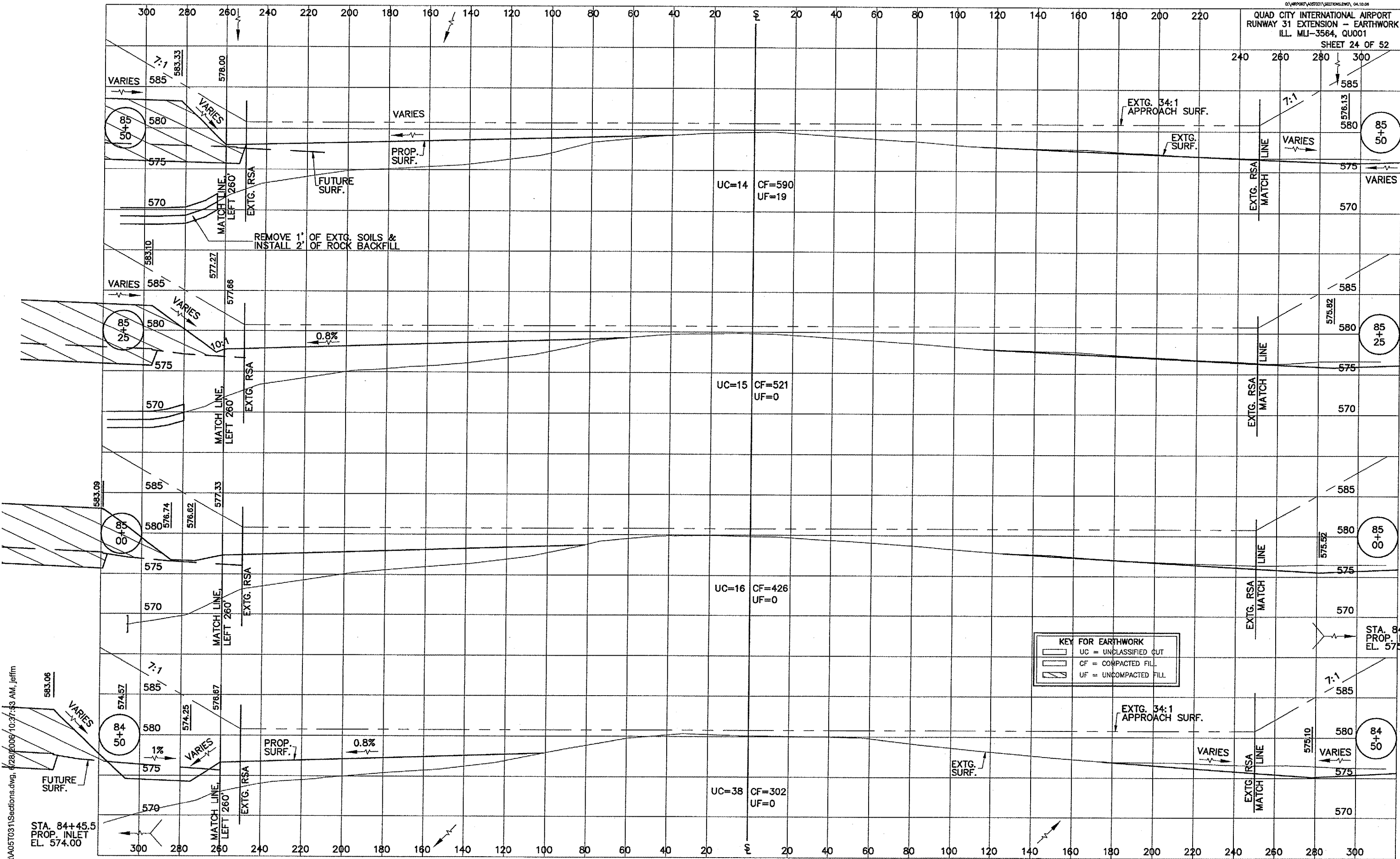
- NOTES:
- INSTALL PROPOSED SETTLEMENT PLATES PRIOR TO INSTALLING THE EMBANKMENT SURCHARGE SOILS.
 - SEE SPECIFICATIONS FOR ADDITIONAL INSTALLATION DETAILS. PLATES TO BE INSTALLED IN THE LOCATIONS SELECTED BY THE ENGINEER.
 - INSTALL ADDITIONAL 3' LONG PIPE SECTIONS AS REQUIRED.

PROPOSED SETTLEMENT PLATES
 (5 EACH REQ'D)



RUNWAY 31 CROSS SECTIONS, STA 83+00 TO STA 84+00

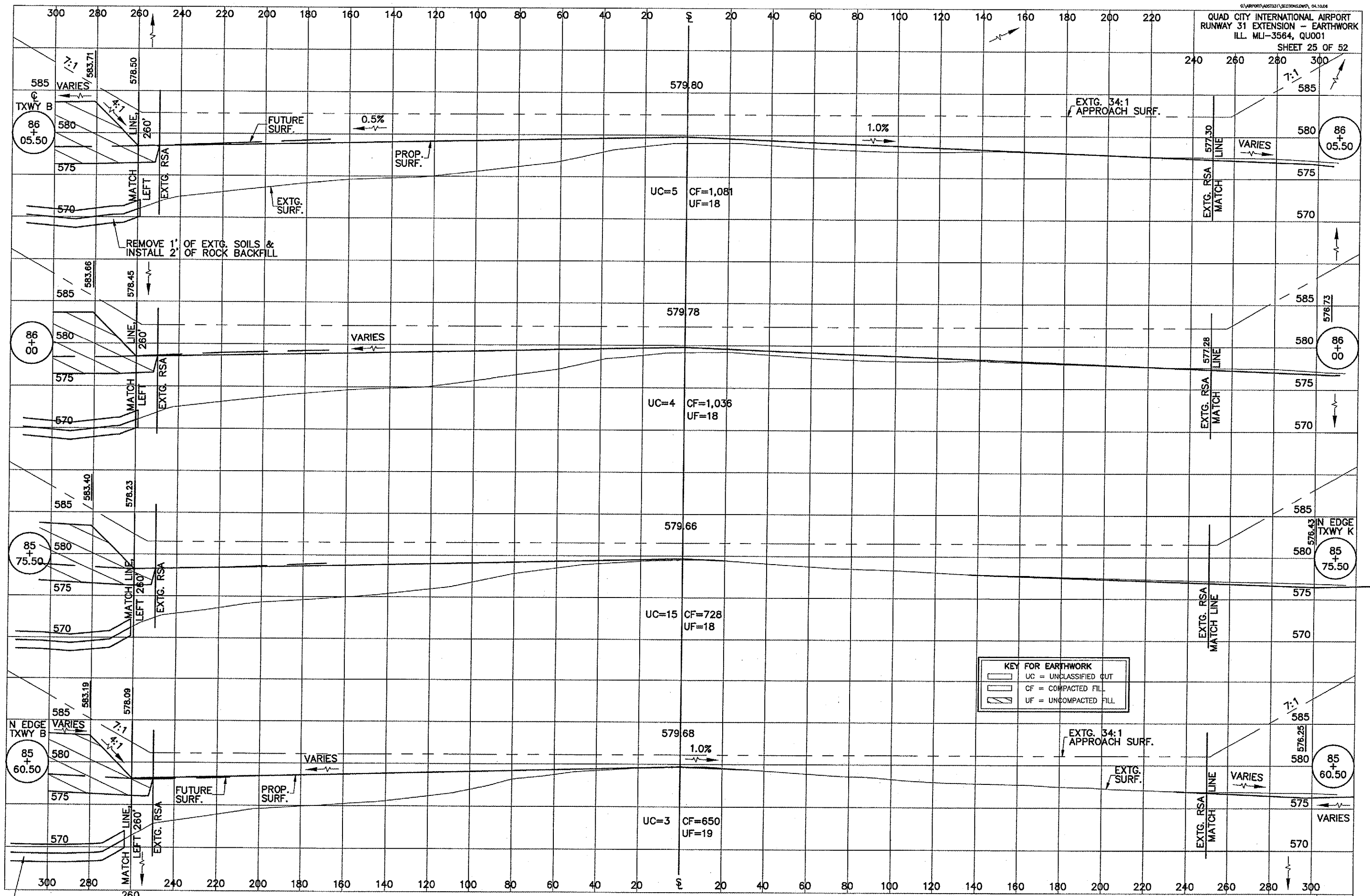
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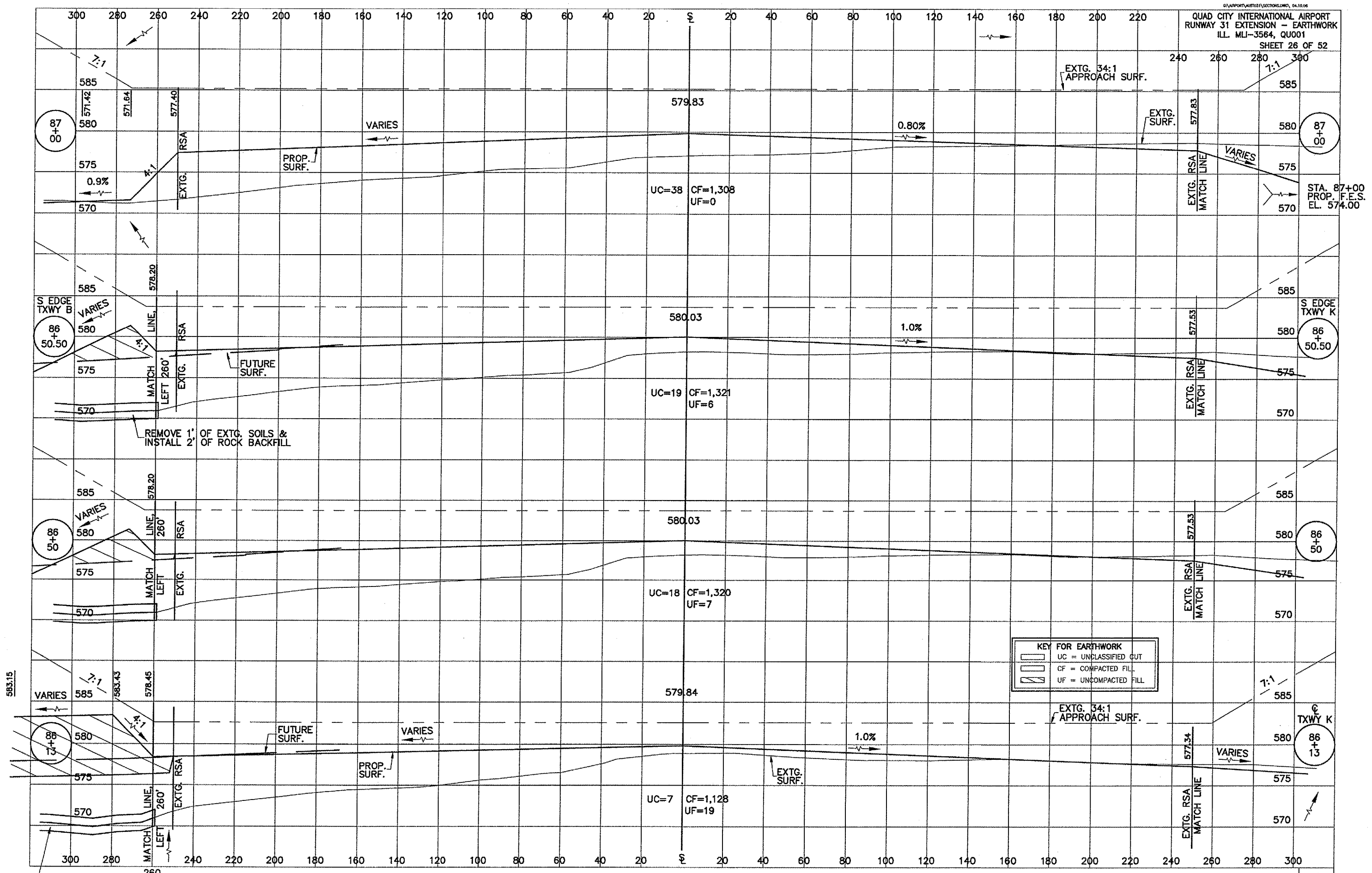
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[Symbol]	UF = UNCOMPACTED FILL

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RUNWAY 31 CROSS SECTIONS, STA 84+50 TO STA 85+50



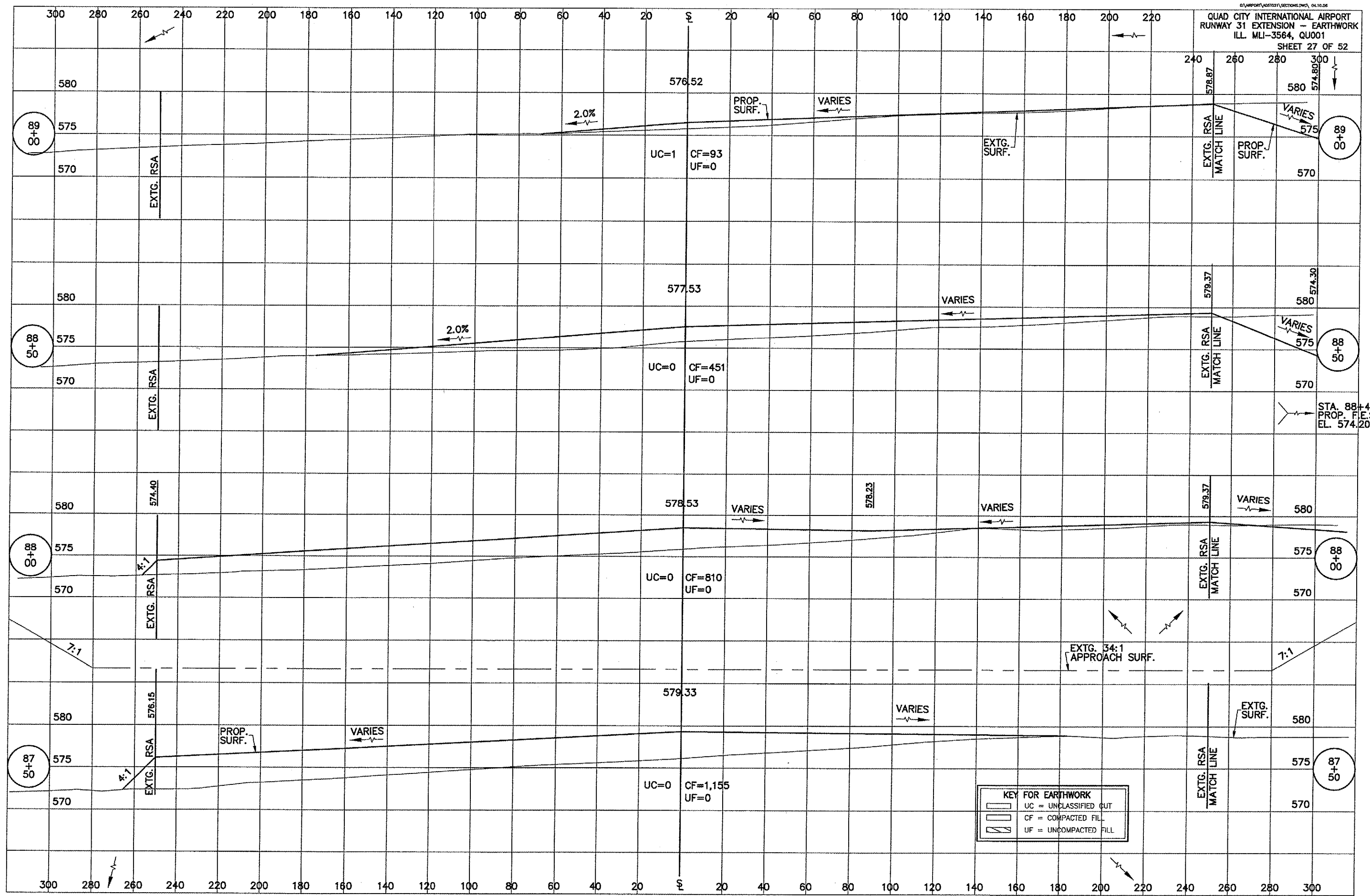
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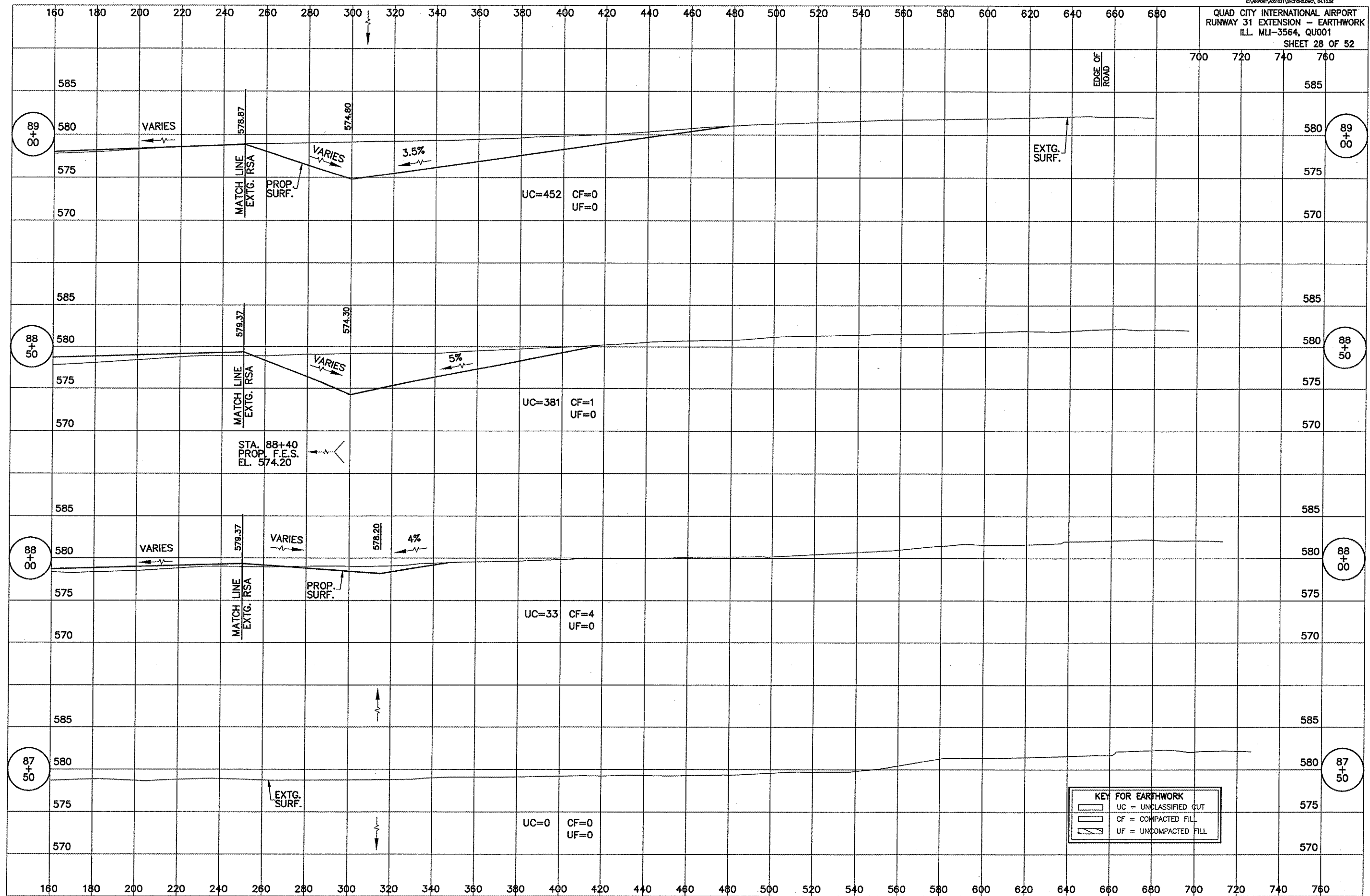
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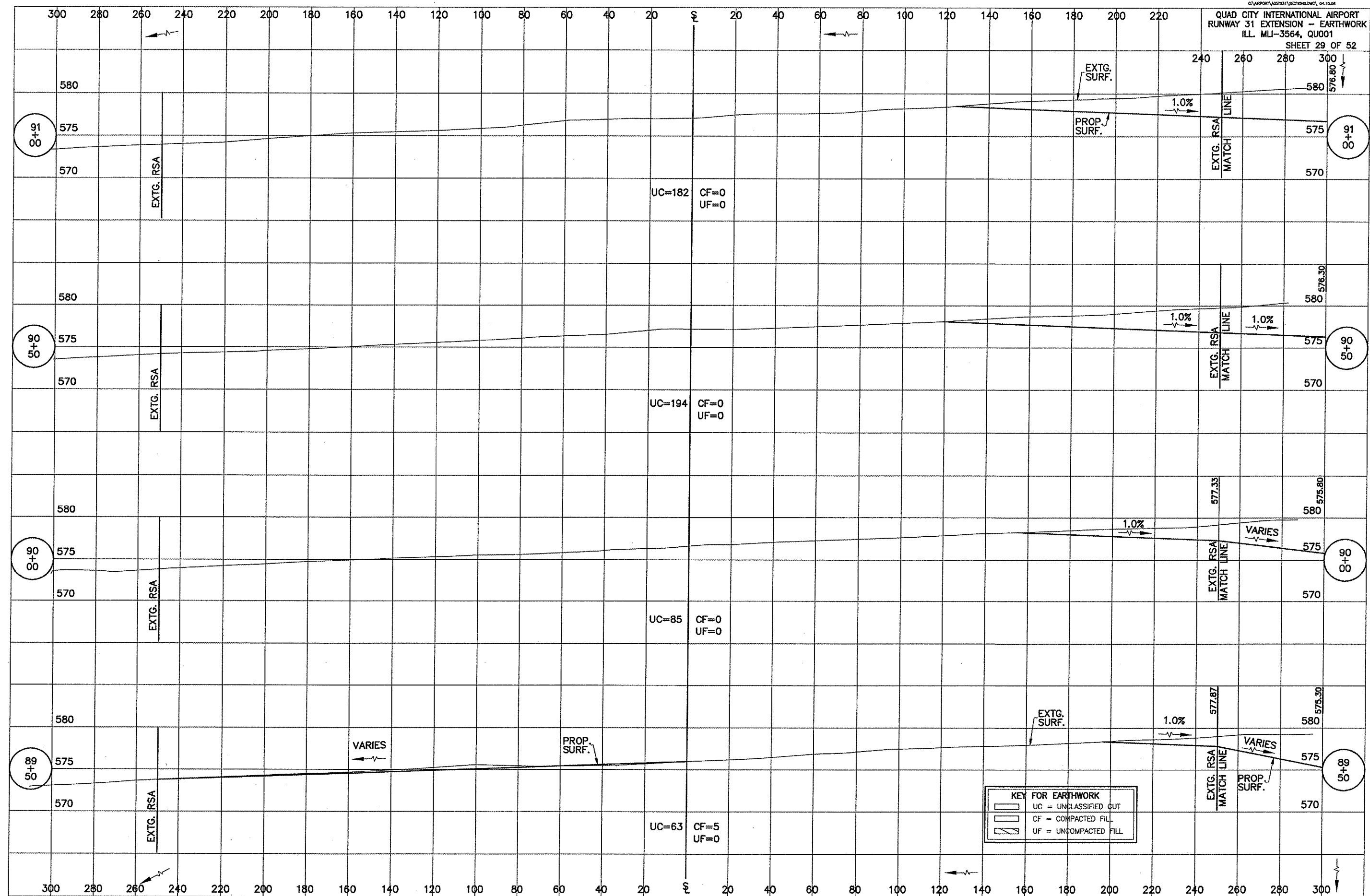
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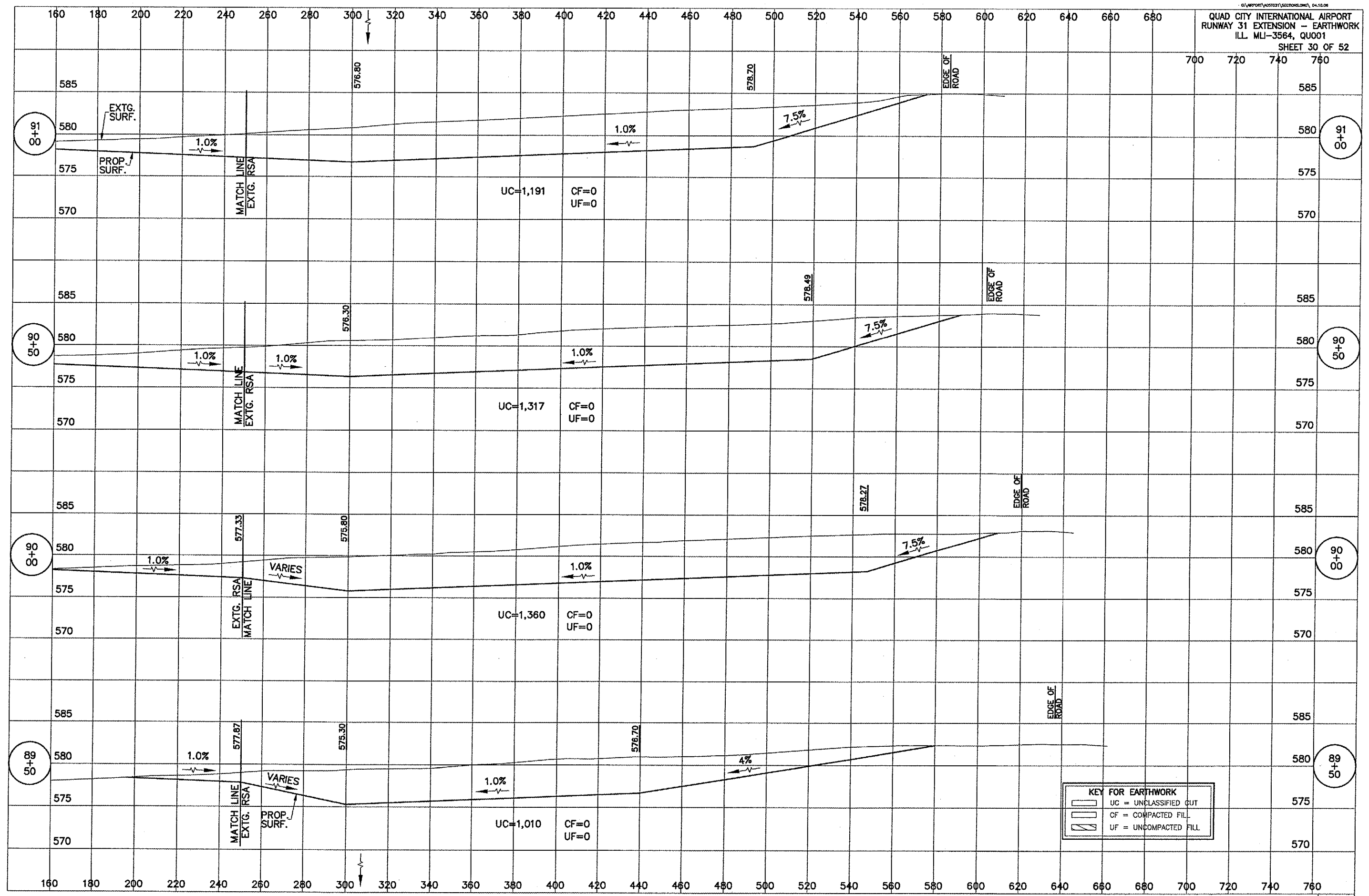
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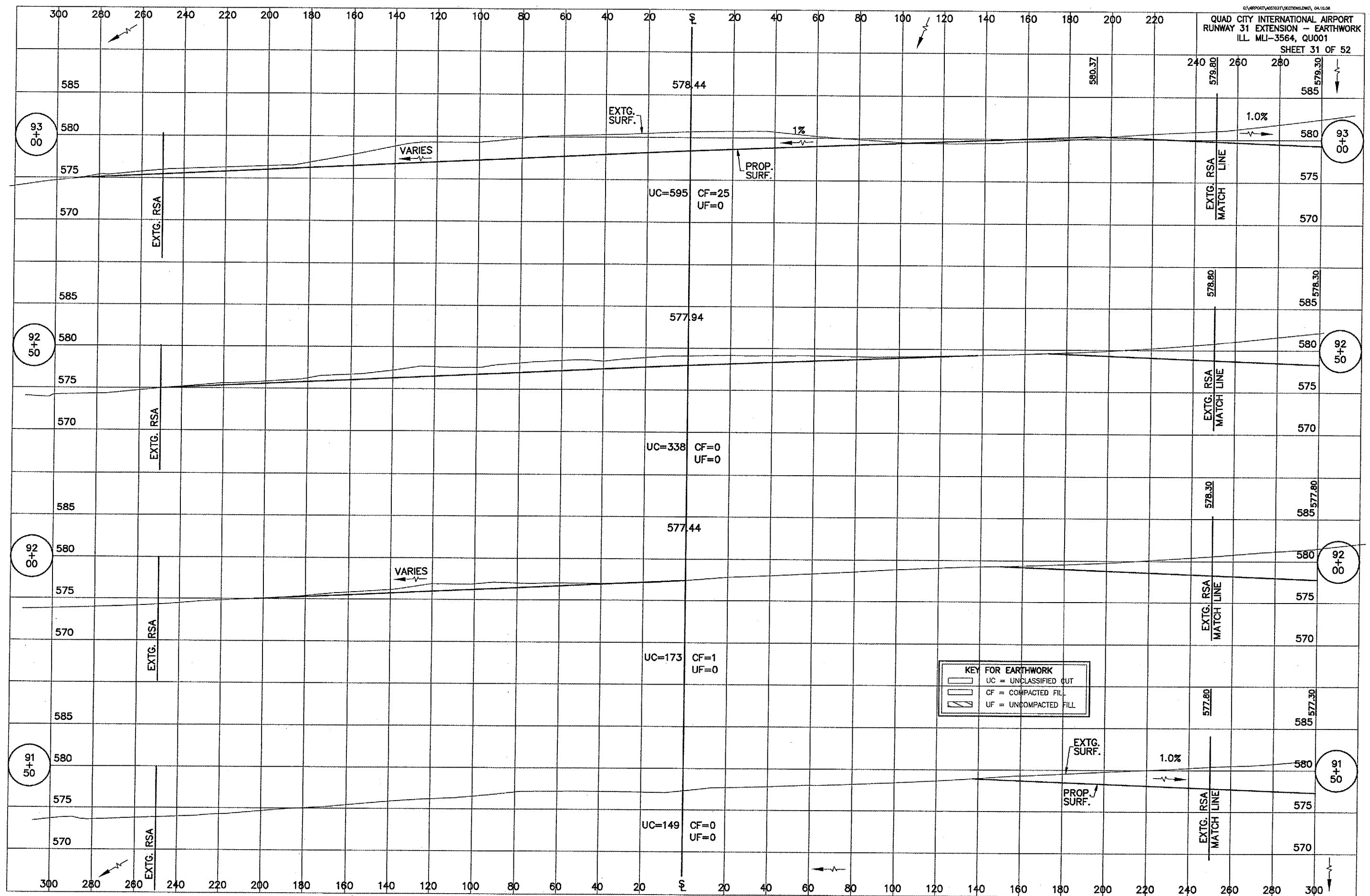


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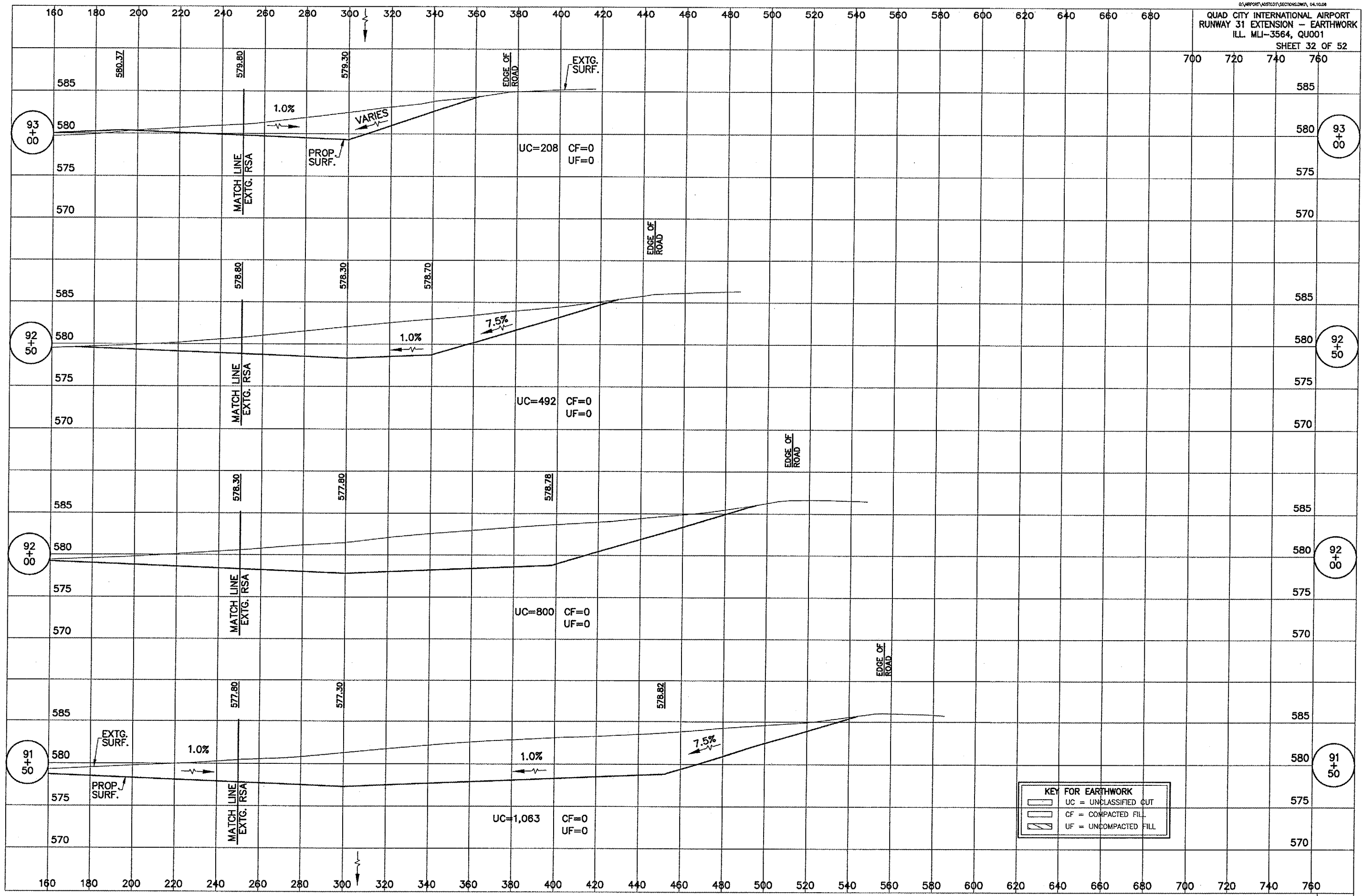


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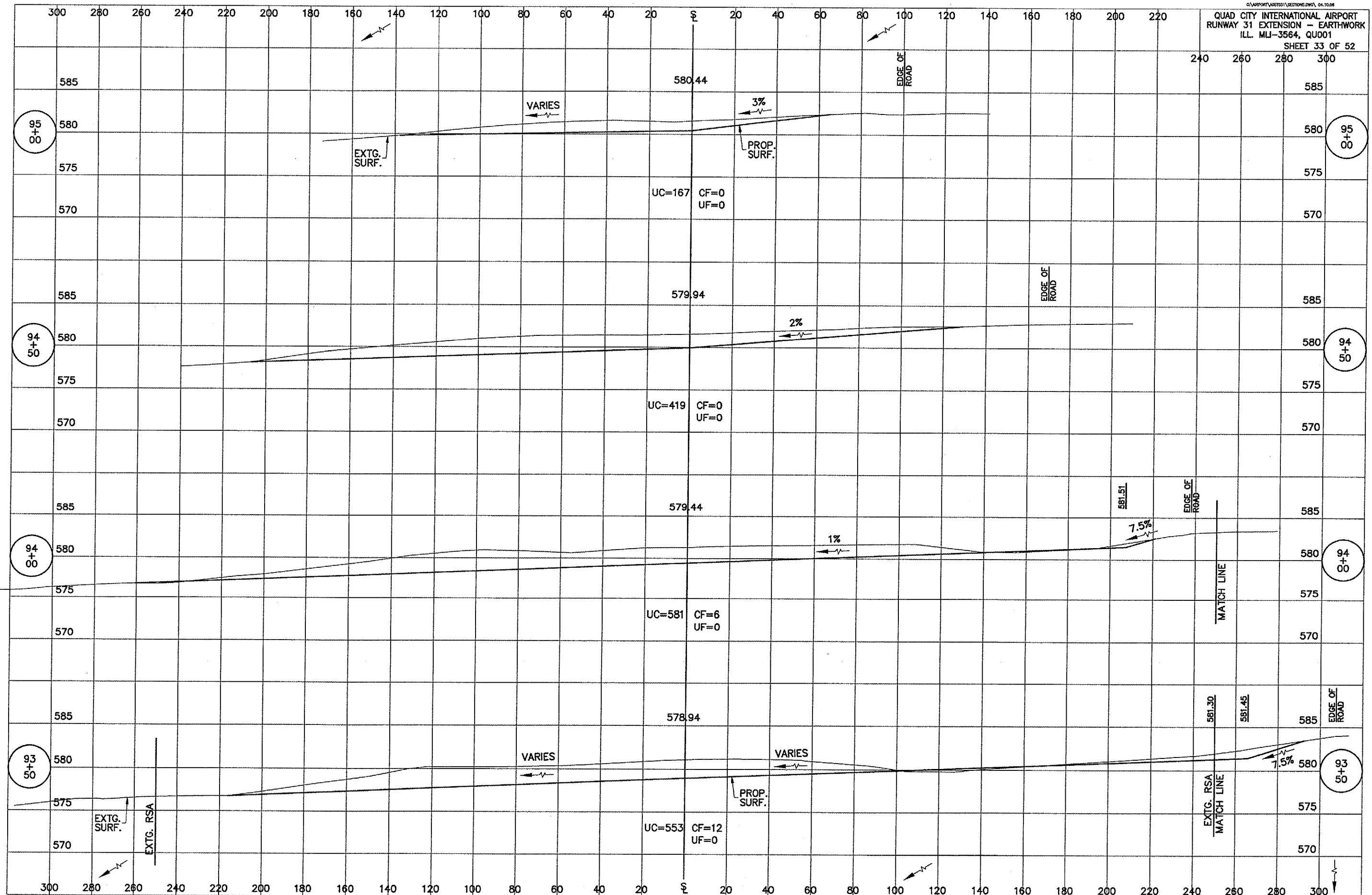


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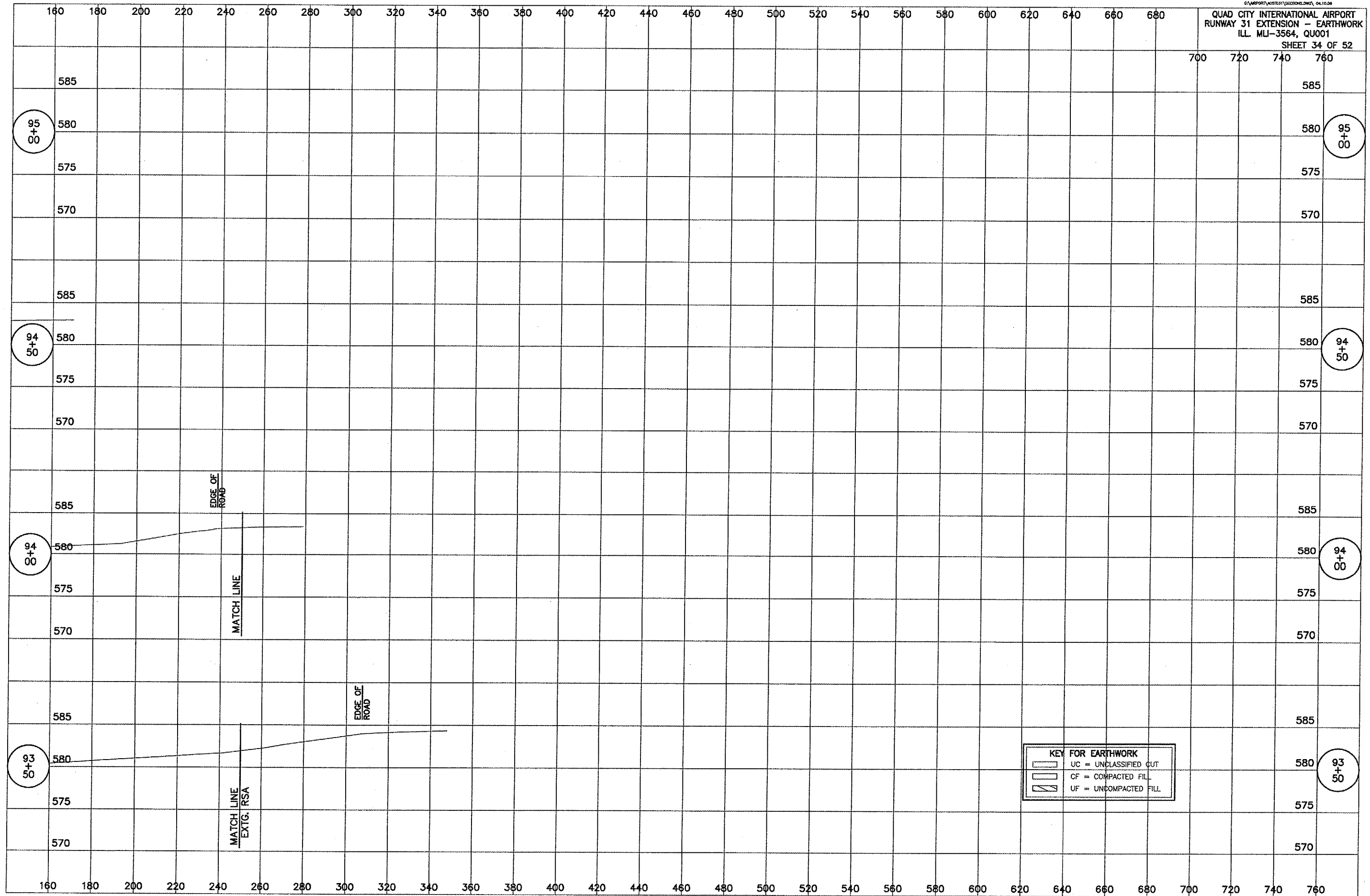


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RUNWAY 31 CROSS SECTIONS. STA 93+50 TO STA 95+00

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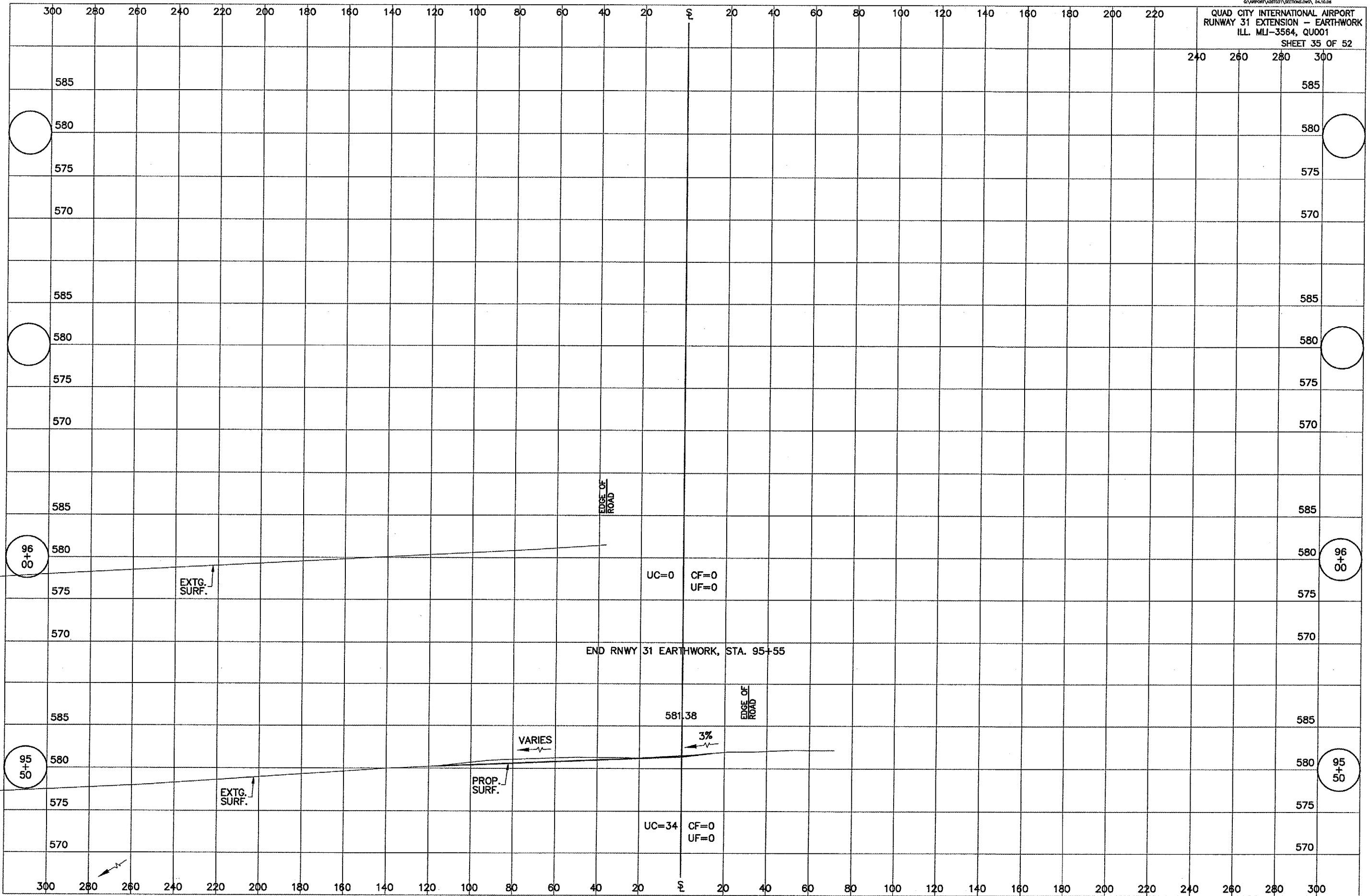


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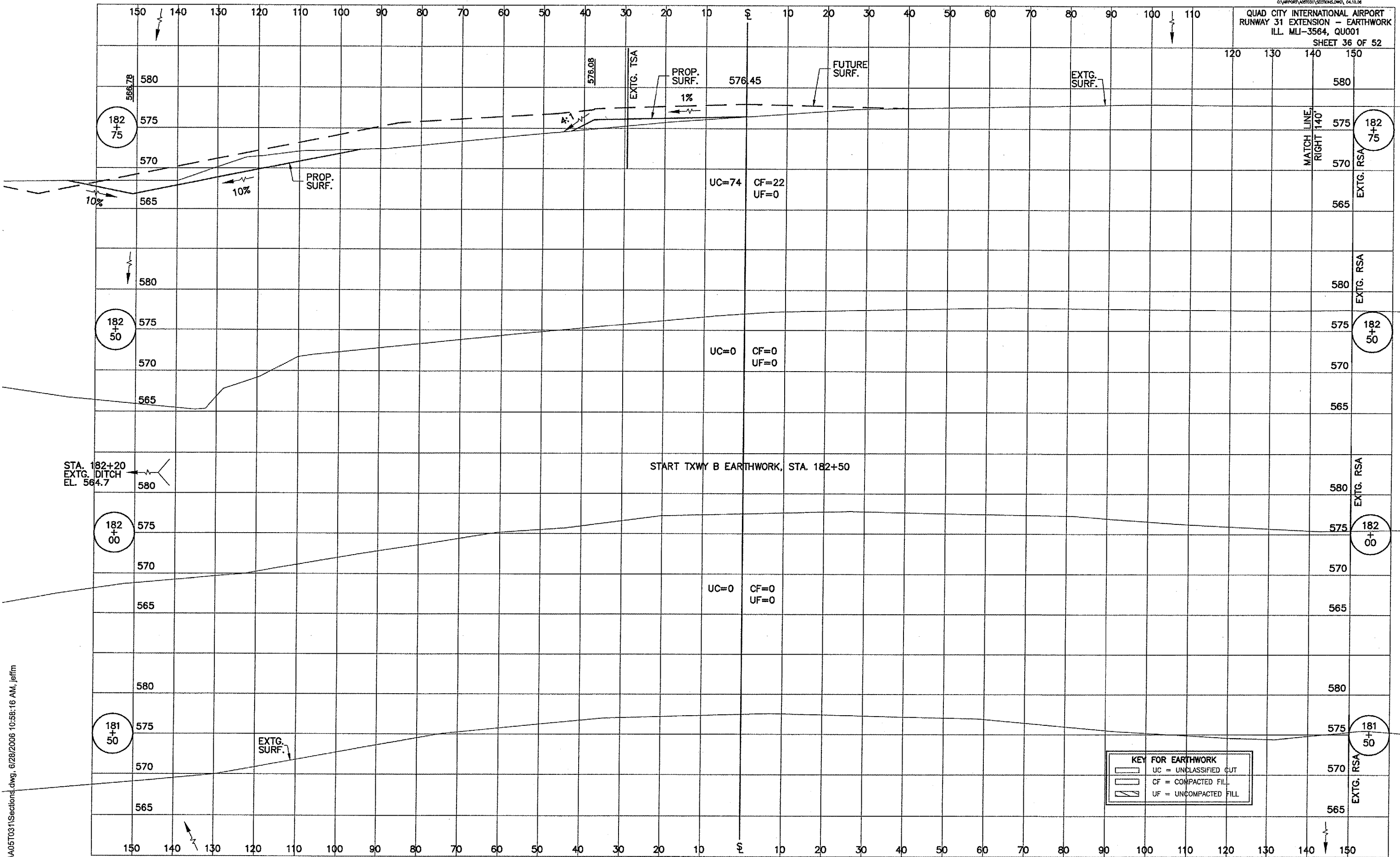
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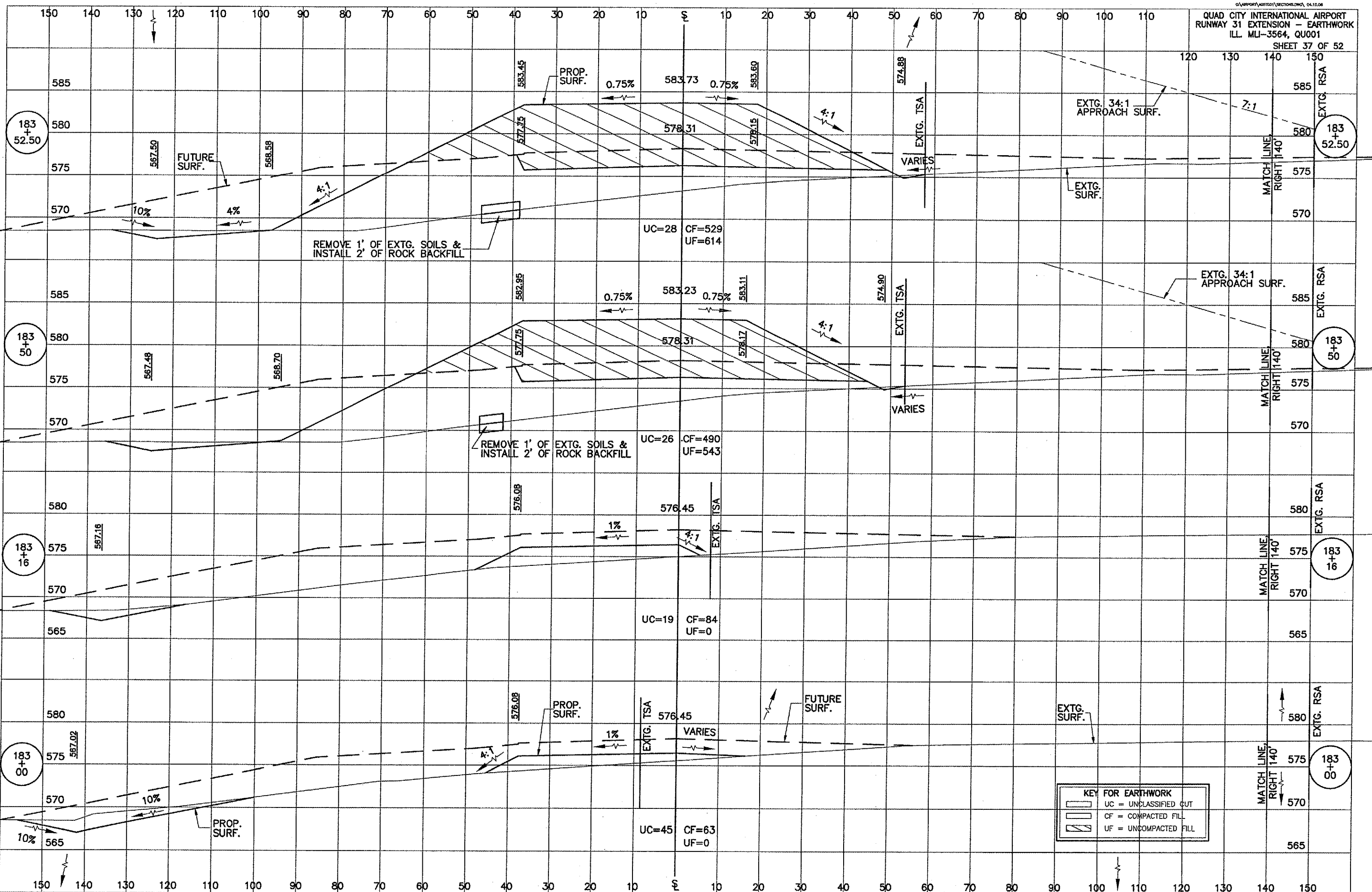
QUAD CITY INTERNATIONAL AIRPORT
RUNWAY 31 EXTENSION - EARTHWORK
ILL. MLJ-3584, QU001
SHEET 35 OF 52



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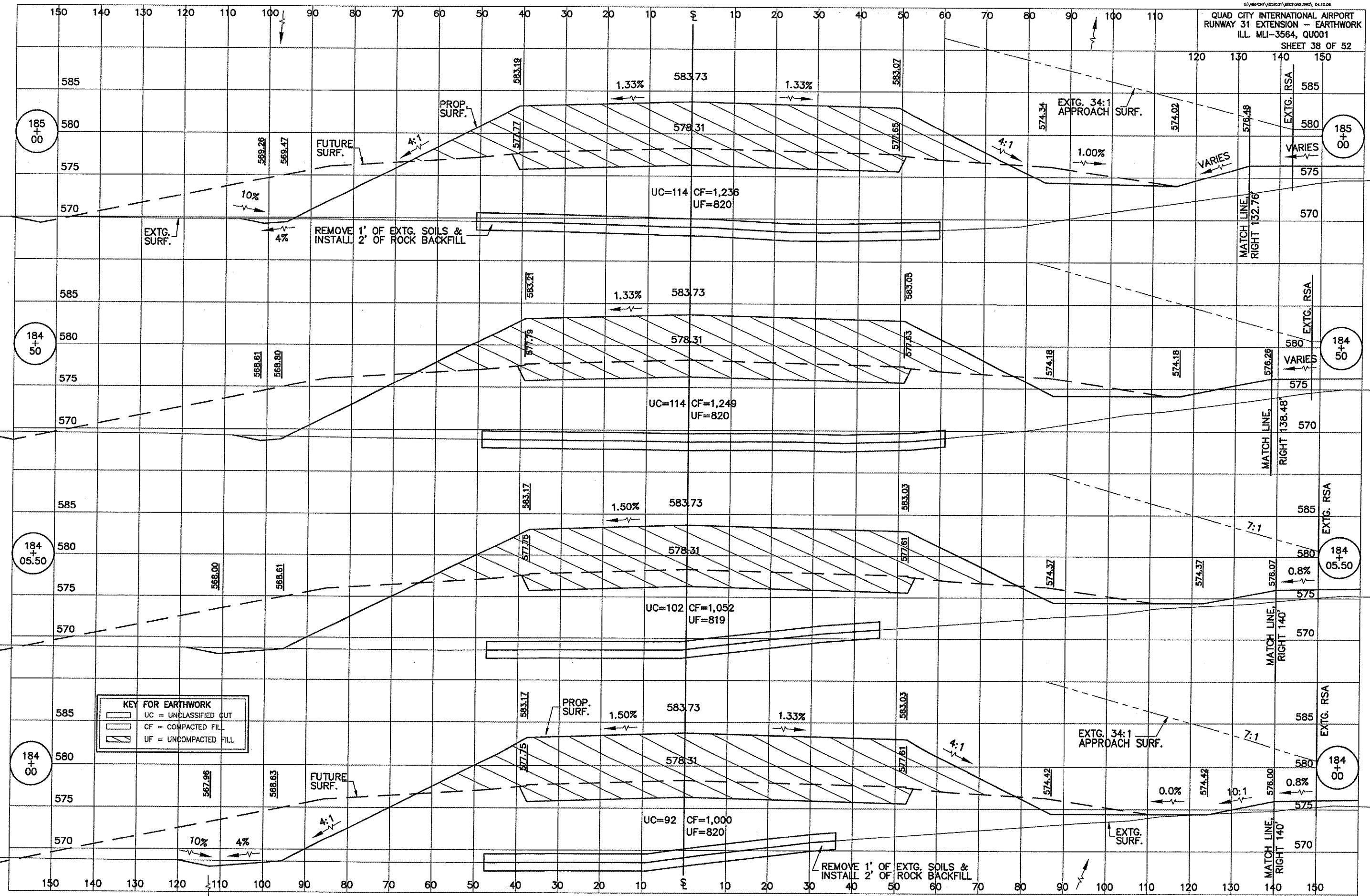


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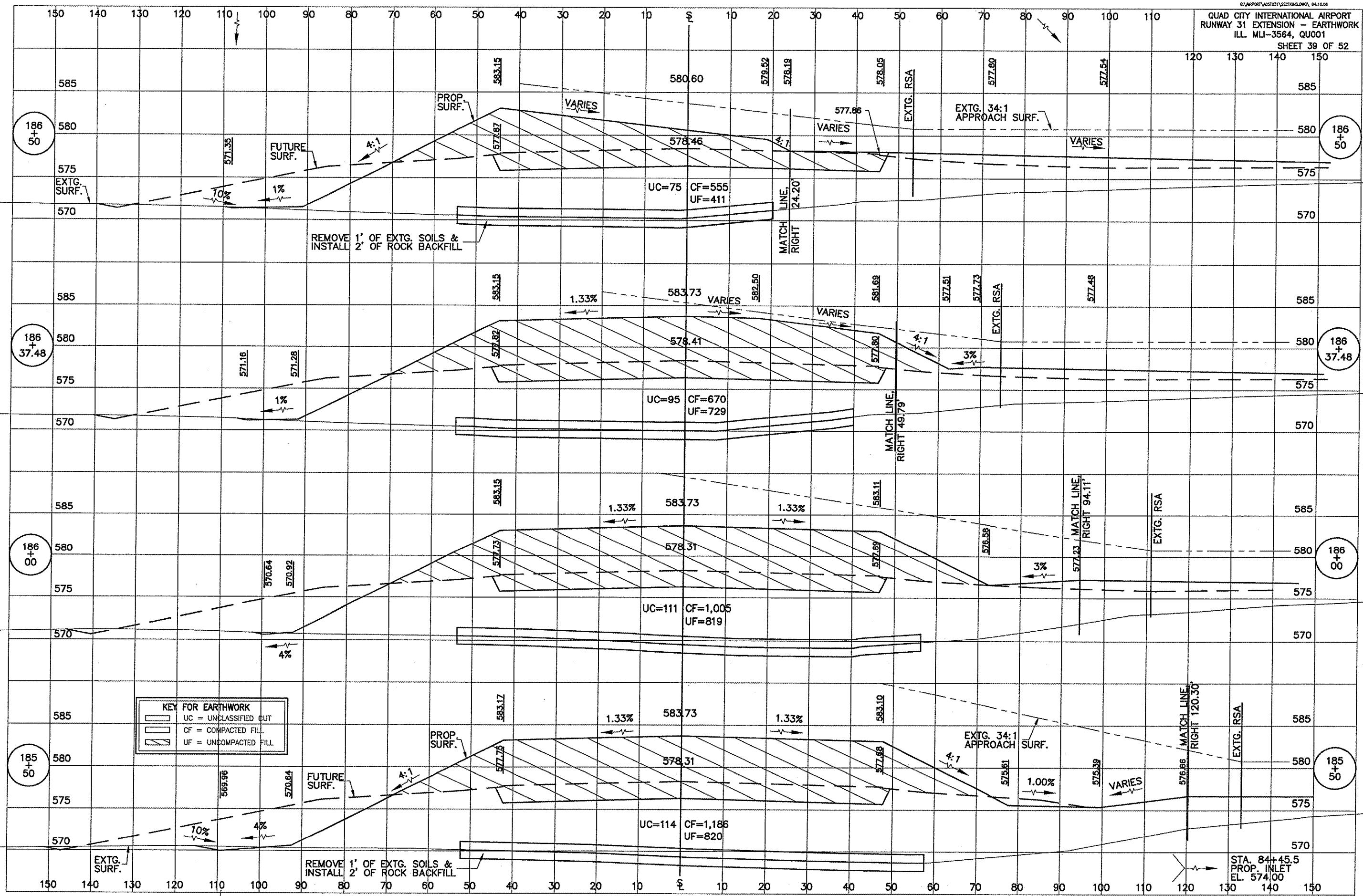
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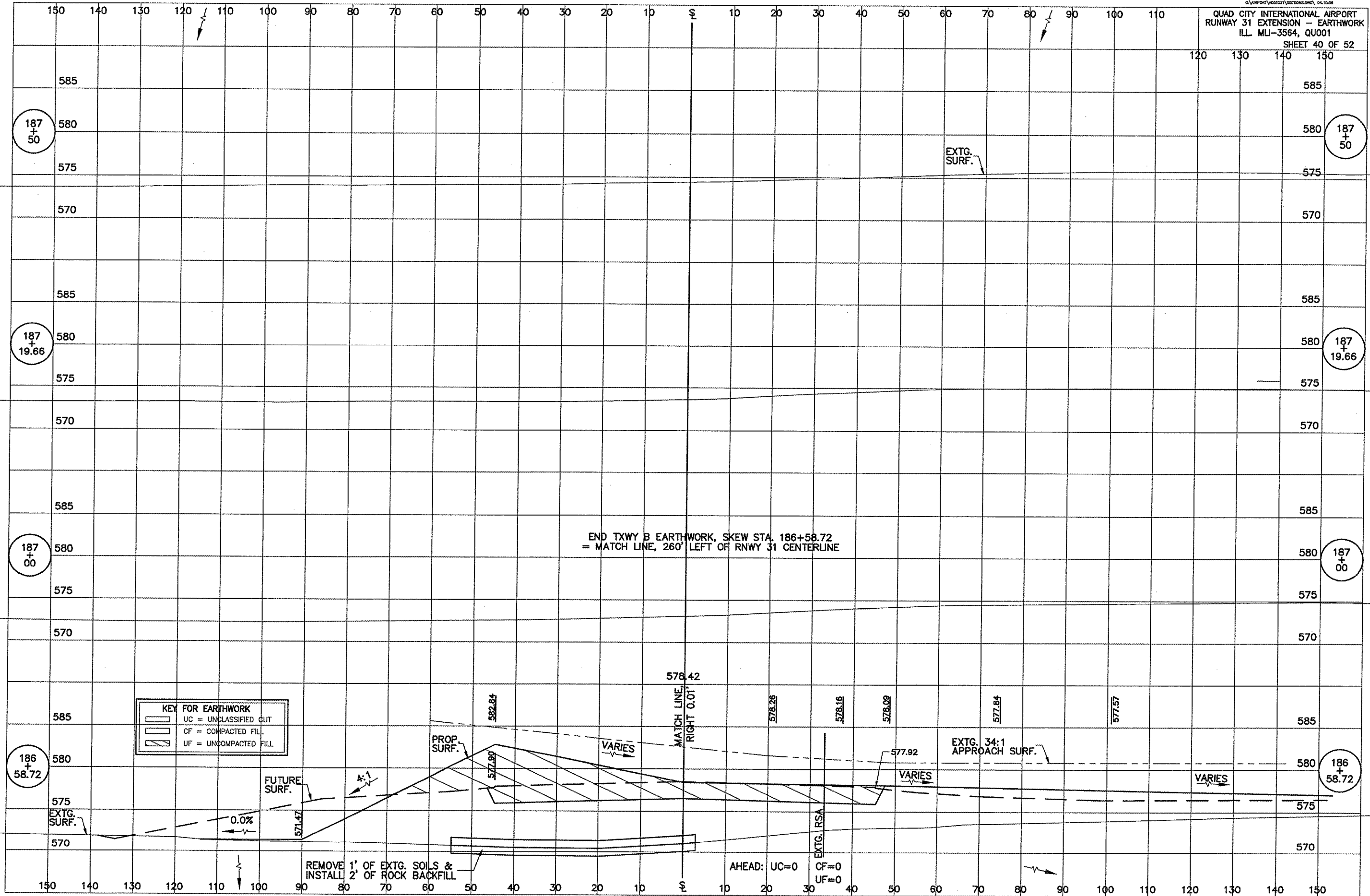


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STA. 84+45.5
 PROP. INLET
 EL. 574.00

TAXIWAY B CROSS SECTIONS, STA 185+50 TO STA 186+50

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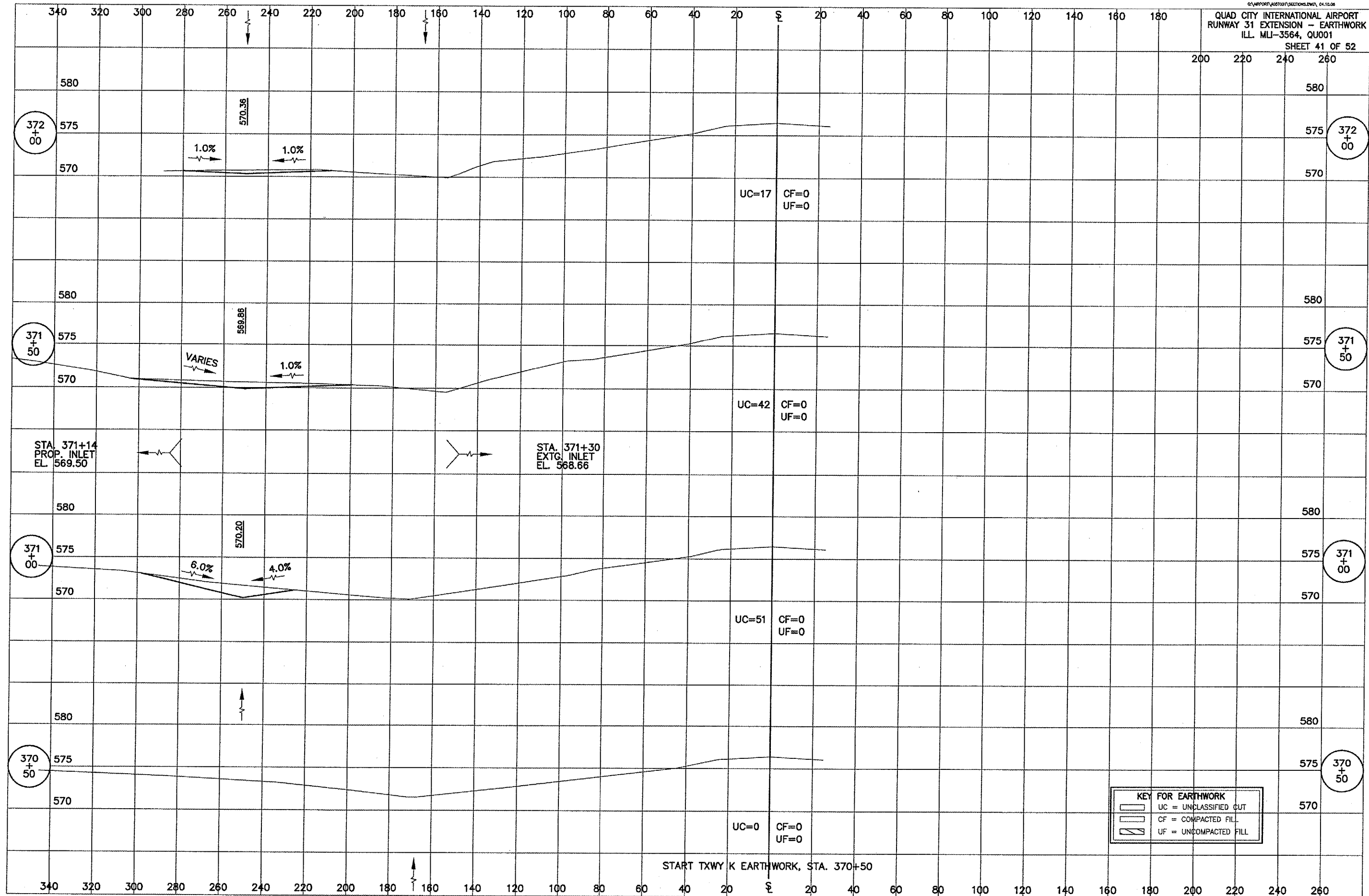


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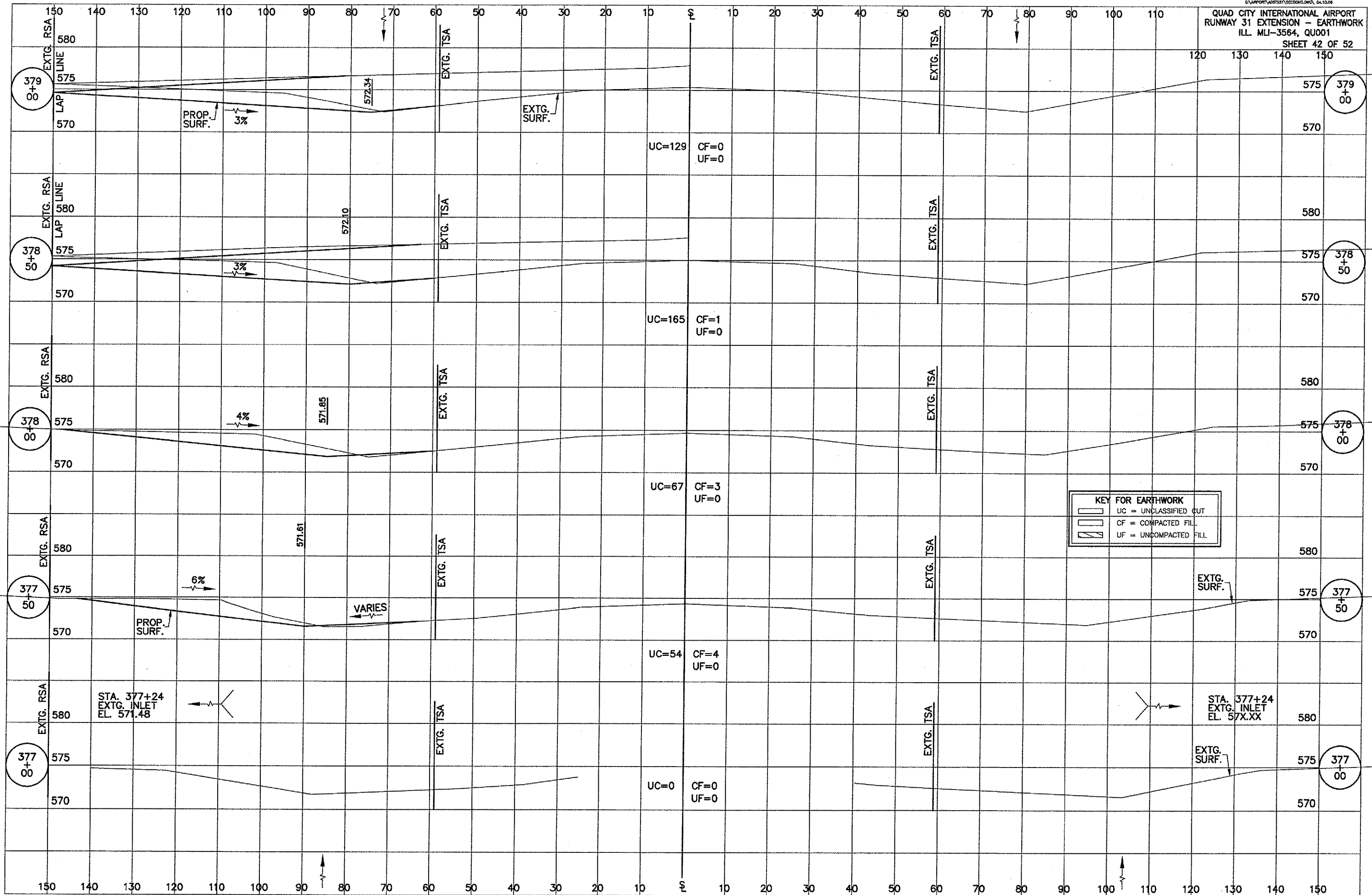
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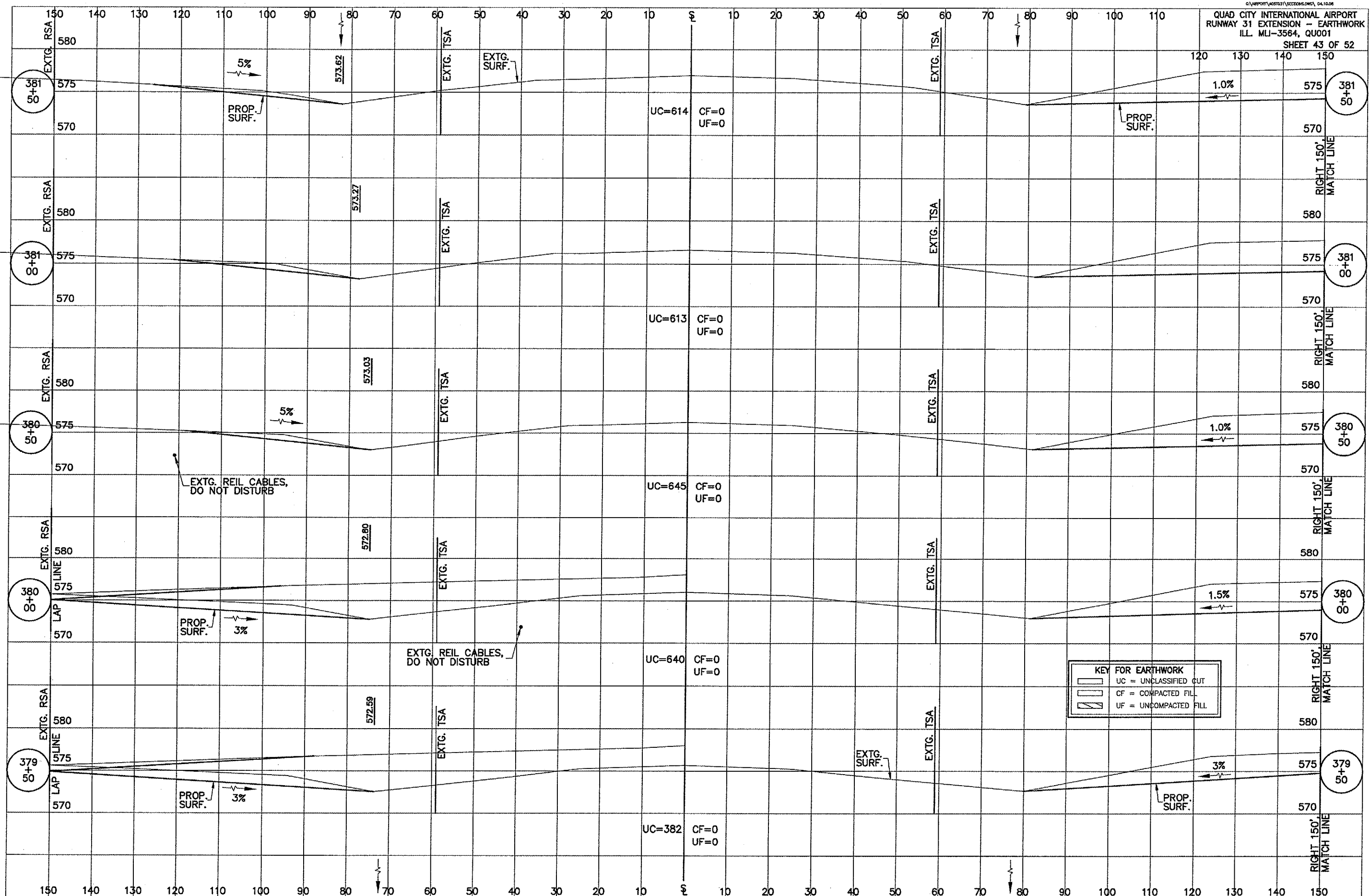
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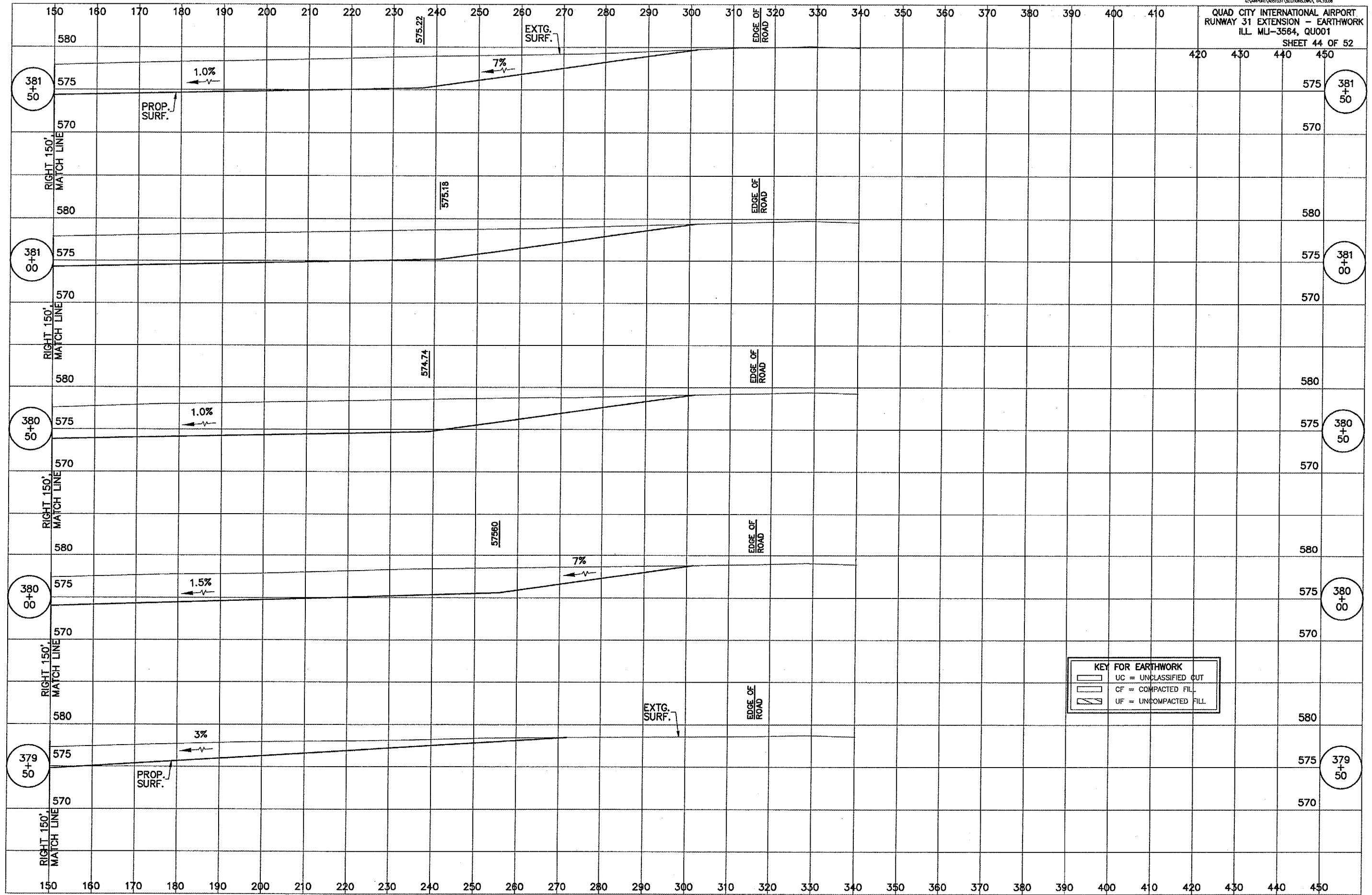
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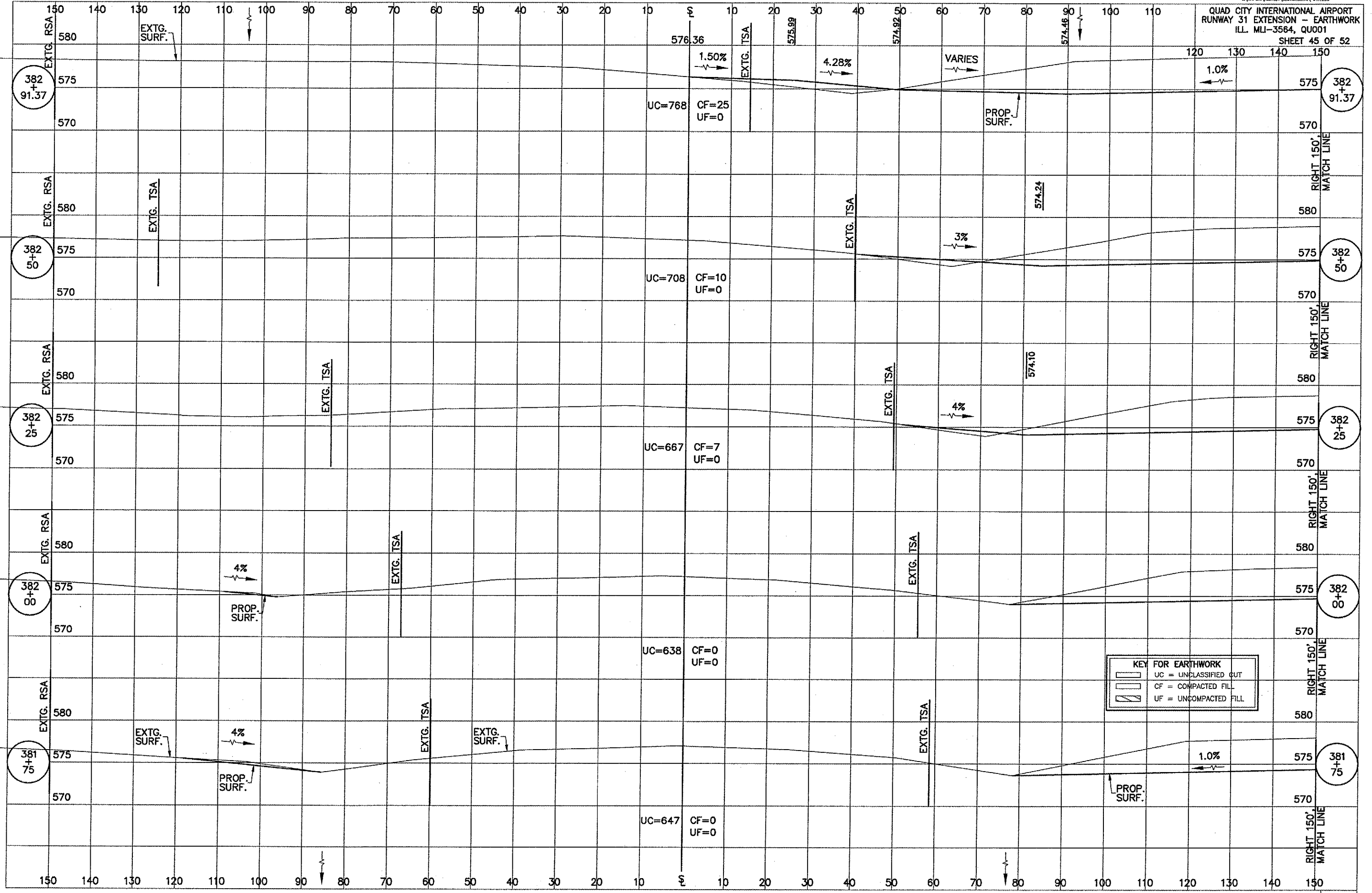
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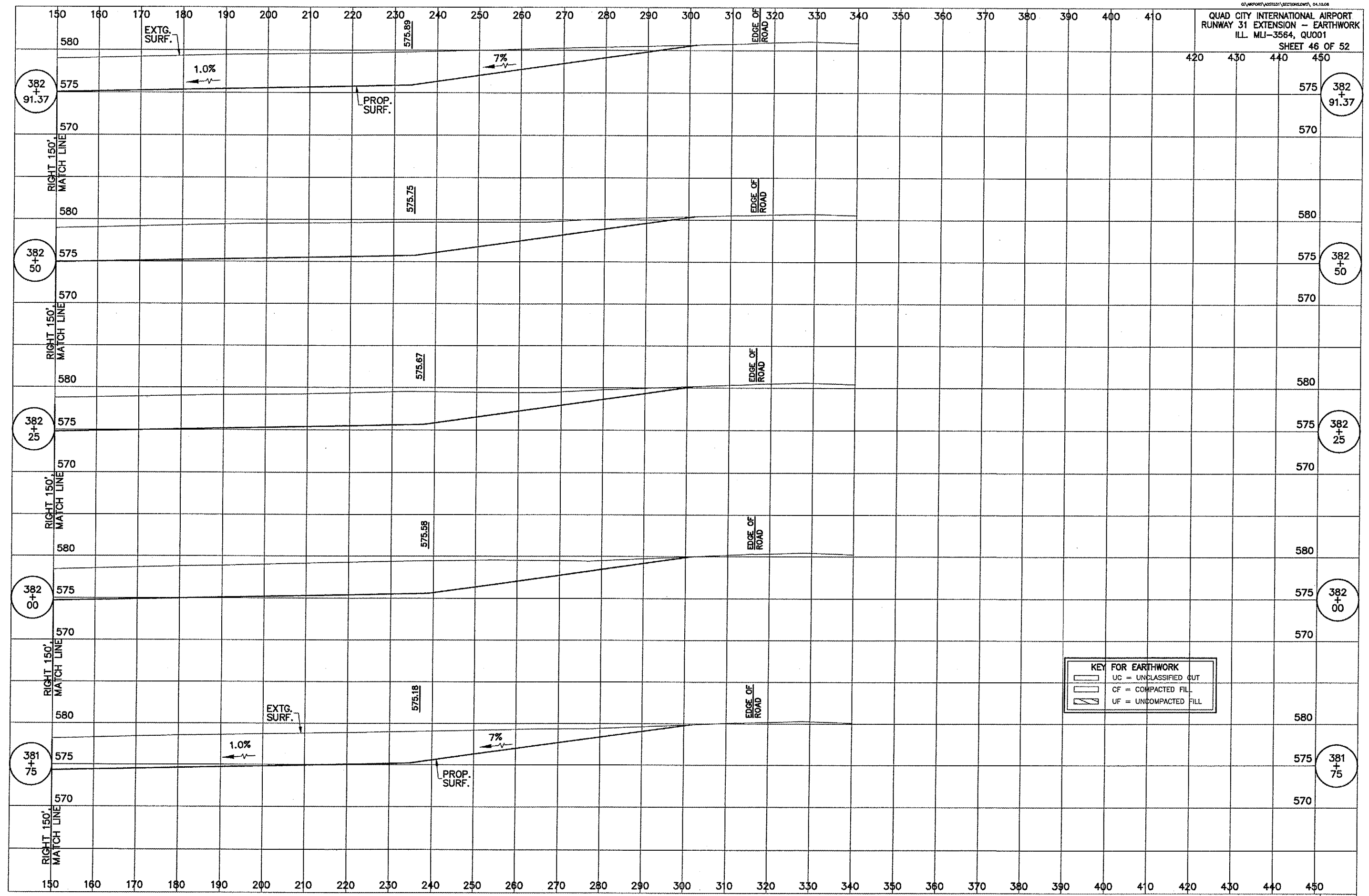
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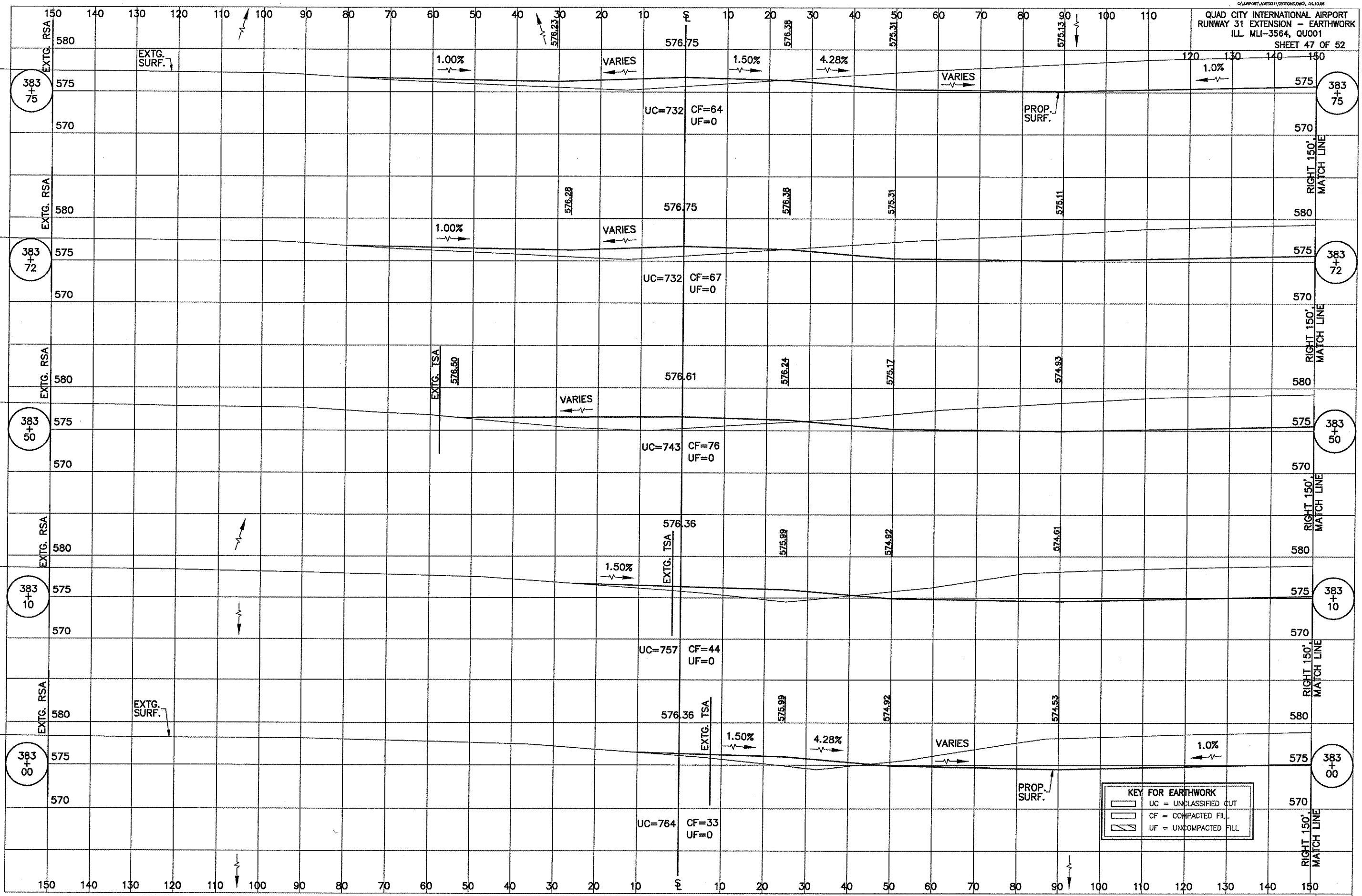
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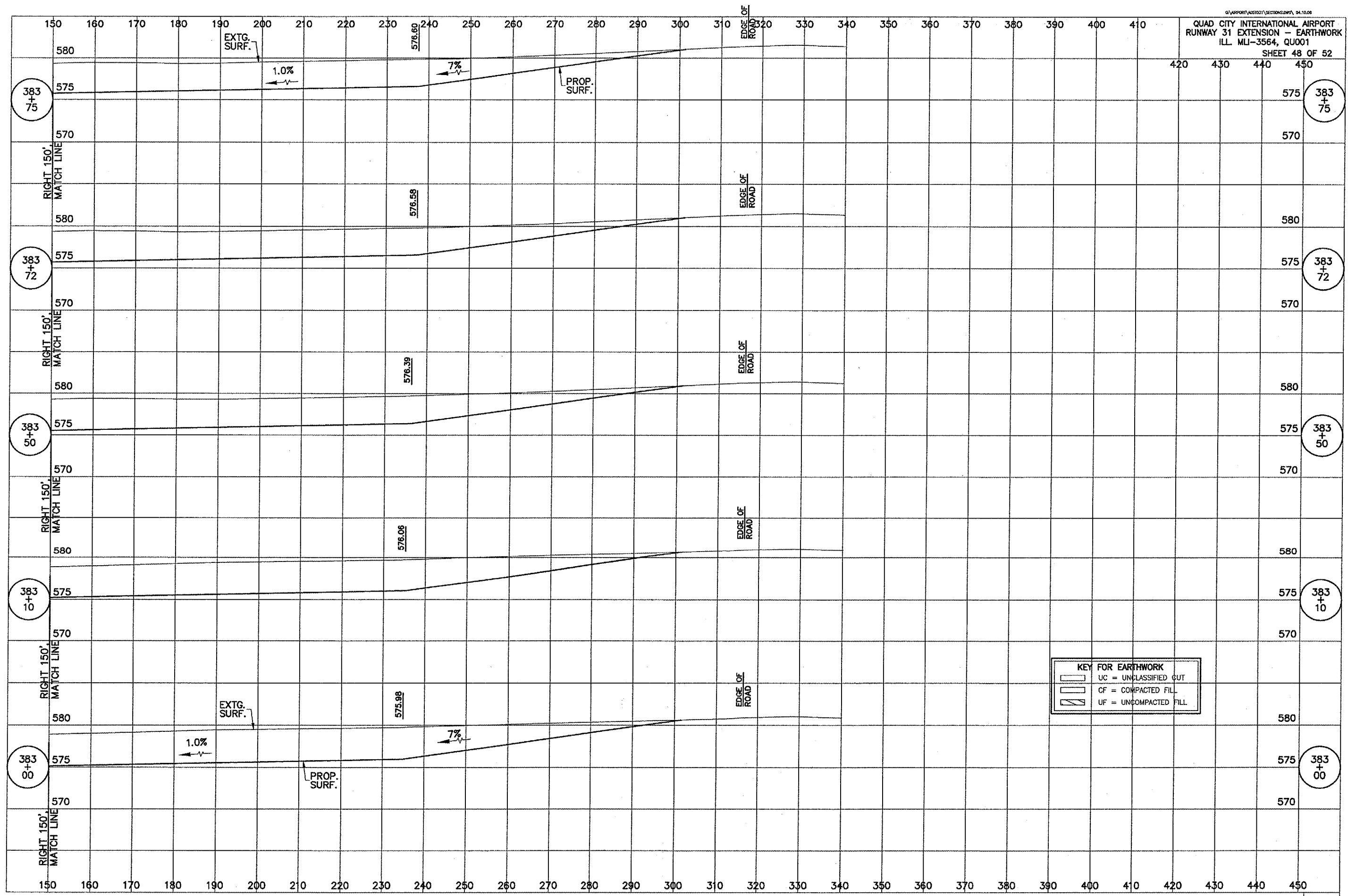


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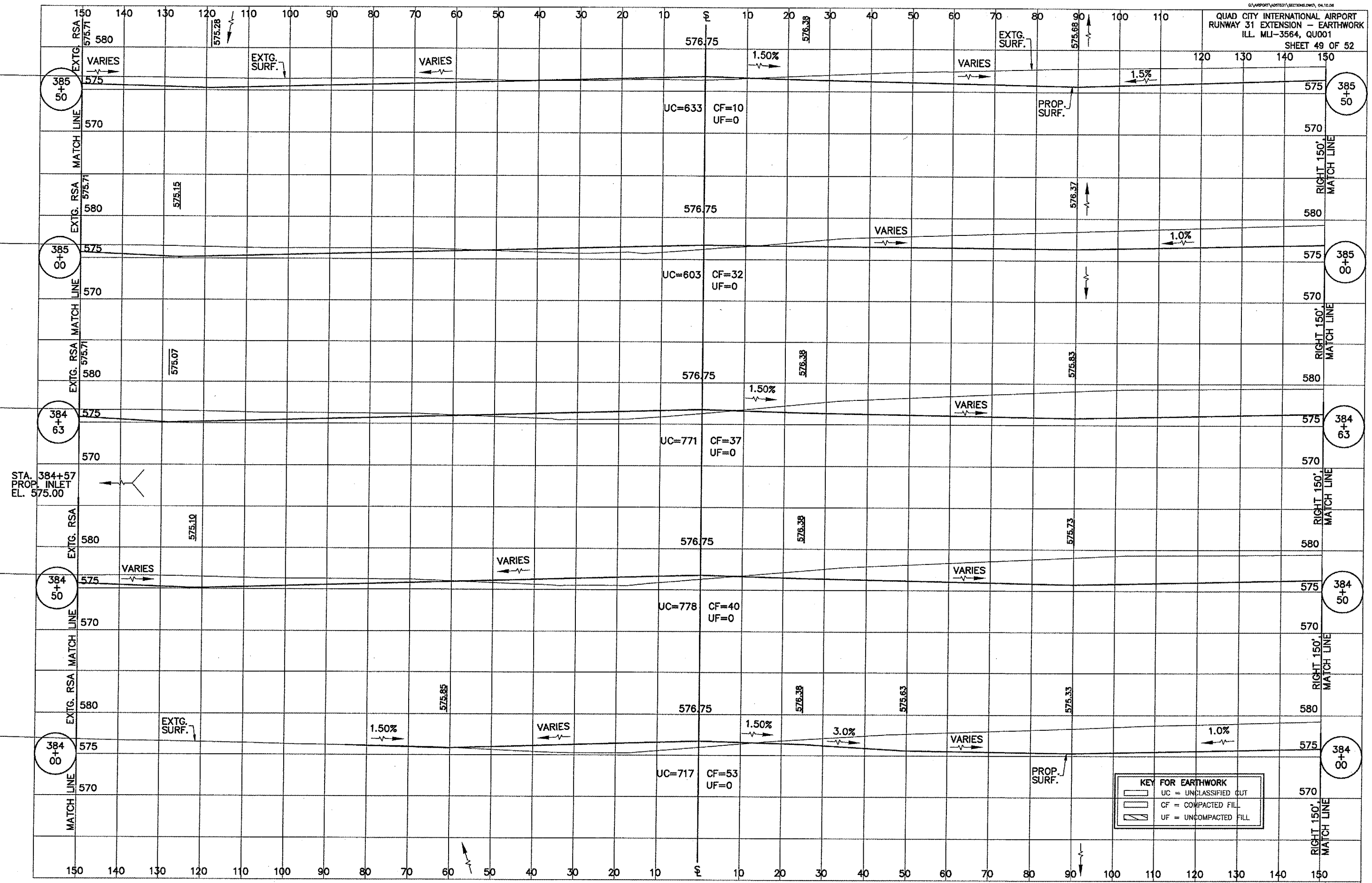
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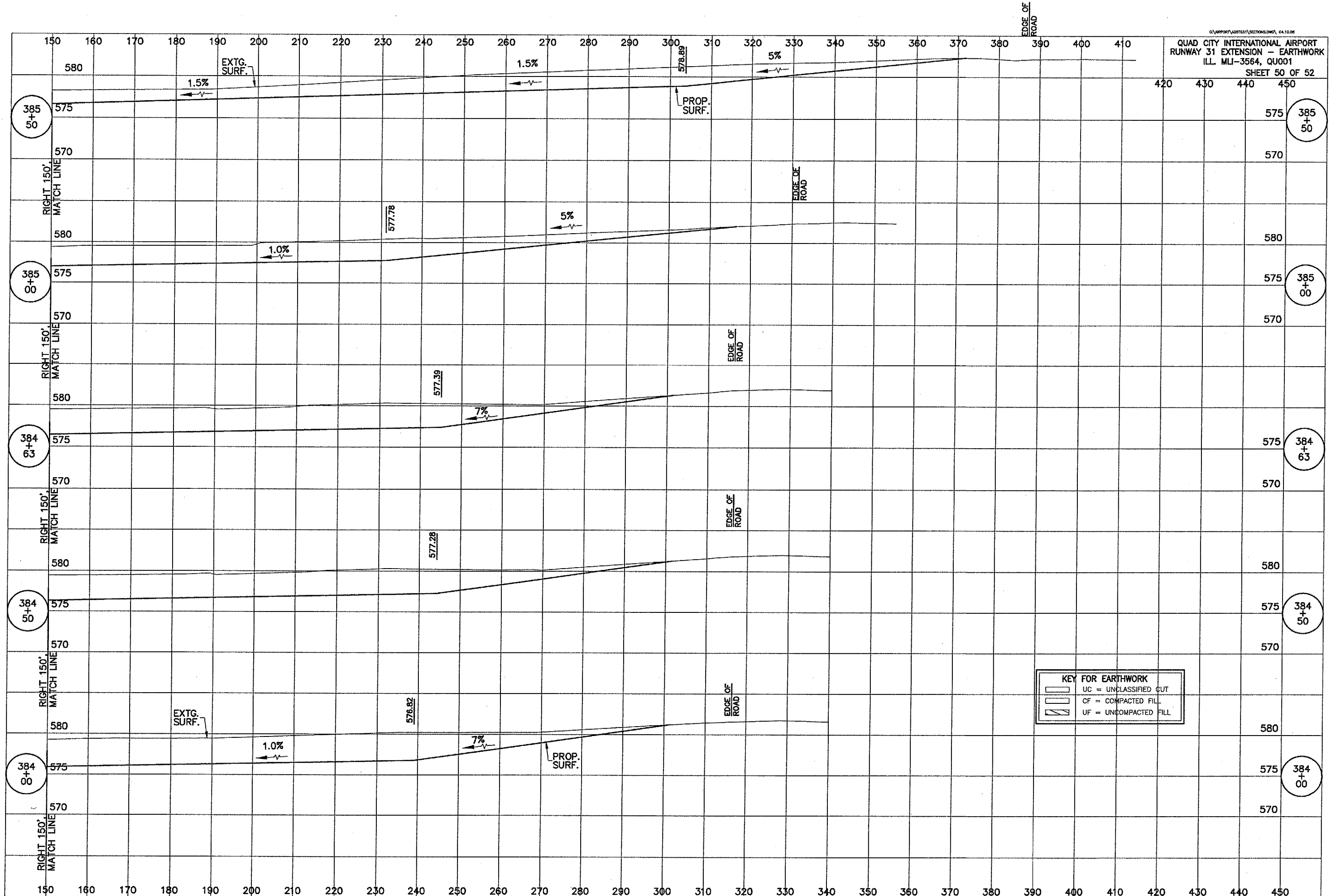
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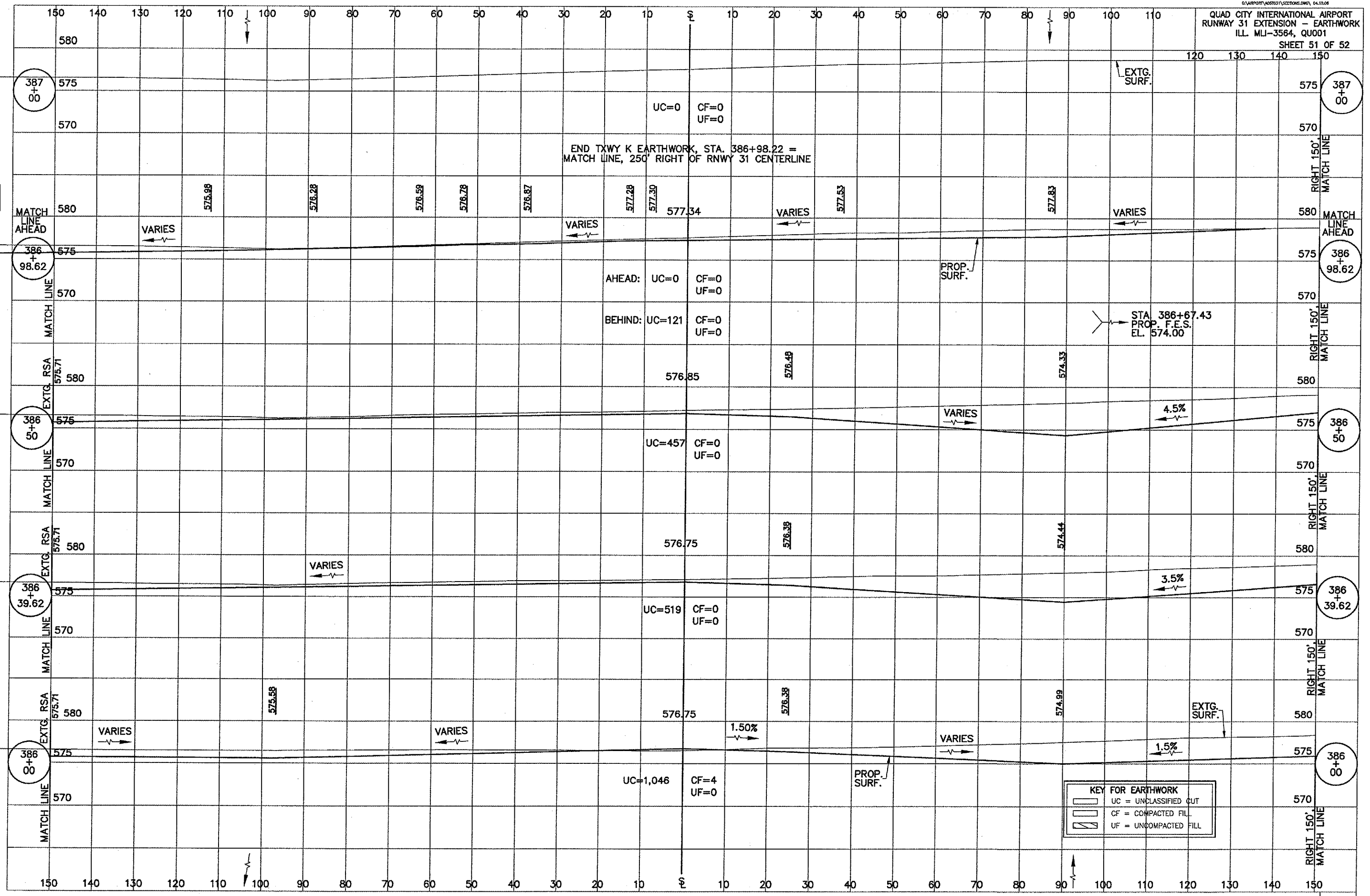
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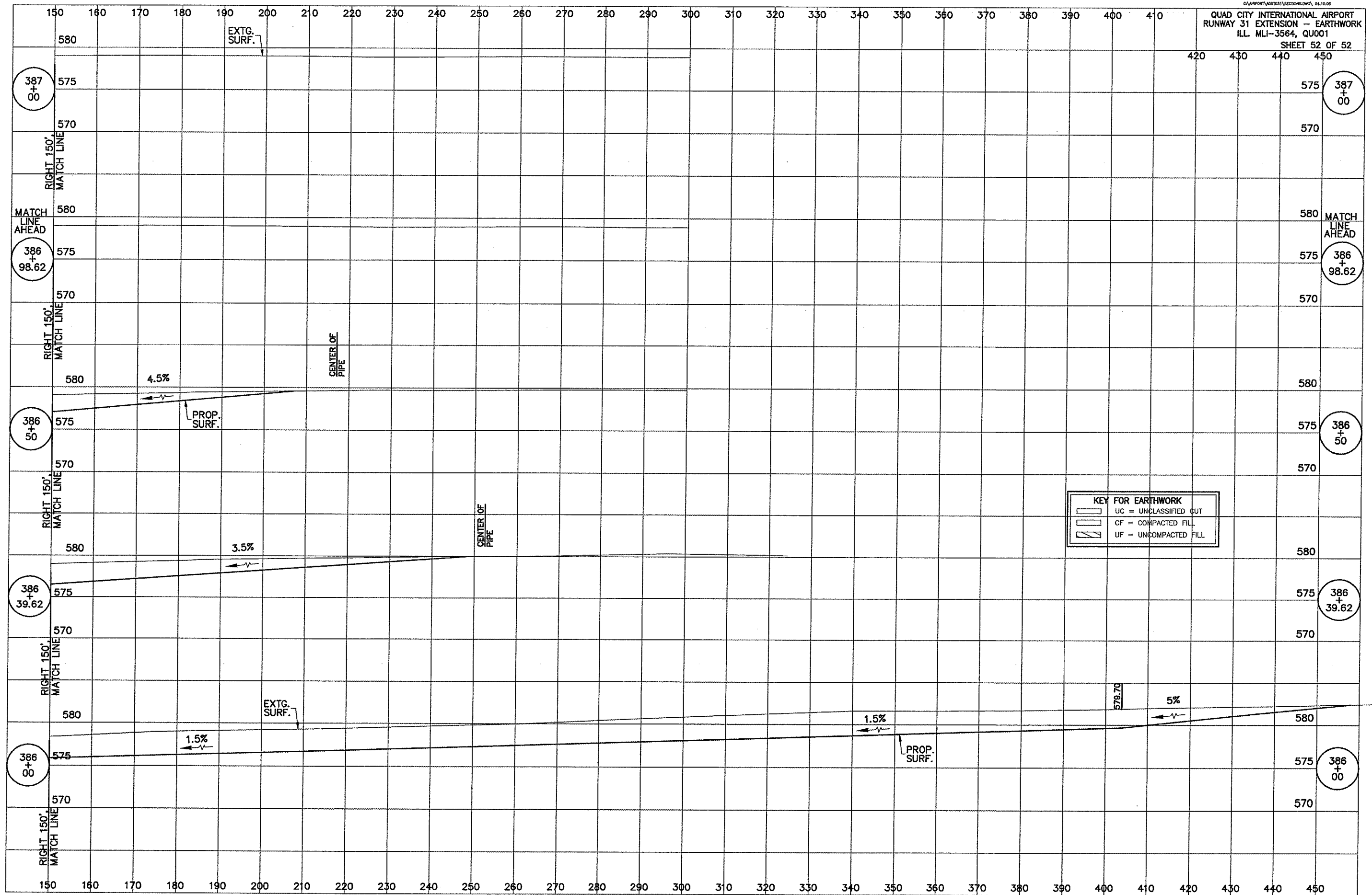
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TAXIWAY K CROSS SECTIONS, STA 386+00 TO STA 387+00 51/52

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