

WAUKEGAN PORT DISTRICT WAUKEGAN, LAKE COUNTY, ILLINOIS

CONSTRUCTION PLANS FOR WAUKEGAN NATIONAL AIRPORT

REHABILITATE HANGAR PAVEMENTS IN SOUTH HANGAR AREA - PHASE 2

ILLINOIS PROJECT: UGN-5129
S.B.G. PROJECT: 3-17-SBGP-XXX

JULY 26, 2024

811 Know what's below.
Call before you dig.
J.U.L.I.E.
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
www.illinois1call.com

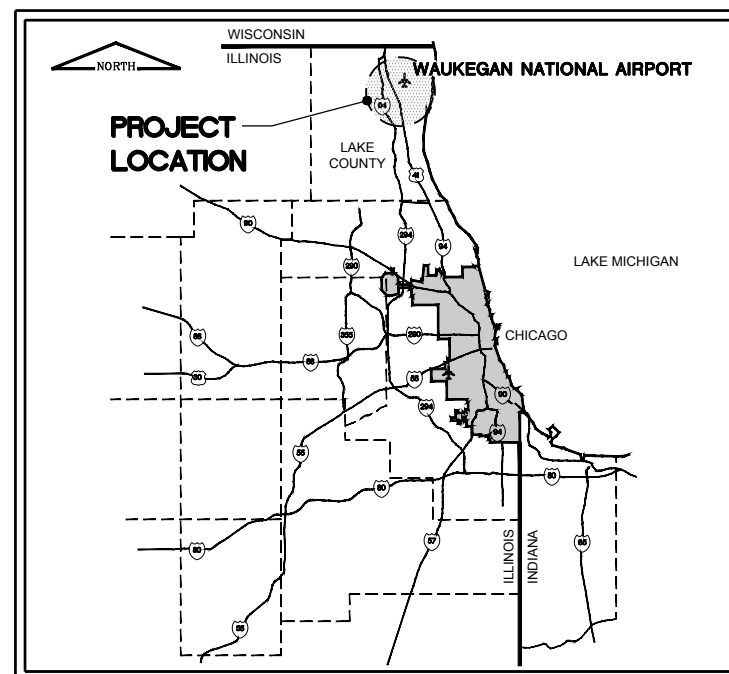
THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE. SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

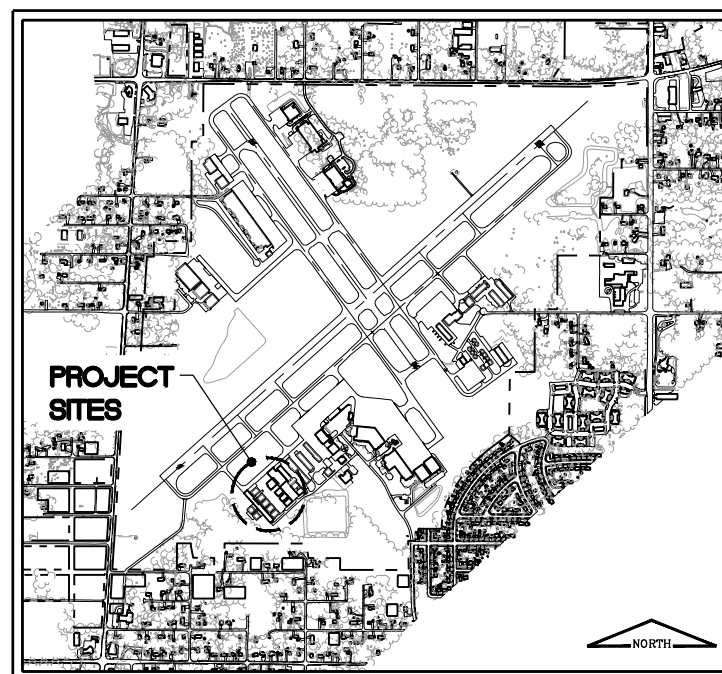


WAUKEGAN NATIONAL AIRPORT
WAUKEGAN PORT DISTRICT
2601 Plane Rest Drive
Waukegan, Illinois 60087
Telephone: 847.244.0055
Fax: 847.244.3813

APPROVED BY 
SKIP GOSS, GENERAL MANAGER
DATE JULY 26 2024



LOCATION MAP



SITE PLAN

CALL J.U.L.I.E. BEFORE
EXCAVATING AT 811
WAUKEGAN NATIONAL AIRPORT

TOWNSHIP: 46 NORTH
RANGE: 12 EAST
SECTION: 31 AND 32
COUNTY: LAKE
TOWNSHIP: BENTON

DESIGN INFORMATION
DESIGN AIRCRAFT APPROACH CATEGORY B
DESIGN AIRCRAFT GROUP I

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

24006440-00



SUBMITTED BY 
CRAIG LOUDEN, P.E.
DATE JULY 26 2024



CONSULTANTS

IL PROJECT: UGN-5129
IL LETTING ITEM: 01A
IL CONTRACT: WA081

REHABILITATE HANGAR
PAVEMENTS - PHASE 2

FINAL
JULY 26, 2024

OWNER



MARK | DATE | DESCRIPTION

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SHEET TITLE

INDEX TO SHEETS
SUMMARY OF
QUANTITIES

SHEET 2 OF 18

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- 5 SEQUENCE OF CONSTRUCTION NOTES AND DETAILS
- 6 EXISTING CONDITIONS /PROPOSED REMOVALS
- 7 PROPOSED IMPROVEMENTS
- 8 STORM WATER POLLUTION PREVENTION PLAN NOTES
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- 17 CROSS SECTIONS
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SUMMARY OF QUANTITIES

ITEM	DESCRIPTION	QUANTITY	UNIT
AR150510	ENGINEER'S FIELD OFFICE	1	L SUM
AR150520	MOBILIZATION	1	L SUM
AR152410	UNCLASSIFIED EXCAVATION	550	CU YD
AR152511	SUBGRADE REPAIR	1,080	SQ YD
AR152540	SOIL STABILIZATION FABRIC	2,695	SQ YD
AR156520	INLET PROTECTION	4	EACH
AR209608	CRUSHED AGG. BASE COURSE - 8"	2,695	SQ YD
AR401610	BITUMINOUS SURFACE COURSE	905	TON
AR401650	BITUMINOUS PAVEMENT MILLING	2,405	SQ YD
AR401900	REMOVE BITUMINOUS PAVEMENT	2,560	SQ YD
AR401916	REM & REP BIT PAVEMENT - TYPE B	960	SQ YD
AR501605	5" PCC SIDEWALK	640	SQ FT
AR501900	REMOVE PCC PAVEMENT	148	SQ YD
AR602510	BITUMINOUS PRIME COAT	795	GALLON
AR603510	BITUMINOUS TACK COAT	460	GALLON
AR620520	PAVEMENT MARKING - WATERBORNE	1,135	SQ FT
AR620525	PAVEMENT MARKING - BLACK BORDER	1,135	SQ FT
AR620595	TEMPORARY MARKING & REMOVAL	210	SQ FT
AR701900	REMOVE PIPE	100	FOOT
AR701512	12" RCP, CLASS IV	170	FOOT
AR705526	6" PERFORATED UNDERDRAIN W/SOCK	245	FOOT
AR705900	REMOVE UNDERDRAIN	255	FOOT
AR751412	INLET - TYPE B	1	EACH
AR751540	MANHOLE 4'	1	EACH
AR751900	REMOVE INLET	2	EACH
AR800152	EXPLORATORY TRENCH	220	FOOT



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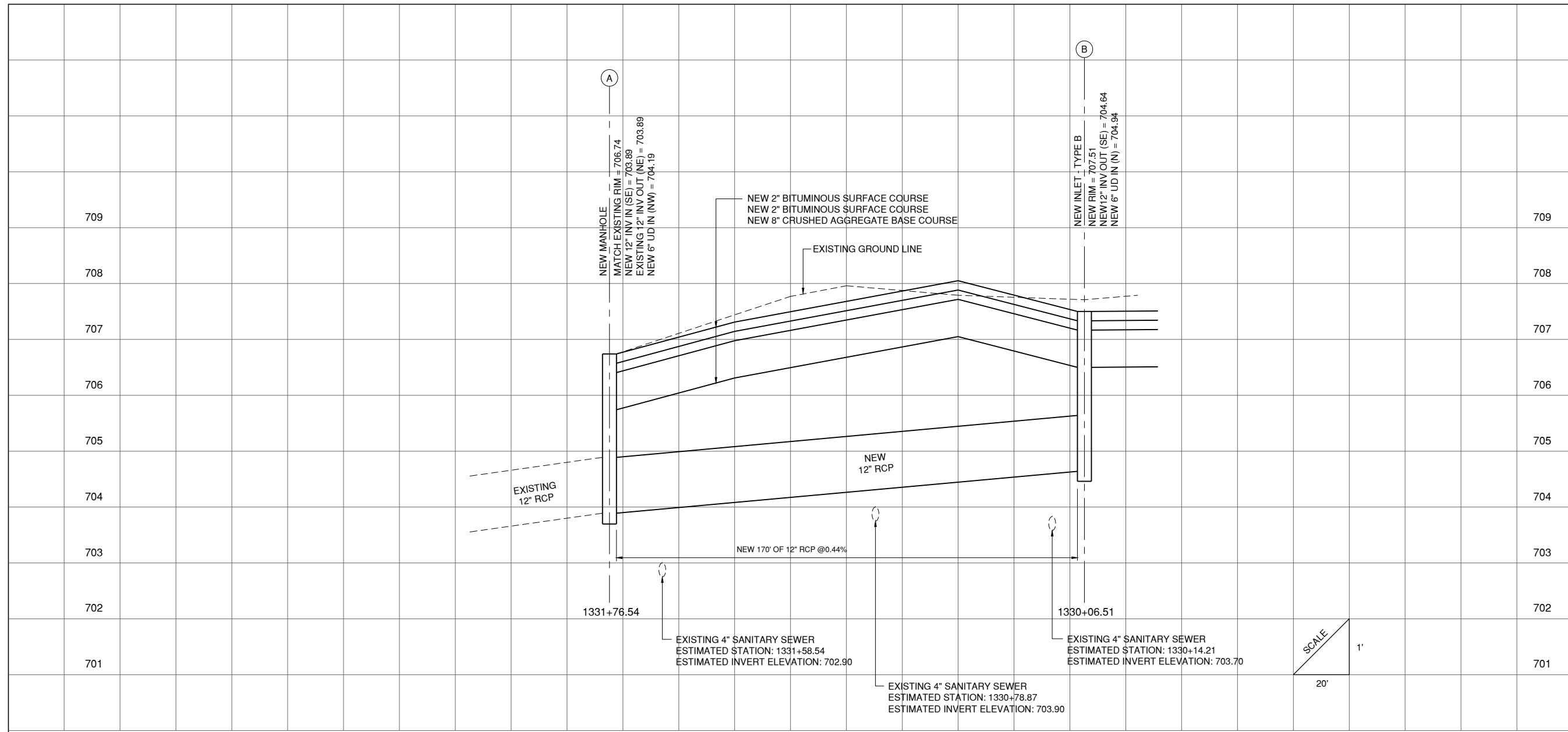
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SHEET TITLE

**STORM SEWER
PROFILES**

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NOTES

- IT IS CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT CONTRACTOR'S EXPENSE.
- WHENEVER A PROPOSED STORM SEWER CROSSES EXISTING SANITARY SEWER, THE MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE STORM SEWER TO THE BOTTOM OF THE SANITARY SEWER SHALL BE 18 INCHES OR VICE VERSA. FURTHERMORE, A MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SANITARY SEWERS AND STORM SEWERS SHALL BE MAINTAINED.



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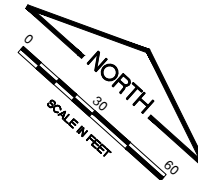


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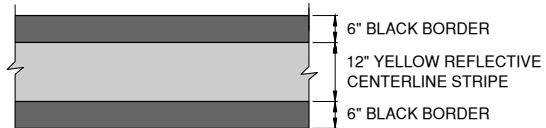
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**EROSION CONTROL
AND MARKING PLAN**

SHEET 15 OF 18



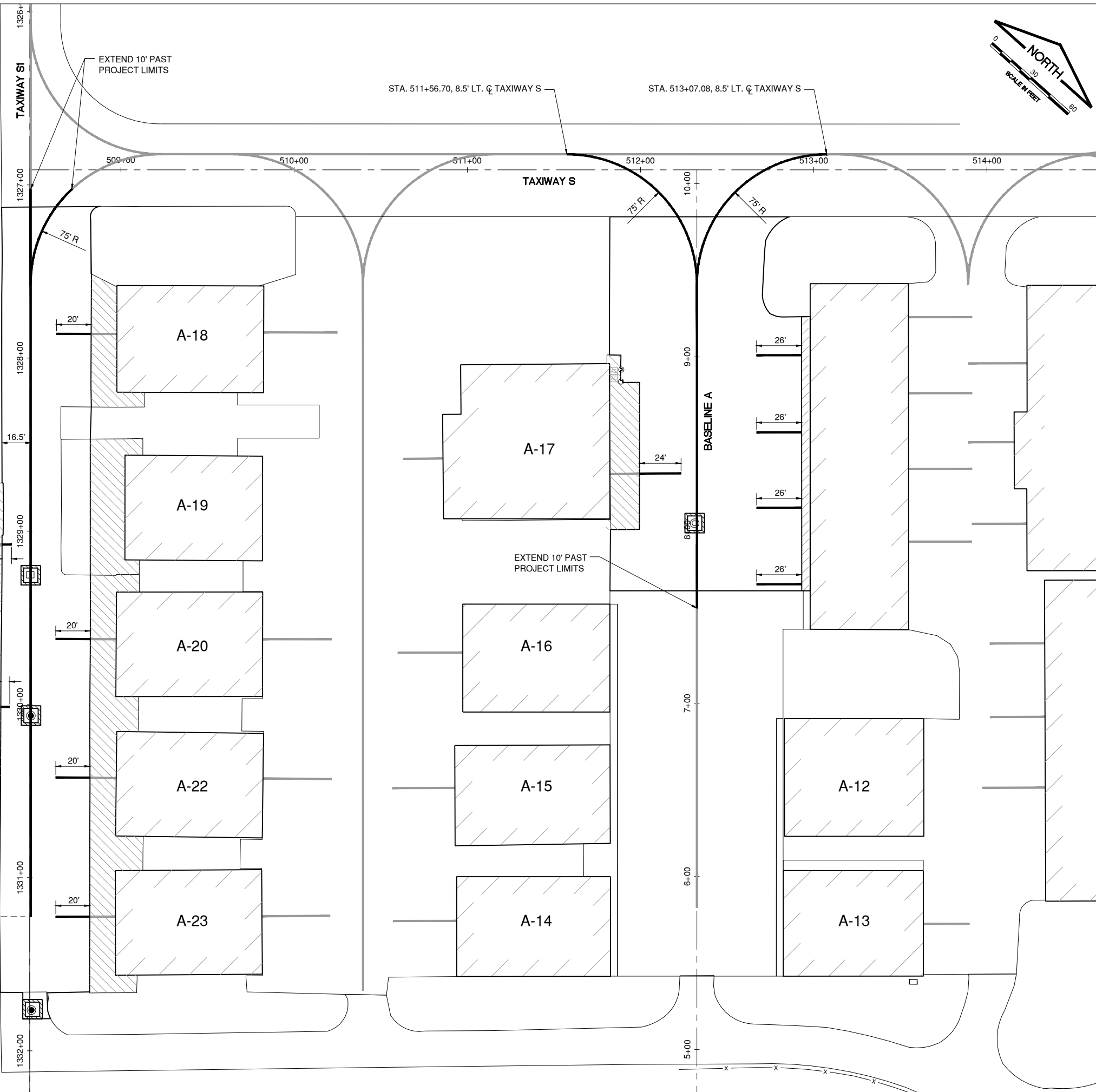
LEGEND

- EXISTING BUILDING
- EXISTING/NEW STORM SEWER MANHOLE
- NEW INLET PROTECTION
- NEW PAVEMENT MARKING



TAXIWAY CENTERLINE DETAIL

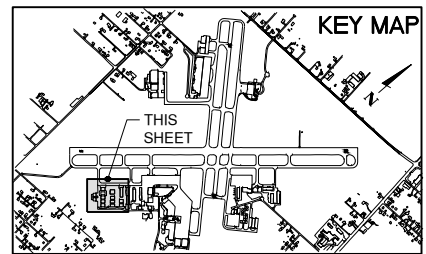
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EXTEND 10' PAST PROJECT LIMITS

EXTEND 10' PAST PROJECT LIMITS

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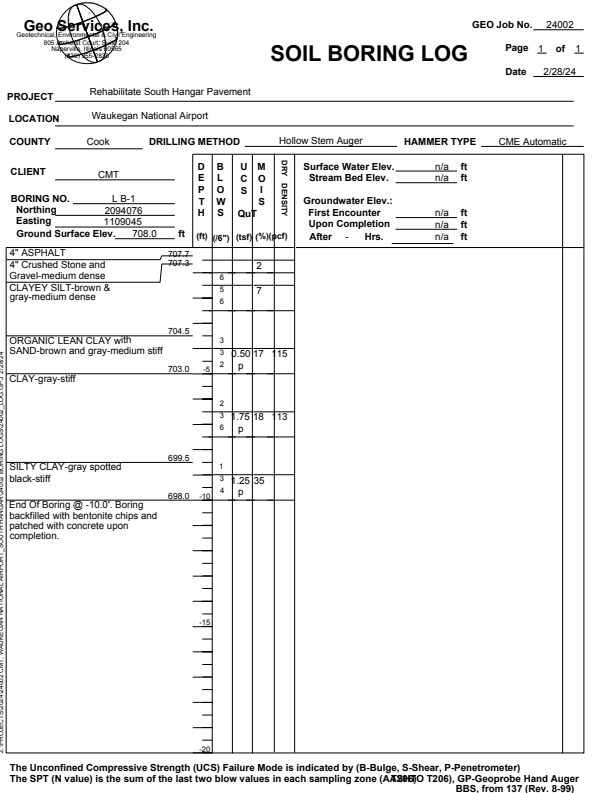
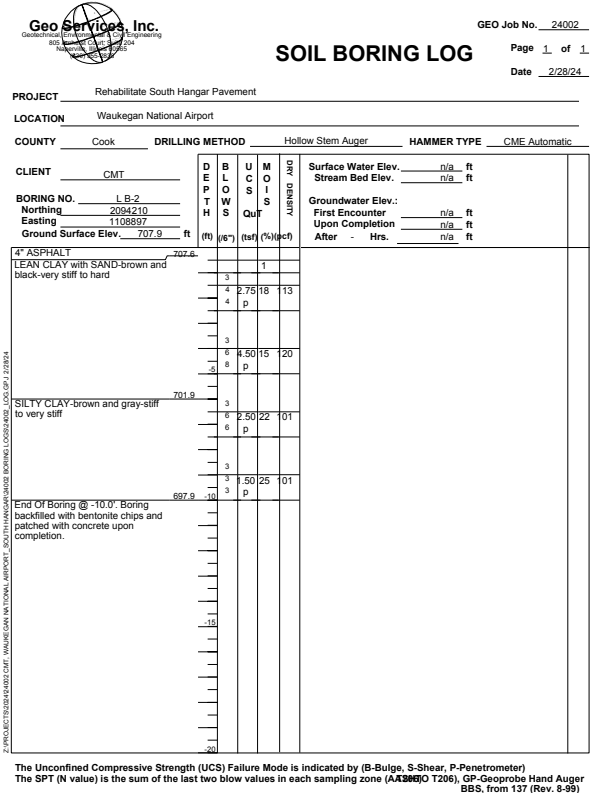
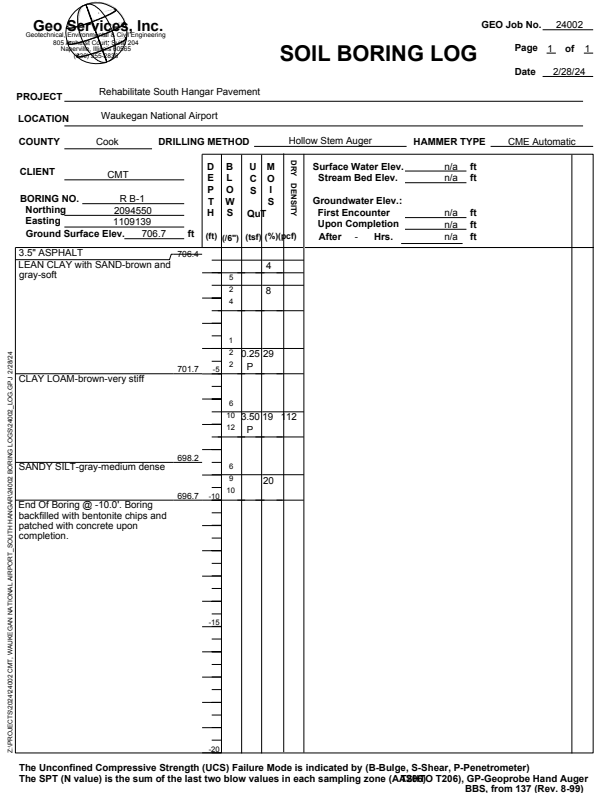
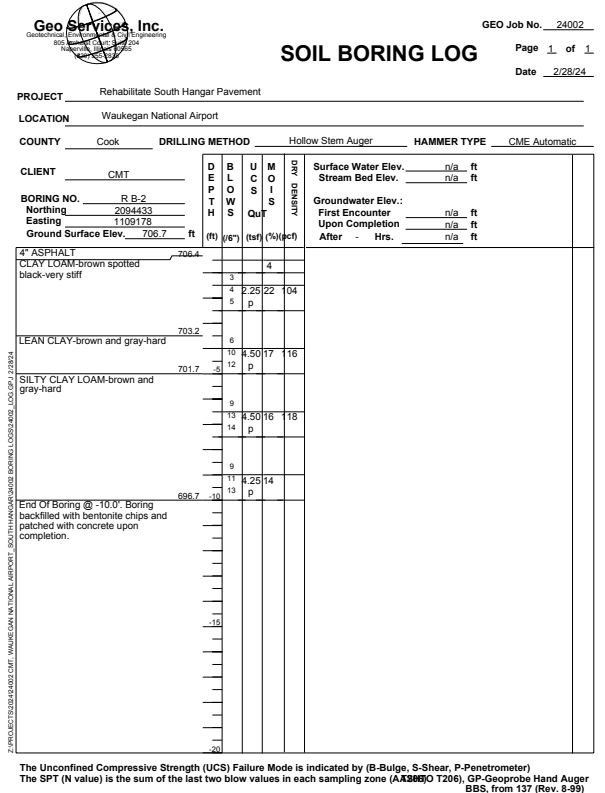


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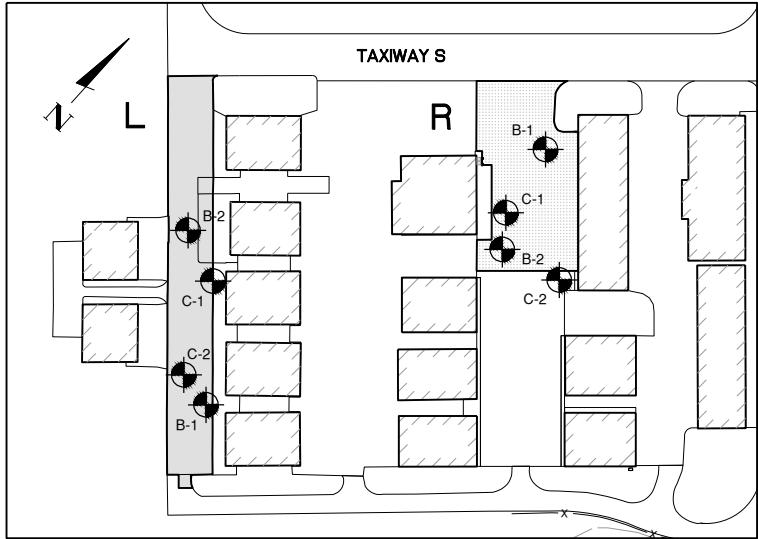
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GEOTECHNICAL
 ENGINEERING
 INFORMATION

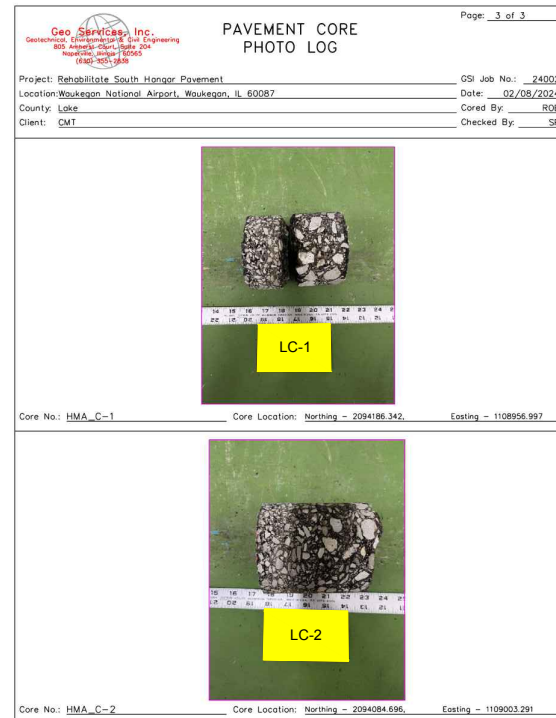
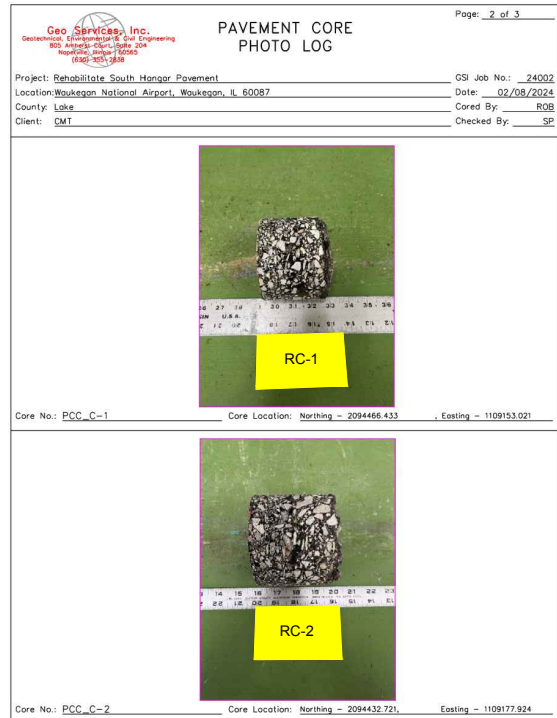
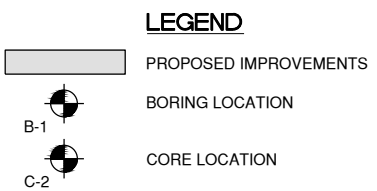


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BORE/CORE LOCATIONS

	NORTHING	EASTING
R		
B-1	2094550.12	1109138.62
B-2	2094432.72	1109177.92
C-1	2094466.43	1109153.02
C-2	2094450.07	1109249.39
L		
B-1	2094076.10	1109044.89
B-2	2094210.34	1108897.25
C-1	2094186.34	1108957.00
C-2	2094084.70	1109003.29



PAVEMENT CORE SUMMARY Page 1 of 3

CORE NO.	THICKNESS (in.)	MATERIAL DESCRIPTION
R C-1	1.25 2.0	ASPHALT-well consolidated, fine to medium aggregates-Surface mix. ASPHALT-well consolidated, fine to medium coarse aggregates-Binder mix.
R C-2	1.5 2.0	ASPHALT-well consolidated, fine to medium aggregates-Surface mix. ASPHALT-well consolidated, fine to medium coarse aggregates-Binder mix.
L C-1	2.0 2.5	ASPHALT-well consolidated, fine to medium aggregates-Surface mix. ASPHALT-well consolidated, fine to medium coarse aggregates-Binder mix.
L C-2	2.25 2.5	ASPHALT-well consolidated, fine to medium aggregate-Surface mix. ASPHALT-well consolidated, fine to medium coarse aggregate-Binder mix.