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- 420501-04 PCC PVT. AND PCC BASE COURSE ADJACENT TO R.R. GRADE CROSSING
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PRINCIPAL ARTERIAL TWS-6 2050-2900
 DESIGN TRAFFIC YEAR 2026 IS 2,360 DHV
 DESIGN SPEED = 30 MPH
 CLOSED ADT* < 200

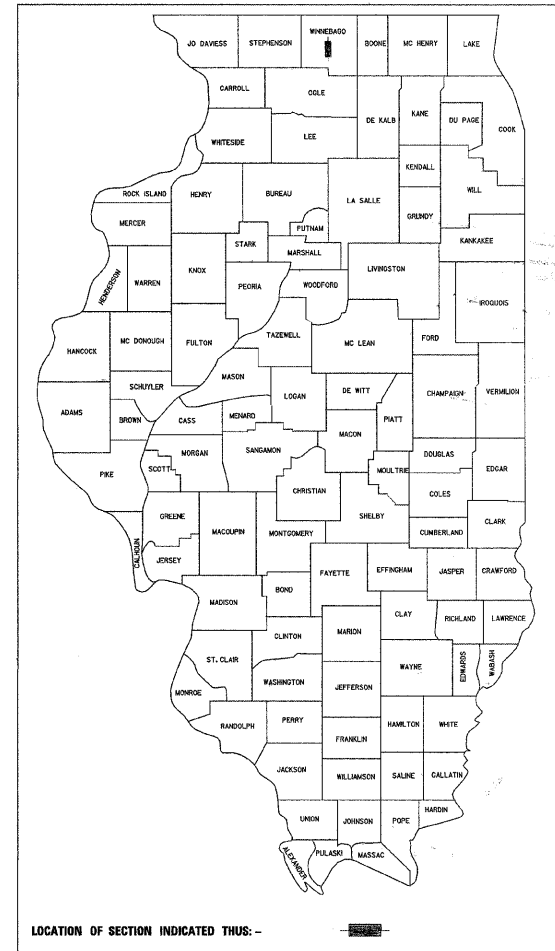
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PLANS FOR
 PROPOSED LOCAL AGENCY IMPROVEMENT
 FEDERAL-AID BRIDGE REPLACEMENT &
 REHABILITATION PROGRAM**

SCALES: PLAN 1" = 20'
 PROFILE HORIZ. 1" = 20'
 PROFILE VERT. 1" = 5'
 CROSS SECTIONS 1" = 5'

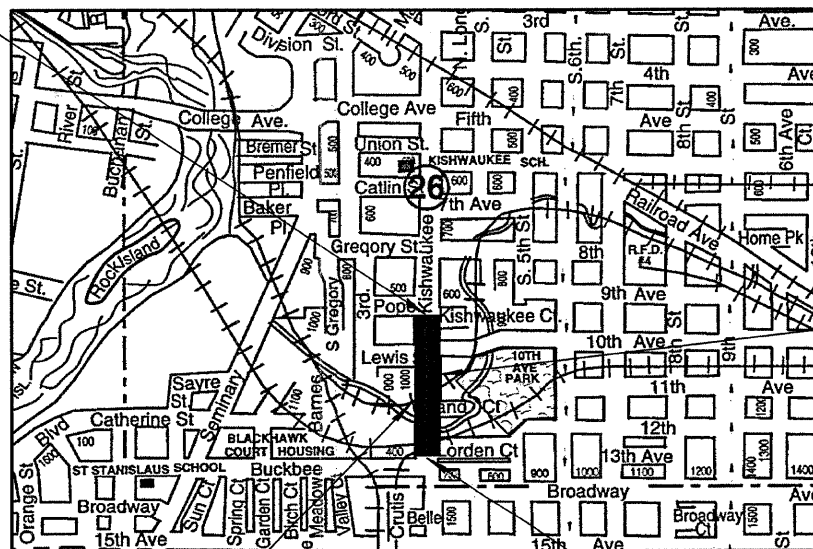
**F.A.P. ROUTE 734
 SECTIONS 98-00480-00-BR AND 03-00522-00-TL
 IL. RT. 251 KISHWAUKEE STREET
 WINNEBAGO COUNTY
 CITY OF ROCKFORD
 PROJECT NO. BRF-734 (37)
 JOB NO. C-92-079-05
 CONTRACT NO. 85393**

2008



**SECTION ENDS
 (POPE STREET)
 STA. 428+38.84**

NET LENGTH OF SECTION = 1,252.41 FEET = 0.237 MILES



**RR OMISSION
 417+78.72 - 417+97.27
 18.55'**

LOCATION MAP

**SECTION BEGINS
 (BUCKBEE STREET)
 STA. 415+67.88**



PROPOSED STRUCTURE

S.N. 101-6160 TRIPLE BARREL
 (11'X11') PRECAST CONCRETE
 BOX CULVERT AT STATION
 423+30 SKEW 0°



Jerry Hinficks
 4-17-08
 Lic. Exp. 11-30-09

APPROVED April 16 2008
Wendy Williams, P.E. LOCAL AGENCY OFFICIAL

PASSED JUNE 5 2008
John T. Nelson DISTRICT ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
 BASED ON LIMITED REVIEW JUNE 5 2008
George F. Ryan DEPUTY DIRECTOR OF HIGHWAYS, REGION 2 ENGINEER

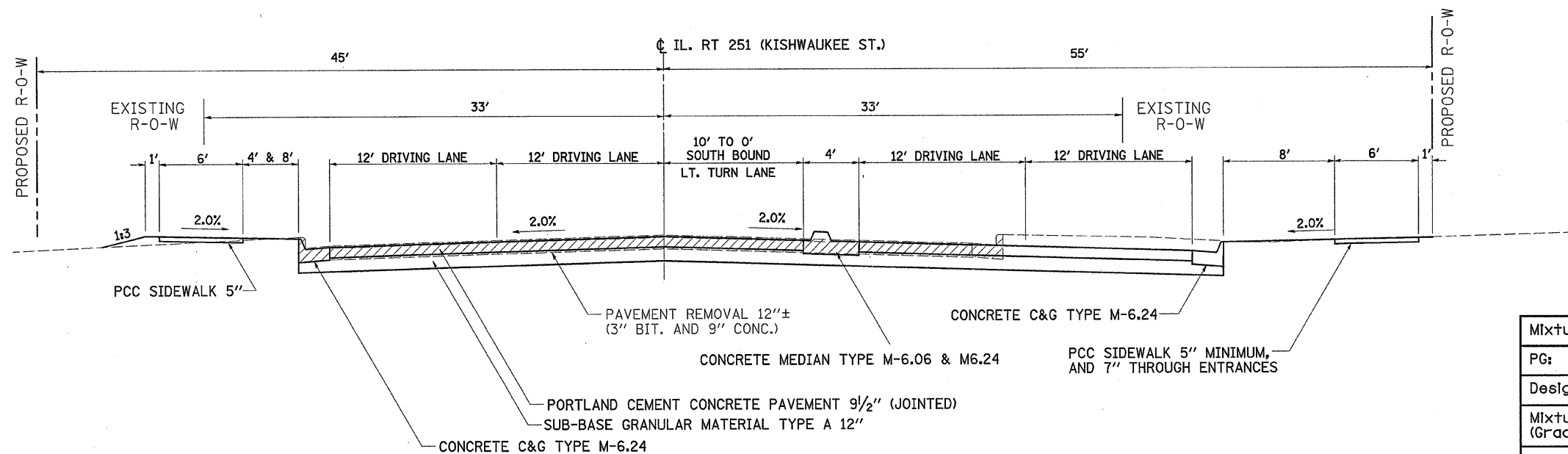
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION



TYPICAL SECTIONS

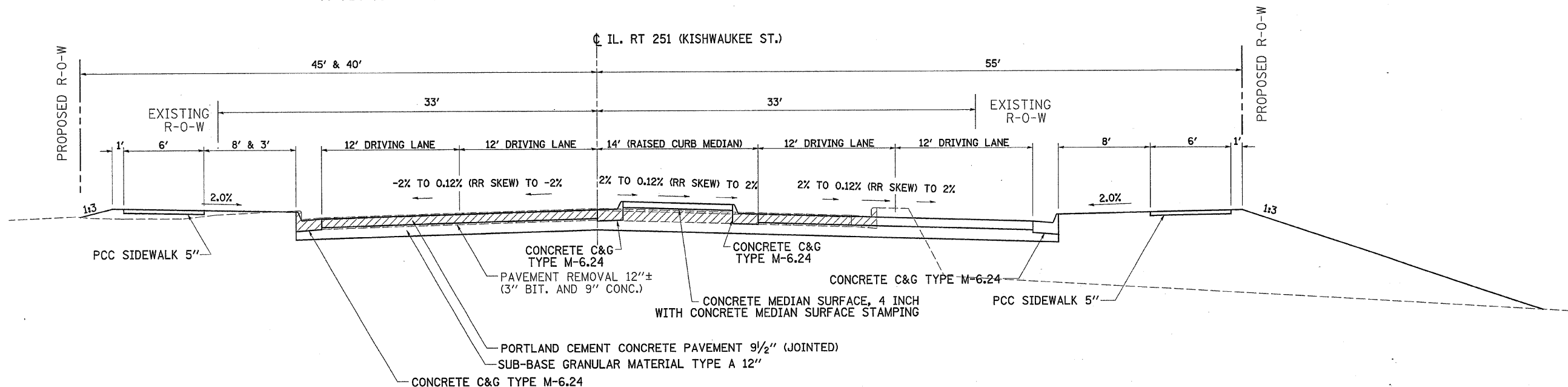
| | | | | |
|---------------------------|----------------------------------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 3 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ | | ILLINOIS FED. AID PROJECT | | |

STA. 415+67.88 TO 417+57.80



| | MAINLINE | DRIVEWAYS |
|-----------------------------------------|-----------|----------------|
| Mixture Uses(s): | Surface | Surface |
| PG: | PG 64-22 | PG 58-22 |
| Design Air Voids | 4.2 @ N70 | 3.0 @ N50 |
| Mixture Composition (Gradation Mixture) | IL 9.5 | IL 9.5 or 12.5 |
| Friction Aggregate | D | C |
| 20 Year ESAL | 6.9 | 2.8 |

STA. 417+57.80 TO 417+78.72 & 417+97.27 TO 418+30
(R.R. OMISSION 417+78.72 TO 417+97.27)

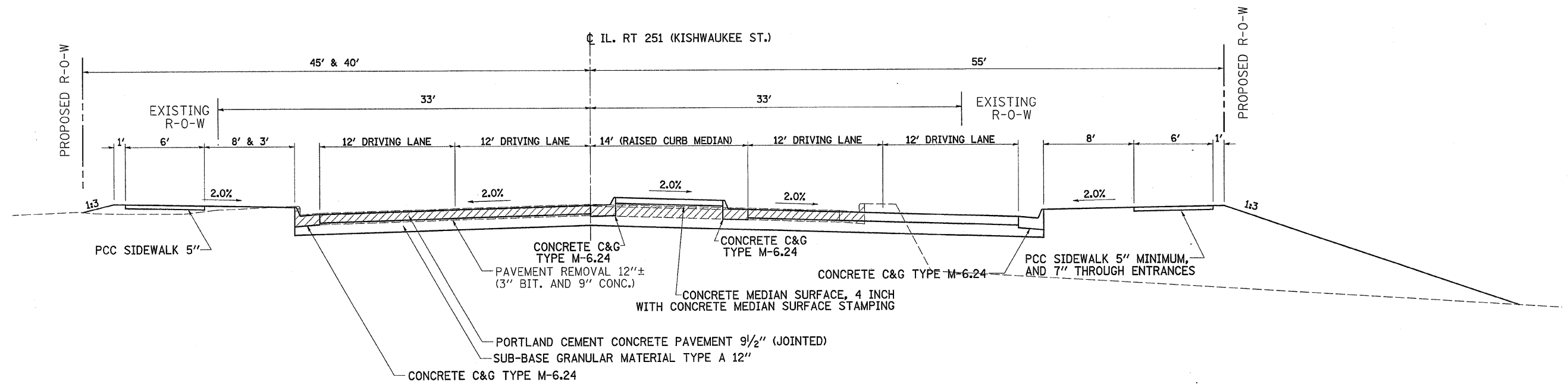


DATE TIME
BY
APP
REF

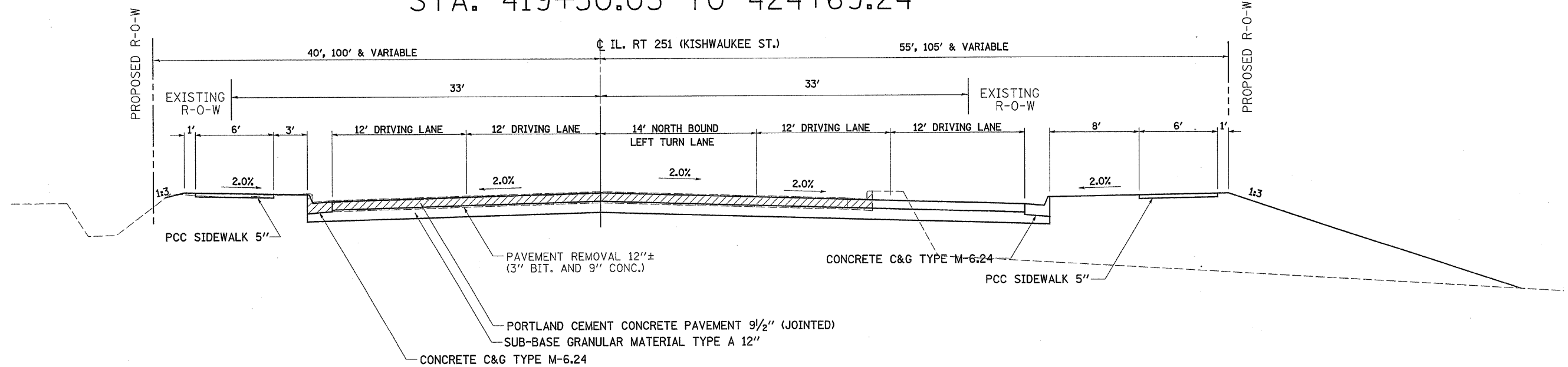
TYPICAL SECTIONS

| | | | | |
|--------------------------|----------------------------------|-----------------------------------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00822-00-TL | WINNEBAGO | 62 | 4 |
| STA. _____ TO STA. _____ | | FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT | | |

STA. 418+30 TO 419+50.05



STA. 419+50.05 TO 424+69.24

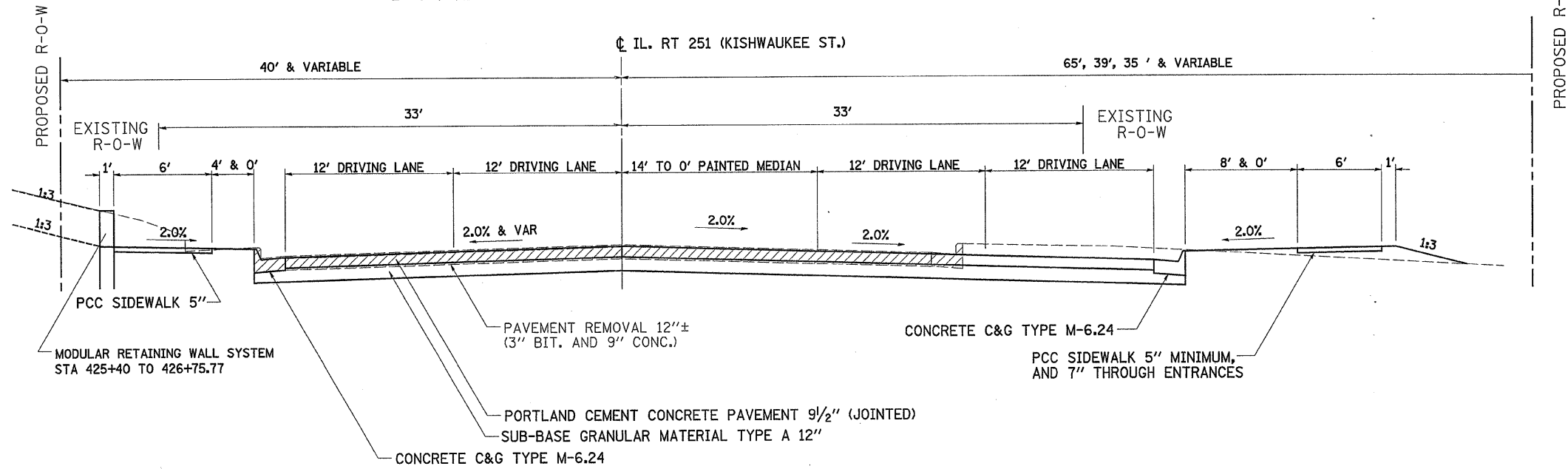


DATE: TIME:
 DRAWN: SPEC:
 PREP: REF:
 REF: REF:

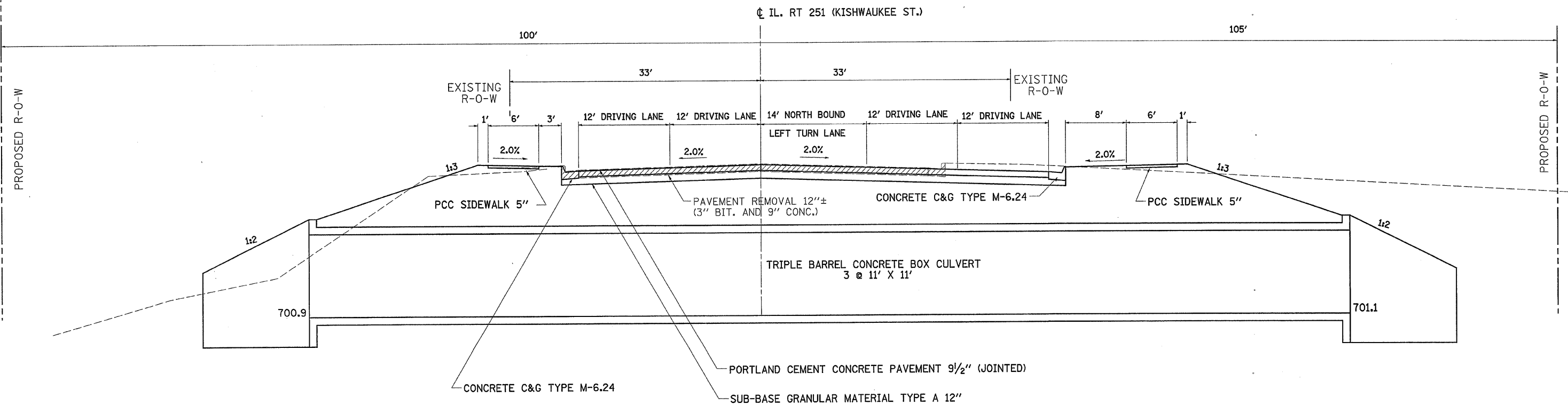
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|-------------------------------------------------|----------------------------------|---------------|--------------|-----------|
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| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT | | | | |

TYPICAL SECTIONS

STA. 424+69.24 TO 428+38.84



STA. 423+30

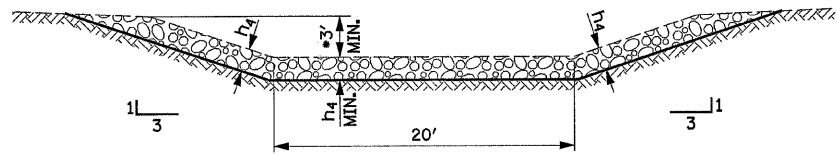


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BY
APP
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REF

| | | | | |
|---------------------------|----------------------------------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 6 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ | | ILLINOIS FED. AID PROJECT | | |

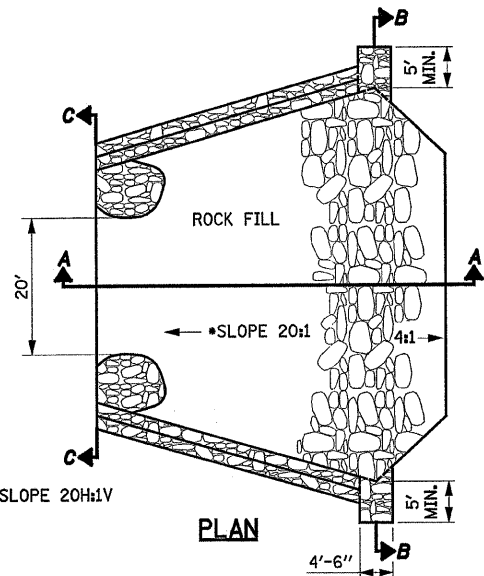
TYPICAL SECTIONS

IL. RT 251 (KISHWAUKEE ST.) CHANNEL DETAILS

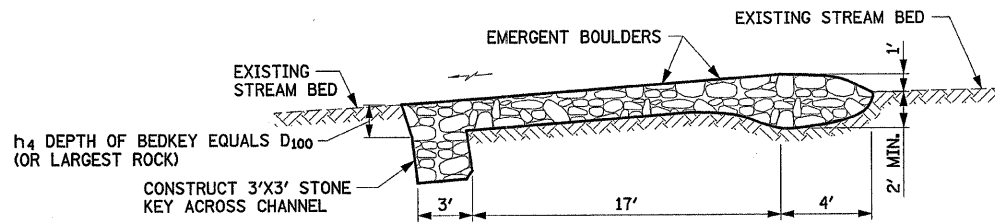


SECTION C-C

* EXTEND ROCK RIPRAP TO TOP OF PRIMARY CHANNEL BANK OR 3.0 FEET, WHICHEVER IS GREATER



PLAN

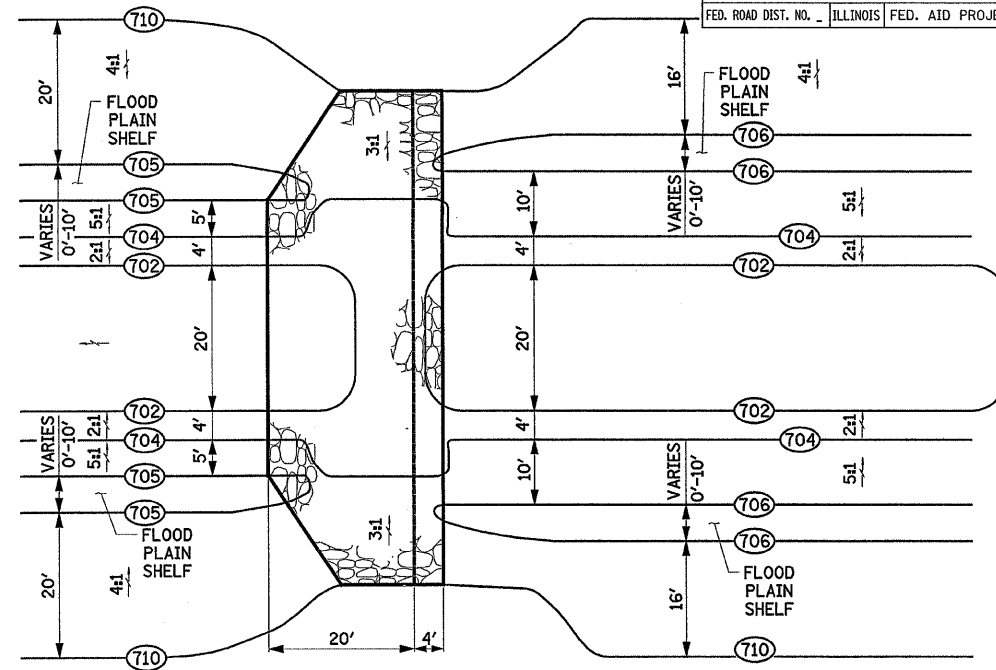


SECTION A-A

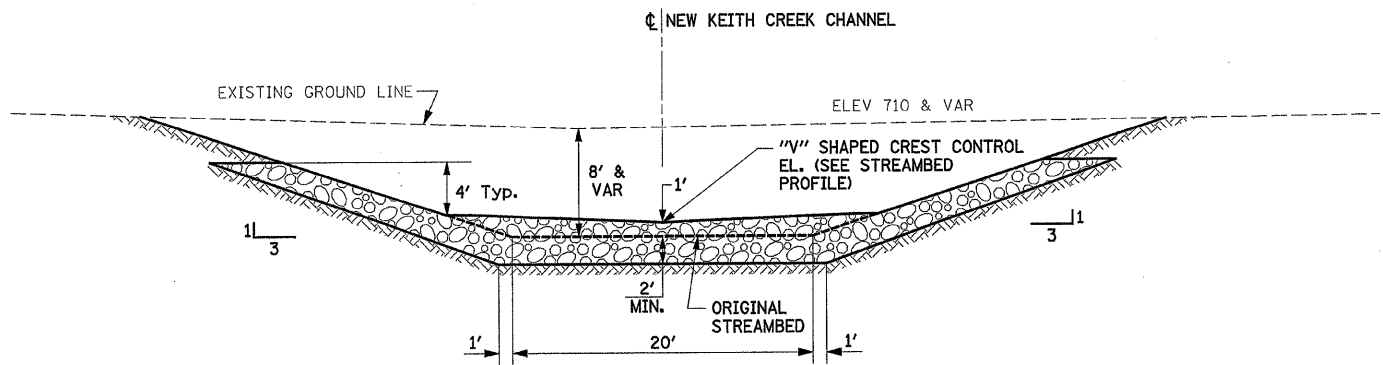
REFERENCE TABLE

| IDOT CLASS | h_{14} (D_{100}) | D_{50} |
|------------|------------------------|----------|
| 3 | 10.2 in. | 6.0 in. |

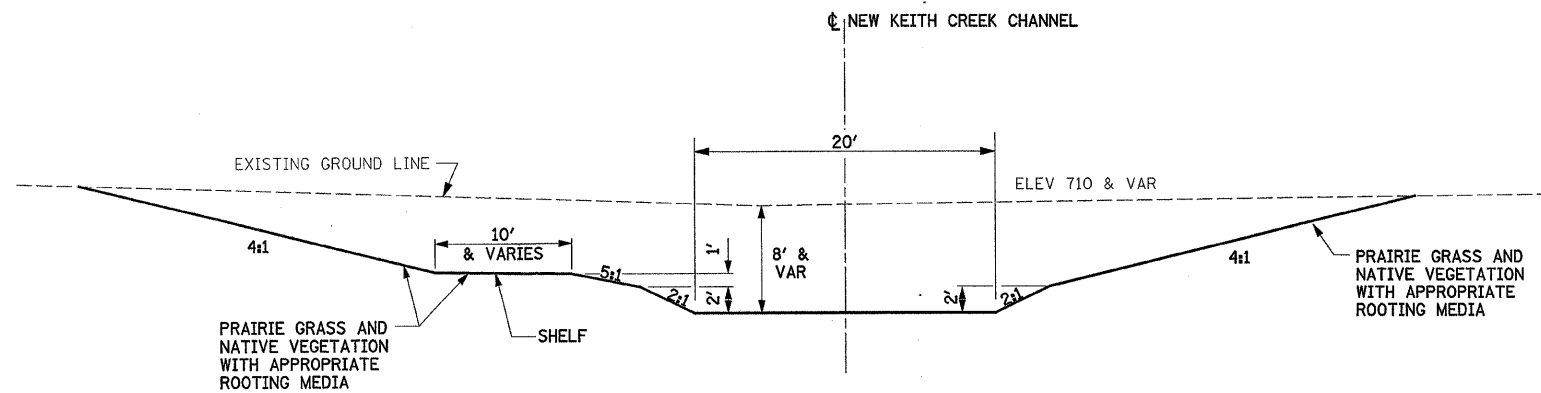
NOTE:
ROCK GRADATION USED SHALL MEET IDOT REQUIREMENTS FOR GRADATION 3 RIPRAP QUALITY DESIGNATION "A" OF "B" ($D_{50} = 6.0$ in.) USE LARGEST INDIVIDUAL STONES FROM AVAILABLE MATERIAL AS SHOWN FOR THE EMERGENT BOULDERS AND CREST STONE. TO BE VERIFIED.



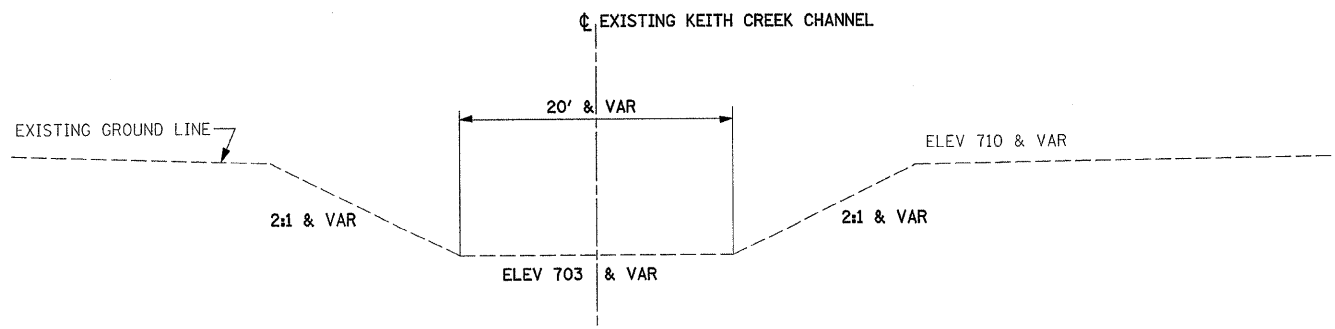
TYPICAL TRANSITION AT RIFFLE



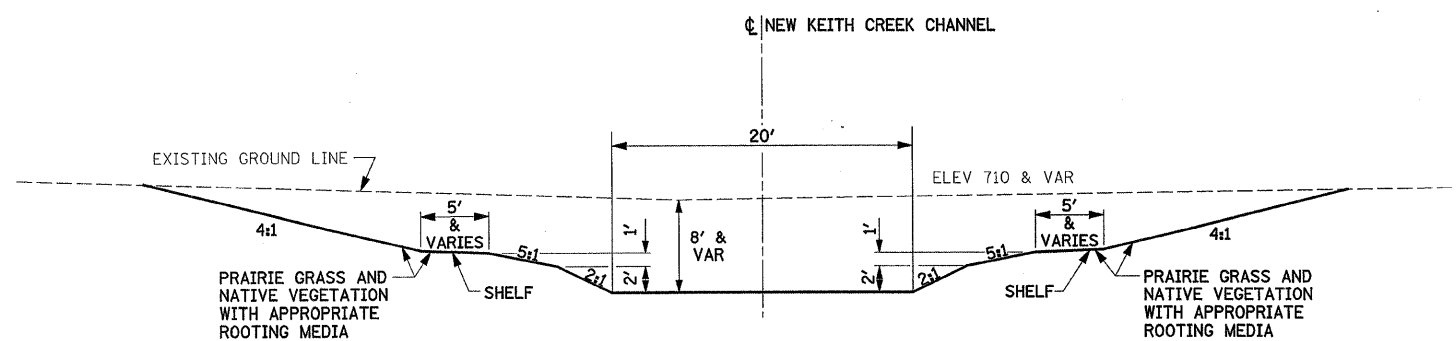
SECTION B-B



TYPICAL SECTIONS BETWEEN RIFFLE STRUCTURES



EXISTING CHANNEL SECTION



DATE-TIME
ADDSPEC
PREP
REF
REF

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------------------------|---------------------------|--------------|-----------|
| FAPT34 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 7 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ | | ILLINOIS FED. AID PROJECT | | |

TYPICAL SECTIONS AND GENERAL NOTES

GENERAL NOTES

EXISTING STRUCTURES (INCLUDING FOUNDATIONS, WALLS, CISTERNS, WELLS OR OTHER UNDERGROUND STRUCTURES) WITHIN THE RIGHT OF WAY SHALL BE REMOVED IN ACCORDANCE WITH ARTICLE 501.04 AND 501.05 OF THE STANDARD SPECIFICATIONS, WITHOUT ADDITIONAL COMPENSATION, UNLESS OTHERWISE NOTED IN THE PLANS OR SPECIAL PROVISIONS.

THE CONTRACTOR SHALL SEED ALL DISTURBED AREAS WITHIN THE PROJECT LIMITS.

NO OVERHAUL HAS BEEN COMPUTED AND NONE SHALL BE PAID FOR FROM ANY SOURCE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY DURING CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS.

A MINIMUM OF 48 HOURS ADVANCE NOTICE IS REQUIRED FOR NON-EMERGENCY WORK.

WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS, MONUMENTS AND RIGHT-OF-WAY PINS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR, OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION.

EXISTING MAIL BOXES, STREET SIGNS AND TRAFFIC SIGNS THAT ARE WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED AND RESET BY THE CONTRACTOR. COST OF REMOVING AND RESETTING TO BE INCLUDED IN THE CONTRACT UNIT PRICE BID PER CUBIC YARD FOR EARTH EXCAVATION (SPECIAL).

ALL STREET SIGNS SHALL BE SAVAGED AND RELOCATED AS DIRECTED BY THE ENGINEER, COST INCLUDED IN COST PER CUBIC YARD OF EARTH EXCAVATION (SPECIAL).

ALL WATER MAIN, WATER SERVICES AND STORM SEWER SHALL BE INSTALLED PRIOR TO SANITARY SEWER TESTING.

SERVICE RE-CONNECTIONS MAY ONLY BE PERFORMED BY A FIRM OR INDIVIDUAL THAT IS REGISTERED AND BONDED WITH THE DISTRICT AS A PRIVATE PROPERTY PLUMBING CONTRACTOR. FORMS AVAILABLE AT DISTRICT ENGINEERING DEPARTMENT.

ALL ROADWAY AND CONSTRUCTION SHALL BE COMPLETED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS (CURRENT ADDITION), AND THE REQUIREMENTS OF THE CITY OF ROCKFORD.

ALL WATER AND SEWER MAINS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS (CURRENT EDITION), THE REQUIREMENTS OF THE ROCK RIVER WATER RECLAMATION DISTRICT AND THE CITY OF ROCKFORD WATER DEPARTMENT.

UPON COMPLETION OF CONSTRUCTION, ALL SANITARY MANHOLE CASTINGS SHALL BE EXPOSED AND SET TO FINAL GRADE.

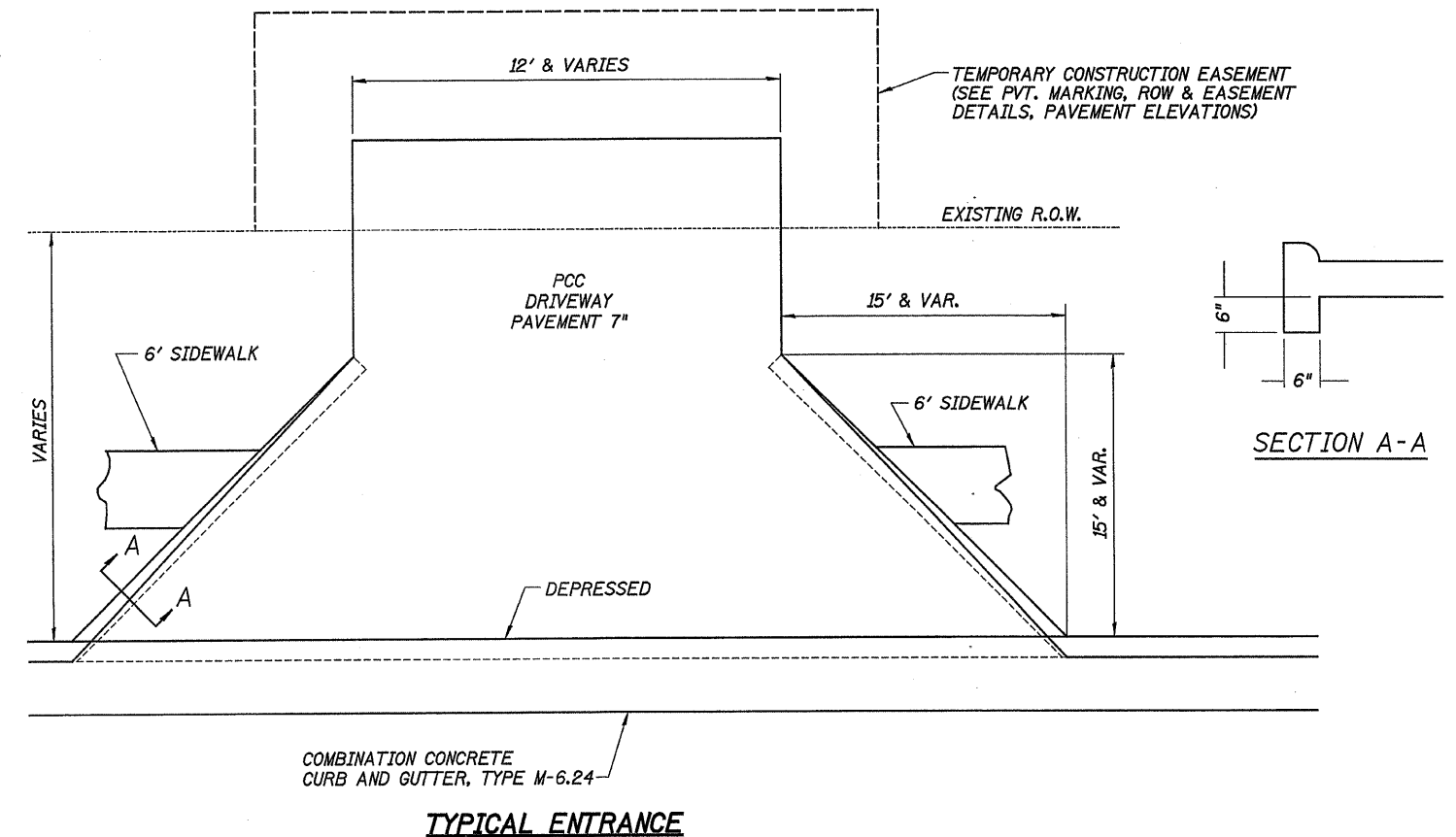
THE ROCK RIVER WATER RECLAMATION DISTRICT SHALL BE NOTIFIED 48 HOURS PRIOR TO CONSTRUCTION.

THE ROCK RIVER WATER RECLAMATION DISTRICT WILL FURNISH A FULL-TIME INSPECTOR FOR SANITARY WORK.

ALL PVC (SDR 35) SANITARY SEWER MAINS SHALL MEET THE REQUIREMENTS OF ASTM D-3034 WITH JOINTS CONFORMING TO ASTM D-3212. ALL PVC SANITARY SERVICES SHALL BE WATERMAIN-QUALITY (SDR 26) PIPE CONFORMING TO ASTM D-2241 WITH ASTM D-3139 JOINTS. BEDDING SHALL BE CLASS IA, ASTM D-2321-74 FOR MAINS AND SERVICES.

ALL NEW SANITARY MANHOLES MUST BE VACUUM TESTED PER ASTM C-1244.

TRENCHES MUST BE CONSTRUCTED TO GRADE OR SUBGRADE, AS APPLICABLE, BEFORE AIR AND DEFLECTION TESTS ARE PERFORMED. ALL WATERMAINS, SERVICES AND STORM SEWER MUST BE INSTALLED PRIOR TO AIR AND DEFLECTION TESTING.



TYPICAL ENTRANCE

MANHOLE ADJUSTMENTS

THE CONTRACTOR SHALL ADVISE THE ROCK RIVER WATER RECLAMATION DISTRICT PRIOR TO MANHOLE ADJUSTMENTS BEING MADE, SO THE DISTRICT MAY WITNESS THEM. THE METHOD AND AMOUNT OF ADJUSTMENT SHOULD BE NOTED ON THE PLANS. DISTRICT SPECIFICATIONS REQUIRE:

- A MINIMUM OF 4 INCHES OF ADJUSTING RINGS (4" ADJUSTMENT RING NOT REQUIRED IN TURF AREA OR CURB AND GUTTER ROADWAYS)
- A MAXIMUM OF 12" ADJUSTING RINGS
- NO MORE THAN ONE (1) 2" ADJUSTING RING PER MANHOLE
- A MAXIMUM OF 30" FROM THE TOP OF CASTING TO THE FIRST STEP
- INSTALLATION OF AN EXTERNAL MANHOLE SEAL BETWEEN THE CONE SECTION AND CASTING

MANHOLE ADJUSTMENT BY GROUTING IS NOT PERMITTED.

IF THE CASTING IS NOT A STANDARD NEENAH R-1670 OR EAST JORDAN E 1117 THE DISTRICT SUPPORTING SERVICES DEPARTMENT SHOULD BE CONTACTED PRIOR TO ADJUSTING THE MANHOLE FOR A REPLACEMENT CASTING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING THE DISTRICT AND OF THE NEED FOR THE NEW CASTING.

SANITARY SEWERS

- WATER MAINS SHALL BE LOCATED AT LEAST TEN FEET HORIZONTALLY FROM ANY EXISTING OR PROPOSED DRAIN, STORM SEWER, SANITARY SEWER, COMBINED SEWER OR SEWER SERVICE CONNECTION.
- WATER MAINS MAY BE LOCATED CLOSER THAN TEN FEET TO A SEWER LINE WHEN:
 - LOCAL CONDITIONS PREVENT A LATERAL SEPARATION OF TEN FEET; AND
 - THE WATER MAIN INVERT IS AT LEAST 18 INCHES ABOVE THE CROWN OF THE SEWER; AND
 - THE WATER MAIN IS EITHER IN A SEPARATE TRENCH OR IN THE SAME TRENCH ON AN UNDISTURBED EARTH SHELF LOCATED TO ONE SIDE OF THE SEWER.
- WHEN IT IS POSSIBLE TO MEET (1) OR (2) ABOVE, BOTH THE WATER MAIN AND DRAIN OR SEWER SHALL BE CONSTRUCTED OF SLIP-ON OR MECHANICAL JOINT CAST OR DUCTILE IRON PIPE, ASBESTOS-CEMENT PRESSURE PIPE, PRESTRESSED CONCRETE PIPE, OR PVC PIPE EQUIVALENT TO WATER MAIN STANDARDS OR CONSTRUCTION. THE DRAIN OR SEWER SHALL BE PRESSURE TESTED TO THE MAXIMUM EXPECTED SURCHARGE HEAD BEFORE BACKFILLING.

NEW STREET NAME SIGNS

CITY TO PROVIDE NEW SIGNS TO BE ERECTED ONTO COMBINATION MAST ARM ASSEMBLY AND POLE. THE COST TO ERECT SHALL BE INCLUDED IN THIS CONTRACT, INCLUDING ANY FASTENERS AND/OR HARDWARE TO COMPLETE THE INSTALLATION.

SCHEDULES

| | | | | |
|---------------------|---------------------------------------------|---------------------------|--------------------|----------------|
| F.A. RTE. FAP734 | SECTION 98-00480-00-BR 03-00522-00-TL | COUNTY WINNEBAGO | TOTAL SHEETS 62 | SHEET NO. 9 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

| PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH | | |
|-------------------------------------------------------|--------------|---------|
| STATION | SQ YD | REMARKS |
| CER 416+40 | 99 | |
| CEL 416+95 | 104 | |
| CEL 419+89 | 852 | |
| PEL 426+85 | 41 | |
| PER 427+20 | 20 | |
| PROJECT TOTAL | 1,116 | |

| CONCRETE COLLAR | | |
|----------------------|----------|--------------------------|
| STATION | EACH | REMARKS |
| R 415+70 | 1 | 12" DIAMETER STORM SEWER |
| R 416+03 | 1 | 12" DIAMETER STORM SEWER |
| L 416+47.35 | 1 | 12" DIAMETER STORM SEWER |
| R 418+59 | 1 | 18" DIAMETER STORM SEWER |
| PROJECT TOTAL | 4 | |

| FIRE HYDRANTS TO BE REMOVED | | |
|-----------------------------|----------|---------|
| STATION | EACH | REMARKS |
| 33' L 417+16 | 1 | |
| 27' L 421+66 | 1 | |
| 27' L 424+31 | 1 | |
| PROJECT TOTAL | 3 | |

| REMOVING MANHOLES | | |
|----------------------|----------|-------------|
| STATION | EACH | REMARKS |
| 18.97' R 419+07.82 | 1 | STORM SEWER |
| PROJECT TOTAL | 1 | |

| PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH | | |
|------------------------------------------|---------------|---------------------------------------|
| STATION | SQ FT | REMARKS |
| L 415+87 TO 416+71 | 637 | 6' SIDEWALK |
| L 417+18 TO 417+74 | 317 | 6' SIDEWALK |
| L 417+83 TO 419+54 | 1,036 | 6' SIDEWALK |
| L 418+11.42 | 33 | 5' SIDEWALK WEST OF STEPS |
| L 420+24 TO 423+77 | 2,123 | 6' SIDEWALK |
| L 424+64 TO 426+76 | 1,574 | 6' SIDEWALK |
| L 425+56 | 18 | 4' SIDEWALK WEST OF STEPS |
| L 426+10 | 18 | 4' SIDEWALK WEST OF STEPS |
| L 426+50 | 22 | 4' SIDEWALK WEST OF STEPS |
| L 426+96 TO 428+22 | 791 | 6' & VAR. SIDEWALK (MATCH @ POPE ST.) |
| R 415+74 TO 416+19 | 525 | 6' SIDEWALK |
| R 416+58 TO 416+80 | 122 | 6' SIDEWALK |
| R 417+10 TO 417+98 | 530 | 6' SIDEWALK |
| R 418+07 TO 426+06 | 4,964 | 6' SIDEWALK |
| R 424+68.5 | 64 | 8' SIDEWALK |
| R 426+56 TO 427+13 | 432 | 6' SIDEWALK |
| R 427+27 TO 428+36 | 834 | 6' SIDEWALK (MATCH @ KISH. CT.) |
| PROJECT TOTAL | 14,040 | |

| STORM SEWERS, CLASS A, TYPE 1 12" | | |
|-----------------------------------|------------|------------|
| STATION | FOOT | REMARKS |
| R 415+70 | 23 | |
| R 416+03 | 6 | |
| L 416+47.35 | 6 | |
| L 417+41.98 TO 417+65.46 | 23 | |
| R 417+53 TO 417+67 | 20 | |
| R 417+53 TO 417+59 | 20 | |
| R 419+42.29 | 4 | WEST OF MH |
| R 419+42.29 | 4 | EAST OF MH |
| R 419+90 | 35 | |
| R 421+61 TO 421+76 | 11 | |
| L 423+51 TO 423+88.43 | 49 | |
| L 424+15.54 TO L 9+00.78 | 50 | |
| L 9+00.78 TO 9+00.75 | 26 | |
| L 424+40.09 TO R 424+80.30 | 39 | |
| L 426+20.65 TO 426+28.69 | 9 | |
| PROJECT TOTAL | 325 | |

| MANHOLES, TYPE A, 4' - DIAMETER, TYPE 1 FRAME, CLOSED LID | | |
|--------------------------------------------------------------|----------|---------|
| STATION | EACH | REMARKS |
| 18.97' R 419+07.82 | 1 | |
| 6.78' R 419+42.29 | 1 | |
| 5.32' R 419+90 | 1 | |
| 29.67' L 422+73.88 | 1 | |
| 26.17' 423+88.43 | 1 | |
| PROJECT TOTAL | 5 | |

| VALVE VAULTS TO BE ADJUSTED | | |
|-----------------------------|----------|---------|
| STATION | EACH | REMARKS |
| 37' L 415+74 | 1 | |
| 23' R 415+74 | 1 | |
| 37' L 423+95.50 | 1 | |
| PROJECT TOTAL | 3 | |

| DETECTABLE WARNINGS | | |
|----------------------|------------|---------|
| STATION | SQ FT | REMARKS |
| L 415+88 | 8 | 4'X2' |
| L 423+75 | 8 | 4'X2' |
| L 424+74.50 | 16 | 8'X2' |
| L 428+18 | 8 | 4'X2' |
| R 415+88 | 8 | 4'X2' |
| R 416+75.50 | 8 | 4'X2' |
| R 417+12.50 | 8 | 4'X2' |
| R 424+68.50 | 12 | 6'X2' |
| R 425+92 | 8 | 4'X2' |
| R 426+63 | 8 | 4'X2' |
| R 428+30 | 8 | 4'X2' |
| PROJECT TOTAL | 100 | |

| STORM SEWERS, CLASS A, TYPE 1 18" | | |
|-----------------------------------|------------|---------|
| STATION | FOOT | REMARKS |
| R 418+59 TO 419+07.82 | 48 | |
| R 419+07.82 TO 419+42.29 | 34 | |
| R 419+42.29 TO 419+90 | 46 | |
| L 419+90 TO 421+62 | 170 | |
| PROJECT TOTAL | 298 | |

| MANHOLES, TYPE A, 5' - DIAMETER, TYPE 1 FRAME, CLOSED LID | | |
|--------------------------------------------------------------|----------|---------|
| STATION | EACH | REMARKS |
| 18.97' R 419+07.82 | 1 | |
| 6.78' R 419+42.29 | 1 | |
| PROJECT TOTAL | 2 | |

| VALVE VAULTS TO BE REMOVED | | |
|----------------------------|-----------|---------|
| STATION | EACH | REMARKS |
| 20.5' L 417+16 | 1 | |
| 22' L 419+58 | 1 | |
| 26' L 421+66 | 1 | |
| 20' L 424+31 | 1 | |
| 27' R 424+50 | 1 | |
| 28' L 425+70 | 1 | |
| 27' L 425+83 | 1 | |
| 27' L 426+67 | 1 | |
| 27.5' L 427+15 | 1 | |
| 26.5 R 427+54 | 1 | |
| PROJECT TOTAL | 10 | |

| STORM SEWERS, CLASS A, TYPE 2 18" | | |
|-----------------------------------|-----------|---------|
| STATION | FOOT | REMARKS |
| L 421+77 TO 422+73.88 | 94 | |
| PROJECT TOTAL | 94 | |

| INLETS, SPECIAL, NO. 1 | | |
|------------------------|----------|---------|
| STATION | EACH | REMARKS |
| L 416+47.35 | 1 | |
| L 417+41.98 | 1 | |
| R 419+90 | 1 | |
| R 421+61 | 1 | |
| L 421+62 | 1 | |
| R 421+76 | 1 | |
| L 421+77 | 1 | |
| R 424+81.86 | 1 | |
| L 426+28.69 | 1 | |
| PROJECT TOTAL | 9 | |

| REMOVING INLETS | | |
|----------------------|-----------|---------|
| STATION | EACH | REMARKS |
| R 415+72 | 1 | |
| R 416+00 | 1 | |
| L 417+42 | 1 | |
| L 417+65 | 1 | |
| R 418+66 | 1 | |
| R 421+05 | 1 | |
| L 421+13 | 1 | |
| R 421+96 | 1 | |
| L 423+88 | 1 | |
| L 424+19 | 1 | |
| L 424+40 | 1 | |
| PROJECT TOTAL | 11 | |

| HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" | | |
|-----------------------------------------|-------------|---------|
| STATION | SQ YD | REMARKS |
| 415+12.72 - 415+67.91 | 683 | |
| 428+38.84 - 428+84.98 | 341 | |
| PROJECT TOTAL | 1024 | |

| STORM SEWERS, CLASS A, TYPE 3 18" | | |
|-----------------------------------|-----------|---------|
| STATION | FOOT | REMARKS |
| L 422+73.88 TO 423+05.34 | 45 | |
| PROJECT TOTAL | 45 | |

| MANHOLES TO BE ADJUSTED | | |
|-------------------------------------|----------|---------------------------------------|
| STATION | EACH | REMARKS |
| WITHIN KISHWAUKEE STREET ROW | | |
| 21' R 415+83 | 1 | STORM SEWER |
| 17' R 416+45 | 1 | STORM SEWER |
| 4.5' L 417+46 | 1 | STORM SEWER |
| 18' R 417+53 | 1 | STORM SEWER |
| 8' R 426+34.5 (MH 022-058) | 1 | SANITARY SEWER |
| PARK AREA EAST OF KISHWAUKEE | | |
| ADJ-1 (MH 023-001) | 1 | SAN. SEWER (SEE SHT. 21 FOR LOCATION) |
| ADJ-2 (MH 022-113) | 1 | SAN. SEWER (SEE SHT. 21 FOR LOCATION) |
| PROJECT TOTAL | 7 | |

| FILLING MANHOLES | | |
|-------------------------------------|-----------|----------------------------------------|
| STATION | EACH | REMARKS |
| WITHIN KISHWAUKEE STREET ROW | | |
| 24' R 417+52 | 1 | STORM SEWER |
| 31' L 417+65 | 1 | STORM SEWER |
| 20' R 420+85 (MH 022-063) | 1 | SAN SEWER |
| 9' L 421+19.5 (MH 022-055) | 1 | SAN SEWER |
| 7.4' L 421+31.5 | 1 | SAN SEWER |
| 49' R 421+32 (MH 023-010) | 1 | SAN SEWER |
| 21' L 421+43 | 1 | STORM SEWER |
| 7' R 423+89 | 1 | SAN SEWER |
| 7' R 424+06 | 1 | SAN SEWER |
| 24' R 3+46 (MH 022-048) | 1 | SAN SEWER - LEWIS STREET |
| 24' R 4+69.5 (MH 022-049) | 1 | SAN SEWER - LEWIS STREET |
| 24' R 7+69 (MH 022-053) | 1 | SAN SEWER - LEWIS STREET |
| PARK AREA EAST OF KISHWAUKEE | | |
| FILL-1 (MH 023-011) | 1 | SAN SEWER - SEE SHEET #21 FOR LOCATION |
| FILL-2 | 1 | SAN SEWER - SEE SHEET #21 FOR LOCATION |
| FILL-3 | 1 | SAN SEWER - SEE SHEET #21 FOR LOCATION |
| FILL-4 | 1 | SAN SEWER - SEE SHEET #21 FOR LOCATION |
| FILL-5 | 1 | SAN SEWER - SEE SHEET #21 FOR LOCATION |
| FILL-6 (MH 022-069) | 1 | SAN SEWER - SEE SHEET #21 FOR LOCATION |
| PROJECT TOTAL | 18 | |

| CONCRETE ENCASEMENT | | |
|-------------------------------------|--------------|---------------------------------|
| STATION | CU YD | REMARKS |
| PARK AREA EAST OF KISHWAUKEE | | |
| 489' R OF KISHWAUKEE CL (UPSTREAM) | 119.5 | 150' (SEE TYP. PLAN, SHEET #21) |
| PROJECT TOTAL | 119.5 | |

| WATER MAIN 12" | | |
|------------------------------|--------------|---------|
| STATION | FOOT | REMARKS |
| 14' L 415+76 TO 15' L 421+96 | 620 | (NP) |
| 15' L 421+96 TO 15' L 423+96 | 200 | (P) |
| 15' L 423+96 TO 9' L 423+96 | 6 | (NP) |
| 9' L 423+96 TO 8' L 428+45 | 449 | (NP) |
| 8' L 428+45 TO 10' L 428+45 | 2 | (NP) |
| 10' L 428+45 TO 10' L 428+50 | 5 | (NP) |
| PARTICIPATING | 200 | (P) |
| NON-PARTICIPATING | 1,082 | (NP) |
| PROJECT TOTAL | 1,282 | |

| MANHOLES TO BE RECONSTRUCTED (SPECIAL) | | |
|----------------------------------------|----------|----------|
| STATION | EACH | REMARKS |
| 6' R 415+68 (MH 022-065) | 1 | SANITARY |
| PROJECT TOTAL | 1 | |

SCHEDULES

| | | | | |
|---------------------|----------------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR | WINNEBAGO | 62 | 10 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

| STATION | EACH | REMARKS |
|----------------------|----------|---------|
| 27' R 417+80 | 1 | |
| L 421+96 | 1 | |
| PROJECT TOTAL | 2 | |

60500305*

| STATION | FOOT | REMARKS |
|--------------------------|-----------|---------------------|
| R 417+24.00 TO 417+65.38 | 43 | LEFT SIDE OF MEDIAN |
| PROJECT TOTAL | 43 | |

6069100*

| STATION | FOOT | REMARKS |
|--------------------------|--------------|---------------------------------------|
| L 415+77.92 TO 416+37.74 | 87 | RADIUS |
| L 416+37.74 TO 417+76.46 | 139 | |
| L 417+76.46 TO 423+40.66 | 555 | |
| L 423+40.66 TO 423+89.59 | 76 | RADIUS |
| L 424+13.08 TO 424+34.15 | 129 | BOTH SIDES MEDIAN |
| L 424+34.15 TO 425+09.54 | 103 | RADIUS |
| L 425+09.54 TO 428+31.17 | 294 | |
| L 428+31.17 TO 428+28.23 | 37 | RADIUS |
| R 415+67.88 TO 416+12.90 | 69 | RADIUS |
| R 416+12.90 TO 416+55.84 | 43 | |
| R 416+55.84 TO 416+85.84 | 46 | RADIUS |
| R 417+03.86 TO 417+33.28 | 45 | RADIUS |
| R 417+33.28 TO 417+94.23 | 61 | |
| R 418+03.90 TO 425+79.26 | 775 | |
| R 425+79.26 TO 426+15.55 | 53 | RADIUS |
| R 426+15.55 TO 426+76.77 | 54 | RADIUS |
| R 426+76.77 TO 428+18.84 | 142 | |
| R 428+18.84 TO 428+38.84 | 29 | RADIUS |
| R 417+24 TO 417+82.45 | 69 | RIGHT SIDE MEDIAN AND ALONG TRACKS |
| 417+65.38 TO 417+78.68 | 13 | LEFT SIDE OF MEDIAN |
| R 417+97.42 TO 419+45 | 299 | BOTH SIDES OF MEDIAN AND ALONG TRACKS |
| PROJECT TOTAL | 3,119 | |

60610400*

| STATION | SQ FT | REMARKS |
|------------------------|--------------|---------|
| 417+24.00 TO 417+79.00 | 361 | |
| 418+00.68 TO 419+45.00 | 1,264 | |
| PROJECT TOTAL | 1,625 | |

60618300

| STATION | SQ FT | REMARKS |
|----------------------|------------|---------|
| R 416+00 TO 417+24 | 496 | |
| PROJECT TOTAL | 496 | |

60621900*

| STATION | SQ FT | REMARKS |
|----------------------|------------|---------|
| L 419+89 | 185 | |
| PROJECT TOTAL | 185 | |

60624600

| STATION | EACH | REMARKS |
|--------------------------|----------|-----------------------------------|
| R 419+45.00 TO 419+50.00 | 1 | MEDIAN NOSE - 5' R |
| 9+51.00 TO 9+59.00 | 1 | MEDIAN NOSE - 8' R (LEWIS STREET) |
| PROJECT TOTAL | 2 | |

60625900*

| STATION | FOOT | REMARKS |
|----------------------------|------------|---------|
| R 416+83.79 TO 417+53 | 80 | |
| R 421+76 TO L 421+77 | 68 | |
| L 423+88.43 TO 424+15.54 | 35 | |
| L 424+40.09 TO R 424+61.86 | 75 | |
| L 426+20.65 TO R 426+78.39 | 77 | |
| PROJECT TOTAL | 335 | |

61140200*

| STATION | FOOT | REMARKS |
|----------------------|-----------|---------|
| R 419+90 TO L 419+90 | 32 | |
| L 421+62 TO 421+77 | 11 | |
| PROJECT TOTAL | 43 | |

61140600*

| STATION | FOOT | REMARKS |
|----------------------|------------|--------------------------------------|
| L 416+00 TO 423+50 | 68 | DIVIDES SB LANES - WHITE, 4'-40' SP |
| L 424+80 TO 428+00 | 32 | DIVIDES SB LANES - WHITE, 4'-40' SP |
| R 416+00 TO 423+50 | 68 | DIVIDES NB LANES - WHITE, 4'-40' SP |
| R 424+80 TO 428+00 | 32 | DIVIDES NB LANES - WHITE, 4'-40' SP |
| 416+00 TO 417+15 | 12 | SB TURN LANE - WHITE, 4'-40' SP |
| R 421+00 TO 423+84 | 28 | NB TURN LANE - WHITE, 4'-40' SP |
| 419+00 TO 423+84 | 88 | (2) MEDIAN LINES - YELLOW, 4'-40' SP |
| 424+00 TO 426+09 | 40 | (2) MEDIAN LINES - YELLOW, 4'-40' SP |
| 426+53 TO 427+94 | 28 | (2) MEDIAN LINES - YELLOW, 4'-40' SP |
| R 416+95 TO 417+06 | 11 | STOP BAR - WHITE |
| R 417+65 | 24 | STOP BAR - WHITE |
| L 418+12 | 24 | STOP BAR - WHITE |
| R 423+83 | 38 | STOP BAR - WHITE |
| R 9+52 | 34 | STOP BAR - WHITE |
| L 424+79 | 35 | STOP BAR - WHITE |
| R 426+28 TO 426+52 | 24 | STOP BAR - WHITE |
| PROJECT TOTAL | 586 | |

70300100

| STATION | SQ FT | REMARKS |
|----------------------|------------|--------------------------------------|
| L 416+00 TO 423+50 | 23 | DIVIDES SB LANES - WHITE, 4'-40' SP |
| L 424+80 TO 428+00 | 11 | DIVIDES SB LANES - WHITE, 4'-40' SP |
| R 416+00 TO 423+50 | 23 | DIVIDES NB LANES - WHITE, 4'-40' SP |
| R 424+80 TO 428+00 | 11 | DIVIDES NB LANES - WHITE, 4'-40' SP |
| 416+00 TO 417+15 | 4 | SB TURN LANE - WHITE, 4'-40' SP |
| R 421+00 TO 423+84 | 9 | NB TURN LANE - WHITE, 4'-40' SP |
| 419+00 TO 423+84 | 29 | (2) MEDIAN LINES - YELLOW, 4'-40' SP |
| 424+00 TO 426+09 | 13 | (2) MEDIAN LINES - YELLOW, 4'-40' SP |
| 426+53 TO 427+94 | 9 | (2) MEDIAN LINES - YELLOW, 4'-40' SP |
| R 416+95 TO 417+06 | 4 | STOP BAR - WHITE |
| R 417+65 | 8 | STOP BAR - WHITE |
| L 418+12 | 8 | STOP BAR - WHITE |
| R 423+83 | 13 | STOP BAR - WHITE |
| R 9+52 | 11 | STOP BAR - WHITE |
| L 424+79 | 12 | STOP BAR - WHITE |
| R 426+28 TO 426+52 | 8 | STOP BAR - WHITE |
| PROJECT TOTAL | 196 | |

70301000

| STATION | SQ FT | REMARKS |
|----------------------|--------------|---------------------------------------|
| R 416+09 | 15.6 | LEFT TURN ARROW - KISHWAUKEE ST |
| R 416+26 | 14.4 | 4- 'R'S @ 3.6 SQ FT - KISHWAUKEE ST |
| R 416+26 | 108.0 | 2- RR 'X'S @ 54 SQ FT - KISHWAUKEE ST |
| R 416+94 | 15.6 | LEFT TURN ARROW - KISHWAUKEE ST |
| L 419+38 | 14.4 | 4- 'R'S @ 3.6 SQ FT - KISHWAUKEE ST |
| L 419+38 | 108.0 | 2- RR 'X'S @ 54 SQ FT - KISHWAUKEE ST |
| R 421+14 | 15.6 | LEFT TURN ARROW - KISHWAUKEE ST |
| R 421+94 | 15.6 | LEFT TURN ARROW - KISHWAUKEE ST |
| R 422+82 | 15.6 | LEFT TURN ARROW - KISHWAUKEE ST |
| R 423+68 | 15.6 | LEFT TURN ARROW - KISHWAUKEE ST |
| R 9+38 | 15.6 | LEFT TURN ARROW - LEWIS ST |
| R 9+38 | 15.6 | RIGHT TURN ARROW - LEWIS ST |
| PROJECT TOTAL | 369.6 | |

78001100

| STATION | FOOT | REMARKS |
|--------------------------|--------------|---------------------------------------|
| R 416+00 TO 417+78.67 | 180 | WEST EDGE OF MEDIAN - YELLOW |
| R 416+00 TO 417+82.45 | 182 | EAST EDGE OF MEDIAN - YELLOW |
| 417+97.23 TO 419+50 | 153 | WEST EDGE OF MEDIAN - YELLOW |
| R 418+01.23 TO 419+50 | 149 | EAST EDGE OF MEDIAN - YELLOW |
| 419+50 TO 420+92 | 284 | 2 @ 4" - WEST SIDE OF MEDIAN - YELLOW |
| R 419+50 TO 420+92 | 286 | 2 @ 4" - EAST SIDE OF MEDIAN - YELLOW |
| 420+92 TO 423+84 | 584 | 2 @ 4" - YELLOW |
| L 424+13.08 TO 424+34.15 | 134 | EDGE AROUND ISLAND - YELLOW |
| R 424+74 TO 426+09 | 545 | 2 @ 4" - MEDIAN COMPLETE - YELLOW |
| R 426+53 TO 427+38 | 340 | 2 @ 4" - MEDIAN COMPLETE - YELLOW |
| R 427+38 TO 427+94 | 112 | 2 @ 4" - YELLOW |
| PROJECT TOTAL | 2,948 | |

78001110

| STATION | FOOT | REMARKS |
|--------------------------|------------|------------------------------------------|
| L 416+21.15 TO 417+51.15 | 40 | 10 - 30 SKIP DASH - WHITE |
| L 418+21.32 TO 423+51.32 | 140 | 10 - 30 SKIP DASH - WHITE |
| R 418+21.32 TO 423+51.32 | 140 | 10 - 30 SKIP DASH - WHITE |
| L & R 9+01 TO 9+51 | 40 | 10 - 30 SKIP DASH - WHITE (LEWIS STREET) |
| L 424+74 TO 428+04 | 90 | 10 - 30 SKIP DASH - WHITE |
| R 424+74 TO 428+04 | 90 | 10 - 30 SKIP DASH - WHITE |
| 423+75.20 TO 424+74 | 335 | CROSSWALK STRIPING - COMPLETE - WHITE |
| PROJECT TOTAL | 875 | |

78001130

| STATION | FOOT | REMARKS |
|----------------------|------------|---------------|
| 416+00 TO 417+15 | 115 | SOLID - WHITE |
| R 421+00 TO 423+84 | 284 | SOLID - WHITE |
| PROJECT TOTAL | 399 | |

78001140

| STATION | FOOT | REMARKS |
|----------------------|------------|------------------------------|
| R 419+51 TO 420+94 | 63 | DIAGONALS IN MEDIAN - YELLOW |
| R 424+76 TO 426+3.5 | 82 | DIAGONALS IN MEDIAN - YELLOW |
| R 426+59 TO 427+79 | 22 | DIAGONALS IN MEDIAN - YELLOW |
| PROJECT TOTAL | 167 | |

78001150

| STATION | FOOT | REMARKS |
|----------------------|------------|--------------------------------|
| R 416+00 | 26 | STOP BAR @ RR CROSSING - WHITE |
| R 416+51 | 24 | STOP BAR @ RR CROSSING - WHITE |
| R 416+95 TO 417+06 | 11 | STOP BAR @ LORDEM CT - WHITE |
| R 417+65 | 24 | STOP BAR @ RR CROSSING - WHITE |
| L 418+12 | 24 | STOP BAR @ RR CROSSING - WHITE |
| L 419+12 | 24 | STOP BAR @ RR CROSSING - WHITE |
| L 419+62 | 24 | STOP BAR @ RR CROSSING - WHITE |
| R 423+83 | 38 | STOP BAR - WHITE |
| R 9+52 | 34 | STOP BAR @ LEWIS ST - WHITE |
| L 424+79 | 35 | STOP BAR - WHITE |
| R 426+28 TO 426+52 | 24 | STOP BAR @ 10TH ST - WHITE |
| PROJECT TOTAL | 288 | |

78001180

| STATION | EACH | REMARKS |
|----------------------------------|-----------|----------------------------------|
| WEST SIDE KISHWAUKEE @ C.L. | 14 | ONE-WAY CRYSTAL MARKER @ 80' CTS |
| EAST SIDE KISHWAUKEE @ C.L. | 14 | ONE-WAY CRYSTAL MARKER @ 80' CTS |
| WEST SIDE KISHWAUKEE @ TURN LANE | 3 | ONE-WAY CRYSTAL MARKER @ 40' CTS |
| EAST SIDE KISHWAUKEE @ TURN LANE | 8 | ONE-WAY CRYSTAL MARKER @ 40' CTS |
| WEST SIDE KISHWAUKEE @ MEDIAN | 16 | ONE-WAY AMBER MARKER @ 40' CTS |
| EAST SIDE KISHWAUKEE @ MEDIAN | 16 | ONE-WAY AMBER MARKER @ 40' CTS |
| WEST SIDE KISHWAUKEE @ MEDIAN | 13 | TWO-WAY AMBER MARKER @ 40' CTS |
| EAST SIDE KISHWAUKEE @ MEDIAN | 13 | TWO-WAY AMBER MARKER @ 40' CTS |
| PROJECT TOTAL | 97 | |

78100100

| STATION | EACH | REMARKS |
|-------------------------------------|----------|---------------------------------------|
| LEWIS STREET | | |
| R 4+69.98 | 1 | 9" DIA. |
| PARK AREA EAST OF KISHWAUKEE | | |
| PLUG-1 | 1 | 24" DIA. (SEE SHEET #21 FOR LOCATION) |
| PLUG-2 | 1 | 24" DIA. (SEE SHEET #21 FOR LOCATION) |
| PLUG-3 | 1 | 24" DIA. (SEE SHEET #21 FOR LOCATION) |
| PLUG-4 | 1 | 20" DIA. (SEE SHEET #21 FOR LOCATION) |
| PROJECT TOTAL | 5 | |

X0320374*

| STATION | EACH | REMARKS |
|---------------------------|----------|---------|
| 7' R 416+54 (MH 022-108) | 1 | |
| 21' R 418+91 (MH 022-110) | 1 | |
| PROJECT TOTAL | 2 | |

X0320591*

| STATION | SQ FT | REMARKS |
|-------------------------------------|------------|----------------------------|
| PARK AREA EAST OF KISHWAUKEE | | |
| OVER PROPOSED CREEK | 889 | SEE SHEET #21 FOR LOCATION |
| PROJECT TOTAL | 889 | |

X0322508*

SCHEDULES

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------------------------|------------------|--------------|-----------|
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 11 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

| STATION | SQ FT | REMARKS |
|-------------------------|--------------|---------------------|
| L 425+40 TO 425+53.5 | 102 | |
| LT 425+56 | 26 | BOTH SIDES OF STEPS |
| L 425+58.5 TO 426+07.5 | 420 | |
| L 426+10 | 26 | BOTH SIDES OF STEPS |
| L 426+12.5 TO 426+47.5 | 291 | |
| L 426+50 | 17 | BOTH SIDES OF STEPS |
| L 426+52.5 TO 426+75.77 | 142 | |
| PROJECT TOTAL | 1,024 | |

| STATION | EACH | REMARKS |
|------------------------------------|----------|----------------------------|
| PARK AREA EAST OF KISHWAUKEE | | |
| 168' R OF KISHWAUKEE CL (UPSTREAM) | 1 | SEE SHEET #21 FOR LOCATION |
| 337' R OF KISHWAUKEE CL (UPSTREAM) | 1 | SEE SHEET #21 FOR LOCATION |
| 489' R OF KISHWAUKEE CL (UPSTREAM) | 1 | SEE SHEET #21 FOR LOCATION |
| PROJECT TOTAL | 3 | |

| STATION | EACH | REMARKS |
|----------------------|----------|---------|
| 9' L 426+33 | 1 | |
| PROJECT TOTAL | 1 | |

| STATION | SQ FT | REMARKS |
|------------------------|--------------|---------|
| 417+24.00 TO 417+79.00 | 361 | |
| 418+00.68 TO 419+45.00 | 1,264 | |
| PROJECT TOTAL | 1,625 | |

| STATION | EACH | REMARKS |
|----------------------|----------|--------------------------|
| 13' L 417+29 | 1 | |
| 16' L 421+65 | 1 | |
| 9' L 424+92 | 1 | REQUIRES PIPE PROTECTION |
| PROJECT TOTAL | 3 | |

| STATION | EACH | REMARKS |
|----------------------|----------|--------------|
| 10' L 417+27 | 1 | TO BE FILLED |
| 12.5' L 421+03 | 1 | TO BE FILLED |
| 7.3' L 421+28 | 1 | TO BE FILLED |
| 59.5' R 421+34 | 1 | TO BE FILLED |
| 12' L 426+27 | 1 | TO BE FILLED |
| 14' L 426+65 | 1 | TO BE FILLED |
| 11' L 428+37 | 1 | TO BE FILLED |
| PROJECT TOTAL | 7 | |

| STATION | CU YD | REMARKS |
|--------------------------------|--------------|---------------------------------------|
| SANITARY SEWER | | |
| 425+04 | 41 (NP) | 4" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 425+76 | 42 (NP) | 4" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 426+00 | 42 (NP) | 4" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 426+43 | 42 (NP) | 4" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 427+24 | 42 (NP) | 4" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 427+36 | 26 (NP) | 4" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 427+88 | 24 (NP) | 4" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 416+50 | 65 (NP) | 6" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 416+61 | 51 (NP) | 6" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 417+32 | 53 (NP) | 6" DIA. SAN. SERVICE - KISHWAUKEE ST |
| L 417+56 | 80 (NP) | 6" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 415+67.32 TO 417+62 | 226 (NP) | 12" DIA. SANITARY - KISHWAUKEE STREET |
| 418+22 TO 419+18.08 | 113 (NP) | 12" DIA. SANITARY - KISHWAUKEE STREET |
| 396' R 420+35 TO 383' R 420+50 | 21 (P) | 21" DIA. SAN. SEWER - PARK AREA |
| 3+17.28 TO 4+69.98 | 465 (P) | 24" DIA. SAN. SEWER - LEWIS ST |
| 4+69.98 TO 7+34.18 | 986 (P) | 24" DIA. SAN. SEWER - LEWIS ST |
| 7+34.18 TO 10+07.30 | 1054 (P) | 24" DIA. SAN. SEWER - LEWIS ST |
| 10+07.30 TO 12+54.16 | 688 (P) | 24" DIA. SAN. SEWER - LEWIS ST |
| WATER MAIN | | |
| 14' L 415+76 TO 13' L 417+34 | 83 (NP) | 12" WATER MAIN |
| 14' L 418+34 TO 16' L 421+00 | 160 (NP) | 12" WATER MAIN |
| 13' L 417+29 | 13 (NP) | 12" WATER MAIN |
| 16' L 421+00 TO 15' L 421+96 | 50 (NP) | 12" WATER MAIN |
| 15' L 421+96 TO 15' L 423+96 | 105 (P) | 12" WATER MAIN |
| 15' L 423+96 TO 9' L 423+96 | 3 (NP) | 12" WATER MAIN |
| 9' L 423+96 TO 8' L 427+00 | 160 (NP) | 12" WATER MAIN |
| 16' L 421+65 | 11 (NP) | 12" WATER MAIN |
| 9' L 424+92 | 16 (NP) | 12" WATER MAIN |
| 8' L 427+00 TO 8' L 428+45 | 76 (NP) | 12" WATER MAIN |
| 8' L 428+45 TO 10' L 428+45 | 1 (NP) | 12" WATER MAIN |
| 10' L 428+45 TO 10' L 428+50 | 3 (NP) | 12" WATER MAIN |
| COPPER WATER SERVICES (ALL) | 165 (NP) | |
| PARTICIPATING | 3,319 | (P) |
| NON-PARTICIPATING | 1,588 | (NP) |
| PROJECT TOTAL | 4,907 | |

| STATION | SQ YD | REMARKS |
|------------------------------|--------------|---------------------------------------|
| PARK AREA EAST OF KISHWAUKEE | | |
| L 20+30 TO 27+12.50 | 2,420 | NEW CHANNEL - SEEDING AND LANDSCAPING |
| R 20+30 TO 27+12.50 | 2,323 | NEW CHANNEL - SEEDING AND LANDSCAPING |
| PROJECT TOTAL | 4,743 | |

| STATION | EACH | REMARKS |
|----------------------|-----------|---------|
| 78' R 415+70 | 1 | |
| R 416+03.30 | 1 | |
| 62.54' R 416+83.79 | 1 | |
| L 417+65.46 | 1 | |
| 1.39' R 417+67 | 1 | |
| R 417+59 | 1 | |
| 1.39' R 419+42.29 | 1 | |
| 12.16' R 419+42.29 | 1 | |
| 49.54' L 424+15.54 | 1 | |
| 8.5' L 9+00.78 | 1 | |
| 35.8' L 9+00.75 | 1 | |
| 35.48' L 424+80.30 | 1 | |
| R 426+78.39 | 1 | |
| PROJECT TOTAL | 13 | |

| STATION | SQ YD | REMARKS |
|------------------------|------------|------------------------|
| STA. 423+30 | 424 | EAST ABUT. BOX CULVERT |
| EAST ABUT. PED. BRIDGE | 76 | |
| PROJECT TOTAL | 500 | |

| STATION | FOOT | REMARKS |
|------------------------------------|------------|----------------------------|
| PARK AREA EAST OF KISHWAUKEE | | |
| 194' R OF KISHWAUKEE CL (UPSTREAM) | 100 | SEE SHEET #21 FOR LOCATION |
| PROJECT TOTAL | 100 | |

| STATION | EACH | REMARKS |
|----------------------|----------|-----------------------|
| 3+17.28 (MH #3) | 1 | SAN. SEWER - LEWIS ST |
| 7+34.18 (MH #5) | 1 | SAN. SEWER - LEWIS ST |
| PROJECT TOTAL | 2 | |

| STATION | FOOT | REMARKS |
|----------------------|------------|-------------------------------|
| 416+50 | 58 | EXTENDS TO ROW (R) |
| 416+61 | 52 | EXTENDS TO ROW (L) |
| 417+15 | 48 | EXTENDS TO ROW (L) |
| L 417+56 | 68 | TIES INTO EXIST. SAN. SERVICE |
| PROJECT TOTAL | 226 | |

| STATION | EACH | REMARKS |
|----------------------|----------|---------|
| 14' L 415+76 | 1 | |
| 10' L 428+50 | 1 | |
| PROJECT TOTAL | 2 | |

| STATION | EACH | REMARKS |
|----------------------|----------|---------|
| 15' L 423+96 | 1 | |
| 428+50 | 1 | |
| PROJECT TOTAL | 2 | |

| STATION | EACH | REMARKS |
|----------------------|----------|---------|
| 14' L 415+76 | 1 | |
| 10' L 428+50 | 1 | |
| PROJECT TOTAL | 2 | |

| STATION | FOOT | REMARKS |
|----------------------|------------|---------------------------------|
| 425+04 | 56 | EXTENDS TO ROW (L) |
| 425+76 | 48 | EXTENDS PAST RETAINING WALL (L) |
| 426+00 | 48 | EXTENDS PAST RETAINING WALL (L) |
| 426+43 | 48 | EXTENDS PAST RETAINING WALL (L) |
| 427+24 | 48 | EXTENDS TO ROW (L) |
| 427+36 | 30 | EXTENDS TO ROW (R) |
| 427+88 | 27 | EXTENDS TO ROW (R) |
| PROJECT TOTAL | 305 | |

| STATION | EACH | REMARKS |
|----------------------|----------|---------|
| 426+33 | 1 | |
| PROJECT TOTAL | 1 | |

| STATION | FOOT | REMARKS |
|------------------------------|------------|------------------------|
| WATER MAIN | | |
| 13' L 417+34 TO 14' L 418+34 | 100 | FOR 12" WATER MAIN |
| SANITARY SEWER | | |
| 7' R 417+62 TO 418+22 | 60 | FOR 12" SANITARY SEWER |
| PROJECT TOTAL | 160 | |

| STATION | EACH | REMARKS |
|----------------------|----------|---------|
| 428+50 | 1 | |
| PROJECT TOTAL | 1 | |

| STATION | EACH | REMARKS |
|----------------------|----------|---------|
| 14' L 415+76 | 1 | |
| 9' L 423+96 | 1 | |
| 7' L 428+32 | 1 | |
| PROJECT TOTAL | 3 | |

| STATION | FOOT | REMARKS |
|----------------------|------------|---------|
| R 416+15 | 22 | |
| R 416+33 | 22 | |
| R 417+30 | 22 | |
| L 425+64 | 22 | |
| L 425+88 | 22 | |
| L 426+70 | 22 | |
| L 427+12 | 22 | |
| PROJECT TOTAL | 154 | |

| STATION | EACH | REMARKS |
|----------------------|----------|---------|
| 15' L 416+15 | 1 | |
| 14' L 416+33 | 1 | |
| 13' L 417+30 | 1 | |
| 14' L 418+65 | 1 | |
| 9' L 425+64 | 1 | |
| 9' L 425+88 | 1 | |
| 9' L 426+70 | 1 | |
| 8' L 427+12 | 1 | |
| 8' L 427+56 | 1 | |
| PROJECT TOTAL | 9 | |

| | | | | |
|---------------------|----------------------------------|------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 12 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

SCHEDULES

| SANITARY (9" DIAMETER DROP) MANHOLE, 5' DIAMETER | | |
|-----------------------------------------------------|----------|-----------------------|
| STATION | EACH | REMARKS |
| 4+69.98 (MH #4) | 1 | SAN. SEWER - LEWIS ST |
| PROJECT TOTAL | 1 | |

| BREAKER-RUN CRUSHED STONE | | |
|---------------------------|--------------|-----------------------------------|
| STATION | TONS | REMARKS |
| CONTINGENCY ITEM | 1,538 | ROAD - AS REQUIRED BY ENGINEER |
| CONTINGENCY ITEM | 410 | CULVERT - AS REQUIRED BY ENGINEER |
| PROJECT TOTAL | 1,948 | |

| SANITARY (12" DIAMETER DROP) MANHOLE, 6' DIAMETER, SPECIAL | | |
|---------------------------------------------------------------|----------|---------------------------------------|
| STATION | EACH | REMARKS |
| PARK AREA EAST OF KISHWAUKEE 383' R 420+50 (MH #6) | 1 | BUILD OVER EXIST. PIPE - SEE SHT. #21 |
| PROJECT TOTAL | 1 | |

| SANITARY (10" DIAMETER DROP) MANHOLE, 5' DIAMETER | | |
|------------------------------------------------------|----------|----------------------------|
| STATION | EACH | REMARKS |
| 7.30' R 424+25.14 (MH #2) | 1 | SAN. SEWER - KISHWAUKEE ST |
| PROJECT TOTAL | 1 | |

| CONCRETE STEPS | | |
|----------------------|-------------|----------------|
| STATION | CU YD | REMARKS |
| L 418+11.42 | 0.53 | 2 STEPS - 5' W |
| L 425+56 | 1.15 | 6 STEPS - 4' W |
| L 426+10 | 1.15 | 6 STEPS - 4' W |
| L 426+50 | 1.15 | 6 STEPS - 4' W |
| PROJECT TOTAL | 3.98 | |

| INLETS, TYPE 700 (DRIVEWAY) | | |
|-----------------------------|----------|---------|
| STATION | EACH | REMARKS |
| L 419+90 | 1 | |
| PROJECT TOTAL | 1 | |

| SANITARY (12" DIAMETER DROP) MANHOLE, 6' DIAMETER | | |
|------------------------------------------------------|----------|----------------------------|
| STATION | EACH | REMARKS |
| 7' R 419+18.08 (MH #1) | 1 | SAN. SEWER - KISHWAUKEE ST |
| PROJECT TOTAL | 1 | |

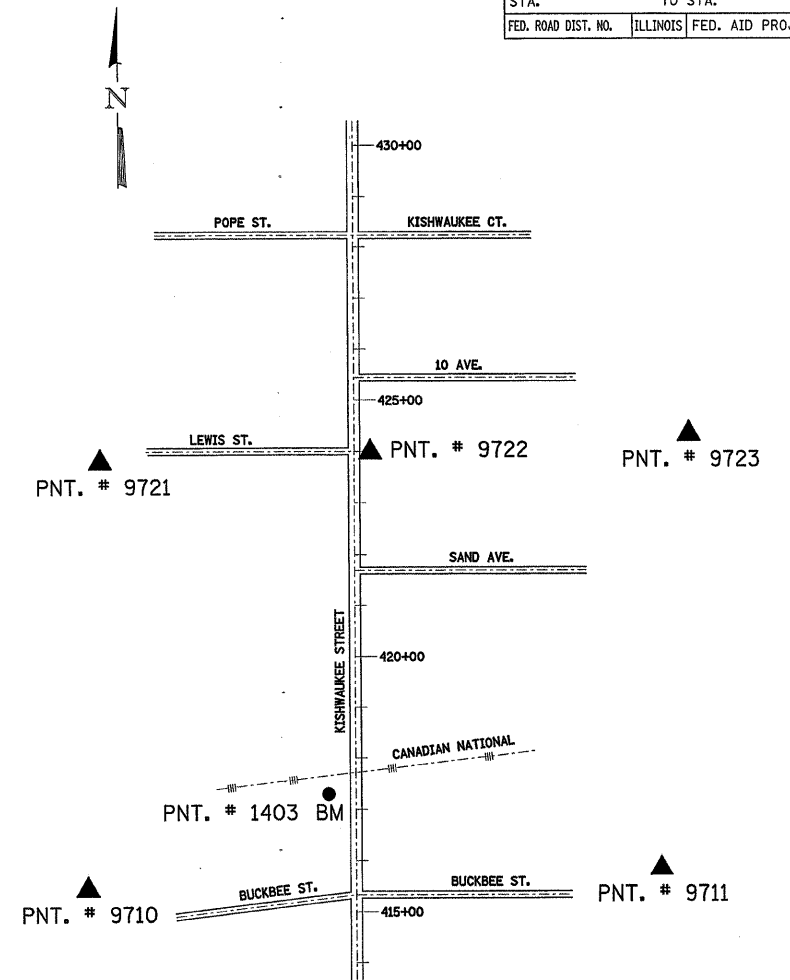
| SANITARY SEWER 12" | | |
|------------------------|------------|---------|
| STATION | FOOT | REMARKS |
| 415+67.32 TO 419+18.08 | 350 | |
| PROJECT TOTAL | 350 | |

| GRANULAR CRADLE AND INITIAL BACKFILL | | |
|--------------------------------------|--------------|---------------------------------------|
| STATION | CU YD | REMARKS |
| SANITARY SEWER | | |
| 425+04 | 12 (NP) | 4" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 425+76 | 11 (NP) | 4" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 426+00 | 11 (NP) | 4" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 426+43 | 11 (NP) | 4" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 427+24 | 11 (NP) | 4" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 427+36 | 7 (NP) | 4" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 427+88 | 6 (NP) | 4" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 416+50 | 14 (NP) | 6" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 416+61 | 12 (NP) | 6" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 417+32 | 12 (NP) | 6" DIA. SAN. SERVICE - KISHWAUKEE ST |
| L 417+56 | 20 (NP) | 6" DIA. SAN. SERVICE - KISHWAUKEE ST |
| 415+67.32 TO 417+62 | 64 (NP) | 12" DIA. SANITARY - KISHWAUKEE STREET |
| 418+22 TO 419+18.08 | 35 (NP) | 12" DIA. SANITARY - KISHWAUKEE STREET |
| 396' R 420+35 TO 383' R 420+50 | 8 (P) | 21" DIA. SAN. SEWER - PARK AREA |
| 3+17.28 TO 4+69.98 | 87 (P) | 24" DIA. SAN. SEWER - LEWIS ST |
| 4+69.98 TO 7+34.18 | 151 (P) | 24" DIA. SAN. SEWER - LEWIS ST |
| 7+34.18 TO 10+07.30 | 156 (P) | 24" DIA. SAN. SEWER - LEWIS ST |
| 10+07.30 TO 12+54.16 | 141 (P) | 24" DIA. SAN. SEWER - LEWIS ST |
| WATER MAIN | | |
| 14' L 415+76 TO 13' L 417+34 | 54 (NP) | 12" WATER MAIN |
| 14' L 418+34 TO 16' L 421+00 | 90 (NP) | 12" WATER MAIN |
| 13' L 417+29 | 7 (NP) | 12" WATER MAIN |
| 16' L 421+00 TO 15' L 421+96 | 33 (NP) | 12" WATER MAIN |
| 15' L 421+96 TO 15' L 423+96 | 68 (P) | 12" WATER MAIN |
| 15' L 423+96 TO 9' L 423+96 | 2 (NP) | 12" WATER MAIN |
| 9' L 423+96 TO 8' L 427+00 | 103 (NP) | 12" WATER MAIN |
| 16' L 421+65 | 6 (NP) | 12" WATER MAIN |
| 9' L 424+92 | 8 (NP) | 12" WATER MAIN |
| 8' L 427+00 TO 8' L 428+45 | 49 (NP) | 12" WATER MAIN |
| 8' L 428+45 TO 10' L 428+45 | 1 (NP) | 12" WATER MAIN |
| 10' L 428+45 TO 10' L 428+50 | 2 (NP) | 12" WATER MAIN |
| COPPER WATER SERVICES (ALL) | 88 (NP) | |
| PARTICIPATING | 611 | (P) |
| NON-PARTICIPATING | 669 | (NP) |
| PROJECT TOTAL | 1,280 | |

| UTILITIES | | |
|-------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------|
| TYPE | NAME AND ADDRESS OF UTILITY | TELEPHONE |
| STORM SEWER | CITY OF ROCKFORD 425 EAST STATE STREET ROCKFORD, IL. 61104 CONTACT: LARRY LAURSEN | (815) 987-5570 |
| ELECTRIC | COMED 123 ENERGY AVENUE ROCKFORD, IL. 61109 CONTACT: DAVE SCHACHT | (630) 437-2129 |
| GAS | NICOR GAS 300 WEST TERRA COTTA AVENUE CRYSTAL LAKE, IL. 60014 CONTACT: SCOTT PUFFER | (815) 455-0271 EXT. 250 |
| CABLE | INSIGHT COMMUNICATIONS 811 21ST. STREET ROCKFORD, IL. 61108 CONTACT: LORI BYRNE | (815) 962-4400 EXT. 2012 |
| TELEPHONE | SBC 2404 8TH AVENUE ROCKFORD, IL. 61108 CONTACT: STEVE JONES CONTACT: FRANK ROSE | (815) 394-7270 (815) 394-7276 |
| WATER | CITY OF ROCKFORD WATER DIVISION 1111 CEDAR STREET ROCKFORD, IL. 61102 | (815) 987-5725 |
| RAILROAD | CN RAILROAD 17641 SOUTH ASHLAND AVENUE HOMEROD, IL. 60430-1345 CONTACT: JOHN HENDKRICKSON | (708) 332-3557 |
| TELEPHONE | A. T & T (FIBER OPTICS IN ROW) REQUIRES LOCATE TECHNICIAN ON SITE (NO CHARGE) 866 ROCK CREEK ROAD PLANO, IL. 60545 CONTACT: BRIAN CRULL CONTACT: CARL DONAHUE | (815) 566-1976 CELL (630) 552-4677 |
| TELEPHONE | VERIZON 112 WEST ELM STREET SYCAMORE, IL. 60178 CONTACT: GARY KLUZ | (815) 895-1515 |
| SAN. SEWER | ROCK RIVER WATER RECLAMATION DISTRICT 3333 KISHWAUKEE STREET ROCKFORD, IL. 61126-7480 CONTACT: DAVID COOK | (815) 387-7663 |

| SANITARY SEWER 21" | | |
|----------------------------------------------------------------|-----------|---------|
| STATION | FOOT | REMARKS |
| PARK AREA EAST OF KISHWAUKEE 396' R 420+35 TO 383' R 420+50 | 16 | |
| PROJECT TOTAL | 16 | |

| SANITARY SEWER 24" | | |
|----------------------|------------|-----------------------|
| STATION | FOOT | REMARKS |
| 3+17.28 TO 4+69.98 | 153 | SAN. SEWER - LEWIS ST |
| 4+69.98 TO 7+34.18 | 264 | SAN. SEWER - LEWIS ST |
| 7+34.18 TO 10+07.30 | 273 | SAN. SEWER - LEWIS ST |
| 10+07.30 TO 12+54.16 | 247 | SAN. SEWER - LEWIS ST |
| PROJECT TOTAL | 937 | |



CONTROL POINT AND BENCH MARK LOCATIONS

| CONTROL POINTS: | | | |
|-----------------|-------------|-------------|----------------------------|
| POINT: | NORTHING: | EASTING: | STATION |
| #9710 | #2036606.33 | #2588274.82 | LT. 663.58' STA. 415+46.05 |
| #9711 | #2036654.42 | #2589534.40 | RT. 596.30' STA. 415+85.44 |
| #9721 | #2037443.89 | #2588437.70 | LT. 496.08' STA. 423+81.32 |
| #9722 | #2037464.25 | #2588965.19 | RT. 31.50' STA. 423+99.25 |
| #9723 | #2037502.81 | #2589587.02 | RT. 653.50' STA. 424+34.96 |

BENCH MARK: DISC ON RETAINING WALL (POINT #1403 VERTICAL CONTROL STATION)
LT. 53.05' STA. 417+29.50 ELEV. 732.91

I Hereby state that this project is based on WINGIS control coordinates

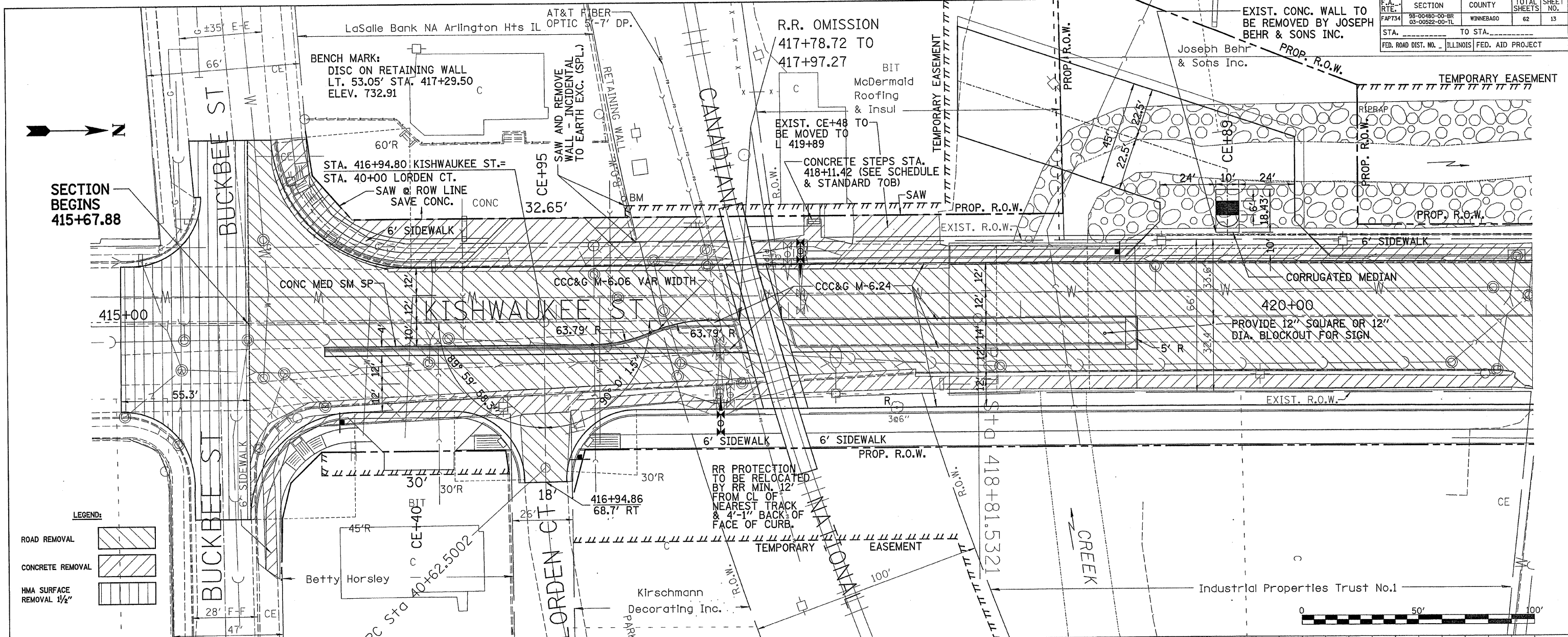
John Huseman
John Huseman PLS

| | | | | |
|---------------------|----------------|-----------|------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR | WINNEBAGO | 62 | 13 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | | FED. AID PROJECT | |

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| DATE | BY |
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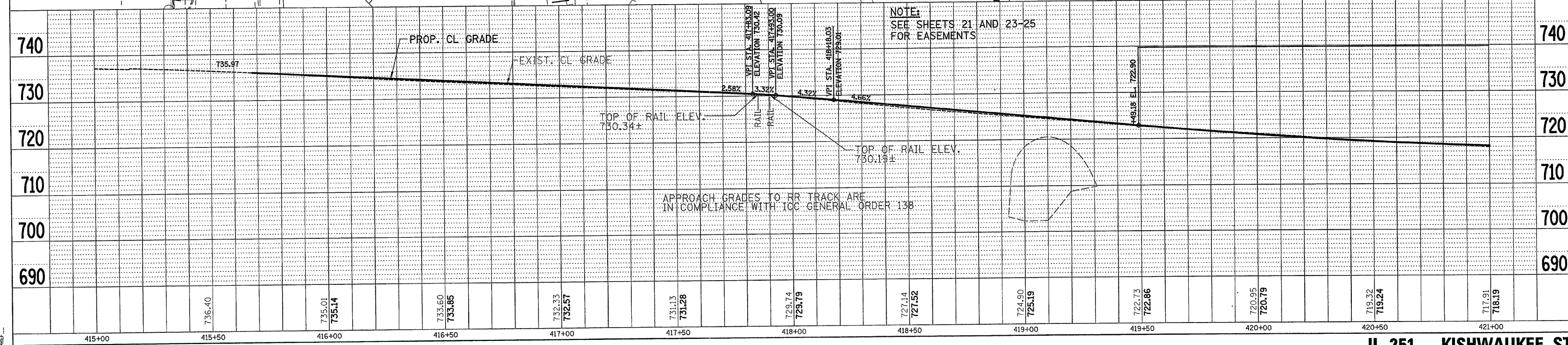
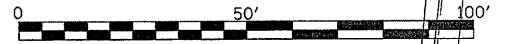
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| DATE | BY |
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LEGEND:

| | |
|--------------------------|--|
| ROAD REMOVAL | |
| CONCRETE REMOVAL | |
| HMA SURFACE REMOVAL 1/2" | |



NOTE:
SEE SHEETS 21 AND 23-25
FOR EASEMENTS

APPROACH GRADES TO RR TRACK ARE
IN COMPLIANCE WITH ICC GENERAL ORDER 138

BENCH MARK:
DISC ON RETAINING WALL
LT. 53.05' STA. 417+29.50
ELEV. 732.91

EXIST. CONC. WALL TO
BE REMOVED BY JOSEPH
BEHR & SONS INC.
Joseph Behr
& Sons Inc.

NOTE: LEWIS STREET TO BE EXTENDED
AND WIDENED BY OTHERS
LIMITS OF CONST.
STA. 9+00 (LEWIS ST.)

LEGEND:

| | |
|------------------|--|
| ROAD REMOVAL | |
| CONCRETE REMOVAL | |

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------------------------|------------------|--------------|-----------|
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 14 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

| DATE | BY |
|------|----|
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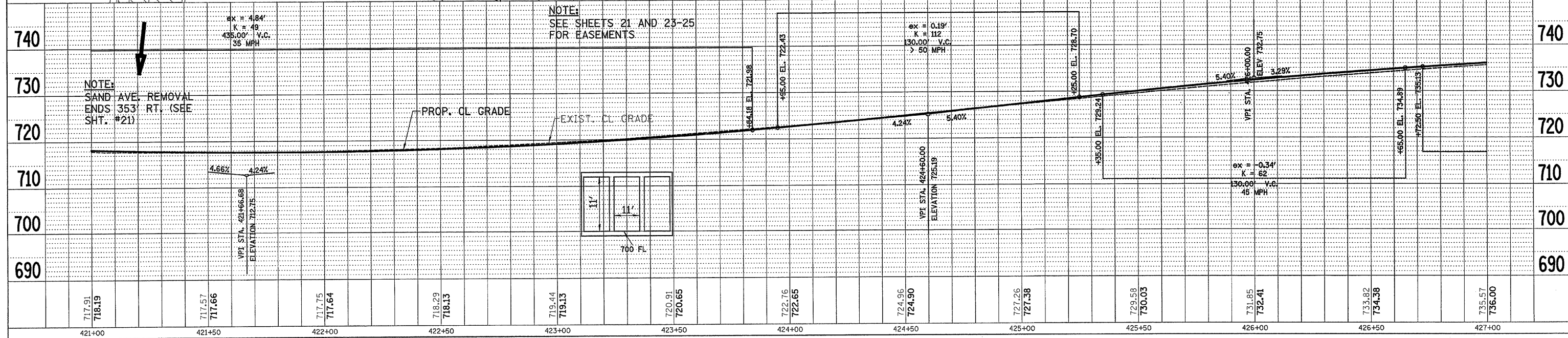
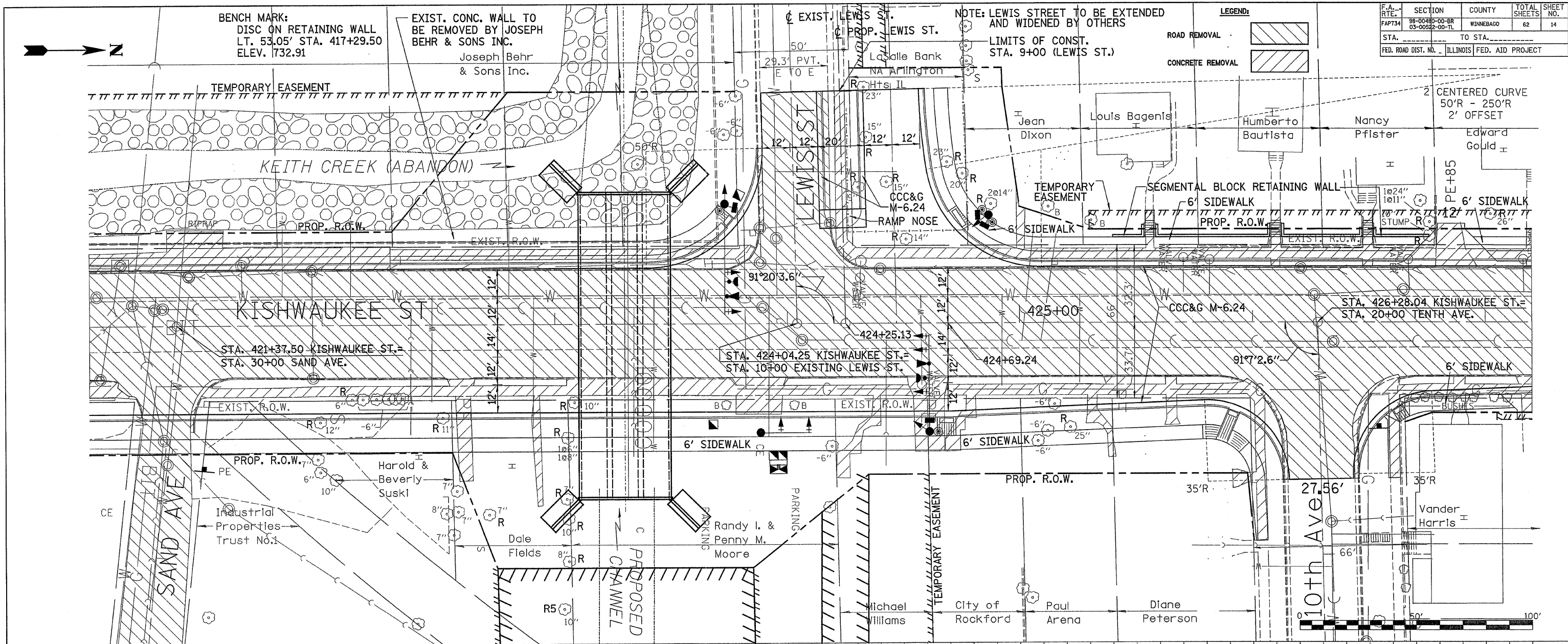
REVISIONS:

| NO. | DESCRIPTION |
|-----|---------------------------|
| 1 | DATE PLOTTED |
| 2 | ALIGNMENT CHECKED |
| 3 | GRADES CHECKED |
| 4 | STRUCTURE NOTATIONS CPOVD |
| 5 | DATE |
| 6 | BY |

| DATE | BY |
|------|----|
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REVISIONS:

| NO. | DESCRIPTION |
|-----|---------------------------|
| 1 | DATE PLOTTED |
| 2 | ALIGNMENT CHECKED |
| 3 | GRADES CHECKED |
| 4 | STRUCTURE NOTATIONS CPOVD |
| 5 | DATE |
| 6 | BY |

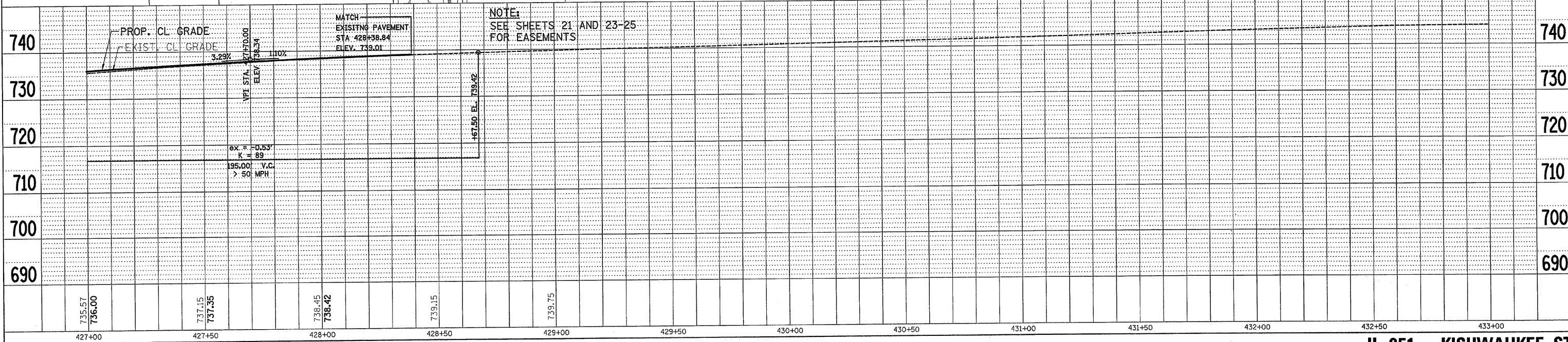
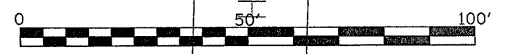
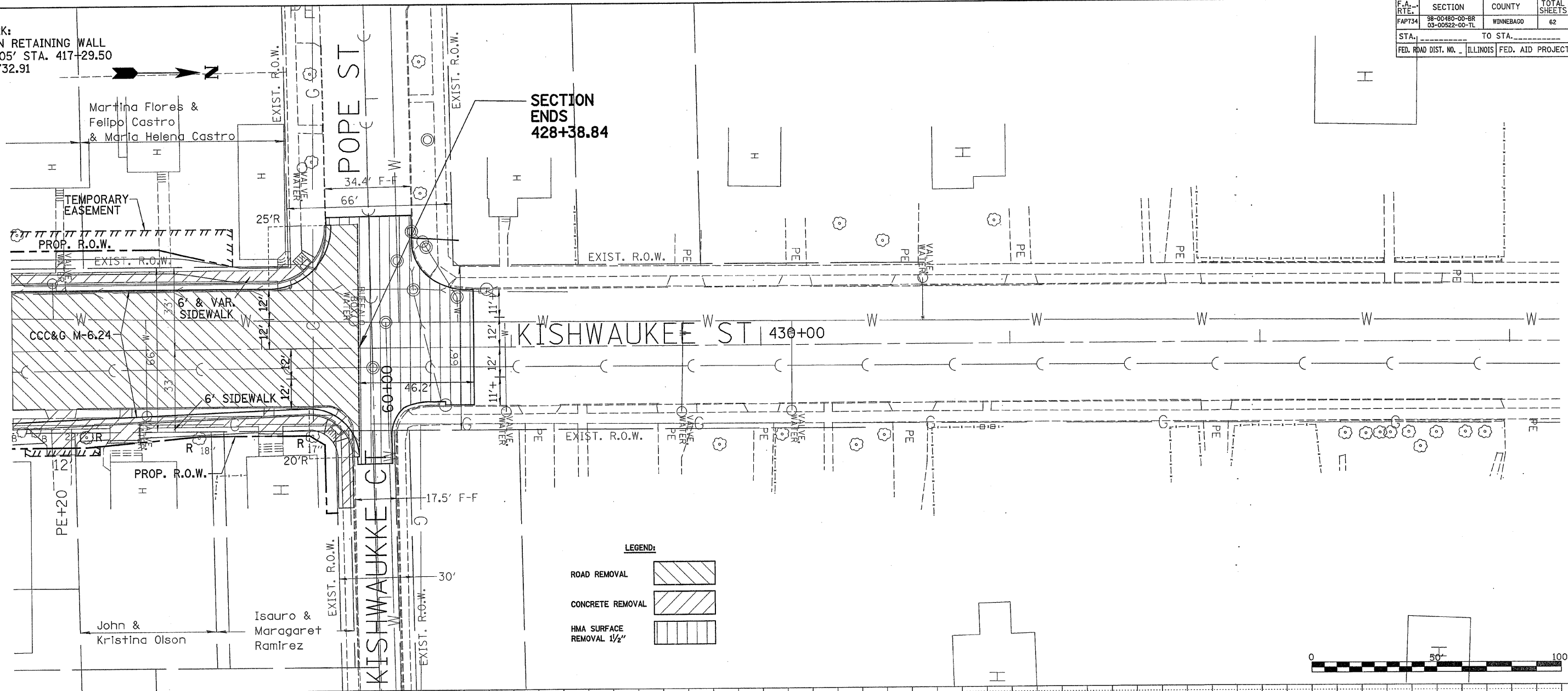


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|---------------------------|----------------------------------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 15 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ | | ILLINOIS FED. AID PROJECT | | |

BENCH MARK:
DISC ON RETAINING WALL
LT. 53.05' STA. 417+29.50
ELEV. 732.91

| | |
|------|----|
| DATE | BY |
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| DATE | BY |
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| DATE | BY |
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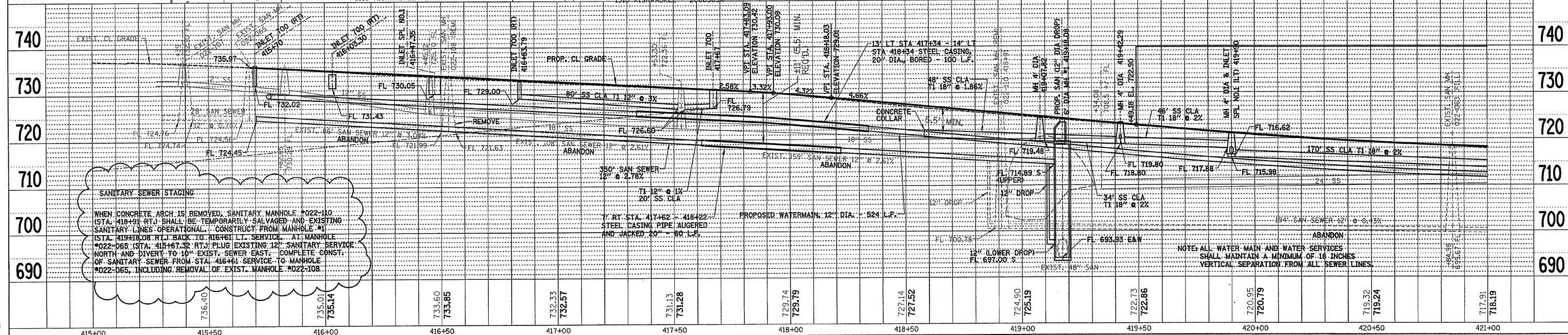
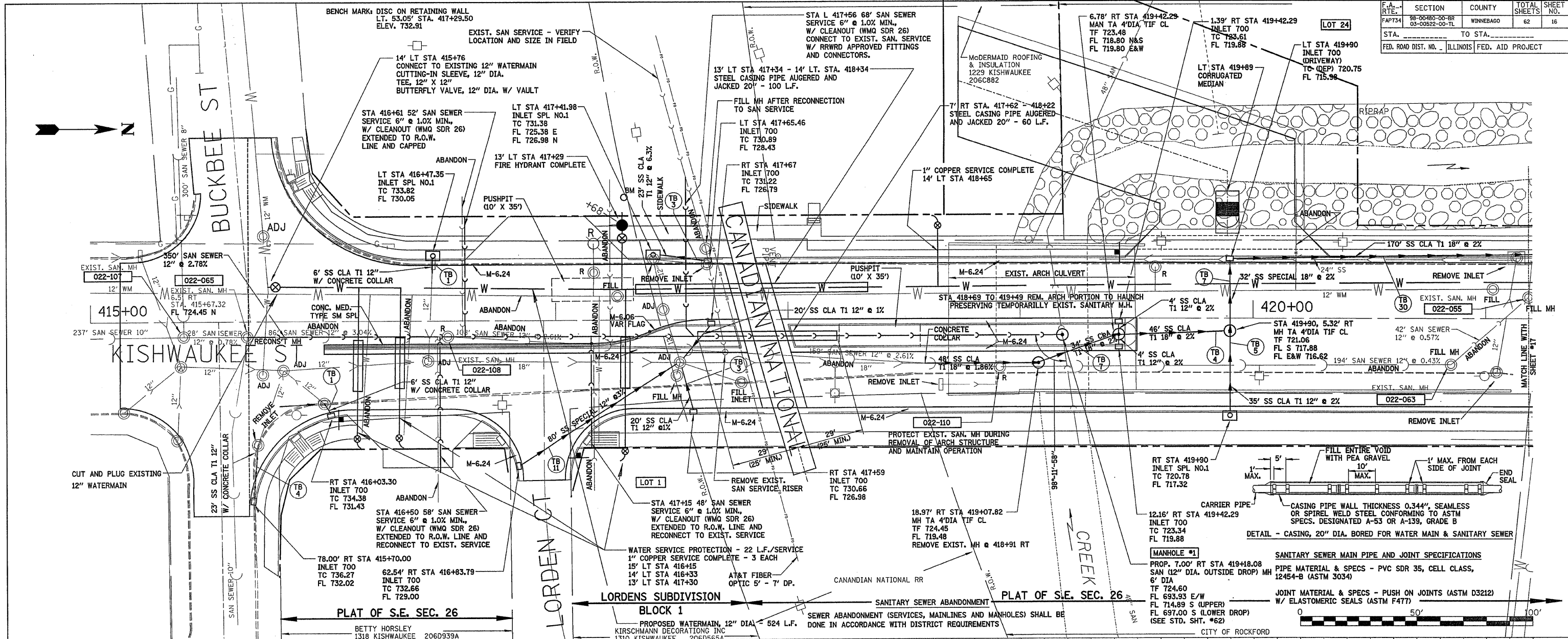
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| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------------------------------|----------------------------------|-----------|--------------|-----------|
| FAP734 | 98-00480-00-BR 03-00822-00-TL | WINNEBAGO | 62 | 16 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

| DATE | BY | REVISION |
|------|----|----------|
| | | |

| DATE | BY | REVISION |
|------|----|----------|
| | | |



MANHOLE #1
PROP. 7.00' RT STA 419+08.08
SAN 12" DIA. OUTSIDE DROP) MH
6" DIA
TF 724.60
FL 693.93 E/W
FL 714.89 S (UPPER)
FL 697.00 S (LOWER DROP)
(SEE STD. SHT. #62)

SANITARY SEWER MAIN PIPE AND JOINT SPECIFICATIONS
PIPE MATERIAL & SPECS - PVC SDR 35, CELL CLASS, 12454-B (ASTM 3034)
JOINT MATERIAL & SPECS - PUSH ON JOINTS (ASTM D3212) W/ ELASTOMERIC SEALS (ASTM F477)

DETAIL - CASING, 20" DIA. BORED FOR WATER MAIN & SANITARY SEWER
CARRYER PIPE
12.16' RT STA 419+42.29
INLET SPL NO.1
TC 720.78
FL 719.88
CASING PIPE WALL THICKNESS 0.344", SEAMLESS OR SPIRAL WELD STEEL CONFORMING TO ASTM SPECS. DESIGNATED A-53 OR A-139, GRADE B
1" MAX. FROM EACH SIDE OF JOINT
END SEAL

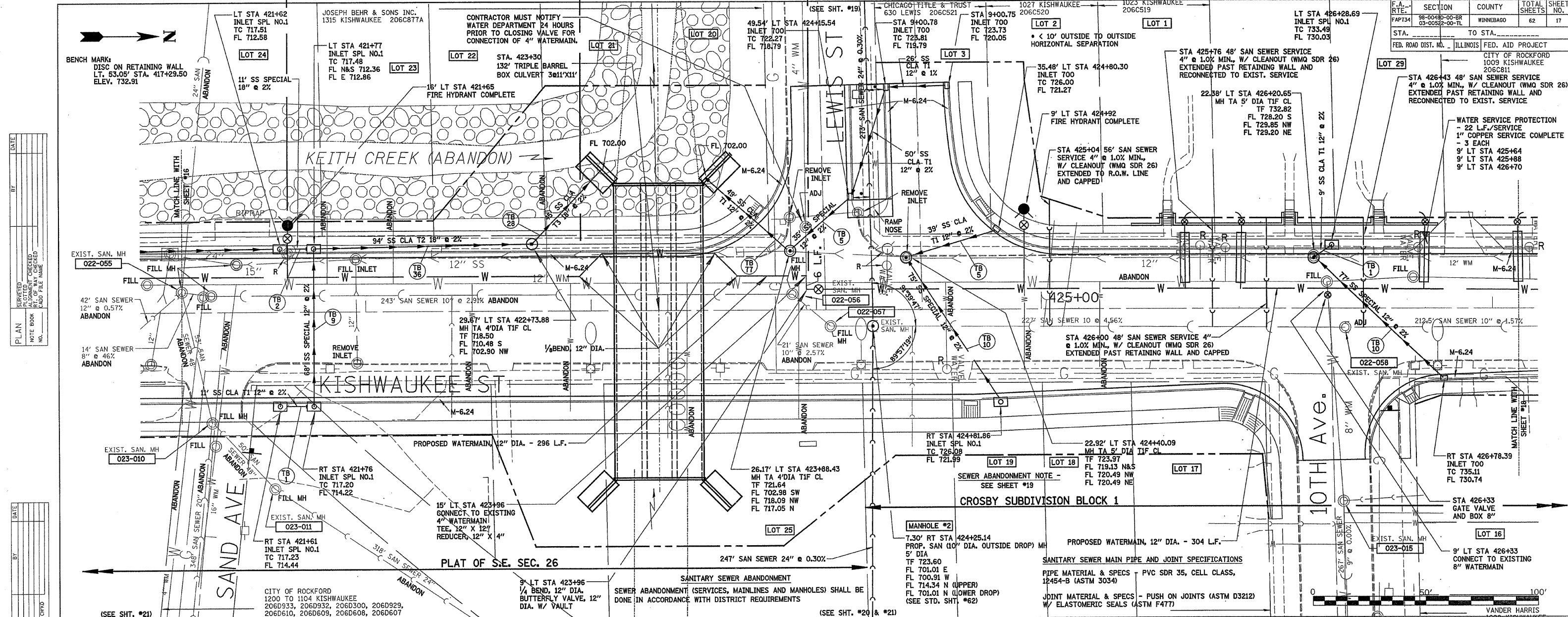
IL 251 - DRAINAGE, WATER MAIN & SANITARY SEWER DETAILS

STAGE IV WATERMAIN

STAGE II WATERMAIN

STAGE III WATERMAIN

STAGE II WATERMAIN



| DATE | BY | REVISION |
|------|----|----------------------|
| | | PLAN |
| | | REVIEWED |
| | | PROJECTED |
| | | ALIGNMENT CHECKED |
| | | CONSTRUCTION CHECKED |
| | | NO. _____ |
| | | DATE _____ |
| | | BY _____ |

| DATE | BY | REVISION |
|------|----|----------------|
| | | PROFILE |
| | | REVIEWED |
| | | GRADES CHECKED |
| | | NO. _____ |
| | | DATE _____ |
| | | BY _____ |

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|----------------|----------------|-----------|--------------|-----------|
| 98-00480-00-BR | 03-00522-00-TL | WINNEBAGO | 62 | 17 |

FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT

CITY OF ROCKFORD
1009 KISHWAUKEE
206C01

STA 426+43 48" SAN SEWER SERVICE
4" @ 1.0% MIN., W/ CLEANOUT (WMO SDR 26)
EXTENDED PAST RETAINING WALL AND
RECONNECTED TO EXIST. SERVICE

WATER SERVICE PROTECTION
- 22 L.F. SERVICE
1" COPPER SERVICE COMPLETE
- 3 EACH
9' LT STA 425+64
9' LT STA 425+88
9' LT STA 426+70

STA 425+76 48" SAN SEWER SERVICE
4" @ 1.0% MIN., W/ CLEANOUT (WMO SDR 26)
EXTENDED PAST RETAINING WALL AND
RECONNECTED TO EXIST. SERVICE

22.38' LT STA 426+20.65
MH TA 5' DIA TIF CL
TF 732.82
FL 728.20 S
FL 729.85 NW
FL 729.20 NE

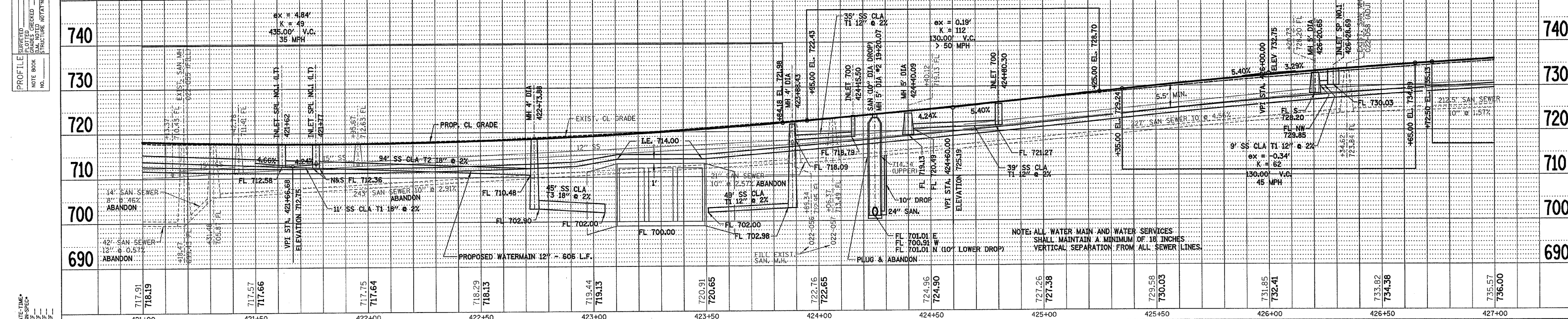
RT STA 426+78.39
INLET 700
TC 735.11
FL 730.74

STA 426+33
GATE VALVE
AND BOX 8"

9' LT STA 426+33
CONNECT TO EXISTING
8" WATERMAIN

267' SAN SEWER
9" @ 0.00%

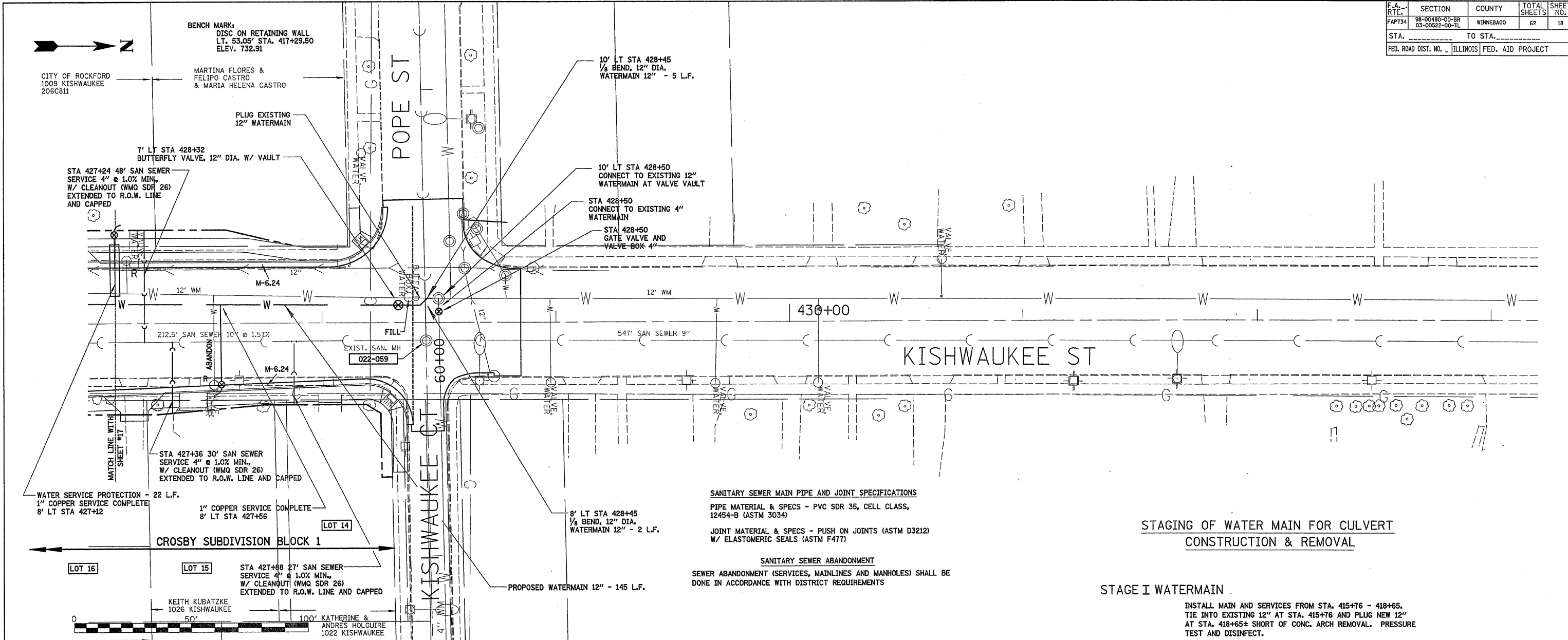
212.5' SAN SEWER
10" @ 1.57%



| F.A. SITE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------------------------|---------------------------|--------------|-----------|
| FA734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 18 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ | | ILLINOIS FED. AID PROJECT | | |

| DATE | BY | REVISION |
|------|----|----------|
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| DATE | BY | REVISION |
|------|----|----------|
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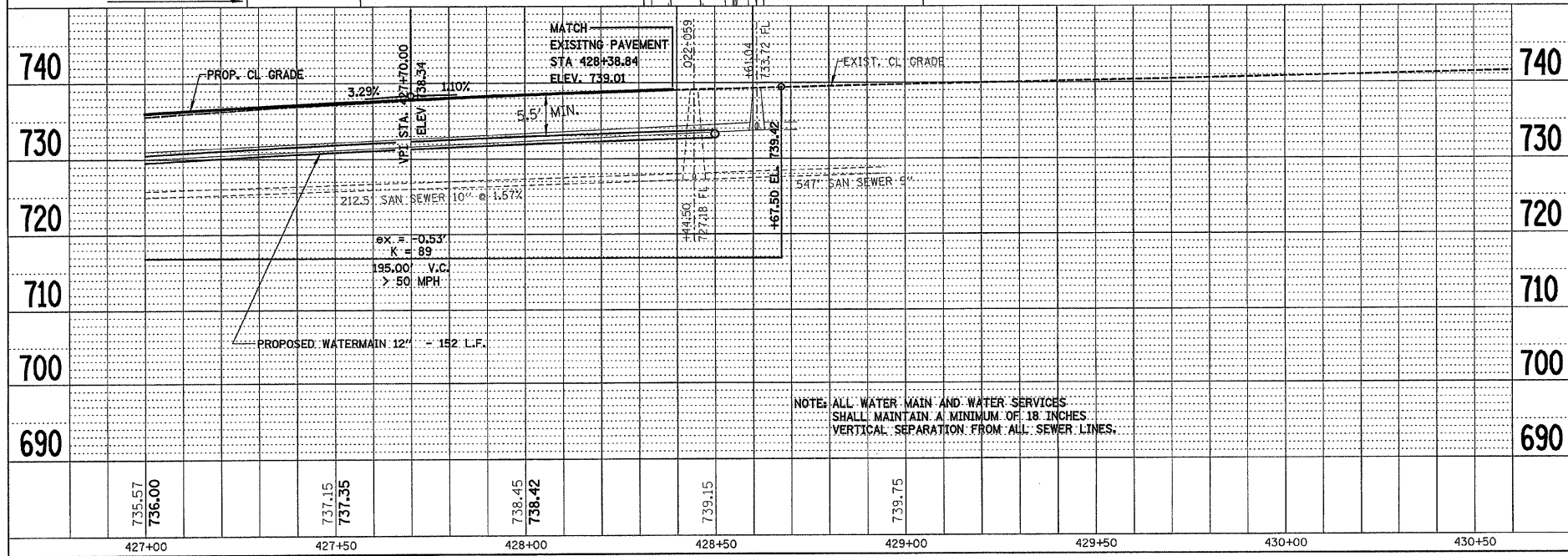


SANITARY SEWER MAIN PIPE AND JOINT SPECIFICATIONS
 PIPE MATERIAL & SPECS - PVC SDR 35, CELL CLASS, 12454-B (ASTM 3034)
 JOINT MATERIAL & SPECS - PUSH ON JOINTS (ASTM D3212) W/ ELASTOMERIC SEALS (ASTM F477)

SANITARY SEWER ABANDONMENT
 SEWER ABANDONMENT (SERVICES, MAINLINES AND MANHOLES) SHALL BE DONE IN ACCORDANCE WITH DISTRICT REQUIREMENTS

STAGING OF WATER MAIN FOR CULVERT CONSTRUCTION & REMOVAL

- STAGE I WATERMAIN**
 INSTALL MAIN AND SERVICES FROM STA. 415+76 - 418+65. TIE INTO EXISTING 12" AT STA. 415+76 AND PLUG NEW 12" AT STA. 418+65± SHORT OF CONC. ARCH REMOVAL. PRESSURE TEST AND DISINFECT.
- STAGE II WATERMAIN**
 INSTALL MAIN AND SERVICES FROM STA. 423+96 VALVE TO 428+45. NO HOOK-UP TO MAIN OR HOMES. INSTALL 12" MAIN AND HYDRANT FROM STA. 421+65 - 422+90± AND PLUG MAIN WEST OF 421+65. DISINFECT DURING STAGE III
- STAGE III WATERMAIN**
 INSTALL PARTIAL CULVERT AT STA. 423+60 AND THEN CONNECT STAGE II MAIN OVER CULVERT. MAKE CONNECTION TO MAIN AT STA. 428+50 AND 423+96. PRESSURE TEST AND DISINFECT. CONNECT SERVICES TO HOMES.
- STAGE IV WATERMAIN**
 REMOVE CONC. ARCH AT STA. 418+69 - 419+49 AND INSTALL 12" MAIN FROM STA. 418+65 - 421+62. PRESSURE TEST AND DISINFECT. MAKE CONNECTION TO LIVE MAIN.

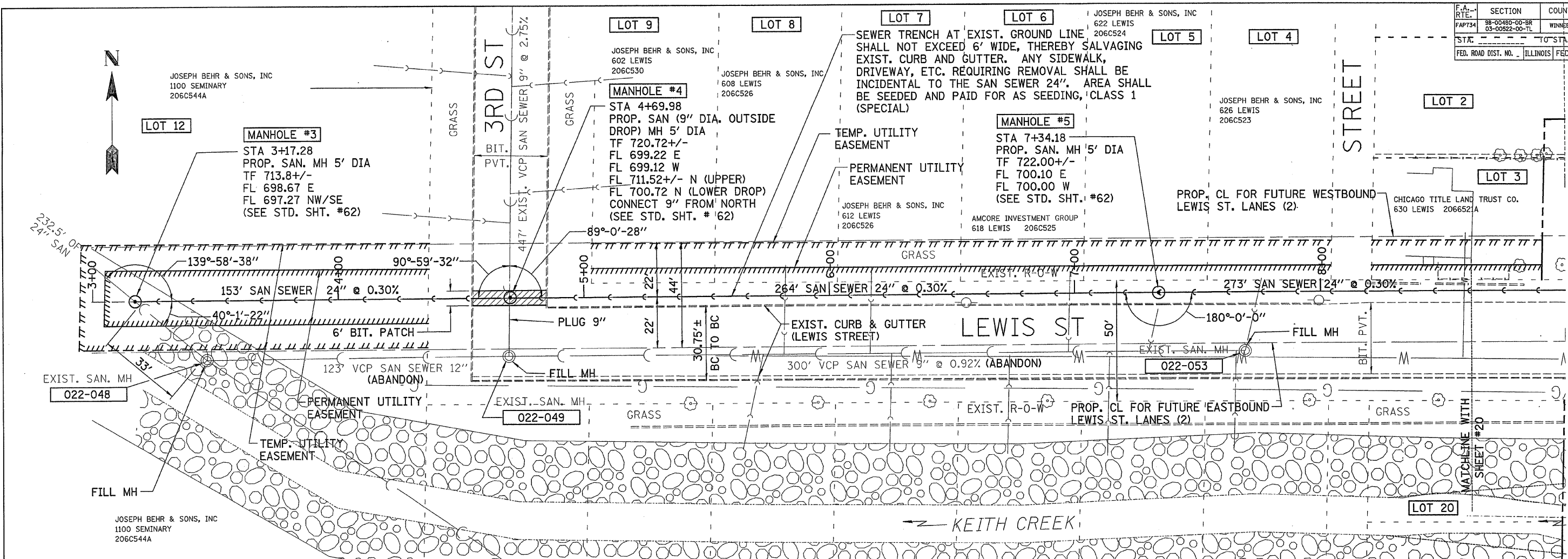


SPAFFORDS SUBDIVISION

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|----------------------|----------------------------------|---------------------------------------------------|--------------|-----------|
| FAP734 | 98-00480-00-BR 03-00822-00-TL | WINNEBAGO | 62 | 19 |
| STA. --- TO STA. --- | | FED. ROAD DIST. NO. --- ILLINOIS FED. AID PROJECT | | |

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| DATE | BY | NO. |
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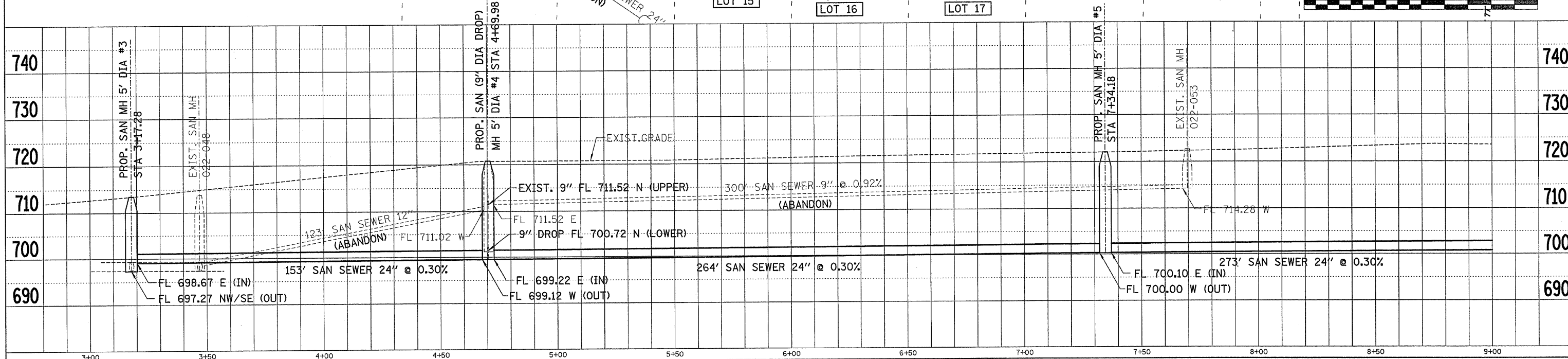
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|------|----|-----|
| DATE | BY | NO. |
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SANITARY SEWER MAIN PIPE AND JOINT SPECIFICATIONS

PIPE MATERIAL & SPECS - PVC SDR 35, CELL CLASS, 12454-B (ASTM 3034)

JOINT MATERIAL & SPECS - PUSH ON JOINTS (ASTM D3212) W/ ELASTOMERIC SEALS (ASTM F477)



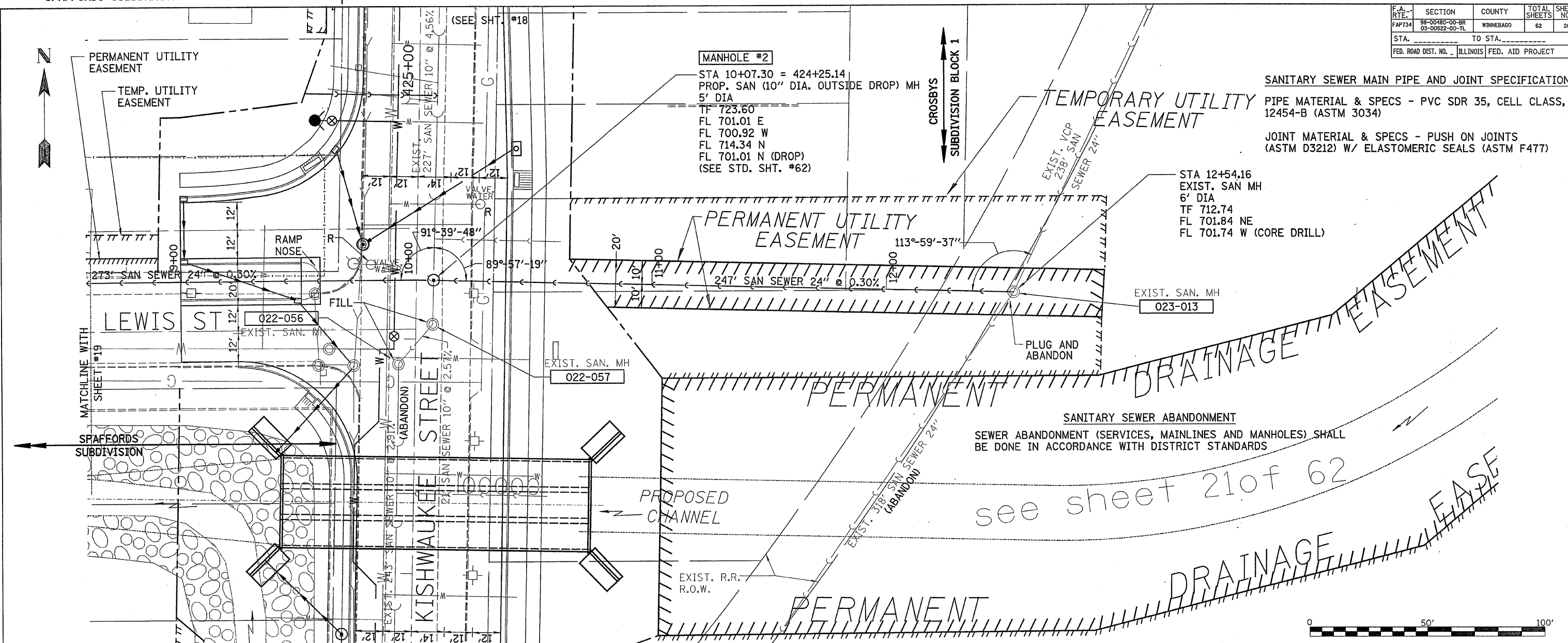
LEWIS ST. PROPOSED 24" SAN. SEWER

SPAFFORDS SUBDIVISION

| | | | | |
|-----------------------------------------------|----------------------------------|-----------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 20 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

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| DATE | BY |
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SANITARY SEWER MAIN PIPE AND JOINT SPECIFICATIONS

PIPE MATERIAL & SPECS - PVC SDR 35, CELL CLASS, 12454-B (ASTM 3034)

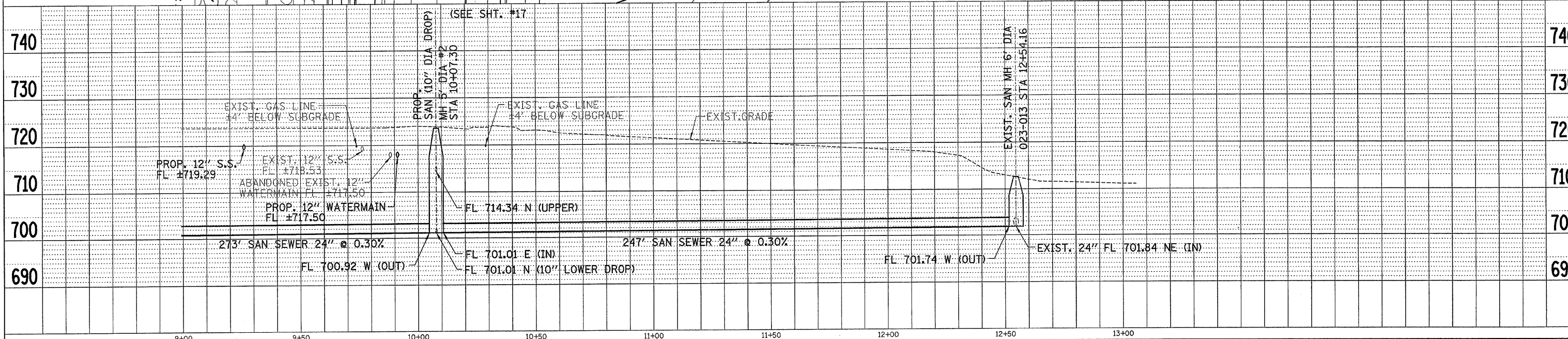
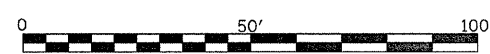
JOINT MATERIAL & SPECS - PUSH ON JOINTS (ASTM D3212) W/ ELASTOMERIC SEALS (ASTM F477)

MANHOLE #2
 STA 10+07.30 = 424+25.14
 PROP. SAN (10" DIA. OUTSIDE DROP) MH 5' DIA
 TF 723.60
 FL 701.01 E
 FL 700.92 W
 FL 714.34 N
 FL 701.01 N (DROP)
 (SEE STD. SHT. #62)

STA 12+54.16
 EXIST. SAN MH 6' DIA
 TF 712.74
 FL 701.84 NE
 FL 701.74 W (CORE DRILL)

SANITARY SEWER ABANDONMENT
 SEWER ABANDONMENT (SERVICES, MAINLINES AND MANHOLES) SHALL BE DONE IN ACCORDANCE WITH DISTRICT STANDARDS

see sheet 21 of 62

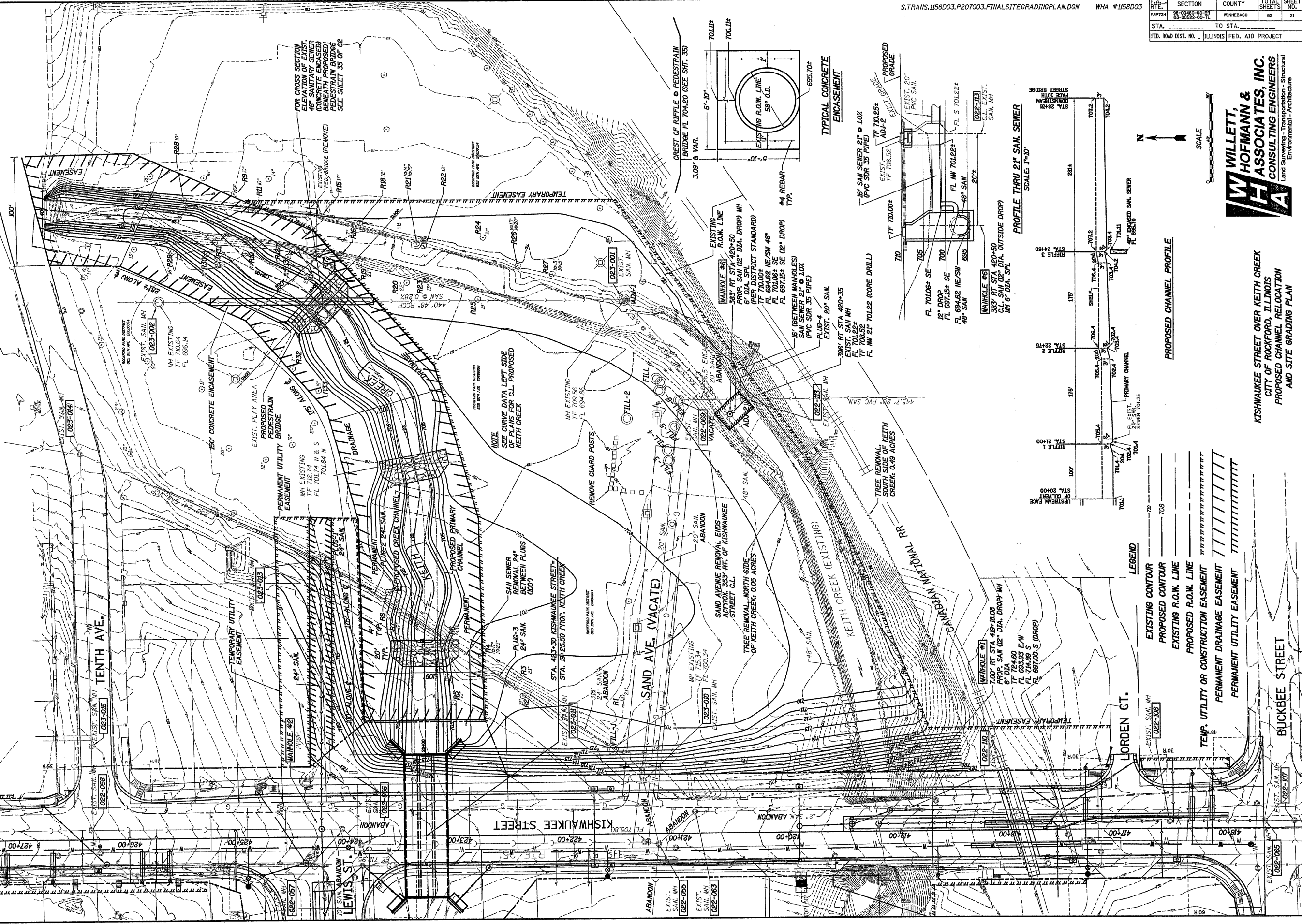


LEWIS ST. PROPOSED 24" SAN. SEWER

| | | | |
|---------------------|-----------|--------------|------------------|
| SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 88-00480-00-ER | WINNEBAGO | 62 | 21 |
| FAPT24 | | | |
| STA. | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS | | FED. AID PROJECT |

| PROP. CURVE CIRCLED2 | | PROP. CURVE CIRCLED3 | | PROP. CURVE CIRCLED4 | | PROP. CURVE CIRCLED5 | | PROP. CURVE CIRCLED6 | | PROP. CURVE CIRCLED7 | | PROP. CURVE CIRCLED8 | | PROP. CURVE CIRCLED9 | | PROP. CURVE CIRCLED10 | |
|----------------------|-----------|----------------------|-----------|----------------------|-----------|----------------------|-----------|----------------------|-----------|----------------------|-----------|----------------------|-----------|----------------------|-----------|-----------------------|-----------|
| P.I. STA. | P.T. STA. | P.I. STA. | P.T. STA. | P.I. STA. | P.T. STA. | P.I. STA. | P.T. STA. | P.I. STA. | P.T. STA. | P.I. STA. | P.T. STA. | P.I. STA. | P.T. STA. | P.I. STA. | P.T. STA. | P.I. STA. | P.T. STA. |
| 41.11 | 41.89 | 42.37 | 43.15 | 43.71 | 44.49 | 45.05 | 45.83 | 46.39 | 47.17 | 47.73 | 48.51 | 49.07 | 49.85 | 50.41 | 51.19 | 51.75 | 52.53 |
| 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' |
| 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° |
| 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' |
| 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' | 11.50' |
| 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° | 0.42° |
| 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' | 1.89' |

PROPOSED CHANNEL CURVE DATA



LEGEND

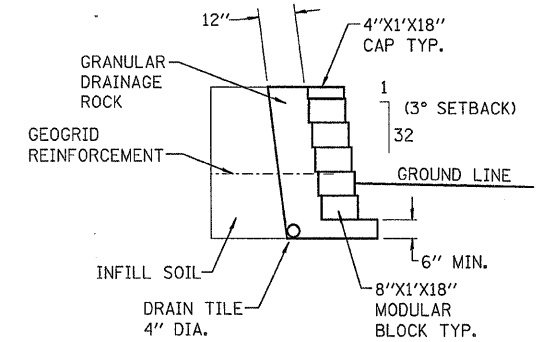
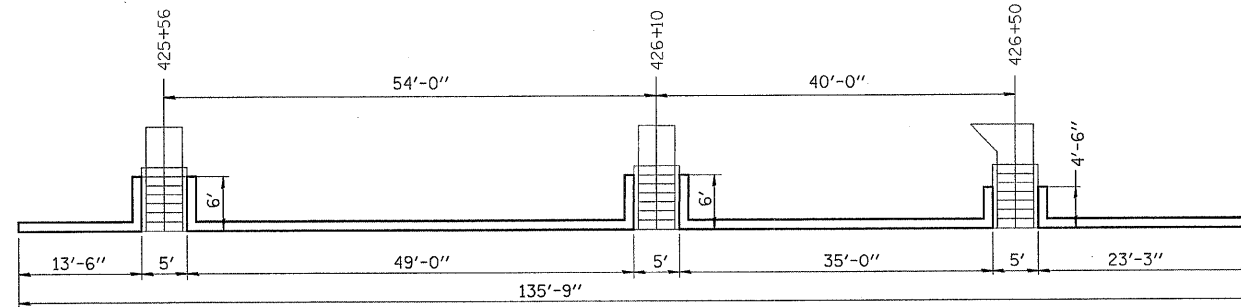
| | |
|----------------------------------------|-----|
| EXISTING CONTOUR | --- |
| PROPOSED CONTOUR | --- |
| EXISTING R.O.W. LINE | --- |
| PROPOSED R.O.W. LINE | --- |
| TEMP. UTILITY OR CONSTRUCTION EASEMENT | |
| PERMANENT DRAINAGE EASEMENT | |
| PERMANENT UTILITY EASEMENT | |



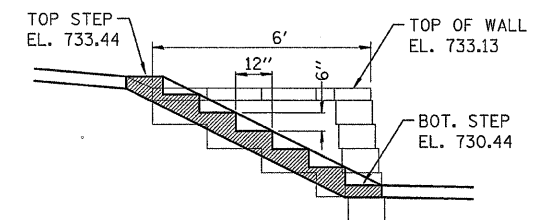
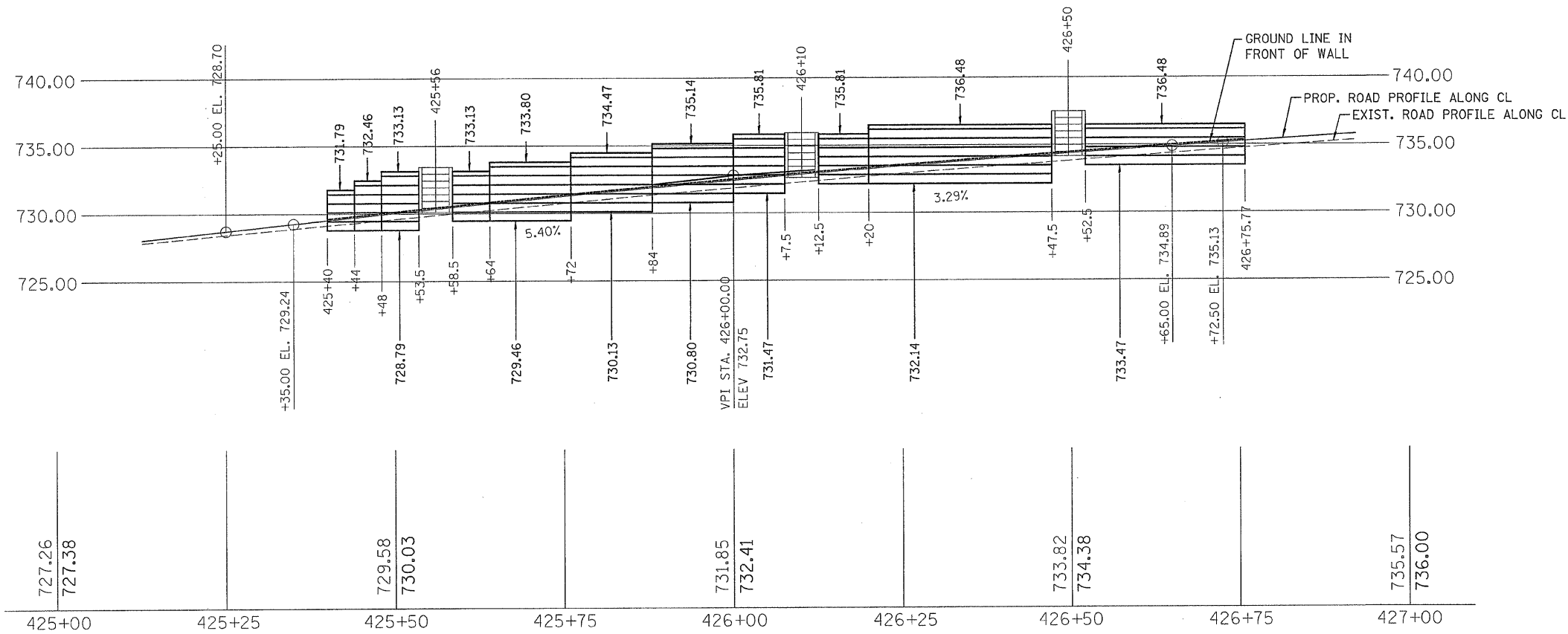
KISHWAUKEE STREET OVER KEITH CREEK
CITY OF ROCKFORD, ILLINOIS
PROPOSED CHANNEL RELOCATION
AND SITE GRADING PLAN

SEGMENTAL CONCRETE BLOCK WALL SYSTEM

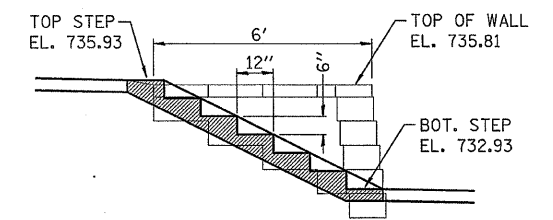
| F.A. R.T.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------------------------------------|----------------------------------|---------------|--------------|-----------|
| FAP734 | 98-00480-00-BR 03-00222-00-TL | WINNEBAGO | 62 | 22 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT | | | | |



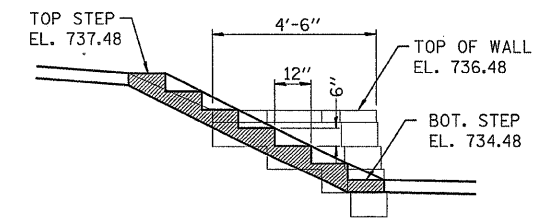
TYPICAL CROSS SECTION THRU RETAINING WALL



CROSS SECTION @ 425+56



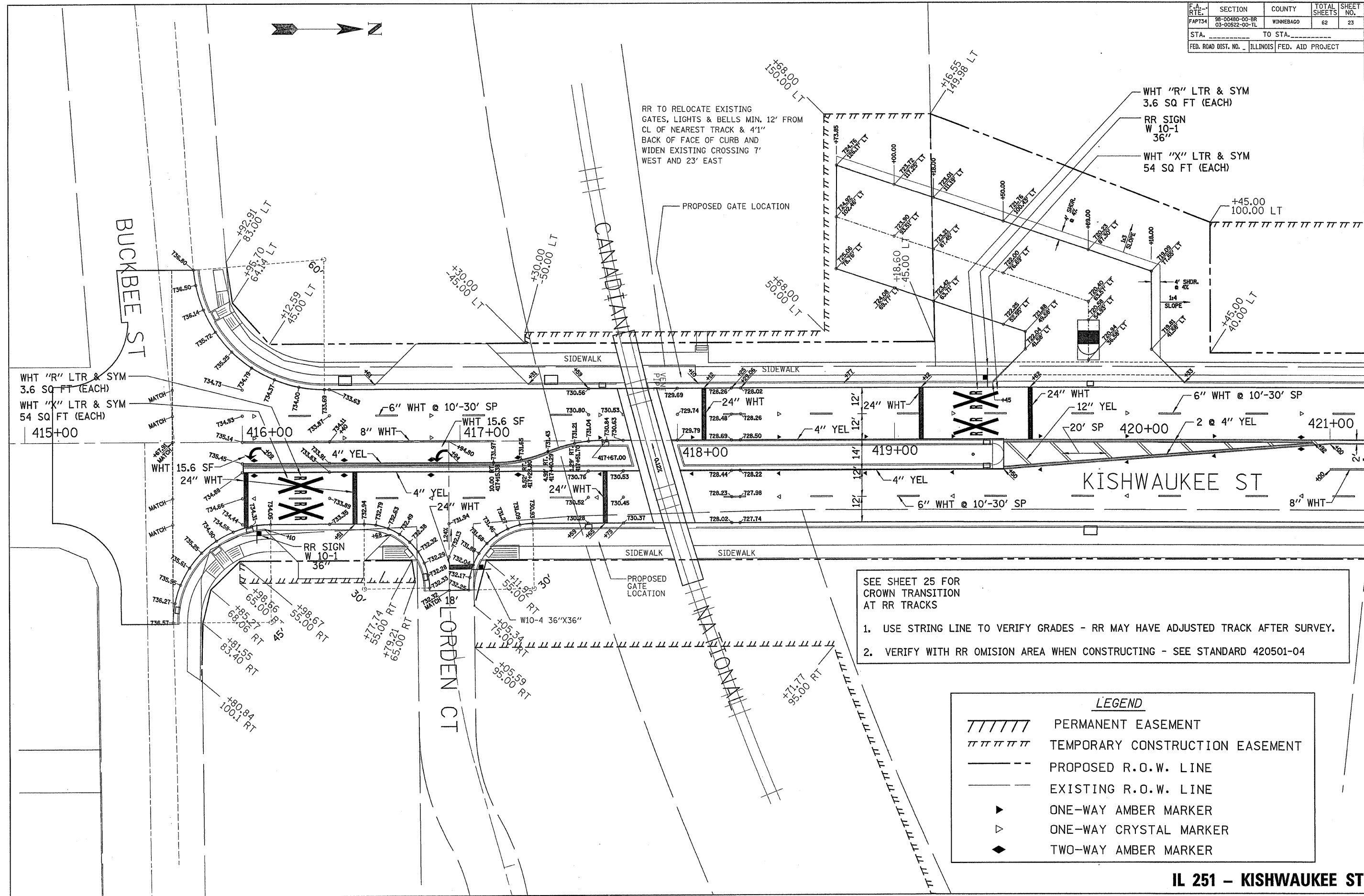
CROSS SECTION @ 426+10



CROSS SECTION @ 426+50

DATE/TIME
 000N-SPEC
 REF
 REF
 REF

| | | | | |
|---------------------|----------------------------------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAPT34 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 23 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



RR TO RELOCATE EXISTING GATES, LIGHTS & BELLS MIN. 12' FROM CL OF NEAREST TRACK & 41" BACK OF FACE OF CURB AND WIDEN EXISTING CROSSING 7' WEST AND 23' EAST

SEE SHEET 25 FOR CROWN TRANSITION AT RR TRACKS

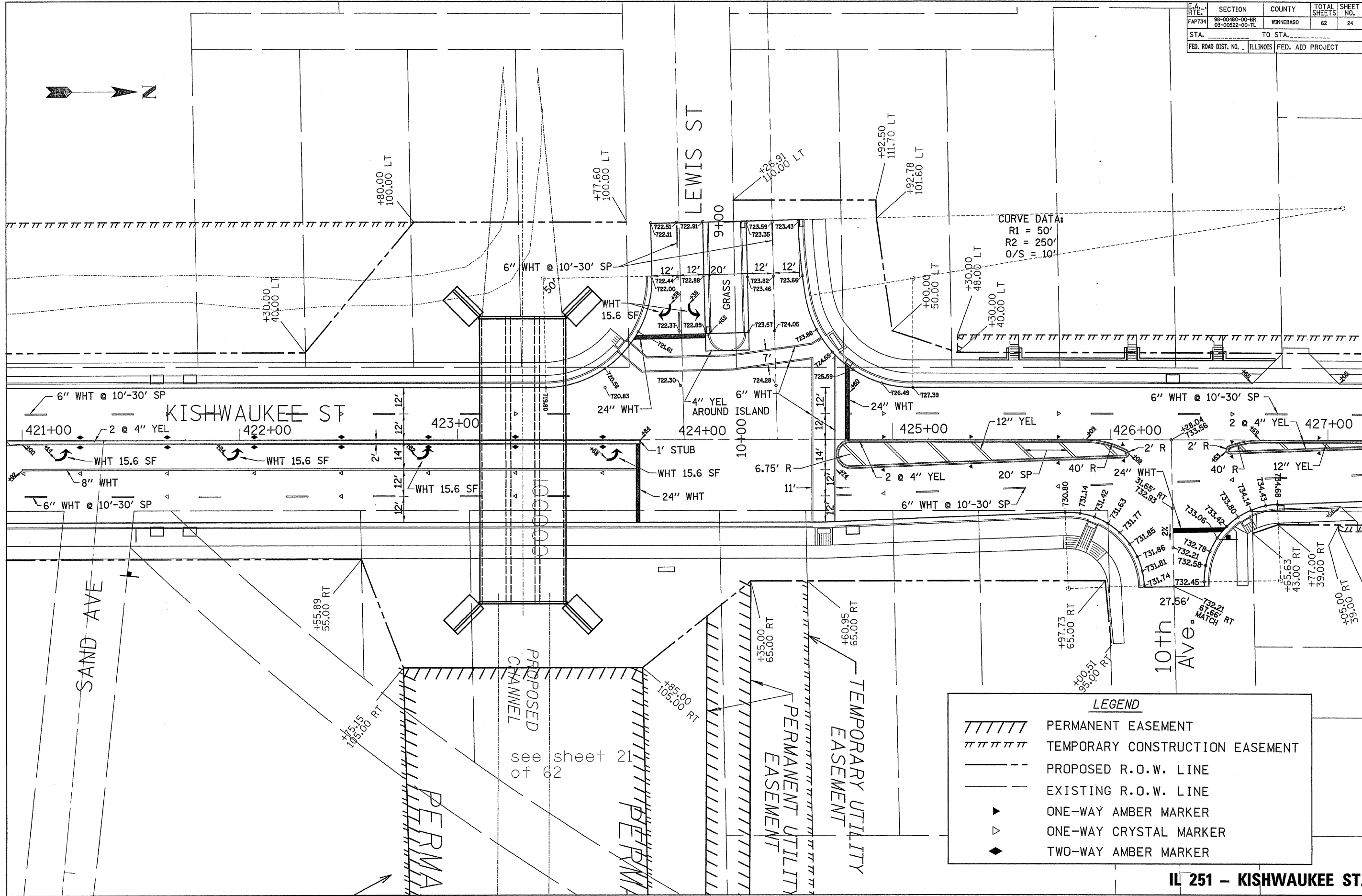
1. USE STRING LINE TO VERIFY GRADES - RR MAY HAVE ADJUSTED TRACK AFTER SURVEY.
2. VERIFY WITH RR OMISSION AREA WHEN CONSTRUCTING - SEE STANDARD 420501-04

LEGEND

| | |
|--------|---------------------------------|
| TTTTTT | PERMANENT EASEMENT |
| TTTTTT | TEMPORARY CONSTRUCTION EASEMENT |
| --- | PROPOSED R.O.W. LINE |
| --- | EXISTING R.O.W. LINE |
| ▶ | ONE-WAY AMBER MARKER |
| ▽ | ONE-WAY CRYSTAL MARKER |
| ◆ | TWO-WAY AMBER MARKER |

**IL 251 - KISHWAUKEE ST.
PAVEMENT MARKING, ROW, EASEMENT DETAILS & PAVEMENT ELEVATIONS**

| E.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------------|----------------------------------|-----------------------------------------------------|--------------|-----------|
| FAPT34 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 24 |
| STA. _____ TO STA. _____ | | FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT | | |



CURVE DATA:
 R1 = 50'
 R2 = 250'
 O/S = 10'

LEGEND

| | |
|--|---------------------------------|
| | PERMANENT EASEMENT |
| | TEMPORARY CONSTRUCTION EASEMENT |
| | PROPOSED R.O.W. LINE |
| | EXISTING R.O.W. LINE |
| | ONE-WAY AMBER MARKER |
| | ONE-WAY CRYSTAL MARKER |
| | TWO-WAY AMBER MARKER |

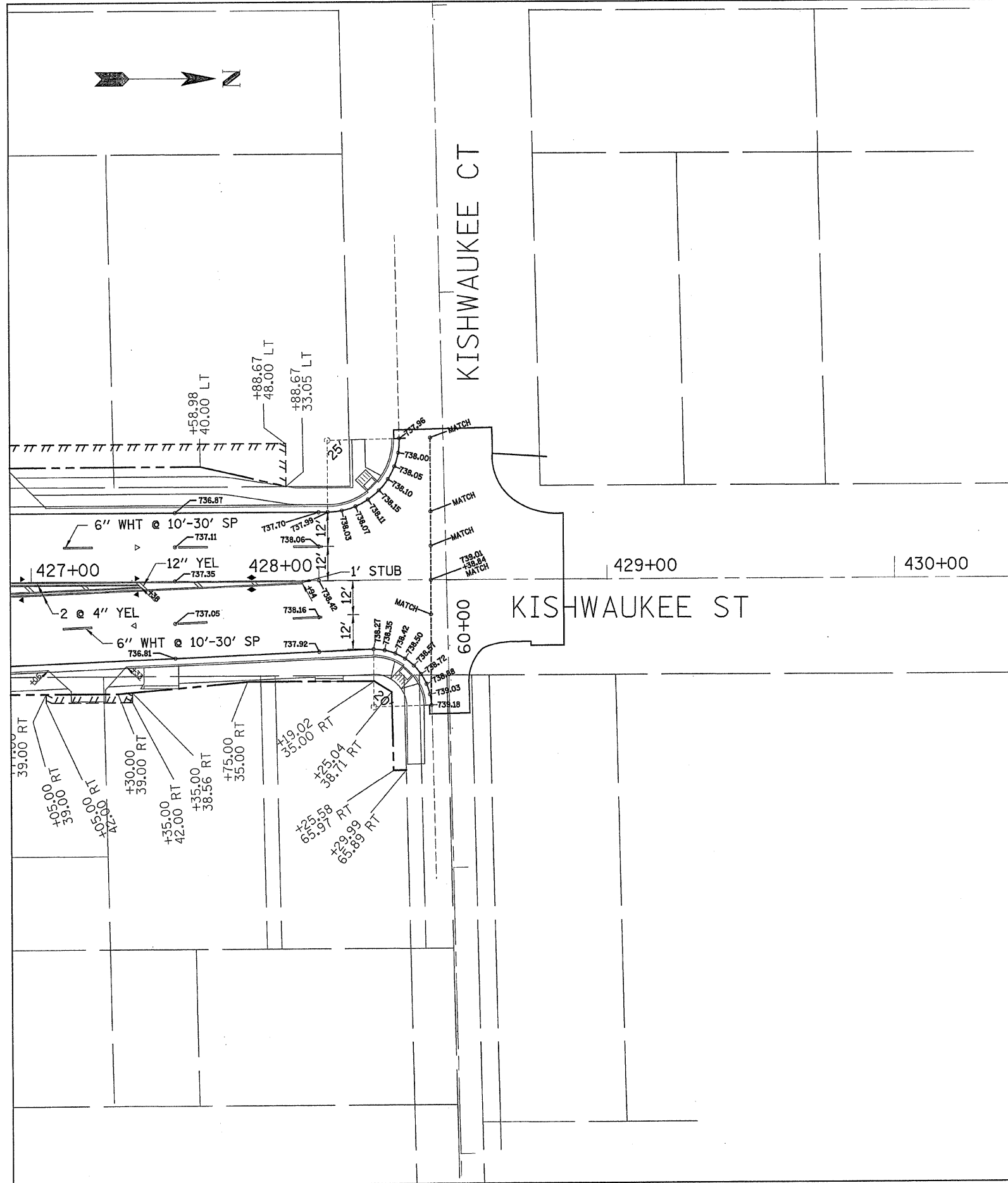
IL 251 - KISHWAUKEE ST.

PAVEMENT MARKING, ROW, EASEMENT DETAILS & PAVEMENT ELEVATIONS

| | | | | |
|---------------------|----------------------------------|-----------|------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 25 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

CROWN TRANSITION AT RAILROAD

| STA | PERCENT | | LT | | CL | RT | | |
|----------|----------|----------|--------|--------|--------|--------|--------|--------|
| | CROWN LT | CROWN RT | 24 | 12 | 0 | 14 | 26 | 38 |
| 41758.84 | 2 | -2 | 730.57 | 730.81 | 731.05 | 730.77 | 730.53 | 730.29 |
| 41759 | 1.9885 | -1.9870 | 730.56 | 730.80 | 731.04 | 730.76 | 730.52 | 730.28 |
| 41759.05 | 1.9850 | -1.9830 | 730.56 | 730.80 | 731.04 | 730.76 | 730.52 | 730.29 |
| 41760 | 1.9169 | -1.9059 | 730.56 | 730.79 | 731.02 | 730.75 | 730.52 | 730.30 |
| 41765.46 | 1.5258 | -1.4627 | 730.51 | 730.70 | 730.88 | 730.68 | 730.50 | 730.32 |
| 41767 | 1.4155 | -1.3378 | 730.50 | 730.67 | 730.84 | 730.65 | 730.49 | 730.33 |
| 41772.32 | 1.0344 | -0.9060 | 730.45 | 730.58 | 730.70 | 730.57 | 730.46 | 730.36 |
| 41775 | 0.8424 | -0.6885 | 730.43 | 730.53 | 730.63 | 730.53 | 730.45 | 730.37 |
| 41782.24 | 0.3238 | -0.1009 | 730.37 | 730.41 | 730.45 | 730.44 | 730.42 | 730.41 |
| 41782.4 | 0.3124 | -0.0879 | 730.37 | 730.40 | 730.44 | 730.43 | 730.42 | 730.41 |
| 41784 | 0.1978 | 0.0419 | 730.34 | 730.37 | 730.39 | 730.40 | 730.40 | 730.41 |
| 41785.01 | 0.1254 | 0.1239 | 730.33 | 730.34 | 730.36 | 730.38 | 730.39 | 730.41 |
| 41785.02 | 0.1247 | 0.1247 | 730.33 | 730.35 | 730.36 | 730.38 | 730.39 | 730.41 |
| 41786 | 0.1247 | 0.1247 | 730.30 | 730.32 | 730.33 | 730.35 | 730.36 | 730.38 |
| 41788.85 | 0.1247 | 0.1247 | 730.20 | 730.22 | 730.23 | 730.25 | 730.26 | 730.28 |
| 41790.5 | 0.1247 | 0.1247 | 730.15 | 730.17 | 730.18 | 730.20 | 730.21 | 730.23 |
| 41794.5 | 0.1247 | 0.1247 | 730.00 | 730.02 | 730.03 | 730.05 | 730.06 | 730.08 |
| 41795.1 | 0.1247 | 0.1247 | 729.97 | 729.99 | 730.00 | 730.02 | 730.03 | 730.05 |
| 41797.23 | 0.2431 | -0.0094 | 729.85 | 729.88 | 729.91 | 729.91 | 729.91 | 729.91 |
| 41800 | 0.3970 | -0.1839 | 729.69 | 729.74 | 729.79 | 729.76 | 729.74 | 729.72 |
| 41801.24 | 0.4660 | -0.2620 | 729.63 | 729.68 | 729.74 | 729.70 | 729.67 | 729.64 |
| 41808.06 | 0.8450 | -0.6914 | 729.24 | 729.34 | 729.44 | 729.34 | 729.26 | 729.18 |
| 41825 | 1.7866 | -1.7582 | 728.26 | 728.48 | 728.69 | 728.44 | 728.23 | 728.02 |
| 41828.83 | 1.9994 | -1.9994 | 728.03 | 728.27 | 728.51 | 728.23 | 727.99 | 727.75 |
| 41828.84 | 2 | -2 | 728.03 | 728.27 | 728.51 | 728.23 | 727.99 | 727.75 |
| 41829.06 | 2 | -2 | 728.02 | 728.26 | 728.50 | 728.22 | 727.98 | 727.74 |



| LEGEND | |
|--------|---------------------------------|
| TTTTTT | PERMANENT EASEMENT |
| TTTTTT | TEMPORARY CONSTRUCTION EASEMENT |
| --- | PROPOSED R.O.W. LINE |
| --- | EXISTING R.O.W. LINE |
| ▶ | ONE-WAY AMBER MARKER |
| ▷ | ONE-WAY CRYSTAL MARKER |
| ◆ | TWO-WAY AMBER MARKER |

IL 251 - KISHWAUKEE ST.

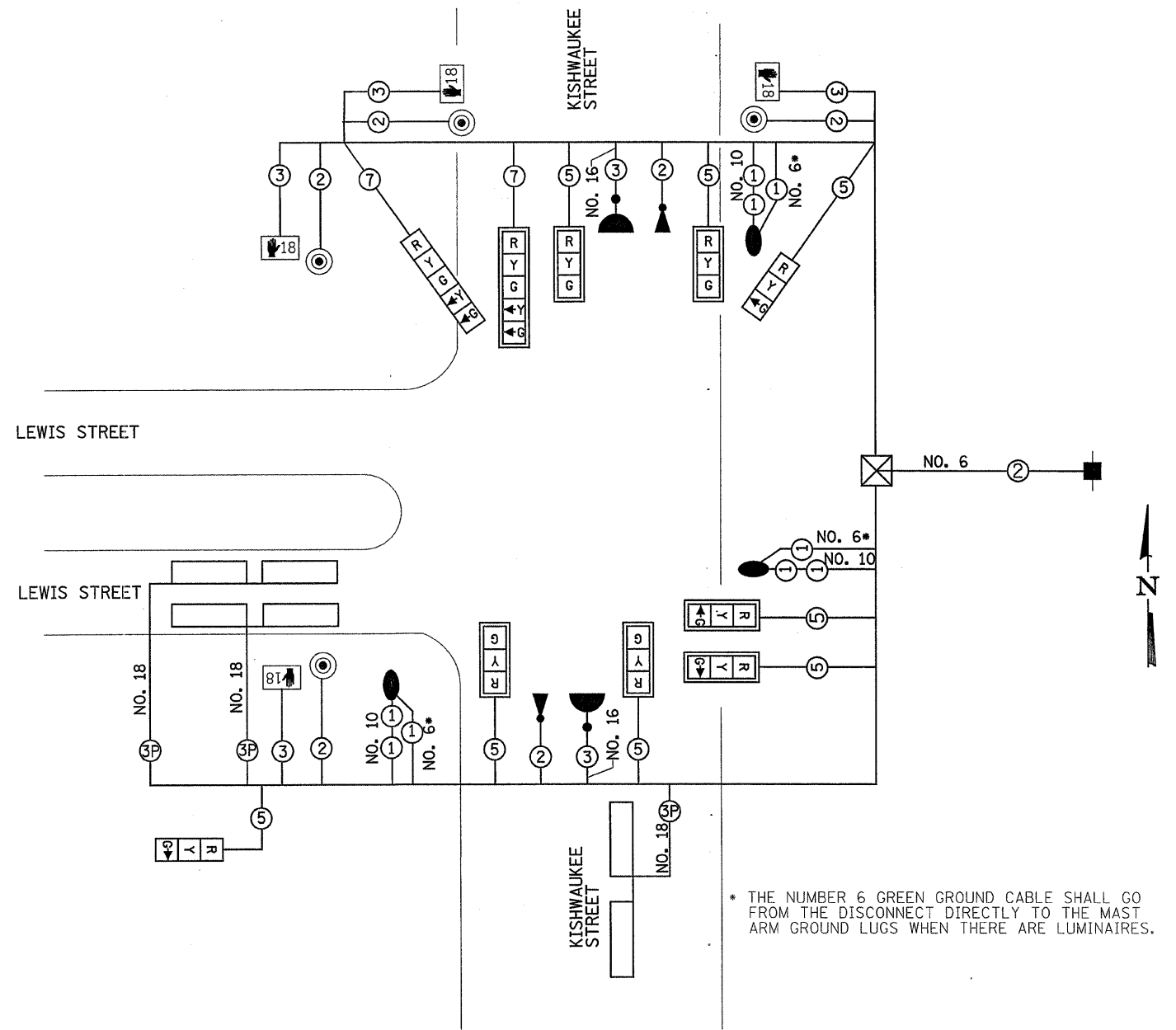
PAVEMENT MARKING, ROW, EASEMENT DETAILS & PAVEMENT ELEVATIONS

TABULATION OF QUANTITIES

| | | | |
|-----------|---------------------------------------------------------------------------|------|-------|
| 80500100* | SERVICE INSTALLATION, TYPE A | EACH | 1 |
| 81012600 | CONDUIT IN TRENCH, 2" DIA., PVC | FOOT | 253 |
| 81012700 | CONDUIT IN TRENCH, 2 1/2" DIA., PVC | FOOT | 26 |
| 81012800 | CONDUIT IN TRENCH, 3" DIA., PVC | FOOT | 8 |
| 81012900 | CONDUIT IN TRENCH, 3 1/2" DIA., PVC | FOOT | 273 |
| 81013000 | CONDUIT IN TRENCH, 4" DIA., PVC | FOOT | 10 |
| 81400100 | HANDHOLE | EACH | 2 |
| 81400300 | DOUBLE HANDHOLE | EACH | 1 |
| 81702110 | ELECTRIC CABLE IN CONDUIT, 600 V (XLP TYPE USE) 1/C NO.10 | FOOT | 694 |
| 81702130 | ELECTRIC CABLE IN CONDUIT, 600 V (XLP TYPE USE) 1/C NO.6 | FOOT | 347 |
| 81900200 | TRENCH AND BACKFILL FOR ELECTRICAL WORK | FOOT | 570 |
| 82103250 | LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, PHOTO CELL CONTROL, 250 WATT | EACH | 3 |
| 85700200* | FULL-ACTUATED CONTROLLER AND TYPE IV CABINET | EACH | 1 |
| 87301215 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C | FOOT | 633 |
| 87301225 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | FOOT | 670 |
| 87301245 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | FOOT | 1,065 |
| 87301255 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | FOOT | 349 |
| 87301515 | ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO.18 3 PAIR | FOOT | 657 |
| 87301805 | ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C | FOOT | 225 |
| 87502480 | TRAFFIC SIGNAL POST, GALVANIZED STEEL, 14 FT. | EACH | 1 |
| 87702840 | STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 22 FT. | EACH | 1 |
| 87702930 | STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 40 FT. | EACH | 1 |
| 87702960 | STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 46 FT. | EACH | 1 |
| 87800100 | CONCRETE FOUNDATION, TYPE A | FOOT | 3 |
| 87800200 | CONCRETE FOUNDATION, TYPE D | FOOT | 3.5 |
| 87800400 | CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER | FOOT | 10 |
| 87800415* | CONCRETE FOUNDATION, TYPE F 36-INCH DIAMETER | FOOT | 26 |
| 88040070 | SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3 SECTION, BRACKET MOUNTED | EACH | 2 |
| 88040090 | SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3 SECTION, MAST ARM MOUNTED | EACH | 6 |
| 88040150 | SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5 SECTION, BRACKET MOUNTED | EACH | 1 |
| 88040160 | SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5 SECTION, MAST ARM MOUNTED | EACH | 1 |
| 88102717 | PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER | EACH | 4 |
| 88200410 | TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC | EACH | 7 |
| 88500100 | INDUCTIVE LOOP DETECTOR | EACH | 3 |
| 88600100 | DETECTOR LOOP, TYPE 1 | FOOT | 411 |
| 88800100 | PEDESTRIAN PUSH BUTTON | EACH | 4 |
| * | ELECTRIC CABLE IN CONDUIT, EVP, NO. 14 2/C | FOOT | 335 |
| * | ELECTRIC CABLE IN CONDUIT, EVP, NO. 16 3/C | FOOT | 335 |
| XX004010* | OPTIC 4 CARD & RACK IN CONTROLLER | EACH | 1 |
| XX004011* | TOMAR EVP SIGNAL SENSOR | EACH | 2 |

CABLE PLAN

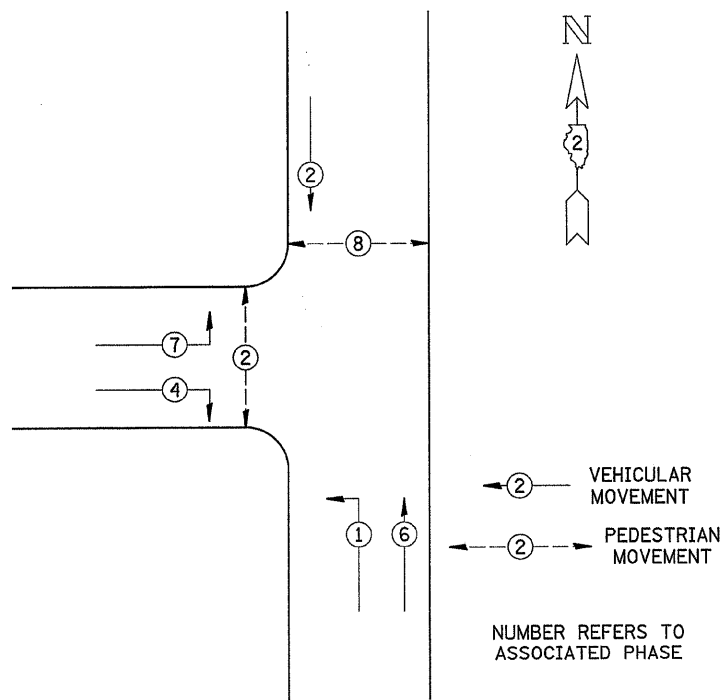
| | | | | |
|---------------------|----------------------------------|------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 26 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



* THE NUMBER 6 GREEN GROUND CABLE SHALL GO FROM THE DISCONNECT DIRECTLY TO THE MAST ARM GROUND LUGS WHEN THERE ARE LUMINAIRES.

PHASE DESIGNATION

PHASE DESIGNATION DIAGRAM



REFER TO STANDARD 857001

Lewis St

2 REQUIRED (FONT PER CITY STANDARD)

NOTE:

CITY WILL SUPPLY STREET SIGNS.
THE CONTRACTOR WILL BE RESPONSIBLE FOR THEIR INSTALLATION.

Kishwaukee St

1 REQUIRED (FONT PER CITY STANDARD)

LEGEND

- ② INDICATES NUMBER OF NEW CONDUCTORS. ALL CABLE IS NO. 14 EXCEPT AS INDICATED. "P" INDICATES PAIR CABLE.
- 18 PROPOSED PEDESTRIAN COUNTDOWN SIGNAL HEAD
- R Y G PROPOSED SIGNAL HEAD WITHOUT BACKPLATE
- R Y G PROPOSED SIGNAL HEAD WITH BACKPLATE
- ⊗ PROPOSED CONTROLLER CABINET
- PROPOSED LUMINAIRE, 250 WATT
- ▲ EMERGENCY VEHICLE LIGHT DETECTOR
- ◐ CONFIRMATION BEACON
- PROPOSED VEHICLE DETECTOR LOOP
- ⊙ PROPOSED PEDESTRIAN PUSH BUTTON
- PROPOSED POWER SOURCE

WILLET, HOFMANN & ASSOCIATES, INC.
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| | | | | |
|---------------------------|----------------------------------|---------------|--------------|------------------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| PAPT34 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 28 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ | | ILLINOIS | | FED. AID PROJECT |

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM SEWER WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE UNDER NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE CONSTRUCTION SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIME FRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN IN STANDARD 280001 OF THE PLANS.

SECTION 280, TEMPORARY EROSION CONTROL, OF THE STANDARD SPECIFICATIONS ADDITIONALLY SUPPLEMENTS THIS PLAN.

SITE DESCRIPTION

DESCRIPTION OF CONSTRUCTION ACTIVITY:

1. THE PROJECT CONSISTS OF TOTAL RECONSTRUCTION OF KISHWAUKEE STREET, REMOVAL OF SAND AVENUE AND ABANDONING AND REDIRECTING THE FLOW OF KEITH CREEK WITH THE INSTALLATION OF A CONCRETE BOX CULVERT.
2. CONSTRUCTION INCLUDES EARTH EXCAVATION (SPECIAL), DRIVEWAYS, PIPE CULVERTS, VARIOUS PAVEMENT ITEMS AND OTHER MISCELLANEOUS ITEMS OF CONSTRUCTION.

DESCRIPTION OF INTENDED SEQUENCE FOR MAJOR CONSTRUCTION ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE:

1. SEEDING CLASS 1, (SPECIAL) AND INSTALL PERIMETER EROSION BARRIER AS DIRECTED BY THE ENGINEER.
2. EARTH EXCAVATION (SPECIAL)
3. CULVERT, WATER MAIN, SANITARY SEWER AND STORM SEWER INSTALLATION
4. AGGREGATE BASE, CONCRETE SURFACE AND RELATED APPURTENANCES
5. PLACEMENT OF PERMANENT EROSION CONTROL IN DITCHES AND AROUND CULVERTS INCLUDING SEEDING, EROSION CONTROL MULCH, DITCH CHECKS, ETC.

AREA OF CONSTRUCTION SITE:

THE TOTAL AREA OF THE CONSTRUCTION IS ESTIMATED TO BE 9.98 ACRES OF WHICH 9.98 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING, AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM SEWER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLANS, DOCUMENTS, SPECIFICATIONS AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

1. DITCH OUTLETS AND CULVERT FLOWS INTO TRIBUTARY OF KEITH CREEK

CONTROLS - EROSION CONTROLS AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION


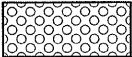



1. THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, PERIMETER EROSION BARRIER, AND OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
 - (a) AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.
 - (b) DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER, ALONG WITH REQUIRED TREE REMOVAL.
 - (c) AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, TEMPORARY DITCH CHECKS AND PERIMETER EROSION BARRIER SHALL BE INSTALLED AS CALLED OUT IN THIS PLAN AND DIRECTED BY THE ENGINEER.
 - (d) BARE AND SPARSELY VEGETATED GROUND IN HIGH ERODABLE AREAS AS DETERMINED BY THE ENGINEER SHALL BE TEMPORARILY SEEDED AT THE BEGINNING OF CONSTRUCTION WHERE NO CONSTRUCTION ACTIVITIES ARE EXPECTED WITHIN SEVEN DAYS.
 - (e) IMMEDIATELY AFTER TREE REMOVAL IS COMPLETED, AREAS WHICH ARE HIGHLY ERODABLE AS DETERMINED BY THE ENGINEER, SHALL BE TEMPORARILY SEEDED WHEN NO CONSTRUCTION ACTIVITIES ARE EXPECTED WITHIN SEVEN DAYS.
 - (f) AT LOCATIONS WHERE A SIGNIFICANT AMOUNT OF WATER DRAINS INTO THE CONSTRUCTION ZONE FROM OUTSIDE AREAS (ADJACENT LANDOWNERS), TEMPORARY DITCH CHECKS WILL BE UTILIZED TO LOCALLY DIVERT WATER, REDUCE FLOW RATES, AND COLLECT OUTSIDE SILTATION INSIDE THAT RIGHT-OF-WAY LINE.
2. ESTABLISHMENT OF THESE TEMPORARY EROSION CONTROL MEASURES WILL HAVE ADDITIONAL BENEFITS TO THE PROJECT. DESIRABLE GRASS SEED WILL BECOME ESTABLISHED IN THESE AREAS AND WILL SPREAD SEEDS ONTO THE CONSTRUCTION SITE UNTIL PERMANENT SEEDING/MOWING AND OVERSEEDING CAN BE COMPLETED.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

I CERTIFY UNDER PENALTY OF LAW THAT THIS DOCUMENT AND ALL ATTACHMENTS WERE PREPARED UNDER MY DIRECTION OR SUPERVISION IN ACCORDANCE WITH A SYSTEM DESIGNED TO ASSURE THAT QUALIFIED PERSONNEL PROPERLY GATHERED AND EVALUATED THE INFORMATION SUBMITTED. BASED ON MY INQUIRY OF THE PERSON OR PERSONS WHO MANAGE THE SYSTEM, OR THOSE PERSONS DIRECTLY RESPONSIBLE FOR GATHERING THE INFORMATION, THE INFORMATION SUBMITTED IS, TO THE BEST OF MY KNOWLEDGE AND BELIEF, TRUE, ACCURATE AND COMPLETE. I AM AWARE THAT THERE ARE SIGNIFICANT PENALTIES FOR SUBMITTING FALSE INFORMATION, INCLUDING THE POSSIBILITY OF FINE AND IMPRISONMENT FOR KNOWING VIOLATIONS.

Wendy K. Williams, P.E. 4/16/08
PROJECT ENGINEER DATE

LEGEND

-  TEMPORARY DITCH CHECK
-  EROSION CONTROL BLANKET
-  PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
-  EXISTING DRAINAGE FLOW
-  PROPOSED DRAINAGE FLOW

NOTE: ALL ITEMS SHALL BE CONSTRUCTED AS SHOWN ON STANDARD 280001 AND AS DIRECTED BY THE ENGINEER. MAINTENANCE AND CLEANING OF THE EROSION CONTROL ITEMS SHALL BE INCLUDED IN THE RESPECTIVE EROSION CONTROL PAY ITEM.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

1. DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.
 - (a) WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
 - (b) EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
 - (c) AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
 - I. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
 - II. TEMPORARILY SEED ERODABLE BARE EARTH ON A WEEKLY BASIS TO MINIMIZE THE AMOUNT OF ERODABLE SURFACE AREA WITHIN THE CONTRACT LIMITS.
 - III. CONSTRUCT ROADSIDE DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.
 - IV. TEMPORARILY DIVERT WATER AROUND PROPOSED CULVERT LOCATIONS.
 - V. BUILD NECESSARY EMBANKMENT AT CULVERT LOCATIONS AND THEN EXCAVATE AND PLACE CULVERT.
 - VI. CONTINUE BUILDING UP THE EMBANKMENT TO THE PROPOSED GRADE WHILE AT THE SAME TIME, PLACING PERMANENT EROSION CONTROL SUCH AS RIPRAP DITCH LINING AND CONDUCTING FINAL SHAPING TO THE SLOPES.
 - (d) EXCAVATED AREAS AND EMBANKMENT SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
 - (e) CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR OTHER POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
 - (f) THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2 INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING THE WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE CONSTRUCTION FIELD ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT EROSION CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.
 - (g) SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON THE SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR EARTH EXCAVATION (SPECIAL).
 - (h) THE TEMPORARY EROSION CONTROL SYSTEM SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

1. TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS SEEDED AND ESTABLISHED.
2. ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

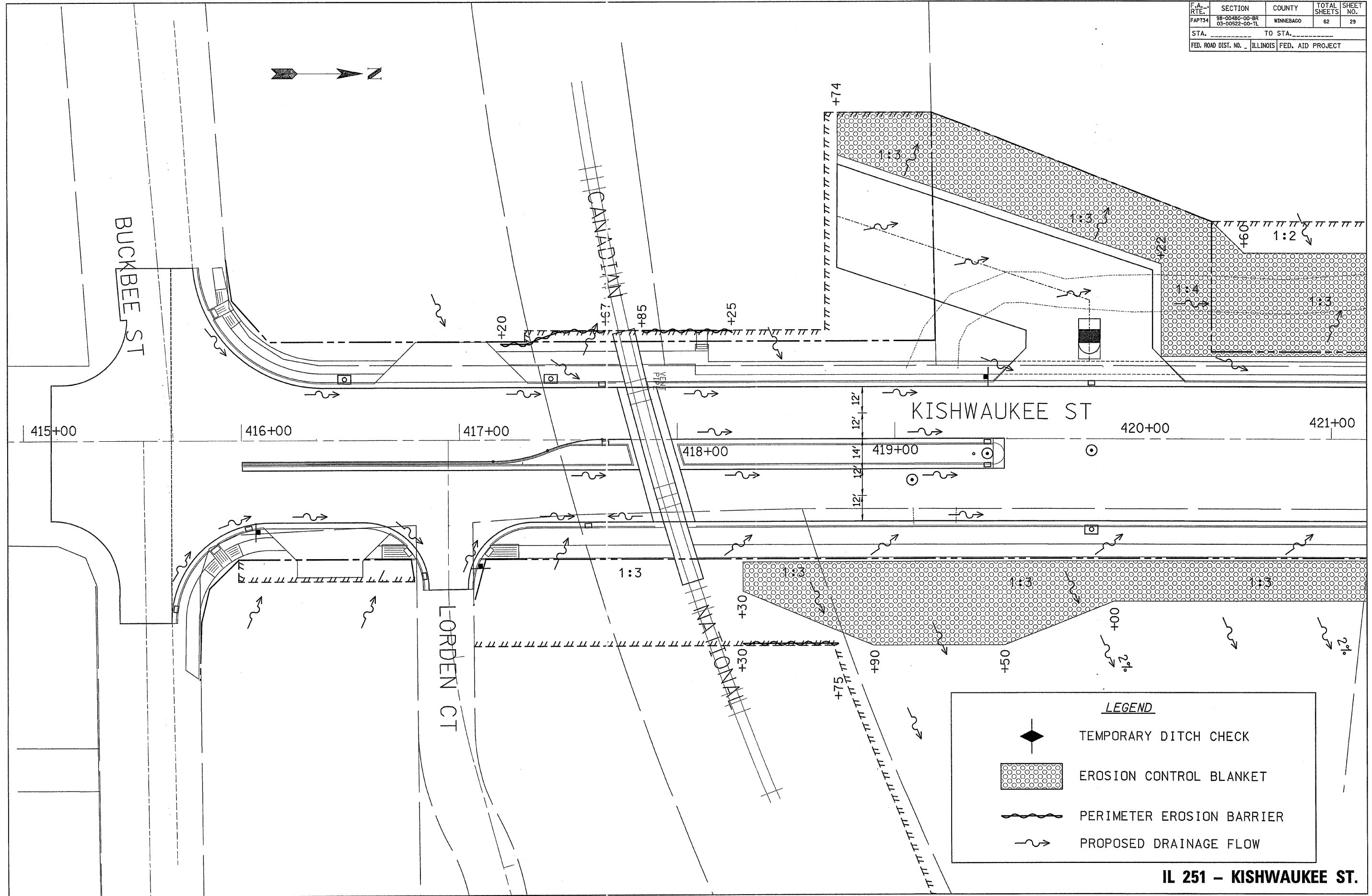
MAINTENANCE AFTER CONSTRUCTION:

1. CONSTRUCTION IS COMPLETE AFTER ACCEPTANCE BY I.D.O.T. FINAL INSPECTION. MAINTENANCE UP TO THIS DATE WILL BE BY THE CONTRACTOR.

MISCELLANEOUS:

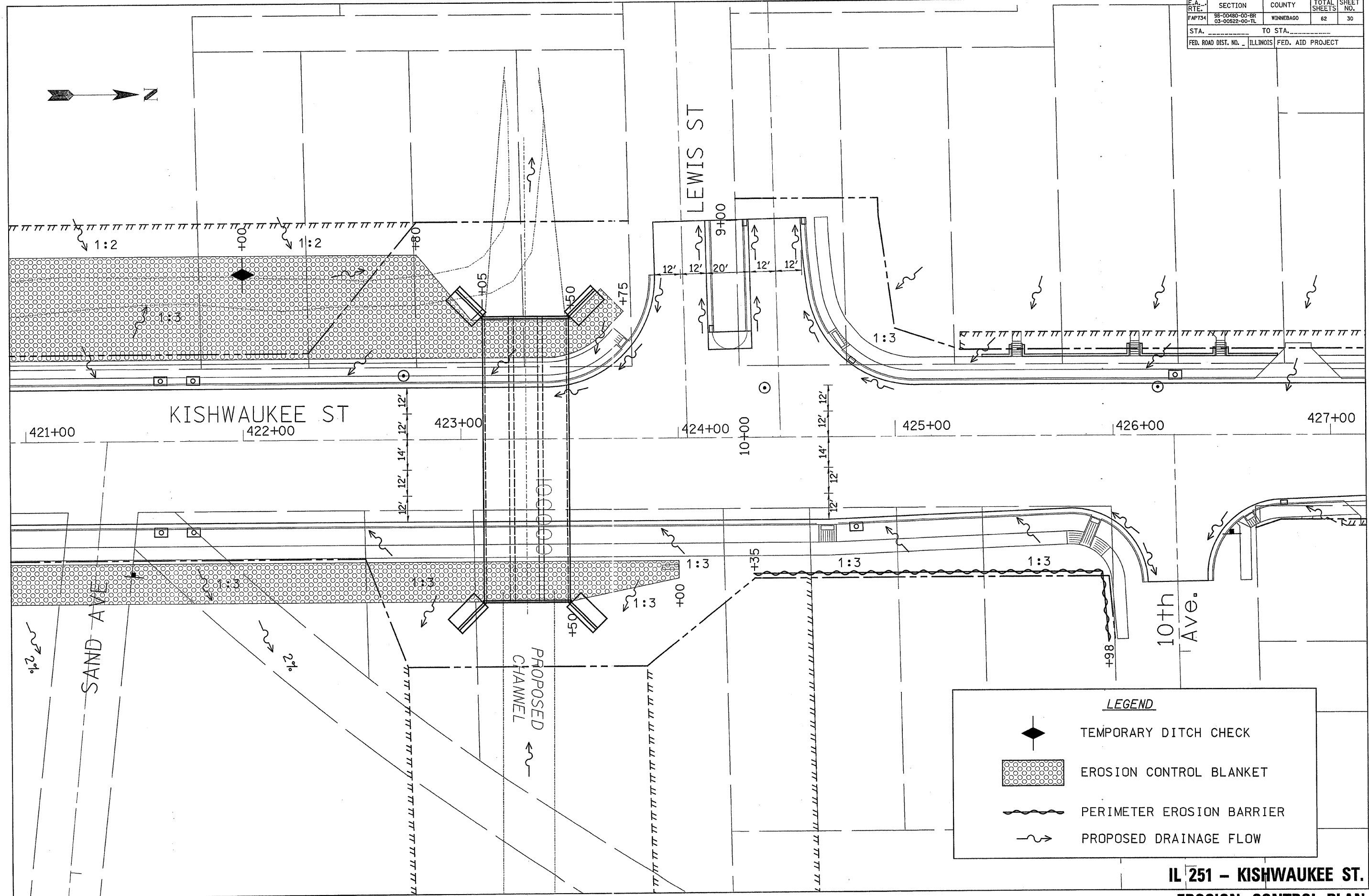
1. TEMPORARY DITCH CHECKS SHALL BE LOCATED AT EVERY 15 FT. FALL/RISE IN DITCH GRADE
2. TEMPORARY EROSION CONTROL SEEDING SHALL BE APPLIED AT A RATE OF 100 LBS/ACRES.
3. STRAW BALES, HAY BALES, PERIMETER EROSION BARRIER AND SILT FENCES WILL BE PERMITTED FOR TEMPORARY OR PERMANENT DITCH CHECKS. DITCH CHECKS MAY BE COMPOSED OF AGGREGATE, SILT PANELS, ROLLED EXCELSTOR, URETHANE FOAM/GEOTEXTILE (SILT SEDGES), AND/OR ANY OTHER MATERIAL APPROVED BY THE EROSION AND SEDIMENT CONTROL COORDINATOR.
4. ALL EROSION CONTROL PRODUCTS FURNISHED SHALL BE SPECIFICALLY RECOMMENDED BY THE MANUFACTURER FOR THE USE SPECIFIED IN THE EROSION CONTROL PLAN. PRIOR TO THE APPROVAL AND USE OF THE PRODUCT, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A NOTARIZED CERTIFICATION BY THE PRODUCER STATING THE INTENDED USE OF THE PRODUCT AND THAT THE PHYSICAL PROPERTIES REQUIRED FOR THIS APPLICATION ARE MET OR EXCEEDED. THE CONTRACTOR SHALL PROVIDE MANUFACTURER INSTALLATION PROCEDURES TO FACILITATE THE ENGINEER IN CONSTRUCTION INSPECTION.

| | | | | |
|---------------------|----------------|------------------|--------|-------|
| F.A.: | SECTION | COUNTY | TOTAL | SHEET |
| RTE.: | 98-00480-00-BR | WINNEBAGO | SHEETS | NO. |
| FAPT34 | 03-00522-00-TL | | 62 | 29 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



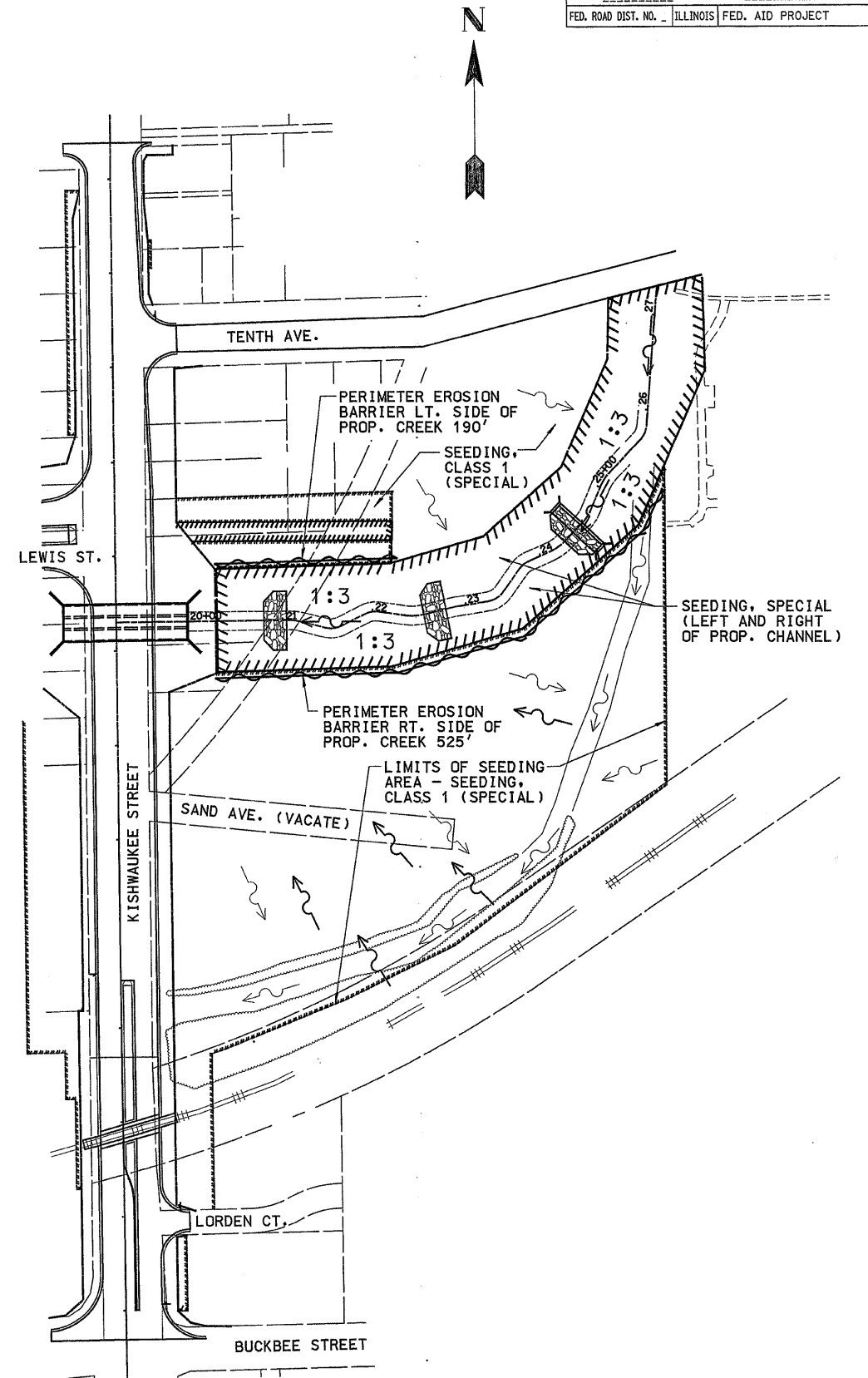
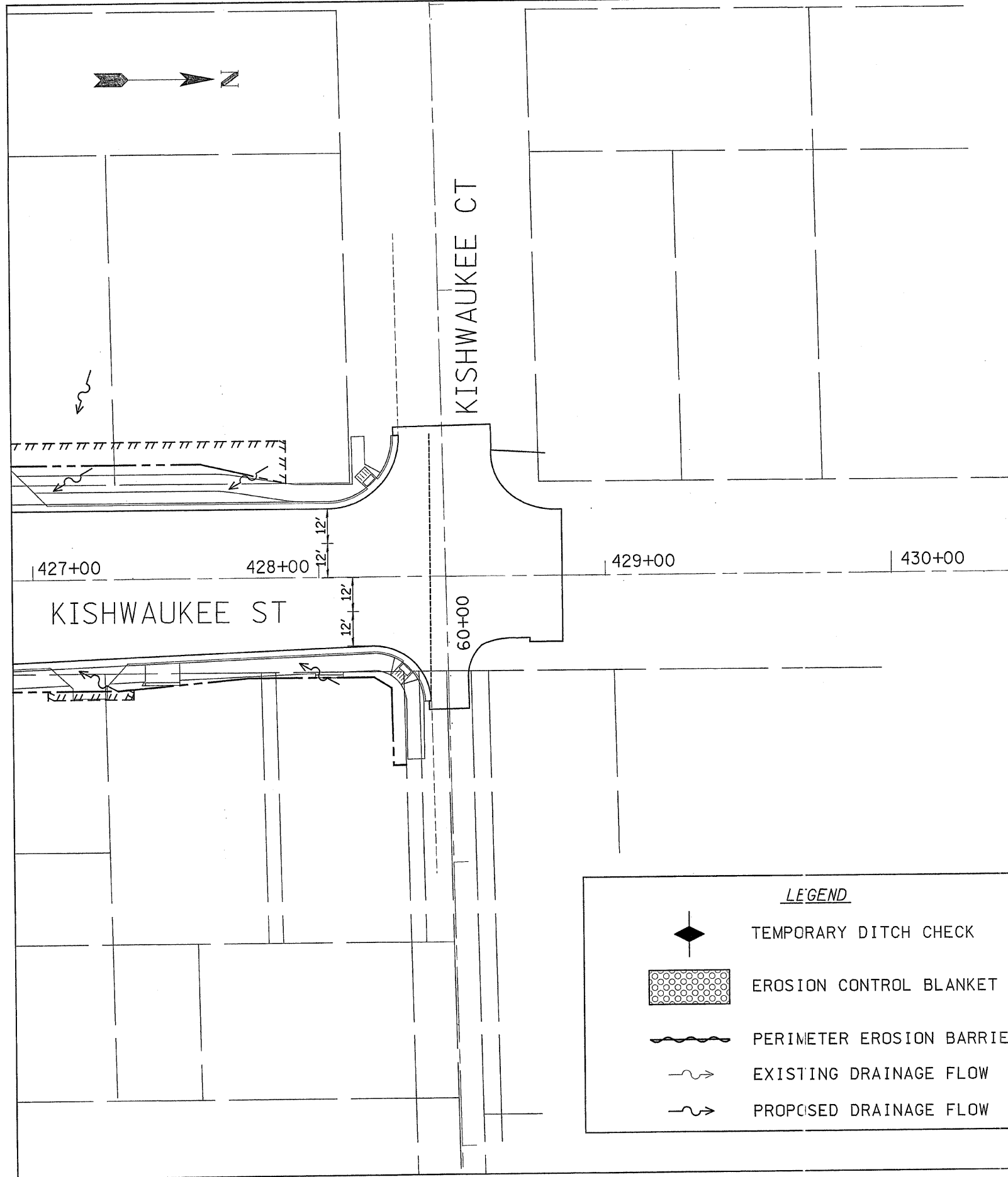
IL 251 - KISHWAUKEE ST.
EROSION CONTROL PLAN

| | | | | |
|---------------------------|----------------------------------|---------------------------|--------------|-----------|
| E.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAPT34 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 30 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ | | ILLINOIS FED. AID PROJECT | | |



**IL 251 - KISHWAUKEE ST.
EROSION CONTROL PLAN**

| | | | | |
|---------------------|----------------------------------|-----------|------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAPT34 | 98-00480-03-BR 03-00522-00-TL | WINNEBAGO | 62 | 31 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

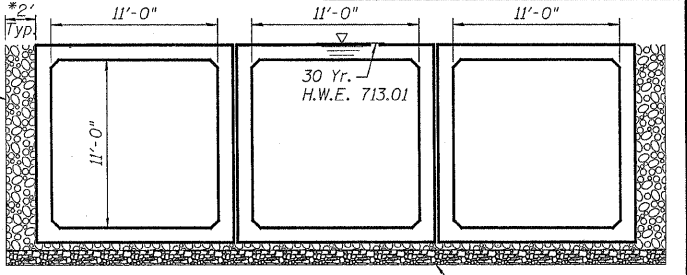
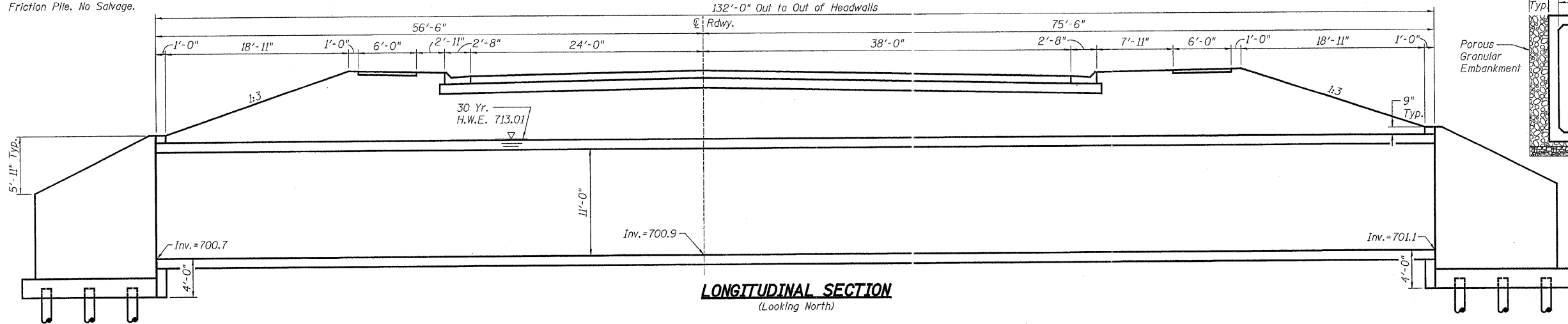


PARK AREA - EROSION CONTROL

**IL 251 - KISHWAUKEE ST.
EROSION CONTROL PLAN**

Existing Structure S.N. 101-6041
A Single Span (1045'-0") Reinforced
Concrete Filled Spandrel Arch on
Reinforced Concrete Pile Caps and
Friction Pile. No Salvage.

| | | | | |
|---------------------|----------------|---------------------------|--------------|-----------|
| STATE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| IL | 98-00480-00-BR | WINNEBAGO | 62 | 32 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



SECTION THRU BARREL
Looking East

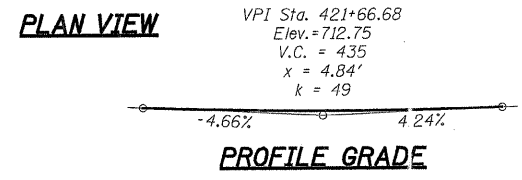
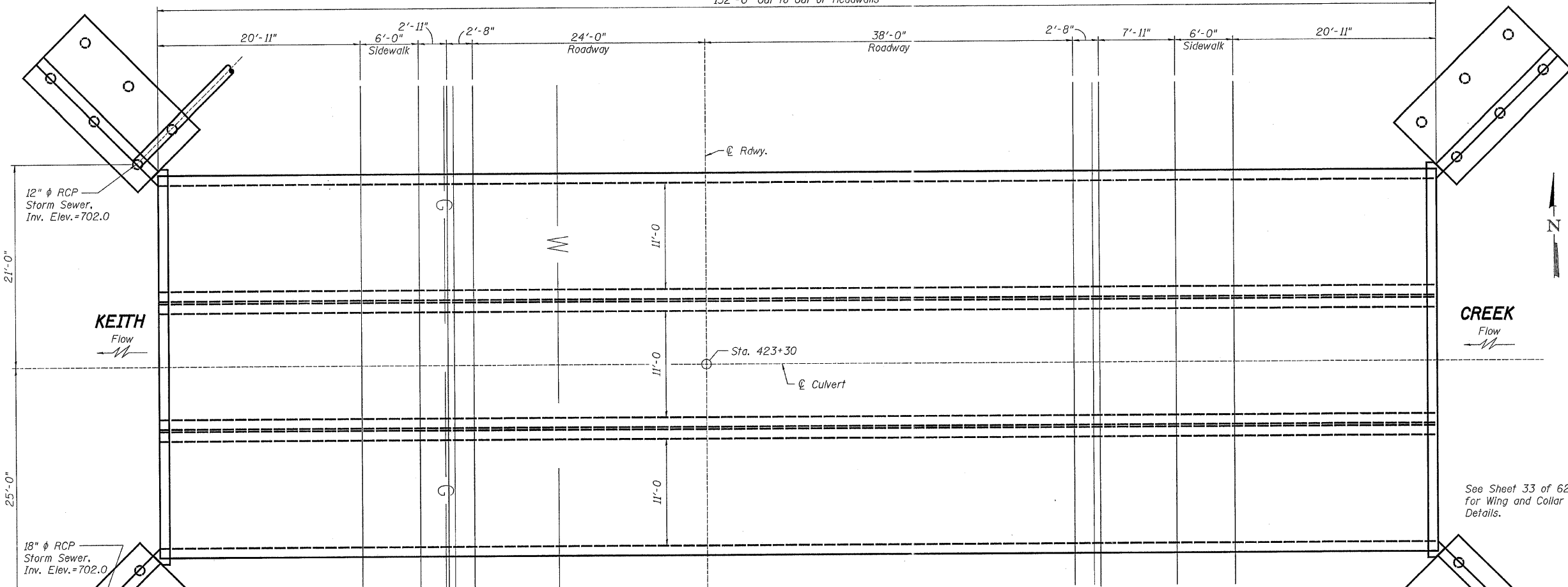
*Pay Limits of Porous Granular Embankment

BILL OF MATERIAL - CULVERT

| ITEM | UNIT | SUB | SUPER | TOTAL |
|-------------------------------------------|---------|--------|-------|--------|
| Porous Granular Embankment | Cu. Yd. | 255 | --- | 255 |
| Removal Of Existing Structures | Each | --- | --- | 1 |
| Concrete Structures | Cu. Yd. | 91.6 | --- | 91.6 |
| Reinforcement Bars | Pound | 10,730 | --- | 10,730 |
| Furnishing Metal Shell Piles 12" X 0.179" | Foot | 825 | --- | 825 |
| Driving Piles | Foot | 825 | --- | 825 |
| Test Pile Metal Shells | Each | 2 | --- | 2 |
| Name Plates | Each | 1 | --- | 1 |
| Breaker Run Crushed Stone | Ton | 410 | --- | 410 |
| Precast Conc. Box Culvert 11'x11' | Foot | 396 | --- | 396 |
| Form Liner Textured Surface | Sq. Yd. | 47 | --- | 47 |
| Anti-Graffiti Coating | Sq. Ft. | 424 | --- | 424 |

GENERAL NOTES

- Precast concrete box culvert sections shall conform to the requirements of article 540.06 of the standard specifications and the applicable requirements of AASHTO M259.
- Reinforcement bars shall conform to the requirements of AASHTO M31, M42, or M53 Grade 60.
- Lifting holes shall be filled with concrete plugs and mastic after box sections are in place.
- ** Cost of porous granular material, other than breaker run crushed stone under culvert, shall be paid for as specified in section 540 of the standard specifications.
- The contractor shall drive a 12" ϕ metal shell test pile in a permanent location at the northeast and southwest wingwalls as directed by the engineer prior to ordering the remainder of the piles.
- Precaster shall provide bar splicers and shear reinforcement in the upstream and downstream sections as shown in Section B-B Sheet 33 of 62. Cost for these items shall be included in the Contract Unit per FOOT for PRECAST CONCRETE BOX CULVERT 11' X 11'
- All Exposed Cast in Place Edges shall have 3/4" Chamfers, Except as Noted.



KEITH CREEK
BUILT 2008 BY
CITY OF ROCKFORD
SECTION 98-00480-00-BR
F.A.P. RT. 734
STR. NO. 101-6160 LOADING HS20

NAME PLATE LETTERING
REFER TO STD. 515001

DESIGN SPECIFICATIONS

Design In Accordance With AASHTO Specifications Dated 2002

SEISMIC DATA

Seismic Performance Category (SPC) = A
Bedrock Acceleration Coefficient (A) = 0.031
Site Coefficient (S) = 1.2

DESIGN STRESSES

f'c = 5,000 PSI (Conc.)
Fy = 60,000 PSI (Reinf.)

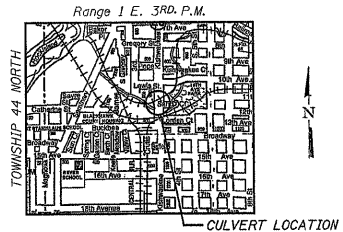
LOADING HS20-44

Allow 50#/Sq. Ft. For Future Wearing Surface.



DATE: 4/17/08
EXPIRES 11/30/08

"I CERTIFY THAT TO THE BEST OF MY KNOWLEDGE, INFORMATION AND BELIEF, THIS BRIDGE DESIGN IS STRUCTURALLY ADEQUATE FOR THE DESIGN LOADING SHOWN ON THE PLANS. THE DESIGN IS AN ECONOMICAL ONE FOR THE STYLE OF STRUCTURE AND COMPLIES WITH REQUIREMENTS OF THE CURRENT AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES."



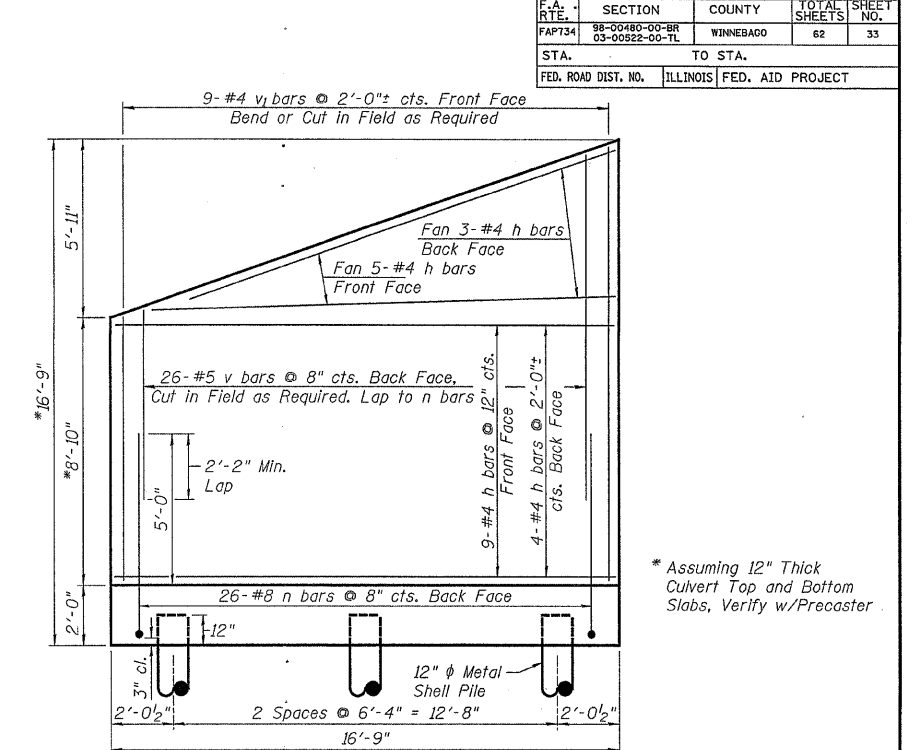
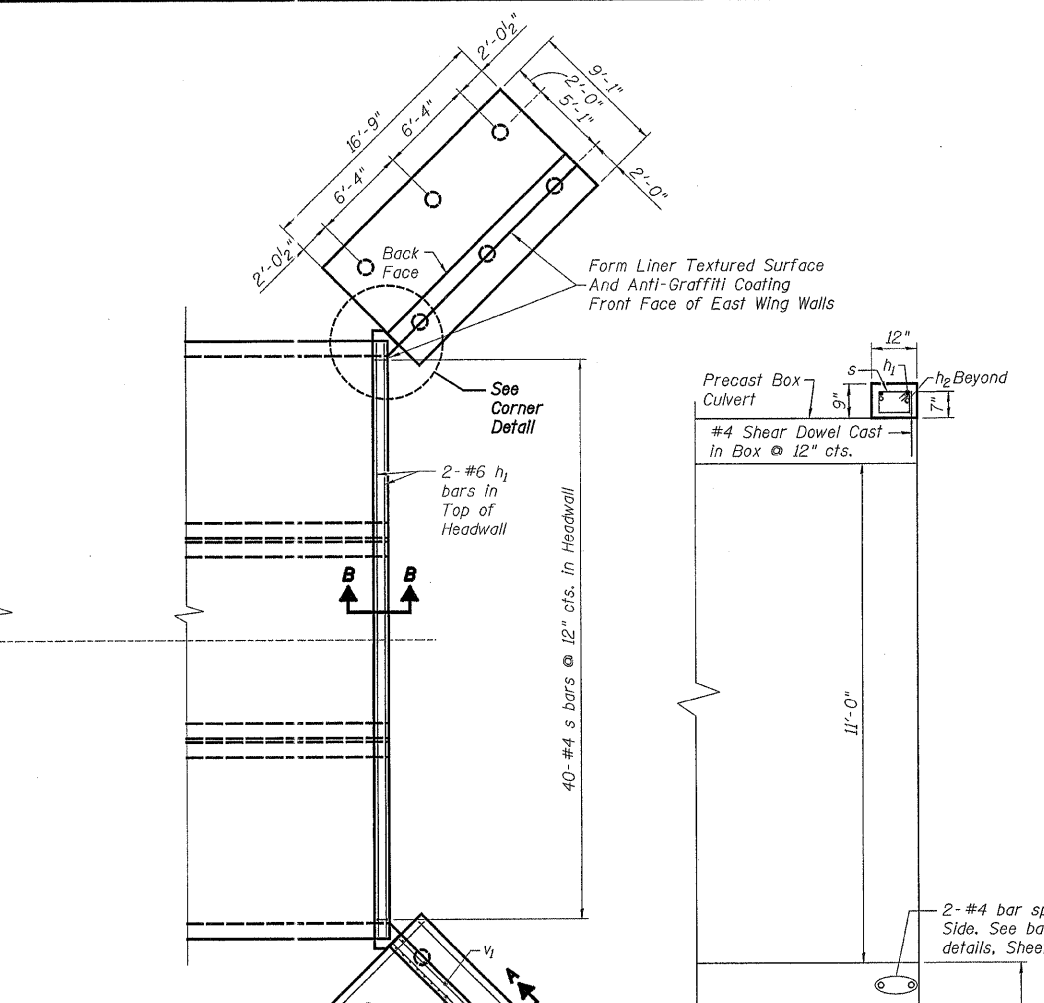
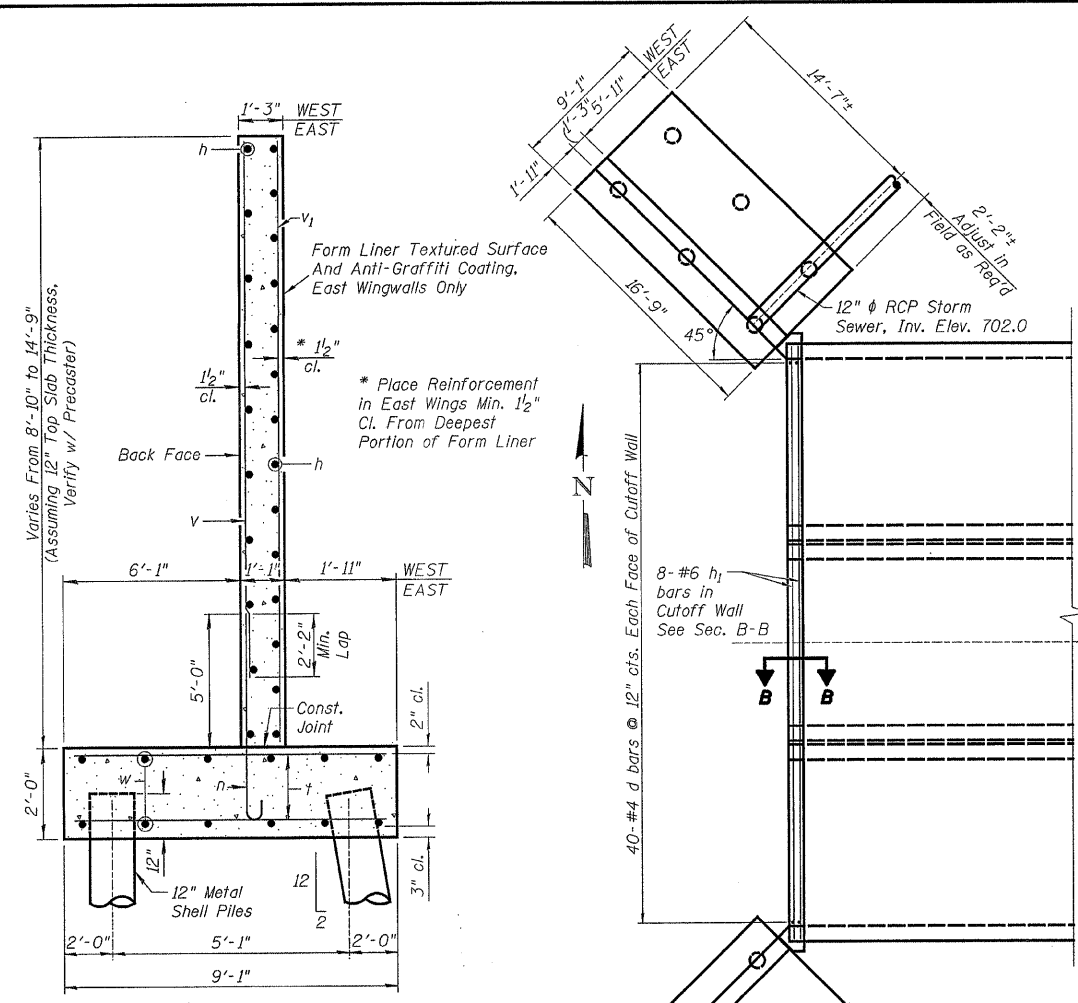
LOCATION SKETCH

**GENERAL PLAN AND ELEVATION
KISHWAUKEE STREET OVER KEITH CREEK
STA. 423+30 (S.N. 101-6160)
SECTION 98-00480-00-BR
ROCKFORD, ILLINOIS**

WILLET, HOFMANN & ASSOCIATES, INC.
CONSULTING ENGINEERS
Land Surveying - Transportation - Structural
Environmental - Architecture
809 East Second Street Dixon, Illinois 61021
Phone 815.284.3381 Fax 815.284.3385
Design Firm #194-00018
www.willett-hofmann.com
WHA # 1158003

DESIGNED BY:
M. R. LESLIE
DATE: 2/06
CHECKED BY:
B. K. OVERSE
DATE: 3/06
DRAWN BY:
R. D. ALLEN
DATE: 2/06

| | | | | |
|---------------------|----------------------------------|------------------|--------------|-----------|
| F.A. RT# | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| PAP134 | 98-00480-00-BR 03-00222-00-TL | WINNEBAGO | 62 | 33 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

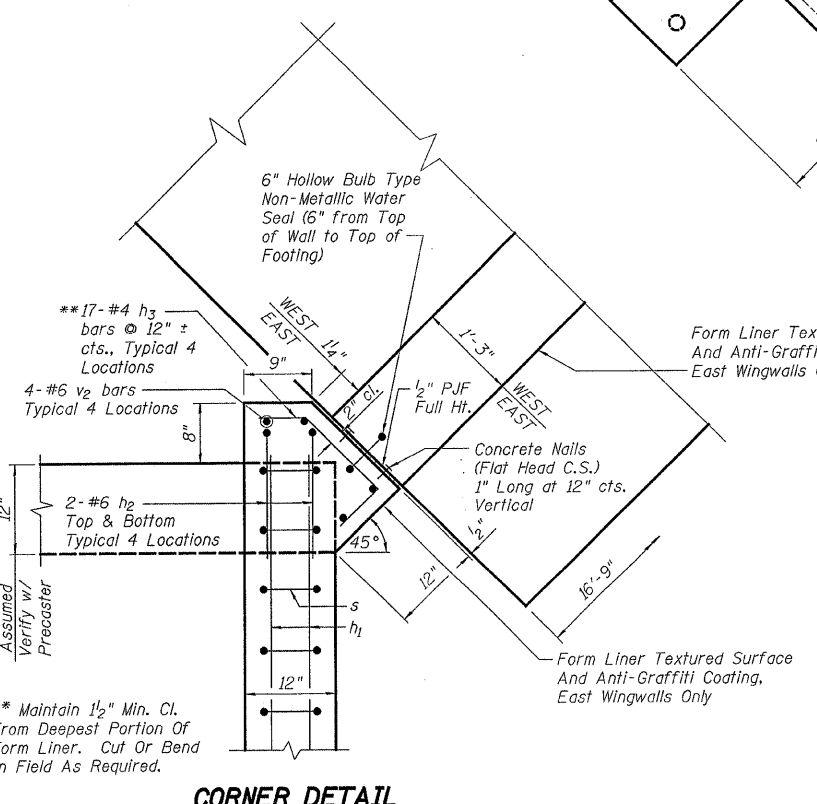


Notes:
The NW and SW Wings require Penetrations for Storm Sewer. See Sheet 32 of 62 for Location and Invert.

BILL OF MATERIAL-WINGWALLS AND COLLARS

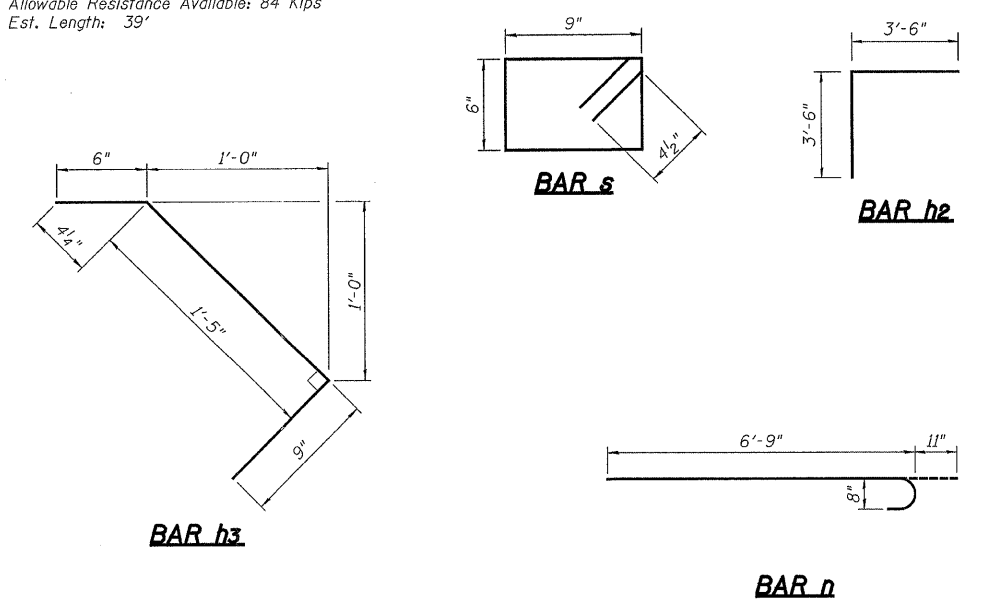
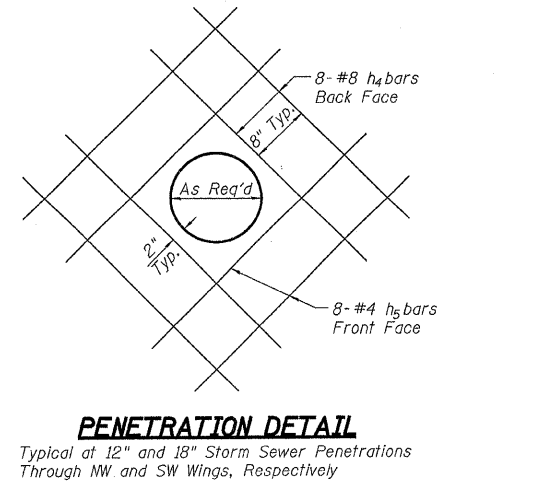
| Bar | No. | Size | Length | Shape |
|------------------------------|-----|------|---------|--------|
| d | 160 | #4 | 2'-6" | — |
| h | 84 | #4 | 16'-6" | — |
| h ₁ | 20 | #6 | 39'-3" | — |
| h ₂ | 16 | #6 | 7'-0" | — |
| h ₃ | 68 | #4 | 2'-8" | — |
| h ₄ | 16 | #8 | 5'-6" | — |
| h ₅ | 16 | #4 | 5'-6" | — |
| n | 104 | #8 | 7'-8" | — |
| s | 80 | #4 | 3'-3" | — |
| t | 208 | #6 | 8'-9" | — |
| v | 104 | #5 | 12'-0" | — |
| v ₁ | 36 | #4 | 14'-6" | — |
| v ₂ | 16 | #6 | 16'-6" | — |
| w | 40 | #5 | 16'-6" | — |
| Concrete Structures | | | Cu. Yd. | 91.6 |
| Reinforcement Bars | | | Pound | 10,730 |
| Furnishing Metal Shell Piles | | | Foot | 825 |
| 12" X 0.179" | | | Foot | 825 |
| Driving Piles | | | Foot | 2 |
| Test Pile Metal Shells | | | Sq. Yd. | 47 |
| Form Liner Textured Surface | | | Sq. Ft. | 424 |
| Anti-Graffiti Coating | | | Sq. Ft. | 424 |

Notes:
Class SI concrete shall be used throughout.
Exposed edges shall be beveled 3/4".
For backfill & embankment, See Special Provisions.
Reinforcement bars shall conform to the requirements of AASHTO M-31, M-42, or M-53 Grade 60.
For Bar Splicer Details See Structural Sheet 34 of 62.
All Excavation Required for Wingwall Construction shall be Included in the Contract Unit Price per Foot for PRECAST CONCRETE BOX CULVERT 11'x11'.



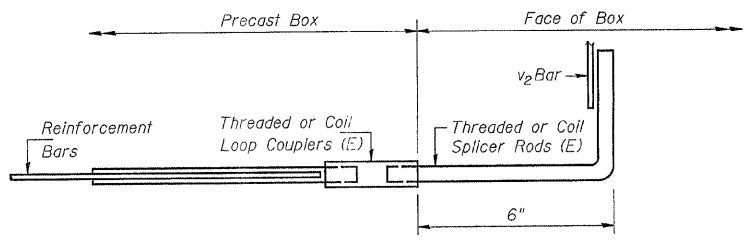
PILE DATA-WEST WINGWALLS
Type & Size: Metal Shell - 12 Inch Dia. X 0.179 Inch Walls
Nominal Required Bearing: 252 Kips
Allowable Resistance Available: 84 Kips
Est. Length: 39'

PILE DATA-EAST WINGWALLS
Type & Size: Metal Shell - 12 Inch Dia. X 0.179 Inch Walls
Nominal Required Bearing: 252 Kips
Allowable Resistance Available: 84 Kips
Est. Length: 39'



WINGWALL AND COLLAR DETAILS
KISHWAUKEE STREET OVER KEITH CREEK
STA. 423+30 (S.N. 101-6160)
SECTION 98-00480-00-BR
ROCKFORD, ILLINOIS
WHA # 1158D03

| | | | | |
|-----------------------------------------------------|----------------------------------|---------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 34 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT | | | | |

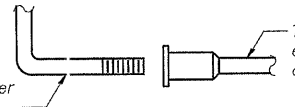


SPLICER DETAIL

PRECAST BOX CULVERTS

| Bar Size | No. Assemblies Required | Location |
|----------|-------------------------|-----------------------|
| #4 | 8 | Bottom of Bottom Slab |

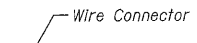
The diameter of this part is the same as the diameter of the bar spliced.



ROLLED THREAD DOWEL BAR



ONE PIECE

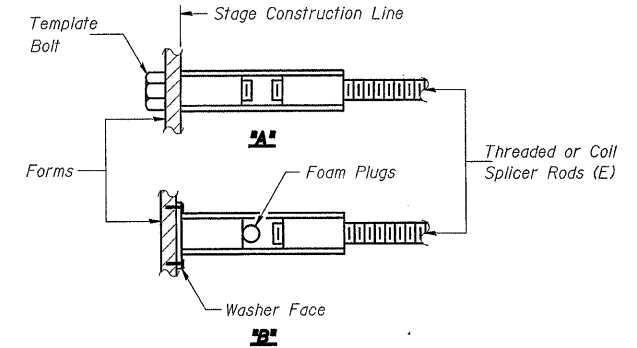


WIRE CONNECTOR

WELDED SECTIONS

BAR SPLICER ASSEMBLY ALTERNATIVES

** Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.

NOTES

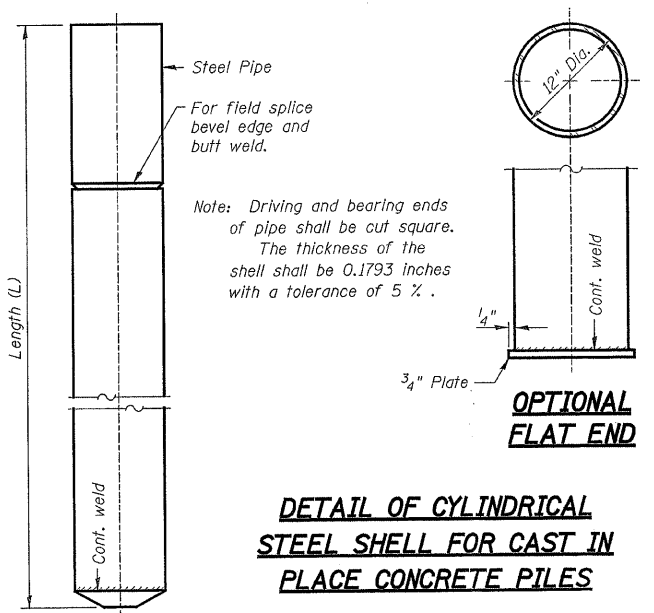
Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- Minimum Capacity (Tension in kips) = $1.25 \times f_y \times A_t$
- Minimum *Pull-out Strength (Tension in kips) = $1.25 \times f_{sallow} \times A_t$

Where f_y = Yield strength of lapped reinforcement bars in ksi.
 f_{sallow} = Allowable tensile stress in lapped reinforcement bars in ksi (Service Load)
 A_t = Tensile stress area of lapped reinforcement bars.
 * = 28 day concrete

| Bar Size to be Spliced | Splicer Rod or Dowel Bar Length | Strength Requirements | |
|------------------------|---------------------------------|------------------------------|---------------------------------------|
| | | Min. Capacity kips - tension | Min. Pull-Out Strength kips - tension |
| #4 | 1'-6" | 14.7 | 5.9 |
| #5 | 2'-0" | 23.0 | 9.2 |
| #6 | 2'-7" | 33.1 | 13.3 |
| #7 | 3'-5" | 45.1 | 18.0 |
| #8 | 4'-6" | 58.9 | 23.6 |

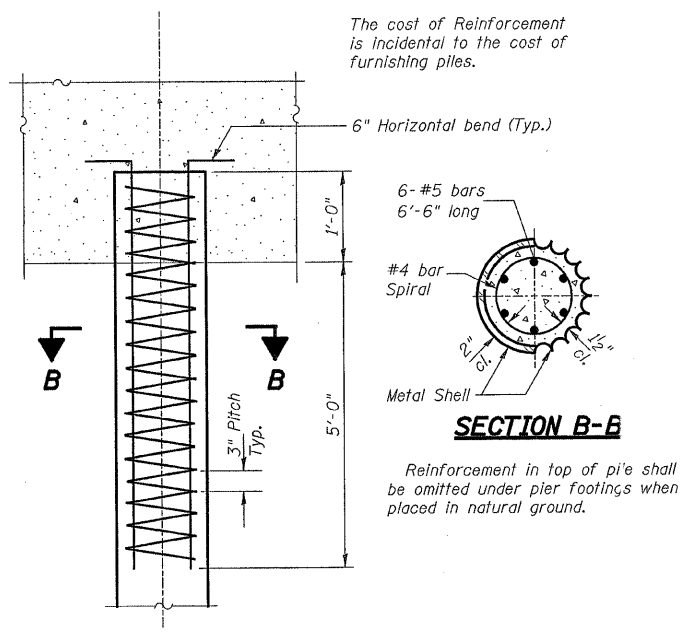
Bar splicer assemblies shall be according to Section 508 of the Standard Specifications, except as noted. The costs for furnishing and installation of bar splicer assemblies shall be included in the contract unit price per FOOT for PRECAST CONCRETE BOX CULVERT 11' X 11'



Note: Driving and bearing ends of pipe shall be cut square. The thickness of the shell shall be 0.1793 inches with a tolerance of 5%.

OPTIONAL FLAT END

DETAIL OF CYLINDRICAL STEEL SHELL FOR CAST IN PLACE CONCRETE PILES



The cost of Reinforcement is incidental to the cost of furnishing piles.

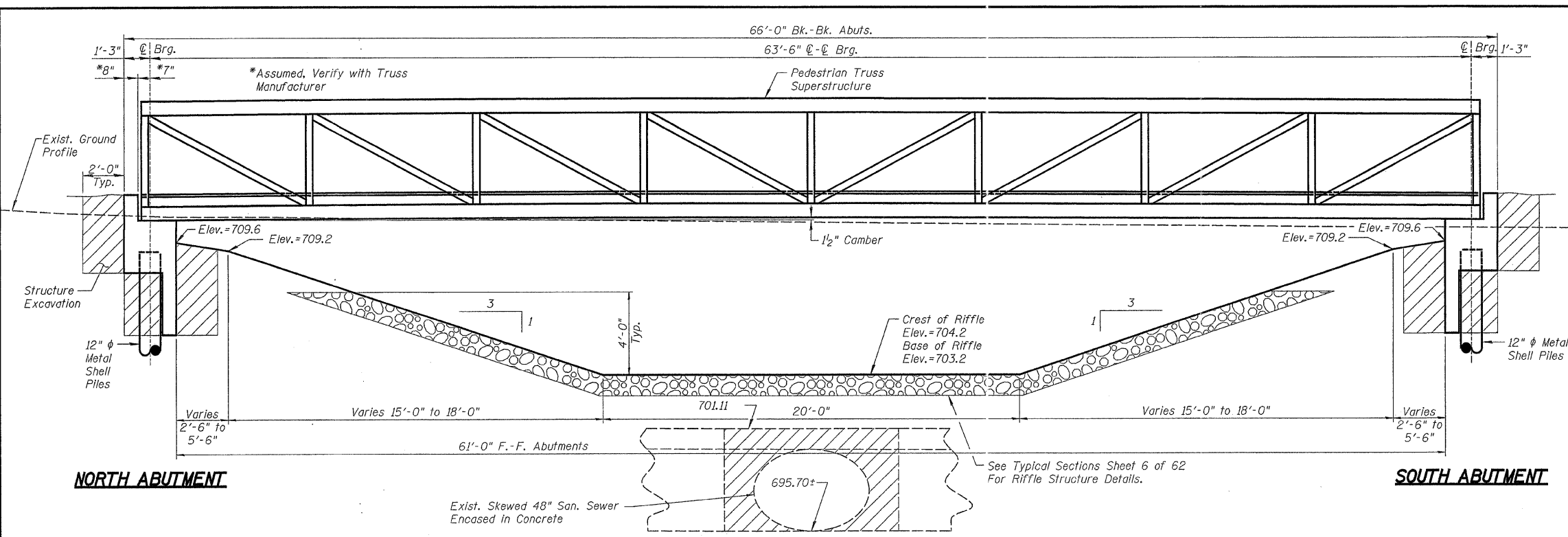
SECTION B-B

Reinforcement in top of pile shall be omitted under pier footings when placed in natural ground.

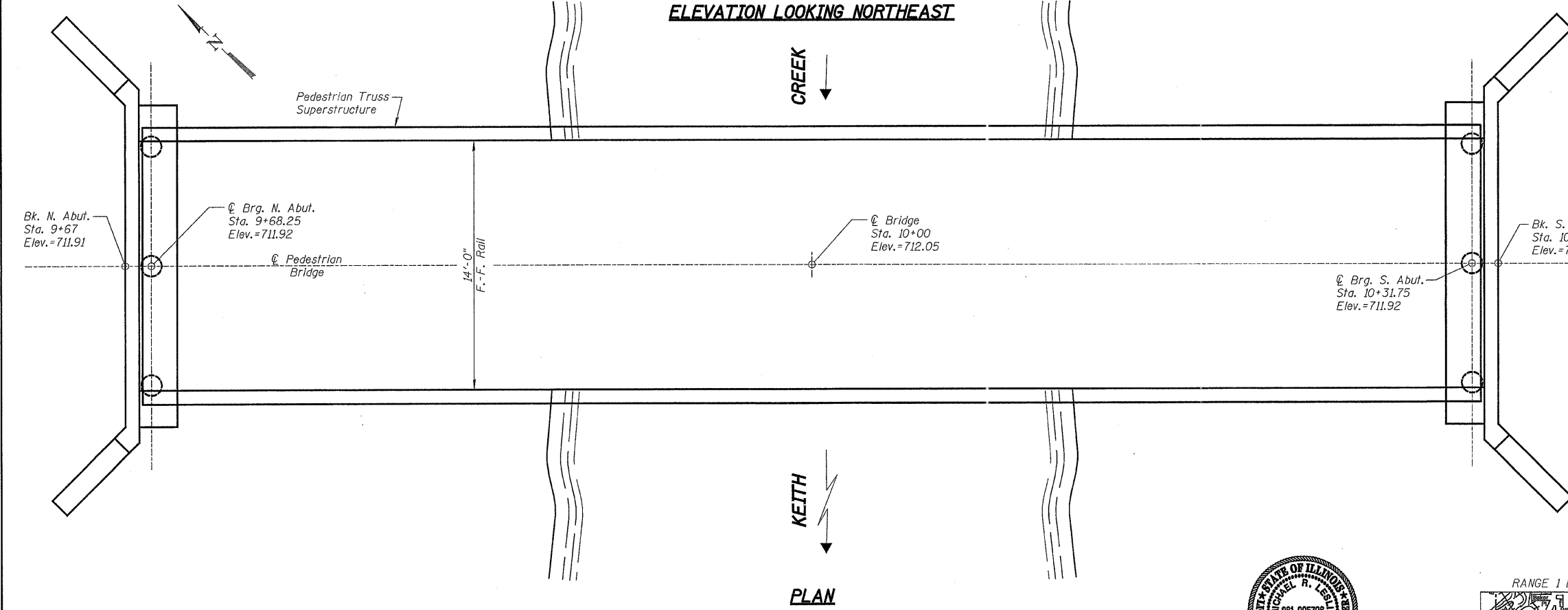
DETAIL OF REINFORCEMENT FOR METAL SHELLS

BAR SPLICER ASSEMBLY DETAILS & PILE DETAILS
KISHWAUKEE STREET OVER KEITH CREEK
STATION 423+30 (S.N. 101-6160)
SECTION 98-00480-00-BR
ROCKFORD, ILLINOIS
 WHA # 1158D03

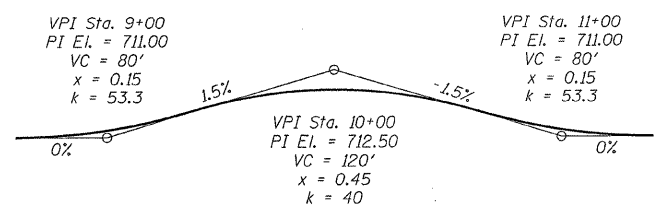
| | | | | |
|---------------------|----------------------------------|------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP134 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 35 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



ELEVATION LOOKING NORTHEAST



PLAN



PROFILE GRADE

DESIGN STRESSES

FIELD UNITS
 $f'_c = 3500$ psi
 $f_y = 60000$ psi

DESIGN SPECIFICATIONS

Design in Accordance with AASHTO Guide for Development of Bicycle Facilities, 1999 and AASHTO Guide Specifications for Design of Pedestrian Bridges, 1997

LOADING

85 psf Pedestrian & 20,000 lb. Emergency Vehicle

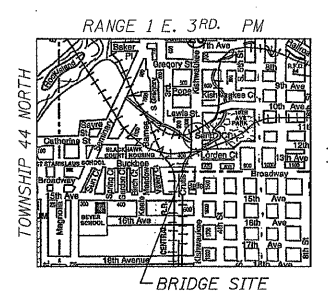
ASSUMED TRUSS LOADS

Vertical LL = 38.7 k/Abut.
 Vertical DL = 22.0 k/Abut.
 Horizontal WL = 8.0 k/Abut.



Michael R. Leslie
 DATE: 4/17/08
 EXPIRES 11/30/08

"I CERTIFY THAT TO THE BEST OF MY KNOWLEDGE, INFORMATION AND BELIEF, THIS BRIDGE DESIGN IS STRUCTURALLY ADEQUATE FOR THE DESIGN LOADING SHOWN ON THE PLANS. THE DESIGN IS AN ECONOMICAL ONE FOR THE STYLE OF STRUCTURE AND COMPLIES WITH REQUIREMENTS OF THE CURRENT AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES."



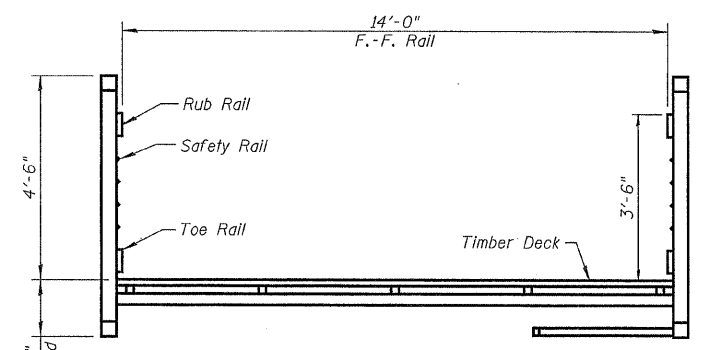
LOCATION SKETCH

BILL OF MATERIALS - BRIDGE

| ITEM | UNIT | SUB. | SUPER. | TOTAL |
|-------------------------------------------|---------|------|--------|-------|
| Porous Granular Embankment | Cu. Yd. | 10 | — | 10 |
| Structure Excavation | Cu. Yd. | 52 | — | 52 |
| Concrete Structures | Cu. Yd. | 13.7 | — | 13.7 |
| Reinforcement Bars | Pound | 1130 | — | 1130 |
| Furnishing Metal Shell Piles 12" X 0.179" | Foot | 145 | — | 145 |
| Driving Piles | Foot | 145 | — | 145 |
| Test Pile Metal Shells | Each | 1 | — | 1 |
| Pedestrian Truss Superstructure | Sq. Ft. | — | 889 | 889 |
| Form Liner Textured Surface | Sq. Yd. | 8 | — | 8 |
| Anti-Graffiti Coating | Sq. Ft. | 76 | — | 76 |

GENERAL NOTES

Reinforcement Bars shall conform to the Requirements of AASHTO M-31, M-42, or M-53 Grade 60.
 The Contractor Shall Drive a Test Pile in a Permanent Location at the North Abutment, as Directed by the Engineer Prior to Ordering the Remainder of the Piles.
 Porous Granular Embankment shall be Placed a Distance of 2 Feet Behind Each Abutment from Bottom of Cap to Top of Backwall.
 Anchor Bolt Sizes and Layout and Bearing Design and Materials by Bridge Manufacturer.



TYPICAL SECTION THRU TRUSS

** To be Determined by Truss Manufacturer.

GENERAL PLAN AND ELEVATION KISHWAUKEE STREET PEDESTRIAN BRIDGE OVER KEITH CREEK ROCKFORD, ILLINOIS

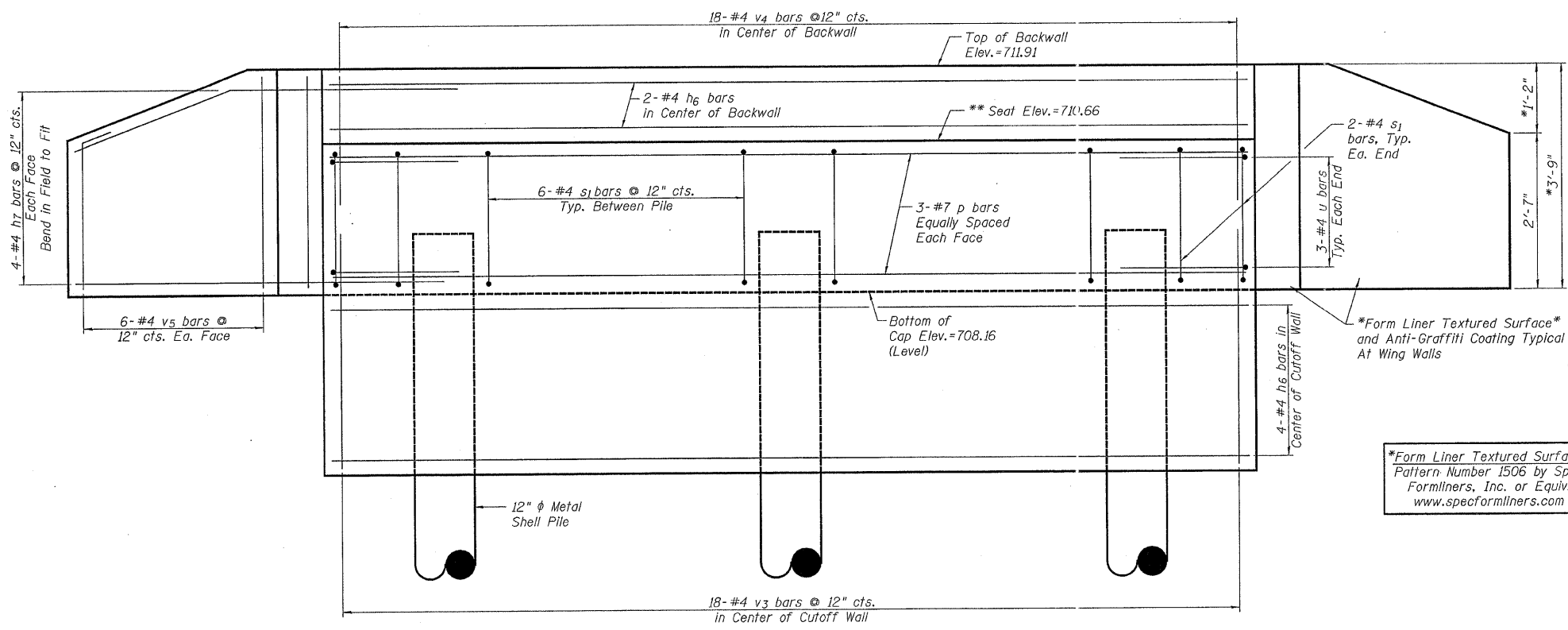


800 East Second Street Dixon, Illinois 61021
 Phone 815.294.2881 Fax 815.294.3985
 Design Firm #164-000018
 www.willett-hofmann.com
 WHA # 1158D03

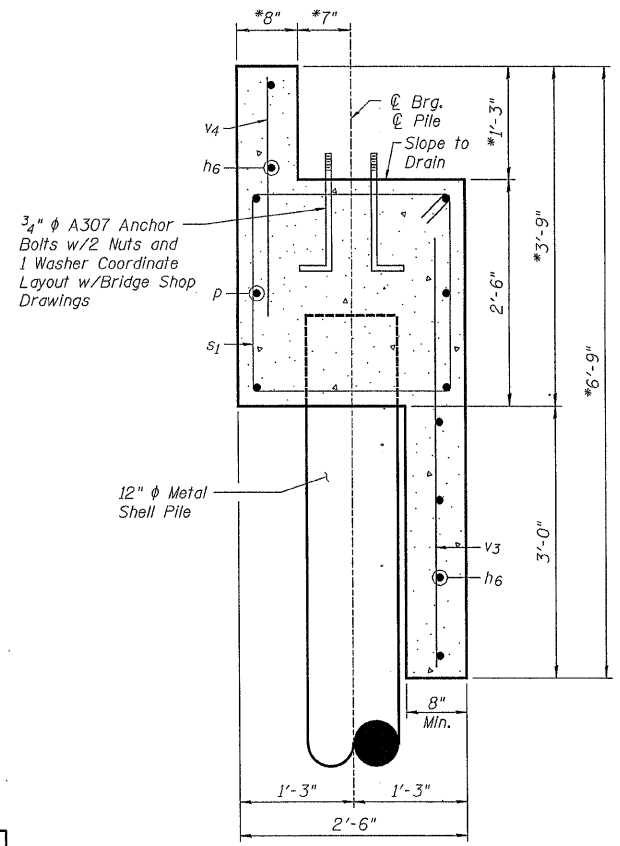
DESIGNED BY: M.R. Leslie
 DATE: 3/06
 CHECKED BY: B.K. Converse
 DATE: 3/06
 DRAWN BY: R.D. Allen
 DATE: 3/06

| | | | | |
|---------------------|----------------------------------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP134 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 36 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

** Seat Elevation may require Adjustment After Approval of Final Shop Drawings.



ELEVATION

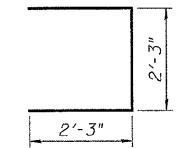


SECTION THRU ABUTMENT

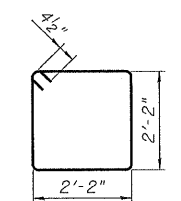
*Verify Dimensions Based on Final Bridge Shop Drawings

BILL OF MATERIAL-N. & S. ABUT'S.

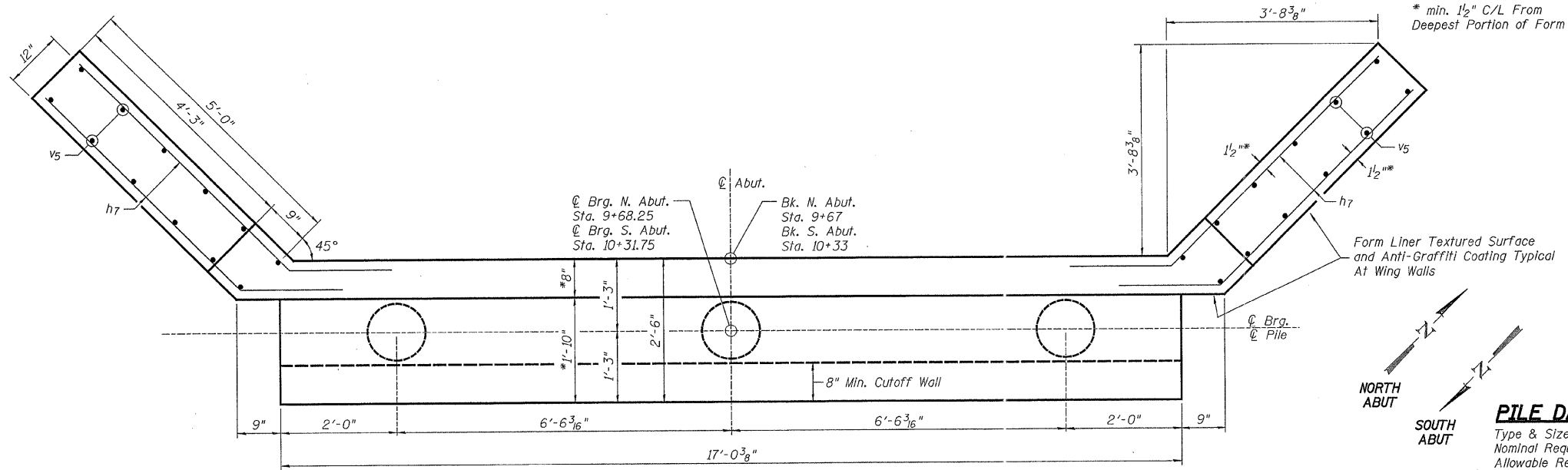
| Bar | No. | Size | Length | Shape |
|-------------------------------------------|-----|------|---------|-------|
| h6 | 12 | #4 | 15'-3" | — |
| h7 | 32 | #4 | 8'-0" | — |
| p | 12 | #7 | 15'-3" | — |
| s1 | 32 | #4 | 9'-5" | □ |
| u | 12 | #4 | 6'-9" | — |
| v3 | 36 | #4 | 4'-6" | — |
| v4 | 36 | #4 | 2'-9" | — |
| v5 | 48 | #4 | 3'-6" | — |
| Concrete Structures | | | Cu. Yd. | 13.7 |
| Reinforcement Bars | | | Pound | 1130 |
| Structure Excavation | | | Cu. Yd. | 52 |
| Furnishing Metal Shell Piles 12" X 0.179" | | | Foot | 145 |
| Driving Piles | | | Foot | 145 |
| Test Pile Metal Shells | | | Each | 1 |
| Porous Granular Embankment | | | Cu. Yd. | 10 |
| Form Liner Textured Surface | | | Sq. Yd. | 8 |
| Anti-Graffiti Coating | | | Sq. Ft. | 76 |



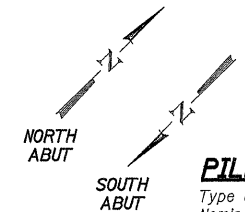
BAR u



BAR s1



ABUTMENT PLAN



PILE DATA

Type & Size: Metal Shell - 12 Inch Dia. X 0.179 Inch Walls
Nominal Required Bearing: 102 Kips
Allowable Resistance Available: 34 Kips
Estimated Length: 29'

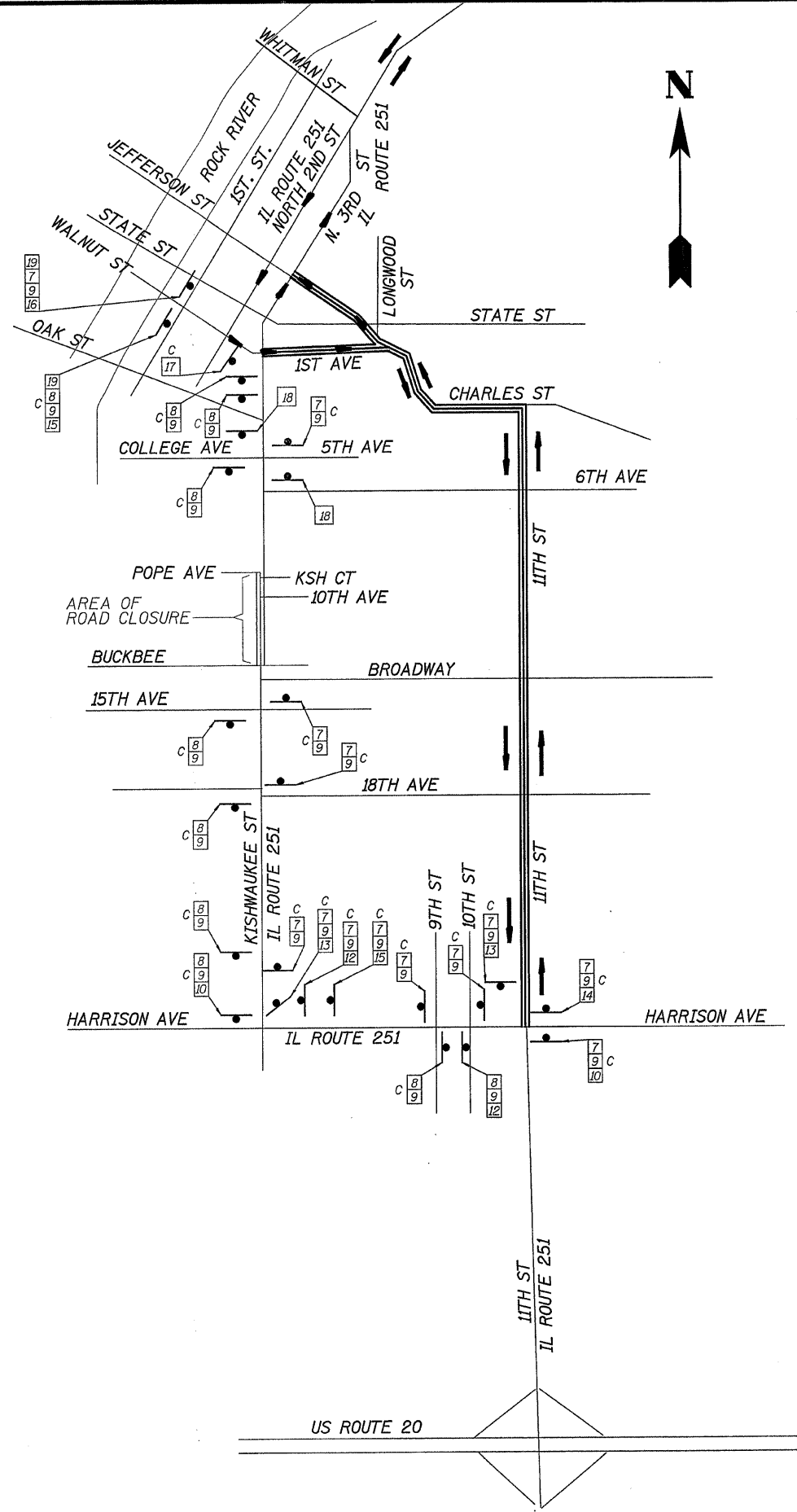
NOTES:

All Exposed Edges shall have Standard 3/4" Chamfers, Except as Noted.
Seat Elevations, Abutment Cap Dimensions, and Backwall Dimensions shall be Verified by Engineer after Bridge Shop Drawings are Approved and Prior to Ordering Reinforcement Bars.
Plywood Placed Against Pile to Form Cutoff Wall may Remain in Place After Cutoff Wall is Poured. Any Cribbing, Bracing, or Shoring Required Behind Cutoff Wall shall be Removed Prior to Pouring Abutment Cap.

**NORTH AND SOUTH ABUTMENT DETAILS
KISHWAUKEE STREET PEDESTRIAN
BRIDGE OVER KEITH CREEK
ROCKFORD, ILLINOIS
WHA # 1158D03**

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|----------------------------------|---------|---------------------------|--------------|-----------|
| 99-00480-00-BR 03-00522-00-TL | | WINNEBAGO | 62 | 37 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ | | ILLINOIS FED. AID PROJECT | | |

EXISTING SIGN LOCATION & REVISIONS



SIGN LEGEND

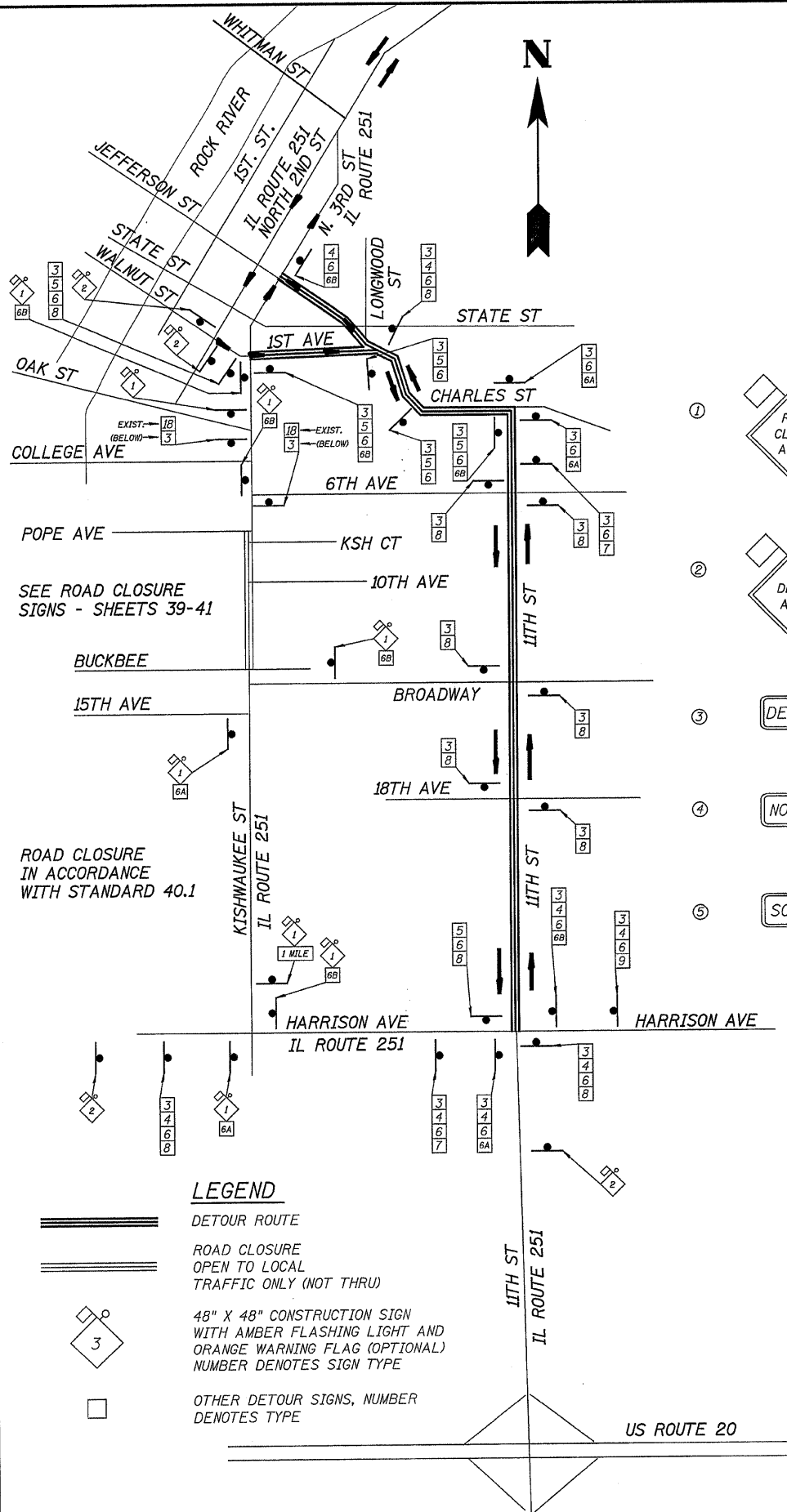
- ⑦ NORTH BLACK ON WHITE
- ⑧ SOUTH BLACK ON WHITE
- ⑨ ILLINOIS 251 BLACK ON WHITE
- ⑩ BLACK ON WHITE
- ⑪ BLACK ON WHITE
- ⑫ BLACK ON WHITE
- ⑬ BLACK ON WHITE
- ⑭ BLACK ON WHITE
- ⑮ RIGHT LANE BLACK ON WHITE
- ⑯ LEFT LANE BLACK ON WHITE
- ⑰ ILLINOIS SOUTH 251 → BLACK LETTERS ON GREEN BACKGROUND (OVERHEAD)
- ⑱ MORGAN STREET BRIDGE CLOSED TO ALL TRUCKS FOLLOW IL 251 BLACK ON WHITE
- ⑲ JCT. BLACK ON WHITE
- C COVER WITH BLACK PLASTIC (WRAP AROUND SIGN & POST)

NOTES

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL BARRICADES, SIGNS, LIGHTS, AND OTHER DEVICES INSTALLED BY HIM ARE IN PLACE AND OPERATING 24 HOURS EACH DAY INCLUDING SUNDAYS AND HOLIDAYS DURING THE TIME THE DETOUR IS IN EFFECT.
2. THE CONTRACTOR SHALL MAKE ALL CHANGES IN SIGNING THAT ARE DEEMED NECESSARY BY THE ENGINEER.

| | | | | |
|----------------------------------|---------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 98-00480-00-BR 03-00522-00-TL | | WINNEBAGO | 62 | 38 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ | | ILLINOIS FED. AID PROJECT | | |

DETOUR SIGNING



SIGN LEGEND

- ① W20-3, 48" X 48" WITH AMBER FLASHING LIGHTS AND FLAG. (8 REQ'D) OR DISTANCE AS SHOWN
- ② W20-2, 48" X 48" WITH AMBER FLASHING LIGHTS AND FLAG. (4 REQ'D)
- ③ M4-8, 24" X 12" (23 REQ'D)
- ④ M3-1, 24" X 12" (8 REQ'D)
- ⑤ M3-3, 24" X 12" (6 REQ'D)
- ⑥ ILLINOIS ROUTE MARKER 24"X24" (17 REQ'D)
- ⑥A M6-1 L, 21" X 15" (5 REQ'D)
- ⑥B M6-1 R, 21" X 15" (8 REQ'D)
- ⑦ M5-1 L, 21" X 15" (2 REQ'D)
- ⑧ M6-3, 21" X 15" (11 REQ'D)
- ⑨ M5-1 R, 21" X 15" (1 REQ'D)
- ⑩ SEE SHEET 37

SIGN SPACING

FIRST SIGN FROM INTERSECTION 25'. ALL DETOUR SIGNS (M) SPACED 250' EXCEPT #2 WHICH IS 500' TO SUCCEEDING SIGN.

SPECIAL DETOUR NOTES

THE TOTAL LENGTH OF THE DETOUR IS 2.3 MILES.

ADDITIONAL SIGNS

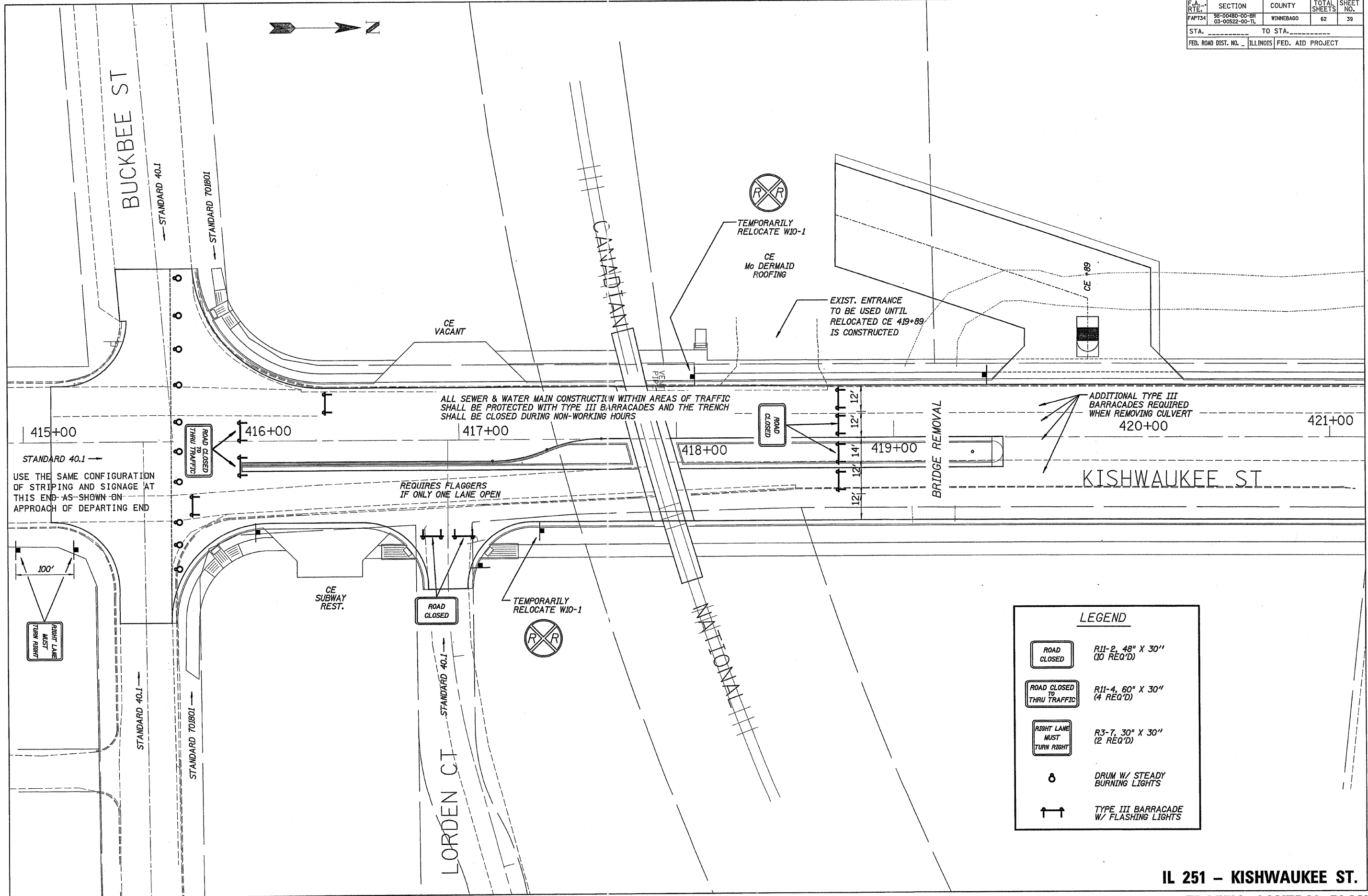
SEE SPECIAL PROVISIONS TRAFFIC CONTROL AND PROTECTION FOR ROAD CLOSED TO OVERSIZED LOADS XX MILES AHEAD AND OTHER SIGNING REQUIREMENTS

LEGEND

- DETOUR ROUTE
- ROAD CLOSURE OPEN TO LOCAL TRAFFIC ONLY (NOT THRU)
- 48" X 48" CONSTRUCTION SIGN WITH AMBER FLASHING LIGHT AND ORANGE WARNING FLAG (OPTIONAL) NUMBER DENOTES SIGN TYPE
- OTHER DETOUR SIGNS, NUMBER DENOTES TYPE

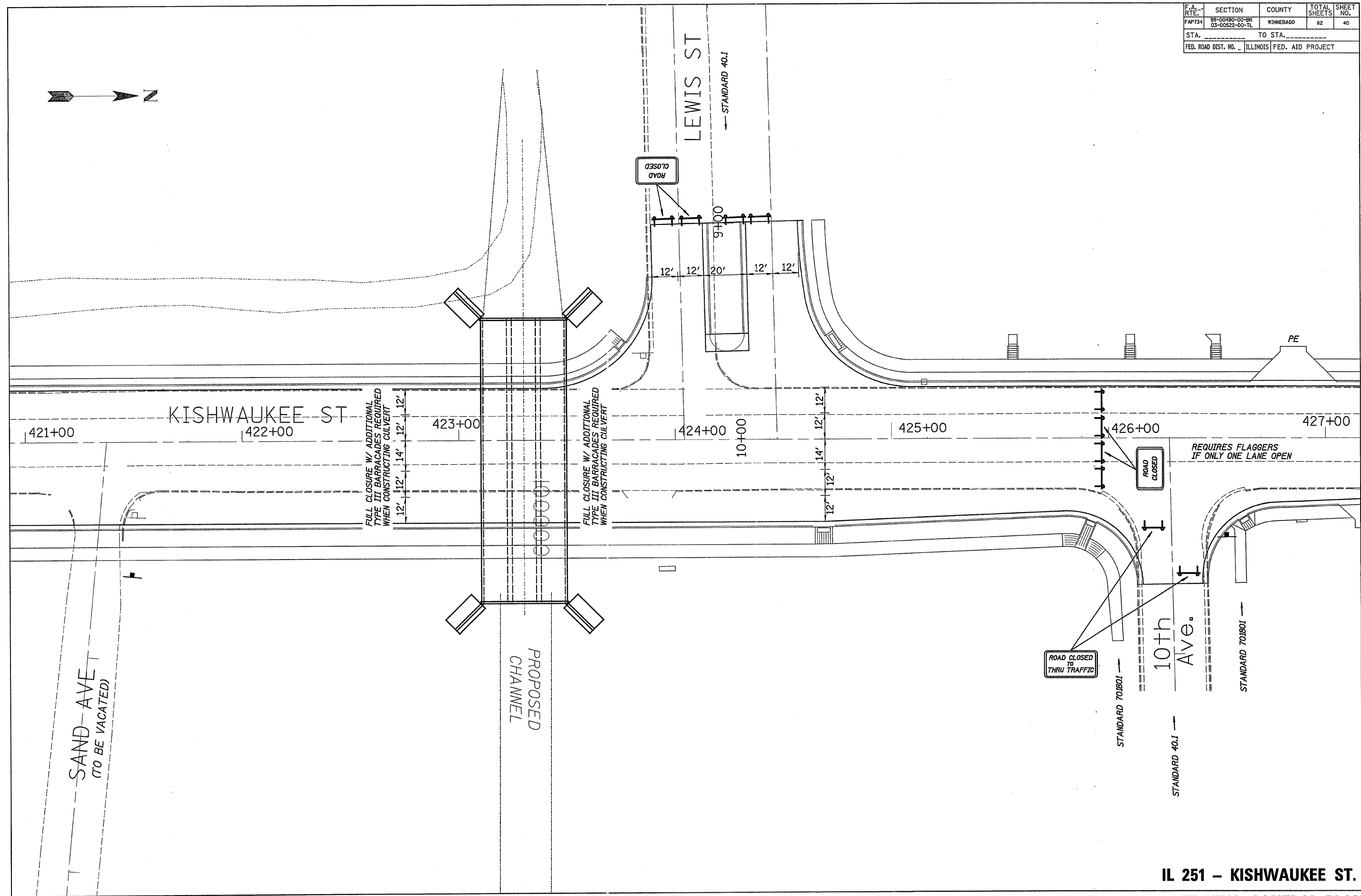
1. ALL SIGNING SHALL BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTIONS ADOPTED JAN. 1, 2007", "THE QUALITY STANDARD FOR WORK ZONE TRAFFIC CONTROL DEVICES, LATEST EDITION", THE DETAILS IN THESE PLANS, AND THE LATEST EDITION OF THE STATE OF ILLINOIS "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".
2. THE CONTRACTOR SHALL SCHEDULE ALL WORK IN AN EXPEDIENT MANNER TO REDUCE THE LENGTH OF TIME THAT THE DETOUR NEEDS TO BE IN EFFECT.
3. THE ENGINEER SHALL BE NOTIFIED IN WRITING AT LEAST THREE WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT. THE ENGINEER WILL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES FOR APPROVAL OF SUCH DATE.
4. IF DEEMED NECESSARY BY THE ENGINEER A PRE-CONSTRUCTION MEETING WITH THE CONTRACTOR SHALL BE HELD AT LEAST TWO WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT.
5. THE CONTRACTOR SHALL SUPPLY TO THE ENGINEER THE NAMES AND TELEPHONE NUMBERS OF HIS REPRESENTATIVES ON THE CONSTRUCTION SITE AND HIS REPRESENTATIVES FOR THE DETOUR SIGNING PRIOR TO THE START OF WORK.
6. IF REQUESTED BY THE CONTRACTOR IN WRITING AT LEAST THREE WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT THE ENGINEER WILL FIELD LOCATE THE POSITIONS OF ANY SIGNS.
7. LONGITUDINAL DIMENSIONS SHOWN ON THE PLANS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
8. THE ROAD SHALL NOT BE CLOSED UNTIL ALL SIGNING IS ERECTED IN ACCORDANCE WITH THE DETOUR PLAN AND INSPECTED AND APPROVED BY THE ENGINEER.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL BARRICADES, SIGNS, LIGHTS, AND OTHER DEVICES INSTALLED BY HIM ARE IN PLACE AND OPERATING 24 HOURS EACH DAY INCLUDING SUNDAYS AND HOLIDAYS DURING THE TIME THE DETOUR IS IN EFFECT.
10. THE CONTRACTOR SHALL MAKE ALL CHANGES IN SIGNING THAT ARE DEEMED NECESSARY BY THE ENGINEER.
11. ALL EXISTING SIGNING THAT IS NOT APPLICABLE WHILE THE DETOUR IS IN EFFECT SHALL BE COMPLETELY COVERED BY THE CONTRACTOR, IN A MANNER APPROVED BY THE ENGINEER.
12. ALL DETOUR SIGNING EXCEPT REGULATORY SIGNS SHALL HAVE BLACK LEGENDS ON FLUORESCENT ORANGE SHEETING AND STANDARD BLACK BORDERS. THE FLUORESCENT ORANGE REFLECTIVE SHEETING SHALL MEET THE REQUIREMENTS OF ARTICLE 1084.02 OF THE STANDARD SPECIFICATIONS. ALL DETOUR SIGNING SHALL BE NEW OR LIKE NEW CONDITION. THE ENGINEER SHALL BE THE SOLE JUDGE OF THE CONDITION AND ACCEPTANCE OF THE SIGNS.
13. THE SIZES OF ALL SIGNS NOT SPECIFIED IN THESE PLANS SHALL BE AS REQUIRED BY THE ILLINOIS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
14. AS A MINIMUM, ALL AMBER FLASHING LIGHTS THAT ARE REQUIRED FOR THIS DETOUR SHALL MEET THE REQUIREMENTS FOR TYPE A-LOW INTENSITY FLASHING LIGHTS IN ARTICLE 1084.01 OF THE STANDARD SPECIFICATIONS. ALL LIGHTS SHALL OPERATE DURING THE HOURS OF DARKNESS. ONLY LIGHTS THAT HAVE BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION SHALL BE USED.
15. THE MINIMUM DIMENSIONS OF THE ORANGE WARNING FLAGS SHOWN IN THE PLANS ARE 18" X 18".
16. THE TYPE III BARRICADES USED AT THE POINT OF CLOSURE TO THRU TRAFFIC SHALL NOT EXCEED 12'-0" IN WIDTH EACH, FOR A SINGLE APPROACH LANE.
17. ALL TYPE III BARRICADES SHALL HAVE TWO (2) AMBER TYPE A-LOW INTENSITY FLASHING LIGHTS SPACED NEAR THE CENTERLINE OF THE SUPPORTS.
18. CONSTRUCTION EQUIPMENT SHALL NOT BE PARKED IMMEDIATELY BEHIND THE TYPE III BARRICADES DURING NON-WORKING HOURS. IN ANY EVENT ARTICLE 701.11 OF THE STANDARD SPECIFICATIONS SHALL APPLY.
19. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE VISIBILITY OF ALL DETOUR AND CONSTRUCTION SIGNING. BRUSHING BACK VEGETATION IF DEEMED BY THE ENGINEER.
20. THE ENGINEER SHALL BE NOTIFIED AT LEAST TWO (2) HOURS BEFORE THE ROAD IS TO BE OPENED TO TRAFFIC. THE ENGINEER WILL CONTACT THE APPROPRIATE LOCAL AGENCIES, IDOT AND INTERESTED PARTIES.

| | | | | |
|---------------------|----------------------------------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAPT34 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 39 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



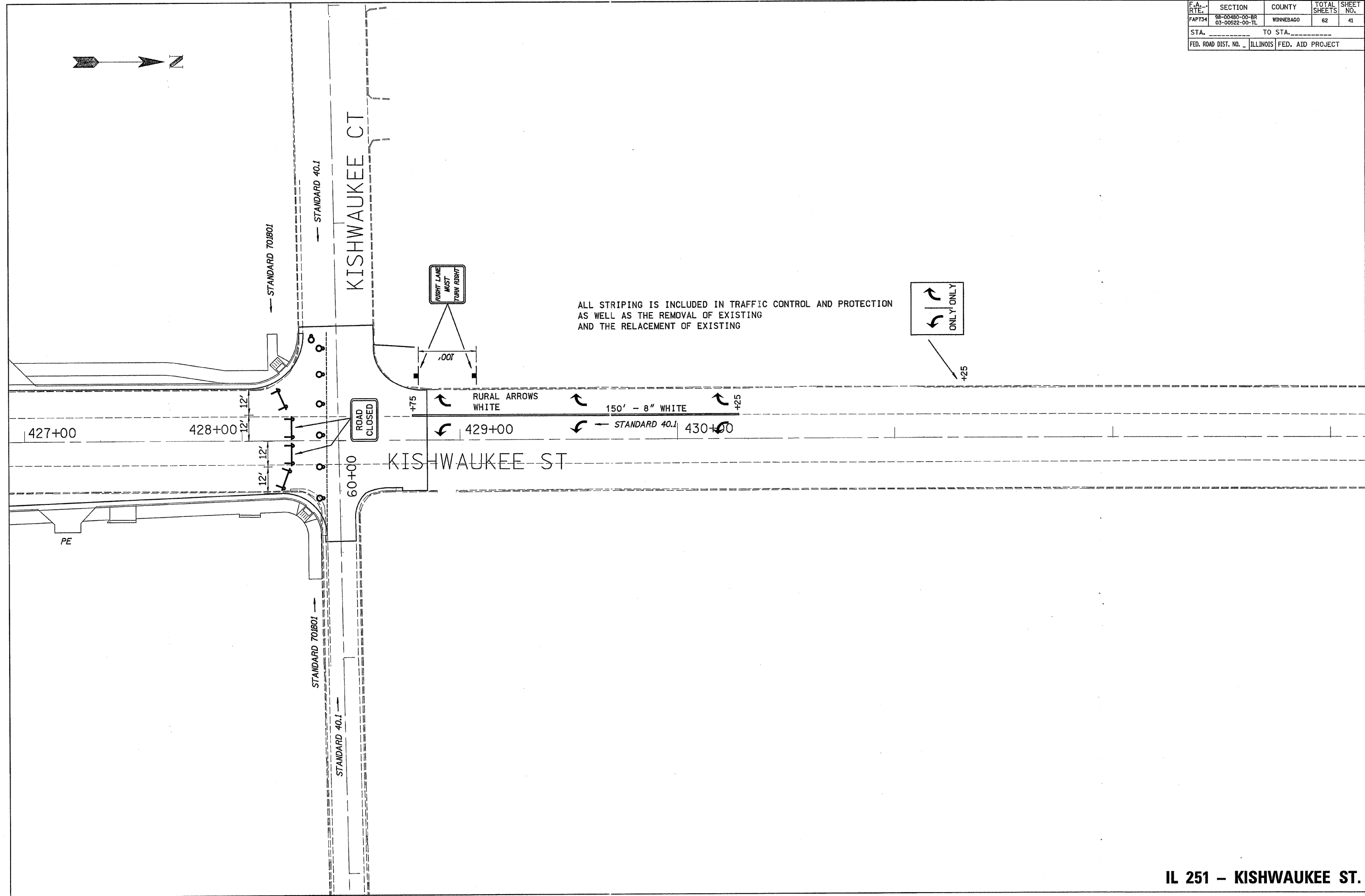
**IL 251 – KISHWAUKEE ST.
TRAFFIC CONTROL PLAN**

| | | | | |
|---------------------|----------------------------------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| F4P734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 40 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



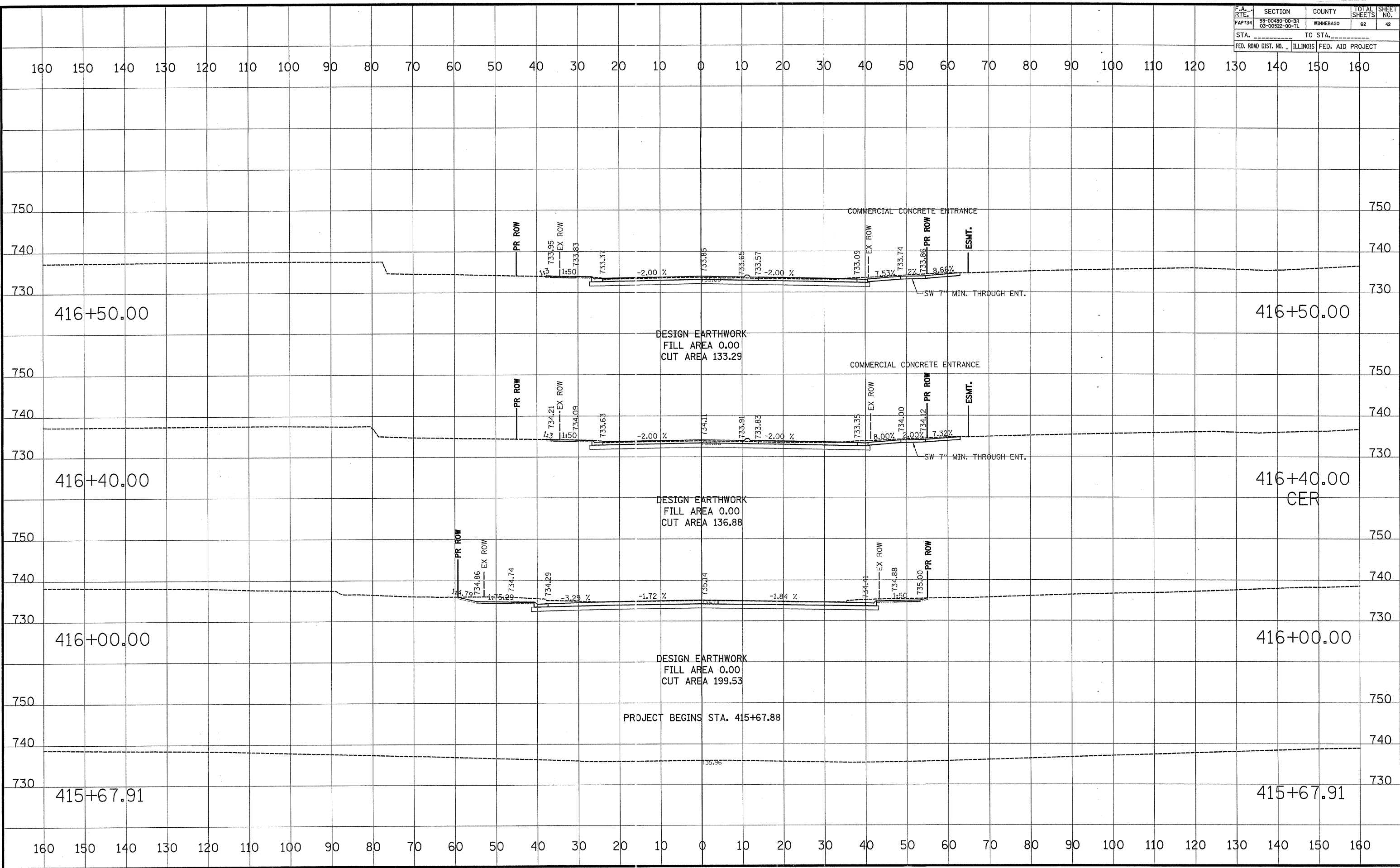
**IL 251 - KISHWAUKEE ST.
TRAFFIC CONTROL PLAN**

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------------------------|---------------------------|--------------|-----------|
| FAPT34 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 41 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ | | ILLINOIS FED. AID PROJECT | | |



**IL 251 - KISHWAUKEE ST.
TRAFFIC CONTROL PLAN**

| | | | | |
|---------------------|----------------------------------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAPT34 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 42 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



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| AREAS CHECKED | |
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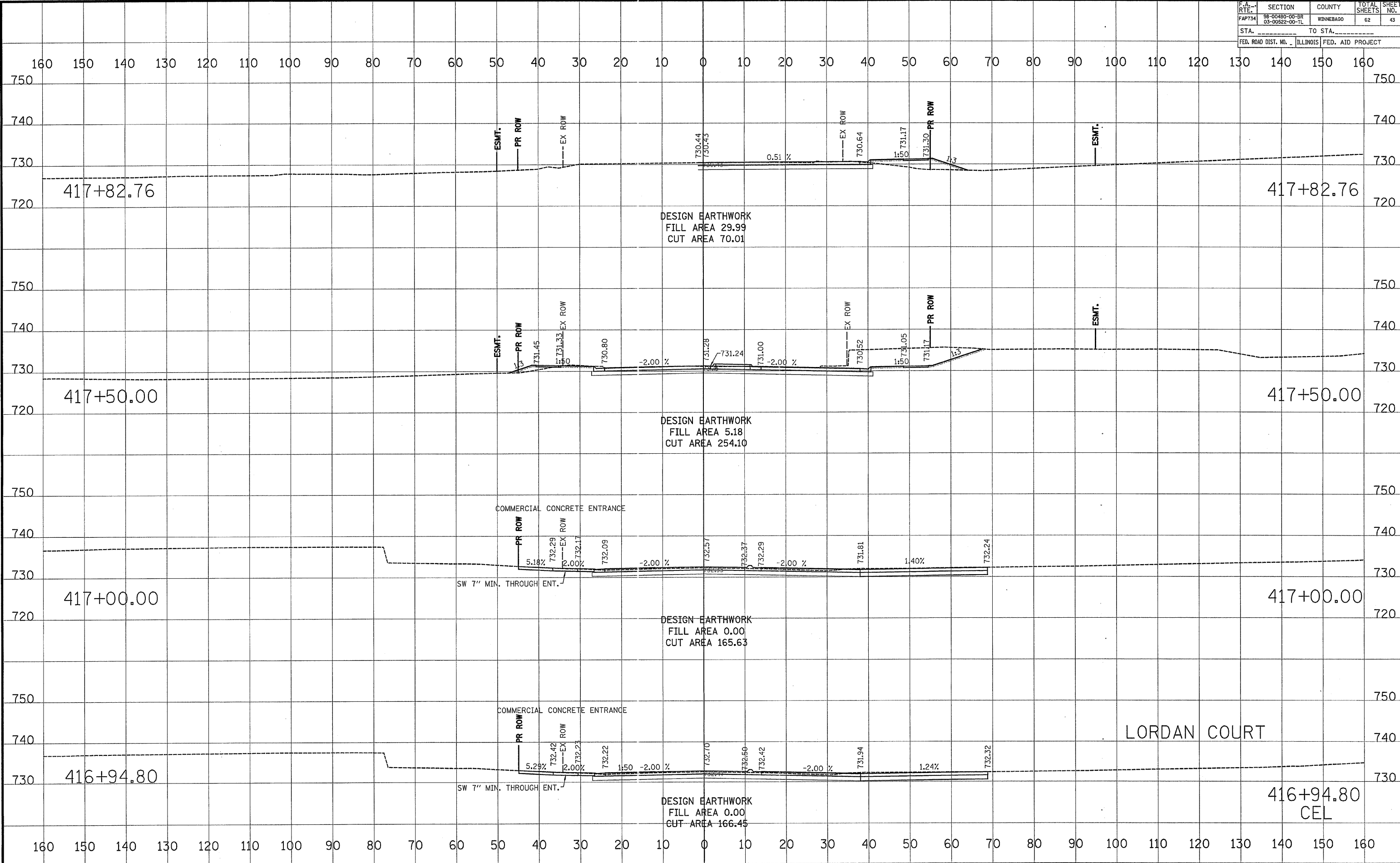
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| F.A. DIST. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 43 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

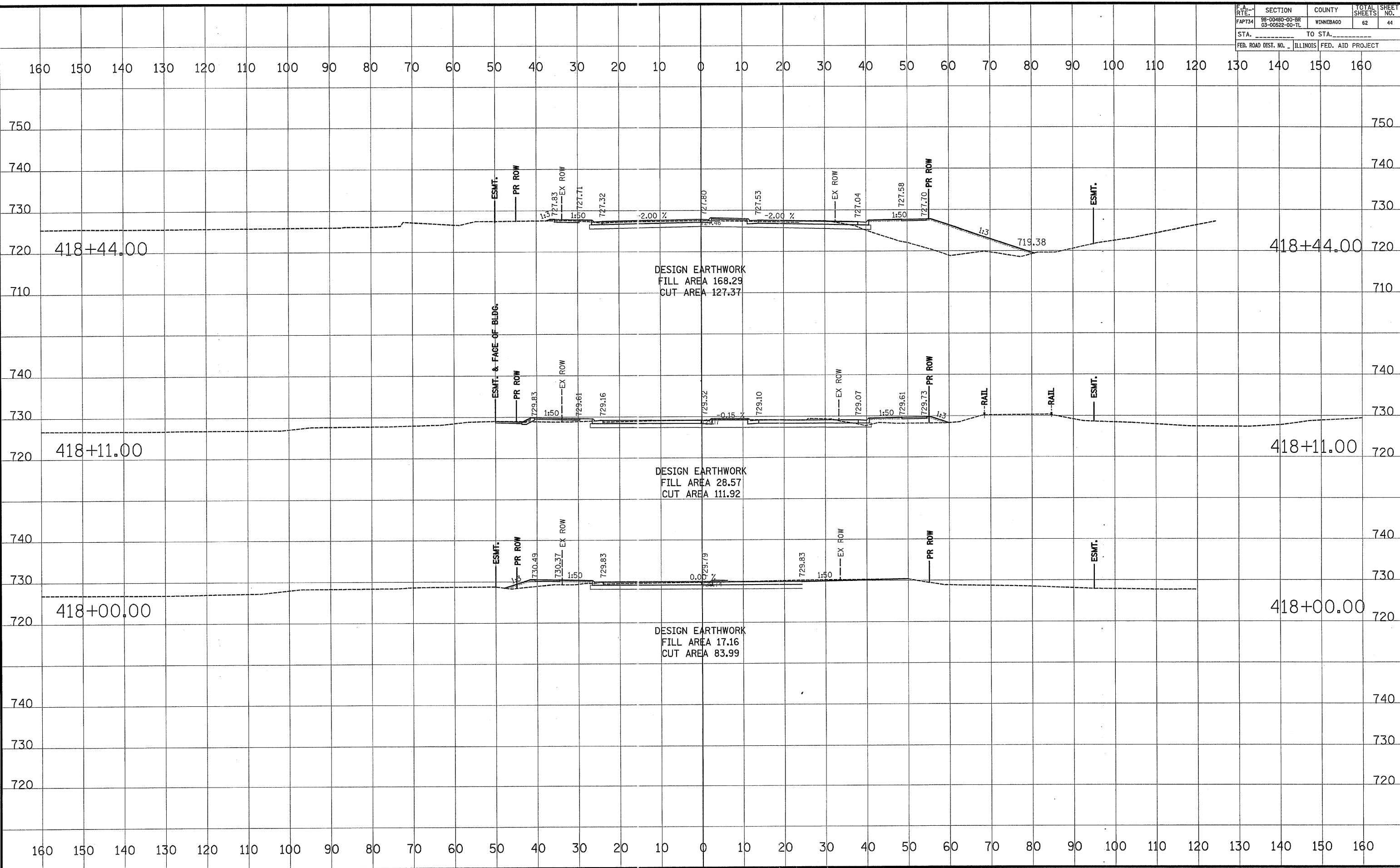
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| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAPT34 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 44 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT | | | | |



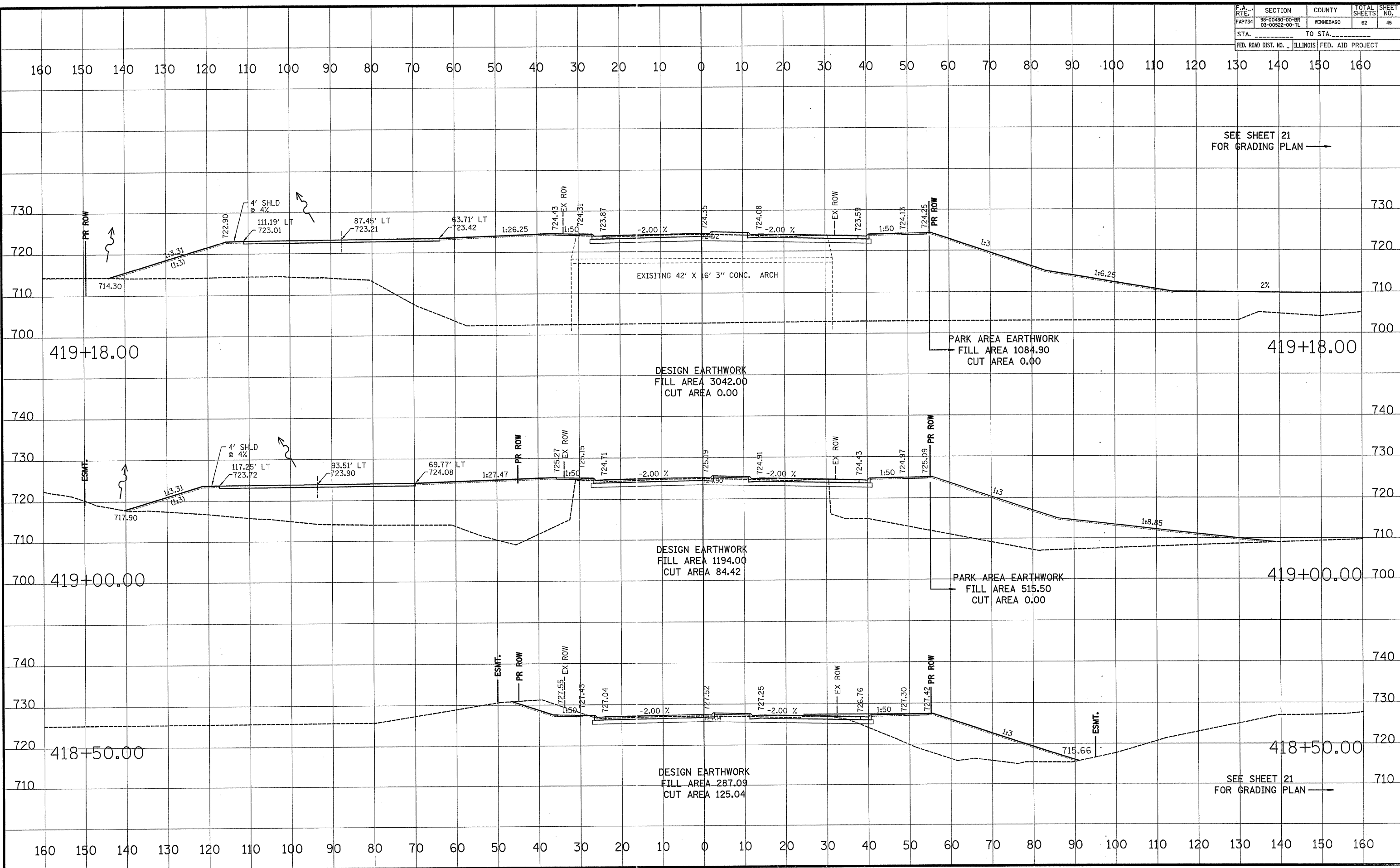
CROSS SECTIONS - KISHWAUKEE STREET

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PLOT DATE = \$DATE\$
FILE NAME = \$FILEL\$
PLOT SCALE = \$SCALE\$
REFERENCE = \$REF\$

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| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 45 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. ILLINOIS | | | FED. AID PROJECT | |



SEE SHEET 21 FOR GRADING PLAN →

SEE SHEET 21 FOR GRADING PLAN →

CROSS SECTIONS - KISHWAUKEE STREET

FINAL SURVEY PLOTTED DATE NO. BY DATE

ORIGINAL SURVEY PLOTTED DATE NO. BY DATE

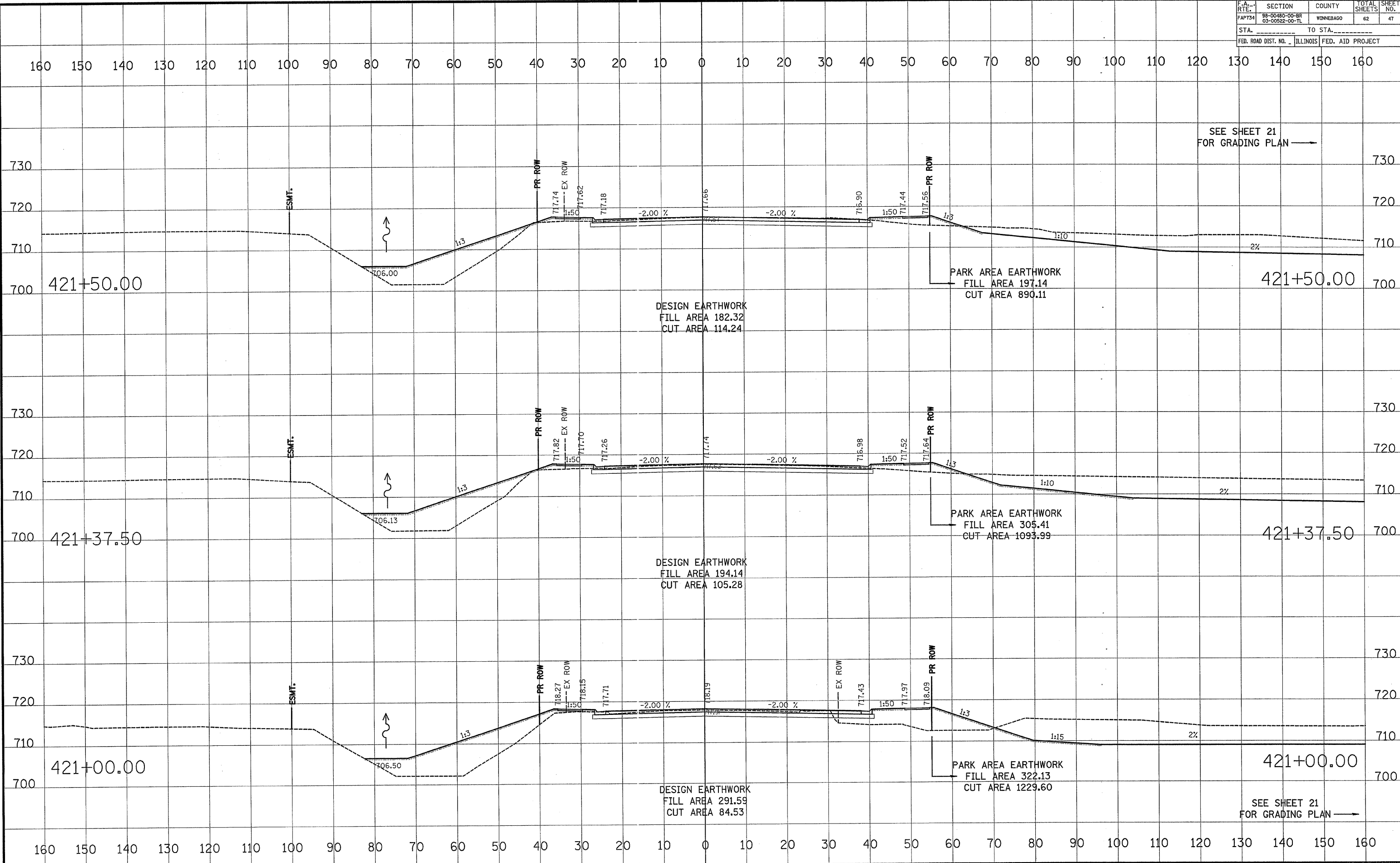
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SCALE = #SCALE#
REFERENCE = #REF#

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|---------------------|----------------------------------|------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 47 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

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| DATE | |
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| FINAL SURVEY | |
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| NOTE BOOK | |
| AREAS CHECKED | |
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| ORIGINAL SURVEY | |
| PLOTTED | |
| NOTE BOOK | |
| AREAS CHECKED | |
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| AREAS CHECKED | |
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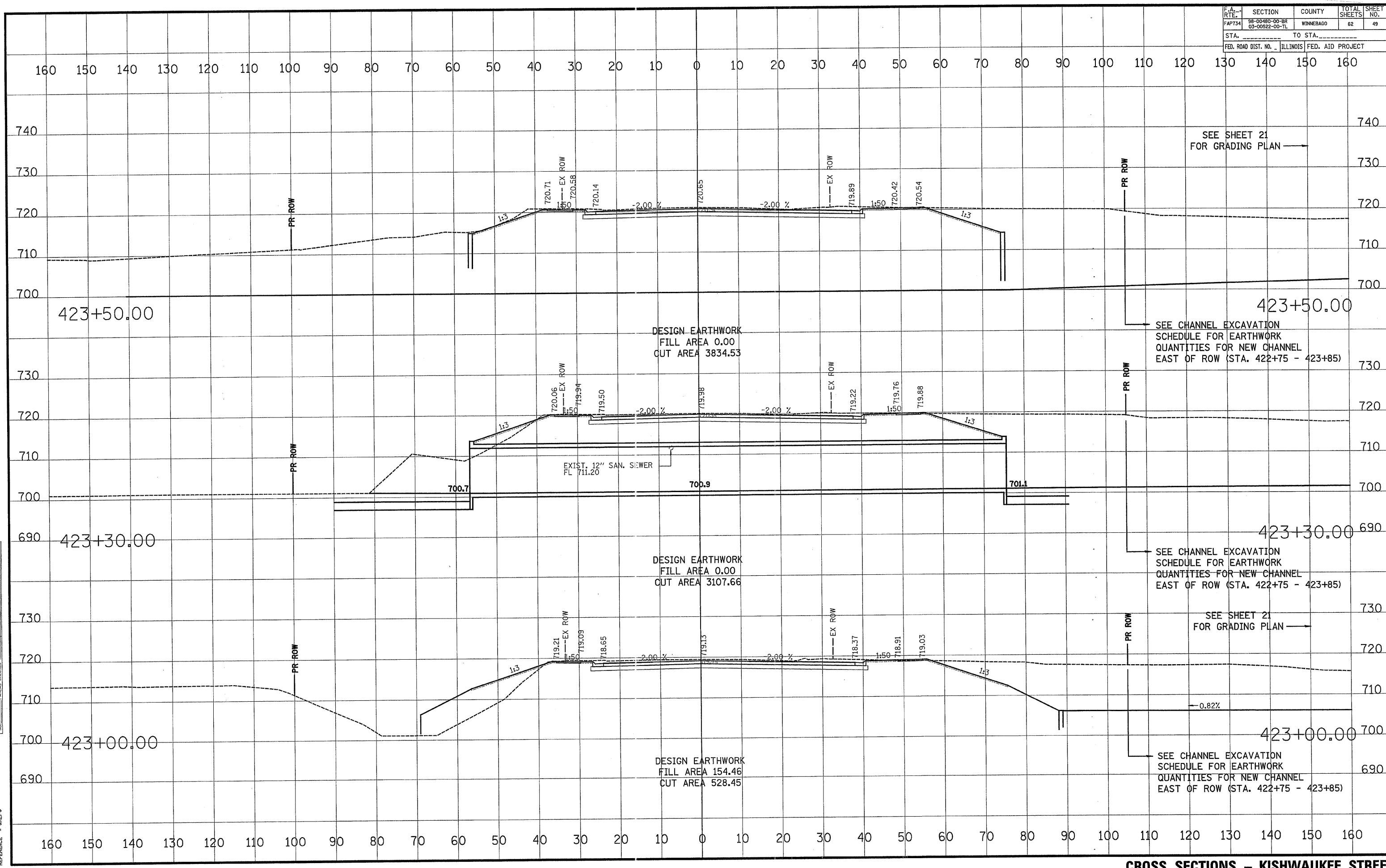


| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------------------------------------------|----------------------------------|---------------|--------------|-----------|
| FAPT34 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 49 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT _____ | | | | |

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 PLOT SCALE = #SCALE*
 REFERENCE = #REF*



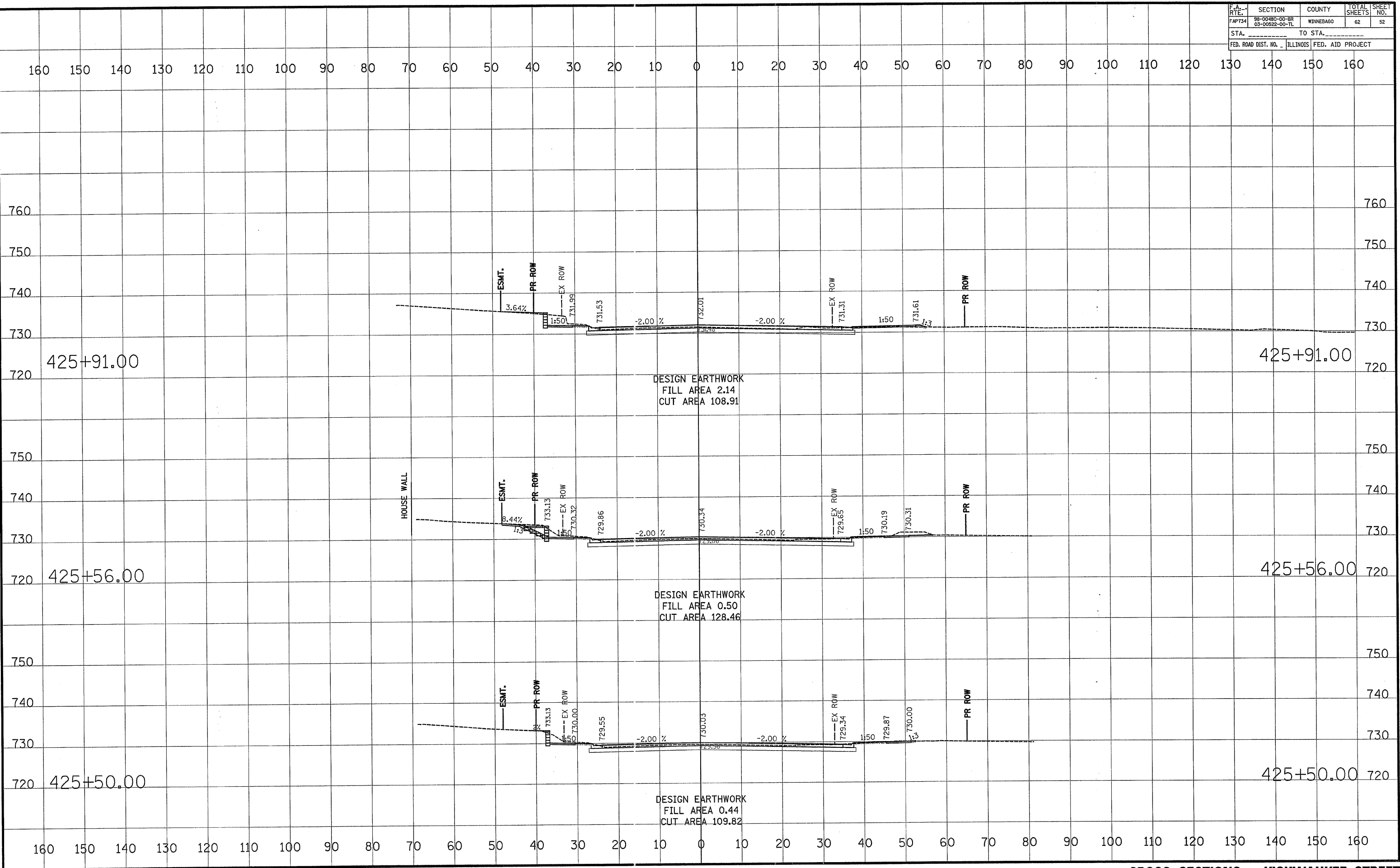
CROSS SECTIONS - KISHWAUKEE STREET

| | | | | |
|---------------------|----------------------------------|-----------|------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAPT34 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 52 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

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| FINAL SURVEY | BY | DATE |
| NOTE BOOK | | |
| NO. | | |
| SURVEYED | | |
| PLOTTED | | |
| AREA CHECKED | | |

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| ORIGINAL SURVEY | BY | DATE |
| NOTE BOOK | | |
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PLOT DATE = #DATE#
 PLOT SCALE = #SCALE#
 REFERENCE = #REF#



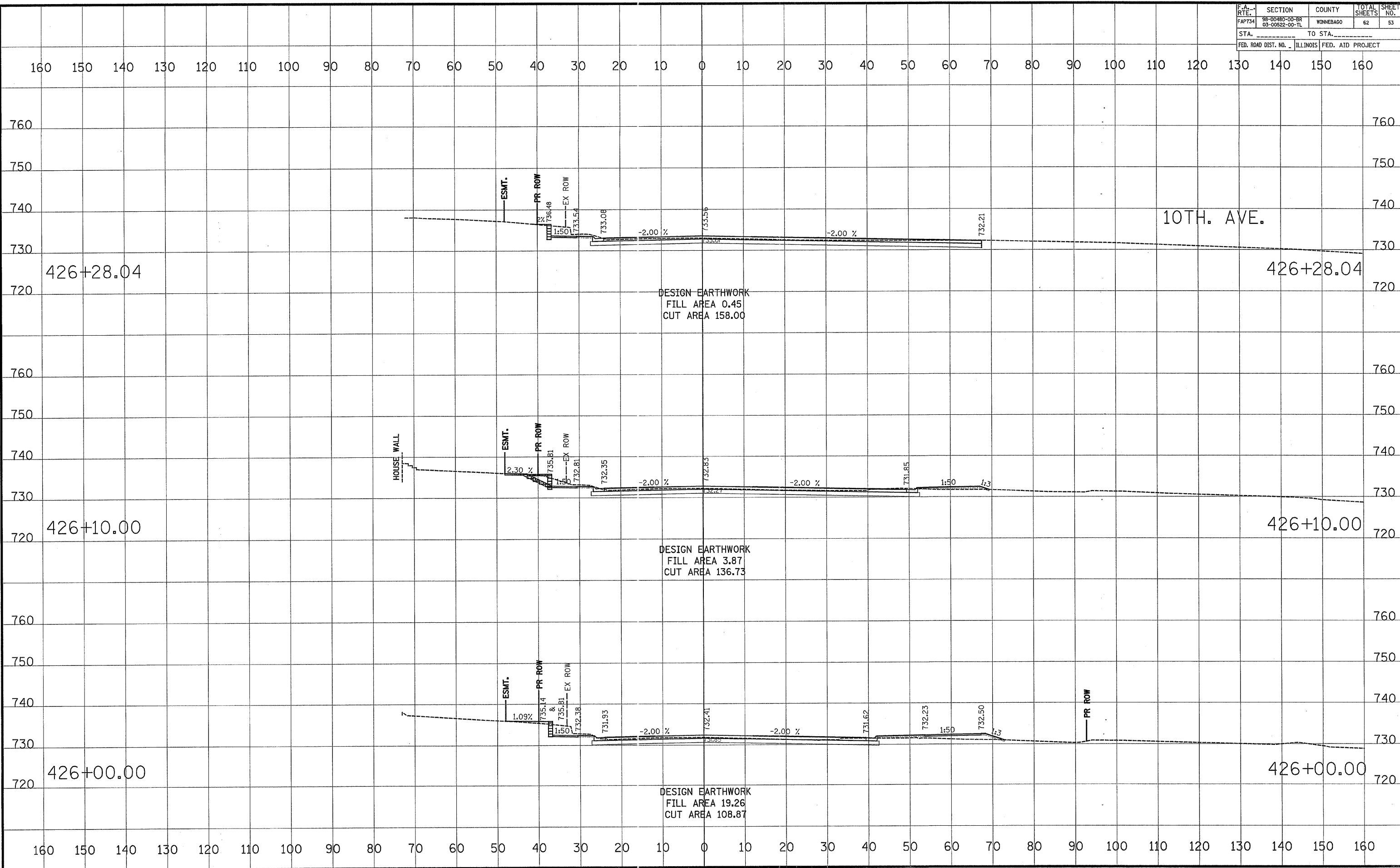
CROSS SECTIONS - KISHWAUKEE STREET

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|---------------------------|----------------------------------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 53 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ | | ILLINOIS FED. AID PROJECT | | |

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| FINAL SURVEY | DATE |
| SURVEYED | BY |
| NOTED | BY |
| NO. | |

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| ORIGINAL SURVEY | DATE |
| PLOTTED | BY |
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| PLAT DATE | DATE |
| PLAT SCALE | SCALE |
| REFERENCE | REF |



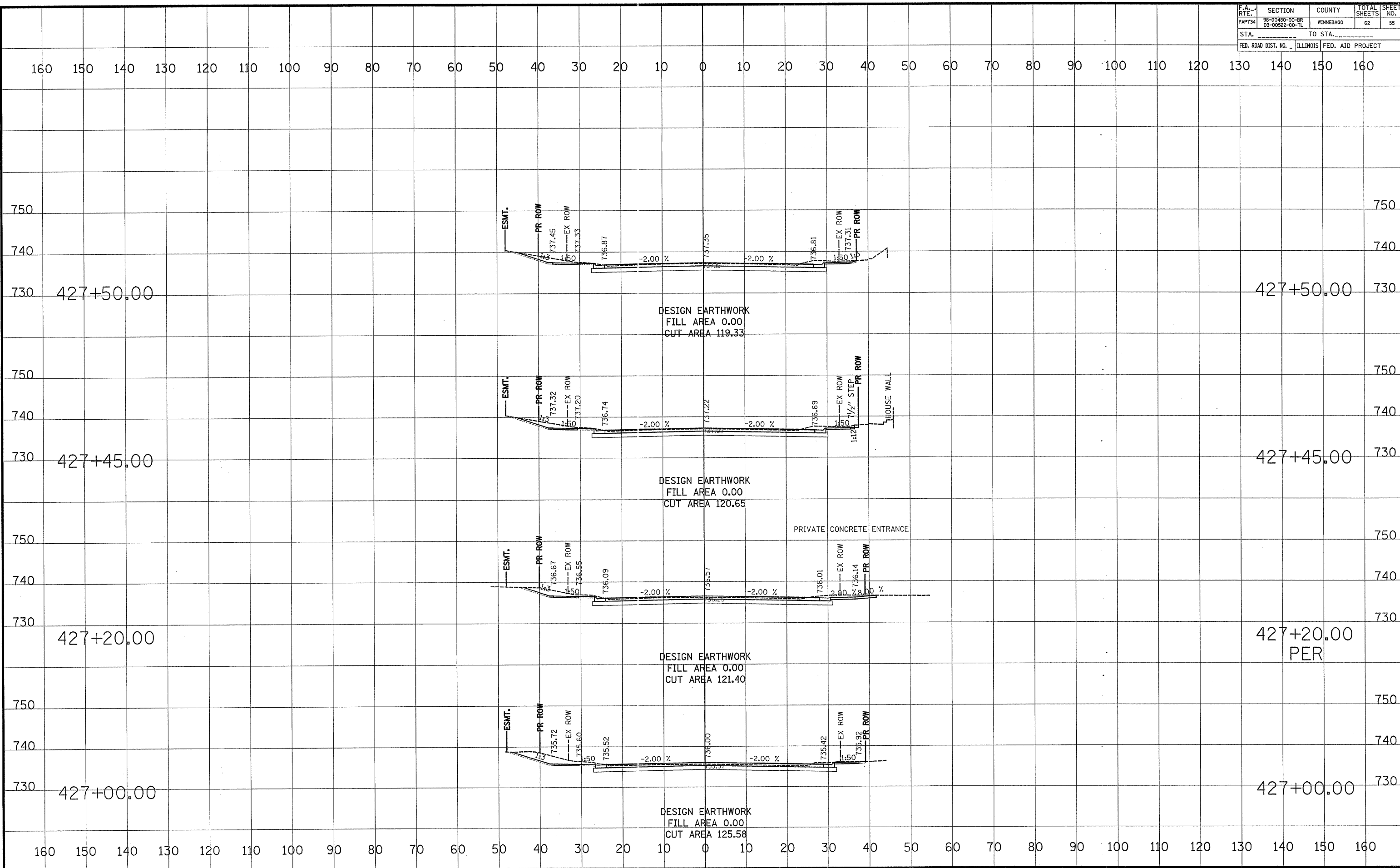
CROSS SECTIONS - KISHWAUKEE STREET

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|--------------------------|----------------------------------|-----------------------------------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAPT34 | 98-00480-00-BR 03-00822-00-TL | WINNEBAGO | 62 | 55 |
| STA. _____ TO STA. _____ | | FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT | | |

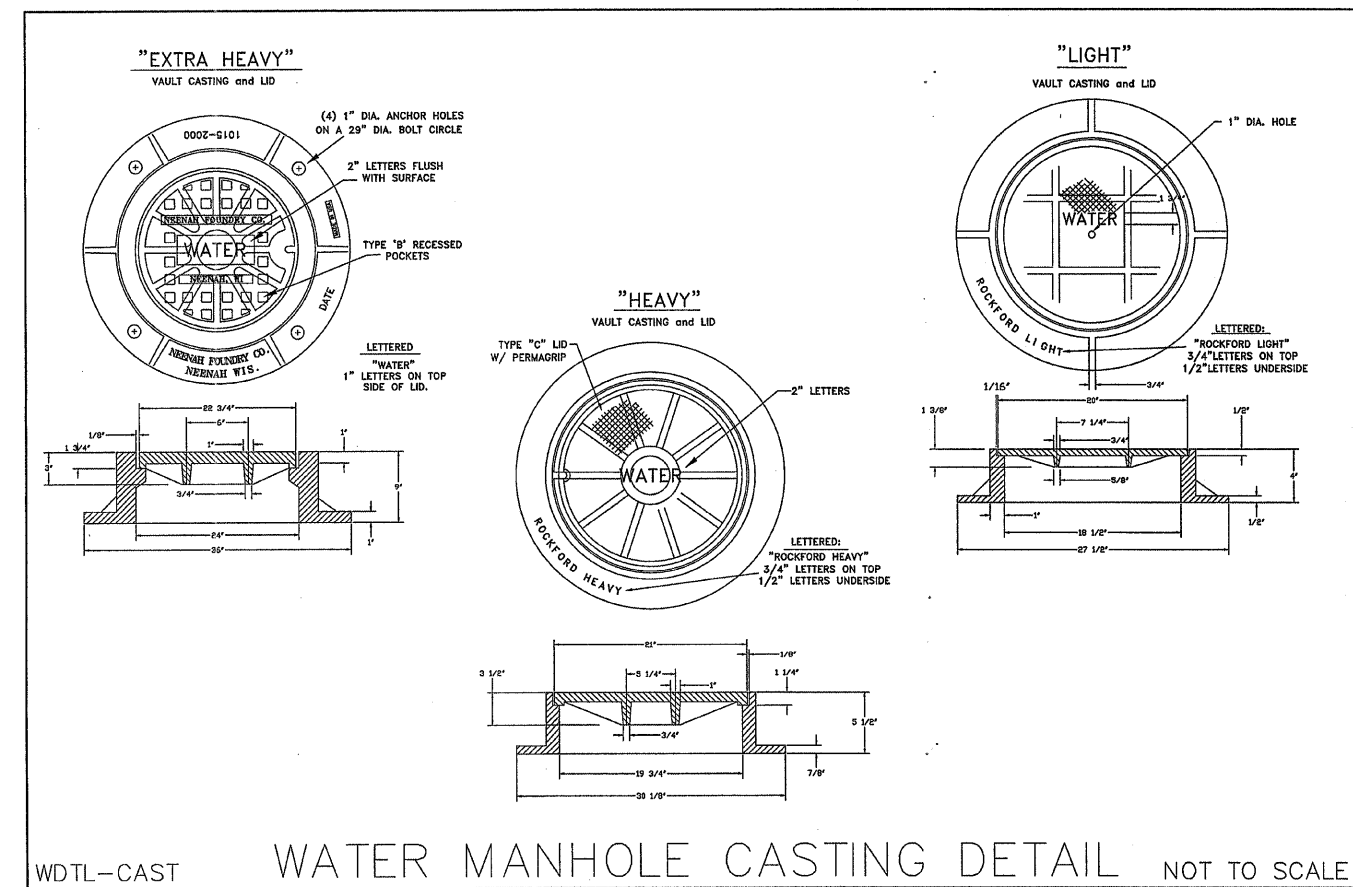
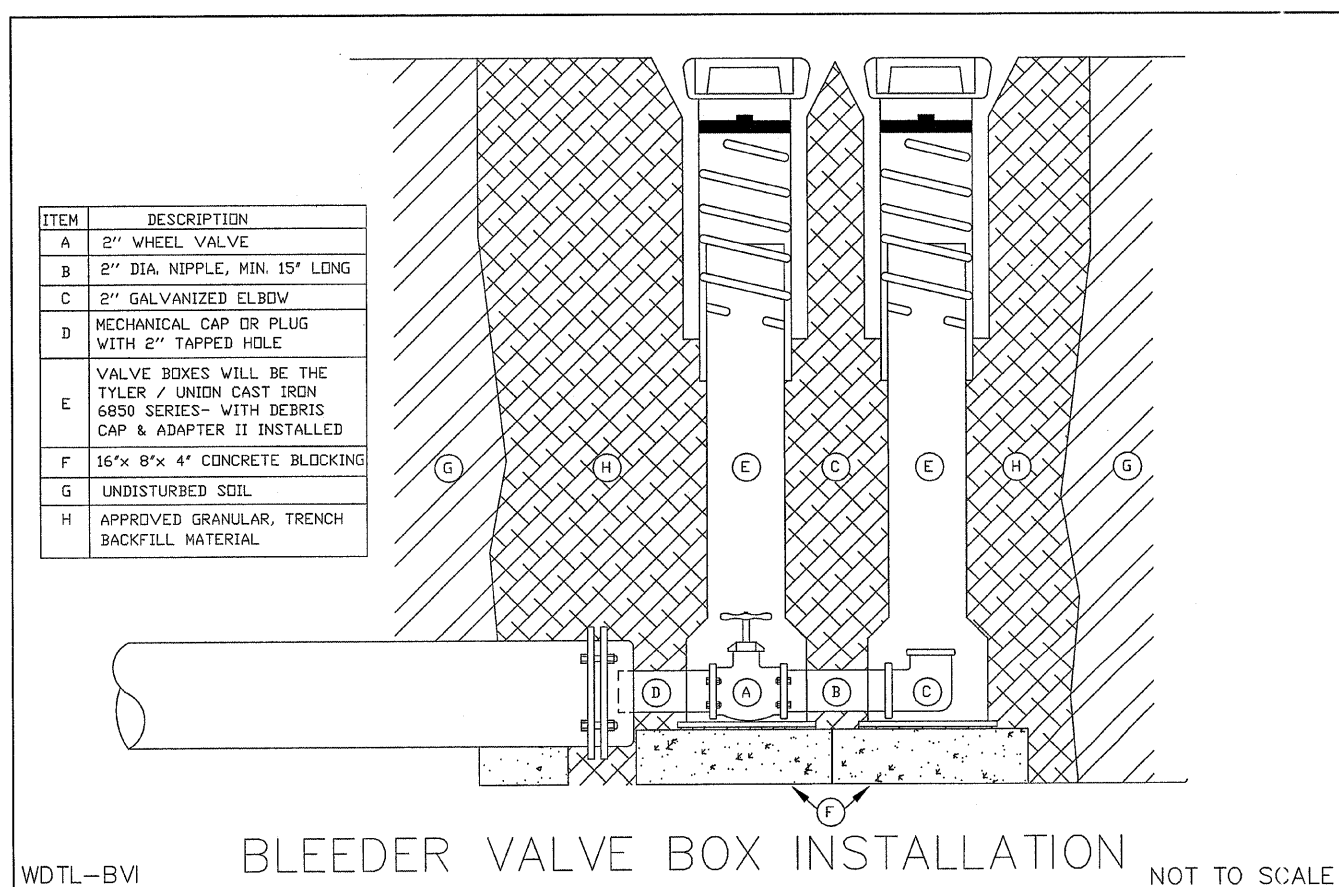
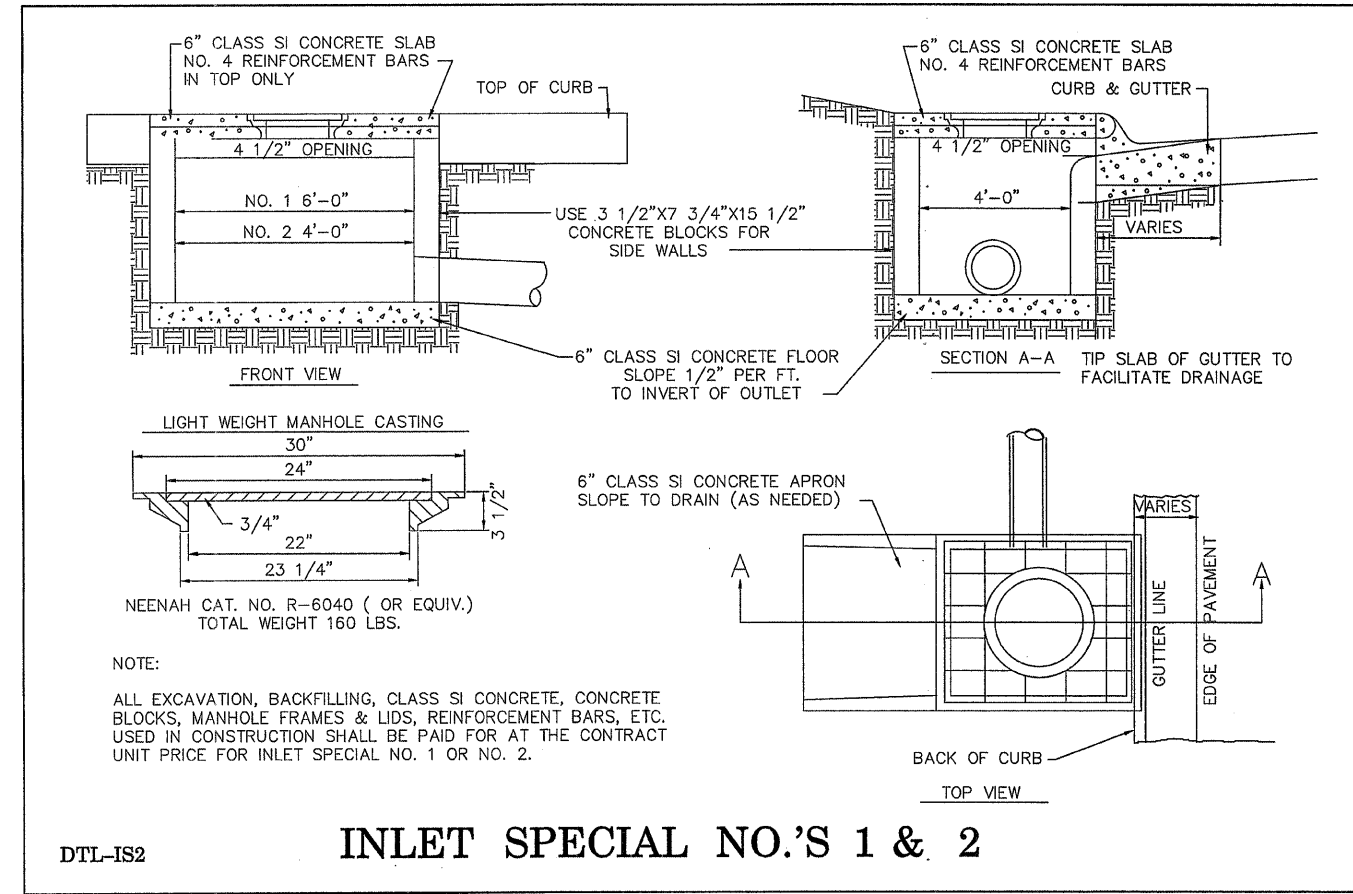
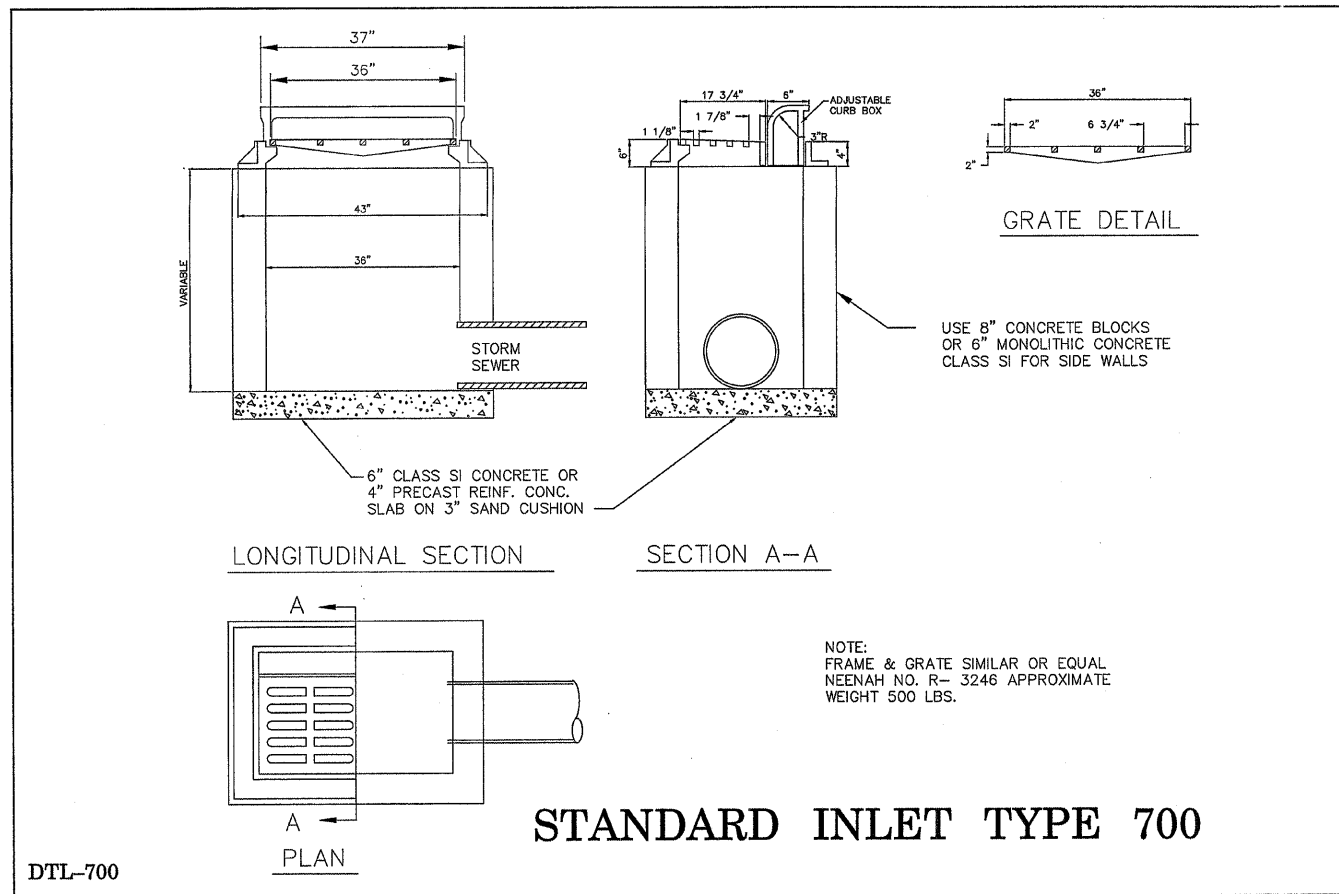
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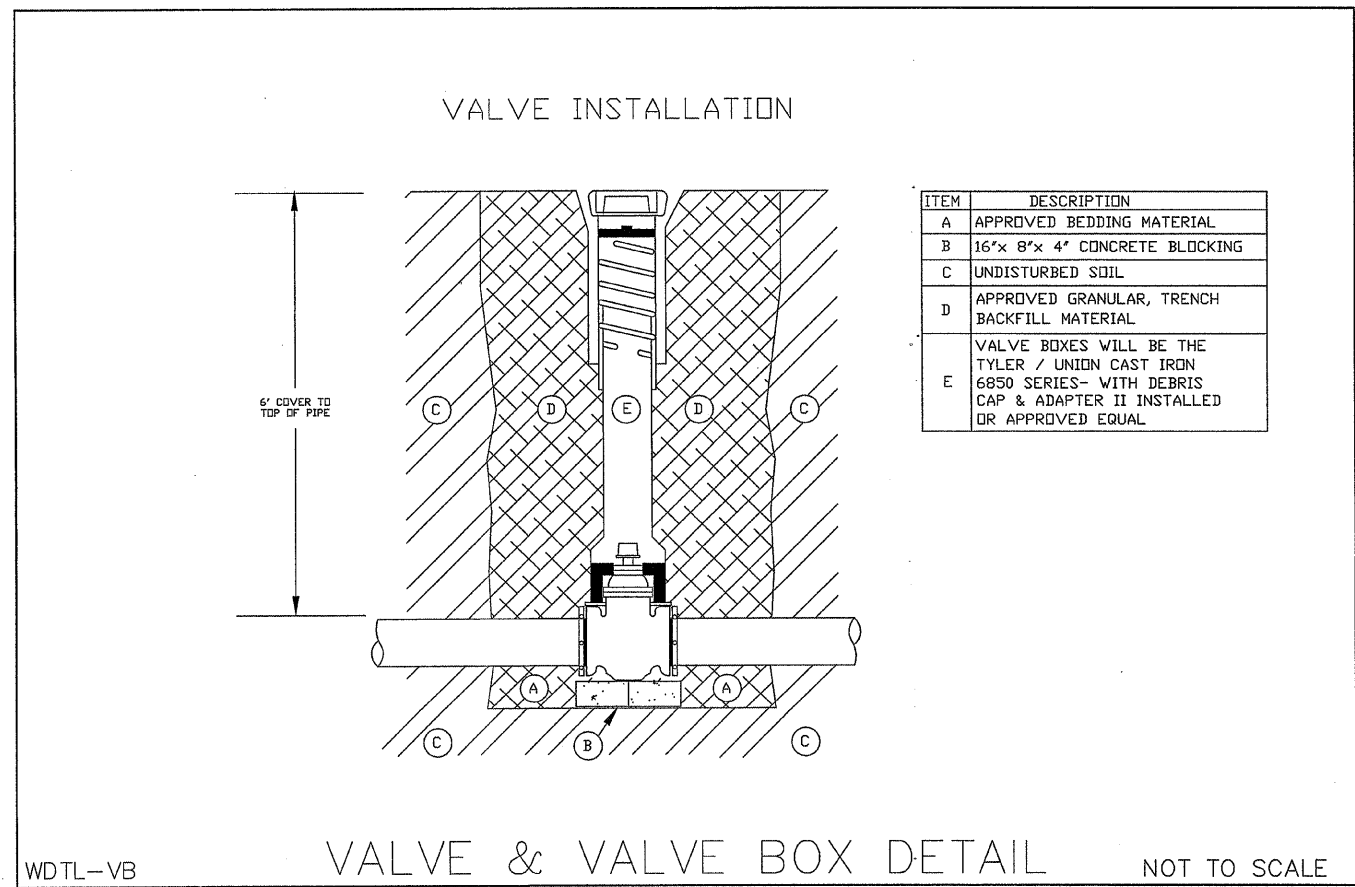
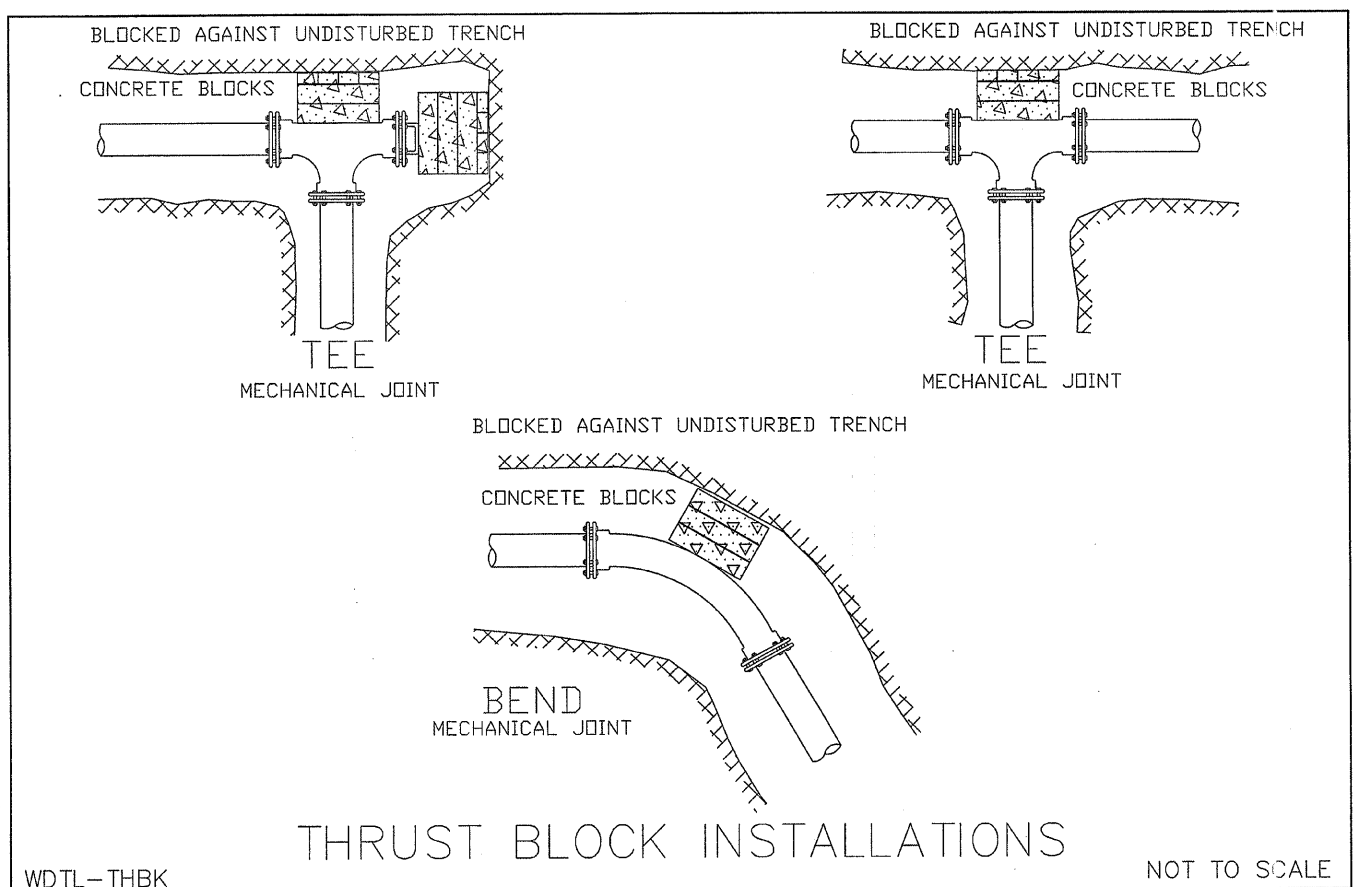
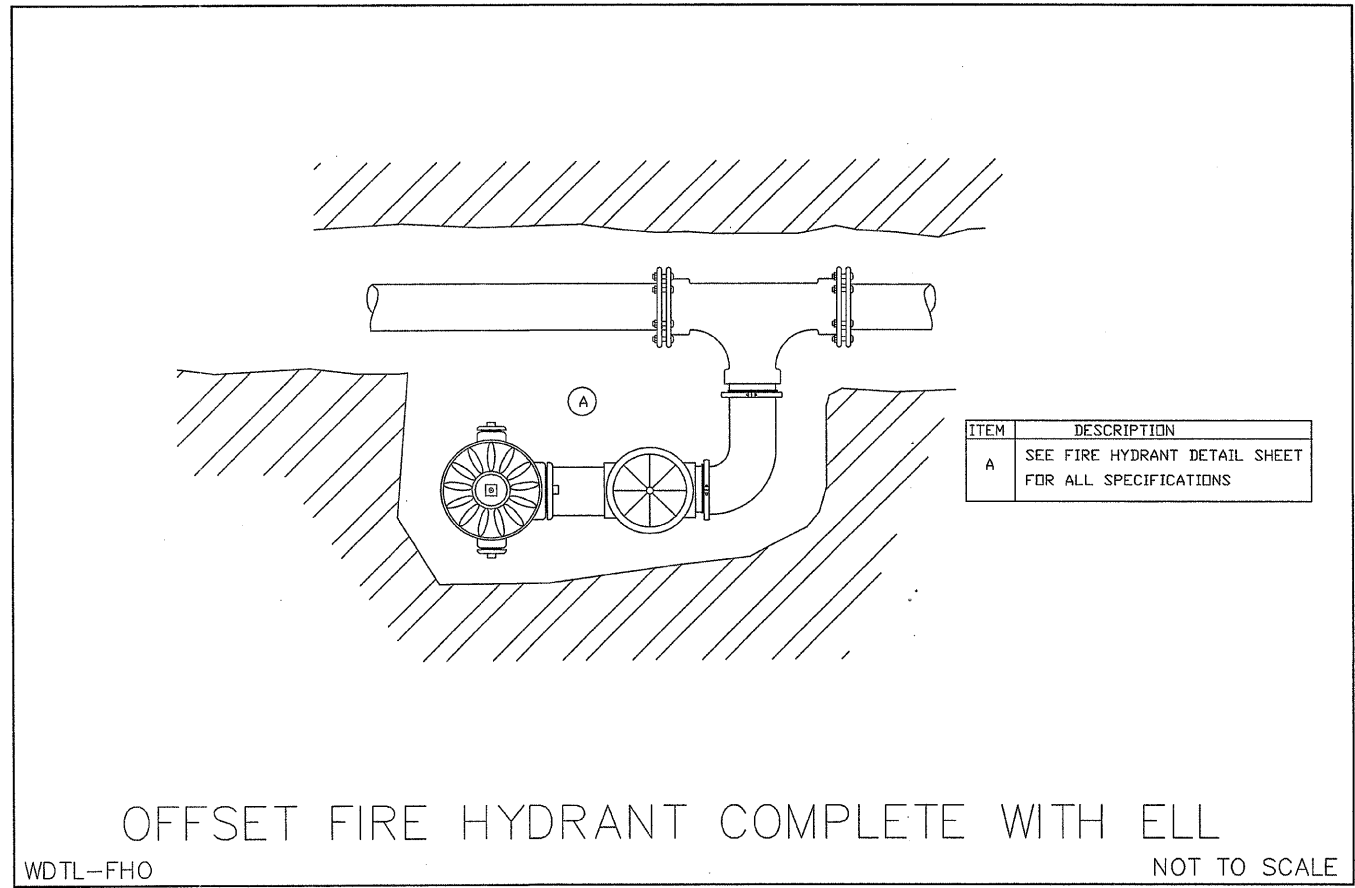
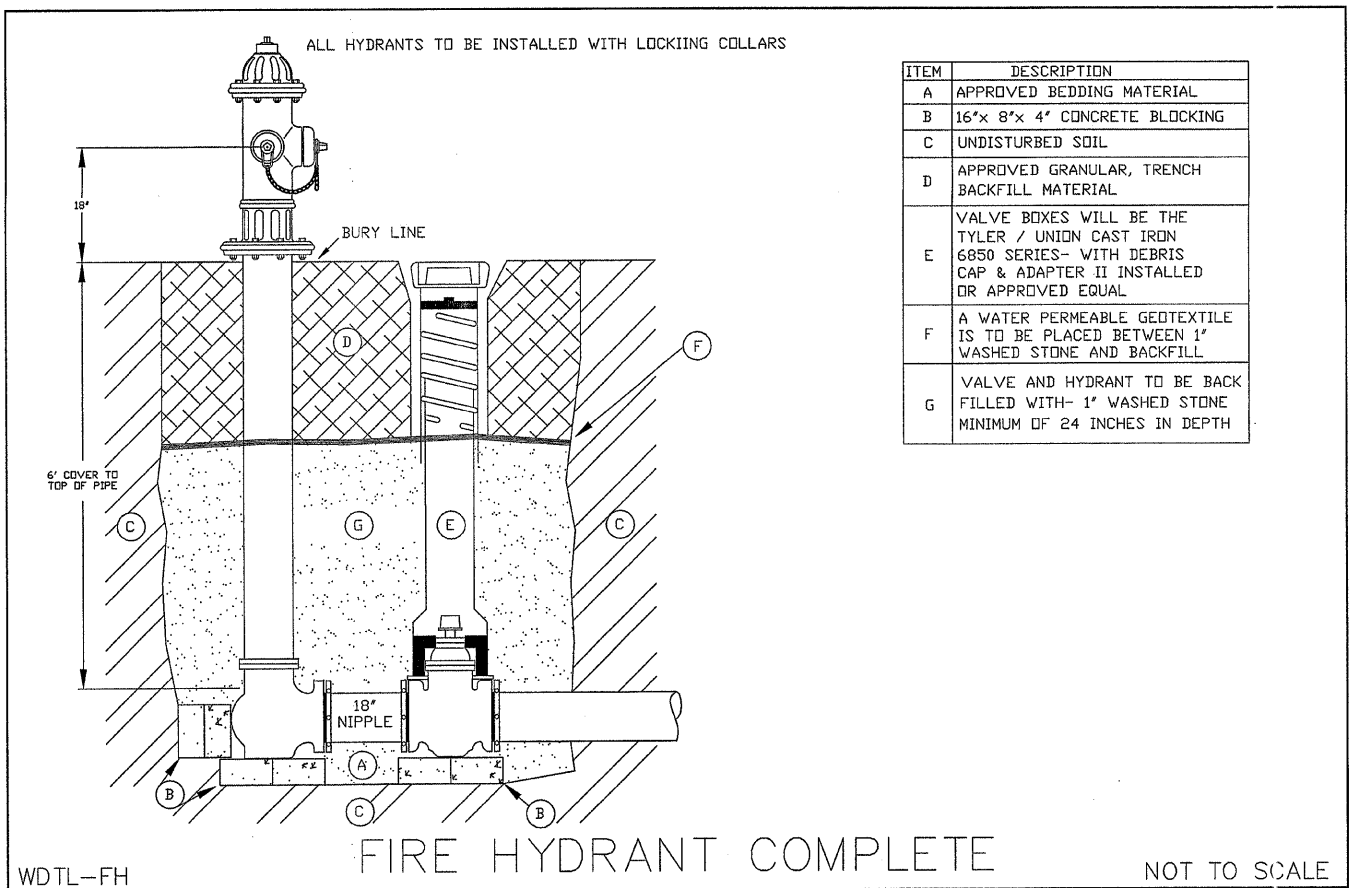
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PLOT DATE = #DATE#
 PLOT SCALE = #SCALE#
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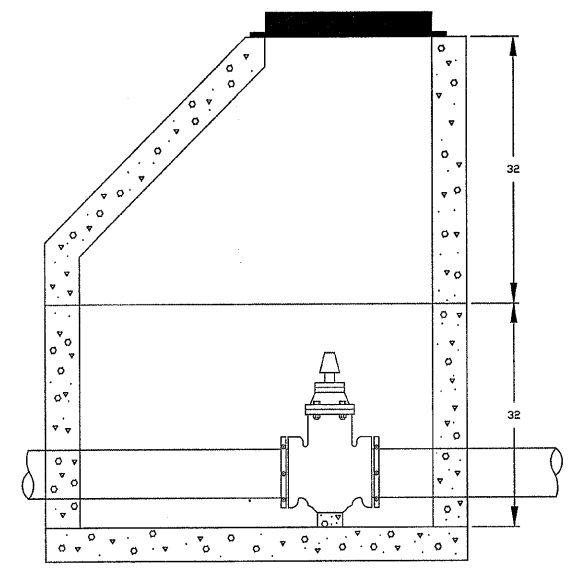
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| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 58 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT | | | | |



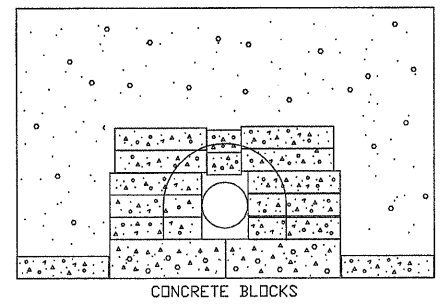


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| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAP734 | 98-00480-00-BR 03-00522-00-TL | WINNEBAGO | 62 | 60 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. _ | | ILLINOIS FED. AID PROJECT | | |

VALVE MANHOLE INSTALLATION



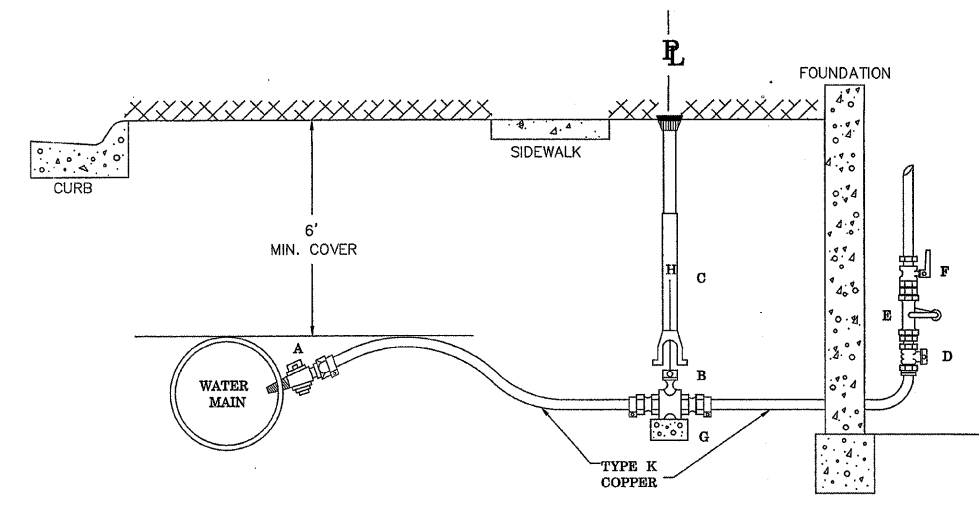
DOGHOUSE BLOCKING



VALVE & VAULT DETAIL NOT TO SCALE

WDTL-VV

ROCKFORD WATER DEPARTMENT

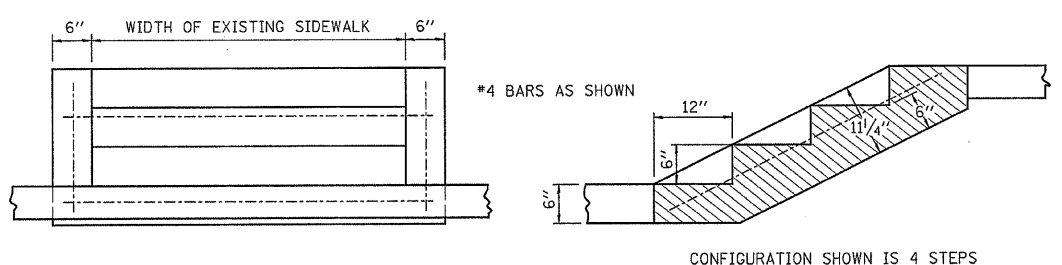


| ITEM | DESCRIPTION | SIZE |
|------|-----------------------|------------|
| A | CORPORATION STOP | 3/4"-2" |
| B | CURB STOP | 3/4"-2" |
| C | CURB BOX | EXTENDABLE |
| D | METER STOP | 3/4"-2" |
| E | METER SPREAD | 5/8"-2" |
| F | METER STOP HOUSE SIDE | 5/8"-2" |
| G | BRICK | CEMENT |
| H | ROD | 3/8" |

WATER SERVICE DETAIL NOT TO SCALE

WDTL-WSER

CONCRETE STEPS



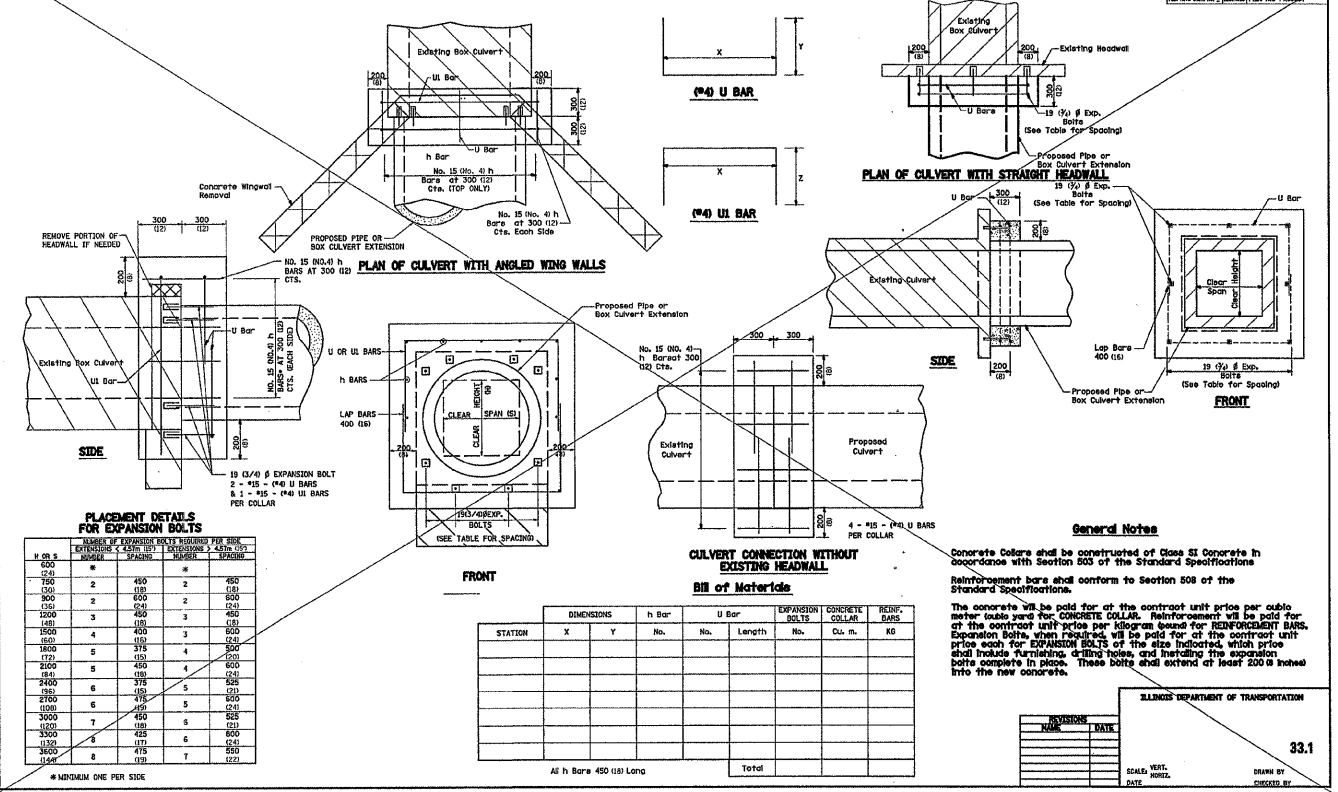
CONCRETE QUANTITY (CU. YD.)

| NO. OF STEPS | 2' WIDE | 3' WIDE | 4' WIDE | 5' WIDE |
|--------------|---------|---------|---------|---------|
| 1 | 0.17 | 0.22 | 0.27 | 0.32 |
| 2 | 0.28 | 0.36 | 0.44 | 0.53 |
| 3 | 0.39 | 0.51 | 0.62 | 0.73 |
| 4 | 0.51 | 0.65 | 0.80 | 0.94 |
| 5 | 0.62 | 0.80 | 0.97 | 1.15 |
| 6 | 0.73 | 0.94 | 1.15 | 1.35 |
| 7 | 0.85 | 1.08 | 1.32 | 1.56 |
| 8 | 0.96 | 1.23 | 1.50 | 1.77 |
| 9 | 1.07 | 1.37 | 1.68 | 1.98 |
| 10 | 1.19 | 1.52 | 1.85 | 2.18 |

CONCRETE STEPS SHALL HAVE 6" RISERS AND 12" TREADS. CLASS SI CONCRETE SHALL BE USED THROUGHOUT. CONCRETE STEPS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR CLASS SI CONCRETE OR CONCRETE STEPS WHICH PRICE SHALL INCLUDE THE COST OF FURNISHING AND INSTALLING THE REINFORCEMENT BARS.

70 B

CONCRETE COLLARS FOR PIPE OR BOX CULVERT EXTENSIONS



PLACEMENT DETAILS FOR EXPANSION BOLTS

| U OR S | NO. OF EXPANSION BOLTS | CONCRETE QUANTITY | REINFORCING BARS |
|--------|------------------------|-------------------|------------------|
| 400 | 2 | 400 | 2 |
| 450 | 2 | 450 | 2 |
| 500 | 2 | 500 | 2 |
| 550 | 2 | 550 | 2 |
| 600 | 2 | 600 | 2 |
| 650 | 2 | 650 | 2 |
| 700 | 2 | 700 | 2 |
| 750 | 2 | 750 | 2 |
| 800 | 2 | 800 | 2 |
| 850 | 2 | 850 | 2 |
| 900 | 2 | 900 | 2 |
| 950 | 2 | 950 | 2 |
| 1000 | 2 | 1000 | 2 |
| 1050 | 2 | 1050 | 2 |
| 1100 | 2 | 1100 | 2 |
| 1150 | 2 | 1150 | 2 |
| 1200 | 2 | 1200 | 2 |
| 1250 | 2 | 1250 | 2 |
| 1300 | 2 | 1300 | 2 |
| 1350 | 2 | 1350 | 2 |
| 1400 | 2 | 1400 | 2 |
| 1450 | 2 | 1450 | 2 |
| 1500 | 2 | 1500 | 2 |
| 1550 | 2 | 1550 | 2 |
| 1600 | 2 | 1600 | 2 |
| 1650 | 2 | 1650 | 2 |
| 1700 | 2 | 1700 | 2 |
| 1750 | 2 | 1750 | 2 |
| 1800 | 2 | 1800 | 2 |
| 1850 | 2 | 1850 | 2 |
| 1900 | 2 | 1900 | 2 |
| 1950 | 2 | 1950 | 2 |
| 2000 | 2 | 2000 | 2 |
| 2050 | 2 | 2050 | 2 |
| 2100 | 2 | 2100 | 2 |
| 2150 | 2 | 2150 | 2 |
| 2200 | 2 | 2200 | 2 |
| 2250 | 2 | 2250 | 2 |
| 2300 | 2 | 2300 | 2 |
| 2350 | 2 | 2350 | 2 |
| 2400 | 2 | 2400 | 2 |
| 2450 | 2 | 2450 | 2 |
| 2500 | 2 | 2500 | 2 |
| 2550 | 2 | 2550 | 2 |
| 2600 | 2 | 2600 | 2 |
| 2650 | 2 | 2650 | 2 |
| 2700 | 2 | 2700 | 2 |
| 2750 | 2 | 2750 | 2 |
| 2800 | 2 | 2800 | 2 |
| 2850 | 2 | 2850 | 2 |
| 2900 | 2 | 2900 | 2 |
| 2950 | 2 | 2950 | 2 |
| 3000 | 2 | 3000 | 2 |
| 3050 | 2 | 3050 | 2 |
| 3100 | 2 | 3100 | 2 |
| 3150 | 2 | 3150 | 2 |
| 3200 | 2 | 3200 | 2 |
| 3250 | 2 | 3250 | 2 |
| 3300 | 2 | 3300 | 2 |
| 3350 | 2 | 3350 | 2 |
| 3400 | 2 | 3400 | 2 |
| 3450 | 2 | 3450 | 2 |
| 3500 | 2 | 3500 | 2 |
| 3550 | 2 | 3550 | 2 |
| 3600 | 2 | 3600 | 2 |
| 3650 | 2 | 3650 | 2 |
| 3700 | 2 | 3700 | 2 |
| 3750 | 2 | 3750 | 2 |
| 3800 | 2 | 3800 | 2 |
| 3850 | 2 | 3850 | 2 |
| 3900 | 2 | 3900 | 2 |
| 3950 | 2 | 3950 | 2 |
| 4000 | 2 | 4000 | 2 |

BILL OF MATERIALS

| STATION | DIMENSIONS | | h Bar | U Bar | EXPANSION BOLTS | CONCRETE COLLAR | REINFORCING BARS |
|---------|------------|---|-------|-------|-----------------|-----------------|------------------|
| | X | Y | | | | | |
| | | | No. | No. | Length | Du. n. | KG |
| Total | | | | | | | |

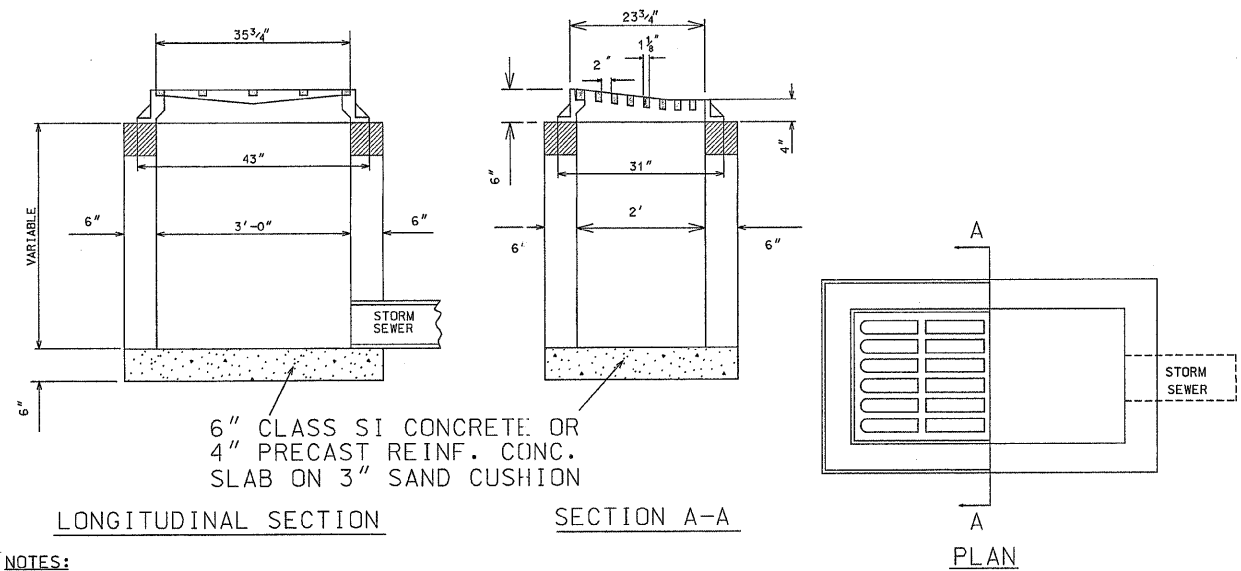
General Notes
Concrete Collars shall be constructed of Class SI Concrete in accordance with Section 503 of the Standard Specifications. Reinforcement bars shall conform to Section 508 of the Standard Specifications.

The concrete will be paid for at the contract unit price per cubic meter (cubic yard) for CONCRETE COLLAR. Reinforcement will be paid for at the contract unit price per kilogram (pound) based on REINFORCEMENT BARS. Expansion bolts, when required, will be paid for at the contract unit price each for EXPANSION BOLTS of the size indicated, which price shall include furnishing, drilling holes, and installing the expansion bolts complete in place. These bolts shall extend at least 200 mm into the new concrete.

ILLINOIS DEPARTMENT OF TRANSPORTATION
33.1
SCALE: AS SHOWN
DATE: _____
DRAWN BY: _____
CHECKED BY: _____

| | | | | |
|---------------------------|----------------------------------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAPT34 | 98-00480-00-BR 03-00922-00-TL | WINNEBAGO | 62 | 61 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ | | ILLINOIS FED. AID PROJECT | | |

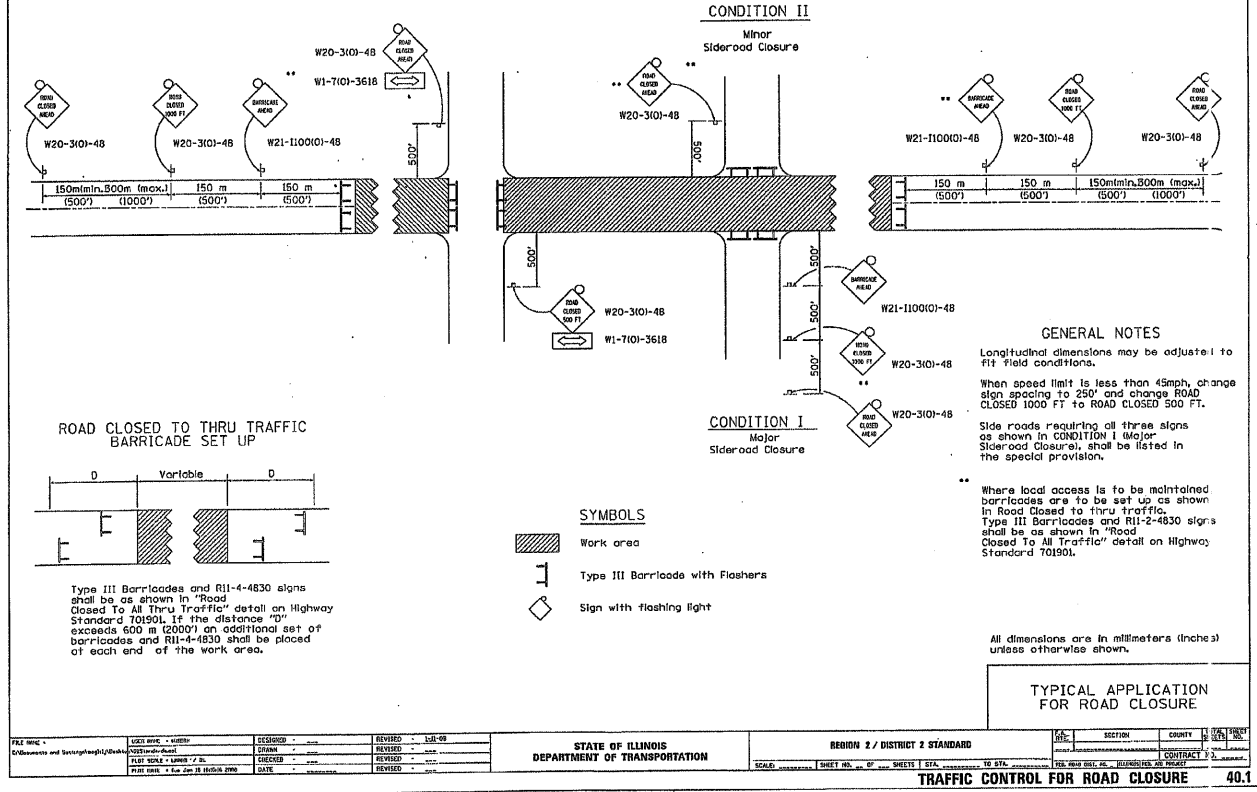
STANDARD INLET TYPE 700 (DRIVEWAY)



NOTES:
 THE INLET SHALL BE CAST IN PLACE OR PRECAST, EXCEPT AS NOTED HERE ON INLET TYPE B. SPECIAL WITH FRAME AND GRATE SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 602 OF THE STANDARD SPECIFICATIONS.
 THE CONTRACT UNIT PRICE EACH FOR INLET, TYPE 700 (DRIVEWAY) WITH FRAME AND GRATE SHALL INCLUDE THE COST OF FURNISHING AND INSTALLING THE INLET BOX, FRAME AND GRATE, AND FLOOR OF INLET WITH SAND CUSHION IF REQUIRED.
 CLASS SI CONCRETE OR PRECAST CONCRETE SHALL BE USED THROUGHOUT.
 THE SIDEWALLS MAY BE BUILT AS PRECAST SEGMENTAL SECTIONS.
 THE HEIGHT OF THE BOX MAY BE CONSTRUCTED 6" SHORT TO ALLOW FOR FIELD ADJUSTMENTS.
 THE WALL ADJUSTMENTS SHALL BE MADE WITH CONCRETE BUILDING BRICK OR CLASS SI CONCRETE.
 FRAME AND GRATE TO WEIGHT APPROXIMATELY 420 LBS.
 (NEENAH R-3290-A W/TYPE C GRATE OR EQUAL)

STANDARD INLET
 TYPE 700 (DRIVEWAY)
 STANDARD: WHA 644

TRAFFIC CONTROL FOR ROAD CLOSURE



TRAFFIC CONTROL FOR ROAD CLOSURE WITH SIDE ROAD WITHIN 150'

