

09-19-14 LETTING ITEM 078

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	1

CONTRACT NO. 63859  
+2=138

FOR INDEX OF SHEETS, SEE SHEET NO. 2

DESIGN DESIGNATION

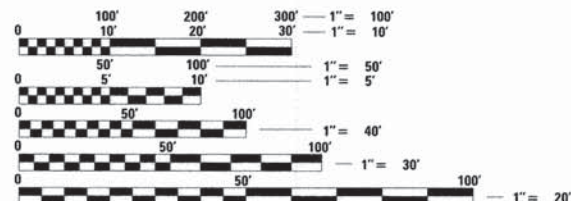
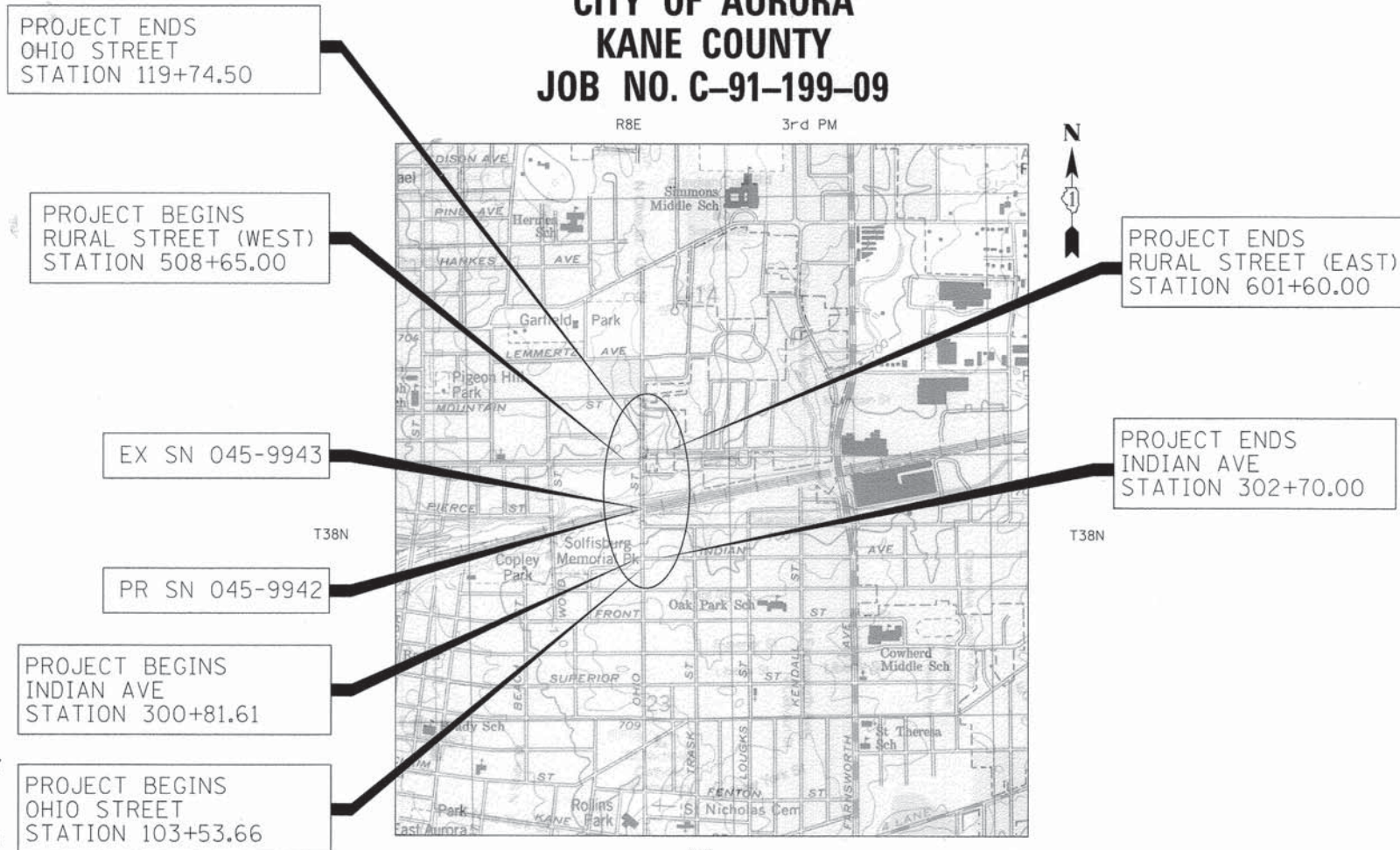
OHIO STREET  
16,100 (14) ARTERIAL 2.54 (PCC-20)

TRAFFIC DATA

LOCAL ROAD (URBAN)  
OHIO STREET  
DESIGN SPEED: 30 MPH  
POSTED SPEED: 30 MPH  
ADT: 12,200 (2005) 14,000 (2030)

PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY

OHIO STREET – INDIAN AVENUE TO RURAL STREET AND  
OVER THE BURLINGTON NORTHERN SANTA FE (BNSF)  
RAILROAD AND INDIAN CREEK  
BRIDGE REPLACEMENT  
SECTION 08-00278-00-BR  
PROJECT BRM-9003(165)  
CITY OF AURORA  
KANE COUNTY  
JOB NO. C-91-199-09



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

LOCATION MAP  
NOT TO SCALE

PROJECT LENGTH (GROSS / NET)  
OHIO STREET 1,620.84 FT (0.31 MILES)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED ON: April 22<sup>nd</sup>, 2014  
*Kenneth Schmitt*  
CITY OF AURORA, CITY ENGINEER

PASSED MAY 22, 2014  
*Christopher Holt*  
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID  
BASED ON LIMITED REVIEW  
May 22, 2014  
*John Fortman*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER



*Eric Neubaier* 1/29/2014  
ERIC S. NEUBAUER, P.E.  
NO. 062-059188  
EXP. DATE 11/30/15  
(ALL SHEETS EXCEPT NOS. 1 TO 39) 46 TO 71, 121 TO 136

*Math A. Santeford* 5/12/2014  
MATTHEW D. SANTEFORD, S.E.  
NO. 081-007244  
EXP. DATE 11/30/2014  
(SHEET NOS. 40 TO 45) 72 TO 120

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OF THE STATE OF ILLINOIS

FEDERAL AID PROGRAM ENGINEER: FAWAD AOUJEEL, P.E. (847) 705-4021, SCHAUMBURG, IL  
PUBLIC WORKS/ENGINEERING DIVISION: RICHARD A. MUNSON, P.E. CITY OF AURORA (630) 256-3256

CONTRACT NO. 63859



GENERAL NOTES:

- 1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED JAN 1, 2012, THE CITY OF AURORA STANDARD SPECIFICATIONS, THE DETAILS IN THESE PLANS AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.
2. NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET.
3. ALL UTILITIES, SCHOOL DISTRICTS, LOCAL POLICE, AND FIRE DEPARTMENT SHALL BE NOTIFIED BY CONTRACTOR PRIOR TO THE START OF CONSTRUCTION. SEE SHEET 3 FOR CONTACT LIST.
4. UNLESS AUTHORIZED BY THE ENGINEER, ALL EXISTING ACCESS POINTS SHALL BE MAINTAINED AT ALL TIMES BY THE CONTRACTOR.
5. DURING THE CONSTRUCTION, THE CONTRACTOR WILL BE REQUIRED, AT HIS EXPENSE, TO HAVE AVAILABLE A WATER TRUCK OR SIMILAR EQUIPMENT TO CONTROL DUST. IF NECESSARY, THE CONTRACTOR SHALL BE REQUIRED TO CONTROL DUST DURING NON-WORKING HOURS.

6. ALL EXCESS MATERIAL (BROKEN CONCRETE, CULVERT PIPE, WASTE ROADWAY EXCAVATIONS, SURPLUS MATERIAL FROM SEWER TRENCHES, ETC.) SHALL BE LEGALLY DISPOSED OF OUTSIDE LIMITS OF THE RIGHT-OF-WAY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SELECT DUMP SITES AND OBTAIN PERMISSION AND ALL NECESSARY PERMITS TO USE SUCH DUMP SITES. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL. (ITEM 20201200)

TREE REMOVAL CLEARING HEDGE REMOVAL
7. THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE PRESERVATION OF EXISTING TREES IS OF UTMOST IMPORTANCE TO THE CITY OF AURORA. ALL TREE PROTECTION, TREE REMOVAL, PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREE UNLESS SPECIFICALLY DIRECTED BY THE ENGINEER.

8. THE CONTRACTOR SHALL ERECT A TEMPORARY FENCE AROUND ALL TREES WITHIN THE CONSTRUCTION AREA TO ESTABLISH A "TREE PROTECTION ZONE" BEFORE ANY WORK BEGINS OR ANY MATERIAL IS DELIVERED TO THE JOBSITE. NO WORK IS TO BE PERFORMED (OTHER THAN ROOT PRUNING), MATERIALS STORED OR VEHICLES DRIVEN OR PARKED WITHIN THE "TREE PROTECTION ZONE". REMOVE PROTECTIVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED.

9. THE CONTRACTOR SHALL TAKE EXTRA CARE IN GRADING AND EXCAVATING NEAR TREES WHICH ARE NOT MARKED FOR REMOVAL SO AS NOT TO CAUSE INJURY TO THE ROOT SYSTEM OR TRUNKS. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.

10. TREE ROOT PRUNING IS TO BE USED ON EXISTING TREES TO PREVENT THE RIPPING UP OF ROOTS WHEN TRENCHING OR EXCAVATION IS WITHIN THE ROOT ZONE OF ADJACENT TREES TO REMAIN. SUPPLEMENTAL WATERING OF TREES SHOULD BEGIN IMMEDIATELY AFTER ROOT PRUNING OF THE TREES HAS OCCURRED.

11. PRUNE TREE LIMBS THAT MIGHT BE DAMAGED BY EQUIPMENT OPERATIONS AT LEAST ONE WEEK PRIOR TO THE START OF CONSTRUCTION BY A CERTIFIED ARBORIST. ANY TREE LIMBS THAT ARE BROKEN BY CONSTRUCTION EQUIPMENT AFTER THE INITIAL PRUNING MUST BE PRUNED CORRECTLY WITHIN 72 HOURS.

12. SUPPLEMENTAL WATERING IS SPECIFIED FOR TREES AND SHRUBS THAT WILL BE DISTURBED BY CONSTRUCTION BUT WILL REMAIN. NOTE THAT WATERING SHOULD BEGIN IMMEDIATELY AFTER ROOT PRUNING, TOP PRUNING OR OTHER CONSTRUCTION DISTURBANCE.

13. THE EMERALD ASH BORER IS AFFECTING MANY TREES WITHIN DISTRICT ONE WHICH AT THE TIME THE TREE SURVEY WAS COMPLETED THE TREE MAY HAVE BEEN HEALTHY AND SOUND. THE RESIDENT ENGINEER MAY REQUEST ADDITIONAL TREE(S) WITHIN THE RIGHT-OF-WAY TO BE REMOVED THAT WERE NOT ORIGINALLY SHOWN FOR REMOVAL IF THE TREE(S) IS UNSTRUCTURALLY SOUND, DISEASED, OR DEAD.

- 14. TREES NOT MARKED FOR REMOVAL SHALL BE CONSIDERED AS DESIGNATED TO BE SAVED AND SHALL BE PROTECTED UNDER THE PROVISIONS OF ARTICLE 201.05 OF THE STANDARD SPECIFICATIONS.
15. ALL LIMBS, BRANCHES, AND OTHER DEBRIS RESULTING FROM THIS WORK SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN EXPENSE OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY.

16. ALL CLEARING REMOVAL OF BUSHES, HEDGES AND TREES UNDER SIX (6) INCHES IN DIAMETER WILL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR EARTH EXCAVATION. (ITEM 20200100)

OVERHANGING LIMBS
17. OVERHANGING LIMBS ARE TO BE TRIMMED OR CUT OFF TO PROVIDE A MINIMUM VERTICAL CLEARANCE OF TWENTY (20) FEET FROM THE FINISHED SURFACE OF THE ROAD.

LIMB PRUNING SHALL BE PERFORMED UNDER THE SUPERVISION OF AN APPROVED TREE EXPERT AS STATED IN THE SPECIAL PROVISIONS AND SHALL BE UNDERTAKEN IN A TIMELY FASHION SO AS NOT TO INTERFERE WITH CONSTRUCTION.

ALL CUTS OVER ONE (1) INCH IN DIAMETER SHALL BE MADE FLUSH WITH THE NEXT LARGE BRANCH.

ALL LIMBS, BRANCHES, AND OTHER DEBRIS RESULTING FROM THIS WORK SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS EXPENSE OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY.

TOPSOIL
18. TOPSOIL SHALL BE PLACED TO A DEPTH OF FOUR (4) INCHES AND BE MEASURED IN SQUARE YARDS. (ITEM 21101615)

19. THE CROSS SECTIONS INDICATE TO FINISHED GRADE OF TOPSOIL.

20. THE LOCATIONS OF TOPSOIL STOCKPILED WITHIN THE RIGHT-OF-WAY MUST BE APPROVED BY THE ENGINEER. TOPSOIL STOCKPILE WILL REQUIRE EROSION CONTROL.

ROADWAY EXCAVATION
21. ALL EXISTING GRANULAR AND HOT-MIX ASPHALT TO BE REMOVED AND NOT PAID AS A SPECIFIC ITEM SHALL BE CONSIDERED EARTH EXCAVATION, AND WILL BE PAID FOR AT THE UNIT PRICE FOR EARTH EXCAVATION. THE CONTRACTOR WILL HAVE THE OPTION OF REMOVING THE EXISTING HOT-MIX ASPHALT BY GRINDING OR EXCAVATING THE MATERIAL.

22. THE CONTRACTOR SHALL NOT CROSS COMPLETED BASE COURSE OR EXISTING PAVEMENT, NOT SCHEDULED TO BE REMOVED WITH LOAD SCRAPERS OR TRACK EQUIPMENT.

STRUCTURES UTILITIES
23. THE STATION / OFFSET / ELEVATIONS NOTED FOR ALL DRAINAGE STRUCTURES LOCATED IN THE CURB LINE REFER TO THE POSITION OF THE ADJACENT PROPOSED EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE OFFSET NECESSARY FOR THE STRUCTURES TO SET THE FRAME AND GRATES IN THE PROPER LOCATION. ALL OTHER STRUCTURES ARE DIMENSIONED TO THE CENTER OF THE STRUCTURES UNLESS OTHERWISE NOTED. ELEVATIONS INDICATES RIM GRADES.

24. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING LOCAL AGENCIES MAINTAINING SANITARY SEWERS WATERMAINS TO VERIFY THE MATERIALS AND METHODS ALLOWED FOR THE ADJUSTMENT, RELOCATION, OR EXTENSION OF THE UTILITY INVOLVED.

25. THE LOCATIONS AND ELEVATION OF EXISTING UTILITIES ARE APPROXIMATE AND ARE PROVIDED BY THE OWNERS. THE EXACT LOCATIONS AND ELEVATION ARE TO BE VERIFIED BY THE CONTRACTOR.

26. ALL ADJUSTMENTS OR RECONSTRUCTIONS SHALL INCLUDE THE REMOVAL AND REPLACEMENT, AT THE CONTRACTOR'S EXPENSE, OF ALL UNSUITABLE MATERIAL TWO (2) FOOT INSIDE DIAMETER ADJUSTING RINGS.

27. ADJUSTMENT AND RECONSTRUCTION OF STRUCTURES MAINTAINED BY OTHER AGENCIES SHALL BE MADE TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY MAINTAINING THE SYSTEM OF THE STRUCTURES INVOLVED. THE FINAL STRUCTURE SHOULD NOT HAVE MORE THAN (8) INCHES OF ADJUSTMENT RINGS.

HOT-MIX ASPHALT
28. HOT-MIX ASPHALT SURFACE COURSE SHALL NOT BE PLACED UNTIL ALL EARTH EXCAVATION, TOPSOIL PLACEMENT, AGGREGATE BASE COURSE, AND HOT-MIX ASPHALT BINDER COURSE HAVE BEEN COMPLETED TO THE SATISFACTION OF THE ENGINEER.

29. SAW CUT CONSTRUCTION JOINTS SHALL BE PROVIDED AT PAVED COMMERCIAL OR PRIVATE ENTRANCES AND AT ALL SIDE ROAD. THE COST SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR HOT-MIX ASPHALT SURFACE COURSE.

30. THE MINIMUM COMPACTED THICKNESS OF ANY LIFT OF BINDER SHALL BE 2.25 INCHES.

31. BASE COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN BACKFILLED TO THE SATISFACTION OF THE ENGINEER.

TRAFFIC CONTROL AND PROTECTION
32. TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE TRAFFIC CONTROL PLAN AND SECTION 701 OF THE STANDARD SPECIFICATIONS AS AMENDED BY THE SPECIAL PROVISION FOR CONSTRUCTION ZONE TRAFFIC CONTROL (CHECK SHEET LRS3).

33. THE TYPE III BARRICADES ARE TO BE PLACED IN ACCORDANCE WITH STANDARD 701901 UNLESS AUTHORIZED BY THE ENGINEER TO USE AN ALTERNATE ARRANGEMENT.

34. EXISTING TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE REMOVED BY THE CONTRACTOR AFTER TRAFFIC CONTROL REQUIREMENTS ARE MET, OR AS AUTHORIZED BY THE ENGINEER, AND DELIVERED TO THE CITY OF AURORA AS DIRECTED BY THE ENGINEER. ANY SIGNS OR DEVICES LEFT IN PLACE ARE TO BE RELOCATED, MAINTAINED AND PROTECTED FROM DAMAGE BY THE CONTRACTOR AT NO EXPENSE TO THE CITY OF AURORA.

35. ANY DROP OFF GREATER THAN THREE (3) INCHES BUT LESS THAN SIX (6) INCHES, WITHIN EIGHT (8) FEET OF THE PAVEMENT EDGE, SHALL BE PROTECTED BY TYPE I OR TYPE II BARRICADES, DRUM OR VERTICAL PANELS WITH MONODIRECTIONAL STEADY-BURN LIGHTS AT 100 FOOT CENTER TO CENTER SPACING. IF THE DROP OFF WITHIN EIGHT (8) FEET OF THE PAVEMENT EDGE EXCEEDS SIX (6) INCHES, THE BARRICADES, DRUMS OR VERTICAL PANELS MENTIONED ABOVE SHALL BE PLACED AT FIFTY (50) FOOT CENTER TO CENTER SPACING. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOP OF THE BARRICADE IS IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF STANDARD 701901.

36. TYPE I OR TYPE II BARRICADES WITH TWO-WAY FLASHING LIGHTS SHALL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, TRANSVERSE PAVEMENT JOINTS, MATERIAL OR EQUIPMENT WITHIN THE RIGHT-OF-WAY (NUMBER AND SPACING DEPENDS ON THE CONDITIONS) AND AT LOCATIONS DESIGNATED BY THE ENGINEER OR LOCAL LAW ENFORCEMENT AGENCIES.

37. TYPE I, II AND / OR III BARRICADES WITH TWO-WAY FLASHING LIGHTS WILL BE REQUIRED TO GUIDE TRAFFIC AWAY FROM PAVEMENT AREAS CLOSED FOR CONSTRUCTION.

38. THE COST OF SUPPLYING, ERECTING, AND MAINTAINING BARRICADES, WARNING LIGHTS, AND SIGNS WILL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR TRAFFIC CONTROL AND PROTECTION. (SPECIAL). (ITEM X7010216)

CONSTRUCTION SEQUENCE
39. PAVEMENT CONSTRUCTION SHALL NOT BEGIN UNTIL A LEAST ONE MONTH AFTER FINAL COMPLETION OF THE NEW EMBANKMENT TO REDUCE THE AMOUNT OF DIFFERENTIAL SETTLEMENT.

RAILROAD FLAGGERS
40. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COORDINATE WITH THE BNSF RAILROAD WHENEVER CONSTRUCTION ACTIVITY IS WITHIN 25 FEET OF THE RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY THE BNSF RAILROAD TO MONITOR ON-COMING TRAIN TRAFFIC, AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD RIGHT-OF-WAY MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05.

NOTE: BOXED ITEMS ARE INCLUDED IN THE COST OF THE CONTRACT.

SHEET NO. INDEX OF SHEETS

Table listing sheet numbers and corresponding titles such as TITLE SHEET, GENERAL NOTES, INDEX OF SHEETS AND LIST OF STANDARDS, SUMMARY OF QUANTITIES, EARTHWORK SCHEDULE OF QUANTITIES, SCHEDULE OF DRAINAGE QUANTITIES, TYPICAL SECTIONS, ALIGNMENT AND TIES, REMOVAL PLANS, ROADWAY PLAN AND PROFILES, INTERSECTION PAVING PLANS, MAINTENANCE OF TRAFFIC-DETOUR ROUTE PLAN, DRAINAGE AND UTILITIES PLAN AND PROFILE, SANITARY SEWER PLAN AND PROFILE, WATER MAIN PLAN AND PROFILE, FOX METRO WRD DETAILS, RIPRAP DETAIL, HEADWALL 200 DETAILS, DRAINAGE STRUCTURES, NO. 1 DETAILS 1, DRAINAGE STRUCTURES, NO. 1 DETAILS 2, DRAINAGE STRUCTURES, NO. 2 DETAILS 1, DRAINAGE STRUCTURES, NO. 2 DETAILS 2, JUNCTION BOX 202 AND 207 TEMPORARY SHEET PILING, EROSION AND SEDIMENT CONTROL PLAN, LANDSCAPING, SIGNING, AND PAVEMENT MARKING PLAN, IDOT PLAT OF HIGHWAYS, DISTRICT ONE DETAILS, CONDUIT SUPPORT ATTACHED TO BRIDGE DECK DETAIL, GENERAL PLAN AND ELEVATION, GENERAL DATA, FOUNDATION PLAN, TOP OF SLAB ELEVATIONS, TOP OF SOUTH AND NORTH APPROACH SLAB ELEVATIONS, DECK PLAN AND CROSS SECTION, SUPERSTRUCTURE DETAIL AND DIAPHRAGM DETAILS, BRIDGE APPROACH SLAB DETAILS, BRIDGE FENCE RAILING, PARAPET MOUNTED AND PARAPET RAILING DETAILS, FRAMING PLAN, STRUCTURAL STEEL DETAILS, BEARING DETAILS, SOUTH ABUTMENT, NORTH ABUTMENT, PIER 1 AND PIER 2, HP PILE DETAILS, BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS, BORING LOGS, GENERAL PLAN AND ELEVATION SOUTH RETAINING WALLS, GENERAL PLAN AND ELEVATION NORTH RETAINING WALLS, GENERAL DATA RETAINING WALLS, MOMENT SLAB PLAN AND ELEVATION SOUTHEAST RETAINING WALL, MOMENT SLAB PLAN AND ELEVATION NORTHEAST RETAINING WALL, RETAINING WALL DETAILS, TRANSMISSION MAIN PROTECTION SYSTEM, BICYCLE RAILING DETAILS, BORING LOGS RETAINING WALLS, CROSS SECTIONS.

HIGHWAY STANDARDS

Table listing standard numbers and descriptions such as STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS, AREAS OF REINFORCEMENT BARS, DECIMAL OF AN INCH AND OF A FOOT, TEMPORARY EROSION CONTROL SYSTEMS, PCC PAVEMENT ROUNDOUTS, BRIDGE APPROACH PAVEMENT CONNECTOR, PERPENDICULAR CURB RAMPS FOR SIDEWALKS, CORNER PARALLEL CURB RAMPS FOR SIDEWALKS, MID-BLOCK CURB RAMPS FOR SIDEWALKS, DEPRESSED CORNER FOR SIDEWALKS, NAME PLATE FOR BRIDGES, PRECAST REINFORCED CONCRETE FLARED END SECTION, TRAVERSABLE PIPE GRATE, CATCH BASIN, TYPE A, INLET, TYPE A, MANHOLE, TYPE A, MANHOLE, TYPE A, 9' (2.7 m) DIAMETER, VALVE VAULT, TYPE A, MANHOLE STEPS, FRAME AND LIDS, TYPE 1, GRATE, TYPE 8, FRAME AND GRATE, TYPE 11V, CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER, STEEL PLATE BEAM GUARDRAIL, SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GAURDRAIL TERMINALS, TRAFFIC BARRIER TERMINAL, TYPE 2, TRAFFIC BARRIER TERMINAL, TYPE 5, TRAFFIC BARRIER TERMINAL, TYPE 6, REFLECTOR AND TERMINAL MARKER PLACEMENT, REFLECTOR MARKER AND MOUNTING DETAILS, CHAIN LINK FENCE, OFF-RD OPERATIONS, 2L, 2W 15 (4.5m) TO 24' (600mm) FROM PAVEMENT EDGE, OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY, LANE CLOSURE, 2L, 2W SHORT TIME OPERATIONS, URBAN LANE CLOSURE 2L, 2W, UNDIVIDED, SIDEWALK, CORNER OR CROSSWALK CLOSURE, TRAFFIC CONTROL DEVICES, SIGN PANEL MOUNTING DETAILS, SIGN PANEL ERECTION DETAILS, METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS, TELESCOPING STEEL SIGN SUPPORT, APPLICATIONS OF TYPES A AND B METAL POSTS (FOR SIGNS & MARKERS), TYPICAL PAVEMENT MARKINGS.

DISTRICT DETAILS STANDARDS table with columns for item ID (BD-01, BD-02, BD-32, BD-34, TC-10, TC-13, TC-26) and description (DRIVEWAY DETAILS - DISTANCE BETWEEN ROW AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m), DRIVEWAY DETAILS DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m), BUTT JOINT AND HMA TAPER DETAILS, DETAILS FOR DEPRESSED CURB & GUTTER AND SHOULDER TREATMENT AT TBT TY 1 SPL, TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS, DISTRICT ONE TYPICAL PAVEMENT MARKINGS, DRIVEWAY ENTRANCE SIGNING).

Table with columns: DATE, BY, CHECKED, NO. (for PLAN, PROFILE, NOTE BOOK).

Table with columns: DATE, BY, CHECKED, NO. (for PROFILE, NOTE BOOK).

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, INDEX OF SHEETS AND LIST OF STANDARDS

Table with columns: F.A.P. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO., CONTRACT NO. (values: 4087, 08-00278-00-BR, KANE, 136, 2, CONTRACT NO. 63859).

SCALE: N.T.S. SHEET NO. 2 OF 136 SHEETS STA. TO STA.

[ILLINOIS] FED. AID PROJECT



**EROSION CONTROL NOTES**

41. ALL SEDIMENT AND EROSION CONTROL MEASURES WILL BE INSTALLED PER IDOT STANDARD 208881 OR AS SPECIFIED HEREIN AND PAID FOR IN ACCORDANCE WITH SECTION 280 OF THE STANDARD SPECIFICATIONS.
42. EROSION CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH THE SEQUENCE OF STAGE CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT A DETAILED SCHEDULE FOR APPROVAL.
43. SEDIMENT AND EROSION CONTROL DEVICES SHALL BE FUNCTIONAL BEFORE THE PROJECT SITE IS OTHERWISE DISTURBED.
44. IN DISTURBED AREAS WHERE WORK IS COMPLETED, FINAL GRADING AND PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS. IN AREA WHERE WORK HAS TEMPORARILY CEASED OR WILL NOT BE COMPLETED PRIOR TO THE FALL SEEDING RESTRICTION, TEMPORARY STABILIZATION (CLASS 7 SEEDING) SHALL BE COMPLETED BY THE 14TH DAY AFTER WORK HAS CEASED.
45. WATER PUMPED OR OTHERWISE DISCHARGE FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED WITH FILTER BAGS, OR OTHER METHOD APPROVED BY THE ENGINEER.
46. GRAVEL ROADS, ACCESS DRIVES, PARKING AREA OF SUFFICIENT WIDTH AND LENGTH, AND VEHICLE WASH DOWN FACILITIES IF NECESSARY, SHALL BE PROVIDED TO PREVENT THE DEPOSIT OF SOIL FROM BEING TRACKED ONTO PUBLIC OR PRIVATE ROADWAYS. ANY SOIL REACHING A PUBLIC OR PRIVATE ROADWAY SHALL BE REMOVED BEFORE THE END OF EACH WORKDAY OR AS NEEDED.
47. ALL EROSION CONTROL MEASURES SHALL BE INSPECTED BY THE CONTRACTOR WEEKLY AND WITHIN 24 HOURS OF ANY STORM EXCEEDING 1/2 INCH PRECIPITATION. DAMAGED AND INEFFECTIVE EROSION CONTROL MEASURES SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR WITHIN 72 HOURS OF NOTIFICATION BY THE ENGINEER OR THE CITY OF AURORA.
48. THE CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SEDIMENT AND EROSION CONTROL MEASURES ARE OPERATIONAL.
49. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND / OR PERMANENT VEGETATIVE COVER FOR PROPER EROSION AND SEDIMENT CONTROL. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND / OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREA TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
50. STOCKPILING OF SOIL AND OTHER BUILDING MATERIALS TO REMAIN IN PLACE MORE THAN THREE (3) DAYS SHALL BE FURNISHED WITH EROSION AND SEDIMENT CONTROL MEASURES (I.E. PERIMETER EROSION BARRIER). STOCKPILES TO REMAIN IN PLACE FOR 30 DAYS OR MORE SHALL RECEIVE TEMPORARY SEEDING.

NOTE: BOXED ITEMS ARE INCLUDED IN THE COST OF THE CONTRACT.

**CONTACTS**

AURORA TOWNSHIP HIGHWAY COMMISSIONER  
 JOHN SHOEMAKER  
 630-892-0266  
 200 BUTTERFIELD ROAD  
 NORTH AURORA, IL 60542-1316

NICOR GAS  
 CONSTANCE LANE  
 630-388-3830  
 1844 FERRY ROAD  
 NAPERVILLE, IL 60563

AT&T OSP ENGINEER  
 LEGAL MANDATE GROUP  
 BOB ELSINGA  
 630-573-5452  
 1000 COMMERCE DRIVE  
 OAK BROOK, IL 60523

COMED  
 ANA KUNZE  
 630-437-2129  
 2 LINCOLN CENTER-6TH FL  
 OAK BROOK TERRACE, IL 60181

FOX METRO WATER RECLAMATION DISTRICT  
 ZACHERY BONESZ  
 630-301-6805  
 682 STATE ROUTE 31  
 OSWEGO, IL 60543

COMCAST CABLE  
 MARTHA GIERAS  
 630-600-6352  
 688 INDUSTRIAL DRIVE  
 ELMHURST, IL 60126

AURORA POLICE DEPARTMENT  
 MAIN HEADQUARTERS  
 630-256-5000  
 1200 EAST INDIAN TRAIL ROAD  
 AURORA, IL 60505

PACE SUBURBAN BUS SERVICE  
 (Specify Relevant Department)  
 847-364-7223  
 550 W. ALGONQUIN RD.  
 ARLINGTON HEIGHTS, IL 60005

CITY OF AURORA  
 630-256-5636  
 44 E. DOWNER PLACE  
 AURORA, IL 60505

EAST AURORA SCHOOL DISTRICT 131  
 630-299-5550  
 417 FIFTH STREET  
 AURORA, IL 60505

SIMMONS MIDDLE SCHOOL  
 630-299-4150  
 1130 SHEFFER RD.  
 AURORA, IL 60505

HERMES ELEMENTARY SCHOOL  
 630-299-8200  
 1000 JUNGLES AVE.  
 AURORA, IL 60505

ROLLINS ELEMENTARY SCHOOL  
 630-299-5480  
 950 KANE STREET  
 AURORA, IL 60505

OAK PARK ELEMENTARY SCHOOL  
 630-299-8250  
 1200 FRONT STREET  
 AURORA, IL 60505

COWHERD MIDDLE SCHOOL  
 630-299-5900  
 411 N. FARNSWORTH  
 AURORA, IL 60505

AURORA FIRE DEPARTMENT  
 CENTRAL STATION  
 630-256-4000  
 75 N. BROADWAY  
 AURORA, IL 60505

DATE	BY	REVISIONS	DATE
		NO.	
DATE	BY	PLAN	DATE
		NO.	

DATE	BY	REVISIONS	DATE
		NO.	
DATE	BY	PROFILE	DATE
		NO.	

FILE NAME =	USER NAME = a_jpondexter	DESIGNED - MLB	REVISED -
G:\CHI1\0130\Road\Sheets\10130-SHT-GenNotes.dgn		DRAWN - MLB	REVISED -
		CHECKED - MWS	REVISED -
		DATE - 5/12/14	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES, INDEX OF SHEETS  
 AND LIST OF STANDARDS**

SCALE: N.T.S. SHEET NO. 3 OF 136 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	3
			CONTRACT NO. 63859	
ILLINOIS FED. AID PROJECT				



PLAN  
 SURVEYED BY DATE  
 ALTERNATE CHECKED  
 NOTE BOOK NO. OF P.F. CHECKED  
 PLOTTED BY DATE  
 PLOT SCALE: N/A  
 PLOT DATE: 6/27/2014

PROFILE  
 SURVEYED BY DATE  
 GRADES CHECKED  
 NOTE BOOK NO. OF P.F. CHECKED  
 PLOTTED BY DATE  
 PLOT SCALE: N/A  
 PLOT DATE: 6/27/2014

CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				ROADWAY		BRIDGE
				80% FED 20% LOCAL AGENCY 0004	100% LOCAL AGENCY	80% FED 20% LOCAL AGENCY 0011
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	653	653		
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	123	123		
20101000	TEMPORARY FENCE	FOOT	675	675		
20101200	TREE ROOT PRUNING	EACH	75	75		
20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	50	50		
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	25	25		
20200100	EARTH EXCAVATION	CU YD	2791	2221		570
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	452	452		
20400800	FURNISHED EXCAVATION	CU YD	5394	5394		
20800150	TRENCH BACKFILL	CU YD	1945	1945		
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	5508	5508		
21301072	EXPLORATION TRENCH 72" DEPTH	FOOT	100	100		
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	75	75		
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	75	75		
25000750	MOWING	ACRE	17.5	17.5		
25100630	EROSION CONTROL BLANKET	SO YD	4845	4845		
25100635	HEAVY DUTY EROSION CONTROL BLANKET	SO YD	2050	2050		
25200110	SODDING, SALT TOLERANT	SO YD	6358	6358		
25200200	SUPPLEMENTAL WATERING	UNIT	314	314		
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	750	750		
28000305	TEMPORARY DITCH CHECKS	FOOT	24	24		
28000400	PERIMETER EROSION BARRIER	FOOT	3210	3210		
28000500	INLET AND PIPE PROTECTION	EACH	6	6		
28000510	INLET FILTERS	EACH	29	29		
28100111	STONE RIPRAP, CLASS A6	SO YD	232	232		
28200200	FILTER FABRIC	SO YD	257	257		
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	50	50		
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SO YD	6223	6223		
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SO YD	1390	1390		
35101800	AGGREGATE BASE COURSE, TYPE B 6"	SO YD	970	970		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	209	209		
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	2765	2765		
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	623	623		
42001165	BRIDGE APPROACH PAVEMENT	SO YD	200	200		
42001300	PROTECTIVE COAT	SO YD	1122	1122		
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SO YD	562	562		
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SO YD	128	128		
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	2924	2924		
42400300	PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH	SO FT	3358	3358		
42400800	DETECTABLE WARNINGS	SO FT	180	180		
44000100	PAVEMENT REMOVAL	SO YD	5269	5269		
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	700	700		
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1501	1501		
44000600	SIDEWALK REMOVAL	SO FT	10048	10048		

CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				ROADWAY		BRIDGE
				80% FED 20% LOCAL AGENCY 0004	100% LOCAL AGENCY	80% FED 20% LOCAL AGENCY 0011
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SO YD	79	79		
48203021	HOT-MIX ASPHALT SHOULDERS, 6"	SO YD	60	60		
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1			1
50105220	PIPE CULVERT REMOVAL	FOOT	62	62		
50200100	STRUCTURE EXCAVATION	CU YD	2937			2937
50300225	CONCRETE STRUCTURES	CU YD	429			429
50300255	CONCRETE SUPERSTRUCTURE	CU YD	1054			1054
50300260	BRIDGE DECK GROOVING	SO YD	1348			1348
50300285	FORM LINER TEXTURED SURFACE	SO FT	6476			6476
50300300	PROTECTIVE COAT	SO YD	2944			2944
50500105	FURNISHING AND ERECTING STRUCTURAL STEEL	L SUM	1			1
50500505	STUD SHEAR CONNECTORS	EACH	11340			11340
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	272800			272800
50800515	BAR SPLICERS	EACH	80			80
50901720	BICYCLE RAILING	FOOT	430			430
50901730	BRIDGE FENCE RAILING	FOOT	689			689
50901750	PARAPET RAILING	FOOT	738			738
51201800	FURNISHING STEEL PILES HP14X73	FOOT	3028			3028
51202305	DRIVING PILES	FOOT	3028			3028
51203800	TEST PILE STEEL HP14X73	EACH	4			4
51500100	NAME PLATES	EACH	1			1
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	10			10
52100020	ELASTOMERIC BEARING ASSEMBLY, TYPE II	EACH	10			10
52100505	ANCHOR BOLTS, 5/8"	EACH	20			20
52100510	ANCHOR BOLTS, 3/4"	EACH	40			40
52100520	ANCHOR BOLTS, 1"	EACH	20			20
54215472	CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 72"	EACH	1	1		
54260311	TRAVERSABLE PIPE GRATE	FOOT	84	84		
54261278	CONCRETE END SECTION, STANDARD 542001, 78", 1:2	EACH	1	1		
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	283	283		
550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	53	53		
550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	106	106		
550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	405	405		
550A0500	STORM SEWERS, CLASS A, TYPE 2 60"	FOOT	48	48		
550A0520	STORM SEWERS, CLASS A, TYPE 2 72"	FOOT	259	259		
550A0530	STORM SEWERS, CLASS A, TYPE 2 78"	FOOT	313	313		
55100300	STORM SEWER REMOVAL 8"	FOOT	16	16		
55100500	STORM SEWER REMOVAL 12"	FOOT	235	235		
55100900	STORM SEWER REMOVAL 18"	FOOT	90	90		
55101200	STORM SEWER REMOVAL 24"	FOOT	348	348		
55101800	STORM SEWER REMOVAL 42"	FOOT	33	33		
56103300	DUCTILE IRON WATER MAIN 12"	FOOT	166	166		
56103350	DUCTILE IRON WATER MAIN 14"	FOOT	240	240		
56103400	DUCTILE IRON WATER MAIN 16"	FOOT	430	430		



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 CHECKED BY \_\_\_\_\_  
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CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				ROADWAY		BRIDGE
				80% FED 20% LOCAL AGENCY 0004	100% LOCAL AGENCY 0043	80% FED 20% LOCAL AGENCY 0011
* 56400300	FIRE HYDRANTS TO BE ADJUSTED	EACH	3	3		
* 56400600	FIRE HYDRANTS	EACH	1	1		
* 59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	81			81
60104500	PIPE DRAINS, CORRUGATED STEEL 48"	FOOT	5	5		
60200805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	5	5		
60201100	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE IIV FRAME AND GRATE	EACH	15	15		
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	4	4		
60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	4	4		
60223800	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1		
60224459	MANHOLES, TYPE A, 8'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1		
60224469	MANHOLES, TYPE A, 9'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3	3		
60224470	MANHOLES, TYPE A, 9'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	3	3		
60236825	INLETS, TYPE A, TYPE IIV FRAME AND GRATE	EACH	11	11		
* 60248900	VALVE VAULTS, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	5	5		
* 60266600	VALVE BOXES TO BE ADJUSTED	EACH	8	8		
60500040	REMOVING MANHOLES	EACH	3	3		
60500050	REMOVING CATCH BASINS	EACH	4	4		
60500060	REMOVING INLETS	EACH	7	7		
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	2577	2577		
* 63000005	STEEL PLATE BEAM GUARDRAIL, TYPE B	FOOT	112.5	112.5		
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	1	1		
* 63100070	TRAFFIC BARRIER TERMINAL, TYPE 5	EACH	1	1		
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	1	1		
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	3	3		
* 63200310	GUARDRAIL REMOVAL	FOOT	803	803		
* 66400305	CHAIN LINK FENCE, 6'	FOOT	207	207		
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	536	536		
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12		
67100100	MOBILIZATION	L SUM	1	1		
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	8	8		
70300100	SHORT TERM PAVEMENT MARKING	FOOT	634	634		
* 72000100	SIGN PANEL - TYPE 1	SQ FT	113	113		
* 72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	116	116		
* 72900200	METAL POST - TYPE B	FOOT	122	122		
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4095	4095		
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1221	1221		
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	125	125		
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	95	95		
* 78006110	PREFORMED THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	808	808		
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	12	12		
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	3	3		
* 81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	280	280		
* 81101000	CONDUIT ATTACHED TO STRUCTURE, 4" DIA., GALVANIZED STEEL	FOOT	730	730		
* 81400200	HEAVY-DUTY HANDHOLE	EACH	2	2		

CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				ROADWAY		BRIDGE
				80% FED 20% LOCAL AGENCY 0004	100% LOCAL AGENCY 0043	80% FED 20% LOCAL AGENCY 0011
* A2002920	TREE, CELTIS OCCIDENTALIS (COMMON HACKBERRY), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	2	2		
* A2006520	TREE, QUERCUS BICOLOR (SWAMP WHITE OAK), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	2	2		
* K0029634	WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE	POUND	5	5		
X0321963	MICRO-PILES	EACH	20			20
X0323433	MICROPILE PROOF LOAD TEST	EACH	1			1
* X0323814	SANITARY SEWER REMOVAL, 18"	FOOT	320	320		
X0324058	OUTLET SPECIAL	EACH	8	8		
* X0324894	WATERMAIN MANHOLES TO BE ADJUSTED	EACH	1	1		
* X0326659	CURED-IN-PLACE PIPE LINER, 18"	FOOT	165	165		
X0327131	DRAINAGE STRUCTURES, NO. 1	EACH	1	1		
X0327132	DRAINAGE STRUCTURES, NO. 2	EACH	1	1		
X0327357	CONSTRUCTION VIBRATION MONITORING	L SUM	1			1
* X0487700	SANITARY SEWER REMOVAL 10"	FOOT	5	5		
X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	5	5		
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	3	3		
X4060110	BITUMINOUS MATERIALS (PRIME COAT)	POUND	13575	13575		
X4810200	AGGREGATE SHOULDER REMOVAL	CU YD	476	476		
X5210150	HIGH LOAD MULTI-ROTATIONAL BEARINGS, GUIDED EXPANSION, 400K	EACH	10			10
△ X5538200	STORM SEWERS TO BE CLEANED 24"	FOOT	28		28	
* X5610708	WATER MAIN REMOVAL, 8"	FOOT	70	70		
* X5610712	WATER MAIN REMOVAL, 12"	FOOT	31	31		
* X5610716	WATER MAIN REMOVAL, 16"	FOOT	107	107		
* X5630012	CUT AND CAP EXISTING 12" WATER MAIN	EACH	6	6		
* X6022810	MANHOLES, SANITARY, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	5	5		
* X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	3	3		
* X6026054	SANITARY MANHOLES TO BE REMOVED	EACH	3	3		
* X6026622	VALVE VAULTS TO BE REMOVED	EACH	2	2		
* X6026632	VALVE BOXES TO BE REMOVED	EACH	2	2		
* X6330705	RUB RAIL	FOOT	119	119		
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1		
XX008021	REMOVE EXISTING STONE ARCH AND WINGWALLS	EACH	1	1		
XX008570	FURNISHING AND ERECTING PRECAST CONCRETEPANELS, STRUCTURE 1	L SUM	1	1		
XZ127900	RETAINING WALL REMOVAL	FOOT	26	18		8
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1		
△ Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	1		1	
* Z0022800	FENCE REMOVAL	FOOT	308	308		
Z0026407	TEMPORARY SHEET PILING	SQ FT	2055			2055
Z0034210	MECHANICALLY STABILIZED EARTH RETAINING WALL	SQ FT	8890			8890
Z0046304	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	937			937
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1		
* Z0050600	REMOVE AND RESET ORNAMENTAL FENCE	FOOT	682	682		
Z0056668	STORM SEWERS, TYPE 2, WATER MAIN QUALITY PIPE, 12"	FOOT	275	275		
Z0056669	STORM SEWERS, TYPE 2, WATER MAIN QUALITY PIPE, 15"	FOOT	32	32		
* Z0057000	SANITARY SEWER 10"	FOOT	10	10		

FILE NAME = G:\CH1\0138\Road\Sheets\0138-SHT-500.dgn	USER NAME = bafax	DESIGNED - MLB	REVISED -
		DRAWN - MLB	REVISED -
		CHECKED - YSK	REVISED -
		DATE - 5/12/14	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET NO. 5 OF 136 SHEETS STA. TO STA.

F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 5
CONTRACT NO. 63859				
ILLINOIS FED. AID PROJECT				



PLAN	DATE
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ALIGNMENT CHECKED	
NOTE BOOK NO.	
PLANT FILE NAME	

PROFILE	DATE
SUBMITTED	BY
GRADES CHECKED	
NOTE BOOK NO.	
PLANT FILE NAME	

CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				ROADWAY		BRIDGE
				80% FED 20% LOCAL AGENCY 0004	100% LOCAL AGENCY	80% FED 20% LOCAL AGENCY 0011
* Z0057300	SANITARY SEWER 18"	FOOT	351	351		
Z0065704	BITUMINOUS COATED AGGREGATE SLOPEWALL 6"	SQ YD	730			730
# Z0076600	TRAINEES	HOUR	1000	1000		
# Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	1000	1000		

\* DENOTES SPECIALTY ITEM  
 △ NON-PARTICIPATING = 100% LOCAL 0043  
 # CONSTRUCTION TYPE CODE 0042

FILE NAME = G:\CH11\0130\Road\Sheets\10130-SHT-500.dgn	USER NAME = a.jpondexter	DESIGNED - MLB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>				F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 5A
PLOT SCALE = N/A	CHECKED - YSK	REVISED -	REVISED -		SCALE: N.T.S.	SHEET NO. 5A OF 136 SHEETS	STA.	TO STA.	CONTRACT NO. 63859				
PLOT DATE = 6/27/2014	DATE = 5/12/14	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT								



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PLANNED	
GRADES CHECKED	
STRUCTURE NOTATIONS CHECKED	
NO.	

OHIO STREET (INDIAN AVENUE TO RURAL STREET)

STATION	DISTANCE	CUT - SUITABLE	CUT-UNSUITABLE	EMBANKMENT	CUT - SUITABLE	CUT-UNSUITABLE	EMBANKMENT	CUT - SUITABLE	CUT-UNSUITABLE	EMBANKMENT
(XX+XX)	(FT.)	SQ FT	SQ FT	SQ FT	AVG. SQ FT	AVG. SQ FT	AVG. SQ FT	CU YD	CU YD	CU YD
103+50.00		0.0	0.0	0.0						
13.66					12.5	0.0	0.5	0.0	0.0	0.0
103+63.66		25.0	0.0	1.0						
36.34					31.5	2.0	2.5	0.0	0.0	0.0
104+00.00		38	4	4						
50.00					94.5	2.0	2.0	175.0	3.7	3.7
104+36.27		151	0	0						
13.73					132.0	0.0	2.0	67.1	0.0	1.0
104+50.00		113	0	4						
50.00					56.5	2.5	17.0	104.6	4.6	31.5
105+00.00		0	5	30						
50.00					0.0	5.0	43.5	0.0	9.3	80.6
105+50.00		0	5	57						
50.00					0.5	6.5	66.0	0.9	12.0	122.2
106+00.00		1	8	75						
50.00					0.5	9.0	96.0	0.9	16.7	177.8
106+50.00		0	10	117						
50.00					0.5	14.5	161.0	0.9	26.9	298.1
107+00.00		1	19	205						
50.00					1.0	18.0	202.0	1.9	33.3	374.1
107+07.89		1	17	199						
42.11					0.5	15.0	217.0	0.8	23.4	338.4
107+50.00		0	13	235						
50.00					0.0	8.0	192.5	0.0	14.8	356.5
108+00.00		0	3	150						
50.00					0.0	3.0	157.5	0.0	5.6	291.7
108+50.00		0	3	165						
50.00					0.0	3.0	163.5	0.0	5.6	302.8
109+00.00		0	3	162						
50.00					0.0	3.0	161.5	0.0	5.6	299.1
109+12.36		0	3	161						
37.64					0.0	1.5	80.5	0.0	2.1	112.2
109+50.00										
50.00										
110+00.00										
50.00										
110+50.00										
50.00										
111+00.00										
50.00										
111+50.00										
50.00										
112+00.00										
50.00										
112+50.00										
50.00										
112+58.27		159	3	145						
41.73					132.5	3.0	146.5	204.8	4.6	226.4
113+00.00		106	3	148						
50.00					53.0	3.0	147.5	98.1	5.6	273.1
113+50.00		0	3	147						
50.00					0.0	3.0	144.0	0.0	5.6	266.7
113+90.00		0	3	141						
10.00					0.0	10.0	260.0	0.0	3.7	96.3
114+00.00		0	17	379						
50.00					0.0	17.5	340.0	0.0	32.4	629.6
114+50.00		0	18	301						
50.00					0.5	20.0	240.0	0.9	37.0	444.4
115+00.00		1	22	179						
50.00					0.5	26.5	180.5	0.9	49.1	334.3
115+03.86			31	182						
2.93					0.0	35.0	183.5	0.0	3.8	19.9
115+06.79		0	39	185						
43.21					0.5	23.5	119.5	0.8	37.6	191.2
115+50.00		1	8	54						
50.00					24.5	5.5	29.5	45.4	10.2	54.6
116+00.00		48	3	5						
50.00					56.5	1.5	5.0	104.6	2.8	9.3
116+17.43		65	0	5						
12.86					139.0	0.0	3.5	66.2	0.0	1.7
116+30.29		213	0	2						
19.71					158.0	2.5	1.5	115.3	1.8	1.1
116+50.00		103	5	1						
50.00					97.0	5.0	1.0	179.6	9.3	1.9
116+70.04		91	5	1						
29.96					85.0	5.5	0.5	94.3	6.1	0.6

BRIDGE CROSS-SECTIONS

OHIO STREET (INDIAN AVENUE TO RURAL STREET)

STATION	DISTANCE	CUT - SUITABLE	CUT-UNSUITABLE	EMBANKMENT	CUT - SUITABLE	CUT-UNSUITABLE	EMBANKMENT	CUT - SUITABLE	CUT-UNSUITABLE	EMBANKMENT
(XX+XX)	(FT.)	SQ FT	SQ FT	SQ FT	AVG. SQ FT	AVG. SQ FT	AVG. SQ FT	CU YD	CU YD	CU YD
117+00.00		79	6	0						
50.00					72.0	7.0	1.5	133.3	13.0	2.8
117+34.82		65	8	3						
15.18					62.5	7.0	6.0	35.1	3.9	3.4
117+50.00		60	6	9						
50.00					130.5	15.0	4.5	241.7	27.8	8.3
117+87.51		201	24	0						
12.49					155.5	12.0	0.0	71.9	5.6	0.0
118+00.00		110	0	0						
50.00					79.0	4.0	0.5	146.3	7.4	0.9
118+50.00		48	8	1						
50.00					62.5	11.0	0.5	115.7	20.4	0.9
118+94.92		77	14	0						
5.08					64.0	7.0	3.0	12.0	1.3	0.6
119+00.00		51	0	6						
50.00					53.5	0.0	8.0	99.1	0.0	14.8
119+50.00		56	0	10						
50.00					44.0	0.0	9.0	81.5	0.0	16.7
119+64.50		32	0	8						
35.50					16.0	0.0	4.0	21.0	0.0	5.3
120+00.00		0	0	0						
Totals								2,221.0	452.3	5,394.4



### STORM SEWER STRUCTURE TABLE

STR. NUM.	STATION	OFFSET	RIM ELEV.	N INV.	E INV.	S INV.	W INV.	CB TA 4 DIA TBG EACH	CB TA 4 DIA T11VF&G EACH	MAN TA 4 DIA T1F CL EACH	MAN TA 4 DIA T1F OL EACH	MAN TA 5 DIA T1F CL EACH	MAN TA 6 DIA T1F CL EACH	MAN TA 8 DIA T1F CL EACH	MAN TA 9 DIA T1F CL EACH	MAN TA 9 DIA T1F OL EACH	INLETS TA T11VF&G EACH	DRAINAGE STRUCTURE NO.1 EACH	DRAINAGE STRUCTURE NO. 2 EACH	CONC ES 542001 78 1:2 EACH	CONC END SECTIONS 72" EACH	COMMENTS	
S100	103+68.00	17.88 RT	705.60	695.75		695.80				1													
S101	104+02.00	20.30 LT	704.33			697.27											1						
S102	103+86.00	19.93 LT	704.60	697.14	697.04			1															
S103	103+90.00	20.55 RT	704.10	695.66		695.66	695.76	1															
S104	301+98.20	17.18 RT	703.35			694.85											1						
S105	302+12.50	14.00 RT	703.61				694.86										1						
S106	301+98.20	17.09 RT	703.89	694.23	694.63		694.13	1															
S107	104+19.36	20.25 RT	704.10	693.82	693.92	695.53			1														
S108	104+85.35	14.00 LT	702.64			698.36											1						
S109	104+85.35	14.00 RT	702.64			696.11		1															
S110	104+85.35	19.58 RT	702.90	692.08		693.08	696.08					1											
S111	106+25.00	14.00 LT	699.83	696.36													1						
S112	106+50.00	14.00 LT	699.81			696.29											1						
S113	106+39.16	14.00 LT	699.80	696.14	695.64	696.14		1															
S114	106+25.00	14.00 RT	699.83	694.88													1						
S115	106+39.16	14.00 RT	699.80	694.66		694.66	695.16	1															
S116	106+50.00	14.00 RT	699.81	694.52	692.52	694.52		1															
S117	106+50.00	37.44 RT	696.25			691.07		1															
S118	106+50.00	19.58 RT	698.80			690.50	690.80	692.50				1											
S119	106+74.95	14.00 LT	699.87			695.07											1						
S120	106+74.95	14.00 RT	699.87			694.72	694.82		1														
S121	106+50.00	27.62 RT	698.89	689.78	690.80		689.78							1									
S122	NA																						
S123	108+10.00	27.06 RT	697.70	689.00		689.00								1									
S200	601+31.52	22.09 LT	692.75			686.00																1	
S201	601+31.52	25.00 RT	693.50	685.80			679.80								1								
S202	600+81.15	25.00 RT	691.99			679.60	678.80											1					
S203	118+25.00	19.50 RT	692.45			679.04						1											
S204	118+01.49	14.00 LT	692.63			684.72							1										
S205	118+01.49	50.00 RT	691.45				685.01		1														
S206	118+01.49	20.34 RT	690.60	678.84	684.75	678.74	684.44							1									
S207	117+62.31	31.01 RT	691.32	678.60	678.59	678.22													1				
S208	117+05.00	23.96 RT	690.54	678.13		678.03										1							
S209	116+35.00	24.00 RT	689.73														1						
S210	116+37.49	24.03 RT	689.44														1						
S211	116+47.49	28.73 LT	689.86				686.82										1						
S212	116+46.18	39.79 LT	689.71			686.73	686.63		1														
S213	116+15.81	40.20 LT	689.13	686.24	686.14																		
S214	115+81.93	14.00 LT	689.85			685.70		685.80		1													
S215	115+81.88	24.00 RT	689.01	677.88		677.78	685.50									1							
S216	115+30.00	41.00 LT	688.00			685.20									1								
S217	115+45.00	38.00 RT	688.25				685.48		1														
S218	115+45.00	24.00 RT	689.75	677.70	685.40	677.60	684.60									1							
S219	113+25.00	14.00 LT	699.32			693.11											1						
S220	113+25.00	14.54 RT	699.32				693.00										1						
S221	113+25.00	6.00 RT	699.48	686.58	692.95		692.95			1													
S222	114+70.00	14.00 LT	692.32			685.33											1						
S223	114+70.00	6.00 RT	692.48			685.07	685.17	685.17								1							
S224	114+70.00	24.00 RT	692.48			684.83		684.93									1						
S225	114+70.00	38.00 RT	689.93	677.53		676.41	684.75									1							
S226	112+87.23	38.00 RT	686.00	676.24		674.24										1							
S227	112+70.00	47.95 RT	684.00	674.22																	1		
<b>TOTAL =</b>								5	15	4	0	4	1	1	3	3	11	1	1	1	1		

PLAN	DATE
DESIGNED	
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NO. _____	

PROFILE	DATE
DESIGNED	
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GRADES CHECKED	
B.M. NOTED	
STRUCTURE NOTATIONS CHECKED	
NO. _____	



### STORM SEWER PIPE SCHEDULE

PIPE NUM	STORM SEWERS, CLASS A, TYPE 2 12"	STORM SEWERS, CLASS A, TYPE 2 15"	STORM SEWERS, CLASS A, TYPE 2 18"	STORM SEWERS, CLASS A, TYPE 2 24"	STORM SEWERS, CLASS A, TYPE 2 60"	STORM SEWERS, CLASS A, TYPE 2 72"	STORM SEWERS, CLASS A, TYPE 2 78"	STORM SEWERS, TYPE 2 WAT MAN 12"	STORM SEWERS, TYPE 2 WAT MAN 15"	PIPE SLOPE	TRENCH BACKFILL	COMMENTS
	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	(%)	CU YD	
P100			18.1							0.50%	25	
P101	13.0									1.00%	10	
P102								36.7		3.50%	33	
P103			25.4							0.50%	27	
P104								31.3		2.00%	34	
P105	11.7									2.00%	13	
P106		10.3								2.00%	13	
P107			62.0							1.20%	64	
P108								25.0		3.00%	6	
P109	1.1									3.00%	7	
P110				159.7						0.80%	0	
P111								11.2		2.00%	2	
P112								7.8		2.00%	2	
P113										2.00%	5	
P114	11.2									2.00%	3	
P115	6.8									2.00%	2	
P116		1.1								2.00%	9	
P117	6.0									4.47%	0	
P118				90.0						0.80%	36	
P119	25.0									1.00%	7	
P120		20.0								1.00%	13	
P121				155.0						0.50%	182	
P200							39.8			0.50%	25	
P201							40.8			0.49%	0	
P202							41.5			0.51%	123	
P203					19.5					1.02%	44	
P204								28.0		1.00%	21	
P205	22.5									1.16%	17	
P206					28.3					0.49%	62	
P207						50.5				0.18%	149	
P208						85.8				0.17%	163	
P211								9.2		0.98%	2	
P212								26.4		1.48%	4	
P213								38.8		0.88%	6	
P214									31.5	0.63%	4	
P215							56.4			0.14%	98	
P216								60.2		1.00%	13	
P217	7.5									1.07%	3	
P218							67.3			0.10%	182	
P219	16.0									1.00%	11	
P220	4.5									1.11%	3	
P221	141.0									1.00%	175	
P222	16.0									1.00%	13	
P223		14.0								1.00%	12	
P224		7.5								1.07%	8	
P225							173.8			0.10%	0	
P226							15.4			0.13%	0	
TOTAL	283	53	106	405	48	259	313	275	32		1,631	

PLAN	DATE
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NOT PLOTTED	
ALIGNMENT CHECKED	
NOTE BOOK CHECKED	
NO. _____	

PROFILE	DATE
REWORKED	
GRADES CHECKED	
STRUCTURE NOTATION CHECKED	
NO. _____	

FILE NAME * G:\CHI\1\0130\Road\Sheets\10130-SHT-DRAIN-SH.dgn	USER NAME * e.pandexter	DESIGNED - YSK	REVISED - -
PLOT SCALE * N/A	CHECKED - MWS	REVISED - -	REVISED - -
PLOT DATE * 5/12/2014	DATE - 5/12/14	REVISED - -	REVISED - -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STORM SEWER STRUCTURE AND PIPE TABLES**

SCALE: N.T.S.      SHEET NO. 8 OF 136 SHEETS      STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 8
CONTRACT NO. 63859				

ILLINOIS FED. AID PROJECT

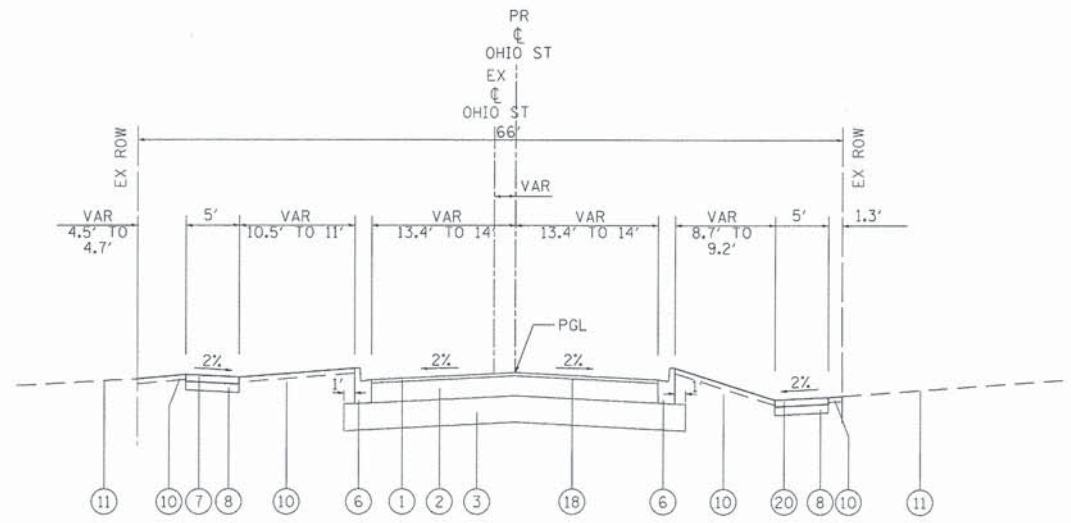




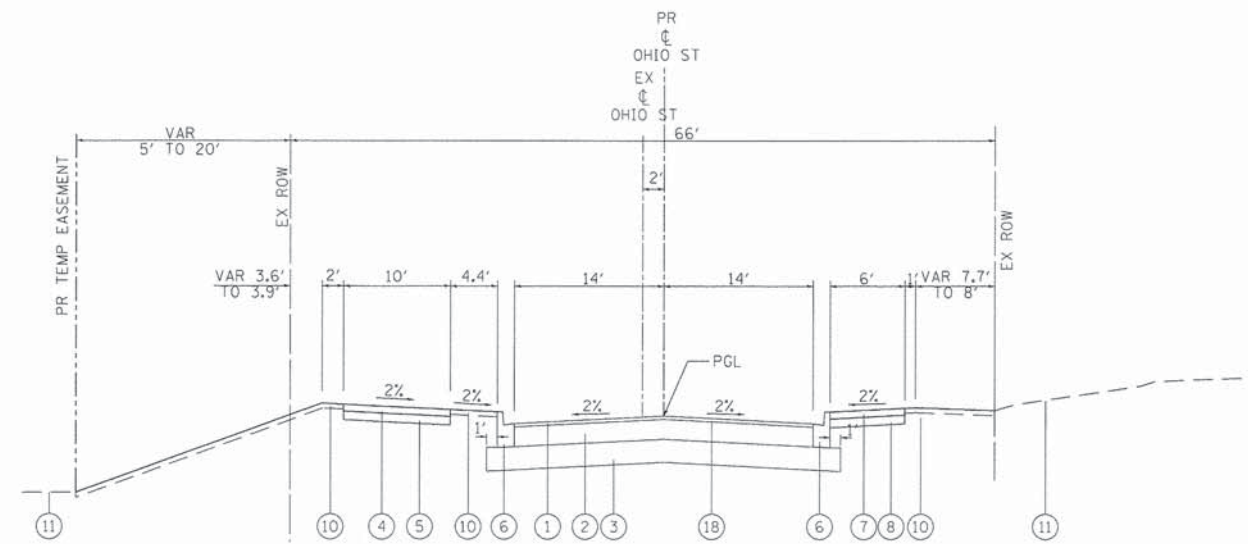


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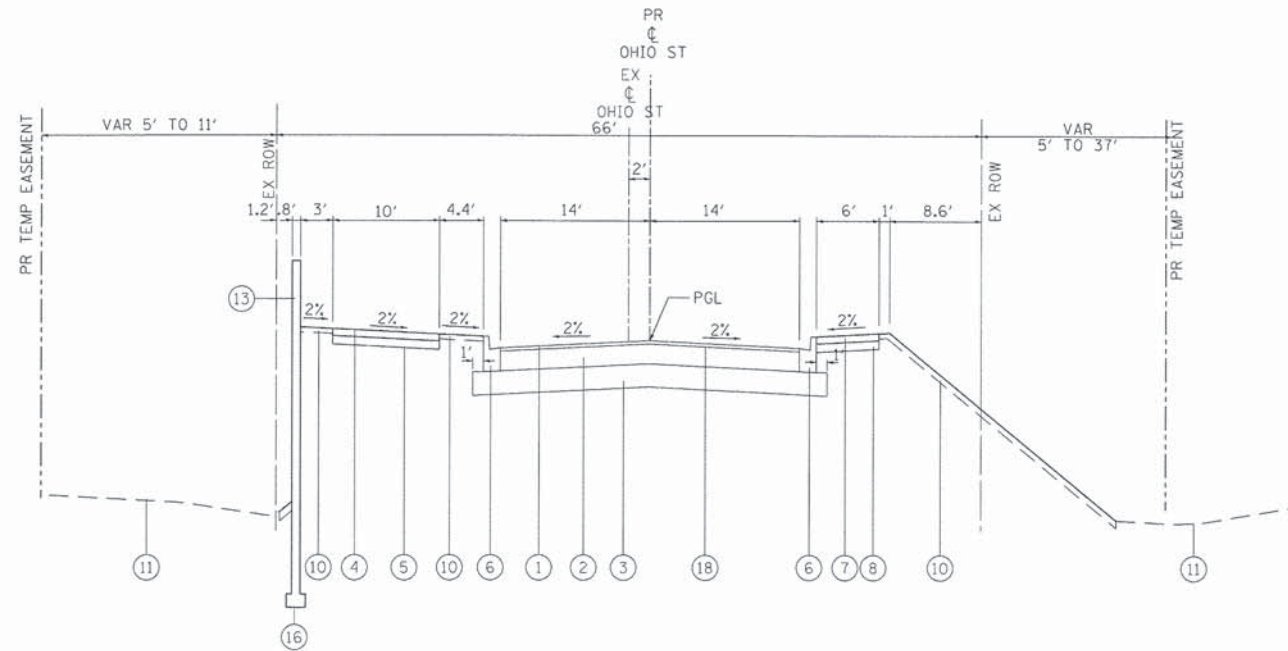
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PROFILES	
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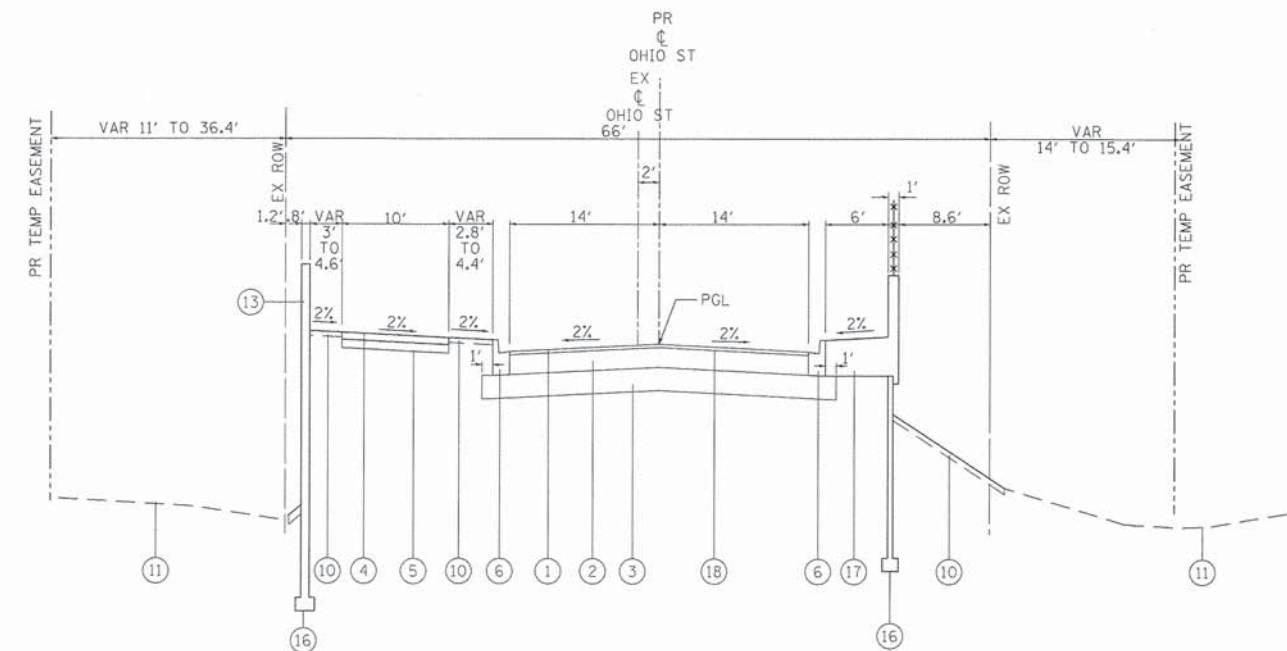
**PROPOSED TYPICAL SECTION NO. 1**  
OHIO STREET  
STATION 103+53.66 TO STATION 103+82.13  
OMIT STATION 103+82.13 TO STATION 104+85.40 - INTERSECTION



**PROPOSED TYPICAL SECTION NO. 2**  
OHIO STREET  
STATION 104+85.40 TO STATION 106+25.00



**PROPOSED TYPICAL SECTION NO. 3**  
OHIO STREET  
STATION 106+97.56 TO STATION 107+52.90



**PROPOSED TYPICAL SECTION NO. 4**  
OHIO STREET  
STATION 107+52.90 TO STATION 108+83.2  
OMIT STATION 108+83.20 TO STATION 112+87.40 - BRIDGE

**PROPOSED LEGEND**

- |                                                        |                                     |                                                |
|--------------------------------------------------------|-------------------------------------|------------------------------------------------|
| ① HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2" | ⑧ AGGREGATE BASE COURSE, TYPE B, 4" | ⑮ HOT-MIX ASPHALT SHOULDER, 6"                 |
| ② HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 9"      | ⑨ AGGREGATE SHOULDER, 6"            | ⑯ MECHANICALLY STABILIZED EARTH RETAINING WALL |
| ③ AGGREGATE SUBGRADE IMPROVEMENT, 12"                  | ⑩ TOPSOIL FURNISH AND PLACE, 4"     | ⑰ MOMENT SLAB                                  |
| ④ HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"     | ⑪ EXISTING GROUND                   | ⑱ BITUMINOUS MATERIAL (PRIME COAT)             |
| ⑤ AGGREGATE BASE COURSE, TYPE B, 8"                    | ⑫ PROPOSED DITCH                    | ⑲ PARAPET RAILING                              |
| ⑥ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12    | ⑬ BICYCLE RAILING                   | ⑳ PORTLAND CEMENT CONCRETE SIDEWALK, 5"        |
| ⑦ PORTLAND CEMENT CONCRETE SIDEWALK, 6"                | ⑭ STEEL PLATE BEAM GUARDRAIL        |                                                |

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

MIXTURE TYPE	LIFT THICKNESS	AIR VOIDS @ NDES
<b>FULL DEPTH PAVEMENT</b>		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5mm), 1 1/2"	1 1/2"	4% @ 50 GYR
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 9" (IN 4 LIFTS)	2 1/4" MIN	4% @ 50 GYR
<b>SHARED USE PATH</b>		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5mm) 2"	1 1/2"	4% @ 50 GYR
<b>BITUMINOUS STABILIZATION AT STEEL PLATE BEAM GUARDRAIL</b>		
HOT-MIX ASPHALT SHOULDER (HMA BINDER IL-19mm), 6" (IN 3 LIFTS)	2 1/4" MIN	4% @ 50 GYR

- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SOYD/IN.
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS FOR % OF RAP. SEE **B&E**

FILE NAME *	USER NAME = ejpondexter	DESIGNED - MLB	REVISED -
G:\CH11\0130\Road\Sheets\10130-SHT-TYPICAL.dgn		DRAWN - MLB	REVISED -
	PLOT SCALE = N/A	CHECKED - JMG	REVISED -
	PLOT DATE = 5/12/2014	DATE - 5/12/14	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED TYPICAL SECTIONS**

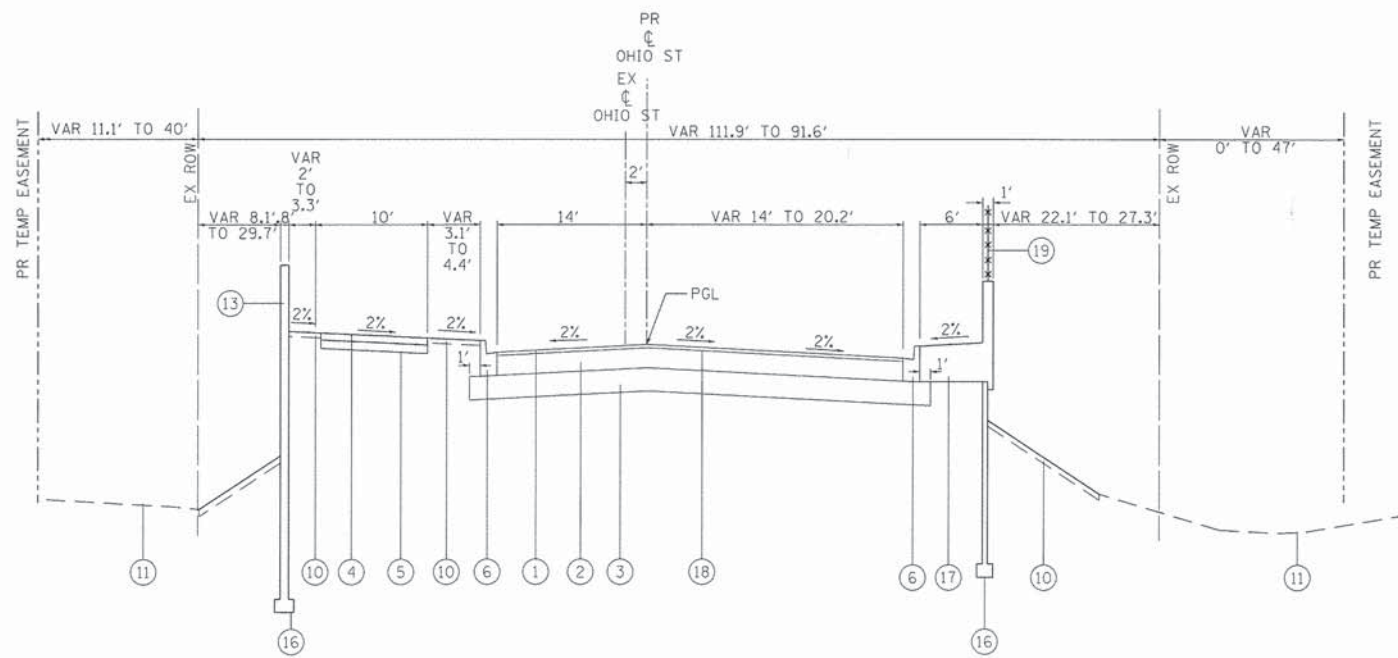
SCALE: NTS SHEET NO. 10 OF 136 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	10
				CONTRACT NO. 63859
ILLINOIS FED. AID PROJECT				

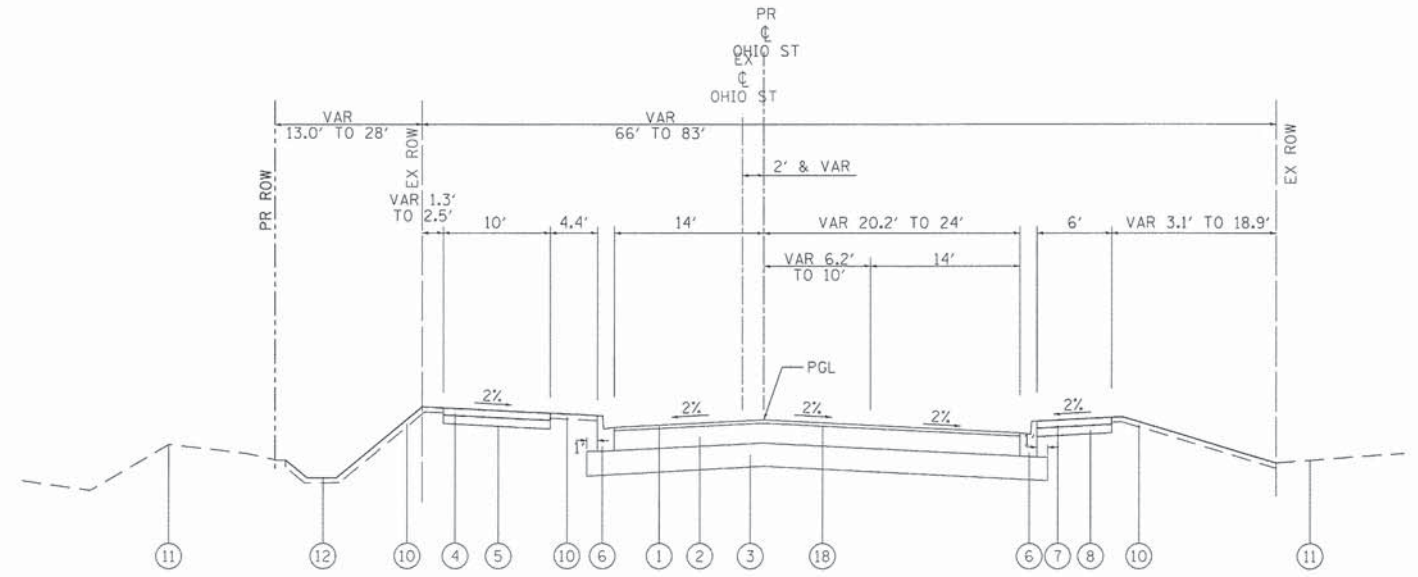


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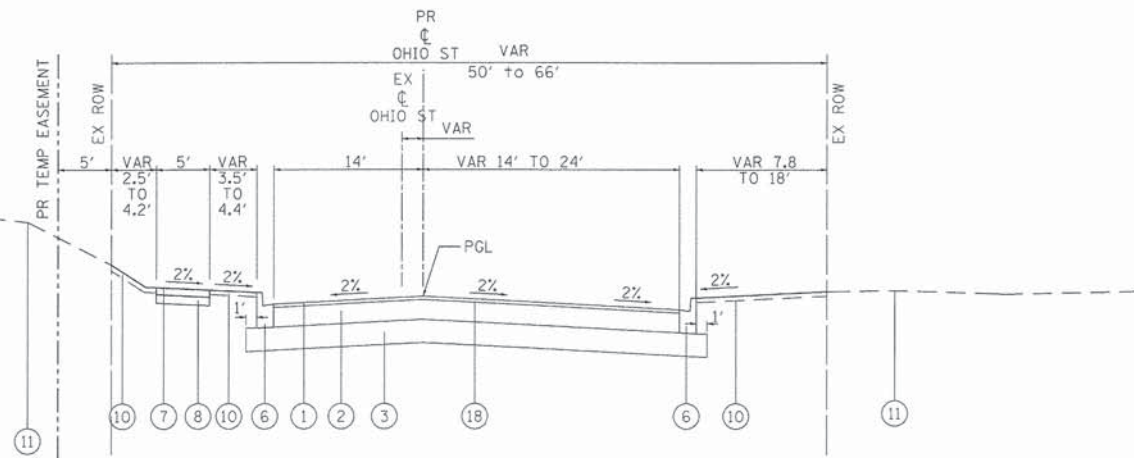
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REVISIONS	
PROFILES	
GRADES	
CHECKED	
STRUCTURE	
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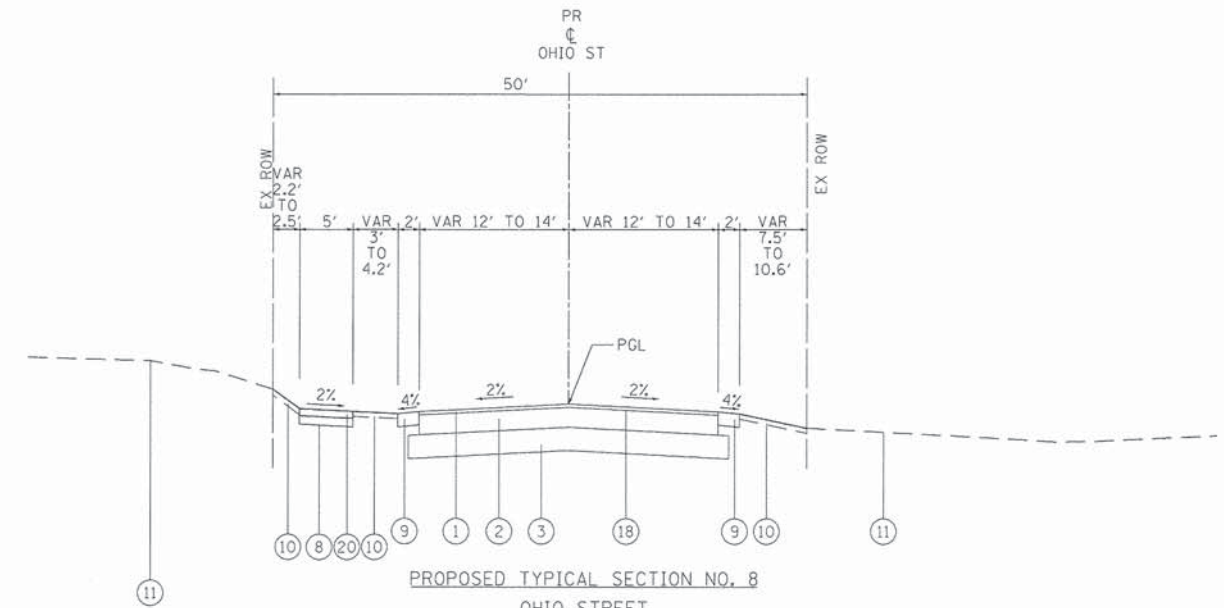
PROPOSED TYPICAL SECTION NO. 5  
OHIO STREET  
STATION 112+87.40 TO STATION 114+06.86



PROPOSED TYPICAL SECTION NO. 6  
OHIO STREET  
STATION 114+06.86 TO STATION 115+81.90  
OMIT STATION 115+81.90 TO STATION 116+44.00 - INTERSECTION



PROPOSED TYPICAL SECTION NO. 7  
OHIO STREET  
STATION 116+44.00 TO STATION 118+36.20



PROPOSED TYPICAL SECTION NO. 8  
OHIO STREET  
STATION 118+36.20 TO STATION 119+64.50

PROPOSED LEGEND

- |                                                        |                                     |                                         |
|--------------------------------------------------------|-------------------------------------|-----------------------------------------|
| ① HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2" | ⑧ AGGREGATE BASE COURSE, TYPE B, 4" | ⑮ HOT-MIX ASPHALT SHOULDER, 6"          |
| ② HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N50, 9"      | ⑨ AGGREGATE SHOULDER, 6"            | ⑯ MECHANICALLY STABILIZED EARTH WALL    |
| ③ AGGREGATE SUBGRADE IMPROVEMENT, 12"                  | ⑩ TOPSOIL FURNISH AND PLACE, 4"     | ⑰ MOMENT SLAB                           |
| ④ HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"     | ⑪ EXISTING GROUND                   | ⑱ BITUMINOUS MATERIAL (PRIME COAT)      |
| ⑤ AGGREGATE BASE COURSE, TYPE B, 8"                    | ⑫ PROPOSED DITCH                    | ⑲ PARAPET RAILING                       |
| ⑥ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12    | ⑬ BICYCLE RAILING                   | ⑳ PORTLAND CEMENT CONCRETE SIDEWALK, 5" |
| ⑦ PORTLAND CEMENT CONCRETE SIDEWALK, 6"                | ⑭ STEEL PLATE BEAM GUARDRAIL        |                                         |

FILE NAME =	USER NAME = ajpondexter	DESIGNED - MLB	REVISED -
Gr:\CHI1\0130\Road\Sheets\10130-SHT-TYPICAL.dgn		DRAWN - MLB	REVISED -
		CHECKED - JMG	REVISED -
		DATE - 5/12/14	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PROPOSED TYPICAL SECTIONS

SCALE: NTS SHEET NO. 11 OF 136 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	11
CONTRACT NO. 63859			ILLINOIS FED. AID PROJECT	





**LEGEND**

---	EXISTING ROW
- - -	PROPOSED ROW
	PROPOSED PERMANENT EASEMENT
//////	PROPOSED TEMPORARY EASEMENT
	EXISTING PERMANENT EASEMENT
---	EXISTING PROPERTY LINE

**NOTES:**

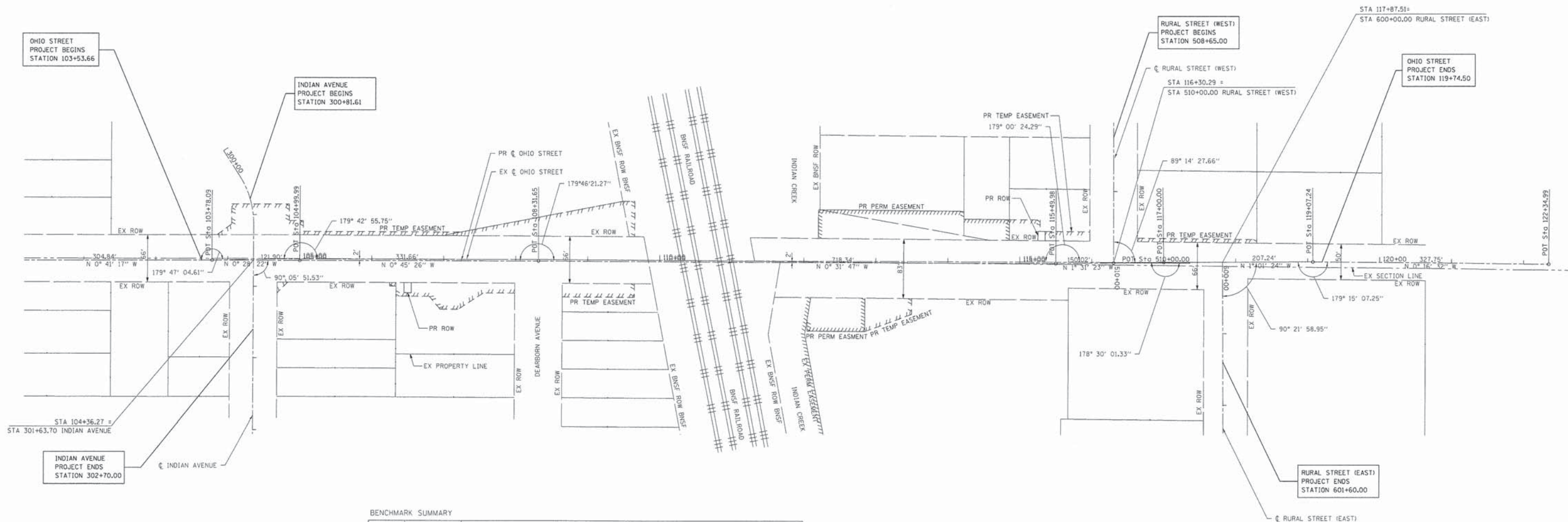
1. ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE SHOWN.
2. ALL COORDINATES SHOWN HEREON ARE BASED UPON THE ILLINOIS STATE PLANE COORDINATES SYSTEM:  
(NAD83) - NORTH AMERICAN DATUM 1983 (2007 ADJUSTMENT) ILLINOIS EAST ZONE  
(NAVD88) - NORTH AMERICAN VERTICAL DATUM 1988
3. FOR PROPOSED RIGHT-OF-WAY AND EASEMENT INFORMATION REFER TO PLAT OF HIGHWAY DRAWING. SEE SHEET NO. 54 THRU 63

**PLAN**

DESIGNED	BY	DATE
PLOTTED		
ALIGNED		
CHECKED		
FILED		
NO.		

**PROFILE**

DESIGNED	BY	DATE
PLOTTED		
GRADES CHECKED		
STRUCTURE NOTATIONS CHECKED		
NO.		



**OHIO STREET ALIGNMENT DATA**

DESCR	STATION	OFFSET	NORTHING	EASTING
PI-1	103+78.09	0.00	1,857,324.859	995,717.918
PI-2	104+99.99	0.00	1,857,446.756	995,716.912
PI-3	108+31.65	0.00	1,857,778.383	995,712.529
PI-4	115+49.98	0.00	1,858,496.688	995,705.887
PI-5	117+00.00	0.00	1,858,646.655	995,701.900
PI-6	119+07.24	0.00	1,858,853.861	995,698.199

**INDIAN AVENUE ALIGNMENT DATA**

DESCR	STATION	OFFSET	NORTHING	EASTING
B.O.P.	300+00.00	0.00	1,857,338.533	995,564.784
PI	300+79.53	0.00	1,857,382.177	995,631.269
PC	300+42.85	0.00	1,857,362.045	995,600.601
PT	301+14.21	0.00	1,857,382.542	995,667.953
POT	304+60.05	0.00	1,857,385.984	996,013.772

**RURAL STREET (WEST)**

DESCR	STATION	OFFSET	NORTHING	EASTING
POT	500+00.00	0.00	1,858,563.624	994,703.234
POT	510+00.00	0.00	1,858,576.959	995,703.145

**RURAL STREET (EAST)**

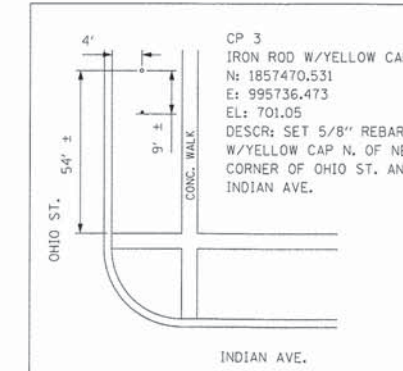
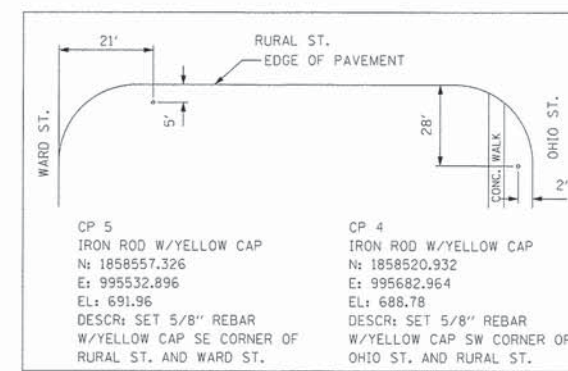
DESCR	STATION	OFFSET	NORTHING	EASTING
POT	600+00.00	0.00	1,858,734.145	995,700.337
POT	603+00.00	0.00	1,858,737.585	996,000.367

**BENCHMARK SUMMARY**

NUMBER	ELEVATION	DESCRIPTION
19E	700.98	CITY OF AURORA SURVEY MONUMENT, 1 FOOT WEST OF WALK AND 3 FEET NORTH OF LANDSCAPING RETAINING WALL ON N-W CORNER OF OHIO STREET & INDIAN AVE.
19W	700.89	CITY OF AURORA SURVEY MONUMENT, 1 FOOT EAST OF BACK OF CURB AND 90 FEET EAST OF PARK BUILDING ON WEST SIDE OF OHIO ST. AND INDIAN AVE. ON SOLFISBURG PARK
CP3	701.05	SET 5/8" REBAR W/YELLOW CAP N. OF NE CORNER OF OHIO ST. AND INDIAN AVE.
CP4	688.78	SET 5/8" REBAR W/YELLOW CAP SW CORNER OF OHIO ST. AND RURAL ST.
CP5	691.96	SET 5/8" REBAR W/YELLOW CAP SE CORNER OF RURAL ST. & WARD ST.

**CONTROL POINT INFORMATION**

CP#	NORTHING	EASTING	ELEVATION	STATION	OFFSET	DESCRIPTION
CP1	1,857,426.933	995,684.771	700.47	104+80.47	32.2' RT	FOUND 5/8" REBAR NW CORNER OF OHIO ST. & INDIAN AVE.
CP3	1,857,470.531	995,736.473	701.05	105+23.50	19.8' RT	SET 5/8" REBAR W/YELLOW CAP N. OF NE CORNER OF OHIO ST. & INDIAN AVE.
CP4	1,858,520.932	995,682.964	688.78	115+74.81	22.2' RT	SET 5/8" REBAR W/YELLOW CAP SW CORNER OF OHIO ST. & RURAL ST.
CP5	1,858,557.326	995,532.896	691.96	508+29.50	17.3' LT	SET 5/8" REBAR W/YELLOW CAP SE CORNER OF RURAL ST. & WARD ST.
CP20	1,858,323.320	995,690.019	691.98	113+85.77	17.3' LT	SET "X" IN SIDEWALK W. SIDE OF OHIO ST. NEAR END OF GUARDRAIL
CP22	1,857,865.895	995,693.973	704.79	109+19.33	17.7' LT	SET "X" IN SIDEWALK AR SW CORNER OF RR BRIDGE AND OHIO ST.
CP32	1,858,419.975	995,688.873	688.67	114+73.43	17.7' LT	SET MAG NAIL W. SIDE OF OHIO ST. SOUTH SIDE OF DRIVE TO ADDRESS 989 RURAL ST.



FILE NAME *	USER NAME = ajpondexter	DESIGNED -	REVISED -
G:\CHI\1\0130\Road\Sheets\10130-SHT-AT&B.dwg		DRAWN -	REVISED -
	PLOT SCALE = N/A	CHECKED -	REVISED -
	PLOT DATE = 5/12/2014	DATE = 5/12/14	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT AND TIES**

SCALE: N.T.S. SHEET NO. 12 OF 136 SHEETS STA. 103+63.66 TO STA. 119+19.17

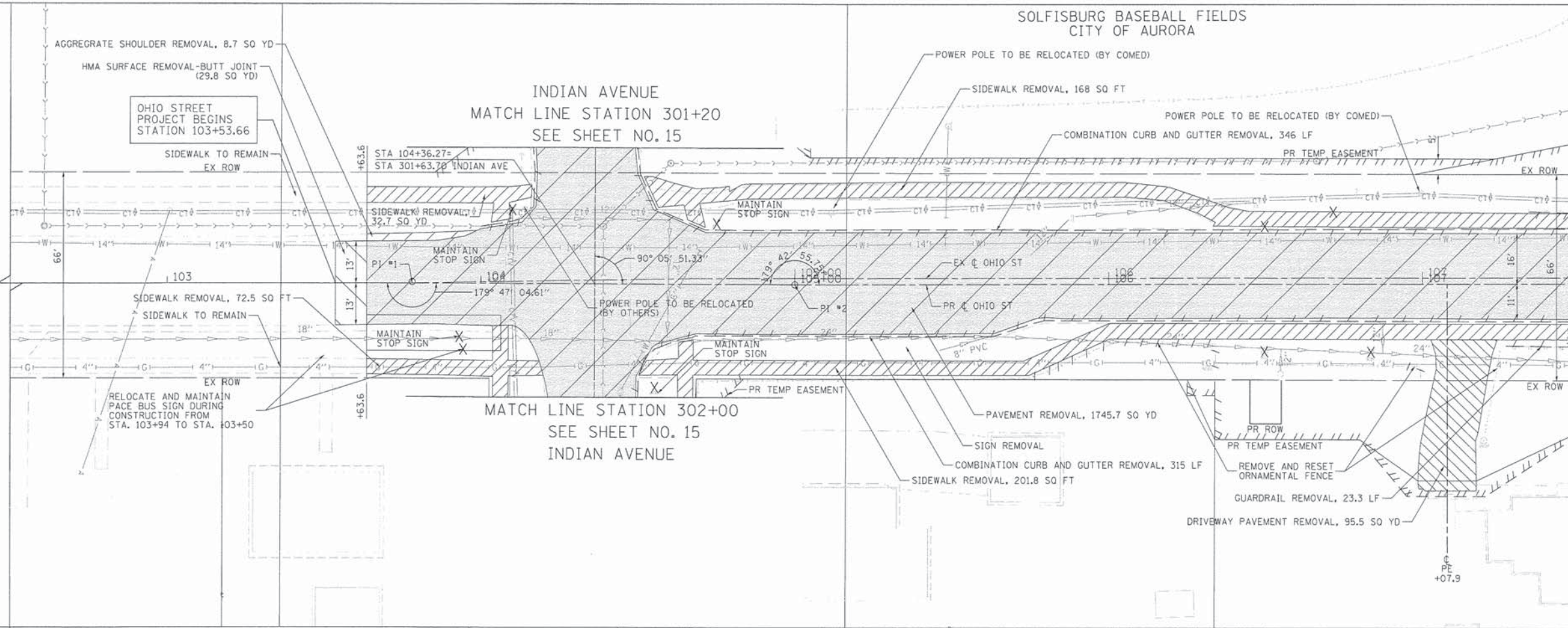
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	12
				CONTRACT NO. 63859
ILLINOIS FED. AID PROJECT				





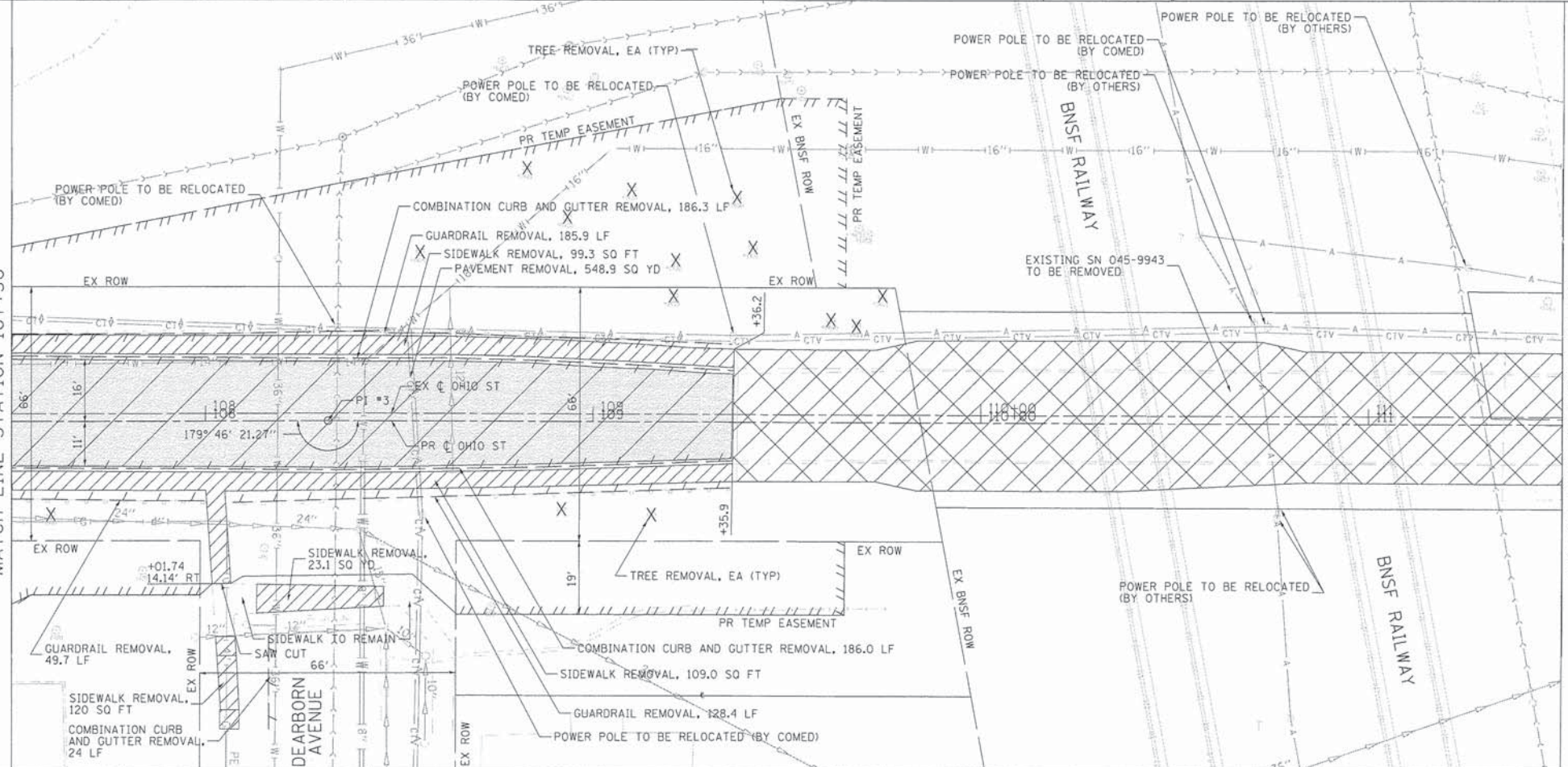
OHIO STREET

MATCH LINE STATION 107+50  
SEE BOTTOM LEFT



SEE TOP RIGHT  
MATCH LINE STATION 107+50

MATCH LINE STATION 111+50  
SEE SHEET NO. 14



- REMOVAL LEGEND**
- BRIDGE REMOVAL
  - PAVEMENT REMOVAL
  - HMA SURFACE REMOVAL - BUTT JOINT
  - COMBINATION CURB AND GUTTER REMOVAL
  - SIDEWALK REMOVAL
  - DRIVEWAY PAVEMENT REMOVAL
  - AGGREGATE SHOULDER REMOVAL
  - FENCE REMOVAL
  - GUARDRAIL REMOVAL
  - RETAINING WALL REMOVAL
  - TREE REMOVAL (6 TO 15 UNITS DIAMETER)  
TREE REMOVAL (OVER 15 UNITS DIAMETER)
  - SIGN REMOVAL

- NOTES:**
- ALL EXISTING SIGNS TO BE REMOVED SHALL BE SALVAGED AND DELIVERED TO THE CITY OF AURORA.
  - ALL STORM SEWER REMOVAL, CULVERT REMOVAL, DRAINAGE STRUCTURE REMOVAL AND ADJUSTMENTS, AND UTILITY ADJUSTMENTS ARE SHOWN ON THE DRAINAGE AND UTILITIES PLANS 27 AND 33.



FILE NAME =	USER NAME =	DESIGNED -	REVISED -
G:\CH11\0130\Road\Sheets\10130-SHT-RM-1.dgn	ajpandexter	MLB	
	PLOT SCALE =	DRAWN -	REVISED -
	N/A	MLB	
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	5/12/2014	MWS	
		DATE -	REVISED -
		5/12/14	

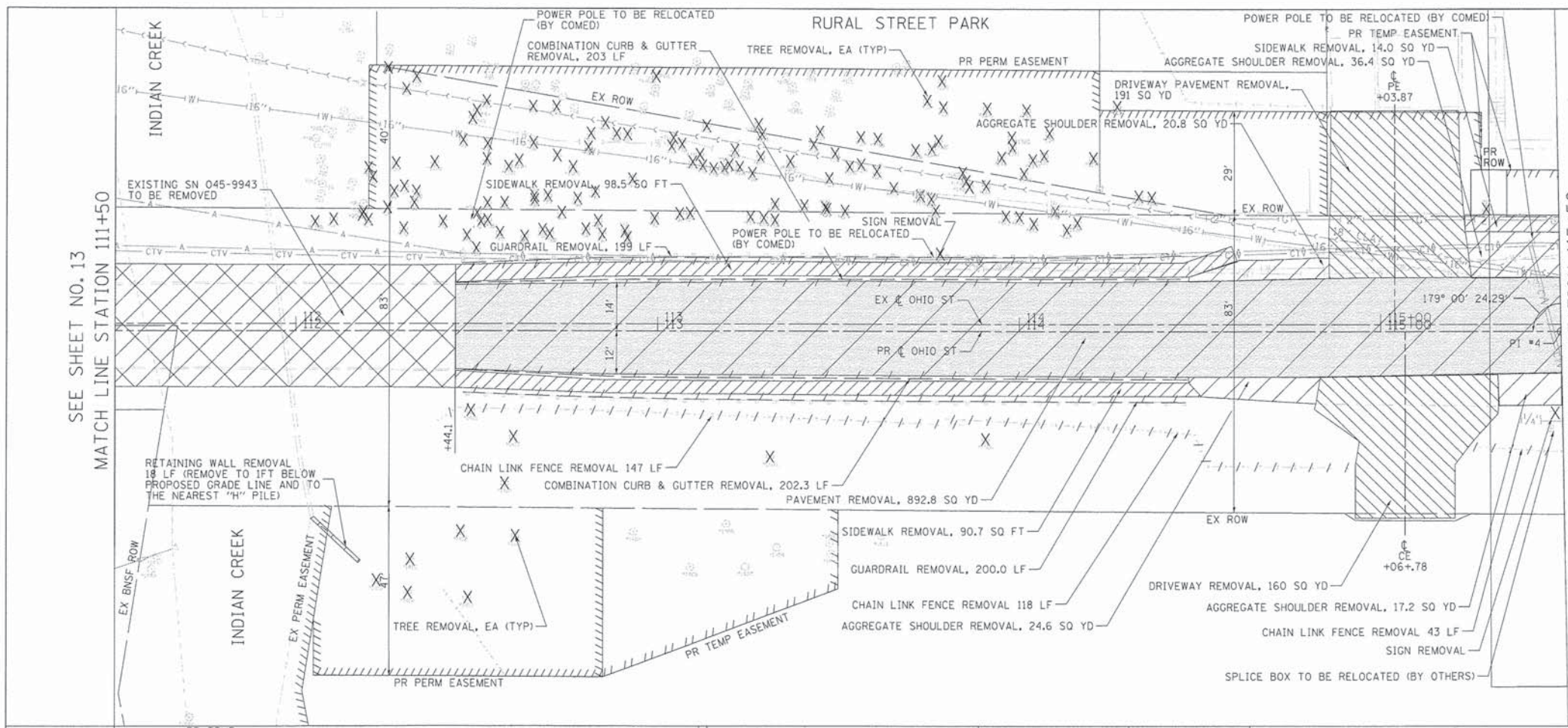
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

REMOVAL PLAN

SCALE: 1"=20' SHEET NO. 13 OF 136 SHEETS STA. 103+53.66 TO STA. 111+50

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	13
				CONTRACT NO. 63859
[ILLINOIS] FED. AID PROJECT				

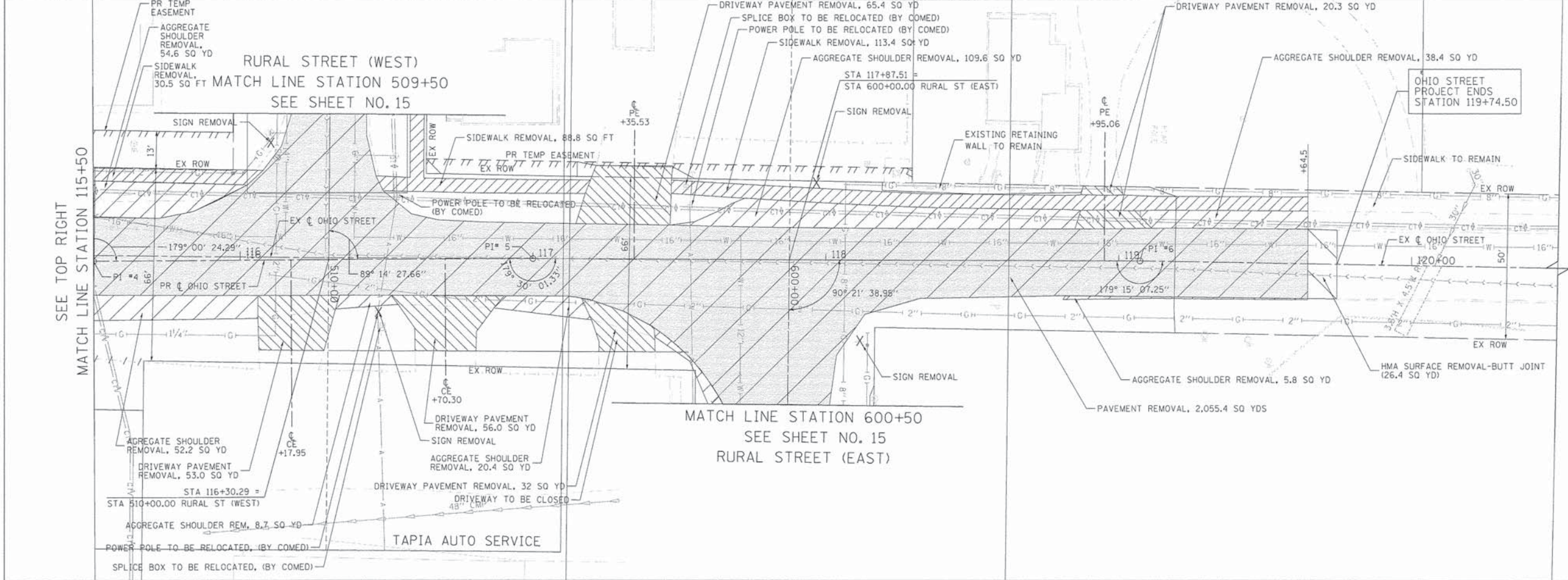




MATCH LINE STATION 115+50  
SEE BOTTOM LEFT

- REMOVAL LEGEND**
- BRIDGE REMOVAL
  - PAVEMENT REMOVAL
  - HMA SURFACE REMOVAL - BUTT JOINT
  - COMBINATION CURB AND GUTTER REMOVAL
  - SIDEWALK REMOVAL
  - DRIVEWAY PAVEMENT REMOVAL
  - AGGREGATE SHOULDER REMOVAL
  - FENCE REMOVAL
  - GUARDRAIL REMOVAL
  - RETAINING WALL REMOVAL
  - TREE REMOVAL (6 TO 15 UNITS DIAMETER)
  - TREE REMOVAL (OVER 15 UNITS DIAMETER)
  - SIGN REMOVAL

- NOTES:**
1. ALL EXISTING SIGNS TO BE REMOVED SHALL BE SALVAGED AND DELIVERED TO THE CITY OF AURORA.
  2. ALL STORM SEWER REMOVAL, CULVERT REMOVAL, DRAINAGE STRUCTURE REMOVAL AND ADJUSTMENTS, AND UTILITY ADJUSTMENTS ARE SHOWN ON THE DRAINAGE AND UTILITIES PLANS 27 AND 33.



MATCH LINE STATION 115+50  
SEE TOP RIGHT

OHIO STREET



FILE NAME =	USER NAME = asmith	DESIGNED - MLB	REVISED -
G:\CH11\0130\Road\Sheets\010130-SHT-RM-2.dgn		DRAWN - MLB	REVISED -
	PLOT SCALE = N/A	CHECKED - MWS	REVISED -
MODELNAME	PLOT DATE = 5/13/2014	DATE = 5/12/14	REVISED -

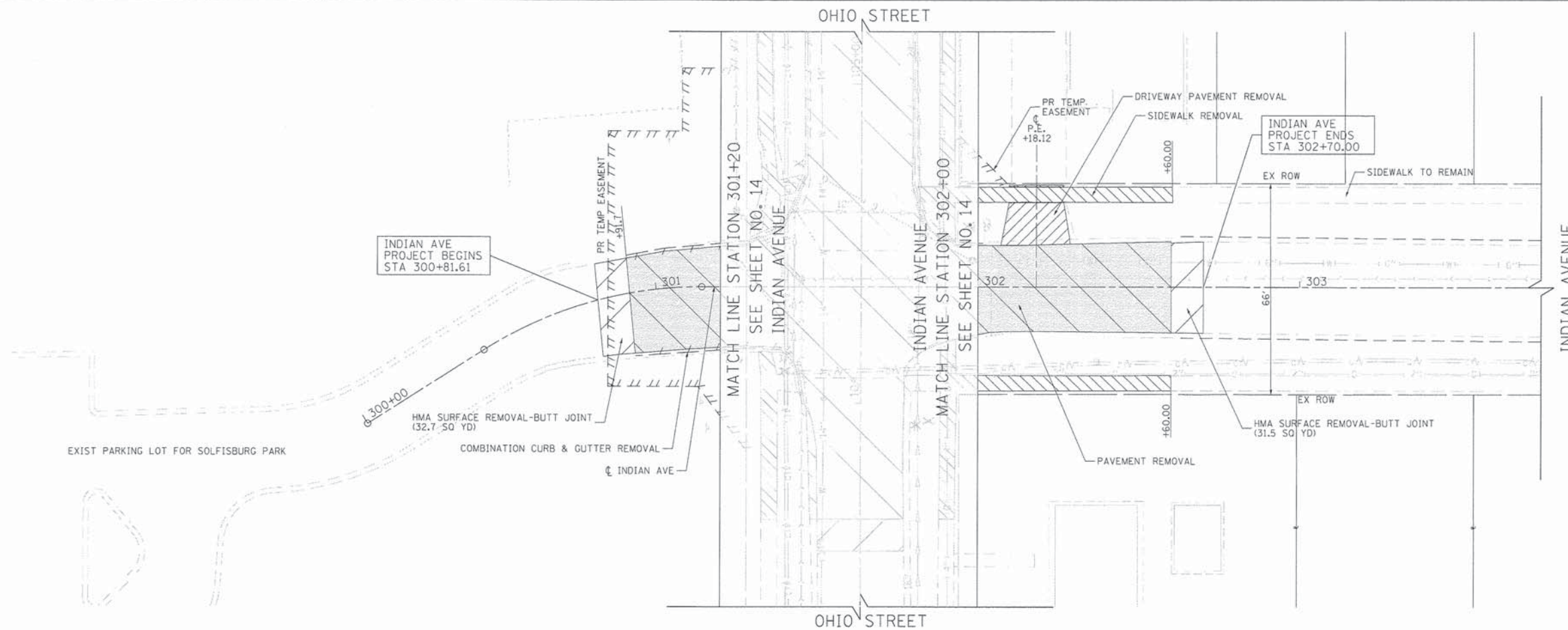
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN**

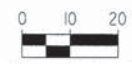
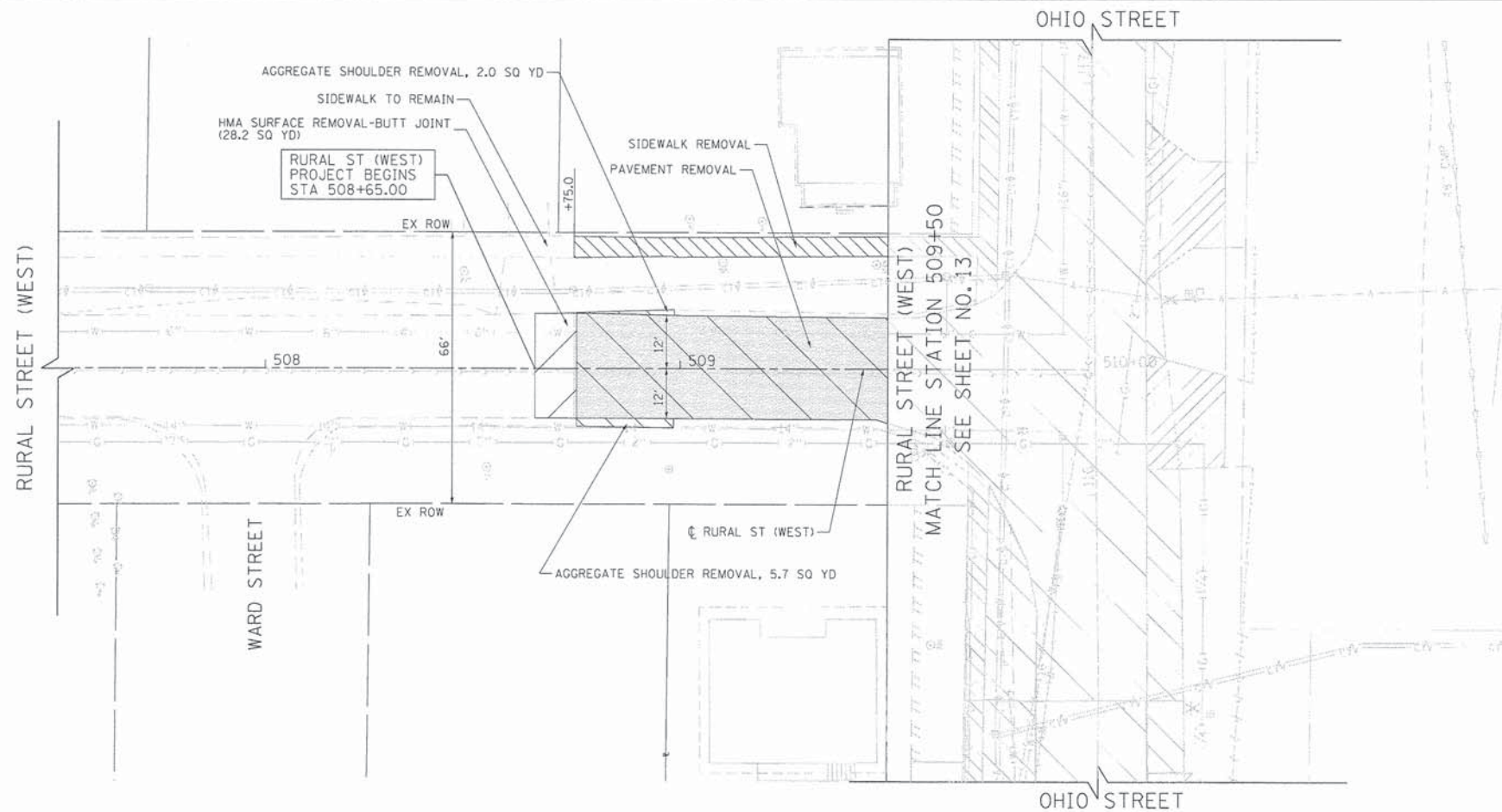
SCALE: 1"=20'    SHEET NO. 14 OF 136 SHEETS    STA. 111+50    TO STA. 119+74.50

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	14
			CONTRACT NO. 63859	
ILLINOIS FED. AID PROJECT				





HORIZONTAL SCALE: 1"=20'



HORIZONTAL SCALE: 1"=20'

- REMOVAL LEGEND**
- BRIDGE REMOVAL
  - PAVEMENT REMOVAL
  - HMA SURFACE REMOVAL - BUTT JOINT
  - COMBINATION CURB AND GUTTER REMOVAL
  - SIDEWALK REMOVAL
  - DRIVEWAY PAVEMENT REMOVAL
  - AGGREGATE SHOULDER REMOVAL
  - FENCE REMOVAL
  - GUARDRAIL REMOVAL
  - RETAINING WALL REMOVAL
  - TREE REMOVAL (6 TO 15 UNITS DIAMETER)
  - TREE REMOVAL (OVER 15 UNITS DIAMETER)
  - SIGN REMOVAL

- NOTES:**
- ALL EXISTING SIGNS TO BE REMOVED SHALL BE SALVAGED AND DELIVERED TO THE CITY OF AURORA.
  - ALL STORM SEWER REMOVAL, CULVERT REMOVAL, DRAINAGE STRUCTURE REMOVAL AND ADJUSTMENTS, AND UTILITY ADJUSTMENTS ARE SHOWN ON THE DRAINAGE AND UTILITIES PLANS 27 AND 33.

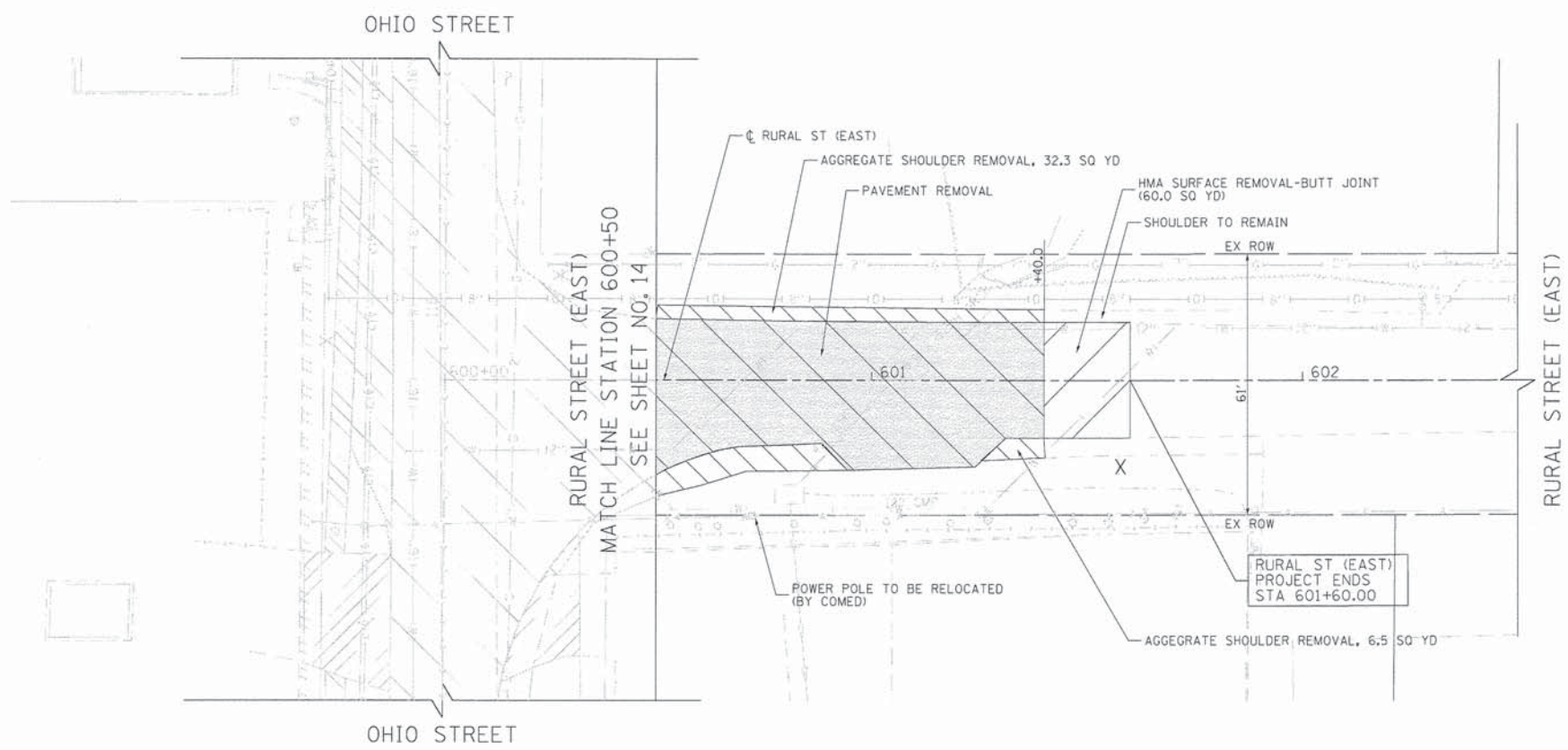
FILE NAME =	USER NAME = a_jpondexter	DESIGNED - MLB	REVISED -
G:\CH11\0130\Road\Sheets\010130-SHT-RM-3.dgn		DRAWN - MLB	REVISED -
		CHECKED - MWS	REVISED -
		DATE - 5/12/14	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>REMOVAL PLAN</b>	
SCALE: 1"=20'	SHEET NO. 15 OF 136 SHEETS
STA. 509+50.00 TO STA. 510+00.00	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	15
				CONTRACT NO. 63859
[ILLINOIS] FED. AID PROJECT				





- REMOVAL LEGEND**
- BRIDGE REMOVAL
  - PAVEMENT REMOVAL
  - HMA SURFACE REMOVAL - BUTT JOINT
  - COMBINATION CURB AND GUTTER REMOVAL
  - SIDEWALK REMOVAL
  - DRIVEWAY PAVEMENT REMOVAL
  - AGGREGATE SHOULDER REMOVAL
  - FENCE REMOVAL
  - GUARDRAIL REMOVAL
  - RETAINING WALL REMOVAL
  - TREE REMOVAL (6 TO 15 UNITS DIAMETER)
  - TREE REMOVAL (OVER 15 UNITS DIAMETER)
  - SIGN REMOVAL

- NOTES:**
1. ALL EXISTING SIGNS TO BE REMOVED SHALL BE SALVAGED AND DELIVERED TO THE CITY OF AURORA.
  2. ALL STORM SEWER REMOVAL, CULVERT REMOVAL, DRAINAGE STRUCTURE REMOVAL AND ADJUSTMENTS, AND UTILITY ADJUSTMENTS ARE SHOWN ON THE DRAINAGE AND UTILITIES PLANS 27 AND 33.



HORIZONTAL SCALE: 1"=20'

FILE NAME *	USER NAME = e.jpondexter	DESIGNED - MLB	REVISED -
G:\CH11\0130\Road\Sheets\0130-SHT-RM-4.dgn		DRAWN - MLB	REVISED -
		CHECKED - MWS	REVISED -
#MODELNAME#	PLOT DATE = 5/12/2014	DATE - 5/12/14	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN**

SCALE: 1"=20'    SHEET NO. 16 OF 136 SHEETS    STA. 600+00.00 TO STA. 600+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	06-00278-00-BR	KANE	136	16
				CONTRACT NO. 63859
ILLINOIS FED. AID PROJECT				

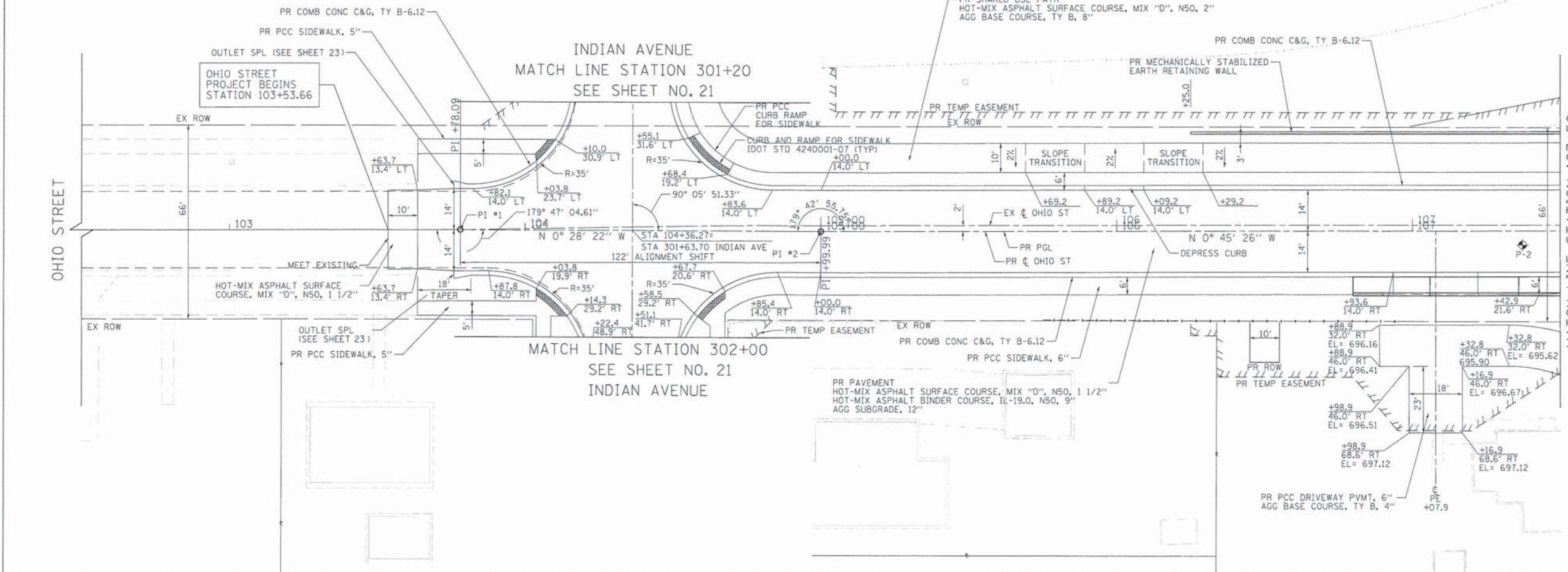


SOLFISBURG BASEBALL FIELDS  
CITY OF AURORA



DATE	
BY	
REVIEWED	
PLOTTED	
ALIGNMENT CHECKED	
GRADES CHECKED	
NOTE BOOK NO.	
FILE NAME	

DATE	
BY	
REVIEWED	
PLOTTED	
GRADES CHECKED	
STRUCTURE NOTATIONS OK'D	
NOTE BOOK NO.	
FILE NAME	

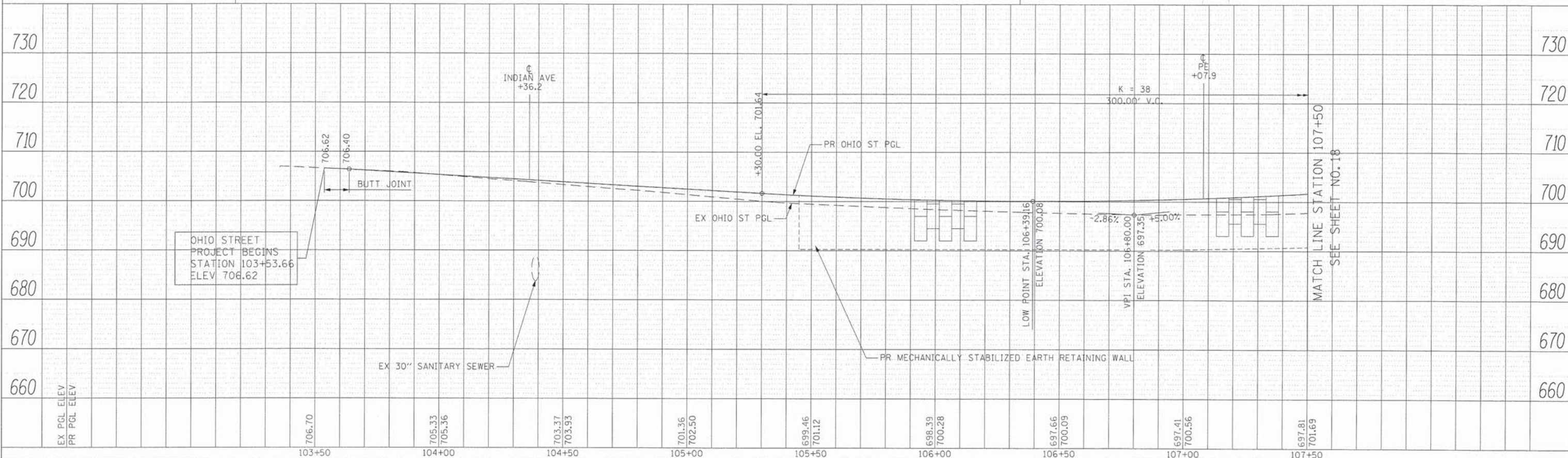


- NOTES:**
- FOR PROPOSED RIGHT-OF-WAY AND TEMPORARY EASEMENT INFORMATION SEE PLAT OF HIGHWAY SHEETS.
  - ALL RADII ARE MEASURED TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
  - ALL DRIVEWAY FLARES SHALL BE 5' (1:1 RATE) UNLESS OTHERWISE NOTED.
  - ALL SIDEWALK RAMPS MUST BE ADA COMPLIANT.
  - FOR INTERSECTIONS PAVING PLANS SEE SHEETS NO. 24 AND 25

**LEGEND**  
◆ SOIL BORING



HORIZONTAL SCALE: 1"=20'



FILE NAME =	USER NAME = a_jpondexter	DESIGNED - MLB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN AND PROFILE</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
G:\CH11\0130\Road\Sheets\010130-SHT-ROAD-1.dgn	PLLOT SCALE = N/A	DRAWN - MLB	REVISED -			4087	08-00278-00-BR	KANE	136	17	
#MODELNAME#	DATE = 5/12/2014	CHECKED - MWS	REVISED -			CONTRACT NO. 63859					
SCALE: 1"=20'						SHEET NO. 17 OF 136 SHEETS		STA. 103+53.66 TO STA. 107+50		ILLINOIS FED. AID PROJECT	









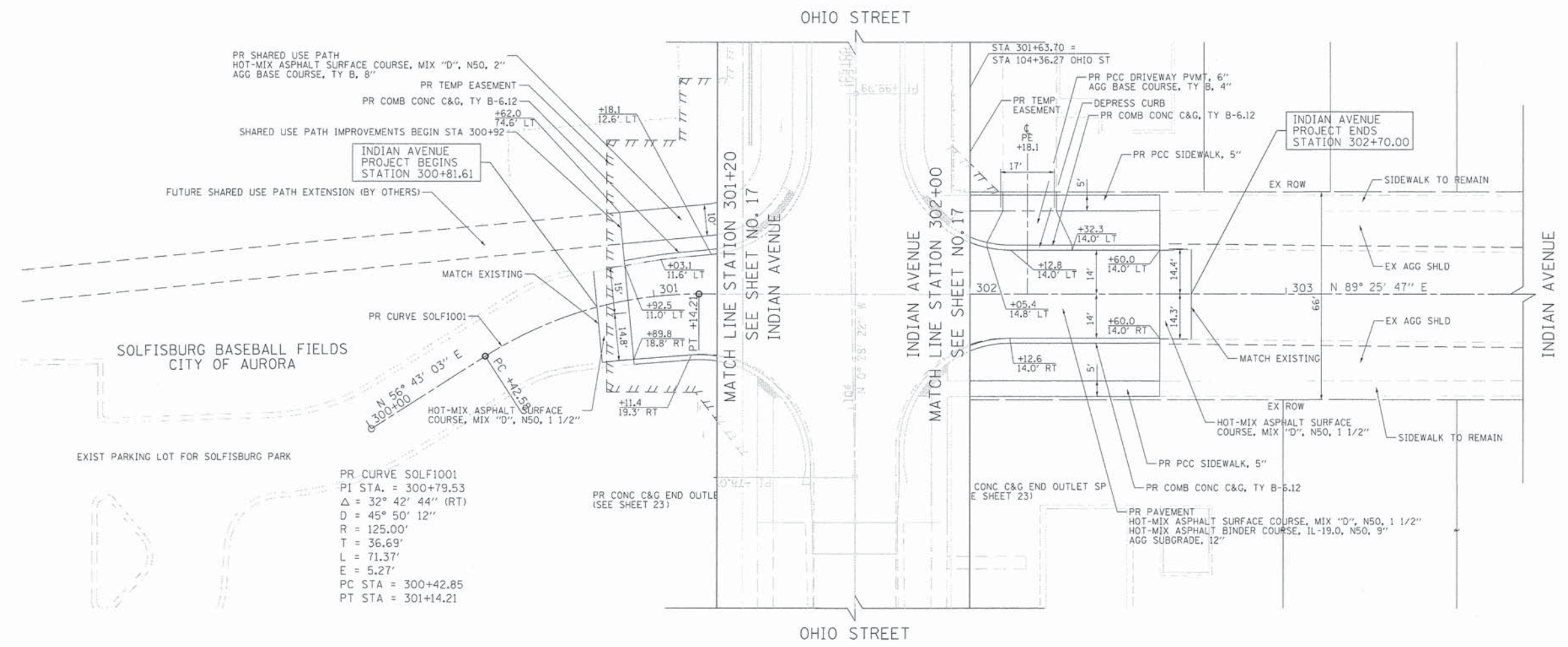






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DESIGNED	
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PLAN	
NOTE BOOK	
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PROFILE	
NOTE BOOK	
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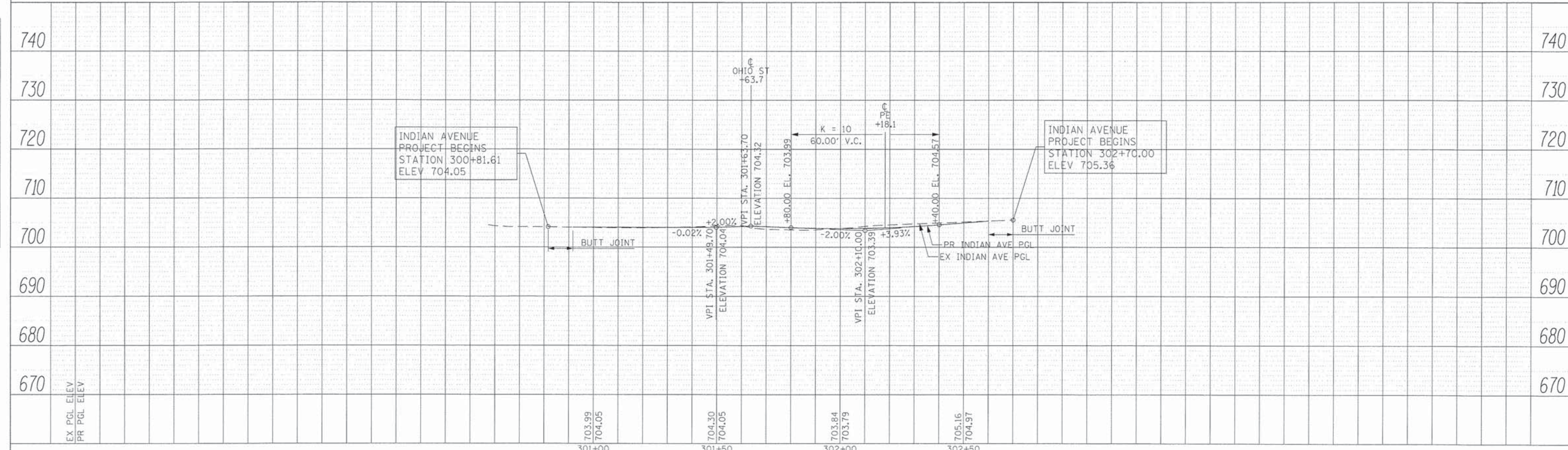


- NOTES:**
- FOR PROPOSED RIGHT-OF-WAY AND TEMPORARY EASEMENT INFORMATION SEE PLAT OF HIGHWAY SHEETS.
  - ALL RADII ARE MEASURED TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
  - ALL DRIVEWAY FLARES SHALL BE 5' (1:1 RATE) UNLESS OTHERWISE NOTED.
  - ALL SIDEWALK RAMPS MUST BE ADA COMPLIANT.
  - FOR INTERSECTIONS PAVING PLANS SEE SHEETS NO. 24 AND 25

**LEGEND**

◆ SOIL BORING

0 10 20  
HORIZONTAL SCALE: 1"=20'



FILE NAME =	USER NAME = epondexter	DESIGNED - MLB	REVISED -	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
G:\CH11\0130\Road\Sheets\0130-SHT-ROAD-5.dgn		DRAWN - MLB	REVISED -	4087	08-00278-00-BR	KANE	136	21
#MODELNAME#	PLDT SCALE = N/A	CHECKED - MWS	REVISED -	CONTRACT NO. 63859				
	PLDT DATE = 5/12/2014	DATE = 5/12/14	REVISED -	ILLINOIS FED. AID PROJECT				

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN AND PROFILE**

SCALE: 1"=20' SHEET NO. 21 OF 136 SHEETS STA. 300+81.61 TO STA. 302+70.00













HORIZONTAL SCALE: 1"=20'

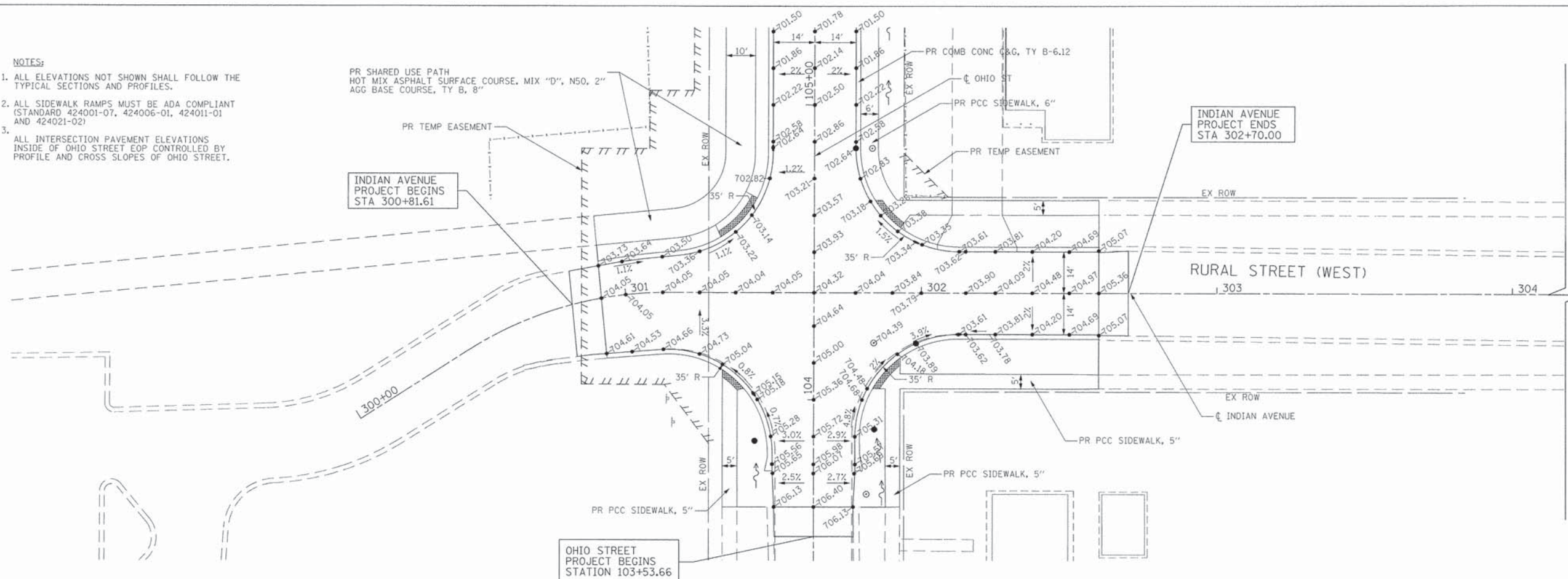
- NOTES:**
1. ALL ELEVATIONS NOT SHOWN SHALL FOLLOW THE TYPICAL SECTIONS AND PROFILES.
  2. ALL SIDEWALK RAMP MUST BE ADA COMPLIANT (STANDARD 424001-07, 424006-01, 424011-01 AND 424021-02)
  3. ALL INTERSECTION PAVEMENT ELEVATIONS INSIDE OF OHIO STREET EOP CONTROLLED BY PROFILE AND CROSS SLOPES OF OHIO STREET.

PR SHARED USE PATH  
HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"  
AGG BASE COURSE, TY B, 8"

INDIAN AVENUE  
PROJECT BEGINS  
STA 300+81.61

OHIO STREET  
PROJECT BEGINS  
STATION 103+53.66

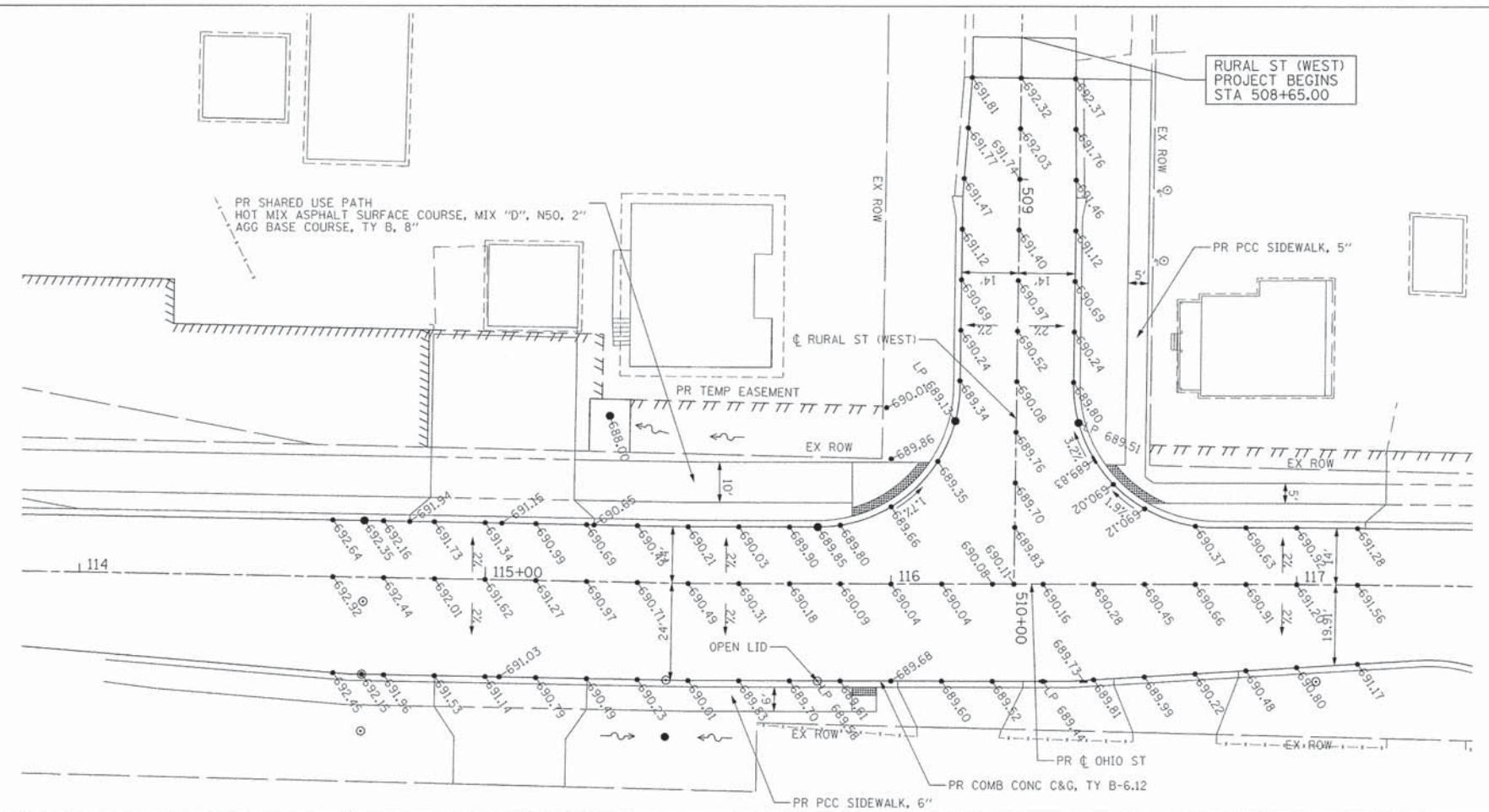
INDIAN AVENUE  
PROJECT ENDS  
STA 302+70.00



HORIZONTAL SCALE: 1"=20'

PR SHARED USE PATH  
HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"  
AGG BASE COURSE, TY B, 8"

RURAL ST (WEST)  
PROJECT BEGINS  
STA 508+65.00

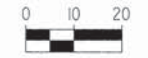
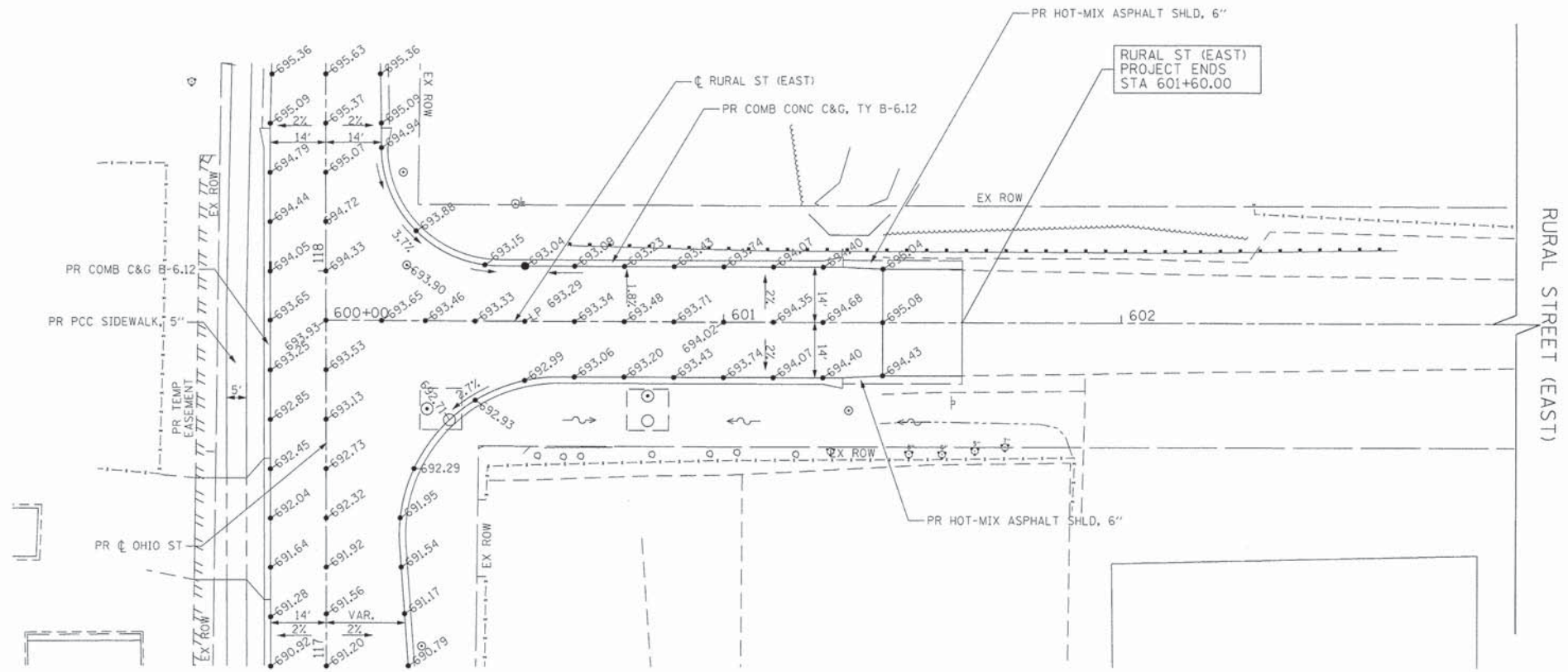


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PLOT SCALE = N/A	CHECKED -- MWS	REVISED --	REVISED --		SCALE: 1"=20'	SHEET NO. 24 OF 136 SHEETS	STA.	CONTRACT NO. 63859		ILLINOIS FED. AID PROJECT	
PLOT DATE = 5/27/2014	DATE -- 5/12/14	REVISED --	REVISED --				TO STA.				



**NOTES:**

1. ALL ELEVATIONS NOT SHOWN SHALL FOLLOW THE TYPICAL SECTIONS AND PROFILES.
2. ALL SIDEWALK RAMP MUST BE ADA COMPLIANT (STANDARD 424001-07, 424006-01, 424011-01 AND 424021-02)
3. ALL INTERSECTION PAVEMENT ELEVATIONS INSIDE OF OHIO STREET EOP CONTROLLED BY PROFILE AND CROSS SLOPES OF OHIO STREET.



HORIZONTAL SCALE: 1"=20'

FILE NAME *	USER NAME * a_jpondexter	DESIGNED - MLB	REVISED -
Gr\CHI1\0130\Road\Sheets\010130-SHT-1-PP-2.dgn		DRAWN - MLB	REVISED -
		CHECKED - MWS	REVISED -
#MODELNAME#	PLOT DATE = 5/12/2014	DATE - 5/12/14	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INTERSECTION PAVING PLAN**

SCALE: 1"=20' SHEET NO. 25 OF 136 SHEETS STA. 600+00.00 TO STA. 601+40.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	25
CONTRACT NO. 63859			ILLINOIS FED. AID PROJECT	



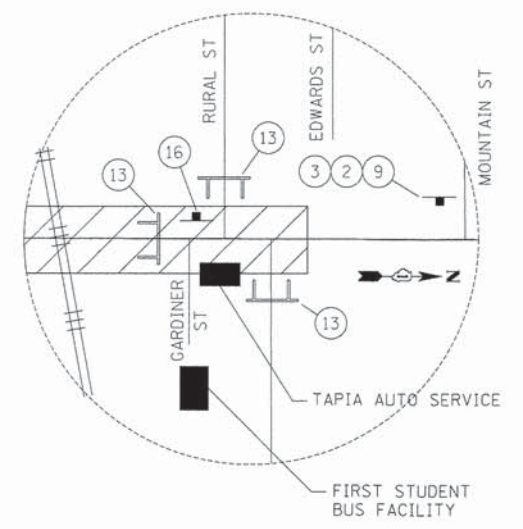
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DATE	
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PLANNED	
NOTED	
NO.	



- LEGEND:**
- WORK ZONE
  - BARRICADE TYPE III WITH LIGHT (ONE SYMBOL SHALL REPRESENT ANY NUMBER OF BARRICADES REQUIRED TO ADEQUATELY PROTECT THE AREA SHOWN)
  - TEMPORARY TRAFFIC ADVISORY SIGN
  - SIGN LEGEND NUMBER
  - DIRECTION OF NORTHBOUND TRAFFIC
  - DIRECTION OF SOUTHBOUND TRAFFIC
  - FLASHING AMBER BEACON
  - CHANGEABLE MESSAGE SIGN

- NOTES:**
1. CONTRACTOR SHALL MAINTAIN TEMPORARY ACCESS ON OHIO STREET FOR "TAPIA AUTO SERVICE" AND "FIRST STUDENT BUS FACILITY".
  2. NO CONSTRUCTION TRAFFIC OR CONSTRUCTION PARKING WILL BE ALLOWED IN THE SOLFISBURG PARKING LOT.
  3. THE CONSTRUCTION OF THE INTERSECTIONS OF OHIO STREET AND RURAL STREET IS TO BE COMPLETED WITHIN 60 DAYS OF THE CLOSING OF THE INTERSECTIONS. SEE BELOW FOR MODIFIED RURAL STREET DETOUR SIGNING UPON THE COMPLETION OF INTERSECTION CONSTRUCTION.
  4. LOCATIONS SHOWN ARE APPROXIMATE.
  5. CONTRACTOR SHALL PROVIDE TEMPORARY ACCESS TO ALL DRIVEWAYS TO REMAIN OPEN. DRIVEWAY ENTRANCE SIGNS TO BE PROVIDED AT DRIVEWAYS TO REMAIN OPEN.
  6. CONTRACTOR SHALL PROVIDE TEMPORARY ACCESS TO SOLFISBURG PARK. PARK ENTRANCE SIGNS TO BE PROVIDED AT THE SOLFISBURG PARK ENTRANCE. THE CONSTRUCTION OF THE INTERSECTIONS OF OHIO STREET AND INDIAN STREET IS TO BE COMPLETED WITHIN 60 DAYS OF THE CLOSING OF THE INTERSECTIONS.



SIGN LEGEND

 1 W20-2 48"X48"	 2 36"X12"	 3 M3-1 24"X12"	 4 M3-3 21"X12"	 5 M4-9-S 30"X24"	 6 M4-9-AR 30"X24"	 7 M4-9-AL 30"X24"	 8 M4-9-R 30"X24"	 9 M4-9-L 30"X24"	 18 R11-3b 60"X30"
 10 M4-8A 24"X18"	 11 M4-10-R 48"X18"	 12 M4-10-L 48"X18"	 13 R11-2 48"X30"	 14 R11-4 60"X30"	 15 W20-3 48"X48"	 16 R3-2 24"X24"	 17 36"X12"	 19 30"X30"	

FILE NAME = G:\CH11\0130\Road\Sheets\10130-SHT-DETOUR.dgn	USER NAME = ajpdxeter	DESIGNED - YSK	REVISED -
		DRAWN - YSK	REVISED -
		CHECKED - MWS	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

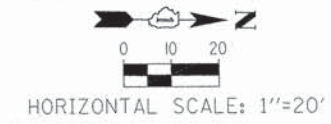
**MAINTENANCE OF TRAFFIC - DETOUR PLAN**

SHEET NO. 26 OF 136 SHEETS STA. TO STA.

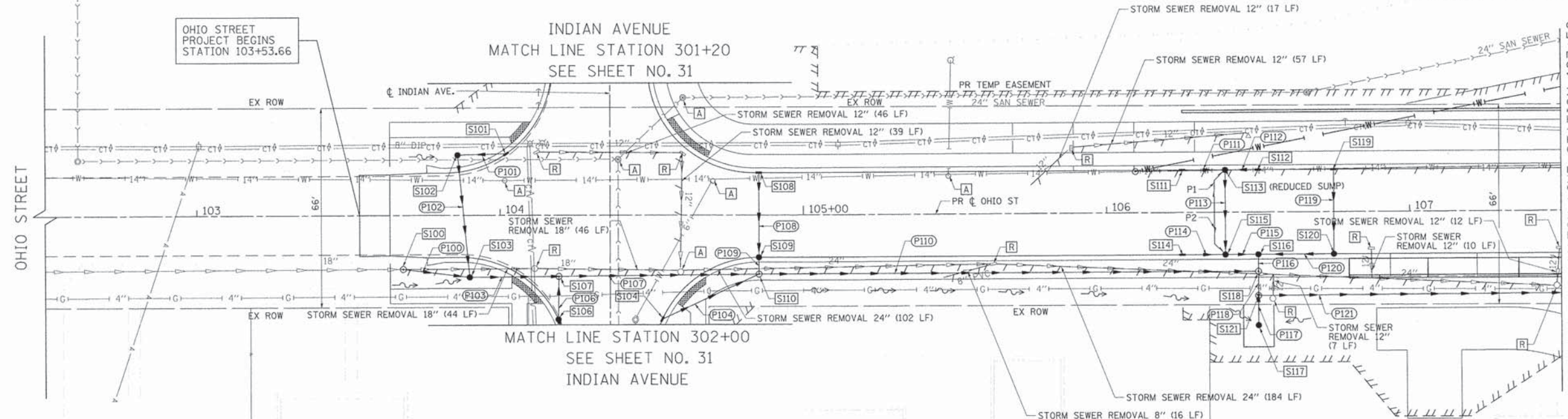
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	26
CONTRACT NO. 63859				
ILLINOIS FED. AID PROJECT				



SOLFISBURG BASEBALL FIELDS  
CITY OF AURORA



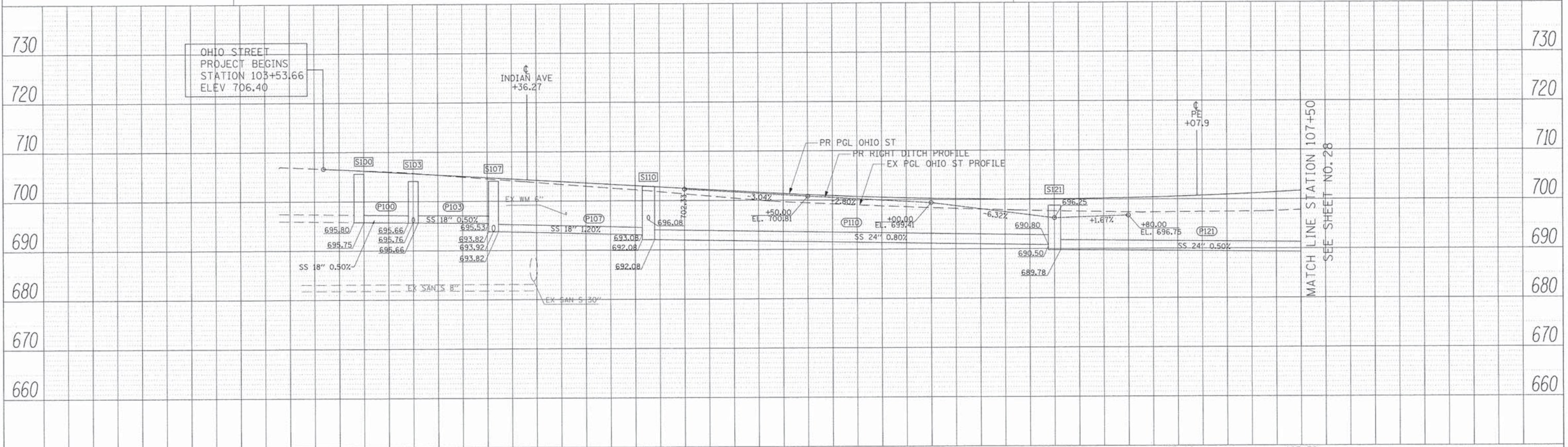
- LEGEND:**
- EXISTING INLET
  - EXISTING CATCH BASIN
  - EXISTING MANHOLE
  - EXISTING STORM SEWER
  - - - EXISTING PIPE CULVERT
  - LINEAL REMOVAL ITEM
  - PROPOSED INLET
  - PROPOSED CATCH BASIN
  - PROPOSED MANHOLE
  - - - PROPOSED DITCH
  - ◄ PROPOSED END SECTION
  - PROPOSED STORM SEWER
  - S100 PROPOSED DRAINAGE STRUCTURE
  - P100 PROPOSED DRAINAGE PIPE
  - R REMOVE EXISTING STRUCTURE
  - A ADJUST EXISTING STRUCTURE



- NOTES:**
- 1) FOR STORM SEWER NOTES, SEE SHEET NO. 2.
  - 2) ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE NOTED.
  - 3) ALL STATIONS AND OFFSETS ARE FROM THE PROPOSED ROADWAY CENTERLINE, UNLESS OTHERWISE NOTED.
  - 4) FOR DRAINAGE STRUCTURE AND PIPE SCHEDULE, SEE SHEET NOS. 7 & 8.
  - 5) ALL EXISTING STORM SEWER SHALL REMAIN IN SERVICE UNTIL THE NEW STORM SEWER IS INSTALLED AND PLACED INTO SERVICE. NEW STORM SEWER SHALL BE TEMPORARILY CONNECTED TO THE EXISTING STORM SEWER AT THE END OF EACH WORKING DAY.
  - 6) ALL EXISTING SANITARY SEWER SHALL REMAIN IN SERVICE UNTIL THE NEW SANITARY SEWER IS INSTALLED, TESTED, AND PLACED INTO SERVICE.

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FILE NAME: G:\CH1\10130\Road\Sheets\DI0130-SHT-DRAIN-	USER NAME: a_jpandexter	DESIGNED: YSK	REVISED: -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DRAINAGE AND UTILITIES PLAN AND PROFILE</b>	F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 27		
#MODELNAME:	PLOT SCALE: N/A	CHECKED: MWS	REVISED: -			SCALE: 1"=20' SHEET NO. 27 OF 136 SHEETS STA. 103+53.66 TO STA. 107+50		CONTRACT NO. 63859				
	PLOT DATE: 6/27/2014	DATE: 5/12/14	REVISED: -			ILLINOIS FED. AID PROJECT						















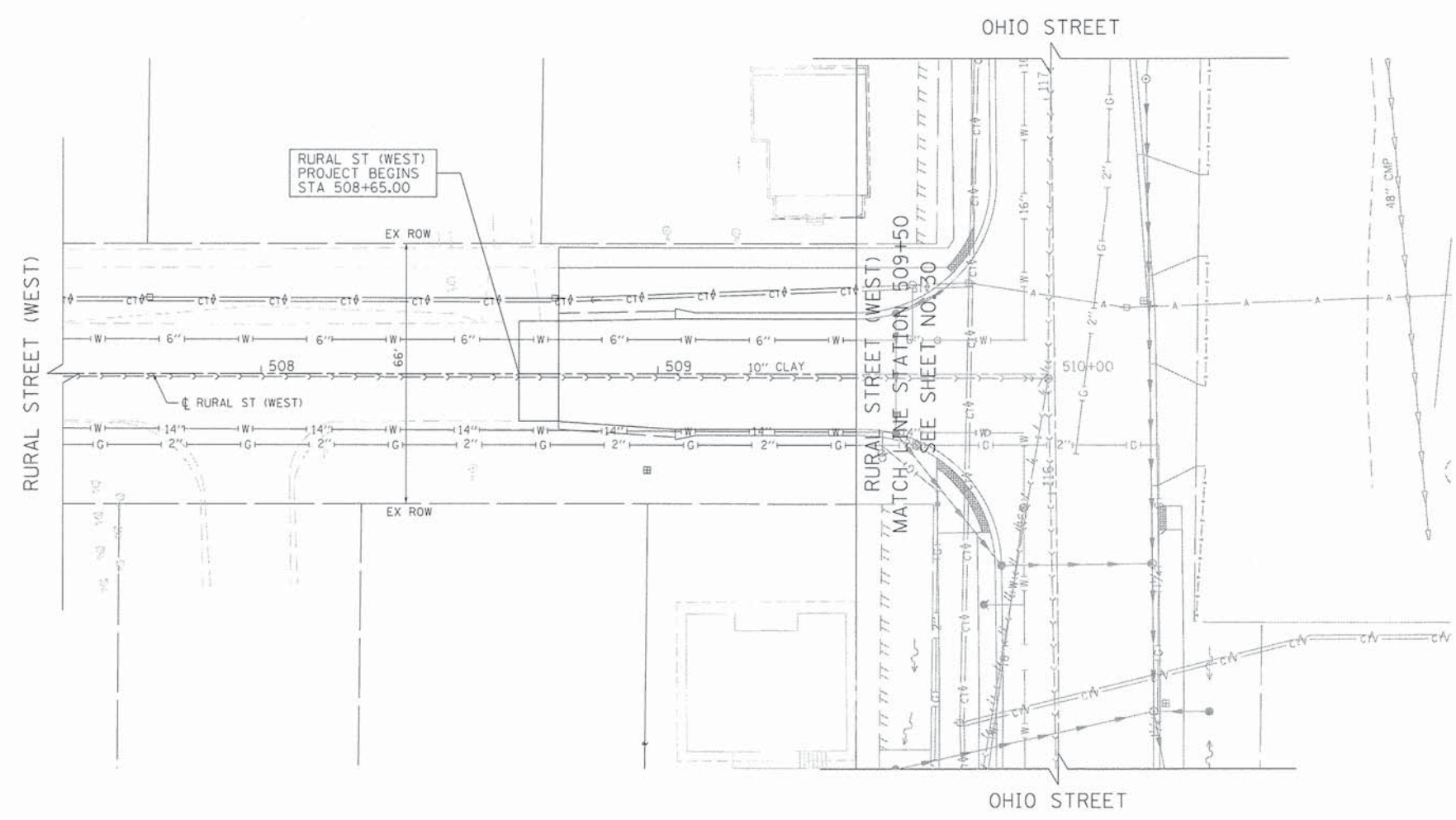






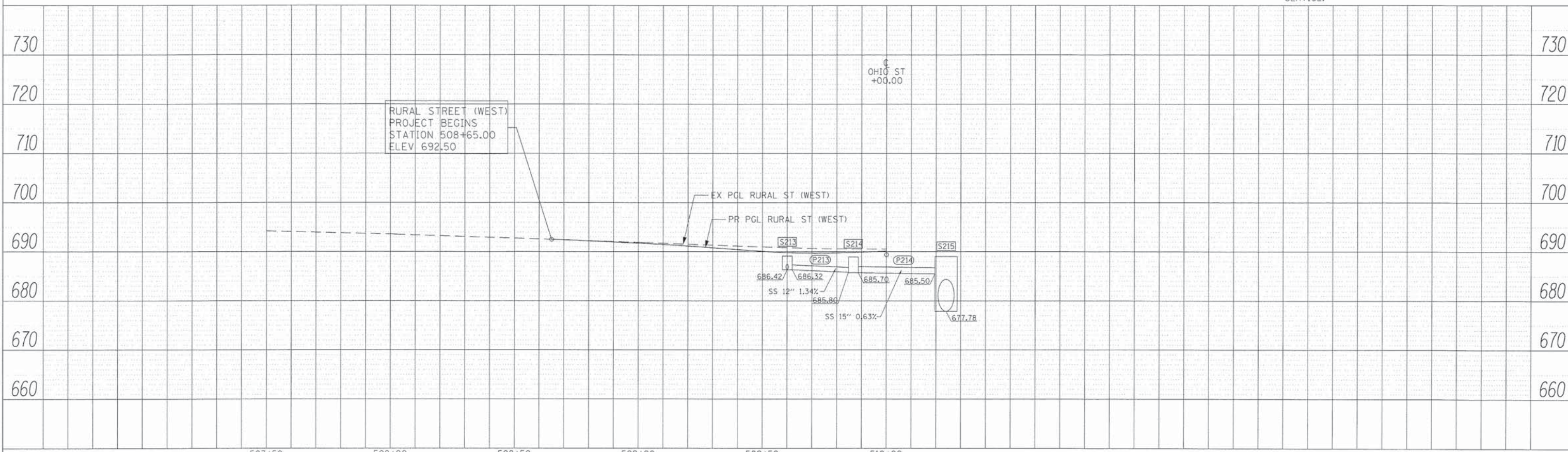
HORIZONTAL SCALE: 1"=20'

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- LEGEND:**
- ◻ EXISTING INLET
  - EXISTING CATCH BASIN
  - EXISTING MANHOLE
  - EXISTING STORM SEWER
  - - - EXISTING PIPE CULVERT
  - LINEAL REMOVAL ITEM
  - PROPOSED INLET
  - PROPOSED CATCH BASIN
  - PROPOSED MANHOLE
  - - - PROPOSED DITCH
  - ◄ PROPOSED END SECTION
  - PROPOSED STORM SEWER
  - [S100] PROPOSED DRAINAGE STRUCTURE
  - [P100] PROPOSED DRAINAGE PIPE
  - [R] REMOVE EXISTING STRUCTURE
  - [A] ADJUST EXISTING STRUCTURE
- NOTES:**
- 1) FOR STORM SEWER NOTES, SEE SHEET NO. 2.
  - 2) ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE NOTED.
  - 3) ALL STATIONS AND OFFSETS ARE FROM THE PROPOSED ROADWAY CENTERLINE, UNLESS OTHERWISE NOTED.
  - 4) FOR DRAINAGE STRUCTURE AND PIPE SCHEDULE, SEE SHEET NOS. 7 & 8.
  - 5) ALL EXISTING STORM SEWER SHALL REMAIN IN SERVICE UNTIL THE NEW STORM SEWER IS INSTALLED AND PLACED INTO SERVICE. NEW STORM SEWER SHALL BE TEMPORARILY CONNECTED TO THE EXISTING STORM SEWER AT THE END OF EACH WORKING DAY.
  - 6) ALL EXISTING SANITARY SEWER SHALL REMAIN IN SERVICE UNTIL THE NEW SANITARY SEWER IS INSTALLED, TESTED, AND PLACED INTO SERVICE.

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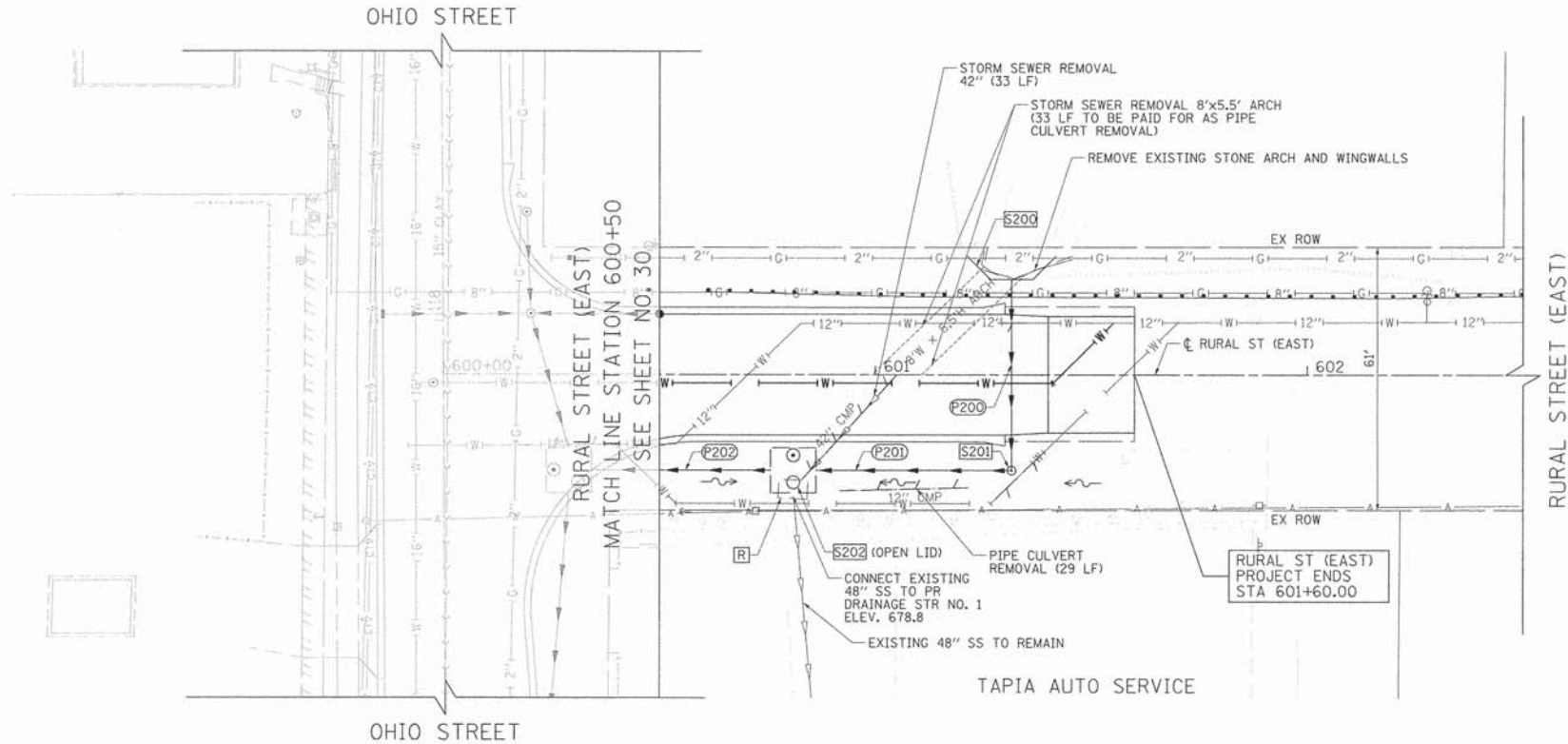


FILE NAME *	USER NAME = a.pandexter	DESIGNED - YSK	REVISED -	<p align="center"><b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b></p> <p align="center"><b>DRAINAGE AND UTILITIES PLAN AND PROFILE</b></p>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
G:\CH11\0130\Road\Sheets\DI0130-SHT-DRAIN-6.dgn		DRAWN - YSK	REVISED -		4087	08-00278-00-BR	KANE	136	32
*MODELNAME*		CHECKED - MWS	REVISED -		CONTRACT NO. 63859				
		DATE - 5/12/14	REVISED -		ILLINOIS FED. AID PROJECT				
				SCALE: 1"=20'	SHEET NO. 32 OF 136 SHEETS		STA. 508+65.00 TO STA. 510+00.00		



PLAN	NO.	DATE
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PROFILE	NO.	DATE
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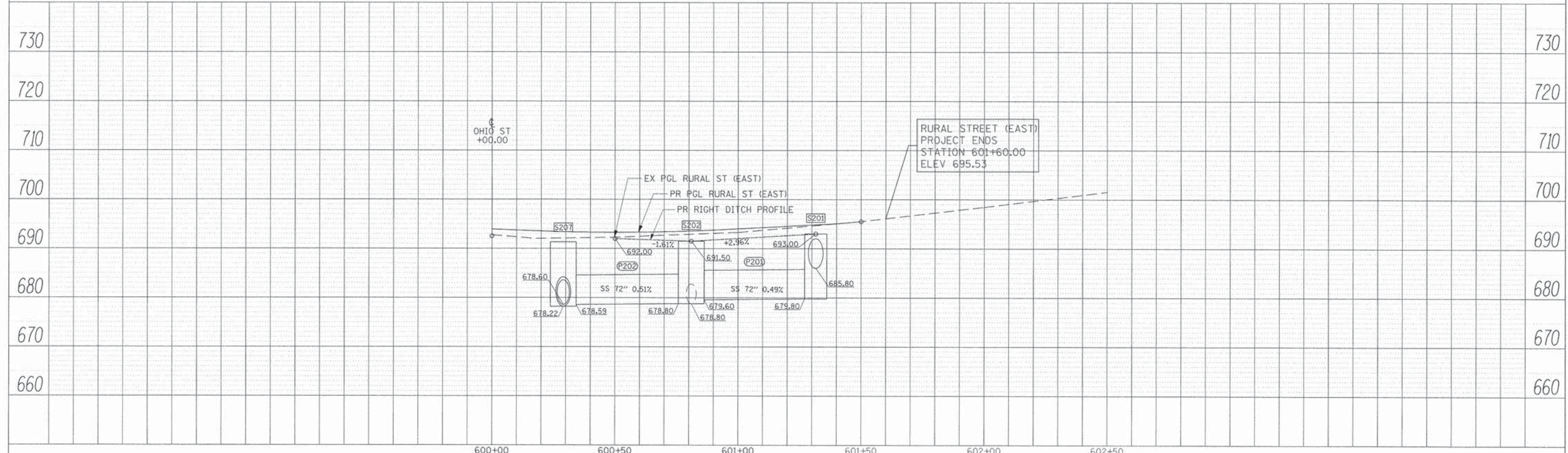


- LEGEND:**
- EXISTING INLET
  - EXISTING CATCH BASIN
  - EXISTING MANHOLE
  - EXISTING STORM SEWER
  - EXISTING PIPE CULVERT
  - LINEAL REMOVAL ITEM
  - PROPOSED INLET
  - PROPOSED CATCH BASIN
  - PROPOSED MANHOLE
  - PROPOSED DITCH
  - ◄ PROPOSED END SECTION
  - PROPOSED STORM SEWER
  - S100 PROPOSED DRAINAGE STRUCTURE
  - P100 PROPOSED DRAINAGE PIPE
  - R REMOVE EXISTING STRUCTURE
  - A ADJUST EXISTING STRUCTURE

- NOTES:**
- 1) FOR STORM SEWER NOTES, SEE SHEET NO. 2.
  - 2) ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE NOTED.
  - 3) ALL STATIONS AND OFFSETS ARE FROM THE PROPOSED ROADWAY CENTERLINE, UNLESS OTHERWISE NOTED.
  - 4) FOR DRAINAGE STRUCTURE AND PIPE SCHEDULE, SEE SHEET NOS. 7 & 8.
  - 5) ALL EXISTING STORM SEWER SHALL REMAIN IN SERVICE UNTIL THE NEW STORM SEWER IS INSTALLED AND PLACED INTO SERVICE. NEW STORM SEWER SHALL BE TEMPORARILY CONNECTED TO THE EXISTING STORM SEWER AT THE END OF EACH WORKING DAY.
  - 6) ALL EXISTING SANITARY SEWER SHALL REMAIN IN SERVICE UNTIL THE NEW SANITARY SEWER IS INSTALLED, TESTED, AND PLACED INTO SERVICE.



HORIZONTAL SCALE: 1"=20'

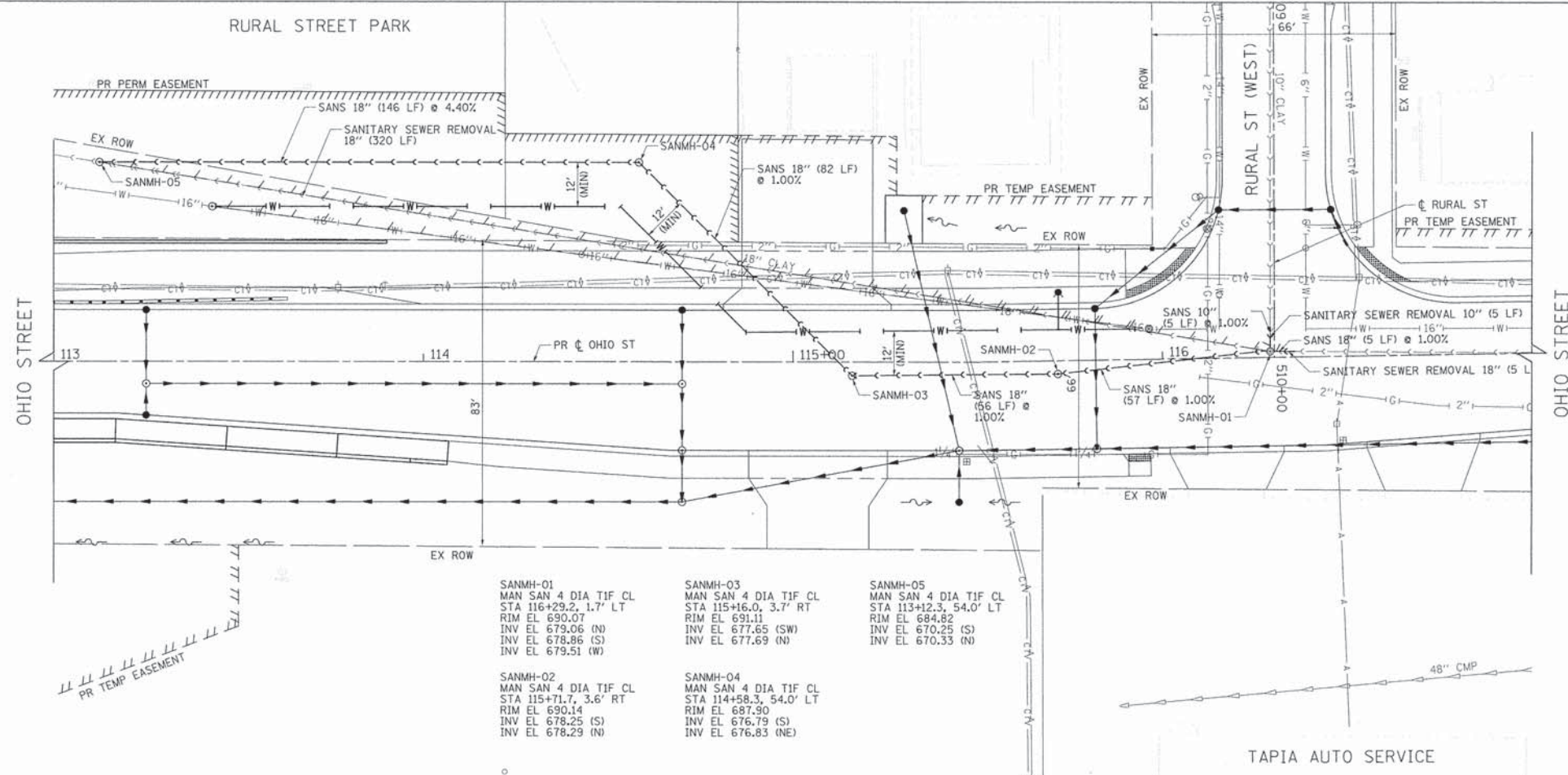


FILE NAME =	USER NAME = bafex	DESIGNED - YSK	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DRAINAGE AND UTILITIES PLAN AND PROFILE</b>	F.A.P. R.T.E. = 4087	SECTION = 08-00278-00-BR	COUNTY = KANE	TOTAL SHEETS = 136	SHEET NO. = 33		
PILOT SCALE = N/A	CHECKED - MWS	REVISED -	REVISED -			SCALE: 1"=20	SHEET NO. 33 OF 136 SHEETS	STA. 600+50	TO STA. 601+60	CONTRACT NO. 63859		
#MODELNAME#	PLLOT DATE = 7/16/2014	DATE - 5/12/14	REVISED -			ILLINOIS FED. AID PROJECT						



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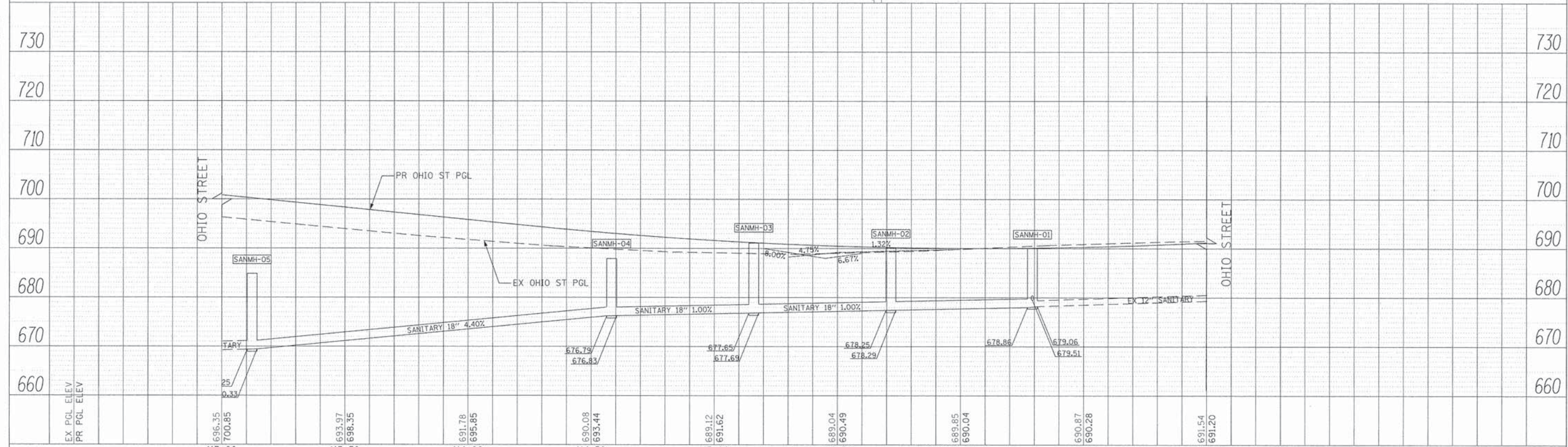


- LEGEND:**
- ◻ EXISTING INLET
  - EXISTING CATCH BASIN
  - EXISTING MANHOLE
  - EXISTING STORM SEWER
  - - - EXISTING PIPE CULVERT
  - LINEAL REMOVAL ITEM
  - ← PROPOSED SANITARY SEWER
  - PROPOSED SANITARY MANHOLE

- NOTES:**
- 1) ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE NOTED.
  - 2) ALL STATIONS AND OFFSETS ARE FROM THE PROPOSED ROADWAY CENTERLINE, UNLESS OTHERWISE NOTED.



<b>SANMH-01</b> MAN SAN 4 DIA TIF CL STA 116+29.2, 1.7' LT RIM EL 690.07 INV EL 679.06 (N) INV EL 678.86 (S) INV EL 679.51 (W)	<b>SANMH-03</b> MAN SAN 4 DIA TIF CL STA 115+16.0, 3.7' RT RIM EL 691.11 INV EL 677.65 (SW) INV EL 677.69 (N)	<b>SANMH-05</b> MAN SAN 4 DIA TIF CL STA 113+12.3, 54.0' LT RIM EL 684.82 INV EL 670.25 (S) INV EL 670.33 (N)
<b>SANMH-02</b> MAN SAN 4 DIA TIF CL STA 115+71.7, 3.6' RT RIM EL 690.14 INV EL 678.25 (S) INV EL 678.29 (N)	<b>SANMH-04</b> MAN SAN 4 DIA TIF CL STA 114+58.3, 54.0' LT RIM EL 687.90 INV EL 676.79 (S) INV EL 676.83 (NE)	



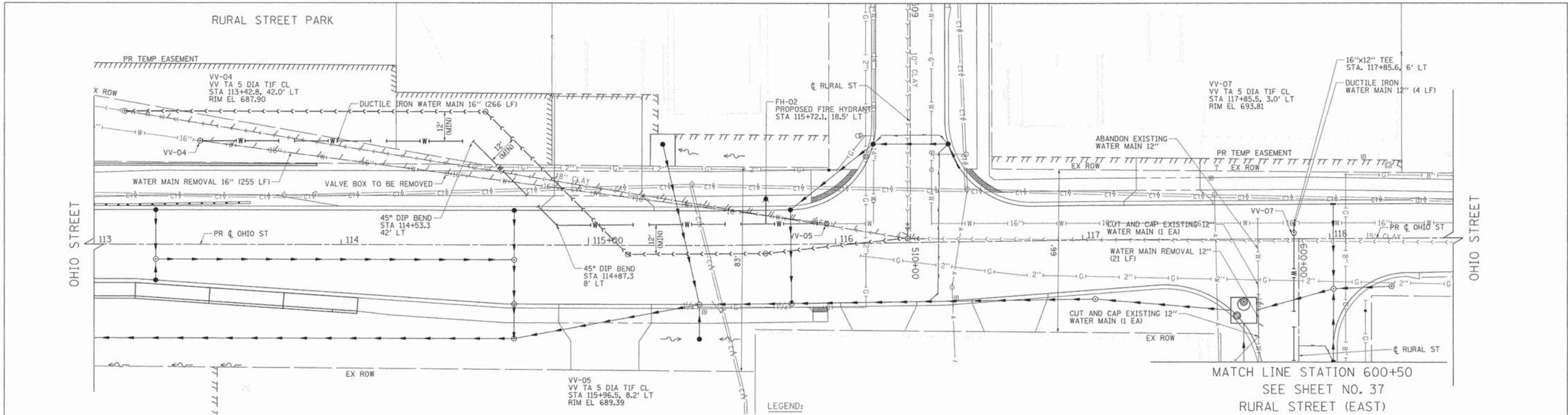
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G:\CH11\0130\Road\Sheets\010130-SHT-SSEWER-1.dgn		DRAWN - YSK	REVISED -		SCALE: 1"=20'	SHEET NO. 29 OF 136 SHEETS	STA. 111+50	TO STA. 115+50
PLOT SCALE = N/A		CHECKED - MWS	REVISED -		F.A.P. SECTION COUNTY TOTAL SHEETS SHEET NO. 4087 08-00278-00-BR KANE 136 34			
#MODELNAME#	PLOT DATE = 6/27/2014	DATE - 5/12/14	REVISED -		CONTRACT NO. 63859 ILLINOIS FED. AID PROJECT			







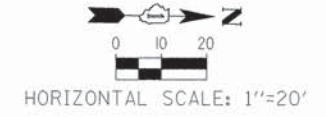
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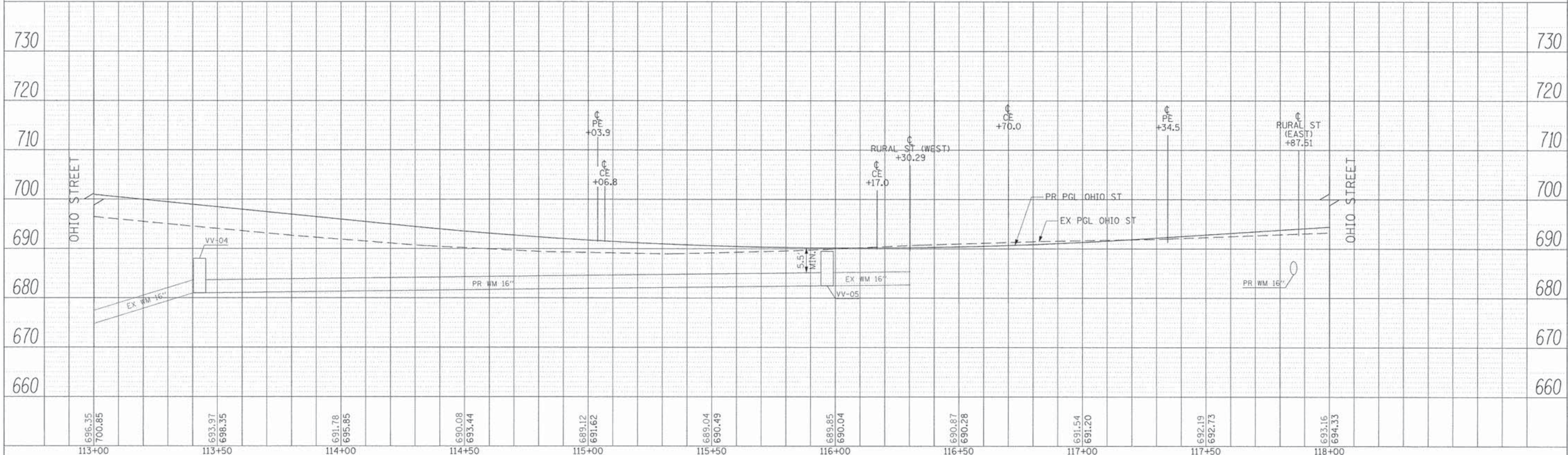
- LEGEND:**
- EXISTING INLET
  - EXISTING CATCH BASIN
  - EXISTING MANHOLE
  - EXISTING STORM SEWER
  - EXISTING PIPE CULVERT
  - LINEAL REMOVAL ITEM
  - PROPOSED INLET
  - PROPOSED CATCH BASIN
  - PROPOSED MANHOLE
  - PROPOSED DITCH
  - ▲ PROPOSED END SECTION
  - PROPOSED STORM SEWER

**NOTES:**

- 1) ALL DIMENSIONS ARE IN FEET, UNLESS OTHERWISE NOTED.
- 2) ALL STATIONS AND OFFSETS ARE FROM THE PROPOSED ROADWAY CENTERLINE, UNLESS OTHERWISE NOTED.



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FILE NAME = G:\CH11\0130\Road\Sheets\010130-SHT-WM-2.dgn	USER NAME = a_jpondexter	DESIGNED - YSK	REVISOR -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>WATER MAIN PLAN AND PROFILE</b>	F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 36		
#MODELNAME#	PLOT SCALE = N/A	CHECKED - MWS	REVISOR -			SCALE: 1"=20	SHEET NO. 36 OF 136 SHEETS	STA. 103+53.66	TO STA. 107+50	CONTRACT NO. 63859		
	PLOT DATE = 6/27/2014	DATE - 5/12/14	REVISOR -			ILLINOIS FED. AID PROJECT						





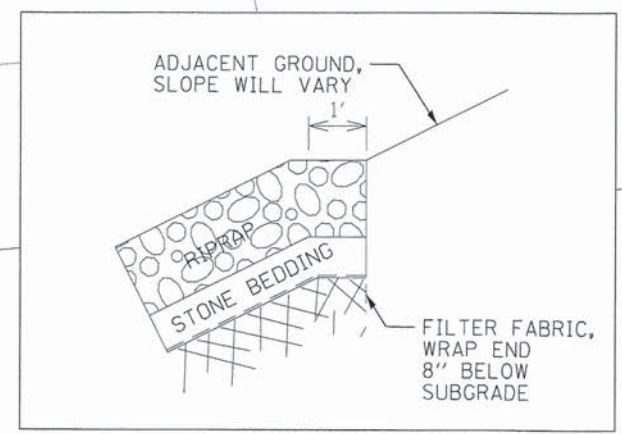
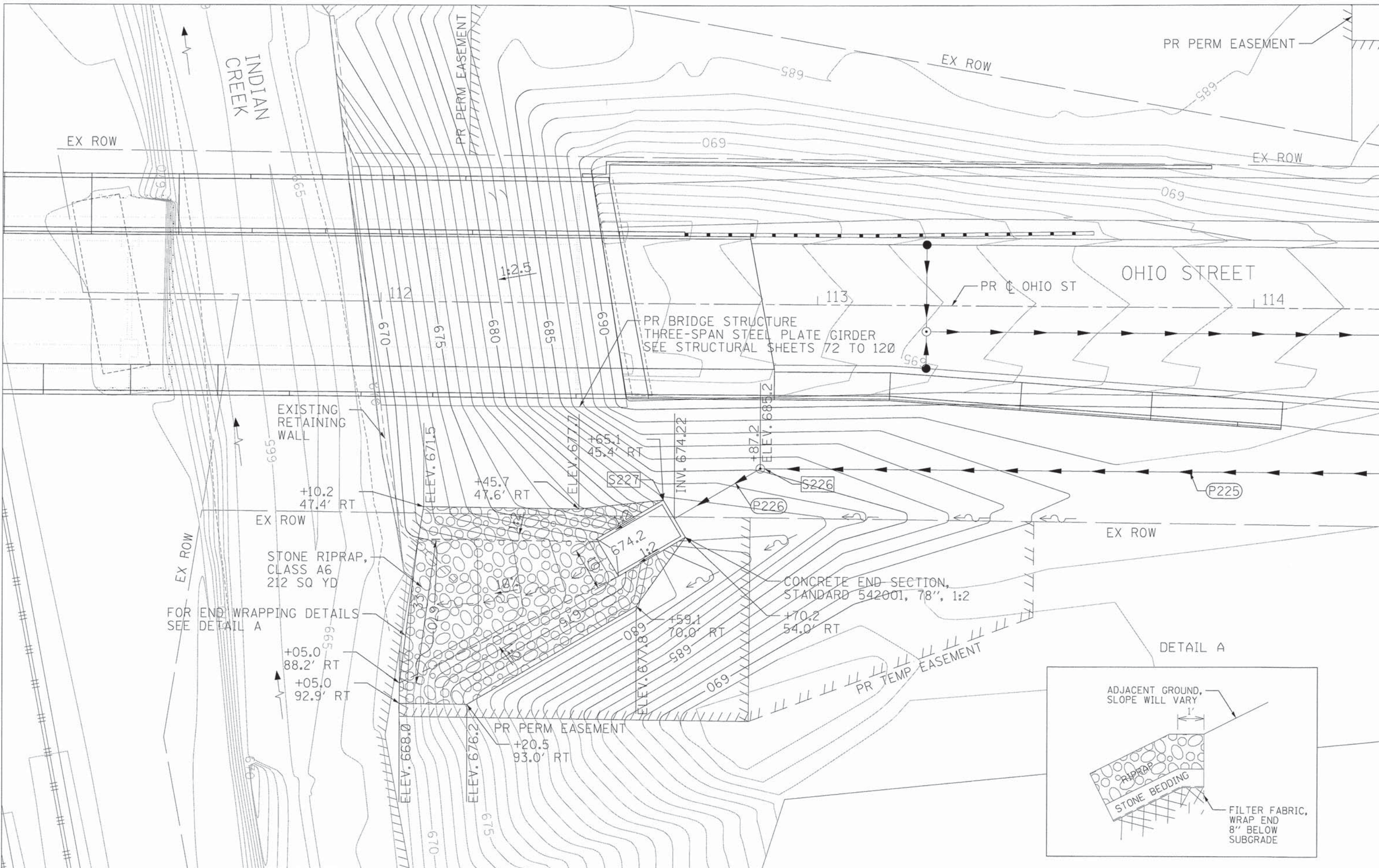






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FILE NAME =	USER NAME = ajpandexter	DESIGNED - BSH	REVISED -
G:\CHI1\0130\Road\Sheets\010130-SHT-DET-RIPRAP.dgn		DRAWN - BSH	REVISED -
		CHECKED - MWS	REVISED -
		DATE - 5/12/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

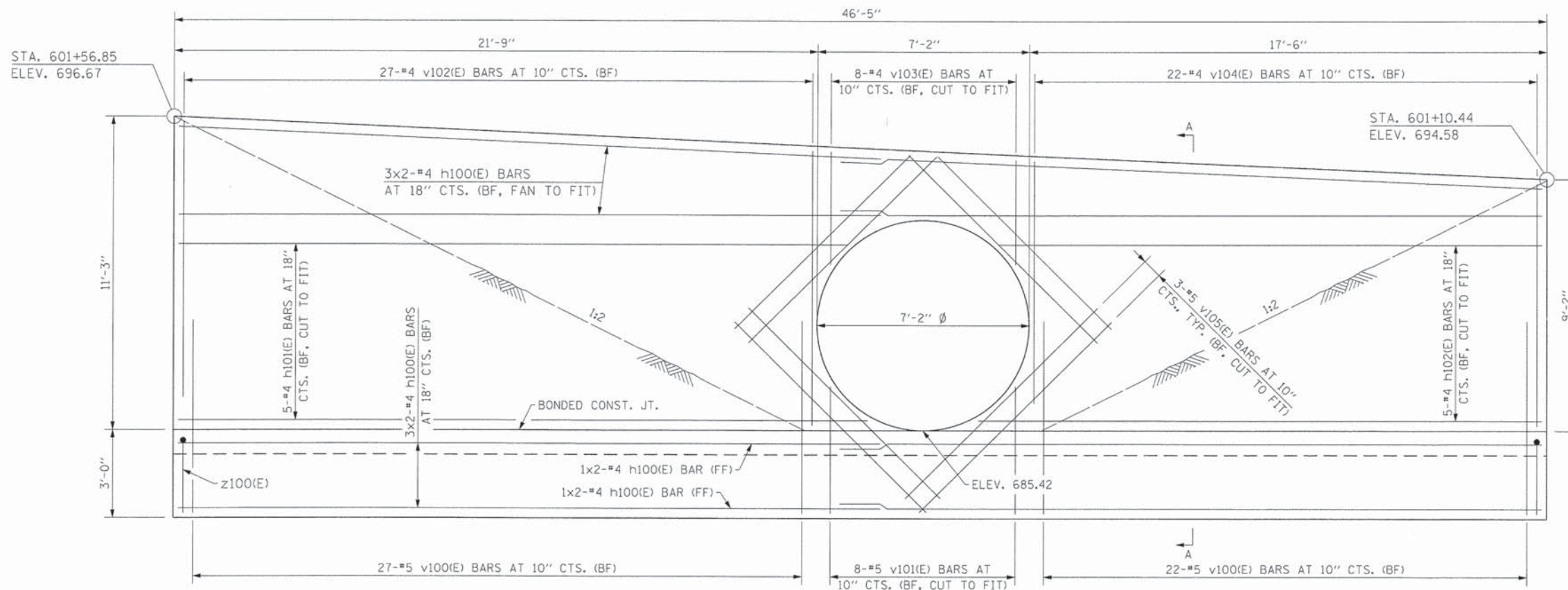
<b>RIPRAP GRADING DETAILS</b>	
SCALE: 1"=10'	SHEET NO. 39 OF 136 SHEETS
STA. N/A	TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	39
				CONTRACT NO. 63859
ILLINOIS FED. AID PROJECT				

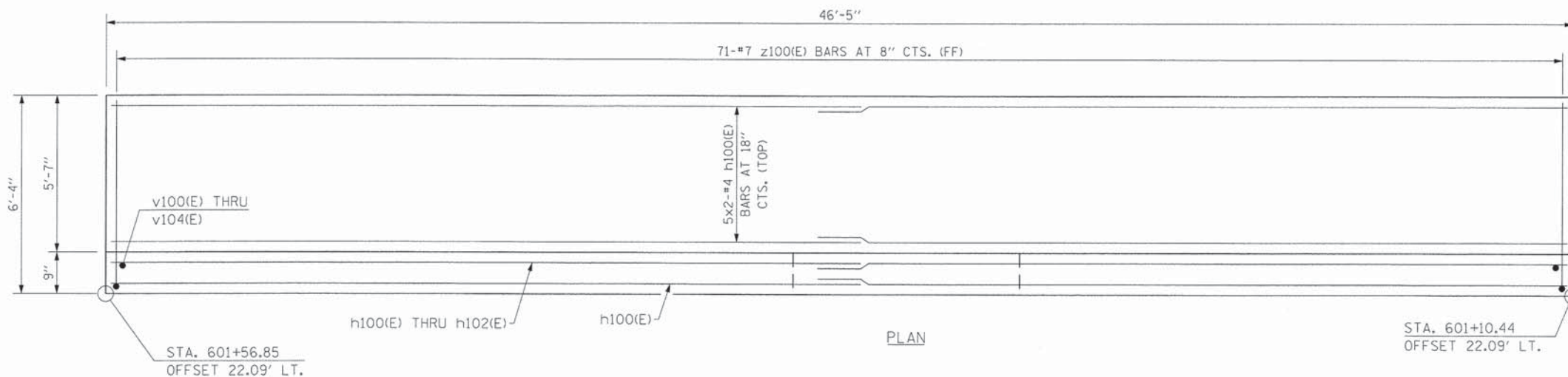


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ELEVATION  
(LOOKING SOUTH)

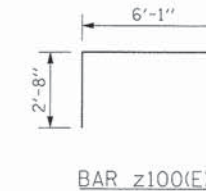


PLAN

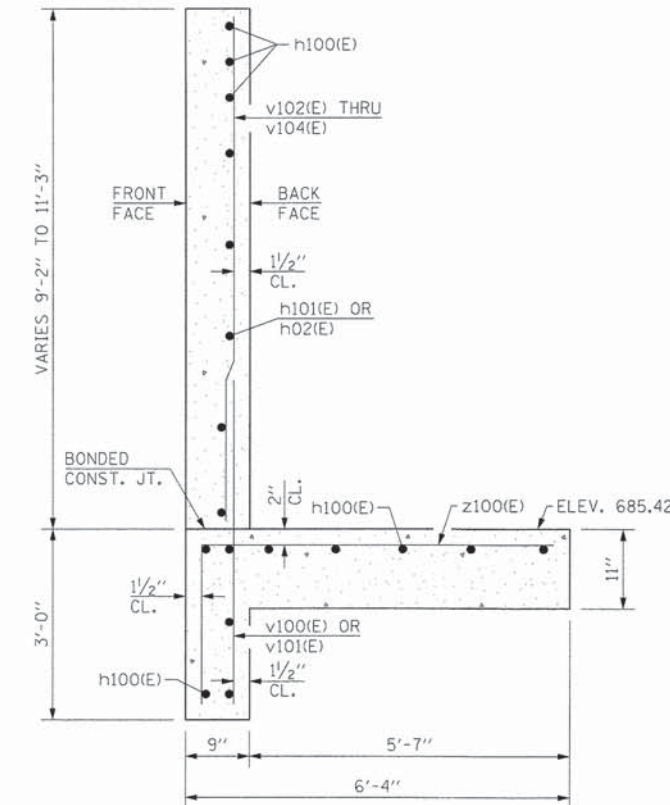
- NOTES:
1. A PRECAST REINFORCED CONCRETE ALTERNATE SHALL BE ALLOWED FOR THIS STRUCTURE. SHOP PLANS AND CALCULATIONS SHALL BE SUBMITTED BY THE CONTRACTOR TO THE ENGINEER ACCORDING TO THE APPLICABLE REQUIREMENTS OF ARTICLE 504 OF THE STANDARD SPECIFICATIONS.
  2. THE CONTRACT UNIT PRICE FOR EACH SHALL INCLUDE ALL EXCAVATION, BACKFILL, CONCRETE, REINFORCEMENT AND OTHER INCIDENTALS AS SPECIFIED ON THIS DETAIL AND AS DIRECTED BY THE ENGINEER.
  3. ALL INVERTS AND ELEVATIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO ORDERING MATERIALS AND CONSTRUCTION. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED WITHIN THE CONTRACT UNIT PRICE FOR CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 72".
  4. STATIONS AND OFFSETS PROVIDED ARE AT THE FRONT FACE OF THE WALL.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
CAST-IN PLACE REINFORCED CONCRETE END SECTIONS 72"	EACH	1



BAR z100(E)



SECTION A-A

BILL OF MATERIAL  
(FOR INFORMATION ONLY)

BAR	NO.	SIZE	LENGTH	SHAPE	
h100(E)	26	#4	24'-5"	—	
h101(E)	5	#4	23'-8"	—	
h102(E)	5	#4	19'-5"	—	
v100(E)	49	#5	7'-3"	—	
v101(E)	8	#5	4'-1"	—	
v102(E)	27	#4	9'-11"	—	
v103(E)	8	#4	3'-7"	—	
v104(E)	22	#4	8'-10"	—	
v105(E)	12	#5	10'-11"	—	
z100(E)	71	#7	8'-9"	┌	
* CONCRETE STRUCTURES				CU YD	25.9
* REINFORCEMENT BARS, EPOXY COATED				POUND	2,710

BARS INDICATED THUS 20X3-#5 ETC. INDICATES 20 LINES OF BARS WITH 3 LENGTHS PER LINE.

\* COST OF CONCRETE STRUCTURES AND REINFORCEMENT BARS, EPOXY COATED IS INCLUDED WITH CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 72".

FILE NAME =	USER NAME = jrmichow	DESIGNED - WJC	REVISED -
G:\CHI\1\0130\Road\Sheets\0130-SHT-DRASTR	PLDT SCALE = N/A	DRAWN - WJC	REVISED -
	PLDT DATE = 5/12/2014	CHECKED - MDS	REVISED -
		DATE - 5/12/14	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

HEADWALL 200 DETAILS

SCALE: NTS SHEET NO. 40 OF 136 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	40
			CONTRACT NO. 63859	
ILLINOIS FED. AID PROJECT				

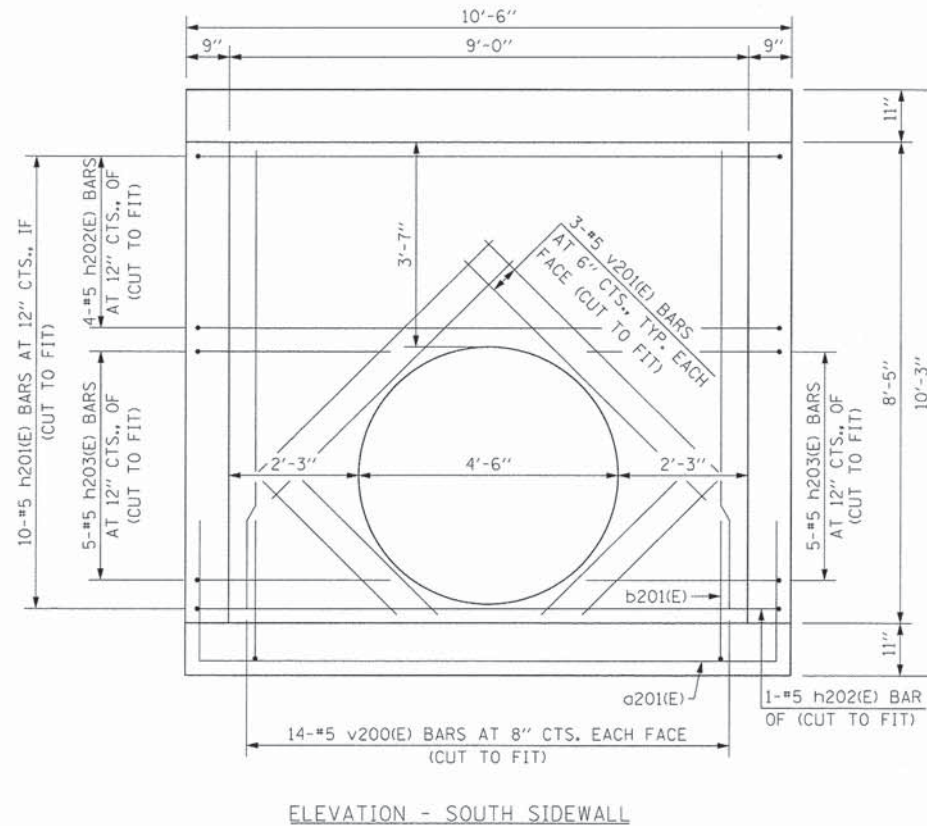
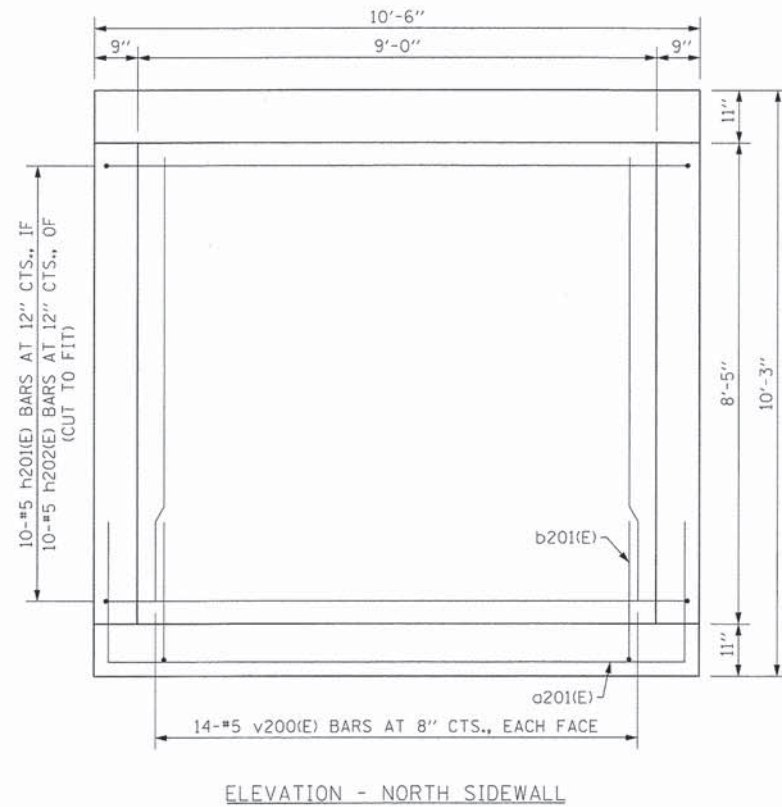
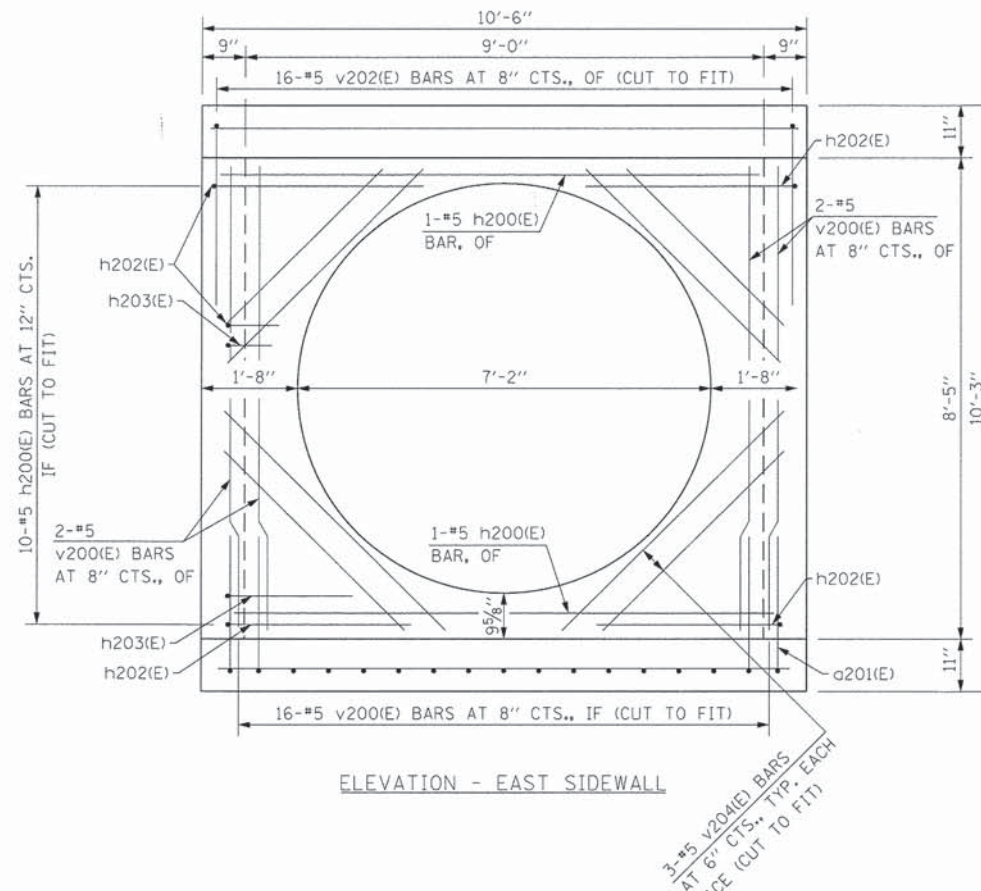
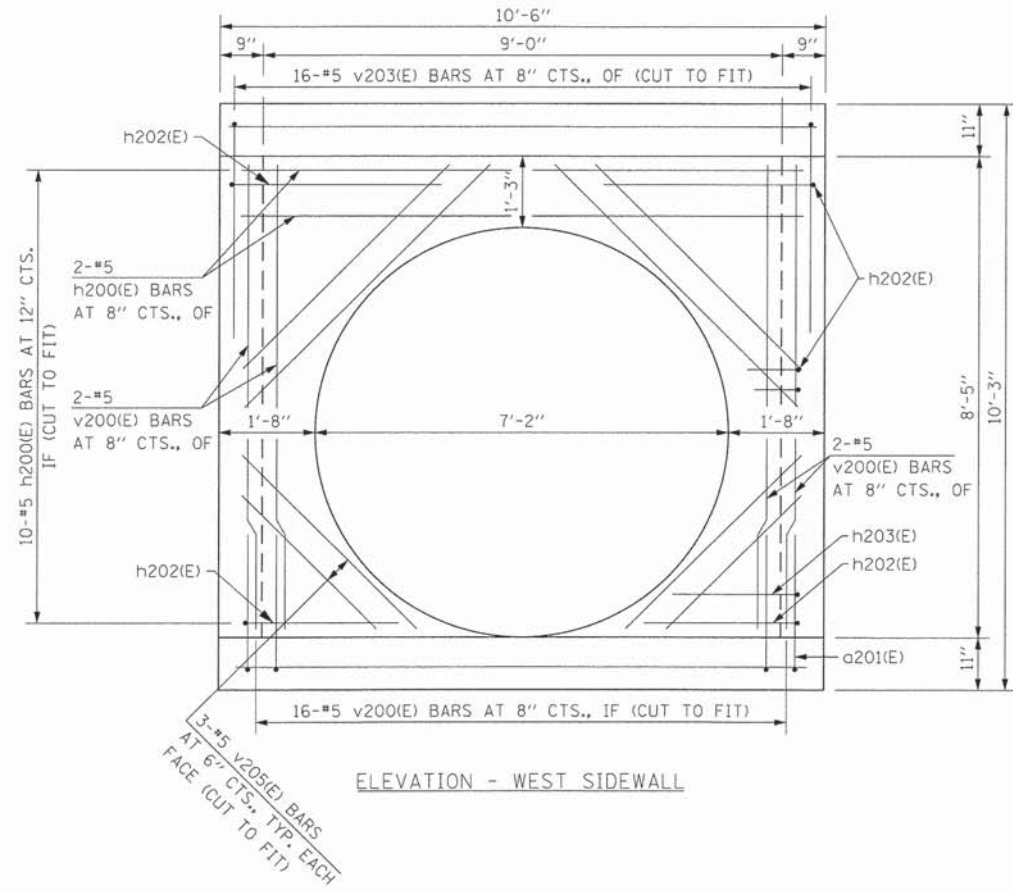




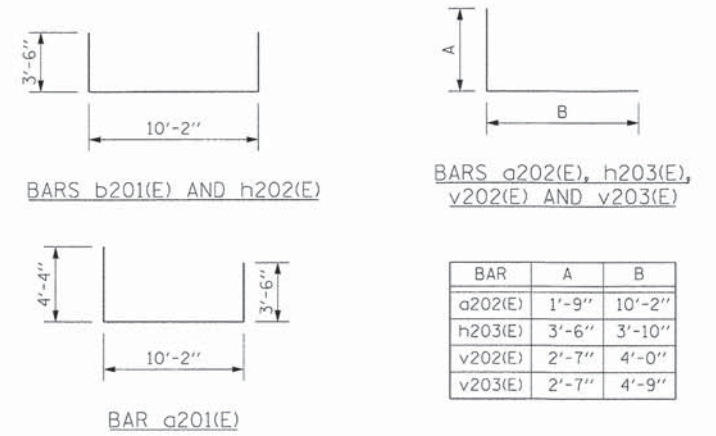


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NOTES:  
1. WORK THIS SHEET WITH SHEET 41 OF 136.



BILL OF MATERIAL  
(FOR INFORMATION ONLY)

BAR	NO.	SIZE	LENGTH	SHAPE
a200(E)	32	#5	10'-2"	
a201(E)	10	#5	18'-0"	
a202(E)	6	#5	11'-11"	
a203(E)	22	#6	10'-2"	
a204(E)	24	#5	6'-8"	
a205(E)	24	#5	4'-8"	
b200(E)	32	#5	10'-2"	
b201(E)	16	#5	17'-2"	
b202(E)	22	#6	10'-2"	
h200(E)	24	#5	10'-2"	
h201(E)	20	#5	8'-8"	
h202(E)	15	#5	17'-2"	
h203(E)	10	#5	7'-4"	
v200(E)	96	#5	8'-1"	
v201(E)	24	#5	7'-2"	
v202(E)	16	#5	6'-7"	
v203(E)	16	#5	7'-4"	
v204(E)	24	#5	5'-8"	
v205(E)	24	#5	6'-3"	
CONCRETE STRUCTURES		CU YD	13.4	
REINFORCEMENT BARS, EPOXY COATED		POUND	4,490	

\* COST OF CONCRETE STRUCTURES AND REINFORCEMENT BARS, EPOXY COATED IS INCLUDED WITH DRAINAGE STRUCTURES, NO. 1.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
DRAINAGE STRUCTURES, NO. 1	EACH	1

FILE NAME =	USER NAME = jrmckaw	DESIGNED - WJC	REVISED -
G:\CH11\0130\Road\Sheets\0130-SHT-DRSTR-3.dgn		DRAWN - MTS	REVISED -
		CHECKED - MDS	REVISED -
		DATE - 5/12/14	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRAINAGE STRUCTURES, NO. 1  
DETAILS 2

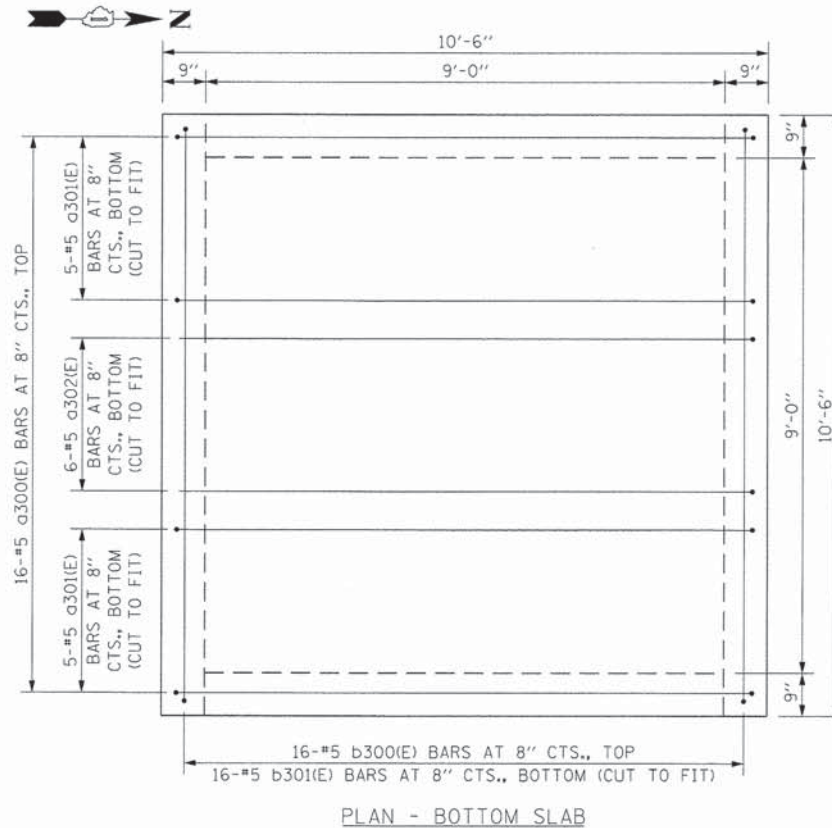
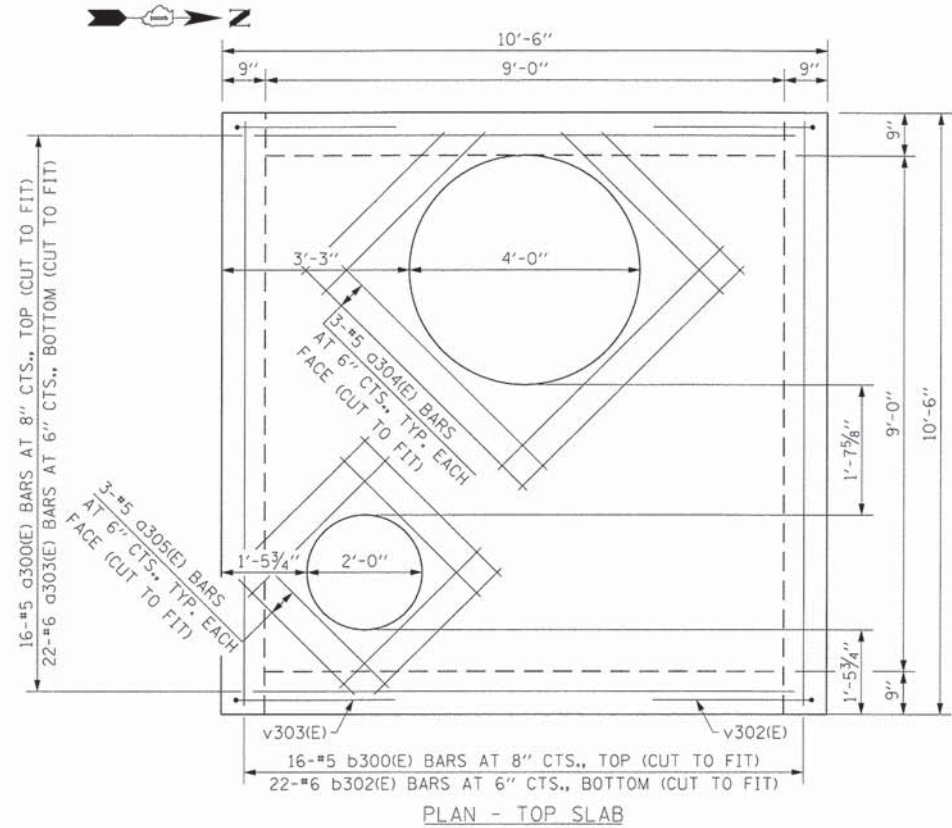
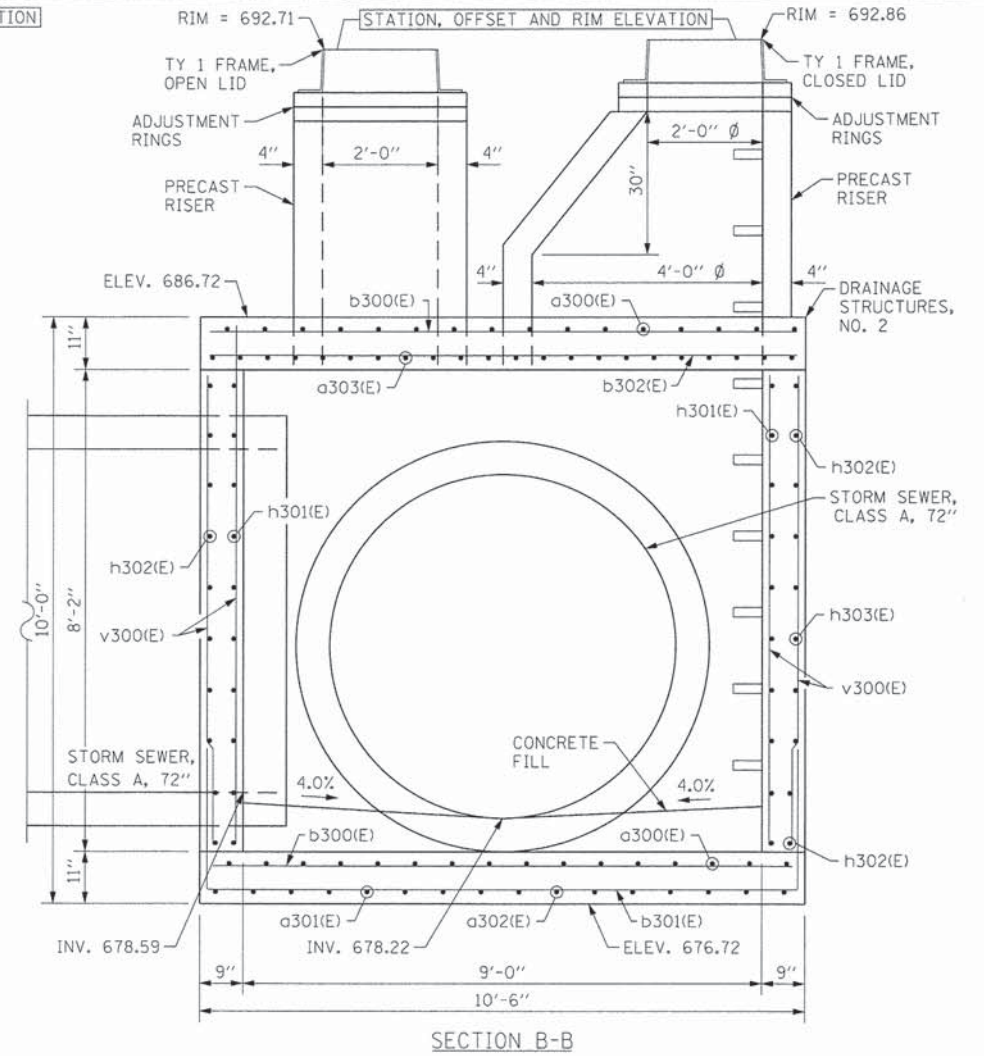
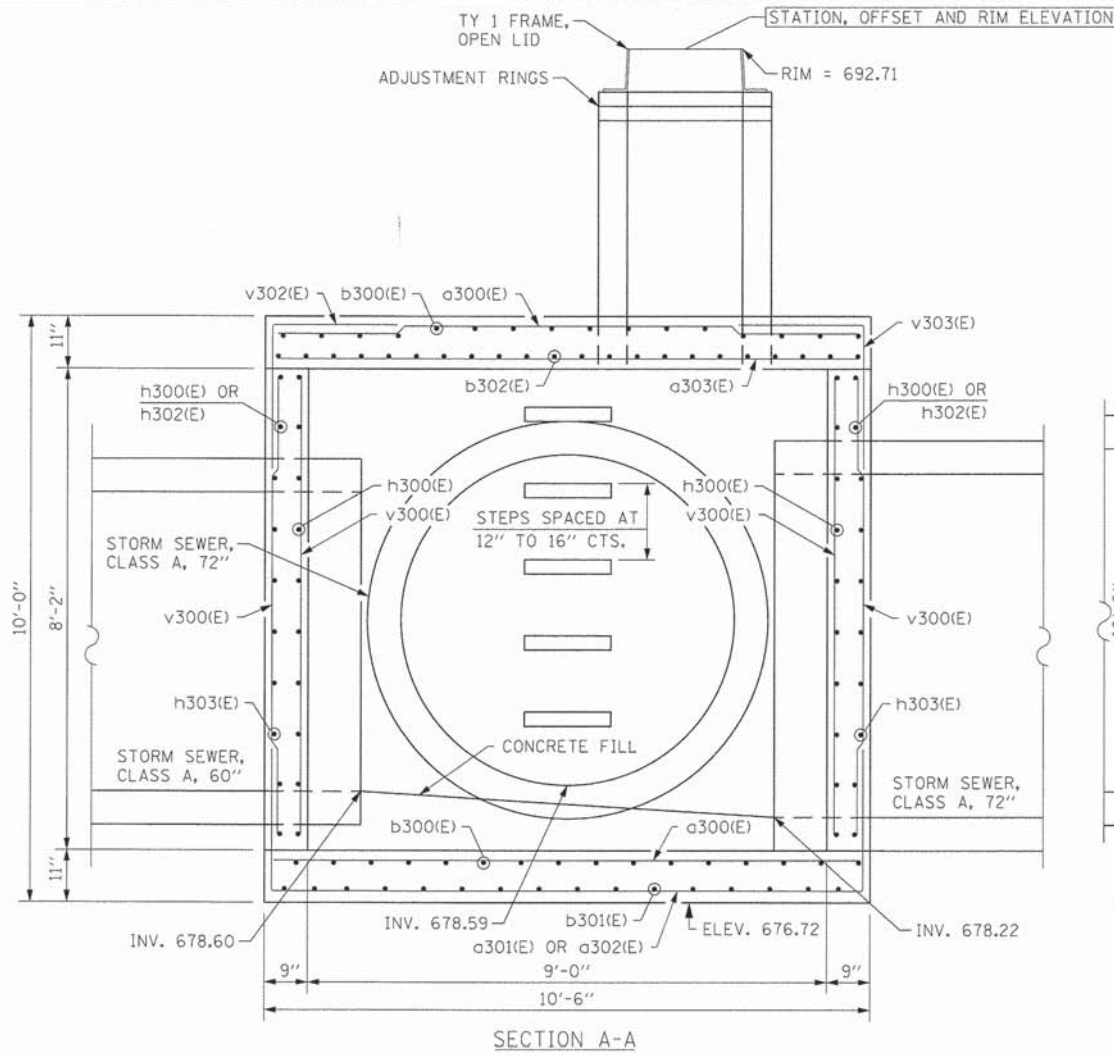
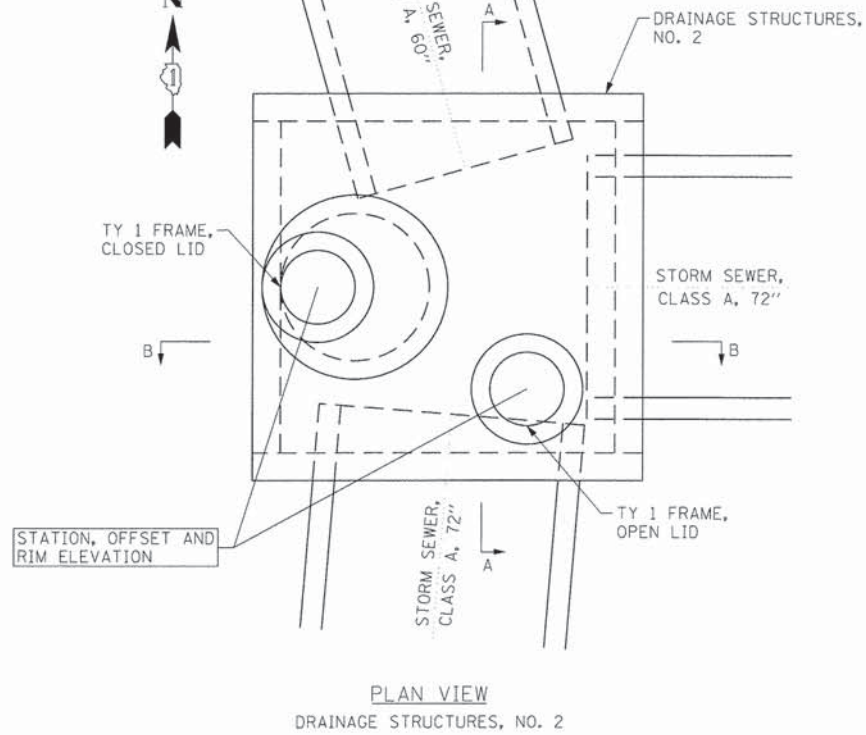
SCALE: SHEET NO. 42 OF 136 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	42
CONTRACT NO. 63859			ILLINOIS FED. AID PROJECT	



DATE	
BY	
REVISIONS	
NO.	
DESCRIPTION	
DATE	
BY	
REVISIONS	
NO.	
DESCRIPTION	
DATE	
BY	
REVISIONS	
NO.	
DESCRIPTION	

DATE	
BY	
REVISIONS	
NO.	
DESCRIPTION	
DATE	
BY	
REVISIONS	
NO.	
DESCRIPTION	
DATE	
BY	
REVISIONS	
NO.	
DESCRIPTION	



**NOTES:**

1. A PRECAST REINFORCED CONCRETE ALTERNATE SHALL BE ALLOWED FOR THIS STRUCTURE. SHOP PLANS AND CALCULATIONS SHALL BE SUBMITTED BY THE CONTRACTOR TO THE ENGINEER ACCORDING TO THE APPLICABLE REQUIREMENTS OF ARTICLE 504 OF THE STANDARD SPECIFICATIONS.
2. SEE HIGHWAY STANDARDS 602701-02 AND 604001-03 FOR ADDITIONAL INFORMATION AND REQUIREMENTS.
3. THE CONTRACT UNIT PRICE FOR EACH SHALL INCLUDE ALL EXCAVATION, BACKFILL, CONCRETE, REINFORCEMENT, PRECAST RISERS, FRAME, LID AND OTHER INCIDENTALS AS SPECIFIED ON THIS DETAIL AND AS DIRECTED BY THE ENGINEER.
4. ALL INVERTS AND ELEVATIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO ORDERING MATERIALS AND CONSTRUCTION. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED WITHIN THE CONTRACT UNIT PRICE FOR DRAINAGE STRUCTURES, NO. 2.
5. STATION, OFFSET AND RIM ELEVATION PROVIDED ARE AT THE CENTER OF THE PROPOSED FRAME AND LID.
6. WORK THIS SHEET WITH SHEET 44 OF 136.

FILE NAME =	USER NAME = jrmckow	DESIGNED - WJC	REVISED -
G:\CH11\0130\Road\Sheets\DI0130-SHT-DRSTR-4.dgn		DRAWN - MTS	REVISED -
		CHECKED - MDS	REVISED -
		DATE - 5/12/14	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DRAINAGE STRUCTURES, NO. 2  
DETAILS 1**

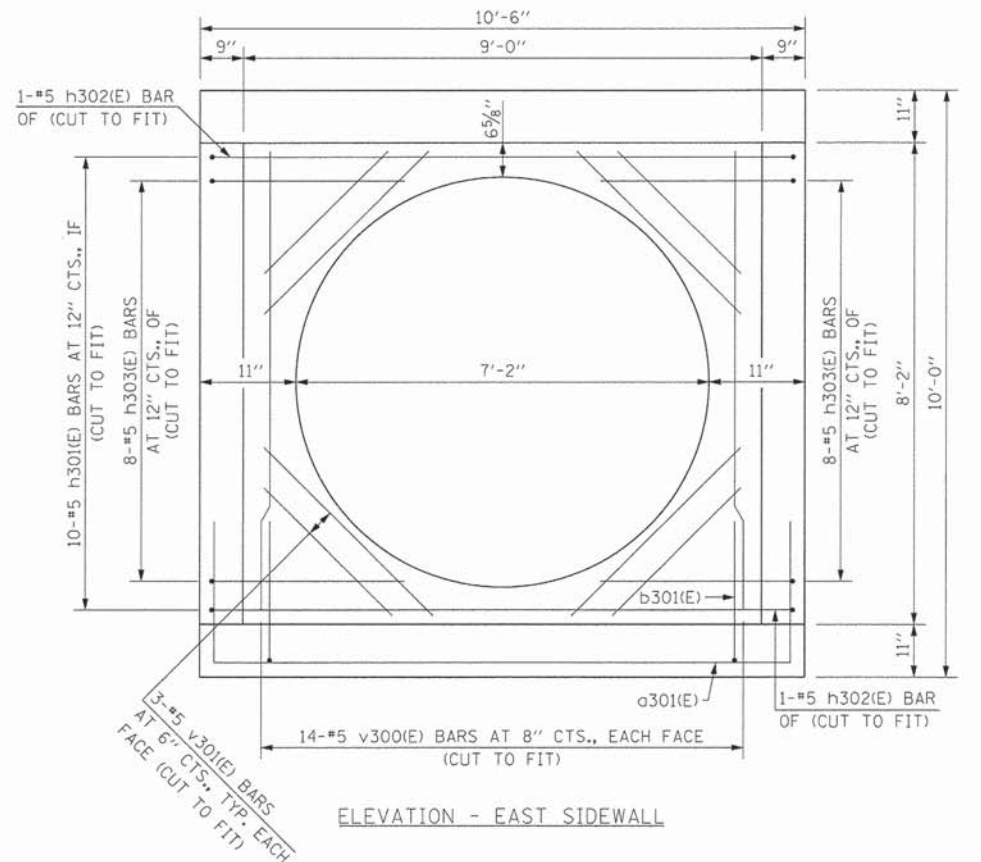
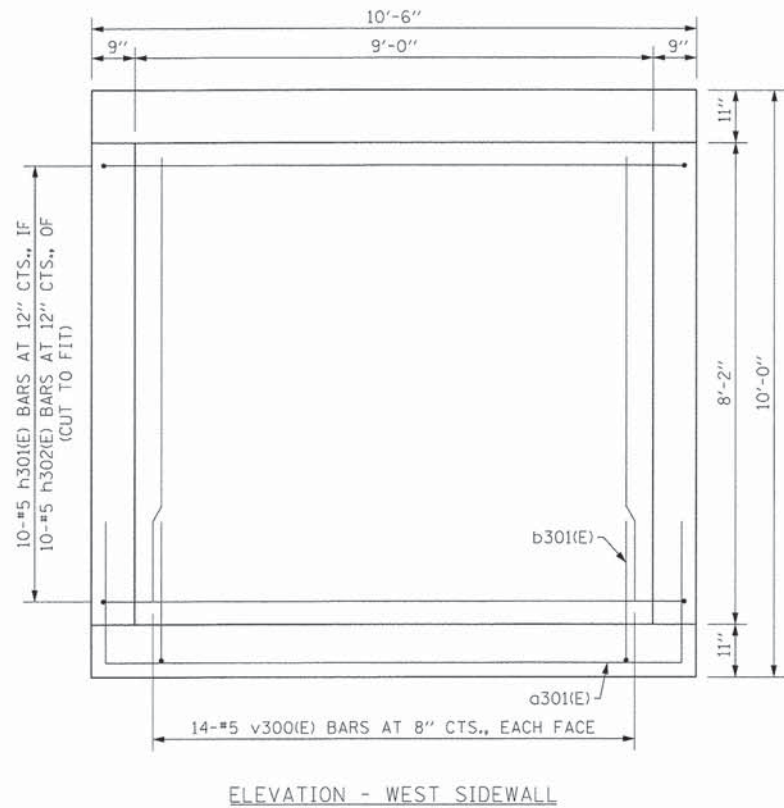
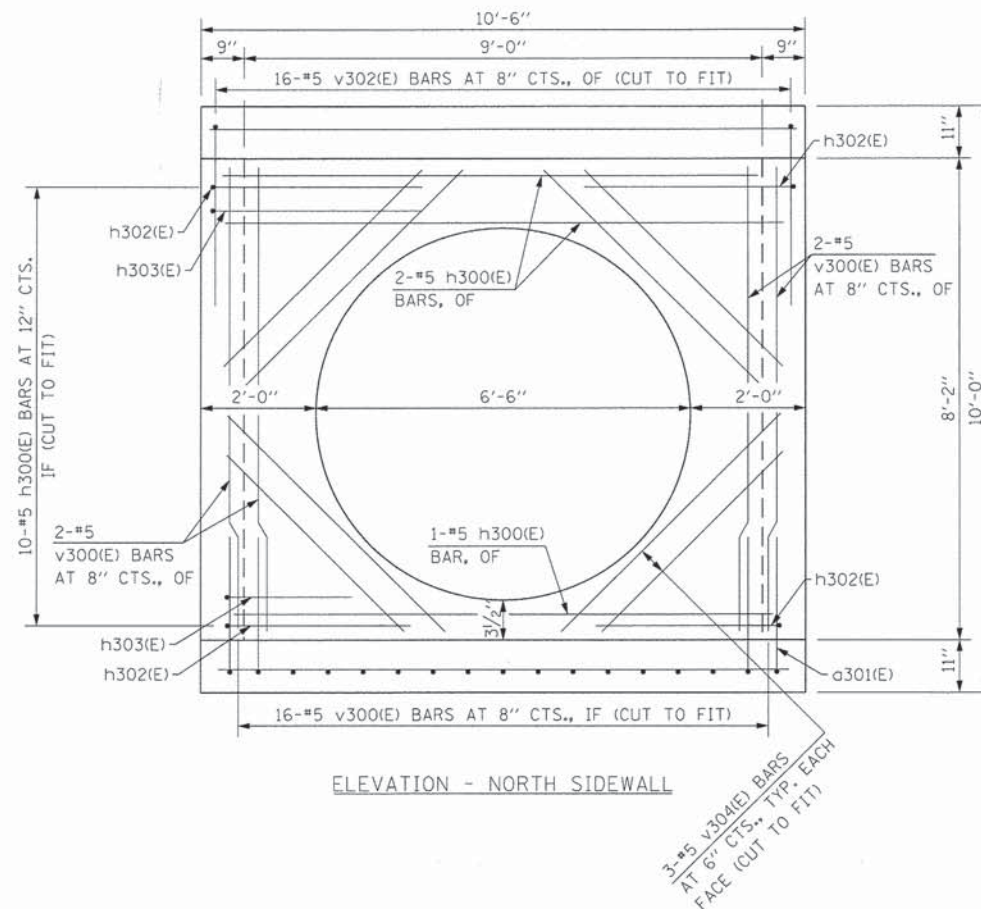
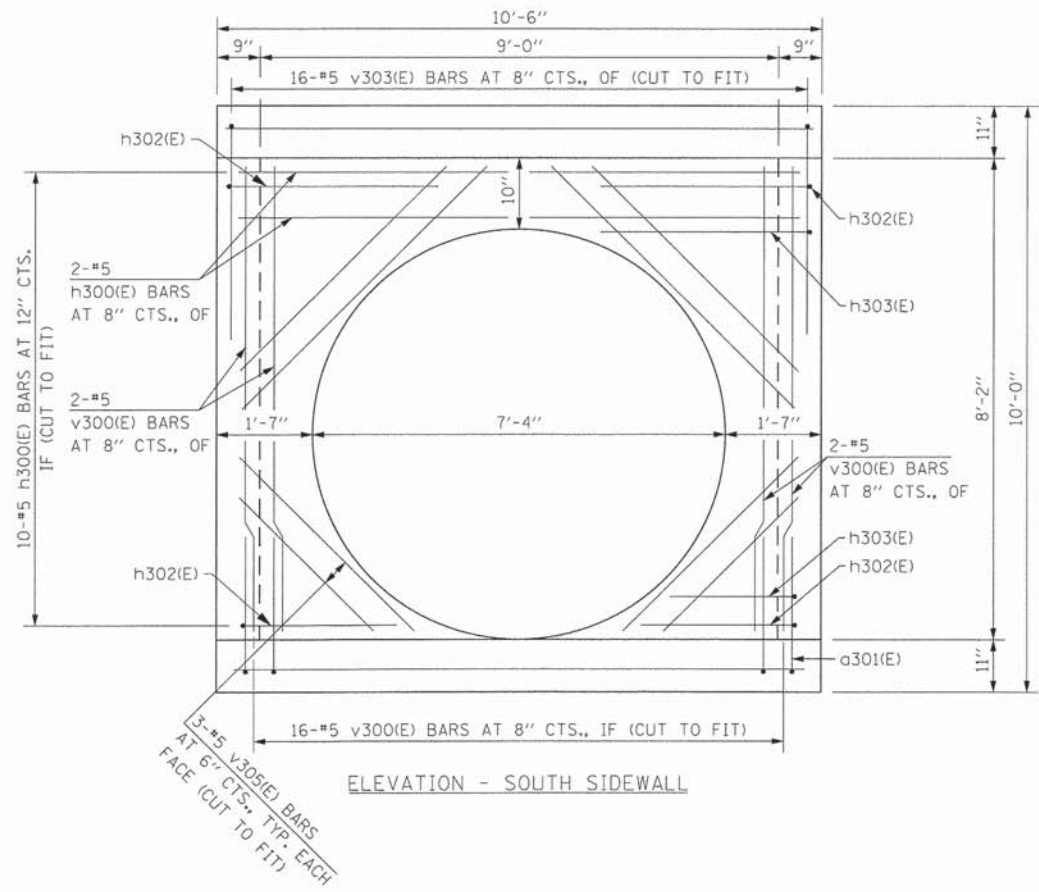
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	43
CONTRACT NO. 63859			ILLINOIS FED. AID PROJECT	

SCALE: SHEET NO. 43 OF 136 SHEETS STA. TO STA.

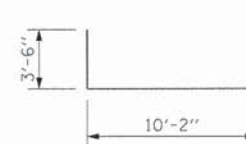


DATE	
BY	
DESIGNED	
CHECKED	
PLotted	
NOTE BOOK	
NO.	

DATE	
BY	
DESIGNED	
CHECKED	
PLotted	
NOTE BOOK	
NO.	

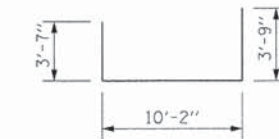


NOTES:  
1. WORK THIS SHEET WITH SHEET 43 OF 136.



BARS b301(E) AND h302(E)

BARS a302(E), h303(E), v302(E) AND v303(E)



BAR a301(E)

BAR	A	B
a302(E)	1'-4"	10'-2"
h303(E)	3'-5"	3'-0"
v302(E)	2'-7"	2'-11"
v303(E)	2'-7"	3'-5"

BILL OF MATERIAL  
(FOR INFORMATION ONLY)

BAR	NO.	SIZE	LENGTH	SHAPE
a300(E)	32	#5	10'-2"	—
a301(E)	10	#5	16'-6"	—
a302(E)	6	#5	11'-6"	—
a303(E)	22	#6	10'-2"	—
a304(E)	24	#5	6'-8"	—
a305(E)	24	#5	4'-8"	—
b300(E)	32	#5	10'-2"	—
b301(E)	16	#5	17'-2"	—
b302(E)	22	#6	10'-2"	—
h300(E)	25	#5	10'-2"	—
h301(E)	20	#5	8'-8"	—
h302(E)	12	#5	17'-2"	—
h303(E)	16	#5	6'-5"	—
v300(E)	96	#5	7'-10"	—
v301(E)	24	#5	4'-3"	—
v302(E)	16	#5	5'-6"	—
v303(E)	16	#5	6'-0"	—
v304(E)	24	#5	6'-8"	—
v305(E)	24	#5	5'-9"	—

- \* CONCRETE STRUCTURES CU YD 13.4
- \* REINFORCEMENT BARS, EPOXY COATED POUND 4,330

\* COST OF CONCRETE STRUCTURES AND REINFORCEMENT BARS, EPOXY COATED IS INCLUDED WITH DRAINAGE STRUCTURES, NO. 2.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
DRAINAGE STRUCTURES, NO. 2	EACH	1

FILE NAME =	USER NAME = jrmickow	DESIGNED = WJC	REVISED =
G:\CH11\0130\Road\Sheets\010130-SHT-DRSTR-2.dgn		DRAWN = MTS	REVISED =
		CHECKED = MDS	REVISED =
		DATE = 5/12/2014	REVISED =

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRAINAGE STRUCTURES, NO. 2  
DETAILS 2

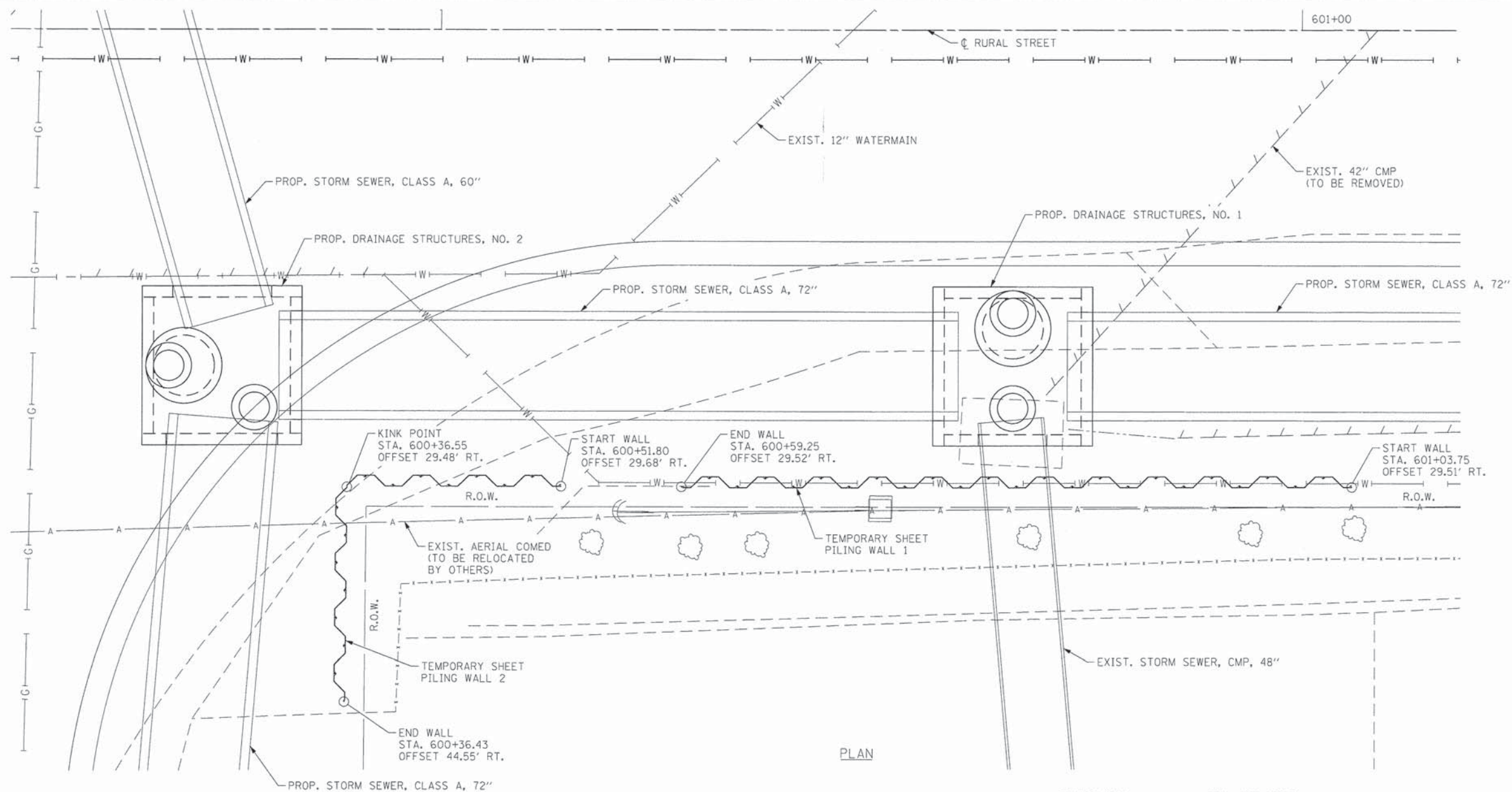
SCALE: SHEET NO. 44 OF 136 SHEETS STA. TO STA.

F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 44
				CONTRACT NO. 63859
ILLINOIS FED. AID PROJECT				



PLAN	SURVEYED	DATE
	ALIGNED	
	NOTED	
	CHECKED	
	BY	
	NO.	

PROFILE	GRADES	DATE
	NOTED	
	CHECKED	
	BY	
	NO.	



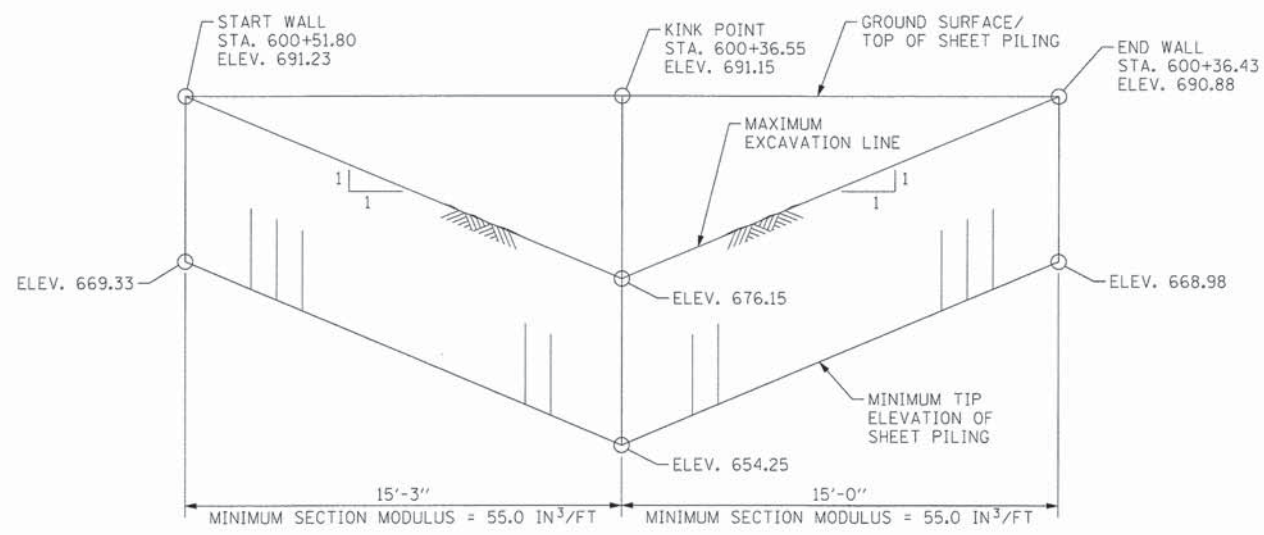
PLAN



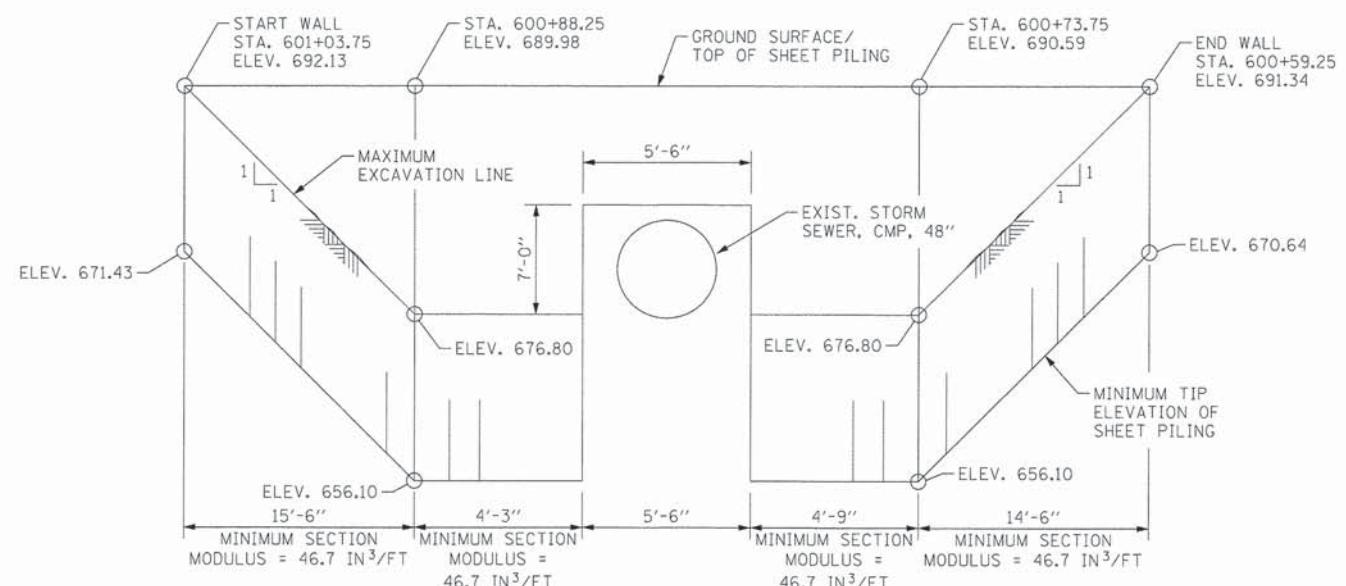
- NOTES:**
- STATIONS AND OFFSETS ARE RELATIVE TO THE CENTERLINE OF RURAL STREET, ALONG THE FRONT FACE OF WALL.
  - IF THE CONTRACTOR CHOOSES TO ALTER THE TEMPORARY CANTILEVERED SHEET PILING DESIGN REQUIREMENTS SHOWN ON THE PLANS, A DESIGN SUBMITTAL INCLUDING PLAN DETAILS AND CALCULATIONS WILL BE REQUIRED FOR REVIEW AND ACCEPTANCE BY THE ENGINEER.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
TEMPORARY SHEET PILING	SQ. FT.	2055



ELEVATION - TEMPORARY SHEET PILING, WALL 2  
(LOOKING SOUTH AND EAST)



ELEVATION - TEMPORARY SHEET PILING, WALL 1  
(LOOKING SOUTH)

FILE NAME *	USER NAME = jmicow	DESIGNED - WJC	REVISED -
G:\CH11\0130\Road\Sheets\DI0130-SHT-DRSTR-5.dgn		DRAWN - MTS	REVISED -
		CHECKED - MDS	REVISED -
		DATE - 5/12/14	REVISED -

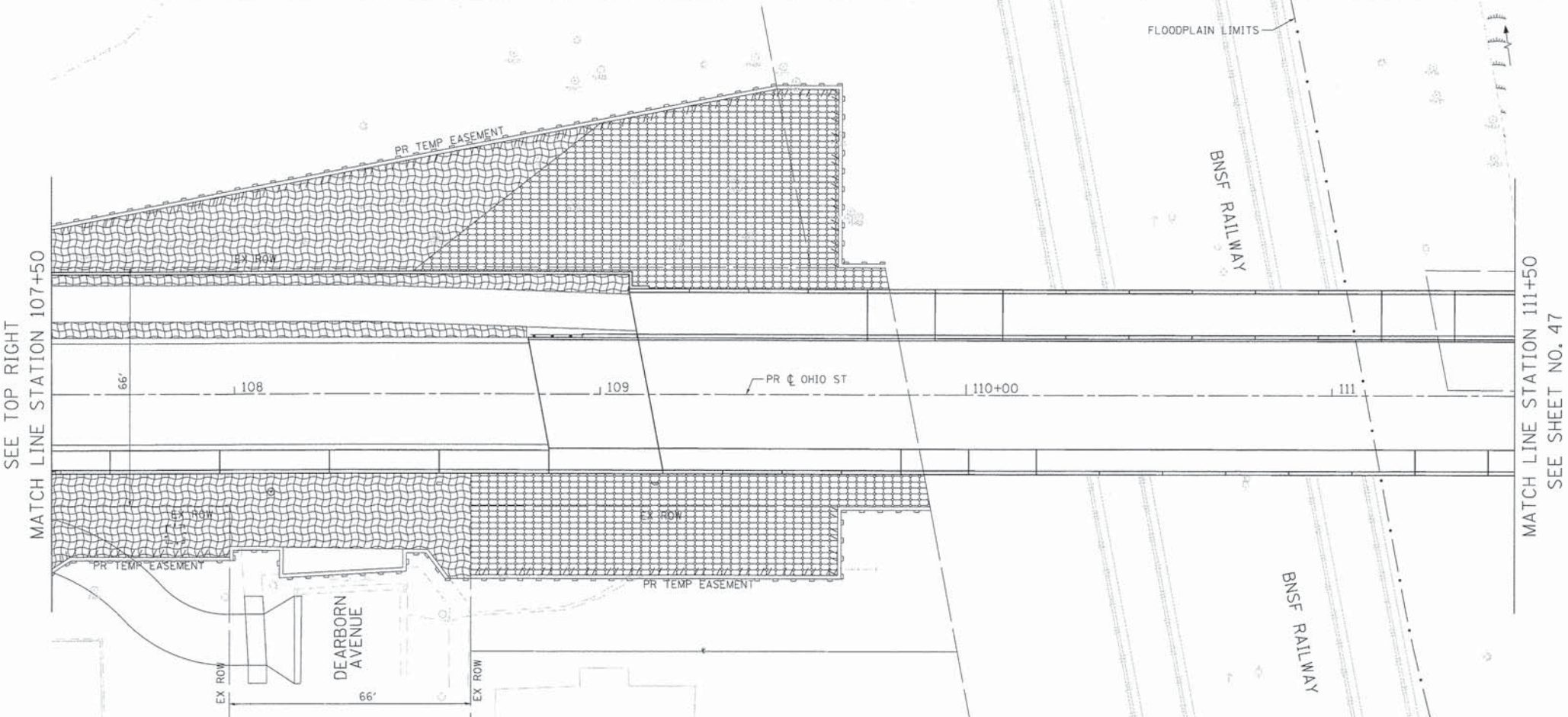
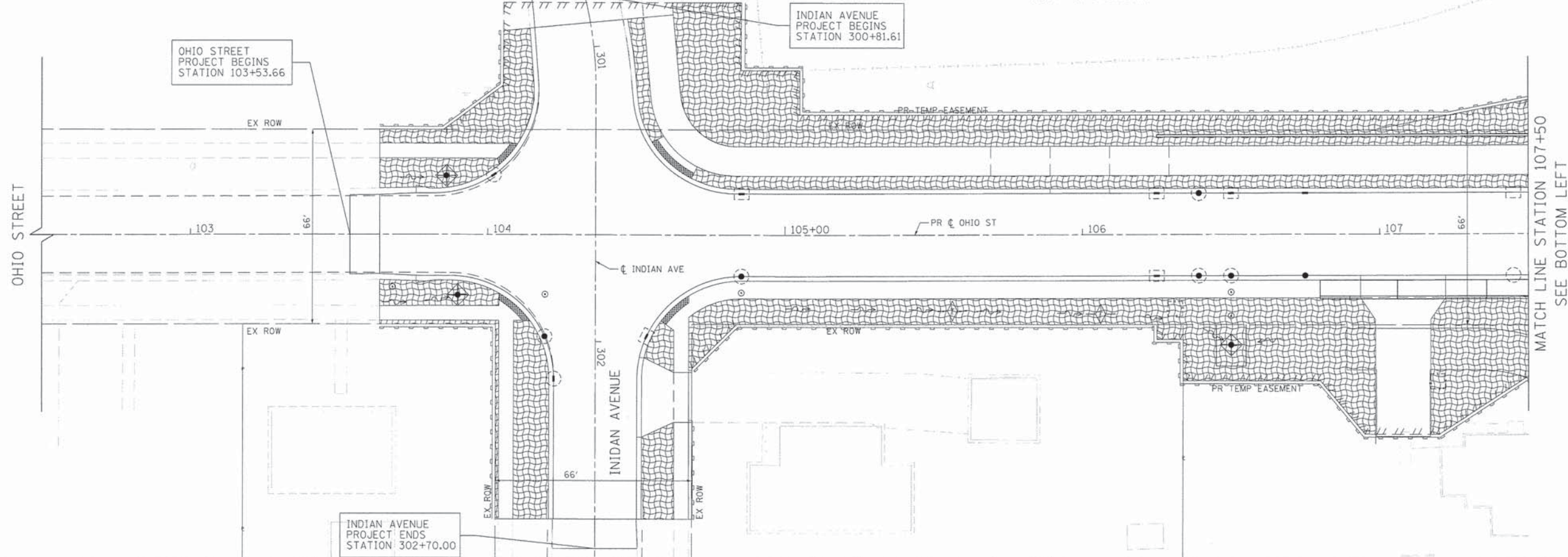
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DRAINAGE STRUCTURES, NO. 1 AND NO. 2  
TEMPORARY SHEET PILING**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	45
CONTRACT NO. 63859			[ILLINOIS] FED. AID PROJECT	

SCALE: SHEET NO. 45 OF 136 SHEETS STA. TO STA.





LEGEND:

- INLET AND PIPE PROTECTION
- TEMPORARY DITCH CHECK
- INLET FILTER
- PERIMETER EROSION BARRIER
- TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET
- TEMPORARY EROSION CONTROL SEEDING AND HEAVY DUTY EROSION CONTROL BLANKET
- STONE RIPRAP
- DITCH FLOW
- TEMPORARY FENCE

NOTES:

1. FOR EROSION CONTROL NOTES, SEE SHEET NO. 49
2. PERIMETER EROSION BARRIER HAS BEEN OFFSET FOR CLARITY.
3. HEAVY DUTY EROSION CONTROL BLANKET IS TO BE USED ON ALL SIDESLOPES GREATER THAN 1:3 AND LESS THAN 1:2.



FILE NAME =	USER NAME = apandexter	DESIGNED = YSK	REVISED =
Gr:\CHI1\0130\Road\Sheets\010130-SHT-EC-1.dgn		DRAWN = YSK	REVISED =
	PLOT SCALE = N/A	CHECKED = MWS	REVISED =
#MODELNAME#	PLOT DATE = 5/12/2014	DATE = 5/12/14	REVISED =

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EROSION AND SEDIMENT CONTROL PLAN

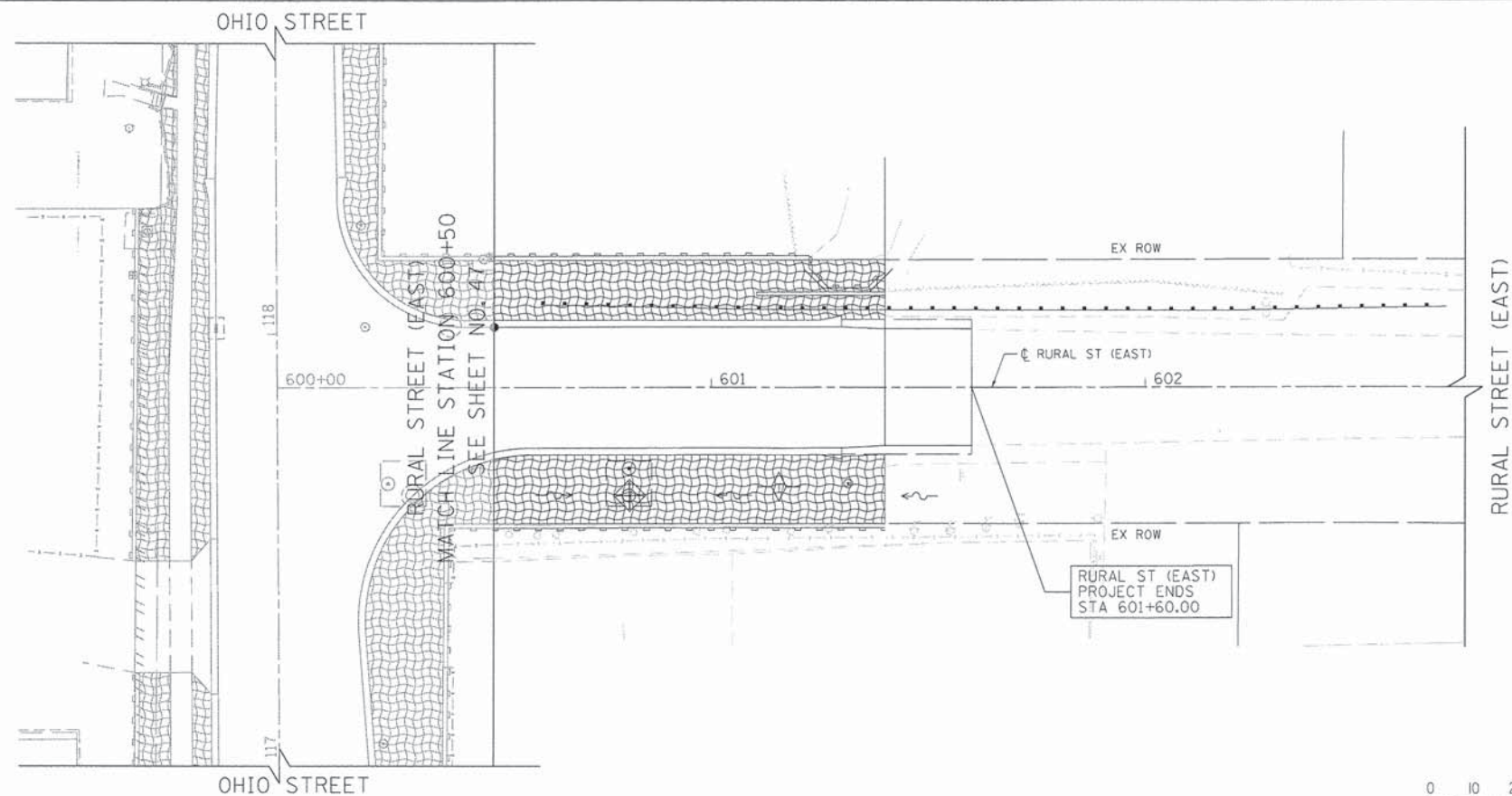
SCALE: 1"=20' SHEET NO. 46 OF 136 SHEETS STA. 103+53.66 TO STA. 111+50

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	46
				CONTRACT NO. 63859
ILLINOIS FED. AID PROJECT				

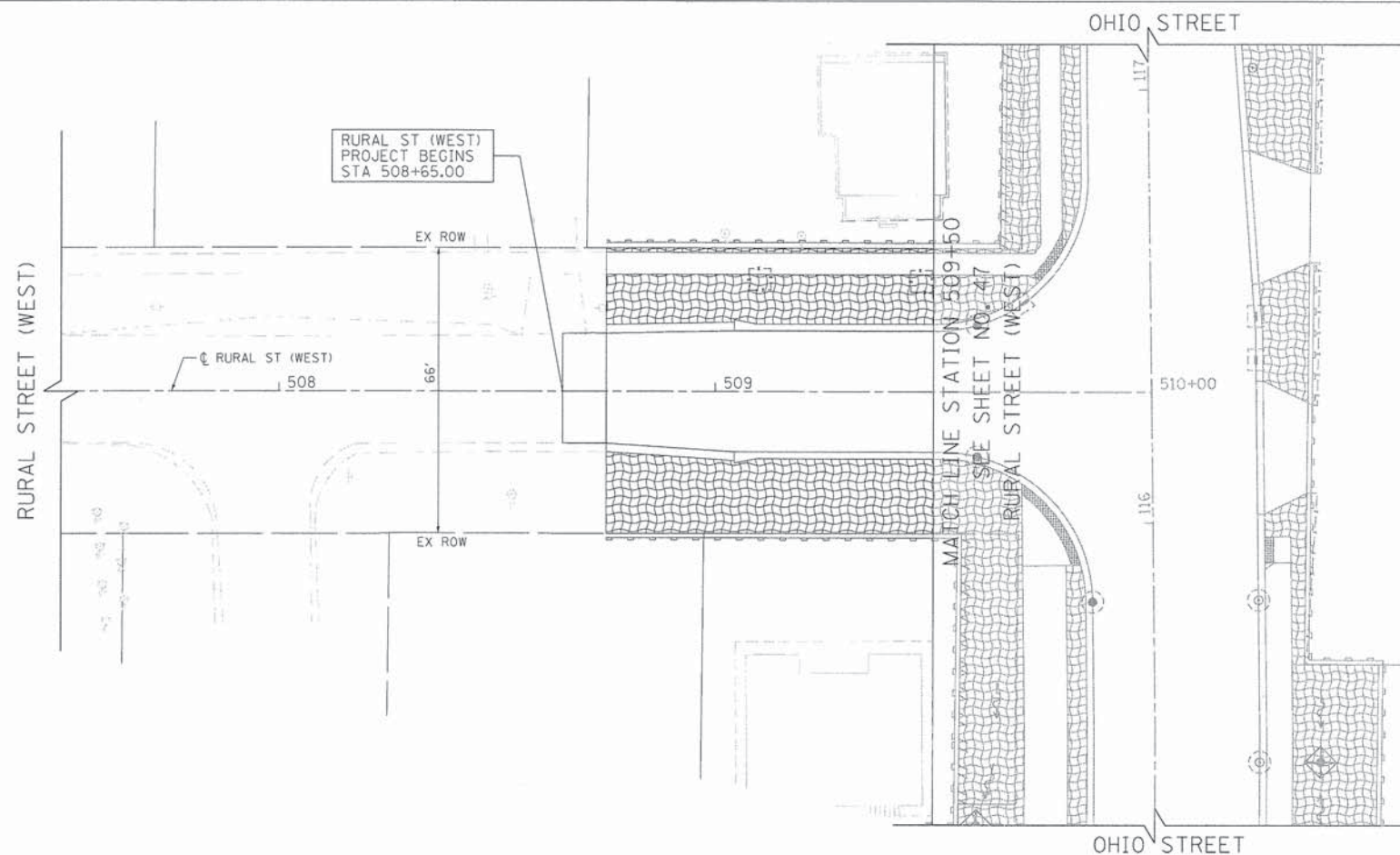








HORIZONTAL SCALE: 1"=20'

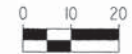


**LEGEND:**

- INLET AND PIPE PROTECTION
- TEMPORARY DITCH CHECK
- INLET FILTER
- PERIMETER EROSION BARRIER
- TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET
- TEMPORARY EROSION CONTROL SEEDING AND HEAVY DUTY EROSION CONTROL BLANKET
- STONE RIPRAP
- DITCH FLOW
- TEMPORARY FENCE

**NOTES:**

1. FOR EROSION CONTROL NOTES, SEE SHEET NO. 49
2. PERIMETER EROSION BARRIER HAS BEEN OFFSET FOR CLARITY.
3. HEAVY DUTY EROSION CONTROL BLANKET IS TO BE USED ON ALL SIDESLOPES GREATER THAN 1:3 AND LESS THAN 1:2.



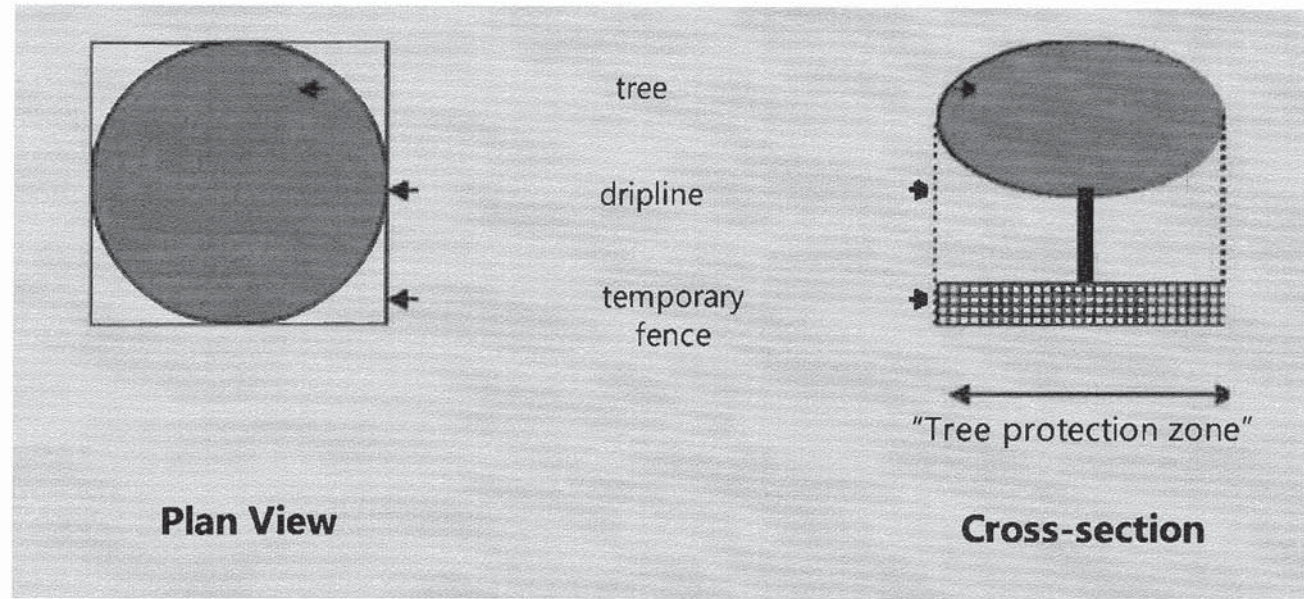
HORIZONTAL SCALE: 1"=20'

FILE NAME =	USER NAME = a_jpondexter	DESIGNED - YSK	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EROSION AND SEDIMENT CONTROL PLAN</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
G:\CH11\0130\Road\Sheets\010130-SHT-EC-3.dgn		DRAWN - YSK	REVISED -		SCALE: 1"=20'	SHEET NO. 48 OF 136 SHEETS	STA.	TO STA.	4087	08-00278-00-BR	KANE	136 48
#MODELNAME#		CHECKED - MWS	REVISED -		CONTRACT NO. 63859							
		DATE - 5/12/14	REVISED -		ILLINOIS FED. AID PROJECT							



PLAN	DESIGNED	BY	DATE
NO.	NOTED		
	GRADES CHECKED		
	ALIGNMENT CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	FILE NAME		

PROFILE	DESIGNED	BY	DATE
NO.	NOTED		
	GRADES CHECKED		
	ALIGNMENT CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	FILE NAME		



FILE NAME :	USER NAME = e_pondexter	DESIGNED -	REVISED -
G:\CH11\0130\Road\Sheets\DI0130-SHT-EC-4.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE - 5/12/14	REVISED -

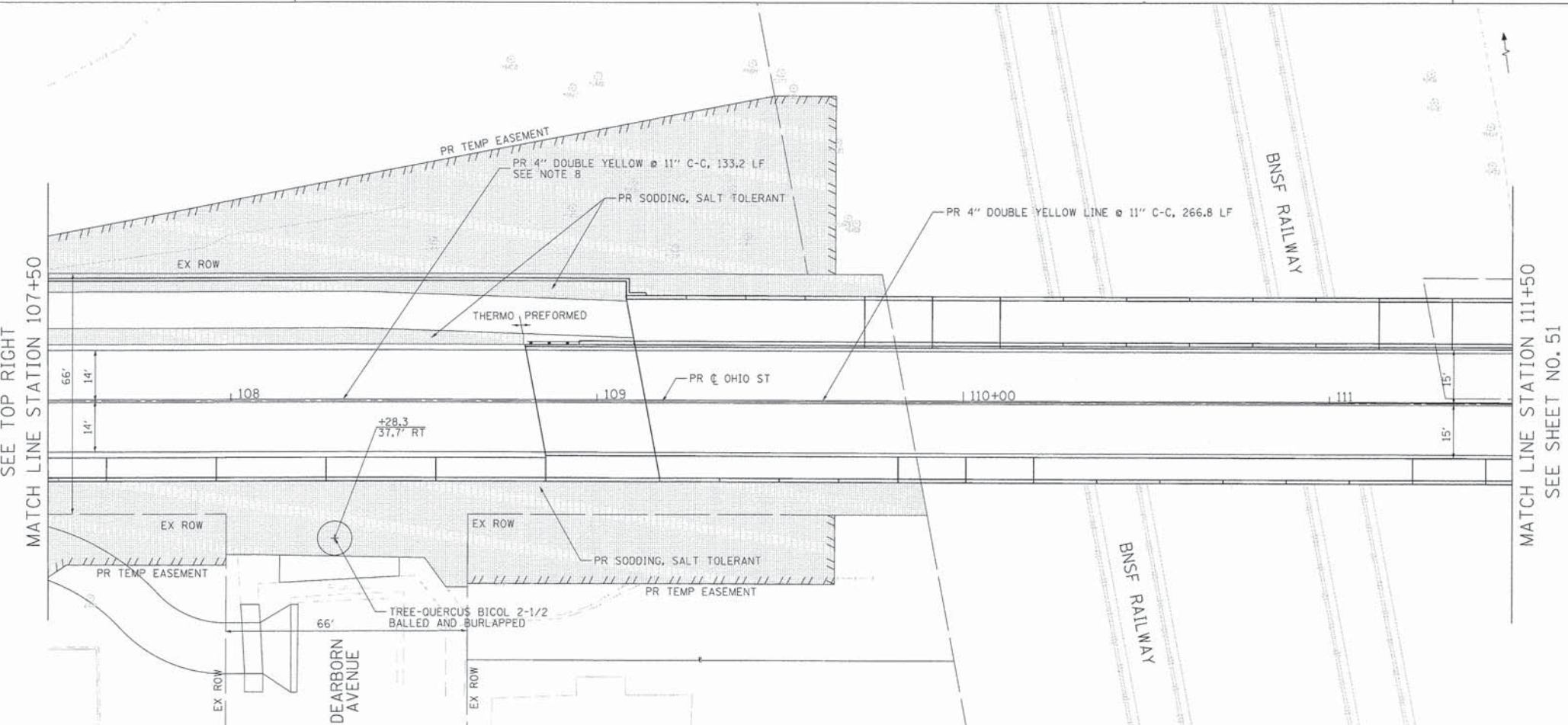
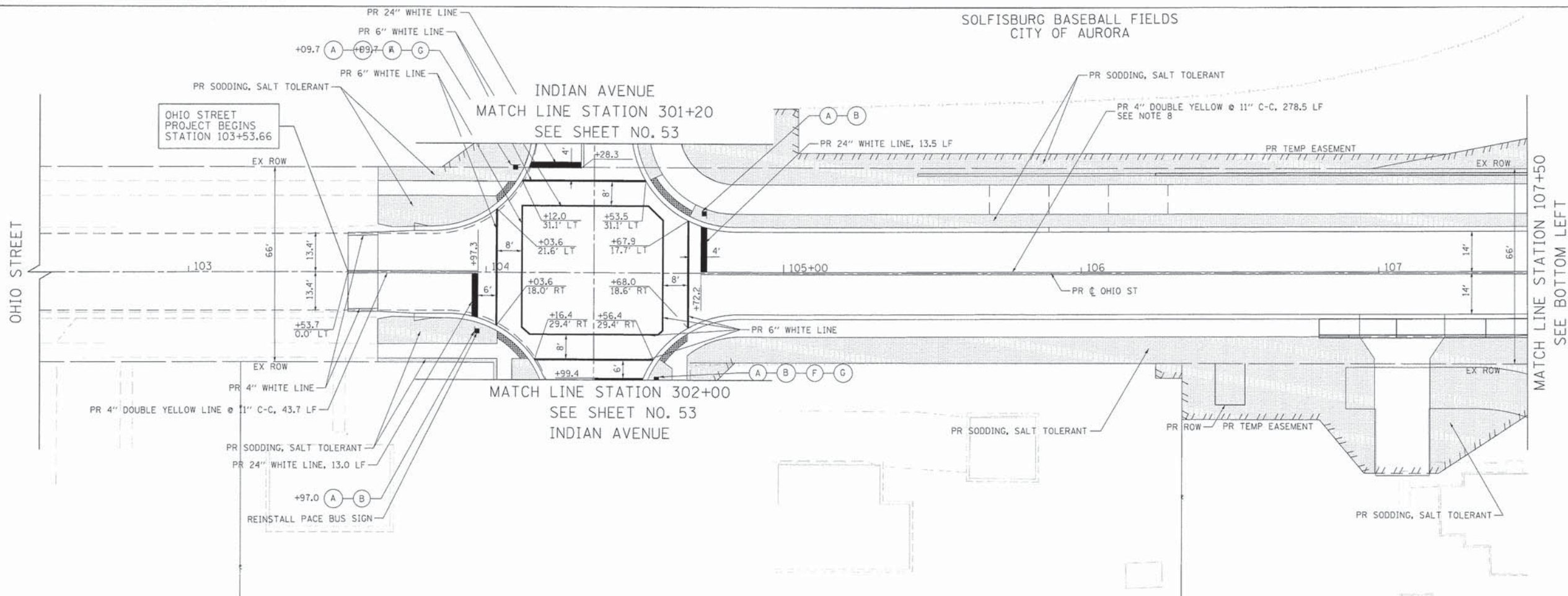
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EROSION AND SEDIMENT CONTROL DETAILS

SCALE: SHEET NO. 49 OF 136 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
40BT	08-00278-00-BR	KANE	136	49
			CONTRACT NO. 63859	
ILLINOIS FED. AID PROJECT				





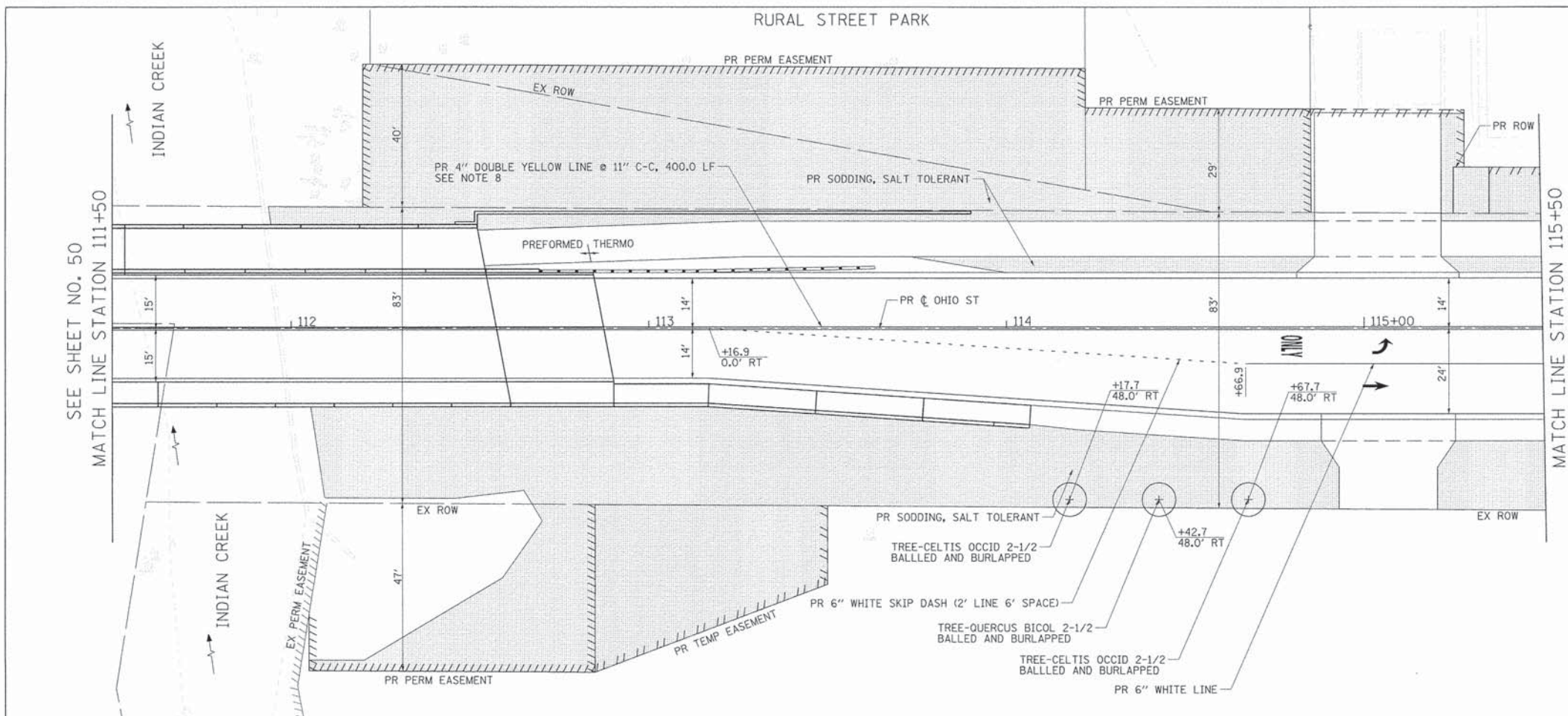
- NOTES:**
1. THERMOPLASTIC PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON HMA SURFACES UNLESS NOTED OTHERWISE.
  2. FOR ADDITIONAL DETAILS SEE DISTRICT ONE TYPICAL PAVEMENT MARKING TC-13.
  3. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 72 HOURS PRIOR TO THE PLACEMENT OF THE PAVEMENT MARKING.
  4. EXISTING PAVEMENT MARKINGS THAT ARE REMOVED OUTSIDE THE IMPROVEMENT LIMITS FOR MAINTENANCE OF TRAFFIC STAGING OR CONSTRUCTION SHALL BE REAPPLIED OR REINSTALLED IN THE SAME LOCATION.
  5. STATION OR OFFSET FOR STOP BAR LOCATIONS IS GIVEN AT THE FRONT OF THE STOP BAR.
  6. A MINIMUM DISTANCE OF 4 FEET SHALL BE MAINTAINED BETWEEN THE EDGE OF THE CROSSWALK AND THE EDGE OF THE STOP BAR CLOSEST TO IT EXCEPT WHERE OTHERWISE NOTED IN THE PLANS.
  7. UNLESS OTHERWISE INDICATED, STREET NAME SIGNS SHALL MEET THE COLOR AND LETTERING SIZE ACCORDANCE WITH THE MANUAL ON TRAFFIC CONTROL DEVICES, 2009 EDITION.
  8. PREFORMED THERMOPLASTIC PAVEMENT MARKING SHALL BE USED ON THE BRIDGE ALL CONCRETE STRUCTURES.
  9. ALL EXISTING SIGNS ARE TO BE RETURNED TO THE CITY OF AURORA.

- LEGEND:**
- SODDING SALT TOLERANT
  - PROPOSED TREE
  - (A) R1-1 36"X36"
  - (B) R1-4 18"X6"
  - (C) R2-1 30"X36"
  - (D) R8-3 18"X24"
  - (E) W1-7 36"X18"
  - (F) D3-1 36"X12"
  - (G) D3-1 36"X12"
  - (H) D3-1 36"X12"



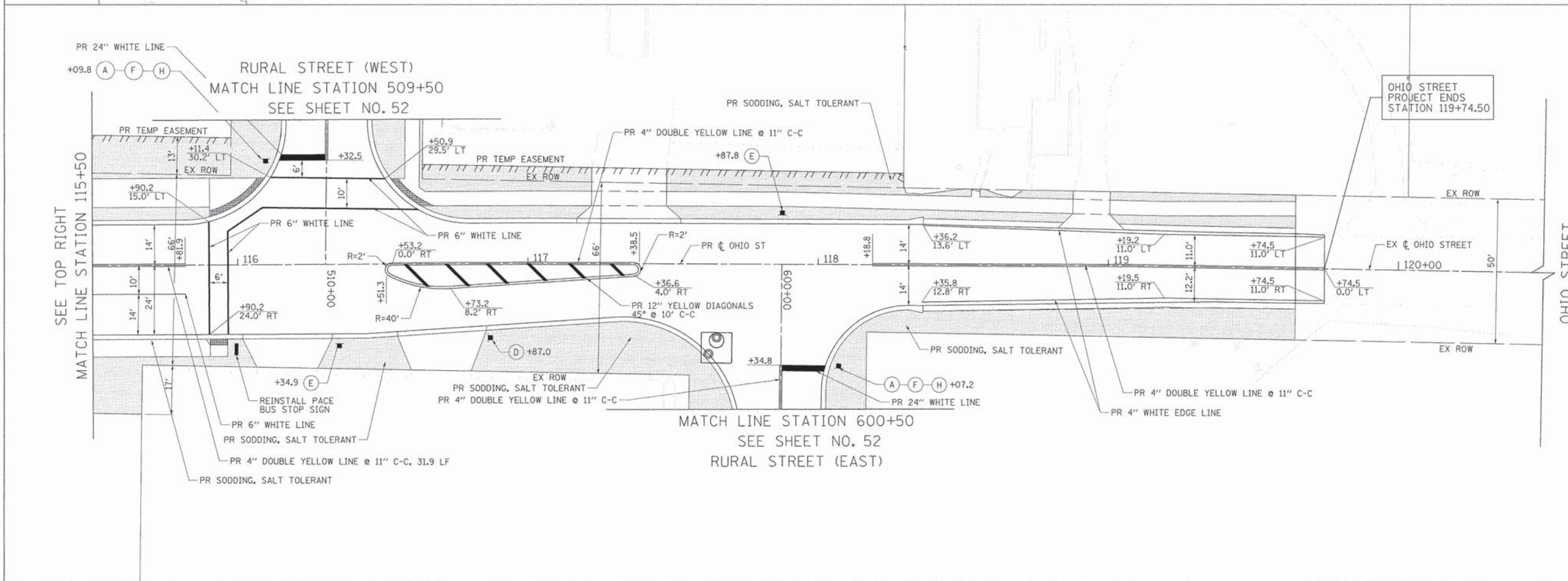
FILE NAME = G:\CH11\0130\Road\Sheets\010130-SHT-LS-1.dgn	USER NAME = a_jpondexter	DESIGNED - MLB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>LANDSCAPING, SIGNING, AND PAVEMENT MARKING PLAN</b>	F.A.P. R.T.E. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 50		
#MODELNAME#	PLOT SCALE = N/A	CHECKED - MWS	REVISED -			SCALE: 1"=20'	SHEET NO. 50 OF 136 SHEETS	STA. 103+53.66	TO STA. 111+50	CONTRACT NO. 63859		
	PLOT DATE = 5/12/2014	DATE = 5/12/14	REVISED -			ILLINOIS FED. AID PROJECT						





- NOTES:
1. THERMOPLASTIC PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON HMA SURFACES UNLESS NOTED OTHERWISE.
  2. FOR ADDITIONAL DETAILS SEE DISTRICT ONE TYPICAL PAVEMENT MARKING TC-13.
  3. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 72 HOURS PRIOR TO THE PLACEMENT OF THE PAVEMENT MARKING.
  4. EXISTING PAVEMENT MARKINGS THAT ARE REMOVED OUTSIDE THE IMPROVEMENT LIMITS FOR MAINTENANCE OF TRAFFIC STAGING OR CONSTRUCTION SHALL BE REAPPLIED OR REINSTALLED IN THE SAME LOCATION.
  5. STATION OR OFFSET FOR STOP BAR LOCATIONS IS GIVEN AT THE FRONT OF THE STOP BAR.
  6. A MINIMUM DISTANCE OF 4 FEET SHALL BE MAINTAINED BETWEEN THE EDGE OF THE CROSSWALK AND THE EDGE OF THE STOP BAR CLOSEST TO IT EXCEPT WHERE OTHERWISE NOTED IN THE PLANS.
  7. UNLESS OTHERWISE INDICATED, STREET NAME SIGNS SHALL MEET THE COLOR AND LETTERING SIZE ACCORDANCE WITH THE MANUAL ON TRAFFIC CONTROL DEVICES, 2009 EDITION.
  8. PREFORMED THERMOPLASTIC PAVEMENT MARKING SHALL BE USED ON THE BRIDGE ALL CONCRETE STRUCTURES.
  9. ALL EXISTING SIGNS ARE TO BE RETURNED TO THE CITY OF AURORA.

- LEGEND:
- SODDING SALT TOLERANT
  - PROPOSED TREE
  - (A) R1-1 36"X36"
  - (B) R1-4 18"X6"
  - (C) R2-1 30"X36"
  - (D) R8-3 18"X24"
  - (E) W1-7 36"X18"
  - (F) D3-1 36"X12"
  - (G) D3-1 36"X12"
  - (H) D3-1 36"X12"



FILE NAME = G:\DH11\0130\Road\Sheets\010130-SHT-LS-2.dgn	USER NAME = bafax	DESIGNED - MLB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>LANDSCAPING, SIGNING, AND PAVEMENT MARKING PLAN</b>	F.A.P. R.T.E. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 51		
PLOT SCALE = N/A	PLOT DATE 5/27/2014	CHECKED - MWS	REVISED -			SCALE: 1"=20'	SHEET NO. 51 OF 136 SHEETS	STA. 111+50	TO STA. 119+74.50	CONTRACT NO. 63859		
		DATE - 5/12/14	REVISED -			ILLINOIS FED. AID PROJECT						
#MODELNAME#												

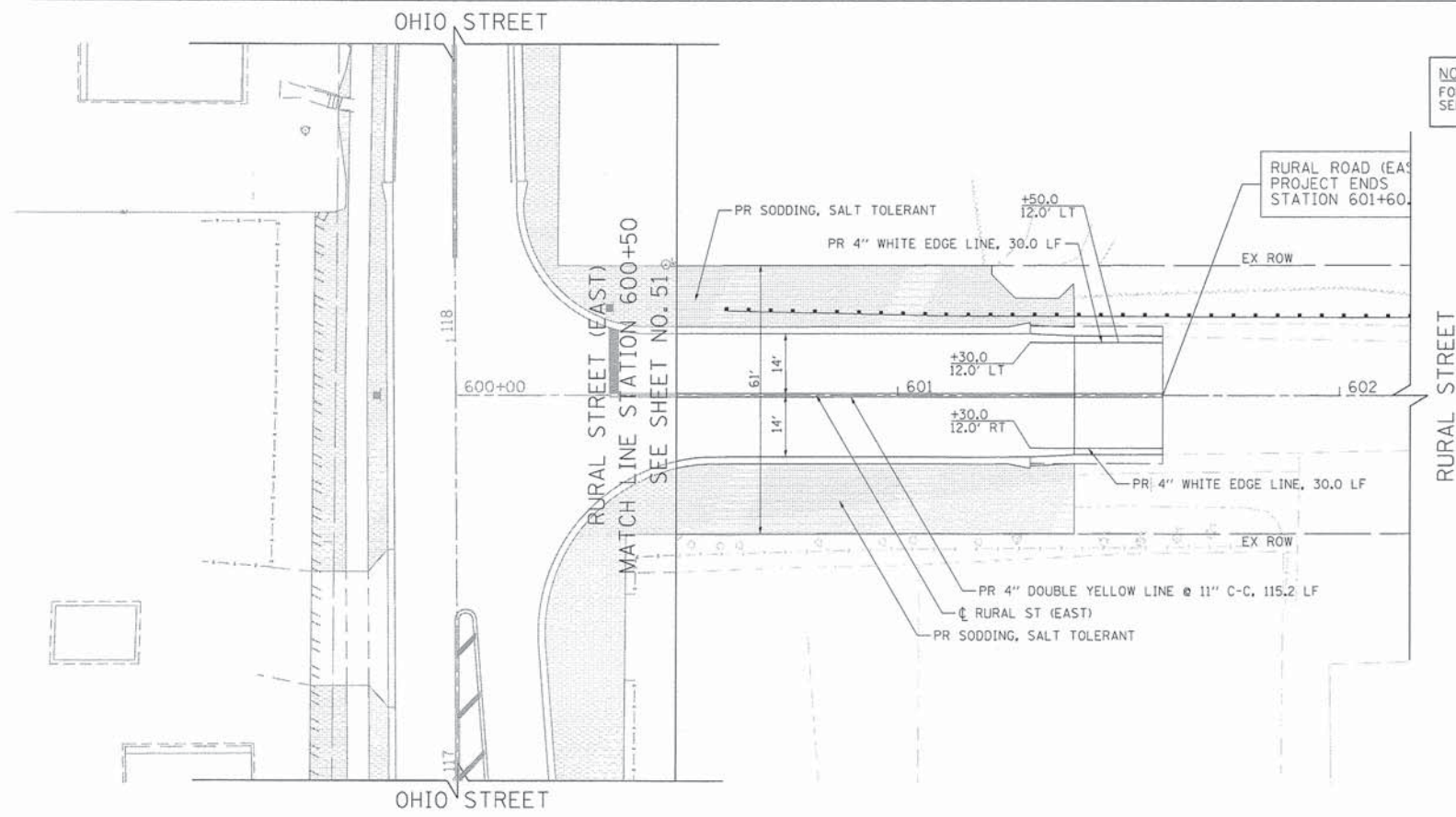


NOTES:

1. THERMOPLASTIC PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON HMA SURFACES UNLESS NOTED OTHERWISE.
2. FOR ADDITIONAL DETAILS SEE DISTRICT ONE TYPICAL PAVEMENT MARKING TC-13.
3. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 72 HOURS PRIOR TO THE PLACEMENT OF THE PAVEMENT MARKING.
4. EXISTING PAVEMENT MARKINGS THAT ARE REMOVED OUTSIDE THE IMPROVEMENT LIMITS FOR MAINTENANCE OF TRAFFIC STAGING OR CONSTRUCTION SHALL BE REAPPLIED OR REINSTALLED IN THE SAME LOCATION.
5. STATION OR OFFSET FOR STOP BAR LOCATIONS IS GIVEN AT THE FRONT OF THE STOP BAR.
6. A MINIMUM DISTANCE OF 4 FEET SHALL BE MAINTAINED BETWEEN THE EDGE OF THE CROSSWALK AND THE EDGE OF THE STOP BAR CLOSEST TO IT EXCEPT WHERE OTHERWISE NOTED IN THE PLANS.
7. UNLESS OTHERWISE INDICATED, STREET NAME SIGNS SHALL MEET THE COLOR AND LETTERING SIZE ACCORDANCE WITH THE MANUAL ON TRAFFIC CONTROL DEVICES, 2009 EDITION.
8. PREFORMED THERMOPLASTIC PAVEMENT MARKING SHALL BE USED ON THE BRIDGE ALL CONCRETE STRUCTURES.
9. ALL EXISTING SIGNS ARE TO BE RETURNED TO THE CITY OF AURORA.

LEGEND:

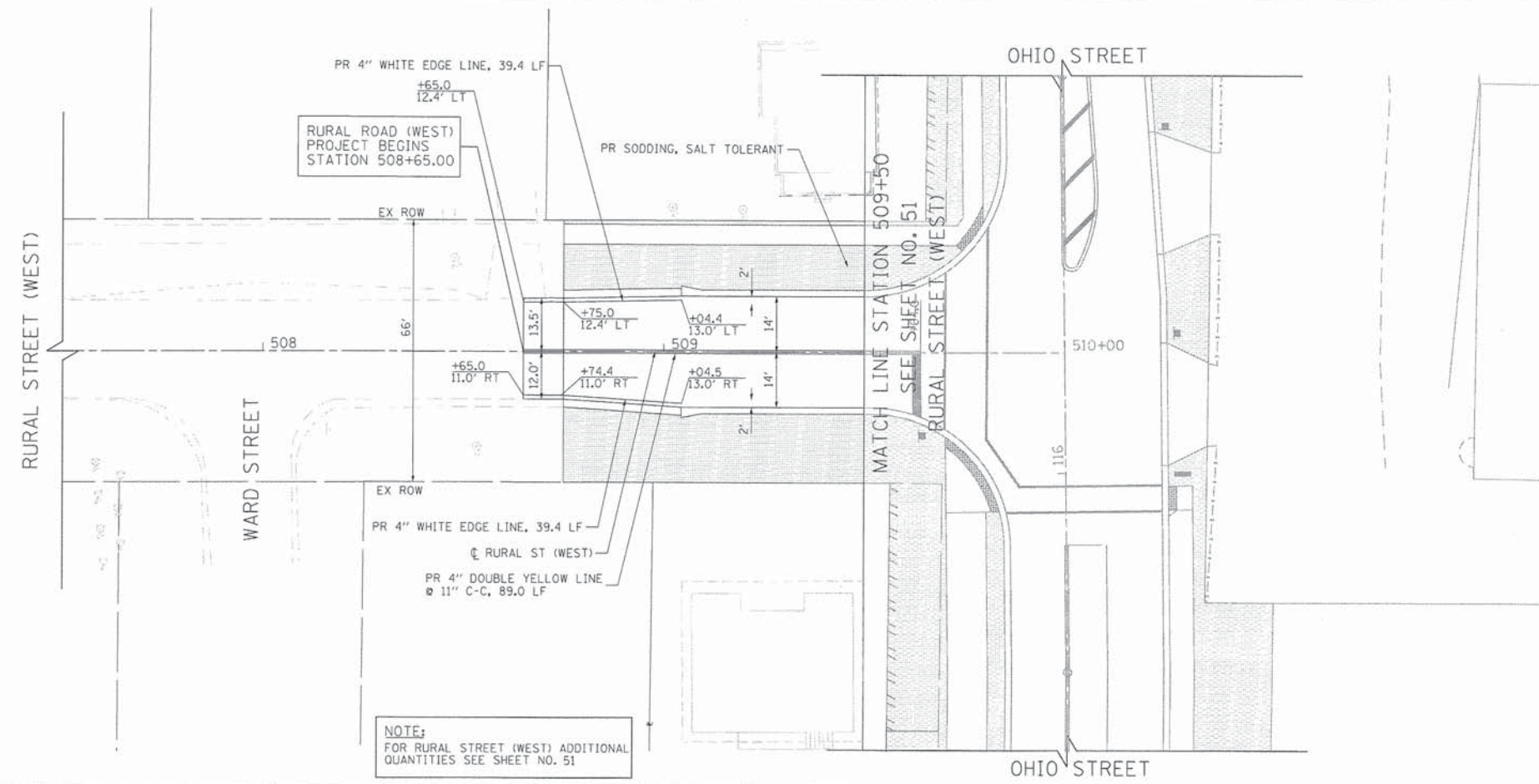
- SODDING SALT TOLERANT
- PROPOSED TREE
- (A) R1-1 36"X36"
- (B) R1-4 18"X6"
- (C) R2-1 30"X36"
- (D) R8-3 18"X24"
- (E) W1-7 36"X18"
- (F) D3-1 36"X12"
- (G) D3-1 36"X12"
- (H) D3-1 36"X12"



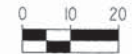
NOTE:  
FOR INDIAN AVENUE QUANTITIES  
SEE SHEET NO. 50



HORIZONTAL SCALE: 1"=20'



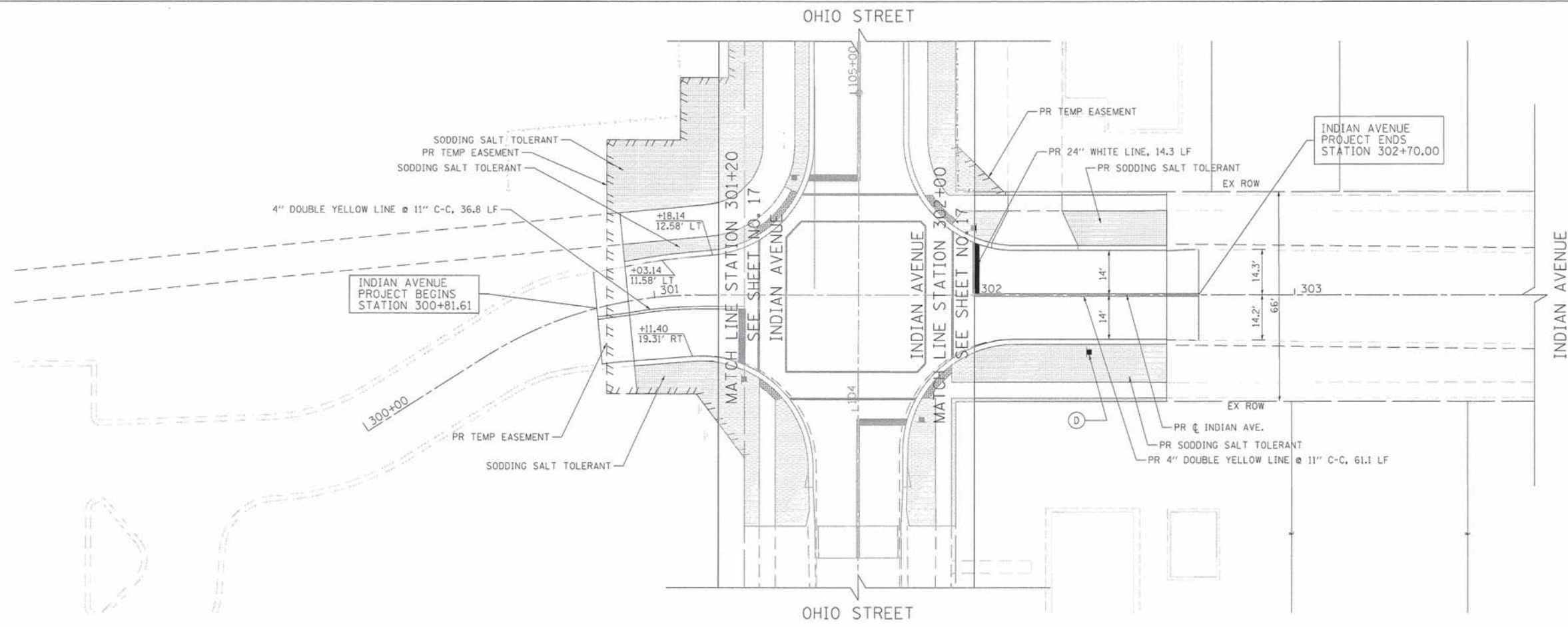
NOTE:  
FOR RURAL STREET (WEST) ADDITIONAL  
QUANTITIES SEE SHEET NO. 51



HORIZONTAL SCALE: 1"=20'

FILE NAME =	USER NAME = a_jpondexter	DESIGNED - MLB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>LANDSCAPING, SIGNING, AND PAVEMENT MARKING PLAN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
G:\CH11\0130\Road\Sheets\010130-SHT-LS-3.dgn		DRAWN - MLB	REVISED -			4087	08-00278-00-BR	KANE	136	52	
#MODELNAME#		CHECKED - MWS	REVISED -			CONTRACT NO. 63859					
		DATE - 5/12/14	REVISED -			[ILLINOIS] FED. AID PROJECT					
					SCALE: 1"=20'	SHEET NO. 52 OF 136 SHEETS		STA.	TO STA.		





HORIZONTAL SCALE: 1"=20'

**NOTES:**

1. THERMOPLASTIC PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON HMA SURFACES UNLESS NOTED OTHERWISE.
2. FOR ADDITIONAL DETAILS SEE DISTRICT ONE TYPICAL PAVEMENT MARKING TC-13.
3. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 72 HOURS PRIOR TO THE PLACEMENT OF THE PAVEMENT MARKING.
4. EXISTING PAVEMENT MARKINGS THAT ARE REMOVED OUTSIDE THE IMPROVEMENT LIMITS FOR MAINTENANCE OF TRAFFIC STAGING OR CONSTRUCTION SHALL BE REAPPLIED OR REINSTALLED IN THE SAME LOCATION.
5. STATION OR OFFSET FOR STOP BAR LOCATIONS IS GIVEN AT THE FRONT OF THE STOP BAR.
6. A MINIMUM DISTANCE OF 4 FEET SHALL BE MAINTAINED BETWEEN THE EDGE OF THE CROSSWALK AND THE EDGE OF THE STOP BAR CLOSEST TO IT EXCEPT WHERE OTHERWISE NOTED IN THE PLANS.
7. UNLESS OTHERWISE INDICATED, STREET NAME SIGNS SHALL MEET THE COLOR AND LETTERING SIZE ACCORDANCE WITH THE MANUAL ON TRAFFIC CONTROL DEVICES, 2009 EDITION.
8. PREFORMED THERMOPLASTIC PAVEMENT MARKING SHALL BE USED ON THE BRIDGE ALL CONCRETE STRUCTURES.
9. ALL EXISTING SIGNS ARE TO BE RETURNED TO THE CITY OF AURORA.

**LEGEND:**

- SODDING SALT TOLERANT
- PROPOSED TREE
- (A) R1-1 36"X36"
- (B) R1-4 18"X6"
- (C) R2-1 30"X36"
- (D) R8-3 18"X24"
- (E) W1-7 36"X18"
- (F) D3-1 36"X12"
- (G) D3-1 36"X12"
- (H) D3-1 36"X12"



HORIZONTAL SCALE: 1"=20'

FILE NAME =	USER NAME = ajpondexter	DESIGNED = MGB	REVISED =
Go\CH11\0130\Road\Sheets\0130-SHT-LS-4.dgn		DRAWN = MGB	REVISED =
	PLDT SCALE = N/A	CHECKED = MWS	REVISED =
#MODELNAME#	PLOT DATE = 5/12/2014	DATE = 5/12/14	REVISED =

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**LANDSCAPING, SIGNING, AND  
PAVEMENT MARKING PLAN**

SCALE: 1"=20' SHEET NO. 21 OF 136 SHEETS STA. 300+81.61 TO STA. 302+70.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	53
				CONTRACT NO. 63859
[ILLINOIS] FED. AID PROJECT				



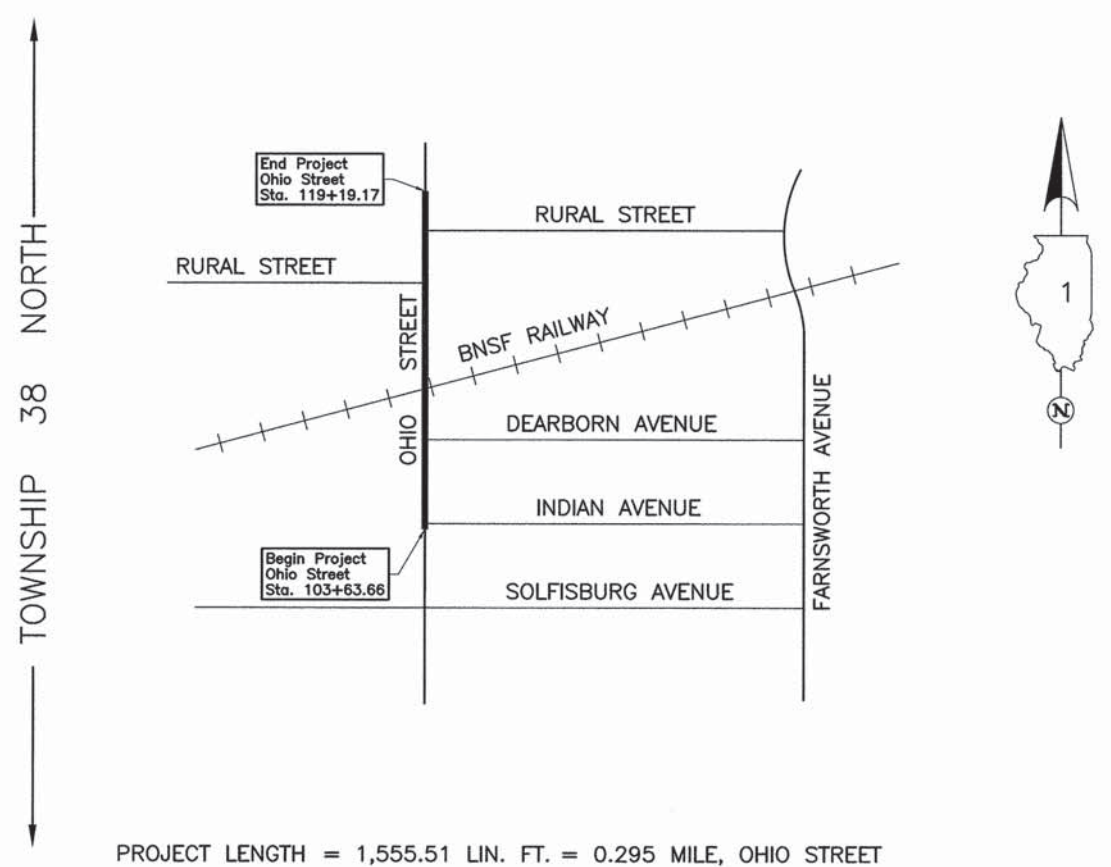
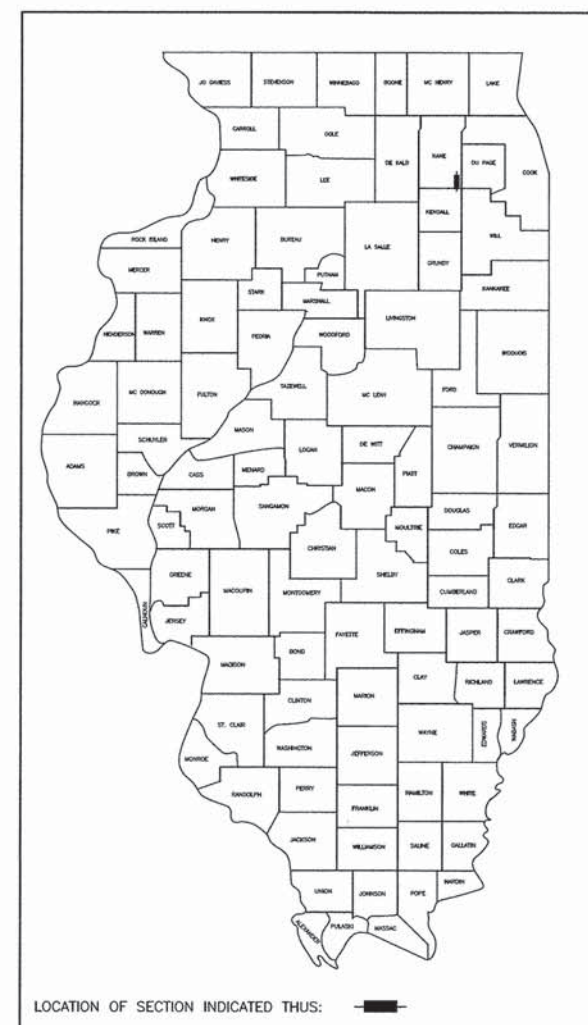
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
RIGHT OF WAY PLANS  
FOR PROPOSED  
FEDERAL AID HIGHWAY

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*		KANE	10	1

\* 08-00278-00-BR  
R-55-001-97

ROUTE: OHIO STREET  
SECTION: 08-00278-00-BR  
PROJECT NO.:  
JOB NO.: R-55-001-97  
COUNTY: KANE  
LIMITS: OHIO STREET @ BNSF RAILWAY

← RANGE 8 EAST →



APPROVED		20
	LOCAL AGENCY OFFICIAL	
APPROVED		20
	ENGINEER OF LAND ACQUISITION	
APPROVED		20
	ENGINEER OF LOCAL ROADS & STREETS	
APPROVED		20
	DISTRICT ENGINEER	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



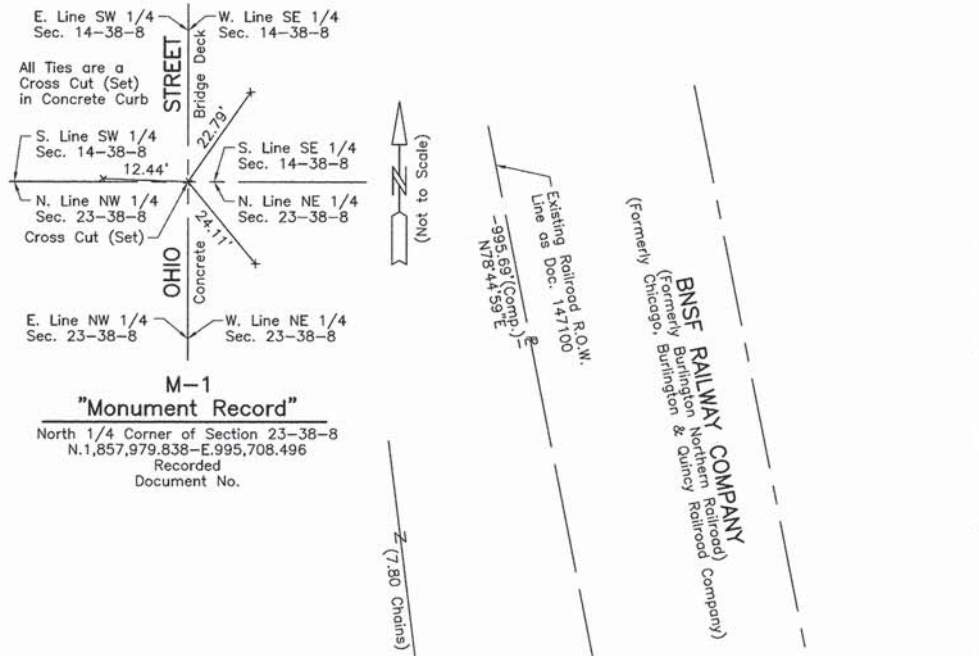
PART OF THE NORTH 1/2 OF SEC. 23, TWP. 38 N., R. 8 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
0001T.E.	City of Aurora, an Illinois municipal corporation	14.961	N/A	N/A	14.961	0.115	Construction Purposes	15-23-126-016	
0005T.E.	BNSF Railway Company, formerly Burlington Northern Railroad, which acquired the property as Chicago, Burlington & Quincy Railroad Company	N/A	N/A	N/A	N/A	0.160	Grading	15-14-501-002 (pt) 15-23-501-001 (pt) 15-23-501-005 (pt)	

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
0001	32151	September 19, 1890
0001	147100	May 18, 1915
0001	147105	May 18, 1915
0005	147100	May 18, 1915
0005	147105	May 18, 1915
----	24046	July 8, 1889
----	33259	November 8, 1890

**WOODRUFF'S SUBDIVISION**  
Recorded December 6, 1866  
in Book 1 of Plats, Page 161

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
105+05.03	40.81' Lt.	1,857,451.231	995,676.093
105+05.03	55.81' Lt.	1,857,451.033	995,661.094
106+97.58	35.31' Lt.	1,857,643.838	995,679.053
107+23.11	40.24' Lt.	1,857,669.299	995,673.783
107+98.66	30.96' Rt.	1,857,745.781	995,743.977
108+64.78	31.00' Rt.	1,857,811.777	995,743.278
109+48.14	84.25' Lt.	1,857,894.069	995,627.260
109+65.02	35.13' Lt.	1,857,911.401	995,676.219
109+65.02	84.27' Lt.	1,857,910.947	995,627.081
109+77.79	35.15' Lt.	1,857,924.172	995,676.084
109+90.28	30.83' Rt.	1,857,937.274	995,741.949
110+33.16	2.23' Lt.	1,857,979.838	995,708.496



**ASSESSORS MAP OF AURORA**  
Recorded November 7, 1885  
as Document No. 4211

**LEGEND**

SECTION CORNER 16 QUARTER SECTION CORNER 15

SECTION LINE  
QUARTER SECTION LINE  
QUARTER SECTION LINE  
PLATTED LOT LINE  
PROPERTY (DEED) LINE

APL APPARENT PROPERTY LINE  
CENTER LINE  
EXISTING RIGHT OF WAY LINE  
PROPOSED RIGHT OF WAY LINE  
PROPOSED EASEMENT  
MEASURED DIMENSION  
COMPUTED DIMENSION  
RECORD DATA

EXISTING BUILDING

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2011) East Zone.

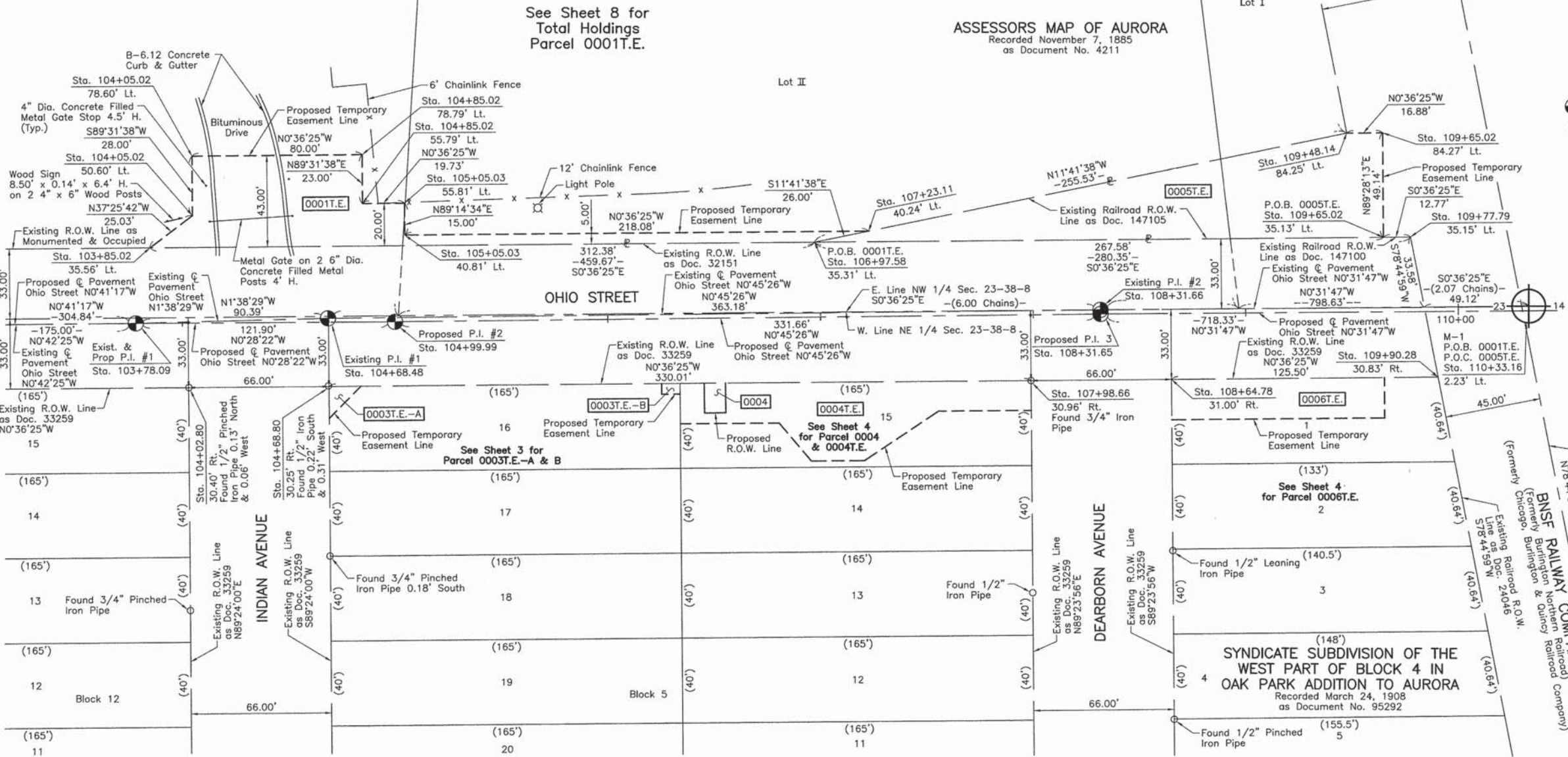
○ IRON PIPE OR ROD FOUND      ⊗ "MAG" NAIL SET  
+ CUT CROSS FOUND OR SET      ● 5/8" REBAR SET

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.  
■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
⊙ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)  
□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 23, TOWNSHIP 38N., RANGE 8E., OF THE THIRD PRINCIPAL MERIDIAN, KANE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS \_\_\_\_\_ DAY OF \_\_\_\_\_ 20\_\_ A.D.



ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2014  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
Note: Surface Coordinates are Shown

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
103+85.02	35.56' Lt.	1,857,331.476	995,682.362
104+02.80	30.40' Rt.	1,857,349.792	995,748.171
104+05.02	50.60' Lt.	1,857,351.351	995,667.150
104+05.02	78.60' Lt.	1,857,351.120	995,639.151
104+68.80	30.25' Rt.	1,857,415.788	995,747.472
104+85.02	55.79' Lt.	1,857,431.301	995,661.303
104+85.02	78.79' Lt.	1,857,431.111	995,638.304

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

**PLAT OF HIGHWAYS**  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
OHIO STREET

SECTION 08-00278-00-BR KANE COUNTY  
PROJECT JOB NO. R-55-001-97  
STATION 103+00 TO STATION 111+00  
SCALE: 1"=30' SHEET 2 OF 10

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

**OAK PARK ADDITION TO AURORA**  
Recorded November 8, 1890  
as Document No. 33259



PART OF THE NORTH 1/2 OF SEC. 23, TWP. 38 N., R. 8 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA		EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
						ACRES	SQUARE FEET			
0003T.E.-A 0003T.E.-B	Santiago Barajas and Ninfa Barajas, husband and wife, as tenants by the entirety	0.303	N/A	N/A	0.303	T.E.-A=0.003 T.E.-B=0.001	113 43	Grading Grading	15-23-202-012	

### LEGEND

SECTION CORNER 16 QUARTER SECTION CORNER

SECTION LINE  
QUARTER SECTION LINE  
QUARTER, QUARTER SECTION LINE  
PLATTED LOT LINE  
PROPERTY (DEED) LINE

APL APPARENT PROPERTY LINE  
CENTER LINE  
EXISTING RIGHT OF WAY LINE  
PROPOSED RIGHT OF WAY LINE  
PROPOSED EASEMENT  
MEASURED DIMENSION  
COMPUTED DIMENSION  
RECORD DATA

EXISTING BUILDING

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2011) East Zone.

O IRON PIPE OR ROD FOUND ⊕ "MAG" NAIL SET  
+ CUT CROSS FOUND OR SET ● 5/8" REBAR SET

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● T2  
● T3

● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● BT2  
● BT3

■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

● PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)

□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 23, TOWNSHIP 38N., RANGE 8E., OF THE THIRD PRINCIPAL MERIDIAN, KANE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS. DATED AT LAKE VILLA, ILLINOIS THIS \_\_\_\_ DAY OF \_\_\_\_ 20\_\_ A.D.

PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2014  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
Note: Surface Coordinates are Shown

STATION	OFFSET	NORTH	EAST
104+02.80	30.40' Rt.	1,857,349.792	995,748.171
104+68.80	30.25' Rt.	1,857,415.788	995,747.472
104+68.83	45.25' Rt.	1,857,415.945	995,762.473
104+83.80	30.21' Rt.	1,857,430.787	995,747.314
106+25.03	30.50' Rt.	1,857,572.158	995,745.816
106+25.03	35.50' Rt.	1,857,572.224	995,750.816
106+33.64	35.53' Rt.	1,857,580.837	995,750.724
106+33.65	30.53' Rt.	1,857,580.784	995,745.725

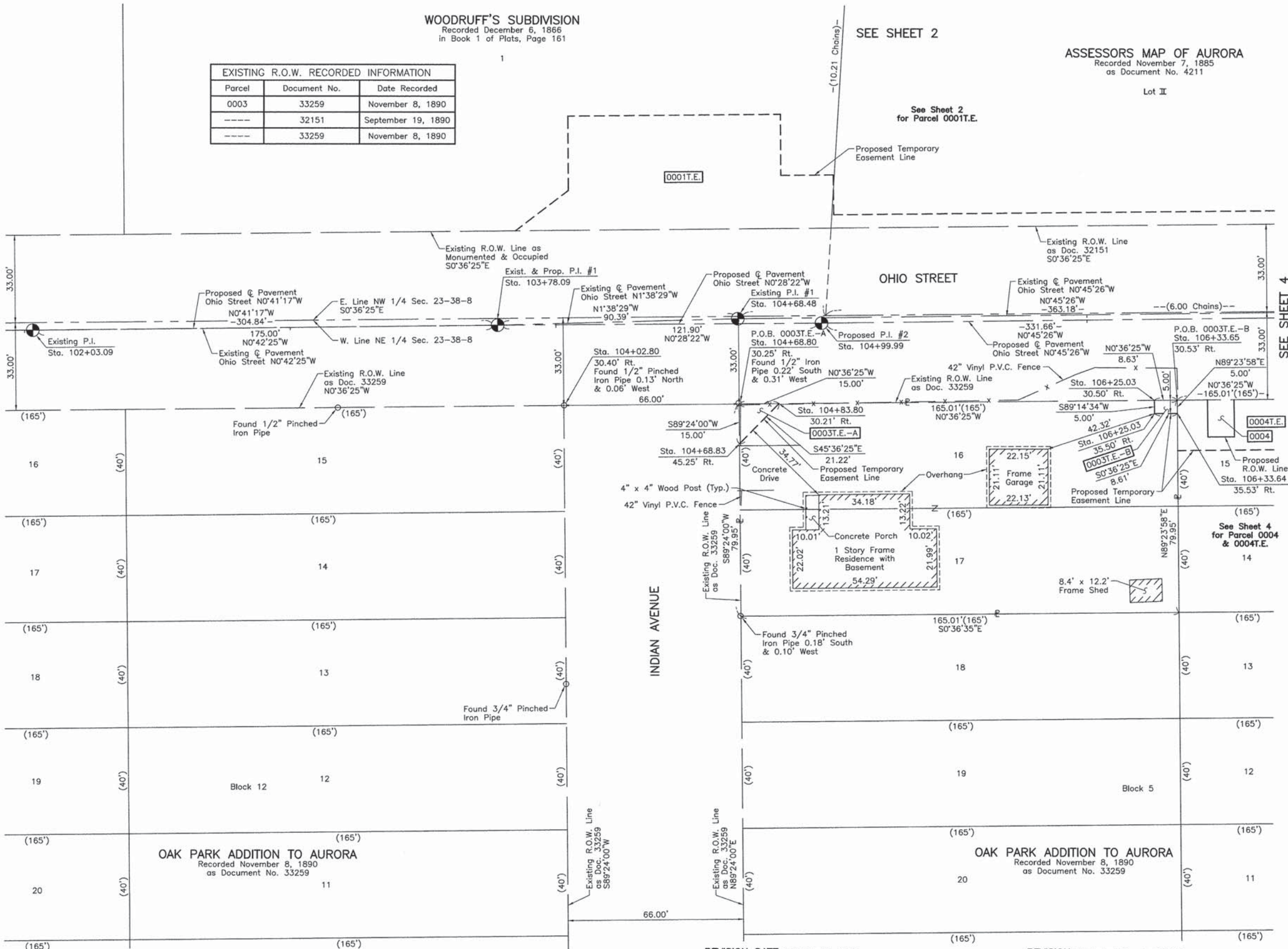
JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046 SHEET 1 IS A COVER  
(847) 356-3371 SHEET AND IS NOT RECORDED.

#### EXISTING R.O.W. RECORDED INFORMATION

Parcel	Document No.	Date Recorded
0003	33259	November 8, 1890
-----	32151	September 19, 1890
-----	33259	November 8, 1890

**WOODRUFF'S SUBDIVISION**  
Recorded December 6, 1866  
in Book 1 of Plats, Page 161

**ASSESSORS MAP OF AURORA**  
Recorded November 7, 1885  
as Document No. 4211



REVISION DATE October 31, 2013

REVISION Eliminated Parcel 0002T.E.

MADE BY

**PLAT OF HIGHWAYS**  
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
OHIO STREET  
SECTION 08-00278-00-BR KANE COUNTY  
PROJECT JOB NO. R-55-001-97  
STATION 102+00 TO STATION 107+00  
SCALE: 1"=20' SHEET 3 OF 10

**BUREAU OF LAND ACQUISITION**  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196



PART OF THE NORTH 1/2 OF SEC. 23, TWP. 38 N., R. 8 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
106+95.02	67.69' Rt.	1,857,642.639	995,782.072
106+97.58	35.31' Lt.	1,857,643.838	995,679.053
107+20.02	67.75' Rt.	1,857,667.638	995,781.807
107+55.03	44.85' Rt.	1,857,702.339	995,758.438
107+98.62	44.96' Rt.	1,857,745.928	995,757.976
107+98.66	30.96' Rt.	1,857,745.781	995,743.977
108+64.78	31.00' Rt.	1,857,811.777	995,743.278
108+64.80	50.00' Rt.	1,857,811.976	995,762.277
109+48.14	84.25' Lt.	1,857,894.069	995,627.260
109+65.02	30.87' Rt.	1,857,912.016	995,742.216
109+65.02	49.87' Rt.	1,857,912.192	995,761.215
109+77.79	35.15' Lt.	1,857,924.172	995,676.084
109+90.28	30.83' Rt.	1,857,937.274	995,741.949
110+33.16	2.23' Lt.	1,857,979.838	995,708.496

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART ACRES	TAKEN SQUARE FEET	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
0004 0004T.E.	Heliodoro Silva	0.379	0.003	140	N/A	0.376	0.082	Construction Purposes	15-23-202-001	
0006T.E.	BNSF Railway Company, formerly Burlington Northern Railroad, which acquired the property as Chicago, Burlington & Quincy Railroad Company	0.119	N/A	N/A	N/A	0.119	0.044	Grading	15-23-501-006	

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
0004	33259	November 8, 1890
0006	24046	July 8, 1889
0006	33259	November 8, 1890
-----	24046	July 8, 1889
-----	32151	September 19, 1890
-----	33259	November 8, 1890
-----	147100	May 18, 1915
-----	147105	May 18, 1915

ASSESSORS MAP OF AURORA  
Recorded November 7, 1885  
as Document No. 4211

Schedule of Ties		
Point Number	Tie to point	Tie Distance (feet)
1	T1 T2 T3	29.40 23.85 23.40
2	T1 T2 T3	20.21 10.76 10.23
3	T1 T2 T3	12.25 10.23 17.55
4	T1 T2 T3	24.62 23.64 27.42

**LEGEND**

SECTION CORNER 16 QUARTER SECTION CORNER 15

SECTION LINE  
QUARTER SECTION LINE  
QUARTER, QUARTER SECTION LINE  
PLATTED LOT LINE  
PROPERTY (DEED) LINE

APL  
APPARENT PROPERTY LINE  
CENTER LINE  
EXISTING RIGHT OF WAY LINE  
PROPOSED RIGHT OF WAY LINE  
PROPOSED EASEMENT  
MEASURED DIMENSION  
COMPUTED DIMENSION  
RECORD DATA

EXISTING BUILDING

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2011) East Zone.

SCALE: 1"=20'

0 20' 40'

○ IRON PIPE OR ROD FOUND      ⊕ "MAG" NAIL SET  
+ CUT CROSS FOUND OR SET      ● 5/8" REBAR SET

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● T2  
● T3

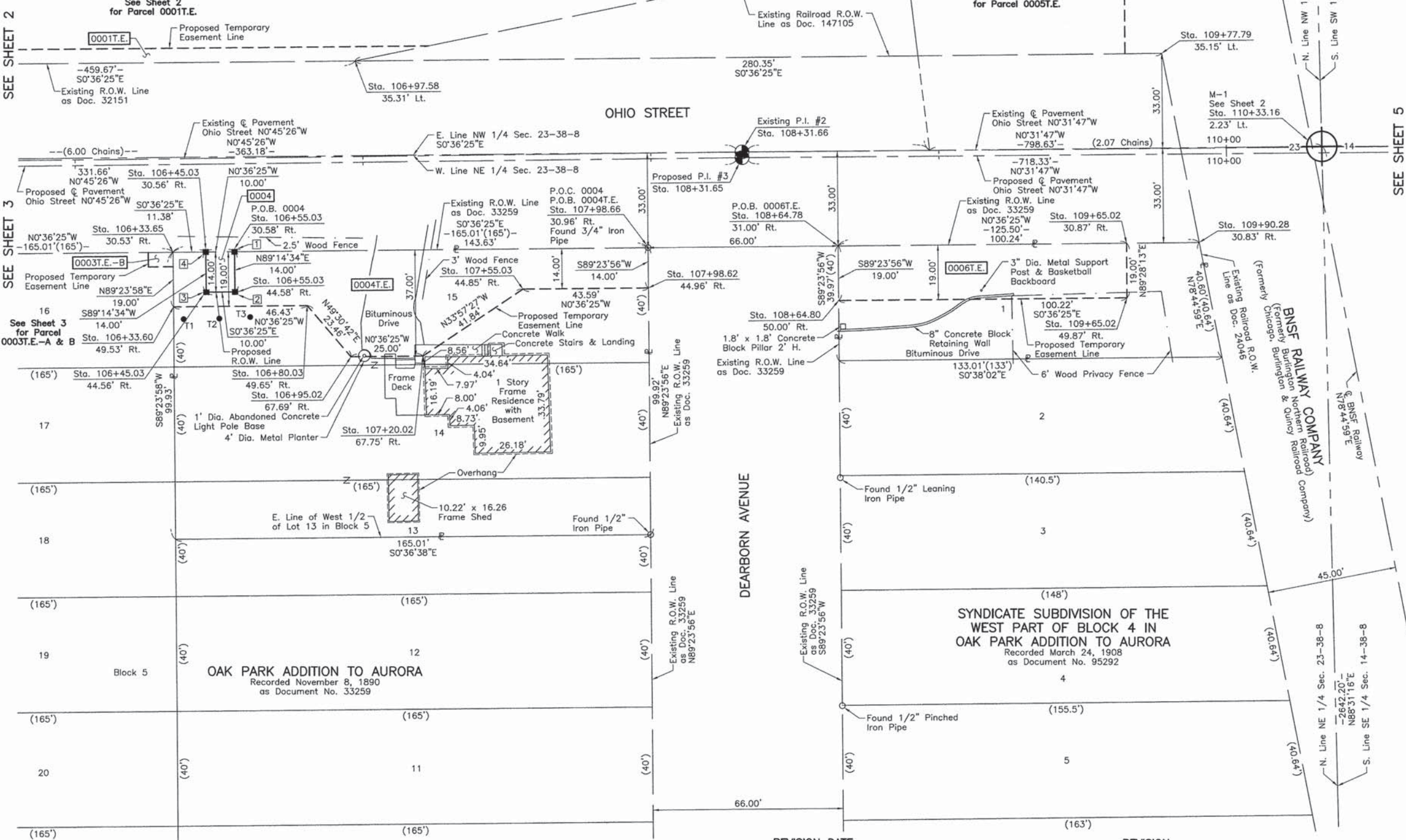
● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● BT2  
● BT3

■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

⊙ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)

□ RIGHT OF WAY STAKING PROPOSED TO BE SET.



STATE OF ILLINOIS }  
COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 23, TOWNSHIP 38N., RANGE 8E., OF THE THIRD PRINCIPAL MERIDIAN, KANE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS \_\_\_\_\_ DAY OF \_\_\_\_\_ 20\_\_ A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2014  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
Note: Surface Coordinates are Shown

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
106+33.60	49.53' Rt.	1,857,580.983	995,764.724
106+33.65	30.53' Rt.	1,857,580.784	995,745.725
106+45.03	30.56' Rt.	1,857,592.162	995,745.604
106+45.03	44.56' Rt.	1,857,592.347	995,759.603
106+55.03	30.58' Rt.	1,857,602.161	995,745.498
106+55.03	44.58' Rt.	1,857,602.346	995,759.497
106+80.03	49.65' Rt.	1,857,627.409	995,764.232

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046 SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.  
(847) 356-3371

**PLAT OF HIGHWAYS**  
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
OHIO STREET  
SECTION 08-00278-00-BR KANE COUNTY  
PROJECT JOB NO. R-55-001-97  
STATION 106+00 TO STATION 111+00  
SCALE: 1"=20' SHEET 4 OF 10

**BUREAU OF LAND ACQUISITION**  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196



PART OF THE SOUTH 1/2 OF SEC. 14 AND PART OF THE NORTH 1/2 OF SEC. 23, TWP. 38 N., R. 8 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.

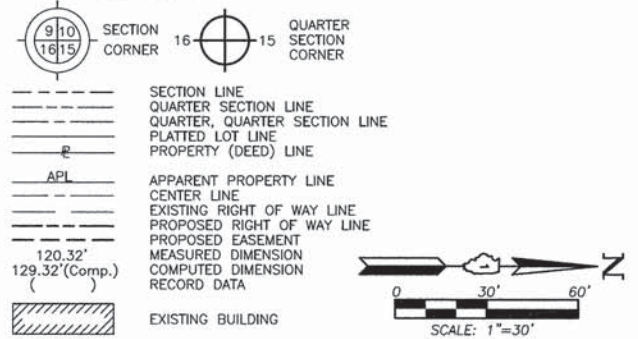
PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT SQUARE FEET	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
0007P.E. 0007T.E.-A 0007T.E.-B	Ohio Street, LLC, an Illinois limited liability company	5.316	N/A	N/A	5.316	P.E.=0.085 T.E.-A=0.051 T.E.-B=0.004	N/A N/A 175	Drainage Grading Driveway Construction	15-14-452-003	
0008P.E.	Fox Valley Park District	0.557	N/A	N/A	0.557	0.078	N/A	Highway Purposes	15-14-378-019	
0015P.E.	BNSF Railway Company, formerly Burlington Northern Railroad, which acquired the property as Chicago, Burlington & Quincy Railroad Company	N/A	N/A	N/A	N/A	0.110	N/A	Highway Purposes	15-14-501-003	

Point Number	Tie to point	Tie Distance (feet)
6	T1 T2 T3	
7	T1 T2 T3	
8	T1 T2 T3	

Parcel	Document No.	Date Recorded
0007	Book 79 of Deeds, Page 579	June 2, 1864
0007	24046	July 8, 1889
0007	98K085063	September 18, 1998
0008	147100	May 18, 1915
0015	Book 1 of Plats, Page 72	July 9, 1857
0015	147100	May 18, 1915
----	Book 1 of Plats, Page 72	July 9, 1857
----	24046	July 8, 1889

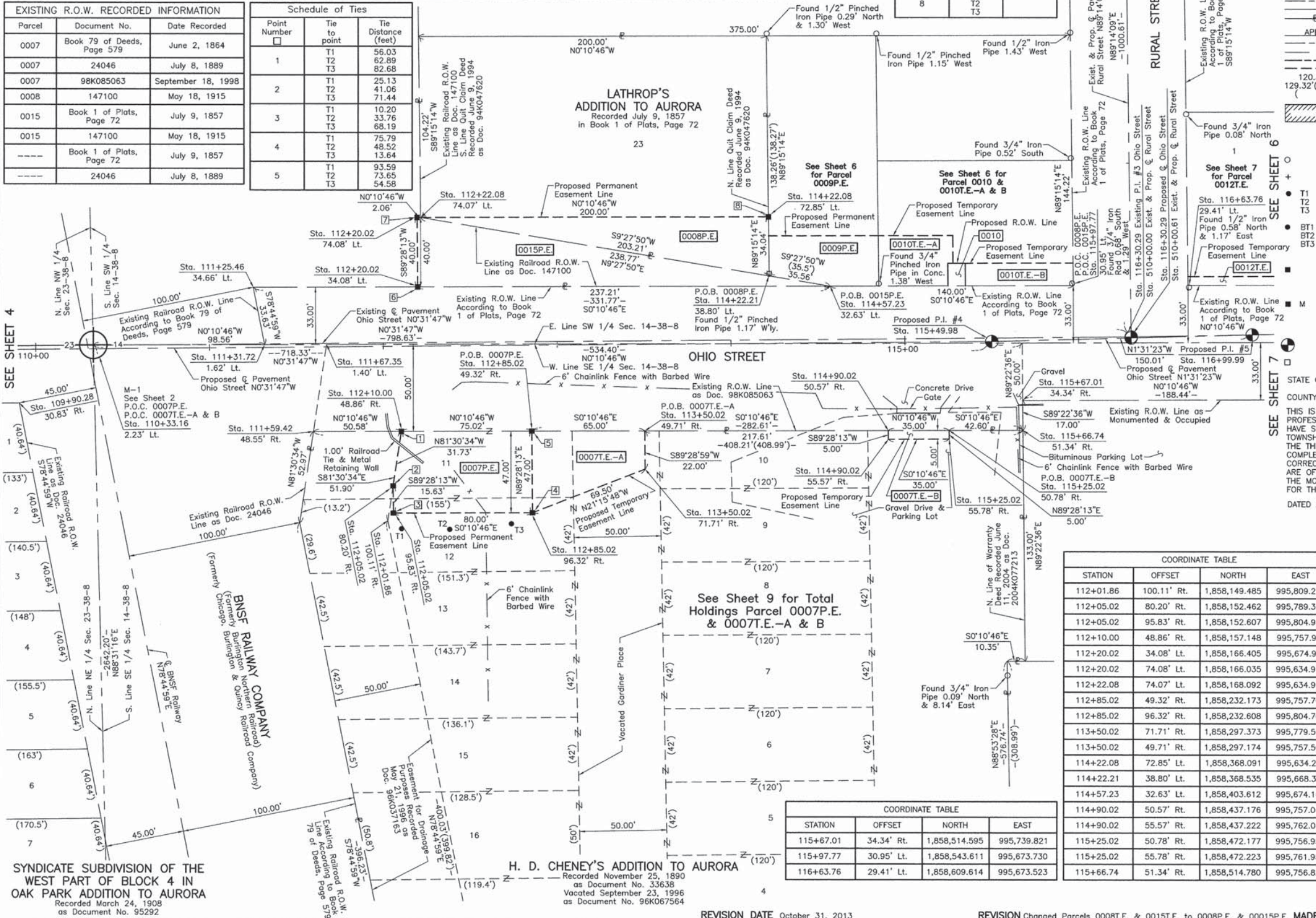
Point Number	Tie to point	Tie Distance (feet)
1	T1 T2 T3	56.03 82.89 82.68
2	T1 T2 T3	25.13 41.06 71.44
3	T1 T2 T3	10.20 33.76 68.19
4	T1 T2 T3	75.79 48.52 13.64
5	T1 T2 T3	93.59 73.85 54.58

LEGEND



Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2011) East Zone.

- IRON PIPE OR ROD FOUND
- MAG NAIL SET
- CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.



SYNDICATE SUBDIVISION OF THE WEST PART OF BLOCK 4 IN OAK PARK ADDITION TO AURORA  
Recorded March 24, 1908  
as Document No. 95292

H. D. CHENEY'S ADDITION TO AURORA  
Recorded November 25, 1890  
as Document No. 33638  
Vacated September 23, 1996  
as Document No. 96K067564

STATION	OFFSET	NORTH	EAST
115+67.01	34.34' Rt.	1,858,514.595	995,739.821
115+97.77	30.95' Lt.	1,858,543.611	995,673.730
116+63.76	29.41' Lt.	1,858,609.614	995,673.523

STATION	OFFSET	NORTH	EAST
112+01.86	100.11' Rt.	1,858,149.485	995,809.267
112+05.02	80.20' Rt.	1,858,152.462	995,789.326
112+05.02	95.83' Rt.	1,858,152.607	995,804.955
112+10.00	48.86' Rt.	1,858,157.148	995,757.941
112+20.02	34.08' Lt.	1,858,166.405	995,674.911
112+20.02	74.08' Lt.	1,858,166.035	995,634.914
112+22.08	74.07' Lt.	1,858,168.092	995,634.908
112+85.02	49.32' Rt.	1,858,232.173	995,757.706
112+85.02	96.32' Rt.	1,858,232.608	995,804.705
113+50.02	71.71' Rt.	1,858,297.373	995,779.502
113+50.02	49.71' Rt.	1,858,297.174	995,757.502
114+22.08	72.85' Lt.	1,858,368.091	995,634.281
114+22.21	38.80' Lt.	1,858,368.535	995,668.321
114+57.23	32.63' Lt.	1,858,403.612	995,674.168
114+90.02	50.57' Rt.	1,858,437.176	995,757.064
114+90.02	55.57' Rt.	1,858,437.222	995,762.063
115+25.02	50.78' Rt.	1,858,472.177	995,756.954
115+25.02	55.78' Rt.	1,858,472.223	995,761.954
115+66.74	51.34' Rt.	1,858,514.780	995,756.820

PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2014  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

Note: Surface Coordinates are Shown

STATION	OFFSET	NORTH	EAST
109+90.28	30.83' Rt.	1,857,937.274	995,741.949
110+33.16	2.23' Lt.	1,857,979.838	995,708.496
111+25.46	34.66' Lt.	1,858,071.839	995,675.208
111+31.72	1.62' Lt.	1,858,078.399	995,708.187
111+59.42	48.55' Rt.	1,858,106.570	995,758.099
111+67.35	1.40' Lt.	1,858,114.038	995,708.076

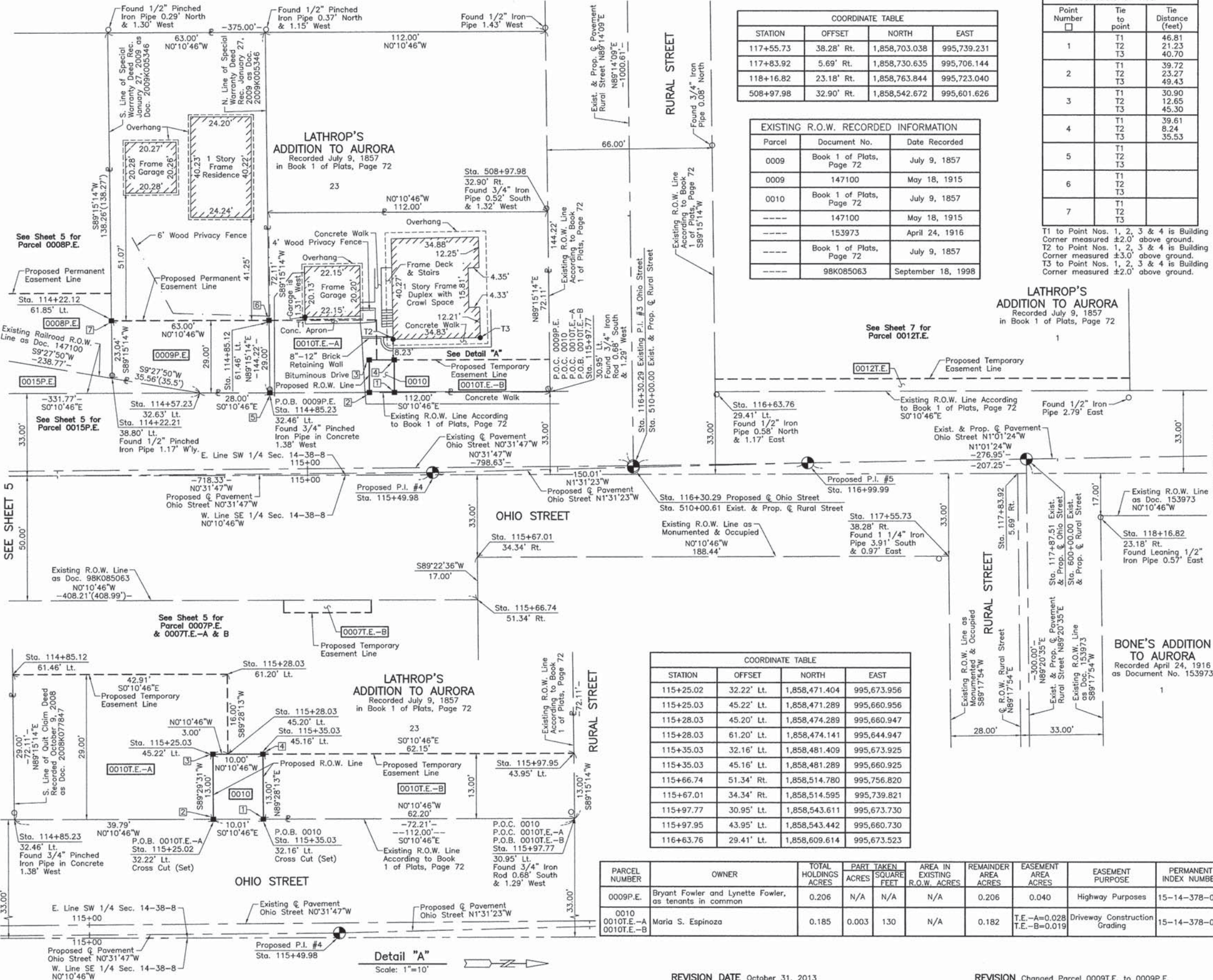
JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

PLAT OF HIGHWAYS  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
OHIO STREET  
SECTION 08-00278-00-BR KANE COUNTY  
PROJECT JOB NO. R-55-001-97  
STATION 110+00 TO STATION 117+00  
SCALE: 1"=30' SHEET 5 OF 10

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196



PART OF THE SOUTH 1/2 OF SEC. 14, TWP. 38 N., R. 8 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.



STATION	OFFSET	NORTH	EAST
117+55.73	38.28' Rt.	1,858,703.038	995,739.231
117+83.92	5.69' Rt.	1,858,730.635	995,706.144
118+16.82	23.18' Rt.	1,858,763.844	995,723.040
508+97.98	32.90' Rt.	1,858,542.672	995,601.626

Parcel	Document No.	Date Recorded
0009	Book 1 of Plats, Page 72	July 9, 1857
0009	147100	May 18, 1915
0010	Book 1 of Plats, Page 72	July 9, 1857
----	147100	May 18, 1915
----	153973	April 24, 1916
----	Book 1 of Plats, Page 72	July 9, 1857
----	98K085063	September 18, 1998

Point Number	Tie to point	Tie Distance (feet)
1	T1	46.81
	T2	21.23
	T3	40.70
2	T1	39.72
	T2	23.27
	T3	49.43
3	T1	30.90
	T2	12.65
	T3	45.30
4	T1	39.61
	T2	8.24
	T3	35.53
5	T1	
	T2	
	T3	
6	T1	
	T2	
	T3	
7	T1	
	T2	
	T3	

**LEGEND**

SECTION CORNER 16 QUARTER SECTION CORNER 15

SECTION LINE  
QUARTER SECTION LINE  
QUARTER, QUARTER SECTION LINE  
PLATTED LOT LINE  
PROPERTY (DEED) LINE

APL APPARENT PROPERTY LINE  
CENTER LINE  
EXISTING RIGHT OF WAY LINE  
PROPOSED RIGHT OF WAY LINE  
PROPOSED EASEMENT  
MEASURED DIMENSION  
COMPUTED DIMENSION  
RECORD DATA

EXISTING BUILDING

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2011) East Zone.

0 IRON PIPE OR ROD FOUND @ "MAG" NAIL SET  
+ CUT CROSS FOUND OR SET • 5/8" REBAR SET

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● T2  
● T3

● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● BT2  
● BT3

■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

○ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)

□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 14, TOWNSHIP 38N., RANGE 8E., OF THE THIRD PRINCIPAL MERIDIAN, KANE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS \_\_\_\_ DAY OF \_\_\_\_ 20\_\_ A.D.

\_\_\_\_ PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2014  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
Note: Surface Coordinates are Shown

STATION	OFFSET	NORTH	EAST
115+25.02	32.22' Lt.	1,858,471.404	995,673.956
115+25.03	45.22' Lt.	1,858,471.289	995,660.956
115+28.03	45.20' Lt.	1,858,474.289	995,660.947
115+28.03	61.20' Lt.	1,858,474.141	995,644.947
115+35.03	32.16' Lt.	1,858,481.409	995,673.925
115+35.03	45.16' Lt.	1,858,481.289	995,660.925
115+66.74	51.34' Rt.	1,858,514.780	995,756.820
115+67.01	34.34' Rt.	1,858,514.595	995,739.821
115+97.77	30.95' Lt.	1,858,543.611	995,673.730
115+97.95	43.95' Lt.	1,858,543.442	995,660.730
116+63.76	29.41' Lt.	1,858,609.614	995,673.523

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	SQUARE FEET	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
0009P.E.	Bryant Fowler and Lynette Fowler, as tenants in common	0.206	N/A	N/A	N/A	0.206	0.040	Highway Purposes	15-14-378-018	
0010	Maria S. Espinoza	0.185	0.003	130	N/A	0.182	T.E.-A=0.028 T.E.-B=0.019	Driveway Construction Grading	15-14-378-017	

STATION	OFFSET	NORTH	EAST
114+22.12	61.85' Lt.	1,858,368.234	995,645.279
114+22.21	38.80' Lt.	1,858,368.535	995,668.321
114+57.23	32.63' Lt.	1,858,403.612	995,674.168
114+85.12	61.46' Lt.	1,858,431.234	995,645.082
114+85.23	32.46' Lt.	1,858,431.612	995,674.081

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

SHEET 1 IS A COVER  
SHEET AND IS NOT RECORDED.

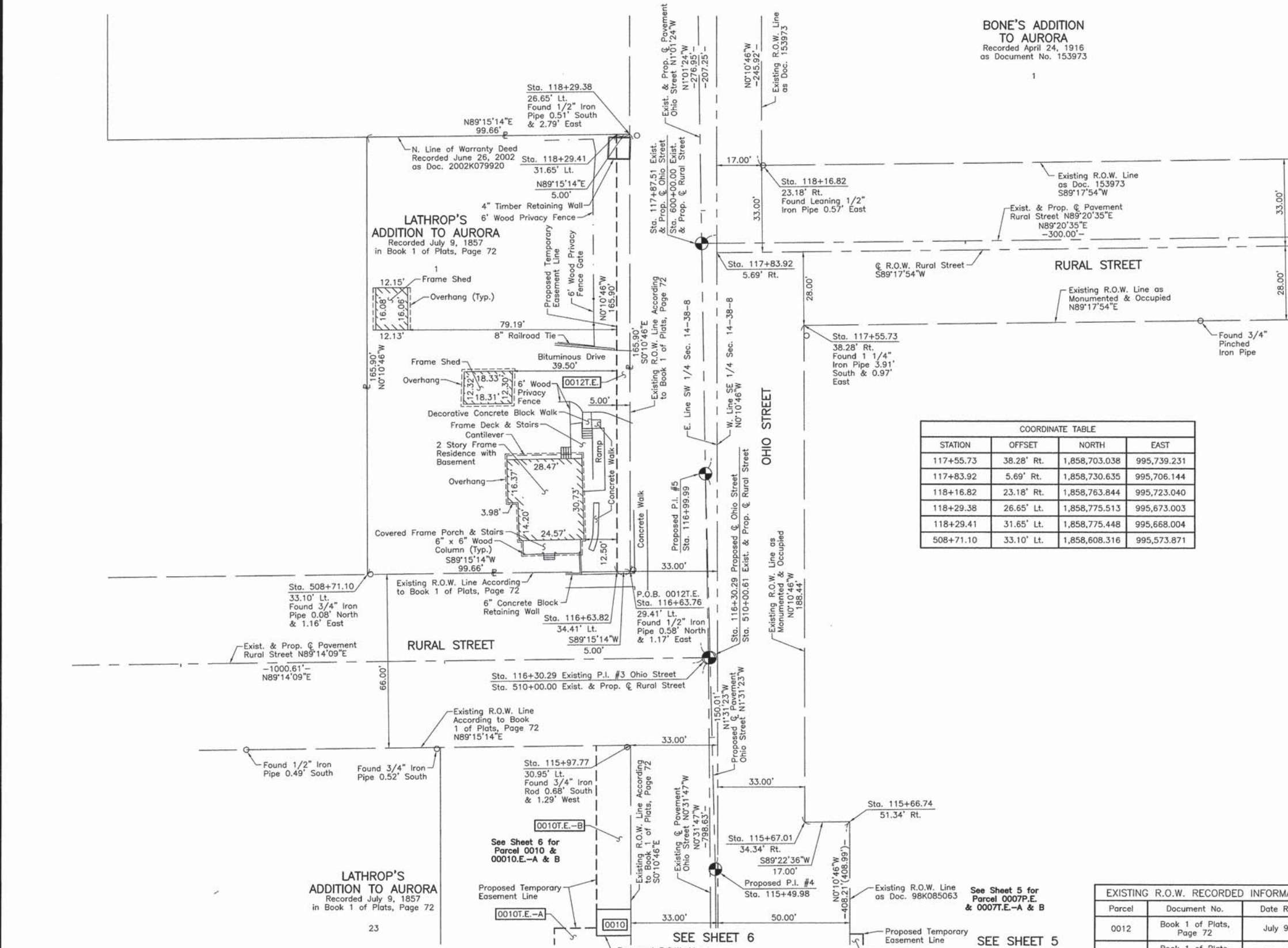
**PLAT OF HIGHWAYS**  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
OHIO STREET

SECTION 08-00278-00-BR KANE COUNTY  
PROJECT JOB NO. R-55-001-97  
STATION 114+00 TO STATION 119+00  
STATION 508+00 TO STATION 510+00.61  
STATION 600+00 TO STATION 601+00  
SCALE: 1"=20' SHEET 6 OF 10

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196



PART OF THE SOUTH 1/2 OF SEC. 14, TWP. 38 N., R. 8 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.

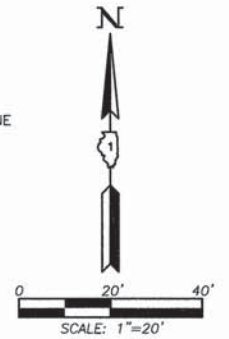


**BONE'S ADDITION TO AURORA**  
Recorded April 24, 1916  
as Document No. 153973

**LATHROP'S ADDITION TO AURORA**  
Recorded July 9, 1857  
in Book 1 of Plats, Page 72

**LEGEND**

- SECTION CORNER
  - QUARTER SECTION CORNER
  - SECTION LINE
  - QUARTER SECTION LINE
  - QUARTER, QUARTER SECTION LINE
  - PLATTED LOT LINE
  - PROPERTY (DEED) LINE
  - APL
  - APPARENT PROPERTY LINE
  - CENTER LINE
  - EXISTING RIGHT OF WAY LINE
  - PROPOSED RIGHT OF WAY LINE
  - PROPOSED EASEMENT
  - MEASURED DIMENSION
  - COMPUTED DIMENSION
  - RECORD DATA
  - EXISTING BUILDING
- Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2011) East Zone.
- IRON PIPE OR ROD FOUND
  - ⊕ "MAG" NAIL SET
  - + CUT CROSS FOUND OR SET
  - 5/8" REBAR SET
  - T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - BT2
  - BT3
  - STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
  - M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - ⊙ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
  - RIGHT OF WAY STAKING PROPOSED TO BE SET.



COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
117+55.73	38.28' Rt.	1,858,703.038	995,739.231
117+83.92	5.69' Rt.	1,858,730.635	995,706.144
118+16.82	23.18' Rt.	1,858,763.844	995,723.040
118+29.38	26.65' Lt.	1,858,775.513	995,673.003
118+29.41	31.65' Lt.	1,858,775.448	995,668.004
508+71.10	33.10' Lt.	1,858,608.316	995,573.871

STATE OF ILLINOIS }  
COUNTY OF LAKE } SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 14, TOWNSHIP 38N., RANGE 8E., OF THE THIRD PRINCIPAL MERIDIAN, KANE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS \_\_\_\_\_ DAY OF \_\_\_\_\_ 20\_\_ A.D.

\_\_\_\_\_  
PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2014  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
Note: Surface Coordinates are Shown

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
115+66.74	51.34' Rt.	1,858,514.780	995,756.820
115+67.01	34.34' Rt.	1,858,514.595	995,739.821
115+97.77	30.95' Lt.	1,858,543.611	995,673.730
116+63.76	29.41' Lt.	1,858,609.614	995,673.523
116+63.82	34.41' Lt.	1,858,609.549	995,668.523

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

**PLAT OF HIGHWAYS**  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
OHIO STREET

SECTION 08-00278-00-BR KANE COUNTY  
PROJECT JOB NO. R-55-001-97  
STATION 115+00 TO STATION 119+00  
STATION 508+00 TO STATION 510+00.61  
STATION 600+00.00 TO STATION 602+00  
SCALE: 1"=20' SHEET 7 OF 10

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
0012	Book 1 of Plats, Page 72	July 9, 1857
----	Book 1 of Plats, Page 72	July 9, 1857
----	153973	April 24, 1916
----	98K085063	September 18, 1998

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
0012.E.	Ismael Guido and Herminia Jimenez, and Silvia Jimenez, as joint tenants	0.380	N/A	N/A	0.380	0.019	Construction Purposes	15-14-377-026	

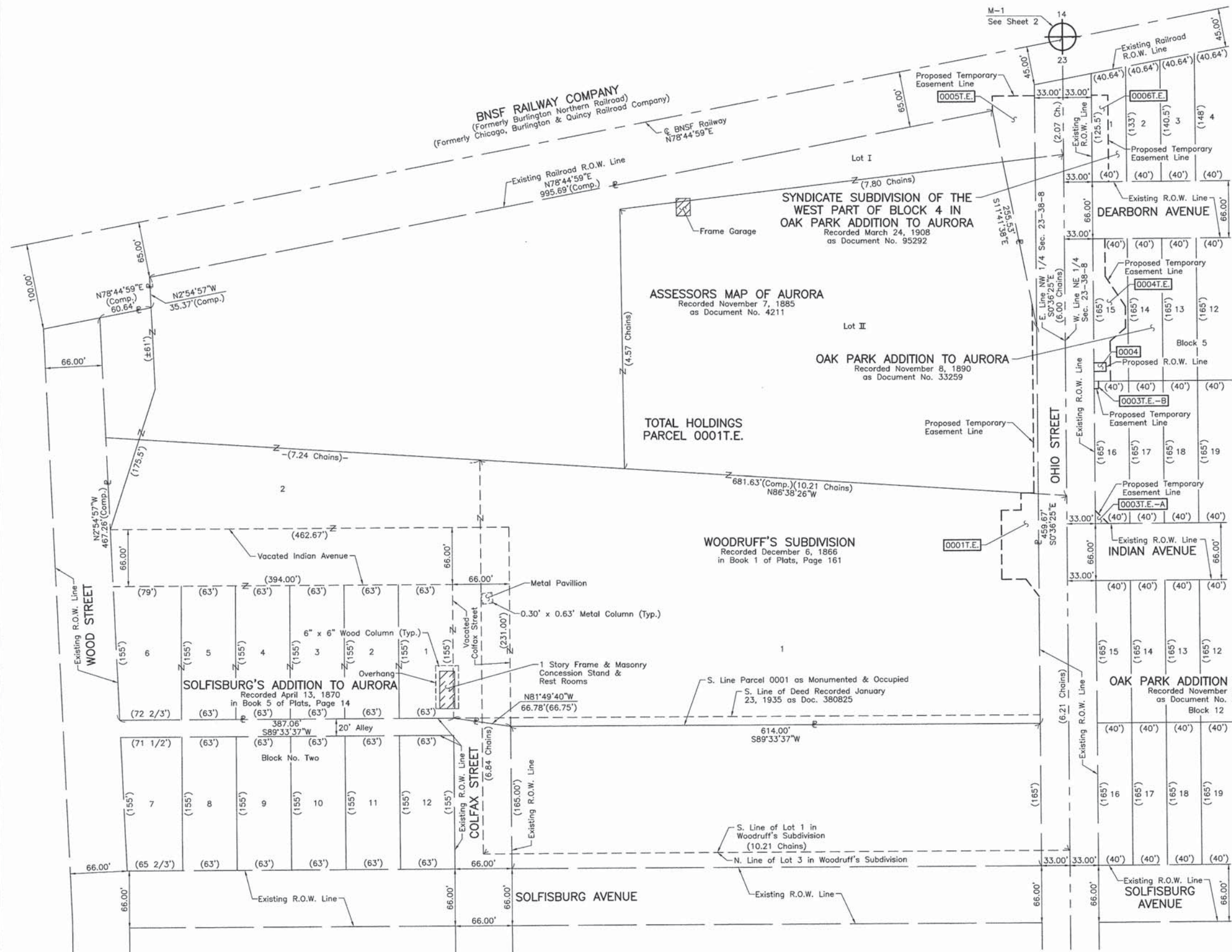
REVISION DATE November 1, 2013

REVISION Eliminated Parcel 0011

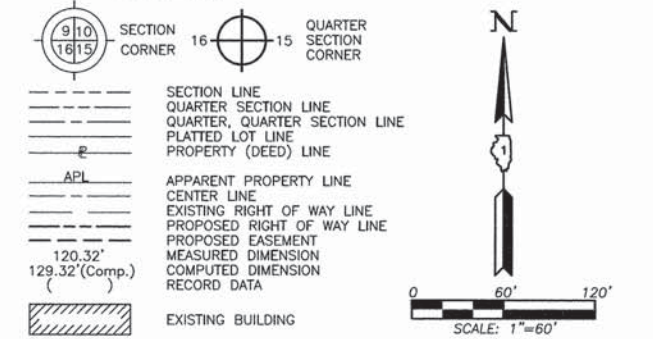
MADE BY



PART OF THE NORTH 1/2 OF SEC. 23, TWP. 38 N., R. 8 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.



**LEGEND**



- Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2011) East Zone.
- IRON PIPE OR ROD FOUND      ⊕ "MAG" NAIL SET
  - + CUT CROSS FOUND OR SET      ● 5/8" REBAR SET
  - T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
  - M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - ⊕ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
  - RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
 COUNTY OF LAKE } SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 23, TOWNSHIP 38N., RANGE 8E., OF THE THIRD PRINCIPAL MERIDIAN, KANE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS \_\_\_\_\_ DAY OF \_\_\_\_\_ 20\_\_\_\_ A.D.

\_\_\_\_\_  
 PRESIDENT  
 ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2014  
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

JORGENSEN & ASSOCIATES, INC.  
 120 PARK AVENUE  
 LAKE VILLA, ILLINOIS 60046 SHEET 1 IS A COVER  
 (847) 356-3371 SHEET AND IS NOT RECORDED.

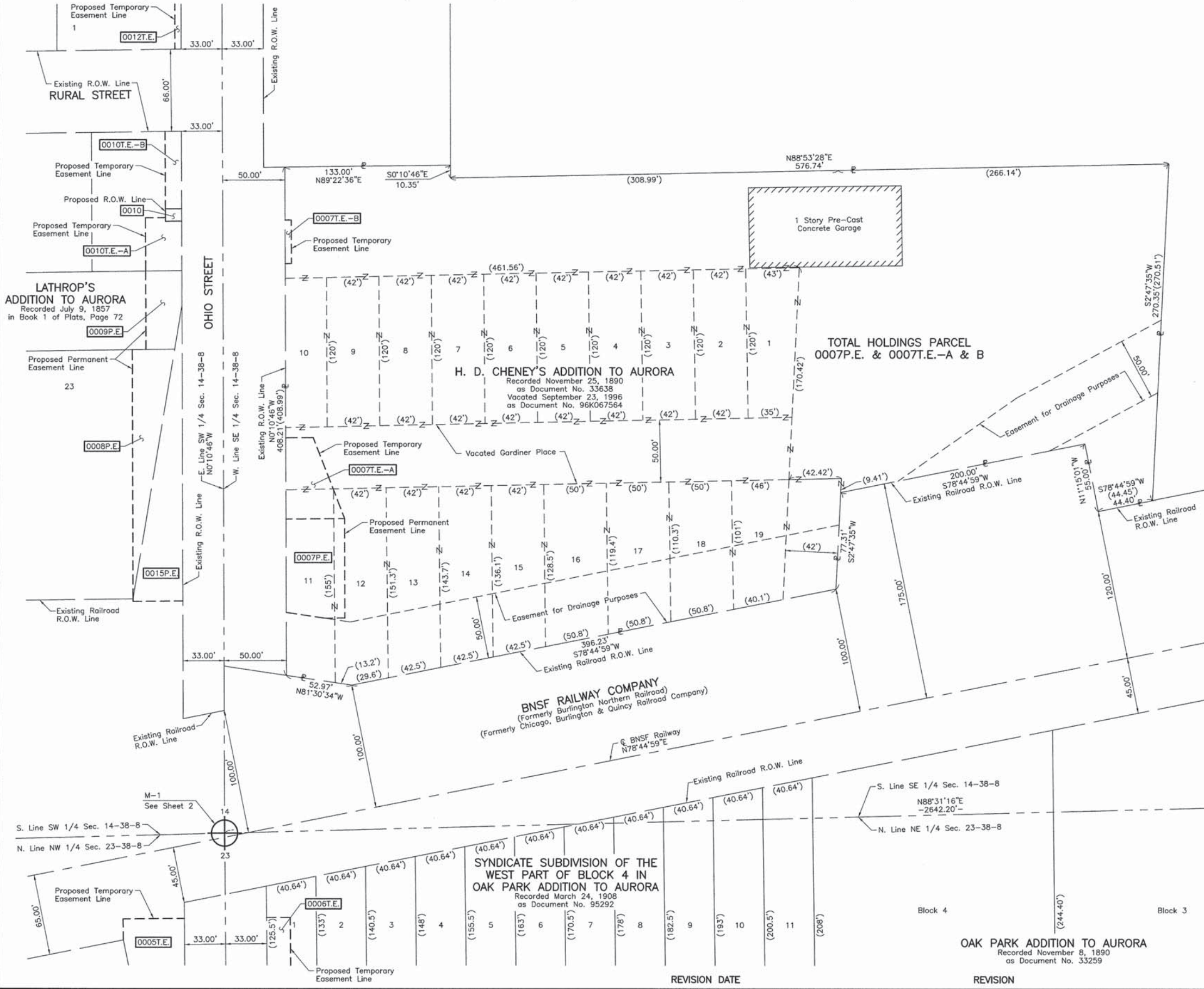
**PLAT OF HIGHWAYS**  
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
 OHIO STREET  
 SECTION 08-00278-00-BR KANE COUNTY  
 PROJECT JOB NO. R-55-001-97  
 STATION NONE TO STATION  
 SCALE: 1"=60' SHEET 8 OF 10

**BUREAU OF LAND ACQUISITION**  
 201 WEST CENTER COURT  
 SCHAUMBURG, ILLINOIS 60196

REVISION DATE REVISION MADE BY



PART OF THE SOUTH 1/2 OF SEC. 14 AND PART OF THE NORTH 1/2 OF SEC. 23, TWP. 38 N., R. 8 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.



**LEGEND**

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- CENTER LINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORD DATA
- EXISTING BUILDING

- Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2011) East Zone.
- IRON PIPE OR ROD FOUND
  - CUT CROSS FOUND OR SET
  - THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
  - STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
  - RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
 COUNTY OF LAKE } SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 14, TOWNSHIP 38N., RANGE 8E. AND SECTION 23, TOWNSHIP 38N., RANGE 8E., OF THE THIRD PRINCIPAL MERIDIAN, KANE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS \_\_\_\_\_ DAY OF \_\_\_\_\_ 20\_\_ A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2014  
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

JORGENSEN & ASSOCIATES, INC.  
 120 PARK AVENUE  
 LAKE VILLA, ILLINOIS 60046  
 (847) 356-3371

SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

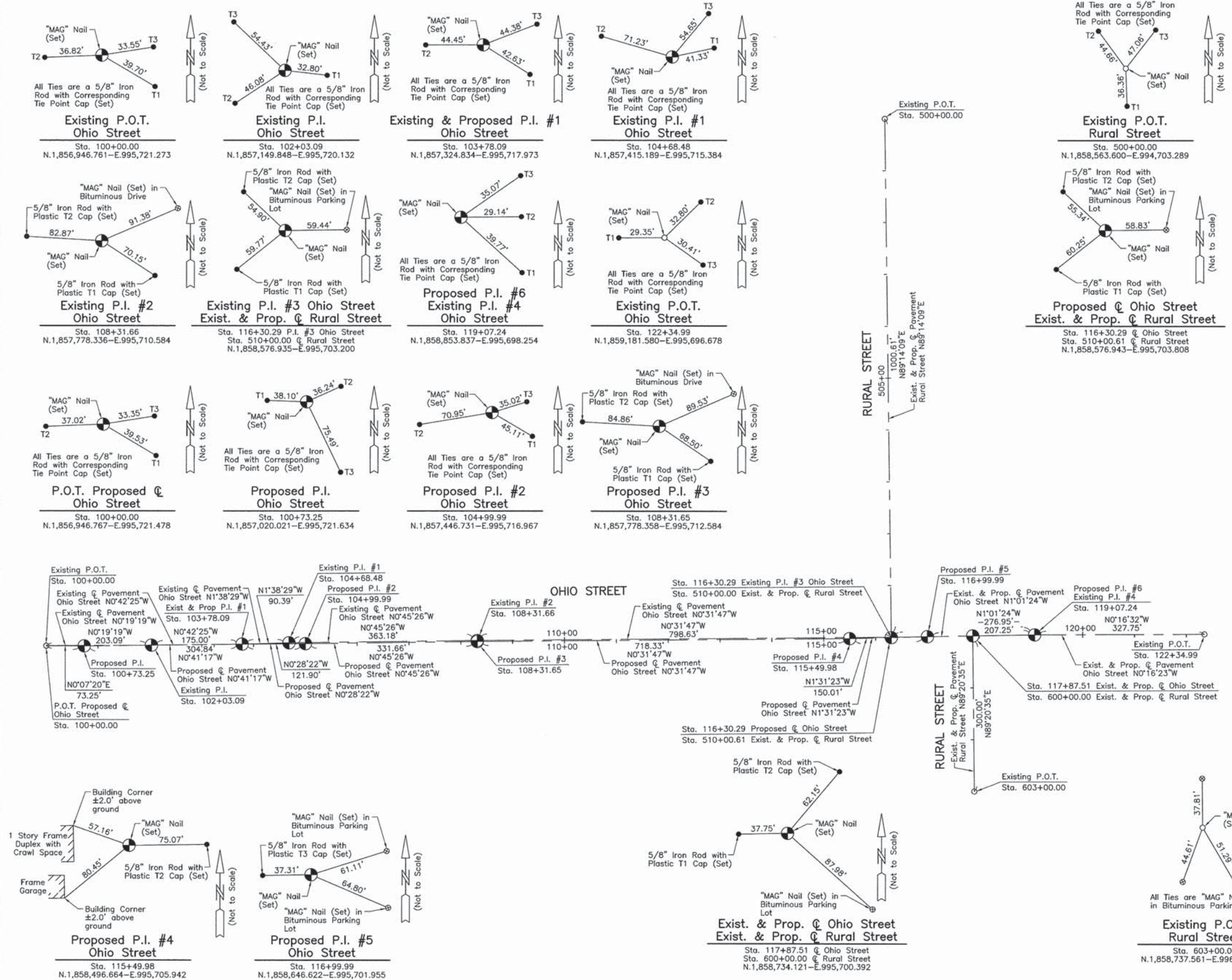
**PLAT OF HIGHWAYS**  
 STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 OHIO STREET

SECTION 08-00278-00-BR KANE COUNTY  
 PROJECT JOB NO. R-55-001-97  
 STATION NONE TO STATION  
 SCALE: 1"=40' SHEET 9 OF 10

BUREAU OF LAND ACQUISITION  
 201 WEST CENTER COURT  
 SCHAUMBURG, ILLINOIS 60196



PART OF THE SOUTH 1/2 OF SEC. 14 AND PART OF THE NORTH 1/2 OF SEC. 23, TWP. 38 N., R. 8 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.



### LEGEND

SECTION CORNER 16 15  
QUARTER SECTION CORNER

SECTION LINE  
QUARTER SECTION LINE  
QUARTER, QUARTER SECTION LINE  
PLATTED LOT LINE  
PROPERTY (DEED) LINE

APL APPARENT PROPERTY LINE  
CENTER LINE  
EXISTING RIGHT OF WAY LINE  
PROPOSED RIGHT OF WAY LINE  
PROPOSED EASEMENT  
MEASURED DIMENSION  
COMPUTED DIMENSION  
RECORD DATA

120.32'  
129.32'(Comp.)

EXISTING BUILDING

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2011) East Zone.

○ IRON PIPE OR ROD FOUND      ⊕ "MAG" NAIL SET  
+ CUT CROSS FOUND OR SET      ● 5/8" REBAR SET

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● T2  
● T3  
● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● BT2  
● BT3

■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

⊕ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)

□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

SCALE: 1"=100'

STATE OF ILLINOIS }  
COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 14, TOWNSHIP 38N., RANGE 8E. AND SECTION 23, TOWNSHIP 38N., RANGE 8E., OF THE THIRD PRINCIPAL MERIDIAN, KANE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_ A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2014  
Note: Surface Coordinates are Shown

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

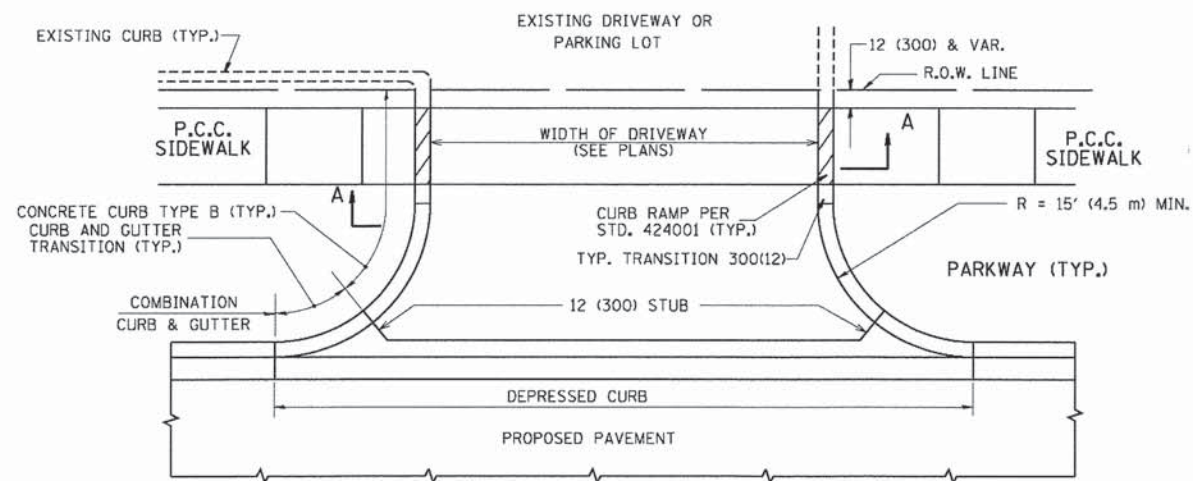
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SHEET AND IS NOT RECORDED.

### PLAT OF HIGHWAYS

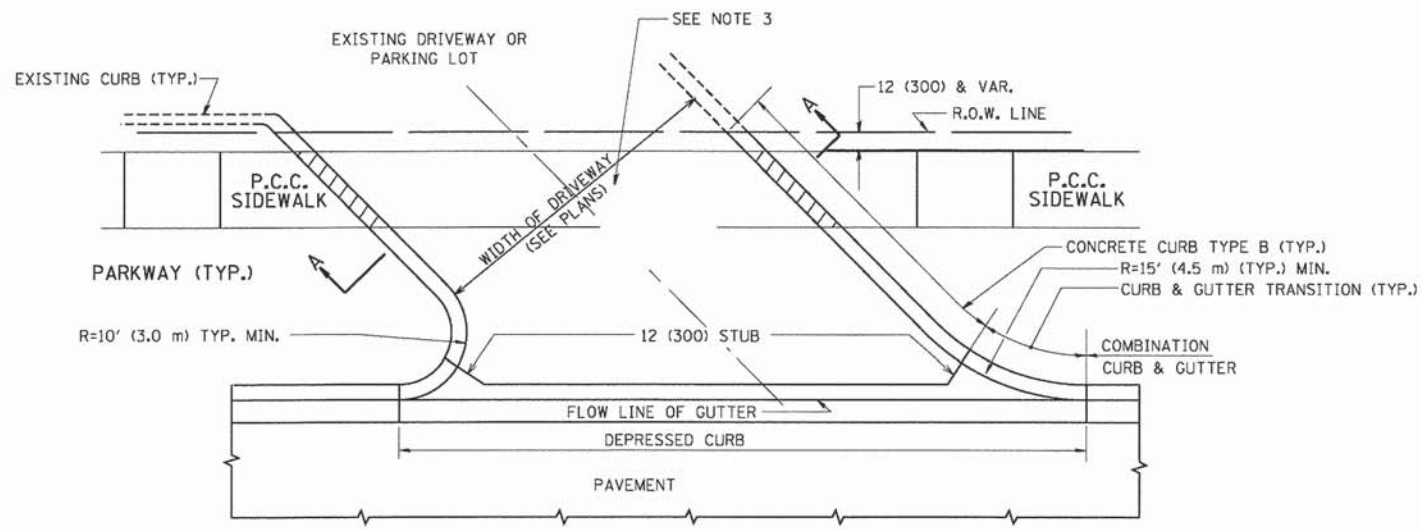
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
OHIO STREET

SECTION 08-00278-00-BR KANE COUNTY  
PROJECT JOB NO. R-55-001-97  
STATION 100+00.00 TO STATION 122+34.99  
STATION 500+00.00 TO STATION 510+00.61  
STATION 600+00.00 TO STATION 603+00.00  
SCALE: 1"=100' SHEET 10 OF 10

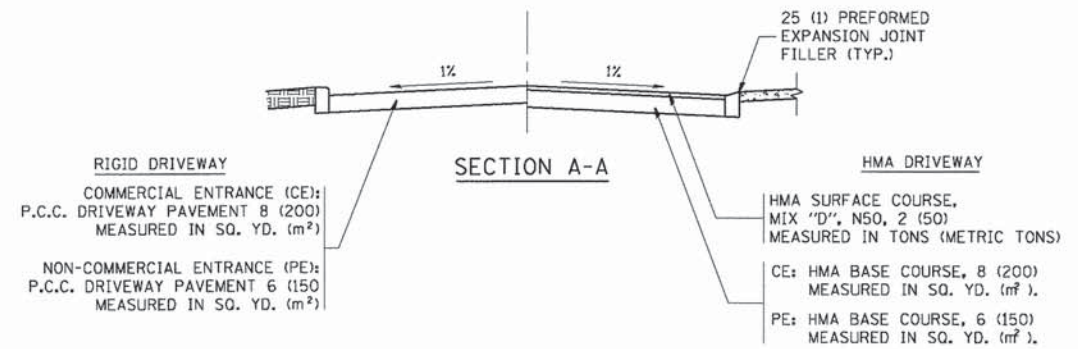




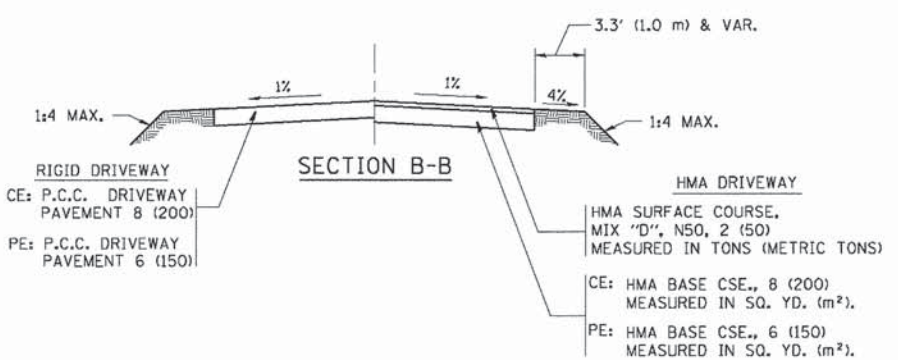
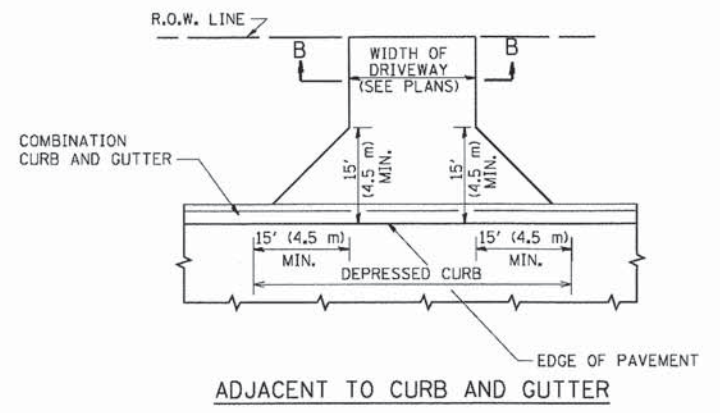
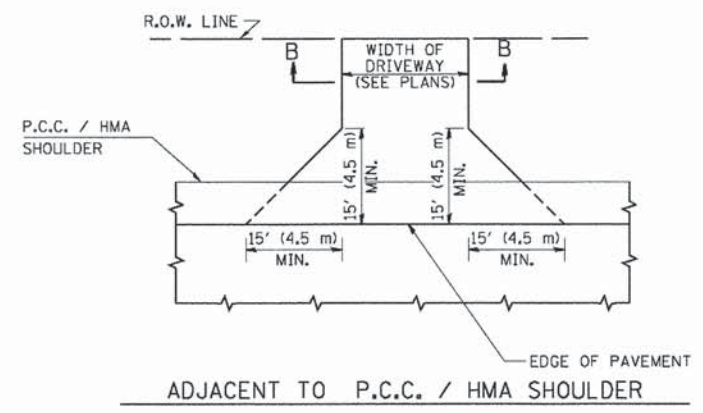
WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



SECTION A-A



SECTION B-B

RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m<sup>2</sup>).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

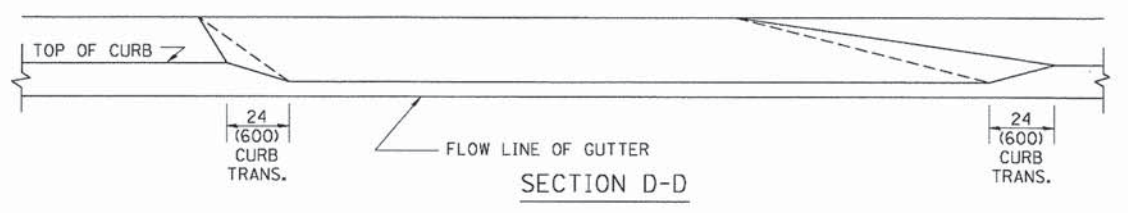
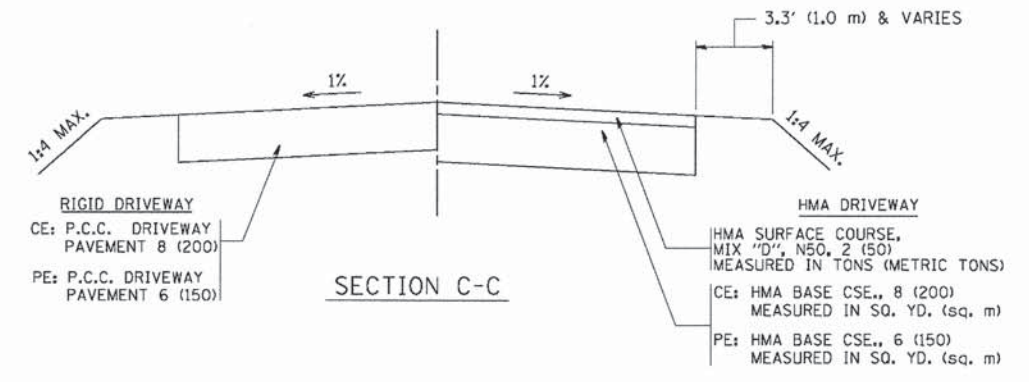
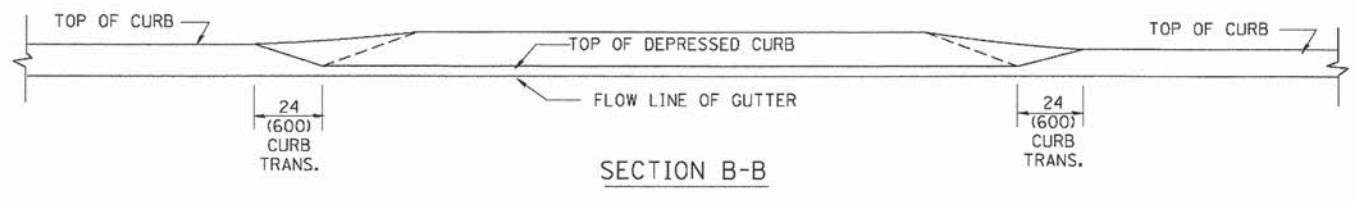
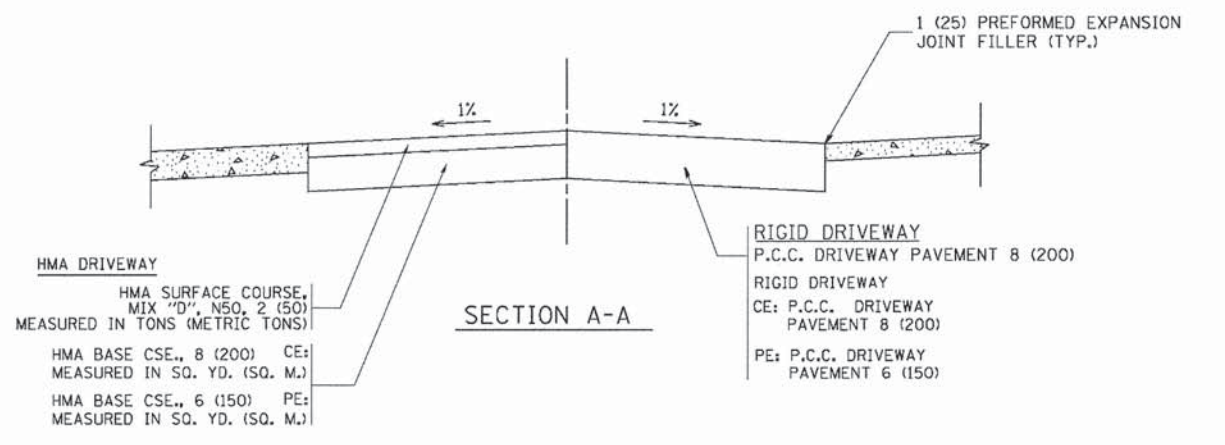
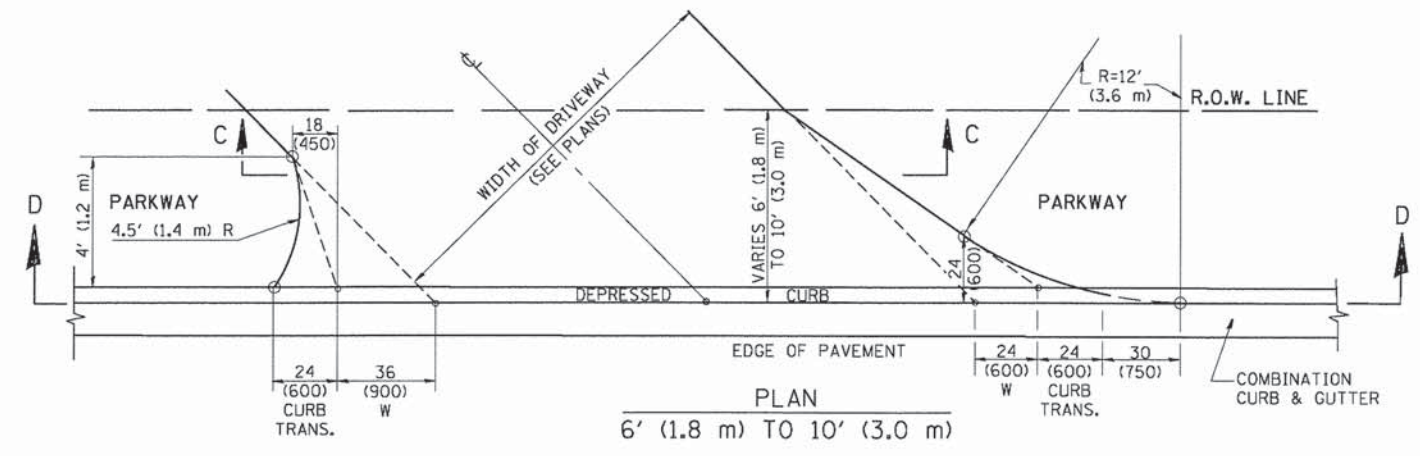
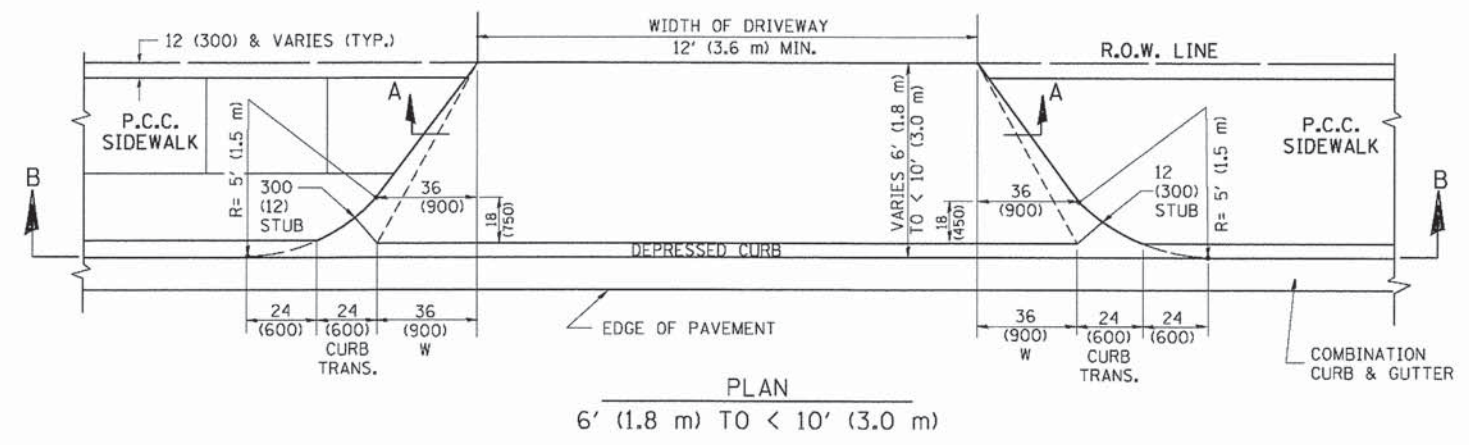
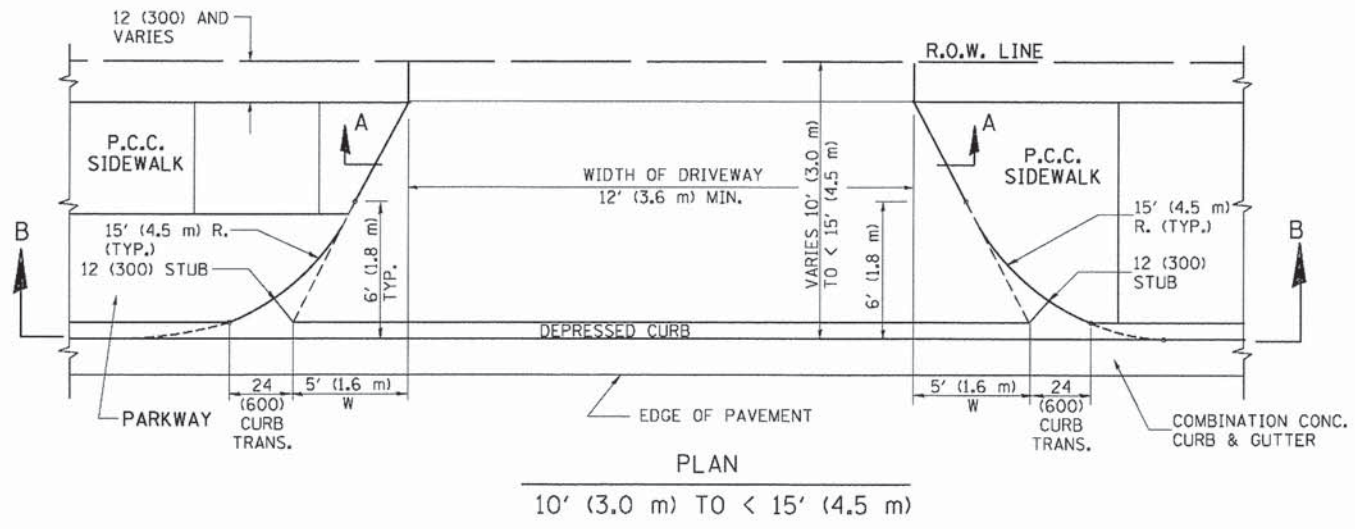
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		PLOT SCALE = 50,000' / 1" =	REVISED - R. BORO 06-11-08
		PLOT DATE = 9/6/2011	REVISED - R. BORO 09-06-11
		DATE - 11-04-95	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	64
BD0156-07 (BD-01)			CONTRACT NO. 63859	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**GENERAL NOTES**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS: SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

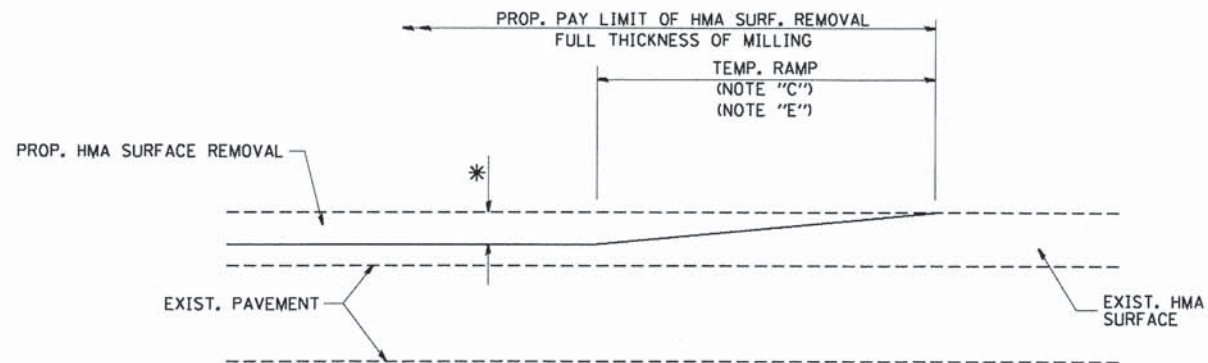
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PLOT SCALE = 50.0000' / 1"		CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 10/28/2011		DATE - 11-06-95	REVISED - R. BORO 09-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS			
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

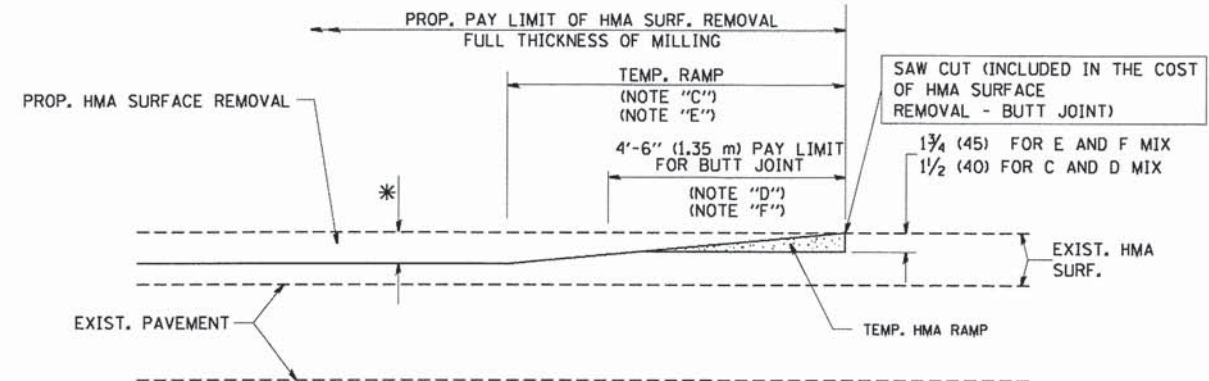
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	65
BD400-02 (BD-02)			CONTRACT NO. 63859	
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				





MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

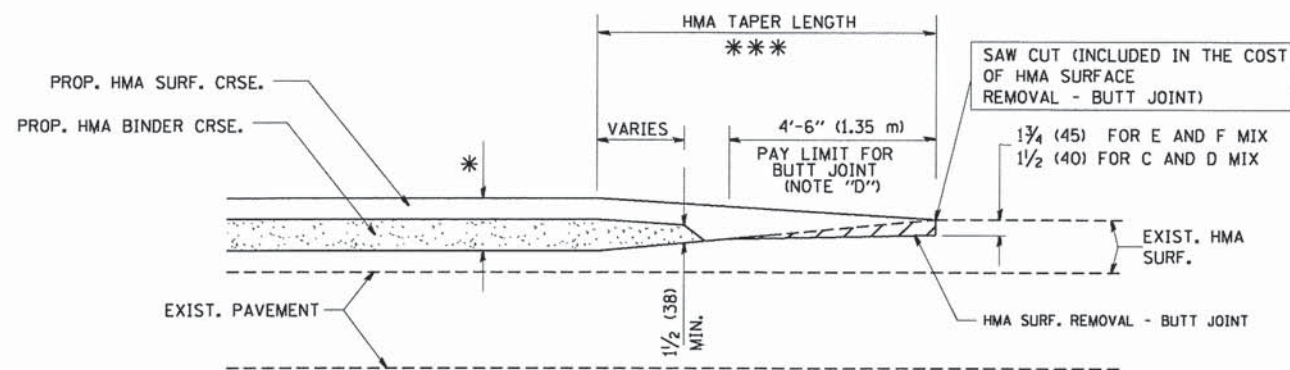
**OPTION 1**



HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

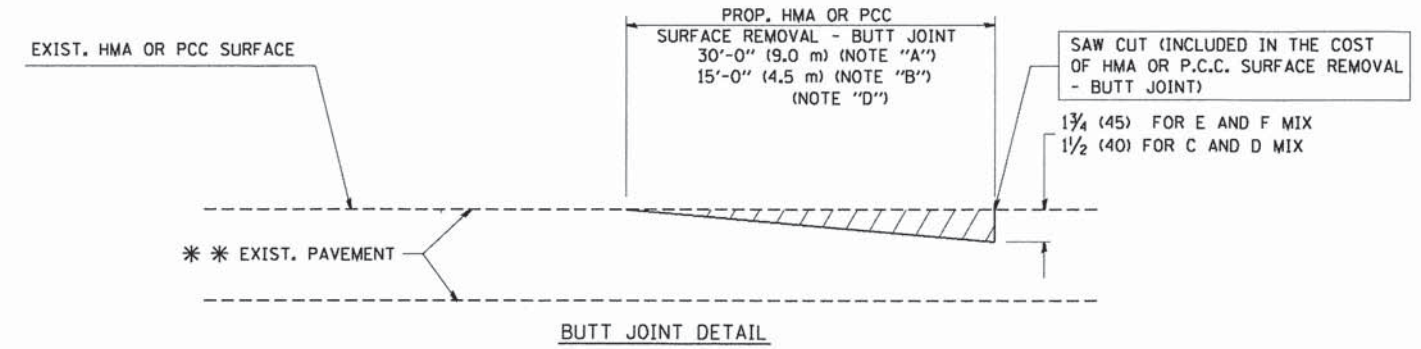
**OPTION 2**

**TYPICAL TEMPORARY RAMP**

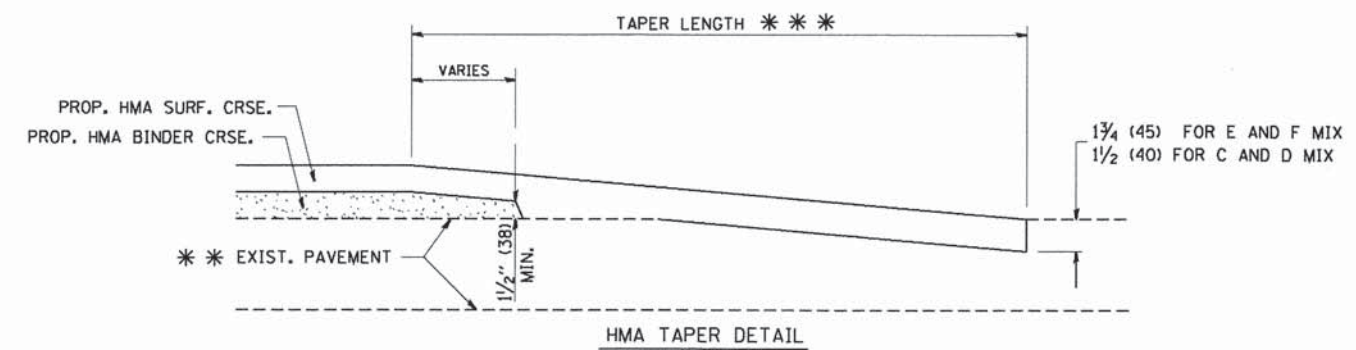


BUTT JOINT AND  
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY**

\* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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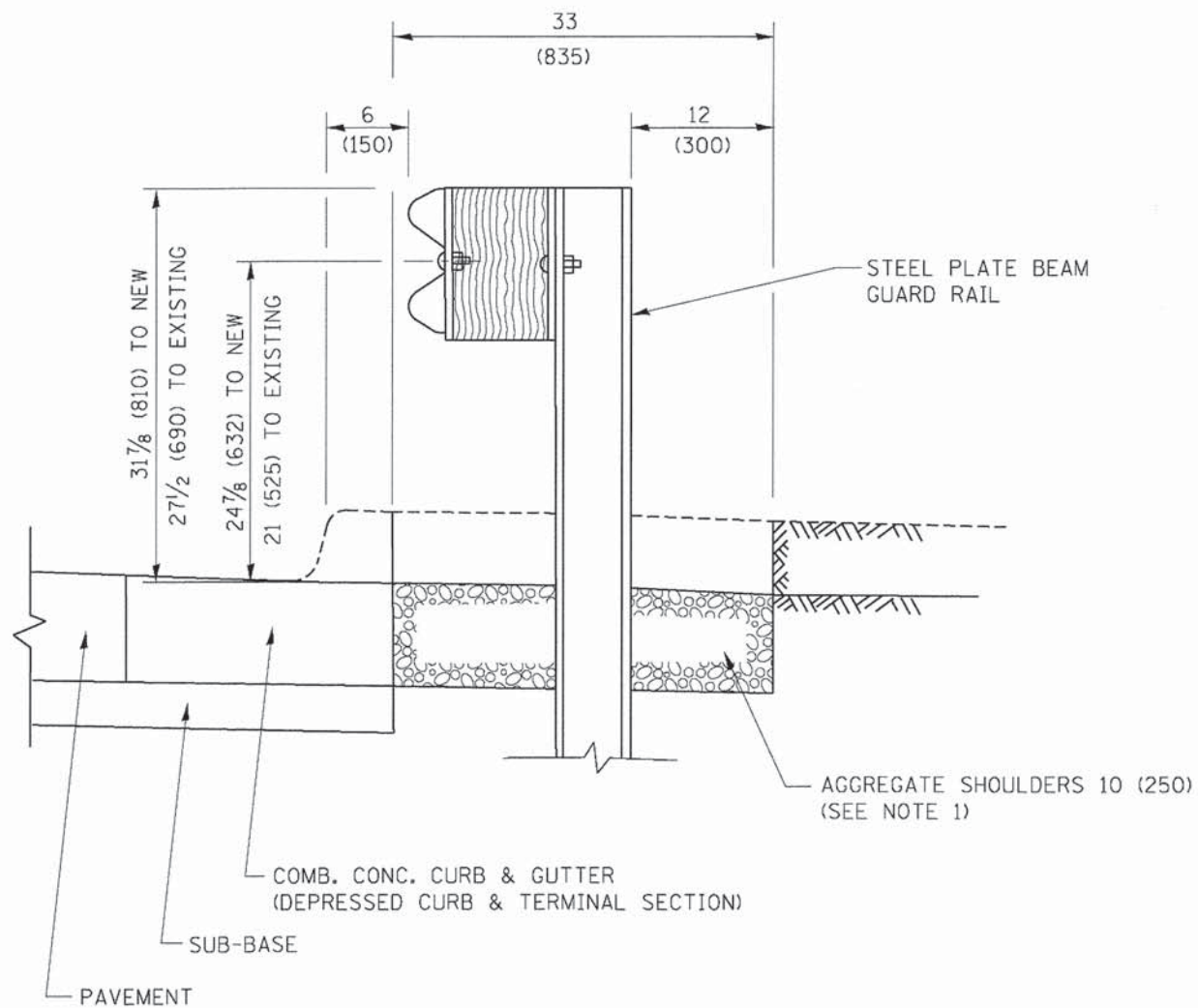
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DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 50.0000' / IN.	REVISED - M. GOMEZ 04-06-01
CHECKED -	REVISED - R. BORO 01-01-07
DATE - 06-13-90	

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	66
BD400-05 BD32			CONTRACT NO. 63859	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

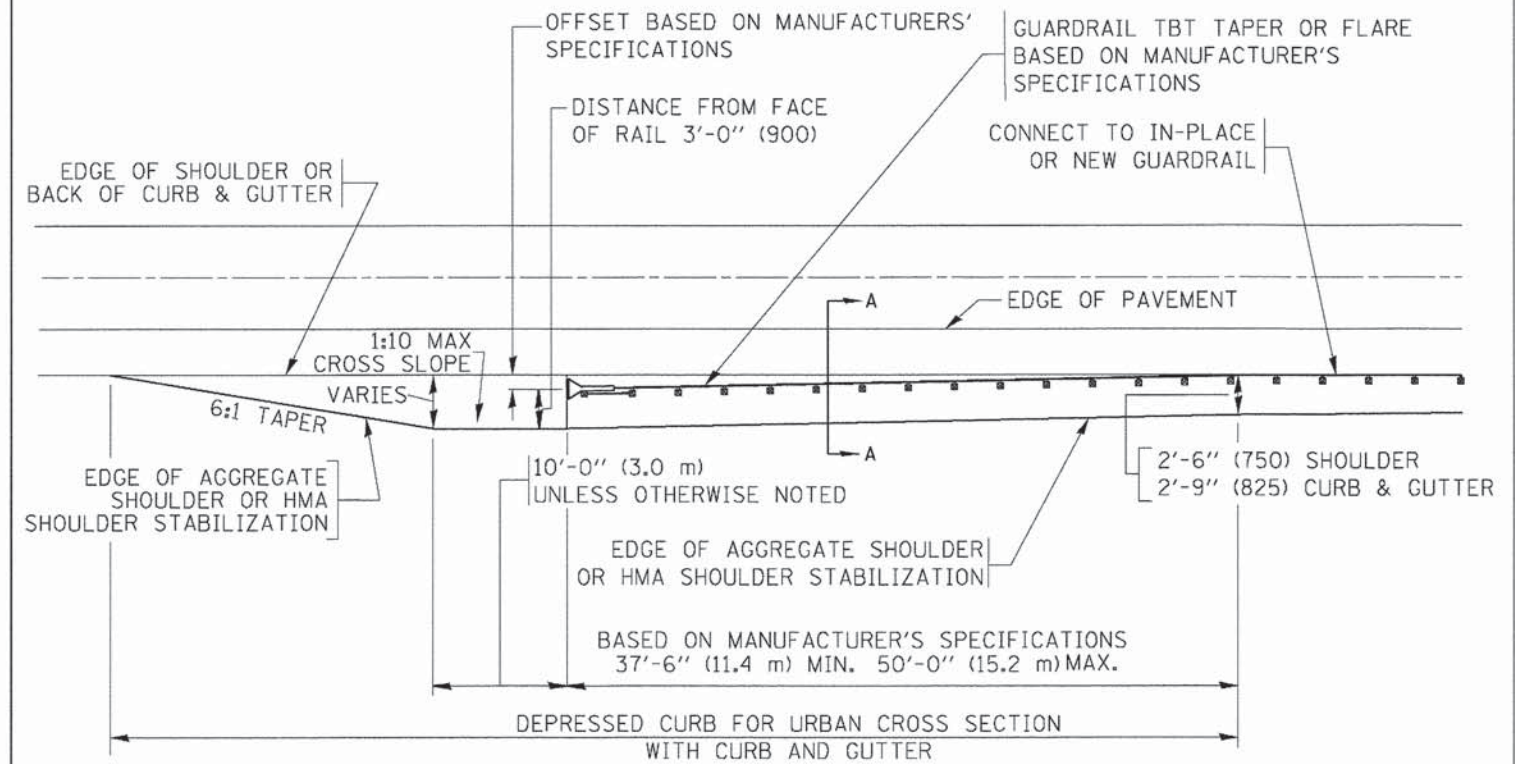




SECTION A-A

- NOTES:
1. THE AGGREGATE SHOULDER, 10 (250) OR HMA SHOULDER, 6 (150) (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
  2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
  3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

**DETAILS FOR STEEL PLATE BEAM  
GUARD RAIL ADJACENT TO CURB AND GUTTER  
[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]**



**DEPRESSED CURB AND GUTTER AND  
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

AGGREGATE SHOULDER, 10 (250) WILL BE PAID ACCORDING TO SECTION 481.

HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID ACCORDING TO SECTION 482.

COMB. CONC. C&G, STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL  
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivekosgn	DESIGNED - M. DE YONG	REVISED - R. BORO 01-01-07
ct\pw_work\p\dot\drivekosgn\08108315\bd64.dgn		DRAWN -	REVISED - R. BORO 12-08-2008
	PLOT SCALE = 50.0000' / 1in.	CHECKED -	REVISED - R. BORO 09-14-2009
	PLOT DATE = 9/4/2012	DATE - 09-22-90	REVISED - R. BORO 08-06-2012

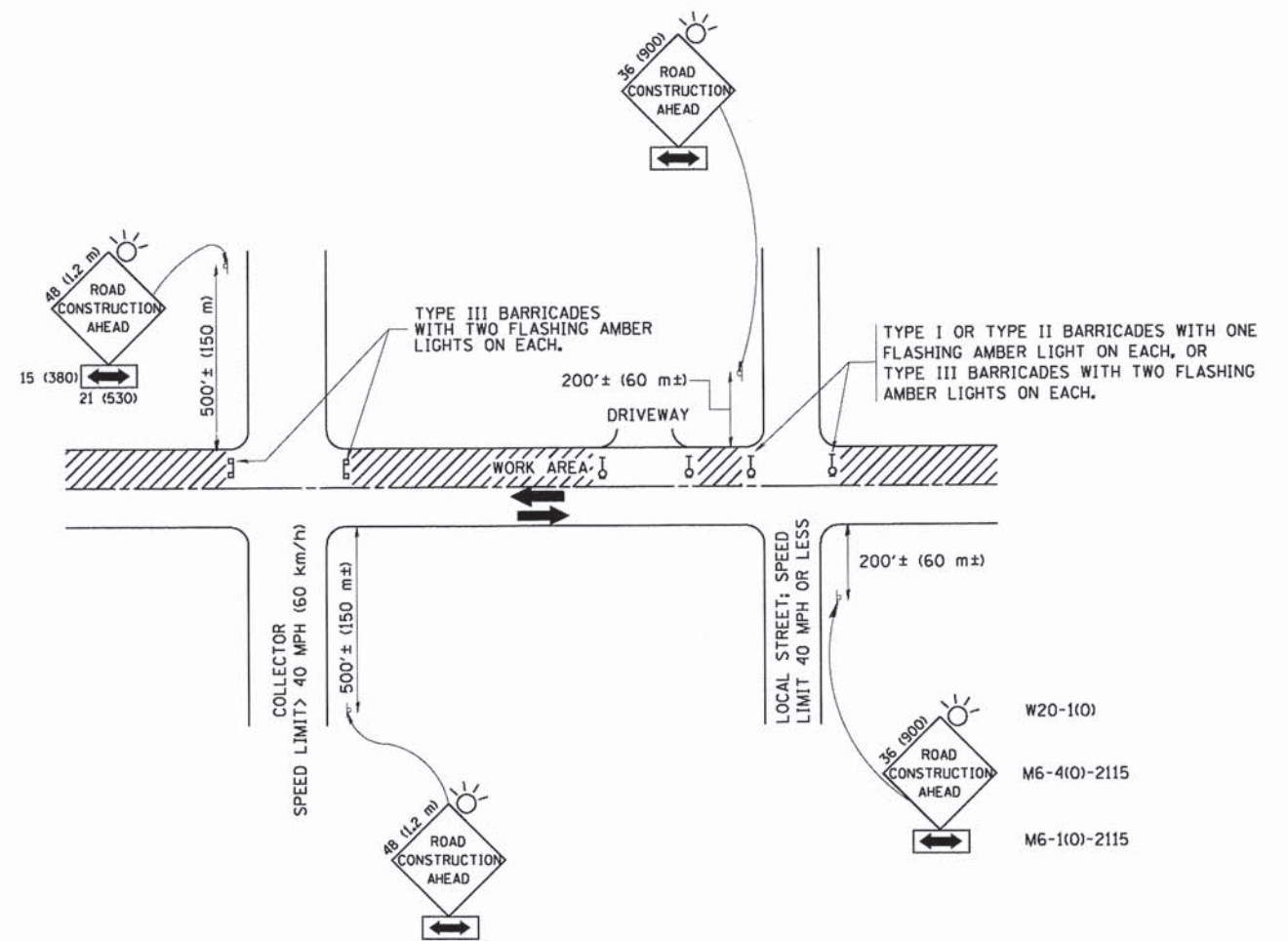
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR DEPRESSED CURB & GUTTER AND  
SHOULDER TREATMENT AT TBT TY 1 SPL.**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	67
<b>BD600-10 (BD 34)</b>		CONTRACT NO. 63859		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
  - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\datastd\22x34\to10.dgn	USER NAME = ggliemobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 1/4" = 10'-0"	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2000	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

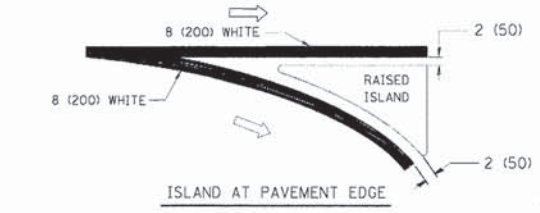
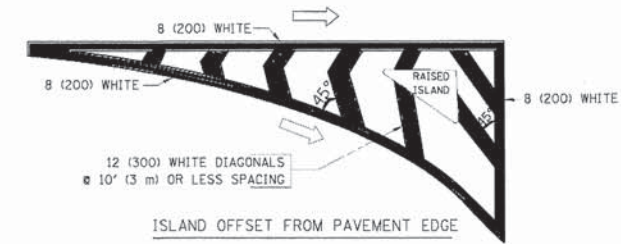
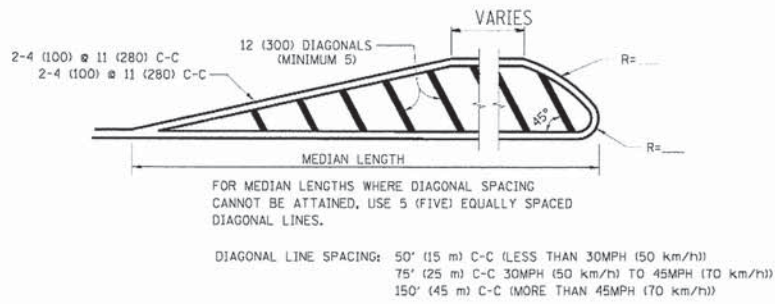
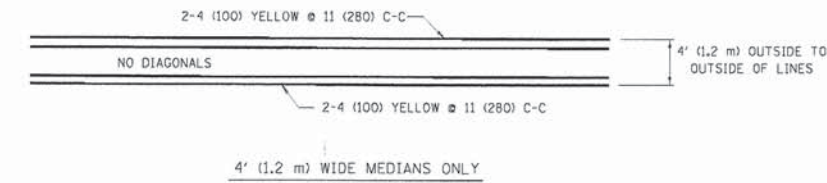
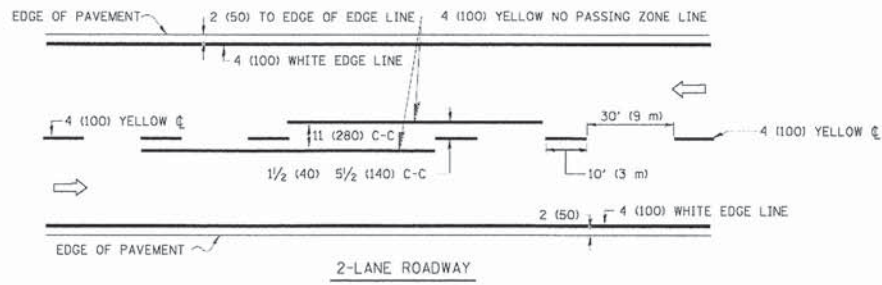
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

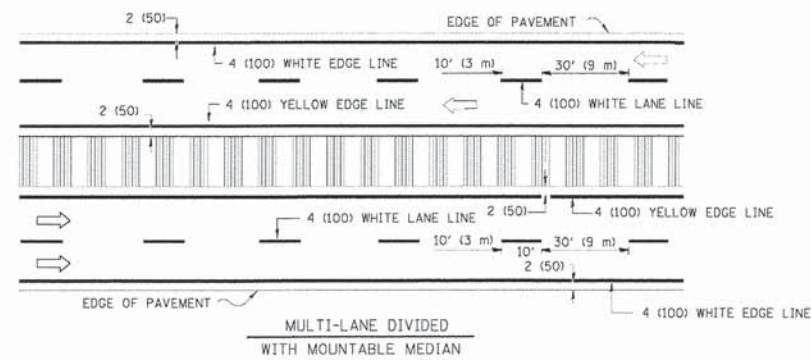
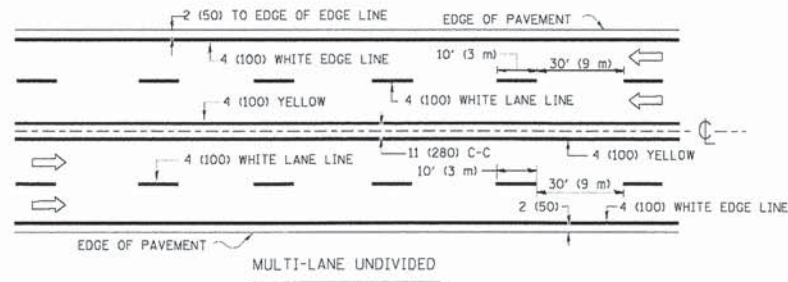
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	68
TC-10			CONTRACT NO. 63859	
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				



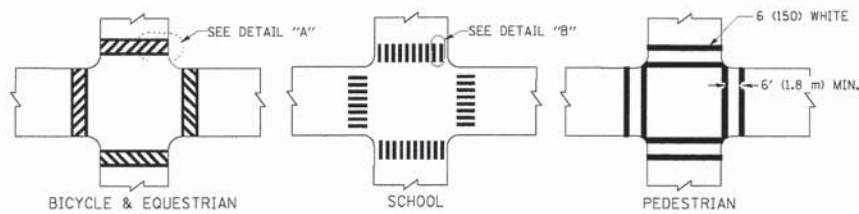


TYPICAL ISLAND MARKING

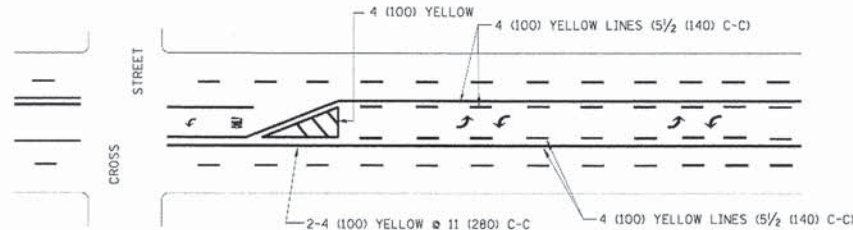


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

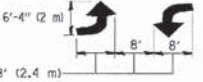
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

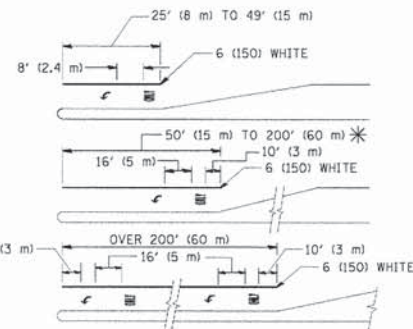


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
dr:\p\work\pav\dat\drivakosgn\2128319\to	3.dgn	DRAWN -	REVISED -C. JUCIUS 09-09-09
PLOT SCALE = 26.000 1/1 IN.	CHECKED -	REVISOR -	
PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		4087	08-00278-00-BR	KANE	136	69
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. 63859		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT						





3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE  
 PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)  
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY  
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE  
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = W:\diststd\22x34\to26.dgn	USER NAME = geglienabt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
		DRAWN -	REVISED -
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY ENTRANCE SIGNING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	70
<b>TC-26</b>			<b>CONTRACT NO. 63859</b>	
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				



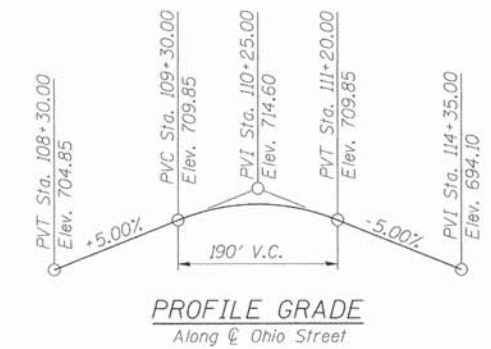
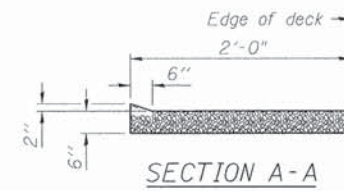
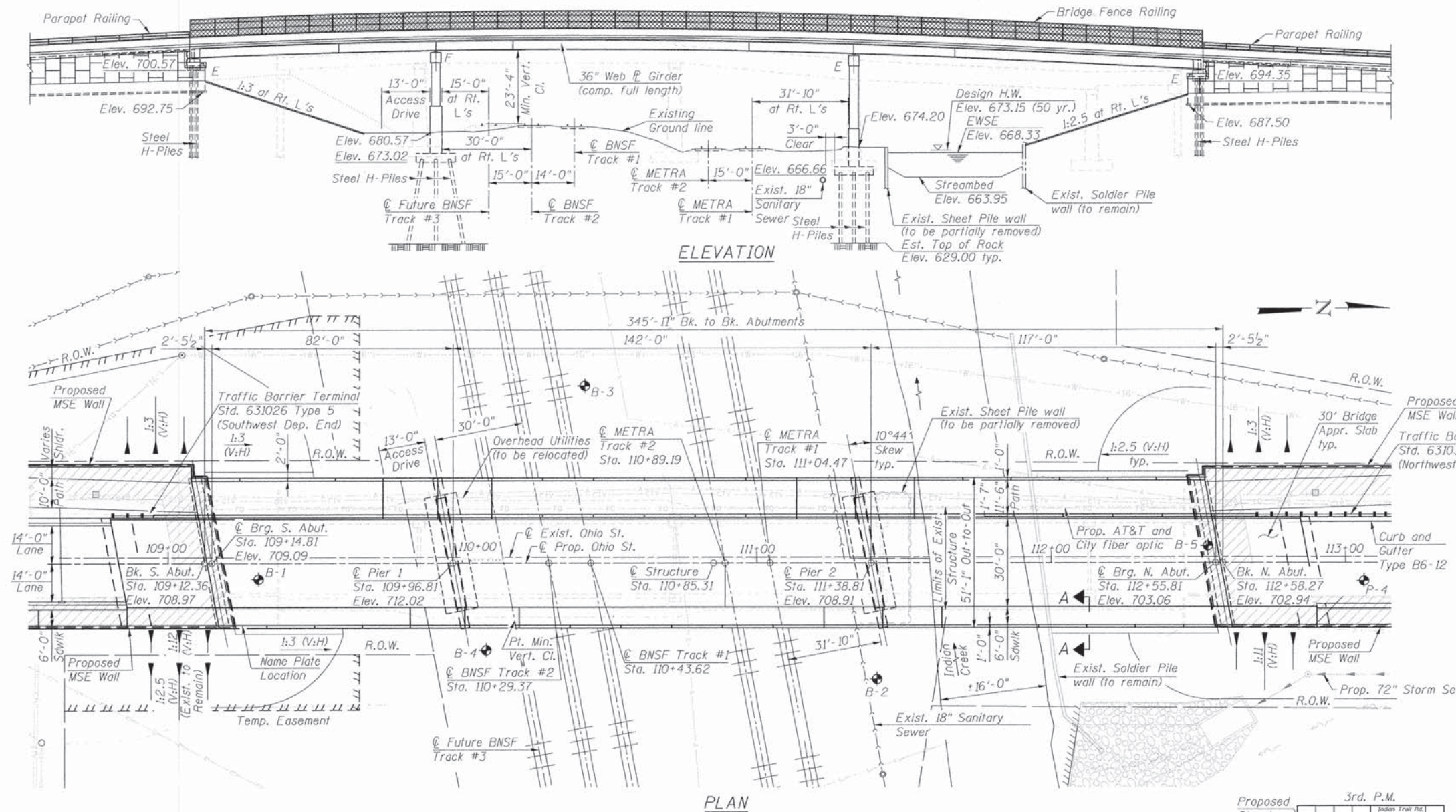




Bench Mark: 6" rebar with yellow cap west of Ohio Street bridge El. 673.30

Existing Structure: SN 045-9943. Constructed in 1923. Five-span bridge that measures 310'-4 1/2" back to back of abutments. Out-to-out width varies. Span 1 and Span 3 thru 5 have an out-to-out width of 35'-0". Span 2 has an out-to-out width of 39'-8". Span 1 is supported by two 48" deep steel girders and floorbeam system. Span 2 is supported by two 96" steel through girders with no floorbeam system. Span 3 and Span 4 are supported by two 78" steel girders and a floorbeam system. Span 5 is supported by six rolled steel beams. Substructure is pile bent abutments and concrete piers on spread footings. The existing bridge is to be removed and replaced. The road will be closed and traffic detoured during construction.

No salvage.



Matthew D. Santeford, P.E., S.E.  
 No. 081-007244  
 Exp. Date 11/30/2014

"I certify that to the best of my knowledge, information and belief, this bridge design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with the requirements of the current 'AASHTO LRFD Bridge Design Specifications'."

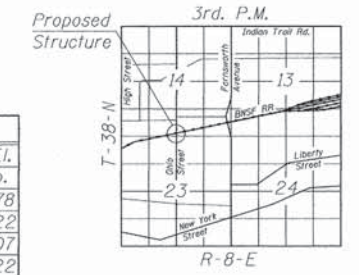
**LOADING HL-93**  
 Allow 50#/sq. ft. for future wearing surface.

**DESIGN SPECIFICATIONS**  
 2010 AASHTO LRFD Bridge Design Specifications (5th Edition)  
 BNSF Railway - Union Pacific Railroad Guidelines for Railroad Grade Separation Projects

**DESIGN STRESSES**  
**FIELD UNITS**  
 f'c = 3,500 psi (Cast-in-Place)  
 fy = 60,000 psi (Reinforcement)  
 fy = 50,000 psi (M270 Grade 50)  
**PRECAST UNITS**  
 f'c = 4,500 psi (Precast Panels)

**SEISMIC DATA**  
 Seismic Performance Zone (SPZ) = 1  
 Design Spectral Acceleration at 1.0 sec. (S<sub>01</sub>) = 0.091g  
 Design Spectral Acceleration at 0.2 sec. (S<sub>05</sub>) = 0.164g  
 Soil Site Class = D

**GENERAL PLAN AND ELEVATION**  
**OHIO STREET OVER**  
**BNSF, METRA AND INDIAN CREEK**  
**SEC. 08-00278-00-BR**  
**KANE COUNTY**  
**STATION 110+85.31**  
**STRUCTURE NO. 045-9942**



BNSF, METRA AND INDIAN CREEK  
 BUILT 20\_\_ BY  
 CITY OF AURORA  
 SEC. 08-00278-00-BR  
 STA. 110+85.31  
 STR. NO. 045-9942 LOADING HL-93  
**NAME PLATE**  
 See Std. 515001

**DESIGN SCOUR ELEVATION TABLE**

Design Scour Elevation (ft.)	Pier 2 666.66	N. Abut. 687.50
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**WATERWAY INFORMATION**

Drainage Area = 16.2 mi<sup>2</sup> Low Grade Elev. 703.03 at Sta. 112+56.46

Flood Yr.	Freq. Q	C.F.S.	Opening Sq. Ft.		Nat. H.W.E.		Head - Ft.		Headwater El.	
			Exist.	Prop.	Exist.	Prop.	Exist.	Prop.	Exist.	Prop.
10	1061	199	202	202	670.76	0.17	0.02	670.87	670.78	
Design	50	2311	312	316	673.15	0.33	0.07	673.39	673.22	
Base	100	2962	356	362	673.98	0.42	0.09	674.30	674.07	
Max. Calc.	500	4331	435	443	675.36	1.23	0.86	676.46	676.22	

4/29/2014 PM - G:\Chil\0310\Bridges\CADD\Sheet\08-00278-00-01-01.dwg



USER NAME = jrmckow	DESIGNED - JRM	REVISED -
PLOT SCALE = 20x8.0000 1" = 10'	CHECKED - MDS	REVISED -
PLOT DATE = 5/12/2014	DRAWN - SFW	REVISED -
	CHECKED - MDS	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

SHEET NO. 1 OF 34 SHEETS

F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 72
ILLINOIS FED. AID PROJECT CONTRACT NO. 63859				



**GENERAL NOTES**

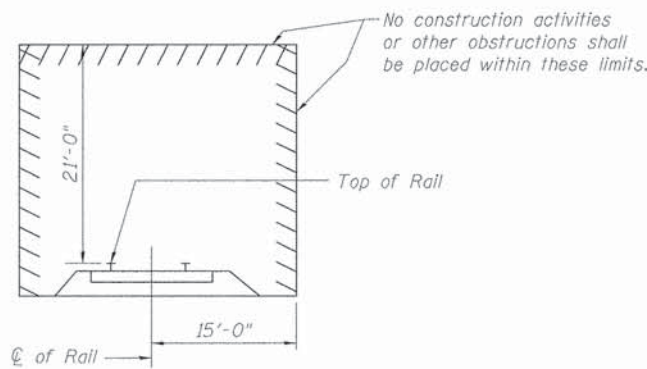
- Fasteners shall be ASTM A325 Type 1, mechanically galvanized bolts. Bolts 7/8 in.  $\phi$ , holes 15/16 in.  $\phi$ , unless otherwise noted.
- Calculated weight of Structural Steel = 774,600 pounds (AASHTO M 270 Grade 50)  
Calculated weight of Structural Steel = 46,420 pounds (AASHTO M 270 Grade 36)
- No field welding is permitted except as specified in the contract documents.
- Reinforcement bars designated (E) shall be epoxy coated.
- If the Contractor elects to use cantilever forming brackets on the exterior beams or girders, the brackets shall be placed at the same locations as required for the hardwood blocks in Article 503.06(b) of the Standard Specifications. If additional cantilever forming brackets are required, hardwood blocking shall be wedged between the exterior and first interior beam at each of these additional bracket locations.
- Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of 1/8 in. (0.01 ft). Adjustment shall be made either by grinding the surface or by shimming the bearings.
- The Organic Zinc Rich Primer / Epoxy / Urethane Paint System shall be used for painting of new structural steel except where otherwise noted. The entire system shall be shop applied, with the exception of the exterior surface and the bottom of the bottom flange of fascia beams, masked off connection surfaces, field installed fasteners and damaged areas shall be touched up in the field. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Gray, Munsell No. 5B 7/1.
- Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.
- Work windows for work on or over B.N.S.F. right of way shall be coordinated with the B.N.S.F.
- Falsework and demolition over B.N.S.F. right of way must be reviewed and approved by B.N.S.F. prior to the Contractor beginning each operation. Coordinate with B.N.S.F. for flagging during construction.
- B.N.S.F. review and approval of demolition, falsework and erosion control plans are required.
- All demolitions within the B.N.S.F.'s Right-of-Way and/or demolition that may impact the B.N.S.F.'s tracks or operations shall comply with the B.N.S.F.'s demolition guidelines.
- Falsework clearance shall comply with the B.N.S.F.'s minimum construction clearance envelope (see diagram on this sheet).
- For B.N.S.F. coordination please refer to the B.N.S.F. minimum requirements as part of the Special Provisions.
- The Contractor must submit a proposed method of erosion and sediment control and have the method approved by the B.N.S.F.

**TOTAL BILL OF MATERIAL**

Item	Unit	Super	Sub	Total
Removal of Existing Structures	Each			1
Structure Excavation	Cu. Yd.		464	464
Concrete Structures	Cu. Yd.		412.0	412.0
Concrete Superstructure	Cu. Yd.	923.8		923.8
Bridge Deck Grooving	Sq. Yd.	1348		1348
Protective Coat	Sq. Yd.	2636		2636
Furnishing and Erecting Structural Steel	L Sum	1		1
Stud Shear Connectors	Each	11,340		11,340
Reinforcement Bars, Epoxy Coated	Pound	193,170	58,910	252,080
Bar Splicers	Each	80		80
Bridge Fence Railing	Foot	689		689
Parapet Railing	Foot	435		435
Furnishing Steel Piles HP14x73	Foot		3028	3028
Driving Piles	Foot		3028	3028
Test Pile Steel HP14x73	Each		4	4
Name Plates	Each	1		1
Elastomeric Bearing Assembly, Type I	Each		10	10
Elastomeric Bearing Assembly, Type II	Each		10	10
Anchor Bolts, 5/8"	Each		20	20
Anchor Bolts, 3/4"	Each		40	40
Anchor Bolts, 1"	Each		20	20
Geocomposite Wall Drain	Sq. Yd.		81	81
Construction Vibration Monitoring	L Sum			0.5
High Load Multi-Rotational Bearings, Guided Expansion, 400 K	Each		10	10
Retaining Wall Removal	Foot		8	8
Bituminous Coated Aggregate Slopewall 6"	Sq. Yd.		730	730

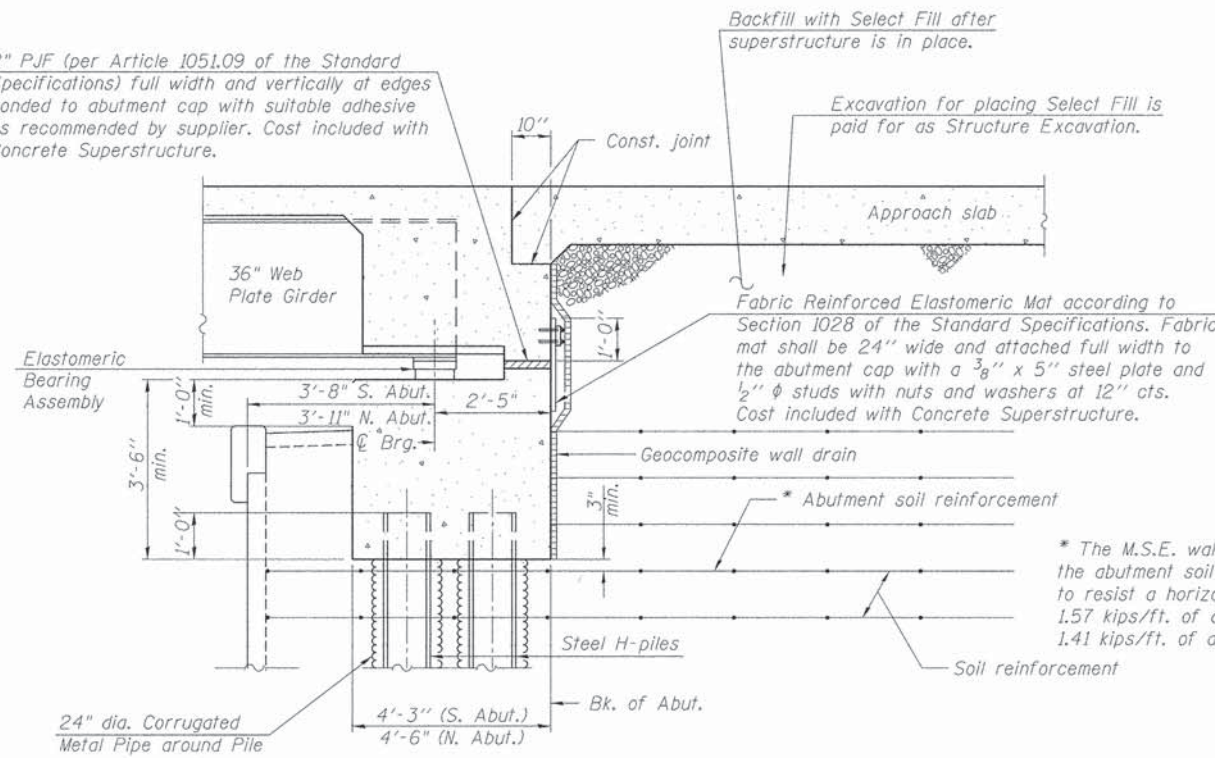
**INDEX OF SHEETS**

- General Plan and Elevation
- General Data
- Foundation Plan
- Top of Slab Elevations
- Top of South Approach Slab Elevations
- Top of North Approach Slab Elevations
- Deck Plan and Cross Section
- Superstructure Details
- Diaphragm Details
- Bridge Approach Slab Details
- Bridge Fence Railing, Parapet Mounted
- Parapet Railing Details
- Framing Plan
- Structural Steel Details
- Abutment Bearing Details
- Pier Bearing Details
- South Abutment
- North Abutment
- Pier 1
- Pier 2
- HP Pile Details
- Bar Splicer Assembly And Mechanical Splicer Details
- Boring Logs

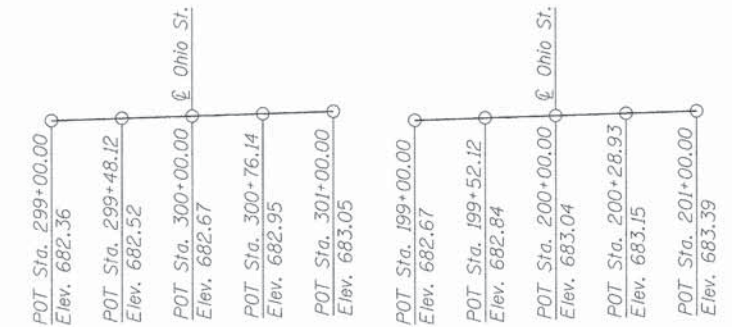


**MINIMUM CONSTRUCTION CLEARANCES**  
(Normal to railroad)  
Not to scale

2" PJF (per Article 1051.09 of the Standard Specifications) full width and vertically at edges bonded to abutment cap with suitable adhesive as recommended by supplier. Cost included with Concrete Superstructure.

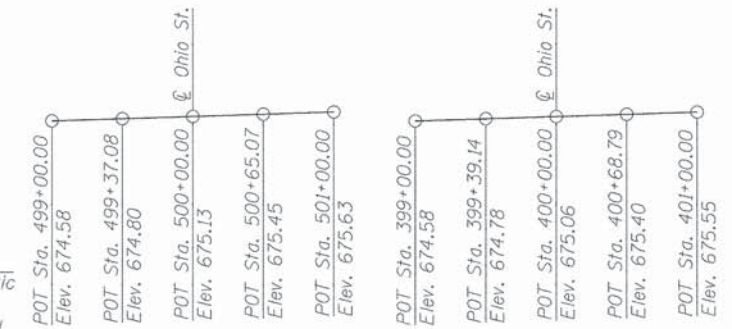


**SECTION THRU SEMI-INTEGRAL ABUTMENT**  
(Horiz. dim. @ Rt. L's)



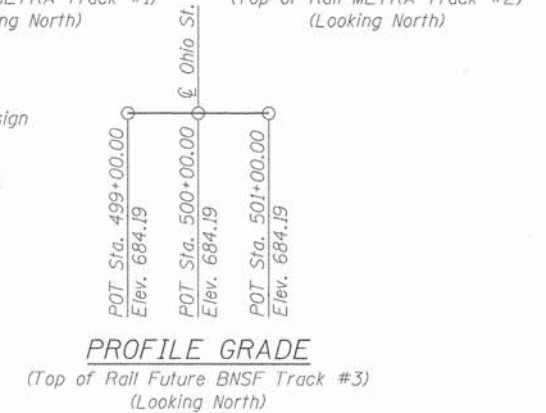
**PROFILE GRADE**  
(Top of Rail BNSF Track #1)  
(Looking North)

**PROFILE GRADE**  
(Top of Rail BNSF Track #2)  
(Looking North)



**PROFILE GRADE**  
(Top of Rail METRA Track #1)  
(Looking North)

**PROFILE GRADE**  
(Top of Rail METRA Track #2)  
(Looking North)



**PROFILE GRADE**  
(Top of Rail Future BNSF Track #3)  
(Looking North)

7/23/2014 9:07:38 AM - G:\CH\030\Bridges\CADD\Sheets\0459942-63859-002-C0A1A.dgn



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PLDT DATE = 7/23/2014	DRAWN - SFW	REVISED -
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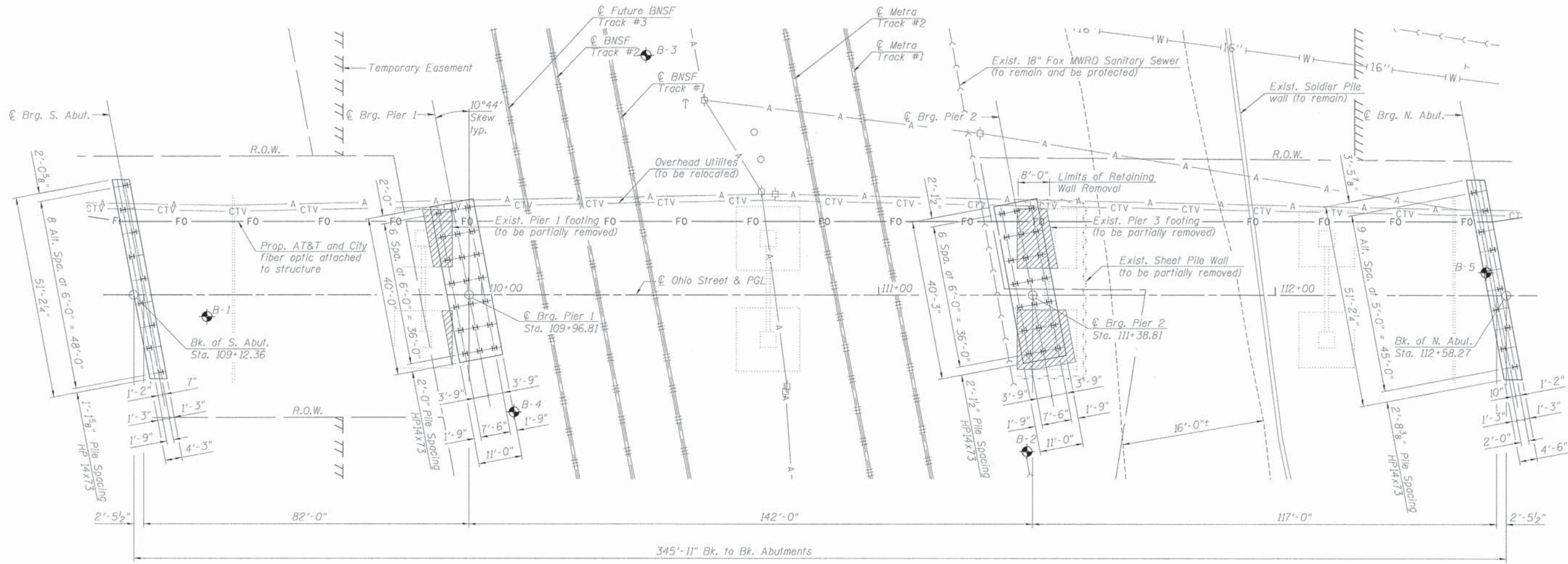
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL DATA  
STRUCTURE NO. 045-9942**

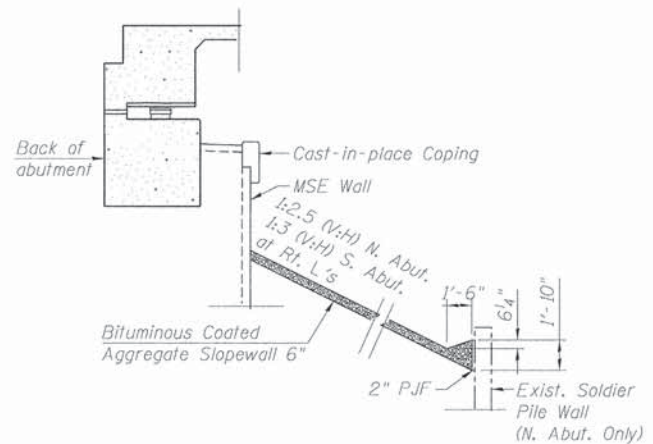
SHEET NO. 2 OF 34 SHEETS

F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 73
CONTRACT NO. 63859				
ILLINOIS FED. AID PROJECT				

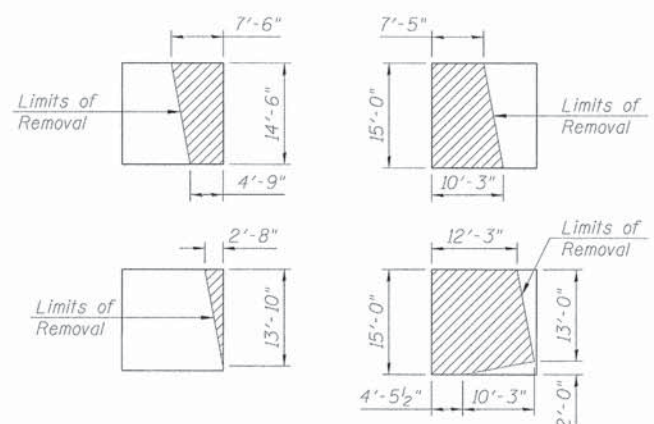




**FOUNDATION PLAN**



**SECTION THRU SLOPEWALL**



**EXIST. PIER 1 FOUNDATION PLAN**

**EXIST. PIER 3 FOUNDATION PLAN**

**LEGEND**



**Note:**  
 Removal of existing structures shall be in accordance with Section 501 of the Standard Specifications. This item shall include complete removal of the concrete bridge rails, concrete deck, superstructure, abutments, piers and pier footings as indicated.  
 Protection of existing sewers in locations identified on the plans or as directed by the Engineer shall be paid for as Construction Vibration Monitoring.  
 No excavation around or in the vicinity of Fox MWRD structures, sewers and facilities will be allowed until the design and details to support the Fox MWRD structures, sewers and facilities are prepared, signed and sealed by a Structural Engineer registered in the State of Illinois.  
 Piles at Pier 2 shall be driven through 18" diameter precored holes extending to elevation 662.50 according to Article 512.09(c) of the Standard Specifications. Cost included in Driving Piles.

**BILL OF MATERIAL**

Item	Unit	Total
Bituminous Coated Aggregate Slopewall 6"	Sq. Yd.	730
Retaining Wall Removal	Foot	8

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USER NAME = jrmakow	DESIGNED - JRM	REVISED -
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PLLOT DATE = 5/28/2014	DRAWN - SFW	REVISED -
	CHECKED - MDS	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

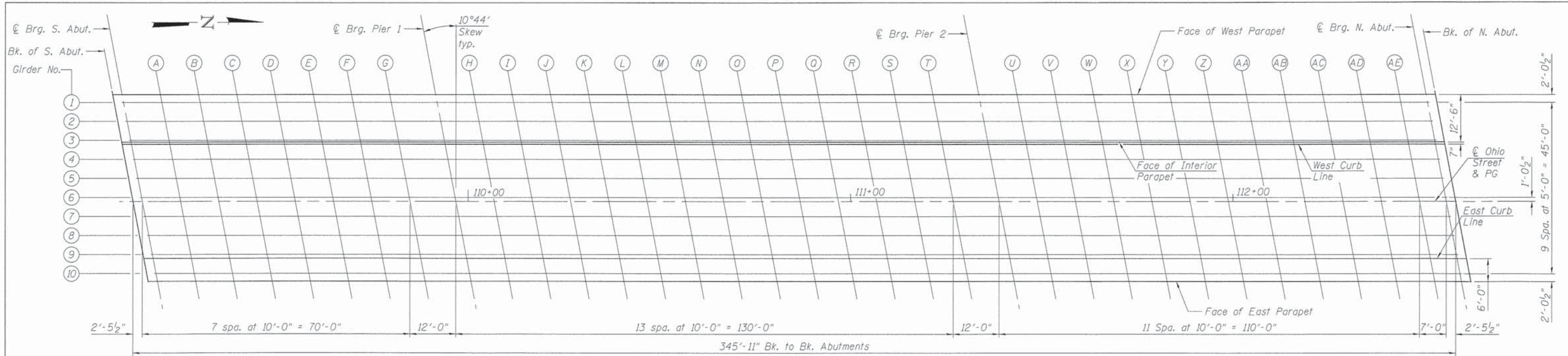
**FOUNDATION PLAN  
 STRUCTURE NO. 045-9942**

SHEET NO. 3 OF 34 SHEETS

F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 74
			CONTRACT NO. 63859	

ILLINOIS FED. AID PROJECT





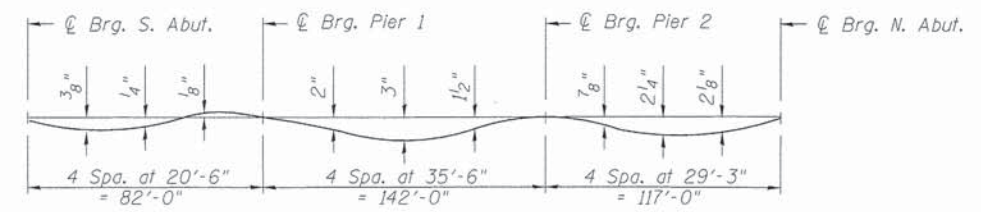
DECK PLAN

FACE OF WEST PARAPET

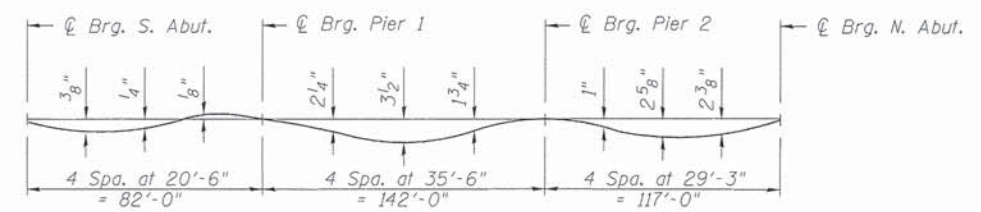
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+07.03	-28.08	708.76	708.76
☉ Brg. S. Abut.	109+09.49	-28.08	708.88	708.88
A	109+19.49	-28.08	709.38	709.40
B	109+29.49	-28.08	709.88	709.91
C	109+39.49	-28.08	710.36	710.39
D	109+49.49	-28.08	710.78	710.80
E	109+59.49	-28.08	711.16	711.16
F	109+69.49	-28.08	711.47	711.46
G	109+79.49	-28.08	711.74	711.72
☉ Brg. Pier 1	109+91.49	-28.08	711.99	711.99
H	110+01.49	-28.08	712.14	712.18
I	110+11.49	-28.08	712.24	712.33
J	110+21.49	-28.08	712.28	712.44
K	110+31.49	-28.08	712.27	712.49
L	110+41.49	-28.08	712.21	712.47
M	110+51.49	-28.08	712.10	712.38
N	110+61.49	-28.08	711.93	712.22
O	110+71.49	-28.08	711.72	711.98
P	110+81.49	-28.08	711.45	711.67
Q	110+91.49	-28.08	711.12	711.30
R	111+01.49	-28.08	710.75	710.86
S	111+11.49	-28.08	710.32	710.38
T	111+21.49	-28.08	709.84	709.86
☉ Brg. Pier 2	111+33.49	-28.08	709.24	709.23
U	111+43.49	-28.08	708.74	708.75
V	111+53.49	-28.08	708.24	708.28
W	111+63.49	-28.08	707.74	707.82
X	111+73.49	-28.08	707.24	707.37
Y	111+83.49	-28.08	706.74	706.92
Z	111+93.49	-28.08	706.24	706.46
AA	112+03.49	-28.08	705.74	705.97
AB	112+13.49	-28.08	705.24	705.46
AC	112+23.49	-28.08	704.74	704.92
AD	112+33.49	-28.08	704.24	704.36
AE	112+43.49	-28.08	703.74	703.79
☉ Brg. N. Abut.	112+50.49	-28.08	703.39	703.38
Bk. N. Abut.	112+52.95	-28.08	703.26	703.26

BEAM 1

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+07.42	-26.04	708.72	708.72
☉ Brg. S. Abut.	109+09.88	-26.04	708.84	708.85
A	109+19.88	-26.04	709.34	709.36
B	109+29.88	-26.04	709.84	709.88
C	109+39.88	-26.04	710.32	710.35
D	109+49.88	-26.04	710.74	710.76
E	109+59.88	-26.04	711.11	711.12
F	109+69.88	-26.04	711.43	711.42
G	109+79.88	-26.04	711.69	711.68
☉ Brg. Pier 1	109+91.88	-26.04	711.94	711.94
H	110+01.88	-26.04	712.08	712.13
I	110+11.88	-26.04	712.18	712.28
J	110+21.88	-26.04	712.22	712.38
K	110+31.88	-26.04	712.21	712.43
L	110+41.88	-26.04	712.15	712.41
M	110+51.88	-26.04	712.04	712.32
N	110+61.88	-26.04	711.87	712.16
O	110+71.88	-26.04	711.65	711.92
P	110+81.88	-26.04	711.37	711.61
Q	110+91.88	-26.04	711.05	711.23
R	111+01.88	-26.04	710.67	710.79
S	111+11.88	-26.04	710.24	710.31
T	111+21.88	-26.04	709.76	709.79
☉ Brg. Pier 2	111+33.88	-26.04	709.16	709.16
U	111+43.88	-26.04	708.66	708.67
V	111+53.88	-26.04	708.16	708.20
W	111+63.88	-26.04	707.66	707.75
X	111+73.88	-26.04	707.16	707.30
Y	111+83.88	-26.04	706.66	706.85
Z	111+93.88	-26.04	706.16	706.38
AA	112+03.88	-26.04	705.66	705.90
AB	112+13.88	-26.04	705.16	705.38
AC	112+23.88	-26.04	704.66	704.85
AD	112+33.88	-26.04	704.16	704.29
AE	112+43.88	-26.04	703.66	703.72
☉ Brg. N. Abut.	112+50.88	-26.04	703.31	703.31
Bk. N. Abut.	112+53.34	-26.04	703.18	703.19

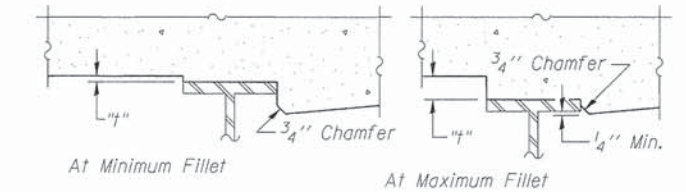


INTERIOR BEAM DEAD LOAD DEFLECTION DIAGRAM  
(Includes weight of concrete only.)



EXTERIOR BEAM DEAD LOAD DEFLECTION DIAGRAM  
(Includes weight of concrete only.)

Note:  
The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on sheets 4 thru 9.



To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown above. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on sheets 4 thru 9, minus slab thickness, equals the fillet heights "t" above top flange of beams.

FILLET HEIGHTS

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PLLOT DATE = 5/12/2014	DRAWN - SFW	REVISED -
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS  
STRUCTURE NO. 045-9942  
SHEET NO. 4 OF 34 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	75
CONTRACT NO. 63859			ILLINOIS FED. AID PROJECT	



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**BEAM 2**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+08.37	-21.04	708.64	708.63
⊕ Brg. S. Abut.	109+10.83	-21.04	708.76	708.76
A	109+20.83	-21.04	709.26	709.27
B	109+30.83	-21.04	709.76	709.78
C	109+40.83	-21.04	710.23	710.25
D	109+50.83	-21.04	710.65	710.66
E	109+60.83	-21.04	711.01	711.01
F	109+70.83	-21.04	711.32	711.31
G	109+80.83	-21.04	711.58	711.57
⊕ Brg. Pier 1	109+92.83	-21.04	711.82	711.82
H	110+02.83	-21.04	711.97	712.00
I	110+12.83	-21.04	712.06	712.14
J	110+22.83	-21.04	712.09	712.23
K	110+32.83	-21.04	712.08	712.26
L	110+42.83	-21.04	712.01	712.23
M	110+52.83	-21.04	711.89	712.14
N	110+62.83	-21.04	711.72	711.97
O	110+72.83	-21.04	711.49	711.72
P	110+82.83	-21.04	711.22	711.41
Q	110+92.83	-21.04	710.88	710.04
R	111+02.83	-21.04	710.50	710.60
S	111+12.83	-21.04	710.07	710.12
T	111+22.83	-21.04	709.58	709.60
⊕ Brg. Pier 2	111+34.83	-21.04	708.98	708.97
U	111+44.83	-21.04	708.48	708.49
V	111+54.83	-21.04	707.98	708.01
W	111+64.83	-21.04	707.48	707.55
X	111+74.83	-21.04	706.98	707.10
Y	111+84.83	-21.04	706.48	706.64
Z	111+94.83	-21.04	705.98	706.17
AA	112+04.83	-21.04	705.48	705.68
AB	112+14.83	-21.04	704.98	705.17
AC	112+24.83	-21.04	704.48	704.64
AD	112+34.83	-21.04	703.98	704.09
AE	112+44.83	-21.04	703.48	703.52
⊕ Brg. N. Abut.	112+51.83	-21.04	703.13	703.12
Bk. N. Abut.	112+54.29	-21.04	703.01	703.00

**BEAM 3**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+09.31	-16.04	708.55	708.54
⊕ Brg. S. Abut.	109+11.77	-16.04	708.67	708.67
A	109+21.77	-16.04	709.17	709.18
B	109+31.77	-16.04	709.67	709.69
C	109+41.77	-16.04	710.13	710.16
D	109+51.77	-16.04	710.54	710.56
E	109+61.77	-16.04	710.90	710.91
F	109+71.77	-16.04	711.21	711.20
G	109+81.77	-16.04	711.46	711.45
⊕ Brg. Pier 1	109+93.77	-16.04	711.70	711.70
H	110+03.77	-16.04	711.84	711.87
I	110+13.77	-16.04	711.92	712.00
J	110+23.77	-16.04	711.95	712.09
K	110+33.77	-16.04	711.93	712.12
L	110+43.77	-16.04	711.86	712.09
M	110+53.77	-16.04	711.74	711.98
N	110+63.77	-16.04	711.56	711.81
O	110+73.77	-16.04	711.33	711.56
P	110+83.77	-16.04	711.05	711.24
Q	110+93.77	-16.04	710.71	710.86
R	111+03.77	-16.04	710.32	710.43
S	111+13.77	-16.04	709.88	709.94
T	111+23.77	-16.04	709.39	709.41
⊕ Brg. Pier 2	111+35.77	-16.04	708.79	708.79
U	111+45.77	-16.04	708.29	708.30
V	111+55.77	-16.04	707.79	707.83
W	111+65.77	-16.04	707.29	707.37
X	111+75.77	-16.04	706.79	706.91
Y	111+85.77	-16.04	706.29	706.45
Z	111+95.77	-16.04	705.79	705.98
AA	112+05.77	-16.04	705.29	705.50
AB	112+15.77	-16.04	704.79	704.99
AC	112+25.77	-16.04	704.29	704.45
AD	112+35.77	-16.04	703.79	703.90
AE	112+45.77	-16.04	703.29	703.34
⊕ Brg. N. Abut.	112+52.77	-16.04	702.94	702.94
Bk. N. Abut.	112+55.23	-16.04	702.82	702.82



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PLDT DATE = 5/12/2014	CHECKED = WJC	REVISED =

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TOP OF SLAB ELEVATIONS  
STRUCTURE NO. 045-9942**

SHEET NO. 5 OF 34 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	76
			CONTRACT NO. 63859	
ILLINOIS FED. AID PROJECT				



WEST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+09.51	-15.00	708.53	708.53
⊕ Brg. S. Abut.	109+11.97	-15.00	708.65	708.65
A	109+21.97	-15.00	709.15	709.16
B	109+31.97	-15.00	709.65	709.67
C	109+41.97	-15.00	710.11	710.14
D	109+51.97	-15.00	710.52	710.54
E	109+61.97	-15.00	710.88	710.89
F	109+71.97	-15.00	711.18	711.18
G	109+81.97	-15.00	711.44	711.43
⊕ Brg. Pier 1	109+93.97	-15.00	711.67	711.67
H	110+03.97	-15.00	711.81	711.85
I	110+13.97	-15.00	711.89	711.98
J	110+23.97	-15.00	711.92	712.06
K	110+33.97	-15.00	711.90	712.09
L	110+43.97	-15.00	711.83	712.06
M	110+53.97	-15.00	711.70	711.95
N	110+63.97	-15.00	711.53	711.78
O	110+73.97	-15.00	711.29	711.53
P	110+83.97	-15.00	711.01	711.21
Q	110+93.97	-15.00	710.67	710.83
R	111+03.97	-15.00	710.28	710.39
S	111+13.97	-15.00	709.84	709.90
T	111+23.97	-15.00	709.35	709.38
⊕ Brg. Pier 2	111+35.97	-15.00	708.75	708.75
U	111+45.97	-15.00	708.25	708.26
V	111+55.97	-15.00	707.75	707.79
W	111+65.97	-15.00	707.25	707.33
X	111+75.97	-15.00	706.75	706.87
Y	111+85.97	-15.00	706.25	706.41
Z	111+95.97	-15.00	705.75	705.94
AA	112+05.97	-15.00	705.25	705.46
AB	112+15.97	-15.00	704.75	704.95
AC	112+25.97	-15.00	704.25	704.42
AD	112+35.97	-15.00	703.75	703.87
AE	112+45.97	-15.00	703.25	703.30
⊕ Brg. N. Abut.	112+52.97	-15.00	702.90	702.90
Bk. N. Abut.	112+55.43	-15.00	702.78	702.78

BEAM 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+10.26	-11.04	708.64	708.64
⊕ Brg. S. Abut.	109+12.72	-11.04	708.77	708.76
A	109+22.72	-11.04	709.27	709.28
B	109+32.72	-11.04	709.76	709.79
C	109+42.72	-11.04	710.22	710.25
D	109+52.72	-11.04	710.63	710.65
E	109+62.72	-11.04	710.98	710.99
F	109+72.72	-11.04	711.29	711.28
G	109+82.72	-11.04	711.53	711.52
⊕ Brg. Pier 1	109+94.72	-11.04	711.76	711.76
H	110+04.72	-11.04	711.90	711.93
I	110+14.72	-11.04	711.98	712.06
J	110+24.72	-11.04	712.00	712.14
K	110+34.72	-11.04	711.98	712.17
L	110+44.72	-11.04	711.90	712.13
M	110+54.72	-11.04	711.77	712.02
N	110+64.72	-11.04	711.59	711.84
O	110+74.72	-11.04	711.35	711.59
P	110+84.72	-11.04	711.07	711.27
Q	110+94.72	-11.04	710.73	710.88
R	111+04.72	-11.04	710.33	710.44
S	111+14.72	-11.04	709.89	709.95
T	111+24.72	-11.04	709.39	709.42
⊕ Brg. Pier 2	111+36.72	-11.04	708.79	708.79
U	111+46.72	-11.04	708.29	708.31
V	111+56.72	-11.04	707.79	707.83
W	111+66.72	-11.04	707.29	707.37
X	111+76.72	-11.04	706.79	706.92
Y	111+86.72	-11.04	706.29	706.46
Z	111+96.72	-11.04	705.79	705.99
AA	112+06.72	-11.04	705.29	705.50
AB	112+16.72	-11.04	704.79	704.99
AC	112+26.72	-11.04	704.29	704.46
AD	112+36.72	-11.04	703.79	703.91
AE	112+46.72	-11.04	703.29	703.34
⊕ Brg. N. Abut.	112+53.72	-11.04	702.94	702.94
Bk. N. Abut.	112+56.18	-11.04	702.82	702.82

BEAM 5

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+11.21	-6.04	708.79	708.79
⊕ Brg. S. Abut.	109+13.67	-6.04	708.91	708.91
A	109+23.67	-6.04	709.41	709.43
B	109+33.67	-6.04	709.91	709.94
C	109+43.67	-6.04	710.36	710.39
D	109+53.67	-6.04	710.77	710.78
E	109+63.67	-6.04	711.11	711.12
F	109+73.67	-6.04	711.41	711.40
G	109+83.67	-6.04	711.66	711.64
⊕ Brg. Pier 1	109+95.67	-6.04	711.88	711.88
H	110+05.67	-6.04	712.01	712.04
I	110+15.67	-6.04	712.08	712.17
J	110+25.67	-6.04	712.10	712.24
K	110+35.67	-6.04	712.08	712.26
L	110+45.67	-6.04	711.99	712.22
M	110+55.67	-6.04	711.86	712.11
N	110+65.67	-6.04	711.67	711.92
O	110+75.67	-6.04	711.43	711.66
P	110+85.67	-6.04	711.14	711.34
Q	110+95.67	-6.04	710.79	710.95
R	111+05.67	-6.04	710.39	710.50
S	111+15.67	-6.04	709.94	710.00
T	111+25.67	-6.04	709.45	709.47
⊕ Brg. Pier 2	111+37.67	-6.04	708.85	708.85
U	111+47.67	-6.04	708.35	708.36
V	111+57.67	-6.04	707.85	707.89
W	111+67.67	-6.04	707.35	707.43
X	111+77.67	-6.04	706.85	706.97
Y	111+87.67	-6.04	706.35	706.51
Z	111+97.67	-6.04	705.85	706.04
AA	112+07.67	-6.04	705.35	705.55
AB	112+17.67	-6.04	704.85	705.04
AC	112+27.67	-6.04	704.35	704.51
AD	112+37.67	-6.04	703.85	703.96
AE	112+47.67	-6.04	703.35	703.40
⊕ Brg. N. Abut.	112+54.67	-6.04	703.00	703.00
Bk. N. Abut.	112+57.13	-6.04	702.87	702.87

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USER NAME = jrmickow	DESIGNED - JRM	REVISED -
	CHECKED - WJC	REVISED -
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PLLOT DATE = 5/12/2014	CHECKED - WJC	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS  
STRUCTURE NO. 045-9942

SHEET NO. 6 OF 34 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	77
			CONTRACT NO. 63859	
[ILLINOIS] FED. AID PROJECT				



BEAM 6

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+12.16	-1.04	708.94	708.94
⊕ Brg. S. Abut.	109+14.62	-1.04	709.06	709.06
A	109+24.62	-1.04	709.56	709.58
B	109+34.62	-1.04	710.05	710.08
C	109+44.62	-1.04	710.50	710.53
D	109+54.62	-1.04	710.90	710.92
E	109+64.62	-1.04	711.24	711.25
F	109+74.62	-1.04	711.54	711.53
G	109+84.62	-1.04	711.77	711.76
⊕ Brg. Pier 1	109+96.62	-1.04	711.99	711.99
H	110+06.62	-1.04	712.12	712.15
I	110+16.62	-1.04	712.19	712.27
J	110+26.62	-1.04	712.20	712.34
K	110+36.62	-1.04	712.17	712.36
L	110+46.62	-1.04	712.08	712.31
M	110+56.62	-1.04	711.94	712.19
N	110+66.62	-1.04	711.75	712.00
O	110+76.62	-1.04	711.50	711.74
P	110+86.62	-1.04	711.21	711.41
Q	110+96.62	-1.04	710.85	711.01
R	111+06.62	-1.04	710.45	710.56
S	111+16.62	-1.04	710.00	710.06
T	111+26.62	-1.04	709.50	709.52
⊕ Brg. Pier 2	111+38.62	-1.04	708.90	708.90
U	111+48.62	-1.04	708.40	708.41
V	111+58.62	-1.04	707.90	707.94
W	111+68.62	-1.04	707.40	707.48
X	111+78.62	-1.04	706.90	707.02
Y	111+88.62	-1.04	706.40	706.56
Z	111+98.62	-1.04	705.90	706.09
AA	112+08.62	-1.04	705.40	705.61
AB	112+18.62	-1.04	704.90	705.09
AC	112+28.62	-1.04	704.40	704.56
AD	112+38.62	-1.04	703.90	704.01
AE	112+48.62	-1.04	703.40	703.45
⊕ Brg. N. Abut.	112+55.62	-1.04	703.05	703.05
Bk. N. Abut.	112+58.08	-1.04	702.93	702.93

CENTERLINE AND PROFILE GRADE LINE

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+12.36	0.00	708.97	708.97
⊕ Brg. S. Abut.	109+14.81	0.00	709.09	709.09
A	109+24.81	0.00	709.59	709.61
B	109+34.81	0.00	710.08	710.11
C	109+44.81	0.00	710.53	710.56
D	109+54.81	0.00	710.93	710.95
E	109+64.81	0.00	711.27	711.28
F	109+74.81	0.00	711.56	711.56
G	109+84.81	0.00	711.80	711.79
⊕ Brg. Pier 1	109+96.81	0.00	712.02	712.02
H	110+06.81	0.00	712.14	712.18
I	110+16.81	0.00	712.21	712.29
J	110+26.81	0.00	712.22	712.36
K	110+36.81	0.00	712.19	712.38
L	110+46.81	0.00	712.10	712.33
M	110+56.81	0.00	711.96	712.21
N	110+66.81	0.00	711.77	712.02
O	110+76.81	0.00	711.52	711.75
P	110+86.81	0.00	711.22	711.42
Q	110+96.81	0.00	710.87	711.02
R	111+06.81	0.00	710.46	710.57
S	111+16.81	0.00	710.01	710.07
T	111+26.81	0.00	709.51	709.53
⊕ Brg. Pier 2	111+38.81	0.00	708.91	708.91
U	111+48.81	0.00	708.41	708.42
V	111+58.81	0.00	707.91	707.95
W	111+68.81	0.00	707.41	707.49
X	111+78.81	0.00	706.91	707.03
Y	111+88.81	0.00	706.41	706.57
Z	111+98.81	0.00	705.91	706.10
AA	112+08.81	0.00	705.41	705.62
AB	112+18.81	0.00	704.91	705.11
AC	112+28.81	0.00	704.41	704.57
AD	112+38.81	0.00	703.91	704.02
AE	112+48.81	0.00	703.41	703.46
⊕ Brg. N. Abut.	112+55.81	0.00	703.06	703.06
Bk. N. Abut.	112+58.27	0.00	702.94	702.94

BEAM 7

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+13.11	3.96	708.93	708.93
⊕ Brg. S. Abut.	109+15.57	3.96	709.05	709.05
A	109+25.57	3.96	709.55	709.57
B	109+35.57	3.96	710.04	710.07
C	109+45.57	3.96	710.48	710.51
D	109+55.57	3.96	710.88	710.90
E	109+65.57	3.96	711.22	711.22
F	109+75.57	3.96	711.50	711.50
G	109+85.57	3.96	711.74	711.73
⊕ Brg. Pier 1	109+97.57	3.96	711.95	711.95
H	110+07.57	3.96	712.06	712.10
I	110+17.57	3.96	712.13	712.22
J	110+27.57	3.96	712.14	712.28
K	110+37.57	3.96	712.10	712.29
L	110+47.57	3.96	712.01	712.24
M	110+57.57	3.96	711.87	712.12
N	110+67.57	3.96	711.67	711.92
O	110+77.57	3.96	711.42	711.65
P	110+87.57	3.96	711.12	711.32
Q	110+97.57	3.96	710.76	710.92
R	111+07.57	3.96	710.35	710.46
S	111+17.57	3.96	709.89	709.95
T	111+27.57	3.96	709.39	709.42
⊕ Brg. Pier 2	111+39.57	3.96	708.79	708.79
U	111+49.57	3.96	708.29	708.30
V	111+59.57	3.96	707.79	707.83
W	111+69.57	3.96	707.29	707.37
X	111+79.57	3.96	706.79	706.91
Y	111+89.57	3.96	706.29	706.46
Z	111+99.57	3.96	705.79	705.99
AA	112+09.57	3.96	705.29	705.50
AB	112+19.57	3.96	704.79	704.99
AC	112+29.57	3.96	704.29	704.46
AD	112+39.57	3.96	703.79	703.91
AE	112+49.57	3.96	703.29	703.34
⊕ Brg. N. Abut.	112+56.57	3.96	702.94	702.94
Bk. N. Abut.	112+59.02	3.96	702.82	702.82

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PLOT DATE = 5/12/2014	CHECKED - WJC	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS  
STRUCTURE NO. 045-9942

SHEET NO. 7 OF 34 SHEETS

F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 78
CONTRACT NO. 63859				
ILLINOIS FED. AID PROJECT				



BEAM 8

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+14.05	8.96	708.87	708.87
⊕ Brg. S. Abut.	109+16.51	8.96	709.00	709.00
A	109+26.51	8.96	709.50	709.51
B	109+36.51	8.96	709.98	710.01
C	109+46.51	8.96	710.42	710.45
D	109+56.51	8.96	710.81	710.83
E	109+66.51	8.96	711.14	711.15
F	109+76.51	8.96	711.43	711.42
G	109+86.51	8.96	711.65	711.64
⊕ Brg. Pier 1	109+98.51	8.96	711.86	711.86
H	110+08.51	8.96	711.97	712.01
I	110+18.51	8.96	712.03	712.12
J	110+28.51	8.96	712.04	712.18
K	110+38.51	8.96	712.00	712.19
L	110+48.51	8.96	711.90	712.13
M	110+58.51	8.96	711.75	712.00
N	110+68.51	8.96	711.55	711.80
O	110+78.51	8.96	711.29	711.53
P	110+88.51	8.96	710.98	711.18
Q	110+98.51	8.96	710.62	710.78
R	111+08.51	8.96	710.21	710.32
S	111+18.51	8.96	709.74	709.81
T	111+28.51	8.96	709.24	709.27
⊕ Brg. Pier 2	111+40.51	8.96	708.64	708.65
U	111+50.51	8.96	708.14	708.16
V	111+60.51	8.96	707.64	707.69
W	111+70.51	8.96	707.14	707.22
X	111+80.51	8.96	706.64	706.77
Y	111+90.51	8.96	706.14	706.31
Z	112+00.51	8.96	705.64	705.84
AA	112+10.51	8.96	705.14	705.35
AB	112+20.51	8.96	704.64	704.84
AC	112+30.51	8.96	704.14	704.31
AD	112+40.51	8.96	703.64	703.76
AE	112+50.51	8.96	703.14	703.20
⊕ Brg. N. Abut.	112+57.51	8.96	702.79	702.80
Bk. N. Abut.	112+59.97	8.96	702.67	702.67

BEAM 9

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+15.00	13.96	708.82	708.82
⊕ Brg. S. Abut.	109+17.46	13.96	708.94	708.94
A	109+27.46	13.96	709.44	709.46
B	109+37.46	13.96	709.93	709.96
C	109+47.46	13.96	710.36	710.39
D	109+57.46	13.96	710.74	710.76
E	109+67.46	13.96	711.07	711.08
F	109+77.46	13.96	711.35	711.34
G	109+87.46	13.96	711.57	711.56
⊕ Brg. Pier 1	109+99.46	13.96	711.77	711.77
H	110+09.46	13.96	711.88	711.92
I	110+19.46	13.96	711.94	712.02
J	110+29.46	13.96	711.94	712.08
K	110+39.46	13.96	711.89	712.08
L	110+49.46	13.96	711.79	712.01
M	110+59.46	13.96	711.63	711.88
N	110+69.46	13.96	711.43	711.68
O	110+79.46	13.96	711.16	711.40
P	110+89.46	13.96	710.85	711.05
Q	110+99.46	13.96	710.49	710.64
R	111+09.46	13.96	710.07	710.17
S	111+19.46	13.96	709.60	709.66
T	111+29.46	13.96	709.10	709.12
⊕ Brg. Pier 2	111+41.46	13.96	708.50	708.50
U	111+51.46	13.96	708.00	708.01
V	111+61.46	13.96	707.50	707.54
W	111+71.46	13.96	707.00	707.08
X	111+81.46	13.96	706.50	706.62
Y	111+91.46	13.96	706.00	706.16
Z	112+01.46	13.96	705.50	705.69
AA	112+11.46	13.96	705.00	705.20
AB	112+21.46	13.96	704.50	704.69
AC	112+31.46	13.96	704.00	704.16
AD	112+41.46	13.96	703.50	703.61
AE	112+51.46	13.96	703.00	703.05
⊕ Brg. N. Abut.	112+58.46	13.96	702.65	702.65
Bk. N. Abut.	112+60.92	13.96	702.52	702.53

EAST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+15.20	15.00	708.81	708.81
⊕ Brg. S. Abut.	109+17.66	15.00	708.93	708.93
A	109+27.66	15.00	709.43	709.45
B	109+37.66	15.00	709.92	709.94
C	109+47.66	15.00	710.35	710.38
D	109+57.66	15.00	710.73	710.75
E	109+67.66	15.00	711.06	711.07
F	109+77.66	15.00	711.33	711.33
G	109+87.66	15.00	711.56	711.55
⊕ Brg. Pier 1	109+99.66	15.00	711.76	711.76
H	110+09.66	15.00	711.86	711.90
I	110+19.66	15.00	711.92	712.00
J	110+29.66	15.00	711.92	712.06
K	110+39.66	15.00	711.87	712.06
L	110+49.66	15.00	711.77	711.99
M	110+59.66	15.00	711.61	711.86
N	110+69.66	15.00	711.40	711.65
O	110+79.66	15.00	711.14	711.37
P	110+89.66	15.00	710.83	711.03
Q	110+99.66	15.00	710.46	710.61
R	111+09.66	15.00	710.04	710.14
S	111+19.66	15.00	709.57	709.63
T	111+29.66	15.00	709.07	709.09
⊕ Brg. Pier 2	111+41.66	15.00	708.47	708.47
U	111+51.66	15.00	707.97	707.98
V	111+61.66	15.00	707.47	707.51
W	111+71.66	15.00	706.97	707.05
X	111+81.66	15.00	706.47	706.59
Y	111+91.66	15.00	705.97	706.13
Z	112+01.66	15.00	705.47	705.66
AA	112+11.66	15.00	704.97	705.17
AB	112+21.66	15.00	704.47	704.66
AC	112+31.66	15.00	703.97	704.13
AD	112+41.66	15.00	703.47	703.58
AE	112+51.66	15.00	702.97	703.02
⊕ Brg. N. Abut.	112+58.66	15.00	702.62	702.62
Bk. N. Abut.	112+61.12	15.00	702.49	702.49

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PLOT DATE = 5/12/2014	CHECKED = WJC	REVISED =

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TOP OF SLAB ELEVATIONS  
STRUCTURE NO. 045-9942**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	79
			CONTRACT NO. 63859	
ILLINOIS FED. AID PROJECT				

SHEET NO. 8 OF 34 SHEETS



BEAM 10

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+15.95	18.96	708.99	708.98
⊕ Brg. S. Abut.	109+18.41	18.96	709.11	709.11
A	109+28.41	18.96	709.61	709.63
B	109+38.41	18.96	710.09	710.12
C	109+48.41	18.96	710.52	710.55
D	109+58.41	18.96	710.90	710.92
E	109+68.41	18.96	711.22	711.23
F	109+78.41	18.96	711.49	711.48
G	109+88.41	18.96	711.71	711.70
⊕ Brg. Pier 1	110+00.41	18.96	711.91	711.90
H	110+10.41	18.96	712.01	712.05
I	110+20.41	18.96	712.06	712.15
J	110+30.41	18.96	712.06	712.21
K	110+40.41	18.96	712.00	712.22
L	110+50.41	18.96	711.90	712.15
M	110+60.41	18.96	711.74	712.02
N	110+70.41	18.96	711.52	711.81
O	110+80.41	18.96	711.26	711.52
P	110+90.41	18.96	710.94	711.17
Q	111+00.41	18.96	710.57	710.74
R	111+10.41	18.96	710.15	710.26
S	111+20.41	18.96	709.67	709.74
T	111+30.41	18.96	709.17	709.19
⊕ Brg. Pier 2	111+42.41	18.96	708.57	708.57
U	111+52.41	18.96	708.07	708.08
V	111+62.41	18.96	707.57	707.61
W	111+72.41	18.96	707.07	707.16
X	111+82.41	18.96	706.57	706.71
Y	111+92.41	18.96	706.07	706.25
Z	112+02.41	18.96	705.57	705.79
AA	112+12.41	18.96	705.07	705.31
AB	112+22.41	18.96	704.57	704.79
AC	112+32.41	18.96	704.07	704.26
AD	112+42.41	18.96	703.57	703.70
AE	112+52.41	18.96	703.07	703.12
⊕ Brg. N. Abut.	112+59.41	18.96	702.72	702.72
Bk. N. Abut.	112+61.87	18.96	702.60	702.59

FACE OF EAST PARAPET

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	109+16.34	21.00	709.08	709.08
⊕ Brg. S. Abut.	109+18.80	21.00	709.20	709.20
A	109+28.80	21.00	709.70	709.72
B	109+38.80	21.00	710.18	710.21
C	109+48.80	21.00	710.61	710.64
D	109+58.80	21.00	710.98	711.00
E	109+68.80	21.00	711.30	711.31
F	109+78.80	21.00	711.57	711.56
G	109+88.80	21.00	711.79	711.78
⊕ Brg. Pier 1	110+00.80	21.00	711.98	711.98
H	110+10.80	21.00	712.08	712.12
I	110+20.80	21.00	712.13	712.23
J	110+30.80	21.00	712.13	712.28
K	110+40.80	21.00	712.07	712.28
L	110+50.80	21.00	711.96	712.22
M	110+60.80	21.00	711.80	712.08
N	110+70.80	21.00	711.58	711.87
O	110+80.80	21.00	711.32	711.58
P	110+90.80	21.00	711.00	711.22
Q	111+00.80	21.00	710.62	710.80
R	111+10.80	21.00	710.20	710.32
S	111+20.80	21.00	709.72	709.79
T	111+30.80	21.00	709.22	709.25
⊕ Brg. Pier 2	111+42.80	21.00	708.62	708.62
U	111+52.80	21.00	708.12	708.13
V	111+62.80	21.00	707.62	707.67
W	111+72.80	21.00	707.12	707.21
X	111+82.80	21.00	706.62	706.76
Y	111+92.80	21.00	706.12	706.31
Z	112+02.80	21.00	705.62	705.84
AA	112+12.80	21.00	705.12	705.36
AB	112+22.80	21.00	704.62	704.84
AC	112+32.80	21.00	704.12	704.31
AD	112+42.80	21.00	703.62	703.75
AE	112+52.80	21.00	703.12	703.18
⊕ Brg. N. Abut.	112+59.80	21.00	702.77	702.77
Bk. N. Abut.	112+62.26	21.00	702.65	702.65

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USER NAME = jrmakow	DESIGNED - JRM	REVISED -
	CHECKED - WJC	REVISED -
PLOT SCALE = 1/8" = 1' / 1/4"	DRAWN - SFW	REVISED -
PLOT DATE = 5/12/2014	CHECKED - WJC	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TOP OF SLAB ELEVATIONS  
STRUCTURE NO. 045-9942**

SHEET NO. 9 OF 34 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	80
CONTRACT NO. 63859			ILLINOIS FED. AID PROJECT	



FACE OF WEST CURB LINE

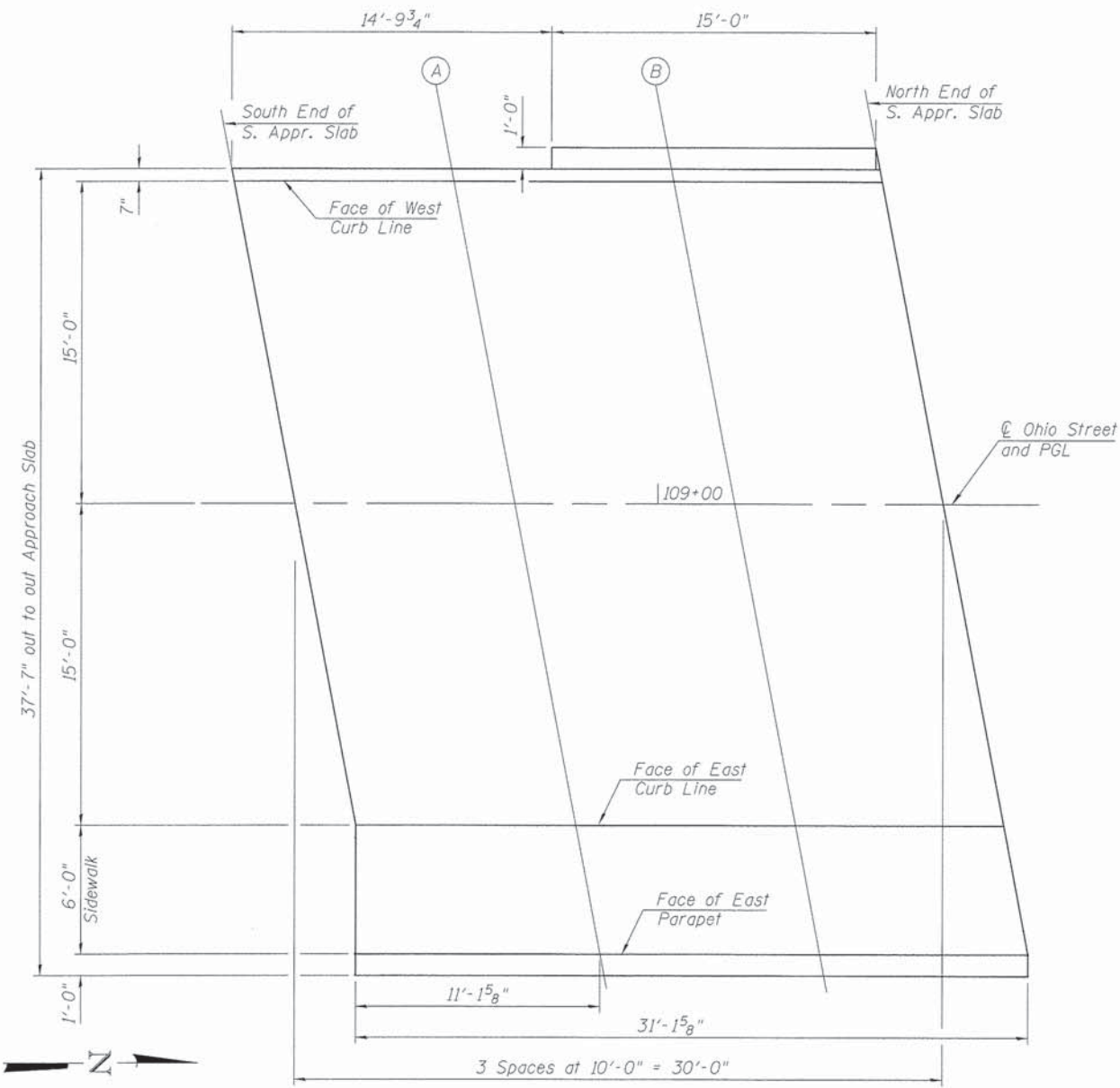
Location	Station	Offset	Theoretical Grade Elevations
South End of S. Appr. Slab	108+80.36	-15.00	707.07
A	108+90.36	-15.00	707.57
B	109+00.36	-15.00	708.07
North End of S. Appr. Slab	109+10.36	-15.00	708.57

CENTERLINE OHIO STREET AND PROFILE GRADE LINE

Location	Station	Offset	Theoretical Grade Elevations
South End of S. Appr. Slab	108+83.20	0.00	707.51
A	108+93.20	0.00	708.01
B	109+03.20	0.00	708.51
North End of S. Appr. Slab	109+13.20	0.00	709.01

FACE OF EAST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
South End of S. Appr. Slab	108+86.04	15.00	707.35
A	108+96.04	15.00	707.85
B	109+06.04	15.00	708.35
North End of S. Appr. Slab	109+16.04	15.00	708.85



SOUTH APPROACH PLAN

FACE OF EAST PARAPET

Location	Station	Offset	Theoretical Grade Elevations
South End of S. Appr. Slab	108+86.04	21.00	707.56
A	108+97.17	21.00	708.12
B	109+07.17	21.00	708.62
North End of S. Appr. Slab	109+17.17	21.00	709.12

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USER NAME = jrmickow	DESIGNED - JRM	REVISED -
PLOT SCALE = 4/8" = 1' / in.	CHECKED - MDS	REVISED -
PLOT DATE = 5/12/2014	DRAWN - SFW	REVISED -
	CHECKED - MDS	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SOUTH APPROACH SLAB ELEVATIONS  
STRUCTURE NO. 045-9942

SHEET NO. 10 OF 34 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	81
			CONTRACT NO. 63859	
[ILLINOIS] FED. AID PROJECT				



FACE OF WEST CURB LINE

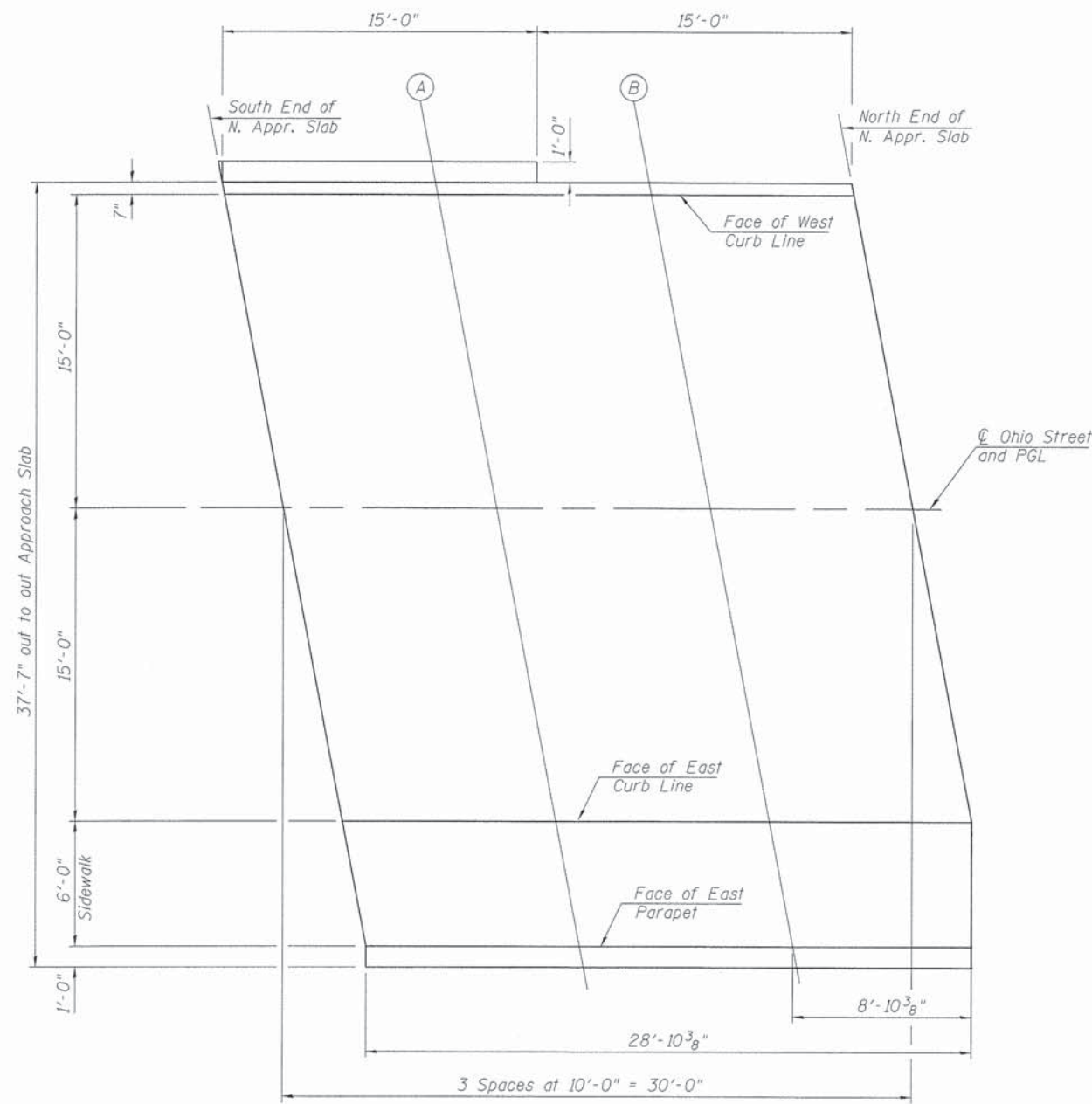
Location	Station	Offset	Theoretical Grade Elevations
South End of N. Appr. Slab	112+54.58	-15.00	702.82
A	112+64.58	-15.00	702.32
B	112+74.58	-15.00	701.82
North End of N. Appr. Slab	112+84.58	-15.00	701.32

CENTERLINE OHIO STREET AND PROFILE GRADE LINE

Location	Station	Offset	Theoretical Grade Elevations
South End of N. Appr. Slab	112+57.43	0.00	702.98
A	112+67.43	0.00	702.48
B	112+77.43	0.00	701.98
North End of N. Appr. Slab	112+87.43	0.00	701.48

FACE OF EAST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
South End of N. Appr. Slab	112+60.27	15.00	702.54
A	112+70.27	15.00	702.04
B	112+80.27	15.00	701.54
North End of N. Appr. Slab	112+90.27	15.00	701.04



FACE OF EAST PARAPET

Location	Station	Offset	Theoretical Grade Elevations
South End of N. Appr. Slab	112+61.41	21.00	702.69
A	112+71.41	21.00	702.19
B	112+81.41	21.00	701.69
North End of N. Appr. Slab	112+90.27	21.00	701.25

NORTH APPROACH PLAN

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PLOT DATE = 5/12/2014	CHECKED - MDS	REVISED -

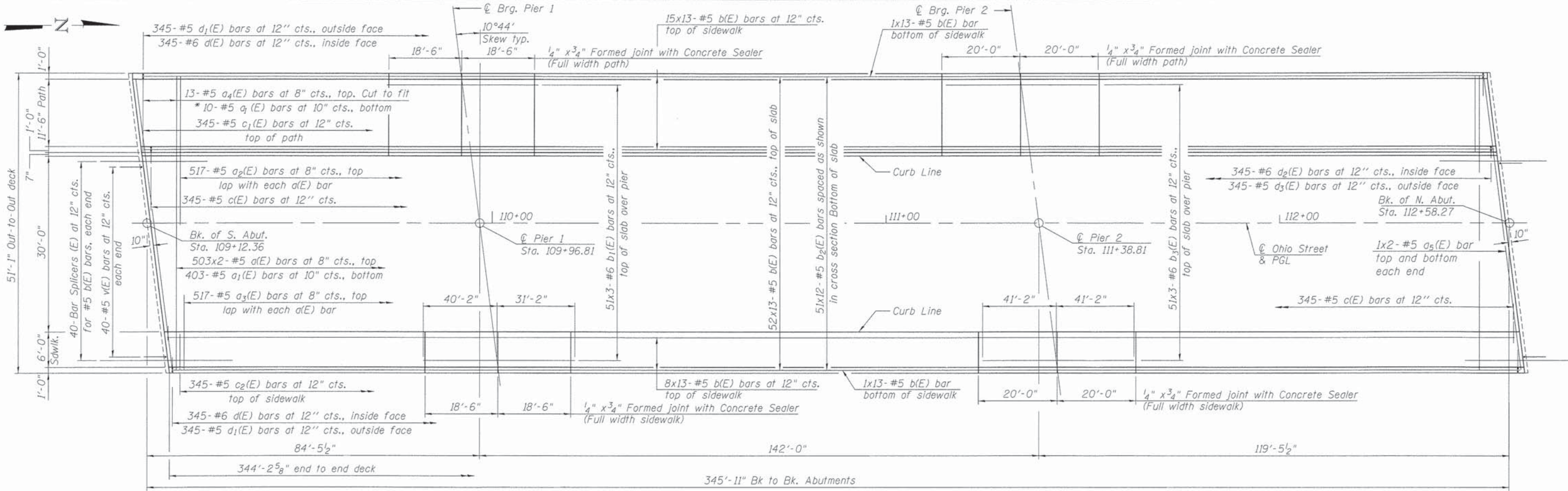
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF NORTH APPROACH SLAB ELEVATIONS  
STRUCTURE NO. 045-9942

SHEET NO. 11 OF 34 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	82
				CONTRACT NO. 63859
ILLINOIS FED. AID PROJECT				

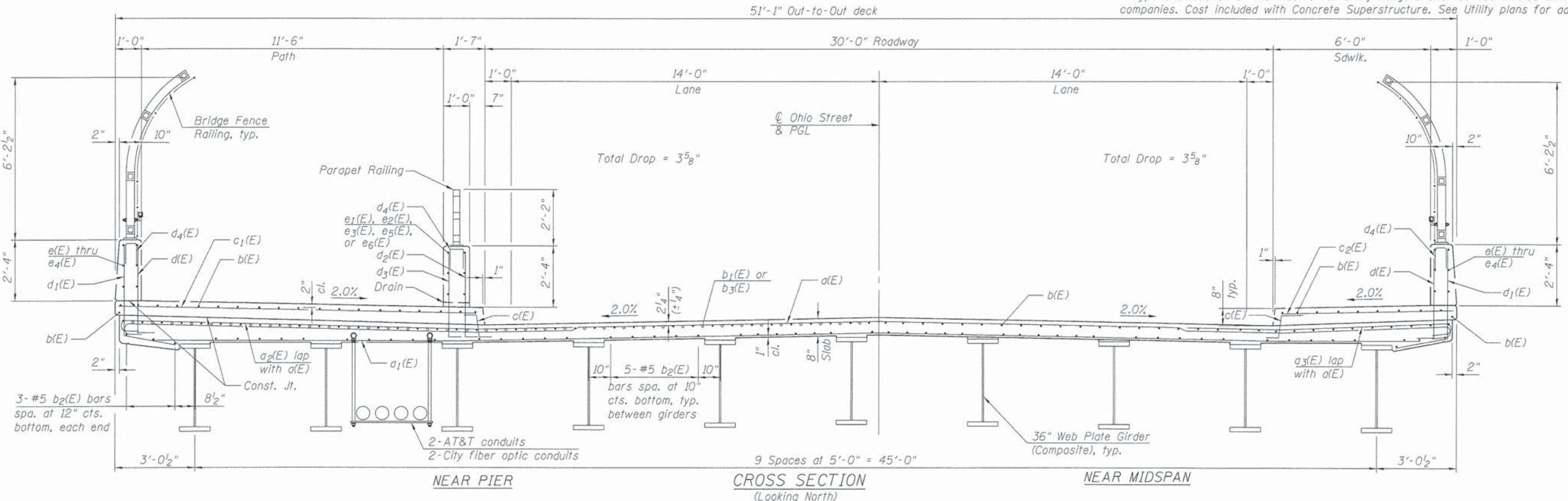




**DECK PLAN**

\* Order a<sub>1</sub>(E) bars full length. Cut bars to fit skew and use remainder of bars in opposite end.

Note:  
 See Sheet 13 of 34 for superstructure details and Bill of Material.  
 Bars indicated thus 20x3-#5 etc. indicates 20 lines of bars with 3 lengths per line.  
 See Sheet 18 of 34 for parapet reinforcement.  
 See Sheet 13 of 34 for Deck Pouring Sequence.  
 Type and location of the inserts for utility hangers shall be coordinated with the utility companies. Cast included with Concrete Superstructure. See Utility plans for additional details.



**CROSS SECTION**  
(Looking North)

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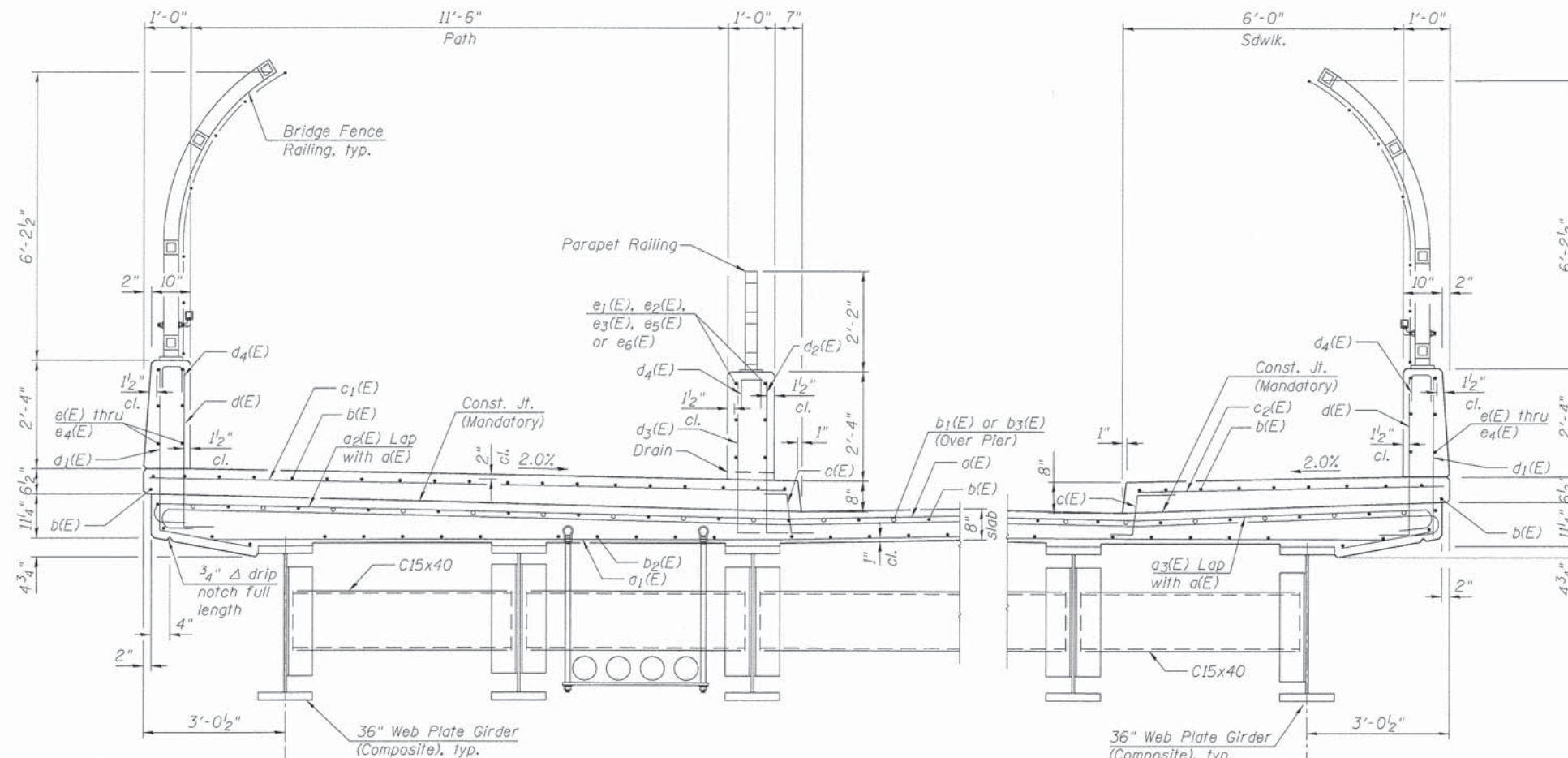
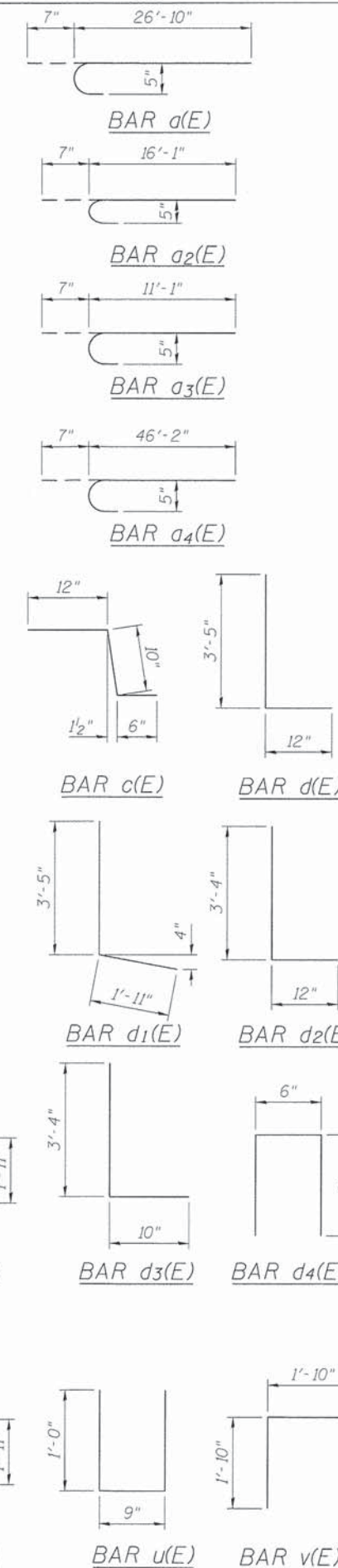
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	PLOT SCALE = 1/8" = 1'-0"	DRAWN - SFW	REVISED -			CONTRACT NO. 63859				
	PLOT DATE = 5/12/2014	CHECKED - MDS	REVISED -			[ILLINOIS] FED. AID PROJECT				
	SHEET NO. 12 OF 34 SHEETS									



**SUPERSTRUCTURE  
BILL OF MATERIAL**

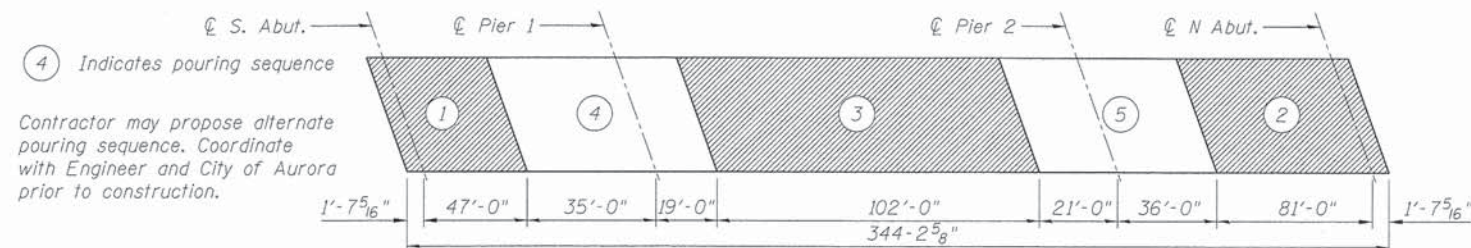
Bar	No.	Size	Length	Shape
a(E)	1006	#5	27'-5"	
a <sub>1</sub> (E)	413	#5	50'-5"	
a <sub>2</sub> (E)	517	#5	16'-8"	
a <sub>3</sub> (E)	517	#5	11'-8"	
a <sub>4</sub> (E)	26	#5	46'-9"	
a <sub>5</sub> (E)	8	#5	27'-4"	
b(E)	1001	#5	29'-9"	
b <sub>1</sub> (E)	153	#6	27'-8"	
b <sub>2</sub> (E)	612	#5	31'-11"	
b <sub>3</sub> (E)	153	#6	31'-3"	
c(E)	690	#5	2'-4"	
c <sub>1</sub> (E)	345	#5	13'-8"	
c <sub>2</sub> (E)	345	#5	6'-7"	
d(E)	690	#6	4'-5"	
d <sub>1</sub> (E)	690	#5	5'-4"	
d <sub>2</sub> (E)	345	#6	4'-4"	
d <sub>3</sub> (E)	345	#5	4'-2"	
d <sub>4</sub> (E)	222	#4	2'-0"	
e(E)	48	#5	15'-11"	
e <sub>1</sub> (E)	36	#5	18'-2"	
e <sub>2</sub> (E)	108	#5	16'-11"	
e <sub>3</sub> (E)	36	#5	19'-8"	
e <sub>4</sub> (E)	72	#5	16'-1"	
e <sub>5</sub> (E)	24	#5	15'-5"	
e <sub>6</sub> (E)	36	#5	16'-6"	
h <sub>5</sub> (E)	6	#5	3'-8"	
h <sub>6</sub> (E)	8	#5	1'-9"	
m(E)	16	#6	50'-8"	
m <sub>1</sub> (E)	32	#6	8'-11"	
m <sub>2</sub> (E)	4	#6	6'-7"	
m <sub>3</sub> (E)	36	#6	4'-8"	
m <sub>4</sub> (E)	4	#6	1'-11"	
m <sub>5</sub> (E)	4	#6	2'-7"	
m <sub>6</sub> (E)	4	#6	7'-3"	
s(E)	51	#4	10'-2"	
s <sub>1</sub> (E)	61	#5	9'-0"	
s <sub>2</sub> (E)	51	#4	10'-1"	
s <sub>3</sub> (E)	61	#5	8'-9"	
u(E)	104	#5	2'-9"	
v(E)	80	#5	3'-8"	
v <sub>3</sub> (E)	8	#5	1'-3"	
Concrete Superstructure		Cu. Yd.	797.8	
Bridge Deck Grooving		Sq. Yd.	1148	
Protective Coat		Sq. Yd.	2350	
Reinforcement Bars, Epoxy Coated		Pound	164,480	

Note:  
For parapet joint locations, see sheet 18 of 34.  
When the deck pour is stopped for the day at one or more of the transverse bonded construction joints in the deck pouring sequence as shown, the next pour shall not be made until both of the following are met:  
1) At least 72 hours shall have elapsed from the end of the previous pour.  
2) The concrete strength shall have attained a minimum flexural strength of 650 psi or a minimum compressive strength of 3500 psi.

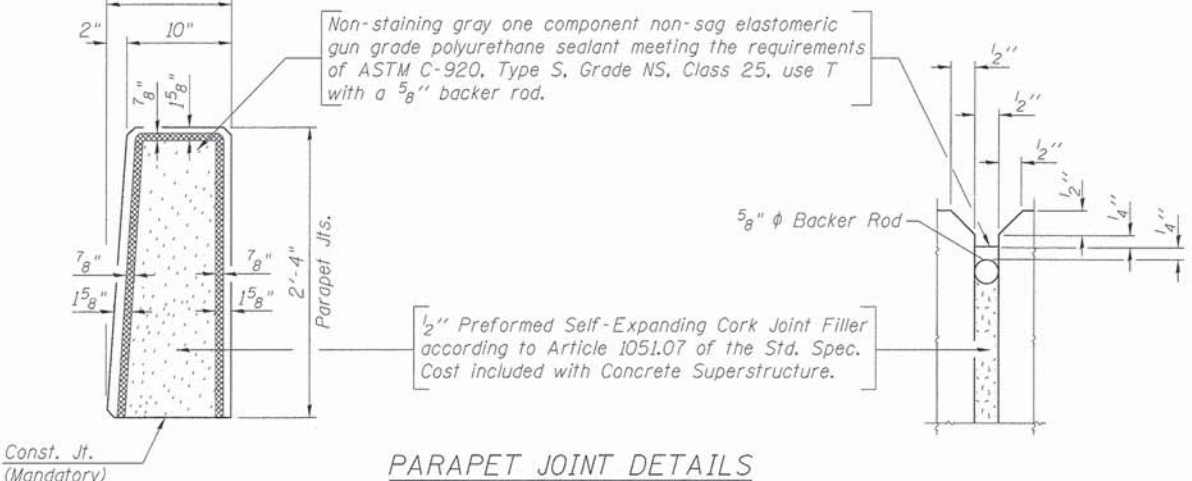


**SECTION THRU PATH**

**SECTION THRU SIDEWALK**



**DECK POURING SEQUENCE**



**PARAPET JOINT DETAILS**

**MINIMUM BAR LAP**  
#5 bar = 3'-3"  
#6 bar = 3'-10"

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	CHECKED - MDS	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUPERSTRUCTURE DETAILS  
STRUCTURE NO. 045-9942**  
SHEET NO. 13 OF 34 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	84
			CONTRACT NO. 63859	
ILLINOIS FED. AID PROJECT				



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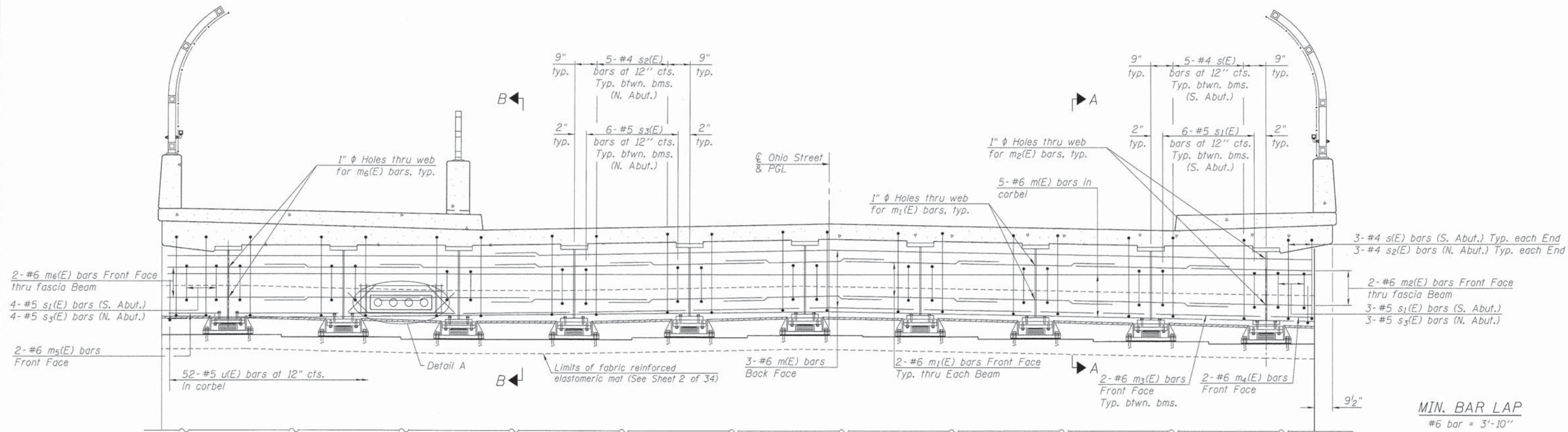
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DIAPHRAGM DETAILS  
STRUCTURE NO. 045-9942**

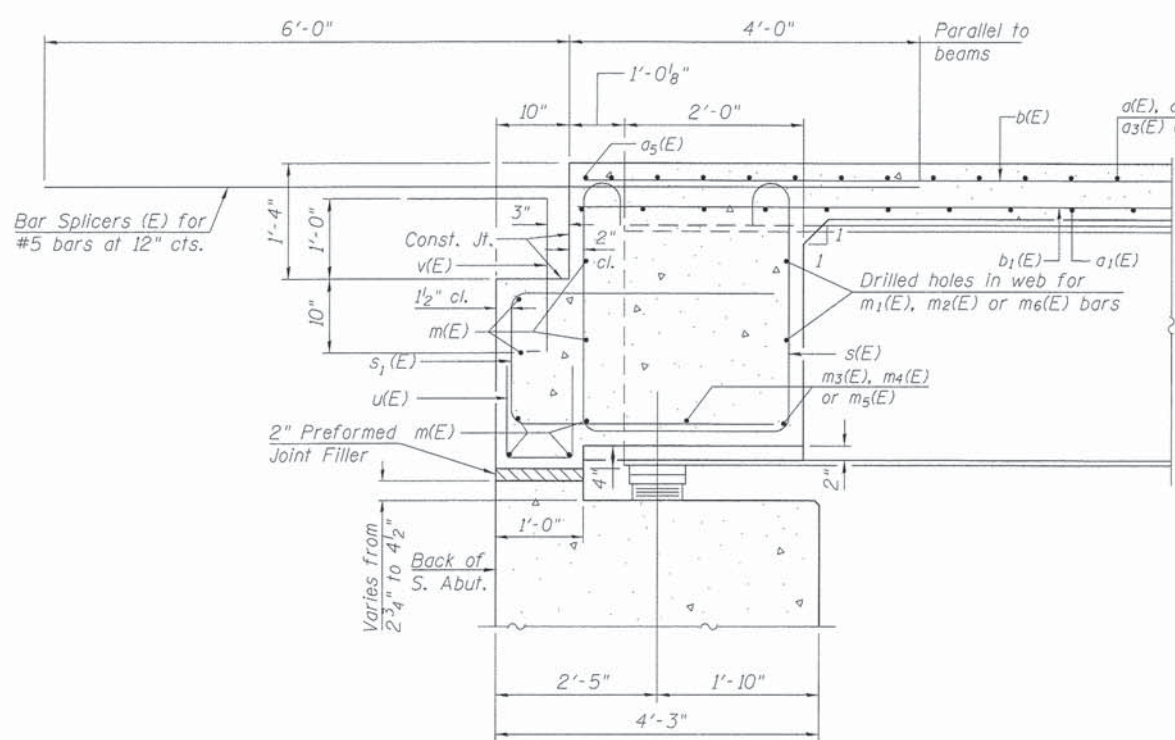
SHEET NO. 14 OF 34 SHEETS

F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 85
			CONTRACT NO. 63859	

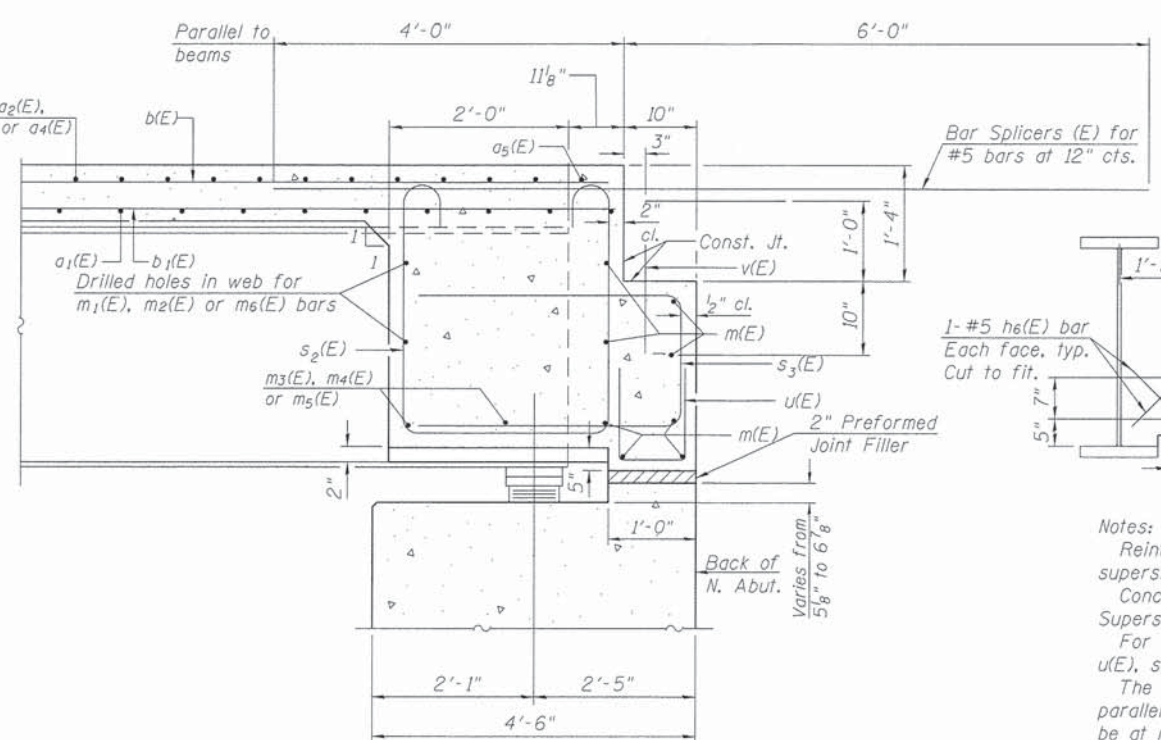
[ILLINOIS] FED. AID PROJECT



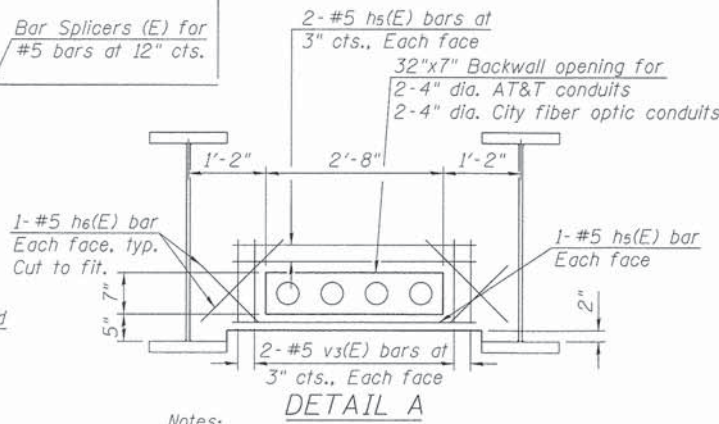
**DIAPHRAGM ELEVATION**  
(North Abutment shown, South Abutment similar)



**SECTION A-A**  
(At South Abutment)

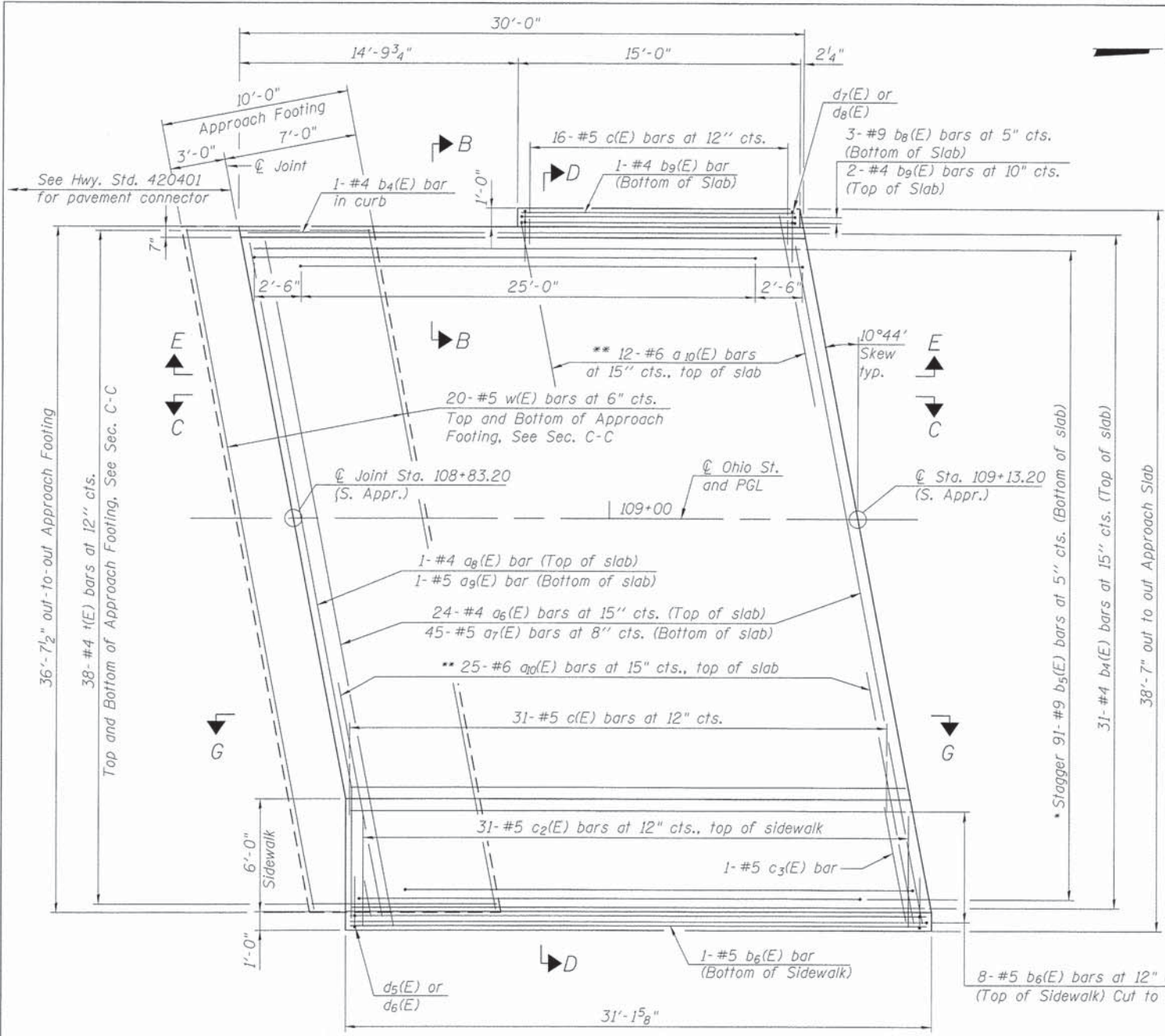


**SECTION B-B**  
(At North Abutment)



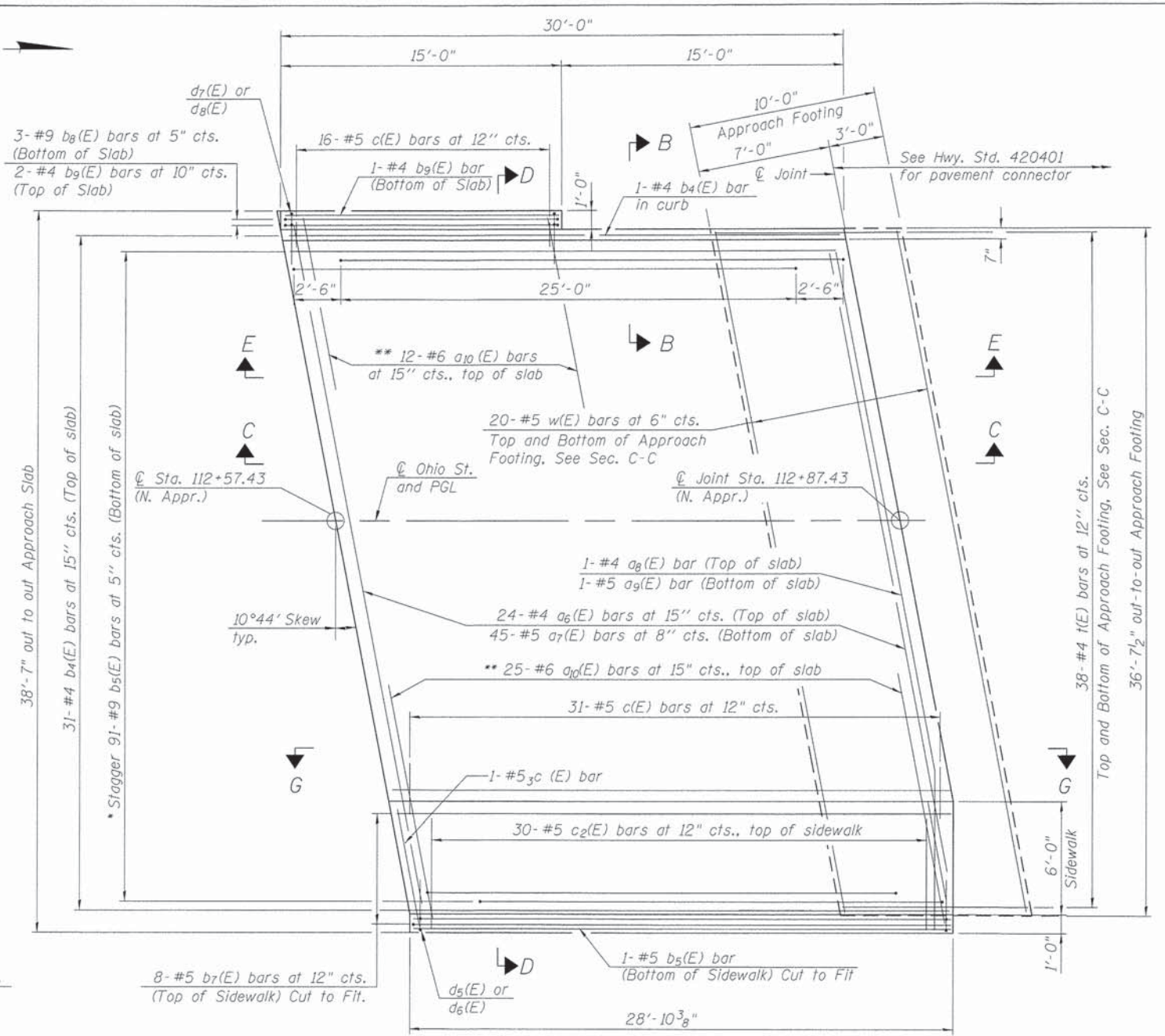
**Notes:**  
 Reinforcement bars in diaphragm are billed with superstructure on sheet 13 of 34.  
 Concrete in diaphragm is included with Concrete Superstructure on sheet 13 of 34.  
 For details of bars s(E), s1(E), s2(E), s3(E), v(E) & u(E), see sheet 13 of 34.  
 The s(E), s1(E), s2(E) and s3(E) bars shall be placed parallel to the beams. Spacing for these bars shall be at right angles to the beams.  
 The cost of 2" Preformed Joint Filler shall be included with Concrete Superstructure.  
 Utility sleeve installed in this Contract. Contractor to coordinate with utility owner for location and size of the utility sleeves. Cost of utility sleeves included in Concrete Superstructure. (See Utility plans).





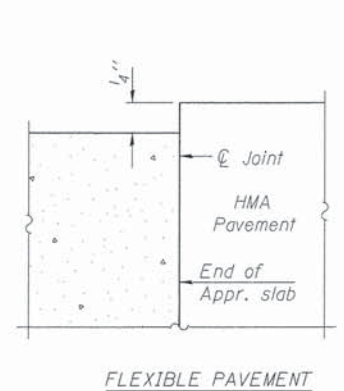
**SOUTH APPROACH PLAN**

\* Tilt #9 b<sub>5</sub>(E) bars as required to maintain clearance.  
 \*\* Space between a<sub>6</sub>(E) bars, typ. each parapet.

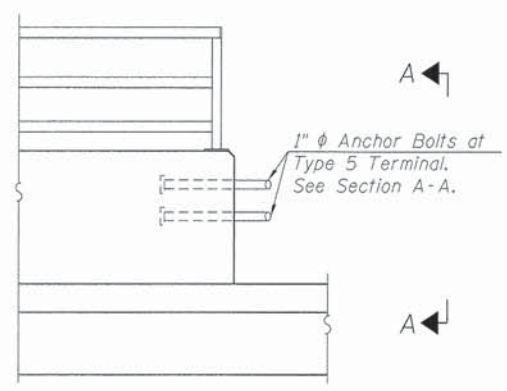


**NORTH APPROACH PLAN**

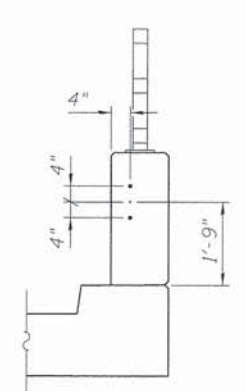
\* Tilt #9 b<sub>5</sub>(E) bars as required to maintain clearance.  
 \*\* Space between a<sub>6</sub>(E) bars, typ. each parapet.



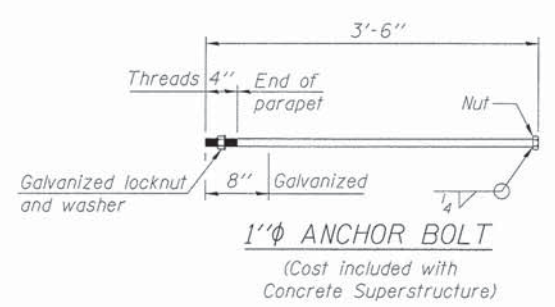
**DETAIL A**



**DETAIL B**  
(At South Approach)



**SECTION A-A**



**1" ANCHOR BOLT**  
(Cost included with Concrete Superstructure)

Notes:  
 See sheet 16 of 34 for Sections C-C & D-D and Views E-E & G-G.  
 a<sub>6</sub>(E) and a<sub>7</sub>(E) bar spacings measured along @ Rdwy.

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PLLOT DATE = 5/12/2014	DRAWN - SFW	REVISED -
	CHECKED - MDS	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**BRIDGE APPROACH SLAB DETAILS**  
**STRUCTURE NO.045-9942**

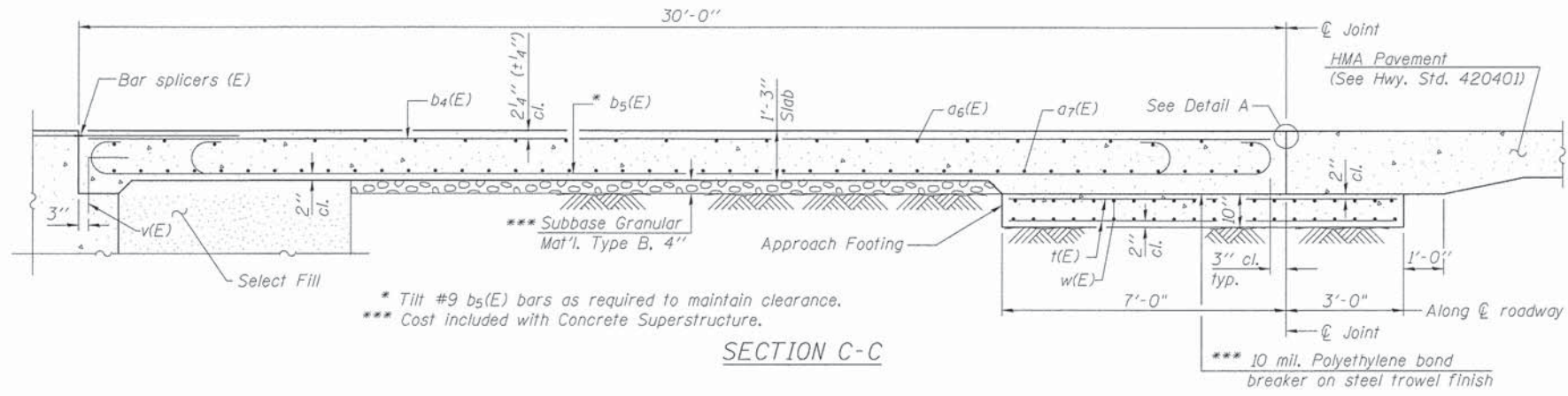
SHEET NO. 15 OF 34 SHEETS

F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 86
				CONTRACT NO. 63859
ILLINOIS FED. AID PROJECT				



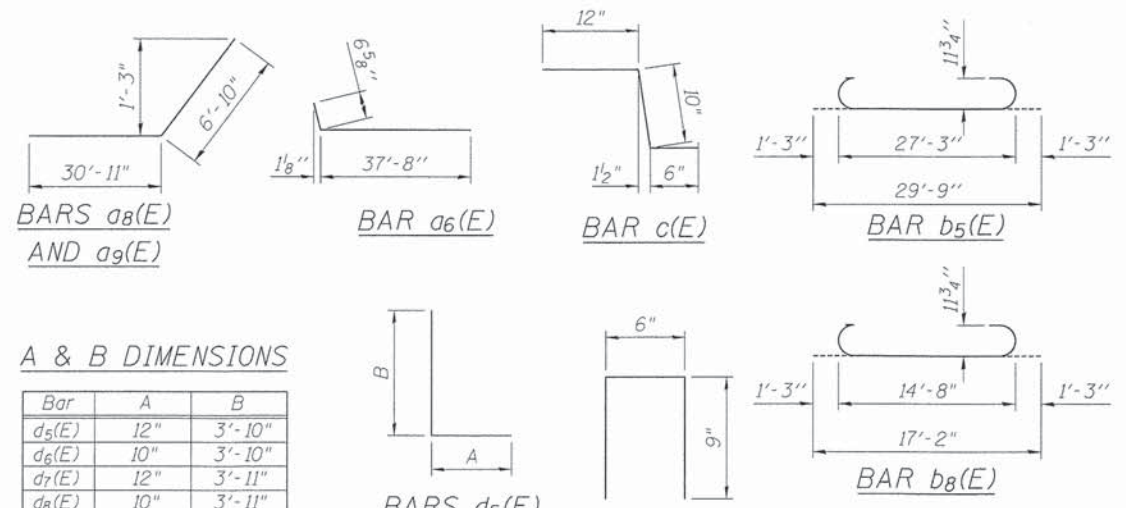
Notes:

See sheet 15 of 34 for Detail A and Detail B.  
 Approach slab and parapet concrete shall be paid for as Concrete Superstructure.  
 Approach footing concrete shall be paid for as Concrete Structures.  
 Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.  
 For v(E) bar details, see sheet 13 of 34.  
 The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.  
 For bar splicer details, see sheet 29 of 34.  
 Cost of excavation for approach footing included with Concrete Structures.  
 For additional parapet details, see sheet 15 of 34.



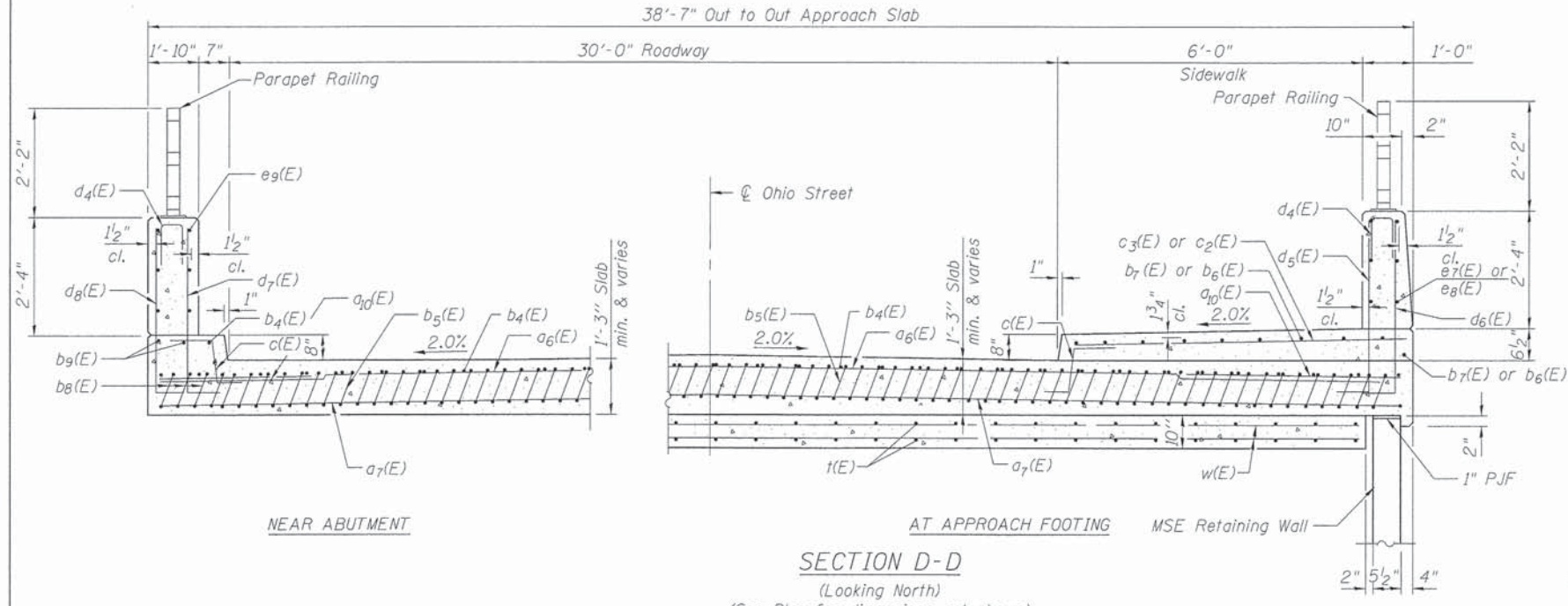
\* Tilt #9 b5(E) bars as required to maintain clearance.  
 \*\*\* Cost included with Concrete Superstructure.

SECTION C-C



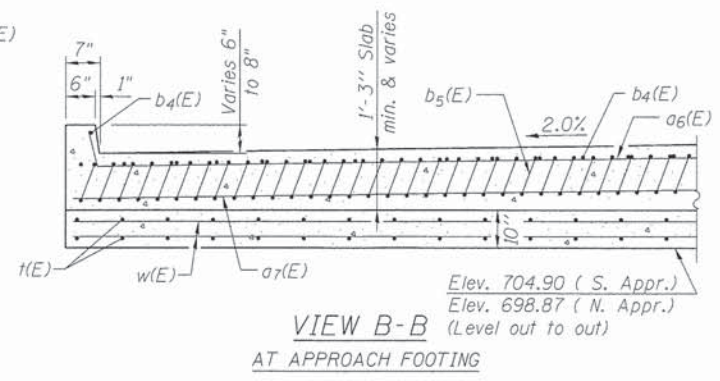
A & B DIMENSIONS

Bar	A	B
d5(E)	12"	3'-10"
d6(E)	10"	3'-10"
d7(E)	12"	3'-11"
d8(E)	10"	3'-11"



SECTION D-D  
 (Looking North)

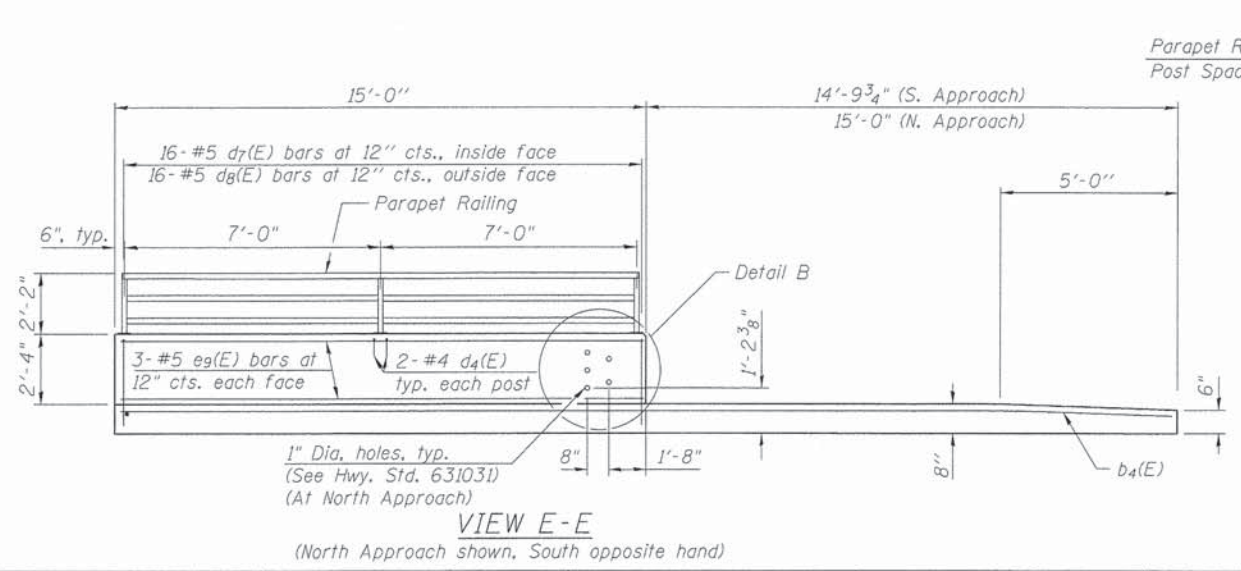
(See Plan for dimensions not shown)



VIEW B-B  
 AT APPROACH FOOTING

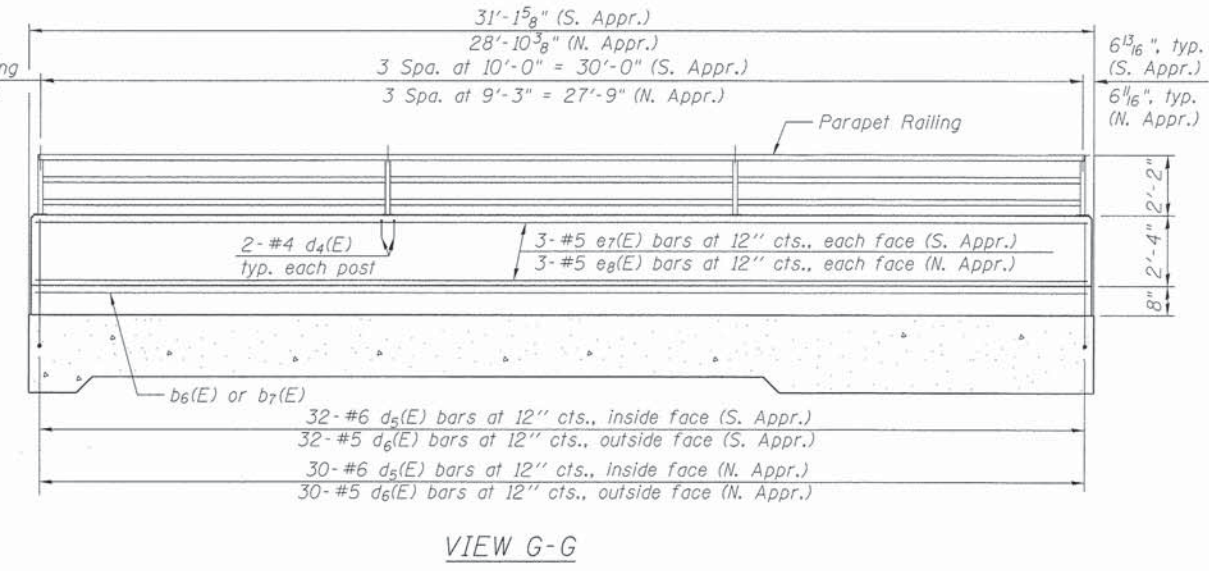
TWO APPROACHES  
 BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a6(E)	48	#4	38'-3"	—
a7(E)	90	#5	37'-11"	—
a8(E)	2	#4	37'-9"	—
a9(E)	2	#5	37'-9"	—
a10(E)	74	#6	6'-6"	—
b4(E)	64	#4	29'-8"	—
b5(E)	182	#9	29'-9"	—
b6(E)	9	#5	30'-9"	—
b7(E)	9	#5	29'-8"	—
b8(E)	6	#9	17'-2"	—
b9(E)	6	#4	14'-8"	—
c(E)	94	#5	2'-4"	—
c2(E)	61	#5	6'-7"	—
c3(E)	2	#5	6'-9"	—
d4(E)	28	#4	2'-0"	□
d5(E)	62	#6	4'-10"	—
d6(E)	62	#5	4'-8"	—
d7(E)	32	#6	4'-11"	—
d8(E)	32	#5	4'-9"	—
e7(E)	6	#5	30'-9"	—
e8(E)	6	#5	28'-6"	—
e9(E)	12	#5	14'-8"	—
t(E)	152	#4	9'-10"	—
w(E)	80	#5	36'-11"	—
Concrete Superstructure			Cu. Yd.	126.0
Concrete Structures			Cu. Yd.	23.0
Bridge Deck Grooving			Sq. Yd.	200
Protective Coat			Sq. Yd.	286
Reinforcement Bars, Epoxy Coated			Pound	32,760
Parapet Railing			Foot	90



VIEW E-E

(North Approach shown, South opposite hand)



VIEW G-G

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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

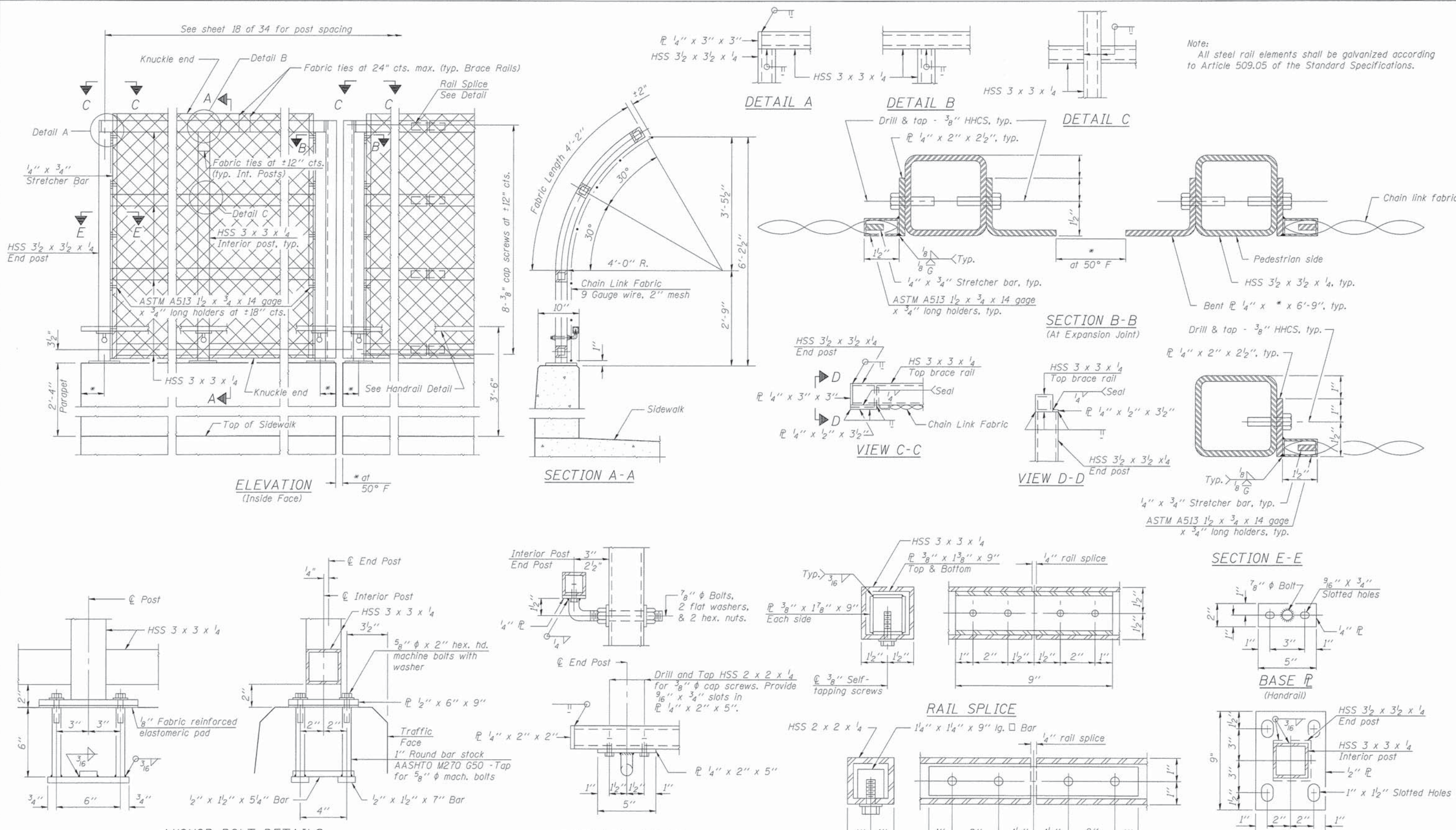
BRIDGE APPROACH SLAB DETAILS  
 STRUCTURE NO. 045-9942

SHEET NO. 16 OF 34 SHEETS

F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 87
			CONTRACT NO. 63859	
ILLINOIS FED. AID PROJECT				



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Note:  
All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.

**ANCHOR BOLT DETAILS**  
 In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 5/8" φ anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.

\*Variable - See Plans  
 (10'-0" Maximum Post Spacing)

R-32

7-1-10



USER NAME = jrmckow	DESIGNED - JRM	REVISED -
PLOT SCALE = 1/8" = 1'-0"	CHECKED - MDS	REVISED -
PLOT DATE = 5/12/2014	DRAWN - SFW	REVISED -
	CHECKED - MDS	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BRIDGE FENCE RAILING, PARAPET MOUNTED  
 STRUCTURE NO. 045-9942

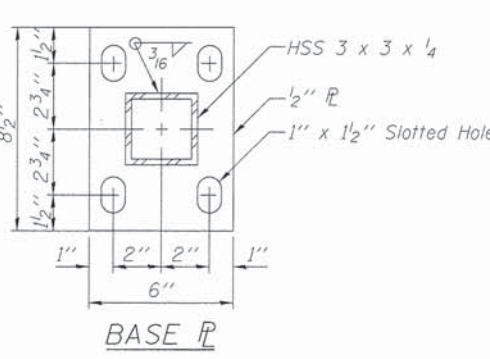
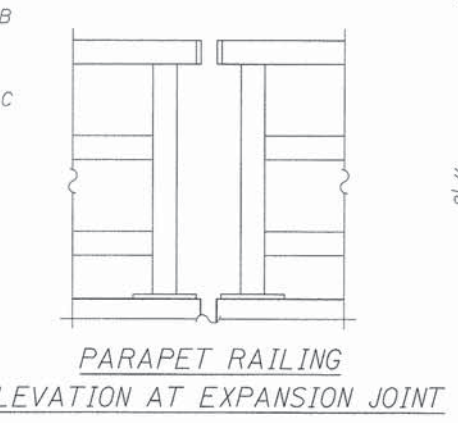
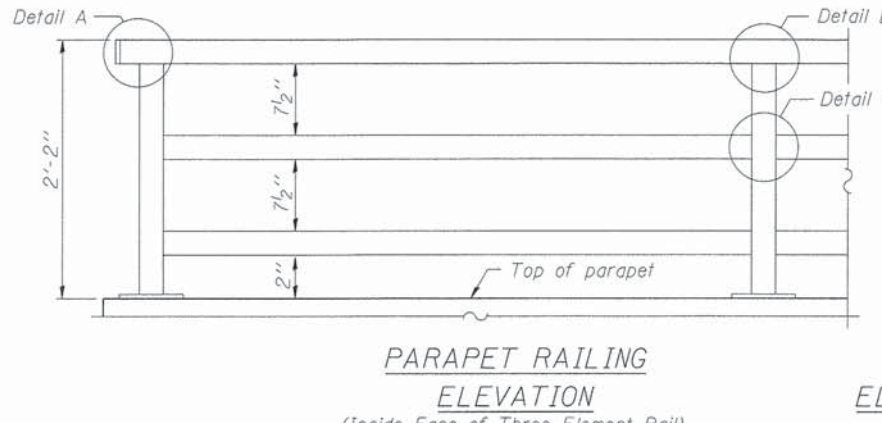
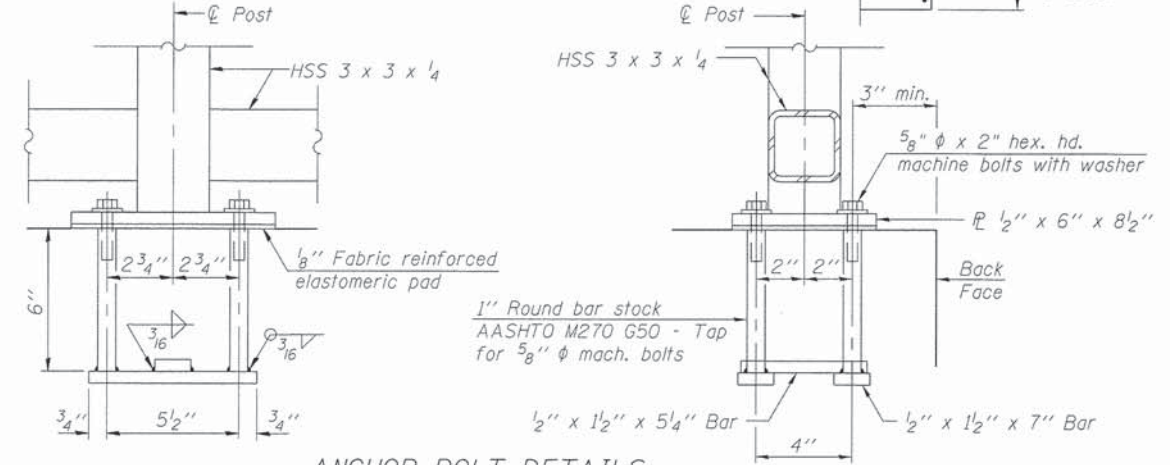
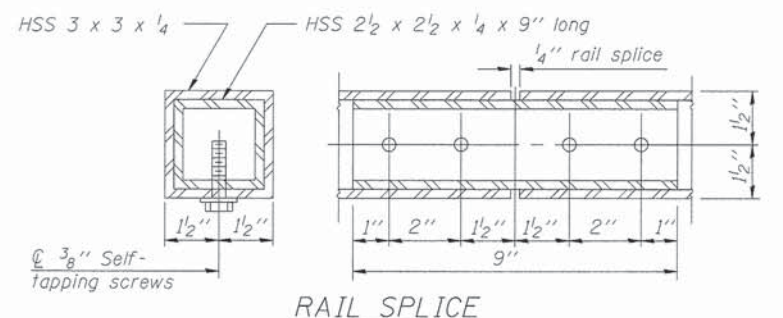
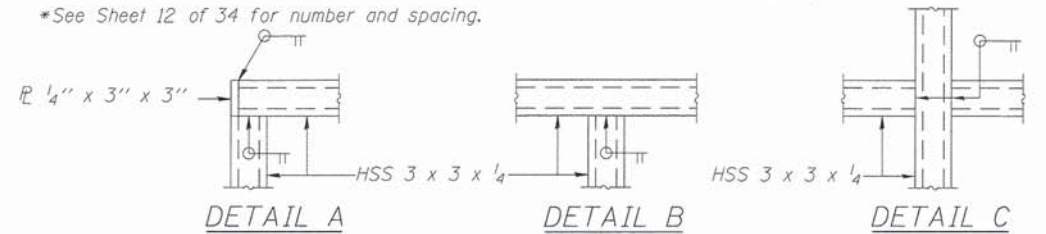
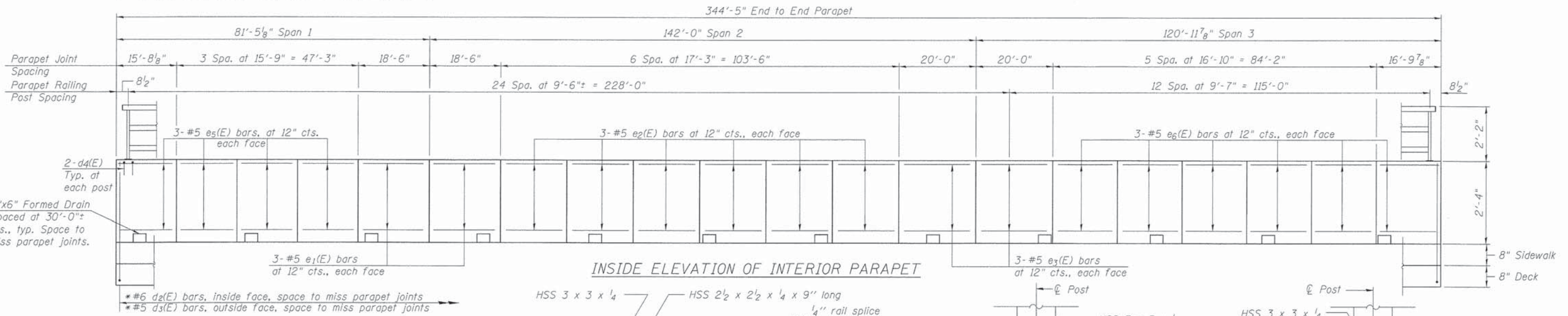
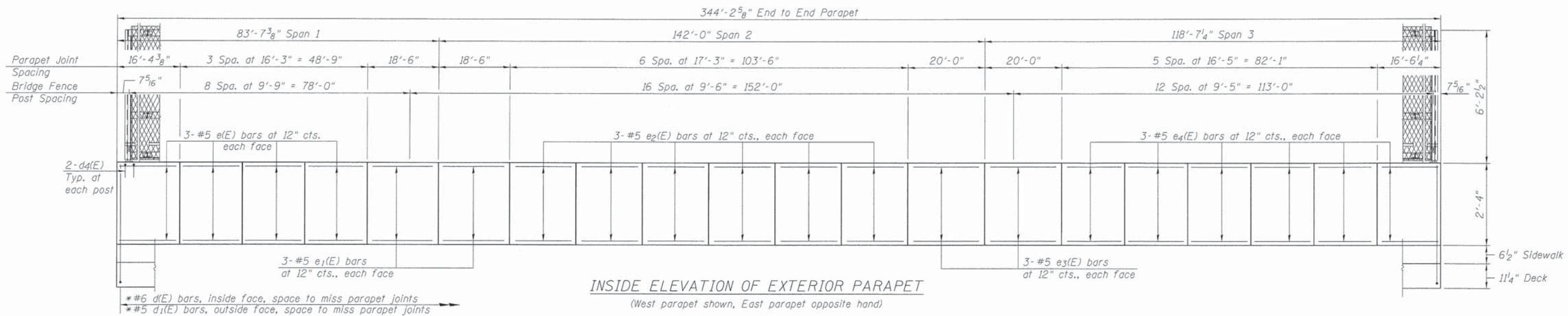
SHEET NO. 17 OF 34 SHEETS

**BILL OF MATERIAL**

Item	Unit	Quantity
Bridge Fence Railing	Foot	689

F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 88
			CONTRACT NO. 63859	
ILLINOIS FED. AID PROJECT				





In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 5/8"  $\phi$  anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.

Note:  
For Parapet Joint Details, see Sheet 13 of 34.  
All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.

**BILL OF MATERIAL**

Item	Unit	Quantity
Parapet Railing	Foot	345

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USER NAME = jrmckow	DESIGNED - JRM	REVISED -
PLLOT SCALE = 1/8" = 1'-0"	CHECKED - MDS	REVISED -
PLLOT DATE = 5/12/2014	DRAWN - SFW	REVISED -
	CHECKED - MDS	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PARAPET RAILING DETAILS  
STRUCTURE NO. 045-9942**

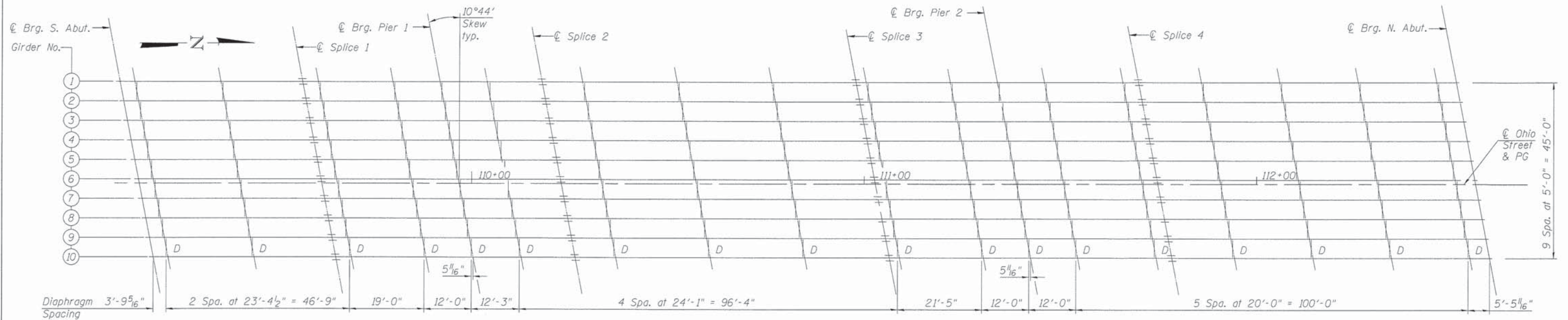
SHEET NO. 18 OF 34 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	89

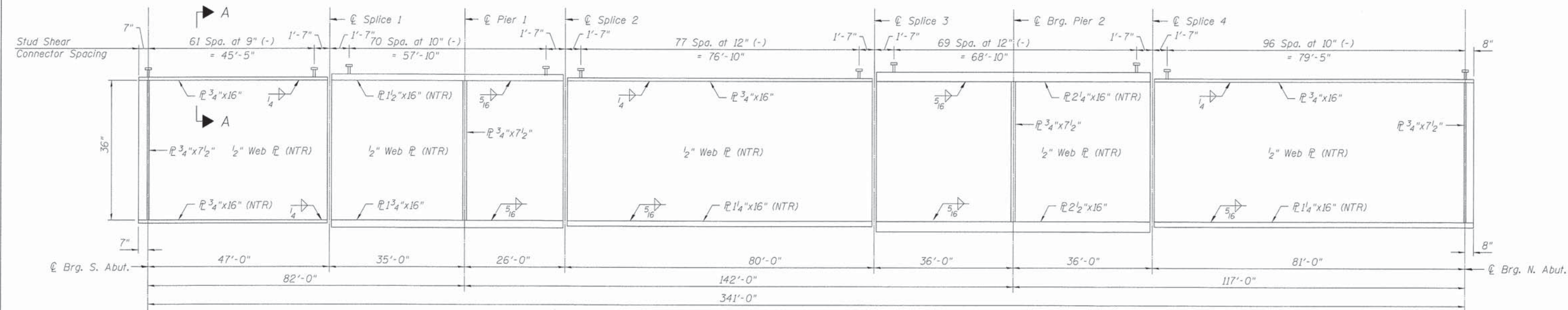
CONTRACT NO. 63859

ILLINOIS FED. AID PROJECT

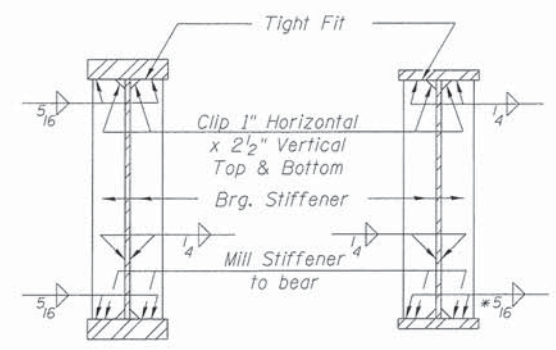




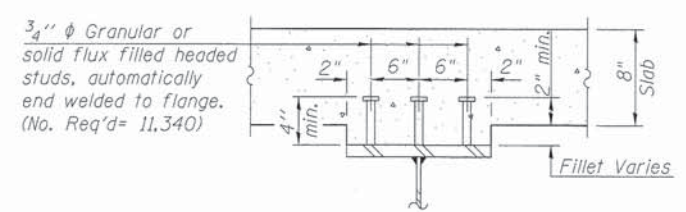
FRAMING PLAN



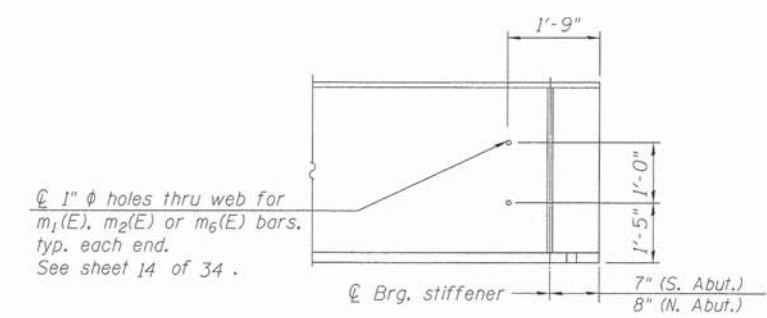
GIRDER ELEVATION



SECTION AT PIER  
SECTION AT ABUTMENT  
\* 1/4" at South Abutment



SECTION A-A



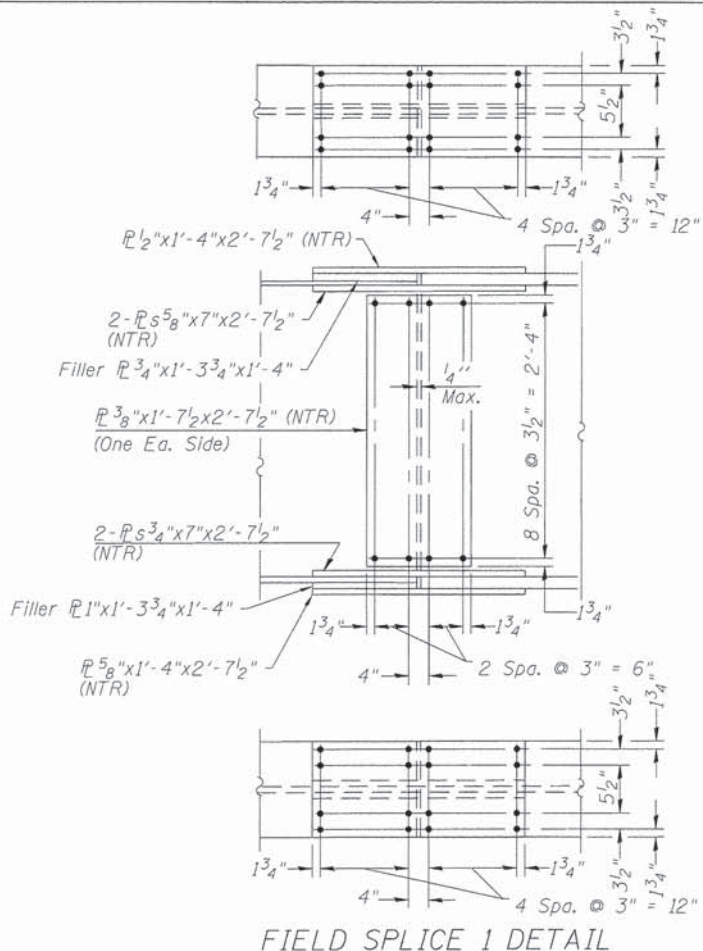
END OF BEAM ELEVATION

NOTES:

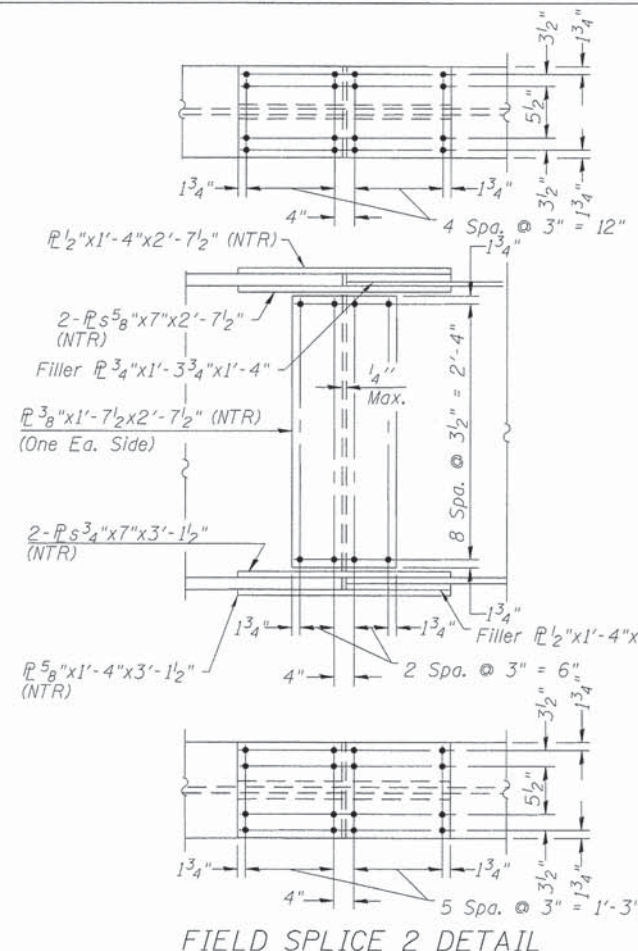
1. All plates of the girders, including bearing stiffeners and splice plates shall be AASHTO M 270, Grade 50.
2. All diaphragms, angles, fill plates and connecting plates may be AASHTO M270, Grade 36.
3. Work this sheet with sheets 20 & 21 of 34.
4. Load carrying components designated "NTR" shall conform to the Impact Testing Requirement, Zone 2.
5. All cross frames or diaphragms shall be installed as steel is erected and secured with erection pins and bolts. Individual cross frames or diaphragms at supports may be temporarily disconnected to install bearing anchor rods.

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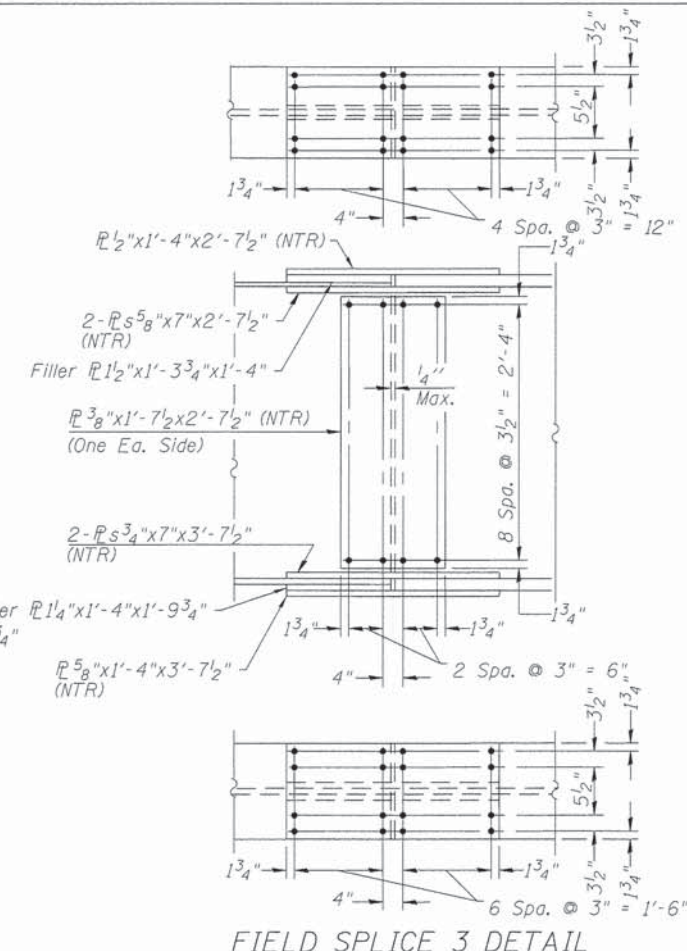




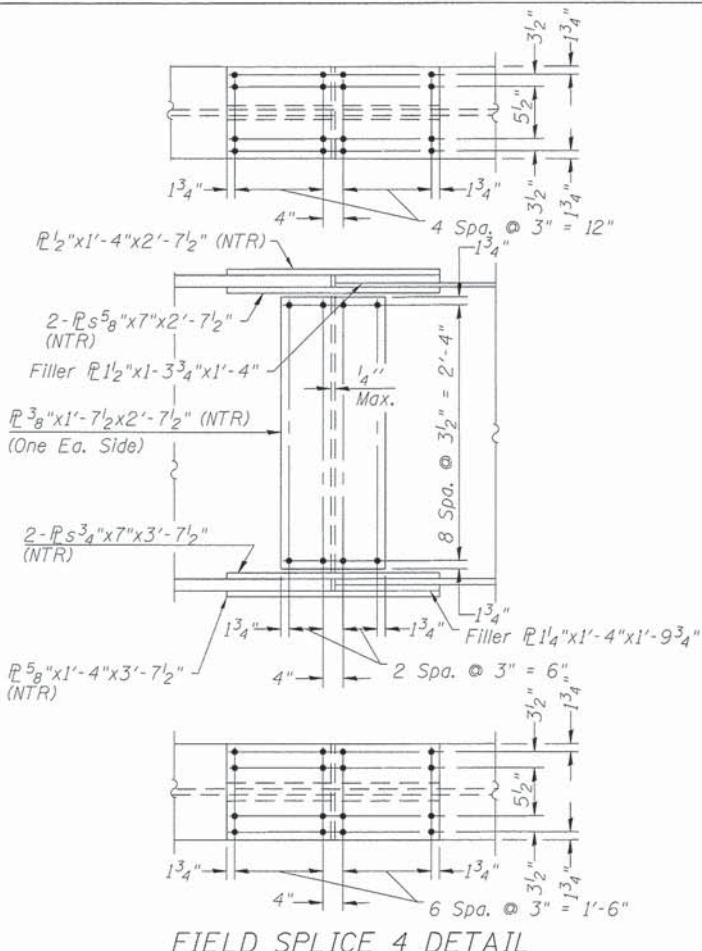
FIELD SPLICE 1 DETAIL



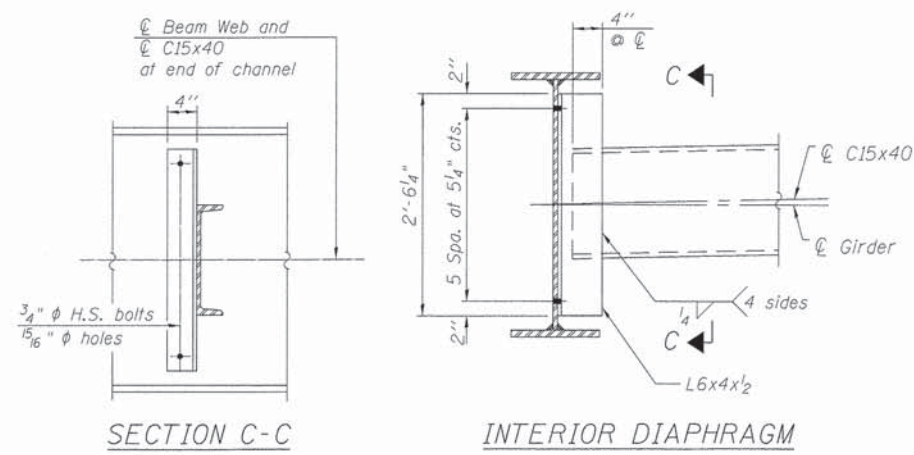
FIELD SPLICE 2 DETAIL



FIELD SPLICE 3 DETAIL



FIELD SPLICE 4 DETAIL



Note:  
 Two hardened washers required for each set of oversized holes.  
 Alternate channels C15X50 are permitted to facilitate material acquisition. Calculated weight of structural steel is based on C15x40 sections. The alternate, if utilized, shall be provided at no extra cost to the department.

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USER NAME = jrmckaw	DESIGNED - DL	REVISED -
PLOT SCALE = 1/8" = 1'-0"	CHECKED - WJC	REVISED -
PLOT DATE = 5/12/2014	DRAWN - SFW	REVISED -
	CHECKED - WJC	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**STRUCTURAL STEEL DETAILS I  
 STRUCTURE NO. 045-9942**

SHEET NO. 20 OF 34 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	91
				CONTRACT NO. 63859
[ILLINOIS] FED. AID PROJECT				



INTERIOR GIRDER MOMENT TABLE						
	0.4 Span 1	Pier 1	0.5 Span 2	Pier 2	0.6 Span 3	
$I_s$	(in <sup>4</sup> )	10,049	20,281	12,475	29,900	12,475
$I_c(n)$	(in <sup>4</sup> )	22,494	-	29,924	-	29,924
$I_c(3n)$	(in <sup>4</sup> )	16,608	-	21,293	-	21,293
$I_c(cr)$	(in <sup>4</sup> )	-	24,028	-	32,551	-
$S_s$	(in <sup>3</sup> )	536	1,089	770	1,523	770
$S_c(n)$	(in <sup>3</sup> )	715	-	1,014	-	1,014
$S_c(3n)$	(in <sup>3</sup> )	651	-	927	-	927
$S_c(cr)$	(in <sup>3</sup> )	-	1,152	-	1,565	-
DC1	(k/')	0.73	0.84	0.76	0.93	0.76
M <sub>DC1</sub>	(k)	99	1,049	524	1,704	574
DC2	(k/')	0.31	0.31	0.31	0.31	0.31
M <sub>DC2</sub>	(k)	44	432	240	650	249
DW	(k/')	0.15	0.15	0.15	0.15	0.15
M <sub>DW</sub>	(k)	21	209	116	314	121
M <sub>ℓ+IM</sub>	(k)	846	1,342	1,206	1,663	1,295
M <sub>u</sub> (Strength I)	(k)	1,691	4,513	3,240	6,324	3,477
φ <sub>r</sub> M <sub>n</sub>	(k)	3,364	5,216	4,523	7,232	4,523
f <sub>s</sub> DC1	(ksi)	2.2	11.6	8.2	13.4	9.0
f <sub>s</sub> DC2	(ksi)	0.8	4.5	3.1	5.0	3.2
f <sub>s</sub> DW	(ksi)	0.4	2.2	1.5	2.4	1.6
f <sub>s</sub> (ℓ+IM)	(ksi)	14.2	14.0	14.3	12.8	15.3
f <sub>s</sub> (Service II)	(ksi)	21.9	36.4	31.3	37.4	33.7
0.95R <sub>n</sub> F <sub>yf</sub>	(ksi)	47.5	47.5	47.5	47.5	47.5
f <sub>s</sub> (Total)(Strength I)	(ksi)	-	47.8	-	48.9	-
φ <sub>r</sub> F <sub>n</sub>	(ksi)	-	50.0	-	50.0	-
V <sub>r</sub>	(k)	26.0	29.2	-	27.4	22.1

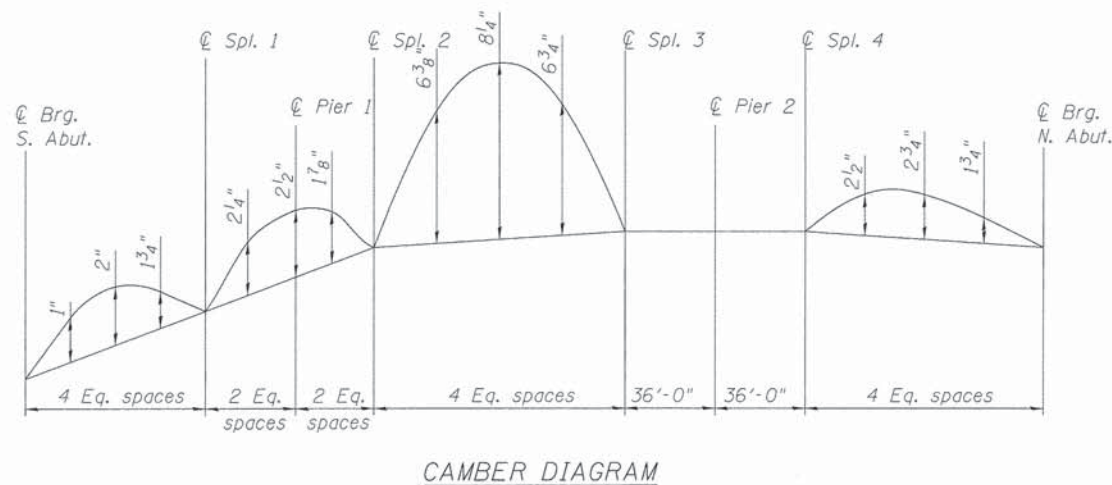
INTERIOR GIRDER REACTION TABLE					
	S. Abut.	Pier 1	Pier 2	N. Abut.	
R <sub>DC1</sub>	(k)	17.4	94.0	124.8	30.0
R <sub>DC2</sub>	(k)	7.4	38.5	47.2	12.6
R <sub>DW</sub>	(k)	3.6	18.6	22.9	6.1
R <sub>ℓ+IM</sub>	(k)	66.9	137.2	147.7	73.5
R <sub>Total</sub>	(k)	95.3	288.2	342.6	122.2

- $I_s, S_s$ : Non-composite moment of inertia and section modulus of the steel section used for computing  $f_s$  (Total-Strength I, and Service II) due to non-composite dead loads (in<sup>4</sup> and in<sup>3</sup>).
- $I_c(n), S_c(n)$ : Composite moment of inertia and section modulus of the steel and deck based upon the modular ratio, "n", used for computing  $f_s$  (Total-Strength I, and Service II) in uncracked sections due to short-term composite live loads (in<sup>4</sup> and in<sup>3</sup>).
- $I_c(3n), S_c(3n)$ : Composite moment of inertia and section modulus of the steel and deck based upon 3 times the modular ratio, "3n", used for computing  $f_s$  (Total-Strength I, and Service II) in uncracked sections, due to long-term composite (superimposed) dead loads (in<sup>4</sup> and in<sup>3</sup>).
- DC1: Un-factored non-composite dead load (kips/ft.).
- M<sub>DC1</sub>: Un-factored moment due to non-composite dead load (kip-ft.).
- DC2: Un-factored long-term composite (superimposed excluding future wearing surface) dead load (kips/ft.).
- M<sub>DC2</sub>: Un-factored moment due to long-term composite (superimposed excluding future wearing surface) dead load (kip-ft.).
- DW: Un-factored long-term composite (superimposed future wearing surface only) dead load (kips/ft.).
- M<sub>DW</sub>: Un-factored moment due to long-term composite (superimposed future wearing surface only) dead load (kip-ft.).
- M<sub>ℓ+IM</sub>: Un-factored live load moment plus dynamic load allowance (impact) (kip-ft.).
- M<sub>u</sub> (Strength I): Factored design moment (kip-ft.).  
1.25 (M<sub>DC1</sub> + M<sub>DC2</sub>) + 1.5 M<sub>DW</sub> + 1.75 M<sub>ℓ+IM</sub>
- φ<sub>r</sub>M<sub>n</sub>: Compact composite positive moment capacity computed according to Article 6.10.7.1 or non-slender negative moment capacity according to Article A6.1.1 or A6.1.2 (kip-ft.).
- f<sub>s</sub> DC1: Un-factored stress at edge of flange for controlling steel flange due to vertical non-composite dead loads as calculated below (ksi).  
M<sub>DC1</sub> / S<sub>nc</sub>
- f<sub>s</sub> DC2: Un-factored stress at edge of flange for controlling steel flange due to vertical composite dead loads as calculated below (ksi).  
M<sub>DC2</sub> / S<sub>c(3n)</sub> or M<sub>DC2</sub> / S<sub>c(cr)</sub> as applicable.
- f<sub>s</sub> DW: Un-factored stress at edge of flange for controlling steel flange due to vertical composite future wearing surface loads as calculated below (ksi).  
M<sub>DW</sub> / S<sub>c(3n)</sub> or M<sub>DW</sub> / S<sub>c(cr)</sub> as applicable.
- f<sub>s</sub> (ℓ+IM): Un-factored stress at edge of flange for controlling steel flange due to vertical composite live load plus impact loads as calculated below (ksi).  
M<sub>ℓ+IM</sub> / S<sub>c(n)</sub> or M<sub>DW</sub> / S<sub>c(cr)</sub> as applicable.
- f<sub>s</sub> (Service II): Sum of stresses as computed below (ksi).  
f<sub>s</sub>DC1 + f<sub>s</sub>DC2 + f<sub>s</sub>DW + 1.3 f<sub>s</sub>(ℓ+IM)
- 0.95R<sub>n</sub>F<sub>yf</sub>: Composite stress capacity for Service II loading according to Article 6.10.4.2 (ksi).
- f<sub>s</sub> (Total)(Strength I): Sum of stresses as computed below on non-compact section (ksi).  
1.25 (f<sub>s</sub>DC1 + f<sub>s</sub>DC2) + 1.5 f<sub>s</sub>DW + 1.75 f<sub>s</sub>(ℓ+IM)
- φ<sub>r</sub>F<sub>n</sub>: Non-Compact composite positive or negative stress capacity for Strength I loading according to Article 6.10.7 or 6.10.8 (ksi).
- V<sub>r</sub>: Maximum factored shear range in span computed according to Article 6.10.10.

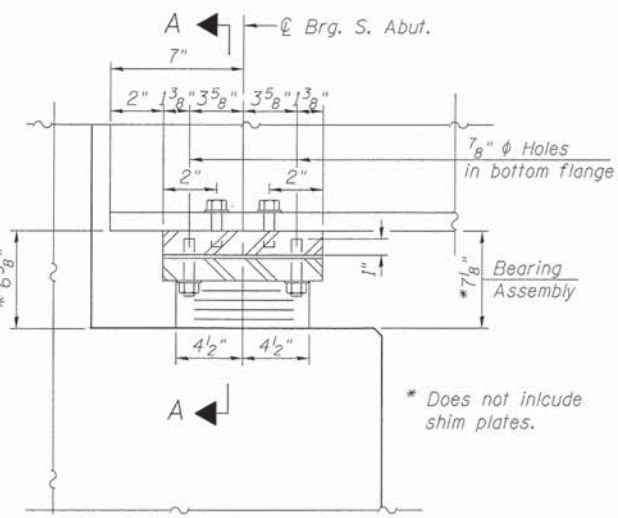
\* TOP OF WEB ELEVATION

Beam	S. Abut.	Splice 1	Pier 1	Splice 2	Splice 3	Pier 2	Splice 4	N. Abut.
1	707.73	709.80	710.76	711.11	709.66	707.92	706.18	702.19
2	707.78	709.83	710.78	711.11	709.62	707.87	706.12	702.15
3	707.83	709.87	710.79	711.12	709.58	707.82	706.07	702.10
4	707.95	709.98	710.89	711.20	709.61	707.86	706.10	702.13
5	708.10	710.11	711.00	711.30	709.67	707.91	706.15	702.18
6	708.25	710.24	711.11	711.40	709.73	707.96	706.20	702.24
7	708.24	710.21	711.07	711.34	709.62	707.86	706.09	702.13
8	708.18	710.15	710.98	711.24	709.48	707.71	705.93	701.98
9	708.13	710.08	710.90	711.14	709.34	707.56	705.78	701.84
10	708.16	710.08	710.89	711.12	709.27	707.49	705.71	701.77

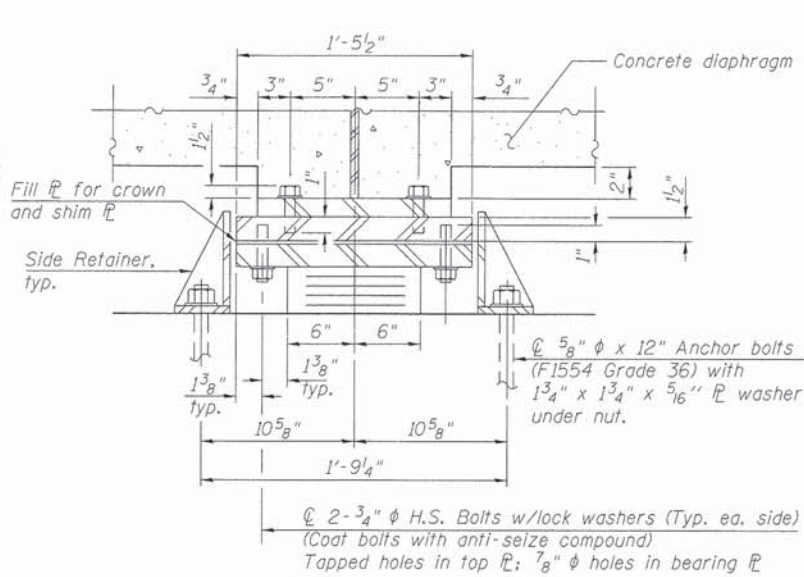
\* For fabrication use only.



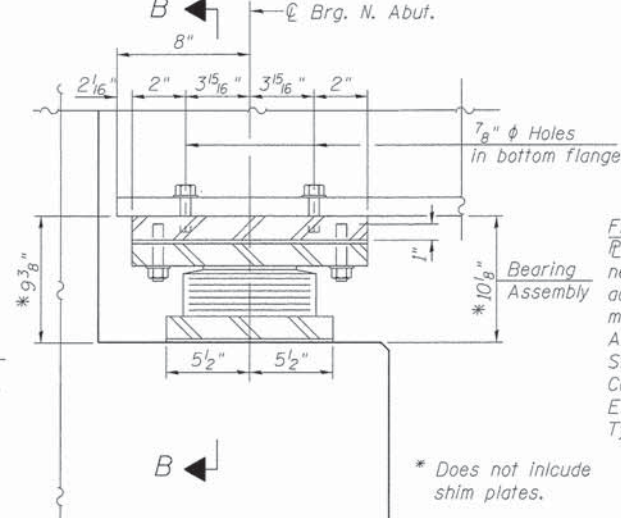




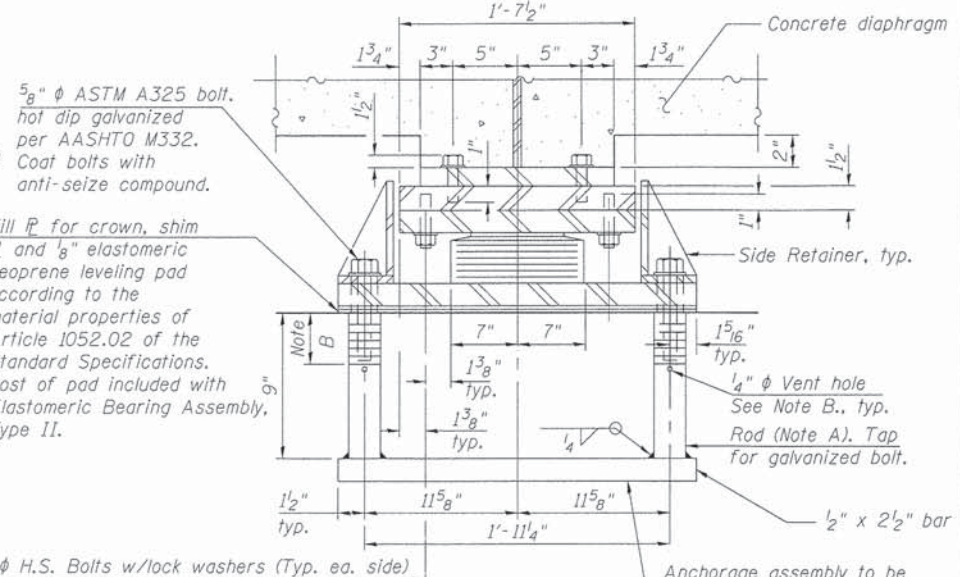
ELEVATION AT ABUT.



SECTION A-A



ELEVATION AT ABUT.



SECTION B-B

**TYPE I ELASTOMERIC EXP. BRG.**  
(at South Abutment)

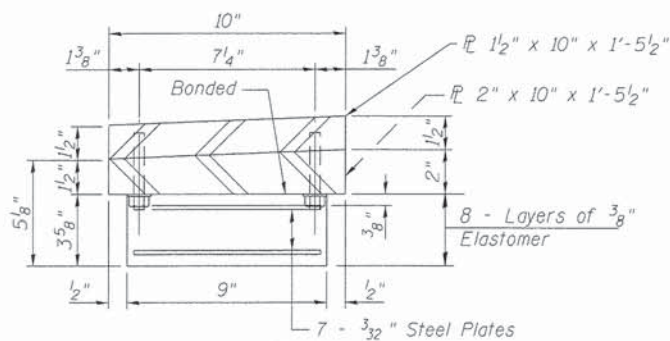
**TYPE II ELASTOMERIC EXP. BRG.**  
(at North Abutment)

Note A:  
AASHTO M270 G50 or G50W or similar material.  
Rod dia. = 1 3/8" φ

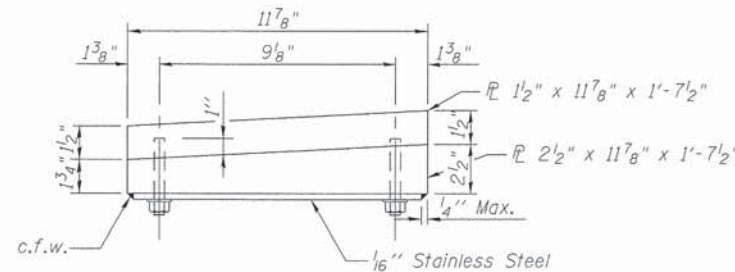
Note B:  
Bolt engagement 1 1/4" min., 1 5/8" max., allowing up to 3/8"  
adjustment shims. Tap full threads in rod 1 3/4" deep.  
Provide 1/4" φ galvanizing vent hole below full thread.

**SHIM PLATE THICKNESS TABLE**

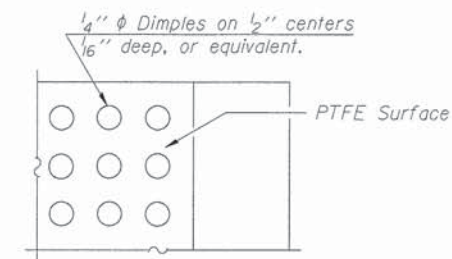
Location	G1	G2	G3	G4	G5	G6	G7	G8	G9	G10
S. Abut.	1/4"	1/8"	-	-	-	1/8"	-	-	1/8"	1/2"
N. Abut.	-	5/8"	-	3/8"	-	-	-	-	-	-



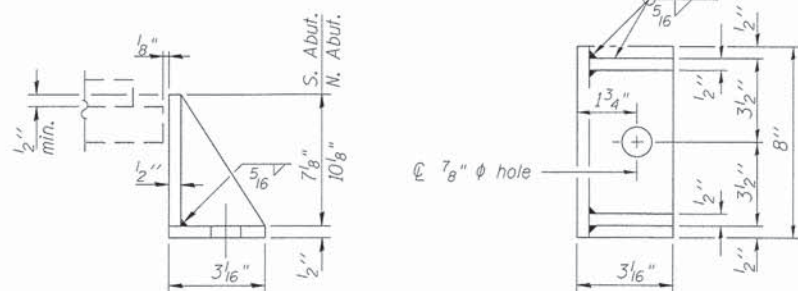
BEARING ASSEMBLY



TOP BEARING ASSEMBLY

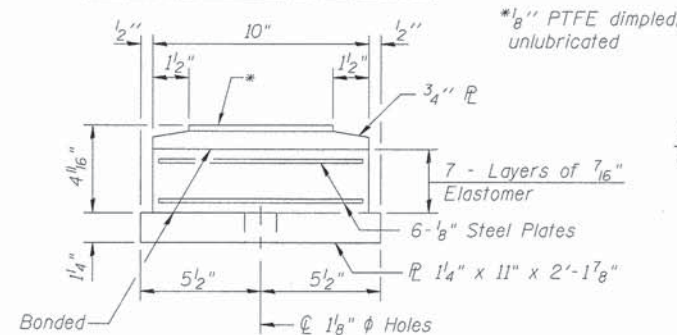


PLAN-PTFE SURFACE

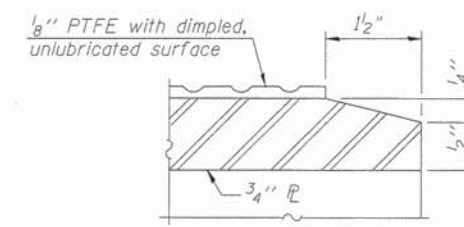


SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



BOTTOM BEARING ASSEMBLY



SECTION THRU PTFE

Notes:

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Anchor bolts for side retainers for Type I bearings may be cast in place or installed in holes drilled before or after members are in place.

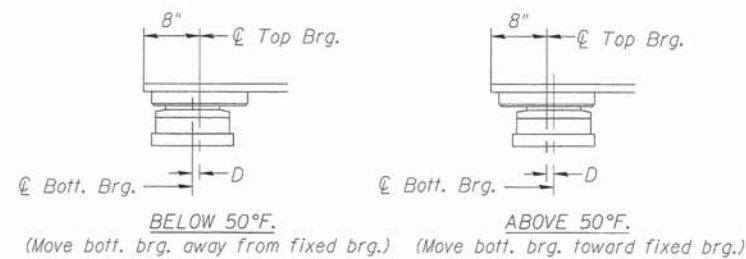
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Side retainers and other steel members required for the elastomeric bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I or Elastomeric Bearing Assembly, Type II.

Two 1/8" in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.

The 1/8" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.

Banding of 1/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.



SETTING ANCHOR BOLTS AT EXP. BRG.

D = 1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.

**BILL OF MATERIAL**

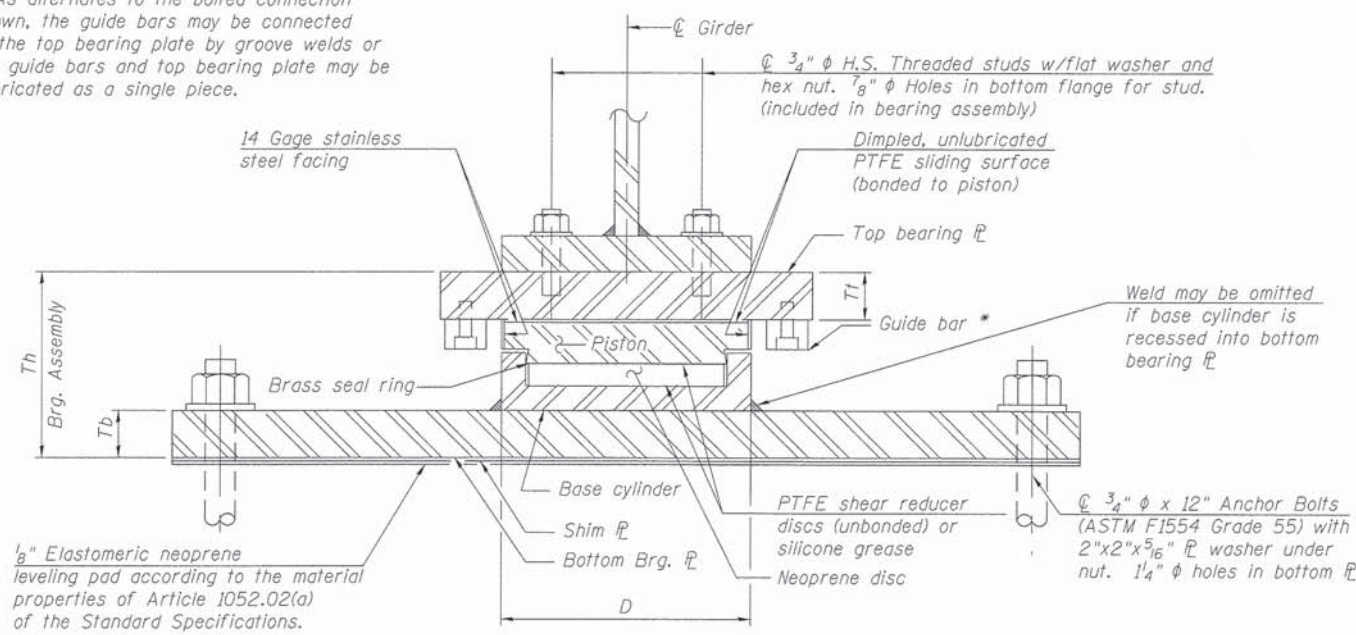
Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	10
Elastomeric Bearing Assembly, Type II	Each	10
Anchor Bolts, 5/8"	Each	20

USER NAME = jrmakow	DESIGNED - JRM	REVISED -
PLOT SCALE = 1/8" = 1' / in.	CHECKED - WJC	REVISED -
PLOT DATE = 5/12/2014	DRAWN - SFW	REVISED -
	CHECKED - MDS	REVISED -

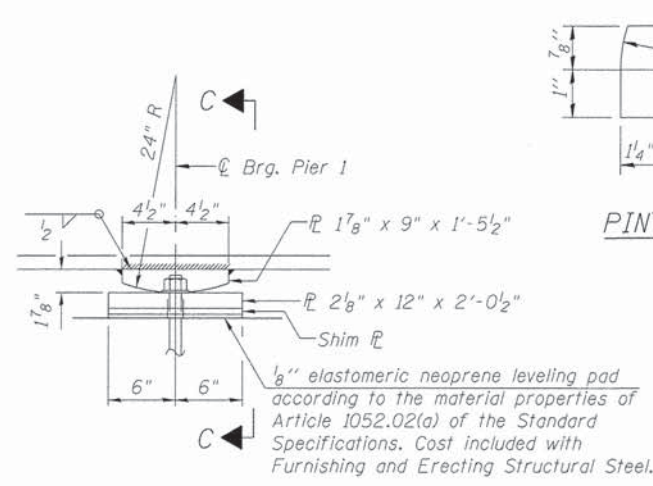
F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 93
				CONTRACT NO. 63859
ILLINOIS FED. AID PROJECT				



\* As alternates to the bolted connection shown, the guide bars may be connected to the top bearing plate by groove welds or the guide bars and top bearing plate may be fabricated as a single piece.

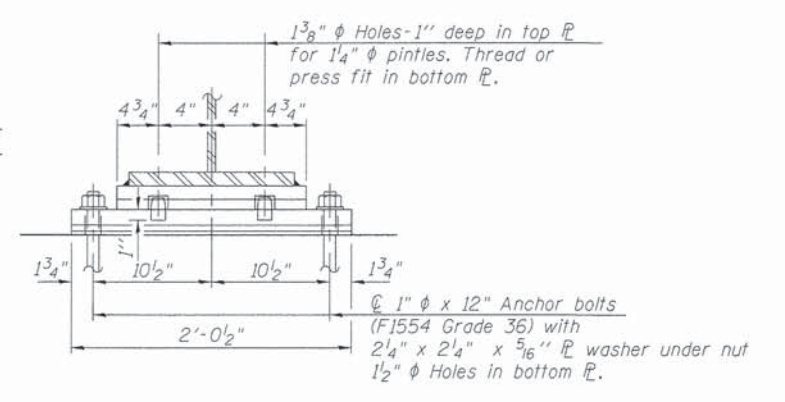


**GUIDED EXPANSION HLMR BEARING**  
(at Pier 2)

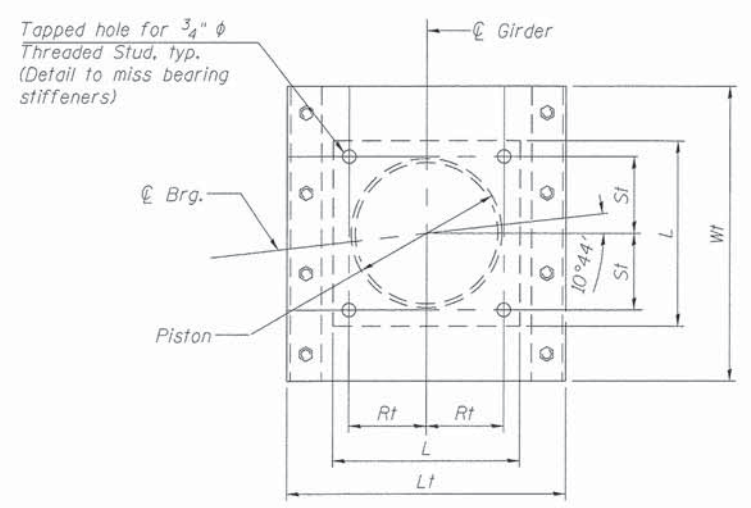


**ELEVATION AT PIER**

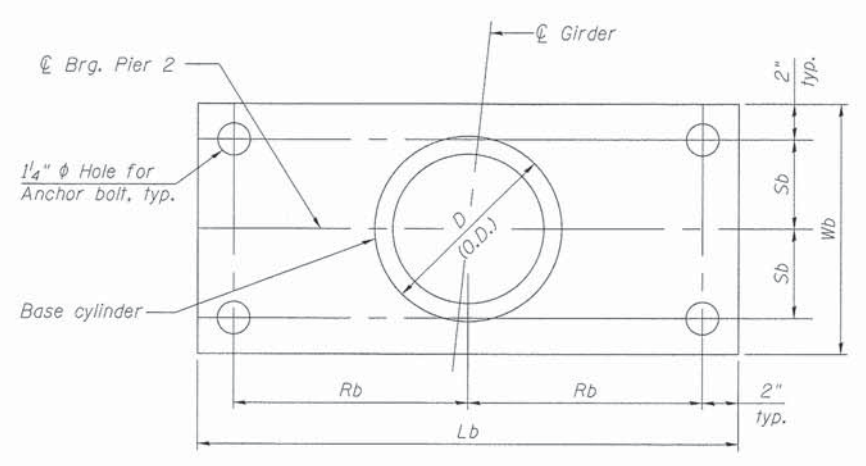
**FIXED BEARING**  
(at Pier 1)



**SECTION C-C**



**TOP BEARING PLATE AND PISTON PLAN**



**BOTTOM BEARING PLATE AND  
BASE CYLINDER PLAN**

**SHIM PLATE THICKNESS TABLE**

Location	G1	G2	G3	G4	G5	G6	G7	G8	G9	G10
Pier 1	-	1/4"	3/8"	-	-	-	1/4"	-	1/8"	-
Pier 2	-	1/8"	1/2"	1/4"	1/8"	-	-	-	-	-

**Notes:**  
 Cost of top and bottom bearing plates, 1/8" elastomeric neoprene leveling pad, adjusting shims and threaded studs with washers at Pier 2 shall be included with High Load Multi-Rotational Bearings, Guided Expansion, 400 kips.  
 Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternative material) of the grade and diameter specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.  
 Anchor bolts for HLMR bearings shall be placed in holes drilled in the concrete through holes in the bottom bearing plate after members are in place. Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.  
 Two 1/8" adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.  
 The 1/8" PTFE sheet be bonded directly to the piston with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.  
 Anchor bolts at fixed bearings may be either cast in place or installed in holes drilled before or after the supported member is in place.

**HIGH LOAD MULTI-ROTATIONAL BEARINGS SCHEDULE**

Location	Quantity Each	Service Vertical Design Load * (kips)	Lateral design Load (kips)	Total Required Movement (in)	Rotation (radians) **	D (in)	L (in)	Th (in)	Top Plate / Bearing Assembly					Masonry Plate				
									Wt (in)	Lt (in)	Tt (in)	Rt (in)	St (in)	Wb (in)	Lb (in)	Tb (in)	Rb (in)	Sb (in)
Pier 2	10	320	64	3/4"	0.006	12.25"	13"	9 1/2"	22"	19"	2"	4 1/2"	4 1/2"	20"	27"	2"	11 1/2"	8"

\* No Impact  
 \*\* Maximum Factored Ultimate (Strength) Design Rotation

**BILL OF MATERIAL**

Item	Unit	Total
High Load Multi-Rotational Bearings, Guided Expansion, 400 k	Each	10
Anchor Bolts, 3/4"	Each	40
Anchor Bolts, 1"	Each	20

5/12/2014 5:35:30 PM - G:\CH\10130\B\Fig\CA00\Sheet\0459942-63859-023-BRG2.dgn



USER NAME = jrmckow	DESIGNED - JRM	REVISED -
PLLOT SCALE = 1:8 1/2" = 1"	CHECKED - WJC	REVISED -
PLLOT DATE = 5/12/2014	DRAWN - SFW	REVISED -
	CHECKED - MDS	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

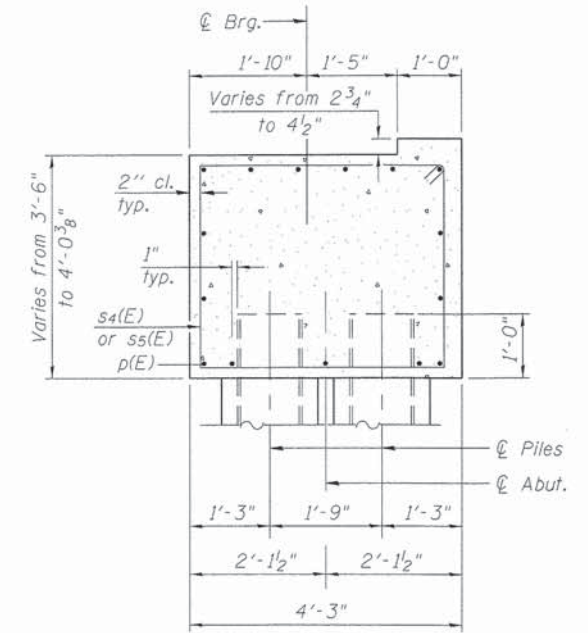
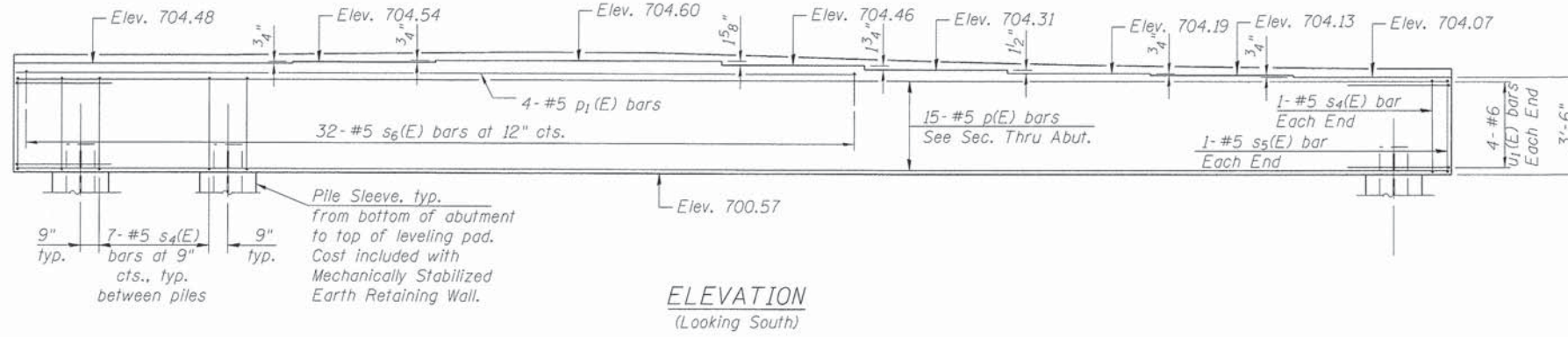
**PIER BEARING DETAILS  
STRUCTURE NO. 045-9942**

SHEET NO. 23 OF 34 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	94
CONTRACT NO. 63859				
ILLINOIS FED. AID PROJECT				



Notes:  
Pour steps monolithically with cap.

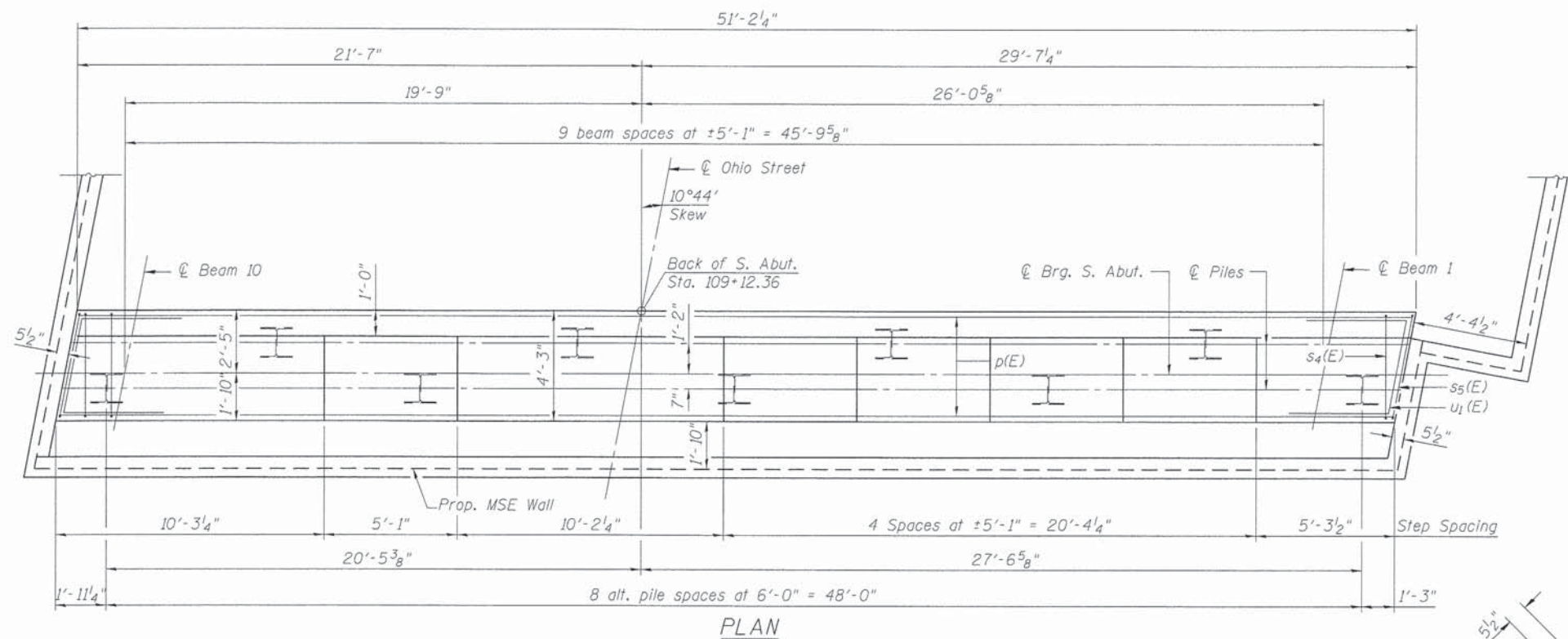


SEC. THRU ABUT.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
p(E)	15	#5	50'-10"	—
p1(E)	4	#5	29'-11"	—
s4(E)	58	#5	15'-1"	□
s5(E)	2	#5	15'-3"	□
s6(E)	32	#5	4'-11"	□
u1(E)	8	#6	11'-8"	┘
Structure Excavation			Cu. Yd.	36
Concrete Structures			Cu. Yd.	30.7
Reinforcement Bars, Epoxy Coated			Pound	2,170
Furnishing Steel Piles HP 14x73			Foot	560
Driving Piles			Foot	560
Test Pile Steel HP 14x73			Each	1
Geocomposite Wall Drain			Sq. Yd.	40

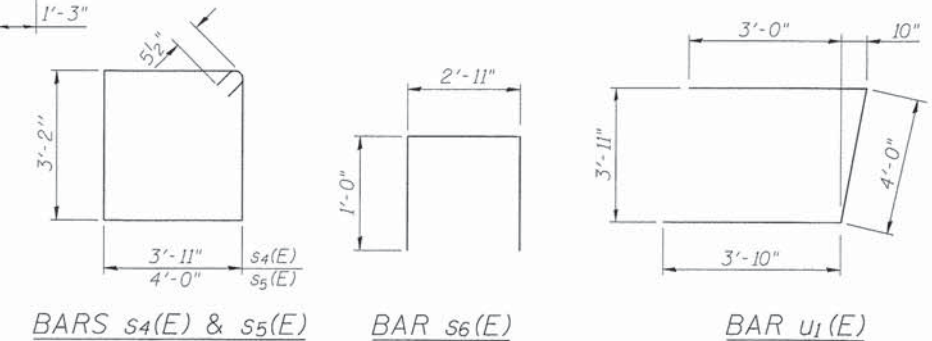
For details of piles see sheet 28 of 34.



PLAN

PILE DATA

Type: Steel HP 14x73  
Nominal Required Bearing: 578 k  
Factored Resistance Available: 318 k  
Est. Length: 70'  
No. Production Piles: 8  
No. Test Piles: 1



BARS s4(E) & s5(E)

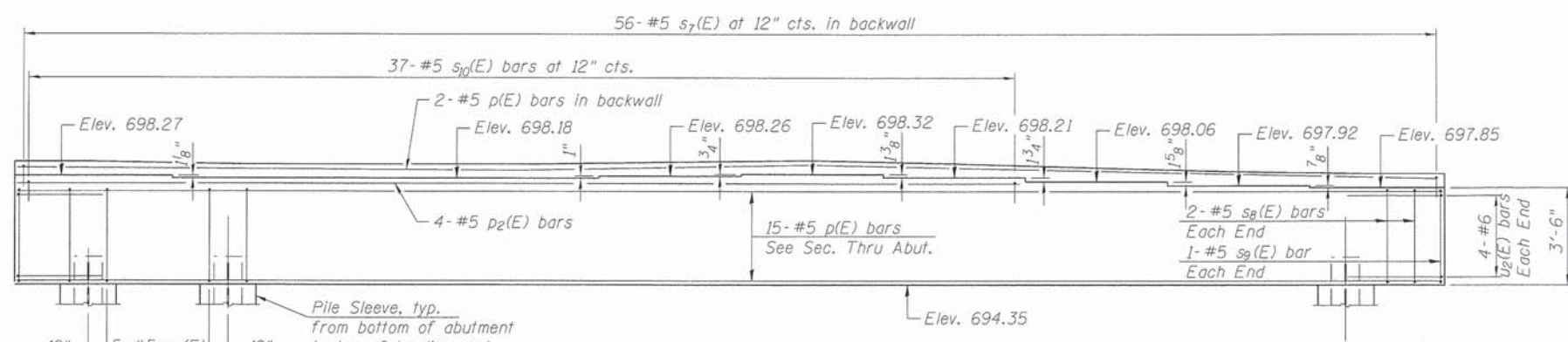
BAR s6(E)

BAR u1(E)

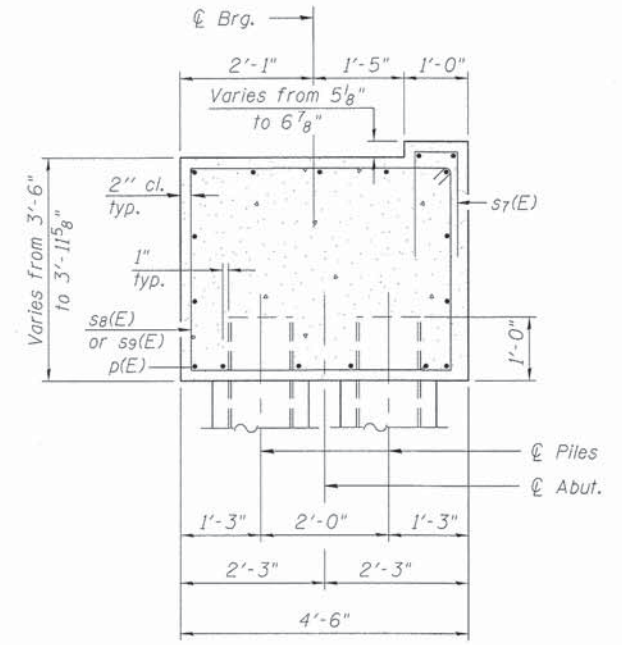
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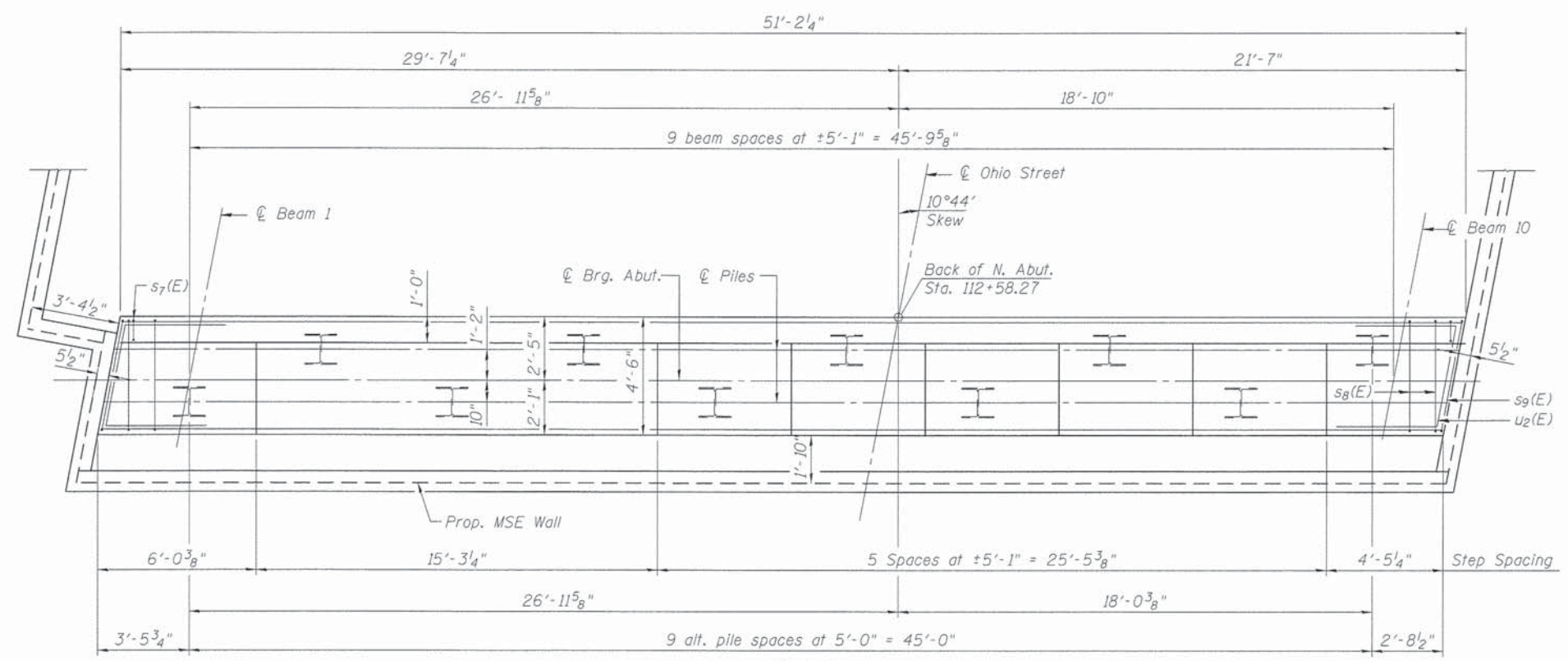
Notes:  
Pour steps monolithically with cap.



ELEVATION  
(Looking North)



SEC. THRU ABUT.



PLAN

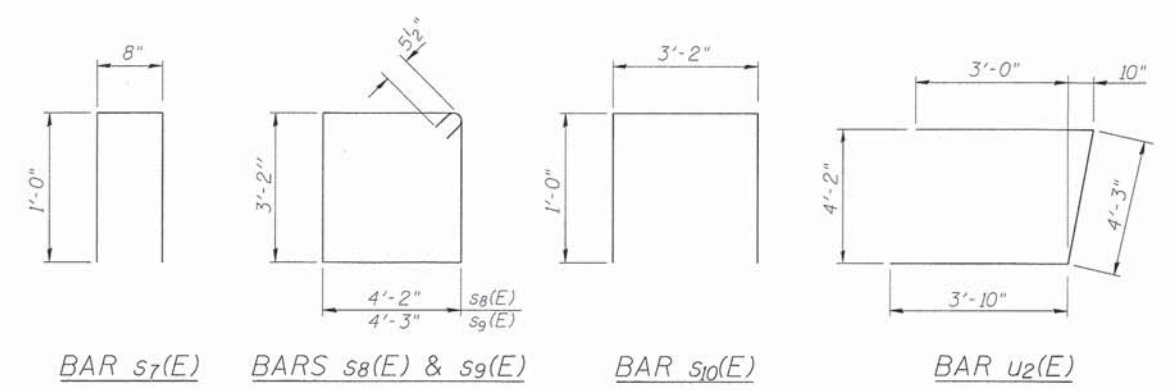
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
p(E)	17	#5	50'-10"	—
p2(E)	4	#5	35'-0"	—
s7(E)	52	#5	2'-8"	□
s8(E)	49	#5	15'-7"	□
s9(E)	2	#5	15'-9"	□
s10(E)	37	#5	5'-2"	□
u2(E)	8	#6	11'-11"	┘
Structure Excavation		Cu. Yd.	43	
Concrete Structures		Cu. Yd.	32.8	
Reinforcement Bars, Epoxy Coated		Pound	2,370	
Furnishing Steel Piles HP 14x73		Foot	540	
Driving Piles		Foot	540	
Test Pile Steel HP 14x73		Each	1	
Geocomposite Wall Drain		Sq. Yd.	41	

For details of piles see sheet 28 of 34.

PILE DATA

Type: Steel HP 14x73  
Nominal Required Bearing: 578 k  
Factored Resistance Available: 318 k  
Est. Length: 60'  
No. Production Piles: 9  
No. Test Piles: 1



5/2/2014 5:35:31 PM G:\CHIV\030\Bridges\CADD\Sheets\0459942-63859-025-NABUT.dgn



USER NAME = jrmuskew	DESIGNED - JRM	REVISED -
PLOT SCALE = 3/8" = 1'	CHECKED - MDS	REVISED -
PLOT DATE = 5/12/2014	DRAWN - RLS	REVISED -
	CHECKED - MDS	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

NORTH ABUTMENT  
STRUCTURE NO. 045-9942

SHEET NO. 25 OF 34 SHEETS

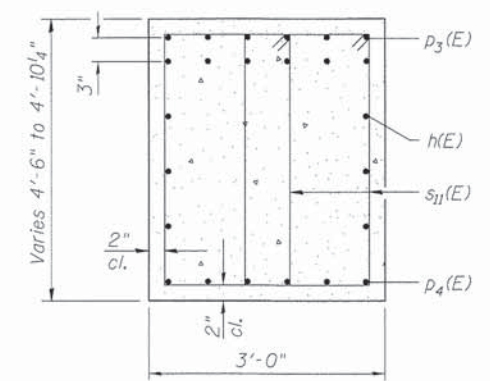
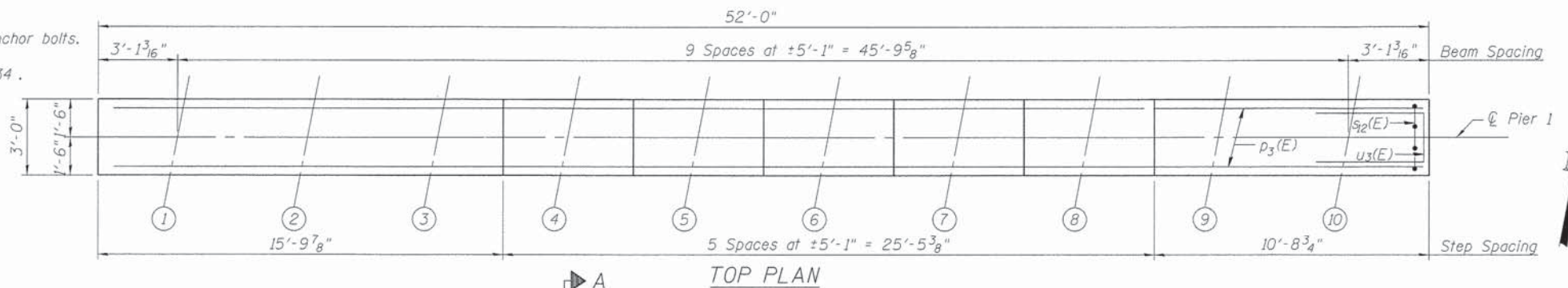
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	96
			CONTRACT NO. 63859	
ILLINOIS FED. AID PROJECT				



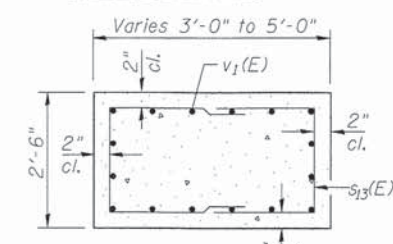
Notes:  
 Space reinforcement in cap to miss anchor bolts.  
 Pour steps monolithically with cap.  
 For details of piles, see sheet 28 of 34.

**PILE DATA**

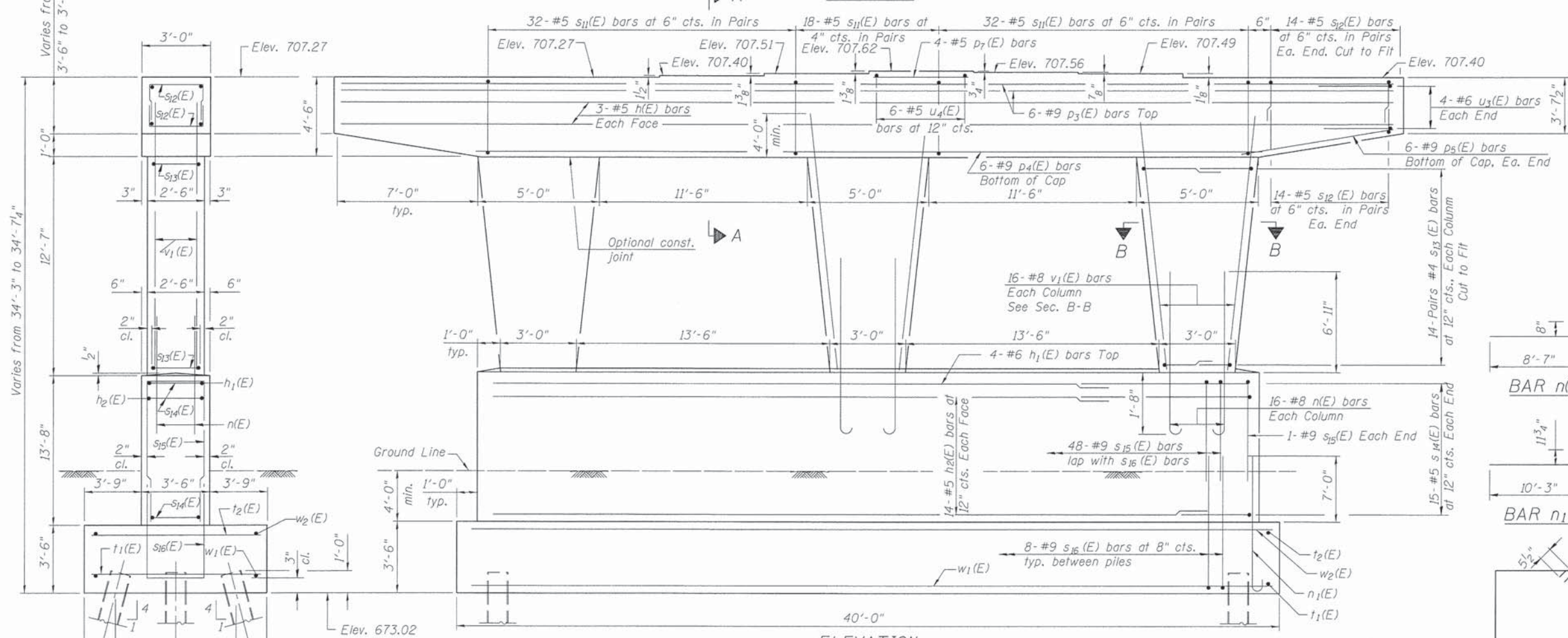
Type: Steel HP14x73  
 Nominal Required Bearing: 578 k  
 Factored Resistance Available: 318 k  
 Est. Length: 50'  
 52' (4V:1H)  
 No. Production Piles: 20  
 No. Test Piles: 1



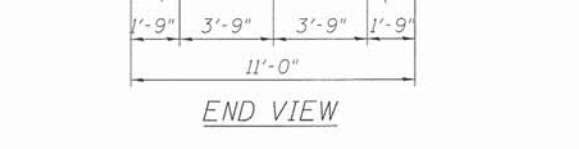
SECTION A-A



SECTION B-B



ELEVATION (Looking North)

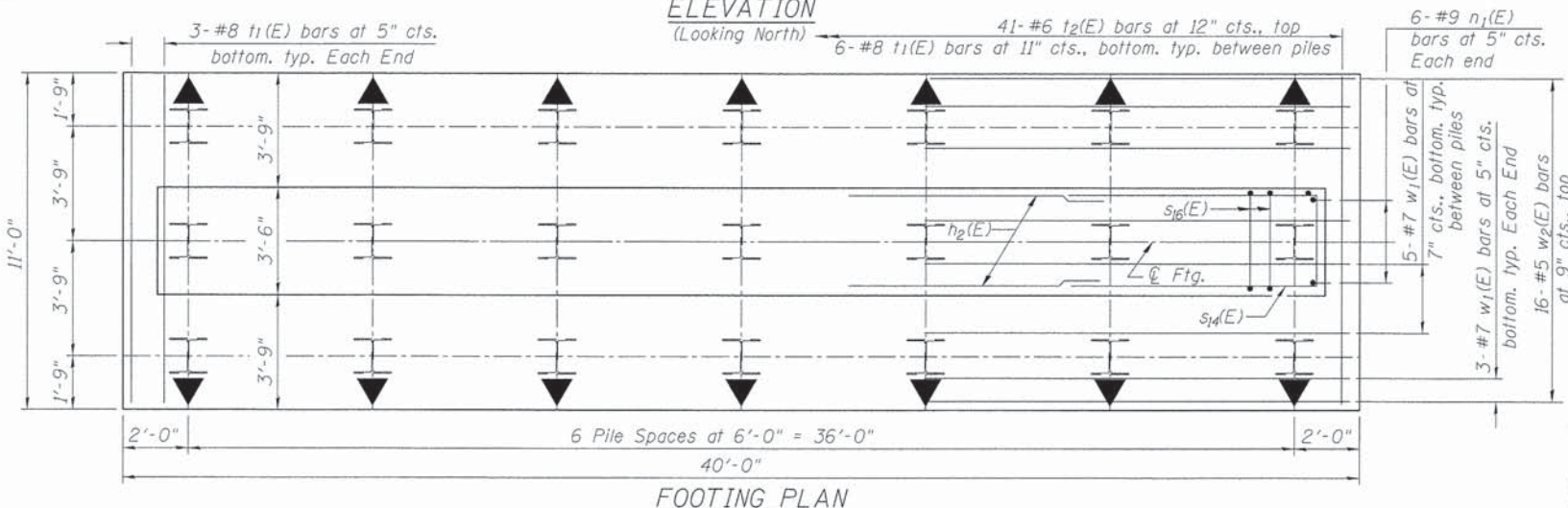


END VIEW

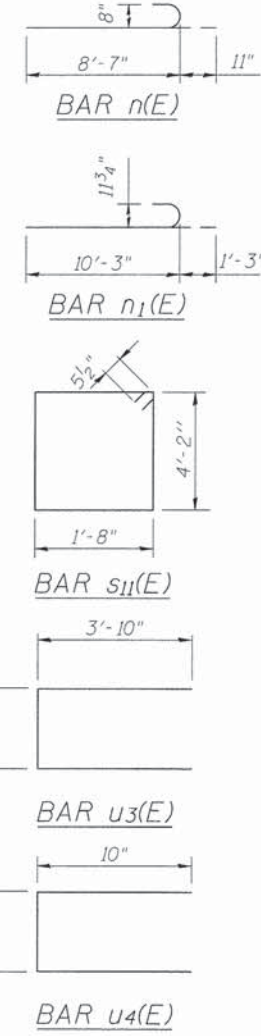
**A & B DIMENSIONS**

Bar	A	B
s12(E)	1'-8"	3'-9"
s13(E)	2'-2"	3'-8"
s14(E)	3'-2"	3'-3"
s15(E)	3'-2"	13'-4"
s16(E)	3'-2"	10'-3"

BARS s12 (E) THRU s16 (E)



FOOTING PLAN



**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
n(E)	6	#5	51'-8"	
h1(E)	4	#6	37'-8"	
h2(E)	28	#5	37'-8"	
n(E)	48	#8	9'-6"	
n1(E)	12	#9	11'-6"	
p3(E)	12	#9	51'-8"	
p4(E)	6	#9	38'-0"	
p5(E)	12	#9	6'-11"	
p7(E)	4	#5	4'-9"	
s11(E)	164	#5	12'-7"	
s12(E)	112	#5	9'-2"	
s13(E)	84	#4	9'-6"	
s14(E)	30	#5	9'-8"	
s15(E)	50	#9	29'-10"	
s16(E)	48	#9	23'-8"	
t1(E)	42	#8	10'-8"	
t2(E)	41	#6	10'-8"	
u3(E)	8	#6	10'-4"	
u4(E)	6	#5	4'-4"	
v1(E)	48	#8	16'-6"	
w1(E)	16	#7	39'-8"	
w2(E)	16	#5	39'-8"	
Structure Excavation		Cu. Yd.	194	
Concrete Structures		Cu. Yd.	164.6	
Reinforcement Bars, Epoxy Coated		Pound	25,600	
Furnishing Steel Piles HP14x73		Foot	1,028	
Driving Piles		Foot	1,028	
Test Pile Steel HP14x73		Each	1	



USER NAME = jrmickow	DESIGNED - JRM	REVISED -
CHECKED - MDS	CHECKED - MDS	REVISED -
DRAWN - RLS	CHECKED - MDS	REVISED -
PLOT SCALE = 3/8" = 1'		
PLOT DATE = 5/12/2014		

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PIER 1  
 STRUCTURE NO. 045-9942  
 SHEET NO. 26 OF 34 SHEETS

F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 97
			CONTRACT NO. 63859	
ILLINOIS FED. AID PROJECT				

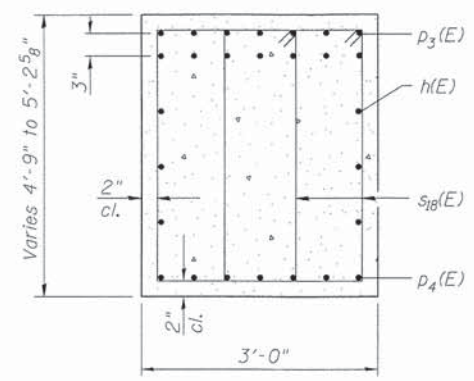
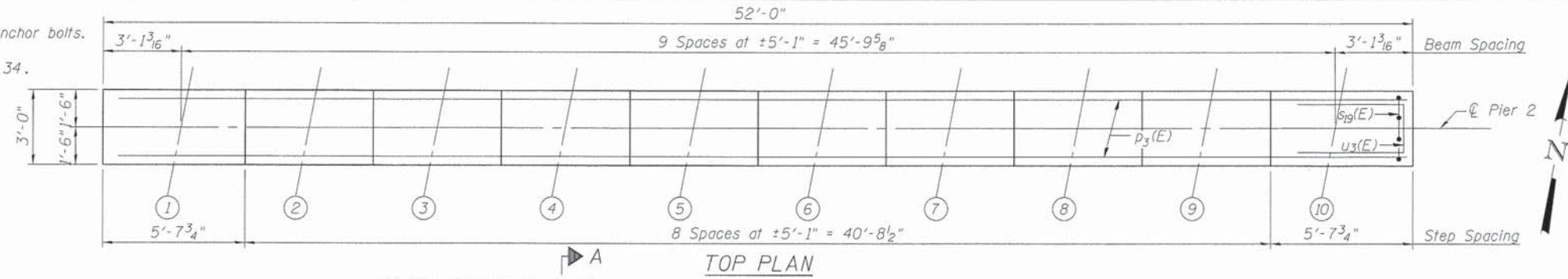
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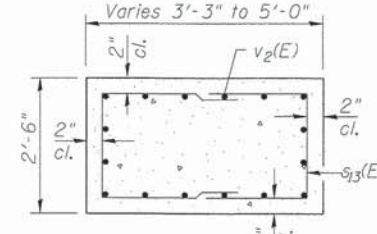
Notes:  
 Space reinforcement in cap to miss anchor bolts.  
 Pour steps monolithically with cap.  
 For details of piles, see sheet 28 of 34.

**PILE DATA**

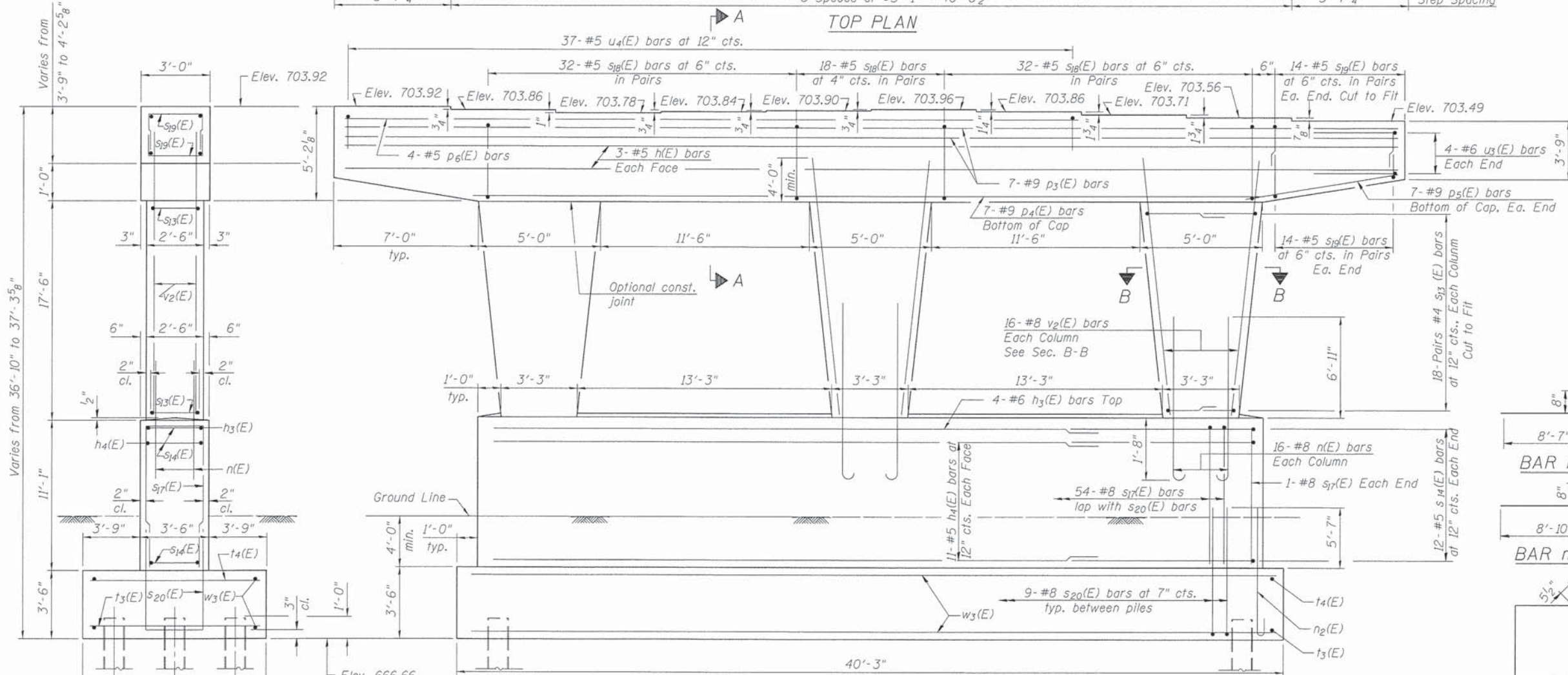
Type: Steel HP14x73  
 Nominal Required Bearing: 578 k  
 Factored Resistance Available: 318 k  
 Est. Length: 45'  
 No. Production Piles: 20  
 No. Test Piles: 1



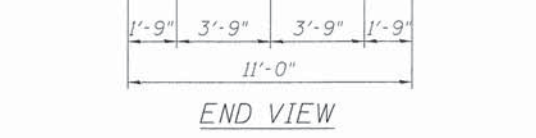
SECTION A-A



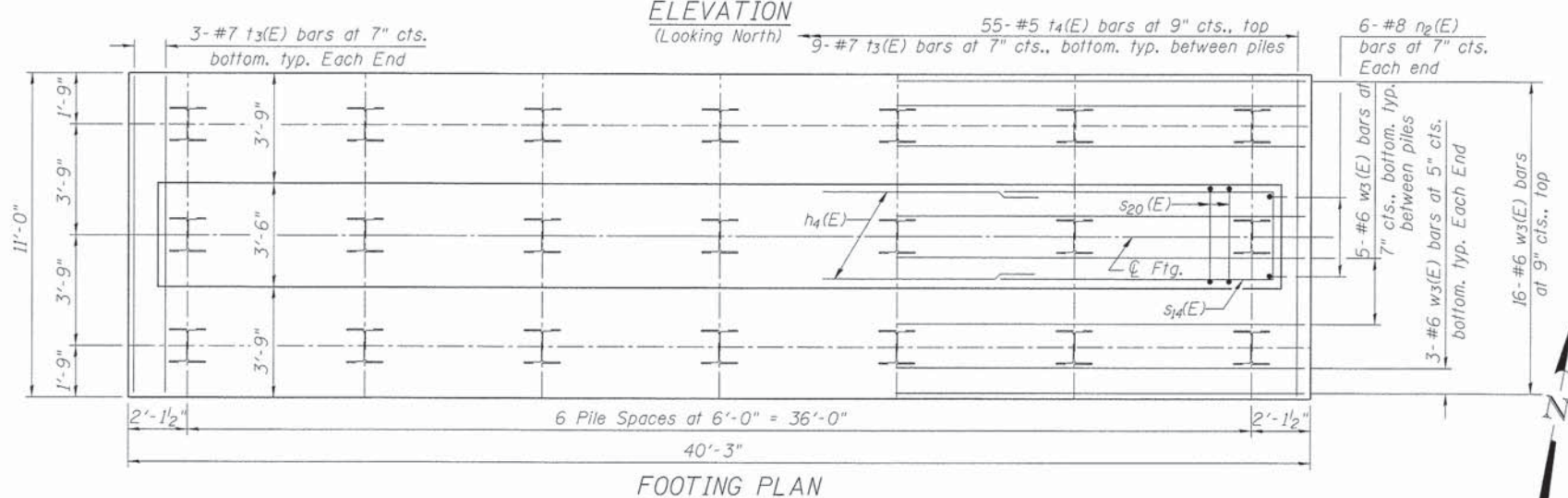
SECTION B-B



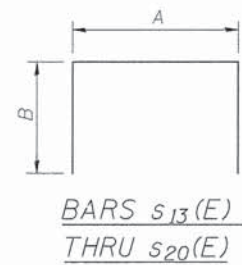
ELEVATION (Looking North)



END VIEW



FOOTING PLAN

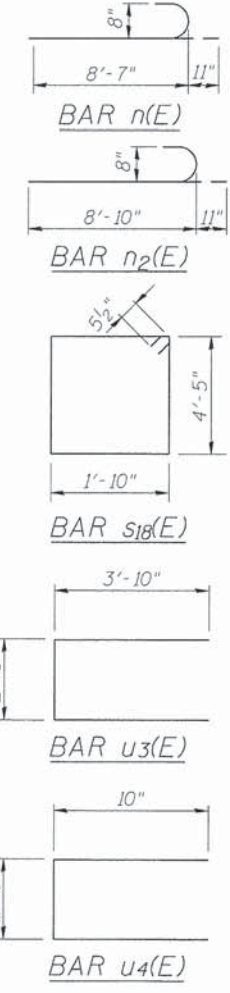


A & B DIMENSIONS

Bar	A	B
s13(E)	2'-2"	3'-8"
s14(E)	3'-2"	3'-3"
s17(E)	3'-2"	10'-9"
s18(E)	1'-10"	3'-10"
s20(E)	3'-2"	8'-10"

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
h(E)	6	#5	51'-8"	—
h3(E)	4	#6	38'-2"	—
h4(E)	22	#5	38'-2"	—
n(E)	48	#8	9'-6"	C
n2(E)	12	#8	9'-9"	C
p3(E)	14	#9	51'-8"	—
p4(E)	7	#9	38'-0"	—
p5(E)	14	#9	6'-11"	—
p6(E)	4	#5	35'-10"	—
s13(E)	108	#4	9'-6"	□
s14(E)	24	#5	9'-8"	□
s17(E)	56	#8	24'-8"	□
s18(E)	164	#5	13'-5"	□
s19(E)	112	#5	9'-6"	□
s20(E)	54	#8	20'-10"	□
t3(E)	60	#7	10'-8"	—
t4(E)	55	#5	10'-8"	—
u3(E)	8	#6	10'-4"	—
u4(E)	37	#5	4'-4"	—
v2(E)	48	#8	21'-5"	—
w3(E)	32	#6	39'-11"	—
Structure Excavation	Cu. Yd.	191		
Concrete Structures	Cu. Yd.	160.9		
Reinforcement Bars, Epoxy Coated	Pound	24,700		
Furnishing Steel Piles HP14x73	Foot	900		
Driving Piles	Foot	900		
Test Pile Steel HP14x73	Each	1		



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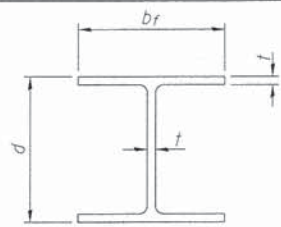
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PLOT SCALE * 3/8" = 1'	CHECKED - MDS	REVISED -
PLOT DATE * 5/12/2014	DRAWN - RLS	REVISED -
	CHECKED - MDS	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PIER 2  
 STRUCTURE NO. 045-9942  
 SHEET NO. 27 OF 34 SHEETS

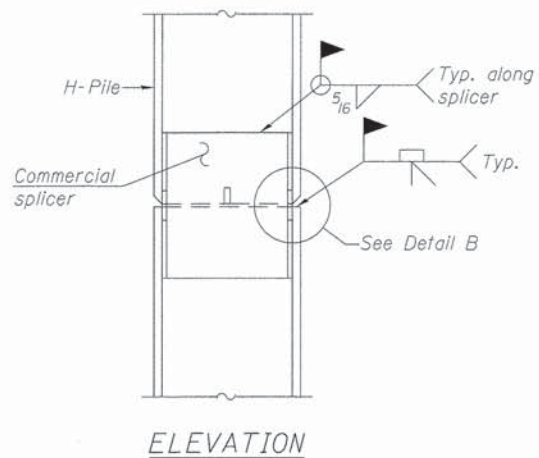
F.A.P. RTE. 4087	SECTION 08-00278-00-BR	COUNTY KANE	TOTAL SHEETS 136	SHEET NO. 98
			CONTRACT NO. 63859	
ILLINOIS FED. AID PROJECT				



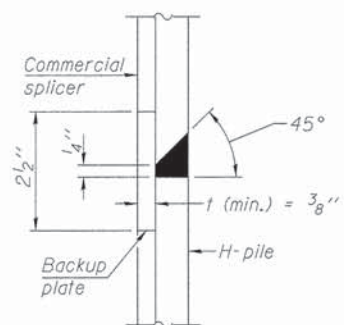


STEEL PILE TABLE

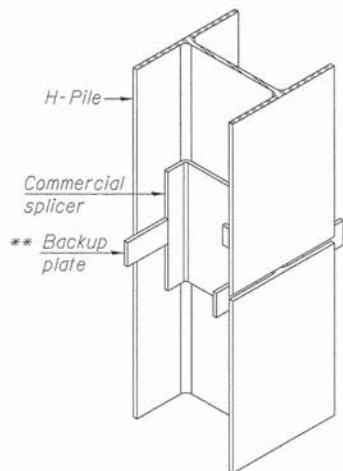
Designation	Depth d	Flange width b <sub>f</sub>	Web and Flange thickness t	Encasement diameter A
HP 14x117	14 1/4"	14 7/8"	13/16"	30"
x102	14"	14 3/4"	1/16"	30"
x89	13 7/8"	14 3/4"	5/8"	30"
x73	13 5/8"	14 5/8"	1/2"	30"
HP 12x84	12 1/4"	12 1/4"	1/16"	24"
x74	12 1/8"	12 1/4"	5/8"	24"
x63	12"	12 1/8"	1/2"	24"
x53	11 3/4"	12"	7/16"	24"
HP 10x57	10"	10 1/4"	9/16"	24"
x42	9 3/4"	10 1/8"	7/16"	24"
HP 8x36	8"	8 1/8"	7/16"	18"



ELEVATION

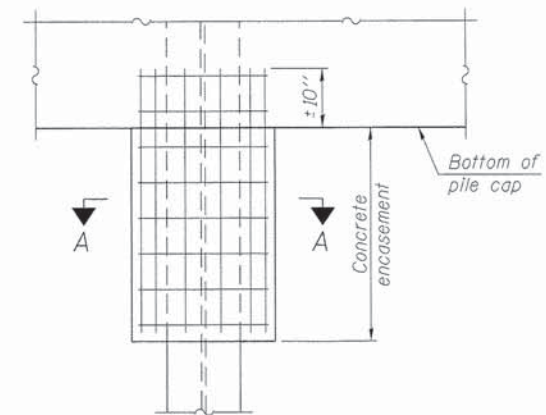


DETAIL "B"



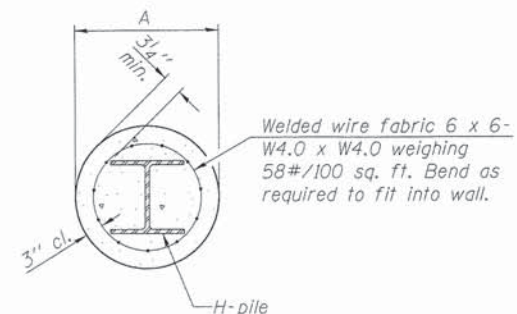
ISOMETRIC VIEW

WELDED COMMERCIAL SPLICE



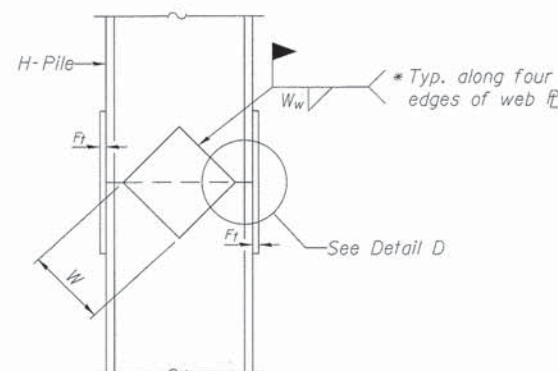
ELEVATION

PILE ENCASEMENT

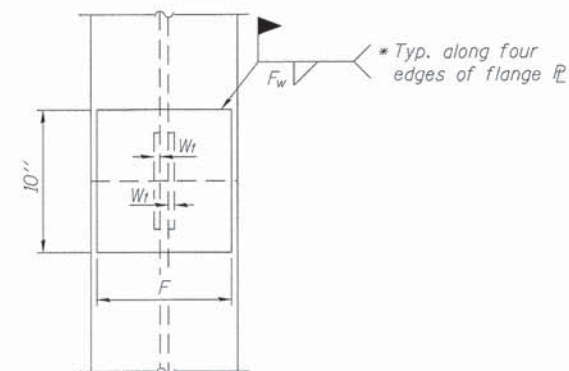


Note:  
Forms for encasement may be omitted when soil conditions permit.

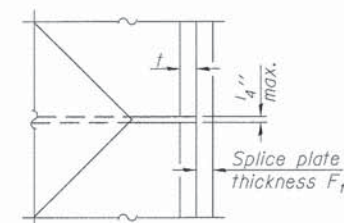
SECTION A-A



ELEVATION



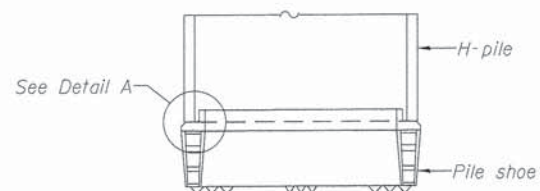
END VIEW



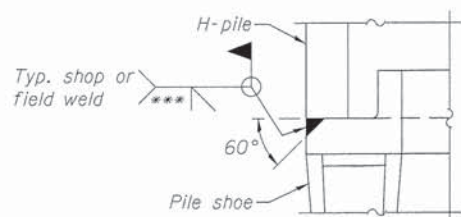
DETAIL D

WELDED PLATE FIELD SPLICE

Designation	F	F <sub>t</sub>	F <sub>w</sub>	W	W <sub>t</sub>	W <sub>w</sub>
HP 14x117	12 1/2"	1"	7/8"	7 3/4"	5 8/8"	1/2"
x102	12 1/2"	7/8"	3/4"	7 3/4"	5 8/8"	1/2"
x89	12 1/2"	3/4"	1/16"	7 3/4"	5 8/8"	1/2"
x73	12 1/2"	5/8"	9/16"	7 3/4"	5 8/8"	1/2"
HP 12x84	10"	7/8"	1/16"	6 1/2"	5 8/8"	1/2"
x74	10"	7/8"	1/16"	6 1/2"	5 8/8"	1/2"
x63	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
x53	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
HP 10x57	8"	3/4"	9/16"	5 1/4"	1/2"	3/8"
x42	8"	5/8"	9/16"	5 1/4"	1/2"	3/8"
HP 8x36	7"	5/8"	7/16"	4 1/4"	1/2"	3/8"

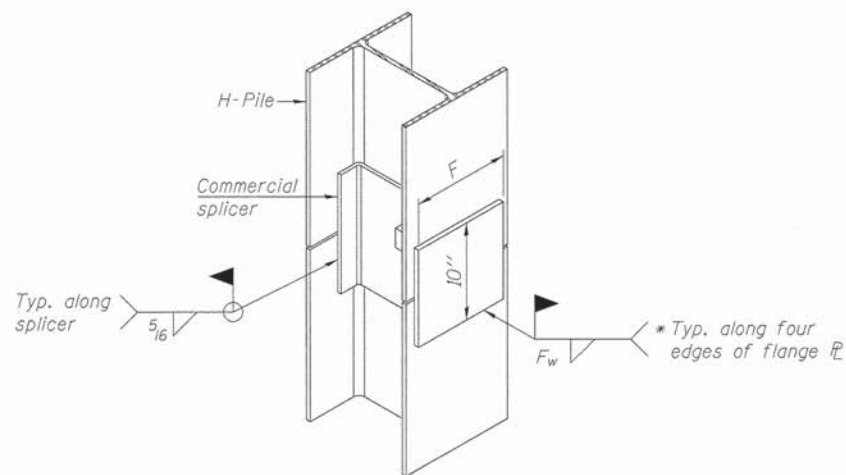


ELEVATION



DETAIL A

H-PILE SHOE ATTACHMENT



ISOMETRIC VIEW

WELDED COMMERCIAL SPLICE ALTERNATE

- \* Interrupt welds 1/4" from end of web and/or each flange.
- \*\* Remove portions of backup plates that extend outside the flanges.
- \*\*\* Weld size per pile shoe manufacturer (5/16" min.).

Note:  
The steel H-piles shall be according to AASHTO M270 Grade 50.

5/12/2014 5:35:33 PM - C:\CHN\0130\Bridg\CA00\Sheet\0459942-63859-028-PILE.dgn

F-HP

1-27-12



USER NAME = jrmiskov	DESIGNED - JRM	REVISED -
PLOT SCALE = 1:8,000 1/4" = 1'	CHECKED - MDS	REVISED -
PLOT DATE = 5/12/2014	DRAWN - SFW	REVISED -
	CHECKED - MDS	REVISED -

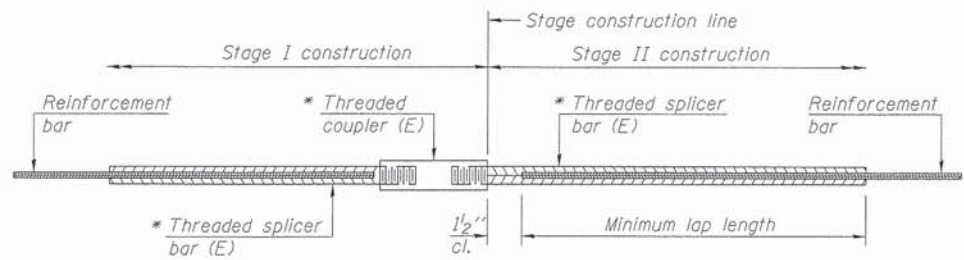
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

HP PILE DETAILS  
STRUCTURE NO. 045-9942

SHEET NO. 28 OF 34 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	99
			CONTRACT NO. 63859	
ILLINOIS FED. AID PROJECT				





**STANDARD BAR SPLICER ASSEMBLY**

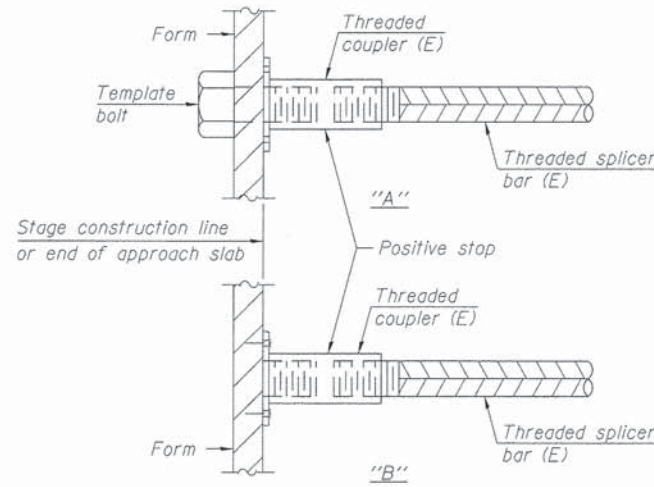
Bar size to be spliced	Minimum Lap Lengths					
	Table 1	Table 2	Table 3	Table 4	Table 5	Table 6
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-7"	2'-11"
5	1'-9"	2'-5"	2'-7"	2'-11"	3'-3"	3'-8"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-10"	4'-5"
7	2'-9"	3'-10"	4'-2"	4'-8"	5'-2"	5'-10"
8	3'-8"	5'-1"	5'-5"	6'-2"	6'-9"	7'-8"
9	4'-7"	6'-5"	6'-10"	7'-9"	8'-7"	9'-8"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Class C
- Table 6: Epoxy bar, Top bar top, Class C

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

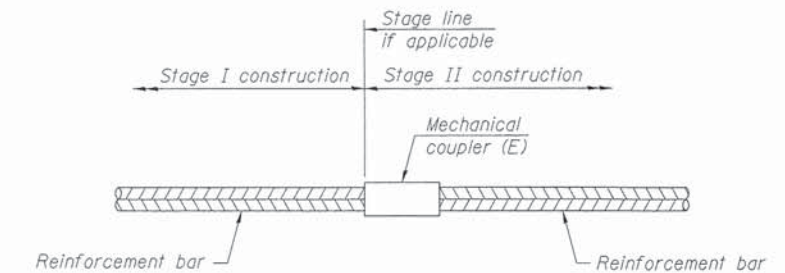
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length



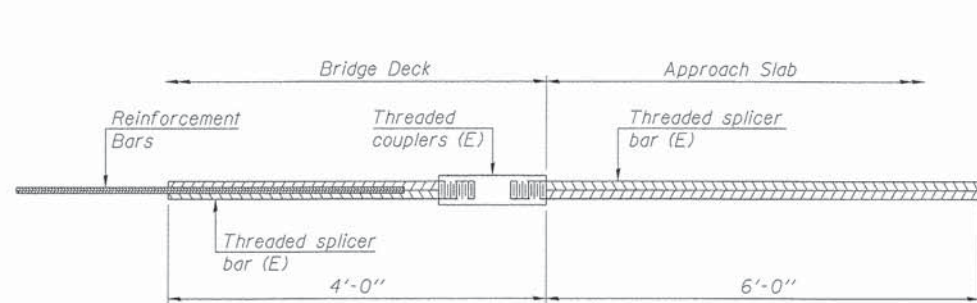
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



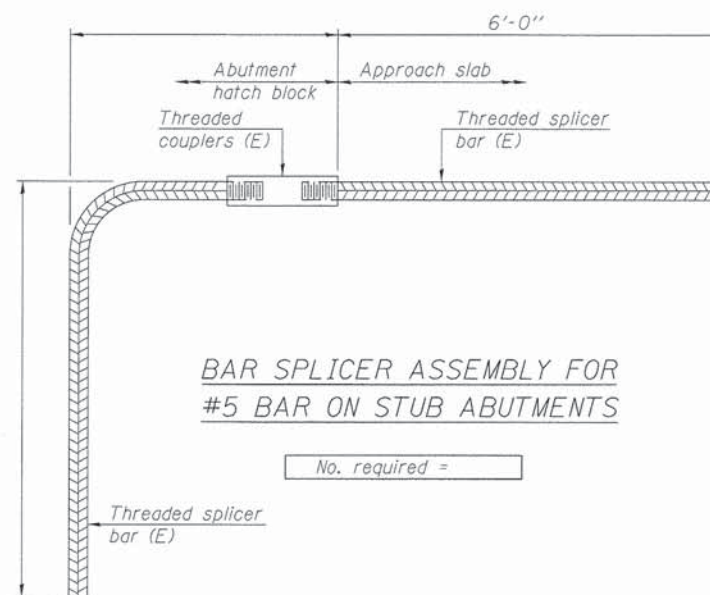
**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required



**BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

No. required = 80



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required =

**NOTES**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
 All reinforcement shall be lapped and tied to the splicer bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

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BSD-1

1-27-12



USER NAME = jmsokow	DESIGNED = JRM	REVISED =
PLOT SCALE = 1/8" = 1' / in.	CHECKED = MDS	REVISED =
PLOT DATE = 5/12/2014	DRAWN = SFW	REVISED =
	CHECKED = MDS	REVISED =

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
STRUCTURE NO. 045-9942**

SHEET NO. 29 OF 34 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4087	08-00278-00-BR	KANE	136	100
				CONTRACT NO. 63859

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