

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

F.A.P. 704 (I-55 BUSN., VETERANS PARKWAY)
SECTION (57-7)RS-1 & 1RS-3

RESURFACING (3R)
BRIDGE DECK REPAIRS, & SAFETY IMPROVEMENTS
MCLEAN COUNTY

C-95-021-11

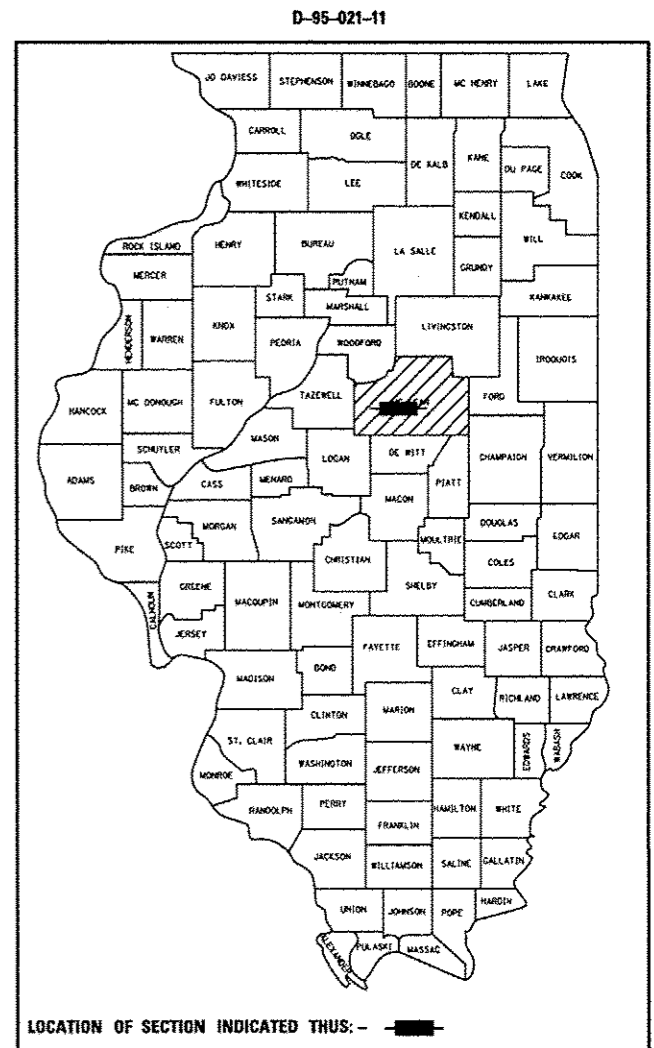
W OF FOX CREEK RD TO 0.2 MI E OF BUNN ST IN BLOOMINGTON

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|--------------------|--------|--------------------|-----------|
| 704 | (57-7)RS-1 & 1RS-3 | McLean | 273 | 1 |
| ILLINOIS | | | CONTRACT NO. 70838 | |

| CURRENT TRAFFIC DATA | | | |
|----------------------|---------|---------|---------|
| | LEG "A" | LEG "B" | LEG "C" |
| 2011 ADT | 15,400 | 23,500 | 28,200 |
| P.U. % | 91.6 | 93.3 | 93.7 |
| S.U. % | 3.0 | 2.5 | 2.9 |
| M.U. % | 5.4 | 4.2 | 3.4 |

LEG "A" IS FROM NORTH OF I-74 TO WEST OF MORRIS AVE.
LEG "B" IS THE OMISSION SECTION FROM WEST OF MORRIS AVE. TO EAST OF MORRIS AVE.
LEG "C" IS FROM EAST OF MORRIS AVE. TO COMMERCE PARKWAY.

DESIGN DESIGNATION
N/A



EXISTING SN 057-0064 AT STA. 74 + 41.00
306'-0" B-B ABUTMENTS
5 SPAN STEEL I-BEAM OVER N&S RR

EXISTING SN 057-0063 AT STA. 48 + 08.37
167'-1" B-B ABUTMENTS
3 SPAN STEEL I-BEAM OVER FAP 730

BEGIN PROJECT LEG C:
STA. 32 + 26.74 (LEG C EB)
STA. 32 + 32.06 (LEG C WB)

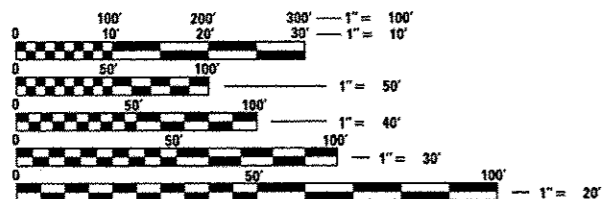
STATION EQUATION
STA. 390 + 96.85 BK. = STA. 29 + 96.20 AH.

END PROJECT LEG A:
STA. 366 + 35.20 (LEG A EB)
STA. 366 + 35.33 (LEG A WB)

STATION EQUATION
STA. 18 + 75.00 BK. = STA. 335 + 55.87 AH.

STATION EQUATION
STA. 87 + 41.66 BK. = STA. 87 + 45.84 AH.

END PROJECT LEG C:
STA. 89 + 40.28 (LEG C EB)
STA. 89 + 41.79 (LEG C WB)

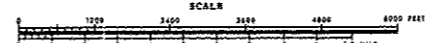
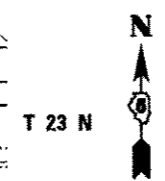
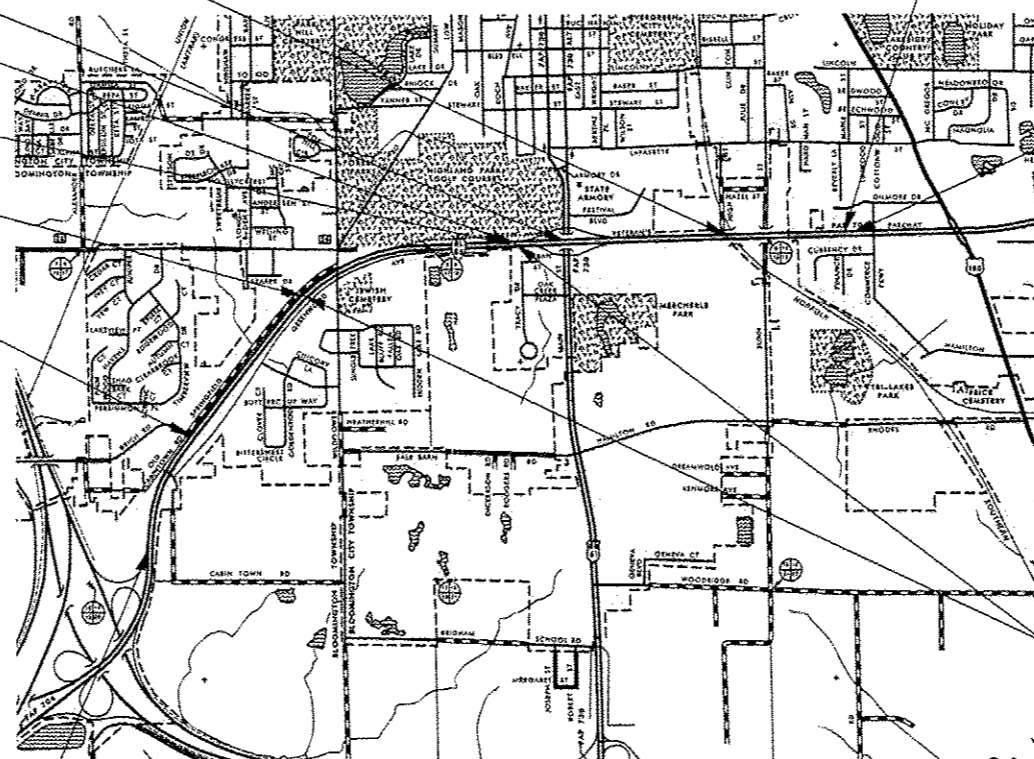


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811
BLOOMINGTON TOWNSHIP

PROJECT ENGINEER: NANCY FASIG
SQUAD LEADER: BRIAN J. HOGAN
DESIGNER: BILLY J. MURPHY
(217) 465-4181
CONTRACT NO. 70838

BEGIN PROJECT LEG A:
STA. 33 + 30.00 (LEG A EB)
STA. 29 + 70.00 (LEG A WB)



GROSS LENGTH = 12,935.88 FT. = 2.450 MILE
NET LENGTH = 10,243.69 FT. = 1.940 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED June 11 2014

[Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

July 18 2014
[Signature]
ENGINEER OF DESIGN AND ENVIRONMENT

July 18 2014
[Signature]
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

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| | | | | | | | | | | |
|---|------------------------------|------------|-----------|---|------------------------|-------------|-----------------|--------|--------------|--------------------|
| FILE NAME = | USER NAME = dodgers | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | INDEX OF SHEETS | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwork\pvidot\dodgers\0267971\0570038-sht-gennote.dgn | 038-sht-gennote.dgn | DRAWN - | REVISED - | | | 704 | (57-7)RS-1&RS-3 | McLean | 273 | 2 |
| MODELNAME | PLOT SCALE = 48.0000' / 1" = | CHECKED - | REVISED - | | SCALE: | | | | | |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - | | SHEET 1 OF 1 SHEETS | STA. | TO STA. | | | CONTRACT NO. 70838 |
| ILLINOIS F&D PROJECT | | | | | | | | | | |

HIGHWAY STANDARDS

| STANDARD NO. | DESCRIPTION |
|--------------|---|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS |
| 001001-02 | AREAS OF REINFORCEMENT BARS |
| 001006 | DECIMAL OF AN INCH AND OF A FOOT |
| 280001-07 | TEMPORARY EROSION CONTROL SYSTEMS |
| 406001-05 | ENTRANCE RAMP TERMINAL |
| 406101-04 | EXIT RAMP TERMINAL |
| 420001-07 | PAVEMENT JOINTS |
| 420101-04 | 24' (7.2 m) JOINTED PCC PAVEMENT |
| 424001-07 | PERPENDICULAR CURB RAMPS FOR SIDEWALKS |
| 424031-01 | MEDIAN PEDESTRIAN CROSSINGS |
| 442201-03 | CLASS C AND D PATCHES |
| 482001-02 | HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT |
| 542301-03 | PRECAST REINFORCED CONCRETE FLARED END SECTION |
| 542401-01 | METAL END SECTION FOR PIPE CULVERTS |
| 604036-02 | GRATE TYPE 8 |
| 606001-05 | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER |
| 606301-04 | PC CONCRETE ISLANDS AND MEDIANS |
| 610001-06 | SHOULDER INLET WITH CURB |
| 630001-10 | STEEL PLATE BEAM GUARDRAIL |
| 630101-09 | GUARDRAIL MOUNTED ON EXISTING CULVERTS |

HIGHWAY STANDARDS

| STANDARD NO. | DESCRIPTION |
|--------------|---|
| 630301-06 | SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS |
| 631011-09 | TRAFFIC BARRIER TERMINAL, TYPE 2 |
| 631026-05 | TRAFFIC BARRIER TERMINAL, TYPE 5 |
| 631031-12 | TRAFFIC BARRIER TERMINAL, TYPE 6 |
| 635001-01 | DELINEATORS |
| 635006-03 | REFLECTOR AND TERMINAL MARKER PLACEMENT |
| 635011-02 | REFLECTOR MARKER AND MOUNTING DETAILS |
| 701101-04 | OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24' (600 mm) FROM PAVEMENT EDGE |
| 701106-02 | OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY |
| 701411-08 | LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS \geq 45 MPH |
| 701422-06 | LANE CLOSURE, MULTILANE, FOR SPEEDS \geq 45 MPH TO 55 MPH |
| 701426-06 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS \geq 45 MPH |
| 701601-09 | URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN |
| 701901-03 | TRAFFIC CONTROL DEVICES |
| 780001-04 | TYPICAL PAVEMENT MARKINGS |
| 781001-03 | TYPICAL APPLICATIONS OF RAISED REFLECTIVE PAVEMENT MARKERS |
| 886001-01 | DETECTOR LOOP INSTALLATIONS |
| 886006-01 | TYPICAL LAYOUTS FOR DETECTION LOOPS |

| | | | | | | | | | | | |
|--|---------------------|------------|-----------|---|----------------------------------|--------------------|---------------------|--------------|---------------------------|-----------|--|
| FILE NAME * | USER NAME * dodgens | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | LIST OF HIGHWAY STANDARDS | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ai\p-work\psdot\dodgens\08267071\0572938-shi-gennote.dgn | | DRAWN - | REVISED - | | | 704 | (57-TRS-1&RS-3 | McLean | 273 | 3 | |
| PLOT SCALE * 48.0000' / in. | | CHECKED - | REVISED - | | | CONTRACT NO. 70838 | | | | | |
| PLOT DATE * 6/11/2014 | | DATE - | REVISED - | | | SCALE: | SHEET 1 OF 1 SHEETS | STA. TO STA. | ILLINOIS FED. AID PROJECT | | |

GENERAL NOTES

G.N.-100
ENGLISH UNITS OF MEASUREMENT SHALL GOVERN OVER AND SUPERSEDE ANY METRIC UNITS SHOWN IN THIS CONTRACT. WHERE INCLUDED, METRIC UNITS ARE FOR INFORMATION ONLY.

G.N.-100A
ELECTRONIC FILES AND/OR ELECTRONIC SURVEY INFORMATION INCLUDING CADD FILES WILL NOT BE AVAILABLE TO THE CONTRACTOR.

G.N.-105.07B
EXISTING STATE-OWNED AND MAINTAINED UTILITY LINES ARE SHOWN ON THE PLANS TO INDICATE THEIR PRESENCE AND APPROXIMATE LOCATION. THE CONTRACTOR SHALL SECURE AN APPROVED LOCATING FIRM TO LOCATE STATE-OWNED UTILITIES PRIOR TO COMMENCING ANY EXCAVATION IN THE VICINITY OF THESE LINES IN ACCORDANCE TO SECTION 803 OF THE STANDARD SPECIFICATIONS. SHOULD ANY OF THE LINES BE DAMAGED BY THE CONTRACTOR'S OPERATION, THE CONTRACTOR SHALL REPAIR THEM TO THE SATISFACTION OF THE ENGINEER AT NO COST TO THE STATE.

ALSO THERE MAY BE UTILITIES PRESENT WHICH WERE INSTALLED BY THE STATE BUT ARE MAINTAINED BY OTHERS (CITY, TOWN, ETC.) THE APPROXIMATE LOCATIONS OF THESE LINES ARE ALSO SHOWN ON THE PLANS ALONG WITH THE NAME OF THE MAINTAINING AGENCY. THE CONTRACTOR SHALL COORDINATE THE LOCATION OF THESE LINES WITH THE LOCAL AGENCY PRIOR TO COMMENCING ANY EXCAVATION OR BORING IN THEIR VICINITY. SHOULD THESE LINES BE DAMAGED BY THE CONTRACTOR'S OPERATIONS, THE CONTRACTOR SHALL REPAIR THEM TO THE SATISFACTION OF, AND AT NO COST TO, THE LOCAL AGENCY AND THE STATE.

G.N.-105.09A
ALL ELEVATIONS SHOWN IN THE PLANS ARE BASED ON NORTH AMERICAN VERTICAL DATUM OF 1988. (NAVD 88)

G.N.-107.12
THE NAME, ADDRESS AND TELEPHONE NUMBER OF THE LOCAL RAILROAD CONTACT IS:
MS. CRYSTAL FRY
DIVISION OFFICE MANAGER - ILLINOIS DIVISION
NORFOLK SOUTHERN RAILWAY COMPANY
1735 EAST CONDUIT STREET
DECATUR, IL. 62521
(217) 425-2078

SPECIAL ATTENTION IS CALLED TO ARTICLE 107.12 REGARDING RAILROAD FLAGGERS. THE NAME, ADDRESS AND TELEPHONE NUMBER OF THE RAILROAD CONTACT PERSON FOR FLAGGERS IS:
MS. CRYSTAL FRY
DIVISION OFFICE MANAGER - ILLINOIS DIVISION
NORFOLK SOUTHERN RAILWAY COMPANY
1735 EAST CONDUIT STREET
DECATUR, IL. 62521
(217) 425-2078

SOME RAILROADS REQUIRE CONTRACTORS TO OBTAIN A RIGHT OF ENTRY BEFORE ENTERING ON RAILROAD RIGHT OF WAY. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN A RAILROAD RIGHT OF ENTRY PERMIT IF REQUIRED BY THE RAILROAD(S).

G.N.-107.37
UTILITY LINES WERE PLOTTED FROM INFORMATION FURNISHED BY THE VARIOUS UTILITY COMPANIES INVOLVED (QUALITY LEVEL C &/OR QUALITY LEVEL D) AND THE ACCURACY SHOULD BE CONSIDERED APPROXIMATE ONLY.

UTILITY COMPANIES MAY BE ADJUSTING THEIR FACILITIES DURING CONSTRUCTION. THE CONTRACTOR SHALL COOPERATE WITH THESE ORGANIZATIONS WHILE THESE ADJUSTMENTS ARE BEING PERFORMED. J.U.L.I.E. - JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS SYSTEM (800) 892-0123 OR 811.

G.N.-201
TREES THAT INTERFERE WITH THE CONSTRUCTION OPERATIONS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER. ANY TREE DUE TO ITS LOCATION AND DEEMED SUITABLE FOR SAVING BY THE ENGINEER SHALL BE PROTECTED DURING CLEARING AND SUBSEQUENT CONSTRUCTION OPERATIONS.

G.N.-202
GRADING SHALL BE DONE BY HAND AROUND LIGHT POLES, UTILITY POLES, SIGN POSTS, SHRUBS, TREES OR OTHER NATURAL OR MAN-MADE OBJECTS WHERE SHALLOW FILLS OR CUTS ARE ADJACENT TO THE ITEMS. IT IS THE INTENT THAT THE LIMITS OF CONSTRUCTION BE SUCH AS TO PRESERVE IN THE ORIGINAL STATE AS MUCH AREA OF TEMPORARY EASEMENTS AS POSSIBLE. THE DECISION AS TO ITEMS TO REMAIN IN PLACE SHALL BE AS DIRECTED BY THE ENGINEER.

THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE PER CUBIC YARD FOR EARTH EXCAVATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

G.N.-205 - SPL
BENCHING PROCEDURES SHALL BE USED IN AREAS WHERE EXISTING EMBANKMENTS ARE WIDENED. STEPS SHALL BE CUT INTO THE EXISTING EMBANKMENT SLOPES AND SHALL HAVE THE FOLLOWING DIMENSIONS:
HORIZONTAL: 2
VERTICAL: 1

G.N.-250C - SPL
SEEDING, CLASS 7 AND MULCH, METHOD 3 IS INCLUDED IN THIS CONTRACT TO SEED NEW EMBANKMENT DURING TIME PERIODS WHEN PERMANENT SEEDING IS NOT ALLOWED. SOME OR ALL OF THE CLASS 7 SEEDING AND MULCH WILL BE DELETED IF IT IS POSSIBLE TO PLACE PERMANENT SEEDING ON THE EMBANKMENT AT THE TIME OF THEIR COMPLETION.

G.N.-280
TEMPORARY EROSION CONTROL SEEDING IS INCLUDED IN THIS CONTRACT TO SEED DISTURBED EARTH DURING TIME PERIODS WHEN PERMANENT SEEDING IS NOT ALLOWED. SOME OR ALL OF THE TEMPORARY EROSION CONTROL SEEDING WILL BE DELETED IF IT IS POSSIBLE TO PLACE PERMANENT SEEDING ON EARTH AT THE TIME OF THEIR COMPLETION.

G.N.-280A
THE VARIOUS MULCH PAY ITEMS IN THE PLANS INCLUDE QUANTITIES FOR TEMPORARY MULCH FOR EROSION CONTROL. THE TEMPORARY MULCH INCLUDES MAINTENANCE AND REMOVAL IF NECESSARY, PER THE REQUIREMENTS OF ARTICLE 280 OF THE STANDARD SPECIFICATIONS, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. SOME OR ALL OF THE MULCH USED AS TEMPORARY EROSION CONTROL WILL BE DELETED IF IT IS NOT NECESSARY DUE TO ESTABLISHMENT OF PERMANENT SEEDING.

G.N.-406
THE QUANTITIES INCLUDED IN THE PLANS FOR HOT-MIX ASPHALT RESURFACING ARE INTENDED TO GIVE THE COVERAGE SHOWN ON THE TYPICAL CROSS SECTIONS. IT IS NOT INTENDED TO INCREASE THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURE IN ORDER TO USE ALL OF THE QUANTITIES INCLUDED IN THE CONTRACT.

| | | | | | | | | | | | |
|--|---|--|--|---|---|--------------------|-----------------------------|------------------|---------------------|----------------|---|
| FILE NAME = c:\pwork\p1\dot\dodge\10570930-eh-genna.dgn | USER NAME = dodgema PLOT SCALE = 40,0000' / 1" | DESIGNED - DRAWN - CHECKED - DATE - | REVISED - REVISED - REVISED - REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GENERAL NOTES SCALE: SHEET 1 OF 3 SHEETS STA. TO STA. | F.A.P. RTE. 704 | SECTION (57-7)RS-1&1RS-3 | COUNTY McLean | TOTAL SHEETS 273 | SHEET NO. 4 | ILLINOIS FED. AID PROJECT CONTRACT NO. 70838 |
|--|---|--|--|---|---|--------------------|-----------------------------|------------------|---------------------|----------------|---|

GENERAL NOTES

G.N.-406.05b
ALL LEVELING BINDER OR BINDER SHALL BE GIVEN A FOG COAT OF PRIME BEFORE THE SURFACE COURSE IS PLACED WHEN DIRECTED BY THE ENGINEER.

THE FOG COAT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER GALLON FOR BITUMINOUS MATERIAL (PRIME COAT) AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

G.N.-406.10
FOR MULTILANE RESURFACING

WHEN BEGINNING THE RESURFACING WITH NEW MIXTURES FOR LEVELING BINDER, BINDER COURSE, AND SURFACE COURSE MIXTURES, THE WORK WILL BE CONFINED TO THE INSIDE TRAFFIC LANE (PASSING LANE) FIRST. THE WORK WILL REMAIN ON THE INSIDE LANE UNTIL THE MIX HAS BEEN ADJUSTED AND APPROVED BY THE ENGINEER BEFORE ANY RESURFACING IS ALLOWED ON THE OUTSIDE (DRIVING) TRAFFIC LANE(S).

ANY DELAYS OR INCONVENIENCES CAUSED THE CONTRACTOR IN COMPLYING WITH THIS REQUIREMENT WILL BE CONSIDERED INCIDENTAL TO THE VARIOUS HOT-MIX ASPHALT PAY ITEMS, AS SHOWN IN THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

GN 406H MIXTURE REQUIREMENTS CONTRACT: 70838

| Location | Bus 55 | Bus 55 | Bus 55 |
|---------------------|----------------------------------|-----------------|---------------------|
| Mixture Use | Polymer Fine Graded Level Binder | Polymer Surface | Partial Depth Patch |
| AC/PG | SBS PG 70-22 | SBS PG 70-22 | PG 64-22 |
| RAP % (Max) | 10 | 10 | 10 |
| Design Air Voids | 4.0% @ Ndes=90 | 4.0% @ Ndes=90 | 4.0% @ Ndes=90 |
| Mix Comp(Gradation) | IL 9.5 FG | IL 9.5 | IL 9.5 |
| Friction Aggregate | Mix C | Mix E | Mix C |

| Location | Bus 55 | Bus 55 | Bus 55 |
|---------------------|----------------|---------------------------------|--------------------|
| Mixture Use | Class D Patch | Bottom lifts HMA shldr >=2 1/4" | Top lift HMA Shldr |
| AC/PG | PG 64-22 | PG 64-22 | PG 64-22 |
| RAP % (Max) | . | . | . |
| Design Air Voids | 4.0% @ Ndes=90 | 2.0% @ Ndes=30 | 4.0% @ Ndes=30 |
| Mix Comp(Gradation) | IL 19.0 F.G. | Other | IL 9.5L |
| Friction Aggregate | N.A. | N.A. | Mix C |

* See RAP/RAS Special Provision

G.N.-406K

THERE ARE VARIOUS PAY ITEMS IN THIS CONTRACT THAT INCLUDE THE USE OF HOT-MIX ASPHALT. UNLESS OTHERWISE LISTED BELOW THE HOT-MIX ASPHALT USED IN THE PAY ITEMS SHALL BE CONTROLLED AND ACCEPTED IN ACCORDANCE WITH ARTICLE 1030.05 "QUALITY CONTROL/QUALITY ASSURANCE (QC/QA)" OF THE STANDARD SPECIFICATIONS.

| PAY CODE | ITEM DESCRIPTION | CONTROL AND ACCEPTANCE METHOD |
|----------|---------------------------------|-------------------------------|
| 40600909 | Poly Level Binder IL 9.5 FG N90 | QCP |
| 40603570 | Poly HMA Surface Mix E N90 | QCP |
| | | |
| | | |

PPF - Hot Mix Asphalt Pay for Performance Using Percent within Limits
QCP - Hot Mix Asphalt Quality Control for Performance
QCAA - Hot Mix Asphalt - Assurance and Acceptance

G.N.-440B
THE EXISTING TIE BARS BETWEEN THE EXISTING PAVEMENT AND EXISTING MEDIANS, GUTTERS AND/OR COMBINATION CURB AND GUTTERS THAT ARE FOUND SUITABLE FOR REUSE SHALL BE CLEANED, STRAIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY EXISTING TIE BARS THAT ARE FOUND UNSUITABLE TO BE INCORPORATED INTO THE PROPOSED CONSTRUCTION DUE TO EXCESSIVE RUSTING OR DISTRESS SHALL BE REMOVED FLUSH WITH THE FACE OF THE EXISTING CONCRETE AND DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY IN ACCORDANCE WITH ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS.

THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE VARIOUS REMOVAL PAY ITEMS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

G.N.-442B - PATCHING SCHEDULES
THE PATCHING SCHEDULES INCLUDED IN THE PLANS REPRESENT THE BEST INFORMATION AVAILABLE AT THE TIME OF COMPLETION OF THE PLANS FOR LETTING. VARIATIONS IN LOCATION AND SIZES OF BOTH FULL-DEPTH AND PARTIAL-DEPTH PATCHES MAY OCCUR.

G.N.-482
ALL MATERIAL PLACED AS HOT-MIX ASPHALT SHOULDERS SHALL BE COMPACTED FROM 94.0 TO 98.4 PERCENT OF THE MAXIMUM THEORETICAL DENSITY. THIS REQUIREMENT SHALL APPLY TO IL 9.5L GRADATION SHOULDER MIXES AND OTHER MIXES (BOTTOM LIFT OF SHOULDERS). THIS MAXIMUM DENSITY SHALL BE DETERMINED FROM THE MOVING AVERAGE OF FOUR TESTS AS IN OTHER QC/QA TESTING. A NUCLEAR GAUGE DENSITY/CORE CORRELATION SHALL BE PERFORMED FOR THE IL 9.5L MIXES AND OTHER MIXES USING STANDARD CORRELATION PROCEDURES.

G.N.-542
BEFORE ORDERING PIPE CULVERTS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR THE EXACT LENGTHS.

G.N.-542.07
AT LOCATIONS WHERE END SECTIONS ARE SPECIFIED, CAST-IN-PLACE CONCRETE HEADWALLS WILL NOT BE ALLOWED.

G.N.-609
PRIOR TO ROUTING TRAFFIC ONTO THE SHOULDERS AS SHOWN IN THE STAGING PLANS, THE CONTRACTOR SHALL SECURE THE GRATINGS ON SHOULDER INLETS AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE PAID FOR ACCORDING TO ARTICLE 109.04.

G.N.-631
IF THE CONTRACTOR ELECTS TO USE THE ALTERNATE MOUNTING METHOD OF THRU DRILLING THE MOUNTING HOLES FOR THE TRAFFIC BARRIER TERMINALS, TYPE 6, THE HOLES SHALL BE DRILLED USING A CORE DRILL. A HAMMER DRILL WILL NOT BE ALLOWED.

G.N.-667
THE RESIDENT ENGINEER SHALL CONTACT THE PROGRAM DEVELOPMENT CHIEF OF SURVEYS PRIOR TO THE PRE-CONSTRUCTION CONFERENCE FOR INSTRUCTION AS TO SETTING OF TEMPORARY OR PERMANENT TIES FOR CENTERLINE ALIGNMENT CONTROL SURVEY MARKERS (PC'S, PT'S, AND PI'S). PROJECT IMPLEMENTATION PERSONNEL WILL BE RESPONSIBLE FOR LAYOUT OF THESE MARKERS.

G.N.-703A
SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE PAVEMENT AFTER ANY OF THE FOLLOWING: COLD MILLING AND/OR PLACING BITUMINOUS MATERIALS (PRIME COAT), LEVELING BINDER (MACHINE METHOD), BINDER AND SURFACE COURSES. SHORT TERM PAVEMENT MARKING PLACED ON THE SURFACE, SHALL COINCIDE WITH THE FINAL PAVEMENT STRIPING. SHORT TERM PAVEMENT MARKING PLACED PRIOR TO THE SURFACE SHALL COINCIDE WITH THE EXISTING PAVEMENT MARKINGS. USE 4 FEET PER 40 FEET (OR 10% PER STATION).

GENERAL NOTES

G.N.-781- SPL
 RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH STANDARD 781001, AND THE DETAILS SHOWN IN THE PLANS. IF THERE IS ANY DISCREPANCY BETWEEN THE STANDARD AND THE DETAILS IN THE PLANS, THE DETAILS IN THE PLANS SHALL GOVERN. THE FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING THE RAISED REFLECTIVE PAVEMENT MARKERS.

PAVEMENT MARKERS ON F.A.P. 704 SHALL BE PLACED TWO (EACH) AT EVEN SPACING IN EVERY OTHER 30 FOOT (9 m) SPACE BETWEEN THE DASHED CENTERLINE STRIPES.

G.N.-873
 EXISTING DETECTOR LOOPS IN THE AREAS OF PROPOSED SURFACE REMOVAL SHALL BE REPLACED PER THE EXISTING SIZE AND LOCATION EXCEPT AS NOTED IN THE PLANS. EXISTING DETECTOR LOOPS SHALL BE DISCONNECTED AT THE GULFBOX JUNCTION OR HANDHOLE PRIOR TO COLD MILLING AT THAT RESPECTIVE LOCATION. NEW DETECTOR LOOPS SHALL BE CONNECTED TO THE RESPECTIVE EXISTING AMPLIFIER VIA THE CORRESPONDING EXISTING LEAD - IN CABLE.

G.N.-1004.01
 COARSE AGGREGATE GRADATION CA-10 MAY BE USED WHENEVER COARSE AGGREGATE CA-6 IS SPECIFIED IN THE STANDARD SPECIFICATIONS

G.N. SPL
 UNLESS DIRECTLY SPECIFIED IN THE PLAN DOCUMENTS OR DIRECTED OTHERWISE BY THE ENGINEER, THE CONTRACTOR SHALL LAYOUT PROPOSED GUARDRAIL POST LOCATIONS TO AVOID ANY CONFLICT WITH EXISTING OR PROPOSED CROSS ROAD CULVERTS, PIPES, PIPE DRAINS, DRAINAGE STRUCTURES OR OTHER APPURTENANCES. THE COST OF THIS CONSIDERATION TO BE INCLUDED IN THE COST OF THE VARIOUS GUARDRAIL ITEMS INVOLVED WITH NO ADDITIONAL COMPENSATION ALLOWED.

NO COMMITMENTS

| | | | | | | | | | | | |
|---|-------------------------------|------------|-----------|---|----------------------|---|-----------------|--------------------|-----------------|--------------|--|
| FILE NAME = | USER NAME = dodgema | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GENERAL NOTES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| c:\pwork\pvidoc\dodgema\0267571\0570338-ent-gennote.dgn | PLOT SCALE = 40,0000 1" = 40' | DRAWN - | REVISED - | | | 704 | (57-7)RS-1&RS-3 | McLean | 273 | 6 | |
| #MODELNAME# | PLOT DATE = 6/11/2014 | CHECKED - | REVISED - | | | SCALE: SHEET 3 OF 3 SHEETS STA. TO STA. | | CONTRACT NO. 70838 | | | |
| | | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | |

| | | | |
|-------------------------|---------------------------------|--------------------------------|--------------------------------|
| LOCATION OF WORK: | FAP 704 (I-55BUSN/VET' S PKWY) | S. N. 057-0063 | S. N. 057-0064 |
| | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE |
| | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL |
| | STA. 33+30 TO STA. 89+41.79 | STA. 47+06.97 TO STA. 49+10.79 | STA. 72+79.68 TO STA. 75+96.94 |
| | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY |
| FUNDING BREAKOUT: | 100% STATE | 100% STATE | 100% STATE |
| CONSTRUCTION TYPE CODE: | 0004 | 0014 | 0014 |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | |
|----------|--|-------|----------------|----------|-----|
| 20100110 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT | 35.0 | 35.0 | 0.0 |
| 20100210 | TREE REMOVAL (OVER 15 UNITS DIAMETER) | UNIT | 25.0 | 25.0 | 0.0 |
| 20200100 | EARTH EXCAVATION | CU YD | 1,540.0 | 1,540.0 | 0.0 |
| 25000210 | SEEDING, CLASS 2A | ACRE | 2.00 | 2.00 | 0.0 |
| 25000350 | SEEDING, CLASS 7 | ACRE | 6.00 | 6.00 | 0.0 |
| 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 170.0 | 170.0 | 0.0 |
| 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 170.0 | 170.0 | 0.0 |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 170.0 | 170.0 | 0.0 |
| 25100125 | MULCH, METHOD 3 | ACRE | 6.00 | 6.00 | 0.0 |
| 25100635 | HEAVY DUTY EROSION CONTROL BLANKET | SQ YD | 8,620.0 | 8,620.0 | 0.0 |
| 28000250 | TEMPORARY EROSION CONTROL SEEDING | POUND | 1,080.0 | 1,080.0 | 0.0 |
| 28000305 | TEMPORARY DITCH CHECKS | FOOT | 840.0 | 840.0 | 0.0 |
| 28000400 | PERIMETER EROSION BARRIER | FOOT | 13,190.0 | 13,190.0 | 0.0 |
| 28000500 | INLET AND PIPE PROTECTION | EACH | 15.0 | 15.0 | 0.0 |

W • DENOTES SPECIALTY ITEM

| | | | | | | | | | | | | | |
|---|---------------------|------------|-----------|---|------------------------------|---------|-------|--------|-----------------------|-----------------------------|--------------------|------------------------|-------------------|
| FILE NAME : | USER NAME = dodgene | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES | | | | F.A.P. RTE. 704 | SECTION (57-7)RS-1&1RS-3 | COUNTY McLean | TOTAL SHEETS 273 | SHEET NO. 7 |
| o:\p\work\p\dodgene\026797\10572830-shr-800.dgn | | DRAWN - | REVISED - | | SCALE: | SHEET 1 | OF 10 | SHEETS | STA. | TO STA. | CONTRACT NO. 70838 | | |
| | | CHECKED - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | | |
| #MODELNAME# | | DATE - | REVISED - | | | | | | | | | | |

LOCATION OF WORK: FAP 704 (I-55BUSN/VET'S PKWY) S. N. 057-0063 S. N. 057-0064
 URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE
 OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL
 STA. 33+30 TO STA. 89+41. 79 STA. 47+06. 97 TO STA. 49+10. 79 STA. 72+79. 68 TO STA. 75+96. 94
 MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY
 FUNDING BREAKOUT: 100% STATE 100% STATE 100% STATE
 CONSTRUCTION TYPE CODE: 0004 0014 0014

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | |
|---------------------|---|-------------------|---------------------|---------------------|----------------|
| 28100107 | STONE RIPRAP, CLASS A4 | SQ YD | 420.0 | 420.0 | 0.0 |
| 28200200 | FILTER FABRIC | SQ YD | 420.0 | 420.0 | 0.0 |
| 40600100 | BITUMINOUS MATERIALS (PRIME COAT) | GALLON | 15,070.0 | 15,070.0 | 0.0 |
| 40600275 | BITUMINOUS MATERIALS (PRIME COAT) | POUND | 52,000 | 52,000 | 0.0 |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 135.0 | 135.0 | 0.0 |
| 40600909 | POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1L-9.5FG, N90 | TON | 4,480.0 | 4,480.0 | 0.0 |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 2,060.0 | 2,060.0 | 0.0 |
| 40600985 | PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT | SQ YD | 790.0 | 790.0 | 0.0 |
| 40600990 | TEMPORARY RAMP | SQ YD | 1,970.0 | 1,970.0 | 0.0 |
| 40603570 | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90 | TON | 7,100.0 | 7,100.0 | 0.0 |
| 42400100 | PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH | SQ FT | 785.0 | 785.0 | 0.0 |
| 42400800 | DETECTABLE WARNINGS | SQ FT | 245.0 | 245.0 | 0.0 |
| 44000155 | HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" | SQ YD | 45,835.0 | 45,835.0 | 0.0 |
| 44000156 | HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4" | SQ YD | 575.0 | 575.0 | 0.0 |
| 44000162 | HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/4" | SQ YD | 8,265.0 | 8,265.0 | 0.0 |

* DENOTES SPECIALTY ITEM

LOCATION OF WORK: FAP 704 (I-55BUSN/VET' S PKWY) S. N. 057-0063 S. N. 057-0064
 URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE
 OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL
 STA. 33+30 TO STA. 89+41.79 STA. 47+06.97 TO STA. 49+10.79 STA. 72+79.68 TO STA. 75+96.94
 MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY
 FUNDING BREAKOUT: 100% STATE 100% STATE 100% STATE
 CONSTRUCTION TYPE CODE: 0004 0014 0014

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | | |
|----------|-------------------------------------|-------|----------------|---------|-------|-------|
| 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 30.0 | 30.0 | 0.0 | 0.0 |
| 44000600 | SIDEWALK REMOVAL | SQ FT | 785.0 | 785.0 | 0.0 | 0.0 |
| 44004000 | PAVED DITCH REMOVAL | FOOT | 31.0 | 31.0 | 0.0 | 0.0 |
| 44004250 | PAVED SHOULDER REMOVAL | SQ YD | 1,310.0 | 1,310.0 | 0.0 | 0.0 |
| 44201753 | CLASS D PATCHES, TYPE II, 9 INCH | SQ YD | 995.0 | 995.0 | 0.0 | 0.0 |
| 44213198 | TIE BARS 1/2" | EACH | 120.0 | 120.0 | 0.0 | 0.0 |
| 48101200 | AGGREGATE SHOULDERS, TYPE B | TON | 150.0 | 150.0 | 0.0 | 0.0 |
| 48101500 | AGGREGATE SHOULDERS, TYPE B 6" | SQ YD | 4,160.0 | 4,160.0 | 0.0 | 0.0 |
| 48203029 | HOT-MIX ASPHALT SHOULDERS, 8" | SQ YD | 5,220.0 | 5,220.0 | 0.0 | 0.0 |
| 48203100 | HOT-MIX ASPHALT SHOULDERS | TON | 1,430.0 | 1,430.0 | 0.0 | 0.0 |
| 50102400 | CONCRETE REMOVAL | CU YD | 15.4 | 1.2 | 7.0 | 7.2 |
| 50300255 | CONCRETE SUPERSTRUCTURE | CU YD | 12.8 | 0.0 | 6.2 | 6.6 |
| 50300300 | PROTECTIVE COAT | SQ YD | 39.0 | 0.0 | 17.5 | 21.5 |
| 50800205 | REINFORCEMENT BARS, EPOXY COATED | POUND | 1,620.0 | 0.0 | 820.0 | 800.0 |

* DENOTES SPECIALTY ITEM

| | | | |
|-------------------------|---------------------------------|--------------------------------|--------------------------------|
| LOCATION OF WORK: | FAP 704 (I-55BUSN/VET' S PKWY) | S. N. 057-0063 | S. N. 057-0064 |
| | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE |
| | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL |
| | STA. 33+30 TO STA. 89+41.79 | STA. 47+06.97 TO STA. 49+10.79 | STA. 72+79.68 TO STA. 75+96.94 |
| | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY |
| FUNDING BREAKOUT: | 100% STATE | 100% STATE | 100% STATE |
| CONSTRUCTION TYPE CODE: | 0004 | 0014 | 0014 |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | | |
|----------|---|-------|----------------|---------|------|------|
| 50800515 | BAR SPLICERS | EACH | 44.0 | 0.0 | 28.0 | 16.0 |
| 54213681 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 36" | EACH | 2.0 | 2.0 | 0.0 | 0.0 |
| 54215547 | METAL END SECTIONS 12" | EACH | 1.0 | 1.0 | 0.0 | 0.0 |
| 54248510 | CONCRETE COLLAR | CU YD | 2.8 | 2.8 | 0.0 | 0.0 |
| 542A1081 | PIPE CULVERTS, CLASS A, TYPE 2 36" | FOOT | 16.0 | 16.0 | 0.0 | 0.0 |
| 54390230 | INSERTION CULVERT LINER 36" | FOOT | 84.0 | 84.0 | 0.0 | 0.0 |
| 60100945 | PIPE DRAINS 12" | FOOT | 20.0 | 20.0 | 0.0 | 0.0 |
| 60261000 | INLETS TO BE ADJUSTED WITH NEW TYPE B GRATE | EACH | 1.0 | 1.0 | 0.0 | 0.0 |
| 60300105 | FRAMES AND GRATES TO BE ADJUSTED | EACH | 32.0 | 32.0 | 0.0 | 0.0 |
| 60500060 | REMOVING INLETS | EACH | 3.0 | 3.0 | 0.0 | 0.0 |
| 60600095 | CLASS SI CONCRETE (OUTLET) | CU YD | 9.2 | 9.2 | 0.0 | 0.0 |
| 60600605 | CONCRETE CURB, TYPE B | FOOT | 5,270.0 | 5,270.0 | 0.0 | 0.0 |
| 60605000 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 | FOOT | 30.0 | 30.0 | 0.0 | 0.0 |
| 60900515 | CONCRETE THRUST BLOCKS | EACH | 1.0 | 1.0 | 0.0 | 0.0 |

* DENOTES SPECIALTY ITEM

| | | | |
|-------------------------|--------------------------------|--------------------------------|--------------------------------|
| LOCATION OF WORK: | FAP 704 (I-55BUSN/VET'S PKWY) | S. N. 057-0063 | S. N. 057-0064 |
| | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE |
| | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL |
| | STA. 33+30 TO STA. 89+41.79 | STA. 47+06.97 TO STA. 49+10.79 | STA. 72+79.68 TO STA. 75+96.94 |
| | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY |
| FUNDING BREAKOUT: | 100% STATE | 100% STATE | 100% STATE |
| CONSTRUCTION TYPE CODE: | 0004 | 0014 | 0014 |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | |
|------------|--|--------|----------------|----------|-----|
| • 63000001 | STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS | FOOT | 362.5 | 362.5 | 0.0 |
| • 63000003 | STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS | FOOT | 812.5 | 812.5 | 0.0 |
| • 63000009 | STEEL PLATE BEAM GUARDRAIL, TYPE B, 9 FOOT POSTS | FOOT | 4,287.5 | 4,287.5 | 0.0 |
| • 63000025 | STEEL PLATE BEAM GUARDRAIL, ATTACHED TO STRUCTURES | FOOT | 25.0 | 25.0 | 0.0 |
| • 63100045 | TRAFFIC BARRIER TERMINAL, TYPE 2 | EACH | 4.0 | 4.0 | 0.0 |
| • 63100167 | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT | EACH | 5.0 | 5.0 | 0.0 |
| • 63100169 | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) FLARED | EACH | 1.0 | 1.0 | 0.0 |
| 63200310 | GUARDRAIL REMOVAL | FOOT | 5,780.0 | 5,780.0 | 0.0 |
| 66201120 | CONCRETE SHOULDER CURB | FOOT | 10.0 | 10.0 | 0.0 |
| 67000500 | ENGINEER'S FIELD OFFICE, TYPE B | CAL MO | 14.0 | 10.0 | 2.0 |
| 67100100 | MOBILIZATION | L SUM | 1.0 | 0.8 | 0.1 |
| 70103815 | TRAFFIC CONTROL SURVEILLANCE | CAL DA | 12.0 | 0.0 | 6.0 |
| 70200100 | NIGHTTIME WORK ZONE LIGHTING | L SUM | 1.0 | 1.0 | 0.0 |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 10,460.0 | 10,460.0 | 0.0 |

• DENOTES SPECIALTY ITEM

| | | | |
|-------------------------|--------------------------------|--------------------------------|--------------------------------|
| LOCATION OF WORK: | FAP 704 (I-55BUSN/VET'S PKWY) | S. N. 057-0063 | S. N. 057-0064 |
| | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE |
| | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL |
| | STA. 33+30 TO STA. 89+41.79 | STA. 47+06.97 TO STA. 49+10.79 | STA. 72+79.68 TO STA. 75+96.94 |
| | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY |
| FUNDING BREAKOUT: | 100% STATE | 100% STATE | 100% STATE |
| CONSTRUCTION TYPE CODE: | 0004 | 0014 | 0014 |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | |
|------------|--|-------|----------------|----------|-------|
| 70300210 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS | SO FT | 514.8 | 514.8 | 0.0 |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 50,580.0 | 50,580.0 | 0.0 |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 740.0 | 740.0 | 0.0 |
| 70300250 | TEMPORARY PAVEMENT MARKING - LINE 8" | FOOT | 3,235.0 | 3,235.0 | 0.0 |
| 70300260 | TEMPORARY PAVEMENT MARKING - LINE 12" | FOOT | 760.0 | 760.0 | 0.0 |
| 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 100.0 | 100.0 | 0.0 |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SO FT | 22,230.0 | 22,230.0 | 0.0 |
| • 78003100 | PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LETTERS AND SYMBOLS | SO FT | 514.8 | 514.8 | 0.0 |
| • 78004210 | PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 4" | FOOT | 4,720.0 | 4,720.0 | 0.0 |
| • 78009004 | MODIFIED URETHANE PAVEMENT MARKING - LINE 4" | FOOT | 45,860.0 | 43,510.0 | 920.0 |
| • 78009006 | MODIFIED URETHANE PAVEMENT MARKING - LINE 6" | FOOT | 740.0 | 740.0 | 0.0 |
| • 78009008 | MODIFIED URETHANE PAVEMENT MARKING - LINE 8" | FOOT | 3,235.0 | 3,235.0 | 0.0 |
| • 78009012 | MODIFIED URETHANE PAVEMENT MARKING - LINE 12" | FOOT | 760.0 | 760.0 | 0.0 |
| • 78009024 | MODIFIED URETHANE PAVEMENT MARKING - LINE 24" | FOOT | 100.0 | 100.0 | 0.0 |

• DENOTES SPECIALTY ITEM

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|---|------------------------------|------------|-----------|---|------------------------------|---------|--------------|--------------|---------------------------|--------|-----------------|
| FILE NAME * | USER NAME = dodgens | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEET NO. |
| c:\p\work\pvidot\dodgens\08267571\0570938-ahc-SC0.dgn | PLOT SCALE = 42,0000 ' / in. | DRAWN - | REVISED - | | | | | 704 | (57-7)RS-1&RS-3 | McLean | 273 12 |
| MODELNAME* | PLOT DATE = 6/11/2014 | CHECKED - | REVISED - | | SCALE: | SHEET 6 | OF 10 SHEETS | STA. TO STA. | CONTRACT NO. 70838 | | |
| | | DATE - | REVISED - | | | | | | ILLINOIS FED. AID PROJECT | | |

| | | | |
|-------------------------|---------------------------------|--------------------------------|--------------------------------|
| LOCATION OF WORK: | FAP 704 (I-55BUSN/VET' S PKWY) | S. N. 057-0063 | S. N. 057-0064 |
| | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE |
| | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL |
| | STA. 33+30 TO STA. 89+41.79 | STA. 47+06.97 TO STA. 49+10.79 | STA. 72+79.68 TO STA. 75+96.94 |
| | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY |
| FUNDING BREAKOUT: | 100% STATE | 100% STATE | 100% STATE |
| CONSTRUCTION TYPE CODE: | 0004 | 0014 | 0014 |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | | |
|--------------------------|---|-------|----------------|----------|-------|-------|
| • 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 730.0 | 730.0 | 0.0 | 0.0 |
| • 78200410 | GUARDRAIL MARKERS, TYPE A | EACH | 40.0 | 40.0 | 0.0 | 0.0 |
| • 78200530 | BARRIER WALL MARKERS, TYPE C | EACH | 2.0 | 0.0 | 0.0 | 2.0 |
| • 78201000 | TERMINAL MARKER - DIRECT APPLIED | EACH | 6.0 | 6.0 | 0.0 | 0.0 |
| 78300100 | PAVEMENT MARKING REMOVAL | SQ FT | 960.0 | 175.0 | 305.0 | 480.0 |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 510.0 | 510.0 | 0.0 | 0.0 |
| • 88600100 | DETECTOR LOOP, TYPE I | FOOT | 440.0 | 440.0 | 0.0 | 0.0 |
| X0323586 | PIPE DRAIN REMOVAL | FOOT | 40.0 | 40.0 | 0.0 | 0.0 |
| X4402400 | CURB REMOVAL (SPECIAL) | FOOT | 4,150.0 | 4,150.0 | 0.0 | 0.0 |
| X4403300 | CONCRETE MEDIAN REMOVAL | SQ FT | 17,225.0 | 17,225.0 | 0.0 | 0.0 |
| X4406060 | PARTIAL DEPTH REMOVAL, TYPE I, 6" | SQ YD | 60.0 | 60.0 | 0.0 | 0.0 |
| X4406070 | PARTIAL DEPTH REMOVAL, TYPE I, 7" | SQ YD | 50.0 | 50.0 | 0.0 | 0.0 |
| X4406080 | PARTIAL DEPTH REMOVAL, TYPE I, 8" | SQ YD | 40.0 | 40.0 | 0.0 | 0.0 |
| X4406090 | PARTIAL DEPTH REMOVAL, TYPE I, 9" | SQ YD | 20.0 | 20.0 | 0.0 | 0.0 |
| • DENOTES SPECIALTY ITEM | | | | | | |

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|---|-----------------------------|------------|-----------|---|------------------------------|--------------------|----------------------|--------|--------------|---------------------------|--|
| FILE NAME = | USER NAME = dodgens | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| er\p\work\p\dot\dodgens\0267571\0570338-sht-500.dgn | PLOT SCALE = 48,0000' / in. | DRAWN - | REVISED - | | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 13 | |
| MODEL NAME = | PLOT DATE = 8/11/2014 | CHECKED - | REVISED - | | | CONTRACT NO. 70838 | | | | | |
| | | DATE - | REVISED - | | | SCALE: | SHEET 7 OF 10 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT | |

| | | | |
|-------------------------|--------------------------------|--------------------------------|--------------------------------|
| LOCATION OF WORK: | FAP 704 (I-55BUSN/VET'S PKWY) | S. N. 057-0063 | S. N. 057-0064 |
| | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE |
| | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL |
| | STA. 33+30 TO STA. 89+41.79 | STA. 47+06.97 TO STA. 49+10.79 | STA. 72+79.68 TO STA. 75+96.94 |
| | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY |
| FUNDING BREAKOUT: | 100% STATE | 100% STATE | 100% STATE |
| CONSTRUCTION TYPE CODE: | 0004 | 0014 | 0014 |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | |
|------------|---|-------|----------------|----------|-----|
| X4406100 | PARTIAL DEPTH REMOVAL, TYPE I, 10" | SQ YD | 160.0 | 160.0 | 0.0 |
| X4406110 | PARTIAL DEPTH REMOVAL, TYPE I, 11" | SQ YD | 80.0 | 80.0 | 0.0 |
| X4406260 | PARTIAL DEPTH REMOVAL, TYPE II, 6" | SQ YD | 620.0 | 620.0 | 0.0 |
| X4406270 | PARTIAL DEPTH REMOVAL, TYPE II, 7" | SQ YD | 540.0 | 540.0 | 0.0 |
| X4406280 | PARTIAL DEPTH REMOVAL, TYPE II, 8" | SQ YD | 400.0 | 400.0 | 0.0 |
| X4406290 | PARTIAL DEPTH REMOVAL, TYPE II, 9" | SQ YD | 170.0 | 170.0 | 0.0 |
| X4406300 | PARTIAL DEPTH REMOVAL, TYPE II, 10" | SQ YD | 1,940.0 | 1,940.0 | 0.0 |
| X4406310 | PARTIAL DEPTH REMOVAL, TYPE II, 11" | SQ YD | 1,010.0 | 1,010.0 | 0.0 |
| X4406500 | PARTIAL DEPTH REMOVAL, TYPE III, 10" | SQ YD | 100.0 | 100.0 | 0.0 |
| X4406510 | PARTIAL DEPTH REMOVAL, TYPE III, 11" | SQ YD | 100.0 | 100.0 | 0.0 |
| X4421000 | PARTIAL DEPTH PATCHING | TON | 2,720.0 | 2,720.0 | 0.0 |
| X6061702 | CONCRETE MEDIAN, TYPE SM (DOWELLED) | SQ FT | 17,225.0 | 17,225.0 | 0.0 |
| X6100120 | TYPE E INLET BOX, STANDARD 610001 (SPECIAL) | EACH | 1.0 | 1.0 | 0.0 |
| • X6310179 | TRAFFIC BARRIER TERMINAL, TYPE 5 (SPECIAL) | EACH | 2.0 | 2.0 | 0.0 |

• DENOTES SPECIALTY ITEM

| | | | | | | | | | | | | | |
|--|------------------------------|------------|-----------|---|--|-----------------|--------|-----|--------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = dodgers | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES | | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwork\p\dat\dodgers\0267071\0972338-sh1-500.dgn | | DRAWN - | REVISED - | | 704 | (57-7)RS-1&RS-3 | McLean | 273 | 14 | | | | |
| MODEL NAME = | PLOT SCALE = 40.0000' / 1" = | CHECKED - | REVISED - | | SCALE: SHEET 8 OF 10 SHEETS STA. TO STA. | | | | CONTRACT NO. 70838 | | | | |
| | PLOT DATE = 8/11/2014 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | | |

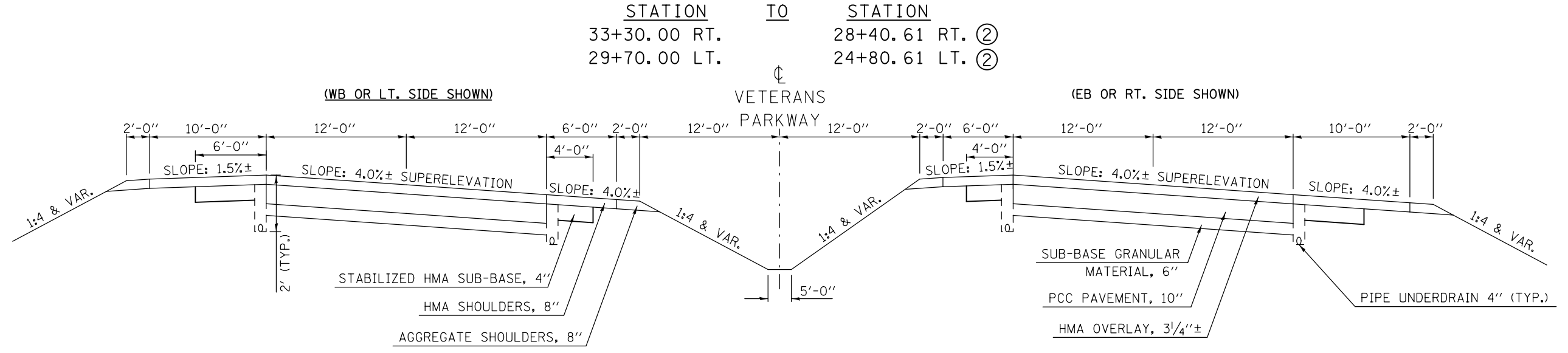
| | | | |
|-------------------------|-------------------------------|--------------------------------|--------------------------------|
| LOCATION OF WORK: | FAP 704 (I-55BUSN/VET'S PKWY) | S. N. 057-0063 | S. N. 057-0064 |
| | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE |
| | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL |
| | STA. 33+30 TO STA. 89+41.79 | STA. 47+06.97 TO STA. 49+10.79 | STA. 72+79.68 TO STA. 75+96.94 |
| | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY |
| FUNDING BREAKOUT: | 100% STATE | 100% STATE | 100% STATE |
| CONSTRUCTION TYPE CODE: | 0004 | 0014 | 0014 |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | | |
|------------|--|--------|----------------|----------|-------|---------|
| • X6310214 | TRAFFIC BARRIER TERMINAL, TYPE 6 (SPECIAL) | EACH | 4.0 | 4.0 | 0.0 | 0.0 |
| X7010216 | TRAFFIC CONTROL AND PROTECTION, (SPECIAL) | L SUM | 1.0 | 0.8 | 0.1 | 0.1 |
| X7015005 | CHANGEABLE MESSAGE SIGN | CAL DA | 70.0 | 0.0 | 35.0 | 35.0 |
| • X7830070 | GROOVING FOR RECESSED PAVEMENT MARKING 5" | FOOT | 45,860.0 | 43,510.0 | 920.0 | 1,430.0 |
| • X7830074 | GROOVING FOR RECESSED PAVEMENT MARKING 7" | FOOT | 740.0 | 740.0 | 0.0 | 0.0 |
| • X7830076 | GROOVING FOR RECESSED PAVEMENT MARKING 9" | FOOT | 3,235.0 | 3,235.0 | 0.0 | 0.0 |
| • X7830078 | GROOVING FOR RECESSED PAVEMENT MARKING 13" | FOOT | 760.0 | 760.0 | 0.0 | 0.0 |
| • X7830090 | GROOVING FOR RECESSED PAVEMENT MARKING 25" | FOOT | 100.0 | 100.0 | 0.0 | 0.0 |
| XZ193400 | SURVEY MARKER, TYPE 2 (SPECIAL) | EACH | 5.0 | 5.0 | 0.0 | 0.0 |
| Z0001800 | APPROACH SLAB REPAIR (PARTIAL DEPTH) | SQ YD | 18.0 | 0.0 | 5.8 | 12.2 |
| Z0012754 | STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES) | SQ FT | 379.4 | 0.0 | 379.4 | 0.0 |
| Z0012800 | CONCRETE PAVEMENT SCARIFICATION | SQ YD | 4590.0 | 4590.0 | 0.0 | 0.0 |
| Z0013798 | CONSTRUCTION LAYOUT | L SUM | 1.0 | 0.8 | 0.1 | 0.1 |
| Z0021907 | SILICONE JOINT SEALER, 1.75" | FOOT | 58.0 | 0.0 | 58.0 | 0.0 |

• DENOTES SPECIALTY ITEM

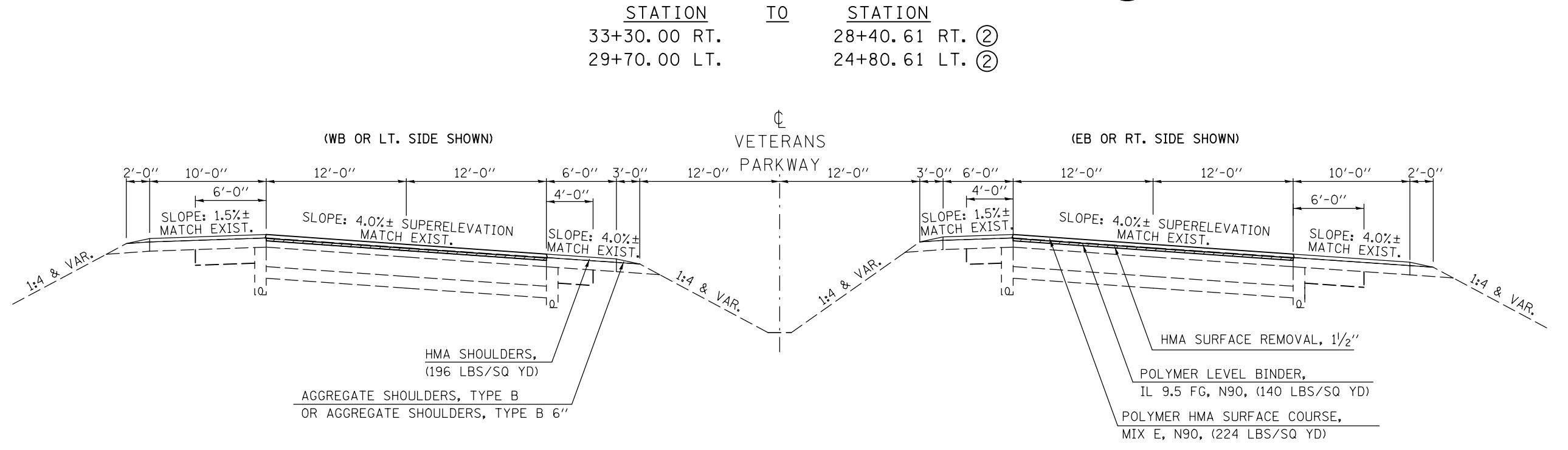
| | | | | | | | | | | | | |
|---|-----------------------------|------------|-----------|---|------------------------------|---------|--------------|------------------|---------------------------|--------|--------------------|-----------|
| FILE NAME # | USER NAME # dodgers | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwork\pvidot\dodgers\d267571\0578038-ahs-500.dgn | DRAWN - | REVISED - | 704 | | | | | (57-7)RS-1&1RS-3 | McLean | 273 | 15 | |
| #MODELNAME# | PLOT SCALE = 48,0000 // in. | CHECKED - | REVISED - | | SCALE: | SHEET 9 | OF 10 SHEETS | STA. TO STA. | ILLINOIS FED. AID PROJECT | | CONTRACT NO. 70838 | |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - | | | | | | | | | |

EXISTING TYPICAL CROSS SECTION ①



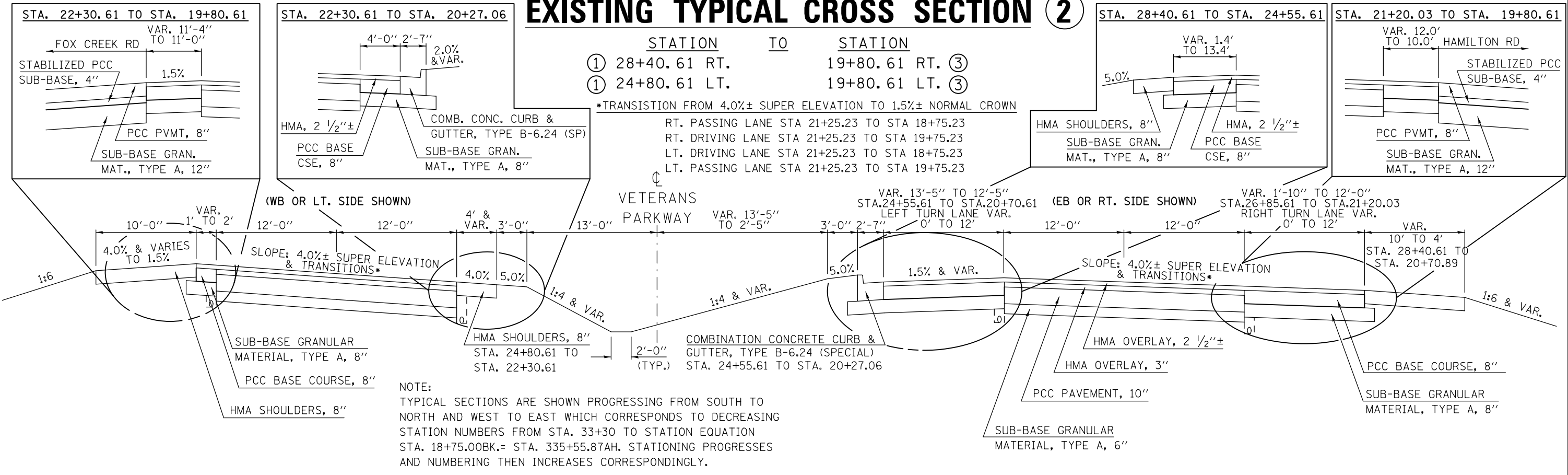
NOTE:
 TYPICAL SECTIONS ARE SHOWN PROGRESSING FROM SOUTH TO NORTH AND WEST TO EAST WHICH CORRESPONDS TO DECREASING STATION NUMBERS FROM STA. 33+30 TO STATION EQUATION STA. 18+75.00BK.= STA. 335+55.87AH. STATIONING PROGRESSES AND NUMBERING THEN INCREASES CORRESPONDINGLY.

PROPOSED TYPICAL CROSS SECTION ①

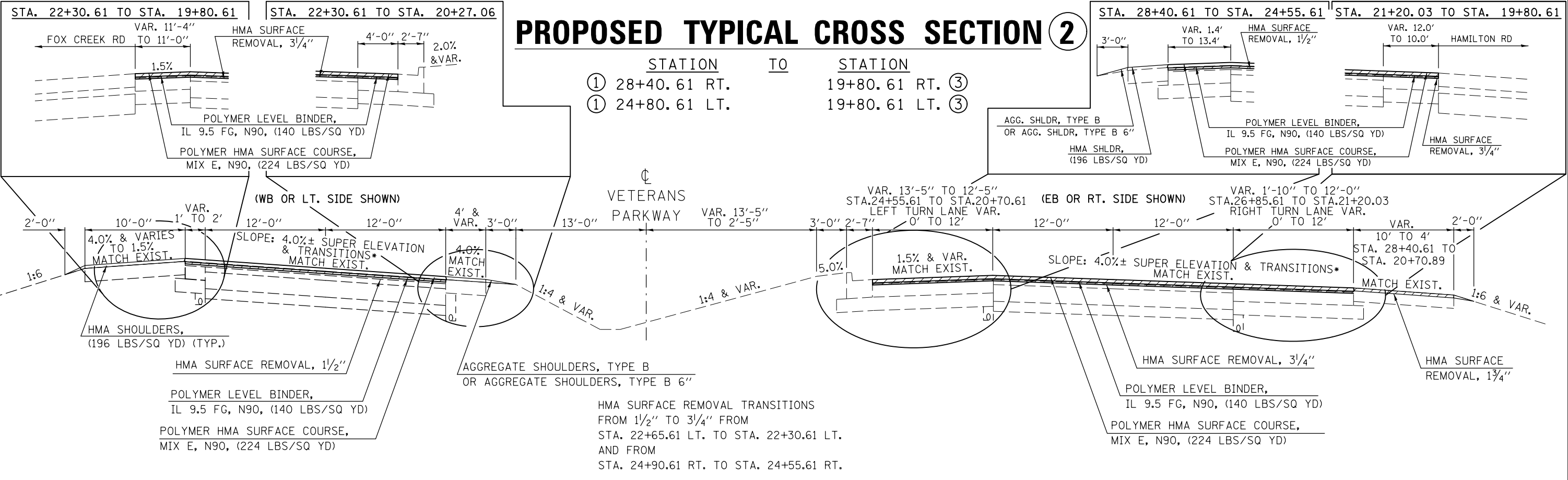


| | | | | | | | | | | | | |
|---|---------------------|------------|-----------|---|-------------------------------|--------------------------|--------|---------------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL CROSS SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht-typicals.dgn | | DRAWN - | REVISED - | | 704 | (57-7RS-1&1RS-3 | McLean | 273 | 17 | | | |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - | | CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | | | | |
| PLOT DATE = 6/11/2014 | | DATE - | REVISED - | | SCALE: | SHEET NO. 1 OF 12 SHEETS | STA. | TO STA. | | | | |

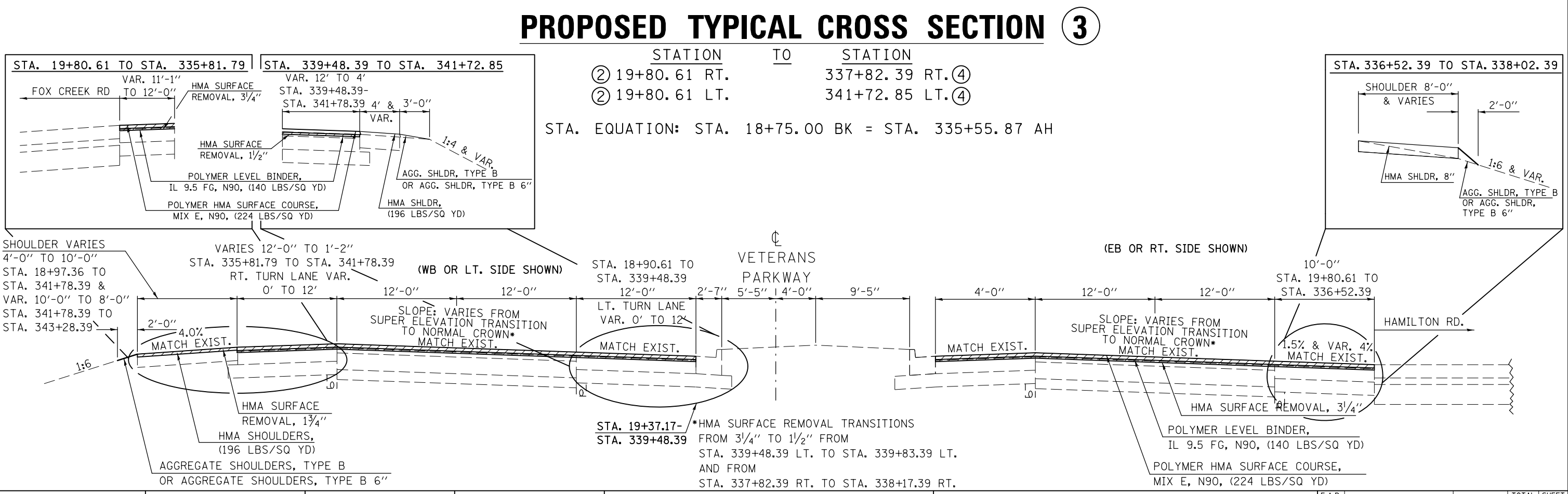
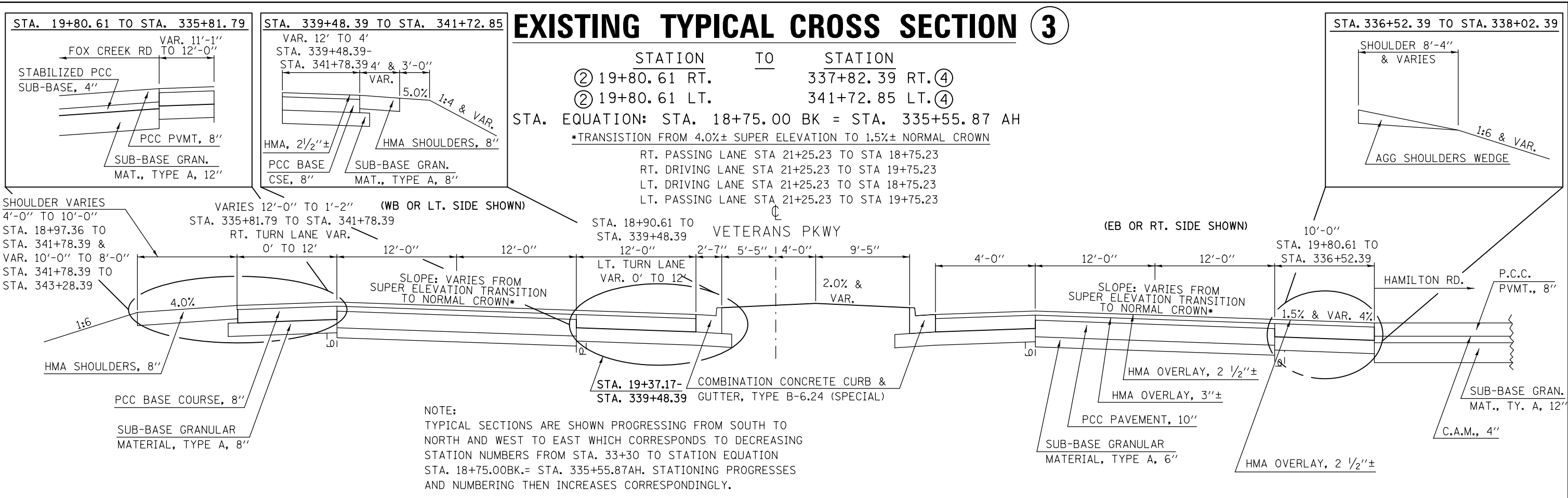
EXISTING TYPICAL CROSS SECTION 2



PROPOSED TYPICAL CROSS SECTION 2



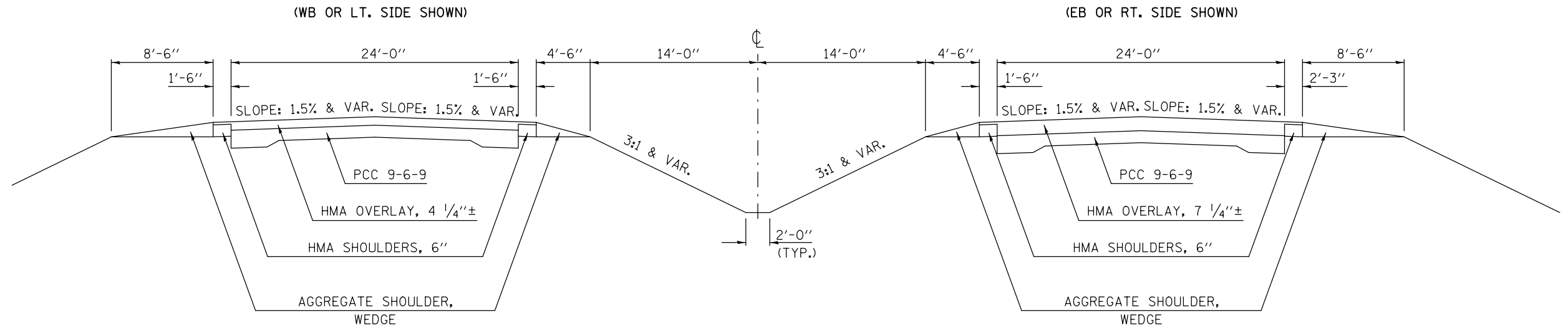
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|---|-----------------------------|------------|-----------|---|--|--|--|--------------------|-----------------|---------------------------|--------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL CROSS SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht-typicals.dgn | PLOT SCALE = 40.0000' / in. | DRAWN - | REVISED - | | | | | 704 | (57-7RS-1&1RS-3 | McLean | 273 | 18 |
| PLOT DATE = 6/11/2014 | DATE - | CHECKED - | REVISED - | | SCALE: SHEET NO. 2 OF 12 SHEETS STA. TO STA. | | | CONTRACT NO. 70838 | | | | |
| | | | | | | | | | | ILLINOIS FED. AID PROJECT | | |



| | | | | | | | | | | | |
|---|---------------------|------------|-----------|---|-------------------------------|---------------------------|-----------------|--------|--------------|-----------|--|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL CROSS SECTIONS | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ci:\pwwork\pwwork\dodgems\d0267571\0570838-sht-typicals.dgn | | DRAWN - | REVISED - | | | 704 | (57-7RS-1&1RS-3 | McLean | 273 | 19 | |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - | | | CONTRACT NO. 70838 | | | | | |
| PLOT DATE = 6/11/2014 | | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | |

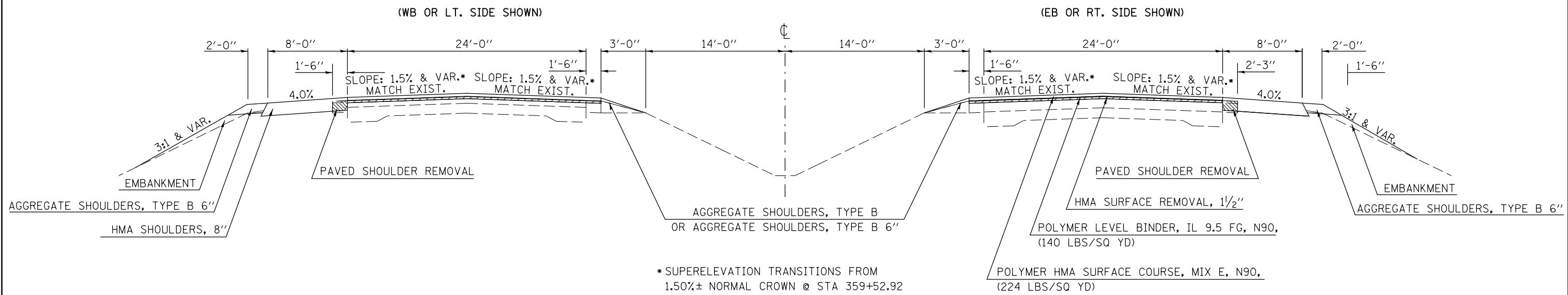
EXISTING TYPICAL CROSS SECTION ④

STATION TO STATION
 ③ 337+82.39 RT. 361+10.00 RT. ⑤
 ③ 341+72.85 LT. 361+10.00 LT. ⑤



PROPOSED TYPICAL CROSS SECTION ④

STATION TO STATION
 ③ 337+82.39 RT. 361+10.00 RT. ⑤
 ③ 341+72.85 LT. 361+10.00 LT. ⑤



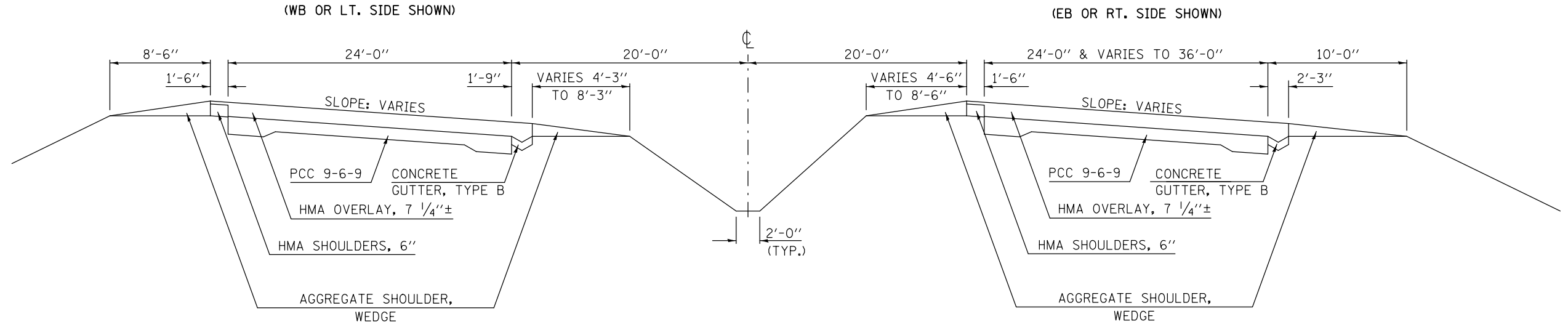
* SUPERELEVATION TRANSITIONS FROM 1.50%± NORMAL CROWN @ STA 359+52.92 TO 2.20% SUPERELEVATION @ STA 361+52.92

PAVED SHOULDER REMOVAL
 STA. 336+52.39 RT. - STA. 361+10.00 RT.
 STA. 341+78.39 LT. - STA. 361+10.00 LT.

| | | | | | | | | | | |
|--|-----------------------------|------------|-----------|---|-------------------------------|-------------|--------------------------|--------|--------------------|---------------------------|
| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL CROSS SECTIONS | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ct:\pwork\pwork\dot\dodgms\d0267571\0570838-sht-typicals.dgn | | DRAWN - | REVISED - | | | 704 | (57-7RS-1&1RS-3 | McLean | 273 | 20 |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | | | | | CONTRACT NO. 70838 | |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - | | | SCALE: | SHEET NO. 4 OF 12 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT |

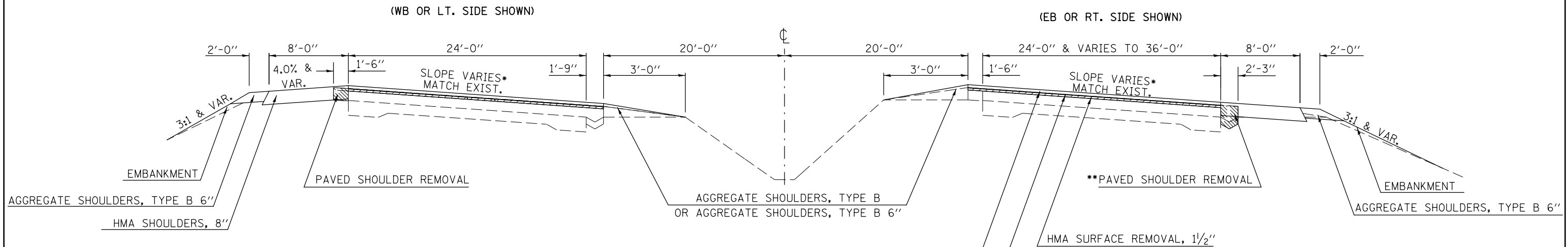
EXISTING TYPICAL CROSS SECTION 5

STATION TO STATION
 ④ 361+10.00 RT. 366+35.20 RT.
 ④ 361+10.00 LT. 366+35.33 LT.



PROPOSED TYPICAL CROSS SECTION 5

STATION TO STATION
 ④ 361+10.00 RT. 366+35.20 RT.
 ④ 361+10.00 LT. 366+35.33 LT.



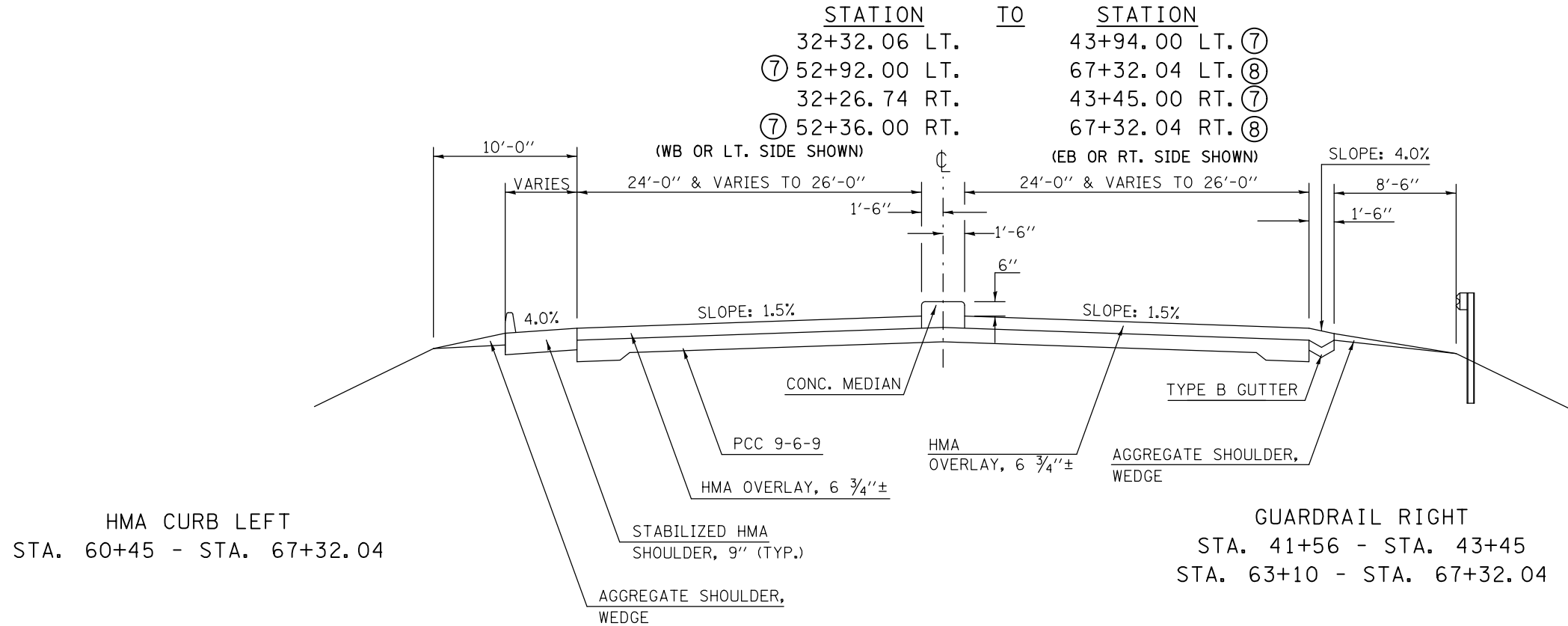
* SUPERELEVATION TRANSITIONS FROM 1.50%± NORMAL CROWN @ STA 359+52.92 TO 2.20% SUPERELEVATION @ STA 361+52.92

** REMOVAL OF CONCRETE GUTTER, TYPE B, SHALL BE INCLUDED IN THE COST OF PAVED SHOULDER REMOVAL WITH NO ADDITIONAL COMPENSATION ALLOWED

PAVED SHOULDER REMOVAL
 STA. 361+10.00 RT. - STA. 366+25.20 RT.
 STA. 361+10.00 LT. - STA. 366+22.33 LT.

| | | | | | | | | | | | | |
|--|--------------------|------------|-----------|---|-------------------------------|--------------------------|--------|---------------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL CROSS SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pw\work\p\dot\dodgms\d0267571\0570838-sht-Typicals.dgn | | DRAWN - | REVISED - | | 704 | (57-7RS-1&1RS-3 | McLean | 273 | 21 | | | |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - | | CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | | | | |
| PLOT DATE = 6/11/2014 | | DATE - | REVISED - | | SCALE: | SHEET NO. 5 OF 12 SHEETS | STA. | TO STA. | | | | |

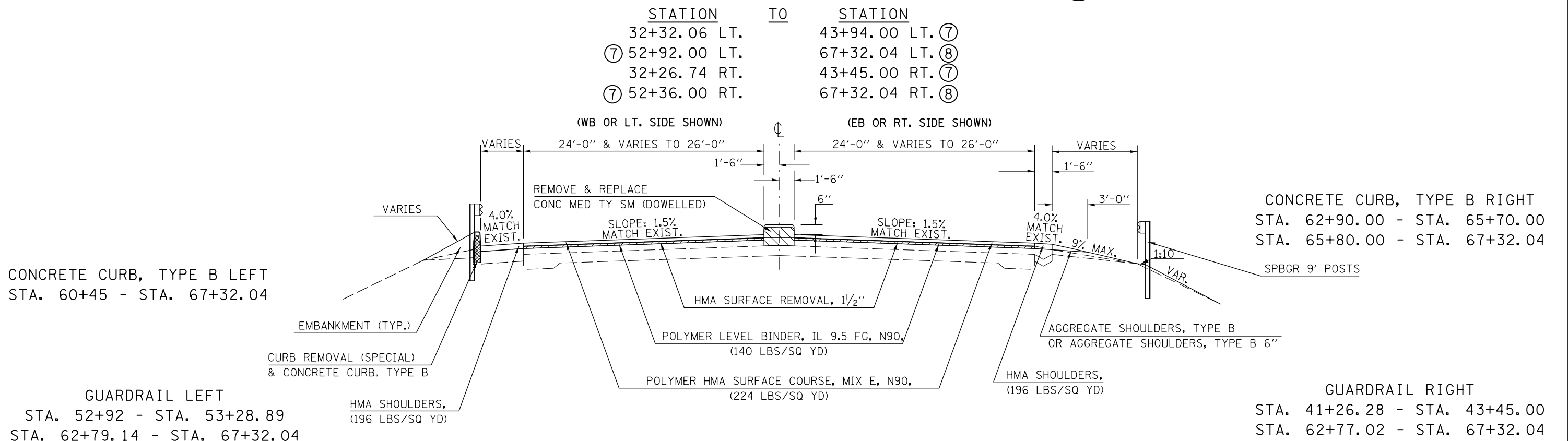
EXISTING TYPICAL CROSS SECTION ⑥



STABILIZED HMA SHOULDER TABLE

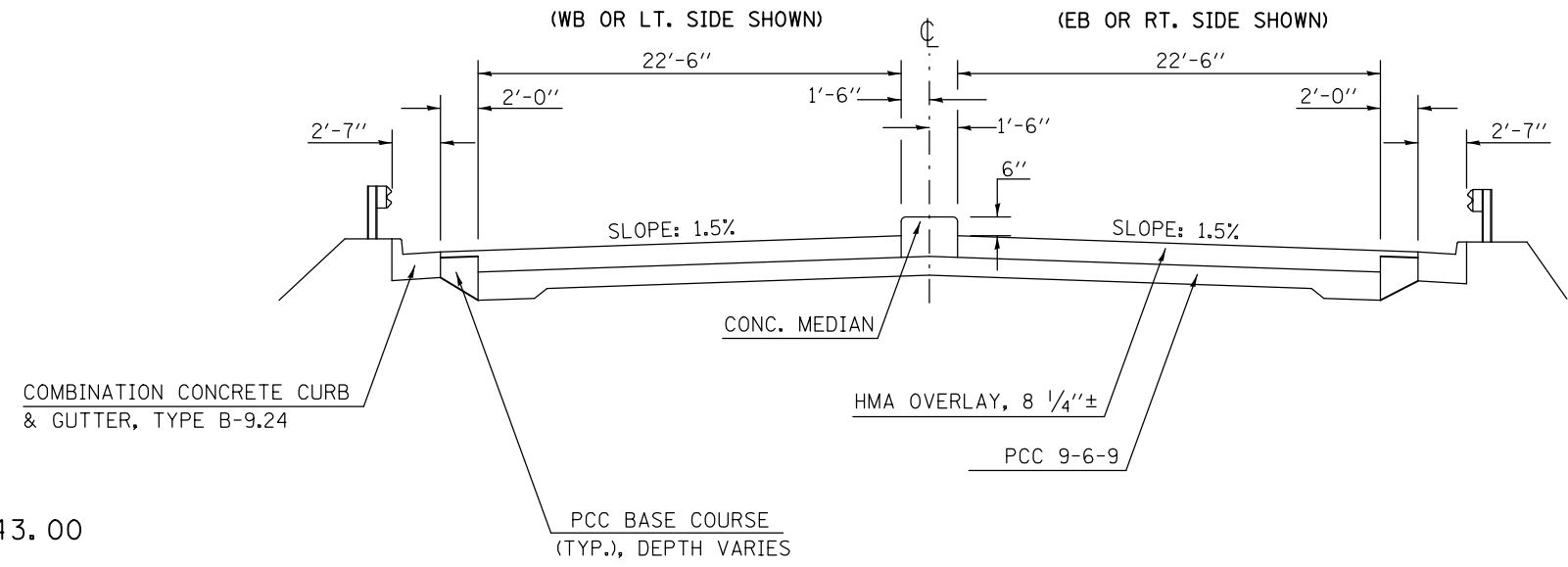
| LOCATION: | STA. | TO | STA. | OFFSET | WIDTH (') |
|-----------|----------|-------|-------------------|--------|------------|
| I-55 BUS. | 29+93 | | 33+93.5 | Rt. | 6 |
| | 40+76 | | 41+38 | Rt. | 4 |
| | 42+00 | | 43+40 | Lt. | 4 |
| | 53+00 | | 55+25 | Rt. | 4 |
| | 66+12 | | 66+96 | Rt. | 6 & Varies |
| NW RAMP | 11+08 | | 11+13 | Lt. | Varies |
| | 11+13 | | 16+20.43 | Lt. | 4 |
| | 16+20.43 | | 17+50.68 | Lt. | Varies |
| | 11+33 | | 11+59 | Rt. | Varies |
| | 11+59 | | 25+24 | Rt. | 6 |
| NE RAMP | 25+24 | | 29+93 I-55 (Bus.) | Rt. | 6 |
| | 10+00 | | 21+17 | Rt. | 6 |
| | 21+17 | | 21+42 | Rt. | Varies |
| | 15+30 | | 16+62.40 | Lt. | Varies |
| | 16+62.40 | | 21+96 | Lt. | 4 |
| SE RAMP | 21+96 | | 22+02.5 | Lt. | Varies |
| | 10+99 | | 11+06 | Lt. | Varies |
| | 11+06 | | 17+84.25 | Lt. | 4 |
| | 17+84.25 | | 19+15.00 | Lt. | Varies |
| | 11+34 | | 11+60 | Rt. | Varies |
| SW RAMP | 11+60 | | 28+64.95 | Rt. | 6 |
| | 10+00 | | 11+76 | Rt. | 6 |
| | 11+76 | | 16+24 | Rt. | 6 |
| | 16+24 | | 22+20 | Rt. | 6 |
| | 22+20 | | 22+47 | Rt. | Varies |
| | 15+30 | | 16+62.40 | Lt. | Varies |
| | 16+62.40 | | 23+13 | Lt. | 4 |
| 23+13 | | 23+18 | Lt. | Varies | |

PROPOSED TYPICAL CROSS SECTION ⑥



EXISTING TYPICAL CROSS SECTION 7

| | | |
|------------------------------|----|------------------------------|
| STATION | TO | STATION |
| ⑥ 43+94.00 LT. | | 47+06.97 LT. BRIDGE OMISSION |
| BRIDGE OMISSION 49+10.79 LT. | | 52+92.00 LT. ⑥ |
| ⑥ 43+45.00 RT. | | 47+06.97 RT. BRIDGE OMISSION |
| BRIDGE OMISSION 49+10.79 RT. | | 52+36.00 RT. ⑥ |

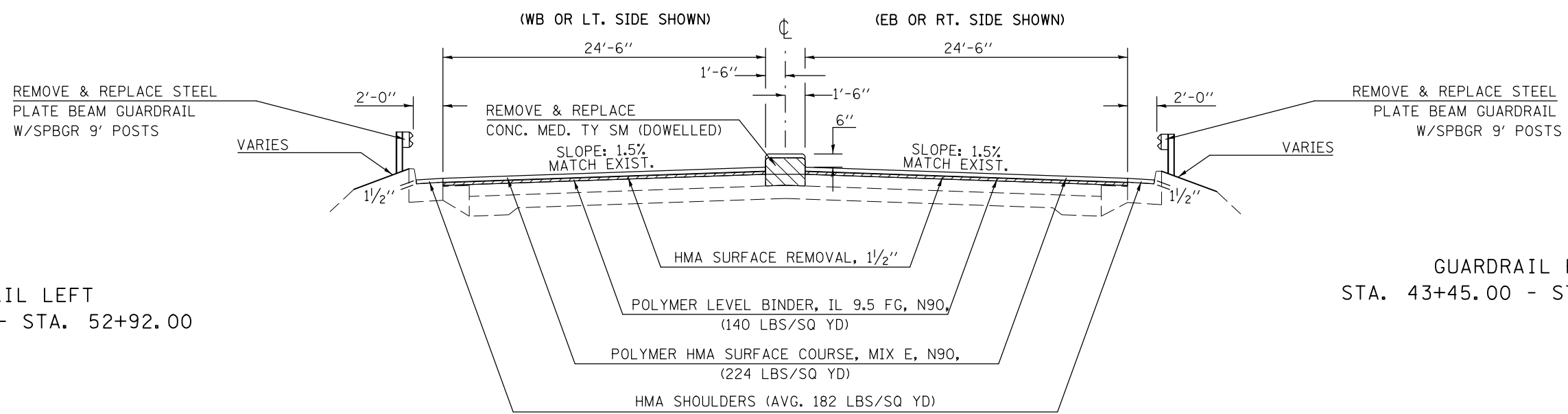


GUARDRAIL LEFT
STA. 49+10.79 - STA. 52+43.00

GUARDRAIL RIGHT
STA. 43+45.00 - STA. 47+06.97

PROPOSED TYPICAL CROSS SECTION 7

| | | |
|------------------------------|----|------------------------------|
| STATION | TO | STATION |
| ⑥ 43+94.00 LT. | | 47+06.97 LT. BRIDGE OMISSION |
| BRIDGE OMISSION 49+10.79 LT. | | 52+92.00 LT. ⑥ |
| ⑥ 43+45.00 RT. | | 47+06.97 RT. BRIDGE OMISSION |
| BRIDGE OMISSION 49+10.79 RT. | | 52+36.00 RT. ⑥ |



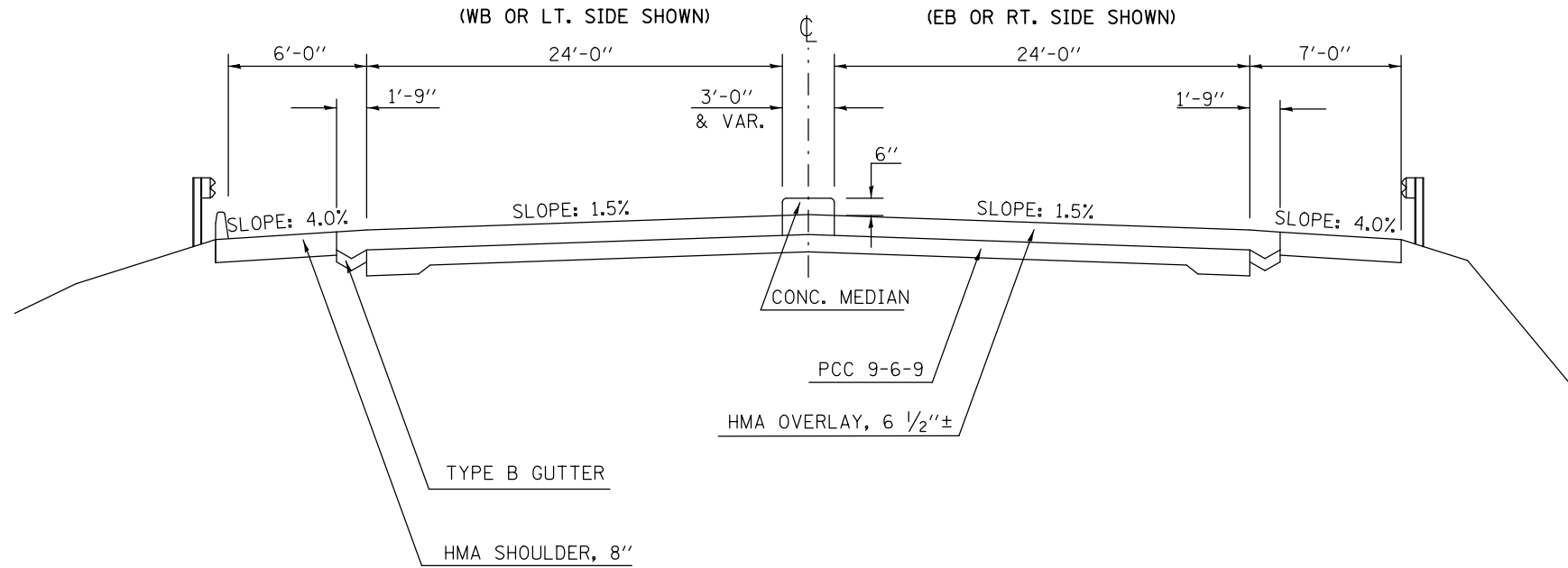
GUARDRAIL LEFT
STA. 49+08.24 - STA. 52+92.00

GUARDRAIL RIGHT
STA. 43+45.00 - STA. 47+09.32

| | | | | | | | | | | | | |
|---|---------------------|------------|-----------|---|-------------------------------|--------------------------|--------|---------------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL CROSS SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht-typicals.dgn | | DRAWN - | REVISED - | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 23 | | | |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - | | CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | | | | |
| PLOT DATE = 6/11/2014 | | DATE - | REVISED - | | SCALE: | SHEET NO. 7 OF 12 SHEETS | STA. | TO STA. | | | | |

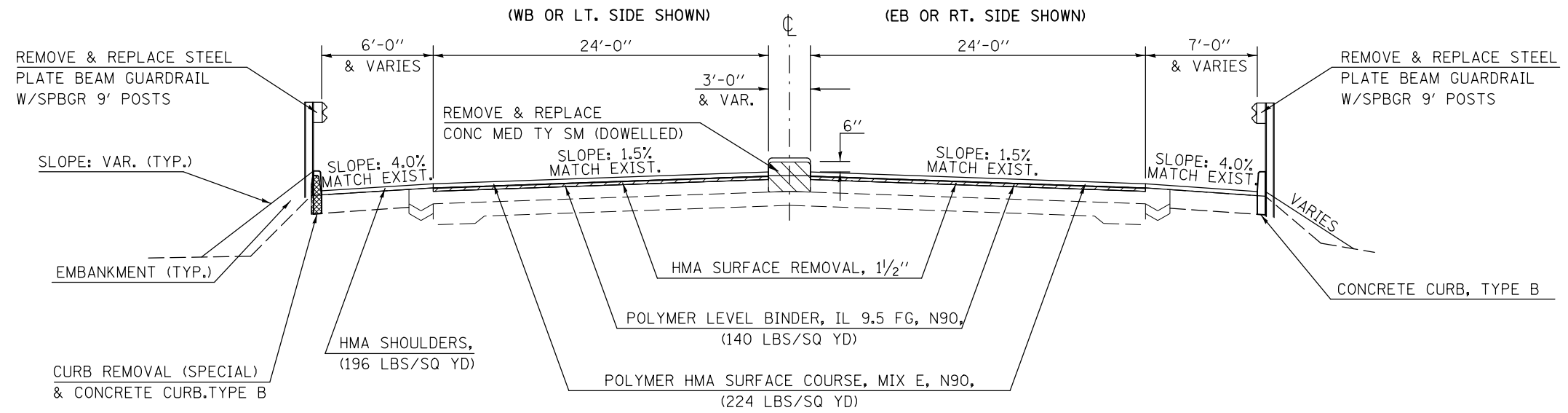
EXISTING TYPICAL CROSS SECTION 8

STATION TO STATION
 ⑥ 67+32.04 68+53.00 ⑨



PROPOSED TYPICAL CROSS SECTION 8

STATION TO STATION
 ⑥ 67+32.04 68+53.00 ⑨

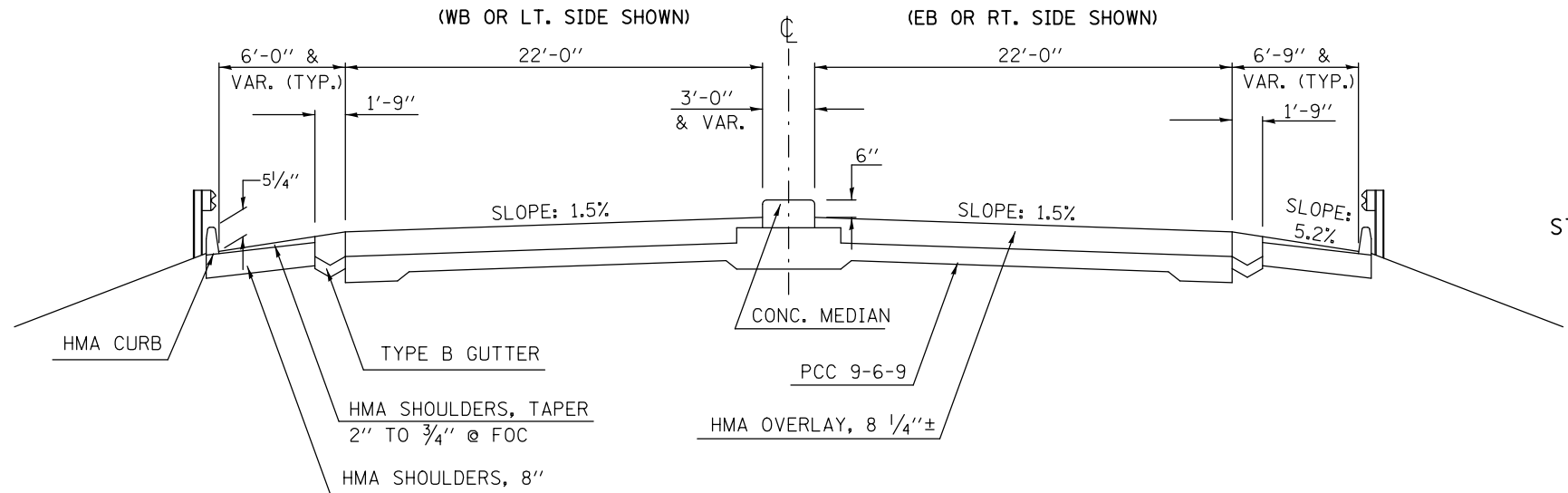


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|--|---------------------|------------|-----------|---|--|-----------------|--------|--------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL CROSS SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sh1-typical.dgn | | DRAWN - | REVISED - | | 704 | (57-7RS-1&1RS-3 | McLean | 273 | 24 | | | |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - | | SCALE: SHEET NO. 8 OF 12 SHEETS STA. TO STA. | | | CONTRACT NO. 70838 | | | | |
| PLOT DATE = 6/11/2014 | | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |

EXISTING TYPICAL CROSS SECTION 9

| STATION | TO | STATION | |
|-----------------|--------------|--------------|-----------------|
| ⑧ 68+53.00 LT. | | 72+70.53 LT. | BRIDGE OMISSION |
| BRIDGE OMISSION | 75+88.67 LT. | 82+04.00 LT. | ⑩ |
| ⑧ 68+53.00 RT. | | 72+89.02 RT. | BRIDGE OMISSION |
| BRIDGE OMISSION | 76+05.90 RT. | 82+04.00 RT. | ⑩ |
| ⑩ 84+54.00 | | 87+41.66 BK. | |
| 87+45.84 AH. | | 87+53.36 | ⑪ |

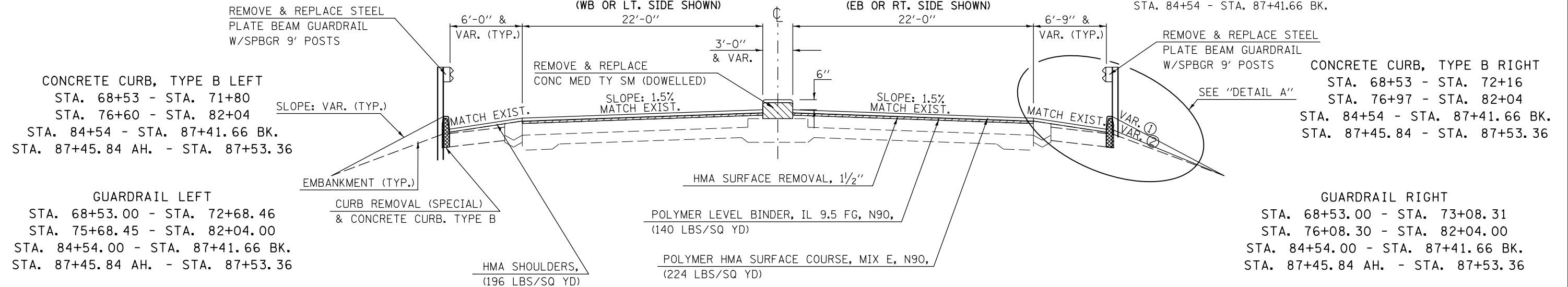
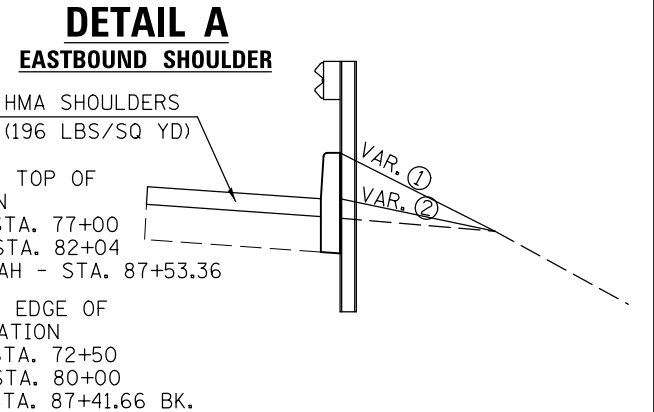
STA. EQUATION: STA. 87+41.66 BK = STA. 87+45.84 AH



PROPOSED TYPICAL CROSS SECTION 9

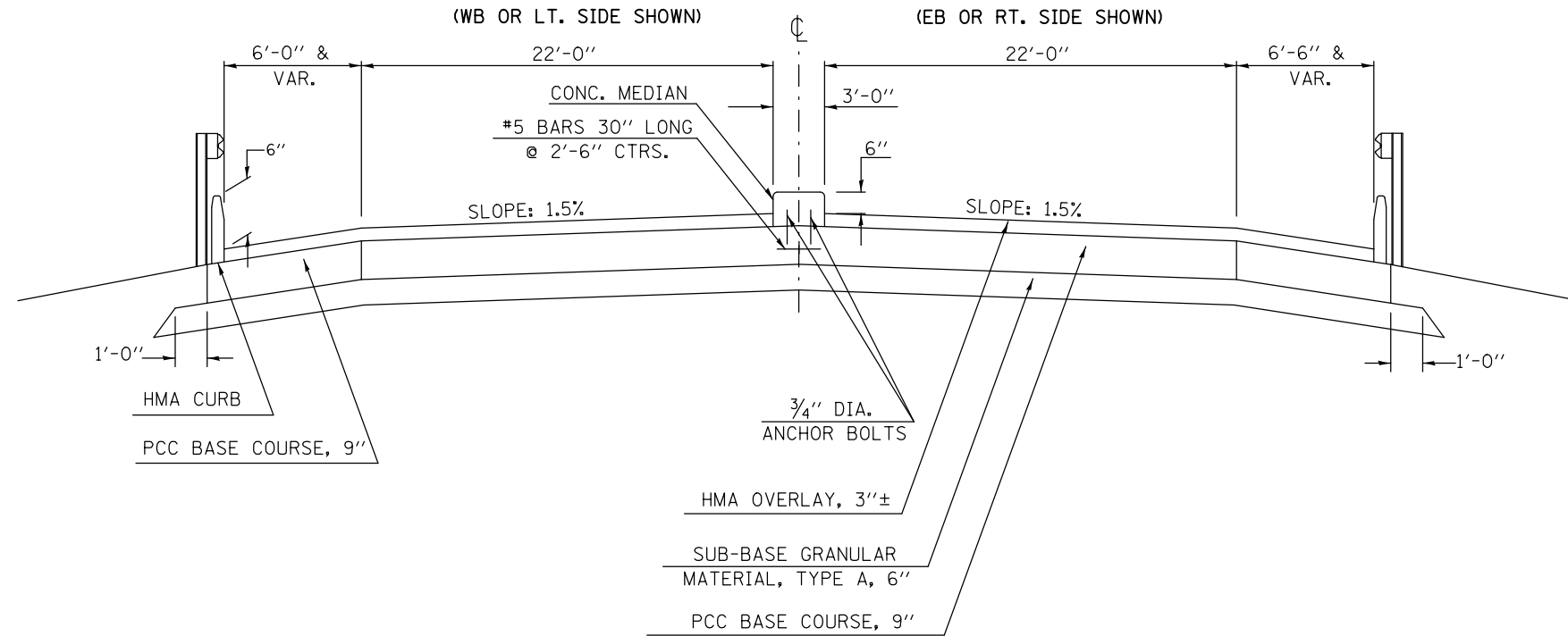
| STATION | TO | STATION | |
|-----------------|--------------|--------------|-----------------|
| ⑧ 68+53.00 LT. | | 72+70.53 LT. | BRIDGE OMISSION |
| BRIDGE OMISSION | 75+88.67 LT. | 82+04.00 LT. | ⑩ |
| ⑧ 68+53.00 RT. | | 72+89.02 RT. | BRIDGE OMISSION |
| BRIDGE OMISSION | 76+05.90 RT. | 82+04.00 RT. | ⑩ |
| ⑩ 84+54.00 | | 87+41.66 BK. | |
| 87+45.84 AH. | | 87+53.36 | ⑪ |

STA. EQUATION: STA. 87+41.66 BK = STA. 87+45.84 AH



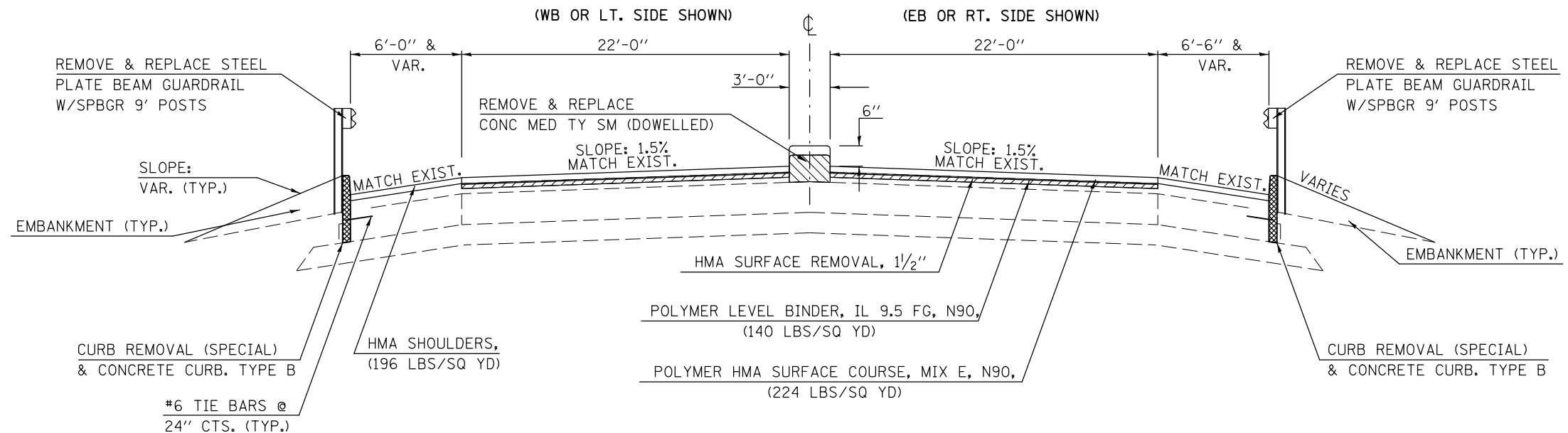
EXISTING TYPICAL CROSS SECTION 10

STATION TO STATION
 ⑨ 82+04.00 84+54.00 ⑨



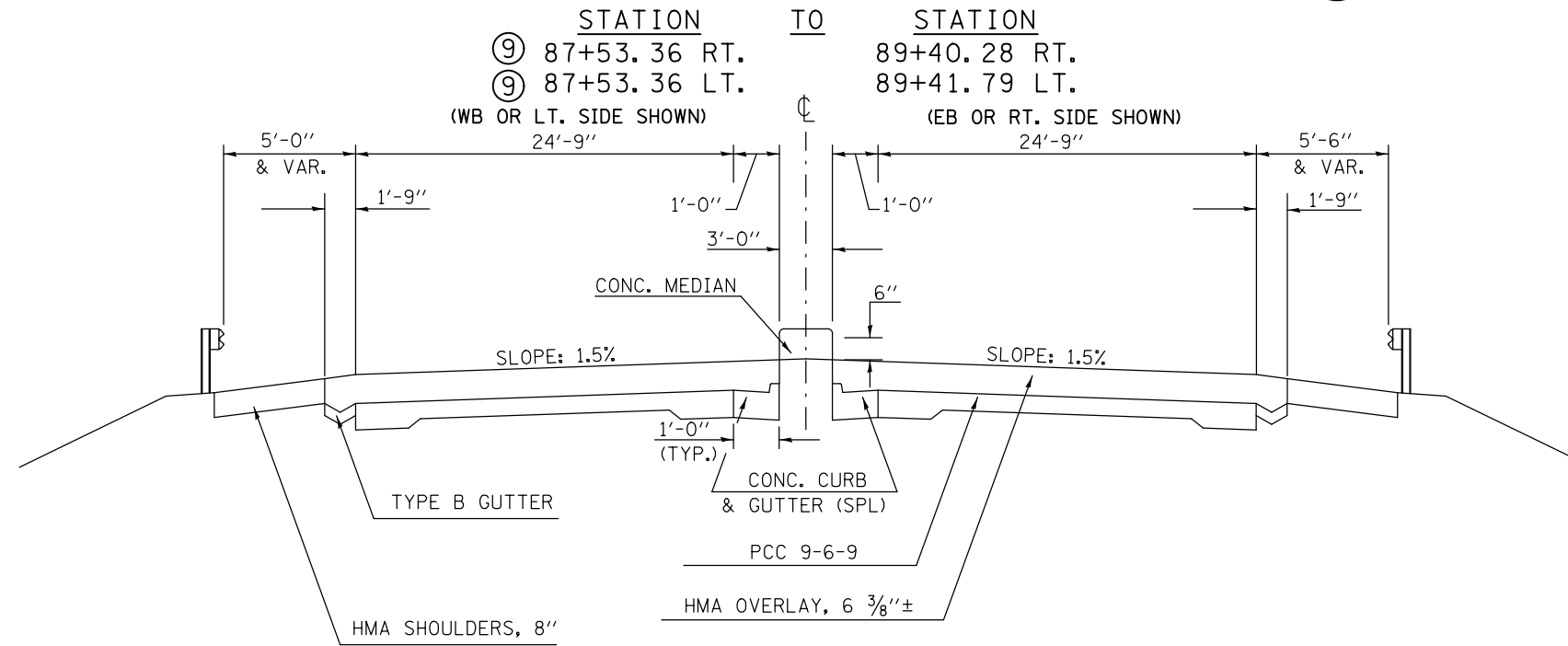
PROPOSED TYPICAL CROSS SECTION 10

STATION TO STATION
 ⑨ 82+04.00 84+54.00 ⑨

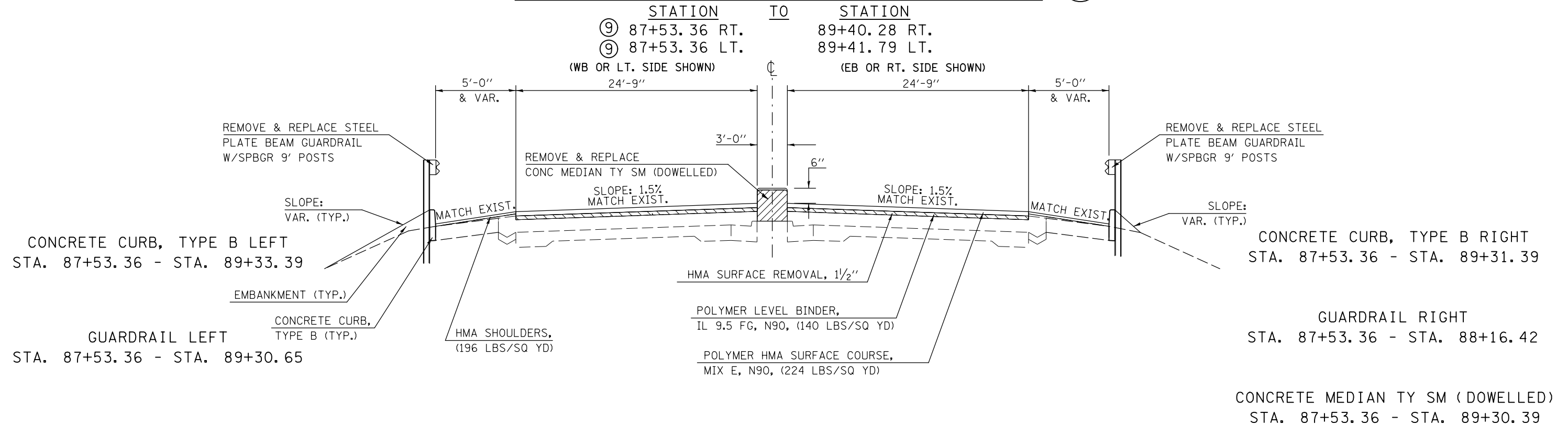


| | | | | | | | | | | |
|---|---------------------|------------|-----------|---|-------------------------------|-------------|---------------------------|--------|---------------------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL CROSS SECTIONS | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pwork\pwork\dot\dodgems\d0267571\0570838-sht-typicals.dgn | | DRAWN - | REVISED - | | | 704 | (57-7RS-1&1RS-3 | McLean | 273 | 26 |
| | | CHECKED - | REVISED - | | | | | | CONTRACT NO. 70838 | |
| | | DATE - | REVISED - | | | | | | ILLINOIS FED. AID PROJECT | |
| | | | | | | SCALE: | SHEET NO. 10 OF 12 SHEETS | STA. | TO STA. | |

EXISTING TYPICAL CROSS SECTION 11



PROPOSED TYPICAL CROSS SECTION 11



| | | | | | | | | | | |
|--|-----------------------------|------------|-----------|---|-------------------------------|---------------------------|------------------|--------|--------------------|-----------|
| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL CROSS SECTIONS | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pw\work\p\dot\dodgms\d0267571\0570838-sht-typicals.dgn | | DRAWN - | REVISED - | | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 27 |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | SCALE: | SHEET NO. 11 OF 12 SHEETS | | STA. | TO STA. | |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - | | | | | | CONTRACT NO. 70838 | |
| ILLINOIS FED. AID PROJECT | | | | | | | | | | |

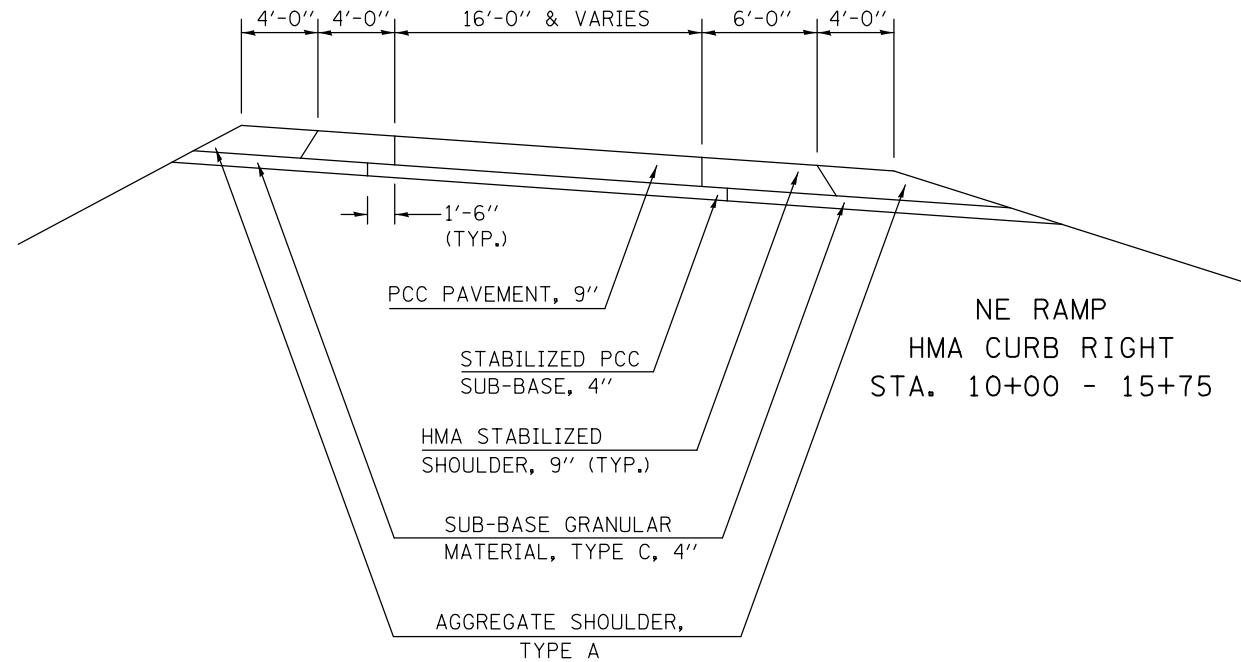
EXISTING TYPICAL CROSS SECTION 12

INTERCHANGE RAMPS AT U.S. 51

S. E. RAMP STA. 10+36.23 TO STA. 28+64.95 N. E. RAMP STA. 10+00.00 TO STA. 22+60.38
 S. W. RAMP STA. 10+00.00 TO STA. 23+68.27 N. W. RAMP STA. 10+37.25 TO STA. 27+00.63

COMBINATION CONCRETE CURB & GUTTER TYPE B-6.24 TABLE

| ROUTE | OFFSET | STA. | TO | STA. | LIN. FEET |
|---------------|--------|-----------|----|-----------|-----------|
| U.S. 51 (BUS) | LT. | 262+06.72 | | | |
| NE RAMP | RT. | | | 21+17 | 213 |
| NE RAMP | LT. | 21+96 | | | |
| U.S. 51 (BUS) | LT. | | | 263+28.76 | 59 |
| U.S. 51 (BUS) | RT. | 263+40.01 | | | |
| NW RAMP | LT. | | | 11+13 | 70 |
| U.S. 51 (BUS) | RT. | 262+25.47 | | | |
| NW RAMP | RT. | | | 11+59 | 177 |
| NW RAMP | RT. | 11+59 | | 25+00 | 1338 |
| SW RAMP | RT. | 12+00 | | 16+00 | 400 |
| SW RAMP | RT. | 22+20 | | | |
| U.S. 51 (BUS) | RT. | | | 268+78.52 | 221 |
| SW RAMP | LT. | 23+13 | | | |
| U.S. 51 (BUS) | RT. | | | 267+53.53 | 54 |
| U.S. 51 (BUS) | LT. | 267+43.64 | | | |
| SE RAMP | LT. | | | 11+06 | 67 |
| U.S. 51 (BUS) | LT. | 268+57.21 | | | |
| SE RAMP | RT. | | | 11+60 | 181 |



STABILIZED HMA SHOULDER TABLE

| LOCATION: | STA. | TO | STA. | OFFSET | WIDTH (') |
|-----------|----------|-------|-------------------|--------|------------|
| I-55 BUS. | 29+93 | | 33+93.5 | Rt. | 6 |
| | 40+76 | | 41+38 | Rt. | 4 |
| | 42+00 | | 43+40 | Lt. | 4 |
| | 53+00 | | 55+25 | Rt. | 4 |
| | 66+12 | | 66+96 | Rt. | 6 & Varies |
| NW RAMP | 11+08 | | 11+13 | Lt. | Varies |
| | 11+13 | | 16+20.43 | Lt. | 4 |
| | 16+20.43 | | 17+50.68 | Lt. | Varies |
| | 11+33 | | 11+59 | Rt. | Varies |
| | 11+59 | | 25+24 | Rt. | 6 |
| NE RAMP | 25+24 | | 29+93 I-55 (Bus.) | Rt. | 6 |
| | 10+00 | | 21+17 | Rt. | 6 |
| | 21+17 | | 21+42 | Rt. | Varies |
| | 15+30 | | 16+62.40 | Lt. | Varies |
| | 16+62.40 | | 21+96 | Lt. | 4 |
| SE RAMP | 21+96 | | 22+02.5 | Lt. | Varies |
| | 10+99 | | 11+06 | Lt. | Varies |
| | 11+06 | | 17+84.25 | Lt. | 4 |
| | 17+84.25 | | 19+15.00 | Lt. | Varies |
| | 11+34 | | 11+60 | Rt. | Varies |
| SW RAMP | 11+60 | | 28+64.95 | Rt. | 6 |
| | 10+00 | | 11+76 | Rt. | 6 |
| | 11+76 | | 16+24 | Rt. | 6 |
| | 16+24 | | 22+20 | Rt. | 6 |
| | 22+20 | | 22+47 | Rt. | Varies |
| | 15+30 | | 16+62.40 | Lt. | Varies |
| | 16+62.40 | | 23+13 | Lt. | 4 |
| 23+13 | | 23+18 | Lt. | Varies | |

PROPOSED TYPICAL CROSS SECTION 12

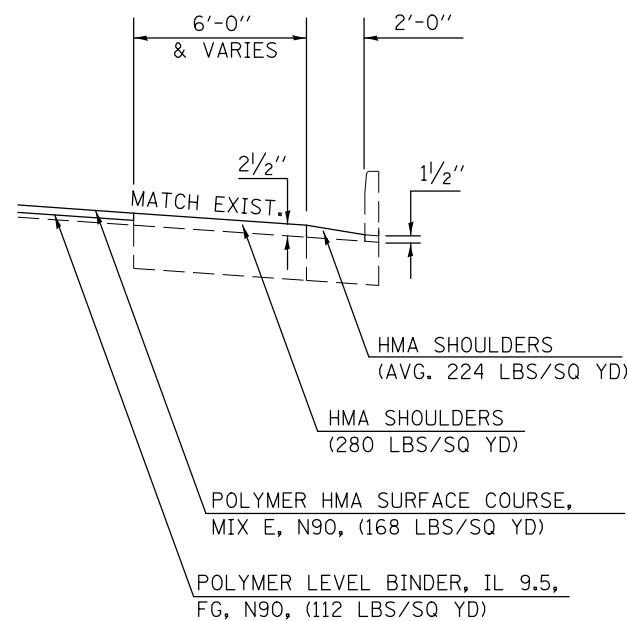
INTERCHANGE RAMPS AT U.S. 51

S. E. RAMP STA. 10+36.23 TO STA. 28+64.95 N. E. RAMP STA. 10+00.00 TO STA. 22+60.38
 S. W. RAMP STA. 10+00.00 TO STA. 23+68.27 N. W. RAMP STA. 10+37.25 TO STA. 27+00.63

DETAIL B

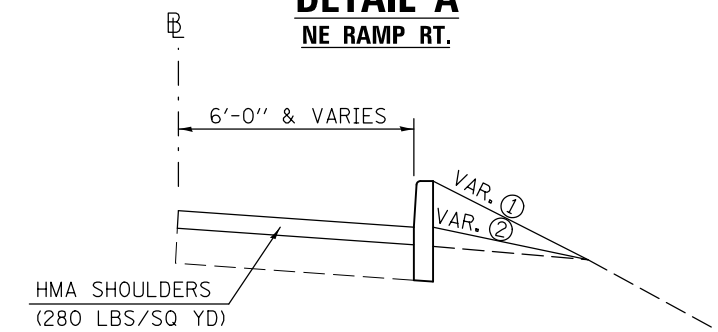
AT INTERCHANGE RAMPS

PAVING INTO COMBINATION CONCRETE CURB & GUTTER TYPE B-6.24

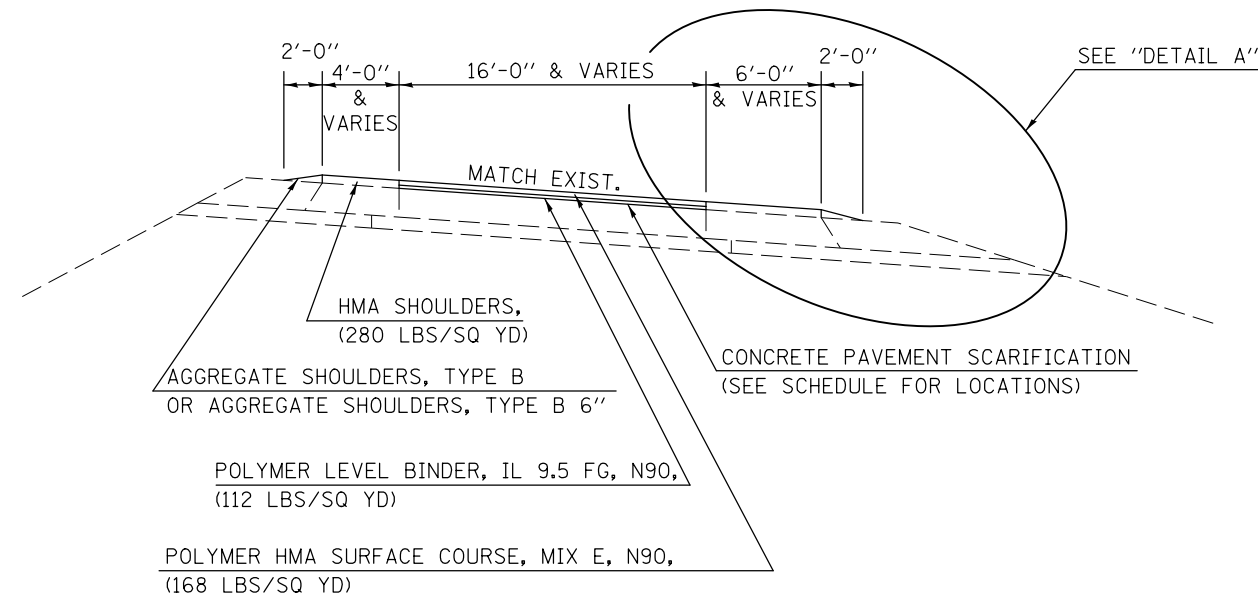


DETAIL A

NE RAMP RT.



- ① EMBANKMENT TO TOP OF CURB ELEVATION
STA. 10+00 RT. - STA. 12+00 RT.
- ② EMBANKMENT TO EDGE OF SHOULDER ELEVATION
STA. 12+00 RT. - STA. 17+19.16 RT.



NE RAMP
 CONCRETE CURB, TYPE B RIGHT
 STA. 10+00 - 17+19.16

SCHEDULE OF QUANTITIES

TEMPORARY RAMPS

| STATION | TO | STATION | LOCATION | LENGTH (FOOT) | WIDTH (FOOT) | TEMPORARY RAMP 40600990 (SQ YD) | LENGTH (FOOT) | WIDTH (FOOT) | TEMPORARY RAMP 40600990 (SQ YD) |
|---------------|----|-----------|---------------|------------------|-----------------|--|------------------|-----------------|--|
| | | | LEG "A": | | | AFTER MILLING | | | AFTER BINDER |
| 33+30 | | 33+19.17 | EB MAIN. | 10.83 | 24.0 | 28.9 | 6.67 | 24.0 | 17.8 |
| 33+30 | | 33+24.17 | EB SHLDRS. | 5.83 | 16.0 | 10.4 | | | |
| 29+70 | | 29+59.17 | WB MAIN. | 10.83 | 24.0 | 28.9 | 6.67 | 24.0 | 17.8 |
| 29+70 | | 29+64.17 | WB SHLDRS. | 5.83 | 16.0 | 10.4 | | | |
| 20+70 | | 18+75 BK. | EB MAIN. | 201.0 | 10.83 | 241.9 | 201.0 | 6.67 | 149.0 |
| 20+70 | | | EB SHLDR. | 5.83 | 6.0 | 3.9 | | | |
| 335+55.87 AH. | | 335+83.87 | EB MAIN. | 28.0 | 10.83 | 33.7 | 28.0 | 6.67 | 20.8 |
| 21+06 | | 18+99 | WB MAIN. | 207.0 | 10.83 | 249.1 | 207.0 | 6.67 | 153.4 |
| 18+97 | | | WB MAIN. | 10.0 | 10.83 | 12.0 | 10.0 | 6.67 | 7.4 |
| 18+95 | | | WB SHLDR. | 4.0 | 5.83 | 2.6 | | | |
| 366+24.37 | | 366+35.20 | EB MAIN. | 10.83 | 24.0 | 28.9 | 6.67 | 24.0 | 17.8 |
| 366+29.37 | | 366+35.20 | EB DR. SHLDR. | 5.83 | 8.0 | 5.2 | | | |
| 366+24.50 | | 366+35.33 | WB MAIN. | 10.83 | 24.0 | 28.9 | 6.67 | 24.0 | 17.8 |
| 366+29.50 | | 366+35.33 | WB DR. SHLDR. | 5.83 | 8.0 | 5.2 | | | |
| | | | LEG "C": | | | | | | |
| 32+26.74 | | 32+37.57 | EB MAIN. | 10.83 | 24.0 | 28.9 | 6.67 | 24.0 | 17.8 |
| 32+26.74 | | 32+32.57 | EB SHLDR. | 5.83 | 6.0 | 3.9 | | | |
| 32+32.06 | | 32+42.89 | WB MAIN. | 10.83 | 24.0 | 28.9 | 6.67 | 24.0 | 17.8 |
| 46+96.14 | | 47+06.97 | EB MAIN. | 10.83 | 24.0 | 28.9 | 6.67 | 24.0 | 17.8 |
| 47+01.14 | | 47+06.97 | EB SHLDR. | 5.83 | 2.0 | 1.3 | | | |
| 46+96.14 | | 47+06.97 | WB MAIN. | 10.83 | 24.0 | 28.9 | 6.67 | 24.0 | 17.8 |
| 47+01.14 | | 47+06.97 | WB SHLDR. | 5.83 | 2.0 | 1.3 | | | |
| 49+10.79 | | 49+21.62 | EB MAIN. | 10.83 | 24.0 | 28.9 | 6.67 | 24.0 | 17.8 |
| 49+10.79 | | 49+16.62 | EB SHLDR. | 5.83 | 2.0 | 1.3 | | | |
| 49+10.79 | | 49+21.62 | WB MAIN. | 10.83 | 24.0 | 28.9 | 6.67 | 24.0 | 17.8 |
| 49+10.79 | | 49+16.62 | WB SHLDR. | 5.83 | 2.0 | 1.3 | | | |
| 72+89.02 | | | EB MAIN. | 10.83 | 27.58 | 33.2 | 6.67 | 27.58 | 20.4 |
| 72+97.50 | | | EB SHLDR. | 5.83 | 4.04 | 2.6 | | | |
| 72+70.53 | | | WB MAIN. | 10.83 | 27.6 | 33.2 | 6.67 | 27.6 | 20.5 |
| 72+62.00 | | | WB SHLDR. | 5.83 | 4.39 | 2.8 | | | |
| 76+05.90 | | | EB MAIN. | 10.83 | 27.58 | 33.2 | 6.67 | 27.58 | 20.4 |
| 76+14.89 | | | EB SHLDR. | 5.83 | 3.9 | 2.5 | | | |
| 75+88.67 | | | WB MAIN. | 10.83 | 27.58 | 33.2 | 6.67 | 27.58 | 20.4 |
| 75+79.37 | | | WB SHLDR. | 5.83 | 4.3 | 2.8 | | | |
| 89+29.45 | | 89+40.28 | EB MAIN. | 10.83 | 26.0 | 31.3 | 6.67 | 26.0 | 19.3 |
| 89+34.45 | | 89+40.28 | EB SHLDR. | 5.83 | 8.0 | 5.2 | | | |
| 89+30.96 | | 89+41.79 | WB MAIN. | 10.83 | 24.5 | 29.5 | 6.67 | 24.5 | 18.2 |
| 89+35.96 | | 89+41.79 | WB SHLDR. | 5.83 | 7.0 | 4.5 | | | |
| | | | NW RAMP: | | | | | | |
| 25+82.4 | | 25+74.07 | MAIN. | 8.33 | 3.0 | 2.8 | 5.0 | 3.0 | 1.7 |
| 25+82.4 | | 25+76.57 | SHLDR. | 5.83 | 6.0 | 3.9 | | | |
| 10+51 | | 10+59.33 | MAIN. | 8.33 | 61.0 | 56.5 | 5.0 | 61.0 | 33.9 |
| | | | SW RAMP: | | | | | | |
| 23+23.04 | | 23+31.37 | MAIN. | 8.33 | 22.0 | 20.4 | 5.0 | 22.0 | 12.2 |
| | | | NE RAMP: | | | | | | |
| 21+91.60 | | 21+99.93 | MAIN. | 8.33 | 43.0 | 39.8 | 5.0 | 43.0 | 23.9 |
| | | | SE RAMP: | | | | | | |
| 10+68.91 | | 10+77.24 | MAIN. | 8.33 | 40.0 | 37.0 | 5.0 | 40.0 | 22.2 |
| | | | | | | SUB-TOTAL = | | | 721.5 |
| | | | | | | GRAND TOTAL = | | | 1967.9 |
| | | | | | | ROUND TO | | | 1970.0 |

PCC SIDEWALK

| LOCATION: | PC CONCRETE SIDEWALK 4 42400100 (SQ FT) | DETECTABLE WARNINGS 42400800 (SQ FT) | SIDEWALK REMOVAL 44000600 (SQ FT) |
|--|--|---|--|
| LEG "A": FOX CR. RD. - HAMILTON RD. INTERSECTION | | | |
| STA. | OFFSET | | |
| 18+82 | 96' LT. | 112.89 | 112.89 |
| 18+85 | 96' LT. | | 30.23 |
| 19+15 | 70' LT. | | 39.42 |
| 19+18 | 67' LT. | 231.02 | 231.02 |
| 19+18 | 5' LT. | 148.90 | 148.90 |
| 19+20 | 8' LT. | | 30.46 |
| 19+20 | 1.5' LT. | | 31.51 |
| 19+23 | 58' LT. | | 28.41 |
| 19+45 | 100' RT. | 178.59 | 178.59 |
| | NW RAMP: | | |
| 10+70 | 35' LT. | 25.50 | 25.50 |
| | SW RAMP: | | |
| 23+46 | 22' LT. | 26.80 | 11.39 |
| 23+56 | 32' RT. | 60.70 | 11.56 |
| | TOTAL = | 784.4 | 784.4 |
| | ROUND TO: | 785.0 | 785.0 |

(ALL AREAS FROM CADD)

EARTHWORK

| LOCATION | EARTH EXCAVATION 20200100 (CU YD) | EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (CU YD) | EMBANKMENT (CU YD) | EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD) | FURNISHED EXCAVATION 20400800 (CU YD) |
|--------------|--|---|-----------------------|---|--|
| LEG A EBDL | 509.2 | 381.9 | 48.2 | 333.7 | 0.0 |
| LEG A EBPL | 102.9 | 77.1 | 55.6 | 21.6 | 0.0 |
| LEG A WBDL | 487.0 | 365.3 | 7.6 | 357.7 | 0.0 |
| LEG A WBPL | 77.4 | 58.1 | 31.5 | 26.6 | 0.0 |
| LEG C EBDL | 51.1 | 38.3 | 135.3 | -97.0 | 0.0 |
| LEG C EBPL | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| LEG C WBDL | 218.6 | 163.9 | 483.2 | -319.3 | 0.0 |
| LEG C WBPL | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| NW RAMP - LT | 11.3 | 8.5 | 0.0 | 8.5 | 0.0 |
| NW RAMP - RT | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| NE RAMP - LT | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| NE RAMP - RT | 2.2 | 1.7 | 7.3 | -5.7 | 0.0 |
| SW RAMP - LT | 15.1 | 11.3 | 32.6 | -21.3 | 0.0 |
| SW RAMP - RT | 10.8 | 8.1 | 31.5 | -23.4 | 0.0 |
| SE RAMP - LT | 19.8 | 14.9 | 0.9 | 13.9 | 0.0 |
| SE RAMP - RT | 27.9 | 20.9 | 169.8 | -148.8 | 0.0 |
| TOTAL = | 1533.3 | 1150.0 | 1003.5 | 146.4 | 0.0 |
| ROUND TO: | 1540.0 | | | | 0.0 |

NOTES:

1. THE SHRINKAGE FACTOR USED IS 25%.
2. SHRINKAGE, EMBANKMENT, AND BALANCE IS FOR INFORMATION ONLY.
3. NO PAYMENT WILL BE ALLOWED FOR OVERHAUL.
4. EXCAVATION REQUIRED FOR PCC BASE COURSE, FULL DEPTH BITUMINOUS PAVEMENT, BITUMINOUS SHOULDERS, BITUMINOUS BASE COURSE AND AGGREGATE SHOULDER IS MEASURED AND PAID FOR AS EARTH EXCAVATION.

| | | | | | | | | | | |
|---|--------------------|------------|-----------|---|-------------------------------|--------------------|----------------------|--------|-----------------|---------------------------|
| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SCHEDULE OF QUANTITIES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ca:\pw\work\p1dot\dodgms\40267571\0570838-sht-schedules.dgn | | DRAWN - | REVISED - | | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 29 |
| PLLOT SCALE = 40.0000 ' / in. | | CHECKED - | REVISED - | | | CONTRACT NO. 70838 | | | | |
| MODELNAME# | | DATE - | REVISED - | | | SCALE: | SHEET 1 OF 15 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT |

SCHEDULE OF QUANTITIES

EROSION CONTROL (SHEET 1 OF 3)

| DIRECTION | STATION | TO | STATION | OFFSET | TEMPORARY DITCH CHECKS 28000305 (FOOT) | PERIMETER EROSION BARRIER 28000400 (FOOT) | INLET & PIPE PROTECTION 28000500 (EACH) | STONE RIPRAP CLASS A 4 28100107 (SQ YD) | FILTER FABRIC 28200200 (SQ YD) | STONE RIPRAP CLASS A4 (TONS) | BEDDING STONE (TONS) |
|-------------|---------------|----|-------------|--------------|---|--|--|--|---|------------------------------------|----------------------------|
| LEG "A": | | | | | | | | | | | |
| EB | 33+30 | | | 1.75' Rt. | 17.0 | | | | | | |
| EB | 31+00 | | | CL | 10.0 | | | | | | |
| EB | 30+50 | | | 0.5' LT | 15.0 | | | | | | |
| EB | 29+50 | | | 0.5' LT | 15.0 | | | | | | |
| EB | 29+00 | | | CL | 15.0 | | | | | | |
| WB | 32+50 | | | 1.0' RT | 12.0 | | | | | | |
| WB | 31+50 | | | 1.0' RT | 10.0 | | | | | | |
| WB | 28+50 | | | 1.0' RT | 15.0 | | | | | | |
| WB | 28+00 | | | 3.0' RT. | 15.0 | | | | | | |
| EB | 28+00 | | 26+00 | 65.0' LT | | 194.3 | | | | | |
| WB | 27+50 | | | 2.5' RT | 12.0 | | | | | | |
| WB | 27+00 | | | 3.0' RT | 12.0 | | | | | | |
| WB | 26+00 | | | 4.5' RT | 10.0 | | | | | | |
| EB | 26+00 | | 25+00 | 65'-71' LT | | 97.2 | | | | | |
| EB | 25+00 | | 23+19 | 71'-68' LT | | 175.5 | | | | | |
| WB | 25+00 | | | 6.0' RT | 6.0 | | | | | | |
| EB | 23+19 | | 23+00 | 68'-75' LT | | 19.7 | | | | | |
| WB | 23+19 | | | 5.0' RT. | | | 1.0 | | | | |
| EB | 23+00 | | 20+73 | 75'-68' LT | | 220.6 | | | | | |
| EB | 19+06.39 | | | 3.44' LT | | | 1.0 | | | | |
| WB | 23+00 | | 21+50 | 62.0' RT | | 154.0 | | | | | |
| WB | 21+50 | | 21+05.5 | 62'-58' RT | | 45.9 | | | | | |
| WB | 18+96 | | 18+75.00 BK | 68'-70' RT | | 21.1 | | | | | |
| WB | 335+55.87 AH. | | 337+00 | 70.0' LT | | 144.1 | | | | | |
| EB | 335+83 | | 337+00 | 58.5'-60' RT | | 116.5 | | | | | |
| EB | 337+00 | | 356+00 | 60.0' RT | | 1900.0 | | | | | |
| WB | 337+00 | | 338+50 | 70'-72' LT | | 150.0 | | | | | |
| EB | 337+75 | | | 4.0' RT. | | | 1.0 | | | | |
| WB | 338+50 | | 339+50 | 72.0' LT | | 100.0 | | | | | |
| EB | 339+00 | | | 3.0' RT. | | | 1.0 | | | | |
| WB | 339+50 | | 341+50 | 72'-62' LT | | 200.2 | | | | | |
| EB | 339+50 | | | 5.0' RT. | | | 1.0 | | | | |
| WB | 341+50 | | 343+00 | 62'-60' LT | | 150.0 | | | | | |
| EB | 342+50 | | | CL | 15.0 | | | | | | |
| WB | 343+00 | | 366+00 | 60.0' LT | | 2310.2 | | | | | |
| EB | 345+50 | | | 0.5' RT | 15.0 | | | | | | |
| EB | 347+00 | | | 0.5' RT | 15.0 | | | | | | |
| EB | 348+00 | | | CL | 15.0 | | | | | | |
| EB | 349+00 | | | CL | 15.0 | | | | | | |
| EB | 349+75 | | | CL | 15.0 | | | | | | |
| WB | 350+00 | | | 6.0' LT | | | 1.0 | | | | |
| EB | 351+00 | | | 1.0' RT | 15.0 | | | | | | |
| EB | 351+50 | | | 0.75' RT | 15.0 | | | | | | |
| WB | 352+50 | | | 4.0' LT | 10.0 | | | | | | |
| WB | 353+10 | | | 4.0' LT | 10.0 | | | | | | |
| WB | 353+62 | | | 1.0' LT | | | 1.0 | | | | |
| WB | 353+68 | | | 1.0' LT | | | 1.0 | | | | |
| WB | 354+50 | | | 0.5' LT | 15.0 | | | | | | |
| WB | 355+50 | | | 1.0' LT | 15.0 | | | | | | |
| WB | 356+50 | | | 3.5' LT | 10.0 | | | | | | |
| WB | 357+00 | | | 8.0' LT. | | | 1.0 | | | | |
| EB | 357+00 | | | 64' RT | 12.0 | | | | | | |
| WB | 357+50 | | | 4.0' LT | 10.0 | | | | | | |
| EB | 358+00 | | | 63.0' RT | 6.0 | | | | | | |
| WB | 358+50 | | | 4.0' LT | 10.0 | | | | | | |
| EB | 359+00 | | | 59.0' RT | 8.0 | | | | | | |
| WB | 359+50 | | | 0.5' LT | 15.0 | | | | | | |
| EB | 360+00 | | | 58.0' RT | 8.0 | | | | | | |
| EB | 360+50 | | | CL | 15.0 | | | | | | |
| EB | 361+00 | | | 59.0' RT | 6.0 | | | | | | |
| EB | 361+50 | | | CL | 15.0 | | | | | | |
| EB | 362+00 | | | 61.0' RT | 4.0 | | | | | | |
| EB | 362+50 | | | 1.0' RT | 15.0 | | | | | | |
| EB | 362+75 | | | 63.0' RT | 4.0 | | | | | | |
| EB | 363+25 | | | 64.0' RT | 6.0 | | | | | | |
| EB | 363+75 | | | 65.0' RT | 6.0 | | | | | | |
| EB | 364+25 | | | 67.0' RT | 6.0 | | | | | | |
| EB | 364+75 | | | 70.0' RT | 8.0 | | | | | | |
| EB | 365+25 | | | 72.0' RT | 8.0 | | | | | | |
| EB | 365+58 | | | 72.0' RT | 8.0 | | | | | | |
| WB | 366+00 | | 366+22 | 60'-53.5' LT | | 23.5 | | | | | |
| WB | 366+12.5 | | | CL | | | 1.0 | | | | |
| SUB-TOTAL = | | | | | 514.0 | 6022.8 | 10.0 | 0 | 0 | 0 | 0 |

| | | | | | | | | | | | |
|---|--------------------|------------|-----------|---|-------------------------------|--------------------|----------------------|--------|--------------|---------------------------|--|
| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SCHEDULE OF QUANTITIES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ci:\pw\work\p\dot\dodgms\d0267571\0570838-sht-schedules.dgn | | DRAWN - | REVISED - | | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 30 | |
| \$MODELNAME\$ | | CHECKED - | REVISED - | | | CONTRACT NO. 70838 | | | | | |
| | | DATE - | REVISED - | | | SCALE: | SHEET 2 OF 15 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT | |

SCHEDULE OF QUANTITIES

EROSION CONTROL (SHEET 2 OF 3)

| DIRECTION | STATION | TO | STATION | OFFSET | TEMPORARY DITCH CHECKS 28000305 (FOOT) | PERIMETER EROSION BARRIER 28000400 (FOOT) | INLET & PIPE PROTECTION 28000500 (EACH) | STONE RIPRAP CLASS A 4 28100107 (SQ YD) | FILTER FABRIC 28200200 (SQ YD) | STONE RIPRAP CLASS A4 (TONS) | BEDDING STONE (TONS) |
|------------|-------------|----|-------------|----------------|---|--|--|--|---|------------------------------------|----------------------------|
| LEG "C": | | | | | | | | | | | |
| WB | 32+25 | | 32+50 | 39'-46' LT | | | | 26.0 | | | |
| WB | 32+50 | | 32+88 | 46'-39' LT | | | | 38.6 | | | |
| WB | 41+67.5 | | 42+00 | 33'-38' LT | | | | 32.9 | | | |
| WB | 42+00 | | 43+00 | 38.0' LT | | | | 100.0 | | | |
| EB | 40+58 | | 41+13 | 37.5'-37' RT | | | | 55.0 | | | |
| EB | 41+13 | | 41+61 | 37'-41' RT | | | | 48.2 | | | |
| EB | 41+61 | | 42+00 | 41'-37' RT | | | | 39.2 | | | |
| EB | 42+00 | | 44+00 | 37.0' RT | | | | 200.0 | | | |
| EB | 44+35 | | | 65.0' RT | | | 1.0 | | | | |
| EB | 52+36 | | 52+50 | 31'-40' RT | | | | 16.6 | | | |
| EB | 52+50 | | 54+50 | 40.0' RT | | | | 200.0 | | | |
| EB | 54+50 | | 55+23 | 40'-30' RT | | | | 73.7 | | | |
| WB | 51+00 | | 51+50 | 40.0' LT | | | | 50.0 | | | |
| WB | 51+50 | | 52+50 | 40'-32' LT | | | | 100.3 | | | |
| WB | 52+50 | | 53+21 | 32'-38' LT | | | | 71.3 | | | |
| WB | 60+43 | | 61+00 | 42'-40' LT | | | | 57.0 | | | |
| WB | 61+00 | | 63+00 | 40.0' LT | | | | 200.0 | | | |
| WB | 64+50 | | | 55.0' LT | 10.0 | | | | | | |
| WB | 65+00 | | | 55.5' LT | 10.0 | | | | | | |
| WB | 65+75 | | | 59.0' LT | 10.0 | | | | | | |
| WB | 67+00 | | | 66.25' LT | 10.0 | | | | | | |
| WB | 67+50 | | | 67.75' LT | 10.0 | | | | | | |
| WB | 68+25 | | | 70.0' LT | 10.0 | | | | | | |
| WB | 69+50 | | | 70.5' LT | 10.0 | | | | | | |
| WB | 70+00 | | | 72.0' LT | 10.0 | | | | | | |
| WB | 70+50 | | | 73.25' LT | 10.0 | | | | | | |
| WB | 72+00 | | | 78.5' LT | 10.0 | | | | | | |
| WB | 72+25 | | | 77.0' LT | 10.0 | | | | | | |
| WB | 72+50 | | | 75.5' LT | 10.0 | | | | | | |
| EB | 62+50 | | | 78.0' RT | 15.0 | | | | | | |
| EB | 62+86 | | | 50'-80' RT | | | | 20.7 | 20.7 | 13.9 | 6.2 |
| EB | 63+00 | | | 78.0' RT | 14.0 | | | | | | |
| EB | 63+50 | | | 81.0' RT | 10.0 | | | | | | |
| WB | 63+94 | | | 45.0' LT | | | 1.0 | | | | |
| EB | 63+94.35 | | | 65.5'-86.5' RT | | | | 25.7 | 25.7 | 17.2 | 7.7 |
| EB | 64+50 | | | 81.0' RT | 10.0 | | | | | | |
| EB | 65+00 | | | 81.0' RT | 10.0 | | | | | | |
| EB | 65+75 | | | 52'-83' RT | | | | 20.7 | 20.7 | 13.9 | 6.2 |
| EB | 67+00 | | | 80.0' RT | 10.0 | | | | | | |
| EB | 67+50 | | | 80.5' RT | 10.0 | | | | | | |
| EB | 67+75 | | | 79.0' RT | 10.0 | | | | | | |
| EB | 68+00 | | | 78.0' RT | 10.0 | | | | | | |
| EB | 68+50 | | | 75.0' RT | 10.0 | | | | | | |
| EB | 69+50 | | | 74.0' RT | 10.0 | | | | | | |
| EB | 70+50 | | | 78.0' RT | 10.0 | | | | | | |
| EB | 71+00 | | 73+00 | 90.0' RT | | | | 200.0 | | | |
| EB | 76+50 | | 79+00 | 85.0' RT | | | | 250.0 | | | |
| EB | 79+00 | | 79+23 | 40'-43' RT | | | | 23.2 | | | |
| EB | 79+23 | | 79+54 | 43.0' RT | | | | 31.0 | | | |
| EB | 79+54 | | 80+00 | 43'-40' RT | | | | 46.1 | | | |
| EB | 80+00 | | 82+00 | 80.0' RT | | | | 200.0 | | | |
| EB | 82+50 | | | 75.0' RT | 10.0 | | | | | | |
| EB | 83+00 | | | 78.0' RT | 10.0 | | | | | | |
| EB | 83+52 | | | 78.5' RT | | | 1.0 | | | | |
| EB | 84+50 | | | 70.5' RT | 10.0 | | | | | | |
| EB | 85+50 | | | 67.0' RT | 10.0 | | | | | | |
| EB | 86+54 | | | 60.0' RT | | | 1.0 | | | | |
| EB | 87+00 | | | 65.5' RT | 10.0 | | | | | | |
| WB | 75+70 | | 76+00 | 30'-40' LT | | | | 31.6 | | | |
| WB | 76+00 | | 79+00 | 82.0' LT | | | | 300.0 | | | |
| WB | 79+00 | | 79+23 | 40'-42' LT | | | | 23.3 | | | |
| WB | 79+23 | | 79+54 | 42.0' LT | | | | 31.0 | | | |
| WB | 79+54 | | 80+00 | 42'-40' LT | | | | 46.0 | | | |
| WB | 80+00 | | 82+50 | 80.0' LT | | | | 250.0 | | | |
| WB | 82+50 | | 83+00 | 80'-90' LT | | | | 51.0 | | | |
| WB | 83+00 | | 83+50 | 90.0' LT | | | | 50.0 | | | |
| WB | 83+50 | | 87+41.66 BK | 40.0' LT | | | | 391.7 | | | |
| WB | 87+45.84 AH | | 88+50.39 | 40.0' LT | | | | 104.6 | | | |
| EB | 88+50 | | | 63.0' RT | 10.0 | | | | | | |
| WB | 88+50 | | | 67.0' LT | 10.0 | | | | | | |
| EB | 89+31.39 | | | 47'-66' RT | | | | 13.3 | 13.3 | 8.9 | 4.0 |
| WB | 89+33.39 | | | 46'-68' LT | | | | 14.7 | 14.7 | 9.8 | 4.4 |
| SUB-TOTAL= | | | | | 319.0 | 3338.3 | 4.0 | 95.1 | 95.1 | 63.7 | 28.5 |

| | | | | | | | | | | | |
|--|-----------------------|------------|-----------|---|-------------------------------|--------------------|------------------|-----------------------------------|-----------------|--------------|--|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SCHEDULE OF QUANTITIES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ci:\pw\work\p1dot\dodgems\d0267571\0570838-sht-schedules.dgn | | DRAWN - | REVISED - | | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 31 | |
| \$MODELNAME\$ | | CHECKED - | REVISED - | | | CONTRACT NO. 70838 | | | | | |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - | | | SCALE: | | SHEET 3 OF 15 SHEETS STA. TO STA. | | | |
| ILLINOIS FED. AID PROJECT | | | | | | | | | | | |

SCHEDULE OF QUANTITIES

EROSION CONTROL (SHEET 3 OF 3)

| DIRECTION | STATION | TO | STATION | OFFSET | TEMPORARY DITCH CHECKS 28000305 (FOOT) | PERIMETER EROSION BARRIER 28000400 (FOOT) | INLET & PIPE PROTECTION 28000500 (EACH) | STONE RIPRAP CLASS A 4 28100107 (SQ YD) | FILTER FABRIC 28200200 (SQ YD) | STONE RIPRAP CLASS A4 (TONS) | BEDDING STONE (TONS) | |
|-----------|----------|----|----------|----------------|---|--|--|--|---|------------------------------------|----------------------------|-------|
| SW RAMP: | | | | | | | | | | | | |
| RT. | 16+25 | | 18+00 | 10'-15' RT | | 175.1 | | | | | | |
| RT. | 18+00 | | 18+09 | 15'-17' RT | | 9.2 | | | | | | |
| RT. | 18+09 | | 18+30 | 17'-20' RT | | 21.2 | | | | | | |
| RT. | 18+30 | | 20+00 | 20'-19' RT | | 170.0 | | | | | | |
| RT. | 20+00 | | 20+39.37 | 19'-28' RT | | 40.4 | | | | | | |
| RT. | 20+39.37 | | 21+00 | 28'-16' RT | | 61.8 | | | | | | |
| RT. | 21+00 | | 22+26 | 16'-12' RT | | 126.1 | | | | | | |
| RT. | 22+26 | | 22+32 | 12'-7' RT | | 7.8 | | | | | | |
| LT. | 16+66 | | 17+59 | 24'-30' LT | | 93.2 | | | | | | |
| LT. | 17+59 | | 18+00 | 30.0' LT | | 41.0 | | | | | | |
| LT. | 18+00 | | 19+00 | 30'-47' LT | | 101.4 | | | | | | |
| LT. | 19+00 | | 19+50 | 47'-33' LT | | 50.4 | | | | | | |
| LT. | 19+50 | | 20+00 | 33'-40' LT | | 50.5 | | | | | | |
| LT. | 20+00 | | 23+20 | 40'-25' LT | | 320.4 | | | | | | |
| LT. | 23+20 | | 23+23 | 25'-18' LT | | 7.6 | | | | | | |
| LT. | 20+40 | | | 48'-90' LT | | | | 71.0 | 71.0 | 47.6 | 21.3 | |
| SE RAMP: | | | | | | | | | | | | |
| RT. | 11+35 | | 11+35 | 9'-15' RT | | 6.0 | | | | | | |
| RT. | 11+35 | | 18+00 | 15.0' RT | | 665.0 | | | | | | |
| RT. | 18+00 | | 18+50 | 15'-10' RT | | 50.2 | | | | | | |
| LT. | 10+90 | | 10+90 | 19'-30' LT | | 11.0 | | | | | | |
| LT. | 10+90 | | 15+00 | 30.0' LT | | 410.0 | | | | | | |
| LT. | 15+00 | | 16+50 | 30'-27' LT | | 150.0 | | | | | | |
| LT. | 16+50 | | 17+00 | 27'-25' LT | | 50.0 | | | | | | |
| LT. | 17+00 | | 17+50 | 25.0' LT | | 50.0 | | | | | | |
| NE RAMP: | | | | | | | | | | | | |
| RT. | 10+00 | | 10+45 | 15.0' RT | | 45.0 | | | | | | |
| RT. | 11+75 | | | 12'-20' RT | | | | 5.3 | 5.3 | 3.6 | 1.6 | |
| RT. | 10+45 | | 11+00 | 15'-18' RT | | 55.1 | | | | | | |
| RT. | 13+75 | | | 12.5'-21.5' RT | | | | 6.0 | 6.0 | 4.0 | 1.8 | |
| RT. | 11+00 | | 11+70 | 18'-14' RT | | 70.1 | | | | | | |
| RT. | 15+65 | | | 12'-23' RT | | | | 7.3 | 7.3 | 4.9 | 2.2 | |
| RT. | 11+78 | | 13+70 | 13.0' RT | | 192.0 | | | | | | |
| RT. | 13+78 | | 15+60 | 13.0' RT | | 182.0 | | | | | | |
| RT. | 15+68 | | 17+00 | 13.0' RT | | 132.0 | | | | | | |
| RT. | 17+00 | | 17+20 | 13'-11' RT | | 20.1 | | | | | | |
| NW RAMP: | | | | | | | | | | | | |
| RT. | 10+50 | | 11+00 | 15'-85' RT | | | | 235.0 | 235.0 | 157.5 | 70.5 | |
| RT. | 10+54 | | 10+97 | 88'-64' RT. | | 50.0 | | | | | | |
| LT. | 11+25 | | 11+25 | 20'-30' LT | | 10.0 | | | | | | |
| LT. | 11+25 | | 14+00 | 30.0' LT | | 275.0 | | | | | | |
| LT. | 14+00 | | 15+00 | 30'-26' LT | | 100.1 | | | | | | |
| LT. | 15+00 | | 15+26 | 26'-21' LT | | 26.5 | | | | | | |
| | | | | | SUB-TOTAL = | 0.0 | 3826.2 | 0.0 | 324.6 | 324.6 | 217.5 | 97.4 |
| | | | | | TOTAL = | 833.0 | 13187.3 | 14.0 | 419.7 | 419.7 | 281.2 | 125.9 |
| | | | | | ROUND TO: | 840.0 | 13190.0 | 15.0 | 420.0 | 420.0 | 281.0 | 126.0 |

NOTES:

1. STONE RIPRAP AND BEDDING STONE TONNAGE QUANTITIES ARE ESTIMATES AND FOR INFORMATION ONLY.
2. STONE RIPRAP IS ESTIMATED BASED ON 1.5 TONS/ CU. YD.
3. BEDDING STONE IS ESTIMATED BASED ON 1.8 TONS/ CU. YD.

SCHEDULE OF QUANTITIES

REMOVAL ITEMS (SHEET 1 OF 2)

| STATION | TO | STATION | LOCATION | LENGTH (FOOT) | WIDTH (FOOT) | HMA SURFACE REMOVAL BUTT JOINT 40600982 (SQ YD) | PCC SURFACE REMOVAL BUTT JOINT 40600985 (SQ YD) | HMA SURFACE REMOVAL 1 1/2" 44000155 (SQ YD) | HMA SURFACE REMOVAL 1 3/4" 44000156 (SQ YD) | HMA SURFACE REMOVAL 3 1/4" 44000162 (SQ YD) | CONCRETE PAVEMENT SCARIFICATION Z0012800 (SQ YD) | PCC SURFACE REMOVAL 3/4" Z0038116 (SQ YD) |
|-------------|----|-----------|-------------------|------------------|-----------------|---|---|---|---|---|--|---|
| LEG "A": | | | | | | | | | | | | |
| 33+30 | | 32+95 | EB MAIN. & SHLDR. | 35.0 | 40.0 | 155.6 | | | | | | |
| 32+95 | | 26+90.5 | EB MAIN. | 604.5 | 24.0 | | | 1612.0 | | | | |
| 26+90.5 | | 26+78 | EB MAIN. | 12.5 | 24.75 | | | 34.4 | | | | |
| 26+78 | | 24+90.61 | EB MAIN. | 187.39 | 37.25 | | | 775.6 | | | | |
| 24+90.61 | | 24+55.61 | EB MAIN. | 35.0 | 47.25 | | | | | 183.8 | | |
| 24+90.61 | | 24+55.61 | EB PASS. SHLDR. | 35.0 | 5.5 | 21.4 | | | | | | |
| 24+90.61 | | 20+70 | EB DR. SHLDR. | 420.61 | 5.0 | | | 233.7 | | | | |
| 24+55.61 | | 22+00 | EB MAIN. | 255.61 | 49.0 | | | | | 1391.7 | | |
| 22+00 | | 20+70 | EB MAIN. | 130.0 | 49.75 | | | | | 718.6 | | |
| 20+70 | | 20+30.5 | EB MAIN. | 39.5 | 50.0 | | | | | 219.4 | | |
| 20+30.5 | | 19+37 | EB MAIN. | 93.5 | 53.5 | | | | | 555.8 | | |
| 19+37 | | 18+97 | EB MAIN. | 40.0 | 45.75 | | | | | 203.3 | | |
| 18+97 | | 18+75 | EB MAIN. | 22.0 | 38.0 | | | | | 92.9 | | |
| 335+55.87 | | 336+52.39 | EB MAIN. | 96.52 | 38.0 | | | | | 407.5 | | |
| 336+52.39 | | 338+17.39 | EB MAIN. | 165.0 | 27.5 | | | | | 504.2 | | |
| 338+17.39 | | 349+50 | EB MAIN. | 1132.61 | 25.5 | | | 3209.1 | | | | |
| 349+50 | | 358+00 | EB MAIN. | 850.00 | 26.5 | | | 2502.8 | | | | |
| 358+00 | | 360+06 | EB MAIN. | 206.00 | 26.8 | | | 613.4 | | | | |
| 360+06 | | 365+37.80 | EB MAIN. | 531.80 | 25.0 | | | 1477.2 | | | | |
| 365+37.80 | | 365+93.80 | EB MAIN. | 56.00 | 26.0 | | | 161.8 | | | | |
| 365+93.80 | | 366+35.20 | EB MAIN. | 41.4 | 25.5 | 117.3 | | | | | | |
| 366+35.20 | | 366+35.20 | EB DR. SHLDR. | 10.6 | 8.0 | 9.4 | | | | | | |
| 29+70 | | 29+35 | WB MAIN. & SHLDR. | 35.0 | 40.0 | 155.6 | | | | | | |
| 29+35 | | 22+65.61 | WB MAIN. | 669.39 | 24.0 | | | 1785.0 | | | | |
| 22+65.61 | | 22+30.61 | WB MAIN. | 35.0 | 24.5 | | | | | 95.3 | | |
| 22+65.61 | | 22+30.61 | WB PASS. SHLDR. | 35.0 | 4.5 | 17.5 | | | | | | |
| 22+65.61 | | 21+38 | WB DR. SHLDR. | 127.61 | 10.0 | | | 141.8 | | | | |
| 22+30.61 | | 21+38 | WB MAIN. | 92.61 | 27.0 | | | | | 277.8 | | |
| 21+38 | | 20+74 | WB MAIN. | 64.0 | 37.0 | | | | | 263.1 | | |
| 20+74 | | 20+26 | WB MAIN. | 48.0 | 42.0 | | | | | 224.0 | | |
| 20+26 | | 19+37 | WB MAIN. | 89.0 | 55.0 | | | | | 543.9 | | |
| 19+37 | | 18+97 | WB MAIN. | 40.0 | 49.5 | | | | | 220.0 | | |
| 18+97 | | 18+75 | WB MAIN. | 22.0 | 52.0 | | | | | 127.1 | | |
| 18+97 | | 18+75 | WB DR. SHLDR. | 22.0 | 4.0 | | | 9.8 | | | | |
| 335+55.87 | | 339+41.16 | WB MAIN. | 385.29 | 48.0 | | | | | 2054.9 | | |
| 339+41.16 | | 339+76.16 | WB DR. SHLDR. | 420.29 | 4.0 | | | 186.8 | | | | |
| 339+76.16 | | 339+76.16 | WB MAIN. | 35.0 | 46.5 | | | | | 180.8 | | |
| 339+76.16 | | 339+76.16 | WB PASS. SHLDR. | 35.0 | 4.5 | 17.5 | | | | | | |
| 339+76.16 | | 341+78.39 | WB MAIN. | 202.23 | 38.3 | | | 860.6 | | | | |
| 341+78.39 | | 343+28.39 | WB MAIN. | 150.0 | 25.5 | | | 425.0 | | | | |
| 343+28.39 | | 365+93.93 | WB MAIN. | 2265.54 | 26.0 | | | 6544.9 | | | | |
| 365+93.93 | | 366+35.33 | WB MAIN. | 41.4 | 25.5 | 117.3 | | | | | | |
| 366+35.33 | | 366+35.33 | WB DR. SHLDR. | 13.0 | 9.0 | 13.0 | | | | | | |
| SUB-TOTAL = | | | | | | 624.5 | 0.0 | 20001.8 | 572.0 | 8264.1 | 0.0 | 0.0 |

SCHEDULE OF QUANTITIES

REMOVAL ITEMS (SHEET 2 OF 2)

| STATION | TO | STATION | LOCATION | LENGTH (FOOT) | WIDTH (FOOT) | HMA SURFACE REMOVAL BUTT JOINT 40600982 (SQ YD) | PCC SURFACE REMOVAL BUTT JOINT 40600985 (SQ YD) | HMA SURFACE REMOVAL 1 1/2" 44000155 (SQ YD) | HMA SURFACE REMOVAL 1 3/4" 44000156 (SQ YD) | HMA SURFACE REMOVAL 3 1/4" 44000162 (SQ YD) | CONCRETE PAVEMENT SCARIFICATION Z0012800 (SQ YD) | PCC SURFACE REMOVAL 3/4" Z0038116 (SQ YD) |
|-------------|----|------------|--------------------|------------------|--------------|---|---|---|---|---|--|---|
| | | | | | | LEG "C": | | | | | | |
| 32+26.74 | | 32+68.14 | EB MAIN. & SHLDR. | 41.4 | 30.0 | 138.0 | | | | | | |
| 32+68.14 | | 36+55.33 | EB MAIN. | 387.19 | 24.0 | | | 1032.5 | | | | |
| 36+55.33 | | 43+45 | EB MAIN. | 689.67 | 22.5 | | | 1724.2 | | | | |
| 43+45 | | 46+71.97 | EB MAIN. | 326.97 | 24.5 | | | 890.1 | | | | |
| 32+32.06 | | 32+73.46 | WB MAIN. & SHLDR. | 41.4 | 31.0 | 142.6 | | | | | | |
| 32+73.46 | | 35+00 | WB MAIN. | 226.54 | 25.0 | | | 629.3 | | | | |
| 35+00 | | 40+63.87 | WB MAIN. | 563.87 | 24.0 | | | 1503.7 | | | | |
| 40+63.87 | | 43+45 | WB MAIN. | 281.13 | 25.0 | | | 780.9 | | | | |
| 43+45 | | 46+71.97 | WB MAIN. | 326.97 | 24.5 | | | 890.1 | | | | |
| 46+71.97 | | 47+06.97 | EB MAIN. | 35.0 | 24.0 | 93.3 | | | | | | |
| 46+71.97 | | 47+06.97 | WB MAIN. | 35.0 | 24.0 | 93.3 | | | | | | |
| 49+10.79 | | 49+45.79 | EB MAIN. | 35.0 | 24.0 | 93.3 | | | | | | |
| 49+45.79 | | 52+36 | EB MAIN. | 290.21 | 24.5 | | | 790.0 | | | | |
| 52+36 | | 55+00 | EB MAIN. | 264.00 | 24.0 | | | 704.0 | | | | |
| 55+00 | | 72+46.00 | EB MAIN. | 1746.00 | 23.0 | | | 4462.0 | | | | |
| 49+10.79 | | 49+45.79 | WB MAIN. | 35.0 | 24.0 | 93.3 | | | | | | |
| 49+45.79 | | 53+67 | WB MAIN. | 421.21 | 24.5 | | | 1146.6 | | | | |
| 53+67 | | 72+28.5 | WB MAIN. | 1861.5 | 23.0 | | | 4757.2 | | | | |
| 72+46.00 | | 72+89.02 | EB MAIN. | (AREA FROM CADD) | | 124.9 | | | | | | |
| 72+28.50 | | 72+70.53 | WB MAIN. | (AREA FROM CADD) | | 116.8 | | | | | | |
| 76+05.90 | | 76+48.39 | EB MAIN. | (AREA FROM CADD) | | 119.2 | | | | | | |
| 76+48.39 | | 87+41.66 | EB MAIN. | 1093.27 | 23.0 | | | 2793.9 | | | | |
| 87+45.84 | | 89+05.28 | EB MAIN. | 159.44 | 25.5 | | | 451.7 | | | | |
| 75+88.67 | | 76+30.87 | WB MAIN. | (AREA FROM CADD) | | 125.3 | | | | | | |
| 76+30.87 | | 87+41.66 | WB MAIN. | 1110.79 | 23.0 | | | 2838.7 | | | | |
| 87+45.84 | | 89+06.79 | WB MAIN. | 160.95 | 24.5 | | | 438.1 | | | | |
| 89+05.28 | | 89+40.28 | EB MAIN. & SHLDR. | 35.0 | 33.5 | 130.3 | | | | | | |
| 89+06.79 | | 89+41.79 | WB MAIN. & SHLDR. | 35.0 | 33.5 | 130.3 | | | | | | |
| 46+71.97 | | 47+06.97 | WB DR. SHLDR. GUT. | 35.0 | 2.0 | | 7.8 | | | | | |
| 46+71.97 | | 47+06.97 | EB DR. SHLDR. GUT. | 35.0 | 2.0 | | 7.8 | | | | | |
| 49+10.79 | | 49+45.79 | WB DR. SHLDR. GUT. | 35.0 | 2.0 | | 7.8 | | | | | |
| 49+10.79 | | 49+45.79 | EB DR. SHLDR. GUT. | 35.0 | 2.0 | | 7.8 | | | | | |
| 72+28.50 | | 72+62.00 | WB DR. SHLDR. GUT. | 33.5 | 2.0 | | 7.4 | | | | | |
| 72+46.00 | | 72+97.50 | EB DR. SHLDR. GUT. | 51.5 | 2.0 | | 11.4 | | | | | |
| 75+79.37 | | 76+30.87 | WB DR. SHLDR. GUT. | 51.5 | 2.0 | | 11.4 | | | | | |
| 76+14.89 | | 76+48.39 | EB DR. SHLDR. GUT. | 33.5 | 2.0 | | 7.4 | | | | | |
| | | | | | | NW RAMP: | | | | | | |
| 32+32.06 C | | 32+73.46 C | MAIN. | 41.4 | 3.0 | | 13.8 | | | | | |
| 25+41.0 | | 19+54 | MAIN. | 587.0 | 11.5 | | | | | | | 750.1 |
| 19+54 | | 16+35.66 | MAIN. | 318.34 | 16.0 | | | | | | | 565.9 |
| 16+35.66 | | 11+29 | MAIN. | 506.66 | 16.0 | | | | | 900.7 | | |
| 11+29 | | 11+12.50 | MAIN. | 16.50 | 19.0 | | | | | 34.8 | | |
| 10+52.00 | | 11+12.50 | MAIN. | (AREA FROM CADD) | | | 186.5 | | | | | |
| | | | | | | SW RAMP: | | | | | | |
| 10+00 | | 13+03 | MAIN. | 303.0 | 10.0 | | | | | | | 336.7 |
| 13+03 | | 16+77.90 | MAIN. | 374.90 | 16.0 | | | | | | | 666.5 |
| 17+10 | | 22+81.37 | MAIN. | 571.37 | 16.0 | | | | | 1015.8 | | |
| 22+81.37 | | 23+19.88 | LT. SHLDR. | 38.5 | 4.0 | 17.1 | | | | | | |
| 22+81.37 | | 23+31.37 | MAIN. | (AREA FROM CADD) | | | 122.8 | | | | | |
| | | | | | | NE RAMP: | | | | | | |
| 10+00 | | 12+65 | MAIN. | 265.0 | 9.0 | | | | | | | 265.0 |
| 12+65 | | 15+30 | MAIN. | 265.0 | 16.0 | | | | | | | 471.1 |
| 15+30 | | 17+23.12 | MAIN. | 193.12 | 20.0 | | | | | | | 429.2 |
| 17+23.12 | | 18+69 | MAIN. | 145.88 | 30.5 | | | | | 494.4 | | |
| 18+69 | | 21+49.93 | MAIN. | 280.93 | 31.0 | | | | | 967.6 | | |
| 21+49.93 | | 21+99.93 | MAIN. | (AREA FROM CADD) | | | 221.8 | | | | | |
| | | | | | | SE RAMP: | | | | | | |
| 10+68.91 | | 11+18.91 | MAIN. | (AREA FROM CADD) | | | 168.8 | | | | | |
| 10+95.72 | | 11+18.91 | LT. SHLDR. | 23.19 | 4.0 | 10.3 | | | | | | |
| 11+18.91 | | 11+39 | MAIN. | 20.09 | 24.5 | | | | | 54.7 | | |
| 11+39 | | 17+64.89 | MAIN. | 625.89 | 16.0 | | | | | 1112.7 | | |
| 17+64.89 | | 21+15 | MAIN. | 350.11 | 16.0 | | | | | | | 622.4 |
| 21+15 | | 28+53.77 | MAIN. | 738.77 | 10.5 | | | | | | | 861.9 |
| SUB-TOTAL = | | | | | | 1428.1 | 782.6 | 25833.0 | 0.0 | 0.0 | 4580.7 | 4968.7 |
| TOTAL = | | | | | | 2052.6 | 782.6 | 45834.8 | 572.0 | 8264.1 | 4580.7 | 4968.7 |
| ROUND TO: | | | | | | 2060.0 | 790.0 | 45835.0 | 575.0 | 8265.0 | 4590.0 | 4970.0 |

SCHEDULE OF QUANTITIES

DRAINAGE ITEMS

| STATION | LOCATION | PIPE CULVERT, CLASS A, TYPE 2 36" 542A1081 (FOOT) | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 36" 54213681 (EACH) | METAL END SECTIONS 12" 54215547 (EACH) | CONCRETE COLLAR 54248510 (CU YD) | INSERTION CULVERT LINER 36" 54390230 (FOOT) | PIPE DRAINS 12" 60100945 (FOOT) | INLETS TO BE ADJUSTED WITH NEW TYPE 8 GRATE 60261000 (EACH) | FRAMES AND GRATES TO BE ADJUSTED 60300105 (EACH) | REMOVING INLETS 60500060 (EACH) | CLASS SI CONCRETE (OUTLET) 60600095 (CU YD) | CONCRETE THRUST BLOCKS 60900515 (EACH) | PIPE DRAIN REMOVAL X0323586 (FOOT) | TYPE E INLET BOX 610001SPL X6100120 (EACH) |
|-----------|-----------------------|---|--|--|---|---|---|---|--|--|---|--|--|--|
| 339+50 | CL | | | | | | | 1.0 | | | | | | |
| LEG "A": | | | | | | | | | | | | | | |
| LEG "C": | | | | | | | | | | | | | | |
| 45+00 | 28' RT. | | | | | | | | 1.0 | | | | | |
| 46+89 | 28' RT. | | | | | | | | 1.0 | | | | | |
| 46+89 | 28' LT. | | | | | | | | 1.0 | | | | | |
| 46+97 | 28' RT. | | | | | | | | 1.0 | | | | | |
| 46+97 | 28' LT. | | | | | | | | 1.0 | | | | | |
| 49+23 | 28' RT. | | | | | | | | 1.0 | | | | | |
| 49+25 | 28' LT. | | | | | | | | 1.0 | | | | | |
| 49+31 | 28' RT. | | | | | | | | 1.0 | | | | | |
| 51+03 | 28' LT. | | | | | | | | 1.0 | | | | | |
| 52+36 | 29' RT. | | | | | | | | 1.0 | | | | | |
| 52+90 | 29' LT. | | | | | | | | 1.0 | | | | | |
| 61+60 | 29' LT. | | | | | | | | 1.0 | | | | | |
| 62+85.6 | 40.4' RT. | | | | | | | | | | 2.08 | | | |
| 63+94.35 | 35.85' LT.-48.10' RT. | | | | | 83.95 | | | | | | | | |
| 63+94.35 | 35.85' LT. | | | | 0.69 | | | | | | | | | |
| 63+94.35 | 35.85' LT.-43.85' LT. | 8.0 | | | | | | | | | | | | |
| 63+94.35 | 43.85' LT. | | | | 0.69 | | | | | | | | | |
| 63+94.35 | 52.0' LT. | | 1.0 | | | | | | | | | | | |
| 63+94.35 | 48.10' RT. | | | | 0.69 | | | | | | | | | |
| 63+94.35 | 48.10' RT.-56.10' RT. | 8.0 | | | | | | | | | | | | |
| 63+94.35 | 56.10' RT. | | | | 0.69 | | | | | | | | | |
| 63+94.35 | 64.25' RT. | | 1.0 | | | | | | | | | | | |
| 65+20 | 29' LT. | | | | | | | | 1.0 | | | | | |
| 65+75 | 34' RT. | | | | | | | | | | | | | 1.0 |
| 65+75 | 42' RT. | | | | | | 20.0 | | | | | | | |
| 65+75 | 53' RT. | | | 1.0 | | | | | | | | | | |
| 65+75 | 35' RT. | | | | 0.03 | | | | | | | | | |
| 65+75 | 51' RT. | | | | | | | | | | | 1.0 | | |
| 68+60 | 29' RT. | | | | | | | | 1.0 | | | | | |
| 68+60 | 29' LT. | | | | | | | | 1.0 | | | | | |
| 71+10 | 29' RT. | | | | | | | | 1.0 | | | | | |
| 71+10 | 29' LT. | | | | | | | | 1.0 | | | | | |
| 75+78 | 28' LT. | | | | | | | | 1.0 | | | | | |
| 75+87 | 28' LT. | | | | | | | | 1.0 | | | | | |
| 76+16 | 28' RT. | | | | | | | | 1.0 | | | | | |
| 76+23 | 28' RT. | | | | | | | | 1.0 | | | | | |
| 80+80 | 29' RT. | | | | | | | | 1.0 | | | | | |
| 80+80 | 29' LT. | | | | | | | | 1.0 | | | | | |
| 84+76 | 29' RT. | | | | | | | | 1.0 | | | | | |
| 84+76 | 29' LT. | | | | | | | | 1.0 | | | | | |
| 87+33 | 29' LT. | | | | | | | | 1.0 | | | | | |
| 87+46.39 | 29' RT. | | | | | | | | 1.0 | | | | | |
| 89+35.79 | 35.2' RT. | | | | | | | | | | 2.08 | | | |
| 89+37.79 | 36.3' LT. | | | | | | | | | | 2.08 | | | |
| NW RAMP: | | | | | | | | | | | | | | |
| 10+75 | 30' LT. | | | | | | | | 1.0 | | | | | |
| 10+69 | 12' RT. | | | | | | | | 1.0 | | | | | |
| 15+00 | 8.5' RT. | | | | | | | | 1.0 | | | | | |
| SE RAMP: | | | | | | | | | | | | | | |
| 10+80 | 16' RT. | | | | | | | | 1.0 | | | | | |
| NE RAMP: | | | | | | | | | | | | | | |
| 11+75 | 8.5' RT. | | | | | | | | | 1.0 | 0.98 | | 10.0 | |
| 13+75 | 8.5' RT. | | | | | | | | | 1.0 | 0.98 | | 12.0 | |
| 15+65 | 8.5' RT. | | | | | | | | | 1.0 | 0.98 | | 14.0 | |
| 16+93 | 29' LT. | | | | | | | | 1.0 | | | | | |
| TOTAL = | | 16.0 | 2.0 | 1.0 | 2.79 | 83.95 | 20.0 | 1.0 | 32.0 | 3.0 | 9.18 | 1.0 | 36.0 | 1.0 |
| ROUND TO: | | 16.0 | 2.0 | 1.0 | 2.8 | 84 | 20.0 | 1.0 | 32.0 | 3.0 | 9.2 | 1.0 | 40.0 | 1.0 |

SCHEDULE OF QUANTITIES

PCC CURB & MEDIAN

| LOCATION | | | LENGTH (FOOT) | WIDTH (FOOT) | COMBINATION CURB & GUTTER REMOVAL 44000500 (FOOT) | PAVED DITCH REMOVAL 44004000 (FOOT) | PAVED SHOULDER REMOVAL 44004250 (SQ YD) | TIE BARS 1/2" 44213198 (EACH) | CONCRETE REMOVAL 50102400 (CU YD) | CONCRETE CURB TYPE B 60600605 (FOOT) | COMBINATION CONCRETE CURB & GUTTER TYPE B-6.24 60605000 (FOOT) | CONCRETE SHOULDER CURB 66201120 (FOOT) | CURB REMOVAL SPECIAL X4402400 (FOOT) | CONCRETE MEDIAN REMOVAL X4403300 (SQ FT) | CONCRETE MEDIAN TYPE SM (DOWELLED) X6061702 (SQ FT) |
|-------------------|----|---------------------|------------------|-----------------|---|---|---|--|--|--|---|--|--|--|--|
| STATION | TO | STA./DIR./OFFSET | | | | | | | | | | | | | |
| NE RAMP: | | | | | | | | | | | | | | | |
| 10+00 RT. | | 15+75 RT. | 575.0 | | | | | | | | | | 575.0 | | |
| 15+75 RT. | | 17+19.16 RT. | 144.16 | *0.5 | | | 8.0 | | | | | | | | |
| 10+00 RT. | | 17+19.16 RT. | 719.16 | | | | | | | 719.16 | | | | | |
| LEG "C": | | | | | | | | | | | | | | | |
| 32+32.92 CL | | 47+06.97 CL | 1474.05 | 3.33 | | | | | | | | | | 4908.6 | 4908.6 |
| 40+58.47 EB | | 41+65.00 EB | (AREA FROM CADD) | | | | 27.4 | | | | | | | | |
| 49+10.79 CL | | 72+79.83 CL | 2369.04 | 3.33 | | | | | | | | | | 7888.9 | 7888.9 |
| 53+98.45 EB | | 55+23.25 EB | (AREA FROM CADD) | | | | 59.6 | | | | | | | | |
| 61+60 WB | | 29' LT. | | | | | | | | 10.0 | | | | | |
| S.N. 057-0063 | | | | | | | | | 7.0 | | | | | | |
| 63+94.35 | | 46' RT. - 77' RT. | 31.0 | | | 31.0 | | | | | | | | | |
| 63+94.35 | | EB & WB | | | | | | | 1.2 | | | | | | |
| 65+20 | | WB 29' LT. | | | | | | | | 10.0 | | | | | |
| 68+60 | | WB 29' LT. | | | | | | | | 10.0 | | | | | |
| 68+60 | | EB 29' RT. | | | | | | | | 10.0 | | | | | |
| 71+10 | | WB 29' LT. | | | | | | | | 10.0 | | | | | |
| 71+10 | | EB 29' RT. | | | | | | | | 10.0 | | | | | |
| S.N. 057-0064 | | | | | | | | | 7.2 | | | | | | |
| 75+97.03 CL | | 87+41.66 BK. CL | 1144.63 | 3.33 | | | | | | | | | | 3811.6 | 3811.6 |
| 80+80 | | WB 29' LT. | | | | | | | | 10.0 | | | | | |
| 80+80 | | EB 29' RT. | | | | | | | | 10.0 | | | | | |
| 84+76 | | WB 29' LT. | | | | | | | | 10.0 | | | | | |
| 84+76 | | EB 29' RT. | | | | | | | | 10.0 | | | | | |
| 87+33 | | WB 29' LT. | | | | | | | | 10.0 | | | | | |
| 87+42 | | EB 29' RT. | | | | | | | | 10.0 | | | | | |
| 87+45.84 (AH.) CL | | 89+30.39 CL | 184.55 | 3.33 | | | | | | | | | | 614.6 | 614.6 |
| 60+45 | | WB 71+80 LT. | 1135.0 | | | | | | | 1135.0 | | | 1135.0 | | |
| 62+77.5 | | EB 69+17 RT. | 639.5 | *0.5 | | | 35.5 | | | | | | | | |
| 62+90 | | EB 65+70 RT. | 280.0 | | | | | | | 280.0 | | | | | |
| 65+70 | | EB 65+80 RT. | 10.0 | | | | | | | | | 10.0 | | | |
| 65+80 | | EB 72+16 RT. | 636.0 | | | | | | | 636.0 | | | | | |
| 69+17 | | EB 72+16 RT. | 299.0 | | | | | | | | | | 299.0 | | |
| 76+60 | | WB 87+41.66 BK. | 1081.66 | | | | | | | 1081.66 | | | 1081.66 | | |
| 87+45.84 AH. | | WB 89+33.39 | 187.55 | | | | | | | 187.55 | | | | | |
| 76+97 | | EB 87+41.66 BK. | 1044.66 | | | | | | | 1044.66 | | | 1044.66 | | |
| 87+45.84 | | EB 87+52.23 | 6.39 | | | | | | | | | | 6.39 | | |
| 87+52.23 | | EB 89+40.28 RT. | 188.05 | *0.5 | | | 10.4 | | | | | | | | |
| 87+45.84 AH. | | EB 89+31.39 | 185.55 | | | | | | | 185.55 | | | | | |
| 87+45.84 AH. | | WB 89+41.79 LT. | 195.95 | *0.5 | | | 10.9 | | | | | | | | |
| NW RAMP: | | | | | | | | | | | | | | | |
| 11+00 RT. | | 11+30 RT. | 30.0 | | 30.0 | | | | | | 30.0 | | | | |
| LEG "A": | | | | | | | | | | | | | | | |
| 336+52.39 (OUT.) | | EB 361+10 (OUT.) | 2457.61 | 2.25 | | | 614.4 | | | | | | | | |
| 361+10 (OUT.) | | EB 366+24.60 (OUT.) | 514.6 | 2.25 | | | 128.7 | | | | | | | | |
| 341+78.39 (OUT.) | | WB 366+22.33 (OUT.) | 2443.94 | 1.5 | | | 407.3 | | | | | | | | |
| TOTAL= | | | 30.0 | | 31.0 | | 1,302.2 | 120.0 | 15.4 | 5,269.6 | 30.0 | 10.0 | 4,141.7 | 17,223.7 | 17,223.7 |
| ROUND TO: | | | 30.0 | | 31.0 | | 1,310.0 | 120.0 | 15.4 | 5,270.0 | 30.0 | 10.0 | 4,150.0 | 17,225.0 | 17,225.0 |

NOTE: *0.5' ESTIMATED WIDTH TO PRODUCE STRAIGHT LINE. R. E. TO DETERMINE REMOVAL LINE IN FIELD.
THE CONTRACTOR WILL BE PAID FOR THE ACTUAL QUANTITY OF PAVED SHOULDER REMOVED.

SCHEDULE OF QUANTITIES

HOT-MIX ASPHALT (SHEET 2 OF 3)

| LOCATION | DIRECTION | STATION | TO | STATION | WIDTH OF INSIDE SHOULDER (FOOT) | WIDTH OF OUTSIDE SHOULDER (FOOT) | THICKNESS (INCHES) | (A) LENGTH (FOOT) | (B) WIDTH (FOOT) | (C) AREA (SQ YD) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GALLON) | HOT-MIX ASPHALT SHOULDERS 8" 48203029 (SQ YD) | HOT-MIX ASPHALT SHOULDERS 48203100 (TON) | |
|-------------|-----------|-----------|----------|-----------|---------------------------------|----------------------------------|--------------------|-------------------|------------------|------------------|---|---|--|------|
| LEG "A": | EB | 33+30.00 | | 26+78.00 | | 10.0 | 1.75 | 652.00 | 10.0 | 724.4 | 72.4 | | 71.0 | |
| | EB | 26+78.00 | | 20+74.00 | | 5.0 | 1.75 | 604.00 | 5.0 | 335.6 | 33.6 | | 32.9 | |
| | EB | 33+30.00 | | 24+55.00 | 6.0 | | 1.75 | 875.00 | 6.0 | 583.3 | 58.3 | | 57.2 | |
| | WB | 29+70.00 | | 22+30.61 | 5.0 | 10.0 | 1.75 | 739.39 | 15.0 | 1232.3 | 123.2 | | 120.8 | |
| | WB | 22+30.61 | | 21+36.11 | | 10.0 | 1.75 | 94.50 | 10.0 | 105.0 | 10.5 | | 10.3 | |
| | WB | 18+97.36 | | 18+75.00 | | 4.0 | 1.75 | 22.36 | 4.0 | 9.9 | 1.0 | | 1.0 | |
| | WB | 335+55.87 | | 340+00.00 | | 4.0 | 1.75 | 444.13 | 4.0 | 197.4 | 19.7 | | 19.3 | |
| | WB | 340+00 | | 341+78.39 | | 5.5 | 1.75 | 178.39 | 5.5 | 109.0 | 10.9 | | 10.7 | |
| | WB | 339+41.16 | | 341+73.00 | 4.0 | | 1.75 | 231.84 | 4.0 | 103.0 | 10.3 | | 10.1 | |
| | EB | 336+52.39 | | 366+24.60 | | 8.0 | 8.0 | 2972.21 | 8.0 | 2642.0 | 264.2 | 2642.0 | | |
| | EB | 366+24.60 | | 366+35.20 | | 8.0 | 1.75 | 10.60 | 8.0 | 9.4 | 0.9 | | 0.9 | |
| | WB | 341+78.39 | | 366+22.33 | | 8.0 | 8.0 | 2443.94 | 8.0 | 2172.4 | 217.2 | 2172.4 | | |
| | WB | 366+22.33 | | 366+35.33 | | 8.0 | 1.75 | 13.00 | 8.0 | 11.6 | 1.2 | | 1.1 | |
| | LEG "C": | EB | 32+26.74 | | 33+93.50 | | 8.0 | 1.75 | 166.76 | 8.0 | 148.2 | 14.8 | | 14.5 |
| | | EB | 36+97.00 | | 39+21.00 | | 9.0 | 1.75 | 224.00 | 9.0 | 224.0 | 22.4 | | 22.0 |
| EB | | 39+21.00 | | 40+58.47 | | 5.5 | 1.75 | 137.47 | 5.5 | 84.0 | 8.4 | | 8.2 | |
| EB | | 40+58.47 | | 43+41.11 | | 4.0 | 8 | 282.64 | 4.0 | 125.6 | 12.6 | 125.6 | | |
| EB | | 43+41.11 | | 47+06.97 | | 2.0 | 1.625 | 365.86 | 2.0 | 81.3 | 8.1 | | 7.4 | |
| EB | | 49+10.79 | | 52+35.85 | | 2.0 | 1.625 | 325.06 | 2.0 | 72.2 | 7.2 | | 6.6 | |
| EB | | 52+35.85 | | 55+23.25 | | 4.0 | 8 | 287.40 | 4.0 | 127.7 | 12.8 | 127.7 | | |
| EB | | 55+23.25 | | 58+65.00 | | 10.5 | 1.75 | 341.75 | 10.5 | 398.7 | 39.9 | | 39.1 | |
| EB | | 66+00.00 | | 69+20.00 | | 8.0 | 1.75 | 320.00 | 8.0 | 284.4 | 28.4 | | 27.9 | |
| EB | | 69+20.00 | | 72+16.00 | | 6.25 | 1.75 | 296.00 | 6.25 | 205.6 | 20.6 | | 20.1 | |
| EB | | 72+16.00 | | 72+96.00 | | 3.0 | 1.75 | 80.00 | 3.0 | 26.7 | 2.7 | | 2.6 | |
| EB | | 72+16.00 | | 72+96.00 | | 2.0 | 1.625 | 80.00 | 2.0 | 17.8 | 1.8 | | 1.6 | |
| EB | | 76+14.89 | | 76+96.00 | | 2.0 | 1.625 | 81.11 | 2.0 | 18.0 | 1.8 | | 1.6 | |
| EB | | 76+14.89 | | 76+96.00 | | 3.0 | 1.75 | 81.11 | 3.0 | 27.0 | 2.7 | | 2.6 | |
| EB | | 76+96.00 | | 82+04.00 | | 6.75 | 1.75 | 508.00 | 6.75 | 381.0 | 38.1 | | 37.3 | |
| EB | | 82+04.00 | | 84+54.00 | | 6.5 | 1.75 | 250.00 | 6.5 | 180.6 | 18.1 | | 17.7 | |
| EB | | 84+54.00 | | 87+41.66 | | 6.75 | 1.75 | 287.66 | 6.75 | 215.7 | 21.6 | | 21.1 | |
| EB | | 87+45.84 | | 89+40.28 | | 5.5 | 1.75 | 194.44 | 5.5 | 118.8 | 11.9 | | 11.6 | |
| WB | | 40+63.00 | | 41+67.81 | | 4.0 | 1.75 | 104.81 | 4.0 | 46.6 | 4.7 | | 4.6 | |
| WB | | 41+67.81 | | 43+93.38 | | 4.0 | 8 | 225.57 | 4.0 | 100.3 | 10.0 | 100.3 | | |
| WB | | 43+93.38 | | 47+06.97 | | 2.0 | 1.625 | 313.59 | 2.0 | 69.7 | 7.0 | | 6.3 | |
| WB | | 49+10.79 | | 52+91.71 | | 2.0 | 1.625 | 380.92 | 2.0 | 84.6 | 8.5 | | 7.7 | |
| WB | | 52+91.71 | | 53+88.46 | | | 8 | (Area From CADD) | | 49.3 | 4.9 | 49.3 | | |
| WB | | 53+34.92 | | 53+88.46 | | | 1.75 | (Area From CADD) | | 24.9 | 2.5 | | 2.4 | |
| WB | | 53+88.46 | | 55+23.00 | | 10.0 | 1.75 | 134.54 | 10.0 | 149.5 | 14.9 | | 14.6 | |
| WB | | 55+23.00 | | 57+79.00 | | 7.5 | 1.75 | 256.00 | 7.5 | 213.3 | 21.3 | | 20.9 | |
| WB | | 60+44.00 | | 71+80.00 | | 6.0 | 1.75 | 1136.00 | 6.0 | 757.3 | 75.7 | | 74.2 | |
| WB | | 71+80.00 | | 72+62.00 | | 3.0 | 1.75 | 82.00 | 3.0 | 27.3 | 2.7 | | 2.7 | |
| WB | | 71+80.00 | | 72+62.00 | | 2.0 | 1.625 | 82.00 | 2.0 | 18.2 | 1.8 | | 1.7 | |
| WB | | 75+79.37 | | 76+60.00 | | 3.0 | 1.75 | 80.63 | 3.0 | 26.9 | 2.7 | | 2.6 | |
| WB | 75+79.37 | | 76+60.00 | | 2.0 | 1.625 | 80.63 | 2.0 | 17.9 | 1.8 | | 1.6 | | |
| WB | 76+60.00 | | 87+41.66 | | 6.0 | 1.75 | 1081.66 | 6.0 | 721.1 | 72.1 | | 70.7 | | |
| WB | 87+45.84 | | 89+41.79 | | 5.0 | 1.75 | 195.95 | 5.0 | 108.9 | 10.9 | | 10.7 | | |
| SUB-TOTAL = | | | | | | | | | | | 1338.9 | 5217.3 | 798.1 | |

SCHEDULE OF QUANTITIES

HOT-MIX ASPHALT (SHEET 3 OF 3)

| LOCATION | DIRECTION | STATION | TO | STATION | WIDTH OF INSIDE SHOULDER (FOOT) | WIDTH OF OUTSIDE SHOULDER (FOOT) | THICKNESS (INCHES) | (A) LENGTH (FOOT) | (B) WIDTH (FOOT) | (C) AREA (SQ YD) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GALLON) | HOT-MIX ASPHALT SHOULDERS 8" 48203029 (SQ YD) | HOT-MIX ASPHALT SHOULDERS 48203100 (TON) |
|---------------------------|-----------|----------|----|----------|---------------------------------|----------------------------------|--------------------|-------------------|------------------|------------------|---|---|--|
| NW RAMP: | RT. | 11+00.00 | | 11+56.00 | | 3.0 | 2.5 | 56.00 | 3.0 | 18.7 | 1.9 | | 2.6 |
| | RT. | 11+00.00 | | 11+56.00 | | 2.0 | 2.0 | 56.00 | 2.0 | 12.4 | 1.2 | | 1.4 |
| | RT. | 11+56.00 | | 17+36.00 | | 6.0 | 2.5 | 580.00 | 6.0 | 386.7 | 38.7 | | 54.1 |
| | RT. | 11+56.00 | | 17+36.00 | | 2.0 | 2.0 | 580.00 | 2.0 | 128.9 | 12.9 | | 14.4 |
| | RT. | 17+36.00 | | 17+51.00 | | 6.0 | 2.125 | 15.00 | 6.0 | 10.0 | 1.0 | | 1.2 |
| | RT. | 17+36.00 | | 17+51.00 | | 2.0 | 1.8125 | 15.00 | 2.0 | 3.3 | 0.3 | | 0.3 |
| | RT. | 17+51.00 | | 25+82.40 | | 6.0 | 1.75 | 831.40 | 6.0 | 554.3 | 55.4 | | 54.3 |
| | RT. | 17+51.00 | | 25+24.50 | | 2.0 | 1.625 | 773.50 | 2.0 | 171.9 | 17.2 | | 15.6 |
| | RT. | 25+17 | | OUTLET | | 2.0 | 0.875 | 20.00 | 2.0 | 4.4 | 0.4 | | 0.2 |
| | LT. | 11+29.00 | | 16+49.00 | | 4.0 | 2.5 | 520.00 | 4.0 | 231.1 | 23.1 | | 32.4 |
| | LT. | 15+26.00 | | 16+49.00 | | 2.0 | 2.0 | 123.00 | 2.0 | 27.3 | 2.7 | | 3.1 |
| | LT. | 17+51.00 | | 19+54.00 | | 4.5 | 1.75 | 203.00 | 4.5 | 101.5 | 10.2 | | 9.9 |
| SW RAMP: | RT. | 10+00.00 | | 15+27.00 | | 6.0 | 1.75 | 527.00 | 6.0 | 351.3 | 35.1 | | 34.4 |
| | RT. | 11+75.00 | | 15+27.00 | | 2.0 | 1.625 | 352.00 | 2.0 | 78.2 | 7.8 | | 7.1 |
| | RT. | 15+27.00 | | 15+42.00 | | 6.0 | 2.125 | 15.00 | 6.0 | 10.0 | 1.0 | | 1.2 |
| | RT. | 15+27.00 | | 16+18.00 | | 2.0 | 1.8125 | 91.00 | 2.0 | 20.2 | 2.0 | | 2.1 |
| | RT. | 15+42.00 | | 22+43.00 | | 6.0 | 2.5 | 701.00 | 6.0 | 467.3 | 46.7 | | 65.4 |
| | LT. | 15+27.00 | | 15+42.00 | | 5.0 | 2.125 | 15.00 | 5.0 | 8.3 | 0.8 | | 1.0 |
| SE RAMP: | LT. | 15+42.00 | | 16+62.00 | | 8.0 | 2.5 | 120.00 | 8.0 | 106.7 | 10.7 | | 14.9 |
| | LT. | 16+62.00 | | 23+20.00 | | 4.0 | 2.5 | 658.00 | 4.0 | 292.4 | 29.2 | | 40.9 |
| | RT. | 11+39.00 | | 17+59.00 | | 6.0 | 2.5 | 620.00 | 6.0 | 413.3 | 41.3 | | 57.9 |
| | RT. | 17+59.00 | | 17+74.00 | | 6.0 | 2.125 | 15.00 | 6.0 | 10.0 | 1.0 | | 1.2 |
| NE RAMP: | RT. | 17+74.00 | | 28+53.77 | | 6.0 | 1.75 | 1079.77 | 6.0 | 719.8 | 72.0 | | 70.5 |
| | LT. | 10+96.00 | | 17+55.00 | | 4.0 | 2.5 | 659.00 | 4.0 | 292.9 | 29.3 | | 41.0 |
| | LT. | 17+55.00 | | 17+74.00 | | 12.5 | 2.125 | 19.00 | 12.5 | 26.4 | 2.6 | | 3.1 |
| | RT. | 10+00.00 | | 16+55.00 | | 8.0 | 1.75 | 655.00 | 8.0 | 582.2 | 58.2 | | 57.1 |
| NE RAMP: | RT. | 16+55.00 | | 16+80.00 | | 5.5 | 2.125 | 25.00 | 5.5 | 15.3 | 1.5 | | 1.8 |
| | RT. | 16+80.00 | | 17+20.00 | | 4.75 | 2.5 | 40.00 | 4.75 | 21.1 | 2.1 | | 3.0 |
| | RT. | 17+20.00 | | 21+86.00 | | 2.0 | 2.0 | 466.00 | 2.0 | 103.6 | 10.4 | | 11.6 |
| | LT. | 15+30.00 | | 16+55.00 | | 4.0 | 1.75 | 125.00 | 4.0 | 55.6 | 5.6 | | 5.4 |
| | LT. | 16+55.00 | | 16+92.00 | | 6.0 | 1.75 | 37.00 | 6.0 | 24.7 | 2.5 | | 2.4 |
| | LT. | 16+92.00 | | 17+07.00 | | 5.5 | 1.75 | 15.00 | 5.5 | 9.2 | 0.9 | | 0.9 |
| | LT. | 17+07.00 | | 21+86.00 | | 2.0 | 2.0 | 479.00 | 2.0 | 106.4 | 10.6 | | 11.9 |
| SUB-TOTAL = | | | | | | | | | | | 536.6 | 0.0 | 624.6 |
| FROM HMA PAVING SCHEDULE: | | | | | | | | | | | 13188.3 | | |
| TOTAL = | | | | | | | | | | | 15063.7 | 5217.3 | 1422.7 |
| ROUND TO: | | | | | | | | | | | 15070.0 | 5220.0 | 1430.0 |

HOT-MIX ASPHALT & SHOULDER SUMMARY

| LOCATION: | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GALLON) | AGGREGATE (PRIME COAT) 40600300 (TON) | POLYMERIZED LEVEL BINDER MACHINE METHOD, IL-9.5, FG, N90 40600909 (TON) | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90 40603570 (TON) | HOT-MIX ASPHALT SHOULDERS, 8" 48203029 (SQ YD) | HOT-MIX ASPHALT SHOULDERS 48203100 (TON) |
|-----------|---|---------------------------------------|---|---|--|--|
| LEG "A": | 6,538.2 | 57.1 | 2,000.2 | 3200.1 | 4,814.4 | 335.3 |
| LEG "C": | 5,929.2 | 54.2 | 1,894.9 | 3,031.7 | 402.9 | 462.9 |
| SW RAMP: | 566.4 | 4.3 | 121.3 | 181.9 | | 167.0 |
| NW RAMP: | 623.0 | 4.7 | 128.3 | 192.4 | | 189.6 |
| NE RAMP: | 696.5 | 5.9 | 169.3 | 254.0 | | 94.1 |
| SE RAMP: | 710.6 | 5.6 | 158.1 | 237.1 | | 173.7 |
| TOTAL = | 15,063.9 | 131.8 | 4,472.1 | 7,097.2 | 5,217.3 | 1,422.6 |
| ROUND TO | 15,070.0 | 135.0 | 4,480.0 | 7,100.0 | 5,220.0 | 1,430.0 |

SCHEDULE OF QUANTITIES

PAVEMENT MARKING (SHEET 1 OF 2)

PREFORMED PLASTIC PAVEMENT MARKING, LETTERS & SYMBOLS

| DIRECTION | DESCRIPTION | STATION | PREFORMED PLASTIC PAVEMENT MARKING LETTERS & SYMBOLS 78003100 (SQ FT) |
|-----------|--------------------|----------|---|
| LEG "A": | | | |
| WB | LEFT ARROW | 18+86.75 | 15.6 |
| WB | RIGHT ARROW | 18+86.75 | 15.6 |
| WB | LEFT ARROW | 336+23.5 | 15.6 |
| WB | RIGHT ARROW | 336+23.5 | 15.6 |
| WB | LEFT ARROW | 337+02.8 | 15.6 |
| WB | RIGHT ARROW | 337+02.8 | 15.6 |
| WB | LEFT ARROW | 337+82.1 | 15.6 |
| WB | RIGHT ARROW | 337+82.1 | 15.6 |
| WB | LEFT ARROW | 338+61.4 | 15.6 |
| WB | RIGHT ARROW | 338+61.4 | 15.6 |
| WB | LEFT ARROW | 339+40.7 | 15.6 |
| WB | RIGHT ARROW | 339+40.7 | 15.6 |
| EB | LEFT ARROW | 24+55 | 15.6 |
| EB | RIGHT ARROW | 24+55 | 15.6 |
| EB | LEFT ARROW | 23+86.5 | 15.6 |
| EB | RIGHT ARROW | 23+86.5 | 15.6 |
| EB | LEFT ARROW | 23+19 | 15.6 |
| EB | RIGHT ARROW | 23+19 | 15.6 |
| EB | LEFT ARROW | 22+51.5 | 15.6 |
| EB | RIGHT ARROW | 22+51.5 | 15.6 |
| EB | LEFT ARROW | 21+84 | 15.6 |
| EB | RIGHT ARROW | 21+84 | 15.6 |
| EB | LEFT ARROW | 21+16.5 | 15.6 |
| EB | RIGHT ARROW | 21+16.5 | 15.6 |
| EB | LEFT ARROW | 20+49 | 15.6 |
| EB | RIGHT ARROW | 20+49 | 15.6 |
| NE RAMP: | | | |
| LT | INSIDE LEFT ARROW | 20+08 | 15.6 |
| LT | OUTSIDE LEFT ARROW | 19+08 | 15.6 |
| LT | RIGHT ARROW | 20+08 | 15.6 |
| LT | OUTSIDE LEFT ARROW | 20+08 | 15.6 |
| LT | INSIDE LEFT ARROW | 21+08 | 15.6 |
| LT | RIGHT ARROW | 21+08 | 15.6 |
| LT | OUTSIDE LEFT ARROW | 21+08 | 15.6 |
| TOTAL = | | | 514.8 |
| ROUND TO: | | | 514.8 |

MODIFIED URETHANE PAVEMENT MARKING, 4"

| DIRECTION | DESCRIPTION | STATION | TO | STATION | LENGTH (FEET) | MODIFIED URETHANE PAVEMENT MARKING LINE 4 78009004 (FOOT) | | | GROOVING FOR RECESSED PAVEMENT MARKING 5" X7830070 (FOOT) |
|-----------|-----------------------------|-----------|----|-----------|---------------|---|-------------|------------|---|
| | | | | | | WHITE E.L. | YELLOW E.L. | WHITE C.L. | |
| LEG "A": | | | | | | | | | |
| WB | EL | 29+70 | | 20+27 | 943.0 | | 943.0 | | 943.0 |
| WB | EL | 29+70 | | 21+36 | 834.0 | 834.0 | | | 834.0 |
| WB | EL | 18+97 | | 18+75 | 22.0 | 22.0 | | | 22.0 |
| WB | EL / LEFT TURN LANE LINE | 19+05 | | 18+75 | 30.0 | 30.0 | | | 30.0 |
| WB | EL / RIGHT TURN LANE LINE | 19+05 | | 18+75 | 30.0 | 30.0 | | | 30.0 |
| WB | EL | 335+55.87 | | 366+35.33 | 3079.5 | 3079.5 | | | 3079.5 |
| WB | EL / LEFT TURN LANE LINE | 335+55.87 | | 339+40.7 | 384.83 | 384.8 | | | 384.8 |
| WB | EL / RIGHT TURN LANE LINE | 335+55.87 | | 339+40.7 | 384.83 | 384.8 | | | 384.8 |
| WB | EL | 339+41 | | 366+35.33 | 2694.3 | | 2694.3 | | 2694.3 |
| EB | CL | 43+70 | | 33+30 | 1040.0 | | | 260.0 | 260.0 |
| EB | EL | 33+30 | | 20+31 | 1299.0 | 1299.0 | | | 1299.0 |
| EB | EL | 33+30 | | 24+55.61 | 874.4 | | 874.4 | | 874.4 |
| EB | EL / LEFT TURN LANE LINE | 24+55 | | 20+31 | 424.0 | 424.0 | | | 424.0 |
| EB | EL / RIGHT TURN LANE LINE | 24+55 | | 20+31 | 424.0 | 424.0 | | | 424.0 |
| EB | EL | 19+12 | | 18+75 | 37.0 | | 37.0 | | 37.0 |
| EB | EL | 335+55.87 | | 366+35.20 | 3079.3 | | 3079.3 | | 3079.3 |
| EB | EL | 335+97 | | 366+35.20 | 3038.2 | 3038.2 | | | 3038.2 |
| LEG "C": | | | | | | | | | |
| WB | EL | 32+32.06 | | 47+06.97 | 1474.9 | | 1474.9 | | 1474.9 |
| WB | EL | 41+90 | | 47+06.97 | 516.97 | 517.0 | | | 517.0 |
| WB | CL/EL | 47+06.97 | | 49+10.79 | 203.82 | 203.8 | | 51.0 | 458.6 |
| WB | EL | 49+10.79 | | 53+35 | 424.21 | 424.2 | | | 424.2 |
| WB | EL | 49+10.79 | | 72+70.53 | 2359.7 | | 2359.7 | | 2359.7 |
| WB | EL | 60+44 | | 72+70.53 | 1226.5 | 1226.5 | | | 1226.5 |
| WB | CL/EL | 72+70.53 | | 75+88.67 | 318.14 | 318.1 | | 79.5 | 715.8 |
| WB | EL | 75+88.67 | | 87+41.66 | 1153.0 | 1153.0 | | | 2306.0 |
| WB | EL | 87+45.84 | | 89+41.79 | 195.95 | 195.9 | | | 391.9 |
| WB | CL | 89+41.79 | | 99+81.79 | 1040.0 | | | 260.0 | 260.0 |
| EB | EL | 32+26.74 | | 47+06.97 | 1480.2 | | 1480.2 | | 1480.2 |
| EB | EL | 32+26.74 | | 33+95 | 168.26 | 168.3 | | | 168.3 |
| EB | EL | 40+59 | | 47+06.97 | 647.97 | 648.0 | | | 648.0 |
| EB | CL/EL | 47+06.97 | | 49+10.79 | 203.82 | 203.8 | | 51.0 | 458.6 |
| EB | EL | 49+10.79 | | 55+55 | 644.21 | 644.2 | | | 644.2 |
| EB | EL | 49+10.79 | | 72+89.02 | 2378.2 | | 2378.2 | | 2378.2 |
| EB | EL | 66+03 | | 72+89.02 | 686.02 | 686.0 | | | 686.0 |
| EB | CL/EL | 72+89.02 | | 76+05.90 | 316.88 | 316.9 | | 79.2 | 713.0 |
| EB | EL | 76+05.90 | | 87+41.66 | 1135.8 | 1135.8 | | | 2271.5 |
| EB | EL | 87+45.84 | | 89+40.28 | 194.44 | 194.4 | | 194.4 | 388.9 |
| NE RAMP: | | | | | | | | | |
| RT | EL | 17+08 | | 17+08 | 708.0 | 708.0 | | | 708.0 |
| LT | EL / OUTSIDE LEFT LANE LINE | 19+08 | | 22+00 | 292.0 | 292.0 | | | 292.0 |
| LT | EL / INSIDE LEFT LANE LINE | 17+20 | | 21+86 | 466.0 | 466.0 | | | 466.0 |
| NW RAMP: | | | | | | | | | |
| RT | EL | 10+55 | | 25+56.5 | 1501.5 | 1501.5 | | | 1501.5 |
| LT | EL | 10+70.5 | | 16+50 | 579.5 | | 579.5 | | 579.5 |
| SW RAMP: | | | | | | | | | |
| RT | EL | 10+00 | | 23+31.37 | 1331.4 | 1331.4 | | | 1331.4 |
| LT | EL | 16+62.90 | | 23+28 | 665.1 | | 665.1 | | 665.1 |
| SE RAMP: | | | | | | | | | |
| RT | EL | 10+66 | | 28+53.77 | 1787.8 | 1787.8 | | | 1787.8 |
| LT | EL | 10+66 | | 17+79.89 | 713.9 | | 713.9 | | 713.9 |
| TOTAL = | | | | | | 24072.9 | 21001.5 | 780.7 | 45855.0 |
| ROUND TO: | | | | | | | 45,860.0 | | 45860.0 |

SCHEDULE OF QUANTITIES

PAVEMENT MARKING (SHEET 2 OF 2)

MODIFIED URETHANE PAVEMENT MARKING, 6"

| DIRECTION | LOCATION | MODIFIED URETHANE PAVEMENT MARKING LINE 6 78009006 (FOOT) | GROOVING FOR RECESSED PAVEMENT MARKING 7" X7830074 (FOOT) |
|--|-------------------|---|---|
| LEG "A": FOX CR. RD. - HAMILTON RD. INTERSECTION | | | |
| WB | EAST SIDE ISLAND | 80.0 | 80.0 |
| WB | EAST SIDE X-WALKS | 175.0 | 175.0 |
| WB | WEST SIDE ISLAND | 85.0 | 85.0 |
| EB | WEST SIDE ISLAND | 100.0 | 100.0 |
| EB | EAST SIDE ISLAND | 80.0 | 80.0 |
| EB | EAST SIDE X-WALKS | 70.0 | 70.0 |
| | NE RAMP ISLAND: | 30.0 | 30.0 |
| | NW RAMP X-WALKS: | 105.0 | 105.0 |
| | SE RAMP ISLAND: | 15.0 | 15.0 |
| | TOTAL = | 740.0 | 740.0 |
| | ROUND TO: | 740.0 | 740.0 |

MODIFIED URETHANE PAVEMENT MARKING, 12"

| DIRECTION | LOCATION | MODIFIED URETHANE PAVEMENT MARKING LINE 12 78009012 (FOOT) | GROOVING FOR RECESSED PAVEMENT MARKING 13" X7830078 (FOOT) |
|--|-------------------|--|--|
| LEG "A": FOX CR. RD. - HAMILTON RD. INTERSECTION | | | |
| WB | EAST SIDE ISLAND | 40.0 | 40.0 |
| WB | EAST SIDE X-WALKS | 305.0 | 305.0 |
| WB | WEST SIDE ISLAND | 65.0 | 65.0 |
| EB | WEST SIDE ISLAND | 40.0 | 40.0 |
| EB | EAST SIDE ISLAND | 55.0 | 55.0 |
| EB | EAST SIDE X-WALKS | 150.0 | 150.0 |
| LT | NW RAMP X-WALKS: | 105.0 | 105.0 |
| | TOTAL = | 760.0 | 760.0 |
| | ROUND TO: | 760.0 | 760.0 |

MODIFIED URETHANE PAVEMENT MARKING, 8"

| DIRECTION | STATION | TO | STATION | LENGTH (FEET) | MODIFIED URETHANE PAVEMENT MARKING LINE 8 78009008 (FOOT) | GROOVING FOR RECESSED PAVEMENT MARKING 9" X7830076 (FOOT) |
|-----------|---------|----|-----------|---------------|---|---|
| LEG "C": | | | | | | |
| WB | 38+14 | | 41+65 | 351.0 | 351.0 | 351.0 |
| WB | 53+35 | | 57+80 | 445.0 | 445.0 | 445.0 |
| EB | 36+67 | | 40+59 | 392.0 | 392.0 | 392.0 |
| EB | 55+23 | | 59+55 | 432.0 | 432.0 | 432.0 |
| NE RAMP: | | | | | | |
| LT | 12+63 | | 17+08 | 445.0 | 445.0 | 445.0 |
| NW RAMP: | | | | | | |
| LT | 16+50 | | 20+00 | 350.0 | 350.0 | 350.0 |
| SW RAMP: | | | | | | |
| LT | 12+73 | | 16+62.9 | 389.9 | 389.9 | 389.9 |
| SE RAMP: | | | | | | |
| LT | 17+80 | | 22+07 | 427.0 | 427.0 | 427.0 |
| | | | TOTAL = | | 3231.9 | 3231.9 |
| | | | ROUND TO: | | 3235.0 | 3235.0 |

MODIFIED URETHANE PAVEMENT MARKING, 24"

| DIRECTION | STATION | MODIFIED URETHANE PAVEMENT MARKING LINE 24 78009024 (FOOT) | GROOVING FOR RECESSED PAVEMENT MARKING 25" X7830090 (FOOT) |
|-----------|-----------|--|--|
| LEG "A": | | | |
| WB | 19+06.75 | 55.0 | 55.0 |
| EB | 20+29 | 45.0 | 45.0 |
| | TOTAL = | 100.0 | 100.0 |
| | ROUND TO: | 100.0 | 100.0 |

PREFORMED PLASTIC PAVEVENT MARKING, TYPE B - INLAID - 4"

| DIRECTION | DESCRIPTION | STATION | TO | STATION | LENGTH (FEET) | PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 4 78004210 (FOOT) C.L. | RAISED REFLECTIVE PAVEMENT MARKERS 78100100 (EACH) |
|------------------------------------|---------------|-----------|----|-----------|---------------|---|--|
| LEG "A" CL: | | | | | | | |
| WB | CL | 29+70 | | 20+26 | 944.0 | 236.0 | 24.0 |
| WB | CL | 19+05 | | 18+75 | 30.0 | 7.5 | 2.0 |
| WB | CL | 335+55.87 | | 366+35.33 | 3079.46 | 769.9 | 78.0 |
| EB | CL | 43+70 | | 33+30 | 1040.0 | | 26.0 |
| EB | CL | 33+30 | | 20+31 | 1299.0 | 324.8 | 34.0 |
| FOX CR.-HAMILTON RD. INTERSECTION: | | | | | | | |
| EB | CL | 19+13 | | 18+75 | 38.0 | 9.5 | 2.0 |
| EB | CL | 335+55.87 | | 366+35.20 | 3079.33 | 769.8 | 78.0 |
| LEG "C" CL: | | | | | | | |
| WB | CL | 32+32.06 | | 47+06.97 | 1474.91 | 368.7 | 38.0 |
| WB | CL | 49+10.79 | | 72+70.53 | 2359.74 | 589.9 | 60.0 |
| WB | CL | 75+88.67 | | 87+41.66 | 1152.99 | 288.2 | 30.0 |
| WB | CL | 87+45.84 | | 89+41.79 | 195.95 | 49.0 | 6.0 |
| WB | CL | 89+41.79 | | 99+81.79 | 1040.0 | | 26.0 |
| EB | CL | 32+26.74 | | 47+06.97 | 1480.23 | 370.1 | 38.0 |
| EB | CL | 49+10.79 | | 72+89.02 | 2378.23 | 594.6 | 60.0 |
| EB | CL | 76+05.90 | | 87+41.66 | 1135.76 | 283.9 | 30.0 |
| EB | CL | 87+45.84 | | 89+40.28 | 194.44 | 48.6 | 6.0 |
| LEG "A" TURN LANES: | | | | | | | |
| WB | LT. TURN | 19+05 | | 18+75 | 30 | | 1.0 |
| WB | LT. TURN | 335+55.87 | | 339+40.7 | 384.83 | | 10.0 |
| WB | RT. TURN | 19+05 | | 18+75 | 30 | | 1.0 |
| WB | RT. TURN | 335+55.87 | | 339+40.7 | 384.83 | | 10.0 |
| EB | LT. TURN | 24+55 | | 20+31 | 424 | | 11.0 |
| EB | RT. TURN | 24+55 | | 20+31 | 424 | | 11.0 |
| RAMP GORE AREAS: | | | | | | | |
| NE RAMP: | | | | | | | |
| LT | 8" LINE | 12+63 | | 17+08 | 445.0 | | 12.0 |
| SW RAMP: | | | | | | | |
| LT | 8" LINE | 12+73 | | 16+62.9 | 389.9 | | 10.0 |
| LEG "C" MAINLINE GORE AREAS | | | | | | | |
| WB | 8" LINE | 53+35 | | 57+80 | 445.0 | | 12.0 |
| EB | 8" LINE | 36+67 | | 40+59 | 392.0 | | 10.0 |
| SW RAMP RRPMS @ 20' ctrs. | | | | | | | |
| RT | EL | 10+00 | | 16+62.9 | 662.9 | | 34.0 |
| SW RAMP RRPMS @ 40' ctrs. | | | | | | | |
| LT | EL | 16+62.9 | | 18+20.17 | 157.27 | | 4.0 |
| NE RAMP RRPMS @ 20' ctrs. | | | | | | | |
| RT | EL | 10+00 | | 17+08 | 708.0 | | 36.0 |
| NE RAMP RRPMS @ 40' ctrs. | | | | | | | |
| LT | EL | 17+08 | | 18+79 | 171.0 | | 5.0 |
| NE RAMP LANE LINES | | | | | | | |
| LT. | OUT. LT. TURN | 19+08 | | 22+00 | 292.0 | | 8.0 |
| LT | IN. LT. TURN | 17+20 | | 21+86 | 466.0 | | 12.0 |
| | | | | TOTAL = | | 4,710.5 | 725.0 |
| | | | | ROUND TO: | | 4,720.0 | 730.0 |

SCHEDULE OF QUANTITIES

GUARDRAIL

| STATION | TO | STATION | DIRECTION | STEEL PLATE BEAM GUARDRAIL TYPE A 6 FT POSTS 63000001 (FOOT) | STEEL PLATE BEAM GUARDRAIL TYPE A 9 FT POSTS 63000003 (FOOT) | STEEL PLATE BEAM GUARDRAIL TYPE B 9 FT POSTS 63000009 (FOOT) | STEEL PLATE BEAM GUARDRAIL ATTACH TO STRUCTURE 63000025 (FOOT) | TRAFFIC BARRIER TERMINAL TYPE 2 63100045 (EACH) | TRAFFIC BARRIER TERMINAL TYPE 1 SPECIAL TANGENT 63100167 (EACH) | TRAFFIC BARRIER TERMINAL TYPE 1 SPECIAL FLARED 63100169 (EACH) | GUARDRAIL REMOVAL 63200310 (FOOT) | GUARDRAIL MARKER TYPE A 78200410 (EACH) | BARRIER WALL MARKER TYPE C 78200530 (EACH) | TERMINAL MARKER DIRECT APPLIED 78201000 (EACH) | TRAFFIC BARRIER TERMINAL TYPE 5 SPECIAL X6310179 (EACH) | TRAFFIC BARRIER TERMINAL TYPE 6 SPECIAL X6310214 (EACH) |
|--------------|----|--------------|-----------|---|---|---|---|--|---|--|--|---|--|--|---|---|
| SW RAMP: | | | | | | | | | | | | | | | | |
| 19+25 | | 20+52 | LT. | | | | | | | | 127.0 | | | | | |
| 19+25 | | 20+52 | RT. | | | | | | | | 127.0 | | | | | |
| 17+75.50 | | 18+25.50 | LT. | | | | | | 1.0 | | | | | 1.0 | | |
| 18+25.50 | | 20+38 | LT. | 212.5 | | | | | | | | 4.0 | | | | |
| 20+38 | | 20+52.27 | LT. | | | | | 1.0 | | | | | | | | |
| 18+42 | | 18+92 | RT. | | | | | | 1.0 | | | | | 1.0 | | |
| 18+92 | | 20+42 | RT. | 150.0 | | | | | | | | 4.0 | | | | |
| 20+42 | | 20+56.27 | RT. | | | | | 1.0 | | | | | | | | |
| LEG "C": | | | | | | | | | | | | | | | | |
| 41+26.17 | | 41+76.17 | EB | | | | | | 1.0 | | | | | 1.0 | | |
| 41+76.17 | | 46+63.67 | EB | | 487.5 | | | | | | | 5.0 | | | | |
| 46+63.67 | | 47+09.32 | EB | | | | | | | | | | | | | 1.0 |
| 49+08.24 | | 49+53.89 | WB | | | | | | | | | | | | | 1.0 |
| 49+53.89 | | 52+78.89 | WB | | 325.0 | | | | | | | 5.0 | | | | |
| 52+78.89 | | 53+28.89 | WB | | | | | | 1.0 | | | | | 1.0 | | |
| 63+00.16 | | 63+50.16 | EB | | | | | | 1.0 | | | | | 1.0 | | |
| 63+50.16 | | 63+87.66 | EB | | | 37.50 | | | | | | 1.0 | | | | |
| 63+87.66 | | 64+00.16 | EB | | | | 12.5 | | | | | | | | | |
| 63+91.29 | | 64+03.79 | WB | | | | 12.5 | | | | | | | | | |
| 64+00.16 | | 72+62.66 | EB | | | 862.50 | | | | | | 5.0 | | | | |
| 62+91.29 | | 63+91.29 | WB | | | 100.00 | | | | | | | | | | |
| 64+03.79 | | 72+53.79 | WB | | | 850.00 | | | | | | | | | | |
| 72+62.66 | | 73+08.31 | EB | | | | | | | | | | | | | 1.0 |
| 75+00.16 | | | EB | | | | | | | | | | 1.0 | | | |
| 62+77.02 | | 62+91.29 | WB | | | | | 1.0 | | | | | | | | |
| 62+91.29 | | 72+53.79 | WB | | | | | | | | | 2.0 | | | | |
| 73+26.60 | | | WB | | | | | | | | | | 1.0 | | | |
| 72+53.79 | | 72+68.46 | WB | | | | | | | | | | | | 1.0 | |
| 76+08.30 | | 76+22.97 | EB | | | | | | | | | | | | 1.0 | |
| 76+22.97 | | 87+41.66 BK. | EB | | | 1118.69 | | | | | | | | | | |
| 87+45.84 AH. | | 88+02.15 | EB | | | 56.31 | | | | | | 3.0 | | | | |
| 88+02.15 | | 88+16.42 | EB | | | | | 1.0 | | | | | | | | |
| 75+68.45 | | 76+14.10 | WB | | | | | | | | | | | | | 1.0 |
| 76+14.10 | | 87+41.66 BK. | WB | | | 1127.56 | | | | | | 7.0 | | | | |
| 88+45.84 AH. | | 88+80.78 | WB | | | 134.94 | | | | | | | | | | |
| 88+80.78 | | 89+30.65 | WB | | | | | | | 1.0 | | | | 1.0 | | |
| 41+56 | | 47+27 | EB | | | | | | | | 571 | | | | | |
| 48+90 | | 52+43 | WB | | | | | | | | 353 | | | | | |
| 63+10 | | 73+20 | EB | | | | | | | | 1010 | | | | | |
| 63+80 | | 72+70 | WB | | | | | | | | 890 | | | | | |
| 75+68 | | 87+41.66 BK. | WB | | | | | | | | 1173.66 | | | | | |
| 87+45.84 AH. | | 89+41.39 | WB | | | | | | | | 195.55 | | | | | |
| 76+10 | | 87+41.66 BK. | EB | | | | | | | | 1131.66 | | | | | |
| 87+45.84 AH. | | 89+42.39 | EB | | | | | | | | 196.55 | | | | | |
| TOTAL : | | | | 362.5 | 812.5 | 4287.50 | 25.0 | 4.0 | 5.0 | 1.0 | 5,775.4 | 36.0 | 2.0 | 6.0 | 2.0 | 4.0 |
| ROUND TO: | | | | 362.5 | 812.5 | 4287.50 | 25.0 | 4.0 | 5.0 | 1.0 | 5,780.0 | 40.0 | 2.0 | 6.0 | 2.0 | 4.0 |

SCHEDULE OF QUANTITIES

AGGREGATE SHOULDERS

| STATION | TO STATION | SHLDR. | LENGTH (FOOT) | WIDTH (FOOT) | VOLUME (CU FT) | AGGREGATE SHOULDERS TYPE B 48101200 (TON) | AGGREGATE SHOULDERS TYPE B 6 48101500 (SQ YD) |
|---------------|------------|----------|---------------|--------------|----------------|---|---|
| LEG "A": | | | | | | | |
| 33+30 | 20+70 | EB DR. | 1260 | 2 | | | 280.0 |
| 33+30 | 24+55.61 | EB PASS. | 874.39 | 3 | | | 291.5 |
| 29+70 | 28+50 | WB DR. | 120 | 2 | | | 26.7 |
| 29+70 | 29+00 | WB PASS. | 70 | 3 | | | 23.3 |
| 28+50 | 28+00 | WB DR. | 50.0 | 2 | 20.0 | 1.3 | |
| 28+00 | 27+50 | WB DR. | 50.0 | 2 | | | 11.1 |
| 27+50 | 26+00 | WB DR. | 150.0 | 2 | 60.0 | 4.0 | |
| 26+00 | 21+06 | WB DR. | 494.0 | 2 | | | 109.8 |
| 29+00 | 26+00 | WB PASS. | 300.0 | 3 | 177.0 | 11.8 | |
| 26+00 | 24+50 | WB PASS. | 150.0 | 3 | | | 50.0 |
| 24+50 | 22+30.61 | WB PASS. | 219.39 | 3 | 129.4 | 8.6 | |
| 18+95 | 18+75 BK. | WB DR. | 20 | 2 | | | 4.4 |
| 335+55.87 AH. | 366+35.33 | WB DR. | 3079.5 | 2 | | | 684.3 |
| 335+84 | 366+35.20 | EB DR. | 3051.2 | 2 | | | 678.0 |
| 337+82.39 | 338+50 | EB PASS. | 67.61 | 3 | 39.9 | 2.7 | |
| 338+50 | 360+50 | EB PASS. | 2200 | 3 | | | 733.3 |
| 360+50 | 366+35.20 | EB PASS. | 585.2 | 3 | 345.3 | 23.0 | |
| 339+40.83 | 350+50 | WB PASS. | 1109.2 | 3 | | | 369.7 |
| 350+50 | 360+50 | WB PASS. | 1000.0 | 3 | 590.0 | 39.3 | |
| 360+50 | 362+00 | WB PASS. | 150.0 | 3 | | | 50.0 |
| 362+00 | 365+00 | WB PASS. | 300.0 | 3 | 177.0 | 11.8 | |
| 365+00 | 366+35.33 | WB PASS. | 135.33 | 3 | | | 45.1 |
| LEG "C": | | | | | | | |
| 32+32.06 | 32+89 | WB DR. | 56.94 | 2 | | | 12.7 |
| 32+26.74 | 35+67 | EB DR. | 340.26 | 2 | 136.1 | 9.1 | |
| 40+58.47 | 43+40 | EB DR. | 281.53 | 2 | | | 62.6 |
| 41+67.8 | 43+93.3 | WB DR. | 225.5 | 2 | | | 50.1 |
| 52+36 | 55+23 | EB DR. | 287.0 | 2 | 114.8 | 7.7 | |
| 52+91.78 | 53+34.64 | WB DR. | 42.9 | 2 | | | 9.5 |
| SW RAMP: | | | | | | | |
| 16+62.86 | 23+20 | LT. | 657.14 | 2 | | | 146.0 |
| 16+23.91 | 22+43 | RT. | 619.09 | 2 | | | 137.6 |
| NW RAMP: | | | | | | | |
| 11+29 | 15+26 | LT. | 397 | 2 | | | 88.2 |
| SE RAMP: | | | | | | | |
| 10+95 | 17+55 | LT. | 660 | 2 | | | 146.7 |
| 11+39 | 18+00 | RT. | 661 | 2 | | | 146.9 |
| 18+00 | 25+28.81 | RT. | 728.81 | 2 | 335.3 | 22.4 | |
| | | | | | | TOTAL = | 4157.6 |
| | | | | | | ROUND TO: | 4160.0 |

CHANGEABLE MESSAGE SIGN

| LOCATION | DIRECTION | CHANGEABLE MESSAGE SIGN X7015005 (CAL DA) |
|--------------------------------|-----------|---|
| *I- 55 BUSN. | EB | 14 |
| *I- 55 BUSN. | WB | 14 |
| *NW RAMP | WB | 14 |
| *SE RAMP | EB | 14 |
| *I- 74 OFF-RAMP TO I- 55 BUSN. | EB | 14 |
| TOTAL = | | 70 |

* R.E. TO LOCATE SIGN IN FIELD

SURVEY MARKERS

| DESCRIPTION | STATION | OFFSET | SURVEY MARKER TYPE 2 (SPECIAL) XZ193400 (EACH) |
|---------------|-----------------------------|-----------|--|
| LEG "A": | | | |
| P.C. | 20+00 | CL | 1 |
| LEG "C": | | | |
| P.C. | 33+00.01 | CL | 1 |
| P.I. | 38+00.01 | 1.44' RT. | 1 |
| P.T. | 43+00.00 | CL | 1 |
| STA. EQUATION | 87+41.66 BK. = 87+45.84 AH. | CL | 1 |
| TOTAL = | | | 5 |

TREE REMOVAL

| LOCATION: | STA. | OFFSET | TREE REMOVAL 6 TO 15 UNITS 20100110 (UNIT) | TREE REMOVAL OVER 15 UNITS 20100210 (UNIT) |
|-----------|----------|----------|--|--|
| MAINLINE | 64+80.28 | 47.3' LT | 8 | |
| MAINLINE | 65+05.68 | 49.0' LT | 8 | |
| MAINLINE | 67+21.12 | 50.8' RT | 9 | |
| MAINLINE | 67+24.46 | 44.6' RT | 10 | |
| NW RAMP | 10+80 | 58' RT | | 25.0 |
| TOTAL = | | | 35.0 | 25.0 |

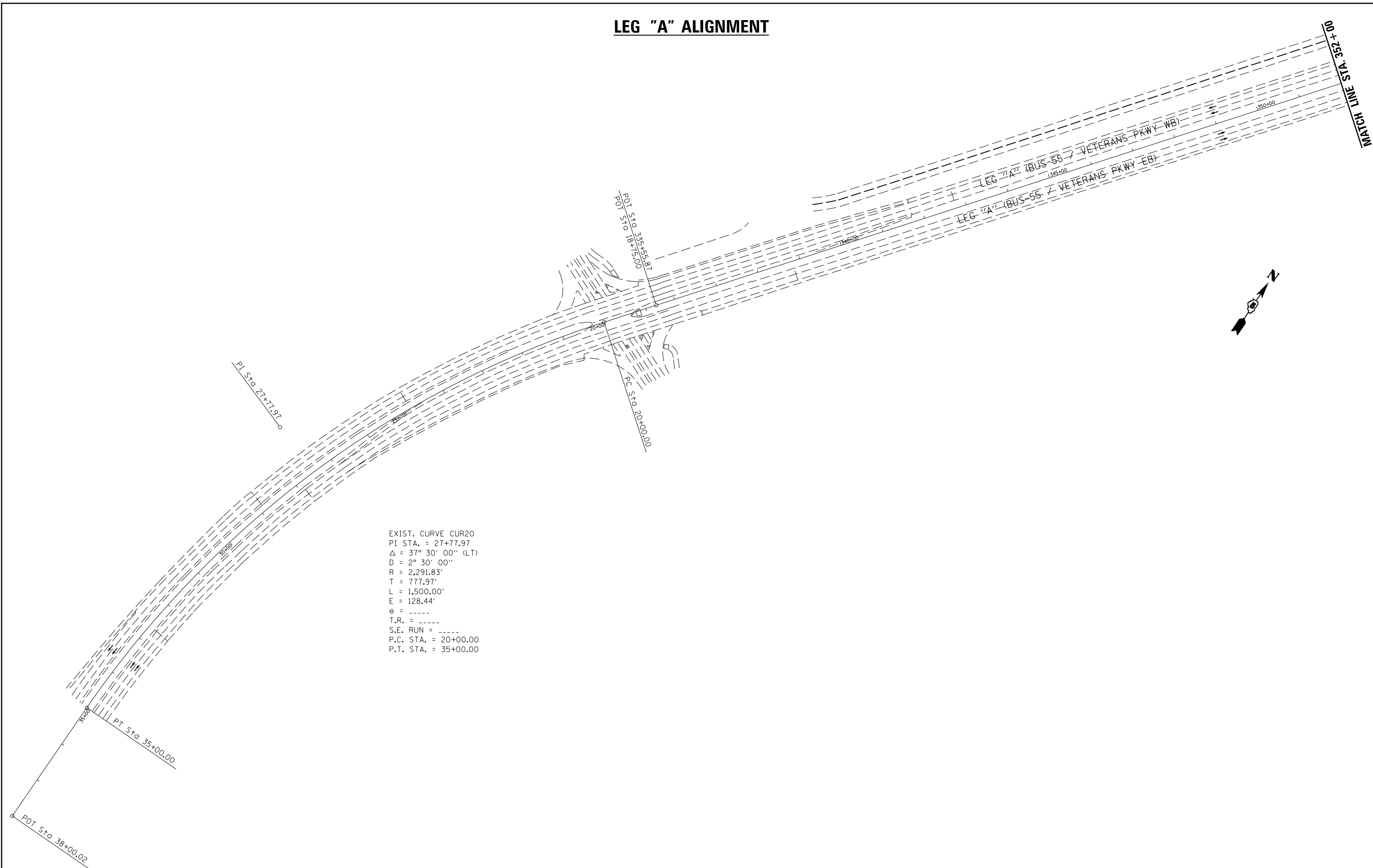
PARTIAL DEPTH PATCHING

| | (SQ YD) | PARTIAL DEPTH PATCH X4421000 (TON) |
|-----------|---------|------------------------------------|
| 6" PATCH | 680.0 | 228.5 |
| 7" PATCH | 590.0 | 231.3 |
| 8" PATCH | 440.0 | 197.1 |
| 9" PATCH | 190.0 | 95.8 |
| 10" PATCH | 2,200.0 | 1,232.0 |
| 11" PATCH | 1,190.0 | 733.0 |
| TOTAL | | 2,717.7 |
| ROUND TO | | 2,720.0 |

PAVEMENT PATCHING SCHEDULE

| | CL D PATCH T II 9" 44201753 (SQ YD) | PART DEP REM T I 6" X4406060 (SQ YD) | PART DEP REM T I 7" X4406070 (SQ YD) | PART DEP REM T I 8" X4406080 (SQ YD) | PART DEP REM T I 9" X4406090 (SQ YD) | PART DEP REM T I 10" X4406100 (SQ YD) | PART DEP REM T I 11" X4406110 (SQ YD) | PART DEP REM T II 6" X4406260 (SQ YD) | PART DEP REM T II 7" X4406270 (SQ YD) | PART DEP REM T II 8" X4406280 (SQ YD) | PART DEP REM T II 9" X4406290 (SQ YD) | PART DEP REM T II 10" X4406300 (SQ YD) | PART DEP REM T II 11" X4406310 (SQ YD) | PART DEP REM T III 10" X4406500 (SQ YD) | PART DEP REM T III 11" X4406510 (SQ YD) |
|------------|-------------------------------------|--------------------------------------|--------------------------------------|--------------------------------------|--------------------------------------|---------------------------------------|---------------------------------------|---------------------------------------|---------------------------------------|---------------------------------------|---------------------------------------|--|--|---|---|
| LOCATION: | | | | | | | | | | | | | | | |
| SW RAMP | 234.7 | | | | | | | | | | | | | | |
| SE RAMP | 288.0 | | | | | | | | | | | | | | |
| NE RAMP | 245.3 | | | | | | | | | | | | | | |
| NW RAMP | 224.0 | | | | | | | | | | | | | | |
| LEG "A" EB | | 26.7 | 46.1 | 6.6 | | 10.4 | | 307.4 | 530.2 | 75.5 | | 119.6 | | | |
| LEG "A" WB | | 14.9 | | 27.5 | | 42.9 | | 170.3 | | 317.3 | | 493.7 | | | |
| LEG "C" EB | | 5.9 | | | 7.4 | 52.4 | 38.1 | 68.6 | | | 84.4 | 676.9 | 513.0 | 50.0 | 50.0 |
| LEG "C" WB | | 5.8 | | | 7.2 | 50.0 | 36.6 | 66.2 | | | 82.0 | 649.7 | 495.9 | 50.0 | 50.0 |
| TOTAL | 992.0 | 53.3 | 46.1 | 34.1 | 14.6 | 155.7 | 74.7 | 612.5 | 530.2 | 392.8 | 166.4 | 1,939.9 | 1,008.9 | 100.0 | 100.0 |
| ROUND TO | 995.0 | 60.0 | 50.0 | 40.0 | 20.0 | 160.0 | 80.0 | 620.0 | 540.0 | 400.0 | 170.0 | 1,940.0 | 1,010.0 | 100.0 | 100.0 |

LEG "A" ALIGNMENT



EXIST. CURVE CUR20
 PI STA. = 27+77.97
 $\Delta = 37^\circ 30' 00''$ (LT)
 D = 2° 30' 00"
 R = 2,291.83'
 T = 777.97'
 L = 1,500.00'
 E = 128.44'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 20+00.00
 P.T. STA. = 35+00.00

| | | | |
|--|------------------------------|------------|-----------|
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| | PLOT SCALE = 200.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

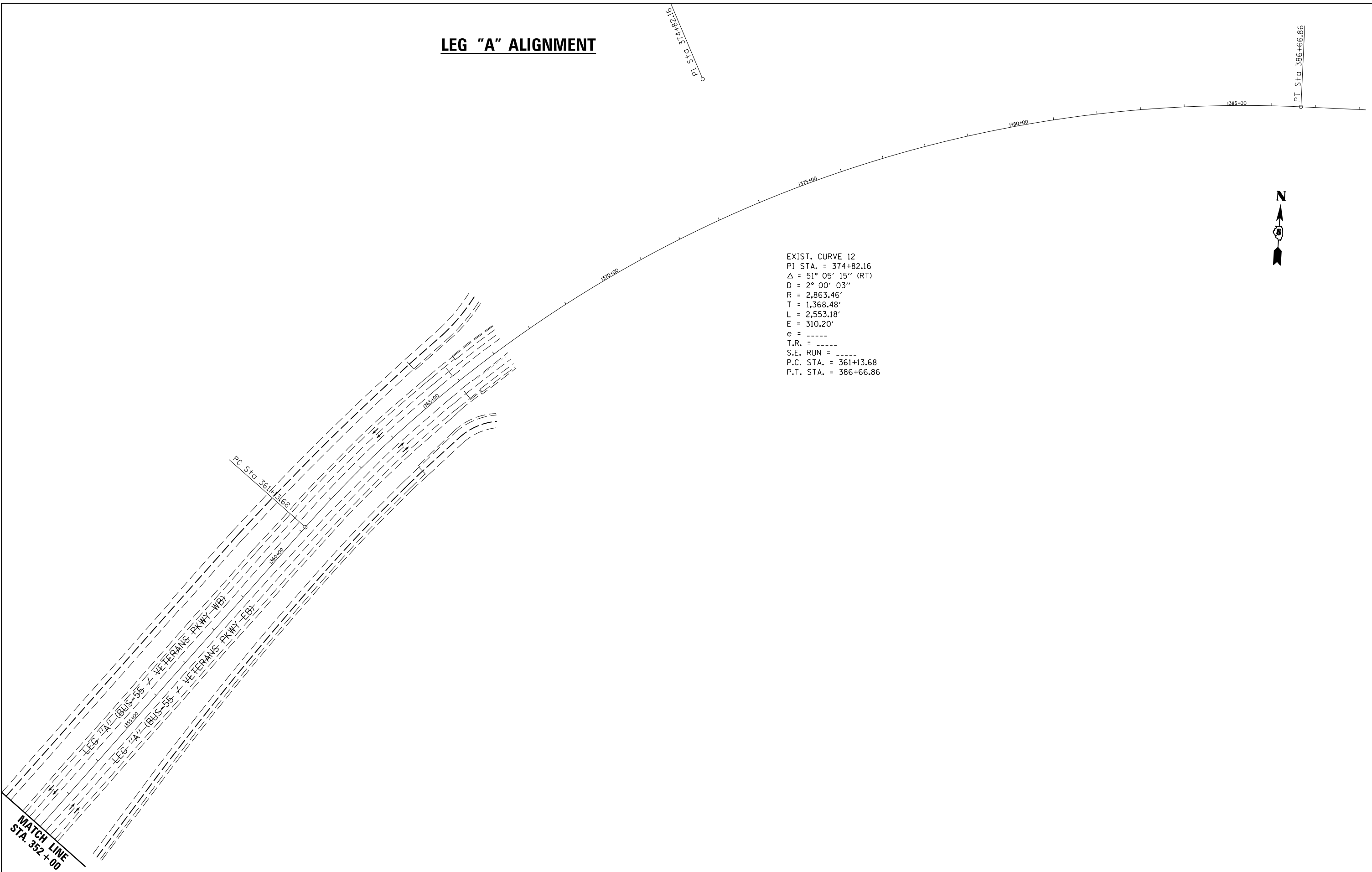
BUS 55 PLAN SHEET - ALIGNMENT

SCALE: 1" = 200' SHEET 1 OF 5 SHEETS STA. 38+00.00 TO STA. 352+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 44 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |

LEG "A" ALIGNMENT

EXIST. CURVE 12
 PI STA. = 374+82.16
 Δ = 51° 05' 15" (RT)
 D = 2° 00' 03"
 R = 2,863.46'
 T = 1,368.48'
 L = 2,553.18'
 E = 310.20'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 361+13.68
 P.T. STA. = 386+66.86



| | | | |
|---|------------------------------|------------|-----------|
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| | PLOT SCALE = 200.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

BUS 55 PLAN SHEET - ALIGNMENT

SCALE: 1" = 200' SHEET 2 OF 5 SHEETS STA. 352+00.00 TO STA. 388+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------|--------|---------------------------|-----------|
| 704 | (57-7RS-1&1RS-3 | McLean | 273 | 45 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |

LEG "C" ALIGNMENT



EXIST. CURVE 2
 PI STA. = 16+44.97
 $\Delta = 18^\circ 07' 37''$ (RT)
 $D = 8^\circ 06' 49''$
 $R = 706.17'$
 $T = 112.65'$
 $L = 223.41'$
 $E = 8.93'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 15+32.33$
 $P.T. STA. = 17+55.74$

EXIST. CURVE 206
 PI STA. = 17+71.79
 $\Delta = 18^\circ 01' 40''$ (RT)
 $D = 8^\circ 18' 31''$
 $R = 689.58'$
 $T = 109.39'$
 $L = 216.97'$
 $E = 8.62'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 16+62.40$
 $P.T. STA. = 18+79.37$

RAMP STA. 27+01.87 =
 MAINLINE STA. 31+11.93
 O/S 27.41' LT

PI Sta 24+44.82

PI Sta 21+12.61

PT Sta 17+55.74

PI Sta 16+44.97

PC Sta 15+32.33

S.N. 057-0063

NW RAMP

NE RAMP

PT Sta 18+79.37

PI Sta 17+71.79

PC Sta 16+62.40

LEG "C" (BUS-55 / VETERANS PKWY WB)

LEG "C" (BUS-55 / VETERANS PKWY EB)

EQUATION:
 Sta 390+96.85 BK =
 Sta 29+93.05 AH

PC Sta 33+00.01

RAMP STA. 10+00.00 =
 MAINLINE STA. 33+94.60
 O/S 27.44' RT

PI Sta 38+00.01

PC Sta 16+66.22

PI Sta 17+43.50

PT Sta 18+20.17

PT Sta 43+00.00

SW RAMP

SE RAMP

PC Sta 17+59.12

PI Sta 18+36.59

PT Sta 19+13.42

EXIST. CURVE 6
 PI STA. = 38+00.01
 $\Delta = 0^\circ 39' 42''$ (LT)
 $D = 0^\circ 03' 58''$
 $R = 86,574.40'$
 $T = 500.00'$
 $L = 999.99'$
 $E = 1.44'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 33+00.01$
 $P.T. STA. = 43+00.00$

EXIST. CURVE 96
 PI STA. = 17+43.50
 $\Delta = 12^\circ 36' 04''$ (RT)
 $D = 8^\circ 11' 07''$
 $R = 699.99'$
 $T = 77.29'$
 $L = 153.95'$
 $E = 4.25'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 16+66.22$
 $P.T. STA. = 18+20.17$

EXIST. CURVE 1
 PI STA. = 18+36.59
 $\Delta = 12^\circ 46' 31''$ (RT)
 $D = 8^\circ 16' 47''$
 $R = 692.01'$
 $T = 77.47'$
 $L = 154.30'$
 $E = 4.32'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 17+59.12$
 $P.T. STA. = 19+13.42$

MATCH LINE STA. 60+00

| | | | |
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| PLOT DATE = 6/11/2014 | | DATE - | REVISED - |

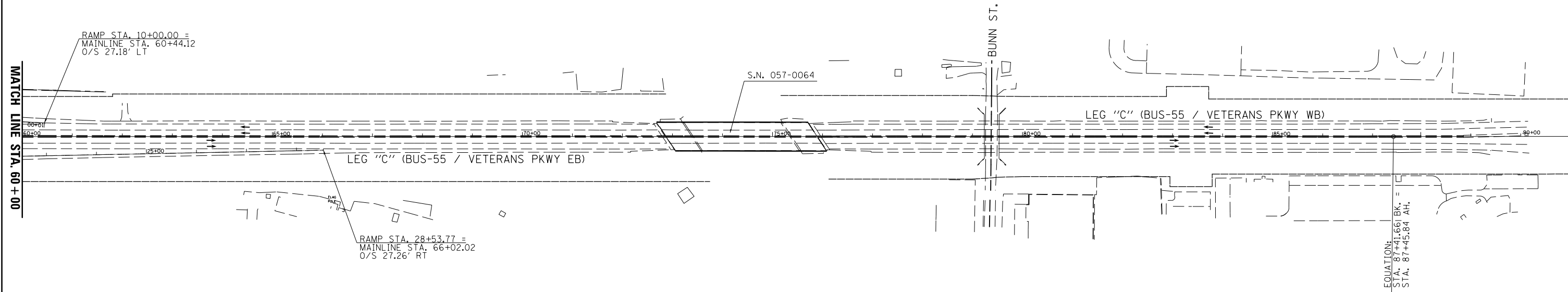
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BUS 55 PLAN SHEET - ALIGNMENT

SCALE: 1" = 200' SHEET 3 OF 5 SHEETS STA. 390+00.00 TO STA. 60+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 46 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |

LEG "C" ALIGNMENT



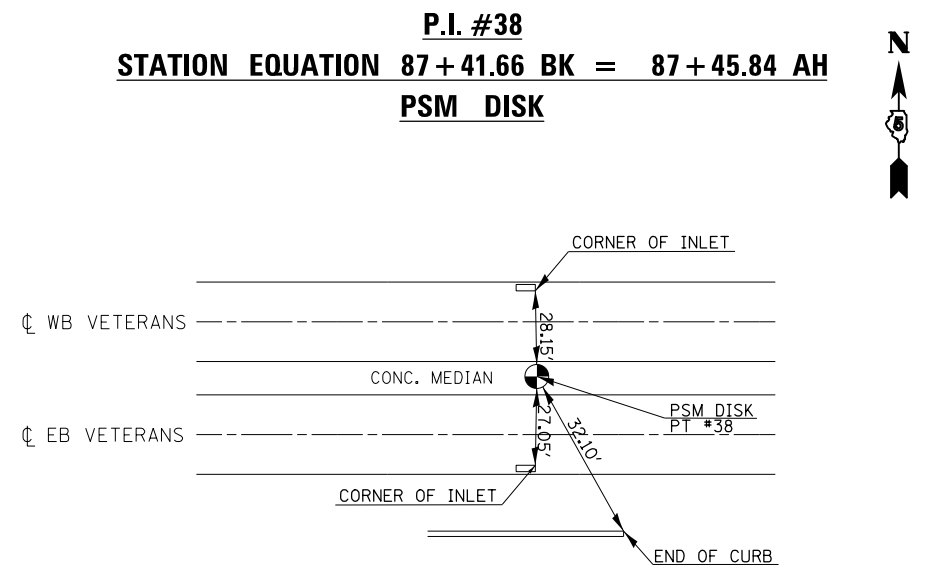
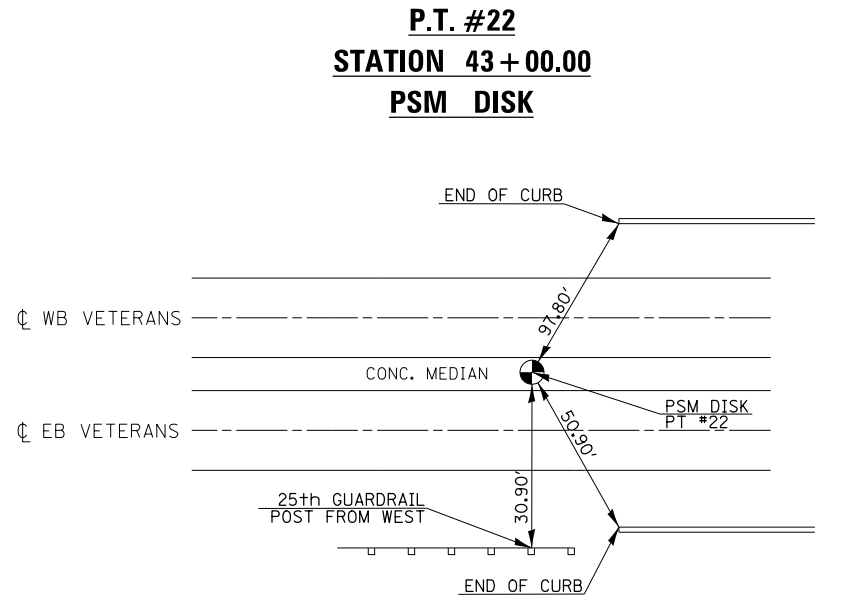
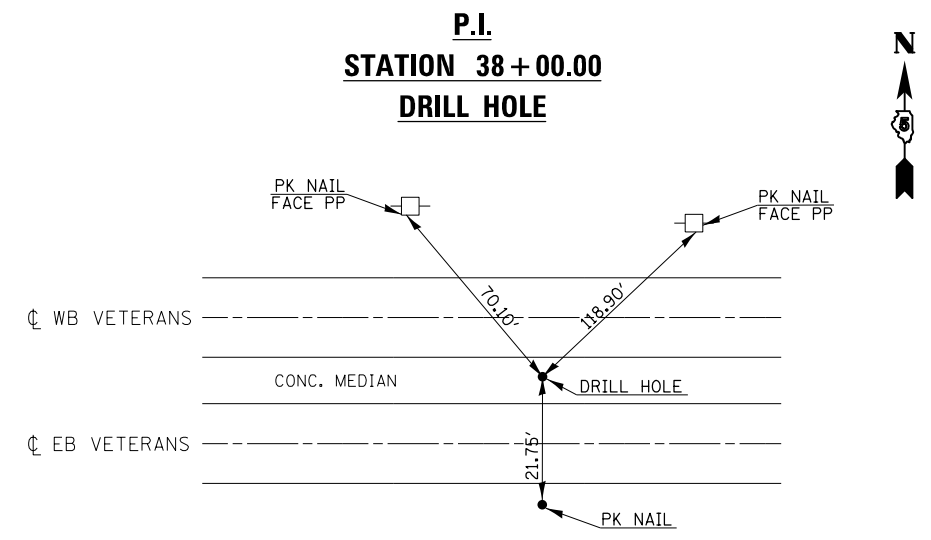
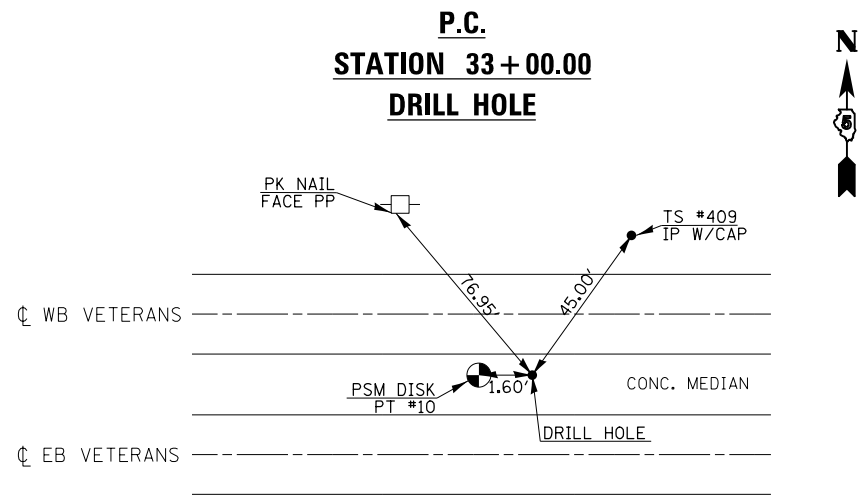
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| | PLOT SCALE = 200.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUS 55 PLAN SHEET - ALIGNMENT

SCALE: 1" = 200' SHEET 4 OF 5 SHEETS STA. 60+00.00 TO STA. 108+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 47 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |

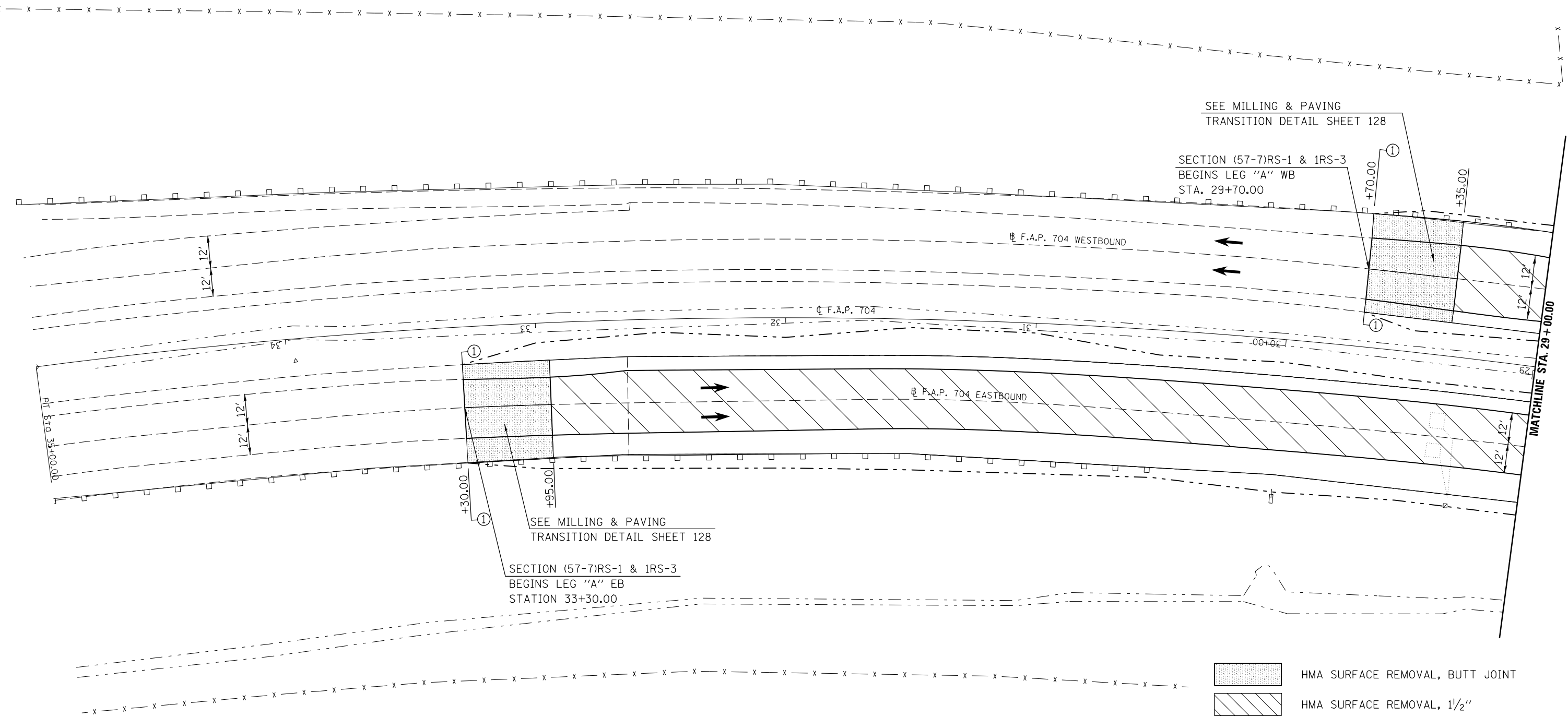


BENCHMARKS

- BM #4920-1 STA. 339+50.46, 66.17' RT
CHISELED SQUARE, TOP OF CENTER OF EAST PIPE CULVERT HEADWALL FROM INTERSECTION OF I-55 BUS AND FOX CREEK RD
GO NORTHEASTERLY ALONG I-55 BUS 0.10 MI TO MARK ON THE RIGHT
ELEV. = 850.49
- BM #4920-3 STA. 32+98.32, 0.15' LT
BRASS DISK IN CONCRETE MEDIAN
FROM INTERSECTION OF I-55 BUS AND FOX CREEK RD
GO NORTHEASTERLY ALONG I-55 BUS 1.10 MI TO MARK ON THE LEFT
ELEV. = 835.63
- BM #4920-4 STA. 49+08.87, 29.59' RT
CHISELED SQUARE, TOP OF PARAPET WALL AT SE CORNER OF BRIDGE FROM INTERSECTION OF I-55 BUS AND FOX CREEK RD
GO NORTHEASTERLY ALONG I-55 BUS 1.40 MI TO MARK ON THE RIGHT
ELEV. = 831.22
- BM #4920-5 STA. 61+68.87, 88.92' LT
CHISELED SQUARE, TOP OF CONCRETE RETAINING WALL AT "THE GREAT ESCAPE"
FROM INTERSECTION OF I-55 BUS AND FOX CREEK RD
GO NORTHEASTERLY ALONG I-55 BUS 1.64 MI TO MARK ON THE LEFT
ELEV. = 844.06

- BM #4920-6 STA. 75+68.82, 28.75' LT
CHISELED SQUARE, TOP OF PARAPET WALL AT NE CORNER OF RR BRIDGE FROM INTERSECTION OF I-55 BUS AND FOX CREEK RD
GO NORTHEASTERLY ALONG I-55 BUS 1.90 MI TO MARK ON THE LEFT
ELEV. = 887.06
- BM #4920-7 STA. 107+44, 90' RT
CHISELED SQUARE, N SIDE OF LIGHT POLE FOUNDATION, NE QUADRANT OF HAYASHI RESTAURANT PARKING LOT FROM INTERSECTION OF I-55 BUS AND FOX CREEK RD
GO NORTHEASTERLY ALONG I-55 BUS 2.20 MI TO MARK ON THE RIGHT
ELEV. = 859.82
- BM #4920-8 STA. 11+00, 13.59' RT (NW RAMP)
CHISELED SQUARE, CENTER OF N HEADWALL OF BOX CULVERT UNDER NW RAMP, STRUCTURE NO. 057-8204
IN NW QUADRANT OF VETERANS PARKWAY AND BUS 51 INTERCHANGE
ELEV. = 808.85
- BM #4920-9 STA. 20+39.13, 49.46' LT (SW RAMP)
CHISELED SQUARE, CENTER OF N HEADWALL OF BOX CULVERT UNDER SW RAMP, STRUCTURE NO. 057-8205
IN SW QUADRANT OF VETERANS PARKWAY AND BUS 51 INTERCHANGE
ELEV. = 814.16

| | | | | | | | | | | | |
|-------------|--|------------|-----------|---|----------------------------------|--------------------|---------------------|---------------------------|--------------|-----------|--|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TIE POINTS AND BENCHMARKS | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| | et:\pwork\pwork\dot\dodgems\d0267571\0570838-sh1-ATB.dgn | DRAWN - | REVISED - | | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 48 | |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | | CONTRACT NO. 70838 | | ILLINOIS FED. AID PROJECT | | | |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - | | | SCALE: | SHEET 5 OF 5 SHEETS | STA. | TO STA. | | |



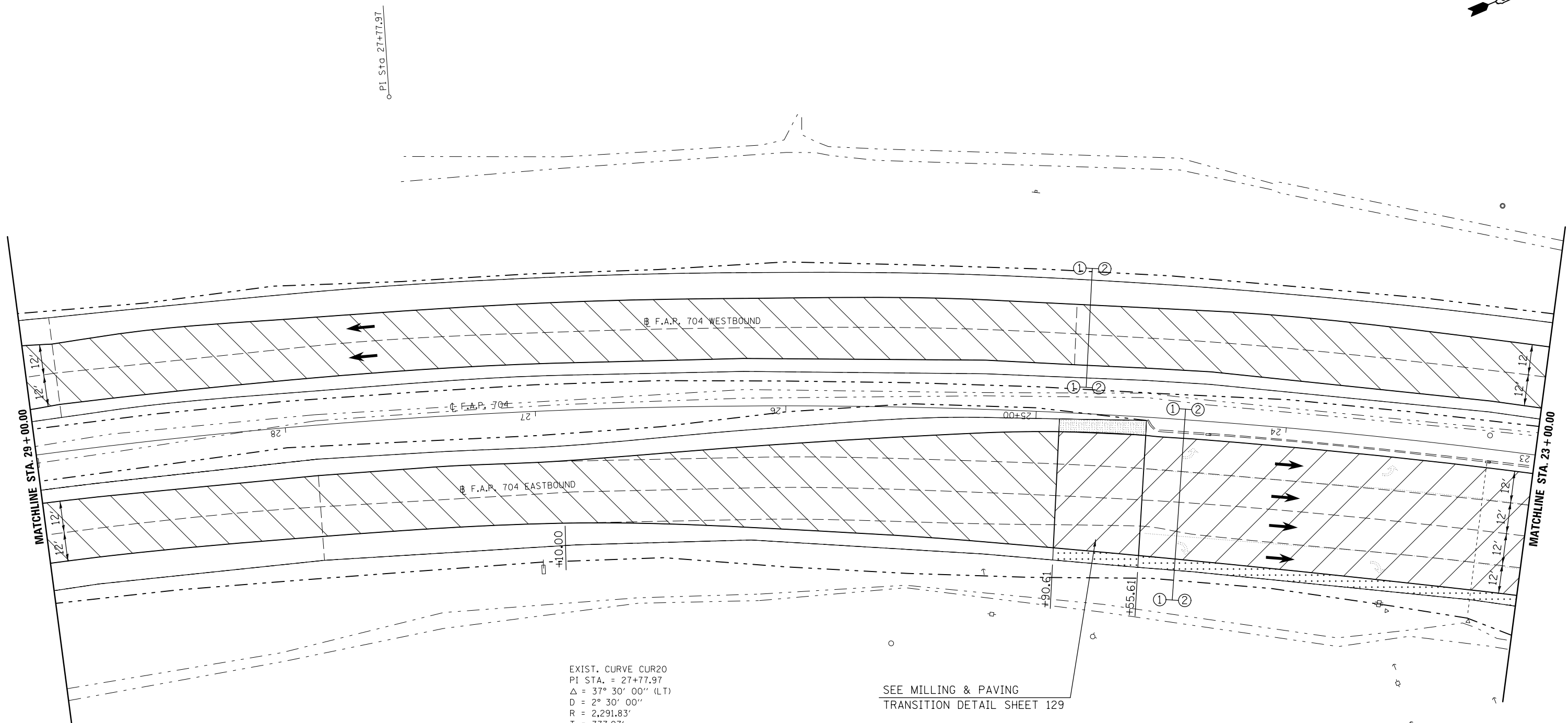
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUS 55 PLAN SHEET

SCALE: 1" = 20' SHEET 1 OF 18 SHEETS STA. 33+30.00 TO STA. 29+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 49 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



EXIST. CURVE CUR20
 PI STA. = 27+77.97
 $\Delta = 37^\circ 30' 00''$ (LT)
 $D = 2^\circ 30' 00''$
 $R = 2,291.83'$
 $T = 777.97'$
 $L = 1,500.00'$
 $E = 128.44'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. \text{ RUN} = \text{-----}$
 $P.C. \text{ STA.} = 20+00.00$
 $P.T. \text{ STA.} = 35+00.00$

SEE MILLING & PAVING
 TRANSITION DETAIL SHEET 129

- HMA SURFACE REMOVAL, BUTT JOINT
- HMA SURFACE REMOVAL, 1 1/2"
- HMA SURFACE REMOVAL, 1 3/4"
- HMA SURFACE REMOVAL, 3 1/4"

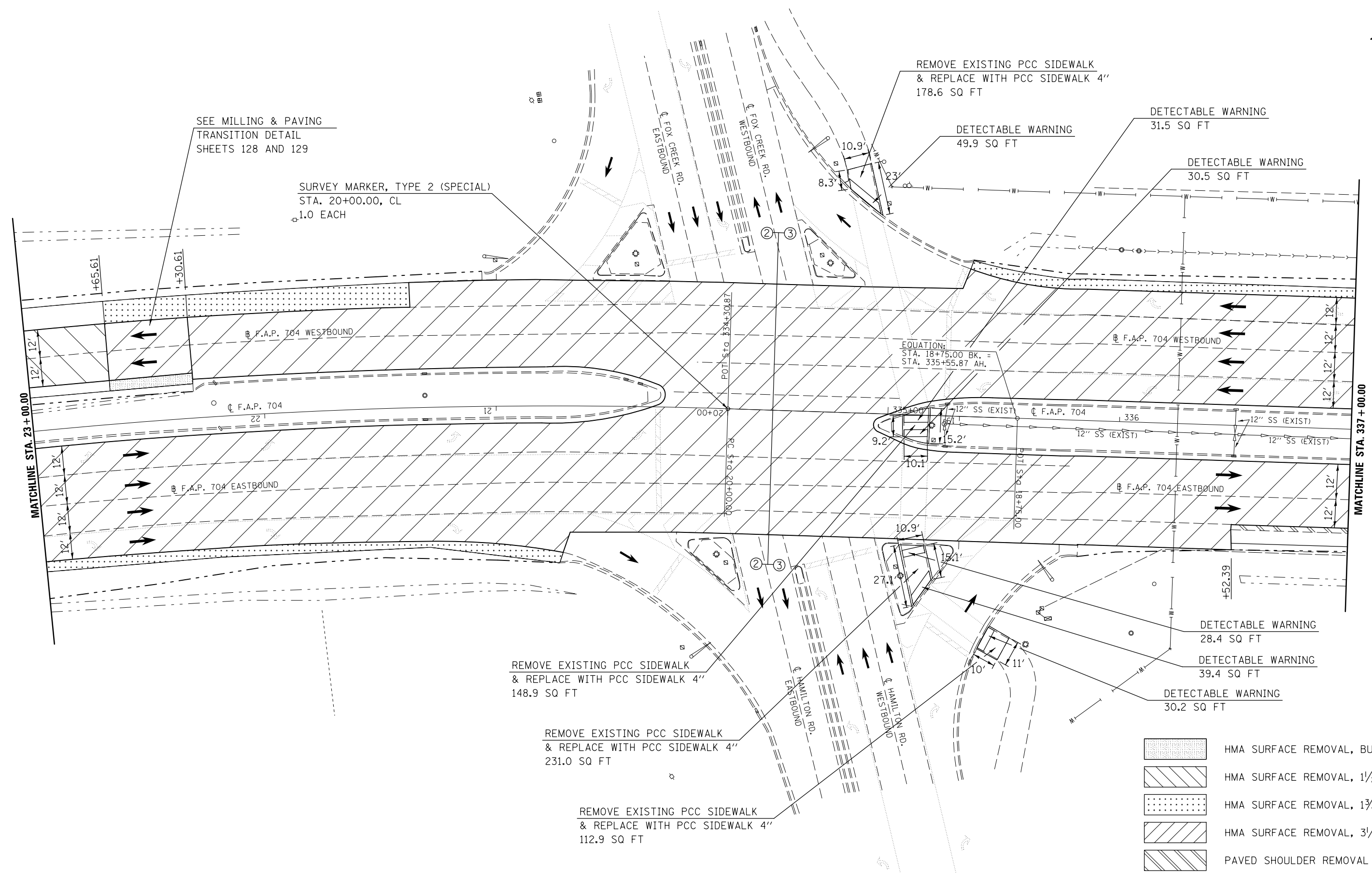
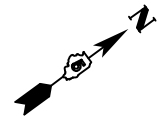
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|--|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - |
| et:\pw\work\p\dot\dodgms\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

BUS 55 PLAN SHEET

SCALE: 1" = 20' SHEET 2 OF 18 SHEETS STA. 29+00.00 TO STA. 23+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 50 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



- HMA SURFACE REMOVAL, BUTT JOINT
- HMA SURFACE REMOVAL, 1 1/2"
- HMA SURFACE REMOVAL, 1 3/4"
- HMA SURFACE REMOVAL, 3 1/4"
- PAVED SHOULDER REMOVAL

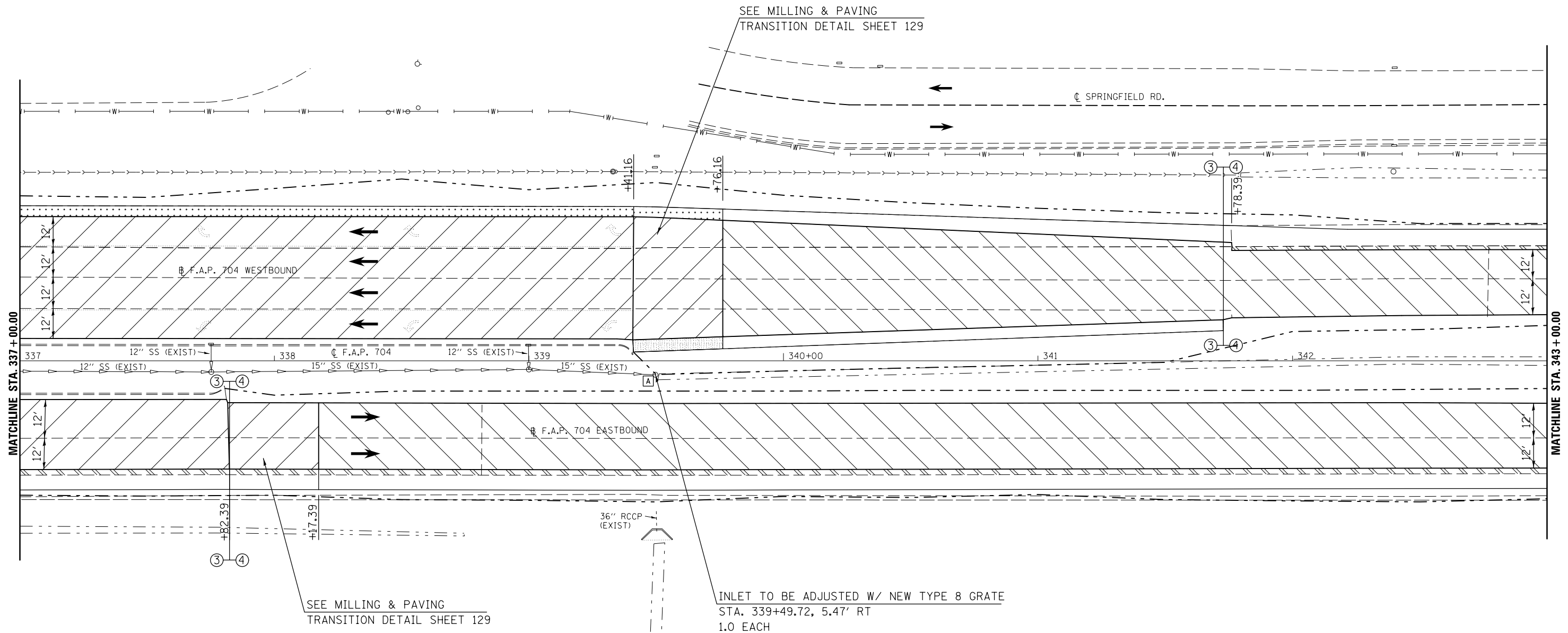
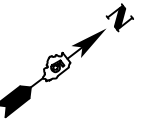
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUS 55 PLAN SHEET

SCALE: 1" = 20' SHEET 3 OF 18 SHEETS STA. 23+00.00 TO STA. 337+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 51 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



- HMA SURFACE REMOVAL, BUTT JOINT
- HMA SURFACE REMOVAL, 1/2"
- HMA SURFACE REMOVAL, 1 3/4"
- HMA SURFACE REMOVAL, 3/4"
- PAVED SHOULDER REMOVAL

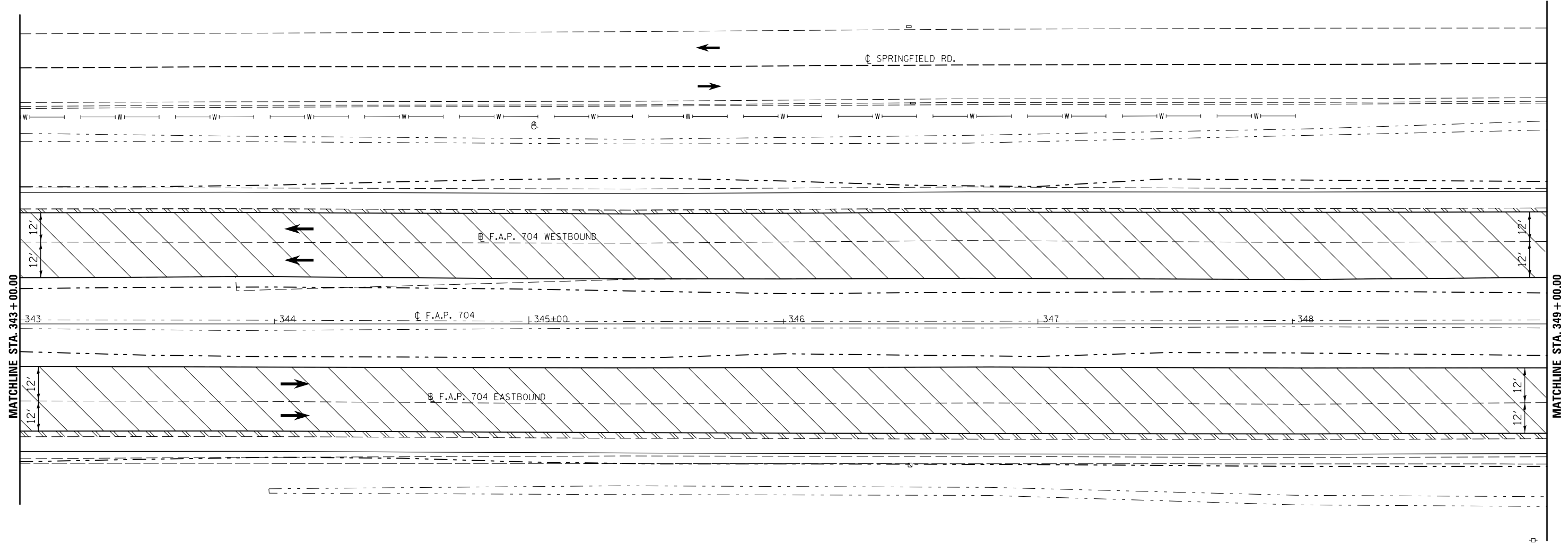
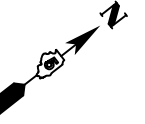
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| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - |
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

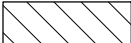
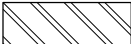
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUS 55 PLAN SHEET

SCALE: 1" = 20' SHEET 4 OF 18 SHEETS STA. 337+00.00 TO STA. 343+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 52 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



 HMA SURFACE REMOVAL, 1 1/2"
 PAVED SHOULDER REMOVAL

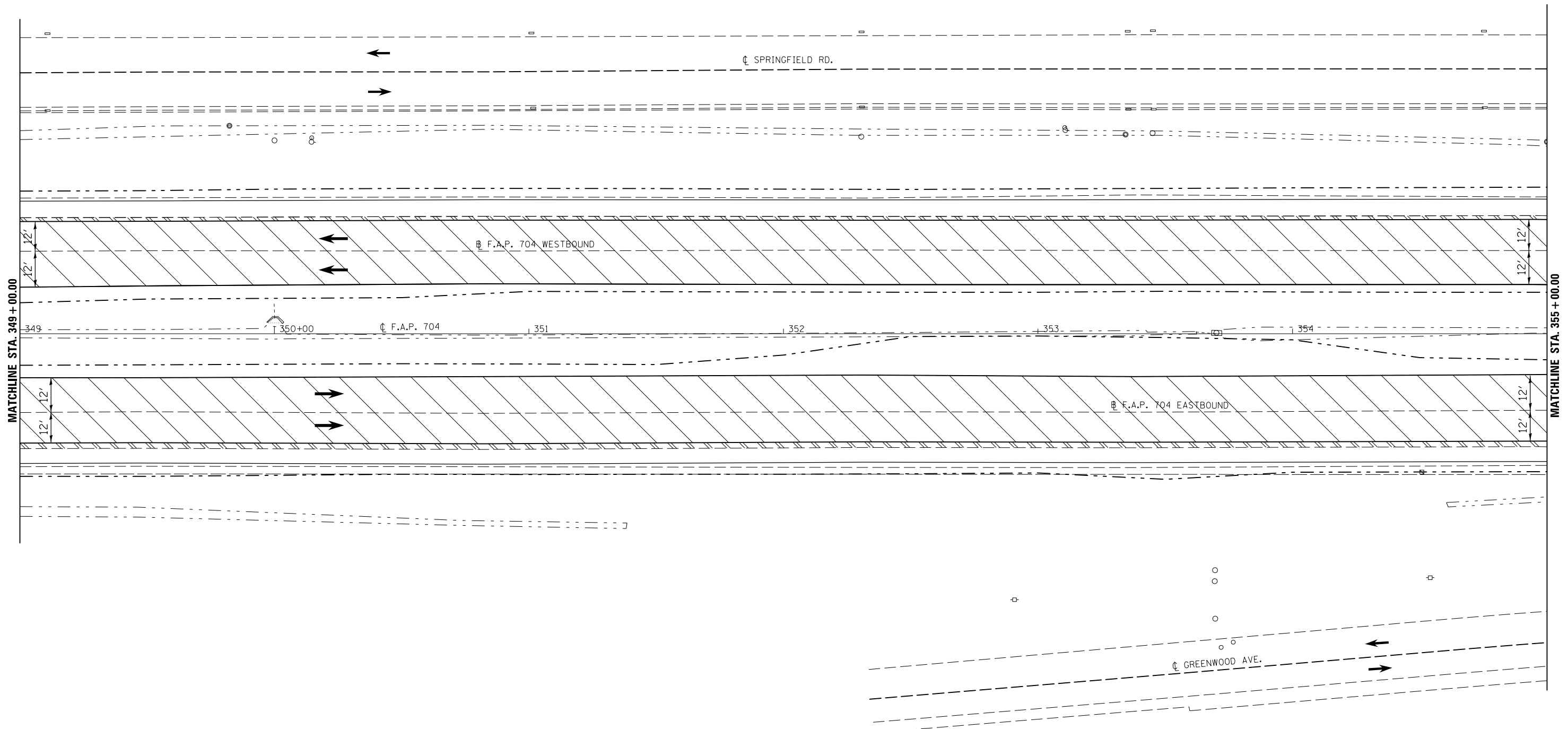
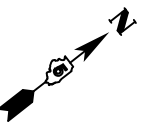
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|---|-----------------------------|------------|-----------|
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |


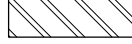
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUS 55 PLAN SHEET

SCALE: 1" = 20' SHEET 5 OF 18 SHEETS STA. 343+00.00 TO STA. 349+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 53 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



 HMA SURFACE REMOVAL, 1/2"
 PAVED SHOULDER REMOVAL

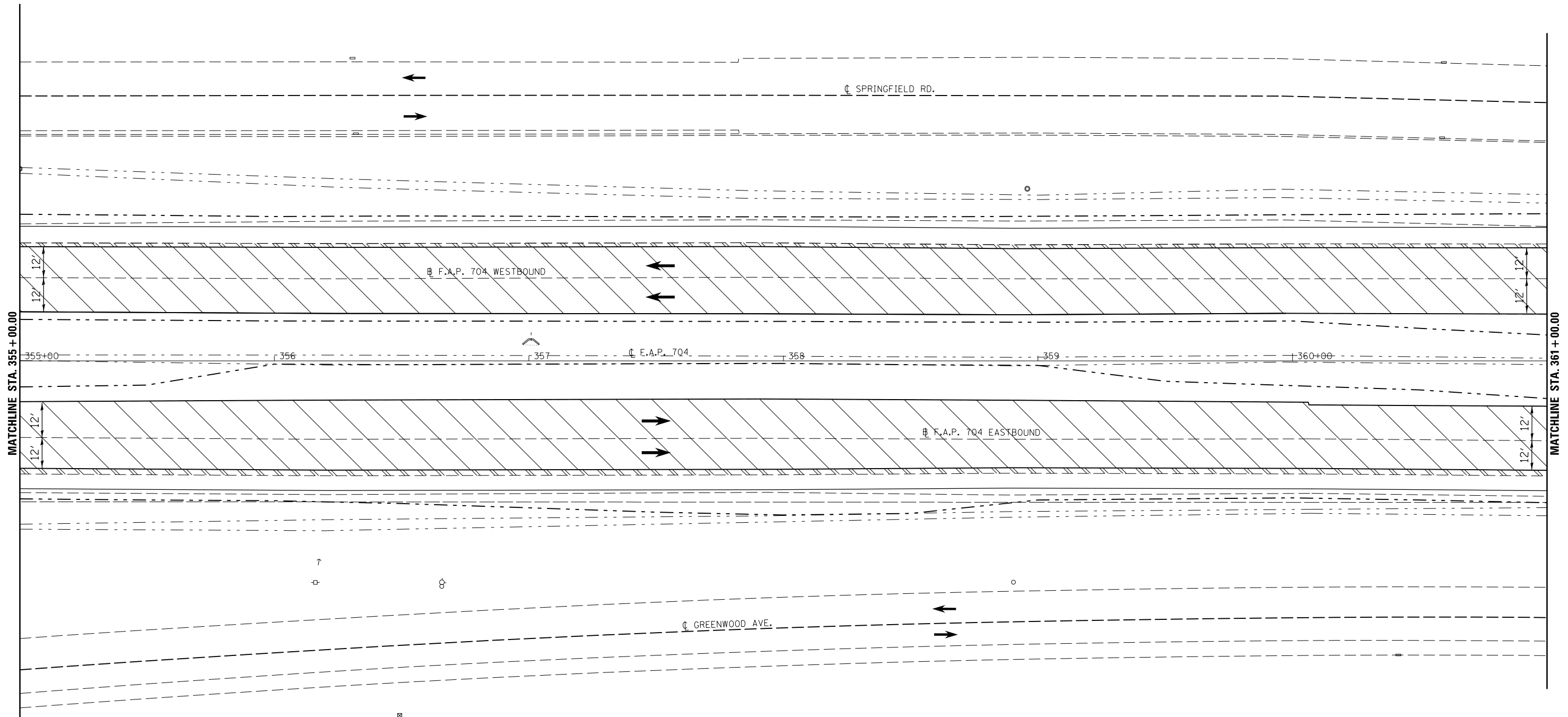
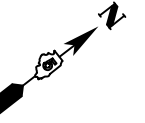
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| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - |
| et:\pw\work\p\dot\dodgms\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUS 55 PLAN SHEET

SCALE: 1" = 20' SHEET 6 OF 18 SHEETS STA. 349+00.00 TO STA. 355+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 54 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



 HMA SURFACE REMOVAL, 1/2"
 PAVED SHOULDER REMOVAL

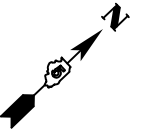
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|---|-----------------------------|------------|-----------|
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUS 55 PLAN SHEET

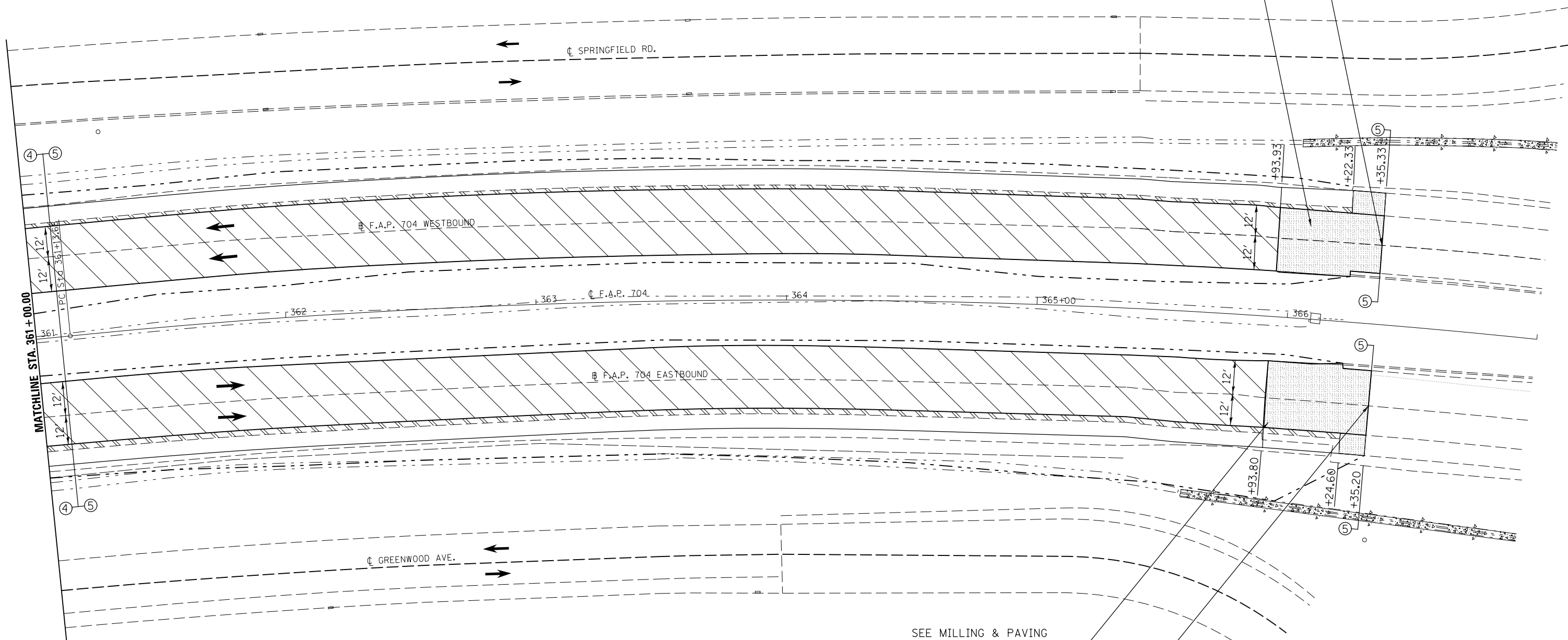
SCALE: 1" = 20' SHEET 7 OF 18 SHEETS STA. 355+00.00 TO STA. 361+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 55 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



SECTION (57-7)RS-1 & 1RS-3
ENDS LEG "A" WB
STA. 366+35.33

SEE MILLING & PAVING
TRANSITION DETAIL SHEET 130



SEE MILLING & PAVING
TRANSITION DETAIL SHEET 130

SECTION (57-7)RS-1 & 1RS-3
ENDS LEG "A" EB
STA. 366+35.20

- HMA SURFACE REMOVAL, BUTT JOINT
- HMA SURFACE REMOVAL, 1 1/2"
- PAVED SHOULDER REMOVAL

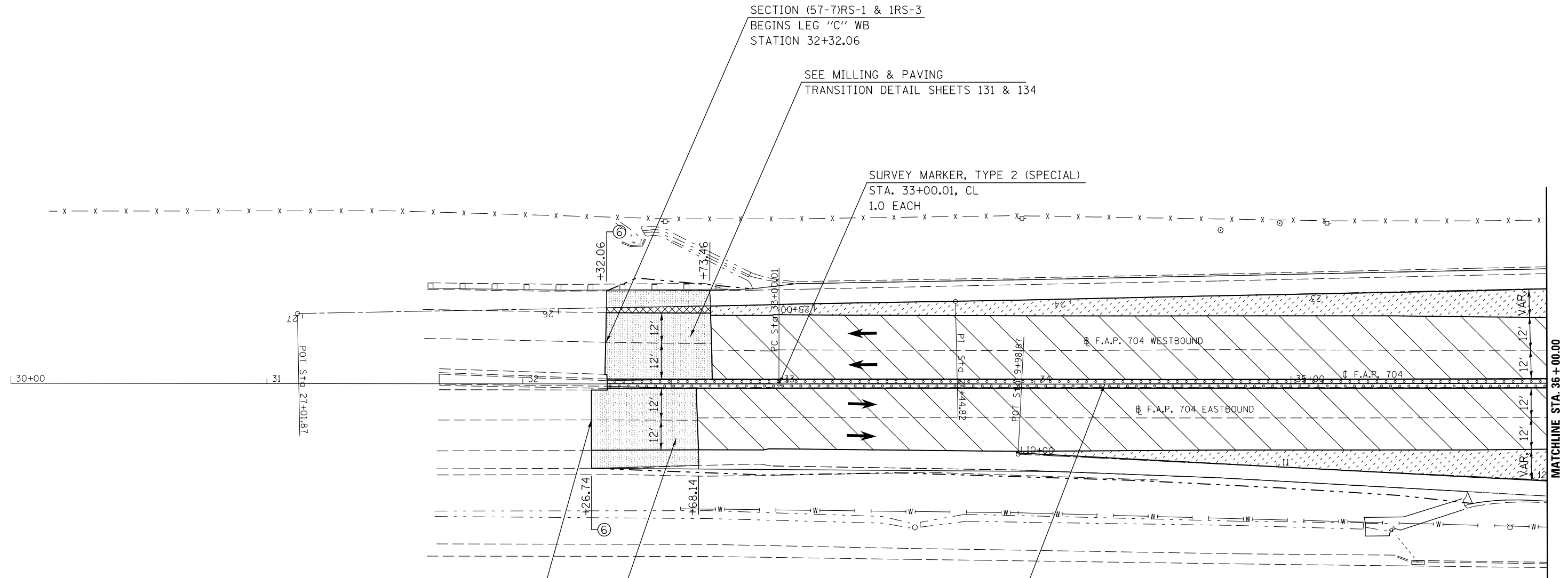
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| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - |
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUS 55 PLAN SHEET

SCALE: 1" = 20' SHEET 8 OF 18 SHEETS STA. 361+00.00 TO STA. 366+35.33



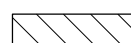
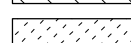

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------------|--------|--------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 56 |
| | | | CONTRACT NO. 70838 | |
| ILLINOIS FED. AID PROJECT | | | | |



SECTION (57-7)RS-1 & 1RS-3
 BEGINS LEG "C" EB
 STATION 32+26.74

SEE MILLING & PAVING
 TRANSITION DETAIL SHEET 131

REMOVE EXISTING CONCRETE MEDIAN & REPLACE
 WITH CONCRETE MEDIAN, TYPE SM (DOWELLED)

-  HMA SURFACE REMOVAL, BUTT JOINT
-  PCC SURFACE REMOVAL, BUTT JOINT
-  HMA SURFACE REMOVAL, 1 1/2"
-  PCC SURFACE REMOVAL, 3/4"
-  CONCRETE MEDIAN REMOVAL

| | | | |
|--|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - |
| et:\pw\work\p\dot\dodgms\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUS 55 PLAN SHEET

SCALE: 1" = 20' SHEET 9 OF 18 SHEETS STA. 32+26.74 TO STA. 36+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 57 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |

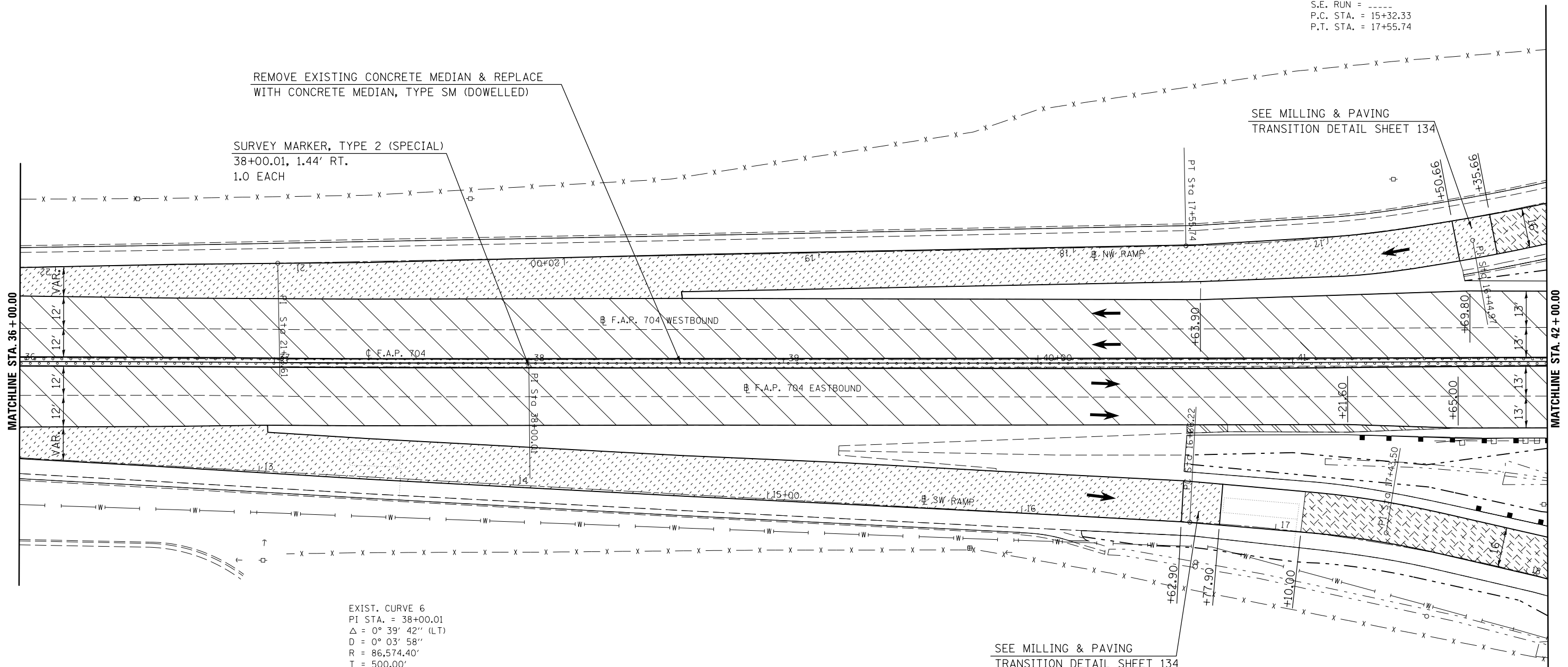
EXIST. CURVE 2
 PI STA. = 16+44.97
 $\Delta = 18^\circ 07' 37''$ (RT)
 D = 8° 06' 49"
 R = 706.17'
 T = 112.65'
 L = 223.41'
 E = 8.93'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 15+32.33
 P.T. STA. = 17+55.74



REMOVE EXISTING CONCRETE MEDIAN & REPLACE
 WITH CONCRETE MEDIAN, TYPE SM (DOWELLED)

SURVEY MARKER, TYPE 2 (SPECIAL)
 38+00.01, 1.44' RT.
 1.0 EACH

SEE MILLING & PAVING
 TRANSITION DETAIL SHEET 134



EXIST. CURVE 6
 PI STA. = 38+00.01
 $\Delta = 0^\circ 39' 42''$ (LT)
 D = 0° 03' 58"
 R = 86,574.40'
 T = 500.00'
 L = 999.99'
 E = 1.44'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 33+00.01
 P.T. STA. = 43+00.00

SEE MILLING & PAVING
 TRANSITION DETAIL SHEET 134

EXIST. CURVE 96
 PI STA. = 17+43.50
 $\Delta = 12^\circ 36' 04''$ (RT)
 D = 8° 11' 07"
 R = 699.99'
 T = 77.29'
 L = 153.95'
 E = 4.25'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 16+66.22
 P.T. STA. = 18+20.17

- PAVED SHOULDER REMOVAL
- HMA SURFACE REMOVAL, 1/2"
- PCC SURFACE REMOVAL, 3/4"
- CONCRETE MEDIAN REMOVAL
- CONCRETE PAVEMENT SCARIFICATION

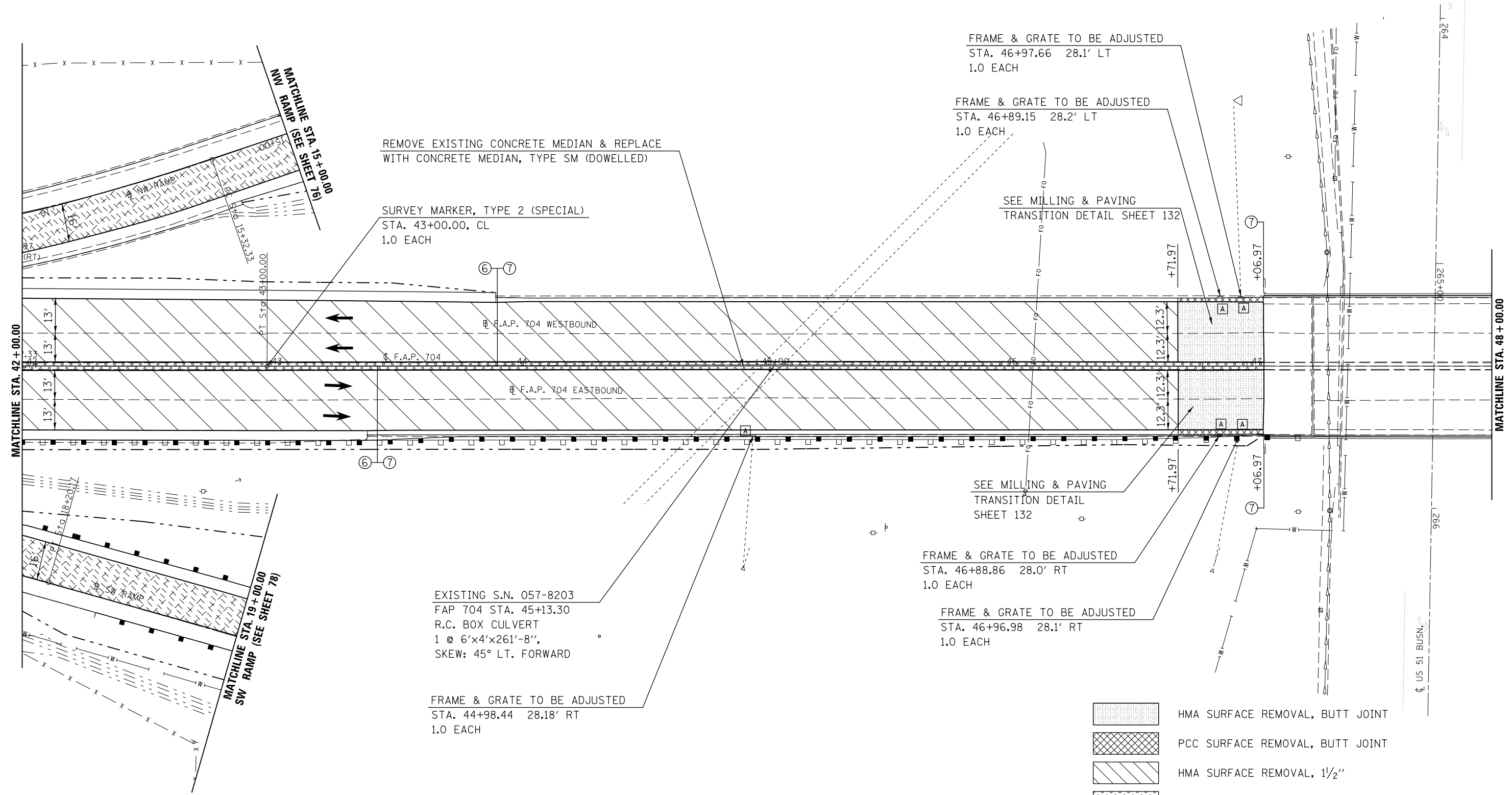
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

BUS 55 PLAN SHEET

SCALE: 1" = 20' SHEET 10 OF 18 SHEETS STA. 36+00.00 TO STA. 42+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------------|--------|--------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 58 |
| CONTRACT NO. 70838 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



SECTION 16

- HMA SURFACE REMOVAL, BUTT JOINT
- PCC SURFACE REMOVAL, BUTT JOINT
- HMA SURFACE REMOVAL, 1/2"
- CONCRETE MEDIAN REMOVAL
- CONCRETE PAVEMENT SCARIFICATION

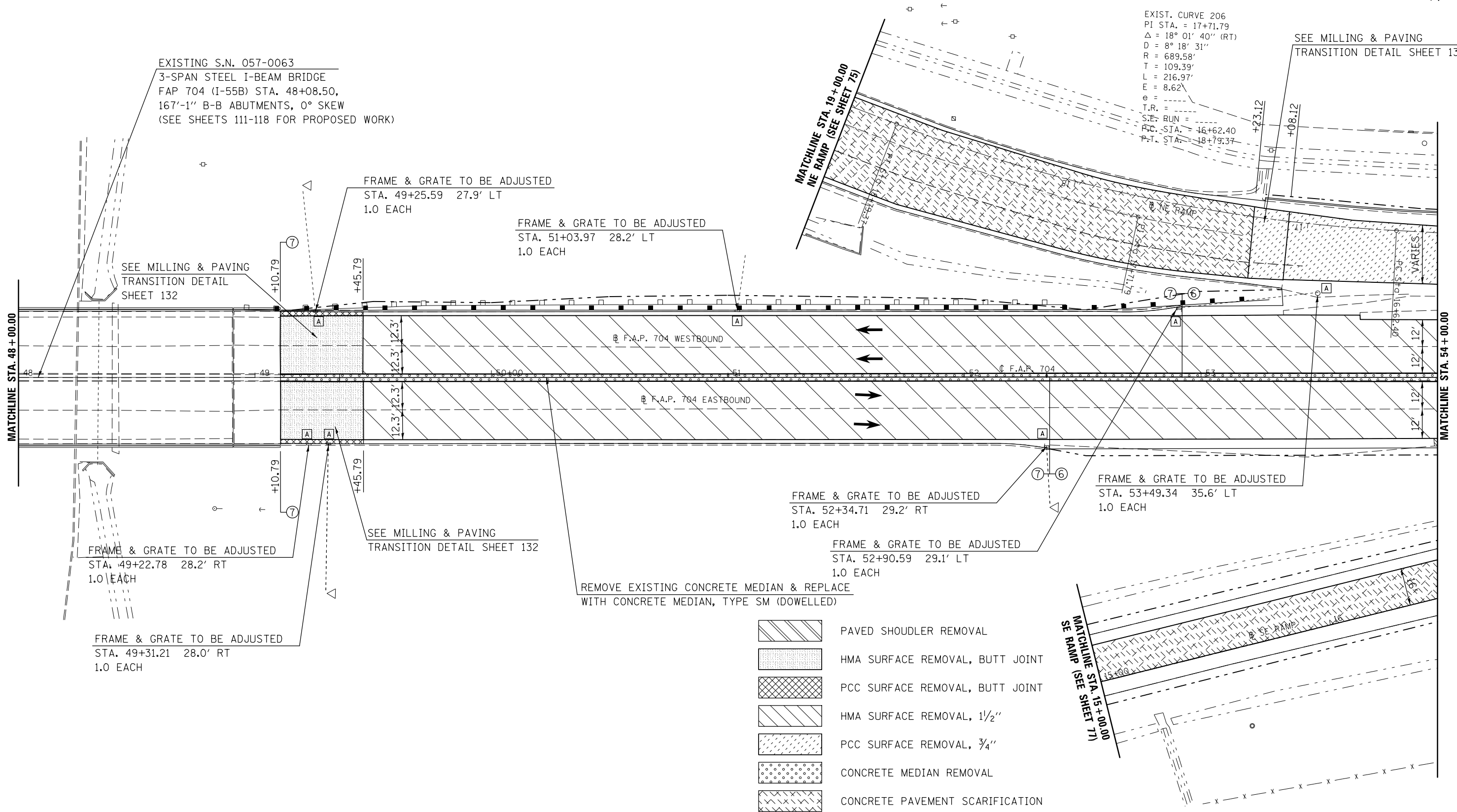
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUS 55 PLAN SHEET

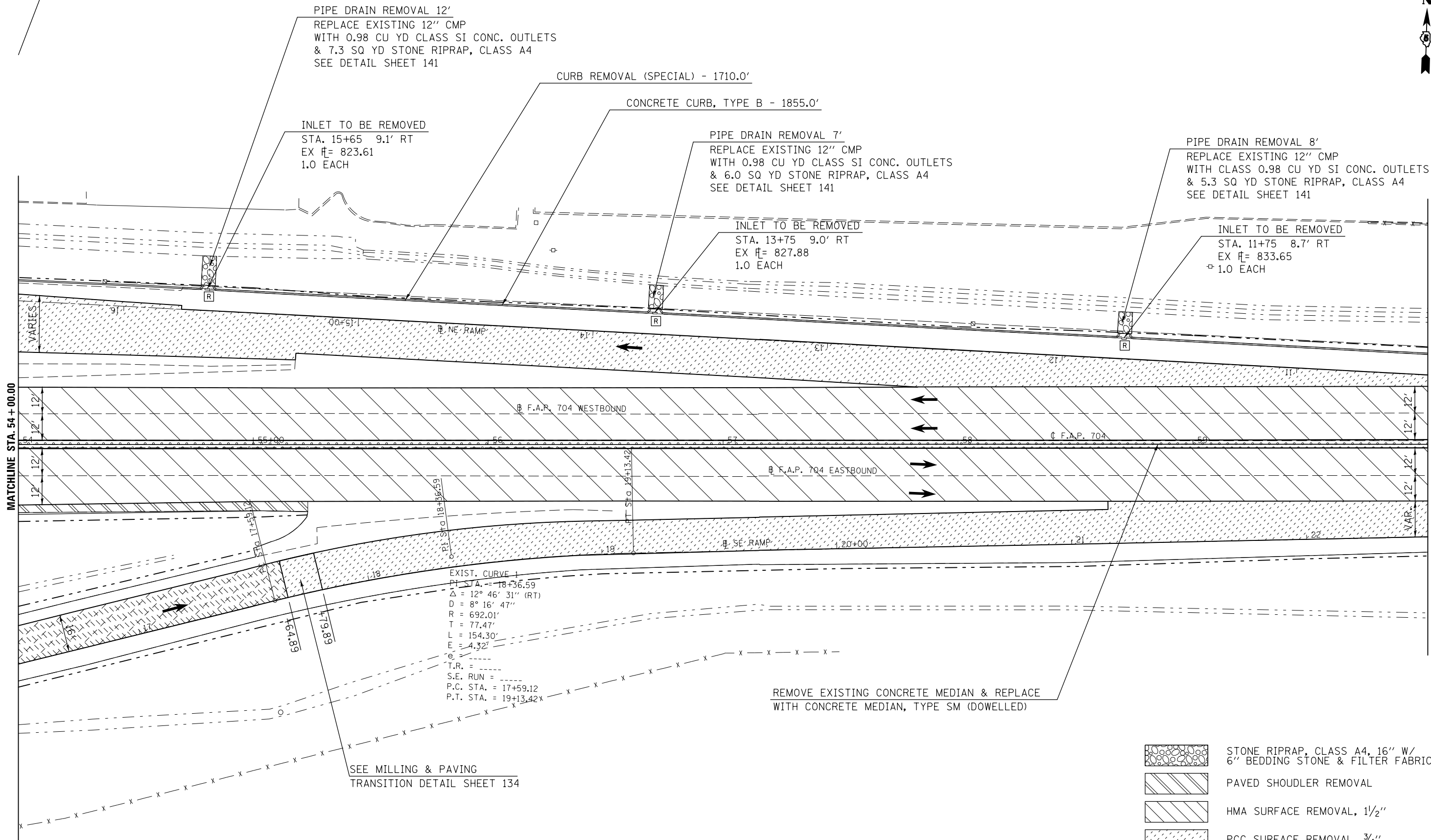
SCALE: 1" = 20' SHEET 11 OF 18 SHEETS STA. 42+00.00 TO STA. 48+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 59 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



SECTION 16

| | | | | | | | | | | | | |
|---|-----------------------------|------------|-----------|---|--|--|--|---------------------------|------------------|--------|--------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | BUS 55 PLAN SHEET | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht-plan_BUS55.dgn | | DRAWN - | REVISED - | | | | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 60 |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | SCALE: 1" = 20' | | | CONTRACT NO. 70838 | | | | |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - | | SHEET 12 OF 18 SHEETS STA. 48+00.00 TO STA. 54+00.00 | | | ILLINOIS FED. AID PROJECT | | | | |





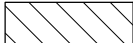
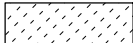
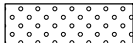
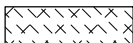
MATCHLINE STA. 54+00.00

MATCHLINE STA. 60+00.00

EXIST. CURVE 1
 PT. STA. = 18+36.59
 Δ = 12° 46' 31" (RT)
 D = 8° 16' 47"
 R = 692.01'
 T = 77.47'
 L = 154.30'
 E = 4.32'
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 17+59.12
 P.T. STA. = 19+13.42

REMOVE EXISTING CONCRETE MEDIAN & REPLACE WITH CONCRETE MEDIAN, TYPE SM (DOWELLED)

SEE MILLING & PAVING TRANSITION DETAIL SHEET 134

-  STONE RIPRAP, CLASS A4, 16" W/ 6" BEDDING STONE & FILTER FABRIC
-  PAVED SHOULDER REMOVAL
-  HMA SURFACE REMOVAL, 1 1/2"
-  PCC SURFACE REMOVAL, 3/4"
-  CONCRETE MEDIAN REMOVAL
-  CONCRETE PAVEMENT SCARIFICATION

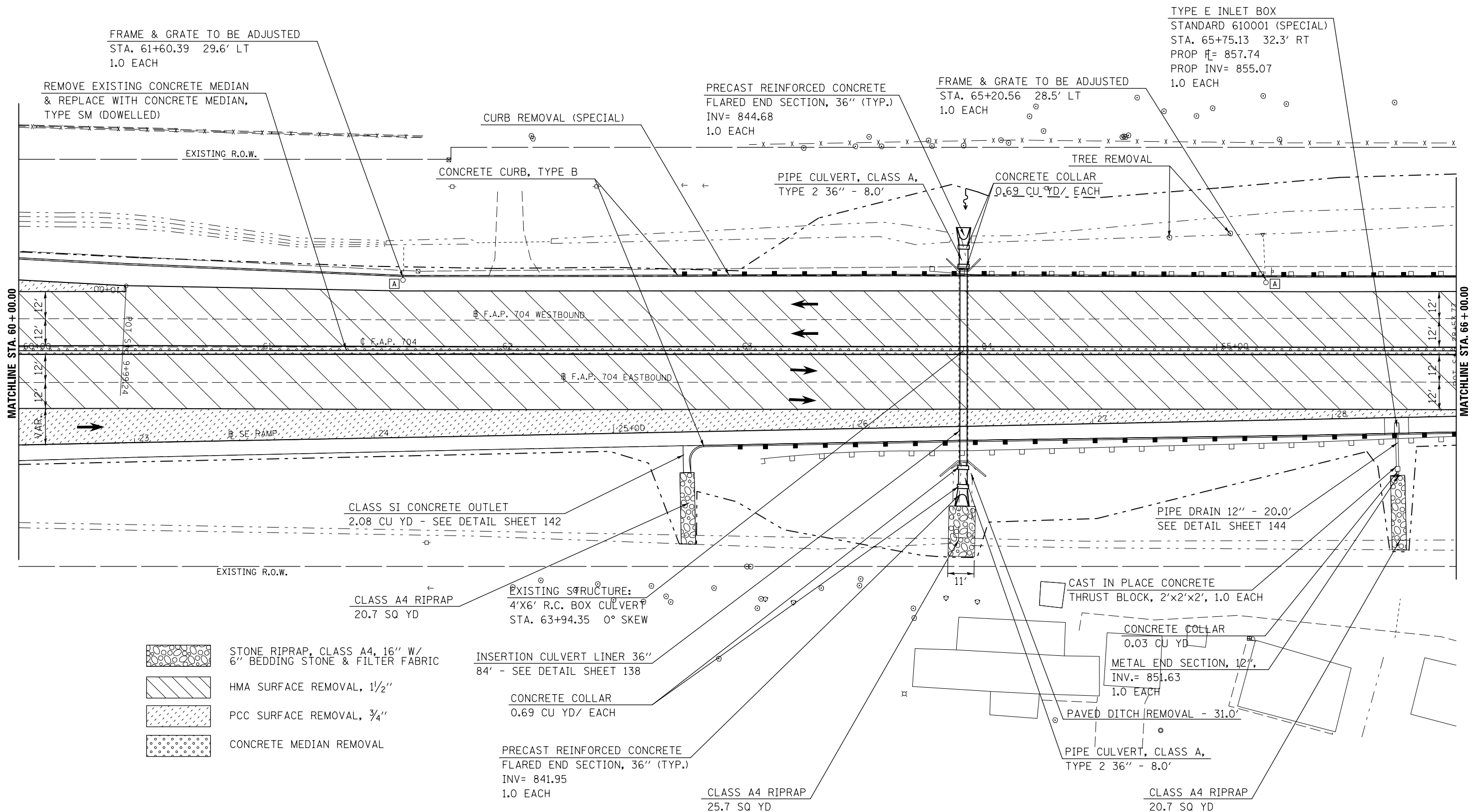
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|---|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BUS 55 PLAN SHEET

SCALE: 1" = 20' SHEET 13 OF 18 SHEETS STA. 54+00.00 TO STA. 60+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------------|--------|--------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 61 |
| | | | CONTRACT NO. 70838 | |
| ILLINOIS FED. AID PROJECT | | | | |



FRAME & GRATE TO BE ADJUSTED
STA. 61+60.39 29.6' LT
1.0 EACH

TYPE E INLET BOX
STANDARD 610001 (SPECIAL)
STA. 65+75.13 32.3' RT
PROP \bar{r} = 857.74
PROP INV = 855.07
1.0 EACH

REMOVE EXISTING CONCRETE MEDIAN
& REPLACE WITH CONCRETE MEDIAN,
TYPE SM (DOWELLED)

PRECAST REINFORCED CONCRETE
FLARED END SECTION, 36" (TYP.)
INV = 844.68
1.0 EACH

FRAME & GRATE TO BE ADJUSTED
STA. 65+20.56 28.5' LT
1.0 EACH

CURB REMOVAL (SPECIAL)

CONCRETE CURB, TYPE B

PIPE CULVERT, CLASS A,
TYPE 2 36" - 8.0'

CONCRETE COLLAR
0.69 CU YD/ EACH

MATCHLINE STA. 60+00.00

MATCHLINE STA. 66+00.00

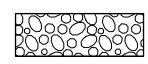

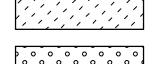
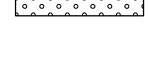
CLASS SI CONCRETE OUTLET
2.08 CU YD - SEE DETAIL SHEET 142

PIPE DRAIN 12" - 20.0'
SEE DETAIL SHEET 144

CLASS A4 RIPRAP
20.7 SQ YD

EXISTING STRUCTURE:
4'X6' R.C. BOX CULVERT
STA. 63+94.35 0° SKEW

CAST IN PLACE CONCRETE
THRUST BLOCK, 2'X2'X2', 1.0 EACH

-  STONE RIPRAP, CLASS A4, 16" W/
6" BEDDING STONE & FILTER FABRIC
-  HMA SURFACE REMOVAL, 1 1/2"
-  PCC SURFACE REMOVAL, 3/4"
-  CONCRETE MEDIAN REMOVAL

INSERTION CULVERT LINER 36"
84' - SEE DETAIL SHEET 138

CONCRETE COLLAR
0.69 CU YD/ EACH

CONCRETE COLLAR
0.03 CU YD
METAL END SECTION, 12"
INV = 851.63
1.0 EACH

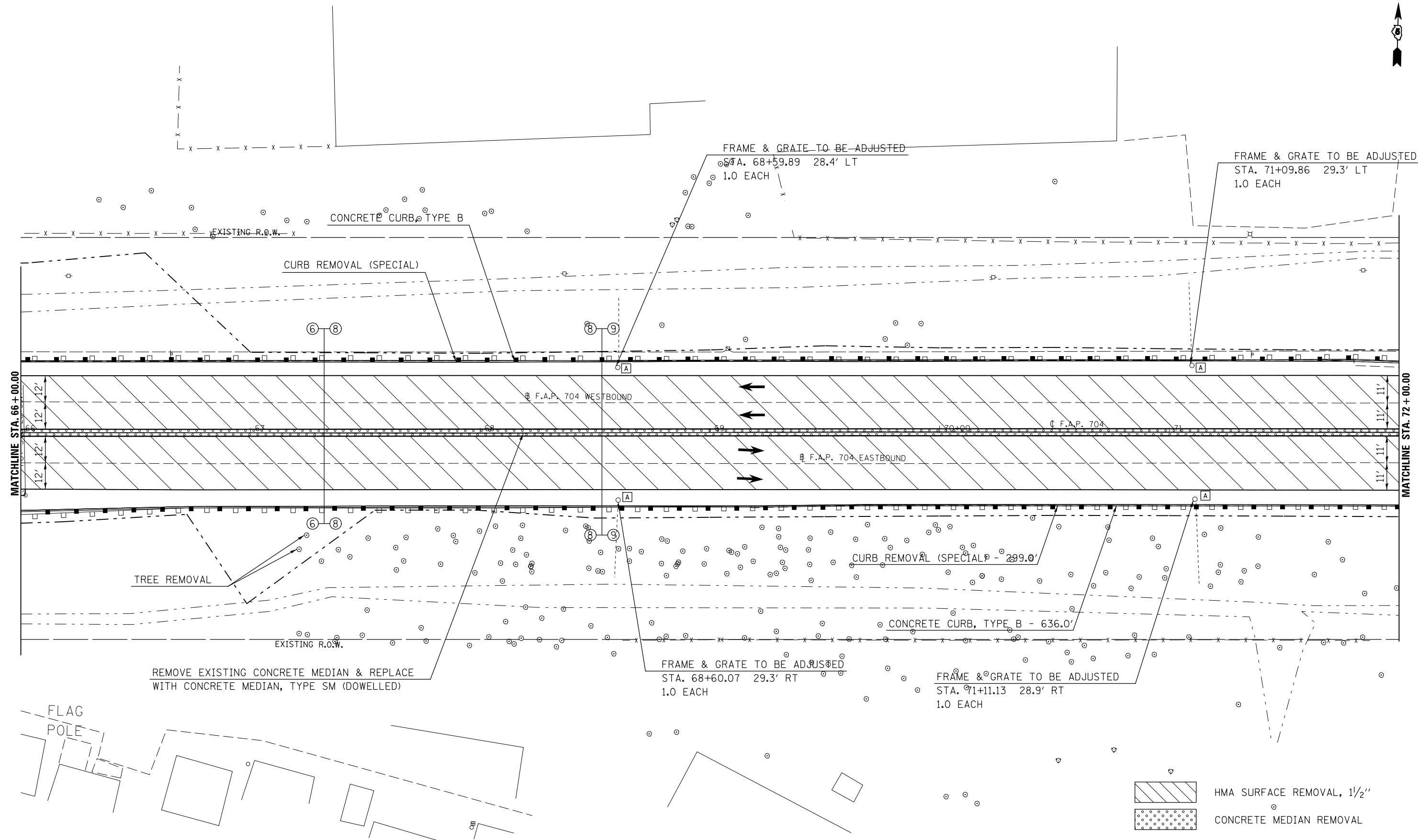
PRECAST REINFORCED CONCRETE
FLARED END SECTION, 36" (TYP.)
INV = 841.95
1.0 EACH

CLASS A4 RIPRAP
25.7 SQ YD

PIPE CULVERT, CLASS A,
TYPE 2 36" - 8.0'

CLASS A4 RIPRAP
20.7 SQ YD

| | | | | | | | | | | | | |
|---|-----------------------------|------------|-----------|---|--|--|--|--------------------|------------------|---------------------------|-----------------|--------------|
| FILE NAME = c:\pwwork\pwwork\dodgema\d0267571\0570838-sht-plan_BUS55.dgn | USER NAME = dodgema | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | BUS 55 PLAN SHEET | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | | | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 62 |
| PLOT DATE = 6/11/2014 | DATE - | REVISED - | REVISED - | | SCALE: 1" = 20' SHEET 14 OF 18 SHEETS STA. 60+00.00 TO STA. 66+00.00 | | | CONTRACT NO. 70838 | | ILLINOIS FED. AID PROJECT | | |



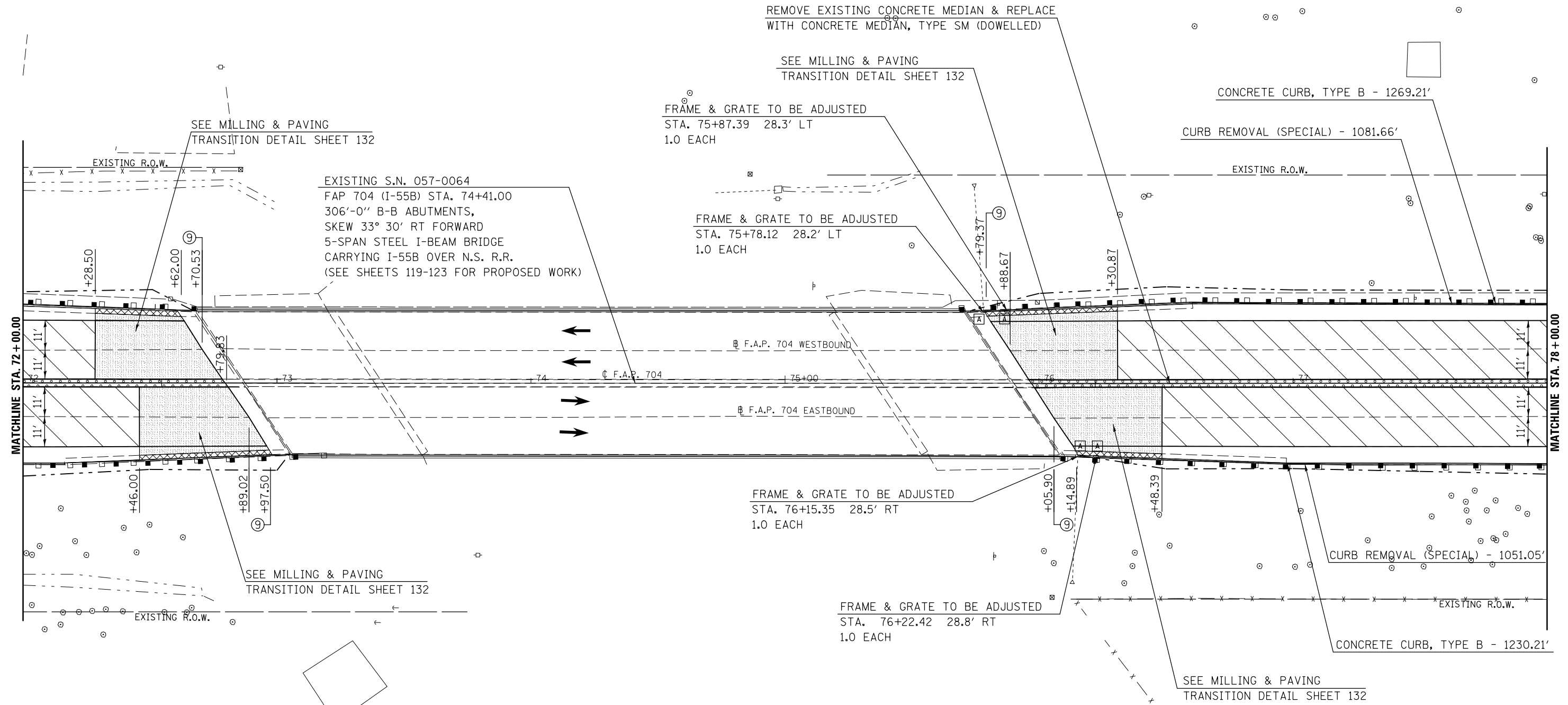
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUS 55 PLAN SHEET

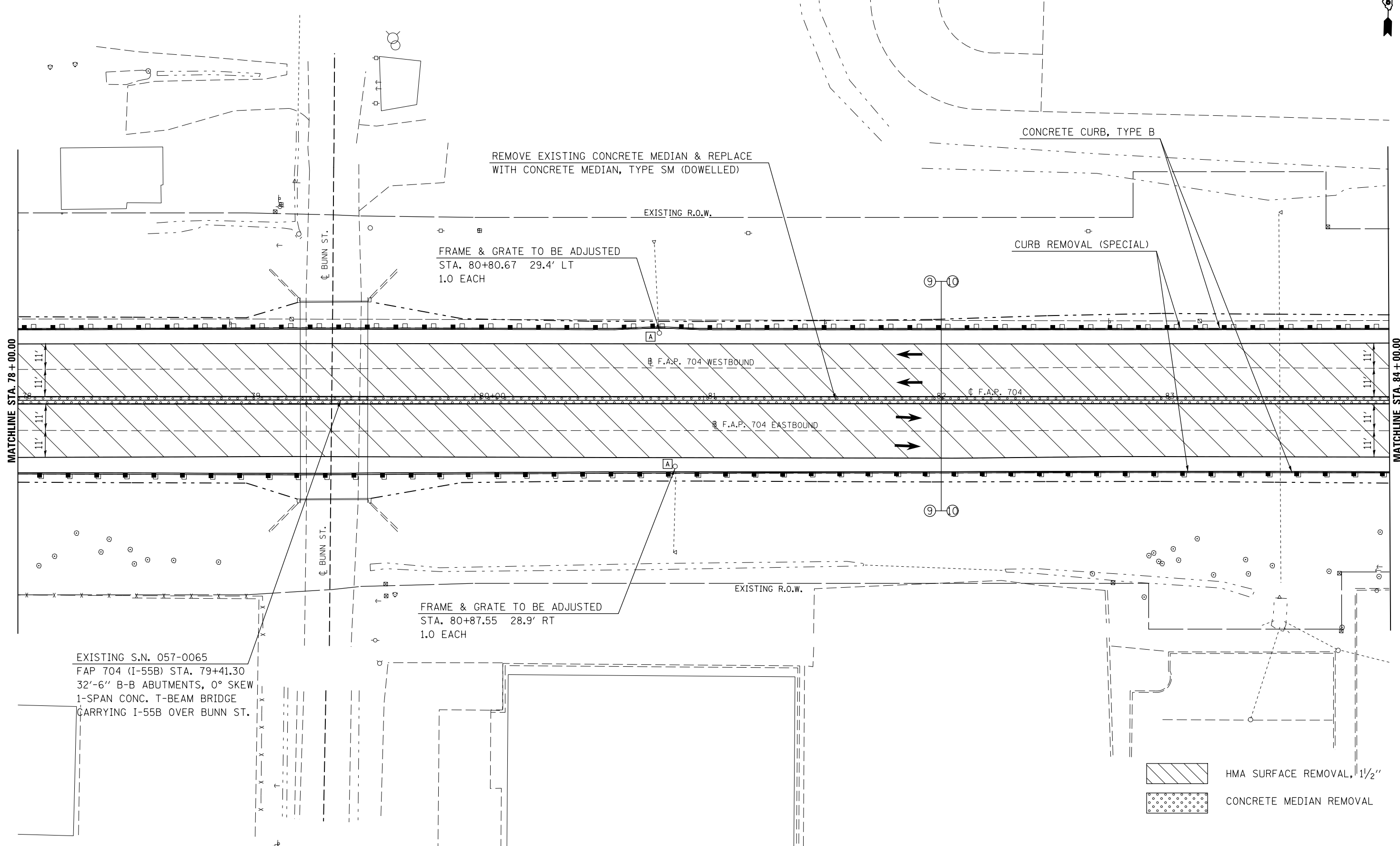
SCALE: 1" = 20' SHEET 15 OF 18 SHEETS STA. 66+00 TO STA. 72+00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 63 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



- HMA SURFACE REMOVAL, BUTT JOINT
- PCC SURFACE REMOVAL, BUTT JOINT
- HMA SURFACE REMOVAL, 1 1/2"
- CONCRETE MEDIAN REMOVAL

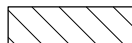
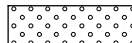
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| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | BUS 55 PLAN SHEET | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| et:\pw\work\p\dot\dodgms\d0267571\0570838-sht-plan_BUS55.dgn | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | | | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 64 | |
| PLOT DATE = 6/11/2014 | DATE - | REVISED - | REVISED - | | SCALE: 1" = 20' | | | SHEET 16 OF 18 SHEETS | | STA. 72+00.00 TO STA. 78+00.00 | | CONTRACT NO. 70838 | |
| ILLINOIS FED. AID PROJECT | | | | | | | | | | | | | |



MATCHLINE STA. 78+00.00

MATCHLINE STA. 84+00.00

EXISTING S.N. 057-0065
 FAP 704 (I-55B) STA. 79+41.30
 32'-6" B-B ABUTMENTS, 0° SKEW
 1-SPAN CONC. T-BEAM BRIDGE
 CARRYING I-55B OVER BUNN ST.

 HMA SURFACE REMOVAL, 1 1/2"
 CONCRETE MEDIAN REMOVAL

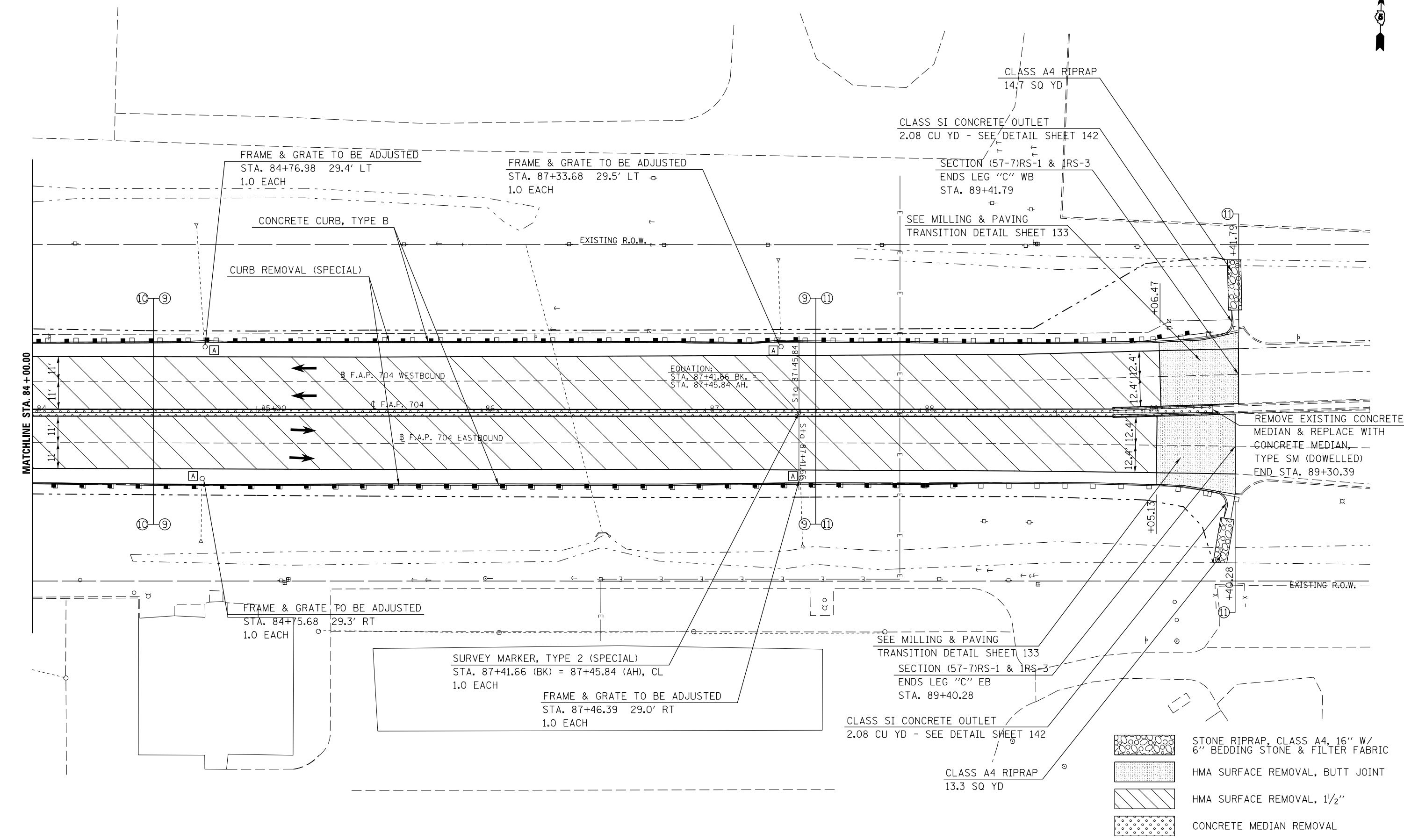
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

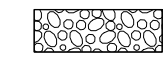

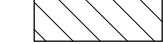
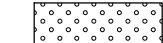
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

BUS 55 PLAN SHEET

SCALE: 1" = 20' SHEET 17 OF 18 SHEETS STA. 78+00.00 TO STA. 84+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 65 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



-  STONE RIPRAP, CLASS A4, 16" W/ 6" BEDDING STONE & FILTER FABRIC
-  HMA SURFACE REMOVAL, BUTT JOINT
-  HMA SURFACE REMOVAL, 1 1/2"
-  CONCRETE MEDIAN REMOVAL

| | | | |
|--|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - |
| et:\pw\work\p1dot\dodgms\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

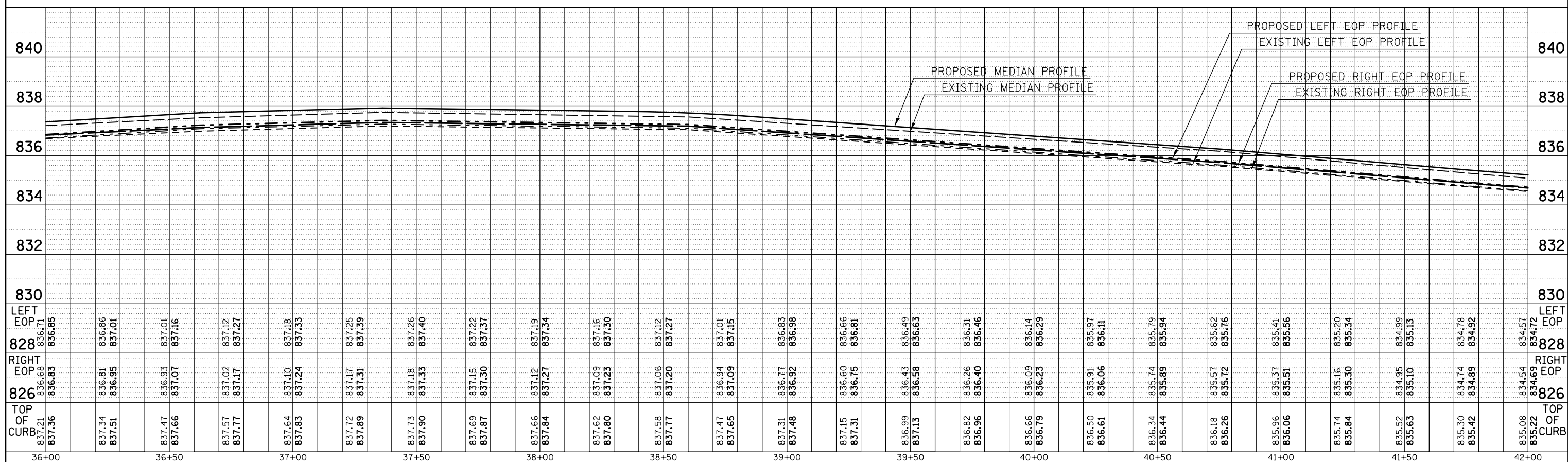
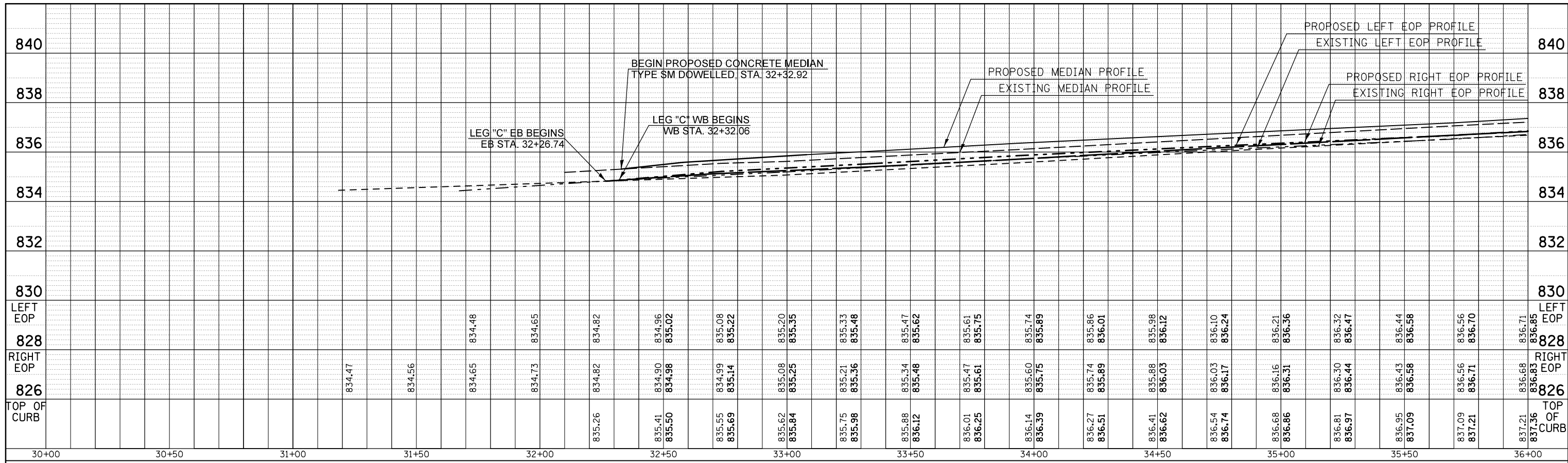
BUS 55 PLAN SHEET

SCALE: 1" = 20' SHEET 18 OF 18 SHEETS STA. 84+00.00 TO STA. 106+41.40

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&RS-3 | McLean | 273 | 66 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |

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| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | ALIGNED | | |
| | CHECKED | | |
| | FILE NAME | | |
| | NO. | | |

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| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES | | |
| | CHECKED | | |
| | STRUCTURE | | |
| | NOTATIONS | | |
| | CPWD | | |
| | NO. | | |



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| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - |
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| *MODELNAME* | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

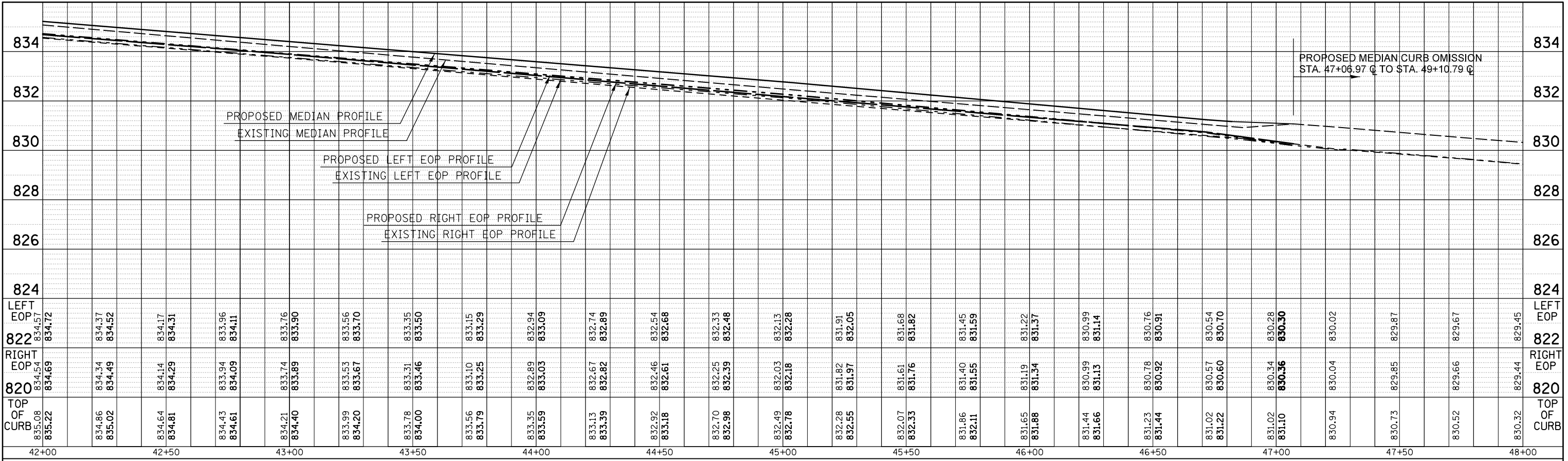
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUS 55
MEDIAN PROFILES**

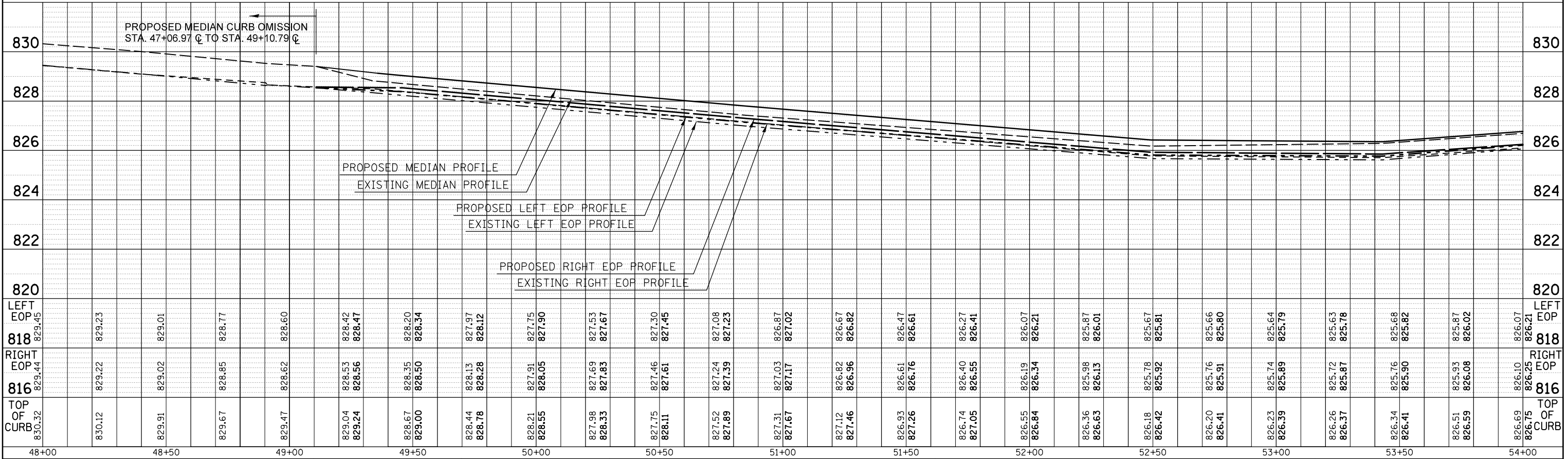
SCALE: SHEET 1 OF 7 SHEETS STA. 32+26.74 TO STA. 42+00.00

| | | | | |
|--------------------|------------------|--------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 67 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |

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|------|-----------------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | ALIGNMENT CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |
| | CARD FILE NAME | | |



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|---------|-----------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | ALIGNMENT CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |
| | CARD FILE NAME | | |



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| *MODELNAME* | PLOT DATE = 6/11/2014 | DATE = | REVISED = |

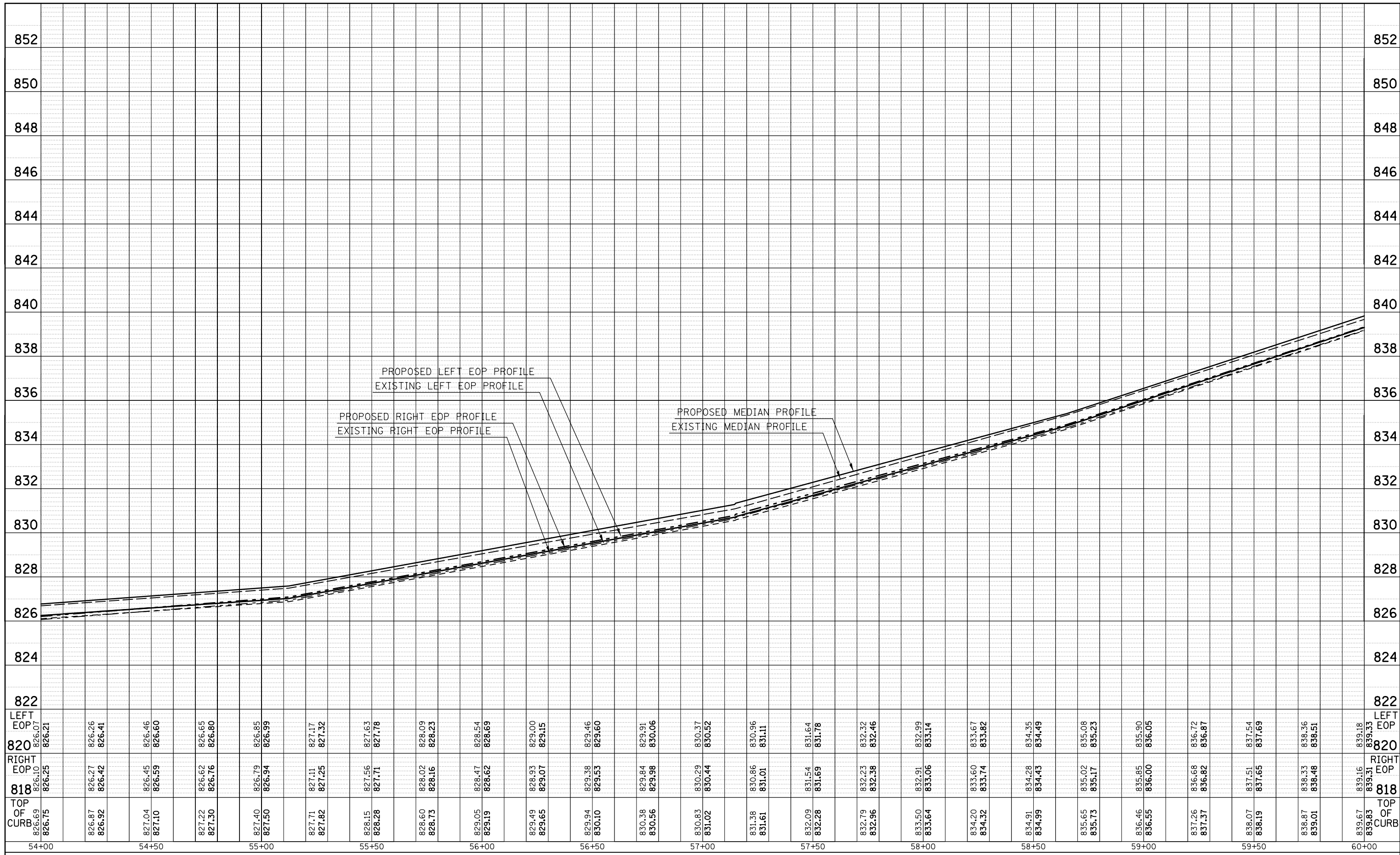
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUS 55
MEDIAN PROFILES**

| | | | | |
|--------------------|------------------|--------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 68 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |

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|---------------|-------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| NOTE BOOK NO. | PLOTTED | | |
| | ALIGNMENT CHECKED | | |
| | FIELD FILE NAME | | |

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| PROFILE | SURVEYED | BY | DATE |
| NOTE BOOK NO. | PLOTTED | | |
| | GRADES CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |



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| \$MODELNAME\$ | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

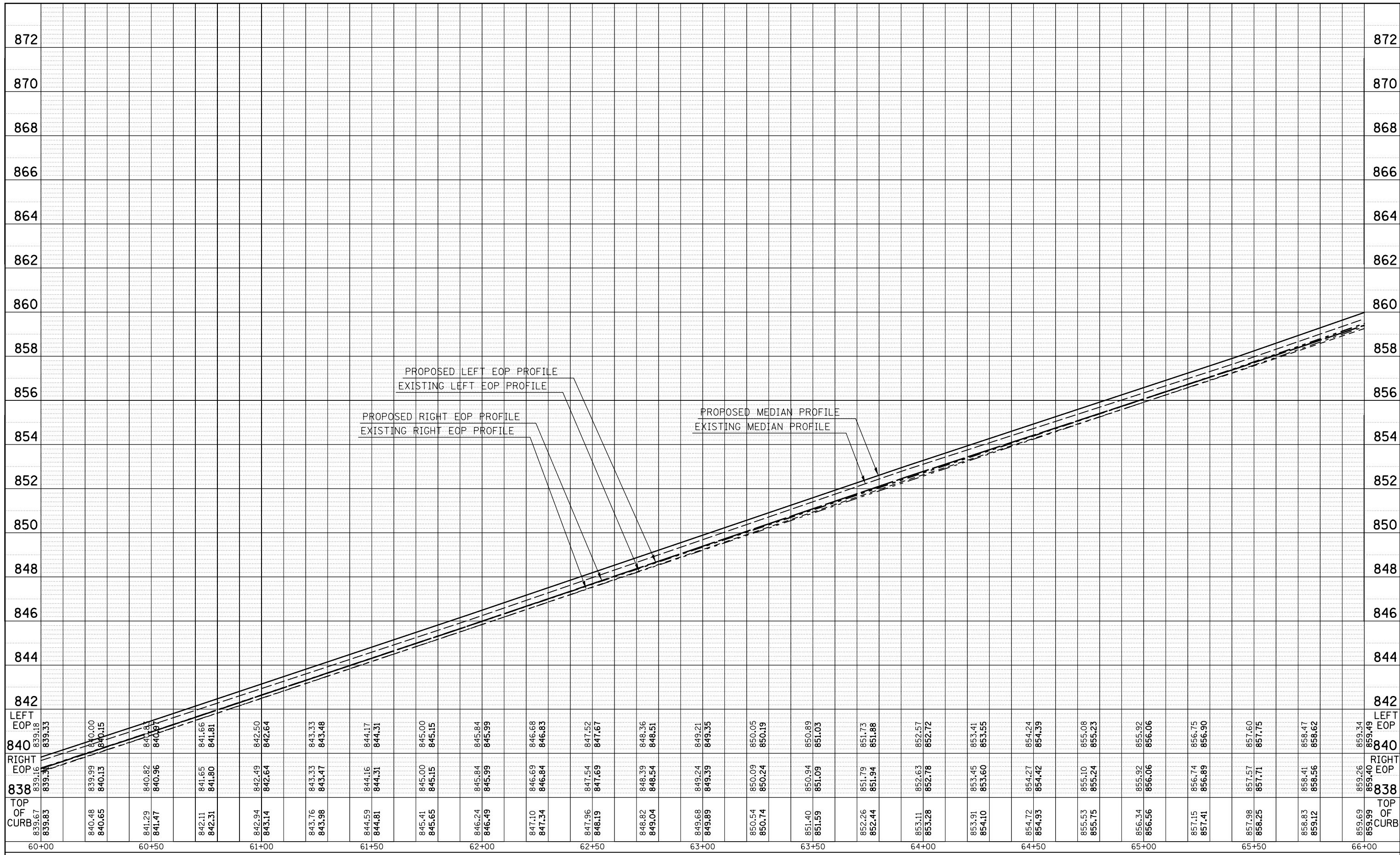
**BUS 55
MEDIAN PROFILES**

SCALE: SHEET 3 OF 7 SHEETS STA. 54+00.00 TO STA. 60+00.00

| | | | | |
|--------------------|------------------|--------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 69 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |

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|---------------|-----------------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| NOTE BOOK NO. | PLOTTED | | |
| | ALIGNMENT CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | CARD FILE NAME | | |

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|---------------|-----------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| NOTE BOOK NO. | PLOTTED | | |
| | GRADES CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |



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| \$MODELNAME\$ | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUS 55
MEDIAN PROFILES**

SCALE: SHEET 4 OF 7 SHEETS STA. 60+00.00 TO STA. 66+00.00

| | | | | |
|--------------------|------------------|--------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 70 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |

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|---------------|-----------------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| NOTE BOOK NO. | PLOTTED | | |
| | ALIGNMENT CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | CARD FILE NAME | | |

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|---------------|-----------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| NOTE BOOK NO. | PLOTTED | | |
| | GRADES CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |



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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| \$MODELNAME\$ | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

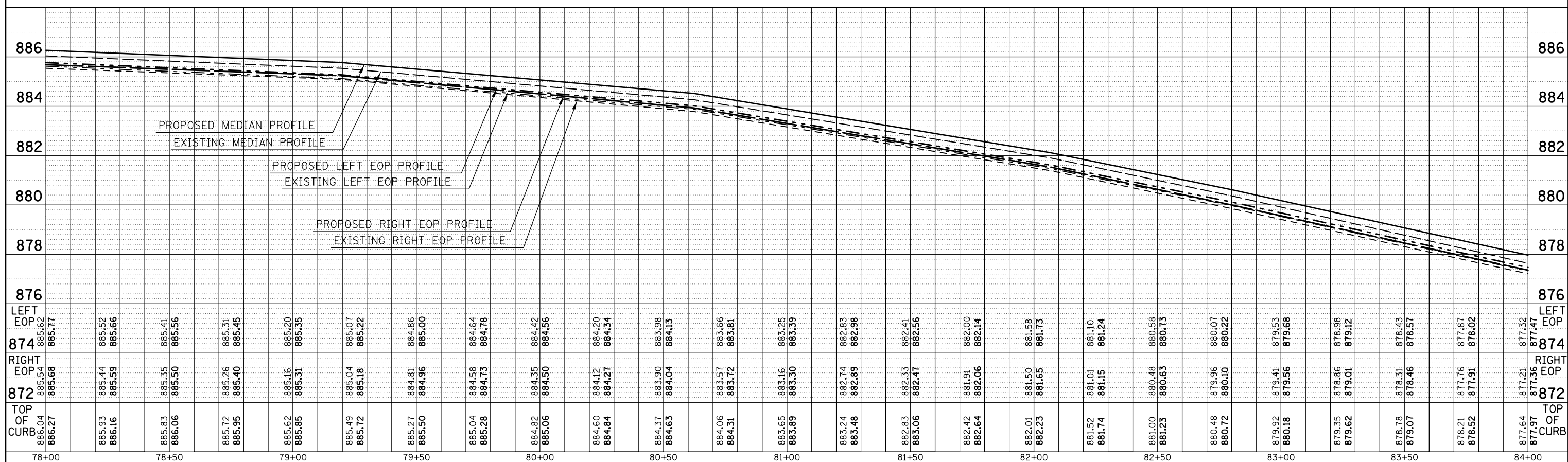
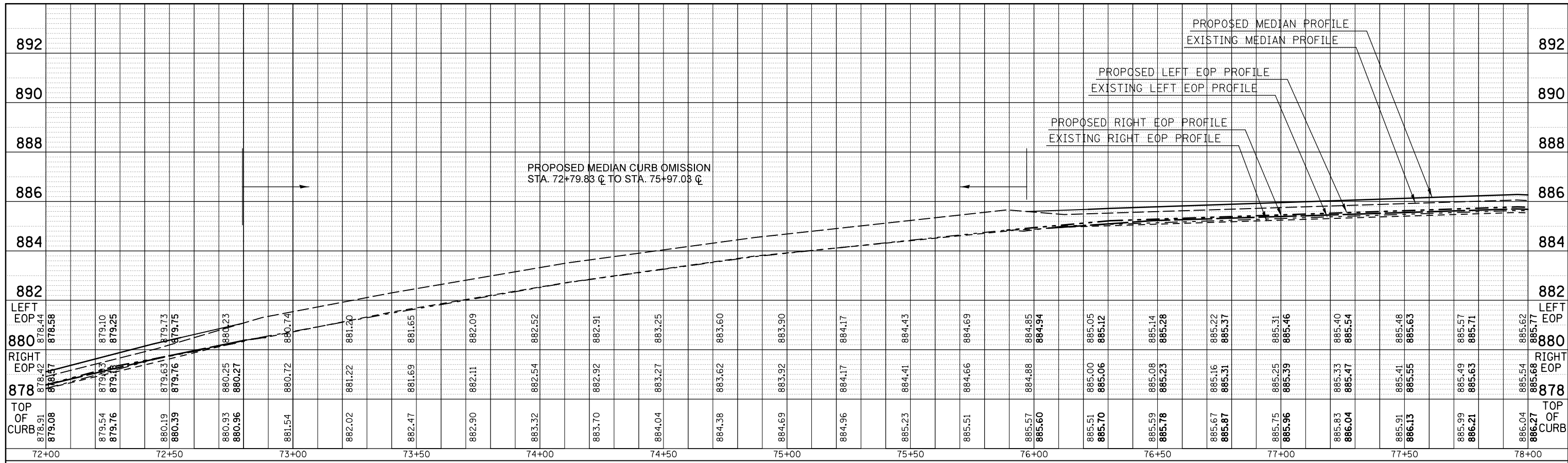
**BUS 55
MEDIAN PROFILES**

SCALE: SHEET 5 OF 7 SHEETS STA. 66+00.00 TO STA. 72+00.00

| | | | | |
|--------------------|------------------|--------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 71 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |

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| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | ALIGNED | | |
| | CHECKED | | |
| | FILED | | |
| | NO. | | |

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| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES | | |
| | CHECKED | | |
| | STRUCTURE | | |
| | NOTATIONS | | |
| | CPAD | | |
| | NO. | | |



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| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

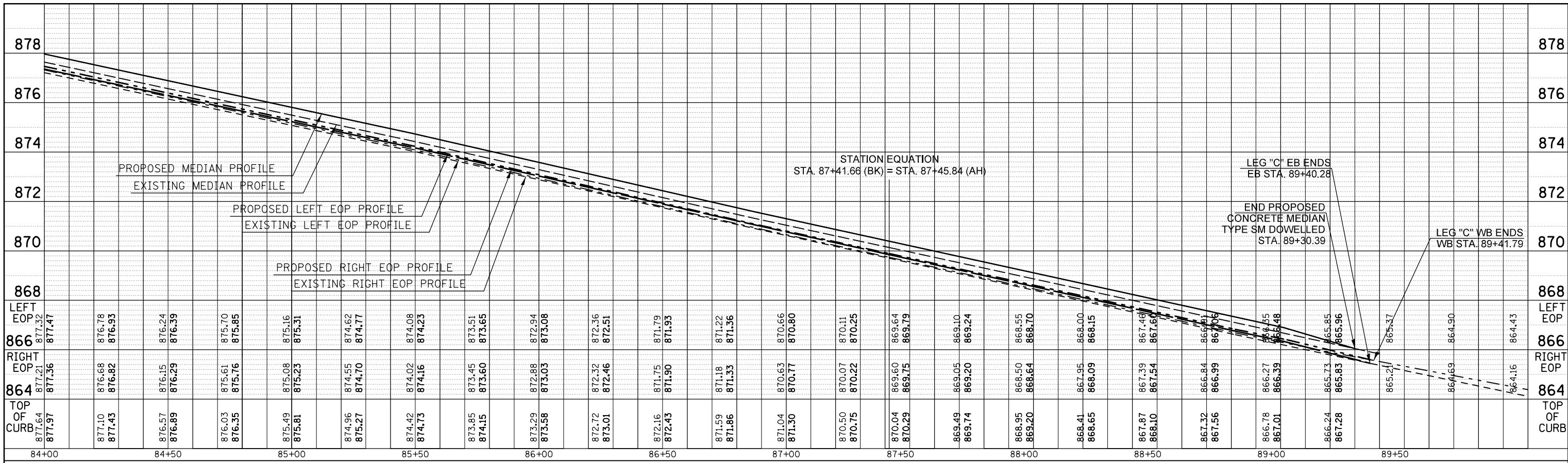
| | | | | |
|--------|---------|-------------|---------------|------------------|
| SCALE: | SHEET 6 | OF 7 SHEETS | STA. 72+00.00 | TO STA. 84+00.00 |
|--------|---------|-------------|---------------|------------------|

**BUS 55
MEDIAN PROFILES**

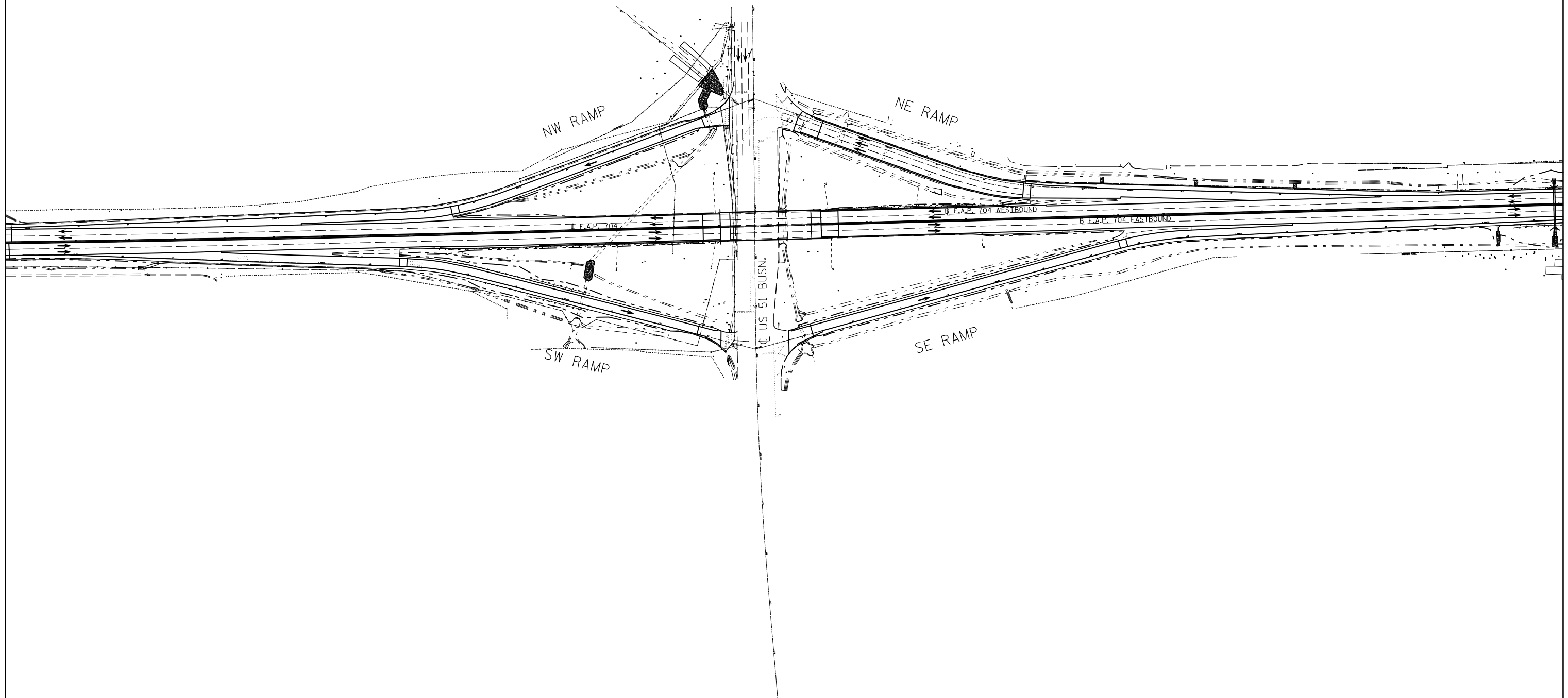
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 72 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |

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|------|-----------------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |
| | ALIGNMENT CHECKED | | |
| | CARD FILE NAME | | |

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|---------|-----------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |



| STATION | TOP OF CURB | RIGHT EOP | LEFT EOP |
|---------|-------------|-----------|----------|
| 84+00 | 877.97 | 877.36 | 877.47 |
| 84+50 | 876.89 | 876.29 | 876.39 |
| 85+00 | 875.81 | 875.23 | 875.31 |
| 85+50 | 874.73 | 874.16 | 874.23 |
| 86+00 | 873.58 | 873.03 | 873.08 |
| 86+50 | 872.43 | 871.90 | 871.93 |
| 87+00 | 871.86 | 871.33 | 871.36 |
| 87+50 | 870.29 | 869.75 | 869.79 |
| 88+00 | 868.20 | 868.64 | 868.70 |
| 88+50 | 867.10 | 867.54 | 867.60 |
| 89+00 | 866.01 | 866.39 | 866.48 |
| 89+50 | 865.28 | 865.83 | 865.96 |



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|--|------------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - |
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| | PLOT SCALE = 200.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INTERCHANGE
PLAN SHEET**

SCALE: 1" = 100' SHEET 1 OF 5 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------------|--------|---------------------------|-----------|
| 704 | (57-7RS-1&1RS-3 | McLean | 273 | 74 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |

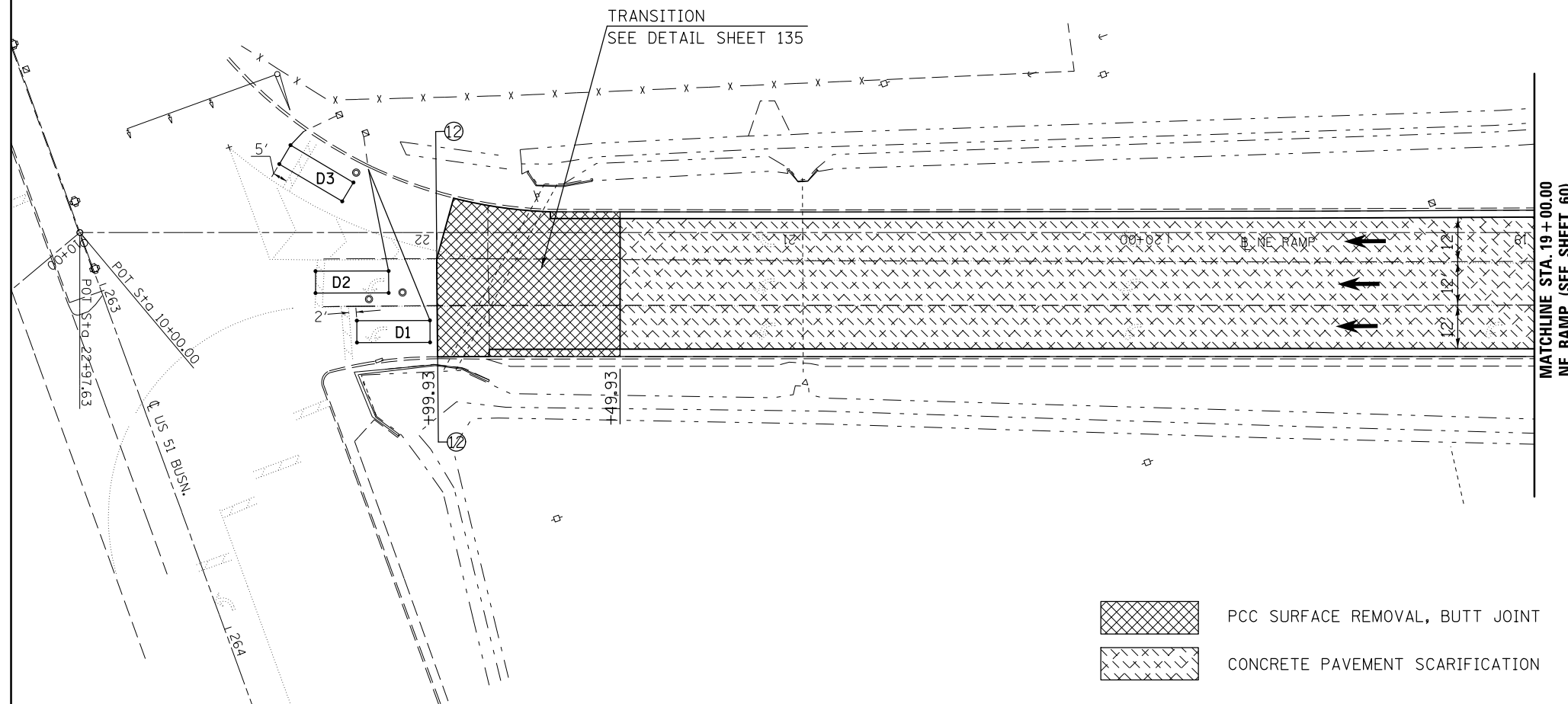
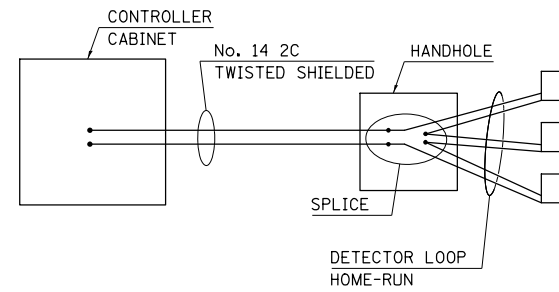


DETAIL OF DETECTOR LOOP WIRING

WIRED IN SERIES WITH SINGLE PAIR CABLE

| LOOP INDUCTANCE DATA | | | |
|---------------------------------------|--------|-------|-----------------|
| U.S. 51 Bus. & Vet. Pky - WB off ramp | | | |
| Loop | Length | Width | Number of Turns |
| D1 | 20' | 6' | 4 |
| D2 | 20' | 6' | 4 |
| D3 | 20' | 6' | 4 |

DETECTOR LOOP, TYPE 1 = 234 FT.



- PCC SURFACE REMOVAL, BUTT JOINT
- CONCRETE PAVEMENT SCARIFICATION

NOTE:
 DETECTOR LOOPS ARE ONLY TO BE INSTALLED IF
 DISTURBED BY THE PCC SURFACE REMOVAL,
 BUTT JOINT OPERATION.

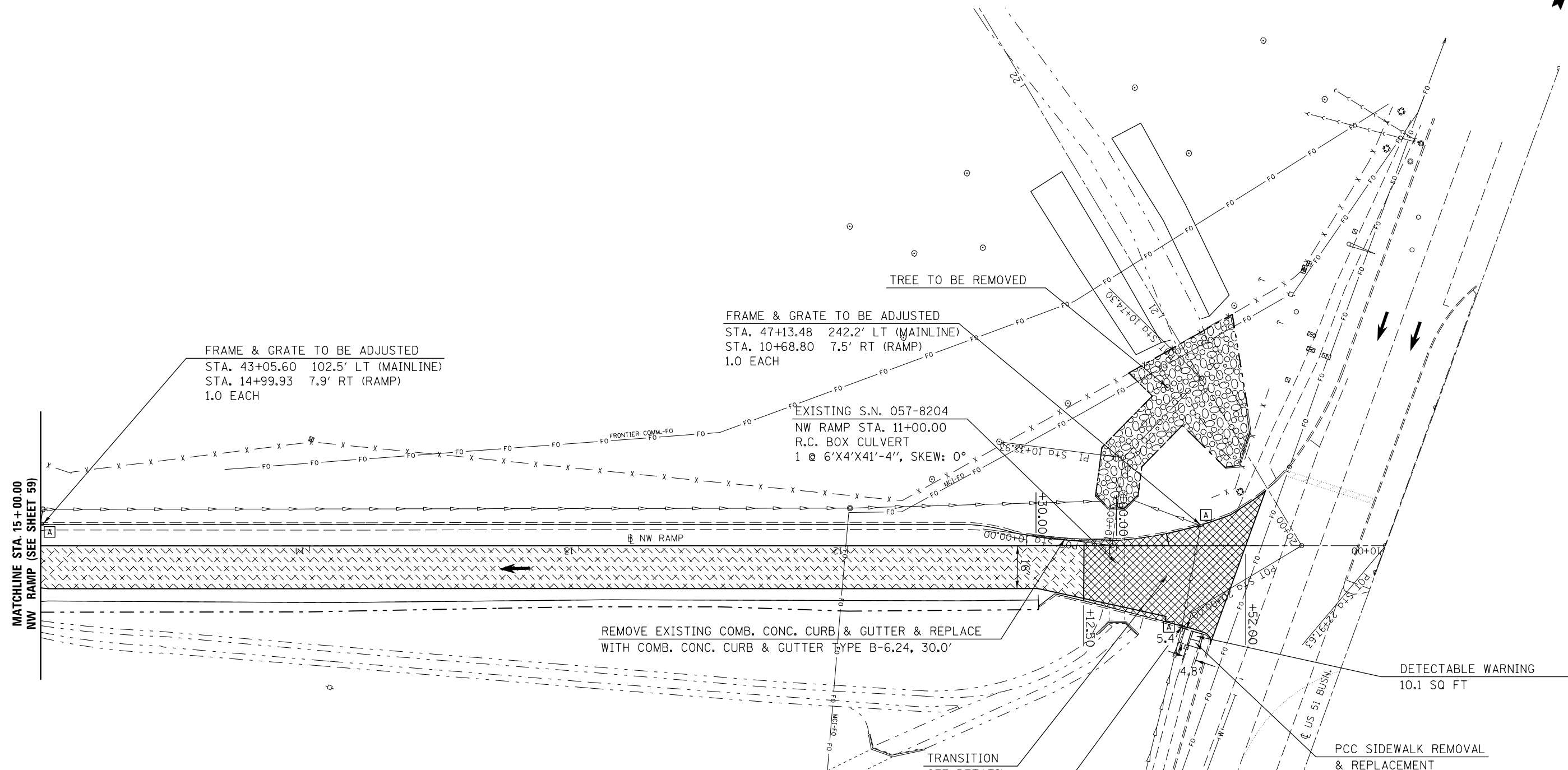
| | | | |
|--|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - |
| et:\pw\work\p\dot\dodgms\d0267571\0570838-sht\plan_Ramps.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**N.E. RAMP
 PLAN SHEET**

SCALE: 1" = 20' SHEET 2 OF 5 SHEETS STA. 19+00.00 TO STA. 22+97.63

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------------|--------|--------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 75 |
| | | | CONTRACT NO. 70838 | |
| ILLINOIS FED. AID PROJECT | | | | |



FRAME & GRATE TO BE ADJUSTED
 STA. 43+05.60 102.5' LT (MAINLINE)
 STA. 14+99.93 7.9' RT (RAMP)
 1.0 EACH

FRAME & GRATE TO BE ADJUSTED
 STA. 47+13.48 242.2' LT (MAINLINE)
 STA. 10+68.80 7.5' RT (RAMP)
 1.0 EACH

EXISTING S.N. 057-8204
 NW RAMP STA. 11+00.00
 R.C. BOX CULVERT
 1 @ 6'X4'X41'-4", SKEW: 0°

REMOVE EXISTING COMB. CONC. CURB & GUTTER & REPLACE
 WITH COMB. CONC. CURB & GUTTER TYPE B-6.24, 30.0'

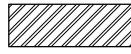

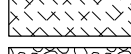

DETECTABLE WARNING
 10.1 SQ FT

PCC SIDEWALK REMOVAL
 & REPLACEMENT
 25.5 SQ FT

TRANSITION
 SEE DETAIL
 SHEET 135

FRAME & GRATE TO BE ADJUSTED
 STA. 47+19.10 204.6' LT (MAINLINE)
 STA. 10+75.66 29.8' LT (RAMP)
 1.0 EACH

NOTE: FOR CLASS A4 RIPRAP, FILTER FABRIC,
 AND TREE REMOVAL ITEMS, SEE BRIDGE REPAIR PLANS.

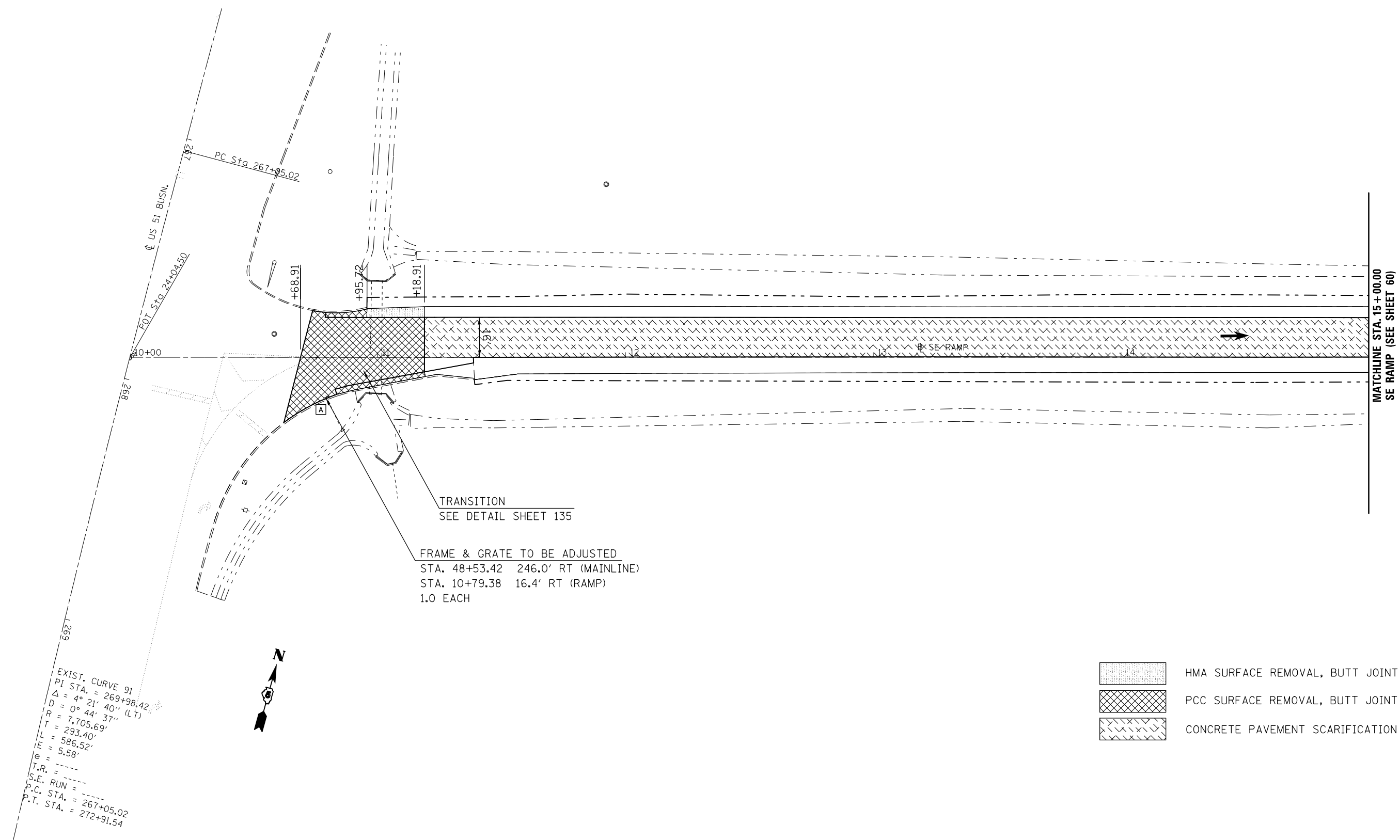
-  COMB. CONC. CURB & GUTTER REMOVAL
-  PCC SURFACE REMOVAL, BUTT JOINT
-  CONCRETE PAVEMENT SCARIFICATION
-  STONE RIPRAP, CLASS A4, 16" W/ 6" BEDDING STONE AND FILTER FABRIC (SEE SHEETS 104, 124, & 125)

| | | | |
|---|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - |
| et:\pwork\pwork\dot\dodgems\d0267571\0570838-sht\plan_Ramps.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

| | |
|--|---------------------|
| N.W. RAMP PLAN SHEET | |
| SCALE: 1" = 20' | SHEET 3 OF 5 SHEETS |
| STA. 10+00.00 | TO STA. 15+00.00 |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------------|--------|--------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 76 |
| CONTRACT NO. 70838 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



EXIST. CURVE 91
 PI STA. = 269+98.42
 $\Delta = 4^\circ 21' 40''$ (LT)
 $D = 0^\circ 44' 37''$
 $R = 7,705.69'$
 $T = 293.40'$
 $L = 586.52'$
 $e = 5.58'$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 267+05.02$
 $P.T. STA. = 272+91.54$

TRANSITION
 SEE DETAIL SHEET 135

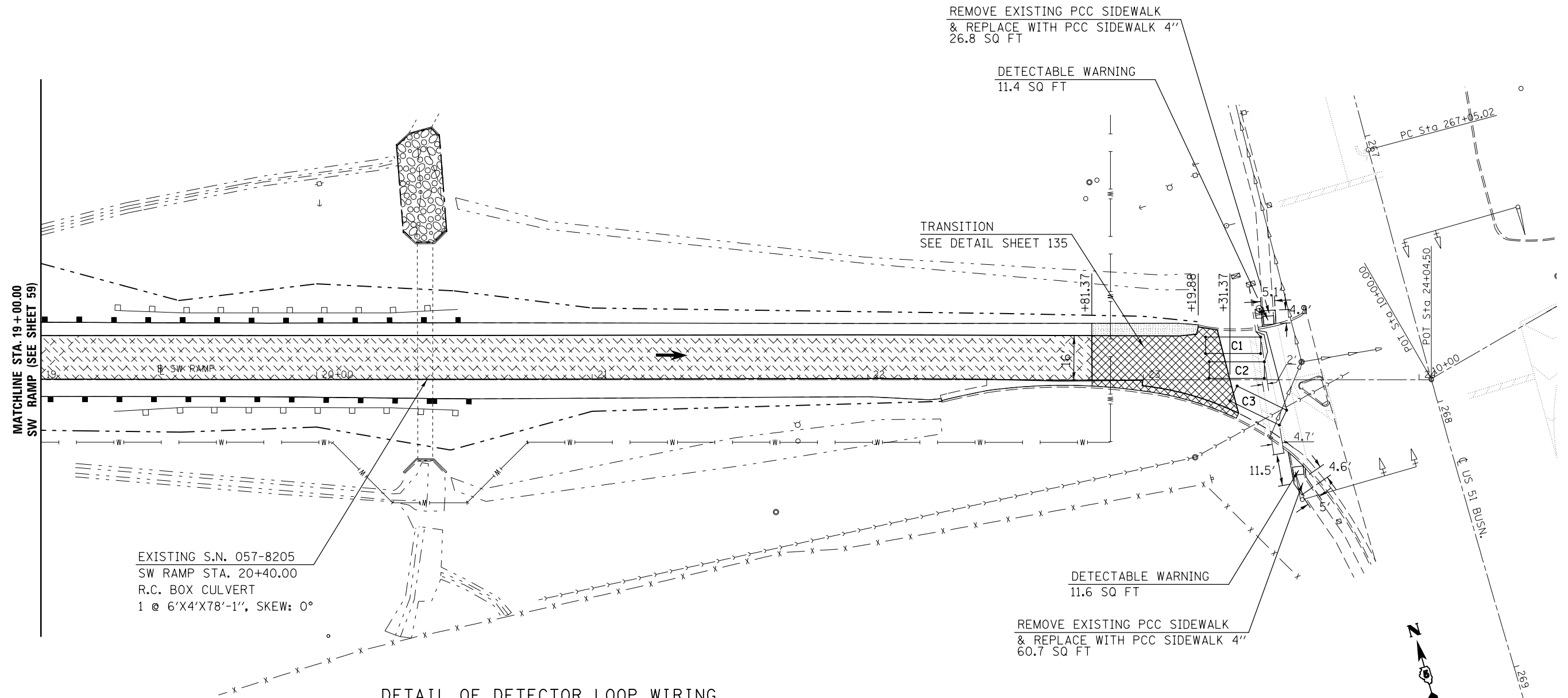
FRAME & GRATE TO BE ADJUSTED
 STA. 48+53.42 246.0' RT (MAINLINE)
 STA. 10+79.38 16.4' RT (RAMP)
 1.0 EACH

- HMA SURFACE REMOVAL, BUTT JOINT
- PCC SURFACE REMOVAL, BUTT JOINT
- CONCRETE PAVEMENT SCARIFICATION

MATCHLINE STA. 15 + 00.00
SE RAMP (SEE SHEET 60)

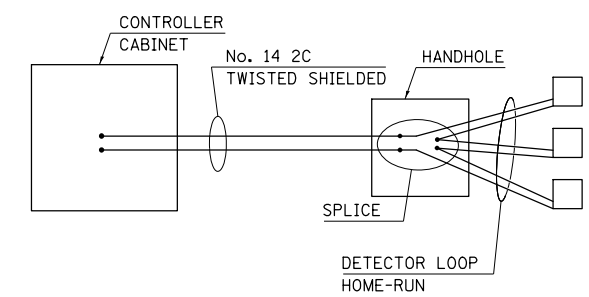
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|---|-----------------------------|------------|-----------|--|--|---------------------------|------------------|--------|--------------|-----------|--|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | S.E. RAMP PLAN SHEET | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht\plan_Ramps.dgn | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 77 | |
| PLOT DATE = 6/11/2014 | DATE - | REVISED - | REVISED - | | | CONTRACT NO. 70838 | | | | | |
| | | | | | | ILLINOIS FED. AID PROJECT | | | | | |

SCALE: 1" = 20' SHEET 4 OF 5 SHEETS STA. 10+00.00 TO STA. 15+00.00



EXISTING S.N. 057-8205
 SW RAMP STA. 20+40.00
 R.C. BOX CULVERT
 1 @ 6'X4'X78'-1", SKEW: 0°

DETAIL OF DETECTOR LOOP WIRING
 WIRED IN SERIES WITH SINGLE PAIR CABLE

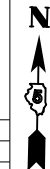


| LOOP INDUCTANCE DATA | | | |
|---------------------------------------|--------|-------|-----------------|
| U.S. 51 Bus. & Vet. Pky - EB off ramp | | | |
| Loop | Length | Width | Number of Turns |
| C1 | 20' | 6' | 3 |
| C2 | 20' | 6' | 3 |
| C3 | 20' | 6' | 3 |

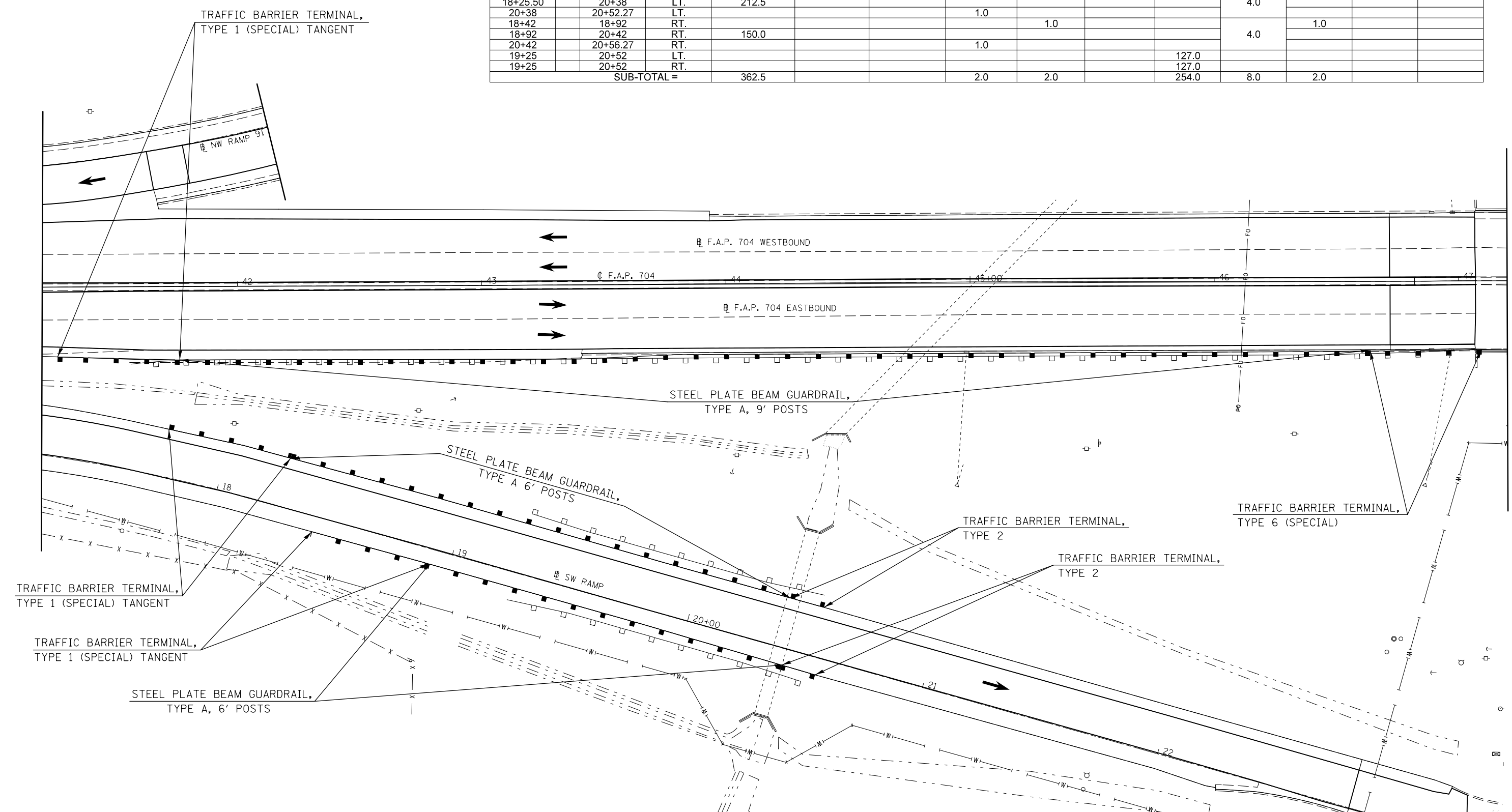
DETECTOR LOOP, TYPE 1 = 205 FT.

NOTE:
 FOR CLASS A4 RIPRAP AND FILTER FABRIC,
 SEE BRIDGE REPAIR PLANS.

- HMA SURFACE REMOVAL, BUTT JOINT
- PCC SURFACE REMOVAL, BUTT JOINT
- CONCRETE PAVEMENT SCARIFICATION
- STONE RIPRAP, CLASS A4, 16" W/ 6" BEDDING STONE AND FILTER FABRIC (SEE SHEETS 96, 126, & 127)



| STA. | TO | STA. | DIRECTION | STEEL PLATE BEAM G'RAIL TYPE A 6 FT POSTS 63000001 (FOOT) | STEEL PLATE BEAM G'RAIL TYPE A 9 FT POSTS 63000003 (FOOT) | STEEL PLATE BEAM G'RAIL ATTACH TO STRUCTURE 63000025 (FOOT) | TRAF. BAR. TERM. TY. 2 63100045 (EACH) | TRAF. BAR. TERM. TY. 1 SPL. TAN. 63100167 (EACH) | TRAF. BAR. TERM. TY. 1 SPL. FLR. 63100169 (EACH) | GUARDRAIL REMOVAL 63200310 (FOOT) | G'RAIL MKR. TY. A 78200410 (EACH) | TERMINAL MARKER D A 78201000 (EACH) | TRAF. BAR. TERM. TY. 5 SPL. X6310179 (EACH) | TRAF. BAR. TERM. TY. 6 SPL. X6310214 (EACH) |
|-------------|----|----------|-----------|---|---|---|--|--|--|-----------------------------------|-----------------------------------|-------------------------------------|---|---|
| LEG "C": | | | | | | | | | | | | | | |
| 41+26.17 | | 41+76.17 | EB | | | | | 1.0 | | | | 1.0 | | |
| 41+76.17 | | 46+63.67 | EB | | 487.5 | | | | | | 5.0 | | | |
| 46+63.67 | | 47+09.32 | EB | | | | | | | | | | | 1.0 |
| 41+56 | | 47+27 | EB | | | | | | | 571.0 | | | | |
| SUB-TOTAL = | | | | | 487.5 | | | 1.0 | | 571.0 | 5.0 | 1.0 | | 1.0 |
| SW RAMP: | | | | | | | | | | | | | | |
| 17+75.50 | | 18+25.50 | LT. | | | | | 1.0 | | | | 1.0 | | |
| 18+25.50 | | 20+38 | LT. | 212.5 | | | | | | | 4.0 | | | |
| 20+38 | | 20+52.27 | LT. | | | | 1.0 | | | | | | | |
| 18+42 | | 18+92 | RT. | | | | | 1.0 | | | | 1.0 | | |
| 18+92 | | 20+42 | RT. | 150.0 | | | | | | | 4.0 | | | |
| 20+42 | | 20+56.27 | RT. | | | | 1.0 | | | | | | | |
| 19+25 | | 20+52 | LT. | | | | | | | 127.0 | | | | |
| 19+25 | | 20+52 | RT. | | | | | | | 127.0 | | | | |
| SUB-TOTAL = | | | | 362.5 | | | 2.0 | 2.0 | | 254.0 | 8.0 | 2.0 | | |

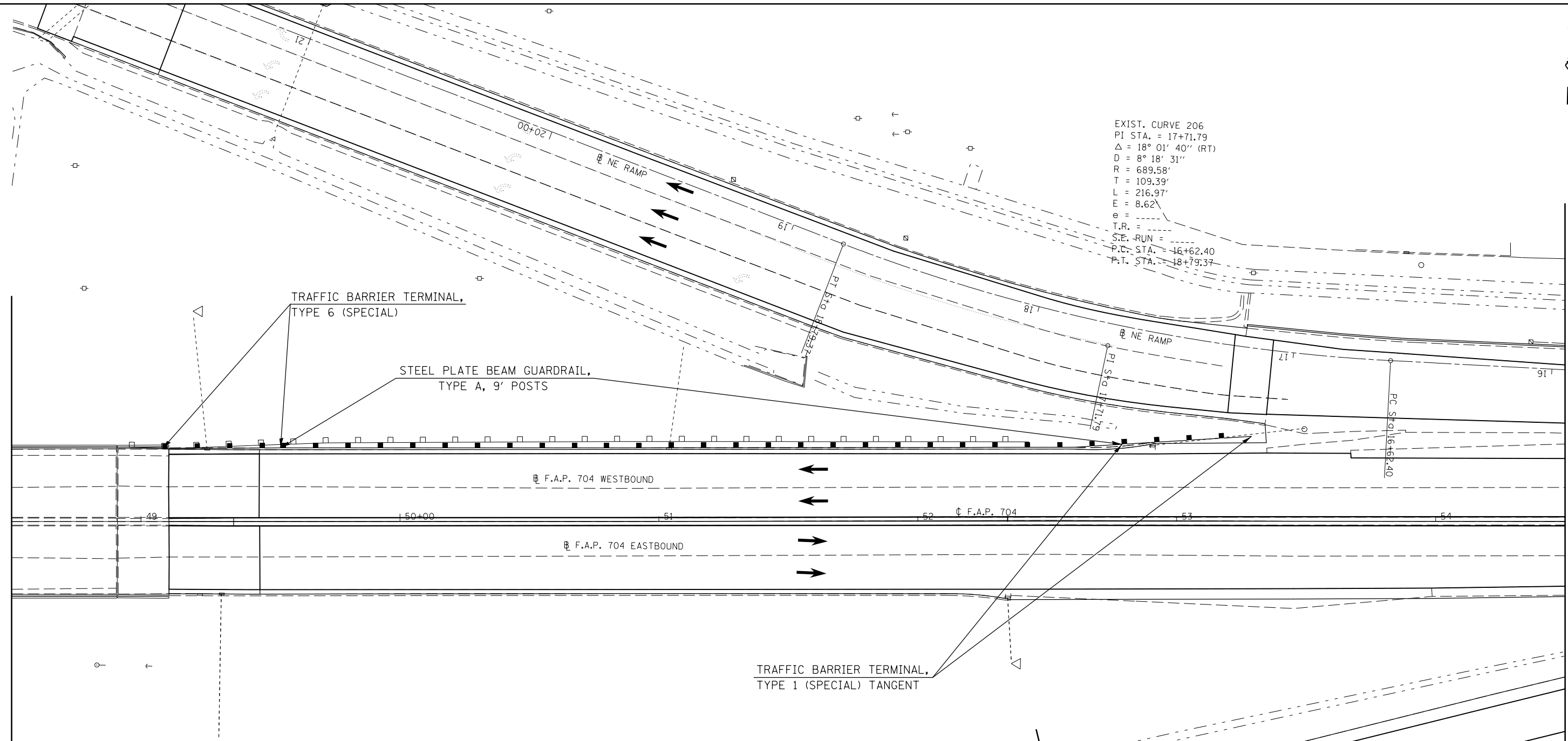


| | | | | | | | | | | |
|---|---------------------|------------|---------------------------|---|-----------------------------|------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GUARDRAIL PLAN SHEET | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht\plan_BUS55.dgn | DRAWN - | REVISED - | 704 | | | (57-7)RS-1&1RS-3 | McLean | 273 | 79 | |
| PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | CONTRACT NO. 70838 | | | | | | | |
| PLOT DATE = 6/11/2014 | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |

SCALE: 1" = 20' SHEET 1 OF 7 SHEETS STA. 41+20.00 TO STA. 47+20.00



EXIST. CURVE 206
 PI STA. = 17+71.79
 $\Delta = 18^\circ 01' 40''$ (RT)
 $D = 8^\circ 18' 31''$
 $R = 689.58'$
 $T = 109.39'$
 $L = 216.97'$
 $E = 8.62'$
 $e =$
 T.R. =
 S.E. RUN =
 P.C. STA. = 16+62.40
 P.T. STA. = 18+79.37



| STA. | TO | STA. | DIRECTION | STEEL PLATE BEAM G'RAIL TYPE A 9 FT POSTS 63000003 (FOOT) | TRAF. BAR. TERM. TY. 1 SPL. TAN. 63100167 (EACH) | GUARDRAIL REMOVAL 63200310 (FOOT) | G'RAIL MKR. TY. A 78200410 (EACH) | TERMINAL MARKER D A 78201000 (EACH) | TRAF. BAR. TERM. TY. 6 SPL. X6310214 (EACH) |
|-------------|----|----------|-----------|---|--|-----------------------------------|-----------------------------------|-------------------------------------|---|
| LEG "C": | | | | | | | | | |
| 49+08.24 | | 49+53.89 | WB | | | | 5.0 | | 1.0 |
| 49+53.89 | | 52+78.89 | WB | 325.0 | | | | | |
| 52+78.89 | | 53+28.89 | WB | | 1.0 | | | 1.0 | |
| 48+90 | | 52+43 | WB | | | 353.0 | | | |
| SUB-TOTAL = | | | | 325.0 | 1.0 | 353.0 | 5.0 | 1.0 | 1.0 |

| | | | |
|---|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

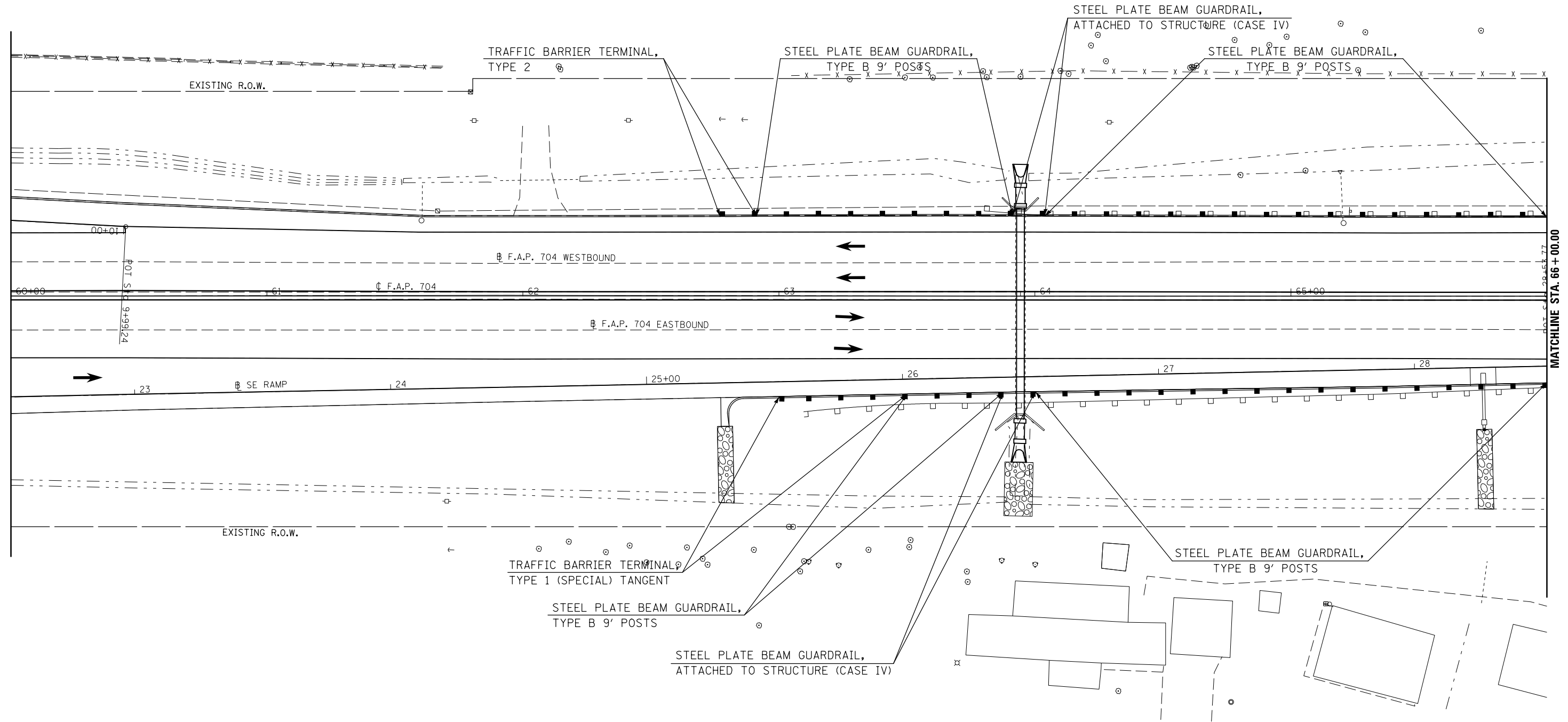
GUARDRAIL PLAN SHEET

SCALE: 1" = 20' SHEET 2 OF 7 SHEETS STA. 48+50.00 TO STA. 54+50.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 80 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



| STA. | TO | STA. | DIRECTION | STEEL PLATE BEAM GUARD RAIL ATTACH TO STRUCTURE 63000025 (FOOT) | TRAF. BAR. TERM. TY. 2 63100045 (EACH) | TRAF. BAR. TERM. TY. 1 SPL. TAN. 63100167 (EACH) | GUARDRAIL REMOVAL 63200310 (FOOT) | TERMINAL MARKER D A 78201000 (EACH) |
|-------------|----|----------|-----------|---|--|--|-----------------------------------|-------------------------------------|
| LEG "C": | | | | | | | | |
| 62+77.02 | | 62+91.29 | WB | | 1.0 | | | |
| 63+00.16 | | 63+50.16 | EB | | | 1.0 | | 1.0 |
| 63+10 | | 73+20 | EB | | | | 1010.0 | |
| 63+80 | | 72+70 | WB | | | | 890.0 | |
| 63+87.66 | | 64+00.16 | EB | 12.5 | | | | |
| 63+91.29 | | 64+03.79 | WB | 12.5 | | | | |
| SUB-TOTAL = | | | | 25.0 | 1.0 | 1.0 | 1900.0 | 1.0 |

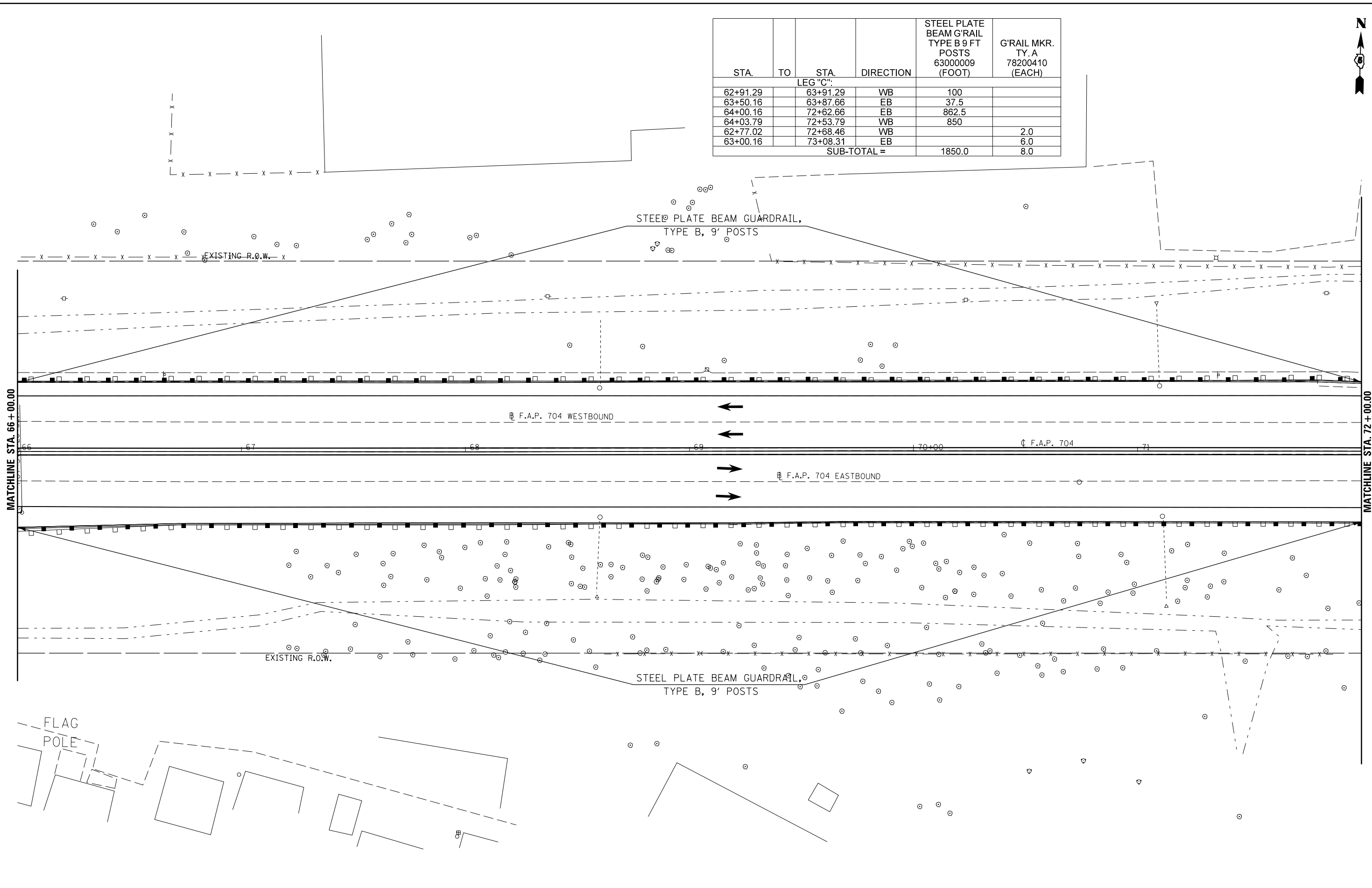


MATCHLINE STA. 66+00.00

| | | | | | | | | | | | |
|---|---------------------|------------|-----------|---|-----------------------------|---|------------------|---------------------|--------------|--------------------------------|--|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GUARDRAIL PLAN SHEET | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - | | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 81 | |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - | | | SCALE: 1" = 20' | | SHEET 3 OF 7 SHEETS | | STA. 60+00.00 TO STA. 66+00.00 | |
| PLOT DATE = 6/11/2014 | | DATE - | REVISED - | | | CONTRACT NO. 70838 ILLINOIS FED. AID PROJECT | | | | | |



| STA. | TO | STA. | DIRECTION | STEEL PLATE BEAM G'RAIL TYPE B 9 FT POSTS 63000009 (FOOT) | G'RAIL MKR. TY. A 78200410 (EACH) |
|-------------|----|----------|-----------|---|-----------------------------------|
| LEG "C": | | | | | |
| 62+91.29 | | 63+91.29 | WB | 100 | |
| 63+50.16 | | 63+87.66 | EB | 37.5 | |
| 64+00.16 | | 72+62.66 | EB | 862.5 | |
| 64+03.79 | | 72+53.79 | WB | 850 | |
| 62+77.02 | | 72+68.46 | WB | | 2.0 |
| 63+00.16 | | 73+08.31 | EB | | 6.0 |
| SUB-TOTAL = | | | | 1850.0 | 8.0 |



| | | | |
|---|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

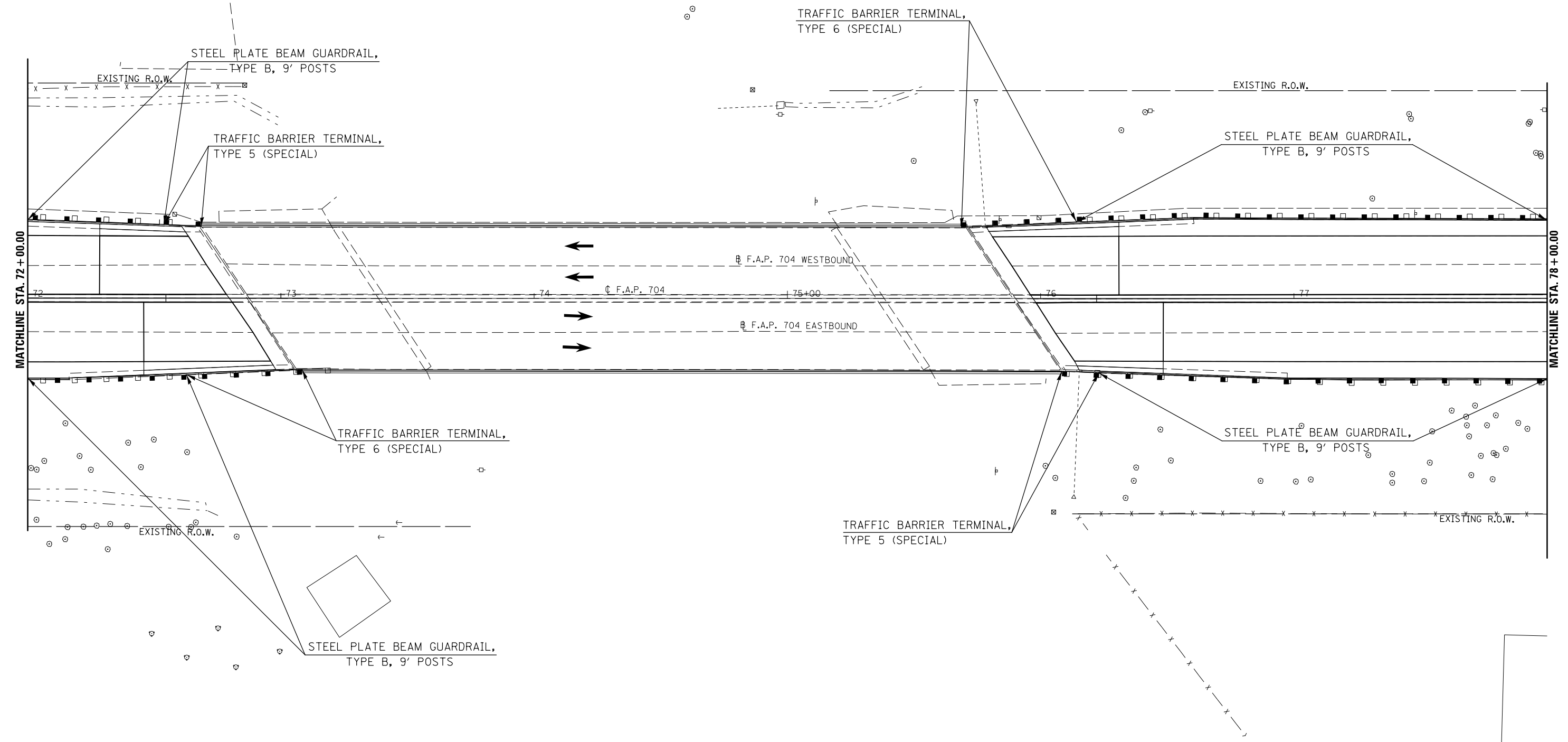
GUARDRAIL PLAN SHEET

SCALE: 1" = 20' SHEET 4 OF 7 SHEETS STA. 66+00.00 TO STA. 72+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 82 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



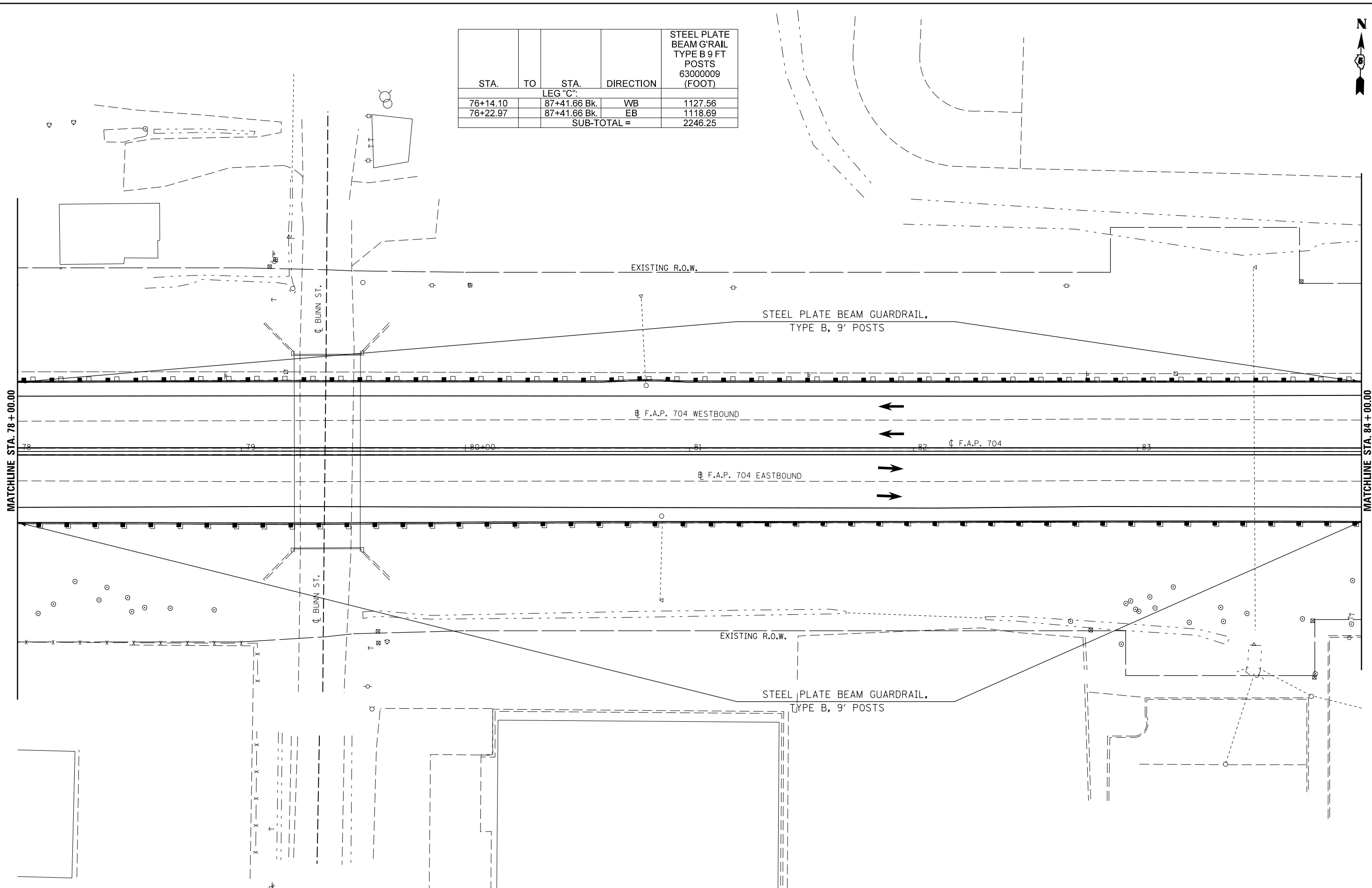
| STA. | TO | STA. | DIRECTION | GUARDRAIL REMOVAL 63200310 (FOOT) | BARRIER WAL MARKER TYPE C 78200530 (EACH) | TRAF. BAR. TERM. TY. 5 SPL. X6310179 (EACH) | TRAF. BAR. TERM. TY. 6 SPL. X6310214 (EACH) |
|-------------|----|--------------|-----------|--|---|---|---|
| LEG "C": | | | | | | | |
| 73+26.60 | | | WB | | 1.0 | | |
| 75+00.16 | | | EB | | 1.0 | | |
| 75+68 | | 87+41.66 Bk. | WB | 1173.66 | | | |
| 76+10 | | 87+41.66 Bk. | EB | 1131.66 | | | |
| 72+53.79 | | 72+68.46 | WB | | | 1.0 | |
| 72+62.66 | | 73+08.31 | EB | | | | 1.0 |
| 76+08.30 | | 76+22.97 | EB | | | 1.0 | |
| 75+68.45 | | 76+14.10 | WB | | | | 1.0 |
| SUB-TOTAL = | | | | 2305.32 | 2.0 | 2.0 | 2.0 |



| | | | | | | | | | | | | | | |
|---|-----------------------------|------------|-----------|---|-----------------------------|-----------------|--------|---------------------|---------|--------|--------------------------------|--------------|--------------------|--|
| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GUARDRAIL PLAN SHEET | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | | |
| c:\pwork\pwork\dot\dodgms\d0267571\0570838-sht-plan_BUS55.dgn | | DRAWN - | REVISED - | | 704 | (57-7RS-1&1RS-3 | McLean | 273 | 83 | | | | | |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | SCALE: 1" = 20' | | | SHEET 5 OF 7 SHEETS | | | STA. 72+00.00 TO STA. 78+00.00 | | CONTRACT NO. 70838 | |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | | | |



| STA. | TO | STA. | DIRECTION | STEEL PLATE BEAM G'RAIL TYPE B 9 FT POSTS 63000009 (FOOT) |
|-------------|----|--------------|-----------|---|
| LEG "C": | | | | |
| 76+14.10 | | 87+41.66 Bk. | WB | 1127.56 |
| 76+22.97 | | 87+41.66 Bk. | EB | 1118.69 |
| SUB-TOTAL = | | | | 2246.25 |



| | | | |
|---|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht\plan_BU555.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

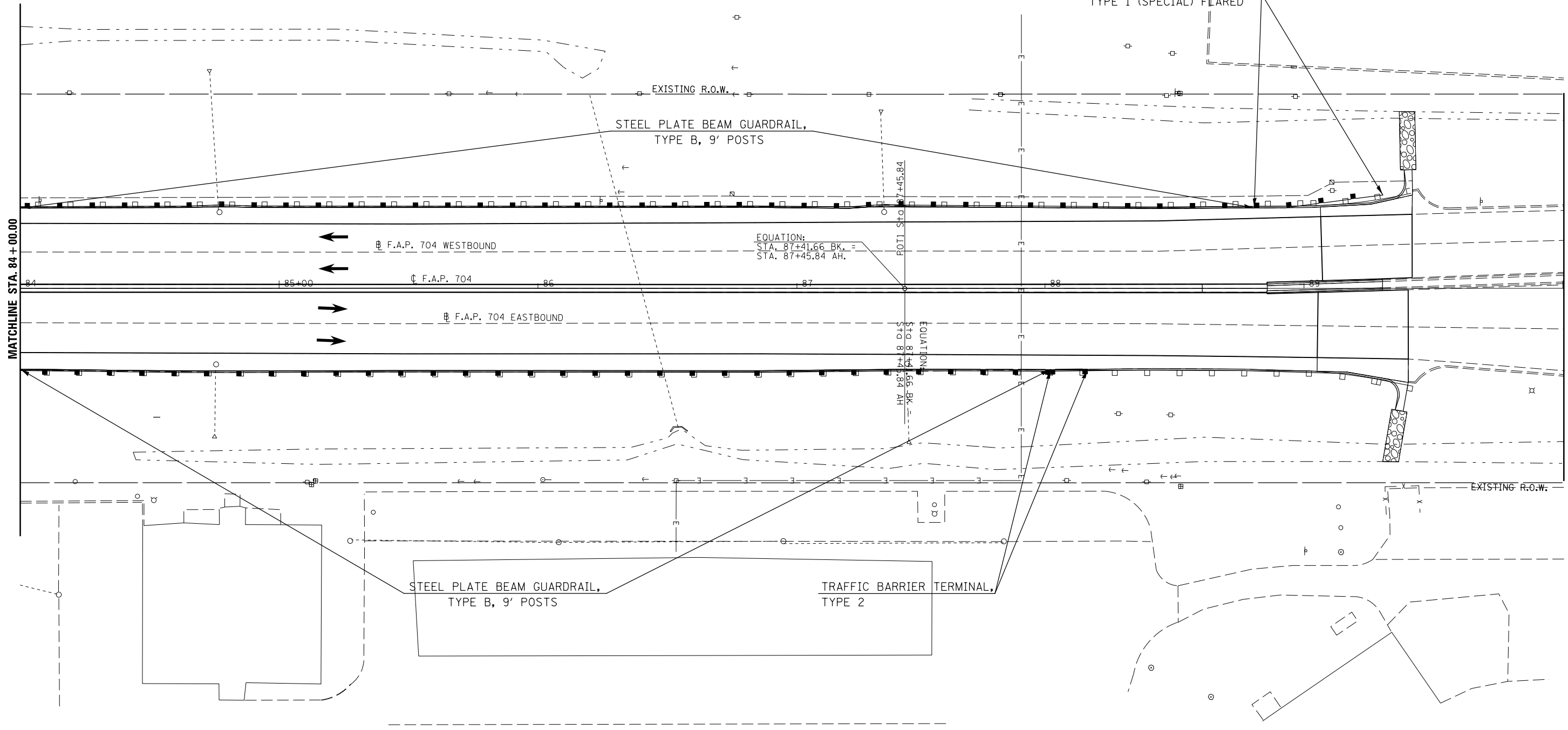
GUARDRAIL PLAN SHEET

SCALE: 1" = 20' SHEET 6 OF 7 SHEETS STA. 78+00.00 TO STA. 84+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------------|--------|--------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 84 |
| CONTRACT NO. 70838 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



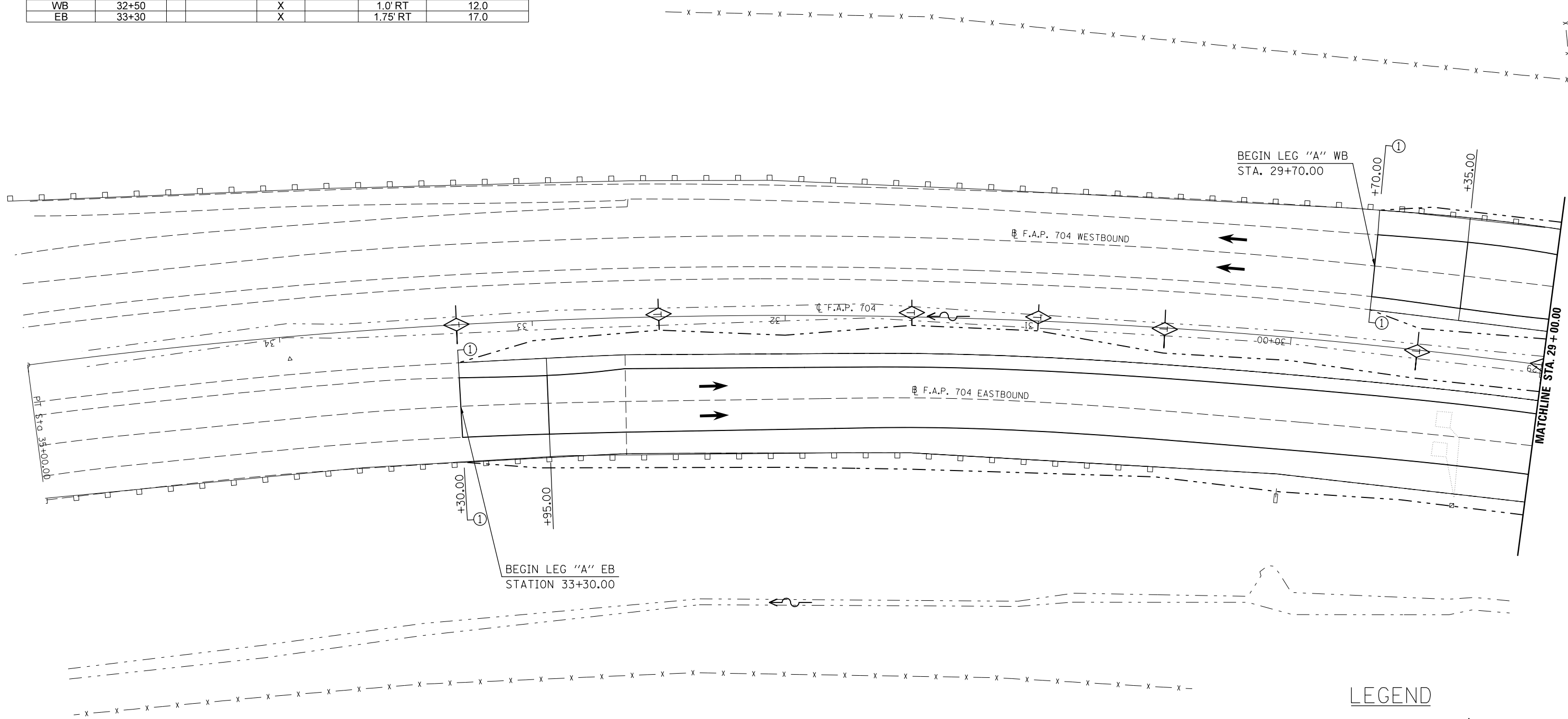
| STA. | TO | STA. | DIRECTION | STEEL PLATE BEAM G'RAIL TYPE B 9 FT POSTS 63000009 (FOOT) | TRAF. BAR. TERM. TY. 2 63100045 (EACH) | TRAF. BAR. TERM. TY. 1 SPL. FLR. 63100169 (EACH) | GUARDRAIL REMOVAL 63200310 (FOOT) | G'RAIL MKR. TY. A 78200410 (EACH) | TERMINAL MARKER D A 78201000 (EACH) |
|--------------|----|--------------|-----------|---|--|--|-----------------------------------|-----------------------------------|-------------------------------------|
| LEG "C": | | | | | | | | | |
| 87+45.84 Ah. | | 88+02.15 | EB | 56.31 | | | | | |
| 87+45.84 Ah. | | 88+80.78 | WB | 134.94 | | | | | |
| 88+02.15 | | 88+16.42 | EB | | 1.0 | | | | |
| 88+80.78 | | 89+30.65 | WB | | | 1.0 | | | 1.0 |
| 87+45.84 Ah. | | 89+41.39 | WB | | | | 195.55 | | |
| 87+45.84 Ah. | | 89+42.39 | EB | | | | 196.55 | | |
| 76+08.30 | | 87+41.66 Bk. | EB | | | | | 3.0 | |
| 87+45.84 Ah. | | 88+16.42 | EB | | | | | | |
| 75+68.45 | | 87+41.66 Bk. | WB | | | | | 7.0 | |
| 87+45.84 Ah. | | 89+30.65 | WB | | | | | | |
| SUB-TOTAL = | | | | 191.25 | 1.0 | 1.0 | 392.1 | 10.0 | 1.0 |



| | | | | | | | | | | |
|---|---------------------|------------|-----------|---|-----------------------------|--------------------|---------------------|---------------------------------|---------------------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GUARDRAIL PLAN SHEET | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - | | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 85 |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - | | | CONTRACT NO. 70838 | | | | |
| PLOT DATE = 6/11/2014 | | DATE - | REVISED - | | | SCALE: 1" = 20' | SHEET 7 OF 7 SHEETS | STA. 84+00.00 TO STA. 106+41.40 | ILLINOIS FED. AID PROJECT | |



| DIRECTION | LOCATION: LEG "A" | | | MEDIAN | OUTSIDE | OFFSET | TEMPORARY DITCH CHECKS 28000305 (FOOT) |
|-----------|----------------------|----|------|--------|---------|----------|---|
| | STA. | TO | STA. | | | | |
| EB | 29+00 | | | X | | CL | 15.0 |
| EB | 29+50 | | | X | | 0.5' LT | 15.0 |
| EB | 30+50 | | | X | | 0.5' LT | 15.0 |
| EB | 31+00 | | | X | | CL | 10.0 |
| WB | 31+50 | | | X | | 1.0' RT | 10.0 |
| WB | 32+50 | | | X | | 1.0' RT | 12.0 |
| EB | 33+30 | | | X | | 1.75' RT | 17.0 |



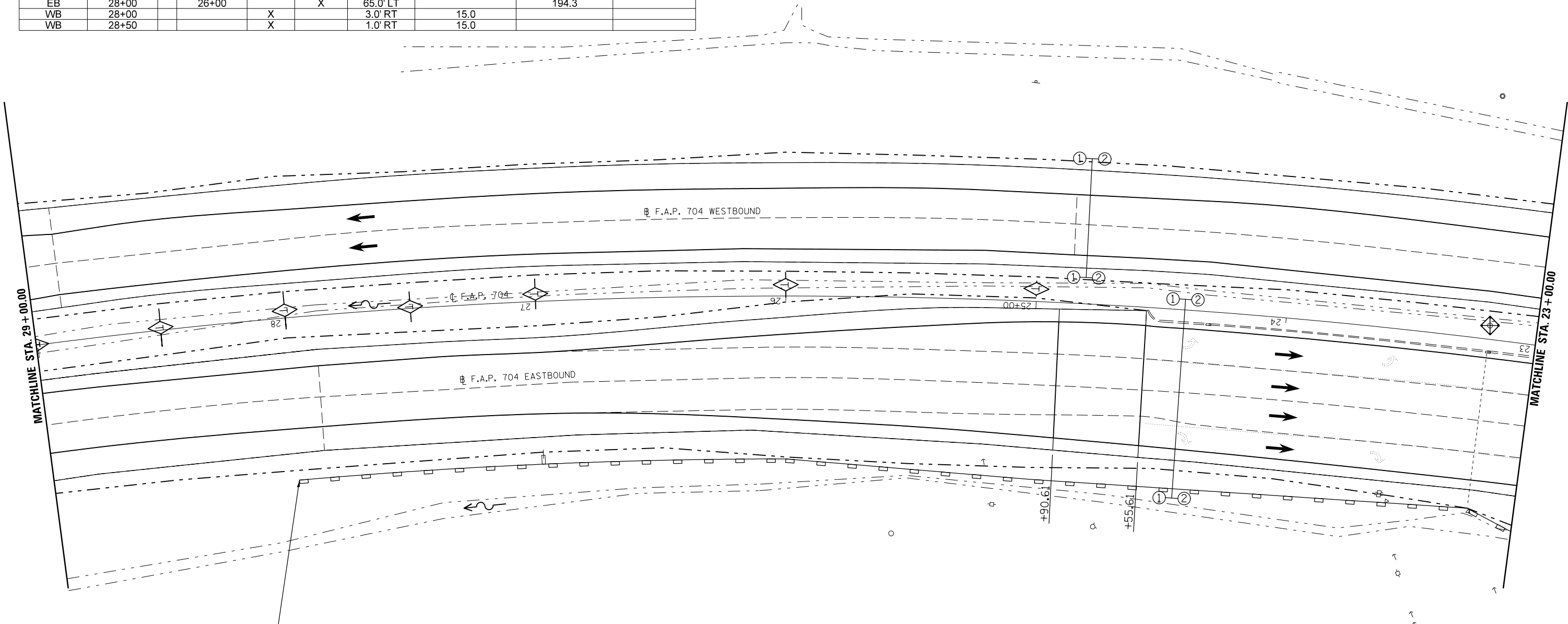
LEGEND

- TEMPORARY DITCH CHECK
- PERIMETER EROSION BARRIER
- INLET AND PIPE PROTECTION
- CONSTRUCTION LIMITS

| | | | | | | | | | | | |
|--|-----------------------------|------------|-----------|---|-------------------------------------|--|--------------------|----------------------|--------------------------------|---------------------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | BUS 55 EROSION CONTROL SHEET | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pw\work\p\wdot\dodgems\d0267571\0570838-sht\plan_BUS55.dgn | PLOT SCALE = 40.0000' / in. | DRAWN - | REVISED - | | | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 86 |
| | PLOT DATE = 6/11/2014 | CHECKED - | REVISED - | | | | CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |
| | | DATE - | REVISED - | | | | SCALE: 1" = 20' | SHEET 1 OF 20 SHEETS | STA. 33+30.00 TO STA. 29+00.00 | | |



| DIRECTION | LOCATION: LEG "A" | | | MEDIAN | OUTSIDE | OFFSET | TEMPORARY DITCH CHECKS 28000305 (FOOT) | PERIMETER EROSION BARRIER 28000400 (FOOT) | INLET & PIPE PROTECTION 28000500 (EACH) |
|-----------|----------------------|----|-------|--------|---------|------------|---|--|--|
| | STA. | TO | STA. | | | | | | |
| WB | 23+19 | | | X | | 5.0' RT. | | | 1.0 |
| EB | 23+19 | | 23+00 | | X | 68'-75' | | 19.7 | |
| WB | 25+00 | | | X | | 6.0' RT | 6.0 | | |
| EB | 25+00 | | 23+19 | | X | 71'-68' LT | | 175.5 | |
| EB | 26+00 | | 25+00 | | X | 65'-71' LT | | 97.2 | |
| WB | 26+00 | | | X | | 4.5' RT | 10.0 | | |
| WB | 27+00 | | | X | | 3.0' RT | 12.0 | | |
| WB | 27+50 | | | X | | 2.5' RT | 12.0 | | |
| EB | 28+00 | | 26+00 | | X | 65.0' LT | | 194.3 | |
| WB | 28+00 | | | X | | 3.0' RT | 15.0 | | |
| WB | 28+50 | | | X | | 1.0' RT | 15.0 | | |



BEGIN PERIMETER EROSION BARRIER
STA. 28+00

LEGEND

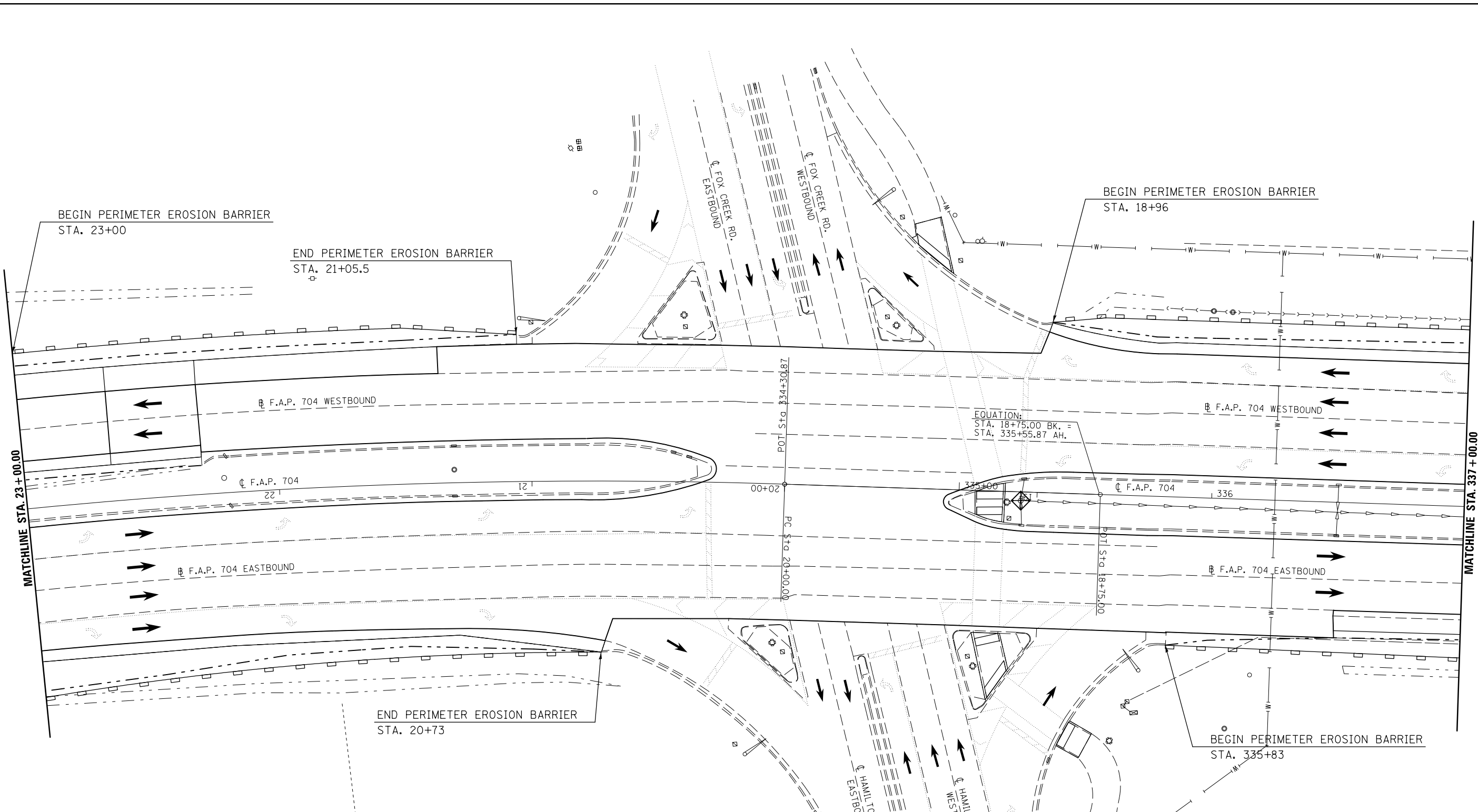
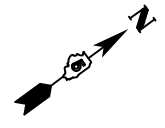
- TEMPORARY DITCH CHECK
- EROSION CONTROL BARRIER
- INLET AND PIPE PROTECTION
- CONSTRUCTION LIMITS

| | | | |
|--|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - |
| et:\pw\work\p1dot\dodgems\d0267571\0570838-sht.pln_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUS 55 EROSION CONTROL SHEET
SCALE: 1" = 20' SHEET 2 OF 20 SHEETS STA. 29+00.00 TO STA. 23+00.00

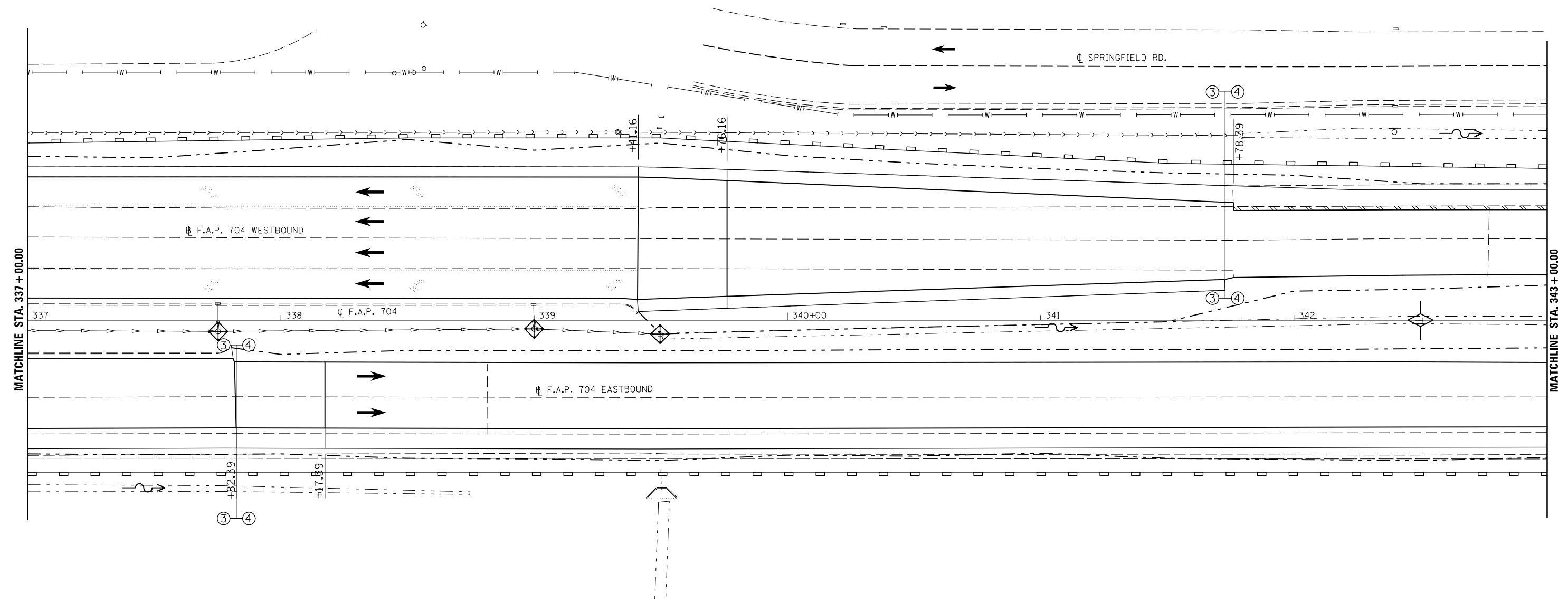
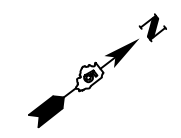
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 87 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



LEGEND

- TEMPORARY DITCH CHECK
- EROSION CONTROL BARRIER
- INLET AND PIPE PROTECTION
- CONSTRUCTION LIMITS

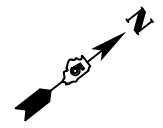
| DIRECTION | LOCATION: LEG "A" | | | MEDIAN | OUTSIDE | OFFSET | PERIMETER EROSION BARRIER 28000400 (FOOT) | INLET & PIPE PROTECTION 28000500 (EACH) |
|-----------|----------------------|----|--------------|--------|---------|--------------|--|--|
| | STA. | TO | STA. | | | | | |
| WB | 23+00 | | 21+50 | | X | 62.0' RT | 154.0 | |
| WB | 21+50 | | 21+05.5 | | X | 62'-58' RT | 45.9 | |
| WB | 18+96 | | 18+75.00 BK. | | X | 68'-70' RT | 21.1 | |
| WB | 335+55.87 AH. | | 337+00 | | X | 70' LT | 144.1 | |
| EB | 23+00 | | 20+73 | | X | 75'-68' LT | 220.6 | |
| EB | 19+06.39 | | | X | | 3.44' LT | | 1.0 |
| EB | 335+83 | | 337+00 | | X | 58.5'-60' RT | 116.5 | |



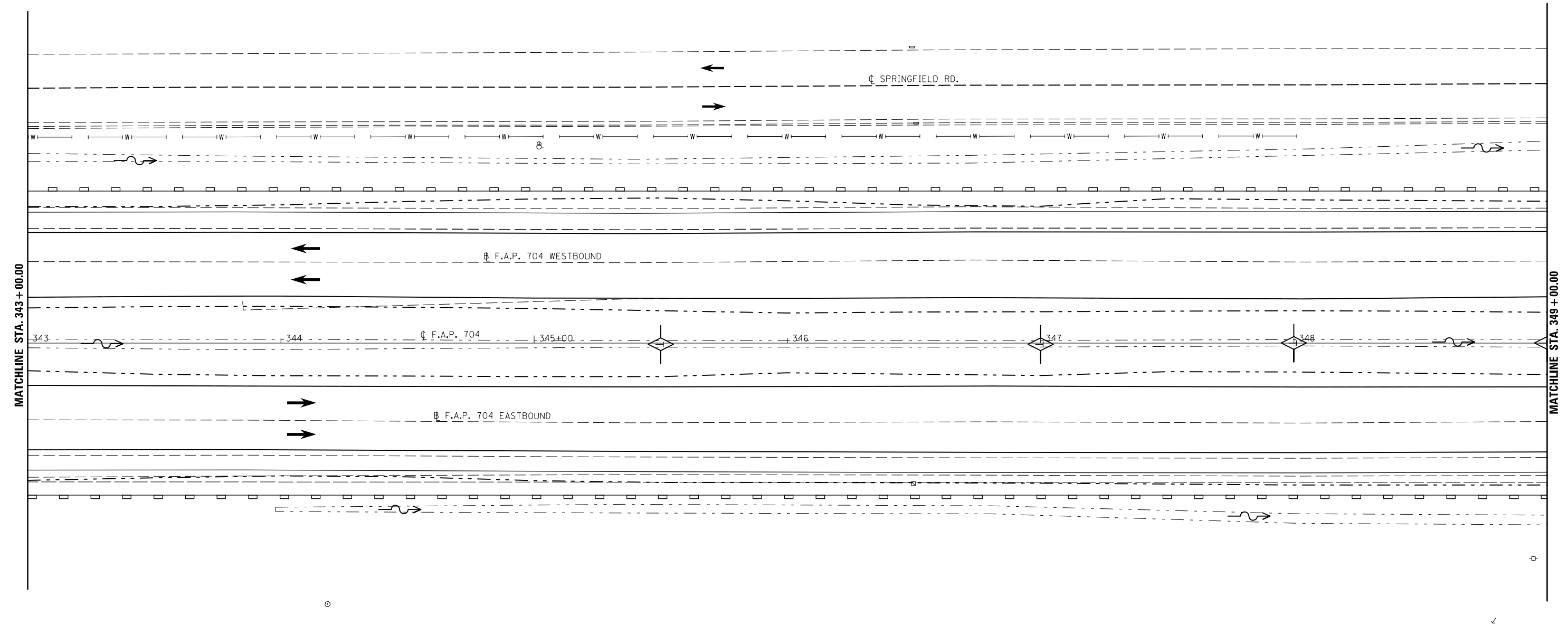
| DIRECTION | LOCATION: LEG "A" | | MEDIAN | OUTSIDE | OFFSET | TEMPORARY DITCH CHECKS 28000305 (FOOT) | PERIMETER EROSION BARRIER 28000400 (FOOT) | INLET & PIPE PROTECTION 28000500 (EACH) |
|-----------|----------------------|---------|--------|---------|------------|---|--|--|
| | STA. | TO STA. | | | | | | |
| EB | 337+00 | 343+00 | | X | 60.0' RT | | 600.0 | |
| WB | 337+00 | 338+50 | | | 70'-72' LT | | 150.0 | |
| WB | 338+50 | 339+50 | | X | 72.0' LT | | 100.0 | |
| EB | 337+75 | | X | | 4.0' RT. | | | 1.0 |
| EB | 339+00 | | X | | 3.0' RT. | | | 1.0 |
| EB | 339+50 | | X | | 5.0' RT. | | | 1.0 |
| WB | 339+50 | 341+50 | | X | 72'-62' LT | | 200.2 | |
| WB | 341+50 | 343+00 | | X | 62'-60' LT | | 150.0 | |
| EB | 342+50 | | X | | CL | 15.0 | | |

LEGEND

- TEMPORARY DITCH CHECK
- EROSION CONTROL BARRIER
- INLET AND PIPE PROTECTION
- CONSTRUCTION LIMITS

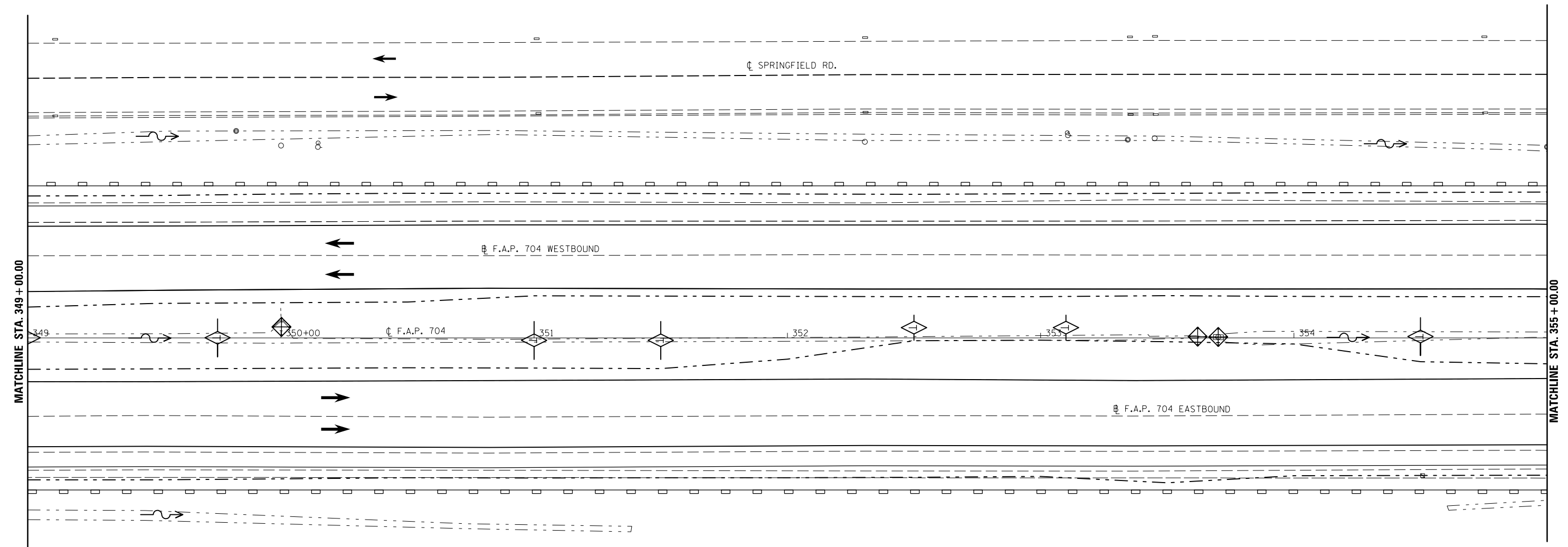
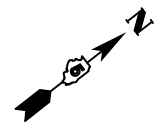


| DIRECTION | LOCATION: LEG "A" | | MEDIAN | OUTSIDE | OFFSET | TEMPORARY DITCH CHECKS 28000305 (FOOT) | PERIMETER EROSION BARRIER 28000400 (FOOT) |
|-----------|----------------------|---------|--------|---------|----------|---|--|
| | STA. | TO STA. | | | | | |
| EB | 343+00 | 349+00 | | X | 60.0' RT | | 600.0 |
| WB | 343+00 | 349+00 | | X | 60.0' LT | | 600.0 |
| EB | 345+50 | | X | | 0.5' RT | 15.0 | |
| EB | 347+00 | | X | | 0.5' RT | 15.0 | |
| EB | 348+00 | | X | | CL | 15.0 | |
| EB | 349+00 | | X | | CL | 15.0 | |

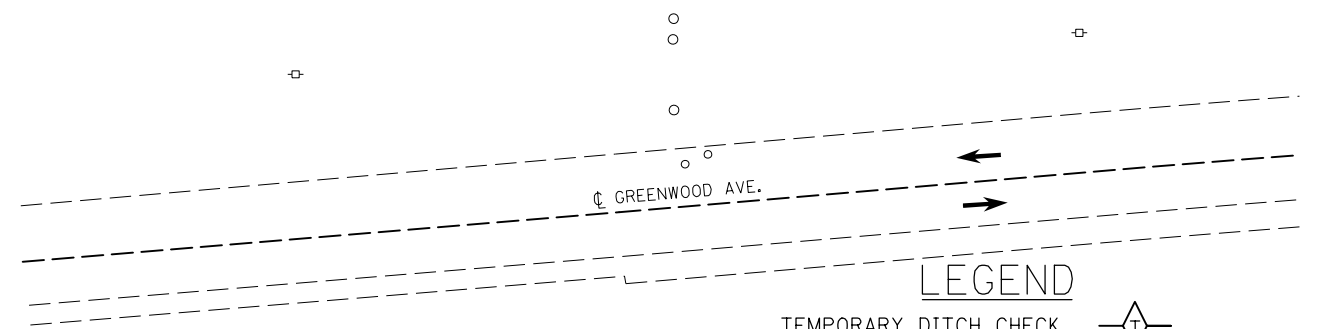


LEGEND

- TEMPORARY DITCH CHECK
- PERIMETER EROSION BARRIER
- INLET AND PIPE PROTECTION
- CONSTRUCTION LIMITS

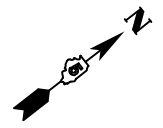


| DIRECTION | LOCATION: LEG "A" | | | MEDIAN | OUTSIDE | OFFSET | TEMPORARY DITCH CHECKS 28000305 (FOOT) | PERIMETER EROSION BARRIER 28000400 (FOOT) | INLET & PIPE PROTECTION 28000500 (EACH) |
|-----------|----------------------|----|--------|--------|---------|----------|---|--|--|
| | STA. | TO | STA. | | | | | | |
| EB | 349+00 | | 355+00 | | X | 60.0' RT | | 600.0 | |
| WB | 349+00 | | 355+00 | | X | 60.0' LT | | 600.0 | |
| EB | 349+75 | | | X | | CL | 15.0 | | |
| WB | 350+00 | | | X | | 6' LT | | | 1.0 |
| EB | 351+00 | | | X | | 1' RT | 15.0 | | |
| EB | 351+50 | | | X | | 0.75' RT | 15.0 | | |
| WB | 352+50 | | | X | | 4.0' LT | 10.0 | | |
| WB | 353+10 | | | X | | 4.0' LT | 10.0 | | |
| WB | 353+62 | | | X | | 1.0' LT | | | 1.0 |
| WB | 353+68 | | | X | | 1.0' LT | | | 1.0 |
| WB | 354+50 | | | X | | 0.5' LT | 15.0 | | |

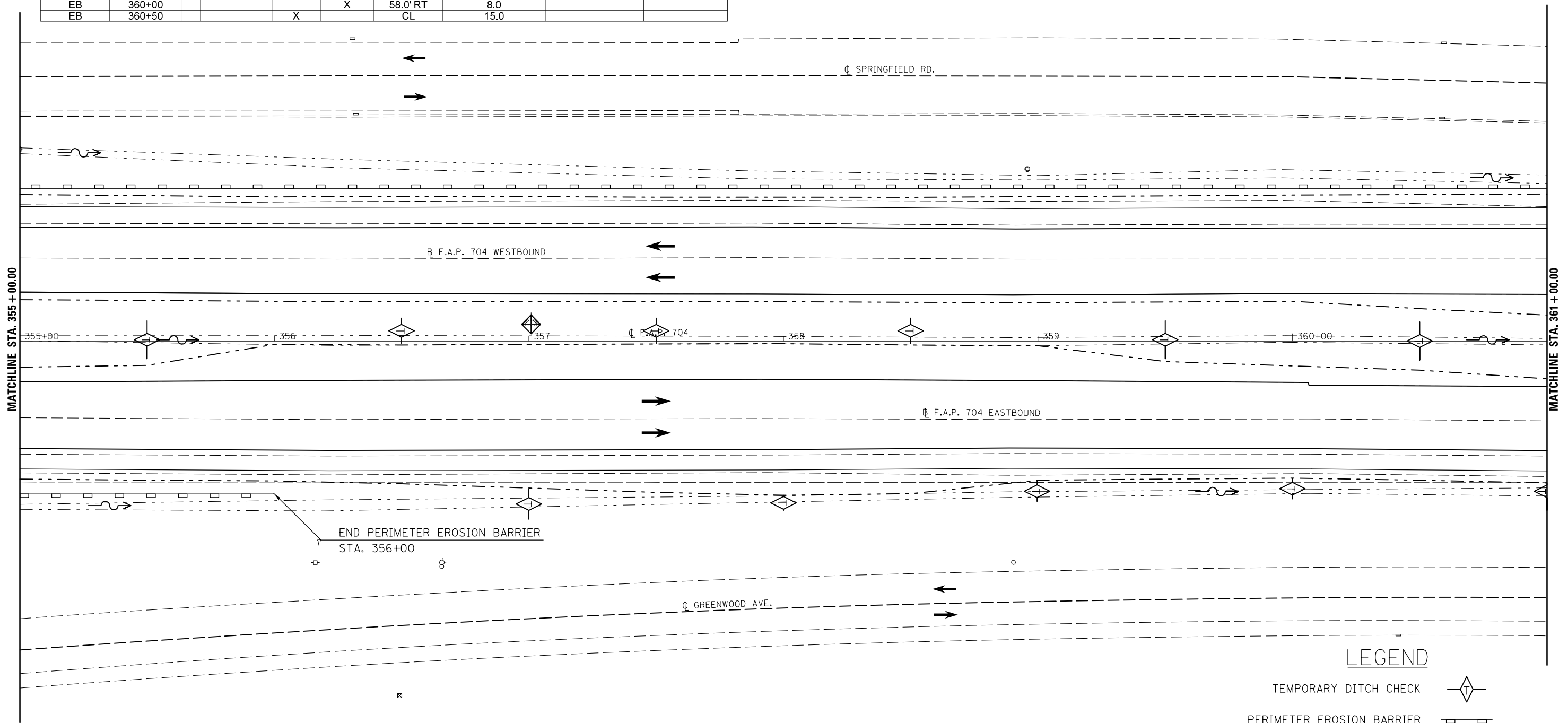


LEGEND

- TEMPORARY DITCH CHECK
- PERIMETER EROSION BARRIER
- INLET AND PIPE PROTECTION
- CONSTRUCTION LIMITS



| DIRECTION | LOCATION: LEG "A" | | | MEDIAN | OUTSIDE | OFFSET | TEMPORARY DITCH CHECKS 28000305 (FOOT) | PERIMETER EROSION BARRIER 28000400 (FOOT) | INLET & PIPE PROTECTION 28000500 (EACH) |
|-----------|----------------------|----|--------|--------|---------|----------|---|--|--|
| | STA. | TO | STA. | | | | | | |
| WB | 355+00 | | 361+00 | | X | 60.0' LT | | 600.0 | |
| EB | 355+00 | | 356+00 | | X | 60.0' RT | | 100.0 | |
| WB | 355+50 | | | X | | 1.0' LT | 15.0 | | |
| WB | 356+50 | | | X | | 3.5' LT | 10.0 | | |
| WB | 357+00 | | | X | | 8.0' LT | | | 1.0 |
| EB | 357+00 | | | | X | 64.0' RT | 12.0 | | |
| WB | 357+50 | | | X | | 4.0' LT | 10.0 | | |
| EB | 358+00 | | | | X | 63.0' RT | 6.0 | | |
| WB | 358+50 | | | X | | 4.0' LT | 10.0 | | |
| EB | 359+00 | | | | X | 59.0' RT | 8.0 | | |
| WB | 359+50 | | | X | | 0.5' LT | 15.0 | | |
| EB | 360+00 | | | | X | 58.0' RT | 8.0 | | |
| EB | 360+50 | | | X | | CL | 15.0 | | |



LEGEND

- TEMPORARY DITCH CHECK
- PERIMETER EROSION BARRIER
- INLET AND PIPE PROTECTION
- CONSTRUCTION LIMITS

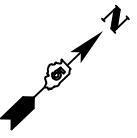
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| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

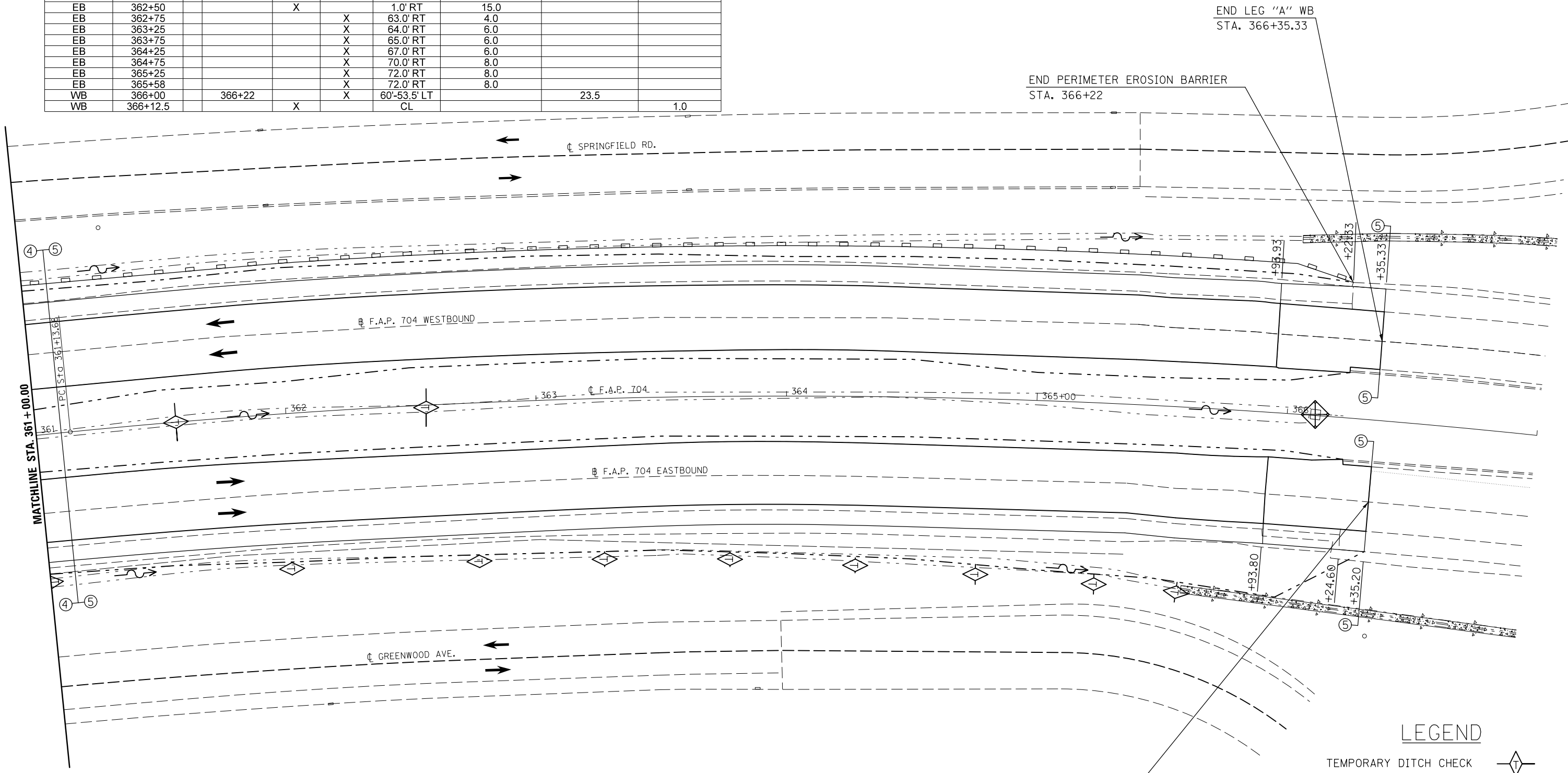
BUS 55 EROSION CONTROL SHEET

SCALE: 1" = 20' SHEET 7 OF 20 SHEETS STA. 355+00.00 TO STA. 361+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 92 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



| DIRECTION | LOCATION: LEG "A" | | | MEDIAN | OUTSIDE | OFFSET | TEMPORARY DITCH CHECKS 28000305 (FOOT) | PERIMETER EROSION BARRIER 28000400 (FOOT) | INLET & PIPE PROTECTION 28000500 (EACH) |
|-----------|----------------------|----|--------|--------|---------|--------------|---|--|--|
| | STA. | TO | STA. | | | | | | |
| WB | 361+00 | | 366+00 | | X | 60.0' LT | | 510.2 | |
| EB | 361+00 | | | | X | 59.0' RT | 6.0 | | |
| EB | 361+50 | | | X | | CL | 15.0 | | |
| EB | 362+00 | | | | X | 61.0' RT | 4.0 | | |
| EB | 362+50 | | | X | | 1.0' RT | 15.0 | | |
| EB | 362+75 | | | | X | 63.0' RT | 4.0 | | |
| EB | 363+25 | | | | X | 64.0' RT | 6.0 | | |
| EB | 363+75 | | | | X | 65.0' RT | 6.0 | | |
| EB | 364+25 | | | | X | 67.0' RT | 6.0 | | |
| EB | 364+75 | | | | X | 70.0' RT | 8.0 | | |
| EB | 365+25 | | | | X | 72.0' RT | 8.0 | | |
| EB | 365+58 | | | | X | 72.0' RT | 8.0 | | |
| WB | 366+00 | | 366+22 | | X | 60'-53.5' LT | | 23.5 | |
| WB | 366+12.5 | | | X | | CL | | | 1.0 |



END LEG "A" WB
STA. 366+35.33

END PERIMETER EROSION BARRIER
STA. 366+22

END LEG "A" EB
STATION 366+35.20

LEGEND

- TEMPORARY DITCH CHECK
- PERIMETER EROSION BARRIER
- INLET AND PIPE PROTECTION
- CONSTRUCTION LIMITS

| | | | |
|--|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - |
| et:\pw\work\p\dot\dodgms\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

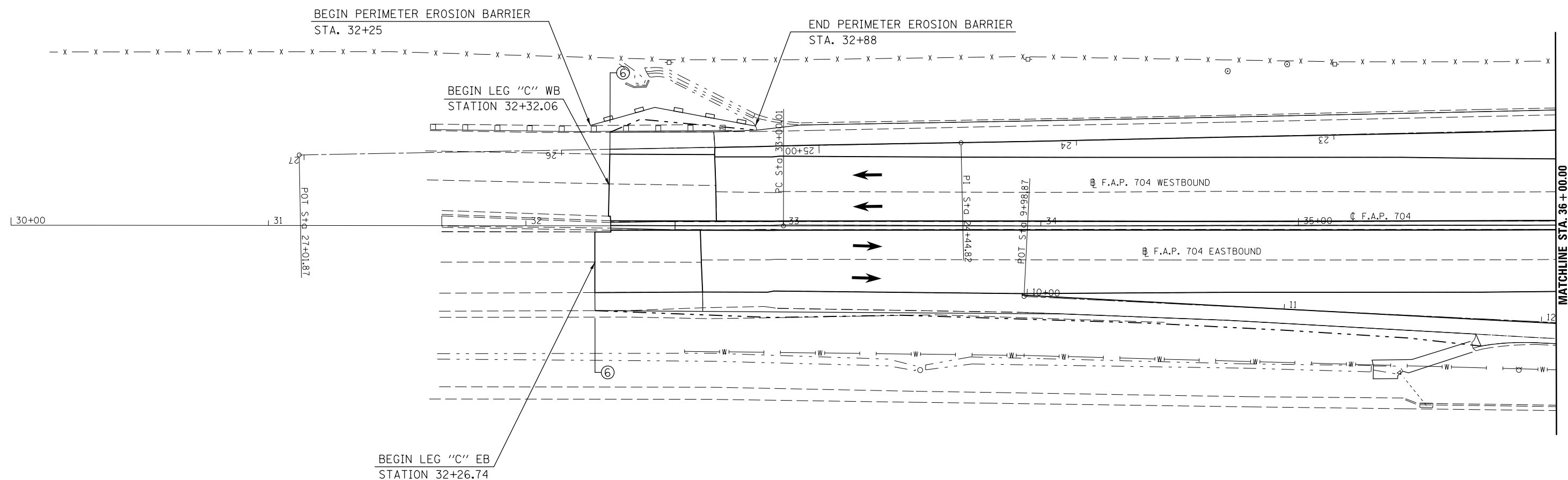
BUS 55 EROSION CONTROL SHEET

SCALE: 1" = 20' SHEET 8 OF 20 SHEETS STA. 361+00.00 TO STA. 366+35.33

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 93 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



| DIRECTION | LOCATION: LEG "C" | | | MEDIAN | OUTSIDE | OFFSET | PERIMETER EROSION BARRIER 28000400 (FOOT) |
|-----------|----------------------|----|-------|--------|---------|-----------|--|
| | STA. | TO | STA. | | | | |
| WB | 32+25 | | 32+50 | | X | 39-46' LT | 26.0 |
| WB | 32+50 | | 32+88 | | X | 46-39' LT | 38.6 |



LEGEND

- TEMPORARY DITCH CHECK
- PERIMETER EROSION BARRIER
- INLET AND PIPE PROTECTION
- CONSTRUCTION LIMITS

| | | | |
|---|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

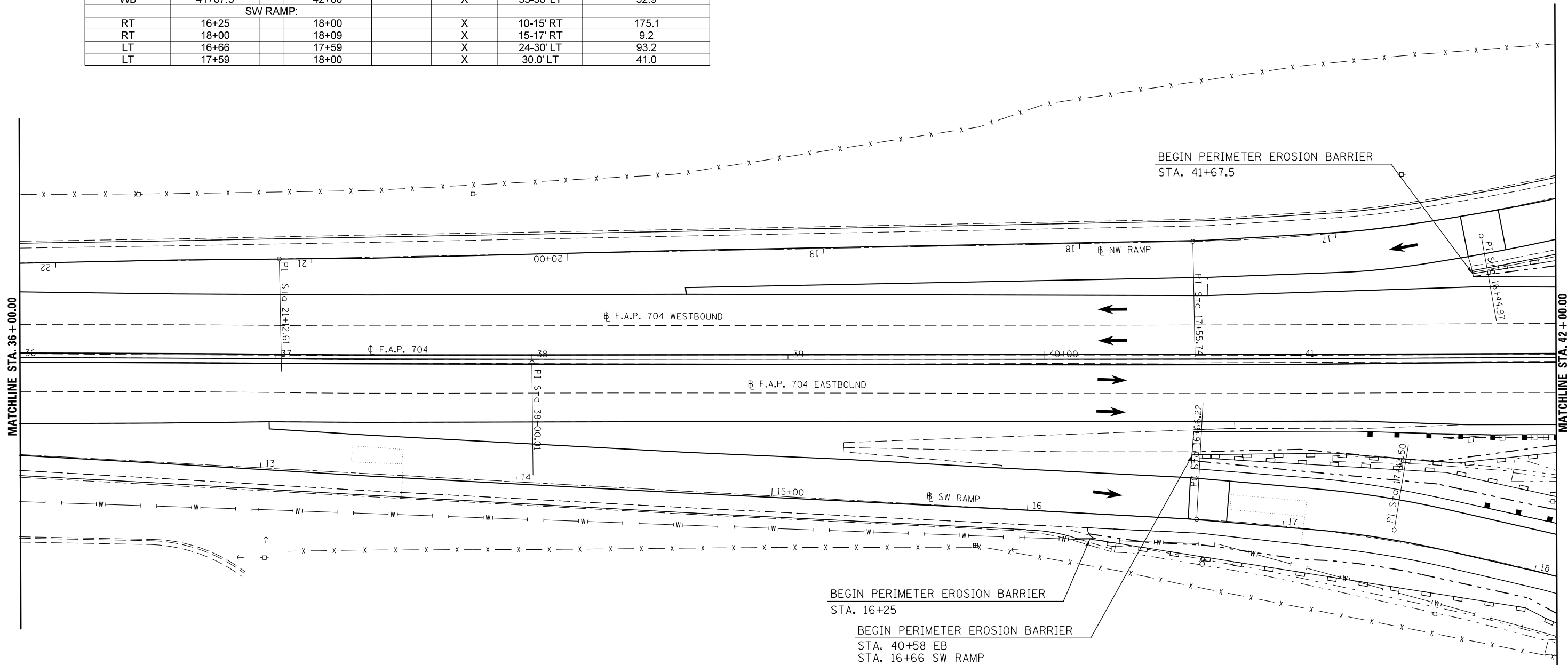
BUS 55 EROSION CONTROL SHEET

SCALE: 1" = 20' SHEET 9 OF 20 SHEETS STA. 32+26.74 TO STA. 36+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 94 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



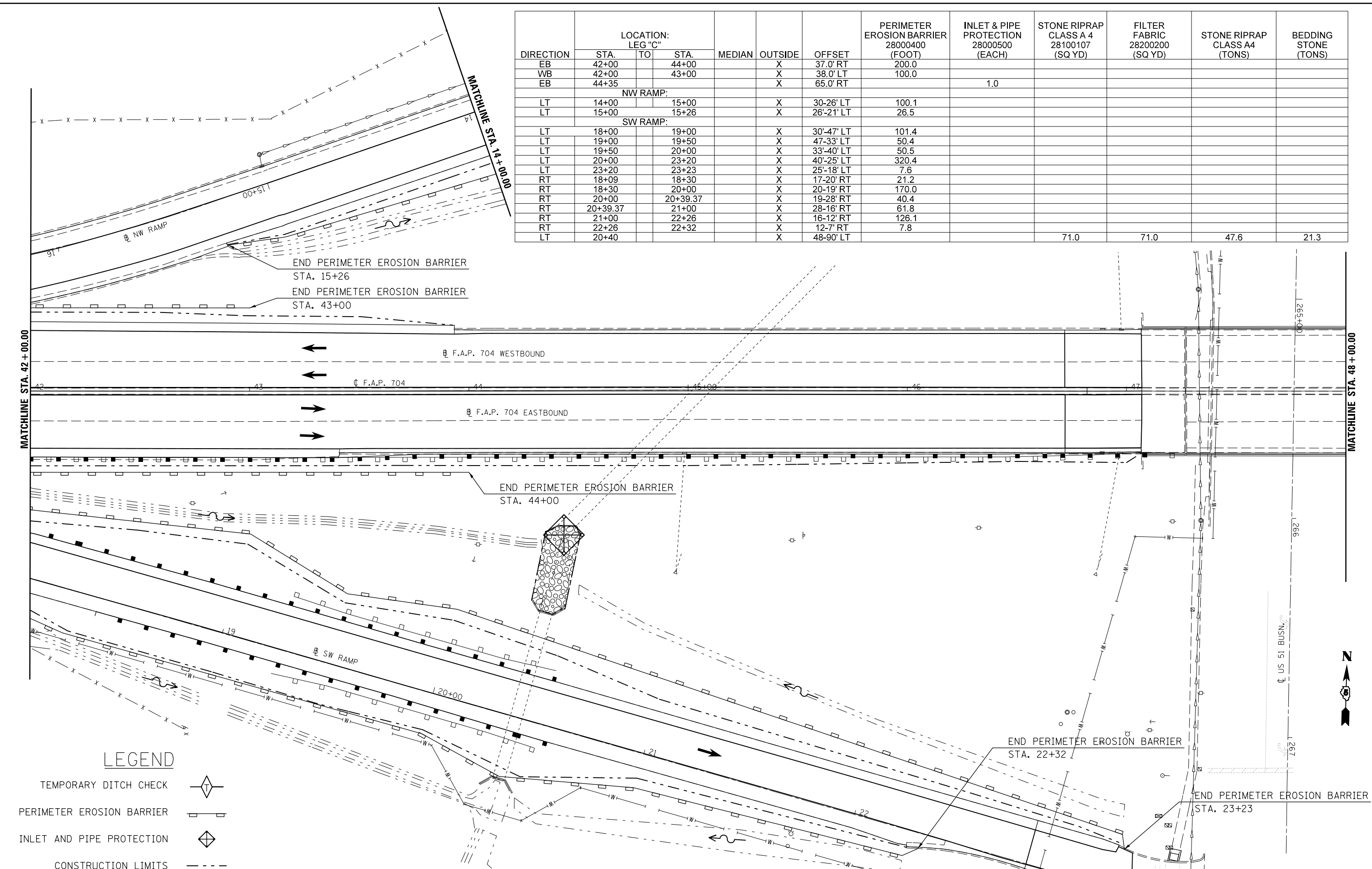
| DIRECTION | LOCATION: LEG "C" | | | MEDIAN | OUTSIDE | OFFSET | PERIMETER EROSION BARRIER 28000400 (FOOT) |
|-----------|----------------------|----|-------|--------|---------|-------------|--|
| | STA. | TO | STA. | | | | |
| EB | 40+58 | | 41+13 | | X | 37.5-37' RT | 55.0 |
| EB | 41+13 | | 41+61 | | X | 37-41' RT | 48.2 |
| EB | 41+61 | | 42+00 | | X | 41-37' RT | 39.2 |
| WB | 41+67.5 | | 42+00 | | X | 33-38' LT | 32.9 |
| SW RAMP: | | | | | | | |
| RT | 16+25 | | 18+00 | | X | 10-15' RT | 175.1 |
| RT | 18+00 | | 18+09 | | X | 15-17' RT | 9.2 |
| LT | 16+66 | | 17+59 | | X | 24-30' LT | 93.2 |
| LT | 17+59 | | 18+00 | | X | 30.0' LT | 41.0 |



LEGEND

- TEMPORARY DITCH CHECK
- PERIMETER EROSION BARRIER
- INLET AND PIPE PROTECTION
- CONSTRUCTION LIMITS

| DIRECTION | LOCATION: LEG "C" | | | MEDIAN | OUTSIDE | OFFSET | PERIMETER EROSION BARRIER 28000400 (FOOT) | INLET & PIPE PROTECTION 28000500 (EACH) | STONE RIPRAP CLASS A 4 28100107 (SQ YD) | FILTER FABRIC 28200200 (SQ YD) | STONE RIPRAP CLASS A4 (TONS) | BEDDING STONE (TONS) | |
|-----------|----------------------|----|----------|--------|---------|------------|--|--|--|---|------------------------------------|----------------------------|------|
| | STA. | TO | STA. | | | | | | | | | | |
| EB | 42+00 | | 44+00 | | X | 37.0' RT | 200.0 | | | | | | |
| WB | 42+00 | | 43+00 | | X | 38.0' LT | 100.0 | | | | | | |
| EB | 44+35 | | | | X | 65.0' RT | | 1.0 | | | | | |
| NW RAMP: | | | | | | | | | | | | | |
| LT | 14+00 | | 15+00 | | X | 30-26' LT | 100.1 | | | | | | |
| LT | 15+00 | | 15+26 | | X | 26'-21' LT | 26.5 | | | | | | |
| SW RAMP: | | | | | | | | | | | | | |
| LT | 18+00 | | 19+00 | | X | 30'-47' LT | 101.4 | | | | | | |
| LT | 19+00 | | 19+50 | | X | 47-33' LT | 50.4 | | | | | | |
| LT | 19+50 | | 20+00 | | X | 33'-40' LT | 50.5 | | | | | | |
| LT | 20+00 | | 23+20 | | X | 40'-25' LT | 320.4 | | | | | | |
| LT | 23+20 | | 23+23 | | X | 25'-18' LT | 7.6 | | | | | | |
| RT | 18+09 | | 18+30 | | X | 17-20' RT | 21.2 | | | | | | |
| RT | 18+30 | | 20+00 | | X | 20-19' RT | 170.0 | | | | | | |
| RT | 20+00 | | 20+39.37 | | X | 19-28' RT | 40.4 | | | | | | |
| RT | 20+39.37 | | 21+00 | | X | 28-16' RT | 61.8 | | | | | | |
| RT | 21+00 | | 22+26 | | X | 16-12' RT | 126.1 | | | | | | |
| RT | 22+26 | | 22+32 | | X | 12-7' RT | 7.8 | | | | | | |
| LT | 20+40 | | | | X | 48-90' LT | | | | 71.0 | 71.0 | 47.6 | 21.3 |



LEGEND

- TEMPORARY DITCH CHECK
- PERIMETER EROSION BARRIER
- INLET AND PIPE PROTECTION
- CONSTRUCTION LIMITS

| | | | |
|--|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - |
| et:\pw\work\p\dot\dodgms\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

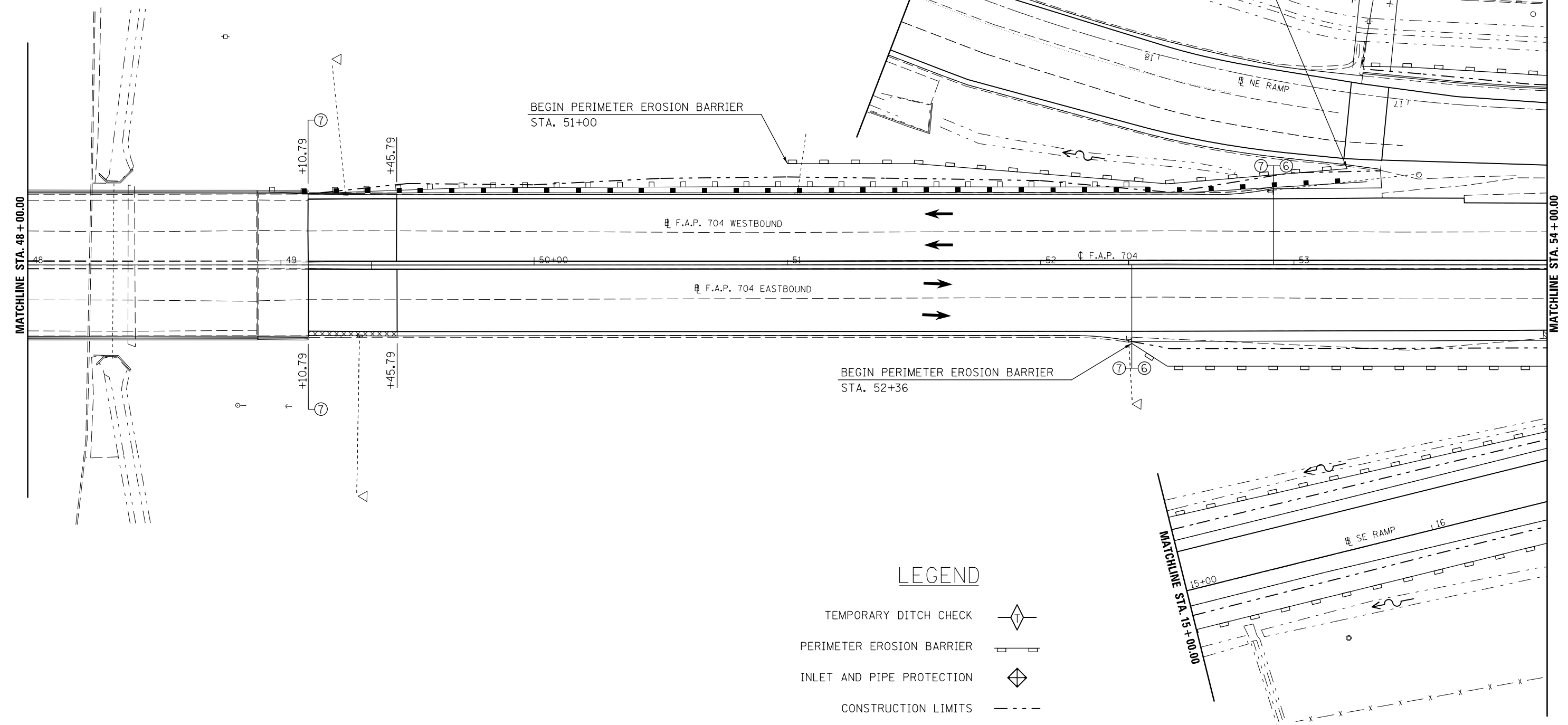
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUS 55 EROSION CONTROL SHEET
SCALE: 1" = 20' SHEET 11 OF 20 SHEETS STA. 42+00.00 TO STA. 48+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 96 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



| DIRECTION | LOCATION: LEG "C" | | | MEDIAN | OUTSIDE | OFFSET | PERIMETER EROSION BARRIER 28000400 (FOOT) |
|-----------|----------------------|----|-------|--------|---------|-----------|--|
| | STA. | TO | STA. | | | | |
| EB | 52+36 | | 52+50 | | X | 31-40' RT | 16.6 |
| EB | 52+50 | | 54+00 | | X | 40.0' RT | 150.0 |
| WB | 51+00 | | 51+50 | | X | 40.0' LT | 50.0 |
| WB | 51+50 | | 52+50 | | X | 40-32' LT | 100.3 |
| WB | 52+50 | | 53+21 | | X | 32-38' LT | 71.3 |
| NE RAMP: | | | | | | | |
| RT | 16+45 | | 17+00 | | X | 13.0' RT | 55.0 |
| RT | 17+00 | | 17+20 | | X | 13-11' RT | 20.1 |
| SE RAMP: | | | | | | | |
| RT | 15+00 | | 16+50 | | X | 15.0' RT | 150.0 |
| LT | 15+00 | | 16+50 | | X | 30-27' LT | 150.0 |



LEGEND

- TEMPORARY DITCH CHECK
- PERIMETER EROSION BARRIER
- INLET AND PIPE PROTECTION
- CONSTRUCTION LIMITS

| | | | |
|---|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |





**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

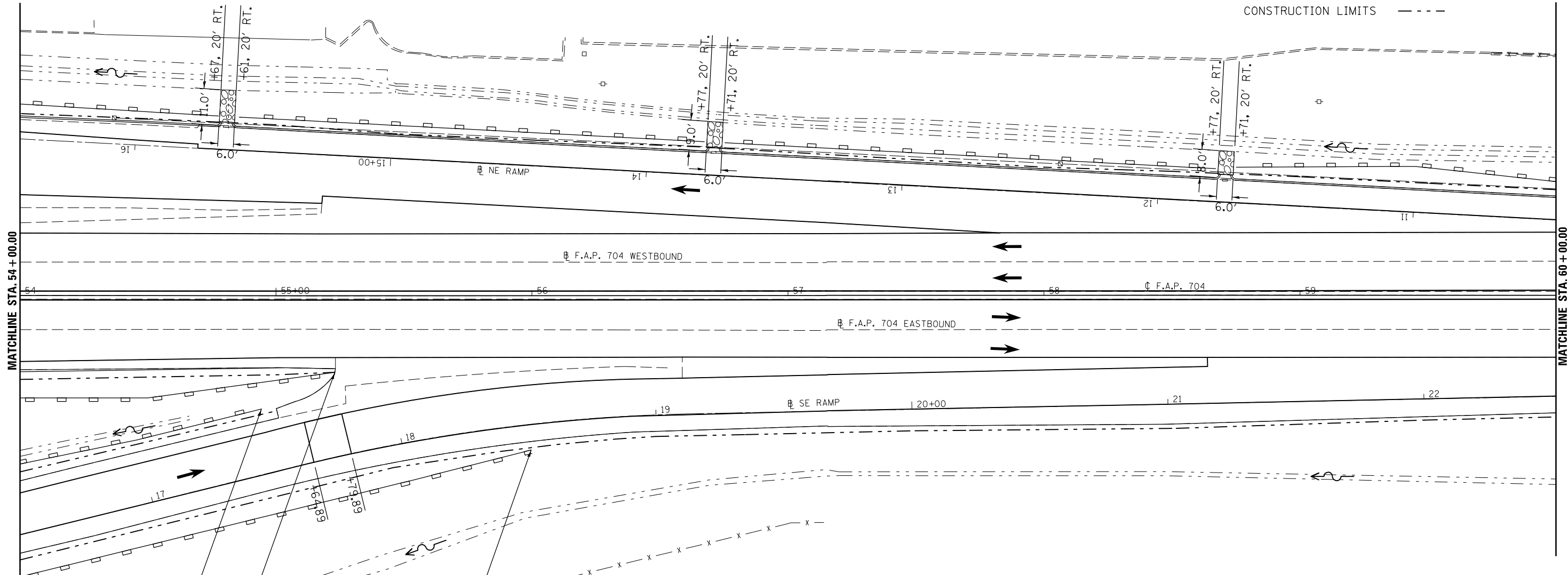
BUS 55 EROSION CONTROL SHEET

SCALE: 1" = 20' SHEET 12 OF 20 SHEETS STA. 48+00.00 TO STA. 54+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|------------------|--------|--------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 97 |
| CONTRACT NO. 70838 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

LEGEND

- TEMPORARY DITCH CHECK 
- PERIMETER EROSION BARRIER 
- INLET AND PIPE PROTECTION 
- CONSTRUCTION LIMITS 



| DIRECTION | LOCATION: LEG "C" | | | MEDIAN | OUTSIDE | OFFSET | PERIMETER EROSION BARRIER 28000400 (FOOT) | STONE RIPRAP CLASS A 4 28100107 (SQ YD) | FILTER FABRIC 28200200 (SQ YD) | STONE RIPRAP CLASS A4 (TONS) | BEDDING STONE (TONS) |
|-----------|----------------------|----|-------|--------|---------|---------------|--|--|---|------------------------------------|----------------------------|
| | STA. | TO | STA. | | | | | | | | |
| EB | 54+00 | | 54+50 | | X | 40.0' RT | 50.0 | | | | |
| EB | 54+50 | | 55+23 | | X | 40-30' RT | 73.7 | | | | |
| SE RAMP: | | | | | | | | | | | |
| LT | 16+50 | | 17+00 | | X | 27-25' LT | 50.0 | | | | |
| LT | 17+00 | | 17+50 | | X | 25.0' LT | 50.0 | | | | |
| RT | 16+50 | | 18+00 | | X | 15.0' RT | 150.0 | | | | |
| RT | 18+00 | | 18+50 | | X | 15-10' RT | 50.2 | | | | |
| NE RAMP: | | | | | | | | | | | |
| RT | 10+45 | | 11+00 | | X | 15-18' RT | 55.1 | | | | |
| RT | 11+00 | | 11+70 | | X | 18-14' RT | 70.1 | | | | |
| RT | 11+75 | | | | X | 12-20' RT | | 5.3 | 5.3 | 3.6 | 1.6 |
| RT | 11+78 | | 13+70 | | X | 13.0' RT | 192.0 | | | | |
| RT | 13+75 | | | | X | 12.5-21.5' RT | | 6.0 | 6.0 | 4.0 | 1.8 |
| RT | 13+78 | | 15+60 | | X | 13.0' RT | 182.0 | | | | |
| RT | 15+65 | | | | X | 12-23' RT | | 7.3 | 7.3 | 4.9 | 2.2 |
| RT | 15+68 | | 16+45 | | X | 13.0' RT | 77.0 | | | | |

| | | | |
|--|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgms | DESIGNED - | REVISED - |
| et:\pwork\pwork\dot\dodgms\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

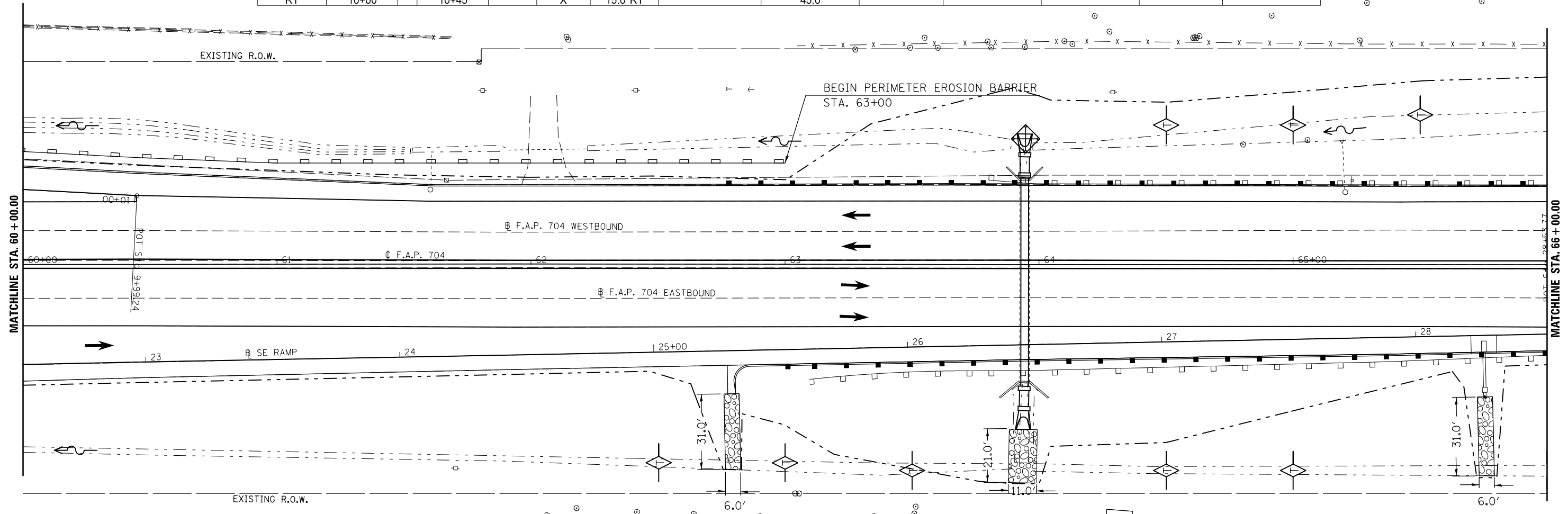
BUS 55 EROSION CONTROL SHEET

SCALE: 1" = 20' SHEET 13 OF 20 SHEETS STA. 54+00.00 TO STA. 60+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 98 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |



| DIRECTION | LOCATION: LEG "C" | | | MEDIAN | OUTSIDE | OFFSET | TEMPORARY DITCH CHECKS 28000305 (FOOT) | PERIMETER EROSION BARRIER 28000400 (FOOT) | INLET & PIPE PROTECTION 28000500 (EACH) | STONE RIPRAP CLASS A 4 28100107 (SQ YD) | FILTER FABRIC 28200200 (SQ YD) | STONE RIPRAP CLASS A4 (TONS) | BEDDING STONE (TONS) |
|-----------|----------------------|----|-------|--------|---------|---------------|---|--|--|--|---|------------------------------------|----------------------------|
| | STA. | TO | STA. | | | | | | | | | | |
| EB | 62+50 | | | | X | 78.0' RT | 15.0 | | | | | | |
| EB | 62+86 | | | | X | 50-80' RT | | | | 20.7 | 20.7 | 13.9 | 6.2 |
| EB | 63+00 | | | | X | 78.0' RT | 14.0 | | | | | | |
| EB | 63+50 | | | | X | 81.0' RT | 10.0 | | | | | | |
| EB | 63+94.35 | | | | X | 65.5-86.5' RT | | | | 25.7 | 25.7 | 17.2 | 7.7 |
| EB | 64+50 | | | | X | 81.0' RT | 10.0 | | | | | | |
| EB | 65+00 | | | | X | 81.0' RT | 10.0 | | | | | | |
| EB | 65+75 | | | | X | 52-83' RT | | | | 20.7 | 20.7 | 13.9 | 6.2 |
| WB | 60+43 | | 61+00 | | X | 42-40' LT | | 57.0 | | | | | |
| WB | 61+00 | | 63+00 | | X | 40.0' LT | | 200.0 | | | | | |
| WB | 63+94 | | | | X | 45.0' LT | | | 1.0 | | | | |
| WB | 64+50 | | | | X | 55.0' LT | 10.0 | | | | | | |
| WB | 65+00 | | | | X | 55.5' LT | 10.0 | | | | | | |
| WB | 65+75 | | | | X | 59.0' LT | 10.0 | | | | | | |
| NE RAMP: | | | | | | | | | | | | | |
| RT | 10+00 | | 10+45 | | X | 15.0' RT | | 45.0 | | | | | |



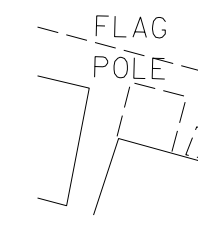
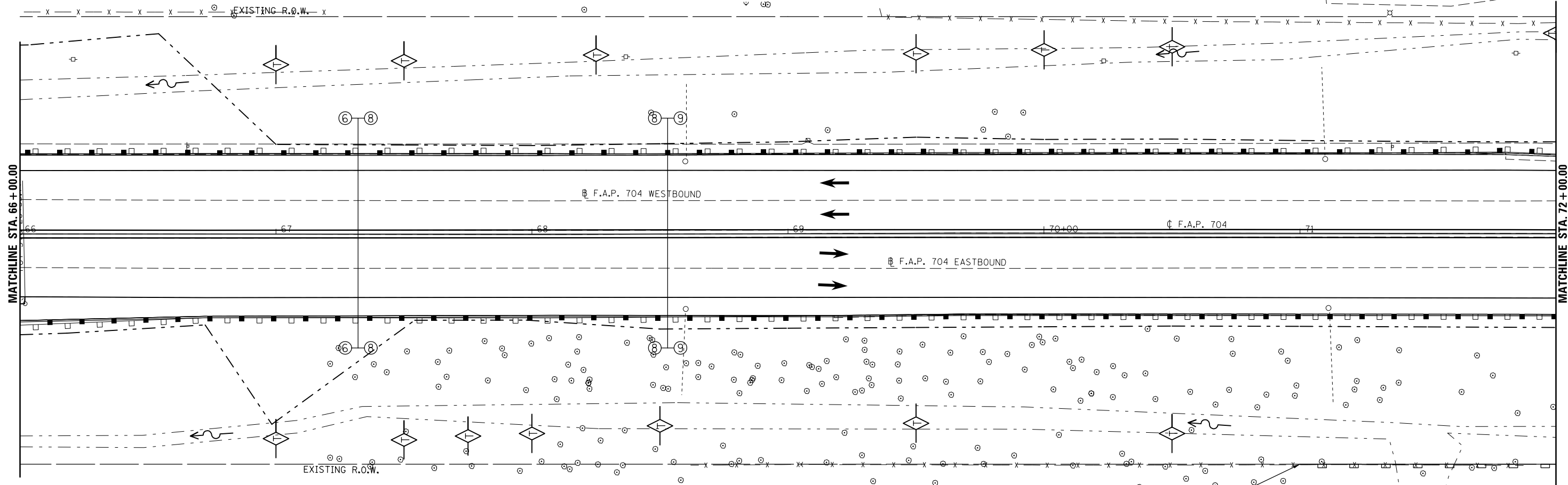
LEGEND

- TEMPORARY DITCH CHECK
- PERIMETER EROSION BARRIER
- INLET AND PIPE PROTECTION
- CONSTRUCTION LIMITS

| | | | | | | | | | | | | |
|--|---------------------|------------|-----------|---|-------------------------------------|-----------------------|--------------------------------|---------------------------|---------|--------|-----------------|--------------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | BUS 55 EROSION CONTROL SHEET | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pw\work\p\dot\dodgems\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - | | 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 99 | | | |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - | | CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | | | | |
| PLOT DATE = 6/11/2014 | | DATE - | REVISED - | | SCALE: 1" = 20' | SHEET 14 OF 20 SHEETS | STA. 60+00.00 TO STA. 66+00.00 | | | | | |



| DIRECTION | LOCATION: LEG "C" | | | MEDIAN | OUTSIDE | OFFSET | TEMPORARY DITCH CHECKS 28000305 (FOOT) | PERIMETER EROSION BARRIER 28000400 (FOOT) |
|-----------|----------------------|----|-------|--------|---------|-----------|---|--|
| | STA. | TO | STA. | | | | | |
| EB | 67+00 | | | | X | 80.0' RT | 10.0 | |
| EB | 67+50 | | | | X | 80.5' RT | 10.0 | |
| EB | 67+75 | | | | X | 79.0' RT | 10.0 | |
| EB | 68+00 | | | | X | 78.0' RT | 10.0 | |
| EB | 68+50 | | | | X | 75.0' RT | 10.0 | |
| EB | 69+50 | | | | X | 74.0' RT | 10.0 | |
| EB | 70+50 | | | | X | 78.0' RT | 10.0 | |
| EB | 71+00 | | 73+00 | | X | 90.0' RT | | 200.0 |
| WB | 67+00 | | | | X | 66.25' LT | 10.0 | |
| WB | 67+50 | | | | X | 67.75' LT | 10.0 | |
| WB | 68+25 | | | | X | 70.0' LT | 10.0 | |
| WB | 69+50 | | | | X | 70.5' LT | 10.0 | |
| WB | 70+00 | | | | X | 72.0' LT | 10.0 | |
| WB | 70+50 | | | | X | 73.25' LT | 10.0 | |
| WB | 72+00 | | | | X | 78.5' LT | 10.0 | |



LEGEND

- TEMPORARY DITCH CHECK
- PERIMETER EROSION BARRIER
- INLET AND PIPE PROTECTION
- CONSTRUCTION LIMITS

| | | | |
|---|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = dodgems | DESIGNED - | REVISED - |
| et:\pw\work\p\dot\dodgems\d0267571\0570838-sht\plan_BUS55.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 6/11/2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUS 55 EROSION CONTROL SHEET

SCALE: 1" = 20' SHEET 15 OF 20 SHEETS STA. 66+00 TO STA. 72+00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------------|--------|---------------------------|-----------|
| 704 | (57-7)RS-1&1RS-3 | McLean | 273 | 100 |
| CONTRACT NO. 70838 | | | ILLINOIS FED. AID PROJECT | |