

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED
HIGHWAY PLANS

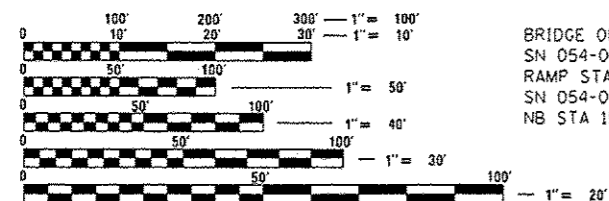
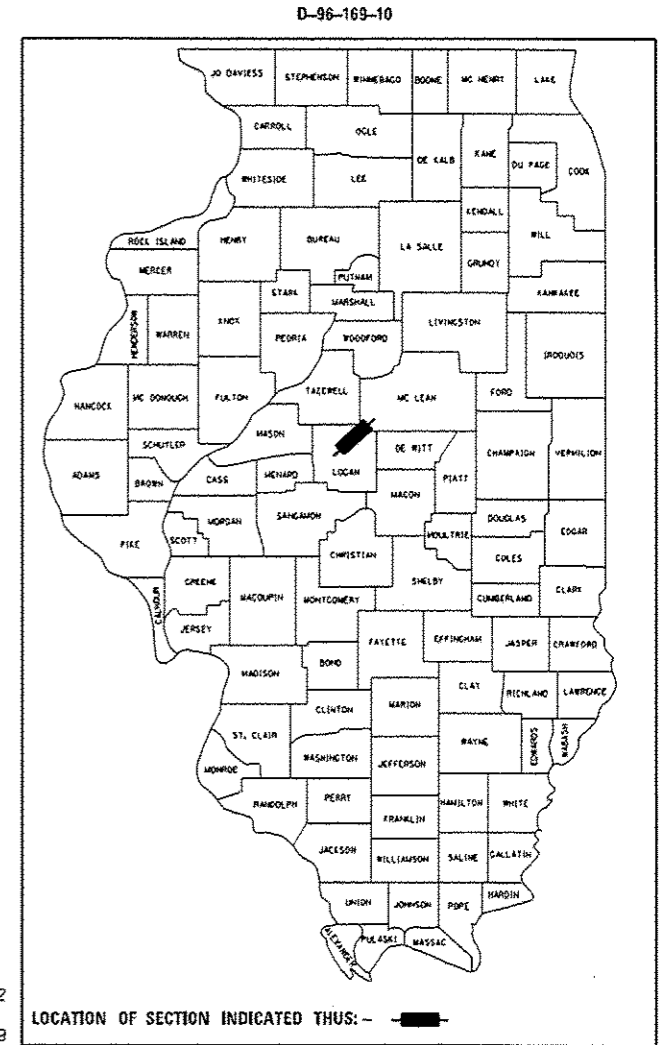
FOR INDEX OF SHEETS, SEE SHEET NO. 2

FAI ROUTE 55 & 155 (I-55 & I-155)
SECTION (54-4,54-5)RS-3, (54-6)RS-2

RESURFACING, 3R
LOGAN COUNTY

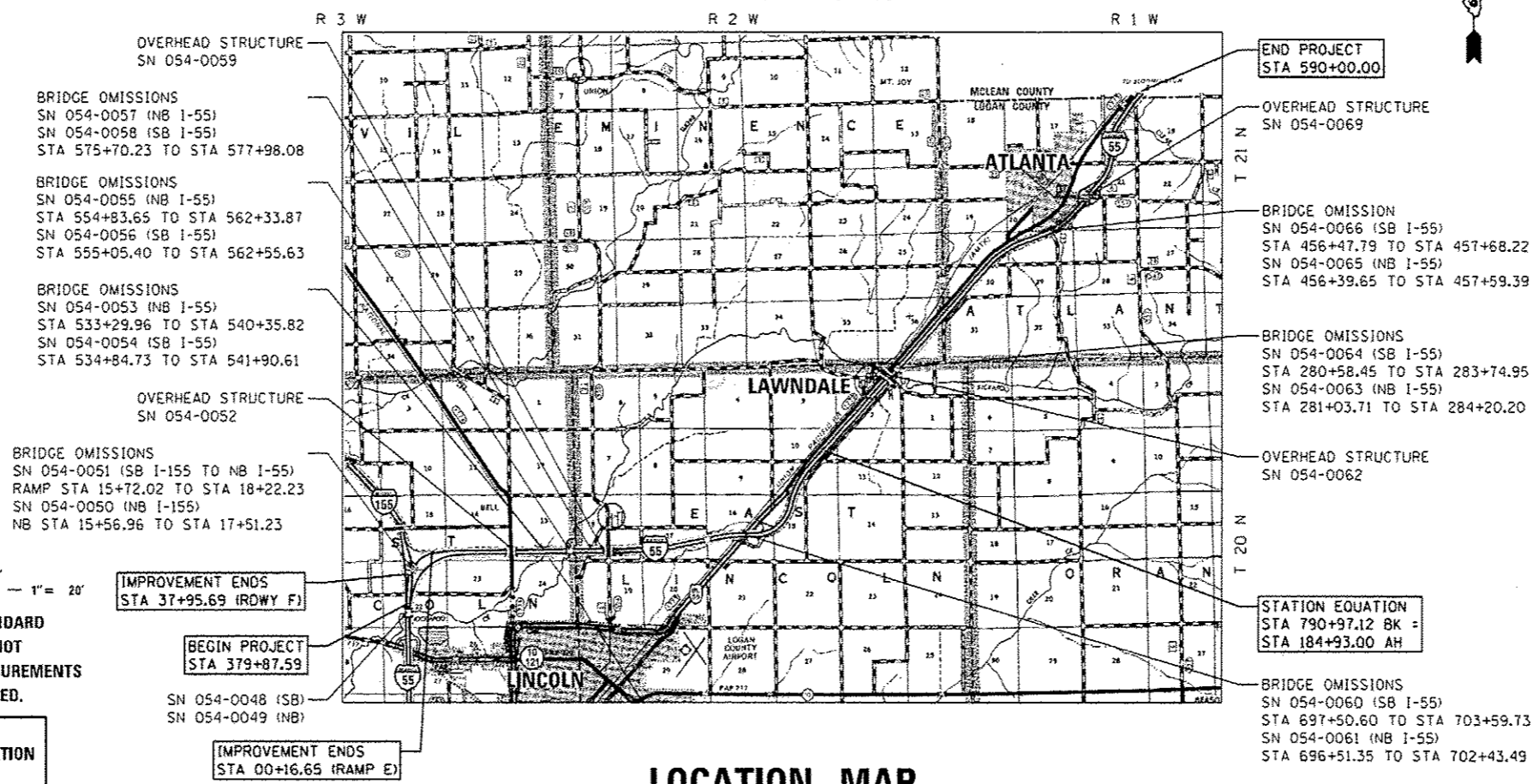
C-96-169-10

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55/155		LOGAN	108	1
		ILLINOIS	CONTRACT NO. 72D41	
		** (54-4,54-5)RS-3, (54-6)RS-2		



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811



PROJECT ENGINEER: KEITH DONOVAN (217) 782-4761
TEAM MANAGER: TOM COX (217) 557-6349
CONTRACT NO. 72D41

LOCATION MAP

0 1 2 3 4 5 MILES
 SCALE

GROSS LENGTH = 81,616.53 FT = 15.458 MI
 NET LENGTH = 78,964.88 FT = 14.956 MI

F.A.I. ROUTE 55 (I-55)
 ADT (2013) = 20400
 SU = 5.64 %, MU = 28.92%, PV = 65.44%
 SPEED LIMIT = 70 MPH (POSTED)

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED July 11 2014
Boya Z. Durr
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Aug 15 2014
John D. Baranzelli, P.E.
 ENGINEER OF DESIGN AND ENVIRONMENT

Aug 15 2014
Omer Osman, P.E.
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET #	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS, GENERAL NOTES
3	APPLICATION RATES, MIXTURE REQUIREMENTS, SIGNATURES
4-7	SUMMARY OF QUANTITIES
8-24	SCHEDULE OF QUANTITIES
25-34	TYPICAL SECTIONS
35-51	ALIGNMENT, CROSS TIES, AND BENCHMARKS
52-69	PLAN SHEETS
70-87	PAVEMENT MARKING PLAN SHEETS
88-90	UNDERDRAIN PROFILE SHEETS
91-92	TYPICAL PAVEMENT MARKING DETAILS
93	OVERHEAD STRUCTURE RAMPING DETAIL
94	MED GUARD POST AND UD OUTLET MARKER DETAIL
95	CLASS B PATCHING DETAIL
96	EMERGENCY MEDIAN CROSS OVER DETAIL
97	EXIT RAMP DETAILS
98	ENTRANCE RAMP DETAILS
99-100	BUTT JOINT AND TRANSITION DETAILS
101	INLET REPAIR DETAIL
102	PIPE INSTALLATION DETAIL
103	PRISMATIC REFLECTOR DETAIL
104-108	SUPER ELEVATION TRANSITION DETAIL

HIGHWAY STANDARDS

STANDARD #	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
442001-04	CLASS A PATCHES
442101-07	CLASS B PATCHES
542001-04	CONCRETE END SECTION FOR PIPE CULVERT 15" THRU 84" DIA
542401-01	METAL END SECTION FOR PIPE CULVERTS
601001-04	SUB-SURFACE DRAINS
601101-01	CONCRETE HEADWALL FOR PIPE DRAIN
604101-01	MEDIAN INLET FOR 24" REINFORCED CONCRETE PIPE
630001-10	STEEL PLATE BEAM GUARDRAIL
635001-01	DELINEATORS
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
642001-02	SHOULDER RUMBLE STRIPS, 16 INCH
665001-02	WOVEN WIRE FENCE
701101-04	OFF-RD OPERATIONS, MULTILANE, 15' TO 24' FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' AWAY
701400-07	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-08	LANE CLOSURE, FREEWAY / EXPRESSWAY
701411-08	LANE CLOSURE, MULTILANE, AT ENTRANCES OR EXIT RAMP, FOR SPEEDS > OR = 45 MPH
701426-06	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION FOR SPEEDS > OR = 45 MPH
701428	TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY / EXPRESSWAY
701446-05	TWO LANE CLOSURE FREEWAY / EXPRESSWAY
701451-02	RAMP CLOSURE FREEWAY / EXPRESSWAY
701456-03	PARTIAL EXIT RAMP CLOSURE FREEWAY / EXPRESSWAY
701901-03	TRAFFIC CONTROL DEVICES
780001-04	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
782001	PRISMATIC CURB REFLECTOR

GENERAL NOTES:

1. ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER LISTED ON THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.
2. ALL OF THE DISTURBED AREAS WITHIN THE RIGHT-OF-WAY NOT COVERED BY SURFACING MATERIAL SHALL BE SEEDED.
3. CONTRACTOR SHALL PROVIDE ADEQUATE DRAINAGE AND EROSION PROTECTION FOR THE DURATION OF THIS PROJECT.
4. SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SEEDED SHALL BE DETERMINED BY THE ENGINEER.
5. THE LOCATIONS OF THE BURIED AND ABOVE GROUND UTILITIES SHOWN IN THE PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.26 OF THE STANDARD SPECIFICATIONS. THE J.U.L.I.E. NUMBER IS (800) 892-0123. A MINIMUM OF FORTY-EIGHT HOURS ADVANCED NOTICE IS REQUIRED PRIOR TO PROCEEDING WITH ANY EXCAVATION WORK ON THE PROJECT.
6. THE THICKNESS OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT-MIX ASPHALT MIXTURE IS PLACED.
7. BEFORE ORDERING PIPE CULVERTS OR PIPE DRAINS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR EXACT LENGTHS.
8. THE CONNECTIONS OF THE PROPOSED PIPE CULVERTS TO EXISTING PIPES, CULVERTS, OR STRUCTURES WILL BE TO THE SATISFACTION OF THE ENGINEER, AND WILL BE INCLUDED IN THE COST OF THE PIPE CULVERTS.
9. FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SANDBAGS PER BARRICADE.
10. WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL MONUMENTS UNTIL AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR RE-ESTABLISH ANY SECTION OR SUBSECTION MONUMENTS DESTROYED BY HIS OPERATIONS.
11. ALL ELEVATIONS REFER TO U.S.G.S. MEAN SEA LEVEL DATUM.
12. UNLESS NOTED OTHERWISE, STATIONS AND OFFSETS REFER TO CENTERLINE OF PROJECT.
13. LAYOUT OF RIPRAP MAY BE VARIED IN THE FIELD TO SUIT GROUND CONDITIONS AS DIRECTED BY THE ENGINEER.
14. IN ADDITION TO FIELD SURVEYS AND AERIAL SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
15. EVERY TREE SHALL BE SAVED IF POSSIBLE. THE ENGINEER IN THE FIELD WILL VERIFY AND MARK ALL TREES REQUIRED TO BE REMOVED. SHOULD THE ENGINEER'S DECISION INCREASE OR DECREASE THE QUANTITIES OF WORK TO BE PERFORMED FROM THE PLANS, THE CONTRACTOR SHALL ACCEPT PAYMENT AS STATED IN ARTICLE 104.02 OF THE STANDARD SPECIFICATIONS. TREES OUTSIDE THE LIMITS OF CONSTRUCTION SHALL NOT BE DISTURBED UNLESS DESIGNATED BY THE ENGINEER.
16. THE GRADING AND SHAPING OF THE DITCHES SHALL BE DONE IN SUCH A MANNER TO ESTABLISH POSITIVE FLOW BETWEEN DRAINAGE STRUCTURES. OR BETWEEN THE EXISTING ELEVATION AT THE UPSTREAM END OF THE GRADING TO THE DOWNSTREAM DRAINAGE STRUCTURE.
17. HOT-MIX ASPHALT RESURFACING OF THE EXISTING PAVEMENT AND SHOULDERS SHALL BE DONE IN A MANNER THAT PROVIDES A MINIMUM VERTICAL CLEARANCE OF 16 FEET AT ALL OVERHEAD STRUCTURES. THE MINIMUM VERTICAL CLEARANCE SHALL BE MEASURED AND DOCUMENTED BY THE CONTRACTOR AS NOTED IN THE "OVERHEAD STRUCTURE RAMPING DETAIL" SHEET.
18. RAISED REFLECTIVE PAVEMENT MARKERS INSTALLED INTO THE ORIGINAL CONCRETE PAVEMENT MAY NOT HAVE BEEN REMOVED WHEN THE PAVEMENT WAS RESURFACED. ANY MARKERS FOUND IN THE ORIGINAL CONCRETE WILL BE REMOVED AND PAID FOR AS RAISED REFLECTIVE PAVEMENT MARKER REMOVAL.
19. ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER LISTED ON THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.
20. REMOVING BRUSH AT LOCATIONS SPECIFIED IN THE PLANS SHALL NOT BE PAID FOR SEPARATELY BUT INCLUDED IN THE COST OF THE ASSOCIATED PAY ITEMS.
21. QUANTITY FOR PAY ITEM "X482010 - HMA SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)" IS AN ESTIMATED QUANTITY TO BE USED AT THE DIRECTION OF THE ENGINEER ON THE FIELD.

FILE NAME : c:\pwork\pwork\sporkage\0215133\07241\sh-gennote.dgn	USER NAME : sporkage	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, HIGHWAY STANDARD, GENERAL NOTES	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN - DMS	REVISED -			*	**	LOGAN	108	2	
		CHECKED - DMS	REVISED -			CONTRACT NO. 72D41					
Default	PLOT DATE : Jul-14-2014 03:26:37PM	DATE -	REVISED -			SCALE:	SHEET 1 OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		

APPLICATION RATES

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

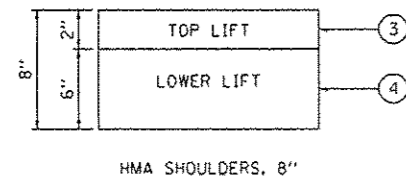
GRANULAR MATERIALS	2.05	TON / CU YD
RIPRAP	1.75	TON / CU YD
BITUMINOUS MATERIALS PRIME COAT	0.05	LB / SQ FT (REGULAR)
	0.025	LB / SQ FT (FOG COAT)
HMA RESURFACING (BINDER, SURF) (112 LBS)	0.056	TON/SQ YD*IN

SIGNATURES

MIXTURE REQUIREMENT

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT				
MIX NUMBER:	1	2	3	4
MIX USE:	INTERSTATE MAINLINE PAVEMENT & RAMP (POLYMER HMA BINDER)	INTERSTATE MAINLINE PAVEMENT AND RAMP (POLYMER HMA SURF COURSE)	INTERSTATE SHOULDERS, HMA SHOULDERS, 8" (TOP LIFT 2" MIN.)	HMA SHOULDERS, 8" (LOWER LIFTS)
AC/PG:	SBS PG 70-22	SBS PG 70-22	PG 64-22	PG 64-22
DESIGN AIR VOIDS:	4.0% @ N90	4.0% @ N90	4.0% @ N50	4.0% @ N50
MIXTURE COMPOSITION:	IL 19.0, FG	IL 9.5	IL 9.5	IL 19.0
FRICTION AGGREGATE:	N/A	MIX E	MIX C	N/A
QUALITY MANAGEMENT:	PFM	PFM	QC/QA	QC/QA

ASPHALT PAVING LIFT DIAGRAM



DISTRICT SIX

EXAMINED 7/3 20 14
Michelle Ph for John Negand
 OPERATIONS ENGINEER

EXAMINED June 30 20 14
Ron Chamberlain
 PROJECT IMPLEMENTATION ENGINEER

EXAMINED July 11 20 14
Jeffrey R. Moore
 PROGRAM DEVELOPMENT ENGINEER

(1) (2)

CONSTRUCTION CODE	
100% STATE	
PPS NO.	PPS NO.
6-00173-0200	6-00173-0300
	6-00173-0400
	6-00173-0500

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				ROADWAY	ROADWAY
				0005	0005
				RURAL	RURAL
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	66	0	66
20400800	FURNISHED EXCAVATION	CU YD	19	2	17
21400100	GRADING AND SHAPING DITCHES	FOOT	1,950	350	1,600
25000200	SEEDING, CLASS 2	ACRE	0.54	0.06	0.48
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	48	5	43
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	48	5	43
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	48	5	43
25000700	AGRICULTURAL GROUND LIMESTONE	TON	1.08	0.12	0.96
25100115	MULCH, METHOD 2	ACRE	0.54	0.06	0.48
28000500	INLET AND PIPE PROTECTION	EACH	5	1	4
28001000	AGGREGATE (EROSION CONTROL)	TON	140	35	105
28100207	STONE RIPRAP, CLASS A4	TON	139	0	139
28100807	STONE DUMPED RIPRAP, CLASS A4	TON	13	10	3
28200200	FILTER FABRIC	SO YD	174	0	174

(1) (2)

CONSTRUCTION CODE	
100% STATE	
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6-00173-0200	6-00173-0300
	6-00173-0400
	6-00173-0500

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				ROADWAY	ROADWAY
				0005	0005
				RURAL	RURAL
31101900	SUBBASE GRANULAR MATERIAL, TYPE C	TON	20	0	20
40600285	POLYMERIZED BITUMINOUS MATERIALS (PRIME COAT)	POUND	453,038	90,438	362,600
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	3,738	1,062	2,676
40600990	TEMPORARY RAMP	SO YD	1,146	312	834
40603243	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90	TON	63,053	12,296	50,757
40603570	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90	TON	39,663	8,197	31,466
44000152	HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"	SO YD	19,798	0	19,798
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SO YD	236,942	54,595	182,347
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SO YD	41,743	0	41,743
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	34,475	0	34,475
44000162	HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/4"	SO YD	370,891	96,911	273,980
44000168	HOT-MIX ASPHALT SURFACE REMOVAL, 4 3/4"	SO YD	63,076	0	63,076
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	127	0	127
44004000	PAVED DITCH REMOVAL	FOOT	10	0	10

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6-00173-0200	6-00173-0300
	6-00173-0400
	6-00173-0500

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				ROADWAY 0005 RURAL	ROADWAY 0005 RURAL
44004250	PAVED SHOULDER REMOVAL	SO YD	916	0	916
44200541	CLASS A PATCHES, TYPE II, 9 INCH	SO YD	1,181	560	621
44200545	CLASS A PATCHES, TYPE III, 9 INCH	SO YD	151	40	111
44200547	CLASS A PATCHES, TYPE IV, 9 INCH	SO YD	163	40	123
44200577	CLASS A PATCHES, TYPE II, 12 INCH	SO YD	792	198	594
44200581	CLASS A PATCHES, TYPE III, 12 INCH	SO YD	352	88	264
44200583	CLASS A PATCHES, TYPE IV, 12 INCH	SO YD	220	55	165
44200620	CLASS A PATCHES, TYPE II, 14 INCH	SO YD	108	27	81
44200624	CLASS A PATCHES, TYPE III, 14 INCH	SO YD	48	12	36
44200628	CLASS A PATCHES, TYPE IV, 14 INCH	SO YD	30	7	23
44200982	CLASS B PATCHES, TYPE II, 11 INCH	SO YD	351	0	351
44200986	CLASS B PATCHES, TYPE III, 11 INCH	SO YD	71	0	71
44200988	CLASS B PATCHES, TYPE IV, 11 INCH	SO YD	81	0	81
44201299	DOWEL BARS 1 1/2"	EACH	260	0	260

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6-00173-0200	6-00173-0300
	6-00173-0400
	6-00173-0500

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				ROADWAY 0005 RURAL	ROADWAY 0005 RURAL
44213000	PATCHING REINFORCEMENT	SO YD	2,828	973	1,855
44213200	SAW CUTS	FOOT	20,668	6,335	14,333
44213204	TIE BARS 3/4"	EACH	703	80	623
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	19,411	3,420	15,991
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SO YD	3,430	0	3,430
48203100	HOT-MIX ASPHALT SHOULDERS	TON	33,430	6,114	27,316
50105220	PIPE CULVERT REMOVAL	FOOT	140	0	140
54200229	PIPE CULVERTS, CLASS D, TYPE 1 24"	FOOT	40	0	40
54213879	STEEL END SECTIONS 24"	EACH	2	0	2
60100060	CONCRETE HEADWALLS FOR PIPE DRAINS	EACH	44	0	44
60100074	SHOULDER REMOVAL AND REPLACEMENT 8"	FOOT	157,657	0	157,657
60107600	PIPE UNDERDRAINS 4"	FOOT	135,979	0	135,979
60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	4,079	0	4,079
60262700	INLETS TO BE RECONSTRUCTED	EACH	4	0	4

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				6-00173-0200	6-00173-0300 6-00173-0400 6-00173-0500
		ROADWAY	ROADWAY		
		0005	0005		
		RURAL	RURAL		
60262900	INLETS TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	0	1
60608600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06	FOOT	87	0	87
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SO FT	342	0	342
60622400	CONCRETE MEDIAN, TYPE SM-6.06	SO FT	47	0	47
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	3,701	50	3,651
63200310	GUARDRAIL REMOVAL	FOOT	3,701	50	3,651
* 63301995	REMOVE AND REERECT TRAFFIC BARRIER TERMINALS, TYPE 1A	EACH	4	0	4
* 63302700	REMOVE AND REERECT TRAFFIC BARRIER TERMINALS, TYPE 6	EACH	9	1	8
* 63400105	GUARD POSTS	EACH	54	0	54
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	306,897	60,050	246,847
66500105	WOVEN WIRE FENCE, 4'	FOOT	745	100	645
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	30	8	22
67100100	MOBILIZATION	L SUM	1	0.25	0.75
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	10	2	8

* SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				100% STATE	
				PPS NO.	PPS NO.
				6-00173-0200	6-00173-0300 6-00173-0400 6-00173-0500
		ROADWAY	ROADWAY		
		0005	0005		
		RURAL	RURAL		
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	0.25	0.75
70100815	TRAFFIC CONTROL AND PROTECTION, STANDARD 701446	L SUM	1	0.25	0.75
70100820	TRAFFIC CONTROL AND PROTECTION, STANDARD 701451	L SUM	1	0.25	0.75
70100825	TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	L SUM	1	0.25	0.75
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	15	4	11
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	48	12	36
70300100	SHORT TERM PAVEMENT MARKING	FOOT	57,888	16,553	41,335
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	417	0	417
70300230	TEMPORARY PAVEMENT MARKING - LINE 5"	FOOT	401,605	76,613	324,992
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	12,595	6,534	6,061
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	643	0	643
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	1,665	1,515	150
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	10,970	2,994	7,976
* 78004200	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS AND SYMBOLS	SO FT	354	0	354

CONSTRUCTION CODE	
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6-00173-0200	6-00173-0300
	6-00173-0400
	6-00173-0500

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				ROADWAY 0005 RURAL	ROADWAY 0005 RURAL
* 78004220	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	FOOT	41,998	8,411	33,587
* 78004240	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	FOOT	12,295	6,534	5,761
* 78004250	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	FOOT	643	0	643
* 78004280	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	FOOT	1,593	1,515	78
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	62	0	62
* 78009005	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	FOOT	359,607	68,202	291,405
* 78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	300	0	300
* 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	72	0	72
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	4,440	946	3,494
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	231	2	229
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	9	1	8
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	2,350	462	1,888
X0320157	CLEANING UNDERDRAIN OUTLETS	EACH	381	120	261
X0322279	OUTLET MARKER	EACH	697	121	576

14
* SPECIALTY ITEM

CONSTRUCTION CODE	
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6-00173-0200	6-00173-0300
	6-00173-0400
	6-00173-0500

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				ROADWAY 0005 RURAL	ROADWAY 0005 RURAL
X0325893	CLEAN EXISTING END SECTION	EACH	8	2	6
X0358300	REMOVE AND RELAY END SECTIONS	EACH	5	1	4
X2503000	MAINTENANCE MOWING	ACRE	377	72	305
X4403800	MEDIAN SURFACE REMOVAL	SO FT	342	0	342
X4820110	HOT-MIX ASPHALT SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)	SO YD	64,176	16,044	48,132
X6013600	PIPE UNDERDRAINS 4" (MODIFIED)	FOOT	21,485	0	21,485
X6015015	REMOVE AND REPLACE CONCRETE HEADWALLS FOR PIPE UNDERDRAINS	EACH	272	0	272
X6330900	VERTICAL ADJUSTMENT OF GUARDRAIL	FOOT	2,070	0	2,070
X6350110	DELINEATORS (SPECIAL)	EACH	765	65	700
X6350120	DELINEATOR REMOVAL	EACH	400	100	300
X6650202	WOVEN WIRE FENCE REMOVAL	FOOT	745	100	645
X7010206	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401 (SPECIAL)	EACH	10	2	8
X7010410	SPEED DISPLAY TRAILER	CAL MO	48	12	36
* X7830072	GROOVING FOR RECESSED PAVEMENT MARKING 6"	FOOT	347,318	64,976	282,342
Z0010615	CLEANING EXISTING INLETS	EACH	6	0	6
Z0034105	MATERIAL TRANSFER DEVICE	TON	102,716	20,493	82,223
φ Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	Hour	1000	250	750
X6010810	PIPE UNDERDRAINS 4" (MODIFIED) (SPECIAL)	FOOT	645	0	645

17

MAINLINE IMPROVEMENTS																		
BITUMINOUS MATERIAL (PRIME COAT) RATE (REGULAR) - 0.05 LB / SQ FT							40600285	40600285	40603243	40603570	44000159	44000162	44000168	Z0034105				
BITUMINOUS SURFACE / BINDER (112 LBS) RATE - 0.056 T / SY • IN							POLYMERIZED	POLYMERIZED	POLY HMA	POLY HMA	HMA SURFACE	HMA SURFACE	HMA SURFACE	MATERIAL				
BITUMINOUS MATERIAL (PRIME COAT) RATE (FOG) - 0.025 LB / SQ FT							BITUMINOUS	BITUMINOUS	BINDER COURSE	SURF COURSE	REMOVAL	REMOVAL	REMOVAL	TRANSFER				
							(PRIME COAT)	(PRIME COAT)	IL-19.0, FG,	MIX "E",	2 1/2"	3 1/4"	4 3/4"	DEVICE				
LOCATION				LENGTH	WIDTH	AREA	HMA REMOVAL	BINDER	SURFACE	REGULAR COAT	FOG COAT	N90	N90	(-568.89 SY	(-2702.22 SY	(-466.67 SY	TON	
STATION TO STATION				FOOT	FOOT	SQ YD	DEPTH	DEPTH	DEPTH	POUND	POUND	TON	TON	SQ YD	SQ YD	SQ YD	TON	
							INCH	INCH	INCH									
RAMP B (R102)																		
STA	02+31.21	TO	STA	03+24.28	93.07	13.0 TO 16.0	149.95	2.50	2.25	1.50	67.48	33.74	18.89	12.60	149.95	-	-	31.49
STA	03+24.28	TO	STA	30+77.69	2,753.41	16.0	4,894.95	2.50	2.25	1.50	2,202.73	1,101.36	616.76	411.18	4,894.95	-	-	1,027.94
STA	30+77.69	TO	STA	31+95.00	117.31	16.0 TO 145.5	1,052.53	2.50	2.25	1.50	473.64	236.82	132.62	88.41	1,052.53	-	-	221.03
GORE					-		225.78	2.50	2.25	1.50	101.60	50.80	28.45	18.97	225.78	-	-	47.41
RAMP C (R103)																		
STA	00+45.52	TO	STA	01+22.67	77.15	115.5 TO 16.0	563.62	2.50	2.25	1.50	253.63	126.82	71.02	47.34	563.62	-	-	118.36
STA	01+22.67	TO	STA	17+56.75	1,634.08	16.0	2,905.03	2.50	2.25	1.50	1,307.26	653.63	366.03	244.02	2,905.03	-	-	610.06
STA	17+56.75	TO	STA	18+73.11	116.36	15.5 TO 18.0	216.56	2.50	2.25	1.50	97.45	48.73	27.29	18.19	216.56	-	-	45.48
RAMP D (R104)																		
STA	00+00.00	TO	STA	03+86.01	386.01	1.0 TO 22.0	493.24	2.50	2.25	1.50	221.96	110.98	62.15	41.43	493.24	-	-	103.58
STA	03+86.01	TO	STA	22+02.83	1,816.82	16.0	3,229.90	2.50	2.25	1.50	1,453.46	726.73	406.97	271.31	3,229.90	-	-	678.28
STA	22+02.83	TO	STA	23+15.70	112.87	16.0 TO 140.5	981.34	2.50	2.25	1.50	441.60	220.80	123.65	82.43	981.34	-	-	206.08
ATLANTA INTERCHANGE																		
RAMP A (R201)																		
STA	00+00.00	TO	STA	03+57.36	357.36	1.0 TO 20.0	416.92	2.50	2.25	1.50	187.61	93.81	52.53	35.02	416.92	-	-	87.55
STA	03+57.36	TO	STA	22+74.34	1,916.98	16.0	3,407.96	2.50	2.25	1.50	1,533.58	766.79	429.40	286.27	3,407.96	-	-	715.67
GORE					-		245.10	2.50	2.25	1.50	110.29	55.15	30.88	20.59	245.10	-	-	51.47
RAMP B (R202)																		
STA	01+05.28	TO	STA	11+74.03	1,068.75	16.0	1,900.00	2.50	2.25	1.50	855.00	427.50	239.40	159.60	1,900.00	-	-	399.00
STA	11+74.03	TO	STA	19+73.26	799.23	17.0 TO 1.0	799.23	2.50	2.25	1.50	359.65	179.83	100.70	67.14	799.23	-	-	167.84
RAMP C (R203)																		
STA	00+00.00	TO	STA	02+96.38	296.38	1.0 TO 17.0	296.38	2.50	2.25	1.50	133.37	66.69	37.34	24.90	296.38	-	-	62.24
STA	02+96.38	TO	STA	23+44.47	2,048.09	16.0	3,641.05	2.50	2.25	1.50	1,638.47	819.24	458.77	305.85	3,641.05	-	-	764.62
GORE					-		238.52	2.50	2.25	1.50	107.33	53.67	30.05	20.04	238.52	-	-	50.09
RAMP D (R204)																		
STA	01+26.44	TO	STA	11+83.53	1,057.09	16.0	1,879.27	2.50	2.25	1.50	845.67	422.84	236.79	157.86	1,879.27	-	-	394.65
STA	11+83.53	TO	STA	20+32.06	848.53	18.0 TO 1.0	895.67	2.50	2.25	1.50	403.05	201.53	112.85	75.24	895.67	-	-	188.09
										TOTAL:								
										212,481	106,241	63,053	39,663	34,475	370,891	63,076	102,716	

• SUMMARY:

40600285	
POLY BIT MATERIALS (PRIME COAT) (POUND)	
ITEM	QUANTITY
MAINLINE (REGULAR COAT)	212,481
MAINLINE (FOG COAT)	106,241
SHOULDER (REGULAR COAT)	134,317
TOTAL:	453,038

FILE NAME =	USER NAME = sparksgw	DESIGNED - DMS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pw\work\p\idot\sparksgw\10215133\0672041-sht-schedule.dgn		DRAWN - DMS	REVISED -							LOGAN	108	9	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE:	SHEET 2	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 72D41
	PLOT DATE = Jul-14-2014 03:36:44PM	DATE -	REVISED -										

BUTT - JOINTS AND TEMPORARY RAMPS									
LOCATION						40600982 HMA SURF REMOVAL - BUTT JOINT	40600990 TEMPORARY RAMP 1:40 (V:H)		
STATION TO STATION	LENGTH FOOT	WIDTH FOOT	AREA SQ YD	SURF REM DEPTH INCH	SQ YD			SQ YD	
I-155 SB (R006)									
STA 37+95.69 TO STA 38+35.69	40.0	24.0	106.7	3.25	106.67	28.89			
I-55 SB TO I-155 NB (R005)									
STA 00+16.65 TO STA -00+23.35 OFF RAMP	40.0	24.0	106.7	3.25	106.67	28.89			
I-55 SB(A003)									
STA 379+87.59 TO STA 380+27.59	40.0	36.0	160.0	3.25	160.00	43.33			
I-55 NB (R001)									
STA 380+57.28 TO STA 380+97.28	40.0	36.0	160.0	3.25	160.00	43.33			
I-155 SB TO I-55 NB (R004)									
STA 18+21.66 TO STA 18+61.66 SN 054-0051	40.0	16.0	71.1	3.25	71.11	19.26			
I-55 NB TO I-155 NB (R002)									
STA 15+56.96 TO STA 15+16.96 SN 054-0050	40.0	16.0	71.1	3.25	71.11	19.26			
I-55 NB (A004, I55)									
STA 533+29.96 TO STA 532+89.96 SN 054-0053	40.0	24.0	106.7	3.25	106.67	28.89			
STA 540+35.82 TO STA 540+75.82 SN 054-0053	40.0	24.0	106.7	3.25	106.67	28.89			
STA 554+83.64 TO STA 554+43.64 SN 054-0055	40.0	24.0	106.7	3.25	106.67	28.89			
STA 562+33.87 TO STA 562+73.87 SN 054-0055	40.0	24.0	106.7	3.25	106.67	28.89			
STA 575+70.23 TO STA 575+30.23 SN 054-0057	40.0	24.0	106.7	3.25	106.67	28.89			
STA 577+98.08 TO STA 578+38.08 SN 054-0057	40.0	24.0	106.7	3.25	106.67	28.89			
STA 696+51.35 TO STA 696+11.35 SN 054-0061	40.0	24.0	106.7	3.25	106.67	28.89			
STA 702+43.49 TO STA 702+83.49 SN 054-0061	40.0	24.0	106.7	3.25	106.67	28.89			
STATION EQUATION: 790+97.12 BK = STA 184+93.00 AH									
STA 281+03.71 TO STA 280+63.71 SN 054-0063	40.0	24.0	106.7	3.25	106.67	28.89			
STA 284+20.20 TO STA 284+60.20 SN 054-0063	40.0	24.0	106.7	3.25	106.67	28.89			
STA 456+39.65 TO STA 455+99.65 SN 054-0065	40.0	24.0	106.7	3.25	106.67	28.89			
STA 457+59.39 TO STA 457+99.39 SN 054-0065	40.0	24.0	106.7	3.25	106.67	28.89			
STA 590+00.00 TO STA 589+60.00	40.0	24.0	106.7	3.25	106.67	28.89			
I-55 SB (A004, I55)									
STA 534+84.73 TO STA 534+44.73 SN 054-0054	40.0	24.0	106.7	3.25	106.67	28.89			
STA 541+90.61 TO STA 542+15.61 SN 054-0054	25.0	24.0	66.7	4.75	66.67	42.22			
STA 555+05.40 TO STA 554+80.40 SN 054-0056	25.0	24.0	66.7	4.75	66.67	42.22			
STA 562+55.63 TO STA 562+80.63 SN 054-0056	25.0	24.0	66.7	4.75	66.67	42.22			
STA 575+70.23 TO STA 575+45.23 SN 054-0058	25.0	24.0	66.7	4.75	66.67	42.22			
STA 577+98.08 TO STA 578+23.08 SN 054-0058	25.0	24.0	66.7	4.75	66.67	42.22			
STA 697+52.85 TO STA 697+27.85 SN 054-0060	25.0	24.0	66.7	4.75	66.67	42.22			
STA 703+59.73 TO STA 703+84.73 SN 054-0060	25.0	24.0	66.7	4.75	66.67	42.22			
STATION EQUATION: 790+97.12 BK = STA 184+93.00 AH									
STA 280+58.45 TO STA 280+18.45 SN 054-0064	40.0	24.0	106.7	3.25	106.67	28.89			
STA 283+74.95 TO STA 284+14.95 SN 054-0064	40.0	24.0	106.7	3.25	106.67	28.89			
STA 456+47.79 TO STA 456+07.79 SN 054-0066	40.0	24.0	106.7	3.25	106.67	28.89			
STA 457+68.22 TO STA 458+08.22 SN 054-0066	40.0	24.0	106.7	3.25	106.67	28.89			
STA 590+00.00 TO STA 589+60.00	40.0	24.0	106.7	3.25	106.67	28.89			
BUSINESS 55 INTERCHANGE									
RAMP A									
STA 00+45.68 TO STA 00+70.68	40.0	16.0	71.1	2.50	71.11	14.81			
RAMP B									
STA 31+95.00 TO STA 31+70.00	40.0	16.0	71.1	2.50	71.11	14.81			
RAMP C									
STA 00+45.52 TO STA 00+70.52	40.0	16.0	71.1	2.50	71.11	14.81			
RAMP D									
STA 23+15.70 TO STA 22+90.70	40.0	16.0	71.1	2.50	71.11	14.81			

SCHEDULE CONTINUED ->

SCHEDULE CONTINUED

BUTT - JOINTS AND TEMPORARY RAMPS									
LOCATION						40600982 HMA SURF REMOVAL - BUTT JOINT	40600990 TEMPORARY RAMP 1:40 (V:H)		
STATION TO STATION	LENGTH FOOT	WIDTH FOOT	AREA SQ YD	SURF REM DEPTH INCH	SQ YD			SQ YD	
ATLANTA INTERCHANGE									
RAMP A									
STA 22+74.34 TO STA 22+34.34	40.0	16.0	71.1	2.50	71.11	14.81			
RAMP B									
STA 01+05.28 TO STA 01+45.28	40.0	16.0	71.1	2.50	71.11	14.81			
RAMP C									
STA 23+44.47 TO STA 23+04.47	40.0	16.0	71.1	2.50	71.11	14.81			
RAMP D									
STA 01+26.44 TO STA 01+66.44	40.0	16.0	71.1	2.50	71.11	14.81			
TOTAL:						3738	1146		

21400100 GRADING AND SHAPING DITCHES (FOOT)	
LOCATION	LENGTH
STATION TO STATION	FOOT
I-55 MAINLINE	
STA 476+00.00 TO STA 476+50.00	50.0
STA 474+00.00 TO STA 475+00.00	100.0
STA 512+50.00 TO STA 513+50.00	100.0
STA 521+90.00 TO STA 522+90.00	100.0
STA 588+00.00 TO STA 589+00.00	100.0
STA 591+70.00 TO STA 592+70.00	100.0
STA 635+00.00 TO STA 635+50.00	50.0
STA 660+00.00 TO STA 660+50.00	50.0
STATION EQUATION: 790+97.12 BK = STA 184+93.00 AH	
STA 212+46.00 TO STA 213+46.00	100.0
STA 213+58.00 TO STA 214+58.00	100.0
STA 251+00.00 TO STA 252+00.00	100.0
STA 297+88.00 TO STA 298+88.00	100.0
STA 491+00.00 TO STA 492+50.00	150.0
STA 502+52.00 TO STA 503+52.00	100.0
STA 524+00.00 TO STA 526+00.00	200.0
STA 536+00.00 TO STA 540+50.00	450.0
TOTAL: 1950	

X0358300 REMOVE AND RELAY END SECTIONS (EACH)	
LOCATION	QUANTITY
STATION	EACH
I-55 MAINLINE	
STA 498+00.00	1.00
STA 601+60.00	1.00
BUSINESS 55 INTERCHANGE	
RAMP A	
STA 23+00.00	1.00
RAMP B	
STA 21+81.00	1.00
ATLANTA INTERCHANGE	
RAMP A	
STA 9+40.00	1.00
TOTAL: 5	

X0325893 CLEAN EXISTING END SECTION (EACH)	
LOCATION	QUANTITY
STATION	EACH
I-55 MAINLINE	
STA 476+00.0 RT, OUTSIDE	1
STA 529+28.0 LT, OUTSIDE	1
STA 568+83.0 LT, OUTSIDE	1
STA 589+00.0 RT, OUTSIDE	1
STA 612+00.0 LT, OUTSIDE	1
STA 635+00.0 RT, OUTSIDE	1
STA 660+00.0 RT, OUTSIDE	1
STA 703+50.0 LT, OUTSIDE	1
TOTAL: 8	

63400105 GUARD POST (EACH)	
LOCATION	QUANTITY
STATION	EACH
I-55 MEDIAN	
STA 589+20.00 MEDIAN	6
STA 612+11.00 MEDIAN	6
STA 685+50.00 MEDIAN	6
STA 724+80.00 MEDIAN	6
STA 738+80.00 MEDIAN	6
STATION EQUATION	
STA 344+80.00 MEDIAN	6
STA 445+80.00 MEDIAN	6
STA 492+50.00 MEDIAN	6
STA 525+00.00 MEDIAN	6
TOTAL: 54	

FILE NAME =	USER NAME = sparksgw	DESIGNED - DMS	REVISED -
es:\pw\work\p\midot\sparksgw\10215133\0672041-sht-schedule.dgn		DRAWN - DMS	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Jul-14-2014 03:36:45PM	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: SHEET 7 OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	LOGAN	108	14
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72D41	

SEEDING											
AGRICULTURE GROUND LIMESTONE - 2 TON / ACRE FERTILIZER NUTRIENTS - 90 LB / ACRE MULCH - 2 TON / ACRE				25000200 SEEDING CLASS 2	25000400 NITROGEN FERTILIZER NUTRIENT	25000500 PHOSPHORUS FERTILIZER NUTRIENT	25000600 POTASSIUM FERTILIZER NUTRIENT	25000700 AGRICULTURAL GROUND LIMESTONE	25100115 MULCH METHOD 2		
LOCATION	LENGTH	WIDTH	AREA	ACRE	POUND	POUND	POUND	TON	ACRE		
STATION TO STATION	FOOT	FOOT	ACRE	ACRE	POUND	POUND	POUND	TON	ACRE		
I-55 MAINLINE											
STA 476+00.00	TO	STA 476+50.00	50	10	0.01	0.01	1.03	1.03	1.03	0.02	0.01
STA 474+00.00	TO	STA 475+00.00	100	10	0.02	0.02	2.07	2.07	2.07	0.05	0.02
STA 512+50.00	TO	STA 513+50.00	100	10	0.02	0.02	2.07	2.07	2.07	0.05	0.02
STA 521+90.00	TO	STA 522+10.00	20	10	0.00	0.00	0.41	0.41	0.41	0.01	0.00
STA 588+50.00	TO	STA 589+00.00	50	10	0.01	0.01	1.03	1.03	1.03	0.02	0.01
STA 591+70.00	TO	STA 592+50.00	80	10	0.02	0.02	1.65	1.65	1.65	0.04	0.02
STA 635+00.00	TO	STA 635+50.00	50	10	0.01	0.01	1.03	1.03	1.03	0.02	0.01
STA 660+00.00	TO	STA 660+50.00	50	20	0.023	0.023	2.07	2.07	2.07	0.05	0.023
STATION EQ: 790+97.12 BK = STA 184+93.00 AH											
STA 212+46.00	TO	STA 213+46.00	100	10	0.02	0.02	2.07	2.07	2.07	0.05	0.02
STA 213+58.00	TO	STA 214+58.00	100	11	0.03	0.03	2.27	2.27	2.27	0.05	0.03
STA 251+00.00	TO	STA 252+00.00	100	11	0.03	0.03	2.27	2.27	2.27	0.05	0.03
STA 297+88.00	TO	STA 298+88.00	100	12	0.03	0.03	2.48	2.48	2.48	0.06	0.03
STA 491+00.00	TO	STA 492+50.00	150	13	0.04	0.04	4.03	4.03	4.03	0.09	0.04
STA 502+52.00	TO	STA 503+52.00	100	14	0.03	0.03	2.89	2.89	2.89	0.06	0.03
STA 524+00.00	TO	STA 526+00.00	200	15	0.07	0.07	6.20	6.20	6.20	0.14	0.07
STA 536+00.00	TO	STA 540+50.00	450	16	0.17	0.17	14.88	14.88	14.88	0.33	0.17
TOTAL:					0.54	48.45	48.45	48.45	1.08	0.54	

FENCE IMPROVEMENTS			
LOCATION	LENGTH	X6331007 WOVEN WIRE FENCE REMOVAL FOOT	66500105 WOVEN WIRE FENCE, 4' FOOT
STATION	FOOT		
ROADWAY A			
STA 406+40.00	RT	50.00	50.00
I-55 MAINLINE			
STA 508+80.00	RT	50.00	50.00
STA 601+60.00	RT, LT	150.00	150.00
STA 650+00.00	LT	100.00	100.00
STA 681+00.00	RT	195.00	195.00
STATION EQUATION: 790+97.12 BK = STA 184+93.00 AH			
STA 422+25.00	RT	200.00	200.00
TOTAL:		745	745

PIPE CULVERT IMPROVEMENTS			
LOCATION	50105220 PIPE CULVERT REMOVAL	542D0229 PIPE CULVERT CLASS D, TY 1 24"	54213879 STEEL END SECTIONS 24"
STATION	FOOT	FOOT	EACH
I-55 MAINLINE			
STA 547+00.00	90.00	20.00	1.00
STA 553+20.00	50.00	20.00	1.00
TOTAL:		140	2

GUARDRAIL IMPROVEMENTS								
LOCATIONS TO BE VERIFIED BY THE ENGINEER		63000001 STEEL PLATE BEAM GR, TY A, 6' POSTS (6.25' OFFSET)	63302700 REM & REERECT TRAFFIC BARRIER TERMINALS TY 6 EACH	78200410 GUARDRAIL MARKERS, TYPE A (25' OFFSET)	78201000 TERMINAL MARKER - DIRECT APPLIED EACH	63301995 REM & REERECT TRAF BARRIER TERMINALS TYPE 1A EACH	X6330900 VERTICAL ADJUSTMENT OF GUARDRAIL FOOT	63200310 GUARDRAIL REMOVAL FOOT
LOCATION	ESTIMATED LENGTH	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT
STATION	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT
I-55 MAINLINE								
STA 491+30.00	MEDIAN	-	-	1	1	-	-	-
STA 533+00.00	LT, OUTSIDE	50.00	50	-	2	-	-	50
STA 546+00.00	LT, OUTSIDE	-	-	1	1	-	-	-
STA 552+40.00	RT, OUTSIDE	250.00	250	1	10	-	-	250
STA 568+10.00	RT, OUTSIDE	790.00	-	-	32	1	790.00	-
STA 577+50.00	RT, OUTSIDE	513.00	513	1	21	1	-	513
STA 690+00.00	RT, OUTSIDE	592.00	-	-	24	1	592.00	-
STATION EQUATION								
STA 278+00.00	RT, OUTSIDE	307.00	307	1	12	1	-	307
STA 278+30.00	RT, MEDIAN	264.00	264	1	11	1	-	264
STA 283+75.00	LT, MEDIAN	373.00	-	-	15	1	373.00	-
STA 283+75.00	LT, OUTSIDE	315.00	-	-	13	1	315.00	-
STA 453+32.00	RT, OUTSIDE	307.00	307	1	12	1	-	307
STA 457+68.00	LT, OUTSIDE	200.00	200	1	8	1	-	200
STA 457+68.00	RT, OUTSIDE	1810.17	1810	1	72	1	-	1810
TOTAL:		3701	3701	9	231	9	2070	3701

60262700 INLETS TO BE RECONSTRUCTED (EACH)	
LOCATION	QUANTITY
STATION	EACH
I-55 MAINLINE	
STA 686+00.00	1
STA 787+00.00	1
STA 789+74.00	1
STATION EQN STA 790+97.12 BK = STA 184+93.00 AH	
STA 311+00.00	1
TOTAL: 4	

44004000 PAVED DITCH REMOVAL (FOOT)	
LOCATION	QUANTITY
STATION	FOOT
I-55 MAINLINE	
STA 686+00.00	10
TOTAL: 10	

20010615 CLEANING EXISTING INLETS (EACH)	
LOCATION	QUANTITY
STATION	EACH
I-55 MAINLINE	
JOBSITE	6.0
TOTAL: 6	

60262900 INLETS TO BE RECONSTRUCTED WITH TYPE 1 FRAME, OPEN LID (EACH)	
LOCATION	QUANTITY
STATION	EACH
I-55 MAINLINE	
STA 686+00.00	1
TOTAL: 1	

20400800 FURNISHED EXCAVATION (CU YD)	
LOCATION	QUANTITY
STATION	CY YD
I-55 MAINLINE	
STA 476+00.00	2.0
STA 547+00.00	5.0
STA 553+20.00	5.0
STA 681+25.00	2.0
STA 786+80.00	5.0
TOTAL: 19	

SCHEDULE CONTINUED

MAINLINE PATCHING											
QUANTITY AFTER MILLING OPERATION - CONC PAVT, 9"					44200541 CLASS A PATCHES, TY II, 9 IN	44200545 CLASS A PATCHES, TY III, 9 IN	44200547 CLASS A PATCHES, TY IV, 9 IN	44213200 SAW CUTS	44213000 PATCHING REINFORCE- MENTS	44213204 TIE BARS, 3/4"	
MILE MARKER	LANE	LENGTH FOOT	WIDTH FOOT	AREA SQ YD	SQ YD	SQ YD	SQ YD	FOOT	SQ YD	EACH	
128.4	SBDL	5	12	6.7	6.7	--	--	58.0	6.7	--	
128.4	SBDL	5	12	6.7	6.7	--	--	58.0	6.7	--	
128.2	SBDL	5	12	6.7	6.7	--	--	58.0	6.7	--	
127.7	SBDL	6	12	8.0	8.0	--	--	60.0	8.0	--	
127.7	SBDL	5	12	6.7	6.7	--	--	58.0	6.7	--	
127.5	SBDL	5	12	6.7	6.7	--	--	58.0	6.7	--	
127.4	SBDL	30	12	40.0	--	--	40.0	108.0	40.0	28.0	
127.3	SBDL	5	12	6.7	6.7	--	--	58.0	6.7	--	
127.3	SBDL	5	12	6.7	6.7	--	--	58.0	6.7	--	
RAMP STA AT I-55 AND I-155 SPLIT (SB LANES ONLY)											
127.2	SBPL	6	12	8.0	8.0	--	--	60.0	8.0	--	
127.2	SBDL	6	12	8.0	8.0	--	--	60.0	8.0	--	
127.1	SBDL	8	12	10.7	10.7	--	--	64.0	10.7	--	
127.0	SBDL	6	12	8.0	8.0	--	--	60.0	8.0	--	
126.9	SBDL	5	12	6.7	6.7	--	--	58.0	6.7	--	
126.8	SBDL	5	12	6.7	6.7	--	--	58.0	6.7	--	
126.6	SBDL	5	12	6.7	6.7	--	--	58.0	6.7	--	
126.6	SBDL	7	12	9.3	9.3	--	--	62.0	9.3	--	
RESUME I-55 STATIONING											
126.5	SBML	5	12	6.7	6.7	--	--	58.0	6.7	--	
126.4	SBML	8	12	10.7	10.7	--	--	64.0	10.7	--	
126.4	SBML	6	12	8.0	8.0	--	--	60.0	8.0	--	
126.4	SBPL	6	12	8.0	8.0	--	--	60.0	8.0	--	
126.4	SBML	8	12	10.7	10.7	--	--	64.0	10.7	--	
I-155 MAINLINE NORTHBOUND LANES											
0.0	NBPL	6	12	8.0	8.0	--	--	60.0	8.0	--	
0.0	NBPL	6	12	8.0	8.0	--	--	60.0	8.0	--	
0.0	NBDL	6	12	8.0	8.0	--	--	60.0	8.0	--	
0.0	NBDL	10	12	13.3	13.3	--	--	68.0	13.3	--	
0.0	NBPL	7	12	9.3	9.3	--	--	62.0	9.3	--	
0.1	NBDL	8	12	10.7	10.7	--	--	64.0	10.7	--	
0.1	NBPL	8	12	10.7	10.7	--	--	64.0	10.7	--	
0.2	NBDL	6	12	8.0	8.0	--	--	60.0	8.0	--	
0.2	NBPL	6	12	8.0	8.0	--	--	60.0	8.0	--	
0.2	NBDL	6	12	8.0	8.0	--	--	60.0	8.0	--	
0.2	NBPL	6	12	8.0	8.0	--	--	60.0	8.0	--	
0.3	NBDL	6	12	8.0	8.0	--	--	60.0	8.0	--	
0.3	NBPL	6	12	8.0	8.0	--	--	60.0	8.0	--	
0.4	NBDL	10	6	6.7	6.7	--	--	44.0	6.7	--	
0.5	NBDL	6	6	4.0	4.0	--	--	36.0	4.0	--	
0.6	NBDL	5	12	6.7	6.7	--	--	58.0	6.7	--	
0.7	NBDL	6	12	8.0	8.0	--	--	60.0	8.0	--	
0.7	NBPL	6	12	8.0	8.0	--	--	60.0	8.0	--	
I-155 MAINLINE SOUTHBOUND LANES											
0.7	SBDL	6	12	8.0	8.0	--	--	60.0	8.0	--	
0.7	SBPL	6	12	8.0	8.0	--	--	60.0	8.0	--	
0.2	SBDL	8	6	5.3	5.3	--	--	40.0	5.3	--	
TOTAL:					1181	151	163	9488	1496	234	

ANTICIPATED MAINLINE PATCHING - SPECIFIC LOCATION AND PATCH SIZE TO BE DETERMINED BY ENGINEER ON FIELD - VARIOUS OPERATIONS											
LOCATION WITH 3.25" OF BINDER BEFORE MILLING OPERATION - CONC PAVT + HMA OVERLAY, 12"					44200577 CLASS A PATCHES, TY II, 12 IN	44200581 CLASS A PATCHES, TY III, 12 IN	44200583 CLASS A PATCHES, TY IV, 12 IN	44213200 SAW CUTS	44213000 PATCHING REINFORCE- MENTS	44213204 TIE BARS, 3/4"	
MILE MARKER	LANE	LENGTH FOOT	WIDTH FOOT	AREA SQ YD	SQ YD	SQ YD	SQ YD	FOOT	SQ YD	EACH	
VARIOUS	VARIOUS	VARIOUS	VARIOUS	VARIOUS	352.0	132.0	88.0	3303.4	520.9	81.5	
VARIOUS	VARIOUS	VARIOUS	VARIOUS	VARIOUS	440.0	220.0	132.0	4129.3	651.0	101.8	
TOTAL:					792	352	220	7433	1172	183	

ANTICIPATED MAINLINE PATCHING - SPECIFIC LOCATION AND PATCH SIZE TO BE DETERMINED BY ENGINEER ON FIELD - VARIOUS OPERATIONS											
LOCATION WITH 4.75" BINDER BEFORE MILLING OPERATION - CONC PAVT + HMA OVERLAY, 14"					44200620 CLASS A PATCHES, TY II, 14 IN	44200624 CLASS A PATCHES, TY III, 14 IN	44200628 CLASS A PATCHES, TY IV, 14 IN	44213200 SAW CUTS	44213000 PATCHING REINFORCE- MENTS	44213204 TIE BARS, 3/4"	
MILE MARKER	LANE	LENGTH FOOT	WIDTH FOOT	AREA SQ YD	SQ YD	SQ YD	SQ YD	FOOT	SQ YD	EACH	
VARIOUS	VARIOUS	VARIOUS	VARIOUS	VARIOUS	48.0	18.0	12.0	450.5	71.0	11.1	
VARIOUS	VARIOUS	VARIOUS	VARIOUS	VARIOUS	60.0	30.0	18.0	563.1	89.0	13.9	
TOTAL:					108	48	30	1014	160	25	

•• SUMMARY:

44213200 SAW CUTS (FOOT)		44213204 TIE BARS, 3/4" (EACH)	
ITEM	QUANTITY	ITEM	QUANTITY
CLASS A, 9" PATCHES	9488	CLASS A, 9" PATCHES	234
CLASS A, 12" PATCHES	7433	CLASS A, 12" PATCHES	183
CLASS A, 14" PATCHES	1014	CLASS A, 14" PATCHES	25
CLASS B, 11" PATCHES	2734	CLASS B, 11" PATCHES	260
TOTAL:	20668	TOTAL:	703

44213000 PATCHING REINFORCEMENTS (SQ YD)	
ITEM	QUANTITY
CLASS A, 9" PATCHES	1496
CLASS A, 12" PATCHES	1172
CLASS A, 14" PATCHES	160
TOTAL:	2828

RAMP PATCHING								
			44200982 CLASS B PATCHES, TY II, 11 IN	44200986 CLASS B PATCHES, TY III, 11 IN	44200988 CLASS B PATCHES, TY IV, 11 IN	44213200 SAW CUTS	44201299 DOWEL BARS 1 1/2"	44213204 TIE BARS, 3/4"
LENGTH	WIDTH	AREA						
FOOT	FOOT	SQ YD	SQ YD	SQ YD	SQ YD	FOOT	SQ YD	EACH
EXIT 133 -- BUSINESS LOOP 55 LINCOLN								
NB I-55 to I-55 BL								
16	5	8.9	8.9	--	--	52.0	7.0	7.0
16	6	10.7	10.7	--	--	56.0	7.0	7.0
16	6	10.7	10.7	--	--	56.0	7.0	7.0
I-55 BL to NB I-55								
16	6	10.7	10.7	--	--	56.0	7.0	7.0
16	6	10.7	10.7	--	--	56.0	7.0	7.0
16	6	10.7	10.7	--	--	56.0	7.0	7.0
16	5	8.9	8.9	--	--	52.0	7.0	7.0
16	5	8.9	8.9	--	--	52.0	7.0	7.0
16	5	8.9	8.9	--	--	52.0	7.0	7.0
SB I-55 to I-55 BL								
16	9	16.0	--	16.0	--	68.0	7.0	7.0
16	6	10.7	10.7	--	--	56.0	7.0	7.0
16	5	8.9	8.9	--	--	52.0	7.0	7.0
16	5	8.9	8.9	--	--	52.0	7.0	7.0
16	5	8.9	8.9	--	--	52.0	7.0	7.0
8	15	13.3	13.3	--	--	76.0	3.5	3.5
I-55 BL to SB I-55								
6	20	13.3	13.3	--	--	92.0	2.6	2.6
7	12	9.3	9.3	--	--	62.0	3.1	3.1
16	5	8.9	8.9	--	--	52.0	7.0	7.0
16	7	12.4	12.4	--	--	60.0	7.0	7.0
EXIT 140 -- CH 6 ATLANTA								
NB I-55 to CH 6 RAMP								
16	8	14.2	14.2	--	--	64.0	7.0	7.0
16	6	10.7	10.7	--	--	56.0	7.0	7.0
CH 6 TO NB I-55 RAMP								
16	5	8.9	8.9	--	--	52.0	7.0	7.0
16	5	8.9	8.9	--	--	52.0	7.0	7.0
16	9	16.0	--	16.0	--	68.0	7.0	7.0
16	5	8.9	8.9	--	--	52.0	7.0	7.0
16	5	8.9	8.9	--	--	52.0	7.0	7.0
9	27	27.0	--	--	27.0	126.0	3.9	3.9
16	5	8.9	8.9	--	--	52.0	7.0	7.0
SB I-55 to CH 6 RAMP								
8	15	13.3	13.3	--	--	76.0	3.5	3.5
16	6	10.7	10.7	--	--	56.0	7.0	7.0
16	5	8.9	8.9	--	--	52.0	7.0	7.0
19	6	12.7	12.7	--	--	62.0	8.3	8.3
6	42	28.0	--	--	28.0	180.0	2.6	2.6
16	12	21.3	--	21.3	--	80.0	7.0	7.0
16	10	17.8	--	17.8	--	72.0	7.0	7.0
16	8	14.2	14.2	--	--	64.0	7.0	7.0
15	6	10.0	10.0	--	--	54.0	6.6	6.6
5	46	25.6	--	--	25.6	194.0	2.2	2.2
CH 6 TO SB I-55 RAMP								
16	6	10.7	10.7	--	--	56.0	7.0	7.0
16	5	8.9	8.9	--	--	52.0	7.0	7.0
16	5	8.9	8.9	--	--	52.0	7.0	7.0
TOTAL:			351	71	81	2734	260	260

RAISED REFLECTIVE PAVEMENT MARKERS									
					78100100 RAISED REFL PAVEMENT MARKER EACH	78300200 RAISED REFL PAVT MARK REMOVAL EACH			
LOCATION		STATIONING	LANE LOCATION	COLOR	SPACING # PER OFFSET				
STATION TO STATION									
I-55 NORTHBOUND									
STA 380+57.32	TO	STA 398+95.73	ROADWAY 'A'	CENTERLINE	CRYSTAL	2 PER 80	92	46	
STA 398+97.73	TO	STA 445+62.78	ROADWAY 'A'	CENTERLINE	CRYSTAL	2 PER 80	117	59	
STA EON STA 445+62.78 BK = STA 465+57.74 AH									
STA 465+58.00	TO	STA 533+29.96	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	170	85	
STA 540+35.82	TO	STA 554+83.64	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	37	19	
STA 562+33.87	TO	STA 575+70.23	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	34	17	
STA 577+98.03	TO	STA 696+51.35	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	297	149	
STA 702+43.48	TO	STA 790+97.00	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	222	111	
STA EON STA 790+97.12 BK = STA 184+93.00 AH									
STA 184+93.00	TO	STA 281+03.71	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	241	121	
STA 284+20.20	TO	STA 456+39.65	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	431	216	
STA 457+59.39	TO	STA 590+00.00	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	332	166	
I-55 SOUTHBOUND									
STA 379+87.59	TO	STA 384+79.44	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	26	14	
STA EON STA 384+79 = I-55 STA 385+00.00									
STA 385+00.00	TO	STA 385+77.02	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	4	2	
STA 385+77.02	TO	STA 397+69.00	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	30	15	
I-55 SB STA 397+69.16 = RDWY 'C' STA 43+41.63									
STA 43+42.00	TO	STA 0+00.00	ROADWAY 'C'	CENTERLINE	CRYSTAL	2 PER 80	109	-55	
RDWY 'C' STA 0+00.00 = I-55 BL STA 452+88.82									
STA 452+89.00	TO	STA 534+84.73	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	205	103	
STA 541+90.61	TO	STA 555+05.40	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	33	17	
STA 562+55.63	TO	STA 575+70.23	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	33	17	
STA 577+98.03	TO	STA 697+52.85	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	299	150	
STA 703+58.10	TO	STA 790+97.00	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	219	110	
STA EON STA 790+97.12 BK = STA 184+93.00 AH									
STA 184+93.00	TO	STA 280+58.45	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	240	120	
STA 283+74.95	TO	STA 456+47.79	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	432	216	
STA 457+68.22	TO	STA 590+00.00	I-55 BASELINE	CENTERLINE	CRYSTAL	2 PER 80	331	166	
I-55 NB AND I-155 NB									
STA 391+95.00	TO	STA 398+96.00	ROADWAY 'A'	GORE	CRYSTAL	1 PER 40	36	36	
I-55 SB AND I-155 SB									
STA 37+96.00	TO	STA 46+31.00	ROADWAY 'F'	LANE DROP	AMBER	1 PER 40	21	21	
STA 46+31.00	TO	STA 56+81.00	I-55 BASELINE	GORE	CRYSTAL	1 PER 40	27	27	
STA 387+20.00	TO	STA 397+70.00	I-55 BASELINE	GORE	CRYSTAL	1 PER 40	27	27	
I-55 SB AND I-155 NB									
STA 454+04.00	TO	STA 458+13.00	I-55 BASELINE	GORE	CRYSTAL	1 PER 40	22	22	
STA 454+23.00	TO	STA 460+85.00	I-55 BASELINE	OUTSIDE EOP	CRYSTAL	1 PER 20	34	34	
I-55 NB AND I-155 SB									
STA 18+21.66	TO	STA 23+05.00	RAMP 'D'	INSIDE EOP	AMBER	1 PER 40	13	13	
STA 18+21.66	TO	STA 23+05.00	RAMP 'D'	OUTSIDE EOP	CRYSTAL	1 PER 40	13	13	

SCHEDULE CONTINUED ON NEXT SHEET

FILE NAME =	USER NAME = sparksgw	DESIGNED - DMS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	2041-sht-schedule.dgn	DRAWN - DMS	REVISED -		SCALE:	SHEET 12	OF	SHEETS	STA.	TO STA.	LOGAN	108	19
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -		CONTRACT NO. 72D41								
	PLOT DATE = Jul-14-2014 03:36:47PM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

RAISED REFLECTIVE PAVEMENT MARKERS										
LOCATION					STATIONING	LANE LOCATION	COLOR	SPACING # PER OFFSET	78100100 RAISED REFL PAVEMENT MARKER EACH	78300200 RAISED REFL PAVT MARK REMOVAL EACH
STATION TO STATION										
BUSINESS 55 INTERCHANGE										
RAMP B										
STA 2+31.21	TO	STA 6+63.99			RAMP 'B'	OUTSIDE EOP	CRYSTAL	1 PER 20	22	22
STA 6+64.00	TO	STA 22+54.00			RAMP 'B'	INSIDE EOP	AMBER	1 PER 40	40	40
STA 2+87.00	TO	STA 6+63.99			RAMP 'B'	GORE	CRYSTAL	1 PER 40	20	10
STA 703+00.00	TO	STA 706+76.00			I-55 BASELINE	GORE	CRYSTAL	1 PER 40	10	10
RAMP D										
STA 0+00.00	TO	STA 6+49.00			RAMP 'D'	OUTSIDE EOP	CRYSTAL	1 PER 20	33	33
STA 6+49.00	TO	STA 15+14.00			RAMP 'D'	INSIDE EOP	AMBER	1 PER 40	22	22
STA 2+72.00	TO	STA 6+49.00			RAMP 'D'	GORE	CRYSTAL	1 PER 40	20	10
STA 728+63.00	TO	STA 732+52.00			I-55 BASELINE	GORE	CRYSTAL	1 PER 40	10	10
ATLANTA INTERCHANGE										
RAMP A										
STA 0+00.00	TO	STA 6+64.61			RAMP 'A'	OUTSIDE EOP	CRYSTAL	1 PER 20	34	34
STA 6+98.46	TO	STA 12+94.69			RAMP 'A'	INSIDE EOP	AMBER	1 PER 40	15	15
STA 3+57.00	TO	STA 6+43.04			RAMP 'A'	GORE	CRYSTAL	1 PER 40	8	8
STA 505+99.88	TO	STA 509+41.47			I-55 BASELINE	GORE	CRYSTAL	1 PER 40	9	9
RAMP C										
STA 0+00.00	TO	STA 6+57.28			RAMP 'C'	OUTSIDE EOP	CRYSTAL	1 PER 20	33	33
STA 6+68.64	TO	STA 13+09.06			RAMP 'C'	INSIDE EOP	AMBER	1 PER 40	17	17
STA 2+96.38	TO	STA 6+57.28			RAMP 'C'	GORE	CRYSTAL	1 PER 40	10	10
STA 481+99.70	TO	STA 485+71.68			I-55 BASELINE	GORE	CRYSTAL	1 PER 40	10	10
TOTAL:									4,440	2,350

PIPE UNDERDRAIN IMPROVEMENTS																
THEORETICAL QUANTITY									60100060 CONCRETE HEADWALL FOR PIPE DRAINS EACH	60100074 SHOULDER REMOVAL & REPLACEMENT, 8" FOOT	60107600 PIPE UNDER- DRAINS, 4" FOOT	60108100 PIPE UNDER- DRAINS, 4" (SPECIAL) AVG L = 15' FOOT	X0322279 OUTLET MARKER EACH	X6013600 PIPE UNDER- DRAINS, 4" (MODIFIED) FOOT	X6015015 REM & REPL CONC HDWL FOR PIPE UNDERDRAINS EACH	PIPE UNDER- DRAINS, 4" (MODIFIED) (SPECIAL) L= 15' FOOT
LOCATION			LENGTH	WIDTH	OUTLETTING	FOOT	SHOULDER	PIPE LENGTH								
STATION TO STATION			FOOT	FOOT	FOOT	FOOT	FOOT	FOOT								
I-55 NB																
STA 789+04.12	TO	STA 790+97.12 BK	MEDIAN	193.00	6.0	5.79	-	199.00	193.00	5.79	0	-	0			
STA 789+04.12	TO	STA 790+97.12 BK	OUTSIDE	193.00	10.0	5.79	-	203.00	193.00	5.79	0	-	0			
STA 184+93.00	AH	TO	STA 280+98.86	MEDIAN OMIT SN 054-0063	9605.86	6.0	288.18	-	9611.86	9605.86	288.18	19	-	19		
STA 184+93.00	AH	TO	STA 281+08.56	OUTSIDE OMIT SN 054-0063	9615.56	10.0	288.47	-	9625.56	9615.56	288.47	19	-	19		
STA 284+15.35	TO	STA 456+40.33	MEDIAN OMIT SN 054-0065	17224.98	6.0	516.75	-	17230.98	17224.98	516.75	34	-	34			
STA 284+25.05	TO	STA 456+38.97	OUTSIDE OMIT SN 054-0065	17213.92	10.0	516.42	-	17223.92	17213.92	516.42	34	-	34			
STA 457+60.45	TO	STA 590+00.00	MEDIAN	13239.55	6.0	397.19	-	13245.55	13239.55	397.19	26	-	26			
STA 457+58.34	TO	STA 478+95.07	OUTSIDE ATLANTA INTERCHANGE	2136.73	10.0	64.10	-	2146.73	2136.73	64.10	4	-	4			
STA 485+24.89	TO	STA 508+30.51	OUTSIDE	2305.62	10.0	69.17	-	2315.62	2305.62	69.17	5	-	5			
STA 519+65.42	TO	STA 590+00.00	OUTSIDE	7034.58	10.0	211.04	-	7044.58	7034.58	211.04	14	-	14			
I-55 SB																
STA 789+04.12	TO	STA 790+97.12 BK	MEDIAN	193.00	6.0	5.79	-	199.00	193.00	5.79	0	-	0			
STA 789+04.12	TO	STA 790+97.12 BK	OUTSIDE	193.00	10.0	5.79	1	203.00	-	-	1	193.00	-	6		
STA 184+93.00	AH	TO	STA 190+00.00	MEDIAN	507.00	6.0	15.21	-	513.00	507.00	15.21	1	-	1		
STA 184+93.00	AH	TO	STA 190+00.00	OUTSIDE	507.00	10.0	15.21	1	517.00	-	-	1	507.00	15.21		
STA 190+00.00	TO	STA 280+63.30	MEDIAN OMIT SN 054-0064	9063.3	6.0	271.90	-	9069.30	9063.30	271.90	18	-	18			
STA 190+00.00	TO	STA 280+53.60	OUTSIDE OMIT SN 054-0064	9053.6	10.0	271.61	18	9063.60	-	-	18	9053.60	-	271.61		
STA 283+79.80	TO	STA 456+47.07	MEDIAN OMIT SN 054-0066	17267.27	6.0	518.02	-	17273.27	17267.27	518.02	35	-	35			
STA 283+69.10	TO	STA 401+00.00	OUTSIDE	11730.9	10.0	351.93	23	11740.90	-	-	23	11730.90	-	351.93		
STA 401+00.00	TO	STA 456+48.52	OUTSIDE OMIT SN 054-0066	5548.52	10.0	166.46	-	5558.52	5548.52	166.46	11	-	11			
STA 457+67.11	TO	STA 590+00.00	MEDIAN	13232.89	6.0	396.99	-	13238.89	13232.89	396.99	26	-	26			
STA 457+69.34	TO	STA 473+44.01	OUTSIDE ATLANTA INTERCHANGE	1574.67	10.0	47.24	-	1584.67	1574.67	47.24	3	-	3			
STA 485+27.64	TO	STA 506+55.82	OUTSIDE	2128.18	10.0	63.85	-	2138.18	2128.18	63.85	4	-	4			
STA 512+98.79	TO	STA 590+00.00	OUTSIDE	7701.21	10.0	231.04	-	7711.21	7701.21	231.04	15	-	15			
TOTAL:									44	157657	135979	4079	316	21485	272	645

FILE NAME =	USER NAME = shahdm	DESIGNED - DMS	REVISED -
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Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = Jul-18-2014 12:24:39PM	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: SHEET 13 OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	**	LOGAN	108	20
CONTRACT NO. 72D41			ILLINOIS FED. AID PROJECT	

SCHEDULE CONTINUED

PAVEMENT MARKINGS																	
NOTE 1: 10' SKIP @ 40' OFFSET NOTE 2: 3' SKIP @ 12' OFFSET NOTE 3: CENTERLINE 4" - 4' @ 40 OFFSET NOTE 4: EDGE LINE: 4" - 4' @ 100' OFFSET			70300100		70301000	78004200	78004220	78004240	78004250	78004280		78009005		78009000	78009008	78009024	X7830072
			SHORT TERM PAVT MARKING CENTERLINE WHITE (NOTE 3) 2 APP. FOOT	SHORT TERM PAVT MARKING CENTERLINE YELLOW (NOTE 4) 1 APP. FOOT	WORK ZONE PAVT MARK REM 1 APP. SQ FT	PRE PLAST PAVT MARK, TY B - INLAID - LETTERS & SYMBOLS WHITE (NOTE 1 & 2) SQ FT	PRE PLAST PAVT MARK, TY B - INLAID - LINE 5" WHITE (NOTE 1 & 2) FOOT	PRE PLAST PAVT MARK, TY B - INLAID - LINE 8" WHITE FOOT	PRE PLAST PAVT MARK, TY B - INLAID - LINE 12" WHITE FOOT	PRE PLAST PAVT MARK, TY B - INLAID - LINE 24" WHITE FOOT	PRE PLAST PAVT MARK, TY B - INLAID - LINE 24" YELLOW FOOT	MOD URETH PAVT MARK - LINE 5" YELLOW MEDIAN FOOT	MOD URETH PAVT MARK - LINE 5" WHITE OUTSIDE FOOT	MOD URETH PAVT MARK- LETTERS & SYMBOLS WHITE SQ FT	MOD URETH PAVT MARK- LINE 8" WHITE FOOT	MOD URETH PAVT MARK- LINE 24" WHITE FOOT	GROOVE FOR RECESSED PAVT MARK 6" FOOT
LOCATION	LENGTH	AREA															
STATION TO STATION	FOOT	SQ FT															
I-55 SOUTHBOUND (A004)																	
STA 465+56.26 TO STA 534+84.73	6,928.47		1,385.69	277.14	323.33		1,732.12					6,928.47	6,928.47				13,856.94
STA 534+84.73 TO STA 541+90.61	705.88		141.18	28.24	32.94							705.88	882.35				
STA 541+90.61 TO STA 555+05.40	1,314.79		262.96	52.59	61.36		328.70					1,314.79	1,314.79				2,629.58
STA 555+05.40 TO STA 562+55.63	1,314.60		150.05	30.01	35.01							750.23	937.79				
STA 562+55.63 TO STA 575+70.23	1,314.60		262.92	52.58	61.35		328.65					1,314.60	1,314.60				2,629.20
STA 575+70.23 TO STA 577+98.08	227.85		45.57	9.11	10.63							227.85	284.81				
STA 577+98.08 TO STA 697+52.85	11,954.77		2,390.95	478.19	557.89		2,988.69					11,954.77	11,954.77				23,909.54
STA 697+52.85 TO STA 703+59.73	606.88		121.38	24.28	28.32							606.88	758.60				
STA 703+59.73 TO STA 735+32.73	3,173.00		634.60	126.92	148.07		793.25					3,173.00					3,173.00
STA 708+03.23 TO STA 728+63.06	2,059.83		411.97	82.39	96.13		514.96						2,059.83				2,059.83
STA 735+32.73 TO STA 790+97.12 BK	5,564.39		1,112.88	222.58	259.67		1,391.10					5,564.39	5,564.39				11,128.78
STA 184+93.00 AH TO STA 280+58.45	9,565.45		1,913.09	382.62	446.39		2,391.36					9,565.45	9,565.45				19,130.90
STA 280+58.45 TO STA 283+74.95	316.50		63.30	12.66	14.77							316.50	395.63				
STA 283+74.95 TO STA 456+47.79	17,272.84		3,454.57	690.91	806.07		4,318.21					17,272.84	17,272.84				34,545.68
STA 456+47.79 TO STA 457+68.22	120.43		24.09	4.82	5.62							120.43	150.54				
STA 457+68.22 TO STA 473+44.02	1,575.80		315.16	63.03	73.54								1,575.80				1,575.80
STA 457+68.22 TO STA 512+98.76	5,530.54		1,106.11	221.22	258.09		1,382.64					5,530.54					5,530.54
STA 485+27.64 TO STA 505+99.88	2,072.24		414.45	82.89	96.70								2,072.24				2,072.24
STA 512+98.79 TO STA 590+00.00	7,701.21		1,540.24	308.05	359.39		1,925.30					7,701.21	7,701.21				15,402.42
BUSINESS 55 INTERCHANGE																	
RAMP A (R101)																	
STA 00+00.00 TO STA 27+59.81	2,759.81			110.39	36.80							2,759.81	2,759.81				5,519.62
STA 27+59.81 TO STA 38+95.57	1,135.76			45.43	15.14								1,135.76				1,135.76
STA 27+59.81 TO STA 31+76.47	416.66		833.32		138.89			833.32									
STA 31+76.47 TO STA 38+95.57	719.10						179.78										
STA 1+38		24.33				24.33											
RAMP B (R102)																	
STA 00+00.00 TO STA 06+63.99	663.99			26.56	8.85								663.99				663.99
STA 06+63.99 TO STA 32+40.56	2,576.57			103.06	34.35							2,576.57	2,576.57				5,153.14
AROUND ISLAND	73.00												73.00				73.00
STA 00+00.00 TO STA 02+87.07	287.07						71.77										
STA 02+87.07 TO STA 06+63.99	376.92		753.84		125.64			753.84									
GORE CHEVERONS	153.91		153.91		25.65				153.91								
STA 29+37		24.33				24.33											
STA 30+62		24.33				24.33											
STA 31+67	20.50									20.50							
STA 31+77	18.50									18.50							
STA 31+45		15.6				15.60											
STA 31+50		15.6				15.60											
RAMP C (R103)																	
STA 00+00.00 TO STA 14+31.87	1,431.87			57.27	19.09							1,431.87	1,431.87				2,863.74
STA 14+31.87 TO STA 25+37.77	1,105.90			44.24	14.75								1,105.90				1,105.90
STA 14+31.87 TO STA 17+56.74	324.87		649.74		108.29			649.74									
STA 17+56.74 TO STA 25+37.77	781.03						195.26										
STA 1+38		24.33				24.33											

SCHEDULE CONTINUED ON NEXT SHEET

FILE NAME =	USER NAME = sparksgw	DESIGNED - DMS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Default	DRAWN - DMS	REVISED -		SCALE: 1" = 100.0000' / 1" = 100.0000'	SHEET 15	OF	SHEETS	STA.	TO STA.	LOGAN	108	22
		CHECKED -	REVISED -								CONTRACT NO.	72D41	
		DATE -	REVISED -								ILLINOIS FED. AID PROJECT		

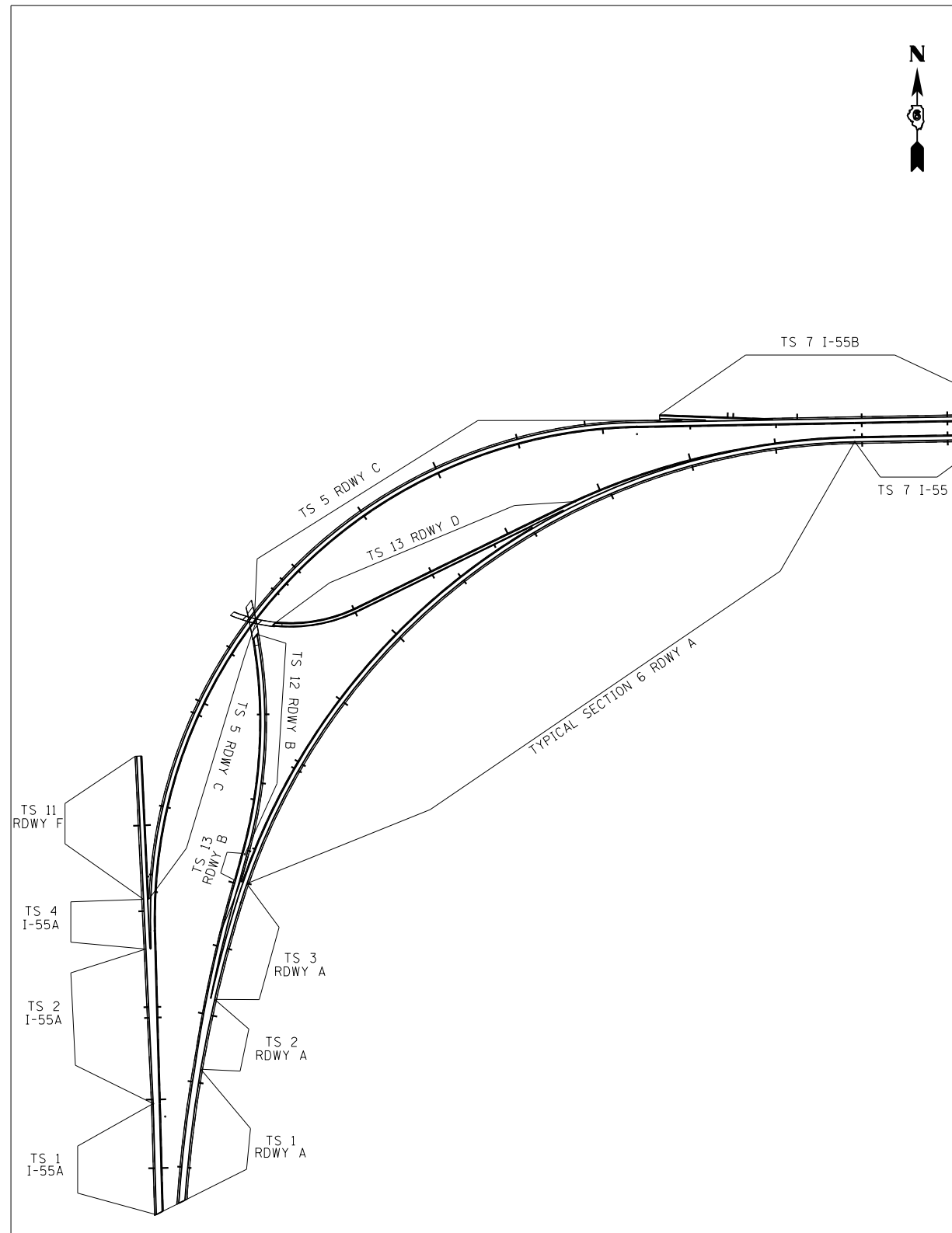
PAVEMENT MARKINGS

NOTE 1: 10' SKIP @ 40' OFFSET
 NOTE 2: 3' SKIP @ 12' OFFSET
 NOTE 3: CENTERLINE 4" - 4" @ 40' OFFSET
 NOTE 4: EDGE LINE: 4" - 4" @ 100' OFFSET

LOCATION	LENGTH	AREA	70300100		70301000	78004200	78004220	78004240	78004250	78004280		78009005		78009000	78009008	78009024	7830072
			SHORT TERM PAVT MARKING CENTERLINE WHITE (NOTE 3) 2 APP. FOOT	SHORT TERM PAVT MARKING CENTERLINE YELLOW (NOTE 4) 1 APP. FOOT	WORK ZONE PAVT MARK REM 1 APP. SQ FT	PRE PLAST PAVT MARK, TY B - INLAID - LETTERS & SYMBOLS WHITE SQ FT	PRE PLAST PAVT MARK, TY B - INLAID - LINE 5" WHITE (NOTE 1 & 2) FOOT	PRE PLAST PAVT MARK, TY B - INLAID - LINE 8" WHITE FOOT	PRE PLAST PAVT MARK, TY B - INLAID - LINE 12" WHITE FOOT	PRE PLAST PAVT MARK, TY B - INLAID - LINE 24" WHITE FOOT	PRE PLAST PAVT MARK, TY B - INLAID - LINE 24" YELLOW FOOT	MOD URETH PAVT MARK - LINE 5" YELLOW MEDIAN FOOT	MOD URETH PAVT MARK - LINE 5" WHITE OUTSIDE FOOT	MOD URETH PAVT MARK- LETTERS & SYMBOLS WHITE SQ FT	MOD URETH PAVT MARK- LINE 8" WHITE FOOT	MOD URETH PAVT MARK- LINE 24" WHITE FOOT	GROOVE FOR RECESSED PAVT MARK 6" FOOT
RAMP D (R104)																	
STA 00+00.00 TO STA 06+48.91	648.91			25.96	8.65								648.91				648.91
STA 06+48.91 TO STA 23+61.23	1,712.32			68.49	22.83						1,712.32	1,712.32					3,424.64
AROUND ISLAND	61.00											61.00					61.00
STA 00+00.00 TO STA 02+72.41	272.41					68.10											
STA 02+72.41 TO STA 06+48.91	376.50		753.00		125.50		753.00										
GORE CHEVERONS																	
STA 20+57		24.33				24.33			188.95								
STA 21+82		24.33				24.33											
STA 22+87	20.50									20.50							
STA 23+00	18.50									18.50							
STA 22+65		15.6				15.60											
STA 22+67		15.6				15.60											
ATLANTA INTERCHANGE																	
RAMP A (R201)																	
STA 00+00.00 TO STA 06+98.46	698.46			27.94	9.31							698.46					698.46
STA 06+98.46 TO STA 24+19.00	1,720.54			68.82	22.94						1,720.54	1,720.54					3,441.08
AROUND ISLAND	157.98													157.98			
STA 00+00.00 TO STA 03+56.74	356.74					89.19											
STA 03+56.74 TO STA 06+98.46	341.72		683.44		113.91		683.44										
GORE CHEVERONS																	
STA 21+44		24.33				24.33											
STA 22+70		24.33				24.33											
STA 23+74	18.00															18.00	
STA 23+82	18.00															18.00	
STA 23+54		15.6										15.60					
STA 23+48		15.6										15.60					
RAMP B (R202)																	
STA 00+00.00 TO STA 07+99.51	799.51			31.98	10.66						799.51	799.51					1,599.02
STA 07+99.51 TO STA 19+73.26	1,173.75			46.95	15.65							1,173.75					1,173.75
STA 07+99.51 TO STA 11+74.02	374.51		749.02		124.84		749.02										
STA 11+74.02 TO STA 19+73.26	799.24					199.81											
STA 1+34		24.33				24.33											
RAMP C (R203)																	
STA 00+00.00 TO STA 06+68.64	668.64			26.75	8.92							668.64					668.64
STA 06+68.64 TO STA 24+86.90	1,818.26			72.73	24.24						1,818.26	1,818.26					3,636.52
AROUND ISLAND	141.95													141.95			
STA 00+00.00 TO STA 02+96.28	296.28					74.07											
STA 02+96.28 TO STA 06+68.64	372.36		744.72		124.12		744.72										
GORE CHEVERONS																	
STA 22+14		24.33				24.33											
STA 23+39		24.33				24.33											
STA 24+50	18.00															18.00	
STA 24+48	18.00															18.00	
STA 24+22		15.6										15.60					
STA 24+19		15.6										15.60					
RAMP D (R204)																	
STA 00+00.00 TO STA 08+86.61	886.61			35.46	11.82						886.61	886.61					1,773.22
STA 08+86.61 TO STA 20+32.06	1,145.45			45.82	15.27							1,145.45					1,145.45
STA 08+86.61 TO STA 11+83.53	296.92		593.84		98.97		593.84										
STA 11+83.53 TO STA 20+32.06	848.53					212.13											
STA 1+55		24.33				24.33											
SUBTOTAL			49,954.07	7,933.88	10,970.31	354.36	41,998.15	12,294.72	642.91	1,329.00	264.00	178,169.80	181,437.26	62.40	299.93	72.00	347,318.33
TOTAL:			57,888		10,970	354	41,998	12,295	643	1,593		359,607		62	300	72	347,318

FILE NAME =	USER NAME = shahdm	DESIGNED - DMS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE:	SHEET 16	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 72041		
	PLOT DATE = Jul-18-2014 12:24:54PM	DATE -	REVISED -										

TYPICAL SECTION LOCATION MAP



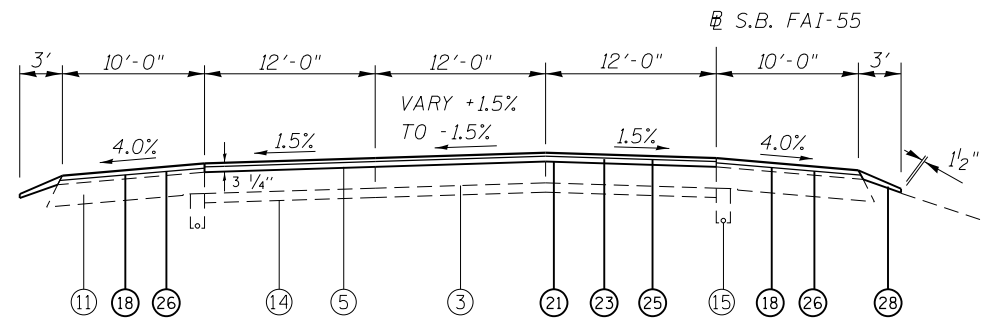
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. ROUTE 55
TYPICAL SECTIONS

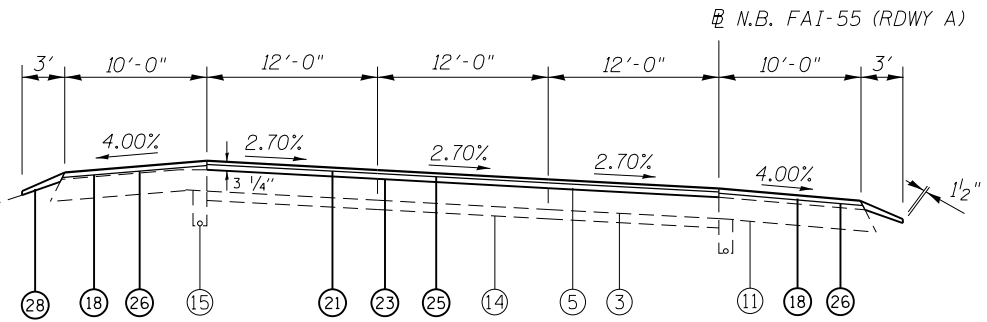
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	**	LOGAN	108	25
			CONTRACT NO.	72D41

SCALE: SHEET 1 OF SHEETS STA. TO STA.

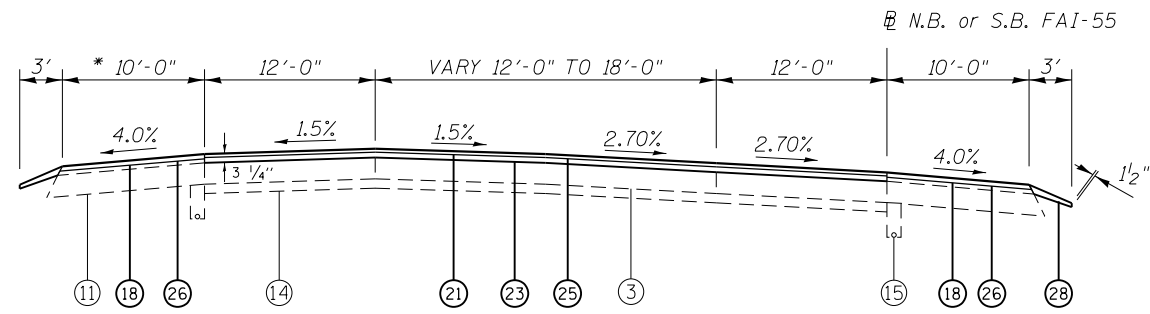


'I-55A SB' STA 379+87.59 TO STA 384+97.44
 STA EQN: 'I-55A SB' STA 384+97.44 BK = STA 385+00.00 AH
 'I-55A SB' STA 385+00.00 TO STA 385+77.02
 STA EQN: 'I-55A SB' STA 385+77.02 BK = 'RDWY F' STA 58+24.11 AH
 TS 1
 I-55A

TYPICAL #1



'RDWY A' STA 380+57.32 TO STA 387+79.45
 TS 1 RDWY-A



'RDWY F' STA 58+24.11 TO STA 49+20.77 TS 2 I-55A
 * 8' HMA SHLD FOR 'RDWY A' STA 387+79.45 TO STA 391+94.60 TS 2 RDWY-A

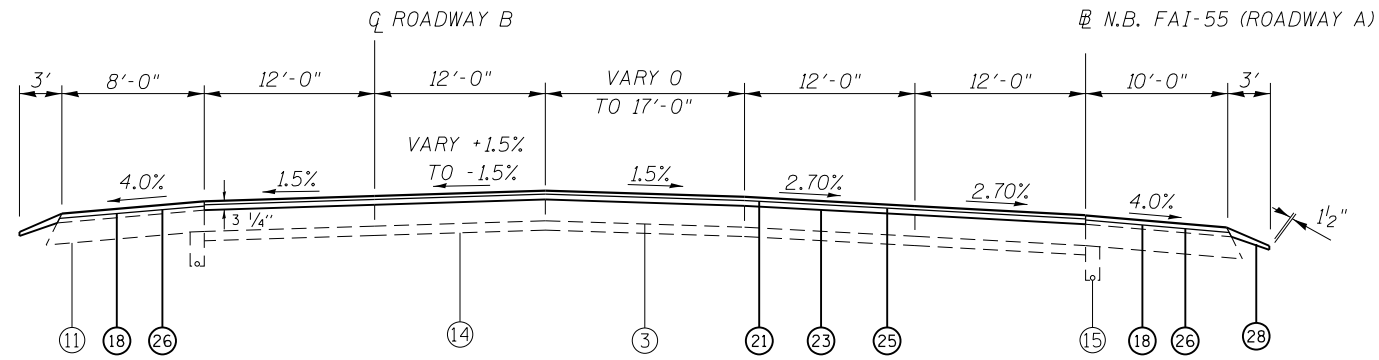
TYPICAL #2

LEGEND

- ① EX PCC CONC PAVEMENT - 8"
- ② EX CRC PAVEMENT - 8"
- ③ EX CRC PAVEMENT - 9"
- ④ EX CRC PAVEMENT - 11"
- ⑤ EX HOT MIX ASPHALT
- ⑥ EX SUBBASE GRANULAR MATERIAL, TY B - 12"
- ⑦ EX HOT MIX ASPHALT PAVEMENT - 3/4"
- ⑧ EX HOT MIX ASPHALT PAVEMENT - 2 1/2"
- ⑨ EX HOT MIX ASPHALT SHOULDER, VARIABLE DEPTH
- ⑩ EX HOT MIX ASPHALT SHOULDER - 8"
- ⑪ EX HOT MIX ASPHALT SHOULDER - 9"
- ⑫ EX HOT MIX ASPHALT SHOULDER - 12 1/4"
- ⑬ EX AGGREGATE SHOULDER
- ⑭ EX STABILIZED SUBBASE - 4"
- ⑮ EX PIPE UNDERDRAINS
- ⑯ EX PAVEMENT (BURIED)
- ⑰ PR HOT MIX ASPHALT SURFACE REMOVAL, 3/4"
- ⑱ PR HOT MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ⑲ PR HOT MIX ASPHALT SURFACE REMOVAL, 2"
- ⑳ PR HOT MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ㉑ PR HOT MIX ASPHALT SURFACE REMOVAL, 3 1/4"
- ㉒ PR HOT MIX ASPHALT SURFACE REMOVAL, 4 3/4"
- ㉓ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (2 1/4")
- ㉔ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (3 1/4")
- ㉕ PR POLY HOT MIX ASPHALT SURFACE COURSE, MIX "E", N90, (1 1/2")
- ㉖ PR HOT MIX ASPHALT SHOULDERS (MIX "C", N50), (2")
- ㉗ PR HOT MIX ASPHALT SHOULDERS, 8"
- ㉘ PR AGGREGATE WEDGE SHOULDER, TYPE B
- ㉙ PR PIPE UNDERDRAINS, 4"
- ㉚ PR PIPE UNDERDRAINS, 4" (MODIFIED)

NOTES:

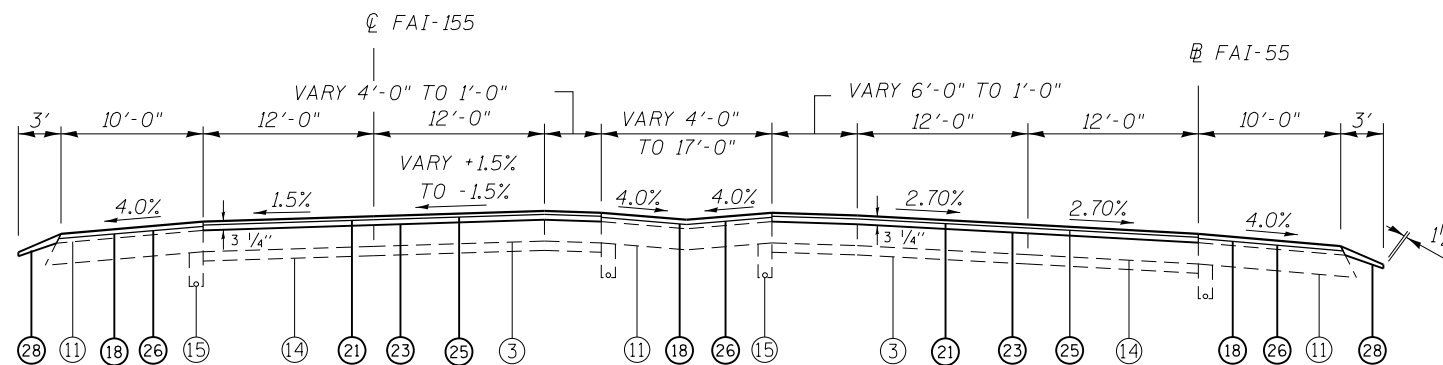
1. SEE "SUPERELEVATION TRANSITION DETAILS" FOR S.E. TABLE.
2. LOW SIDE SHOULD BE 4.0% WHEN SE OF PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SLOPE SHALL MATCH SE. HIGH SIDE SHOULDER SHALL BE SLOPED AT 4.0% WHEN PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SHALL BE SLOPED SO THE ALGEBRAIC DIFFERENCE DOES NOT EXCEED 8.0%.
3. CONTINUOUS RUMBLE STRIPS, AS DETAILED ON STANDARD 642001, SHALL BE CONSTRUCTED ON ALL MAINLINE SHOULDERS EXCEPT WHERE NOTED ON STANDARD AND MEDIAN CROSS OVER DETAIL.



'RDWY A' STA 391+94.60 TO STA 398+95.73
 TS 3 RDWY-A

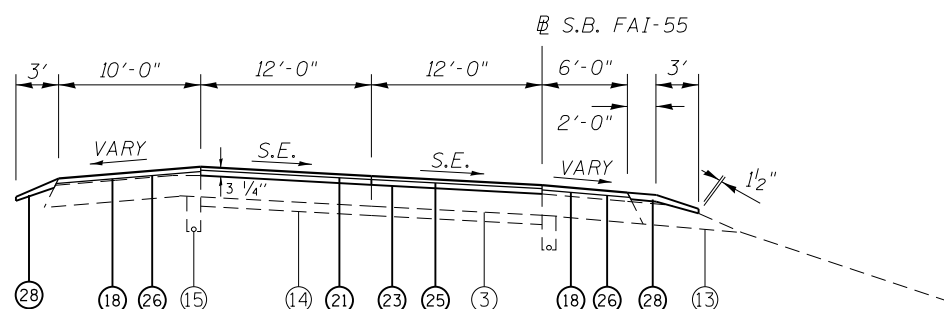
TYPICAL #3

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 TYPICAL SECTIONS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	Plot Scale = 100.0000' / in.	DRAWN -	REVISED -					LOGAN	108	26	
	PLOT DATE = Jul-14-2014 03:36:50PM	CHECKED -	REVISED -			SCALE:	SHEET 2 OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 72D41
		DATE -	REVISED -			•• (54-4,54-5)RS-3, (54-6)RS-2 ••					



TYPICAL #4

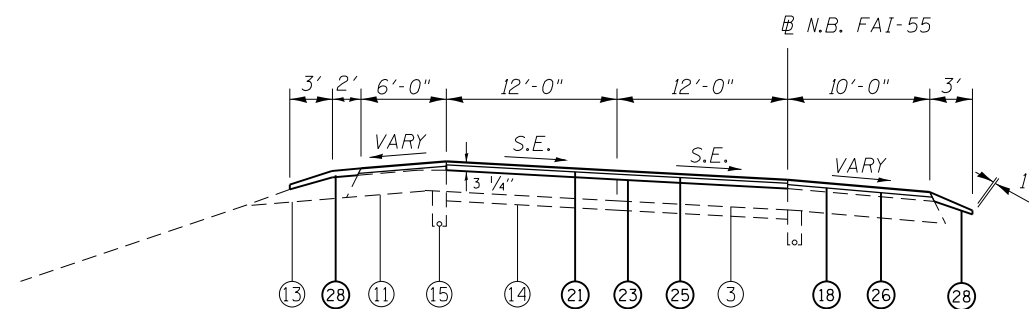
'RDWY F' STA 49+20.77 TO 46+30.88 TS 4 I-55A



TYPICAL #5

'RDWY C' STA 43+41.63 TO STA 0+00.00
 STA EQN: 'RDWY C' STA 0+00.00 BK = STA 452+88.82 AH
 'I-55 SB' STA 452+88.82 TO STA 454+21.83

TS 5
RDWY-C



TYPICAL #6

'RDWY A' STA 398+95.73 TO STA 445+62.28 TS 6 RDWY-A
 STA EQN: 'RDWY A' STA 445+62.28 = 'I-55 NB' STA 465+57.74

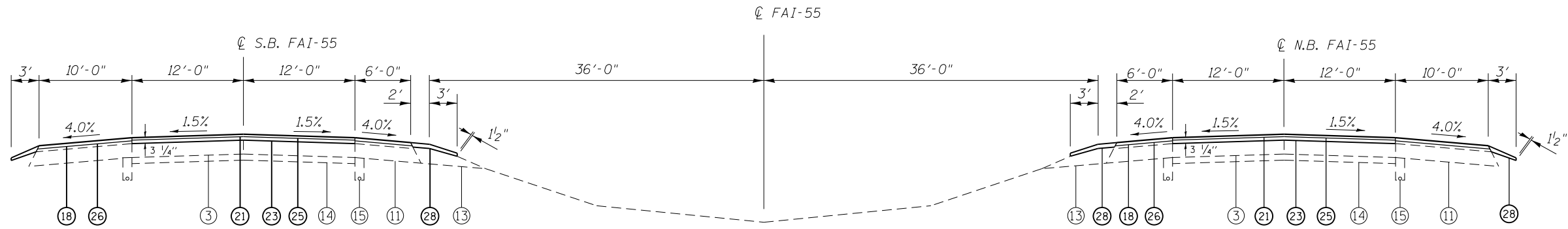
NOTES:

- SEE "SUPERELEVATION TRANSITION DETAILS" FOR S.E. TABLE.
- LOW SIDE SHOULD BE 4.0% WHEN SE OF PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SLOPE SHALL MATCH SE. HIGH SIDE SHOULDER SHALL BE SLOPED AT 4.0% WHEN PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SHALL BE SLOPED SO THE ALGEBRAIC DIFFERENCE DOES NOT EXCEED 8.0%.
- CONTINUOUS RUMBLE STRIPS, AS DETAILED ON STANDARD 642001, SHALL BE CONSTRUCTED ON ALL MAINLINE SHOULDERS EXCEPT WHERE NOTED ON STANDARD AND MEDIAN CROSS OVER DETAIL.

LEGEND

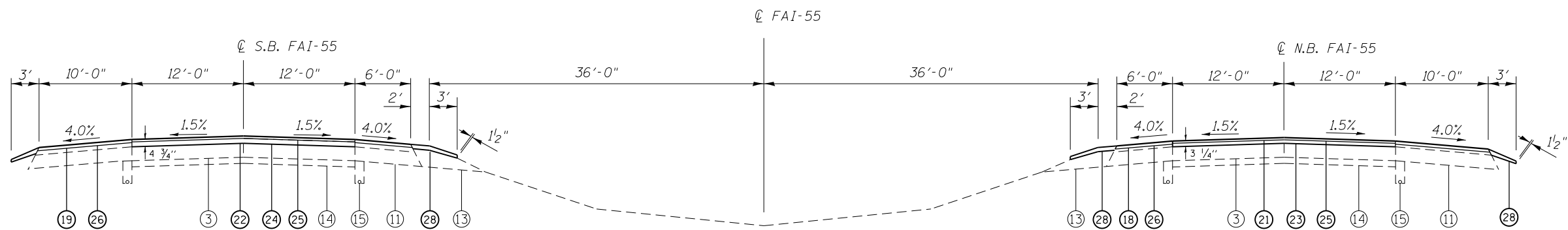
- | | |
|---|---|
| ① EX PCC CONC PAVEMENT - 8" | ⑪ PR HOT MIX ASPHALT SHOULDERS - 9" |
| ② EX CRC PAVEMENT - 8" | ⑫ EX HOT MIX ASPHALT SHOULDER - 12 1/4" |
| ③ EX CRC PAVEMENT - 9" | ⑬ EX AGGREGATE SHOULDER |
| ④ EX CRC PAVEMENT - 11" | ⑭ EX STABILIZED SUBBABSSE - 4" |
| ⑤ EX HOT MIX ASPHALT | ⑮ EX PIPE UNDERDRAINS |
| ⑥ EX SUBBASE GRANULAR MATERIAL, TY B - 12" | ⑯ PR HOT MIX ASPHALT SURFACE REMOVAL, 3/4" |
| ⑦ EX HOT MIX ASPHALT PAVEMENT - 3 1/4" | ⑰ PR HOT MIX ASPHALT SURFACE REMOVAL, 1 1/2" |
| ⑧ EX HOT MIX ASPHALT PAVEMENT - 2 1/2" | ⑱ PR HOT MIX ASPHALT SURFACE REMOVAL, 2" |
| ⑨ EX HOT MIX ASPHALT SHOULDER, VARIABLE DEPTH | ⑲ PR HOT MIX ASPHALT SURFACE REMOVAL, 2" |
| ⑩ EX HOT MIX ASPHALT SHOULDER - 8" | ⑳ PR HOT MIX ASPHALT SURFACE REMOVAL, 2 1/2" |
| | ㉑ PR HOT MIX ASPHALT SURFACE REMOVAL, 3 1/4" |
| | ㉒ PR HOT MIX ASPHALT SURFACE REMOVAL, 4 3/4" |
| | ㉓ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (2 1/4") |
| | ㉔ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (3 1/4") |
| | ㉕ PR POLY HOT MIX ASPHALT SURFACE COURSE, MIX "E", N90, (1 1/2") |
| | ㉖ PR HOT MIX ASPHALT SHOULDERS (MIX "C", N50), (2") |
| | ㉗ PR HOT MIX ASPHALT SHOULDERS, 8" |
| | ㉘ PR AGGREGATE WEDGE SHOULDER, TYPE B |
| | ㉙ PR PIPE UNDERDRAINS, 4" |
| | ㉚ PR PIPE UNDERDRAINS, 4" (MODIFIED) |

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 TYPICAL SECTIONS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
Default	Plot Date = Jul-14-2014 03:37:02PM	DRAWN -	REVISED -		SCALE:	SHEET 3	OF	SHEETS	STA.	TO	STA.	LOGAN	108	27
		CHECKED -	REVISED -								CONTRACT NO. 72D41			
		DATE -	REVISED -								ILLINOIS FED. AID PROJECT			



TYPICAL #7

'I-55 NB' STA 465+57.74 TO STA 533+29.96 TS 7 I-55
 OMISSION: 'I-55 NB' STA 533+29.96 TO STA 540+35.82
 'I-55 SB' STA 454+21.83 TO STA 534+84.73 TS 7 I-55B



TYPICAL #8

'I-55 SB' STA 541+90.61 TO STA 555+05.40
 'I-55 NB' STA 540+35.82 TO STA 554+83.64
 'I-55 SB' STA 562+55.63 TO STA 575+70.23
 'I-55 NB' STA 562+33.87 TO STA 575+70.23

SB & NB FAI-55
 STA 577+98.03 TO STA 622+14.86
 STA 577+98.03 TO STA 622+14.86
 STA 639+48.18 TO STA 675+43.92

'I-55 SB' STA 692+77.24 TO STA 697+52.85
 'I-55 NB' STA 692+77.24 TO STA 696+36.69
 'I-55 SB' STA 703+71.20 TO STA 708+43.00
 'I-55 NB' STA 703+25.92 TO STA 708+43.00

SB & NB FAI-55
 STA 743+07.07 TO STA 761+00.55
 STA 789+63.83 TO STA 790+97.12
 STA EQ: 790+97.12 '55' BK = 184+93.00 '55' AH

STA TO STA OMISSIONS FOR STRUCTURES
 'I-55 SB' STA 555+05.40 TO STA 562+55.63
 'I-55 NB' STA 554+83.64 TO STA 562+33.87
 'I-55 SB' STA 575+70.23 TO STA 577+98.03
 'I-55 NB' STA 575+70.23 TO STA 577+98.03
 'I-55 SB' STA 697+52.85 TO STA 703+71.20
 'I-55 NB' STA 696+36.69 TO STA 703+25.92

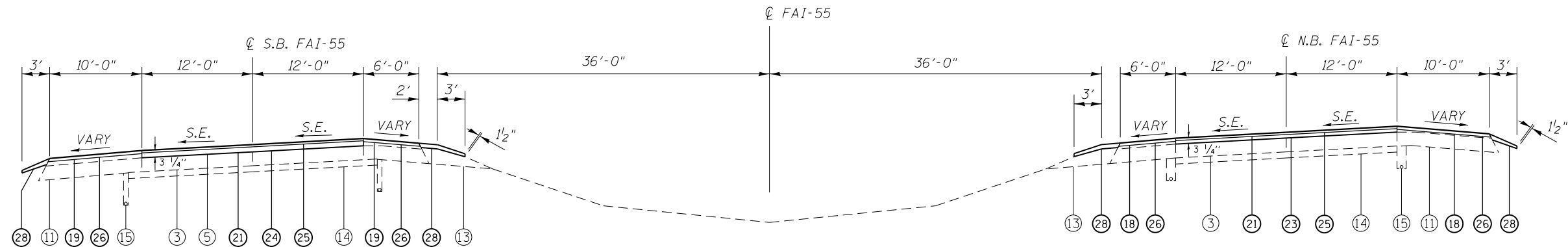
LEGEND

- | | | |
|---|---|---|
| ① EX PCC CONC PAVEMENT - 8" | ⑩ EX HOT MIX ASPHALT SHOULDER - 8" | ⑰ PR HOT MIX ASPHALT SURFACE REMOVAL, 3/4" |
| ② EX CRC PAVEMENT - 8" | ⑪ EX HOT MIX ASPHALT SHOULDER - 9" | ⑱ PR HOT MIX ASPHALT SURFACE REMOVAL, 2" |
| ③ EX CRC PAVEMENT - 9" | ⑫ EX HOT MIX ASPHALT SHOULDER - 12 1/4" | ⑲ PR HOT MIX ASPHALT SURFACE REMOVAL, 2" |
| ④ EX CRC PAVEMENT - 11" | ⑬ EX AGGREGATE SHOULDER | ⑳ PR HOT MIX ASPHALT SURFACE REMOVAL, 2 1/2" |
| ⑤ EX HOT MIX ASPHALT | ⑭ EX STABILIZED SUBBASE - 4" | ㉑ PR HOT MIX ASPHALT SURFACE REMOVAL, 3 1/4" |
| ⑥ EX SUBBASE GRANULAR MATERIAL, TY B - 12" | ⑮ EX PIPE UNDERDRAINS | ㉒ PR HOT MIX ASPHALT SURFACE REMOVAL, 4 3/4" |
| ⑦ EX HOT MIX ASPHALT PAVEMENT - 3 1/4" | ⑯ EX PAVEMENT (BURIED) | ㉓ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (2 1/4") |
| ⑧ EX HOT MIX ASPHALT PAVEMENT - 2 1/2" | | ㉔ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (3 1/4") |
| ⑨ EX HOT MIX ASPHALT SHOULDER, VARIABLE DEPTH | | ㉕ PR POLY HOT MIX ASPHALT SURFACE COURSE, MIX "E", N90, (1 1/2") |

NOTES:

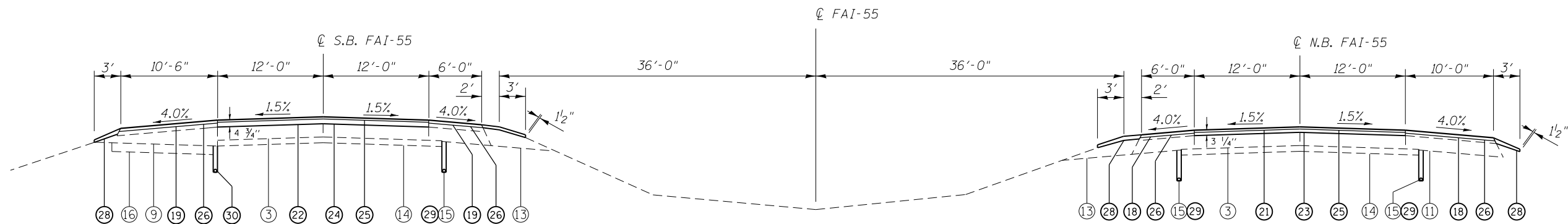
- SEE "SUPERELEVATION TRANSITION DETAILS" FOR S.E. TABLE.
- LOW SIDE SHOULD BE 4.0% WHEN SE OF PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SLOPE SHALL MATCH SE. HIGH SIDE SHOULDER SHALL BE SLOPED AT 4.0% WHEN PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SHALL BE SLOPED SO THE ALGEBRAIC DIFFERENCE DOES NOT EXCEED 8.0%.
- CONTINUOUS RUMBLE STRIPS, AS DETAILED ON STANDARD 642001, SHALL BE CONSTRUCTED ON ALL MAINLINE SHOULDERS EXCEPT WHERE NOTED ON STANDARD AND MEDIAN CROSS OVER DETAIL.

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 TYPICAL SECTIONS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
Default	241-sht-typical.dgn	DRAWN -	REVISED -		SCALE:	SHEET 4	OF	SHEETS	STA.	TO	STA.	LOGAN	108	28
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -		CONTRACT NO. 72D41									
	PLOT DATE = Jul-14-2014 03:37:02PM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT									



TYPICAL #9

S.B. & N.B. FAI-55
 STA 622+14.86 TO STA. 639+48.18 (S.E. LT. 2.70%)
 STA 675+43.92 TO STA. 692+77.24 (S.E. RT. 2.70%)
 STA 708+43.00 TO STA. 743+07.77 (S.E. LT. 5.20%)
 STA 761+00.55 TO STA. 789+63.83 (S.E. RT. 2.70%)



TYPICAL #10

S.B. & N.B. FAI-55
 STA 184+93.00 TO STA 190+00.00

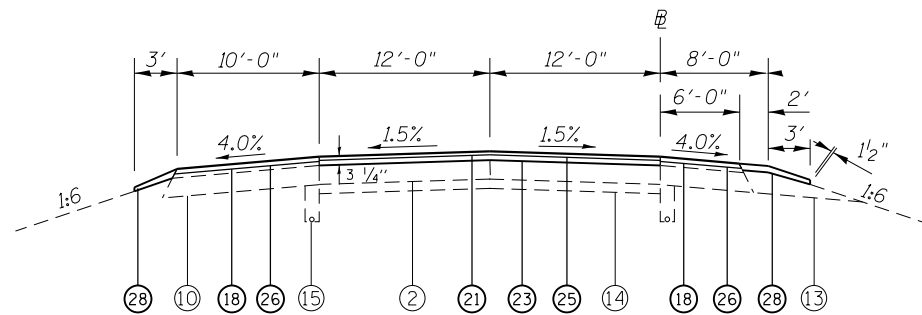
NOTES:

- SEE "SUPERELEVATION TRANSITION DETAILS" FOR S.E. TABLE.
- LOW SIDE SHOULD BE 4.0% WHEN SE OF PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SLOPE SHALL MATCH SE. HIGH SIDE SHOULD BE SLOPED AT 4.0% WHEN PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SHALL BE SLOPED SO THE ALGEBRAIC DIFFERENCE DOES NOT EXCEED 8.0%.
- CONTINUOUS RUMBLE STRIPS, AS DETAILED ON STANDARD 642001, SHALL BE CONSTRUCTED ON ALL MAINLINE SHOULDERS EXCEPT WHERE NOTED ON STANDARD AND MEDIAN CROSS OVER DETAIL.

LEGEND

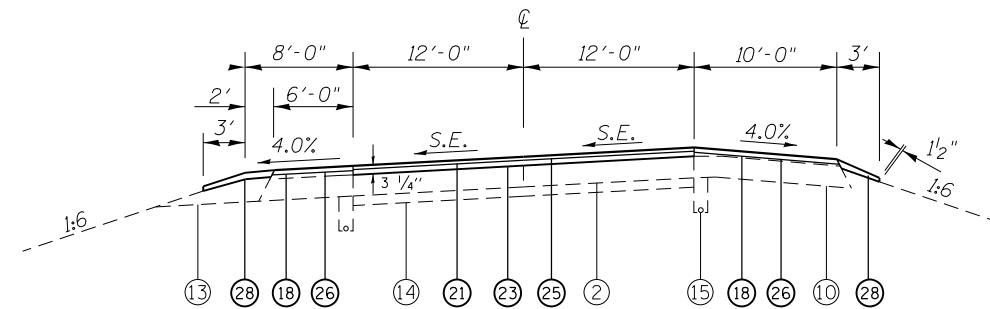
- | | |
|---|--|
| ① EX PCC CONC PAVEMENT - 8" | ①⑦ PR HOT MIX ASPHALT SURFACE REMOVAL, 3/4" |
| ② EX CRC PAVEMENT - 8" | ①⑧ PR HOT MIX ASPHALT SURFACE REMOVAL, 1 1/2" |
| ③ EX CRC PAVEMENT - 9" | ①⑨ PR HOT MIX ASPHALT SURFACE REMOVAL, 2" |
| ④ EX CRC PAVEMENT - 11" | ①⑩ PR HOT MIX ASPHALT SURFACE REMOVAL, 2 1/2" |
| ⑤ EX HOT MIX ASPHALT | ①⑪ PR HOT MIX ASPHALT SURFACE REMOVAL, 3 1/4" |
| ⑥ EX SUBBASE GRANULAR MATERIAL, TY B - 12" | ①⑫ PR HOT MIX ASPHALT SURFACE REMOVAL, 4 3/4" |
| ⑦ EX HOT MIX ASPHALT PAVEMENT - 3 1/4" | ①⑬ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (2 1/4") |
| ⑧ EX HOT MIX ASPHALT PAVEMENT - 2 1/2" | ①⑭ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (3 1/4") |
| ⑨ EX HOT MIX ASPHALT SHOULDER, VARIABLE DEPTH | ①⑮ PR POLY HOT MIX ASPHALT SURFACE COURSE, MIX "E", N90, (1 1/2") |
| ⑩ EX HOT MIX ASPHALT SHOULDER - 8" | ①⑯ PR HOT MIX ASPHALT SHOULDERS (MIX "C", N50), (2") |
| ⑪ EX HOT MIX ASPHALT SHOULDER - 9" | ①⑰ PR HOT MIX ASPHALT SHOULDERS, 8" |
| ⑫ EX HOT MIX ASPHALT SHOULDER - 12 1/4" | ①⑱ PR AGGREGATE WEDGE SHOULDER, TYPE B |
| ⑬ EX AGGREGATE SHOULDER | ①⑲ PR PIPE UNDERDRAINS, 4" |
| ⑭ EX STABILIZED SUBBASE - 4" | ①⑳ PR PIPE UNDERDRAINS, 4" (MODIFIED) |
| ⑮ EX PIPE UNDERDRAINS | |
| ⑯ EX PAVEMENT (BURIED) | |

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 TYPICAL SECTIONS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	2041-sht-typical.dgn	DRAWN -	REVISED -					LOGAN	108	29	
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -			SCALE:	SHEET 5	OF SHEETS	STA.	TO STA.	CONTRACT NO. 72D41
	PLOT DATE = Jul-14-2014 03:37:03PM	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



TYPICAL #11

'RDWY F'
STA 46+30.87 TO STA 37+95.69 TS 12 RDWY-F



TYPICAL #12

'RDWY B'
STA 2+75.00 TO STA 15+56.96 TS 14 RDWY-B

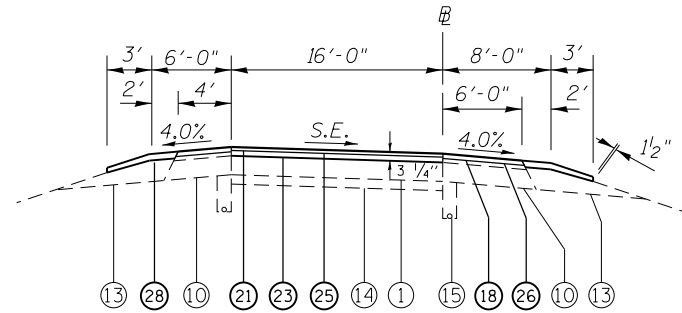
LEGEND

- | | | |
|---|---|---|
| ① EX PCC CONC PAVEMENT - 8" | ⑩ EX HOT MIX ASPHALT SHOULDER - 8" | ⑰ PR HOT MIX ASPHALT SURFACE REMOVAL, 3/4" |
| ② EX CRC PAVEMENT - 8" | ⑪ EX HOT MIX ASPHALT SHOULDER - 9" | ⑱ PR HOT MIX ASPHALT SURFACE REMOVAL, 1 1/2" |
| ③ EX CRC PAVEMENT - 9" | ⑫ EX HOT MIX ASPHALT SHOULDER - 12 1/4" | ⑲ PR HOT MIX ASPHALT SURFACE REMOVAL, 2" |
| ④ EX CRC PAVEMENT - 11" | ⑬ EX AGGREGATE SHOULDER | ⑳ PR HOT MIX ASPHALT SURFACE REMOVAL, 2 1/2" |
| ⑤ EX HOT MIX ASPHALT | ⑭ EX STABILIZED SUBBASE - 4" | ㉑ PR HOT MIX ASPHALT SURFACE REMOVAL, 3 1/4" |
| ⑥ EX SUBBASE GRANULAR MATERIAL, TY B - 12" | ⑮ EX PIPE UNDERDRAINS | ㉒ PR HOT MIX ASPHALT SURFACE REMOVAL, 4 3/4" |
| ⑦ EX HOT MIX ASPHALT PAVEMENT - 3 1/4" | ⑯ EX PAVEMENT (BURIED) | ㉓ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (2 1/4") |
| ⑧ EX HOT MIX ASPHALT PAVEMENT - 2 1/2" | | ㉔ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (3 1/4") |
| ⑨ EX HOT MIX ASPHALT SHOULDER, VARIABLE DEPTH | | ㉕ PR POLY HOT MIX ASPHALT SURFACE COURSE, MIX "E", N90, (1 1/2") |
| | | ㉖ PR HOT MIX ASPHALT SHOULDERS (MIX "C", N50), (2") |
| | | ㉗ PR HOT MIX ASPHALT SHOULDERS, 8" |
| | | ㉘ PR AGGREGATE WEDGE SHOULDER, TYPE B |
| | | ㉙ PR PIPE UNDERDRAINS, 4" |
| | | ㉚ PR PIPE UNDERDRAINS, 4" (MODIFIED) |

NOTES:

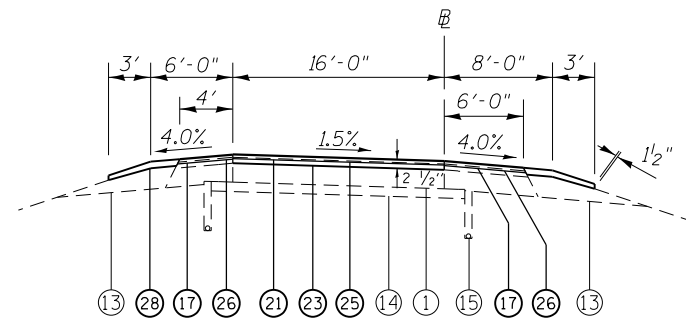
- MATCH EXISTING SUPERELEVATION SLOPES ON RAMPS.
- LOW SIDE SHOULD BE 4.0% WHEN SE OF PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SLOPE SHALL MATCH SE. HIGH SIDE SHOULDER SHALL BE SLOPED AT 4.0% WHEN PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SHALL BE SLOPED SO THE ALGEBRAIC DIFFERENCE DOES NOT EXCEED 8.0%.
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FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 TYPICAL SECTIONS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	Plot Scale = 100.0000' / 1in.	CHECKED -	REVISED -		SCALE:	SHEET 6	OF	SHEETS	STA.	TO STA.	LOGAN	108	30
	PLOT DATE = Jul-14-2014 03:37:03PM	DATE -	REVISED -					CONTRACT NO. 72D41			ILLINOIS FED. AID PROJECT		
											ILLINOIS FED. AID PROJECT		



TYPICAL #13

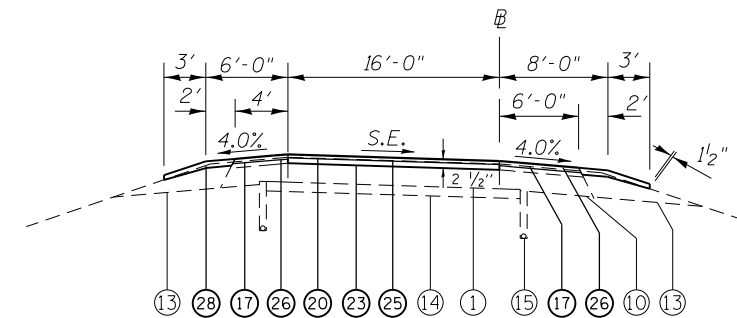
STA 18+21.66 'D' TO STA 37+35.58 'D'



TYPICAL #14

BUSINESS 55 INTERCHANGE

RAMP A
STA 1+25.30 TO STA 27+59.81
RAMP B
STA 6+16.65 TO STA 30+77.69
RAMP C
STA 1+25.16 TO STA 14+31.87
RAMP D
STA 6+48.91 TO STA 22+00.00



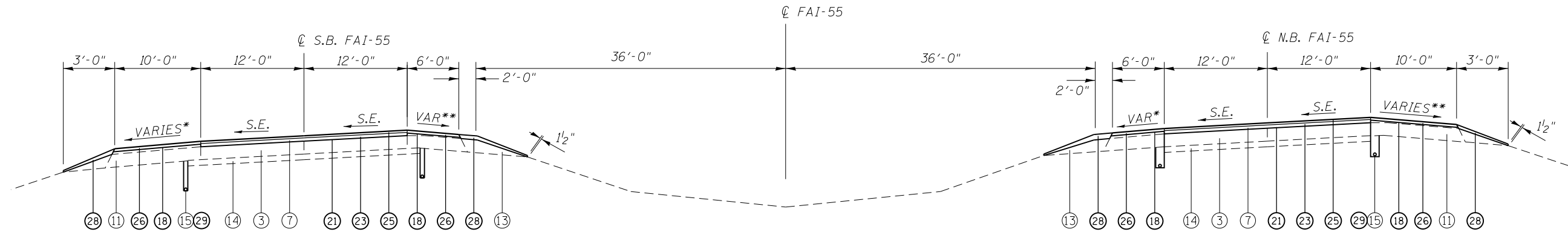
LEGEND

- | | |
|---|--|
| ① EX PCC CONC PAVEMENT - 8" | ①⑦ PR HOT MIX ASPHALT SURFACE REMOVAL, 3/4" |
| ② EX CRC PAVEMENT - 8" | ①⑧ PR HOT MIX ASPHALT SURFACE REMOVAL, 1 1/2" |
| ③ EX CRC PAVEMENT - 9" | ①⑨ PR HOT MIX ASPHALT SURFACE REMOVAL, 2" |
| ④ EX CRC PAVEMENT - 11" | ①⑩ PR HOT MIX ASPHALT SURFACE REMOVAL, 2 1/2" |
| ⑤ EX HOT MIX ASPHALT | ①⑪ PR HOT MIX ASPHALT SURFACE REMOVAL, 3 1/4" |
| ⑥ EX SUBBASE GRANULAR MATERIAL, TY B - 12" | ①⑫ PR HOT MIX ASPHALT SURFACE REMOVAL, 4 3/4" |
| ⑦ EX HOT MIX ASPHALT PAVEMENT - 3 1/4" | ①⑬ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (2 1/4") |
| ⑧ EX HOT MIX ASPHALT PAVEMENT - 2 1/2" | ①⑭ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (3 1/4") |
| ⑨ EX HOT MIX ASPHALT SHOULDER, VARIABLE DEPTH | ①⑮ PR POLY HOT MIX ASPHALT SURFACE COURSE, MIX "E", N90, (1 1/2") |
| ⑩ EX HOT MIX ASPHALT SHOULDER - 8" | ①⑯ PR HOT MIX ASPHALT SHOULDERS (MIX "C", N50), (2") |
| ⑪ EX HOT MIX ASPHALT SHOULDER - 9" | ①⑰ PR HOT MIX ASPHALT SHOULDERS, 8" |
| ⑫ EX HOT MIX ASPHALT SHOULDER - 12 1/4" | ①⑱ PR AGGREGATE WEDGE SHOULDER, TYPE B |
| ⑬ EX AGGREGATE SHOULDER | ①⑲ PR PIPE UNDERDRAINS, 4" |
| ⑭ EX STABILIZED SUBBASE - 4" | ①⑳ PR PIPE UNDERDRAINS, 4" (MODIFIED) |
| ⑮ EX PIPE UNDERDRAINS | |
| ⑯ EX PAVEMENT (BURIED) | |

NOTES:

- MATCH EXISTING SUPERELEVATION SLOPES ON RAMPS.
- LOW SIDE SHOULD BE 4.0% WHEN SE OF PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SLOPE SHALL MATCH SE. HIGH SIDE SHOULDER SHALL BE SLOPED AT 4.0% WHEN PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SHALL BE SLOPED SO THE ALGEBRAIC DIFFERENCE DOES NOT EXCEED 8.0%.
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FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 TYPICAL SECTIONS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ci:\pw\work\p\dot\sparksgw\0215133\0672041-sht-typical.dgn	PLOT SCALE = 100.0000' / 1"	DRAWN -	REVISED -			•	**	LOGAN	108	31	
Default	PLOT DATE = Jul-14-2014 03:37:03PM	CHECKED -	REVISED -			SCALE:		SHEET 7 OF SHEETS		STA.	TO STA.
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



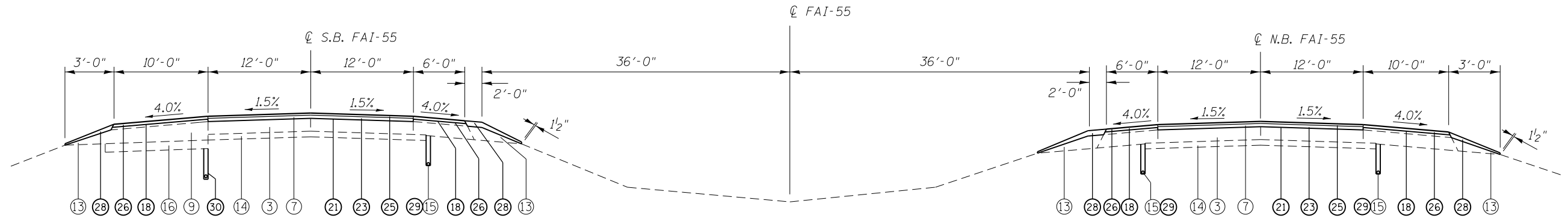
TYPICAL #15

S.B. & N.B. FAI-55

STA. 403+38.86 TO STA. 429+29.22 (S.E. RT. 4.2%)
 STA. 453+88.01 TO STA. 456+43.66 (S.E. LT. 4.2%)
 BRIDGE OMISSION 456+43.66 TO 457+63.74
 STA. 457+63.74 TO STA. 469+58.99 (S.E. LT. 4.2%)
 BRIDGE OMISSION 469+58.99 TO 471+16.93
 STA. 471+16.93 TO STA. 477+74.85 (S.E. LT. 4.2%)
 STA. 513+89.55 TO STA. 539+22.87 (S.E. LT. 4.2%)
 STA. 549+92.23 TO STA. 578+32.01 (S.E. RT. 4.2%)

SEE DETAIL A
FOR AGG. SHLDR

SEE DETAIL A
FOR AGG. SHLDR



TYPICAL #16

S.B. & N.B. FAI-55

STA. 190+00.00 TO STA. 280+81.08
 BRIDGE OMISSION 280+81.08 TO 283+97.57
 STA. 283+97.57 TO STA. 403+38.86
 STA. 578+32.01 TO STA. 590+00.00

SEE DETAIL A
FOR AGG. SHLDR

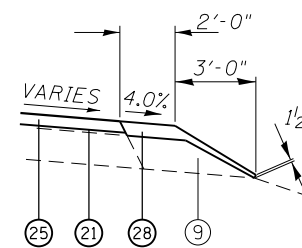
SEE DETAIL A
FOR AGG. SHLDR

LEGEND

- | | | |
|---|---|---|
| ① EX PCC CONC PAVEMENT - 8" | ⑩ EX HOT MIX ASPHALT SHOULDER - 8" | ⑰ PR HOT MIX ASPHALT SURFACE REMOVAL, 3/4" |
| ② EX CRC PAVEMENT - 8" | ⑪ EX HOT MIX ASPHALT SHOULDER - 9" | ⑱ PR HOT MIX ASPHALT SURFACE REMOVAL, 1 1/2" |
| ③ EX CRC PAVEMENT - 9" | ⑫ EX HOT MIX ASPHALT SHOULDER - 12 1/4" | ⑳ PR HOT MIX ASPHALT SURFACE REMOVAL, 2" |
| ④ EX CRC PAVEMENT - 11" | ⑬ EX AGGREGATE SHOULDER | ㉑ PR HOT MIX ASPHALT SURFACE REMOVAL, 2 1/2" |
| ⑤ EX HOT MIX ASPHALT | ⑭ EX STABILIZED SUBBASE - 4" | ㉒ PR HOT MIX ASPHALT SURFACE REMOVAL, 3 1/4" |
| ⑥ EX SUBBASE GRANULAR MATERIAL, TY B - 12" | ⑮ EX PIPE UNDERDRAINS | ㉓ PR HOT MIX ASPHALT SURFACE REMOVAL, 4 3/4" |
| ⑦ EX HOT MIX ASPHALT PAVEMENT - 3 1/4" | ⑯ EX PIPE UNDERDRAINS, 4" | ㉔ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (2 1/4") |
| ⑧ EX HOT MIX ASPHALT PAVEMENT - 2 1/2" | ⑰ EX PIPE UNDERDRAINS, 4" (MODIFIED) | ㉕ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (3 1/4") |
| ⑨ EX HOT MIX ASPHALT SHOULDER, VARIABLE DEPTH | | ㉖ PR POLY HOT MIX ASPHALT SURFACE COURSE, MIX "E", N90, (1 1/2") |

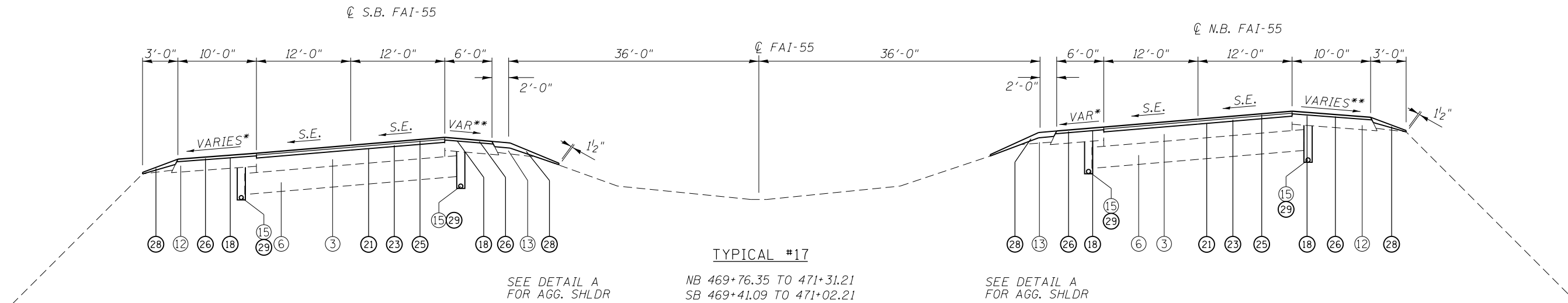
NOTES:

- SEE "SUPERELEVATION TRANSITION DETAILS" FOR S.E. TABLE.
- LOW SIDE SHOULD BE 4.0% WHEN SE OF PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SLOPE SHALL MATCH SE. HIGH SIDE SHOULDER SHALL BE SLOPED AT 4.0% WHEN PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SHALL BE SLOPED SO THE ALGEBRAIC DIFFERENCE DOES NOT EXCEED 8.0%.
- CONTINUOUS RUMBLE STRIPS, AS DETAILED ON STANDARD 642001, SHALL BE CONSTRUCTED ON ALL MAINLINE SHOULDERS EXCEPT WHERE NOTED ON STANDARD AND MEDIAN CROSS OVER DETAIL.



DETAIL A
TYP. INSIDE AGG. SHLDR

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 TYPICAL SECTIONS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
Default	241-sht-typical.dgn	DRAWN -	REVISED -		SCALE:	SHEET 8	OF	SHEETS	STA.	TO	STA.	LOGAN	108	32
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -		CONTRACT NO. 72D41									
	PLOT DATE = Jul-14-2014 03:37:03PM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT									

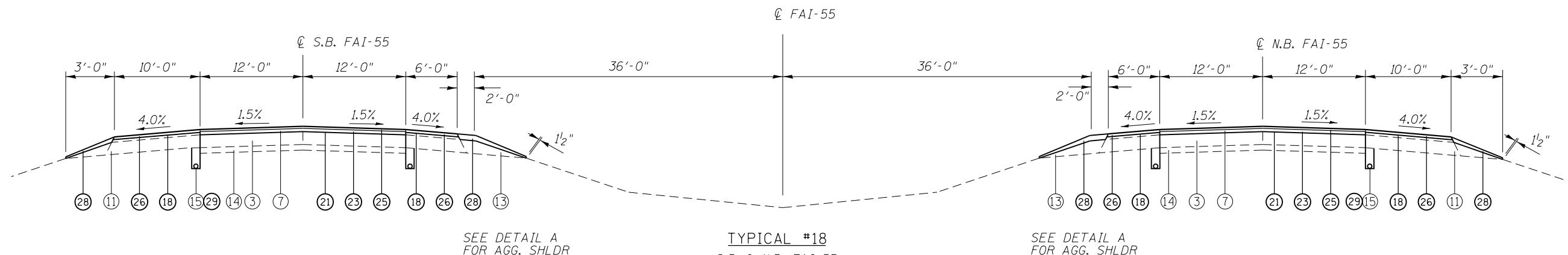


TYPICAL #17

NB 469+76.35 TO 471+31.21
SB 469+41.09 TO 471+02.21

SEE DETAIL A FOR AGG. SHLDR

SEE DETAIL A FOR AGG. SHLDR



TYPICAL #18

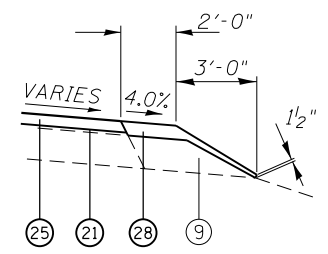
S.B. & N.B. FAI-55
STA. 429+92.22 TO STA. 453+88.01
STA. 477+74.85 TO STA. 513+89.55
STA. 539+22.87 TO STA. 549+92.23

SEE DETAIL A FOR AGG. SHLDR

SEE DETAIL A FOR AGG. SHLDR

NOTES:

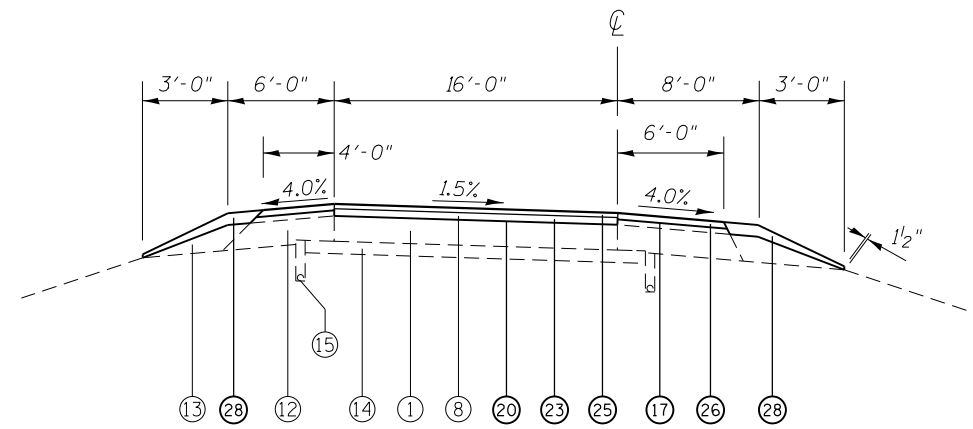
- SEE "SUPERELEVATION TRANSITION DETAILS" FOR S.E. TABLE.
- LOW SIDE SHOULD BE 4.0% WHEN SE OF PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SLOPE SHALL MATCH SE. HIGH SIDE SHOULDER SHALL BE SLOPED AT 4.0% WHEN PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SHALL BE SLOPED SO THE ALGEBRAIC DIFFERENCE DOES NOT EXCEED 8.0%.
- CONTINUOUS RUMBLE STRIPS, AS DETAILED ON STANDARD 642001, SHALL BE CONSTRUCTED ON ALL MAINLINE SHOULDERS EXCEPT WHERE NOTED ON STANDARD AND MEDIAN CROSS OVER DETAIL.



DETAIL A
TYP. INSIDE AGG. SHLDR

LEGEND

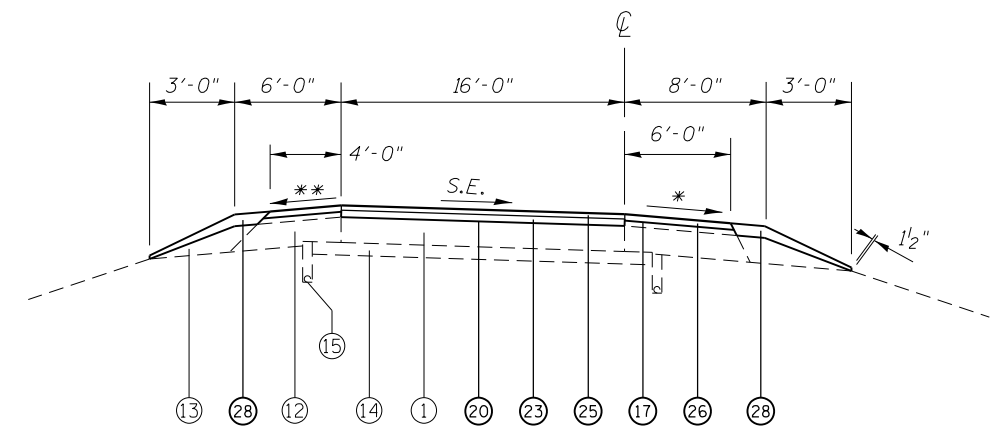
- | | |
|---|--|
| ① EX PCC CONC PAVEMENT - 8" | ①⑦ PR HOT MIX ASPHALT SURFACE REMOVAL, 3/4" |
| ② EX CRC PAVEMENT - 8" | ①⑧ PR HOT MIX ASPHALT SURFACE REMOVAL, 1/2" |
| ③ EX CRC PAVEMENT - 9" | ①⑨ PR HOT MIX ASPHALT SURFACE REMOVAL, 2" |
| ④ EX CRC PAVEMENT - 11" | ①⑩ PR HOT MIX ASPHALT SURFACE REMOVAL, 2 1/2" |
| ⑤ EX HOT MIX ASPHALT | ①⑪ PR HOT MIX ASPHALT SURFACE REMOVAL, 3 1/4" |
| ⑥ EX SUBBASE GRANULAR MATERIAL, TY B - 12" | ①⑫ PR HOT MIX ASPHALT SURFACE REMOVAL, 4 3/4" |
| ⑦ EX HOT MIX ASPHALT PAVEMENT - 3 1/4" | ①⑬ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (2 1/4") |
| ⑧ EX HOT MIX ASPHALT PAVEMENT - 2 1/2" | ①⑭ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (3 1/4") |
| ⑨ EX HOT MIX ASPHALT SHOULDER, VARIABLE DEPTH | ①⑮ PR POLY HOT MIX ASPHALT SURFACE COURSE, MIX "E", N90, (1 1/2") |
| ⑩ EX HOT MIX ASPHALT SHOULDER - 8" | ①⑯ PR HOT MIX ASPHALT SHOULDERS (MIX "C", N50), (2") |
| ⑪ EX HOT MIX ASPHALT SHOULDER - 9" | ①⑰ PR HOT MIX ASPHALT SHOULDERS, 8" |
| ⑫ EX HOT MIX ASPHALT SHOULDER - 12 1/4" | ①⑱ PR AGGREGATE WEDGE SHOULDER, TYPE B |
| ⑬ EX AGGREGATE SHOULDER | ①⑲ PR PIPE UNDERDRAINS, 4" |
| ⑭ EX STABILIZED SUBBASE - 4" | ①⑳ PR PIPE UNDERDRAINS, 4" (MODIFIED) |
| ⑮ EX PIPE UNDERDRAINS | |
| ⑯ EX PAVEMENT (BURIED) | |



SEE DETAIL A
FOR AGG. SHLDR

TYPICAL #19

RAMP (TANGENT)
ATLANTA INTERCHANGE



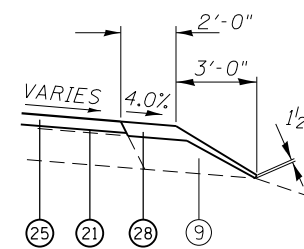
SEE DETAIL A
FOR AGG. SHLDR

TYPICAL #20

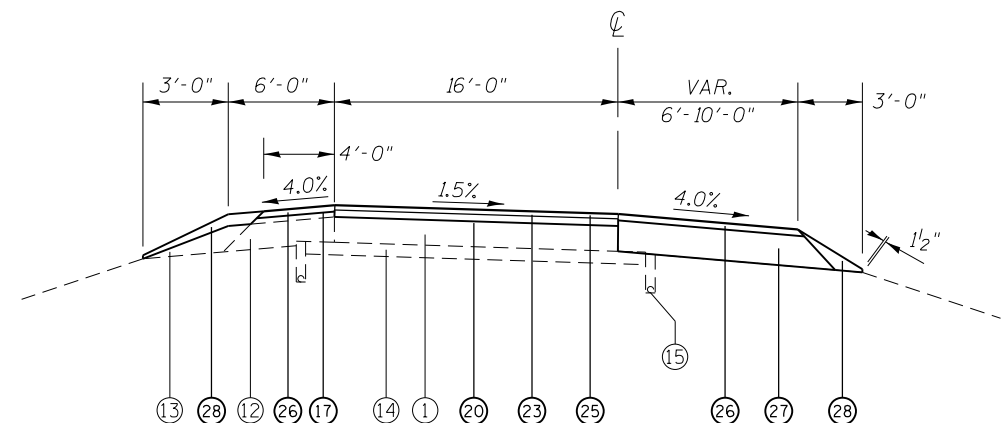
RAMP (SUPERELEVATED)
ATLANTA INTERCHANGE

NOTES:

1. MATCH EXISTING SUPERELEVATION SLOPES ON RAMPS.
2. LOW SIDE SHOULD BE 4.0% WHEN SE OF PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SLOPE SHALL MATCH SE. HIGH SIDE SHOULDER SHALL BE SLOPED AT 4.0% WHEN PAVEMENT IS BETWEEN 0.0% AND 4.0%. WHEN PAVEMENT IS GREATER THAN 4.0%, SHOULDER SHALL BE SLOPED SO THE ALGEBRAIC DIFFERENCE DOES NOT EXCEED 8.0%.
3. CONTINUOUS RUMBLE STRIPS, AS DETAILED ON STANDARD 642001, SHALL BE CONSTRUCTED ON ALL MAINLINE SHOULDERS EXCEPT WHERE NOTED ON STANDARD AND MEDIAN CROSS OVER DETAIL.



DETAIL A
TYP. INSIDE AGG. SHLDR



SEE DETAIL A
FOR AGG. SHLDR

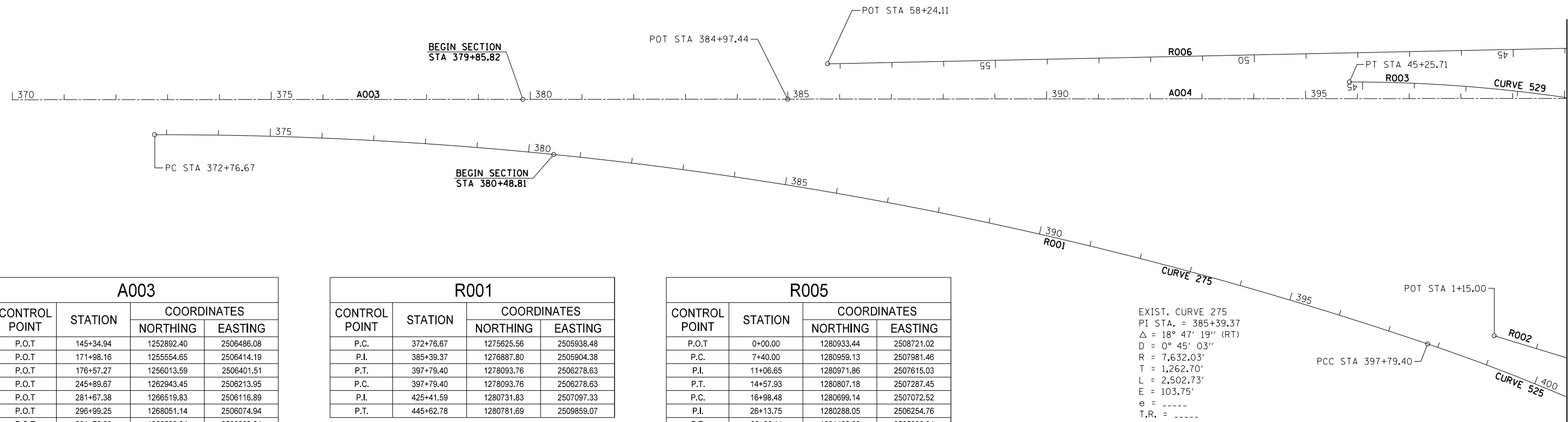
TYPICAL #21

RAMP A STA 11+54 TO STA 18+37
RAMP C STA 11+68 TO STA 18+37
ATLANTA INTERCHANGE

LEGEND

- | | | |
|---|---|---|
| ① EX PCC CONC PAVEMENT - 8" | ⑩ EX HOT MIX ASPHALT SHOULDER - 8" | ⑮ PR HOT MIX ASPHALT SURFACE REMOVAL, 3/4" |
| ② EX CRC PAVEMENT - 8" | ⑪ EX HOT MIX ASPHALT SHOULDER - 9" | ⑯ PR HOT MIX ASPHALT SURFACE REMOVAL, 1 1/2" |
| ③ EX CRC PAVEMENT - 9" | ⑫ EX HOT MIX ASPHALT SHOULDER - 12 1/4" | ⑰ PR HOT MIX ASPHALT SURFACE REMOVAL, 2" |
| ④ EX CRC PAVEMENT - 11" | ⑬ EX AGGREGATE SHOULDER | ⑱ PR HOT MIX ASPHALT SURFACE REMOVAL, 2 1/2" |
| ⑤ EX HOT MIX ASPHALT | ⑭ EX STABILIZED SUBBABSE - 4" | ⑲ PR HOT MIX ASPHALT SURFACE REMOVAL, 3 1/4" |
| ⑥ EX SUBBASE GRANULAR MATERIAL, TY B - 12" | ⑮ EX PIPE UNDERDRAINS | ⑳ PR HOT MIX ASPHALT SURFACE REMOVAL, 4 3/4" |
| ⑦ EX HOT MIX ASPHALT PAVEMENT - 3 1/4" | ⑯ EX PAVEMENT (BURIED) | ㉑ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (2 1/4") |
| ⑧ EX HOT MIX ASPHALT PAVEMENT - 2 1/2" | | ㉒ PR POLY HOT MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90, (3 1/4") |
| ⑨ EX HOT MIX ASPHALT SHOULDER, VARIABLE DEPTH | | ㉓ PR POLY HOT MIX ASPHALT SURFACE COURSE, MIX "E", N90, (1 1/2") |
| | | ㉔ PR HOT MIX ASPHALT SHOULDERS (MIX "C", N50), (2") |
| | | ㉕ PR HOT MIX ASPHALT SHOULDERS, 8" |
| | | ㉖ PR AGGREGATE WEDGE SHOULDER, TYPE B |
| | | ㉗ PR PIPE UNDERDRAINS, 4" |
| | | ㉘ PR PIPE UNDERDRAINS, 4" (MODIFIED) |

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 TYPICAL SECTIONS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Plot DATE = Jul-14-2014 03:37:04PM	DRAWN -	REVISED -					LOGAN	108	34
		CHECKED -	REVISED -			SCALE:	SHEET 10 OF SHEETS	CONTRACT NO.	72D41	
		DATE -	REVISED -			STA.	TO STA.	ILLINOIS FED. AID PROJECT		



MATCH LINE - STA 400+00

EXIST. CURVE 275
 PI STA. = 385+39.37
 $\Delta = 18^\circ 47' 19''$ (RT)
 $D = 0^\circ 45' 03''$
 $R = 7,632.03'$
 $T = 1,262.70'$
 $L = 2,502.73'$
 $E = 103.75'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 P.C. STA. = 372+76.67
 P.T. STA. = 397+79.40

A003			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	145+34.94	1252892.40	2506486.08
P.O.T	171+98.16	1255554.65	2506414.19
P.O.T	176+57.27	1256013.59	2506401.51
P.O.T	245+89.67	1262943.45	2506213.95
P.O.T	281+67.38	1266519.83	2506116.89
P.O.T	296+99.25	1268051.14	2506074.94
P.O.T	301+76.32	1268528.04	2506062.64
P.O.T	343+00.87	1272651.08	2505950.84
P.O.T	384+97.44	1276846.13	2505837.86

R001			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.C.	372+76.67	1275625.56	2505938.48
P.I.	385+39.37	1276887.80	2505904.38
P.T.	397+79.40	1278093.76	2506278.63
P.C.	397+79.40	1278093.76	2506278.63
P.I.	425+41.59	1280731.83	2507097.33
P.T.	445+62.78	1280781.69	2509859.07

R005			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	0+00.00	1280933.44	2508721.02
P.C.	7+40.00	1280959.13	2507981.46
P.I.	11+06.65	1280971.86	2507615.03
P.T.	14+57.93	1280807.18	2507287.45
P.C.	16+98.48	1280699.14	2507072.52
P.I.	26+13.75	1280288.05	2506254.76
P.T.	30+36.11	1281166.00	2505996.04
P.O.T	40+85.24	1282172.34	2505699.49

A004			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	385+00.00	1276846.13	2505837.86
P.O.T	424+30.48	1280775.16	2505731.26
P.O.T	452+88.81	1280826.77	2508589.13
P.O.T	465+56.26	1280849.92	2509856.37
P.O.T	510+82.28	1280931.15	2514381.65
P.O.T	534+00.86	1280972.78	2516699.86
P.O.T	542+02.07	1280987.31	2517500.94
P.O.T	562+97.38	1281025.54	2519595.90
P.O.T	578+00.55	1281052.63	2521098.82
P.O.T	604+03.81	1281099.51	2523701.67
P.C.	622+14.86	1281132.12	2525512.42
P.I.	630+85.26	1281147.79	2526382.68
P.T.	639+48.18	1281358.91	2527227.08
P.O.T	667+81.94	1282046.27	2529976.22
P.C.	675+43.92	1282231.01	2530715.47
P.I.	684+14.32	1282442.04	2531559.90
P.T.	692+77.24	1282457.64	2532430.16
P.C.	708+43.00	1282485.69	2533995.67
P.I.	728+22.98	1282521.17	2535975.33
P.T.	743+07.77	1284385.76	2536641.38
P.C.	761+00.55	1286074.07	2537244.46
P.I.	775+49.21	1287438.30	2537731.77
P.T.	789+63.83	1288529.20	2538684.96
P.O.T	790+97.12	1288629.57	2538772.65

R002			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	1+15.00	1278221.88	2506259.42
P.C.	2+75.00	1278376.07	2506302.16
P.I.	10+68.62	1279140.85	2506514.15
P.T.	18+23.90	1279906.42	2506305.00
P.O.T	28+00.19	1280848.19	2506047.71

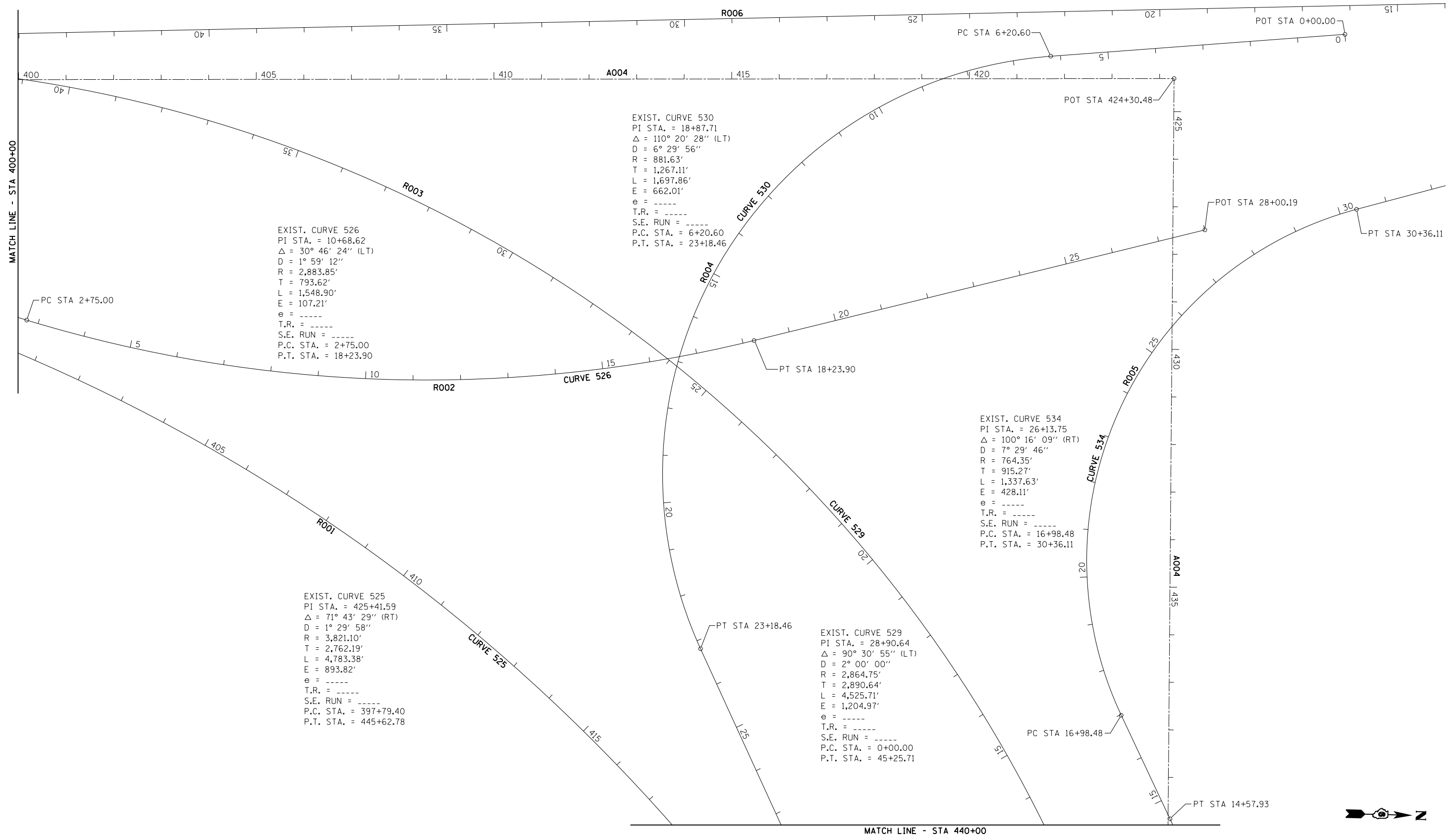
R006			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.C.	0+00.00	1282728.43	2505404.85
P.I.	4+13.64	1282328.87	2505511.88
P.T.	8+24.11	1281915.70	2505531.41
P.O.T	58+24.11	1276921.28	2505767.56

R003			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.C.	0+00.00	1280871.01	2508588.33
P.I.	28+90.64	1280818.97	2505698.16
P.T.	45+25.71	1277929.38	2505776.20

RAILROAD			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	10+00.00	1280979.19	2517053.26
P.O.T	11+22.78	1280881.00	2517126.98
P.O.T	12+75.64	1280758.76	2517218.75

R004			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	0+00.00	1281132.06	2505629.04
P.C.	6+20.60	1280514.57	2505691.11
P.I.	18+87.71	1279253.81	2505817.86
P.T.	23+18.46	1279810.90	2506955.93
P.O.T	34+99.38	1280330.10	2508016.60
P.O.T	37+35.58	1280433.94	2508228.74





EXIST. CURVE 526
 PI STA. = 10+68.62
 Δ = 30° 46' 24" (LT)
 D = 1° 59' 12"
 R = 2,883.85'
 T = 793.62'
 L = 1,548.90'
 E = 107.21'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 2+75.00
 P.T. STA. = 18+23.90

EXIST. CURVE 530
 PI STA. = 18+87.71
 Δ = 110° 20' 28" (LT)
 D = 6° 29' 56"
 R = 881.63'
 T = 1,267.11'
 L = 1,697.86'
 E = 662.01'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 6+20.60
 P.T. STA. = 23+18.46

EXIST. CURVE 534
 PI STA. = 26+13.75
 Δ = 100° 16' 09" (RT)
 D = 7° 29' 46"
 R = 764.35'
 T = 915.27'
 L = 1,337.63'
 E = 428.11'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 16+98.48
 P.T. STA. = 30+36.11

EXIST. CURVE 525
 PI STA. = 425+41.59
 Δ = 71° 43' 29" (RT)
 D = 1° 29' 58"
 R = 3,821.10'
 T = 2,762.19'
 L = 4,783.38'
 E = 893.82'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 397+79.40
 P.T. STA. = 445+62.78

EXIST. CURVE 529
 PI STA. = 28+90.64
 Δ = 90° 30' 55" (LT)
 D = 2° 00' 00"
 R = 2,864.75'
 T = 2,890.64'
 L = 4,525.71'
 E = 1,204.97'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 0+00.00
 P.T. STA. = 45+25.71

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
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Default	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Jul-14-2014 03:37:07PM	DATE -	REVISED -

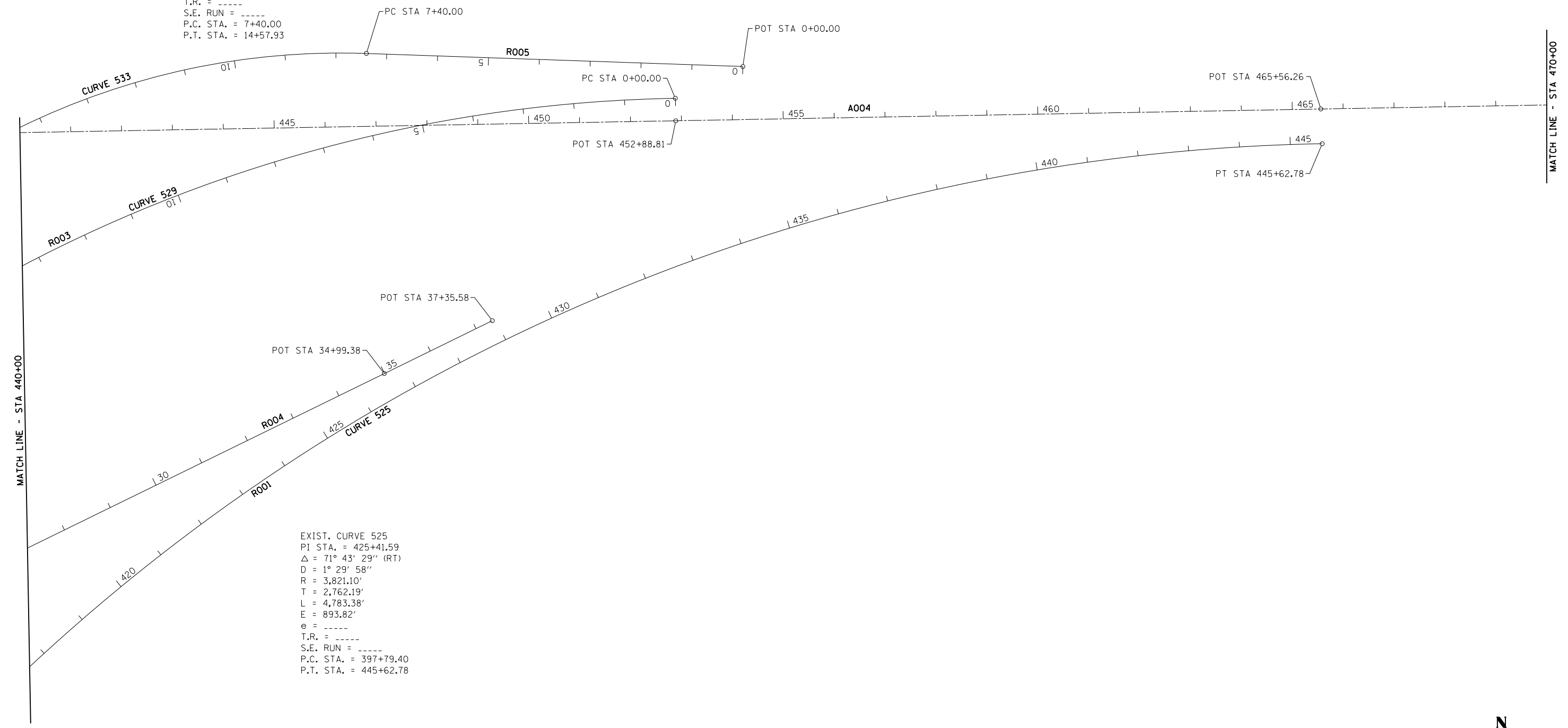
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, CROSS TIES & BENCHMARKS			
SCALE:	SHEET 2	OF	SHEETS
	STA.	TO	STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	LOGAN	108	36
			CONTRACT NO.	72D41
ILLINOIS FED. AID PROJECT				

EXIST. CURVE 533
 PI STA. = 11+06.65
 Δ = 28° 40' 39" (LT)
 D = 3° 59' 40"
 R = 1,434.37'
 T = 366.65'
 L = 717.93'
 E = 46.12'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 7+40.00
 P.T. STA. = 14+57.93

EXIST. CURVE 525
 PI STA. = 425+41.59
 Δ = 71° 43' 29" (RT)
 D = 1° 29' 58"
 R = 3,821.10'
 T = 2,762.19'
 L = 4,783.38'
 E = 893.82'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 397+79.40
 P.T. STA. = 445+62.78



FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ALIGNMENT, CROSS TIES & BENCHMARKS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	Plot Scale = 200.0000' / in.	CHECKED -	REVISED -					•	••	LOGAN	108	37	
	PLOT DATE = Jul-14-2014 03:37:07PM	DATE -	REVISED -		SCALE:	SHEET 3	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		
•• (54-4,54-5)RS-3, (54-6)RS-2													



MATCH LINE - STA 470+00



MATCH LINE - STA 500+00

MATCH LINE - STA 500+00



MATCH LINE - STA 530+00

BM 32A: CHISELED "□" ON GUARDRAIL BRACE
 80.5' RT, STA 528+95
 ELEV 590.20



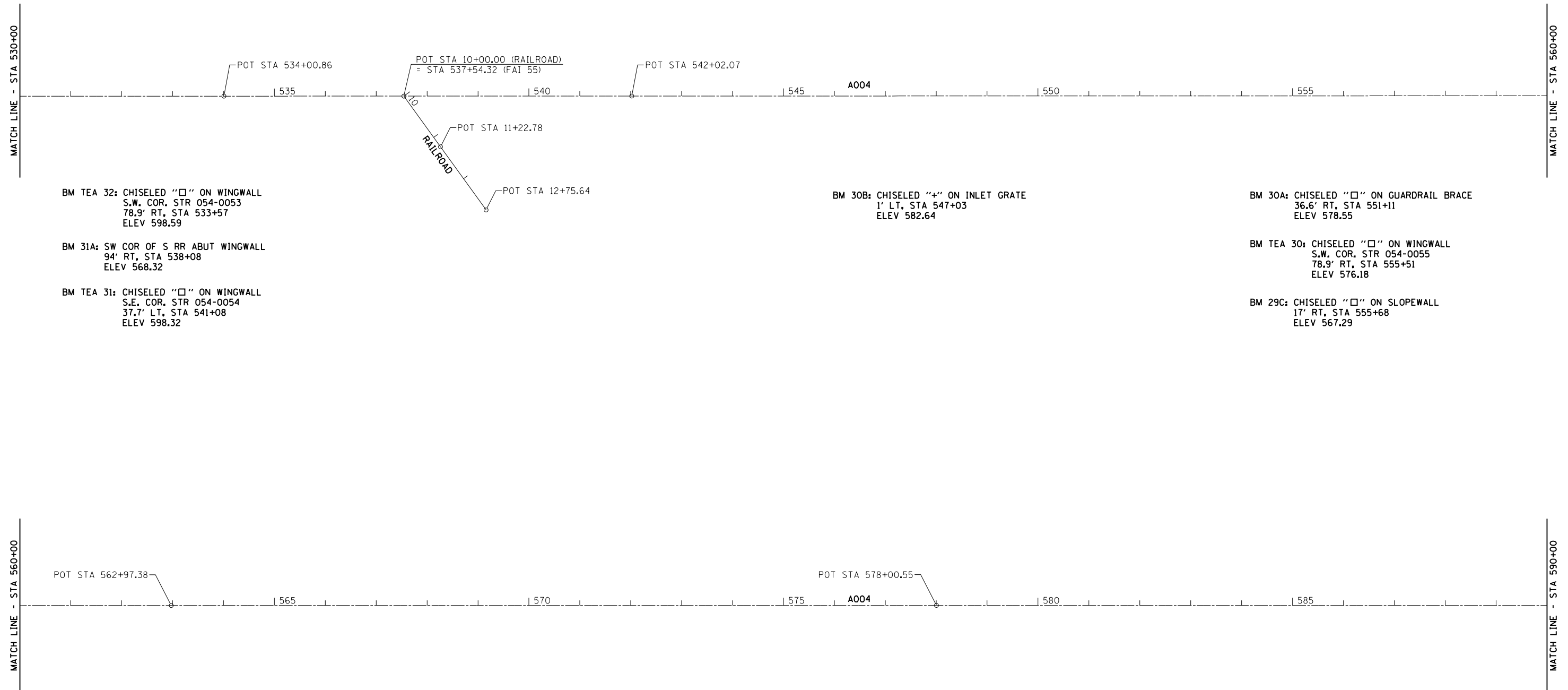
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Default	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Jul-14-2014 03:37:08PM	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, CROSS TIES & BENCHMARKS

SCALE: SHEET 4 OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	LOGAN	108	38
CONTRACT NO. 72D41			ILLINOIS FED. AID PROJECT	



BM TEA 32: CHISELED "□" ON WINGWALL
S.W. COR. STR 054-0053
78.9' RT, STA 533+57
ELEV 598.59

BM 31A: SW COR OF S RR ABUT WINGWALL
94' RT, STA 538+08
ELEV 568.32

BM TEA 31: CHISELED "□" ON WINGWALL
S.E. COR. STR 054-0054
37.7' LT, STA 541+08
ELEV 598.32

BM 30B: CHISELED "+" ON INLET GRATE
1' LT, STA 547+03
ELEV 582.64

BM 30A: CHISELED "□" ON GUARDRAIL BRACE
36.6' RT, STA 551+11
ELEV 578.55

BM TEA 30: CHISELED "□" ON WINGWALL
S.W. COR. STR 054-0055
78.9' RT, STA 555+51
ELEV 576.18

BM 29C: CHISELED "□" ON SLOPEWALL
17' RT, STA 555+68
ELEV 567.29

BM 29B: CHISELED "□" ON SLOPEWALL
9' RT, STA 561+69
ELEV 567.23

BM TEA 29: CHISELED "□" ON WINGWALL
N.E. COR. STR 054-0056
78.8' LT, STA 562+03
ELEV 576.82

BM 29A: CHISELED "□" ON GUARDRAIL BRACE
35.5' LT, STA 566+29
ELEV 584.28



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

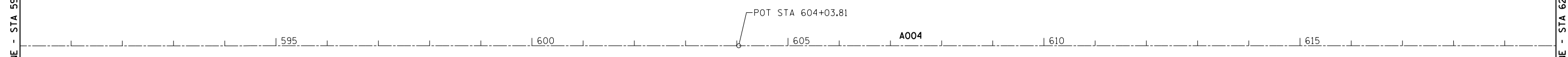
ALIGNMENT, CROSS TIES & BENCHMARKS			
SCALE:	SHEET 5	OF	SHEETS
	STA.		TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	LOGAN	108	39
			CONTRACT NO.	72D41
ILLINOIS FED. AID PROJECT				



MATCH LINE - STA 620+00

MATCH LINE - STA 590+00



MATCH LINE - STA 620+00



EXIST. CURVE 514
 PI STA. = 630+85.26
 Δ = 13° 00' 21" (LT)
 D = 0° 45' 01"
 R = 7,635.91'
 T = 870.40'
 L = 1,733.32'
 E = 49.45'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 622+14.86
 P.T. STA. = 639+48.18



FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
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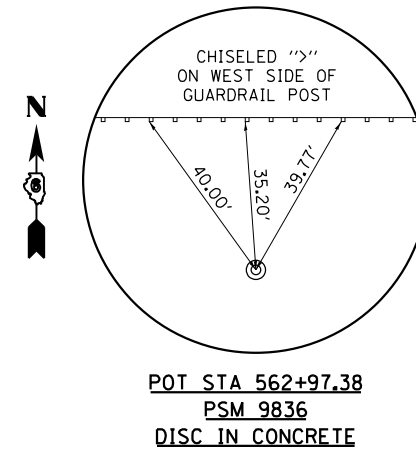
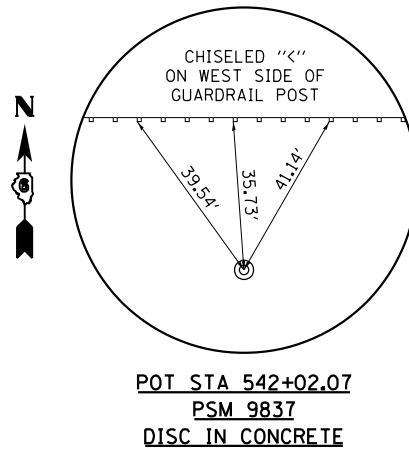
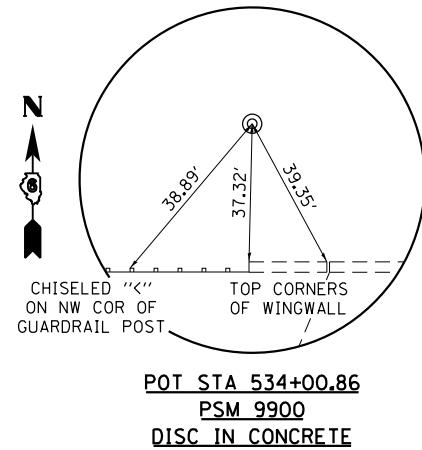
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, CROSS TIES & BENCHMARKS

SCALE: SHEET 6 OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	LOGAN	108	40
ILLINOIS FED. AID PROJECT			CONTRACT NO.	72D41

A004 I-55			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	385+00.00	1276846.13	2505837.86
P.O.T	424+30.48	1280775.16	2505731.26
P.O.T	452+88.81	1280826.77	2508589.13
P.O.T	465+56.26	1280849.92	2509856.37
P.O.T	510+82.28	1280931.15	2514381.65
P.O.T	534+00.86	1280972.78	2516699.86
P.O.T	542+02.07	1280987.31	2517500.94
P.O.T	562+97.38	1281025.54	2519595.90
P.O.T	578+00.55	1281052.63	2521098.82
P.O.T	604+03.81	1281099.51	2523701.67
P.C.	622+14.86	1281132.12	2525512.42
P.I.	630+85.26	1281147.79	2526382.68
P.T.	639+48.18	1281358.91	2527227.08
P.O.T	667+81.94	1282046.27	2529976.22
P.C.	675+43.92	1282231.01	2530715.47
P.I.	684+14.32	1282442.04	2531559.90
P.T.	692+77.24	1282457.64	2532430.16
P.C.	708+43.00	1282485.69	2533995.67
P.I.	728+22.98	1282521.17	2535975.33
P.T.	743+07.77	1284385.76	2536641.38
P.C.	761+00.55	1286074.07	2537244.46
P.I.	775+49.21	1287438.30	2537731.77
P.T.	789+63.83	1288529.20	2538684.96
P.O.T	790+97.12	1288629.57	2538772.65



MATCH LINE - STA 645+00



MATCH LINE - STA 675+00



FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
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	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, CROSS TIES & BENCHMARKS

SCALE: SHEET 7 OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	LOGAN	108	41
CONTRACT NO. 72D41			ILLINOIS FED. AID PROJECT	

MATCH LINE - STA 675+00

PC STA 675+43.92

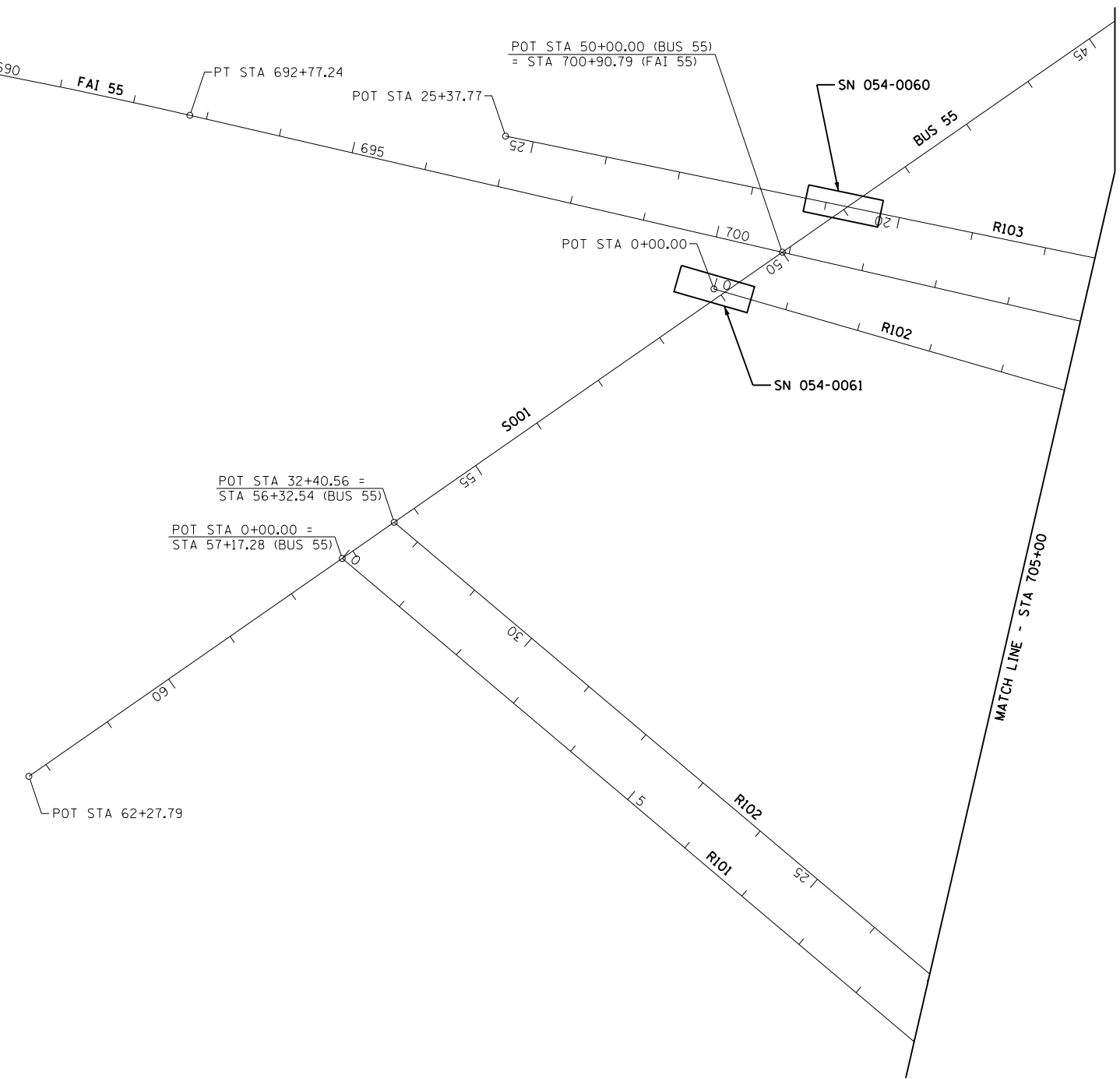
EXIST. CURVE 515
 PI STA. = 684+14.32
 Δ = 13° 00' 17" (RT)
 D = 0° 45' 01"
 R = 7,636.63'
 T = 870.40'
 L = 1,733.32'
 E = 49.44'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 675+43.92
 P.T. STA. = 692+77.24

BM # CB-1: CHISELED "□" IN THE CENTER OF THE NE PIER UNDER STR 054-0060 ALONG BUSINESS 55 ELEV 601.48

BM # TEA 23: CHISELED "□" ON EAST END OF SOUTH APPROACH WALL OF STR 054-0060 ELEV 633.05

BM # TEA 24: CHISELED "□" ±4' EAST OF WEST END OF NORTH APPROACH WALL OF STR 054-0061 ELEV 635.33

S001 BUS 55			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	25+57.79	1284310.90	2534850.94
P.O.T	50+00.00	1282472.21	2533243.58
P.O.T	62+27.79	1281547.83	2532435.50

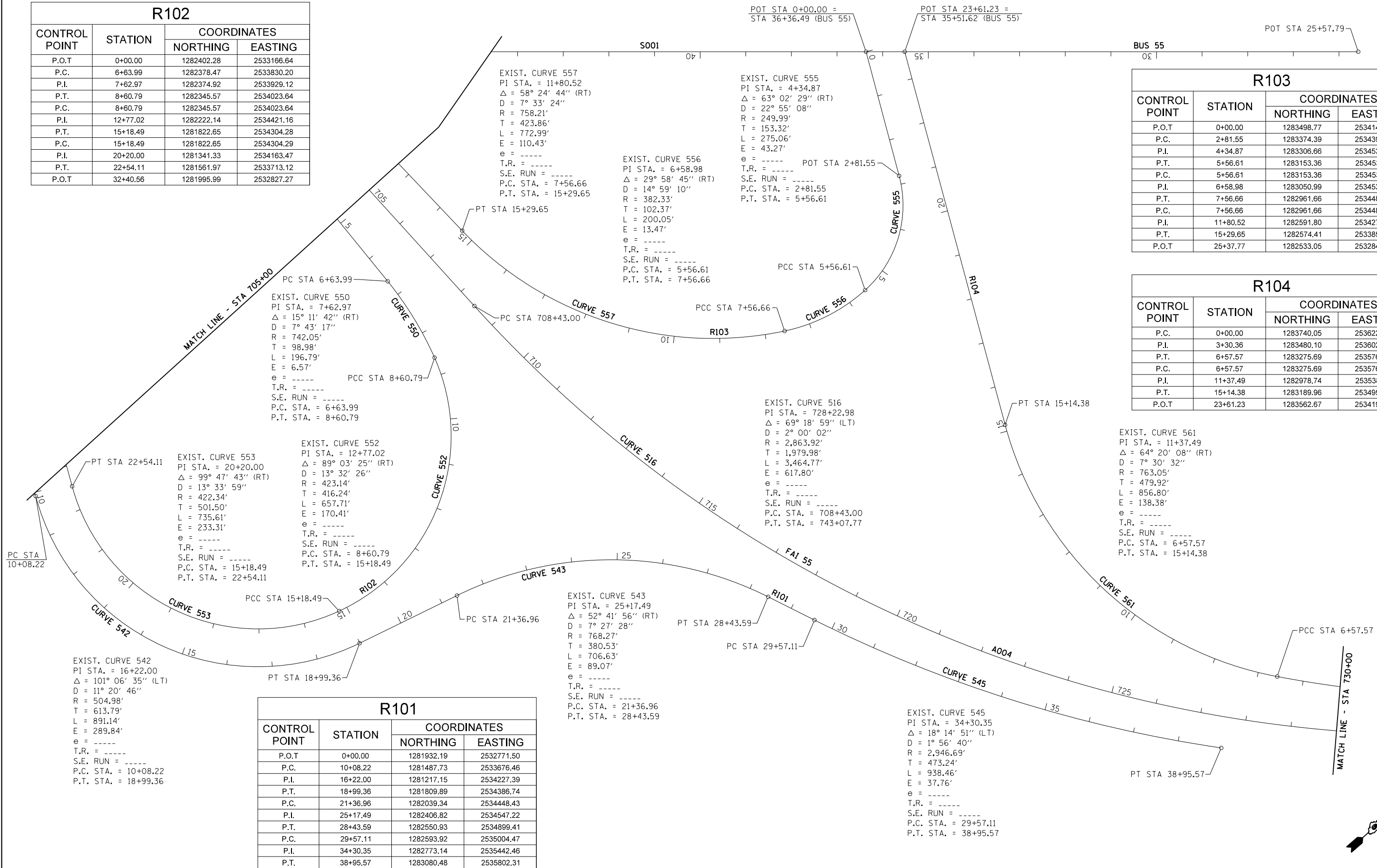


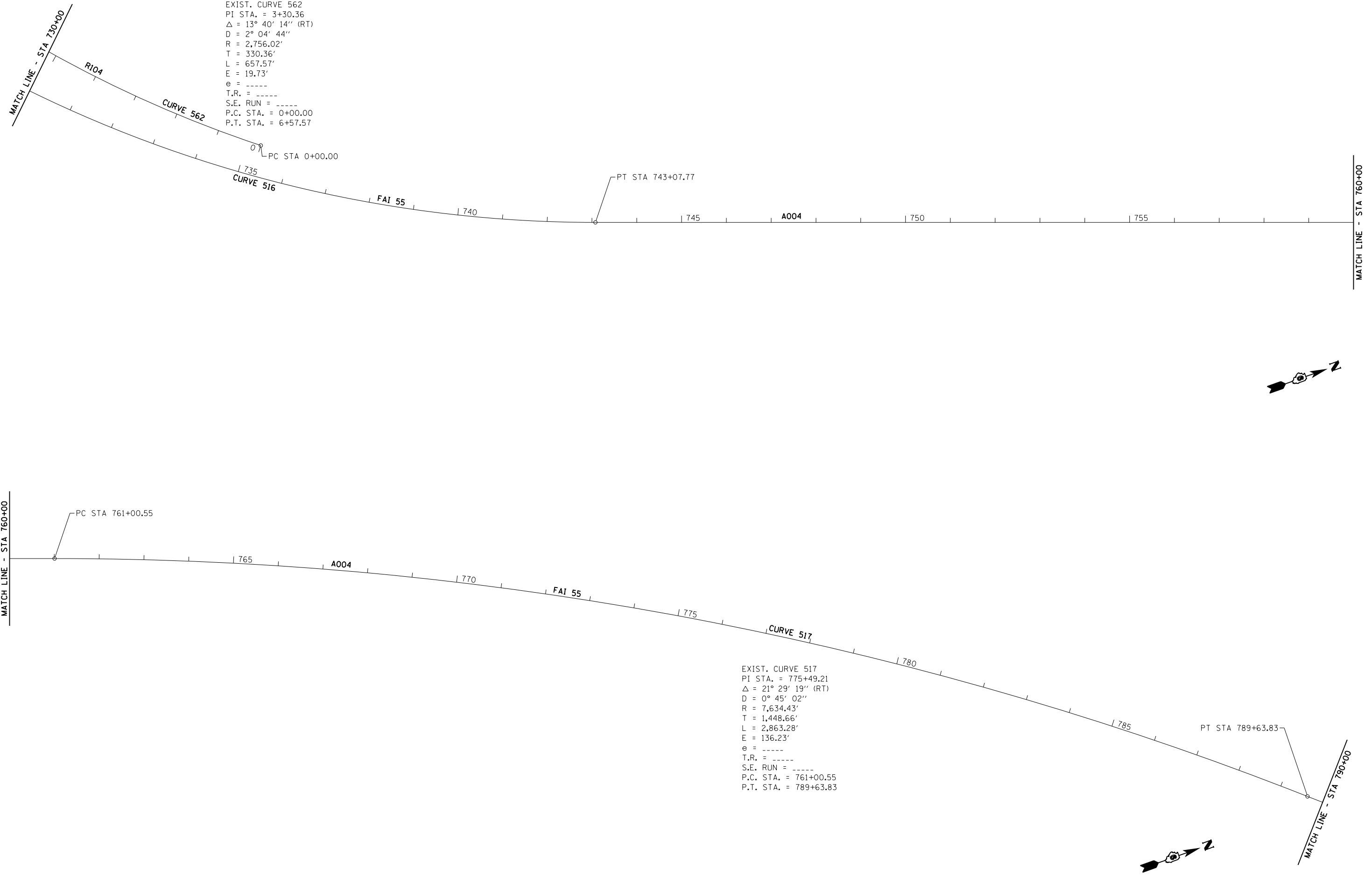
R102			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	0+00.00	1282402.28	2533166.64
P.C.	6+63.99	1282378.47	2533830.20
P.I.	7+62.97	1282374.92	2533929.12
P.T.	8+60.79	1282345.57	2534023.64
P.C.	8+60.79	1282345.57	2534023.64
P.I.	12+77.02	1282222.14	2534421.16
P.T.	15+18.49	1281822.65	2534304.28
P.C.	15+18.49	1281822.65	2534304.29
P.I.	20+20.00	1281341.33	2534163.47
P.T.	22+54.11	1281561.97	2533713.12
P.O.T	32+40.56	1281995.99	2532827.27

R103			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	0+00.00	1283498.77	2534140.98
P.C.	2+81.55	1283374.39	2534393.57
P.I.	4+34.87	1283306.68	2534531.12
P.T.	5+56.61	1283153.36	2534533.11
P.C.	5+56.61	1283153.36	2534533.11
P.I.	6+58.98	1283050.99	2534534.44
P.T.	7+56.66	1282961.66	2534484.44
P.C.	7+56.66	1282961.66	2534484.44
P.I.	11+80.52	1282591.80	2534277.42
P.T.	15+29.65	1282574.41	2533853.92
P.O.T	25+37.77	1282533.05	2532846.65

R104			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.C.	0+00.00	1283740.05	2536228.62
P.I.	3+30.36	1283480.10	2536024.75
P.T.	6+57.57	1283275.69	2535765.23
P.C.	6+57.57	1283275.69	2535765.23
P.I.	11+37.49	1282978.74	2535388.21
P.T.	15+14.38	1283189.96	2534957.27
P.O.T	23+61.23	1283562.67	2534196.84

R101			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	0+00.00	1281932.19	2532771.50
P.C.	10+08.22	1281487.73	2533676.46
P.I.	16+22.00	1281217.15	2534227.39
P.T.	18+99.36	1281809.89	2534386.74
P.C.	21+36.96	1282039.34	2534448.43
P.I.	25+17.49	1282406.82	2534547.22
P.T.	28+43.59	1282550.93	2534899.41
P.C.	29+57.11	1282593.92	2535004.47
P.I.	34+30.35	1282773.14	2535442.46
P.T.	38+95.57	1283080.48	2535802.31



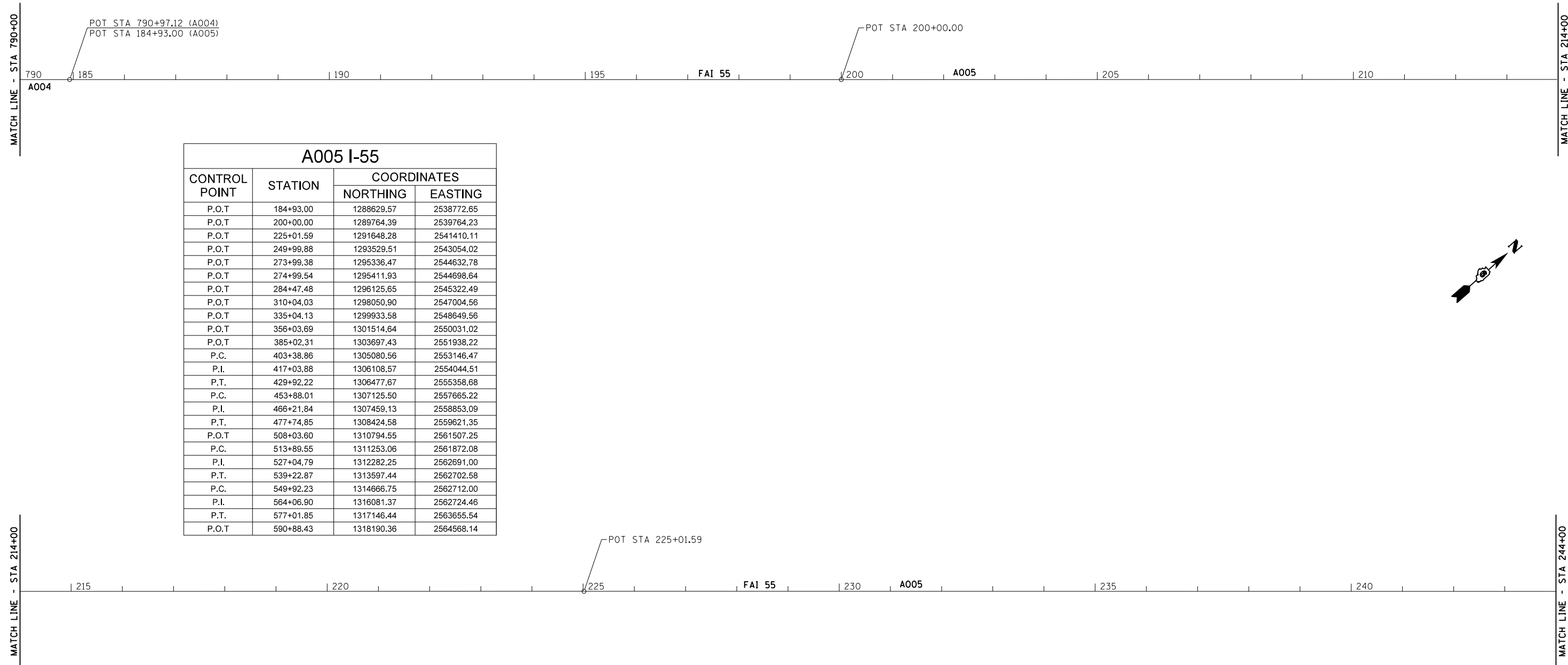


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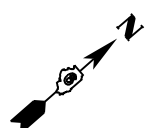
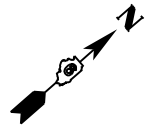
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, CROSS TIES & BENCHMARKS			
SCALE:	SHEET 10	OF	SHEETS
	STA.		TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	LOGAN	108	44
			CONTRACT NO.	72D41
ILLINOIS FED. AID PROJECT				



A005 I-55			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	184+93.00	1288629.57	2538772.65
P.O.T	200+00.00	1289764.39	2539764.23
P.O.T	225+01.59	1291648.28	2541410.11
P.O.T	249+99.88	1293529.51	2543054.02
P.O.T	273+99.38	1295336.47	2544632.78
P.O.T	274+99.54	1295411.93	2544698.64
P.O.T	284+47.48	1296125.65	2545322.49
P.O.T	310+04.03	1298050.90	2547004.56
P.O.T	335+04.13	1299933.58	2548649.56
P.O.T	356+03.69	1301514.64	2550031.02
P.O.T	385+02.31	1303697.43	2551938.22
P.C.	403+38.86	1305080.56	2553146.47
P.I.	417+03.88	1306108.57	2554044.51
P.T.	429+92.22	1306477.67	2555358.68
P.C.	453+88.01	1307125.50	2557665.22
P.I.	466+21.84	1307459.13	2558853.09
P.T.	477+74.85	1308424.58	2559621.35
P.O.T	508+03.60	1310794.55	2561507.25
P.C.	513+89.55	1311253.06	2561872.08
P.I.	527+04.79	1312282.25	2562691.00
P.T.	539+22.87	1313597.44	2562702.58
P.C.	549+92.23	1314666.75	2562712.00
P.I.	564+06.90	1316081.37	2562724.46
P.T.	577+01.85	1317146.44	2563655.54
P.O.T	590+88.43	1318190.36	2564568.14



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

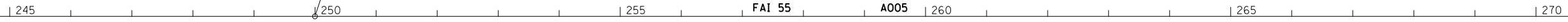
ALIGNMENT, CROSS TIES & BENCHMARKS

SCALE: SHEET 11 OF SHEETS STA. TO STA.

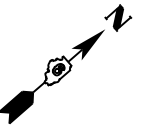
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	LOGAN	108	45
CONTRACT NO. 72D41			ILLINOIS FED. AID PROJECT	

MATCH LINE - STA 244+00

MATCH LINE - STA 274+00



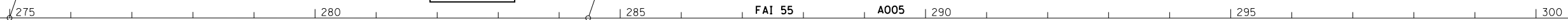
POT STA 273+99.38



BM * CB-2: CHISELED "□" ON CONCRETE SLOPEWALL
 NORTH END BETWEEN SN 054-0063 AND
 SN 054-0064 AT STA 283+85 10' LT
 ELEV 595.82

MATCH LINE - STA 274+00

MATCH LINE - STA 304+00



POT STA 274+99.54

SN 054-0064

POT STA 284+47.48

SN 054-0062

SN 054-0063



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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, CROSS TIES & BENCHMARKS

SCALE: SHEET 12 OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	LOGAN	108	46
CONTRACT NO. 72D41				

MATCH LINE - STA 304+00

MATCH LINE - STA 334+00

305 | 310 | 315 | FAI 55 | A005 | 320 | 325 | 330

POT STA 310+04.03

310

MATCH LINE - STA 334+00

MATCH LINE - STA 364+00

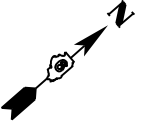
335 | 340 | 345 | FAI 55 | A005 | 350 | 355 | 360

POT STA 335+04.13

335

POT STA 356+03.69

356



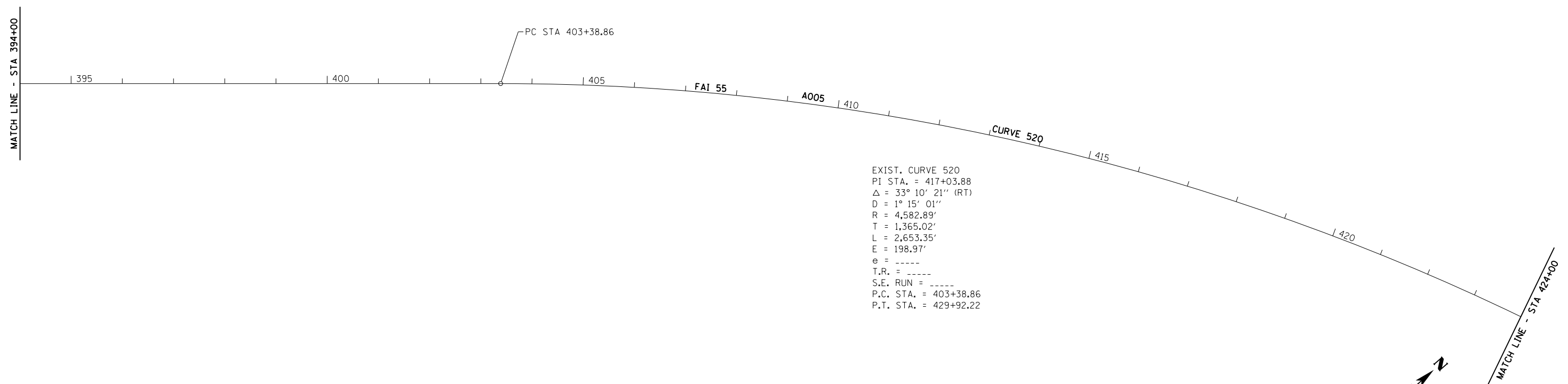
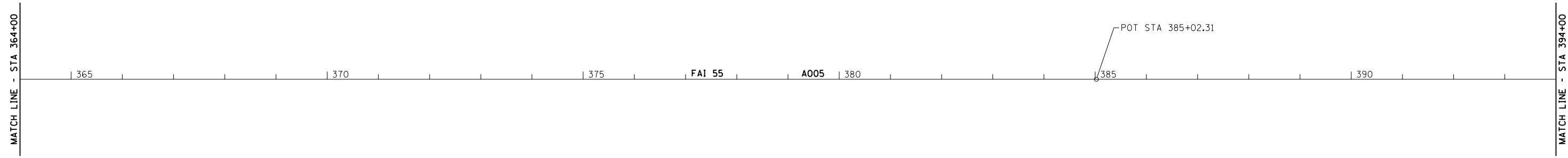
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, CROSS TIES & BENCHMARKS

SCALE: SHEET 13 OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	LOGAN	108	47
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72D41	



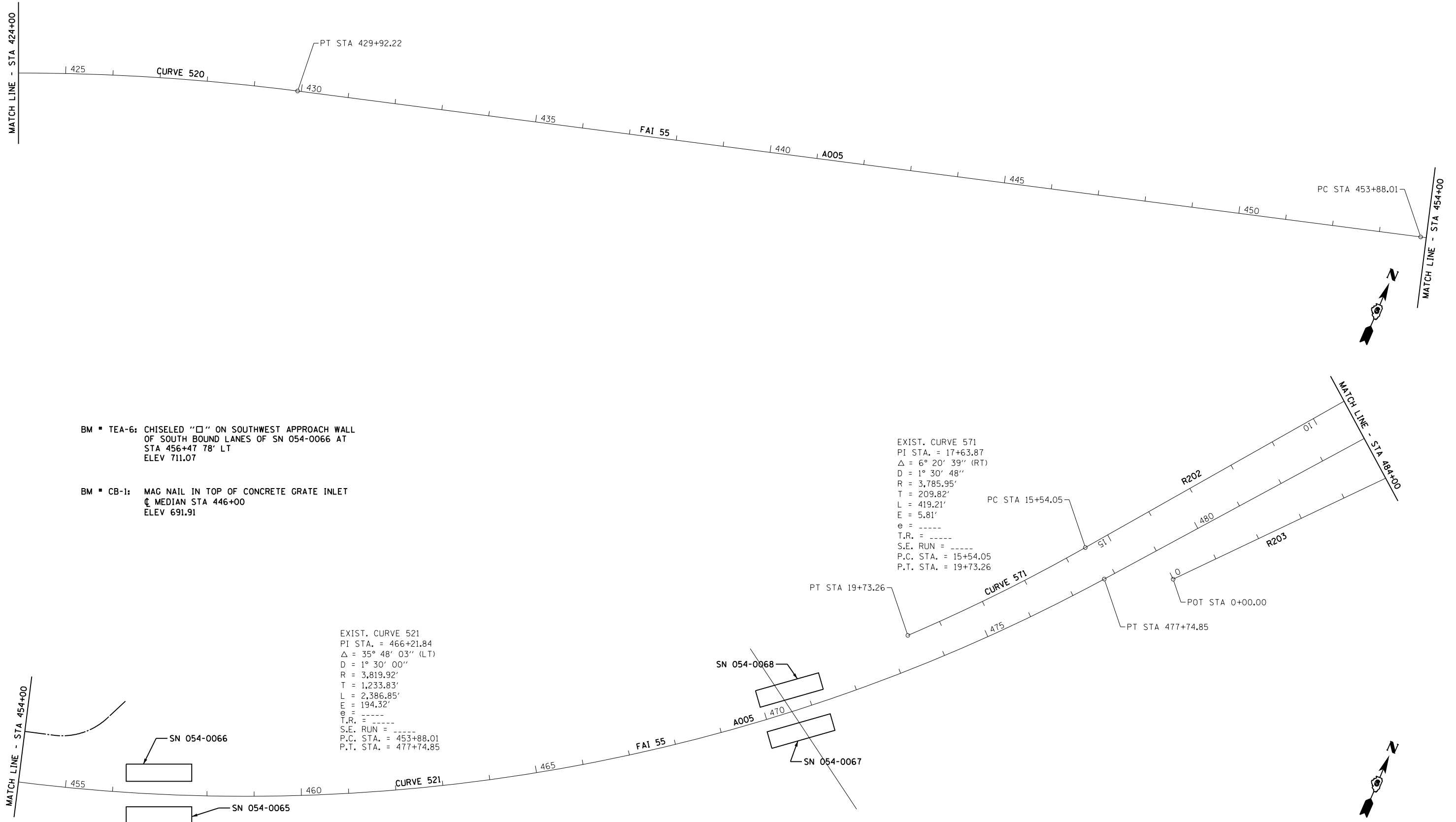
EXIST. CURVE 520
 PI STA. = 417+03.88
 Δ = 33° 10' 21" (RT)
 D = 1° 15' 01"
 R = 4,582.89'
 T = 1,365.02'
 L = 2,653.35'
 E = 198.97'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 403+38.86
 P.T. STA. = 429+92.22

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	PLOT DATE = Jul-14-2014 03:37:10PM	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, CROSS TIES & BENCHMARKS			
SCALE:	SHEET 14	OF	SHEETS
	STA.	TO	STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	LOGAN	108	48
			CONTRACT NO.	72D41
ILLINOIS FED. AID PROJECT				



BM ■ TEA-6: CHISELED "□" ON SOUTHWEST APPROACH WALL OF SOUTH BOUND LANES OF SN 054-0066 AT STA 456+47 78' LT ELEV 711.07

BM ■ CB-1: MAG NAIL IN TOP OF CONCRETE GRATE INLET @ MEDIAN STA 446+00 ELEV 691.91

EXIST. CURVE 571
 PI STA. = 17+63.87
 Δ = 6° 20' 39" (RT)
 D = 1° 30' 48"
 R = 3,785.95'
 T = 209.82'
 L = 419.21'
 E = 5.81'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 15+54.05
 P.T. STA. = 19+73.26

EXIST. CURVE 521
 PI STA. = 466+21.84
 Δ = 35° 48' 03" (LT)
 D = 1° 30' 00"
 R = 3,819.92'
 T = 1,233.83'
 L = 2,386.85'
 E = 194.32'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 453+88.01
 P.T. STA. = 477+74.85

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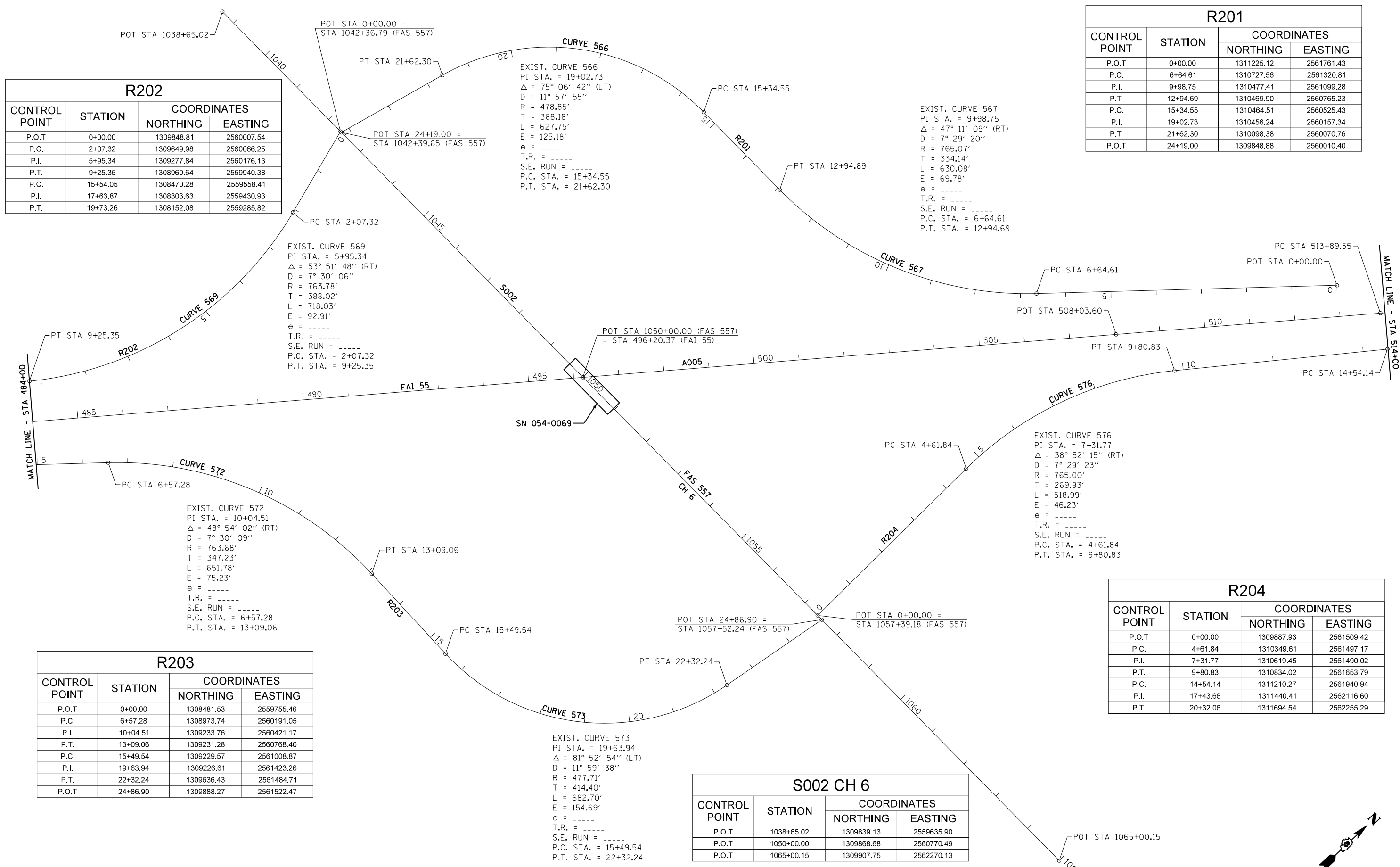
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, CROSS TIES & BENCHMARKS			
SCALE:	SHEET 15	OF	SHEETS
	STA.		TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	LOGAN	108	49
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72D41	

R202			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	0+00.00	1309848.81	2560007.54
P.C.	2+07.32	1309649.98	2560066.25
P.I.	5+95.34	1309277.84	2560176.13
P.T.	9+25.35	1308969.64	2559940.38
P.C.	15+54.05	1308470.28	2559558.41
P.I.	17+63.87	1308303.63	2559430.93
P.T.	19+73.26	1308152.08	2559285.82

R201			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	0+00.00	1311225.12	2561761.43
P.C.	6+64.61	1310727.56	2561320.81
P.I.	9+98.75	1310477.41	2561099.28
P.T.	12+94.69	1310469.90	2560765.23
P.C.	15+34.55	1310464.51	2560525.43
P.I.	19+02.73	1310456.24	2560157.34
P.T.	21+62.30	1310098.38	2560070.76
P.O.T	24+19.00	1309848.88	2560010.40



EXIST. CURVE 569
 PI STA. = 5+95.34
 $\Delta = 53^\circ 51' 48''$ (RT)
 D = 7° 30' 06"
 R = 763.78'
 T = 388.02'
 L = 718.03'
 E = 92.91'
 e = ----
 T.R. = ----
 S.E. RUN = ----
 P.C. STA. = 2+07.32
 P.T. STA. = 9+25.35

EXIST. CURVE 566
 PI STA. = 19+02.73
 $\Delta = 75^\circ 06' 42''$ (LT)
 D = 11° 57' 55"
 R = 478.85'
 T = 368.18'
 L = 627.75'
 E = 125.18'
 e = ----
 T.R. = ----
 S.E. RUN = ----
 P.C. STA. = 15+34.55
 P.T. STA. = 21+62.30

EXIST. CURVE 567
 PI STA. = 9+98.75
 $\Delta = 47^\circ 11' 09''$ (RT)
 D = 7° 29' 20"
 R = 765.07'
 T = 334.14'
 L = 630.08'
 E = 69.78'
 e = ----
 T.R. = ----
 S.E. RUN = ----
 P.C. STA. = 6+64.61
 P.T. STA. = 12+94.69

EXIST. CURVE 576
 PI STA. = 7+31.77
 $\Delta = 38^\circ 52' 15''$ (RT)
 D = 7° 29' 23"
 R = 765.00'
 T = 269.93'
 L = 518.99'
 E = 46.23'
 e = ----
 T.R. = ----
 S.E. RUN = ----
 P.C. STA. = 4+61.84
 P.T. STA. = 9+80.83

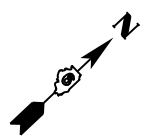
EXIST. CURVE 572
 PI STA. = 10+04.51
 $\Delta = 48^\circ 54' 02''$ (RT)
 D = 7° 30' 09"
 R = 763.68'
 T = 347.23'
 L = 651.78'
 E = 75.23'
 e = ----
 T.R. = ----
 S.E. RUN = ----
 P.C. STA. = 6+57.28
 P.T. STA. = 13+09.06

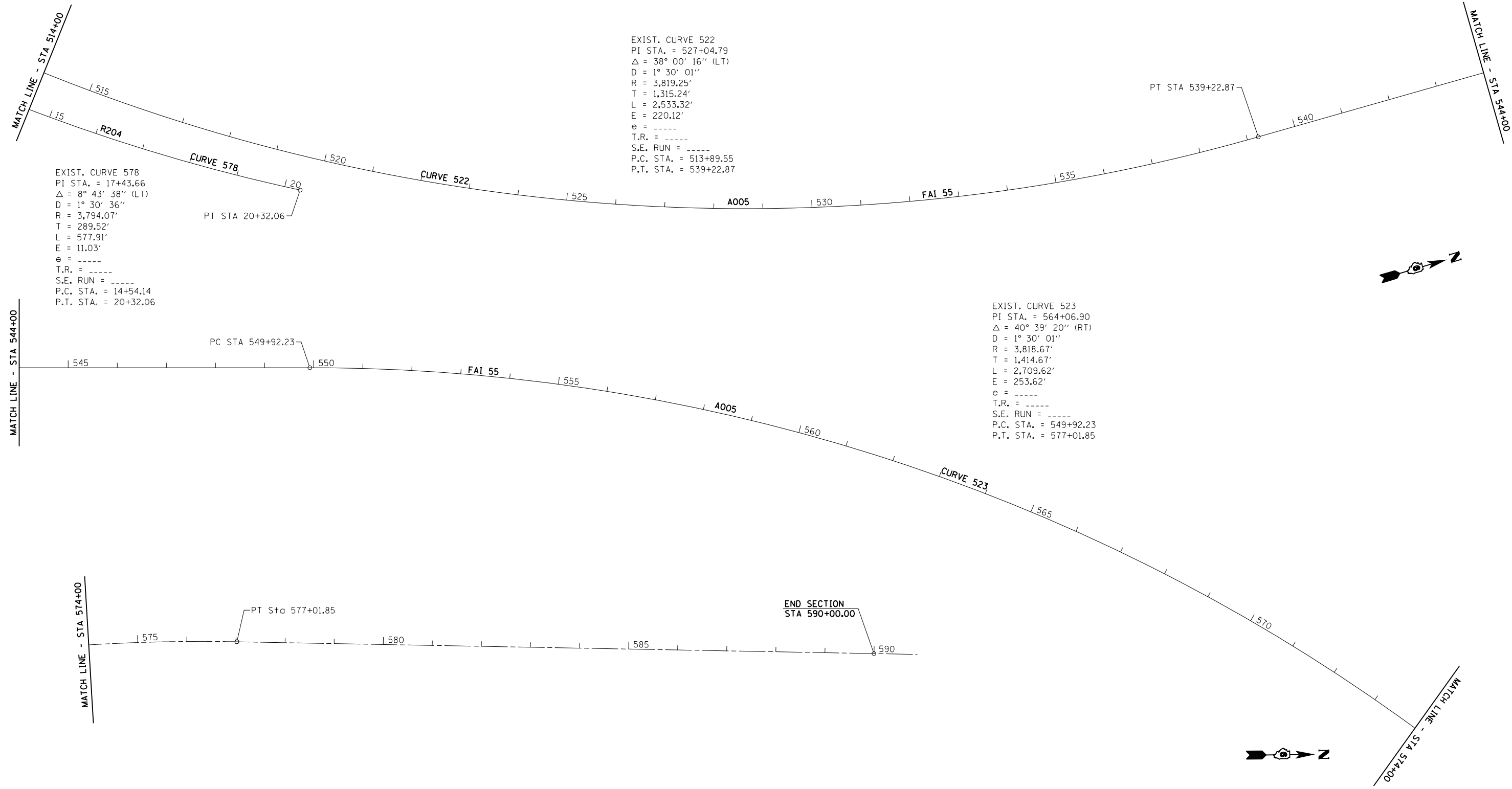
EXIST. CURVE 573
 PI STA. = 19+63.94
 $\Delta = 81^\circ 52' 54''$ (LT)
 D = 11° 59' 38"
 R = 477.71'
 T = 414.40'
 L = 682.70'
 E = 154.69'
 e = ----
 T.R. = ----
 S.E. RUN = ----
 P.C. STA. = 15+49.54
 P.T. STA. = 22+32.24

R203			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	0+00.00	1308481.53	2559755.46
P.C.	6+57.28	1308973.74	2560191.05
P.I.	10+04.51	1309233.76	2560421.17
P.T.	13+09.06	1309231.28	2560768.40
P.C.	15+49.54	1309229.57	2561008.87
P.I.	19+63.94	1309226.61	2561423.26
P.T.	22+32.24	1309636.43	2561484.71
P.O.T	24+86.90	1309888.27	2561522.47

S002 CH 6			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	1038+65.02	1309839.13	2559635.90
P.O.T	1050+00.00	1309868.68	2560770.49
P.O.T	1065+00.15	1309907.75	2562270.13

R204			
CONTROL POINT	STATION	COORDINATES	
		NORTHING	EASTING
P.O.T	0+00.00	1309887.93	2561509.42
P.C.	4+61.84	1310349.61	2561497.17
P.I.	7+31.77	1310619.45	2561490.02
P.T.	9+80.83	1310834.02	2561653.79
P.C.	14+54.14	1311210.27	2561940.94
P.I.	17+43.66	1311440.41	2562116.60
P.T.	20+32.06	1311694.54	2562255.29





EXIST. CURVE 522
 PI STA. = 527+04.79
 Δ = 38° 00' 16" (LT)
 D = 1° 30' 01"
 R = 3,819.25'
 T = 1,315.24'
 L = 2,533.32'
 E = 220.12'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 513+89.55
 P.T. STA. = 539+22.87

EXIST. CURVE 578
 PI STA. = 17+43.66
 Δ = 8° 43' 38" (LT)
 D = 1° 30' 36"
 R = 3,794.07'
 T = 289.52'
 L = 577.91'
 E = 11.03'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 14+54.14
 P.T. STA. = 20+32.06

EXIST. CURVE 523
 PI STA. = 564+06.90
 Δ = 40° 39' 20" (RT)
 D = 1° 30' 01"
 R = 3,818.67'
 T = 1,414.67'
 L = 2,709.62'
 E = 253.62'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 549+92.23
 P.T. STA. = 577+01.85

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
Default	Default	DRAWN - DMS	REVISED -
	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Jul-14-2014 03:37:10PM	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, CROSS TIES & BENCHMARKS			
SCALE:	SHEET 17	OF	SHEETS
	STA.	TO	STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	LOGAN	108	51
			CONTRACT NO.	72D41
ILLINOIS FED. AID PROJECT				



SEC 22, T20N, R3W, 3RD PM

STA 53+00 ROADWAY "F"
60"x144" P CUL TY 2 RCCP
STD 1998 DS-2 HDWLS
SKEW 20°RT

RESURFACING BEGINS
I-55 STA 379+87.59

SEE BUTT
JOINT DETAIL

SN 054-0048

EX ROW

EX ROW

ROADWAY F
CHAIN R006

SOUTHBOUND FAI 55

NORTHBOUND FAI 55

PSM 385+00.00

SURVEY FAI 55
CHAIN A004

MATCHLINE STA 393+00.00

EX ROW

SN 054-0049

SEE BUTT
JOINT DETAIL

RESURFACING BEGINS
RDWY 'A' STA 380+57.30

ROADWAY A
CURVE 275
CHAIN R001

MATCHLINE STA 393+00.00

EXIST. CURVE 275
PI STA. = 385+39.37
 Δ = 18° 47' 19" (RT)
D = 0° 45' 03"
R = 7,632.03'
T = 1,262.70'
L = 2,502.73'
E = 103.75'
e = 2.63%
T.R. = 47.22'
S.E. RUN = 118.30'
P.C. STA. = 372+76.67
P.T. STA. = 397+79.40

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
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	PLOT DATE = Jul-14-2014 03:37:13PM	DATE -	REVISED -

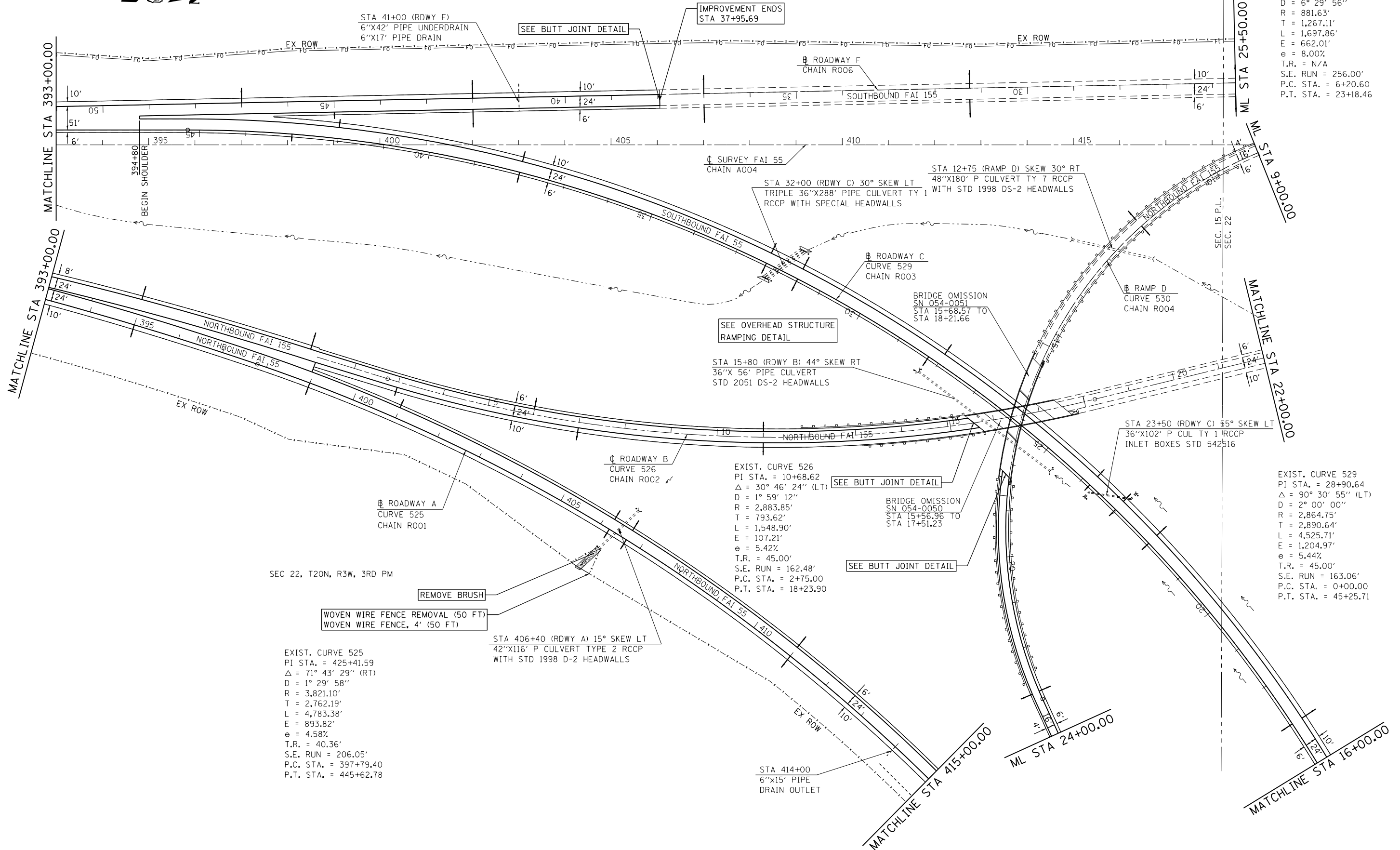
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. ROUTE 55
PLAN SHEET

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	LOGAN	108	52
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72D41	

SCALE: 1" = 100' SHEET 1 OF SHEETS STA. TO STA.

SEC 22, T20N, R3W, 3RD PM



EXIST. CURVE 530
 PI STA. = 18+87.71
 $\Delta = 110^\circ 20' 28''$ (LT)
 D = 6° 29' 56"
 R = 881.63'
 T = 1,267.11'
 L = 1,697.86'
 E = 662.01'
 e = 8.00%
 T.R. = N/A
 S.E. RUN = 256.00'
 P.C. STA. = 6+20.60
 P.T. STA. = 23+18.46

EXIST. CURVE 529
 PI STA. = 28+90.64
 $\Delta = 90^\circ 30' 55''$ (LT)
 D = 2° 00' 00"
 R = 2,864.75'
 T = 2,890.64'
 L = 4,525.71'
 E = 1,204.97'
 e = 5.44%
 T.R. = 45.00'
 S.E. RUN = 163.06'
 P.C. STA. = 0+00.00
 P.T. STA. = 45+25.71

EXIST. CURVE 525
 PI STA. = 425+41.59
 $\Delta = 71^\circ 43' 29''$ (RT)
 D = 1° 29' 58"
 R = 3,821.10'
 T = 2,762.19'
 L = 4,783.38'
 E = 893.82'
 e = 4.58%
 T.R. = 40.36'
 S.E. RUN = 206.05'
 P.C. STA. = 397+79.40
 P.T. STA. = 445+62.78

EXIST. CURVE 526
 PI STA. = 10+68.62
 $\Delta = 30^\circ 46' 24''$ (LT)
 D = 1° 59' 12"
 R = 2,883.85'
 T = 793.62'
 L = 1,548.90'
 E = 107.21'
 e = 5.42%
 T.R. = 45.00'
 S.E. RUN = 162.48'
 P.C. STA. = 2+75.00
 P.T. STA. = 18+23.90

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
Default	2041-sht-plan-1.dgn	DRAWN -	REVISED -
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	PLOT DATE = Jul-14-2014 03:37:13PM	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

F.A.I. ROUTE 55
 PLAN SHEET

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	..	LOGAN	108	53
CONTRACT NO. 72D41			ILLINOIS FED. AID PROJECT	

SCALE: 1" = 100' SHEET 2 OF SHEETS STA. TO STA.

EXIST. CURVE 533
 PI STA. = 11+06.65
 $\Delta = 28^\circ 40' 39''$ (LT)
 $D = 3^\circ 59' 40''$
 $R = 1,434.37'$
 $T = 366.65'$
 $L = 717.93'$
 $E = 46.12'$
 $e = 8.00\%$
 $T.R. = N/A$
 $S.E. RUN = 284.16'$
 $P.C. STA. = 7+40.00$
 $P.T. STA. = 14+57.93$

SEC 15, T20N, R3W, 3RD PM

SEC 15, T20N, R3W, 3RD PM

SEC 14, T20N, R3W, 3RD PM



IMPROVEMENT ENDS
 STA 00+16.65

SEE BUTT JOINT DETAIL

STA 458+13 (C I55) SKEW 15° LT
 DBL 36"X212' P CULVERT TY 2
 RCCP WITH SP HEADWALLS

RAMP E
 CURVE 533
 CHAIN 0005

RAMP E
 CHAIN 0005

EX ROW

ML STA 18+00.00

MATCHLINE STA 16+00.00

MATCHLINE STA 465+00.00

EXIST. CURVE 529
 PI STA. = 28+90.64
 $\Delta = 90^\circ 30' 55''$ (LT)
 $D = 2^\circ 00' 00''$
 $R = 2,864.75'$
 $T = 2,890.64'$
 $L = 4,525.71'$
 $E = 1,204.97'$
 $e = 5.44\%$
 $T.R. = 45.00$
 $S.E. RUN = 163.06'$
 $P.C. STA. = 0+00.00$
 $P.T. STA. = 45+25.71$

ROADWAY C
 CURVE 529
 CHAIN 0003

ROADWAY C
 CURVE 533
 CHAIN 0003

ROADWAY A
 CURVE 525
 CHAIN 0001

STONE DUMPED
 RIPRAP, CLASS A4 (10 TON)

STA 437+77 (RDWY A) SKEW 25° LT
 DBL 36"X208' P CULVERT TY 2 RCCP
 WITH SPECIAL HEADWALLS

SEC. 15 P.L.
 SEC. 22

STA 25+50 (RAMP D)
 36"X266' P CUL
 TY 4 RCCP
 STD 2051 DS-2 HDWLS
 45° SKEW LT

RAMP D
 CHAIN 0004

EXIST. CURVE 525
 PI STA. = 425+41.59
 $\Delta = 71^\circ 43' 29''$ (RT)
 $D = 1^\circ 29' 58''$
 $R = 3,821.10'$
 $T = 2,762.19'$
 $L = 4,783.38'$
 $E = 893.82'$
 $e = 4.58\%$
 $T.R. = 40.36'$
 $S.E. RUN = 206.05'$
 $P.C. STA. = 397+79.40$
 $P.T. STA. = 445+62.78$

ML STA 24+00.00

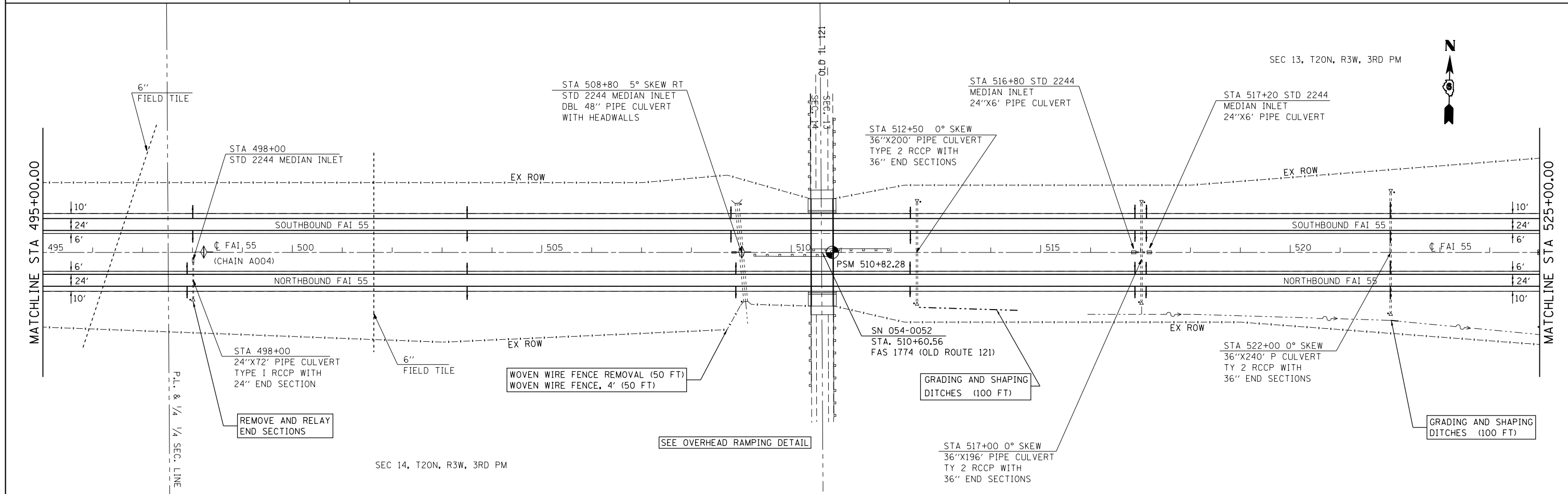
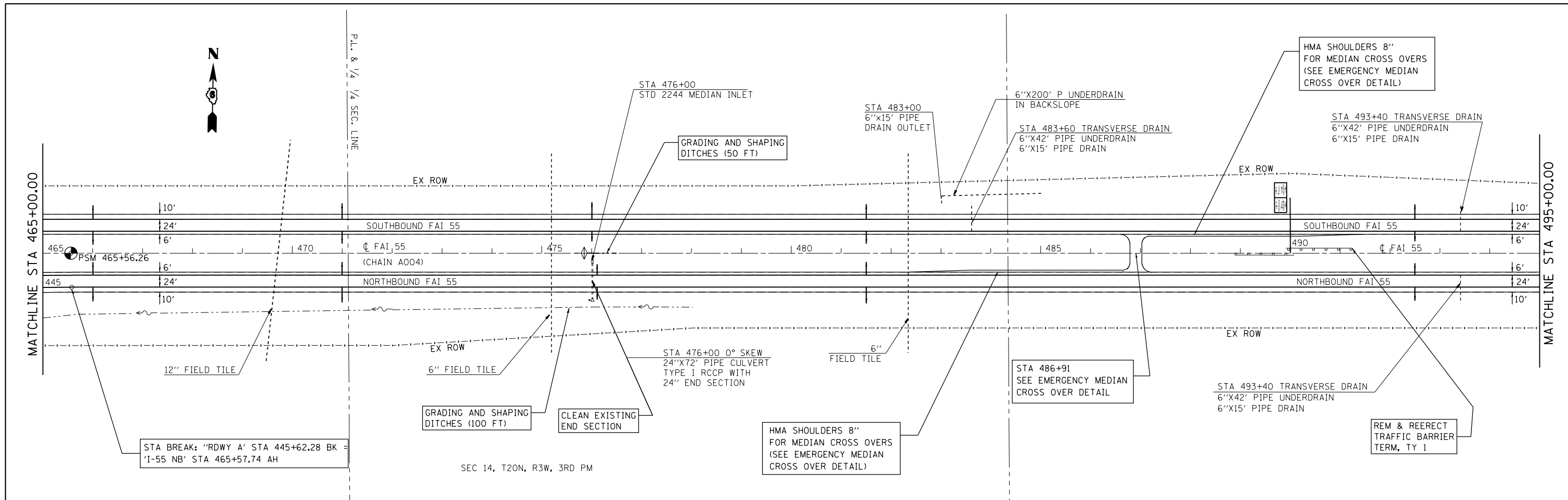
MATCHLINE STA 415+00.00

ROADWAY A
 CHAIN 0001

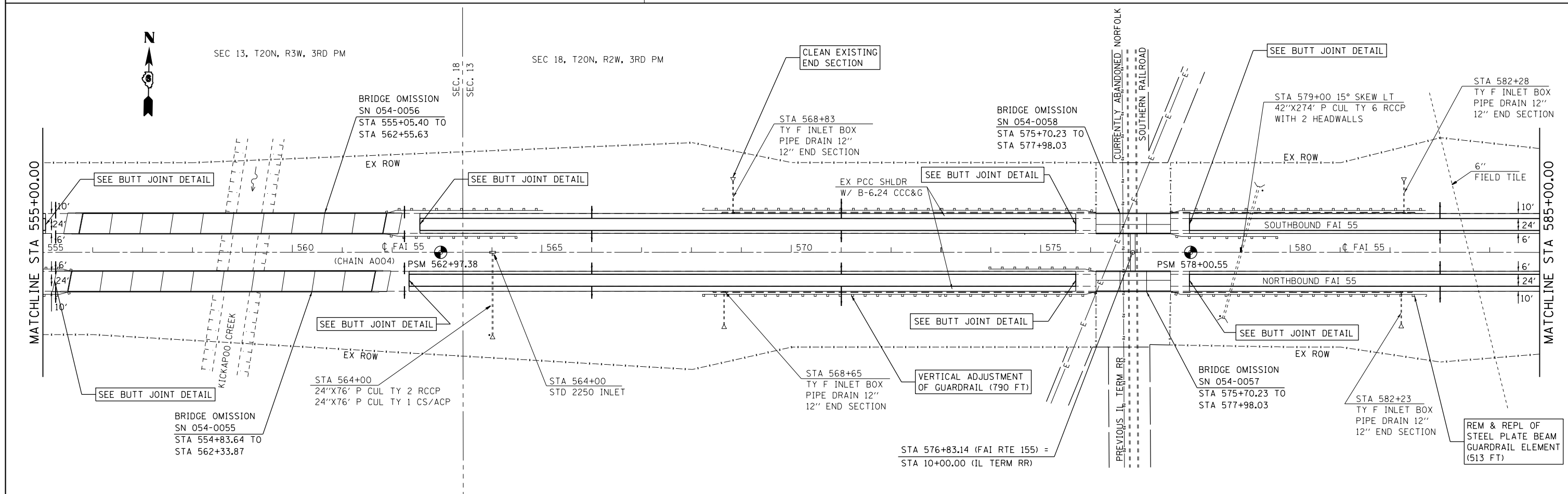
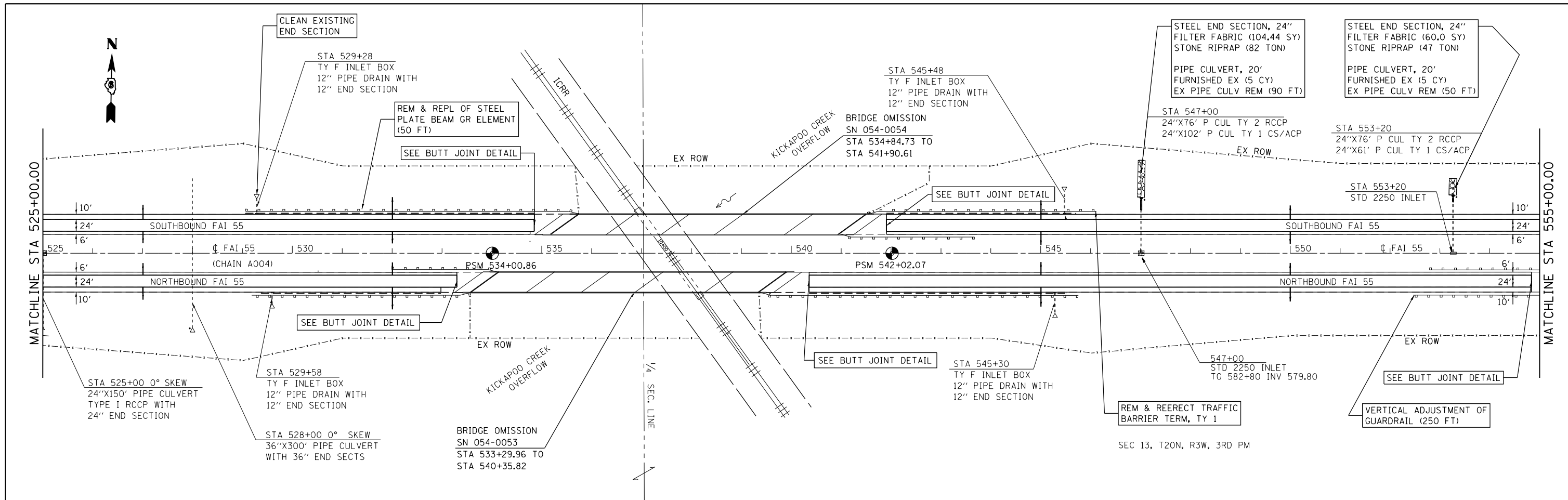
SEC 22, T20N, R3W, 3RD PM

STA 414+00 TO 416+50
 6"X250' P.U.
 IN BACKSLOPE

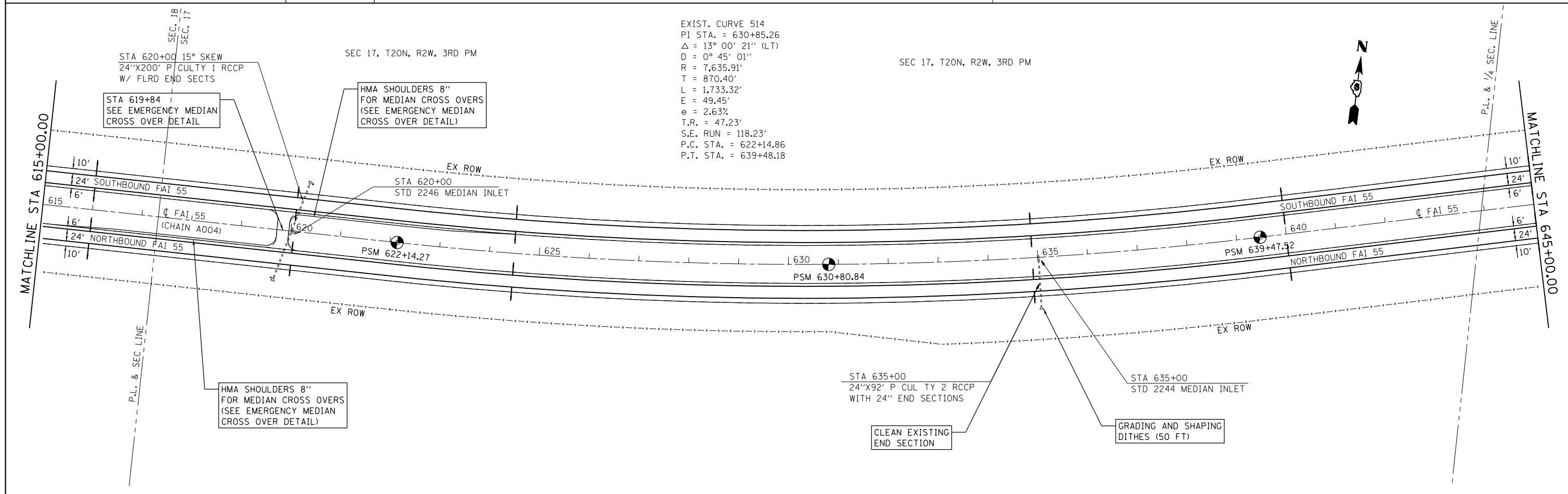
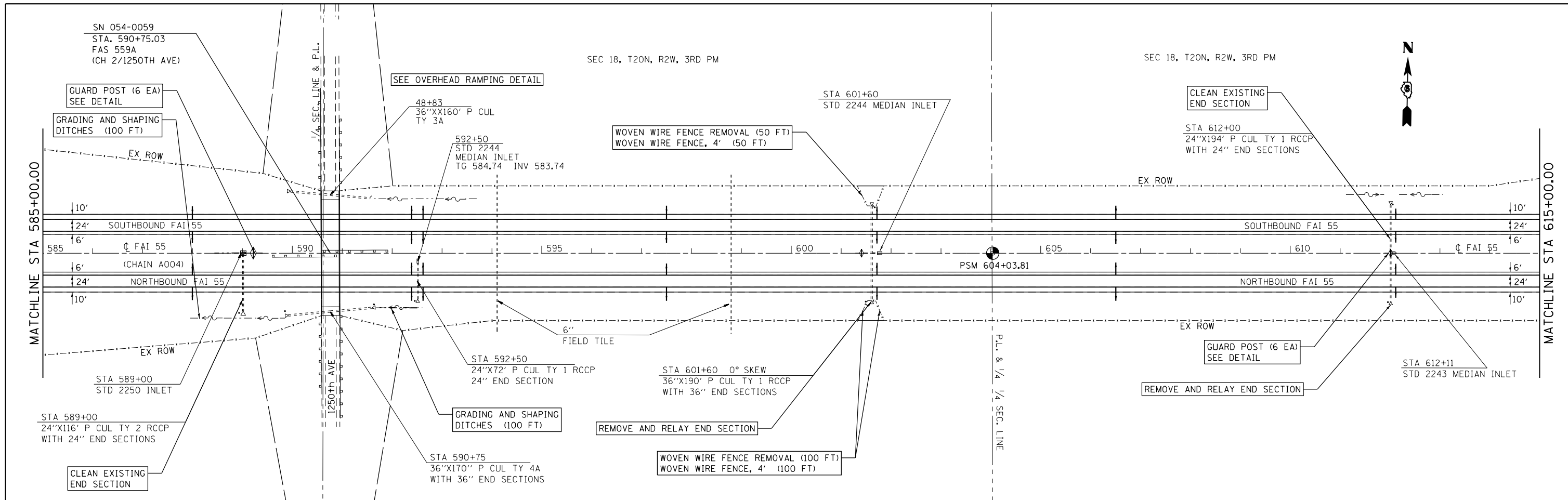
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Default	PLOT SCALE = 200.0000' / in.	DRAWN -	REVISED -		SCALE: 1" = 100'	SHEET 3	OF	SHEETS	STA.	TO STA.	LOGAN	108	54
	PLOT DATE = Jul-14-2014 03:37:14PM	CHECKED -	REVISED -								CONTRACT NO.	72D41	
		DATE -	REVISED -								ILLINOIS FED. AID PROJECT		



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Default	PLOT DATE = Jul-14-2014 03:37:19PM	DRAWN -	REVISED -				••	LOGAN	108	55	
		CHECKED -	REVISED -				CONTRACT NO. 72D41		ILLINOIS FED. AID PROJECT		
		DATE -	REVISED -				SCALE: 1" = 100'	SHEET 4 OF SHEETS	STA.	TO STA.	•• (54-4,54-5)RS-3, (54-6)RS-2

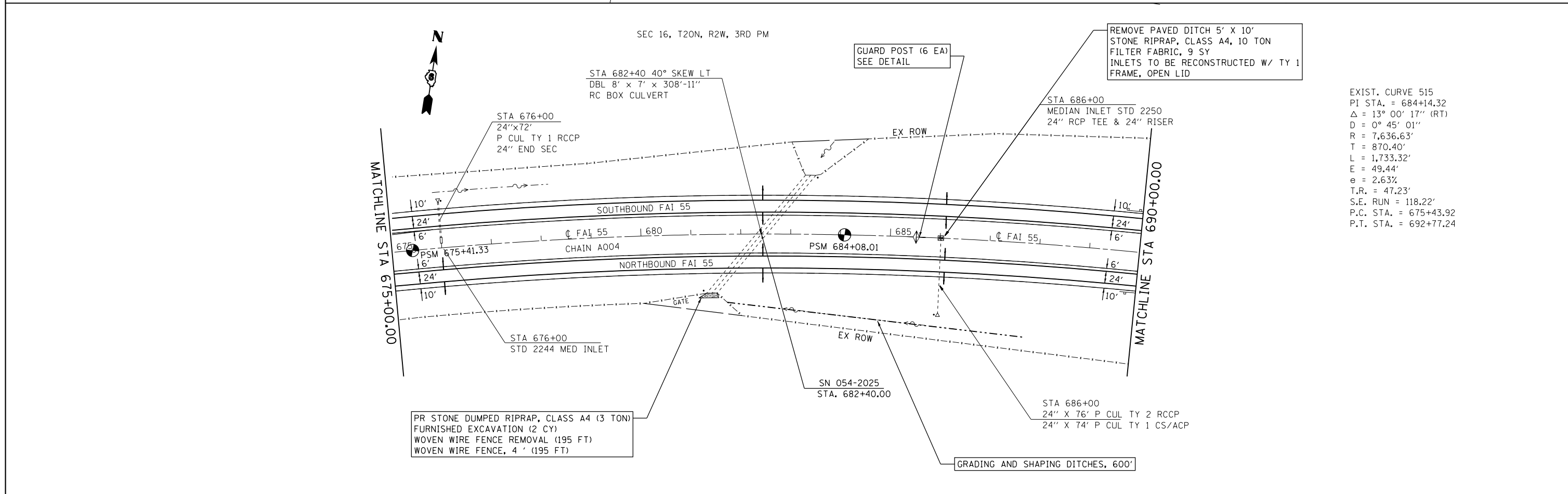
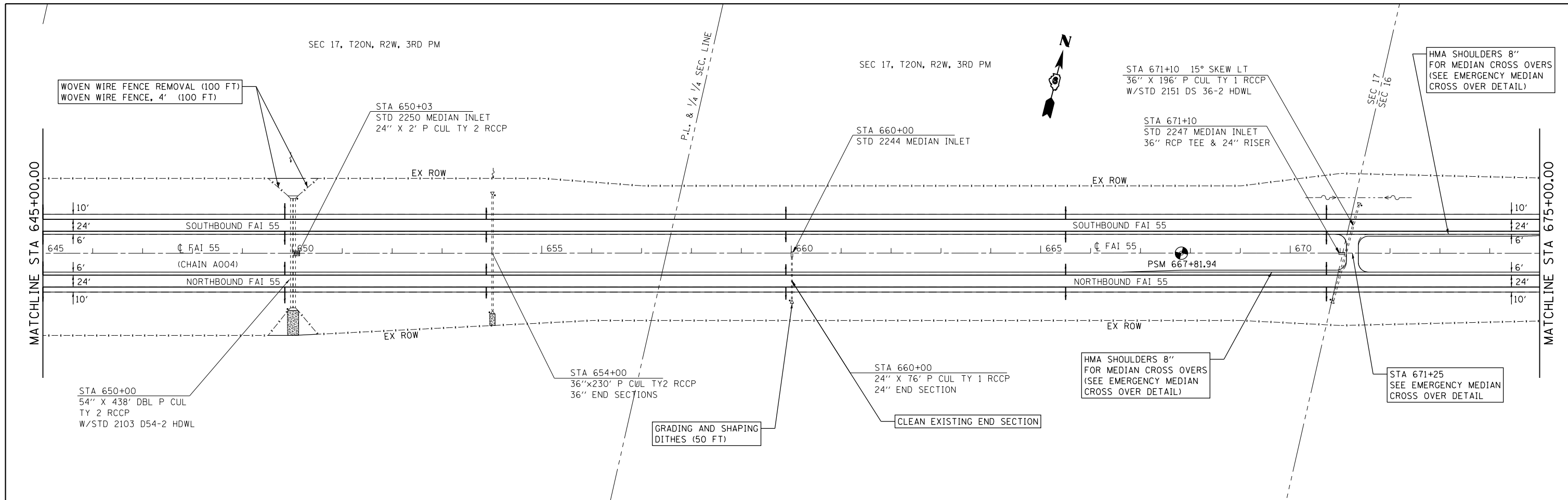


FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 PLAN SHEET		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT DATE = Jul-14-2014 03:37:19PM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



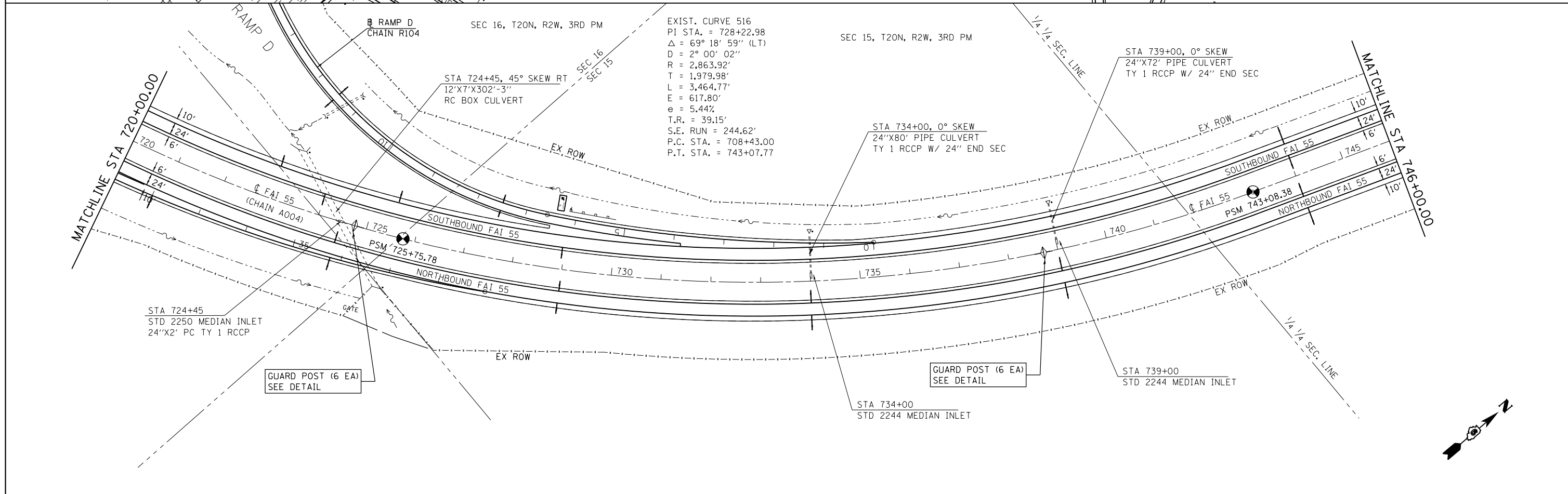
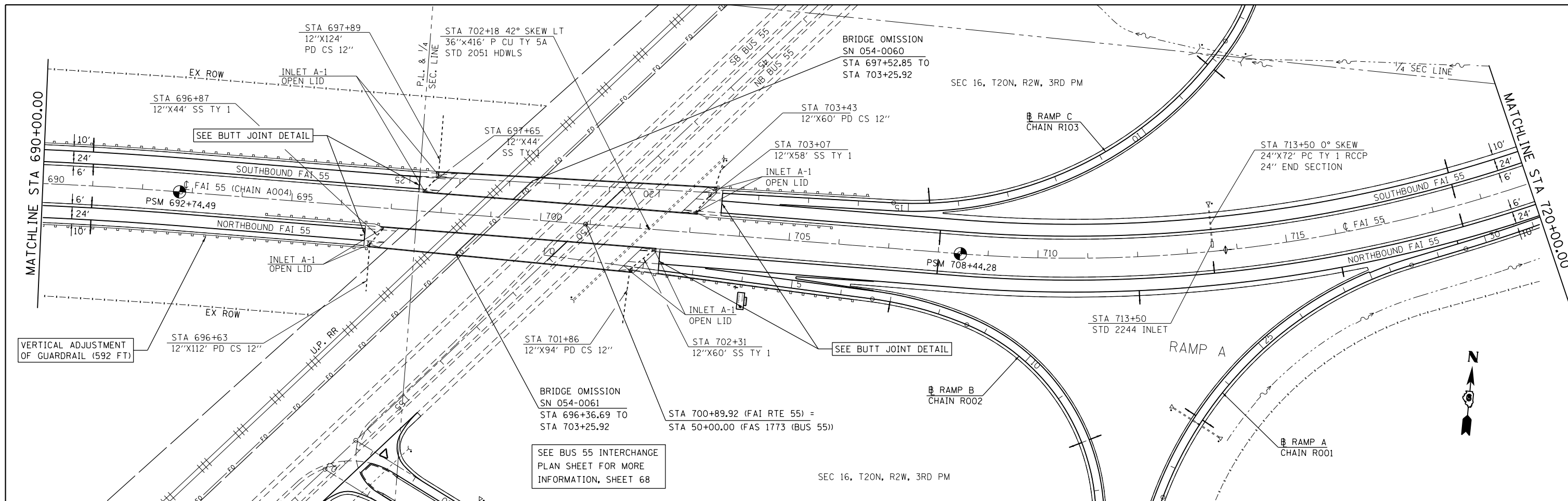
EXIST. CURVE 514
 PI STA. = 630+85.26
 $\Delta = 13^\circ 00' 21''$ (LT)
 $D = 0^\circ 45' 01''$
 $R = 7,635.91'$
 $T = 870.40'$
 $L = 1,733.32'$
 $E = 49.45'$
 $e = 2.63\%$
 $T.R. = 47.23'$
 $S.E. RUN = 118.23'$
 $P.C. STA. = 622+14.86$
 $P.T. STA. = 639+48.18$

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 PLAN SHEET		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	241-sht-plan-2.dgn	DRAWN -	REVISED -				•	••	LOGAN	108	57
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	PLOT DATE = Jul-14-2014 03:37:19PM	DATE -	REVISED -				ILLINOIS FED. AID PROJECT				

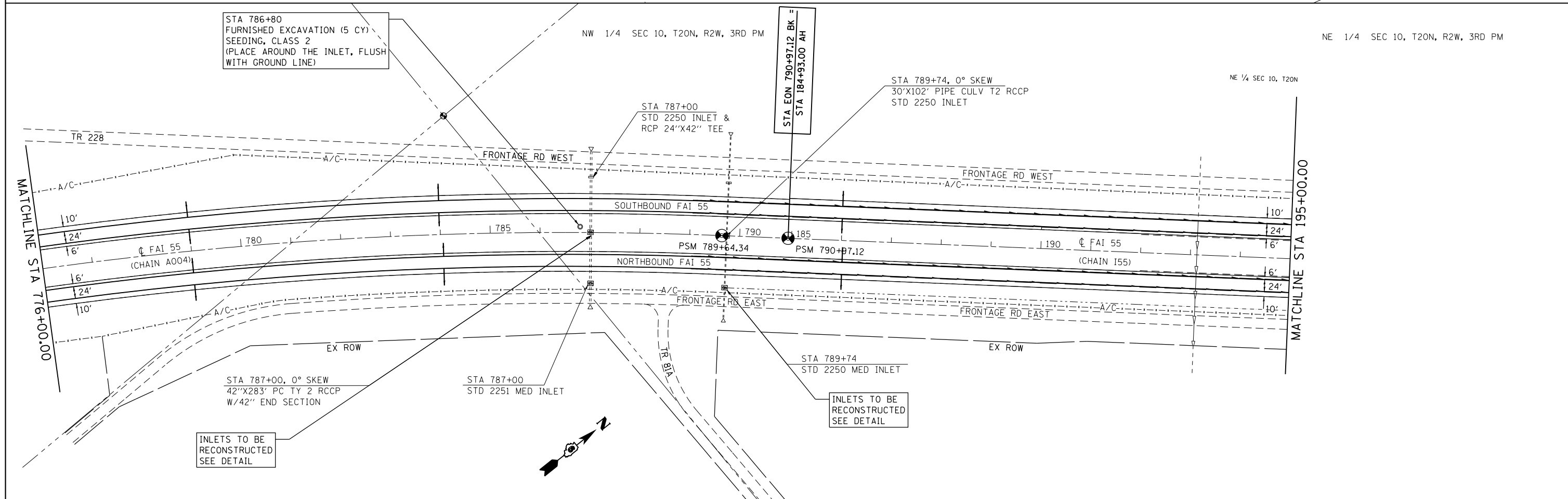
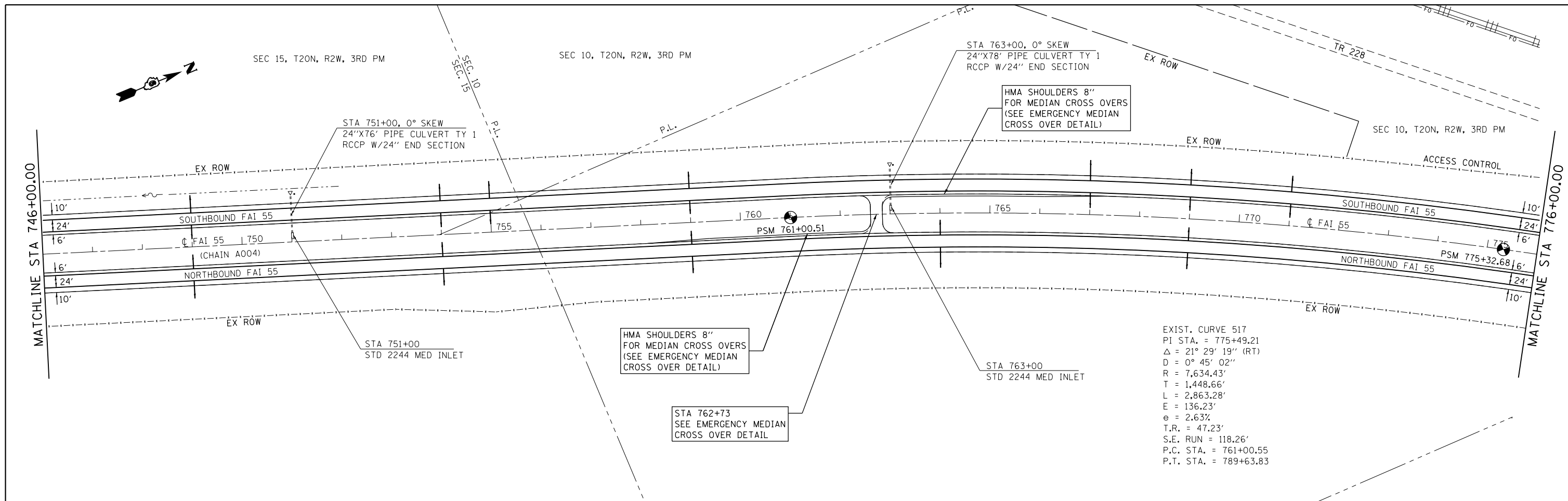


FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 PLAN SHEET			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	c:\pwork\pwork\pwork\sparksgw\0215133\0672041-sht-plan-2.dgn	DRAWN -	REVISED -		SCALE: 1" = 100'	SHEET 7	OF SHEETS	STA.	TO STA.	LOGAN	108	58
	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -							CONTRACT NO. 72D41		
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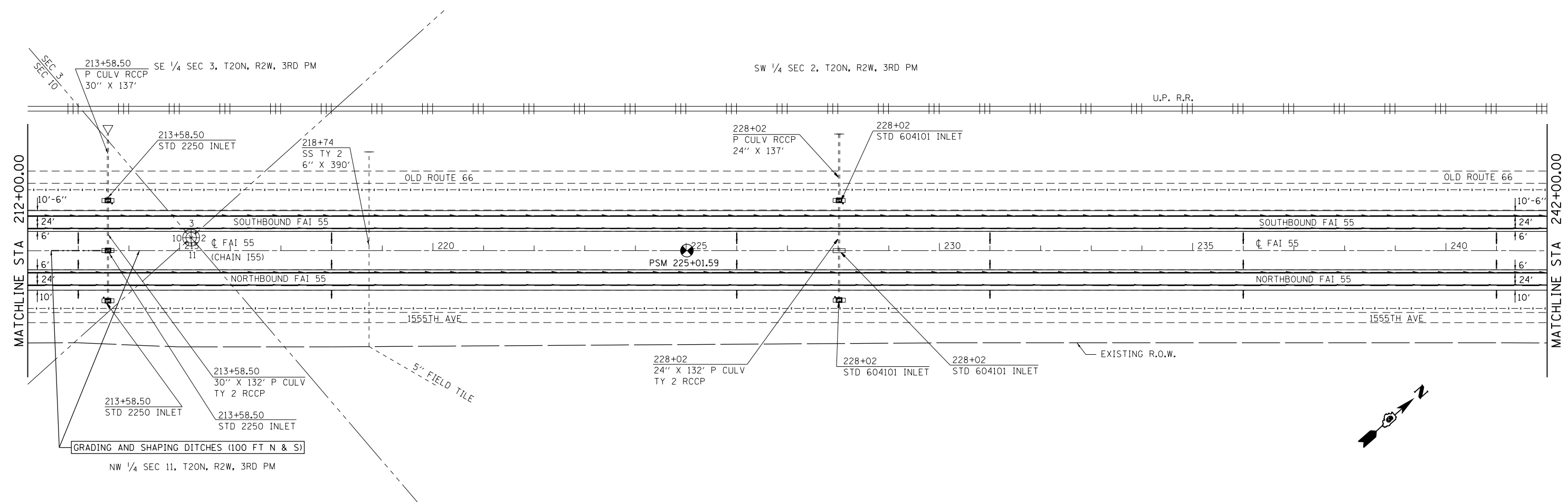
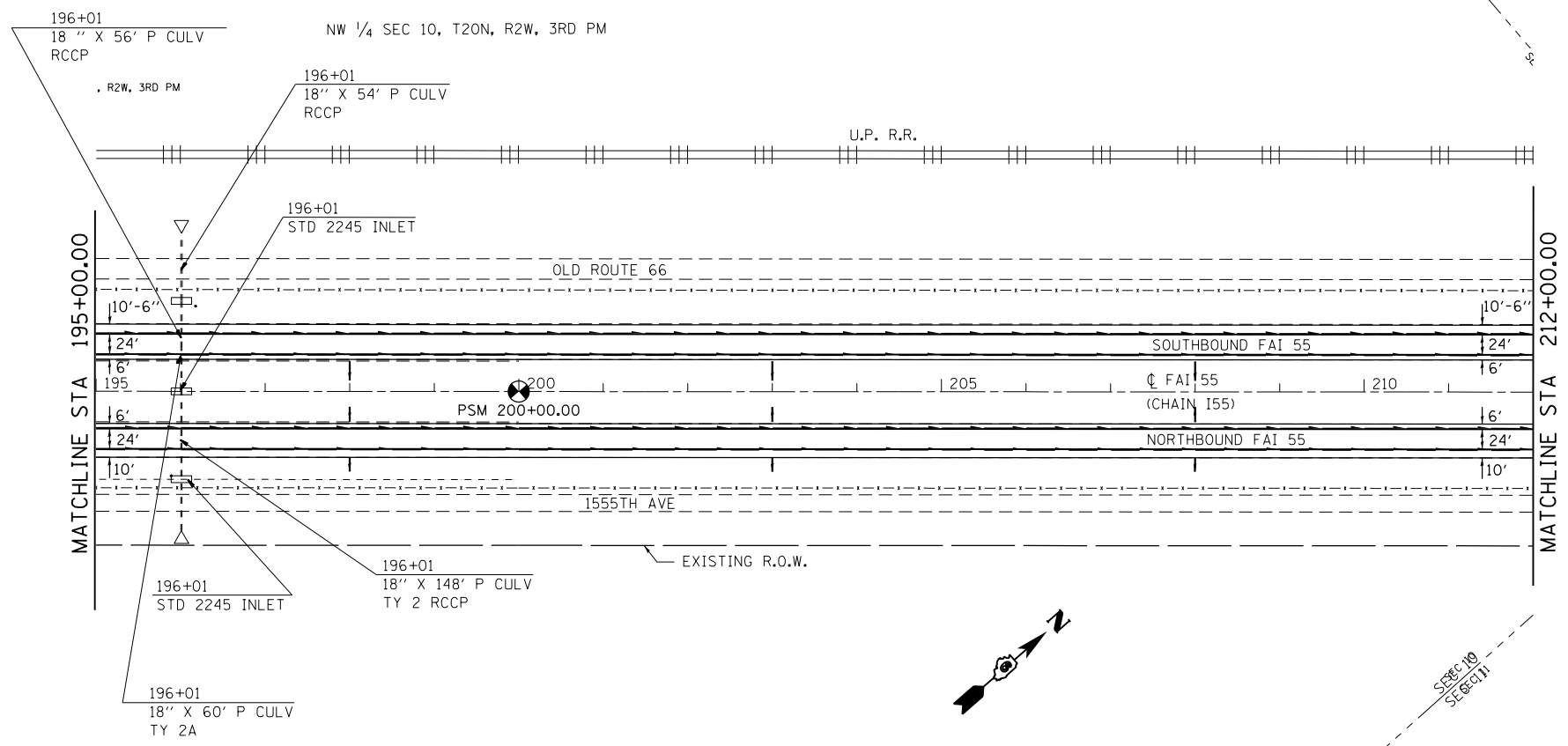
EXIST. CURVE 515
 PI STA. = 684+14.32
 $\Delta = 13^\circ 00' 17''$ (RT)
 $D = 0^\circ 45' 01''$
 $R = 7,636.63'$
 $T = 870.40'$
 $L = 1,733.32'$
 $E = 49.44'$
 $e = 2.63\%$
 $T.R. = 47.23'$
 $S.E. RUN = 118.22'$
 $P.C. STA. = 675+43.92$
 $P.T. STA. = 692+77.24$



FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 PLAN SHEET		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	Plot Date = Jul-14-2014 03:37:20PM	DRAWN -	REVISED -		SCALE: 1" = 100'	SHEET 8	OF SHEETS	STA.	TO STA.	LOGAN	108	59
		CHECKED -	REVISED -								CONTRACT NO. 72D41	
		DATE -	REVISED -								ILLINOIS FED. AID PROJECT	



FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 PLAN SHEET			F.A.I. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT DATE = Jul-14-2014 03:37:20PM	DATE -	REVISED -			SCALE: 1" = 100'	SHEET 9	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	



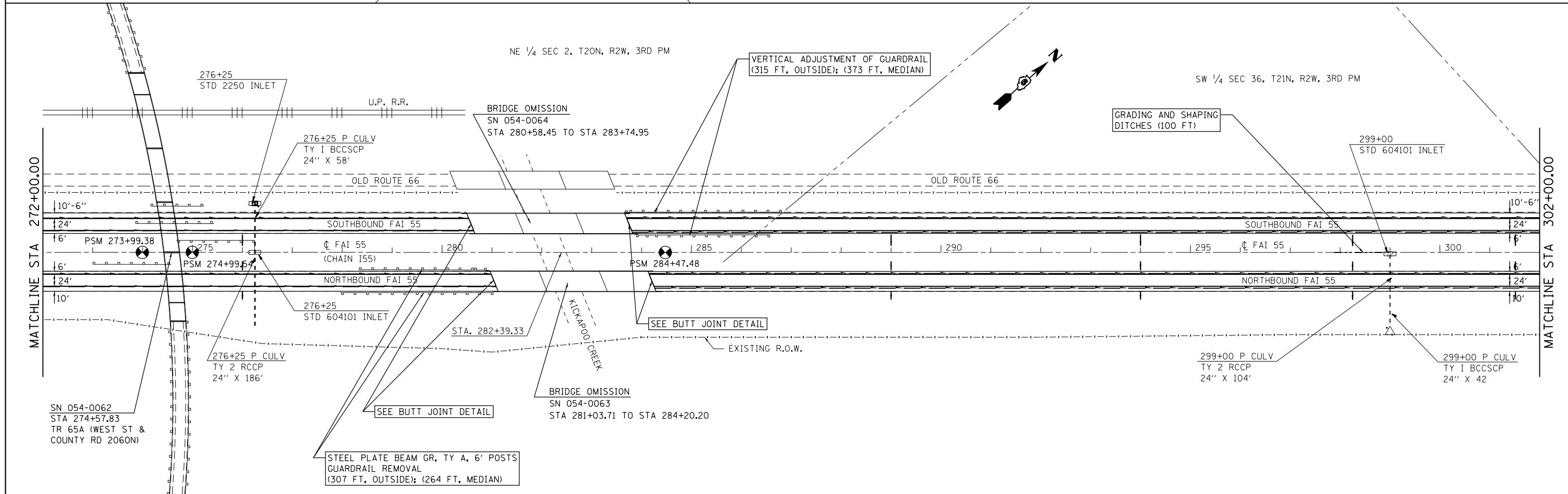
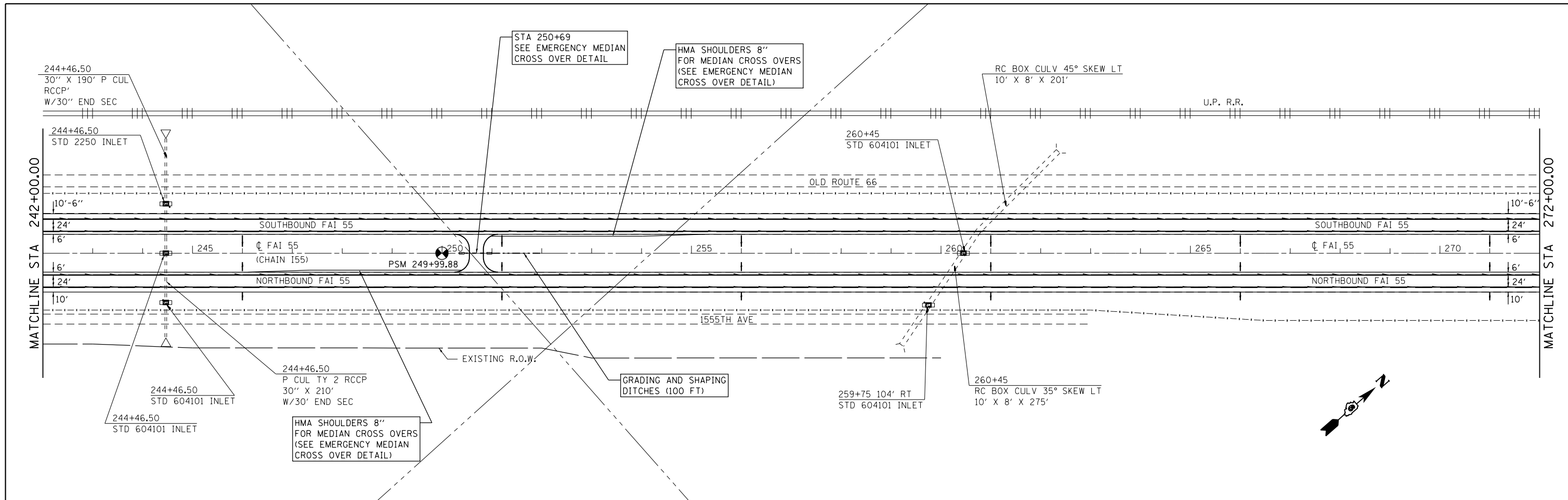
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

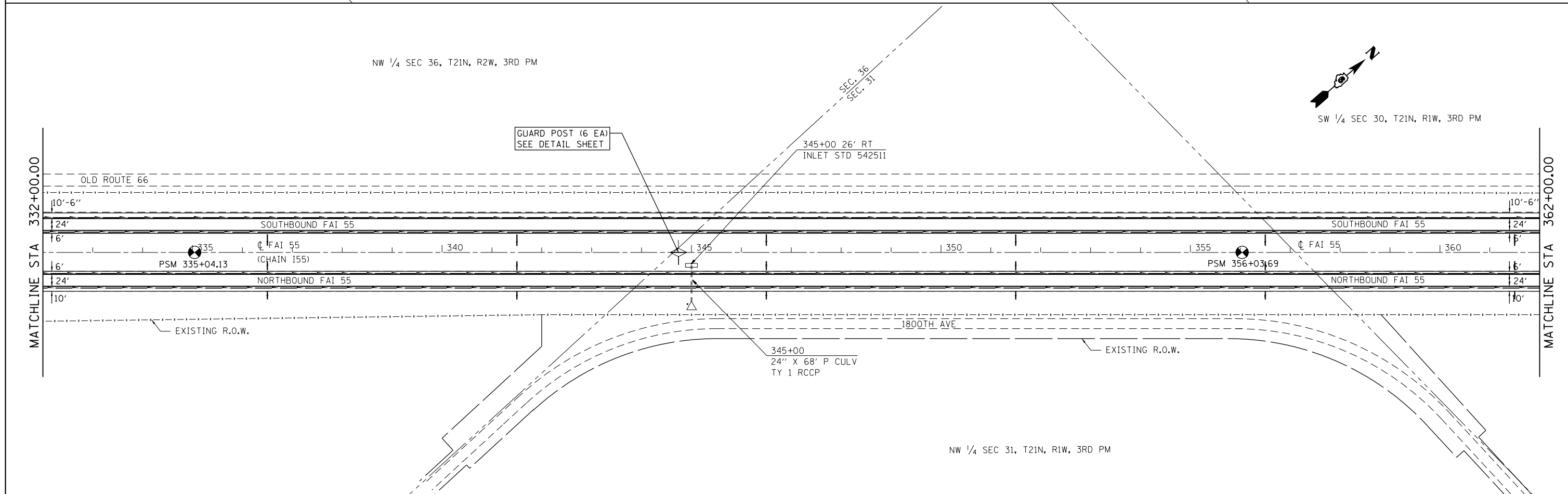
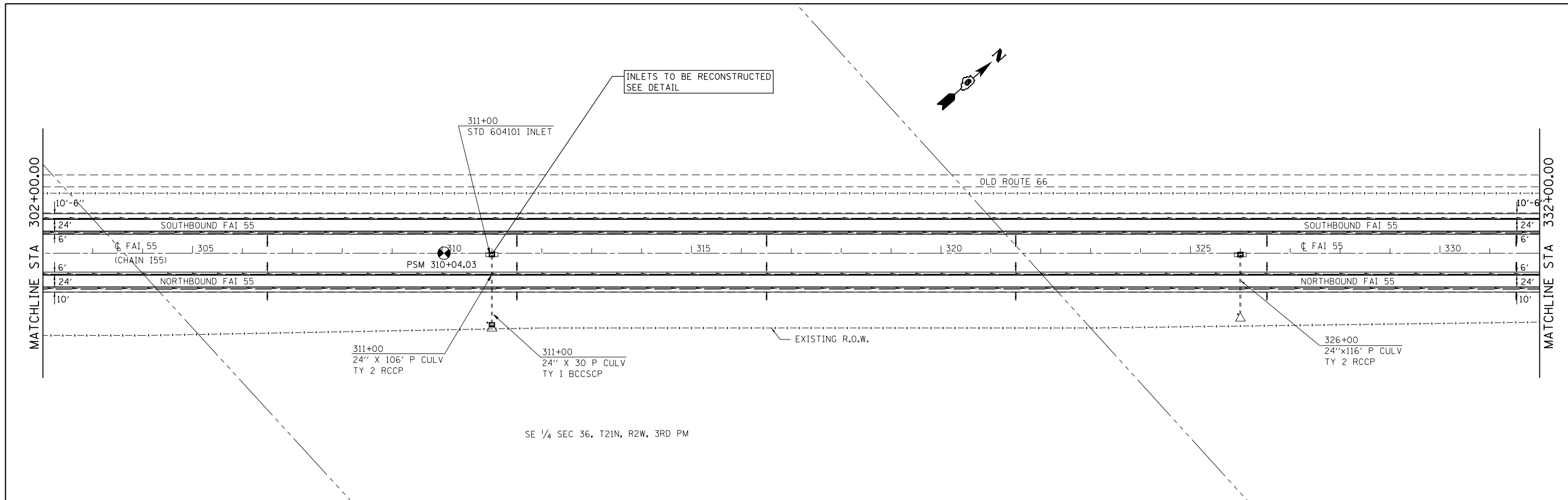
**F.A.I. ROUTE 55
PLAN SHEET**

SCALE: SHEET 10 OF SHEETS STA. TO STA.

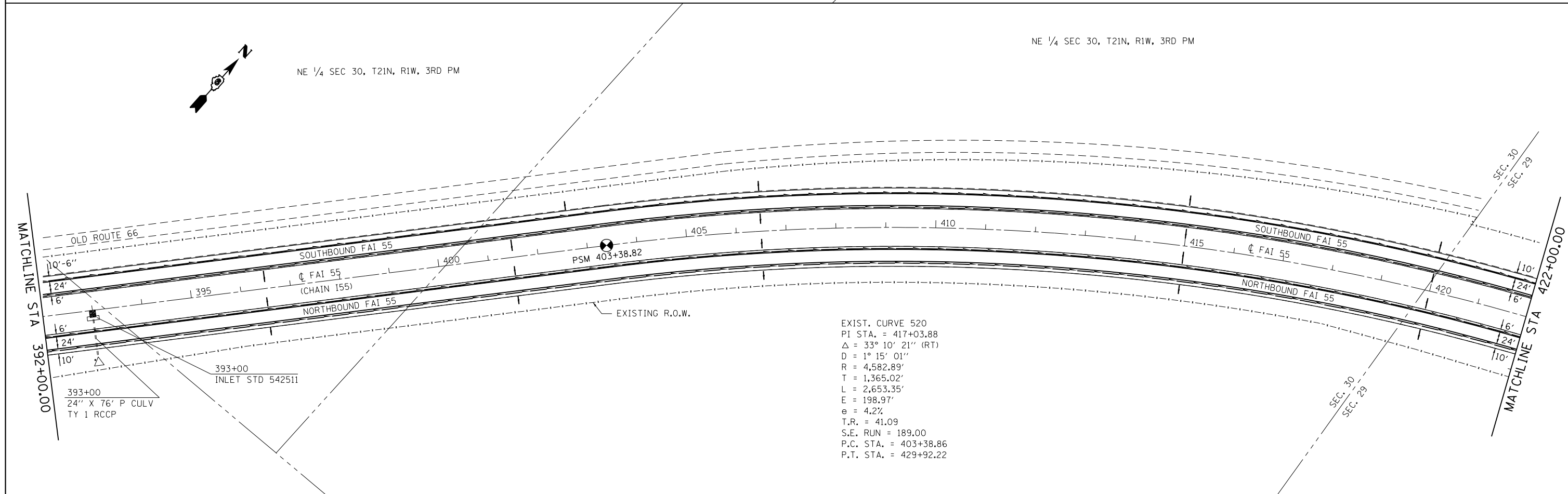
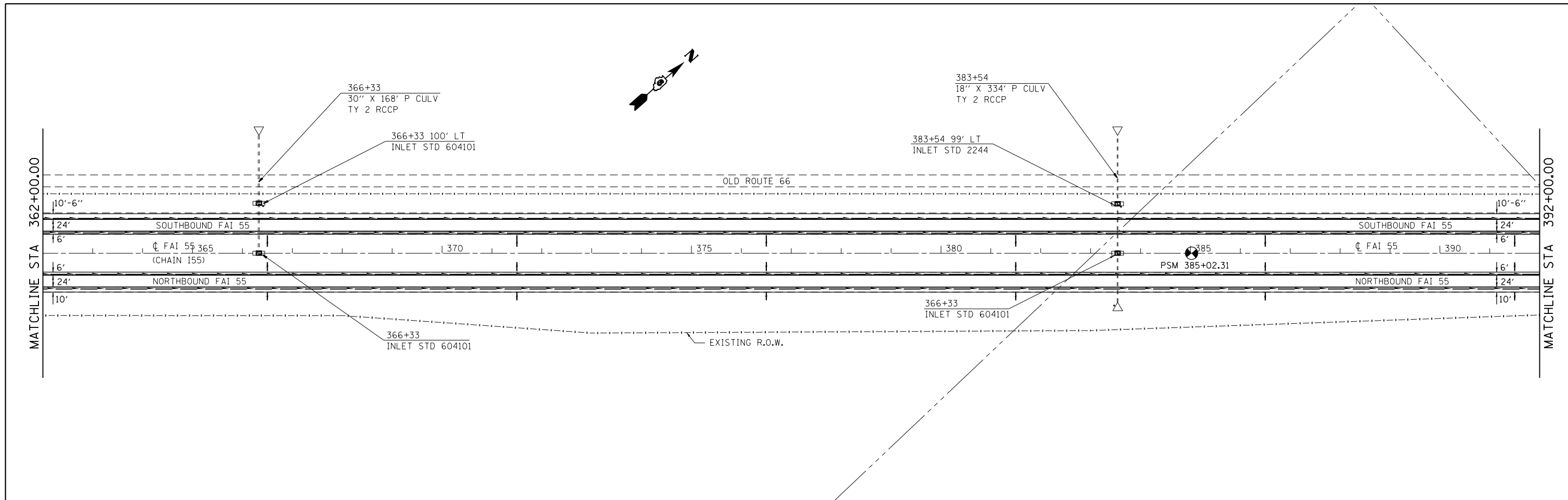
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	LOGAN	108	61
CONTRACT NO. 72D41			ILLINOIS FED. AID PROJECT	



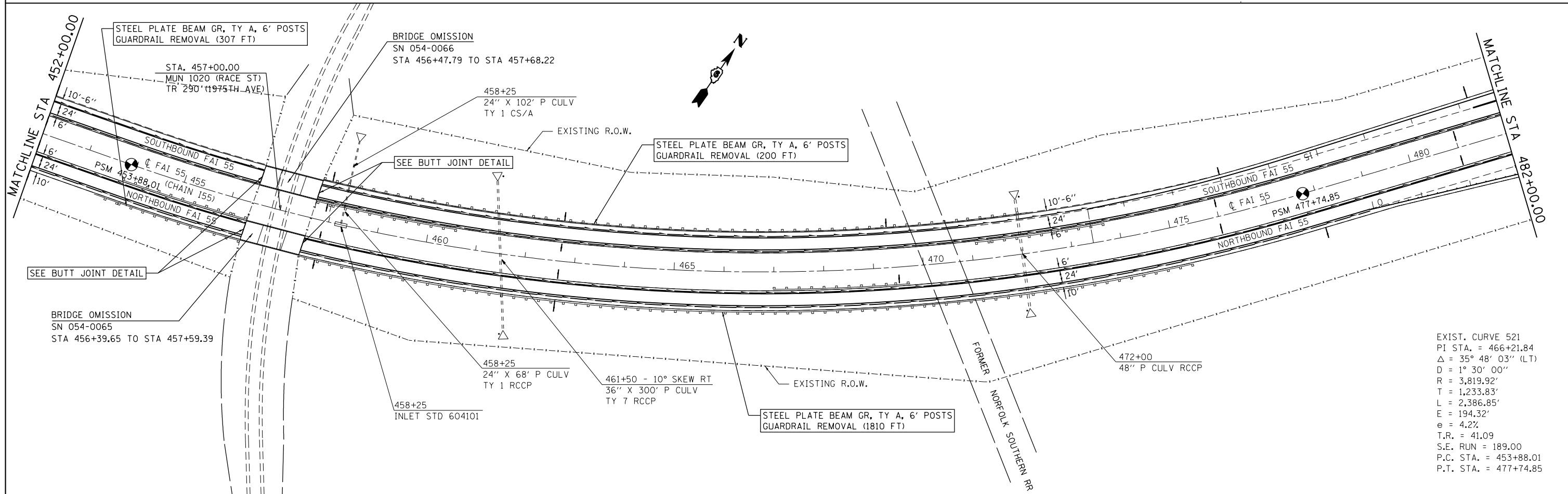
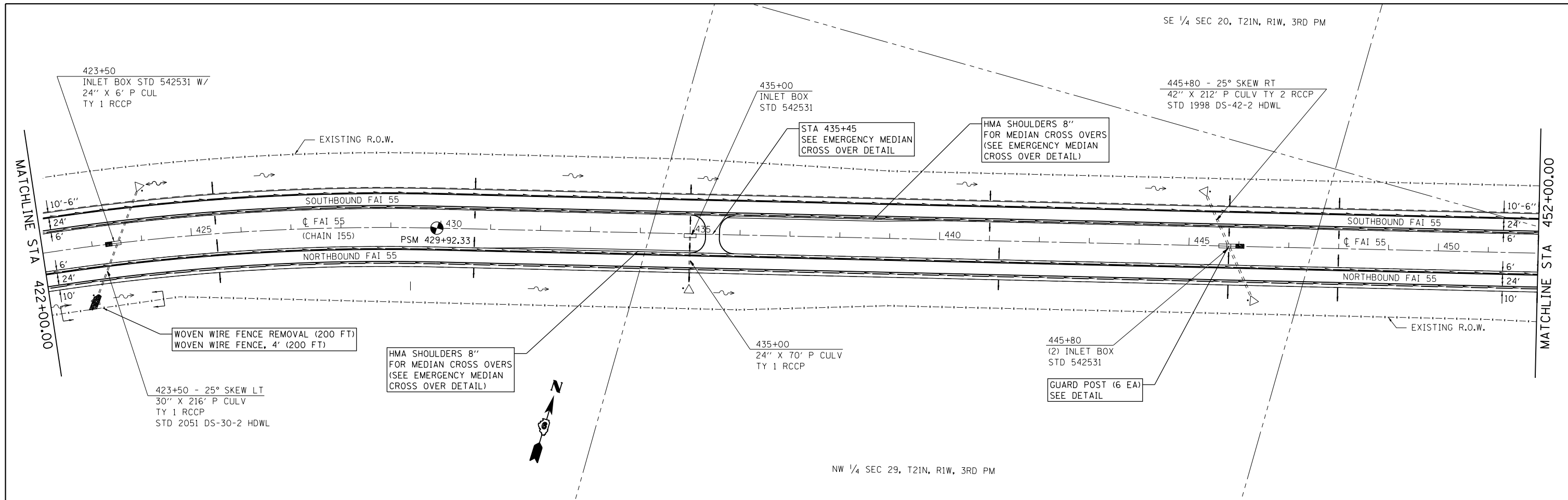
FILE NAME = c:\pwwork\p\d01\sparksgw\10215133\0672041-sh1-plan-3.dgn	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 PLAN SHEET			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	PLOT DATE = Jul-14-2014 03:37:23PM	DRAWN -	REVISED -		SCALE:	SHEET 11	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	108	62
		CHECKED -	REVISED -										
		DATE -	REVISED -										



FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 PLAN SHEET			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	ei:\pwork\pwork\pwork\sparksgw\0215133\0672041-sht-plan-3.dgn	DRAWN -	REVISED -		SCALE:	SHEET 12	OF	SHEETS	STA.	TO STA.	LOGAN	108	63
	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -							CONTRACT NO. 72D41			
	PLOT DATE = Jul-14-2014 03:37:24PM	DATE -	REVISED -							ILLINOIS FED. AID PROJECT			

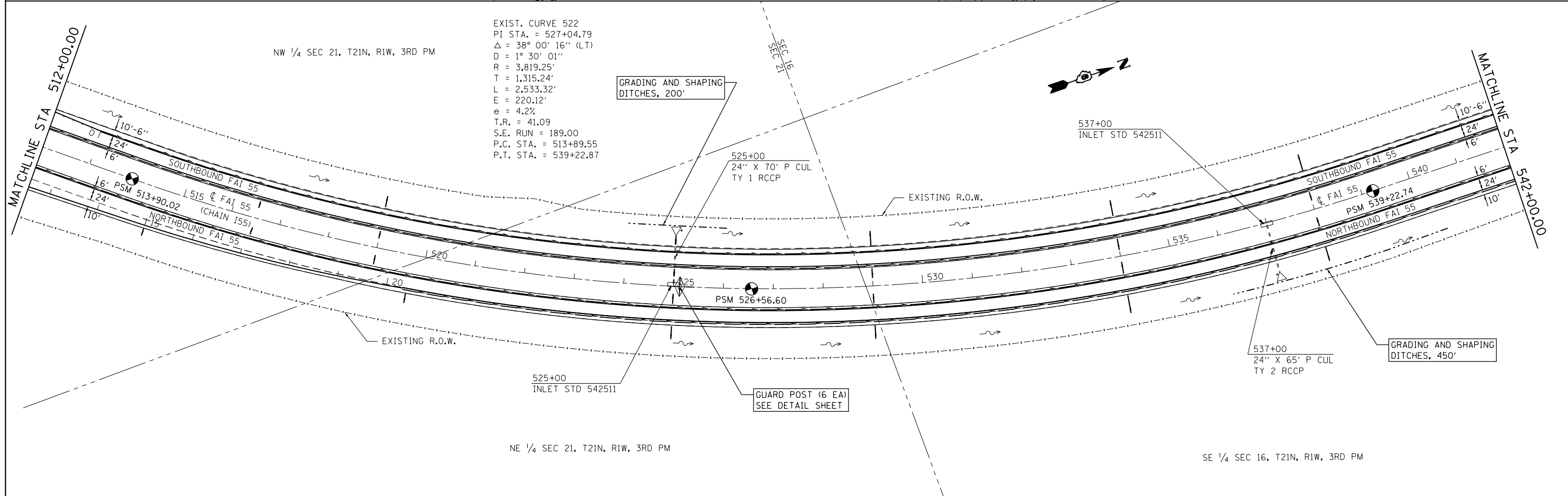
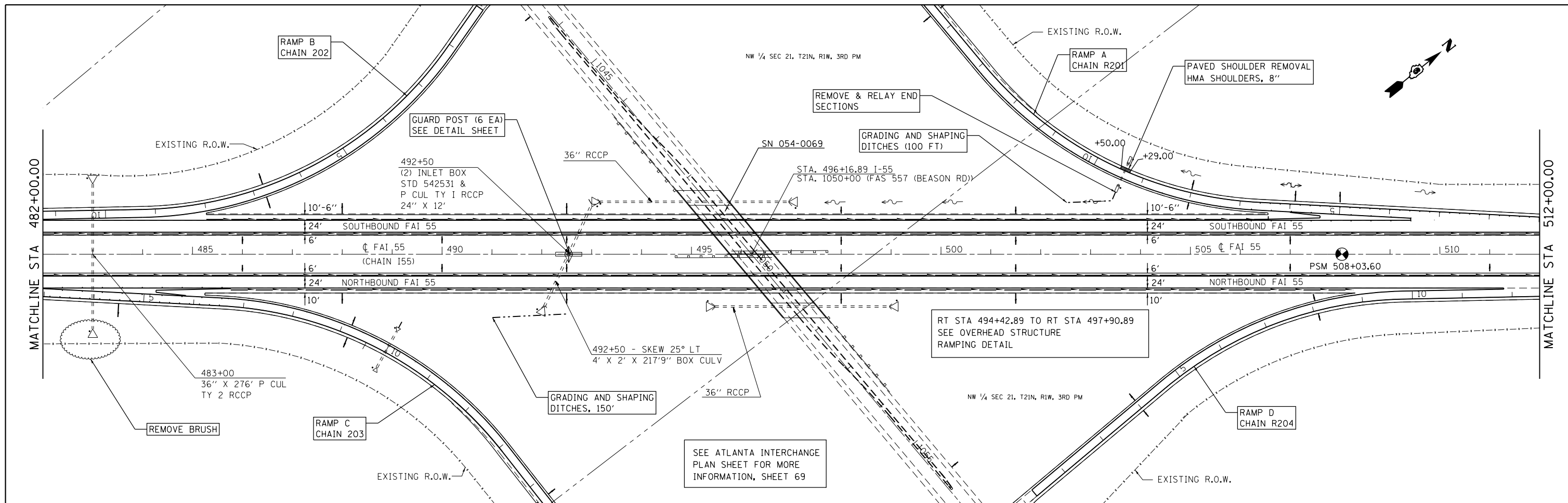


FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 PLAN SHEET		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	ei:\pwork\pilot\sparksgw\10215133\0672041-sht-plan-3.dgn	DRAWN -	REVISED -		SCALE:	SHEET 13 OF SHEETS	STA.	TO STA.	LOGAN	108	64
	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -						CONTRACT NO. 72D41		
	PLOT DATE = Jul-14-2014 03:37:24PM	DATE -	REVISED -						ILLINOIS FED. AID PROJECT		

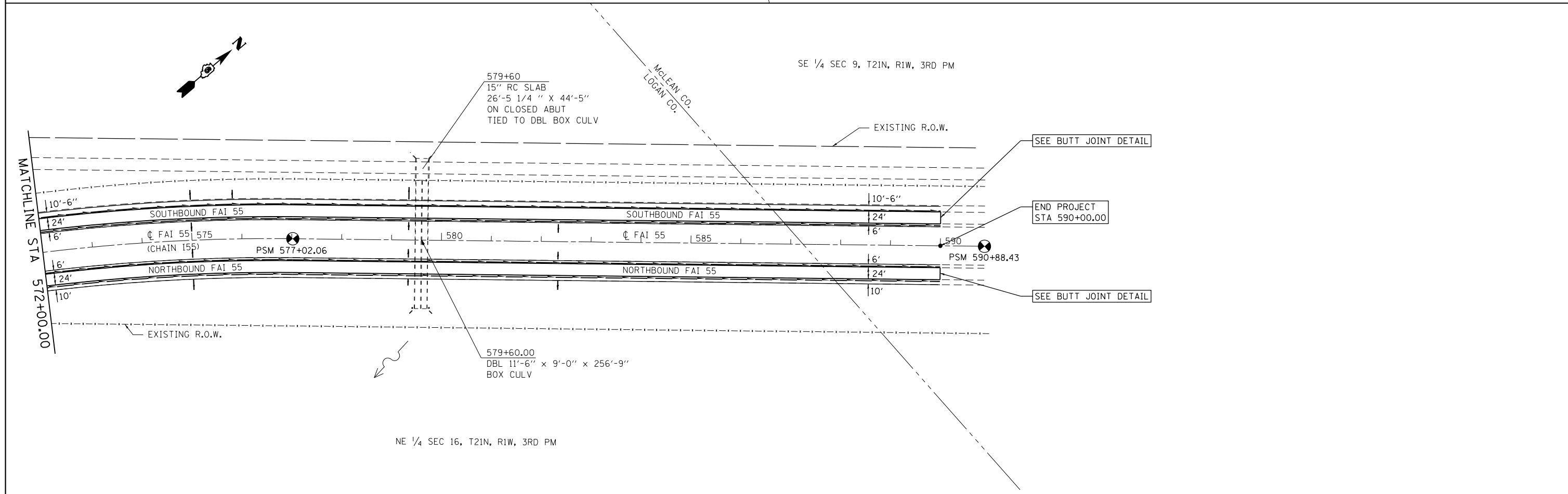
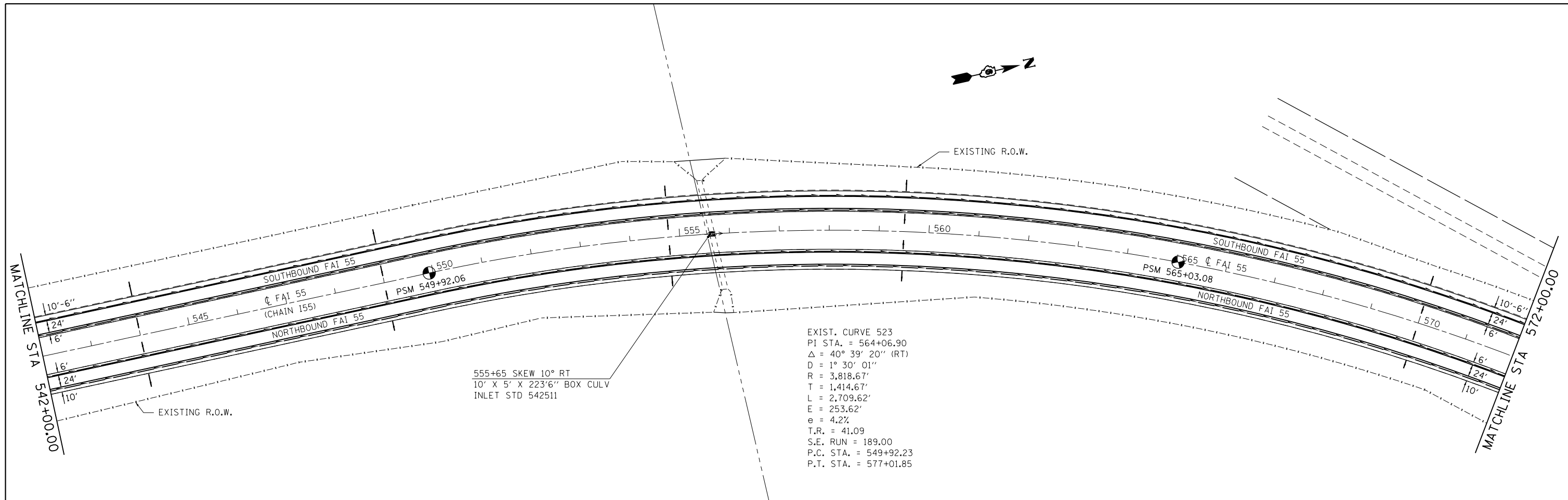


EXIST. CURVE 521
 PI STA. = 466+21.84
 $\Delta = 35^\circ 48' 03''$ (LT)
 $D = 1^\circ 30' 00''$
 $R = 3,819.92'$
 $T = 1,233.83'$
 $L = 2,386.85'$
 $E = 194.32'$
 $e = 4.2\%$
 $T.R. = 41.09$
 $S.E. RUN = 189.00$
 $P.C. STA. = 453+88.01$
 $P.T. STA. = 477+74.85$

FILE NAME = c:\pwork\pwork\dot\sparksgw\10215133\0672041-sht-plan-3.dgn	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 PLAN SHEET		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	PLOT DATE = Jul-14-2014 03:37:24PM	DRAWN -	REVISED -		SCALE:	SHEET 14	OF SHEETS	STA.	TO STA.	LOGAN	108	65
		CHECKED -	REVISED -							ILLINOIS	FED. AID PROJECT	
		DATE -	REVISED -							CONTRACT NO.	72D41	



FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 PLAN SHEET		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	ei:\pw\work\p\dot\sparksgw\10215133\0672041-sht-plan-3.dgn	DRAWN -	REVISED -		SCALE:	SHEET 15 OF SHEETS	STA.	TO STA.	LOGAN	108	66
	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 72D41						
	PLOT DATE = Jul-14-2014 03:37:25PM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT						



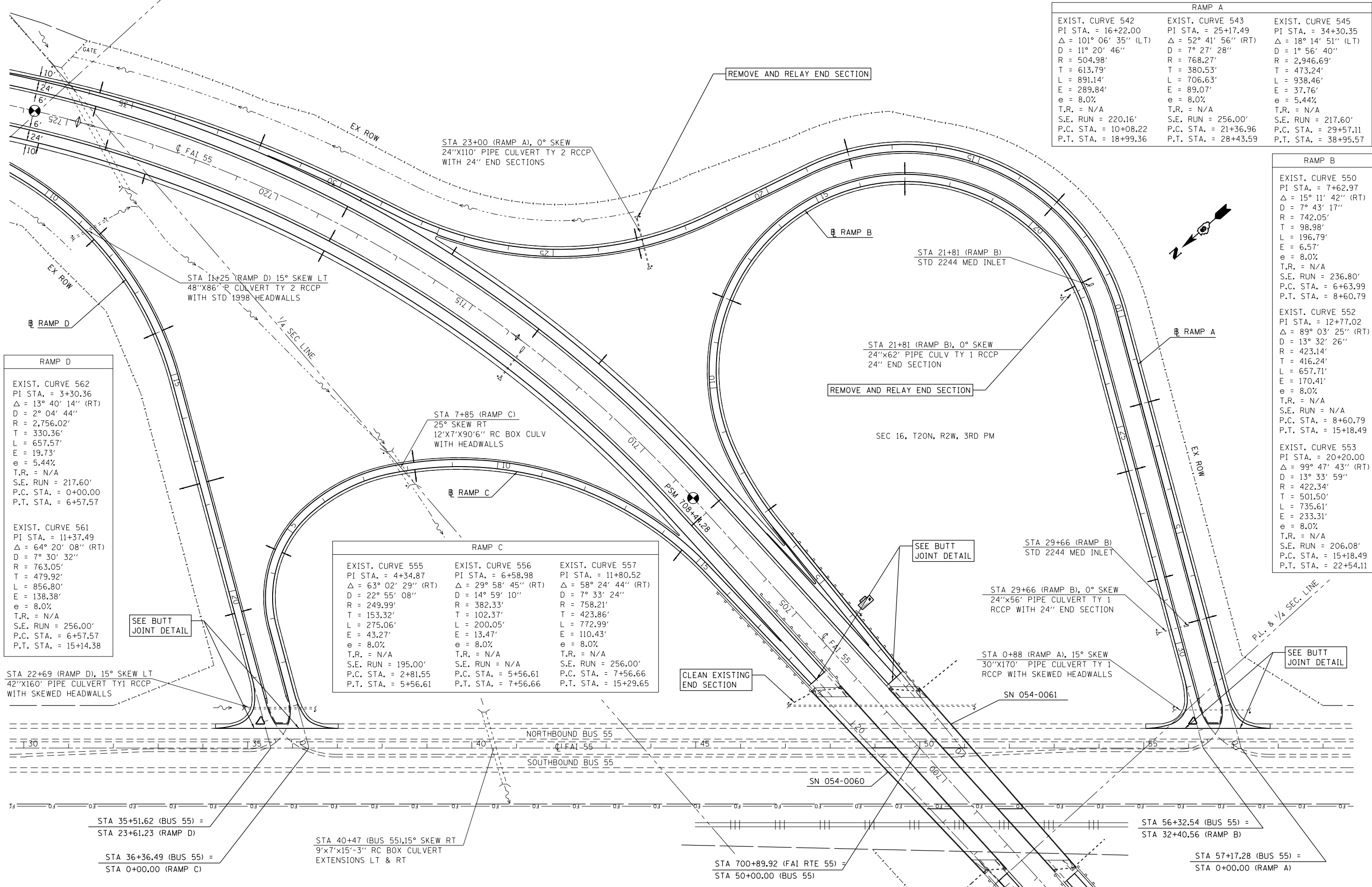
FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 PLAN SHEET			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	ei:\pwwork\pwwork\sparksgw\0215133\0672041-sht-plan-3.dgn	DRAWN -	REVISED -		SCALE:	SHEET 16	OF	SHEETS	STA.	TO STA.	LOGAN	108	67
	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -								ILLINOIS FED. AID PROJECT		
	PLOT DATE = Jul-14-2014 03:37:25PM	DATE -	REVISED -								CONTRACT NO. 72D41		

RAMP A		
EXIST. CURVE 542	EXIST. CURVE 543	EXIST. CURVE 545
PI STA. = 16+22.00	PI STA. = 25+17.49	PI STA. = 34+30.35
$\Delta = 101^\circ 06' 35''$ (LT)	$\Delta = 52^\circ 41' 56''$ (RT)	$\Delta = 18^\circ 14' 51''$ (LT)
D = 11° 20' 46"	D = 7° 27' 28"	D = 1° 56' 40"
R = 504.98'	R = 768.27'	R = 2,946.69'
T = 613.79'	T = 380.53'	T = 473.24'
L = 891.14'	L = 706.63'	L = 938.46'
E = 289.84'	E = 89.07'	E = 37.76'
e = 8.0%	e = 8.0%	e = 5.44%
T.R. = N/A	T.R. = N/A	T.R. = N/A
S.E. RUN = 220.16'	S.E. RUN = 256.00'	S.E. RUN = 217.60'
P.C. STA. = 10+08.22	P.C. STA. = 21+36.96	P.C. STA. = 29+57.11
P.T. STA. = 18+99.36	P.T. STA. = 28+43.59	P.T. STA. = 38+95.57

RAMP B		
EXIST. CURVE 550		
PI STA. = 7+62.97		
$\Delta = 15^\circ 11' 42''$ (RT)		
D = 7° 43' 17"		
R = 742.05'		
T = 98.98'		
L = 196.79'		
E = 6.57'		
e = 8.0%		
T.R. = N/A		
S.E. RUN = 236.80'		
P.C. STA. = 6+63.99		
P.T. STA. = 8+60.79		

RAMP C		
EXIST. CURVE 555	EXIST. CURVE 556	EXIST. CURVE 557
PI STA. = 4+34.87	PI STA. = 6+58.98	PI STA. = 11+80.52
$\Delta = 63^\circ 02' 29''$ (RT)	$\Delta = 29^\circ 58' 45''$ (RT)	$\Delta = 58^\circ 24' 44''$ (RT)
D = 22° 55' 08"	D = 14° 59' 10"	D = 7° 33' 24"
R = 249.99'	R = 382.33'	R = 758.21'
T = 153.32'	T = 102.37'	T = 423.86'
L = 275.06'	L = 200.05'	L = 772.99'
E = 43.27'	E = 13.47'	E = 110.43'
e = 8.0%	e = 8.0%	e = 8.0%
T.R. = N/A	T.R. = N/A	T.R. = N/A
S.E. RUN = 195.00'	S.E. RUN = N/A	S.E. RUN = 256.00'
P.C. STA. = 2+81.55	P.C. STA. = 5+56.61	P.C. STA. = 7+56.66
P.T. STA. = 5+56.61	P.T. STA. = 7+56.66	P.T. STA. = 15+29.65

RAMP D		
EXIST. CURVE 562		
PI STA. = 3+30.36		
$\Delta = 13^\circ 40' 14''$ (RT)		
D = 2° 04' 44"		
R = 2,756.02'		
T = 330.36'		
L = 657.57'		
E = 19.73'		
e = 5.44%		
T.R. = N/A		
S.E. RUN = 217.60'		
P.C. STA. = 0+00.00		
P.T. STA. = 6+57.57		



RAMP D		
EXIST. CURVE 562		
PI STA. = 3+30.36		
$\Delta = 13^\circ 40' 14''$ (RT)		
D = 2° 04' 44"		
R = 2,756.02'		
T = 330.36'		
L = 657.57'		
E = 19.73'		
e = 5.44%		
T.R. = N/A		
S.E. RUN = 217.60'		
P.C. STA. = 0+00.00		
P.T. STA. = 6+57.57		

RAMP C		
EXIST. CURVE 555	EXIST. CURVE 556	EXIST. CURVE 557
PI STA. = 4+34.87	PI STA. = 6+58.98	PI STA. = 11+80.52
$\Delta = 63^\circ 02' 29''$ (RT)	$\Delta = 29^\circ 58' 45''$ (RT)	$\Delta = 58^\circ 24' 44''$ (RT)
D = 22° 55' 08"	D = 14° 59' 10"	D = 7° 33' 24"
R = 249.99'	R = 382.33'	R = 758.21'
T = 153.32'	T = 102.37'	T = 423.86'
L = 275.06'	L = 200.05'	L = 772.99'
E = 43.27'	E = 13.47'	E = 110.43'
e = 8.0%	e = 8.0%	e = 8.0%
T.R. = N/A	T.R. = N/A	T.R. = N/A
S.E. RUN = 195.00'	S.E. RUN = N/A	S.E. RUN = 256.00'
P.C. STA. = 2+81.55	P.C. STA. = 5+56.61	P.C. STA. = 7+56.66
P.T. STA. = 5+56.61	P.T. STA. = 7+56.66	P.T. STA. = 15+29.65

EXIST. CURVE 567
 PI STA. = 9+98.75
 $\Delta = 47^\circ 11' 09''$ (RT)
 $D = 7^\circ 29' 20''$
 $R = 765.07'$
 $T = 334.14'$
 $L = 630.08'$
 $E = 69.78'$
 $e = 8.0\%$
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 6+64.61
 P.T. STA. = 12+94.69

EXIST. CURVE 566
 PI STA. = 19+02.73
 $\Delta = 75^\circ 06' 42''$ (LT)
 $D = 11^\circ 57' 55''$
 $R = 478.85'$
 $T = 368.18'$
 $L = 627.75'$
 $E = 125.18'$
 $e = 8.0\%$
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 15+34.55
 P.T. STA. = 21+62.30

EXIST. CURVE 578
 PI STA. = 17+43.66
 $\Delta = 8^\circ 43' 38''$ (LT)
 $D = 1^\circ 30' 36''$
 $R = 3,794.07'$
 $T = 289.52'$
 $L = 577.91'$
 $E = 11.03'$
 $e = 4.2\%$
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 14+54.14
 P.T. STA. = 20+32.06

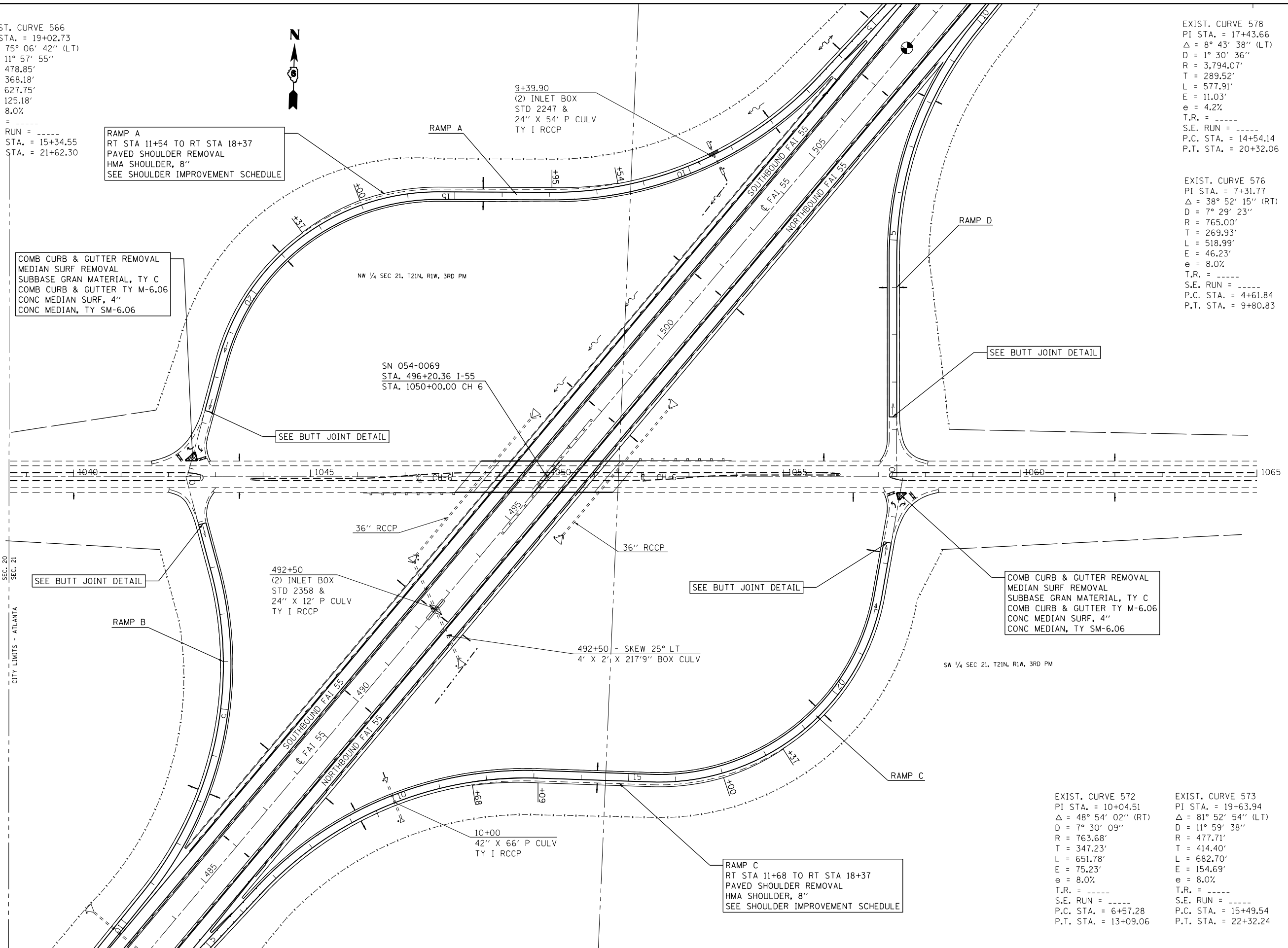
EXIST. CURVE 576
 PI STA. = 7+31.77
 $\Delta = 38^\circ 52' 15''$ (RT)
 $D = 7^\circ 29' 23''$
 $R = 765.00'$
 $T = 269.93'$
 $L = 518.99'$
 $E = 46.23'$
 $e = 8.0\%$
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 4+61.84
 P.T. STA. = 9+80.83

EXIST. CURVE 569
 PI STA. = 5+95.34
 $\Delta = 53^\circ 51' 48''$ (RT)
 $D = 7^\circ 30' 06''$
 $R = 763.78'$
 $T = 388.02'$
 $L = 718.03'$
 $E = 92.91'$
 $e = 8.0\%$
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 2+07.32
 P.T. STA. = 9+25.35

EXIST. CURVE 571
 PI STA. = 17+63.87
 $\Delta = 6^\circ 20' 39''$ (RT)
 $D = 1^\circ 30' 48''$
 $R = 3,785.95'$
 $T = 209.82'$
 $L = 419.21'$
 $E = 5.81'$
 $e = 8.0\%$
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 15+54.05
 P.T. STA. = 19+73.26

EXIST. CURVE 572
 PI STA. = 10+04.51
 $\Delta = 48^\circ 54' 02''$ (RT)
 $D = 7^\circ 30' 09''$
 $R = 763.68'$
 $T = 347.23'$
 $L = 651.78'$
 $E = 75.23'$
 $e = 8.0\%$
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 6+57.28
 P.T. STA. = 13+09.06

EXIST. CURVE 573
 PI STA. = 19+63.94
 $\Delta = 81^\circ 52' 54''$ (LT)
 $D = 11^\circ 59' 38''$
 $R = 477.71'$
 $T = 414.40'$
 $L = 682.70'$
 $E = 154.69'$
 $e = 8.0\%$
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 15+49.54
 P.T. STA. = 22+32.24



RAMP A
 RT STA 11+54 TO RT STA 18+37
 PAVED SHOULDER REMOVAL
 HMA SHOULDER, 8"
 SEE SHOULDER IMPROVEMENT SCHEDULE

COMB CURB & GUTTER REMOVAL
 MEDIAN SURF REMOVAL
 SUBBASE GRAN MATERIAL, TY C
 COMB CURB & GUTTER TY M-6.06
 CONC MEDIAN SURF, 4"
 CONC MEDIAN, TY SM-6.06

SEE BUTT JOINT DETAIL

SEE BUTT JOINT DETAIL

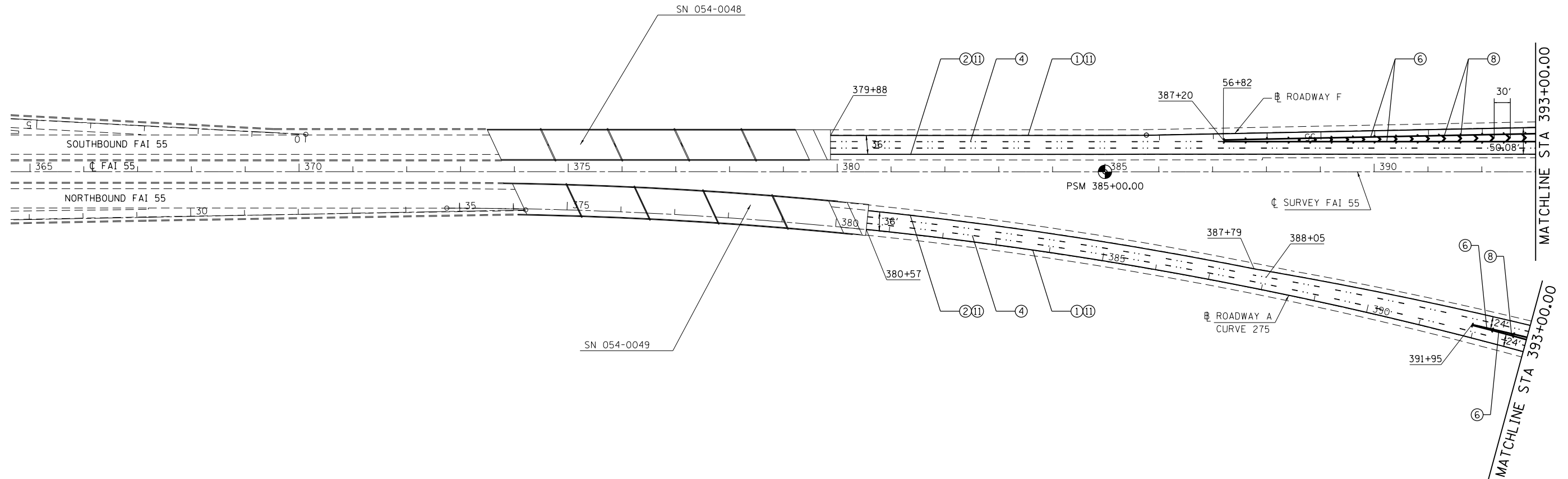
SEE BUTT JOINT DETAIL

SEE BUTT JOINT DETAIL

COMB CURB & GUTTER REMOVAL
 MEDIAN SURF REMOVAL
 SUBBASE GRAN MATERIAL, TY C
 COMB CURB & GUTTER TY M-6.06
 CONC MEDIAN SURF, 4"
 CONC MEDIAN, TY SM-6.06

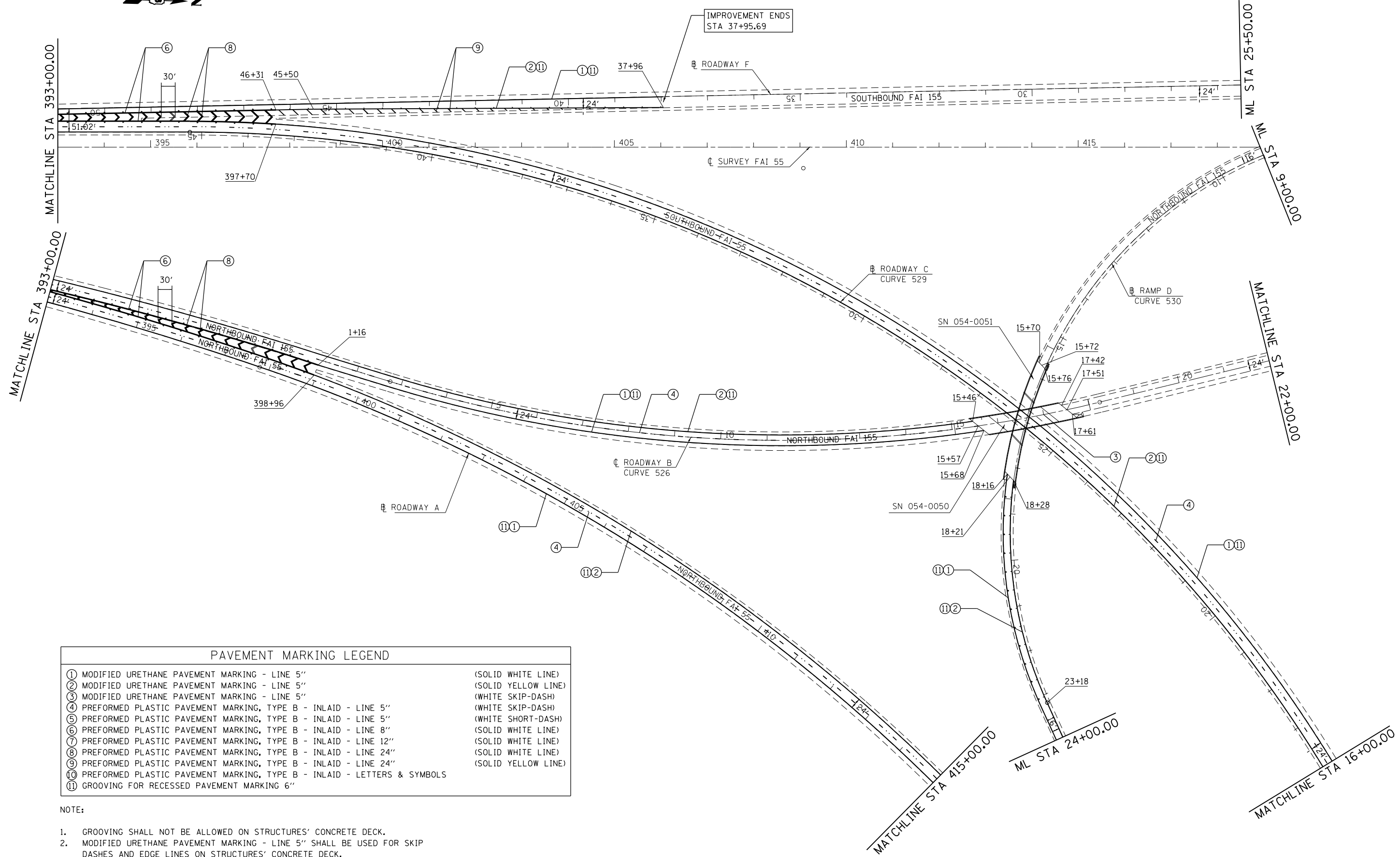
RAMP C
 RT STA 11+68 TO RT STA 18+37
 PAVED SHOULDER REMOVAL
 HMA SHOULDER, 8"
 SEE SHOULDER IMPROVEMENT SCHEDULE

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				F.A.I. ROUTE 55 (ATLANTA (CH #6) INTERCHANGE) PLAN SHEET				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.						
Default	2041-sht-plan-5.dgn	DRAWN -	REVISED -																			
	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -																			
	PLOT DATE = Jul-14-2014 03:37:34PM	DATE -	REVISED -																			
				SCALE:				SHEET 18 OF SHEETS STA. TO STA.				ILLINOIS FED. AID PROJECT										



PAVEMENT MARKING LEGEND	
① MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
② MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③ MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
⑧ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID WHITE LINE)
⑨ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID WHITE LINE)
⑩ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	(SOLID YELLOW LINE)
⑪ GROOVING FOR RECESSED PAVEMENT MARKING 6"	

- NOTE:
- GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
 - MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.



IMPROVEMENT ENDS
STA 37+95.69

PAVEMENT MARKING LEGEND	
①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5" (SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5" (SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5" (WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5" (WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5" (WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8" (SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12" (SOLID WHITE LINE)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24" (SOLID WHITE LINE)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24" (SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS (SOLID YELLOW LINE)
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"

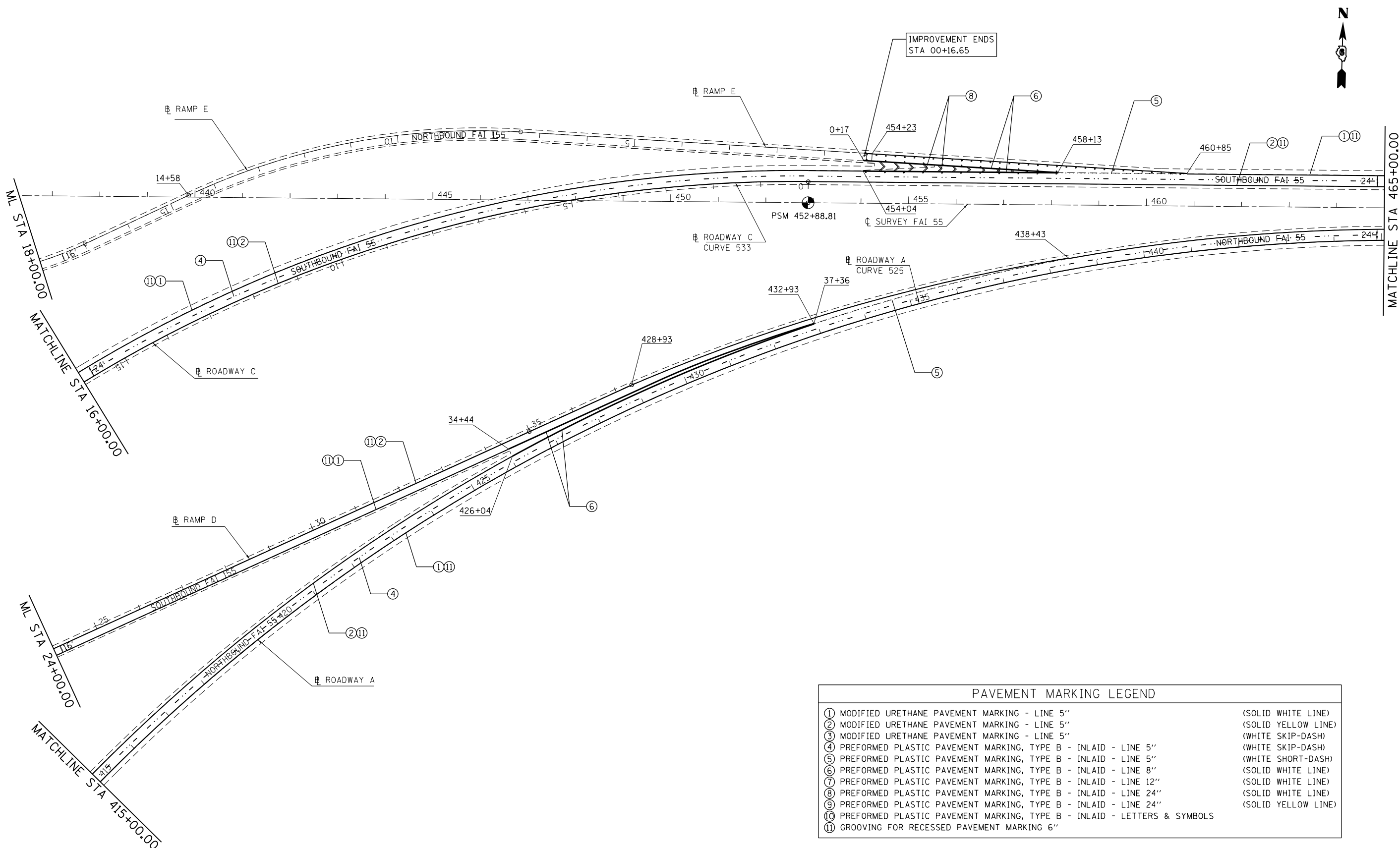
- NOTE:
- GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
 - MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
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Default	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Jul-14-2014 03:37:37PM	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

F.A.I. ROUTE 55 PAVEMENT MARKING PLAN SHEET			
SCALE: 1" = 100'	SHEET 2	OF SHEETS	STA. TO STA.

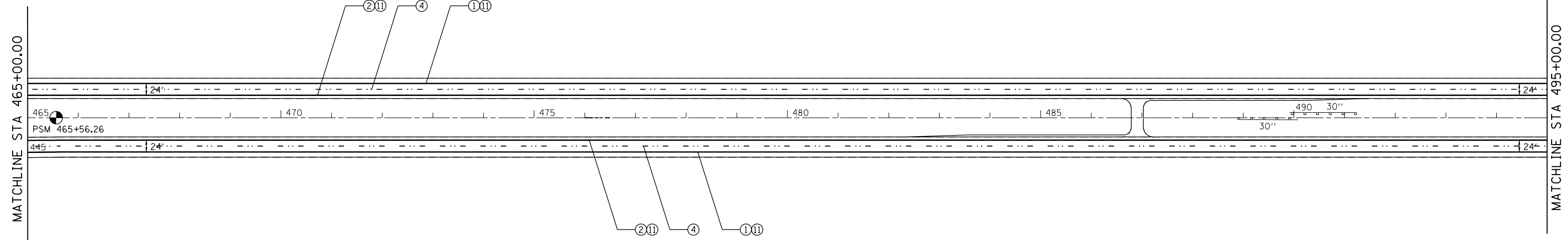
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	LOGAN	108	71
ILLINOIS FED. AID PROJECT			CONTRACT NO.	72D41



PAVEMENT MARKING LEGEND		
①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID WHITE LINE)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	(SOLID YELLOW LINE)
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"	

- NOTE:
- GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
 - MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.

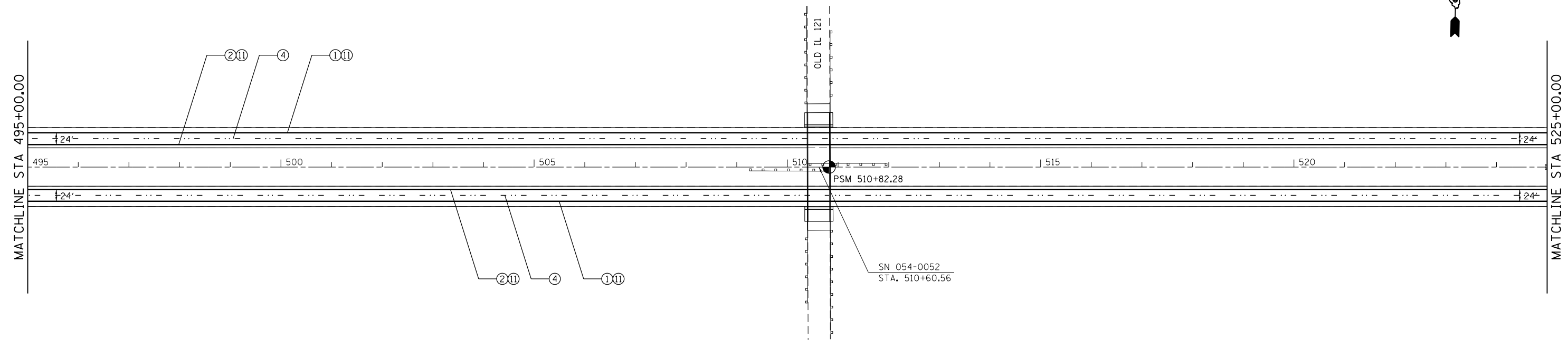
FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.I. ROUTE 55 PAVEMENT MARKING PLAN SHEET			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	Default	DRAWN -	REVISED -		SCALE: 1" = 100'	SHEET 3	OF	SHEETS	STA.	TO	LOGAN	108	72
		CHECKED -	REVISED -								CONTRACT NO.	72D41	
		DATE -	REVISED -								ILLINOIS FED. AID PROJECT		



NOTE:

- GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
- MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.

PAVEMENT MARKING LEGEND		
①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID WHITE LINE)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"	



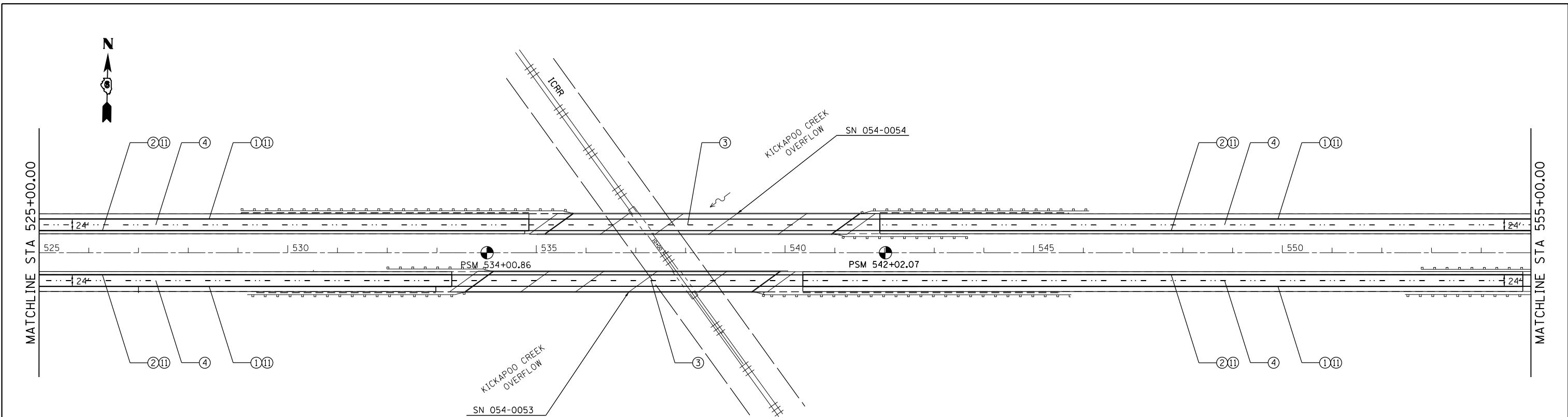
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	PLOT DATE = Jul-14-2014 03:37:41PM	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**F.A.I. ROUTE 55
PAVEMENT MARKING PLAN SHEET**

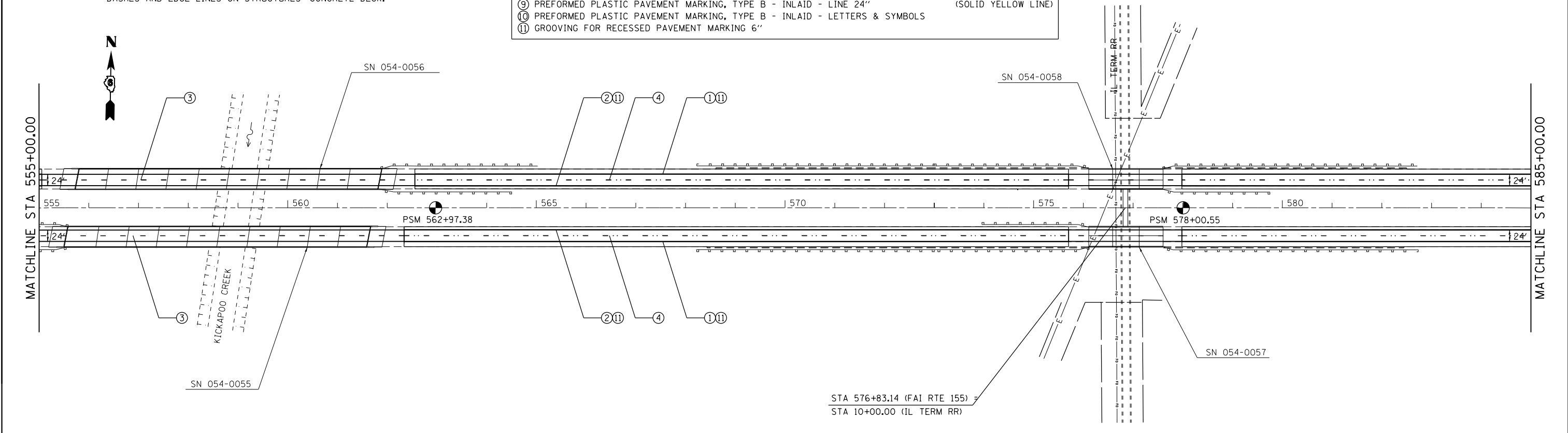
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	LOGAN	108	73
CONTRACT NO. 72D41			ILLINOIS FED. AID PROJECT	

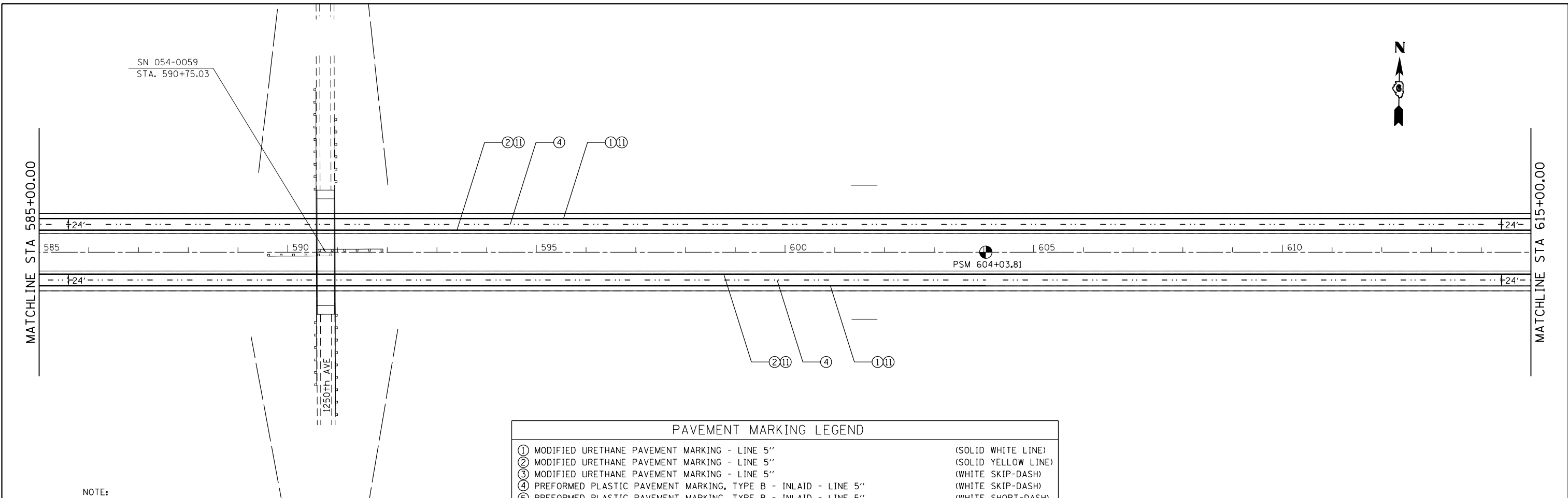


- NOTE:
- GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
 - MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.

PAVEMENT MARKING LEGEND		
①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID WHITE LINE)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	(SOLID YELLOW LINE)
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"	



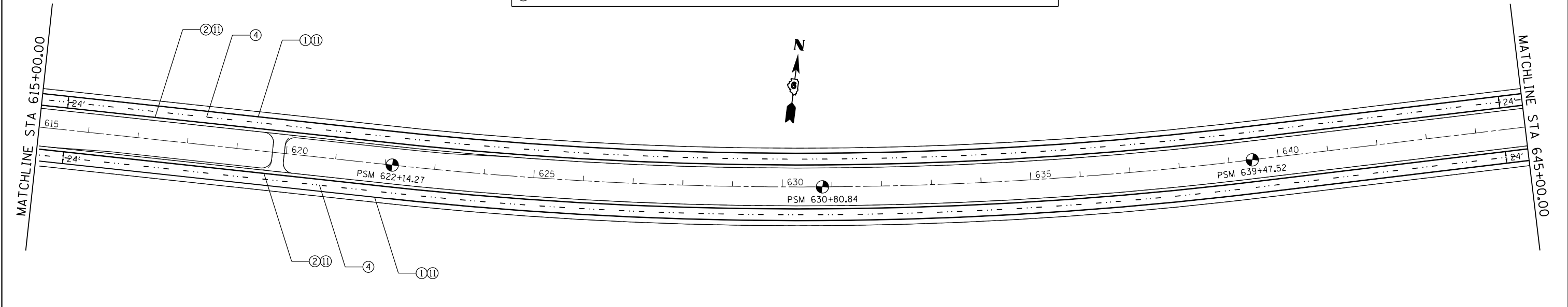
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	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -									CONTRACT NO.	72D41	
	PLOT DATE = Jul-14-2014 03:37:41PM	DATE -	REVISED -									ILLINOIS FED. AID PROJECT		



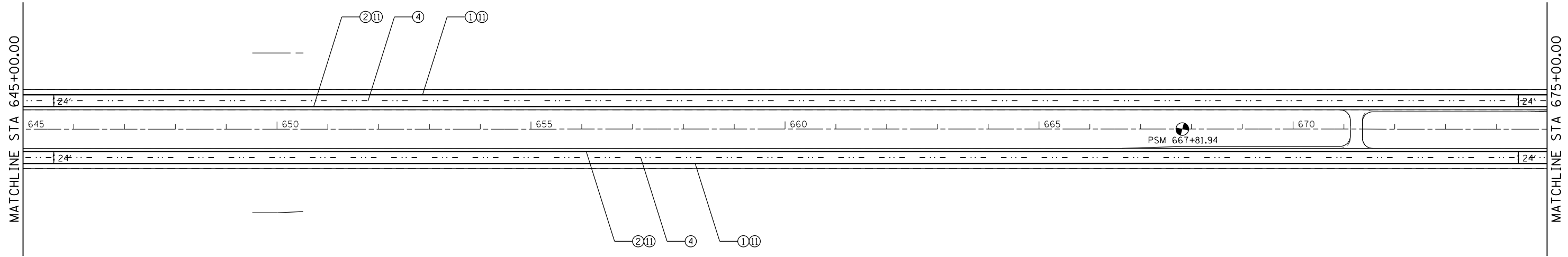
NOTE:

- GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
- MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.

PAVEMENT MARKING LEGEND		
①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID WHITE LINE)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"	



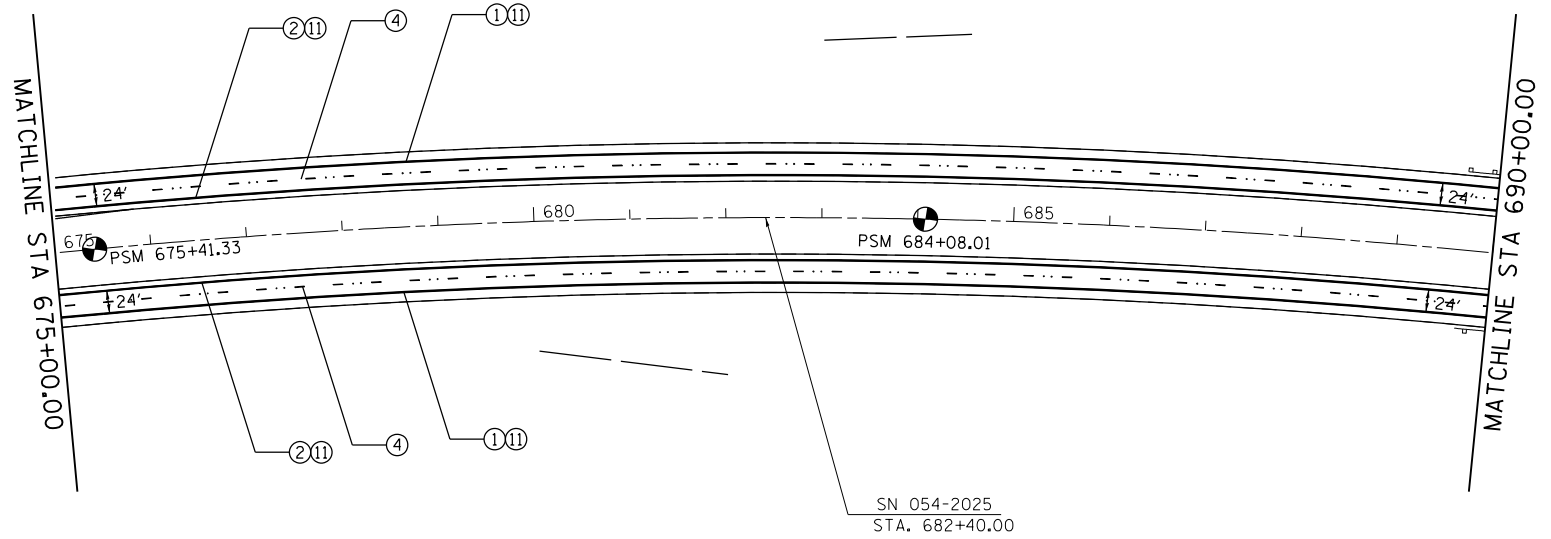
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	PLOT DATE = Jul-14-2014 03:37:41PM	DATE -	REVISED -								ILLINOIS FED. AID PROJECT		



NOTE:

1. GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
2. MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.

PAVEMENT MARKING LEGEND		
①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID WHITE LINE)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"	



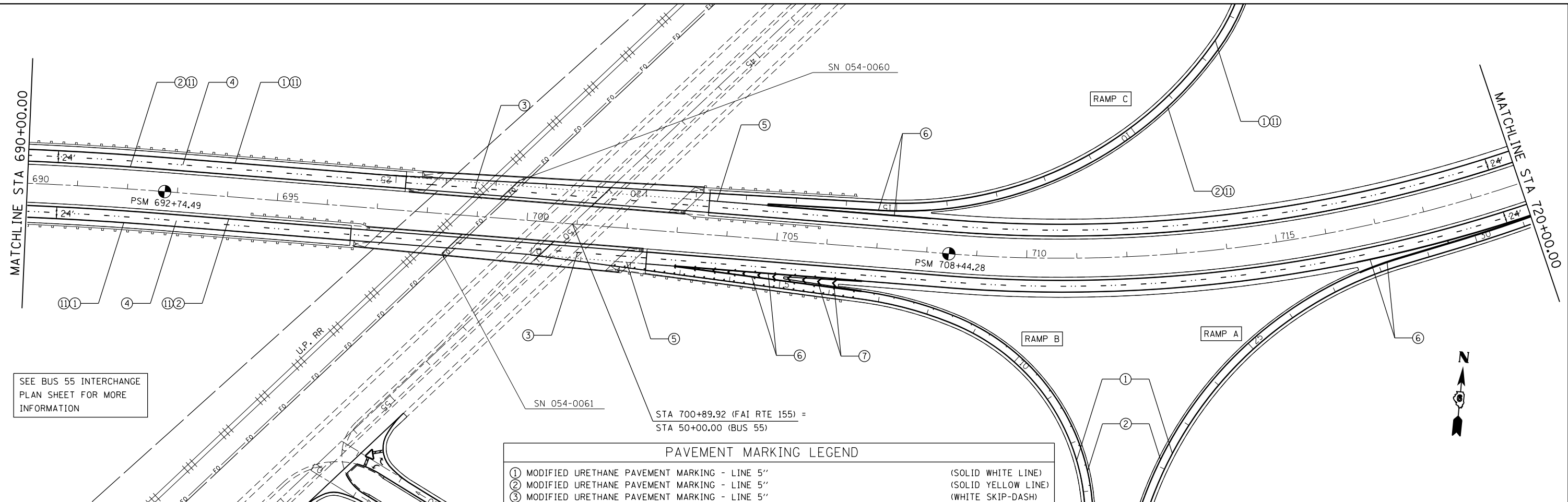
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**F.A.I. ROUTE 55
PAVEMENT MARKING PLAN SHEET**

SCALE: 1" = 100' SHEET 7 OF SHEETS STA. TO STA.

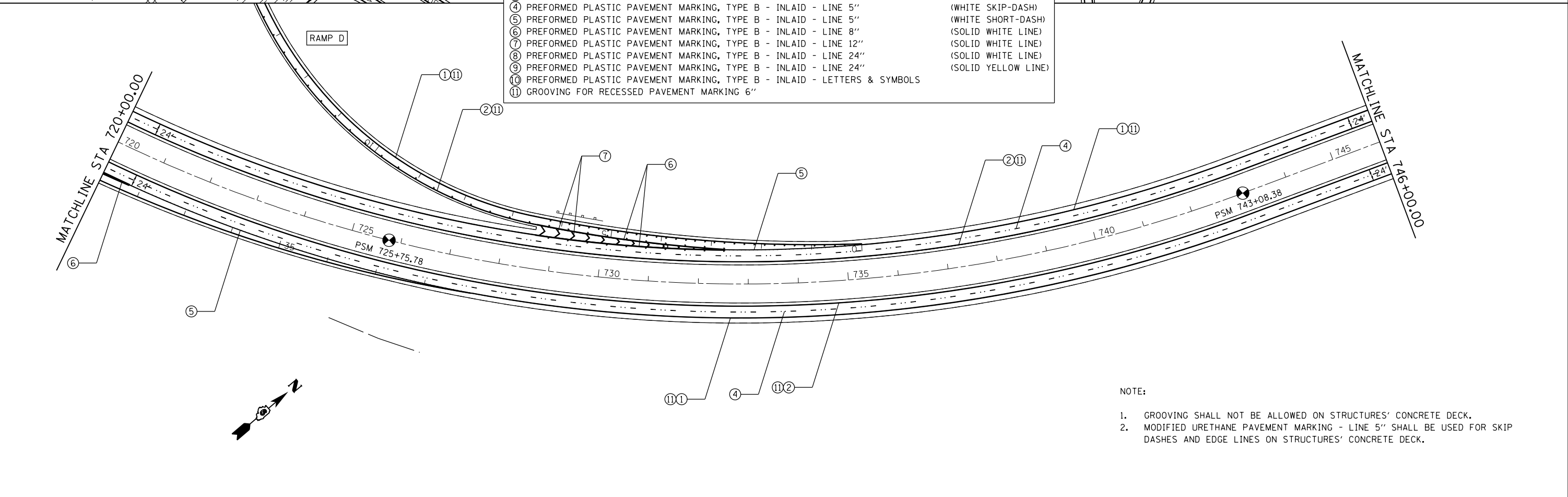
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	..	LOGAN	108	76
CONTRACT NO. 72041			ILLINOIS FED. AID PROJECT	



SEE BUS 55 INTERCHANGE
PLAN SHEET FOR MORE
INFORMATION

PAVEMENT MARKING LEGEND

①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID WHITE LINE)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	(SOLID YELLOW LINE)
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"	

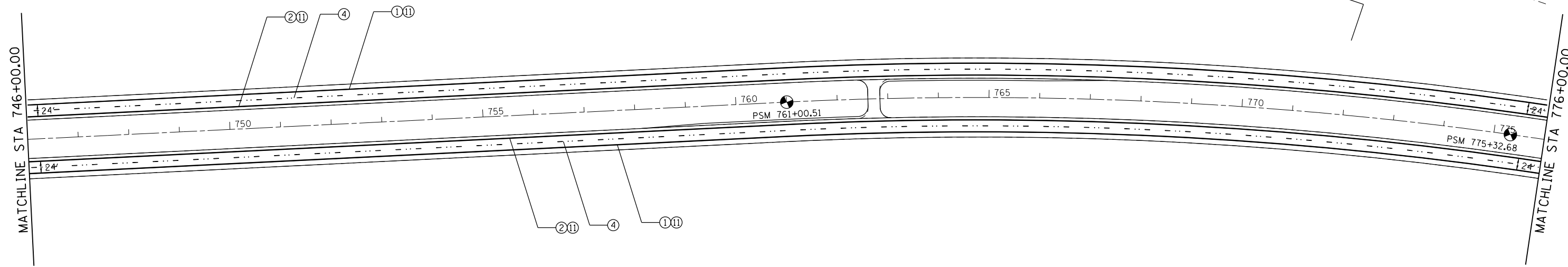


NOTE:
 1. GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
 2. MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.

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Default	PLOT SCALE = 200.0000' / in.	DRAWN -	REVISED -		SCALE: 1" = 100'	SHEET 8	OF	SHEETS	STA.	TO STA.	LOGAN	108	77
	PLOT DATE = Jul-14-2014 03:37:42PM	CHECKED -	REVISED -		CONTRACT NO. 72D41								
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

MATCHLINE STA 746+00.00

MATCHLINE STA 776+00.00

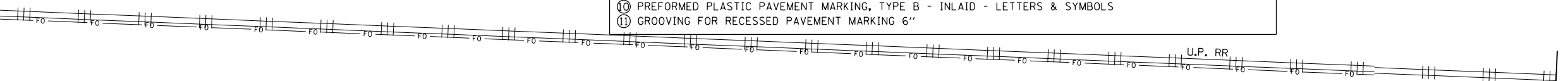


NOTE:

1. GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
2. MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.

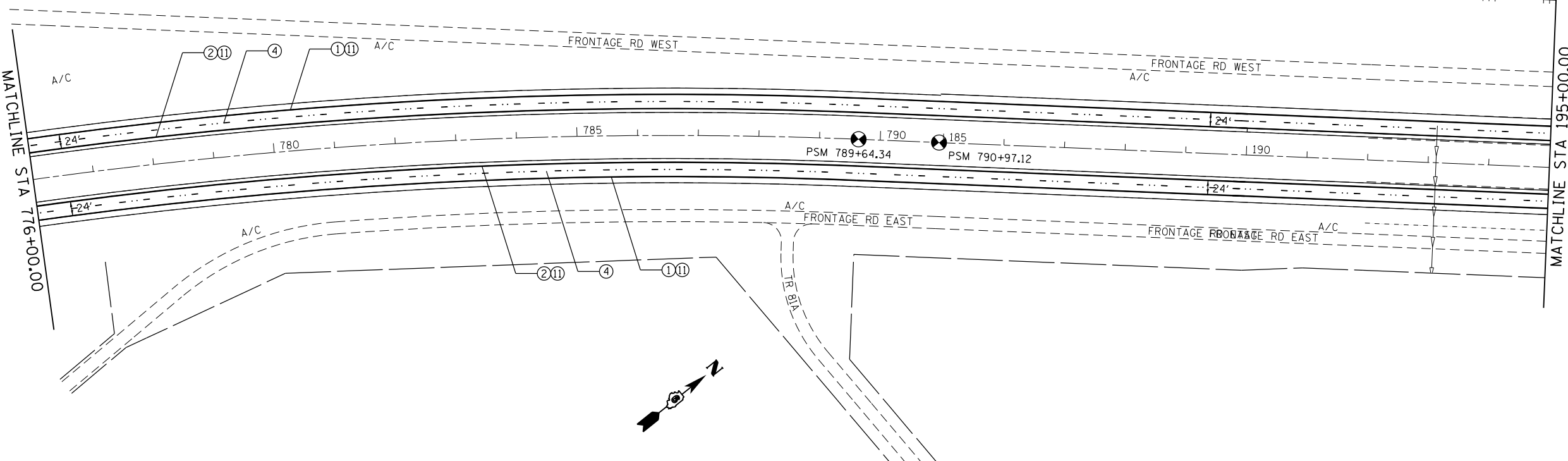
PAVEMENT MARKING LEGEND

①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID WHITE LINE)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"	



MATCHLINE STA 776+00.00

MATCHLINE STA 195+00.00



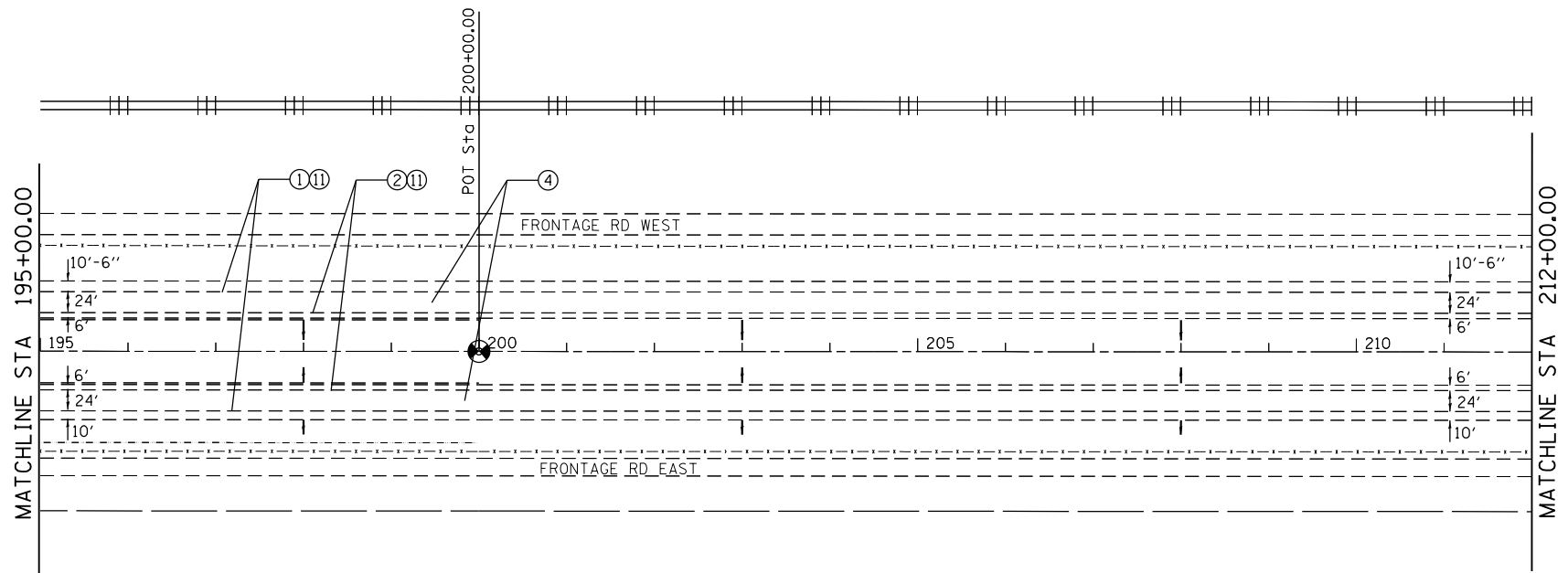
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. ROUTE 55
PAVEMENT MARKING PLAN SHEET

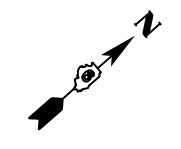
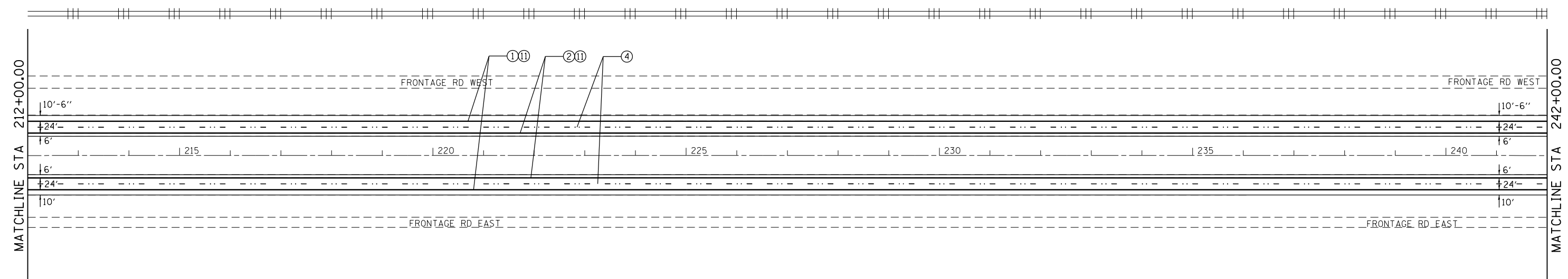
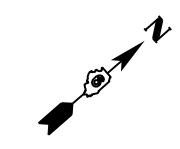
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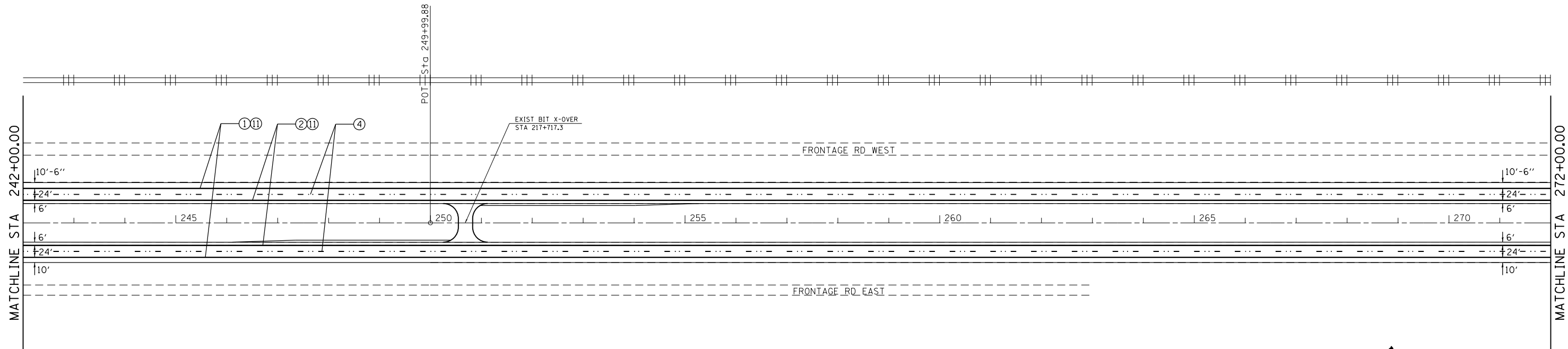
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	LOGAN	108	78
CONTRACT NO. 72D41				



PAVEMENT MARKING LEGEND		
①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID WHITE LINE)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"	

- NOTE:
- GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
 - MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.



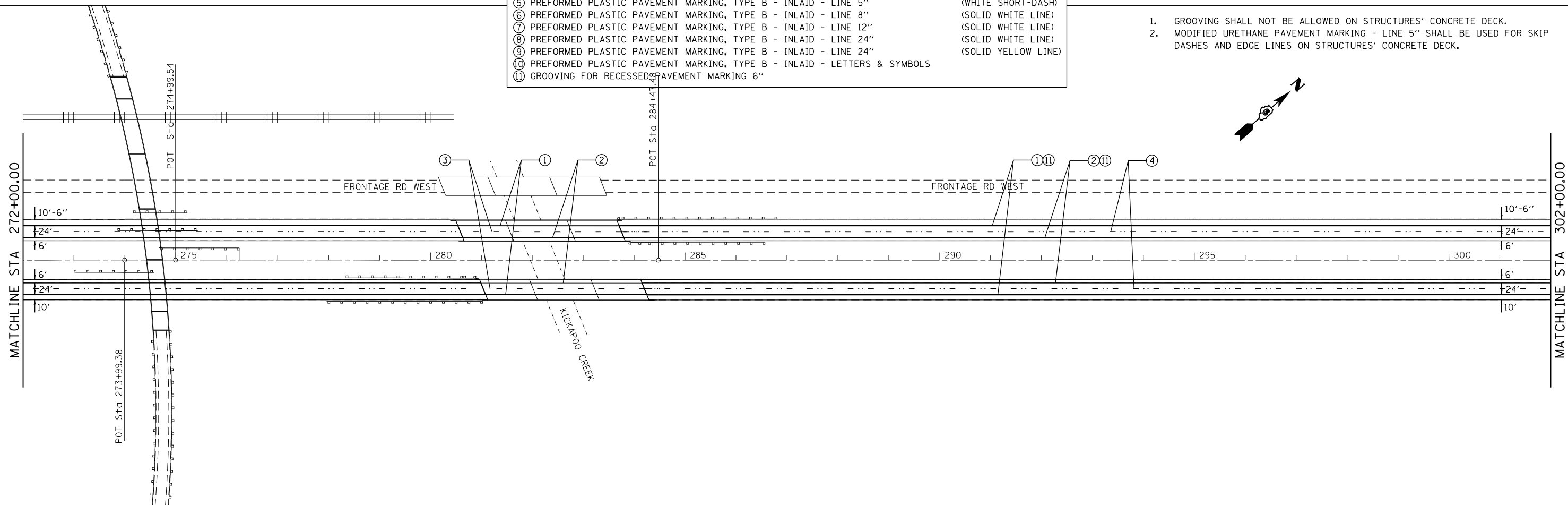


PAVEMENT MARKING LEGEND

①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
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⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	(SOLID YELLOW LINE)
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"	

NOTE:

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- MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.



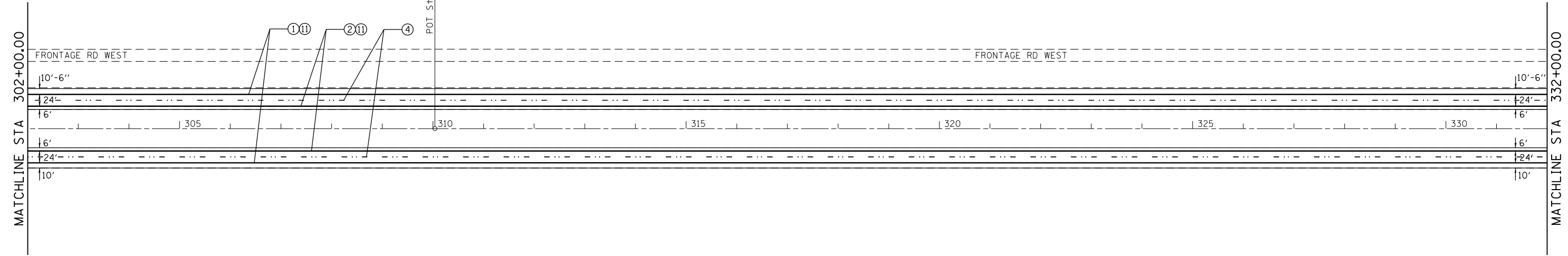
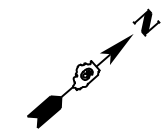
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. ROUTE 55
PAVEMENT MARKING PLAN SHEET

SCALE: SHEET 11 OF SHEETS STA. TO STA.

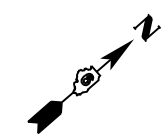
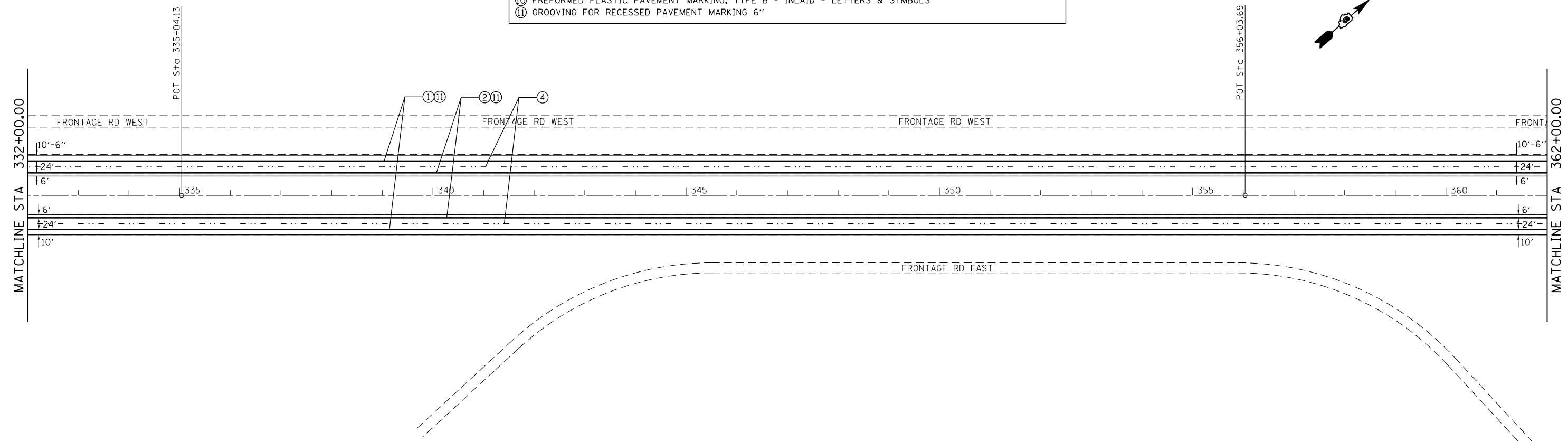
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
••	••	LOGAN	108	80
CONTRACT NO. 72D41			ILLINOIS FED. AID PROJECT	



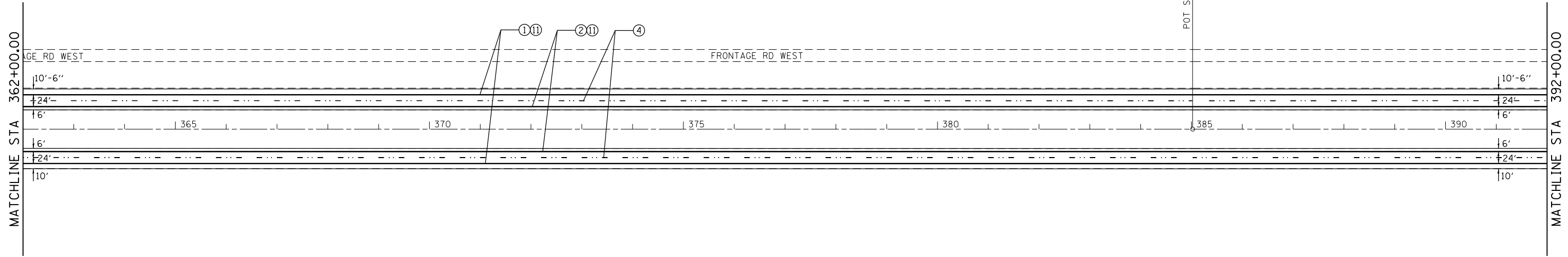
NOTE:

1. GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
2. MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.

PAVEMENT MARKING LEGEND		
①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
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⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"	



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Default	PLOT SCALE = 200.0000' / in.	DRAWN -	REVISED -		SCALE:	SHEET 12 OF SHEETS	STA.	TO STA.	LOGAN	108	81	
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		DATE -	REVISED -		CONTRACT NO. 72D41							

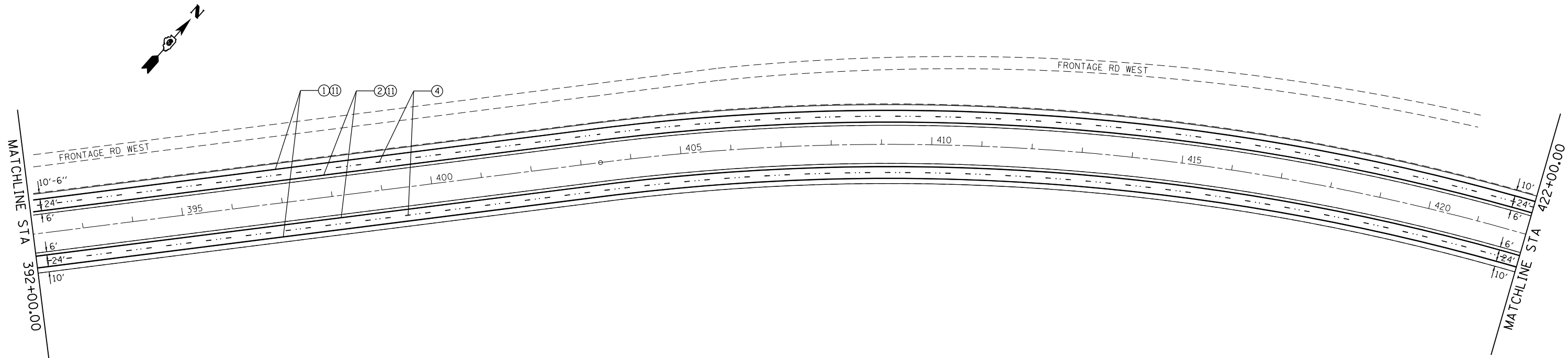


NOTE:

1. GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
2. MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.

PAVEMENT MARKING LEGEND

①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID WHITE LINE)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"	



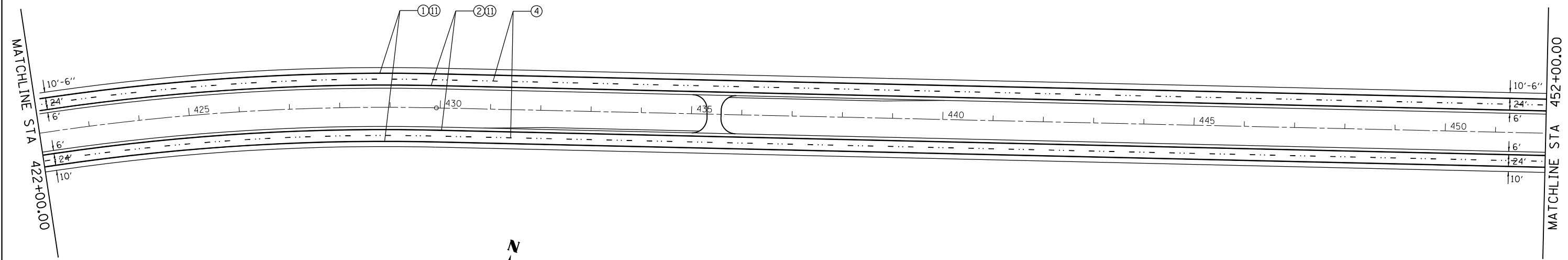
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**F.A.I. ROUTE 55
PAVEMENT MARKING PLAN SHEET**

SCALE: SHEET 13 OF SHEETS STA. TO STA.

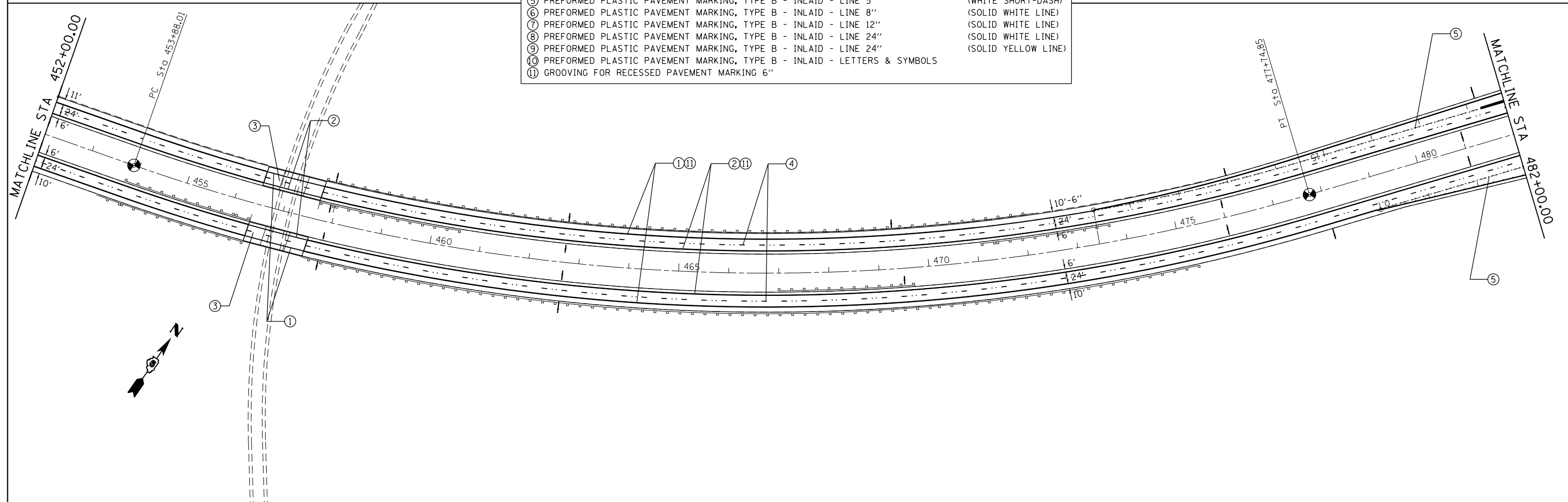
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	LOGAN	108	82
CONTRACT NO. 72D41			ILLINOIS FED. AID PROJECT	



NOTE:

1. GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
2. MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.

PAVEMENT MARKING LEGEND		
①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID WHITE LINE)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	(SOLID YELLOW LINE)
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"	



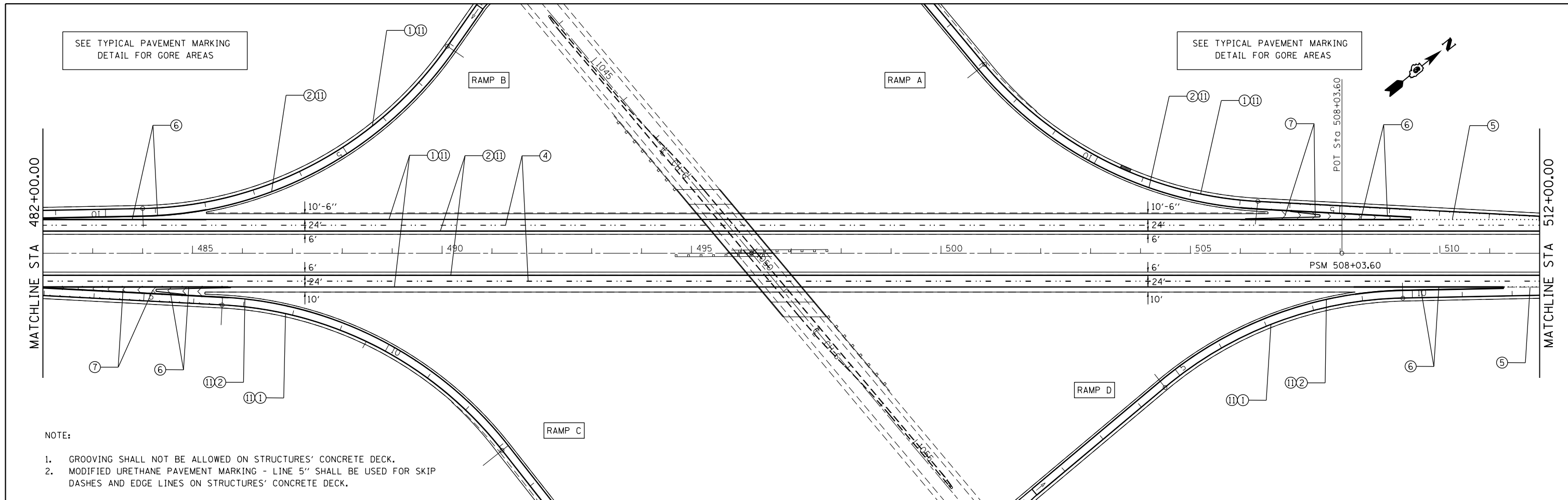
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	PLOT DATE = Jul-14-2014 03:37:48PM	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**F.A.I. ROUTE 55
PAVEMENT MARKING PLAN SHEET**

SCALE: SHEET 14 OF SHEETS STA. TO STA.

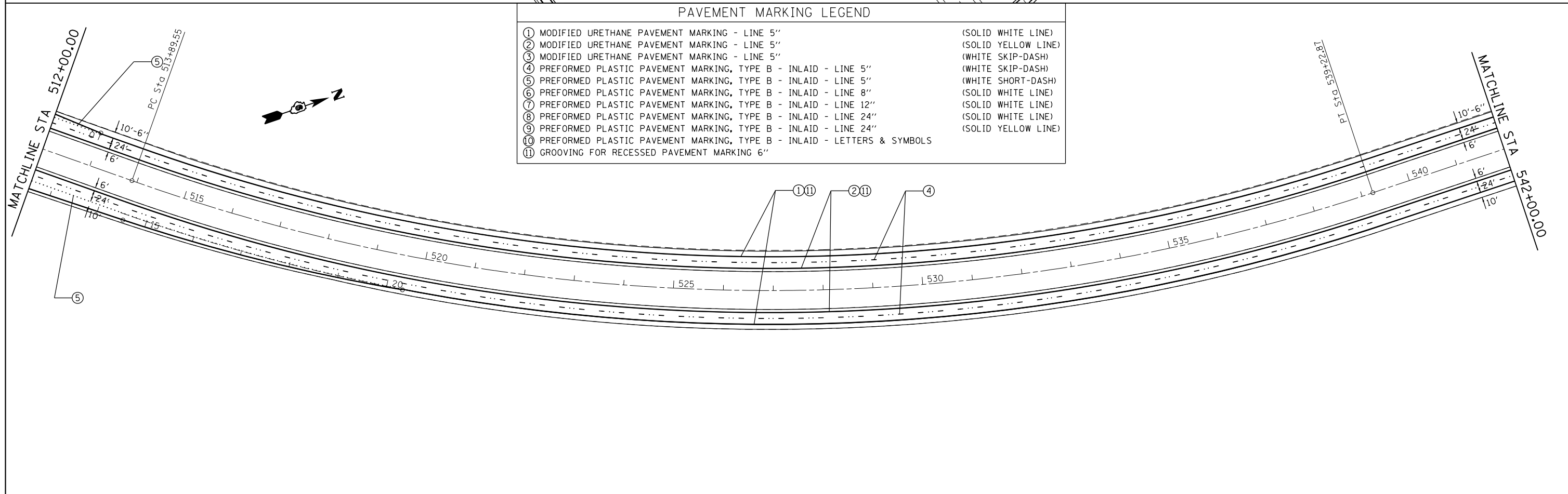
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	..	LOGAN	108	83
CONTRACT NO. 72D41			ILLINOIS FED. AID PROJECT	



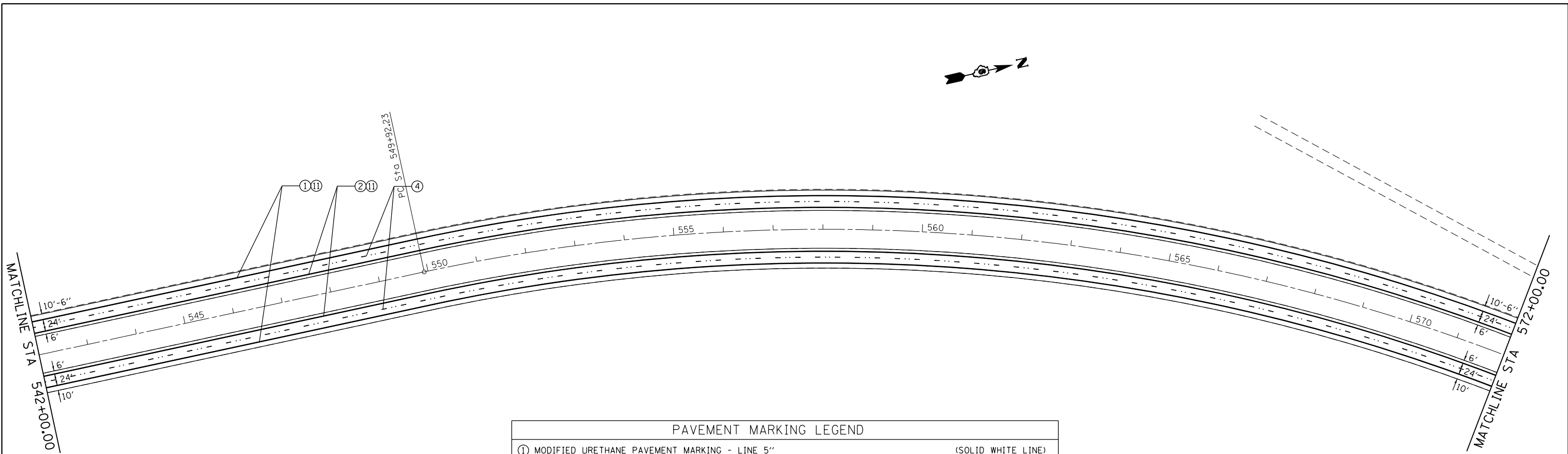
- NOTE:
- GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
 - MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.

PAVEMENT MARKING LEGEND

①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID WHITE LINE)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	(SOLID YELLOW LINE)
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"	



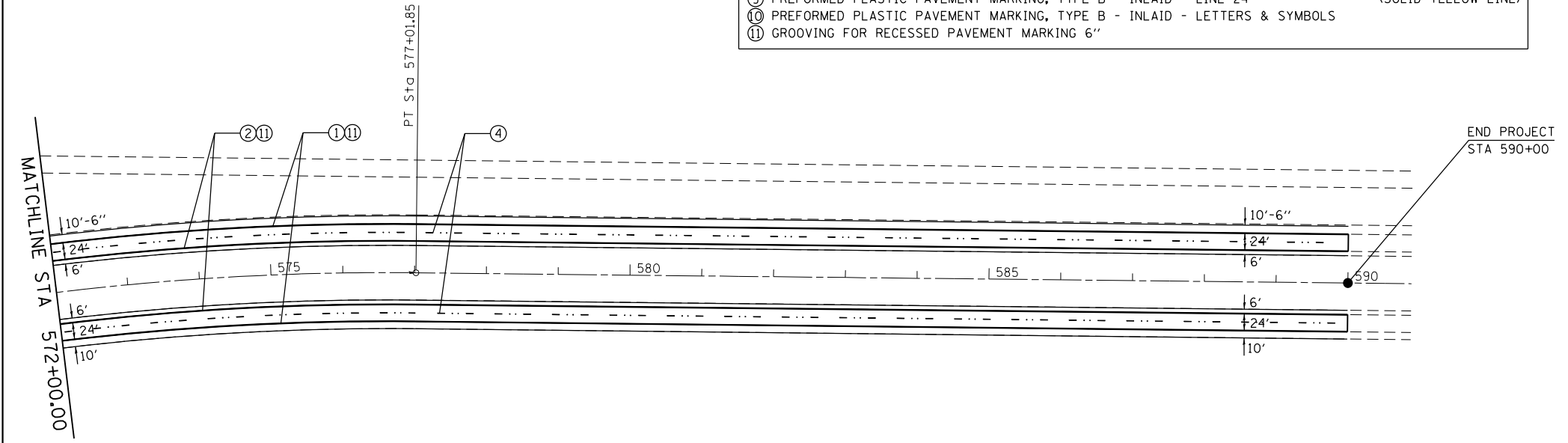
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	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -								CONTRACT NO.	72D41	
	PLOT DATE = Jul-14-2014 03:37:48PM	DATE -	REVISED -								ILLINOIS FED. AID PROJECT		



NOTE:

- GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
- MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.

PAVEMENT MARKING LEGEND		
①	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(SOLID YELLOW LINE)
③	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP-DASH)
④	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SKIP-DASH)
⑤	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	(WHITE SHORT-DASH)
⑥	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	(SOLID WHITE LINE)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID WHITE LINE)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	(SOLID YELLOW LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS	
⑪	GROOVING FOR RECESSED PAVEMENT MARKING 6"	



FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
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	PLOT DATE = Jul-14-2014 03:37:49PM	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**F.A.I. ROUTE 55
PAVEMENT MARKING PLAN SHEET**

SCALE: SHEET 16 OF SHEETS STA. TO STA.

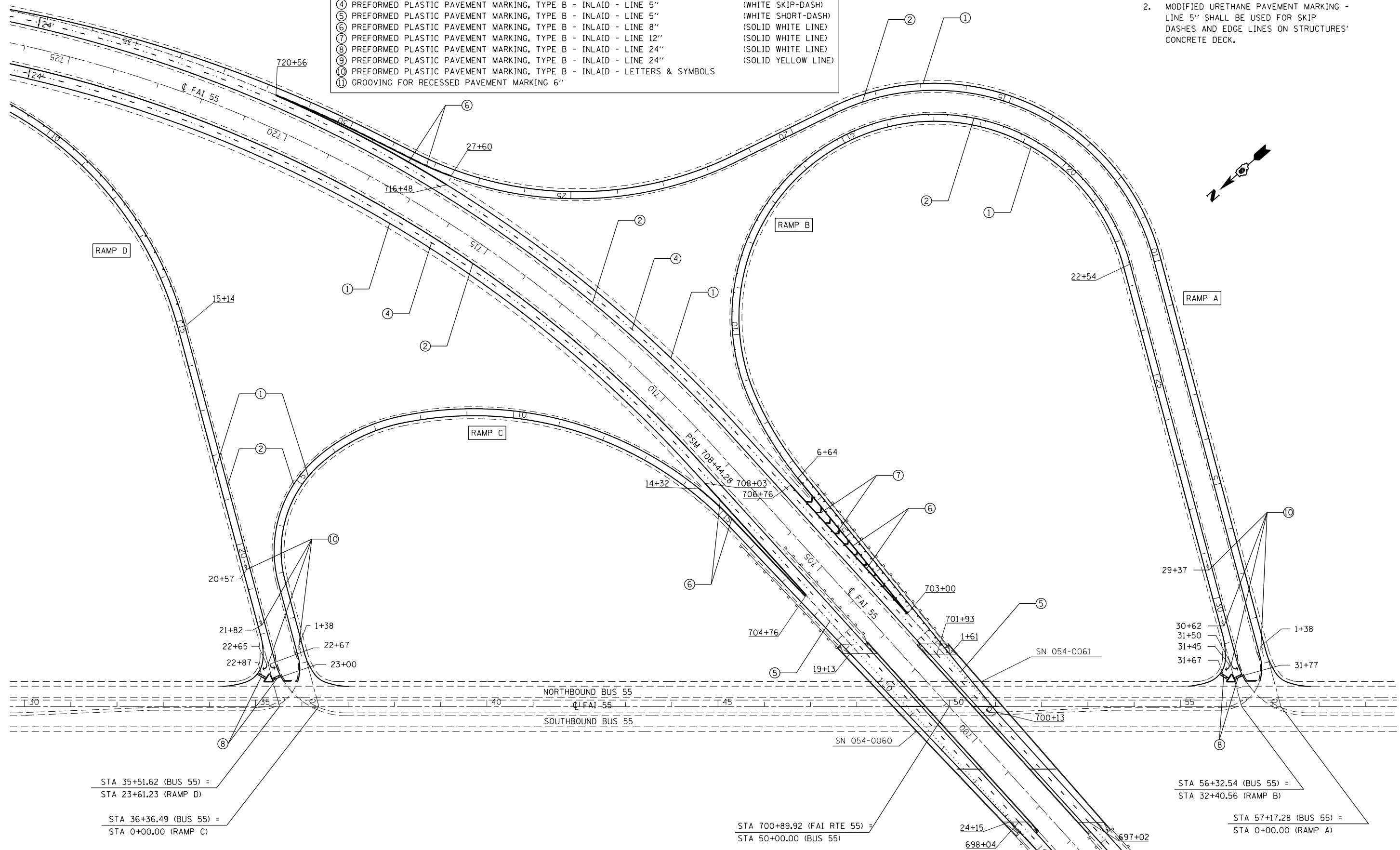
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	LOGAN	108	85
ILLINOIS FED. AID PROJECT			CONTRACT NO.	72D41

PAVEMENT MARKING LEGEND

- | | |
|---|---------------------|
| ① MODIFIED URETHANE PAVEMENT MARKING - LINE 5" | (SOLID WHITE LINE) |
| ② MODIFIED URETHANE PAVEMENT MARKING - LINE 5" | (SOLID YELLOW LINE) |
| ③ MODIFIED URETHANE PAVEMENT MARKING - LINE 5" | (WHITE SKIP-DASH) |
| ④ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5" | (WHITE SKIP-DASH) |
| ⑤ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5" | (WHITE SHORT-DASH) |
| ⑥ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8" | (SOLID WHITE LINE) |
| ⑦ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12" | (SOLID WHITE LINE) |
| ⑧ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24" | (SOLID WHITE LINE) |
| ⑨ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24" | (SOLID WHITE LINE) |
| ⑩ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24" | (SOLID WHITE LINE) |
| ⑪ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS | (SOLID YELLOW LINE) |
| ⑫ GROOVING FOR RECESSED PAVEMENT MARKING 6" | |

NOTE:

- GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
- MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.



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		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

SCALE: 1" = 100' SHEET 17 OF SHEETS STA. TO STA.

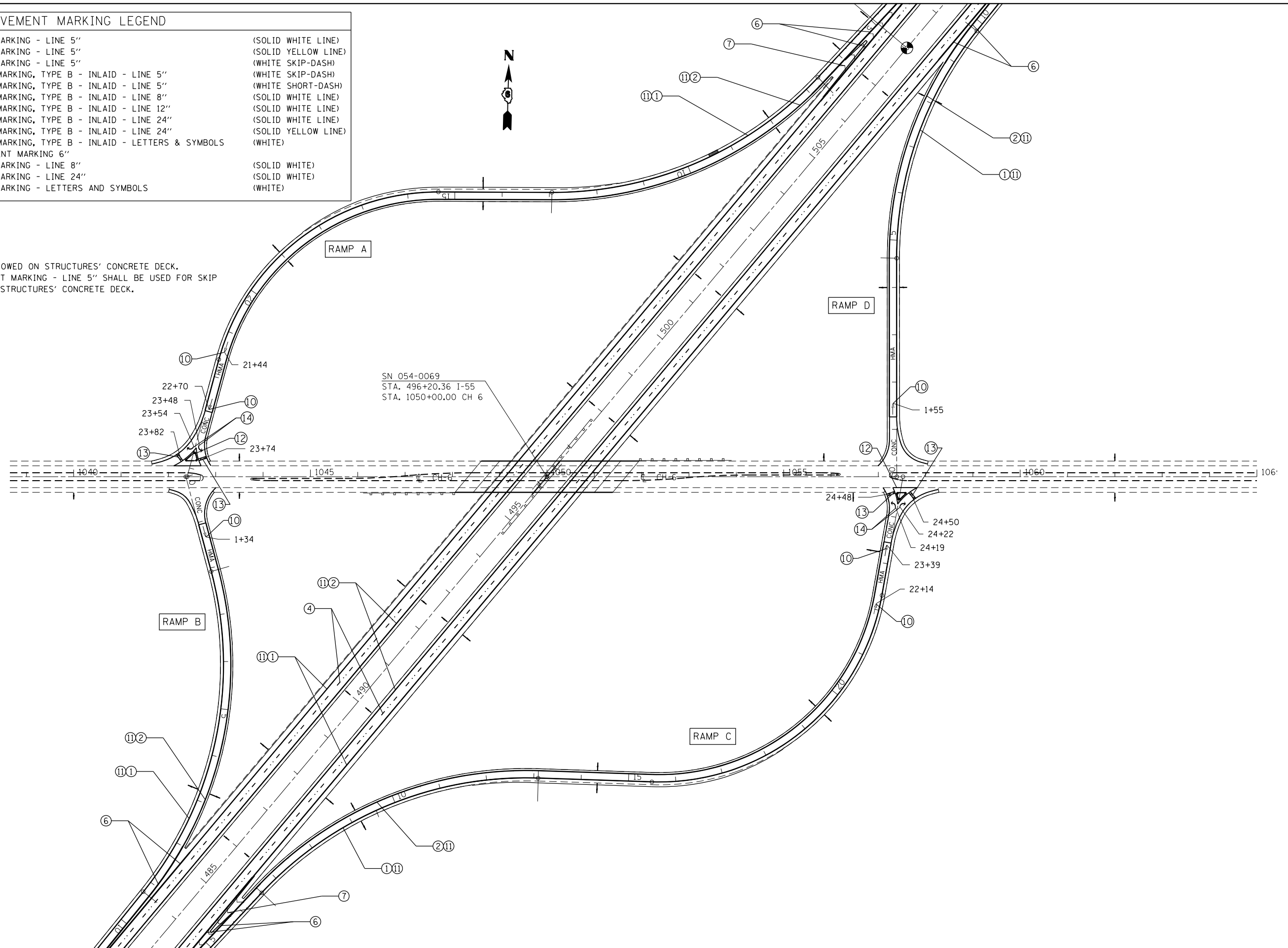
• 55/155

PAVEMENT MARKING LEGEND

- | | |
|---|---------------------|
| ① MODIFIED URETHANE PAVEMENT MARKING - LINE 5" | (SOLID WHITE LINE) |
| ② MODIFIED URETHANE PAVEMENT MARKING - LINE 5" | (SOLID YELLOW LINE) |
| ③ MODIFIED URETHANE PAVEMENT MARKING - LINE 5" | (WHITE SKIP-DASH) |
| ④ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5" | (WHITE SKIP-DASH) |
| ⑤ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5" | (WHITE SHORT-DASH) |
| ⑥ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8" | (SOLID WHITE LINE) |
| ⑦ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12" | (SOLID WHITE LINE) |
| ⑧ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24" | (SOLID WHITE LINE) |
| ⑨ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24" | (SOLID YELLOW LINE) |
| ⑩ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS & SYMBOLS | (WHITE) |
| ⑪ GROOVING FOR RECESSED PAVEMENT MARKING 6" | (SOLID WHITE) |
| ⑫ MODIFIED URETHANE PAVEMENT MARKING - LINE 8" | (SOLID WHITE) |
| ⑬ MODIFIED URETHANE PAVEMENT MARKING - LINE 24" | (SOLID WHITE) |
| ⑭ MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS | (WHITE) |

NOTE:

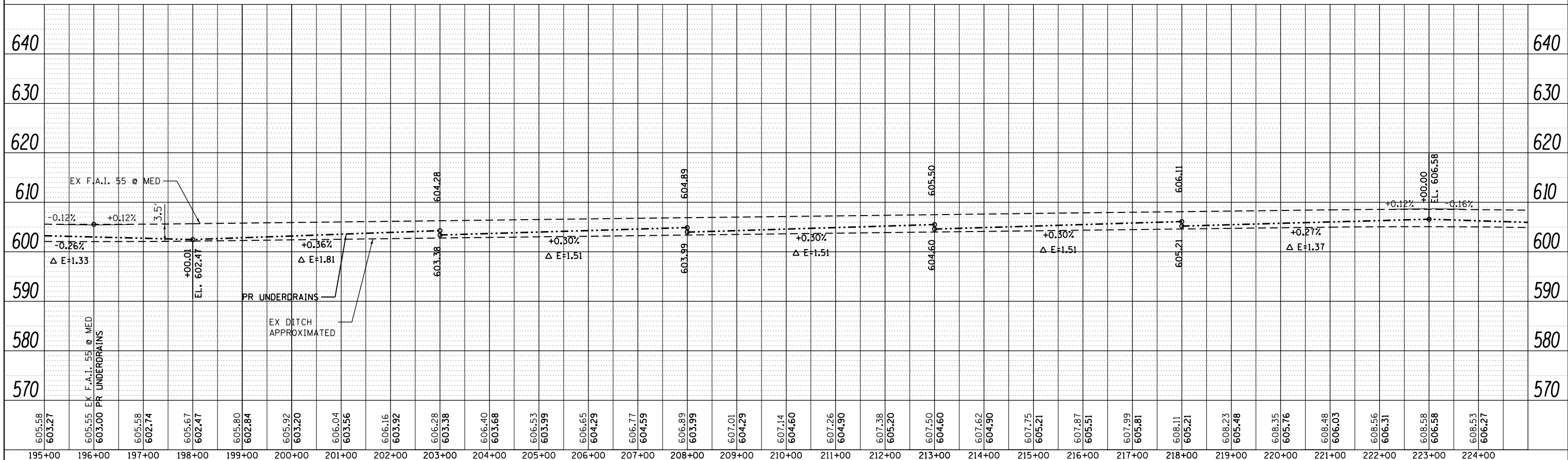
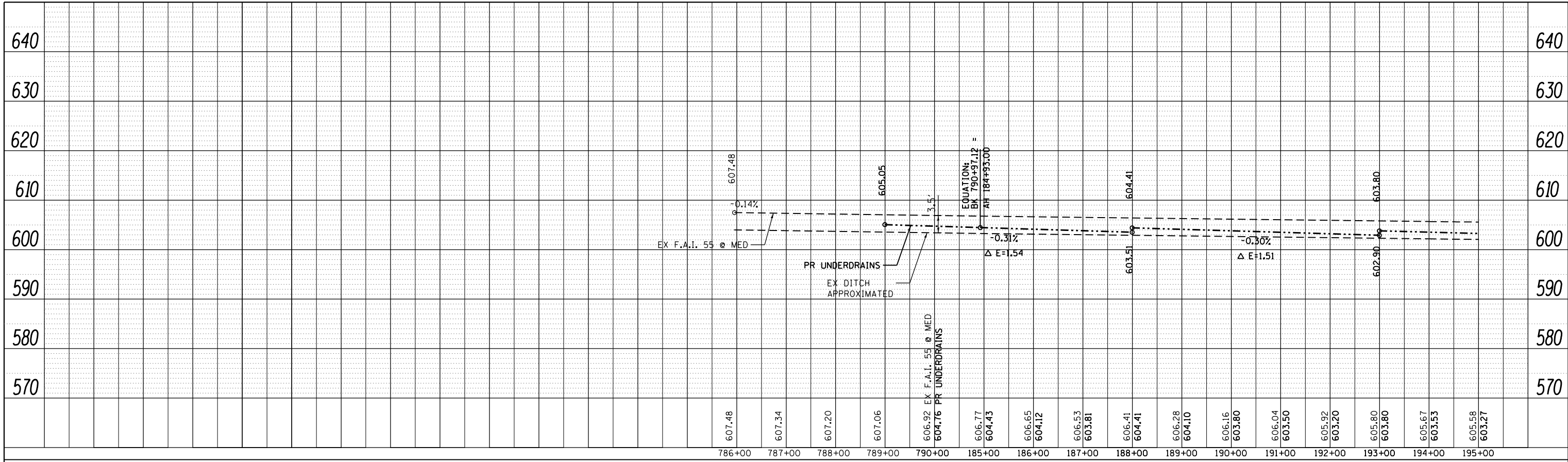
- GROOVING SHALL NOT BE ALLOWED ON STRUCTURES' CONCRETE DECK.
- MODIFIED URETHANE PAVEMENT MARKING - LINE 5" SHALL BE USED FOR SKIP DASHES AND EDGE LINES ON STRUCTURES' CONCRETE DECK.



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PLAN	SURVEYED	DATE
	PLOTTED	
	ALIGNMENT	CHECKED
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	CARD FILE NAME	

PROFILE	SURVEYED	DATE
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	STRUCTURE	NOTATIONS CHECKED
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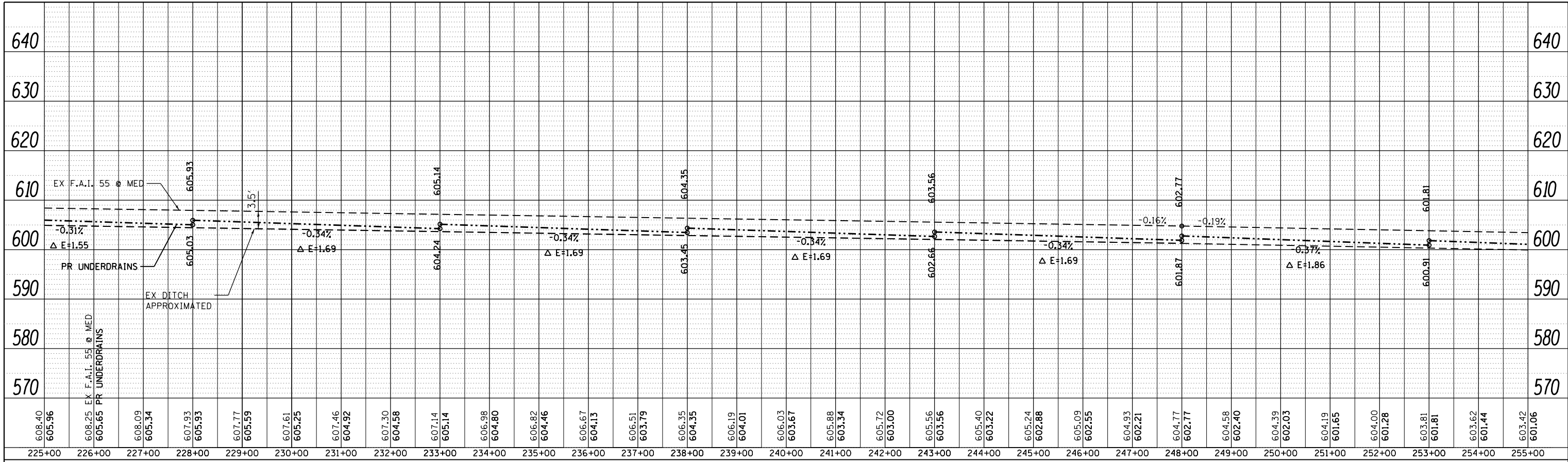
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Default	PLOT DATE = Jul-14-2014 03:37:58PM	DATE -		REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

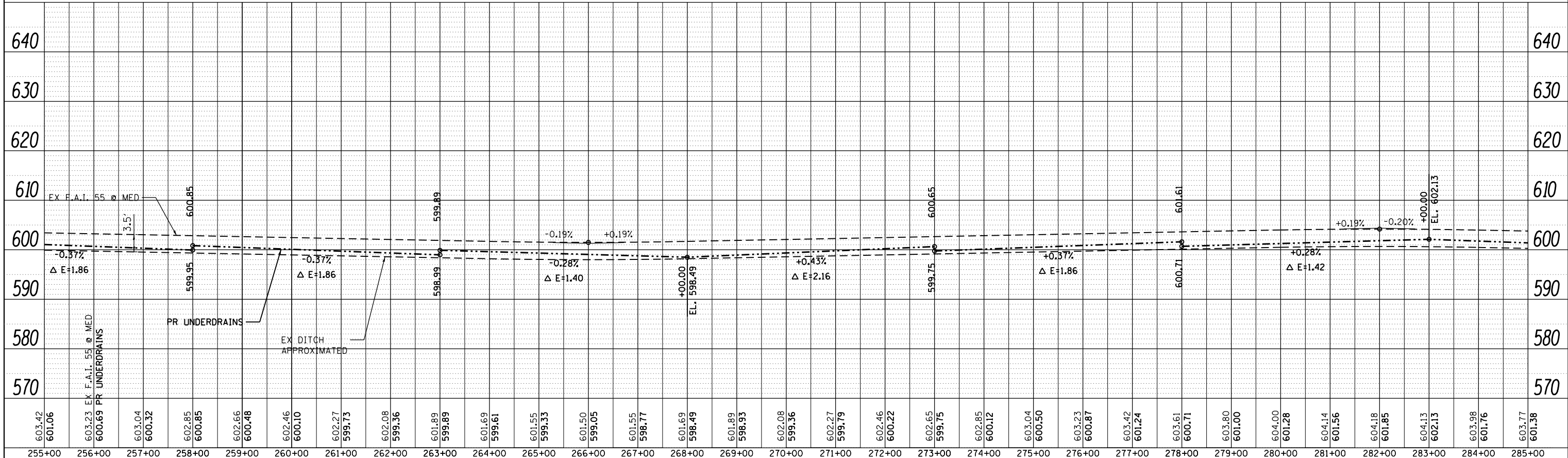
**UNDERDRAINS
DOUBLE PROFILE SHEET**

SCALE:	SHEET 1	OF	SHEETS	STA.	TO	STA.															
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.																	
	..	LOGAN	108	88																	
			CONTRACT NO.	72D41																	

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	ALIGNED		
	CHECKED		
	FILED		
NOTE BOOK NO.	CARD FILE NAME		



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
NOTE BOOK NO.	NOTATIONS CHECKED		



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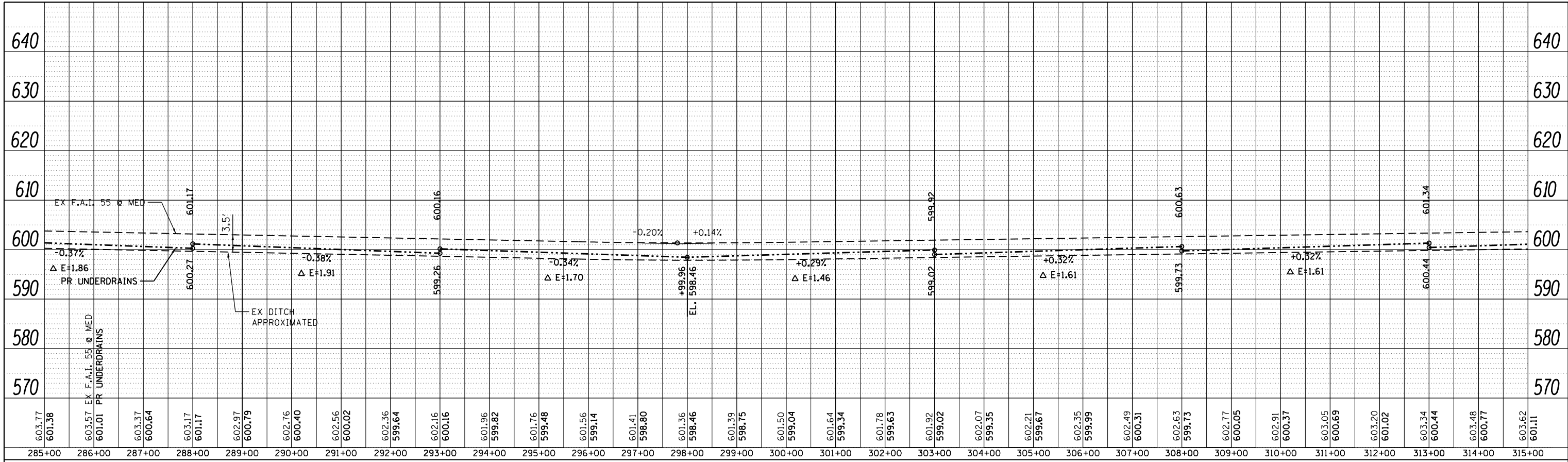
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**UNDERDRAINS
DOUBLE PROFILE SHEET**

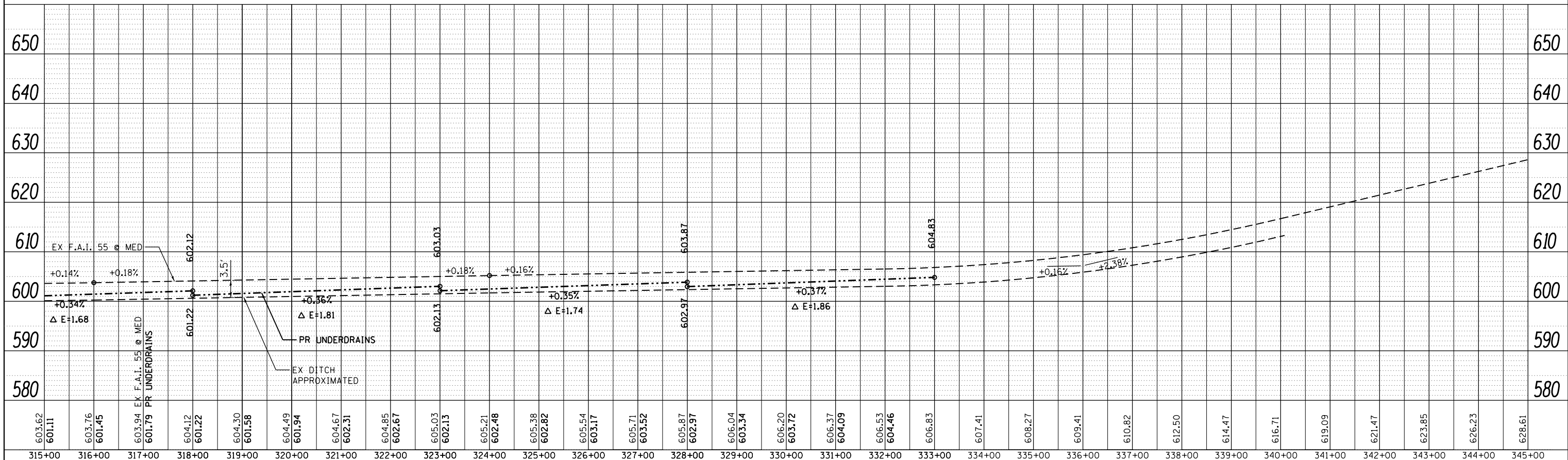
SCALE: SHEET 2 OF SHEETS STA. TO STA. ILLINOIS FED. AID PROJECT

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	LOGAN	108	89
			CONTRACT NO.	72D41

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	ALIGNED		
	CHECKED		
	FILED		
	NO.		
	NO.		
	NO.		



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE		
	NOTATIONS CHECKED		
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	NO.		
	NO.		

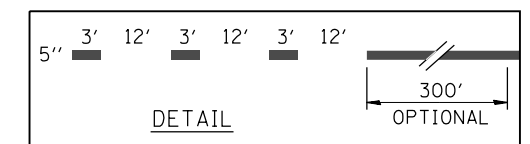
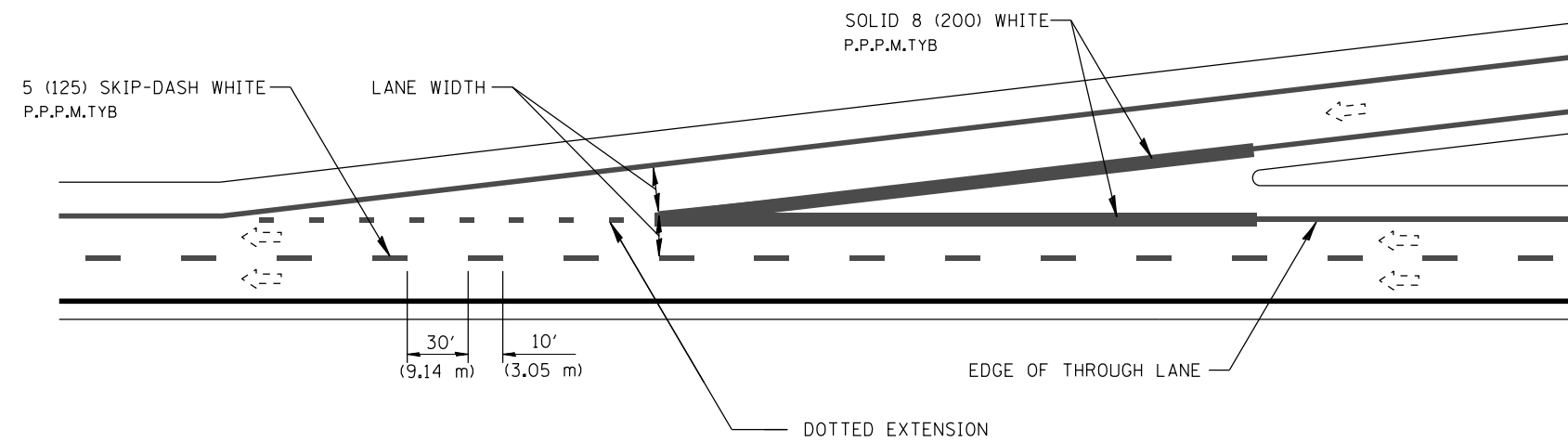
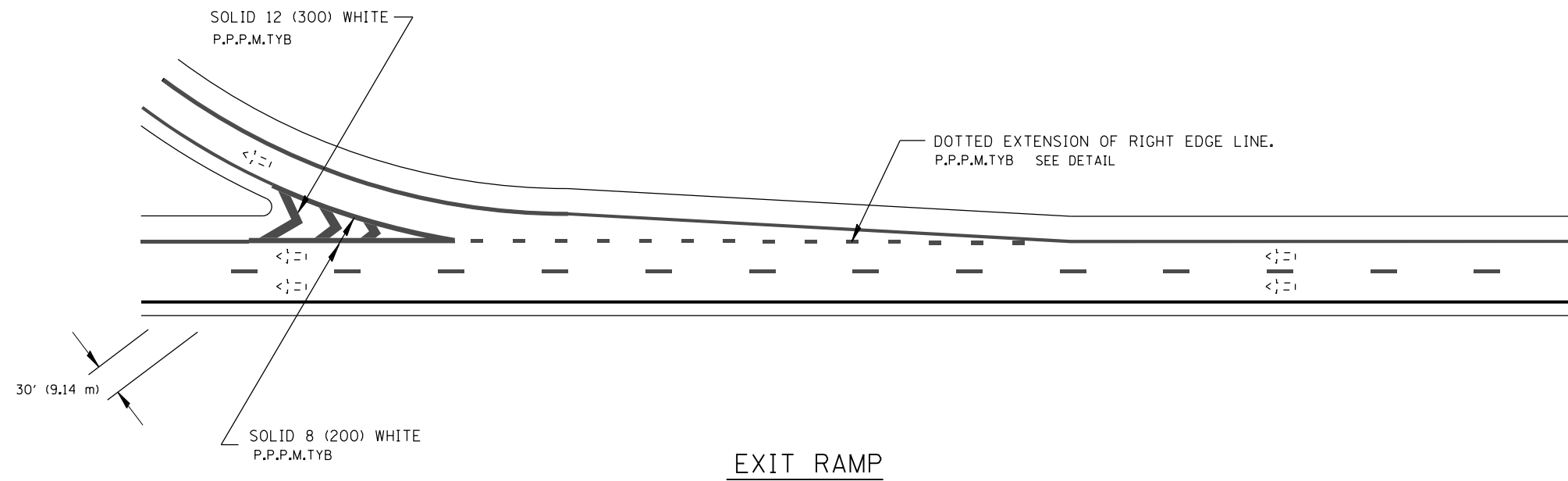


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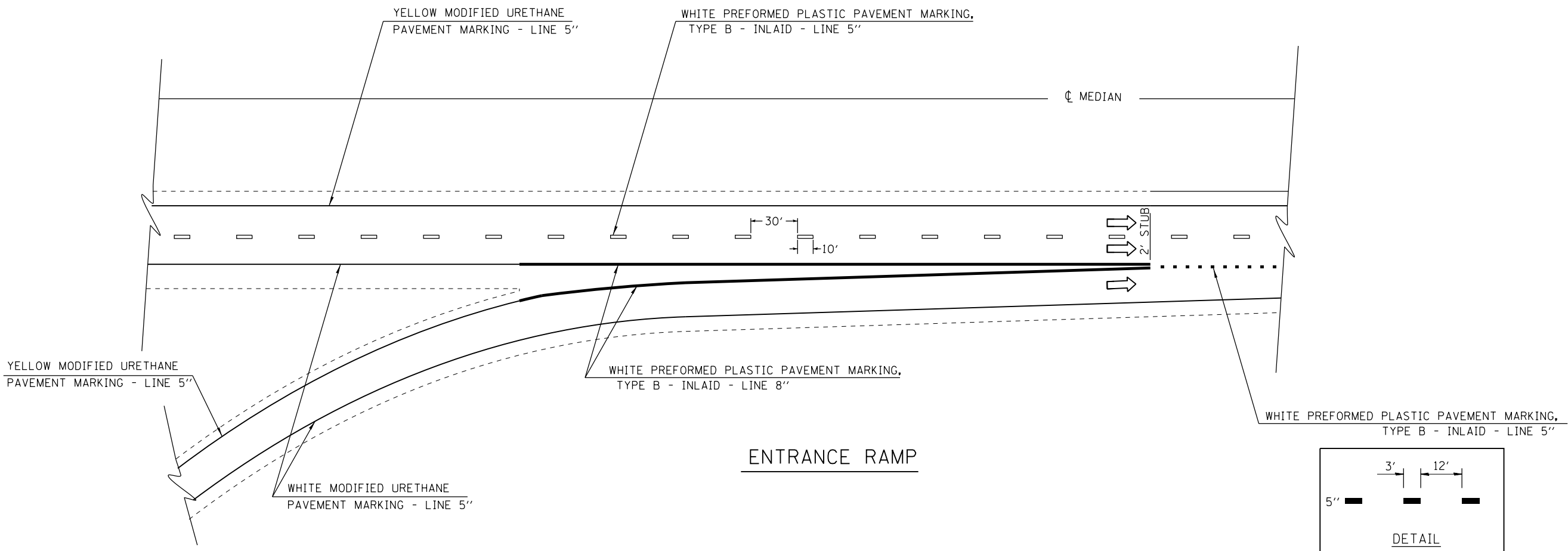
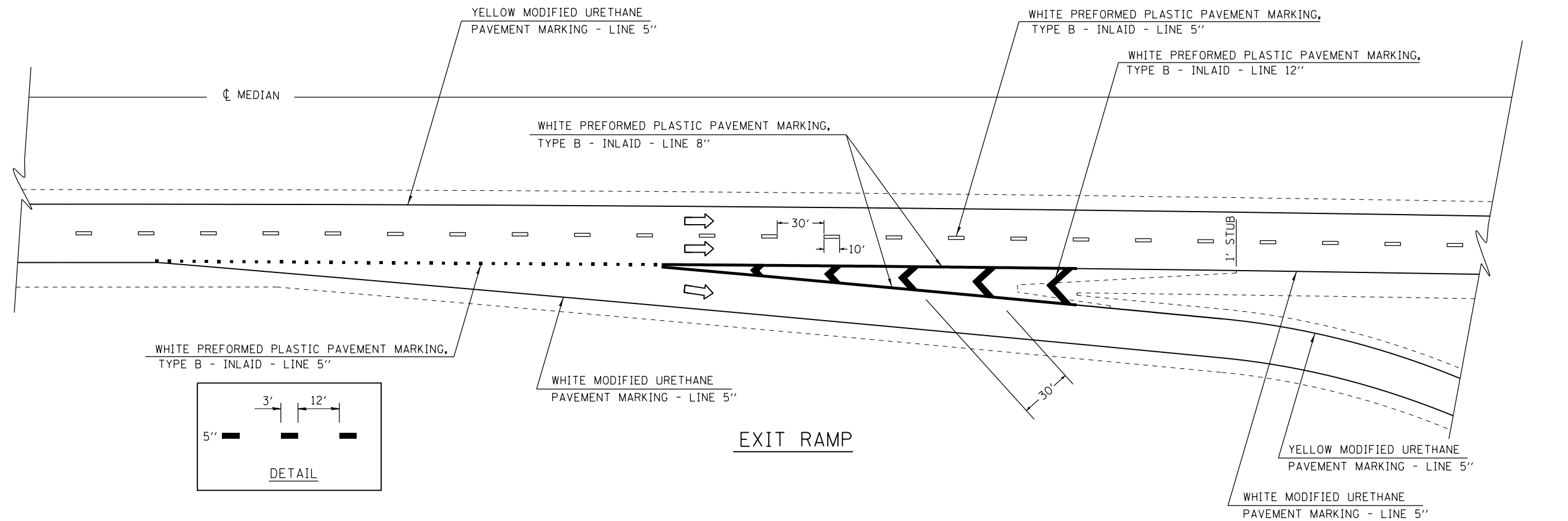
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**UNDERDRAINS
DOUBLE PROFILE SHEET**

SCALE:	SHEET 3	OF	SHEETS	STA.	TO	STA.															
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.																	
	..	LOGAN	108	90																	
			CONTRACT NO.	72D41																	



FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL PAVEMENT MARKING DETAIL			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = Jul-14-2014 03:38:00PM	DATE -	REVISED -	SCALE:		SHEET NO. 1 OF 2 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT				

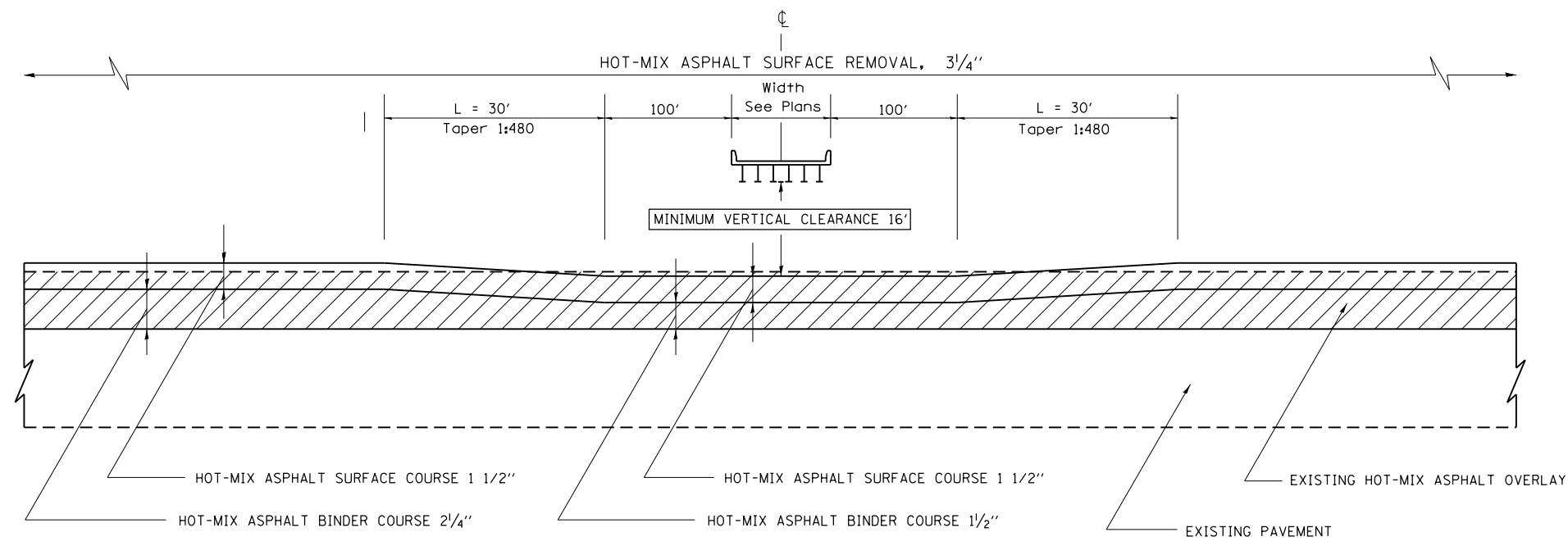


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	PLOT DATE = Jul-14-2014 03:38:00PM	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL PAVEMENT MARKING DETAIL			
SCALE:	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	..	LOGAN	108	92
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72D41	



OVERHEAD STRUCTURE RAMPING DETAIL

SB SN 059-0050
 NB SN 054-0052
 NB & SB SN 054-0059
 SN 054-0069 NB LANES ONLY

NOTES:

HOT-MIX ASPHALT RESURFACING OF THE EXISTING PAVEMENT AND SHOULDERS SHALL BE DONE IN A MANNER THAT MAINTAINS A MINIMUM VERTICAL CLEARANCE OF 16 FEET AT ALL OVERHEAD STRUCTURES. THE MINIMUM VERTICAL CLEARANCE SHALL BE MAINTAINED FROM OUTSIDE TO OUTSIDE OF THE PAVED SHOULDERS.

PRIOR TO THE START OF RESURFACING OPERATIONS, THE CONTRACTOR, IN THE PRESENCE OF THE ENGINEER, SHALL MEASURE AND DOCUMENT THE EXISTING VERTICAL CLEARANCE AT ALL OVERHEAD STRUCTURES. MEASUREMENTS SHALL BE TAKEN AT THE OUTSIDE EDGES OF THE PAVED SHOULDERS, AT THE EDGE OF EACH LANE, AND AT ANY SPLICE PLATES OVER THE SHOULDERS OR PAVEMENT. IF NECESSARY, THE ENGINEER SHALL MAKE ADJUSTMENTS TO THE RESURFACING THICKNESS SHOWN IN THE PLANS TO MAINTAIN THE REQUIRED MINIMUM VERTICAL CLEARANCE.

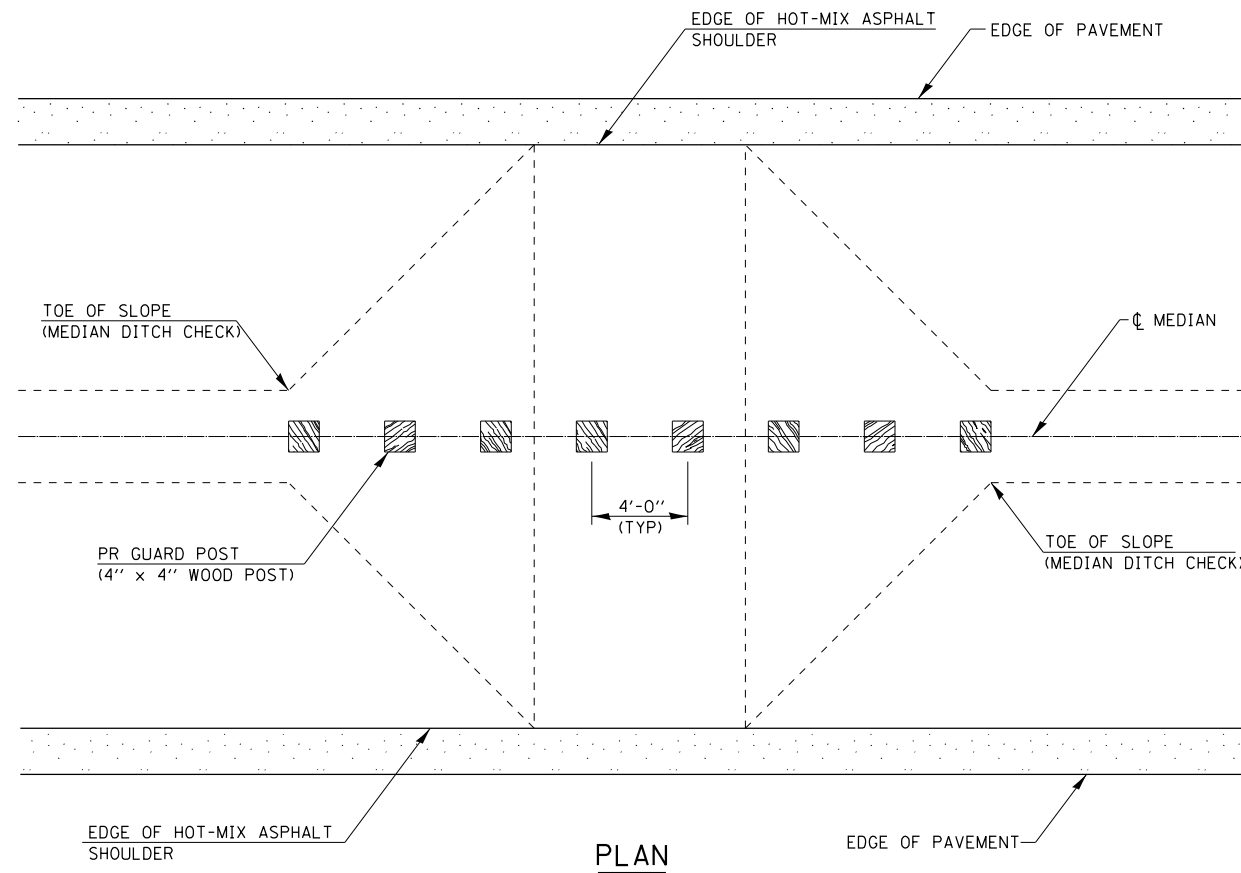
FOLLOWING PLACEMENT OF THE SURFACE COURSE AND HMA SHOULDERS, THE CONTRACTOR, IN THE PRESENCE OF THE ENGINEER, SHALL MEASURE AND DOCUMENT THE VERTICAL CLEARANCE AS DESCRIBED ABOVE. IF THE MINIMUM VERTICAL CLEARANCE IS LESS THAN 16 FEET, THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL REMOVE AND REPLACE THE HMA SURFACE COURSE AS DIRECTED BY THE ENGINEER.

THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE VARIOUS HOT-MIX ASPHALT ITEMS.

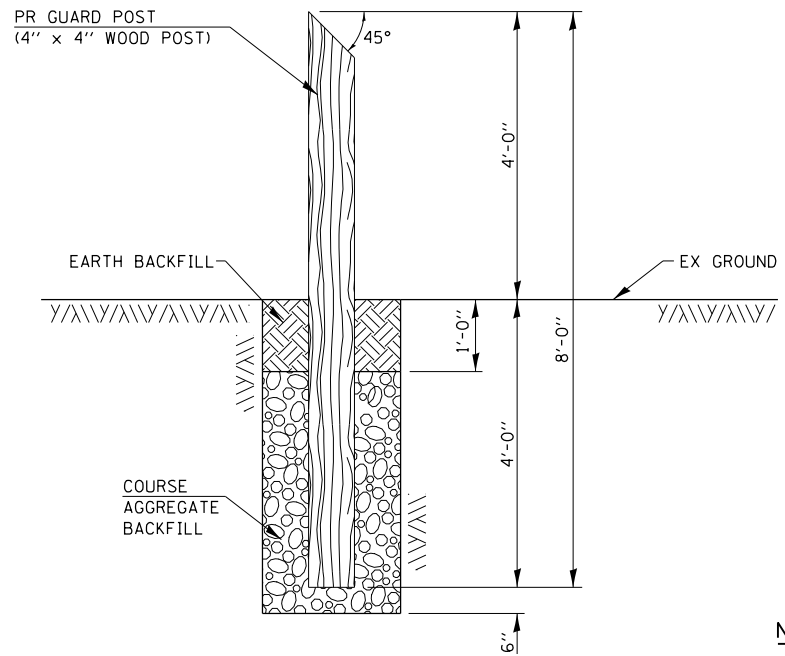
HOT-MIX ASPHALT SHOULDERS SHALL BE RAMPED IN THE SAME MANNER AS THE HMA BINDER AND SURFACE COURSES.

 - HOT-MIX ASPHALT SURFACE REMOVAL

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	OVERHEAD STRUCTURE RAMPING DETAIL			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\idot\sparksgw\0215133\shh- details.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	LOGAN	108	93	
		CHECKED -	REVISED -					CONTRACT NO. 72D41		ILLINOIS FED. AID PROJECT		
		DATE -	REVISED -									



PLAN

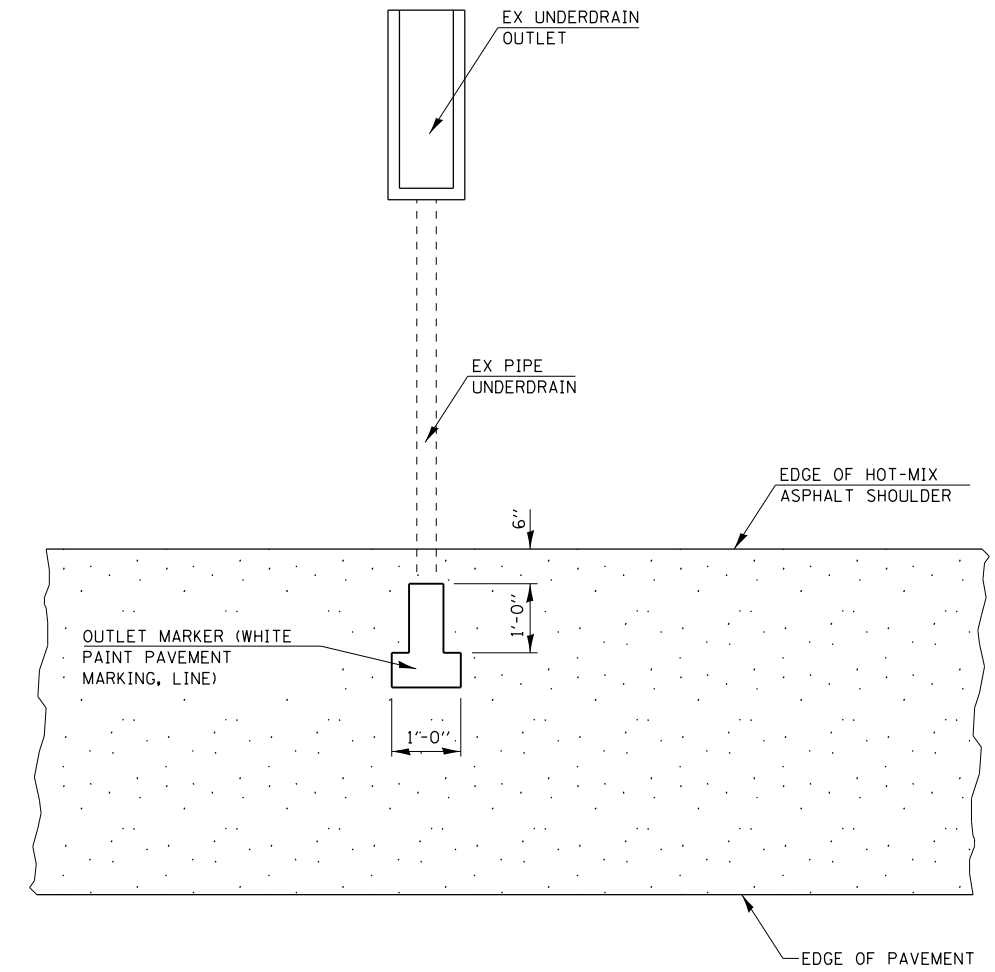


SECTION THRU POST

GUARD POST DETAIL

NOTES:

- 1.) PROPOSED GUARDPOSTS USED TO REPLACE MISSING POSTS AT UNAUTHORIZED MEDIAN CROSSOVERS.
- 2.) ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY FOR INSTALLATION OF GUARD POSTS AS DETAILED HEREIN SHALL BE INCLUDED IN THE COST FOR "GUARD POSTS".



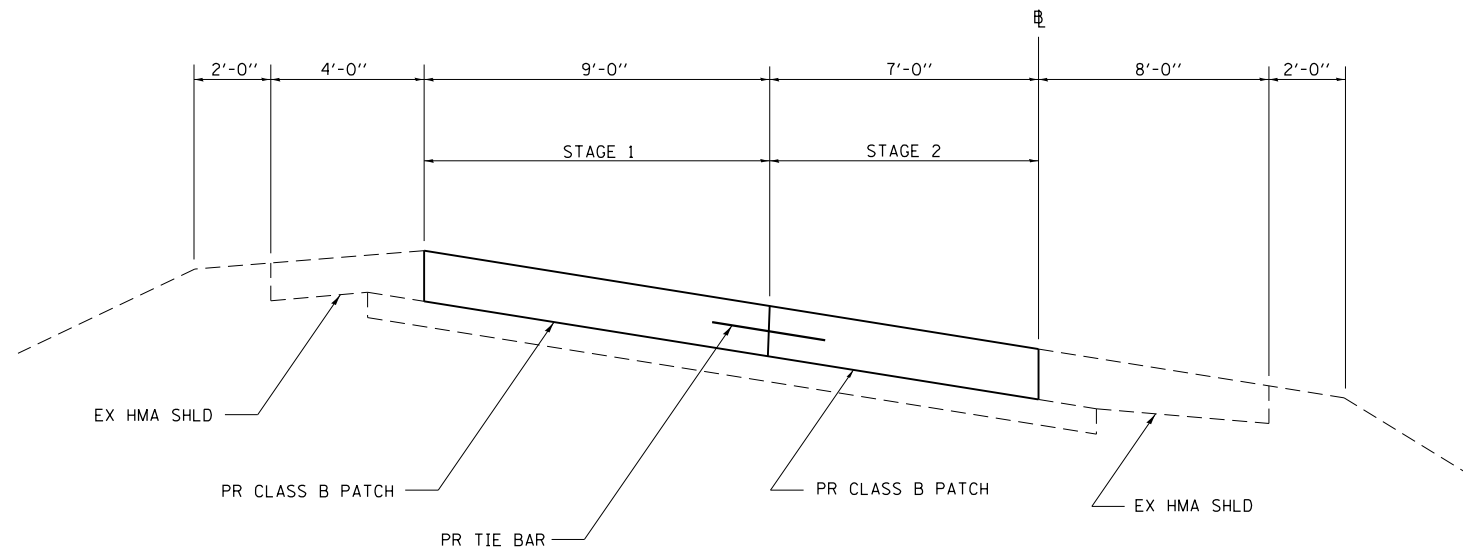
PLAN FOR UNDERDRAIN OUTLET MARKER

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
ca:\pw\work\p\dot\sparksgw\0215133\sh-t\details.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = Jul-14-2014 03:38:00PM	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MEDIAN GUARD POST DETAIL & UNDERDRAIN OUTLET MARKER DETAIL			
SCALE:	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

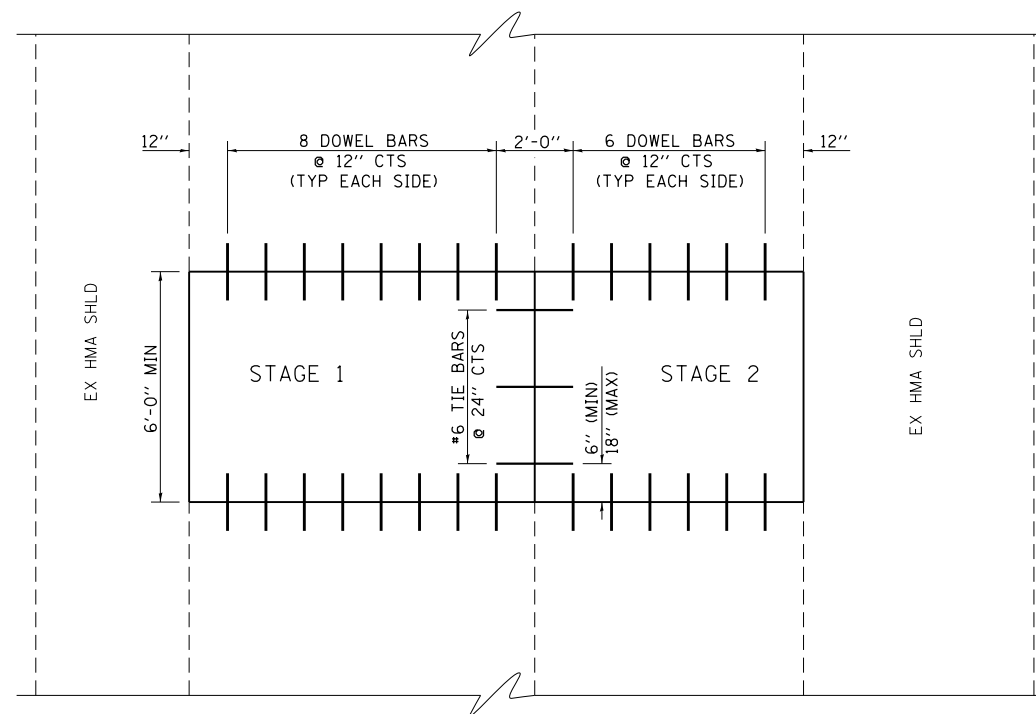
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	LOGAN	108	94
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72D41	



NOTE:

1. THIS DETAIL IS TO BE USED IN CONJUNCTION WITH STANDARD 442101.
2. TIE BARS SHALL BE NO. 6 DEFORMED BARS AT 24" CENTERS WHICH SHALL BE DRILLED AND GROUTED ACCORDING TO ARTICLE 420.05(b) OF THE STANDARD SPECIFICATIONS.

TYPICAL SECTION - CLASS B PATCH ON RAMPS



PLAN - CLASS B PATCH ON RAMPS

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
ct:\pw\work\p\idot\sparksgw\0215133\shh\details.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = Jul-14-2014 03:38:01PM	DATE -	REVISED -

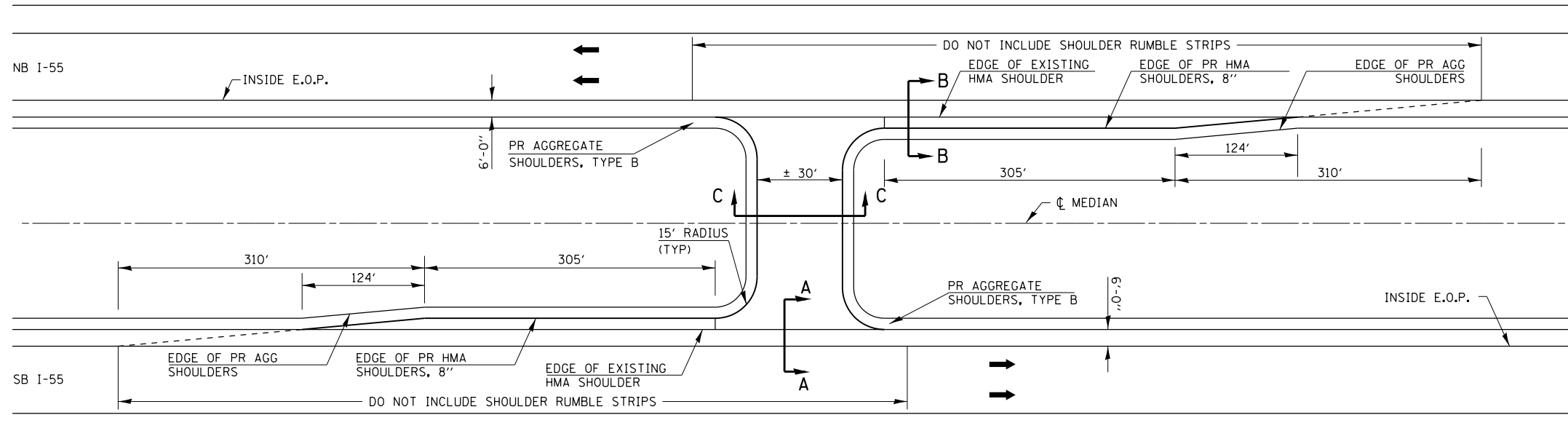
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CLASS B PATCHING
RAMP DETAIL**

SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.

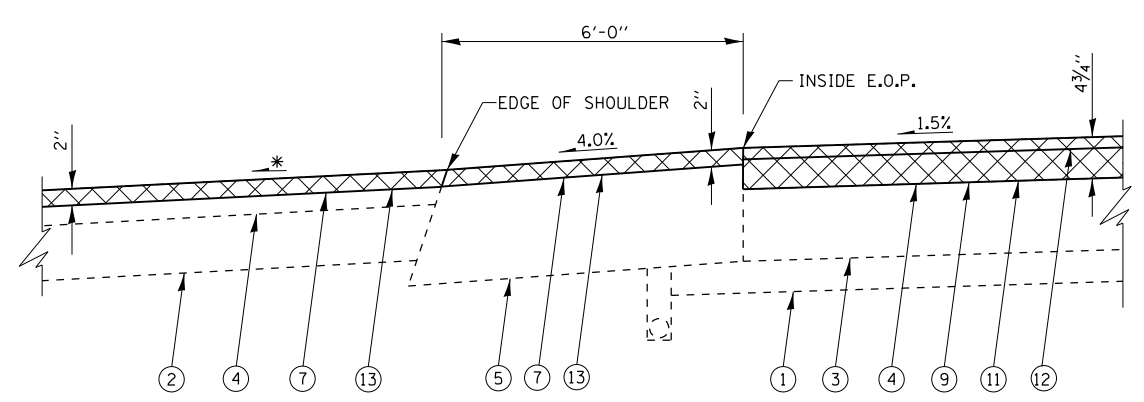
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	**	LOGAN	108	95
			CONTRACT NO. 72D41	
ILLINOIS FED. AID PROJECT				

EMERGENCY MEDIAN CROSSOVERS



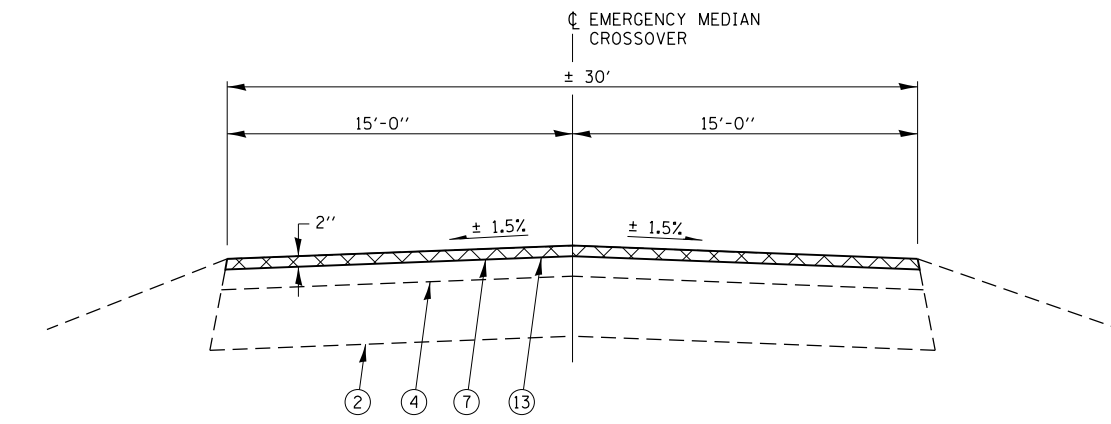
NOTE:
TWO DELINEATORS SHALL BE PLACED IN ADVANCE OF THE MEDIAN CROSSOVERS. ONE SHALL BE AT THE CROSSOVER, THE OTHER 800 FT IN ADVANCE OF THE FIRST. THE DELINEATOR AT THE CROSSOVER SHALL HAVE TWO REFLECTORS, AND THE DELINEATOR 800 FT IN ADVANCE SHALL HAVE THREE REFLECTORS

PLAN VIEW



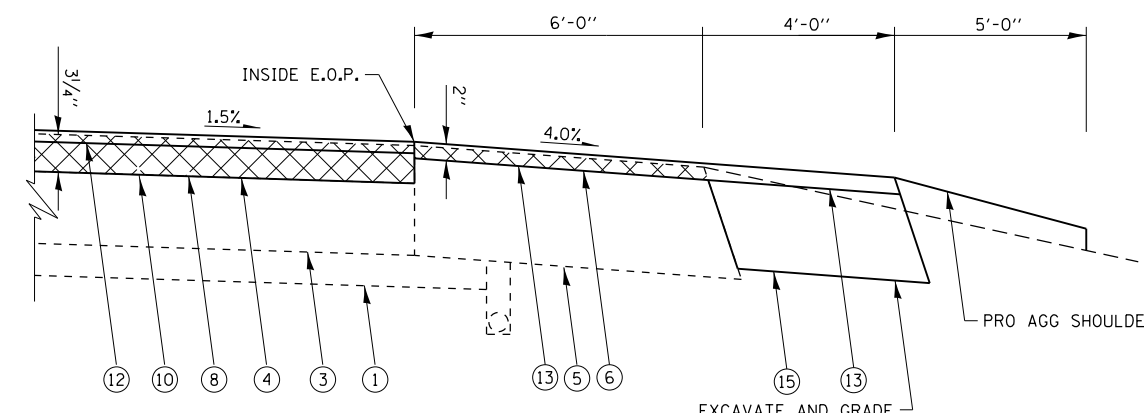
* - MAINTAIN SLOPES AWAY FROM MAINLINE PAVEMENT TO ENSURE PROPER DRAINAGE INTO MEDIAN DITCHES.

SECTION A - A



SECTION C - C

LEGEND	
①	EX AGGREGATE SUB-BASE, 4"
②	EX AGGREGATE BASE COURSE
③	EX P.C.C. PAVEMENT
④	EX HOT-MIX ASPHALT OVERLAY
⑤	EX HOT-MIX ASPHALT SHOULDER
⑥	PR HOT-MIX ASPHALT SURFACE REMOVAL 1 1/2"
⑦	PR HOT-MIX ASPHALT SURFACE REMOVAL 2"
⑧	PR HOT-MIX ASPHALT SURFACE REMOVAL 3 1/4"
⑨	PR HOT-MIX ASPHALT SURFACE REMOVAL 4 3/4"
⑩	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, 2 1/4"
⑪	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE 3 1/4"
⑫	PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, 1 1/2"
⑬	PR HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 2"
⑭	PR AGGREGATE SHOULDERS, TYPE B
⑮	PR HOT-MIX ASPHALT SHOULDERS, 8"



SECTION B - B

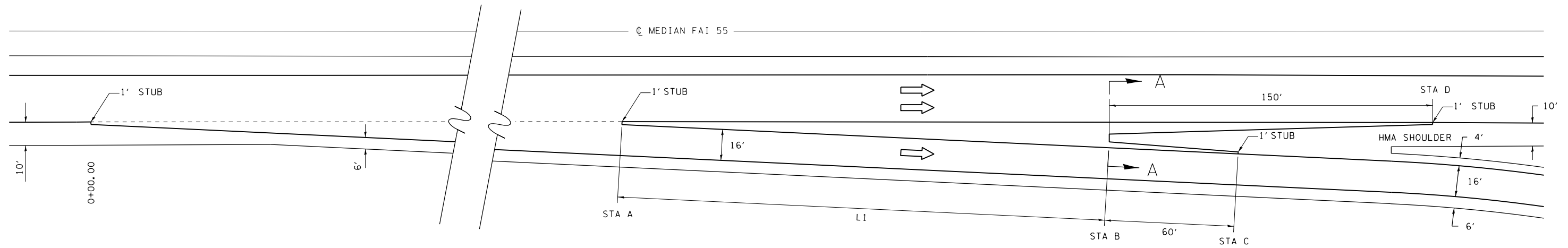
* - MAINTAIN SLOPES AWAY FROM MAINLINE PAVEMENT TO ENSURE PROPER DRAINAGE INTO MEDIAN DITCHES.

EMERGENCY CROSSOVER IMPROVEMENT

LOCATION

I-55	STA 486+91
I-55	STA 619+84
I-55	STA 671+25
I-55	STA 762+73

NOTE:
SEE SHOULDER IMPROVEMENTS TABLE IN SUMMARY OF SCHEDULES



PLAN - EXIT TERMINAL

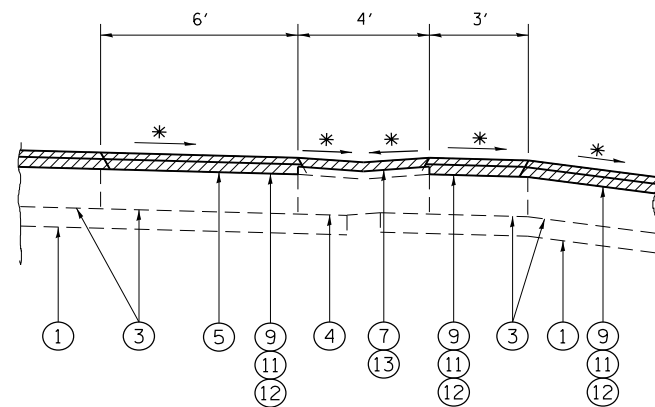
INTERCHANGE	RAMP	ADJACENT MAINLINE	STA A		STA B		STA C	STA D	L1
			RAMP	MAINLINE	RAMP	MAINLINE	RAMP	MAINLINE	(FEET)
BUS 55	B	SB I-55	3+24	703+37	5+06	705+20	6+64	706+76	182
BUS 55	D	NB I-55	0+00	735+33	3+86	731+35	N/A	N/A	386

INTERCHANGE	RAMP	ADJACENT MAINLINE	STA A		STA B		STA C	STA D	L1
			RAMP	MAINLINE	RAMP	MAINLINE	RAMP	MAINLINE	(FEET)
ATLANTA (CH# 6)	A	SB I-55	3+57	509+41	5+39	507+60	5+99	506+10	182
ATLANTA (CH# 6)	C	NB I-55	2+96	482+00	5+24	484+27	5+84	485+77	227

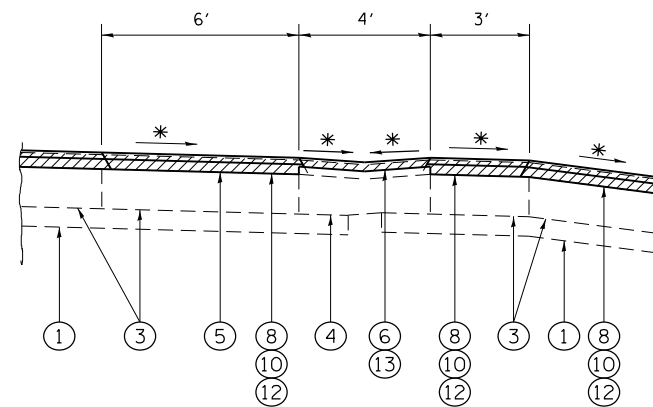
LEGEND

- 1 EX STABILIZED SUB-BASE
- 2 EX PCC BASE COURSE
- 3 EX PCC PAVEMENT
- 4 EX HOT-MIX ASPHALT SHOULDER
- 5 EX HOT-MIX ASPHALT
- 6 PR HOT-MIX ASPHALT SURFACE REMOVAL 1 1/2"
- 7 PR HOT-MIX ASPHALT SURFACE REMOVAL 2"
- 8 PR HOT-MIX ASPHALT SURFACE REMOVAL 3 1/4"
- 9 PR HOT-MIX ASPHALT SURFACE REMOVAL 4 3/4"
- 10 PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N105 2 1/4"
- 11 PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N105 3 1/4"
- 12 PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N105 1 1/2"
- 13 PR HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50 2"

* MATCH EXIST SLOPE

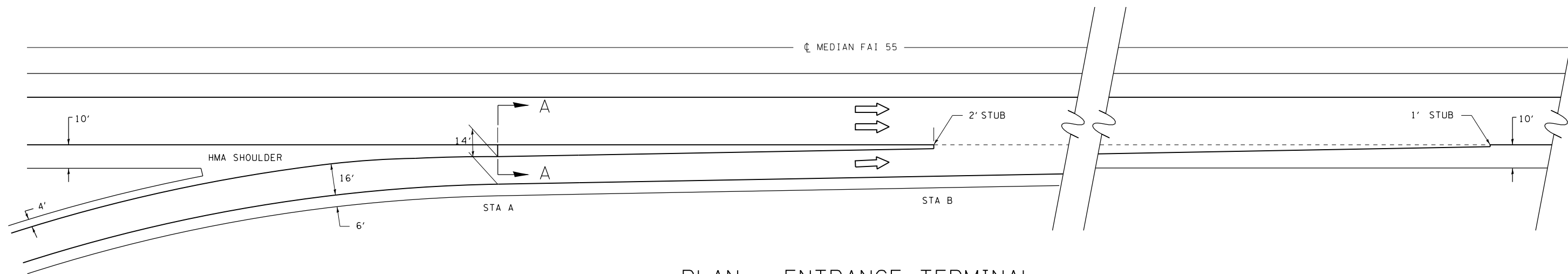


SECTION A-A
SOUTHBOUND LANES



SECTION A-A
NORTHBOUND LANES

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXIT RAMP DETAILS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\pwidot\sparksgw\0215133\shh-details.dgn		DRAWN -	REVISED -		•	••	LOGAN	108	97			
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 72D41							
PLOT DATE = Jul-14-2014 03:38:01PM		DATE -	REVISED -		SCALE:	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			



PLAN - ENTRANCE TERMINAL

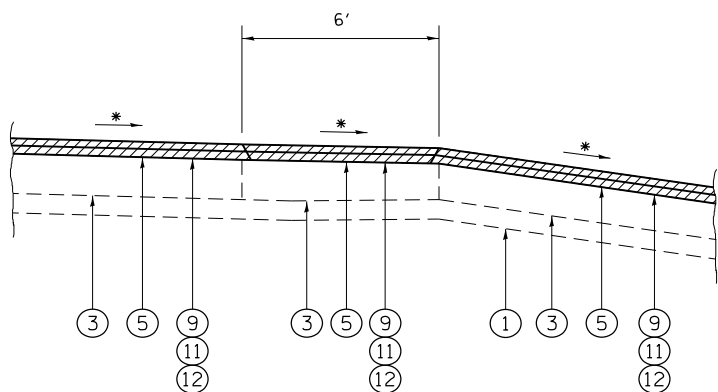
INTERCHANGE	RAMP	ADJACENT MAINLINE	STA A		STA B	
			RAMP	MAINLINE	RAMP	MAINLINE
BUS 55	A	NB I-55	29+45	718+31	31+76	720+56
BUS 55	C	SB I-55	N/A	N/A	17+57	704+76

INTERCHANGE	RAMP	ADJACENT MAINLINE	STA A		STA B	
			RAMP	MAINLINE	RAMP	MAINLINE
ATLANTA (CH# 6)	B	SB I-55	9+74	483+52	11+74	481+52
ATLANTA (CH# 6)	D	NB I-55	9+83	509+28	11+83	511+28

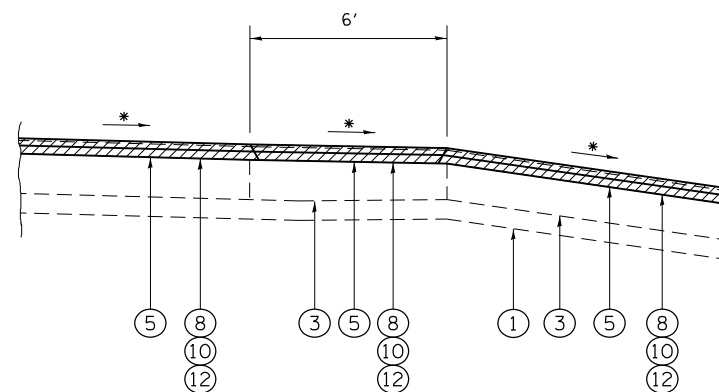
LEGEND

- 1 EX STABILIZED SUB-BASE
- 2 EX PCC BASE COURSE
- 3 EX PCC PAVEMENT
- 4 EX HOT-MIX ASPHALT SHOULDER
- 5 EX HOT-MIX ASPHALT
- 6 PR HOT-MIX ASPHALT SURFACE REMOVAL 1 1/2"
- 7 PR HOT-MIX ASPHALT SURFACE REMOVAL 2"
- 8 PR HOT-MIX ASPHALT SURFACE REMOVAL 3 1/4"
- 9 PR HOT-MIX ASPHALT SURFACE REMOVAL 4 3/4"
- 10 PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N105 2 1/4"
- 11 PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N105 3 1/4"
- 12 PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N105 1 1/2"
- 13 PR HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50 2"

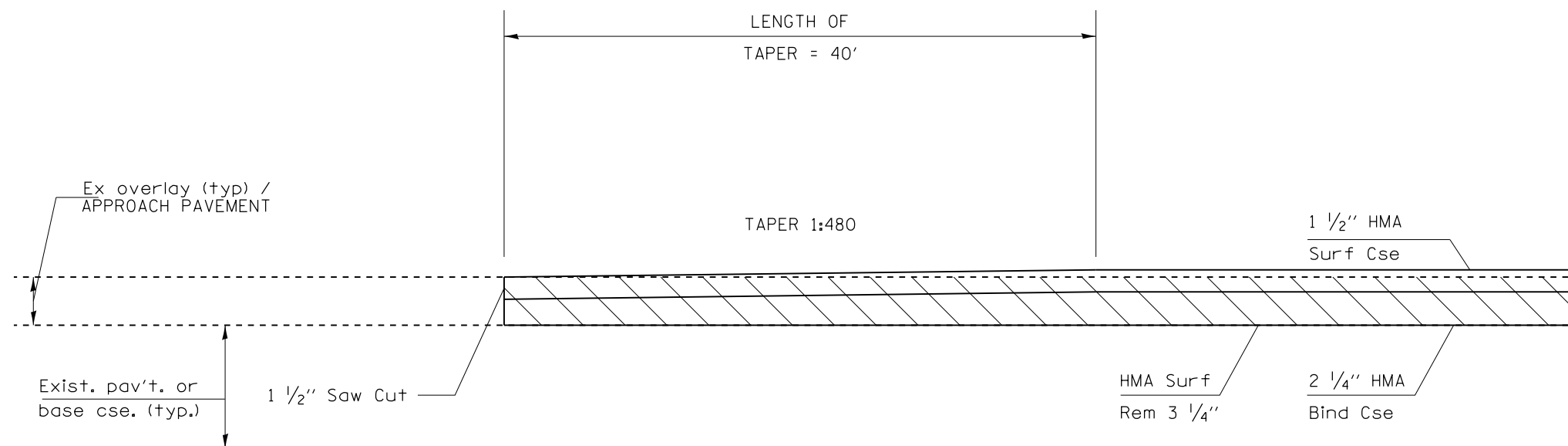
* MATCH EXIST SLOPE



SECTION A-A
(RAMP A)



SECTION A-A
(RAMP C)



BUTT JOINT DETAIL (A)

I-55	SB	STA 534+84.73
I-55	NB	STA 533+29.96
I-55	NB	STA 540+35.82
I-55	NB	STA 554+83.65
I-55	NB	STA 562+33.87
I-55	NB	STA 575+70.23
I-55	NB	STA 577+98.08
I-55	NB	STA 696+51.35
I-55	NB	STA 702+43.49
STA EQ		
I-55	NB	STA 190+00.00
I-55	NB	STA 590+00.00
I-55	SB	STA 590+00.00

ATLANTA (CH#6) INTERCHANGE

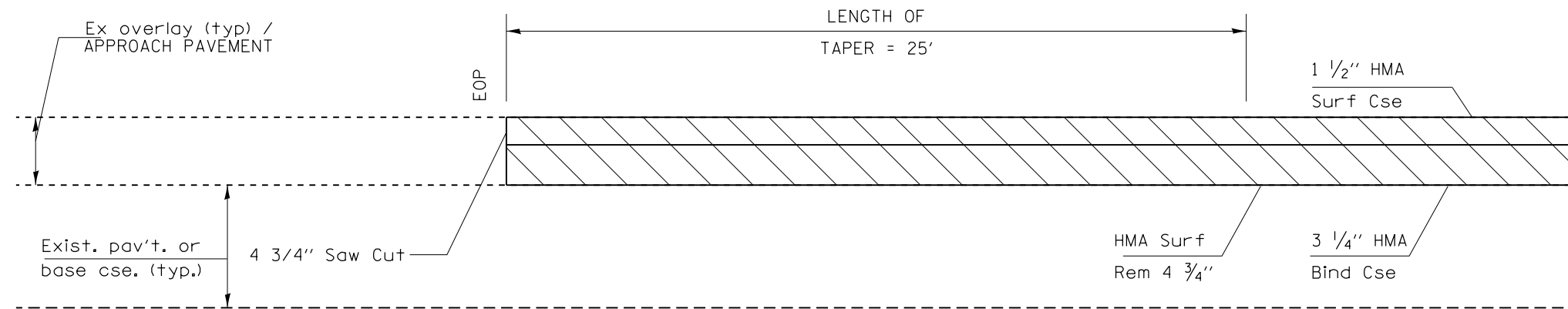
RAMP A	STA 22+74.34
RAMP B	STA 01+05.28
RAMP C	STA 23+44.47
RAMP D	STA 01+26.44

RDWY	F	STA 379+87.59
RDWY	A	STA 380+57.32
RDWY	B	STA 15+56.96
RDWY	B	STA 17+51.23
RAMP	D	STA 15+68.57
RAMP	D	STA 18+21.66
RAMP	E	STA 00+16.65
RDWY	B	STA 37+95.69

I-55	SB	STA 280+58.45
I-55	SB	STA 283+74.95
I-55	NB	STA 281+03.71
I-55	NB	STA 284+40.20

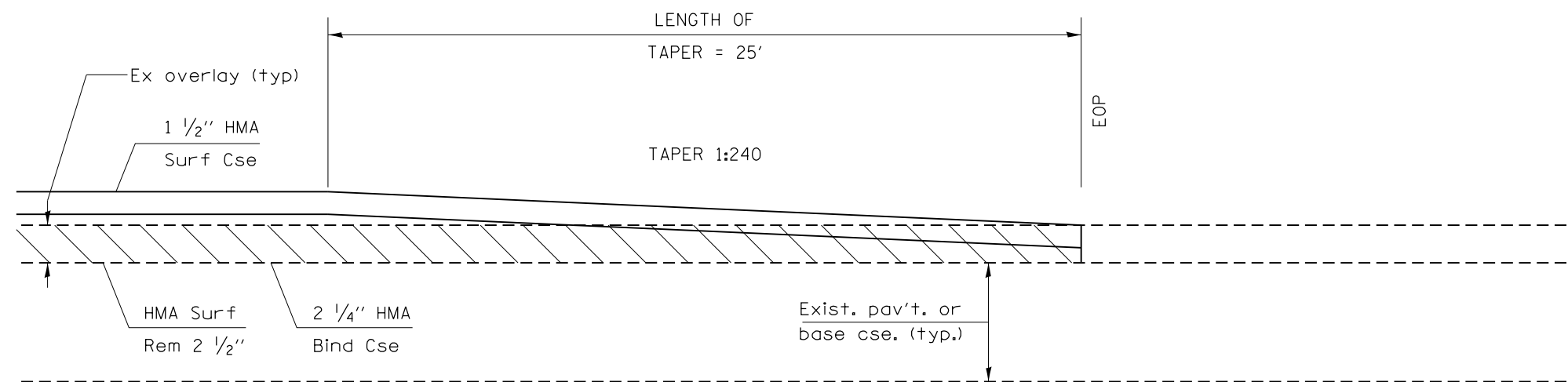
I-55	NB	STA 456+39.65
I-55	NB	STA 457+59.39
I-55	SB	STA 456+47.79
I-55	SB	STA 457+68.22

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND TRANSITION DETAIL			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pwork\pwork\sparksgw\0215133\shh-details.dgn		DRAWN -	REVISED -		•	••	LOGAN	108	99			
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 72D41			ILLINOIS FED. AID PROJECT				
PLOT DATE = Jul-14-2014 03:38:01PM		DATE -	REVISED -		SCALE:	SHEET NO. 1 OF 2 SHEETS	STA.	TO STA.	• 55/155			



BUTT JOINT DETAIL (B)

- I-55 SB STA 541+90.61
- I-55 SB STA 555+05.40
- I-55 SB STA 562+55.63
- I-55 SB STA 575+70.23
- I-55 SB STA 577+98.07
- I-55 SB STA 697+52.85
- I-55 SB STA 703+59.73



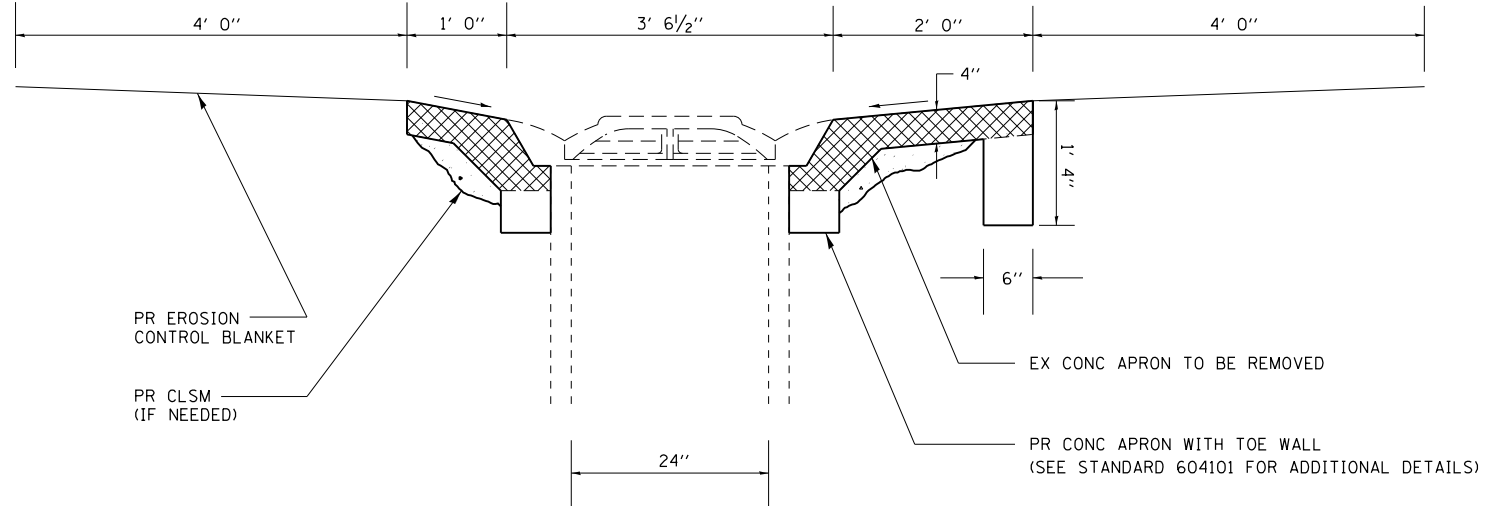
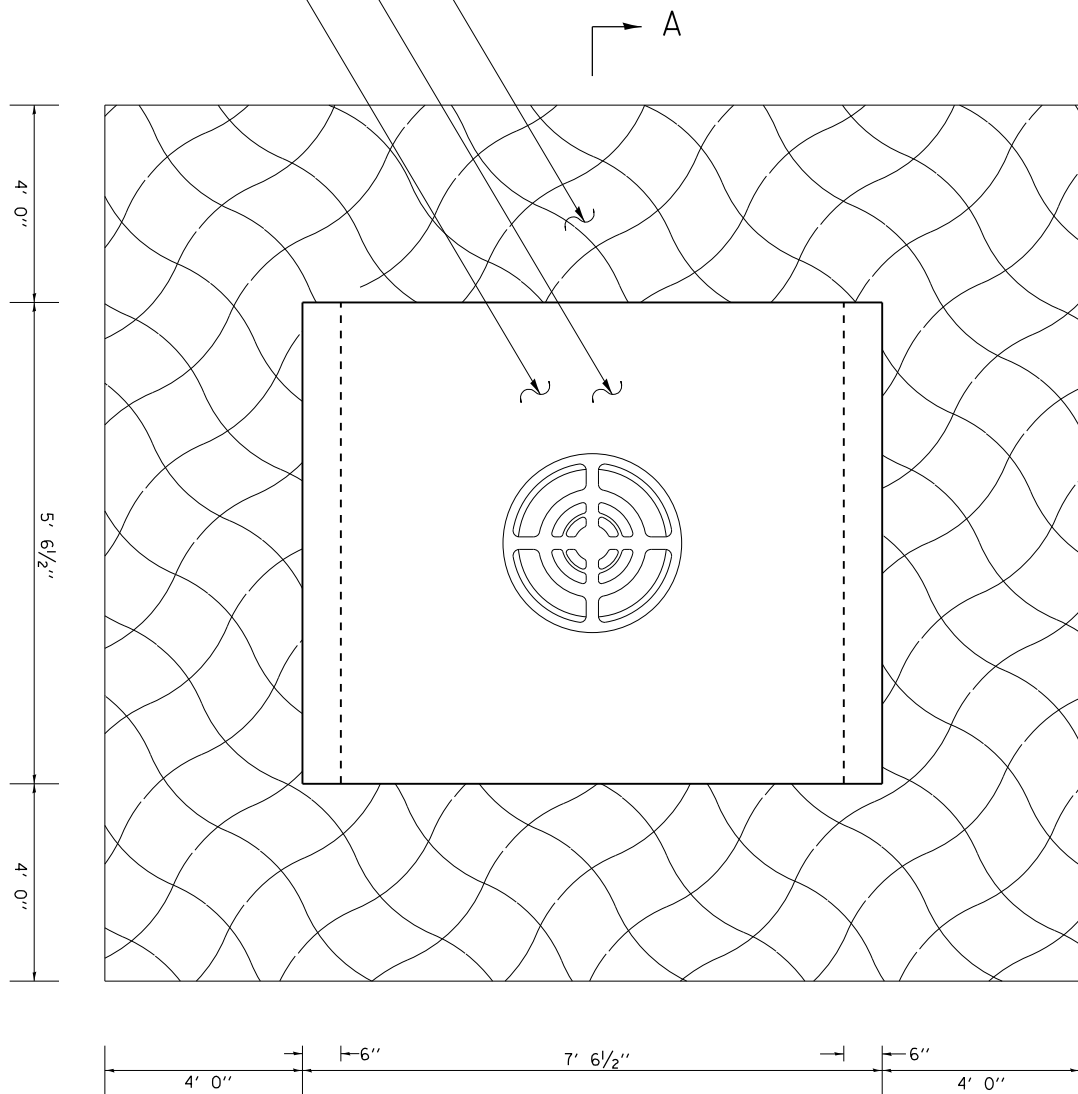
BUTT JOINT DETAIL (C)

BUSINESS 55 INTERCHANGE

- RAMP A STA 0+45.68
- RAMP B STA 31+95.00
- RAMP C STA 0+45.52
- RAMP D STA 23+15.70

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND TRANSITION DETAIL			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ce:\pwork\pwork\sparksgw\0215133\shh- details.dgn	DRAWN -	REVISED -	•					••	LOGAN	108	100	
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 72D41									
PLOT DATE = Jul-14-2014 03:38:01PM	DATE -	REVISED -	SCALE:		SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT				

PR EROSION CONTROL BLANKET
 PR CONC APRON
 EX CONC APRON

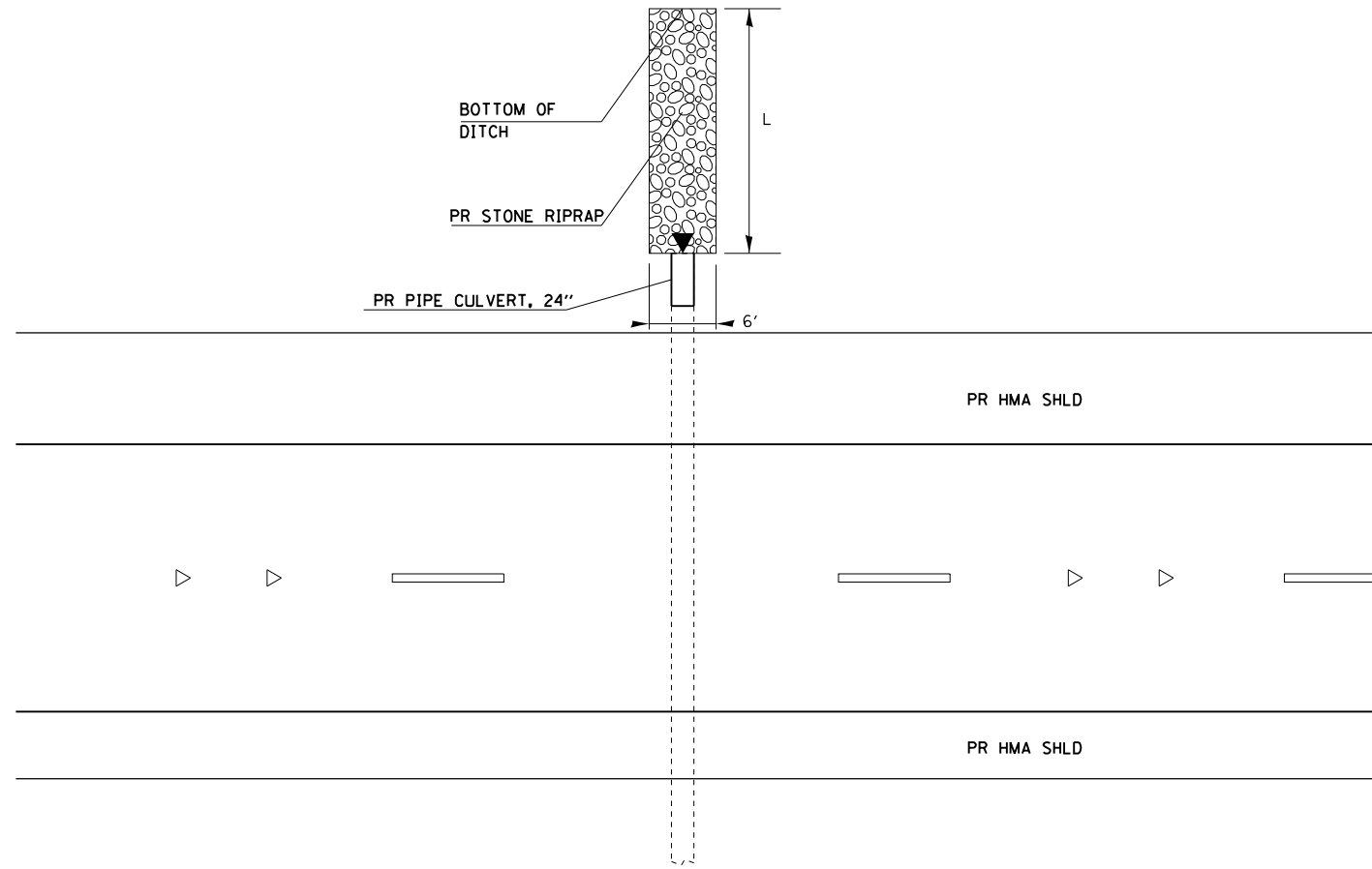


SECTION A-A

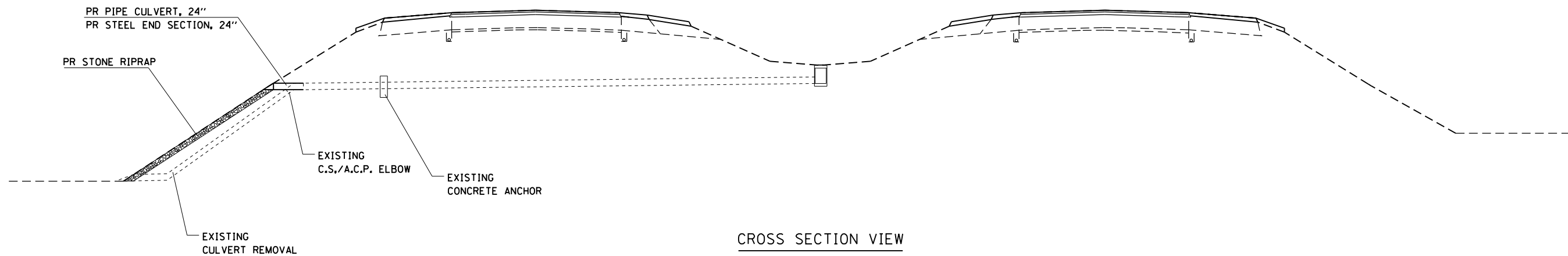
REPAIRS TO INLET STANDARD 2250		FOR INFORMATION ONLY (NOT TO BE PAID SEPARATELY)	
LOCATION	INLET TO BE RECONSTRUCTED (EACH)	EROSION CONTROL BLANKET (SQ YD)	CONTROLLED LOW STRENGTH MATERIAL (CU YD)
MED STA 787+00	1	18.7	1.5
NB STA 789+74	1	18.7	1.5
MED STA 311+00	1	18.7	1.5
TOTAL	3	56.1	4.5

NOTES:

1. SEE SEEDING SCHEDULE FOR SEEDING QUANTITIES.
2. SEE STANDARD 604101 FOR ADDITIONAL DETAILS NOT SHOWN. THE TOE WALL AT THE LONG END OF THE APRON IS IN ADDITION TO WHAT IS SHOWN IN STANDARD 604101.
3. CONTROLLED LOW STRENGTH MATERIAL TO BE USED IF DEEMED NECESSARY BY THE ENGINEER, THE QUANTITY IN THE ABOVE SCHEDULE IS ESTIMATED.



PLAN VIEW (SB)



CROSS SECTION VIEW

LOCATIONS

STA. 547+00
STA. 553+20

NOTE:
SEE RIPRAP AND PIPE CULVERT IMPROVEMENT SCHEDULES

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
ci:\pw\work\p\idot\sparksgw\0215133\shd\details.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = Jul-14-2014 03:38:02PM	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

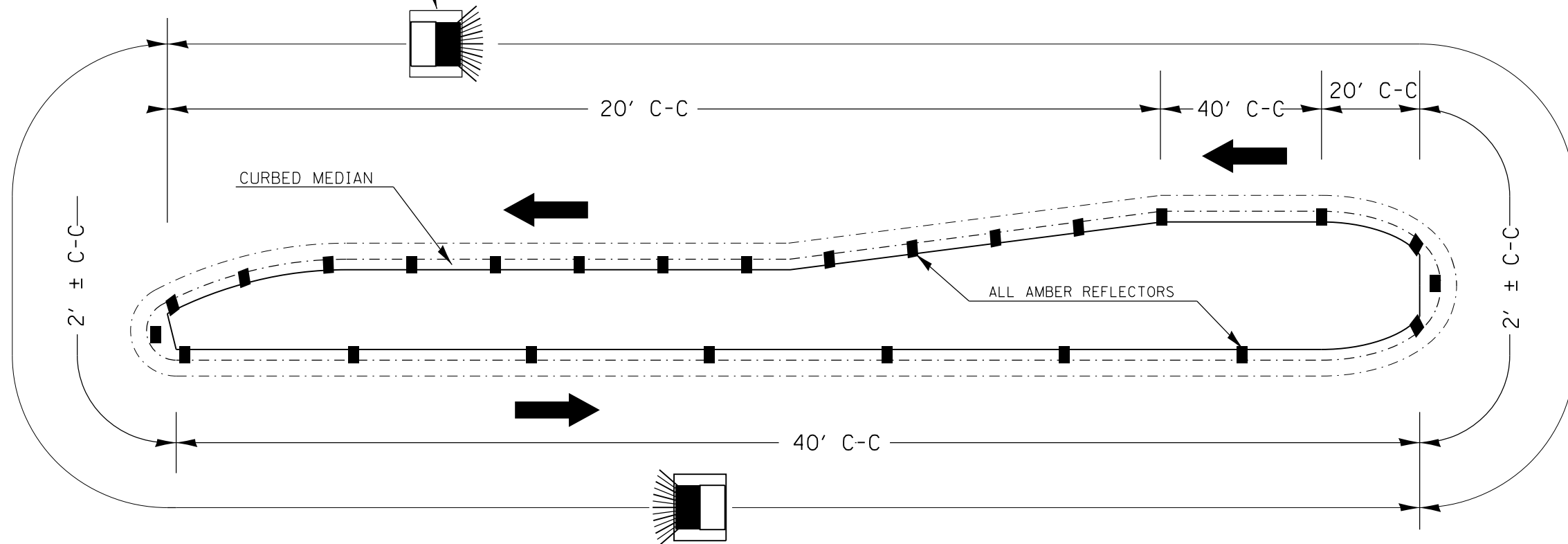
PIPE INSTALLATION DETAIL

SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.

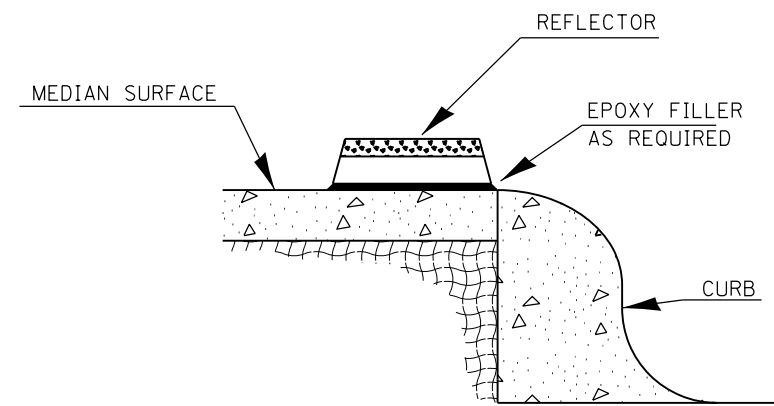
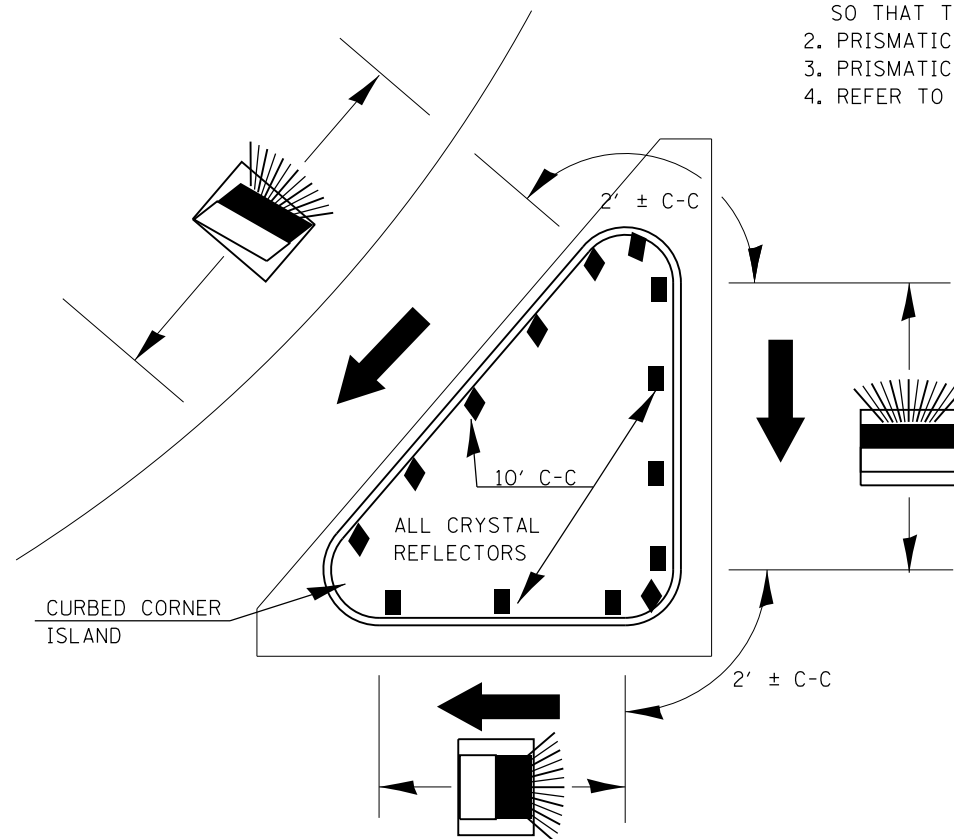
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	LOGAN	108	102
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72D41	

TYPICAL PLACEMENT OF PRISMATIC REFLECTORS ON CURBS

MARKER ORIENTATION WITHIN LIMITS SHOWN

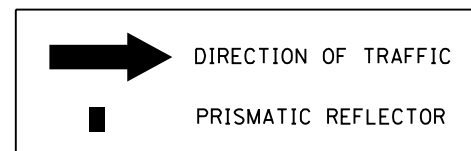


1. PRISMATIC REFLECTORS SHALL BE MONO-DIRECTIONAL AND POSITIONED SO THAT THE REFLECTIVE FACE IS FACING THE APPROACHING TRAFFIC.
2. PRISMATIC REFLECTORS SHALL BE SECURED IN PLACE WITH AN EPOXY ADHESIVE.
3. PRISMATIC REFLECTORS SHALL BE EITHER AMBER OR CRYSTAL IN COLOR.
4. REFER TO SCHEDULES FOR PRISMATIC REFLECTOR QUANTITIES.



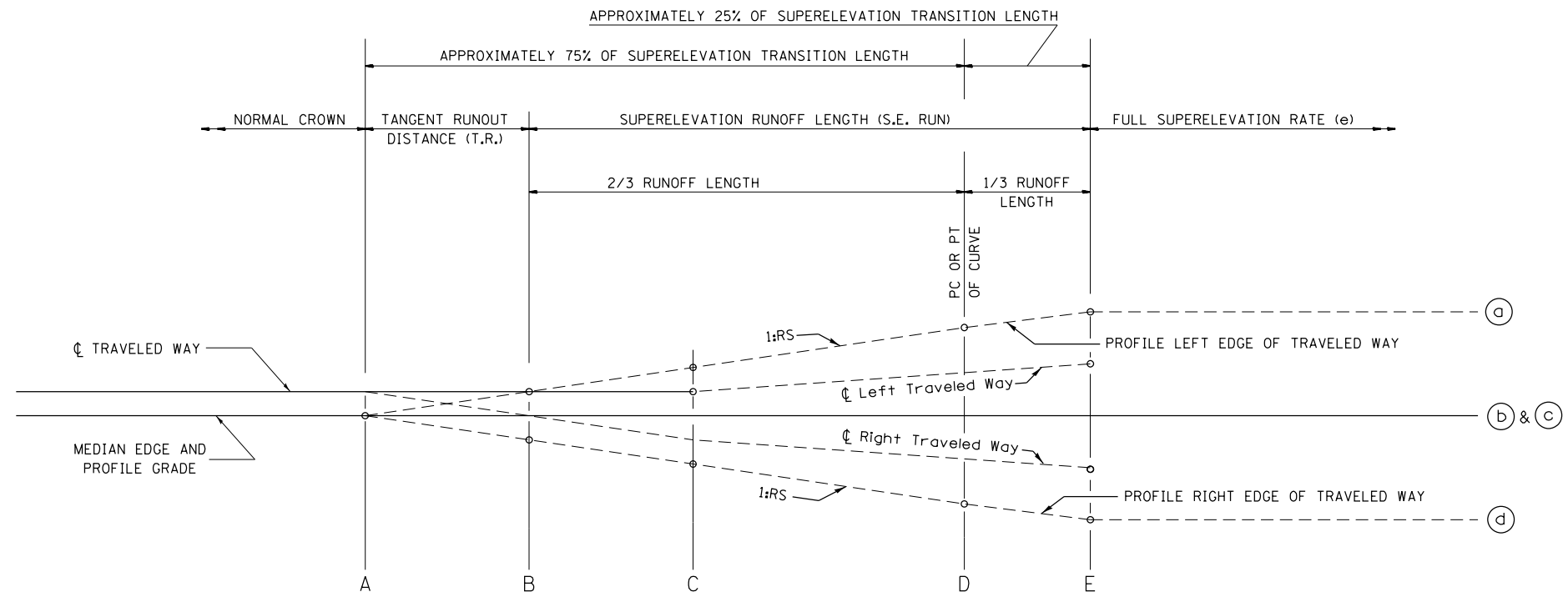
SECTION VIEW

LEGEND

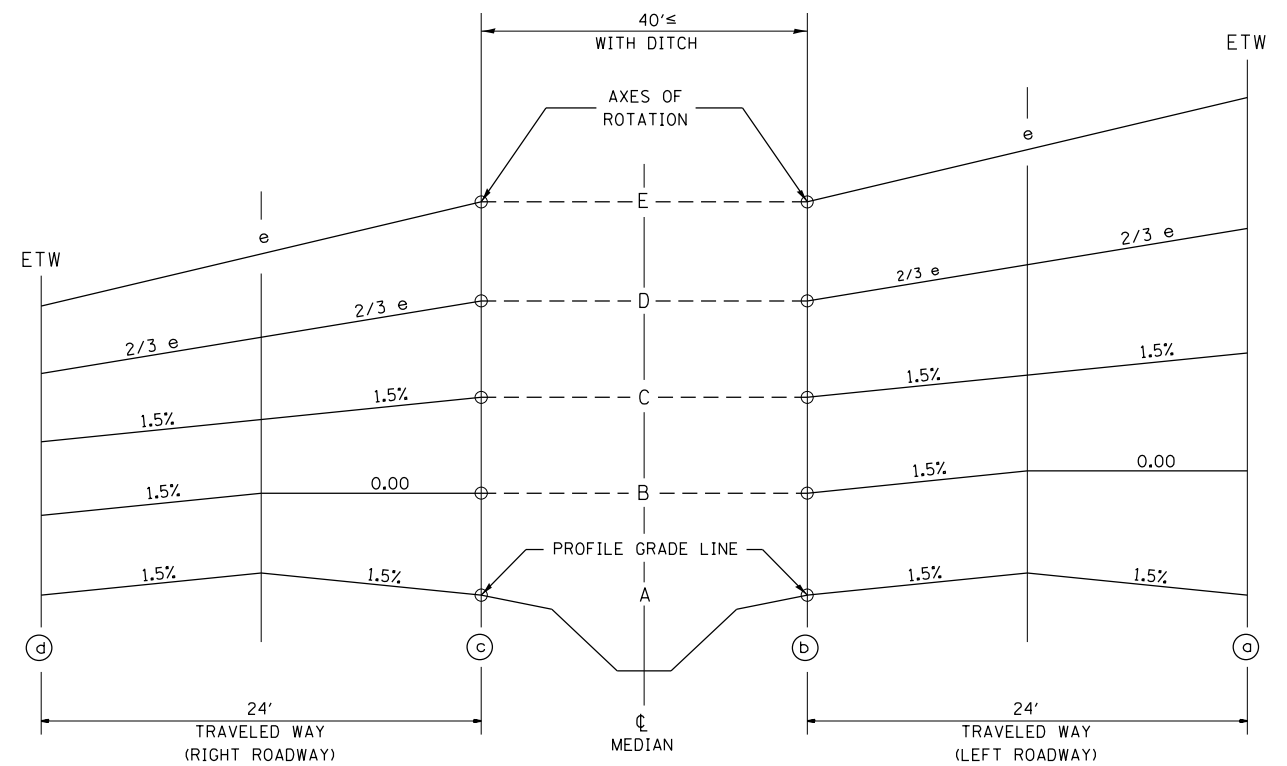
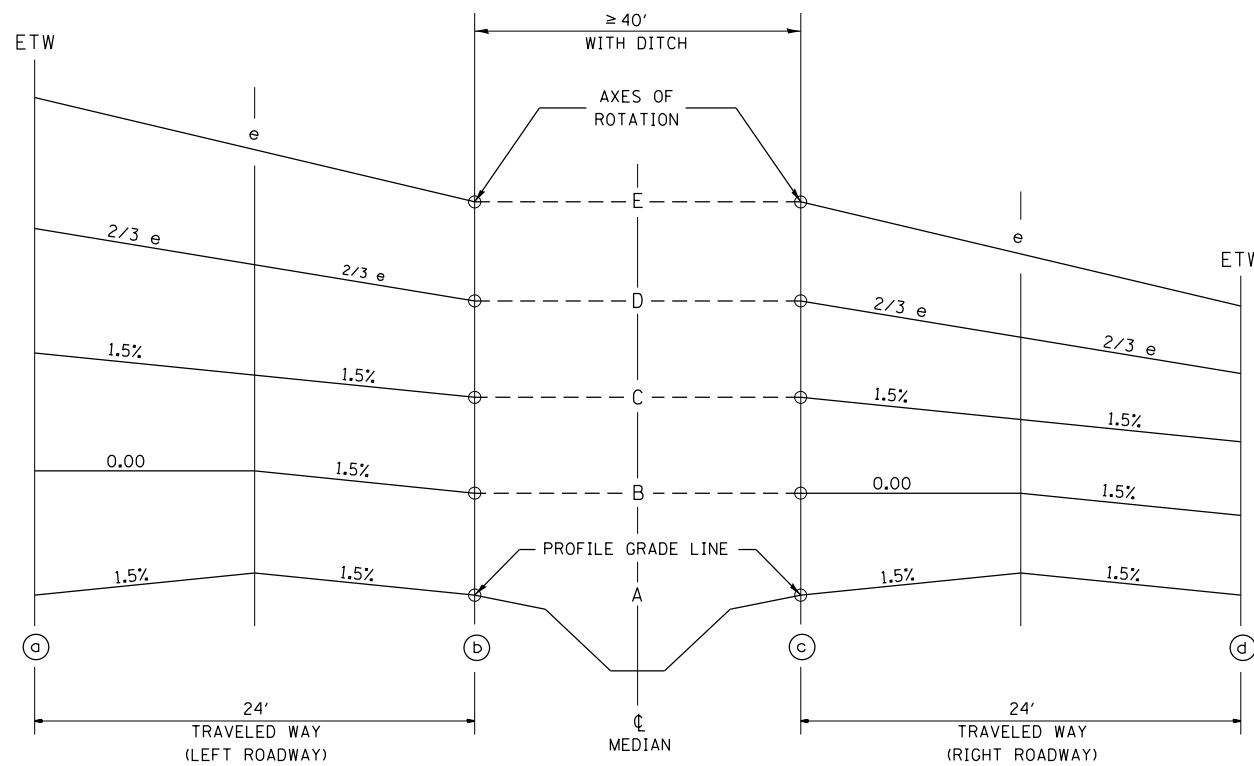


NOT TO SCALE

FILE NAME = PRISM.DGN	USER NAME = sparksgw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PRISMATIC REFLECTOR DETAILS (PLACED ON CURB)			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / 1in.	DRAWN -	REVISED -		SCALE:	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	LOGAN	108	103	
	PLOT DATE = Jul-14-2014 03:38:02PM	CHECKED -	REVISED -					CONTRACT NO. 72D41		ILLINOIS FED. AID PROJECT		
		DATE -	REVISED -							55/155		



TYPICAL PROFILE - S.E. TRANSITION



FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
ct:\pwork\pwork\sparksgw\0215133\sh...details.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUPERELEVATION TRANSITION
DETAIL FOR FOUR-LANE DIVIDED HIGHWAY

SCALE: SHEET NO. 1 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	**	LOGAN	108	104
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72D41	

I-55
 EXIST. CURVE 520
 PI STA. = 417+03.88
 Δ = 33° 10' 21" (RT)
 D = 1° 15' 01"
 R = 4,582.89'
 T = 1,365.02'
 L = 2,653.35'
 E = 198.97'
 e = 4.20%
 T.R. = 41.09
 S.E. RUN = 189.00
 P.C. STA. = 403+38.86
 P.T. STA. = 429+92.22

I-55
 EXIST. CURVE 521
 PI STA. = 466+21.84
 Δ = 35° 48' 03" (LT)
 D = 1° 30' 00"
 R = 3,819.92'
 T = 1,233.83'
 L = 2,386.85'
 E = 194.32'
 e = 4.20%
 T.R. = 41.09
 S.E. RUN = 189.00
 P.C. STA. = 453+88.01
 P.T. STA. = 477+74.85

I-55
 EXIST. CURVE 522
 PI STA. = 527+04.79
 Δ = 38° 00' 16" (LT)
 D = 1° 30' 01"
 R = 3,819.25'
 T = 1,315.24'
 L = 2,533.32'
 E = 194.32'
 e = 4.20%
 T.R. = 41.09
 S.E. RUN = 189.00
 P.C. STA. = 513+89.55
 P.T. STA. = 539+22.87

I-55
 EXIST. CURVE 523
 PI STA. = 564+06.90
 Δ = 40° 39' 20" (RT)
 D = 1° 30' 01"
 R = 3,818.67'
 T = 1,414.67'
 L = 2,709.62'
 E = 253.62'
 e = 4.20%
 T.R. = 41.09
 S.E. RUN = 189.00
 P.C. STA. = 549+92.23
 P.T. STA. = 577+01.85

TABLE OF SUPERELEVATION BREAK POINT LOCATIONS							
CURVE NO.	e	A	B	C	D	E	TRANSITION
520	4.2%	401+71.77	402+12.86	402+53.95	403+38.86	404+01.86	TRANS. IN
		431+59.31	431+18.22	430+77.13	429+92.22	429+29.22	TRANS. OUT
521	4.2%	452+20.92	452+62.01	453+03.10	453+88.01	454+51.01	TRANS. IN
		479+41.94	479+00.85	478+59.76	477+74.85	477+11.85	TRANS. OUT
522	4.2%	512+22.46	512+63.55	513+04.64	513+89.55	514+52.55	TRANS. IN
		540+89.96	540+48.87	540+07.78	539+22.87	538+59.87	TRANS. OUT
523	4.2%	548+25.14	548+66.23	549+07.32	549+92.23	550+55.23	TRANS. IN
		578+68.94	578+27.85	577+86.76	577+01.85	576+38.85	TRANS. OUT

EX CURVE 275
 PI STA = 385+39.37
 $\Delta = 18^\circ 47' 19''$ (RT)
 $D = 0^\circ 45' 03''$
 $R = 7,632.03'$
 $T = 1,262.70'$
 $L = 2,502.73'$
 $E = 103.75'$
 $e = 2.63\%$
 $T.R. = 47.22'$
 $S.E. RUN = 118.30'$
 $P.C. STA = 372+76.67$
 $P.T. STA = 397+79.40$

EX CURVE 525
 PI STA = 425+41.59
 $\Delta = 71^\circ 43' 29''$ (RT)
 $D = 1^\circ 29' 58''$
 $R = 3,821.10'$
 $T = 2,762.19'$
 $L = 4,783.38'$
 $E = 893.82'$
 $e = 4.58\%$
 $T.R. = 40.36'$
 $S.E. RUN = 206.05'$
 $P.C. STA = 397+79.40$
 $P.T. STA = 445+62.78$

EX CURVE 529
 PI STA = 28+90.64
 $\Delta = 90^\circ 30' 55''$ (LT)
 $D = 2^\circ 00' 00''$
 $R = 2,864.75'$
 $T = 2,890.64'$
 $L = 4,525.71'$
 $E = 1,204.97'$
 $e = 5.44\%$
 $T.R. = 45.00'$
 $S.E. RUN = 163.06'$
 $P.C. STA = 0+00.00$
 $P.T. STA = 45+25.71$

Axis of rotation about the center of two lanes.

* STA. EQUATION:
 STA. 0+00.00 AH (RDWY' C')
 STA. 452+88.81 BK ('I-55')

* STA. EQUATION:
 STA. 0+00.00 BK (RDWY' C')
 STA. 395+15.48 AH (RDWY' F')

NORTH BOUND
 Entrance Curve

Point	Sta.	InSide Shld(e)	LT(e)	RT(e)	OutSide shld(e)
A	Sta. 371+50.58	-4.00%	-1.50%	-1.50%	-4.00%
B	Sta. 371+97.81	-4.00%	-1.50%	0.00%	-4.00%
C	Sta. 372+45.03	-4.00%	-1.50%	1.50%	-4.00%
D	Sta. 372+76.67	-4.00%	-2.00%	2.00%	-4.00%
E	Sta. 373+16.1	-4.00%	-2.63%	2.63%	-4.00%

Exit Curve

Point	Sta.	InSide Shld(e)	LT(e)	RT(e)	InSide Shld(e)
*	Sta. 396+91.60	-4.00%	-2.63%	2.63%	-4.00%

* COMPOUND CURVE OF
 CURVE # 275 AND 525

FROM STA. 396+91.60 TO 397+79.40, e changes linearly from 2.63% to 4.58%

NORTH BOUND
 Entrance Curve

Point	Sta.	InSide Shld(e)	LT(e)	RT(e)	OutSide shld(e)
D	Sta. 397+79.40	-3.42%	4.58%	-4.58%	-4.58%

Exit Curve

Point	Sta.	InSide Shld(e)	LT(e)	RT(e)	InSide Shld(e)
E	Sta. 444+94.10	-3.42%	4.58%	-4.58%	-4.58%
D	Sta. 445+62.78	-4.00%	3.30%	-3.30%	-4.00%
C	Sta. 446+59.79	-4.00%	1.50%	-1.50%	-4.00%
B	Sta. 447+00.15	-4.00%	0.00%	-1.50%	-4.00%
A	Sta. 447+40.51	-4.00%	-1.50%	-1.50%	-4.00%

NORTH BOUND
 Entrance Curve

Point	Sta.	InSide Shld(e)	LT(e)	RT(e)	OutSide shld(e)
A	Sta. 454+42.52	-4.00%	-1.50%	-1.50%	-4.00%
B	Sta. 453+97.52	-4.00%	0.00%	-1.50%	-4.00%
C	Sta. 453+52.52	-4.00%	1.50%	-1.50%	-4.00%
D	* Sta. 0+00.00	-4.00%	3.62%	-3.62%	-4.00%
E	Sta. 0+54.35	-2.56%	5.44%	-5.44%	-5.44%

Exit Curve

Point	Sta.	InSide Shld(e)	LT(e)	RT(e)	InSide Shld(e)
E	Sta. 44+44.18	-2.56%	5.44%	-5.44%	-5.44%
D	* Sta. 45+25.71	-4.00%	3.62%	-3.62%	-4.00%
C	Sta. 46+49.61	-4.00%	1.50%	-1.50%	-4.00%
B	Sta. 46+88.77	-4.00%	0.00%	-1.50%	-4.00%
A	Sta. 47+27.92	-4.00%	-1.50%	-1.50%	-4.00%

EX CURVE 526
 PI STA = 10+68.62
 $\Delta = 30^\circ 46' 24''$ (LT)
 $D = 1^\circ 59' 12''$
 $R = 2,883.85$
 $T = 793.62'$
 $L = 1,548.90'$
 $E = 107.21'$
 $e = 5.42\%$
 $T.R. = 39.17'$
 $S.E. RUN = 243.73'$
 $P.C. STA = 2+75.00$
 $P.T. STA = 18+23.90$

Axis of rotation about the center of two lanes.

NORTH BOUND
 Entrance Curve

Point	Sta.	InSide Shld (e)	LT(e)	RT(e)	OutSide shld (e)
A	Sta. 1+21.68	-4.00%	-1.50%	-1.50%	-4.00%
B	Sta. 1+66.68	-4.00%	-1.50%	0.00%	-4.00%
C	Sta. 2+11.68	-4.00%	-1.50%	1.50%	-4.00%
D	Sta. 2+75.00	-4.00%	-3.61%	3.61%	-4.00%
E	Sta. 3+29.16	-5.42%	-5.42%	5.42%	-2.58%

Exit Curve

Point	Sta.	OutSide shld (e)	LT(e)	RT(e)	InSide Shld (e)
E	Sta. 17+69.74	-5.42%	-5.42%	5.42%	-2.58%
D	Sta. 18+23.90	-4.00%	-3.61%	3.61%	-4.00%
C	Sta. 18+87.22	-4.00%	-1.50%	1.50%	-4.00%
B	Sta. 19+32.22	-4.00%	-1.50%	0.00%	-4.00%
A	Sta. 19+77.22	-4.00%	-1.50%	-1.50%	-4.00%

* STA. EQUATION:
 STA. 0+00.00 AH (RDWY' A')
 STA. 0+94.00 BK (' I-155')

EX CURVE 535
 PI STA = 4+13.64
 $\Delta = 12^\circ 17' 20''$ (RT)
 $D = 1^\circ 29' 28''$
 $R = 3,842.35'$
 $T = 413.64'$
 $L = 824.11'$
 $E = 22.20'$
 $e = 4.56\%$
 $T.R. = 40.40'$
 $S.E. RUN = 205.09'$
 $P.C. STA = 0+00.00$
 $P.T. STA = 8+24.11$

SOUTH BOUND
 Entrance Curve

Point	Sta.	OutSide shld (e)	LT(e)	RT(e)	InSide Shld (e)
A	Sta. 2+71.13	-4.00%	-1.50%	-1.50%	-4.00%
B	Sta. 2+30.73	-4.00%	-1.50%	0.00%	-4.00%
C	Sta. 1+90.33	-4.00%	-1.50%	1.50%	-4.00%
D	* Sta. 0+00.00	-4.00%	-3.29%	3.29%	-4.00%
E	Sta. 0+68.36	-4.56%	-4.56%	4.56%	-3.44%

Exit Curve

Point	Sta.	OutSide shld (e)	LT(e)	RT(e)	InSide Shld (e)
E	Sta. 7+55.75	-4.56%	-4.56%	4.56%	-3.44%
D	Sta. 8+24.11	-4.00%	-3.29%	3.29%	-4.00%
C	Sta. 9+20.44	-4.00%	-1.50%	1.50%	-4.00%
B	Sta. 9+60.84	-4.00%	-1.50%	0.00%	-4.00%
A	Sta. 10+01.24	-4.00%	-1.50%	-1.50%	-4.00%

SOUTH BOUND
 Entrance Curve

Point	Sta.	OutSide shld (e)	LT(e)	RT(e)	InSide Shld (e)
A	Sta. 620+88.8	-4.00%	-1.50%	-1.50%	-4.00%
B	Sta. 621+36.04	-4.00%	-1.50%	0.00%	-4.00%
C	Sta. 621+83.27	-4.00%	-1.50%	1.50%	-4.00%
D	Sta. 622+14.86	-4.00%	-2.00%	2.00%	-4.00%
E	Sta. 622+54.27	-4.00%	-2.63%	2.63%	-4.00%

Exit Curve

Point	Sta.	OutSide shld (e)	LT(e)	RT(e)	InSide Shld (e)
E	Sta. 639+08.77	-4.00%	-2.63%	2.63%	-4.00%
D	Sta. 639+48.18	-4.00%	-2.00%	2.00%	-4.00%
C	Sta. 639+79.77	-4.00%	-1.50%	1.50%	-4.00%
B	Sta. 640+27.	-4.00%	-1.50%	0.00%	-4.00%
A	Sta. 640+74.24	-4.00%	-1.50%	-1.50%	-4.00%

NORTH BOUND
 Entrance Curve

Point	Sta.	InSide Shld (e)	LT(e)	RT(e)	OutSide shld (e)
A	Sta. 620+88.8	-4.00%	-1.50%	-1.50%	-4.00%
B	Sta. 621+36.04	-4.00%	-1.50%	0.00%	-4.00%
C	Sta. 621+83.27	-4.00%	-1.50%	1.50%	-4.00%
D	Sta. 622+14.86	-4.00%	-2.00%	2.00%	-4.00%
E	Sta. 622+54.27	-4.00%	-2.63%	2.63%	-4.00%

Exit Curve

Point	Sta.	OutSide shld (e)	LT(e)	RT(e)	InSide Shld (e)
E	Sta. 639+08.77	-4.00%	-2.63%	2.63%	-4.00%
D	Sta. 639+48.18	-4.00%	-2.00%	2.00%	-4.00%
C	Sta. 639+79.77	-4.00%	-1.50%	1.50%	-4.00%
B	Sta. 640+27.	-4.00%	-1.50%	0.00%	-4.00%
A	Sta. 640+74.24	-4.00%	-1.50%	-1.50%	-4.00%

EX CURVE 514
 PI STA = 630+85.26
 $\Delta = 13^\circ 00' 21''$ (LT)
 $D = 0^\circ 45' 01''$
 $R = 7,635.91'$
 $T = 870.40'$
 $L = 1,733.32'$
 $E = 49.45'$
 $e = 2.63\%$
 $T.R. = 47.23'$
 $S.E. RUN = 118.23'$
 $P.C. STA = 622+14.86$
 $P.T. STA = 639+48.18$

EX CURVE 515
 PI STA = 684+14.32
 $\Delta = 13^\circ 00' 17''$ (RT)
 $D = 0^\circ 45' 01''$
 $R = 7,636.63'$
 $T = 870.40'$
 $L = 1,733.32'$
 $E = 49.44'$
 $e = 2.63\%$
 $T.R. = 47.23'$
 $S.E. RUN = 118.22'$
 $P.C. STA = 675+43.92$
 $P.T. STA = 692+77.24$

SOUTH BOUND
 Entrance Curve

Point	Sta.	OutSide shld (e)	LT(e)	RT(e)	InSide Shld(e)
A	Sta. 674+17.87	-4.00%	-1.50%	-1.50%	-4.00%
B	Sta. 674+65.1	-4.00%	0.00%	-1.50%	-4.00%
C	Sta. 675+12.34	-4.00%	1.50%	-1.50%	-4.00%
D	Sta. 675+43.92	-4.00%	2.00%	-2.00%	-4.00%
E	Sta. 675+83.33	-4.00%	2.63%	-2.63%	-4.00%

Exit Curve

Point	Sta.	OutSide shld (e)	LT(e)	RT(e)	InSide Shld(e)
E	Sta. 692+37.83	-4.00%	2.63%	-2.63%	-4.00%
D	Sta. 692+77.24	-4.00%	2.00%	-2.00%	-4.00%
C	Sta. 693+08.82	-4.00%	1.50%	-1.50%	-4.00%
B	Sta. 693+56.06	-4.00%	0.00%	-1.50%	-4.00%
A	Sta. 694+03.29	-4.00%	-1.50%	-1.50%	-4.00%

SOUTH BOUND
 Entrance Curve

Point	Sta.	OutSide shld (e)	LT(e)	RT(e)	InSide Shld(e)
A	Sta. 706+40.77	-4.00%	-1.50%	-1.50%	-4.00%
B	Sta. 706+79.92	-4.00%	-1.50%	0.00%	-4.00%
C	Sta. 707+19.07	-4.00%	-1.50%	1.50%	-4.00%
D	Sta. 708+43.	-4.00%	-3.87%	3.87%	-4.00%
E	Sta. 709+24.54	-5.44%	-5.44%	5.44%	-2.56%

Exit Curve

Point	Sta.	OutSide shld (e)	LT(e)	RT(e)	InSide Shld(e)
E	Sta. 742+26.18	-5.44%	-5.44%	5.44%	-2.56%
D	Sta. 743+07.72	-4.00%	-3.87%	3.87%	-4.00%
C	Sta. 744+31.65	-4.00%	-1.50%	1.50%	-4.00%
B	Sta. 744+70.8	-4.00%	-1.50%	0.00%	-4.00%
A	Sta. 745+09.95	-4.00%	-1.50%	-1.50%	-4.00%

SOUTH BOUND
 Entrance Curve

Point	Sta.	OutSide shld (e)	LT(e)	RT(e)	InSide Shld(e)
A	Sta. 759+74.48	-4.00%	-1.50%	-1.50%	-4.00%
B	Sta. 760+21.71	-4.00%	-1.50%	0.00%	-4.00%
C	Sta. 760+68.94	-4.00%	-1.50%	1.50%	-4.00%
D	Sta. 761+00.55	-4.00%	-2.00%	2.00%	-4.00%
E	Sta. 761+39.97	-4.00%	-2.63%	2.63%	-4.00%

Exit Curve

Point	Sta.	OutSide shld (e)	LT(e)	RT(e)	InSide Shld(e)
E	Sta. 789+24.41	-4.00%	-2.63%	2.63%	-4.00%
D	Sta. 789+63.83	-4.00%	-2.00%	2.00%	-4.00%
C	Sta. 789+95.44	-4.00%	-1.50%	1.50%	-4.00%
B	Sta. 790+42.67	-4.00%	-1.50%	0.00%	-4.00%
A	Sta. 790+89.9	-4.00%	-1.50%	-1.50%	-4.00%

EX CURVE 516
 PI STA = 728+22.98
 $\Delta = 69^\circ 18' 59''$ (LT)
 $D = 2^\circ 00' 02''$
 $R = 2,863.92'$
 $T = 1,979.98'$
 $L = 3,464.77'$
 $E = 617.80'$
 $e = 5.44\%$
 $T.R. = 39.15'$
 $S.E. RUN = 244.62'$
 $P.C. STA = 708+43.00$
 $P.T. STA = 743+07.72$

EX CURVE 517
 PI STA = 775+49.21
 $\Delta = 21^\circ 29' 19''$ (LT)
 $D = 0^\circ 45' 02''$
 $R = 7,634.43'$
 $T = 1,448.66'$
 $L = 2,863.28'$
 $E = 136.23'$
 $e = 2.63\%$
 $T.R. = 47.23'$
 $S.E. RUN = 118.26'$
 $P.C. STA = 761+00.55$
 $P.T. STA = 789+63.83$

NORTH BOUND
 Entrance Curve

Point	Sta.	InSide Shld(e)	LT(e)	RT(e)	OutSide shld (e)
A	Sta. 674+17.87	-4.00%	-1.50%	-1.50%	-4.00%
B	Sta. 674+65.1	-4.00%	0.00%	-1.50%	-4.00%
C	Sta. 675+12.34	-4.00%	1.50%	-1.50%	-4.00%
D	Sta. 675+43.92	-4.00%	2.00%	-2.00%	-4.00%
E	Sta. 675+83.33	-4.00%	2.63%	-2.63%	-4.00%

Exit Curve

Point	Sta.	InSide Shld(e)	LT(e)	RT(e)	InSide Shld(e)
E	Sta. 692+37.83	-4.00%	2.63%	-2.63%	-4.00%
D	Sta. 692+77.24	-4.00%	2.00%	-2.00%	-4.00%
C	Sta. 693+08.82	-4.00%	1.50%	-1.50%	-4.00%
B	Sta. 693+56.06	-4.00%	0.00%	-1.50%	-4.00%
A	Sta. 694+03.29	-4.00%	-1.50%	-1.50%	-4.00%

NORTH BOUND
 Entrance Curve

Point	Sta.	InSide Shld(e)	LT(e)	RT(e)	OutSide shld (e)
A	Sta. 706+40.77	-4.00%	-1.50%	-1.50%	-4.00%
B	Sta. 706+79.92	-4.00%	-1.50%	0.00%	-4.00%
C	Sta. 707+19.07	-4.00%	-1.50%	1.50%	-4.00%
D	Sta. 708+43.	-4.00%	-3.87%	3.87%	-4.00%
E	Sta. 709+24.54	-5.44%	-5.44%	5.44%	-2.56%

Exit Curve

Point	Sta.	InSide Shld(e)	LT(e)	RT(e)	InSide Shld(e)
E	Sta. 742+26.18	-5.44%	-5.44%	5.44%	-2.56%
D	Sta. 743+07.72	-4.00%	-3.87%	3.87%	-4.00%
C	Sta. 744+31.65	-4.00%	-1.50%	1.50%	-4.00%
B	Sta. 744+70.8	-4.00%	-1.50%	0.00%	-4.00%
A	Sta. 745+09.95	-4.00%	-1.50%	-1.50%	-4.00%

NORTH BOUND
 Entrance Curve

Point	Sta.	InSide Shld(e)	LT(e)	RT(e)	OutSide shld (e)
A	Sta. 759+74.48	-4.00%	-1.50%	-1.50%	-4.00%
B	Sta. 760+21.71	-4.00%	-1.50%	0.00%	-4.00%
C	Sta. 760+68.94	-4.00%	-1.50%	1.50%	-4.00%
D	Sta. 761+00.55	-4.00%	-2.00%	2.00%	-4.00%
E	Sta. 761+39.97	-4.00%	-2.63%	2.63%	-4.00%

Exit Curve

Point	Sta.	InSide Shld(e)	LT(e)	RT(e)	InSide Shld(e)
E	Sta. 789+24.41	-4.00%	-2.63%	2.63%	-4.00%
D	Sta. 789+63.83	-4.00%	-2.00%	2.00%	-4.00%
C	Sta. 789+95.44	-4.00%	-1.50%	1.50%	-4.00%
B	Sta. 790+42.67	-4.00%	-1.50%	0.00%	-4.00%
A	Sta. 790+89.9	-4.00%	-1.50%	-1.50%	-4.00%