



PROP. CURVE GWT\_EX3-1  
 PI STA. = 50+71.46  
 $\Delta = 90^\circ 00' 00''$  (RT)  
 $D = 114^\circ 35' 30''$   
 $R = 50.00'$   
 $T = 50.00'$   
 $L = 78.54'$   
 $E = 20.71'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 50+21.46$   
 $P.T. \text{ STA.} = 51+00.00$

ALIGNMENT COORDINATES - GREAT WESTERN TRAIL			
ACCESS DRIVE	STATION	NORTH	EAST
P.O.T.	50+03.12	1,902,874.75	1,072,451.55
P.C.	50+21.46	1,902,892.98	1,072,453.50
P.T.	51+00.00	1,902,937.35	1,072,508.56
P.O.T.	52+74.88	1,902,918.67	1,072,682.44

PROP. CURVE GWT\_CL\_REV-2  
 PI STA. = 19+65.57  
 $\Delta = 81^\circ 48' 51''$  (RT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 86.64'$   
 $L = 142.79'$   
 $E = 32.31'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 18+78.92$   
 $P.T. \text{ STA.} = 20+21.72$

PROP. CURVE GWT\_CL\_REV-1  
 PI STA. = 18+34.29  
 $\Delta = 56^\circ 06' 08''$  (LT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 53.29'$   
 $L = 97.92'$   
 $E = 13.31'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 17+81.01$   
 $P.T. \text{ STA.} = 18+78.92$

PROP. CURVE GWT\_CL\_REV-5  
 PI STA. = 25+49.70  
 $\Delta = 21^\circ 53' 19''$  (LT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 19.34'$   
 $L = 38.20'$   
 $E = 1.85'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 25+30.36$   
 $P.T. \text{ STA.} = 25+68.57$

PROP. CURVE GWT\_CL\_REV-4  
 PI STA. = 23+87.23  
 $\Delta = 62^\circ 36' 31''$  (RT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 60.81'$   
 $L = 109.27'$   
 $E = 17.04'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 23+26.41$   
 $P.T. \text{ STA.} = 24+35.69$

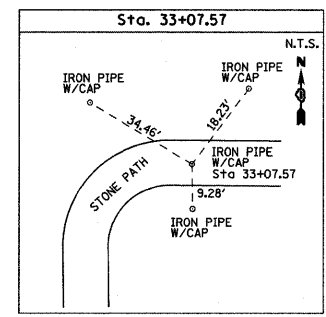
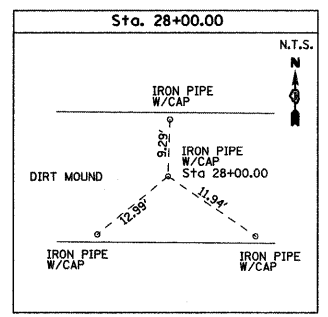
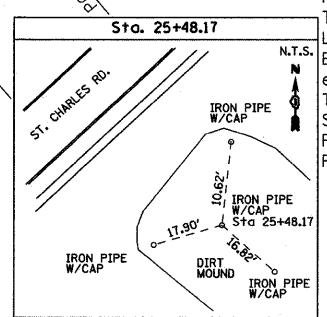
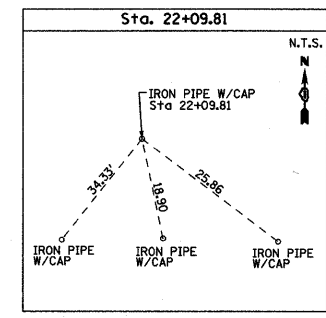
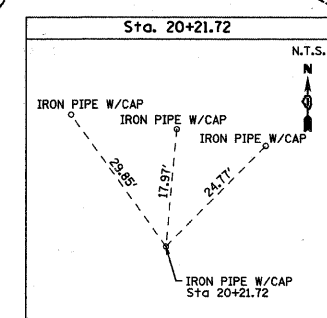
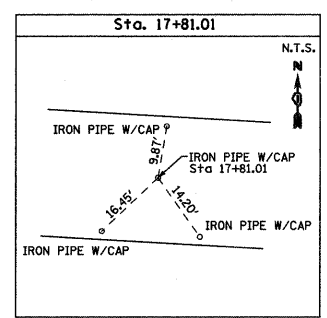
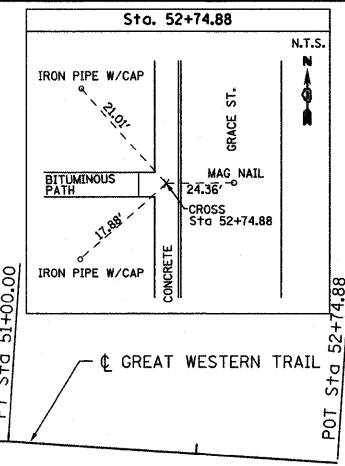
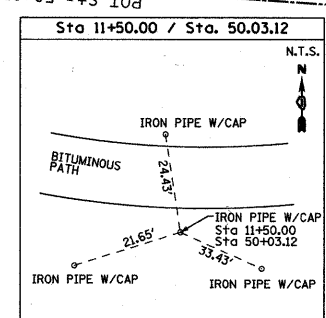
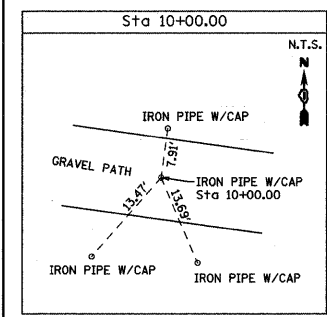
PROP. CURVE GWT\_CL\_REV-3  
 PI STA. = 22+75.76  
 $\Delta = 66^\circ 48' 32''$  (LT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 65.95'$   
 $L = 116.60'$   
 $E = 19.79'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 22+09.81$   
 $P.T. \text{ STA.} = 23+26.41$

PROP. CURVE GWT\_CL\_REV-6  
 PI STA. = 26+23.51  
 $\Delta = 12^\circ 43' 50''$  (RT)  
 $D = 57^\circ 17' 45''$   
 $R = 100.00'$   
 $T = 11.16'$   
 $L = 22.22'$   
 $E = 0.62'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 26+12.35$   
 $P.T. \text{ STA.} = 26+34.57$

ALIGNMENT COORDINATES - GREAT WESTERN TRAIL			
GREAT WESTERN TRAIL	STATION	NORTH	EAST
P.O.T.	10+00.00	1,902,890.77	1,072,302.40
P.C.	17+81.01	1,902,807.34	1,073,078.94
P.R.C.	18+78.92	1,902,843.41	1,073,165.80
P.T.	20+21.72	1,902,865.48	1,073,294.89
P.C.	22+09.81	1,902,768.35	1,073,455.97
P.R.C.	23+26.41	1,902,772.80	1,073,565.99
P.T.	24+35.69	1,902,780.80	1,073,669.60
P.C.	25+30.36	1,902,737.98	1,073,754.04
P.T.	25+68.87	1,902,727.54	1,073,790.55
P.I.	28+00.00	1,902,707.37	1,074,021.10
P.O.T.	33+00.00	1,902,652.15	1,074,518.05

SITE BENCHMARKS:  
 DATUM = NAVD88

- (BM1) CROSS CUT ON NORTHEAST FLANGE OF FIRST FIRE HYDRANT WEST OF THE GREAT WESTERN TRAIL ON NORTH SIDE OF ST. CHARLES ROAD. ELEVATION = 716.63' (NAVD88)
- (BM2) DUPAGE COUNTY BRASS DISK ON WEST SIDE OF GRACE ST. BEYOND SIDEWALK, +/-180' SOUTH OF PRAIRIE AVE. ELEVATION = 712.12' (NAVD88)
- (BM3) CROSS CUT IN SOUTHWEST FLANGE BOLT OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF ST. CHARLES PL. & S. EDGEWOOD AVE. ELEVATION = 719.48' (NAVD88)



FILE NAME = M:\756-004\_Lombard - GWT Br-edges Phase II\CADD Sheets\0756004-INT-111.dgn

**B** Bollinger, Lach & Associates, Inc.  
 ITASCA, ILLINOIS

USER NAME = delj	DESIGNED - BD	REVISED -
PLOT SCALE = 50.0000' / IN.	DRAWN - DC	REVISED -
PLOT DATE = 7/22/2011	CHECKED - BD	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
 GREAT WESTERN TRAIL**

ALIGNMENT AND BENCHMARKS			
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS
	06-00151-00-BR	DUPAGE	201 11
CONTRACT NO. 63568			
SCALE: 1"=50'	SHEET NO.	SHEETS	STA. TO STA.

ILLINOIS FED. AID PROJECT
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