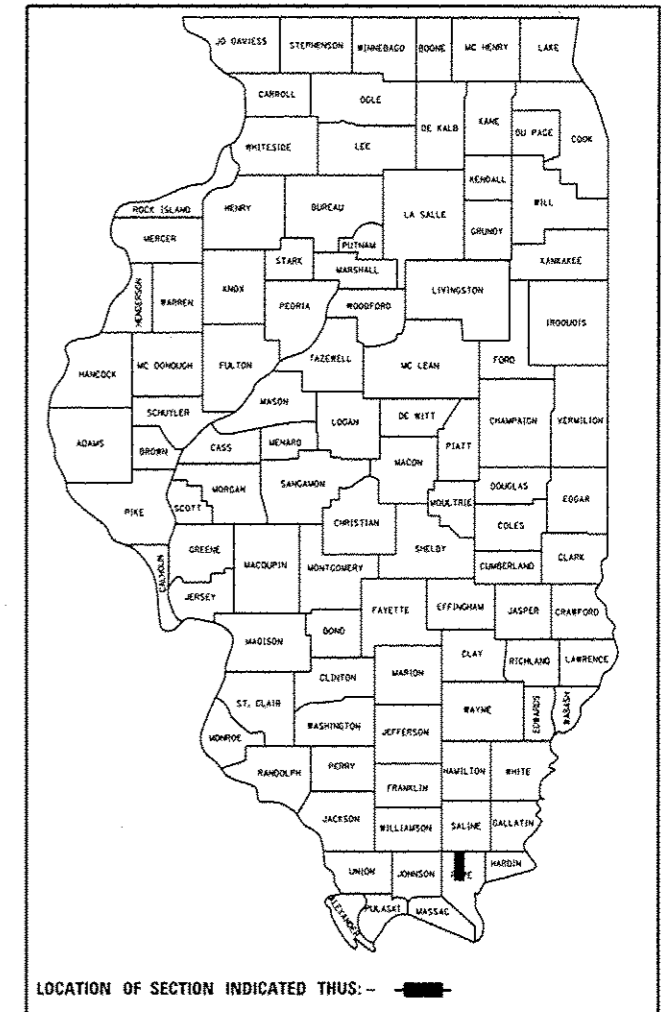


STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

| F.A.P. RTE. | SECTION        | COUNTY | TOTAL SHEETS       | SHEET NO. |
|-------------|----------------|--------|--------------------|-----------|
| *           | 12RS-3; 10RS-2 | POPE   | 21                 | 1         |
| ILLINOIS    |                |        | CONTRACT NO. 78436 |           |

\* FAP 132 / FAS 932

D-99-057-14



**PROPOSED  
HIGHWAY PLANS**

FAP ROUTE 132 / FAS ROUTE 932 (IL 145)  
SECTION 12RS-3; 10RS-2

RESURFACING  
POPE COUNTY

FOR INDEX OF SHEETS, SEE SHEET NO. 2  
FOR SUMMARY OF QUANTITIES, SEE SHEET NO. 4-6

**TRAFFIC DATA**

IL 145 TRAFFIC DATA  
2013 ADT = 1,700 WITH 12.4% TRUCKS

**TOWNSHIPS**

ALEXANDER, EDDYVILLE, POLK

**STATION EQUATIONS**

STA. 64+21.70 BK = STA. 64+23.00 AH  
STA. 37+12.80 BK = STA. 37+14.20 AH  
STA. 18+98.00 BK = STA. 654+25.30 AH

**OMISSIONS**

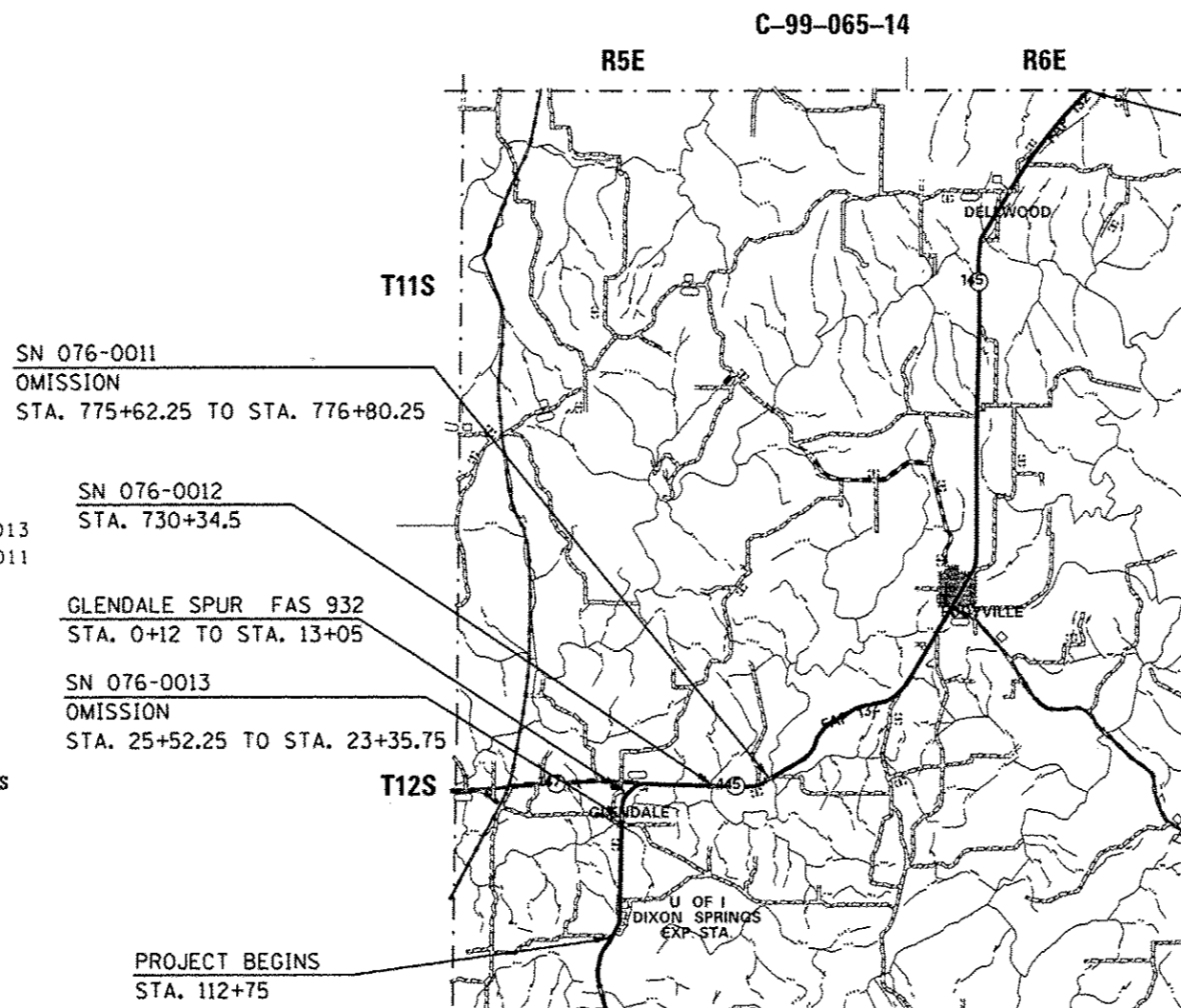
STA. 25+52.25 TO STA. 23+35.75 FOR SN076-0013  
STA. 775+62.25 TO STA. 776+80.25 FOR SN076-0011

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: SUSAN POE  
PROJECT MANAGER: JAY KRANZ

CONTRACT NO. 78436



SN 076-0011  
OMISSION  
STA. 775+62.25 TO STA. 776+80.25

SN 076-0012  
STA. 730+34.5

GLENDALE SPUR FAS 932  
STA. 0+12 TO STA. 13+05

SN 076-0013  
OMISSION  
STA. 25+52.25 TO STA. 23+35.75

PROJECT BEGINS  
STA. 112+75

PROJECT ENDS  
STA. 1364+48



MAP NOT TO SCALE

GROSS LENGTH = 80,402.4 FT. = 15.23 MILE  
NET LENGTH = 80,067.9 FT. = 15.16 MILE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED June 13 2014

Jeffrey L. Kern  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

July 18 2014  
John D. Baranelli, P.E.  
acting ENGINEER OF DESIGN AND ENVIRONMENT

July 18 2014  
Onur Osman, P.E.  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

# INDEX OF SHEETS

| SHEET NO. | DESCRIPTION  |
|-----------|--|
| 1         | COVER SHEET  |
| 2         | INDEX OF SHEETS, STANDARDS, MTD TABLE  |
| 3         | GENERAL NOTES, MIXTURE REQUIREMENTS, COMMITMENTS                               |
| 4 - 6     | SUMMARY OF QUANTITIES  |
| 7 - 9     | TYPICAL SECTIONS, STATIONING DIAGRAM<br>SHOULDER RESURFACE DETAIL              |
| 10 - 16   | SCHEDULES OF QUANTITIES  |
| 17        | EXISTING CURVE DATA  |
| 18        | DETAIL: BUTT JOINTS,   |
| 19        | DETAIL: SIDE ROAD DETAILS  |
| 20        | DETAIL: ENTRANCE DETAILS   |
| 21        | DETAIL: TEMP HMA TRANSITIONS, UNEVEN LANES SIGN,<br>ROUGH GROOVED SURFACE SIGN |

# STANDARDS

|           |   |
|-----------|---|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS                             |
| 001006    | DECIMAL OF AN INCH AND OF A FOOT  |
| 406201-01 | MAILBOX TURNOUT   |
| 442201-03 | CLASS C AND D PATCHES   |
| 482011-03 | HMA SHOULDER STRIPS/SHOULDERS WITH RESURFACING                            |
| 701001-02 | OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 15' AWAY                           |
| 701006-05 | OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE                |
| 701301-04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS                               |
| 701306-03 | LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS > 45MPH |
| 701311-03 | LANES CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY                       |
| 701336-06 | LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS > 45MPH            |
| 701901-03 | TRAFFIC CONTROL DEVICES   |
| 780001-04 | TYPICAL PAVEMENT MARKINGS   |
| 781001-03 | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS                   |

MTD CROSSING RESTRICTIONS TABLE

| SN & CENTERLINE STATION<br>ROUTE IL 145 | EXISTING FILL<br>HEIGHT OVER<br>BOX CULVERT | THICKNESS OF<br>EXISTING PAVEMENT<br>OVER BOX CULVERT | MTD CROSSING<br>RESTRICTIONS |
|---|---|---|------------------------------|
|   |   |   |                              |
| SN076-7036 STA. 33+42.5                 | 21.45                                       | 0.92  | LOADED                       |
| SN076-0013 STA. 24+44.                  |   |   | PENDING                      |
| SN076-7037 STA. 666+50.                 | 6   | 1.08  | LOADED                       |
| SN076-7043 STA. 681+28.                 | 3.15  | 1.08  | LOADED                       |
| SN076-7044 STA. 700+00.                 | 9.71  | 1.08  | LOADED                       |
| SN076-7045 STA. 708+60.                 | 4.37  | 1.08  | LOADED                       |
| SN076-7046 STA. 712+85.                 | 11.86                                       | 1.08  | LOADED                       |
| SN076-0012 STA. 730+34.5                | 6.08  | 1.08  | PENDING                      |
| SN076-0011 STA. 776+21.25               |   |   | PENDING                      |
| SN076-7047 STA. 941+66.                 | 11.74                                       | 1.08  | LOADED                       |
| SN076-7048 STA. 1064+43.                | 4.08  | 1.08  | LOADED                       |
| SN076-7049 STA. 1074+95.                | 15.46                                       | 1.08  | LOADED                       |
| SN076-7050 STA. 1084+74.                | 30.71                                       | 1.08  | LOADED                       |
| SN076-7051 STA. 1160+06.                | 8.54  | 1.08  | LOADED                       |
| SN076-7052 STA. 1198+40.                | 4.6   | 1.08  | LOADED                       |
| SN076-7053 STA. 1236+50.                | 9   | 1.08  | LOADED                       |

Prepared By: *Joe Zdaniewicz*  
DISTRICT STUDIES & PLANS ENGINEER

Examined By: *[Signature]*  
DISTRICT LAND ACQUISITION ENGINEER

Examined By: *Carrisa Nelson*  
DISTRICT PROGRAM DEVELOPMENT ENGINEER

Examined By: *[Signature]*  
DISTRICT OPERATIONS ENGINEER

Examined By: *[Signature]*  
DISTRICT PROJECT IMPLEMENTATION ENGINEER

Examined By: *Daryl J. Jurek*  
DISTRICT CONSTRUCTION ENGINEER

Examined By: *[Signature]*  
DISTRICT MATERIALS ENGINEER

## GENERAL NOTES

FACTORS USED FOR ESTIMATING PLAN QUANTITIES ARE AS FOLLOWS AND SHALL NOT BE USED FOR THE BASIS OF FINAL QUANTITIES:

|                        |                     |
|------------------------|---------------------|
| ALL HOT- MIX ASPHALT   | 2.016 TONS/CU. YD.  |
| ALL AGGREGATE          | 2.05 TONS/CU. YD.   |
| BITUMINOUS MATERIALS:  |                     |
| ON PAVEMENT            | 0.05 LB./SQ.FT.     |
| ON AGGREGATE SURFACE   | 0.25 LB./SQ. FT.    |
| AGGREGATE (PRIME COAT) | 0.0015 TONS/SQ. YD. |
| RIPRAP                 | 1.50 TONS/CU. YD.   |

THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDER SLOPES SHALL NOT EXCEED 8%. THE SHOULDER ON THE OUTSIDE OF SUPERELEVATED CURVES SHALL BE FLATTENED ACCORDINGLY.

STATIONING SHOWN IN THE SCHEDULES IS BASED ON STAMPED STATIONS IN THE EXISTING PAVEMENT AND FROM OLD PLANS. MINOR ADJUSTMENTS MAY BE REQUIRED AS DIRECTED BY THE ENGINEER.

THE THICKNESS OF HOT MIX ASPHALT MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT MIX ASPHALT MIXTURE IS PLACED.

THE QUANTITY OF SHORT TERM PAVEMENT MARKING SHOWN IN THE PLANS IS BASED ON ONE APPLICATION EACH FOR THE SURFACE REMOVAL, SURFACE COURSE, AND LEVELING BINDER.

THE CONTRACTOR SHALL STAMP STATIONING IN THE PROPOSED HOT MIX ASPHALT SURFACE AT 300 FT. INTERVALS ON ALTERNATING SIDES OF THE PAVEMENT AND AS DIRECTED BY THE ENGINEER. THE STATION SYMBOL STAMPS USED SHALL BE FURNISHED BY THE CONTRACTOR. THEY SHALL BE 5.5 IN. TALL, OF A DESIGN APPROVED BY THE ENGINEER, AND SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

BITUMINOUS RESURFACING SHALL BE PLACED IN A SEQUENCE THAT WILL MINIMIZE THE TIME THE CENTERLINE EDGE IS EXPOSED TO TRAFFIC. WHEN AT THE END OF A DAY'S OPERATION THE EXPOSED CENTERLINE EDGE IS GREATER THAN 2,000 FT., THE CONTRACTOR SHALL BE REQUIRED TO PAVE IN THE ADJACENT LANE ON THE FOLLOWING WORK DAY. PRIOR TO WINTER SHUTDOWN, RESURFACING ON ADJACENT LANES IS TO BE BROUGHT UP TO THE SAME ELEVATION.

PRIOR TO PLACEMENT OF THE FINAL PAVEMENT MARKINGS THE RESIDENT ENGINEER SHOULD CONTACT THE BUREAU OF OPERATIONS AND ARRANGE FOR INSPECTION AND APPROVAL OF THE PAVEMENT MARKING LAYOUT.

ALL PAVEMENT MARKINGS SHALL BE LOCATED AND RECORDED BEFORE SURFACE REMOVAL AND/OR HOT-MIX ASPHALT PLACEMENT.

SAWCUTS REQUIRED FOR BUTT JOINTS SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE INCLUDED IN THE COST OF THE BUTT JOINT.

QUANTITIES SHOWN IN THE PLANS FOR PATCHING ARE ESTIMATES. THE ACTUAL AMOUNT OF PATCHING REQUIRED SHALL BE DETERMINED BY THE ENGINEER. PATCHING SURVEY DATE: JUNE 2014.

THERE ARE NO AVAILABLE WASTE SITES ON THE EXISTING RIGHT OF WAY WITHIN THE PROJECT LIMITS. DISPOSAL WILL BE THE RESPONSIBILITY OF THE CONTRACTOR AND WASTE MUST BE DISPOSED OF IN ACCORDANCE WITH ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS

CONTRACTOR SHALL COMPLETE PATCHING PRIOR TO THE HMA SURFACE REMOVAL.

## MIXTURE REQUIREMENTS

|   |   |
|---|---|
| LOCATION(S):                                | HOT-MIX ASPHALT SURFACE COURSE,<br>INCIDENTAL HOT-MIX ASPHALT SURFACING |
| MIXTURE USE(S):                             | HOT-MIX ASPHALT SURFACE COURSE, MIX C, N90                              |
| AC/PG:                                      | PG64-22   |
| ABR% (MAX):                                 | SEE SPECIAL PROVISION   |
| DESIGN AIR VOIDS:                           | 4.0%, 90 GYRATION DESIGN  |
| MIXTURE COMPOSITION:<br>(GRADATION MIXTURE) | IL-9.5MM  |
| FRICTION AGGREGATE:                         | C SURFACE   |
| QUALITY MANAGEMENT<br>PROGRAM:              | OCP   |

|   |  |
|---|--|
| LOCATION(S):                                | HOT-MIX ASPHALT LEVELING BINDER COURSE                       |
| MIXTURE USE(S):                             | HOT-MIX ASPHALT LEVELING BINDER, N90,<br>IL-9.5MM FINE GRADE |
| AC/PG:                                      | PG64-22  |
| ABR% (MAX):                                 | SEE SPECIAL PROVISION  |
| DESIGN AIR VOIDS:                           | 4.0%, 90 GYRATION DESIGN                                     |
| MIXTURE COMPOSITION:<br>(GRADATION MIXTURE) | IL-9.5MM FINE GRADE  |
| FRICTION AGGREGATE:                         | NONE   |
| QUALITY MANAGEMENT<br>PROGRAM:              | OCP  |

|   |   |
|---|---|
| LOCATION(S):                                | PAVEMENT PATCHING   |
| MIXTURE USE(S):                             | HOT-MIX ASPHALT BINDER COURSE, N90,<br>IL-19.0MM FINE GRADE |
| AC/PG:                                      | PG64-22   |
| ABR% (MAX):                                 | SEE SPECIAL PROVISION                                       |
| DESIGN AIR VOIDS:                           | 4.0%, 90 GYRATION DESIGN                                    |
| MIXTURE COMPOSITION:<br>(GRADATION MIXTURE) | IL-19.0MM FINE GRADE  |
| FRICTION AGGREGATE:                         | NONE  |
| QUALITY MANAGEMENT<br>PROGRAM:              | OC/OA   |

## COMMITMENTS

NONE

|  |  |             |            |   |   |                     |                    |        |                 |              |
|--|--|-------------|------------|---|---|---------------------|--------------------|--------|-----------------|--------------|
| FILE NAME *                            | USER NAME * krenzjo                    | DESIGNED -- | REVISED -- | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>GENERAL NOTES, MIXTURE REQUIREMENTS,<br/>COMMITMENTS</b> | F.A.P.<br>RTE.      | SECTION            | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO. |
| o:\pwwork\pwwork\krenzjo\2482088\78436 | o:\pwwork\pwwork\krenzjo\2482088\78436 | DRAWN --    | REVISED -- |   |   | *                   | 12RS-31 10RS-2     | POPE   | 21              | 3            |
| Default                                | PLOT SCALE * 94.444 / 1/16             | CHECKED --  | REVISED -- |   |   | * FAP 132 / FAS 932 | CONTRACT NO. 78436 |        |                 |              |
|  | PLOT DATE * 6/13/2014                  | DATE --     | REVISED -- |   |   | SCALE:              | SHEET              | OF     | SHEETS          | STA.         |

# SUMMARY OF QUANTITIES

|           |                  |
|-----------|------------------|
| FUNDING:  | 100% STATE       |
| LOCATION: | RURAL            |
| ROUTE:    | IL 145 (FAP 132) |
|           | POPE             |

| CODE NUMBER         | ITEM DESCRIPTION                             | UNIT           | TOTAL QUANTITY | RESURFACING    |
|---------------------|--|----------------|----------------|----------------|
|                     |  |                |                | 0005           |
| 35101400            | AGGREGATE BASE COURSE, TYPE B                | TON            | 1,357          | 1,357          |
| <del>40600300</del> | <del>AGGREGATE (PRIME COAT)</del>            | <del>TON</del> | <del>351</del> | <del>351</del> |
| 40600645            | LEVELING BINDER (MACHINE METHOD), N90        | TON            | 9587           | 9587           |
| 40600982            | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SO YD          | 894            | 894            |
| 40600990            | TEMPORARY RAMP                               | SO YD          | 115            | 115            |
| 40603320            | HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N90 | TON            | 19479          | 19479          |
| 40800050            | INCIDENTAL HOT-MIX ASPHALT SURFACING         | TON            | 588            | 588            |
| 44000151            | HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"        | SO YD          | 226859         | 226859         |
| 44000157            | HOT-MIX ASPHALT SURFACE REMOVAL, 2"          | SO YD          | 3353           | 3353           |
| 44201803            | CLASS D PATCHES, TYPE II, 13 INCH            | SO YD          | 816            | 816            |
| 44201807            | CLASS D PATCHES, TYPE III, 13 INCH           | SO YD          | 563            | 563            |
| 44201809            | CLASS D PATCHES, TYPE IV, 13 INCH            | SO YD          | 148            | 148            |
| 48101200            | AGGREGATE SHOULDERS, TYPE B                  | TON            | 3982           | 3982           |
| 67000400            | ENGINEER'S FIELD OFFICE, TYPE A              | CAL MO         | 6              | 6              |

# SUMMARY OF QUANTITIES - CONT

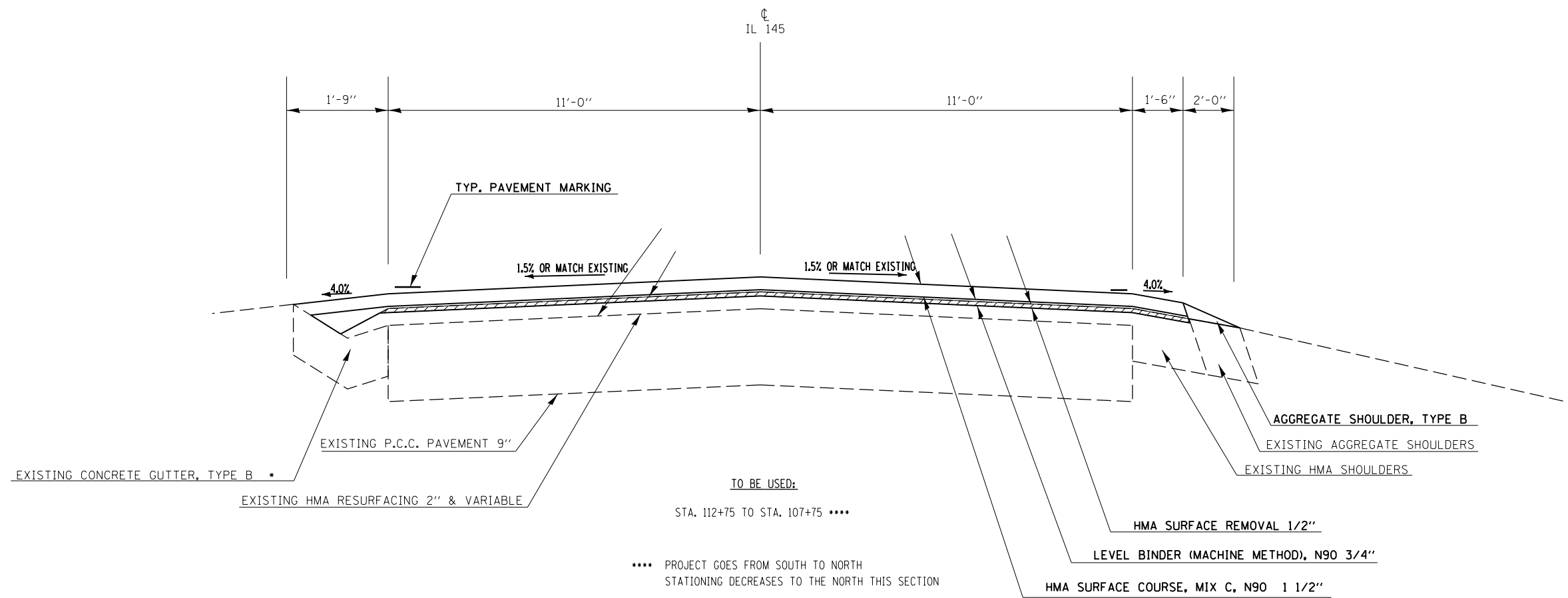
|           |                  |
|-----------|------------------|
| FUNDING:  | 100% STATE       |
| LOCATION: | RURAL            |
| ROUTE:    | IL 145 (FAP 132) |
|           | POPE             |

| CODE NUMBER | ITEM DESCRIPTION                                | UNIT   | TOTAL QUANTITY | RESURFACING 0005 |
|-------------|---|--------|----------------|------------------|
| 67100100    | MOBILIZATION                                    | L SUM  | 1              | 1                |
| 70100460    | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306 | L SUM  | 1              | 1                |
| 70100600    | TRAFFIC CONTROL AND PROTECTION, STANDARD 701336 | L SUM  | 1              | 1                |
| 70106800    | CHANGEABLE MESSAGE SIGN                         | CAL MO | 2              | 2                |
| 70300100    | SHORT TERM PAVEMENT MARKING                     | FOOT   | 24388          | 24388            |
| 70300220    | TEMPORARY PAVEMENT MARKING - LINE 4"            | FOOT   | 235477         | 235477           |
| 70301000    | WORK ZONE PAVEMENT MARKING REMOVAL              | SO FT  | 2724           | 2724             |
| * 78000200  | THERMOPLASTIC PAVEMENT MARKING - LINE 4"        | FOOT   | 64             | 64               |
| * 78000600  | THERMOPLASTIC PAVEMENT MARKING - LINE 12"       | FOOT   | 38             | 38               |
| * 78000650  | THERMOPLASTIC PAVEMENT MARKING - LINE 24"       | FOOT   | 30             | 30               |
| * 78001110  | PAINT PAVEMENT MARKING - LINE 4"                | FOOT   | 235477         | 235477           |
| * 78100100  | RAISED REFLECTIVE PAVEMENT MARKER               | EACH   | 1002           | 1002             |
| 78300200    | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL       | EACH   | 1002           | 1002             |
| 40600275    | BITUMINOUS MATERIALS (PRIME COAT)               | POUND  | 110401         | 110401           |
|             | * SPECIALTY ITEMS                               |        |                |                  |

# SUMMARY OF QUANTITIES - CONT

|           |                  |
|-----------|------------------|
| FUNDING:  | 100% STATE       |
| LOCATION: | RURAL            |
| ROUTE:    | IL 145 (FAP 132) |
|           | POPE             |
|           | RESURFACING      |
|           | 0005             |

| CODE NUMBER | ITEM DESCRIPTION         | UNIT | TOTAL QUANTITY |       |
|-------------|--------------------------|------|----------------|-------|
| Z0034105    | MATERIAL TRANSFER DEVICE | TON  | 28690          | 28690 |



\* EXISTING CONCRETE GUTTER , TYPE B

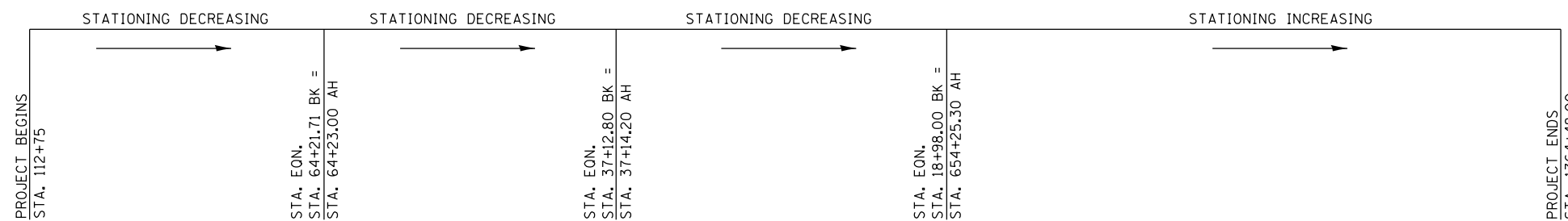
STATIONING FROM OLD PLANS

WEST STA 110+00. TO STA 112+06.  
 WEST STA 112+94. TO STA 119+47.  
 WEST STA 107+75. TO STA 110+00.

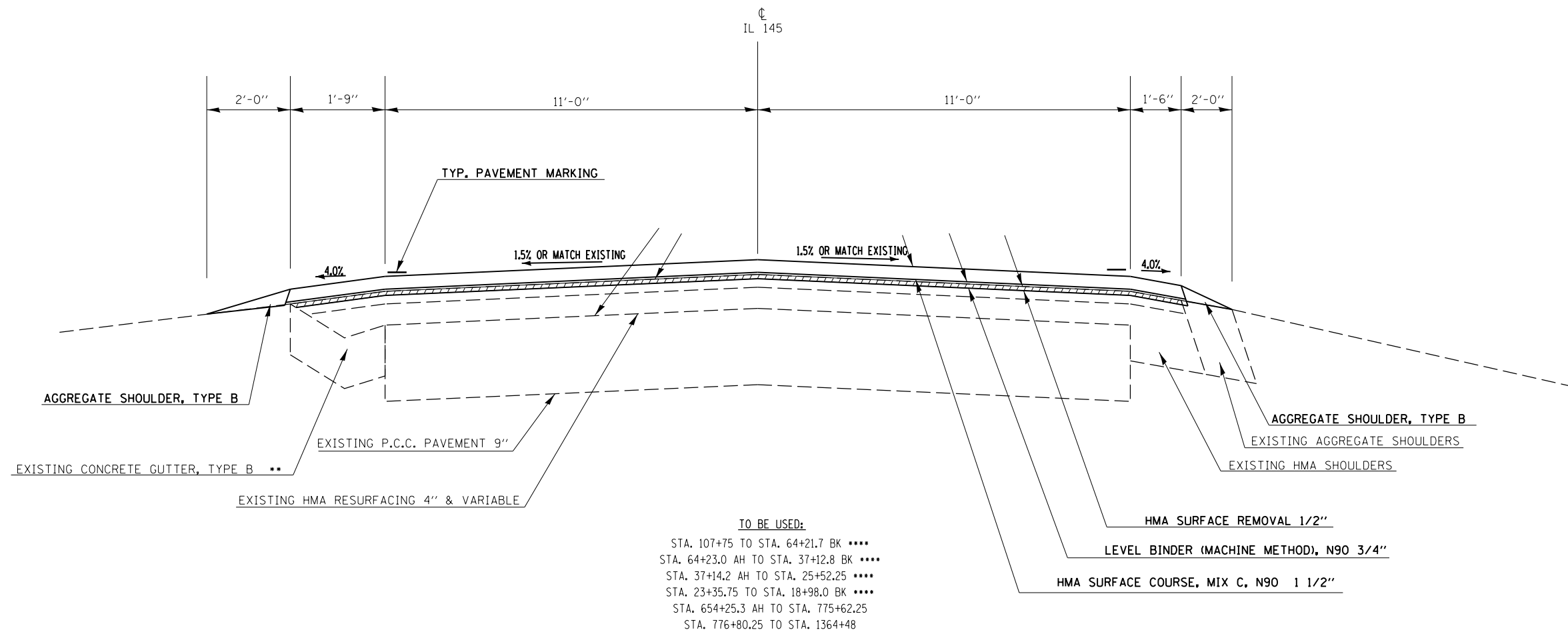
NOTES:

STATIONING DECREASES FROM BEGINNING OF THE PROJECT TO NEAR THE GLENDALE SPUR ROAD.  
 STATIONING INCREASES FROM NEAR THE GLENDALE SPUR ROAD TO THE END OF THE PROJECT.  
 (STA. 18+98.00 BK TO STA. 654+25.30 AH)

STATIONING DIAGRAM



|  |                              |            |           |   |                           |                    |        |                |         |        |                 |              |
|--|------------------------------|------------|-----------|---|---------------------------|--------------------|--------|----------------|---------|--------|-----------------|--------------|
| FILE NAME =  | USER NAME = kranzjc          | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>TYPICAL</b>            |                    |        | F.A.P.<br>RTE. | SECTION | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO. |
| ci:\pw_work\pwidot\kranzjc\1402080\78436-shsta-cvr_sqi.index.dgn |                              | DRAWN -    | REVISED - |   | •                         | 12RS-3; 10RS-2     | POPE   | 21             | 7       |        |                 |              |
| Default  | PLOT SCALE = 94.4444 ' / in. | CHECKED -  | REVISED - |   | • FAP 132 / FAS 932       | CONTRACT NO. 78436 |        |                |         |        |                 |              |
|  | PLOT DATE = 6/13/2014        | DATE -     | REVISED - |   | ILLINOIS FED. AID PROJECT |                    |        |                |         |        |                 |              |
|  |                              |            |           | SCALE:  | SHEET                     | OF                 | SHEETS | STA.           | TO STA. |        |                 |              |



TO BE USED:  
 STA. 107+75 TO STA. 64+21.7 BK \*\*\*\*  
 STA. 64+23.0 AH TO STA. 37+12.8 BK \*\*\*\*  
 STA. 37+14.2 AH TO STA. 25+52.25 \*\*\*\*  
 STA. 23+35.75 TO STA. 18+98.0 BK \*\*\*\*  
 STA. 654+25.3 AH TO STA. 775+62.25  
 STA. 776+80.25 TO STA. 1364+48

\*\*\*\* PROJECT GOES FROM SOUTH TO NORTH  
 STATIONING DECREASES TO THE NORTH THIS SECTION

\*\* EXISTING CONCRETE GUTTER , TYPE B

NOTES:

STATIONING DECREASES FROM BEGINNING OF THE PROJECT TO NEAR THE GLENDALE SPUR ROAD.  
 STATIONING INCREASES FROM NEAR THE GLENDALE SPUR ROAD TO THE END OF THE PROJECT.  
 (STA. 18+98.00 BK TO STA. 654+25.30 AH)

STATIONING FROM OLD PLANS

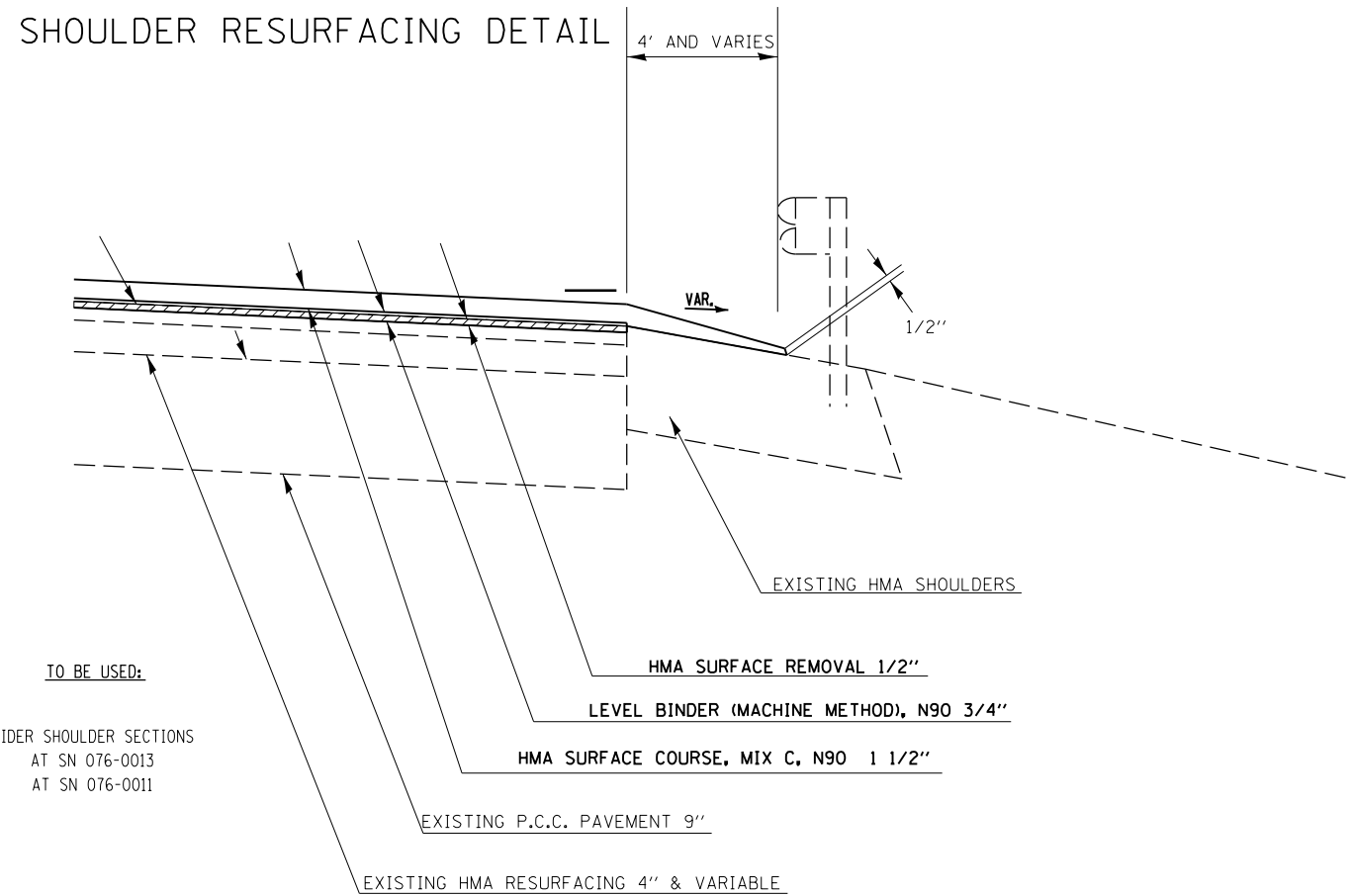
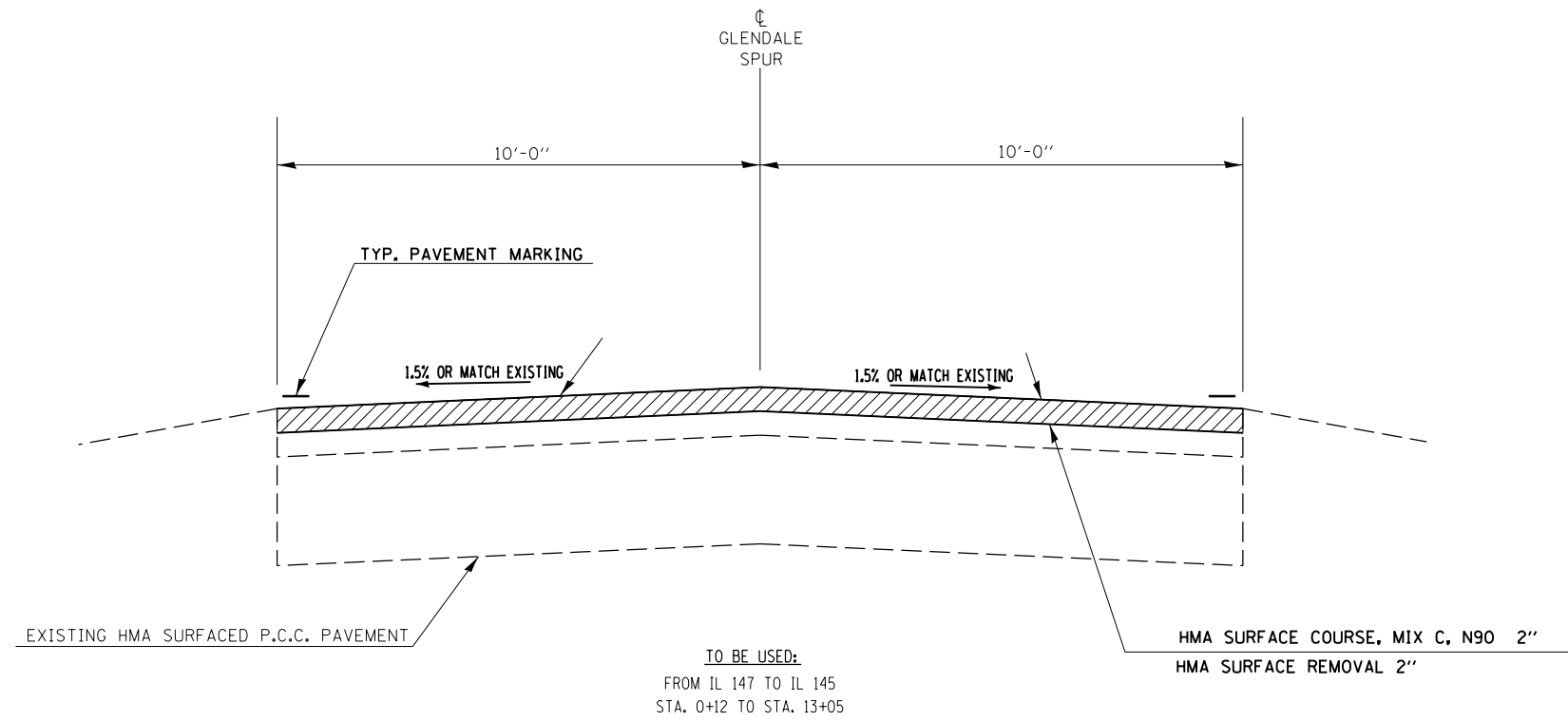
WEST STA 90+78. TO STA 107+75.  
 EAST STA 50+67. TO STA 51+47.  
 EAST STA 22+43.25 TO STA 23+65.25  
 EAST STA 25+22.75 TO STA 26+22.75  
 EAST STA 33+59. TO STA 42+22.  
 WEST STA 22+43.25 TO STA 23+65.25  
 WEST STA 25+22.75 TO STA 31+20.  
 WEST STA 32+10. TO STA 42+22.  
 RT STA 655+78. TO STA 664+83.  
 RT STA 667+17. TO STA 670+00.  
 RT STA 670+00. TO STA 684+00.  
 LT & R STA 710+28. TO STA 715+00.  
 LT & R STA 715+00. TO STA 717+00.  
 LT & R STA 725+00. TO STA 731+47.  
 LT & R STA 733+53. TO STA 745+75.  
 LT STA 750+66. TO STA 775+00.  
 RT STA 763+50. TO STA 768+97.  
 RT STA 774+62.25 TO STA 775+00.  
 LT & R STA 775+00. TO STA 775+62.25

LT & R STA 776+80.25 TO STA 785+50.  
 LT STA 796+51. TO STA 805+00.  
 RT STA 796+53. TO STA 805+00.  
 LT STA 805+00. TO STA 824+50.  
 RT STA 805+00. TO STA 835+50.  
 RT STA 854+50. TO STA 865+00.  
 LT STA 868+88. TO STA 895+00.  
 RT STA 865+00. TO STA 874+55.  
 LT STA 895+00. TO STA 899+70.  
 LT & R STA 929+00. TO STA 937+49.  
 LT STA 941+28. TO STA 955+00.  
 RT STA 941+58. TO STA 955+00.  
 LT & R STA 955+00. TO STA 956+25.  
 LT STA 983+26. TO STA 992+33.  
 RT STA 983+26. TO STA 988+75.  
 LT STA 993+53. TO STA 1008+64.  
 LT & R STA 1037+03. TO STA 1040+00.  
 LT & R STA 1040+00. TO STA 1042+25.

LT & R STA 1051+00. TO STA 1063+72.  
 LT & R STA 1067+01. TO STA 1070+00.  
 LT & R STA 1070+00. TO STA 1097+75.  
 LT & R STA 1145+50. TO STA 1158+74.  
 LT & R STA 1180+61. TO STA 1190+00.  
 LT & R STA 1190+00. TO STA 1197+74.  
 RT STA 1222+50. TO STA 1232+95.  
 RT STA 1233+86. TO STA 1238+10.  
 LT & R STA 1262+51. TO STA 1280+00.  
 LT & R STA 1280+00. TO STA 1282+96.  
 RT STA 1288+78. TO STA 1310+00.  
 LT STA 1301+50. TO STA 1310+00.  
 LT & R STA 1310+00. TO STA 1317+74.  
 LT STA 1342+50. TO STA 1360+24.  
 RT STA 1342+50. TO STA 1360+22.  
 LT STA 1363+25. TO STA 1364+34.  
 RT STA 1363+25. TO STA 1364+62.

|   |                              |            |           |   |                |       |    |                |         |         |                           |                           |      |    |   |
|---|------------------------------|------------|-----------|---|----------------|-------|----|----------------|---------|---------|---------------------------|---------------------------|------|----|---|
| FILE NAME =                             | USER NAME = kranzjc          | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>TYPICAL</b> |       |    | F.A.P.<br>RTE. | SECTION | COUNTY  | TOTAL<br>SHEETS           | SHEET<br>NO.              |      |    |   |
| ci:\pw_work\pvidot\kranzjc\1402080\7843 | 5-sha-cvr_sq_index.dgn       | DRAWN -    | REVISED - |   | SCALE:         | SHEET | OF | SHEETS         | STA.    | TO STA. | •                         | 12RS-3; 10RS-2            | POPE | 21 | 8 |
| Default                                 | PLOT SCALE = 94.4444' / 1in. | CHECKED -  | REVISED - |   |                |       |    |                |         |         | • FAP 132 / FAS 932       | <b>CONTRACT NO. 78436</b> |      |    |   |
|   | PLOT DATE = 6/13/2014        | DATE -     | REVISED - |   |                |       |    |                |         |         | ILLINOIS FED. AID PROJECT |                           |      |    |   |





|             |  |            |           |   |  |       |    |                |         |         |                           |                    |   |
|-------------|--|------------|-----------|---|--|-------|----|----------------|---------|---------|---------------------------|--------------------|---|
| FILE NAME = | USER NAME = kranzjc  | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>TYPICAL<br/>SHOULDER RESURFACING DETAIL</b> |       |    | F.A.P.<br>RTE. | SECTION | COUNTY  | TOTAL<br>SHEETS           | SHEET<br>NO.       |   |
| Default     | ct:\pw_work\pwidot\kranzjc\1402080\78436-shsta-cvr-soq.index.dgn | DRAWN -    | REVISED - |   | SCALE:   | SHEET | OF | SHEETS         | STA.    | TO STA. | POPE                      | 21                 | 9 |
|             | PLOT SCALE = 94.4444' / in.                                      | CHECKED -  | REVISED - |   |  |       |    |                |         |         | FAP 132 / FAS 932         | CONTRACT NO. 78436 |   |
|             | PLOT DATE = 6/13/2014  | DATE -     | REVISED - |   |  |       |    |                |         |         | ILLINOIS FED. AID PROJECT |                    |   |



# SURFACE REMOVAL SCHEDULE

| LOCATION:<br>STATION TO STATION | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL,<br>1/2" | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL,<br>2" | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL -<br>BUTT JOINT | TEMPORARY<br>RAMP |
|---------------------------------|---|---|--|-------------------|
|                                 | SQ YD   | SQ YD   | SQ YD  | SQ YD             |
| FAP 132                         |   |   |  |                   |
| IL 145                          |   |   |  |                   |
| STA 112+75                      |   |   | 99.2   | 14.2              |
| STA 112+75. TO STA 64+21.7 BK   | 13,751.0  |   |  |                   |
| STA 64+23. AH TO STA 37+12.8 BK | 7,678.9   |   |  |                   |
| STA 37+14.2 AH TO STA 25+52.25  | 3,292.2   |   |  |                   |
| STA 25+52.25                    |   |   | 99.2   | 14.2              |
| STA 23+35.75                    |   |   | 99.2   | 14.2              |
| STA 23+35.75 TO STA 18+98. BK   | 1,240.3   |   |  |                   |
| STA 65+25.3 AH TO STA 775+62.25 | 34,388.0  |   |  |                   |
| STA 775+62.25                   |   |   | 99.2   | 14.2              |
| STA 776+80.25                   |   |   | 99.2   | 14.2              |
| STA 776+80.25 TO STA 1364+48.   | 166,508.6   |   |  |                   |
| STA 1364+48.                    |   |   | 99.2   | 14.2              |
|                                 |   |   |  |                   |
| GLENDALE SPUR                   |   |   |  |                   |
| STA 0+12. TO STA 13+05.         |   | 3,353.0   |  | 29.8              |
|                                 |   |   |  |                   |
| <b>SCHEDULE TOTALS:</b>         | <b>226,859.0</b>                                  | <b>3,353.0</b>                                  | <b>595.2</b>   | <b>115.0</b>      |
| SIDE ROAD & ENTRANCE SCHEDULE   |   |   | 0.0  |                   |
| <b>IMPROVEMENT TOTAL:</b>       | <b>226,859.0</b>                                  | <b>3,353.0</b>                                  | <b>595.2</b>   | <b>115.0</b>      |

|  |                              |            |           |   |                                       |                 |                |                     |                |                  |              |                    |
|--|------------------------------|------------|-----------|---|---------------------------------------|-----------------|----------------|---------------------|----------------|------------------|--------------|--------------------|
| FILE NAME =                              | USER NAME = kranzjc          | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>SCHEDULE -<br/>SURFACE REMOVAL</b> |                 | F.A.P.<br>RTE. | SECTION             | COUNTY         | TOTAL<br>SHEETS  | SHEET<br>NO. |                    |
| ci:\pw_work\p1dot\kranzjc\d0402000\78436 | 5-shsta-cvr_ssq_index.dgn    | DRAWN -    | REVISED - |   | SCALE:                                | SHEET OF SHEETS | STA. TO STA.   | •                   | 12RS-3; 10RS-2 | POPE             | 21           | 11                 |
| Default                                  | PLOT SCALE = 94.4444 ' / in. | CHECKED -  | REVISED - |   |                                       |                 |                | • FAP 132 / FAS 932 |                |                  |              | CONTRACT NO. 78436 |
|  | PLOT DATE = 6/13/2014        | DATE -     | REVISED - |   |                                       |                 |                |                     | ILLINOIS       | FED. AID PROJECT |              |                    |

## ENTRANCE SCHEDULE

| LOCATION:<br>STATION TO STATION | NUMBER<br>OF<br>LOCATIONS | EXISTING<br>SURFACE<br>MATERIAL | INCIDENTAL                      | BITUMINOUS                | AGGREGATE<br>(PRIME<br>COAT) | HMA                                   | AGGREGATE<br>SHOULDERS,<br>TYPE B | AGGREGATE<br>BASE<br>COURSE,<br>TYPE B |
|---------------------------------|---------------------------|---------------------------------|---------------------------------|---------------------------|------------------------------|---------------------------------------|-----------------------------------|--|
|                                 |                           |                                 | HOT-MIX<br>ASPHALT<br>SURFACING | MATERIALS<br>(PRIME COAT) |                              | SURFACE<br>REMOVAL -<br>BUTT<br>JOINT |                                   |  |
|                                 |                           |                                 | TON                             | POUND                     | TON                          | SQ YD                                 | TON                               | TON                                    |
| FAP 132                         |                           |                                 |                                 |                           |                              |                                       |                                   |  |
| IL 145                          |                           |                                 |                                 |                           |                              |                                       |                                   |  |
| ENTRANCES                       |                           |                                 |                                 |                           |                              |                                       |                                   |  |
| PRIVATE                         | 86                        | AGGREGATE                       | 89.9                            | 412.8                     |                              |                                       | 45.7                              |  |
| PRIVATE                         | 20                        | PAVED                           | 20.9                            | 96                        |                              |                                       |                                   |  |
| COMMERCIAL                      | 14                        | AGGREGATE                       | 21.3                            | 98                        |                              |                                       | 10.9                              |  |
| COMMERCIAL                      | 5                         | PAVED                           | 7.6                             | 35                        |                              |                                       |                                   |  |
| FIELD                           | 27                        |                                 |                                 |                           |                              |                                       | 28.7                              |  |
| MAILBOX TURNOUT                 | 75                        |                                 |                                 |                           |                              |                                       | 349.7                             |  |
| SIDE ROADS                      |                           |                                 |                                 |                           |                              |                                       |                                   |  |
| LT AIRPORT RD                   |                           | OIL & CHIP                      | 14.3                            | 213.8                     | 0.1                          |                                       |                                   | 37.2                                   |
| RT SHAWNEE FOREST               |                           | AGGREGATE                       | 14.7                            | 225.0                     | 0.2                          |                                       |                                   | 45.6                                   |
| RT SHAWNEE FOREST               |                           | ASPHALT                         | 8.2                             | 35.3                      | 0.1                          | 36.7                                  |                                   | 35.7                                   |
| RT SHAWNEE FOREST               |                           | ASPHALT                         | 8.8                             | 37.9                      | 0.1                          | 40.3                                  |                                   | 38.3                                   |
| RT COUNTY ROAD                  |                           | AGGREGATE                       | 8.3                             | 127.5                     | 0.1                          |                                       |                                   | 25.8                                   |
| RT COUNTY ROAD                  |                           | AGGREGATE                       | 10.9                            | 166.9                     | 0.1                          |                                       |                                   | 33.8                                   |
| LT GLENDALE RD                  |                           | OIL & CHIP                      | 13.1                            | 196.8                     | 0.1                          |                                       |                                   | 34.2                                   |
| RT GLENDALE RD                  |                           | OIL & CHIP                      | 14.0                            | 210.0                     | 0.1                          |                                       |                                   | 36.4                                   |
| LT GLENDALE SPUR                | SEE SURFACE               | SCHEDULE                        |                                 |                           |                              |                                       |                                   |  |
| LT IL 147                       |                           | ASPHALT                         | 19.7                            | 84.4                      | 0.3                          | 103.9                                 |                                   | 85.4                                   |
| RT NORVELL RD                   |                           | AGGREGATE                       | 14.9                            | 228.8                     | 0.2                          |                                       |                                   | 46.3                                   |
| LT NEIGHBORS LN                 |                           | AGGREGATE                       | 7.8                             | 120.0                     | 0.1                          |                                       |                                   | 24.3                                   |
| RT HAZEL RD                     |                           | AGGREGATE                       | 20.6                            | 315.0                     | 0.2                          |                                       |                                   | 63.8                                   |
| LT HARDIN LN                    |                           | AGGREGATE                       | 8.8                             | 135.0                     | 0.1                          |                                       |                                   | 27.3                                   |
| LT FRIENDLY LN                  |                           | AGGREGATE                       | 8.6                             | 131.3                     | 0.1                          |                                       |                                   | 26.6                                   |

1 OF 2

|  |                              |            |           |   |                                       |       |    |                |         |         |                     |              |    |    |
|--|------------------------------|------------|-----------|---|---------------------------------------|-------|----|----------------|---------|---------|---------------------|--------------|----|----|
| FILE NAME =                              | USER NAME = kranzjc          | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>SCHEDULE -<br/>ENTRANCE 1 OF 2</b> |       |    | F.A.P.<br>RTE. | SECTION | COUNTY  | TOTAL<br>SHEETS     | SHEET<br>NO. |    |    |
| ci:\pw_work\pwidot\kranzjc\1402080\78436 | 5-shsta\cvt_ssq_index.dgn    | DRAWN -    | REVISED - |   | SCALE:                                | SHEET | OF | SHEETS         | STA.    | TO STA. | • FAP 132 / FAS 932 | POPE         | 21 | 12 |
| Default                                  | PLOT SCALE = 94.4444' / 1" = | CHECKED -  | REVISED - |   | CONTRACT NO. 78436                    |       |    |                |         |         |                     |              |    |    |
|  | PLOT DATE = 6/13/2014        | DATE -     | REVISED - |   | ILLINOIS FED. AID PROJECT             |       |    |                |         |         |                     |              |    |    |

## ENTRANCE SCHEDULE

| LOCATION:<br>STATION TO STATION | NUMBER<br>OF<br>LOCATIONS | EXISTING<br>SURFACE<br>MATERIAL | INCIDENTAL                      | BITUMINOUS                | AGGREGATE<br>(PRIME<br>COAT) | HMA                                   | AGGREGATE<br>SHOULDERS,<br>TYPE B | AGGREGATE                 |
|---------------------------------|---------------------------|---------------------------------|---------------------------------|---------------------------|------------------------------|---------------------------------------|-----------------------------------|---------------------------|
|                                 |                           |                                 | HOT-MIX<br>ASPHALT<br>SURFACING | MATERIALS<br>(PRIME COAT) |                              | SURFACE<br>REMOVAL -<br>BUTT<br>JOINT |                                   | BASE<br>COURSE,<br>TYPE B |
|                                 |                           |                                 | TON                             | POUND                     | TON                          | SQ YD                                 | TON                               | TON                       |
| RT WALTERSBURG RD               |                           | AGGREGATE                       | 15.8                            | 241.9                     | 0.2                          |                                       |                                   | 49                        |
| LT MAIN ST                      |                           | ASPHALT                         | 11.4                            | 48.8                      | 0.2                          | 56.2                                  |                                   | 49.4                      |
| RT EDDYVILLE RD                 |                           | ASPHALT                         | 12.6                            | 54.0                      | 0.2                          | 61.7                                  |                                   | 54.7                      |
| LT SPRING ST.                   |                           | OIL & CHIP                      | 10.7                            | 162.9                     | 0.1                          |                                       |                                   | 27.7                      |
| LT WASHINGTON ST                |                           | OIL & CHIP                      | 15.4                            | 232.9                     | 0.2                          |                                       |                                   | 39.9                      |
| RT NEW HOME RD                  |                           | OIL & CHIP                      | 20.9                            | 321.3                     | 0.2                          |                                       |                                   | 53.9                      |
| LT STAIGHT ST                   |                           | OIL & CHIP                      | 19.3                            | 293.5                     | 0.2                          |                                       |                                   | 50.1                      |
| LT BEAR BRANCH RD               |                           | OIL & CHIP                      | 12.4                            | 189.5                     | 0.1                          |                                       |                                   | 31.9                      |
| LT WATER TOWER RD               |                           | OIL & CHIP                      | 17.2                            | 264.4                     | 0.2                          |                                       |                                   | 44.4                      |
| RT NEW HOME RD                  |                           | OIL & CHIP                      | 17.9                            | 270.8                     | 0.2                          |                                       |                                   | 46.3                      |
| RT SADDLE DR                    |                           | OIL & CHIP                      | 18.2                            | 276.5                     | 0.2                          |                                       |                                   | 47.1                      |
| LT BIRDY LN                     |                           | AGGREGATE                       | 8.6                             | 131.3                     | 0.1                          |                                       |                                   | 26.6                      |
| RT BARGER DR                    |                           | AGGREGATE                       | 15.1                            | 230.6                     | 0.2                          |                                       |                                   | 46.7                      |
| RT BARGER DR                    |                           | AGGREGATE                       | 16.0                            | 245.6                     | 0.2                          |                                       |                                   | 49.7                      |
| LT BORDEN FALL RD               |                           | AGGREGATE                       | 9.3                             | 142.5                     | 0.1                          |                                       |                                   | 28.9                      |
| LT BORDEN FALL RD               |                           | AGGREGATE                       | 14.1                            | 215.6                     | 0.1                          |                                       |                                   | 43.7                      |
| RT LAY LOOP                     |                           | OIL & CHIP                      | 11.4                            | 170.3                     | 0.1                          |                                       |                                   | 29.6                      |
| RT LAY LOOP                     |                           | OIL & CHIP                      | 13.5                            | 204.5                     | 0.1                          |                                       |                                   | 34.9                      |
| RT OAK RD                       |                           | OIL & CHIP                      | 15.8                            | 238.6                     | 0.2                          |                                       |                                   | 41.8                      |
| <b>SCHEDULE TOTALS:</b>         |                           |                                 | <b>588.0</b>                    | <b>6,805.0</b>            | <b>5.0</b>                   | <b>298.8</b>                          | <b>435.0</b>                      | <b>1,357.0</b>            |
| FROM SURFACING SCHEDULE:        |                           |                                 |                                 | 103,596.0                 | 346.0                        |                                       | 3547                              |                           |
| FROM SURFACE REMOVAL SCHEDULE:  |                           |                                 |                                 |                           |                              | 595.2                                 |                                   |                           |
| <b>IMPROVEMENT TOTALS:</b>      |                           |                                 | <b>588.0</b>                    | <b>110,401.0</b>          | <b>351.0</b>                 | <b>894.0</b>                          | <b>3,982.0</b>                    | <b>1,357.0</b>            |

2 OF 2

# PERMANENT PAVEMENT MARKING SCHEDULE

| LOCATION:<br>STATION TO STATION | PAINT PAVEMENT MARKING |        |           | THERMOPLASTIC PAVEMENT |         |         |
|---------------------------------|------------------------|--------|-----------|------------------------|---------|---------|
|                                 | - LINE 4 INCH          |        |           | MARKING - LINE         |         |         |
|                                 | WHITE                  | YELLOW | YELLOW    | 4 INCH                 | 12 INCH | 24 INCH |
|                                 | SOLID                  | SOLID  | SKIP-DASH | WHITE                  | WHITE   | WHITE   |
|                                 | FOOT                   | FOOT   | FOOT      | FOOT                   | FOOT    | FOOT    |
| FAP 132                         |                        |        |           |                        |         |         |
| IL 145                          |                        |        |           |                        |         |         |
| STA 112+75. TO STA 64+21.7 BK   | 9,707                  |        | 1,210     |                        |         |         |
| STA 64+23. AH TO STA 49+15.     | 3,016                  |        | 380       |                        |         |         |
| STA 49+15. TO STA 43+97.        | 1,036                  | 518    | 130       |                        |         |         |
| STA 43+97. TO STA 38+16.        | 1,162                  |        | 150       |                        |         |         |
| STA 38+16. TO STA 37+12.8 BK    | 206                    | 103    | 30        |                        |         |         |
| STA 37+14.2 AH TO STA 28+27.    | 1,774                  | 887    | 220       |                        |         |         |
| STA 28+27. TO STA 18+98. BK     | 1,858                  |        | 230       |                        |         |         |
| STA 654+25.3 AH TO STA 708+55.  | 10,859                 |        | 1,360     |                        |         |         |
| STA 708+55. TO STA 719+21.      | 2,132                  | 1,066  | 270       |                        |         |         |
| STA 719+21. TO STA 722+53.      | 664                    | 664    |           |                        |         |         |
| STA 722+53. TO STA 733+32.      | 2,158                  | 1,079  | 270       |                        |         |         |
| STA 733+32. TO STA 734+63.      | 262                    |        | 30        |                        |         |         |
| STA 734+63. TO STA 745+20.      | 2,114                  | 1,057  | 260       |                        |         |         |
| STA 745+20. TO STA 761+57.      | 3,274                  | 3,274  |           |                        |         |         |
| STA 761+57. TO STA 772+59.      | 2,204                  | 1,102  | 280       |                        |         |         |
| STA 772+59. TO STA 775+18.      | 518                    |        | 60        |                        |         |         |
| STA 775+18. TO STA 782+30.      | 1,424                  | 712    | 180       |                        |         |         |
| STA 782+30. TO STA 785+91.      | 722                    |        | 90        |                        |         |         |
| STA 785+91. TO STA 793+29.      | 1,476                  | 738    | 180       |                        |         |         |
| STA 793+29. TO STA 795+82.      | 506                    |        | 60        |                        |         |         |
| STA 795+82. TO STA 806+88.      | 2,212                  | 1,106  | 280       |                        |         |         |
| STA 806+88. TO STA 832+99.      | 5,222                  | 5,222  |           |                        |         |         |
| STA 832+99. TO STA 843+88.      | 2,178                  | 1,089  | 270       |                        |         |         |
| STA 843+88. TO STA 869+01.      | 5,026                  |        | 630       |                        |         |         |
| STA 869+01. TO STA 879+84.      | 2,166                  | 1,083  | 270       |                        |         |         |
| STA 879+84. TO STA 902+48.      | 4,528                  | 4,528  |           |                        |         |         |
| STA 902+48. TO STA 912+45.      | 1,994                  | 997    | 250       |                        |         |         |
| STA 912+45. TO STA 916+32.      | 774                    |        | 100       |                        |         |         |
| STA 916+32. TO STA 927+51.      | 2,238                  | 1,119  | 280       |                        |         |         |
| STA 927+51. TO STA 938+11.      | 2,120                  | 1,060  | 270       |                        |         |         |
| STA 938+11. TO STA 943+78.      | 1,134                  |        | 140       |                        |         |         |
| STA 943+78. TO STA 954+81.      | 2,206                  | 1,103  | 280       |                        |         |         |
| STA 954+81. TO STA 963+40.      | 1,718                  | 1,718  |           |                        |         |         |
| STA 963+40. TO STA 967+57.      | 834                    | 417    | 100       |                        |         |         |
| STA 967+57. TO STA 980+00.      | 2,486                  |        | 310       |                        |         |         |
| STA 980+00. TO STA 990+96.      | 2,192                  | 1,096  | 270       |                        |         |         |
| STA 990+96. TO STA 996+93.      | 1,194                  | 1,194  |           |                        |         |         |
| STA 996+93. TO STA 1006+77.     | 1,968                  | 984    | 250       |                        |         |         |
| STA 1006+77. TO STA 1035+02.    | 5,650                  |        | 710       |                        |         |         |
| STA 1035+02. TO STA 1045+00.    | 1,996                  | 998    | 250       |                        |         |         |

|             |  |            |           |   |   |                     |                 |                    |                 |                           |  |
|-------------|--|------------|-----------|---|---|---------------------|-----------------|--------------------|-----------------|---------------------------|--|
| FILE NAME = | USER NAME = kranzjc                      | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>SCHEDULE<br/>PERMANENT PAVEMENT MARKING 1 OF 2</b> | F.A.P.<br>RTE.      | SECTION         | COUNTY             | TOTAL<br>SHEETS | SHEET<br>NO.              |  |
| Default     | ct:\pw_work\pwidot\kranzjc\1402080\78436 | DRAWN -    | REVISED - |   |   | •                   | 12RS-3; 10RS-2  | POPE               | 21              | 14                        |  |
|             | PLOT SCALE = 94.4444' / in.              | CHECKED -  | REVISED - |   |   | • FAP 132 / FAS 932 |                 | CONTRACT NO. 78436 |                 |                           |  |
|             | PLOT DATE = 6/13/2014                    | DATE -     | REVISED - |   |   | SCALE:              | SHEET OF SHEETS | STA.               | TO STA.         | ILLINOIS FED. AID PROJECT |  |



# TEMPORARY PAVEMENT MARKING SCHEDULE

| LOCATION:<br>STATION TO STATION | SHORT-TERM<br>PAVEMENT<br>MARKING | WORK<br>ZONE<br>PAVEMENT<br>MARKING<br>REMOVAL |
|---------------------------------|-----------------------------------|--|
|                                 | FOOT                              | SQ FT  |
| FAP 132                         |                                   |  |
| IL 145                          |                                   |  |
| STA 112+75. TO STA 64+21.7 BK   | 1,452                             | 161.3  |
| STA 64+23. AH TO STA 49+15.     | 456                               | 50.7   |
| STA 49+15. TO STA 43+97.        | 156                               | 17.3   |
| STA 43+97. TO STA 38+16.        | 180                               | 20.0   |
| STA 38+16. TO STA 37+12.8 BK    | 36                                | 4.0  |
| STA 37+14.2 AH TO STA 28+27.    | 264                               | 29.3   |
| STA 28+27. TO STA 18+98. BK     | 276                               | 30.7   |
| STA 654+25.3 AH TO STA 708+55.  | 1,632                             | 181.3  |
| STA 708+55. TO STA 719+21.      | 324                               | 36.0   |
| STA 719+21. TO STA 722+53.      | 96                                | 10.7   |
| STA 722+53. TO STA 733+32.      | 324                               | 36.0   |
| STA 733+32. TO STA 734+63.      | 36                                | 4.0  |
| STA 734+63. TO STA 745+20.      | 312                               | 34.7   |
| STA 745+20. TO STA 761+57.      | 492                               | 54.7   |
| STA 761+57. TO STA 772+59.      | 336                               | 37.3   |
| STA 772+59. TO STA 775+18.      | 72                                | 8.0  |
| STA 775+18. TO STA 782+30.      | 216                               | 24.0   |
| STA 782+30. TO STA 785+91.      | 108                               | 12.0   |
| STA 785+91. TO STA 793+29.      | 216                               | 24.0   |
| STA 793+29. TO STA 795+82.      | 72                                | 8.0  |
| STA 795+82. TO STA 806+88.      | 336                               | 37.3   |
| STA 806+88. TO STA 832+99.      | 780                               | 86.7   |
| STA 832+99. TO STA 843+88.      | 324                               | 36.0   |
| STA 843+88. TO STA 869+01.      | 756                               | 84.0   |
| STA 869+01. TO STA 879+84.      | 324                               | 36.0   |
| STA 879+84. TO STA 902+48.      | 684                               | 76.0   |
| STA 902+48. TO STA 912+45.      | 300                               | 33.3   |
| STA 912+45. TO STA 916+32.      | 120                               | 13.3   |
| STA 916+32. TO STA 927+51.      | 336                               | 37.3   |
| STA 927+51. TO STA 938+11.      | 324                               | 36.0   |
| STA 938+11. TO STA 943+78.      | 168                               | 18.7   |
| STA 943+78. TO STA 954+81.      | 336                               | 37.3   |
| STA 954+81. TO STA 963+40.      | 252                               | 28.0   |
| STA 963+40. TO STA 967+57.      | 120                               | 13.3   |
| STA 967+57. TO STA 980+00.      | 372                               | 41.3   |
| STA 980+00. TO STA 990+96.      | 324                               | 36.0   |
| STA 990+96. TO STA 996+93.      | 180                               | 20.0   |
| STA 996+93. TO STA 1006+77.     | 300                               | 33.3   |
| STA 1006+77. TO STA 1035+02.    | 852                               | 94.7   |
| STA 1035+02. TO STA 1045+00.    | 300                               | 33.3   |

| LOCATION:<br>STATION TO STATION       | SHORT-TERM<br>PAVEMENT<br>MARKING | WORK<br>ZONE<br>PAVEMENT<br>MARKING<br>REMOVAL |
|---------------------------------------|-----------------------------------|--|
|                                       | FOOT                              | SQ FT  |
| STA 1045+00. TO STA 1051+49.          | 192                               | 21.3   |
| STA 1051+49. TO STA 1062+84.          | 336                               | 37.3   |
| STA 1062+84. TO STA 1084+89.          | 660                               | 73.3   |
| STA 1084+89. TO STA 1095+09.          | 312                               | 34.7   |
| STA 1095+09. TO STA 1095+68.          | 12                                | 1.3  |
| STA 1095+68. TO STA 1103+16.          | 228                               | 25.3   |
| STA 1103+16. TO STA 1106+08.          | 84                                | 9.3  |
| STA 1106+08. TO STA 1113+83.          | 228                               | 25.3   |
| STA 1113+83. TO STA 1115+20.          | 36                                | 4.0  |
| STA 1115+20. TO STA 1124+46.          | 276                               | 30.7   |
| STA 1124+46. TO STA 1158+77.          | 1,032                             | 114.7  |
| STA 1158+77. TO STA 1165+27.          | 192                               | 21.3   |
| STA 1165+27. TO STA 1168+94.          | 108                               | 12.0   |
| STA 1168+94. TO STA 1176+29.          | 216                               | 24.0   |
| STA 1176+29. TO STA 1209+13.          | 984                               | 109.3  |
| STA 1209+13. TO STA 1215+27.          | 180                               | 20.0   |
| STA 1215+27. TO STA 1219+76.          | 132                               | 14.7   |
| STA 1219+76. TO STA 1223+50.          | 108                               | 12.0   |
| STA 1223+50. TO STA 1226+10.          | 84                                | 9.3  |
| STA 1226+10. TO STA 1230+33.          | 132                               | 14.7   |
| STA 1230+33. TO STA 1234+33.          | 120                               | 13.3   |
| STA 1234+33. TO STA 1241+48.          | 216                               | 24.0   |
| STA 1241+48. TO STA 1241+52.          | 0                                 | 0.0  |
| STA 1241+52. TO STA 1247+75.          | 192                               | 21.3   |
| STA 1247+75. TO STA 1252+44.          | 144                               | 16.0   |
| STA 1252+44. TO STA 1258+94.          | 192                               | 21.3   |
| STA 1258+94. TO STA 1270+75.          | 360                               | 40.0   |
| STA 1270+75. TO STA 1280+00.          | 276                               | 30.7   |
| STA 1280+00. TO STA 1282+07.          | 60                                | 6.7  |
| STA 1282+07. TO STA 1286+50.          | 132                               | 14.7   |
| STA 1286+50. TO STA 1290+86.          | 132                               | 14.7   |
| STA 1290+86. TO STA 1297+42.          | 192                               | 21.3   |
| STA 1297+42. TO STA 1301+39.          | 120                               | 13.3   |
| STA 1301+39. TO STA 1312+05.          | 324                               | 36.0   |
| STA 1312+05. TO STA 1331+57.          | 588                               | 65.3   |
| STA 1331+57. TO STA 1341+42.          | 300                               | 33.3   |
| STA 1341+42. TO STA 1342+47.          | 36                                | 4.0  |
| STA 1342+47. TO STA 1352+70.          | 312                               | 34.7   |
| STA 1352+70. TO STA 1356+18.          | 108                               | 12.0   |
| STA 1356+18. TO STA 1363+79.          | 228                               | 25.3   |
| STA 1363+79. TO STA 1364+48.          | 24                                | 3.1  |
| STA 0+12. TO STA 13+05. GLENDALE SPUR | 256                               | 42.7   |
| SUBTOTALS:                            |                                   |  |
| IMPROVEMENT TOTALS:                   |                                   | 24,388      2,724.0                            |



EXISTING CURVE DATA

P. I. STA = 102+22.0  
 $\Delta = 34^{\circ} - 05' - 00''$   
 $D = 1^{\circ} - 36' - 00''$   
 R = 3581.1'  
 T = 1097.1'  
 L = 2129.2'  
 E = 164.3'  
 EXIST. SE. = 0.02 FT/FT  
 PC. STA. 91+24.9  
 PT. STA. 112+54.1

P. I. STA = 29+47.5  
 $\Delta = 4^{\circ} - 49' - 00''$   
 $D = 0^{\circ} - 40' - 00''$   
 R = 8594.42'  
 T = 361.4'  
 L = 722.5'  
 E = 7.61'  
 EXIST. SE. = 0.020 FT/FT  
 PC. STA. 25+86.0  
 PT. STA. 33+08.5

P. I. STA. = 827+93.7  
 $\Delta = 44^{\circ} - 11' - 00''$   
 $D = 3^{\circ} - 00' - 00''$   
 R = 1910.08'  
 T = 775.3'  
 L = 1472.8'  
 E = 151.3'  
 EXIST. S.E. = 0.033 FT/FT  
 P.C. STA. 820+18.4  
 P.T. STA. 834+91.2

P. I. STA. = 861+87.9  
 $\Delta = 19^{\circ} - 00' - 00''$   
 $D = 1^{\circ} - 20' - 00''$   
 R = 4297.3'  
 T = 719.1'  
 L = 1425.0'  
 E = 59.8'  
 EXIST. S.E. = 0.020 FT/FT  
 P.C. STA. 854+68.8  
 P.T. STA. 868+93.8

P. I. STA. = 1230+47.3  
 $\Delta = 29^{\circ} - 58' - 00''$   
 $D = 2^{\circ} - 00' - 00''$   
 R = 2864.93'  
 T = 766.8'  
 L = 1498.3'  
 E = 100.8'  
 EXIST. S.E. = 0.020 FT/FT  
 P.C. STA. 1222+80.5  
 P.T. STA. 1237+78.8

P. I. STA. = 1298+06.5  
 $\Delta = 8^{\circ} - 51' - 00''$   
 $D = 0^{\circ} - 30' - 00''$   
 R = 11459.2'  
 T = 886.8'  
 L = 1770.0'  
 E = 34.27'  
 EXIST. S.E. = 0.020 FT/FT  
 P.C. STA. 1289+19.7  
 P.T. STA. 1306+89.7

P. I. STA. = 45+32.4  
 $\Delta = 3^{\circ} - 49' - 00''$   
 $D = 0^{\circ} - 14' - 00''$   
 R = 24555.4'  
 T = 818.2'  
 L = 1635.7'  
 E = 13.7'  
 EXIST. SE. = 0.000 FT/FT  
 P.C. STA. 37+14.2  
 P.T. STA. 53+49.9

P. I. STA. = 673+23.3  
 $\Delta = 93^{\circ} - 20' - 00''$   
 $D = 3^{\circ} - 12' - 00''$   
 R = 1790.73'  
 T = 1898.0'  
 L = 2916.7'  
 E = 818.66'  
 EXIST. SE. = 0.036 FT/FT  
 P.C. STA. 654+25.3  
 P.T. STA. 683+42.0

P. I. STA. = 887+97.8  
 $\Delta = 48^{\circ} - 56' - 00''$   
 $D = 2^{\circ} - 00' - 00''$   
 R = 2864.9'  
 T = 1303.6'  
 L = 2446.7'  
 E = 282.1'  
 EXIST. S.E. = 0.020 FT/FT  
 P.C. STA. 874+94.2  
 P.T. STA. 899+40.9

P. I. STA. = 997+86.5  
 $\Delta = 31^{\circ} - 42' - 00''$   
 $D = 1^{\circ} - 30' - 00''$   
 R = 3819.83'  
 T = 1084.5'  
 L = 2113.3'  
 E = 151.0'  
 EXIST. S.E. = 0.020 FT/FT  
 P.C. STA. 987+02.0  
 P.T. STA. 1008+15.0

P. I. STA. = 1354+04.5  
 $\Delta = 9^{\circ} - 01' - 00''$   
 $D = 1^{\circ} - 00' - 00''$   
 R = 5729.65'  
 T = 451.8'  
 L = 901.7'  
 E = 17.8'  
 EXIST. S.E. = 0.020 FT/FT  
 P.C. STA. 1349+52.7  
 P.T. STA. 1358+54.4

P. I. STA. = 762+72.8  
 $\Delta = 33^{\circ} - 44' - 00''$   
 $D = 1^{\circ} - 30' - 00''$   
 R = 3819.8'  
 T = 1158.1'  
 L = 2248.9'  
 E = 171.7'  
 EXIST. S.E. = 0.020 FT/FT  
 P.C. STA. 751+14.7  
 P.T. STA. 773+63.6

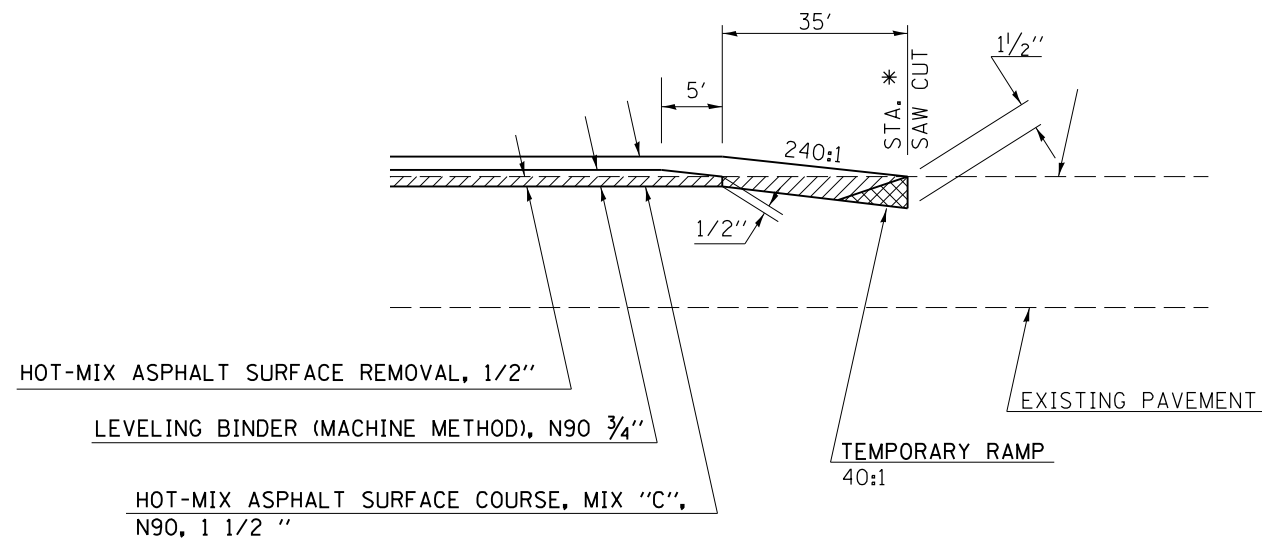
P. I. STA. = 809+31.9  
 $\Delta = 41^{\circ} - 53' - 00''$   
 $D = 3^{\circ} - 30' - 00''$   
 R = 1637.28'  
 T = 626.58'  
 L = 1196.7'  
 E = 115.9'  
 EXIST. S.E. = 0.042 FT/FT  
 P.C. STA. 803+05.3  
 P.T. STA. 815+02.0

P. I. STA. = 1074+44.1  
 $\Delta = 3^{\circ} - 30' - 00''$   
 $D = 0^{\circ} - 07' - 00''$   
 R = 49110.7'  
 T = 1500.1'  
 L = 3000.0'  
 E = 22.89'  
 EXIST. S.E. = 0.000 FT/FT  
 P.C. STA. 1059+44.0  
 P.T. STA. 1089+44.0

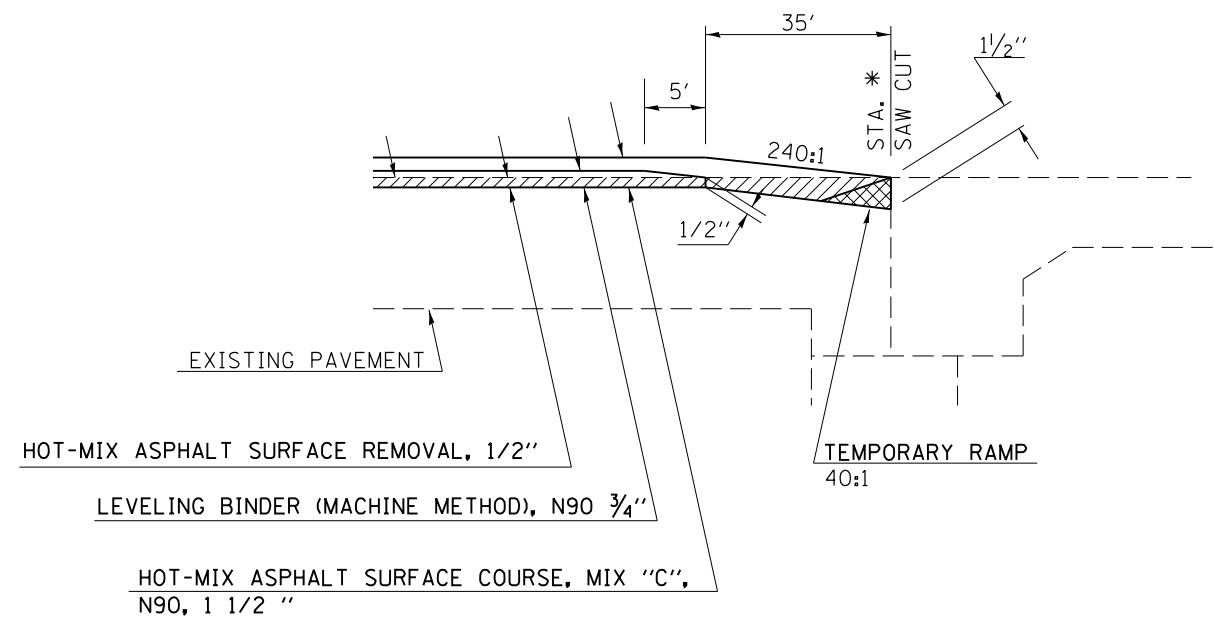
P. I. STA. = 1112+45.3  
 $\Delta = 2^{\circ} - 13' - 00''$   
 $D = 0^{\circ} - 06' - 00''$   
 R = 57295.8'  
 T = 1108.4'  
 L = 2216.7'  
 E = 10.7'  
 EXIST. S.E. = 0.000 FT/FT  
 P.C. STA. 1101+36.9  
 P.T. STA. 1123+53.6

|  |                             |            |           |   |                            |                    |    |                |         |        |                 |                           |
|--|-----------------------------|------------|-----------|---|----------------------------|--------------------|----|----------------|---------|--------|-----------------|---------------------------|
| FILE NAME =  | USER NAME = kranzjc         | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>EXISTING CURVE DATA</b> |                    |    | F.A.P.<br>RTE. | SECTION | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO.              |
| ct:\pw_work\pwidot\kranzjc\10402080\78435-sha-cvr_sq_index.dgn | DRAWN -                     | REVISED -  | •         |   |                            |                    |    | 12RS-3; 10RS-2 | POPE    | 21     | 17              |                           |
| Default  | PLOT SCALE = 94.4444' / in. | CHECKED -  | REVISED - |   | • FAP 132 / FAS 932        | CONTRACT NO. 78436 |    |                |         |        |                 |                           |
|  | PLOT DATE = 6/13/2014       | DATE -     | REVISED - |   | SCALE:                     | SHEET              | OF | SHEETS         | STA.    | TO     | STA.            | ILLINOIS FED. AID PROJECT |

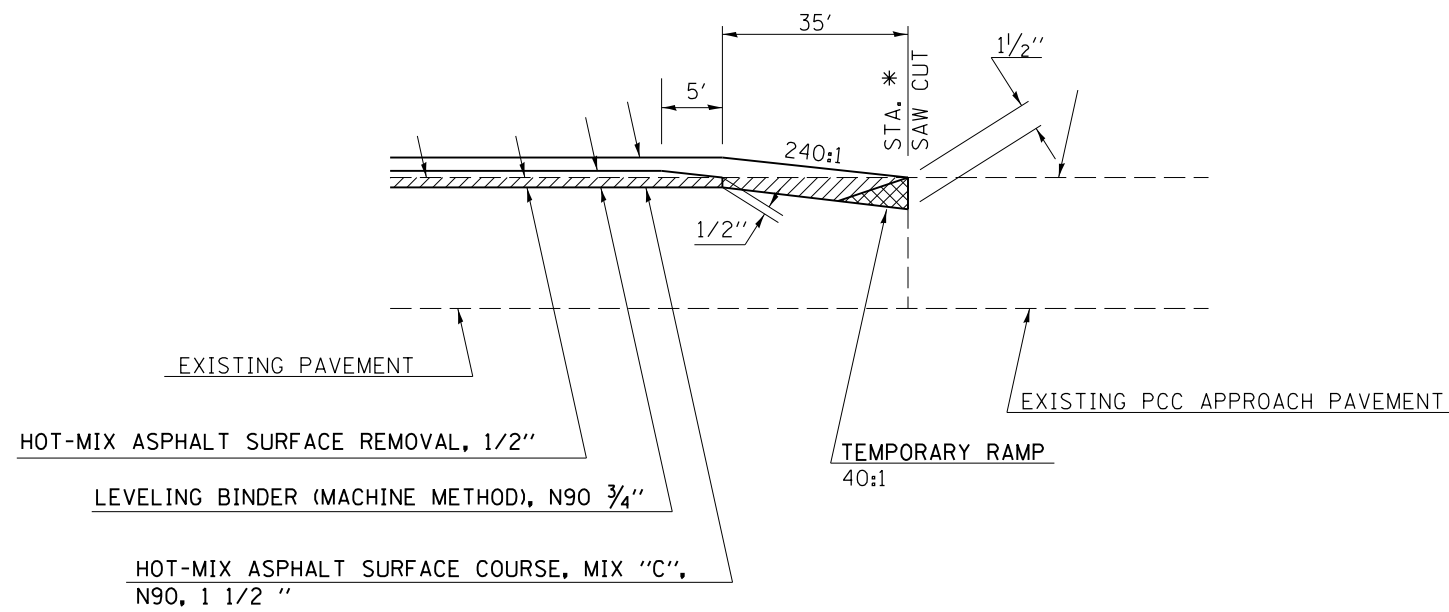
# BUTT JOINT DETAILS



**\* TO BE USED:**  
 STA. 112+75  
 STA. 1364+48



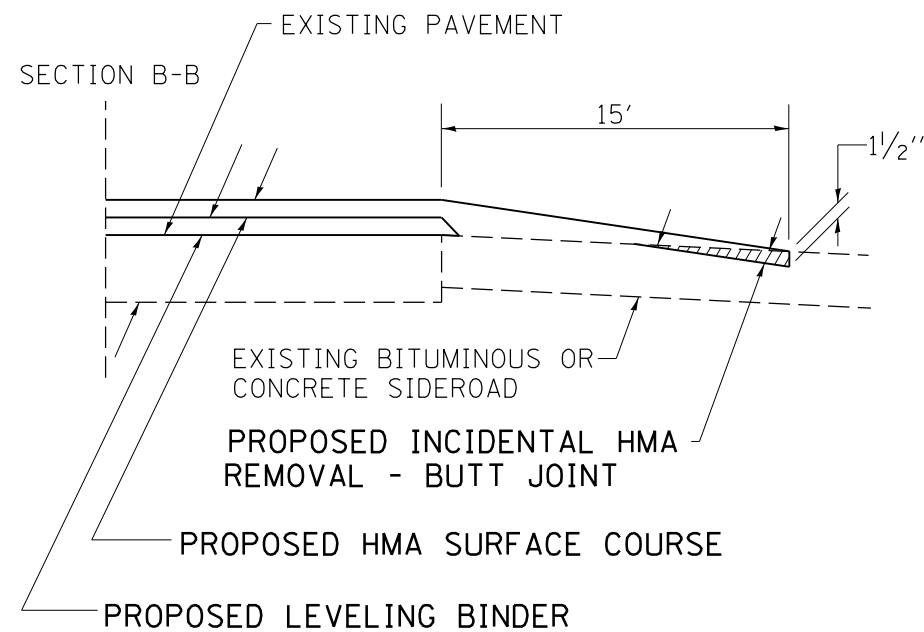
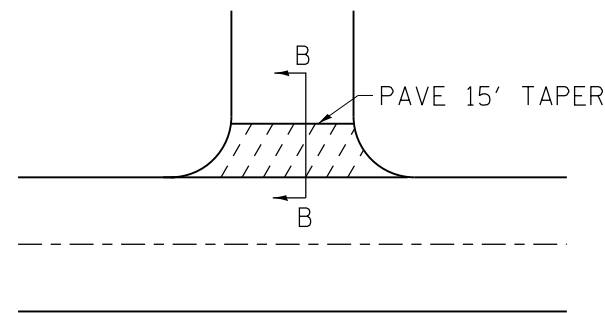
**\* TO BE USED:**  
 STA. 775+62.25 (AT SN076-0011 ABUTMENT)  
 STA. 776+80.25 (AT SN076-0011 ABUTMENT)



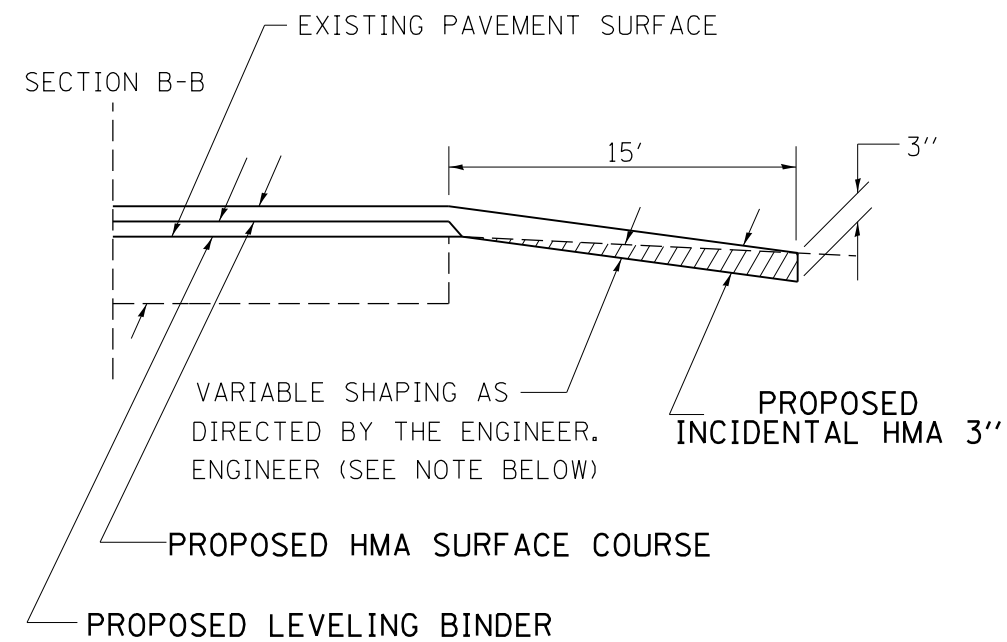
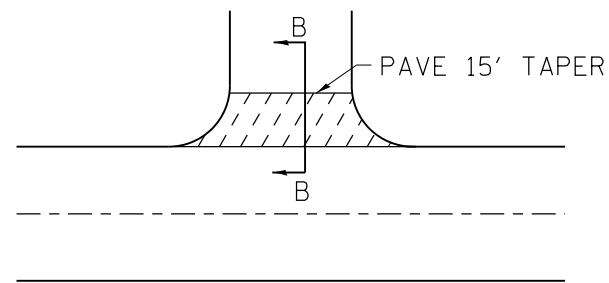
**\* TO BE USED:**  
 STA. 25+52.25 (AT END OF SN076-0013 CONCRETE APPROACH)  
 STA. 23+35.75 (AT END OF SN076-0013 CONCRETE APPROACH)

|  |                              |            |           |   |                            |       |    |                   |         |                           |                 |              |    |
|--|------------------------------|------------|-----------|---|----------------------------|-------|----|-------------------|---------|---------------------------|-----------------|--------------|----|
| FILE NAME =                              | USER NAME = kranzjc          | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>DETAIL: BUTT JOINTS</b> |       |    | F.A.P.<br>RTE.    | SECTION | COUNTY                    | TOTAL<br>SHEETS | SHEET<br>NO. |    |
| ci:\pw_work\pwidot\kranzjc\2402080\78436 | 5-shsta-cvr_sqi.index.dgn    | DRAWN -    | REVISED - |   | SCALE:                     | SHEET | OF | SHEETS            | STA.    | TO STA.                   | POPE            | 21           | 18 |
| Default                                  | PLOT SCALE = 94.4444 ' / in. | CHECKED -  | REVISED - |   |                            |       |    | 12RS-3; 10RS-2    |         | CONTRACT NO. 78436        |                 |              |    |
|  | PLOT DATE = 6/13/2014        | DATE -     | REVISED - |   |                            |       |    | FAP 132 / FAS 932 |         | ILLINOIS FED. AID PROJECT |                 |              |    |

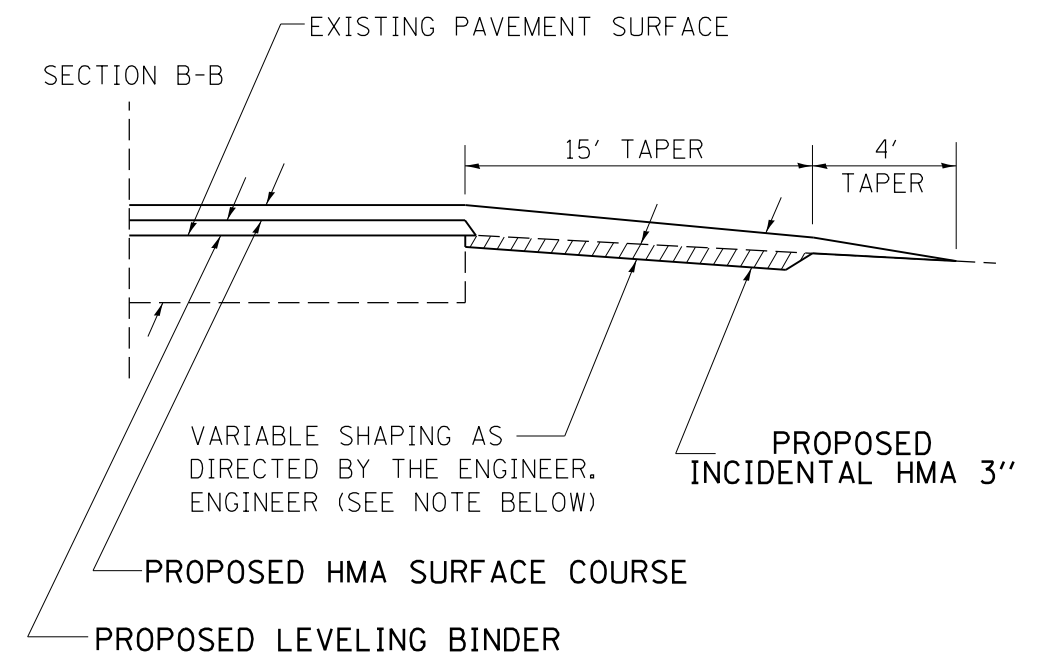
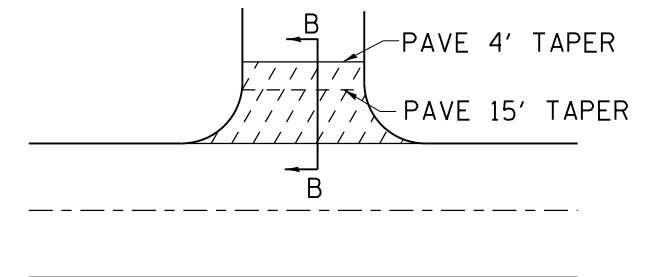
DETAIL OF HMA OR PCC  
SIDEROADS WITH BUTT JOINT



DETAIL OF AGGREGATE  
SIDEROAD



DETAIL OF OIL & CHIP  
SIDEROAD

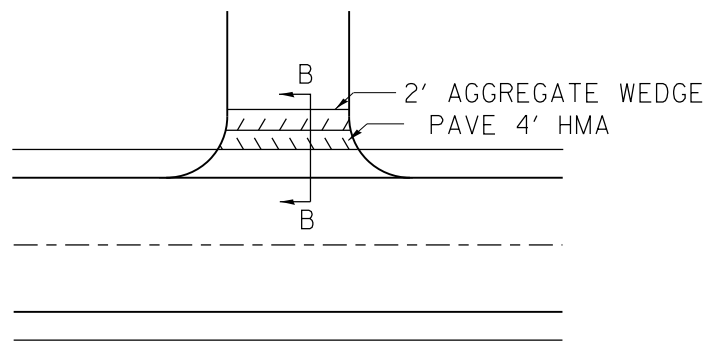


NOTES:

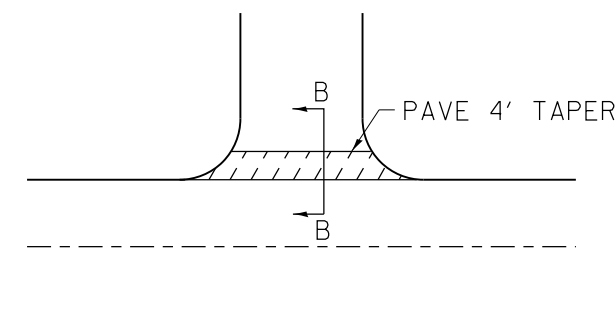
IF EXISTING SUB-BASE IS INADEQUATE, AS DETERMINED BY THE ENGINEER, THE SIDEROADS SHALL BE CORED OUT AND AGGREGATE SUB-BASE, TYPE B SHALL BE PLACED FOR BASE. THE COST OF CORING OUT THE SIDEROAD SHALL BE INCLUDED IN THE COST OF THE AGGREGATE BASE COURSE. IF EXISTING SUB-BASE IS DETERMINED TO BE ADEQUATE, THE AGGREGATE BASE COURSE SHALL BE DELETED AND THE PREPARATION OF THE BASE SHALL BE CONSTRUCTED ACCORDING TO ARTICLE 406.09  
VARIABLE SHAPING IS INCLUDED IN THE COST OF BITUMINOUS CONCRETE SURFACE COURSE.

|             |                                      |            |           |   |                                     |  |  |                     |                |                    |                 |              |
|-------------|--------------------------------------|------------|-----------|---|-------------------------------------|--|--|---------------------|----------------|--------------------|-----------------|--------------|
| FILE NAME = | USER NAME = kranzjc                  | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>DETAILS: SIDEROADS</b>           |  |  | F.A.P.<br>RTE.      | SECTION        | COUNTY             | TOTAL<br>SHEETS | SHEET<br>NO. |
| Default     | Plot Scale = 94.4444' / 1" / 1" / 1" | DRAWN -    | REVISED - |   |                                     |  |  | •                   | 12RS-3; 10RS-2 | POPE               | 21              | 19           |
|             | PLOT DATE = 6/13/2014                | CHECKED -  | REVISED - |   | SCALE: SHEET OF SHEETS STA. TO STA. |  |  | • FAP 132 / FAS 932 |                | CONTRACT NO. 78436 |                 |              |
|             |                                      | DATE -     | REVISED - |   | ILLINOIS FED. AID PROJECT           |  |  |                     |                |                    |                 |              |

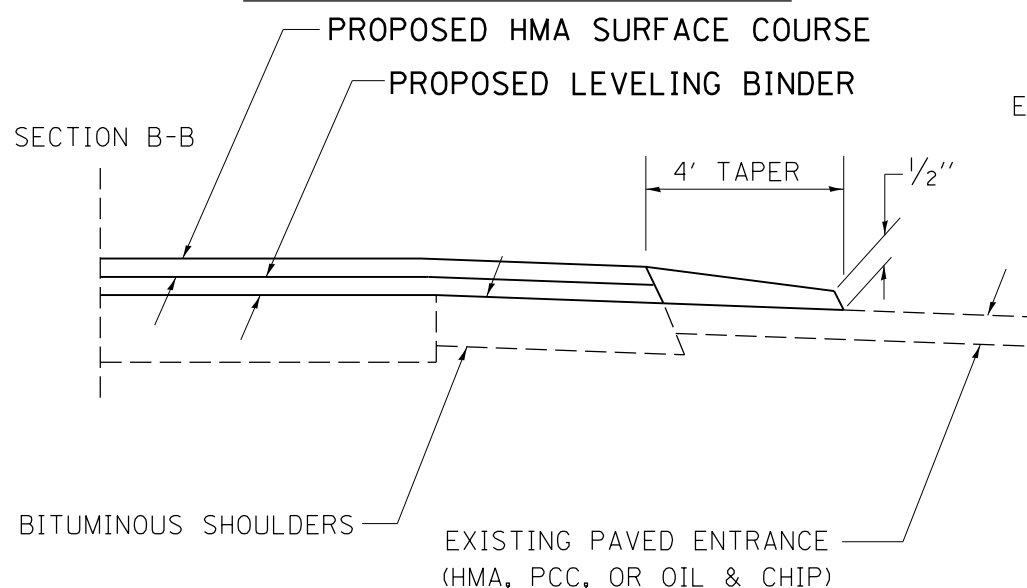
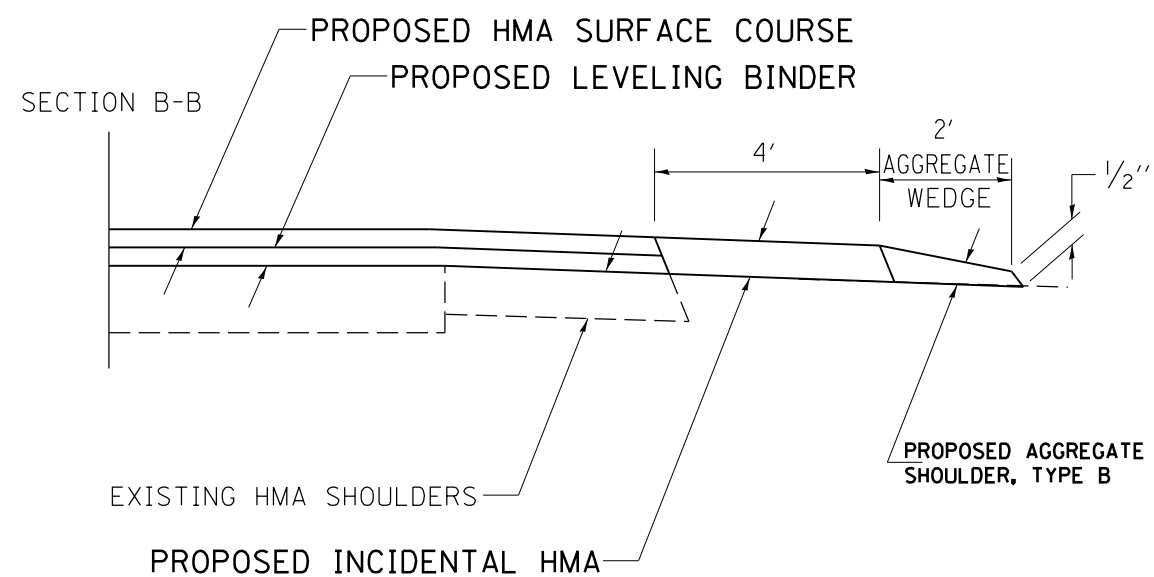
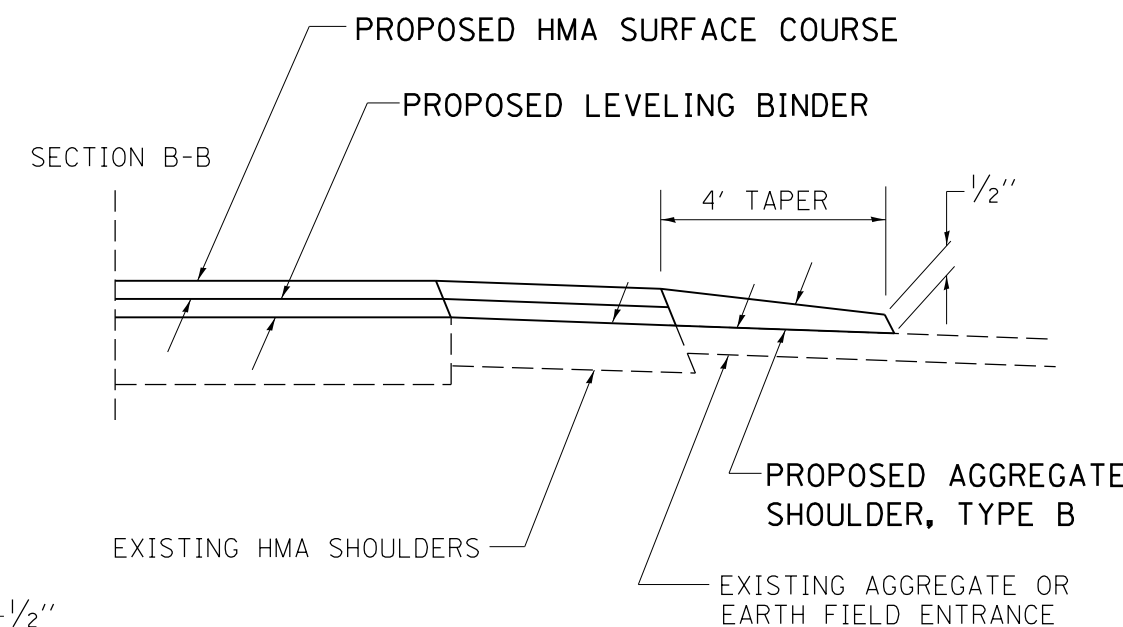
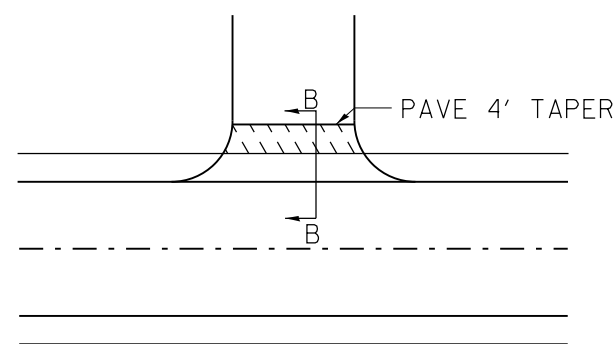
DETAIL OF AGGREGATE  
PRIVATE OR COMMERCIAL  
ENTRANCE



DETAIL OF AGGREGATE OR EARTH  
FIELD ENTRANCE



DETAIL OF HMA, PCC, OR OIL & CHIP  
PRIVATE OR COMMERCIAL ENTRANCE



NOTES:

PREPARATION OF EXISTING SURFACE AND ANY EXCAVATION FOR ENTRANCES SHALL BE IN ACCORDANCE WITH ARTICLE 406.09 OF THE STANDARD SPECIFICATIONS.

|  |                            |            |           |
|--|----------------------------|------------|-----------|
| FILE NAME =  | USER NAME = kranzjc        | DESIGNED - | REVISED - |
| ci:\pw_work\pwidot\kranzjc\1402080\78436-shsta-cvr-soq-index.dgn |                            | DRAWN -    | REVISED - |
|  | PLOT SCALE = 94.4444' / 1" | CHECKED -  | REVISED - |
| Default  | PLOT DATE = 6/13/2014      | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS: ENTRANCES**

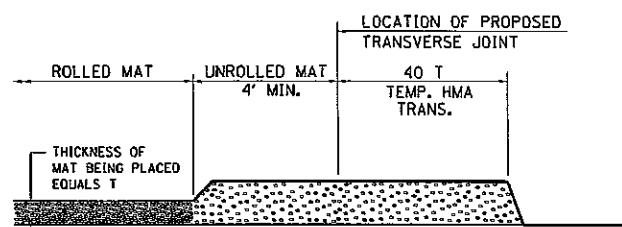
SCALE: SHEET OF SHEETS STA. TO STA.

|                           |                |        |                    |           |
|---------------------------|----------------|--------|--------------------|-----------|
| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS       | SHEET NO. |
| •                         | 12RS-3; 10RS-2 | POPE   | 21                 | 20        |
| • FAP 132 / FAS 932       |                |        | CONTRACT NO. 78436 |           |
| ILLINOIS FED. AID PROJECT |                |        |                    |           |

# TEMPORARY HOT-MIX ASPHALT TRANSITIONS

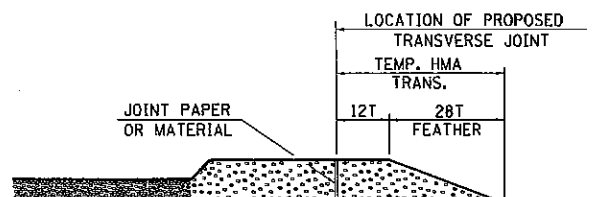
# UNEVEN LANES SIGN

W8-11 (48" x 48")



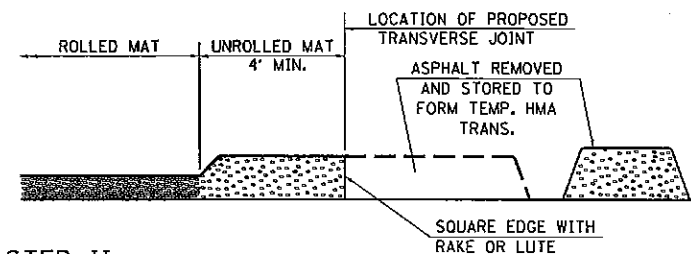
### STEP I

1. PLACE HOT-MIX ASPHALT MAT, LENGTH 40 TIMES THE THICKNESS OF THE MAT BEING PLACED PAST THE PROPOSED TRANSVERSE JOINT LOCATION USING NORMAL OPERATING PROCEDURES.
2. EXTREME CARE SHOULD BE TAKEN TO MAINTAIN ENOUGH MATERIAL IN FRONT OF THE SCREED TO MAINTAIN REQUIRED PAVING DEPTH.



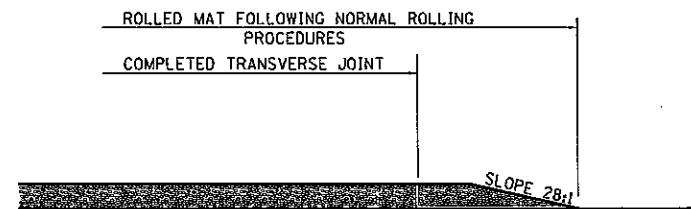
### STEP III

1. JOINT PAPER OR OTHER PRESELECTED JOINT MATERIAL IS THEN PLACED IN THE CLEARED AREA AND THE EXCESS ASPHALT USED TO HAND FORM A TRANSITION TO THE DIMENSIONS SHOWN ABOVE.
2. NOTE THAT IN CONSTRUCTING THE TRANSITION, THE MAT DEPTH IS CONTINUED AS PART OF THE TRANSITION BEFORE FORMING THE FEATHER.



### STEP II

1. MOVE THE PAVER OUT OF THE WAY AND REMOVE THE ASPHALT FROM THE AREA OF THE PROPOSED TEMPORARY HOT-MIX ASPHALT TRANSITION.
2. SQUARE UP THE END OF THE MAT WITH A RAKE OR LUTE.
3. NOTE THAT THE MAT WITHIN 4' OF THE END OF JOINT IS NOT TO BE ROLLED AT THIS TIME.



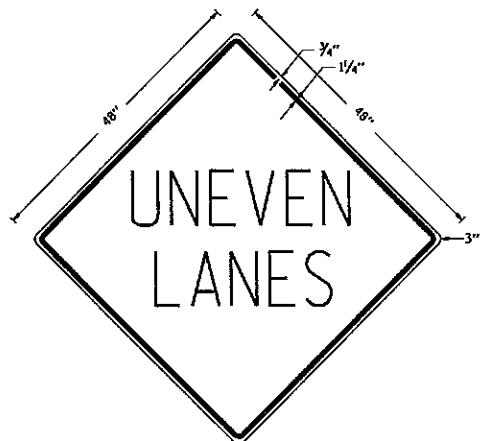
### STEP IV

1. COMPLETE TEMPORARY TRANSITION BY ROLLING.
2. TO RESUME PAVING, AT THE JOINT, REMOVE TEMPORARY TRANSITION AND DISPOSE OF THE MATERIAL ACCORDING TO ART. 202.03 OF THE STD. SPECS. (COST INCLUDED IN THE CONTRACT).
3. CONSTRUCTING THE TEMPORARY TRANSITIONS WILL NOT BE PAID FOR SEPARATELY IN ACCORDANCE WITH ARTICLE 406.14 OF THE STANDARD SPECIFICATIONS.

#### REVISIONS

|         |          |
|---------|----------|
| REDRAWN | 2-15-89  |
| REVISED | 8-16-94  |
| REVISED | 01-09-07 |
| RESIZED | 05-8-08  |
| REVISED | 05-16-13 |

STD. 9-26



#### COLORS:

LEGEND AND BORDER - BLACK NON-REFLECTORIZED  
BACKGROUND - ORANGE REFLECTORIZED

NOTE: PRIOR TO ALLOWING TRAFFIC ON ANY PORTION OF THE ROADWAY THAT HAS BEEN COLDMILLED OR BEFORE RESURFACING OPERATIONS BEGIN, THE CONTRACTOR SHALL HAVE ERECTED "UNEVEN PAVEMENT" SIGNS THAT CONFORM TO THE ABOVE DETAILS. A MINIMUM OF ONE SIGN AT EACH END OF THE IMPROVEMENT WILL BE REQUIRED. THE CONTRACTOR SHALL MAINTAIN THE "UNEVEN PAVEMENT" SIGNS UNTIL THE RESURFACING OPERATIONS ARE COMPLETED.

IF AT ANY TIME THE SIGNS ARE IN PLACE BUT NOT APPLICABLE, THEY SHALL BE TURNED FROM THE VIEW OF MOTORISTS OR COVERED AS DIRECTED BY THE ENGINEER.

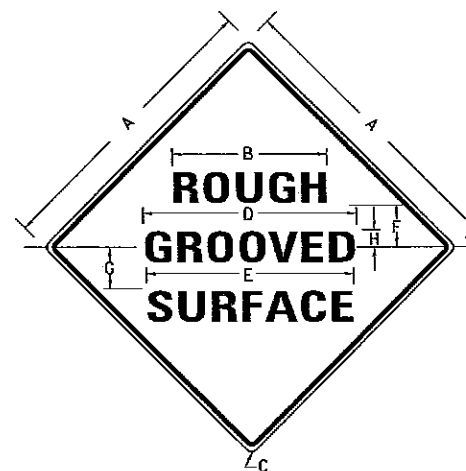
THE COST OF FURNISHING, ERECTING, MAINTAINING, AND REMOVING THE REQUIRED SIGNS SHALL BE INCLUDED IN THE CONTRACT.

#### REVISIONS

|            |         |
|------------|---------|
| DRAWN      | 2-15-89 |
| REVISED    | 4-06-93 |
| REDESIGNED | 7-23-04 |
| RESIZED    | 5-08-08 |
| REVIEWED   | 5-17-13 |

STD. 9-41

# ILLINOIS STANDARD



#### COLORS:

LEGEND AND BORDER- BLACK NON-REFLECTORIZED  
BACKGROUND- ORANGE REFLECTORIZED

| SIGN SIZE | DIMENSIONS |      |     |      |      |     |      |     |
|-----------|------------|------|-----|------|------|-----|------|-----|
|           | A          | B    | C   | D    | E    | F   | G    | H   |
| 48X48     | 48.0       | 24.1 | 3.0 | 34.0 | 33.0 | 6.0 | 13.0 | 3.5 |

| SIGN SIZE | SERIES LINES |    |    | MAR-GIN | BOR-DER | BLANK STD. |
|-----------|--------------|----|----|---------|---------|------------|
|           | 1            | 2  | 3  |         |         |            |
| 48X48     | 7C           | 7C | 7C | 0.8     | 1.2     | B4-48D     |

ALL DIMENSIONS IN INCHES

#### NOTES:

PRIOR TO ALLOWING TRAFFIC ON ANY PORTION OF THE ROADWAY THAT HAS BEEN COLDMILLED, THE CONTRACTOR SHALL HAVE ERECTED "ROUGH GROOVED SURFACE" SIGNS THAT CONFORM TO THE ABOVE DETAILS. A MINIMUM OF ONE SIGN AT EACH END OF THE IMPROVEMENT WILL BE REQUIRED. THE CONTRACTOR SHALL MAINTAIN THE "ROUGH GROOVED SURFACE" SIGNS UNTIL THE COLDMILLED SURFACE IS COVERED WITH LEVELING BINDER OR SURFACE COURSE.

IF AT ANY TIME THE SIGNS ARE IN PLACE BUT NOT APPLICABLE, THEY SHALL BE TURNED FROM THE VIEW OF MOTORISTS OR COVERED AS DIRECTED BY THE ENGINEER.

THE COST OF FURNISHING, ERECTING, MAINTAINING, AND REMOVING THE REQUIRED SIGNS SHALL BE INCLUDED IN THE CONTRACT.

#### REVISIONS

|          |         |
|----------|---------|
| REDRAWN  | 2-15-89 |
| REVISED  | 4-6-93  |
| REVISED  | 3-27-08 |
| REVIEWED | 5-17-13 |

STD. 9-39

|                                       |                              |            |           |
|---------------------------------------|------------------------------|------------|-----------|
| FILE NAME =                           | USER NAME = kranzje          | DESIGNED - | REVISED - |
| c:\pwork\pwork\kranzje\08402088\78435 | sho-cvr_sq_index.dgn         | DRAWN -    | REVISED - |
| Default                               | PLOT SCALE = 100.0000' / in. | CHECKED -  | REVISED - |
|                                       | PLOT DATE = 6/11/2014        | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DETAILS: TEMP. HMA TRANSITIONS,  
UNEVEN LANES SIGN, ROUGH GROOVED SURFACE SIGN

SCALE: SHEET OF SHEETS STA. TO STA.

|                           |                 |                    |              |           |
|---------------------------|-----------------|--------------------|--------------|-----------|
| F.A.P. RTE.               | SECTION         | COUNTY             | TOTAL SHEETS | SHEET NO. |
| *                         | 12RS-3 / 10RS-2 | POPE               | 21           | 21        |
| * FAP 132 / FAS 932       |                 | CONTRACT NO. 78436 |              |           |
| ILLINOIS FED. AID PROJECT |                 |                    |              |           |