

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3586	11-00056-00-RS	WILL	35	1
		ILLINOIS	CONTRACT NO. 63621	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**

**FAU ROUTE 3586 (BOUGHTON ROAD)
SUNSHINE DR TO N SCHMIDT RD
RESURFACING AND RECONSTRUCTION
SECTION 11-00056-00-RS
PROJECT M-9003(834)
VILLAGE OF BOLINGBROOK
WILL COUNTY**

C-91-580-11

FOR INDEX OF SHEETS, SEE SHEET NO. 2



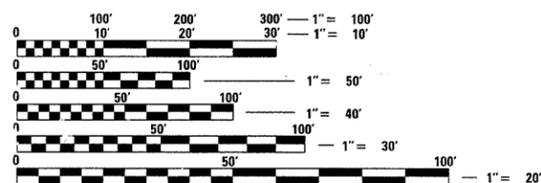
LOCATION OF SECTION INDICATED THUS: - [black rectangle] -

REMPE-SHARPE
CONSULTING ENGINEERS
IL P.D.F. LICENSE NO. 184-000895
324 WEST STATE STREET - GENEVA, ILLINOIS 60134
Telephone (830) 232-0827 - Fax (830) 232-1829

DESIGN CLASSIFICATION: MINOR ARTERIAL

TRAFFIC DATA -

POSTED SPEED LIMIT: 45 MPH
2011 ADT: SUNSHINE DRIVE TO WEBER ROAD - 14,500
WEBER ROAD TO SCHMIDT ROAD - 28,500

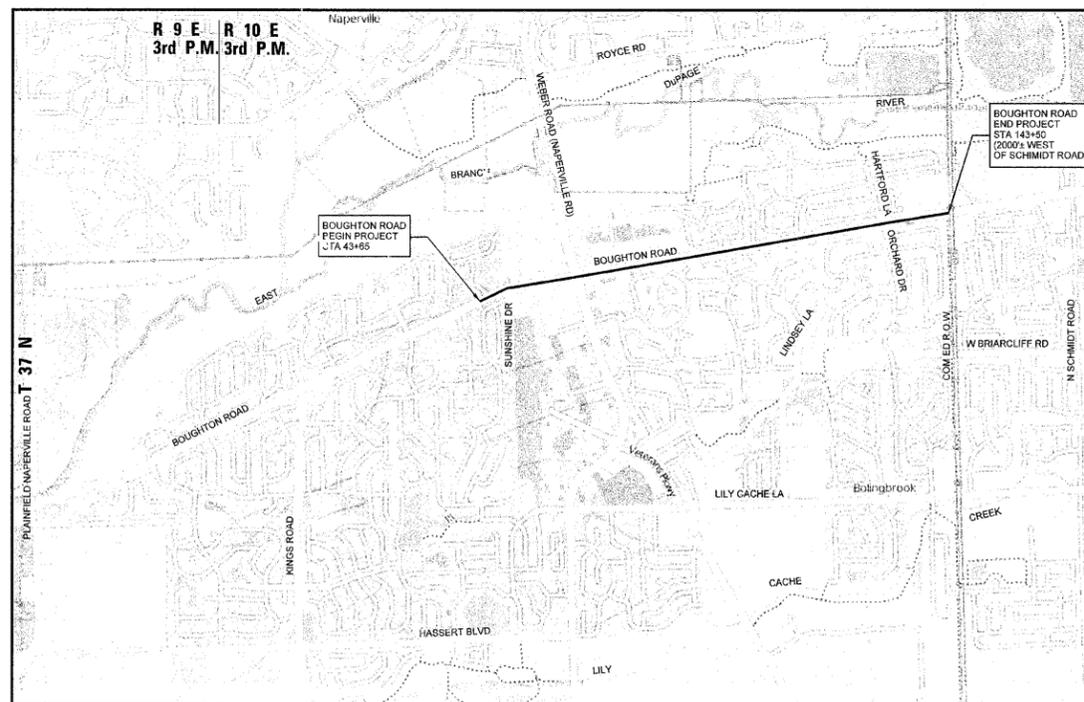


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

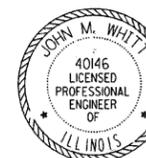
PROJECT ENGINEER - DOUGLAS W RANNEY II, P.E.
PROJECT MANAGER - JOHN M WHITT, P.E.

CONTRACT NO. 63621



LOCATION MAP - NOT TO SCALE

GROSS LENGTH = 9,985 FT. = 1.89 MILES
NET LENGTH = 9,985 FT. = 1.89 MILES



John M. Whitt
JOHN M. WHITT
ILLINOIS LICENSED PROFESSIONAL ENGINEER
NO. 40146 EXPIRES 11-30-11
REMPE-SHARPE CONSULTING ENGINEERS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED *Michael J. Drey*
BOLINGBROOK, VILLAGE ENGINEER

PASSED *July 19, 2011*
Cheryl A. ...
DISTRICT ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW *July 21, 2011*
Dwain M. O'Keefe
DEPUTY OF DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

PROGRAM AND OFFICE ENGINEER: CHARLES F RIDDLE, P.E. (847) 705-4406, SCHAUMBURG, IL

1. SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS:

ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JAN. 1, 2007 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS); THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2011; THE LATEST EDITION OF THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN IN ILLINOIS, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS"; THE "DETAILS" ON THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST STANDARD OF THE DEPARTMENT.

2. EXISTING UTILITIES:

EXISTING UTILITIES ARE SHOWN ON THE PLANS ACCORDING TO INFORMATION OBTAINED FROM THE UTILITY COMPANIES, VILLAGE, AND FIELD SURVEYS. THE ACCURACY AND COMPLETENESS OF SAID INFORMATION IS NOT GUARANTEED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE EXISTENCE, NATURE, AND EXACT LOCATIONS OF ALL UTILITY LINES AND APPURTENANCES WITHIN THE LIMITS OF THE IMPROVEMENTS. THE CONTRACTOR SHALL GIVE PRIOR NOTIFICATION TO THE UTILITY COMPANIES AND THE VILLAGE OF HIS INTENTION TO BEGIN WORK.

SHOULD CERTAIN EXISTING UTILITY FACILITIES INTERFERE WITH THE PROPOSED CONSTRUCTION, THEY WILL HAVE TO BE REMOVED AND RELOCATED BY THE UTILITY COMPANIES. THE CONTRACTOR SHALL COORDINATE AND SCHEDULE THIS WORK WITH ALL AFFECTED UTILITY COMPANIES AND THE VILLAGE SO AS TO INSURE THE TIMELY COMPLETION AND MAINTENANCE OF SAID UTILITY FACILITIES.

THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR THE INTERFERENCE WITH OR DAMAGE TO ANY EXISTING UTILITY FACILITY AND APPURTENANCES SUCH AS WATER MAINS, SEWERS, GAS MAINS, ELECTRIC AND SIGNAL CABLES, CONDUITS, ETC., RESULTING FROM THE OPERATIONS OF HIS EQUIPMENT OR MEN AND SHALL REPAIR OR REPLACE SAME AT HIS OWN EXPENSE AND WITH THE LEAST POSSIBLE DELAY. NO EXTRA COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR FOR ANY EXPENSES INCURRED BECAUSE OF THE DELAYS, INCONVENIENCES, OR INTERRUPTIONS TO HIS WORK RESULTING FROM COMPLIANCE WITH THE ABOVE REQUIREMENTS.

3. PROTECTION OF PUBLIC/PRIVATE PROPERTY:

THE CONTRACTOR SHALL PROTECT ALL EXISTING TREES, PARKWAYS, SHRUBS, FENCES, DRAIN LINES, POWER LINES, AND OTHER PUBLIC/PRIVATE PROPERTY. ANY ITEM THAT IS DAMAGED SHALL BE REPLACED OR REPAIRED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER AT NO COST TO THE CONTRACTOR.

4. CONNECTIONS TO EXISTING STORM SEWERS:

ANY ADDITIONAL COSTS INVOLVED IN CONNECTING PROPOSED STORM SEWER LINES TO EXISTING STORM SEWER STRUCTURES AND PIPES SHALL BE CONSIDERED INCIDENTAL TO THE ITEM BEING CONSTRUCTED AND NO ADDITIONAL COMPENSATION ALLOWED.

5. EXISTING STREET CLEANLINESS:

THE CONTRACTOR SHALL KEEP EXISTING AND ADJACENT STREETS CLEAN OF DIRT, MUD, AND OTHER DEBRIS AND, WHEN NECESSARY, CLEAN SAID PAVEMENTS ON A DAILY BASIS. THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

6. REMOVAL, MAINTENANCE AND RESETTING EXISTING, STREET SIGNS AND STOP SIGNS:

EXISTING STREET SIGNS, AND STOP SIGNS WHICH INTERFERE WITH CONSTRUCTION SHALL BE REMOVED AND TEMPORARILY RELOCATED DURING CONSTRUCTION BY THE CONTRACTOR. AFTER THE WORK HAS BEEN COMPLETED, SAID SIGNS SHALL BE RESET AT A LOCATION SPECIFIED BY THE ENGINEER. THE COST OF THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE CONTRACT WITH NO ADDITIONAL COMPENSATION ALLOWED. ANY SIGN DAMAGED OR LOST SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. TEMPORARY STOP SIGNS SHALL BE PLACED AS REQUIRED BY THE VILLAGE AS PART OF THE TRAFFIC CONTROL AND PROTECTION PAY ITEM

7. EXPANSION JOINTS IN CONCRETE CONSTRUCTION:

EXPANSION JOINTS OF THE THICKNESS SPECIFIED SHALL BE PLACED AT ALL LOCATIONS SHOWN ON THE DRAWINGS AS SPECIFIED IN THE SPECIAL PROVISIONS, OR AS DIRECTED BY THE ENGINEER. THE COST OF FURNISHING AND PLACING ALL EXPANSION JOINT MATERIAL SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT PRICE FOR THE VARIOUS CONCRETE CONSTRUCTION ITEMS OF WORK.

8. BEFORE STARTING ANY EXCAVATIONS, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES (48 HOUR NOTIFICATIONS IS REQUIRED).

GENERAL NOTES:

9. BARRICADES: (AS NEEDED)

THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE. ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.

10. THE CONTRACTOR SHALL MAKE ALL SAW CUTS REQUIRED FOR THE REMOVAL OF CONCRETE CURB AND GUTTER, CONCRETE SIDEWALK, CONCRETE DRIVEWAYS, BITUMINOUS DRIVEWAYS AND BITUMINOUS PAVEMENTS OR AS DIRECTED BY THE ENGINEER. THE COST FOR SAW CUTTING SHALL BE INCLUDED TO THE ASSOCIATED WORK ITEM AND SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT.

11. THE CONTRACTOR SHALL COORDINATE ALL TEMPORARY TRAFFIC SIGNAL OPERATIONS WITH THE VILLAGE OF BOLINGBROOK. TEMPORARY OPERATIONS SHALL BE LIMITED TO THE HOURS OF 7:00 AM TO 5:00 PM, MONDAY THROUGH FRIDAY.

12. WORK HOURS MAY BEGIN NO EARLIER THAN 7:00 AM, MONDAY THROUGH FRIDAY, AND NO EARLIER THAN 8:00 AM ON SATURDAY AND SUNDAY.

13. ALL DAY OPERATIONS WHICH REQUIRE SHIFTING EASTBOUND TRAFFIC TO THE WESTBOUND LANES SHALL ONLY BE PERMITTED ONE BLOCK AT A TIME AND SHALL BE LIMITED TO THE HOURS OF 7:00 AM TO 5:00 PM, MONDAY THROUGH FRIDAY.

14. ALL INTERSECTIONS SHALL BE OPENED EACH NIGHT TO ALLOW TWO-WAY TRAFFIC OVER A MINIMUM 24-FOOT WIDE TEMPORARY SURFACE. TEMPORARY ACCESS MAY OPTIONALLY BE PROVIDED BY USE OF STEEL PLATES, AT NO ADDITIONAL COMPENSATION TO TEMPORARY ACCESS - ROAD.

15. ALL PRIVATE AND COMMERCIAL ENTRANCES SHALL BE OPENED EACH NIGHT TO ALLOW ACCESS OVER A MINIMUM 12-FOOT WIDE (PRIVATE) AND 24-FOOT WIDE (COMMERCIAL) TEMPORARY SURFACE. (A COMMERCIAL DRIVE MAY BE CLOSED FOR NO MORE THAN FIVE (5) CALENDAR DAYS, IF THE SITE HAS MULTIPLE ACCESS DRIVES.) TEMPORARY ACCESS MAY OPTIONALLY BE PROVIDED BY USE OF STEEL PLATES, AT NO ADDITIONAL COMPENSATION TO TEMPORARY ACCESS - PRIVATE ENTRANCE OR TEMPORARY ACCESS - COMMERCIAL ENTRANCE.

16. SPECIAL COORDINATION SHALL BE REQUIRED TO ASSURE 24-HOUR ACCESS, OVER A MINIMUM 24-FOOT WIDE TEMPORARY SURFACE AT FIRE STATION NO. 4, AT NO ADDITIONAL COMPENSATION TO TEMPORARY ACCESS - COMMERCIAL ENTRANCE.

17. AT A MINIMUM, ONE 11-FOOT WIDE EASTBOUND LANE MUST BE RE-OPENED EACH NIGHT. SPECIAL ATTENTION IS CALLED TO BOUGHTON ROAD EAST OF LANDINGS DRIVE AND BOUGHTON ROAD EAST OF WEBER ROAD. NO ADDITIONAL COMPENSATION TO TEMPORARY ACCESS - ROAD.

18. PUMPING, IF REQUIRED TO MAINTAIN DRAINAGE OR FOR THE CONTRACTOR'S CONVENIENCE, SHALL NOT BE COMPENSATED FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

19. NO BARRICADES OR UNEVEN SURFACES SHALL BE ALLOWED OVER WINTER. ANY AREAS MILLED SHALL BE RESURFACED IN THE SAME SEASON. ANY DETECTOR LOOPS REMOVED SHALL BE REPLACED IN THE SAME SEASON.

20. PAVEMENT RECONSTRUCTION OVER THE COMED DUCT SHALL BE TO A DEPTH OF 22-INCHES BELOW NORMAL SUB-GRADE, OR TO THE TOP OF THE DUCT, WHICHEVER IS LESS. EXCAVATION OVER THE COMED VAULTS SHALL BE TO THE TOP OF THE PRE-CAST STRUCTURES, AND MAY EXTEND LATERALLY AS DIRECTED BY THE ENGINEER.

21. ADJUSTMENTS AND RECONSTRUCTION - IN THE SPECIAL PROVISION FOR ADJUSTMENTS AND RECONSTRUCTIONS, ALL REFERENCES TO "CLASS PP CONCRETE" AND "CLASS PP-1 CONCRETE" SHALL BE REPLACED WITH "HIGH EARLY STRENGTH CONCRETE". THIS SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

22. POROUS GRANULAR EMBANKMENT, SUBGRADE - IN THE SPECIAL PROVISION FOR POROUS GRANULAR EMBANKMENT, SUBGRADE, THE MATERIAL SHALL BE FURTHER MODIFIED SO TO BE LIMITED TO VIRGIN MATERIAL ONLY; NO RE-CYCLE MATERIAL WILL BE PERMITTED. THIS SHALL BE INCLUDED IN THE COST FOR POROUS GRANULAR EMBANKMENT, SUBGRADE (SPECIAL)

INDEX OF SHEETS

NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS, APPLICABLE HIGHWAY STANDARDS, GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	TYPICAL SECTIONS
5 - 6	SCHEDULE OF QUANTITIES
7 - 8	ALIGNMENT PLANS
9 - 14	BOUGHTON ROAD RESURFACING PLANS
15	MAINTENANCE OF TRAFFIC NOTES AND DETAILS
16 - 23	MAINTENANCE OF TRAFFIC / SUGGESTION STAGING PLANS
24	CONSTRUCTION DETAILS
25 - 35	DISTRICT 1 STANDARD DETAILS

APPLICABLE HIGHWAY STANDARDS

STANDARD	TITLE
000001-06	Standard Symbols, Abbreviations and Patterns
424001-05	Curb Ramps for Sidewalks
606001-04	Concrete Curb Type B and Combination Concrete Curb and Gutter
701426-04	Lane Closure, Multilane, Intermittent or Moving Oper., for Speeds ≥ 45 MPH
701427	Lane Closure, Multilane, Intermittent or Moving Oper., for Speeds ≤ 40 MPH
701601-07	Urban Lane Closure, Multilane, 1W or 2W With Nontraversable Median
701701-07	Urban Lane Closure, Multilane Intersection
701801-04	Lane Closure Multilane 1W or 2W Crosswalk or Sidewalk Closure
701901-01	Traffic Control Devices
780001-02	Typical Pavement Markings

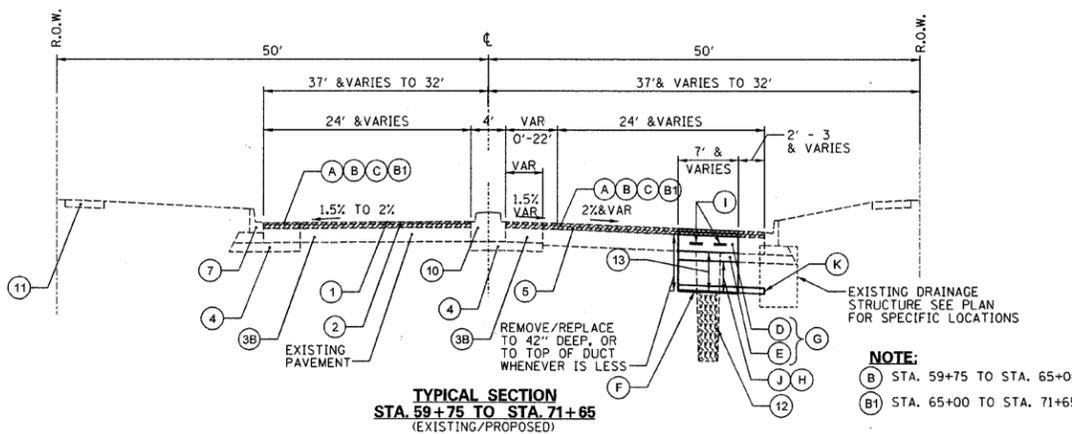
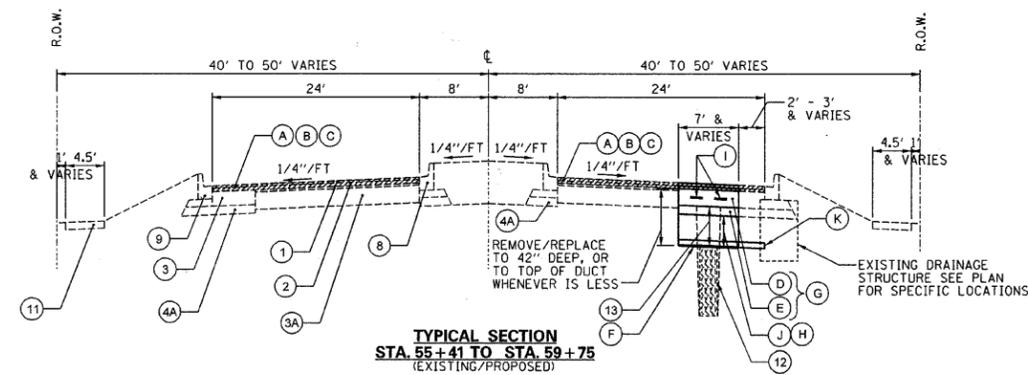
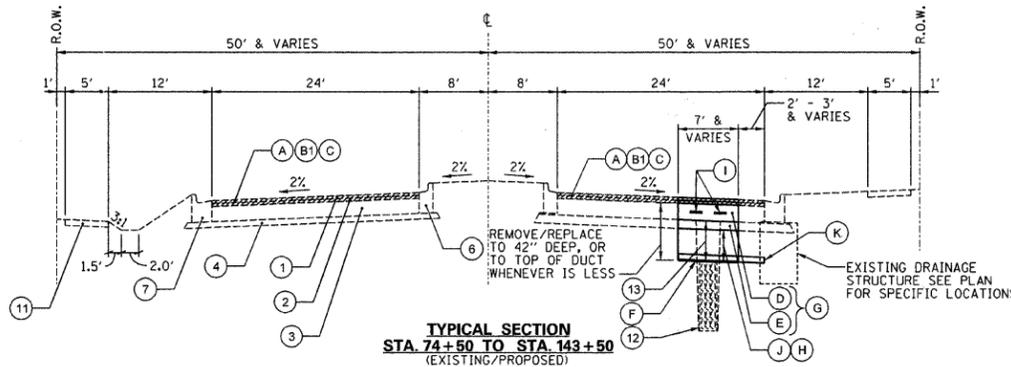
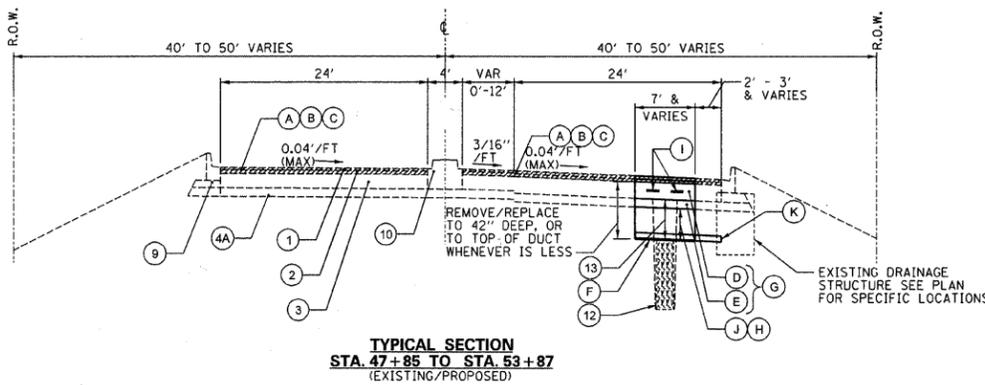
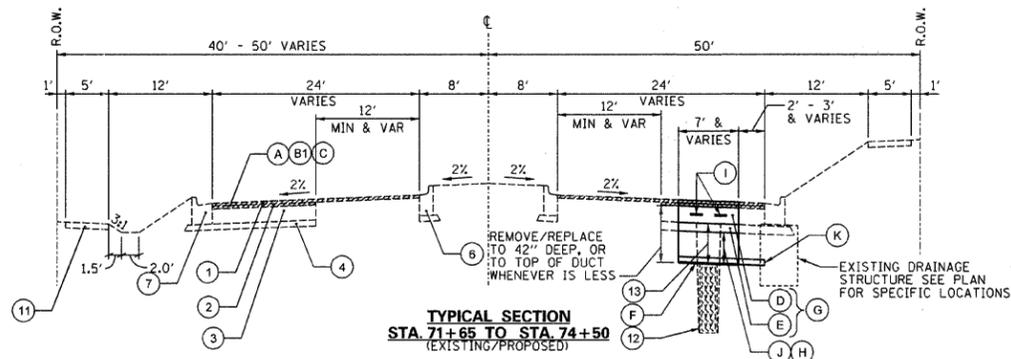
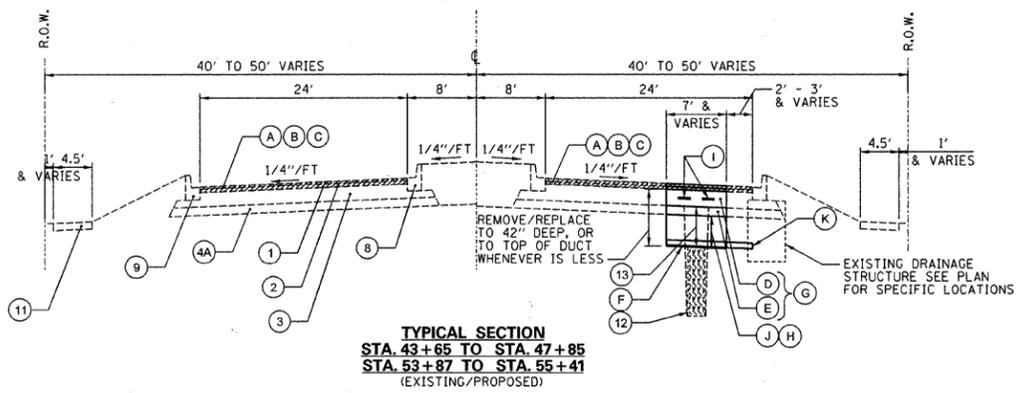
DISTRICT 1 STANDARD DETAILS

STANDARD	TITLE
BD-08	Details for Frames and Lids Adjustment with Milling
BD-22	Pavement Patching for HMA Surface Pavement
BD-24	Curb or Curb and Gutter Removal and Replacement
BD-32	Butt Joint and HMA Taper Details
TC-10	Traffic Control and Protection for Sideroads, Intersections and Driveways
TC-13	District 1 Typical Pavement Markings
TC-14	Traffic Control and Protection at Twin Bays (to Remain Open to Traffic)
TC-16	Pavement Marking Letters and Symbols for Traffic Staging
TC-22	Arterial Road Information Signs
TC-26	Driveway Entrance Sign
TS-07	Detector Loop Installation Details for Roadway Resurfacing

FILE NAME =	USER NAME = _USER_	DESIGNED - J.W	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, APPLICABLE HIGHWAY STANDARDS, AND GENERAL NOTES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\dgn\BB\BB04BoughtonRdResurfacing\sheet\BB04Notes.dgn	DRAWN - G.R	REVISED -	3586			11-00056-00-RS	WILL	35	2	
PLOT SCALE = 1:100 Ft / IN.	CHECKED - E.J / D.R.	REVISED -	CONTRACT NO. 63621							
PLOT DATE = 7/21/2011	DATE -	REVISED -	SCALE:			SHEET NO. 2 OF 35 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	

SUMMARY OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNITS	TOTAL QUANTITY		CONSTRUCTION TYPE CODE
			STP 0005	TRAINEE 0042	
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	200	200	
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	600	600	
21101615	TOPSOIL, FURNISH AND PLACE, 4"	SQ YD	500	500	
25000100	SEEDING, CLASS 1	ACRE	0.1	0.1	
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	9	9	
25000500	PHOSPHOROUS FERTILIZER NUTRIENT	POUND	9	9	
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	9	9	
25100630	EROSION CONTROL BLANKET	SQ YD	500	500	
28000510	INLET FILTERS	EACH	60	60	
35501330	HOT-MIX ASPHALT BASE COURSE, 11-1/2"	SQ YD	7754	7754	
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	6852	6852	
40600300	AGGREGATE (PRIME COAT)	TON	135	135	
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10	10	
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75 N50	TON	2878	2878	
40600895	CONSTRUCTING TEST STRIP	EACH	2	2	
40600982	HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	848	848	
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1560	1560	
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90	TON	5120	5120	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5-INCH	SQ FT	1911	1911	
42400800	DETECTABLE WARNINGS	SQ FT	192	192	
44000100	PAVEMENT REMOVAL	SQ YD	7754	7754	
44000159	HOT MIX ASPHALT SURFACE REMOVAL, 2-1/2"	SQ YD	67676	67676	
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	62	62	
44000600	SIDEWALK REMOVAL	SQ FT	1911	1911	
44201670	CLASS D PATCHES, TYPE 1, 2 INCH	SQ YD	500	500	
44201690	CLASS D PATCHES, TYPE 1, 4 INCH	SQ YD	500	500	
60605900	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.12	FOOT	10	10	
60607400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.24	FOOT	52	52	
67100100	MOBILIZATION	LSUM	1	1	
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	20	20	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	20000	20000	
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1165	1165	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	12000	12000	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	8860	8860	
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	580	580	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	807	807	
Δ 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS	SQ FT	1165	1165	
Δ 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4730	4730	
Δ 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	11115	11115	
Δ 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	580	580	
Δ 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	807	807	
78300100	PAVEMENT MARKING REMOVAL	SQ FT	700	700	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	24	24	
Δ 88600600	DETECTOR LOOP REPLACEMENT	FOOT	3142	3142	
* XX007860	AGGREGATE BASE COURSE, SPECIAL	SQ YD	7754	7754	
* X2020410	EARTH EXCAVATION (SPECIAL)	CU YD	4741	4741	
* X0325110	BIAXIAL GEOGRID	SQ YD	7542	7542	
* X0326160	POROUS GRANULAR EMBANKMENT, SUBGRADE (SPECIAL)	CU YD	200	200	
* X0326911	TRANSVERSE DRAINS COMPLETE	EACH	38	38	
* X3112900	SUBBASE GRANULAR MATERIAL, (SPECIAL)	CU YD	4609	4609	
* X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	6	6	
* X4023000	TEMPORARY ACCESS (ROAD)	EACH	8	8	
* X4430200	REFLECTIVE CRACK CONTROL TREATMENT, SPECIAL	FOOT	19394	19394	
* X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	16	16	
* X7010216	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	LSUM	1	1	
* Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	38	38	
* Z0076600	TRAINEES	HOURS	500		500
	* SPECIAL PROVISION Δ SPECIALTY ITEMS				



EXISTING PAVEMENT LEGEND

- ① HMA CONCRETE SURFACE COURSE MIX C, 1 1/2"-2"
- ② HMA CONCRETE BINDER COURSE, 2"-2 1/2"
- ③ HMA BASE COURSE, 10"
- ③A HMA BASE COURSE
- ③B PCC BASE COURSE, 10"
- ④ SUB-BASE GRANULAR MATERIAL, TY B, 4"
- ④A RECYCLED BIT. GRINDINGS, 6"
- ⑤ LEVEL BINDER
- ⑥ B-6.12 COMB CONC CURB & GUTTER
- ⑦ B-6.24 COMB CONC CURB & GUTTER
- ⑧ B-9.12 COMB CONC CURB & GUTTER
- ⑨ B-9.24 COMB CONC CURB & GUTTER
- ⑩ CONC MEDIAN
- ⑪ PCC SIDEWALK, 5"
- ⑫ COMMONWEALTH EDISON DUCT (COVER VARIES, 30" MIN.)
- ⑬ FLOWABLE FILL (VARIES)

PROPOSED PAVEMENT LEGEND

- (A) HMA SURFACE REMOVAL (2 1/2") (INCLUDES AREA OVER TRENCH)
- (B) HMA SURFACE COURSE, MIX "D", N70 (1 3/4")
- (B1) HMA SURFACE COURSE, MIX "F", N90 (1 3/4")
- (C) POLYMERIZED LEVEL BINDER (MACHINE METHOD), IL-4.75, N50 (3/4")
- (D) HOT MIX ASPHALT BASE COURSE, 11 1/2"
- (E) AGGREGATE BASE COURSE, SPECIAL (6") (TY B, VIRGIN CA-6 ONLY, NO RE-CYCLED CA - 6)
- (F) BIAXIAL GEOGRID - 6 1/2' WIDE
- (G) PAVEMENT REMOVAL (EXISTING BASE COURSE AND SUB-BASE)
- (H) SUB-BASE GRANULAR MATERIAL (SPECIAL) (TY C (CA-7) CRUSHED STONE, VIRGIN CA-7 ONLY, NO RECYCLED CA-7)
- (I) REFLECTIVE CRACK CONTROL TREATMENT - SPECIAL (18" WIDE) EACH
- (J) EARTH EXCAVATION, SPECIAL (INCLUDES FLOWABLE FILL)
- (K) TRANSVERSE DRAIN (SEE DETAIL SHEET 24)

NOTE:

PAVEMENT BASE COURSE/ SUB-BASE REMOVAL AND RECONSTRUCTION IS LIMITED TO THAT AREA OVER THE EXISTING COMMONWEALTH EDISON DUCT, TYPICALLY IN OUTSIDE, EAST BOUND LANE. HOWEVER, COM ED TRENCH LOCATION IS APPROXIMATE. EXACT LOCATION OF TRENCH IS VARYING AT DIFFERENT LOCATIONS.

HOT MIX ASPHALT MIXTURE REQUIREMENT
CONTRACTOR SHALL MILL BEFORE PATCHING

MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT RESURFACING (STA. 43+65 TO STA. 65+00)	
HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70 IL 9.5mm, 1 3/4"	4% @ 90 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	4% @ 50 GYR
PAVEMENT RESURFACING (STA. 65+00 TO STA. 143+50)	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX F, N90 IL 9.5mm, 1 3/4"	4% @ 90 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	4% @ 50 GYR
PAVEMENT RECONSTRUCTION (STA. 43+65 TO STA. 65+00)	
HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70 IL 9.5mm, 1 3/4"	4% @ 90 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	4% @ 50 GYR
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19, N90), 11 1/2" (IN 4 LIFTS)	4% @ 90 GYR
PAVEMENT RECONSTRUCTION (STA. 65+00 TO STA. 143+50)	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX F, N90 IL 9.5mm, 1 3/4"	4% @ 90 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	4% @ 50 GYR
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19, N90), 11 1/2" (IN 4 LIFTS)	4% @ 90 GYR

* THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

* THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS

FILE NAME =	USER NAME = _USER_	DESIGNED - J.W	REVISED -
N:\dgn\BB\BB04BoughtonRdResurfacing\shets\BB04TypSections.dgn		DRAWN - C.R	REVISED -
PLOT SCALE = 10.00 Ft / IN.		CHECKED - E.J / D.R.	REVISED -
PLOT DATE = 7/22/2011		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS

SCALE: NONE SHEET NO. 4 OF 35 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3586	11-00056-00-RS	WILL	35	4
			CONTRACT NO. 63621	
ILLINOIS FED. AID PROJECT				

HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT		
STATION (APPROX.)	OFFSET	AREA (SQ YD)
43+65	LT	12.5
43+65	RT	12.5
47+10	LT	35
47+10	RT	40
54+60	LT	47.5
54+60	RT	42.5
58+20	LT	15
58+20	RT	13.5
59+30	RT	10
60+70	RT	10
65+60	LT	60
65+60	RT	50
69+30	RT	14.5
69+70	RT	8
70+60	LT	8
71+20	LT	8
74+10	LT	37.5
74+10	RT	37.5
80+50	LT	36.5
80+50	RT	45
86+25	LT	7.5
86+25	RT	20
96+00	LT	47.5
96+00	RT	47.5
102+70	LT	18
112+00	LT	38
112+00	RT	45
128+10	LT	45
128+10	RT	48
TOTAL		848

DETECTABLE WARNINGS		
STATION	OFFSET	DETECTABLE WARNING AREA (SQ FT)
51+00	40' LT	8
60+60	50' RT	8
70+50	45' LT	8
70+80	45' LT	8
71+35	45' LT	8
71+35	45' LT	8
73+75	44' RT	8
74+45	45' LT	8
77+10	42' RT	8
78+00	44' RT	8
80+10	44' RT	8
95+40	46' LT	8
95+40	46' RT	8
96+40	46' RT	8
96+45	40' LT	8
102+55	42' LT	8
102+95	42' LT	8
111+40	36' RT	8
111+65	50' LT	8
112+25	50' RT	8
128+55	40' LT	16
129+45	40' LT	16
TOTAL		192

COMBINATION CONCRETE CURB AND GUTTER TYPE B-9.12		
STATION	OFFSET	QUANTITY
51+90	5' LT	10
TOTAL		10

COMBINATION CONCRETE CURB AND GUTTER TYPE B-9.24		
STATION	OFFSET	C & G TY B-9.24
51+00	40' LT	10
80+50	44' LT	14
91+15	32' LT	28
TOTAL		52

SIDEWALK REMOVAL PORTLAND CEMENT CONCRETE SIDEWALK 5-INCH		
STATION	OFFSET	SIDEWALK AREA (SQ FT)
44+00	40' LT	75
48+10	40' RT	250
48+40	50' LT	25
50+60	50' LT	50
51+00	40' LT	48
60+70	50' RT	36
66+50	45' LT	96
66+70	50' RT	25
70+35	45' LT	25
70+60	45' LT	40
70+80	45' LT	30
71+10	45' LT	30
71+35	45' LT	40
73+55	42' LT	50
73+75	44' RT	48
74+45	50' LT	48
76+70	45' RT	25
77+10	42' RT	50
78+00	44' RT	50
79+60	45' LT	30
80+10	44' RT	60
81+80	46' RT	25
89+30	45' LT	50
95+50	46' RT	49
95+50	46' RT	60
96+40	46' RT	48
96+45	40' LT	75
97+80	45' LT	50
102+55	42' LT	30
102+95	42' LT	30
110+35	45' RT	25
111+40	36' RT	30
111+65	50' LT	48
112+25	46' RT	60
128+55	40' LT	100
129+45	40' LT	60
TOTAL		1911

COMBINATION CURB AND GUTTER REMOVAL COMBINATION CONCRETE CURB AND GUTTER				
STATION	OFFSET	REMOVAL	B-9.12	B-9.24
51+00	40' LT	10		10
51+90	5' LT	10		
60+50	44' LT	14		14
91+15	32' LT	28		28
TOTAL		62	10	52

FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)		
STATION	OFFSET	QUANTITY
47+05	24' LT	1
48+68	26' LT	1
51+90	6' LT	1
53+40	30' RT	1
55+20	24' LT	1
60+55	42' LT	1
62+88	32' RT	1
64+50	32' RT	1
79+75	30' RT	1
80+20	38' LT	1
82+16	38' LT	1
95+50	30' RT	1
95+50	45' RT	1
98+14	22' RT	1
111+45	32' RT	1
111+58	30' LT	1
TOTAL		16

THERMOPLASTIC PAVEMENT MARKINGS LETTERS AND SYMBOLS			
STATION	OFFSET	ONLY (SQ FT)	"ARROW" (SW FT)
45+50	3' RT	20.8	
45+75	3' RT		15.6
48+50	2' LT		15.6
48+75	2' LT	20.8	
50+00	2' RT	20.8	
50+25	2' RT		15.6
53+00	2' RT	20.8	
53+25	40' RT	20.8	15.6
53+50	40' RT		15.6
56+40	3' LT		15.6
56+65	3' LT	20.8	
58+10	40' RT	20.8	
58+35	40' RT		15.6
60+00	36' LT		15.6
60+25	36' LT	20.8	
62+40	6' LT	20.8	
62+40	6' RT	20.8	
62+65	6' LT		15.6
62+65	6' RT		15.6
64+25	6' LT	20.8	
64+25	6' RT	20.8	
64+50	6' LT		15.6
64+50	6' RT		15.6
66+65	6' LT		15.6
66+65	6' RT		15.6
66+90	6' LT	20.8	
66+90	6' RT	20.8	
68+25	40' RT	20.8	
68+50	40' RT		15.6
69+10	6' LT		15.6
69+10	6' RT		15.6
69+35	6' LT	20.8	
69+35	6' RT	20.8	
72+50	2' RT	20.8	
72+75	2' RT		15.6
74+70	2' LT		15.6
74+95	2' LT	20.8	
75+00	38' LT		15.6
75+25	38' LT	20.8	
75+55	2' LT		15.6
75+90	2' LT	20.8	
79+15	2' RT	20.8	
79+40	2' RT		15.6
81+70	38' LT		15.6
81+95	38' LT	20.8	
82+00	2' LT		15.6
82+25	2' LT	20.8	
85+25	2' RT	20.8	
85+50	2' RT		15.6
93+90	2' RT	20.8	
94+15	2' RT		15.6
98+20	2' LT		15.6
98+45	2' LT	20.8	
100+75	2' RT	20.8	
101+00	2' RT		15.6
108+55	2' RT	20.8	
109+80	2' RT		15.6
114+35	2' LT		15.6
114+60	2' LT	20.8	
127+30	2' RT	20.8	
127+55	2' RT		15.6
130+65	2' LT		15.6
130+90	2' LT	20.8	
TOTAL		685.6	1164.8

FILE NAME =	USER NAME = .USER.	DESIGNED - J.W.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
no:\dgn\bb\bb04boughtandresurfacing\sheeta\B084ScheduleofQuantities.dgn	DRAWN - G.R.	REVISED -	3586			11-00056-00-RS	WILL	35	5	
PLDT SCALE = 1.00 Ft / IN.	CHECKED - E.J. / D.R.	REVISED -	CONTRACT NO. 63621							
PLDT DATE = 7/20/2011	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
				SCALE: NONE	SHEET NO. 5 OF 35 SHEETS	STA.	TO STA.			

THERMOPLASTIC PAVEMENT MARKING - LINE 4"	
STATION	QUANTITY (FOOT)
43 + 65 to 52 + 00	380
52 + 00 to 61 + 00	420
61 + 00 to 69 + 50	370
69 + 50 to 78 + 50	440
78 + 50 to 87 + 50	420
87 + 50 to 96 + 60	420
96 + 60 to 105 + 50	440
105 + 50 to 114 + 50	400
114 + 50 to 123 + 50	480
123 + 50 to 132 + 50	420
132 + 50 to 141 + 50	480
141 + 50 to 143 + 50	100
TOTAL	4,730

THERMOPLASTIC PAVEMENT MARKING - LINE 12"		
STATION	OFFSET	WHITE LENGTH (FT)
77 + 10 to 78 + 05	LT	300
77 + 10 to 78 + 05	RT	280
TOTAL		580

DETECTOR LOOP REPLACEMENT		
STATION (APPROX.)	OFFSET	LENGTH (FOOT)
44+00	RT	71
46+50	RT	96
47+70	LT	107
50+00	LT	63
51+75	RT	70
54+00	RT	100
55+20	LT	100
57+20	LT	72
62+00	RT	71
64+50	RT	249
66+50	LT	228
69+00	LT	72
71+50	RT	71
73+50	RT	102
74+50	LT	82
77+00	LT	82
78+30	RT	71
80+00	RT	115
81+00	LT	110
83+50	LT	82
93+70	RT	69
95+00	RT	109
96+50	LT	105
99+00	LT	98
108+70	RT	72
111+00	RT	116
112+50	LT	109
115+00	LT	72
126+00	RT	71
128+40	RT	109
129+70	LT	107
132+20	LT	79
TOTAL		3142

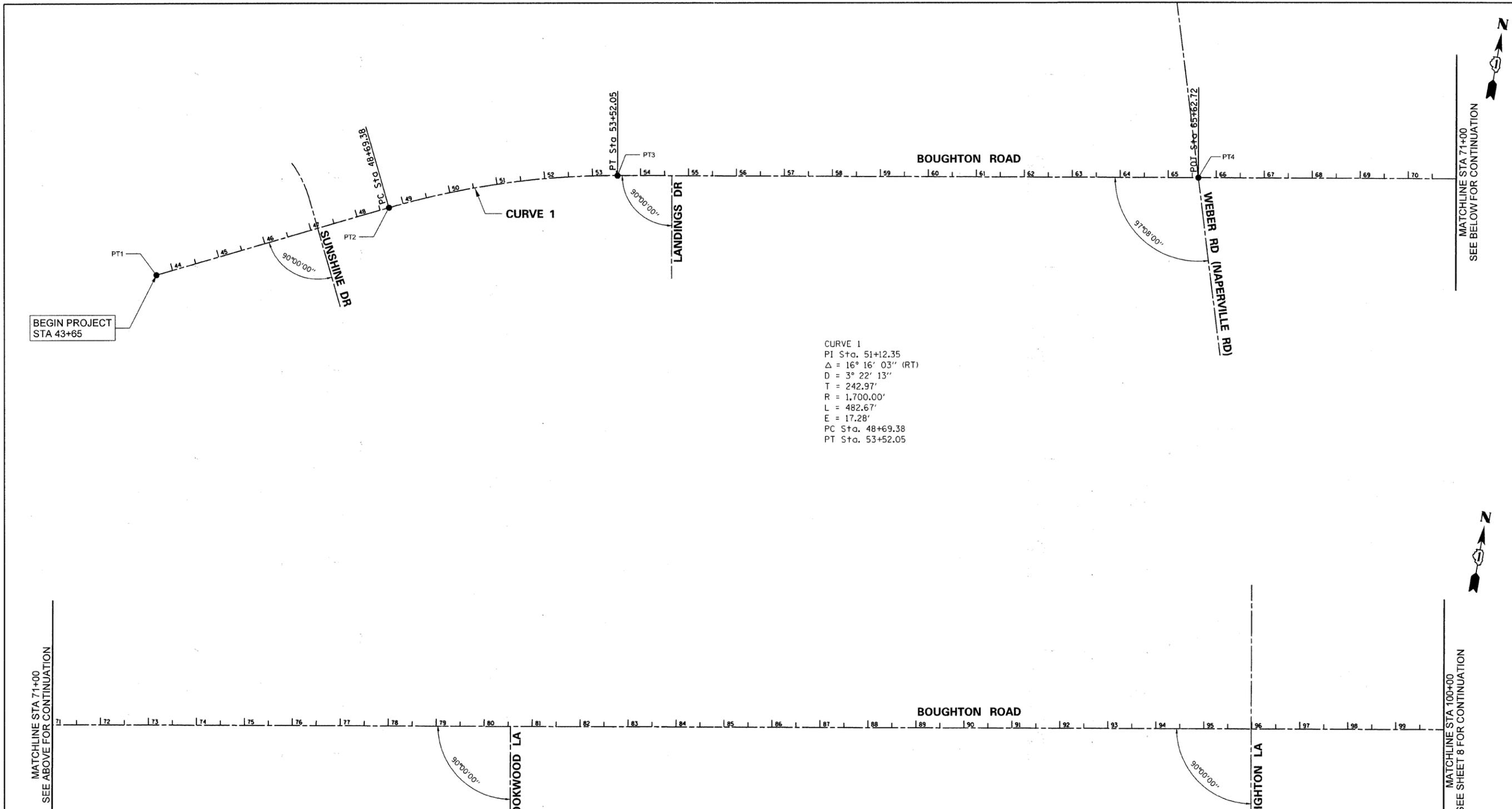
TEMPORARY ACCESS (COMMERCIAL ENTRANCE)			
STATION (APPROX.)	OFFSET	LOCATION	QUANTITY (EACH)
60+00	RT	COMMERCIAL	1
63+00	RT	GAS STATION	1
64+00	RT	GAS STATION	1
69+50	RT	COMMERCIAL	1
74+00	RT	COMMERCIAL	1
77+50	RT	FIRE STATION	1
TOTAL			6

THERMOPLASTIC PAVEMENT MARKING - LINE 6"	
STATION	QUANTITY (FOOT)
43 + 65 to 52 + 00	1,108
52 + 00 to 61 + 00	1,115
61 + 00 to 69 + 50	2,013
69 + 50 to 78 + 50	1,425
78 + 50 to 87 + 50	1,505
87 + 50 to 96 + 60	934
96 + 60 to 105 + 50	544
105 + 50 to 114 + 50	966
114 + 50 to 123 + 50	95
123 + 50 to 132 + 50	1,410
132 + 50 to 141 + 50	0
141 + 50 to 143 + 50	0
TOTAL	11,115

THERMOPLASTIC PAVEMENT MARKING - LINE 24"		
STATION	OFFSET	LENGTH (FOOT)
46 + 62	RT	37
47 + 72	LT	37
54 + 14	RT	52
55 + 31	LT	37
64 + 67	RT	50
66 + 60	LT	50
73 + 56	RT	38
74 + 54	LT	50
77 + 08	RT	25
77 + 10	LT	32
78 + 00	RT	25
78 + 02	LT	36
80 + 12	RT	42
80 + 98	LT	56
95 + 44	RT	40
96 + 50	LT	44
111 + 32	RT	40
112 + 44	LT	40
128 + 57	RT	38
129 + 66	LT	38
TOTAL		807

TRANSVERSE DRAINS COMPLETE		
STATION (APPROX.)	OFFSET	QUANTITY (EACH)
49+30	RT	1
55+50	RT	1
60+50	RT	1
60+55	RT	1
64+50	RT	1
68+50	RT	1
72+70	RT	1
76+25	RT	1
79+25	RT	1
82+10	RT	1
83+40	RT	1
84+60	RT	1
85+80	RT	1
89+50	RT	1
89+55	RT	1
95+35	RT	1
95+40	RT	1
99+50	RT	1
100+50	RT	1
102+50	RT	1
106+90	RT	1
106+95	RT	1
108+40	RT	1
109+90	RT	1
111+45	RT	1
113+65	RT	1
117+85	RT	1
117+90	RT	1
119+50	RT	1
121+10	RT	1
126+30	RT	1
128+00	RT	1
128+05	RT	1
131+25	RT	1
133+50	RT	1
136+00	RT	1
138+50	RT	1
141+00	RT	1
TOTAL		38

TEMPORARY ACCESS (ROAD)			
STATION (APPROX.)	OFFSET	LOCATION	QUANTITY (EACH)
47+15	RT	SUNSHINE DR	1
54+65	RT	LANDINGS DR	1
55+50	RT	NEAR LANDINGS DR	1
68+00	RT	EAST OF WEBER ROAD	1
80+50	RT	BROOKWOOD LN	1
96+00	RT	BRIGHTON LN	1
112+00	RT	LINDSEY LN	1
129+10	RT	ORCHARD DR	1
TOTAL			8



CURVE 1
 PI Sta. 51+12.35
 $\Delta = 16^\circ 16' 03''$ (RT)
 $D = 3^\circ 22' 13''$
 $T = 242.97'$
 $R = 1,700.00'$
 $L = 482.67'$
 $E = 17.28'$
 PC Sta. 48+69.38
 PT Sta. 53+52.05

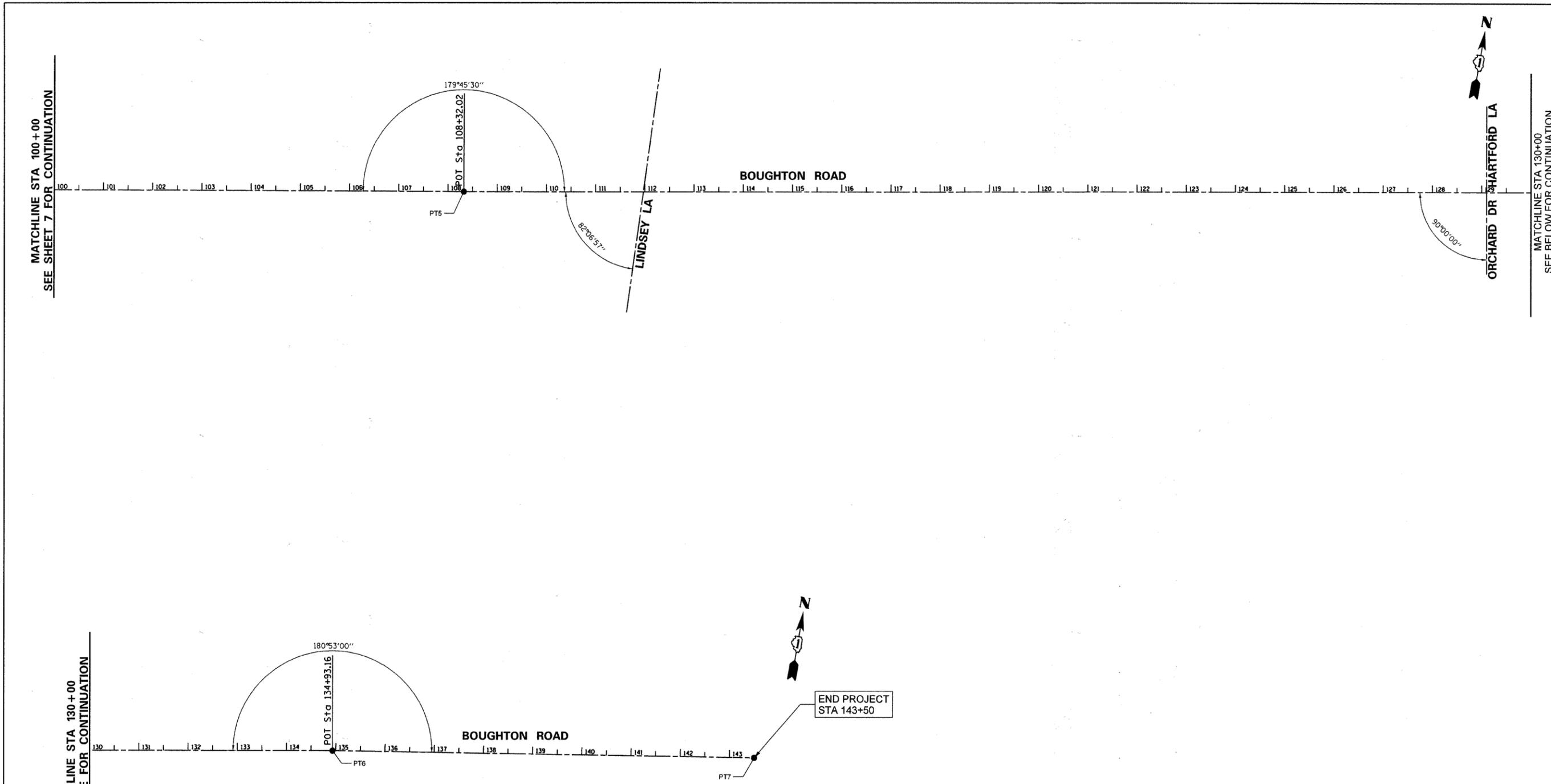
VERTICAL CONTROL (NGS MONUMENTS)
 AJ3064 LOCATED IN FRONT OF BOLINGBROOK FIRE STATION #4, (SOUTH SIDE OF BOUGHTON ROAD, EAST OF WEBER ROAD) 20 FEET WEST NORTHWEST OF FLAG POLE, 13 FEET NORTH OF BACK OF CURB OF FIRE STATION PARKING LOT.
 ELEVATION = (NAVD) 204.80 METERS = 671.94 FEET
 AA3748 LOCATED NORTH OF THE INTERSECTION OF BOUGHTON ROAD AND WEBER ROAD, ON THE WEST SIDE OF WEBER ROAD. STATION IS IN GRASSY AREA BETWEEN CURB AND SIDEWALK 5 FEET NORTHWEST OF CURB RETURN OF NORTHERLY ENTRANCE TO LUMBER YARD/HARDWARE STORE. STATION IS 29.12 FEET SOUTH OF NAIL WITH PLASTIC TAG IN POWER POLE, 5.63 FEET NORTHWEST OF CHISEL "4" ON BACK OF CURB, AND 69.05 FEET EAST OF CHISEL "4" ON LIGHT BASE.
 ELEVATION = (NGVD) 198 METERS = 649.60 FEET

COORDINATE TABLE

PT #	NORTHING	EASTING
1	1833830.11	1038926.40
2	1834050.18	1039380.24
3	1834198.71	1039838.43
4	1834389.84	1041032.14
5	1835110.70	1045241.64
6	1833948.67	1045173.83
7	1834082.11	1046020.22

NOTE:
 BASELINE LOCATION AND NORTHING EASTINGS ARE BASED ON AERIAL SPATIAL REFERENCE (NAD83 ILLINOIS EAST FT US), TO CONVERT TO STATE PLANE COORDINATES: ADD 4.35 FEET NORTHING AND 2.13 FEET EASTING

FILE NAME =	USER NAME = .USER.	DESIGNED - J.W	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BOUGHTON ROAD ALIGNMENT PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.00 FT / IN.	CHECKED - E.J / D.R.	REVISED -	CONTRACT NO. 63621							
PLOT DATE = 7/28/2011	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
SCALE: 1"=100' SHEET NO. 7 OF 35 SHEETS STA. 43+65 TO STA. 100+00										



VERTICAL CONTROL (NGS MONUMENTS)

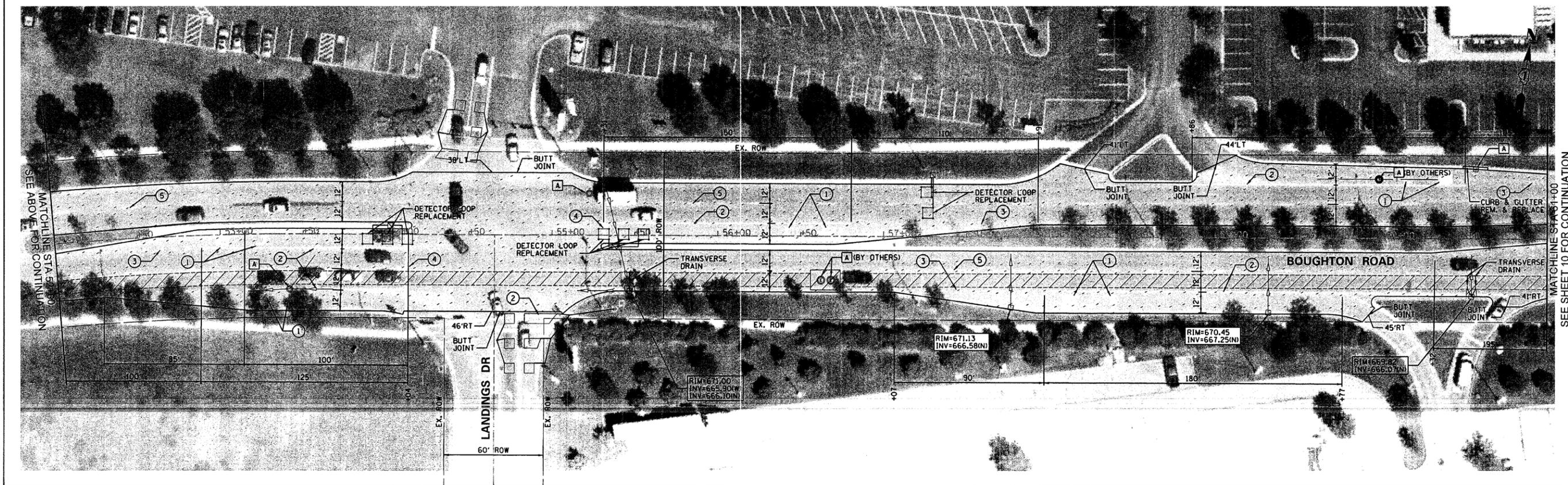
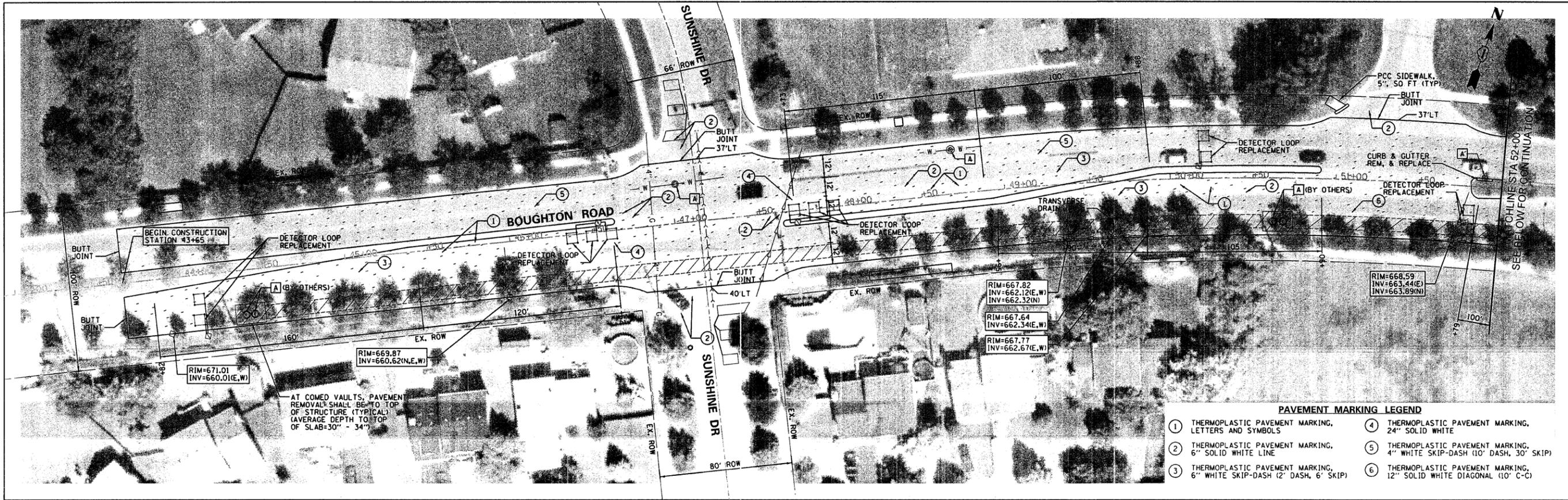
- AJ3064 LOCATED IN FRONT OF BOLINGBROOK FIRE STATION #4, (SOUTH SIDE OF BOUGHTON ROAD, EAST OF WEBER ROAD) 20 FEET WEST NORTHWEST OF FLAG POLE, 13 FEET NORTH OF BACK OF CURB OF FIRE STATION PARKING LOT.
ELEVATION = (NAVD) 204.80 METERS = 671.94 FEET
- AA3748 LOCATED NORTH OF THE INTERSECTION OF BOUGHTON ROAD AND WEBER ROAD, ON THE WEST SIDE OF WEBER ROAD. STATION IS IN GRASSY AREA BETWEEN CURB AND SIDEWALK 5 FEET NORTHWEST OF CURB RETURN OF NORTHERLY ENTRANCE TO LUMBER YARD/HARDWARE STORE. STATION IS 23.12 FEET SOUTH OF NAIL WITH PLASTIC TAG IN POWER POLE, 5.63 FEET NORTHWEST OF CHISEL "+" ON BACK OF CURB, AND 69.05 FEET EAST OF CHISEL "+" ON LIGHT BASE.
ELEVATION = (NGVD) 198 METERS = 649.60 FEET

COORDINATE TABLE

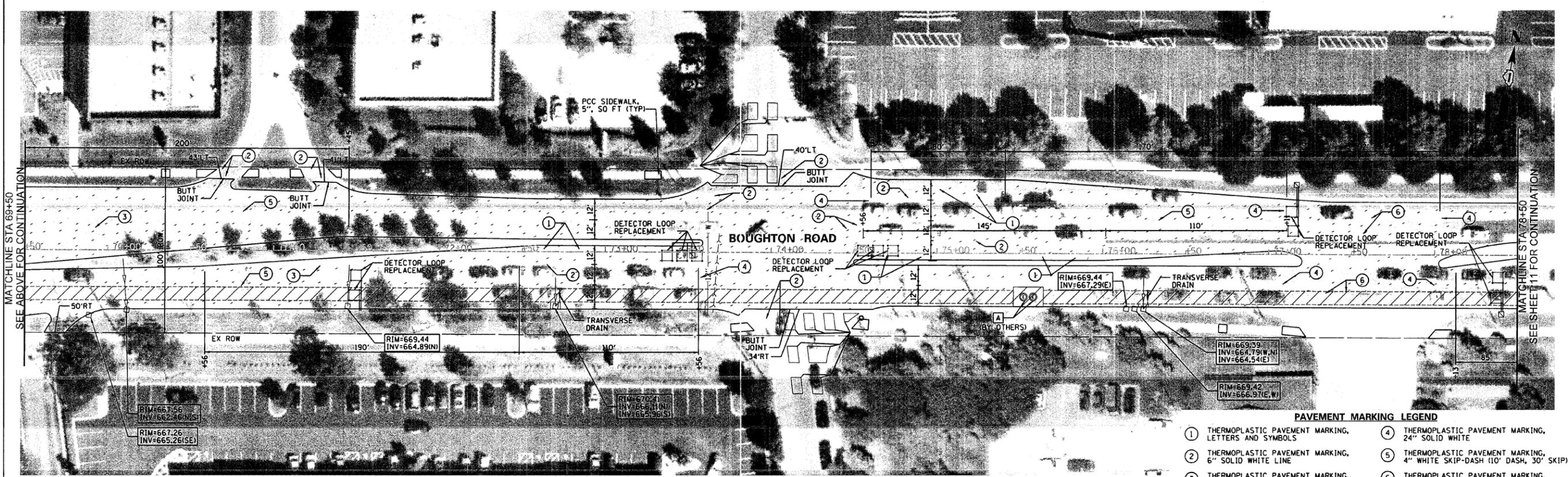
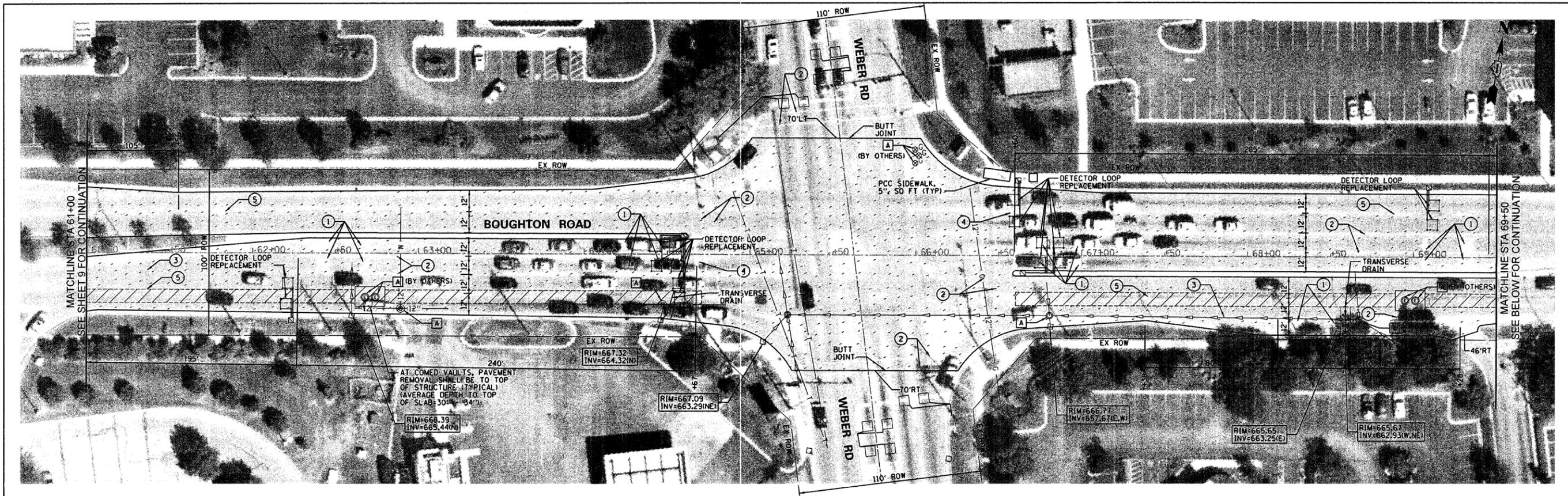
PT#	NORTHING	EASTING
1	1833830.11	1038826.40
2	1834060.18	1038830.24
3	1834196.71	1038838.43
4	1834398.84	1041032.14
5	1835110.70	1045241.84
6	1833948.67	1045173.83
7	1834082.11	1046020.22

NOTE:
 BASELINE LOCATION AND NORTHING EASTINGS ARE BASED ON AERIAL SPATIAL REFERENCE (NAD83 ILLINOIS EAST FT US). TO CONVERT TO STATE PLANE COORDINATES: ADD 4.35 FEET NORTHING AND 2.13 FEET EASTING

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PLOT SCALE = 100.00 ft / IN.	DRAWN - G.R.	CHECKED - E.J / D.R.	REVISED -			SCALE: 1"=100' SHEET NO. 8 OF 35 SHEETS STA. 100+00 TO STA. 143+50					
PLOT DATE = 7/20/2011	DATE -	REVISED -	REVISED -			ILLINOIS FED. AID PROJECT					
						CONTRACT NO. 63621					



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PLOT SCALE = 30.00 ft / IN.	CHECKED - E.J / D.R.	DATE -	REVISED -		SCALE: 1"=30'	SHEET NO. 9 OF 35 SHEETS	STA. 43+65 TO STA. 61+00	CONTRACT NO. 63621		ILLINOIS FED. AID PROJECT		
PLOT DATE = 7/28/2011	DATE -	REVISED -	REVISED -									



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 PLOT DATE = 7/20/2011

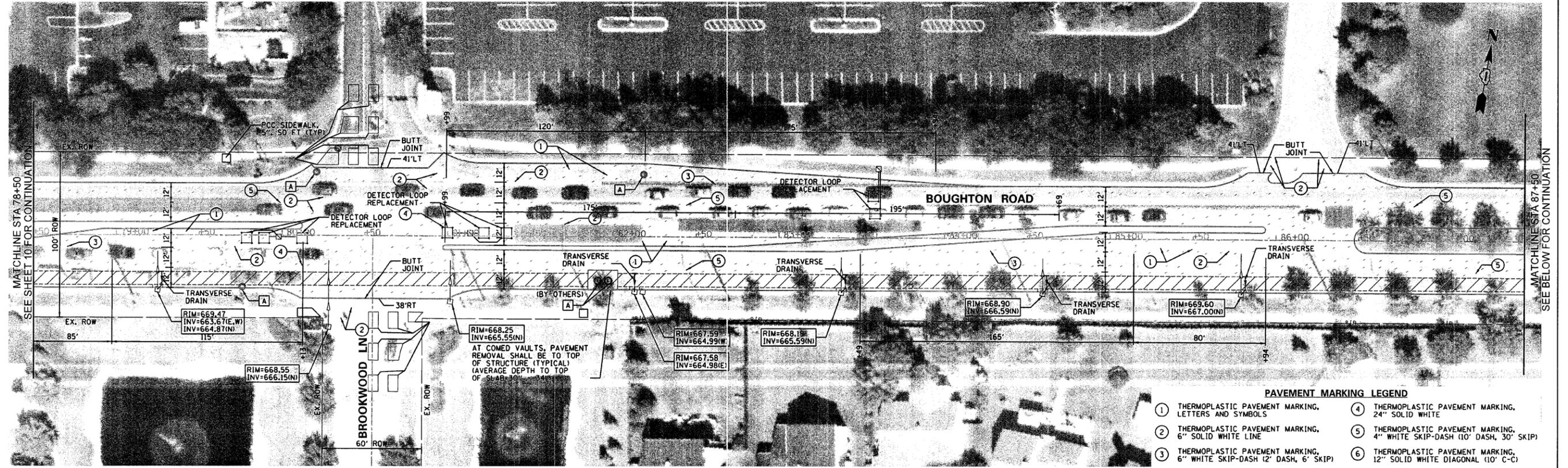
USER NAME = _USER_	DESIGNED - J.W	REVISED -
DRAWN - G.R	REVISED -	
CHECKED - E.J / D.R.	REVISED -	
DATE -	REVISED -	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

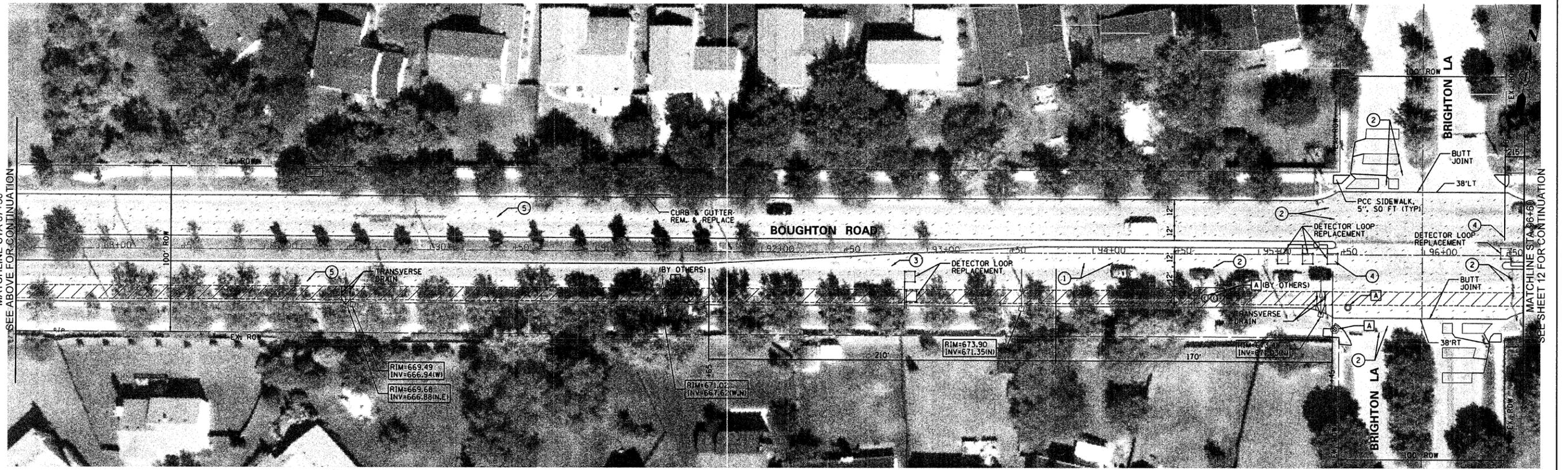
**BOUGHTON ROAD
 RESURFACING PLAN**

SCALE: 1"=30' SHEET NO. 10 OF 35 SHEETS STA. 61+00 TO STA. 78+50

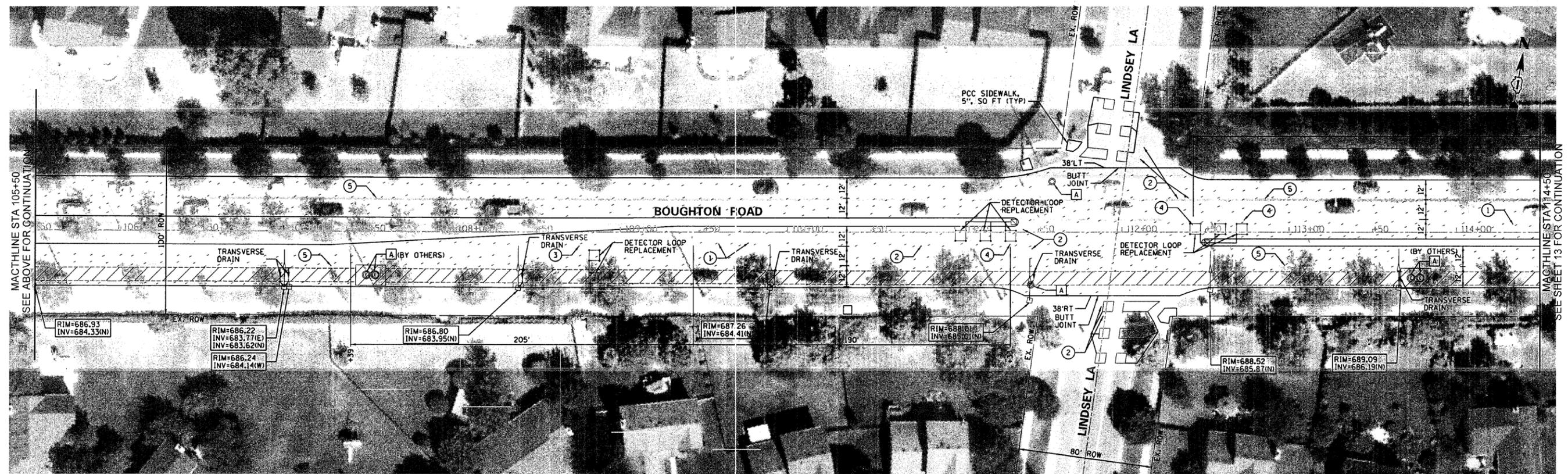
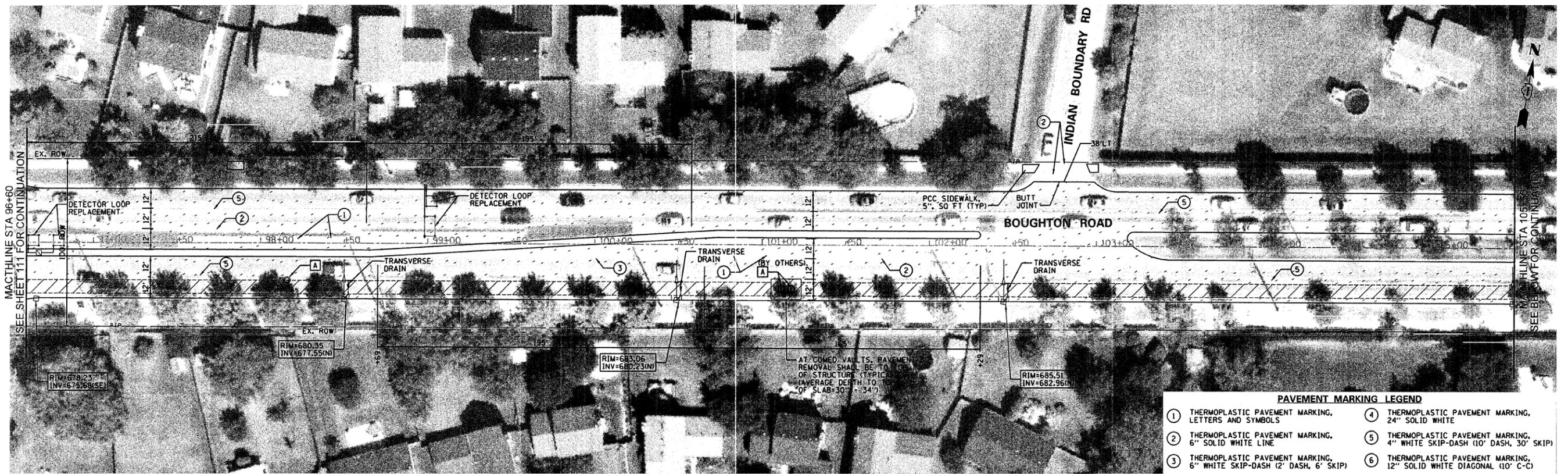
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3586	11-00056-00-R5	WILL	35	10
CONTRACT NO. 63621			ILLINOIS FED. AID PROJECT	



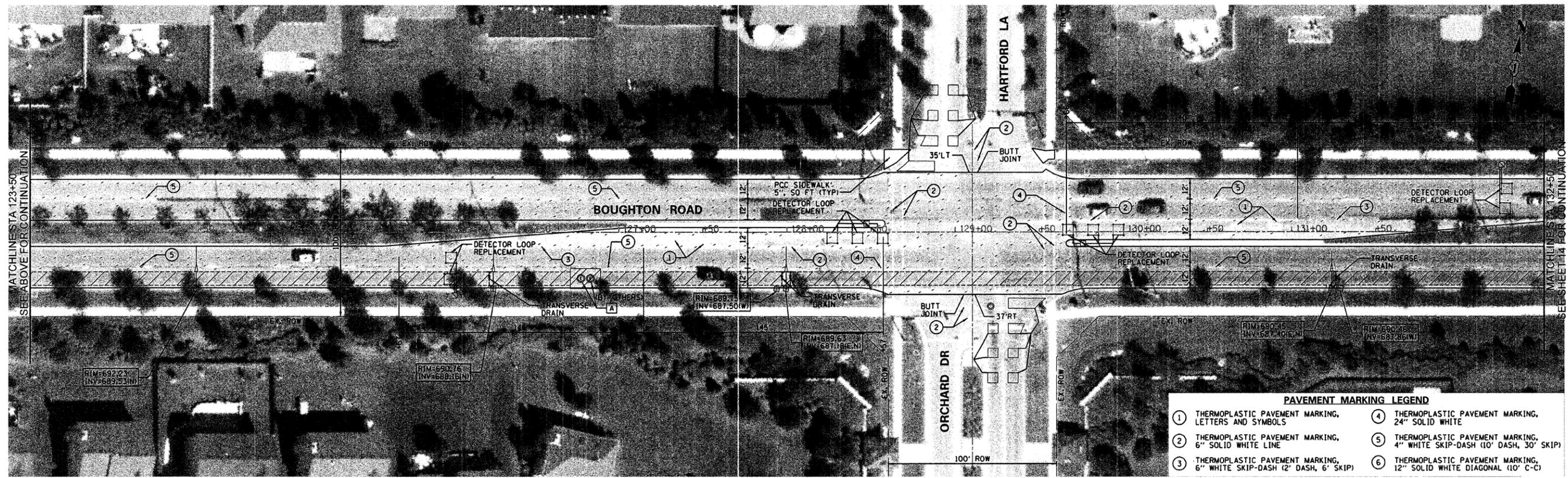
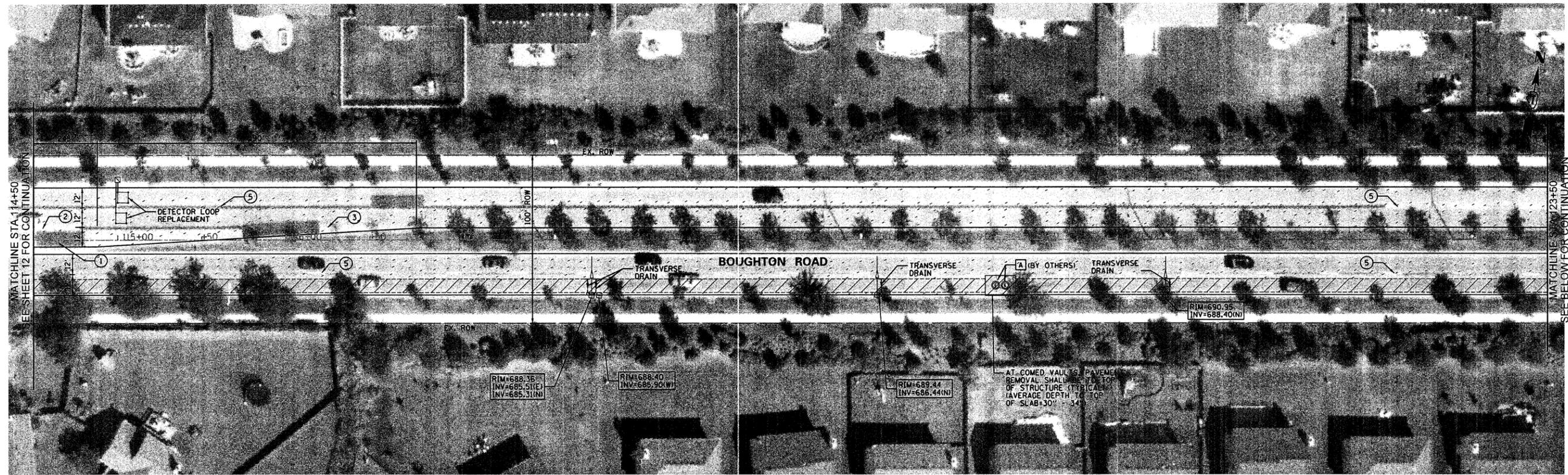
- PAVEMENT MARKING LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
 - ② THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE LINE
 - ③ THERMOPLASTIC PAVEMENT MARKING, 6" WHITE SKIP-DASH (2" DASH, 6" SKIP)
 - ④ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE
 - ⑤ THERMOPLASTIC PAVEMENT MARKING, 4" WHITE SKIP-DASH (10' DASH, 30" SKIP)
 - ⑥ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE DIAGONAL (10' C-C)



FILE NAME = n:\dgn\bb\bb04boughton\resurfacing\sheet\B004Plan03.dgn	USER NAME = USER	DESIGNED - J.W	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BOUGHTON ROAD RESURFACING PLAN		F.A.U. RTE. 3586	SECTION 11-00056-00-RS	COUNTY WILL	TOTAL SHEETS 35	SHEET NO. 11
PLOT SCALE = 3/8" = 1' / IN.	PLOT DATE = 7/28/2011	DRAWN - G.R	REVISED -		SCALE: 1"=30'	SHEET NO. 11 OF 35 SHEETS	STA. 78+50 TO STA. 96+50	CONTRACT NO. 63621		(ILLINOIS) FED. AID PROJECT	
		CHECKED - E.J / D.R.	REVISED -								
		DATE -	REVISED -								



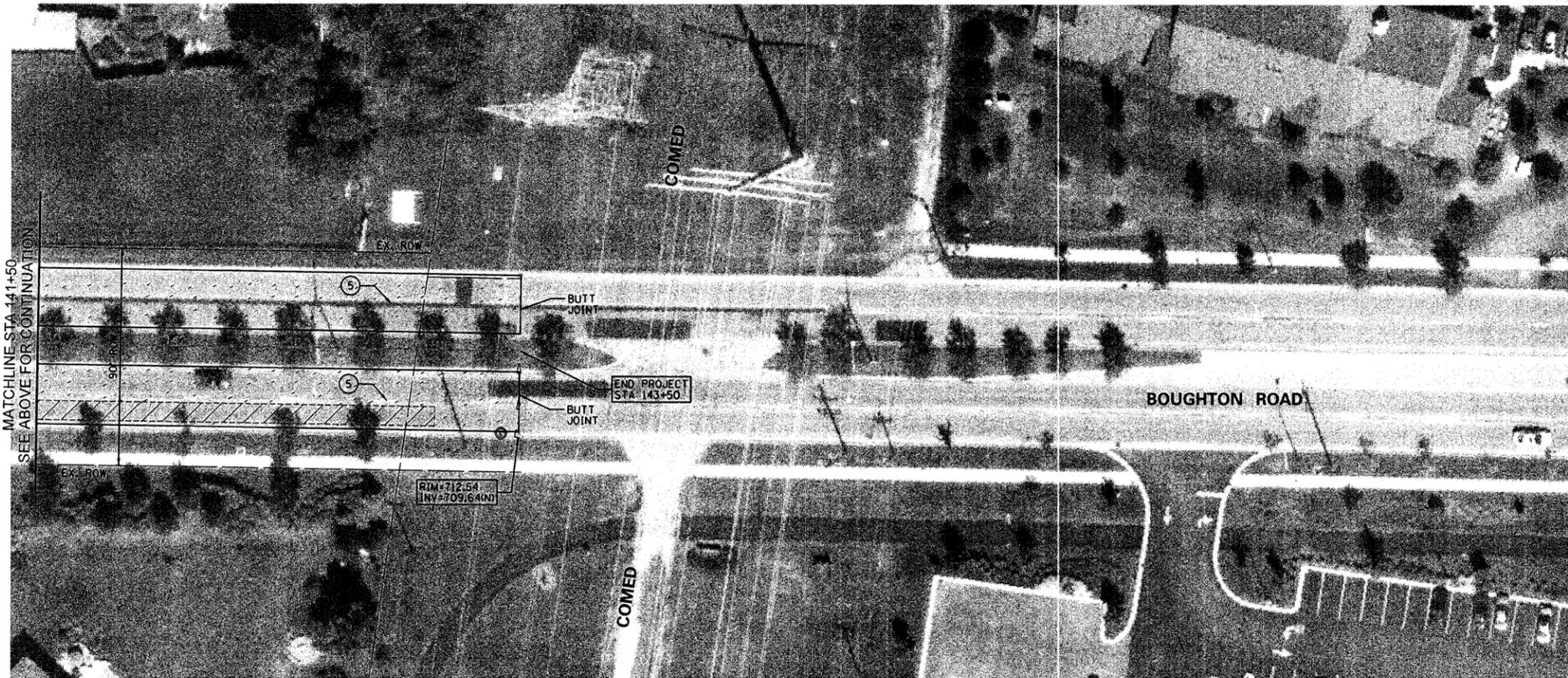
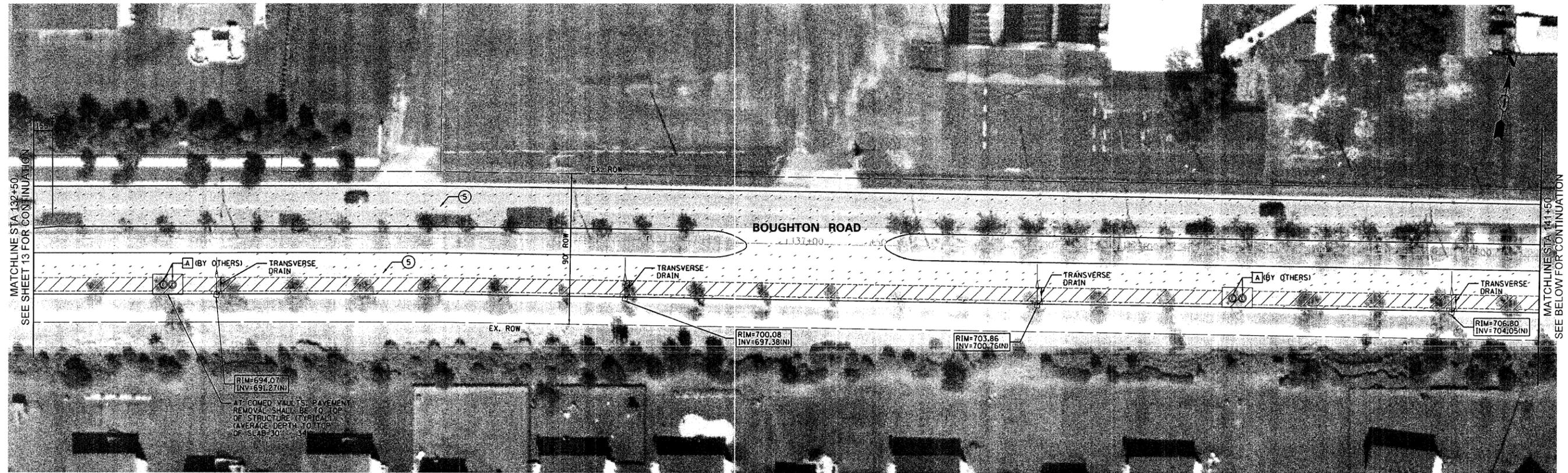
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ns\dgn\bb\bb04boughton\resurfacing\sheet\BB04Plan04.dgn		DRAWN - G.R	REVISED -			3586	11-00056-00-RS	WILL	35	12
PLOT SCALE = 30.00 ft / IN.		CHECKED - E.J / D.R.	REVISED -			CONTRACT NO. 63621				
PLOT DATE = 7/20/2011		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				
SCALE: 1"=30'						SHEET NO. 12 OF 35 SHEETS		STA. 96+60 TO STA. 114+50		



PAVEMENT MARKING LEGEND

① THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS	④ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE
② THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE LINE	⑤ THERMOPLASTIC PAVEMENT MARKING, 4" WHITE SKIP-DASH (10' DASH, 30' SKIP)
③ THERMOPLASTIC PAVEMENT MARKING, 6" WHITE SKIP-DASH (2' DASH, 6' SKIP)	⑥ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE DIAGONAL (10" C-C)

FILE NAME =	USER NAME = _USER_	DESIGNED - J.W	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BOUGHTON ROAD RESURFACING PLAN	F.A.U. RTE. 3586	SECTION 11-00056-00-RS	COUNTY WILL	TOTAL SHEETS 35	SHEET NO. 13		
PLOT SCALE = 30.00 ft / IN.						SCALE: 1"=30'	SHEET NO. 13 OF 35 SHEETS	STA. 114+50	TO STA. 132+50	CONTRACT NO. 63621		
PLOT DATE = 7/20/2011						DATE -	ILLINOIS FED. AID PROJECT					



- PAVEMENT MARKING LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
 - ② THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE LINE
 - ③ THERMOPLASTIC PAVEMENT MARKING, 6" WHITE SKIP-DASH (2' DASH, 6' SKIP)
 - ④ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE
 - ⑤ THERMOPLASTIC PAVEMENT MARKING, 4" WHITE SKIP-DASH (10' DASH, 30' SKIP)
 - ⑥ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE DIAGONAL (10' C-C)

FILE NAME =	USER NAME = .USER.	DESIGNED - J.W	REVISED -
nt\dgn\bb\bb04boughton\dr\surfacing\sh	ta\B804P1en06.dgn	DRAWN - G.R	REVISED -
		CHECKED - E.J / D.R.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BOUGHTON ROAD RESURFACING PLAN	
SCALE: 1"=30'	SHEET NO. 14 OF 35 SHEETS
STA. 132+50	TO STA. 143+50

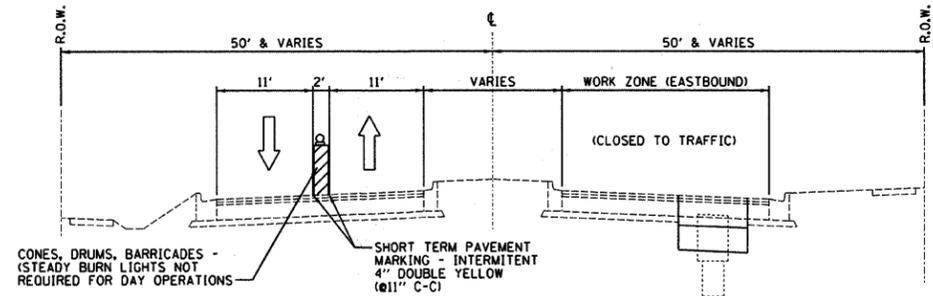
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3586	11-00056-00-RS	WILL	35	14
CONTRACT NO. 63621				
ILLINOIS FED. AID PROJECT				

MAINTENANCE OF TRAFFIC LEGEND

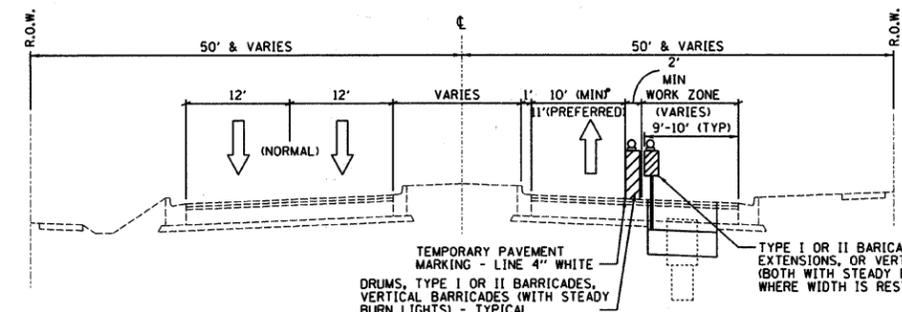
- 1 ROAD CONSTRUCTION AHEAD
- 2* RIGHT LANE CLOSED AHEAD
- 3 WORKERS
- 4 WORKERS
- 5
- 6
- 7 35 MPH
- 8
- 9 LEFT LANE CLOSED AHEAD
- 10 TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
- 11 ○ CONE, DRUM, OR BARRICADE/VERTICLE PANEL **
- 11A FLEXIBLE DELINEATOR
- 12 FLAGGER
- 13 KEEP RIGHT
- 14 KEEP LEFT
- 15 NO LEFT TURN
- 16
- 17 TEMPORARY PAVEMENT MARKING - LINE 6" (WHITE)
- 18 TEMPORARY PAVEMENT MARKING - LINE 24" (WHITE)
- 19 TEMPORARY PAVEMENT MARKING - LETTERS & SYMBOLS
- 20
- 21 THRU-TRAFFIC KEEP RIGHT
- 22
- 23 STOP HERE ON RED
- 24 DRIVEWAY CLOSED
- 25 NO TURN ON RED
- 26 35 MPH SPEED ZONE AHEAD
- 27 LANE SHIFT AHEAD BOUGHTON RD
- 28 LANE SHIFT AHEAD REDUCED SPEED
- TT TYPE III BARRICADE
- ARROW BOARD
- DIRECTION OF TRAFFIC
- ▨ STAGE CONSTRUCTION LIMITS
- ▨▨▨▨ COM ED CONDUIT/TRENCH - PAVEMENT RECONSTRUCTION
- * LEFT LANE CLOSED AHEAD (FOR WESTBOUND CLOSURES), OR 9
- ** CONES - FOR DAYTIME CLOSURE USE ONLY; ALL OTHERS, STEADY-BURN LIGHT FOR NIGHT (OR DAY) CLOSURE (INCLUDED IN COST OF TRAFFIC CONTROL AND PROTECTION, SPECIAL)

MAINTENANCE OF TRAFFIC NOTES:

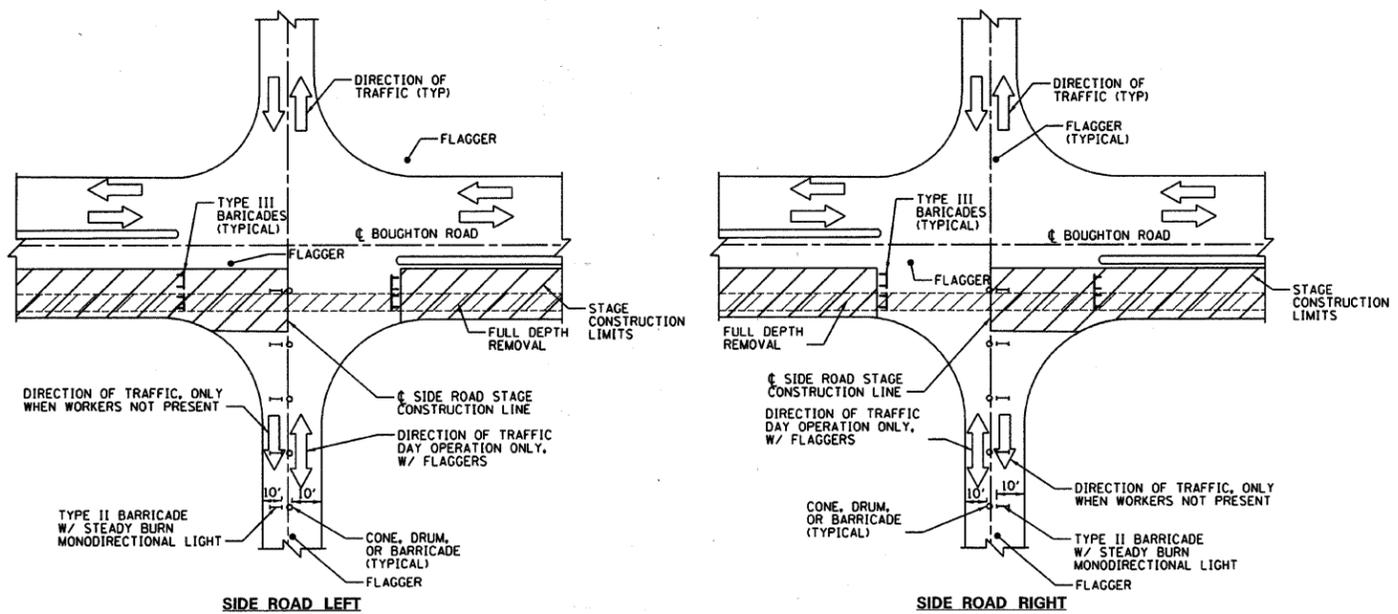
1. DAY CLOSURES (EAST BOUND SHIFT) FOR PAVEMENT RE-CONSTRUCTION BETWEEN THE HOURS OF (7-5 M-F), ONLY, OR BEFORE DUSK, WHICHEVER IS EARLIER.
2. INTERSECTION TRAFFIC SIGNALS SHALL BE PUT ON RED FLASH DURING - DAY CLOSURE (7-5, M-F) CONSTRUCTION HOURS. VILLAGE PERSONNEL WILL OPERATE SIGNALS.
3. ALL DROP-OFFS AT EDGE OF TRAVEL LANE SHALL BE RESTORED NIGHTLY (BY END OF DAY OPERATIONS CLOSURE) TO A DEPTH LESS THAN OR EQUAL TO 24 INCHES.
4. ALL DROP-OFFS AT EDGE OF TRAVEL LANE GREATER THAN 18 INCHES SHALL NOT EXCEED DURATION OF 48 HOURS.
5. CONTRACTOR SHALL MAINTAIN ACCESS TO ENTRANCES AT ALL TIMES AND PROVIDE TEMPORARY ENTRANCES WHERE REQUIRED DURING CONSTRUCTION. WHERE A SITE HAS MORE THAN ONE DRIVE ENTRANCE ALONG BOUGHTON ROAD ONE ENTRANCE MAY BE CLOSED FOR NO MORE THAN FIVE (5) CALENDAR DAYS. DURING DAY CLOSURES, SOME DRIVEWAYS WILL BE ALLOWED TO CLOSE WITH ENGINEER'S PERMISSION. HOWEVER, ALL DRIVEWAYS MUST REMAIN OPEN AFTER WORK HOURS.
6. ACCESS TO FIRE STATION NO. 4 MUST BE COORDINATED BETWEEN CONTRACTOR AND FIRE DEPARTMENT WITH APPROVAL OF ENGINEER.
7. IF MAINTENANCE OF TRAFFIC/SUGGESTED STAGING PLANS CONFLICT WITH TRAFFIC CONTROL STANDARDS, THE STANDARDS SHALL CONTROL.
8. ALL CONFLICTING SIGNS SHALL BE COVERED.
9. ALL STREET INTERSECTIONS SHALL BE FULLY OPEN TO TWO-WAY TRAFFIC AFTER WORKING HOURS. DURING DAY CLOSURES, THE CONTRACTOR IS ONLY ALLOWED TO CLOSE HALF OF THE INTERSECTION WITH A FLAGGER ON BOTH SIDES, IF NEEDED. IN NO CASE SHALL THE ENTIRE STREET INTERSECTION BE ALLOWED TO BE CLOSED DOWN.
10. CONES WILL ONLY BE ALLOWED FOR DAY CLOSURES.
11. IN AREAS UNDERGOING 'BLOCK-BY-BLOCK' RECONSTRUCTION, ONE EASTBOUND LANE FOR THE TRAFFIC SHALL REMAIN OPEN DURING ALL NON-WORKING HOURS.
12. CROSS WALKS AT THE INTERSECTION UNDER CONSTRUCTION SHALL BE CLOSED DURING DAY AND NIGHT (ALL THE TIMES UNTIL BINDER COURSE IS INSTALLED).
13. PAVEMENT RECONSTRUCTION SHALL BE PERFORMED ON A 'BLOCK-BY-BLOCK' BASIS ONLY AS SHOWN IN THE PLAN, UNLESS APPROVED BY THE ENGINEER. THE ENTIRE DURATION OF 'BLOCK BY BLOCK' RECONSTRUCTION (EXCEPT FINAL RESURFACING, I.E. ENTIRE BINDER/SURFACE COURSE) SHALL BE COMPLETED WITHIN 5 WORKING DAYS (COMPLETION OF BLOCK-BY-BLOCK PAVEMENT RECONSTRUCTION SHALL ALSO INCLUDE HMA SURFACE REMOVAL OVER THE BALANCE OF THE OUTSIDE LANE(S) AND ADJOINING INSIDE LANE(S) SO THAT NO UNEVEN SURFACES EXIST WITH/OR ACROSS THE COMPLETED TRENCH RECONSTRUCTION AND ADJOINING PAVEMENT.) UPON COMPLETION BOTH EASTBOUND LANES SHALL REMAIN OPEN ALL THE TIME.
14. INSTALL TRAFFIC LANE SHIFT AHEAD SIGNS ON ALL DRIVEWAYS AND SIDE STREETS BEFORE DAY CLOSURES BECOMES EFFECTIVE. WHENEVER DAY CLOSURES ARE NOT IN PLACE, SUCH SIGNS SHALL BE TEMPORARILY COVERED.



MAINTENANCE OF TRAFFIC /SUGGESTED STAGING DAY OPERATIONS TYPICAL SECTION
SCALE: NONE



MAINTENANCE OF TRAFFIC /SUGGESTED STAGING NIGHT CLOSURE TYPICAL SECTION
SCALE: NONE

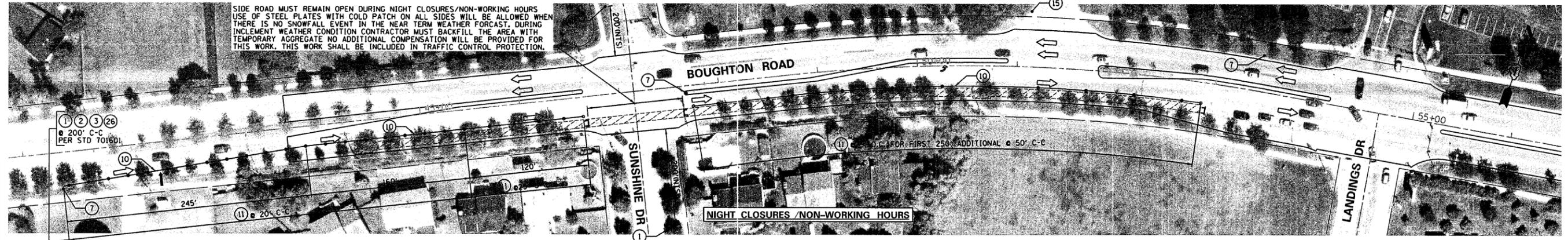
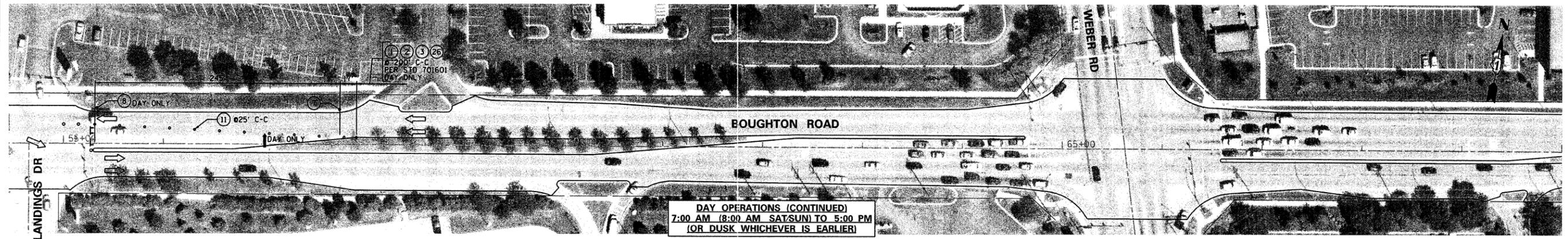
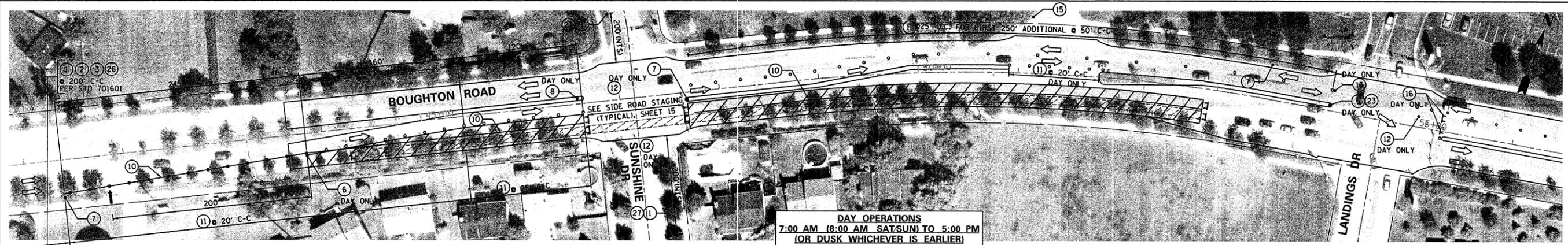


NOTES:

1. SEE ROAD CONSTRUCTION ORDER BETWEEN LEFT AND RIGHT SHALL BE DETERMINED BY THE ENGINEER.
2. THE CONTRACTOR SHALL MAINTAIN 1-LANE, 2-WAY TRAFFIC WITH FLAGGERS DURING STAGE CONSTRUCTION OF SIDE ROADS.
3. SEE STANDARD 701701 AND MAINTENANCE OF TRAFFIC/SUGGESTED STAGING FOR ADVANCE WARNING SIGNING
4. BEFORE SIDE ROAD INTERSECTION CONSTRUCTION CAN BE LEFT UNATTENDED, THE CONTRACTOR MUST FIRST PLACE AGGREGATE SURFACE COURSE FOR TEMPORARY ACCESS, IN PAVEMENT REMOVAL AREAS SO THAT 2 - LANE, 2 - WAY TRAFFIC CAN BE MAINTAINED AT THE END OF DAY OPERATION CLOSURES.

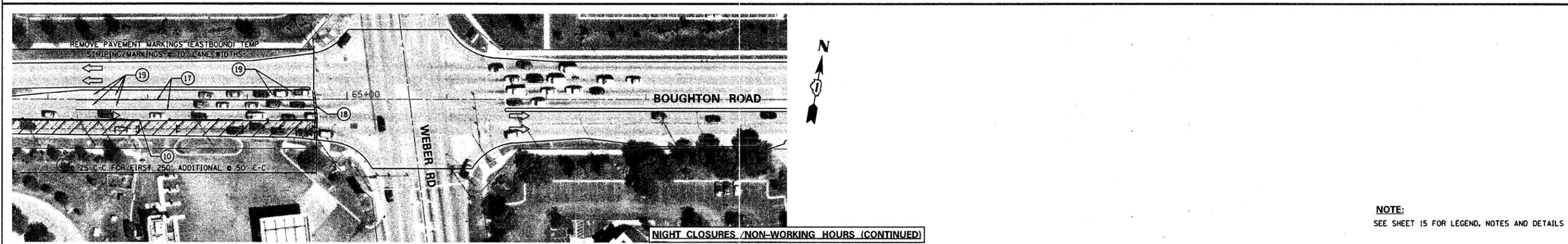
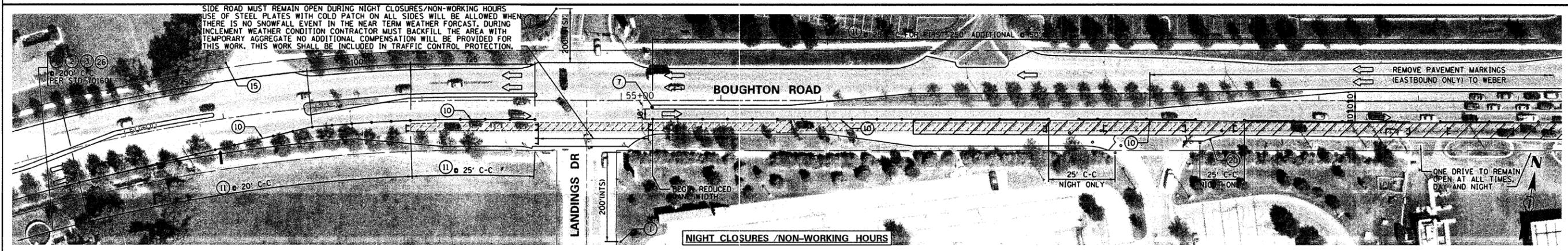
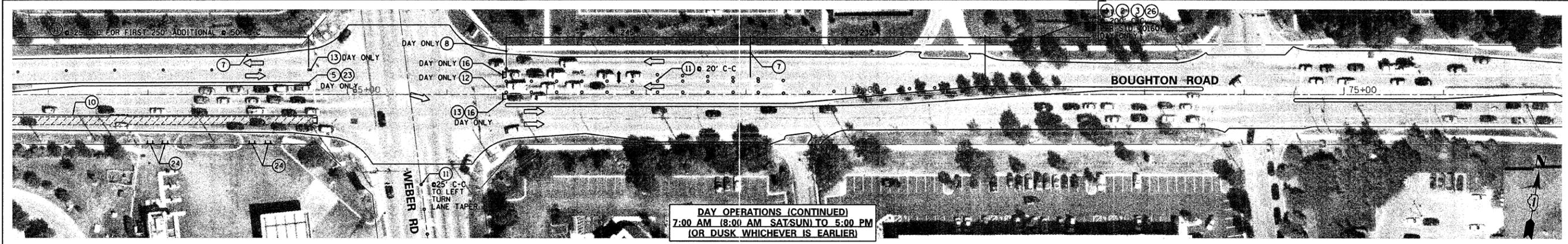
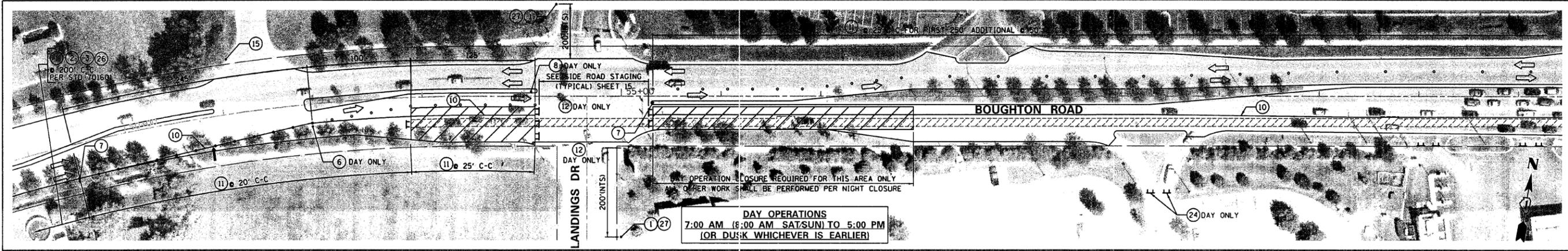
SIDE ROAD STAGING DETAIL - DAY OPERATIONS ONLY

FILE NAME =	USER NAME = .USER.	DESIGNED - J.W	REVISED -	STATE OF ILLINOIS	MAINTENANCE OF TRAFFIC	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 1/80 Ft / IN.				CHECKED - E.J / D.R.	SCALE: NONE	SHEET NO. 15 OF 35 SHEETS		STA. TO STA.		CONTRACT NO. 63621
PLOT DATE = 7/20/2011				DATE -			ILLINOIS FED. AID PROJECT			



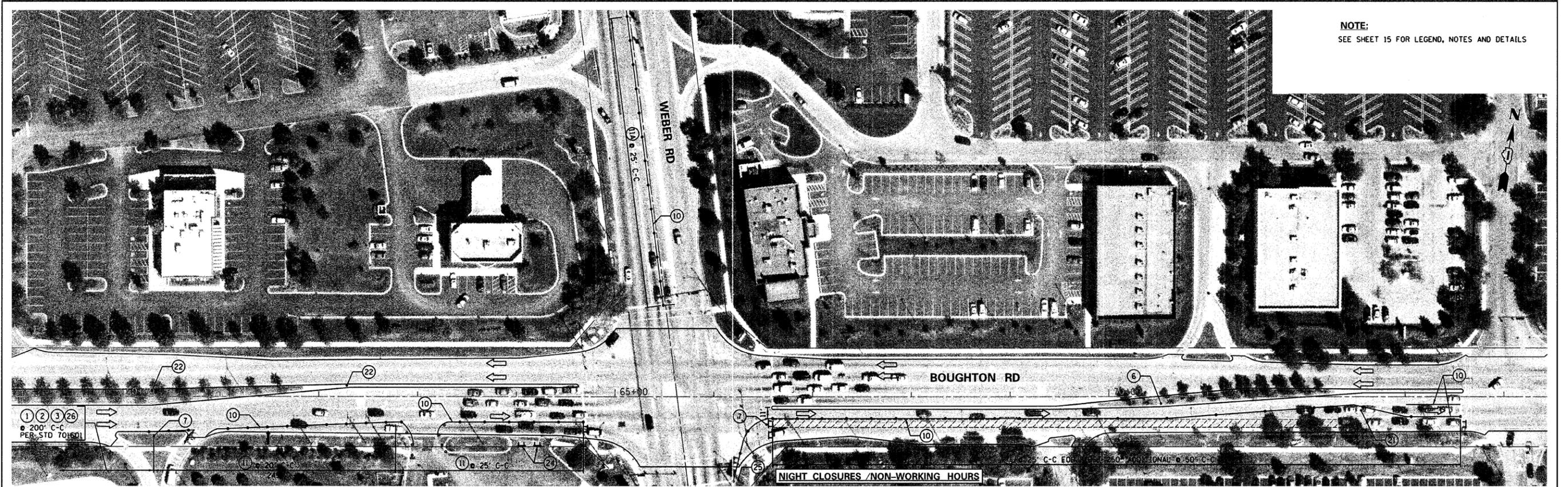
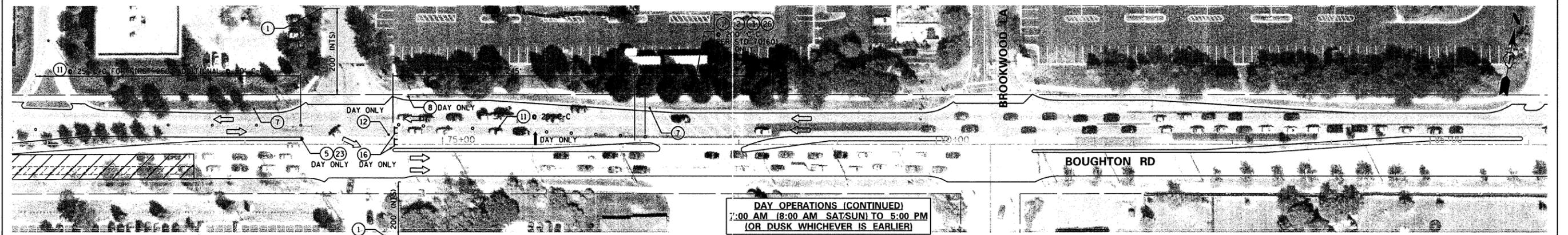
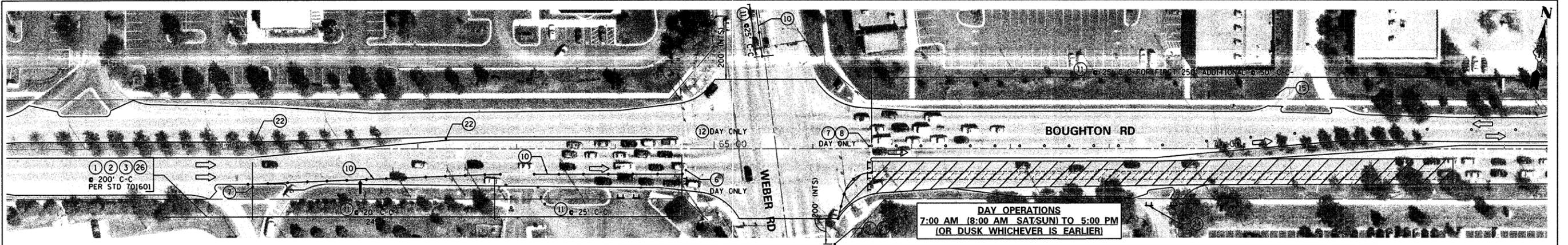
NOTE:
SEE SHEET 15 FOR LEGEND, NOTES AND DETAILS

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PLOT SCALE = 50.00 Ft / IN.	CHECKED - E.J / D.R.	DATE -	REVISED -			SCALE: 1"=50'	SHEET NO. 16 OF 35 SHEETS	STA.	TO STA.	CONTRACT NO. 63621		
PLOT DATE = 7/20/2011	DATE -	REVISED -	REVISED -			ILLINOIS FED. AID PROJECT						



NOTE:
SEE SHEET 15 FOR LEGEND, NOTES AND DETAILS

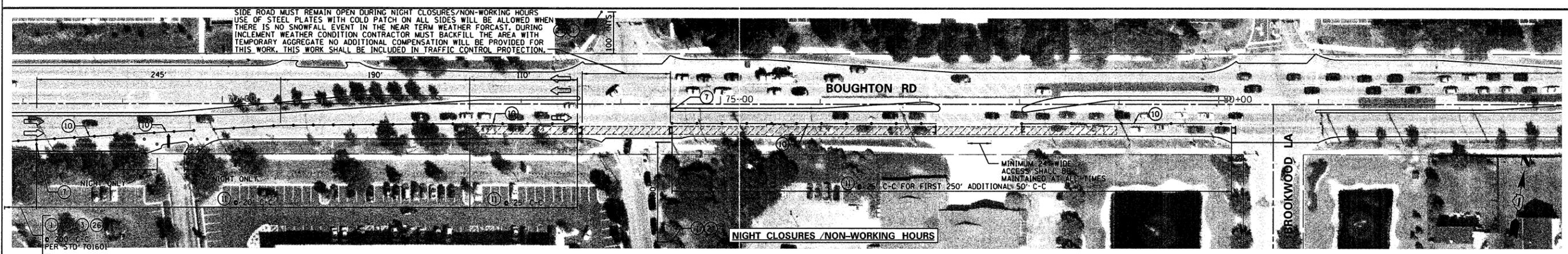
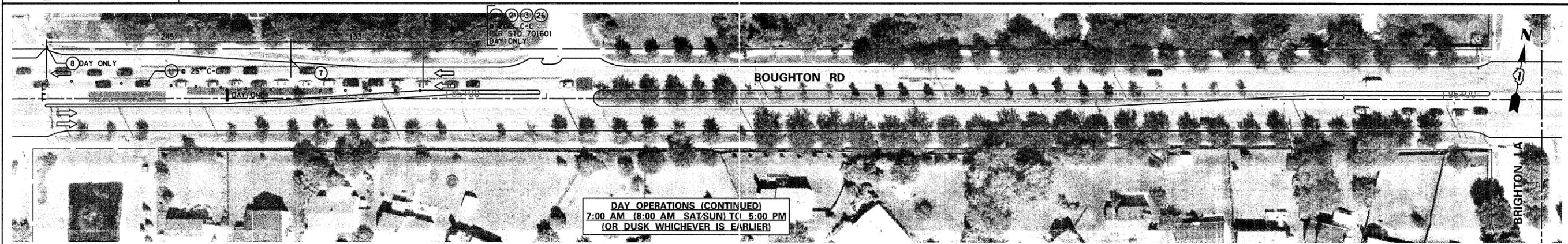
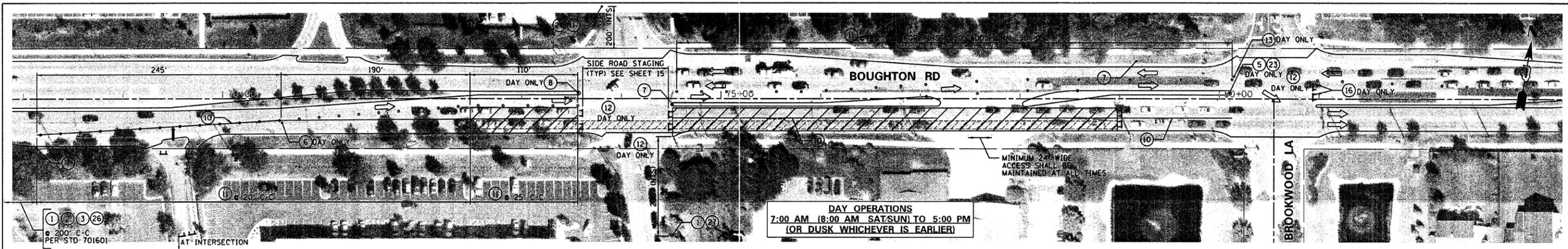
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PLOT SCALE = 50.00 ft / IN.		CHECKED - E.J / D.R.	REVISED -			SCALE: 1"=50'		SHEET NO. 17 OF 35 SHEETS		STA. TO STA.	
PLOT DATE = 7/20/2011		DATE -	REVISED -			CONTRACT NO. 63621					
						ILLINOIS FED. AID PROJECT					



NOTE:
SEE SHEET 15 FOR LEGEND, NOTES AND DETAILS

FILE NAME =	USER NAME = .USER.	DESIGNED - J.W.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BOUGHTON ROAD - WEBER RD TO TARGET WEST ENTRANCE MAINTENANCE OF TRAFFIC /SUGGESTED STAGING PLAN	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT DATE = 7/20/2011	DATE -	CHECKED - E.J / D.R.	REVISED -			CONTRACT NO. 63621					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

SCALE: 1"=50' SHEET NO. 18 OF 35 SHEETS STA. TO STA.

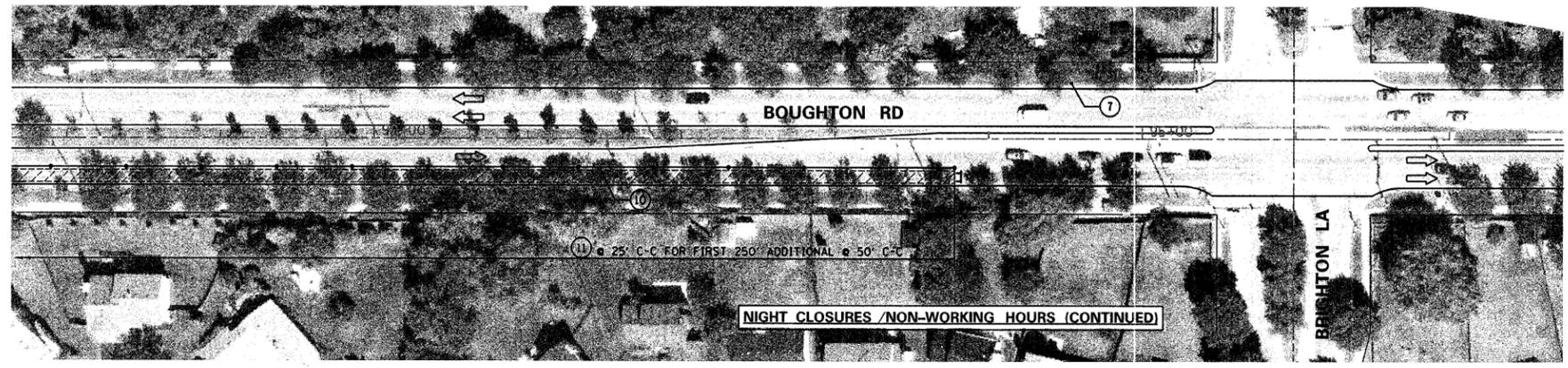
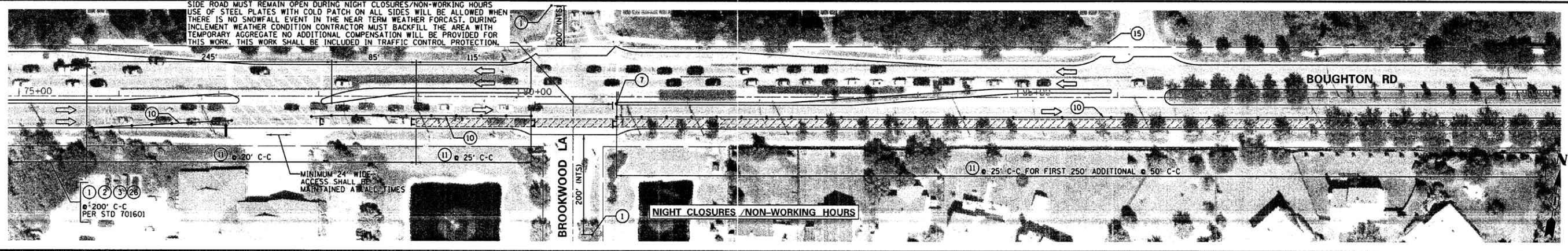
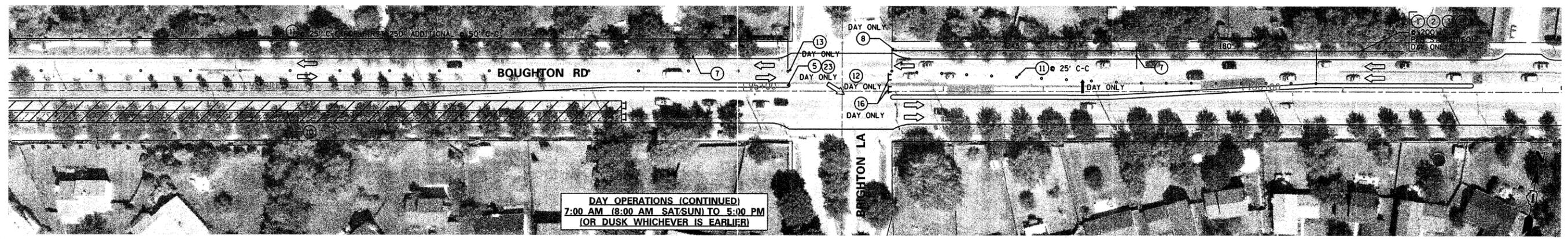
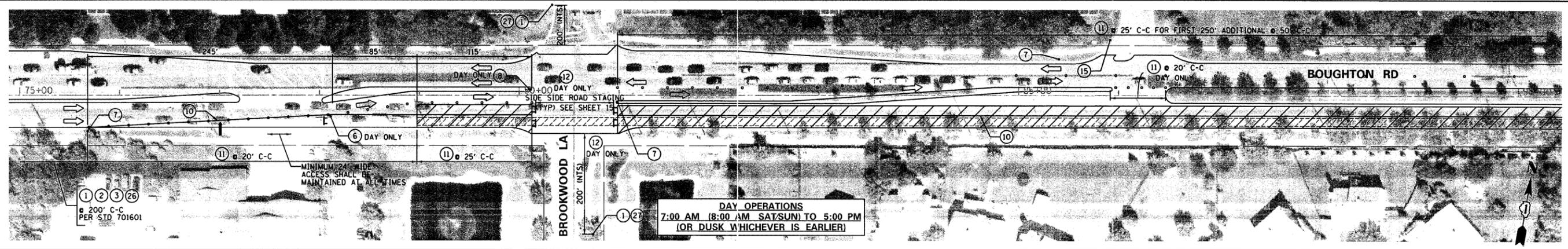


SIDE ROAD MUST REMAIN OPEN DURING NIGHT CLOSURES/NON-WORKING HOURS
 USE OF STEEL PLATES WITH COLD PATCH ON ALL SIDES WILL BE ALLOWED WHEN
 THERE IS NO SNOWFALL EVENT IN THE NEAR TERM WEATHER FORECAST. DURING
 INCLEMENT WEATHER CONDITION CONTRACTOR MUST BACKFILL THE AREA WITH
 TEMPORARY AGGREGATE NO ADDITIONAL COMPENSATION WILL BE PROVIDED FOR
 THIS WORK. THIS WORK SHALL BE INCLUDED IN TRAFFIC CONTROL PROTECTION.

NOTE:
 SEE SHEET 15 FOR LEGEND, NOTES AND DETAILS

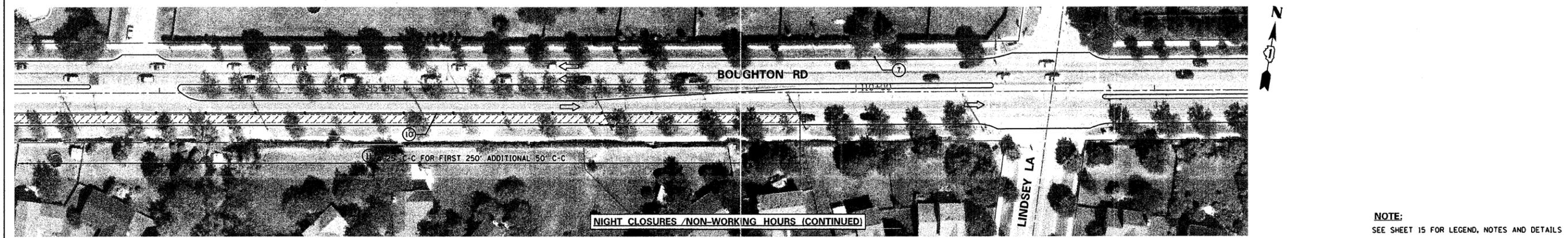
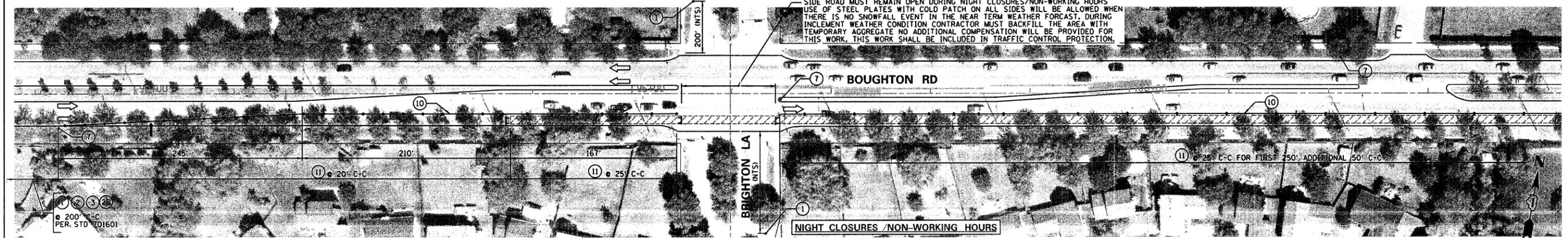
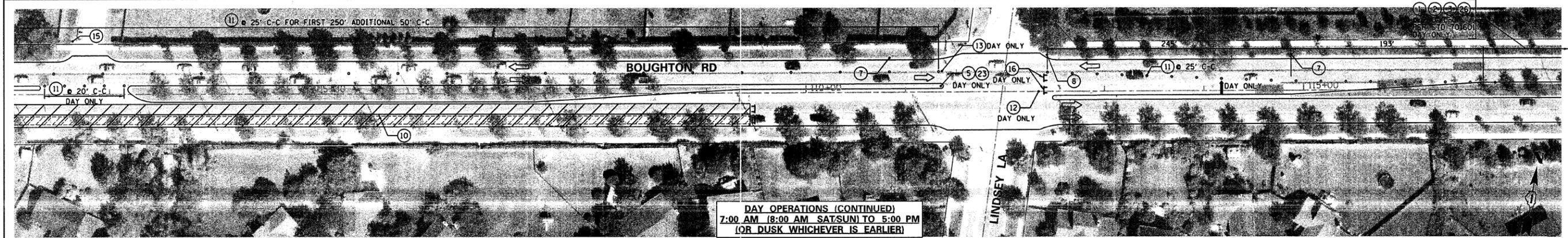
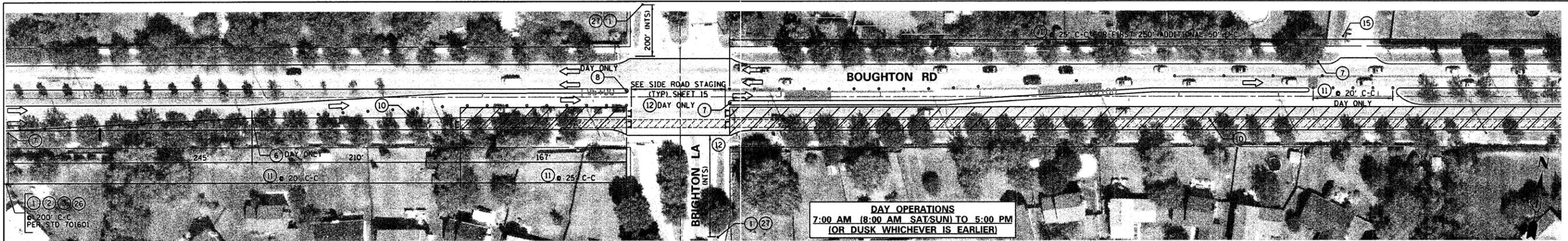
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PLOT SCALE = 50.00 Ft / IN.	CHECKED - E.J / D.R.	REVISED -	3586			11-00056-00-RS	WILL	35	19	
PLOT DATE = 7/20/2011	DATE -	REVISED -	CONTRACT NO. 63621							
ILLINOIS FED. AID PROJECT										

SCALE: 1"=50' SHEET NO. 19 OF 35 SHEETS STA. TO STA.



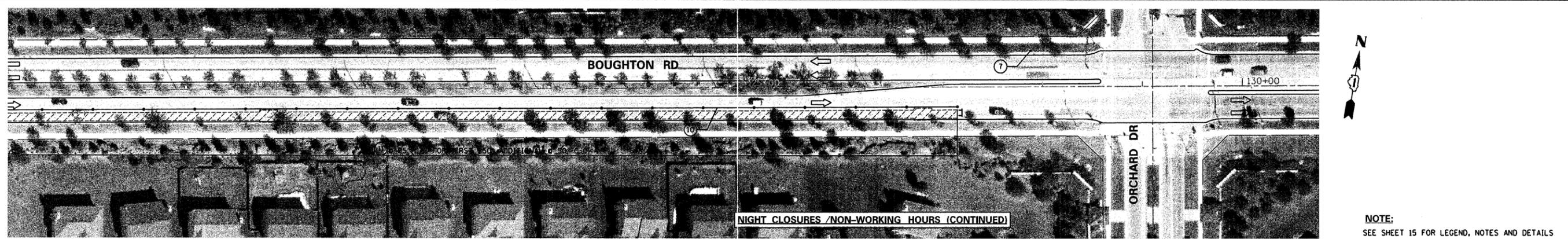
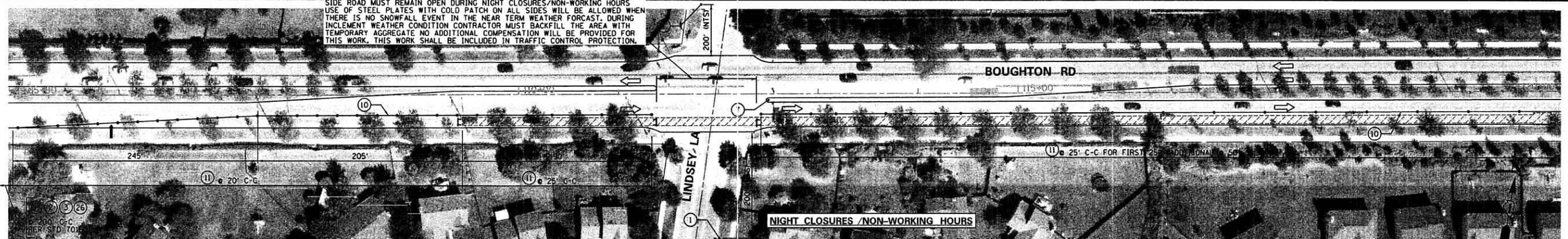
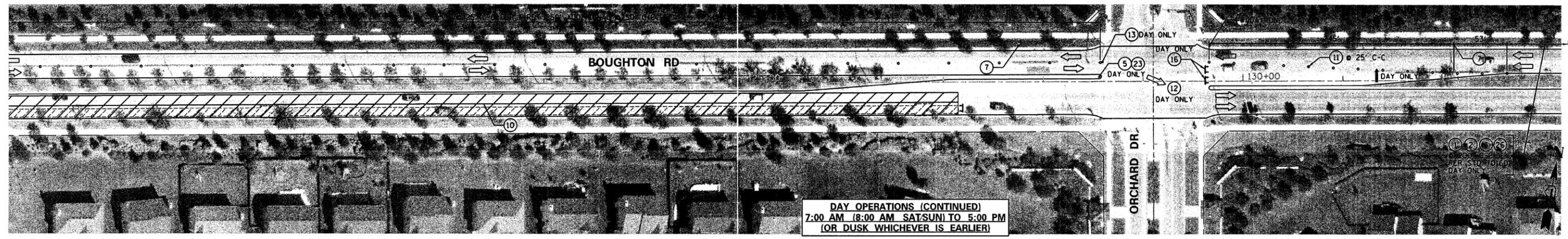
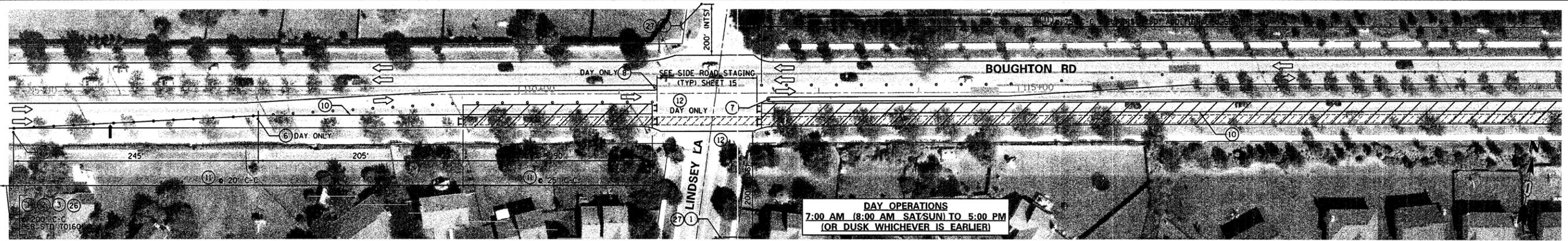
NOTE:
SEE SHEET 15 FOR LEGEND, NOTES AND DETAILS

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PLOT SCALE = 50.00 Ft / IN. PLOT DATE = 7/20/2011				SCALE: 1"=50'		SHEET NO. 20 OF 35 SHEETS		STA. TO STA.		[ILLINOIS] FED. AID PROJECT			



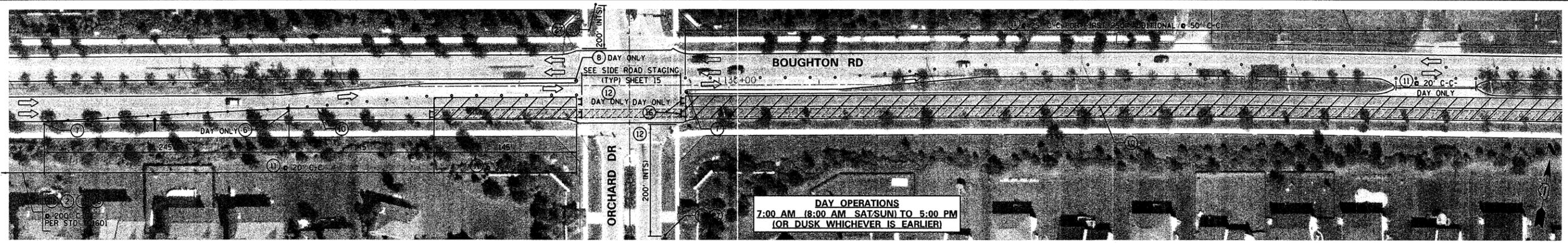
NOTE:
SEE SHEET 15 FOR LEGEND, NOTES AND DETAILS

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PLOT SCALE = 50.00 Ft / IN.	PLOT DATE = 7/28/2011	CHECKED - E.J / D.R. DATE -	REVISED - REVISED -			SCALE: 1"=50'	SHEET NO. 21 OF 35 SHEETS	STA. TO STA.	CONTRACT NO. 63621		
ILLINOIS FED. AID PROJECT											

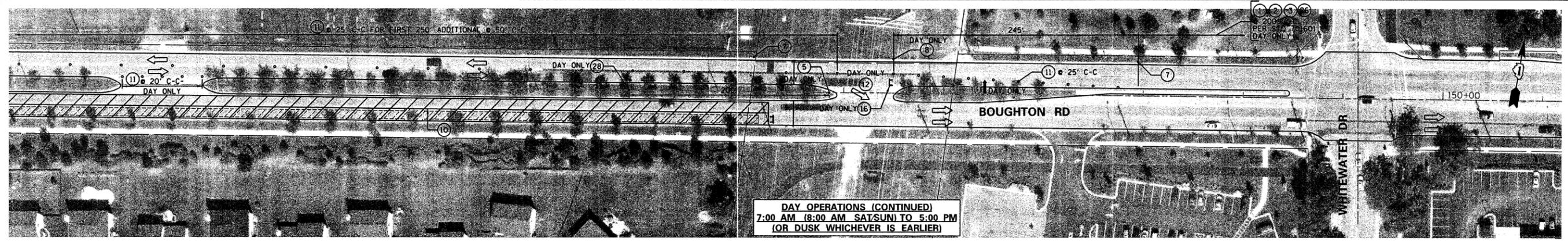


NOTE:
SEE SHEET 15 FOR LEGEND, NOTES AND DETAILS

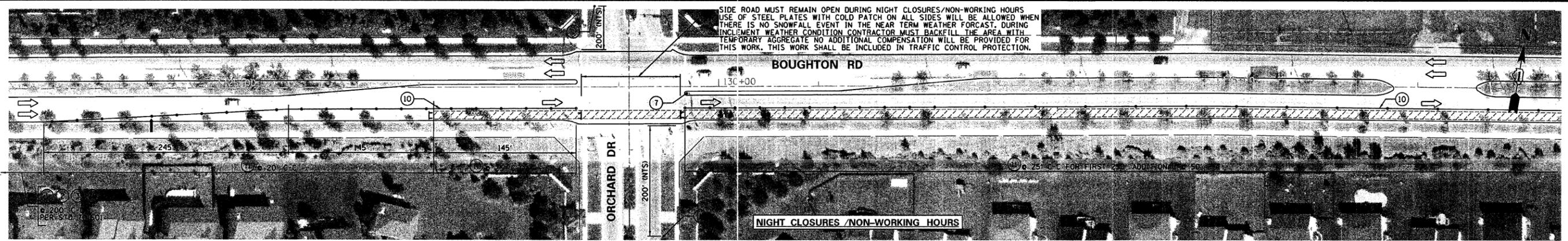
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PLOT SCALE = 50.00 Ft / IN.		DRAWN - G.R.	REVISED -			SCALE: 1"=50'	SHEET NO. 22 OF 35 SHEETS	STA.	TO STA.		
PLOT DATE = 7/20/2011		CHECKED - E.J. / D.R.	REVISED -								
		DATE -	REVISED -								



DAY OPERATIONS
7:00 AM (8:00 AM SAT/SUN) TO 5:00 PM
(OR DUSK WHICHEVER IS EARLIER)

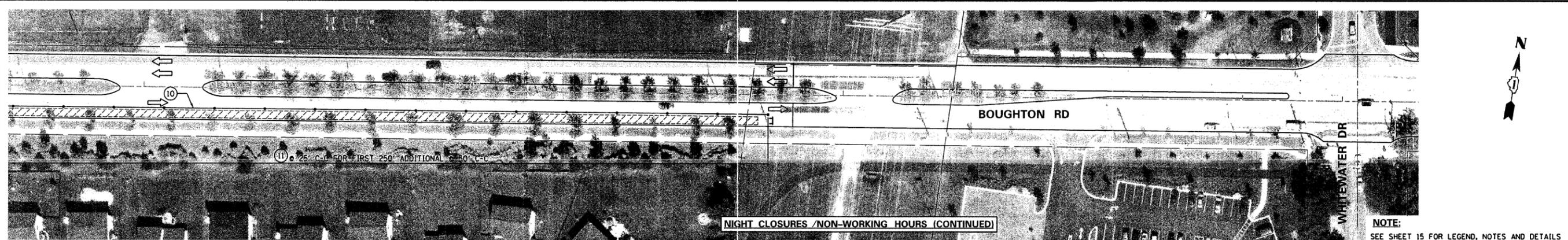


DAY OPERATIONS (CONTINUED)
7:00 AM (8:00 AM SAT/SUN) TO 5:00 PM
(OR DUSK WHICHEVER IS EARLIER)



SIDE ROAD MUST REMAIN OPEN DURING NIGHT CLOSURES/NON-WORKING HOURS
USE OF STEEL PLATES WITH COLD PATCH ON ALL SIDES WILL BE ALLOWED WHEN
THERE IS NO SNOWFALL EVENT IN THE NEAR TERM WEATHER FORECAST. DURING
INCLEMENT WEATHER CONDITION CONTRACTOR MUST BACKFILL THE AREA WITH
TEMPORARY AGGREGATE NO ADDITIONAL COMPENSATION WILL BE PROVIDED FOR
THIS WORK. THIS WORK SHALL BE INCLUDED IN TRAFFIC CONTROL PROTECTION.

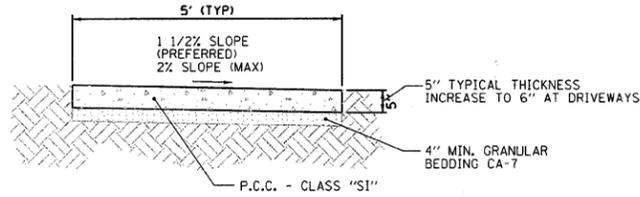
NIGHT CLOSURES /NON-WORKING HOURS



NIGHT CLOSURES /NON-WORKING HOURS (CONTINUED)

NOTE:
SEE SHEET 15 FOR LEGEND, NOTES AND DETAILS

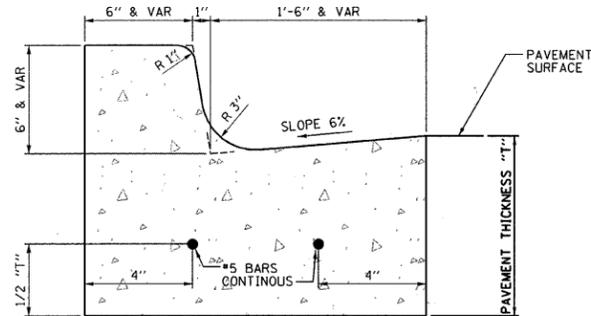
FILE NAME = n:\dgn\bb\bb04boughtonrdresurfacing\sheet\BB04Orchard-WhitewaterMat.dgn	USER NAME = .USER.	DESIGNED - J.W.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BOUGHTON ROAD - ORCHARD DR TO WHITEWATER DR MAINTENANCE OF TRAFFIC / SUGGESTED STAGING PLAN	F.A.U. RTE. 3586	SECTION 11-00056-00-RS	COUNTY WILL	TOTAL SHEETS 35	SHEET NO. 23	
PLOT SCALE = 50.00 Ft / IN.	DATE = 7/20/2011	DRAWN - G.R.	REVISED -			SCALE: 1"=50'	SHEET NO. 23 OF 35 SHEETS	STA. TO STA.	CONTRACT NO. 63621		
		CHECKED - E.J / D.R.	REVISED -			ILLINOIS FED. AID PROJECT					
		DATE -	REVISED -								



NOTES:

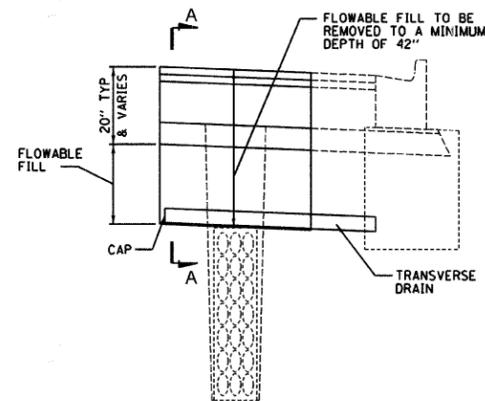
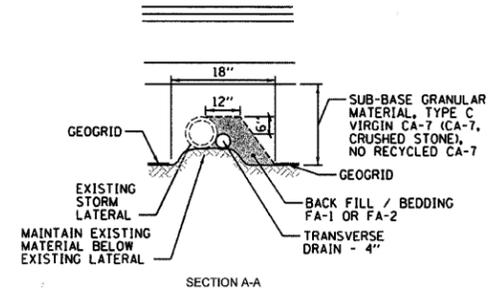
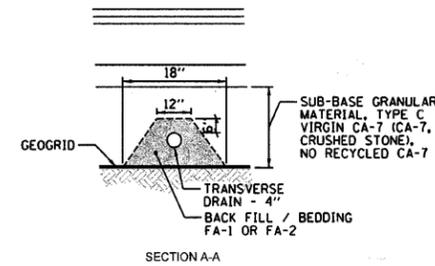
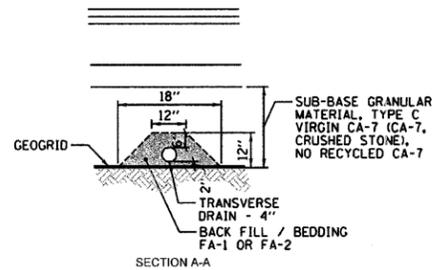
1. THE ACTUAL WIDTH OF THE SIDEWALK IS TO BE PLUS 0" TO 1/2" OF THE WIDTH SPECIFIED.
2. A 1/2" PREFORMED EXPANSION JOINT, CONFORMING TO THE EXACT CROSS-SECTION OF THE SIDEWALK, SHALL BE PLACED AT 50 FOOT INTERVALS AND AT THE END OF EACH POUR.
3. CONTRACTION JOINTS SHALL BE PLACED AT 5 FOOT INTERVALS.
4. THIS ITEM SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQ. FT. FOR P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED WHICH SHALL BE FULL COMPENSATION FOR ALL LABOR AND MATERIAL REQUIRED INCLUDING THE EXPANSION AND CONTRACTION JOINTS AS SPECIFIED, AND ANY MINOR TOUCH-UP EXCAVATION AND BACKFILLING NECESSARY TO CONSTRUCT THE SIDEWALK.
5. SEE STATE DETAILS IN CONTRACT DOCUMENTS FOR CONSTRUCTION OF RED PATTERNED HANDICAPPED RAMPS.
6. LONGITUDINAL SLOPE SHALL NOT EXCEED 5%.

SIDEWALK DETAIL

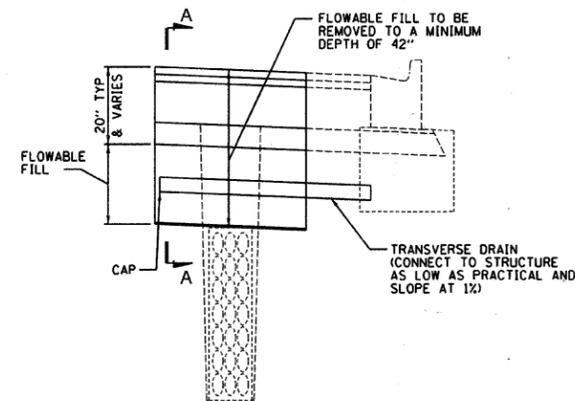


CURB & GUTTER DETAIL

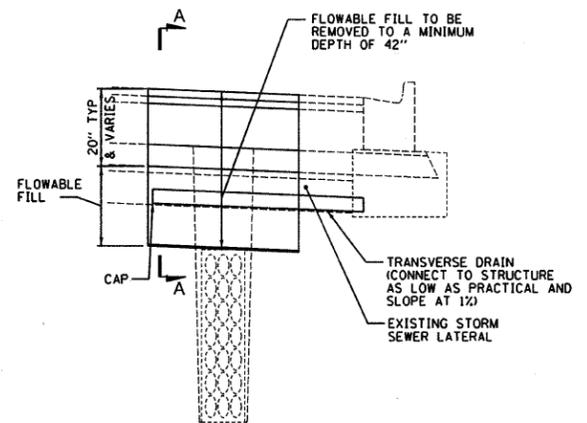
SHOWING CONTINUOUS REINFORCEMENT
SEE STD 606001 FOR ADDITIONAL INFORMATION



CASE I
EXISTING STRUCTURE DEPTH ≥ 42" (3.5')



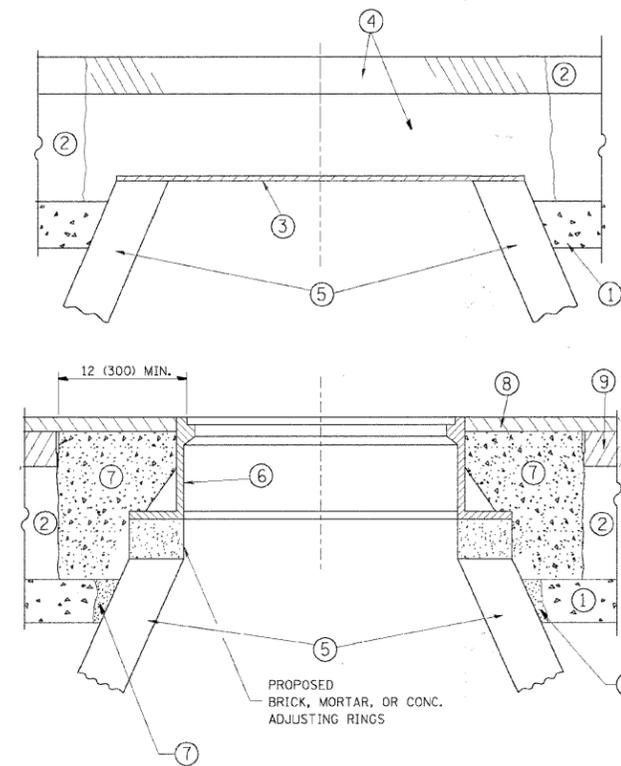
CASE II
EXISTING STRUCTURE DEPTH < 42" (3.5')



CASE III
EXISTING STRUCTURE DEPTH < 42" (3.5')
AND EXISTING SHALLOW STORM SEWER LATERAL

SECTIONS AT TRANSVERSE DRAIN

FILE NAME =	USER NAME = USER	DESIGNED - J.W.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CONSTRUCTION DETAILS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ns\dgn\bb\bb24boughtondresurfacing\sh	sts\BB24ConstructionDetails.dgn	DRAWN - G.R.	REVISED -			3586	11-00056-00-RS	WILL	35	24
PLOT SCALE = 1:00 Ft / IN.	CHECKED - E.J / D.R.	REVISED -	REVISED -			CONTRACT NO. 63621				
PLOT DATE = 7/20/2011	DATE -	REVISED -	REVISED -			ILLINOIS FED. AID PROJECT				
				SCALE: NONE	SHEET NO. 24 OF 35 SHEETS	STA.	TO STA.			



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

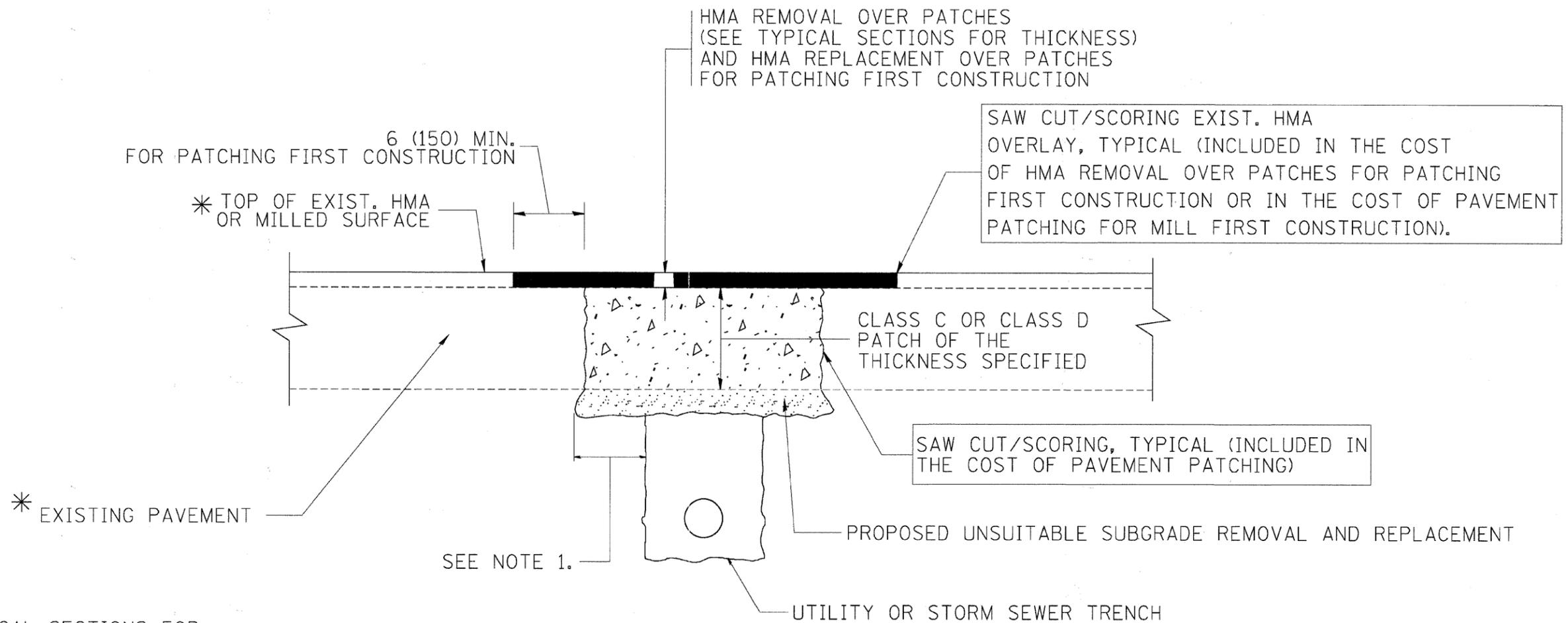
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		PLOT DATE = 3/18/2011	REVISED - R. BORO 03-09-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3586	11-00056-00-RS	WILL	35	25
BD600-03 (BD-8)			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\dststd22x34\bd22.dgn	USER NAME = bouerd1	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT		F.A. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - R. BORO 01-01-07		3586	11-00056-00-RS	WILL	35	26		
	PLOT DATE = 10/27/2008	CHECKED -	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)		CONTRACT NO.				
		DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

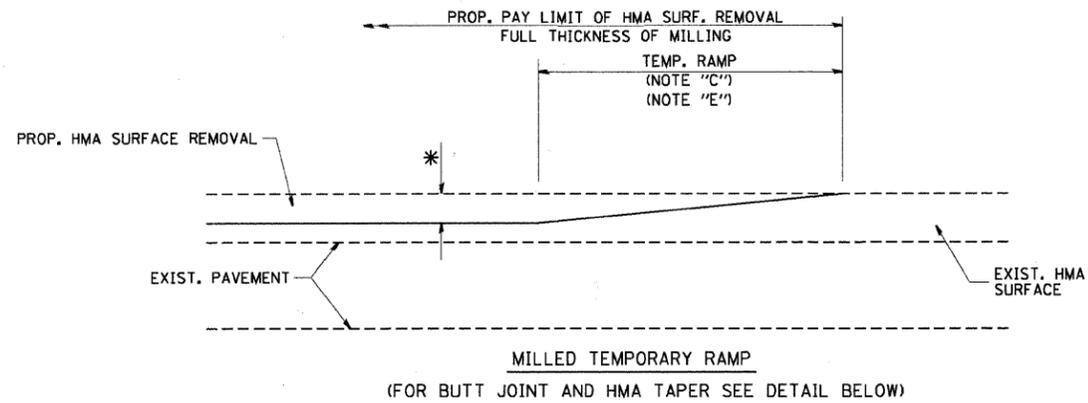
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

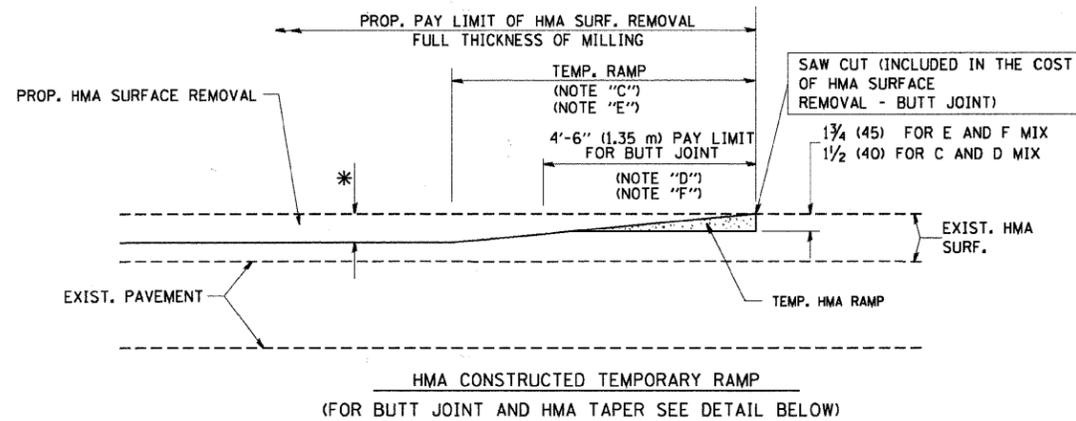
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

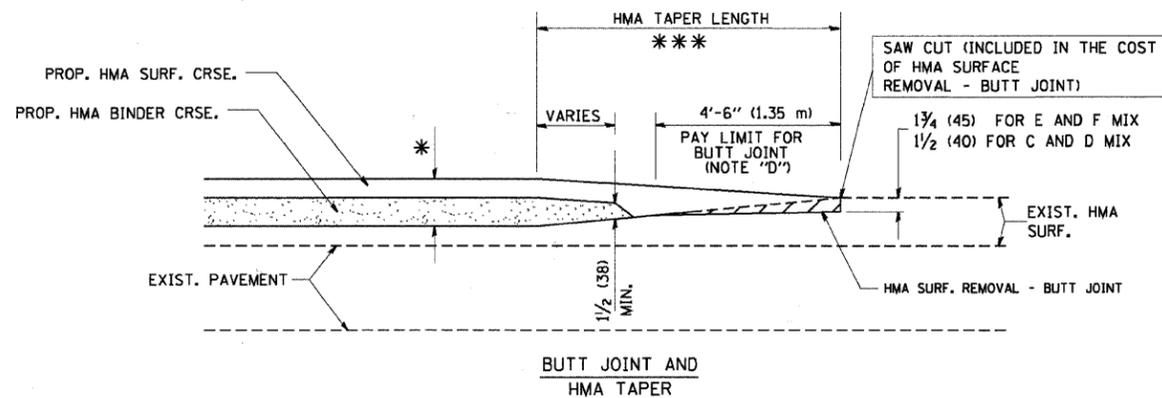
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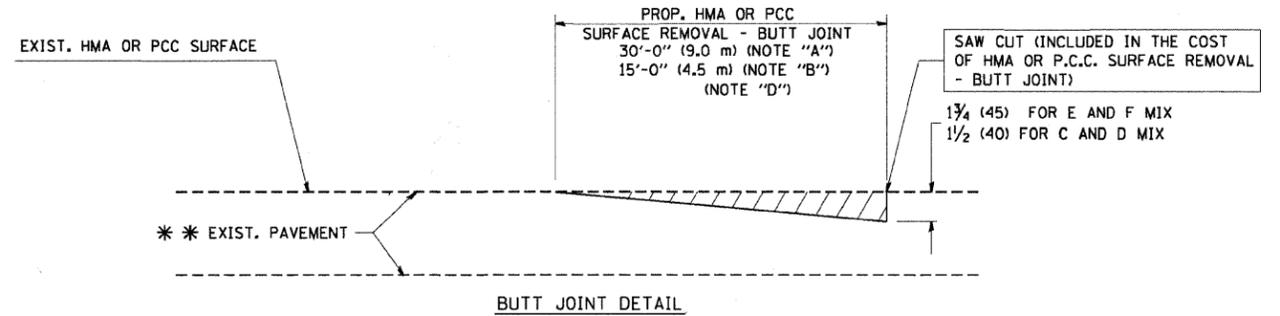
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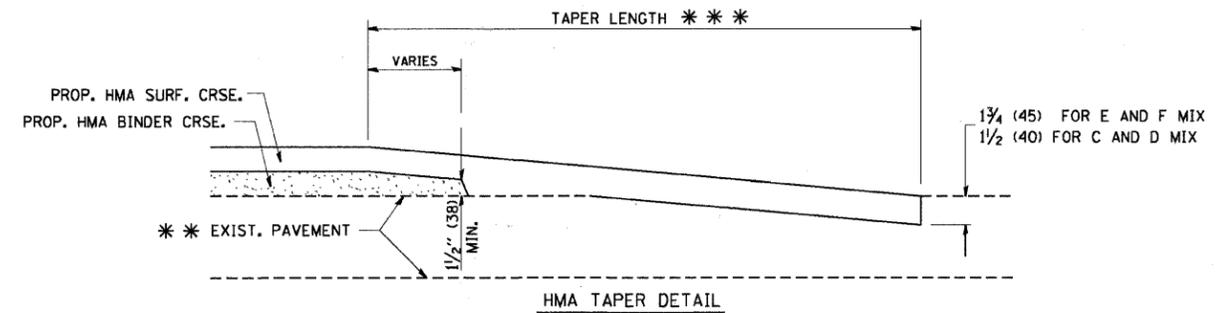
**OPTION 2
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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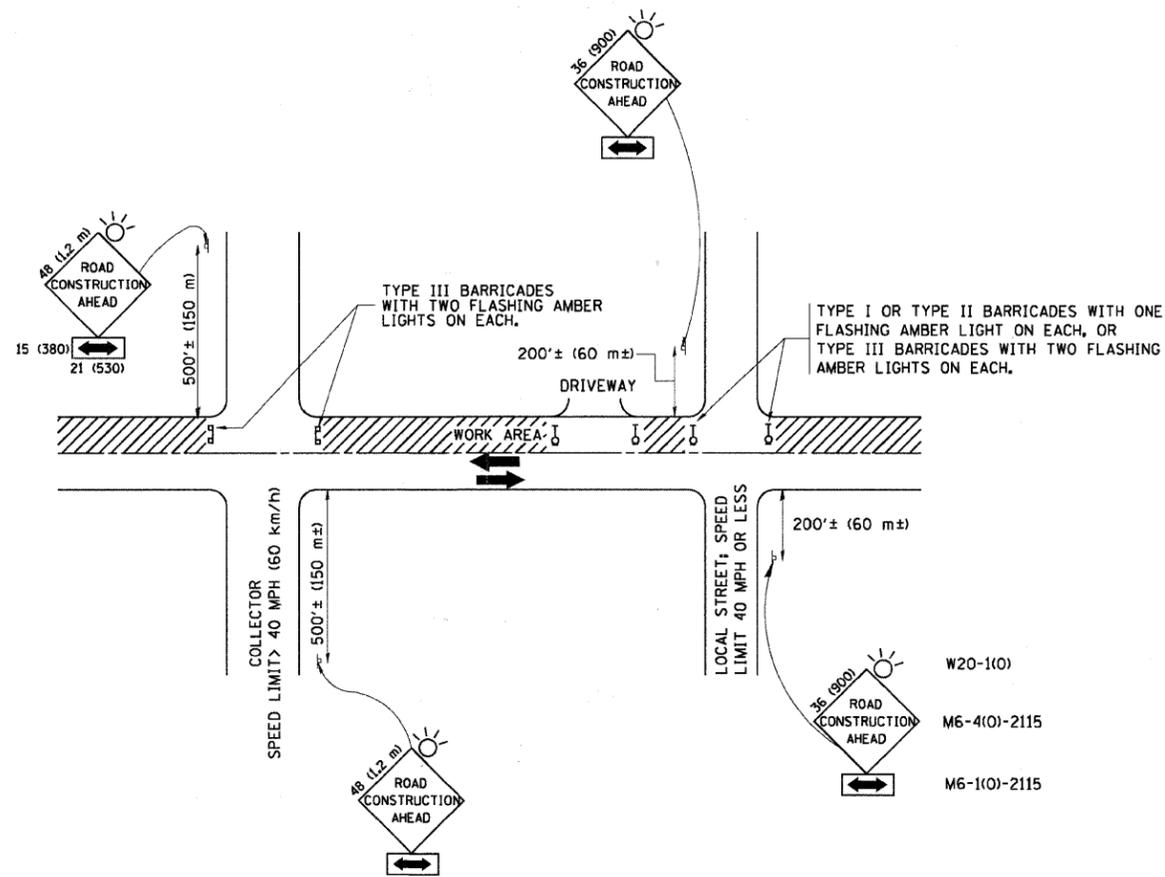
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PLOT DATE = 1/4/2008	CHECKED -	REVISED - M. GOMEZ 04-06-01
	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3586	11-00056-00-RS	WILL	35	28
BD400-05 BD32			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS:
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

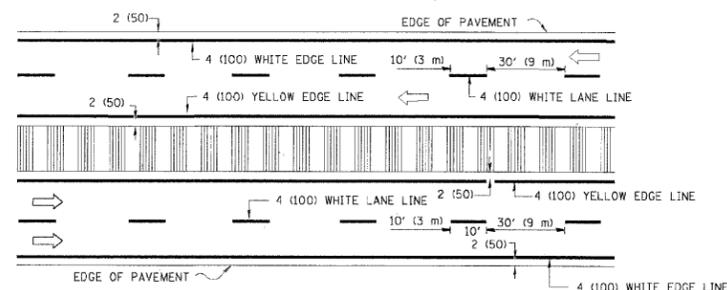
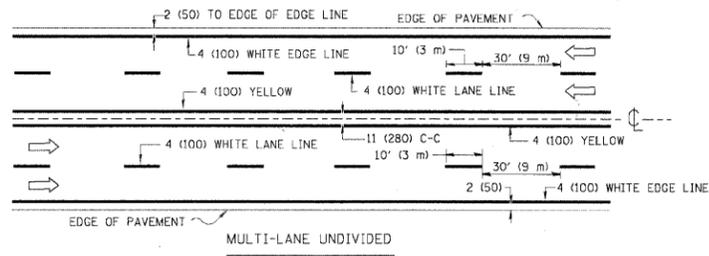
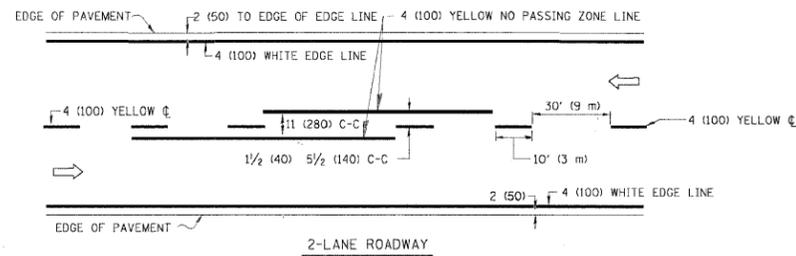
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	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

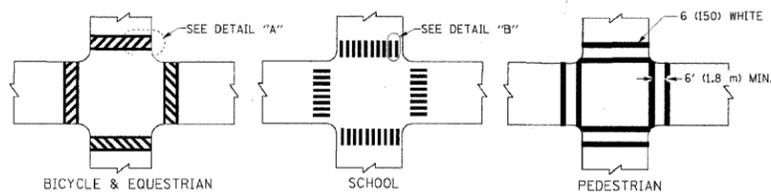
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3586	11-00056-00-RS	WILL	35	29
TC-10			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

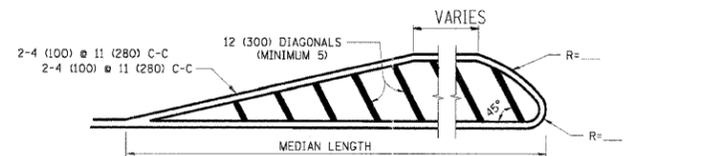
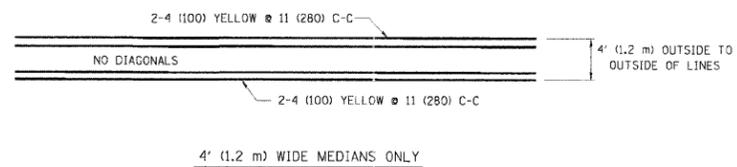


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

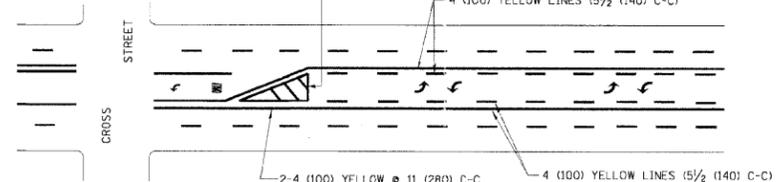


TYPICAL CROSSWALK MARKING

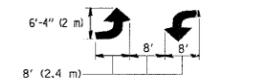


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

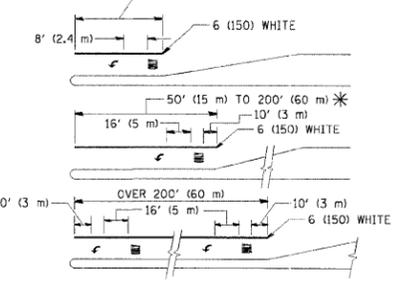
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



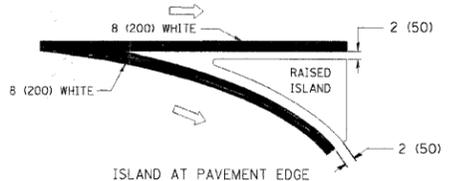
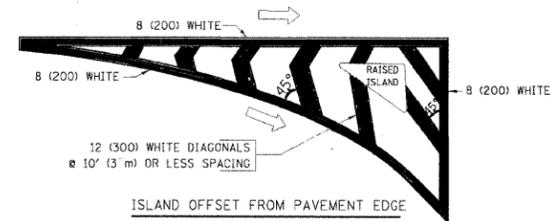
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

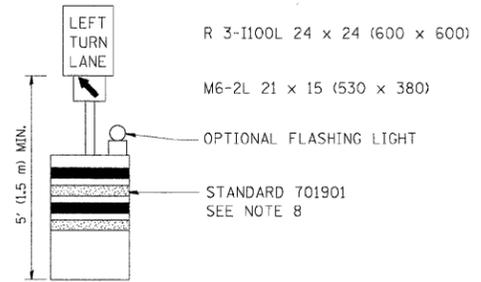
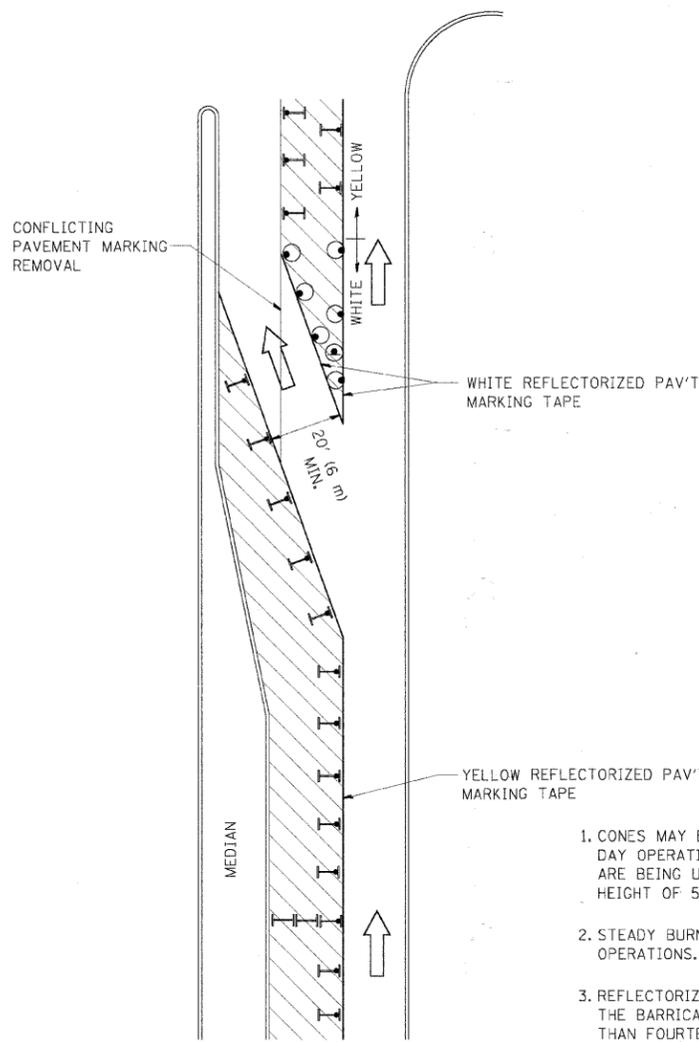


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

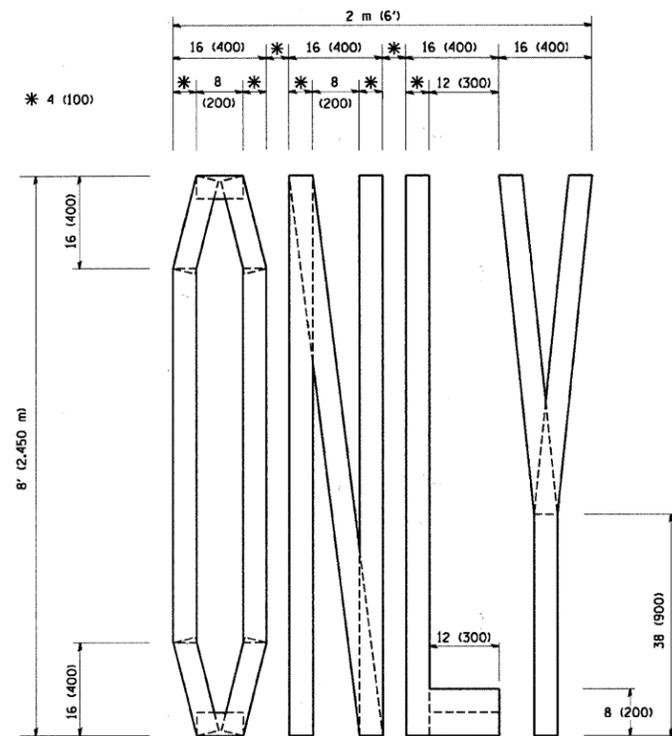
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		REVISED - A. HOUSEH 10-12-96	REVISED -
		REVISED - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

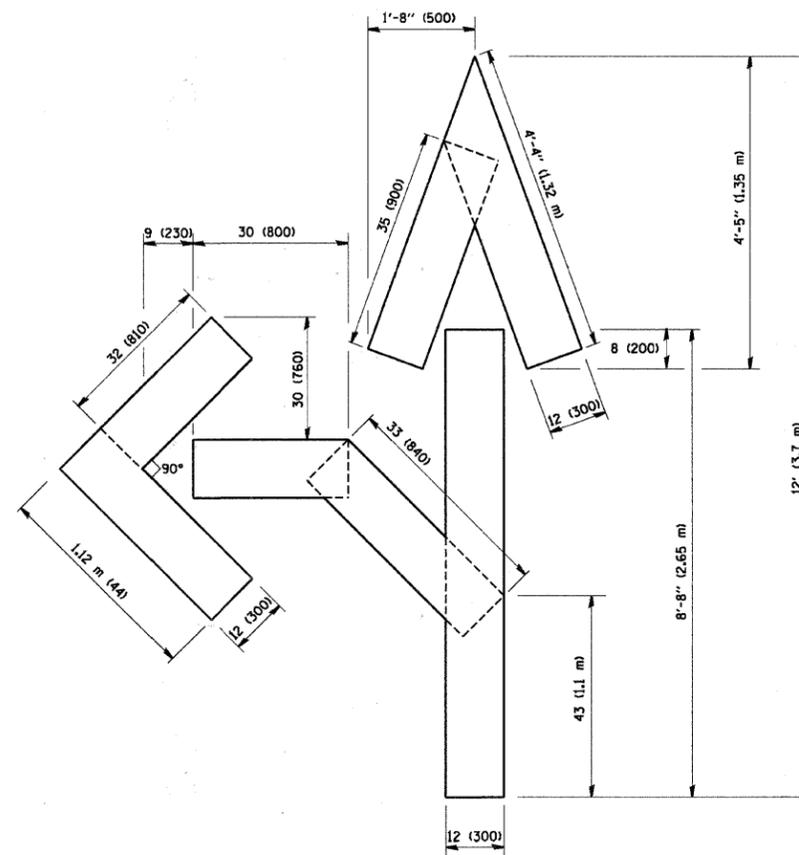
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

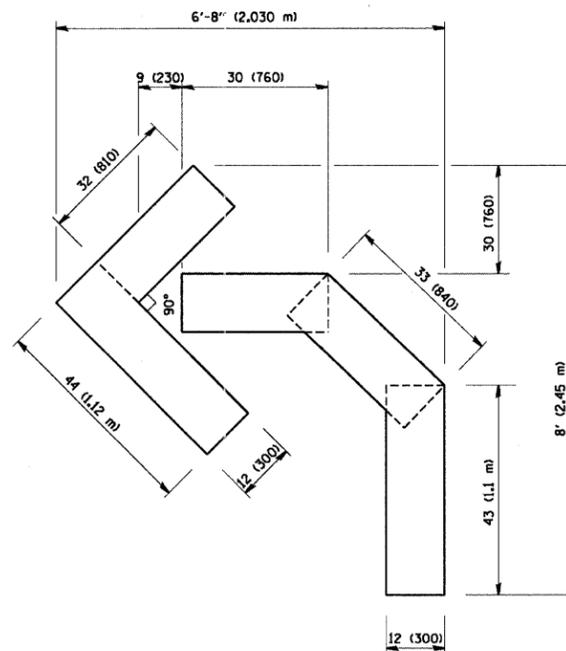
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3586	11-00056-00-RS	WILL	35	31
TC-14		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

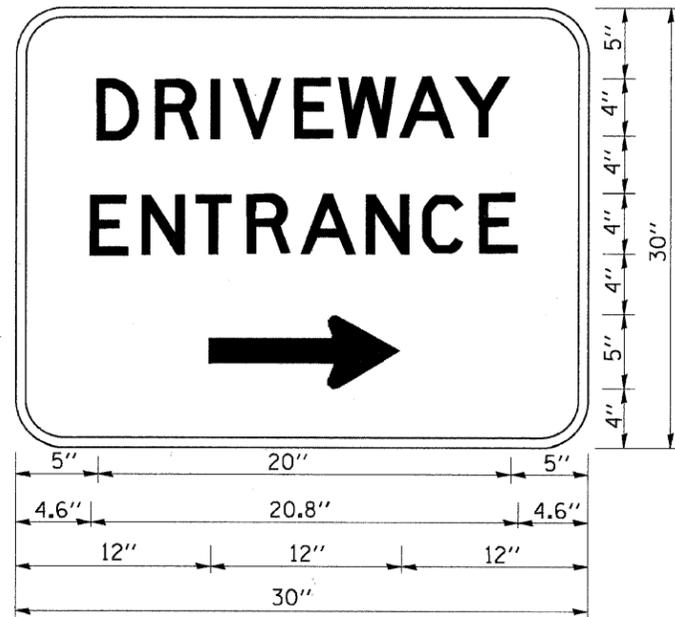
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	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3586	11-00056-00-RS	WILL	35	32
TC-16			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

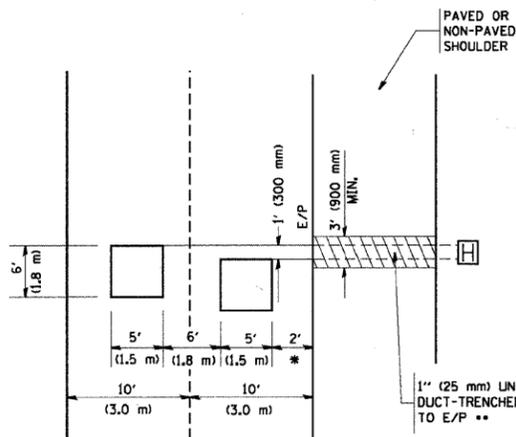
NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = W:\diststd\22x34\to26.dgn	USER NAME = geglienobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
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		DATE -	REVISED -									FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

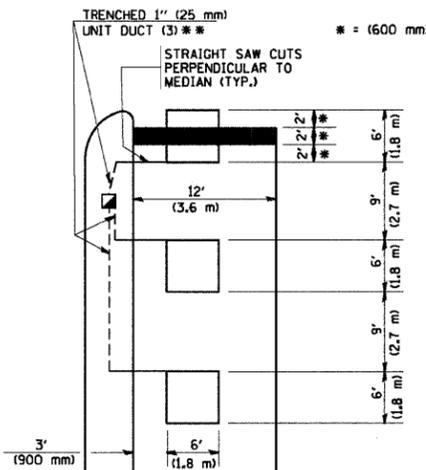


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

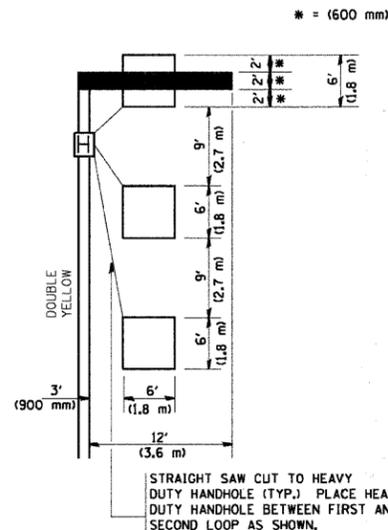
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

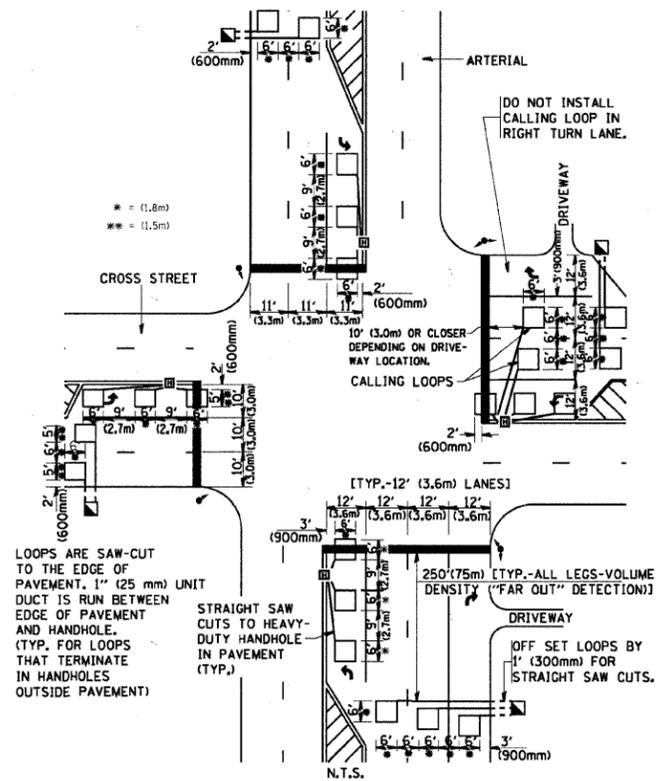
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**

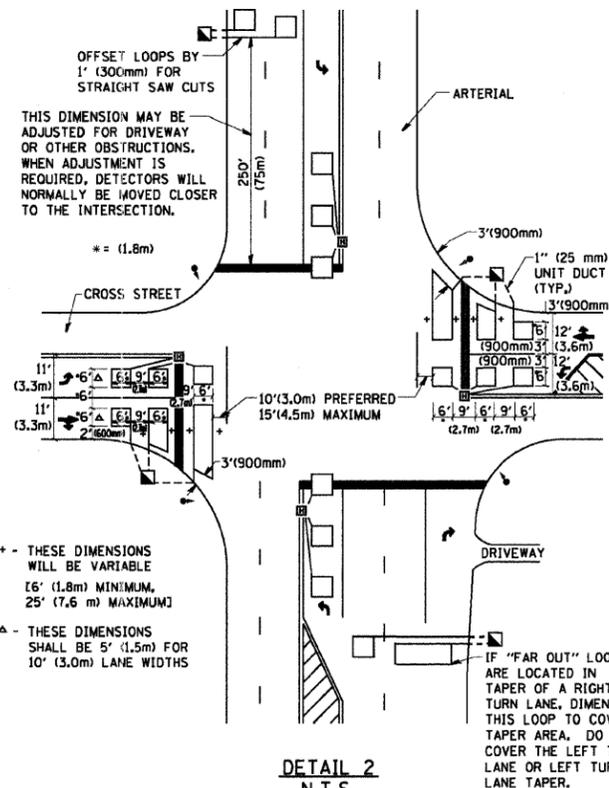


LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]
▲ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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		DRAWN -	REVISED -
		CHECKED - R.K.F.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3586	11-00056-00-RS	WILL	35	35
TS-07		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				