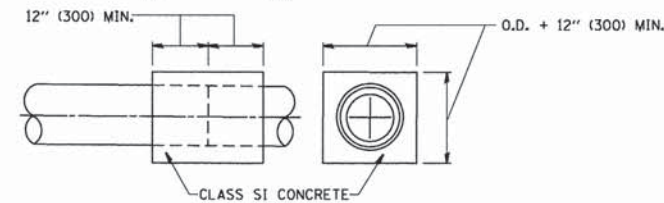
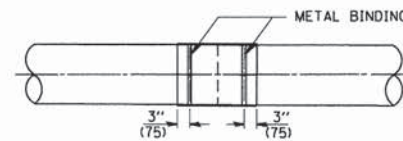
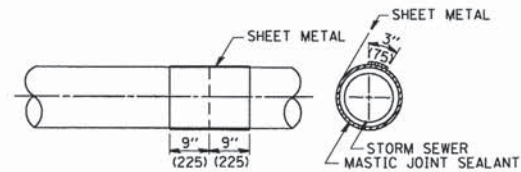
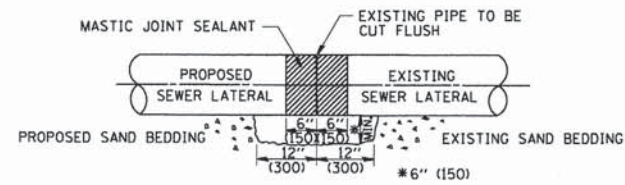


DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

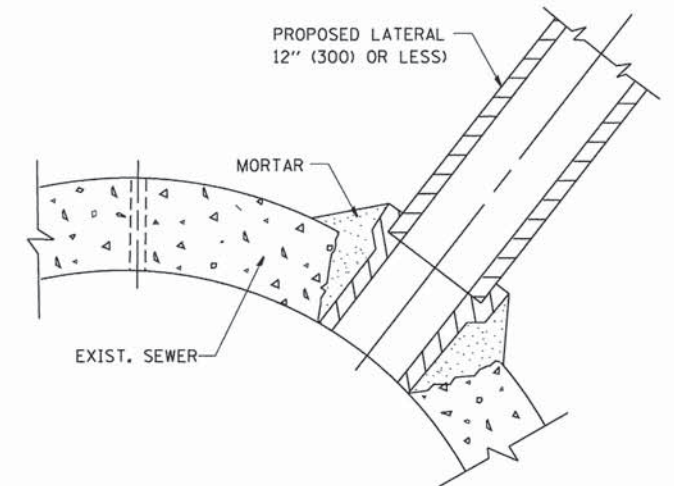


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

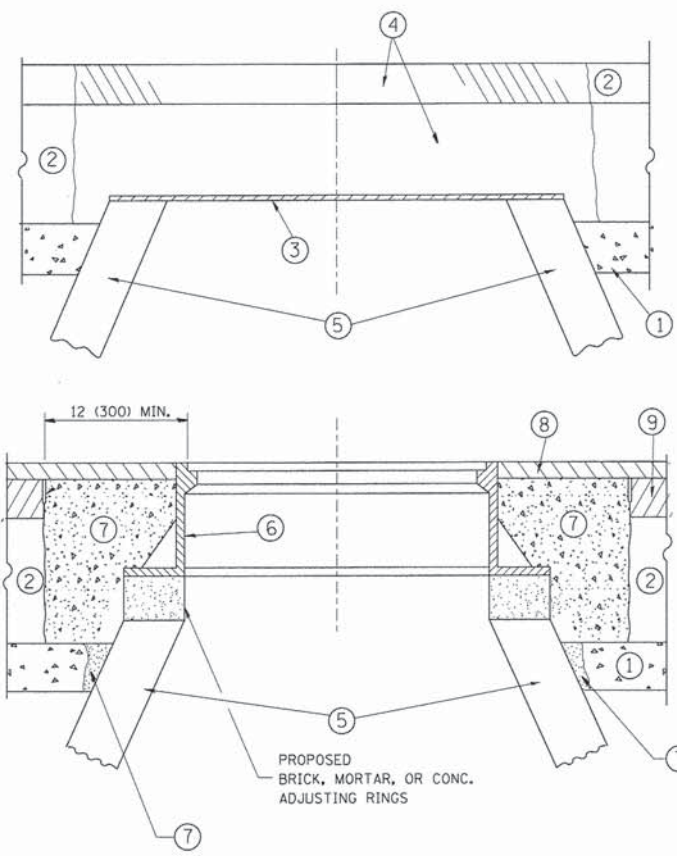
REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd87.dgn	USER NAME = gag11enob1	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER			F.A. RTE. 2714	SECTION	COUNTY	TOTAL SHEETS 240	SHEET NO. 201
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - R. SHAH 09-09-94		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD500-01 (BD-7)		CONTRACT	63887
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - R. SHAH 10-25-94									
		DATE - 07-25-90	REVISED - R. SHAH 06-12-96		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

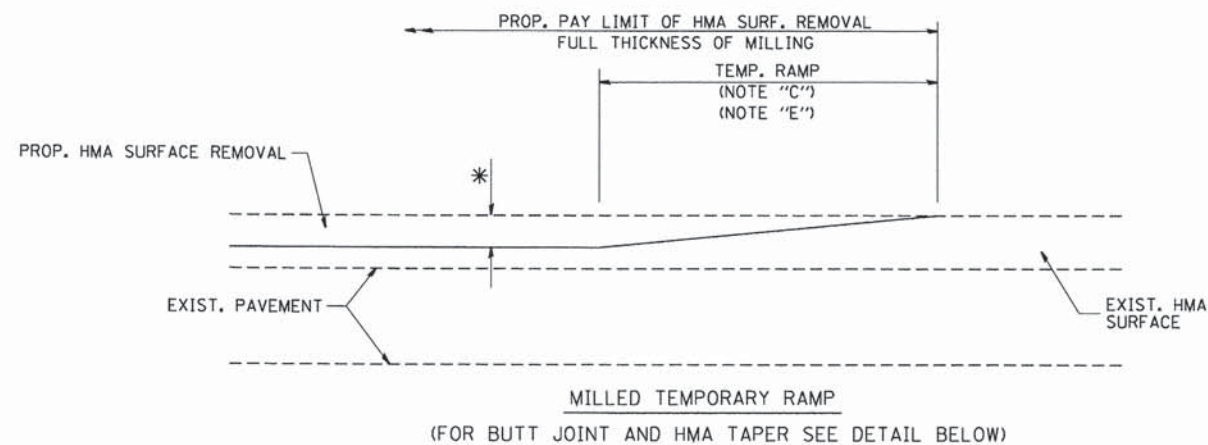
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	PLOT SCALE = 1/8" = 1'-0"	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

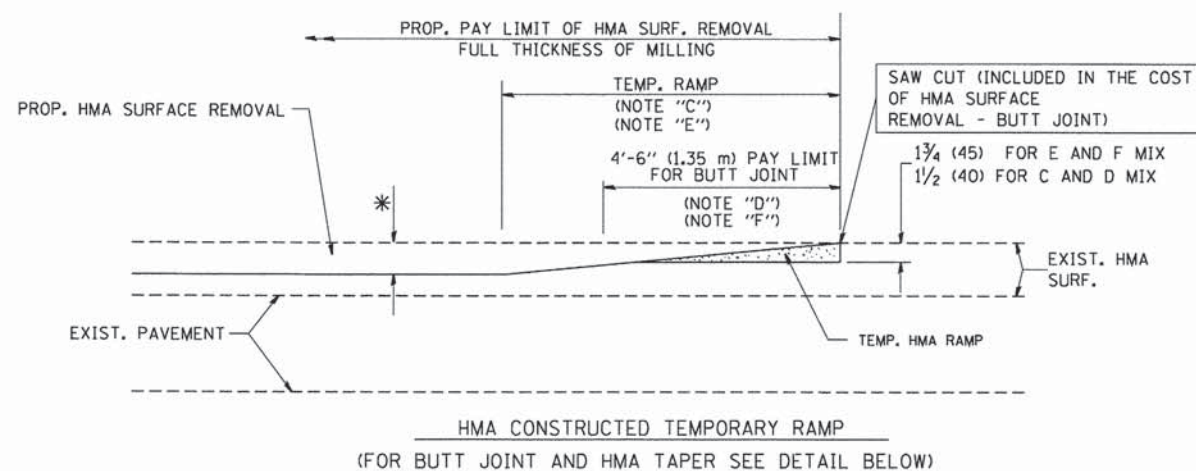
**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

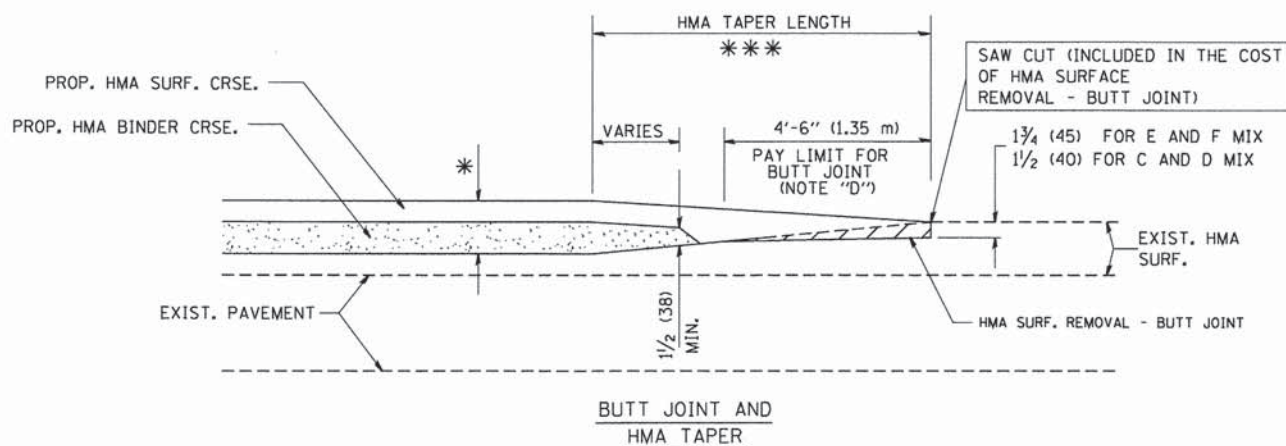
F.A. RTE. 2714	SECTION	COUNTY	TOTAL SHEETS 240	SHEET NO. 202
BD600-03 (BD-8)		CONTRACT	63887	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



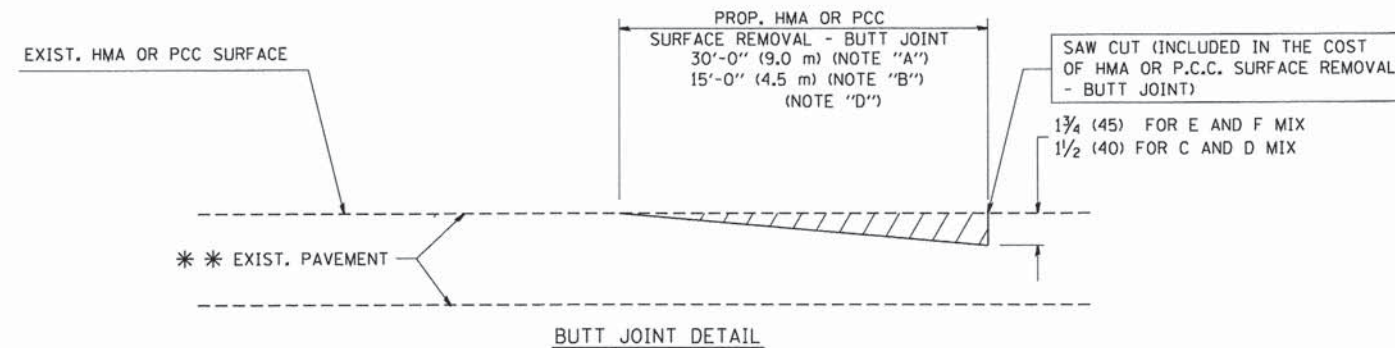
OPTION 1



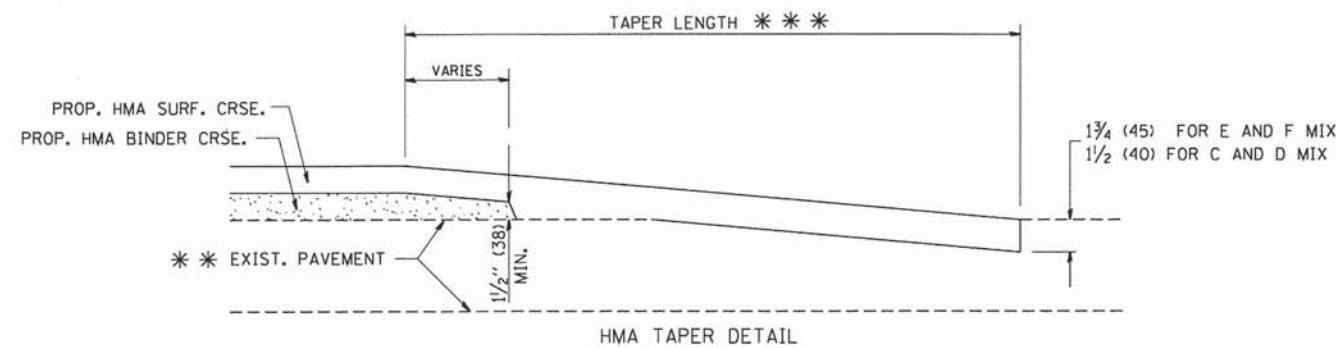
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

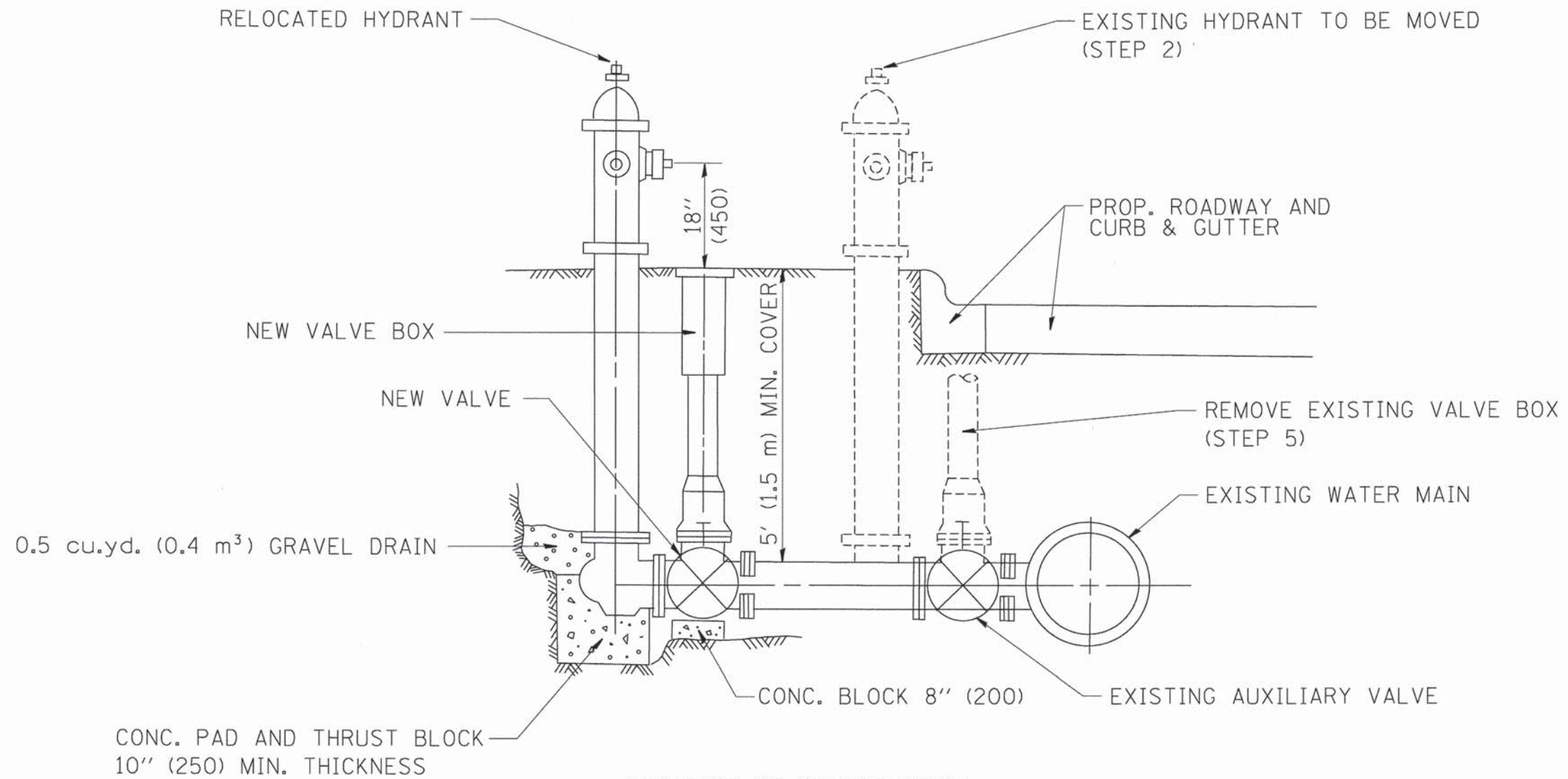
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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 2714	SECTION	COUNTY	TOTAL SHEETS 240	SHEET NO. 203
BD400-05 BD32		CONTRACT	63887	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SEQUENCE OF CONSTRUCTION:

1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

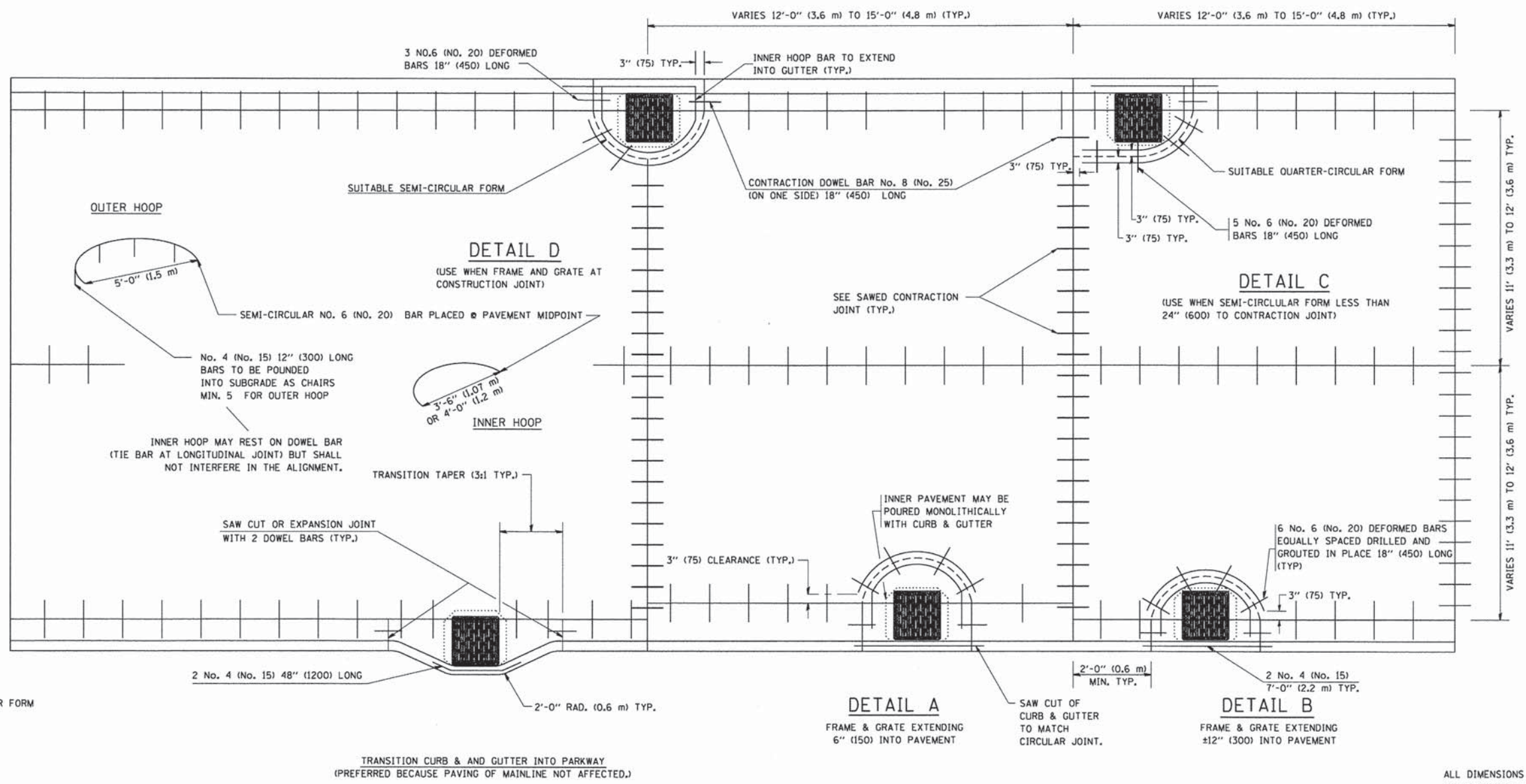
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - R. SHAH 10-25-94		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	2714			240	204
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		PLOT DATE = 1/4/2008	DATE -						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FRAME EXTENSION INTO PAVEMENT	INNER HOOP REINFORCEMENT DIAMETER	SEMI CIRCULAR FORM DIAMETER	OUTER HOOP REINFORCEMENT DIAMETER
UP TO 8" (200)	3'-6" (1.1 m)	4'-0" (1.2 m)	5'-0" (1.5 m)
> 8" (200) TO 14" (360)	4'-0" (1.2 m)	4'-6" (1.4 m)	5'-0" (1.5 m)

DESIGNER NOTE:
THIS DETAIL IS TO BE USED
WHEN THE GUTTER FLAG IS
LESS THAN 24"

- NOTES:**
- THE ROUNDOUT AND ADDED REINFORCEMENT WILL NOT BE PAID SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PAVEMENT.
 - TRANSVERSE JOINTS MAY BE MOVED TO ACCOMMODATE ROUNDOUT, EDGE OF CIRCULAR JOINT SHALL BE MINIMUM 12" (300) FROM TRANSVERSE JOINT. RELOCATED TRANSVERSE JOINT SHALL BE CONTINUOUS FROM EDGE OF PAVEMENT TO EDGE OF PAVEMENT.
 - SEMI-CIRCULAR FORM SHALL BE REMOVED PRIOR TO DRILL AND GROUT OF TIE BARS.
 - ALL REINFORCED BARS SHALL BE EPOXY COATED.
 - DRILL AND GROUT IS PREFERRED, HOWEVER TIE BARS CAN BE POURED IN PLACE IF CLEARANCE IS PROVIDED TO OUTER EDGE OF FRAME. MINIMUM 2" (50) CLEARANCE.
 - WOOD SHIMS SHALL BE USED TO ADJUST ALL FRAMES. AFTER ADJUSTING MORTAR HAS CURED, THE WOOD SHIMS SHALL BE REMOVED AND THE VOIDS UNDER THE FRAMES FILLED WITH NON SHRINK GROUT.
 - HOOP REINFORCEMENT SHALL BE ONE PIECE CONSTRUCTION.
 - CIRCULAR FRAMES AND GRATES MAY BE SUBSTITUTED.
 - CURB DOWELS MUST BE PLACED LEVEL & TRUE TO ALLOW CONTRACTION MOVEMENT.



LEGEND:
 CASTING
 - - - - - SUITABLE SEMI-CIRCULAR FORM

ALL DIMENSIONS ARE IN INCHES
(MILLIMETERS) UNLESS OTHERWISE NOTED

FILE NAME = W:\diststd\22x34\bd48.dgn	USER NAME = goglianobt	DESIGNED - A. ABBAS	REVISED - T. MATOUSEK 08-28-00
		DRAWN - TOM MATOUSEK	REVISED - T. MATOUSEK 10-02-00
	PLOT SCALE = 50.0000' / IN.	CHECKED - A. ABBAS	REVISED - T. MATOUSEK 04-25-02
	PLOT DATE = 1/4/2008	DATE - 01-04-99	REVISED - P. LAFLEUR 08-27-02

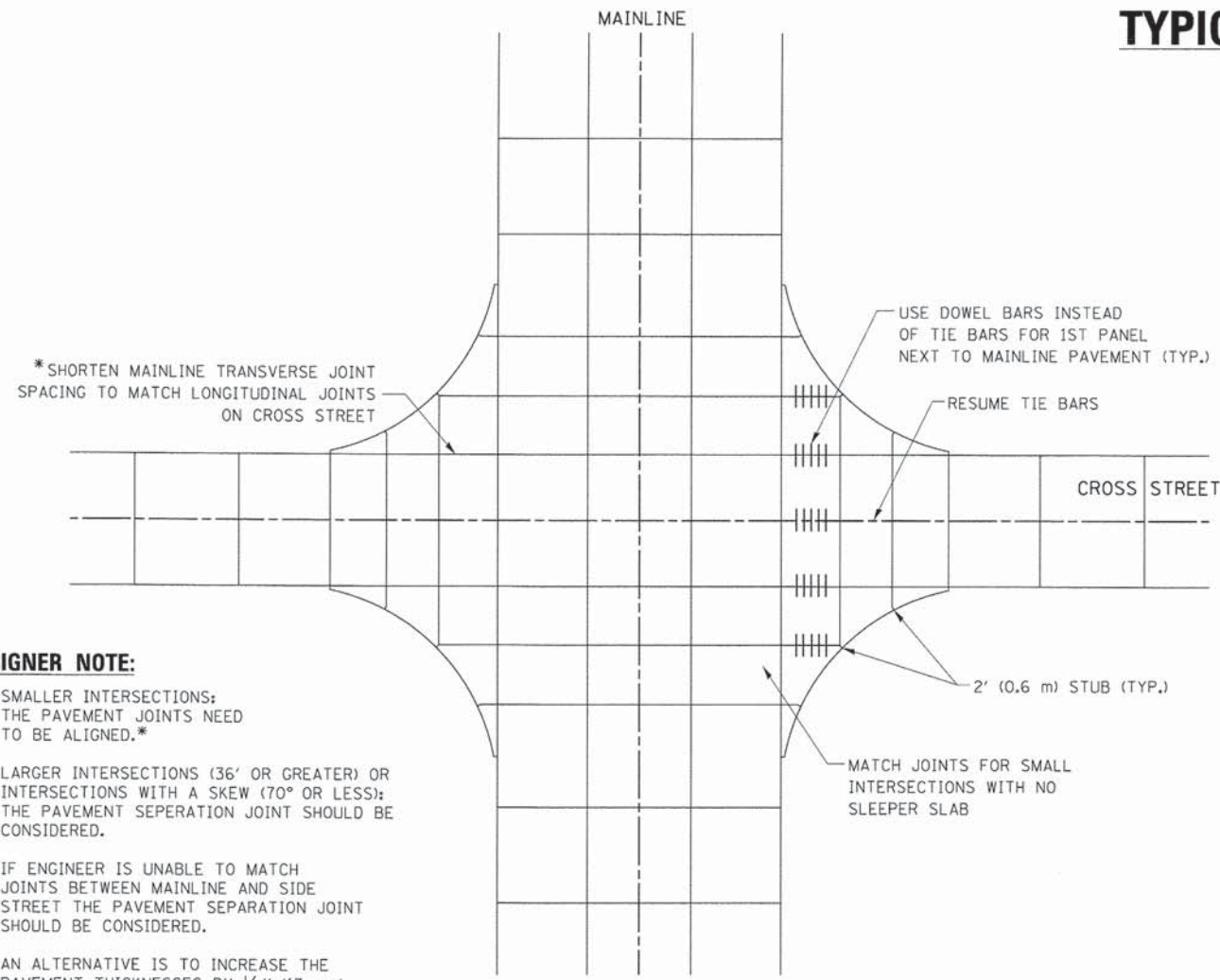
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PCC PAVEMENT ROUNDOUTS AT
CURB AND GUTTER**
 SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. - RTE. 2714	SECTION	COUNTY	TOTAL SHEETS 240	SHEET NO. 205
BD-48		CONTRACT	63887	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

TYPICAL APPLICATION

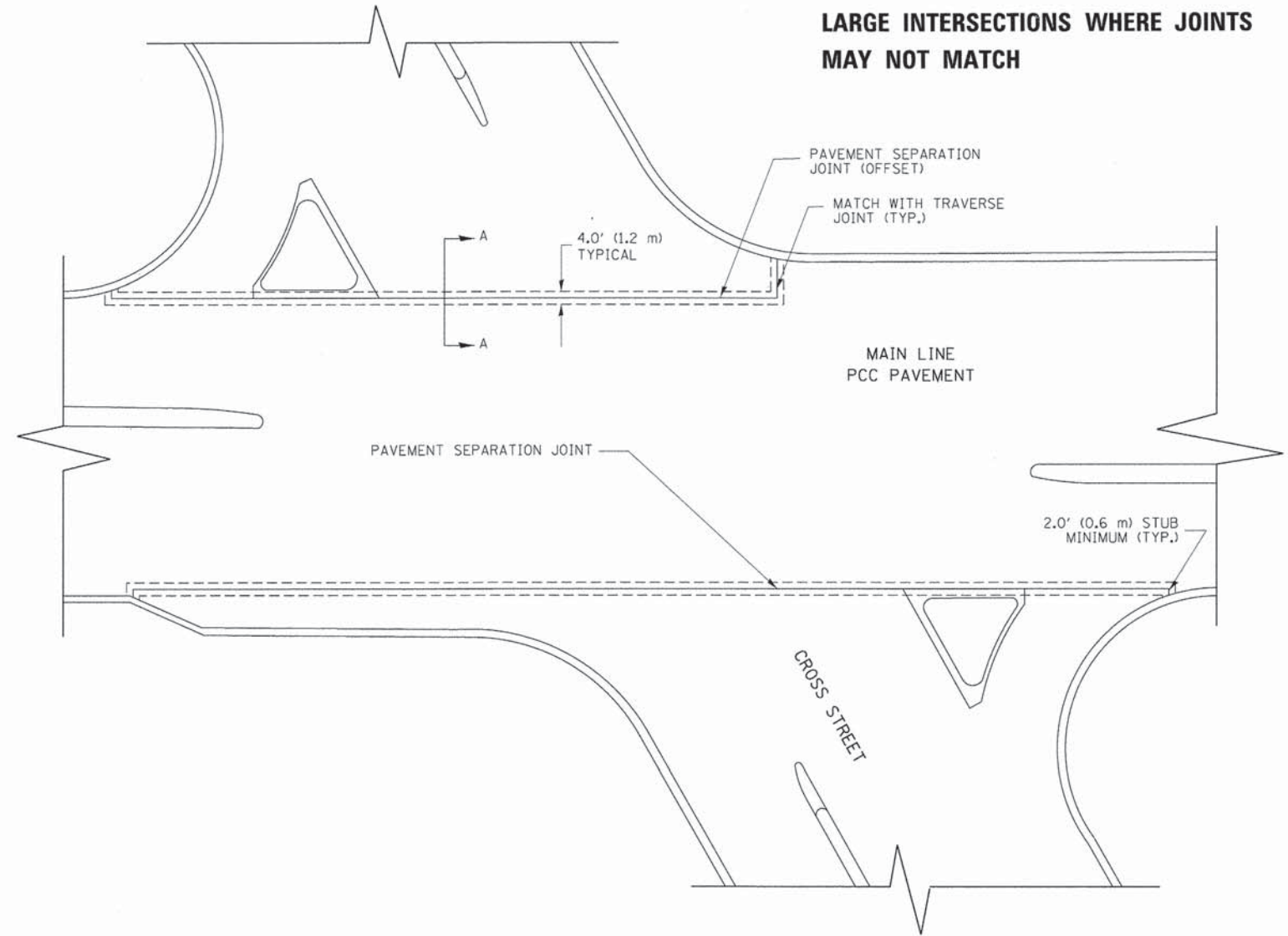
THE USE OF CROSS STREET PAVEMENT SEPARATION JOINTS FOR SKEWED OR LARGE INTERSECTIONS WHERE JOINTS MAY NOT MATCH



PLAN

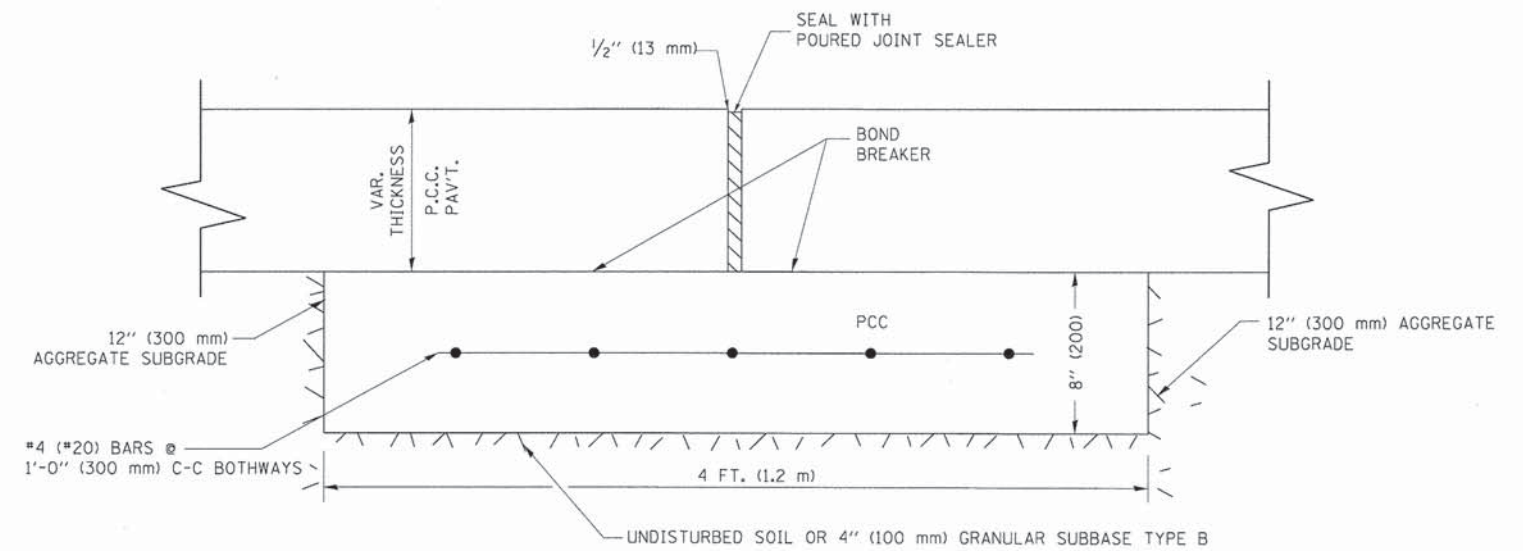
DESIGNER NOTE:

1. SMALLER INTERSECTIONS: THE PAVEMENT JOINTS NEED TO BE ALIGNED.*
2. LARGER INTERSECTIONS (36' OR GREATER) OR INTERSECTIONS WITH A SKEW (70° OR LESS): THE PAVEMENT SEPERATION JOINT SHOULD BE CONSIDERED.
3. IF ENGINEER IS UNABLE TO MATCH JOINTS BETWEEN MAINLINE AND SIDE STREET THE PAVEMENT SEPARATION JOINT SHOULD BE CONSIDERED.
4. AN ALTERNATIVE IS TO INCREASE THE PAVEMENT THICKNESSES BY 1/2" (13 mm) FOR THE LENGTH OF THE AFFECTED PANELS AT THE INTERSECTION.
5. FOR LARGE INTERSECTIONS (6 LANES OR MORE) WHERE JOINTS CAN BE MATCHED, USE #8 (25) DOWEL BARS INSTEAD OF #8 (25) TIE BARS AT EDGE OF MAINLINE PAVEMENT WHEN NO PAVEMENT SEPARATION JOINTS USED.



NOTE:

1. JOINT FILLER SHALL CONSIST OF A SHEET OF 1/2" (13 mm) BITUMINOUS PREFORMED FIBER JOINT FILLER CONFORMING TO ARTICLE 1051.03 OF THE STANDARD SPECIFICATIONS.
2. THE JOINT SHALL BE SEALED WITH A HOT POUR JOINT SEALER CONFORMING TO ARTICLE 1050.02 OF THE STANDARD SPECIFICATIONS.
3. A SINGLE LAYER OF FELT ROOFING PAPER SHALL SERVE AS A BOND BREAKER.
4. JOINT SHALL CONTINUE THROUGH COMBINATION CURB & GUTTER OR PCC SHOULDER.
5. PAVEMENT SEPARATION JOINT IS TO BE PAID FOR AS "SLEEPER SLAB" AND IS TO BE MEASURED IN PLACE BY THE LINEAL FOOT.
6. BOND BREAKER AND 1/2" (13 mm) JOINT AND FILLER SHALL BE INCIDENTAL TO THE PAY ITEM "SLEEPER SLAB".



PROPOSED SECTION A-A

FILE NAME = bd52.dgn	USER NAME = loyse	DESIGNED -	REVISED - CADD 06-18-10
		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

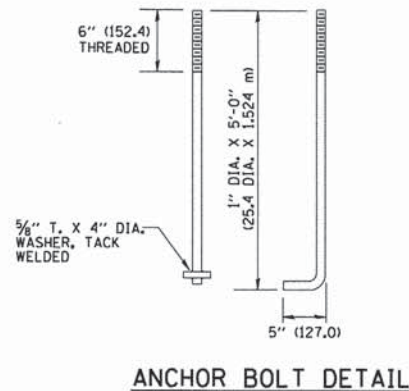
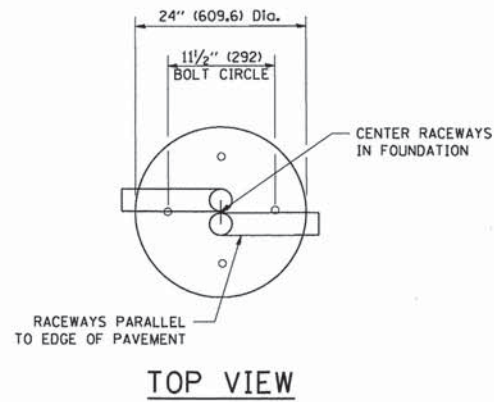
DETAIL OF PAVEMENT SEPARATION
JOINT FOR JOINTED PCC PAVEMENTS AT INTERSECTIONS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

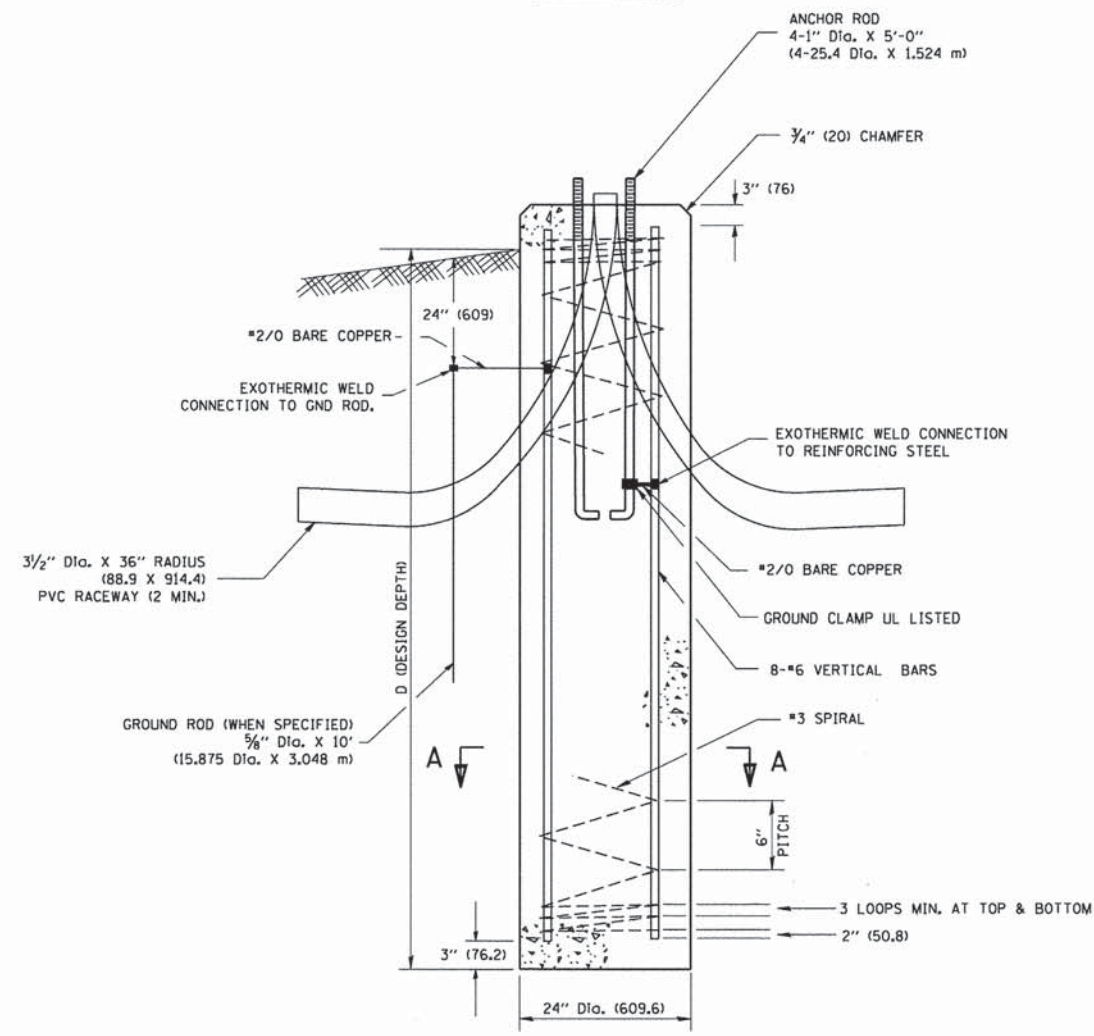
F.A. RTE. 2714	SECTION	COUNTY	TOTAL SHEETS 240	SHEET NO. 206
BD52		CONTRACT	63887	
ILLINOIS FED. AID PROJECT				

LIGHT POLE FOUNDATION DEPTH TABLE
30 FT. (9.144 m) TO 35 FT. (10.668 m) MOUNTING HEIGHT

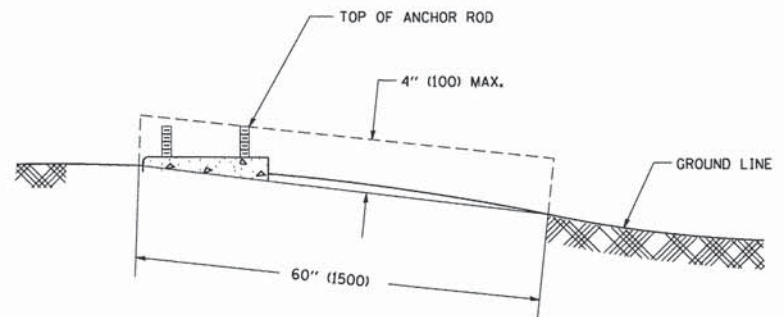
SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY Qu = 0.375 TON/SO. FT.	11'-0" (3.35 m)	12'-8" (3.85 m)
MEDIUM CLAY Qu = 0.75 TON/SO. FT.	9'-0" (2.74 m)	14'-10" (4.52 m)
STIFF CLAY Qu = 1.50 TON/SO. FT.	7'-6" (2.29 m)	8'-7" (2.61 m)
LOOSE SAND φ = 34°	9'-6" (2.90 m)	10'-7" (3.22 m)
MEDIUM SAND φ = 37.5°	9'-0" (2.74 m)	9'-10" (2.99 m)
DENSE SAND φ = 40°	8'-3" (2.51 m)	9'-7" (2.91 m)



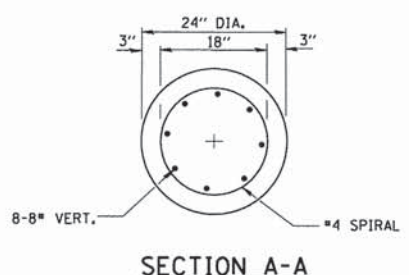
ANCHOR BOLT DETAIL



FOUNDATION DETAIL



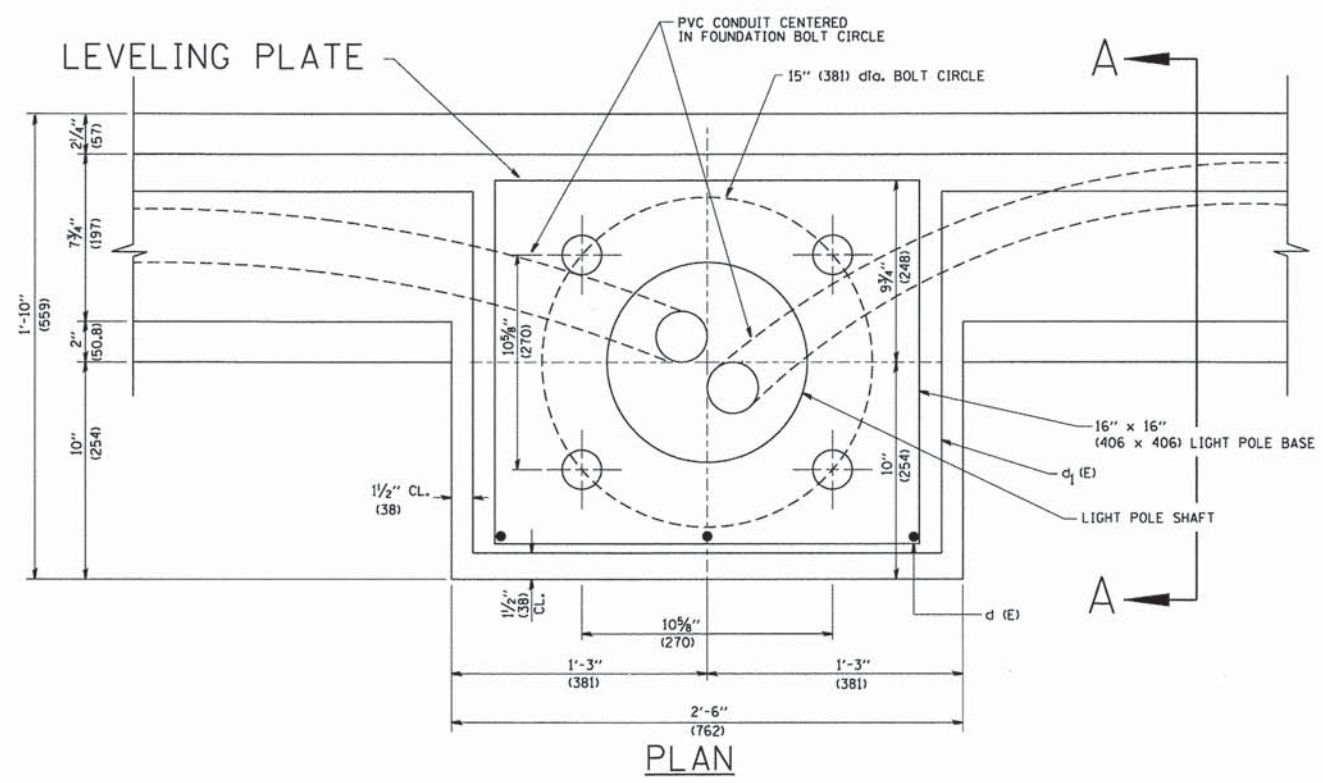
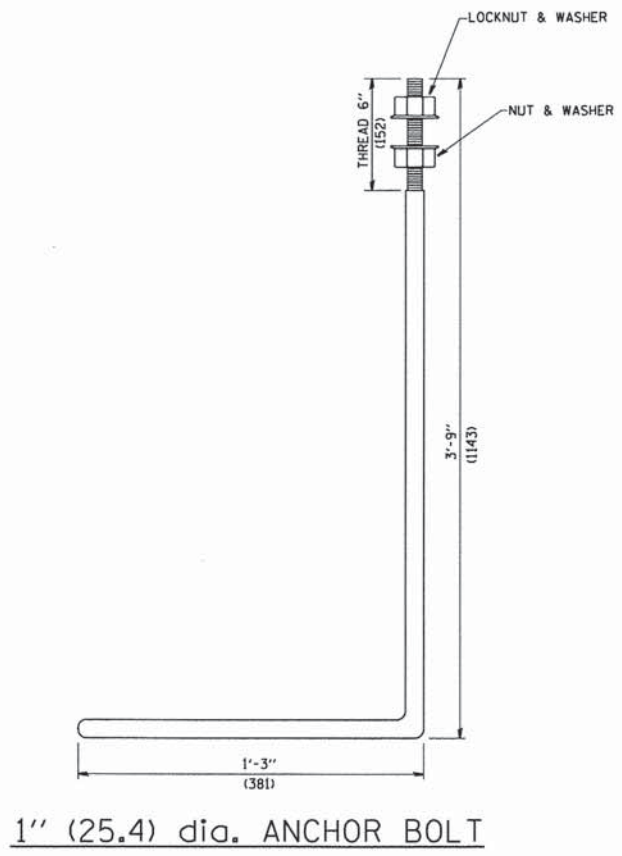
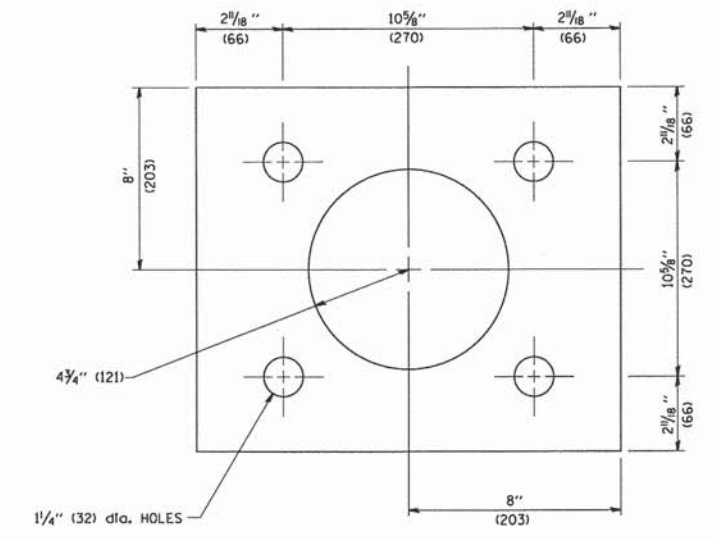
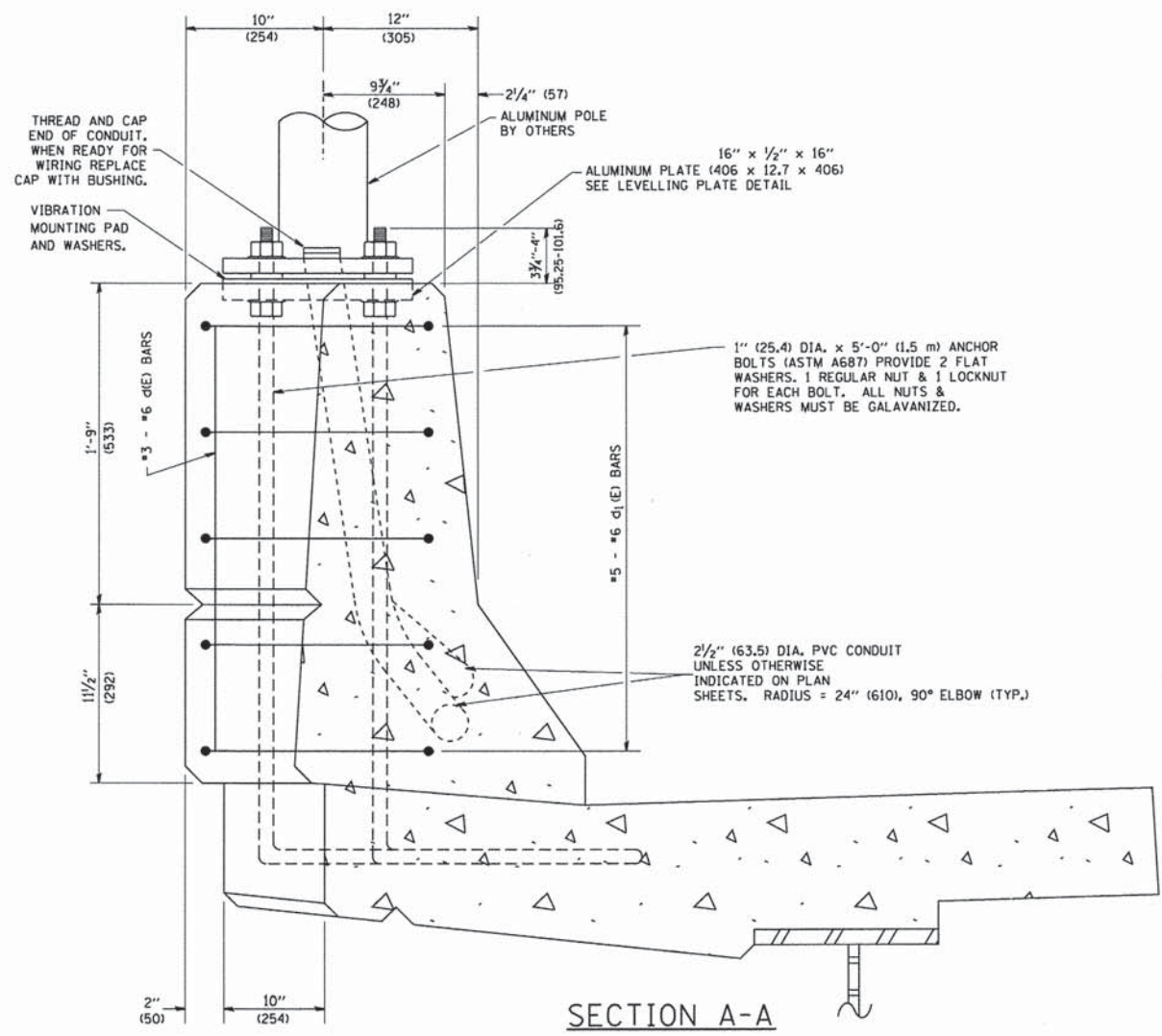
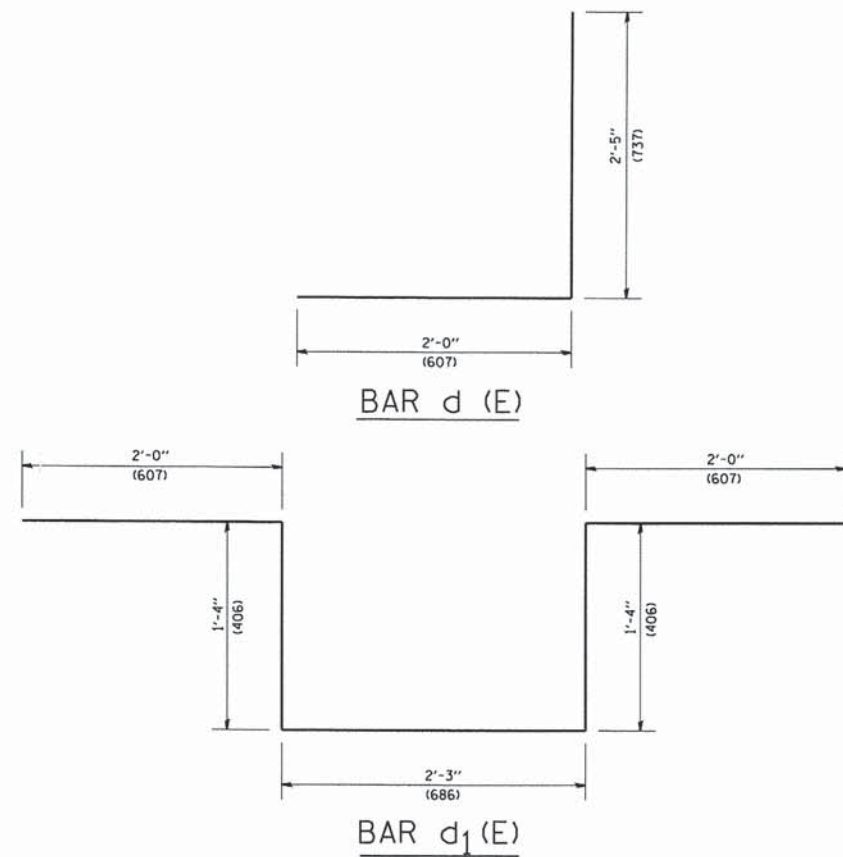
FOUNDATION EXTENSION DETAIL



SECTION A-A

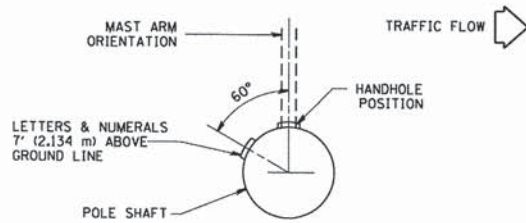
NOTES

- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 4 IN. (100 mm) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS SI. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UM(6 MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.

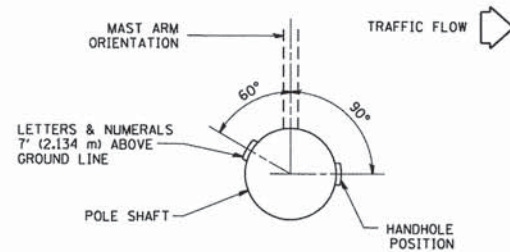


- NOTES**
1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
 2. LEVEL LIGHT POLE PLATES, USING THE FLANGE NUTS, PRIOR TO POURING THE PARAPET WALL. THE TOP OF THE PLATE SHALL BE AT THE SAME ELEVATION AS THE FINISHED CONCRETE PARAPET.
 3. THE COST OF ANCHOR BOLTS, CONDUIT, LEVELLING PLATE AND FOUNDATION IS INCLUDED IN THE COST OF THE BRIDGE STRUCTURE.

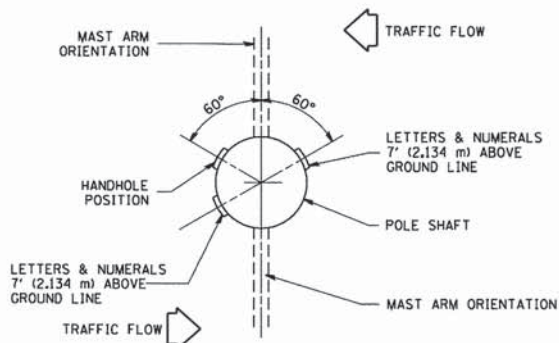
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	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	CONTRACT 63887		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		
	PLOT DATE = 1/4/2008	CHECKED -	REVISED -									
		DATE -	REVISED -									



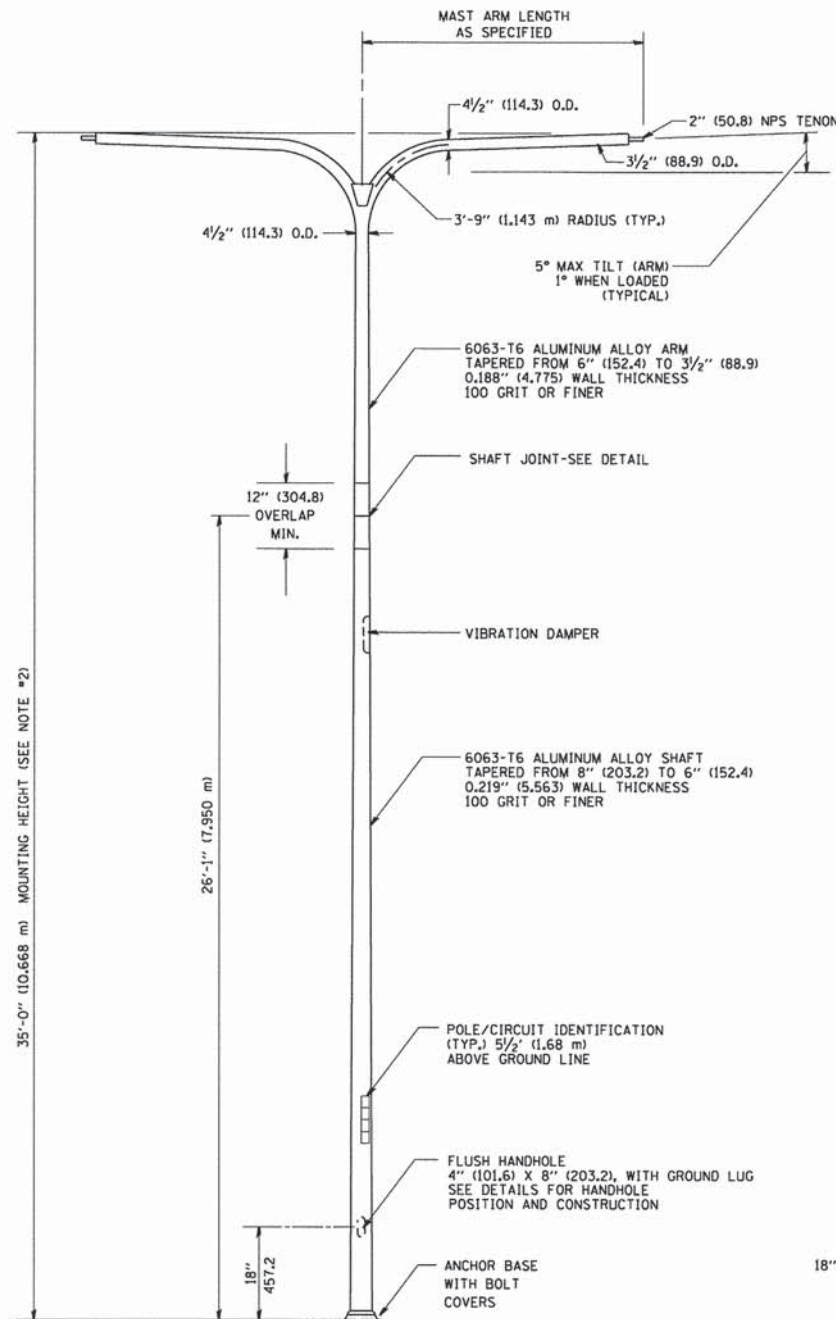
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES MOUNTED ON BRIDGE PARAPET OR BARRIER WALL



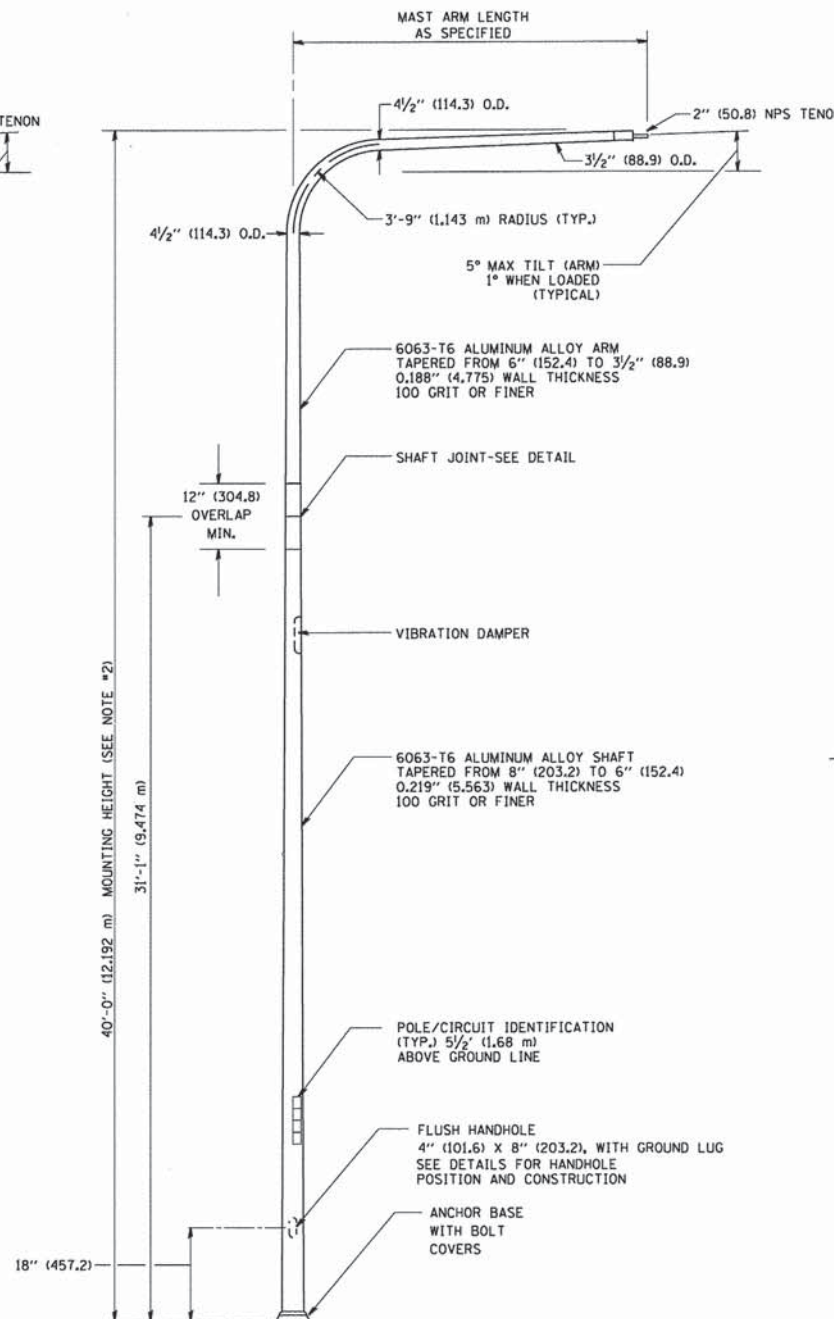
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES



POSITION OF HANDHOLE AND POLE NUMBER FOR TWIN MAST ARM POLES

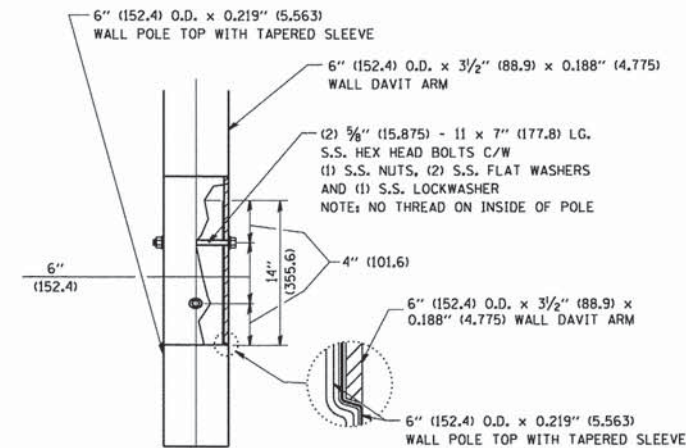


TWIN ARM POLE

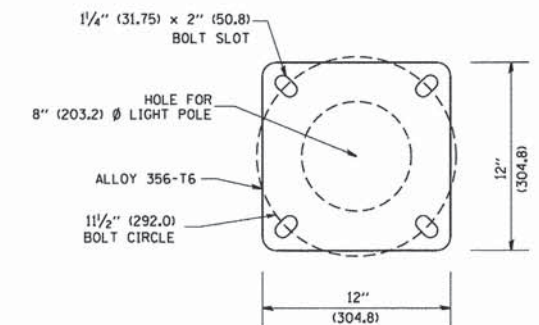


SINGLE ARM POLE

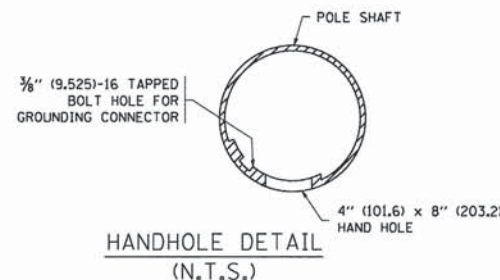
- NOTES:
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
 - MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENON TO THE BOTTOM OF THE ANCHOR BASE.
 - TWO PIECE SHAFT WILL BE MATCHED MARKED AND INTERCHANGEABLE BETWEEN DIFFERENT UNITS. FIELD DRILLING OF THE HOLES WILL NOT BE ALLOWED.
 - THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
 - THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GROUNDING CONNECTOR, BURNDY K2C23, T&B SP-4DL OR APPROVED EQUAL.
 - LIGHT POLES WILL NOT BE INSTALLED WITHOUT MAST ARMS AND LUMINAIRES.
 - LIGHT POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.
 - LIGHTING UNIT IDENTIFICATION NUMBERS SHALL BE INSTALLED BEFORE THE LIGHTING UNIT IS ENERGIZED.



DAVIT ARM CONNECTION
[14" (355.6) OVERLAP SHOWN]

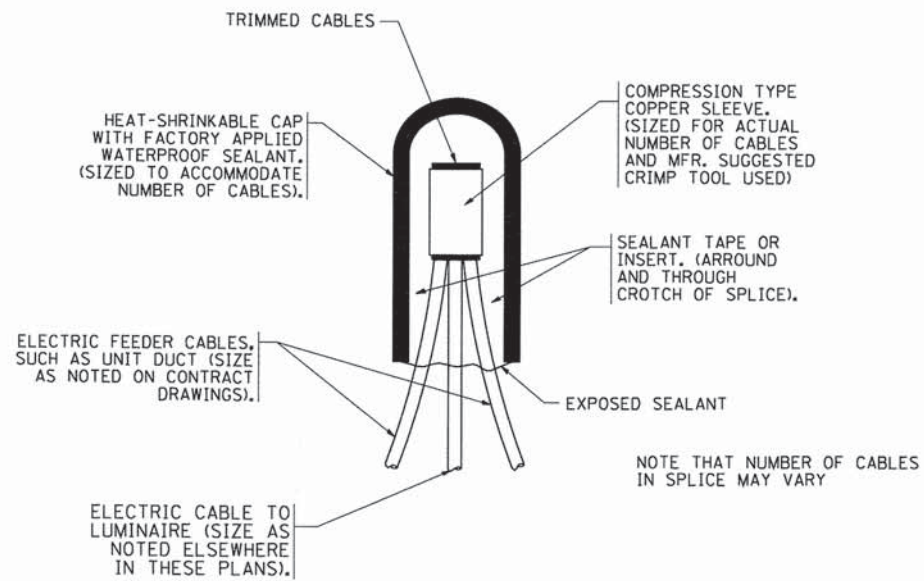


LIGHT POLE BASE PLATE DETAIL
1 1/2" (292.0) BOLT CIRCLE



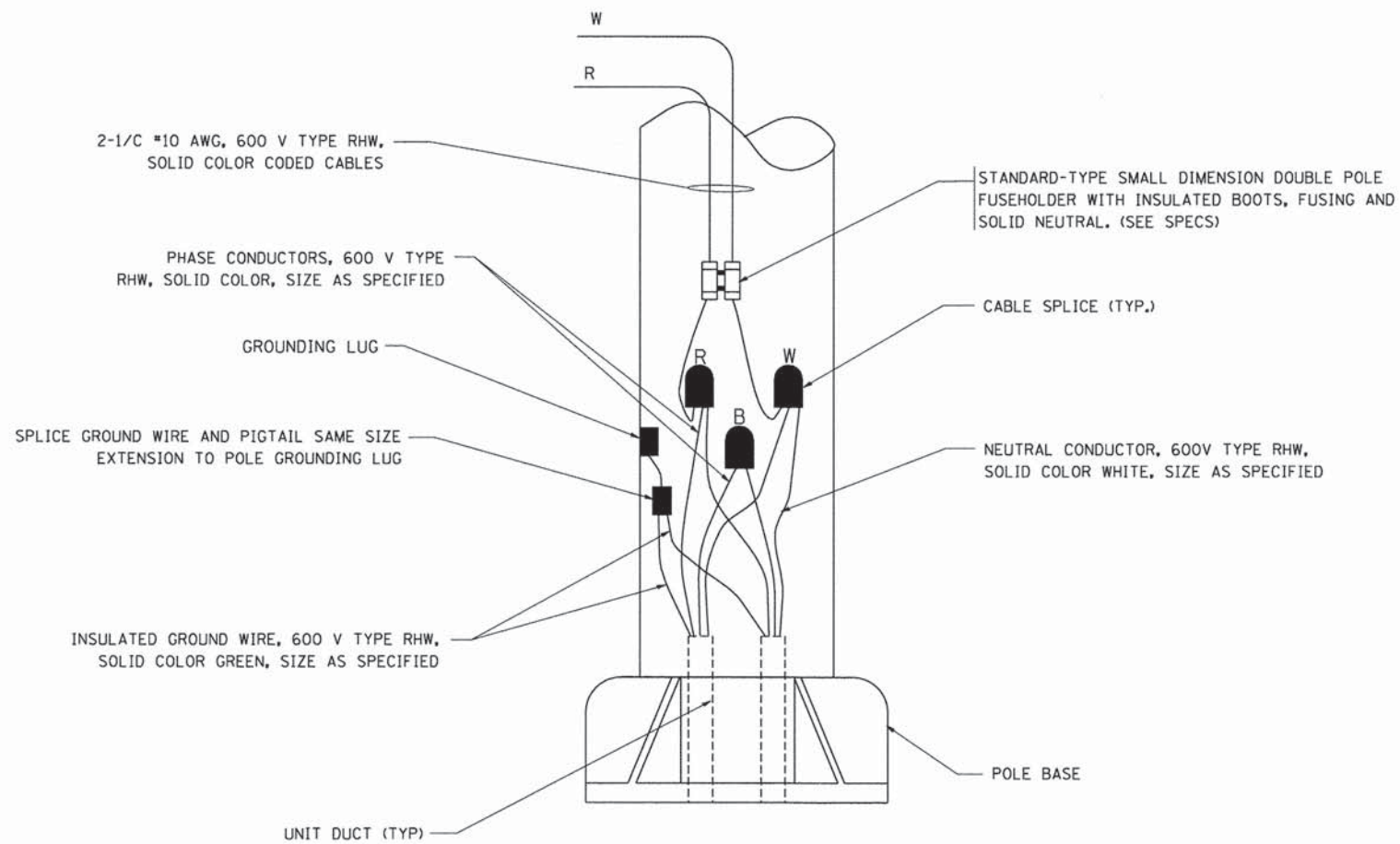
HANDHOLE DETAIL
(N.T.S.)

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							FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



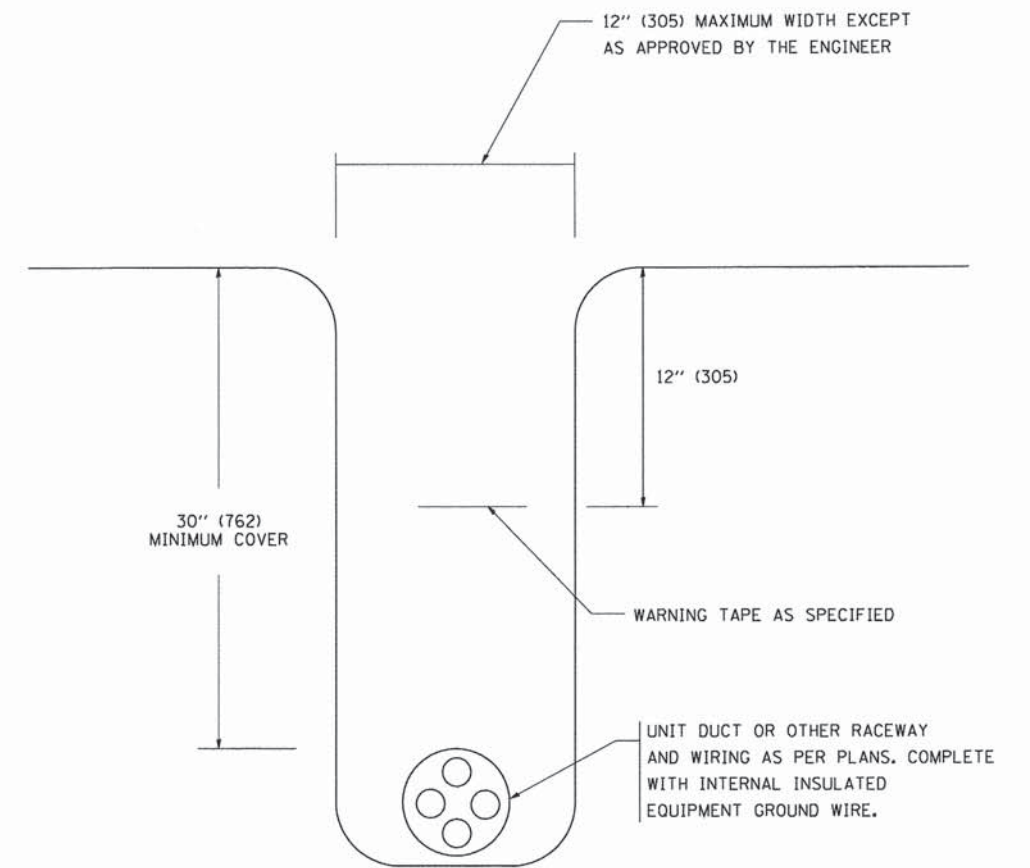
TYPICAL SPLICE DETAIL

N.T.S.



POLE WIRING DETAIL

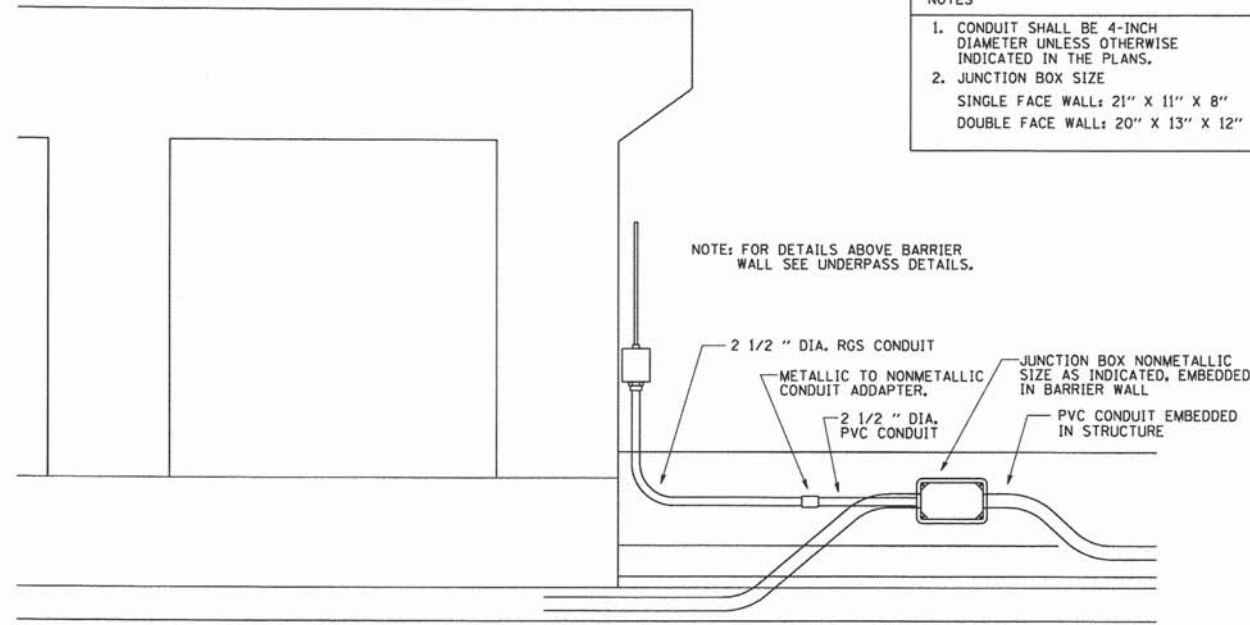
N.T.S.



TYPICAL WIRING IN TRENCH DETAIL

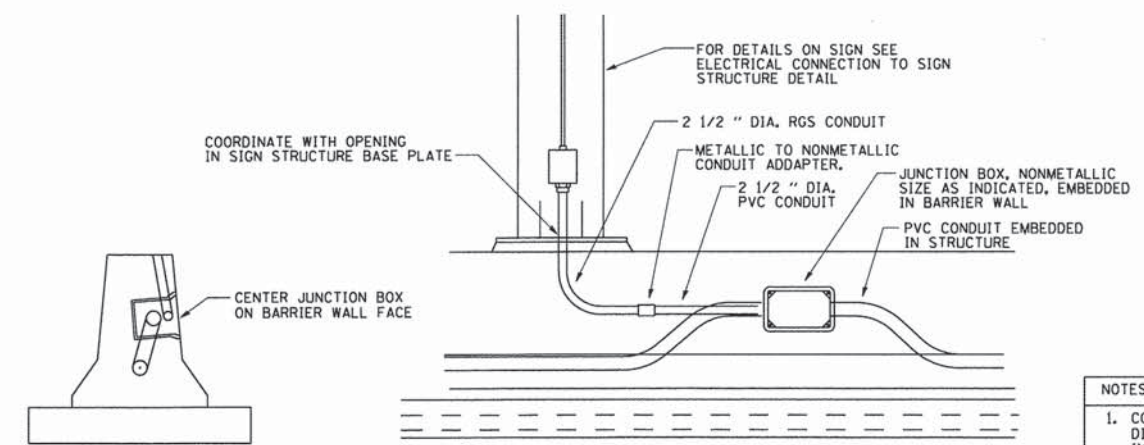
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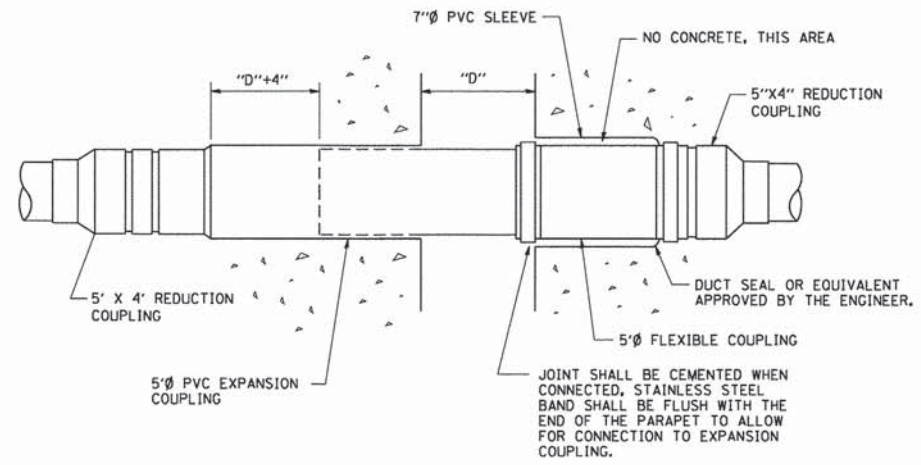
ED - BWD
ELECTRIC CONNECTION TO UNDERPASS LIGHTING

- NOTES
1. CONDUIT SHALL BE 4-INCH DIAMETER UNLESS OTHERWISE INDICATED IN THE PLANS.
 2. JUNCTION BOX SIZE
SINGLE FACE WALL: 21" X 11" X 8"
DOUBLE FACE WALL: 20" X 13" X 12"

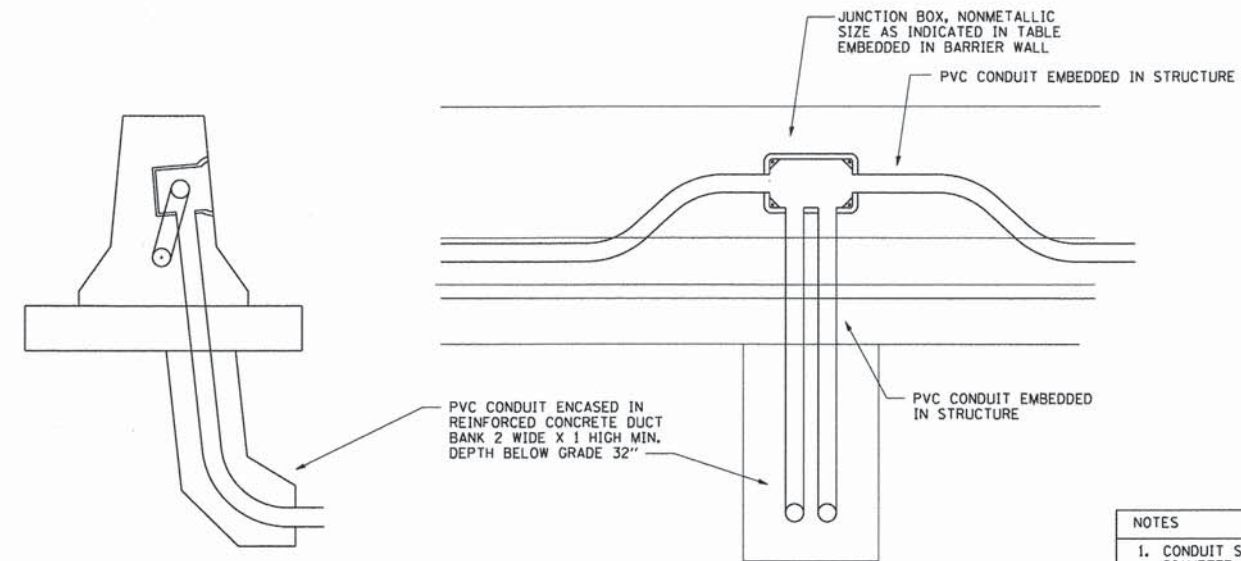


ED - SGN
JUNCTION BOX EMBEDDED IN BARRIER WALL FOR SIGN LIGHTING

- NOTES
1. CONDUIT SHALL BE 4-INCH DIAMETER UNLESS OTHERWISE INDICATED IN THE PLANS.
 2. JUNCTION BOX SIZE
SINGLE FACE WALL: 21" X 11" X 8"
DOUBLE FACE WALL: 20" X 13" X 12"

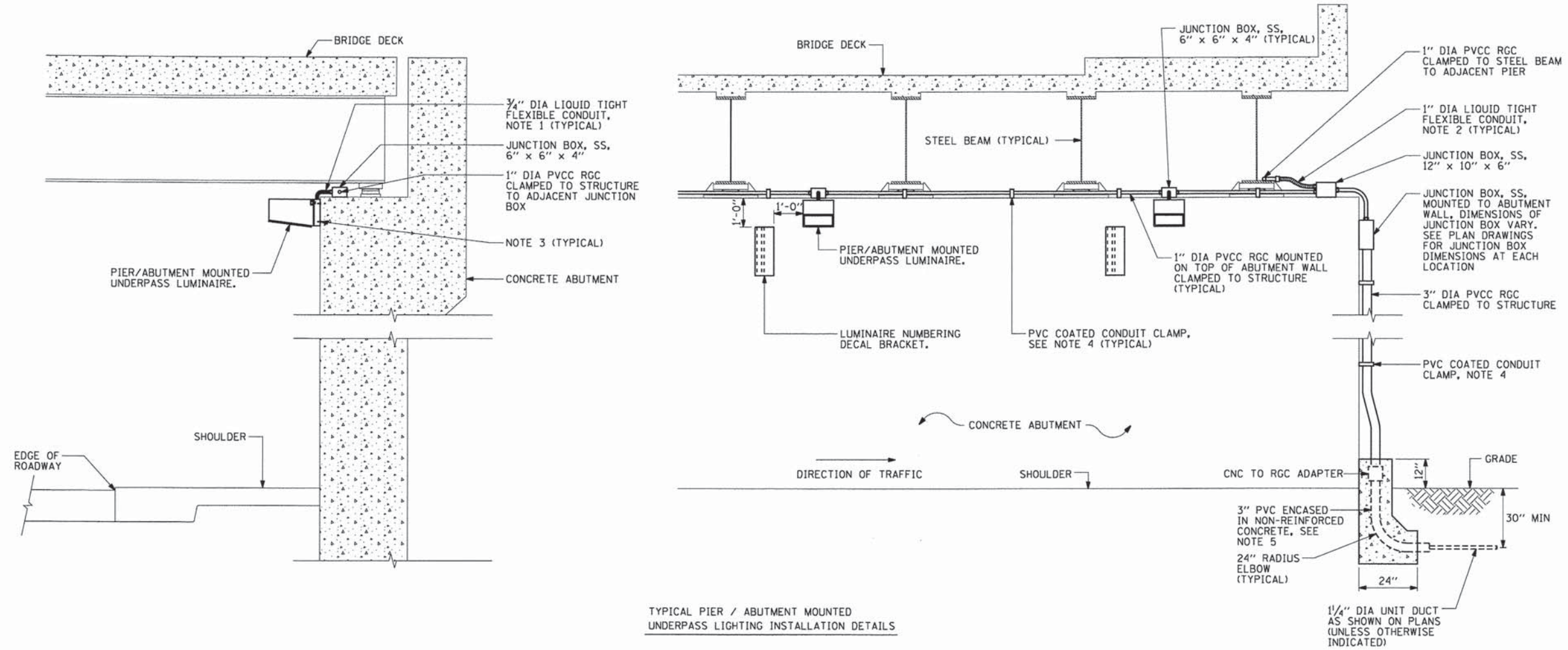


INSTALLATION OF CONDUIT
IN BRIDGE PARAPET EXPANSION JOINT
(N.T.S.)



ED - BW
JUNCTION BOX EMBEDDED IN BARRIER WALL

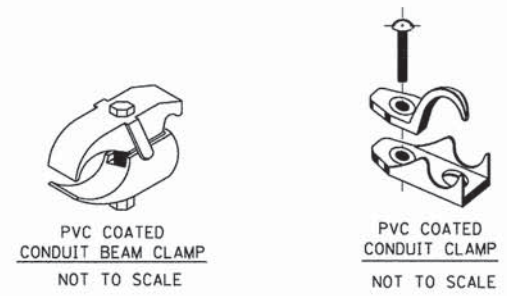
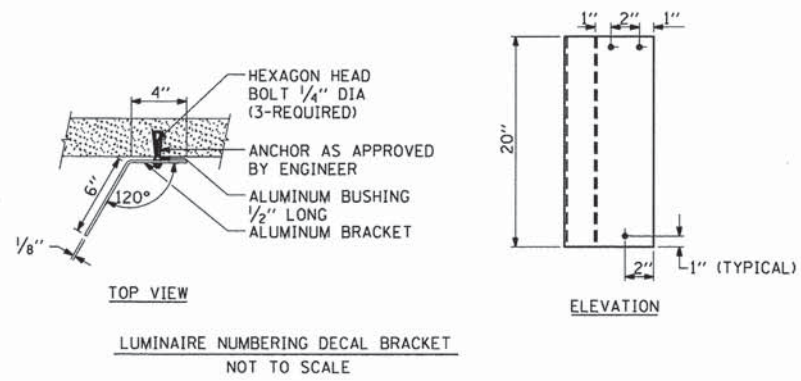
- NOTES
1. CONDUIT SHALL BE 4-INCH DIAMETER UNLESS OTHERWISE INDICATED IN THE PLANS.
 2. JUNCTION BOX SIZE
SINGLE FACE WALL: 21" X 11" X 8"
DOUBLE FACE WALL: 20" X 13" X 12"



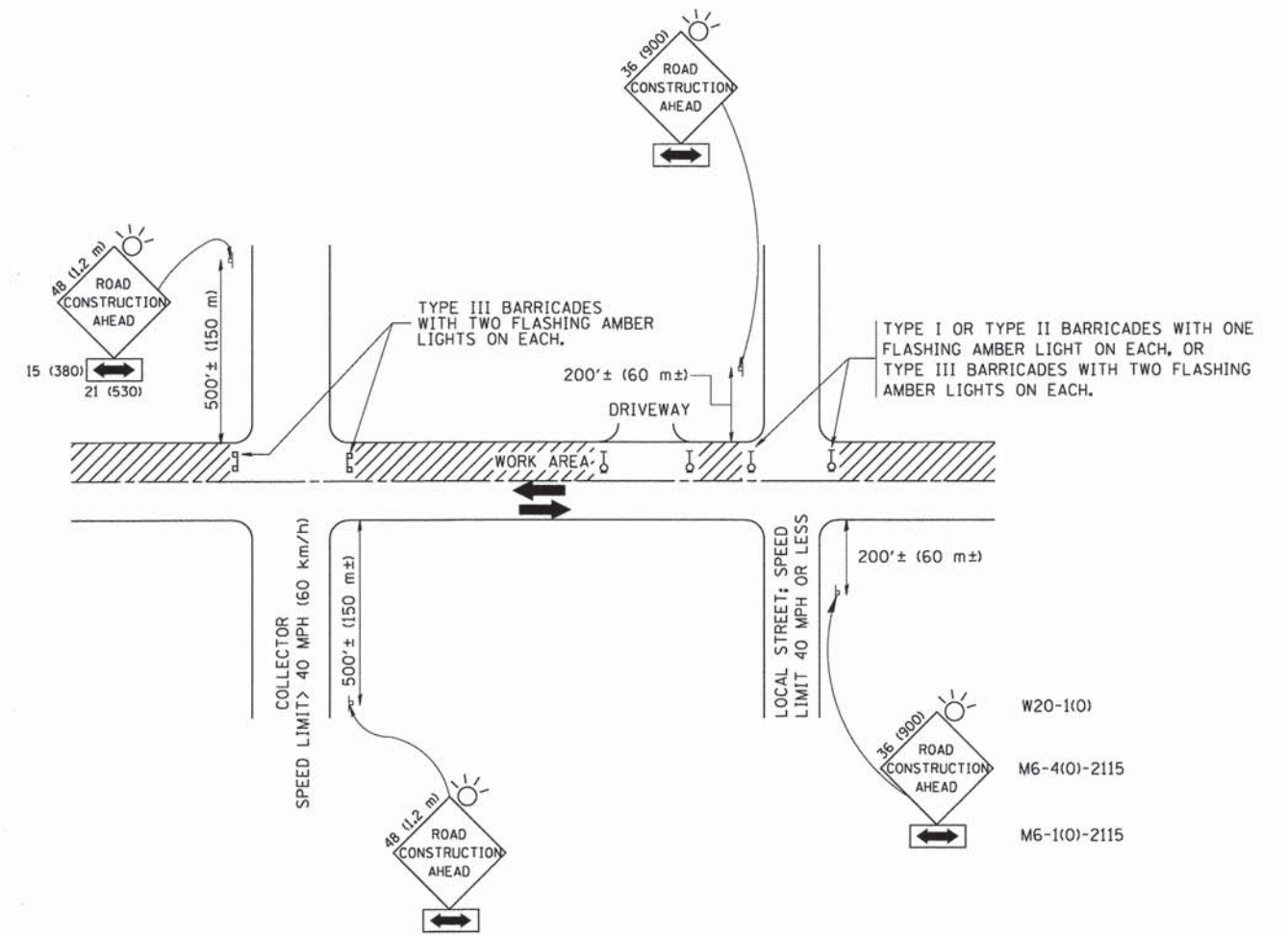
TYPICAL PIER / ABUTMENT MOUNTED UNDERPASS LIGHTING INSTALLATION DETAILS

NOTES:

- LIQUID TIGHT FLEXIBLE METAL CONDUIT, MAXIMUM LENGTH 6'-0", TYPICAL FOR EACH INSTANCE AS SHOWN. PROVIDE PVC COATED RIGID GALVANIZED STEEL CONDUIT AS REQUIRED NOT TO EXCEED 6'-0" OF FLEXIBLE LIQUID TIGHT METAL CONDUIT. LIQUID TIGHT FLEXIBLE METAL CONDUIT WILL BE INCLUDED IN THE COST OF THE CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED PAY ITEM EXCEPT THAT THE COST OF THE 3/4" DIA. RIGID STEEL CONDUIT AND 3/4" DIA. FLEXIBLE CONDUIT SHALL BE INCLUDED IN THE LUMINAIRE INSTALLATION.
- UNDERPASS LUMINAIRE MOUNTED TO FACE OF PIER OR ABUTMENT WALL. MOUNTING HEIGHT OF 1" BELOW THE TOP OF PIER OR ABUTMENT WALL TYPICAL FOR ALL PIER/ABUTMENT MOUNTED UNDERPASS LUMINAIRES UNLESS OTHERWISE NOTED.
- EXPANSION ANCHOR, POWDER ACTUATED FASTENERS WILL NOT BE ALLOWED, EXPANSION ANCHOR MUST BE SIZED IN ACCORDANCE WITH MANUFACTURERS REQUIREMENTS.
- SECURE THE CONDUIT WITH PVC COATED CONDUIT CLAMPS OR CONDUIT BEAM CLAMPS AS SHOWN AT 5'-0" INTERVALS FOR LATERALS AND WITHIN 2'-0" MAXIMUM FROM ANY JUNCTION BOX, FLEXIBLE CONDUIT, OR CHANGE IN DIRECTION. ALL PVC COATED CONDUIT CLAMPS OR BEAM CLAMPS SHALL BE INCLUDED WITH THE COST OF THE "CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED" PAY ITEM.
- THE CONCRETE ENCASED CONDUIT TRANSITION SHALL BE INCLUDED IN THE COST OF THE GALVANIZED RIGID STEEL CONDUIT ITEMS.
- ALL CONDUIT ATTACHED TO STRUCTURE SHALL BE PVC COATED RIGID STEEL CONDUIT (PVCC RGC) TYPICAL.



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	PLOT DATE = 1/4/2008	CHECKED -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

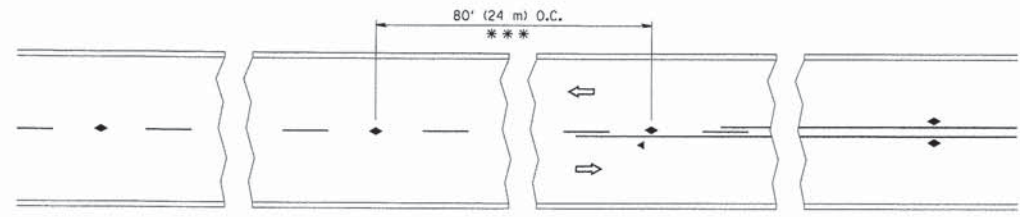
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	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

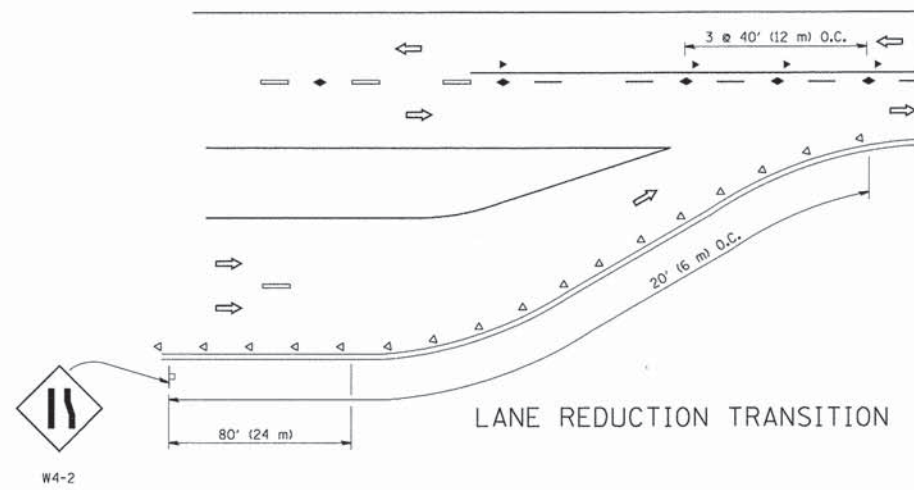
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F.A. RTE. 2714	SECTION	COUNTY	TOTAL SHEETS 240	SHEET NO. 215
TC-10		CONTRACT	63887	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

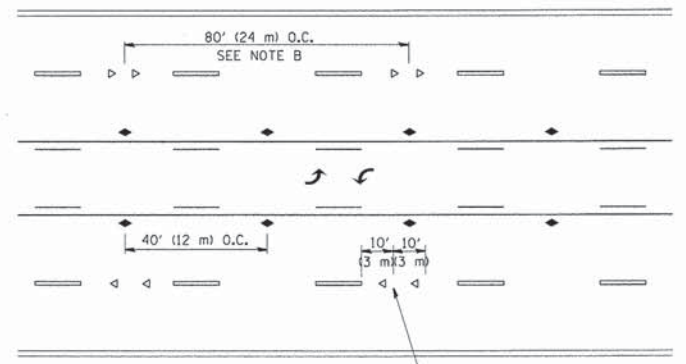


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

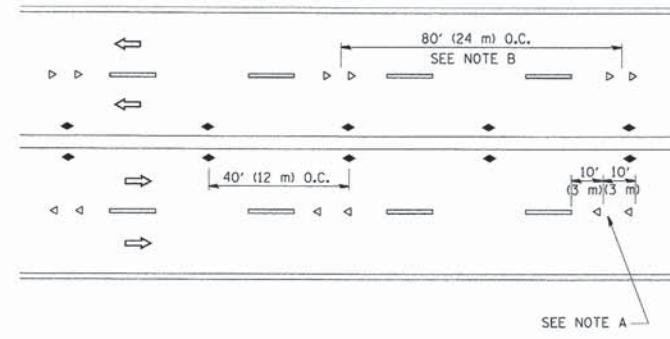
TWO-LANE/TWO-WAY



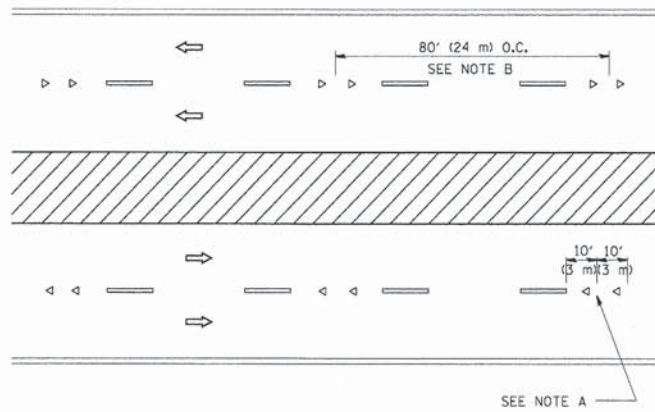
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

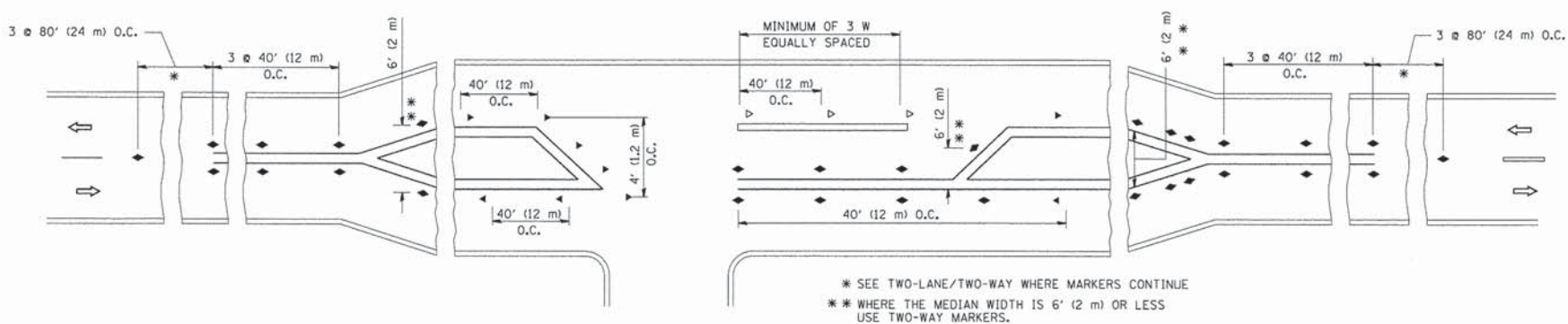
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

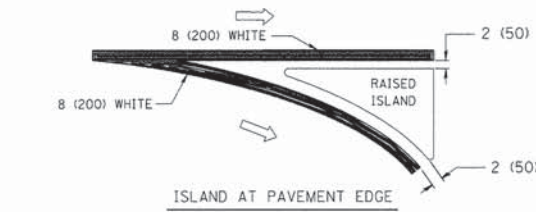
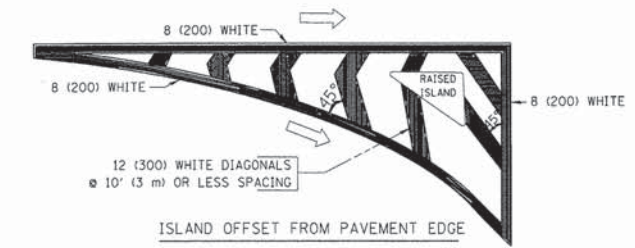
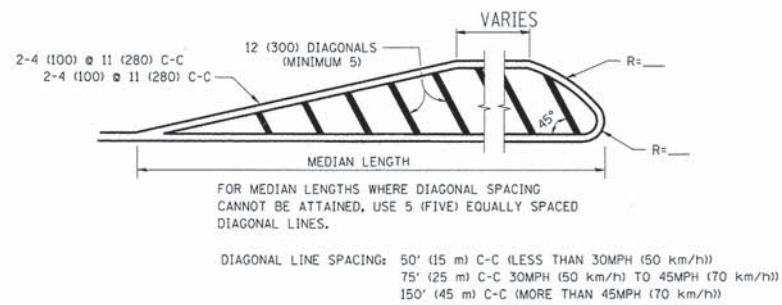
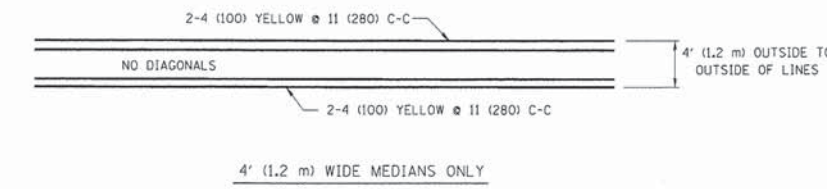
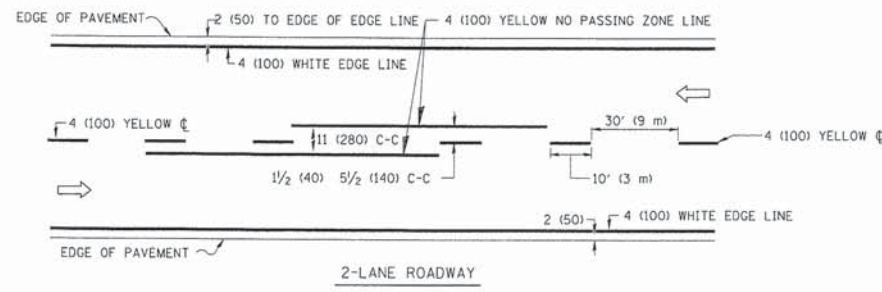
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	PLOT DATE = 3/2/2011	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

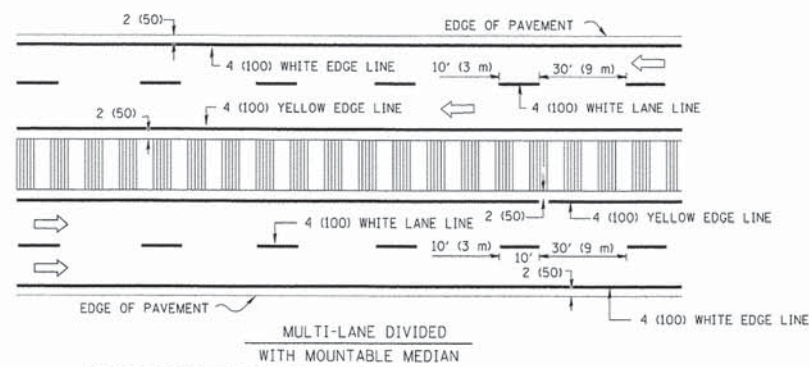
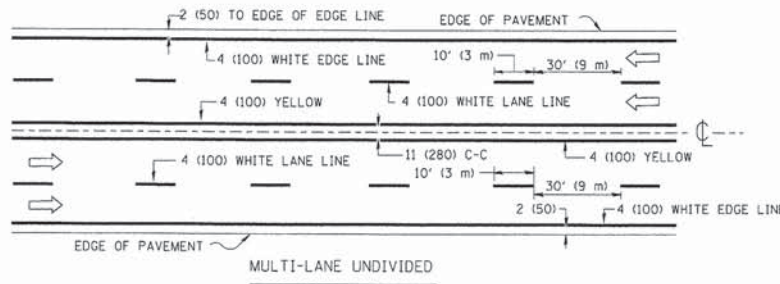
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 2714	SECTION	COUNTY	TOTAL SHEETS 240	SHEET NO. 216
TC-11		CONTRACT		63887
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

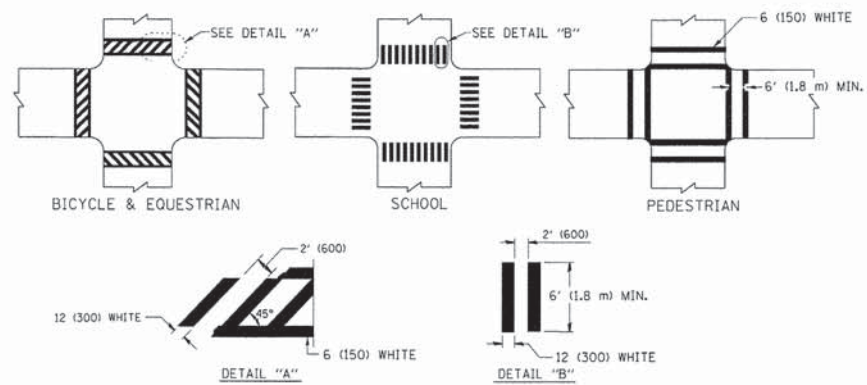


TYPICAL ISLAND MARKING

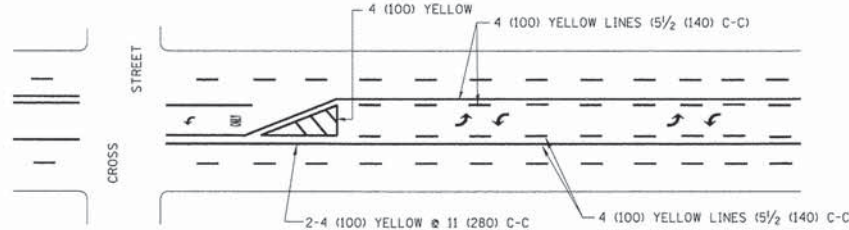


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

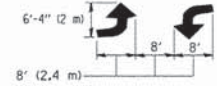
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

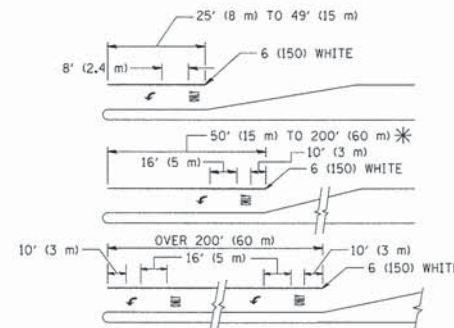


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

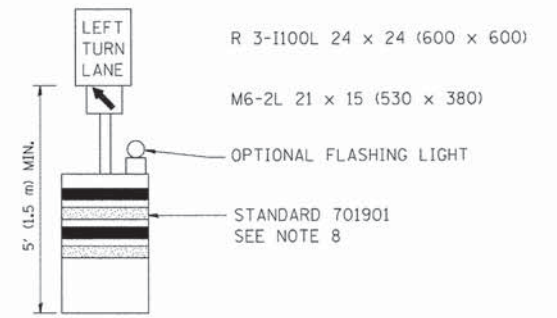
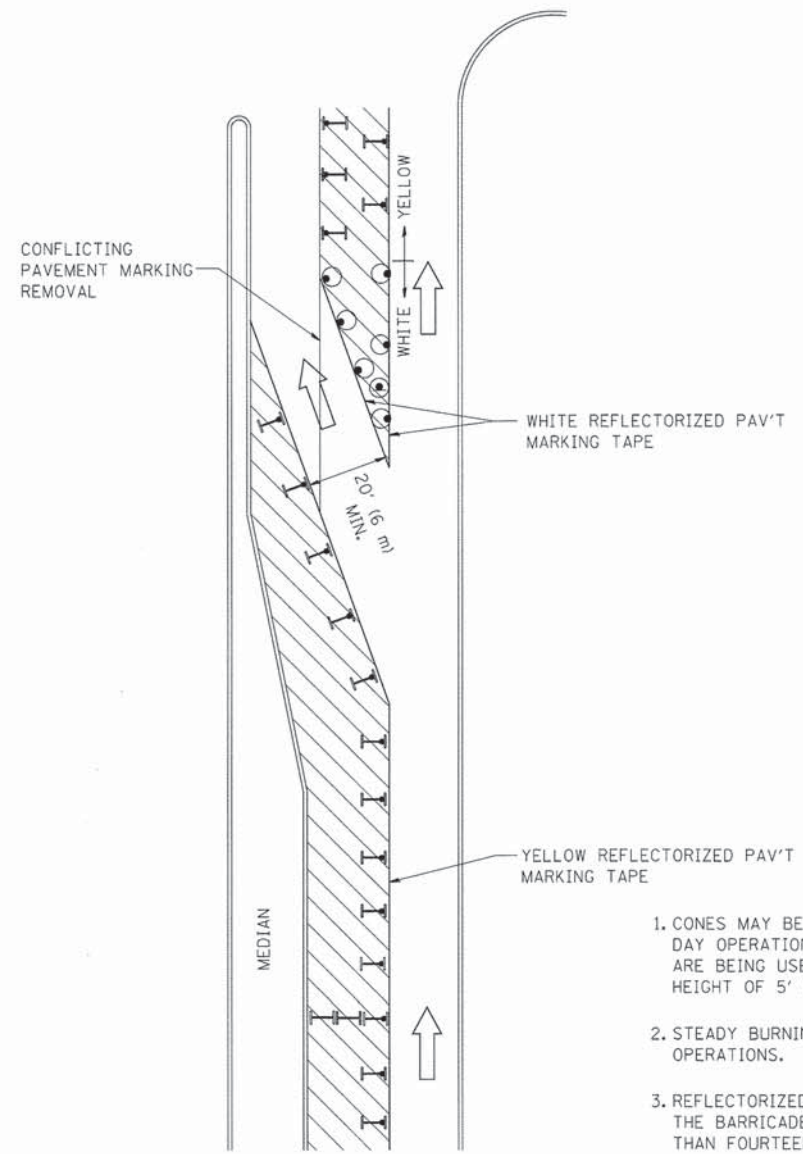
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.









GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

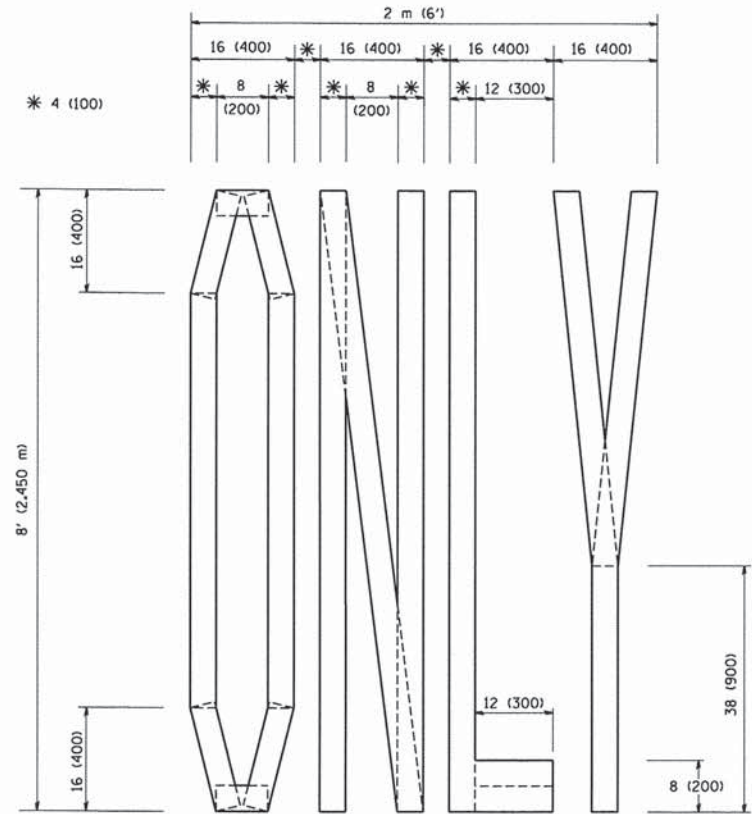
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	PLOT SCALE = 49,9999 ' / IN.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 9/14/2009	REVISED -T, RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

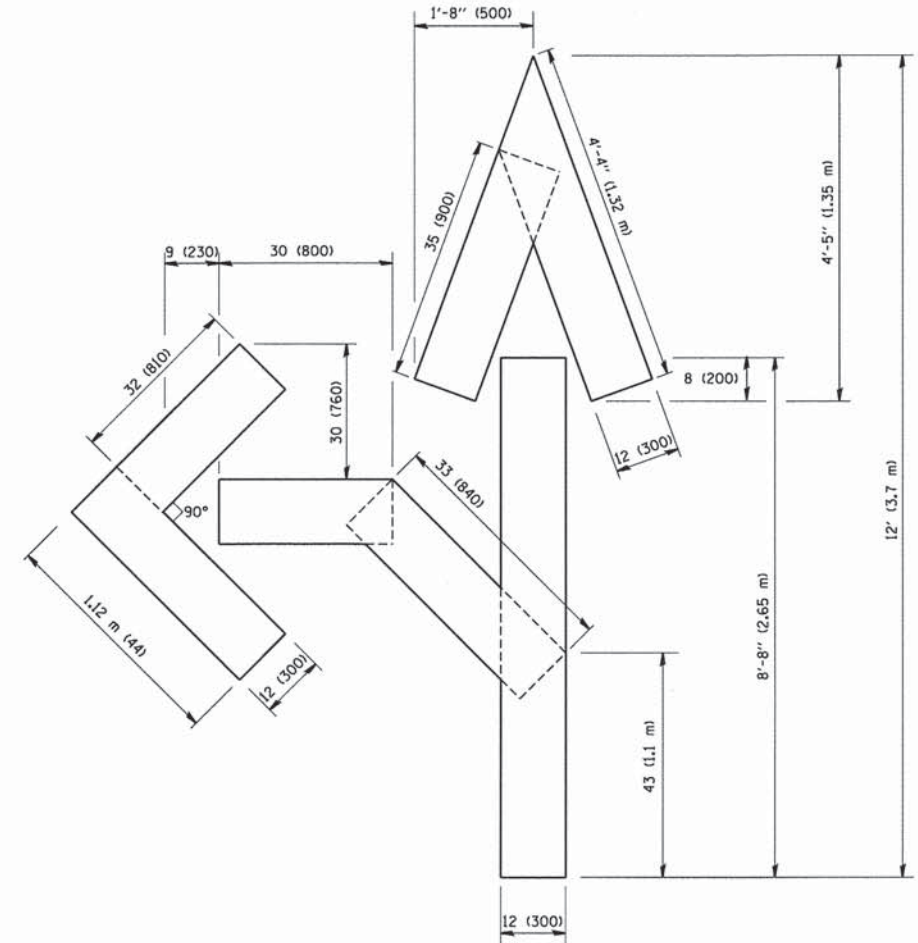
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

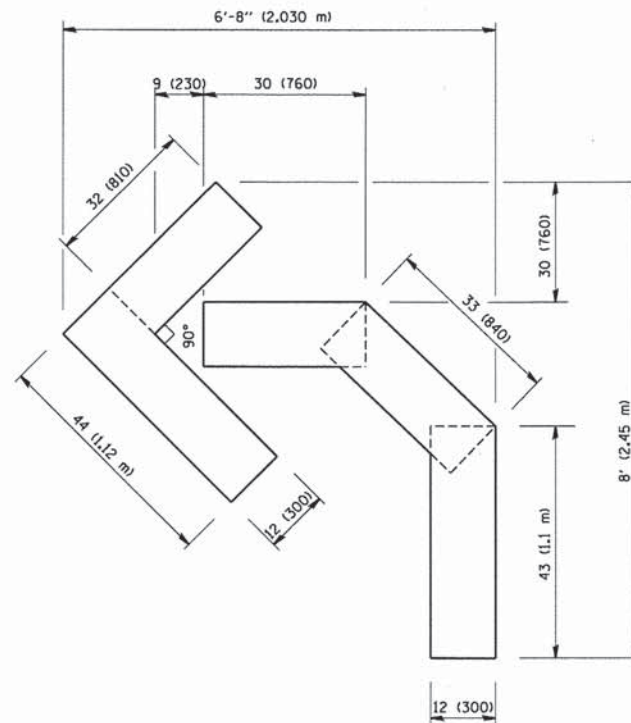
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714			240	218
TC-14		CONTRACT	63887	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\tcl6.dgn	USER NAME = geglianobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE = 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 2714	SECTION TC-16	COUNTY	TOTAL SHEETS 240	SHEET NO. 219
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		CONTRACT 63887		

ROUTE MARKERS

FOR U.S. ROUTES
MI-40-2424

FOR ILLINOIS ROUTES
MI-50-2424

R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-1-2115

M6-3-2115

CARDINAL DIRECTION & DETOUR SIGNS

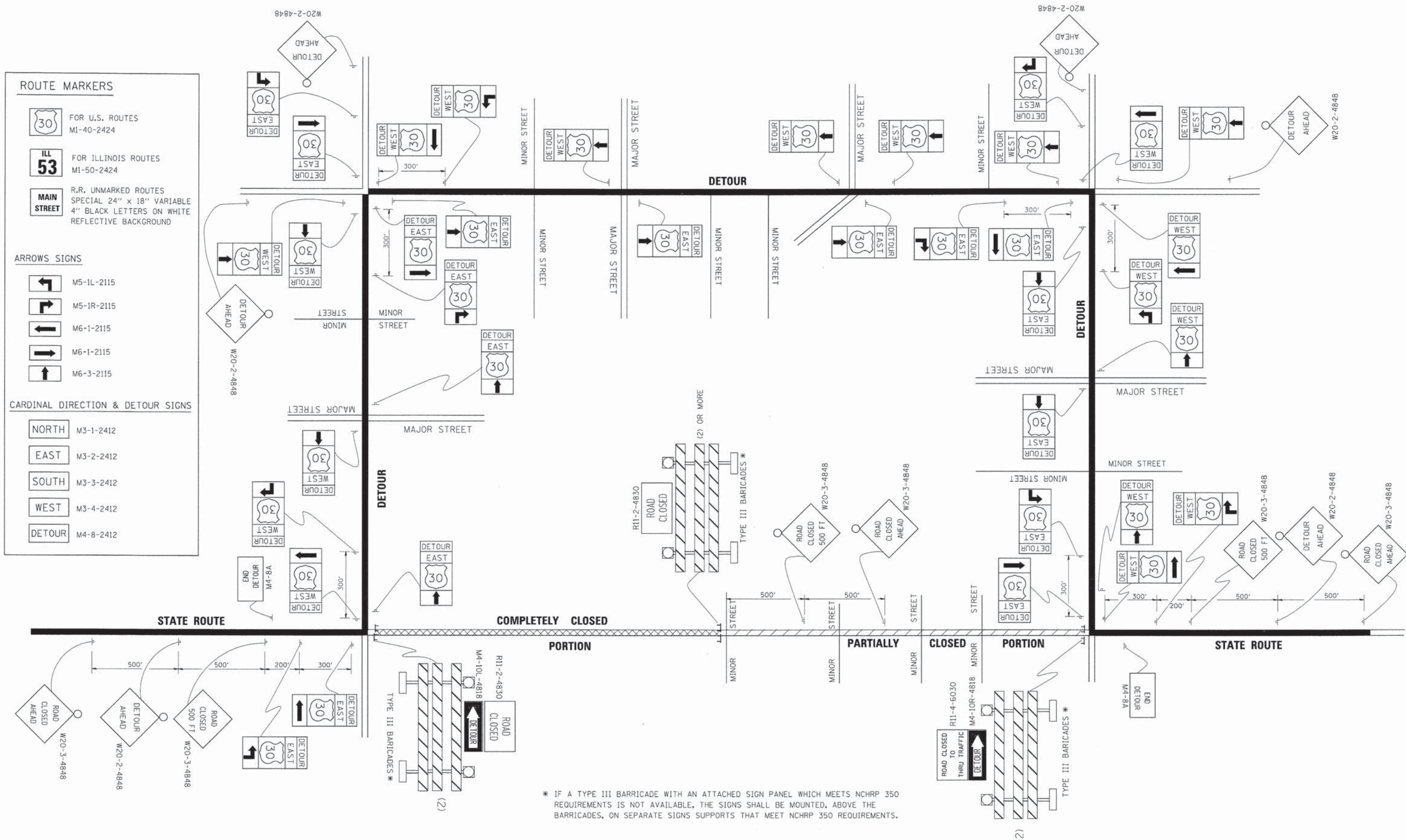
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-8-2412



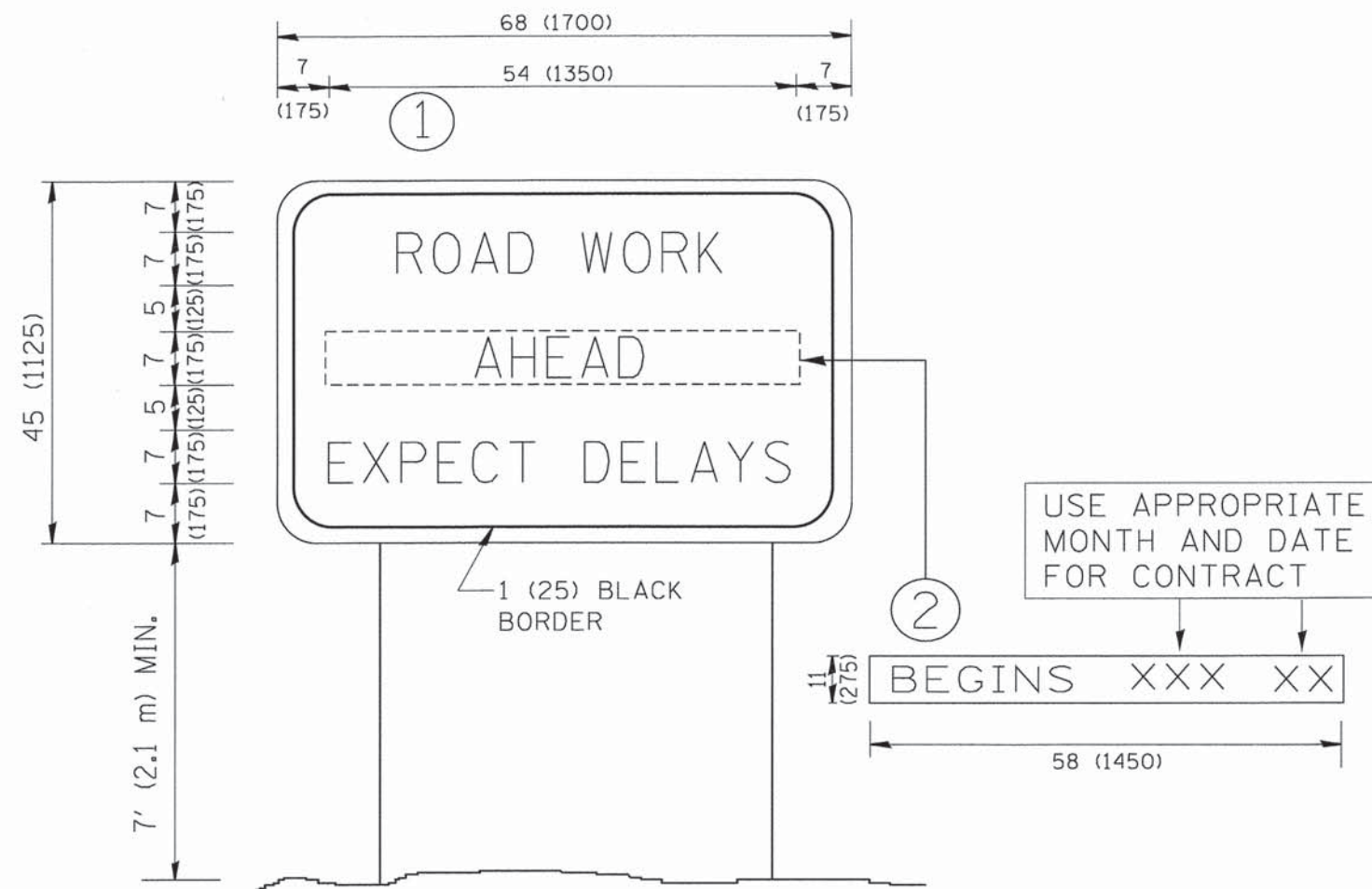
* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - 10-18-02
c:\pwork\PW1001\DRIVAKOSGN\d0108315\21.dgn		DRAWN -	REVISED - R. BORO 09-14-09
	PLOT SCALE = 49.9999' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/14/2009	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETOUR SIGNING FOR CLOSING STATE HIGHWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714			240	220
TC-21		CONTRACT	63887	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gaglionobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 2714	SECTION	COUNTY	TOTAL SHEETS 240	SHEET NO. 221
TC-22		CONTRACT	63887	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = gaglianobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
ci:\pw_work\pwwork\gaglianobt\d0100315\to26.dgn		DRAWN -	REVISED -
	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/13/2012	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

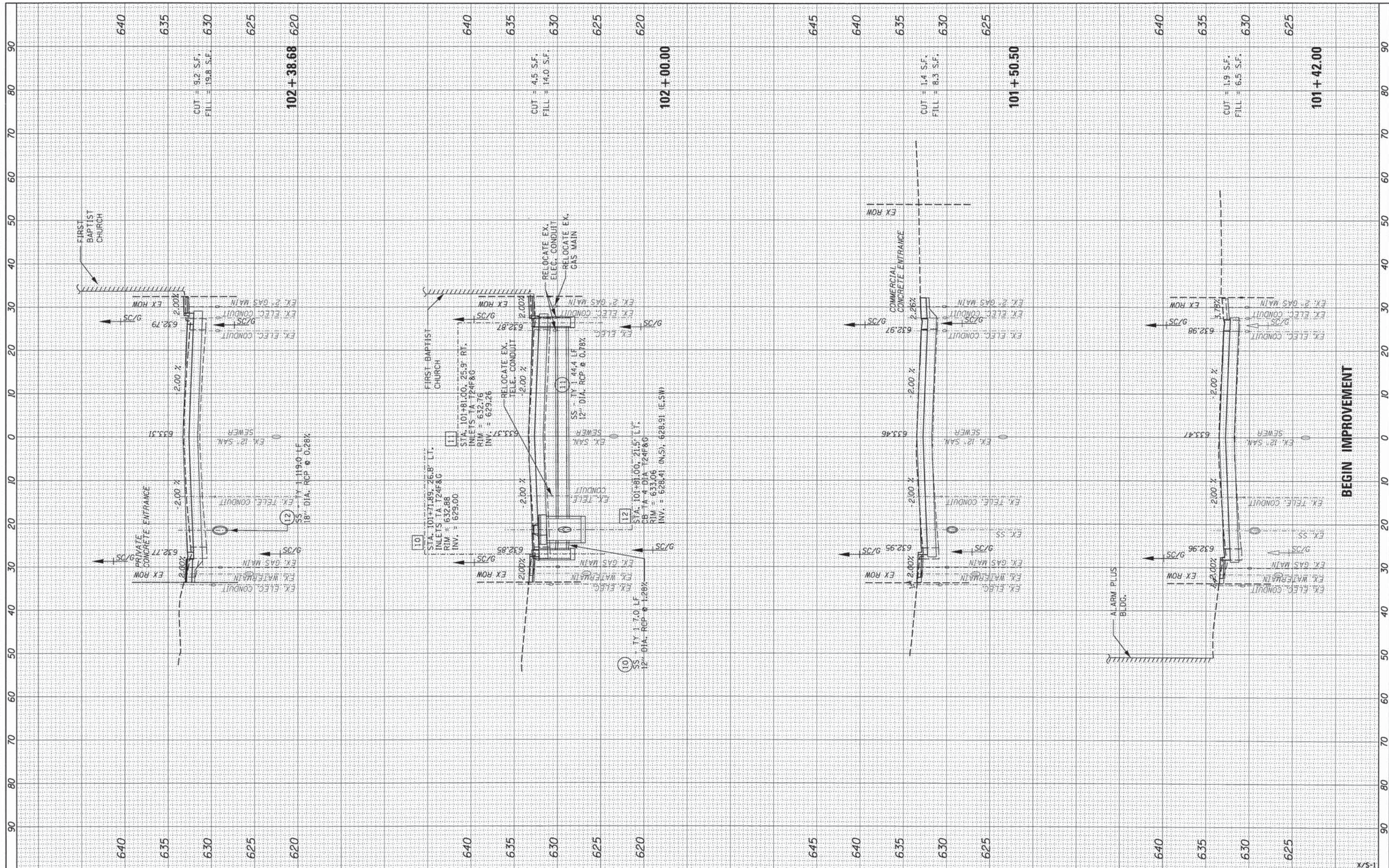
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714			240	222
TC-26		CONTRACT	63887	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FINAL SURVEY	SURVEYED	BY	DATE
PLOTTED	PLOTTED		
NOTE BOOK	NOTE BOOK		
AREAS CHECKED	AREAS CHECKED		
NO.	NO.		

ORIGINAL SURVEY	SURVEYED	BY	DATE
PLOTTED	PLOTTED		
NOTE BOOK	NOTE BOOK		
AREAS CHECKED	AREAS CHECKED		
NO.	NO.		



Globetrotters
Engineering Corporation

FILE NAME: Default

USER NAME = stephen.schuh	DESIGNED - MA	REVISED -
PLLOT SCALE = 10.0000' / 1" =	DRAWN - MA	REVISED -
PLLOT DATE = 3/21/2014	CHECKED - RH	REVISED -
	DATE = 3-24-2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

25TH AVE. CROSS SECTIONS
25TH AVENUE GRADE SEPARATION

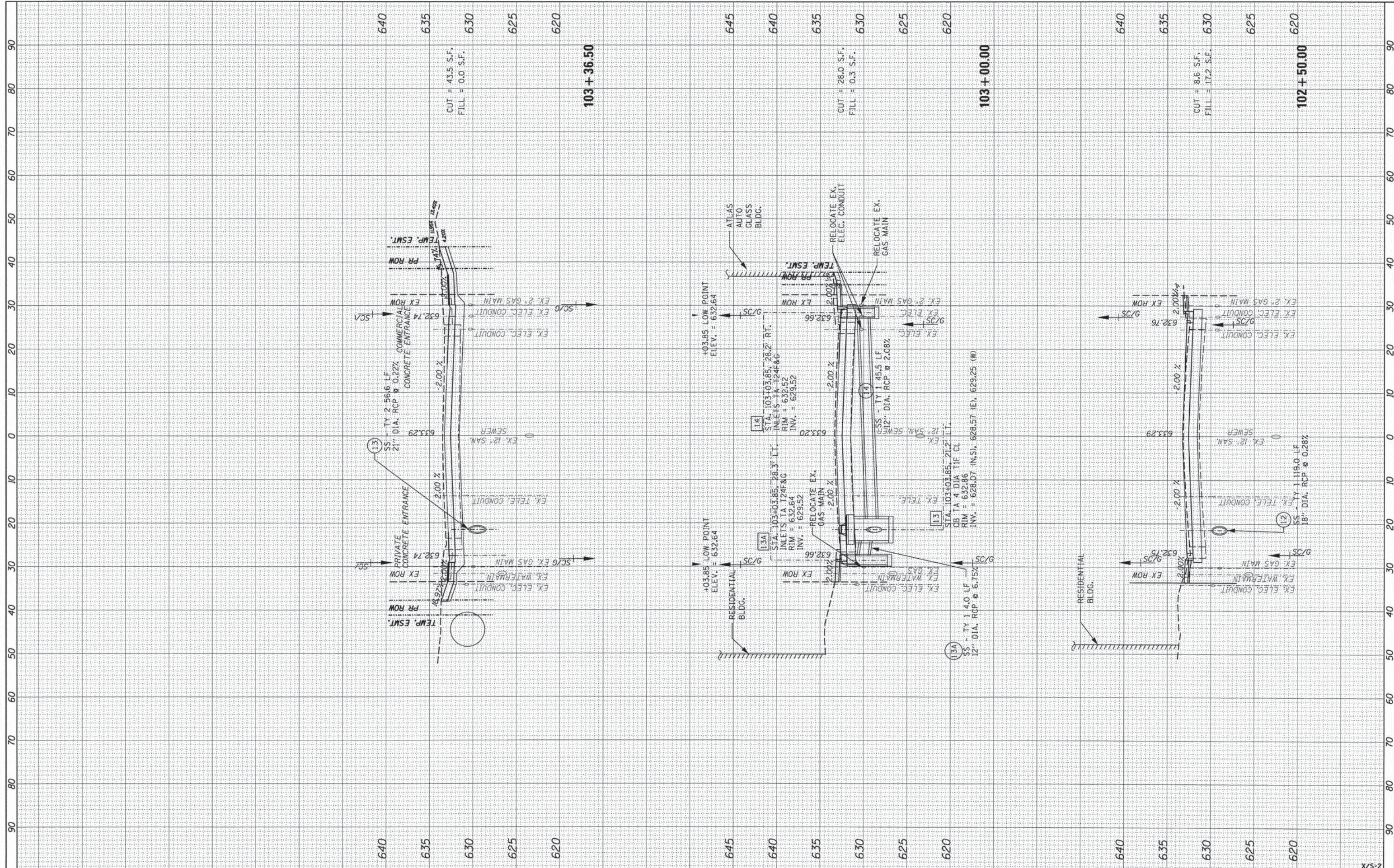
SCALE: 1" = 10' H, 1" = 5' V

SHEET 1 OF 14 SHEETS STA. 101+42.00 TO STA. 102+38.68

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	99-00094-00-GS	COOK	240	223
				CONTRACT NO. 63887
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		



Globetrotters
Engineering Corporation

USER NAME = stephenschuh	DESIGNED - MA	REVISED -
PLOT SCALE = 10.0000' / 1" in.	DRAWN - MA	REVISED -
PLOT DATE = 3/21/2014	CHECKED - RH	REVISED -
	DATE - 3-24-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

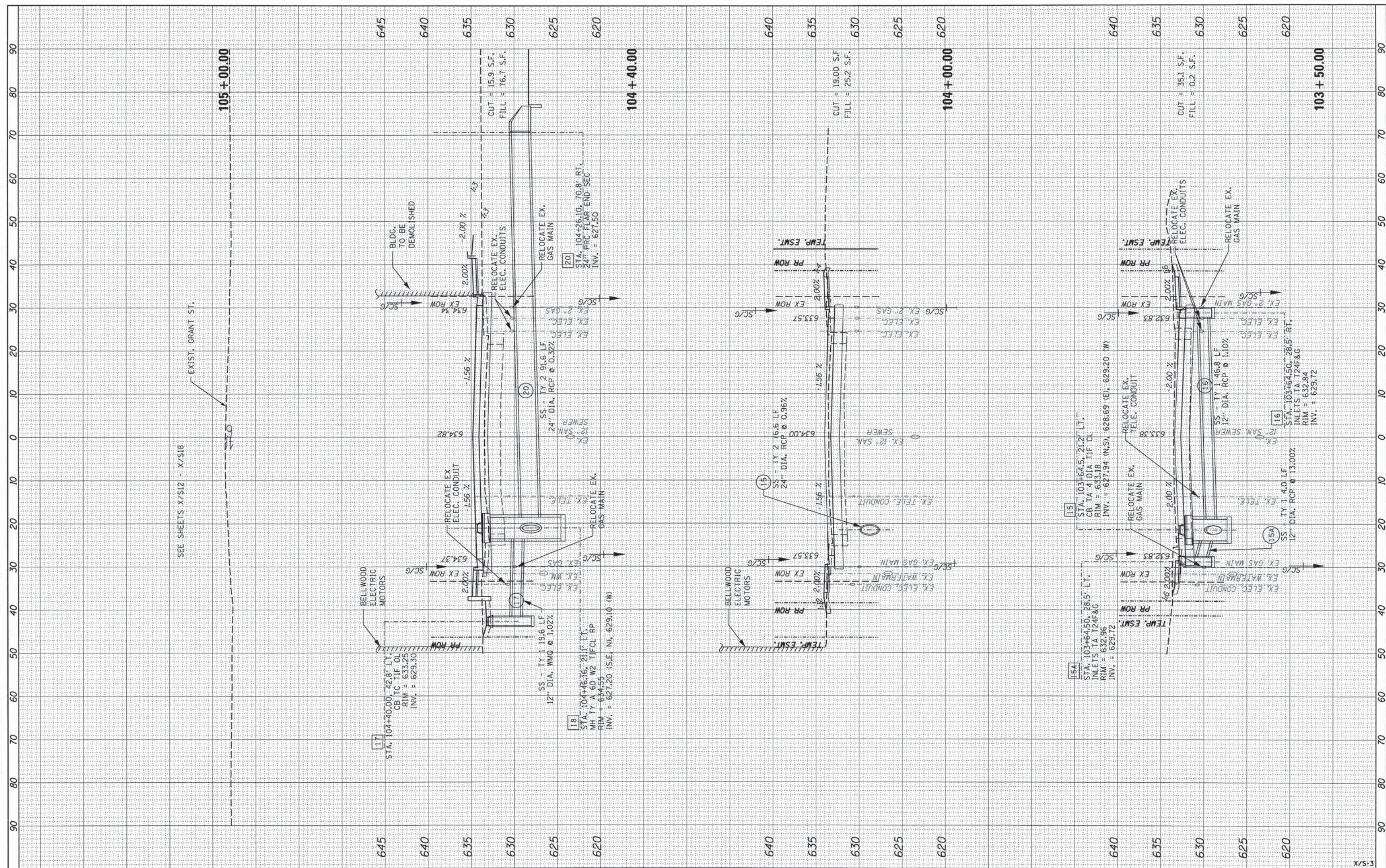
25TH AVE. CROSS SECTIONS	
25TH AVENUE GRADE SEPARATION	
SCALE: 1" = 10' H 1" = 5' V	SHEET 2 OF 14 SHEETS
STA. 102+50.00 TO STA. 103+36.50	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	99-00094-00-GS	COOK	240	224
CONTRACT NO. 63887				
ILLINOIS FED. AID PROJECT				

X/5-2

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		



Globetrotters
Engineering Corporation

USER NAME = stephen.schuh	DESIGNED - MA	REVISED -
PLLOT SCALE = 10.0000' / 1"	DRAWN - MA	REVISED -
PLLOT DATE = 3/21/2014	CHECKED - RH	REVISED -
	DATE - 3-24-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

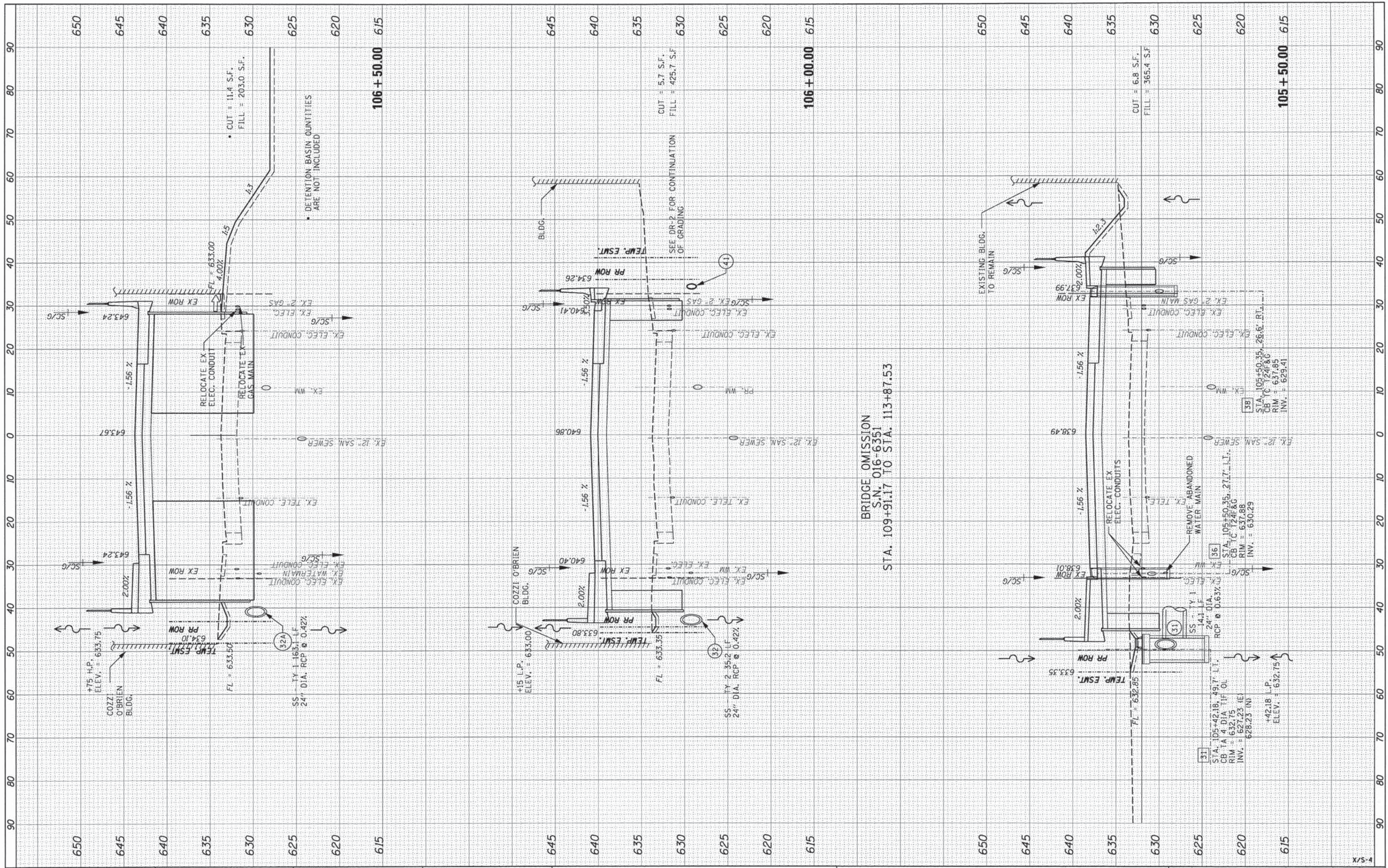
**25TH AVE. CROSS SECTIONS
25TH AVENUE GRADE SEPARATION**

SCALE: 1" = 10'-0"
SHEET 3 OF 14 SHEETS STA. 103+50.00 TO STA. 105+00.00

F.A.U. RTE. 2714	SECTION 99-00094-00-GS	COUNTY COOK	TOTAL SHEETS 240	SHEET NO. 225
CONTRACT NO. 63887				ILLINOIS FED. AID PROJECT

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
NO.		
AREAS CHECKED		
AREAS CHECKED		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
NO.		
AREAS CHECKED		
AREAS CHECKED		



BRIDGE OMISSION
S.N. 016-6351
STA. 109+91.17 TO STA. 113+87.53



USER NAME = mohammed.ali
PLOT SCALE = 1/8" = 10'-0" / 1" / 16"
PLOT DATE = 7/30/2014

DESIGNED - MA
DRAWN - MA
CHECKED - RH
DATE - 3-24-2014

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

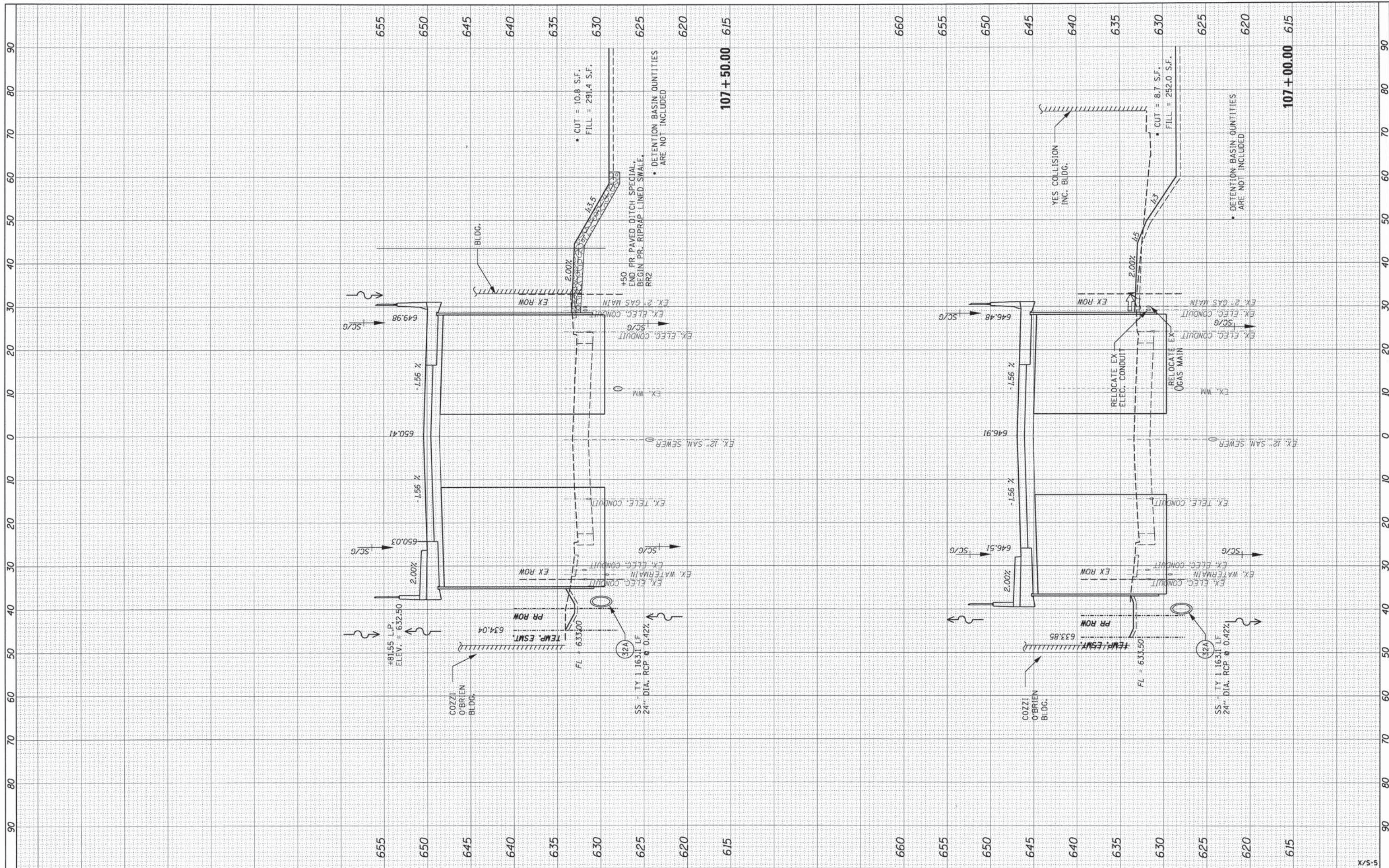
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SHEET 4 OF 14 SHEETS
STA. 105+50.00 TO STA. 106+50.00

25TH AVE. CROSS SECTIONS
25TH AVENUE GRADE SEPARATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	99-00094-00-GS	COOK	240	226
				CONTRACT NO. 63887
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	BY	DATE
SURVEYED		
NOTE BOOK		
NO.		
AREAS CHECKED		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
NOTE BOOK		
NO.		
AREAS CHECKED		



USER NAME = mhammed.o.l	DESIGNED - MA	REVISED -
PLOT SCALE = 10:0000' / 1" =	DRAWN - MA	REVISED -
PLOT DATE = 7/30/2014	CHECKED - RH	REVISED -
	DATE - 3-24-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE: 1" = 10' H 3" V	SHEET 5 OF 14 SHEETS	STA. 107+00.00 TO STA. 107+50.00
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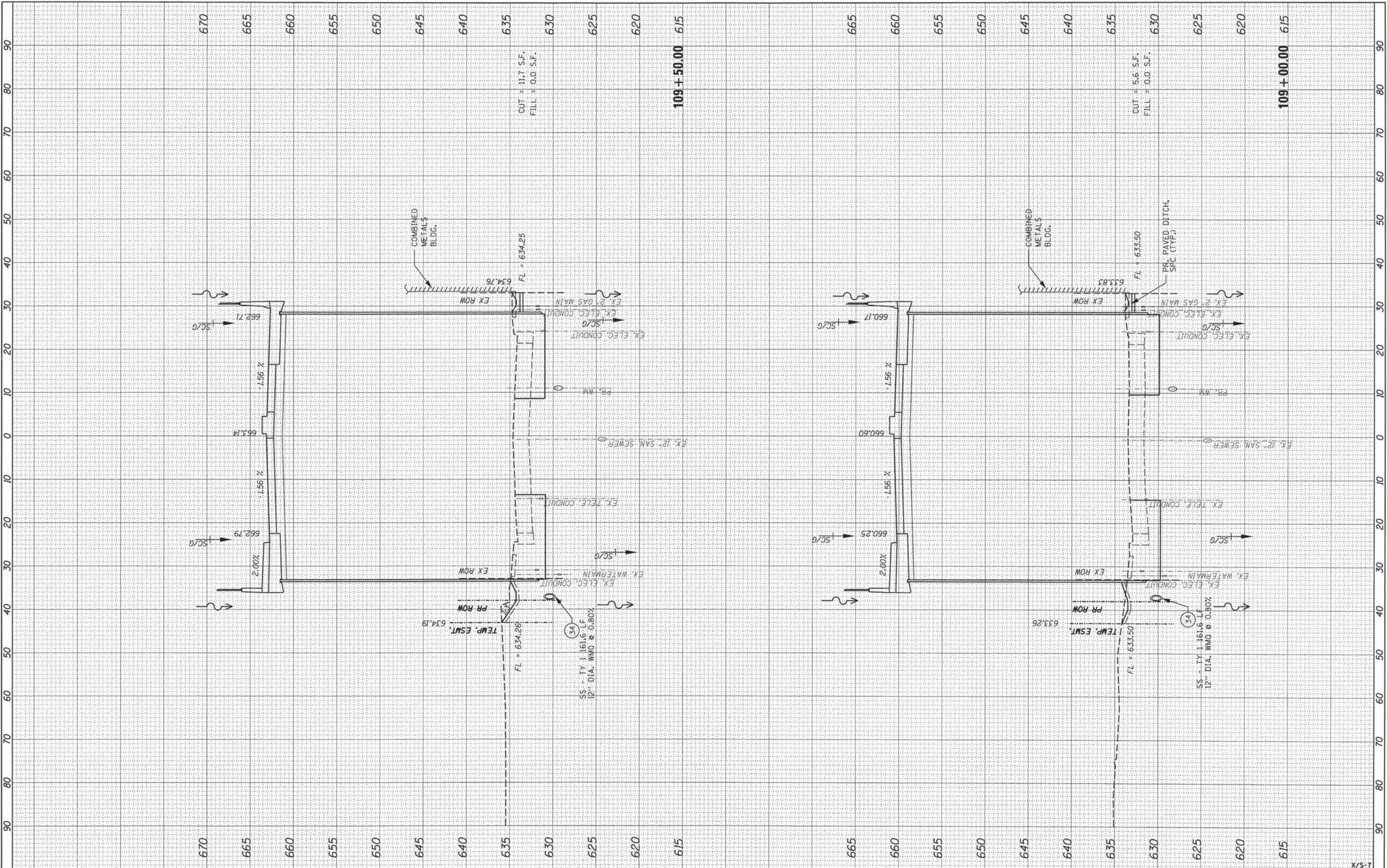
F.A.U. RTE. 2714	SECTION 99-00094-00-GS	COUNTY COOK	TOTAL SHEETS 240	SHEET NO. 227
CONTRACT NO. 63887				
ILLINOIS FED. AID PROJECT				

FILE NAME Default

X/S-5

FINAL SURVEY	BY	DATE
SUBMITTED		
PLOTTED		
TEMPLATE		
NOTE BOOK		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SUBMITTED		
PLOTTED		
TEMPLATE		
NOTE BOOK		
AREAS CHECKED		
NO.		



Globetrotters
Engineering Corporation

USER NAME = stephen.schuh	DESIGNED - MA	REVISED -
PLLOT SCALE = 10.0000' / 1"	DRAWN - MA	REVISED -
PLLOT DATE = 3/21/2014	CHECKED - RH	REVISED -
	DATE - 3-24-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

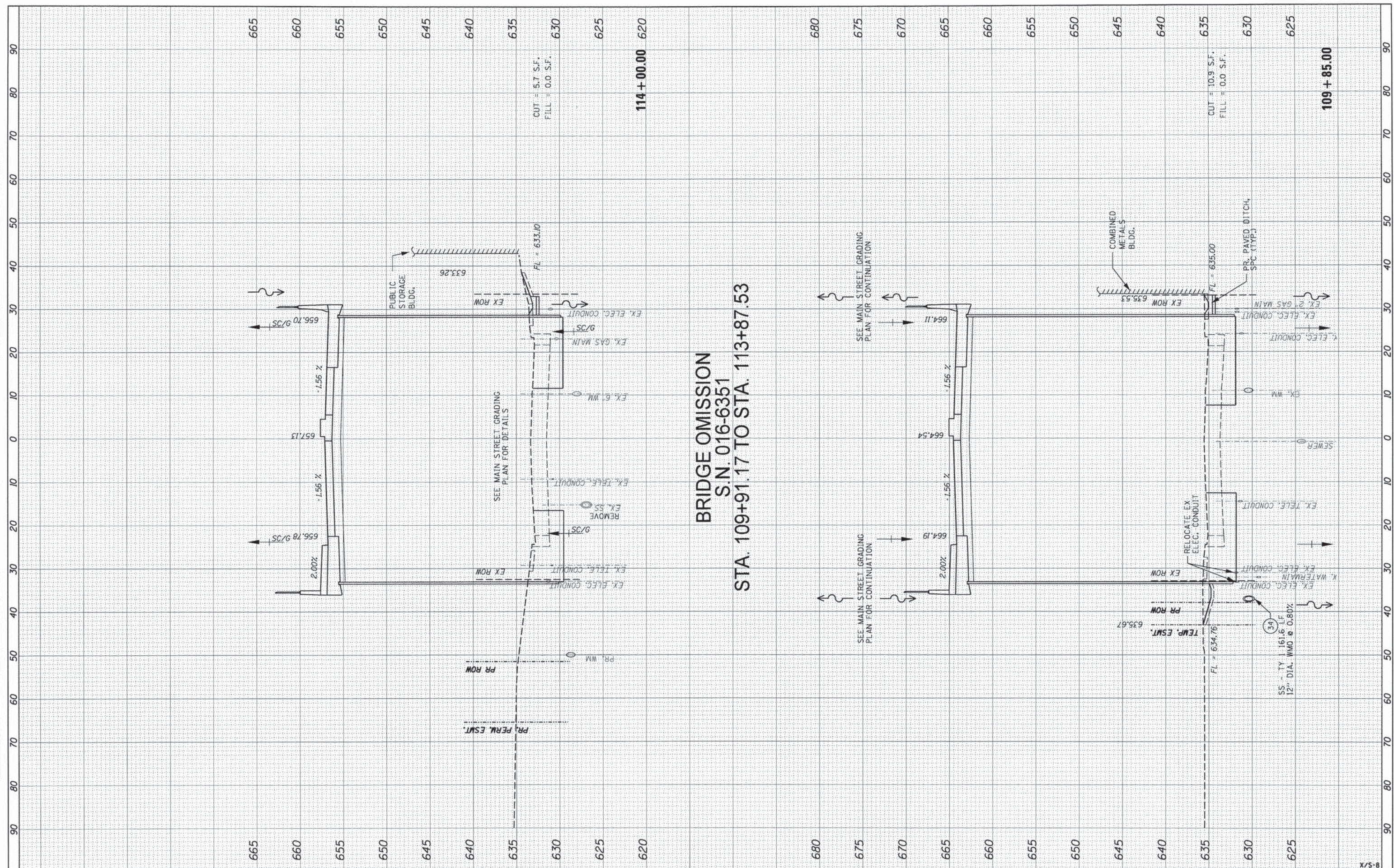
25TH AVE. CROSS SECTIONS	
25TH AVENUE GRADE SEPARATION	
SCALE: 1" = 10' H 1" = 5' V	SHEET 7 OF 14 SHEETS
STA. 109+00.00	TO STA. 109+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	99-00094-00-GS	COOK	240	229
CONTRACT NO. 63887				
ILLINOIS FED. AID PROJECT				

FILE NAME
Default

FINAL SURVEY	EXHIBIT	DATE
NO.	NO.	
NO.	NO.	
NO.	NO.	

ORIGINAL SURVEY	EXHIBIT	DATE
NO.	NO.	
NO.	NO.	
NO.	NO.	



USER NAME = mohammed.als	DESIGNED - MA	REVISED -
PLOT SCALE = 10.0000' / 1" =	DRAWN - MA	REVISED -
PLOT DATE = 7/30/2014	CHECKED - RH	REVISED -
	DATE - 3-24-2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

25TH AVE. CROSS SECTIONS
25TH AVENUE GRADE SEPARATION

SCALE: 1" = 10' H
5" = 5' V SHEET 8 OF 14 SHEETS STA. 109+85.00 TO STA. 114+00.00

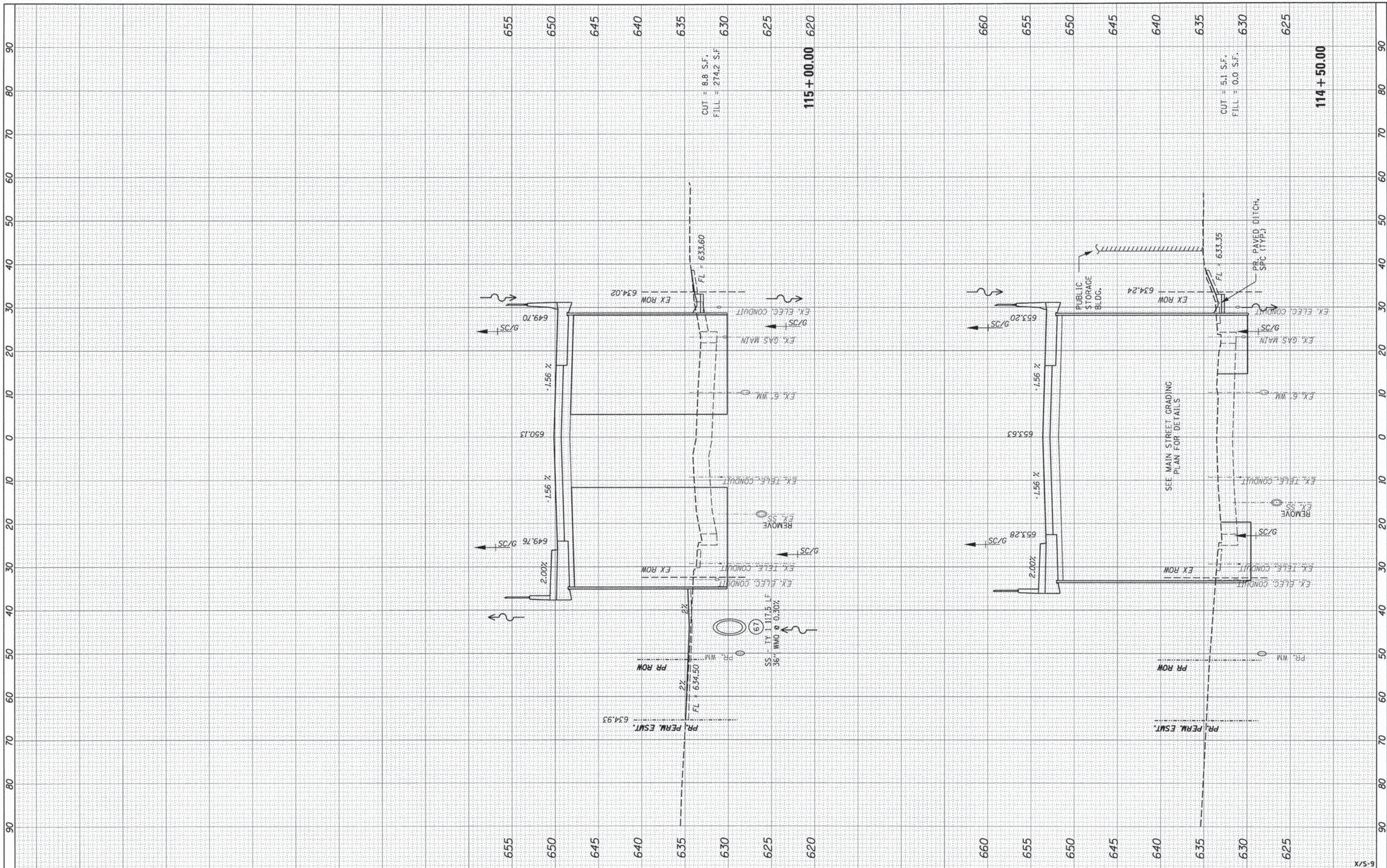
F.A.U. RTE. 2714	SECTION 99-00094-00-GS	COUNTY COOK	TOTAL SHEETS 240	SHEET NO. 230
CONTRACT NO. 63887			ILLINOIS FED. AID PROJECT	

FILE NAME Default

X/5-8

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	DATE		
	AREAS		
	CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	DATE		
	AREAS		
	CHECKED		



USER NAME = mohammed.o.i	DESIGNED - MA	REVISED -
PLLOT SCALE = 10.0000' / 1"	DRAWN - MA	REVISED -
PLLOT DATE = 7/30/2014	CHECKED - RH	REVISED -
	DATE - 3-24-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

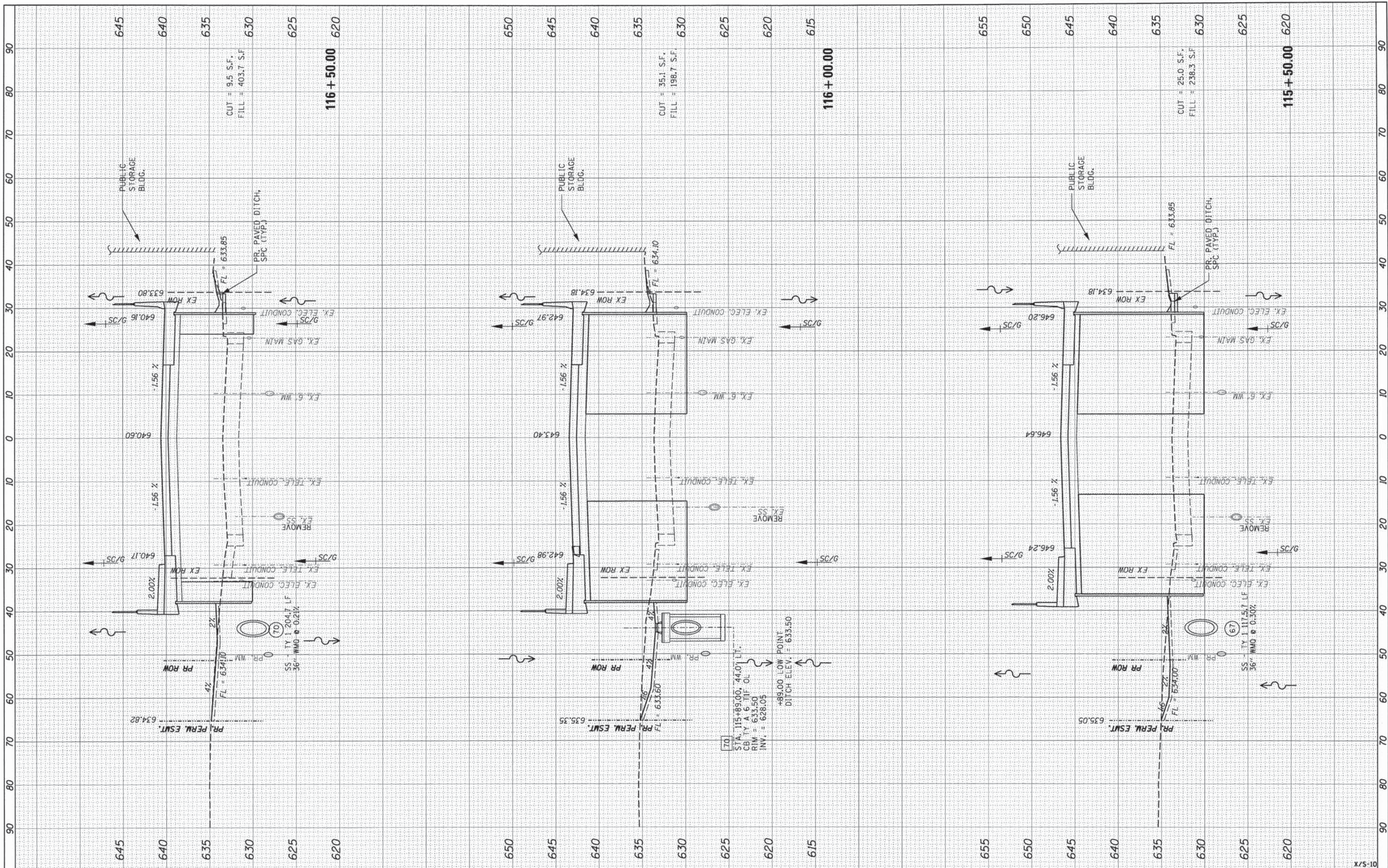
SCALE: 1" = 10'-0"		SHEET 9 OF 14 SHEETS	STA. 114+50.00 TO STA. 115+00.00
--------------------	--	----------------------	----------------------------------

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	99-00094-00-GS	COOK	240	231
				CONTRACT NO. 63887
ILLINOIS FED. AID PROJECT				

X/5-9

FINAL SURVEY	DATE
NO. _____	
BY _____	
DATE _____	
SURVEYED	DATE
NOTE BOOK	NO. _____
AREAS CHECKED	
AREAS CHECKED	

ORIGINAL SURVEY	DATE
NO. _____	
BY _____	
DATE _____	
SURVEYED	DATE
NOTE BOOK	NO. _____
AREAS CHECKED	
AREAS CHECKED	



Globetrotters
Engineering Corporation

USER NAME = mhammed.ali	DESIGNED - MA	REVISED -
PLOT SCALE = 10.0000' / 1"	DRAWN - MA	REVISED -
PLOT DATE = 7/30/2014	CHECKED - RH	REVISED -
	DATE - 3-24-2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

25TH AVE. CROSS SECTIONS
25TH AVENUE GRADE SEPARATION

SCALE: 1" = 10' H
1" = 40' V

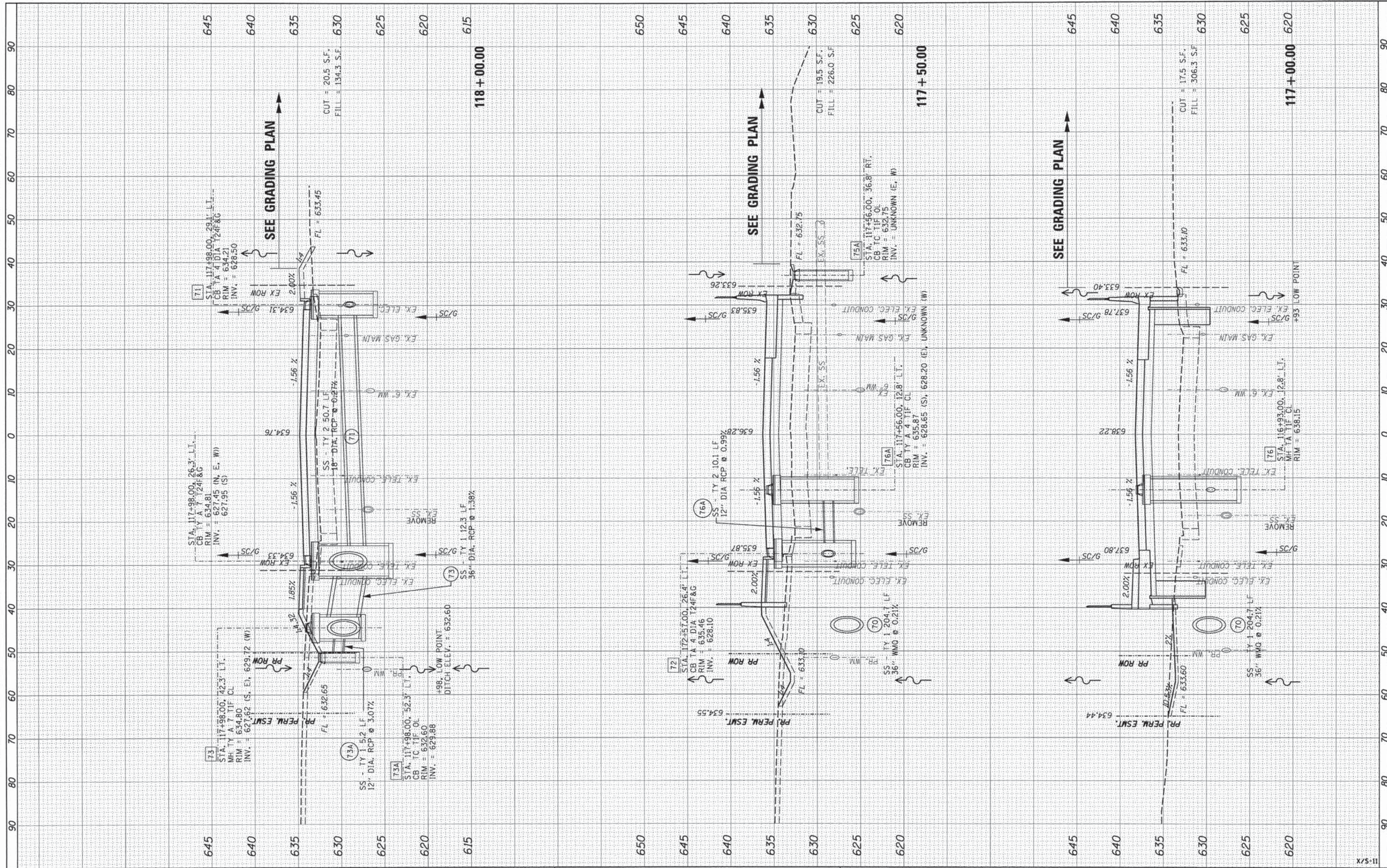
SHEET 10 OF 14 SHEETS STA. 115+50.00 TO STA. 116+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	99-00094-00-G5	COOK	240	232
CONTRACT NO. 63887				
ILLINOIS FED. AID PROJECT				

X/5-10

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		



USER NAME = mohamed.a.1	DESIGNED - MA	REVISED -
PLLOT SCALE = 10.0000' / 1"	DRAWN - MA	REVISED -
PLLOT DATE = 7/30/2014	CHECKED - RH	REVISED -
	DATE - 3-24-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**25TH AVE. CROSS SECTIONS
25TH AVENUE GRADE SEPARATION**

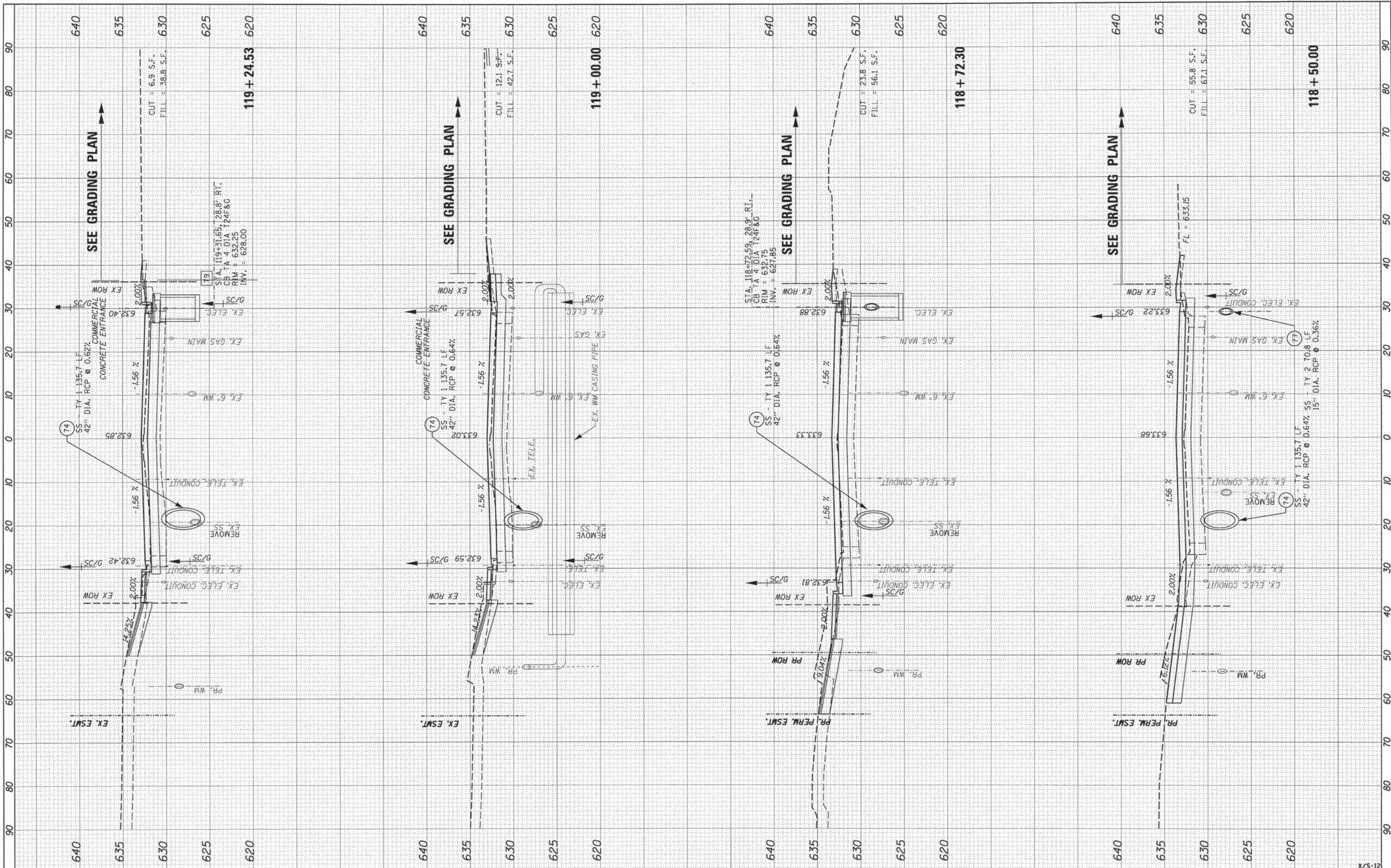
SCALE: 1" = 10' H, 1" = 5' V SHEET 11 OF 14 SHEETS STA. 117+00.00 TO STA. 118+00.00

F.A.U. RTE. 2714	SECTION 99-00094-00-GS	COUNTY COOK	TOTAL SHEETS 240	SHEET NO. 233
CONTRACT NO. 63887				ILLINOIS FED. AID PROJECT

K/S-11

FINAL SURVEY	DATE
QUOTED	
PLANNED	
NOTE BOOK	
AREAS CHECKED	

ORIGINAL SURVEY	DATE
PLOTTED	
NOTE BOOK	
AREAS CHECKED	



USER NAME = mohammed.ali	DESIGNED - MA	REVISED -
PLLOT SCALE = 10.0000' / 1"	DRAWN - MA	REVISED -
PLLOT DATE = 7/30/2014	CHECKED - RH	REVISED -
	DATE - 3-24-2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

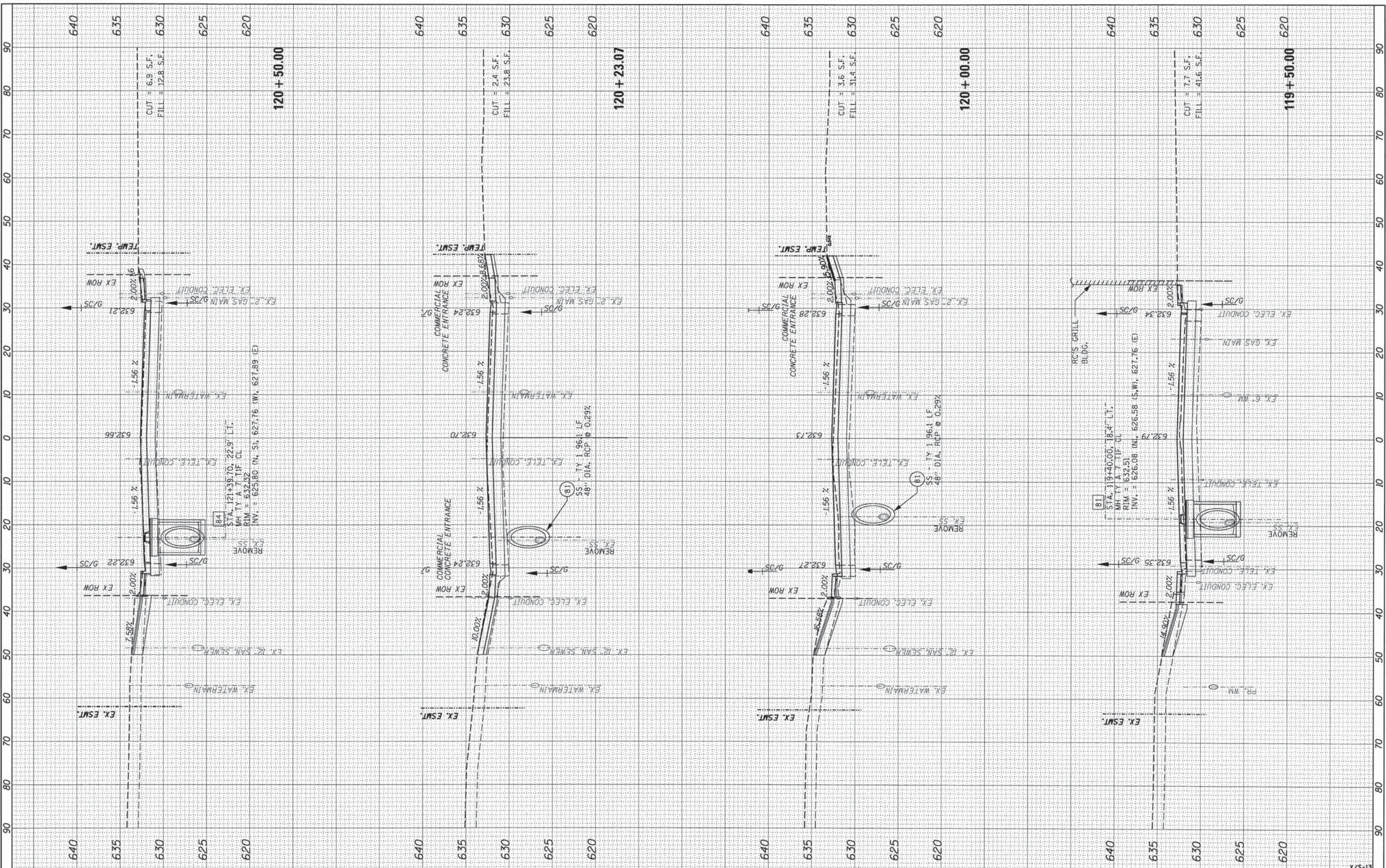
25TH AVE. CROSS SECTIONS			
25TH AVENUE GRADE SEPARATION			
SCALE: 1" = 10' H 1" = 5' V	SHEET 12	OF 14 SHEETS	STA. 118+50.00 TO STA. 119+24.53

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	99-00094-00-G5	COOK	240	234
CONTRACT NO. 63887				
ILLINOIS FED. AID PROJECT				

X/5-12

BY	DATE
DESIGNED	
DRAWN	
CHECKED	
DATE	

BY	DATE
DESIGNED	
DRAWN	
CHECKED	
DATE	



USER NAME = stephen.schuh	DESIGNED - MA	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN - MA	REVISED -
PLOT DATE = 3/21/2014	CHECKED - RH	REVISED -
	DATE - 3-24-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

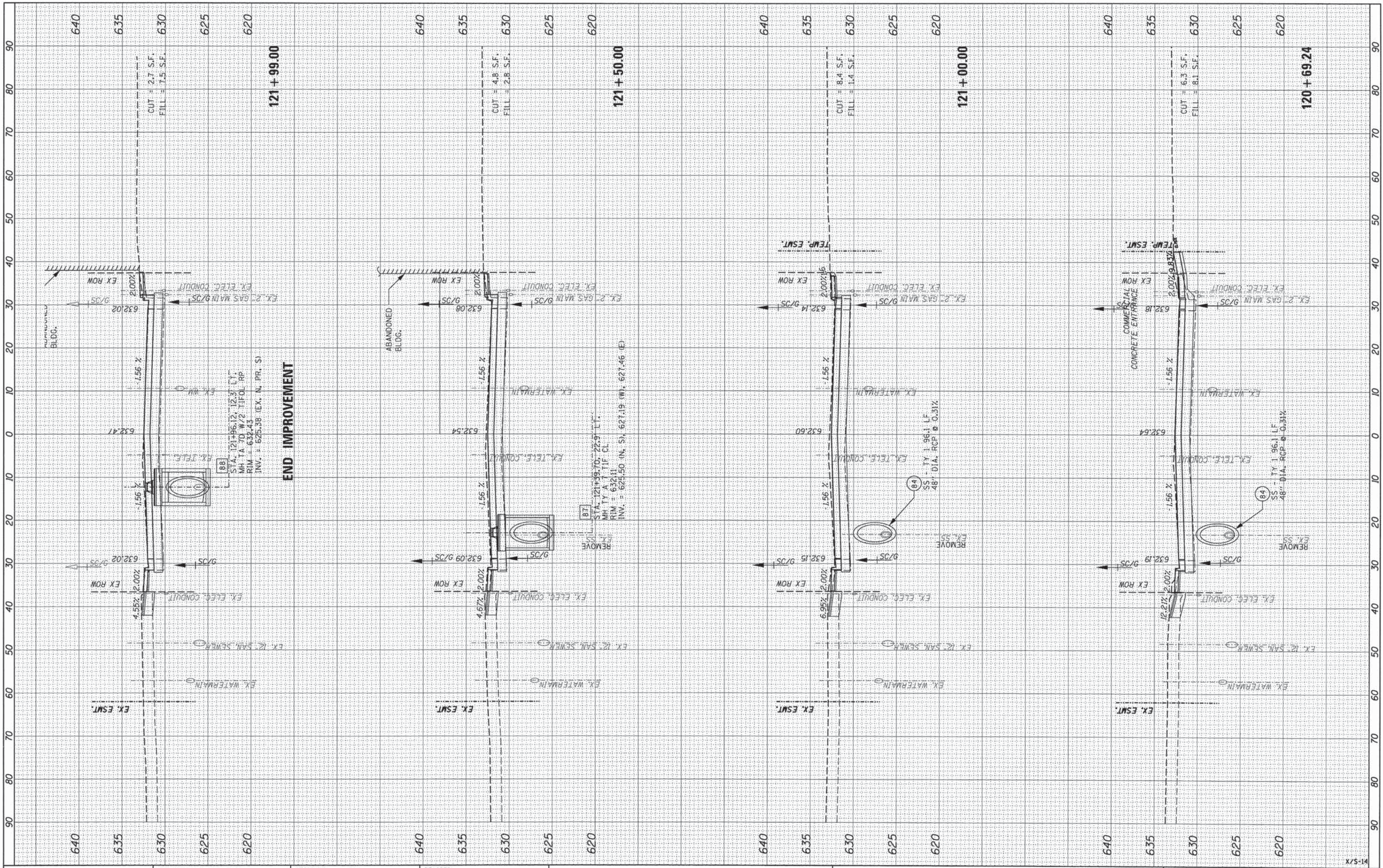
25TH AVE. CROSS SECTIONS 25TH AVENUE GRADE SEPARATION			
SCALE: 1" = 10' H 1" = 5' V	SHEET 13	OF 14 SHEETS	STA. 119+50.00 TO STA. 120+50.00

F.A.U. RTE. 2714	SECTION 99-00094-00-GS	COUNTY COOK	TOTAL SHEETS 240	SHEET NO. 235
CONTRACT NO. 63887				ILLINOIS FED. AID PROJECT

X/S-13

FINAL SURVEY	DATE
PLOTTED	BY
NOTE BOOK	
AREAS CHECKED	
NO.	

ORIGINAL SURVEY	DATE
PLOTTED	BY
NOTE BOOK	
AREAS CHECKED	
NO.	



USER NAME = stephen.schuh
 PLOT SCALE = 10.0000' / 1" = 100
 PLOT DATE = 3/21/2014

DESIGNED - MA
 DRAWN - MA
 CHECKED - RH
 DATE - 3-24-2014

REVISED -
 REVISED -
 REVISED -
 REVISED -

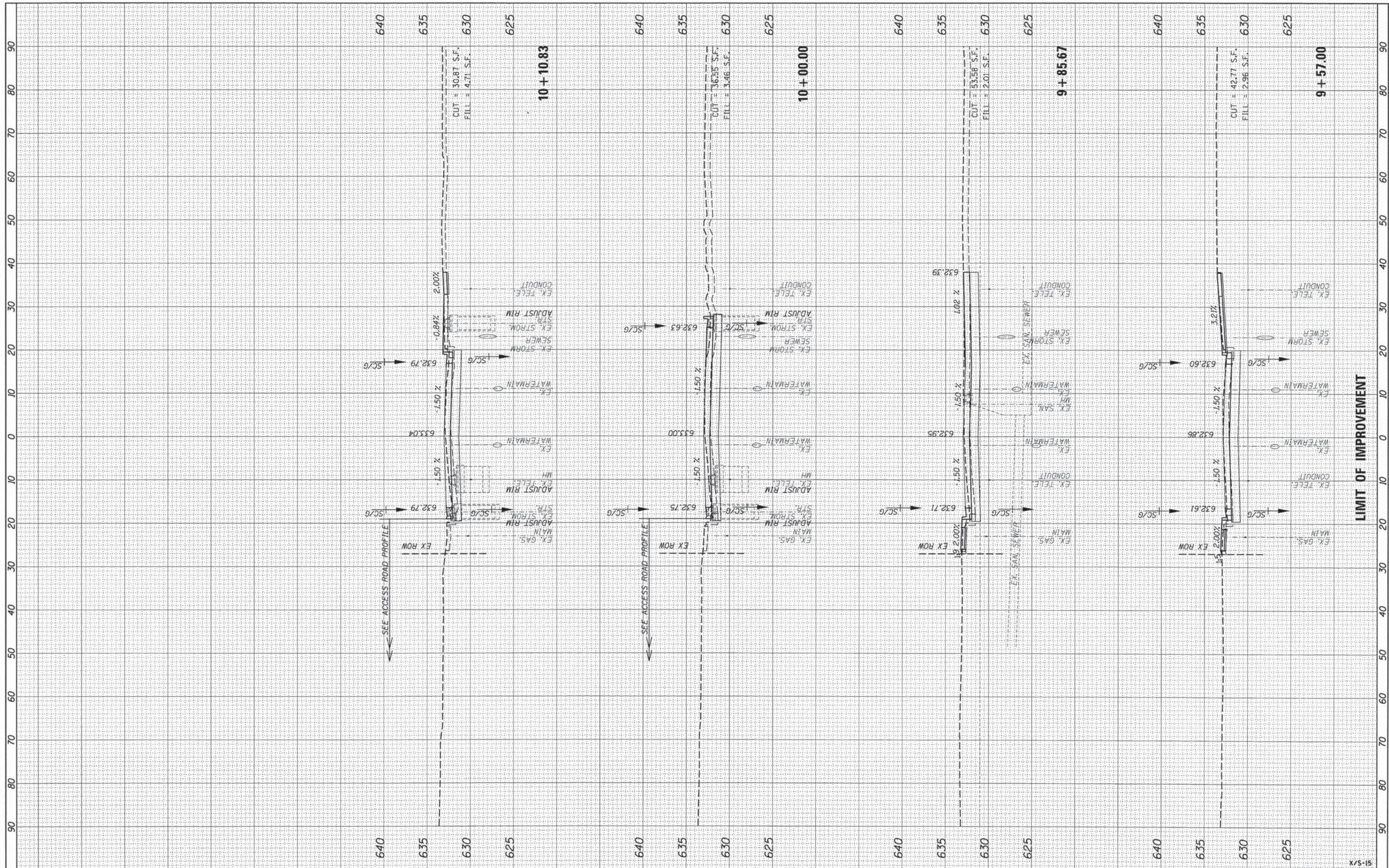
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

25TH AVE. CROSS SECTIONS
 25TH AVENUE GRADE SEPARATION
 SCALE: 1" = 10' H, 1" = 5' V
 SHEET 14 OF 14 SHEETS
 STA. 120+69.24 TO STA. 121+99.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	99-00094-00-05	COOK	240	236
CONTRACT NO. 63887				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	NO.
SUBMITTED	NO.
REVISED	NO.
PROPOSED	NO.
NOTED	NO.
DATE	

ORIGINAL SURVEY	NO.
SUBMITTED	NO.
REVISED	NO.
PROPOSED	NO.
NOTED	NO.
DATE	



USER NAME = stephenschuh	DESIGNED - MA	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN - MA	REVISED -
PLOT DATE = 3/21/2014	CHECKED - RH	REVISED -
	DATE - 3-24-2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GRANT ST. CROSS SECTIONS
25TH AVENUE GRADE SEPARATION

SCALE: 1" = 10' H
1" = 8' V

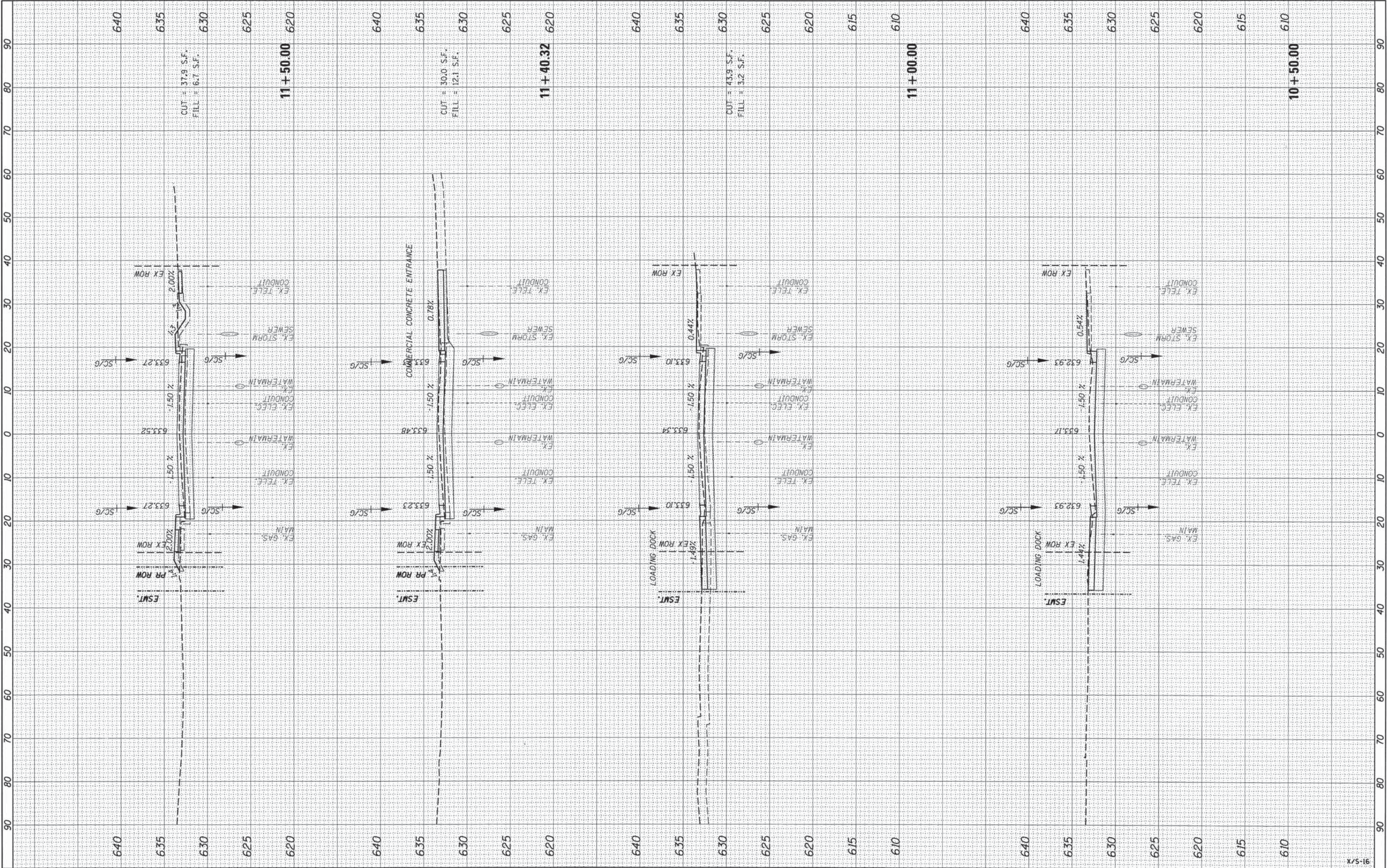
SHEET 1 OF 4 SHEETS STA. 9+57.00 TO STA. 10+10.83

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	99-00094-00-GS	COOK	240	237
CONTRACT NO. 63887				
ILLINOIS FED. AID PROJECT				

X/S-15

FINAL SURVEY	DATE
SURVEYED BY	
NOTE BOOK NO.	
TEMPLATE AREAS CHECKED	

ORIGINAL SURVEY	DATE
SURVEYED BY	
NOTE BOOK NO.	
TEMPLATE AREAS CHECKED	



USER NAME = stephen.schuh	DESIGNED - MA	REVISED -
PLLOT SCALE = 10.0000' / 1" = 10'	DRAWN - MA	REVISED -
PLLOT DATE = 3/21/2014	CHECKED - RH	REVISED -
	DATE - 3-24-2014	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

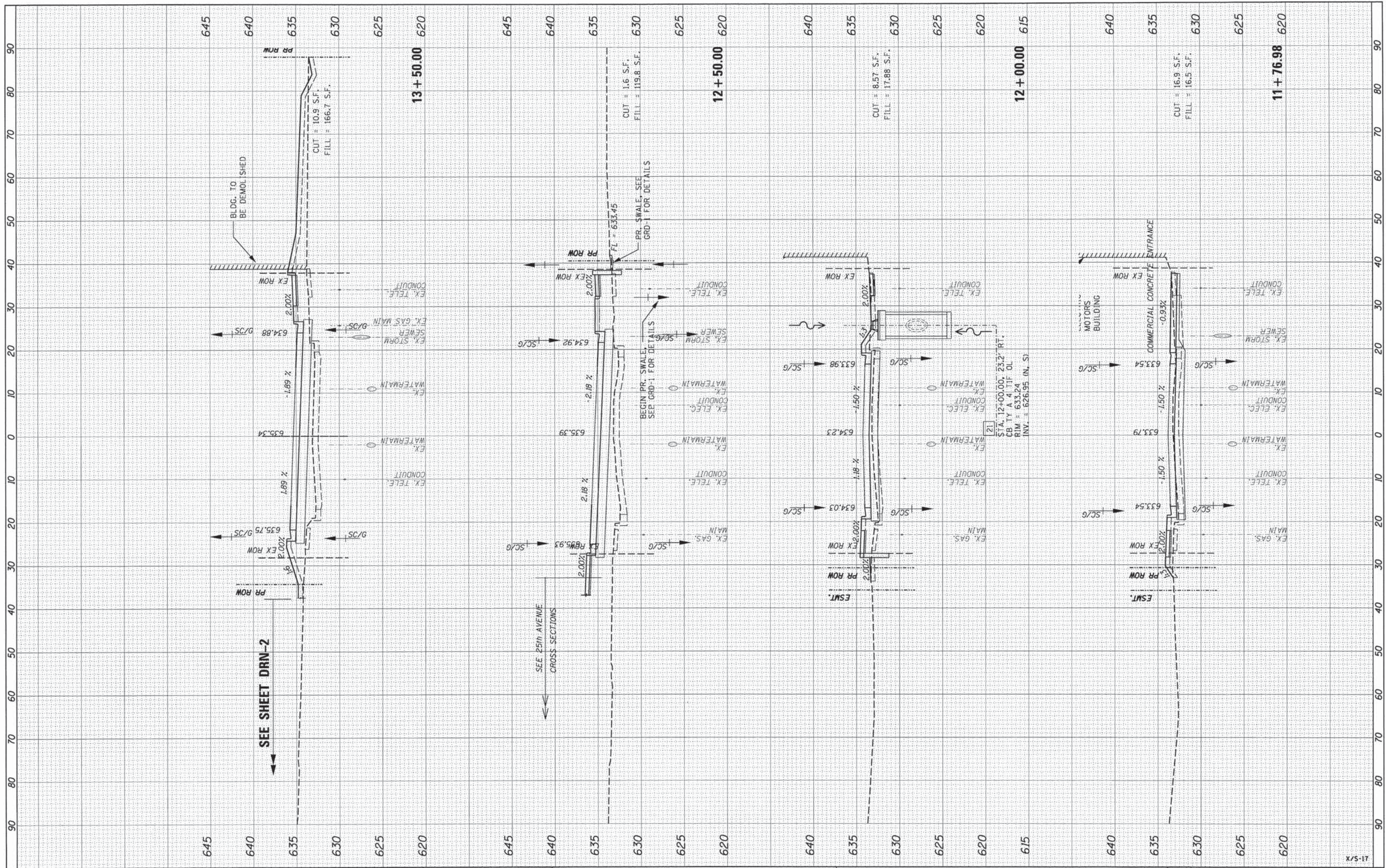
GRANT ST. CROSS SECTIONS 25TH AVENUE GRADE SEPARATION	
SCALE: 1" = 10' H 1" = 5' V	SHEET 2 OF 4 SHEETS STA. 10+50.00 TO STA. 11+50.00

F.A.U. RTE. 2714	SECTION 99-00094-00-GS	COUNTY COOK	TOTAL SHEETS 240	SHEET NO. 238
CONTRACT NO. 63887				ILLINOIS FED. AID PROJECT

X/5-16

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
CHECKED		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
CHECKED		



USER NAME = mohammed.als
 DESIGNED - MA
 DRAWN - MA
 CHECKED - RH
 DATE - 3-24-2014

REVISIONS:
 REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

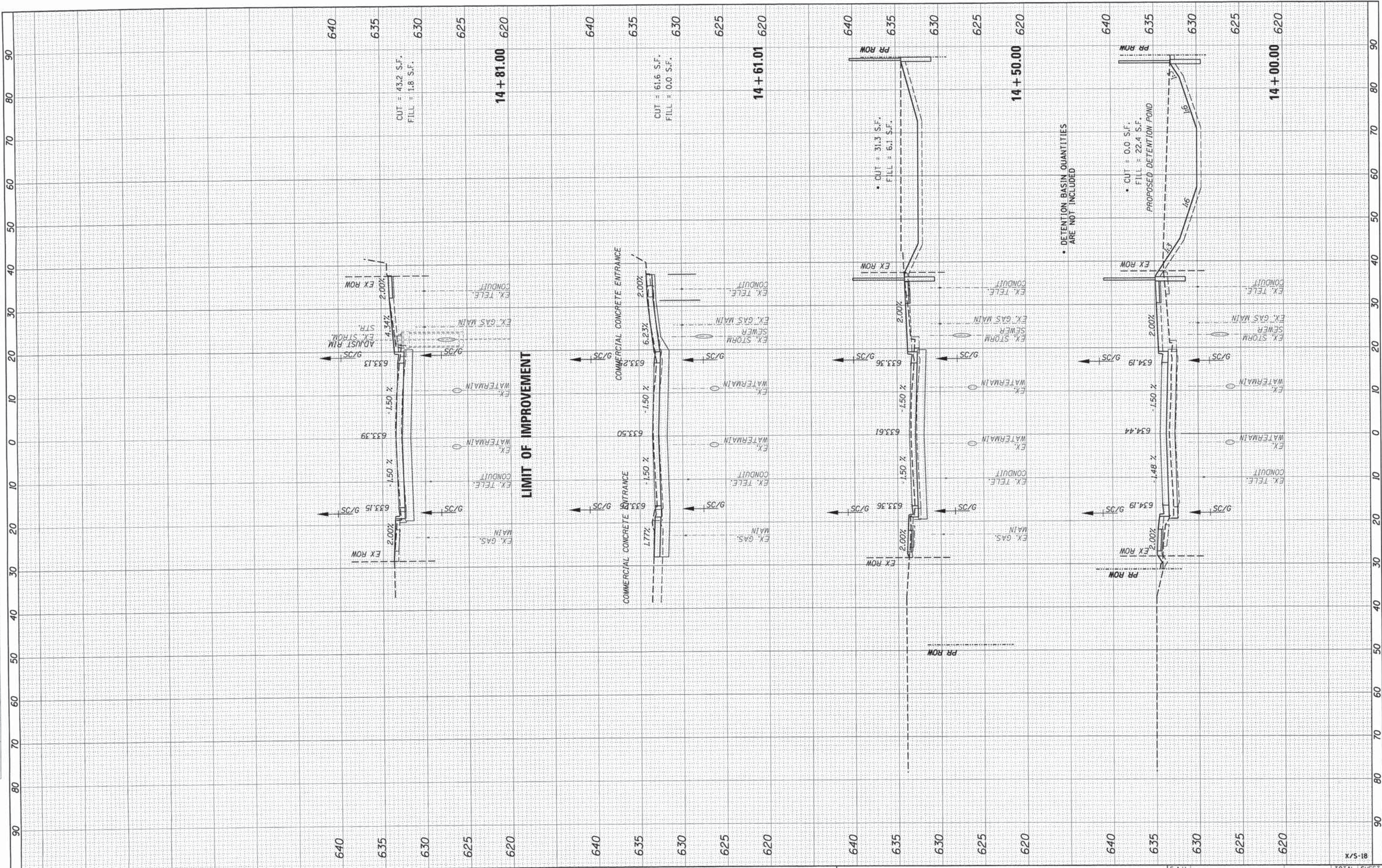
**GRANT ST. CROSS SECTIONS
 25TH AVENUE GRADE SEPARATION**
 SCALE: 1" = 10' H
 1" = 5' V
 SHEET 3 OF 4 SHEETS STA. 11+76.98 TO STA. 13+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	99-00094-00-GS	COOK	240	239
CONTRACT NO. 63887			ILLINOIS FED. AID PROJECT	

X/5-1T

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
AREAS CHECKED		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
AREAS CHECKED		



• DETENTION BASIN QUANTITIES ARE NOT INCLUDED



USER NAME = mohammed.ali
DESIGNED - MA
DRAWN - MA
CHECKED - RH
DATE - 3-24-2014
PLOT SCALE = 10.0000' / in.
PLOT DATE = 7/30/2014

DESIGNED - MA	REVISED -
DRAWN - MA	REVISED -
CHECKED - RH	REVISED -
DATE - 3-24-2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: 1" = 10' H 1" = 5' V	SHEET 4 OF 4 SHEETS	STA. 14+00.00 TO STA. 14+81.00
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GRANT ST. CROSS SECTIONS
25TH AVENUE GRADE SEPARATION

F.A.U. RTE. 2714	SECTION 99-00094-00-GS	COUNTY COOK	TOTAL SHEETS 240	SHEET NO. 240
CONTRACT NO. 63887				ILLINOIS FED. AID PROJECT

x/s-18