# WAUKEGAN PORT DISTRICT WAUKEGAN, LAKE COUNTY, ILLINOIS

# CONSTRUCTION PLANS FOR WAUKEGAN NATIONAL AIRPORT

REHABILITATE HANGAR PAVEMENTS IN SOUTH HANGAR AREA - PHASE 2

ILLINOIS PROJECT: UGN-5129 S.B.G. PROJECT: 3-17-SBGP-XXX

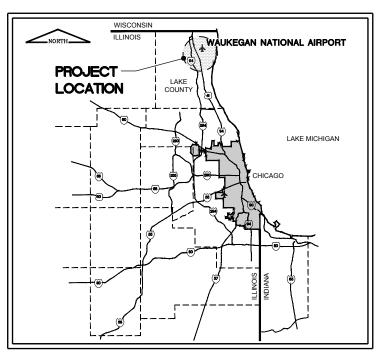
JULY 26, 2024

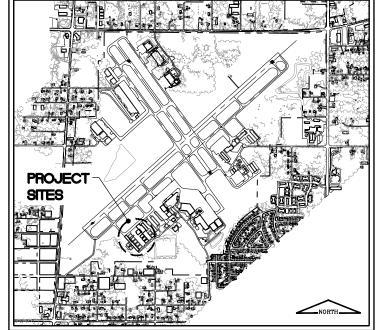


UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES OF BETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL. OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACTOR.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.







CALL J.U.L.I.E. BEFORE EXCAVATING AT 811 WAUKEGAN NATIONAL AIRPORT

> TOWNSHIP: 46 NORTH RANGE: 12 EAST SECTION: 31 AND 32 COUNTY: LAKE TOWNSHIP: BENTON

**DESIGN INFORMATION** 

DESIGN AIRCRAFT APPROACH CATEGORY B
DESIGN AIRCRAFT GROUP I



LOCATION MAP

SITE PLAN

# **INDEX TO SHEETS**

- 1 COVER SHEET
- 2 INDEX TO SHEETS AND SUMMARY OF QUANTITIES
- SITE PLAN
- 4 SEQUENCE OF CONSTRUCTION PLAN
- 5 SEQUENCE OF CONSTRUCTION NOTES AND DETAILS
- 6 EXISTING CONDITIONS /PROPOSED REMOVALS
- PROPOSED IMPROVEMENTS
- 8 STORM WATER POLLUTION PREVENTION PLAN NOTES
- STORM WATER POLLUTION PREVENTION PLAN DETAILS
- 10 TYPICAL SECTIONS
- 11 GRADING PLAN
- 12 DRAINAGE PLAN
- 13 DRAINAGE AND MISCELLANEOUS DETAILS
- 14 STORM SEWER PROFILES
- 15 EROSION CONTROL AND MARKING PLAN
- 16 INDEX TO CROSS SECTIONS
- 17 CROSS SECTIONS
- 18 GEOTECHNICAL ENGINEERING INFORMATION

ITEM	DESCRIPTION	QUANTITY	UN
AR150510	ENGINEER'S FIELD OFFICE	1	LS
AR150520	MOBILIZATION	1	LS
AR152410	UNCLASSIFIED EXCAVATION	550	CU
AR152511	SUBGRADE REPAIR	1,080	SQ
AR152540	SOIL STABILIZATION FABRIC	2,695	SQ
AR156520	INLET PROTECTION	4	EA
AR209608	CRUSHED AGG. BASE COURSE - 8"	2,695	SQ
AR401610	BITUMINOUS SURFACE COURSE	905	TC
AR401650	BITUMINOUS PAVEMENT MILLING	2,405	sq
AR401900	REMOVE BITUMINOUS PAVEMENT	2,560	SQ
AR401916	REM & REP BIT PAVEMENT - TYPE B	960	SQ
AR501605	5" PCC SIDEWALK	640	SQ
AR501900	REMOVE PCC PAVEMENT	148	sq
AR602510	BITUMINOUS PRIME COAT	795	GAL
AR603510	BITUMINOUS TACK COAT	460	GAL
AR620520	PAVEMENT MARKING - WATERBORNE	1,135	SQ
AR620525	PAVEMENT MARKING - BLACK BORDER	1,135	SQ
AR620595	TEMPORARY MARKING & REMOVAL	210	SQ
AR701900	REMOVE PIPE	100	FC
AR701512	12" RCP, CLASS IV	170	FC
AR705526	6" PERFORATED UNDERDRAIN W/SOCK	245	FC
AR705900	REMOVE UNDERDRAIN	255	FC
AR751412	INLET - TYPE B	1	EA
AR751540	MANHOLE 4'	1	EA
AR751900	REMOVE INLET	2	EA
AR800152	EXPLORATORY TRENCH	220	FO



CONSULTANTS

IL PROJECT: UGN-5129 IL LETTING ITEM: 01A IL CONTRACT: WA081

> REHABILITATE HANGAR PAVEMENTS - PHASE 2

> > **FINAL** JULY 26, 2024

OWNER

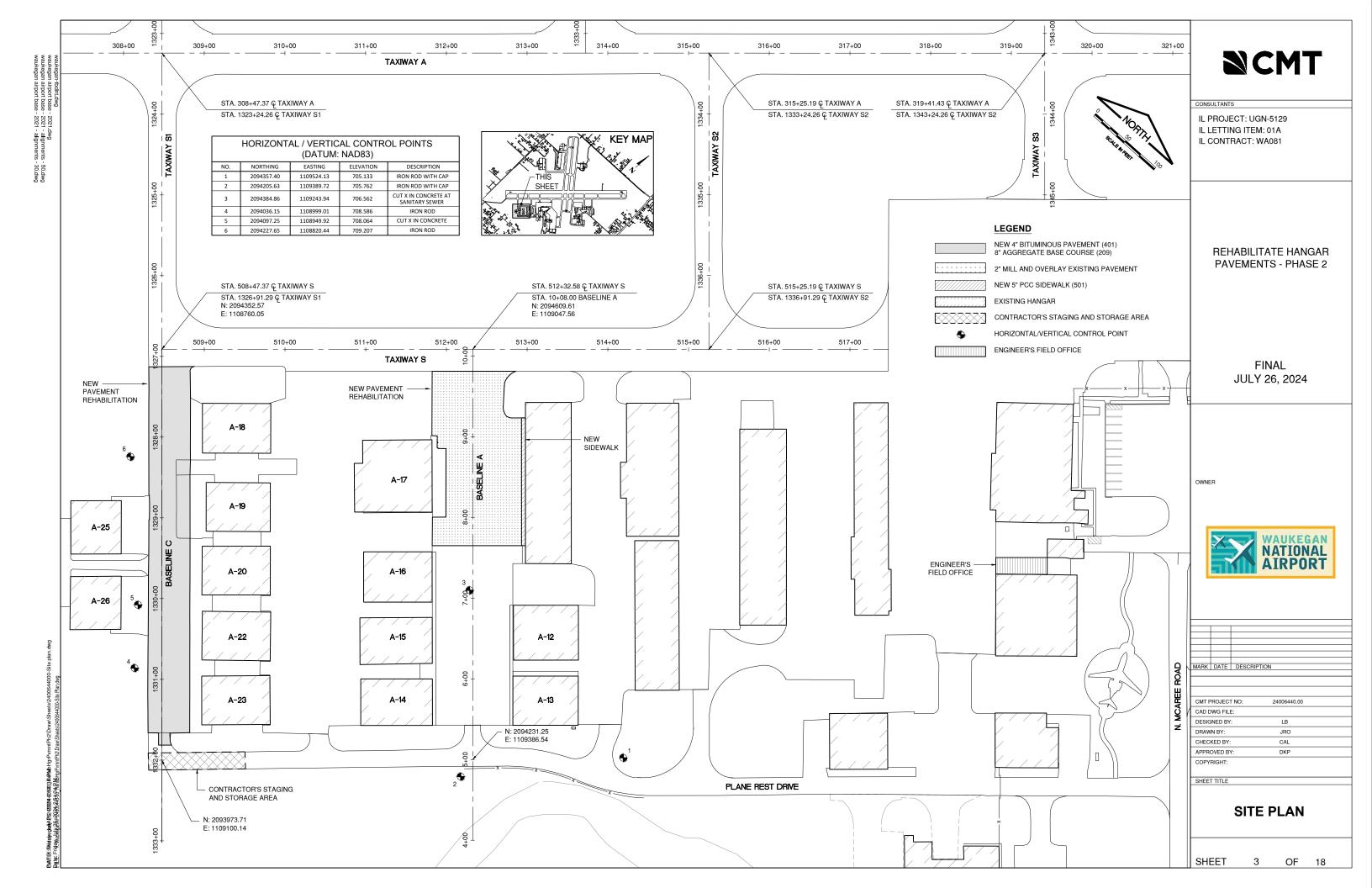


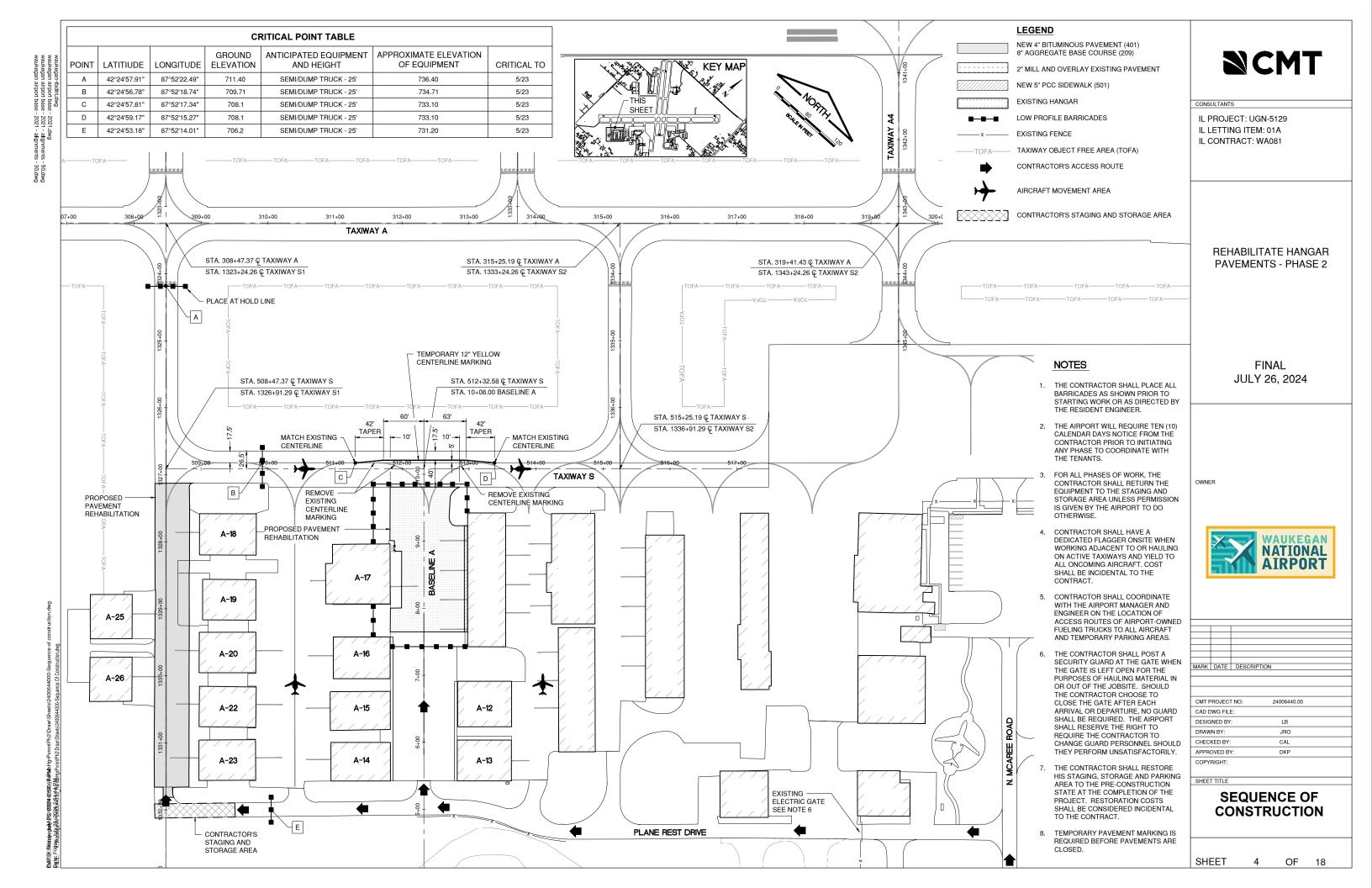
MARK	DATE	DESCRIPTION

CMT PROJECT NO:	24006440.00	
CAD DWG FILE:		
DESIGNED BY:	LB	
DRAWN BY:	JRO	
CHECKED BY:	CAL	
APPROVED BY:	DKP	
CORVEIGHT		

**INDEX TO SHEETS SUMMARY OF QUANTITIES** 

SHEET 2 OF 18





ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.

- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

### 1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE WITH THE AIRPORT RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA) THE COST OF PREPARING FOR AND ATTENDING THE PRE-CONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRE-CONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

### 2. PHASING

- TOTAL CONTRACT TIME SHALL BE 67 CALENDAR DAYS.
- WORK SHALL BE COMPLETED UNDER ONE PHASE AS SHOWN ON THE SEQUENCE OF CONSTRUCTION PLAN SHEETS.
- WORK AREAS AND DESCRIPTIONS

COMPLETE INITIAL MOBILIZATION, STAGING AREA PREPARATION AND LAYOUT IN ORDER TO PROCEED WITH THE REMOVAL AND REPLACEMENT, WHERE SHOWN ON PLANS, OF THE SOUTHERN PAVEMENT AND MILL AND OVERLAY OF THE NORTHERN PAVEMENT. INSTALL THE NEW BITUMINOUS PAVEMENT, CRUSHED AGGREGATE BASE COURSE AND COMPLETE LANDSCAPING AND GENERAL IMPROVEMENTS.

### 3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL BUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

### 4. NAVAIDS THAT COULD BE AFFECTED

- THE CONTRACTOR MUST COORDINATE WITH AIRPORT OPERATIONS/ENGINEER IN ADVANCE FOR ANY WORK WITHIN A NAVAID CRITICAL AREA OR AFFECTING THE VISUAL, TRANSMITTED SIGNAL OR POWER SUPPLY OF A NAVAID.
- EDGE LIGHTS, THRESHOLD LIGHTS, VISUAL AIDS AND ALL ILS EQUIPMENT SHALL BE SHUT OFF FOR THE DURATION OF A CLOSURE PERIOD ON ANY ASSOCIATED PAVEMENTS. IF THE LIGHTING CIRCUIT MUST BE ON FOR OPEN PAVEMENT AREAS, CLOSED PAVEMENT AREA LIGHTS SHALL BE COVERED COMPLETELY.
- EXCEPT WHERE NOTED IN THE PLANS, EXISTING COMMUNICATIONS EQUIPMENT AND NAVIGATIONAL AIDS (NAVAIDS) SHALL NOT BE DISTURBED BY THE CONTRACTOR AND SHALL BE PROTECTED FROM
- PRIOR TO BEGINNING SITE WORK, CONTRACTOR SHALL COORDINATE THROUGH AIRPORT OPERATIONS TO LOCATE, AND THE CONTRACTOR TO MARK ALL UNDERGROUND COMMUNICATIONS CABLES AND FACILITIES, WITHIN THE PROJECT AREA.
- IF CONTRACTOR CAUSES INTERRUPTION OF POWER OR COMMUNICATIONS TO A NAVAID CONTRACTOR SHALL REPAIR WITHIN 24 HOURS AT THE CONTRACTOR'S COST. CONTRACTOR MUST COORDINATE REPAIR WITH AIRPORT OPERATIONS BEFORE ANY REPAIR IS MADE.

### 5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE ROUTES AND GATE(S) SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE CLOSED DURING WORK HOURS.
- CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR
- CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE MARKED AND FLAGGED PER SECTION 40-05 OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25' PER SECTION 40-10 OF THE SPECIAL PROVISIONS.
- DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE ASPHALT, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA FENCE, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR PERSONNEL THAT HAS OBTAINED PROPER DRIVING PRIVILEGES.
- CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE WAUKEGAN AIR TRAFFIC CONTROL TOWER GROUND CONTROL (121.65 MHZ) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA) THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED. FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY SATISFIED THE AIRPORT OF THEIR COMPETENCE MAY OPERATE THESE RADIOS.
- THE CONTRACTORS STORAGE AND STAGING AREAS WILL BE AS SHOWN ON THE SITE PLAN.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREAS.
- DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
- 12. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE COMMON TRAFFIC ADVISORY FREQUENCY. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN
- 13 THE CONTRACTOR SHALL THOROLIGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO VEHICLE AND AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR
- 14. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK
- 15. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
- 16. THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE

### 6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY THE AIRPORT OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS

### 7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

### 8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

### 9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 10 DAYS NOTICE TO THE FAA AND AIRPORT PRIOR TO THE CLOSURE OF ANY RUNWAY SO THAT THE FAA MAY DEACTIVATE THE FAA - OWNED NAVAIDS
- THE CONTRACTOR SHALL GIVE A MINIMUM 30 DAYS NOTICE TO THE AIRPORT, AND PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, PRIOR TO CLOSING ANY RUNWAY OR TAXIWAY HANGAR PAVEMENT SO THAT THE PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT FOR COORDINATION WITH THE AIRPORT TENANTS.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY INDICATION WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS
- 5. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.

### 10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX C OF FAA AC 150/5370-2G OR LATEST EDITION MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL REQUEST OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

### 11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR LITILITY LOCATES SEE SECTION 70-16 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

### 13. SPECIAL CONDITIONS

ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL

### 14. RUNWAY AND TAXIWAY VISUAL AIDS

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT. THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2G OR LATEST EDITION

### 15. MARKING AND SIGNS FOR ACCESS ROUTES

BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THE DRAINAGE AND MISCELLANEOUS DETAILS SHEET

### 16. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS. TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION FOUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5C OR LATEST EDITION AT ALL TIMES WHILE OPERATING ON IRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE SEQUENCE OF CONSTRUCTION SHEET OR AS DIRECTED BY THE
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

### 17. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTION.
- LIGHTS SHALL CONSIST OF VEHICLE OR MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA, VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL NOT INTERFERE WITH AIR OPERATIONS. ANY WORK BEING PERFORMED UNDER INSUFFICIENT ARTIFICIAL LIGHTING. IN THE RESIDENT ENGINEER'S JUDGEMENT, SHALL BE STOPPED LINTIL SLICH TIME AS ADDITIONAL LIGHTING IS PROVIDED. ALL WORK PERFORMED DURING THAT TIME WILL NOT BE ACCEPTABLE UNTIL PROPER INSPECTION AND TESTING CAN BE MADE.

### 18. PROTECTION

- ALL WORK REQUIRED INSIDE OF A RUNWAY SAFETY AREA, WILL REQUIRE THE RUNWAY TO BE CLOSED.
- 2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WILL REQUIRE THE TAXIWAY TO BE

### 19. OTHER LIMITATIONS ON CONSTRUCTION

- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT IF DIRECTED BY THE
- BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE

# 20. COOPERATION BETWEEN CONTRACTORS

- THE AIRPORT RESERVES THE RIGHT TO CONTRACT FOR AND PERFORM OTHER OR ADDITIONAL WORK ON OR NEAR THE WORK COVERED BY THIS CONTRACT
- OTHER CONTRACTS MAY BE UNDER CONSTRUCTION CONCURRENTLY RESULTING IN MORE THAN ONE CONTRACTOR WORKING ON THE AIRPORT AT THE SAME TIME.
- THE CONTRACTOR SHALL PLAN AND CONDUCT HIS/HER WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS. THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS. IT IS TO BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECTS AND THE SAFETY AND CONVENIENCE OF THE AVIATION PUBLIC ARE THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS/HER SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS. WHEN CONFLICTS ARISE RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT MANAGEMENT THROUGH THE AIRPORT'S PROJECT REPRESENTATIVE IN THE BEST INTEREST OF THE AIRPORT. DELAYS, CHANGES IN SCHEDULING, OR EXPEDITION OF WORK UNDER THIS CONTRACT TO COORDINATE THE TIMELY PROSECUTION OF WORK WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED
- THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS AND SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT
  - FENCING PHASE 5
  - REHAB MAIN PARKING LOT

NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND GAIN ACCESS TO THE SITE DUE TO ADJACENT CONSTRUCTION



### CONSULTANTS

IL PROJECT: UGN-5129 IL LETTING ITEM: 01A IL CONTRACT: WA081

> REHABILITATE HANGAR **PAVEMENTS - PHASE 2**

> > **FINAL** JULY 26, 2024

OWNER



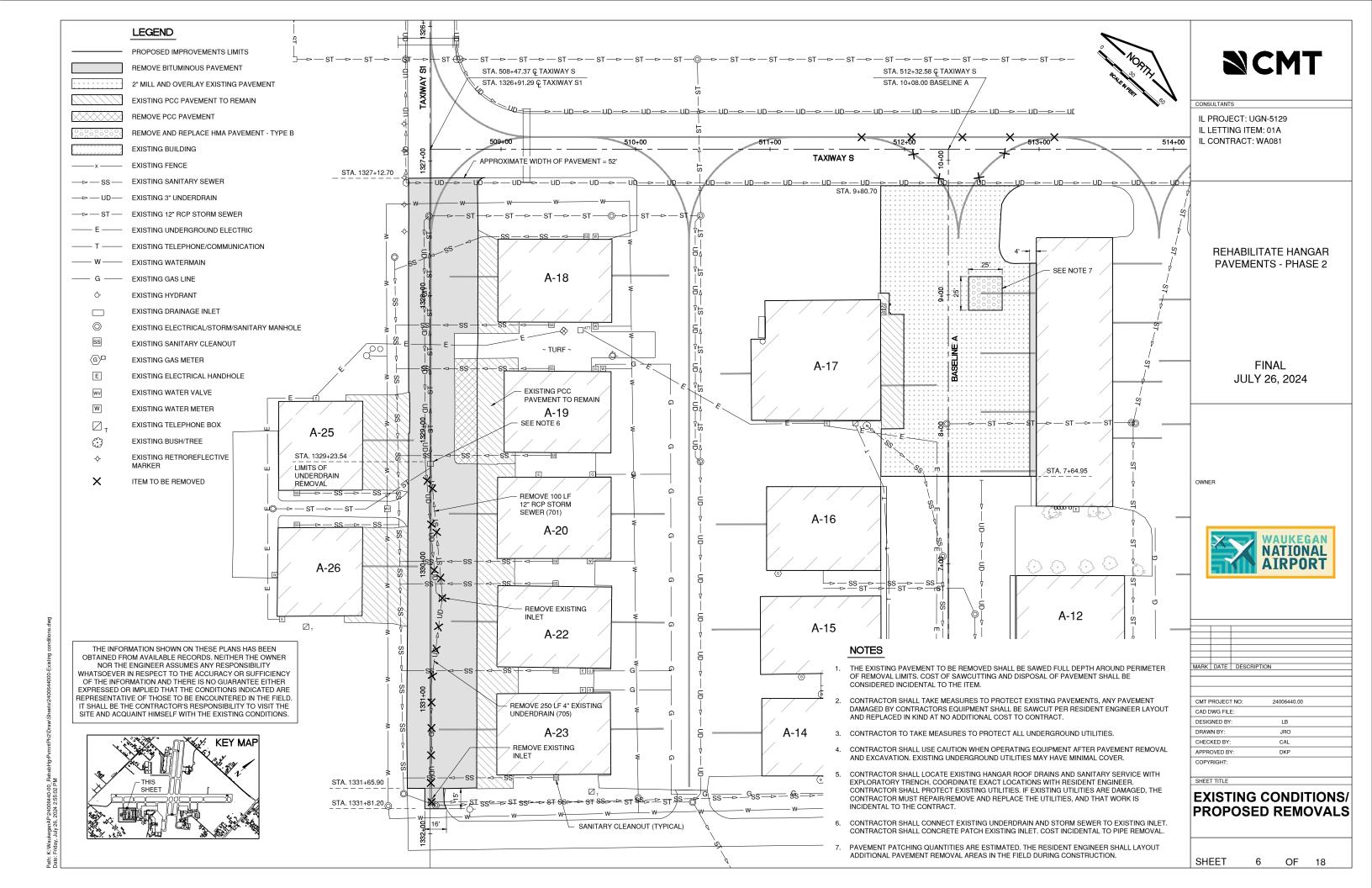
MARK	DATE	DESCRIPTION

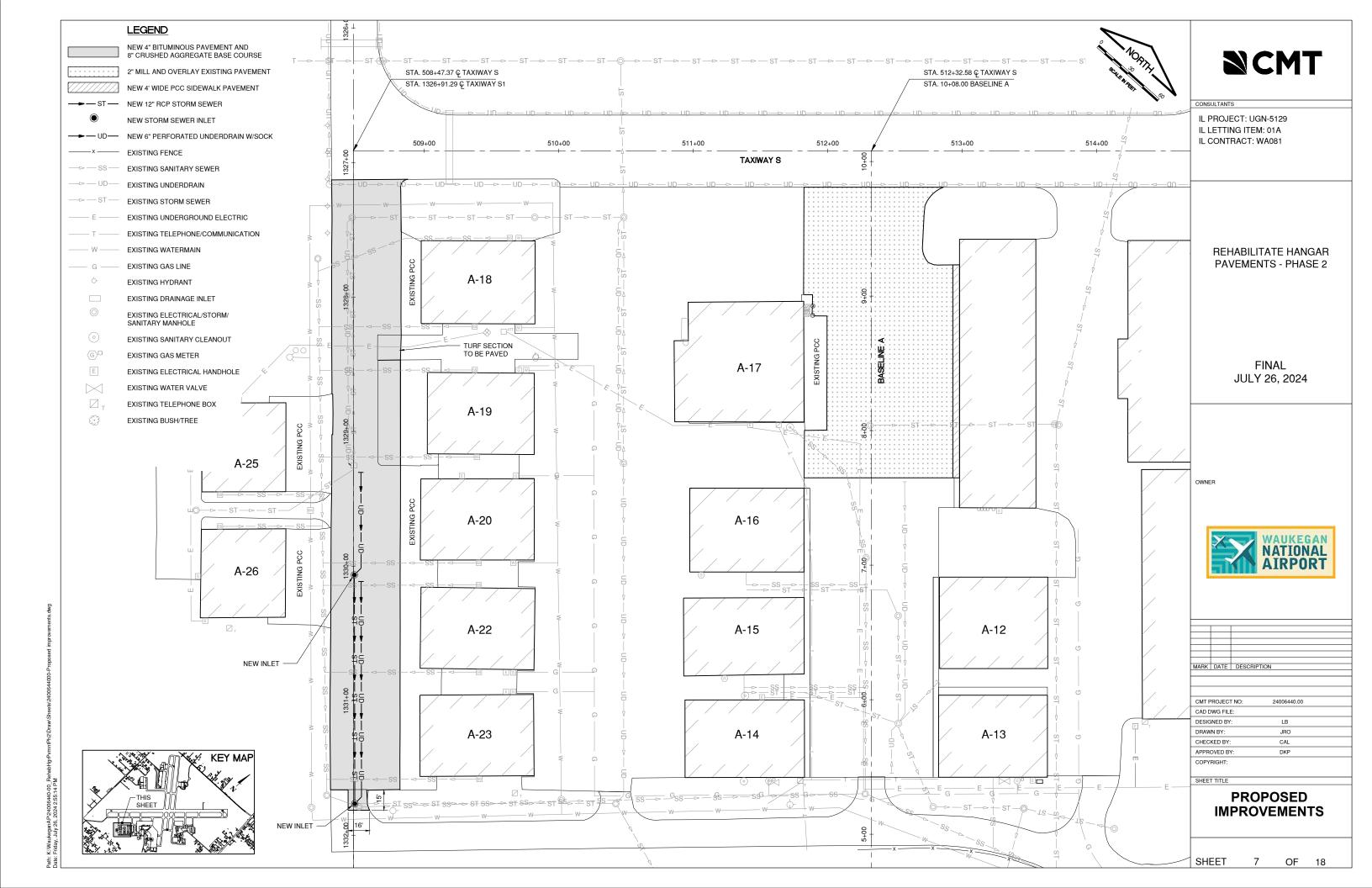
CMT PROJECT NO: 24006440.00 CAD DWG FILE: DESIGNED BY DRAWN BY JRO CHECKED BY CAL APPROVED BY COPYRIGHT

**SEQUENCE OF CONSTRUCTION GENERAL** NOTES AND DETAILS

SHEET

OF





## STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

### SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN

THIS PROJECT CONSISTS OF REHABILITATING PAVEMENT AND DRAINAGE IMPROVEMENTS AT WALKEGAN NATIONAL AIRPORT

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING

- INSTALL AND MAINTAIN TEMPORARY EROSION CONTROL MEASURES.
- 2. CONSTRUCT TEMPORARY CONCRETE WASHOUT
- 3. REMOVE EXISTING PAVEMENT
- 4 EXCAVATION FOR NEW PAVEMENT
- 5. INSTALL STORM SEWER.
- 6. CONSTRUCT NEW HMA PAVEMENT ON SOUTHERN PAVEMENT AND MILL AND OVERLAY PAVEMENT AND CONSTRUCT PCC
- FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
- 8. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND BLANKET

### AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE MORE THAN AN ACRE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS

- INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
- 2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS. AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

### DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE

THE CONSTRUCTION LOCATED IN DES PLANES RIVER WATERSHED. THE SITE DRAINS THROUGH A SERIES OF OVERLAND FLOW ROUTES/STORM SEWER INTO THE SUBURBAN COUNTRY CLUB TRIBUTARY THAT OUTLET INTO ULTIMATELY THE DES PLAINES

### SEDIMENTATION AND EROSION CONTROL NOTES

THE SOIL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE INSPECTED WEEKLY AND AFTER 1/2 INCH OF RAIN OR MORE BY

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL, UNLESS

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORABILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER IL B10. ISSUED BY THE

### DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION

- WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
- EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED. AT THE CONTRACTORS EXPENSE. IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS
- AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS
- B. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF IOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS
- CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
- SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE
- THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER REEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

### DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

### CONTRACTORS

- 1. THE STORM WATER POLITION PREVENTION PLAN MUST CLEARLY IDENTIFY FOR EACH MEASURE IDENTIFIED IN THE PLAN THE CONTRACTOR(S) OR SUBCONTRACTOR(S) THAT WILL IMPLEMENT THE MEASURE. ALL CONTRACTORS AND SUBCONTRACTORS IDENTIFIED IN THE PLAN MUST SIGN A COPY OF THE CERTIFICATION STATEMENT IN PARAGRAPH 2 BELOW IN ACCORDANCE WITH PART VI.G (SIGNATORY REQUIREMENTS) OF THIS PERMIT, ALL CERTIFICATIONS MUST BE INCLUDED IN THE STORM WATER POLLUTION PREVENTION PLAN EXCEPT FOR OWNERS THAT ARE ACTING AS
- CERTIFICATION STATEMENT, ALL CONTRACTORS AND SURCONTRACTORS IDENTIFIED IN A STORM WATER POLITITION PREVENTION PLAN IN ACCORDANCE WITH PARAGRAPH 1 ABOVE SHALL SIGN A COPY OF THE FOLLOWING CERTIFICATION STATEMENT BEFORE CONDUCTING ANY PROFESSIONAL SERVICE AT THE SITE IDENTIFIED IN THE STORM WATER POLLUTION PREVENTION PLAN:

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (1LR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS

THE CERTIFICATION MUST INCLUDE THE NAME AND TITLE OF THE PERSON PROVIDING THE SIGNATURE IN ACCORDANCE WITH PART VIG OF THIS PERMIT: THE NAME, ADDRESS AND TELEPHONE NUMBER OF THE CONTRACTING FIRM; THE ADDRESS (OR OTHER IDENTIFYING DESCRIPTION) OF THE SITE: AND THE DATE THE CERTIFICATION IS MADE.

### CONTRACTOR CERTIFICATION STATEMENT

THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLITION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY

### PROJECT INFORMATION:

STREET ADDRESS:

REHABILITATE HANGAR PAVEMENT IN MARKED: SOUTH HANGAR AREA - PHASE 2 ROUTE: WAUKEGAN NATIONAL AIRPORT

DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SECTION: 31

PROJECT NUMBER: UGN-5129 CONTRACT NUMBER: 3-17-SBGP-TBD

COUNTY: LAKE I CERTIEV LINDER PENALTY OF LAW THAT I LINDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLITION

SIGNATURE: DATE: PRINTED NAME: TITLE:

NAME OF FIRM:

CITY, STATE, ZIP:

PHONE NUMBER:

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT

### ADDITIONAL NOTES

- PRIOR TO COMMENCING DEWATERING ACTIVITIES AND LAND DISTURBING ACTIVITIES IN AREAS OTHER THAN INDICATED ON THESE PLANS (INCLUDING BUT NOT LIMITED TO, ADDITIONAL PHASES OF DEVELOPMENT AND OFF-SITE BORROW OR WASTE AREAS) A SUPPLEMENTARY EROSION CONTROL PLAN SHALL BE SUBMITTED TO THE OWNER FOR REVIEW AND INCLUSION INTO SWPPP.
- DURING DEWATERING OPERATION, WATER SHALL BE PUMPED FROM A SUMP PIT INTO SEDIMENT BASINS OR SILT TRAPS OR OTHER APPROVED BMP. COSTS OF SUMP PITS, SEDIMENT BASINS AND SILT TRAPS AND OTHER BMP ARE INCIDENTAL TO THE DEWATERING PAY ITEM. DEWATERING DIRECTLY INTO FIELD TILES OR STORMWATER STRUCTURES IS PROHIBITED.
- 3. UNI ESS OTHERWISE INDICATED. ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE CONSTRUCTED ACCORDING TO MINIMUM STANDARDS AND SPECIFICATIONS IN THE ILLINOIS URBAN MANUAL CURRENT EDITION FOUND AT ILLINOISMANUAL.ORG.
- WINTER SHUTDOWN SHALL BE ADDRESSED FARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND OR PERMANENT VEGETATIVE COVER FOR PROPER EROSION AND SEDIMENT CONTROL. TEMPORARY SEEDING (156) SHALL BE PAID FOR ONLY WHEN NEEDED FOR WINTERIZATION OF THE SITE AT THE LOCATIONS NOTED BY THE ENGINEER. ALL OTHER APPLICATIONS OF TEMPORARY SEED NEEDED FOR COMPLIANCE FOR THE NPDES PERMIT SHALL BE INCIDENTAL

# LAKE COUNTY STORMWATER MANAGEMENT COMMISSION SOIL EROSION AND SEDIMENT CONTROL CONSTRUCTION NOTES

- A. SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
- FOR THOSE DEVELOPMENTS THAT REQUIRE A DESIGNATED EROSION CONTROL INSPECTOR (DECI), INSPECTIONS AND DOCUMENTATION SHALL BE PERFORMED, AT A MINIMUM
- LIPON COMPLETION OF SEDIMENT AND RUNOFF CONTROL MEASURES (INCLUDING PERIMETER CONTROLS AND DIVERSIONS), PRIOR TO PROCEEDING WITH ANY OTHER EARTH DISTURBANCE OR
- AFTER EVERY SEVEN (7) CALENDAR DAYS OR STORM EVENT WITH GREATER THAN 0.5 INCH OF RAINFALL OR LIQUID EQUIVALENT PRECIPITATION.
- SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. IF STRIPPING CLEARING, GRADING, OR LANDSCAPING ARE TO BE DONE IN PHASES, THE PERMITTEE SHALL PLAN FOR APPROPRIATE SOIL EROSION AND SEDIMENT CONTROL MEASURES.
- A STABILIZED MAT OF CRUSHED STONE MEETING IDOT GRADATION CA 1 UNDERLAIN WITH FILTER FABRIC AND IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL, OR OTHER APPROPRIATE MEASURE(S) AS APPROVED BY THE ENFORCEMENT OFFICER, SHALL BE INSTALLED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE. SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT OF WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
- TEMPORARY DIVERSIONS SHALL BE CONSTRUCTED AS NECESSARY TO DIRECT ALL RUNOFF FROM HYDROLOGICALLY DISTURBED AREAS TO AN APPROPRIATE SEDIMENT TRAP OR BASIN
- DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE OR REDISTURBANCE
- G. ALL STOCKPILES SHALL HAVE APPROPRIATE MEASURES TO PREVENT EROSION. STOCKPILES SHALL NOT BE PLACED IN FLOOD PRONE AREAS OR WETLANDS AND DESIGNATED BUFFERS.
- H. SLOPES STEEPER THAN 3H:1V SHALL BE STABILIZED WITH APPROPRIATE MEASURESAS APPROVED BY THE
- APPROPRIATE EROSION CONTROL BLANKET SHALL BE INSTALLED ON ALL INTERIOR DETENTION BASIN SIDE SLOPES BETWEEN THE NORMAL WATER LEVEL AND HIGH WATER LEVEL.
- STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE
- IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM FROSION AND SEDIMENTATION, DISCHARGES SHALL BE ROUTED THROUGH AN APPROVED ANIONIC POLYMER DEWATERING SYSTEM OR A SIMILAR MEASURE AS APPROVED BY THE ENFORCEMENT ANIONIC POLYMEN DEWN LEND 3151EM ON A SIMILAR MEASURE AS AFFROVED BY THE ENFORCEMENT OFFICER, DEWNTERING SYSTEMS SHOULD BE INSPECTED DAILY DURING OPERATIONAL PERIODS. THE ENFORCEMENT OFFICER, OR APPROVED REPRESENTATIVE, MUST BE PRESENT AT THE COMMENCEMENT OF DEWATERING ACTIVITIES. IF INSTALLED SOIL FROSION AND SEDIMENT CONTROL MEASURES DO NOT MINIMIZE SEDIMENT LEAVING THE DEVELOPMENT SITE, ADDITIONAL MEASURES SUCH AS ANIONIC POLYMERS OR FILTRATION SYSTEMS MAY BE REQUIRED BY THE ENFORCEMENT OFFICER.
- IE INSTALLED SOIL EROSION AND SEDIMENT CONTROL MEASURES DO NOT MINIMIZE SEDIMENT LEAVING THE DEVELOPMENT SITE, ADDITIONAL MEASURES SUCH AS ANIONIC POLYMERS OR FILTRATION SYSTEMS MAY BE REQUIRED BY THE ENFORCEMENT OFFICER.
- ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE PROPERTY OWNER SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR.
- ALL TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.
- THE FROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, ENFORCEMENT OFFICER, OR OTHER GOVERNING AGENCY.



CONSULTANTS

IL PROJECT: UGN-5129 IL LETTING ITEM: 01A IL CONTRACT: WA081

> REHABILITATE HANGAR PAVEMENTS - PHASE 2

> > **FINAL** JULY 26, 2024

OWNER



MARK	DATE	DESC	RIPTIO	N			
CMT F	CMT PROJECT NO: 24006440.00						
CAD	CAD DWG FILE:						
DESIG	DESIGNED BY: LB						

JRO

CAL

COPYRIGHT

DRAWN BY

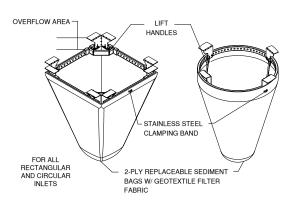
CHECKED BY:

APPROVED BY:

STORM WATER POLLUTION PREVENTION **PLAN NOTES** 

OF 18

SHEET



# INLET PROTECTION - SILT BASKET (PAVEMENT AND TURF)

### NOTES

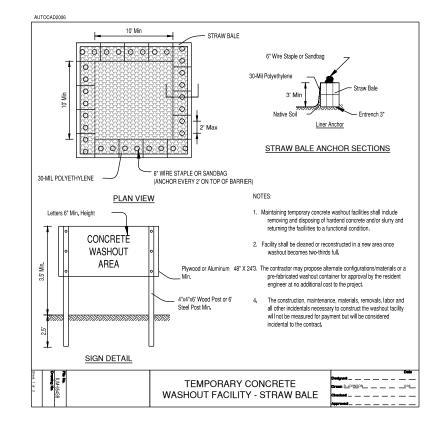
- CONTRACTOR SHALL CLEAR DEBRIS PER THE MANUFACTURER'S
   RECOMMENDATIONS BUT NOT LESS THAN WHEN THE CAPACITY FOR
   SEDIMENT STORAGE HAS BEEN REDUCED BY HALF.
- 2. FILTER FABRIC SHALL MEET THE MATERIAL REQUIREMENTS OF SPECIFICATION 592, TABLE 1, CLASS 2 OF THE ILLINOIS URBAN MANUAL.

SEED TABLE						
CLASS - TYPE	SEED MIXTURE	RATE (LB/ACRE)	APPLICATION WINDOW			
IDOT AERONAUTICS (PERMANENT SEEDING)	INFERNO TALL FESCUE OR TARHEEL II FESCUE ANNUAL RYEGRASS AUDUBON RED FESCUE RESCUE 911 OR RELIANT IV HARD FESCUE ENDOPHYTIC FESCUE CULTIVAR	60 20 30 30 60	APRIL 1 THRU JUNE 1 AUGUST 1 THRU NOVEMBER 1			
TEMPORARY EROSION CONTROL SEEDING I	OATS	100 LB/ACRE	MARCH 1 THRU JULY 31 *			
TEMPORARY EROSION CONTROL SEEDING II	WINTER WHEAT	100 LB/ACRE	AUGUST 1 THRU NOVEMBER 15*			

\* NOTE: WHEN TEMPORARY SEEDING CAN NOT BE COMPLETED DUE TO APPLICATION WINDOW RESTRICTIONS CONTRACTOR SHALL APPLY A LIGHT-DUTY HYDRAULIC MULCH (1000 LB/ACRE MINIMUM) IN ITS PLACE. CONTRACTOR SHALL REAPPLY HYDRAULIC MULCH, AS NECESSARY, TO MINIMIZE EROSION (COST INCIDENTAL TO TEMPORARY SEEDING).

MULCH TYPE				
LIGHT-DUTY HYDRAULIC	APPLIED PER MANUFACTURER'S RECOMMENDATION (2000 LB/ACRE MINIMUM)			
EROSION CONTROL BLANKET	80% WOOD EXCELSIOR MAT WITH 6" - OR LAYER FABRIC (0.63 LB/SY)			

FERTILIZ	ZER MIX
NUTRIENT	PLAT (LB/ACRE)
NITROGEN	90
PHOSPHORUS (P205)	90
POTASSIUM (K20)	90



### DEWATERING - FILTRATION BAG NOTES:

- THE SEDIMENT FILTER BAG SHALL BE SIZED PER MANUFACTURER RECOMMENDATIONS AND BASED ON THE SIZE OF THE PUMP HOWEVER, THE MINIMUM BAG SIZE SHALL BE 10 FEET X 15 FEET WITH A USABLE SURFACE DRAINAGE AREA OF 300 SQUARE FEET (10 X 15 X 2) SIDES, TOP & BOTTOM.
- THE LARGEST DIAMETER SIZE PUMP HOSE TO BE USED WITH A SEDIMENT FILTRATION BAG IS 4-INCH. MULTIPLE HOSES/PIPES SHALL NOT BE ATTACHED TO A SINGLE FILTRATION BAG INLET SI FEVE
- 3. BAG FABRIC SHALL MEET ON OF THE FOLLOWING:
  - A. WOVEN GEOTEXTILE SHALL MEET THE MATERIAL SPECIFICATIPONS OF TABLE 1, CLASS 4
     OF THE ILLINOIS URBAN MANUAL
  - B. NONWOVEN GEOTEXTILE SHALL MET THE MATIERIAL SPECIFICATIONS OF TABLE 2, CLASS I OF THE ILLINOIS URBAN MANUAL EXCEPT IT WILL HAVE A MINIMUM TENSILE STRENGTH OF 200 POUNDS.
- 4. SEDIMENT FILTRATION BAGS SHALL BE PLACED ON A STABILIZED SURFACE AREA.
- SEDIMENT FILTRATION BAGS SHALL NOT BE PLACED, WHOLE OR PARTIALLY, WITHIN 50-FEET OF AQUATIC AREAS (WETLANDS, STREAMS, ETC.), OR WATER CONVEYANCE FEATURES (DITCHES, SWALES, ETC.).
- 6. SEDIMENT FILTRATION BAGS SHALL BE RAISED ABOVE THE SUPPORTING GROUND ON A SURFACE, OR MATERIAL, THAT ALLOWS WATER TO FLOW OUT OF THE BOTTOM OF THE BAG AT THE RESPECTIVE DESIGN DISCHARGE RATE FOR THE SEDIMENT FILTER BAG SELECTED. THE CONTRACTOR MAY PALCE THE BAG ON CELAN OPEN AGGREGATE (6" MINIMUM THICKNESS), STRAW BALES OR OTHER POROUS SURFACE APPROVED BY THE RESIDENT ENGINEER.
- 7. THE CONSTRUCTION, MAINTENANCE, MATERIALS, REMOVALS AND DISPOSAL AND ALL OTHER INCIDENTALS NECESSARY TO CONSTRUCT THE DEWATERING FACILITY WILL NOT BE MEASURED FOR PAYMENT BUT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



CONSULTANTS

IL PROJECT: UGN-5129 IL LETTING ITEM: 01A IL CONTRACT: WA081

> REHABILITATE HANGAR PAVEMENTS - PHASE 2

> > FINAL JULY 26, 2024

OWNER



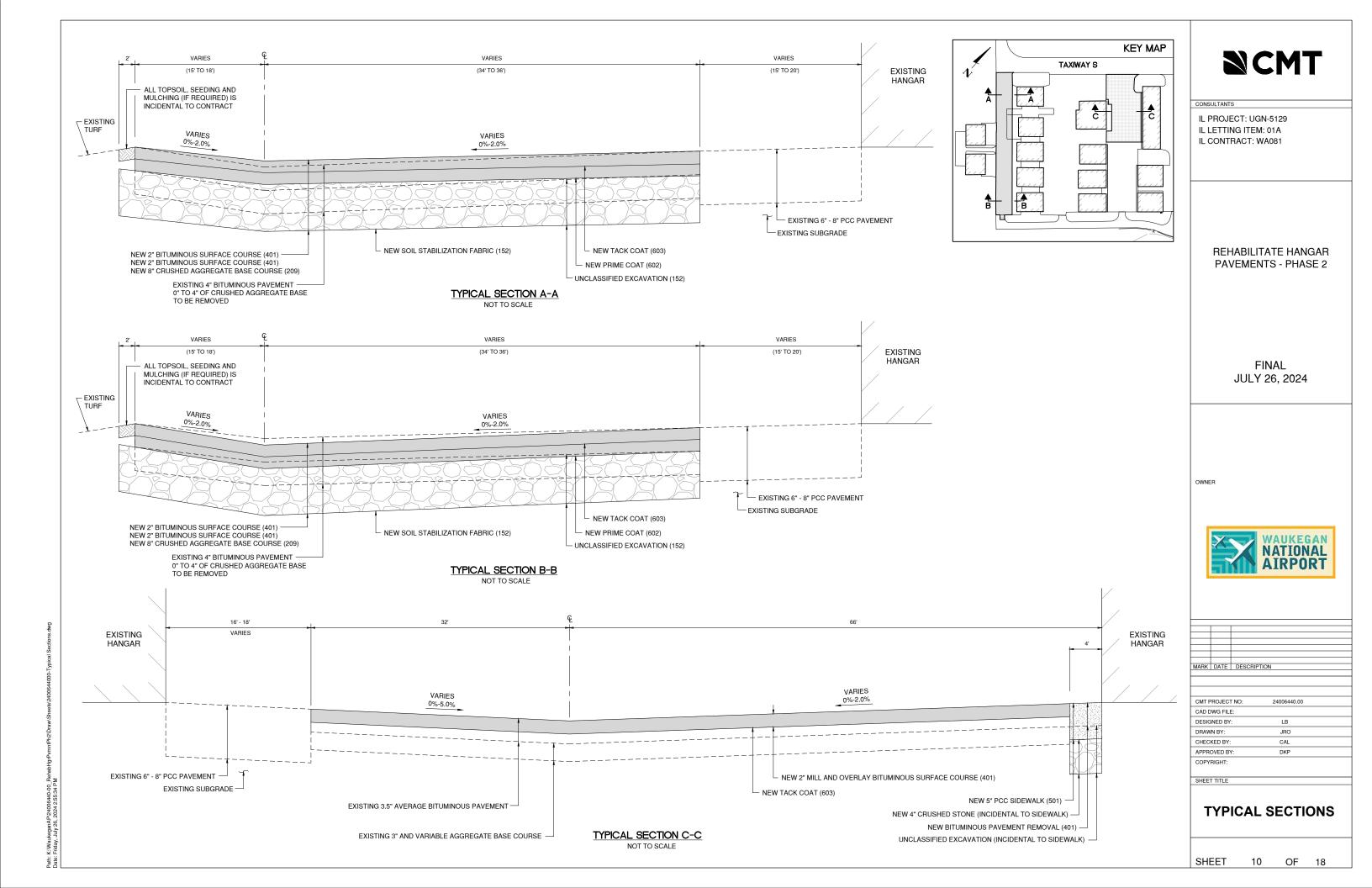
MARK	DATE	DESCRIPTION

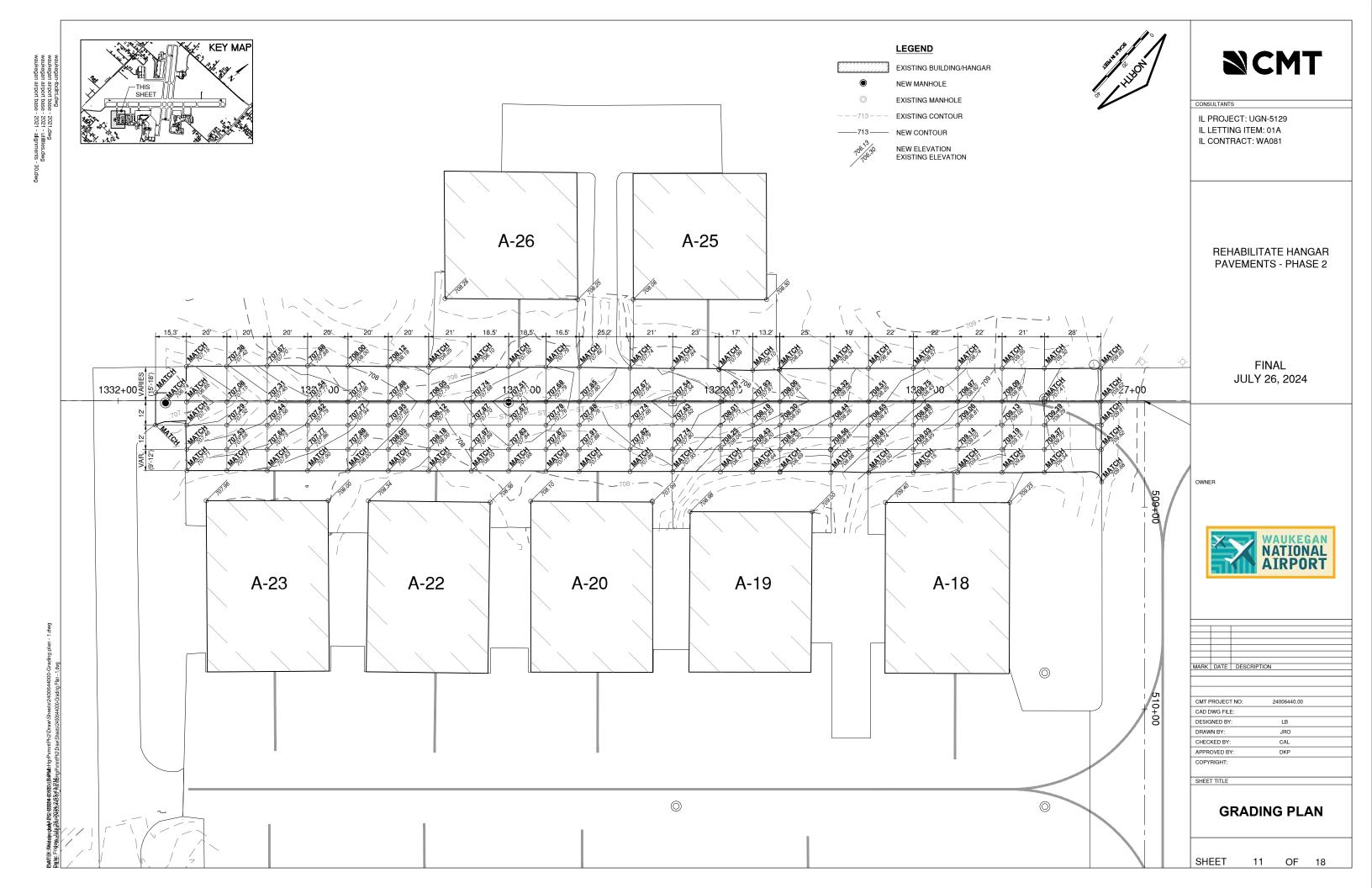
CMT PROJECT NO:	24006440.00	
CAD DWG FILE:		
DESIGNED BY:	LB	
DRAWN BY:	JRO	
CHECKED BY:	CAL	
APPROVED BY:	DKP	
COPYRIGHT:		

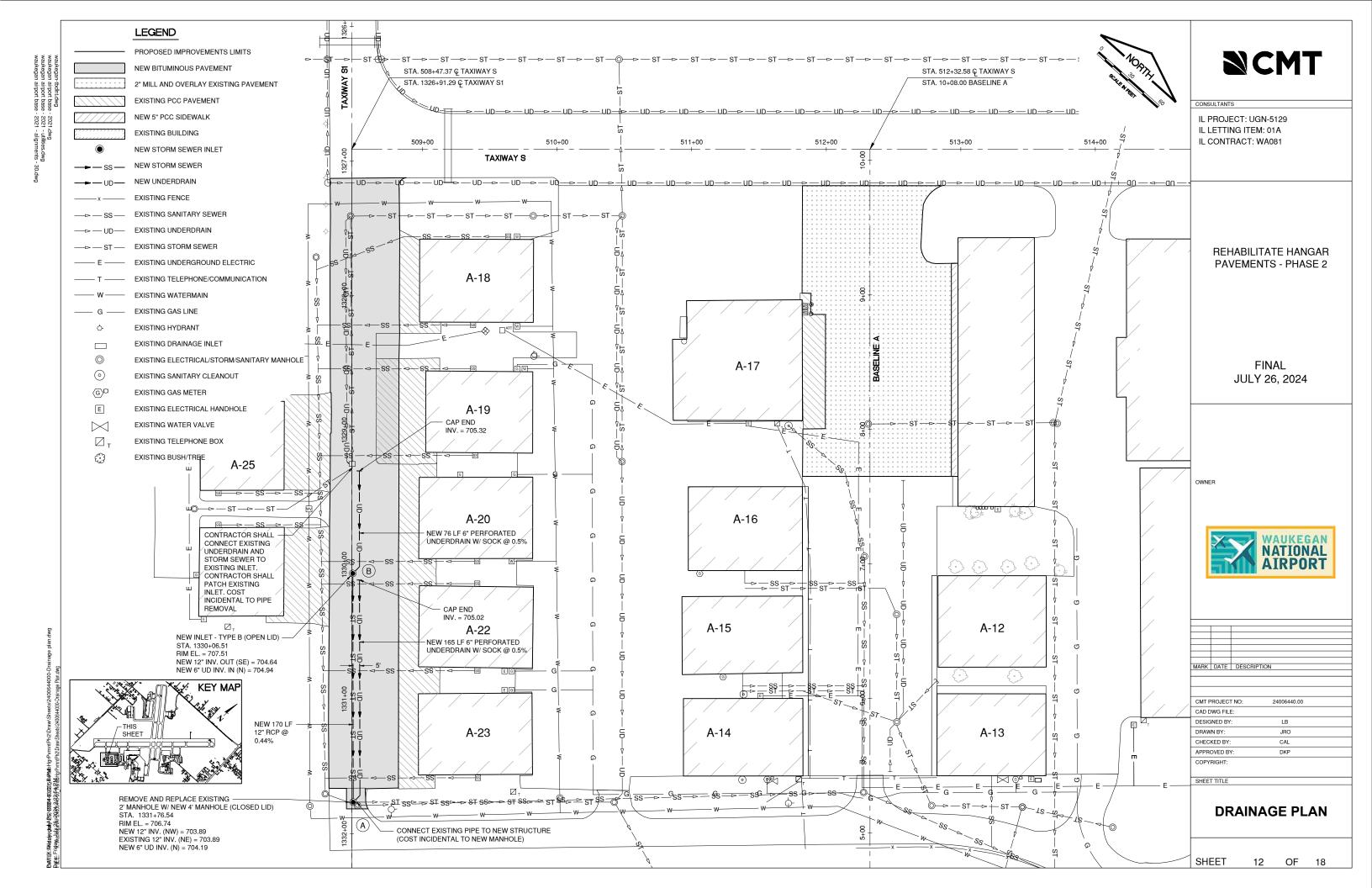
SHEET TITLE

STORM WATER
POLLUTION
PREVENTION PLAN
DETAILS

SHEET 9 OF 18







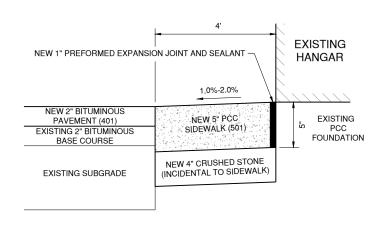
EXCAVATION OF EXISTING SUBGRADE AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT SHALL BE MEASURED AND PAID FOR UNDER AR152511 SUBGRADE REPAIR

### SUBGRADE REPAIR DETAIL

NOT TO SCALE

### NOTES

- THIS DETAIL APPLIES TO THE SUBGRADE REPAIR FOR THE SOUTHWEST PAVEMENT (BASELINE C).
- LOCATION OF SUBGRADE REPAIR SHALL BE DETERMINED AT THE TIME OF CONSTRUCTION AND APPROVED BY THE RESIDENT ENGINEER
- MEASUREMENT AND PAYMENT SHALL BE MADE UNDER AR152511 SUBGRADE REPAIR AND SHALL INCLUDE ALL EXCAVATION (SUBGRADE) AND FULL DEPTH REPLACEMENT WITH POROUS GRANULAR EMBANKMENT TO MEET THE FINAL GRADES OF THE ADJACENT SUBGRADE TO REMAIN.



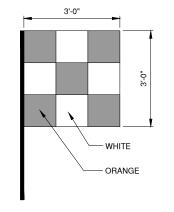
### CONCRETE SIDEWALK DETAIL

NOT TO SCALE

NOTE: JOINT DIMENSIONS SHALL MEET MANUFACTURER'S RECOMMENDATION.

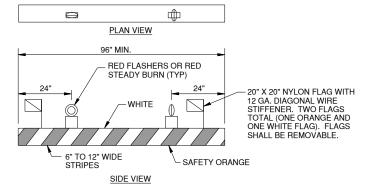
# STORM SEWER/UNDERDRAIN NOTES

- CONTRACTOR SHALL FIELD VERIFY EXISTING STORM SEWER/UNDERDRAIN INVERTS BEFORE INSTALLING PROPOSED PIPE, CONNECTIONS AND ORDERING MATERIALS.
- ALL UNDERDRAIN CONNECTIONS, CORING INTO STRUCTURES, CAPS, TEES, BENDS, STORM SEWER ETC. SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE UNDERDRAIN.
- 3. UNDERDRAIN SLOPES FOLLOW EDGE OF PAVEMENT SLOPE UNLESS OTHERWISE NOTED.
- 4. UNDERDRAIN CONFLICTS WITH EXISTING CONDITIONS SHALL BE RESOLVED AND COST SHALL BE INCIDENTAL TO UNDERDRAIN.
- PRIOR TO ORDERING AND INSTALLING ALL FIELD TILE REPLACEMENT PIPE, THE CONTRACTOR SHALL FIELD VERIFY THE LOCATION AND INVERTS OF EXISTING FIELD TILE CONNECTIONS. ADJUSTMENTS SHALL BE MADE AS NECESSARY AT NO ADDITIONAL COST TO THE CONTRACT
- CORING OF DRAINAGE STRUCTURE AND REMOVAL OF EXISTING STORM SEWER MANHOLE/INLET CONCRETE BENCHES TO FACILITATE CONNECTIONS OF PROPOSED STORM SEWER PIPE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE PIPE.



# CONSTRUCTION EQUIPMENT AND TRUCK/VEHICLE SIGNAL FLAG

NOT TO SCALE

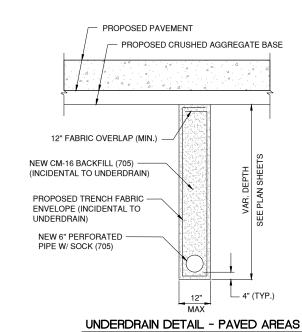


### AIRSIDE LOW PROFILE LIGHTED BARRICADE

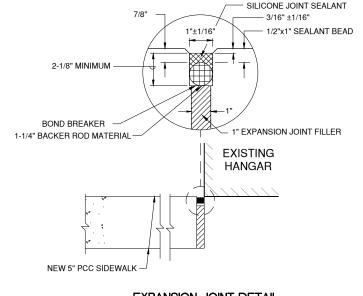
NOT TO SCALE

### BARRICADE NOTES

- 1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE
- 2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
- BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS BOTATED 90°
- 4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
- BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
- 6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA
- PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS, RUNWAY OBSTACLE FREE ZONES AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
- ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT

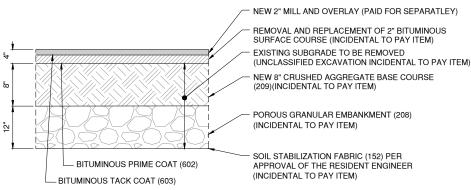


- 1. THE 6" UNDERDRAIN SHALL BE INSTALLED AFTER THE SUBGRADE IS COMPACTED.
- 2. THE SPOILS FROM THE 6" UNDERDRAIN CONSTRUCTION SHALL BE REMOVED DAILY FROM THE SURFACE OF THE CRUSHED AGGREGATE BASE



# **EXPANSION JOINT DETAIL**

NOT TO SCALE

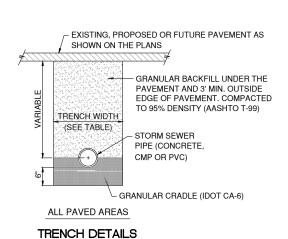


### REM AND REP BIT PAVEMENT - TYPE B DETAIL

### **NOTES**

- 1. THIS DETAIL APPLIES TO THE PAVEMENT PATCHING REQUIRED FOR THE NORTHEAST PAVEMENT (BASELINE A).
- DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY SOIL BORINGS. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
- 3. REM & REP BIT PAVEMENT TYPE B QUANTITIES ARE ESTIMATED. THE RESIDENT ENGINEER SHALL LAY OUT REM & REP BIT PAVEMENT - TYPE B AREAS IN THE FIELD DURING CONSTRUCTION
- AS FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION, FURNISHING AND PLACING OF POROUS GRANULAR EMBANKMENT AND/OR REMOVAL AND REPLACEMENT OF AGGREGATE BASE SHALL BE DONE AT SECTIONS WHERE THE REM & REP RIT PAVEMENT - TYPE R IS LITH IZED. THIS SHALL RE DONE TO REPAIR DAMAGED LINDERLYING PAVEMENT OR SOFT SUBGRADE AS DETERMINED BY THE RESIDENT ENGINEER BASED ON RESULTS OF THE GEOTECHNICAL TESTING OR PROOF ROLL. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES

INSIDE DIAMETER OF STORM SEWER (INCHES)	MAXIMUM TRENCH WIDTH	MAXIMUM PAVEMENT REMOVAL WIDTH	INSIDE DIAMETER OF STORM SEWER (INCHES)	MAXIMUM TRENCH WIDTH	MAXIMUM PAVEMENT REMOVAL WIDTH
6	3'-7"	5'-7"	48	7'-8"	9'-8"
8	3'-9"	5'-9"	54	8'-3"	10'-3"
12	4'-2"	6'-2"	60	8'-10"	10'-10"
15	4'-6"	6'-6"	66	9'-5"	11'-5"
18	4'-9"	6'-9"	72	10'-0"	12'-0"
21	5'-0"	7'-0"	78	10'-7"	12'-7"
24	5'-4"	7'-4"	84	11'-2"	13'-2"
27	5'-7"	7'-7"	90	11'-9"	13'-9"
30	5'-11"	7'-11"	96	12'-4"	14'-4"
36	6'-6"	8'-6"	102	12'-11"	14'-11"
42	7'-1"	9'-1"	108	13'-6"	15'-6"



NOT TO SCALE



CONSULTANTS

IL PROJECT: UGN-5129 IL LETTING ITEM: 01A IL CONTRACT: WA081

> REHABILITATE HANGAR PAVEMENTS - PHASE 2

> > **FINAL** JULY 26, 2024

OWNER



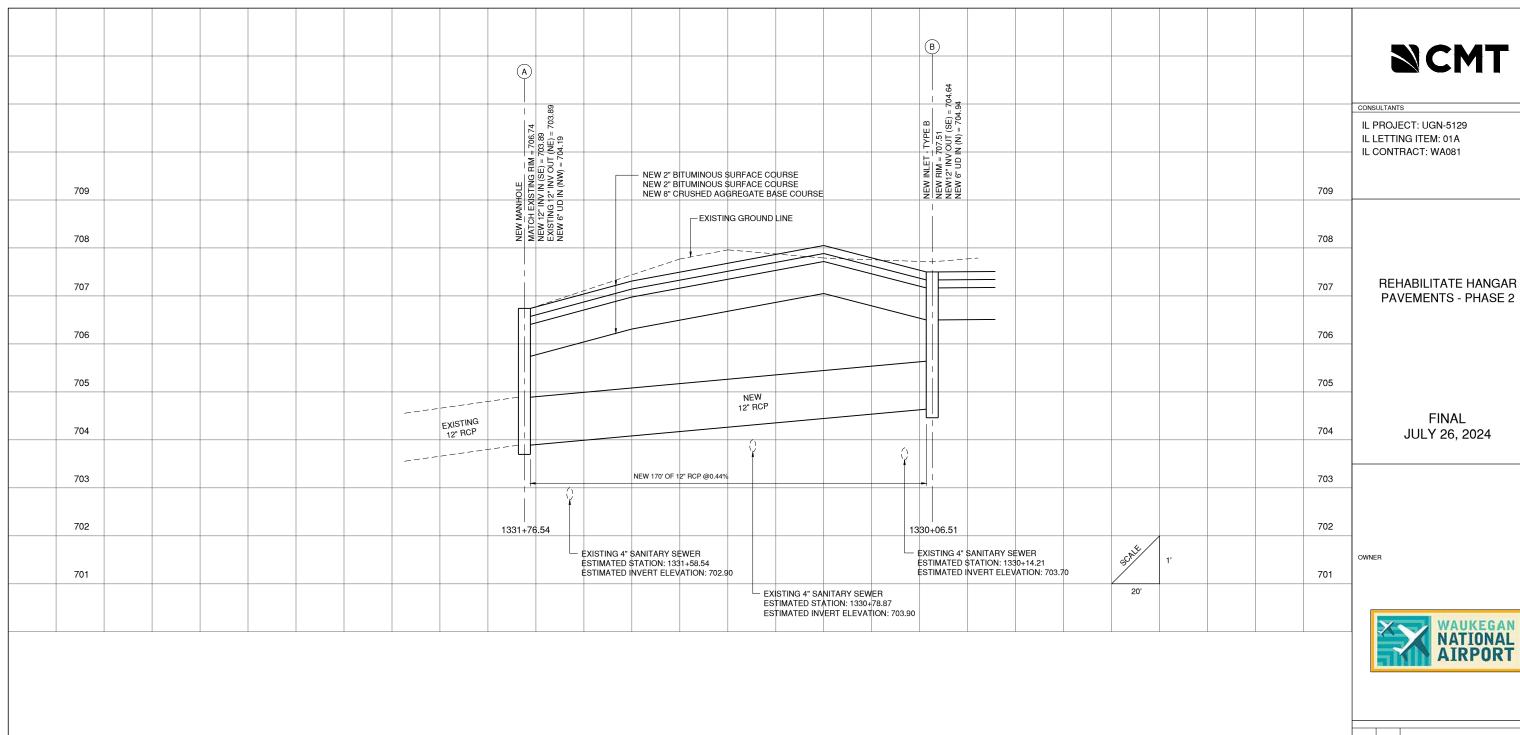
MARK	DATE	DESCRIPTION				
CMT PROJECT NO: 24006440.00						
CAD	CAD DWG FILE:					
DESIGNED BY:			LB			
DRAWN BY: JRO						
CHECKED BY: CAL						
APPR	OVED BY	<b>/</b> :	DKP			
COPY	RIGHT:					

**DRAINAGE AND MISCELLANEOUS DETAILS** 

SHEET

18

13 OF



# NOTES

- IT IS CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT CONTRACTOR'S EXPENSE.
- WHENEVER A PROPOSED STORM SEWER CROSSES EXISTING SANITARY SEWER, THE
  MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE STORM SEWER TO THE BOTTOM
  OF THE SANITARY SEWER SHALL BE 18 INCHES OR VICE VERSA. FURTHERMORE, A
  MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SANITARY SEWERS AND STORM
  SEWERS SHALL BE MAINTAINED.

MARK DATE DESCRIPTION

CMT PROJECT NO: 24006440.00

CAD DWG FILE:
DESIGNED BY: LB
DRAWN BY: JRO

APPROVED BY: COPYRIGHT:

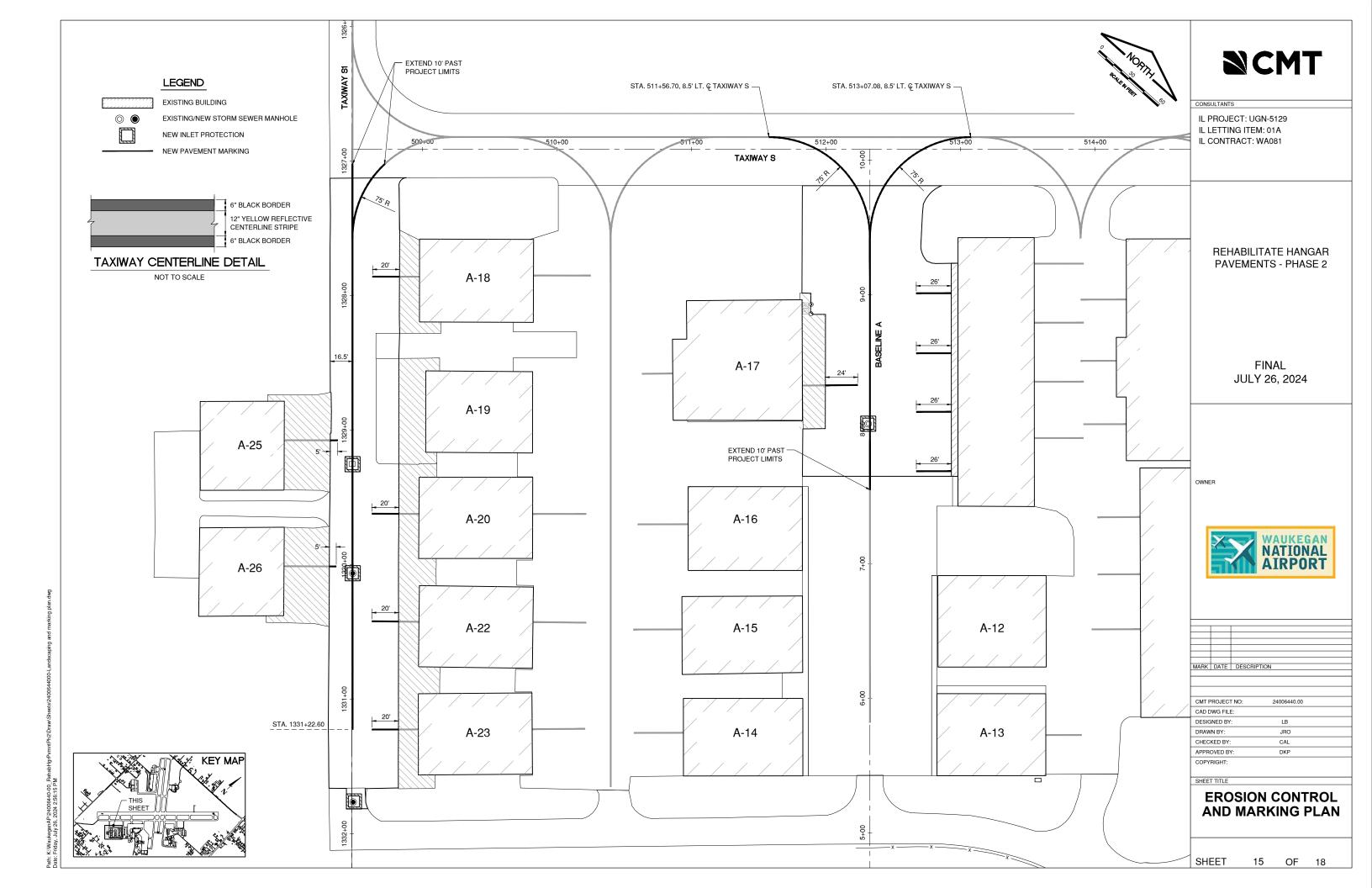
CHECKED BY:

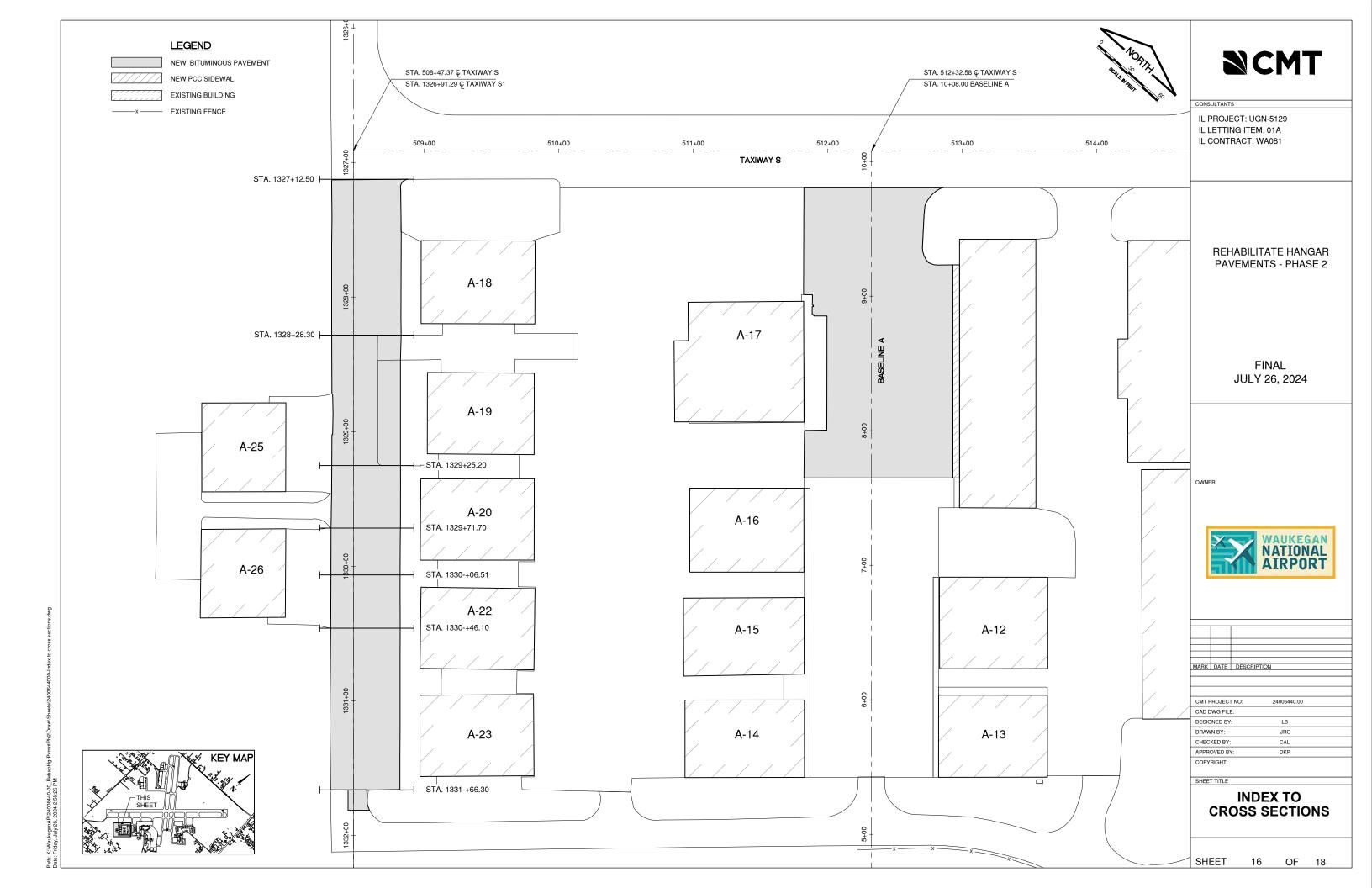
STORM SEWER PROFILES

CAL

DKP

SHEET 14 OF 18



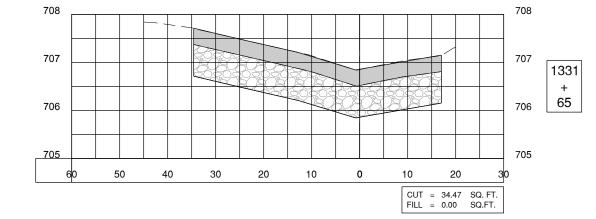


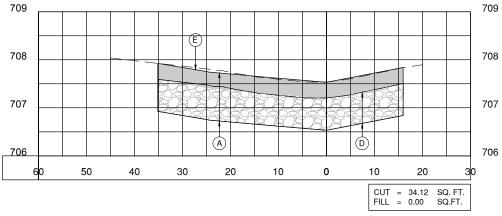
# LEGEND

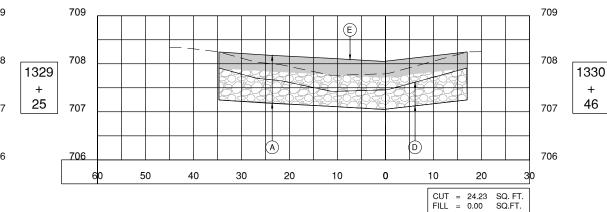
- (A) NEW 4" HMA PAVEMENT (401) NEW 8" CRUSHED AGGREGATE BASE COURSE (209)
- (8) EXISTING 4" HMA PAVEMENT TO BE REMOVED (401)
- © EXISTING GROUND LINE
- D UNCLASSIFIED EXCAVATION (152)
- (E) NEW GROUND LINE

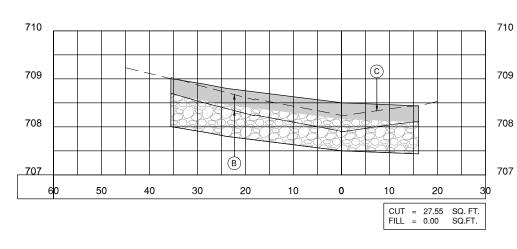
## NOTES

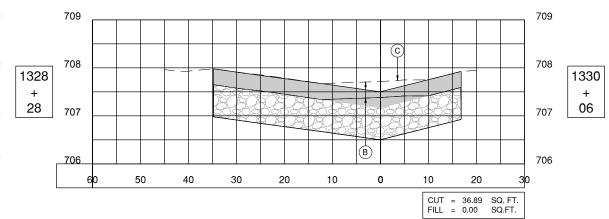
- 1. SEE GRADING PLAN FOR ELEVATIONS.
- 2. EXISTING AND NEW UTILITIES ARE NOT SHOWN FOR CLARITY. SEE EXISTING CONDITIONS AND REMOVALS FOR APPROXIMATE UTILITY LOCATIONS.

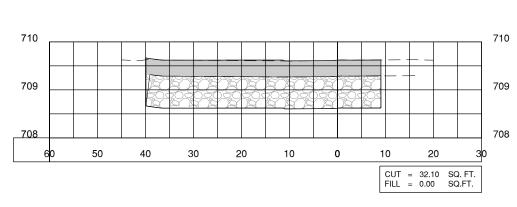


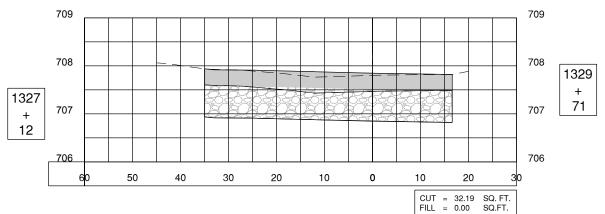














CONSULTANTS

IL PROJECT: UGN-5129 IL LETTING ITEM: 01A IL CONTRACT: WA081

> REHABILITATE HANGAR PAVEMENTS - PHASE 2

> > FINAL JULY 26, 2024

OWNER



MARK DATE DESCRIPTION

CMT PROJECT NO: 24006440.00

CAD DWG FILE:

DESIGNED BY: LB

DRAWN BY: JRO

CAL

DKP

APPROVED BY: COPYRIGHT:

SHEET TITLE

CHECKED BY:

**CROSS SECTIONS** 

SHEET 17 OF 18

GEO Job No. 24002 SOIL BORING LOG

Page <u>1</u> of <u>1</u> Date \_\_2/28/24\_\_

Hollow Stem Auger HAMMER TYPE CME Automatic 4" ASPHALT LEAN CLAY with SAND-brown and black-very stiff to hard

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASSHGO T209), GP-Geoprobe Hand Auger
BBS, from 137 (Rev. 2-89)

Page <u>1</u> of <u>1</u> SOIL BORING LOG

Date 2/28/24 Hollow Stem Auger HAMMER TYPE CME Automatic

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer). The SPT (N value) is the sum of the last two blow values in each sampling zone (AA3960) 7206), GP-Geoprobe Hand Auger (BRS, front 371 (Rev. 8-39)).



SOIL BORING LOG Page 1 of 1

LOCATION Waukegan Nation	nal Air	port							
COUNTY Cook DRI	LLING	ME	тнс	D _		Hol	llow Stem Auger	HAMMER TYPE _	CME Automatic
CLIENT         CMT           BORING NO.         R B-1           Northing         2094550           Easting         1109139           Ground Surface Elev.         706.7	_ _ _ _ ft	P T H	B L O W S	S Qu	M O I S T	DRY DENSITY	Surface Water Elev. Stream Bed Elev. Groundwater Elev.: First Encounter Upon Completion After - Hrs.	n/a ftn/a ft	
3.5" ASPHALT	706.4		,,	(	1 - 70	l ·			
LEAN CLAY with SAND-brown and gray-soft		_	5		4				
		_	2		8				
		=	1						
	701.7	-5	2	0.25 P	29				
CLAY LOAM-brown-very stiff		_	6						
		=	10 12	3.50 P	19	112			
SANDY SILT-gray-medium dense	698.2	_	6						
	696.7	-10	10		20				
End Of Boring @ -10.0°. Boring backfilled with bentonite chips and patched with concrete upon completion.									
		_							
		-15							
		_							
		Ξ							

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (A.7366) 7206), GP-Geoprobe Hand Auger



SOIL BORING LOG

Date 2/28/24 Hollow Stem Auger HAMMER TYPE CME Automatic LEAN CLAY-brown and gray-hard

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AX366) CT26), GP-Geoprobe Hand Auger
1895, Krom 137 (Rev. 8-99)

PAVEMENT CORE

Northing - 2094466.433 , Easting - 1109153.021 ASPHALT-well consolidated, fine to medium aggregates-Surface mix. ASPHALT-well consolidated, fine to medium coarse aggregates-Binder mi Northing - 2094432.721, Easting - 1109177.924 ASPHALT-well consolidated, fine to medium aggregates-Surface mix. ASPHALT-well consolidated, fine to medium coarse aggregates-Binder mi

Northing - 2094186.342, Easting - 1108956.997 ASPHALT-well consolidated, fine to medium aggregates-Surface mix, ASPHALT-well consolidated, fine to medium coarse aggregates-Binde Northing - 2094084.696, Easting - 1109003.291 ASPHALT-well consolidated, fine to medium aggregate-Surface mix. ASPHALT-well consolidated, fine to medium coarse aggregate-Binder mix

Location:Waukegan National Airport, Waukegan, IL 60087

CORE THICKNESS (in.)

R C-2



CONSULTANTS

GEO Job No. 24002

Page <u>1</u> of <u>1</u>

IL PROJECT: UGN-5129 IL LETTING ITEM: 01A IL CONTRACT: WA081

> REHABILITATE HANGAR PAVEMENTS - PHASE 2

> > FINAL JULY 26, 2024

OWNER

Page: 1 of 3

GSI Job No.: <u>24002</u>
Date: <u>02/08/2024</u>
Cored By: <u>ROB</u>
Checked By: <u>SP</u>



MARK DATE DESCRIPTION

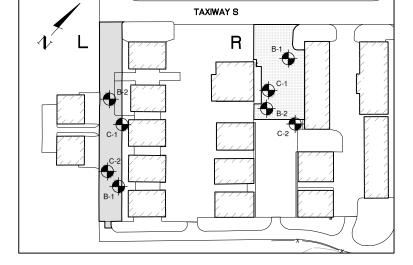
CMT PROJECT NO: 24006440.00 CAD DWG FILE: DESIGNED BY DRAWN BY: JRO CHECKED BY: CAL APPROVED BY: COPYRIGHT

**GEOTECHNICAL ENGINEERING INFORMATION** 

SHEET 18 OF 18

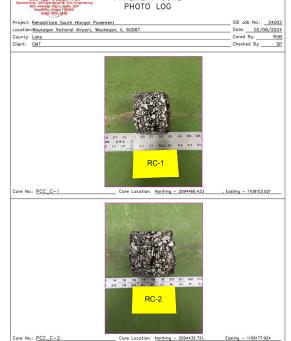
THE GEOTECHNICAL INFORMATION (OR ANY PORTIONS THEREOF) ARE PROVIDED ONLY AS AVAILABLE INFORMATION. THE CONTRACTOR MAY DRAW HIS OWN CONCLUSIONS FROM THE DATA SHOWN. THE SOILS INFORMATION IS NOT REPRESENTATIVE OF ALL SOIL WHICH MIGHT BE ENCOUNTERED WITHIN THE LIMITS OF THE PROJECT. THE CONTRACTOR SHALL BY HIS OWN MEANS, SATISFY HIMSELF AS TO THE EXISTING SITE AND GEOTECHNICAL CONDITIONS FOR DETERMINING COST, MEANS, METHODS, TECHNIQUES AND SEQUENCES OF CONSTRUCTION.

Page: 2 of 3



BORE/CORE LOCATIONS				
R	NORTHING	EASTING		
B-1	2094550.12	1109138.62		
B-2	2094432.72	1109177.92		
C-1	2094466.43	1109153.02		
C-2	2094450.07	1109249.39		
L	NORTHING	EASTING		
B-1	2094076.10	1109044.89		
B-2	2094210.34	1108897.25		
C-1	2094186.34	1108957.00		
C-2	2094084.70	1109003.29		





PAVEMENT CORE

Core No: HMA_C-1  Core Location: Northing - 2094186.342.  Easting - 1108956.997	Geo Services Inc. Ceotechnicol Environmentaria de diel Engineering 805 Ambreita-Gout Justie 204 Naport-init, Justie 204 (642)-435-4358	PAVEMENT CORE PHOTO LOG	Page: <u>3 of 3</u>		
Core No.: HMA_C-1  Core No.: HMA_C-1  Core Location: Northing - 2994186.342.  Easting - 1108956.997					
Core No: HMA_C-1  Core Location: Northing - 2094186.342.  Easting - 1108956.997					
The 15 de 17 de 27 de 28 de 29 de 28 de 18 de 28	County: Lake				
Core No: HMA_C-1  Core Location: Nerthing - 2094186.342.  Easting - 1108956.597	Client: CMT		Checked By:SP		
15 (a 12 ) 10 (a 12 ) 14 P P P P P P P P P P P P P P P P P P		22 12 02 61 61 61 91 91 91 91 91			
LC-2	Core No.: HMA_C-1	Core Location: Northing - 2094186.342,	Eosting - 1108956.997		
Core No.: HMA_C-2		12 05 91 01 71 91 31 41 CI SI II			
	Core No.: HMA_C-2	Core Location; Northing - 2094084.696,	Easting - 1109003.291		

Nop Nop	SERVICES Inc. PAVEMENT CORE PHOTO LOG  (Sub-2001-2005) PHOTO LOG	Page: <u>3 of 3</u>		
	abilitate South Hangar Pavement	GSI Job No.:		
	kegan National Airport, Waukegan, IL 60087	Date: 02/08		
County: Lake		Cored By:		
Client: CMT		Checked By:	SP	
	to 15 No. 17 No. 18 No. 20 No.			
Core No.: <u>HI</u>	MA_C-1 Core Location: Northing - 2094186.342,	Easting — 1108956.	997	
	(5 15 1) (1			
	LC-2			