

CONSTRUCTION PLANS FOR GREATER PEORIA REGIONAL AIRPORT

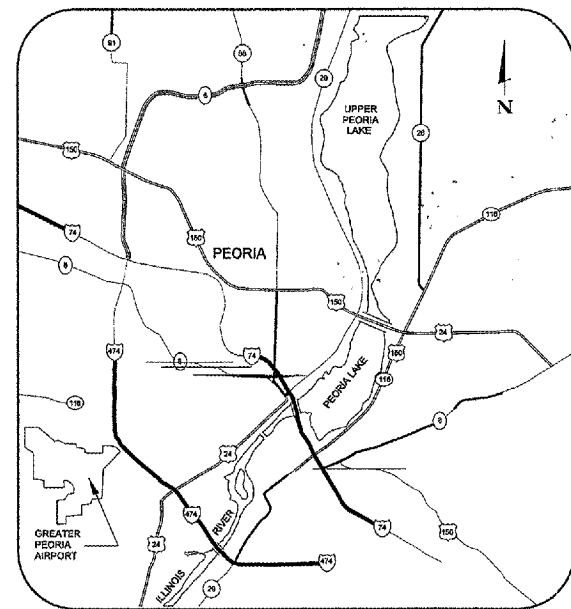
AIP PROJECT NO. 3-17-0080-XX
ILLINOIS PROJECT NO.: PIA-3343

RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION PHASE 1

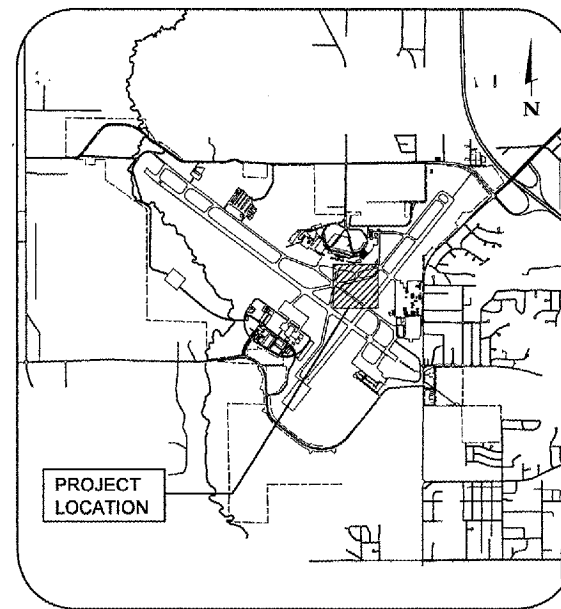
JUNE 24, 2005

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LOCATION MAP



SITE PLAN

DAVID T. NAUMAN
REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS
6/24/05
EXPIRES 11/30/05

MAXIMUM EQUIPMENT HEIGHT = 25'
GROUND FREQUENCY 121.85

CALL J.U.L.I.E.
BEFORE EXCAVATING
1-800-892-0123
GREATER PEORIA REGIONAL AIRPORT

TOWNSHIP: 8 NORTH
RANGE: 7 EAST
COUNTY: PEORIA
TOWNSHIP: LIMESTONE



PLOT 20
FILE: K:\PEORIA\0408103\dra\waheer\001_Cover_Sht.dgn

GREATER PEORIA AIRPORT AUTHORITY GREATER PEORIA REGIONAL AIRPORT PEORIA, ILLINOIS	CMT CRAWFORD, MURPHY & TILLY, INC. CONSULTING ENGINEERS ■ SPRINGFIELD, IL ■ AURORA, IL ■ PEORIA, IL ■ EDWARDSVILLE, IL ■ ST. LOUIS, MO ■ CHICAGO, IL ■ COLUMBUS, OH ■ INDIANAPOLIS, IN
APPROVED: <i>Polomen Balma</i> DATE: JUNE 24, 2005	SUBMITTED BY: <i>Polomen Balma</i> DATE: 6-23-05 CM&T JOB NUMBER 04081-03-00

PE083

REVISIONS

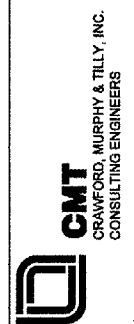
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34X22).
 PLOT 1

Item No.	Item Description	Unit	Quantity
AR107960	Relocate wind cone	EA	1
AR108158	1/C #8 5 KV UG Cable in UD	LF	8,655
AR108558	2/C #8 600 V Cable in UD	LF	713
AR108754	1/C #4 Ground	LF	713
AR110504	4-Way Concrete Encased Duct	LF	120
AR125415	MITL - Base Mounted	EA	56
AR125443	Taxi Guidance Sign, 3 Character	EA	3
AR125444	Taxi Guidance Sign, 4 Character	EA	5
AR125445	Taxi Guidance Sign, 5 Character	EA	1
AR125446	Taxi Guidance Sign, 6 Character	EA	1
AR125470	Modify Existing Sign Panel	EA	20
AR125902	Remove Base Mounted Light	EA	58
AR125904	Remove Taxi Guidance Sign	EA	6
AR125964	Relocate Taxi Guidance Sign	EA	1
AR152410	Unclassified Excavation	CY	22,200
AR155540	By-Product Lime	TON	300
AR155612	Soil Processing - 12"	SY	11,818
AR156510	Silt Fence	LF	3,000
AR156512	Bales	EA	150
AR156513	Separation Fabric	SY	4,749
AR201610	Bituminous Base Course	TON	7,870
AR201630	Bituminous Base Test Section	EA	1
AR209608	Crushed Agg. Base Course - 8"	SY	4,887
AR209617	Crushed Agg. Base Course - 17"	SY	11,818
AR401610	Bituminous Surface Course	TON	2,146
AR401630	Bituminous Surface Test Section	EA	1
AR401900	Remove Bituminous Pavement	SY	7,123
AR501518	18" PCC Pavement	SY	4,595
AR501530	PCC Test Batch	EA	1
AR501900	Remove PCC Pavement	SY	392
AR602510	Bituminous Prime Coat	GAL	4,116
AR603510	Bituminous Tack Coat	GAL	1,764
AR620510	Pavement Marking	SF	6,400
AR620900	Pavement Marking Removal	SF	1,275
AR701518	18" RCP, Class IV	LF	389
AR705504	4" Perforated Underdrain	LF	246
AR705635	Underdrain Collection Structures	EA	1
AR705640	Underdrain Cleanout	EA	1
AR751410	Inlet	EA	2
AR751530	Manhole	EA	1
AR751900	Remove Inlet	EA	1
AR751940	Adjust Inlet	EA	1
AR901510	Seeding	AC	8
AR908510	Mulching	AC	8
AR801953	Bituminous Edge Removal	LF	651



RELOCATE TAXIWAY A FROM
 MID-FIELD INTERSECTION



IL PROJECT: PIA-3343
 AIP PROJ: 3-17-0080-XX

FILE: K:\Pearls\0406103\draw\Sheets\002_Quantities.d

DESIGN BY: TCS

DRAWN BY: CAG

CHECKED BY: TCS

APPROVED BY: CET

DATE: JULY 8, 2005

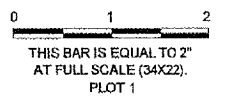
JOB No: 04061-03

SUMMARY OF
 QUANTITIES

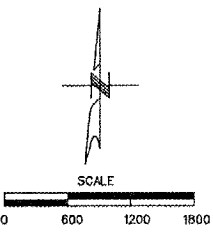
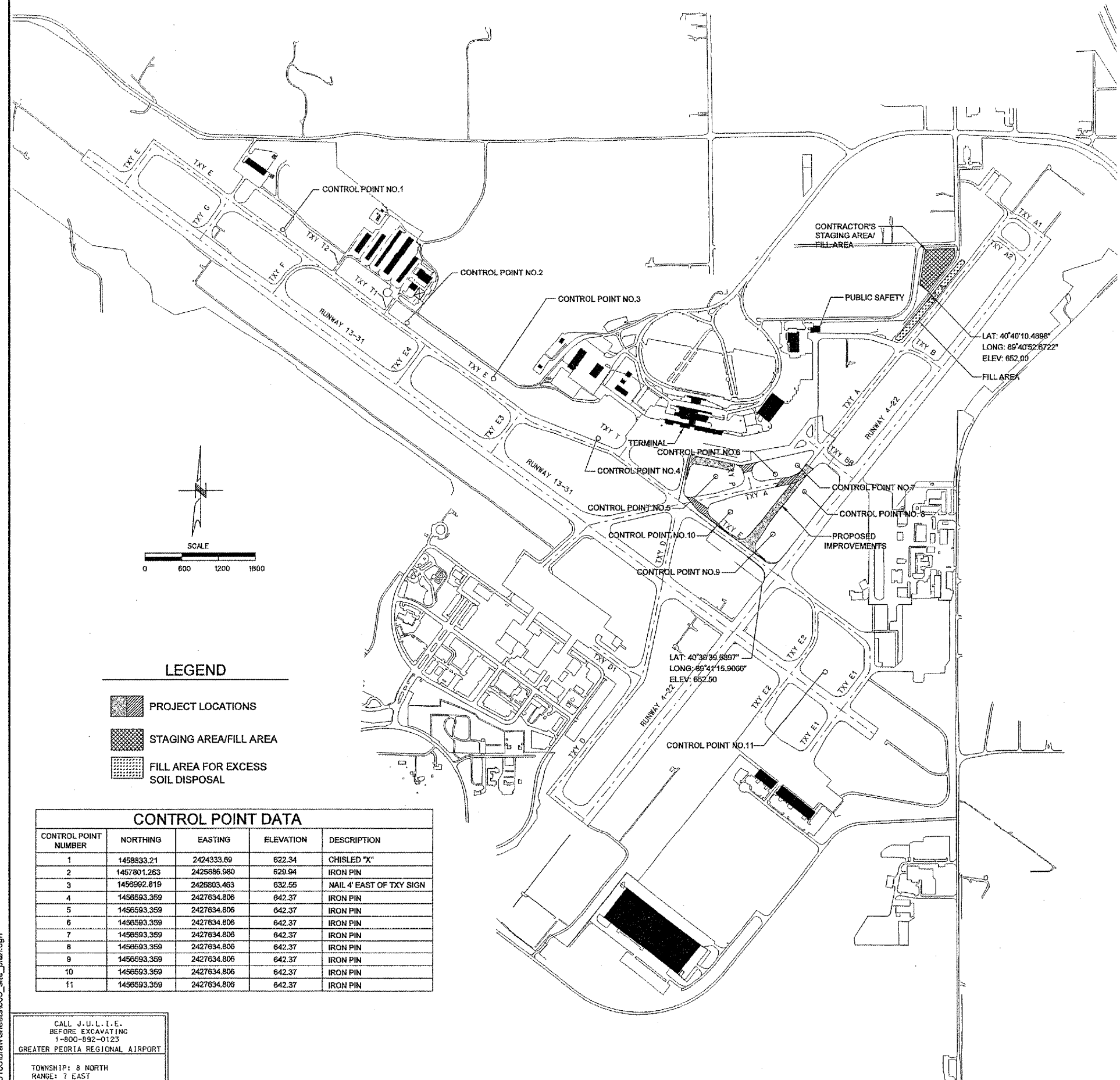
SHEET 2 OF 43 SHEETS

GENERAL NOTES

NUMBER	BY	DATE



- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE CONSTRUCTION ACTIVITY PLAN.
- ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND FLAGMEN. ALL VEHICLES SHALL HAVE A COMPANY-LOGO OR ID ON EACH SIDE OF THE VEHICLE, HAVE A YELLOW FLASHING LIGHT ON TOP OF THE VEHICLE, AND HAVE AN ORANGE AND WHITE AVIATION FLAG MOUNTED AT THE HIGHEST POINT ON THE VEHICLE.
- REFER TO THE CONSTRUCTION ACTIVITY PLAN AND THE SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS CONCERNING COORDINATION OF CONSTRUCTION ACTIVITIES.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT DIRECTOR.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN.
- BROKEN CONCRETE, BROKEN ASPHALT & OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN AREAS 115' OF AN ACTIVE TAXIWAY CENTERLINE AND 200' OF AN ACTIVE RUNWAY CENTERLINE.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE AIRPORT DIRECTOR. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE AIRPORT DIRECTOR.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF THE AIRPORT SECURITY OFFICE. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED.
- THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.
IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH UTILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES AND F.A.A. UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY AND F.A.A. OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, F.A.A., THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER. ANY LOCATING EQUIPMENT AND/OR LABOR NECESSARY FOR LOCATING UTILITIES INSIDE THE AIRFIELD FENCE SHALL BE PROVIDED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - CONTRACTOR ACCESS STAGING AND STORAGE AREAS SHALL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL PUBLIC ACCESS ROADS WITH THE APPROPRIATE AGENCY RESPONSIBLE FOR THE ROADWAYS.
 - CONTRACTOR'S PERSONNEL SHALL PARK IN THE CONTRACTOR'S STAGING AREA. APPROVED ESCORTS MAY ESCORT NON-BADGED INDIVIDUALS OR VEHICLES OPERATED BY NON-BADGED INDIVIDUALS.
 - ESCORTS SHALL REMAIN IN DIRECT CONTROL OF INDIVIDUALS/VEHICLES THEY ARE ESCORTING.
 - SECURITY GUARD/CONTRACTOR SHALL PROVIDE WRITTEN DOCUMENTATION FOR ALL INDIVIDUALS AND VEHICLES ESCORTED. THIS SHALL INCLUDE NAME, SSN, LICENSE PLATE, TIME IN AND TIME OUT.
 - DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
 - WHEN THE CONTRACTOR IS NOT WORKING, UNLESS OTHERWISE NOTED, EACH GATE LOCATION MUST PROVIDE BOTH SECURITY AND ACCESS. THIS MAY BE ACCOMPLISHED BY THE USE OF THE EXISTING GATE, A COMPETENT SECURITY GUARD, A MANUALLY OPERATED GATE WITH PADLOCK AND KEYS GIVEN TO INDIVIDUALS WITH ACCESS, OR ANY OTHER METHOD APPROVED BY THE AIRPORT.
 - ANY UNSATISFACTORY SECURITY GUARD USED BY THE CONTRACTOR SHALL BE REPLACED BY THE CONTRACTOR IF SO DIRECTED BY THE AIRPORT.
 - A LIST OF AUTHORIZED PERSONNEL PERMITTED TO USE THE ACCESS GATES SHALL BE PROVIDED BY THE CONTRACTOR TO THE AIRPORT SECURITY OFFICE. THE CONTRACTOR'S SECURITY GUARD SHALL USE THIS LIST TO ADMIT INDIVIDUALS AT THE SECURITY GATE. THE CONTRACTOR MUST ALSO MAINTAIN A STOP LIST WITH INDIVIDUALS THAT NO LONGER HAVE ACCESS TO THE AIRFIELD FOR USE BY THE SECURITY GUARD AND THE AIRPORT SECURITY OFFICE.
 - THE CONTRACTOR SHALL VERIFY THAT THE GATE IS CLOSED AND LOCKED UPON LEAVING THE SITE. A KEY SHALL BE PROVIDED TO THE AIRPORT SECURITY OFFICE.
 - THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGE TO THE ACCESS RD., ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE OWNER.
 - ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS TO NOT PENETRATE THE PART 77 SURFACES.
 - THE CONTRACTOR SHALL STORE ALL EQUIPMENT AND MATERIALS IN THE STAGING AREA SHOWN ON CONSTRUCTION ACTIVITY PLAN DRAWINGS. ALL EQUIPMENT AND MATERIALS SHALL BE RETURNED TO THE STAGING AREA AT THE END OF EACH WORKING DAY.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL FEES ASSOCIATED WITH BADGING OF PERSONNEL AND THE ASSOCIATED CRIMINAL HISTORY RECORD CHECK



LEGEND

- PROJECT LOCATIONS
- STAGING AREA/FILL AREA
- FILL AREA FOR EXCESS SOIL DISPOSAL

CONTROL POINT DATA

CONTROL POINT NUMBER	NORTHING	EASTING	ELEVATION	DESCRIPTION
1	1458833.21	2424333.69	622.34	CHISLED "X"
2	1457801.263	2425686.980	629.94	IRON PIN
3	1458992.819	2426803.463	632.55	NAIL 4' EAST OF TXY SIGN
4	1458593.359	2427634.806	642.37	IRON PIN
5	1458593.359	2427634.806	642.37	IRON PIN
6	1458593.359	2427634.806	642.37	IRON PIN
7	1458593.359	2427634.806	642.37	IRON PIN
8	1458593.359	2427634.806	642.37	IRON PIN
9	1458593.359	2427634.806	642.37	IRON PIN
10	1458593.359	2427634.806	642.37	IRON PIN
11	1458593.359	2427634.806	642.37	IRON PIN

CALL J.U.L.I.E.
BEFORE EXCAVATING
1-800-892-0123
GREATER PEORIA REGIONAL AIRPORT
TOWNSHIP: 8 NORTH
RANGE: 7 EAST
COUNTY: PEORIA
TOWNSHIP: LIMESTONE
IMMEDIATELY WEST OF INTERSECTION
OF AIRPORT AND SMITHVILLE
ROAD



RELOCATE TAXIWAY A FROM
MID-FIELD INTERSECTION



IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\draw\sheets\003_site_plan.d
DESIGN BY: TCS
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET
DATE: JUNE 24, 2005
JOB No: 04061-03

SITE PLAN

06/23/2005 k:\peoria\0406103\draw\sheets\003_site_plan.dgn







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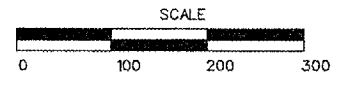
REVISIONS

NUMBER	BY	DATE

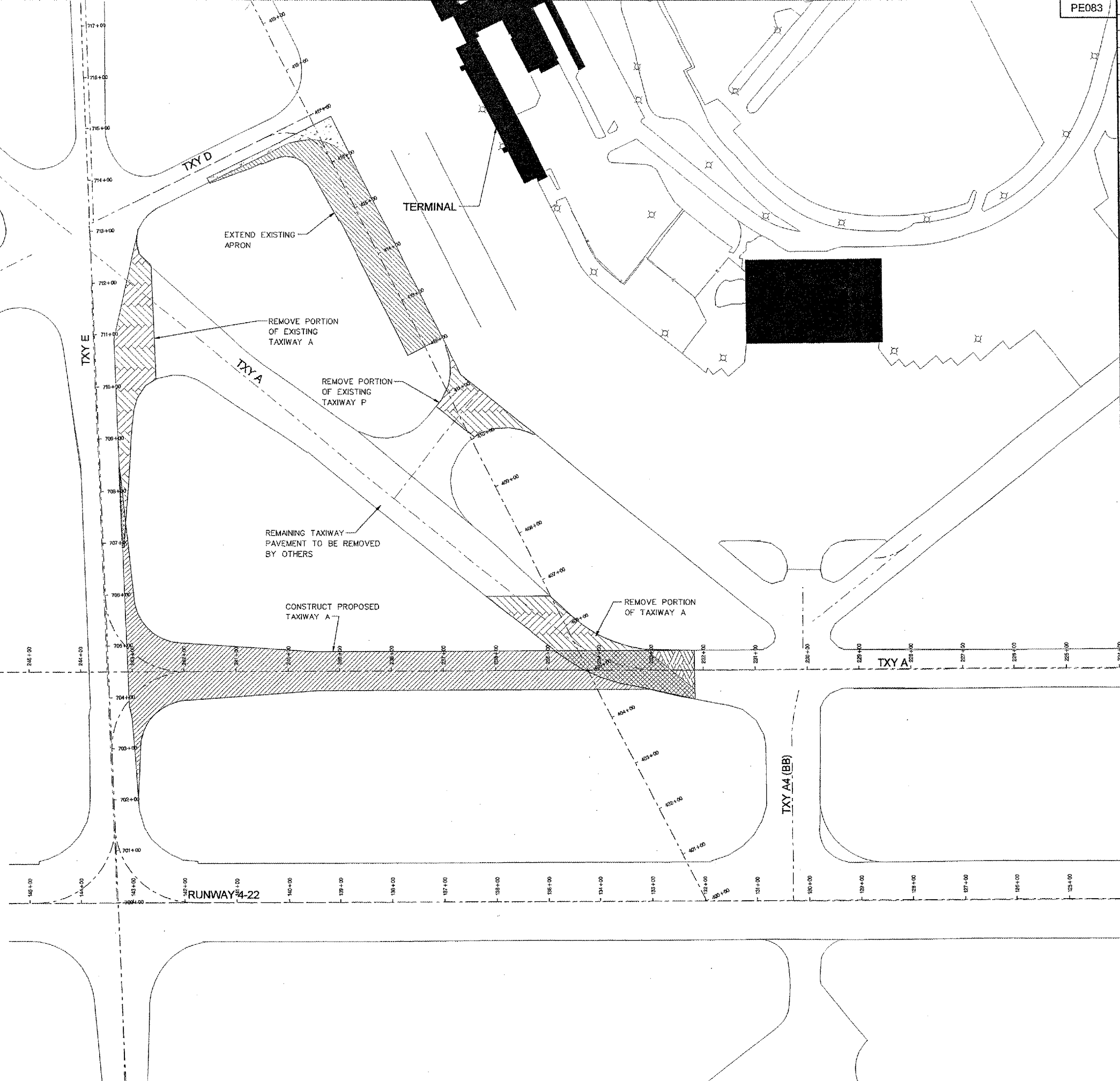
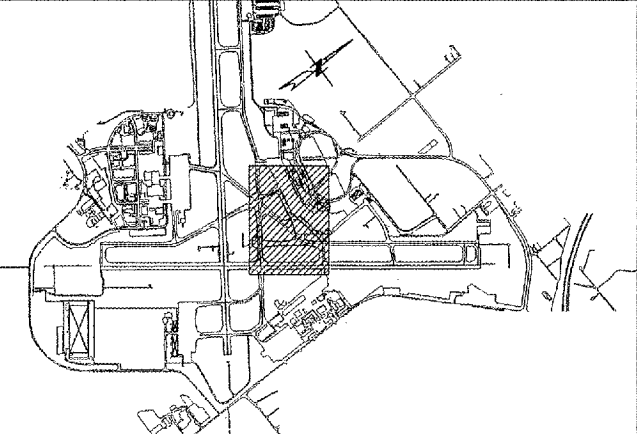
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).
 PLOT 1

LEGEND

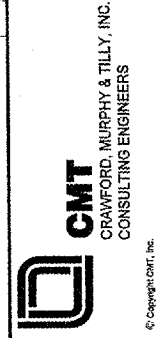
-  CONSTRUCT NEW FULL DEPTH BITUMINOUS PAVEMENT
-  CONSTRUCT NEW FULL DEPTH PCC PAVEMENT
-  REMOVE EXISTING BITUMINOUS PAVEMENT FULL DEPTH, CONSTRUCT NEW FULL DEPTH BITUMINOUS PAVEMENT
-  REMOVE EXISTING BITUMINOUS PAVEMENT FULL DEPTH, CONSTRUCT NEW FULL DEPTH PCC PAVEMENT
-  REMOVE EXISTING PCC PAVEMENT FULL DEPTH, CONSTRUCT NEW FULL DEPTH BITUMINOUS PAVEMENT
-  REMOVE EXISTING BITUMINOUS PAVEMENT FULL DEPTH



KEY MAP



RELOCATE TAXIWAY A FROM
 MID-FIELD INTERSECTION



IL PROJECT: PIA-3343
 AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\drawsheets\004_prop_imp.dgn

DESIGN BY:	TCS
DRAWN BY:	CAG
CHECKED BY:	TCS
APPROVED BY:	CET
DATE:	JUNE 24, 2005
JOB No:	04061-03

PROPOSED IMPROVEMENTS

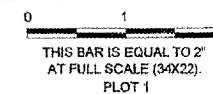
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PHASE 1 NOTES

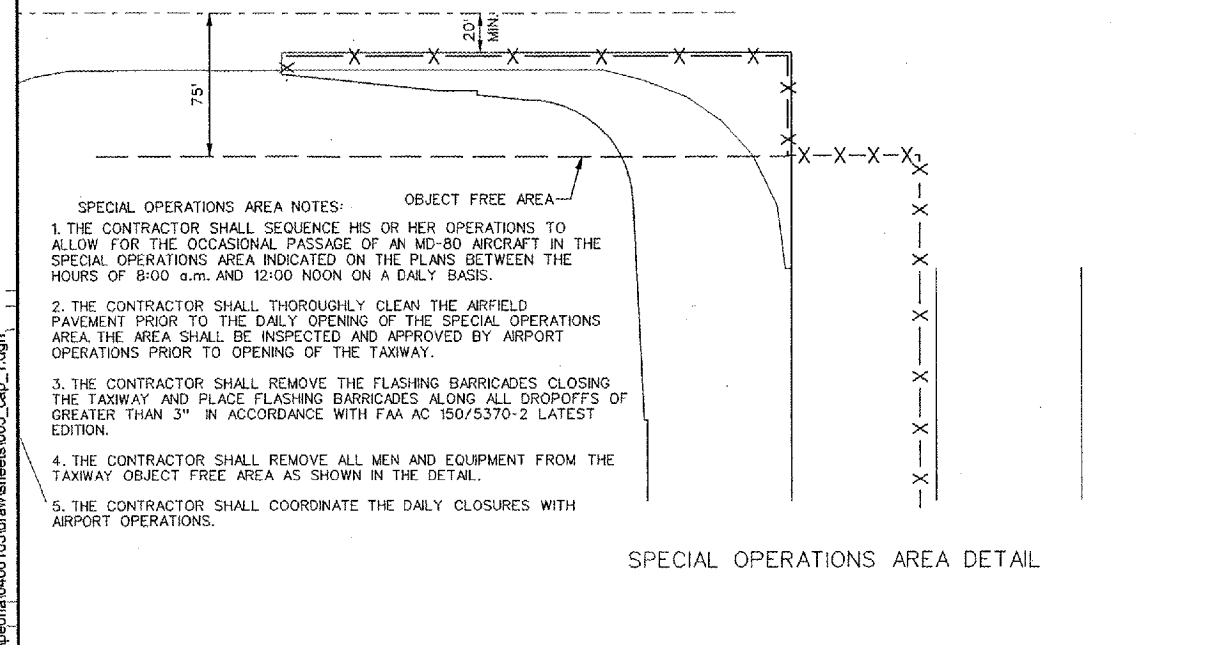
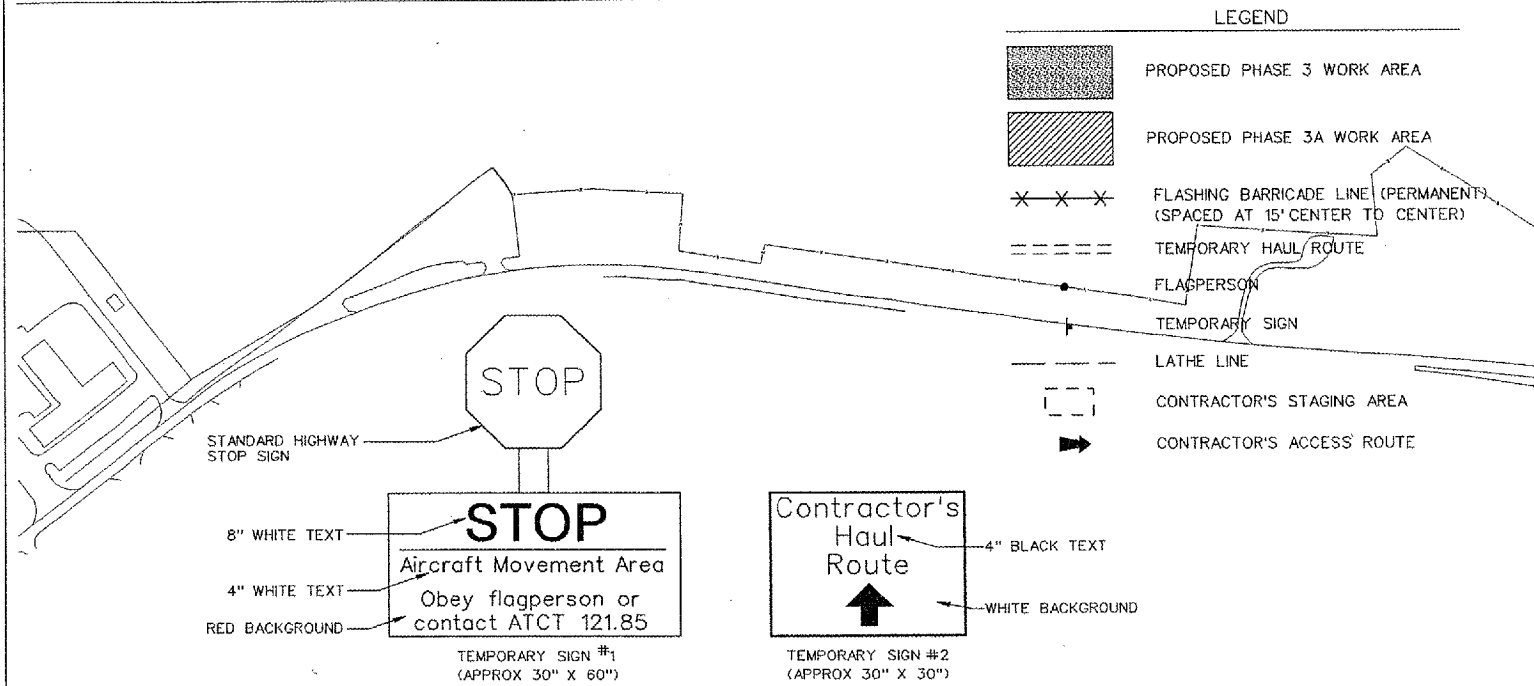
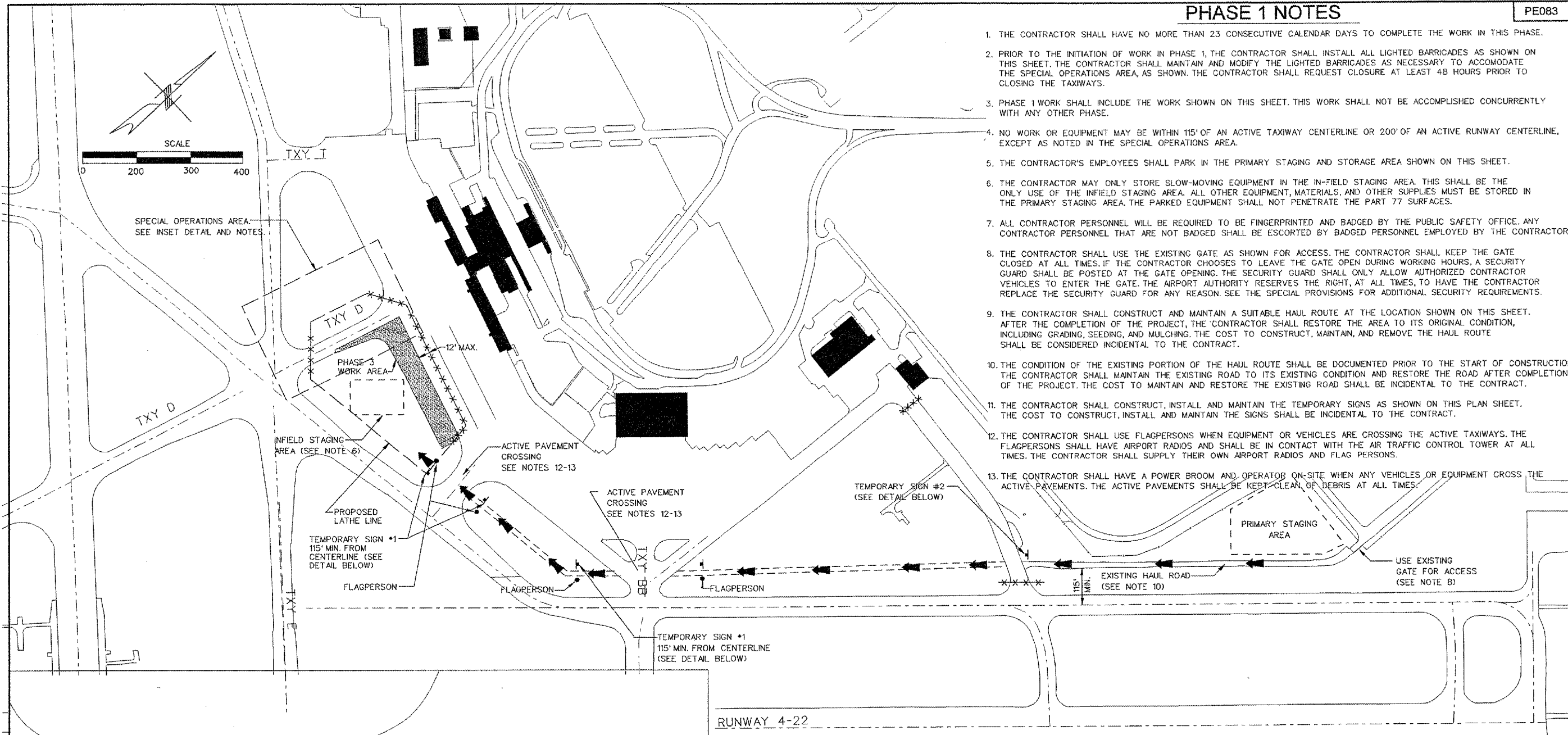
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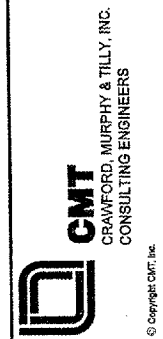
NUMBER	BY	DATE



1. THE CONTRACTOR SHALL HAVE NO MORE THAN 23 CONSECUTIVE CALENDAR DAYS TO COMPLETE THE WORK IN THIS PHASE.
2. PRIOR TO THE INITIATION OF WORK IN PHASE 1, THE CONTRACTOR SHALL INSTALL ALL LIGHTED BARRICADES AS SHOWN ON THIS SHEET. THE CONTRACTOR SHALL MAINTAIN AND MODIFY THE LIGHTED BARRICADES AS NECESSARY TO ACCOMMODATE THE SPECIAL OPERATIONS AREA, AS SHOWN. THE CONTRACTOR SHALL REQUEST CLOSURE AT LEAST 48 HOURS PRIOR TO CLOSING THE TAXIWAYS.
3. PHASE 1 WORK SHALL INCLUDE THE WORK SHOWN ON THIS SHEET. THIS WORK SHALL NOT BE ACCOMPLISHED CONCURRENTLY WITH ANY OTHER PHASE.
4. NO WORK OR EQUIPMENT MAY BE WITHIN 115' OF AN ACTIVE TAXIWAY CENTERLINE OR 200' OF AN ACTIVE RUNWAY CENTERLINE, EXCEPT AS NOTED IN THE SPECIAL OPERATIONS AREA.
5. THE CONTRACTOR'S EMPLOYEES SHALL PARK IN THE PRIMARY STAGING AND STORAGE AREA SHOWN ON THIS SHEET.
6. THE CONTRACTOR MAY ONLY STORE SLOW-MOVING EQUIPMENT IN THE IN-FIELD STAGING AREA. THIS SHALL BE THE ONLY USE OF THE INFIELD STAGING AREA. ALL OTHER EQUIPMENT, MATERIALS, AND OTHER SUPPLIES MUST BE STORED IN THE PRIMARY STAGING AREA. THE PARKED EQUIPMENT SHALL NOT PENETRATE THE PART 77 SURFACES.
7. ALL CONTRACTOR PERSONNEL WILL BE REQUIRED TO BE FINGERPRINTED AND BADGED BY THE PUBLIC SAFETY OFFICE. ANY CONTRACTOR PERSONNEL THAT ARE NOT BADGED SHALL BE ESCORTED BY BADGED PERSONNEL EMPLOYED BY THE CONTRACTOR.
8. THE CONTRACTOR SHALL USE THE EXISTING GATE AS SHOWN FOR ACCESS. THE CONTRACTOR SHALL KEEP THE GATE CLOSED AT ALL TIMES. IF THE CONTRACTOR CHOOSES TO LEAVE THE GATE OPEN DURING WORKING HOURS, A SECURITY GUARD SHALL BE POSTED AT THE GATE OPENING. THE SECURITY GUARD SHALL ONLY ALLOW AUTHORIZED CONTRACTOR VEHICLES TO ENTER THE GATE. THE AIRPORT AUTHORITY RESERVES THE RIGHT, AT ALL TIMES, TO HAVE THE CONTRACTOR REPLACE THE SECURITY GUARD FOR ANY REASON. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL SECURITY REQUIREMENTS.
9. THE CONTRACTOR SHALL CONSTRUCT AND MAINTAIN A SUITABLE HAUL ROUTE AT THE LOCATION SHOWN ON THIS SHEET. AFTER THE COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL RESTORE THE AREA TO ITS ORIGINAL CONDITION, INCLUDING GRADING, SEEDING, AND MULCHING. THE COST TO CONSTRUCT, MAINTAIN, AND REMOVE THE HAUL ROUTE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. THE CONDITION OF THE EXISTING PORTION OF THE HAUL ROUTE SHALL BE DOCUMENTED PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL MAINTAIN THE EXISTING ROAD TO ITS EXISTING CONDITION AND RESTORE THE ROAD AFTER COMPLETION OF THE PROJECT. THE COST TO MAINTAIN AND RESTORE THE EXISTING ROAD SHALL BE INCIDENTAL TO THE CONTRACT.
11. THE CONTRACTOR SHALL CONSTRUCT, INSTALL AND MAINTAIN THE TEMPORARY SIGNS AS SHOWN ON THIS PLAN SHEET. THE COST TO CONSTRUCT, INSTALL AND MAINTAIN THE SIGNS SHALL BE INCIDENTAL TO THE CONTRACT.
12. THE CONTRACTOR SHALL USE FLAGPERSONS WHEN EQUIPMENT OR VEHICLES ARE CROSSING THE ACTIVE TAXIWAYS. THE FLAGPERSONS SHALL HAVE AIRPORT RADIOS AND SHALL BE IN CONTACT WITH THE AIR TRAFFIC CONTROL TOWER AT ALL TIMES. THE CONTRACTOR SHALL SUPPLY THEIR OWN AIRPORT RADIOS AND FLAG PERSONS.
13. THE CONTRACTOR SHALL HAVE A POWER BROOM AND OPERATOR ON-SITE WHEN ANY VEHICLES OR EQUIPMENT CROSS THE ACTIVE PAVEMENTS. THE ACTIVE PAVEMENTS SHALL BE KEPT CLEAN OF DEBRIS AT ALL TIMES.



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION



IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\drawsheets\005_cap_1.dgn

DESIGN BY: TCS
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET
DATE: JUNE 24, 2005
JOB No: 04061-03

CONSTRUCTION ACTIVITY PLAN PHASE 1

SHEET 5 OF 43 SHEETS

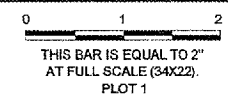
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PHASE 2 NOTES

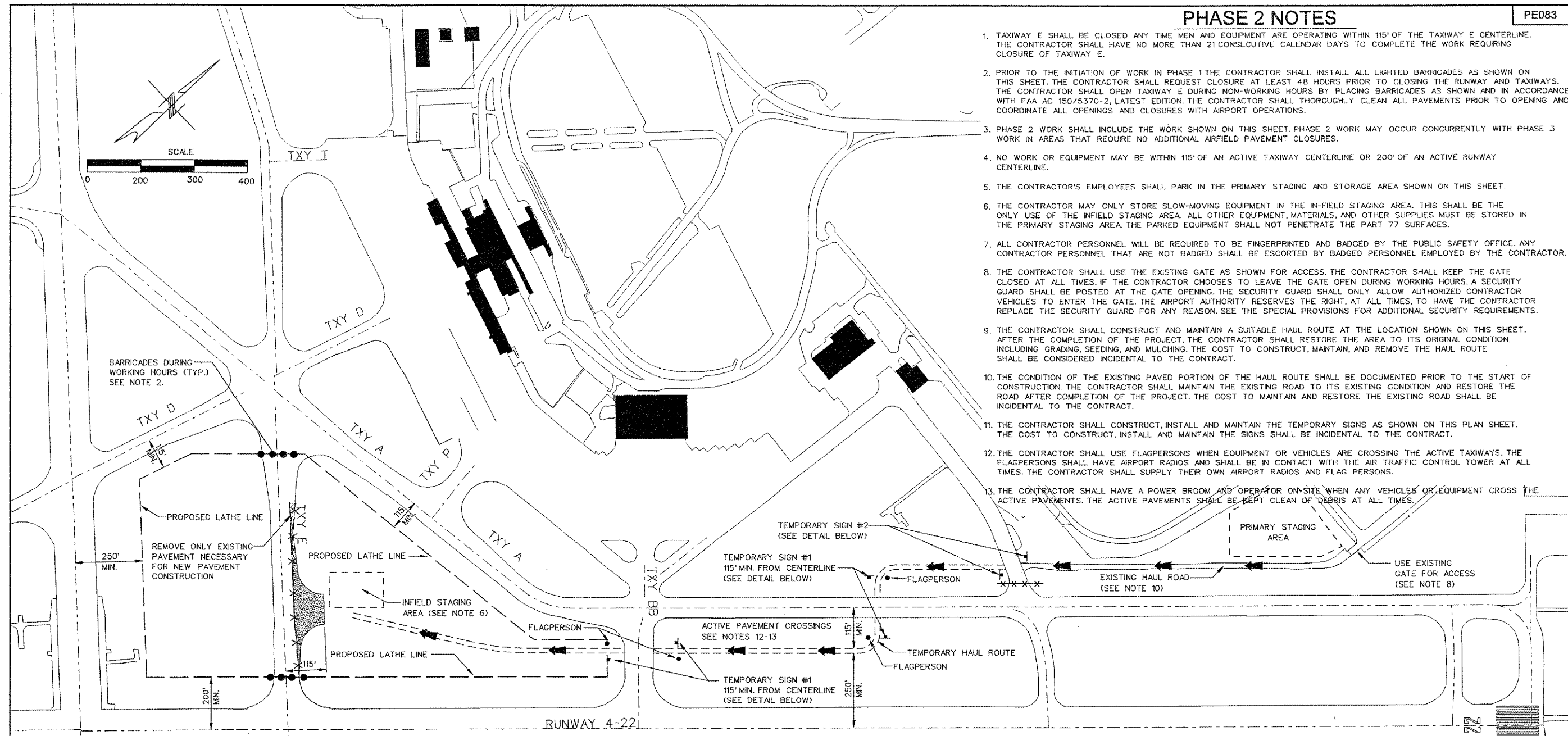
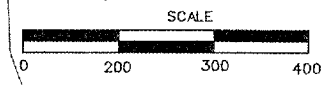
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REVISIONS

NUMBER	BY	DATE

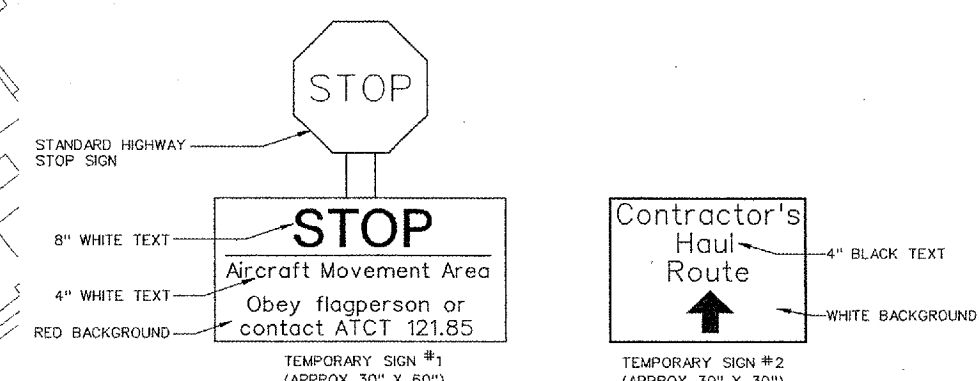


1. TAXIWAY E SHALL BE CLOSED ANY TIME MEN AND EQUIPMENT ARE OPERATING WITHIN 115' OF THE TAXIWAY E CENTERLINE. THE CONTRACTOR SHALL HAVE NO MORE THAN 21 CONSECUTIVE CALENDAR DAYS TO COMPLETE THE WORK REQUIRING CLOSURE OF TAXIWAY E.
2. PRIOR TO THE INITIATION OF WORK IN PHASE 1 THE CONTRACTOR SHALL INSTALL ALL LIGHTED BARRICADES AS SHOWN ON THIS SHEET. THE CONTRACTOR SHALL REQUEST CLOSURE AT LEAST 48 HOURS PRIOR TO CLOSING THE RUNWAY AND TAXIWAYS. THE CONTRACTOR SHALL OPEN TAXIWAY E DURING NON-WORKING HOURS BY PLACING BARRICADES AS SHOWN AND IN ACCORDANCE WITH FAA AC 150/5370-2, LATEST EDITION. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL PAVEMENTS PRIOR TO OPENING AND COORDINATE ALL OPENINGS AND CLOSURES WITH AIRPORT OPERATIONS.
3. PHASE 2 WORK SHALL INCLUDE THE WORK SHOWN ON THIS SHEET. PHASE 2 WORK MAY OCCUR CONCURRENTLY WITH PHASE 3 WORK IN AREAS THAT REQUIRE NO ADDITIONAL AIRFIELD PAVEMENT CLOSURES.
4. NO WORK OR EQUIPMENT MAY BE WITHIN 115' OF AN ACTIVE TAXIWAY CENTERLINE OR 200' OF AN ACTIVE RUNWAY CENTERLINE.
5. THE CONTRACTOR'S EMPLOYEES SHALL PARK IN THE PRIMARY STAGING AND STORAGE AREA SHOWN ON THIS SHEET.
6. THE CONTRACTOR MAY ONLY STORE SLOW-MOVING EQUIPMENT IN THE IN-FIELD STAGING AREA. THIS SHALL BE THE ONLY USE OF THE INFIELD STAGING AREA. ALL OTHER EQUIPMENT, MATERIALS, AND OTHER SUPPLIES MUST BE STORED IN THE PRIMARY STAGING AREA. THE PARKED EQUIPMENT SHALL NOT PENETRATE THE PART 77 SURFACES.
7. ALL CONTRACTOR PERSONNEL WILL BE REQUIRED TO BE FINGERPRINTED AND BADGED BY THE PUBLIC SAFETY OFFICE. ANY CONTRACTOR PERSONNEL THAT ARE NOT BADGED SHALL BE ESCORTED BY BADGED PERSONNEL EMPLOYED BY THE CONTRACTOR.
8. THE CONTRACTOR SHALL USE THE EXISTING GATE AS SHOWN FOR ACCESS. THE CONTRACTOR SHALL KEEP THE GATE CLOSED AT ALL TIMES. IF THE CONTRACTOR CHOOSES TO LEAVE THE GATE OPEN DURING WORKING HOURS, A SECURITY GUARD SHALL BE POSTED AT THE GATE OPENING. THE SECURITY GUARD SHALL ONLY ALLOW AUTHORIZED CONTRACTOR VEHICLES TO ENTER THE GATE. THE AIRPORT AUTHORITY RESERVES THE RIGHT, AT ALL TIMES, TO HAVE THE CONTRACTOR REPLACE THE SECURITY GUARD FOR ANY REASON. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL SECURITY REQUIREMENTS.
9. THE CONTRACTOR SHALL CONSTRUCT AND MAINTAIN A SUITABLE HAUL ROUTE AT THE LOCATION SHOWN ON THIS SHEET. AFTER THE COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL RESTORE THE AREA TO ITS ORIGINAL CONDITION, INCLUDING GRADING, SEEDING, AND MULCHING. THE COST TO CONSTRUCT, MAINTAIN, AND REMOVE THE HAUL ROUTE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. THE CONDITION OF THE EXISTING PAVED PORTION OF THE HAUL ROUTE SHALL BE DOCUMENTED PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL MAINTAIN THE EXISTING ROAD TO ITS EXISTING CONDITION AND RESTORE THE ROAD AFTER COMPLETION OF THE PROJECT. THE COST TO MAINTAIN AND RESTORE THE EXISTING ROAD SHALL BE INCIDENTAL TO THE CONTRACT.
11. THE CONTRACTOR SHALL CONSTRUCT, INSTALL AND MAINTAIN THE TEMPORARY SIGNS AS SHOWN ON THIS PLAN SHEET. THE COST TO CONSTRUCT, INSTALL AND MAINTAIN THE SIGNS SHALL BE INCIDENTAL TO THE CONTRACT.
12. THE CONTRACTOR SHALL USE FLAGPERSONS WHEN EQUIPMENT OR VEHICLES ARE CROSSING THE ACTIVE TAXIWAYS. THE FLAGPERSONS SHALL HAVE AIRPORT RADIOS AND SHALL BE IN CONTACT WITH THE AIR TRAFFIC CONTROL TOWER AT ALL TIMES. THE CONTRACTOR SHALL SUPPLY THEIR OWN AIRPORT RADIOS AND FLAG PERSONS.
13. THE CONTRACTOR SHALL HAVE A POWER BROOM AND OPERATOR ON-SITE WHEN ANY VEHICLES OR EQUIPMENT CROSS THE ACTIVE PAVEMENTS. THE ACTIVE PAVEMENTS SHALL BE KEPT CLEAN OF DEBRIS AT ALL TIMES.



LEGEND

- PROPOSED PHASE 1 WORK AREA
- FLASHING BARRICADE LINE (PERMANENT) (SPACED AT 15' CENTER TO CENTER)
- FLASHING BARRICADE LINE (WORKING HOURS ONLY) (SPACED AT 15' CENTER TO CENTER)
- TEMPORARY HAUL ROUTE
- FLAGPERSON
- TEMPORARY SIGN
- LATHE LINE
- CONTRACTOR'S STAGING AREA
- CONTRACTOR'S ACCESS ROUTE



GREATER Peoria REGIONAL Airport

RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION

CMT CRAWFORD, MURPHY & TILLY, INC. CONSULTING ENGINEERS

IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\drawsheets\006_cap_2.dgn
DESIGN BY: TCS
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET
DATE: JUNE 24, 2005
JOB No: 04061-03
CONSTRUCTION ACTIVITY PLAN PHASE 2
SHEET 6 OF 43 SHEETS

06/23/2005 k:\peoria\0406103\drawsheets\006_cap_2.dgn

PHASE 3 NOTES

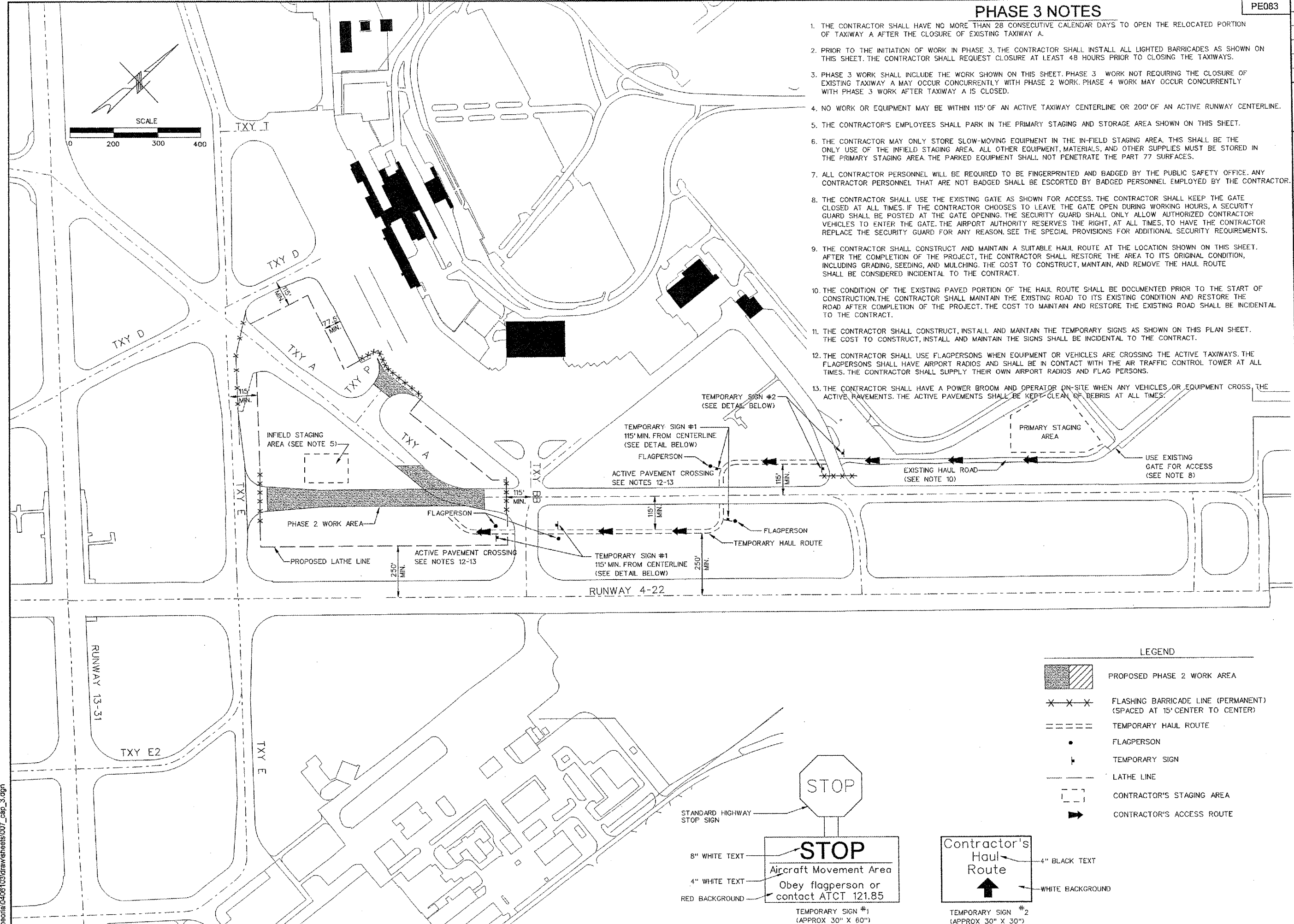
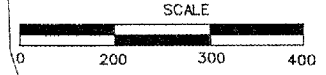
PE083

REVISIONS

NUMBER	BY	DATE

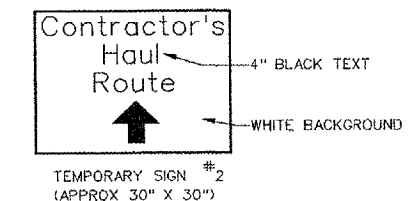
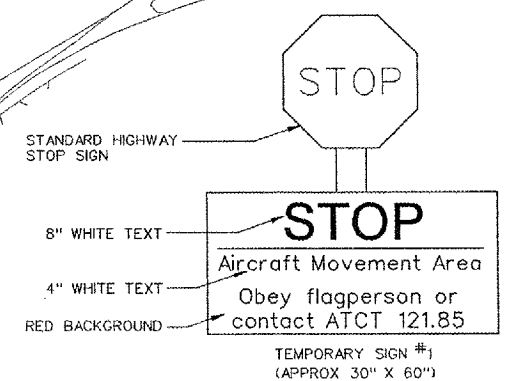
0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).
PLOT 1

1. THE CONTRACTOR SHALL HAVE NO MORE THAN 28 CONSECUTIVE CALENDAR DAYS TO OPEN THE RELOCATED PORTION OF TAXIWAY A AFTER THE CLOSURE OF EXISTING TAXIWAY A.
2. PRIOR TO THE INITIATION OF WORK IN PHASE 3, THE CONTRACTOR SHALL INSTALL ALL LIGHTED BARRICADES AS SHOWN ON THIS SHEET. THE CONTRACTOR SHALL REQUEST CLOSURE AT LEAST 48 HOURS PRIOR TO CLOSING THE TAXIWAYS.
3. PHASE 3 WORK SHALL INCLUDE THE WORK SHOWN ON THIS SHEET. PHASE 3 WORK NOT REQUIRING THE CLOSURE OF EXISTING TAXIWAY A MAY OCCUR CONCURRENTLY WITH PHASE 2 WORK. PHASE 4 WORK MAY OCCUR CONCURRENTLY WITH PHASE 3 WORK AFTER TAXIWAY A IS CLOSED.
4. NO WORK OR EQUIPMENT MAY BE WITHIN 115' OF AN ACTIVE TAXIWAY CENTERLINE OR 200' OF AN ACTIVE RUNWAY CENTERLINE.
5. THE CONTRACTOR'S EMPLOYEES SHALL PARK IN THE PRIMARY STAGING AND STORAGE AREA SHOWN ON THIS SHEET.
6. THE CONTRACTOR MAY ONLY STORE SLOW-MOVING EQUIPMENT IN THE IN-FIELD STAGING AREA. THIS SHALL BE THE ONLY USE OF THE INFIELD STAGING AREA. ALL OTHER EQUIPMENT, MATERIALS, AND OTHER SUPPLIES MUST BE STORED IN THE PRIMARY STAGING AREA. THE PARKED EQUIPMENT SHALL NOT PENETRATE THE PART 77 SURFACES.
7. ALL CONTRACTOR PERSONNEL WILL BE REQUIRED TO BE FINGERPRINTED AND BADGED BY THE PUBLIC SAFETY OFFICE. ANY CONTRACTOR PERSONNEL THAT ARE NOT BADGED SHALL BE ESCORTED BY BADGED PERSONNEL EMPLOYED BY THE CONTRACTOR.
8. THE CONTRACTOR SHALL USE THE EXISTING GATE AS SHOWN FOR ACCESS. THE CONTRACTOR SHALL KEEP THE GATE CLOSED AT ALL TIMES. IF THE CONTRACTOR CHOOSES TO LEAVE THE GATE OPEN DURING WORKING HOURS, A SECURITY GUARD SHALL BE POSTED AT THE GATE OPENING. THE SECURITY GUARD SHALL ONLY ALLOW AUTHORIZED CONTRACTOR VEHICLES TO ENTER THE GATE. THE AIRPORT AUTHORITY RESERVES THE RIGHT, AT ALL TIMES, TO HAVE THE CONTRACTOR REPLACE THE SECURITY GUARD FOR ANY REASON. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL SECURITY REQUIREMENTS.
9. THE CONTRACTOR SHALL CONSTRUCT AND MAINTAIN A SUITABLE HAUL ROUTE AT THE LOCATION SHOWN ON THIS SHEET. AFTER THE COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL RESTORE THE AREA TO ITS ORIGINAL CONDITION, INCLUDING GRADING, SEEDING, AND MULCHING. THE COST TO CONSTRUCT, MAINTAIN, AND REMOVE THE HAUL ROUTE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. THE CONDITION OF THE EXISTING PAVED PORTION OF THE HAUL ROUTE SHALL BE DOCUMENTED PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL MAINTAIN THE EXISTING ROAD TO ITS EXISTING CONDITION AND RESTORE THE ROAD AFTER COMPLETION OF THE PROJECT. THE COST TO MAINTAIN AND RESTORE THE EXISTING ROAD SHALL BE INCIDENTAL TO THE CONTRACT.
11. THE CONTRACTOR SHALL CONSTRUCT, INSTALL AND MAINTAIN THE TEMPORARY SIGNS AS SHOWN ON THIS PLAN SHEET. THE COST TO CONSTRUCT, INSTALL AND MAINTAIN THE SIGNS SHALL BE INCIDENTAL TO THE CONTRACT.
12. THE CONTRACTOR SHALL USE FLAGPERSONS WHEN EQUIPMENT OR VEHICLES ARE CROSSING THE ACTIVE TAXIWAYS. THE FLAGPERSONS SHALL HAVE AIRPORT RADIOS AND SHALL BE IN CONTACT WITH THE AIR TRAFFIC CONTROL TOWER AT ALL TIMES. THE CONTRACTOR SHALL SUPPLY THEIR OWN AIRPORT RADIOS AND FLAG PERSONS.
13. THE CONTRACTOR SHALL HAVE A POWER BROOM AND OPERATOR ON-SITE WHEN ANY VEHICLES OR EQUIPMENT CROSS THE ACTIVE PAVEMENTS. THE ACTIVE PAVEMENTS SHALL BE KEPT CLEAN OF DEBRIS AT ALL TIMES.

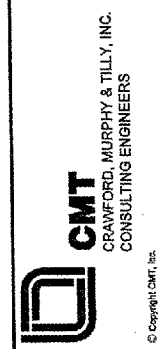


LEGEND

- PROPOSED PHASE 2 WORK AREA
- FLASHING BARRICADE LINE (PERMANENT) (SPACED AT 15' CENTER TO CENTER)
- TEMPORARY HAUL ROUTE
- FLAGPERSON
- TEMPORARY SIGN
- LATHE LINE
- CONTRACTOR'S STAGING AREA
- CONTRACTOR'S ACCESS ROUTE



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION



IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\drawsheets\007_cap_3.dgn

DESIGN BY: TCS

DRAWN BY: CAG

CHECKED BY: TCS

APPROVED BY: CET

DATE: JUNE 24, 2005

JOB No: 04061-03

CONSTRUCTION ACTIVITY PLAN PHASE 3

SHEET 7 OF 43 SHEETS

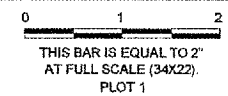
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PHASE 4 NOTES

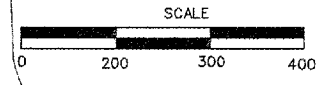
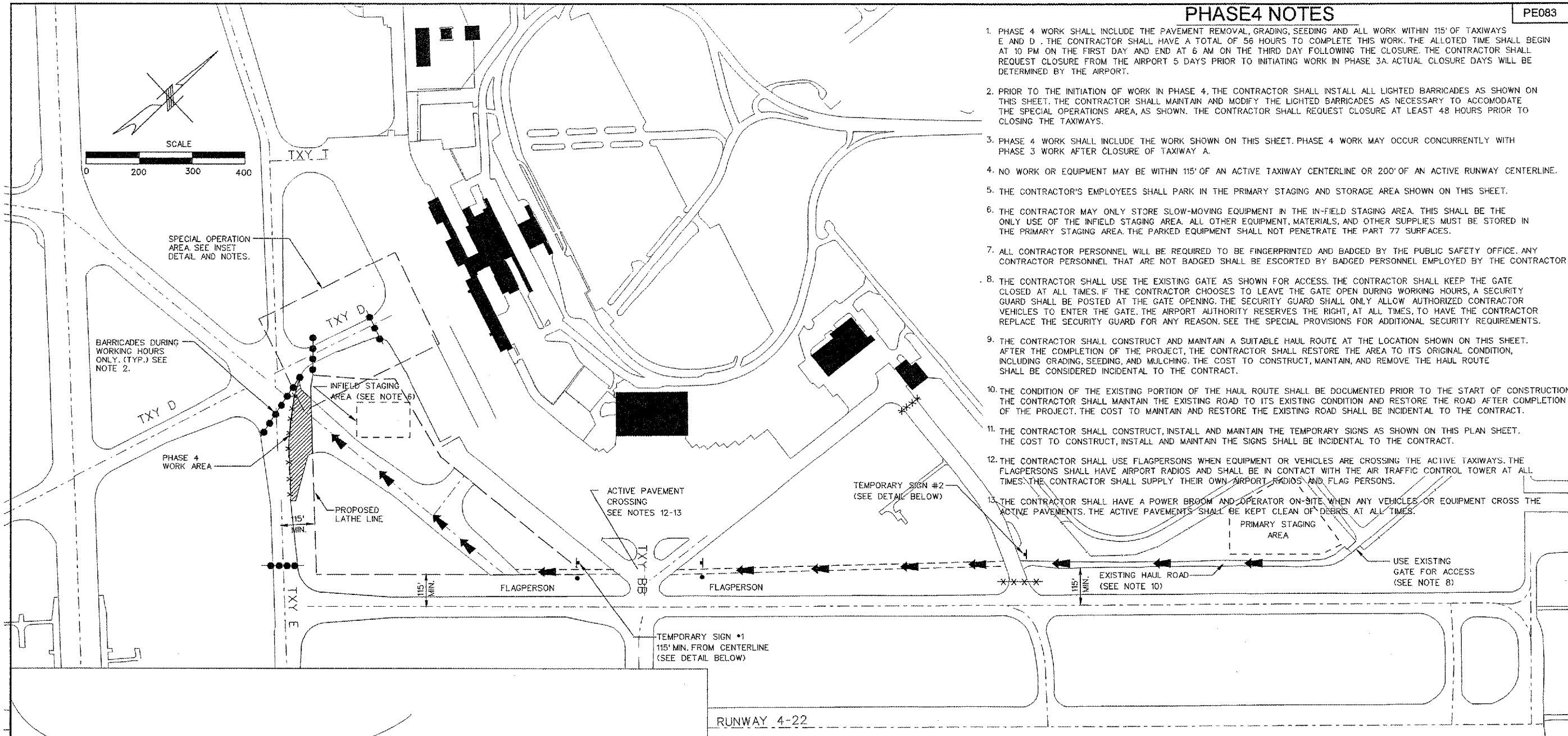
PE083

REVISIONS

NUMBER	BY	DATE



1. PHASE 4 WORK SHALL INCLUDE THE PAVEMENT REMOVAL, GRADING, SEEDING AND ALL WORK WITHIN 115' OF TAXIWAYS E AND D. THE CONTRACTOR SHALL HAVE A TOTAL OF 56 HOURS TO COMPLETE THIS WORK. THE ALLOTTED TIME SHALL BEGIN AT 10 PM ON THE FIRST DAY AND END AT 6 AM ON THE THIRD DAY FOLLOWING THE CLOSURE. THE CONTRACTOR SHALL REQUEST CLOSURE FROM THE AIRPORT 5 DAYS PRIOR TO INITIATING WORK IN PHASE 3A. ACTUAL CLOSURE DAYS WILL BE DETERMINED BY THE AIRPORT.
2. PRIOR TO THE INITIATION OF WORK IN PHASE 4, THE CONTRACTOR SHALL INSTALL ALL LIGHTED BARRICADES AS SHOWN ON THIS SHEET. THE CONTRACTOR SHALL MAINTAIN AND MODIFY THE LIGHTED BARRICADES AS NECESSARY TO ACCOMMODATE THE SPECIAL OPERATIONS AREA, AS SHOWN. THE CONTRACTOR SHALL REQUEST CLOSURE AT LEAST 48 HOURS PRIOR TO CLOSING THE TAXIWAYS.
3. PHASE 4 WORK SHALL INCLUDE THE WORK SHOWN ON THIS SHEET. PHASE 4 WORK MAY OCCUR CONCURRENTLY WITH PHASE 3 WORK AFTER CLOSURE OF TAXIWAY A.
4. NO WORK OR EQUIPMENT MAY BE WITHIN 115' OF AN ACTIVE TAXIWAY CENTERLINE OR 200' OF AN ACTIVE RUNWAY CENTERLINE.
5. THE CONTRACTOR'S EMPLOYEES SHALL PARK IN THE PRIMARY STAGING AND STORAGE AREA SHOWN ON THIS SHEET.
6. THE CONTRACTOR MAY ONLY STORE SLOW-MOVING EQUIPMENT IN THE IN-FIELD STAGING AREA. THIS SHALL BE THE ONLY USE OF THE INFIELD STAGING AREA. ALL OTHER EQUIPMENT, MATERIALS, AND OTHER SUPPLIES MUST BE STORED IN THE PRIMARY STAGING AREA. THE PARKED EQUIPMENT SHALL NOT PENETRATE THE PART 77 SURFACES.
7. ALL CONTRACTOR PERSONNEL WILL BE REQUIRED TO BE FINGERPRINTED AND BADGED BY THE PUBLIC SAFETY OFFICE. ANY CONTRACTOR PERSONNEL THAT ARE NOT BADGED SHALL BE ESCORTED BY BADGED PERSONNEL EMPLOYED BY THE CONTRACTOR.
8. THE CONTRACTOR SHALL USE THE EXISTING GATE AS SHOWN FOR ACCESS. THE CONTRACTOR SHALL KEEP THE GATE CLOSED AT ALL TIMES. IF THE CONTRACTOR CHOOSES TO LEAVE THE GATE OPEN DURING WORKING HOURS, A SECURITY GUARD SHALL BE POSTED AT THE GATE OPENING. THE SECURITY GUARD SHALL ONLY ALLOW AUTHORIZED CONTRACTOR VEHICLES TO ENTER THE GATE. THE AIRPORT AUTHORITY RESERVES THE RIGHT, AT ALL TIMES, TO HAVE THE CONTRACTOR REPLACE THE SECURITY GUARD FOR ANY REASON. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL SECURITY REQUIREMENTS.
9. THE CONTRACTOR SHALL CONSTRUCT AND MAINTAIN A SUITABLE HAUL ROUTE AT THE LOCATION SHOWN ON THIS SHEET. AFTER THE COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL RESTORE THE AREA TO ITS ORIGINAL CONDITION, INCLUDING GRADING, SEEDING, AND MULCHING. THE COST TO CONSTRUCT, MAINTAIN, AND REMOVE THE HAUL ROUTE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. THE CONDITION OF THE EXISTING PORTION OF THE HAUL ROUTE SHALL BE DOCUMENTED PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL MAINTAIN THE EXISTING ROAD TO ITS EXISTING CONDITION AND RESTORE THE ROAD AFTER COMPLETION OF THE PROJECT. THE COST TO MAINTAIN AND RESTORE THE EXISTING ROAD SHALL BE INCIDENTAL TO THE CONTRACT.
11. THE CONTRACTOR SHALL CONSTRUCT, INSTALL AND MAINTAIN THE TEMPORARY SIGNS AS SHOWN ON THIS PLAN SHEET. THE COST TO CONSTRUCT, INSTALL AND MAINTAIN THE SIGNS SHALL BE INCIDENTAL TO THE CONTRACT.
12. THE CONTRACTOR SHALL USE FLAGPERSONS WHEN EQUIPMENT OR VEHICLES ARE CROSSING THE ACTIVE TAXIWAYS. THE FLAGPERSONS SHALL HAVE AIRPORT RADIOS AND SHALL BE IN CONTACT WITH THE AIR TRAFFIC CONTROL TOWER AT ALL TIMES. THE CONTRACTOR SHALL SUPPLY THEIR OWN AIRPORT RADIOS AND FLAG PERSONS.
13. THE CONTRACTOR SHALL HAVE A POWER BROOM AND OPERATOR ON-SITE WHEN ANY VEHICLES OR EQUIPMENT CROSS THE ACTIVE PAVEMENTS. THE ACTIVE PAVEMENTS SHALL BE KEPT CLEAN OF DEBRIS AT ALL TIMES.



BARRICADES DURING WORKING HOURS ONLY. (TYP.) SEE NOTE 2.

SPECIAL OPERATION AREA. SEE INSET DETAIL AND NOTES.

INFIELD STAGING AREA (SEE NOTE 6)

PROPOSED LATHE LINE

ACTIVE PAVEMENT CROSSING SEE NOTES 12-13

TEMPORARY SIGN #2 (SEE DETAIL BELOW)

TEMPORARY SIGN #1 115' MIN. FROM CENTERLINE (SEE DETAIL BELOW)

PRIMARY STAGING AREA

USE EXISTING GATE FOR ACCESS (SEE NOTE 8)

EXISTING HAUL ROAD (SEE NOTE 10)

RUNWAY 4-22

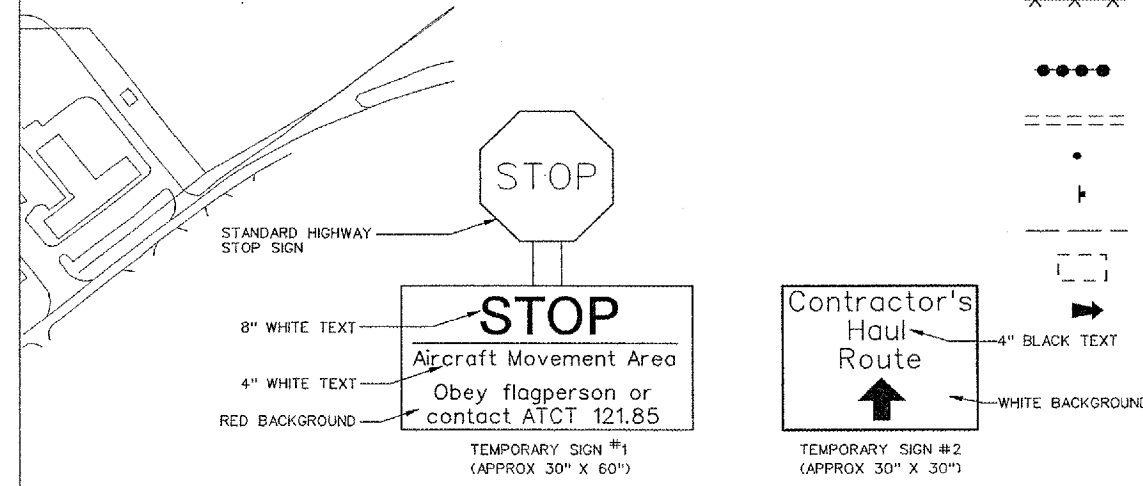
LEGEND

- PROPOSED PHASE 3 WORK AREA
- PROPOSED PHASE 3A WORK AREA
- FLASHING BARRICADE LINE (PERMANENT) (SPACED AT 15' CENTER TO CENTER)
- FLASHING BARRICADE LINE (WORKING HOURS ONLY) (SPACED AT 15' CENTER TO CENTER)
- TEMPORARY HAUL ROUTE
- FLAGPERSON
- TEMPORARY SIGN
- LATHE LINE
- CONTRACTOR'S STAGING AREA
- CONTRACTOR'S ACCESS ROUTE

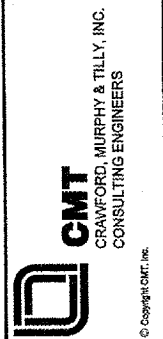
- SPECIAL OPERATIONS AREA NOTES:**
1. THE CONTRACTOR SHALL SEQUENCE HIS OR HER OPERATIONS TO ALLOW FOR THE OCCASIONAL PASSAGE OF AN MD-80 AIRCRAFT IN THE SPECIAL OPERATIONS AREA INDICATED ON THE PLANS BETWEEN THE HOURS OF 8:00 a.m. AND 12:00 NOON ON A DAILY BASIS.
 2. THE CONTRACTOR SHALL THOROUGHLY CLEAN THE AIRFIELD PAVEMENT PRIOR TO THE DAILY OPENING OF THE SPECIAL OPERATIONS AREA. THE AREA SHALL BE INSPECTED AND APPROVED BY AIRPORT OPERATIONS PRIOR TO OPENING OF THE TAXIWAY.
 3. THE CONTRACTOR SHALL REMOVE THE FLASHING BARRICADES CLOSING THE TAXIWAY AND PLACE FLASHING BARRICADES ALONG ALL DROPOFFS OF GREATER THAN 3" IN ACCORDANCE WITH FAA AC 150/5370-2 LATEST EDITION.
 4. THE CONTRACTOR SHALL REMOVE ALL MEN AND EQUIPMENT FROM THE TAXIWAY OBJECT FREE AREA AS SHOWN IN THE DETAIL.
 5. THE CONTRACTOR SHALL COORDINATE THE DAILY CLOSURES WITH AIRPORT OPERATIONS.

OBJECT FREE AREA

SPECIAL OPERATIONS AREA DETAIL



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION



IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\draw\sheet\008_cap_4.dgn
DESIGN BY: TCS
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET
DATE: JUNE 24, 2005
JOB No: 04061-03

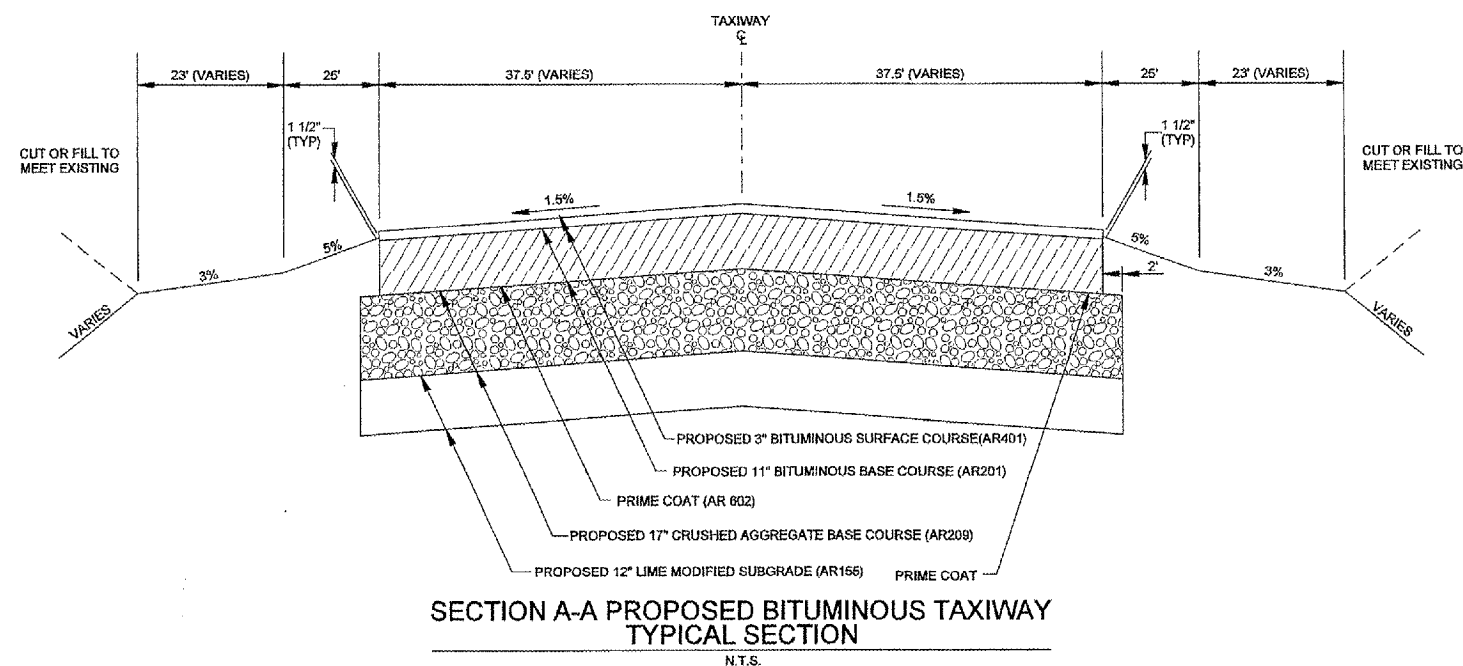
CONSTRUCTION ACTIVITY PLAN
PHASE 4
SHEET 8 OF 43 SHEETS

06/23/2005 k:\peoria\0406103\draw\sheet\008_cap_4.dgn

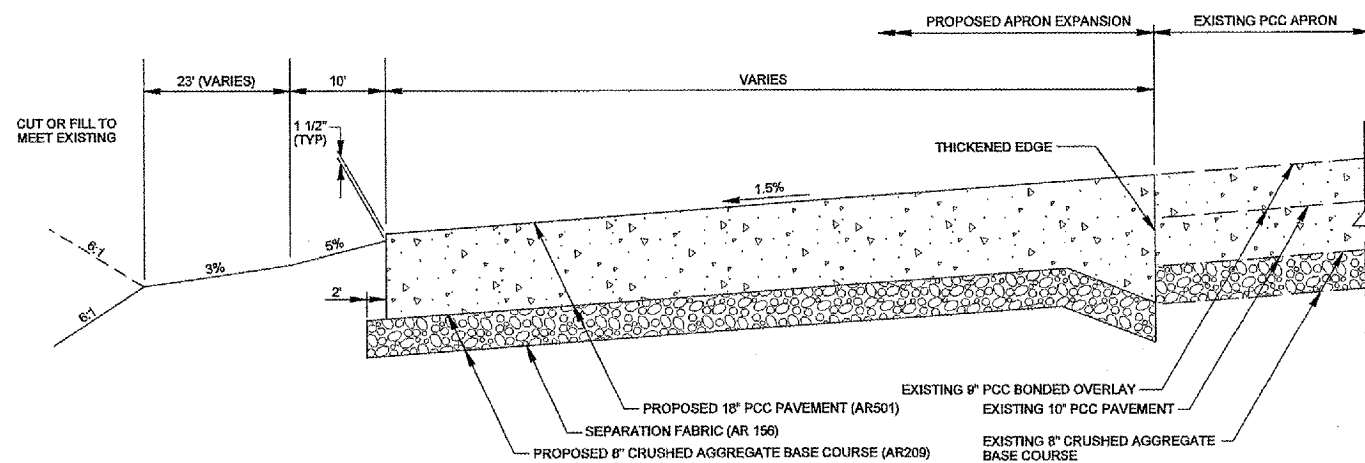
PE083

REVISIONS		
NUMBER	BY	DATE

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 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34X22).
 PLOT 1

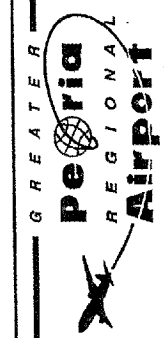


SECTION A-A PROPOSED BITUMINOUS TAXIWAY TYPICAL SECTION
 N.T.S.

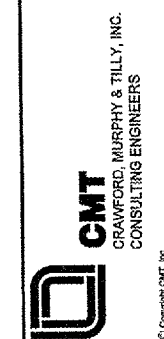


SECTION B-B PROPOSED APRON EXPANSION TYPICAL SECTION
 N.T.S.

NOTE:
 SEE PLAN AND PROFILE SHEETS
 FOR SECTIONS CUT LOCATIONS



RELOCATE TAXIWAY A FROM
 MID-FIELD INTERSECTION



IL PROJECT: PIA-3343
 AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\drawsheets\009_typical_sections.dgn
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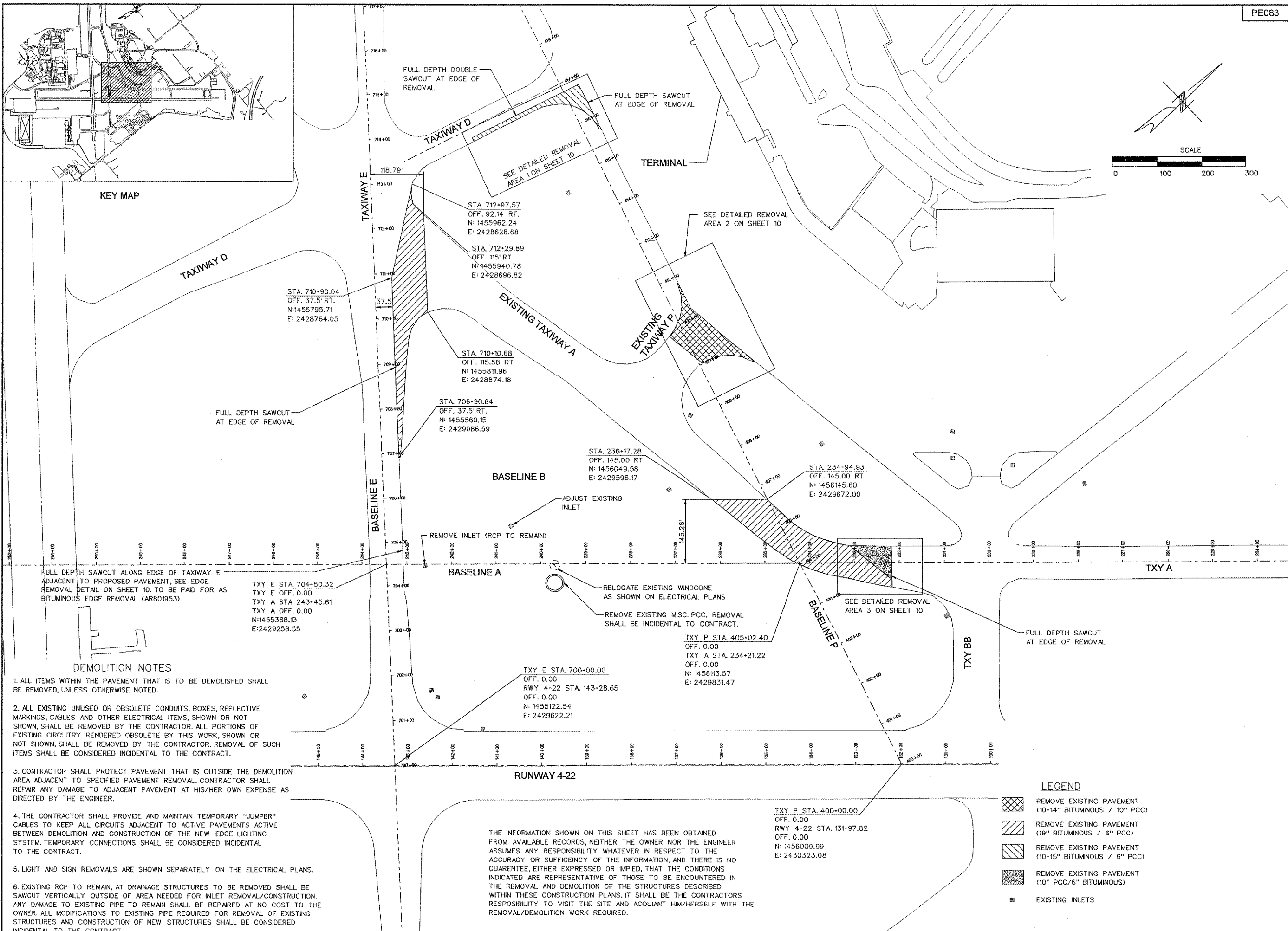
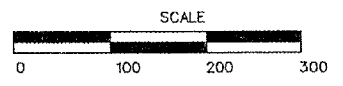
TYPICAL SECTIONS

SHEET 9 OF 43 SHEETS

5/23/2005
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NUMBER	BY	DATE

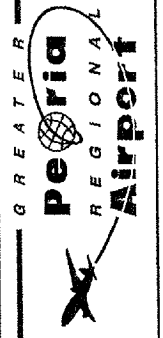
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 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34X22).
 PLOT 1



KEY MAP

SCALE

RELOCATE TAXIWAY A FROM
 MID-FIELD INTERSECTION



CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS



IL PROJECT: PIA-3343
 AIP PROJ: 3-17-0080-XX

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DESIGN BY:	TCS
DRAWN BY:	CAG
CHECKED BY:	TCS
APPROVED BY:	CET
DATE:	JUNE 24, 2005
JOB No:	04061-03

DEMOLITION
 PLAN

06/23/2005
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FULL DEPTH SAWCUT ALONG EDGE OF TAXIWAY E ADJACENT TO PROPOSED PAVEMENT, SEE EDGE REMOVAL DETAIL ON SHEET 10. TO BE PAID FOR AS BITUMINOUS EDGE REMOVAL (ARBO1953)

DEMOLITION NOTES

1. ALL ITEMS WITHIN THE PAVEMENT THAT IS TO BE DEMOLISHED SHALL BE REMOVED, UNLESS OTHERWISE NOTED.
2. ALL EXISTING UNUSED OR OBSOLETE CONDUITS, BOXES, REFLECTIVE MARKINGS, CABLES AND OTHER ELECTRICAL ITEMS, SHOWN OR NOT SHOWN, SHALL BE REMOVED BY THE CONTRACTOR. ALL PORTIONS OF EXISTING CIRCUITRY RENDERED OBSOLETE BY THIS WORK, SHOWN OR NOT SHOWN, SHALL BE REMOVED BY THE CONTRACTOR. REMOVAL OF SUCH ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
3. CONTRACTOR SHALL PROTECT PAVEMENT THAT IS OUTSIDE THE DEMOLITION AREA ADJACENT TO SPECIFIED PAVEMENT REMOVAL. CONTRACTOR SHALL REPAIR ANY DAMAGE TO ADJACENT PAVEMENT AT HIS/HER OWN EXPENSE AS DIRECTED BY THE ENGINEER.
4. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY "JUMPER" CABLES TO KEEP ALL CIRCUITS ADJACENT TO ACTIVE PAVEMENTS ACTIVE BETWEEN DEMOLITION AND CONSTRUCTION OF THE NEW EDGE LIGHTING SYSTEM. TEMPORARY CONNECTIONS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
5. LIGHT AND SIGN REMOVALS ARE SHOWN SEPARATELY ON THE ELECTRICAL PLANS.
6. EXISTING RCP TO REMAIN, AT DRAINAGE STRUCTURES TO BE REMOVED SHALL BE SAWCUT VERTICALLY OUTSIDE OF AREA NEEDED FOR INLET REMOVAL/CONSTRUCTION. ANY DAMAGE TO EXISTING PIPE TO REMAIN SHALL BE REPAIRED AT NO COST TO THE OWNER. ALL MODIFICATIONS TO EXISTING PIPE REQUIRED FOR REMOVAL OF EXISTING STRUCTURES AND CONSTRUCTION OF NEW STRUCTURES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

THE INFORMATION SHOWN ON THIS SHEET HAS BEEN OBTAINED FROM AVAILABLE RECORDS, NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION, AND THERE IS NO GUARENTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE REMOVAL AND DEMOLITION OF THE STRUCTURES DESCRIBED WITHIN THESE CONSTRUCTION PLANS. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIM/HERSELF WITH THE REMOVAL/DEMOLITION WORK REQUIRED.

LEGEND

- REMOVE EXISTING PAVEMENT (10-14" BITUMINOUS / 10" PCC)
- REMOVE EXISTING PAVEMENT (19" BITUMINOUS / 6" PCC)
- REMOVE EXISTING PAVEMENT (10-15" BITUMINOUS / 6" PCC)
- REMOVE EXISTING PAVEMENT (10" PCC/6" BITUMINOUS)
- EXISTING INLETS

TXY E STA. 704+50.32
 TXY E OFF. 0.00
 TXY A STA. 243+45.61
 TXY A OFF. 0.00
 N: 1455388.13
 E: 2429258.55

TXY E STA. 700+00.00
 OFF. 0.00
 RWY 4-22 STA. 143+28.65
 OFF. 0.00
 N: 1455122.54
 E: 2429622.21

TXY P STA. 405+02.40
 OFF. 0.00
 TXY A STA. 234+21.22
 OFF. 0.00
 N: 1456113.57
 E: 2429831.47

TXY P STA. 400+00.00
 OFF. 0.00
 RWY 4-22 STA. 131+97.82
 OFF. 0.00
 N: 1456009.99
 E: 2430323.08

STA. 710+90.04
 OFF. 37.5' RT.
 N: 1455795.71
 E: 2428764.05

STA. 712+97.57
 OFF. 92.14 RT.
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STA. 712+29.89
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 E: 2428696.82

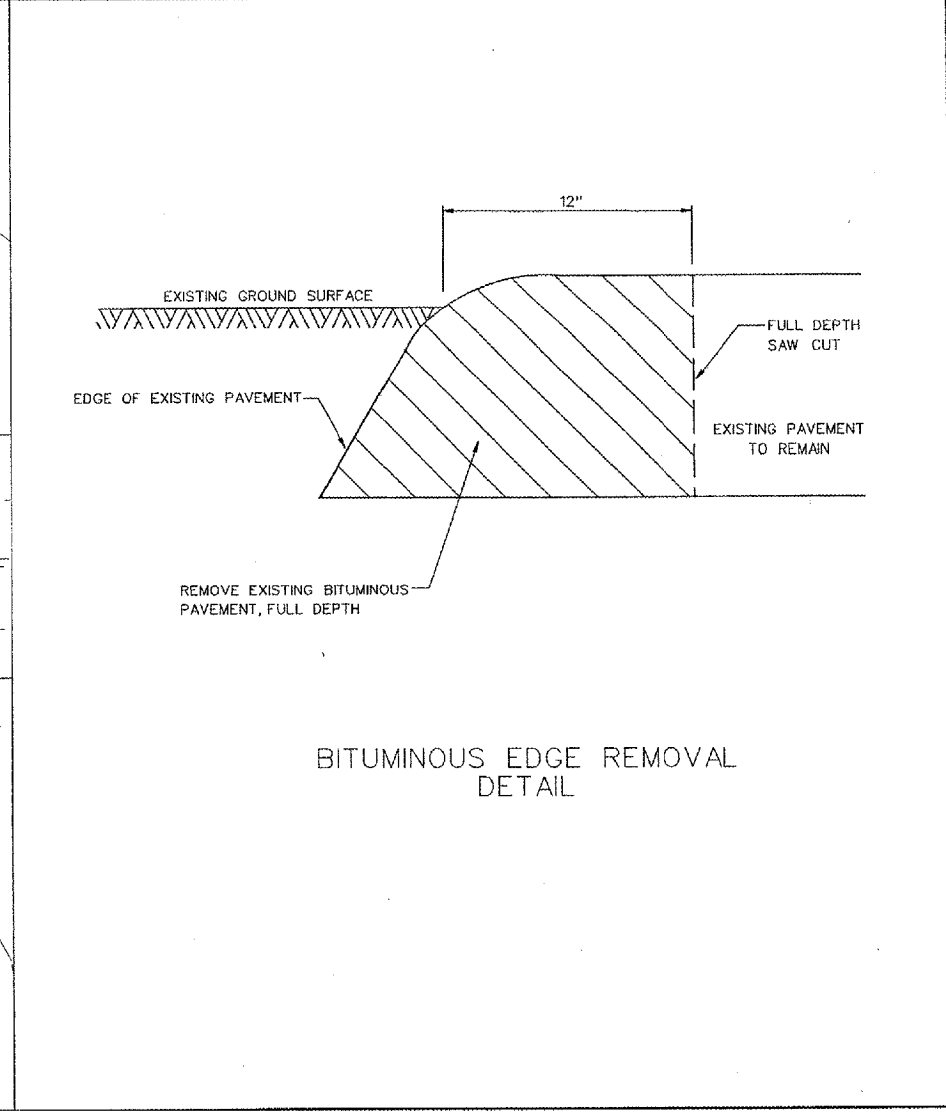
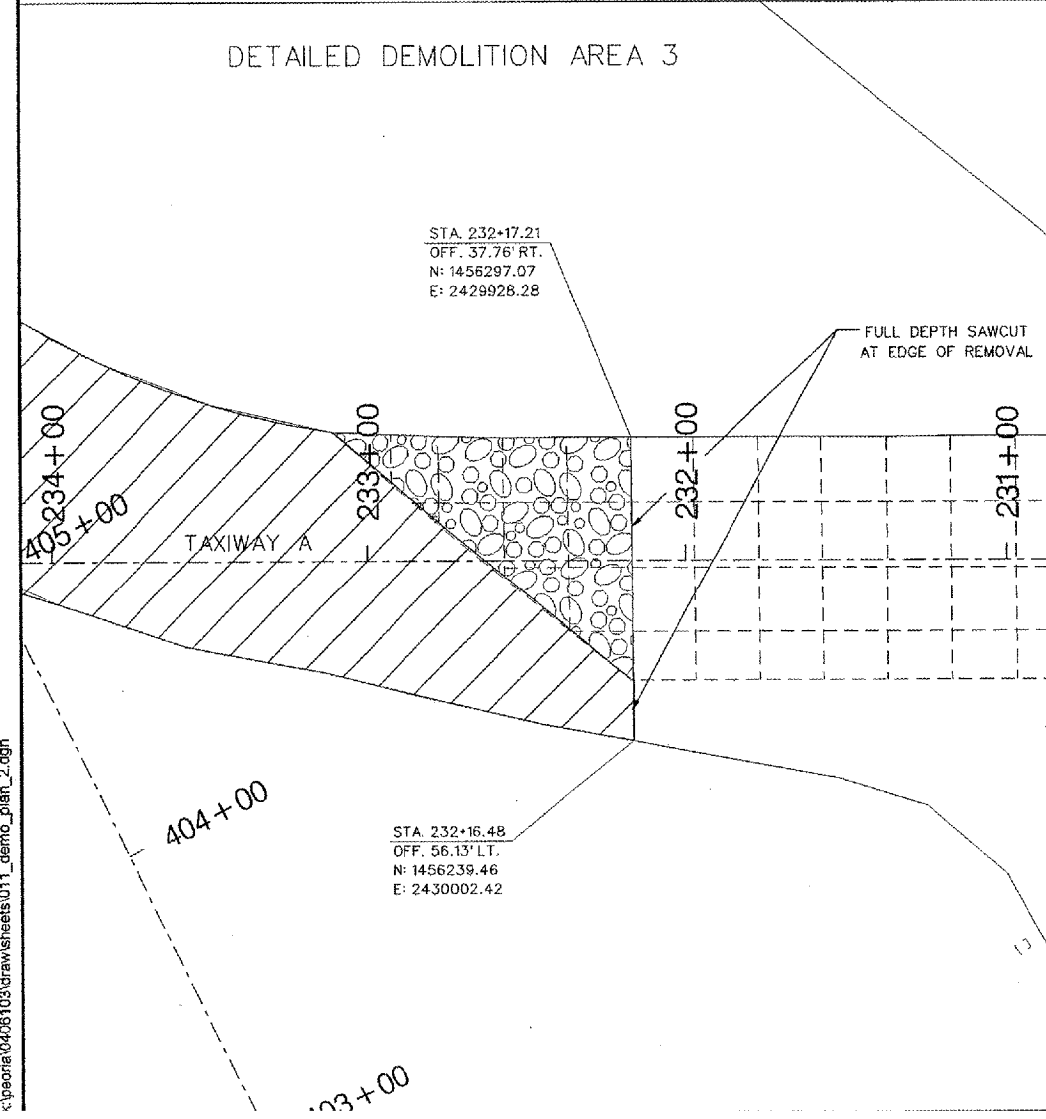
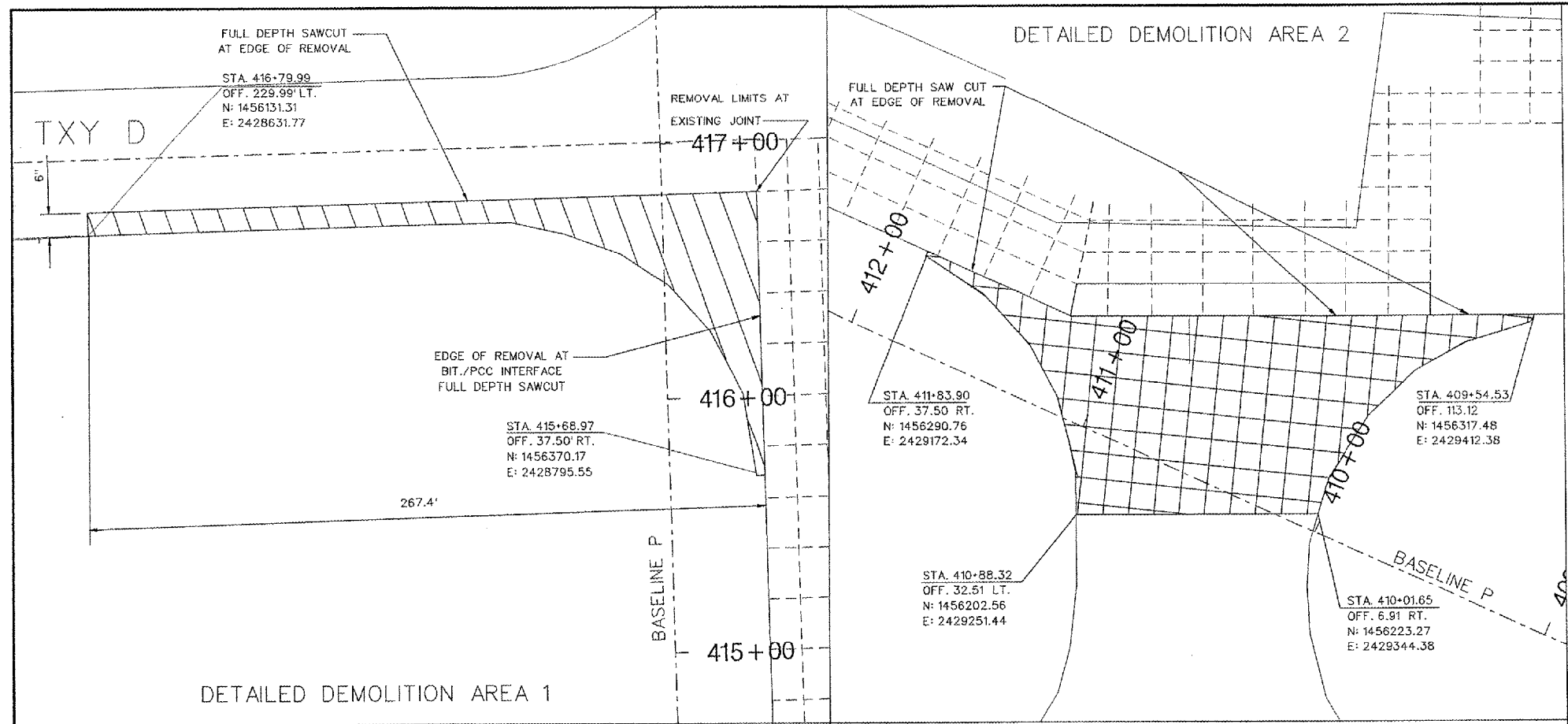
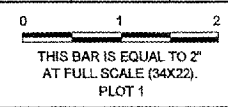
STA. 710+10.68
 OFF. 115.58 RT.
 N: 1455811.96
 E: 2428874.18

STA. 706+90.64
 OFF. 37.5' RT.
 N: 1455560.15
 E: 2429086.59

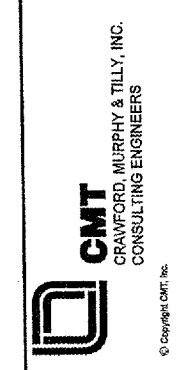
STA. 236+17.28
 OFF. 145.00 RT.
 N: 1456049.58
 E: 2429596.17

STA. 234+94.93
 OFF. 145.00 RT.
 N: 1456145.60
 E: 2429672.00

REVISIONS		
NUMBER	BY	DATE



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION



IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

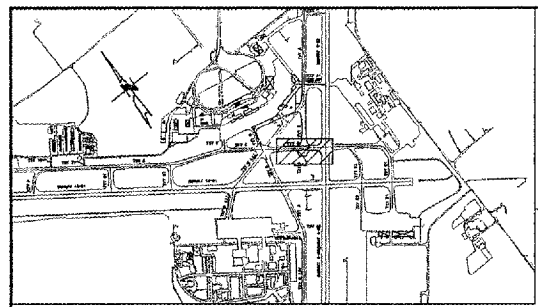
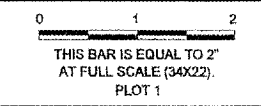
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DESIGN BY: TCS
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET
DATE: JUNE 24, 2005
JOB No: 04061-03

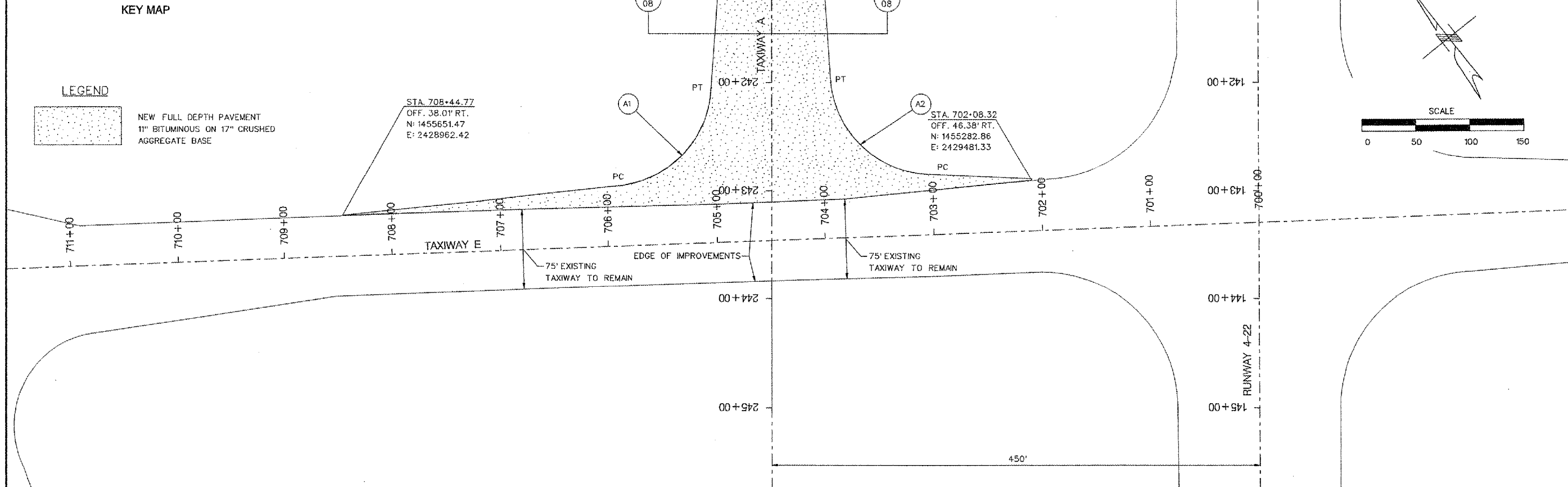
DEMOLITION PLAN DETAILS

06/23/2005 k:\peoria\0406103\drawsheets\011_demo_plan_2.dgn

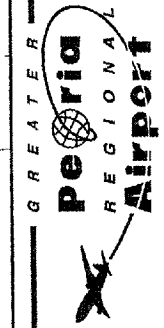
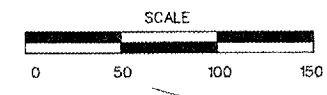
REVISIONS		
NUMBER	BY	DATE



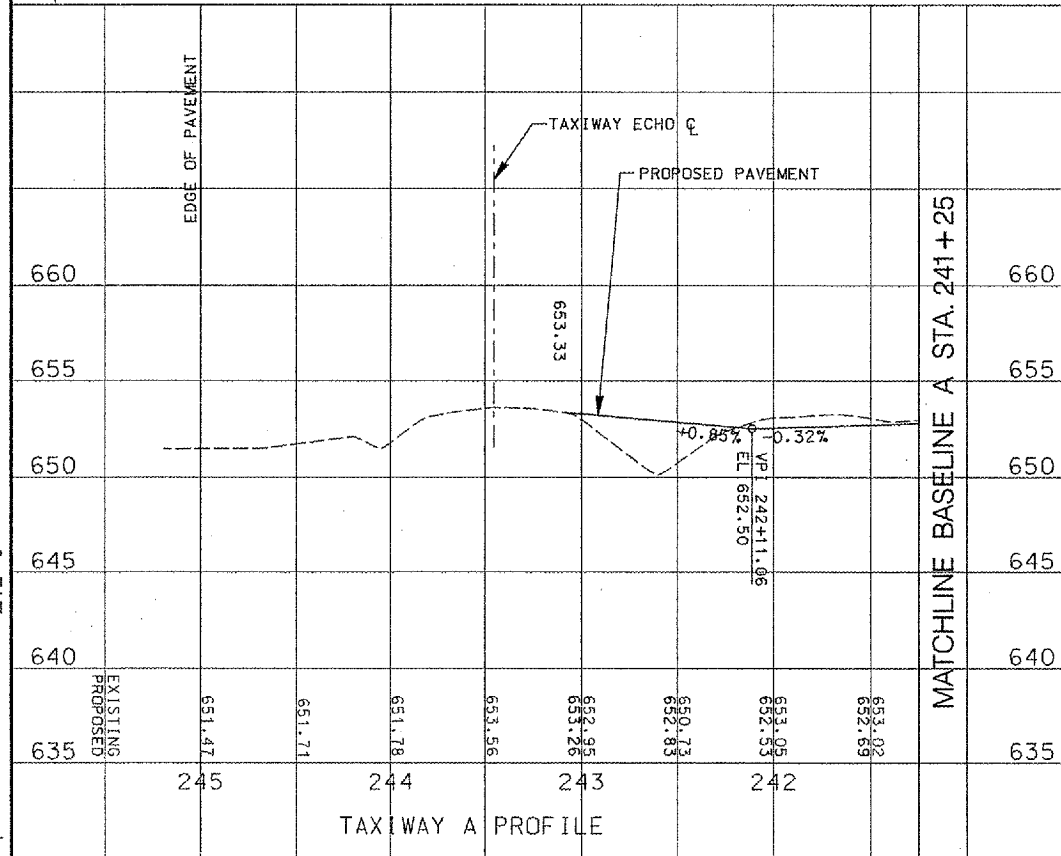
MATCHLINE BASELINE A STA. 241+25



LEGEND
 NEW FULL DEPTH PAVEMENT
 11" BITUMINOUS ON 17" CRUSHED AGGREGATE BASE

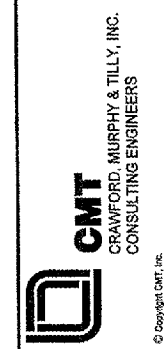


RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION



HORIZONTAL CURVE DATA

CURVE	POINT	STATION	OFFSET	COORDINATES		CURVE ELEMENTS
				NORTHING	EASTING	
A1	PC	242+96.10	146.42' RT	1455517.74	2429174.33	RADIUS=96.00' LENGTH=145.74' DELTA=87.90°
	RP	242+01.16	150.00' RT	1455594.46	2429230.36	
	PT	242+01.07	55.00' RT	1455535.66	2429304.98	
A2	PC	242+84.85	150.00' LT	1455342.85	2429413.93	RADIUS=96.00' LENGTH=149.23' DELTA=90.00°
	RP	241+89.85	150.00' LT	1455417.40	2429472.80	
	PT	241+89.86	55.00' LT	1455476.28	2429398.25	
P1	PC	416+55.58	99.60' LT	1456253.87	2428682.54	RADIUS=55.00' LENGTH=85.92' DELTA=88°30'24.54"
	RP	416+00.58	100.00' LT	1456242.14	2428736.27	
	PT	416+00.65	45.00' LT	1456295.97	2428747.54	



IL PROJECT: PIA-3343
 AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\draw\sheets\012_pp_1.dgn
 DESIGN BY: TCS
 DRAWN BY: CAG
 CHECKED BY: TCS
 APPROVED BY: CET
 DATE: JUNE 24, 2005
 JOB No: 04061-03

PLAN AND PROFILE
 SHEET 1 OF 3

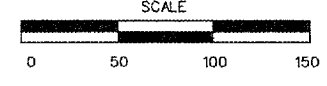
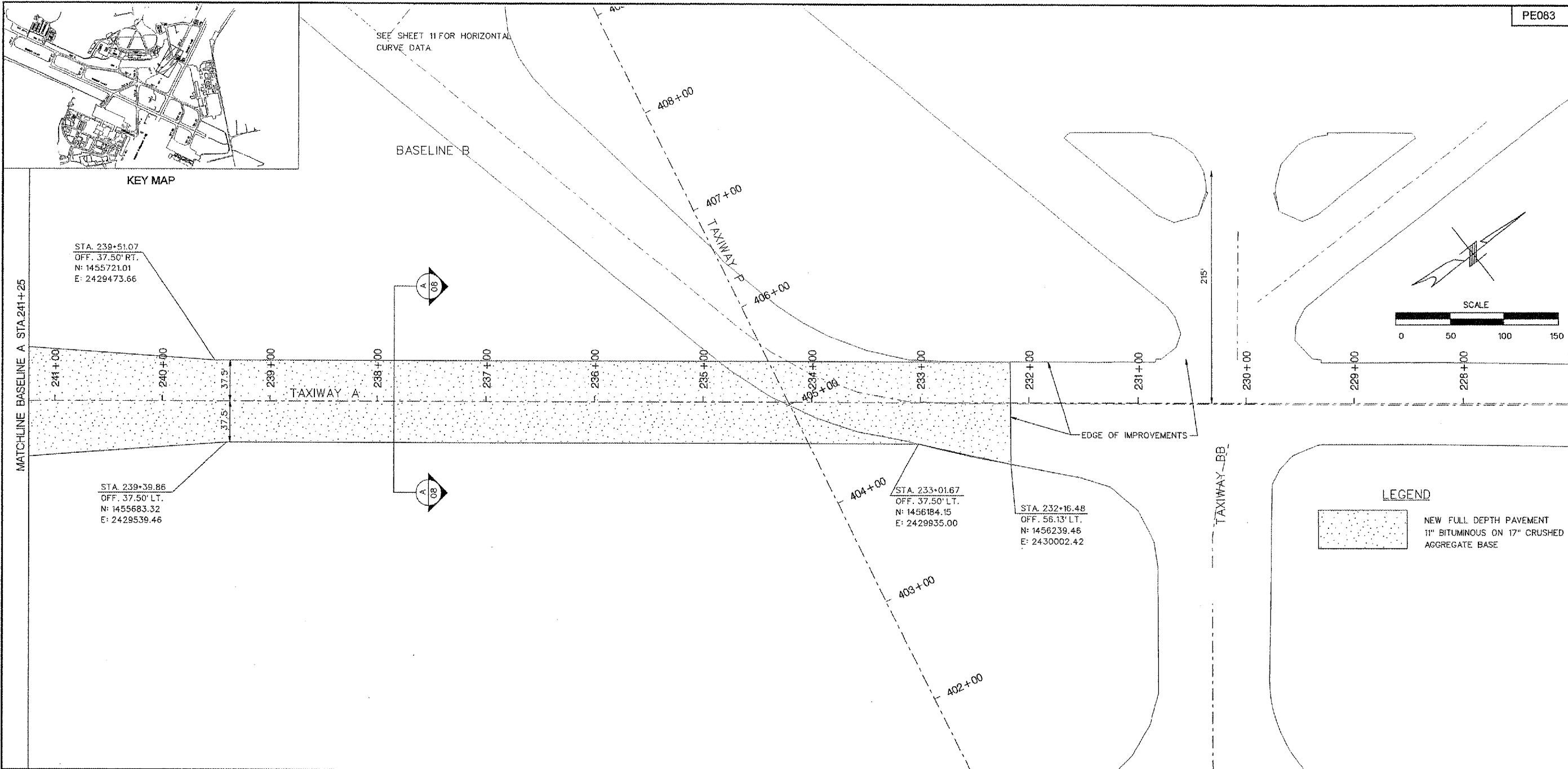
SHEET 12 OF 43 SHEETS

06/23/2005 k:\peoria\0406103\draw\sheets\012_pp_1.dgn

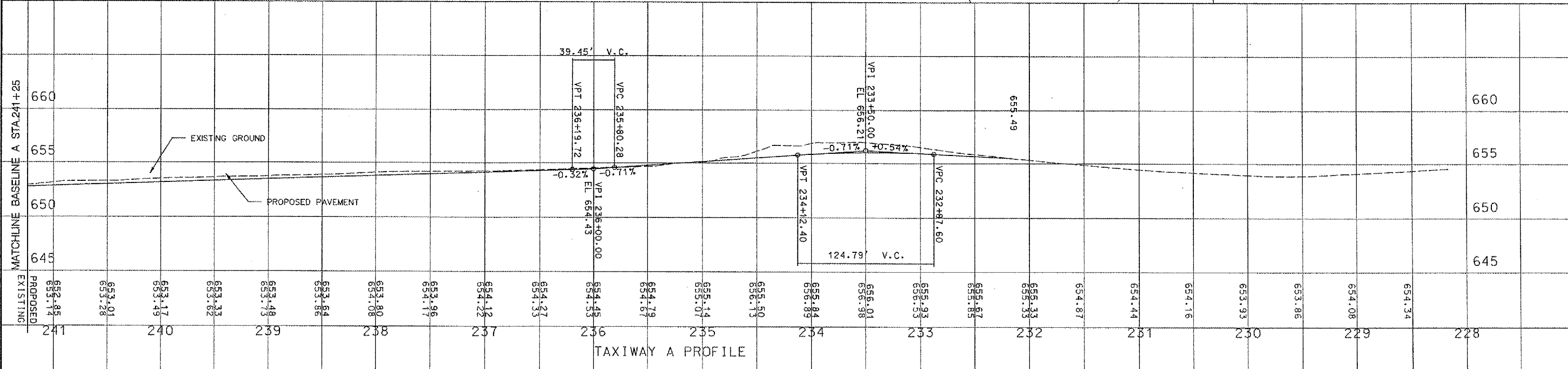
PE083

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22)
 PLOT 1



LEGEND
 NEW FULL DEPTH PAVEMENT
 11" BITUMINOUS ON 17" CRUSHED
 AGGREGATE BASE



GREATER Peoria REGIONAL Airport

RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS

IL PROJECT: PIA-3343
 AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\draw\sheet\013_pp_2.dgn

DESIGN BY: TCS
 DRAWN BY: CAG
 CHECKED BY: TCS
 APPROVED BY: CET

DATE: JUNE 24, 2005
 JOB No: 04061-03

PLAN AND PROFILE SHEET 2 OF 3

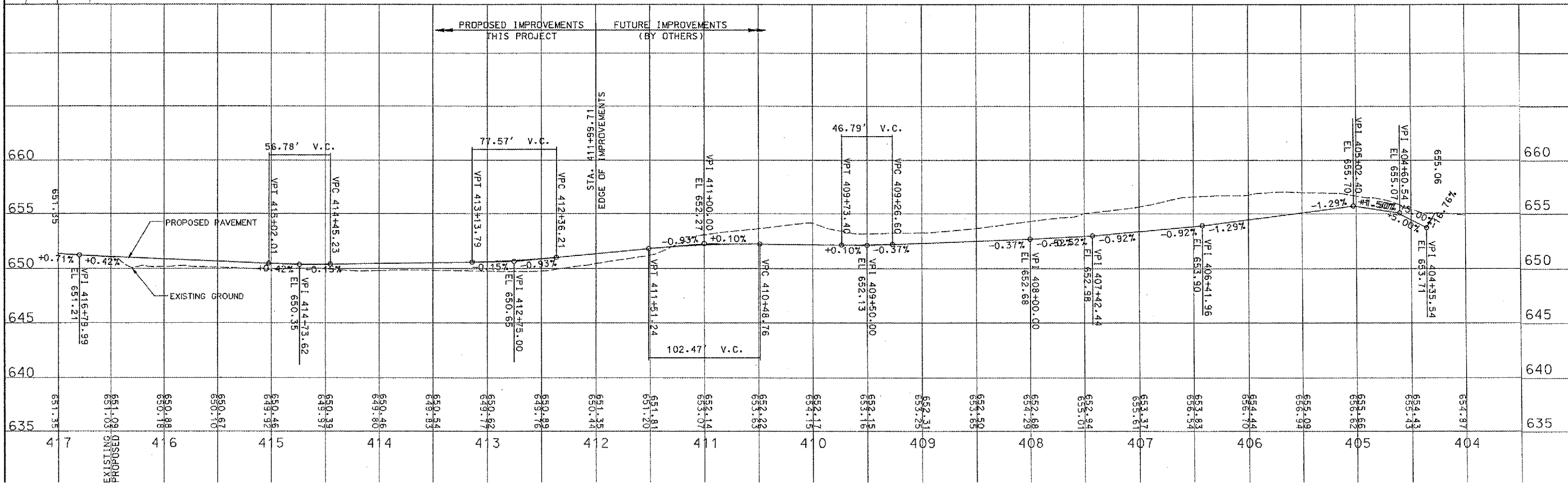
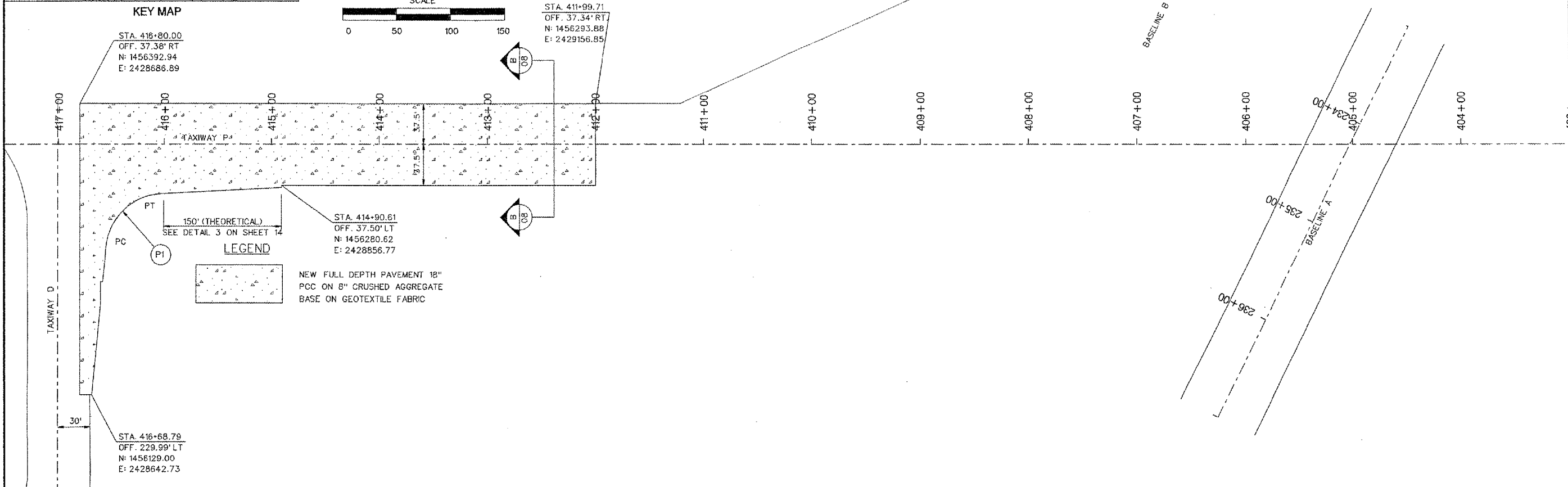
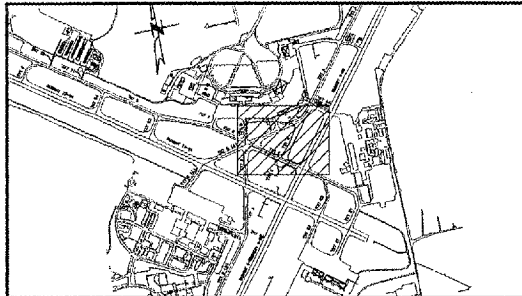
SHEET 13 OF 43 SHEETS

06/23/2005 k:\peoria\0406103\draw\sheet\013_pp_2.dgn

PE083

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34X22).
 PLOT 1



GREATER
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Airport

**RELOCATE TAXIWAY A FROM
 MID-FIELD INTERSECTION**

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS

IL PROJECT: PIA-3343
 AIP PROJ: 3-17-0080-XX

FILE: k:\peria\0406103\draw\sheet\014_pp_3.dgn

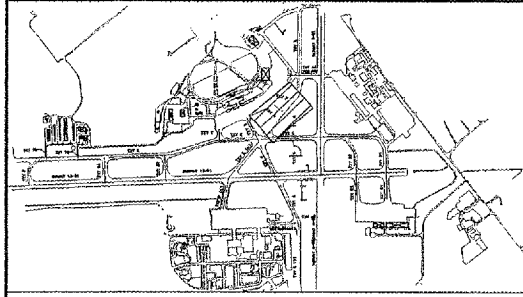
DESIGN BY: TCS
 DRAWN BY: CAG
 CHECKED BY: TCS
 APPROVED BY: CET

DATE: JUNE 24, 2005
 JOB No: 04061-03

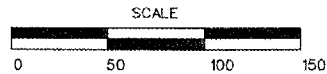
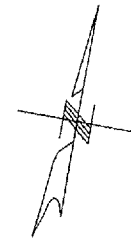
**PLAN AND PROFILE
 SHEET 3 OF 3**

SHEET 14 OF 43 SHEETS

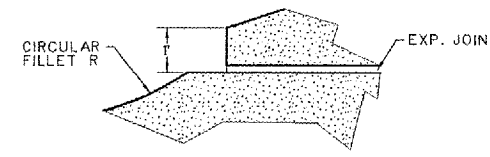
18/23/2005
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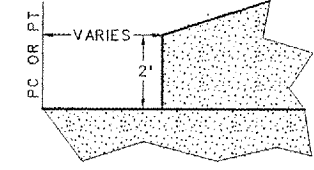
KEY MAP



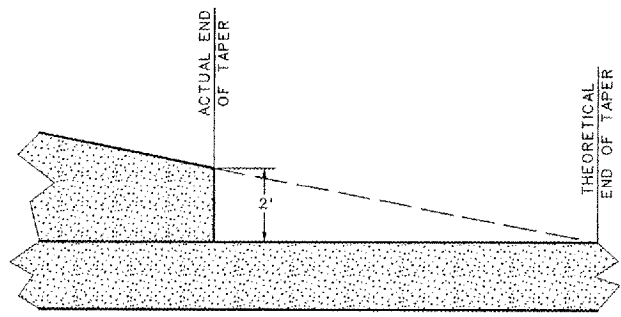
SCALE



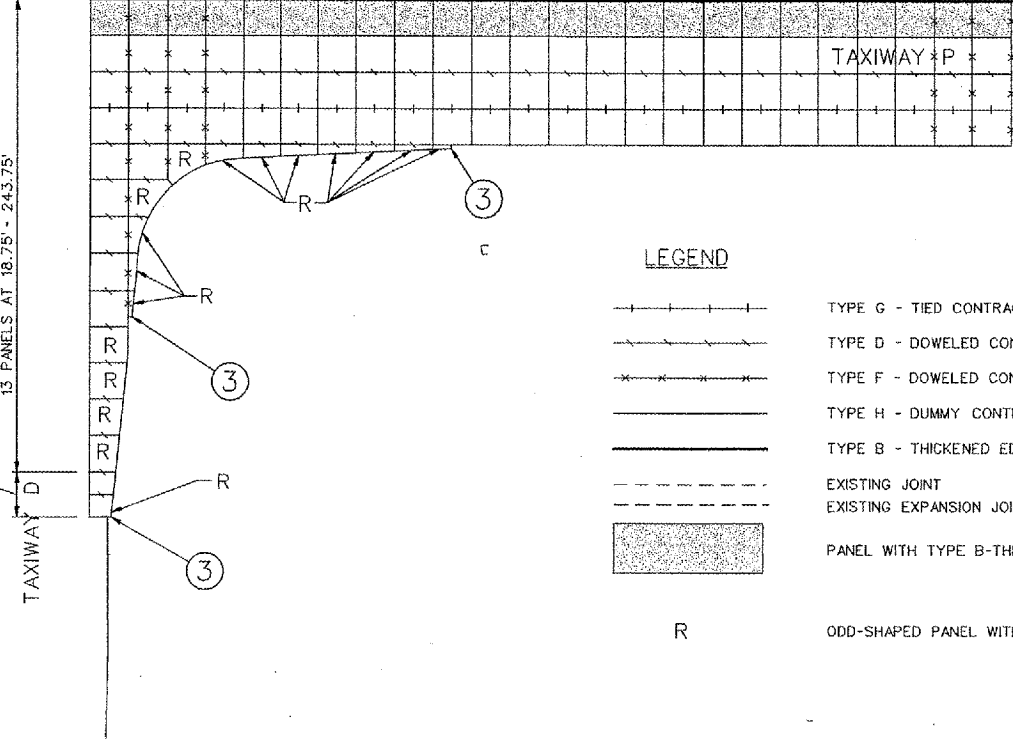
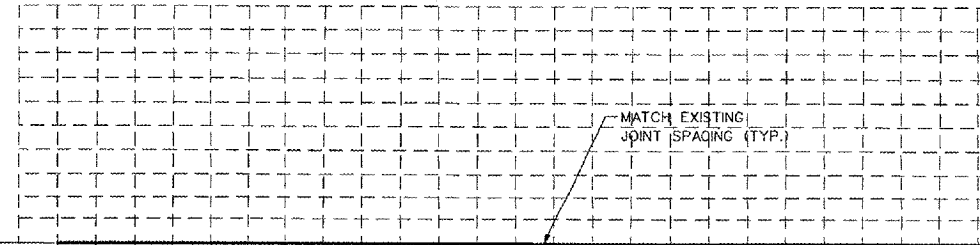
① STUB DETAIL
N.T.S.



② STUB DETAIL
N.T.S.



③ STUB DETAIL
TYPICAL AT END OF TAPER
N.T.S.



LEGEND

- TYPE G - TIED CONTRACTION
- TYPE D - DOWELED CONSTRUCTION
- TYPE F - DOWELED CONTRACTION
- TYPE H - DUMMY CONTRACTION
- TYPE B - THICKENED EDGE
- EXISTING JOINT
- EXISTING EXPANSION JOINT
- PANEL WITH TYPE B-THICKENED EDGE
- R ODD-SHAPED PANEL WITH MESH REINFORCEMENT

PE083

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2'
AT FULL SCALE (34X22).
PLOT 1



RELOCATE TAXIWAY A FROM
MID-FIELD INTERSECTION

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CONSULTING ENGINEERS
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IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

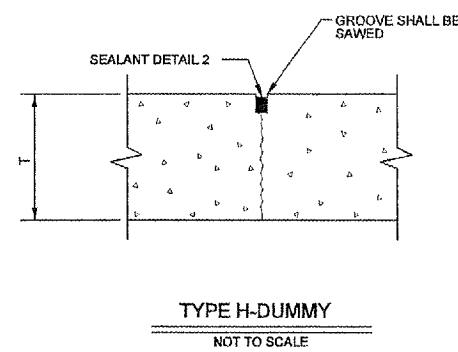
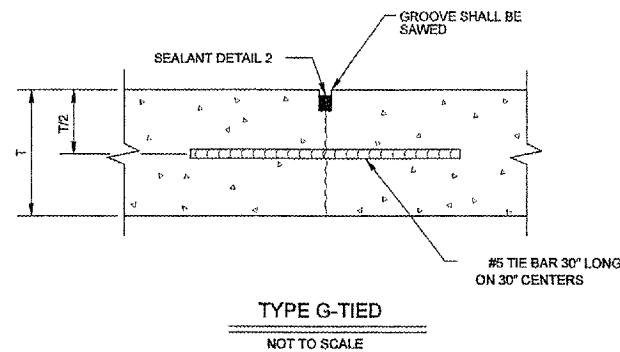
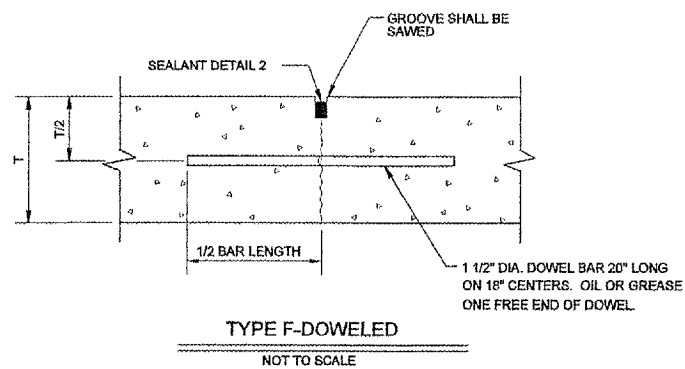
FILE: k:\peoria\0406103\drawsheets\015_joint_plan.d
DESIGN BY: TCS
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET
DATE: JUNE 24, 2005
JOB No: 04061-03

JOINTING PLAN

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).
 PLOT 1

CONTRACTION JOINTS

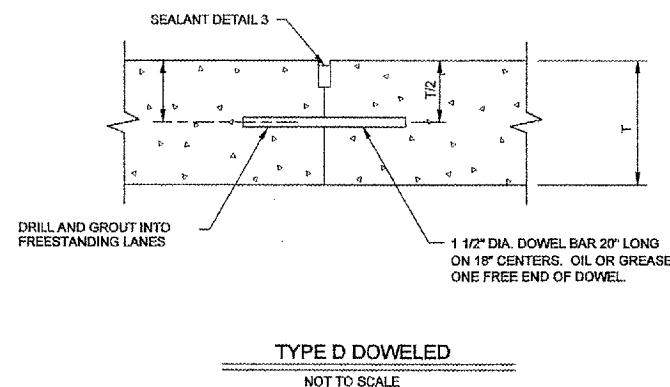


T=18"

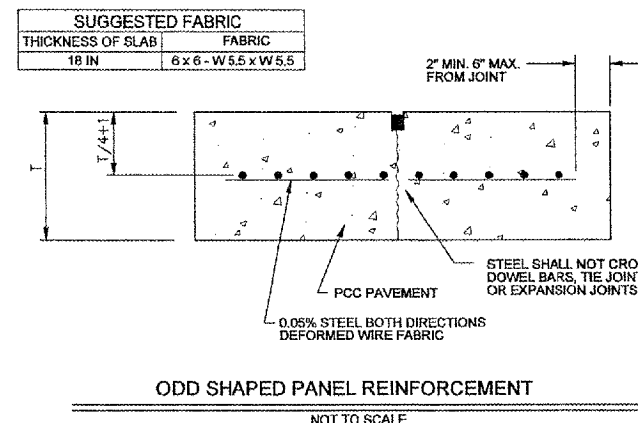
THICKNESS OF SLAB	DIAMETER	LENGTH	SPACING
6-7 IN	3/4 IN	18 IN	12 IN
8-12 IN	1 IN	19 IN	12 IN
13-16 IN	1-1/4 IN	20 IN	15 IN
17-20 IN	1-1/2 IN (1)	20 IN	18 IN
21-24 IN	2 IN (1)	24 IN	18 IN

(1) DOWELS NOTED MAY BE A SOLID BAR OR HIGH-STRENGTH PIPE. HIGH STRENGTH PIPE DOWELS MUST BE PLUGGED ON EACH END WITH A TIGHT-FITTING PLASTIC CAP OR WITH BITUMINOUS OR MORTAR MIX.

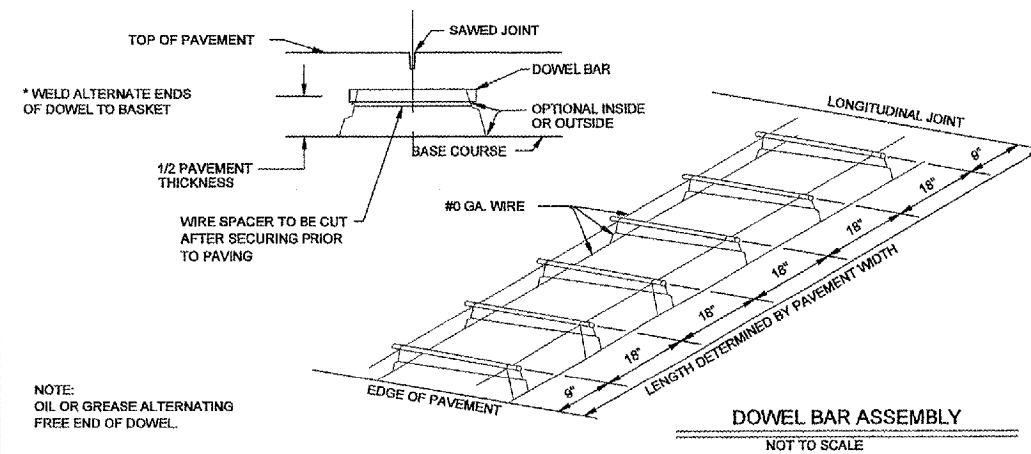
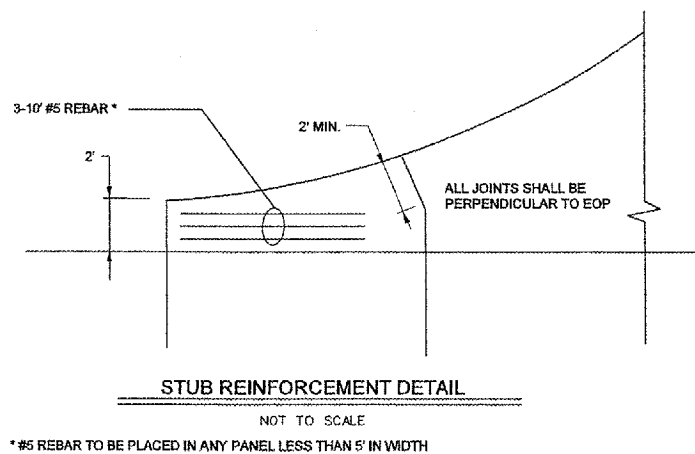
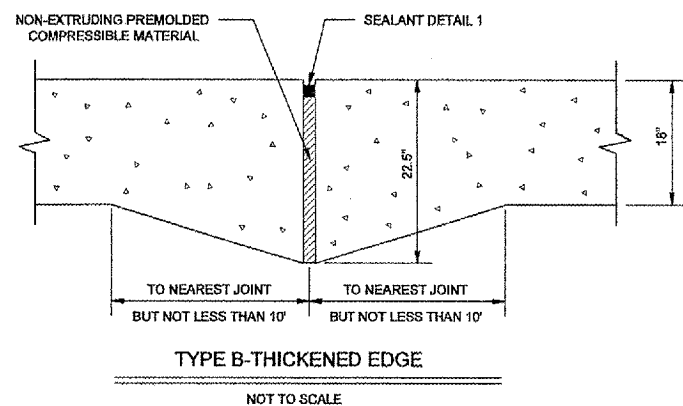
CONSTRUCTION JOINT



REINFORCED PANELS

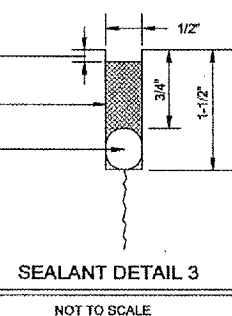
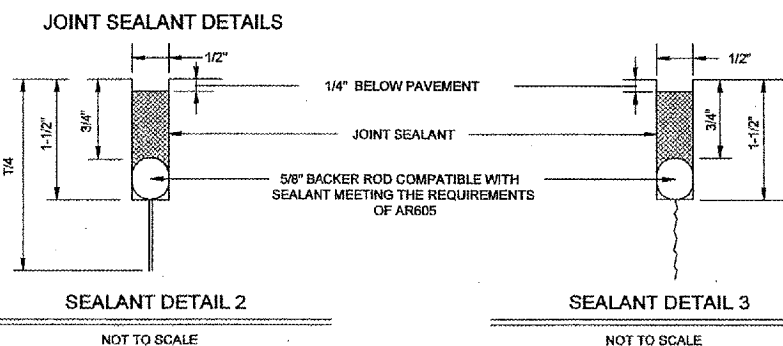
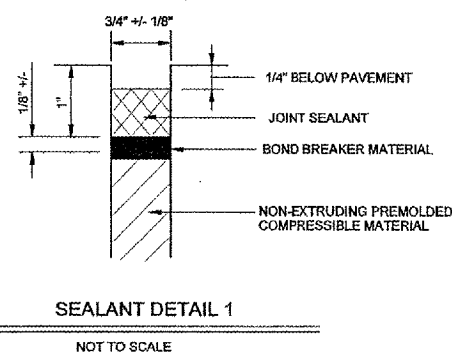


EXPANSION JOINT

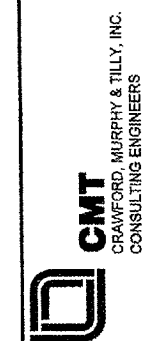


JOINTING NOTES

- ALL EDGES OF NEW SLABS; FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/4", TO FACILITATE SAWING OF THE SEALANT RESERVOIR.
- THE INITIAL SAW CUT FOR ALL TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL DOWEL BARS IN TRANSVERSE JOINTS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL INSURE THAT THEY REMAIN PARALLEL TO THE PAVEMENT LANES. SHOP DRAWING SUBMITTAL OF THE DOWEL BAR ASSEMBLIES SHALL BE MADE PRIOR TO INSTALLATION.
- ALL REINFORCEMENT AND/OR FABRIC SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- THE INITIAL SAW CUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAW CUT WILL NOT BE ALLOWED.
- TYPE D DOWELED CONSTRUCTION JOINT SHALL BE INSTALLED FOR ALL LONGITUDINAL JOINTS AND WHERE PAVING OPERATIONS ARE DELAYED OR STOPPED.



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION

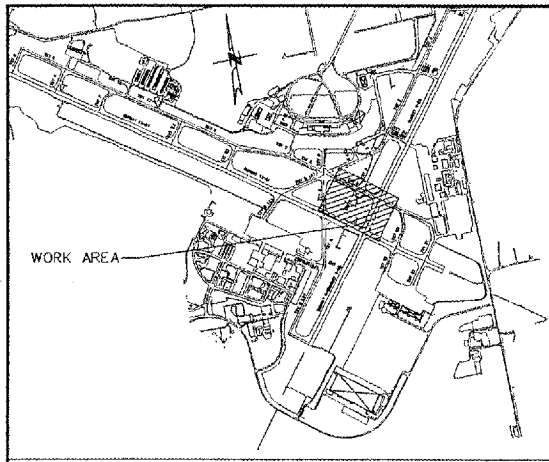


IL PROJECT: PIA-3343
 AIP PROJ: 3-17-0080-XX

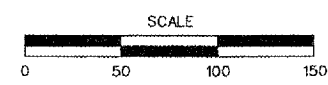
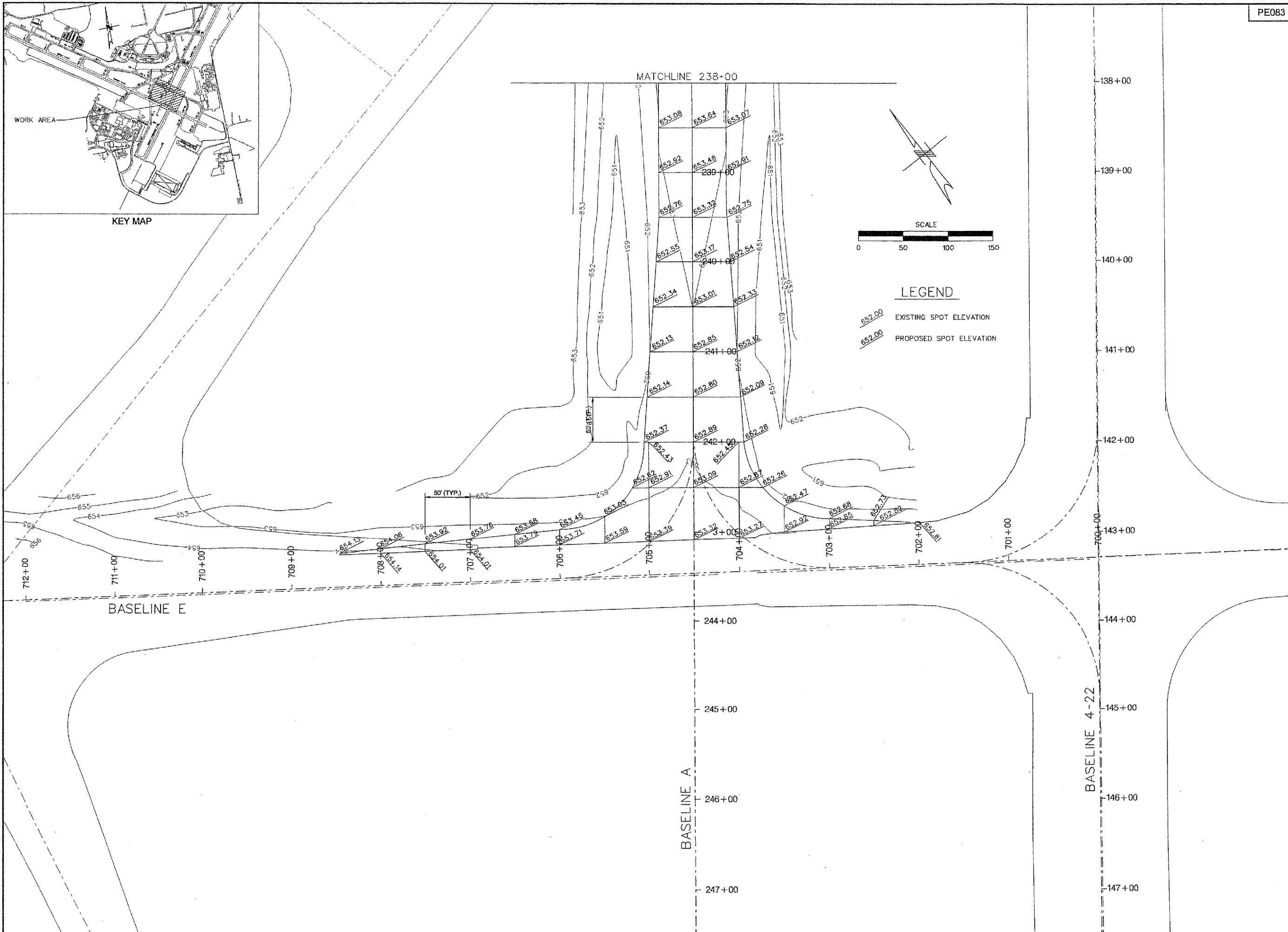
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DESIGN BY:	TCS
DRAWN BY:	CAG
CHECKED BY:	TCS
APPROVED BY:	CET
DATE:	JUNE 24, 2005
JOB No:	04061-03

JOINTING DETAILS



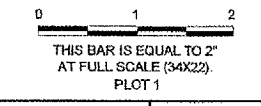
KEY MAP



LEGEND

- 652.00 EXISTING SPOT ELEVATION
- 652.00 PROPOSED SPOT ELEVATION

REVISIONS		
NUMBER	BY	DATE



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RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION

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CONSULTING ENGINEERS

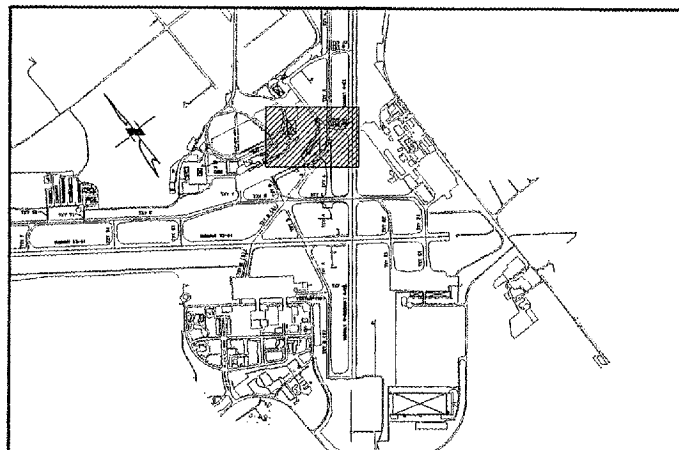
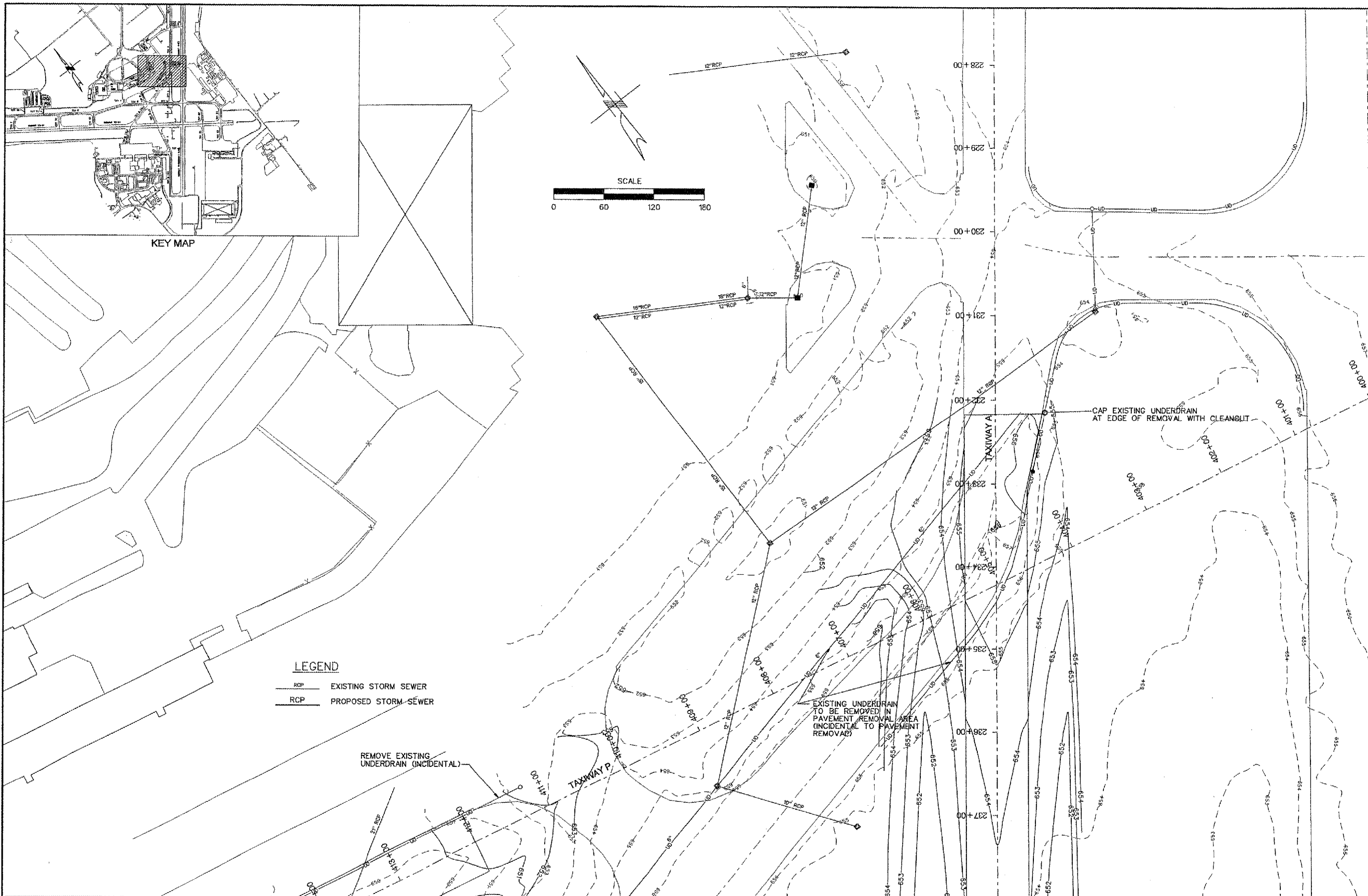
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IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX
FILE: k:\peoria\0406103\drawsheets\018_staking_pl
DESIGN BY: BWG
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET
DATE: JUNE 24, 2005
JOB No: 04061-03

STAKING PLAN
SHEET 2 OF 2

SHEET 18 OF 43 SHEETS

06/23/2005 k:\peoria\0406103\drawsheets\018_staking_plan_2.dgn



KEY MAP

LEGEND
 --- RCP --- EXISTING STORM SEWER
 --- RCP --- PROPOSED STORM SEWER

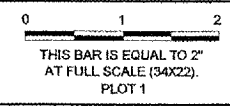
REMOVE EXISTING UNDERDRAIN (INCIDENTAL)

EXISTING UNDERDRAIN TO BE REMOVED IN PAVEMENT REMOVAL AREA (INCIDENTAL TO PAVEMENT REMOVAL)

CAP EXISTING UNDERDRAIN AT EDGE OF REMOVAL WITH CLEANOUT

MATCHLINE 238+00

PE083	REVISIONS		
	NUMBER	BY	DATE



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RELOCATE TAXIWAY A FROM
MID-FIELD INTERSECTION

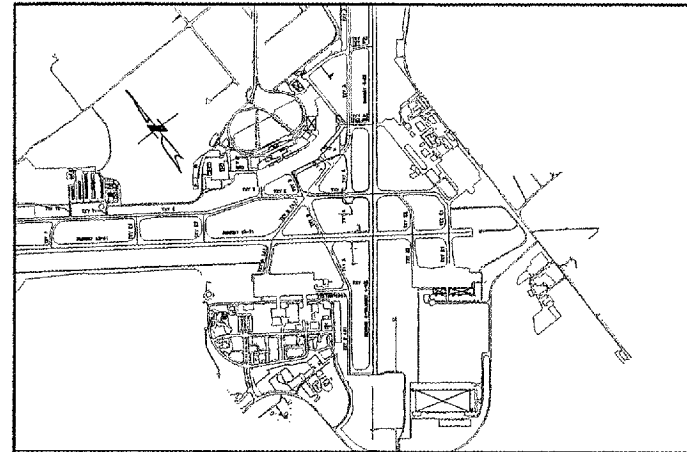
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 CONSULTING ENGINEERS
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IL PROJECT: PIA-3343
 AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\drawsheets\019_grade_drai
DESIGN BY: BWG
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET
DATE: JUNE 24, 2005
JOB No: 04061-03

**GRADING AND
 DRAINAGE PLAN
 SHEET 1 OF 2**
SHEET 19 OF 43 SHEETS

06/23/2005
 k:\peoria\0406103\drawsheets\019_grade_drai_plan_1.dgn



KEY MAP

UNDERDRAIN STRUCTURE SCHEDULE

STRUCTURE	STATION	OFFSET	COORDINATES	
			NORTHING	EASTING
INLET 5	240+75.80	83.40 RT	1455661.56	2429360.33
INLET 6	240+98.21	95.36 LT	1455523.19	2429486.72
INLET 9	240+66.48	87.03 RT	1455661.12	2429363.25

- NOTES:
1. DRAINAGE STRUCTURE RM AND INVERT ELEVATION ARE GIVEN IN DRAINAGE DETAILS. (SHEET 20-24)
 2. INLET/MANHOLE NUMBERING MAY NOT BE SEQUENTIAL.

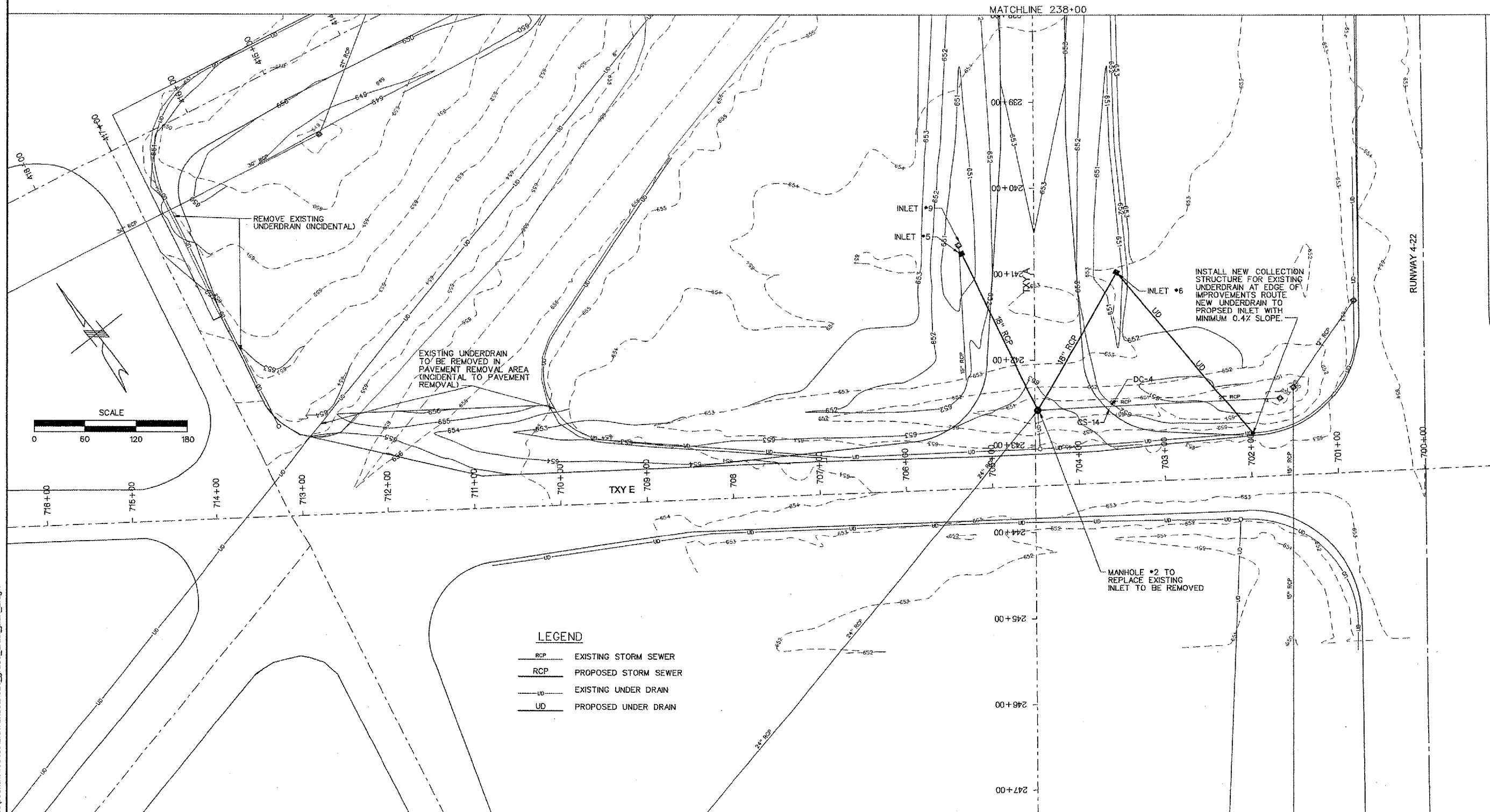
PE083

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2' AT FULL SCALE (34X22).
PLOT 1

NOTE: THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY CONFLICTS FOUND BETWEEN EXISTING INLET #9 AND PROPOSED INLET #5.



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION



IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\drawsheets\020_grad_drai

DESIGN BY: BWG

DRAWN BY: CAG

CHECKED BY: TCS

APPROVED BY: CET

DATE: JUNE 24, 2006

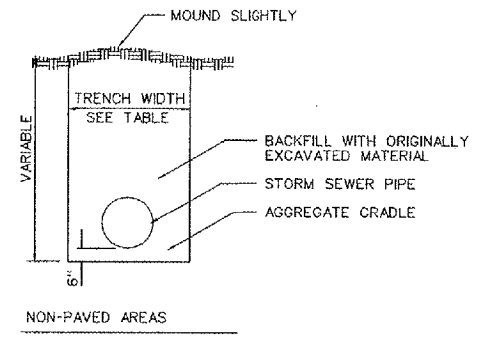
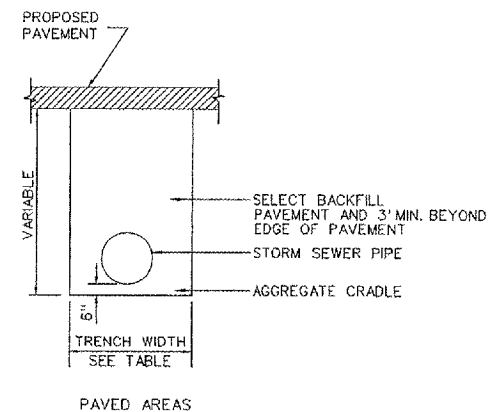
JOB No: 04061-03

GRADING AND DRAINAGE PLAN
SHEET 2 OF 2

SHEET 20 OF 43 SHEETS

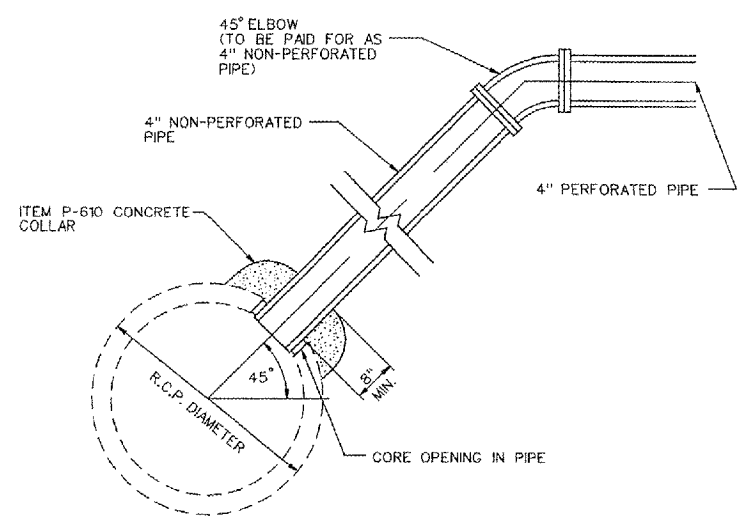
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2' AT FULL SCALE (34X22).
PLOT 1

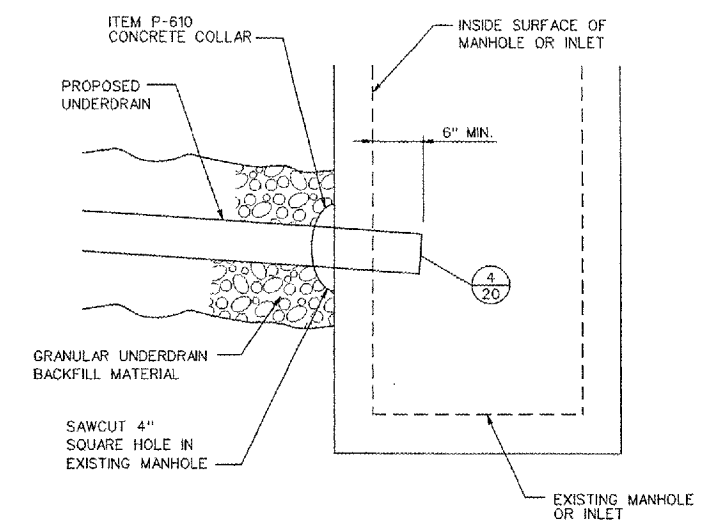


INSIDE DIAMETER OF SEWER (INCHES)	MAXIMUM TRENCH WIDTH
6	3'-2"
8	3'-2"
10	3'-2"
12	3'-4"
15	3'-6"
18	3'-10"
21	4'-4"
24	4'-8"
27	4'-11"
30	5'-3"
33	6'-0"
36	6'-4"
42	6'-11"
48	7'-6"
54	8'-7"
60	9'-2"
66	9'-9"
72	10'-4"
78	10'-11"
84	11'-6"
90	12'-1"
96	12'-8"
102	13'-3"
108	13'-10"

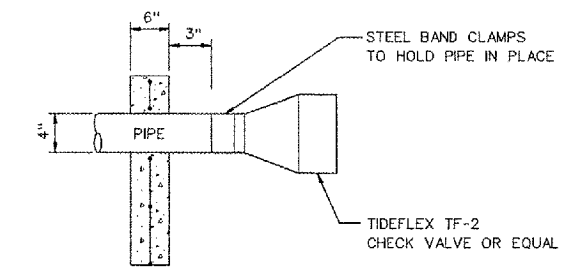
STORM SEWER TRENCH DETAILS
NOT TO SCALE



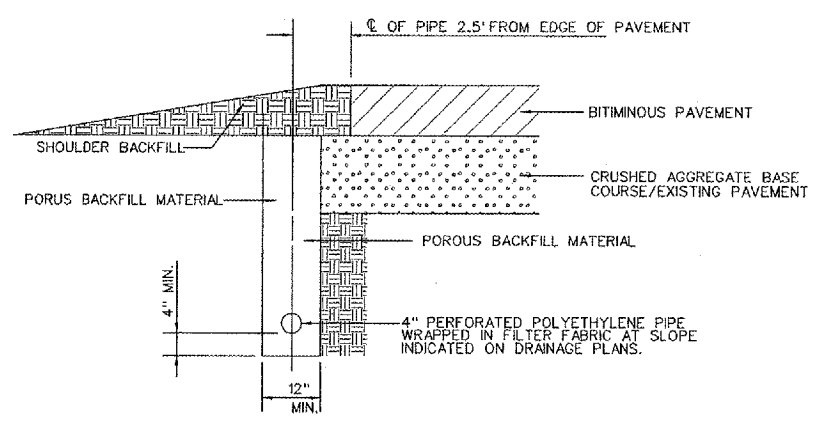
2 UNDERDRAIN DIRECT CONNECTION DETAIL
NOT TO SCALE



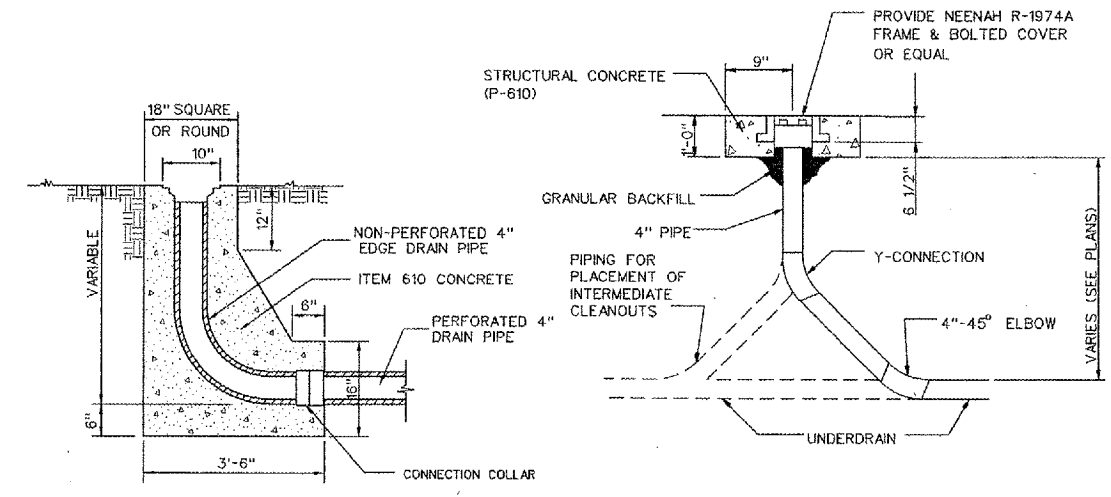
3 DIRECT CONNECTION DETAIL
NOT TO SCALE



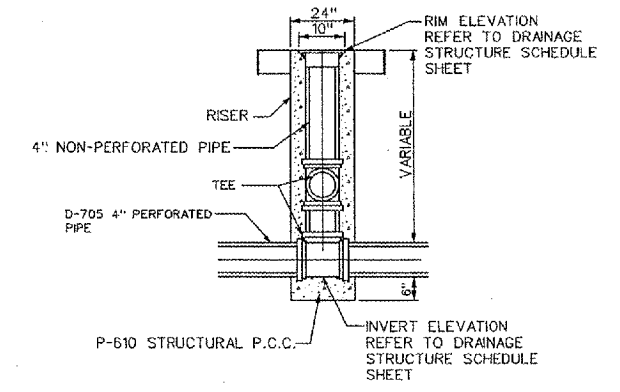
4 UNDERDRAIN END SECTION
NOT TO SCALE



UNDERDRAIN DETAIL
NOT TO SCALE



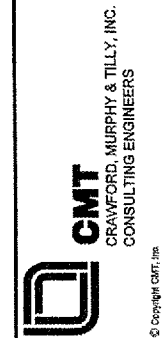
UNDERDRAIN CLEANOUT (CO) DETAIL
NOT TO SCALE



UNDERDRAIN COLLECTION STRUCTURE
NOT TO SCALE



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION



IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

DESIGN BY: TCS
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET
DATE: JUNE 24, 2005
JOB No: 04081-03

DRAINAGE DETAILS
SHEET 1 OF 5

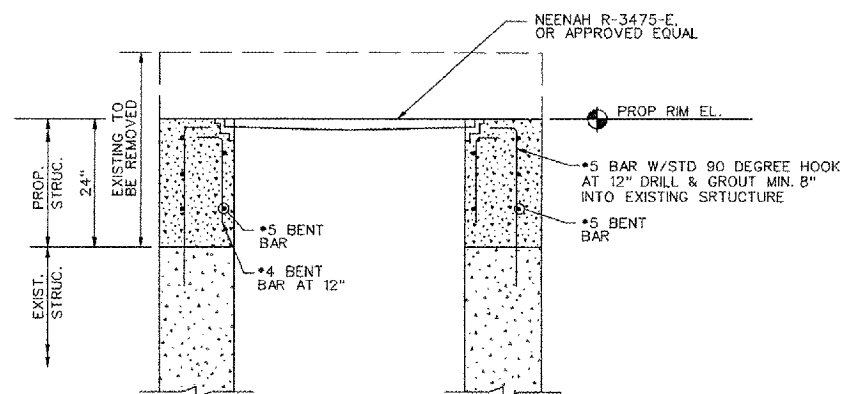
SHEET 21 OF 43 SHEETS

NUMBER	BY	DATE

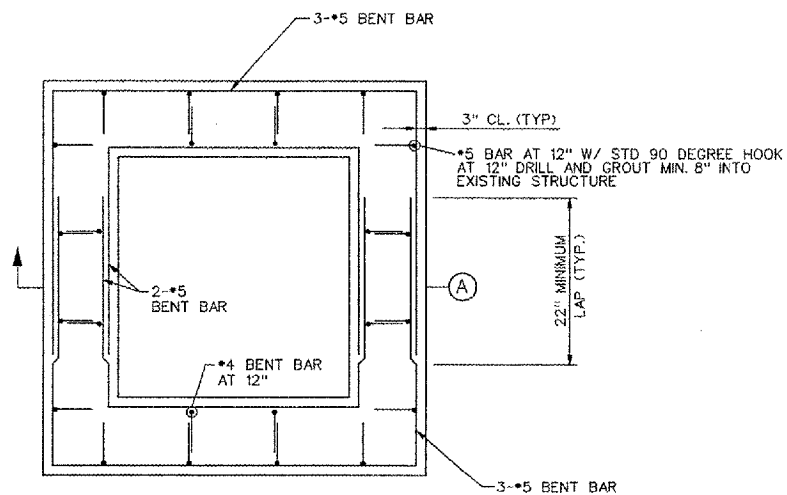
STRUCTURE	EXISTING RIM EL.	PROPOSED RIM EL.
INLET *9	653.16	650.50

GENERAL NOTES:

1. CONCRETE SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 4000 PSI.
2. ALL REINFORCEMENT SHALL CONFORM TO ASTM-A615, GRADE 60.
3. ALL CONCRETE WORK SHALL CONFORM TO ACI-318-02.
4. SAWCUT EXIST. CONCRETE AT LOCATIONS OF PROPOSED STRUCTURE.
5. EXPOSED TOP SURFACE TO RECEIVE A BROOM FINISH.
6. REMOVE THE EXISTING TOP TO A DEPTH OF 24" BELOW THE PROPOSED TOP GRADE.



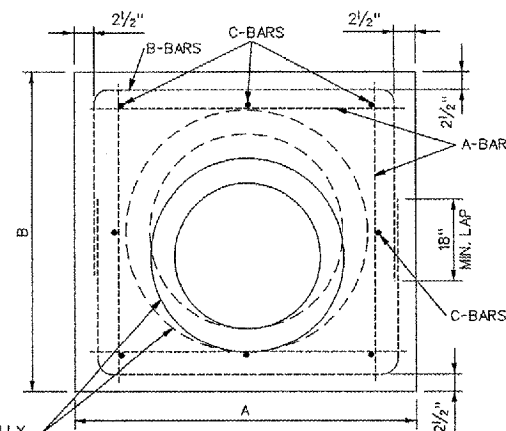
A SECTION



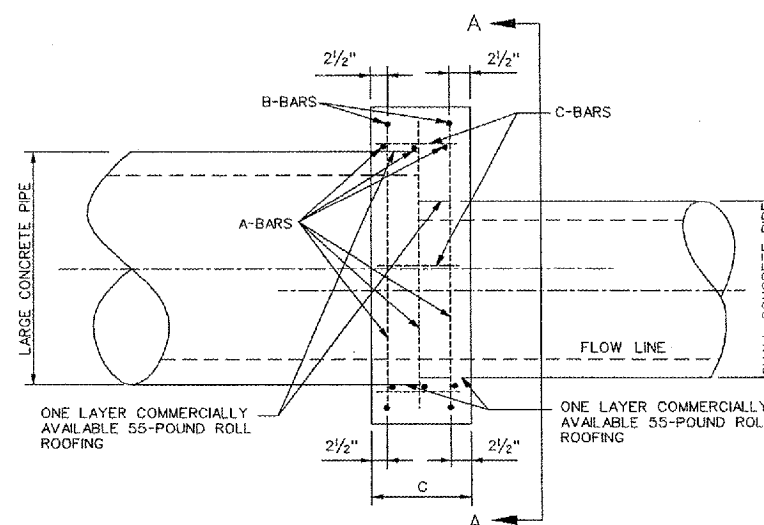
1 INLET ADJUSTMENT DETAIL
NOT TO SCALE

TABLE OF DIMENSIONS

SIZE OF PIPE		DIMENSIONS		LENGTH OF BARS			QUANTITIES	
LARGE IN.	SMALL IN.	A & B FT-IN	C FT-IN	A(*5) 12 REQ'D FT-IN	B(*6) 4 REQ'D FT-IN	C(*4) 8 REQ'D FT-IN	CONC. CU.YD.	STEEL LBS.
12	12	2-8	1-0	2-5	5-10	0-8	0.21	70
15	15	3-0	1-0	2-9	6-6	0-9	0.27	77
18	18	3-3	1-0	3-0	7-0	0-9	0.33	84
21	21	3-6	1-0	3-3	7-6	0-9	0.36	90
24	24	3-10	1-0	3-6	8-2	0-9	0.44	97
30	30	4-5	1-4	4-2	9-4	1-0	0.66	114
36	36	5-0	1-4	4-9	10-6	1-0	0.79	128
42	42	5-7	1-4	5-4	11-8	1-0	0.98	142
48	48	6-2	1-4	6-0	12-10	1-0	1.13	158
54	54	7-1	1-8	6-9	14-8	1-6	1.90	181
60	60	7-8	1-8	7-5	15-10	1-6	2.13	196
66	66	8-3	2-0	8-0	17-0	1-9	2.85	210
72	72	8-10	2-0	7-7	18-2	1-9	3.16	225



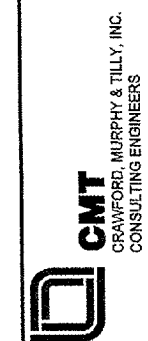
SECTION A-A



ELEVATION
(FOR CONCRETE PIPE TO CONCRETE PIPE)
TYPE A COLLAR



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION



IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\drawings\022_drainage_2.dgn

DESIGN BY: JDJ

DRAWN BY: JSP

CHECKED BY: JDJ

APPROVED BY: CET

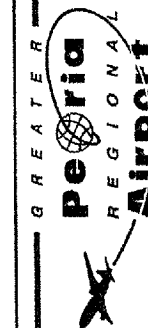
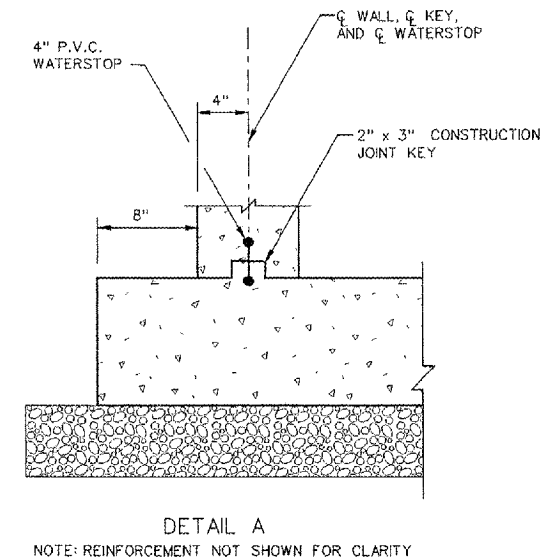
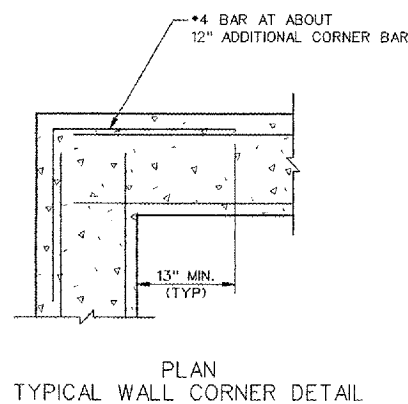
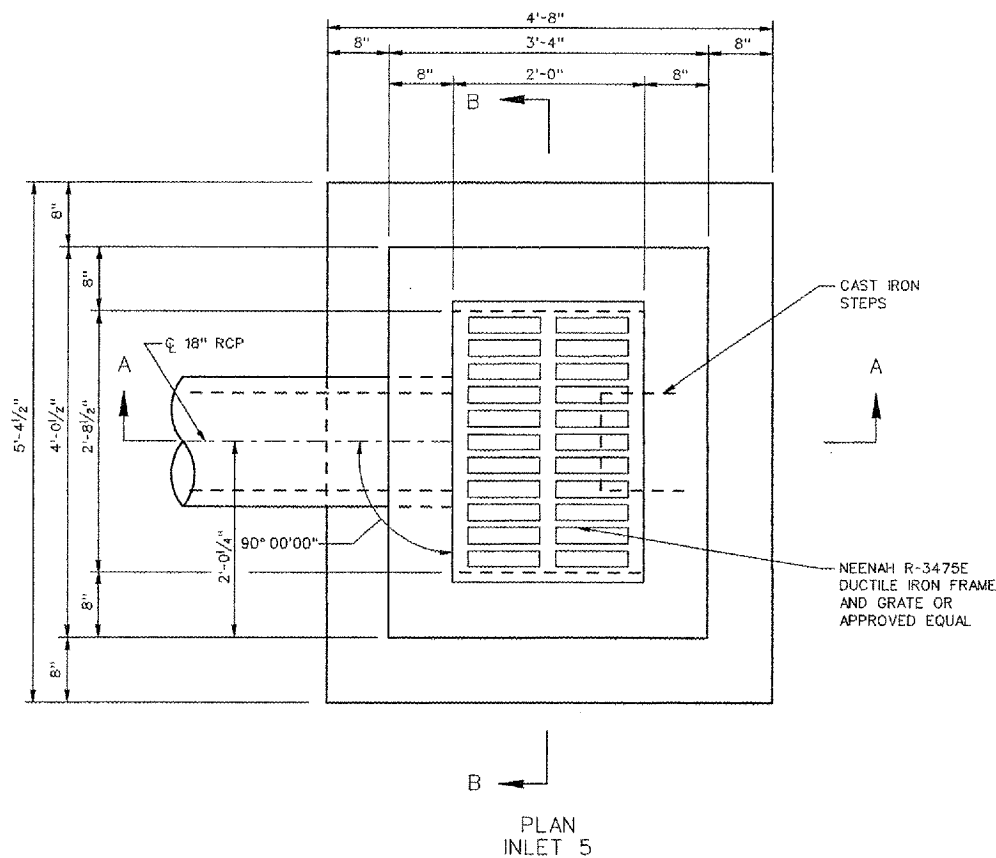
DATE: JUNE 24, 2005

JOB No: 04061-03

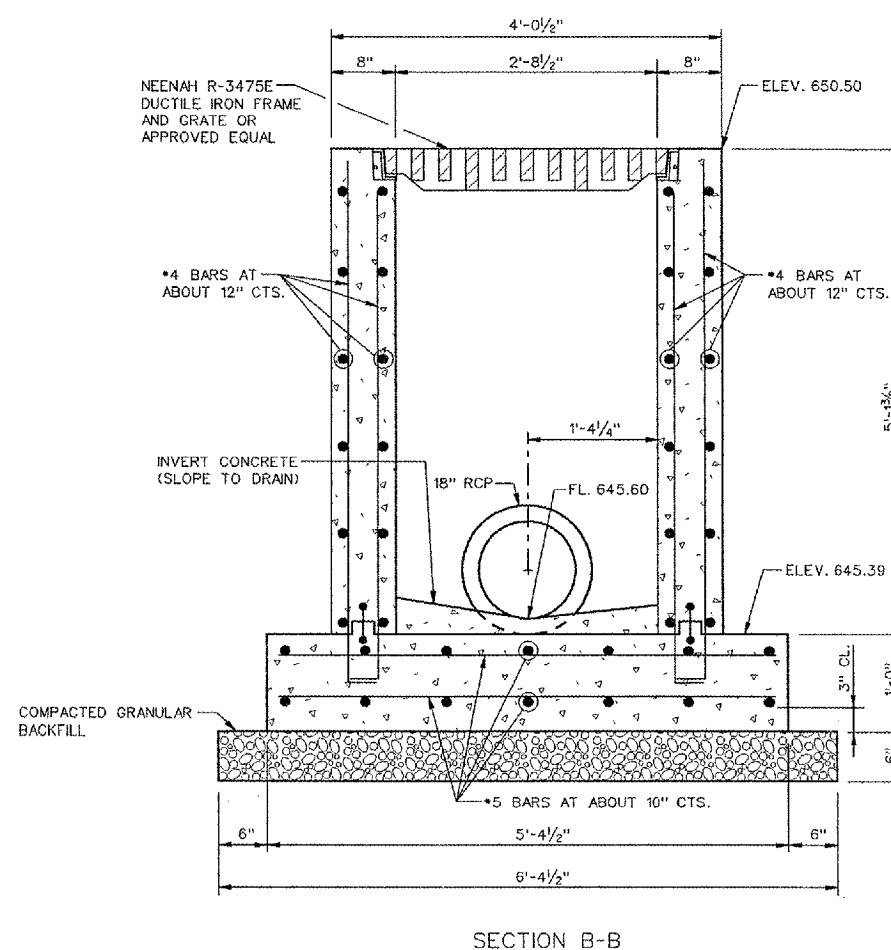
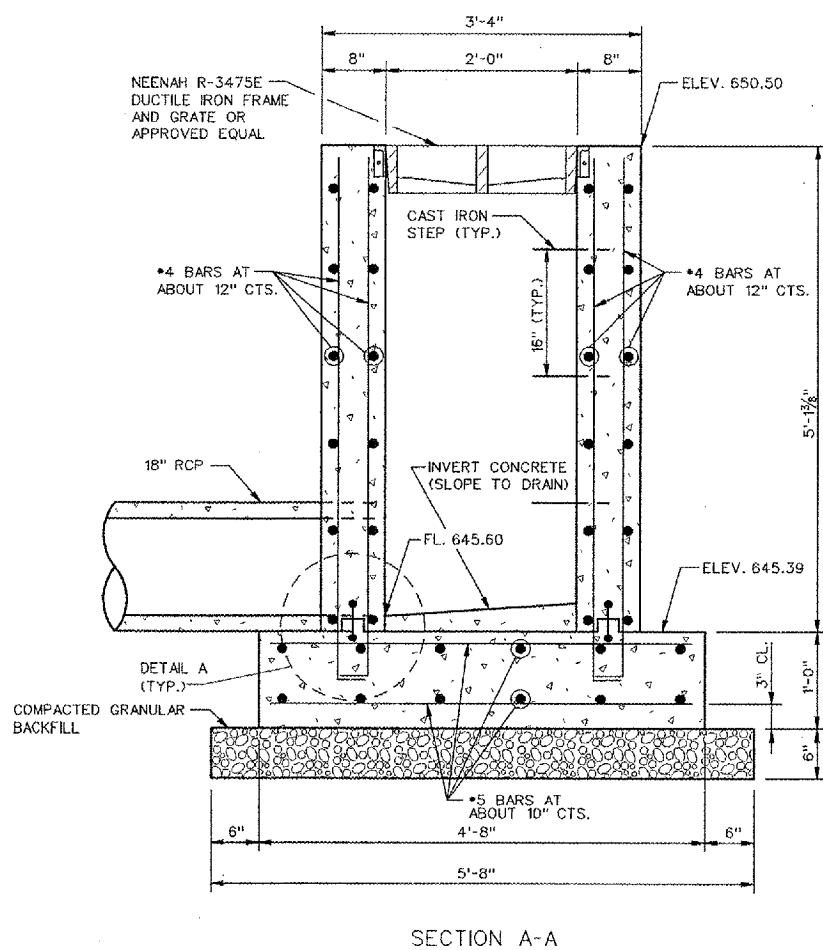
DRAINAGE DETAILS
SHEET 2 OF 5

SHEET 22 OF 43 SHEETS

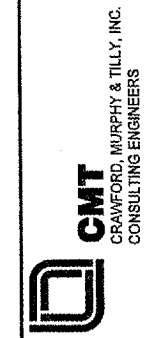
NUMBER	BY	DATE



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION



- NOTES:
1. CAST IN PLACE CONCRETE SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 3000 PSI.
 2. ALL REINFORCEMENT SHALL BE ASTM A615 GRADE 60.
 3. REINFORCING BARS SHALL BE CUT AND/OR BENT AT ALL OPENINGS TO MAINTAIN 1" CLEARANCE FROM PIPES.
 4. MINIMUM CLEARANCE TO REINFORCING STEEL SHALL BE 1/2" UNLESS OTHERWISE SHOWN.
 5. ALL CONCRETE WORK SHALL CONFORM TO ACI-318-02.



IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

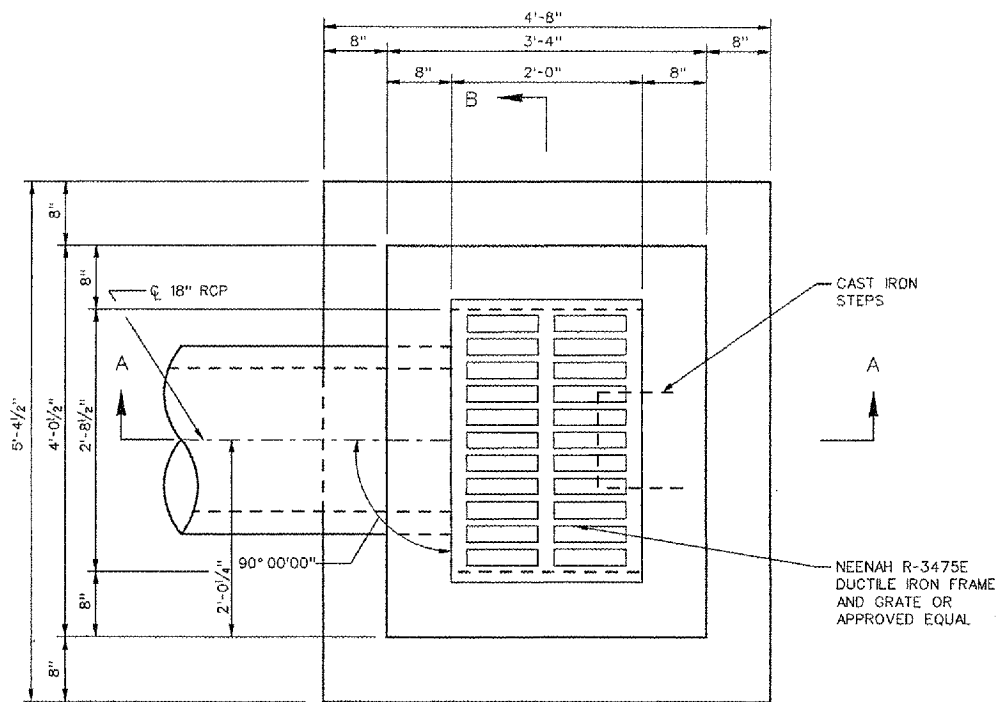
DESIGN BY:	JDJ
DRAWN BY:	JSP
CHECKED BY:	JDJ
APPROVED BY:	CET
DATE:	JUNE 24, 2005
JOB No:	04061-03

DRAINAGE DETAILS
SHEET 3 OF 5

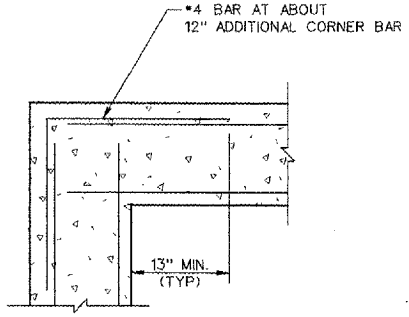
NOTE: THIS DRAWING IS NOT TO SCALE. FOLLOW DIMENSIONS.

06/23/2005 k:\peoria\0406103\drawsheets\023_drainage_details_7.dgn

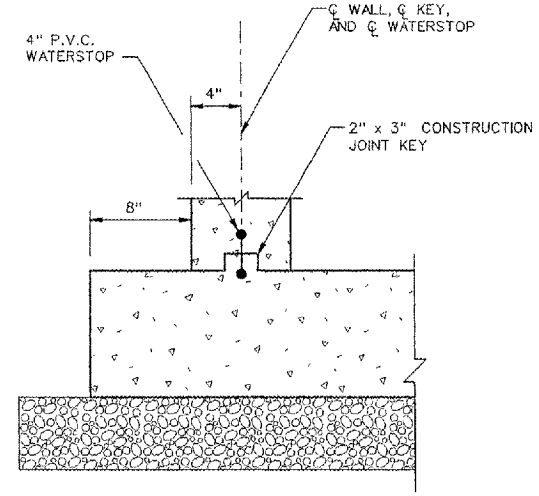
NUMBER	BY	DATE



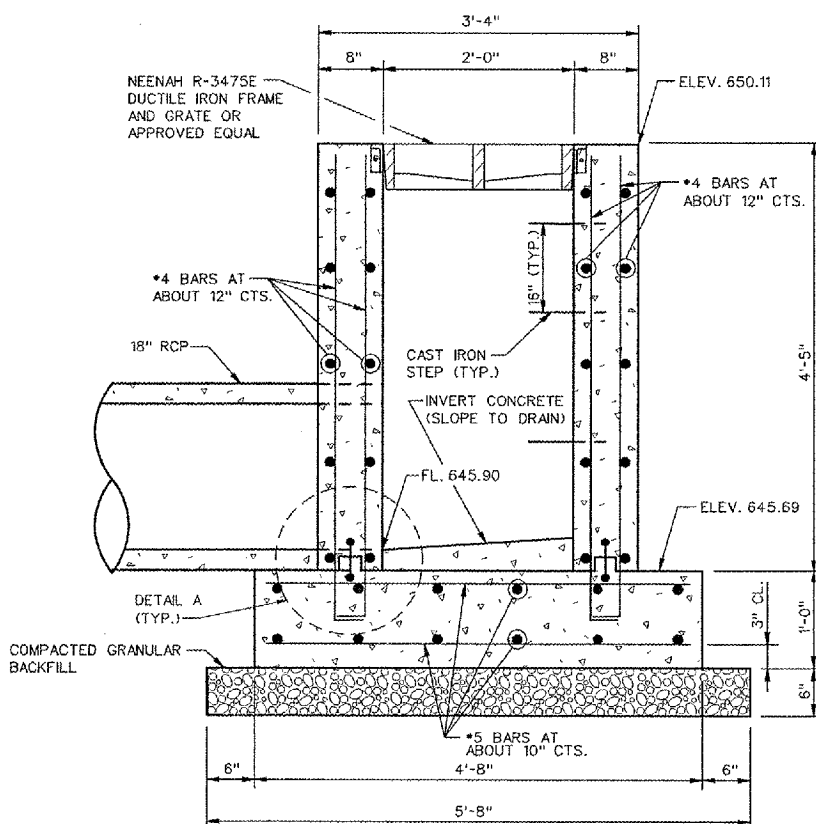
PLAN INLET 6



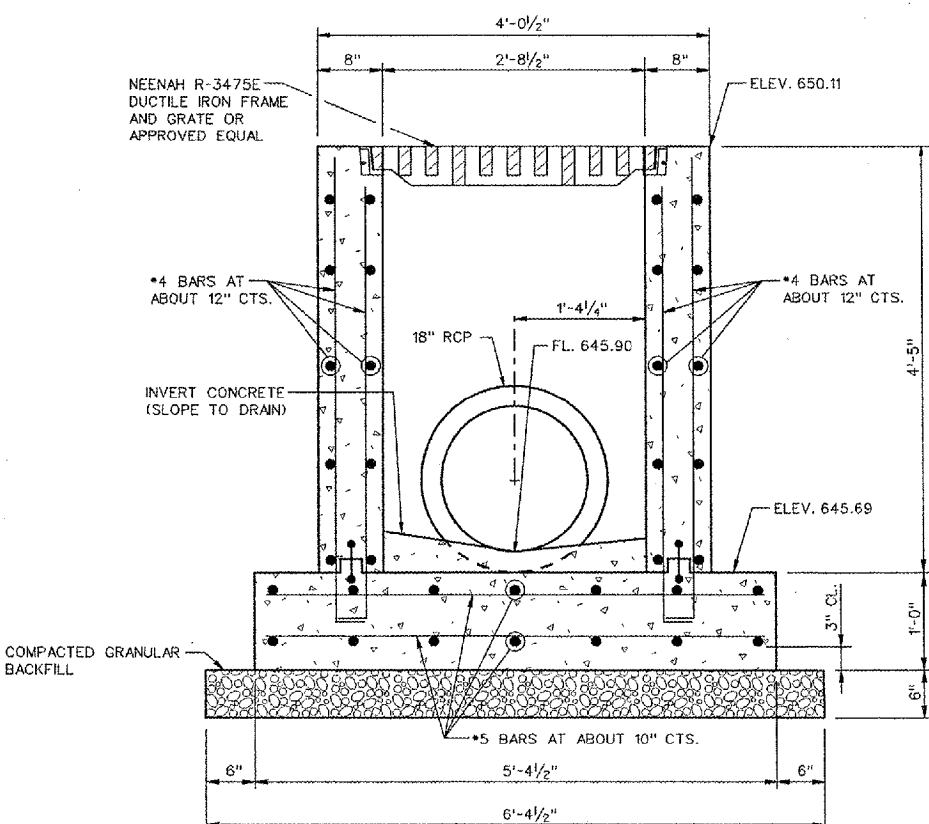
TYPICAL WALL CORNER DETAIL



DETAIL A
NOTE: REINFORCEMENT NOT SHOWN FOR CLARITY



SECTION A-A



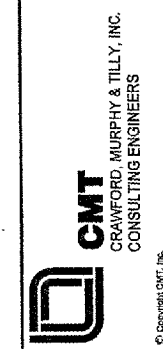
SECTION B-B

- NOTES:
1. CAST IN PLACE CONCRETE SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 3000 PSI.
 2. ALL REINFORCEMENT SHALL BE ASTM A615 GRADE 60.
 3. REINFORCING BARS SHALL BE CUT AND/OR BENT AT ALL OPENINGS TO MAINTAIN 1" CLEARANCE FROM PIPES.
 4. MINIMUM CLEARANCE TO REINFORCING STEEL SHALL BE 1/2" UNLESS OTHERWISE SHOWN.
 5. ALL CONCRETE WORK SHALL CONFORM TO ACI-318-02.

NOTE: THIS DRAWING IS NOT TO SCALE. FOLLOW DIMENSIONS.



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION



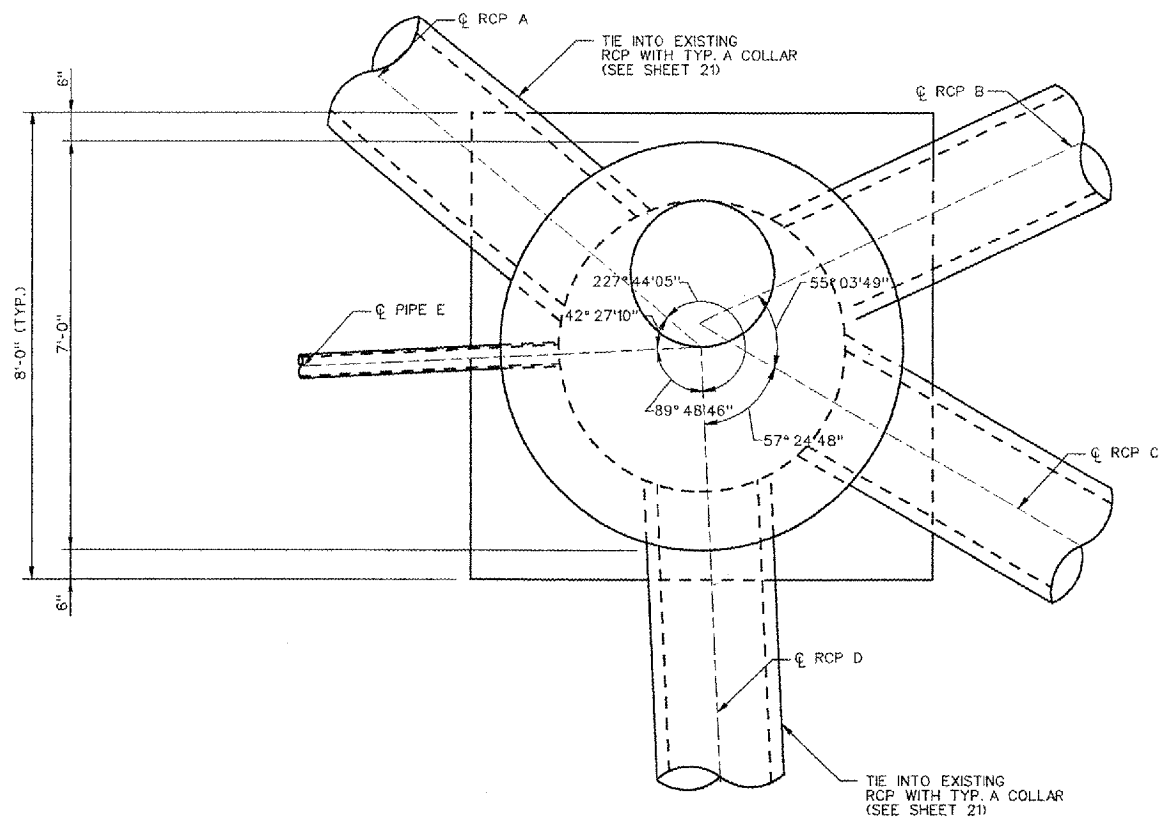
IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\draws\sheet\024_drainage_c
DESIGN BY: JDJ
DRAWN BY: JSP
CHECKED BY: JDJ
APPROVED BY: CET
DATE: JUNE 24, 2005
JOB No: 04061-03

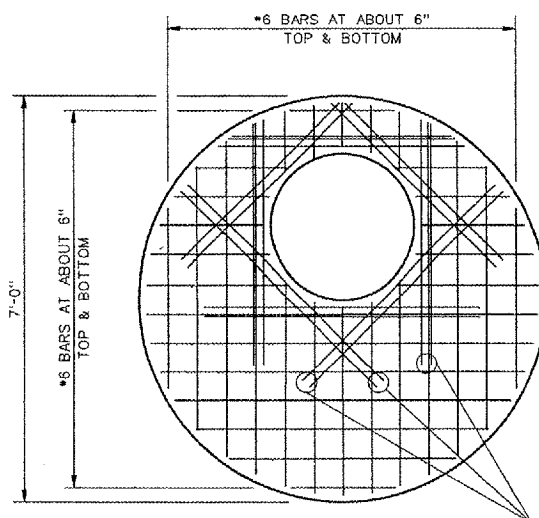
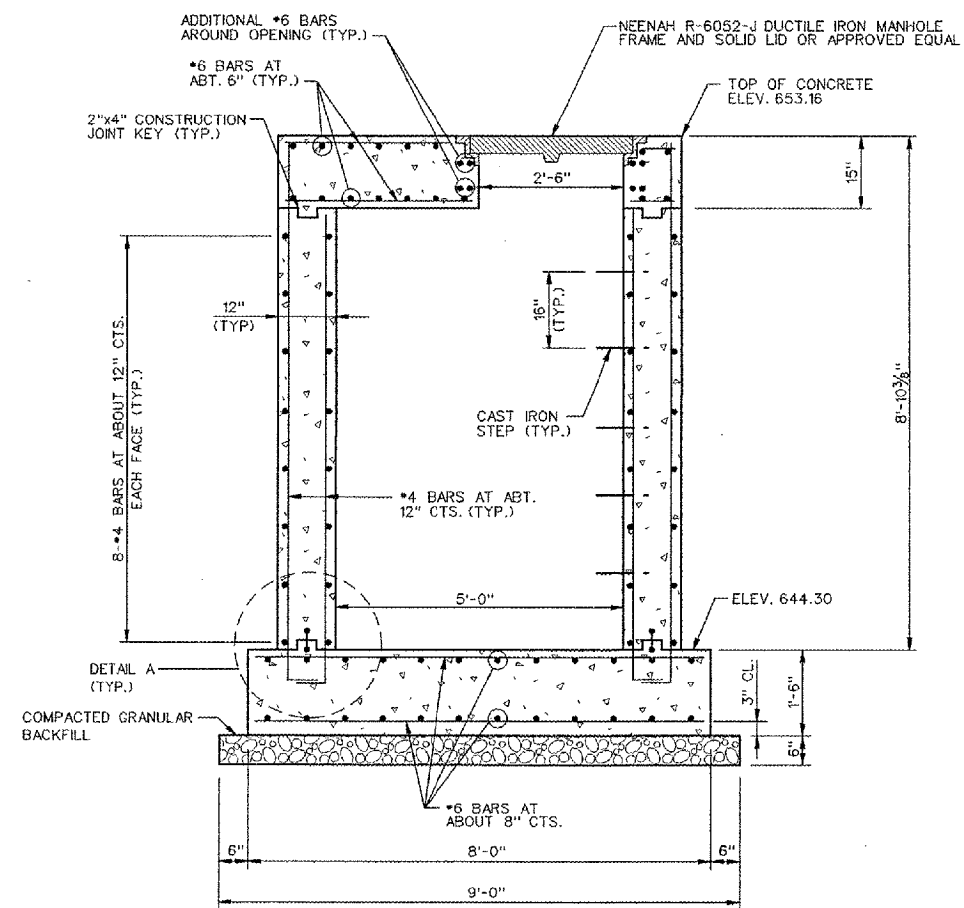
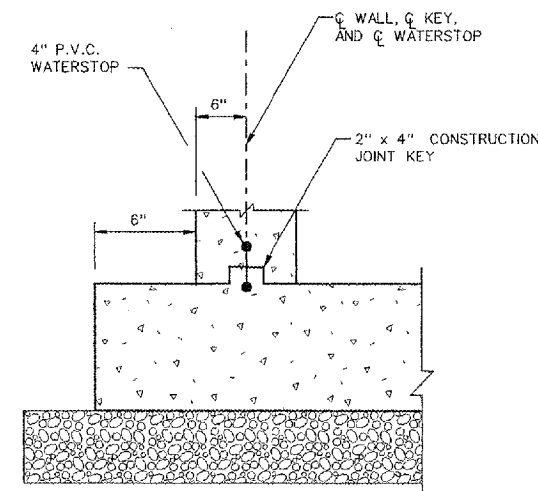
DRAINAGE DETAILS
SHEET 4 OF 5

06/23/2005 k:\peoria\0406103\draws\sheet\024_drainage_details_8.dgn

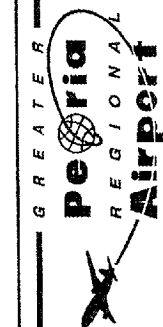
NUMBER	BY	DATE



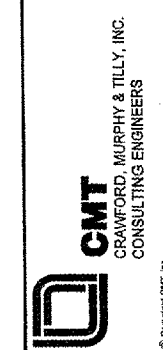
PIPE	SIZE	INVERT ELEV.
A	24"	644.55
B	18"	644.60
C	18"	644.60
D	21"	644.60
E	4"	646.65



- NOTES:
1. CAST IN PLACE CONCRETE SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 4000 PSI.
 2. ALL REINFORCEMENT SHALL BE ASTM A615 GRADE 60.
 3. REINFORCING BARS SHALL BE CUT AND/OR BENT AT ALL OPENINGS TO MAINTAIN 1" CLEARANCE FROM PIPES.
 4. MINIMUM CLEARANCE TO REINFORCING STEEL SHALL BE 1/2" UNLESS OTHERWISE SHOWN.
 5. THE CONTRACTOR SHALL FIELD VERIFY ALL EXISTING PIPE SIZES AND ELEVATIONS PRIOR TO CONSTRUCTION/INSTALLATION OF DRAINAGE STRUCTURES.
 6. PIPE SECTIONS TO TIE EXISTING RCP TO PROPOSED INLET SHALL NOT BE MEASURED FOR PAYMENT AND SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE INLET.
 7. ALL CONCRETE WORK SHALL CONFORM TO ACI-318-02.



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION

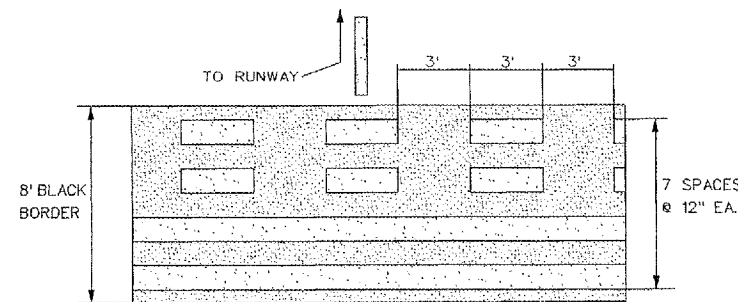
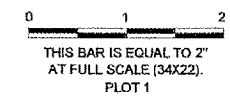


IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0408103\draw\sheets\025_drainage...
DESIGN BY: JDJ
DRAWN BY: JSP
CHECKED BY: JDJ
APPROVED BY: CET
DATE: JUNE 24, 2005
JOB No: 04061-03

DRAINAGE DETAILS
SHEET 5 OF 5

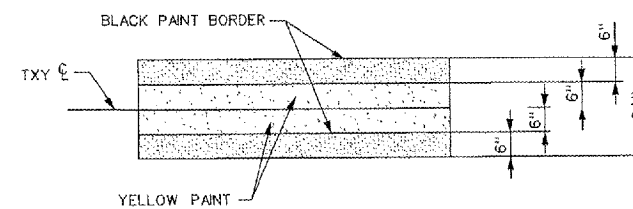
REVISIONS		
NUMBER	BY	DATE



NOTE: HOLD MARKINGS SHALL BE PAINTED YELLOW

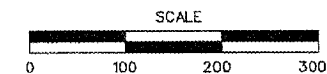
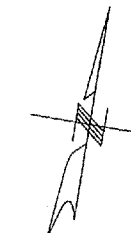
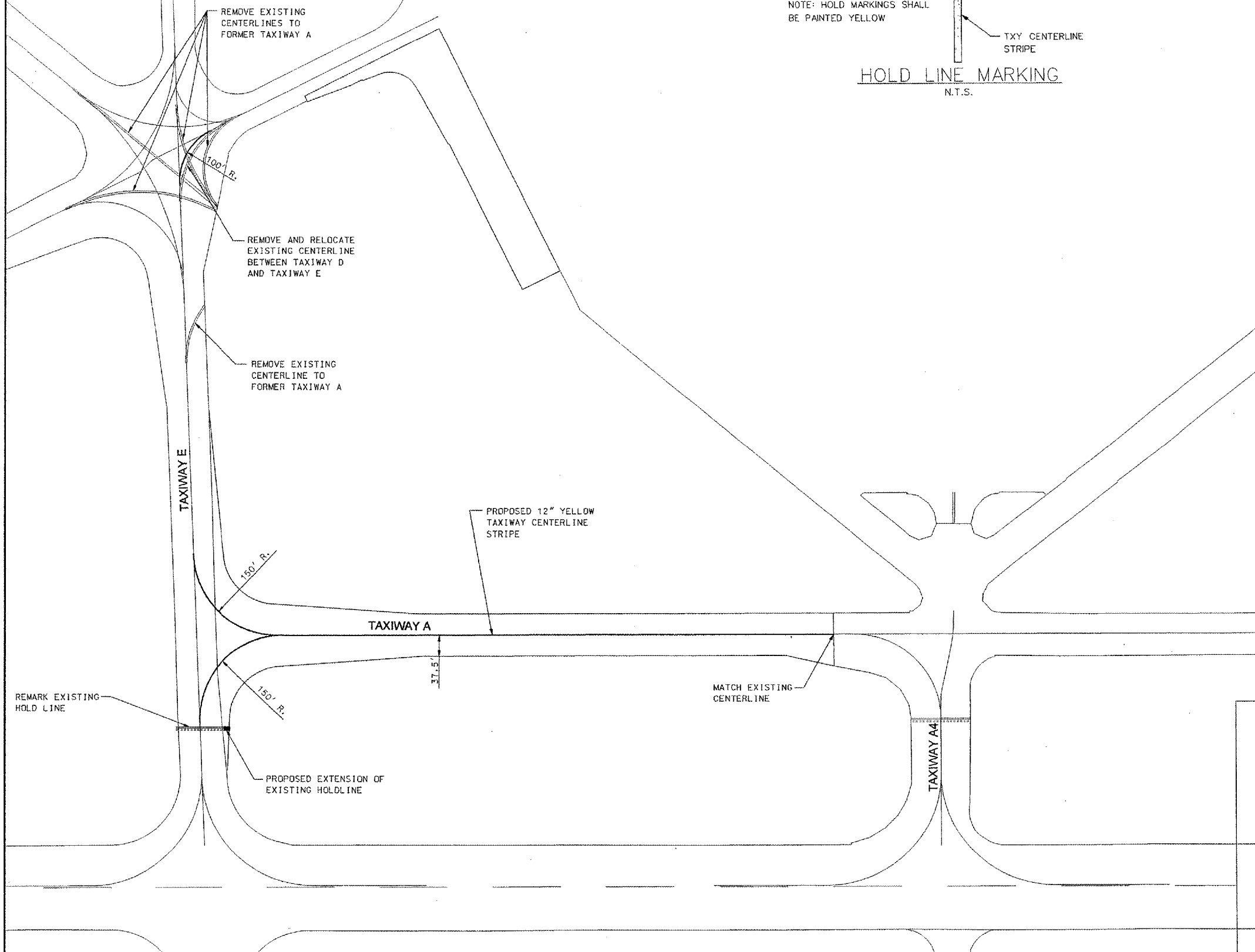
HOLD LINE MARKING

N.T.S.



TAXIWAY CENTERLINE MARKING

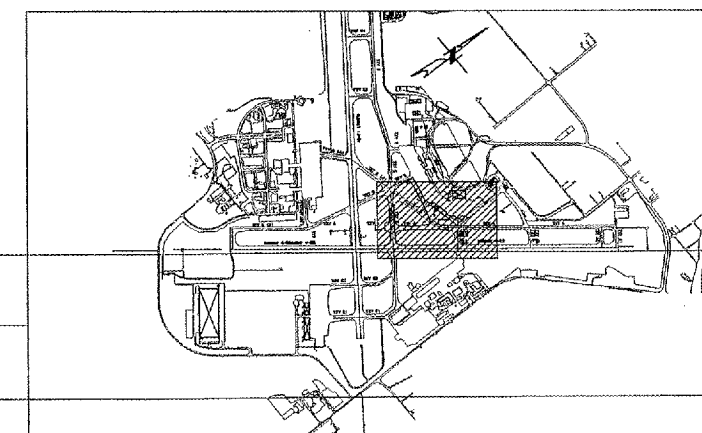
N.T.S.



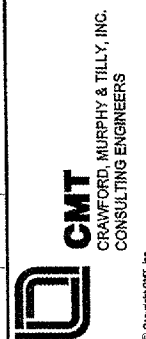
LEGEND

- PROPOSED MARKING
- EXISTING MARKING TO REMAIN
- EXISTING MARKING TO BE REMOVED

KEY MAP



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION

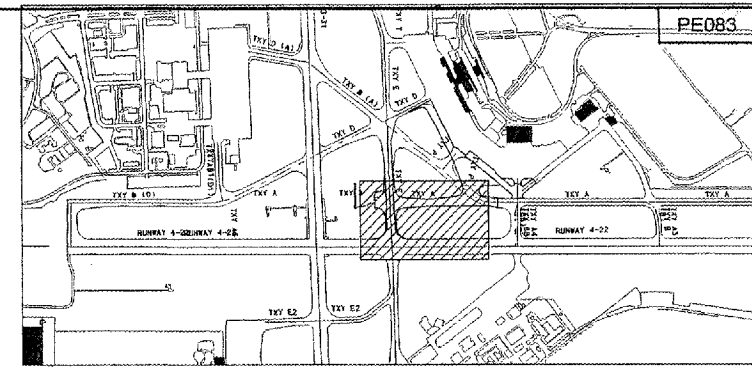
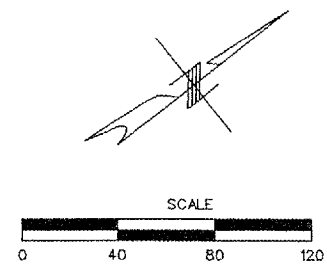
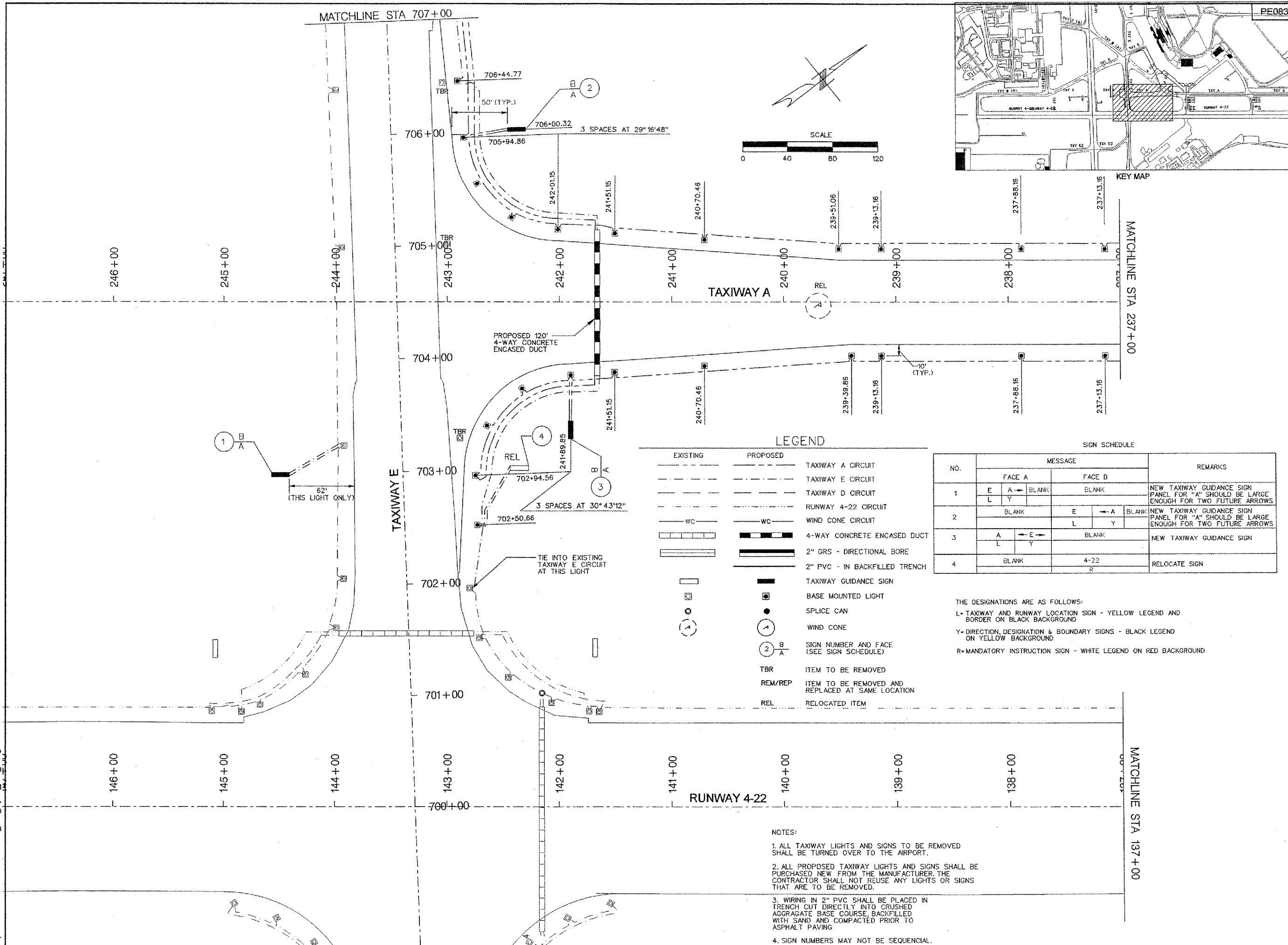


IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\draw\sheet\026_prop.mxd
DESIGN BY: TCS
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET
DATE: JUNE 24, 2006
JOB No: 04061-03

PROPOSED MARKING PLAN

08/23/2005
K:\pcon\0406103\draw\sheet\027_elec_layout_pln_1a.dgn



REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).
PLOT 1

LEGEND

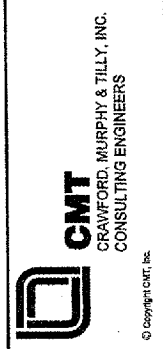
EXISTING	PROPOSED	DESCRIPTION
---	---	TAXIWAY A CIRCUIT
---	---	TAXIWAY E CIRCUIT
---	---	TAXIWAY D CIRCUIT
---	---	RUNWAY 4-22 CIRCUIT
WC	WC	WIND CONE CIRCUIT
▬▬▬	▬▬▬	4-WAY CONCRETE ENCASED DUCT
▬▬▬	▬▬▬	2" GRS - DIRECTIONAL BORE
▬▬▬	▬▬▬	2" PVC - IN BACKFILLED TRENCH
□	■	TAXIWAY GUIDANCE SIGN
○	●	BASE MOUNTED LIGHT
○	○	SPLICE CAN
○	○	WIND CONE
○	○	SIGN NUMBER AND FACE (SEE SIGN SCHEDULE)
TBR		ITEM TO BE REMOVED
REM/REP		ITEM TO BE REMOVED AND REPLACED AT SAME LOCATION
REL		RELOCATED ITEM

SIGN SCHEDULE

NO.	MESSAGE		REMARKS
	FACE A	FACE B	
1	E A → BLANK	BLANK	NEW TAXIWAY GUIDANCE SIGN PANEL FOR "A" SHOULD BE LARGE ENOUGH FOR TWO FUTURE ARROWS
	L Y		
2	BLANK	E → A BLANK	NEW TAXIWAY GUIDANCE SIGN PANEL FOR "A" SHOULD BE LARGE ENOUGH FOR TWO FUTURE ARROWS
		L Y	
3	A → E	BLANK	NEW TAXIWAY GUIDANCE SIGN
	L Y		
4	BLANK	4-22	RELOCATE SIGN
		R	

THE DESIGNATIONS ARE AS FOLLOWS:
 L= TAXIWAY AND RUNWAY LOCATION SIGN - YELLOW LEGEND AND BORDER ON BLACK BACKGROUND
 Y= DIRECTION, DESIGNATION & BOUNDARY SIGNS - BLACK LEGEND ON YELLOW BACKGROUND
 R= MANDATORY INSTRUCTION SIGN - WHITE LEGEND ON RED BACKGROUND

- NOTES:
- ALL TAXIWAY LIGHTS AND SIGNS TO BE REMOVED SHALL BE TURNED OVER TO THE AIRPORT.
 - ALL PROPOSED TAXIWAY LIGHTS AND SIGNS SHALL BE PURCHASED NEW FROM THE MANUFACTURER. THE CONTRACTOR SHALL NOT REUSE ANY LIGHTS OR SIGNS THAT ARE TO BE REMOVED.
 - WIRING IN 2" PVC SHALL BE PLACED IN TRENCH CUT DIRECTLY INTO CRUSHED AGGREGATE BASE COURSE, BACKFILLED WITH SAND AND COMPACTED PRIOR TO ASPHALT PAVING
 - SIGN NUMBERS MAY NOT BE SEQUENTIAL.



IL PROJECT: PIA-3343
 AIP PROJ: 3-17-0080-XX

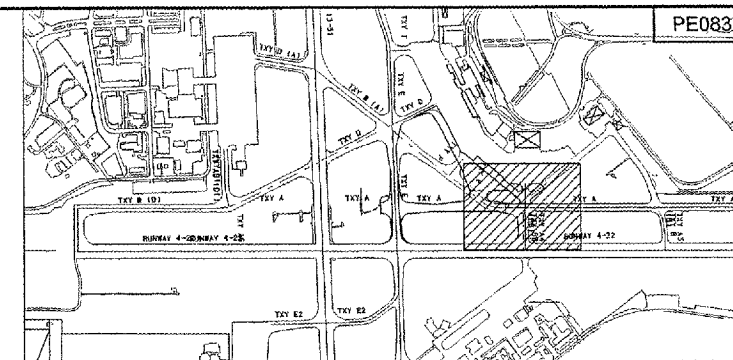
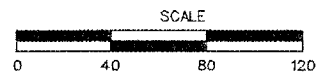
FILE: k:\pcon\0406103\draw\sheet\027_elec_layout_pln_1a.dgn

DESIGN BY: TCS
 DRAWN BY: CAG
 CHECKED BY: TCS
 APPROVED BY: CET
 DATE: JUNE 24, 2005
 JOB No: 04061-03

ELECTRICAL LAYOUT PLAN
SHEET 1 OF 4

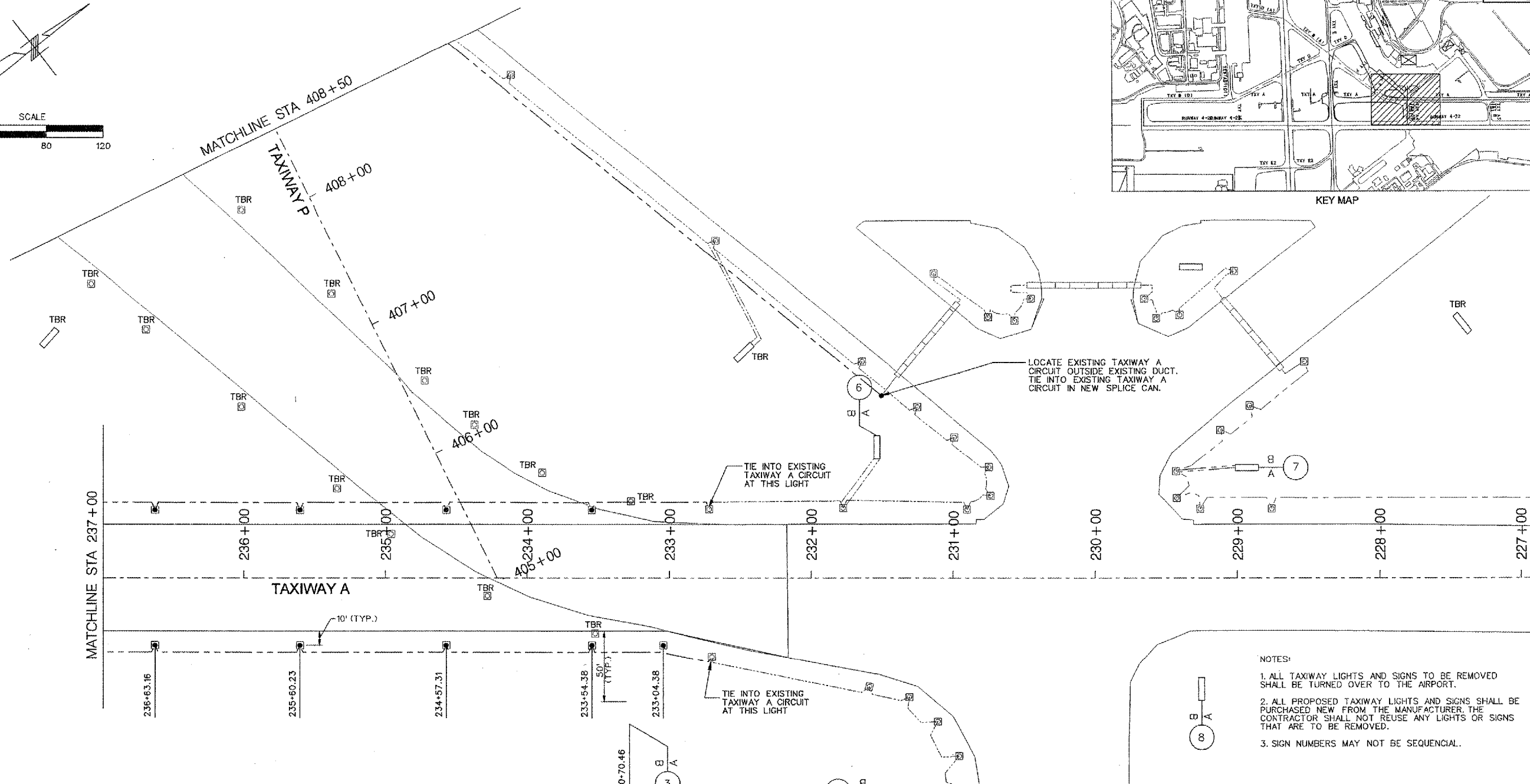
SHEET 27 OF 43 SHEETS

RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION



REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).
PLOT 1



TAXIWAY A4 (BB)

- NOTES:
1. ALL TAXIWAY LIGHTS AND SIGNS TO BE REMOVED SHALL BE TURNED OVER TO THE AIRPORT.
 2. ALL PROPOSED TAXIWAY LIGHTS AND SIGNS SHALL BE PURCHASED NEW FROM THE MANUFACTURER. THE CONTRACTOR SHALL NOT REUSE ANY LIGHTS OR SIGNS THAT ARE TO BE REMOVED.
 3. SIGN NUMBERS MAY NOT BE SEQUENTIAL.

NO.	MESSAGE		REMARKS
	FACE A	FACE B	
4	A4	A4	REPLACE PANELS **
5	BLANK	A4	REPLACE PANELS **
6	A	A	REPLACE PANELS **
7	A4	A4	REPLACE PANELS **
8	A	Z2	REPLACE PANELS **
9	A4	Z2-4	REPLACE PANELS **
10	A4	BLANK	REPLACE PANELS **

LEGEND		
EXISTING	PROPOSED	
---	---	TAXIWAY A CIRCUIT
---	---	TAXIWAY E CIRCUIT
---	---	TAXIWAY D CIRCUIT
---	---	RUNWAY 4-22 CIRCUIT
WC	WC	WIND CONE CIRCUIT
▬▬▬▬	▬▬▬▬	4-WAY CONCRETE ENCASED DUCT
▬▬▬▬	▬▬▬▬	2" GRS - DIRECTIONAL BORE
□	□	TAXIWAY GUIDANCE SIGN
○	○	BASE MOUNTED LIGHT
○	○	SPLICE CAN
○	○	WIND CONE
○	○	SIGN NUMBER AND FACE (SEE SIGN SCHEDULE)
TBR		ITEM TO BE REMOVED
REM/REP		ITEM TO BE REMOVED AND REPLACED AT SAME LOCATION
REL		RELOCATED ITEM

THE DESIGNATIONS ARE AS FOLLOWS:
 L- TAXIWAY AND RUNWAY LOCATION SIGN - YELLOW LEGEND AND BORDER ON BLACK BACKGROUND
 Y- DIRECTION, DESIGNATION & BOUNDARY SIGNS - BLACK LEGEND ON YELLOW BACKGROUND
 R- MANDATORY INSTRUCTION SIGN - WHITE LEGEND ON RED BACKGROUND
 ** REPLACEMENT OF PANELS SHALL BE MEASURED FOR PAYMENT UNDER ITEM AR125470 - MODIFY EXISTING SIGN PANEL



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION



IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\draw\sheets\028_elec_layout_2a.dgn

DESIGN BY: TCS
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET

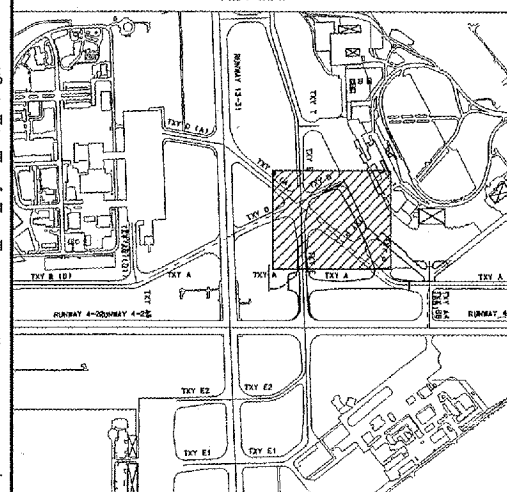
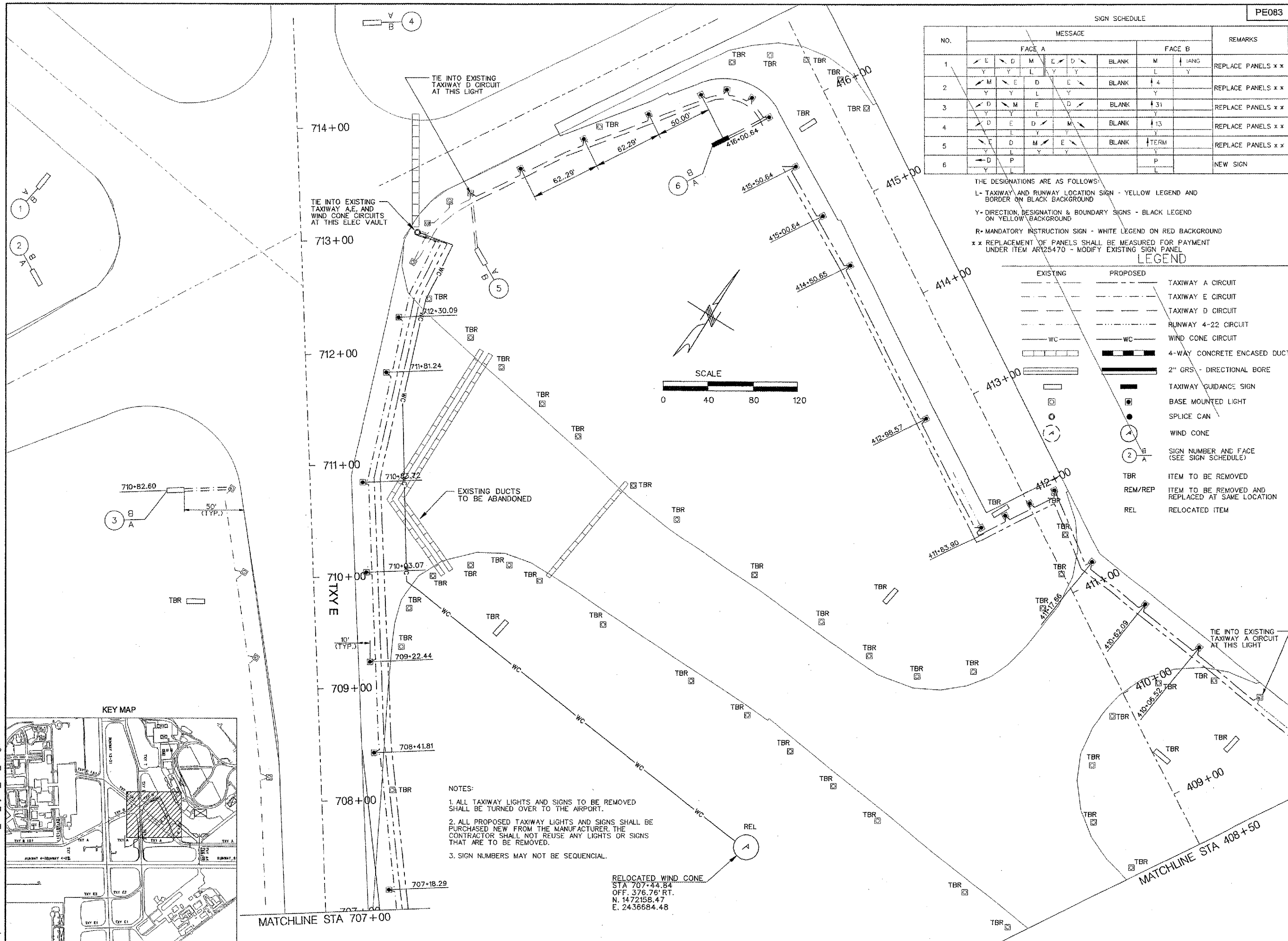
DATE: JUNE 24, 2005
JOB No: 04061-03

ELECTRICAL LAYOUT PLAN SHEET 2 OF 4

SHEET 28 OF 43 SHEETS

06/23/2005 k:\peoria\0406103\draw\sheets\028_elec_layout_2a.dgn

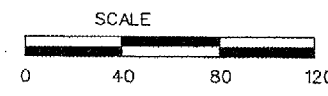
06/23/2005
 k:\peoria\0406103\draw\sheet\029_elec_layout_plan_3a.dgn



SIGN SCHEDULE								PE083	
NO.	FACE A				FACE B		REMARKS		
	1	E	D	M	E	D		BLANK	M
2	M	E	D	E		BLANK	L	Y	REPLACE PANELS **
3	D	M	E	D		BLANK	31		REPLACE PANELS **
4	D	E	D	M		BLANK	13		REPLACE PANELS **
5	E	D	M	E		BLANK	TERM		REPLACE PANELS **
6	D	P					P	L	NEW SIGN

THE DESIGNATIONS ARE AS FOLLOWS:
 L- TAXIWAY AND RUNWAY LOCATION SIGN - YELLOW LEGEND AND BORDER ON BLACK BACKGROUND
 Y- DIRECTION, DESIGNATION & BOUNDARY SIGNS - BLACK LEGEND ON YELLOW BACKGROUND
 R- MANDATORY INSTRUCTION SIGN - WHITE LEGEND ON RED BACKGROUND
 ** REPLACEMENT OF PANELS SHALL BE MEASURED FOR PAYMENT UNDER ITEM AR25470 - MODIFY EXISTING SIGN PANEL

EXISTING	PROPOSED	DESCRIPTION
(Symbol)	(Symbol)	TAXIWAY A CIRCUIT
(Symbol)	(Symbol)	TAXIWAY E CIRCUIT
(Symbol)	(Symbol)	TAXIWAY D CIRCUIT
(Symbol)	(Symbol)	RUNWAY 4-22 CIRCUIT
(Symbol)	(Symbol)	WIND CONE CIRCUIT
(Symbol)	(Symbol)	4-WAY CONCRETE ENCASED DUCT
(Symbol)	(Symbol)	2" GR5 - DIRECTIONAL BORE
(Symbol)	(Symbol)	TAXIWAY GUIDANCE SIGN
(Symbol)	(Symbol)	BASE MOUNTED LIGHT
(Symbol)	(Symbol)	SPLICE CAN
(Symbol)	(Symbol)	WIND CONE
(Symbol)	(Symbol)	SIGN NUMBER AND FACE (SEE SIGN SCHEDULE)
(Symbol)	(Symbol)	ITEM TO BE REMOVED
(Symbol)	(Symbol)	ITEM TO BE REMOVED AND REPLACED AT SAME LOCATION
(Symbol)	(Symbol)	RELOCATED ITEM



- NOTES:
- ALL TAXIWAY LIGHTS AND SIGNS TO BE REMOVED SHALL BE TURNED OVER TO THE AIRPORT.
 - ALL PROPOSED TAXIWAY LIGHTS AND SIGNS SHALL BE PURCHASED NEW FROM THE MANUFACTURER. THE CONTRACTOR SHALL NOT REUSE ANY LIGHTS OR SIGNS THAT ARE TO BE REMOVED.
 - SIGN NUMBERS MAY NOT BE SEQUENTIAL.

RELOCATED WIND CONE
 STA 707+44.84
 OFF. 376.76' RT.
 N. 1472158.47
 E. 2436684.48

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22) PLOT 1

GREATER Peoria REGIONAL Airport

RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION

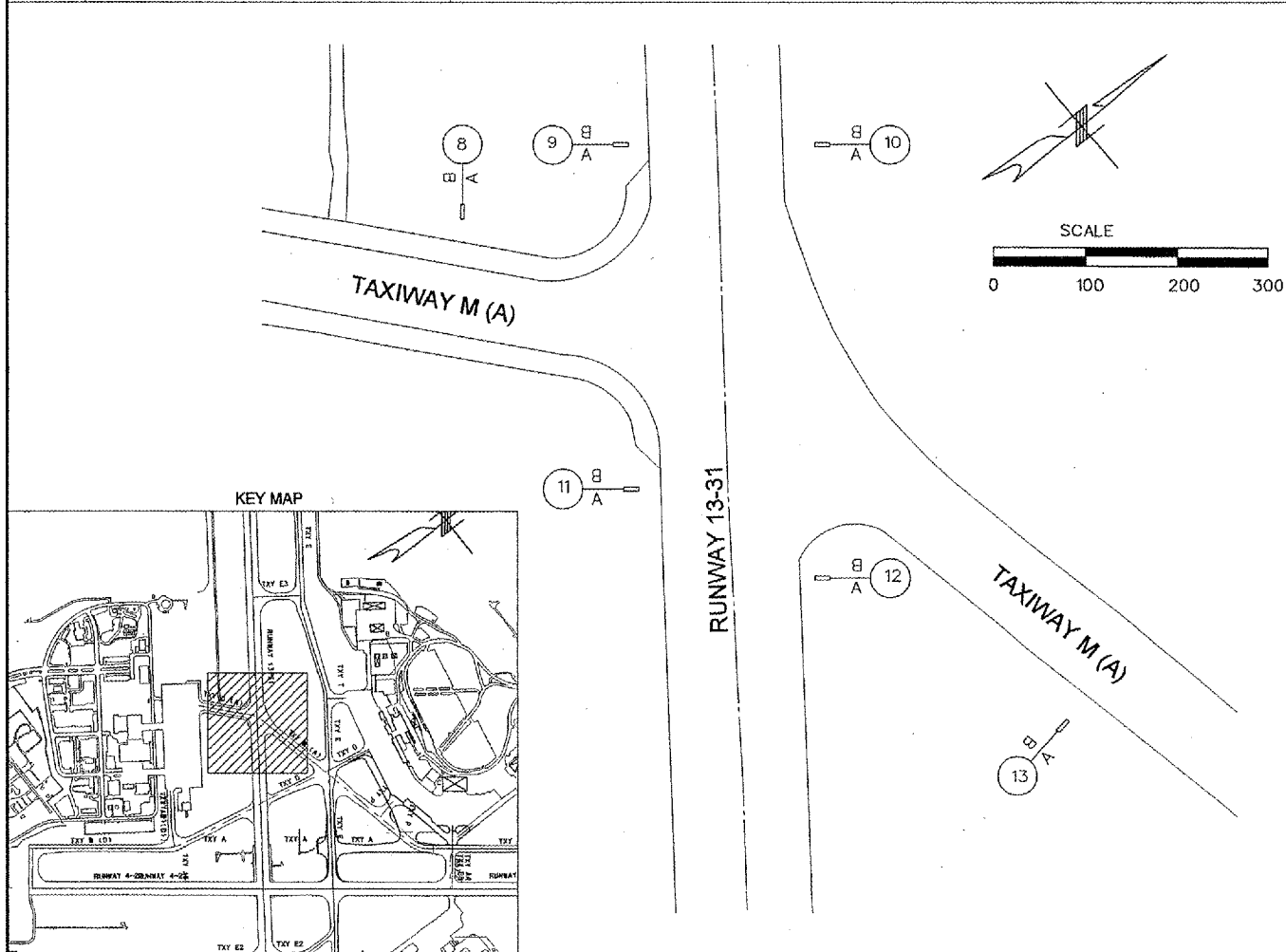
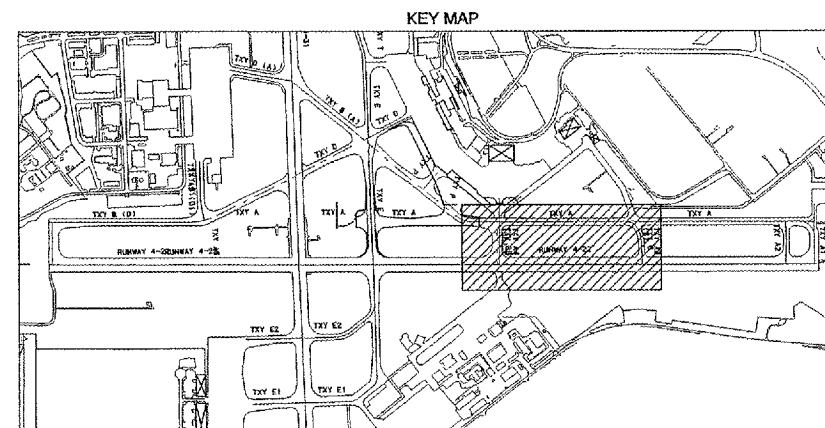
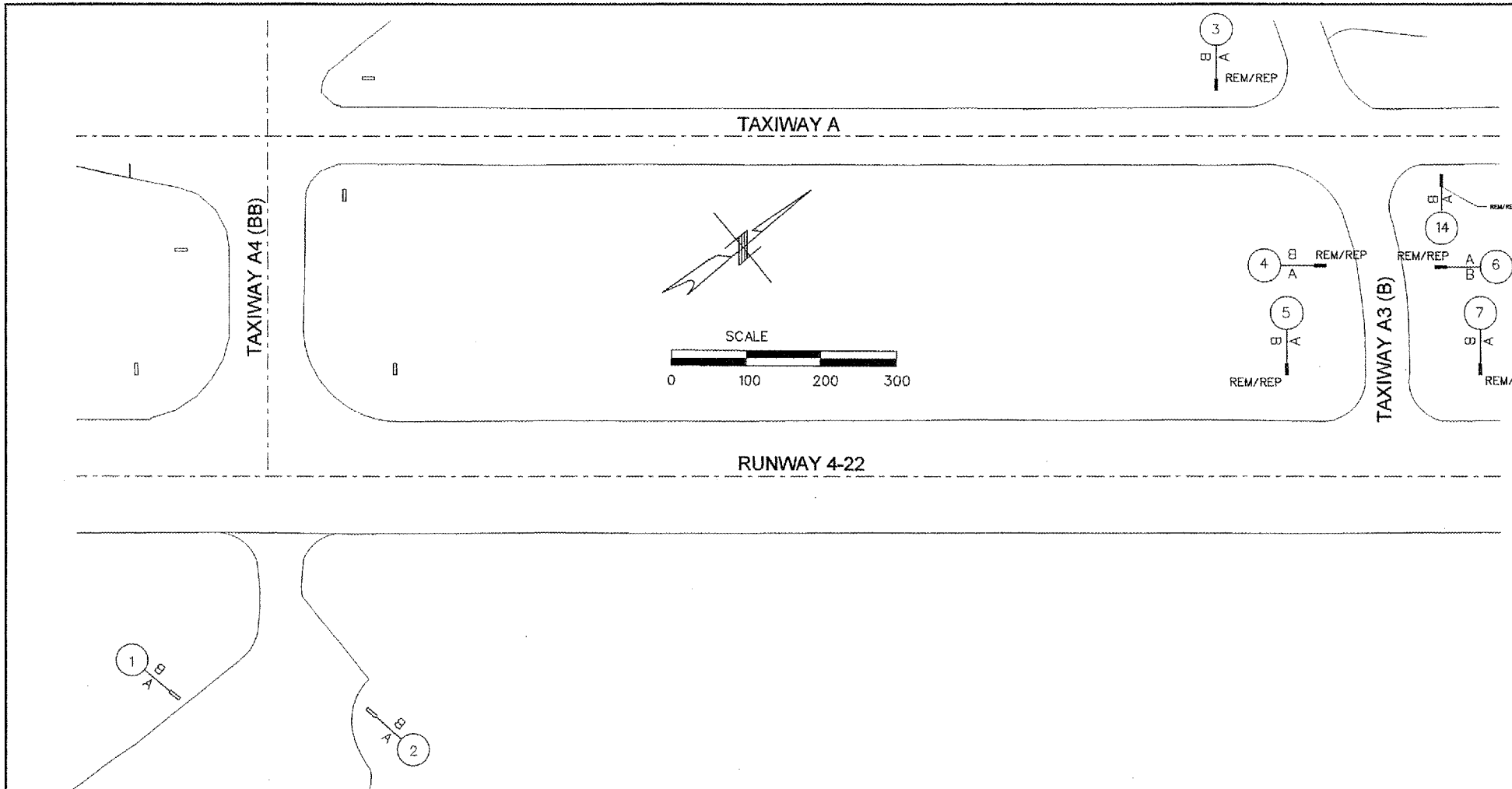
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS

IL PROJECT: PIA-3343
 AIP PROJ: 3-17-0080-XX
 FILE: k:\peoria\0406103\draw\sheet\029_elec_layout_plan_3a.dgn
 DESIGN BY: TCS
 DRAWN BY: CAG
 CHECKED BY: TCS
 APPROVED BY: CET
 DATE: JUNE 24, 2005
 JOB No: 04061-03

ELECTRICAL LAYOUT PLAN
 SHEET 3 OF 4
 SHEET 29 OF 43 SHEETS

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).
PLOT 1



NO.	MESSAGE				REMARKS
	FACE A		FACE B		
1	A4	4-22	A4		REPLACE PANELS **
2	L	R	L		REPLACE PANELS **
3	A		A	A3 →	REMOVE EXISTING SIGN INSTALL NEW SIGN AT SAME LOCATION
4	A3	← A →	A3		REMOVE EXISTING SIGN INSTALL NEW SIGN AT SAME LOCATION
5	BLANK		← A3	Y	REMOVE EXISTING SIGN INSTALL NEW SIGN AT SAME LOCATION
6	A3	22-4	A3		REMOVE EXISTING SIGN INSTALL NEW SIGN AT SAME LOCATION
7	L	R	L		REMOVE EXISTING SIGN INSTALL NEW SIGN AT SAME LOCATION
8	M	← M →	M	13-31	REPLACE PANELS **
9	L	Y	L	R	REPLACE PANELS **
10	BLANK		← M	Y	REPLACE PANELS **
11	Y		BLANK		REPLACE PANELS **
12	Y		BLANK		REPLACE PANELS **
13	M	31-13	M		REPLACE PANELS **
14	L	R	L		REPLACE PANELS **
14	A	← A3	BLANK		NEW SIGN

LEGEND

	EXISTING		PROPOSED	TAXIWAY A CIRCUIT
				TAXIWAY E CIRCUIT
				TAXIWAY D CIRCUIT
				RUNWAY 4-22 CIRCUIT
				WIND CONE CIRCUIT
				4-WAY CONCRETE ENCASED DUCT
				2" GRS - DIRECTIONAL BORE
				TAXIWAY GUIDANCE SIGN
				BASE MOUNTED LIGHT
				SPLICE CAN
				WIND CONE
				SIGN NUMBER AND FACE (SEE SIGN SCHEDULE)
				TBR ITEM TO BE REMOVED
				REM/REP ITEM TO BE REMOVED AND REPLACED AT SAME LOCATION
				REL RELOCATED ITEM

- NOTES:
- ALL TAXIWAY LIGHTS AND SIGNS TO BE REMOVED SHALL BE TURNED OVER TO THE AIRPORT.
 - ALL PROPOSED TAXIWAY LIGHTS AND SIGNS SHALL BE PURCHASED NEW FROM THE MANUFACTURER. THE CONTRACTOR SHALL NOT REUSE ANY LIGHTS OR SIGNS THAT ARE TO BE REMOVED.
 - SIGN NUMBERS MAY NOT BE SEQUENTIAL.

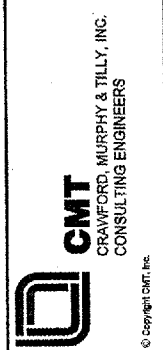
THE DESIGNATIONS ARE AS FOLLOWS:

- L- TAXIWAY AND RUNWAY LOCATION SIGN - YELLOW LEGEND AND BORDER ON BLACK BACKGROUND
- Y- DIRECTION, DESIGNATION & BOUNDARY SIGNS - BLACK LEGEND ON YELLOW BACKGROUND
- R- MANDATORY INSTRUCTION SIGN - WHITE LEGEND ON RED BACKGROUND

** REPLACEMENT OF PANELS SHALL BE MEASURED FOR PAYMENT UNDER ITEM AR125470 - MODIFY EXISTING SIGN PANEL



RELOCATE TAXIWAY A FROM
MID-FIELD INTERSECTION



IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\drawsheets\030_elec_layout_4a.dgn
DESIGN BY: TCS
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET
DATE: JUNE 24, 2005
JOB No: 04061-03
ELECTRICAL LAYOUT PLAN SHEET 4 OF 4
SHEET 30 OF 43 SHEETS

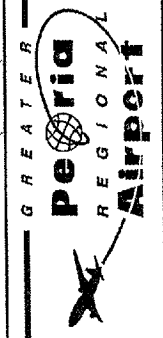
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06/23/2005
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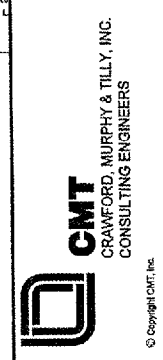
PE083

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34X22).
 PLOT 1



RELOCATE TAXIWAY A FROM
 MID-FIELD INTERSECTION



IL PROJECT: PIA-3343
 AIP PROJ: 3-17-0080-XX

FILE: k:\peoria\0406103\drawsheets\031_elec_schematic.dgn

DESIGN BY: TCS

DRAWN BY: CAG

CHECKED BY: TCS

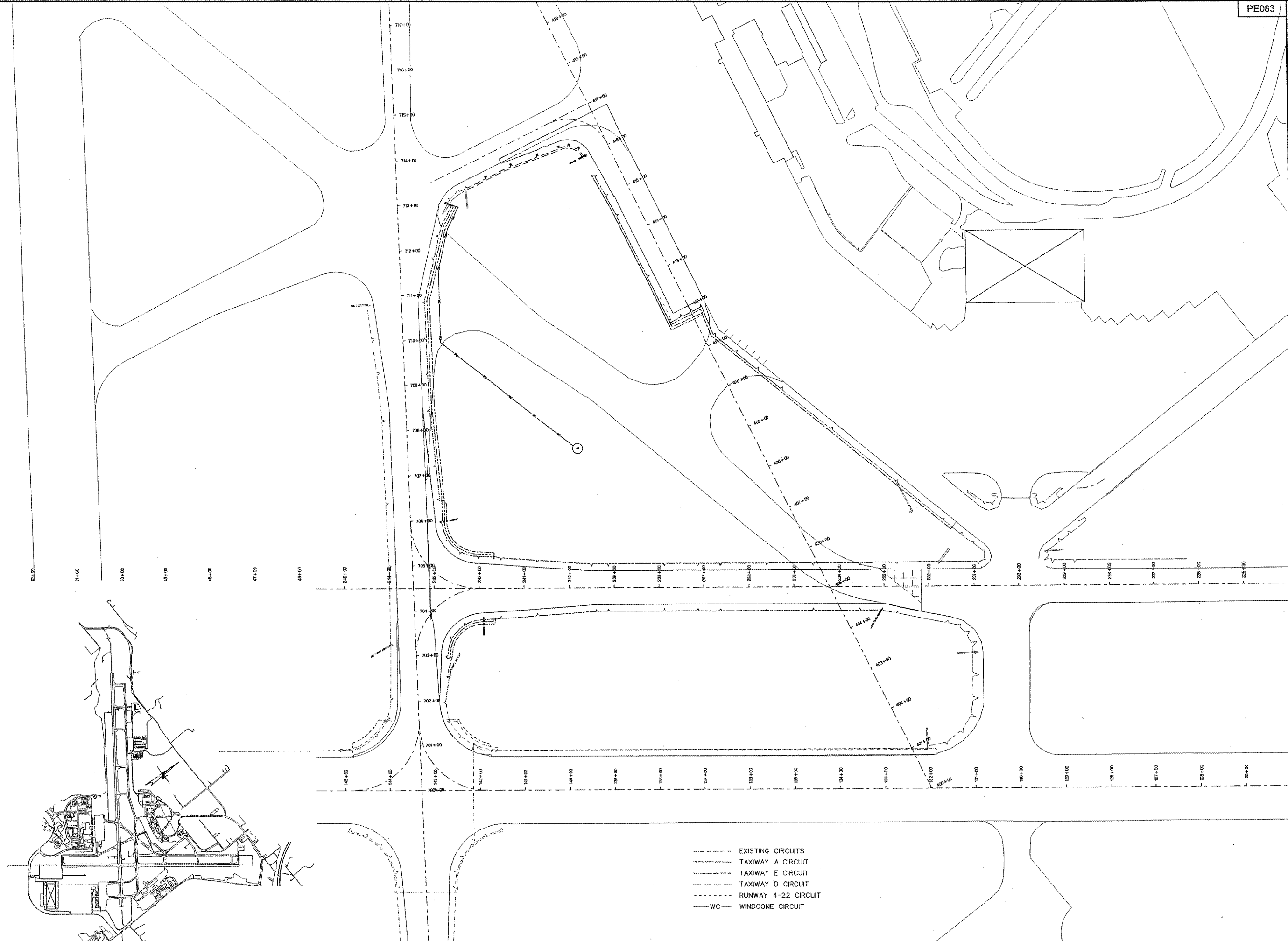
APPROVED BY: CET

DATE: JUNE 24, 2005

JOB No: 04061-03

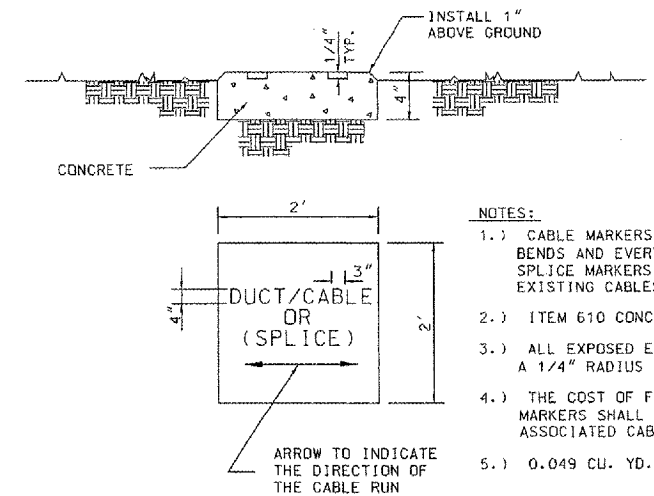
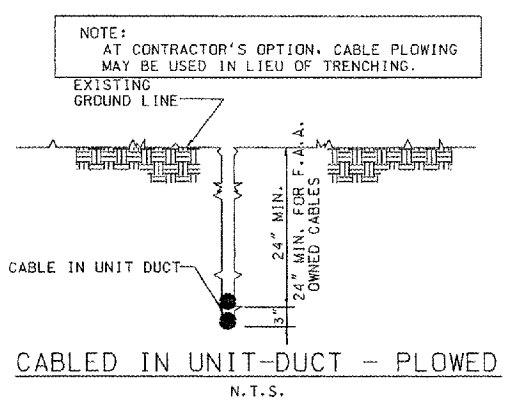
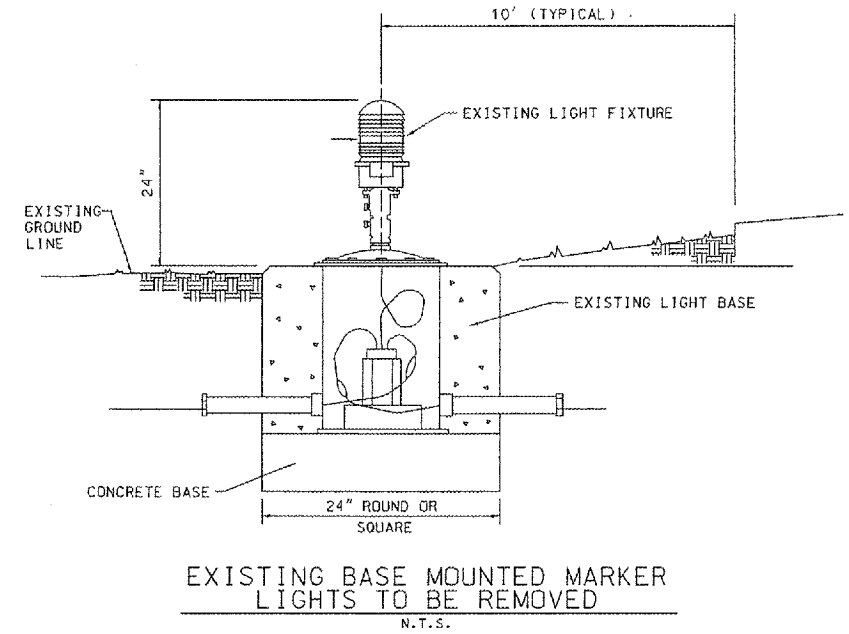
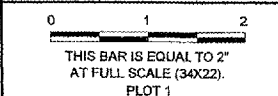
PROPOSED ELECTRICAL
 SCHEMATIC

SHEET 31 OF 43 SHEETS



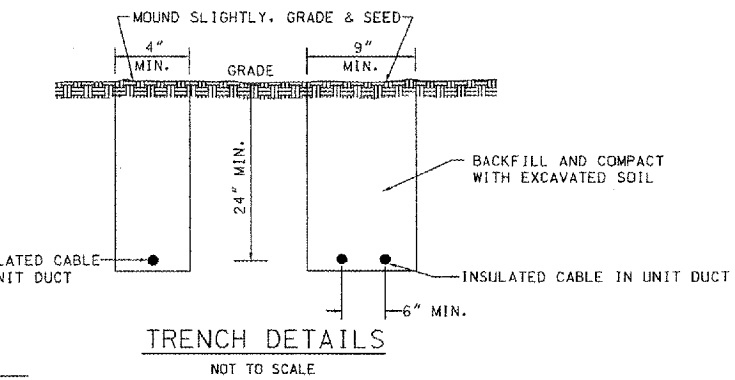
- EXISTING CIRCUITS
- TAXIWAY A CIRCUIT
- TAXIWAY E CIRCUIT
- TAXIWAY D CIRCUIT
- RUNWAY 4-22 CIRCUIT
- WC --- WINDCONE CIRCUIT

REVISIONS		
NUMBER	BY	DATE

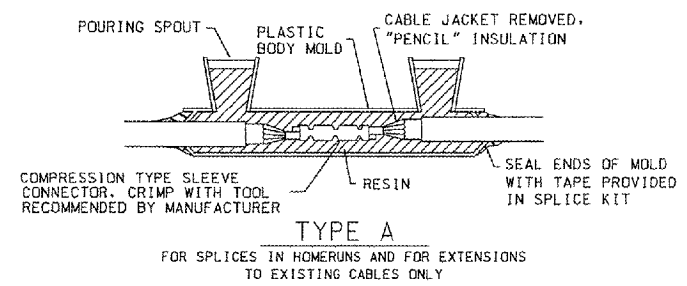


- NOTES:
- 1.) CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE CABLE RUN. SPLICE MARKERS SHALL BE INSTALLED WHEREVER EXISTING CABLES ARE SPLICED.
 - 2.) ITEM 610 CONCRETE SHALL BE USED.
 - 3.) ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
 - 4.) THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
 - 5.) 0.049 CU. YD. CONCRETE PER MARKER.

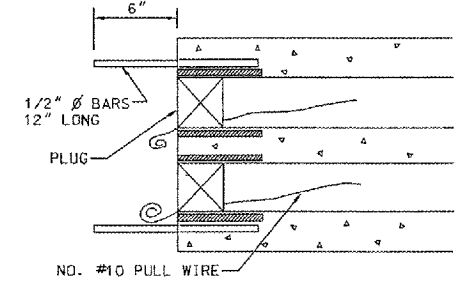
DETAIL - TURF CABLE/DUCT/SPLICE MARKER
NOT TO SCALE



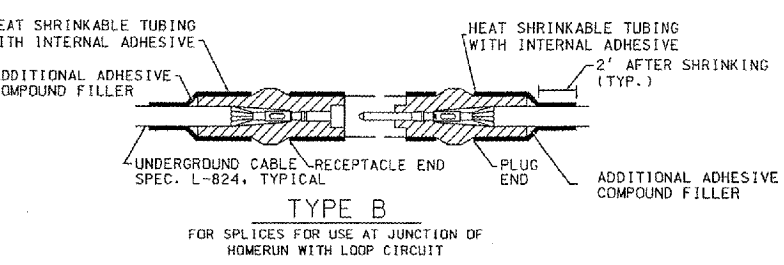
TRENCH DETAILS
NOT TO SCALE



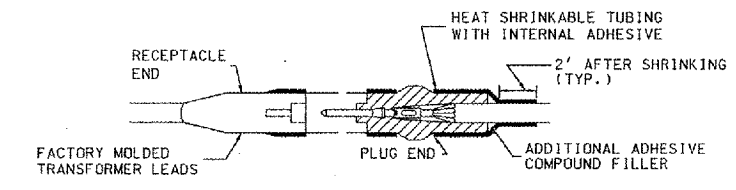
TYPE A
FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY



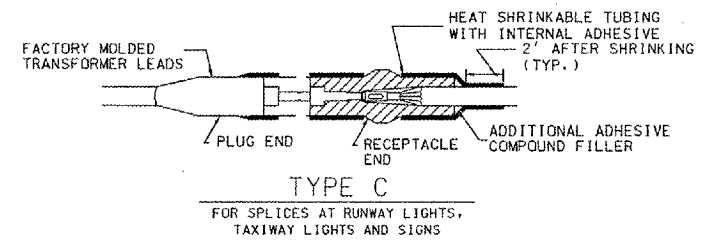
TYPICAL SECTION



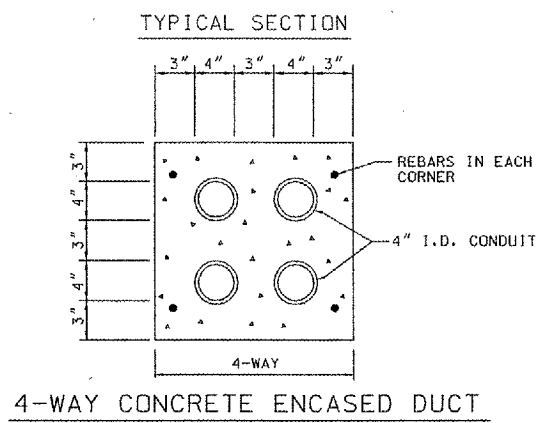
TYPE B
FOR SPLICES FOR USE AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT



TYPE C
FOR SPLICES AT RUNWAY LIGHTS, TAXIWAY LIGHTS AND SIGNS

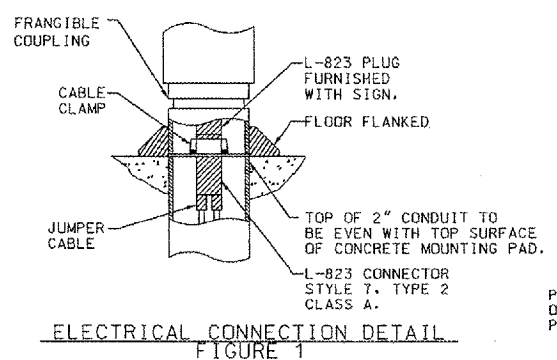


CABLE SPLICES
NOT TO SCALE

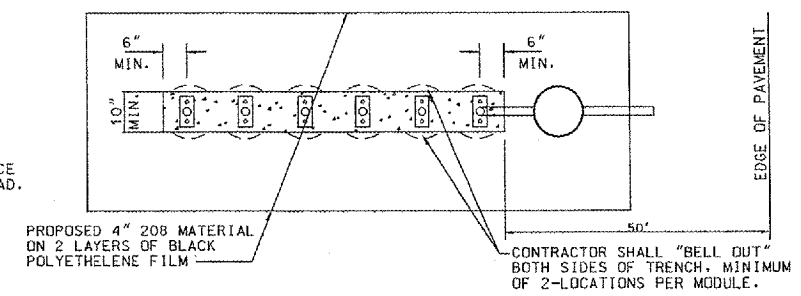


4-WAY CONCRETE ENCASED DUCT

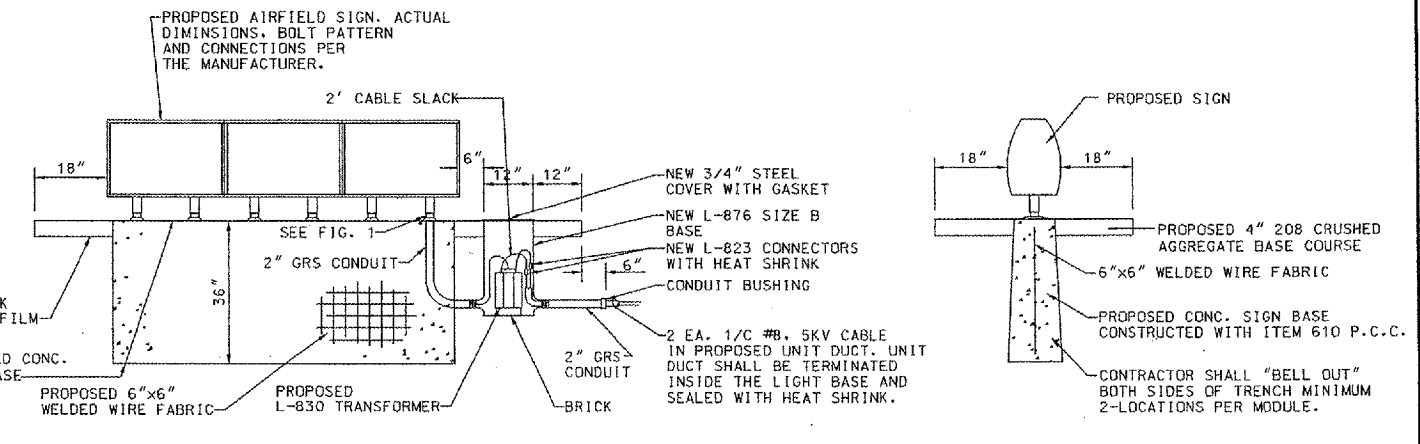
- NOTES
- A. DIMENSIONS SHOWN ARE MINIMUM
 - B. TOP OF CONCRETE ENCASEMENT TO BE NOT LESS THAN 24" BELOW FINISHED SUBGRADE
 - C. DUCT CONCRETE SHALL BE 610 STRUCTURAL P.C. CONCRETE



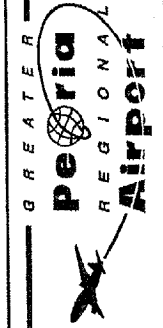
ELECTRICAL CONNECTION DETAIL
FIGURE 1



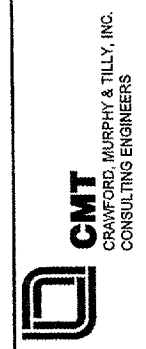
L-858 AIRFIELD SIGN DETAIL
NOT TO SCALE



06/23/2005 k:\peoria\0406103\draw\sheet\032_elect_details.dgn



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION

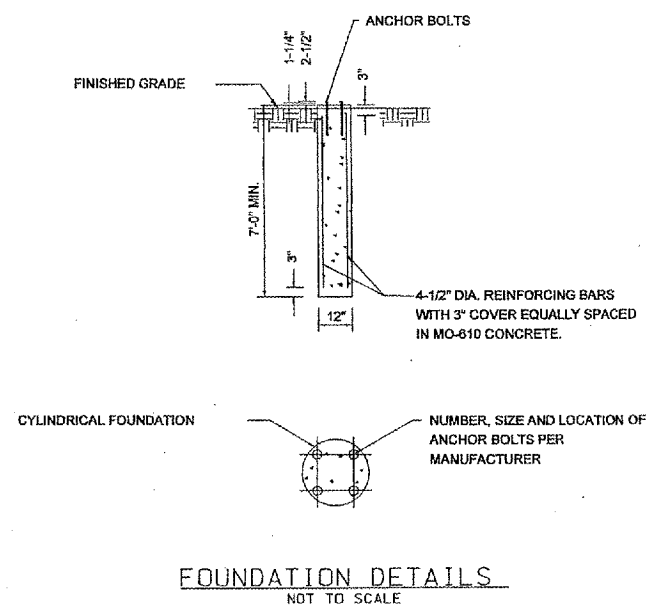
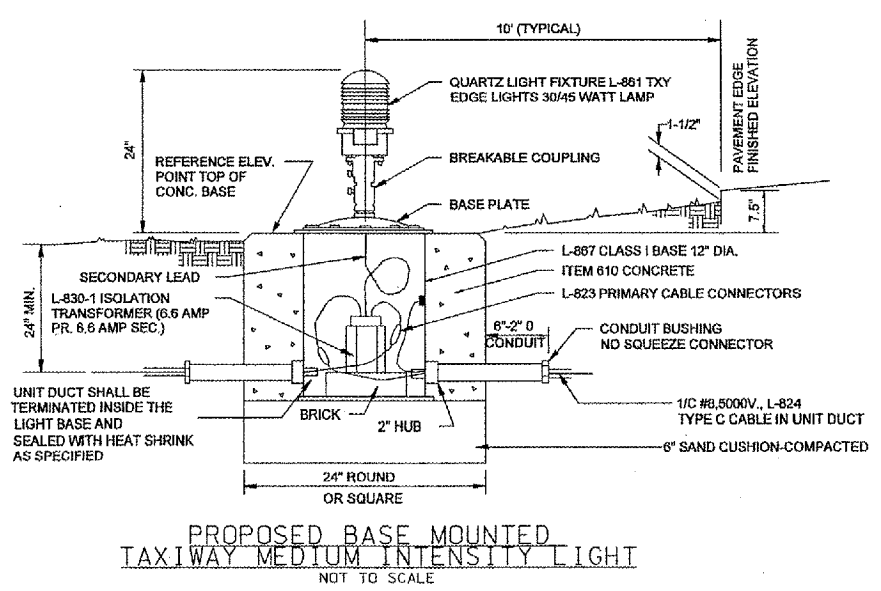
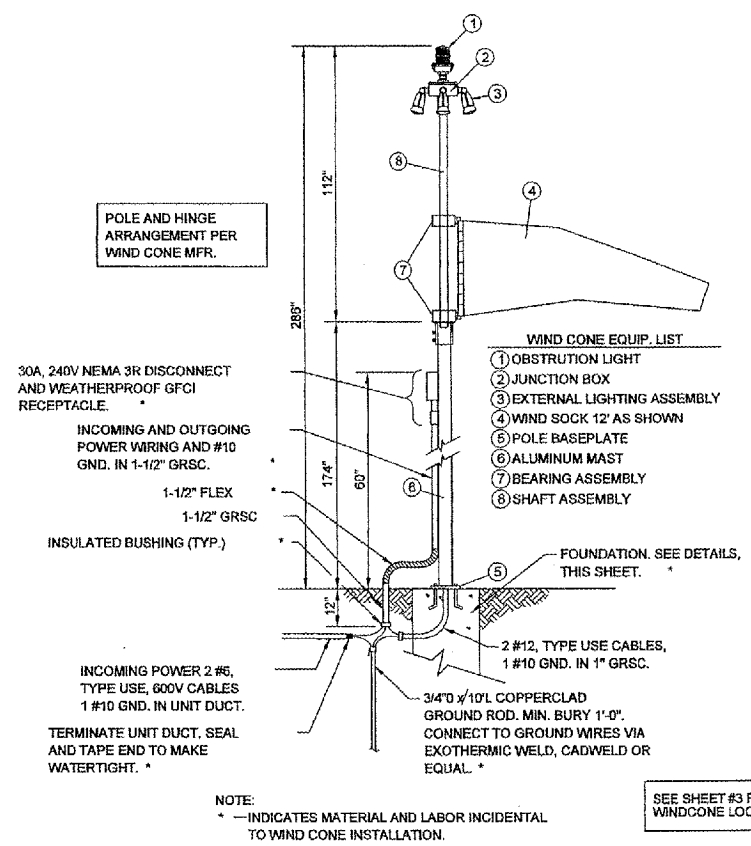
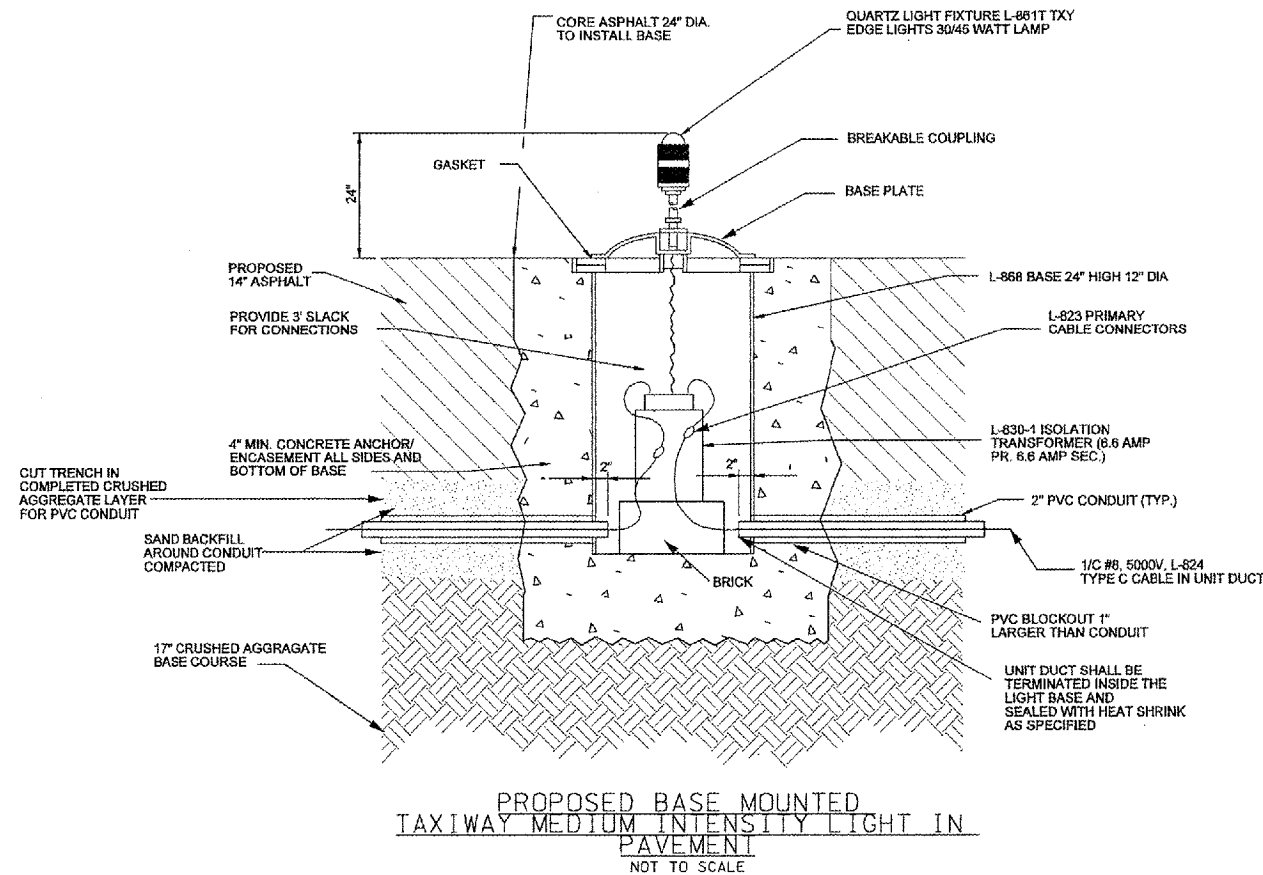
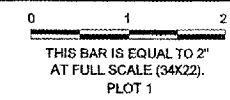


IL PROJECT: PIA-3343
AIP PROJ: 3-17-0080-XX

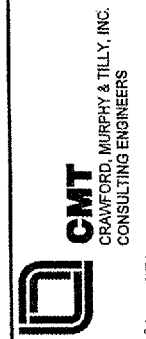
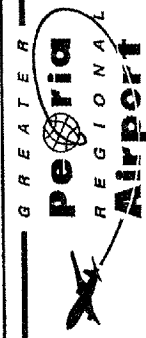
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DESIGN BY: TCS
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET
DATE: JUNE 24, 2005
JOB No: 04061-03

ELECTRICAL
DETAILS
SHEET 1 OF 2

REVISIONS		
NUMBER	BY	DATE



RELOCATE TAXIWAY A FROM MID-FIELD INTERSECTION



IL PROJECT: PIA-3343 AIP PROJ: 3-17-0080-XX
FILE: k:\peoria\0406103\draw\sheet\033_elect.dwg
DESIGN BY: TCS
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET
DATE: JUNE 24, 2005
JOB No: 04061-03
ELECTRICAL DETAILS SHEET 2 OF 2
SHEET 33 OF 43 SHEETS

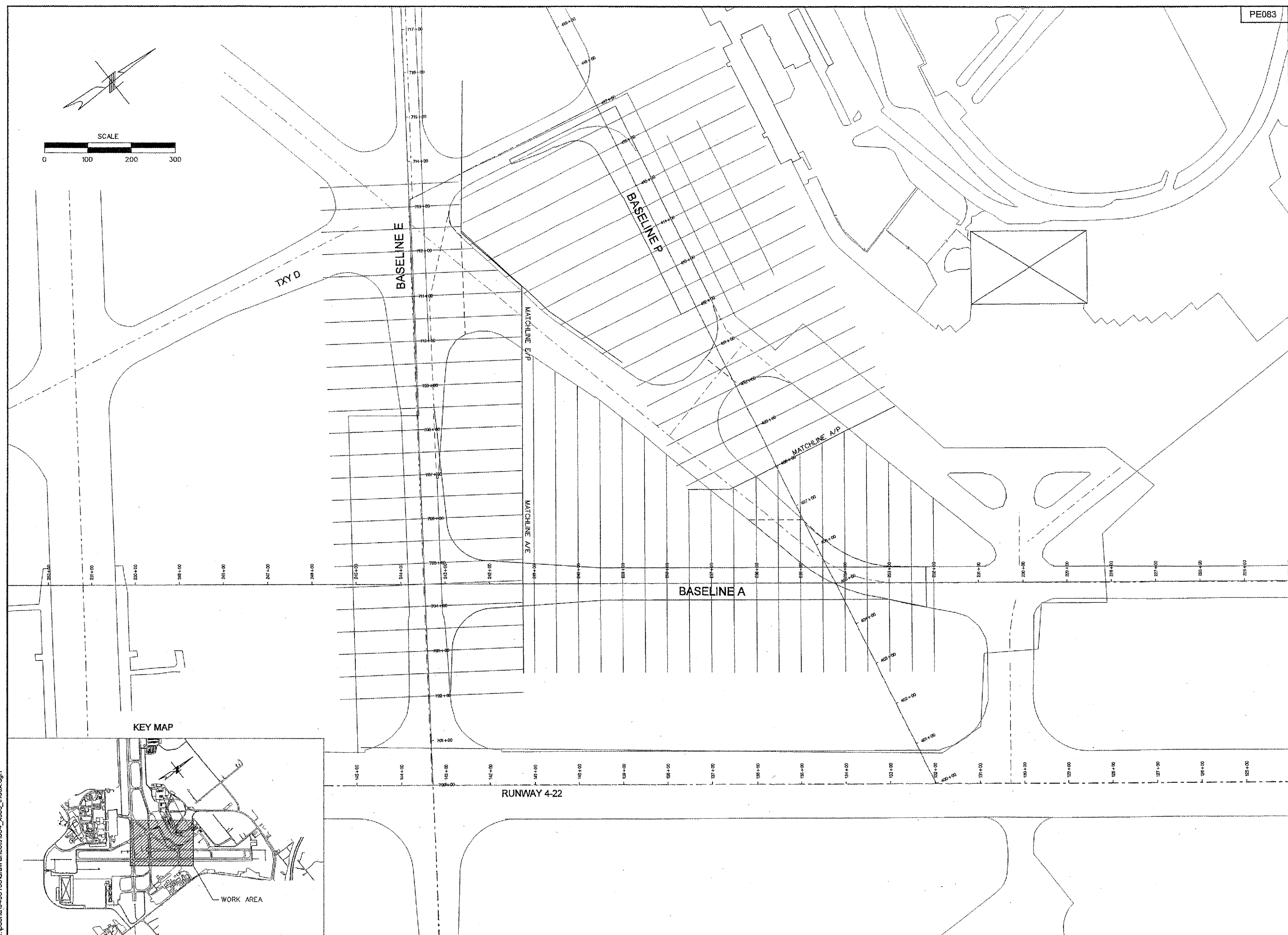
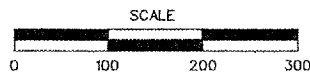
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PE083

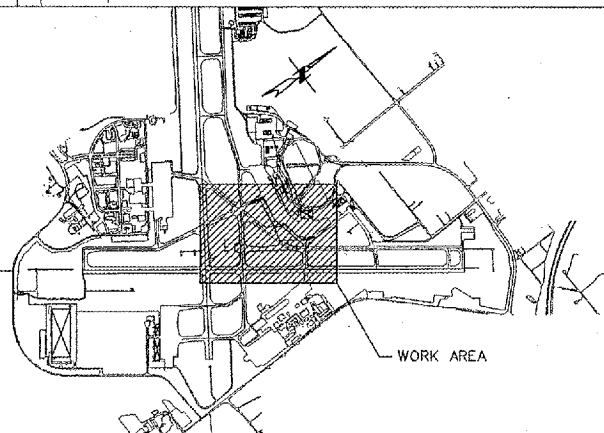
REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34X22).
 PLOT 1



KEY MAP



RELOCATE TAXIWAY A FROM
 MID-FIELD INTERSECTION



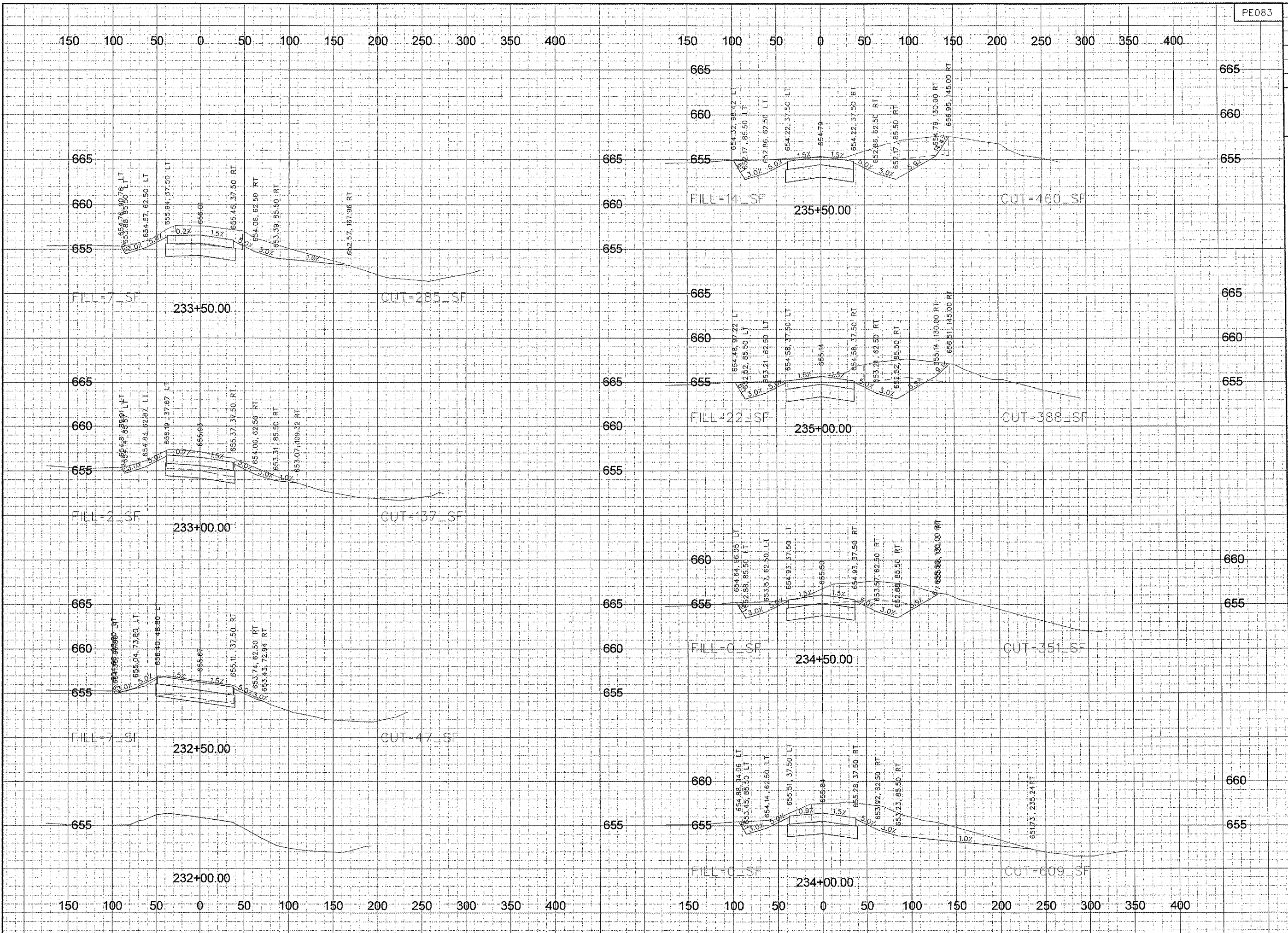
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 AIP PROJ: 3-17-0080-XX

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 DESIGN BY: BWG
 DRAWN BY: CAG
 CHECKED BY: TCS
 APPROVED BY: CET
 DATE: JUNE 24, 2005
 JOB No: 04061-03

CROSS SECTION
 INDEX SHEET

06/23/2005
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06/23/2005
 k:\peoria\0+06103\draw_sheets\035_37_xsec_o_sheets.dgn



PE083

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).
 PLOT 1

GREATER
Peoria
 REGIONAL
Airport

RELOCATE TAXIWAY A FROM
 MID-FIELD INTERSECTION

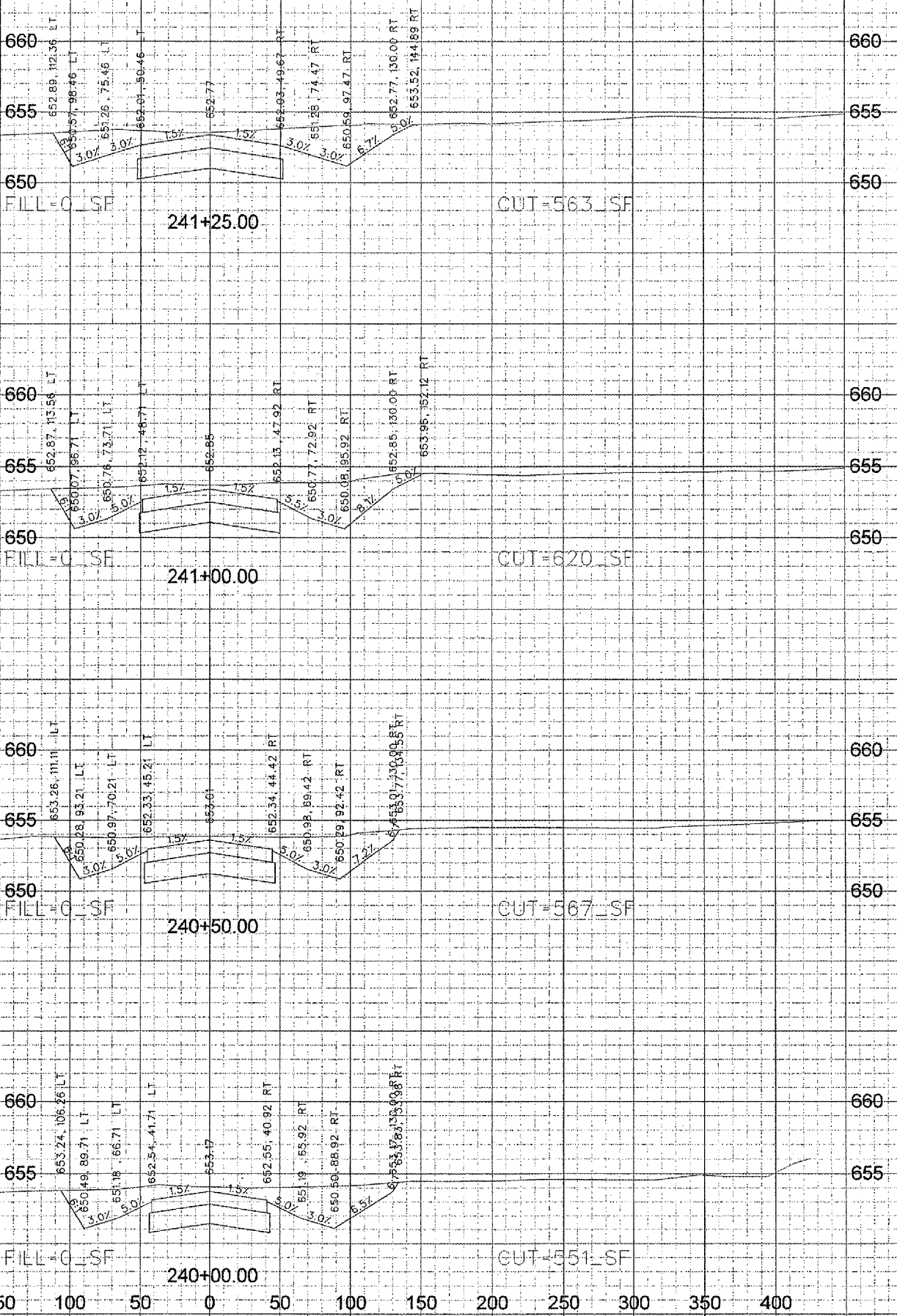
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FILE:
DESIGN BY: BWG
DRAWN BY: CAG
CHECKED BY: JHD
APPROVED BY: CET
DATE: JULY 30, 2004
JOB No: 04061-03
CROSS SECTIONS TAXIWAY A SHEET 1 OF 4
SHEET 35 OF 43 SHEETS

06/23/2005
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150 100 50 0 50 100 150 200 250 300 350 400



150 100 50 0 50 100 150 200 250 300 350 400

PE083		
REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).
 PLOT 1



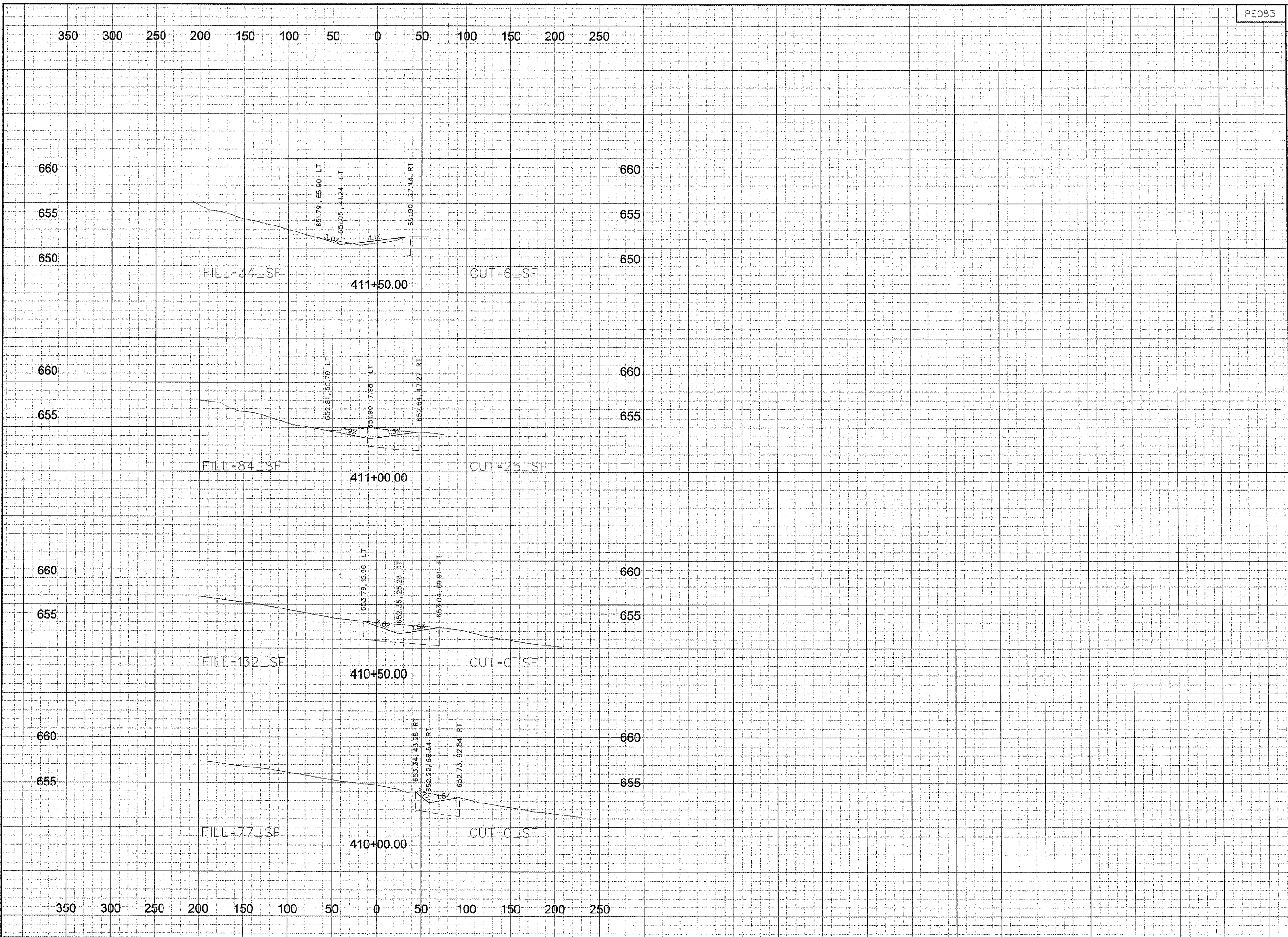
RELOCATE TAXIWAY A FROM
 MID-FIELD INTERSECTION

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FILE:
DESIGN BY: BWG
DRAWN BY: CAG
CHECKED BY: JHD
APPROVED BY: CET
DATE: JULY 30, 2004
JOB No: 04061-03

CROSS SECTIONS
 TAXIWAY A
 SHEET 3 OF 4

06/23/2005
 R:\peoria\0406103\draw\sheet\038_39_xsec_p_sheets.dgn



PE083		
REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34X22).
 PLOT 1

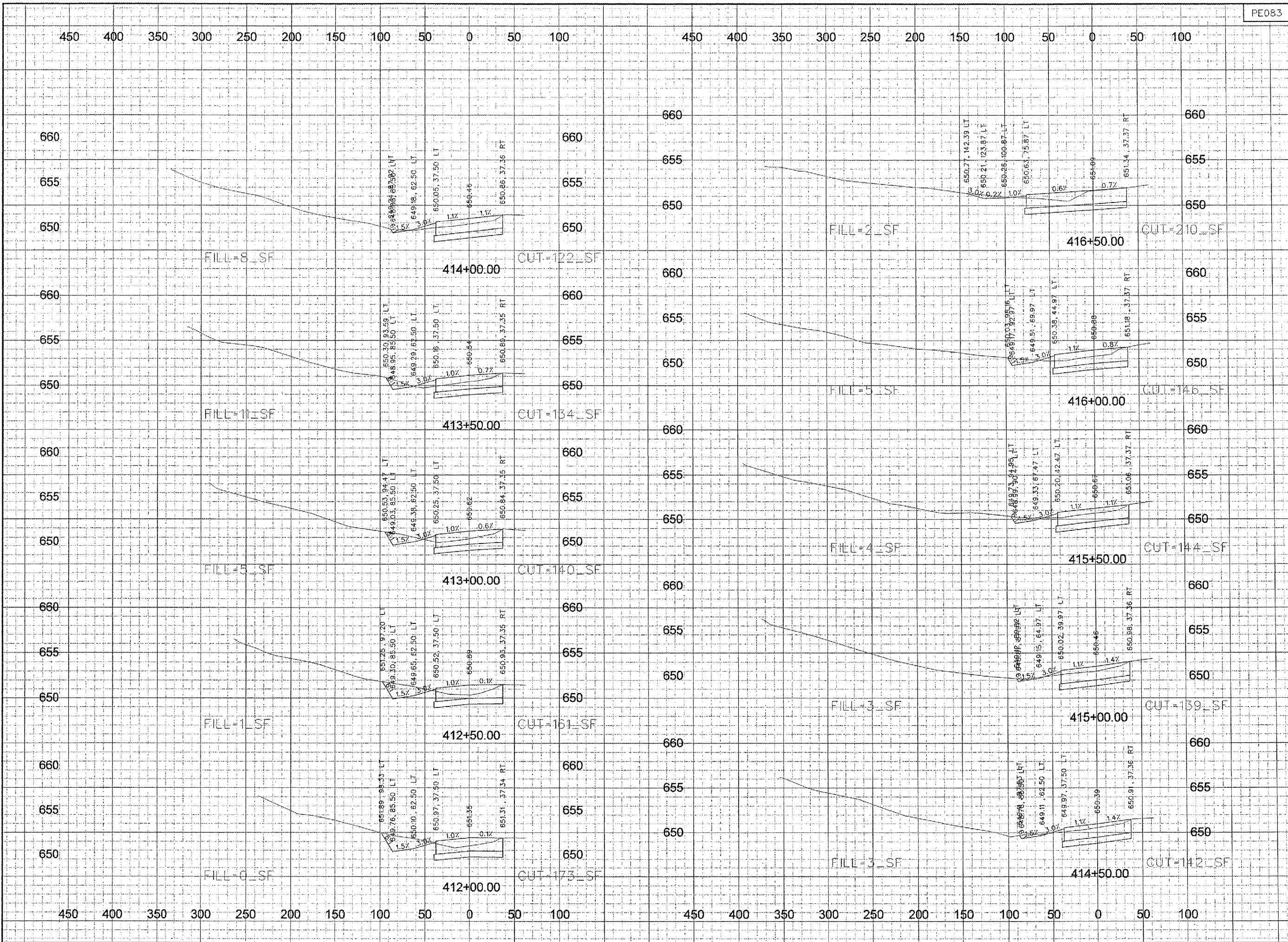
GREATER
Peoria
 REGIONAL
Airport

**RELOCATE TAXIWAY A FROM
 MID-FIELD INTERSECTION**

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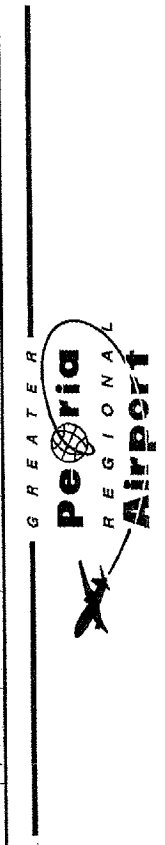
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CHECKED BY: JHD
APPROVED BY: CET
DATE: JULY 30, 2004
JOB No: 04061-03
CROSS SECTIONS TAXIWAY P SHEET 1 OF 2
SHEET 38 OF 43 SHEETS

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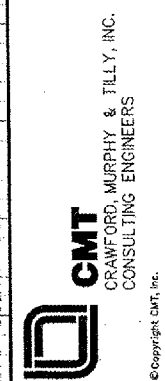


REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).
PLOT 1



RELOCATE TAXIWAY A FROM
MID-FIELD INTERSECTION



FILE:	
DESIGN BY:	BWG
DRAWN BY:	CAG
CHECKED BY:	JHD
APPROVED BY:	CET
DATE:	JULY 30, 2004
JOB No:	04061-03

CROSS SECTIONS
TAXIWAY P
SHEET 2 OF 2

06/23/2005
 K:\peoria\0406103\draw\sheets\040_43_xsec_e_sheets.dgn

PE083

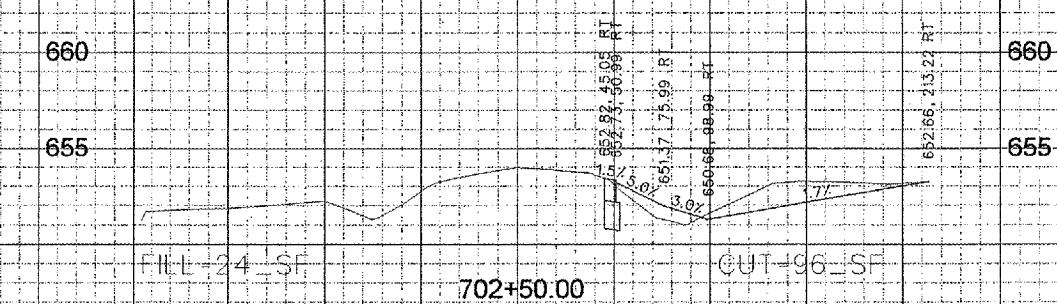
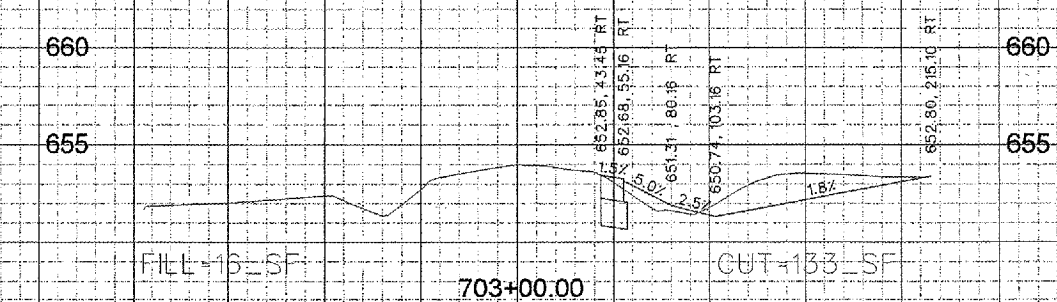
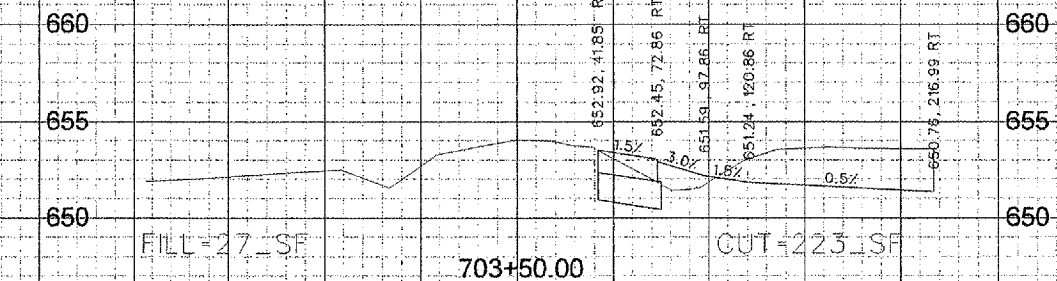
REVISIONS

NUMBER	BY	DATE

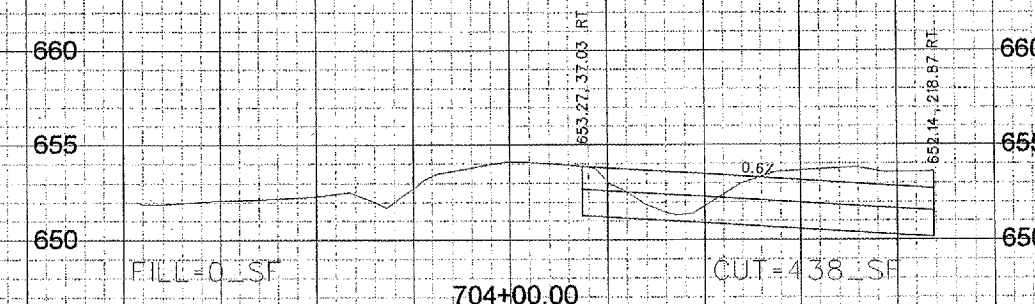
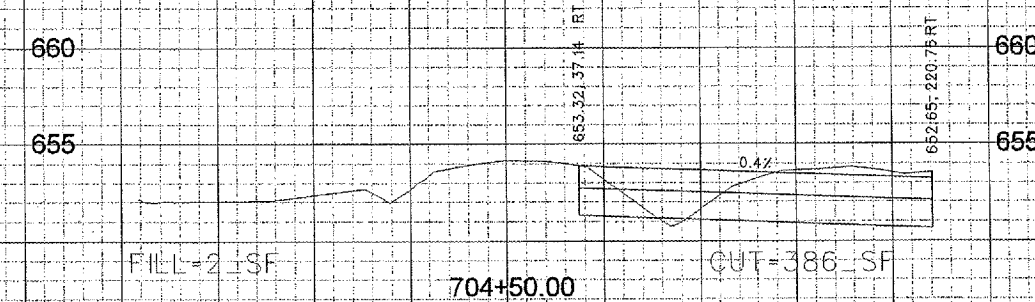
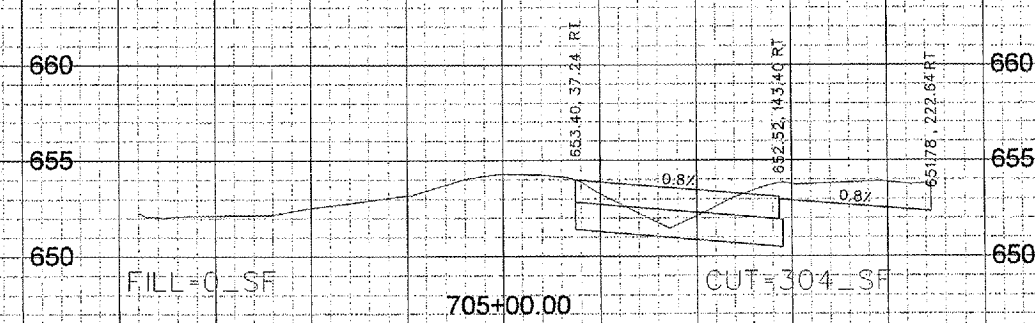
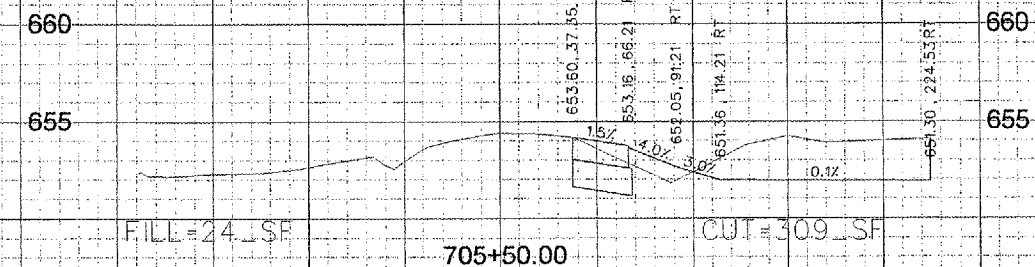
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).
 PLOT 1

250 200 150 100 50 0 50 100 150 200 250

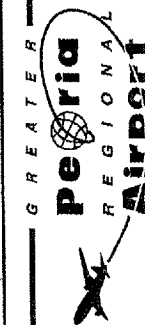
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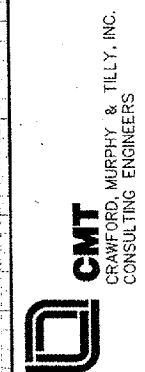
250 200 150 100 50 0 50 100 150 200 250



250 200 150 100 50 0 50 100 150 200 250



RELOCATE TAXIWAY A FROM
 MID-FIELD INTERSECTION



FILE:

DESIGN BY: BWG

DRAWN BY: CAG

CHECKED BY: JHD

APPROVED BY: GET

DATE: JULY 30, 2004

JOB No: 04061-03

CROSS SECTIONS
 TAXIWAY E
 SHEET 1 OF 4

SHEET 40 OF 43 SHEETS

06/23/2005
k:\peoria\0406\03\draw\sheet\040_43_xsec_e_sheets.dgn

PE083

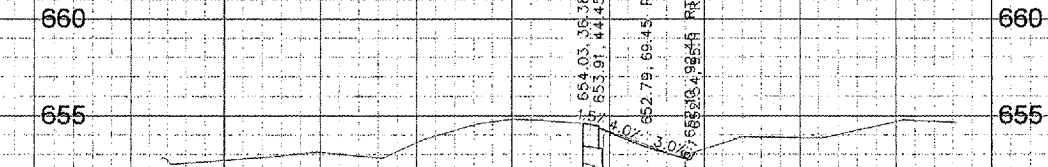
REVISIONS

NUMBER BY DATE

0 1 2

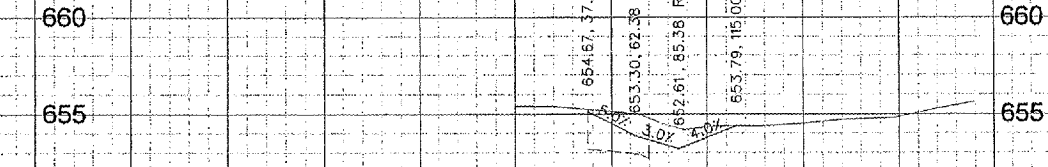
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22),
PLOT 1

250 200 150 100 50 0 50 100 150 200 250

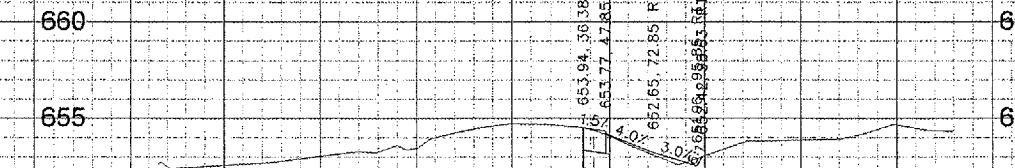


FILL-3_SF 707+50.00 CUT-9_SF

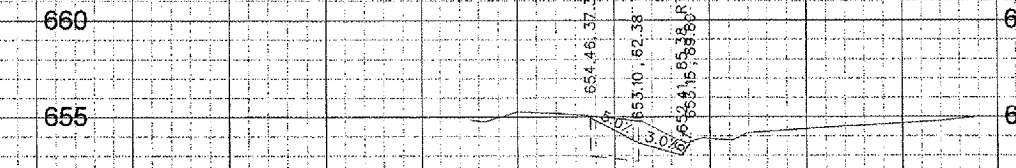
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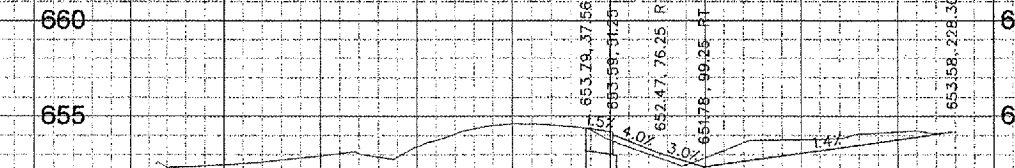
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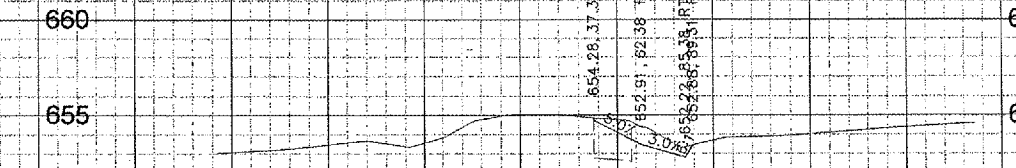
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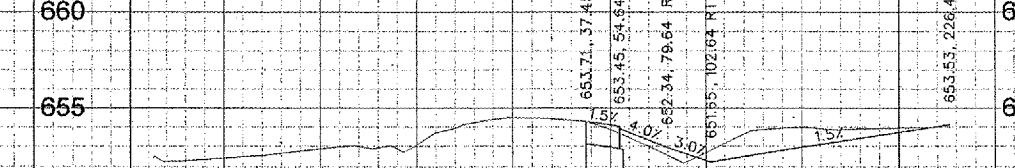
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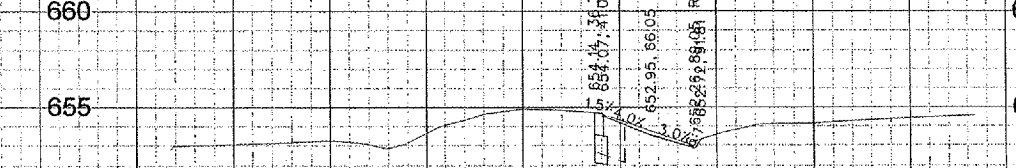
FILL-12_SF 706+50.00 CUT-111_SF



FILL-31_SF 708+50.00 CUT-22_SF



FILL-13_SF 706+00.00 CUT-127_SF



FILL-20_SF 708+00.00 CUT-10_SF

250 200 150 100 50 0 50 100 150 200 250

250 200 150 100 50 0 50 100 150 200 250



RELOCATE TAXIWAY A FROM
MID-FIELD INTERSECTION



FILE:
 DESIGN BY: BWG
 DRAWN BY: CAG
 CHECKED BY: JHD
 APPROVED BY: CET
 DATE: JULY 30, 2004
 JOB No: 04061-03

CROSS SECTIONS
TAXIWAY E
SHEET 2 OF 4

SHEET 41 OF 43 SHEETS

06/23/2005
c:\peoria\0406103\draw\sheets\040_43_xsec_e_sheets.dgn

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250 200 150 100 50 0 50 100 150 200 250

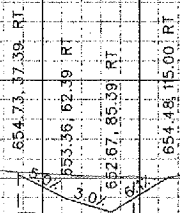
660
655

660
655

FILL = 55_SF

CUT = 9_SF

710+00.00



PE083

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).
PLOT 1



RELOCATE TAXIWAY A FROM
MID-FIELD INTERSECTION

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CONSULTING ENGINEERS
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FILE:
DESIGN BY: BWG
DRAWN BY: CAG
CHECKED BY: JHD
APPROVED BY: CET
DATE: JULY 30, 2004
JOB No: 04061-03

CROSS SECTIONS
TAXIWAY E
SHEET 3 OF 4

06/23/2005
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PE083

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).
 PLOT 1

250 200 150 100 50 0 50 100 150 200 250

665

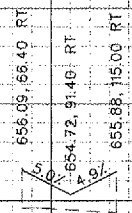
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655

FILL=67

712+00.00

CUT=0 SF



665

660

655

250 200 150 100 50 0 50 100 150 200 250

665

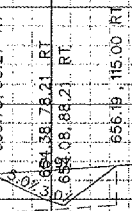
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655

FILL=65

711+50.00

CUT=0 SF



665

660

655

665

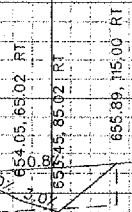
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655

FILL=64

711+00.00

CUT=1 SF



665

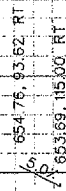
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655

FILL=0

713+00.00

CUT=0 SF



660

655

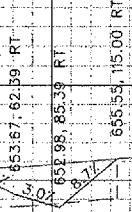
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FILL=66

710+50.00

CUT=1 SF



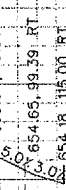
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655

FILL=27

712+50.00

CUT=17 SF



660

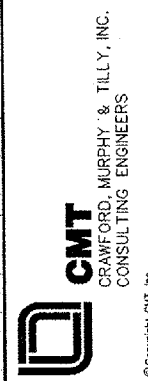
655

250 200 150 100 50 0 50 100 150 200 250

250 200 150 100 50 712+50.00 50 100 150 200 250



RELOCATE TAXIWAY A FROM
 MID-FIELD INTERSECTION



FILE:	
DESIGN BY:	BWG
DRAWN BY:	CAG
CHECKED BY:	JHD
APPROVED BY:	CET
DATE:	JULY 30, 2004
JOB No:	04061-03
CROSS SECTIONS TAXIWAY E SHEET 4 OF 4	
SHEET 43 OF 43 SHEETS	