


LETTING ITEM NO: 02A
LETTING DATE: SEPTEMBER 19, 2025

D1035
TOTAL SHEETS = 22

DIXON MUNICIPAL AIRPORT CHARLES R. WALGREEN FIELD DIXON, ILLINOIS

CONSTRUCTION PLANS FOR DIXON MUNICIPAL AIRPORT

REHABILITATE RUNWAY 12/30



Know what's **below**.
Call before you dig.

J.U.L.I.E.
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE. SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.


CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

ILLINOIS PROJECT: C73-5064
SBG PROJECT: 3-17-SBGP-197

JULY 25 , 2025



D. Kyle Peabody
07/25/2025
EXPIRES: 11/30/2025



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22004684.00

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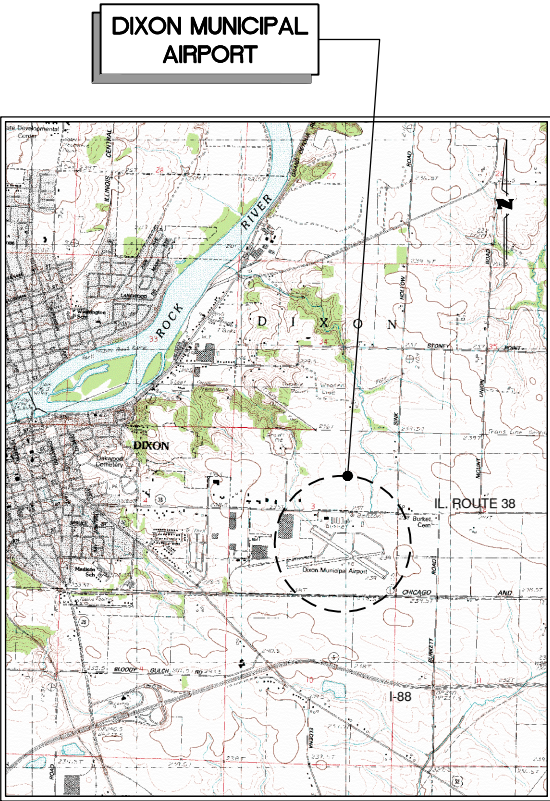
SUBMITTED BY *D. Kyle Peabody*
D. KYLE PEABODY P.E.

DATE JULY 25, 2025

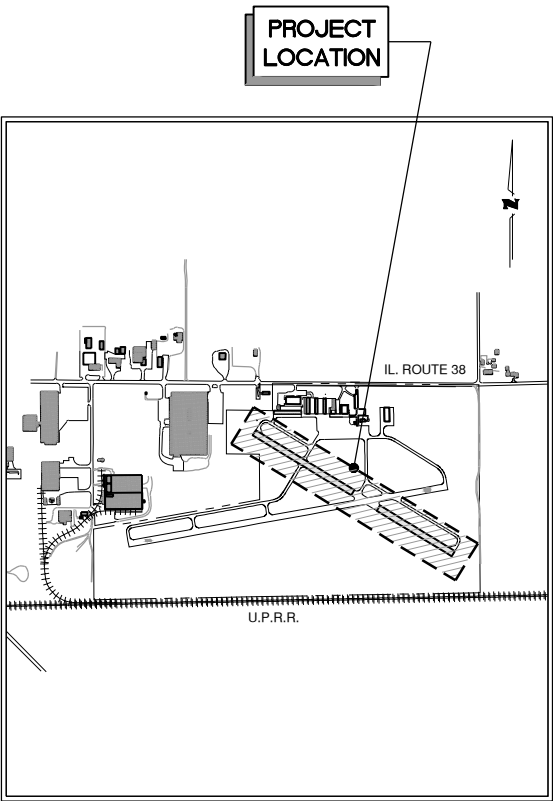
DIXON MUNICIPAL AIRPORT
CHARLES R. WALGREEN FIELD
DIXON, ILLINOIS

Matthew S. Heckman
MATTHEW S. HECKMAN
PUBLIC WORKS DIRECTOR/ASSISTANT CITY MANAGER

DATE 7/21/2025



LOCATION MAP



SITE PLAN

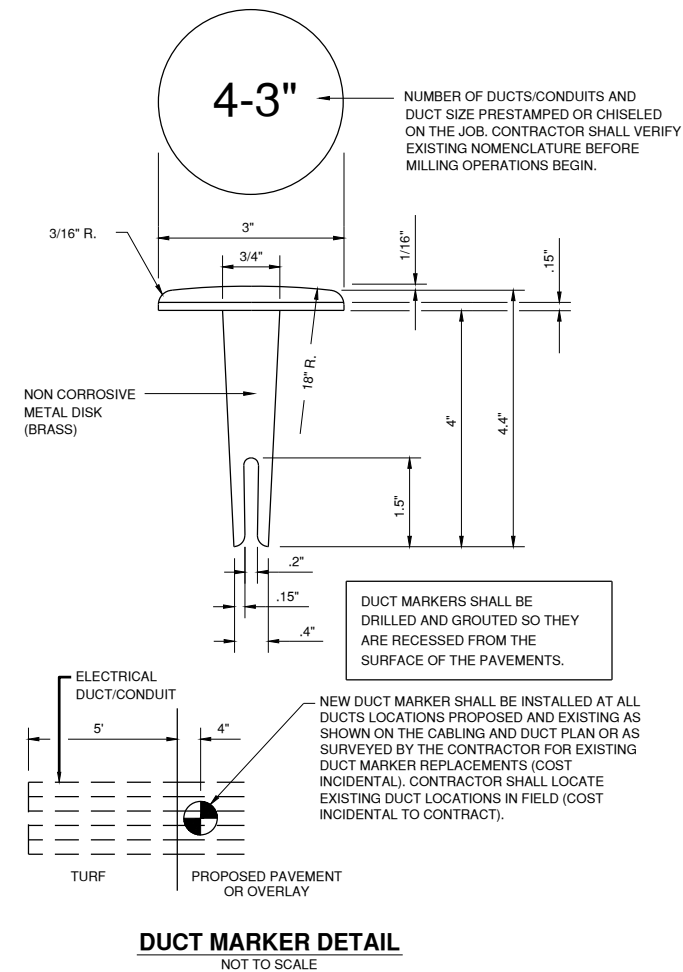
DESIGN INFORMATION	
APPROACH CATEGORY B DESIGN GROUP I	
DIXON MUNICIPAL AIRPORT	
TOWNSHIP: 21 NORTH RANGE: 9 EAST LEE COUNTY	DIXON TOWNSHIP (SECTION: 3) OPPOSITE LINCOLN HIGHWAY 38 (FRANKLIN GROVE ROAD)
UNICOM RADIO FREQUENCY - 123.05	

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22. GEOTECHNICAL ENGINEERING INFORMATION

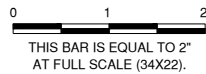
SUMMARY OF QUANTITIES

ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	L SUM	1	
AR150520	MOBILIZATION	L SUM	1	
AR152410	UNCLASSIFIED EXCAVATION	CU YD	100	
AR152480	SHOULDER ADJUSTMENT	SQ YD	2,600	
AR152540	SOIL STABILIZATION FABRIC	SQ YD	190	
AR156520	INLET PROTECTION	EACH	12	
AR208515	POROUS GRANULAR EMBANKMENT	CU YD	65	
AR209607	CRUSHED AGGREGATE BASE COURSE - 7"	SQ YD	190	
AR401610	BITUMINOUS SURFACE COURSE	TON	2,415	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1	
AR401650	BITUMINOUS PAVEMENT MILLING	SQ YD	20,970	
AR401915	REM & REP BIT PAVEMENT - TYPE A	SQ YD	185	
AR401916	REM & REP BIT PAVEMENT - TYPE B	SQ YD	920	
AR403673	REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	185	
AR603510	BITUMINOUS TACK COAT	GALLON	2,520	
AR620520	PAVEMENT MARKING - WATERBORNE	SQ FT	4,950	
AR620525	PAVEMENT MARKING - BLACK BORDER	SQ FT	3,000	
AR620900	PAVEMENT MARKING REMOVAL	SQ FT	470	
AR901510	SEEDING	ACRE	0.6	
AR908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	0.6	



IL. CONTRACT: **DI035**
IL. LETTING ITEM: **02A**
IL. PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

REVISIONS		
NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30**

INDEX TO SHEETS AND SUMMARY OF QUANTITIES

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DESIGN BY:	STL
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DRAWN BY: JRO

CHECKED BY: DKP

APPROVED BY: DKP

DATE: 07/25/2025

JOB No: 22004684-0

FINAL

BM-05-21, AM, TUDOR
BM-05-21, AM, TUDOR

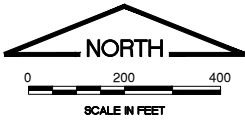
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NOTES

- AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH NEWLY CONSTRUCTED PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
- CONTRACTOR SHALL DISPOSE OF ALL PAVEMENT REMOVAL AND OTHER MISCELLANEOUS CONSTRUCTION DEBRIS OFF OF AIRPORT PROPERTY.
- PAVEMENT REMOVAL AND REPLACEMENT AREAS SHALL BE LAID OUT BY THE RESIDENT ENGINEER IN THE FIELD DURING CONSTRUCTION.

LEGEND

- NEW IMPROVEMENTS
- EXISTING BUILDING
- AIRPORT PROPERTY LINE
- BENCHMARK
- CONTROL POINT



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IL. LETTING ITEM: **02A**
IL. PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

REVISIONS

NUMBER	BY	DATE

0 1 2
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AT FULL SCALE (34X22).

DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

SITE PLAN AND PROJECT CONTROL PLAN

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THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ARCHITECT/ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER WITH RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL, RELOCATION OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ARCHITECT/ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

HORIZONTAL CONTROL

POINT	DESCRIPTION	NORTHING	EASTING
CP-1	IRON ROD WITH CAP	1883512.538	2492469.797
CP-2	IRON ROD WITH CAP	1883204.892	2492985.164
CP-3	IRON ROD WITH CAP	1882400.102	2494331.046
CP-4	IRON ROD WITH CAP	1882758.450	2493729.822

VERTICAL CONTROL

BENCHMARK	DESCRIPTION	NORTHING	EASTING	ELEVATION
BM #1	NGS MARKER - STAINLESS STEEL ROD IN SLEEVE IN TURF AREA JUST WEST OF RUNWAY 12/30 - RUNWAY 8/26 INTERSECTION	1882891.680	2493225.960	778.96
BM #2	NGS MARKER - STAINLESS STEEL ROD IN SLEEVE IN TURF AREA JUST EAST OF TAXIWAY A1	1882579.037	2491500.381	778.23

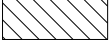
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CHECKED BY: DKP
APPROVED BY: DKP
DATE: 07/25/2025
JOB No: 22004684-00

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SHEET 3 OF 22 SHEETS

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WORK AREA	ALLOWABLE WORK PERIOD	OPERATION STATUS/RESTRICTIONS
PHASE 1 	NO RESTRICTIONS	RUNWAY 12/30 CLOSED RUNWAY 8/26 OPEN TAXIWAY B CLOSED TAXIWAY D CLOSED TAXIWAY E CLOSED ALL OTHER TAXIWAYS OPEN


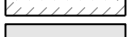
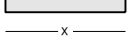


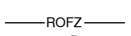







PHASE 1 - SUGGESTED SEQUENCE OF CONSTRUCTION

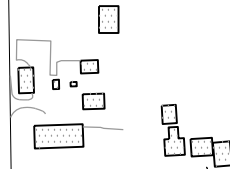
- COORDINATE CLOSURE OF RUNWAY 12/30 WITH THE RESIDENT ENGINEER AND PLACE CLOSED RUNWAY MARKERS.
- MARK AIR OPERATIONS AREA (A.O.A.) WITH LATHE AND RIBBON AND PLACE BARRICADES FOR TAXIWAY CLOSURES.
- CONSTRUCT PROPOSED IMPROVEMENTS INCLUDING RUNWAY PAVEMENT REHABILITATION, SHOULDER ADJUSTMENT, SEEDING AND MULCHING. CONSTRUCT FIRST COAT OF PAVEMENT MARKING WITHOUT BEADS AND NO BLACK BORDER ON RUNWAY AND CONNECTING TAXIWAY.
- OPEN PAVEMENT TO AIRCRAFT TRAFFIC.
- 30 DAY CURE PERIOD THEN CONSTRUCT SECOND COAT OF PAVEMENT MARKING WITH BEADS AND BLACK BORDER ON RUNWAY AND CONNECTING TAXIWAY.

NOTES

- WHEN HAUL ROUTE IS IN USE ON ACTIVE AIRFIELD PAVEMENTS, THE CONTRACTOR WILL BE REQUIRED TO HAVE A CROSSING GUARD OR CONTRACTOR'S ESCORT WITH RADIO FOR CONSTRUCTION PERSONNEL, CONSTRUCTION TRAFFIC, CONTRACTORS VEHICLES AND EQUIPMENT CROSSING BY, TO OR FROM WORK ZONE. STOP SIGNS SHALL BE IN PLACE AT ALL TIMES IN THIS AREA. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.
- THE INTENT OF THE PHASING PLANS IS TO MINIMIZE THE IMPACT OF CONSTRUCTION ON THE OPERATION OF THE AIRPORT. THE CONTRACTOR SHALL CONSTRUCT THE PROJECT AS OUTLINED IN THE PLANS UNLESS OTHERWISE APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL SUBMIT A CONSTRUCTION SCHEDULE. STRICT ADHERENCE TO THE APPROVED SCHEDULE WILL BE ENFORCED TO AVOID CONFLICTS WITH OTHER CONSTRUCTION ACTIVITIES ON THE AIRPORT AND THE ADVERSE EFFECTS THEY COULD HAVE ON AIRPORT OPERATIONS.
- THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT STAFF TO SCHEDULE THE RUNWAY/TAXIWAY CLOSURES. ITEMS SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT DEPENDABILITY AND MANPOWER AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THESE CRITICAL CLOSURES. THE AIRPORT MANAGER AND THE CONTRACTOR SHALL MUTUALLY AGREE ON THE EXACT DATE OF THE CLOSURE.
- CONTRACTOR MUST MAINTAIN ACCESS TO ALL APRON/HANGAR AREAS AT ALL TIMES. CONTRACTOR SHALL RELOCATE EQUIPMENT AT NO ADDITIONAL COST TO THE CONTRACT TO ALLOW AIRCRAFT TO PASS. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION OPERATIONS AT ALL APRON/HANGAR AREAS TO PROVIDE MINIMAL DISRUPTIONS TO AIRCRAFT MOVEMENT IN THAT AREA.
- THE CONTRACTOR SHALL SUFFICIENTLY MARK HIS WORK AREA TO PREVENT PERSONNEL AND EQUIPMENT FROM ACCESSING THE RUNWAY SAFETY AREA(S) WHILE THE RUNWAY IS OPEN TO AIR TRAFFIC. THE MANNER IN WHICH THE WORK AREA IS MARKED SHALL NOT BE CONSIDERED A HAZARD TO AIR TRAFFIC. PREVIOUS METHODS INCLUDE THE INSTALLATION OF LATHE AND RIBBON AT SUFFICIENT SPACING TO PROVIDE GOOD VISIBILITY.
- THE CONTRACTOR SHALL REMOVE ANY LOOSE MATERIAL AND DEBRIS IMMEDIATELY OFF THE BULLSEYE OR AS DIRECTED BY THE RESIDENT ENGINEER.

LEGEND

	CONTRACTOR'S STAGING AND STORAGE AREA
	EXISTING BUILDING
	NEW IMPROVEMENTS
	EXISTING FENCE
	AIRPORT PROPERTY LINE
	TAXIWAY OBJECT FREE AREA (TOFA)
	RUNWAY OBSTACLE FREE ZONE (ROFZ)
	AIRCRAFT MOVEMENT AREA
	CONTRACTOR'S ACCESS/HAUL ROAD
	TEMPORARY CLOSED TAXIWAY MARKER
	TEMPORARY CLOSED RUNWAY MARKER
	LOW PROFILE BARRICADES
	CROSSING GUARD STOP-A.O.A. SIGN (SEE NOTE 1) OR CONTRACTOR'S ESCORT WITH RADIO



IL. CONTRACT: **D1035**
IL. LETTING ITEM: **02A**
IL. PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

REVISIONS

NUMBER	BY	DATE

0 1 2
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AT FULL SCALE (34X22).

DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

CONSTRUCTION SAFETY AND PHASING PLAN - 1

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APPROVED BY: DKP
DATE: 07/25/2025
JOB No: 22004684-00

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SHEET 4 OF 22 SHEETS

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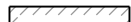
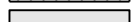
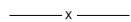
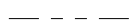







WORK AREA	ALLOWABLE WORK PERIOD	OPERATION STATUS/RESTRICTIONS
PHASE 2 	PHASE 2 SHALL BE RESTRICTED TO THREE (3) 4-HOUR WORK PERIODS. 1. MILL AND TACK PAVEMENT 2. PAVE 2" OVERLAY 3. PAVEMENT MARKING	RUNWAY 12/30 CLOSED RUNWAY 8/26 CLOSED ALL TAXIWAYS CLOSED

PHASE 2 - SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE CLOSURE OF RUNWAYS AND TAXIWAYS WITH THE RESIDENT ENGINEER AND PLACE CLOSED RUNWAY MARKERS.
- MARK AIR OPERATIONS AREA (A.O.A.) WITH LATHE AND RIBBON AND PLACE BARRICADES FOR TAXIWAY CLOSURES.
- CONSTRUCT PROPOSED IMPROVEMENTS INCLUDING RUNWAY PAVEMENT REHABILITATION, SHOULDER ADJUSTMENT, SEEDING AND MULCHING. CONSTRUCT FIRST COAT OF PAVEMENT MARKING WITHOUT BEADS AND NO BLACK BORDER ON RUNWAY AND CONNECTING TAXIWAY.
- OPEN PAVEMENT TO AIRCRAFT TRAFFIC.
- 30 DAY CURE PERIOD THEN CONSTRUCT SECOND COAT OF PAVEMENT MARKING WITH BEADS AND BLACK BORDER ON RUNWAY AND CONNECTING TAXIWAY.

SEE CONSTRUCTION SAFETY AND PHASING PLAN - 1 FOR NOTES

LEGEND

-  CONTRACTOR'S STAGING AND STORAGE AREA
-  EXISTING BUILDING
-  NEW IMPROVEMENTS
-  EXISTING FENCE
-  AIRPORT PROPERTY LINE
-  TAXIWAY OBJECT FREE AREA (TOFA)
-  RUNWAY OBSTACLE FREE ZONE (ROFZ)
-  AIRCRAFT MOVEMENT AREA
-  CONTRACTOR'S ACCESS/HAUL ROAD
-  TEMPORARY CLOSED RUNWAY MARKER
-  LOW PROFILE BARRICADES
-  CROSSING GUARD STOP-A.O.A. SIGN (SEE NOTE 1) OR CONTRACTOR'S ESCORT WITH RADIO



IL. CONTRACT: **D1035**
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DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

CONSTRUCTION SAFETY AND PHASING PLAN - 2

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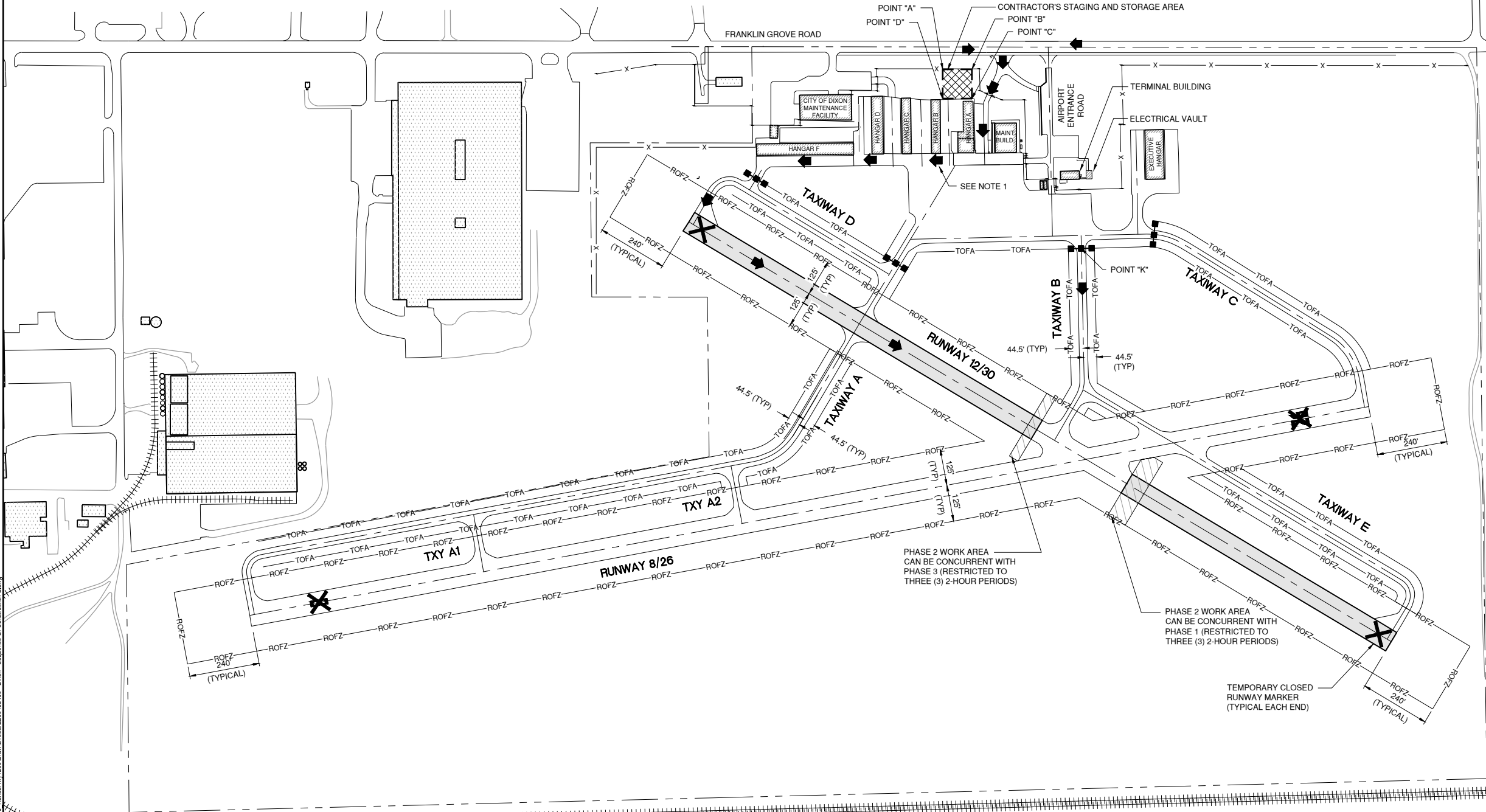


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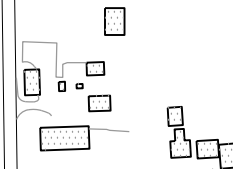
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SHEET 5 OF 22 SHEETS



SEE CONSTRUCTION SAFETY AND
PHASING PLAN - 1 FOR NOTES

- COORDINATE CLOSURE OF RUNWAY 12/30 WITH THE RESIDENT ENGINEER AND PLACE CLOSED RUNWAY MARKERS.
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DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

CONSTRUCTION SAFETY AND PHASING PLAN - 3

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GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2 (LATEST EDITION). NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING SAFETY REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL SUBMIT THE SPCD FOR APPROVAL.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE RESIDENT ENGINEER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPlicing NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- ALL EXISTING AND PROPOSED FENCE LINES, EXCEPT AS OTHERWISE NOTED, SHALL BE MAINTAINED AND SHALL SERVE AS CONSTRUCTION AROUND THE PERIMETER OF THE PROJECT. ALL EXISTING GATES SHALL BE MAINTAINED , CLOSED AND LOCKED AS DIRECTED BY THE AIRPORT OWNER'S REPRESENTATIVE. SHOULD THE CONTRACTOR CHOOSE TO KEEP A GATE OPEN FOR CONSTRUCTION OPERATIONS, A COMPETENT SECURITY GUARD SHALL MONITOR THE OPEN GATE. ANY COST SHALL NOT BE PAID FOR SEPARATELY, BUT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ONSITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE AIRPORT. PAYMENT FOR DUST CONTROL SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING BUT NOT LIMITED TO: TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, FLAGGER, AIR OPERATIONS AREA (A.O.A) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT OR HIS/HER DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD CIRCUITS IMPACTED BY CONSTRUCTION ACTIVITY.

2. PHASING

- TOTAL CONTRACT TIME SHALL BE 29 CALENDAR DAYS.
- PHASING SHALL BE AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.
- SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE EXISTING GATE SHOWN. THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLES ACCESS IF APPLICABLE AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.
- SUPERVISORY PERSONNEL SHALL DEMONSTRATE IN THE PRESENCE OF THE AIRPORT MANAGER THAT THEY ARE FAMILIAR WITH AIRPORT RADIO AND AIRPORT DRIVING PROCEDURES IN ORDER TO PERFORM WORK. OTHER CONSTRUCTION PERSONNEL CAN BE WITHIN THE AIRFIELD LIMITS PROVIDED THAT THEY ARE UNDER ESCORT AND IN THE PRESENCE OF AN AUTHORIZED SUPERVISOR. KNOWLEDGE OF THE AIRPORTS PROCEDURES BY THE SUPERVISORY PERSONNEL MUST BE DEMONSTRATED PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR'S STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.
- THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFER SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT MONITORING AIRPORT TRAFFIC ON THE RADIO. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN AND AIRPORT RADIO.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS, STORAGE AREAS AND/OR STAGING AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
- CONTACTS FOR THIS PROJECT WILL BE DETERMINED AT THE PRECONSTRUCTION MEETING PRIOR TO THE PROJECT START.

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX C OF FAA AC 150/5370-2 MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. SEE SECTION 70-16 OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-16 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.
- SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

12. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- RUNWAY OR TAXIWAY CLOSURES (IF REQUIRED) ARE AS DETAILED IN THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2 (LATEST EDITION).

15. MARKING AND SIGNS FOR ACCESS ROUTES

- MARKING AND SIGNAGE FOR THE ACCESS ROUTE SHALL BE AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER.

16. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2 AND 150/5210-5 AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION SAFETY AND PHASNG PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL PLACE ALL BARRICADES AND CONSTRUCTION SETBACK LINES ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, REPOSITION, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
- ACCESS TO ACTIVE RUNWAY AND TAXIWAY PAVEMENTS SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON IDOT TYPE II BARRICADES (2 EACH, RIGHT AND LEFT). IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "UNAUTHORIZED ACCESS NOT ALLOWED".

17. PROTECTION

- CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS, RUNWAY OBSTACLE FREE ZONE (ROFZ) AND THE RUNWAY SAFETY AREA (RSA) OF ACTIVE RUNWAYS.
- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS.
- THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.
- THE CONTRACTOR SHALL SUPPLY AND HAVE IN THEIR POSSESSION AT ALL TIMES AT LEAST ONE AIRPORT RADIO. IN THE EVENT THAT THE AIRPORT MANAGER NEEDS TO CONTACT THE CONTRACTOR DIRECTLY, THE OPERATOR OF SAID RADIO SHALL BE FAMILIAR WITH AIRPORT RADIO PROCEDURES AND TUNED INTO THE GROUND CONTROL FREQUENCY.

- BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

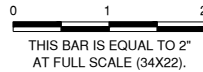
AIRFIELD LIGHTS AND SIGNS NOTES

- CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS INCLUDING LIGHTS/SIGNS ON OTHER SIDE OF BARRICADE LINES WITHIN THE AIRCRAFT MOVEMENT AREA UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
- CONTRACTOR SHALL TURN OFF RUNWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES, CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY, THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.

IL CONTRACT: **D1035**
IL LETTING ITEM: **02A**
IL PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

REVISIONS

NUMBER	BY	DATE



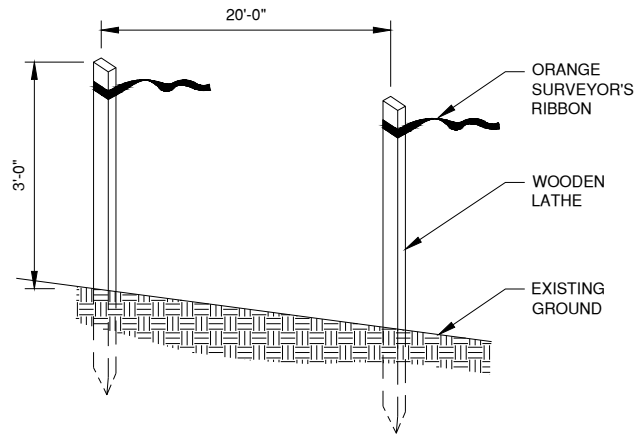
DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

CONSTRUCTION SAEETY AND PHASING PLAN
GENERAL NOTES AND DETAILS - 1

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APPROVED BY:	DKP
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JOB No:	22004684-00

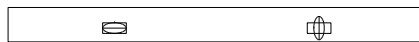
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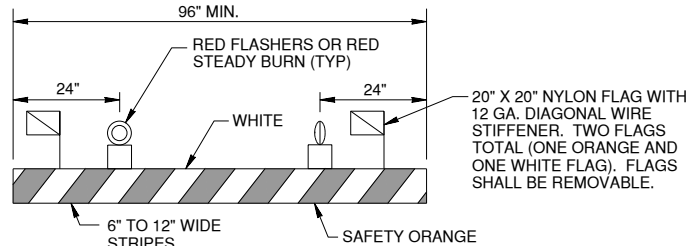
CONSTRUCTION SETBACK LINE DETAIL

NOT TO SCALE

1. PLACE CONSTRUCTION SETBACK LINES AT LOCATIONS OF CONSTRUCTION OPERATIONS NEAR RUNWAY OBSTACLE FREE ZONES AND TAXIWAY OBJECT FREE AREAS WHEN RUNWAYS/TAXIWAYS ARE OPEN.



PLAN VIEW



SIDE VIEW

AIRSIDE LOW PROFILE LIGHTED BARRICADE
NOT TO SCALE

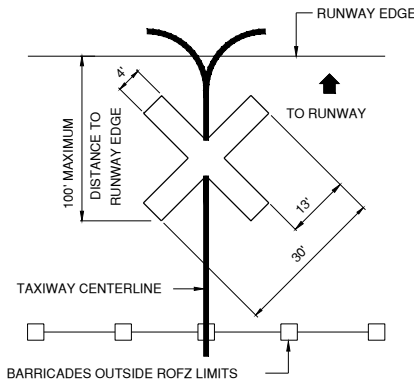
1. MARKERS SHALL BE SOLID YELLOW.
2. MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
4. MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN. HOWEVER, WHEN WORK IS NECESSARY FOR THAT AREA OR PAVEMENT IS OPEN TO TAXIING AIRCRAFT, THE MARKERS SHALL BE RELOCATED OFF THE PAVEMENT AS SHOWN ON THE DETAIL.
5. COST OF FURNISHING, INSTALLING, MAINTAINING, RELOCATING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT, WHERE APPLICABLE, PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE CONSTRUCTION ACTIVITY PLANS.

1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS, RUNWAY OBSTACLE FREE ZONES AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

1. ALL PAVEMENTS OR TURF AREAS UTILIZED BY THE CONTRACTOR FOR AN ACCESS ROUTE, STAGING, OR STORAGE SHALL BE REPAIRED AND RESTORED TO THEIR ORIGINAL CONDITION TO THE SATISFACTION OF THE AIRPORT. NO ADDITIONAL COMPENSATION TO PROTECT, REPAIR, OR RESTORE THESE AREAS SHALL BE MADE.

CONSTRUCTION POINT TABLE					
POINT	NEAREST ACTIVE RUNWAY	LATITUDE	LONGITUDE	GROUND ELEVATION	TOP ELEVATION
A	RUNWAY 8/26	41°50'15.12"	89°26'44.58"	778.0	803.0
B	RUNWAY 8/26	41°50'15.11"	89°26'43.26"	778.0	803.0
C	RUNWAY 8/26	41°50'14.12"	89°26'43.27"	778.0	803.0
D	RUNWAY 8/26	41°50'14.13"	89°26'44.59"	778.0	803.0
E	RUNWAY 8/26	41°50'01.84"	89°26'53.74"	778.0	803.0
F	RUNWAY 8/26	41°50'02.80"	89°26'41.51"	780.0	805.0
G	RUNWAY 8/26	41°50'00.96"	89°26'35.05"	781.0	806.0
H	RUNWAY 8/26	41°50'01.44"	89°26'32.16"	782.0	807.0
I	RUNWAY 8/26	41°50'03.38"	89°26'36.44"	782.0	807.0
J	RUNWAY 8/26	41°50'03.60"	89°26'38.83"	781.0	806.0
K	RUNWAY 8/26	41°50'09.02"	89°26'38.38"	782.0	807.0

NOTE - ALL PHASES
ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.



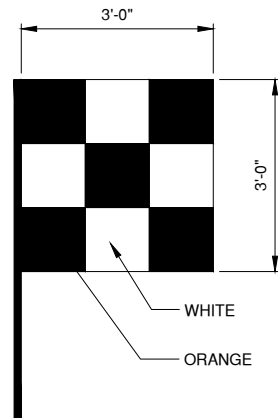
CLOSED TAXIWAY MARKER DETAIL

NOT TO SCALE

RUNWAY 12/30 OBSTACLE FREE ZONE TOTAL WIDTH = 250'
 RUNWAY 8/26 OBSTACLE FREE ZONE TOTAL WIDTH= 250'
 TAXIWAY CENTERLINE TO OBJECT SEPARATION = 44.5'
 TAXILANE CENTERLINE TO OBJECT SEPARATION = 39.5'

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS.

- REPLACE AUTOMATED WEATHER OBSERVATION SYSTEM.

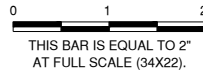


CONSTRUCTION EQUIPMENT AND TRUCK/VEHICLE SIGNAL FLAG

NOT TO SCALE

IL. CONTRACT: **DI035**
IL. LETTING ITEM: **02A**
IL. PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

NUMBER	BY	DATE



DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

CONSTRUCTION SAFETY AND PHASING PLAN GENERAL NOTES AND DETAILS - 2

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APPROVED BY:	DKP
DATE:	07/25/2025
JOB No:	22004684-00

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DATE: Thursday, July 17, 2025 8:59:01 AM
FILE: K:\Drawings\22004684-00_Renovation\230\Draw\Sheets\22004684-00_Dixon - Storm Plan.dwg

LEGEND

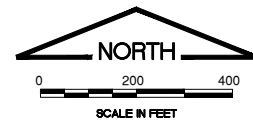
- NEW SHOULDER ADJUSTMENT, SEEDING AND HEAVY-DUTY HYDRAULIC MULCH
- CONTRACTOR'S STAGING AND STORAGE AREA
- NEW BITUMINOUS PAVEMENT OVERLAY
- AIRPORT PROPERTY LINE
- INLET PROTECTION
- ST EXISTING STORM SEWER

STORM WATER POLLUTION PREVENTION GENERAL NOTES

- THE LIMITS OF SHOULDER ADJUSTMENT, SEEDING AND MULCHING SHALL NOT EXCEED 5' FROM THE EDGE OF PAVEMENT.
- THE CONTRACTOR SHALL NOT PERMIT HAULING OUTSIDE OF THE SHOULDER ADJUSTMENT LIMITS WITHOUT THE PRIOR APPROVAL OF THE RESIDENT ENGINEER AND THE EXECUTIVE DIRECTOR.
- THE CONTRACTOR SHALL TAKE PROVISIONS TO PREVENT EROSION AND STORM WATER POLLUTION WITHIN THE PROJECT LIMITS AND AT THE STAGING AREAS, SUCH AS STABILIZED CONSTRUCTION ENTRANCES, SILT FENCE, INLET PROTECTIONS, ETC. COSTS FOR INSTALLATION, MAINTENANCE AND REMOVAL OF EROSION CONTROL DEVICES WITHIN THE PROJECT LIMITS AND AT THE STAGING AREAS SHALL BE BORNE BY THE CONTRACTOR.
- AT THE COMPLETION OF CONSTRUCTION, THE STAGING AREAS SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITION. RUTTED AREAS SHALL BE FILLED AND THE AREA SEEDED AND MULCHED AS NEEDED. RESTORATION COSTS SHALL BE BORNE BY THE CONTRACTOR.

PROJECT SITE AREA AND STAGING AREA PROTECTION NOTES

- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE STORM WATER POLLUTION PREVENTION DEVICES WITHIN THE PROJECT LIMITS AND AT THE STAGING AREA. THESE DEVICES, INCLUDING INLET PROTECTIONS, SILT FENCE, BALES, DITCH CHECKS, STABILIZED CONSTRUCTION ENTRANCES, ETC., SHALL NOT BE PAID FOR, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- AT THE COMPLETION OF WORK, THE STAGING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS, AT NO ADDITIONAL COST TO THE CONTRACT. THIS WORK SHALL INCLUDE REMOVING ANY AND ALL STORM WATER POLLUTION PREVENTION DEVICES, RESTORING RUTTED AREAS, SEEDING AND MULCHING.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE PROJECT SITE AREA AND STAGING AREA(S) IN A MANNER TO PREVENT POLLUTION AND SILTATION OF THE EXISTING STORM SEWER SYSTEM.



IL CONTRACT: **D1035**
IL LETTING ITEM: **02A**
IL PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

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DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

STORMWATER POLLUTION PREVENTION PLAN

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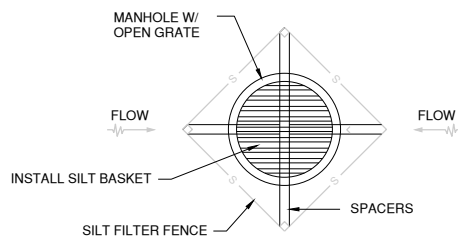


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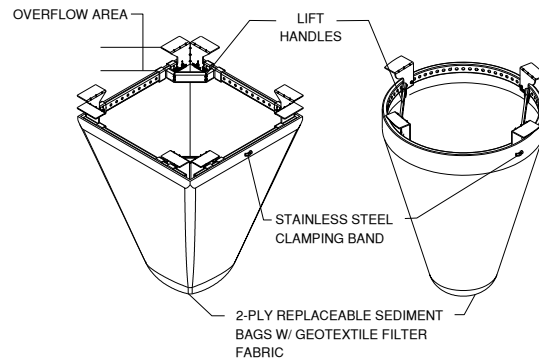
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SHEET 9 OF 22 SHEETS



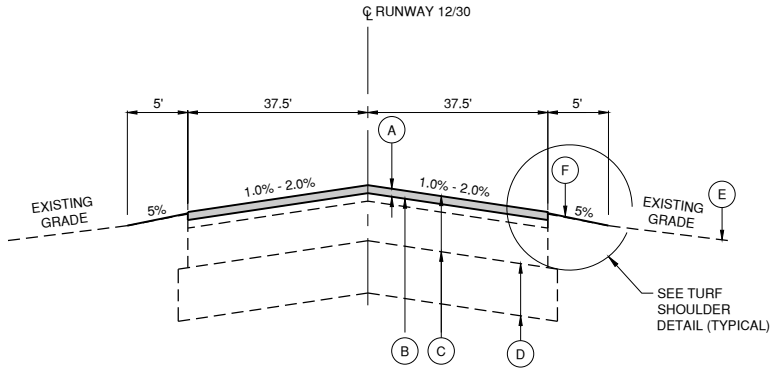
INLET PROTECTION (INLET/MANHOLE - IN TURF)

NOT TO SCALE
IDOT STANDARD 280001-07



INLET PROTECTION - SILT BASKET (PAVEMENT AND TURF)

NOT TO SCALE
FOR ALL RECTANGULAR AND CIRCULAR INLETS



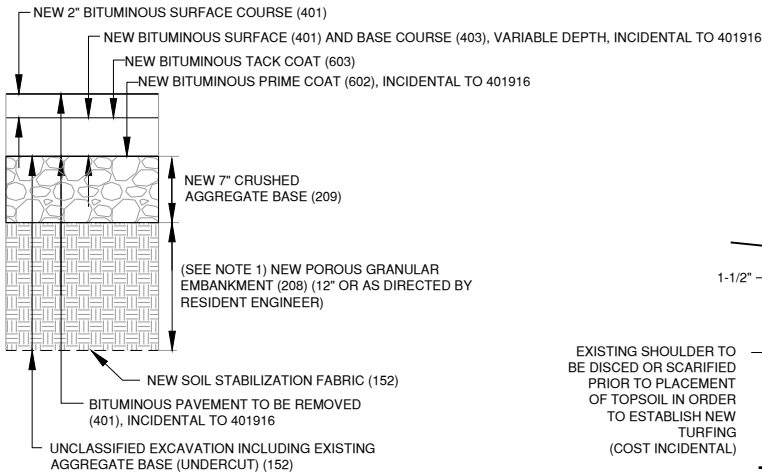
RUNWAY 12/30 TYPICAL SECTION A-A

NOT TO SCALE

LEGEND

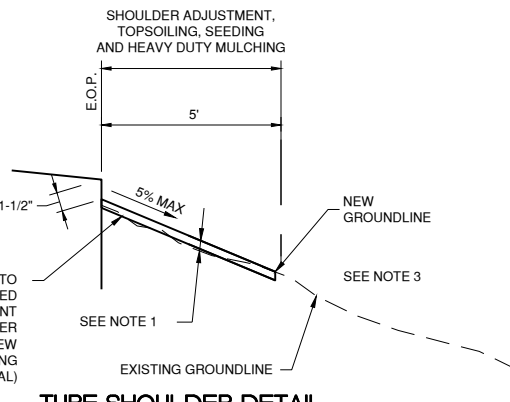
- (A) NEW 2" AND VARIABLE DEPTH BITUMINOUS SURFACE MILLING (401)
NEW 2" BITUMINOUS SURFACE COURSE (401)
- (B) NEW BITUMINOUS TACK COAT (603)
- (C) EXISTING BITUMINOUS PAVEMENT (DEPTH VARIES)
- (D) EXISTING 7" CRUSHED AGGREGATE BASE COURSE
- (E) EXISTING GROUNDLINE
- (F) SHOULDER ADJUSTMENT (152), SEEDING (901) AND
HEAVY-DUTY HYDRAULIC MULCHING (908)

HMA MIXTURE REQUIREMENTS TABLE					
ITEM	RUNWAY/TAXIWAY (<60K LBS) N DES	PG BINDER	AGGREGATE QUALITY	MAX RAP %	DENSITY ACCEPTANCE
401: HMA SURFACE	N30 @ 3.0%	SBS PG 70-28	A	0 (N/A)	PWL
403: HMA BASE (REM & REP PAV'T)	N30 @ 3.0%	SBS PG 70-28 (TOP LIFT) PG 64-22 (LOWER LIFTS)	B	20%	PATCHING: NUCLEAR DENSITY



STABILIZATION TYPICAL SECTION

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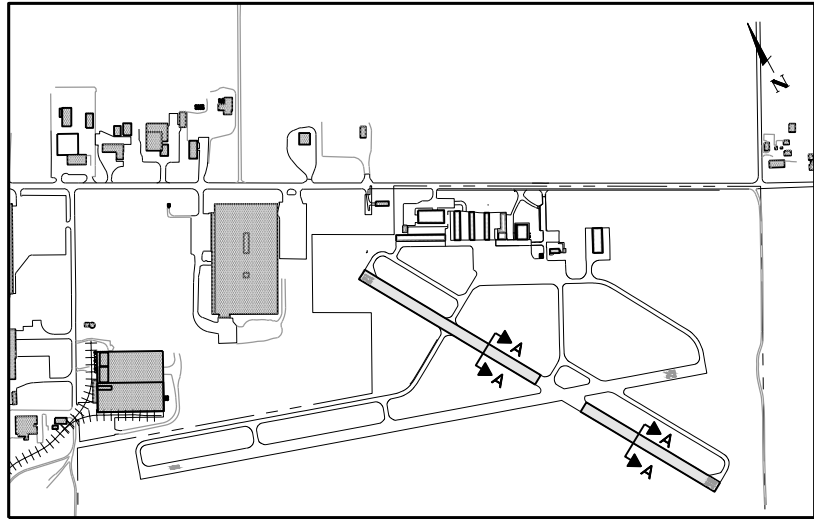


TURF SHOULDER DETAIL

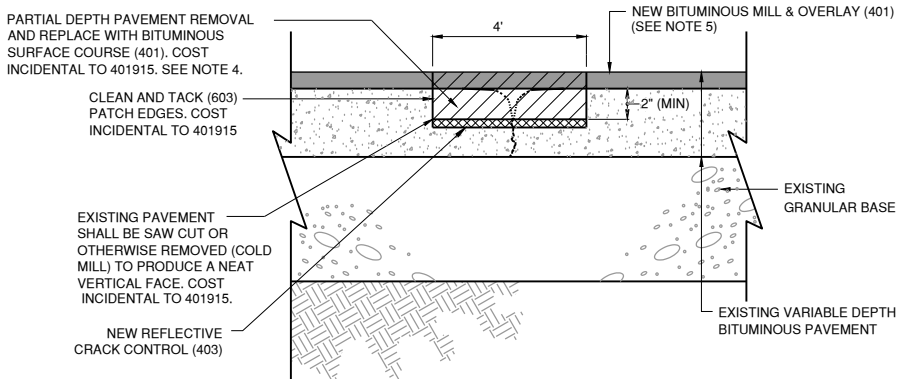
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NOTES

- LOCATIONS FOR FURNISHING AND PLACING POROUS GRANULAR EMBANKMENT SHALL BE AS THE FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION. THIS MATERIAL IS INTENDED TO REPAIR SOFT SUBGRADE AS DIRECTED BY THE RESIDENT ENGINEER. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES. EXCAVATION OF THE SOFT SUBGRADE SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR UNCLASSIFIED EXCAVATION (REFER TO STABILIZATION TYPICAL SECTION).
- STABILIZATION TYPICAL SECTION APPLIES TO REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE B IF REQUIRED.
- TOPSOIL REQUIRED FOR FILL SHALL BE INCLUDED IN THE COSTS FOR SHOULDER ADJUSTMENT (AR152480).
- LIMITS OF TURF SHOULDER SHALL BE ADJUSTED IN THE FIELD AS REQUIRED AT NO ADDITIONAL COST TO THE CONTRACT.
- THE DEFAULT WIDTH OF SHOULDER ADJUSTMENT, TOPSOILING, SEEDING AND HEAVY DUTY MULCHING SHALL BE 5'. IF THE EXISTING ELEVATION CANNOT BE MET AFTER 5' AT A 5% MAXIMUM SLOPE, THESE ITEMS MAY EXTEND OUT AN ADDITIONAL 5' MAXIMUM AT A 5% MAXIMUM SLOPE.
- SHOULDER DROP-OFF SHOULD BE 1.5-INCH WITH A PLUS/MINUS TOLERANCE OF 1/2 INCH FROM THE EDGE OF THE PAVEMENT TO THE ADJACENT UNPAVED AREAS.

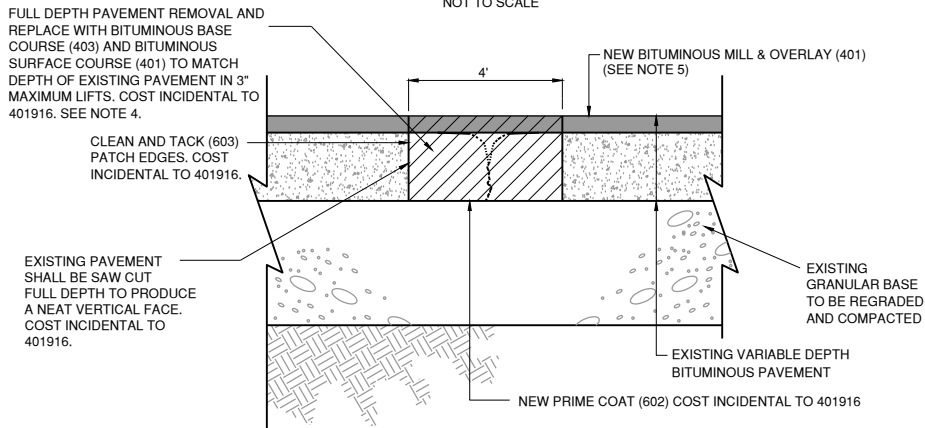


KEY MAP



REMOVE AND REPLACE
BITUMINOUS PAVEMENT TYPE A

NOT TO SCALE



REMOVE AND REPLACE
BITUMINOUS PAVEMENT - TYPE B

NOT TO SCALE

PAVEMENT REMOVAL AND REPLACEMENT NOTES

- DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS, AIRPORT PERSONNEL, AND PAVEMENT CORES. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
- PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. EXISTING MEDIUM TO HIGH SEVERITY TRANSVERSE CRACKS ARE SPACED AT VARIABLE INTERVALS. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.
- THIS WORK SHALL BE DETERMINED IN THE FIELD WITH THE AIRPORT/ENGINEER.
- CONTRACTOR SHALL CONSTRUCT THE TOP 4" OF THE FULL DEPTH PAVEMENT REMOVAL AND REPLACEMENT WITH BITUMINOUS SURFACE COURSE (401).
- THE EXISTING SURFACES ARE CONSTRUCTED TO EXISTING SURFACE GRADES AFTER THE COMPLETION OF OPERATIONS FOR REMOVE AND REPLACE BITUMINOUS TYPE A AND TYPE B.
- CONTRACTOR SHALL INSTALL TACK COAT BETWEEN PAVEMENT LIFTS. COST INCIDENTAL TO REMOVE AND REPLACE BITUMINOUS TYPE A AND TYPE B.

IL CONTRACT: **D1035**
IL LETTING ITEM: **02A**
IL PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

REVISIONS

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DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

TYPICAL SECTIONS AND
PAVEMENT REHABILITATION DETAILS

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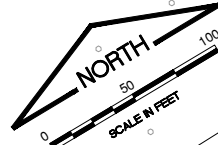
FINAL

LEGEND	
	BITUMINOUS PAVEMENT MILLING - 2" AND VARIABLE DEPTH
	PROPOSED PAVEMENT - PATCH A BITUMINOUS PAVEMENT MILLING - 2" AND VARIABLE DEPTH
	PROPOSED PAVEMENT - PATCH B BITUMINOUS PAVEMENT MILLING - 2" AND VARIABLE DEPTH
	AIRPORT PROPERTY LINE
	EXISTING CONDUIT/DUCT BANK
	EXISTING RUNWAY 12/30 CIRCUIT 1/C #8 5KV
	EXISTING RETROREFLECTIVE MARKER
	EXISTING BASE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT
	EXISTING STAKE MOUNTED MEDIUM INTENSITY RUNWAY/TAXIWAY/THRESHOLD LIGHT
	EXISTING HANDHOLE
	EXISTING RUNWAY END IDENTIFIER LIGHT (REIL)
	EXISTING AIRFIELD GUIDANCE SIGN
	EXISTING WIND CONE
	EXISTING WIND CONE CIRCUIT
	PRECISION APPROACH PATH INDICATOR (PAPI)
	EXISTING AUTOMATED WEATHER OBSERVING SYSTEM (AWOS)

NOTES

- PRIOR TO PAVING OPERATIONS, THE FINAL MILLED SURFACE SHALL BE BROOMED CLEAN AND THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL INSPECT THE FINAL SURFACE. ALL LOOSE PIECES SHALL BE REMOVED.
- THE EXISTING SURFACES ARE TO BE MILLED AFTER THE COMPLETION OF OPERATIONS FOR REMOVE AND REPLACE BITUMINOUS TYPE A AND TYPE B CONSTRUCTED TO EXISTING GRADE.
- THE DETERMINATION AND LIMITS OF REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A AND TYPE B SHALL BE COORDINATED WITH THE RESIDENT ENGINEER.
- EXISTING DUCT MARKERS SHALL BE SURVEYED BY THE CONTRACTOR BEFORE SURFACE IS MILLED. DUCT MARKERS SHALL BE REPLACED AT SAME LOCATIONS SURVEYED (COST INCIDENTAL TO THE CONTRACT).
- THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



MATCHLINE STA. 209+00

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DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

EXISTING CONDITIONS/PROPOSED REMOVALS - 1

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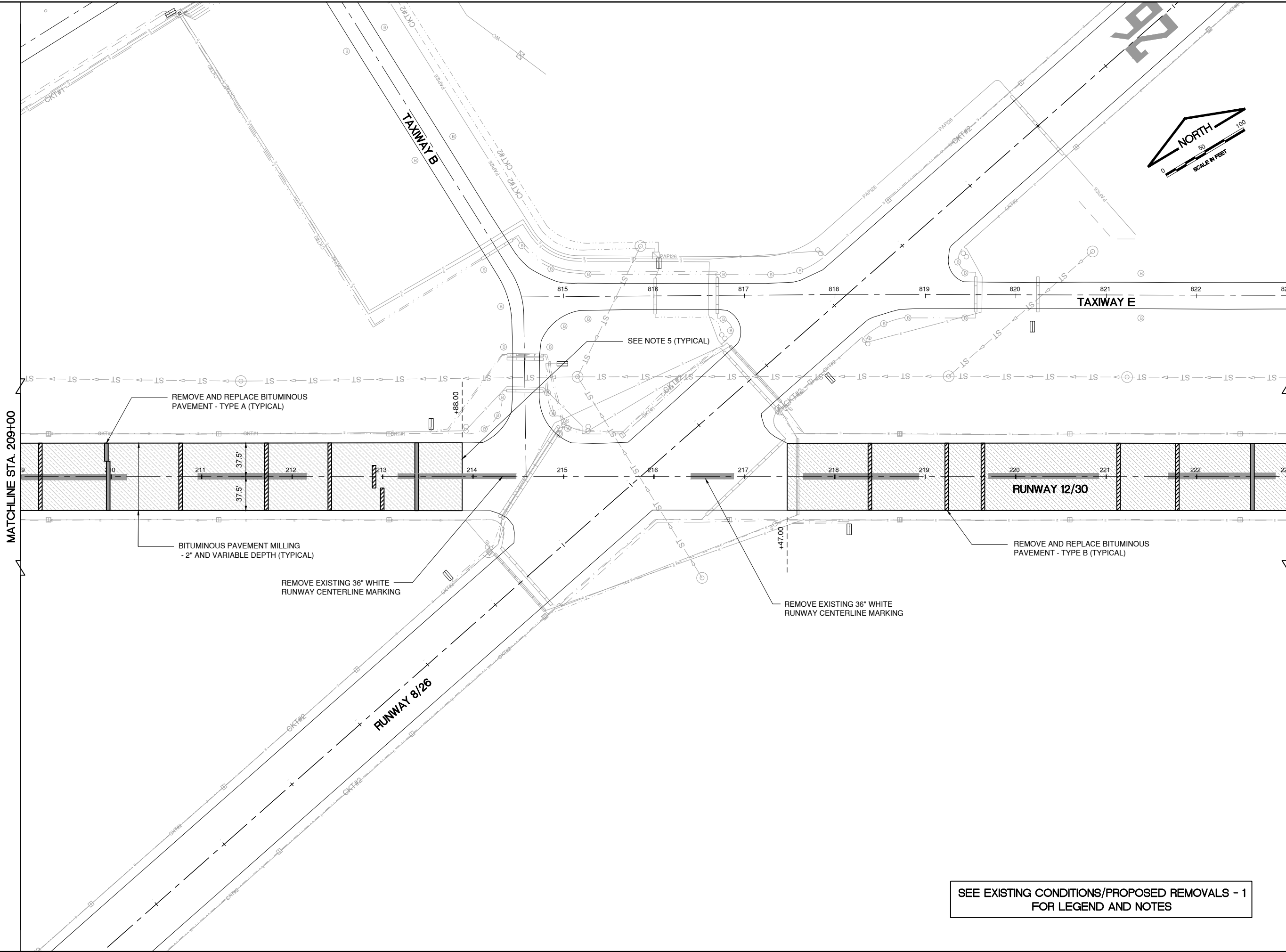


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SHEET 12 OF 22 SHEETS



SEE EXISTING CONDITIONS/PROPOSED REMOVALS - 1
FOR LEGEND AND NOTES

IL CONTRACT: **D1035**
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IL PROJECT: **C73-5064**
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DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

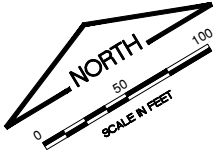
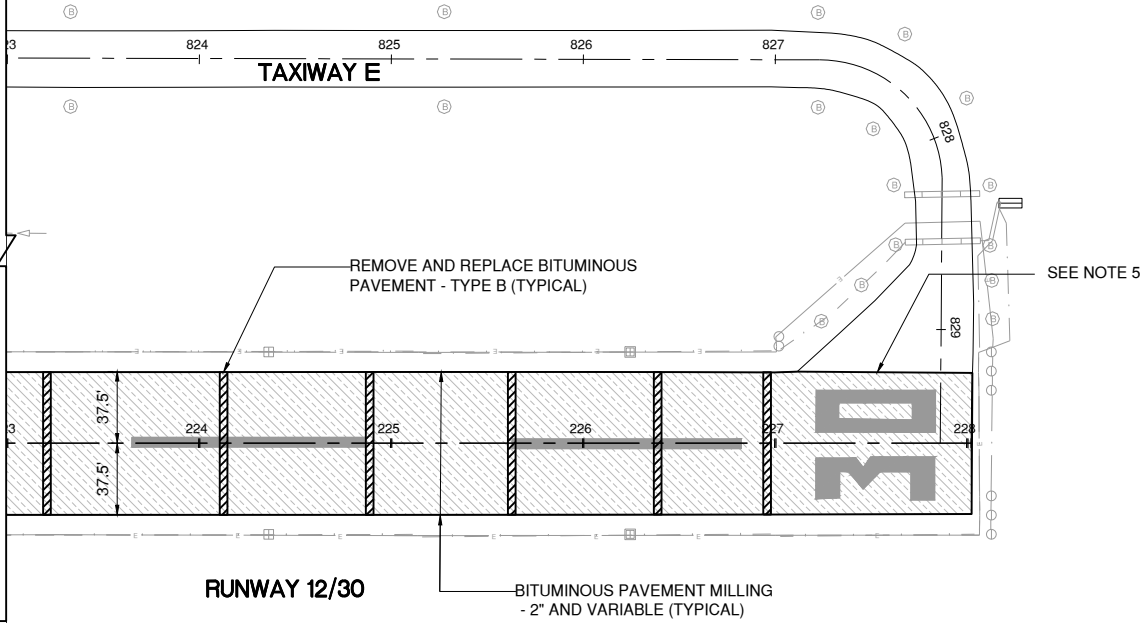
EXISTING CONDITIONS/PROPOSED REMOVALS - 2

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FOR LEGEND AND NOTES

IL. CONTRACT: **D1035**
IL. LETTING ITEM: **02A**
IL. PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

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DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

EXISTING CONDITIONS/PROPOSED REMOVALS - 3

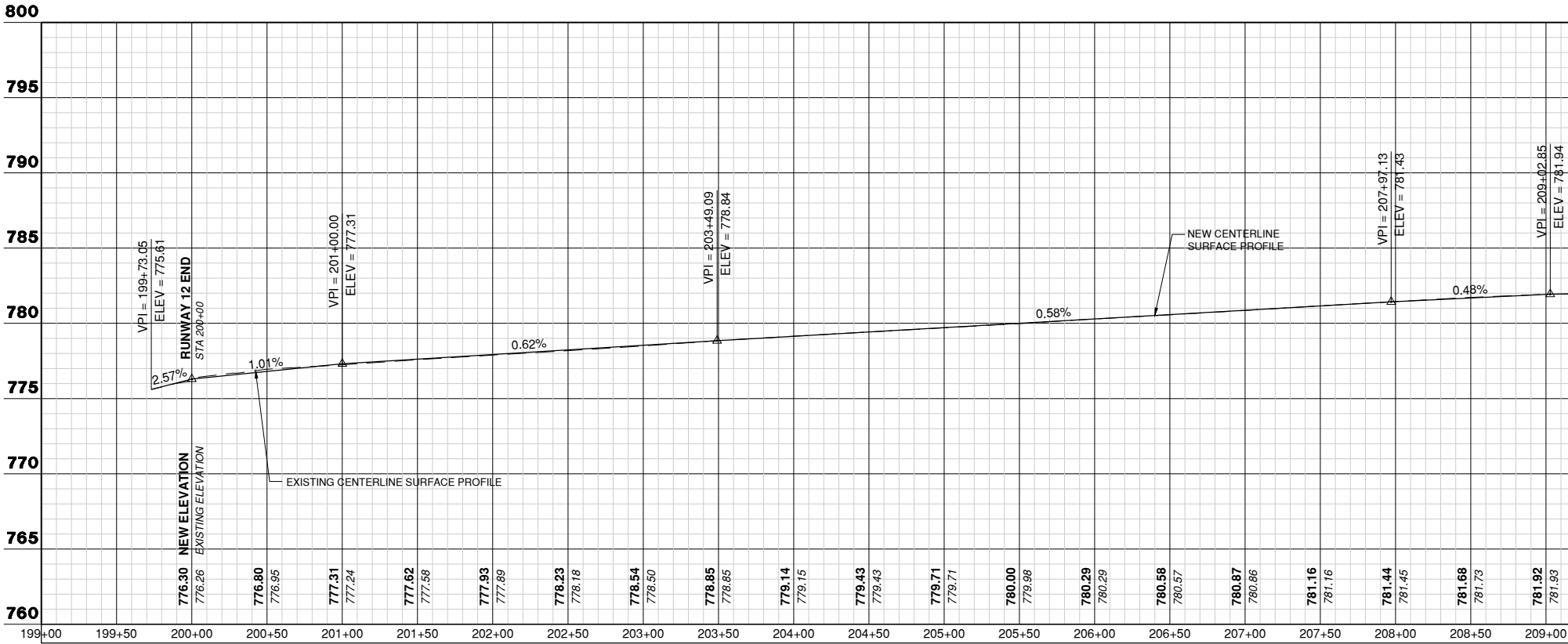
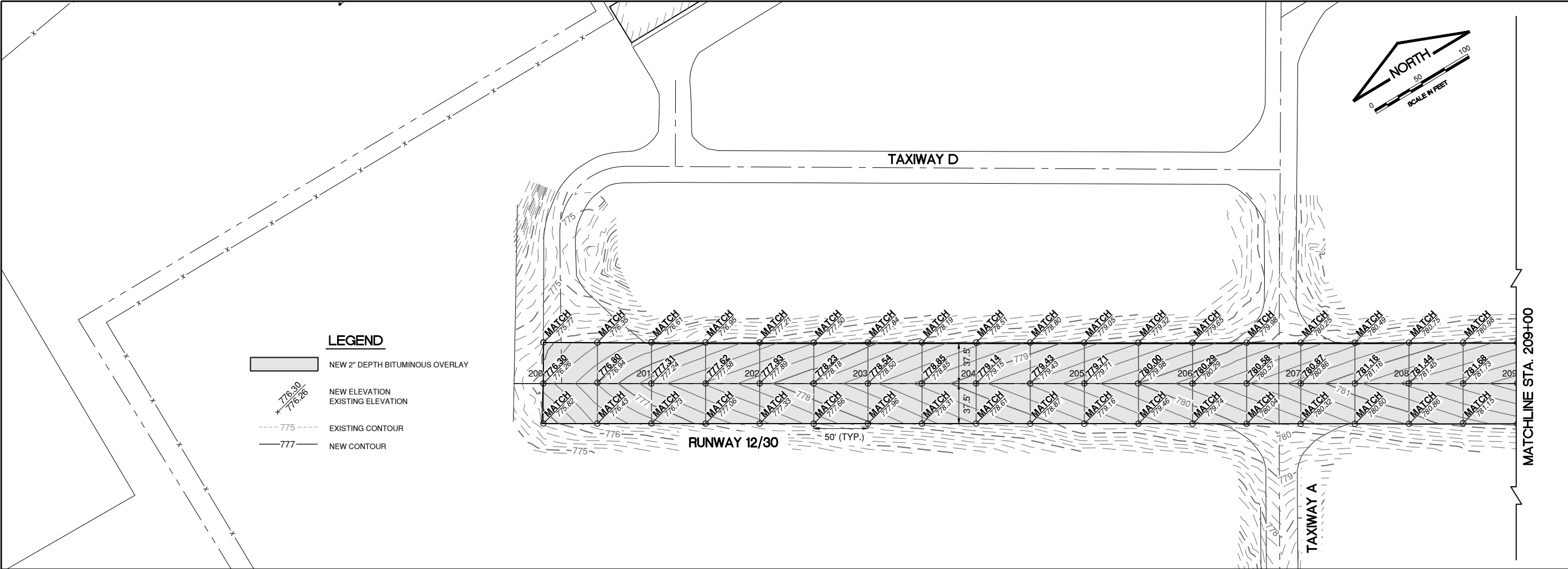
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Sheet: 15 of 22
Plan and Profile.dwg

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IL CONTRACT: **D1035**
IL LETTING ITEM: **02A**
IL PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

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DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

RUNWAY 12-30 PLAN AND PROFILE - 1

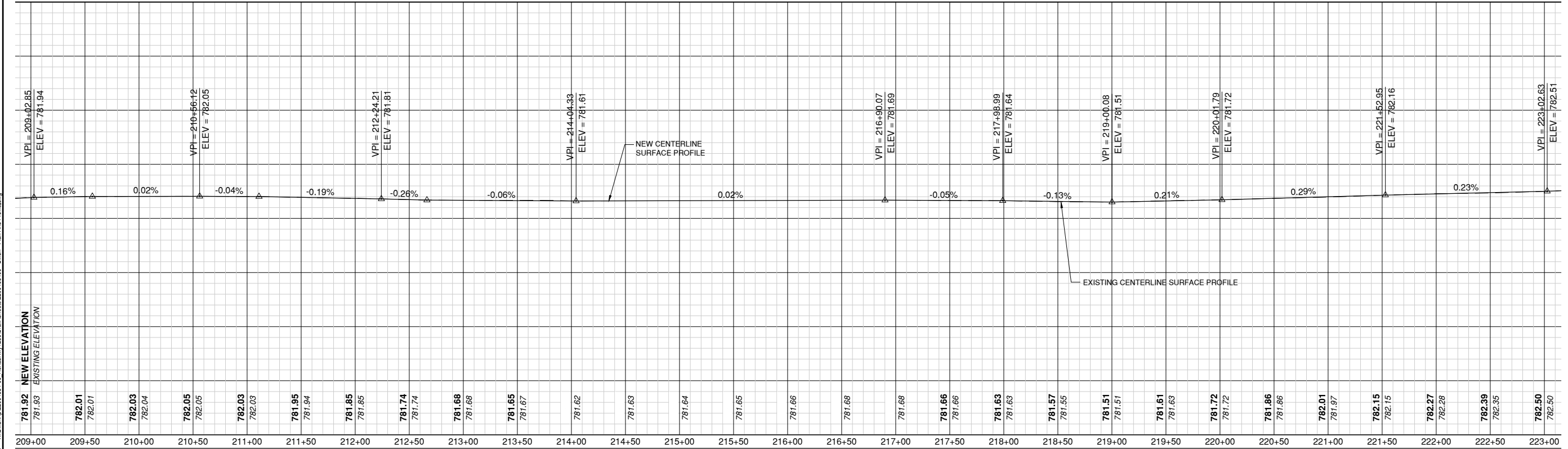
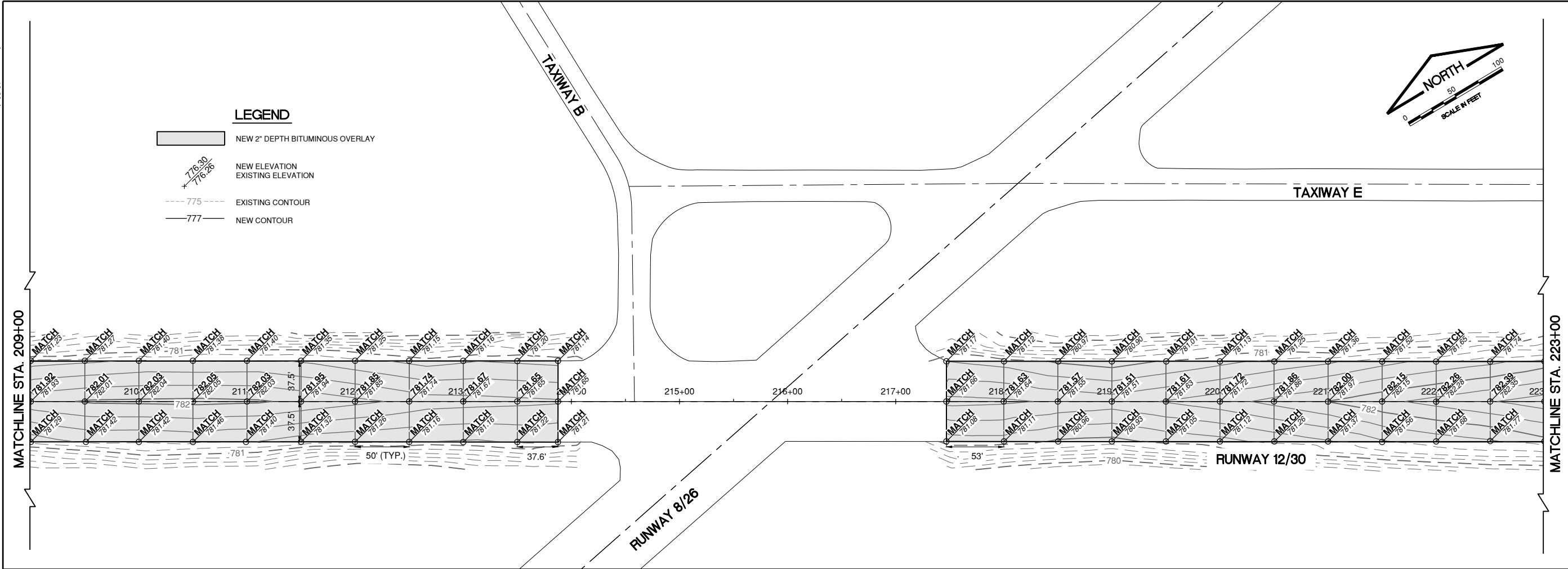
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Sheet: 16 of 22
Plan and Profile.dwg

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IL CONTRACT: **D1035**
IL LETTING ITEM: **02A**
IL PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

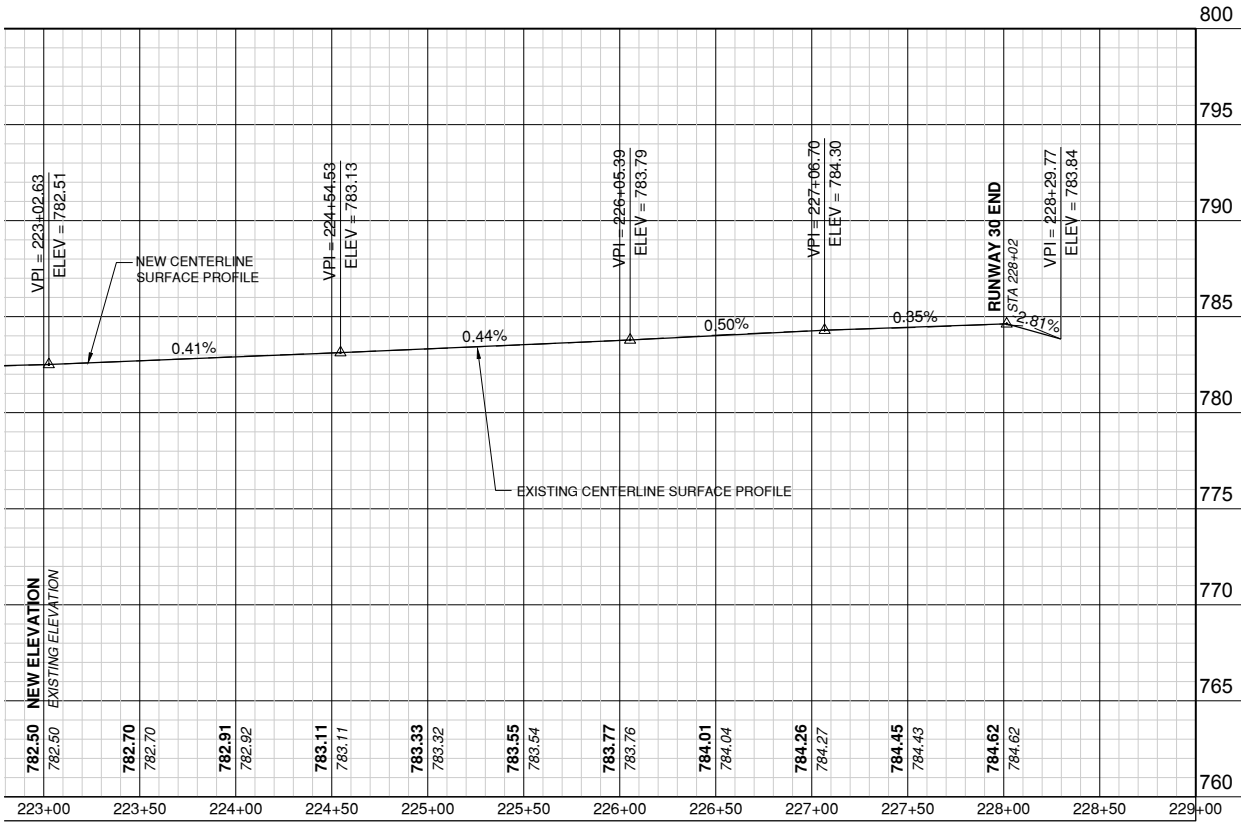
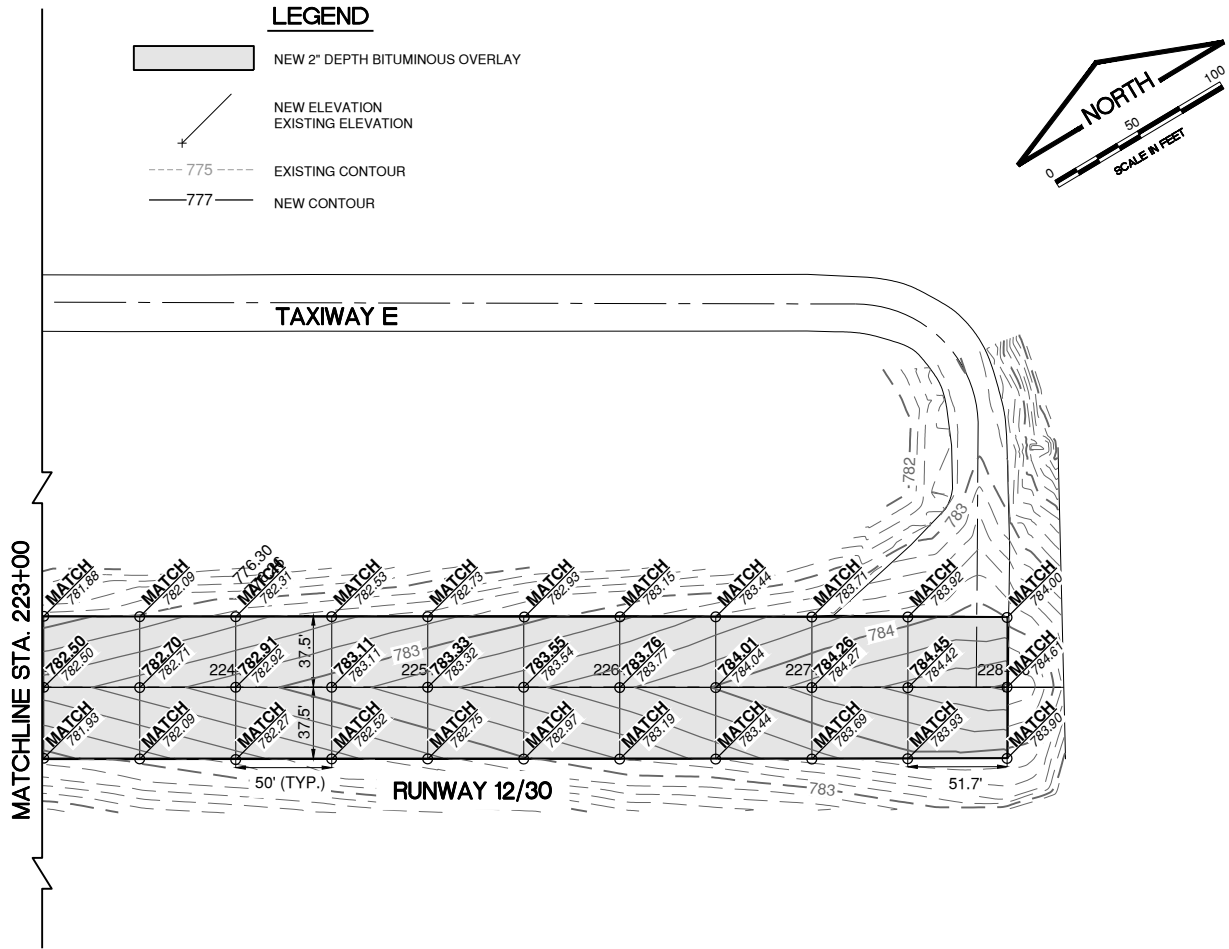
RUNWAY 12-30 PLAN AND PROFILE - 2

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Sheet: 17, 12-30.dwg
Plan and Profile.dwg

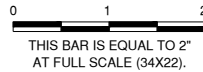


RUNWAY 12/30 GRADING TABLE									
STATION	37.5' LEFT OF CENTERLINE			CENTERLINE			37.5' RIGHT OF CENTERLINE		
	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH
200+00.00	775.77	775.77	0.17	776.30	776.26	0.13	775.82	775.82	0.17
200+50.00	776.35	776.35	0.17	776.80	776.95	0.32	776.43	776.43	0.17
201+00.00	776.61	776.61	0.17	777.31	777.24	0.10	776.73	776.73	0.17
201+50.00	776.95	776.95	0.17	777.62	777.58	0.13	777.06	777.06	0.17
202+00.00	777.21	777.21	0.17	777.93	777.89	0.13	777.33	777.33	0.17
202+50.00	777.50	777.50	0.17	778.23	778.18	0.12	777.66	777.66	0.17
203+00.00	777.84	777.84	0.17	778.54	778.50	0.13	777.96	777.96	0.17
203+50.00	778.19	778.19	0.17	778.85	778.85	0.17	778.31	778.31	0.17
204+00.00	778.51	778.51	0.17	779.14	779.15	0.18	778.61	778.61	0.17
204+50.00	778.80	778.80	0.17	779.43	779.43	0.17	778.87	778.87	0.17
205+00.00	779.05	779.05	0.17	779.71	779.71	0.17	779.16	779.16	0.17
205+50.00	779.32	779.32	0.17	780.00	779.98	0.15	779.46	779.46	0.17
206+00.00	779.65	779.65	0.17	780.29	780.29	0.17	779.74	779.74	0.17
206+50.00	779.98	779.98	0.17	780.58	780.57	0.16	780.04	780.04	0.17
207+00.00	780.25	780.25	0.17	780.87	780.86	0.16	780.32	780.32	0.17
207+50.00	780.49	780.49	0.17	781.16	781.16	0.17	780.60	780.60	0.17
208+00.00	780.75	780.75	0.17	781.44	781.45	0.18	780.86	780.86	0.17
208+50.00	780.98	780.98	0.17	781.68	781.73	0.22	781.15	781.15	0.17
209+00.00	781.23	781.23	0.17	781.92	781.93	0.18	781.29	781.29	0.17
209+50.00	781.27	781.27	0.17	782.01	782.01	0.17	781.42	781.42	0.17
210+00.00	781.40	781.40	0.17	782.03	782.04	0.18	781.42	781.42	0.17
210+50.00	781.38	781.38	0.17	782.05	782.05	0.17	781.46	781.46	0.17
211+00.00	781.40	781.40	0.17	782.03	782.03	0.17	781.40	781.40	0.17
211+50.00	781.35	781.35	0.17	781.95	781.94	0.16	781.32	781.32	0.17
212+00.00	781.25	781.25	0.17	781.85	781.85	0.17	781.26	781.26	0.17
212+50.00	781.15	781.15	0.17	781.74	781.74	0.17	781.16	781.16	0.17
213+00.00	781.16	781.16	0.17	781.67	781.67	0.17	781.16	781.16	0.17
213+50.00	781.20	781.20	0.17	781.65	781.65	0.17	781.22	781.22	0.17
213+87.60	781.14	781.14	0.17	781.65	781.65	0.17	781.21	781.21	0.17
217+48.00	781.17	781.17	0.17	781.66	781.66	0.17	781.08	781.08	0.17
218+00.00	781.12	781.12	0.17	781.63	781.64	0.18	781.11	781.11	0.17
218+50.00	780.97	780.97	0.17	781.57	781.55	0.15	780.96	780.96	0.17
219+00.00	780.90	780.90	0.17	781.51	781.51	0.17	780.93	780.93	0.17
219+50.00	781.01	781.01	0.17	781.61	781.63	0.19	781.05	781.05	0.17
220+00.00	781.13	781.13	0.17	781.72	781.72	0.17	781.12	781.12	0.17
220+50.00	781.25	781.25	0.17	781.86	781.86	0.17	781.26	781.26	0.17
221+00.00	781.36	781.36	0.17	782.00	781.97	0.14	781.37	781.37	0.17
221+50.00	781.52	781.52	0.17	782.15	782.15	0.17	781.56	781.56	0.17
222+00.00	781.65	781.65	0.17	782.26	782.28	0.19	781.68	781.68	0.17
222+50.00	781.74	781.74	0.17	782.39	782.35	0.13	781.77	781.77	0.17
223+00.00	781.88	781.88	0.17	782.50	782.50	0.17	781.93	781.93	0.17
223+50.00	782.09	782.09	0.17	782.70	782.71	0.18	782.09	782.09	0.17
224+00.00	782.31	782.31	0.17	782.91	782.92	0.18	782.27	782.27	0.17
224+50.00	782.53	782.53	0.17	783.11	783.11	0.17	782.52	782.52	0.17
225+00.00	782.73	782.73	0.17	783.32	783.32	0.17	782.75	782.75	0.17
225+50.00	782.93	782.93	0.17	783.55	783.54	0.16	782.97	782.97	0.17
226+00.00	783.15	783.15	0.17	783.76	783.77	0.18	783.19	783.19	0.17
226+50.00	783.44	783.44	0.17	784.01	784.04	0.20	783.44	783.44	0.17
227+00.00	783.71	783.71	0.17	784.26	784.27	0.18	783.69	783.69	0.17
227+50.00	783.92	783.92	0.17	784.45	784.42	0.14	783.93	783.93	0.17
228+01.70	784.00	784.00	0.17	784.61	784.61	0.17	783.90	783.90	0.17

IL. CONTRACT: **D1035**
IL. LETTING ITEM: **02A**
IL. PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

REVISIONS

NUMBER	BY	DATE



DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

RUNWAY 12-30 PLAN AND PROFILE - 3

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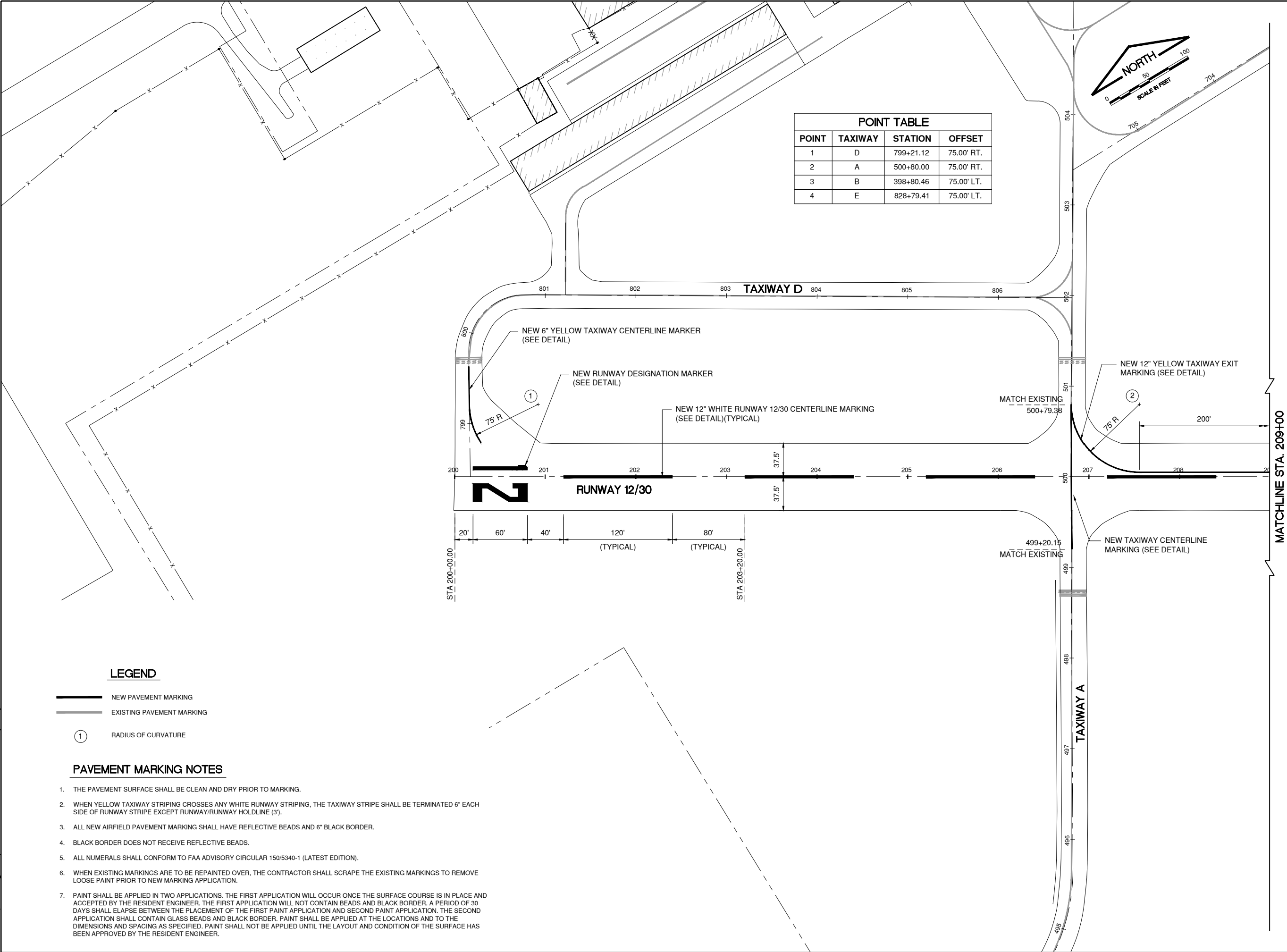
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CONSULTING ENGINEERS
License No. 184-000613

DESIGN BY: STL
DRAWN BY: JRO
CHECKED BY: STL
APPROVED BY: DKP
DATE: 07/25/2025
JOB No: 22004684-00

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6/10/2025 2:11 AM TUDOR
6/10/2025 2:11 AM TUDOR

DATE: Thursday, July 17, 2025 8:59:24 AM
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IL. CONTRACT: **D1035**
IL. LETTING ITEM: **02A**
IL. PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

PAVEMENT MARKING PLAN - 1

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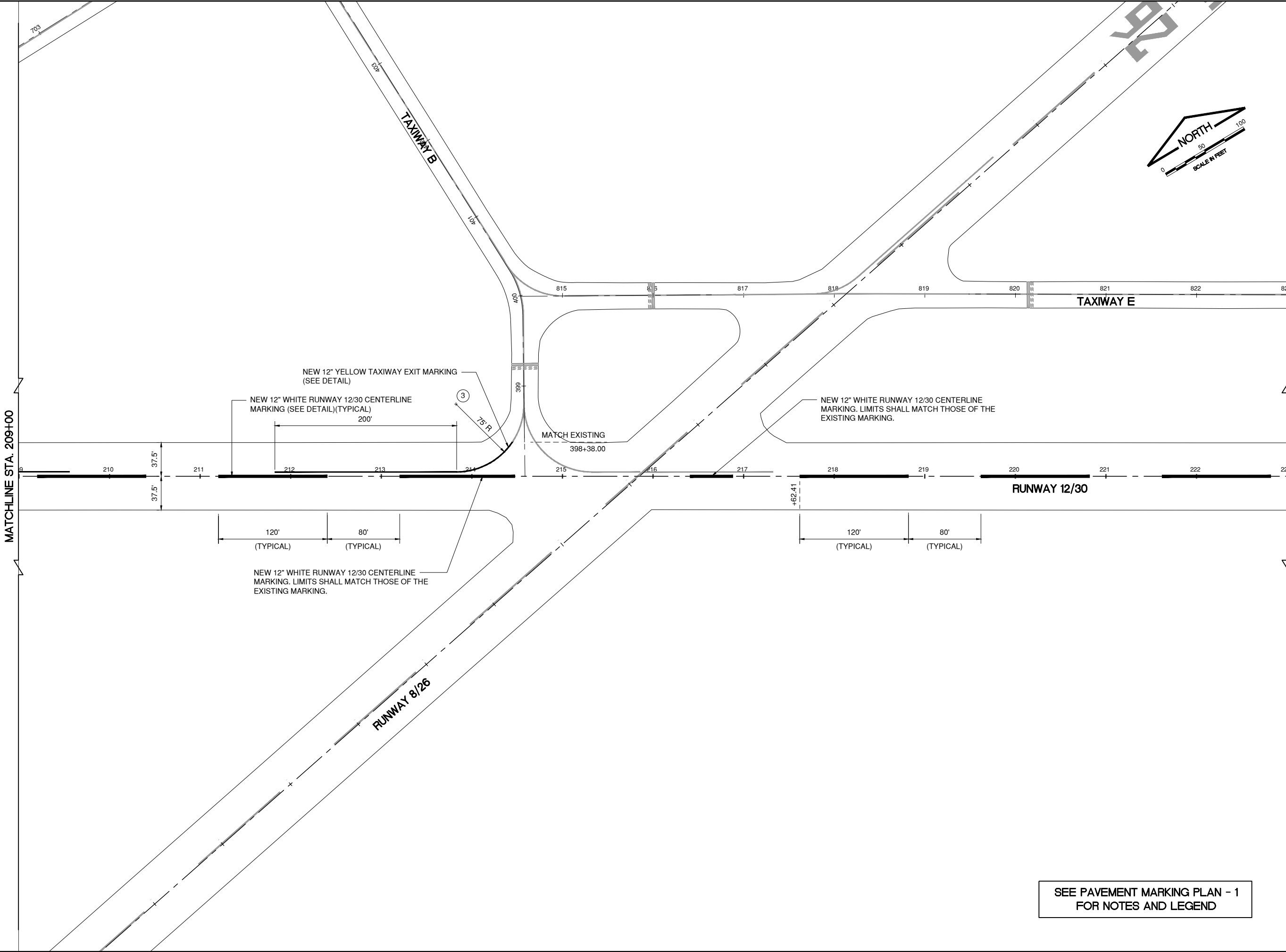
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APPROVED BY: DKP
DATE: 07/25/2025
JOB No: 22004684-00

FINAL

SHEET 18 OF 22 SHEETS

BM0005-21, AM, TUDOR
BM0005-21, AM, TUDOR

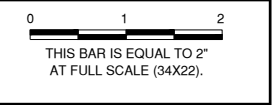
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SEE PAVEMENT MARKING PLAN - 1
FOR NOTES AND LEGEND

IL. CONTRACT: **D1035**
IL. LETTING ITEM: **02A**
IL. PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

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NUMBER	BY	DATE



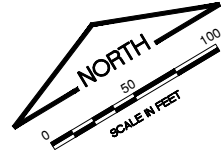
DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

PAVEMENT MARKING PLAN - 2

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DATE:	07/25/2025
JOB No:	22004684-00

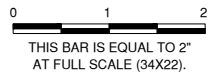
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SEE PAVEMENT MARKING PLAN - 1
FOR NOTES AND LEGEND

IL. CONTRACT: **DI035**
IL. LETTING ITEM: **02A**
IL. PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

REVISIONS		
NUMBER	BY	DATE



**DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30**

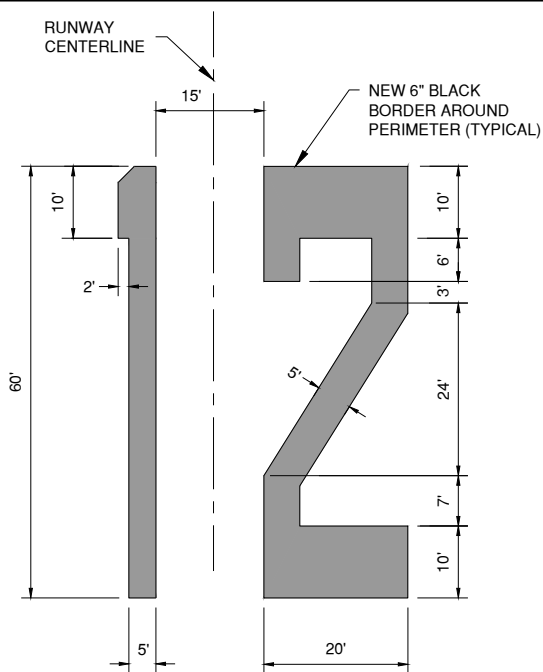
PAVEMENT MARKING PLAN - 3



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CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/25/2025
JOB No:	22004684-00

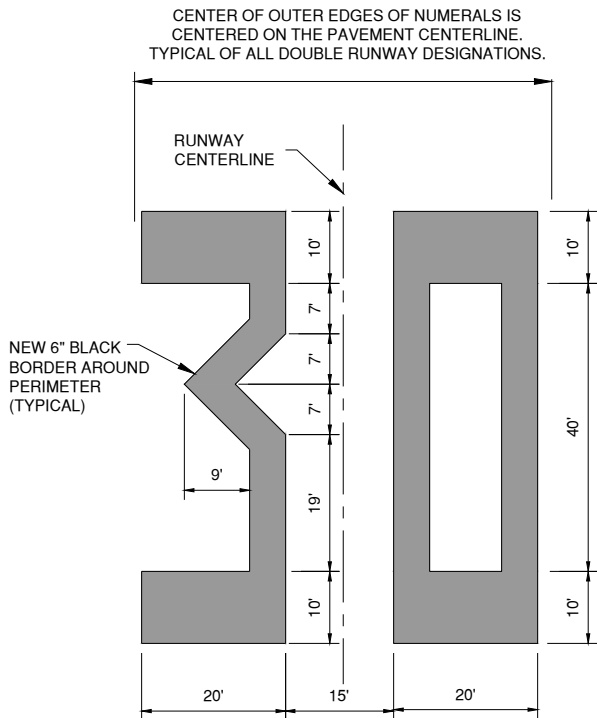
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BMP'05-21_AW_Tup'08



RUNWAY 12 LANDING DESIGNATOR

N.T.S.

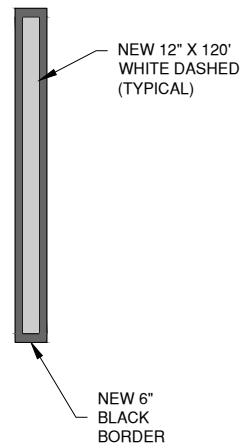


RUNWAY 30 LANDING DESIGNATOR

N.T.S.

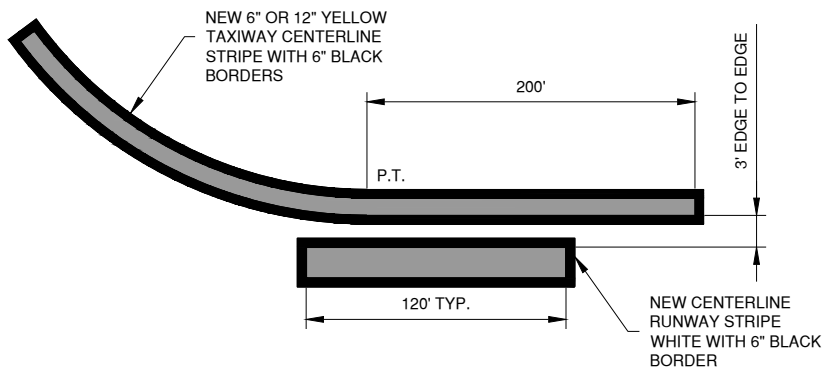
PAVEMENT MARKING NOTES

1. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
2. WHEN YELLOW TAXIWAY STRIPING CROSSES ANY WHITE RUNWAY STRIPING, THE TAXIWAY STRIPE SHALL BE TERMINATED 6" EACH SIDE OF RUNWAY STRIPE EXCEPT RUNWAY/RUNWAY HOLDLINE (3').
3. ALL NEW AIRFIELD PAVEMENT MARKING SHALL HAVE REFLECTIVE BEADS AND 6" BLACK BORDER.
4. BLACK BORDER DOES NOT RECEIVE REFLECTIVE BEADS.
5. ALL NUMERALS SHALL CONFORM TO FAA ADVISORY CIRCULAR 150/5340-1 (LATEST EDITION).
6. WHEN EXISTING MARKINGS ARE TO BE REPAINTED OVER, THE CONTRACTOR SHALL SCRAPE THE EXISTING MARKINGS TO REMOVE LOOSE PAINT PRIOR TO NEW MARKING APPLICATION.
7. PAINT SHALL BE APPLIED IN TWO APPLICATIONS. THE FIRST APPLICATION WILL OCCUR ONCE THE SURFACE COURSE IS IN PLACE AND ACCEPTED BY THE RESIDENT ENGINEER. THE FIRST APPLICATION WILL NOT CONTAIN BEADS AND BLACK BORDER. A PERIOD OF 30 DAYS SHALL ELAPSE BETWEEN THE PLACEMENT OF THE FIRST PAINT APPLICATION AND SECOND PAINT APPLICATION. THE SECOND APPLICATION SHALL CONTAIN GLASS BEADS AND BLACK BORDER. PAINT SHALL BE APPLIED AT THE LOCATIONS AND TO THE DIMENSIONS AND SPACING AS SPECIFIED. PAINT SHALL NOT BE APPLIED UNTIL THE LAYOUT AND CONDITION OF THE SURFACE HAS BEEN APPROVED BY THE RESIDENT ENGINEER.



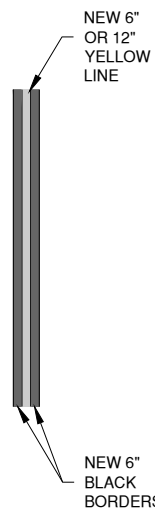
RUNWAY 12/30 CENTERLINE DASHED

N.T.S.



NEW TAXIWAY EXIT MARKING DETAIL

N.T.S.



TAXIWAY CENTERLINE CONTINUOUS

N.T.S.

IL. CONTRACT: **D1035**
IL. LETTING ITEM: **02A**
IL. PROJECT: **C73-5064**
S.B.G. PROJECT: **3-17-SBGP-197**

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

DIXON MUNICIPAL AIRPORT
DIXON, ILLINOIS
REHABILITATE RUNWAY 12/30

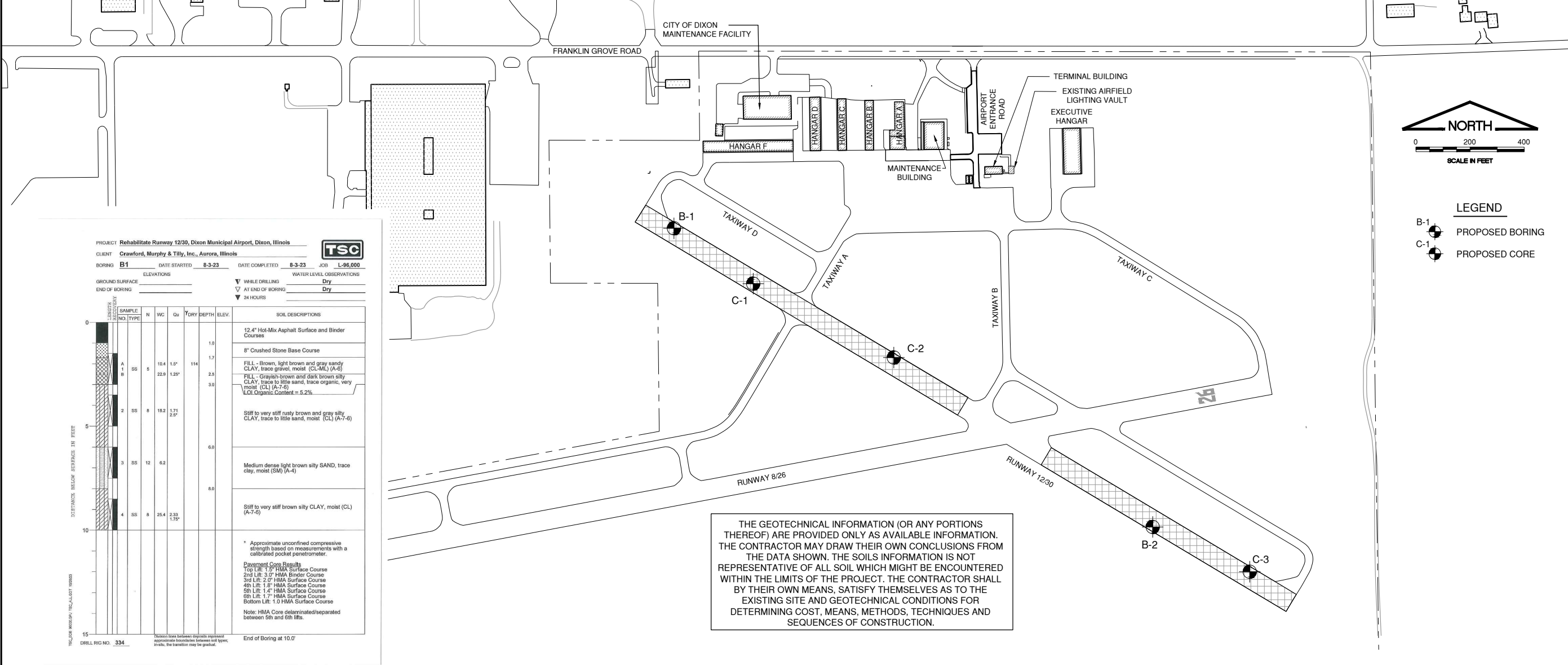
PAVEMENT MARKING DETAILS

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JOB No:	22004684-00

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SHEET 21 OF 22 SHEETS



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IL CONTRACT: D1035		
IL LETTING ITEM: 02A		
IL PROJECT: C73-5064		
S.B.G. PROJECT: 3-17-SBGP-197		
REVISIONS		
NUMBER	BY	DATE
0 1 2		
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DIXON MUNICIPAL AIRPORT DIXON, ILLINOIS REHABILITATE RUNWAY 12/30

GEOTECHNICAL ENGINEERING INFORMATION

DESIGN BY: STL	
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CHECKED BY: STL	
APPROVED BY: DKP	
DATE: 07/25/2025	
JOB NO: 22004684-00	
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SHEET 22 OF 22 SHEETS	

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