

CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS

CONSTRUCTION PLANS FOR CHICAGO EXECUTIVE AIRPORT

CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS



**CONTACT THE METROPOLITAN
 WATER RECLAMATION DISTRICT
 OF GREATER CHICAGO 2 DAYS
 BEFORE STARTING WORK**

P (708) 588-4055
 E WMOJOBSTART@MWRD.ORG

TO THE BEST OF MY KNOWLEDGE AND BELIEF, THE DRAINAGE OF SURFACE WATERS WILL NOT BE CHANGED BY THE PROPOSED DEVELOPMENT. IF ANY DRAINAGE PATTERNS WILL BE CHANGED, REASONABLE PROVISIONS HAVE BEEN MADE FOR THE COLLECTION AND DIVERSION OF SUCH SURFACE WATERS IN TO THE PUBLIC AREA, OR DRAINS APPROVED FOR THE USE BY THE MUNICIPAL ENGINEER, AND THAT SUCH SURFACE WATERS ARE PLANNED FOR IN ACCORDANCE WITH GENERALLY ACCEPTED ENGINEERING PRACTICES SO AS TO REDUCE THE LIKELIHOOD OF DAMAGES TO ADJOINING PROPERTIES.

TRUE COPY OF PLANS ON FILE WITH THE METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO WATERSHED MANAGEMENT ORDINANCE PERMIT NO. 2024-0181.

ALL STORM SEWER AND SANITARY SEWER WITHIN THE AIRFIELD PROPERTY IS OWNED BY CHICAGO EXECUTIVE AIRPORT. CHICAGO EXECUTIVE AIRPORT IS CO-OWNED BY BOTH THE VILLAGE OF WHEELING AND THE CITY OF PROSPECT HEIGHTS. THE SITE IS LOCATED WITHIN THE EXISTING NORTH DETENTION BASIN.

811 Know what's below.
 Call before you dig.

J.U.L.I.E.
 JOINT UTILITY LOCATING
 INFORMATION FOR EXCAVATORS
 www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.



D. Kyle Peabody
 LICENSE EXPIRATION
 DATE 11/30/2025
 DATE SIGNED: 07/26/2025

ILLINOIS PROJECT: PWK-5128
 S.B.G. PROJECT: 3-17-SBGP-TBD

JULY 26, 2024

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

CHICAGO EXECUTIVE AIRPORT

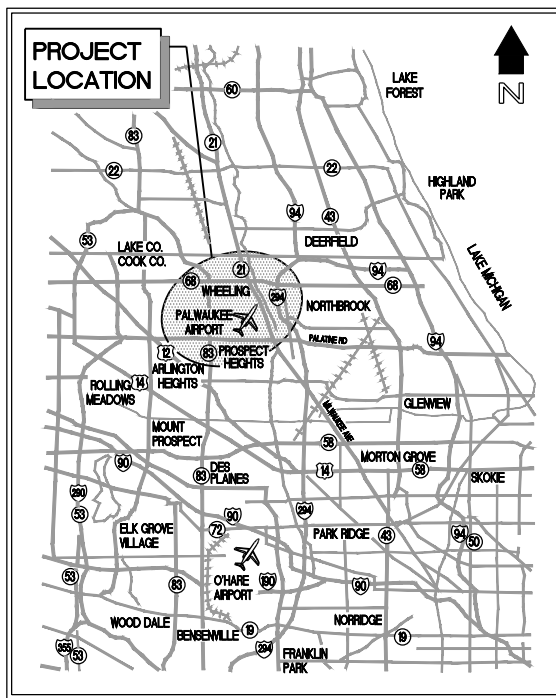
TOWNSHIP: 42 NORTH WHEELING TOWNSHIP
 RANGE: 11 EAST (SECTION: 13)
 COOK COUNTY

CMT 23005747.00
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 062-069052

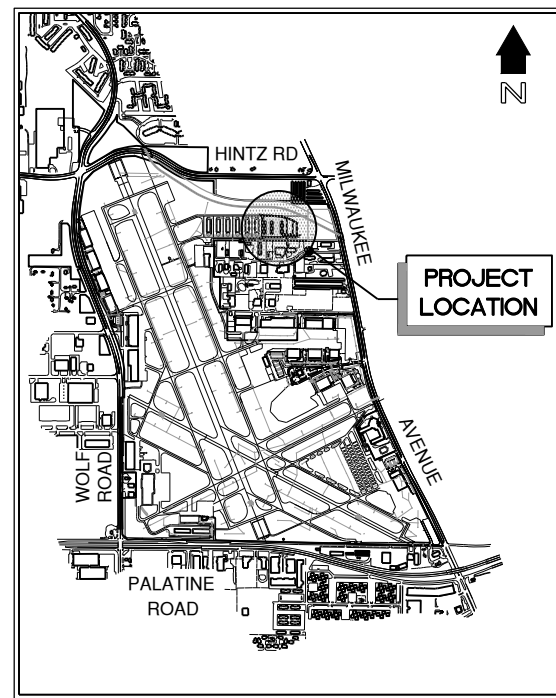
SUBMITTED BY *D. Kyle Peabody*
 D. KYLE PEABODY, PE
 DATE 7/26/2024

CHICAGO EXECUTIVE AIRPORT

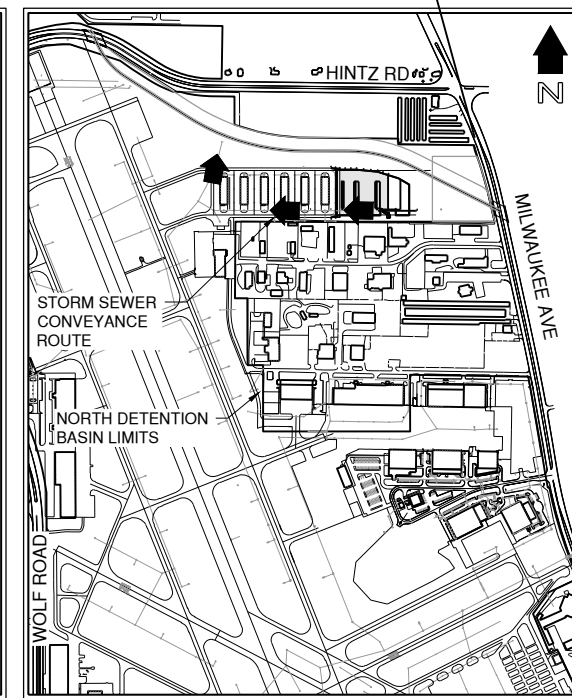
APPROVED *JEFF MILLER* EXECUTIVE DIRECTOR
 JEFFREY J. MILLER, A.A.E., ACE
 DATE 7/26/2024



LOCATION MAP



SITE PLAN



STORMWATER ROUTING MAP

PROJECT INFORMATION

CONTRACTOR:
 RESIDENT ENGINEER:
 ORIGINAL CONTRACT AMOUNT:
 FINAL CONSTRUCTION COST:
 IDOT LETTING DATE:
 IDOT AWARD DATE:
 NOTICE TO PROCEED:
 START OF CONSTRUCTION:
 SUBSTANTIAL COMPLETION:

LOCAL AGENCY CONTACT INFORMATION

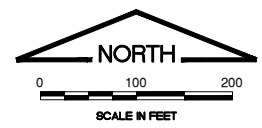
VILLAGE OF WHEELING - 847.459.2600
 CITY OF PROSPECT HEIGHTS - 847.398.6070

ENGINEER'S PROJECT PERMIT LOG

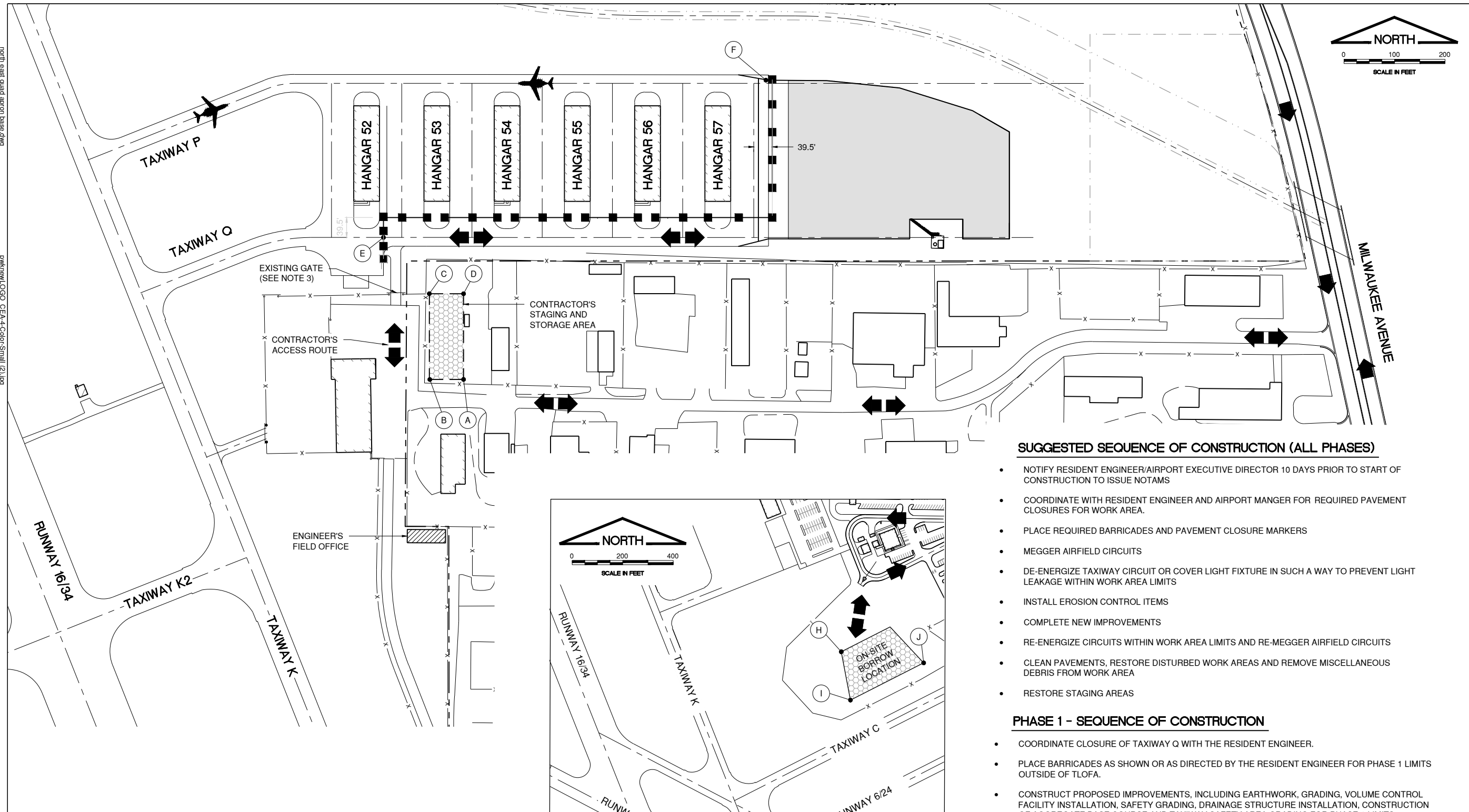
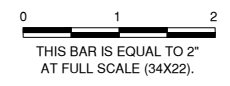
NPDES #
 FAA AIRSPACE #
 CCDD LPC-663 DATED
 MWRDGC PERMIT # 2024 - 0181
 VILLAGE APP FOR CONSTRUCTION PERMIT #
 VILLAGE FLOODPLAIN PERMIT #
 CONTRACTOR'S REGISTRATION WITH VILLAGE
 VILLAGE SITE ALTERATION PERMIT #
 CITY APPLICATION FOR PERMIT #
 CITY FLOODPLAIN PERMIT #
 CITY SITE GRADING PERMIT #
 CONTRACTOR'S REGISTRATION WITH CITY

FILE: K:\ChicagoExecAirport\300517-00_NEC\Drawings\Draw\Sheet\300517-00_CSP.dwg
 K:\ChicagoExecAirport\300517-00_NEC\Drawings\Draw\Sheet\300517-00_CSP.dwg
 6/11/2018 10:00:00 AM
 6/11/2018 10:00:00 AM

IL CONTRACT: **PA066**
 IL LETTING ITEM: **04A**
 IL PROJECT: **PWK-5128**
 S.B.G. PROJECT: **3-17-SBGP-TBD**



REVISIONS		
NUMBER	BY	DATE



SUGGESTED SEQUENCE OF CONSTRUCTION (ALL PHASES)

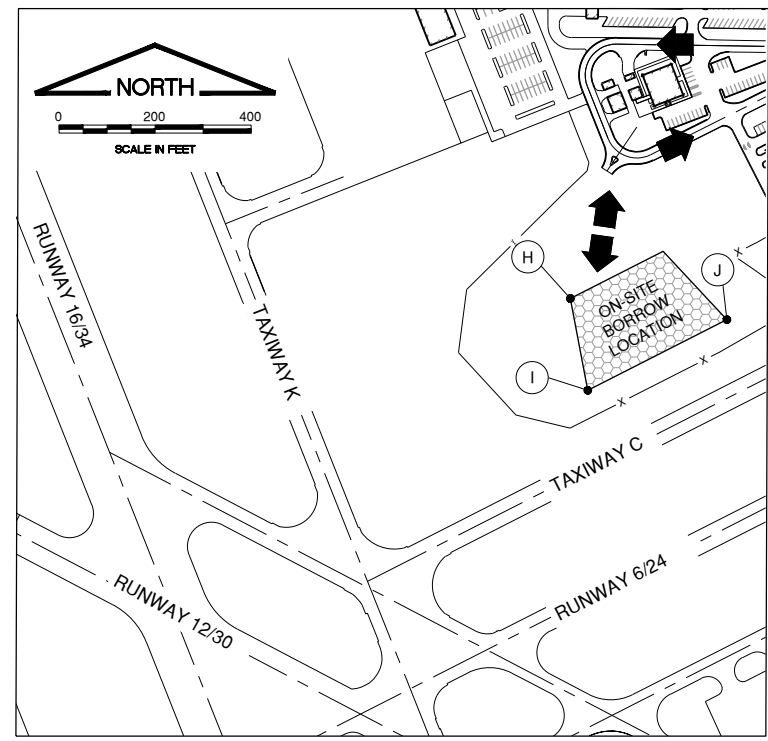
- NOTIFY RESIDENT ENGINEER/AIRPORT EXECUTIVE DIRECTOR 10 DAYS PRIOR TO START OF CONSTRUCTION TO ISSUE NOTAMS
- COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.
- PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS
- MEGGER AIRFIELD CIRCUITS
- DE-ENERGIZE TAXIWAY CIRCUIT OR COVER LIGHT FIXTURE IN SUCH A WAY TO PREVENT LIGHT LEAKAGE WITHIN WORK AREA LIMITS
- INSTALL EROSION CONTROL ITEMS
- COMPLETE NEW IMPROVEMENTS
- RE-ENERGIZE CIRCUITS WITHIN WORK AREA LIMITS AND RE-MEGGER AIRFIELD CIRCUITS
- CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA
- RESTORE STAGING AREAS

PHASE 1 - SEQUENCE OF CONSTRUCTION

- COORDINATE CLOSURE OF TAXIWAY Q WITH THE RESIDENT ENGINEER.
- PLACE BARRICADES AS SHOWN OR AS DIRECTED BY THE RESIDENT ENGINEER FOR PHASE 1 LIMITS OUTSIDE OF TLOFA.
- CONSTRUCT PROPOSED IMPROVEMENTS, INCLUDING EARTHWORK, GRADING, VOLUME CONTROL FACILITY INSTALLATION, SAFETY GRADING, DRAINAGE STRUCTURE INSTALLATION, CONSTRUCTION OF AGGREGATE BASE COURSE AND TAXIWAY SAFETY AREA GRADING FOR PHASE 1 LIMITS.

GENERAL NOTES

1. CONTRACTOR WILL BE REQUIRED TO HAVE A CROSSING GUARD AND/OR VEHICULAR ESCORT FOR CONTRACTOR'S VEHICLES AND EQUIPMENT CROSSING AIRCRAFT MOVEMENT AREA AND CROSSING ACTIVE AIRCRAFT APRON AREAS. CONTRACTOR WILL BE REQUIRED TO HAVE A MOTORIZED SWEEPER TO REMOVE DEBRIS TO THE SATISFACTION OF THE RESIDENT ENGINEER.
2. SEE SHEET 7 FOR CRITICAL POINT TABLE.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON-WORKING HOURS. DURING HAULING OPTIONS, THE CONTRACTOR HAS THE OPTION TO KEEP THE GATE OPEN WITH A GATE GUARD OR CLOSE THE GATE UPON ENTERING AND EXITING THE SITE FOR EACH VEHICLE. DURING NON-HAULING OPERATIONS, THE CONTRACTOR SHALL CLOSE THE GATE UPON ENTERING AND EXITING THE SITE. THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE AT THE END OF EACH DAY. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO THE CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBLE OF THE CONTRACTOR.
4. A PERIOD OF THIRTY (30) DAYS SHALL ELAPSE BETWEEN THE PLACEMENT OF THE FIRST PAINT APPLICATION AND SECOND PAINT APPLICATION.

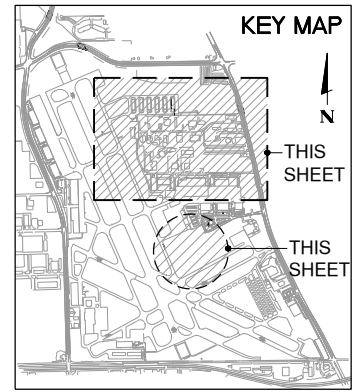


ON-SITE BORROW LOCATION

PHASE	FACILITY STATUS - OPEN	FACILITY STATUS - CLOSED
1	TAXIWAY Q WEST OF HANGAR 52; ALL OTHER TAXIWAYS OPEN; ALL RUNWAYS OPEN	TAXIWAY Q EAST OF HANGAR 52

LEGEND

- PHASE 1 WORK AREA
- EXISTING HANGAR/BUILDING
- EXISTING FENCE
- EXISTING AIRPORT PROPERTY LINE
- CONTRACTOR'S STAGING AND STORAGE AREA
- CONTRACTOR'S ACCESS ROUTE
- LOW PROFILE BARRICADES
- CRITICAL POINT FOR AIRSPACE
- AIRCRAFT MOVEMENT AREA



CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS
CONSTRUCTION SAFETY AND PHASING PLAN - 1

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

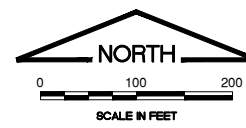
DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	23005747.00

FINAL

6mp\ydrhe aut\m\Tupci
6mp\stsq\locde pemb\paw\paw

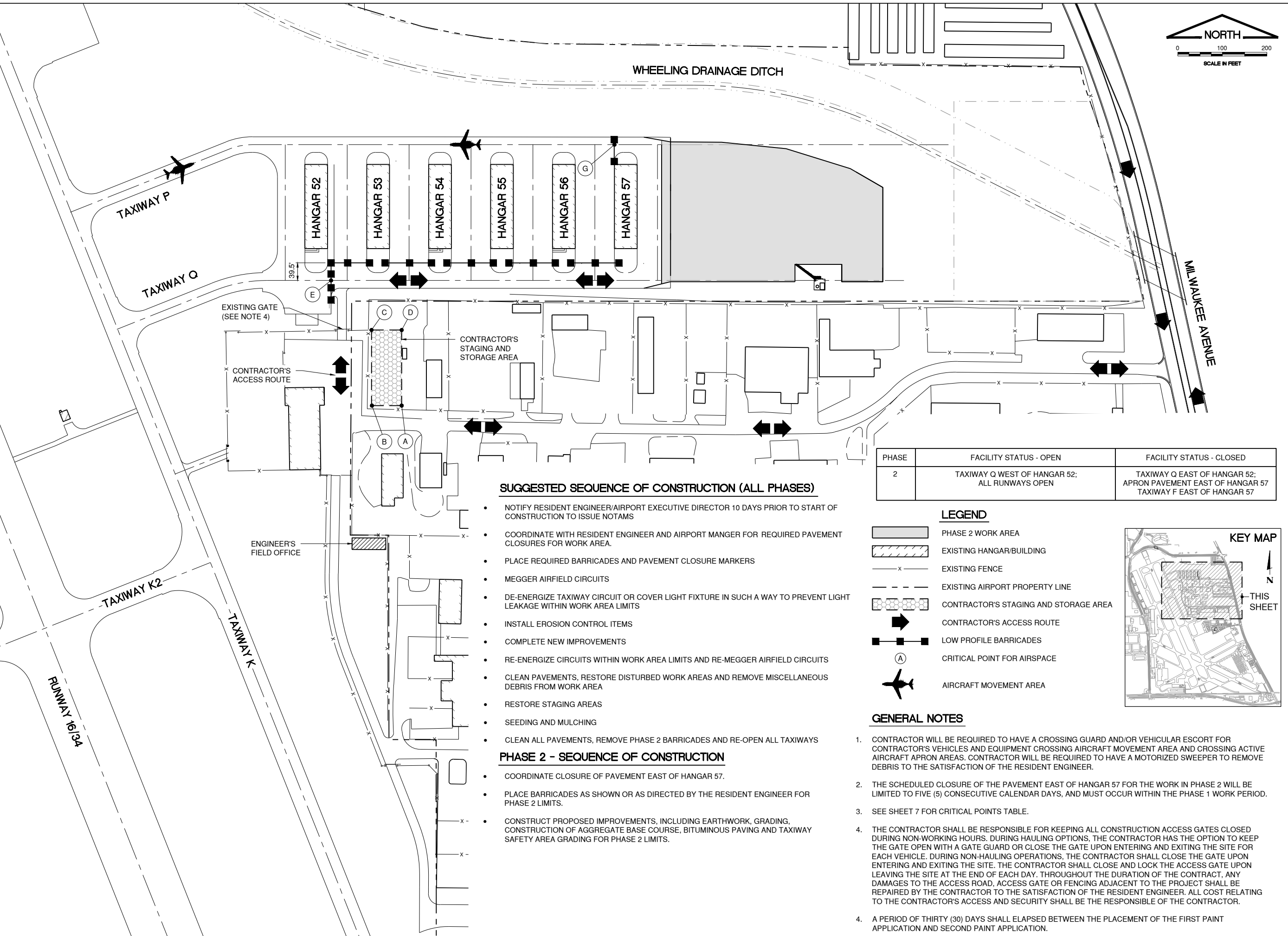
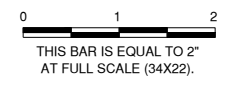
6dfl(2) jhus\cpc\H-V3C_GEA-A-Cor\Small(2).dwg

FILE: K:\ChicagoExecApo\23005747-00_NEC\ad\p\Draw\Sheet\23005747-00_Csfp_2.dwg



IL CONTRACT: PA066
IL LETTING ITEM: 04A
IL PROJECT: PWK-5128
S.B.G. PROJECT: 3-17-SBGP-TBD

REVISIONS		
NUMBER	BY	DATE



SUGGESTED SEQUENCE OF CONSTRUCTION (ALL PHASES)

- NOTIFY RESIDENT ENGINEER/AIRPORT EXECUTIVE DIRECTOR 10 DAYS PRIOR TO START OF CONSTRUCTION TO ISSUE NOTAMS
- COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.
- PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS
- MEGGER AIRFIELD CIRCUITS
- DE-ENERGIZE TAXIWAY CIRCUIT OR COVER LIGHT FIXTURE IN SUCH A WAY TO PREVENT LIGHT LEAKAGE WITHIN WORK AREA LIMITS
- INSTALL EROSION CONTROL ITEMS
- COMPLETE NEW IMPROVEMENTS
- RE-ENERGIZE CIRCUITS WITHIN WORK AREA LIMITS AND RE-MEGGER AIRFIELD CIRCUITS
- CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA
- RESTORE STAGING AREAS
- SEEDING AND MULCHING
- CLEAN ALL PAVEMENTS, REMOVE PHASE 2 BARRICADES AND RE-OPEN ALL TAXIWAYS

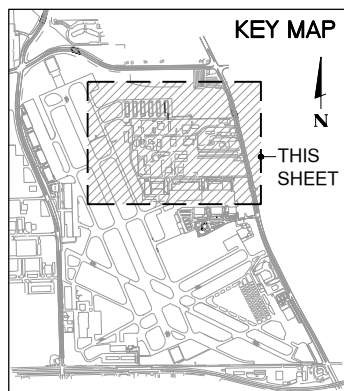
PHASE 2 - SEQUENCE OF CONSTRUCTION

- COORDINATE CLOSURE OF PAVEMENT EAST OF HANGAR 57.
- PLACE BARRICADES AS SHOWN OR AS DIRECTED BY THE RESIDENT ENGINEER FOR PHASE 2 LIMITS.
- CONSTRUCT PROPOSED IMPROVEMENTS, INCLUDING EARTHWORK, GRADING, CONSTRUCTION OF AGGREGATE BASE COURSE, BITUMINOUS PAVING AND TAXIWAY SAFETY AREA GRADING FOR PHASE 2 LIMITS.

PHASE	FACILITY STATUS - OPEN	FACILITY STATUS - CLOSED
2	TAXIWAY Q WEST OF HANGAR 52; ALL RUNWAYS OPEN	TAXIWAY Q EAST OF HANGAR 52; APRON PAVEMENT EAST OF HANGAR 57 TAXIWAY F EAST OF HANGAR 57

LEGEND

- PHASE 2 WORK AREA
- EXISTING HANGAR/BUILDING
- EXISTING FENCE
- EXISTING AIRPORT PROPERTY LINE
- CONTRACTOR'S STAGING AND STORAGE AREA
- CONTRACTOR'S ACCESS ROUTE
- LOW PROFILE BARRICADES
- CRITICAL POINT FOR AIRSPACE
- AIRCRAFT MOVEMENT AREA



GENERAL NOTES

1. CONTRACTOR WILL BE REQUIRED TO HAVE A CROSSING GUARD AND/OR VEHICULAR ESCORT FOR CONTRACTOR'S VEHICLES AND EQUIPMENT CROSSING AIRCRAFT MOVEMENT AREA AND CROSSING ACTIVE AIRCRAFT APRON AREAS. CONTRACTOR WILL BE REQUIRED TO HAVE A MOTORIZED SWEEPER TO REMOVE DEBRIS TO THE SATISFACTION OF THE RESIDENT ENGINEER.
2. THE SCHEDULED CLOSURE OF THE PAVEMENT EAST OF HANGAR 57 FOR THE WORK IN PHASE 2 WILL BE LIMITED TO FIVE (5) CONSECUTIVE CALENDAR DAYS, AND MUST OCCUR WITHIN THE PHASE 1 WORK PERIOD.
3. SEE SHEET 7 FOR CRITICAL POINTS TABLE.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON-WORKING HOURS. DURING HAULING OPERATIONS, THE CONTRACTOR HAS THE OPTION TO KEEP THE GATE OPEN WITH A GATE GUARD OR CLOSE THE GATE UPON ENTERING AND EXITING THE SITE FOR EACH VEHICLE. DURING NON-HAULING OPERATIONS, THE CONTRACTOR SHALL CLOSE THE GATE UPON ENTERING AND EXITING THE SITE. THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE AT THE END OF EACH DAY. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO THE CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBLE OF THE CONTRACTOR.
4. A PERIOD OF THIRTY (30) DAYS SHALL ELAPSE BETWEEN THE PLACEMENT OF THE FIRST PAINT APPLICATION AND SECOND PAINT APPLICATION.

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS
CONSTRUCTION SAFETY AND PHASING PLAN - 2

© Copyright CMT, Inc.

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	23005747.00

FINAL

b6mp uo0d8e auT jnd Tjup04

b6ff(2) j8rus-v0pc0-H-V3C-C6A-P-C0rc0-H-00G0-C0D0Th00k0y0k

FILE: K:\ChicagoExecutiv\0300517-00_NEC\aed\h0m\Draw\Sheet\2300517-00_Cspp_Notes_Anc_Details - 1.dwg

GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G OR LATEST EDITION, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE NOTICE TO PROCEED, THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT THROUGH THE RESIDENT ENGINEER, FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G OR LATEST EDITION. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING TENANT/AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE ENGINEER IN CONSULTATION WITH THE AIRPORT DIRECTOR OF OPERATIONS. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DIRECTOR OF OPERATIONS.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRE-CONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT A PROGRESS SCHEDULE TO BE APPROVED BY THE ENGINEER. THIS SCHEDULE SHALL SHOW START/ STOP DATES OF ALL PHASES. THE APPROVED PROGRESS SCHEDULE SHALL BE DISTRIBUTED TO ALL PARTIES 10 WORKING DAYS PRIOR TO START OF CONSTRUCTION. THE CONTRACTOR MAY BE REQUIRED TO SUBMIT A REVISED PROGRESS SCHEDULE TO ACCOMMODATE AIRPORT EVENTS OR CONTRACTOR'S LACK OF PROGRESS. SHOULD A REVISED SCHEDULE BE REQUIRED, THE REVISION SHALL BE COMPLETED AT NO ADDITIONAL COST TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH AIRPORT STAFF, FAA, RESIDENT ENGINEER AND OTHER APPROPRIATE STAKE HOLDERS TO DISCUSS PROJECT PROGRESS. AT A MINIMUM, PROJECT SCHEDULE AND GATE VISITOR LOGS SHALL BE DISCUSSED. REPRESENTATION BY THE PRIME CONTRACTOR IS MANDATORY. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

- TOTAL CONTRACT TIME FOR BASE BID ONLY SHALL BE 74 CALENDAR DAYS. SHOULD BASE BID PLUS ADDITIVE ALTERNATE NO. 1 BE AWARDED, THE CONTRACT TIME SHALL BE 88 CALENDAR DAYS. SHOULD BASE BID PLUS ADDITIVE ALTERNATES NO. 1 AND NO. 2 BE AWARDED, THE CONTRACT TIME SHALL BE 92 CALENDAR DAYS.
- PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLANS.
- TO CLOSE A RUNWAY OR TAXIWAY, THE CONTRACTOR SHALL PLACE RUNWAY AND TAXIWAY CLOSURE MARKERS AND BARRICADES AT THE LOCATIONS SPECIFIED. TO RE-OPEN THE RUNWAY OR TAXIWAY, THE CONTRACTOR SHALL CLEAN ANY DEBRIS OFF OF THE PAVEMENT AND REMOVE THE RUNWAY/TAXIWAY CLOSURE MARKERS. ALL WORK ASSOCIATED WITH CLOSING AND OPENING AIRFIELD PAVEMENTS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS REOPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL.
- SEE CONTRACTOR ACCESS NOTES ON THIS SHEET FOR SITE ACCESS AND HAULING GUIDELINES.
- PRIOR TO REOPENING A CLOSED RUNWAY OR TAXIWAY, THE ENTIRE RUNWAY SAFETY AREA (RSA), MEASURED 250 FEET FROM THE RUNWAY CENTERLINE, INCLUDING BEYOND THE RUNWAY END WITHIN THE EXTENDED RSA, AND THE ENTIRE TAXILANE OBJECT FREE AREA (TLOFA), MEASURED 39.5 FEET FROM TAXILANE CENTERLINE FOR GROUP 1 TAXILANES (I.E. TAXILANES P AND Q) MUST MEET FAA CRITERIA. FAA CRITERIA REQUIRES THAT THERE BE NO OPEN EXCAVATIONS OR TRENCHES IN THESE AREAS. THE MAXIMUM PAVEMENT DROP OFF SHALL BE 2 INCHES, AND ALL GRADES IN ANY DIRECTION BE LESS THAN 5 PERCENT. STEEL PLATES OR TEMPORARY WEDGING OF BASE COURSE MAY BE REQUIRED TO MEET CRITERIA. ALL NECESSARY TEMPORARY MEASURES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- NO MATERIAL OR EQUIPMENT SHALL BE STOCKPILED WITHIN AN RSA, ROFA OR OBSTACLE FREE ZONE (OFZ) OF AN ACTIVE RUNWAY, OR WITHIN THE TLOFA OF AN ACTIVE TAXILANE.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
- THE ENGINEER AND AIRPORT DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT OPERATIONS. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. VEHICULAR TRAFFIC AND CONTRACTOR ACTIVITIES SHALL YIELD TO AIRCRAFT OPERATIONS. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

4. NAVAIDS THAT COULD BE AFFECTED

- THE CONTRACTOR MUST COORDINATE WITH AIRPORT OPERATIONS/ENGINEER AND FAA A **MINIMUM OF 30 DAYS** IN ADVANCE FOR ANY WORK WITHIN A NAVAID CRITICAL AREA OR AFFECTING THE VISUAL, TRANSMITTED SIGNAL OR POWER SUPPLY OF A NAVAID.
- EDGE LIGHTS, THRESHOLD LIGHTS, VISUAL AIDS AND ALL ILS EQUIPMENT SHALL BE SHUT OFF FOR THE DURATION OF A CLOSURE PERIOD ON ANY ASSOCIATED PAVEMENTS. IF THE LIGHTING CIRCUIT MUST BE ON FOR OPEN PAVEMENT AREAS, CLOSED PAVEMENT AREA LIGHTS SHALL BE COVERED COMPLETELY.
- EXCEPT WHERE NOTED IN THE PLANS, EXISTING COMMUNICATIONS EQUIPMENT AND NAVIGATIONAL AIDS (NAVAIDS) SHALL NOT BE DISTURBED BY THE CONTRACTOR AND SHALL BE PROTECTED FROM DAMAGE.
- PRIOR TO BEGINNING SITE WORK, CONTRACTOR SHALL COORDINATE THROUGH AIRPORT OPERATIONS TO LOCATE, AND THE CONTRACTOR TO MARK ALL UNDERGROUND COMMUNICATIONS CABLES AND FACILITIES, WITHIN THE PROJECT AREA.
- IF CONTRACTOR CAUSES INTERRUPTION OF POWER OR COMMUNICATIONS TO A NAVAID CONTRACTOR SHALL REPAIR WITHIN 24 HOURS AT THE CONTRACTOR'S COST. CONTRACTOR MUST COORDINATE REPAIR WITH AIRPORT OPERATIONS BEFORE ANY REPAIR IS MADE.
- THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD LIGHTING CIRCUITS AT THE START AND END OF EACH CONSTRUCTION DAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE DONE SO AT NO EXTRA COST TO THE CONTRACT.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION, CONTINUOUS ALL WEATHER ACCESS TO EXISTING NAVAIDS IN THE PROJECT AREA MUST BE MAINTAINED AT NO COST TO THE CONTRACT.

5. CONTRACTOR ACCESS

- THE CONTRACTOR ACCESS ROAD AND STAGING AREAS SHALL BE AS SHOWN ON THE REFERENCED PLAN. THE CONTRACTOR SHALL MAINTAIN AND REPAIR THE CONSTRUCTION ACCESS ROAD AND STAGING AREA IN ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT. ALTERNATE STAGING AREAS AND ACCESS FOR THIS AREA WILL NOT BE ALLOWED.
- THE CONTRACTOR SHALL ACCESS THE SITE USING THE ROUTES AND GATES SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED WHEN NOT IN USE.
- CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
- THE CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON (FLASHING YELLOW) LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION.

- THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFERS SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.
- DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM/HER AT HIS/HER EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE CONTROL TOWER. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
- IF NECESSARY, THE CONTRACTOR SHALL CONSTRUCT A HAUL ROUTE TO THE STAGING AREA WITHIN THE PROJECT LIMITS. HAUL ROUTE(S) SHALL BE INCIDENTAL TO THE COST OF MAINTENANCE OF TRAFFIC. ALL HAUL ROUTE(S) INCLUDING EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE ENGINEER AND AIRPORT DIRECTOR OF OPERATIONS. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE HAUL ROUTE(S) SHALL BE INCIDENTAL TO THE COST OF THE CONTRACT.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A STAGING AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE ENGINEER AND AIRPORT OPERATION MANAGER. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE PAVEMENTS / AREAS SHALL BE INCIDENTAL TO THE CONTRACT.
- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ON SITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS. PAYMENT FOR DUST CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY THE AIRPORT OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING PAVEMENT OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE DIRECTOR OF OPERATIONS. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 10 DAYS NOTICE TO THE FAA AND AIRPORT PRIOR TO THE CLOSURE OF ANY RUNWAY SO THAT THE FAA MAY DEACTIVATE THE FAA - OWNED NAVAIDS.
- THE CONTRACTOR SHALL GIVE A MINIMUM 30 DAYS NOTICE TO THE AIRPORT, AND PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, PRIOR TO CLOSING ANY RUNWAY OR TAXIWAY PAVEMENT SO THAT THE PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT FOR COORDINATION WITH THE AIRPORT TENANTS.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2G OR LATEST EDITION MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL REQUEST OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

GROUND CONTROL FREQUENCY: 121.7 MHz
AIR CONTROL FREQUENCY: 119.9 MHz

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: DUMP TRUCK IN DUMP POSITION - 25'

IN THE EVENT THE CONTRACTOR PROPOSES TO UTILIZE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN WHAT IS LISTED, THE CONTRACTOR WILL BE RESPONSIBLE TO SUBMIT FAA FORM 7460 FOR AIRSPACE APPROVAL. THE RESIDENT ENGINEER WILL PROVIDE BASE AIRPORT INFORMATION FOR THE CONTRACTOR'S USE.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND ACCESS THE WORK SITE DUE TO ADJACENT CONSTRUCTION

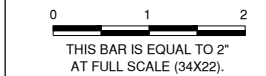
- 2025 AIRFIELD PAVEMENT REPAIR AND REMARKING.
- MWRD PROJECT #06-360-3SR UPPER DES PLAINES INTERCEPTING SEWER 14B REHABILITATION, NSA

ALLOWABLE CONSTRUCTION HOURS

- THE ALLOWABLE CONSTRUCTION HOURS FOR THE VILLAGE OF WHEELING AND THE CITY OF PROSPECT HEIGHTS ARE FROM 7 AM TO 6 PM, MONDAY THROUGH SATURDAY. THE AIRPORT WILL SEEK A WAIVER WITH THE VILLAGE AND CITY TO ALLOW CONSTRUCTION OUTSIDE OF THOSE HOURS FOR THE PHASES SHOWN TO BE COMPLETED OVER WEEKENDS ONLY. AT ALL OTHER TIMES, IT IS EXPECTED THE CONTRACTOR WILL ADHERE TO THE VILLAGE AND CITY NOISE ORDINANCE AND ALLOWABLE CONSTRUCTION HOUR POLICIES. SHOULD THE CONTRACTOR REQUIRE ADDITIONAL WORKING HOURS, HE SHALL REQUEST, THROUGH THE RESIDENT ENGINEER, THAT THE VILLAGE AND CITY BE CONTACTED TO REQUEST ADDITIONAL WAIVER OF THE NOISE ORDINANCE POLICY. ANY FINES LEVIED BY THE VILLAGE OR CITY TO THE AIRPORT FOR VIOLATIONS OF THE NOISE ORDINANCE AND ALLOWABLE CONSTRUCTION HOURS SHALL BE PAID BY THE CONTRACTOR.

IL. CONTRACT: **PA066**
IL. LETTING ITEM: **04A**
IL. PROJECT: **PWK-5128**
S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS		
NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS

CONSTRUCTION SAFETY AND PHASING PLAN
GENERAL NOTES AND DETAILS - 1

© Copyright CMT, Inc.

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	23005747.00

FINAL

SHEET 6 OF 33 SHEETS

11. UNDERGROUND UTILITIES

- 1. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHAT SO EVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.
2. SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATIONS.
3. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

12. PENALTIES

- 1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.
2. THE CONTRACTOR SHALL RESTRICT ALL CONSTRUCTION ACTIVITIES TO THE CONSTRUCTION AREA DETAILED IN THE CONSTRUCTION SAFETY AND PHASING PLAN. ANY UNAUTHORIZED MOVEMENTS, PEDESTRIAN OR VEHICULAR, BEYOND THE CONSTRUCTION LIMITS SHOWN SHALL BE CONSIDERED AND AIRFIELD INCURSION. AIRFIELD INCURSIONS, AT THE DISCRETION OF THE AIRPORT DIRECTOR OF AVIATION, MAY BE FINED UP TO \$1,000 PER INCIDENT. INCURSION FINES WILL BE ASSESSED IMMEDIATELY AND TAKEN FROM MONIES DUE THE CONTRACTOR ON THE NEXT CONSTRUCTION PAYMENT.

13. SPECIAL CONDITIONS

- 1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- 1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION SAFETY AND PHASING PLAN.
2. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2G OR LATEST EDITION.

15. MARKING AND SIGNS FOR ACCESS ROUTES

- 1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THE CONSTRUCTION SAFETY AND PHASING PLAN.

16. HAZARD MARKING AND LIGHTING

- 1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5C OR LATEST EDITION AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
5. PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A.) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES USED ON THE AIRFIELD MUST BE INTERLOCKING LOW PROFILE BARRICADES. INTERLOCKING LOW PROFILE BARRICADES WITH STEADY BURN RED LIGHTS SHALL BE INTERLOCKED WITH NO GAPS BETWEEN BARRICADES. BARRICADES SHALL BE PLACED AS SHOWN ON THIS PLAN AND AS DIRECTED BY THE ENGINEER FOR WORK ADJACENT TO THE EXPEDITED WORK AREA. WHEN NOT IN USE, THESE BARRICADES SHALL BE STORED AT THE CONTRACTOR'S STAGING AREA OR OFF SITE. ACCESS TO THE ACTIVE RUNWAY AND TAXIWAY PAVEMENTS (TOWER CONTROLLED AREAS) SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON THE CLOSEST BARRICADES (2 EACH, RIGHT AND LEFT) AT THE ENTRANCE. IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "TOWER CONTROL AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE." ALL NON AIRFIELD LOCATIONS REQUIRING BARRICADES SHALL BE TYPE II OR TYPE III BARRICADES AND SHALL CONFORM TO DOT STANDARD DETAIL 701901-04.

17. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

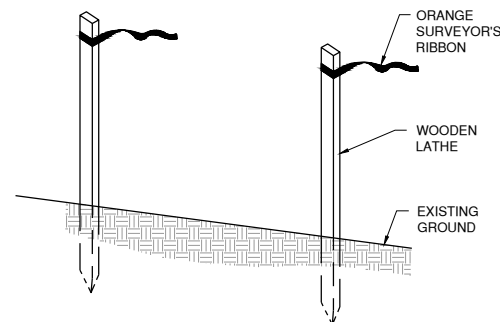
- 1. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTION.
2. LIGHTS SHALL CONSIST OF VEHICLE OR MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL NOT INTERFERE WITH AIR OPERATIONS. ANY WORK BEING PERFORMED UNDER INSUFFICIENT ARTIFICIAL LIGHTING, IN THE RESIDENT ENGINEER'S JUDGEMENT, SHALL BE STOPPED UNTIL SUCH TIME AS ADDITIONAL LIGHTING IS PROVIDED. ALL WORK PERFORMED DURING THAT TIME WILL NOT BE ACCEPTABLE UNTIL PROPER INSPECTION AND TESTING CAN BE MADE.

18. PROTECTION

- 1. ALL WORK REQUIRED INSIDE OF A RUNWAY SAFETY AREA, WILL REQUIRE THE RUNWAY TO BE CLOSED.
2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WILL REQUIRE THE TAXIWAY TO BE CLOSED.

19. OTHER LIMITATIONS ON CONSTRUCTION

- 1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE CONTRACT DOCUMENTS.



CONSTRUCTION SETBACK LINE DETAIL

NOT TO SCALE

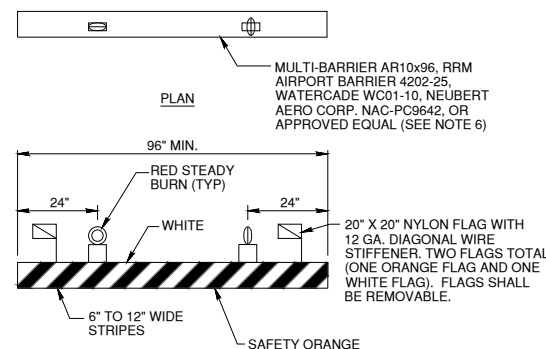
CONSTRUCTION SETBACK NOTES

- 1. CONTRACTOR SHALL MARK THE RUNWAY SAFETY AREA, TAXIWAY OBJECT FREE AREA AND RUNWAY OBSTACLE FREE ZONE PER THE CONSTRUCTION SETBACK DETAIL AS DIRECTED BY THE RESIDENT ENGINEER.
2. ALL COST ASSOCIATED WITH THE CONSTRUCTION SETBACK LINE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

AIRFIELD LIGHTS AND SIGNS NOTES

- 1. CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
2. CONTRACTOR SHALL TURN OFF RUNWAY/TAXIWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT, DURING ALL RUNWAY CLOSURES. CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY/TAXIWAY. THE CONTRACTOR SHALL COORDINATE WITH AIRPORT EXECUTIVE DIRECTOR/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.

DESIGN AIRCRAFT APPROACH CATEGORY: A
DESIGN AIRPORT GROUP: I
MAXIMUM ANTICIPATED WINGSPAN OF ADG I
CIRRUS SR 22 - WINGSPAN = 38.3'

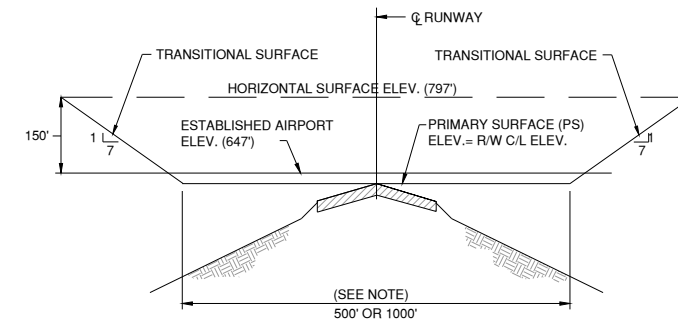


LOW PROFILE LIGHTED BARRICADE

NOT TO SCALE

BARRICADE NOTES:

- 1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OR SOLAR POWER OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED END TO END AS INDICATED AT THE LOCATIONS SHOWN ON THE PLANS ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION OR AS DIRECTED BY THE RESIDENT ENGINEER OR AIRPORT. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN.

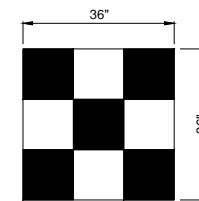


TYPICAL SECTION F.A.R. PART 77 IMAGINARY SURFACES

NO SCALE

NOTE

- 1. IMAGINARY SURFACE REQUIREMENTS FOR EXISTING ACTIVE RUNWAYS (R/W) ARE SIMILAR EXCEPT PRIMARY SURFACE (PS) DIMENSIONS VARY R/W 6/24 & 12/30 500' PS (25' LT & RT OF CENTERLINE) R/W 16/34 1000' PS (500' LT & RT OF CENTERLINE).



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG

NOT TO SCALE

EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS

Table with columns: RUNWAY, APPROACH CATEGORY, DESIGN GROUP, DESIGN AIRCRAFT, APPROACH SPEED, WINGSPAN, TAIL HEIGHT, STRENGTH (MGTW), LENGTH, AOA @ RUNWAY SAFETY AREA WIDTH (RSA), RUNWAY OBJECT FREE AREA WIDTH (ROFA), TAXIWAY SAFETY AREA WIDTH (TSA), AOA @ TAXIWAY OBJECT FREE AREA WIDTH (TOFA). Rows include 16/34, 12/30, and 6/24 runways.

AOA = AIRCRAFT OPERATIONS AREA DATA FROM 2021 CEA APPROVED ALP

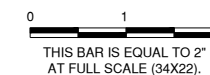
CRITICAL POINTS TABLE

Table with columns: POINT, APPROXIMATE ELEVATION OF GROUND (1929 DATUM), ANTICIPATED EQUIPMENT AND HEIGHT, APPROXIMATE ELEVATION OF EQUIPMENT (1929 DATUM), LATITUDE (NAD 83), LONGITUDE (NAD 83). Rows A through J.

IL CONTRACT: PA066
IL LLETING ITEM: 04A
IL PROJECT: PWK-5128
S.B.G. PROJECT: 3-17-SBGP-TBD

REVISIONS

Table with columns: NUMBER, BY, DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS

CONSTRUCTION SAFETY AND PHASING PLAN
GENERAL NOTES AND DETAILS - 2

© Copyright CMT, Inc.



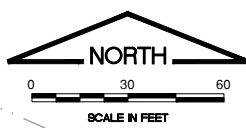
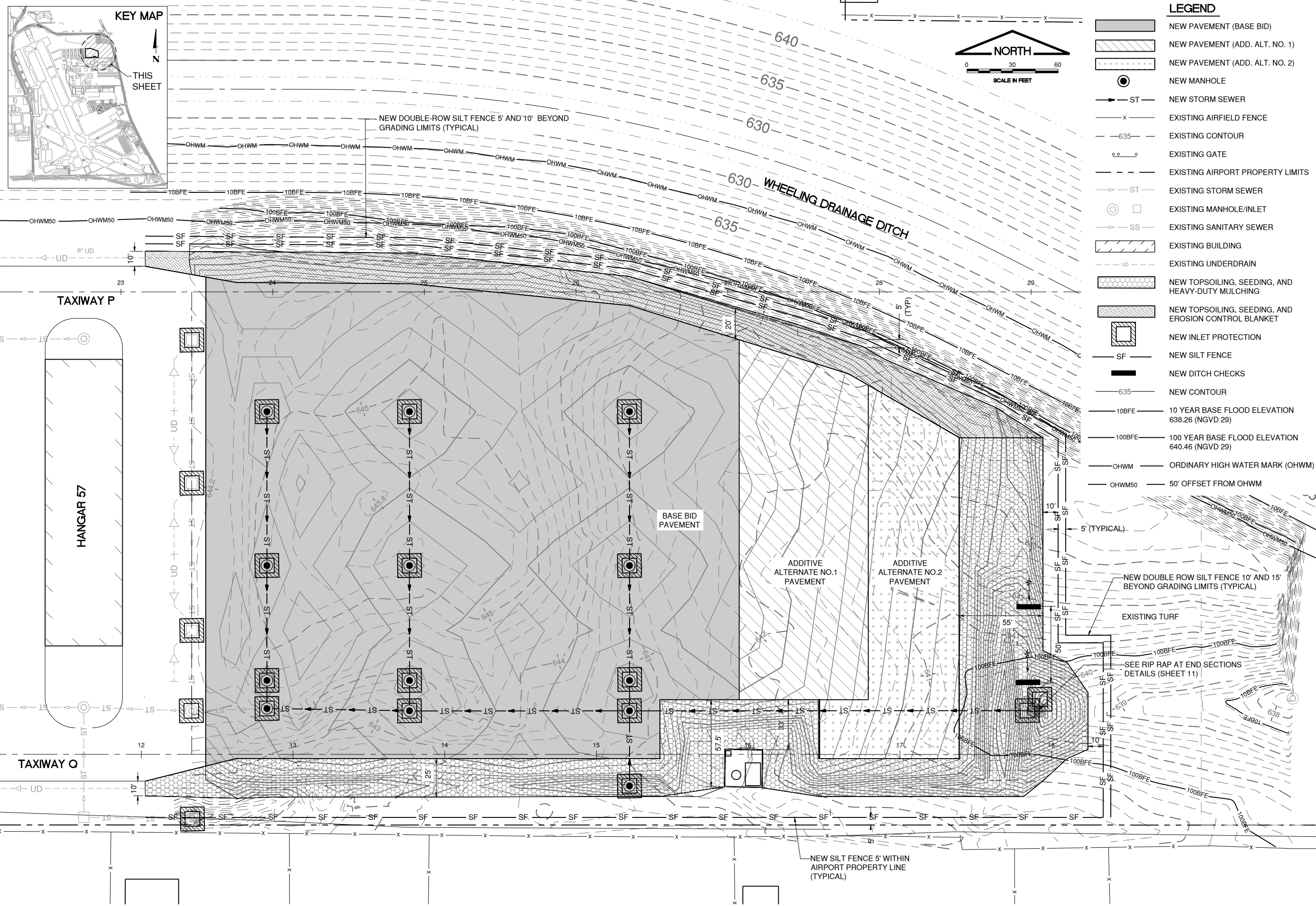
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613



DESIGN BY: STL
DRAWN BY: JRO
CHECKED BY: DKP
APPROVED BY: DKP
DATE: 07/29/2024
JOB No: 2300517-00

FINAL

FILE: K:\ChicagoExecApt\3300517-00_NEC\Drawn\Draw\Sheet\2300517-00_Swp.dwg
 © Copyright CMT, Inc.
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

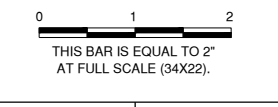


LEGEND

- NEW PAVEMENT (BASE BID)
- NEW PAVEMENT (ADD. ALT. NO. 1)
- NEW PAVEMENT (ADD. ALT. NO. 2)
- NEW MANHOLE
- NEW STORM SEWER
- EXISTING AIRFIELD FENCE
- EXISTING CONTOUR
- EXISTING GATE
- EXISTING AIRPORT PROPERTY LIMITS
- EXISTING STORM SEWER
- EXISTING MANHOLE/INLET
- EXISTING SANITARY SEWER
- EXISTING BUILDING
- EXISTING UNDERDRAIN
- NEW TOPSOILING, SEEDING, AND HEAVY-DUTY MULCHING
- NEW TOPSOILING, SEEDING, AND EROSION CONTROL BLANKET
- NEW INLET PROTECTION
- NEW SILT FENCE
- NEW DITCH CHECKS
- NEW CONTOUR
- 10 YEAR BASE FLOOD ELEVATION 638.26 (NGVD 29)
- 100 YEAR BASE FLOOD ELEVATION 640.46 (NGVD 29)
- ORDINARY HIGH WATER MARK (OHWM)
- 50' OFFSET FROM OHWM

IL CONTRACT: **PA066**
 IL LETTING ITEM: **04A**
 IL PROJECT: **PWK-5128**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS		
NUMBER	BY	DATE



**CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS
 STORM WATER POLLUTION PREVENTION PLAN - 1**

© Copyright CMT, Inc.

 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	23005747.00

FINAL

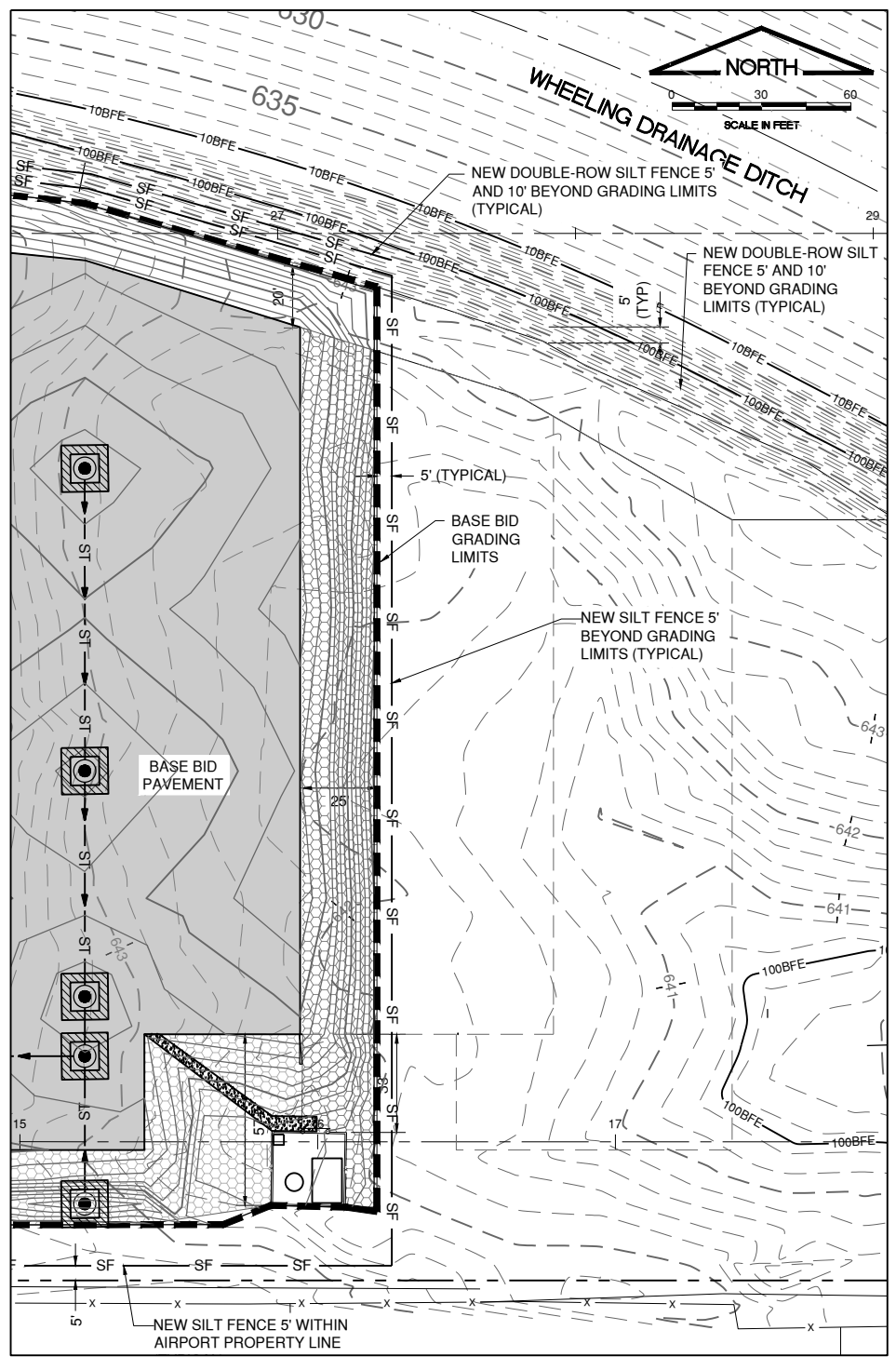
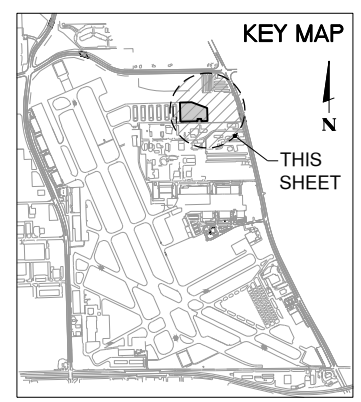
SHEET 8 OF 33 SHEETS

BASE BID + ADDITIVE ALTERNATE NO. 1 + ADDITIVE ALTERNATE NO. 2

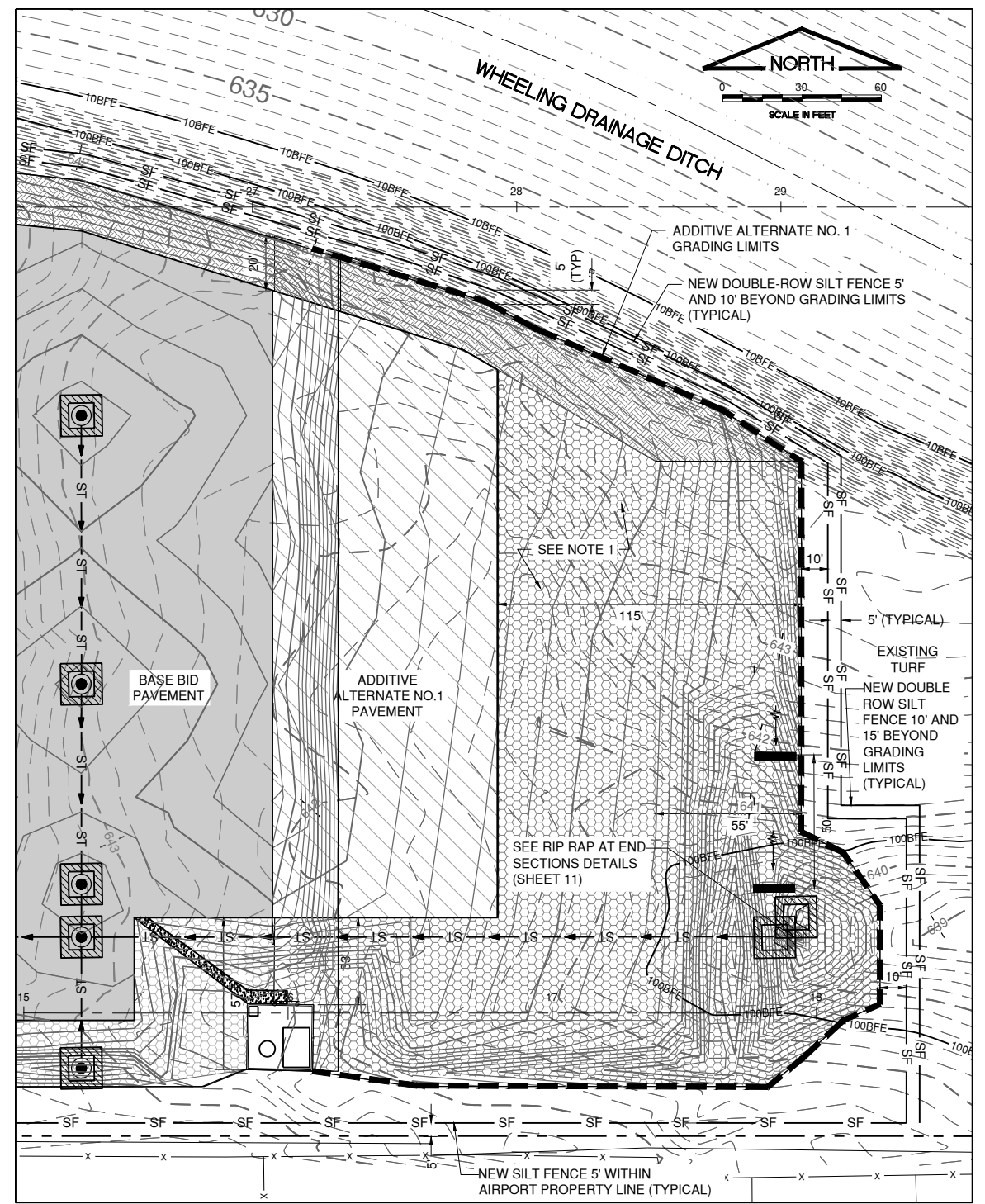
Right of way boundary shown in blue
 Airport property boundary shown in red
 Airport boundary shown in green

CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS

FILE: K:\ChicagoExecApo\23005747-00_NECad\Draw\Draw\Sheet\23005747-00_Swp-2.dwg



BASE BID ONLY



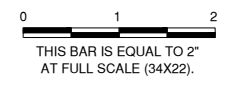
ADDITIVE ALTERNATE NO. 1 ONLY

- LEGEND**
- NEW PAVEMENT (BASE BID)
 - NEW PAVEMENT (ADD. ALT. NO. 1)
 - NEW PAVEMENT (ADD. ALT. NO. 2)
 - NEW MANHOLE/INLET
 - NEW STORM SEWER
 - EXISTING AIRFIELD FENCE
 - EXISTING CONTOUR
 - EXISTING GATE
 - EXISTING AIRPORT PROPERTY LIMITS
 - EXISTING STORM SEWER
 - EXISTING MANHOLE/INLET
 - EXISTING SANITARY SEWER
 - EXISTING BUILDING
 - EXISTING UNDERDRAIN
 - NEW TOPSOILING, SEEDING, AND HEAVY-DUTY MULCHING
 - NEW TOPSOILING, SEEDING, AND EROSION CONTROL BLANKET
 - NEW INLET PROTECTION
 - NEW SILT FENCE
 - NEW DITCH CHECKS
 - NEW CONTOUR
 - 10-YEAR BASE FLOOD ELEVATION 638.26 (NGVD 29)
 - 100-YEAR BASE FLOOD ELEVATION 640.46 (NGVD 29)
 - ORDINARY HIGH WATER MARK (OHWM)
 - 50' OFFSET FROM OHWM
 - NEW GRADING LIMITS

- NOTES**
1. CONTOURS SHOWN ARE FOR NEW ADDITIVE ALTERNATE NO. 2 PAVEMENT GRADES. THIS AREA SHALL HAVE 4" TOPSOILING PLACED ON NEW PAVEMENT SUBGRADE AND GRADE TO DRAIN TO NEW INFILTRATION TRENCH VOLUME CONTROL FACILITY

I.L. CONTRACT: **PA066**
 I.L. LETTING ITEM: **04A**
 I.L. PROJECT: **PWK-5128**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS		
NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS
STORM WATER POLLUTION PREVENTION PLAN - 2

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	23005747.00

FINAL

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE AT THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION:

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTING A NEW TIE-DOWN APRON AND TAXILANE ACCESS AT CHICAGO EXECUTIVE AIRPORT. THE PROJECT INCLUDES EXCAVATION, DRAINAGE, VARIOUS PAVEMENT ITEMS, TIE-DOWN INSTALLATION, PAVEMENT MARKING, AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

DESCRIPTION OF CONSTRUCTION ACTIVITY:

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL SUCH AS INLET PROTECTION.

EXCAVATION AND EMBANKMENT WILL BE COMPLETED WITHIN THE PROJECT LIMITS TO GRADE OUT FOR THE PROPOSED DRAINAGE AND PAVEMENT IMPROVEMENTS.

VOLUME CONTROL FACILITY CONSTRUCTION.

UNDERDRAIN INSTALLATION.

PAVEMENT CONSTRUCTION.

INSTALLATION OF NEW PAVEMENT MARKING AND TIE-DOWNS.

REMOVAL AND DISPOSAL OF TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES.

PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCHING.

AREA OF CONSTRUCTION SITE:

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 4.2 ACRES OF WHICH WILL BE DISTURBED BY GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE DES PLAINES RIVER THROUGH A STORM SEWER SYSTEM.

EROSION AND SEDIMENT CONTROL:

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, SOIL PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND ALL OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.

DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTIONS SHALL BE INSTALLED AS CALLED OUT IN THE PLAN AND DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN (14) DAYS.

THE DOWN STREAM SIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIER.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS WITHIN THE STAGING AREA. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT PERIODICALLY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2" OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE RESIDENT ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT THE EROSION AND SEDIMENT CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT, AND ANY OTHER POTENTIAL HAZARDOUS MATERIALS THAT MAY EXIST ONSITE.

NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

COST OF MAINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCLUDED INCLUDED IN THE UNIT BID PRICE FOR THE VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RE-SEEDED AND/OR SODDED.

MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

DOCUMENTATION:

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF INTENT (NOI)" PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL POST A SIGN OR OTHER NOTICE NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE. IF THIS IS NOT POSSIBLE, THEN IT MAY BE PERMITTED TO POST THIS NOTICE IN A LOCAL PUBLIC BUILDING. THE SIGN OR NOTICE MUST CONTAIN THE FOLLOWING:

1. A COPY OF THE COMPLETED NOTICE OF INTENT (NOI) AS SUBMITTED TO THE IEPA
2. THE LOCATION OF THE SWPPP AND NAME AND 24/7 TELEPHONE NUMBER OF THE CONTACT PERSON.

THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN AND UPDATE AN "AS-BUILT" SET OF STORM WATER POLLUTION PREVENTION PLANS IN THE PROJECT FILES. THE SWPPP SHALL BE UPDATED WITHIN 7-DAYS OF ANY MODIFICATIONS TO THE PLANS. THE SWPPP AND ALL REVISIONS SHALL BE RETAINED FOR THREE YEARS AFTER FINAL STABILIZATION OF THE SITE, WHICH SHALL BE DEFINED AS VEGETATION COVER OF AT LEAST 70% OF HISTORIC CONDITIONS.

A STORM WATER POLLUTION PREVENTION PLAN EROSION CONTROL INSPECTION REPORT (FORM BC 2259) SHALL BE BE COMPLETED WITH INSPECTION FREQUENCIES AS OUTLINED HEREIN. SWPPP REPORTS SHALL BE RETAINED FOR THREE YEARS AFTER THE DATE OF FINAL STABILIZATION AS DEFINED HEREIN.

IF ANY VIOLATION OF THE PROVISIONS OF THE PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION COVERED IN THIS PLAN, THE ENGINEER AND/OR CONTRACTOR SHALL COMPLETE AND FILE AN "INCIDENT OF NONCOMPLIANCE (ION)" REPORT FOR THE IDENTIFIED VIOLATION. THE FORMS SHALL BE AS PROVIDED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, AND SHALL INCLUDE SPECIFIC INFORMATION ON THE INCIDENT THAT CAUSED NONCOMPLIANCE, ACTIONS THAT WERE TAKEN TO CORRECT THE NONCOMPLIANCE AND TO PREVENT ITS REOCCURRENCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NONCOMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G. OF THE GENERAL PERMIT.

AFTER PROJECT FINAL ACCEPTANCE, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF TERMINATION (NOT)" FORM PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY. FORMS FOR THE IEPA SHALL BE MAILED TO THE FOLLOWING ADDRESS:

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
DIVISION OF WATER POLLUTION CONTROL, MAIL CODE #15
ATTN: PERMIT SECTION
1021 NORTH GRAND AVENUE EAST
P.O. BOX 19276
SPRINGFIELD, ILLINOIS 62794-9276

GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL:

1. ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
2. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, COOK COUNTY, CHICAGO EXECUTIVE AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

CONTRACTOR CERTIFICATION STATEMENT

THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PROJECT INFORMATION:

ROUTE: CHICAGO EXECUTIVE AIRPORT MARKED: CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS

SECTION: 13 PROJECT NUMBER: PWK-5128

COUNTY: COOK CONTRACT NUMBER: 3-17-SBGP-TBD (PA066)

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SIGNATURE: _____ DATE: _____

PRINTED NAME: _____ TITLE: _____

NAME OF FIRM: _____

STREET ADDRESS: _____

CITY, STATE, ZIP: _____

PHONE NUMBER: _____

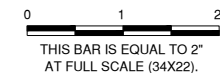
THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.

NPDES PERMIT #	_____
DATE ISSUED	_____
DATE EXPIRED	_____

IL CONTRACT: **PA066**
IL LETTING ITEM: **04A**
IL PROJECT: **PWK-5128**
S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS

NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS

**STORM WATER POLLUTION PREVENTION PLAN
GENERAL NOTES AND DETAILS - 1**

© Copyright CMT, Inc.



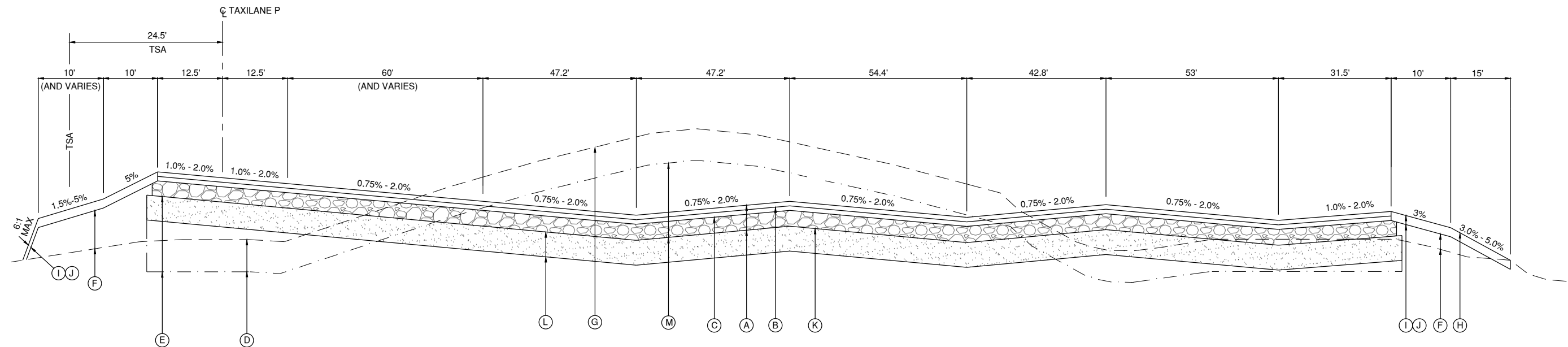
DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	23005747.00

FINAL

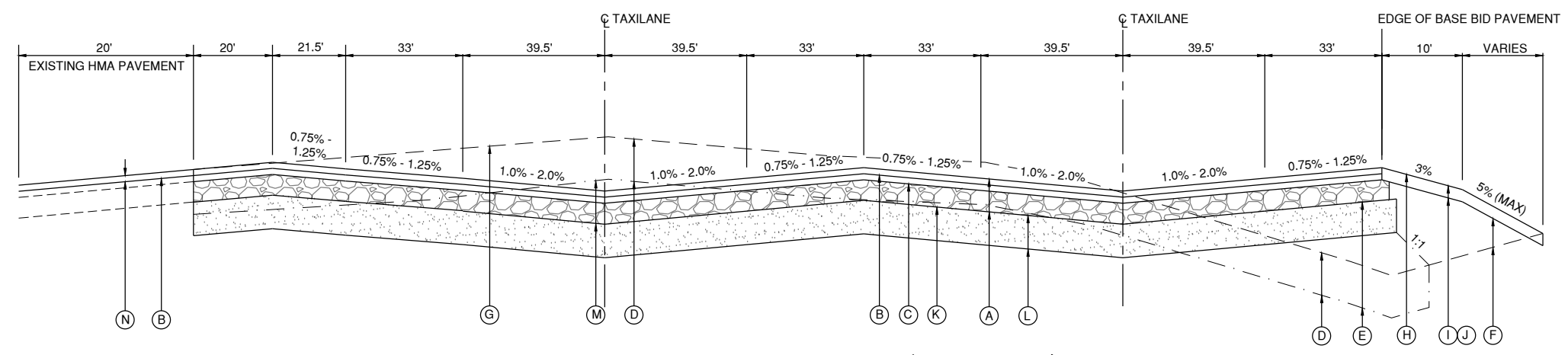
FILE: K:\ChicagoEnergy\300517-00_NEC\as\Draw\Draw\Sheet\300517-00_Swpg_Notes_And_Details - 1.dwg

B:\projects\10401\10401.dwg
 B:\projects\10401\10401.dwg
 B:\projects\10401\10401.dwg

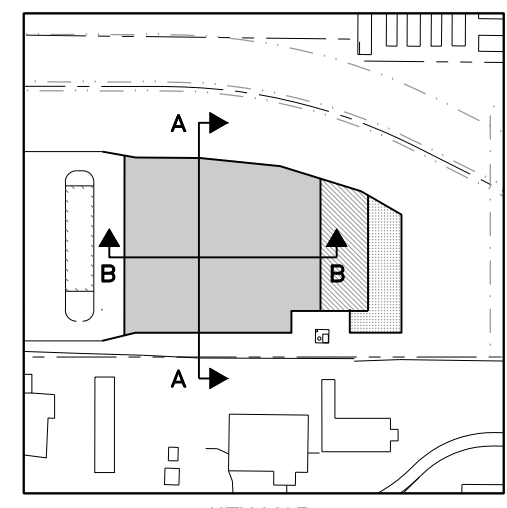
FILE: K:\ChicagoExecApo\23005747-00_NEC\Asst\Draw\Sheet\23005747-00_Typical Sections.dwg



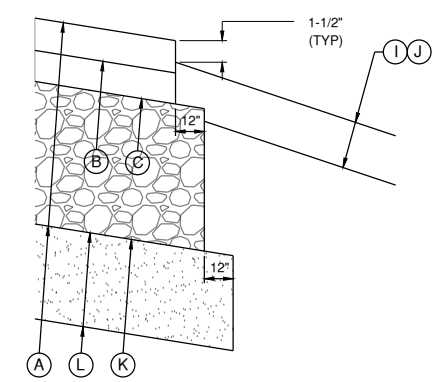
APRON TYPICAL SECTION A-A
NOT TO SCALE



APRON TYPICAL SECTION B-B (BASE BID ONLY)
NOT TO SCALE



KEY MAP



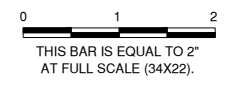
EDGE OF BITUMINOUS PAVEMENT AREAS
NOT TO SCALE

LEGEND

- (A) NEW 1 3/4" BITUMINOUS SURFACE COURSE (401)
NEW 2 1/4" BITUMINOUS BASE COURSE (403)
NEW 7" CRUSHED AGGREGATE BASE COURSE (209)
- (B) NEW TACK COAT (603)
- (C) NEW PRIME COAT (602)
- (D) NEW 12" (AVG.) TOPSOIL STRIPPING (152)
- (E) NEW EMBANKMENT FILL (152)
NEW ON-SITE BORROW AS NEEDED (152)
- (F) NEW SHOULDER FILL (152)
- (G) EXISTING GROUND LINE
- (H) NEW GROUND LINE
- (I) NEW TOPSOIL PLACEMENT (4" DEPTH) (905)
- (J) NEW SEEDING AND MULCHING (901 AND 908)
- (K) NEW SOIL STABILIZATION FABRIC (152)
- (L) NEW 12" LIME-MODIFIED SUBGRADE (155)
- (M) NEW UNCLASSIFIED EXCAVATION
- (N) NEW 2" BITUMINOUS SURFACE COURSE (401)
VARIABLE DEPTH MILLING (401)

IL CONTRACT: **PA066**
 IL LETTING ITEM: **04A**
 IL PROJECT: **PWK-5128**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS		
NUMBER	BY	DATE



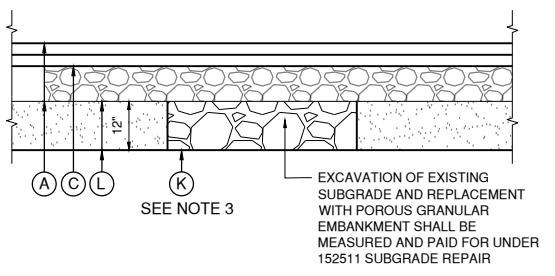
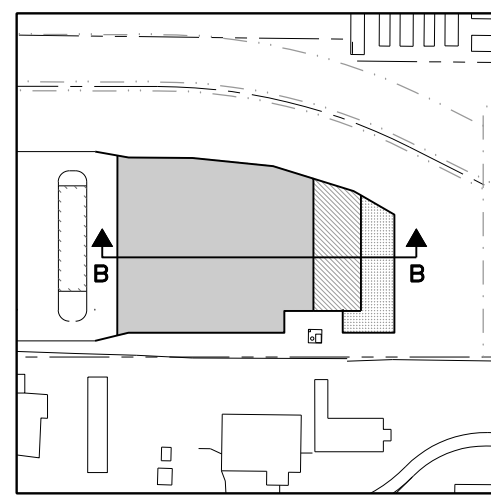
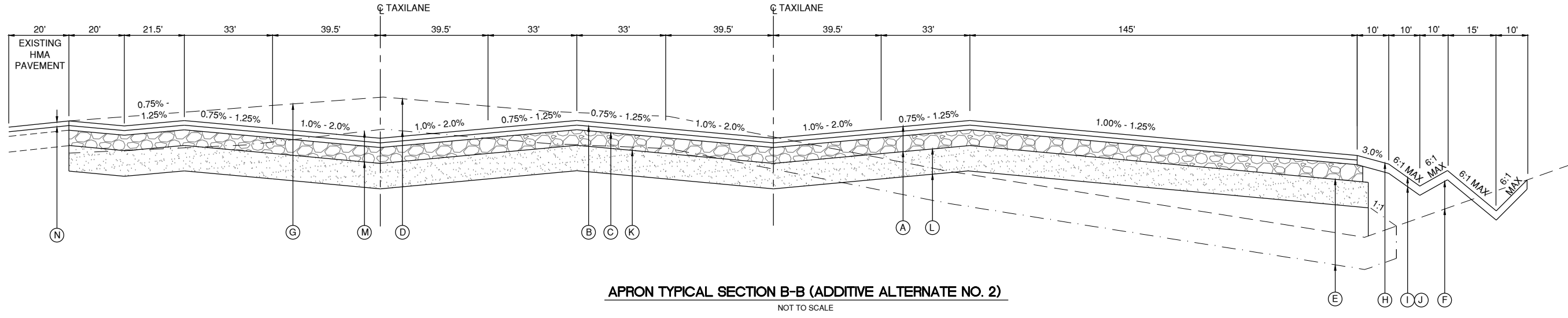
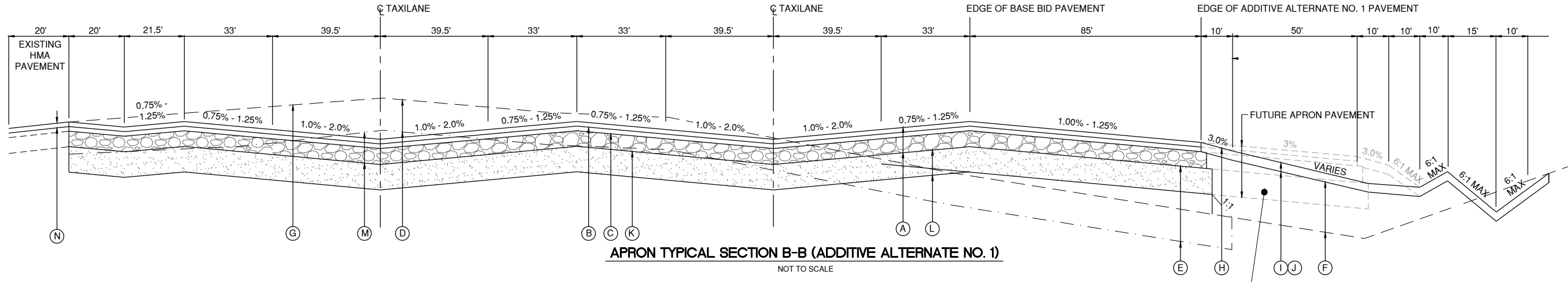
CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS
TYPICAL SECTIONS - 1

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613
CHICAGO EXECUTIVE AIRPORT

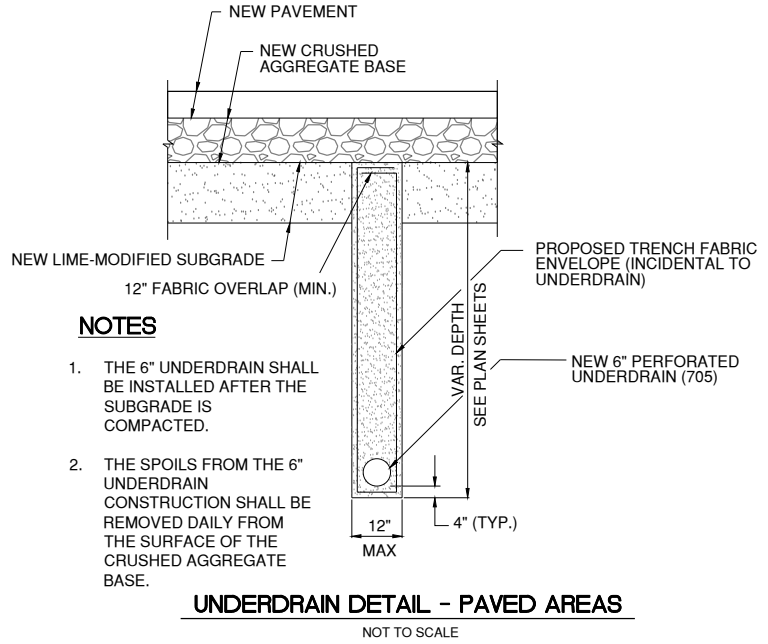
DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	23005747.00

FINAL

FILE: K:\Chicago\Enr\40230517-00_NEC\Add\Draw\Sheet\230517-00_Typical Sections.dwg
 C:\Users\jmurphy\OneDrive\Documents\Small\230517-00_Typical Sections.dwg
 230517-00_Typical Sections.dwg



- NOTES**
1. LOCATION OF SUBGRADE REPAIR SHALL BE DETERMINED AT THE TIME OF CONSTRUCTION AND APPROVED BY THE RESIDENT ENGINEER.
 2. MEASUREMENT AND PAYMENT SHALL BE MADE UNDER 152511 SUBGRADE REPAIR AND SHALL INCLUDE ALL EXCAVATION (SUBGRADE) AND FULL DEPTH REPLACEMENT WITH POROUS GRANULAR EMBANKMENT TO MEET THE FINAL GRADES OF THE ADJACENT SUBGRADE TO REMAIN.
 3. INSTALLATION OF SOIL STABILIZATION FABRIC FOR SUBGRADE REPAIR SHALL BE CONSIDERED INCIDENTAL TO THE PAY ITEM 152511.



NOTES

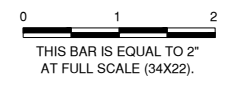
1. THE 6" UNDERDRAIN SHALL BE INSTALLED AFTER THE SUBGRADE IS COMPACTED.
2. THE SPOILS FROM THE 6" UNDERDRAIN CONSTRUCTION SHALL BE REMOVED DAILY FROM THE SURFACE OF THE CRUSHED AGGREGATE BASE.

LEGEND

- (A) NEW 1 3/4" BITUMINOUS SURFACE COURSE (401)
NEW 2 1/4" BITUMINOUS BASE COURSE (403)
NEW 7" CRUSHED AGGREGATE BASE COURSE (209)
- (B) NEW TACK COAT (603)
- (C) NEW PRIME COAT (602)
- (D) NEW 12" (AVG.) TOPSOIL STRIPPING (152)
- (E) NEW EMBANKMENT FILL (152)
NEW ON-SITE BORROW AS NEEDED (152)
- (F) NEW SHOULDER FILL (152)
- (G) EXISTING GROUND LINE
- (H) NEW GROUND LINE
- (I) NEW TOPSOIL PLACEMENT (4" DEPTH) (905)
- (J) NEW SEEDING AND MULCHING (901 AND 908)
- (K) NEW SOIL STABILIZATION FABRIC
- (L) NEW 12" LIME-MODIFIED SUBGRADE (155)
- (M) NEW UNCLASSIFIED EXCAVATION
- (N) NEW 2" BITUMINOUS SURFACE COURSE (401)
VARIABLE DEPTH MILLING (401)

IL CONTRACT: **PA066**
 IL LETTING ITEM: **04A**
 IL PROJECT: **PWK-5128**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS		
NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS
TYPICAL SECTIONS - 2

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

CHICAGO EXECUTIVE AIRPORT

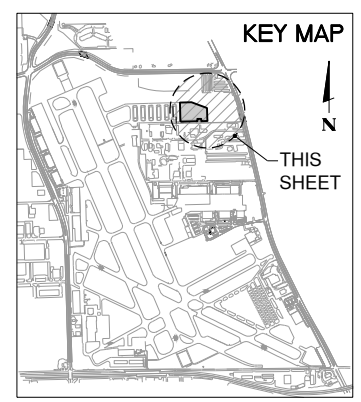
DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	23005747.00

FINAL

B:\Projects\2024\07\23005747_00_NEC\Quad\Draw\Sheet\23005747_00_NEC.dwg

B:\Projects\2024\07\23005747_00_NEC\Quad\Draw\Sheet\23005747_00_NEC.dwg

FILE: K:\ChicagoExecAir\23005747_00_NEC\Quad\Draw\Sheet\23005747_00_NEC.dwg



WHEELING DRAINAGE DITCH

WHEELING DRAINAGE DITCH



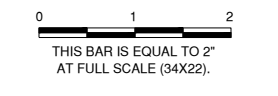
LEGEND

- NEW APRON/TAXILANE PAVEMENT (BASE BID)
- NEW APRON/TAXILANE PAVEMENT (ADDITIVE ALTERNATE NO. 1)
- NEW APRON/TAXILANE PAVEMENT (ADDITIVE ALTERNATE NO. 2)
- EXISTING AIRFIELD FENCE
- EXISTING GATE
- EXISTING AIRPORT PROPERTY LIMITS
- EXISTING BUILDING

IL CONTRACT: **PA066**
 IL LETTING ITEM: **04A**
 IL PROJECT: **PWK-5128**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS

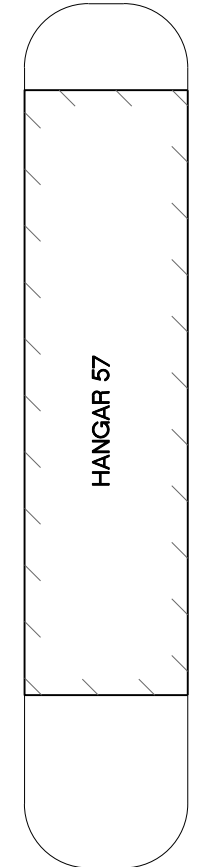
NUMBER	BY	DATE



22+00 TAXIWAY P 23+00 24+00 25+00 26+00 27+00 28+00 29+00

EXISTING HMA PAVEMENT

EXISTING TAXIWAY P



BASE BID

ADDITIVE ALTERNATE NO. 1

ADDITIVE ALTERNATE NO. 2

EXISTING TURF

POINT	STATION	OFFSET	NORTHING	EASTING
1	23+16.01	16.88' LT	1987506.86	617535.63
2	23+55.97	9.76' LT	1987499.75	617575.59
3	23+76.02	6.19' LT	1987496.18	617595.64
4	24+90.72	5.42' LT	1987495.40	617710.34
5	26+35.37	9.06' RT	1987480.91	617854.99
6	27+07.52	31.53' RT	1987458.44	617927.14
7	27+07.53	268.91' RT	1987221.07	617927.14
8	26+55.27	268.91' RT	1987221.06	617874.89
9	26+55.27	307.78' RT	1987182.19	617874.89
10	23+75.87	307.75' RT	1987182.23	617595.48
11	23+55.83	312.74' RT	1987177.24	617575.45
12	23+16.00	322.66' RT	1987167.33	617535.61
13	27+79.66	53.99' RT	1987435.97	617999.28
14	27+92.63	61.50' RT	1987428.47	618012.25
15	27+92.64	268.90' RT	1987221.07	618012.25
16	27+59.99	268.90' RT	1987221.07	617979.60
17	27+60.01	307.78' RT	1987182.19	617979.62
18	28+52.64	307.78' RT	1987182.19	618072.25
19	28+52.63	96.04' RT	1987393.93	618072.25

NOTE: STATION AND OFFSET REFERENCED FROM BASELINE TAXIWAY P. COORDINATES EXPRESSED IN NAD 27.

NEW 5' WIDE AGGREGATE ACCESS WALKWAY (BASE BID)

CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS

GEOMETRY PLAN

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY: STL
 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: DKP
 DATE: 07/29/2024
 JOB No: 23005747.00

FINAL

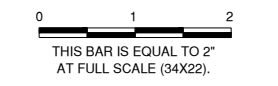
6mp uo0de au 'ad' Jup04

6df(2) jf8us'v0p0+V3C'CEA-C000Tha0kxkd

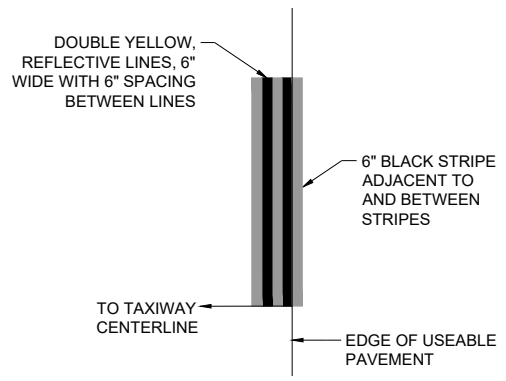
FILE: K:\ChicagoExecAp\23005747-00_NEC\asb\Draw\Sheet\23005747-00_Marking And Tie-Down Details.dwg

IL CONTRACT: PA066
IL LETTING ITEM: 04A
IL PROJECT: PWK-5128
S.B.G. PROJECT: 3-17-SBGP-TBD

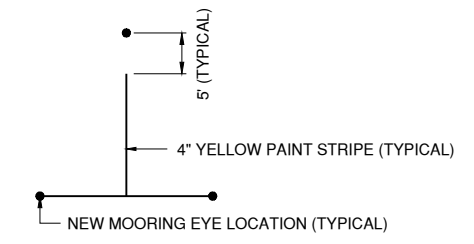
REVISIONS		
NUMBER	BY	DATE



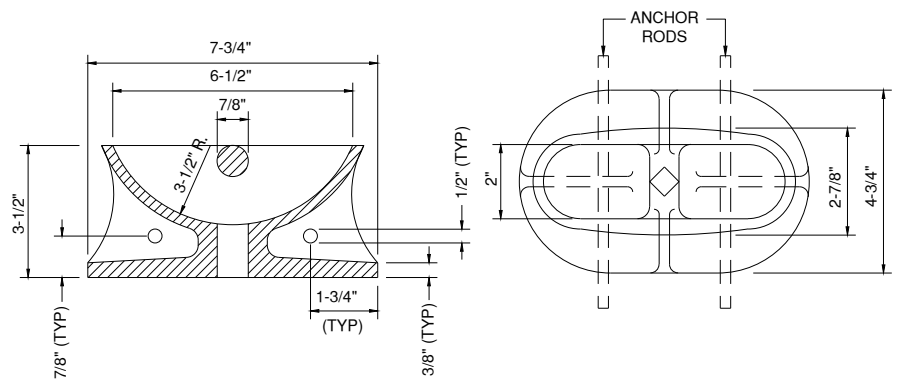
TAXIWAY / TAXILANE CENTERLINE DETAIL
NOT TO SCALE



CONTINUOUS TAXIWAY EDGE LINE MARKING
NOT TO SCALE



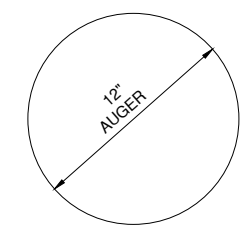
TIEDOWN DETAIL
NOT TO SCALE



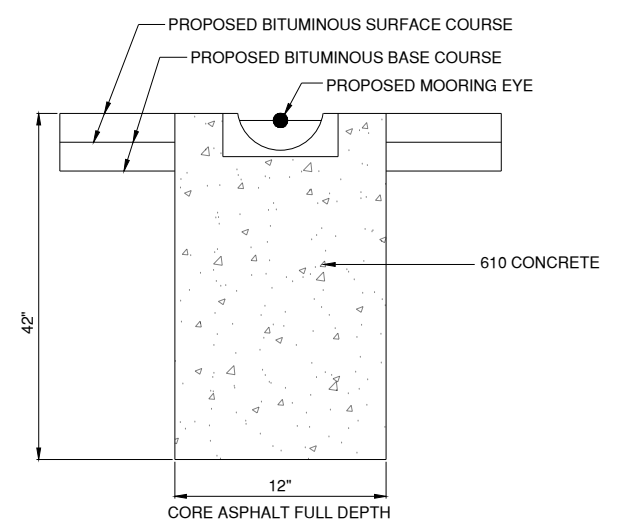
MOORING EYE DETAIL
NOT TO SCALE

NOTES

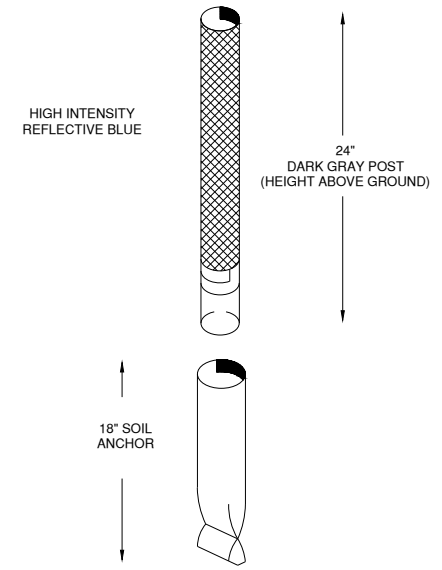
- MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. R3490 OR APPROVED EQUIVALENT.
- ANCHOR RODS TO BE NO. 3 DEFORMED REINFORCING STEEL 15" LONG AND SHALL BE BENT DOWNWARD AT 45°.



PLAN



SECTION
MOORING EYE INSTALLATION DETAIL
NOT TO SCALE



ELEVATED RETROREFLECTIVE MARKER DETAIL
NOT TO SCALE

NOTE: RETROREFLECTIVE MARKER SHALL MET FAA AC 150/5345-39 (LATEST EDITION) AIRFIELD LIGHTING EQUIPMENT CERTIFICATION PROGRAM

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS

PAVEMENT MARKING AND TIE-DOWN DETAILS

© Copyright CMT, Inc.
CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

CHICAGO EXECUTIVE AIRPORT

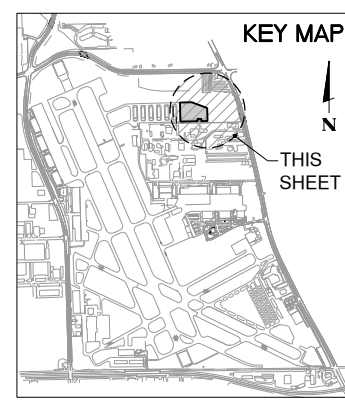
DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	23005747.00

FINAL

Right: 011 no grade and
011 no grade and
011 no grade and
011 no grade and

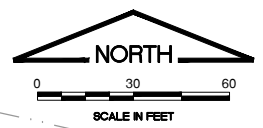
011 no grade and
011 no grade and
011 no grade and
011 no grade and

FILE: K:\Chicago\Exec\23005747-00_NEC\Draw\Draw\Sheets\23005747-00_Grading_Plan.dwg
011 no grade and
011 no grade and
011 no grade and
011 no grade and



- NOTES**
- IF NEITHER ADDITIVE ALTERNATE IS AWARDED, THEN THE NEW TURF GRADES AT THE EDGE OF NEW PAVEMENT SHALL SLOPE AWAY FROM THE PAVED EDGE AT 3% FOR 10', THEN SLOPE AWAY AT A MAXIMUM OF 5% UNTIL MEETING EXISTING GRADE. SEE TYPICAL SECTIONS.
 - IF ADDITIVE ALTERNATE NO. 1 IS AWARDED BUT NOT ADDITIVE ALTERNATE NO. 2, THEN THE TURF AREA BETWEEN THE ADDITIVE ALTERNATE NO. 1 EDGE OF PAVEMENT AND THE IN-TURF DRAINAGE STRUCTURES AT THE EAST PROJECT LIMIT SHALL BE GRADED TO DRAIN. THIS AREA SHALL HAVE 4" TOPSOILING PLACED ON NEW SUBGRADE PAVEMENT. SEE TYPICAL SECTIONS.

FLOODWAY/FLOODPLAIN RESTRICTIONS
 THE CONTRACTOR SHALL NOT PLACE FILL AT ANY TIME OUTSIDE OF THE GRADING LIMITS WITHIN THE EXISTING REGULATORY FLOODWAY/FLOODPLAIN. NOT FILL SHALL BE STOCKPILED WITHIN THE GRADING LIMITS WITHIN THE FLOODPLAIN AREA.



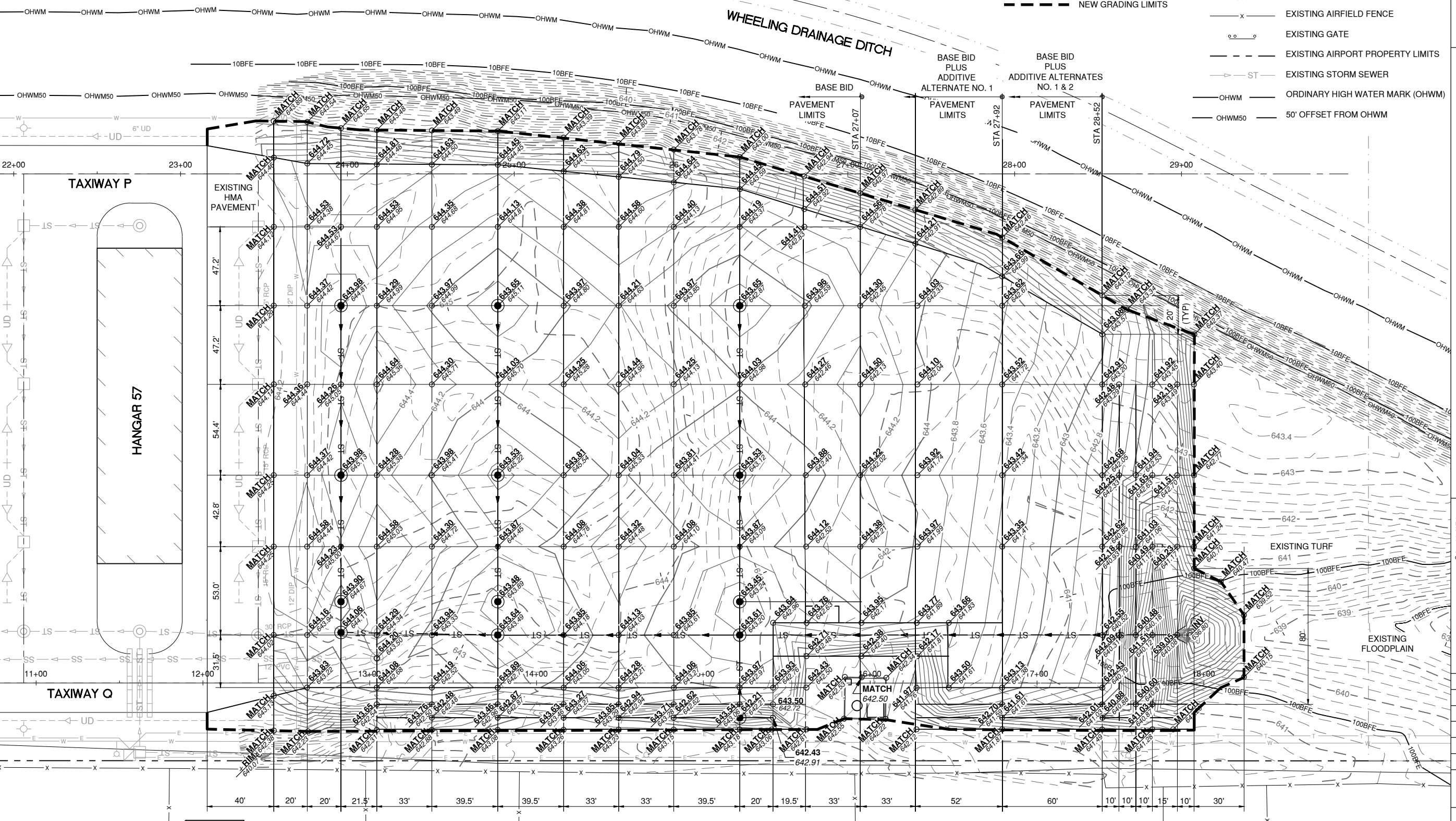
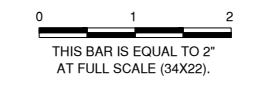
LEGEND

- NEW PAVEMENT
- NEW MANHOLE/INLET
- NEW STORM SEWER
- NEW UNDERDRAIN
- NEW CONTOUR
- NEW ELEVATION
- EXISTING ELEVATION
- 10YR BASE FLOOD ELEVATION (638.26 (NGVD 29))
- 100YR BASE FLOOD ELEVATION (640.46 (NGVD 29))
- NEW GRADING LIMITS
- EXISTING MANHOLE/INLET
- EXISTING SANITARY SEWER
- EXISTING BUILDING
- EXISTING WATERMAIN
- EXISTING UNDERDRAIN
- EXISTING ELECTRIC
- EXISTING TELECOMMUNICATION
- EXISTING DUCT BANK
- EXISTING FIRE HYDRANT
- EXISTING CONTOUR
- EXISTING AIRFIELD FENCE
- EXISTING GATE
- EXISTING AIRPORT PROPERTY LIMITS
- EXISTING STORM SEWER
- ORDINARY HIGH WATER MARK (OHWM)
- 50' OFFSET FROM OHWM

IL CONTRACT: **PA066**
 IL LETTING ITEM: **04A**
 IL PROJECT: **PWK-5128**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS

NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS

GRADING PLAN

© copyright CMT, Inc.

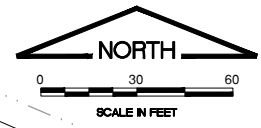
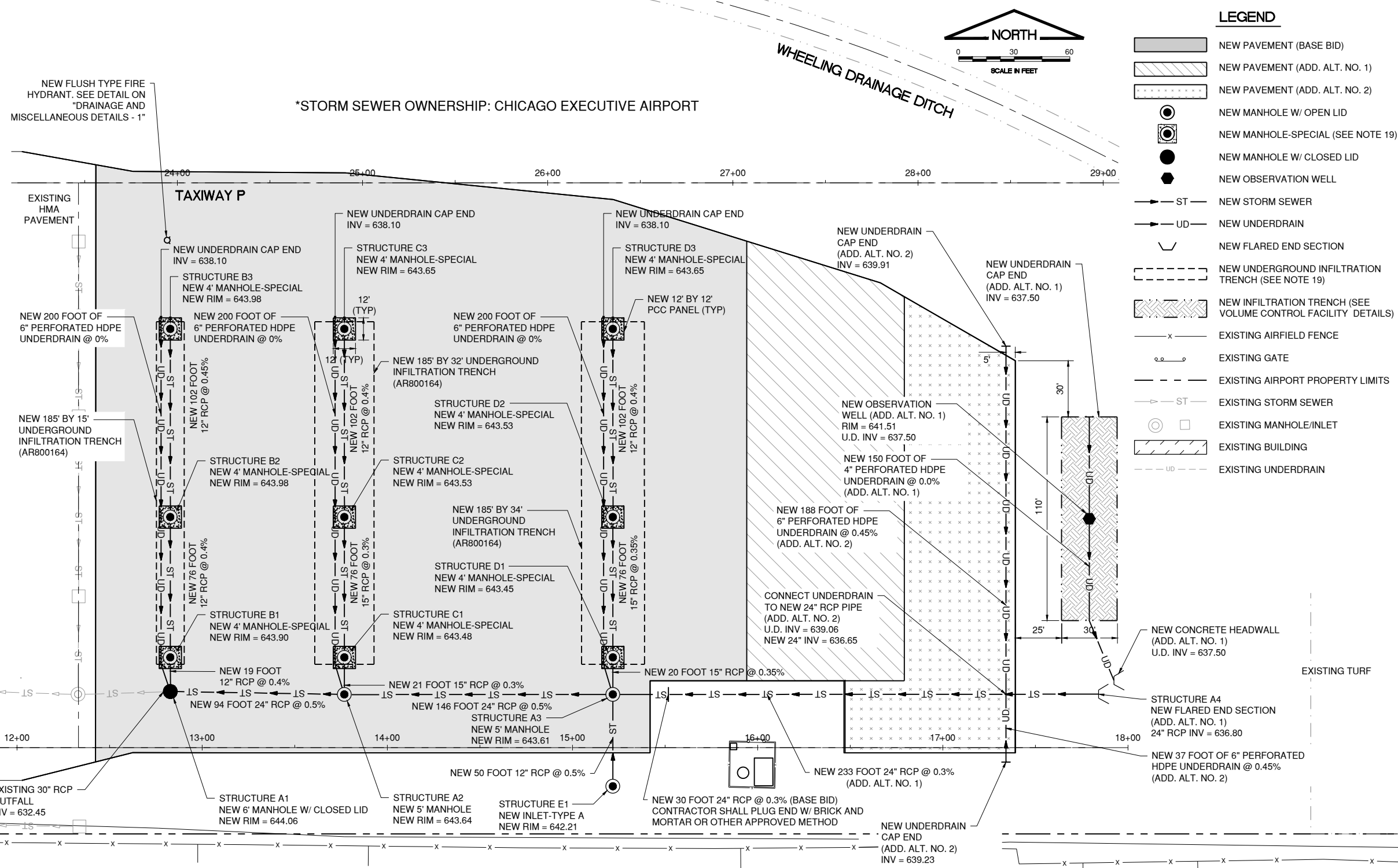
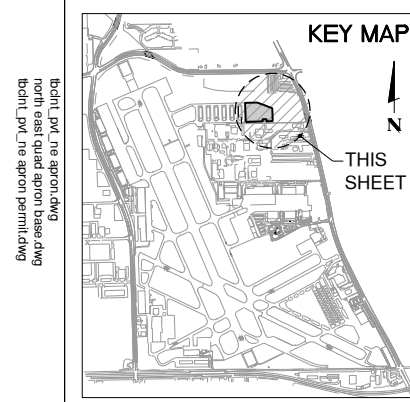
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	23005747.00

FINAL

SHEET 19 OF 33 SHEETS

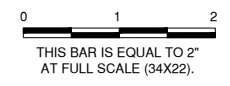


LEGEND

- NEW PAVEMENT (BASE BID)
- NEW PAVEMENT (ADD. ALT. NO. 1)
- NEW PAVEMENT (ADD. ALT. NO. 2)
- NEW MANHOLE W/ OPEN LID
- NEW MANHOLE-SPECIAL (SEE NOTE 19)
- NEW MANHOLE W/ CLOSED LID
- NEW OBSERVATION WELL
- NEW STORM SEWER
- NEW UNDERDRAIN
- NEW FLARED END SECTION
- NEW UNDERGROUND INFILTRATION TRENCH (SEE NOTE 19)
- NEW INFILTRATION TRENCH (SEE VOLUME CONTROL FACILITY DETAILS)
- EXISTING AIRFIELD FENCE
- EXISTING GATE
- EXISTING AIRPORT PROPERTY LIMITS
- EXISTING STORM SEWER
- EXISTING MANHOLE/INLET
- EXISTING BUILDING
- EXISTING UNDERDRAIN

IL. CONTRACT: **PA066**
 IL. LETTING ITEM: **04A**
 IL. PROJECT: **PWK-5128**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS		
NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS
DRAINAGE PLAN

DRAINAGE NOTES

1. DURING CONSTRUCTION OPERATIONS THE CONTRACTOR SHALL ENSURE POSITIVE SITE DRAINAGE AT THE CONCLUSION OF EACH DAY. SITE DRAINAGE MAY BE ACHIEVED BY DITCHING, PUMPING OR ANY OTHER METHOD ACCEPTABLE TO THE ENGINEER.
2. FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE DRAINAGE STRUCTURE COST.
3. CEMENT BRICKS AND NON-SHRINK MORTAR SHALL BE USED IN ALL STORM STRUCTURES FOR PIPE CONNECTIONS.
4. BEFORE ORDERING PIPE CULVERTS, STORM SEWER, INLETS OR MANHOLES, THE CONTRACTOR SHALL VERIFY THE DEPTH OF EXISTING DOWNSTREAM STRUCTURES OR PIPES FOR CONNECTION.
5. DURING CONSTRUCTION OPERATIONS, WHEN ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR DRAINAGE STRUCTURES IMPEDING THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY.
6. THE COST OF CONNECTING EXISTING STORM SEWERS AND REMOVAL OF EXISTING CONCRETE "BENCHES" TO THE PROPOSED DRAINAGE SYSTEM SHALL BE INCIDENTAL TO THE CONTRACT.
7. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS, OR CATCH BASINS. CONTRACTOR SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS. CONTRACTOR SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL PERMANENT CONNECTIONS WITH THE SEWERS ARE BUILT AND IN SERVICE. DISCHARGED WATER MUST BE FILTERED TO THE SATISFACTION OF THE ENGINEER AND SHALL BE VISIBLY FREE OF SEDIMENT. THIS WORK SHALL NOT BE PAID FOR DIRECTLY, BUT WILL BE INCLUDED IN THE COST OF STORM SEWER ITEMS BEING INSTALLED.
8. DRAINAGE STRUCTURE GRADES SHALL BE VERIFIED BY THE CONTRACTOR IN THE FIELD PRIOR TO INSTALLATION OF DRAINAGE ITEMS. GRADES OF EXISTING SEWER LINES WERE DETERMINED FROM AVAILABLE PLANS AND SURVEY. THE INVERTS OF THE PROPOSED DRAINAGE MAY REQUIRE REVISIONS TO MEET THE EXISTING FIELD CONDITIONS. ANY ADJUSTMENTS SHALL BE DIRECTED BY THE ENGINEER. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE PRICE OF THE PROPOSED STORM SEWER AND STRUCTURES.
9. BEFORE FINAL ACCEPTANCE OF THE PROJECT, ALL PROPOSED AND EXISTING STORM SEWER LINES AND STRUCTURES AFFECTED BY PROJECT LIMIT SHALL BE CLEANED AS DIRECTED BY THE ENGINEER. CLEANING OF THE PROPOSED AND EXISTING STORM SEWER LINES AND STRUCTURES IS CONSIDERED TO BE INCLUDED IN THE COST OF THE DRAINAGE ITEM.
10. WHEN STORM SEWER IS CONSTRUCTED NEAR WATER MAIN, SEPARATION REQUIREMENTS SHALL BE MET IN ACCORDANCE WITH THE ILLINOIS STANDARD SPECIFICATIONS FOR WATER AND SEWER CONSTRUCTION.
11. IF DURING MANHOLE ADJUSTMENTS, THE CONTRACTOR DAMAGES EITHER THE FRAME OR LID, THEY SHALL FURNISH A NEW FRAME AND LID, SAME OR EQUAL, AT NO ADDITIONAL COST. ALL NEW MANHOLES AND INLETS AND THOSE TO BE RECONSTRUCTED SHALL BE CONSTRUCTED WITH NEW FRAMES AND GRATES.
12. NEW MANHOLE AND INLET LIDS ON THIS PROJECT SHALL HAVE THE WORD "STORM" ON THE LID. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN THE APPROPRIATE TYPE OF LID. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THIS WORK.
13. THE CONTRACTOR SHALL CALL JULIE AT 811 OR 1-800-892-0123 48 HOURS PRIOR TO CONSTRUCTION FOR CONFIRMATION OF CURRENT UTILITY LOCATIONS AND FOR ALL NON-EMERGENCY WORK. THESE ARE THE KNOWN UTILITIES LOCATED WITHIN THE PROJECT LIMITS OR IMMEDIATELY ADJACENT TO THE PROJECT CONSTRUCTION LIMITS. UTILITIES WHICH ARE NOT MEMBERS OF JULIE SHOULD BE NOTIFIED INDIVIDUALLY BY THE CONTRACTOR.
14. ALL TESTING, FITTINGS, BEDDING AND GRANULAR CRADLE WHERE NECESSARY, SHALL BE INCLUDED IN THE INSTALLATION OF UNDERGROUND FACILITIES. TRENCH BACKFILL IS REQUIRED WHEREVER UNDERGROUND PIPING AND UTILITIES PASS BENEATH OR WITHIN 2 FEET OF THE PAVEMENT, SIDEWALK OR CURB.
15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRS TO ANY UTILITY LINES AND EXISTING IMPROVEMENTS TO REMAIN THAT ARE DAMAGED AS A RESULT OF THE WORK.
16. MANHOLE-SPECIAL IN CONJUNCTION WITH UNDERGROUND INFILTRATION TRENCH DETAILS AND DIMENSIONS ARE SHOWN ON VOLUME CONTROL FACILITY DETAILS SHEET. PCC PANELS SURROUNDING EACH MANHOLE-SPECIAL SHALL BE 12' BY 12'.

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

CHICAGO EXECUTIVE AIRPORT

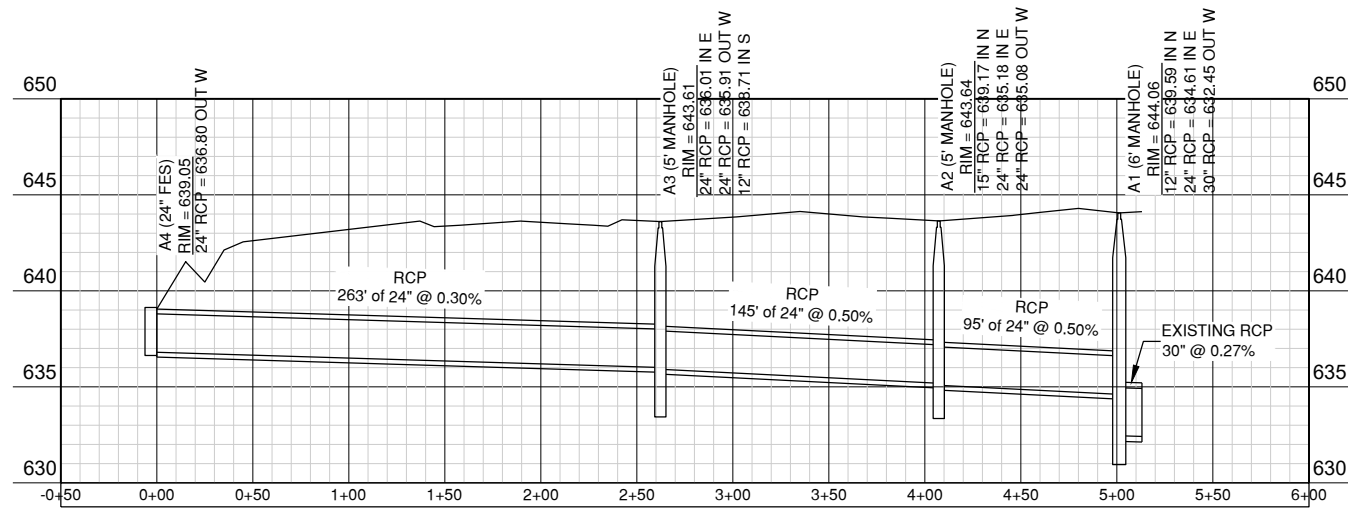
DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	23005747.00

FINAL

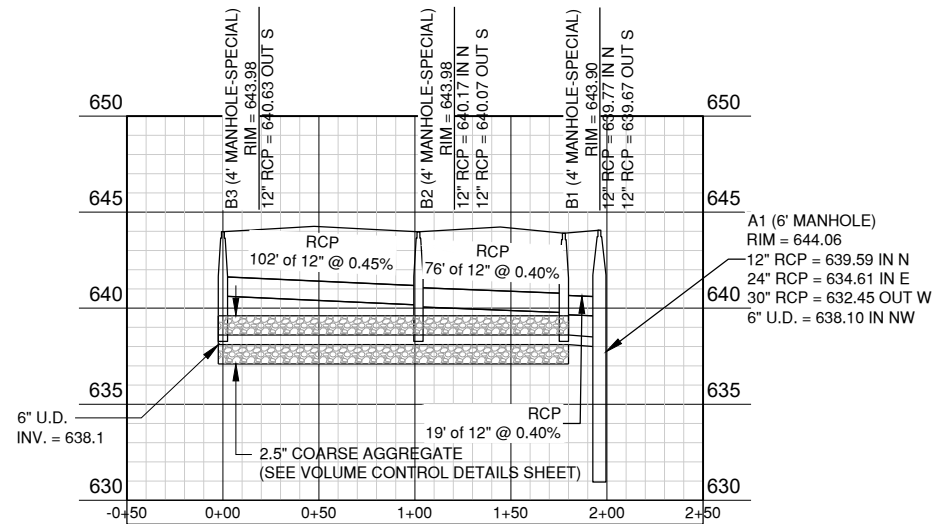
6/11/23 (2) 11:55:30 AM JAC_CDDTHAWKWD
 6/11/23 (2) 11:55:30 AM JAC_CDDTHAWKWD
 6/11/23 (2) 11:55:30 AM JAC_CDDTHAWKWD

6/11/23 (2) 11:55:30 AM JAC_CDDTHAWKWD
 6/11/23 (2) 11:55:30 AM JAC_CDDTHAWKWD
 6/11/23 (2) 11:55:30 AM JAC_CDDTHAWKWD

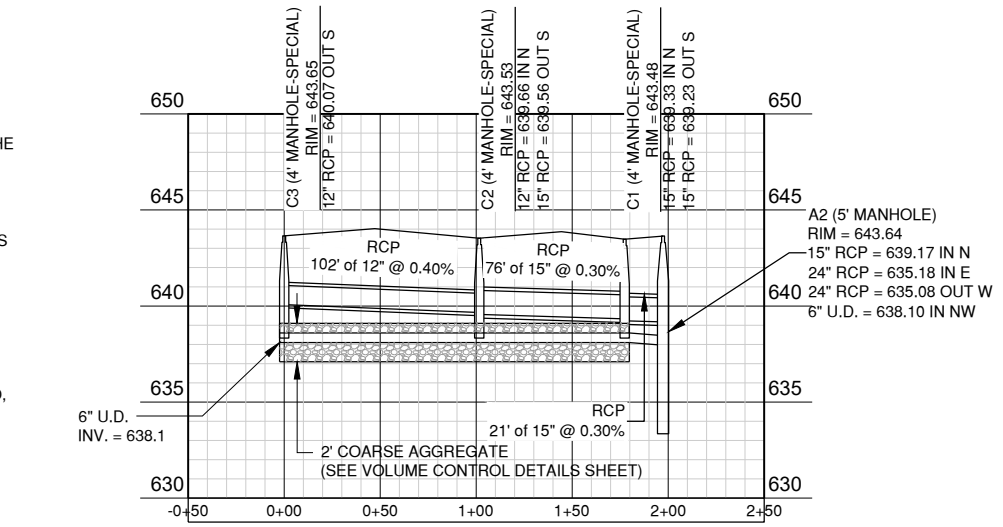
FILE: K:\ChicagoExecAirp\2300517-00_NEC\Draw\Draw\Sheet\2300517-00_Storm Sewer Profiles - 1.dwg
 6/11/23 (2) 11:55:30 AM JAC_CDDTHAWKWD
 6/11/23 (2) 11:55:30 AM JAC_CDDTHAWKWD
 6/11/23 (2) 11:55:30 AM JAC_CDDTHAWKWD



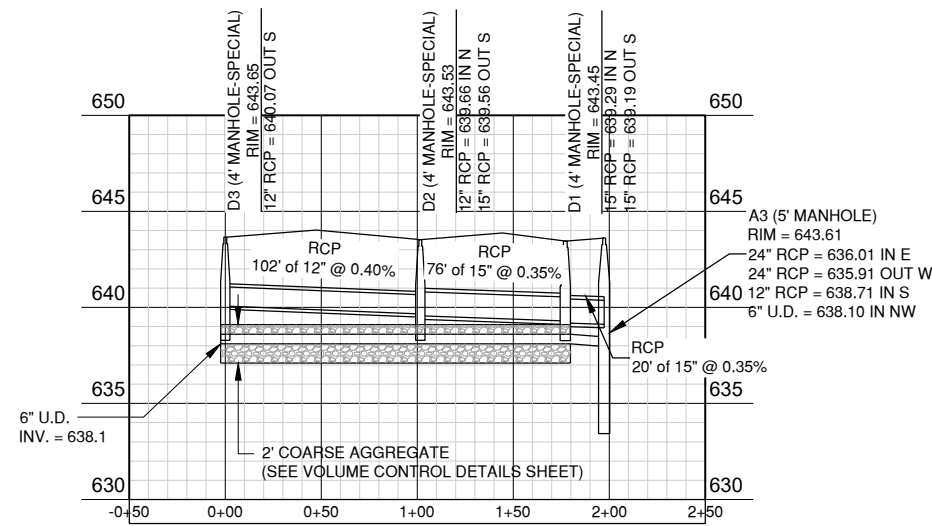
TRUNK A PROFILE



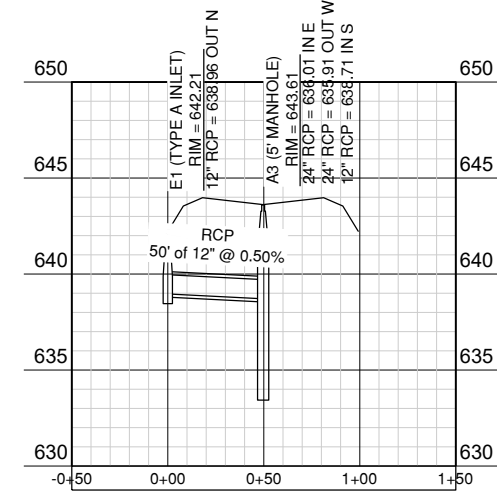
LATERAL B PROFILE



LATERAL C PROFILE



LATERAL D PROFILE



LATERAL E PROFILE

STRUCTURE SCHEDULE

STRUCTURE	TYPE	RIM	INVERT	STATION (OFFSET)
A1	NEW 6" MANHOLE WITH TYPE 1 FRAME AND CLOSED LID	644.06	EXISTING 30" OUT (W) = 632.45 12" IN (N) = 639.591 24" IN (E) = 634.61 6" UD IN (NW) = 638.00	STA. 23+96.24, 274.76 RT. BASELINE CENTERLINE TAXIWAY P
A2	NEW 5" MANHOLE WITH TYPE 1 FRAME AND CLOSED LID	643.64	24" OUT (W) = 635.08 15" IN (N) = 639.171 24" IN (E) = 635.18 6" UD IN (NW) = 638.00	STA. 24+90.18, 276.26 RT. BASELINE CENTERLINE TAXIWAY P
A3	NEW 5" MANHOLE WITH TYPE 1 FRAME AND CLOSED LID	643.61	24" OUT (W) = 635.911 15" IN (N) = 639.12 12" IN (S) = 638.71 24" IN (E) = 636.011 6" UD IN (NW) = 638.00	STA. 26+35.21, 276.26 RT. BASELINE CENTERLINE TAXIWAY P
A4	NEW FLARED END SECTION		24" OUT (W) = 636.8	STA. 28+82.50, 276.14' RT. BASELINE CENTERLINE TAXIWAY P
B1	NEW 4" MANHOLE-SPECIAL WITH TYPE 1 FRAME AND OPEN LID	643.9	12" OUT (S) = 639.667 12" IN (N) = 639.767	STA. 23+96.24, 256.26 RT. BASELINE CENTERLINE TAXIWAY P
B2	NEW 4" MANHOLE-SPECIAL WITH TYPE 1 FRAME AND OPEN LID	643.98	12" OUT (S) = 640.071 12" IN (N) = 640.171	STA. 23+96.24, 180.50 RT. BASELINE CENTERLINE TAXIWAY P
B3	NEW 4" MANHOLE-SPECIAL WITH TYPE 1 FRAME AND OPEN LID	643.98	12" OUT (S) = 640.63	STA. 23+96.24, 78.94 RT. BASELINE CENTERLINE TAXIWAY P
C1	NEW 4" MANHOLE-SPECIAL WITH TYPE 1 FRAME AND OPEN LID	643.48	15" OUT (S) = 639.234 15" IN (N) = 639.334	STA. 24+90.18, 256.26 RT. BASELINE CENTERLINE TAXIWAY P
C2	NEW 4" MANHOLE-SPECIAL WITH TYPE 1 FRAME AND OPEN LID	643.53	15" OUT (S) = 639.562 12" IN (N) = 639.662	STA. 24+90.18, 180.50 RT. BASELINE CENTERLINE TAXIWAY P
C3	NEW 4" MANHOLE-SPECIAL WITH TYPE 1 FRAME AND OPEN LID	643.65	12" OUT (S) = 640.07	STA. 24+90.18, 78.94 RT. BASELINE CENTERLINE TAXIWAY P
D1	NEW 4" MANHOLE-SPECIAL WITH TYPE 1 FRAME AND OPEN LID	643.45	15" OUT (S) = 639.19 15" IN (N) = 639.29	STA. 26+35.21, 256.26 RT. BASELINE CENTERLINE TAXIWAY P
D2	NEW 4" MANHOLE-SPECIAL WITH TYPE 1 FRAME AND OPEN LID	643.53	15" OUT (S) = 639.562 12" IN (N) = 639.662	STA. 26+35.21, 180.50 RT. TAXIWAY P
D3	NEW 4" MANHOLE-SPECIAL WITH TYPE 1 FRAME AND OPEN LID	643.65	12" OUT (S) = 640.07	STA. 26+35.21, 78.93 RT. BASELINE CENTERLINE TAXIWAY P
E1	NEW INLET-TYPE A WITH TYPE 8 GRATE	642.21	12" OUT (N) = 638.96	STA. 26+35.21, 325.86' RT. BASELINE CENTERLINE TAXIWAY P

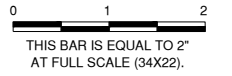
STRUCTURE SCHEDULE NOTES

1. THE STATION AND OFFSET IS MEASURED TO THE CENTER OF THE FRAME OF THE STRUCTURE.
2. ALL ELEVATIONS ARE IN 1929 DATUM.
3. LENGTH OF PIPE FOR MANHOLE TO MANHOLE IS FROM CENTER OF STRUCTURE.
4. RCP: REINFORCED CONCRETE CIRCULAR PIPE, CLASS IV.
5. CONTRACTOR SHALL VERIFY RIM AND INVERT ELEVATIONS ON EXISTING DRAINAGE STRUCTURES THAT ARE TO BE CONNECTED TO, ADJUSTED OR TO RECONSTRUCTED BEFORE ORDERING MATERIAL (INCIDENTAL TO CONTRACT).

IL CONTRACT: **PA066**
 IL LETTING ITEM: **04A**
 IL PROJECT: **PWK-5128**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS

NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS

STORM SEWER PROFILES

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

CHICAGO EXECUTIVE AIRPORT

DESIGN BY: STL
 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: DKP
 DATE: 07/29/2024
 JOB No: 23005747.00

FINAL

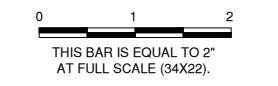
6mp:\projects\23112\Draw\Sheets\23112.dwg

6mp:\projects\23112\Draw\Sheets\23112.dwg

6mp:\projects\23112\Draw\Sheets\23112.dwg

IL CONTRACT: PA066
IL LETTING ITEM: 04A
IL PROJECT: PWK-5128
S.B.G. PROJECT: 3-17-SBGP-TBD

REVISIONS		
NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS
 GEOTECHNICAL ENGINEERING INFORMATION

© Copyright CMT, Inc.

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	2300547.00

FINAL

Geo Services, Inc.
Geotechnical Engineering & Civil Engineering
805 Airport Center, Suite 204
Naperville, Illinois 60563
Phone: 630.206.8888

GSI Job No. 23112
Page 1 of 1
Date 12/13/23

SOIL BORING LOG

PROJECT Chicago Executive Airport
LOCATION Wheeling/City of Prospect Heights, IL
COUNTY Cook DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

CLIENT CMT

BORING NO. B-02
Northing 1987348
Easting 1101943
Ground Surface Elev. 645.1 ft

DEPTH (ft)	SOIL DESCRIPTION	DRILLING METHOD	HAMMER TYPE	BLOWS	BLOWS/FEET	REMARKS
0	12.0" TOPSOIL-black			26		
6	CLAY LOAM-dark brown and gray-very stiff (CL-ML)			4.00	19	
7				4		
8				3.50	12	
9				7		
10				8	2.50	19
11				10		
12				4		
13				3	1.50	21
14				3		
15				3		
16						
17						
18						
19						
20						

End of Boring @ -10.0' Boring backfilled with cuttings.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASH#2006), GP-Geoprobe Hand Auger BBS, from 137 (Rev. 8-99)

Geo Services, Inc.
Geotechnical Engineering & Civil Engineering
805 Airport Center, Suite 204
Naperville, Illinois 60563
Phone: 630.206.8888

GSI Job No. 23112
Page 1 of 1
Date 12/13/23

SOIL BORING LOG

PROJECT Chicago Executive Airport
LOCATION Wheeling/City of Prospect Heights, IL
COUNTY Cook DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

CLIENT CMT

BORING NO. B-01
Northing 1987506
Easting 1101965
Ground Surface Elev. 645.0 ft

DEPTH (ft)	SOIL DESCRIPTION	DRILLING METHOD	HAMMER TYPE	BLOWS	BLOWS/FEET	REMARKS
0	12.0" TOPSOIL-black			33		
9	SILTY CLAY LOAM-brown and gray-hard (CL-ML)			4.50	13	
10				12		
11				7	3.50	13
12				5		
13				12	4.00	23
14				15		
15				5		
16				4	2.00	19
17				7		
18						
19						
20						

End of Boring @ -10.0' Boring backfilled with cuttings.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASH#2006), GP-Geoprobe Hand Auger BBS, from 137 (Rev. 8-99)

Geo Services, Inc.
Geotechnical Engineering & Civil Engineering
805 Airport Center, Suite 204
Naperville, Illinois 60563
Phone: 630.206.8888

GSI Job No. 23112
Page 1 of 1
Date 12/13/23

SOIL BORING LOG

PROJECT Chicago Executive Airport
LOCATION Wheeling/City of Prospect Heights, IL
COUNTY Cook DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

CLIENT CMT

BORING NO. B-07
Northing 1987449
Easting 1102168
Ground Surface Elev. 642.4 ft

DEPTH (ft)	SOIL DESCRIPTION	DRILLING METHOD	HAMMER TYPE	BLOWS	BLOWS/FEET	REMARKS
0	12.0" TOPSOIL-black			26		
7	SILTY CLAY with Gravel-brown-very stiff (CL-ML)			2.50	13	
8				5		
9				7	4.00	21
10				10		
11				4		
12				5	3.50	18
13				6		
14				6		
15				5	4.50	17
16				11		
17						
18						
19						
20						

End of Boring @ -10.0' Boring backfilled with cuttings.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASH#2006), GP-Geoprobe Hand Auger BBS, from 137 (Rev. 8-99)

Geo Services, Inc.
Geotechnical Engineering & Civil Engineering
805 Airport Center, Suite 204
Naperville, Illinois 60563
Phone: 630.206.8888

GSI Job No. 23112
Page 1 of 1
Date 12/13/23

SOIL BORING LOG

PROJECT Chicago Executive Airport
LOCATION Wheeling/City of Prospect Heights, IL
COUNTY Cook DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

CLIENT CMT

BORING NO. B-05
Northing 1987440
Easting 1101965
Ground Surface Elev. 645.4 ft

DEPTH (ft)	SOIL DESCRIPTION	DRILLING METHOD	HAMMER TYPE	BLOWS	BLOWS/FEET	REMARKS
0	12.0" TOPSOIL-black			23		
11	SILTY CLAY-brown-hard (CL-ML)			4.50	12	
12				8		
13				7	4.00	13
14				9		
15				7		
16				8	4.50	15
17				8		
18				4		
19				7	4.50	15
20				8		

End of Boring @ -10.0' Boring backfilled with cuttings.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASH#2006), GP-Geoprobe Hand Auger BBS, from 137 (Rev. 8-99)

Geo Services, Inc.
Geotechnical Engineering & Civil Engineering
805 Airport Center, Suite 204
Naperville, Illinois 60563
Phone: 630.206.8888

GSI Job No. 23112
Page 1 of 1
Date 12/13/23

SOIL BORING LOG

PROJECT Chicago Executive Airport
LOCATION Wheeling/City of Prospect Heights, IL
COUNTY Cook DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

CLIENT CMT

BORING NO. B-04
Northing 1987269
Easting 1102205
Ground Surface Elev. 641.9 ft

DEPTH (ft)	SOIL DESCRIPTION	DRILLING METHOD	HAMMER TYPE	BLOWS	BLOWS/FEET	REMARKS
0	12.0" TOPSOIL-black			19		
9	SILTY CLAY with Sand-brown & gray-stiff (CL-ML)			3.50	18	
10				7		
11				4	2.00	21
12				6		
13				3	3.00	18
14				4		
15				4		
16				6		
17				5		
18						
19						
20						

End of Boring @ -10.0' Boring backfilled with cuttings.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASH#2006), GP-Geoprobe Hand Auger BBS, from 137 (Rev. 8-99)

Geo Services, Inc.
Geotechnical Engineering & Civil Engineering
805 Airport Center, Suite 204
Naperville, Illinois 60563
Phone: 630.206.8888

GSI Job No. 23112
Page 1 of 1
Date 12/13/23

SOIL BORING LOG

PROJECT Chicago Executive Airport
LOCATION Wheeling/City of Prospect Heights, IL
COUNTY Cook DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

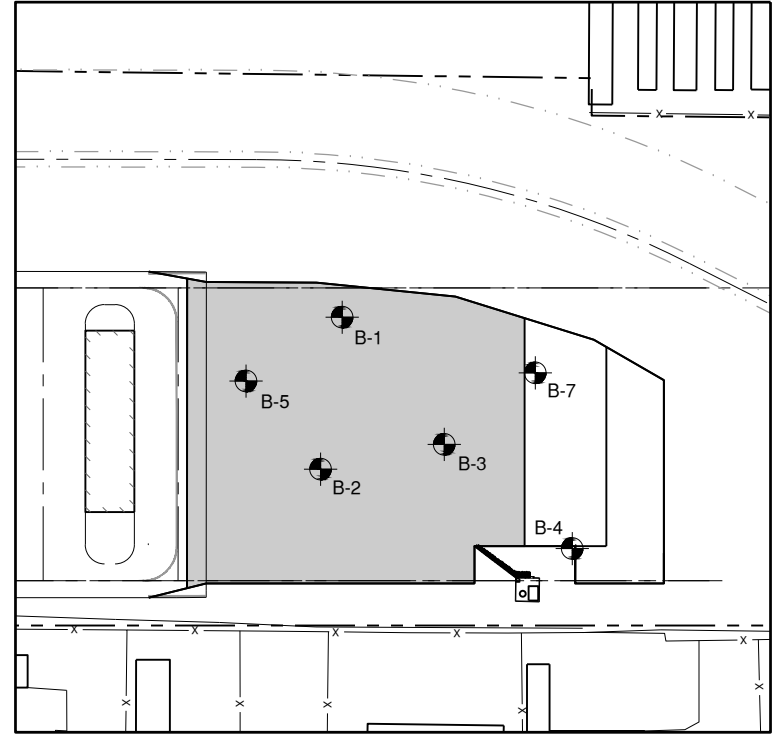
CLIENT CMT

BORING NO. B-03
Northing 1987374
Easting 1102072
Ground Surface Elev. 643.6 ft

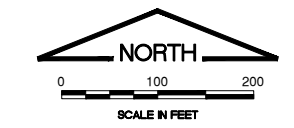
DEPTH (ft)	SOIL DESCRIPTION	DRILLING METHOD	HAMMER TYPE	BLOWS	BLOWS/FEET	REMARKS
0	12.0" TOPSOIL-black			33		
19	SAND & GRAVEL with Clay-brown-dense			19		
20				4		
21				7	4.00	17
22				7		
23				3		
24				7	3.00	18
25				6		
26				3		
27				10	3.00	16
28				7		
29				7		
30						
31						
32						

End of Boring @ -10.0' Boring backfilled with cuttings.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASH#2006), GP-Geoprobe Hand Auger BBS, from 137 (Rev. 8-99)



THE GEOTECHNICAL INFORMATION (OR ANY PORTIONS THEREOF) ARE PROVIDED ONLY AS AVAILABLE INFORMATION. THE CONTRACTOR MAY DRAW THEIR OWN CONCLUSIONS FROM THE DATA SHOWN. THE SOILS INFORMATION IS NOT REPRESENTATIVE OF ALL SOIL WHICH MIGHT BE ENCOUNTERED WITHIN THE LIMITS OF THE PROJECT. THE CONTRACTOR SHALL BY THEIR OWN MEANS, SATISFY THEMSELVES AS TO THE EXISTING SITE AND GEOTECHNICAL CONDITIONS FOR DETERMINING COST, MEANS, METHODS, TECHNIQUES AND SEQUENCES OF CONSTRUCTION.



LEGEND

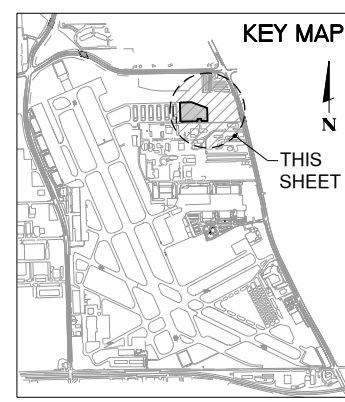
NEW HMA APRON

PROPOSED BORING

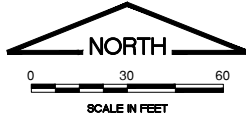
04/11/2024 10:45 AM
 04/11/2024 10:45 AM
 04/11/2024 10:45 AM

04/11/2024 10:45 AM
 04/11/2024 10:45 AM
 04/11/2024 10:45 AM

FILE: K:\ChicagoExecAirport\30057400_NEC\Draw\Sheet\30057400_Index_To_Cross_Sections.dwg
 04/11/2024 10:45 AM
 04/11/2024 10:45 AM
 04/11/2024 10:45 AM



WHEELING DRAINAGE DITCH

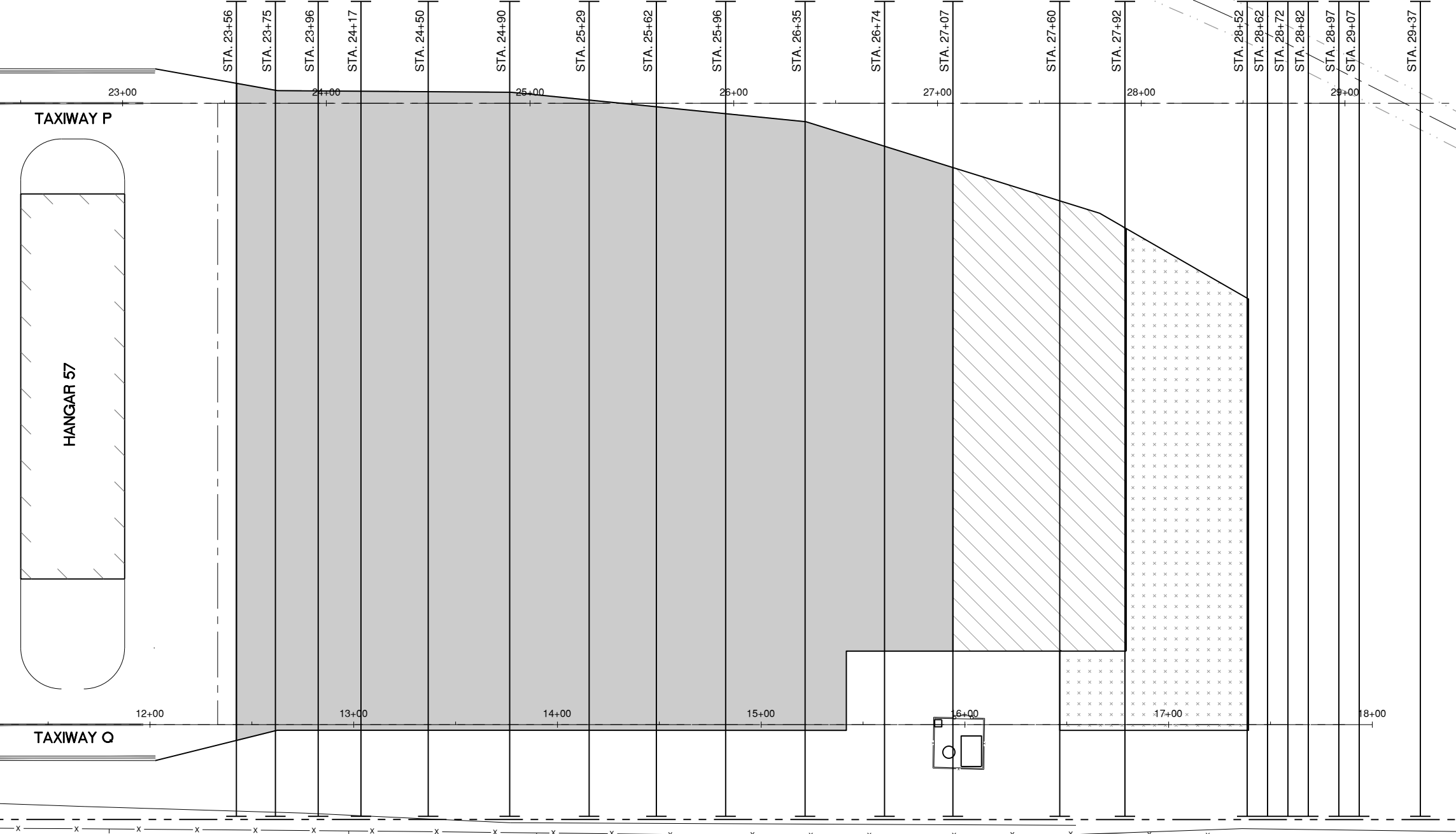


LEGEND

	NEW PAVEMENT (BASE BID)
	NEW PAVEMENT (ADDITIVE ALTERNATE NO. 1)
	NEW PAVEMENT (ADDITIVE ALTERNATE NO. 2)
	EXISTING AIRPORT PROPERTY LIMITS
	EXISTING BUILDING

GENERAL EARTHWORK NOTES:

- ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON THE MATERIAL IN ITS INITIAL OR FINAL POSITION AS SHOWN IN THE PLANS AND QUANTIFIED BY THE METHOD OF AVERAGE END AREAS.
- AREAS OF UNSUITABLE MATERIAL (UNCLASSIFIED EXCAVATION) SHALL BE AS DESIGNATED BY THE ENGINEER. THE QUANTITY OF UNSUITABLE MATERIAL SHALL NOT BE USED AS EMBANKMENT FILL MATERIAL, UNLESS AUTHORIZED BY THE ENGINEER.
- PAYMENT FOR UNCLASSIFIED EXCAVATION IS THE SUM OF TOPSOIL STRIPPING AND UNCLASSIFIED EXCAVATION AREAS.
- PAYMENT FOR ON-SITE BORROW SHALL BE MEASURED FOR AS FINAL POSITION. LOCATION OF ON-SITE BORROW IS SHOWN ON INDEX TO SHEETS AND SUMMARY OF QUANTITIES SHEET.
- THE CONTRACTOR SHALL ENSURE THAT 4 INCHES OF TOPSOIL CAN BE SPREAD OVER THE LIMITS OF THE GRADED AREA. IN SOME CASES, CONTRACTOR MAY BE REQUIRED TO OVER-EXCAVATE TO PROVIDE THE REQUIRED 4-INCH TOPSOIL LAYER. THE EARTHWORK QUANTITIES SHOWN INCLUDE THE REQUIRED OVER-EXCAVATION AND ARE SHOWN IN THE CROSS SECTIONS.
- TOPSOIL PLACEMENT, EMBANKMENT FILL AND SHOULDER FILL SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR UNCLASSIFIED EXCAVATION. NO SEPARATE PAYMENT WILL BE MADE FOR TOPSOIL PLACEMENT, EMBANKMENT FILL AND SHOULDER FILL.
- ANY EXCESS MATERIAL INCLUDING CLAY, UNSUITABLE MATERIAL, UNCLASSIFIED EXCAVATION AND TOPSOIL SHALL BE HAULED OFF AIRPORT PROPERTY AND DISPOSED OF BY THE CONTRACTOR.
- ALL CCDD AND ENVIRONMENTAL TESTING AND ANY TESTING AND HANDLING REQUIREMENT BY THE CONTRACTOR AND/OR CONTRACTOR'S DISPOSAL FACILITY(S) FOR ALL HAULED OFF MATERIALS, SHALL BE COMPLETED BY THE CONTRACTOR AND AT THE CONTRACTORS EXPENSE.
- IF THE CONTRACTOR ENCOUNTERS ANY SOIL FROM THIS SITE/PROJECT THAT IS POTENTIALLY CONTAMINATED, THE ENGINEER AND OWNER SHALL BE NOTIFIED PRIOR TO HAULING THE POTENTIALLY CONTAMINATED SOIL OFF SITE. THE CONTRACTOR SHALL PROVIDE P.I.D. METER RESULTS AND REQUIRED LAB TEST RESULTS TO DETERMINE THE POTENTIAL CONTAMINANT(S) (INCIDENTAL). PRE-CONSTRUCTION TESTING DID NOT INDICATE ANY SOURCES OF CONTAMINATED MATERIALS.
- EARTH STORM SEWER SPOILS AND INFILTRATION TRENCH EXCAVATION SPOILS CAN BE USED AS EMBANKMENT FILL AND COMPACT TO GRADE. THE PLACEMENT AND COMPACTION OF EARTH STORM SEWER SPOILS AND INFILTRATION TRENCH EXCAVATION SPOILS SHALL BE CONSIDERED INCIDENTAL.



EARTHWORK SUMMARY TABLE

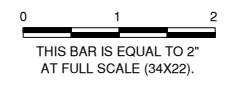
SCOPE OF WORK	TOPSOIL STRIPPING INITIAL POSITION (CUBIC YARD)	TOPSOIL PLACEMENT FINAL POSITION (CUBIC YARD)	SHOULDER FILL FINAL POSITION (CUBIC YARD)	UNCLASSIFIED EXCAVATION INITIAL POSITION (CUBIC YARD)	EMBANKMENT FILL FINAL POSITION (CUBIC YARD)	ON-SITE BORROW FINAL POSITION (CUBIC YARD)
BASE BID ONLY	4,350	180	360	1,490	1,860	530
BASE BID PLUS ADD. ALT. NO. 1	6,370	770	260	1,560	4,440	3,040
BASE BID PLUS ADD. ALT. NO. 1 AND 2	6,370	370	260	1,560	4,440	3,040

NOTES:
ON-SITE BORROW QUANTITY (FINAL POSITION) IS CALCULATED WITH 10% SHRINKAGE FACTOR INCORPORATED TO THE UNCLASSIFIED EXCAVATION QUANTITY (INITIAL POSITION). NO ADDITIONAL COST WILL BE PAID FOR DIFFERENCE IN ACTUAL SHRINKAGE FACTOR.

IL. CONTRACT: **PA066**
 IL. LETTING ITEM: **04A**
 IL. PROJECT: **PWK-5128**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS

NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS
INDEX TO CROSS SECTIONS
AND EARTHWORK SUMMARY

© Copyright CMT, Inc.

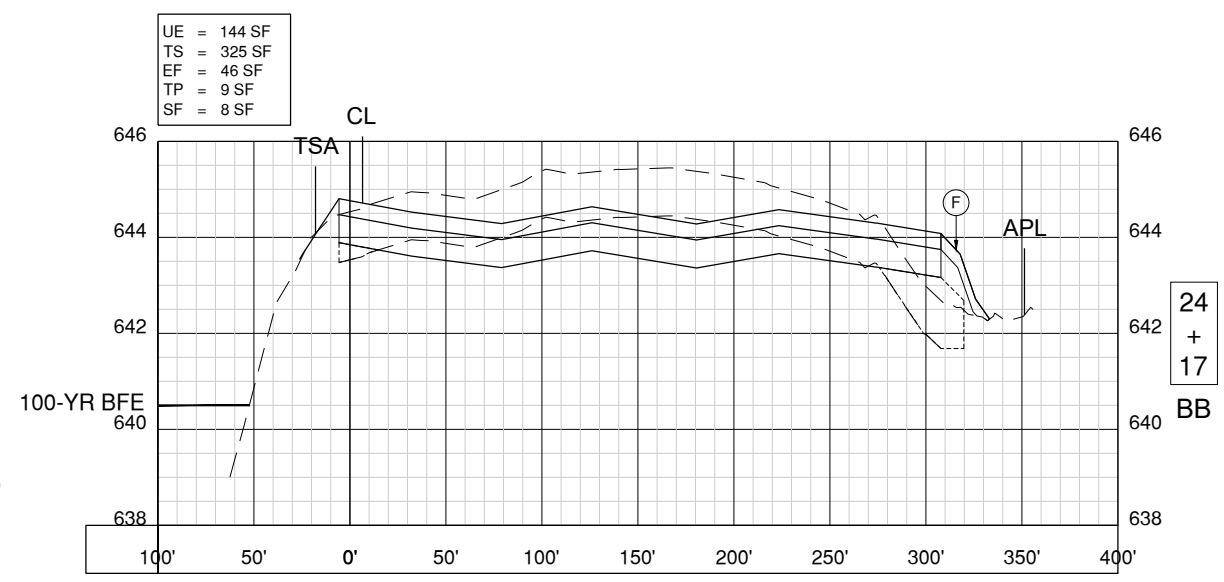
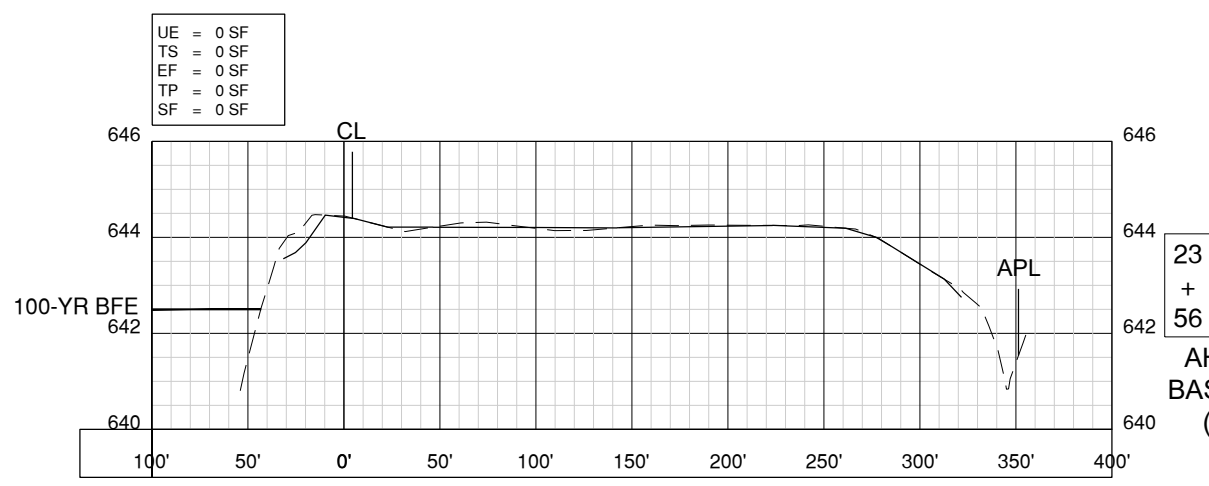
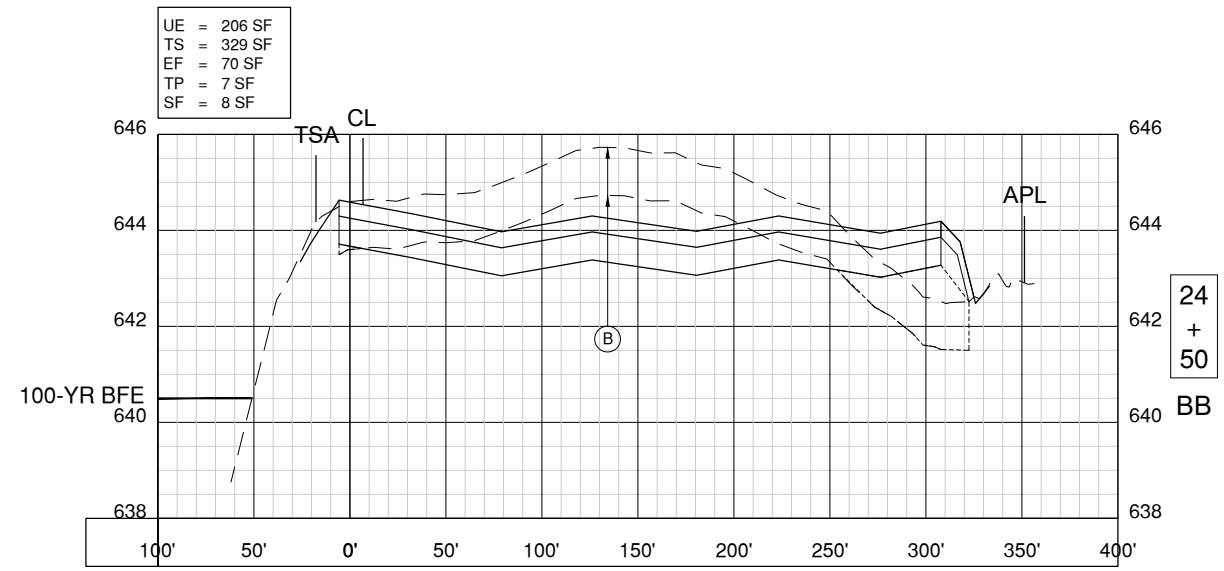
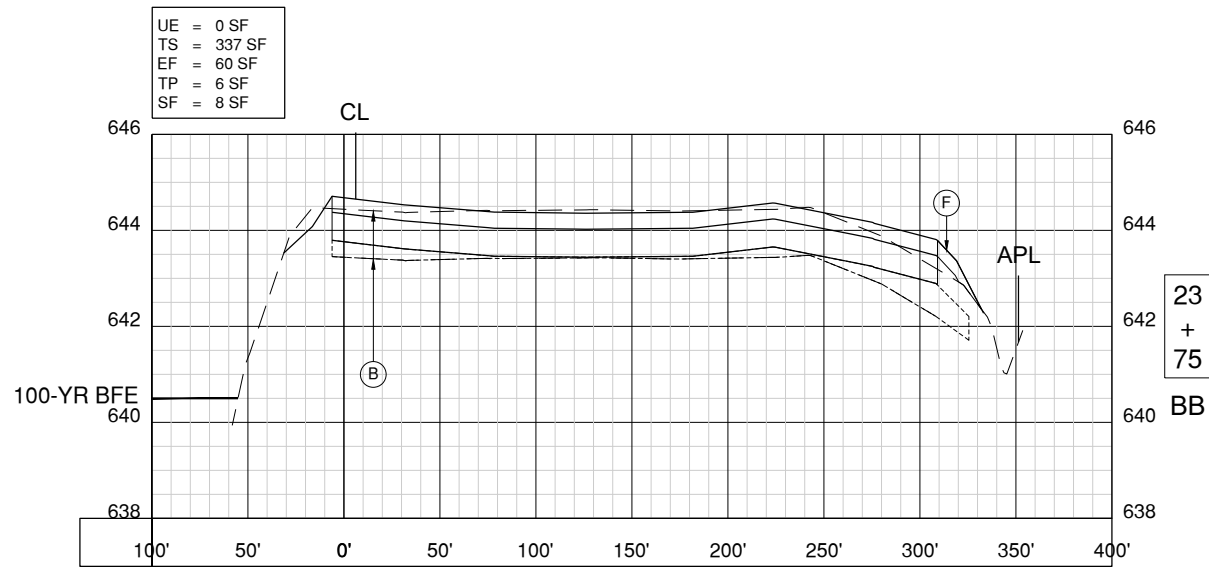
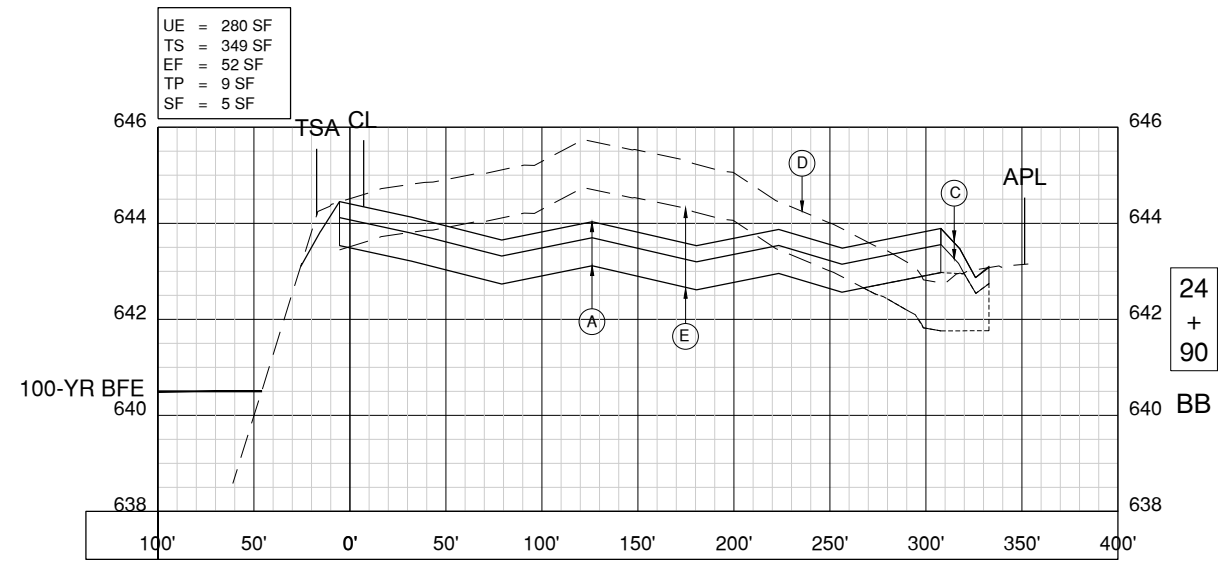
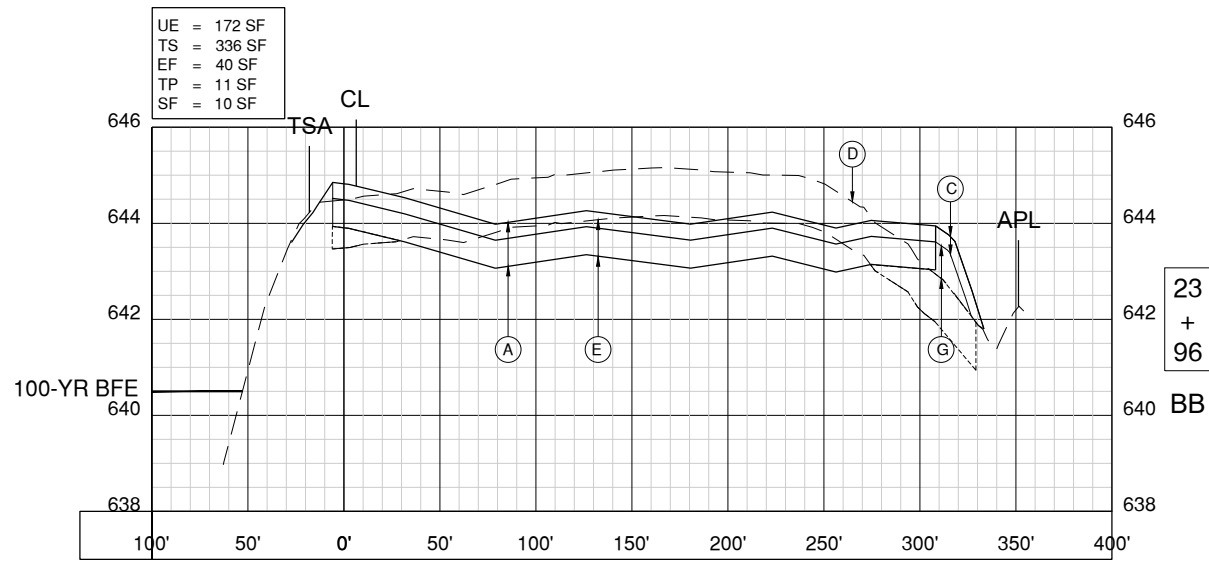
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	23005747.00

FINAL

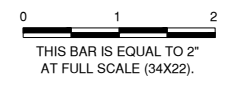
FILE: K:\ChicagoExecA\23005747-00_NEC\Draw\Draw\Sheet\2300574700_Cross Sections.dwg
 C:\Program Files\Autodesk\AutoCAD 2024\AutoCAD.exe
 2/2/2024 10:00:00 AM
 23 + 96



SEE CROSS SECTIONS - 4 FOR NOTES AND LEGEND

IL CONTRACT: **PA066**
 IL LETTING ITEM: **04A**
 IL PROJECT: **PWK-5128**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS		
NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS
CROSS SECTIONS - 1

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613
CHICAGO EXECUTIVE AIRPORT

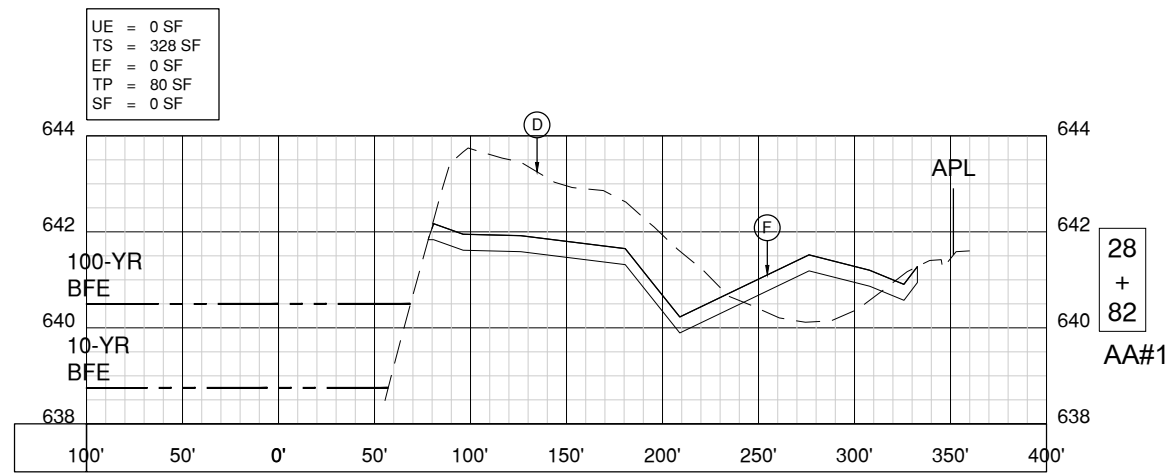
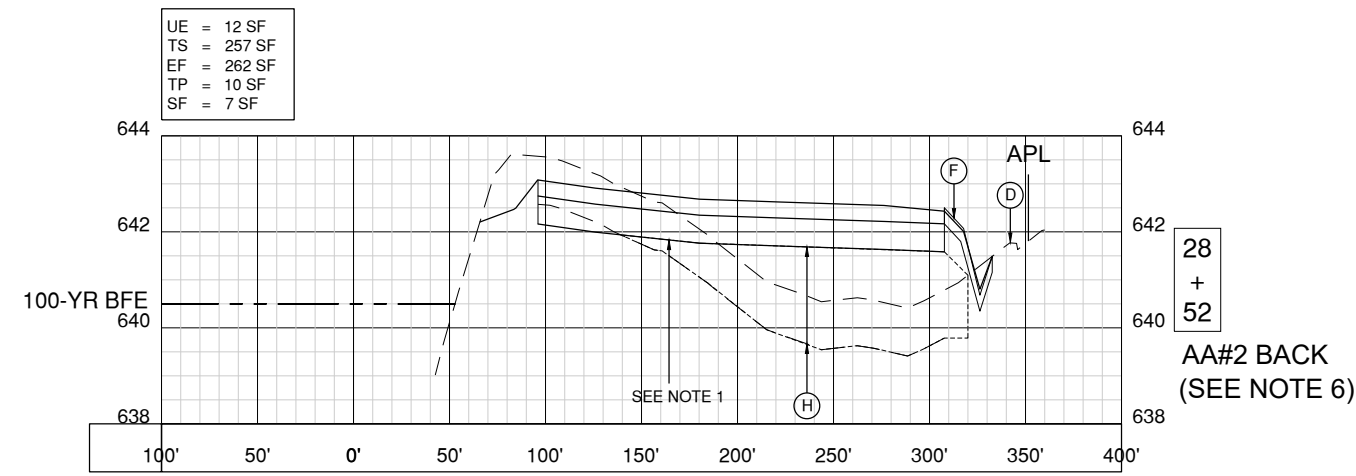
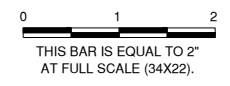
DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	23005747.00

FINAL

FILE: K:\ChicagoExecApt\23005747-00_NEC\Draw\Draw\Sheet\23005747-00_Cross Sections.dwg
 C:\Program Files\Autodesk\AutoCAD 2024\AutoCAD.exe
 2024/07/29 10:00:00 AM
 User: jro
 Plot Device: HP DesignJet T1100e
 Plot Date: 2024/07/29 10:00:00 AM
 Plot Time: 00:00:00

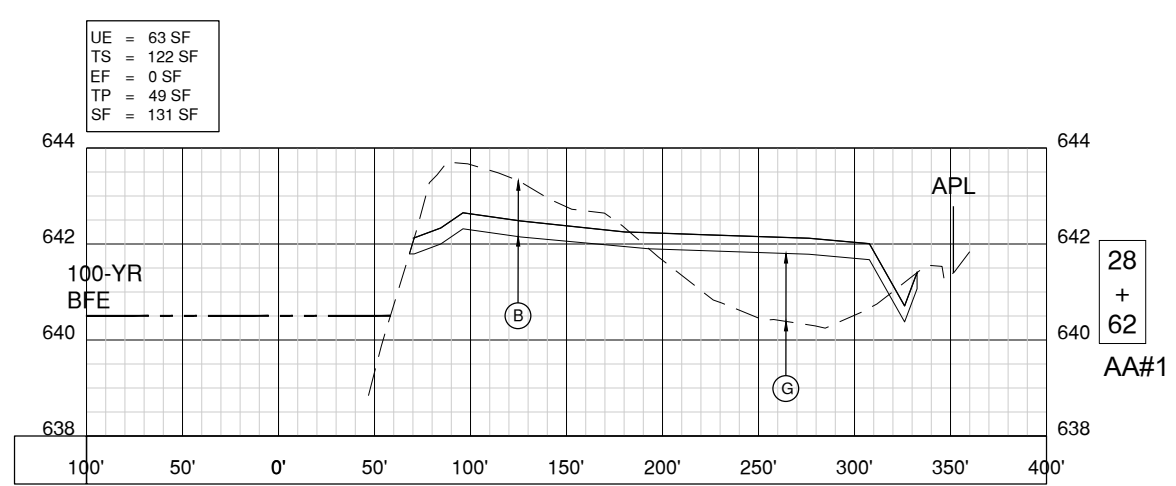
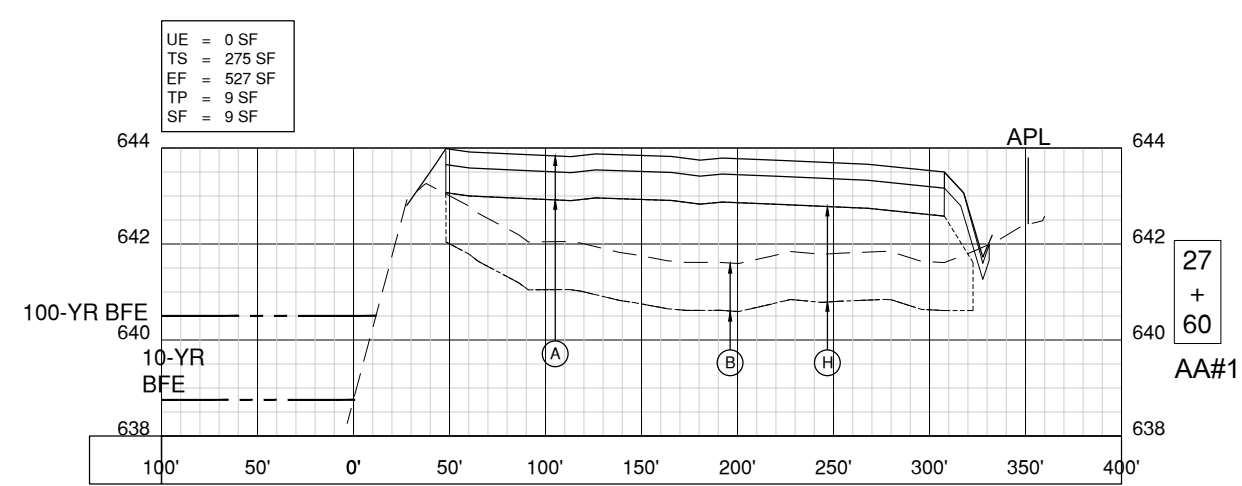
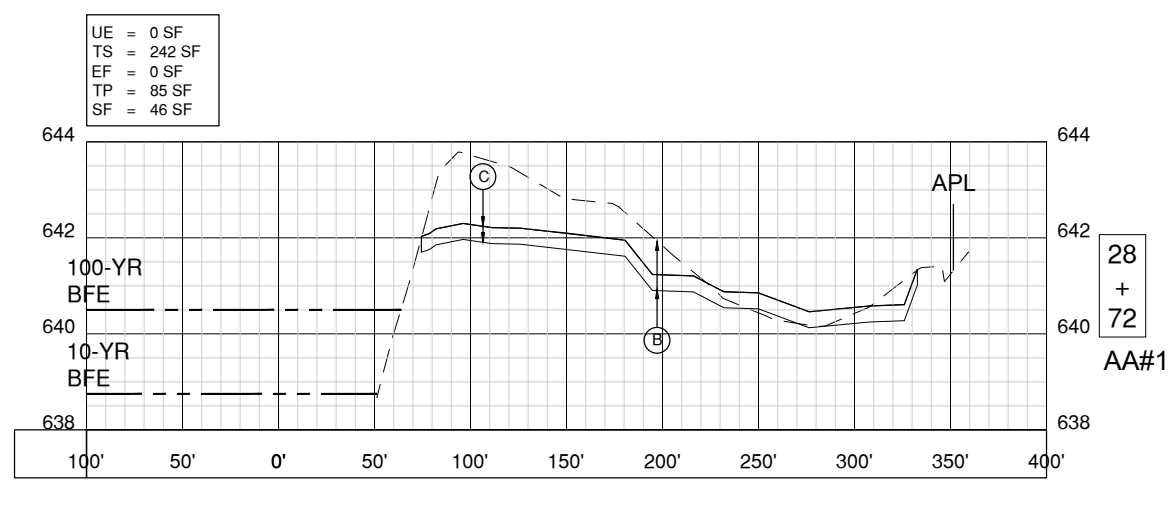
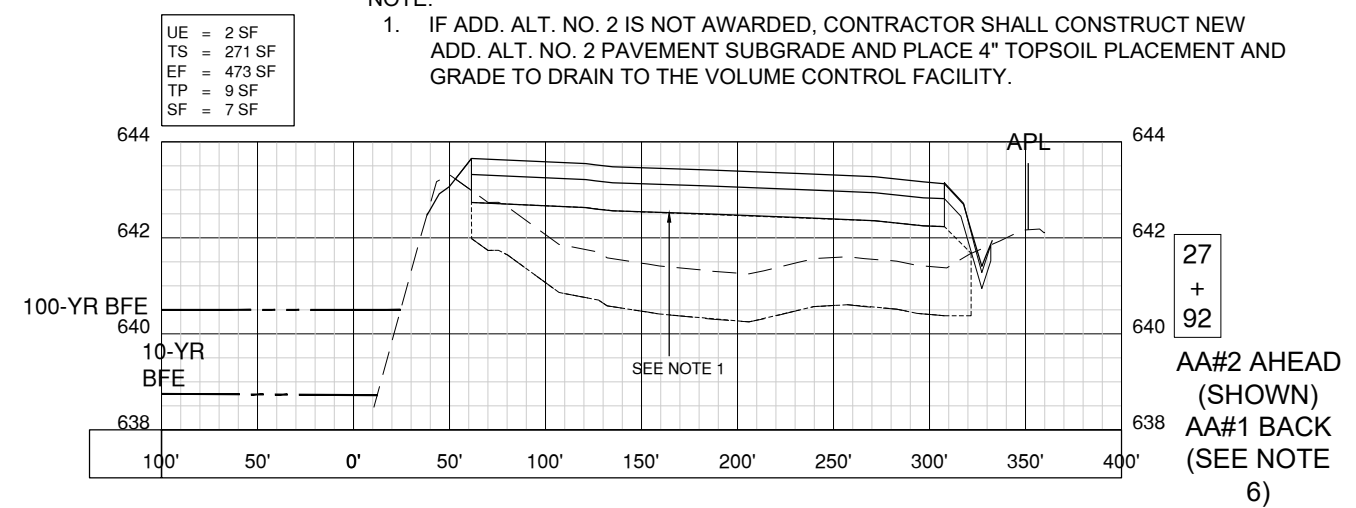
IL CONTRACT: **PA066**
 IL LETTING ITEM: **04A**
 IL PROJECT: **PWK-5128**
 S.B.G. PROJECT: **3-17-SBGP-TBD**

REVISIONS		
NUMBER	BY	DATE



STA. 27+92 TO 28+52:

NOTE:
 1. IF ADD. ALT. NO. 2 IS NOT AWARDED, CONTRACTOR SHALL CONSTRUCT NEW ADD. ALT. NO. 2 PAVEMENT SUBGRADE AND PLACE 4" TOPSOIL PLACEMENT AND GRADE TO DRAIN TO THE VOLUME CONTROL FACILITY.



SEE CROSS SECTIONS - 4 FOR NOTES AND LEGEND

CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 CONSTRUCT APRON (NE QUADRANT) AND TAXILANE ACCESS
CROSS SECTIONS - 3

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	07/29/2024
JOB No:	23005747.00

FINAL

