

LETTING ITEM NO. 02A
 SEPTEMBER 20, 2024 LETTING

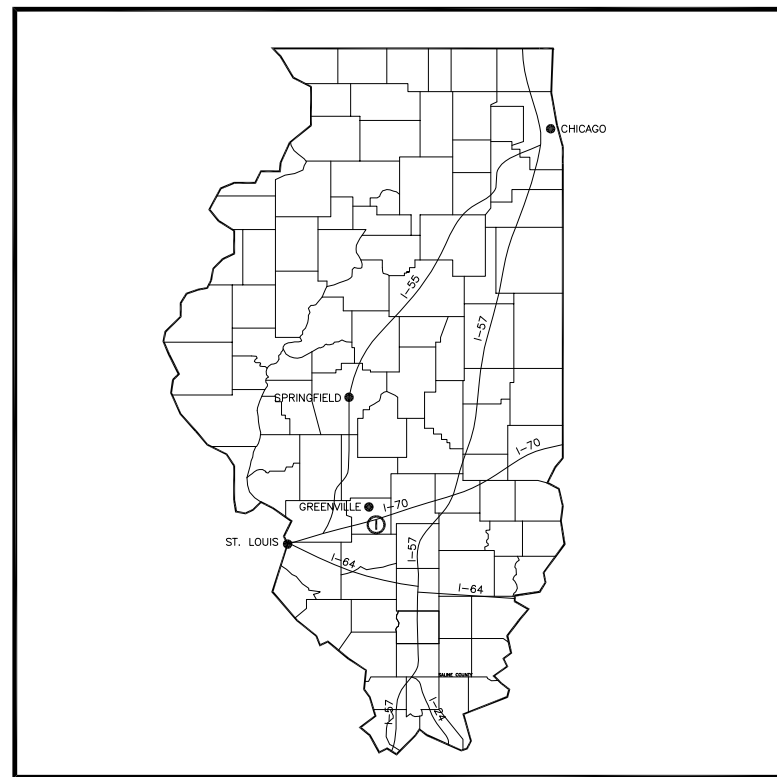
CONSTRUCTION PLANS FOR GREENVILLE AIRPORT AIRCRAFT T-HANGAR APRON EXPANSION

GR013
 TOTAL SHEETS: 12

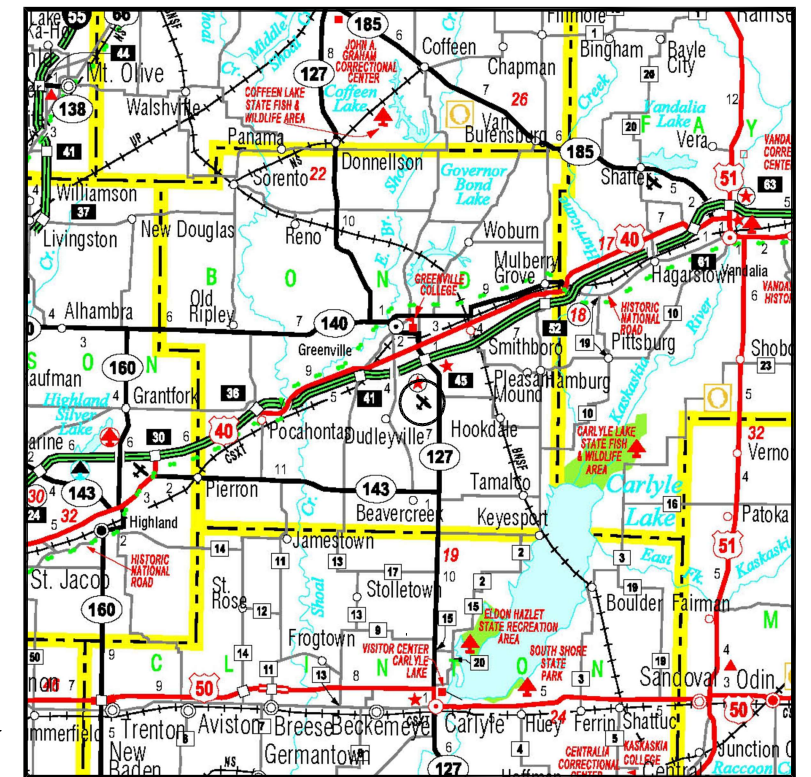
ILLINOIS PROJECT NUMBER: GRE-4992
 REBUILD ILLINOIS AIRPORT CAPITAL
 INVESTMENT PROGRAM

100% SUBMITTAL - JULY 26, 2024

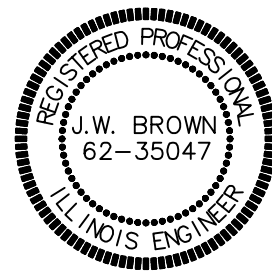
GREENVILLE, ILLINOIS
 BOND COUNTY



LOCATION MAP



VICINITY MAP



DESIGN INFORMATION

- CRITICAL AIRCRAFT = CESSNA CITATION III
- AIRCRAFT APPROACH CATEGORY (AAC) = B
- AIRPLANE DESIGN GROUP (ADG) = II
- TAXIWAY DESIGN GROUP (TDG) = 2
- DEPARTURE WEIGHT = 22,000 LBS.

BROWN AND ROBERTS, INC.
 CONSULTING ENGINEER
 PRESIDENT

SUBMITTED BY: *Jim W. Brown*
 JIM W. BROWN

DATE SUBMITTED: 7/24/2024

LICENSE NUMBER: 184-002518

LICENSE EXPIRATION DATE: APRIL 30, 2025

PLANS PREPARED BY:



BROWN AND ROBERTS, INC.
 1 WESTRIDGE ROAD
 HARRISBURG, IL. 62946
 (618) 252-8111

GREENVILLE AIRPORT AUTHORITY
 CHAIRMAN

APPROVED BY: *Randy Vaad* 7-24-24
 DATE

SECRETARY

ATTESTED BY: *Dan Jackson* 7-25-24
 DATE

29 Jul 2024 - 8:17am X:\2023\23052\acplans\23052 Cover and Quantity Sheets.dwg: Layout Tab 'Cover'

SUMMARY OF QUANTITIES			
ITEM NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY
AR150520	MOBILIZATION	L SUM	1
AR152410	UNCLASSIFIED EXCAVATION	CU YD	1270
AR152511	SUBGRADE REPAIR	SQ YD	111
AR156510	SILT FENCE	FOOT	260
AR156514	DITCH CHECK	FOOT	14
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	715
AR401613	BIT. SURF. CSE. - METHOD I, SUPERPAVE	TON	217
AR401650	BITUMINOUS PAVEMENT MILLING	SQ YD	184
AR403613	BIT. BASE CSE. - METHOD I, SUPERPAVE	TON	222
AR401900	REMOVE BITUMINOUS PAVEMENT	SQ YD	205
AR501506	6" PCC PAVEMENT	SQ YD	371
AR501530	PCC TEST BATCH	EACH	1
AR602510	BITUMINOUS PRIME COAT	GAL	620
AR603510	BITUMINOUS TACK COAT	GAL	370
AR701312	12" RCP, CLASS II	FOOT	28
AR705943	ADJUST UNDERDRAIN INSP. HOLE	EACH	1
AR751416	TYPE 1 INLET	EACH	1
AR751417	TYPE 2 INLET	EACH	1
AR901525	SEEDING	L SUM	1

INDEX OF SHEETS	
SHEET NUMBER	DESCRIPTION
1	COVER SHEET
2	SUMMARY OF QUANTITIES & INDEX OF SHEETS
3	CONSTRUCTION SAFETY & PHASING PLAN
4	TYPICAL SECTIONS
5	REMOVAL PLAN
6	APRON PAVING PLAN
7	JOINT DETAILS
8	APRON CROSS SECTIONS 1
9	APRON CROSS SECTIONS 2
10	APRON CROSS SECTIONS 3
11	TYPE 1 INLET DETAILS
12	TYPE 2 INLET DETAILS

SCOPE OF WORK

THE PROJECT SCOPE CONSISTS OF THE EXPANSION OF THE EXISTING T-HANGAR APRON AREA.

PROPOSED SAFETY PLAN

GENERAL- THE GREENVILLE AIRPORT CURRENTLY HAS A TURF RUNWAY 9-27 WHICH IS 2822 FT. x 250 FT. AND A PAVED RUNWAY 18-36 WHICH IS 4001 FT. x 75 FT.

IT IS ANTICIPATED THAT NO CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS PROJECT WILL IMPACT ANY AIRCRAFT OPERATIONS ON THE AIRPORT RUNWAYS.

ANY WORK WITHIN 125' OF THE CENTERLINE OF A RUNWAY WILL REQUIRE CLOSURE OF THAT RUNWAY.

THE CONTRACTOR SHALL SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) CONFIRMING COMPLIANCE WITH THE CONSTRUCTION SAFETY PHASING PLAN (CSPP) PRIOR TO THE ISSUANCE OF THE NOTICE TO PROCEED AS SPECIFIED IN FAA AC 150/5370-2.

CONTRACTOR'S RESPONSIBILITIES

IDENTIFICATION- THE CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE PROPERLY MARKED WITH 3-FOOT SQUARE INTERNATIONAL ORANGE AND WHITE CHECKERED FLAGS ANYTIME THEY ARE ON AIRPORT PROPERTY.

THE CONTRACTOR AND HIS EMPLOYEES SHALL BE RESTRICTED TO THE WORK AREA.

EQUIPMENT PARKING AND STORAGE- THE CONTRACTOR'S EQUIPMENT PARKING, STORAGE, AND EMPLOYEE PARKING WILL BE AT THE LOCATION SHOWN ON THIS SHEET. ONLY CONTRACTOR VEHICLES AND EQUIPMENT REQUIRED FOR CONSTRUCTION WILL BE ALLOWED OUTSIDE THIS AREA.

BARRICADES AND TRAFFIC CONES- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND TRAFFIC CONES AS REQUIRED AND AS DIRECTED BY THE RESIDENT ENGINEER. BARRICADES, THEIR MAINTENANCE, PLACEMENT, AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE CONTRACTOR WILL BE RESPONSIBLE FOR REPAIRING ANY DAMAGE TO EXISTING PAVEMENTS CAUSED BY HIS PERSONNEL OR EQUIPMENT.

ACCESS/HAUL ROUTE AND EQUIPMENT PARKING

THE CONTRACTOR WILL USE THE DESIGNATED ACCESS/HAUL ROUTE AND EQUIPMENT PARKING AREA SHOWN ON THIS SAFETY PLAN. THE PROPOSED EQUIPMENT PARKING AREA WILL BE APPROXIMATELY 70-FT BY 150-FT. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED ACCESS/HAUL ROUTE AND PARKING AREA THROUGHOUT THE COURSE OF THE PROJECT. AT THE CONCLUSION OF THE PROJECT, ALL AREAS DISTURBED WILL BE RESTORED AS NEEDED TO ITS ORIGINAL STATE. RESTORATION OF THE ACCESS/HAUL ROUTE AND EQUIPMENT PARKING AREA WILL BE CONSIDERED INCLUDED WITH THE AR150540 HAUL ROUTE PAY ITEM AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

UTILITY NOTE

THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING ALL UTILITY COMPANIES AND ORGANIZATIONS THAT HAVE LINES OR CONDUITS IN THE PROPOSED WORK AREA. ALL LINES AND CONDUITS SHALL BE LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. THE CONTRACTOR SHALL CALL JULIE (1-800-892-0123) TO ACCOMPLISH THESE REQUIREMENTS. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING ALL NON-JULIE UTILITIES LOCATED WITHIN THE PROPOSED CONSTRUCTION LIMITS. THESE UTILITIES ARE TO BE LOCATED PRIOR TO THE START OF CONSTRUCTION.

J.U.L.I.E. INFORMATION

COUNTY.....BOND
 CITY.....GREENVILLE (4 MI SOUTHEAST)
 TOWNSHIP.....CENTRAL
 SECTION NO.....36
 NEAREST MAJOR ROAD INTERSECTION...ILLINOIS RT. 127 & SKY LANE
 AIRPORT ADDRESS...GREENVILLE AIRPORT
 1374 SKY LANE
 GREENVILLE, IL 62246

THE AIRPORT MANAGER WILL BE RESPONSIBLE FOR RELOCATING AIRCRAFT FROM HANGARS WITHIN THE BARRICADED AREA TO THE AIRCRAFT PARKING APRON PRIOR TO PLACEMENT OF THE BARRICADES AND PRIOR TO CONSTRUCTION OPERATIONS.

HEIGHT OF CONSTRUCTION EQUIPMENT

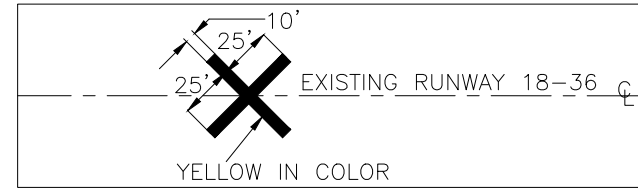
THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT IS 20 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A SEMI TRAILER IN THE UP POSITION.

AIRPORT SECURITY

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE PROPOSED HAUL ROUTE SHOWN ON THIS SAFETY PLAN IS THE ONLY ACCESS CONTRACTOR EQUIPMENT AND PERSONNEL WILL BE ALLOWED TO USE. THE CONTRACTOR SHALL PROVIDE BARRICADES AT THIS ACCESS AND ENSURE THE BARRICADES ARE IN PLACE AT THE END OF EACH WORKING DAY.

AIRCRAFT OPERATIONAL AREA

THE CONTRACTOR, HIS EMPLOYEES, OR ANY EQUIPMENT WILL NOT PROCEED WITH ANY WORK WITHIN THE AIRCRAFT OPERATIONAL AREA WITHOUT FIRST CLOSING THE RUNWAY.



NOTE:

THE COST OF CONSTRUCTING, PLACING, MAINTAINING, AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE RESIDENT ENGINEER. THE CROSSES WILL BE PLACED AT THE ENDS OF THE RUNWAY AND SECURED IN A MANNER APPROVED BY THE RESIDENT ENGINEER. THE PROPOSED CROSSES WILL BE PLACED WHEN THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES AT NO ADDITIONAL COST TO THE CONTRACT.

RUNWAY CLOSURE PROCEDURES:

- * CONTACT THE AIRPORT MANAGER OR HIS ASSIGNED REPRESENTATIVE.
- * ISSUANCE OF NOTAM BY THE AIRPORT MANAGER OR HIS ASSIGNED REPRESENTATIVE.
- * PLACEMENT OF CROSSES (SEE DETAIL THIS SHEET).
- * PLACEMENT OF LIGHTED BARRICADES. ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS WITHIN 200-FT OF THE AFFECTED RUNWAY CENTERLINE AND WITHIN 600 FT OF THE RUNWAY END BEGIN.
- * RUNWAY LIGHTS SHALL BE DISABLED

RUNWAY RE-OPENING PROCEDURES:

- * REMOVE CROSSES.
- * REMOVE LIGHTED BARRICADES.
- * NOTIFY THE AIRPORT MANAGER OR HIS REPRESENTATIVE TO CANCEL THE NOTAM.
- * CANCELLATION OF THE NOTAM. A CLOSED RUNWAY WILL NOT BE RE-OPENED UNTIL ALL EQUIPMENT AND WORK ARE FURTHER THAN 200 FEET FROM THE AFFECTED RUNWAY CENTERLINE.
- * RUNWAY LIGHTS SHALL BE REACTIVATED.

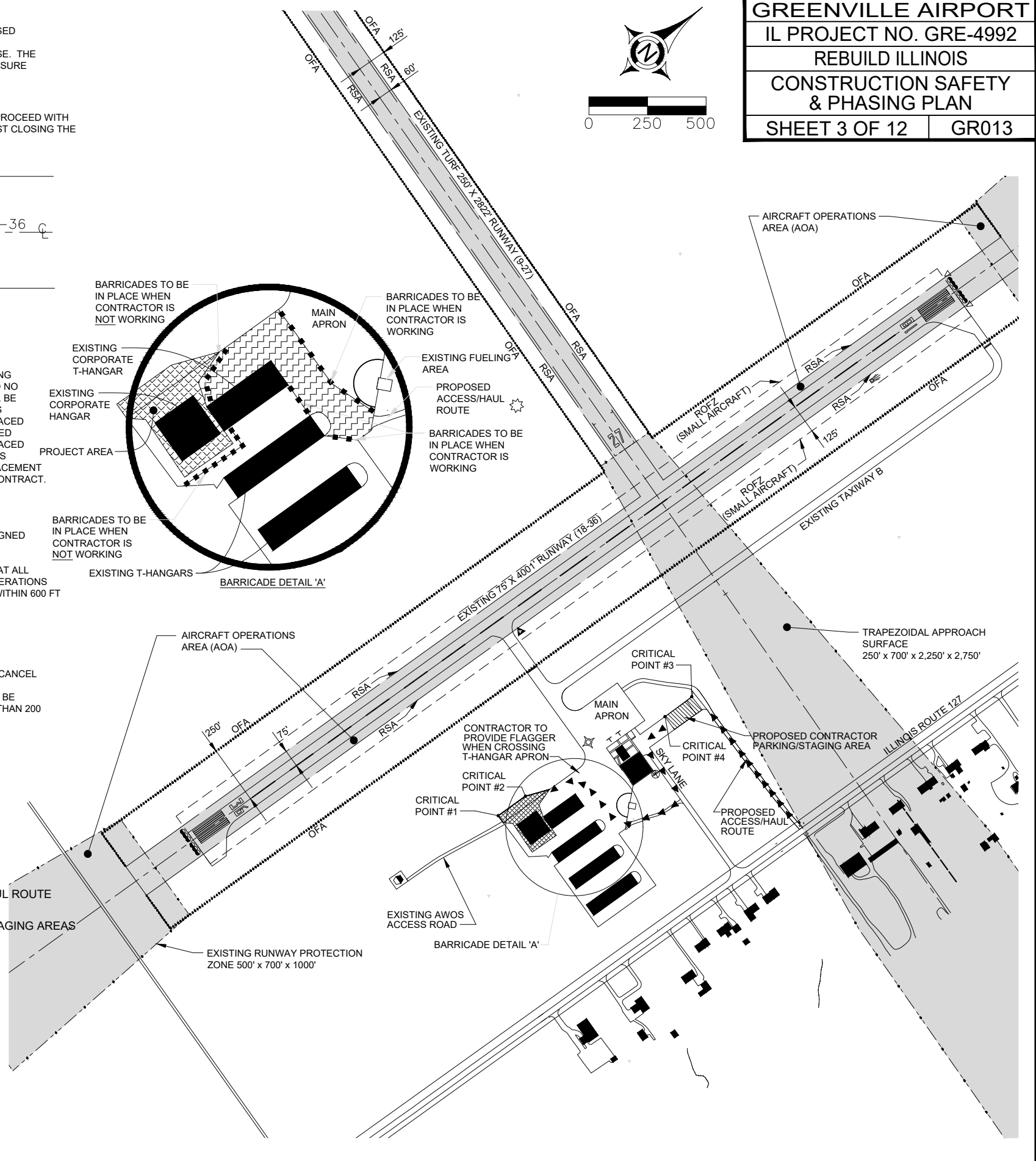
LEGEND

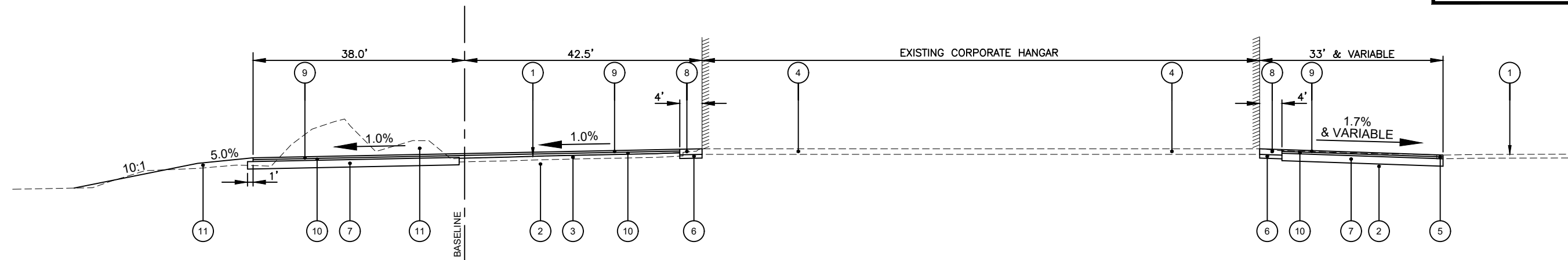
- EXISTING PAVEMENT
- PROPOSED PAVEMENT IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED CONTRACTOR ACCESS AND HAUL ROUTE
- PROPOSED CONTRACTOR PARKING AND STAGING AREAS
- AIRCRAFT OPERATIONS AREA (AOA)
- PROPOSED CONTRACTOR STAGING AREA FOR HAUL TRUCKS

CRITICAL POINTS

NO.	LATITUDE	LONGITUDE	GROUND ELEVATION	DESCRIPTION
1	38° 49' 59.76" N	89° 22' 26.55" W	536.0' MSL	PROPOSED SW CORNER APRON
2	38° 50' 01.27" N	89° 22' 26.55" W	536.0' MSL	PROPOSED NW CORNER APRON
3	38° 50' 09.34" N	89° 22' 25.68" W	537.0' MSL	PROPOSED NW CORNER STAGING AREA
4	38° 50' 07.86" N	89° 22' 25.67" W	537.0' MSL	PROPOSED SW CORNER STAGING AREA

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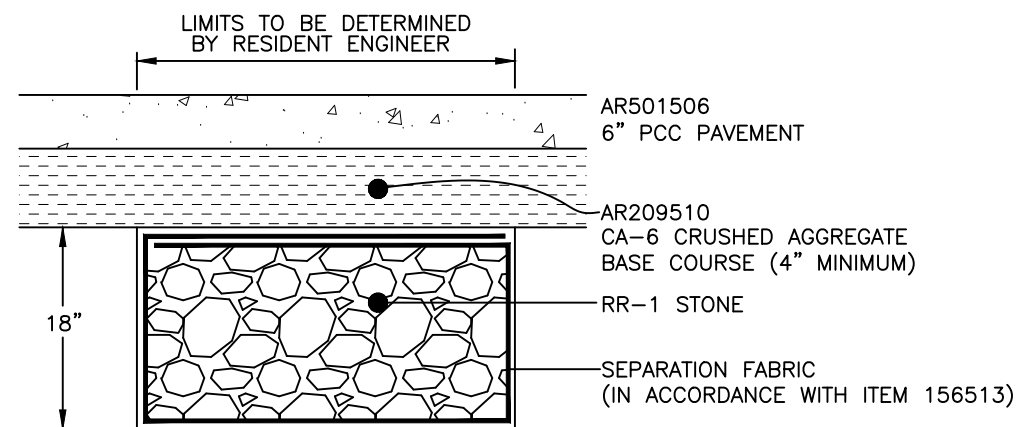




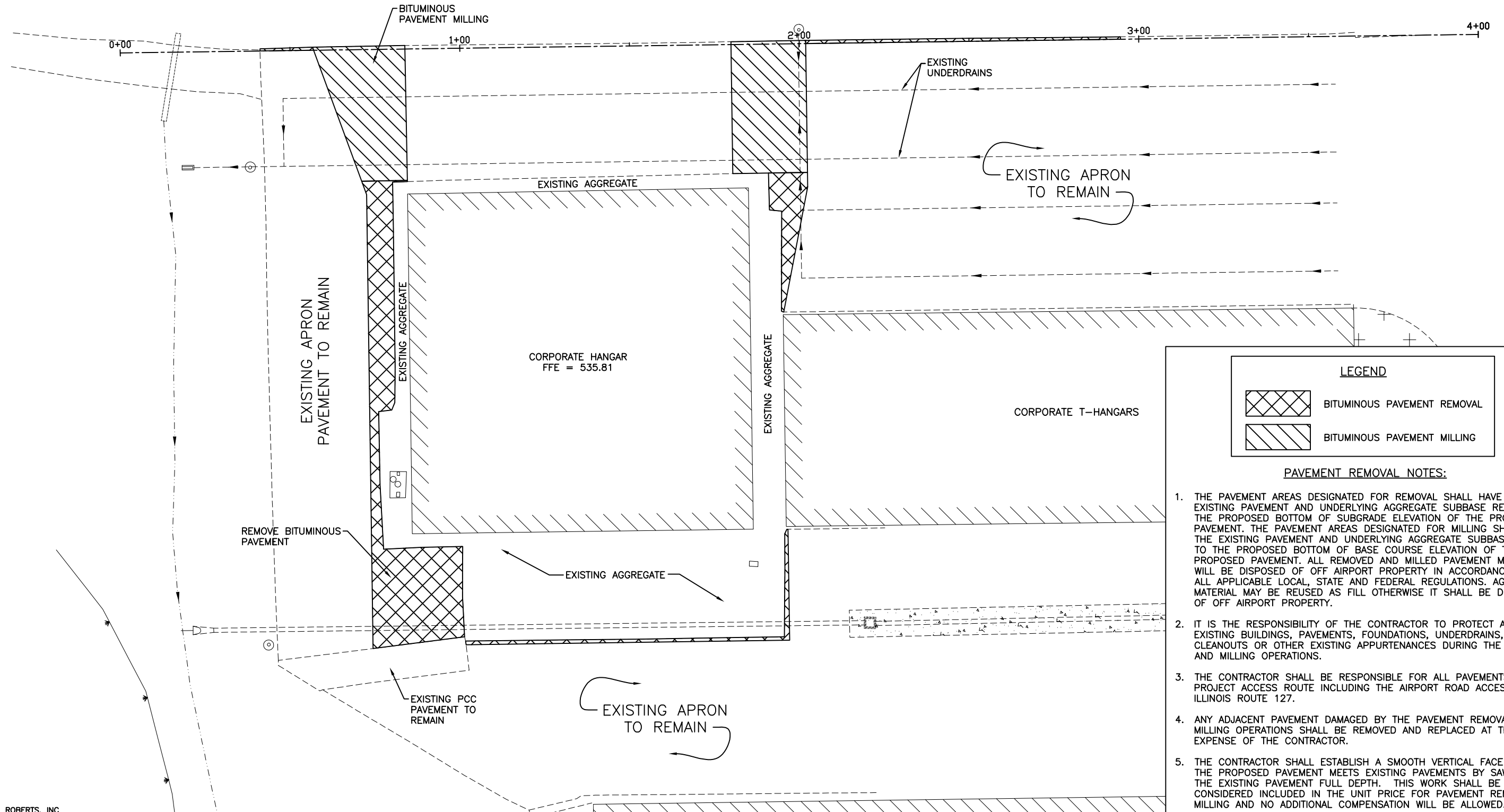
TYPICAL SECTION STATION 1+00 (WEST - EAST)
 NO SCALE

LEGEND

- | | |
|---|---|
| ① EXISTING HMA SURFACE | ⑦ AR209510 - CRUSHED AGGREGATE BASE COURSE (8" DEPTH) |
| ② EXISTING AGGREGATE BASE TO REMAIN | ⑧ AR501506 - 6" PCC PAVEMENT |
| ③ EXISTING HMA PAVEMENT TO REMAIN | ⑨ AR401613 - BITUMINOUS SURFACE COURSE (2" DEPTH) |
| ④ EXISTING PCC PAVEMENT TO REMAIN | ⑩ AR403613 - BITUMINOUS BASE COURSE (2" DEPTH AND VARIES) |
| ⑤ AR401900 - REMOVE BITUMINOUS PAVEMENT | ⑪ AR152410 - UNCLASSIFIED EXCAVATION |
| ⑥ AR209510 - CRUSHED AGGREGATE BASE COURSE (4" DEPTH) | |



SUBGRADE REPAIR DETAIL
 NO SCALE

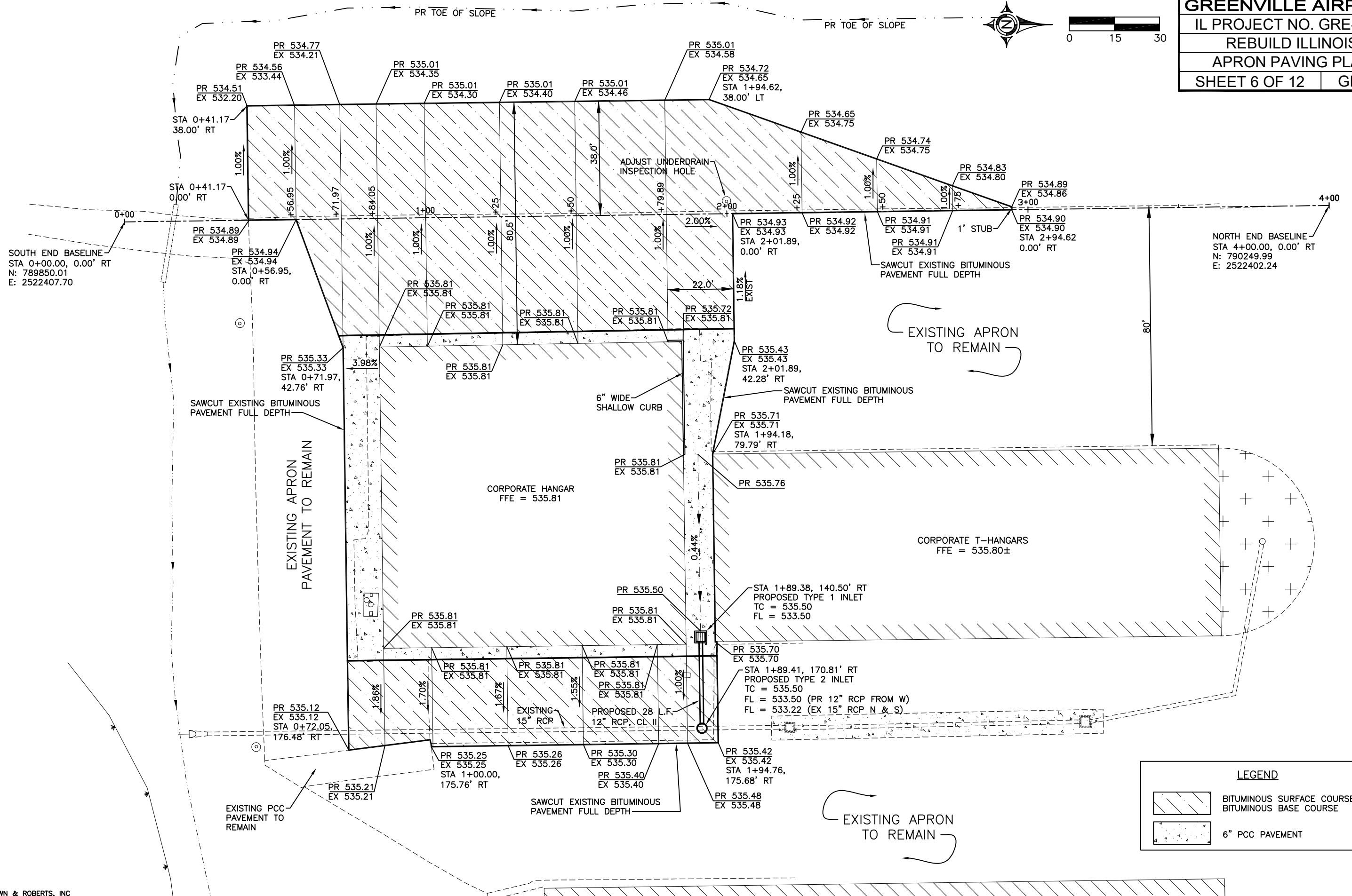
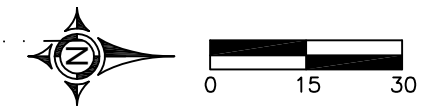


LEGEND	
	BITUMINOUS PAVEMENT REMOVAL
	BITUMINOUS PAVEMENT MILLING

PAVEMENT REMOVAL NOTES:

1. THE PAVEMENT AREAS DESIGNATED FOR REMOVAL SHALL HAVE THE EXISTING PAVEMENT AND UNDERLYING AGGREGATE SUBBASE REMOVED TO THE PROPOSED BOTTOM OF SUBGRADE ELEVATION OF THE PROPOSED PAVEMENT. THE PAVEMENT AREAS DESIGNATED FOR MILLING SHALL HAVE THE EXISTING PAVEMENT AND UNDERLYING AGGREGATE SUBBASE MILLED TO THE PROPOSED BOTTOM OF BASE COURSE ELEVATION OF THE PROPOSED PAVEMENT. ALL REMOVED AND MILLED PAVEMENT MATERIAL WILL BE DISPOSED OF OFF AIRPORT PROPERTY IN ACCORDANCE WITH ALL APPLICABLE LOCAL, STATE AND FEDERAL REGULATIONS. AGGREGATE MATERIAL MAY BE REUSED AS FILL OTHERWISE IT SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
2. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL EXISTING BUILDINGS, PAVEMENTS, FOUNDATIONS, UNDERDRAINS, CLEANOUTS OR OTHER EXISTING APPURTENANCES DURING THE REMOVAL AND MILLING OPERATIONS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PAVEMENTS ON THE PROJECT ACCESS ROUTE INCLUDING THE AIRPORT ROAD ACCESS FROM ILLINOIS ROUTE 127.
4. ANY ADJACENT PAVEMENT DAMAGED BY THE PAVEMENT REMOVAL AND MILLING OPERATIONS SHALL BE REMOVED AND REPLACED AT THE EXPENSE OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL ESTABLISH A SMOOTH VERTICAL FACE WHERE THE PROPOSED PAVEMENT MEETS EXISTING PAVEMENTS BY SAW CUTTING THE EXISTING PAVEMENT FULL DEPTH. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE FOR PAVEMENT REMOVAL OR MILLING AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

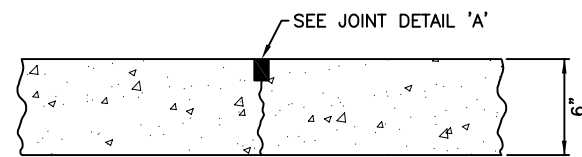
GREENVILLE AIRPORT
IL PROJECT NO. GRE-4992
REBUILD ILLINOIS
APRON PAVING PLAN
SHEET 6 OF 12 | GR013



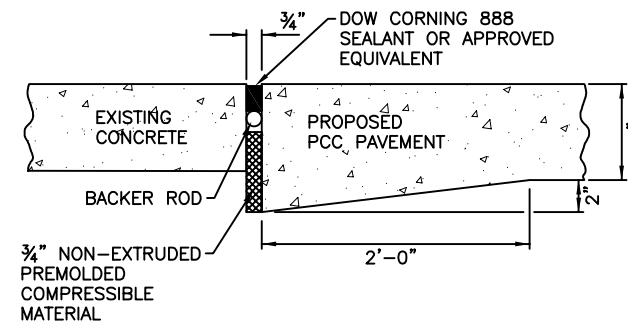
LEGEND

	BITUMINOUS SURFACE COURSE BITUMINOUS BASE COURSE
	6" PCC PAVEMENT

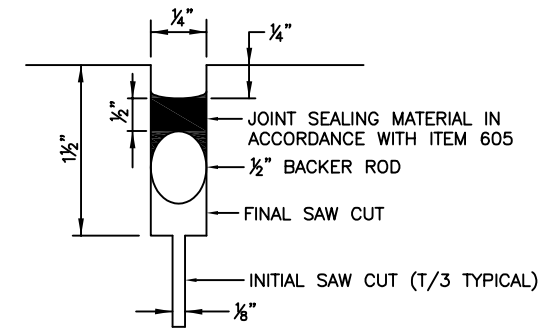
20 Jun 2024 - 3:31pm X:\2023\23052\ac\plans\23052 Base.dwg: Layout Tab '06 Apron Paving Plan'



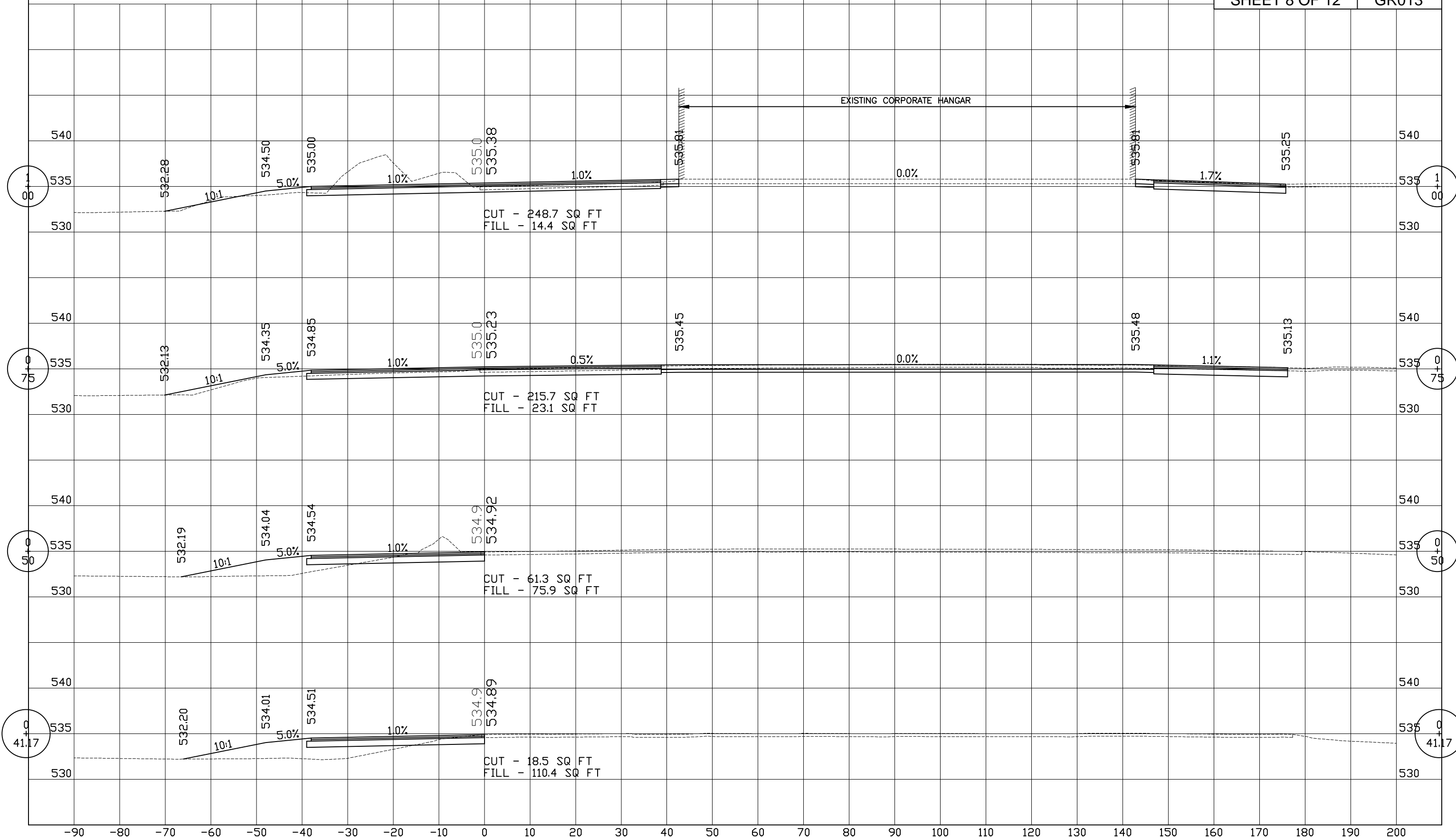
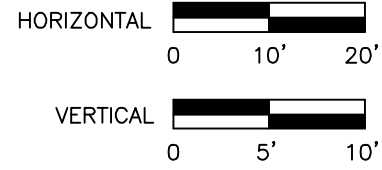
TYPE D DUMMY
CONTRACTION JOINTS

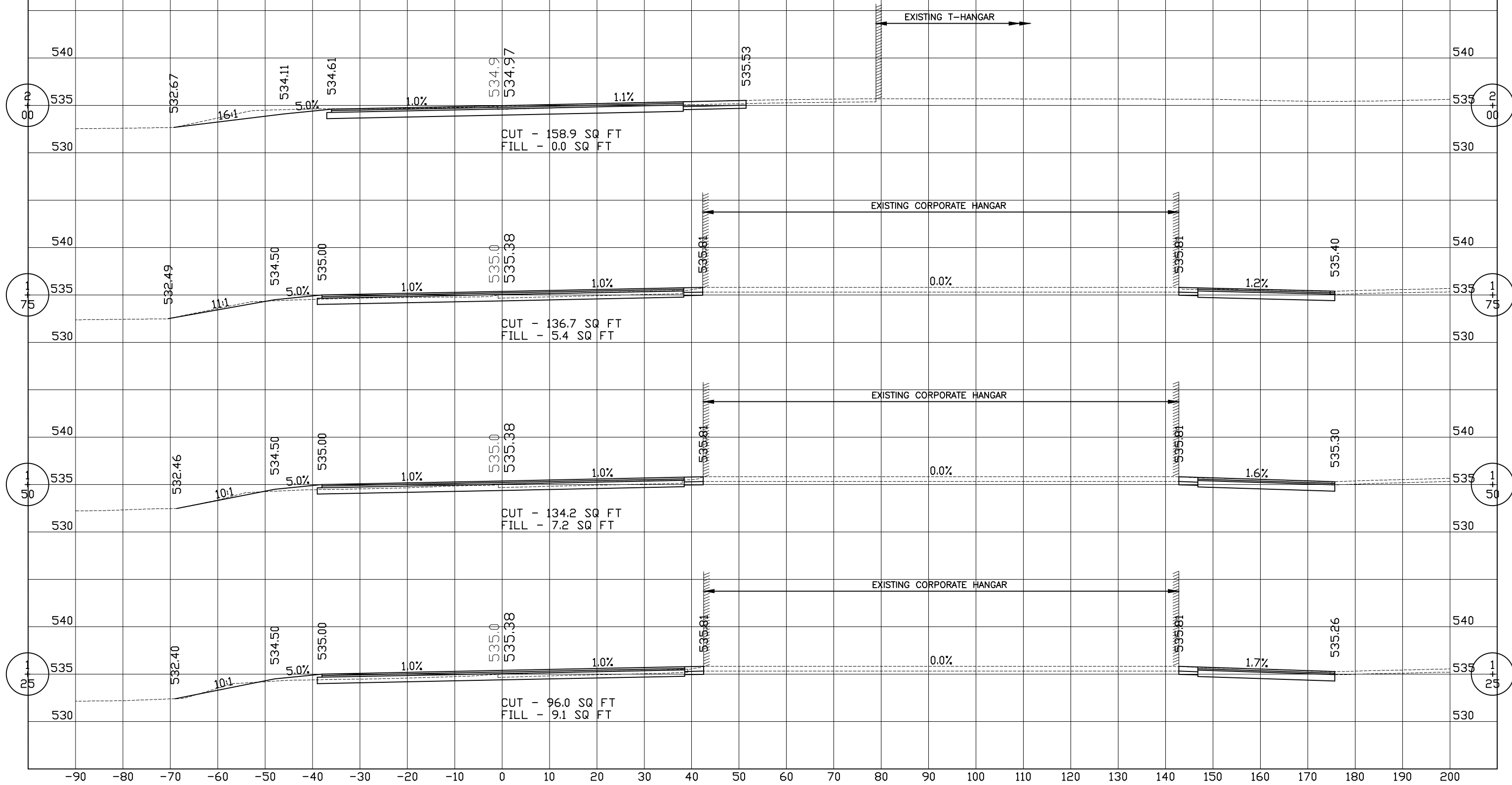
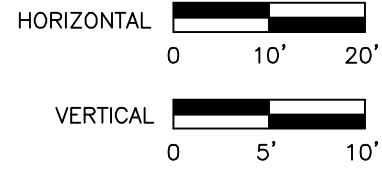


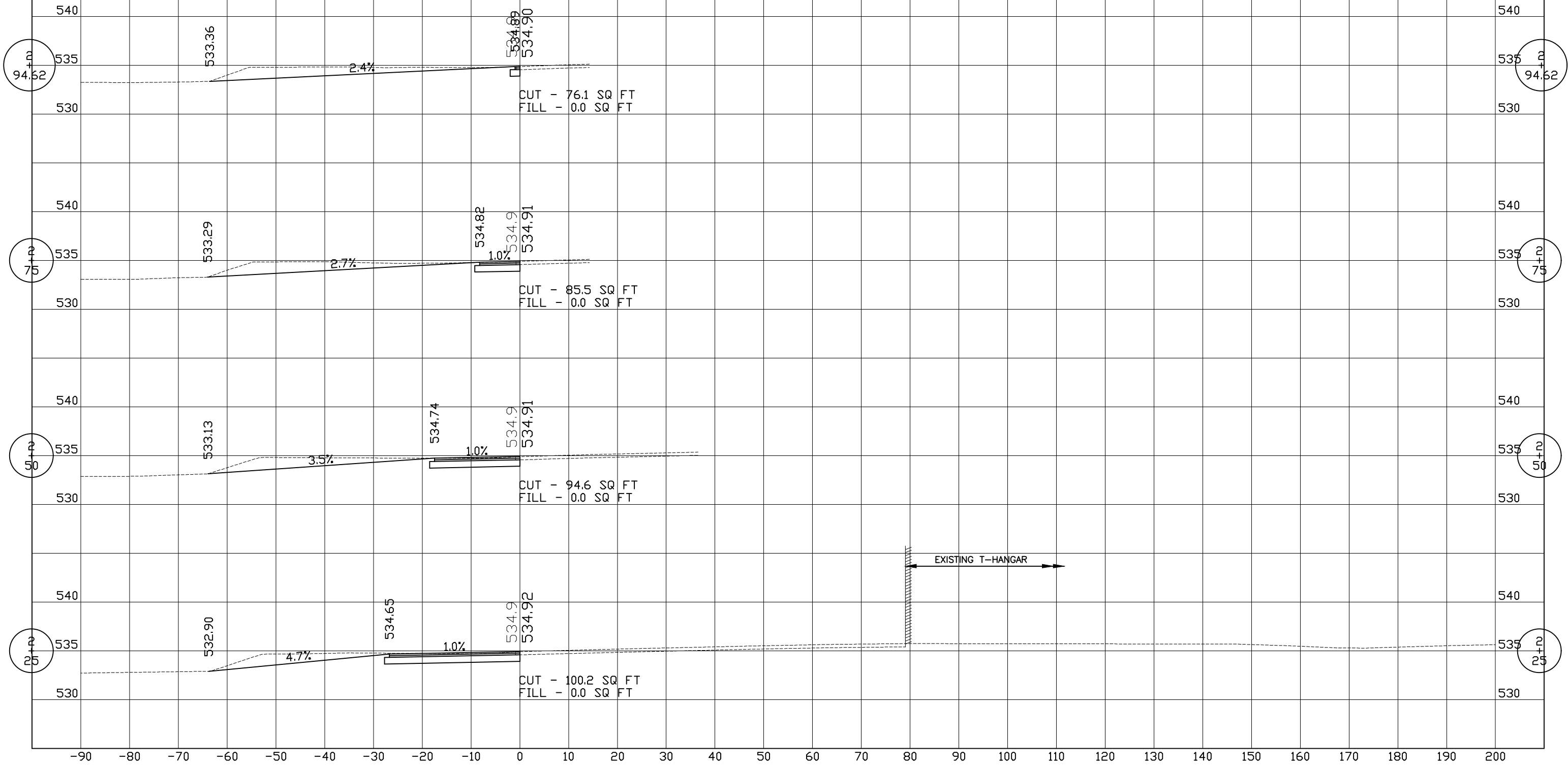
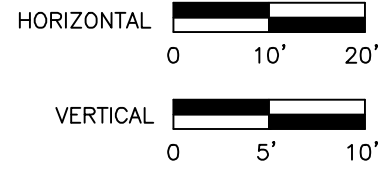
TYPE C EXPANSION
EXPANSION JOINTS

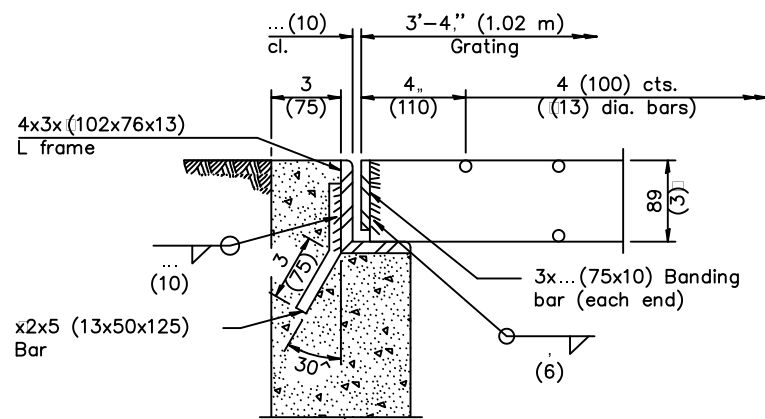
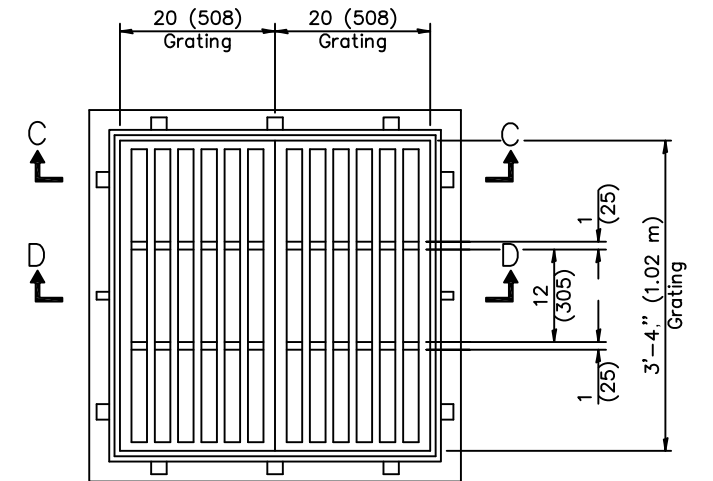
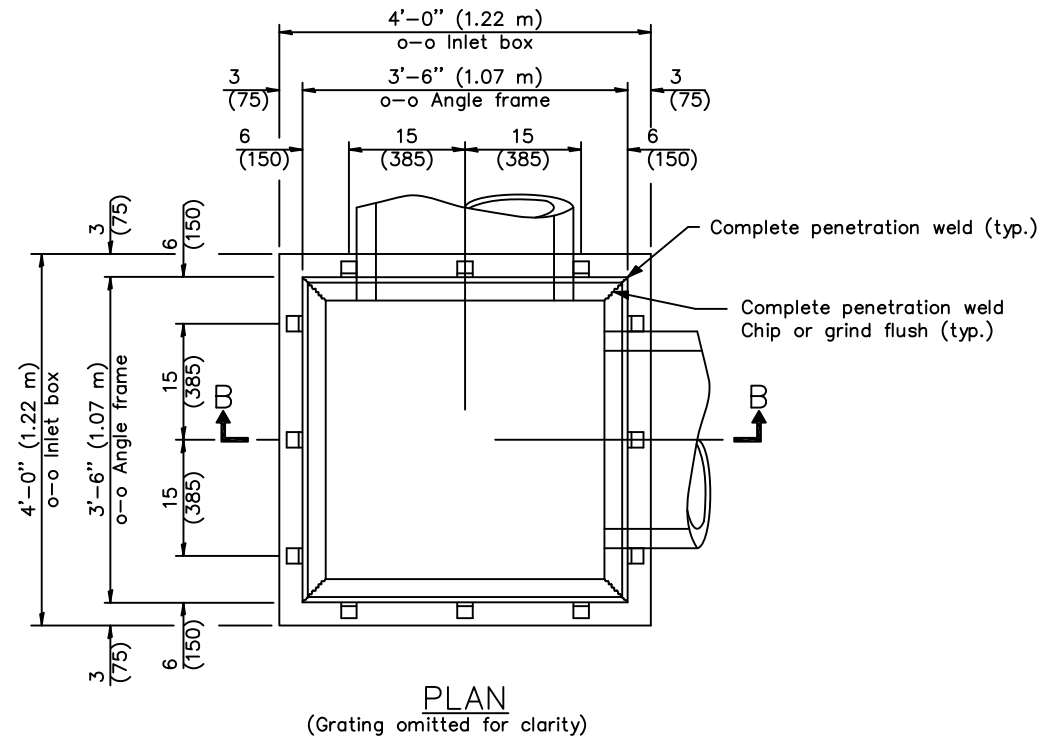
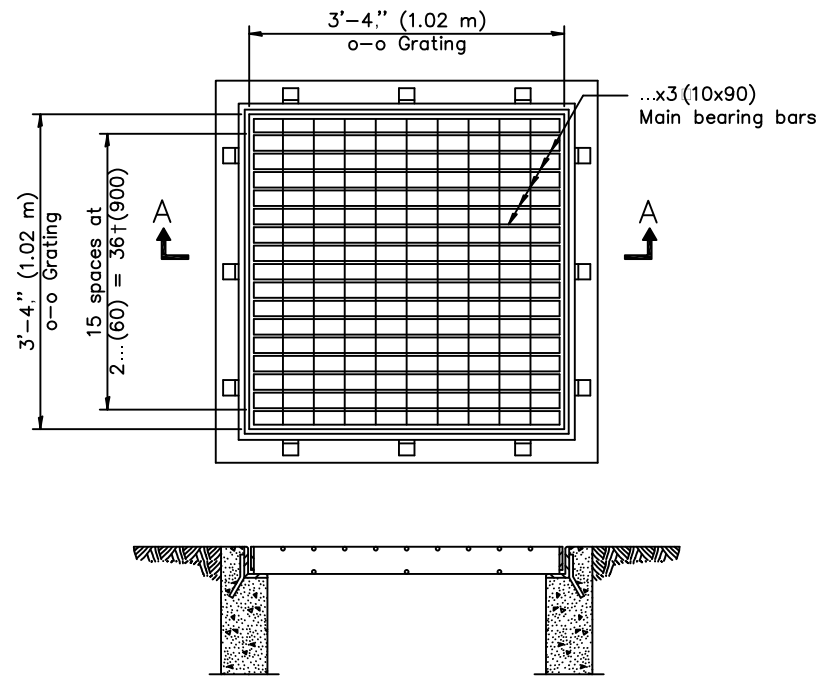


SEE JOINT PLAN FOR SPACING
JOINT DETAIL 'A'
NO SCALE

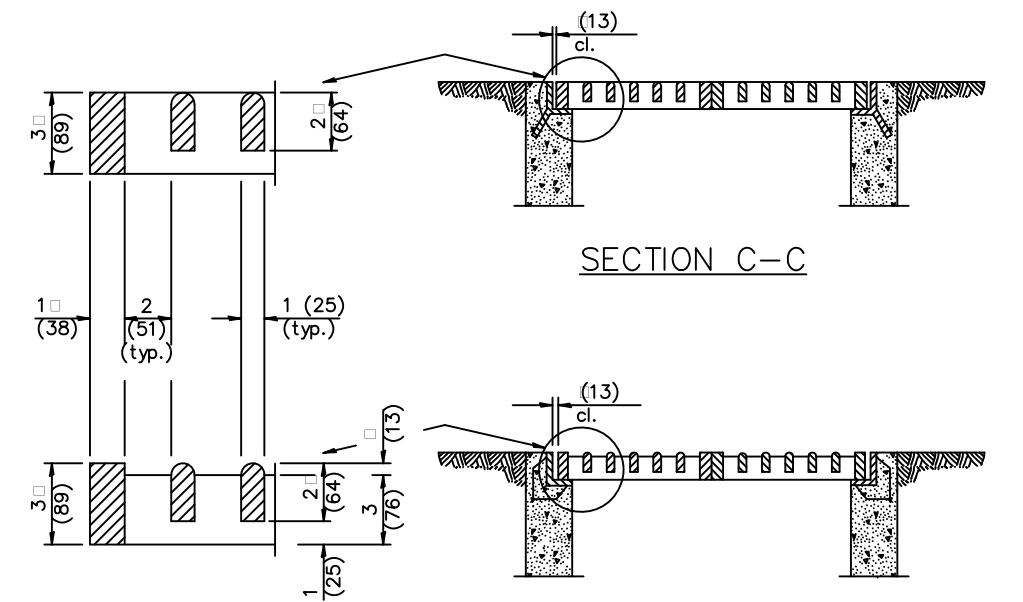
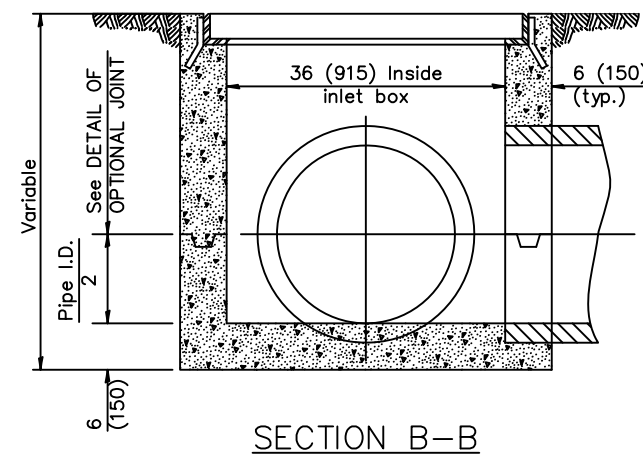
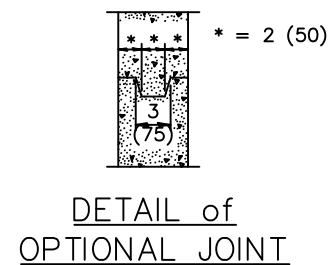




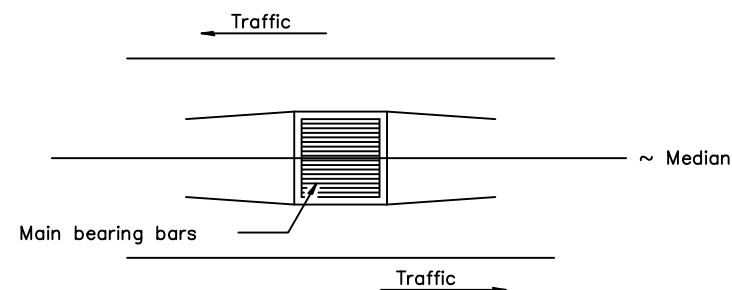




STEEL FRAME & GRATE

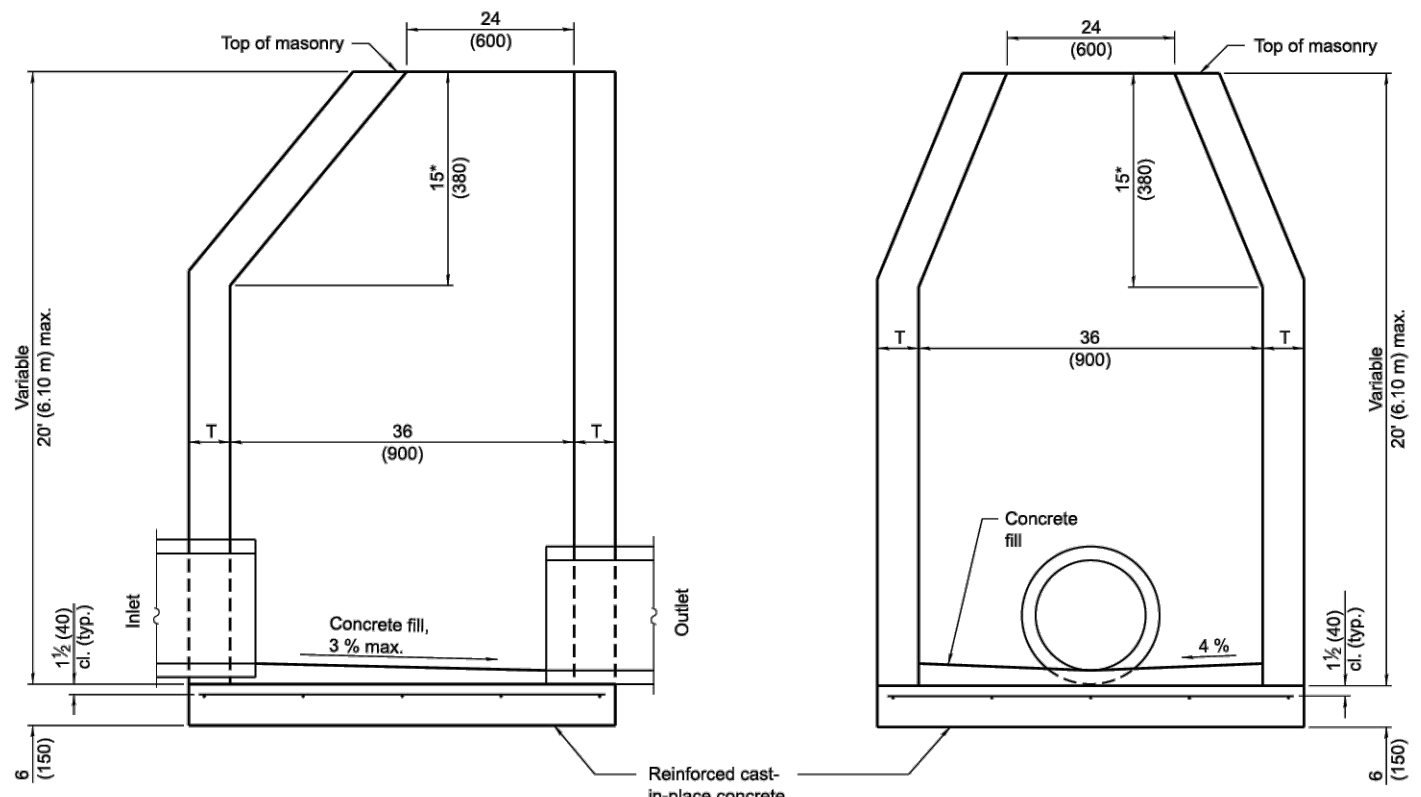


SECTION C-C
 SECTION D-D
 CAST FRAME & GRATE



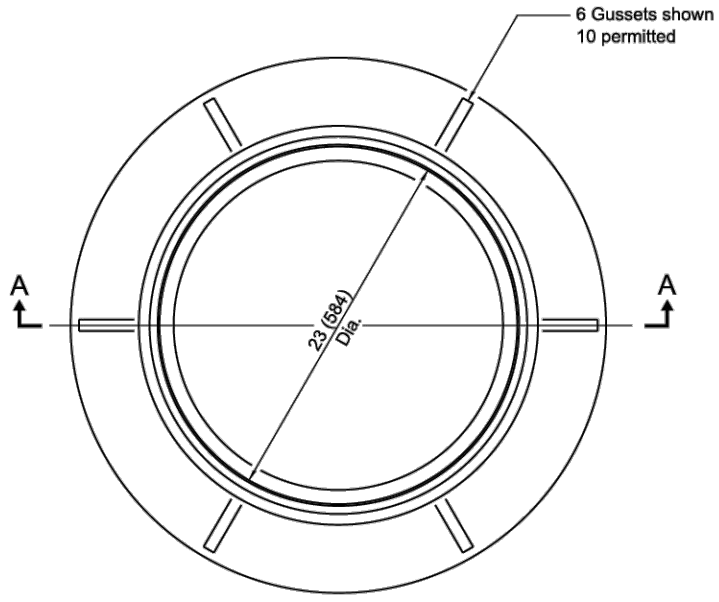
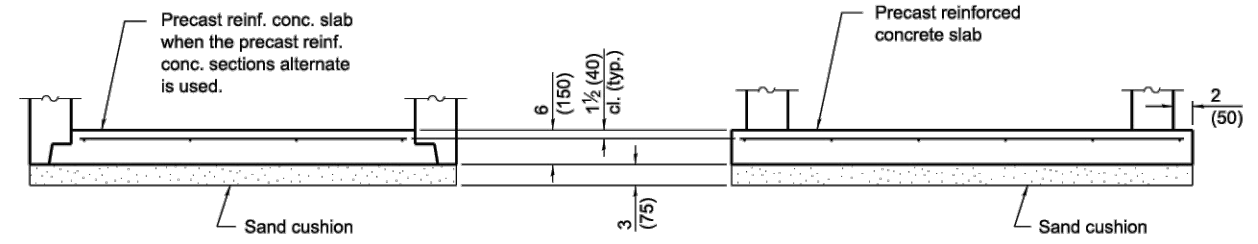
Sketch showing location and direction of
main bearing bars in relation to ~ median

ALTERNATE MATERIALS FOR WALLS	T (min.)
Concrete Masonry Unit	5 (125)
Brick Masonry	8 (200)
Precast Reinforced Concrete Section	3 (75)
Cast-in-Place Concrete	6 (150)

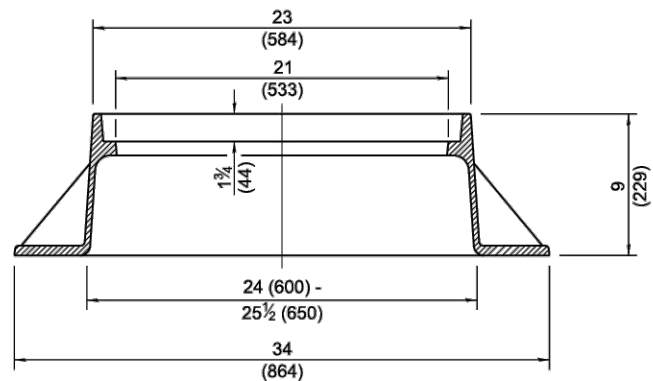


ELEVATION - ECCENTRIC

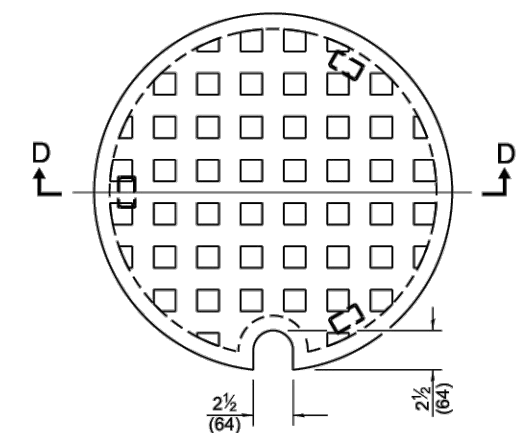
ELEVATION - CONCENTRIC



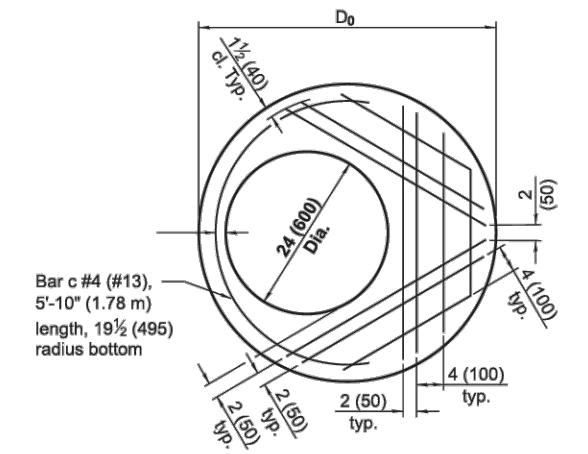
CAST FRAME



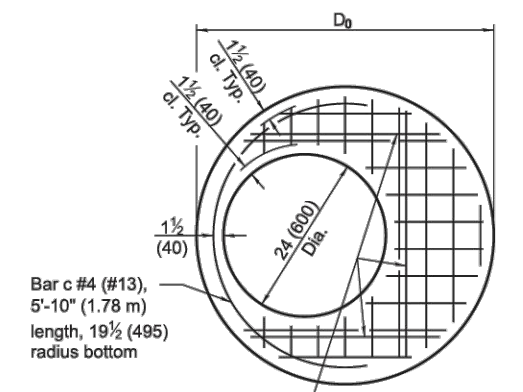
SECTION A-A
Gray Iron



CAST CLOSED LID
Gray Iron Lid

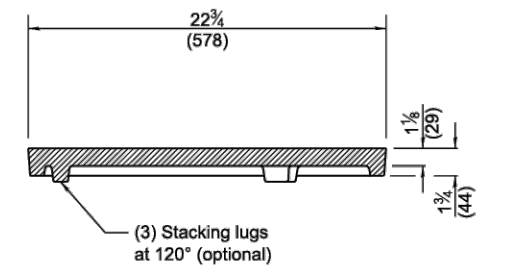


PLAN - FLAT SLAB TOP FOR D = 36 (900)
(Showing layout of reinforcement bars and c bars)



#4 (#13) bars bottom. Bundle with closest WWR bar to the opening.

PLAN - FLAT SLAB TOP FOR D = 36 (900)
(Showing layout of welded wire reinforcement and c bars)



SECTION D-D

Flat slab top used in lieu of tapered top when field conditions prohibit the use of tapered tops.

All dimensions are in inches (millimeters) unless otherwise shown.