

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	1

BRIDGE REHABILITATION CONTRACT NO. 74C55

D-97-101-23

FOR INDEX OF SHEETS, SEE SHEET NO. 2  
ADT 3450 (YEAR 2021)

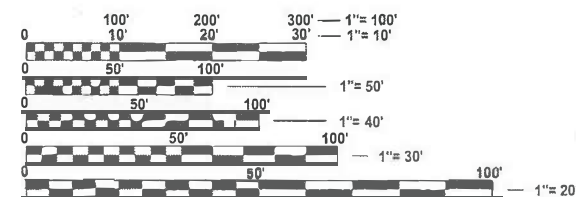
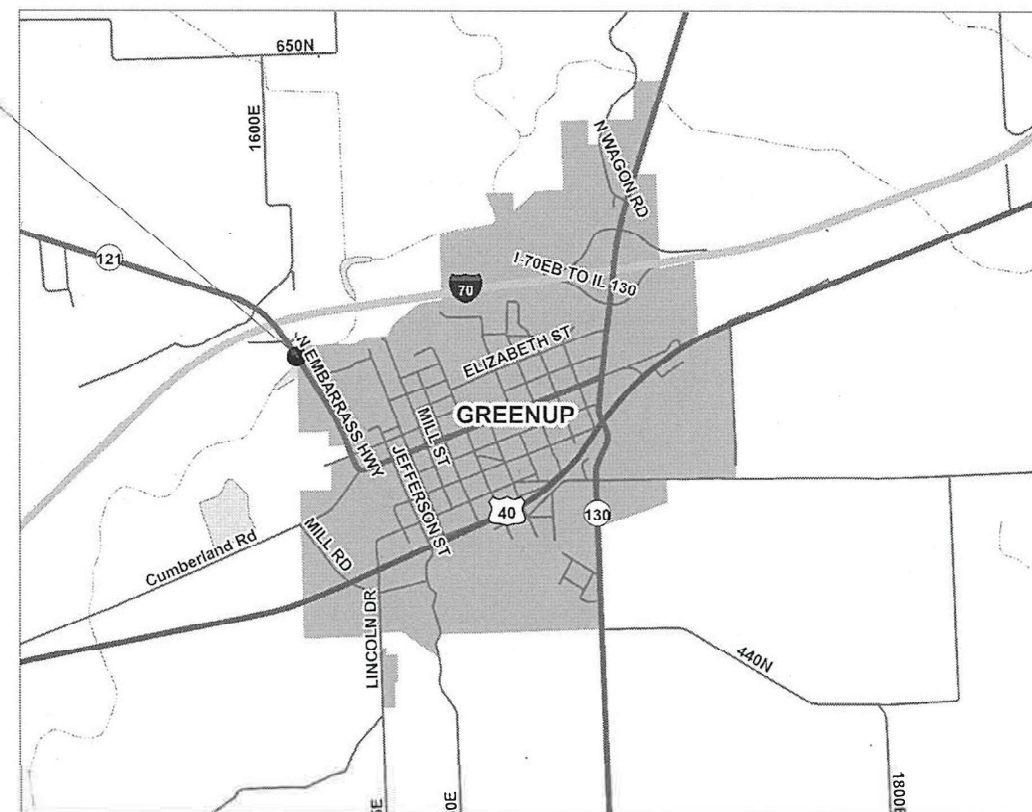
# PROPOSED HIGHWAY PLANS

FAP ROUTE 773 (IL 121)  
D7 BRIDGE REPAIRS 2025-6  
PROJECT STP-LL63(358)  
BRIDGE REHABILITATION  
CUMBERLAND COUNTY

C-97-132-23



PROJECT LOCATION:  
018-0019 OVER  
EMBARRAS RIVER



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: JACQUELYN SEALS  
PROJECT MANAGER: LEAH HILLE

CONTRACT NO. 74C55

GROSS LENGTH = 458.5 FT. = 0.087 MILE  
NET LENGTH = 458.5 FT. = 0.087 MILE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED JUNE 25 2024  
Jeffrey P. Meyer REGIONAL ENGINEER  
August 16, 2024  
Joe A. Elk  
ENGINEER OF DESIGN AND ENVIRONMENT  
August 16, 2024  
James J. [Signature]  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION 13

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OF THE STATE OF ILLINOIS

**GENERAL NOTES**

THE WORK IN THIS SECTION CONSIST OF REMOVAL OF EXISTING JOINT AND CONSTRUCT STRIP SEAL EXPANSION JOINT, BRIDGE DECK SCARIFICATION, FULL DEPTH DECK PATCHING, BRIDGE DECK OVERLAY, DIAMOND GRINDING & BRIDGE DECK GROOVING, SCUPPER MODIFICATIONS, PAVEMENT MARKING, AND ANY OTHER WORK NEEDED TO COMPLETE THIS SECTION.

THE WORK SHALL BE COMPLETED UTILIZING STAGE CONSTRUCTION WITH TEMPORARY TRAFFIC SIGNALS AND TEMPORARY CONCRETE BARRIER.

**INDEX OF SHEETS**

SHEET NO.	ITEM
1	COVER SHEET
2	INDEX OF SHEETS, STANDARDS & GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5	SCHEDULE OF PRICES
6-8	TYPICALS & BUTT JOINT DETAILS
9-10	STAGING
11	SRUCTURE PLAN SHEETS
12-19	STRUCTURE REPAIR PLAN SHEETS
20-23	PAVEMENT MARKING DETAILS

THE FOLLOWING STANDARDS ARE A PART OF THESE PLANS AND ARE INCLUDED AFTER SHEET NO. 23:

STANDARD NO.	DESCRIPTION
000001-08	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
420001-10	PAVEMENT JOINTS
701001-02	OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 15' AWAY
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGI
701201-05	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS >= 45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701321-18	LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER
701901-09	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
780001-05	TYPICAL PAVEMENT MARKINGS
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

**APPLICATION RATES**

THE FOLLOWING APPLICATION RATES WERE USED IN CALCULATING PLAN QUANTITIES AND HAVE BEEN INCLUDED FOR REFERENCE:

BITUMINOUS MATERIALS (TACK COAT)	0.05 LB/SQ FT (ON MILLED SURFACES)
	0.025 LB/SQ FT (ON HMA LIFTS)
HMA SURFACE COURSE	112 LB/SY/IN

**MIXTURE REQUIREMENTS**

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE TO THIS PROJECT:

LOCATION(S)	MIXTURE USE(S)	PG	DESIGN AIR VOIDS	MIXTURE COMPOSITION	FRICTION AGGREGATE	MIXTURE WEIGHT	QUALITY MANAGEMENT PROGRAM	SUBLOT SIZE	MATERIAL TRANSFER DEVICE (REQUIRED?)
MAINLINE	HMA SURFACE COURSE, IL-9.5, MIX "C", N70	PG 64-22	4.0% @ N=70	IL - 9.5	MIXTURE C	N70	QCQA	1000	N/A

REV - MS

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	CHECKED -	REVISED -			CONTRACT NO. 74C55					
PLOT DATE = 5/1/2024	DATE -	REVISED -			SCALE:	SHEET 1	OF 1	SHEETS	STA.	TO STA.
					ILLINOIS   FED. AID PROJECT BRIDGE REHABILITATION					

80% FED  
20% STATE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				0059
20101000	TEMPORARY FENCE	FOOT	1540	1540
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	256	256
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	356	356
40600990	TEMPORARY RAMP	SQ YD	27	27
40604052	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "C", N70	TON	48	48
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	7	7
50102400	CONCRETE REMOVAL	CU YD	12.7	12.7
50300255	CONCRETE SUPERSTRUCTURE	CU YD	14.3	14.3
50300300	PROTECTIVE COAT	SQ YD	1066	1066
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2160	2160
50800515	BAR SPLICERS	EACH	36	36
52000110	PREFORMED JOINT STRIP SEAL	FOOT	68	68
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4	4
67100100	MOBILIZATION	L SUM	1	1
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	1	1

80% FED  
20% STATE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				0059
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	2	2
70107005	PAVEMENT MARKING BLACKOUT TAPE, 5"	FOOT	1314	1314
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	28	28
70300100	SHORT TERM PAVEMENT MARKING	FOOT	145	145
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	596	596
70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	3571	3571
70400100	TEMPORARY CONCRETE BARRIER	FOOT	539	539
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	515	515
70600250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2
70600350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	3571	3571
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	2	2
Z0012111	BRIDGE DECK FLY ASH OR GGBF SLAG CONCRETE OVERLAY, 2 1/2"	SQ YD	1023	1023

\* SPECIALTY ITEM

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REV - MS

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PLOT DATE = 5/1/2024	DATE -	REVISED -		CONTRACT NO. 74C55 BRIDGE REHABILITATION							

80% FED  
20% STATE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				0059
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	1023	1023
Z0016001	DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	15	15
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	9	9
Z0018051	DRAINAGE SCUPPERS TO BE ADJUSTED	EACH	4	4
Z0029090	DIAMOND GRINDING (BRIDGE SECTION)	SQ YD	929	929
X5030250	BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	787	787

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TRAFFIC CONTROL SCHEDULE	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	RELOCATE IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	TEMPORARY CONCRETE BARRIER	RELOCATE TEMPORARY CONCRETE BARRIER	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	TEMPORARY BRIDGE TRAFFIC SIGNALS	PAVEMENT MARKING BLACKOUT TAPE, 5"
LOCATION	EACH	EACH	FOOT	FOOT	EACH	L SUM	EACH	FOOT
STRUCTURE 018-0019	-	-	-	-	1	1	2	-
STAGE 1	2	-	539	-	-	-	-	1118
STAGE 2	-	2	-	515	-	-	-	196
<b>TOTALS:</b>	<b>2</b>	<b>2</b>	<b>539</b>	<b>515</b>	<b>1</b>	<b>1</b>	<b>2</b>	<b>1314</b>

RESURFACING SCHEDULE			HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT	BITUMINOUS MATERIALS (TACK COAT)	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "C", N70	TEMPORARY RAMP
STATION	TO	STATION	SQ YD	POUND	TONS	SQ YD
248+10	TO	248+90	-	128	24	13
248+40	TO	249+90	178	-	-	-
244+32	TO	245+12	-	128	24	13
244+32	TO	244+82	178	-	-	-
			<b>356</b>	<b>256</b>	<b>48</b>	<b>27</b>

PAVEMENT MARKING SCHEDULE	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	SHORT TERM PAVEMENT MARKING	SHORT TERM PAVEMENT MARKING REMOVAL	PAINT PAVEMENT MARKING - LINE 4"	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL
LOCATION	FOOT	FOOT	SQ FT	FOOT	EACH
STRUCTURE 018-0019	3571	145	596	3571	2
<b>TOTALS:</b>	<b>3571</b>	<b>145</b>	<b>596</b>	<b>3571</b>	<b>2</b>

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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

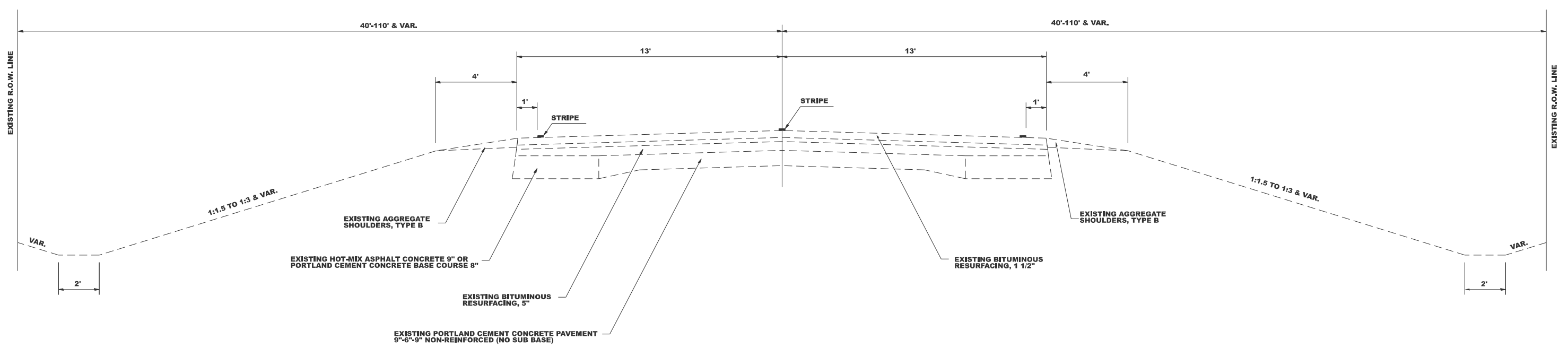
**SCHEDULE OF QUANTITIES**

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F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	5
CONTRACT NO. 74C55				
ILLINOIS FED. AID PROJECT				

BRIDGE REHABILITATION

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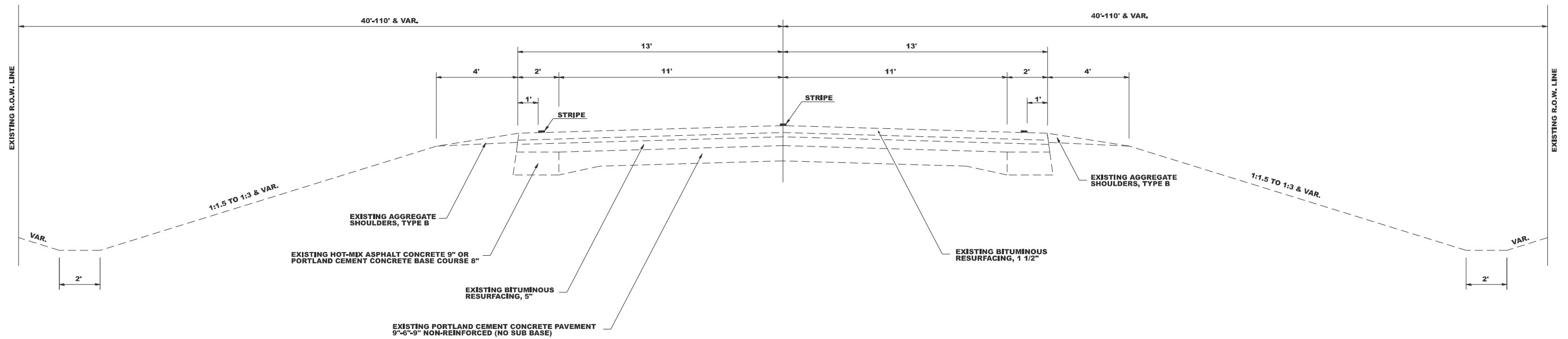
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>TYPICAL EXISTING SECTIONS</b>			
SCALE:	SHEET 1	OF 2	SHEETS
	STA. 240 + 41.04	TO STA. 248 + 10.25	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	6
CONTRACT NO. 74C55				
ILLINOIS FED. AID PROJECT				

BRIDGE REHABILITATION



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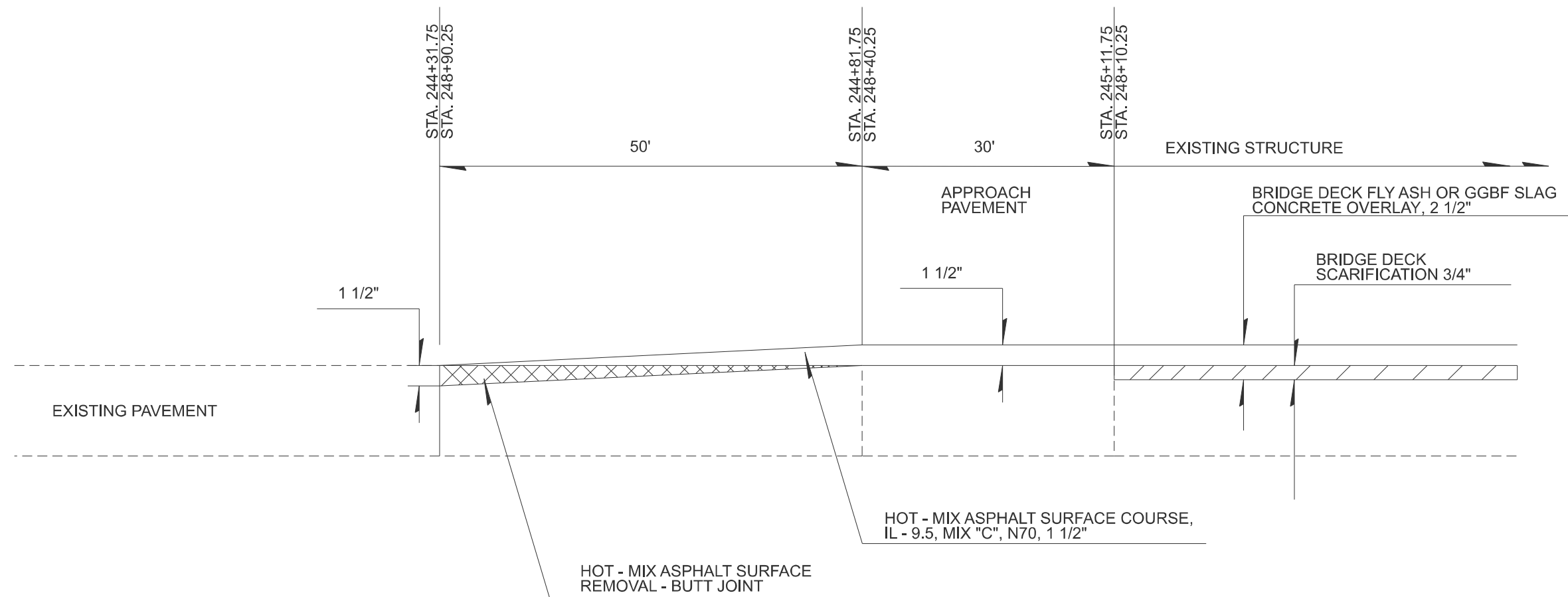
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL EXISTING SECTIONS**

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	7
CONTRACT NO. 74C55				
ILLINOIS FED. AID PROJECT				

BRIDGE REHABILITATION



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PLOT DATE = 5/1/2024	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

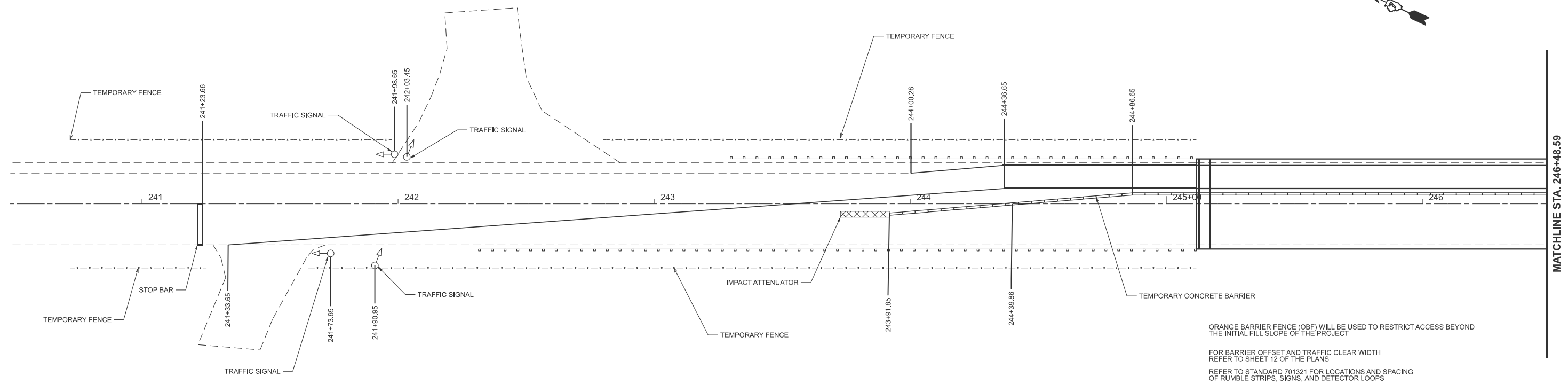
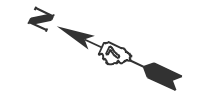
**BUTT JOINT DETAIL**

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	8
CONTRACT NO. 74C55				

ILLINOIS FED. AID PROJECT  
BRIDGE REHABILITATION



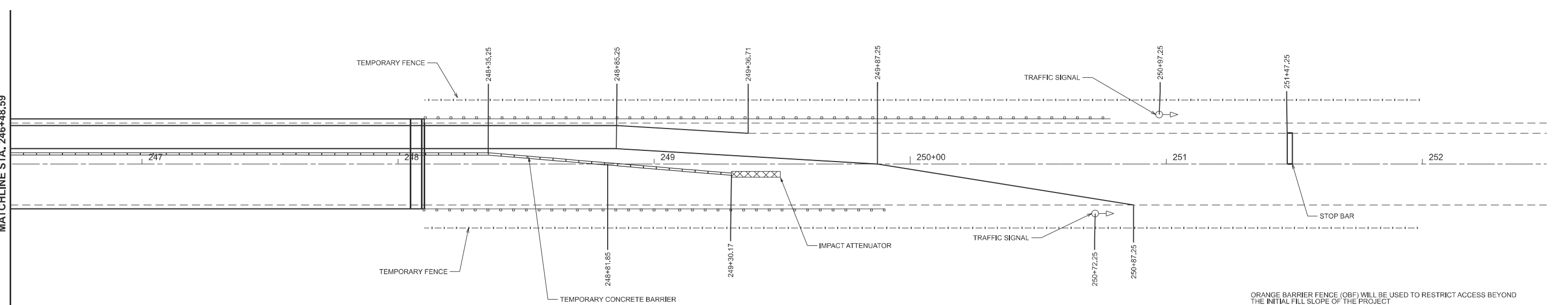
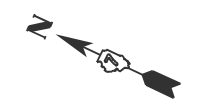


ORANGE BARRIER FENCE (OBF) WILL BE USED TO RESTRICT ACCESS BEYOND THE INITIAL FILL SLOPE OF THE PROJECT

FOR BARRIER OFFSET AND TRAFFIC CLEAR WIDTH REFER TO SHEET 12 OF THE PLANS

REFER TO STANDARD 701321 FOR LOCATIONS AND SPACING OF RUMBLE STRIPS, SIGNS, AND DETECTOR LOOPS

MATCHLINE STA. 246+48.59



ORANGE BARRIER FENCE (OBF) WILL BE USED TO RESTRICT ACCESS BEYOND THE INITIAL FILL SLOPE OF THE PROJECT

FOR BARRIER OFFSET AND TRAFFIC CLEAR WIDTH REFER TO SHEET 12 OF THE PLANS

REFER TO STANDARD 701321 FOR LOCATIONS AND SPACING OF RUMBLE STRIPS, SIGNS, AND DETECTOR LOOPS

MATCHLINE STA. 246+48.59

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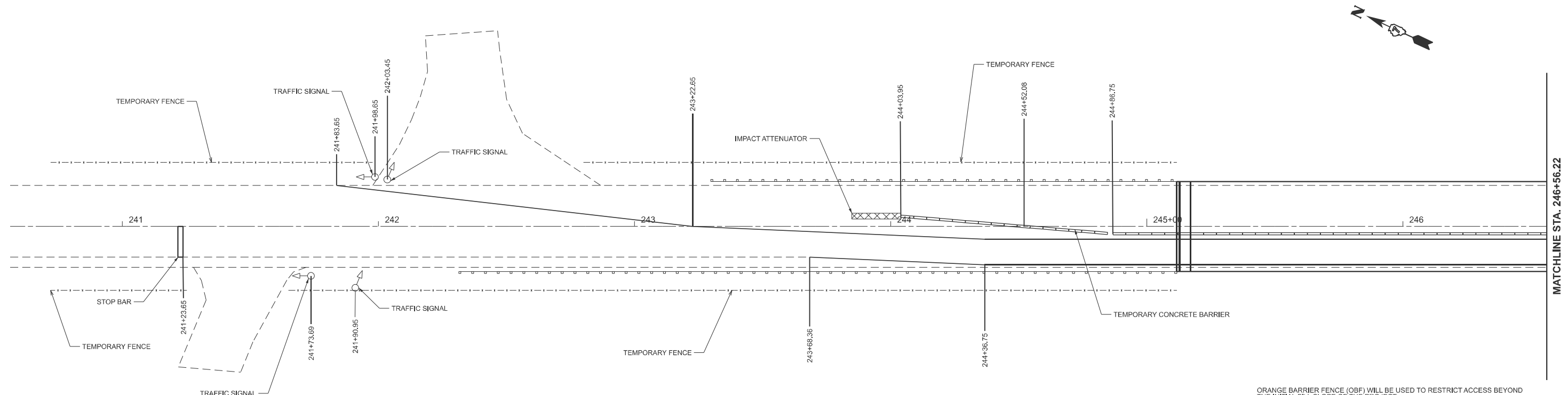
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE I CONSTRUCTION**

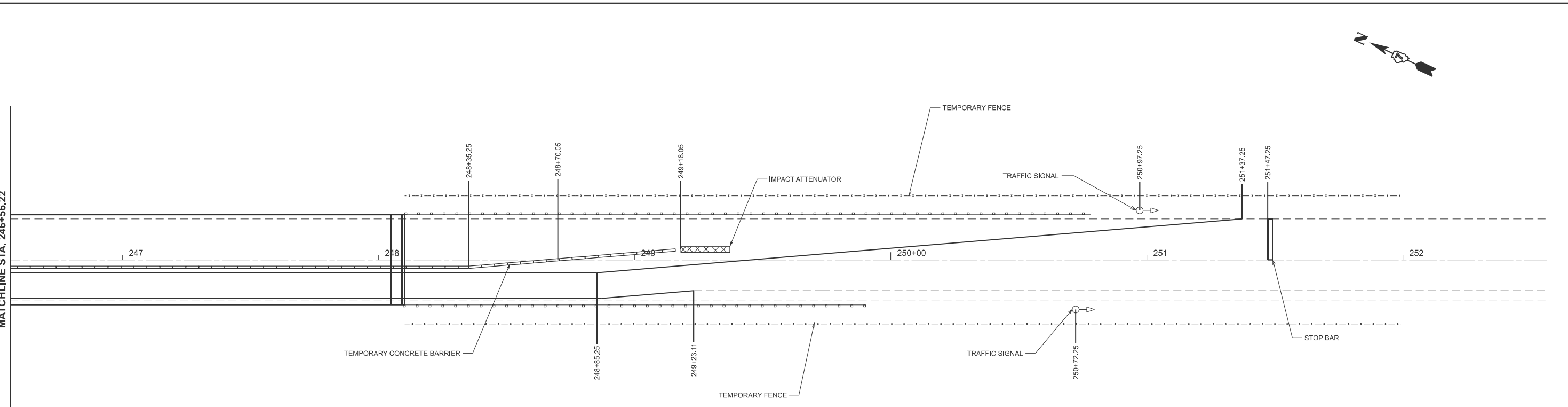
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	9
CONTRACT NO. 74C55				
ILLINOIS FED. AID PROJECT				

BRIDGE REHABILITATION



ORANGE BARRIER FENCE (OBF) WILL BE USED TO RESTRICT ACCESS BEYOND THE INITIAL FILL SLOPE OF THE PROJECT  
 FOR BARRIER OFFSET AND TRAFFIC CLEAR WIDTH REFER TO SHEET 12 OF THE PLANS  
 REFER TO STANDARD 701321 FOR LOCATIONS AND SPACING OF RUMBLE STRIPS, SIGNS, AND DETECTOR LOOPS



ORANGE BARRIER FENCE (OBF) WILL BE USED TO RESTRICT ACCESS BEYOND THE INITIAL FILL SLOPE OF THE PROJECT  
 FOR BARRIER OFFSET AND TRAFFIC CLEAR WIDTH REFER TO SHEET 12 OF THE PLANS  
 REFER TO STANDARD 701321 FOR LOCATIONS AND SPACING OF RUMBLE STRIPS, SIGNS, AND DETECTOR LOOPS

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PLOT DATE = 8/7/2024	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

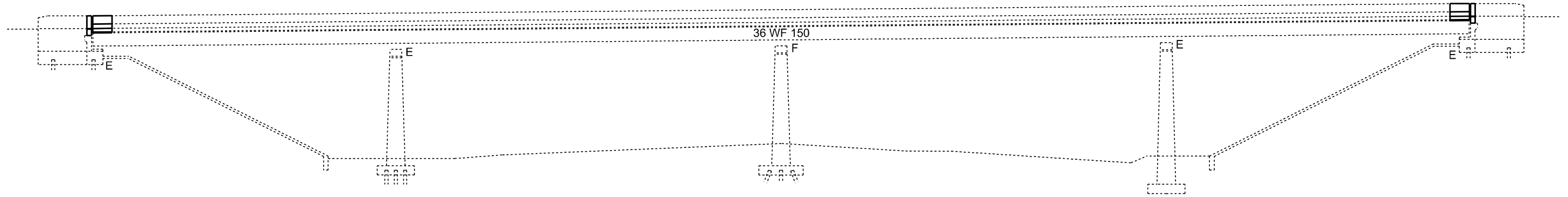
**STAGE II CONSTRUCTION**

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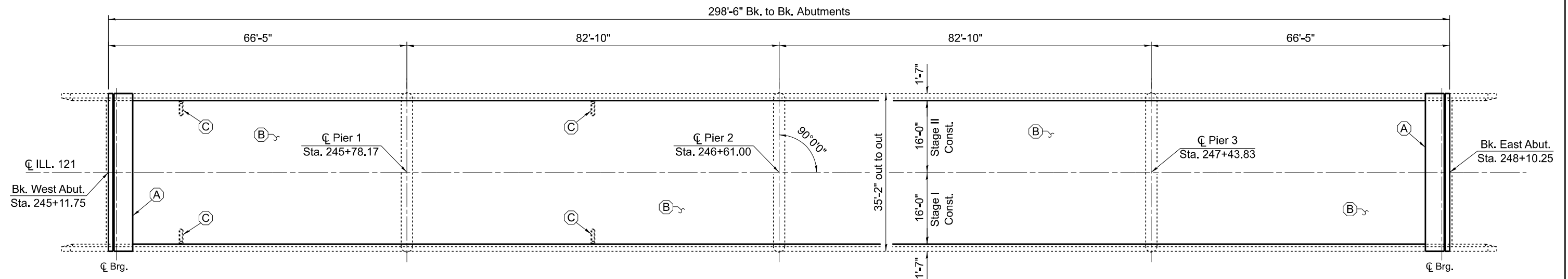
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773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	10
CONTRACT NO. 74C55				

ILLINOIS FED. AID PROJECT  
 BRIDGE REHABILITATION

EXISTING STRUCTURE: SN 018-0019 carries ILL 121 over Embarras River.  
 The four span, steel beam structure was built in 1960, with a new deck constructed in 1995. The proposed project consists of replacement of the expansion joints with new strip seals, full depth deck patching, scarification, & concrete overlay

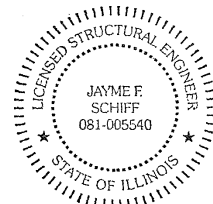


ELEVATION



PLAN

- (A) - Remove Existing Joint and Construct Strip Seal Expansion Joint
- (B) - Bridge Deck Scarification  $\frac{3}{4}$ ", Deck Patching, Bridge Deck Fly Ash or GGBF Slag Concrete Overlay  $2\frac{1}{2}$ ", Diamond Grinding (Bridge Section), & Bridge Deck Grooving (Longitudinal)
- (C) - Adjust Existing Scupper



EXPIRES 11-30-2024

*Jayme F. Schiff*

MODEL: Default  
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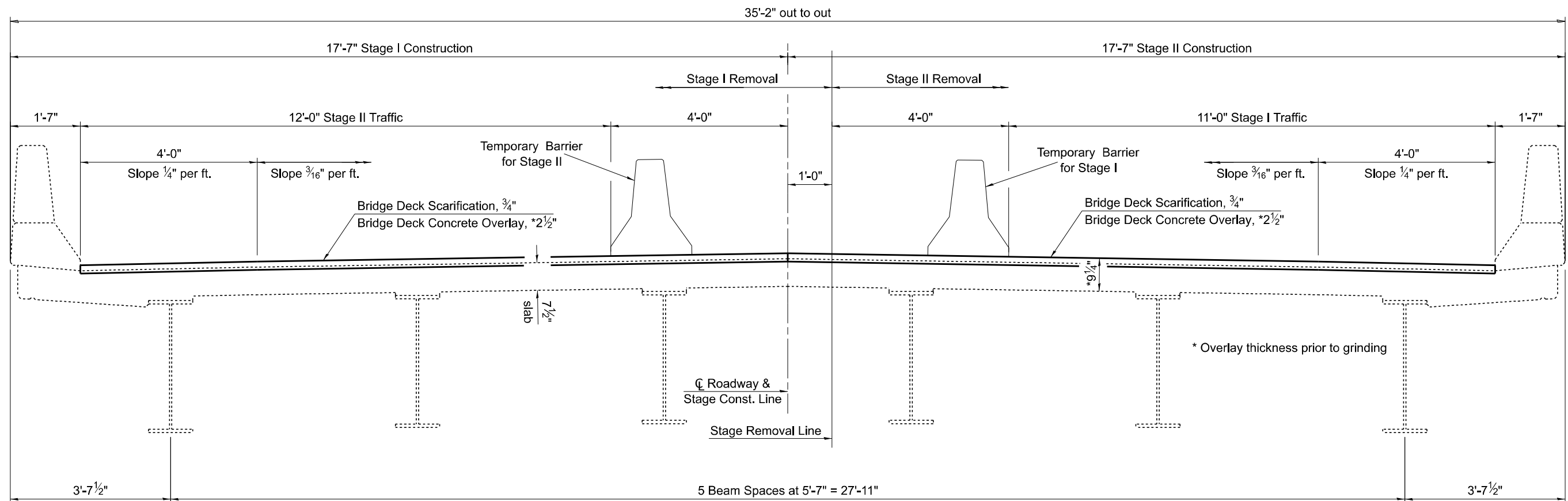
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PLOT DATE = 5/3/2024	DATE - July 2023	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION  
 S.N. 018-0019

SCALE: SHEET 1 OF 9 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	11
			CONTRACT NO. 74C55	
		ILLINOIS	FED. AID PROJECT	



**CROSS SECTION**  
Looking North

**GENERAL NOTES**

Plan dimensions and details relative to the existing structure have been taken from existing plans and are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Reinforcement Bars designated (E) shall be epoxy coated.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal operations shall be replaced with an approved bar splicer or anchorage system. Cost included with CONCRETE REMOVAL.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

Protective Coat to be applied to areas of new concrete only, including bridge deck concrete overlay.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50 °F.

Areas of deck repairs shown are estimated. The Engineer shall show actual locations of deck repairs on as-built plans.

Full depth deck slab repairs performed in the exterior bays of the bridge deck (between the parapet walls and the first interior beams) shall be limited to individual lengths no greater than 10'. In these portions of the deck, repair areas longer than 10' shall be divided into segments not greater than 10' in length, and the segments shall be poured in alternating sequence. Subsequent segments repaired in sequence shall not be removed until 72 hours shall have elapsed from the end of the previous, adjacent pour, and the adjacent pour shall have attained a minimum modulus of rupture of 650psi.

**TOTAL BILL OF MATERIALS**

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	12.7
Concrete Superstructure	Cu. Yd.	14.3
Reinforcement Bars, Epoxy Coated	Pound	2,160
Bar Splicers	Each	36
Preformed Joint Strip Seal	Foot	68
Bridge Deck Scarification, 3/4"	Sq. Yd.	1,023
Bridge Deck Fly Ash or GGBF Slag Concrete Overlay, 2 1/2"	Sq. Yd.	1,023
Diamond Grinding (Bridge Section)	Sq. Yd.	929
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	787
Protective Coat	Sq. Yd.	1,066
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	15.0
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	9.0
Drainage Scuppers to be Adjusted	Each	4

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PLOT DATE = 7/5/2024	DATE - July 2023	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

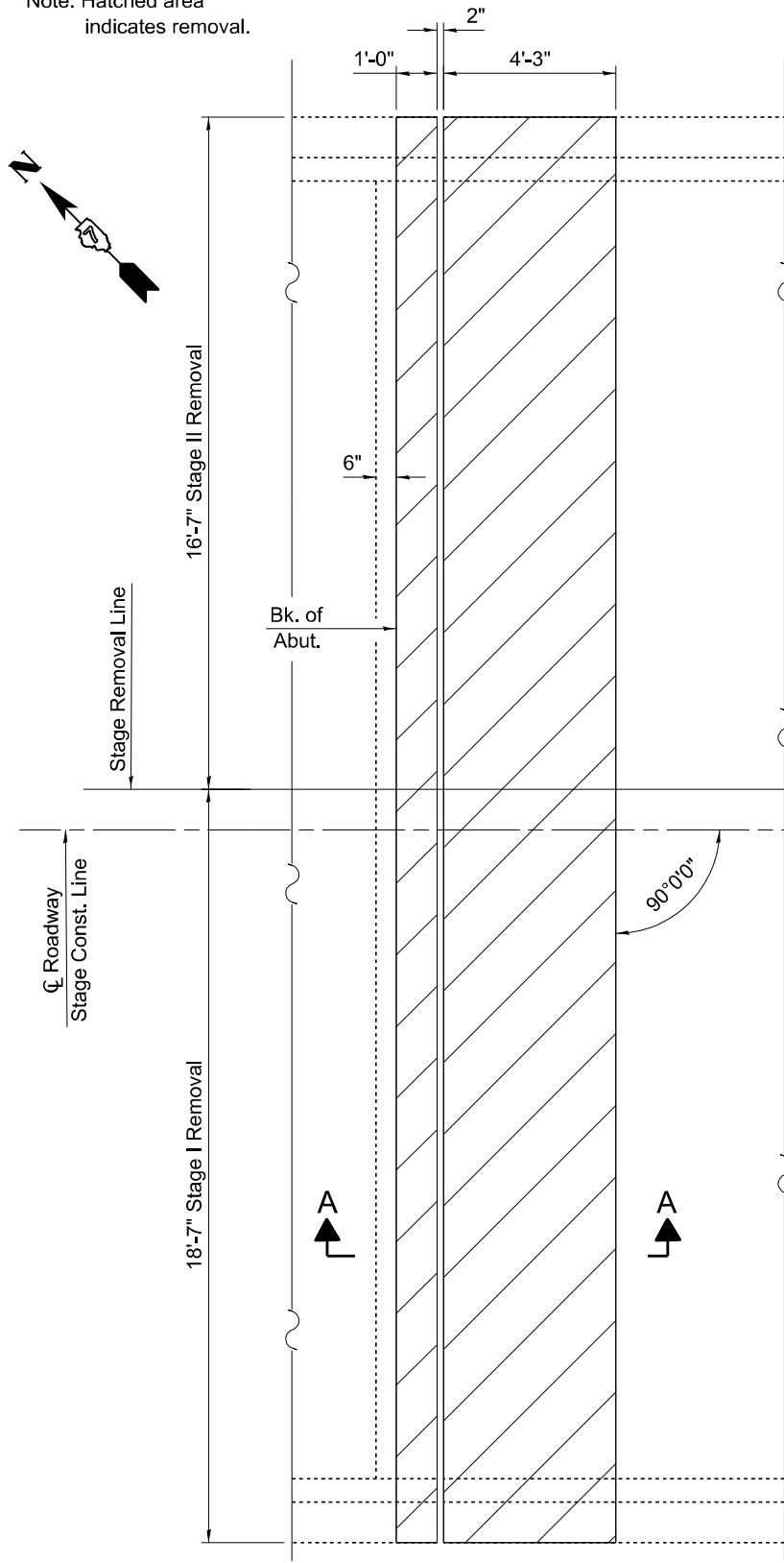
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**BILL OF MATERIALS S.N. 018-0019**

SCALE: SHEET 2 OF 9 SHEETS STA. TO STA.

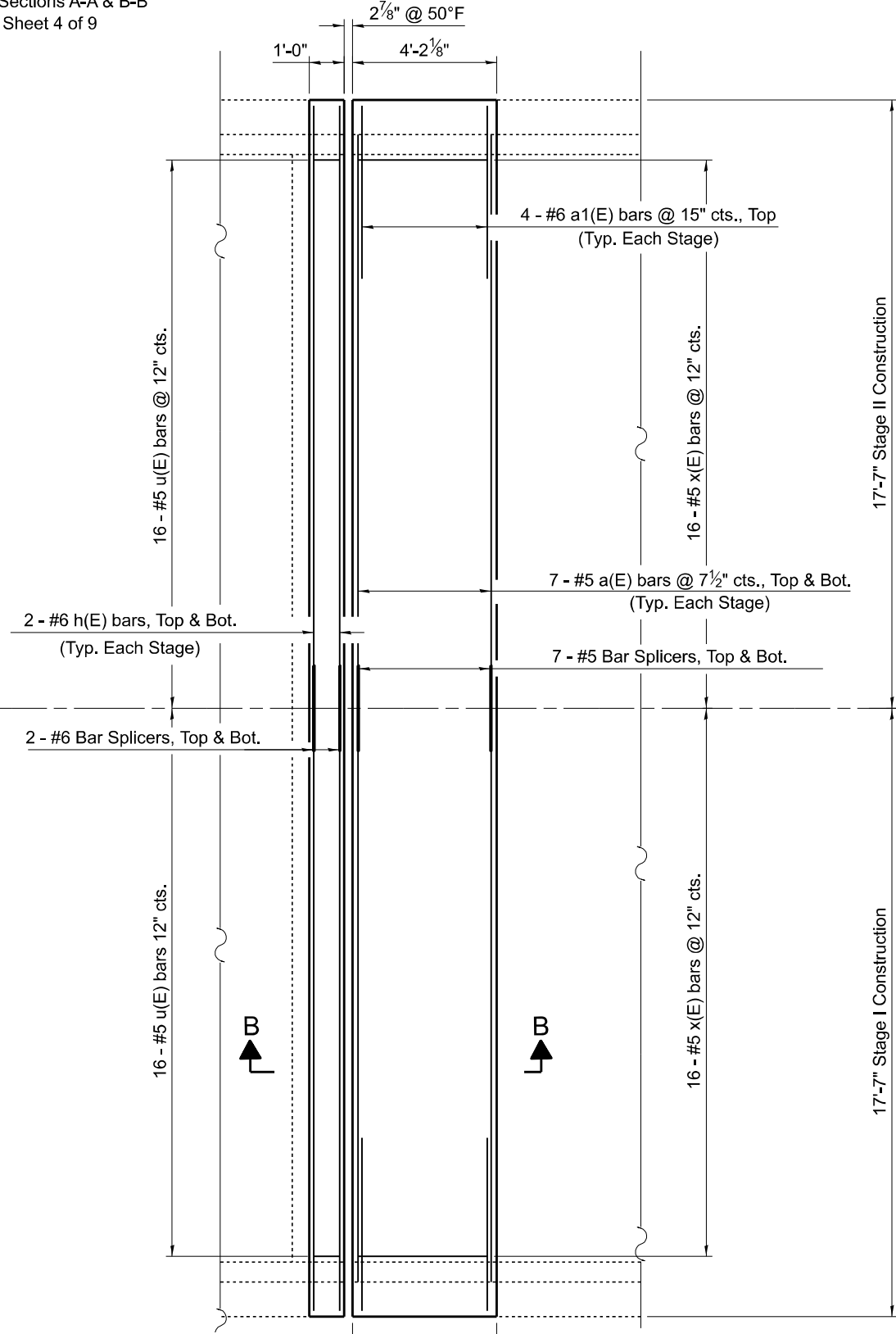
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773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	12
ILLINOIS FED. AID PROJECT			CONTRACT NO. 74C55	

Note: Hatched area indicates removal.

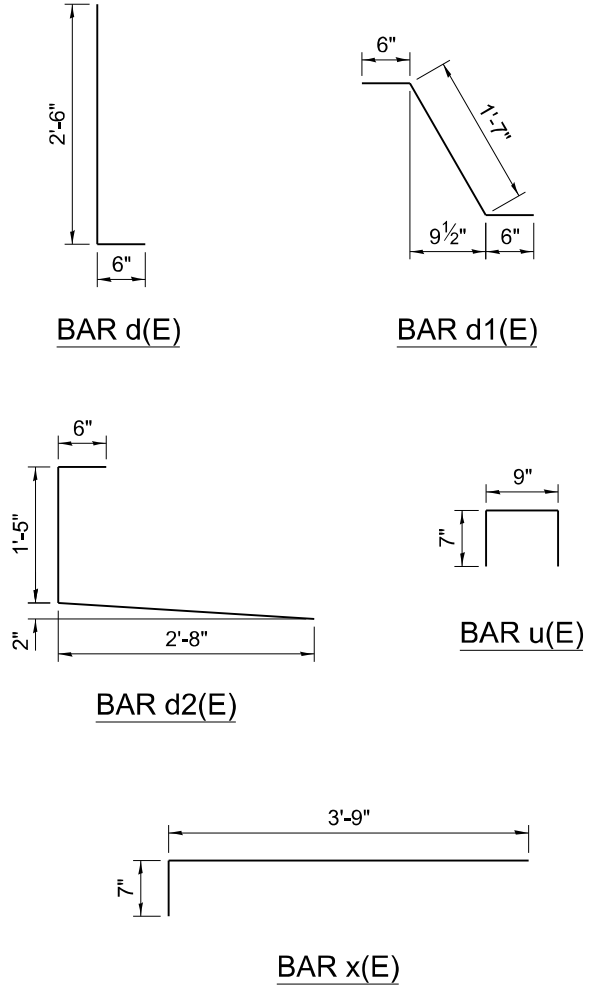
For Sections A-A & B-B  
See Sheet 4 of 9



**REMOVAL PLAN**  
North Abutment Shown  
South Abutment Similar



**REINFORCEMENT PLAN**  
North Abutment Shown  
South Abutment Similar



**BILL OF MATERIALS - TWO ABUTMENTS**

Bar	No.	Size	Length	Shape
a(E)	56	#5	16'-5"	—
a1(E)	16	#6	5'-0"	—
h(E)	16	#6	17'-3"	—
d(E)	40	#5	3'-0"	┌
d1(E)	20	#5	2'-7"	└
d2(E)	20	#4	4'-7"	┌
u(E)	64	#5	1'-11"	U
x(E)	64	#5	4'-4"	—

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	12.7
Concrete Superstructure	Cu. Yd.	14.3
Reinforcement Bars, Epoxy Coated	Pound	2,160
Bar Splicers	Each	36

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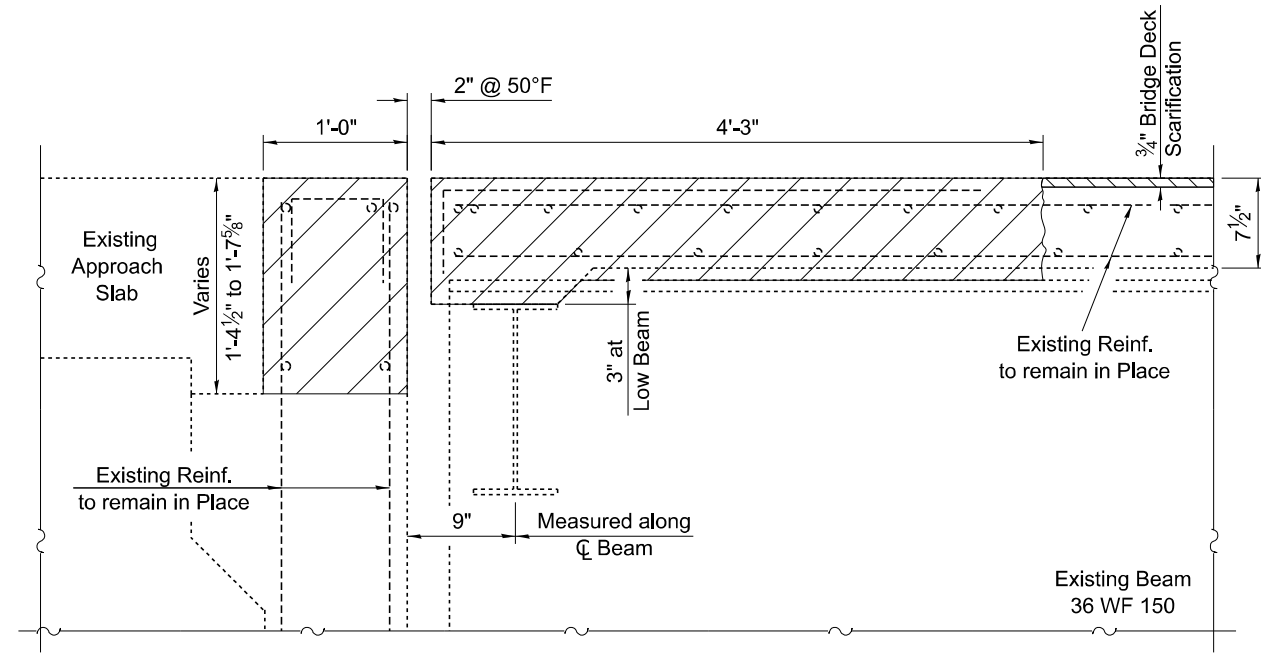
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**STATE OF ILLINOIS  
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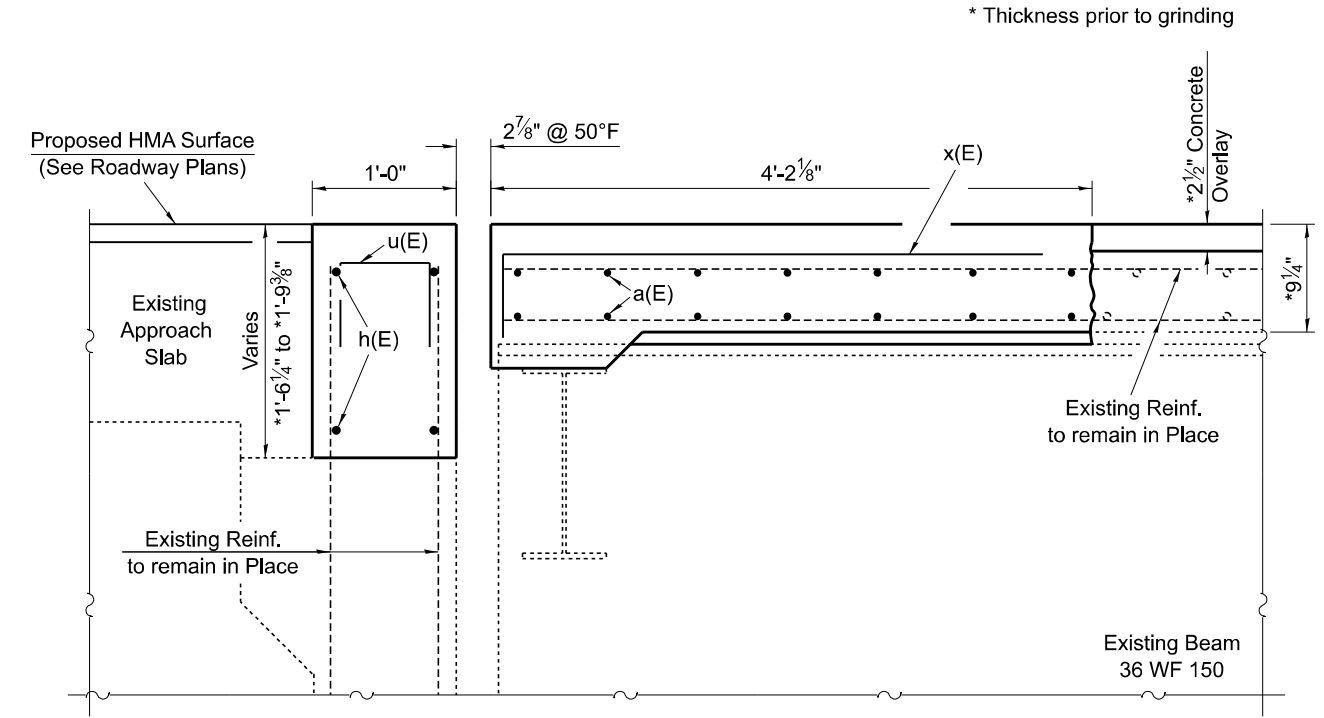
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S.N. 018-0019**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	13
			CONTRACT NO. 74C55	
SCALE:		SHEET 3 OF 9 SHEETS	STA.	TO STA.
ILLINOIS FED. AID PROJECT				

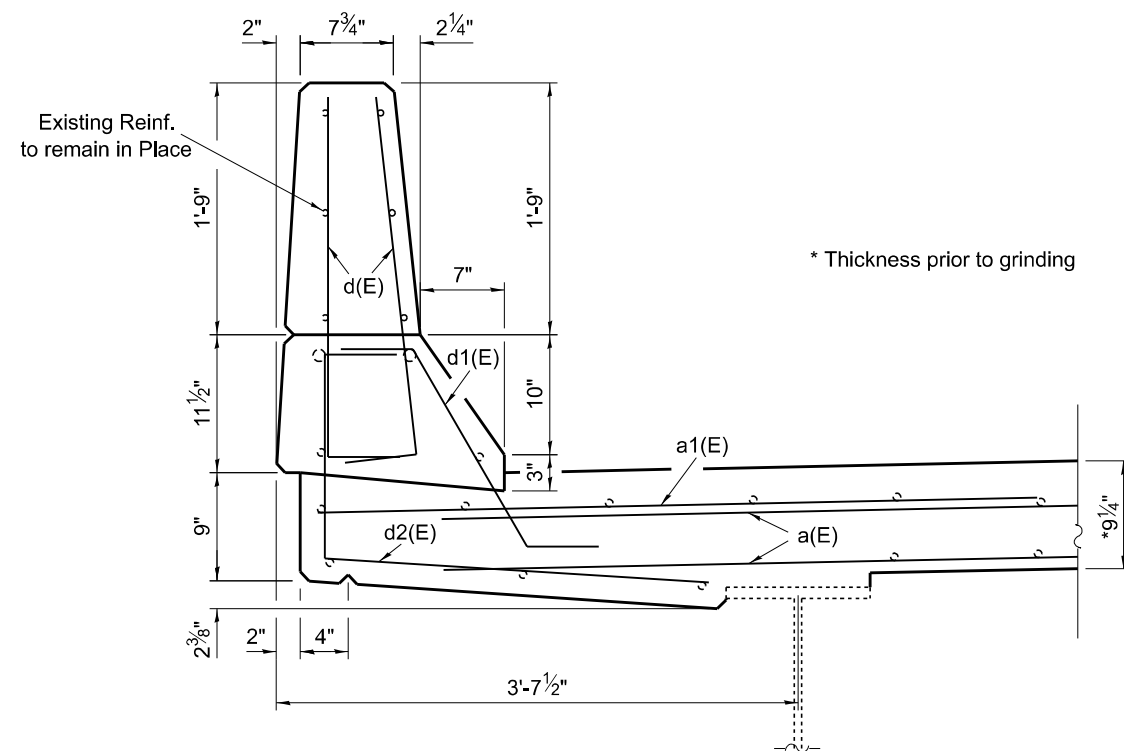
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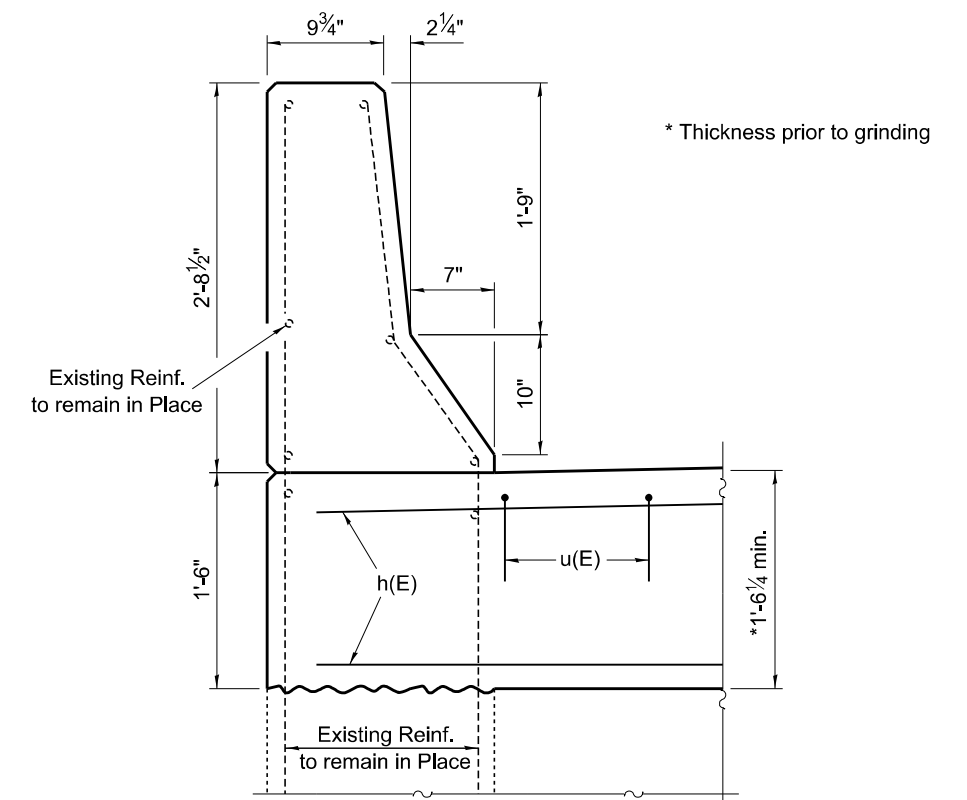
SECTION A-A



SECTION B-B



SECTION THRU DECK PARAPET



SECTION THRU APPROACH PARAPET

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EXPANSION JOINT REPLACEMENT DETAILS  
S.N. 018-0019**

SCALE: SHEET 4 OF 9 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	14
ILLINOIS FED. AID PROJECT			CONTRACT NO. 74C55	

**GENERAL NOTES**

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

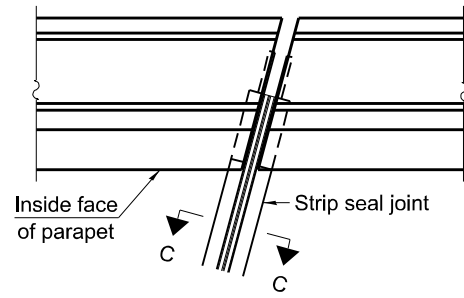
The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

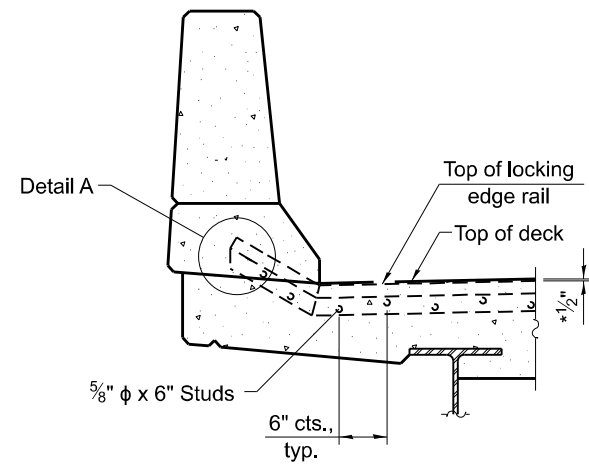
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

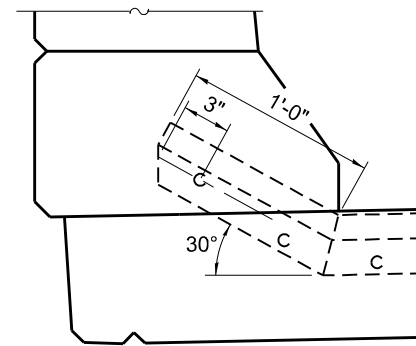
The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.



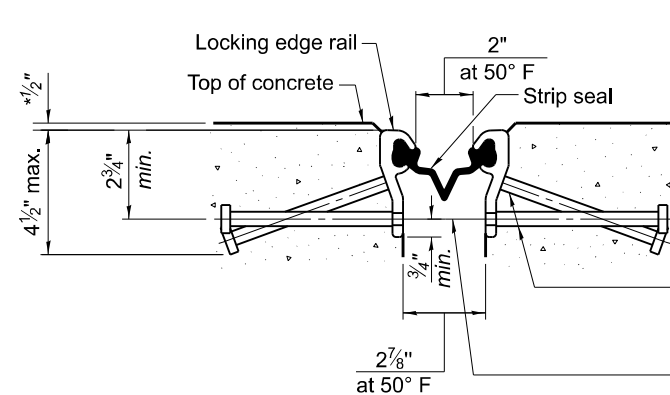
**PLAN AT PARAPET**



**ELEVATION AT PARAPET**



**DETAIL A**



**SHOWING ROLLED RAIL JOINT**

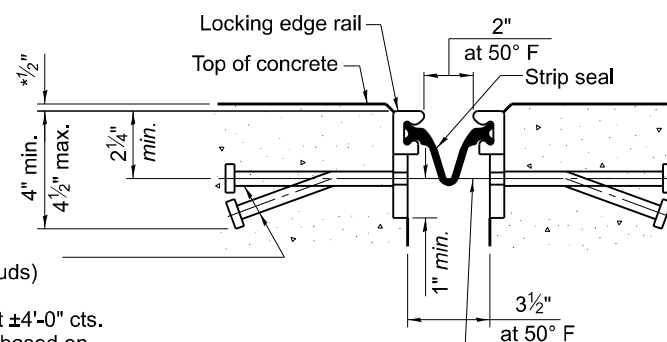
\* Thickness prior to grinding

\*\* 5/8" φ x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

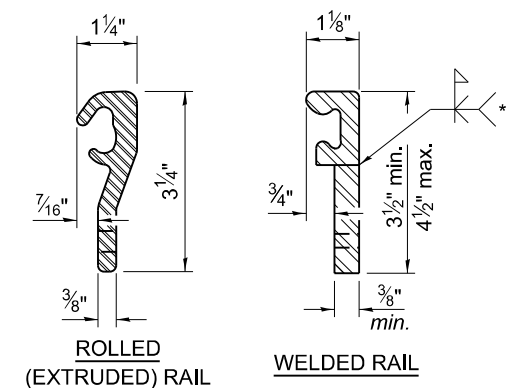
3/8" φ threaded rods in 7/16" φ holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

**SECTION C-C**

\*\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

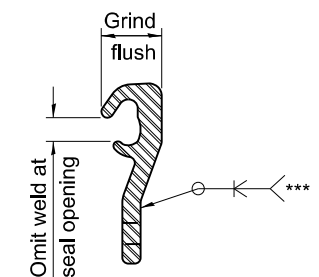


**SHOWING WELDED RAIL JOINT**



**LOCKING EDGE RAILS**

\*\*\* Back gouge not required if complete joint penetration is verified by mock-up.



**LOCKING EDGE RAIL SPLICE**

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

**BILL OF MATERIAL**

Item	Unit	Total
Preformed Joint Strip Seal	Foot	68

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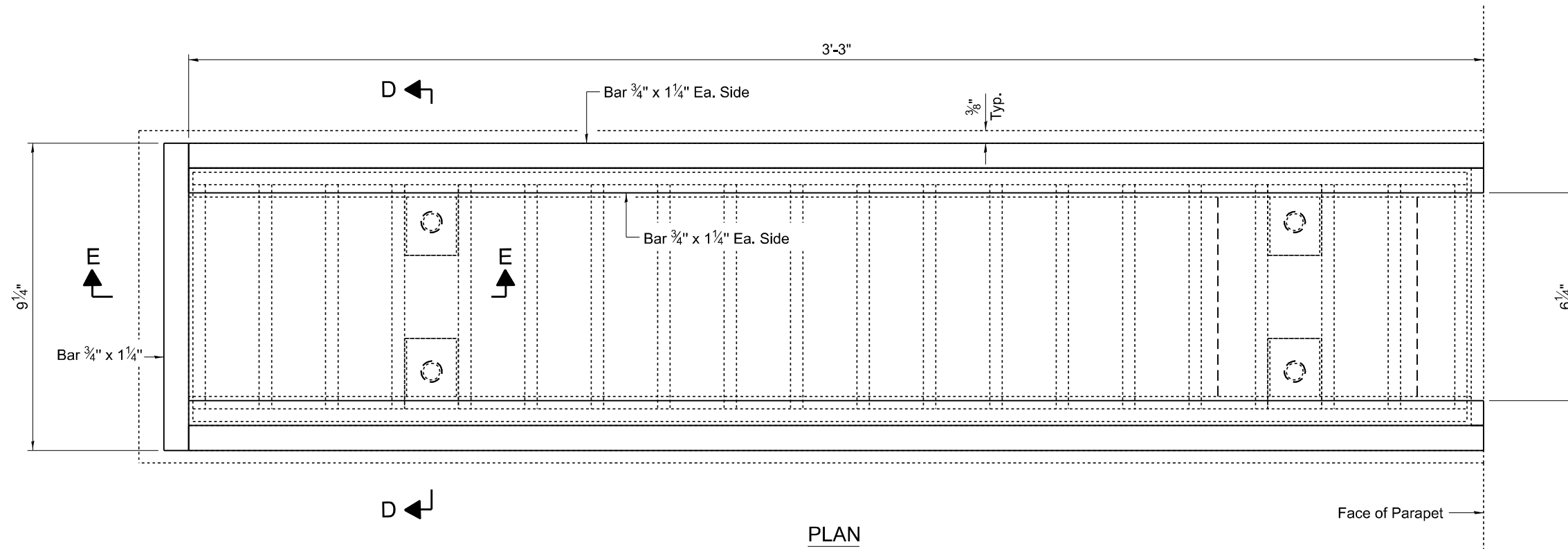
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PLOT DATE = 5/3/2024	DATE - July 2023	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PREFORMED JOINT STRIP SEAL DETAIL  
S.N. 018-0019**

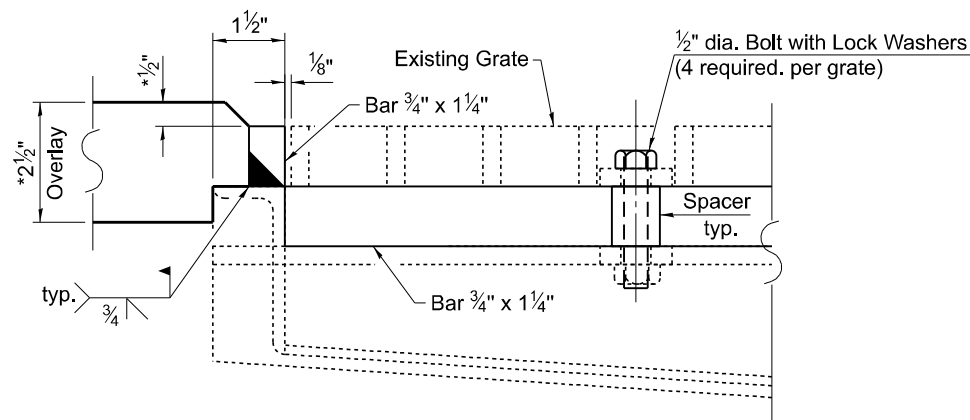
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	15
			CONTRACT NO. 74C55	
ILLINOIS FED. AID PROJECT				



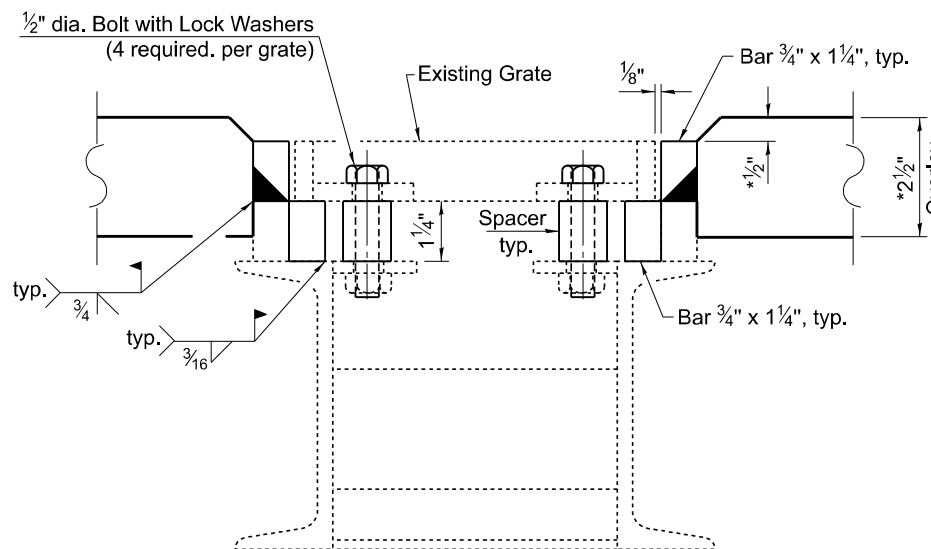
PLAN

\* Thickness prior to grinding



SECTION E-E

\* Thickness prior to grinding



SECTION D-D

**GENERAL NOTES**

The Contractor shall field verify dimensions and details of the existing inlet and make necessary adjustments prior to construction of new adjusting bars or ordering material for adjusting inlet.  
 All structural steel shall conform to AASHTO M-270, Grade 36.  
 The adjusting bars shall be galvanized according to AASHTO M111 and ASTM A385.  
 Bolts, anchor studs, washers, and nuts shall conform to the requirements of ASTM A307 and shall be galvanized according to the requirements of AASHTO M232.  
 The contractor shall ensure that no damage is done to existing grates to be reused.  
 Shop plans for proposed adjusting scupper ring shall be submitted for approval prior to fabrication.  
 Cost of all labor and materials necessary to remove existing grates, clean existing scuppers, install adjusting scupper rings and reinstalling grates is included in the cost per unit each for Drainage Scuppers to be Adjusted.

**BILL OF MATERIAL**

Item	Unit	Total
Drainage Scuppers to be Adjusted	Each	4

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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

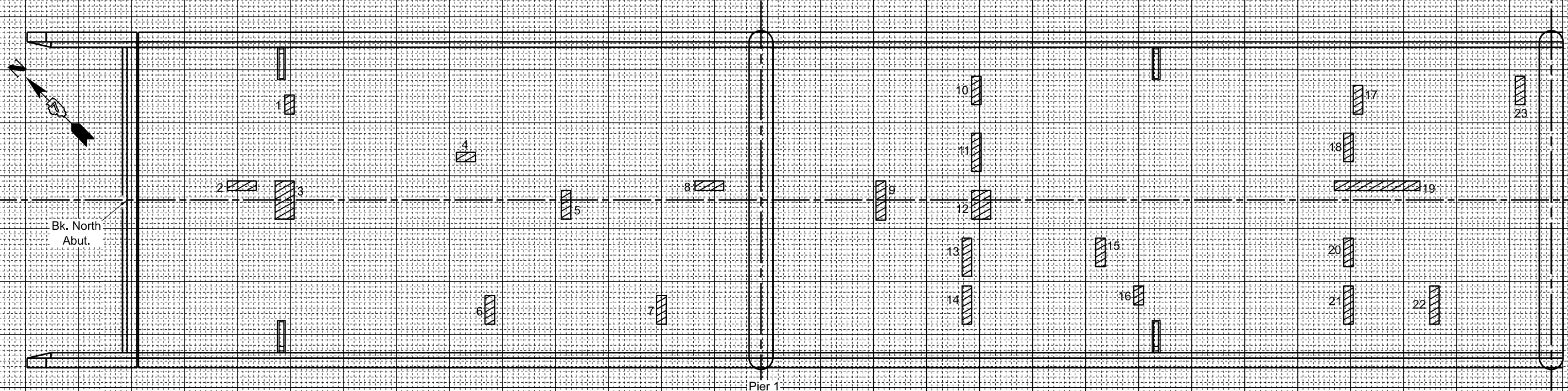
**ADJUSTING EXISTING SCUPPER  
 S.N. 018-0019**

SCALE: SHEET 6 OF 9 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	16
				CONTRACT NO. 74C55
ILLINOIS FED. AID PROJECT				



018-0019 North Half Bridge Deck Patching



PATCH NO.	SIZE		DECK SLAB REPAIR (FD TYPE I) SQ YD	DECK SLAB REPAIR (FD TYPE II) SQ YD
	LENGTH	WIDTH		
1	1.0	2.0	0.2	
2	3.0	1.0	0.3	
3	2.0	4.0		0.9
4	2.0	1.0	0.2	
5	1.0	3.0	0.3	
6	1.0	3.0	0.3	
7	1.0	3.0	0.3	
8	3.0	1.0	0.3	
9	1.0	4.0	0.4	
10	1.0	3.0	0.3	
11	1.0	4.0	0.4	
12	2.0	3.0		0.7
13	1.0	4.0	0.4	
14	1.0	4.0	0.4	
15	1.0	3.0	0.3	
16	1.0	2.0	0.2	
17	1.0	3.0	0.3	
18	1.0	3.0	0.3	

PATCH NO.	SIZE		DECK SLAB REPAIR (FD TYPE I) SQ YD	DECK SLAB REPAIR (FD TYPE II) SQ YD
	LENGTH	WIDTH		
19	9.0	1.0	3.0	1.0
20	1.0	3.0	0.3	
21	1.0	4.0	0.4	
22	1.0	4.0	0.4	
23	1.0	3.0	0.3	
TOTAL ROUNDS TO:			7.0	3.0

THE LOCATIONS AND SIZES SHOWN GRAPHICALLY ABOVE ARE APPROXIMATE. SEE THIS TABLE FOR ACTUAL SIZES.



DATE OF SURVEY: 6-13-23  
 SURVEY BY: DPM & TMW  
 METHOD OF SURVEY: VISUAL

**ESTIMATED PAY QUANTITIES:**  
 DECK SLAB REPAIR (FULL DEPTH TYPE I)  
 7.0 SQ YD  
 DECK SLAB REPAIR (FULL DEPTH TYPE II)  
 3.0 SQ YD

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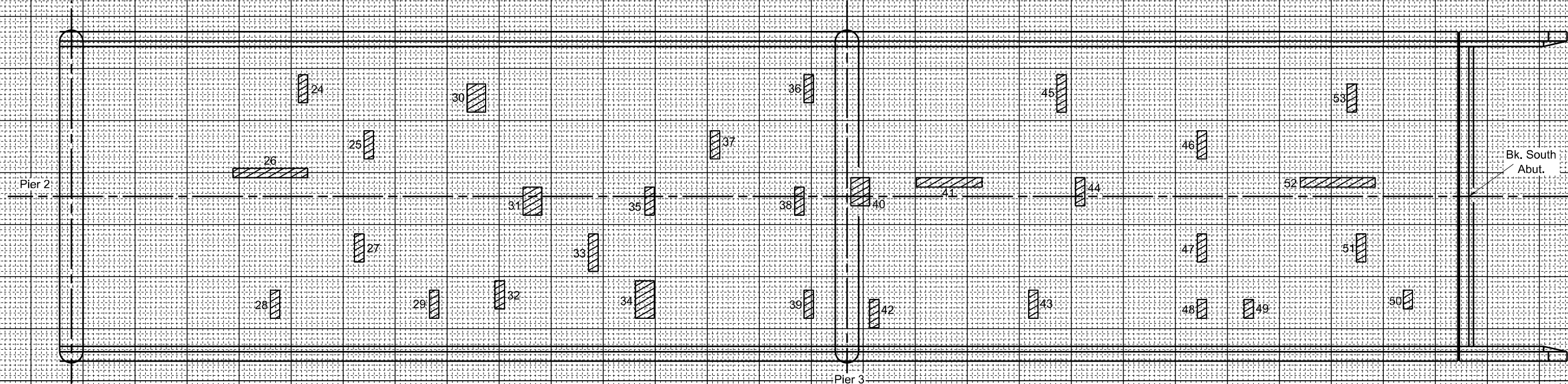
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	DATE - July 2023	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**BRIDGE DECK PATCHING, NORTH HALF**  
**S.N. 018-0019**  
 SCALE: SHEET 7 OF 9 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	17
CONTRACT NO. 74C55				
ILLINOIS FED. AID PROJECT				

018-0019 South Half Bridge Deck Patching



PATCH NO.	SIZE		DECK SLAB REPAIR (FD TYPE I)	DECK SLAB REPAIR (FD TYPE II)
	LENGTH	WIDTH	SQ YD	SQ YD
24	1.0	3.0	0.3	
25	1.0	3.0	0.3	
26	8.0	1.0		0.9
27	1.0	3.0	0.3	
28	1.0	3.0	0.3	
29	1.0	3.0	0.3	
30	2.0	3.0		0.7
31	2.0	3.0		0.7
32	1.0	3.0	0.3	
33	1.0	4.0	0.4	
34	2.0	4.0		0.9
35	1.0	3.0	0.3	
36	1.0	3.0	0.3	
37	1.0	3.0	0.3	
38	1.0	3.0	0.3	
39	1.0	3.0	0.3	
40	2.0	3.0		0.7
41	7.0	1.0		0.8

PATCH NO.	SIZE		DECK SLAB REPAIR (FD TYPE I)	DECK SLAB REPAIR (FD TYPE II)
	LENGTH	WIDTH	SQ YD	SQ YD
42	1.0	3.0	0.3	
43	1.0	3.0	0.3	
44	1.0	3.0	0.3	
45	1.0	4.0	0.4	
46	1.0	3.0	0.3	
47	1.0	3.0	0.3	
48	1.0	2.0	0.2	
49	1.0	2.0	0.2	
50	1.0	2.0	0.2	
51	1.0	3.0	0.3	
52	8.0	1.0		0.9
53	1.0	3.0	0.3	
TOTAL ROUNDS TO:			8.0	6.0

THE LOCATIONS AND SIZES SHOWN GRAPHICALLY ABOVE ARE APPROXIMATE. SEE THIS TABLE FOR ACTUAL SIZES.



DATE OF SURVEY: 6-13-23  
 SURVEY BY: DPM & TMW  
 METHOD OF SURVEY: VISUAL

**ESTIMATED PAY QUANTITIES:**  
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 DECK SLAB REPAIR (FULL DEPTH TYPE II)  
 6.0 SQ YD

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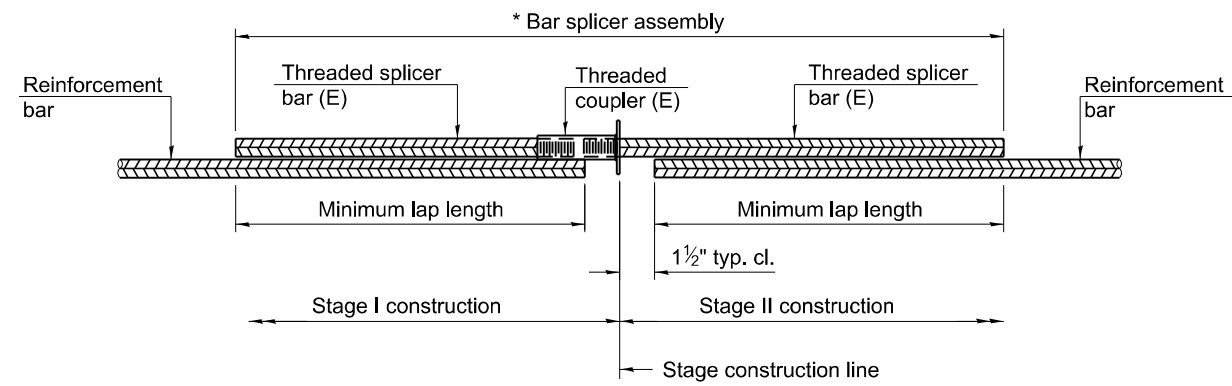
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	DATE - July 2023	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BRIDGE DECK PATCHING, SOUTH HALF  
 S.N. 018-0019

SCALE: SHEET 8 OF 9 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	18
CONTRACT NO. 74C55				
ILLINOIS		FED. AID PROJECT		



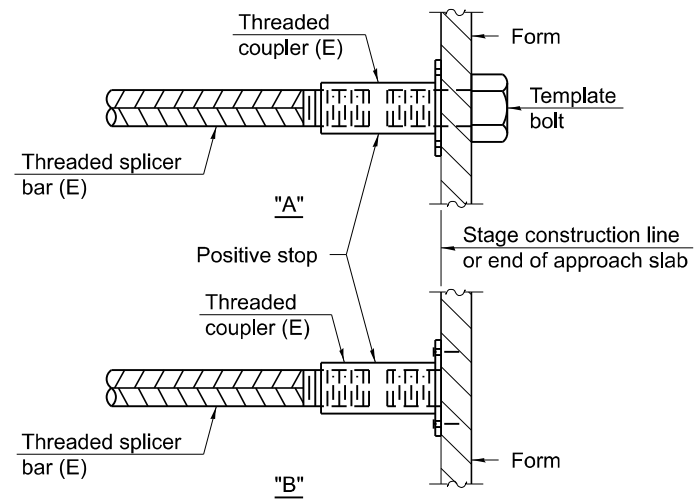
**STANDARD BAR SPLICER ASSEMBLY PLAN**

Only bar splicer assemblies as presented on the approved QPL list may be used.

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar Size	No. assemblies required	Minimum lap length
Deck	#5	28	3'-6"
Abutment	#6	8	4'-0"

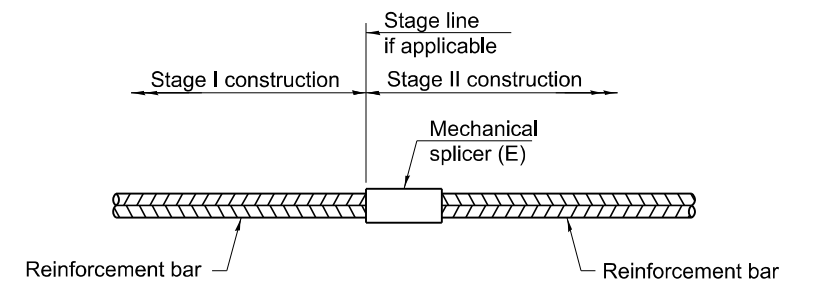


**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar Size	No. assemblies required

**Notes:**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars. Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

5-15-2023

MODEL: Default  
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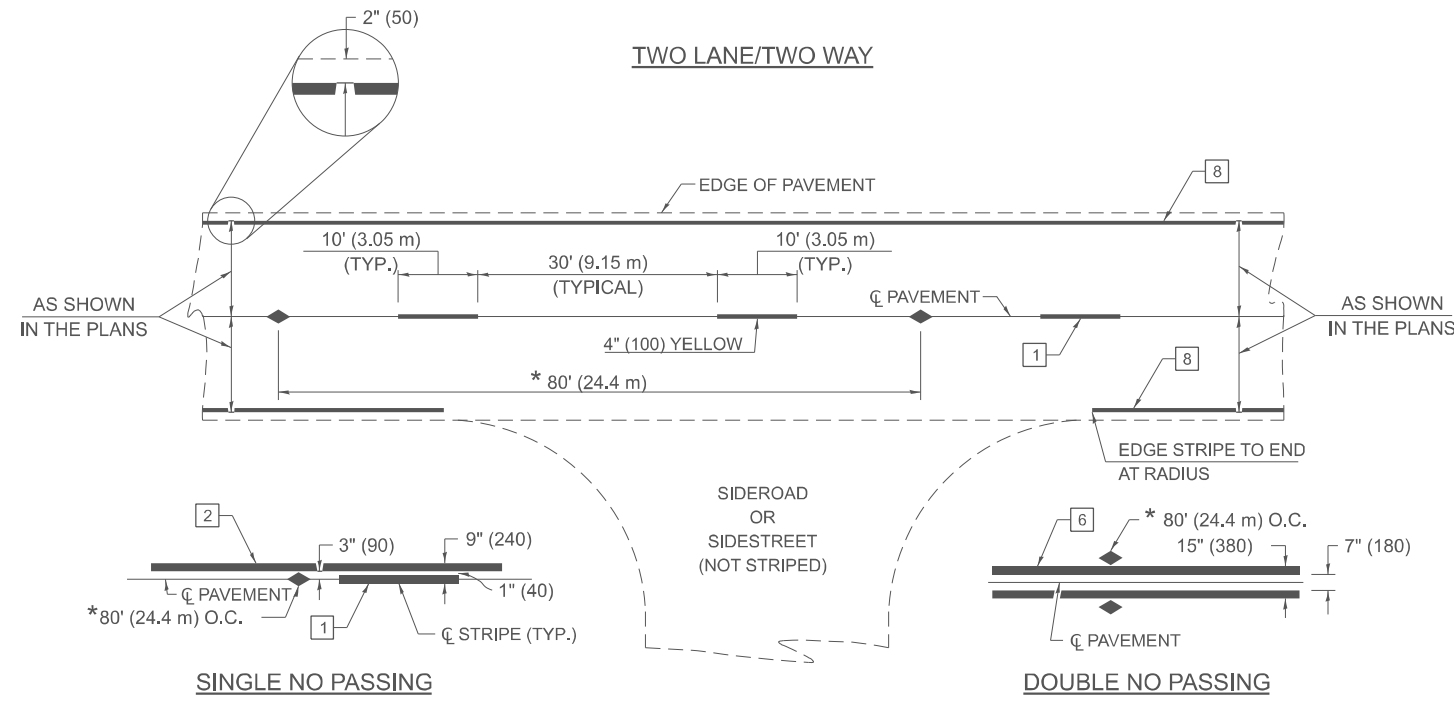
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PLOT DATE = 7/5/2024	DATE - July 2023	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

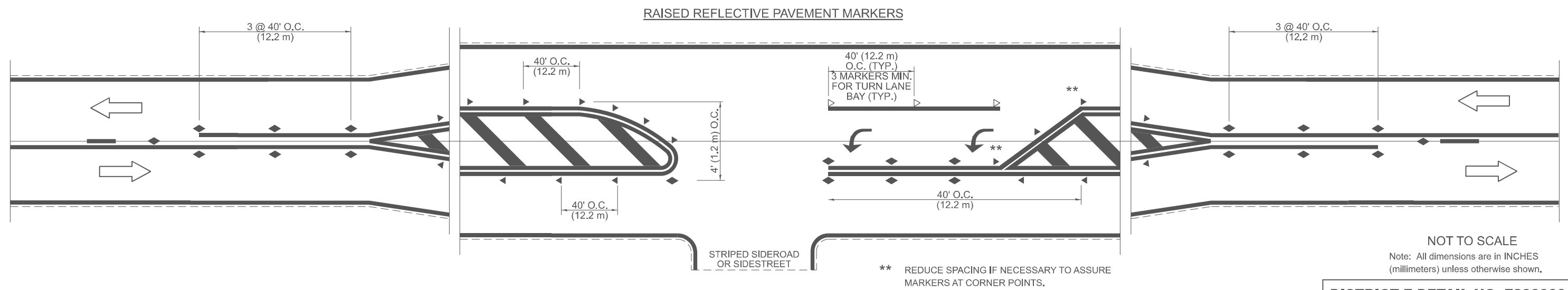
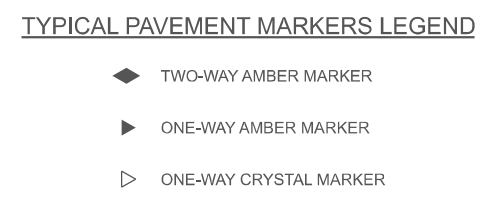
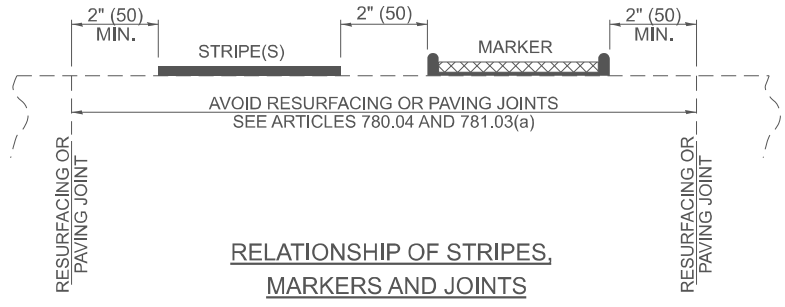
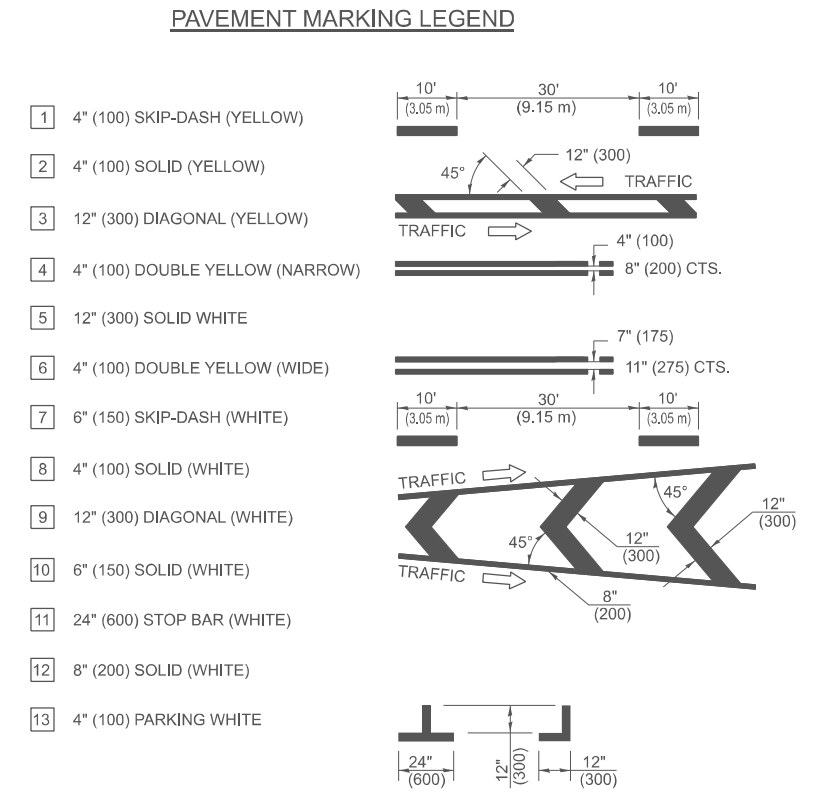
**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
S.N. 018-0019**

SCALE: SHEET 9 OF 9 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	19
			CONTRACT NO. 74C55	
		ILLINOIS	FED. AID PROJECT	



\* REDUCE TO 40' (12.2 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEEDS OF 45 mph (70 km/h) OR LESS.



NOT TO SCALE  
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

MODEL: 02c-78000001 (Sheet)  
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USER NAME = Leah.Hille	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 6/26/2024	DATE -	REVISED -

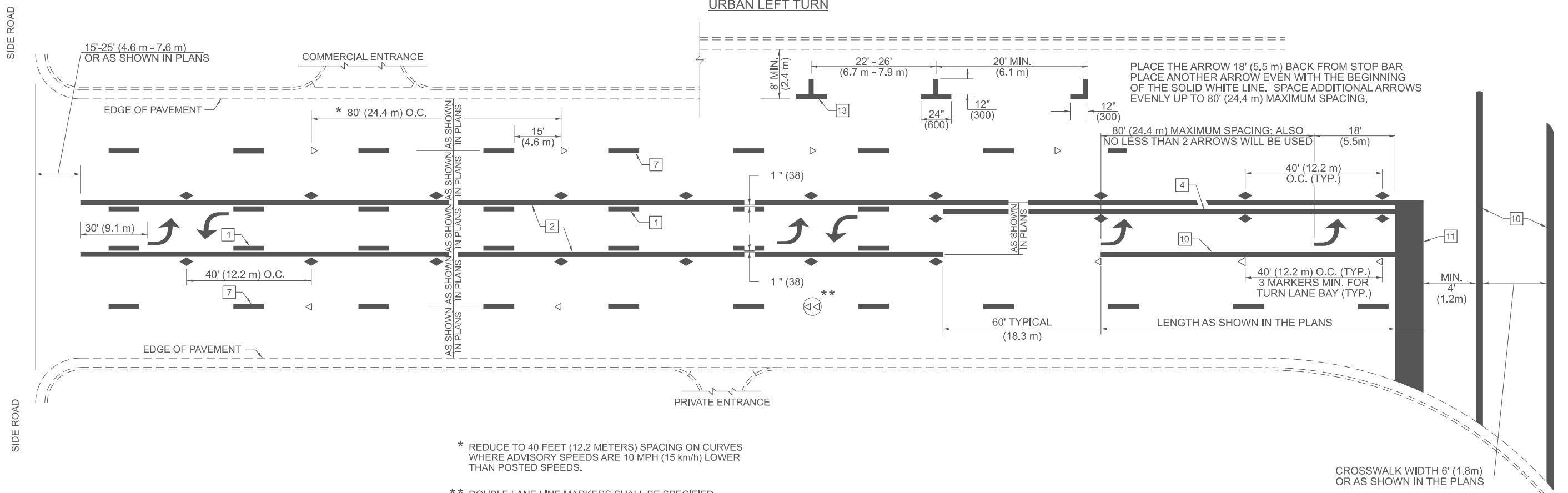
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS  
(RURAL & URBAN APPLICATIONS)

SCALE: SHEET 1 OF 4 SHEETS STA. TO STA.

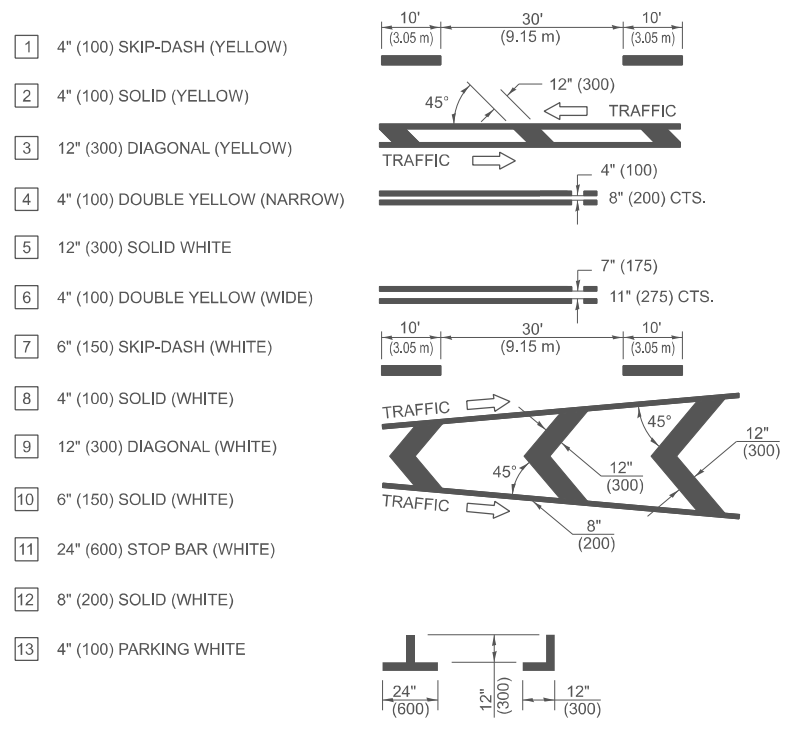
<b>DISTRICT 7 DETAIL NO. 78000001</b>				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	20
CONTRACT NO. 74C55				
ILLINOIS FED. AID PROJECT				
BRIDGE REHABILITATION				

**URBAN LEFT TURN**



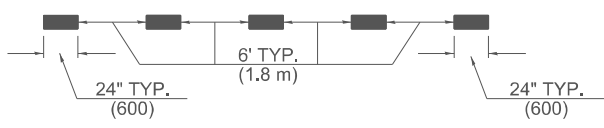
- \* REDUCE TO 40 FEET (12.2 METERS) SPACING ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.
- \*\* DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

**PAVEMENT MARKING LEGEND**

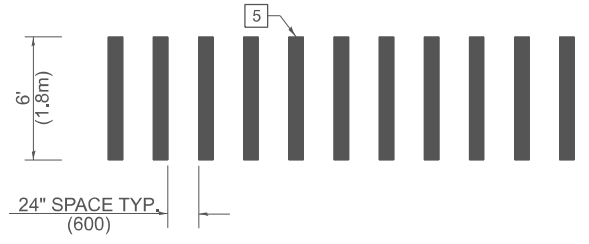


**GENERAL NOTES**

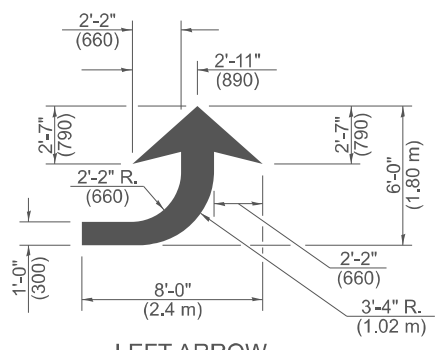
1. TURN ARROW PAIRS SHALL BE PLACED AT 250'(75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE. USE A MINIMUM OF TWO PAIRS PER BLOCK.
2. THE SOLID YELLOW PAVEMENT MARKINGS [2] SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
3. THE SKIP-DASH PAVEMENT MARKINGS [1] OR [7] SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER.
4. USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE SECTION 780 FOR SYMBOLS TABLE)
5. LANE LINE EXTENSIONS SHALL BE THE SAME COLOR AND WIDTH AS THE LANE LINE BEING EXTENDED.
6. ALL WHITE SKIP-DASH LINES SHALL BE 6" IN WIDTH.



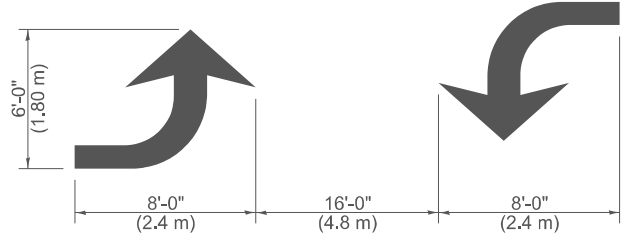
**LANE LINE EXTENSIONS**



**CROSSWALK DETAIL (DECATUR CITY LIMITS ONLY)**



**LEFT ARROW**  
REVERSE FOR RIGHT ARROW  
AREA = 15.6 SQ. FT. (1.47 m<sup>2</sup>) (WHITE)



**TYPICAL DOUBLE TURN ARROWS (WHITE)**

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

**DISTRICT 7 DETAIL NO. 78000001**

MODEL: 021-78000001 (Sheet)  
FILE NAME: c:\p\work\pav\hille\m\088979\774C55-sh1-detaills.dgn

USER NAME = Leah.Hille	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 6/26/2024	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)**

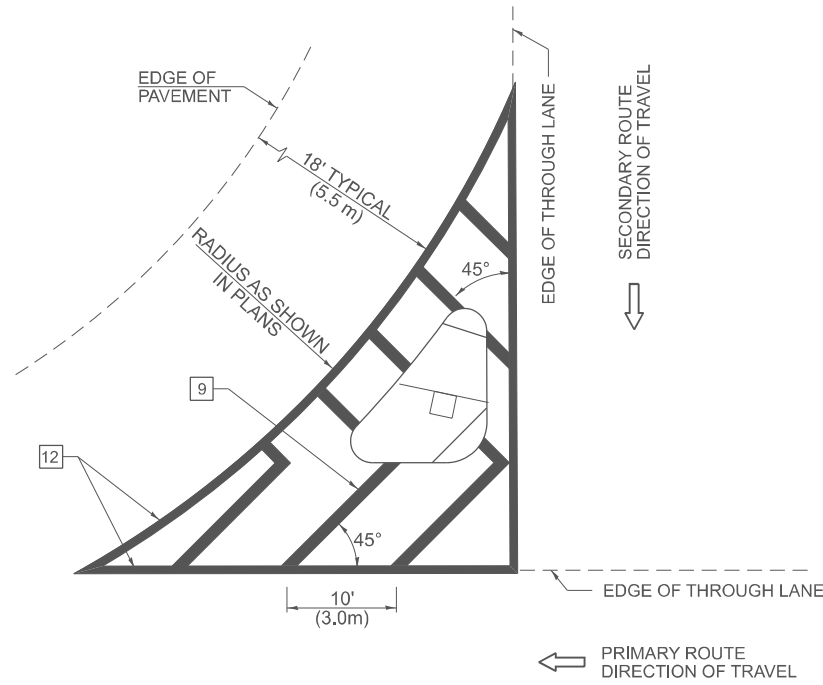
SCALE: SHEET 2 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	21
CONTRACT NO. 74C55				
ILLINOIS FED. AID PROJECT				

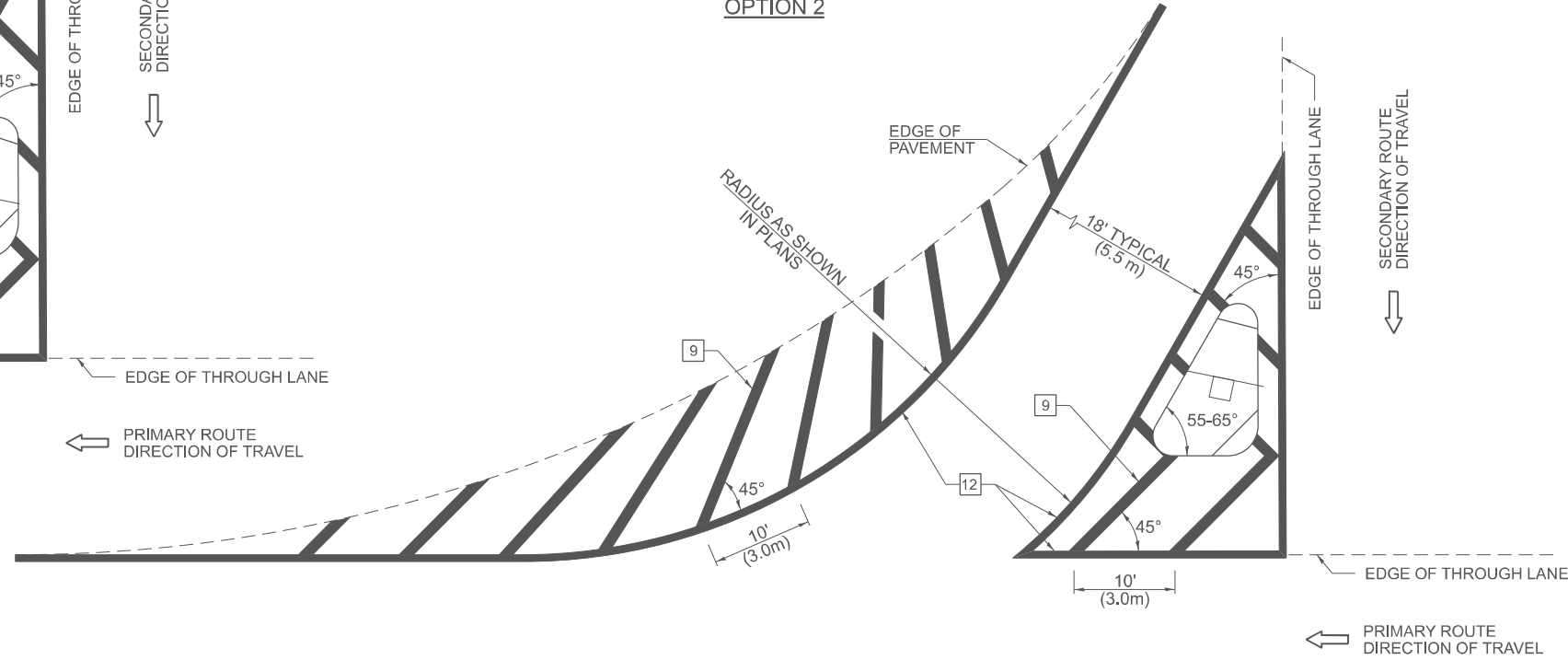
BRIDGE REHABILITATION

**ISLANDS**

**OPTION 1**



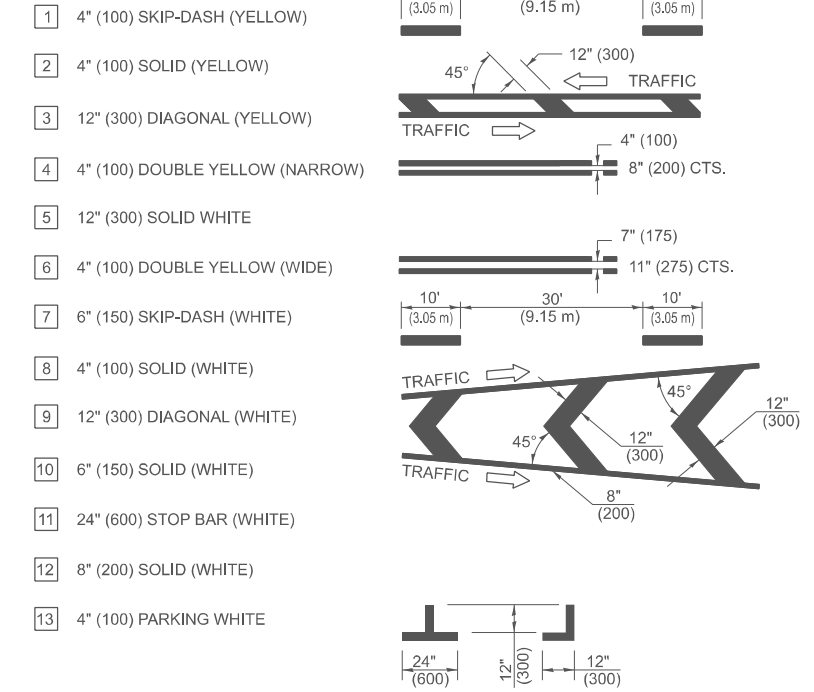
**OPTION 2**



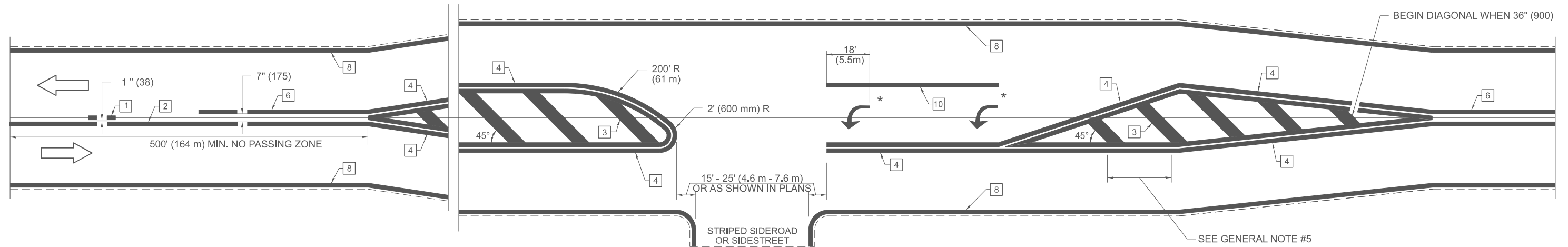
**GENERAL NOTES**

1. RAISED AND CORRUGATED MEDIANS SHALL BE OUTLINED WITH [2].
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
5. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING:
  - <30 MPH (<50 km/h) 15' (4.5 m)
  - 30-45 MPH (50-75 km/h) 20' (6.0 m)
  - >45 MPH (>75 km/h) 30' (9.0 m)
6. THE USE OF ISLAND STRIPING OPTION 1 OR OPTION 2 SHALL BE AS SHOWN ON THE PLANS.

**PAVEMENT MARKING LEGEND**



**RURAL LEFT TURN STRIPING**



\* PLACE AN ARROW 18' (5.5 m) BACK FROM END OF THE SOLID WHITE LINE. PLACE ANOTHER ARROW EVEN WITH THE BEGINNING OF THE SOLID WHITE LINE. SPACE ADDITIONAL ARROWS EVENLY UP TO 80' (24.4 m) MAXIMUM SPACING. USE MINIMUM OF 2 ARROWS.

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

**DISTRICT 7 DETAIL NO. 78000001**

USER NAME = Leah.hille	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 6/26/2024	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS  
(RURAL & URBAN APPLICATIONS)**

SCALE: SHEET 3 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	22
CONTRACT NO. 74C55				

ILLINOIS FED. AID PROJECT  
BRIDGE REHABILITATION

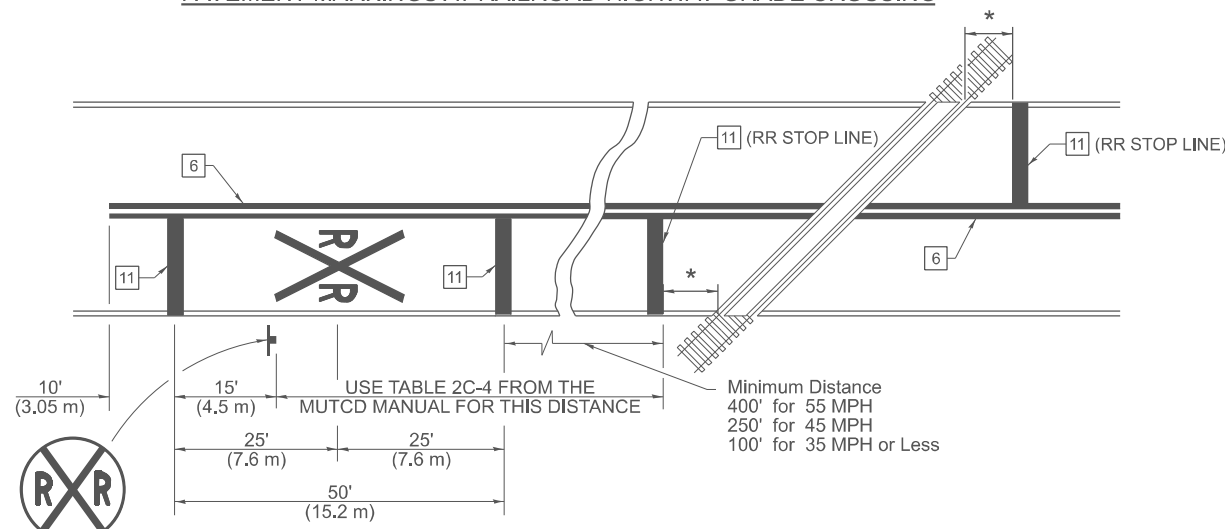
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# SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING

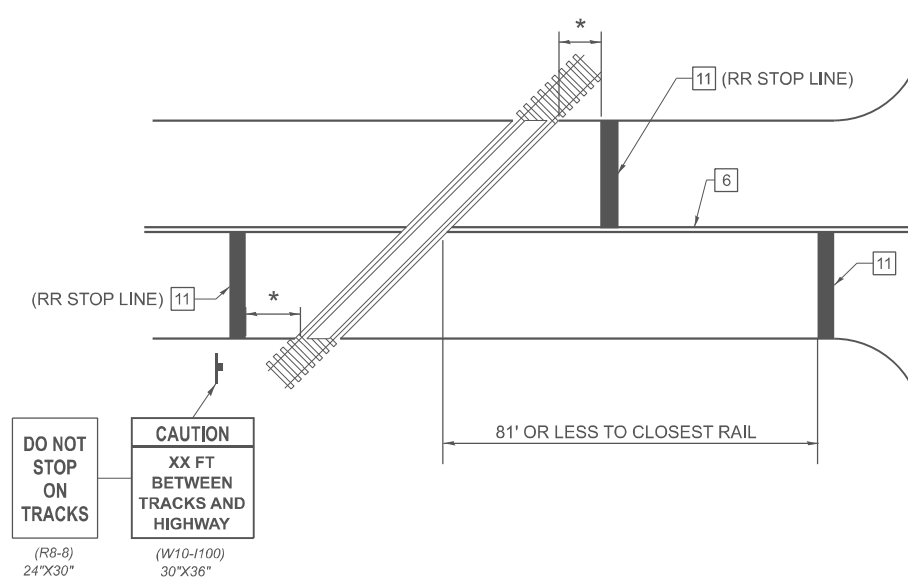
## PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
  - 2 4" (100) SOLID (YELLOW)
  - 3 12" (300) DIAGONAL (YELLOW)
  - 4 4" (100) DOUBLE YELLOW (NARROW)
  - 5 12" (300) SOLID WHITE
  - 6 4" (100) DOUBLE YELLOW (WIDE)
  - 7 6" (150) SKIP-DASH (WHITE)
  - 8 4" (100) SOLID (WHITE)
  - 9 12" (300) DIAGONAL (WHITE)
  - 10 6" (150) SOLID (WHITE)
  - 11 24" (600) STOP BAR (WHITE)
  - 12 8" (200) SOLID (WHITE)
  - 13 4" (100) PARKING WHITE
- 

### PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING



### RAILROAD CROSSING WITH NON-SIGNALIZED INTERSECTION

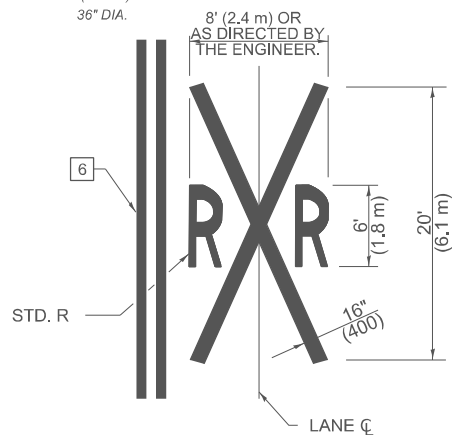


### NOTES

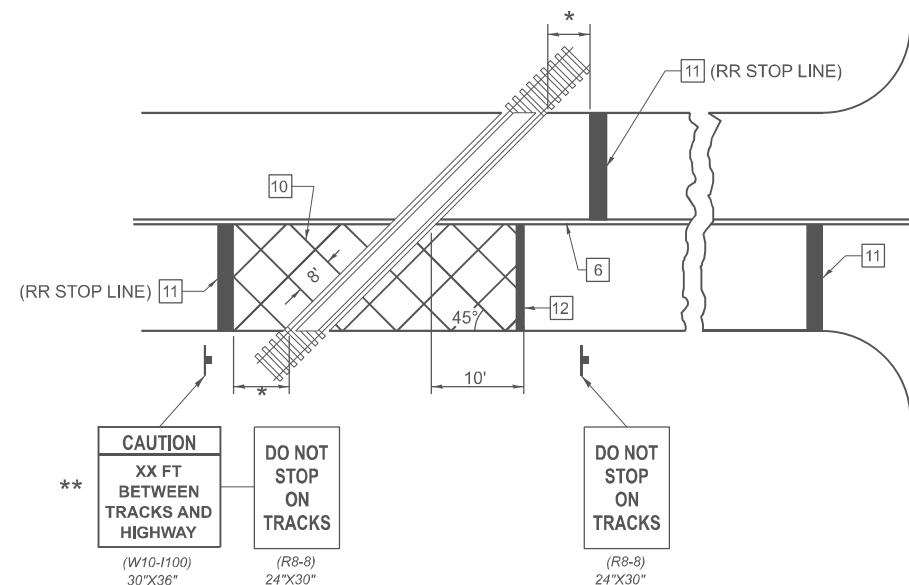
THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

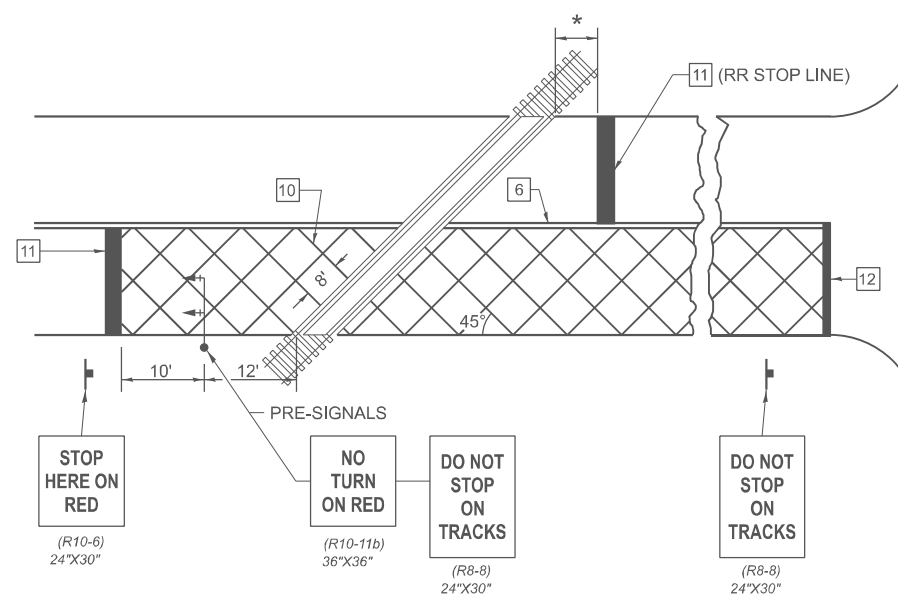
WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.



### RAILROAD CROSSING WITH INTERCONNECT ONLY

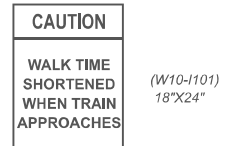


### RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



### GENERAL NOTES

- SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE PRE-SIGNALS ARE USED.
- WHEN PEDESTRIAN SIGNALS ARE PRESENT WITH INTERCONNECTED SIGNALS, WARNING SIGN W10-101 (18"X24") SHALL BE PLACED NEAR EACH PEDESTRIAN SIGNAL HEAD. COUNTDOWN PEDESTRIAN SIGNAL HEADS SHALL NOT BE UTILIZED ALONG WITH INTERCONNECTED SIGNALS.
- PLEASE REFER TO THE IDOT BUREAU OF OPERATION MEMO OPS T-06 DATED DECEMBER 1, 2020 FOR ADDITIONAL INFORMATION.



- \* 15' FROM NEAR RAIL OR 8' FROM AND PARALLEL TO GATE IF PRESENT
- \*\* WARNING SIGN W10-100 SHALL BE USED AS AN INTERIM MEASURE AT INTERCONNECTED SIGNAL LOCATIONS WHERE PRE-SIGNALS ARE TO BE INSTALLED IN THE FUTURE. THIS SIGN SHALL BE REMOVED WHEN THE PRE-SIGNALS ARE INSTALLED AND THE PAVEMENT MARKINGS ARE EXTENDED TO THE INTERSECTION.

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

### DISTRICT 7 DETAIL NO. 78000001

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
773	D7 BRIDGE REPAIRS 2025-6	CUMBERLAND	23	23
CONTRACT NO. 74C55				
ILLINOIS FED. AID PROJECT				

MODEL: 023-78000001 (Sheet)  
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USER NAME = Leah.Hillel	DESIGNED -	REVISED -
	DRAWN -	REVISED -
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PLOT DATE = 6/26/2024	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS  
(RURAL & URBAN APPLICATIONS)

SCALE: SHEET 4 OF 4 SHEETS STA. TO STA.

BRIDGE REHABILITATION