

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90	*	COOK	556	301
FED. ROAD DIST. NO.	F.LINING	FED. AID PROJECT		
			62300 * (1818, ETC, 2324.6-1P1R-8)	

GENERAL NOTES

SPECIFICATIONS:

- DESIGN: AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals. ("AASHTO Specifications") (2)
- CONSTRUCTION: Current (at time of letting) Illinois Department of Transportation Standard Specifications for Road and Bridge Construction, Supplemental Specifications and Special Provisions. ("Standard Specifications")
- LOADING: 80 M.P.H. WIND VELOCITY PLUS 30% GUST FACTOR
WIND LOADING: 35 p.s.f. normal to Sign Panel Area and truss elements not behind sign Loading Diagram.
WALKWAY LOADING: Dead load plus 500 lbs. concentrated live load.
- MINIMUM CLEARANCE: 3" greater than bridge members at all locations. (All Obstructions)
- WELDING: All welds to be continuous unless otherwise shown. All welding to be done in accordance with current AWS D1.1 Structural Welding Code (Steel) and the Standard Specifications.
- MATERIALS: All Structural Steel Pipe shall be ASTM A53 Grade B with a minimum yield of 35,000 p.s.i., or A500 Grade B or C with a minimum yield of 46,000 p.s.i. If A500 pipe is substituted for A53, then the outside diameter shall be as detailed and wall thickness greater than or equal to A53.
All Structural Steel Plates and Shapes shall conform to AASHTO M270 Gr. 36, Gr. 50 (M183, M223 Gr. 50.).
- HIGH STRENGTH BOLTS: All bolts, washers, nuts and locknuts shall satisfy the requirements of ASTM designation A307 unless noted as "H.S." which shall require AASHTO M164 (A325), ASTM A449, or approved alternate. All fasteners shall be hot dip galvanized per AASHTO M232 unless otherwise specified.
- GALVANIZING: All Steel Grating, Plates, Shapes and Pipe shall be Hot Dip Galvanized after fabrication in accordance with AASHTO M111. Painting is not permitted.
- ANCHOR RODS: All threaded rod conforming to ASTM A307, 3/4" ϕ x 12" long, each with one plate washer and locknut and be hot dip galvanized per AASHTO M232. They shall be either cast into the concrete or epoxy grouted in accordance with Section 584 of the Standard Specifications. Minimum embedment in concrete shall be 9".

- Bracket spacing $g \leq 6'-0"$ max. Spacing shall be uniform if possible but may vary $\pm 6"$ to miss existing obstruction (rail post, light poles, web stiffeners, splice plates, etc.). Adjust bracket lengths accordingly on skewed structures.
 - Any design modifications shall be based on the current version of applicable specifications and submitted for the Engineer's approval.
 - Unit price includes grating, handrail, brackets, supports, anchor bolts, fasteners, fabrication, delivery, erection, field drilling and other necessary items. Limits of payment are based on grating length (c_w , d_w) unless otherwise specified.
- For Safety Chain Details and Details D, F and G, see Base Sheet BM-4 Special.
(4) If walkway bracket at safety chain location is behind sign, add angle to bracket. See detail on Base Sheet BM-4 Special.

NUMBER	REVISION	DATE
△	ADDENDUM 1	8/21/05

TOTAL BILL of MATERIAL

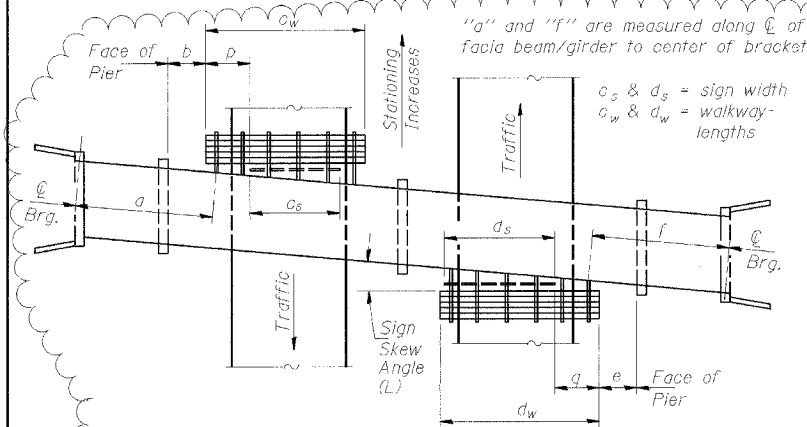
(3) OVERHEAD SIGN STRUCTURE - BRIDGE MOUNTED	Foot	332
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SGN-21

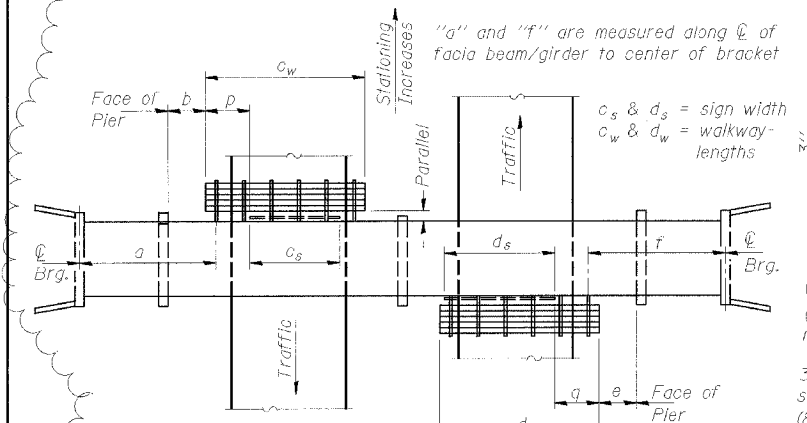
BRIDGE MOUNT SIGN STRUCTURES
GENERAL PLAN AND ELEVATION

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94/90 (DAN RYAN EXPRESSWAY)

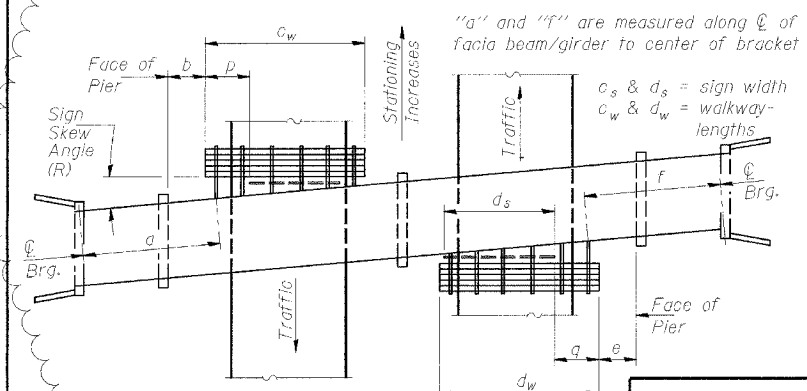
31ST STREET TO 71ST STREET
NB EXPRESS LANE RECONSTRUCTION
BRIDGE MOUNT SIGN STRUCTURES
GENERAL PLAN AND ELEVATION



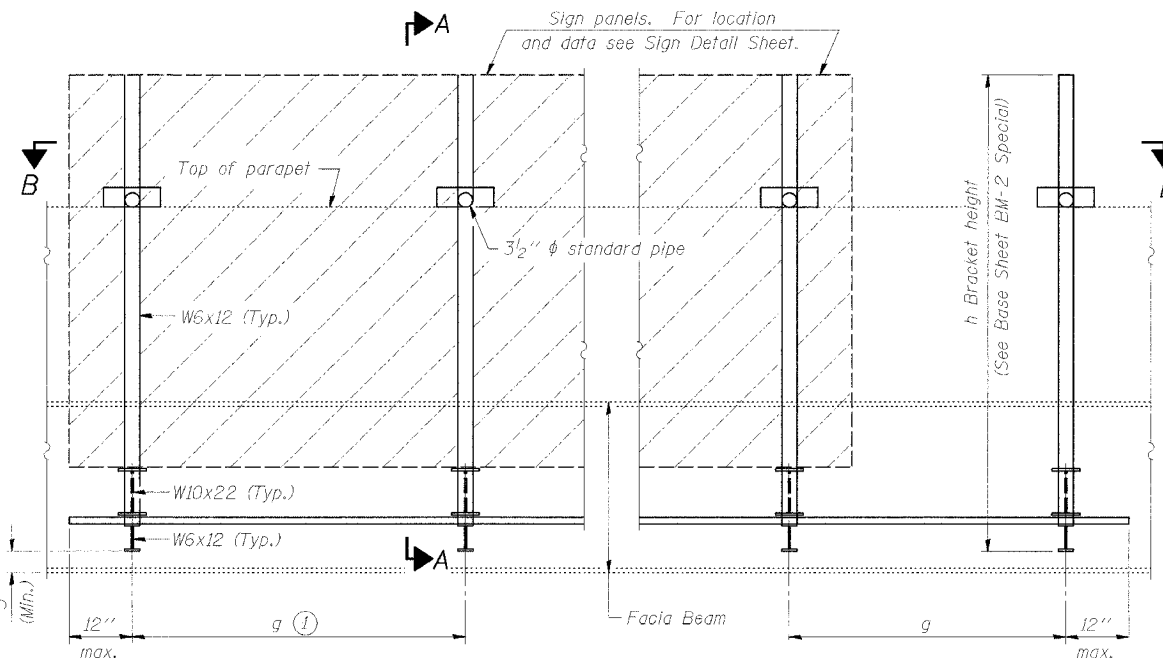
PLAN
(Left Sign Skew > 15°)
WALKWAY AND HANDRAIL SKETCH
(Road plan beneath structure varies.)



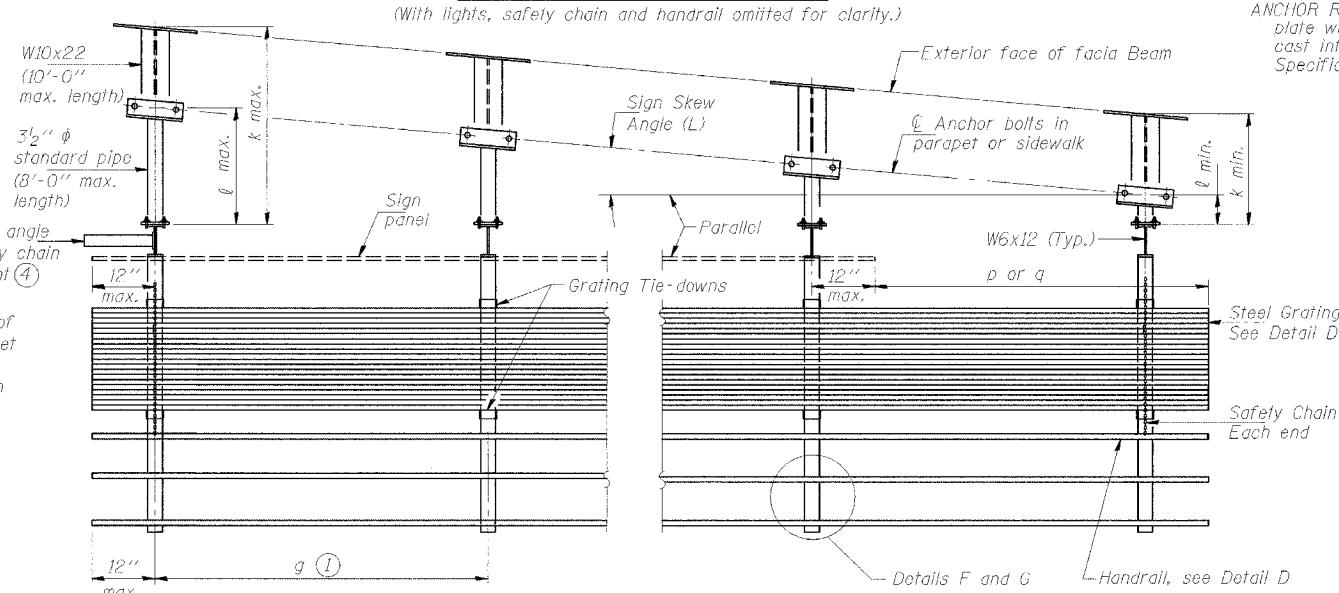
PLAN
(For Sign Skew $\leq 15^\circ$, all brackets constant)
WALKWAY AND HANDRAIL SKETCH
(Road plan beneath structure varies.)



PLAN
(Right Sign Skew > 15°)
WALKWAY AND HANDRAIL SKETCH
(Road plan beneath structure varies.)



TYPICAL FRONT ELEVATION
(With lights, safety chain and handrail omitted for clarity.)



SECTION B-B
(Shown: Left Sign Skew > 15°)

Structure Number	Sign Skew Angle (L) or (R)	Bridge Station	Bridge Structure Number	Contract Route Designation	a	b	c _s	c _w	d _s	d _w	e	f	g	No. of Brackets (Total)	p	q	Total Grating/Handrail Lengths (c _w + d _w)
IB0161094L055.73	0	4582+26±	016-0398	I-94	-	-	-	-	23'/17.5'	56'	7'	67.37'	5'-3"/5'-2"	12	-	9.5'	56'
IB0161094L054.97	0	4622+31±	016-1158	I-94	-	-	-	-	23'/17.5'	52'	7'	113.83'	5'-3"/5'-2"	11	-	9.5'	52'
IB0161094L054.72	0	4635+47±	016-1159	I-94	-	-	-	-	23'/17.5'	56'	6.92'	117.3'	5'-3"/5'-2"	12	-	9.5'	56'
IB0161094L057.2	(L)1°24'0"	2501+77.65	016-1157	I-94	-	-	-	-	16/30	56'	2.75'	53.75'	5'-6"	11	-	5.5'	56'
IB0161094L057.8	(R)10°16'30"	2474+11.88	016-1152	I-94	-	-	-	-	16/30	56'	2.5'	-	5'-6"	11	-	5.5'	56'
IB0161094L056.7	(R)14°36'25"	4528+51.01	016-1156	I-94	-	-	-	-	16/30	56'	2.5'	-	5'-6"	11	-	5.5'	56'

Dimensions a, b, e, f & g may vary as approved by the Engineer, see (1).
When $c_w < c_s$ and/or $d_w < d_s$, use alternate brackets without walkway supports where applicable, see (3).

DESIGNED	RLK	20	EXAMINED	
CHECKED	EL		PASSED	
DRAWN	ABW		ENGINEER OF BRIDGE DESIGN	
CHECKED	PJM		ENGINEER OF BRIDGES AND STRUCTURES	

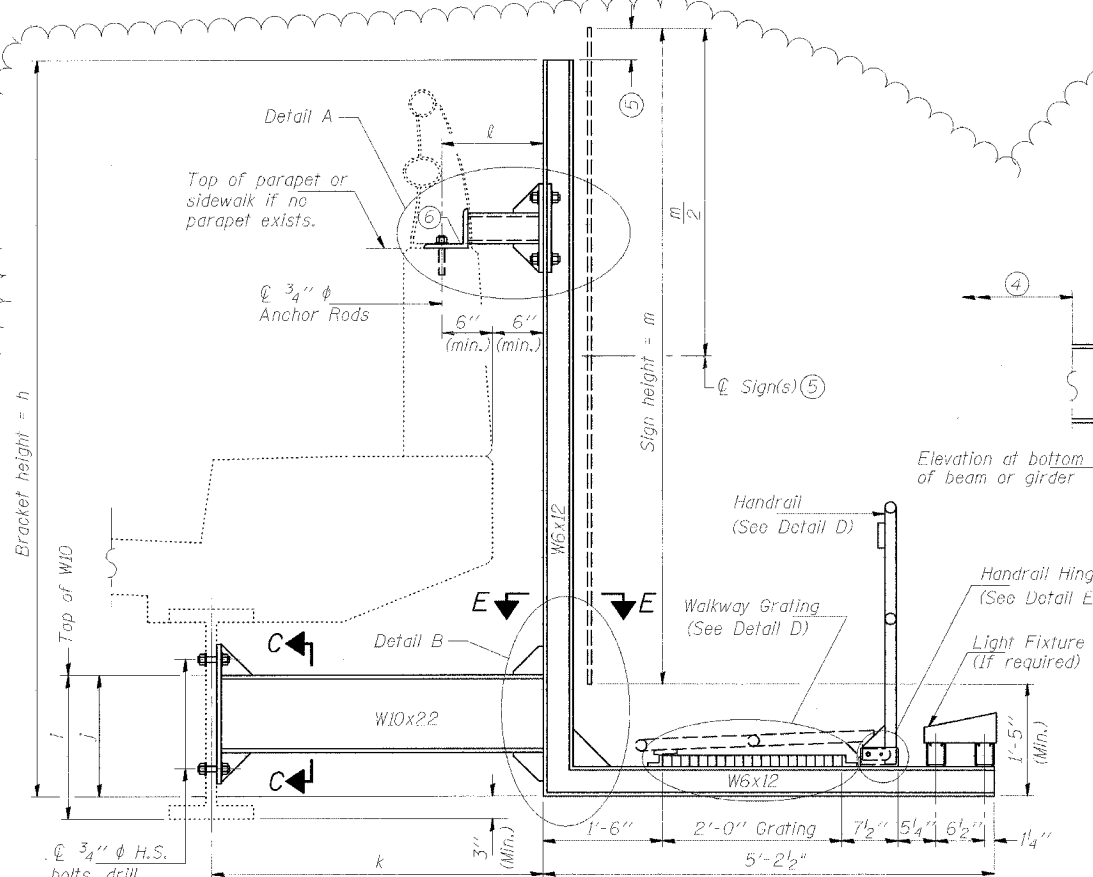
BM-1 Special 1/7/05

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

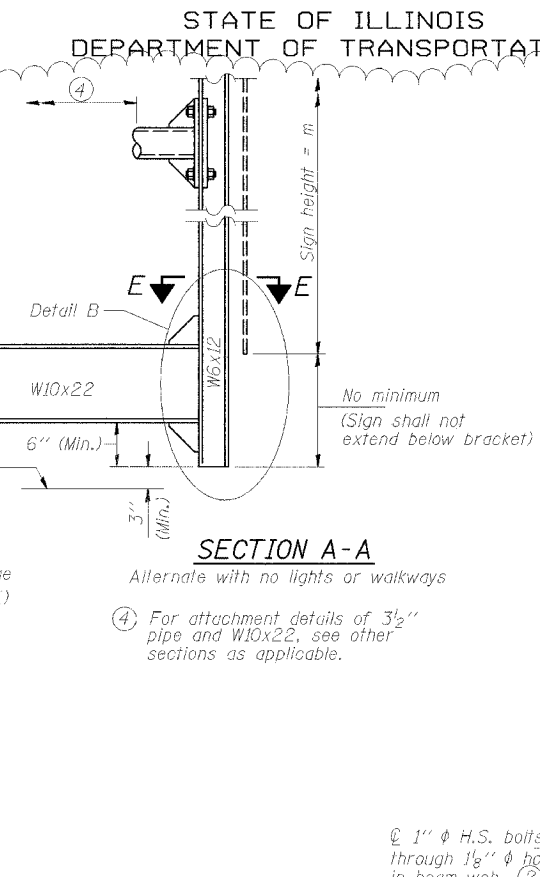
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90	*	COOK	556	302
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT-		

SHEET NO. -
SHEETS

62300 *11818, ETC, 2324.6-IPR-8

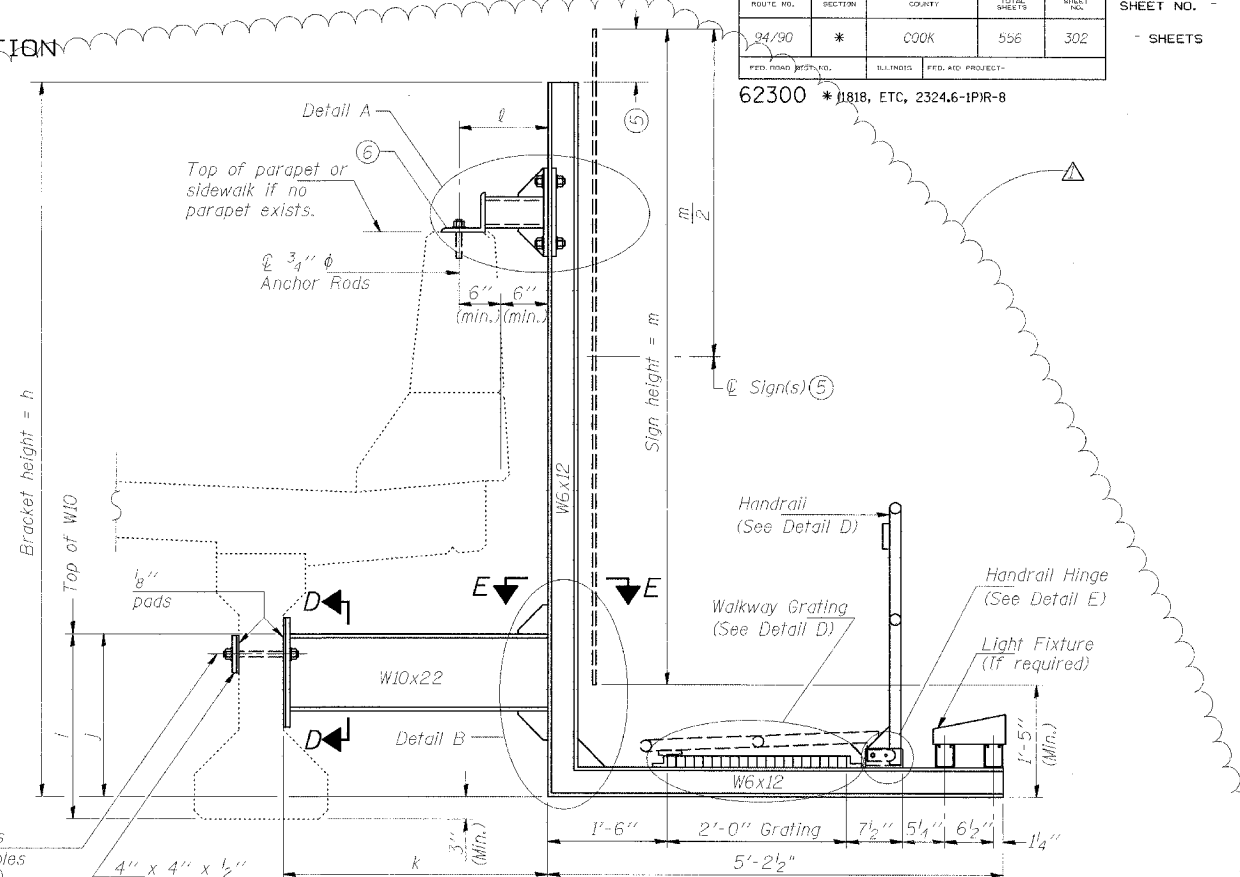


SECTION A-A
Details for mounting to steel beam or girder
& Details for mounting with existing parapet mounted rail



SECTION A-A
Alternate with no lights or walkways

(4) For attachment details of 3/2" pipe and W10x22, see other sections as applicable.



SECTION A-A
Details for mounting to PPC I Beam or Bulb "T"
& Details for mounting to parapet w/o rail

NOTES:
Installations not within dimensional limits shown require special analysis for all components and must be submitted to the Bureau of Bridges and Structures for approval.
Contractor shall field check all pertinent existing bridge dimensions shown on plans before submitting shop drawings.
All holes in bridge beams or girders should be located in the middle half of the member's depth. There shall be no holes drilled in the lower quarter of the member's depth. (For R.C. girder, depth = bottom of deck to bottom of the girder.) Proposed exceptions must be approved by the Bureau of Bridges and Structures.
The Engineer may adjust dimension "i" to meet the above condition and to keep the sign level.

- (5) Sign shall not extend more than 6" above top of bracket, and this dimension may vary to keep sign level if bridge is on grade or vertical curve. Multiple signs of various heights shall share a common horizontal centerline and use equal bracket heights. If no sign is attached to a W6x12 vertical (bracket only supporting walkway), dimension h shall be the same as an adjacent bracket with a sign attached, unless Engineer specifically directs shorter brackets due to locational restraints on future uses. (See Detail A for minimum bracket height.)
- (6) For bridge mounted sign structures installed on new bridges with railing, during design, bracket spacing must be coordinated with railing post spacing and the Contractor must install upper brackets prior to railing installation. For bridge mounted sign structures installed on existing bridges with railing, during design, brackets spacing must be coordinated with railing post spacing and the Contractor must temporarily remove sections of railing to facilitate upper bracket installation. If it is determined during design that existing railings can't be removed, alternate upper connection details must be developed for the contract plans and approved by the Bureau of Bridges and Structures.

Structure Number	Station	h	i	j	k max. (10'-0" max.)	l max. (8'-0" max.)	m (15'-0" max.)
IB0161094L055.73	4562+26±	14.92'	1.69'	1.44'	3.64'	1.13'	14'
IB0161094L054.97	4622+31±	14.92'	1.69'	1.44'	3.50'	1.09'	14'
IB0161094L054.72	4635+47±	14.92'	1.81'	1.56'	3.50'	1.09'	14'
IB0161094L057.2	2501+77.65	13.5'	1.5'	1.75'	3'-8"	1'-1"	12.5'
IB0161094L057.8	2474+11.88	13.5'	1.5'	1.75'	3'-7"	1'-1"	12.5'
IB0161094L056.7	4528+51.01	13.5'	1.5'	1.75'	3'-1"	1'-1"	12.5'

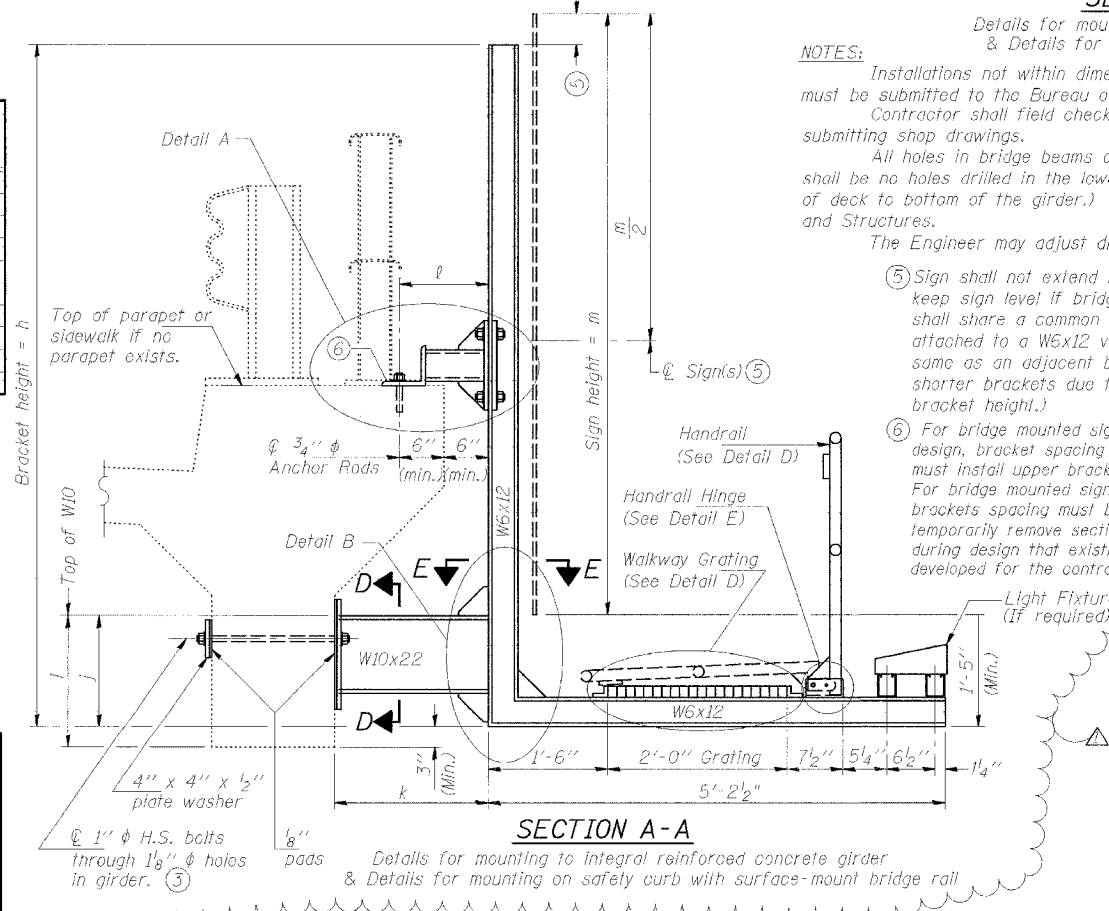
For Details A & B, Sections C-C, D-D and E-E, see Base Sheet BM 3 Special.
For Details D & E, see Base Sheet BM 4 Special.

- (1) Holes in new steel members may be drilled in the fabrication shop or in the field. Field drill existing members.
- (2) For new PPC I beams, holes shall be formed during casting. For existing PPC I beams, prestressing strand locations shall be determined and spaced to miss strands by 6" min. Minimize spalling during field drilling of existing beams.
- (3) For new construction, form holes. For existing RC beams, locate primary reinforcement and space holes to miss by 6" min. Minimize spalling and concrete fracturing/damage during field drilling of existing concrete. Spalls over 1/4" deep or beyond the coverage of the 4x4 plate washer shall be repaired with epoxy mortar before installing washer.

DESIGNED	RLK	20
CHECKED	EL	EXAMINED
DRAWN	ABW	PASSED
CHECKED	PJM	ENGINEER OF BRIDGES AND STRUCTURES

NUMBER	REVISION	DATE
1	ADDENDUM 1	8/21/05

BM-2 Special 1/7/05



SECTION A-A
Details for mounting to integral reinforced concrete girder
& Details for mounting on safety curb with surface-mount bridge rail

SGN-22

**BRIDGE MOUNT SIGN STRUCTURES
WALKWAY AND CONNECTION DETAILS**

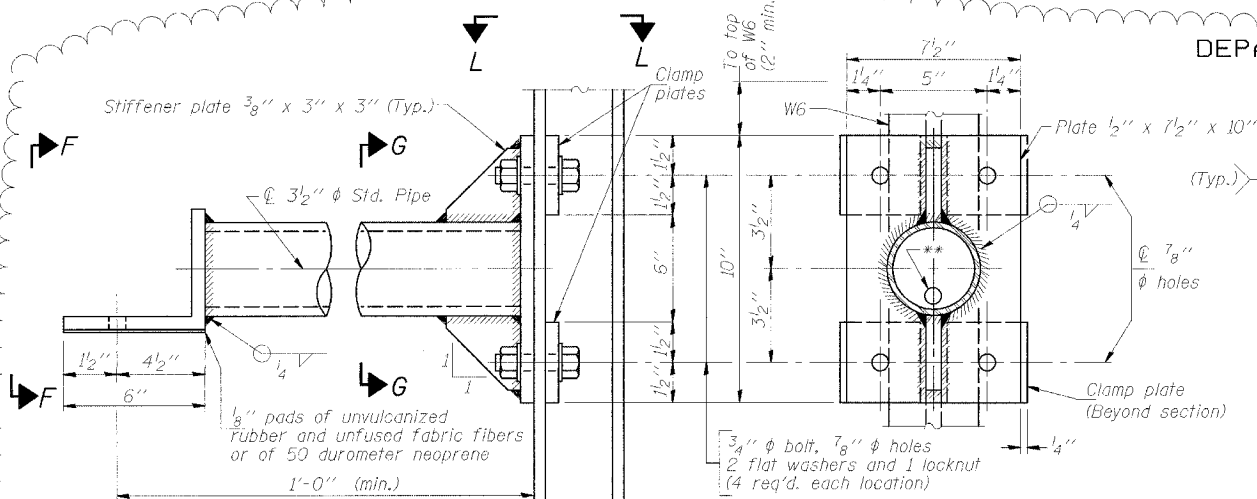
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94/90 (DAN RYAN EXPRESSWAY)
31ST STREET TO 71ST STREET
NB EXPRESS LANE RECONSTRUCTION
BRIDGE MOUNT SIGN STRUCTURES
WALKWAY AND CONNECTION DETAILS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

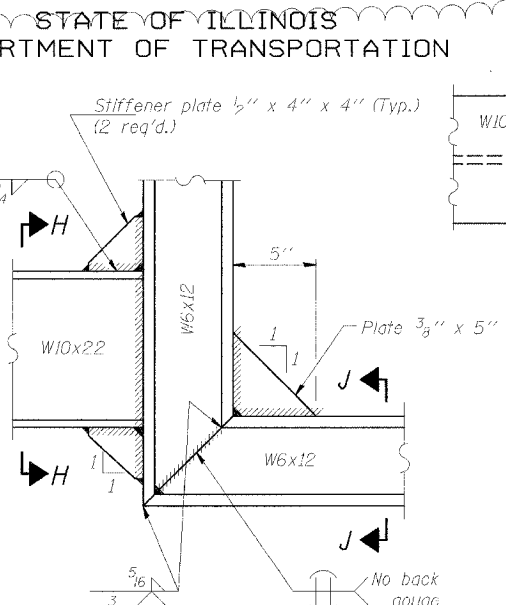
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90	*	COOK	556	303
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

62300 * (1818, ETC, 2324.6-IPR-8)

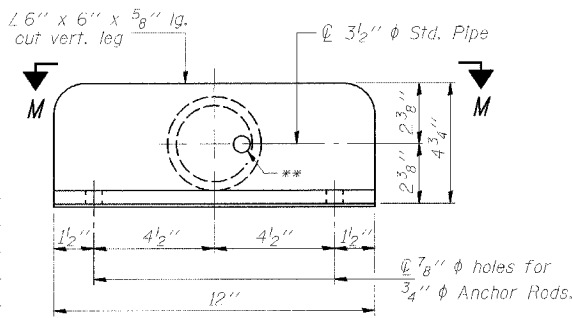
SHEETS



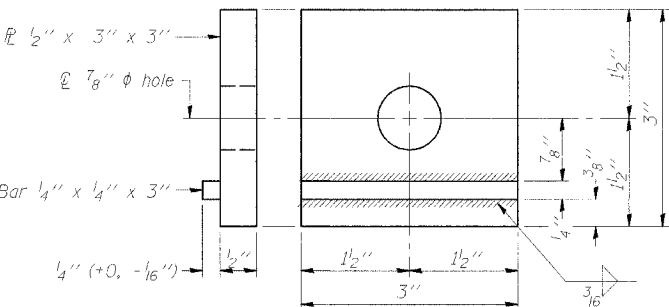
DETAIL A



SECTION G-G

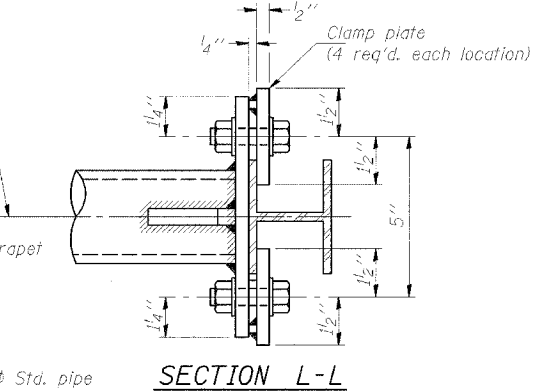


VIEW F-F



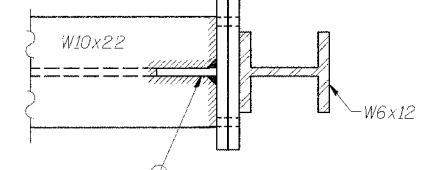
CLAMP PLATE DETAILS

DETAIL B - WELDED
W10x22 TO W6x12 CONNECTION

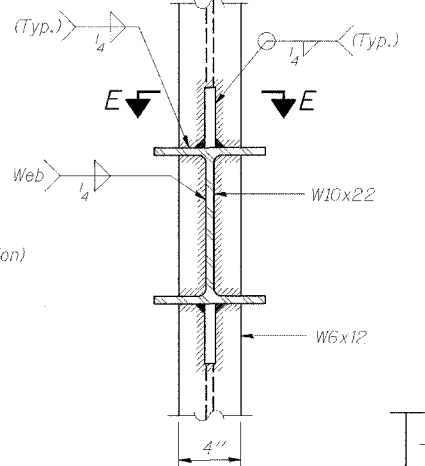


SECTION L-L

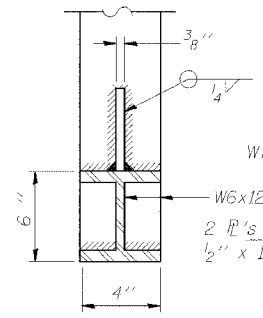
SECTION E-E



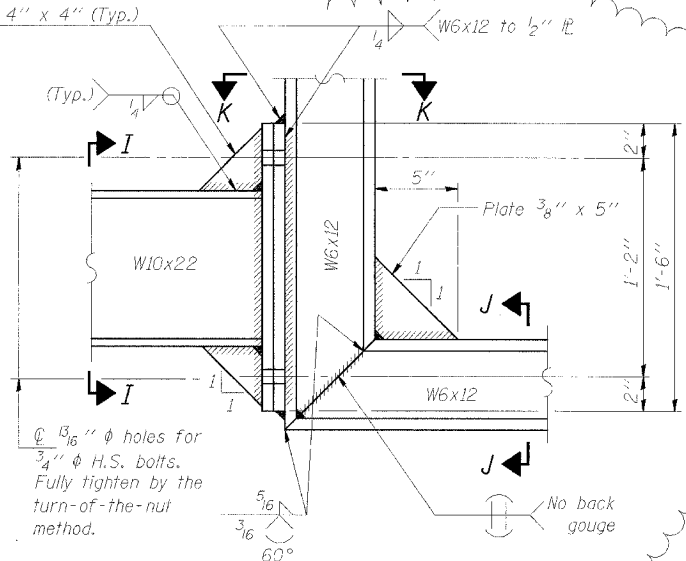
SECTION K-K



SECTION H-H

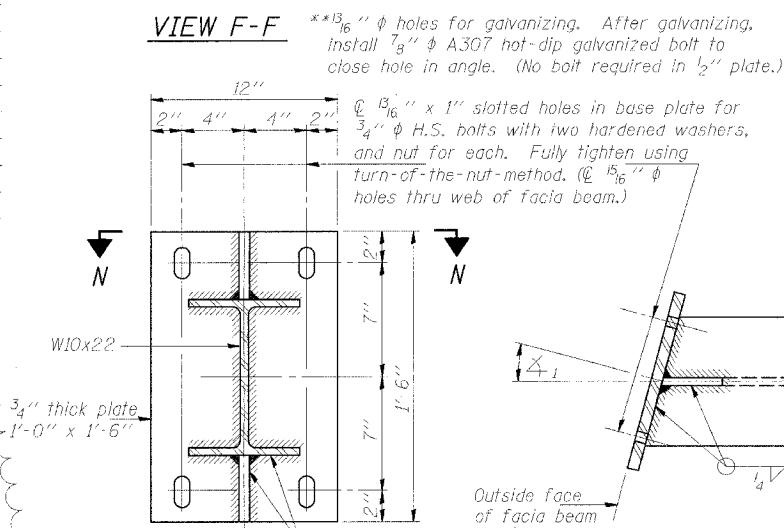


SECTION J-J



DETAIL B - ALTERNATE BOLTED
W10x22 TO W6x12 CONNECTION

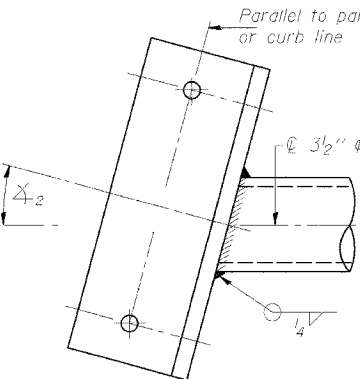
Alternate may be substituted by contractor to facilitate construction or galvanizing, especially on long struts for skewed bridges.



SECTION C-C

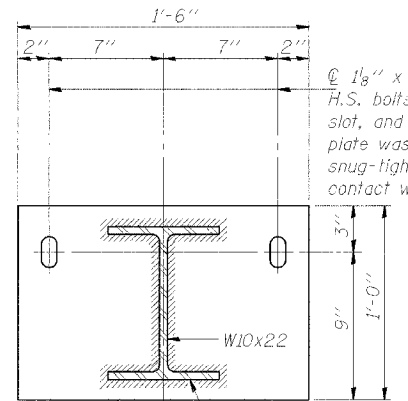
Steel beam or girder connection plate details

Note: For constant slab overhang at fascia beam, $\alpha_1 = \alpha_2 =$ sign angle. For flared beams or other special cases where $\alpha_1 \neq \alpha_2$, $\alpha_1 =$ sign angle.



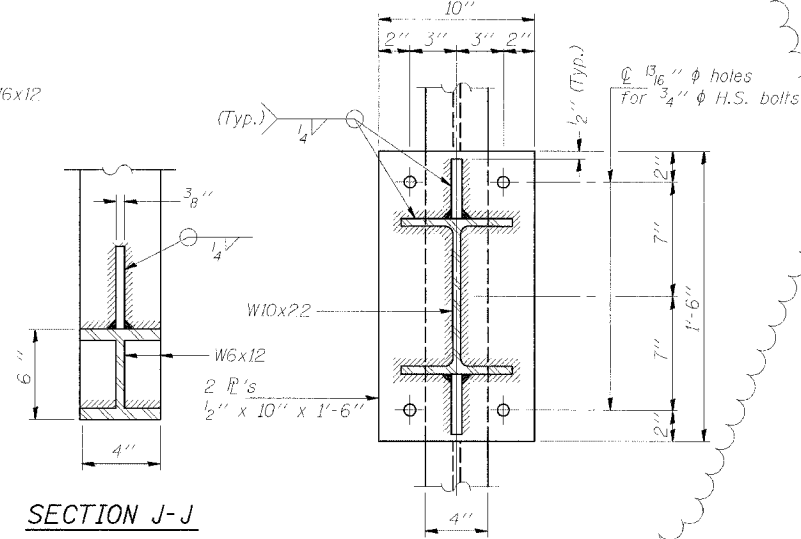
SECTION M-M

Skewed connection detail for 3/2" pipe to parapet.



SECTION D-D

Concrete beam or girder connection plate details.



SECTION I-I

DESIGNED	RTK
CHECKED	EL
DRAWN	ABW
CHECKED	PJM

EXAMINED	20
PASSED	

NUMBER	REVISION	DATE
1	ADDENDUM 1	8/21/05

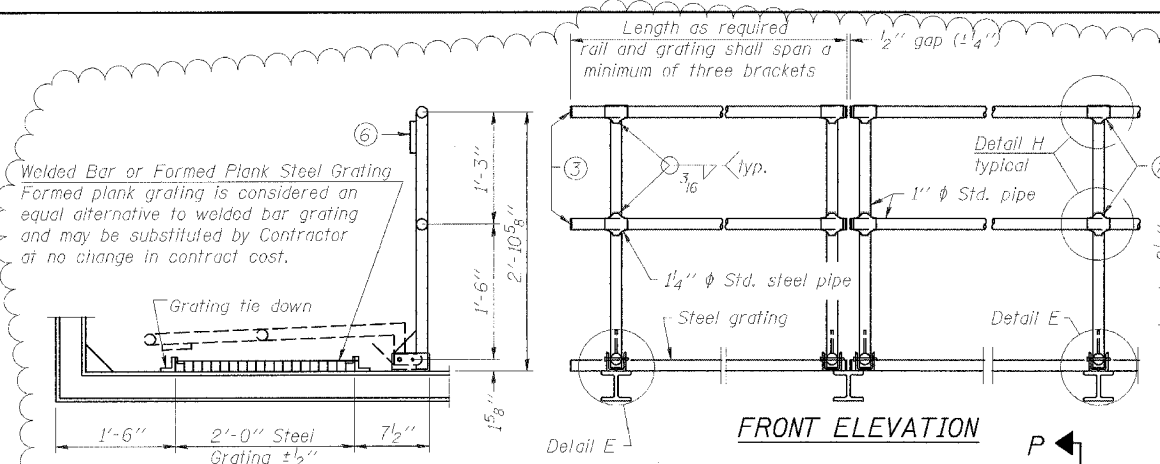
BM-3 Special 1/7/05

BRIDGE MOUNT SIGN STRUCTURES
CONNECTION DETAILS
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94/90 (DAN RYAN EXPRESSWAY)
31ST STREET TO 71ST STREET
NB EXPRESS LANE RECONSTRUCTION
BRIDGE MOUNT SIGN STRUCTURES
CONNECTION DETAILS

SGN-23

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET NO.
94/90	*	COOK	556	304
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT-				

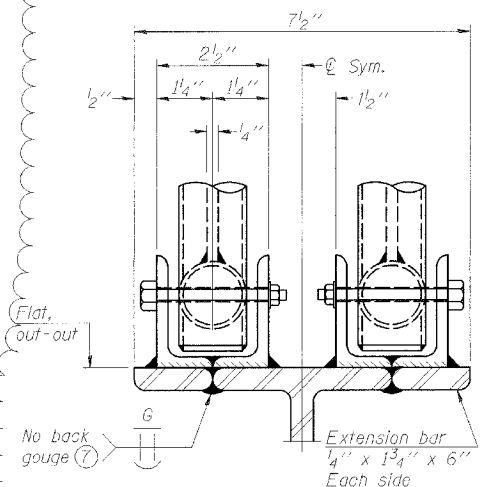


SIDE ELEVATION

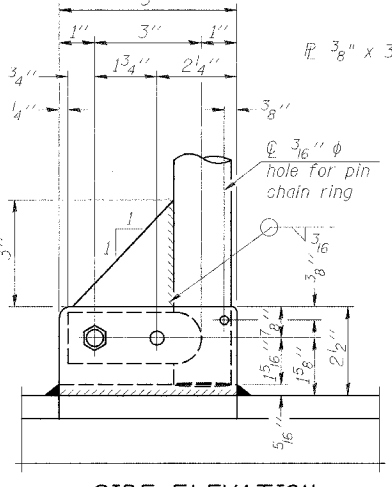
FRONT ELEVATION

DETAIL D HANDRAIL

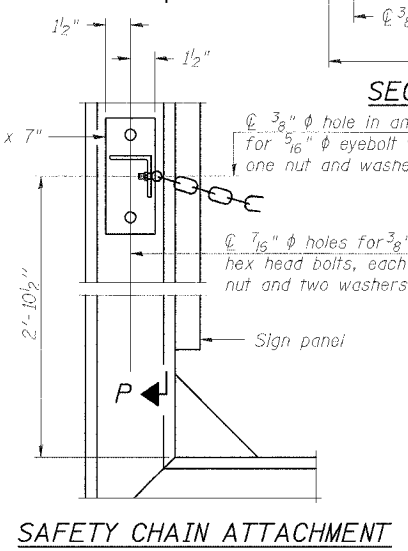
SECTION P-P



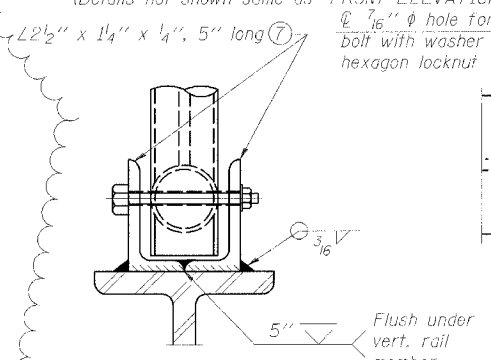
ELEVATION AT HANDRAIL JOINT



SIDE ELEVATION



SAFETY CHAIN ATTACHMENT



FRONT ELEVATION

PLAN AT SINGLE HANDRAIL HINGE

DETAIL E

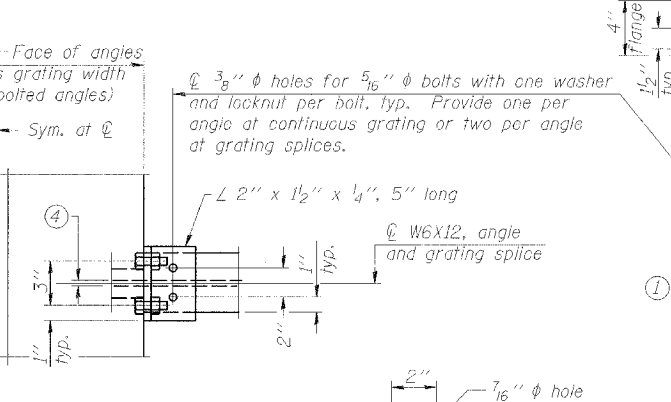
PLAN AT HANDRAIL JOINT

SECTION F-F LIGHTING FIXTURE MOUNTS

NOTES

ALTERNATE FORMED PLANK GRATING DETAILS

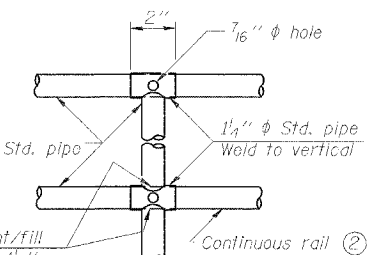
Plank Grating: nominal depth = 2 1/2" (+1/2"); perforated or expanded steel sheet with a non-skid surface (non-serrated) concentrated load capacity = 500 lbs. with 6'-0" clear span.



GRATING CONTINUOUS

AT GRATING SPLICE

VIEW W-W



DETAIL H

SAFETY CHAIN

Field drill 3/8" hole for 5/16" eye-bolt. (At approximately elevation of upper handrail pipe.)

Vertical member of walkway bracket

5/16" eye-bolts. Provide washer and hexagon locknut. (Hot dip galvanize or stainless steel.)

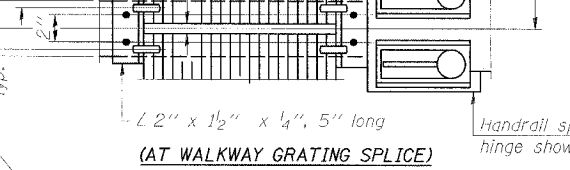
Attach bronze swivel eye snap at handrail end.

No Sign at Bracket

One (1) required for each end of each walkway.

WALKWAY GRATING CONTINUOUS

Face-to-Face of angles equals grating width plus 1/2" (For welded angles)

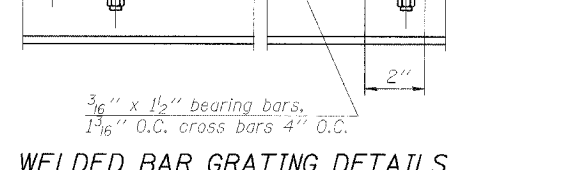


AT WALKWAY GRATING SPLICE

PLAN

WELDED BAR GRATING DETAILS

3/16" x 1 1/2" bearing bars, 1 1/16" O.C. cross bars 4" O.C.



DETAIL F

DETAIL G

BRIDGE MOUNT SIGN STRUCTURES WALKWAY DETAILS

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94/90 (DAN RYAN EXPRESSWAY)

31ST STREET TO 71ST STREET
NB EXPRESS LANE RECONSTRUCTION

OVERHEAD SIGN STRUCTURES
(BRIDGE MOUNTED)

SGN-24

NUMBER	REVISION	DATE
1	ADDENDUM 1	8/21/05

DESIGNED	RLK	20
CHECKED	EL	EXAMINED
DRAWN	ABW	PASSED
CHECKED	PJM	ENGINEER OF BRIDGES AND STRUCTURES

- Drilling holes in grating may be done in shop or field, based on Contractor's preference and subject to accurate alignment. Field drilled holes must be touched up with galvanized paint.
- Horizontal rail member shall be continuous thru 1 1/2" pipe. Provide 7/16" hole in fitting for 3/8" bolt. Field drill 7/16" hole in horizontal rail member. Provide washer and locknut for bolt. (Use 5/16" eyebolts in 1/2" holes on top rail at ends only.)
- Install standard force-fit end caps or weld 1/8" end plates with 1/8" c.f.w. and grind smooth. (All rail ends.)
- 3/8" (+1/4") gap between grating panels at splice.
- Chain to be hot dip galvanized after manufacture or stainless steel suitable for prolonged exterior exposure. Approximately 3'-6" long chain per location. Maximum sag with handrail erected = 4".
- 1/8" x 1/2" x 2" welded to handrail posts to protect locations that contact grating.
- Extrusions may be used in lieu of details shown, with approval by Engineer.
- Field cut ends of light support channels shall be free of burrs or hazardous projections and coated with zinc-rich primer or equivalent.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO.
90/94	*	COOK	556	318A	- SHEETS
FED. ROAD DIST. NO. 7			ILLINOIS FED. AID PROJECT		
(1818, ETC, 2324.6-1P) R-8					
62300					

NOTES:

The foundation dimensions shown are based on the presence of mostly cohesive soils with an average Unconfined Compressive Strength (Q_u) of at least 1.25 tsf, which must be determined by previous soil investigations at the jobsite. When other conditions are indicated, the boring data will be included in the plans and the foundation dimensions shown will be the result of site specific designs.

If the conditions encountered are different than those indicated, the Contractor shall notify the Engineer to determine if the foundation dimensions need to be modified. If dimensions "B" or "F" are revised by more than 12" by the Contractor, "as-built" plans shall be prepared and submitted to the District Bureau of Operations for future reference.

No sonotubes or decomposable forms shall be used below the lower conduit entrance. Permanent metal forms or other shielding may not be left in place below that elevation without the Engineer's written permission.

Concrete shall be placed monolithically, without construction joints.

Backfill shall be placed per Article 502 of Standard Specification and prior to erection of support column.

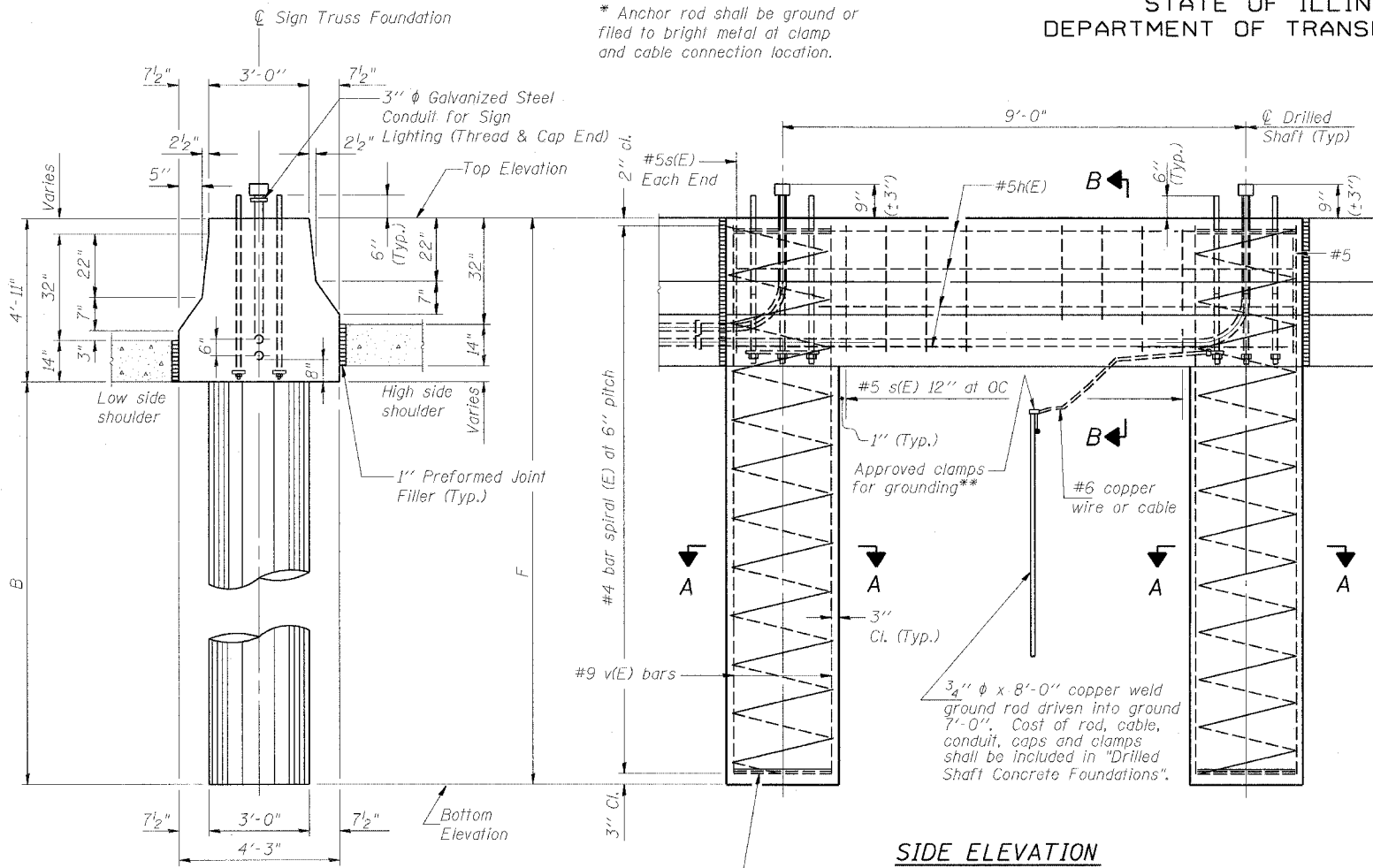
A normal surface finish followed by a Bridge Seal Sealer application will be required on concrete surfaces above the lowest elevation 6" below finished ground line. Cost included in "Drilled Shaft Concrete Foundation".

Face of median support foundation shall match dimensions of permanent barrier wall F shape.

Refer to CONTRACT 62583 for orientation and location of the conduit.

BAR LIST - EACH FOUNDATION

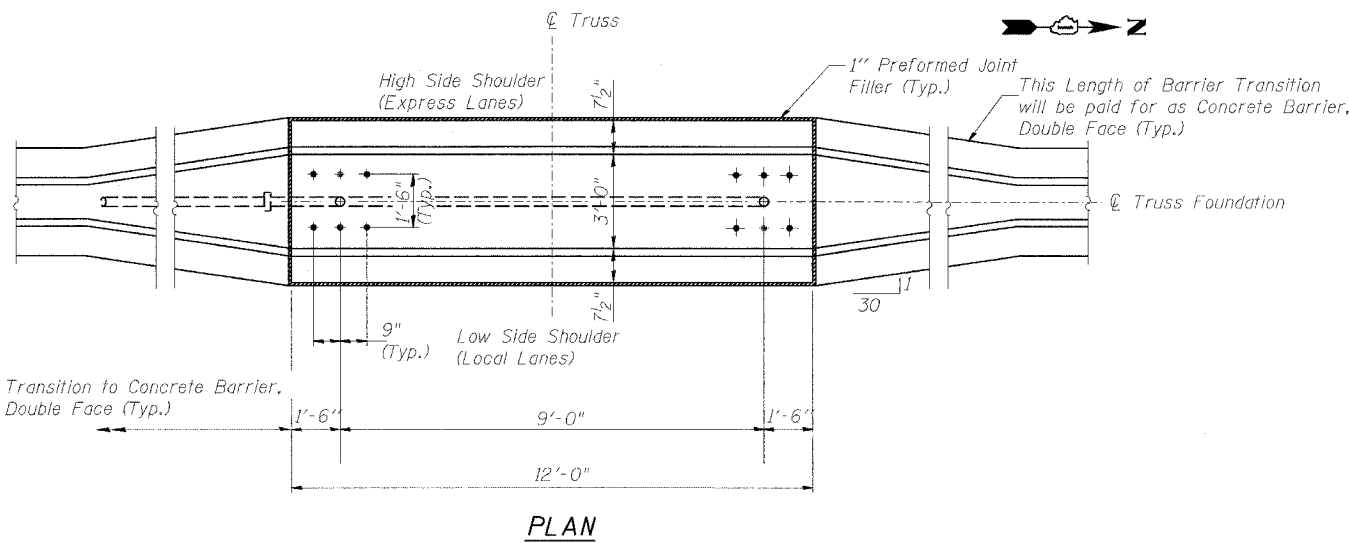
Bar	Number	Size	Length	Shape
h(E)	10	#5	11'-8"	—
s(E)	9	#5	Varies	□
v(E)	24	#9	F less 0'-5"	—
#4(E) bar spiral - see Side Elevation				



SIDE ELEVATION

Concrete Foundation poured monolithically with no construction joint.

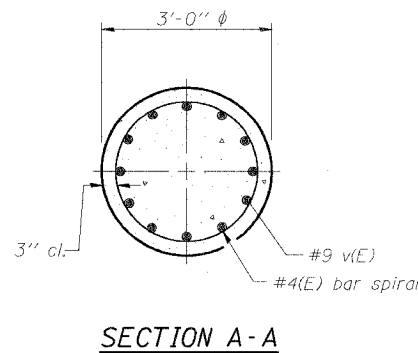
END VIEW



PLAN

LEFT FOUNDATION USES SINGLE FACE MEDIAN SUPPORT FOUNDATION

Structure Number	Station	Left Foundation				Right Foundation				Class SI Concrete (Cu. Yds.)
		Elevation Top	Elevation Bottom	B	F	Elevation Top	Elevation Bottom	B	F	
IS0161094L058.0	2461+51.44					0.95'	-21.97'	18'	22.92'	17.2



SECTION A-A

DESIGNED - SWANG	20
CHECKED - JAL	EXAMINED
DRAWN	PASSED
CHECKED	

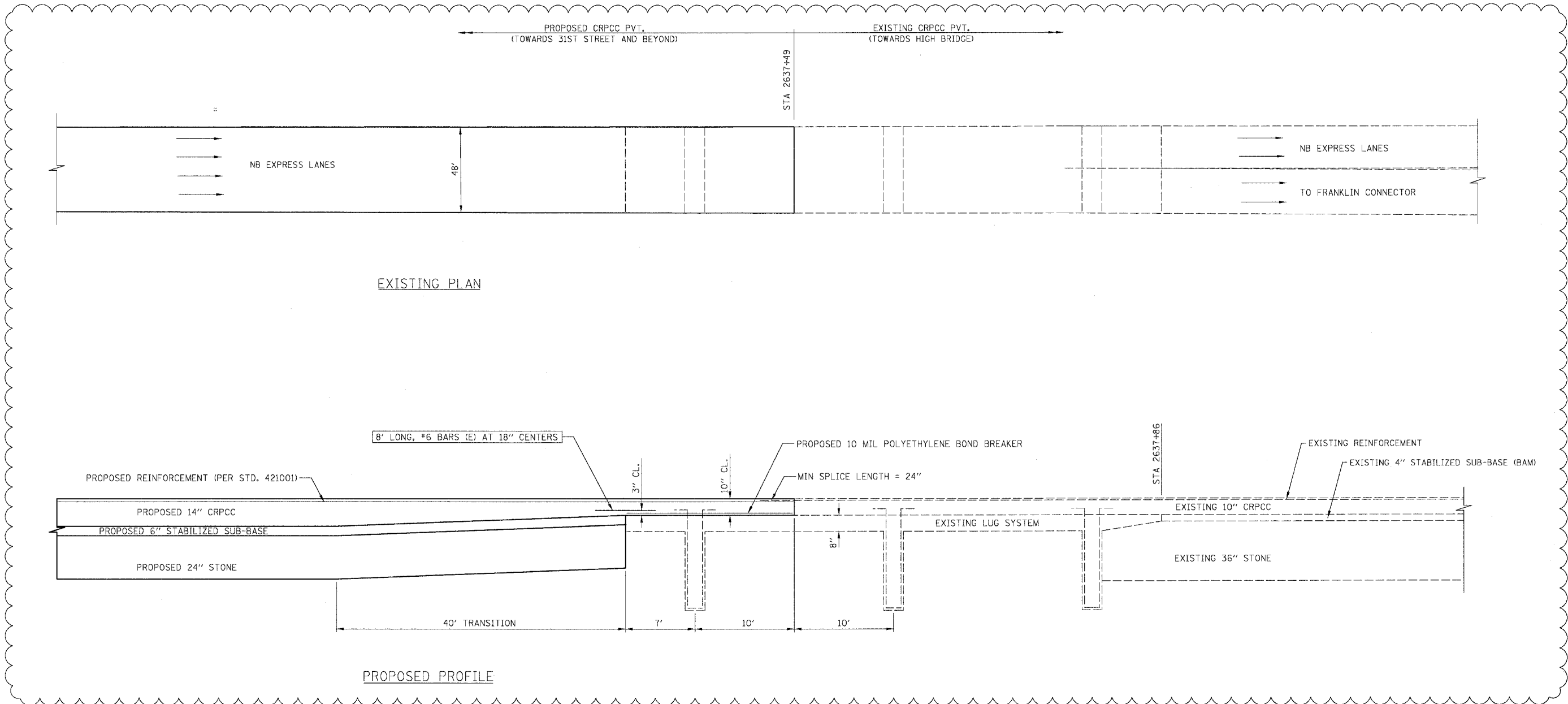
MODIFIED BY CTE ENGINEERS, INC. FROM OS4-MED

REVISIONS	
NAME	DATE
JSS	8-12-05

OVERHEAD SIGN STRUCTURES
MEDIAN SUPPORT FOUNDATION DETAILS

F.A.I. 94/90 (DAN RYAN EXPRESSWAY)
31st STREET TO 71st STREET
(NB Express Lanes)
DOUBLE FACE MEDIAN SUPPORT FOUNDATION

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90	*	COOK	556	356
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
62300	*1818, ETC, 2324, 6-1PR-8			



NOTES

- EXISTING LUG SYSTEM AND RELIEF JOINT LOCATIONS MAY DIFFER FROM STATIONING SHOWN IN PLANS. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING EXISTING LUG SYSTEMS AND RELIEF JOINTS PRIOR TO WORK BEING PERFORMED.
- THE CONTRACTOR SHALL TAKE CARE IN REMOVING THE EXISTING CRPCC PAVEMENT SO AS NOT TO DAMAGE THE PORTIONS OF THE LUG SYSTEM TO BE INCORPORATED. ANY DAMAGE TO THE EXISTING LUG SYSTEM IS TO BE REPAIRED AT THE CONTRACTOR'S EXPENSE AS DIRECTED BY THE ENGINEER.
- 8" #6 BARS ARE TO BE SUPPORTED BY PINS OR OTHER METHOD AS APPROVED BY THE ENGINEER. MUCKING IN OF THE BARS IS NOT ACCEPTABLE. COST OF THESE ADDITIONAL BARS WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE PRICE FOR CRPCC PAVEMENT 14".
- TRANSITION AREA FROM 10" TO 14" CRPCC AND 10" CRPCC WILL BE PAID AS CRPCC PAVEMENT 14".

BOWMAN, BARRETT & ASSOCIATES INC.
CONSULTING ENGINEERS
Chicago, Illinois
312.228.0100
www.bbainc.com

REVISIONS	
NAME	DATE
ADDENDUM 1	8/12/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94/90 (DAN RYAN EXPRESSWAY)
31ST STREET TO 71ST STREET
NB EXPRESS LANE RECONSTRUCTION
LUG SYSTEM INTERFACE
AT 31ST STREET

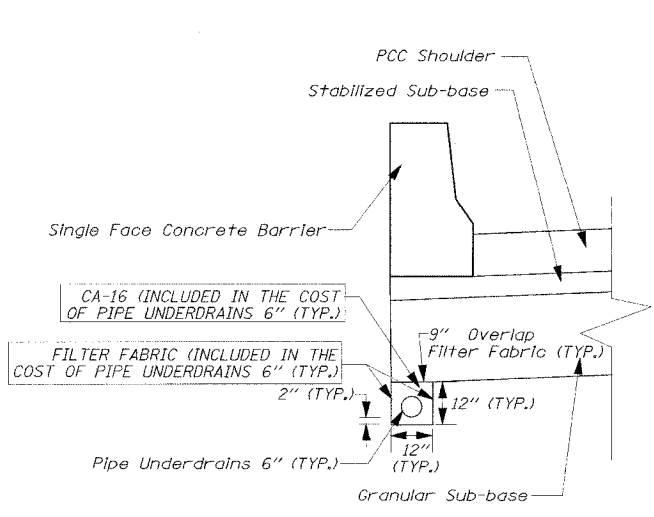
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DATE: 07/07/05

DRAWN BY: RR
CHECKED BY: JDC

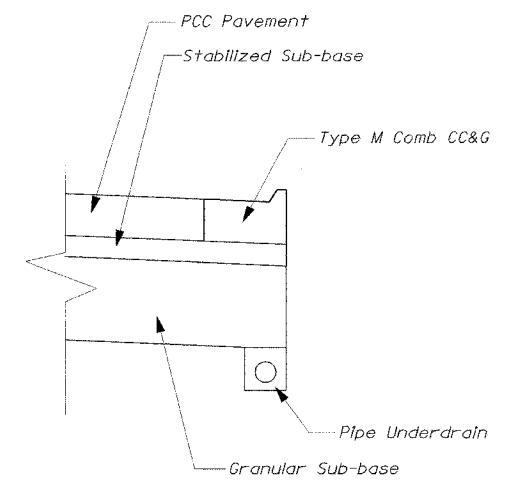
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90		COOK	556	360
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
62300	• (1818, ETC, 2324.6-1PIR-8			

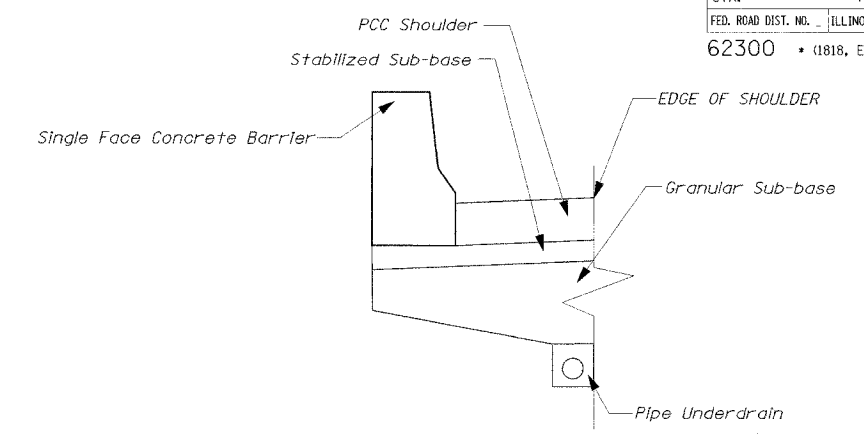
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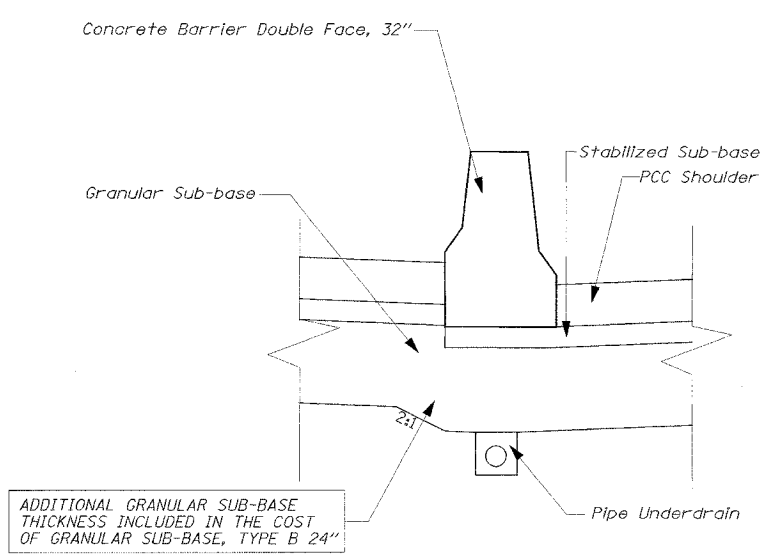
PIPE UNDERDRAINS 6" UNDER SINGLE FACE BARRIER WALL



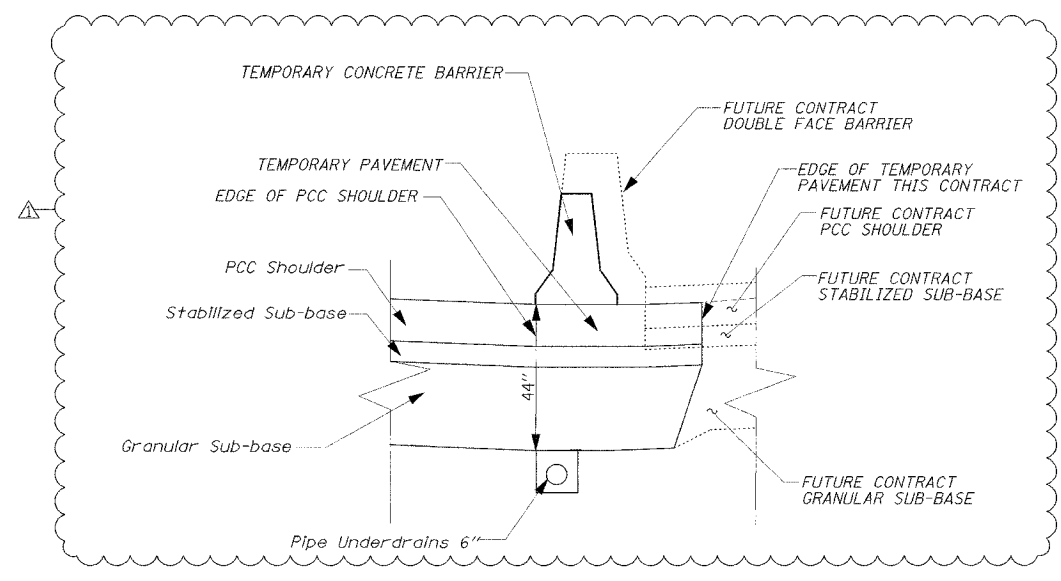
PIPE UNDERDRAINS 6" - UNDER TYPE M COMBINATION CONCRETE CURB & GUTTER



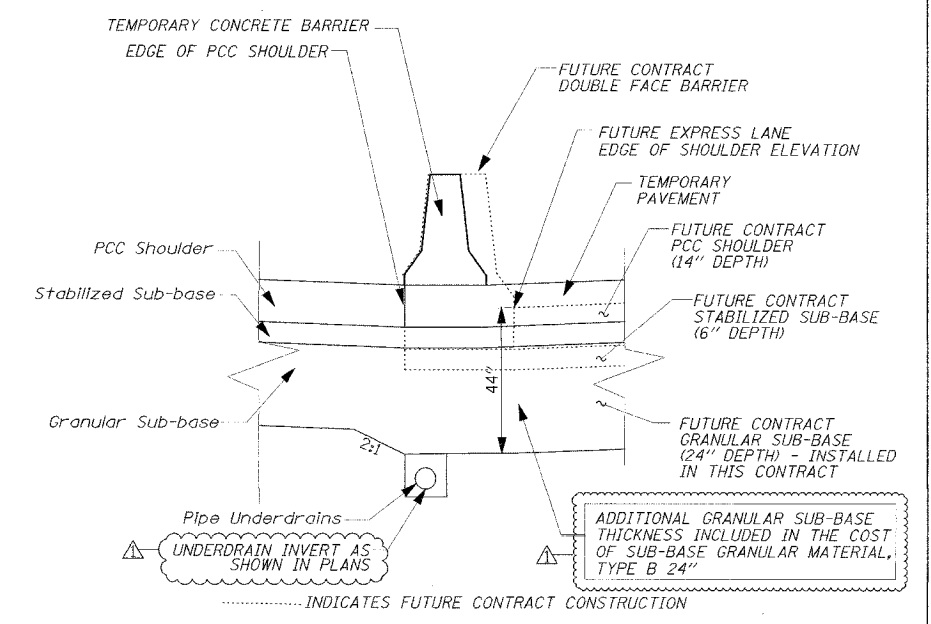
PIPE UNDERDRAINS 6" UNDER SHOULDER ADJACENT TO CTA



PIPE UNDERDRAINS 6" UNDER DOUBLE FACE BARRIER WALL



PIPE UNDERDRAINS 6" UNDER TEMPORARY CONCRETE BARRIER WALL SCENARIO A - FUTURE LOCAL LANES HIGHER THAN EXPRESS LANES



PIPE UNDERDRAINS 6" UNDER TEMPORARY CONCRETE BARRIER WALL SCENARIO B - FUTURE LOCAL LANES LOWER THAN EXPRESS LANES

NOTE:
 GRANULAR SUB-BASE REFERENCES SUB-BASE GRANULAR MATERIAL, TYPE B 24" SOUTH OF STA. 1530+00, AND A COMBINATION OF SUB-BASE GRANULAR MATERIAL TYPE B 12" AND EXCAVATE AND PLACE EXISTING GRANULAR MATERIAL NORTH OF STA. 1530+00 (SEE TYPICAL). THE NOTE "ADDITIONAL GRANULAR SUB-BASE THICKNESS INCLUDED IN THE COST OF SUB-BASE GRANULAR MATERIAL, TYPE B 24" IS ONLY APPLICABLE SOUTH OF STA. 1530+00, NORTH OF STA. 1530+00, ADDITIONAL SUB-BASE THICKNESS WILL BE PROVIDED UNDER THE ITEM EXCAVATE AND PLACE EXISTING GRANULAR MATERIAL.

Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM

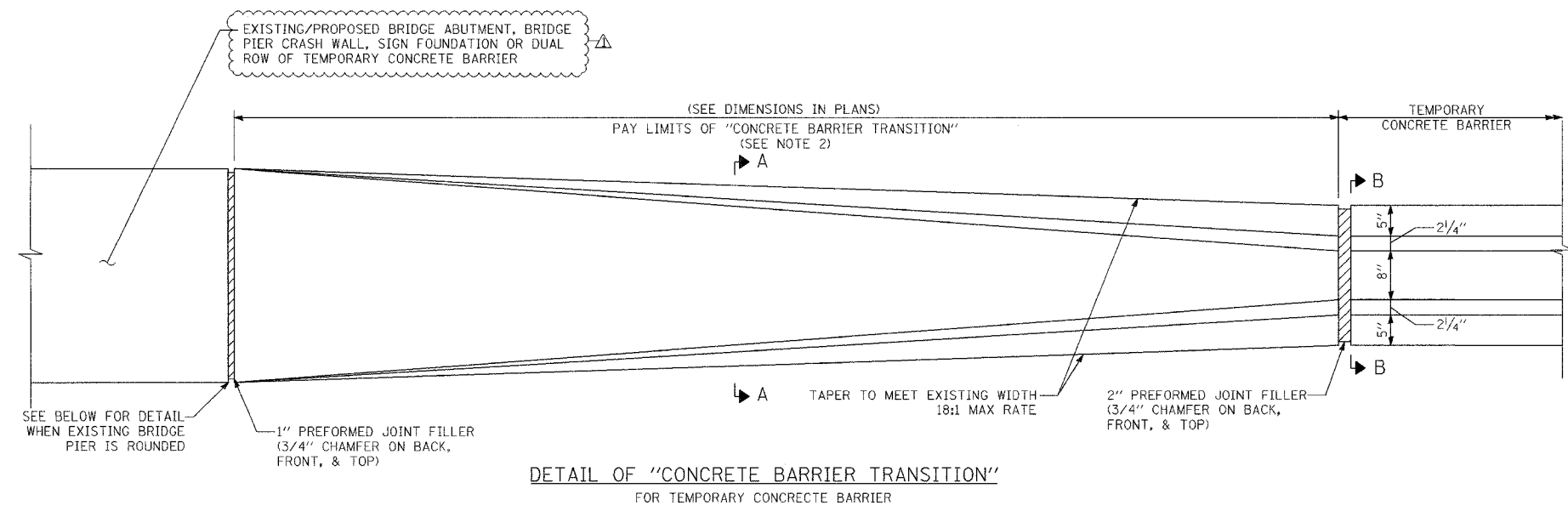
REVISIONS	
NAME	DATE
ADDENDUM 1	8/12/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94/90 (DAN RYAN EXPRESSWAY)
 31ST STREET TO 71ST STREET
 NB EXPRESS LANE RECONSTRUCTION
 CIVIL DETAILS
 PIPE UNDERDRAINS 6"
 SCALE: NONE
 DATE: 7/7/05
 DRAWN BY: TAI
 CHECKED BY: PJM

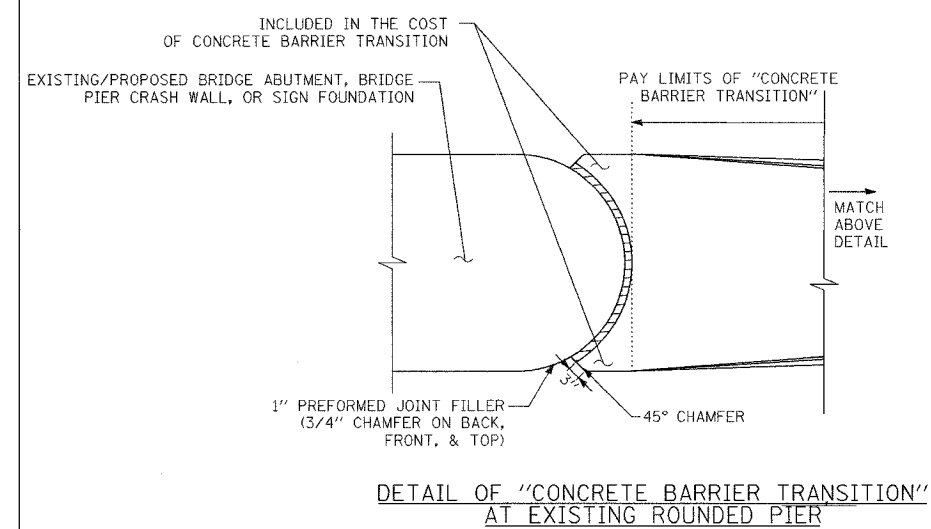
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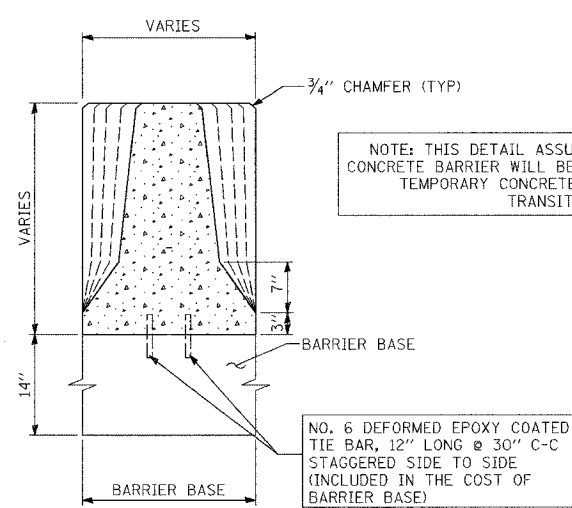
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90 *		COOK	556	368
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
62300	* (1818, ETC, 2324.6-IPR-8			



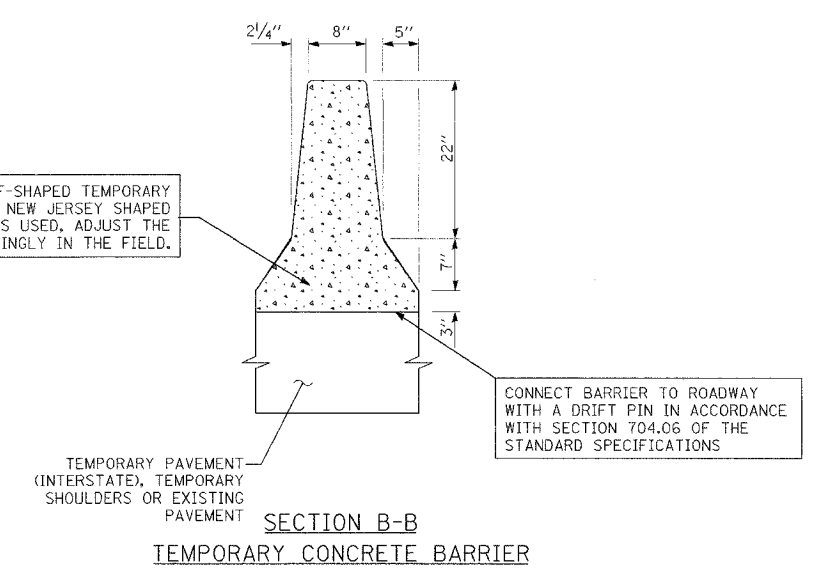
DETAIL OF "CONCRETE BARRIER TRANSITION"
FOR TEMPORARY CONCRETE BARRIER



DETAIL OF "CONCRETE BARRIER TRANSITION"
AT EXISTING ROUNDED PIER



SECTION A-A
CONCRETE BARRIER TRANSITION



SECTION B-B
TEMPORARY CONCRETE BARRIER

- NOTE:
1. ALL TRANSITIONS PAID FOR AS CONCRETE BARRIER TRANSITION.
2. LENGTHS PROVIDED IN THE PLANS ARE APPROXIMATE. THE CONTRACTOR MUST ADJUST THE LENGTH OF CONCRETE BARRIER TRANSITION AS NECESSARY TO MEET TEMPORARY CONCRETE BARRIER SEGMENTS USED.

REVISIONS	
NAME	DATE
ADDENDUM 1	8/12/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94/90 (DAN RYAN EXPRESSWAY)
31ST STREET TO 71ST STREET
NB EXPRESS LANE RECONSTRUCTION
CONCRETE BARRIER TRANSITION
(SHEET 3 OF 3)
SCALE: NONE
DATE: 7/7/05
DRAWN BY: TAJ
CHECKED BY: PJM

Edwards AND Kelcey
ONE NORTH FRANKLIN
CHICAGO, IL 60606
PHONE: (312) 251-3000
FAX: (312) 251-3015
WEB: WWW.EKCORP.COM

CDT-21
03/16/05 PJM

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	*	COOK	556	369
STA.	TO STA.			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			62300	
* (1818, ETC, 2324.6-1)PR-8				

CTA BARRIER WALL LOCATION AND MODIFICATION TABLE

STATION	CONTRACT 62300 - NORTH BOUND EXPRESS LANES						CONTRACT 62302 - SOUTH BOUND EXPRESS LANES							
	Soldier Piles			Proposed T / Shoulder At or Above Existing		Proposed T / Shoulder Below Existing		Soldier Piles			Proposed T / Shoulder At or Above Existing		Proposed T / Shoulder Below Existing	
	Size	Length	Max. Spacing	Location	Modification	Location	Modification	Size	Length	Max. Spacing	Location	Modification	Location	Modification
35th St.	W12x120	30'	5'-0"	Sta. 2609+61 to 2625+07	6" Facing	-----	-----	W12x120	30'	5'-0"	Sta. 1609+39 to 1624+89	6" Facing	-----	-----
47th St.	W12x106	20'	5'-0"	-----	-----	Sta. 2529+46 to 2540+38	1'-4 1/2" Facing	W12x72	20'	6'-6"	Sta. 1529+38 to 1537+94	6" Facing	-----	-----
55th St.	W12x106	20'	8'-0"	-----	-----	Sta. 2476+01 to 2484+81	1'-4 1/2" Facing	W12x106	20'	8'-0"	-----	-----	Sta. 1475+84 to 1484+69	1'-4 1/2" Facing
69th St.	W12x72	20'	8'-0"	Sta. 2372+94 to 2379+56	②	-----	-----	W12x106	20'	8'-0"	Sta. 1372+75 to 1374+50	①	Sta. 1374+50 to 1378+53	①

- ① Replace Exist. 32" Barrier with Single Face Barrier, 42" Special
- ② Reface Existing CTA Barrier Wall with Vertical Facing

GENERAL NOTES:

1. Utility locations shown on these plans may not be correct or complete. The Contractor shall coordinate construction activities with the respective utilities of the city of Chicago. Before starting any excavation, the Contractor shall call "CUAN" (Chicago Utility Alert Network) at 312-744-7000 for field locations of buried electric, telephone, and gas facilities (48 hour notification is required). Contact the department of water management permit section at (312) 747-7893 for water and sewer locations.
2. In locations where the main sewers, drainage structures, public, and private drain connections are disturbed or damaged during construction by the Contractor, it will be the Contractor's responsibility to restore and replace the damaged facilities at his/her own expense to the satisfaction of the department of water management. The sewer flows must be maintained at all times.
3. In locations where pilings/drilled shafts are being proposed in the vicinity of the existing sewer facilities, a licensed Structural Engineer shall approve construction methods of support in order to maintain the structural integrity of the city's sewer system.
4. The Location of various items such as pavement, drainage structures, barrier walls and sign structure foundations built in contracts under construction during the preparation of these plans is based on the published contract plan drawings available during design. The Contractor must field verify limits, locations and elevations of these previously constructed items.
5. The Contractor shall use care in grading or excavating near any and all existing items which will not be removed. Any damage done to existing items by the Contractor shall be repaired by the Contractor at the Contractor's own expense.
6. The Contractor shall take all necessary safety precautions to protect and provide access to abutting CTA property, utilities and vehicular traffic.
7. It shall be the Contractor's responsibility to ensure that no gap remains between proposed CTA fencing or where proposed CTA fencing terminates and existing CTA fence remains in place.
8. Hammer driving of piles will not be allowed. All piles must be drilled or vibrated into place.
9. Plan dimensions and details relating to existing structures have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation or a change in the scope of work. However the Contractor will be paid for the quantity actually furnished at the unit price for the work.
10. All construction joints shall be bonded.
11. All exposed concrete edges shall have a 3/4" by 45° chamfer except where shown otherwise. Chamfer on vertical edges shall be continued a minimum of 1 foot below the finished ground line.
12. A protective coat shall be applied within the limits shown on the plans according to Section 503.19 of the Standard Specifications.
13. Reinforcement bars shall conform to the requirements of AASHTO M31 or M322 Grade 60.
14. Reinforcement bars designated "(E)" shall be epoxy coated.

15. For Existing Barrier Wall Modifications for Proposed T / Shoulder Below Existing, See Sheets B2 and B3.
16. For Existing Barrier Wall Modifications for Proposed T / Shoulder At or Above Existing, See Sheet B4 and B5.
17. For Existing Barrier Wall Modifications for 69th St. Station (SB) See Sheet B6.
18. For Existing Barrier Wall Modifications for 69th St. Station (NB) See Sheet B7.
19. For Refaced Barrier to Pier, Transition Details (Taper), Proposed T / Shoulder Below Existing, See Sheet B8.
20. For Refaced Barrier to Pier, Transition Details (No Taper), Proposed T / Shoulder Below Existing, See Sheet B9.
21. For Refaced Barrier to Pier, Transition Details (Taper), Proposed T / Shoulder At or Above Existing, See Sheet B10.
22. For Refaced Barrier to Pier, Transition Details (No Taper), Proposed T / Shoulder At or Above Existing, See Sheet B11.
23. For New 42" Barrier to Vert. Refaced Barrier, Transition Details (Taper), Proposed T / Shoulder At or Above Existing, See Sheet B12.
24. For Vert. Refaced Barrier to Pier, Transition Details (No Taper), Proposed T / Shoulder At or Above Existing, See Sheet B13.
25. For Refaced Barrier to New 42" Barrier, Transition Details, Proposed T / Shoulder Below Existing, See Sheet B14.
26. For Refaced Barrier to New 42" Barrier, Transition Details, Proposed T / Shoulder At or Above Existing, See Sheet B15.
27. For New 42" Barrier to Pier, Transition Details, Proposed T / Shoulder Below or Above Existing, See Sheet B16.
28. For Refaced Barrier to New Sign Structure, Transition Plan, Elevation and Details, Proposed T / Shoulder Below Existing, See Sheets B17 and B18.
29. If the Contractor chooses to alter the temporary soldier pile and lagging system shown in the Plans, a design submittal including plan details and calculations will be required for review and acceptance by the Engineer.

DESIGN STRESSES:

FIELD UNITS
f'c = 3,500 psi
fy = 60,000 psi (Reinf.)

CTE | AECOM

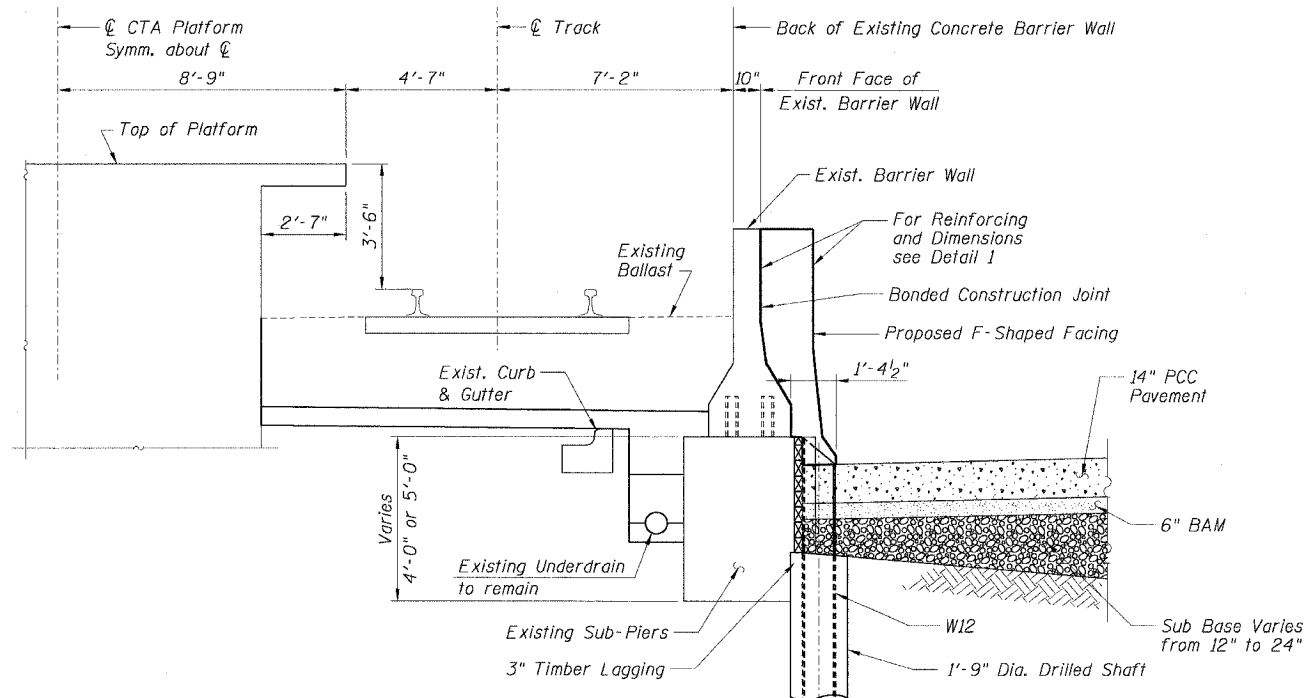
REVISIONS	
NAME	DATE
MJP	8-12-05

Sheet B1 of 16
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
71ST STREET TO 31ST STREET (NB EXPRESS LANES)
**EXISTING BARRIER WALL
AT CTA STATIONS
LOCATION AND MODIFICATION TABLE**
SCALE: None
DATE: July 7, 2005
DRAWN BY: KWT
CHECKED BY: RDP

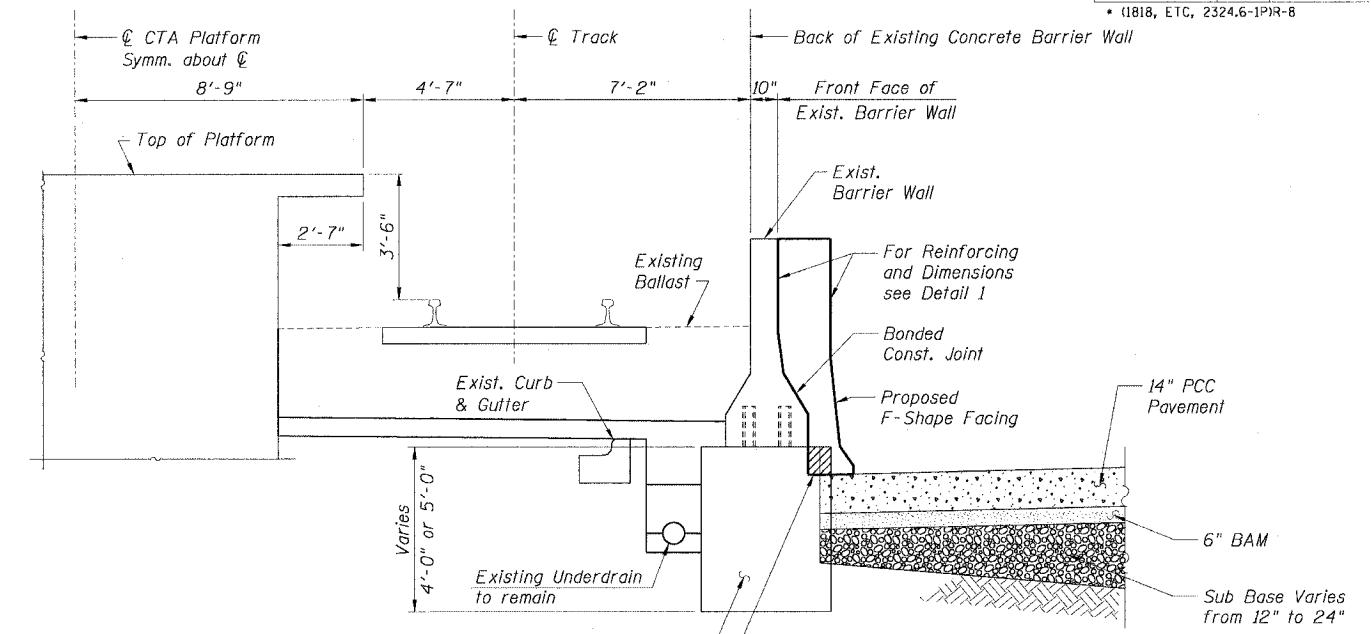
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEET SHEETS	NO.
90/94		COOK	556	370
STA.	TO STA.			
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
			* (1818, ETC, 2324.6-1PR-8	
				62300



TYPICAL SECTION
BETWEEN SUB-PIERS

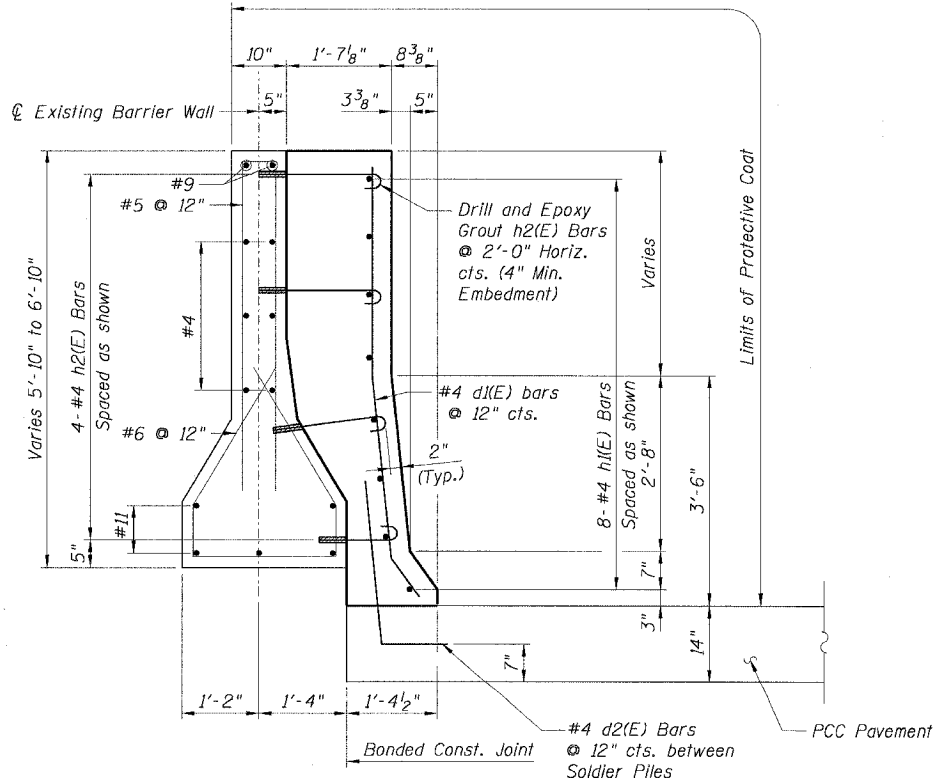


TYPICAL SECTION
AT SUB-PIER

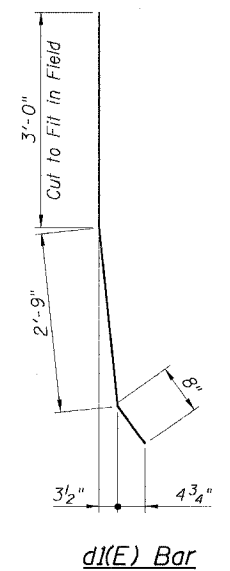
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d1(E)		#4	6'-5"	—
d2(E)		#4	3'-6"	—
h1(E)		#4	—	—
h2(E)		#4	2'-4"	—
Item	Unit	Quantity		
Furnishing Soldier Piles, W12x...	Foot			
Drilling and Setting Soldier Piles (in Soil)	Cu. Ft.			
Untreated Timber Lagging	Sq. Ft.			
Concrete Removal	Cu. Yd.			
Reinforcement Bars, Epoxy Coated	Pound			
Concrete Superstructure	Cu. Yd.			
Protective Coat	Sq. Yd.			

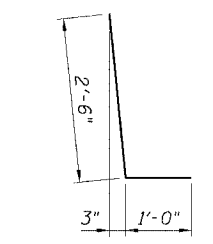
Remove Top Portion of Existing Concrete Sub-Pier. See Plan View



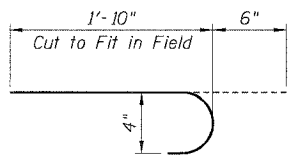
DETAIL 1



d1(E) Bar



d2(E) Bar



h2(E) Bar

LEGEND:

- Concrete Removal
- PCC Pavement
- BAM
- Sub Base
- Earth

NOTES:

- Drilling and grouting of #4 h2(E) bars shall be in accordance with Section 584 of the Standard Specifications.
- The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h2(E) bars shall be included in the cost per cubic yard for Concrete Superstructure.
- #4 d2(E) bars shall be seated in their final position prior to the placement of the concrete shoulder. Bars shall be not mucked into place.
- Provide joints in facing to match those in the existing barrier wall. See Sheet B7 for details.
- Minimum lap length for #4 bars shall be 1'-8".
- For Typical Plan, see Sheet B3.
- For Existing Concrete Repair Details, see Sheet B3.
- For Facing Details at Existing Light Pole Foundation, see Sheet B5.

Sheet B2 of 16

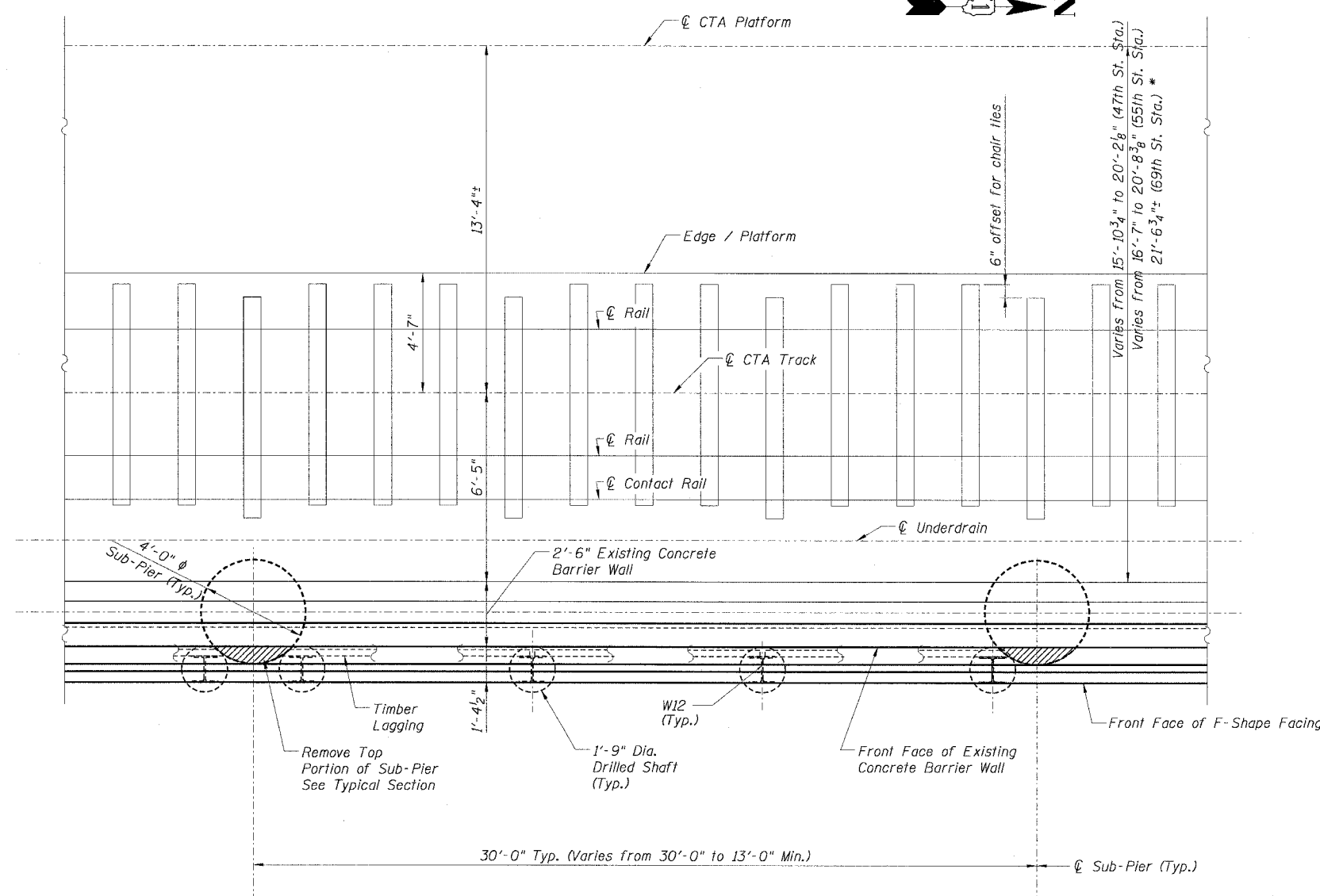
REVISIONS	
NAME	DATE
MJP	8-12-05

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
71ST STREET TO 31ST STREET (NB EXPRESS LANES)
EXISTING BARRIER WALL MODIFICATIONS
AT CTA STATIONS
PROPOSED T/S SHOULDER BELOW EXISTING
TYPICAL SECTIONS & DETAILS
SCALE: None DRAWN BY: KWT
DATE: July 7, 2005 CHECKED BY: RDP

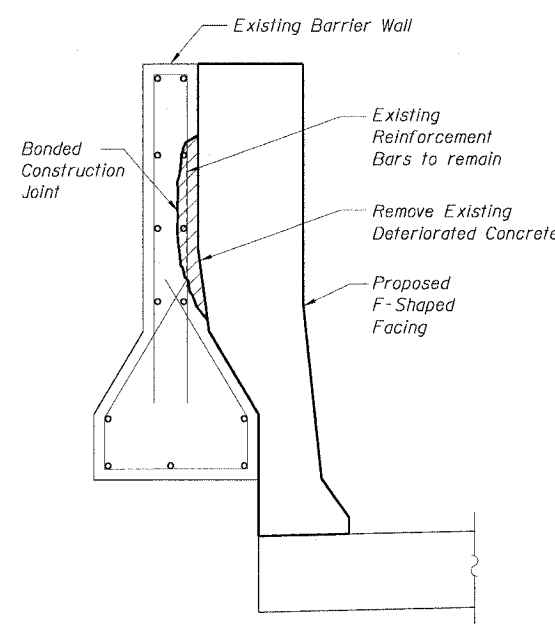
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94		COOK	556	371
STA.	TO STA.			
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT	62300	
(1818, ETC, 2324.6-IPR-8				



PLAN - (NORTH BOUND SHOWN)
(SOUTH BOUND SIMILAR)



CONCRETE REPAIR DETAIL

NOTES:

- The Contractor shall remove deteriorated concrete to sound concrete at all locations designated by the Engineer in accordance with Section 501.03 of the Standard Specifications. Undercut exposed reinforcement bars by 3/4" to provide clearance between existing reinforcement bars and the remaining concrete.
- Exposed existing reinforcement bars shall remain in place and be blast cleaned to gray metal. Reinforcement bars that have been cut or have lost 25% or more of their original cross sectional area shall be supplemented by new epoxy coated reinforcement bars.
- The resulting voids shall be blast cleaned before applying a bonding agent and filled with Concrete Superstructure.
- The cost of removing deteriorated concrete, supplementing and cleaning existing reinforcement bars, blast cleaning and furnishing and placing a bonding agent and concrete shall be included in the cost per cubic yard for Concrete Superstructure.
- For Facing Details and Sections see Sheet B2.

LEGEND:

Concrete Removal

CTE | AECOM

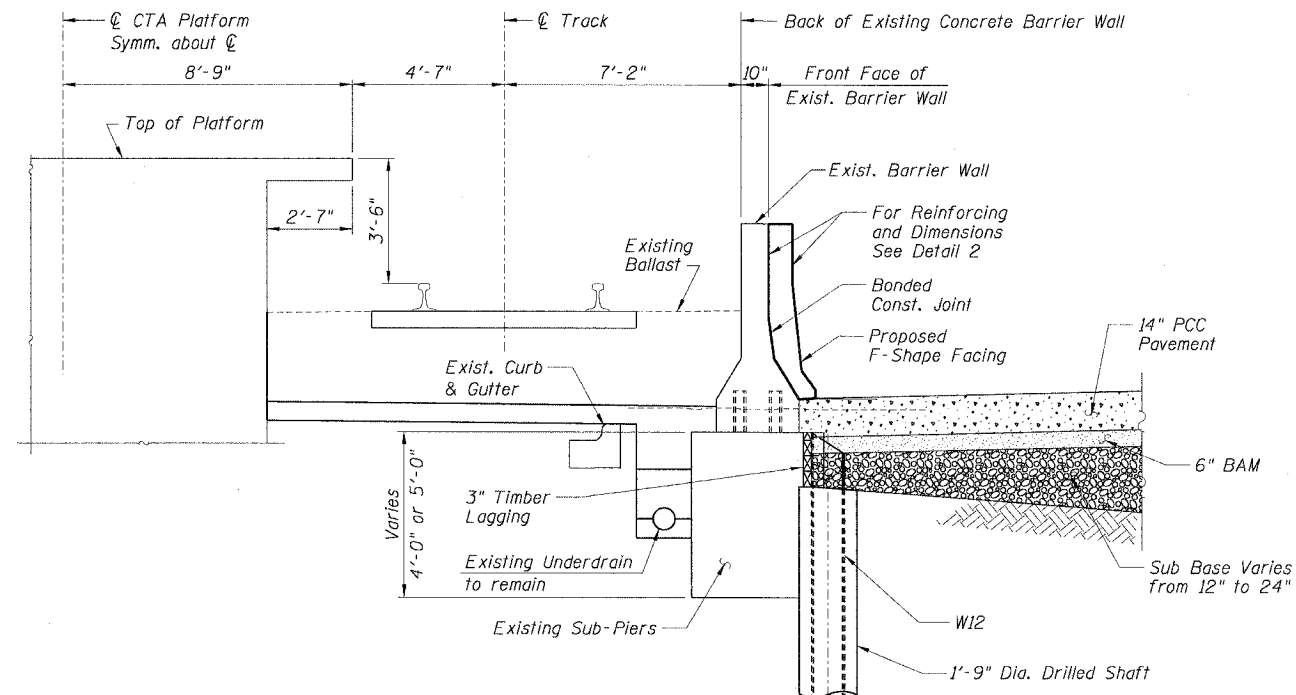
REVISIONS	
NAME	DATE
MJP	8-12-05

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
71ST STREET TO 31ST STREET (NB EXPRESS LANES)
**EXISTING BARRIER WALL MODIFICATIONS
AT CTA STATIONS
PROPOSED T/S SHOULDER BELOW EXISTING
TYPICAL PLAN**
SCALE: None DRAWN BY: KWT
DATE: July 7, 2005 CHECKED BY: RDP

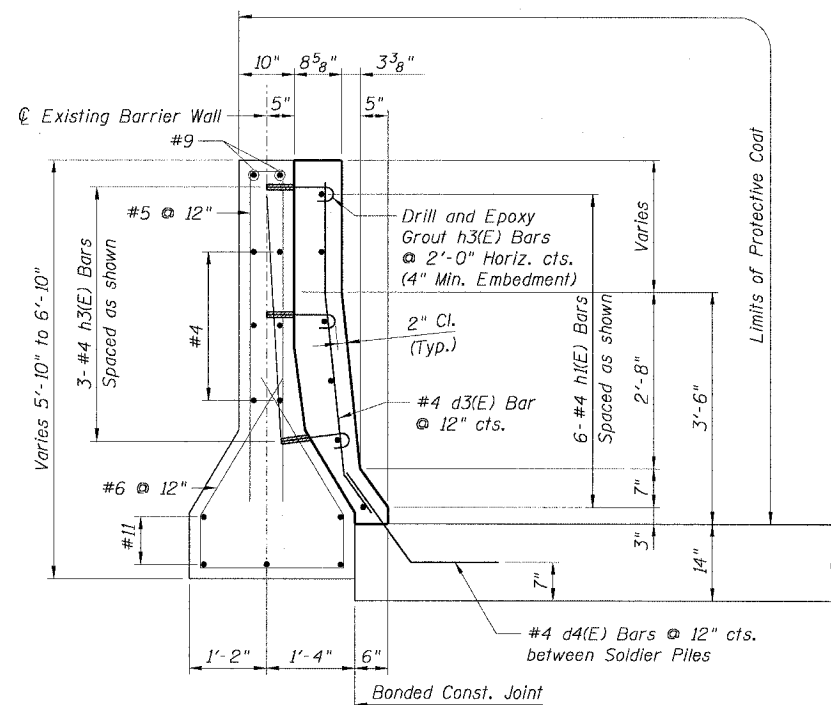
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

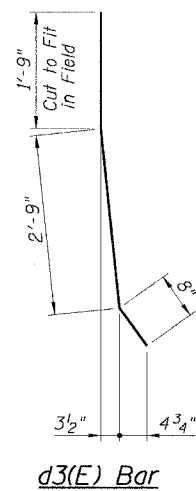
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	*	COOK	556	372
STA.	TO STA.			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
* (181B, ETC, 2324.6-1)PR-8			62300	



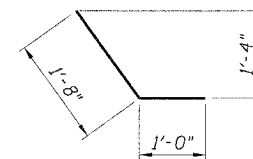
TYPICAL SECTION
BETWEEN SUB-PIERS



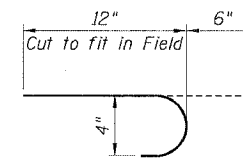
DETAIL 2



d3(E) Bar



d4(E) Bar



h3(E) Bar

LEGEND:

	PCC Pavement
	BAM
	Sub Base
	Earth

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d3(E)		#4	5'-2"	
d4(E)		#4	2'-8"	
h1(E)		#4		
h3(E)		#4	1'-6"	
Item	Unit	Quantity		
Furnishing Soldier Piles, W12x...	Foot			
Drilling and Setting				
Soldier Piles (in Soil)	Cu. Ft.			
Untreated Timber Lagging	Sq. Ft.			
Reinforcement Bars,				
Epoxy Coated	Pound			
Concrete Superstructure	Cu. Yd.			
Protective Coat	Sq. Yd.			

NOTES:

1. Drilling and grouting of #4 h3(E) bars shall be in accordance with Section 584 of the Standard Specifications.
2. The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h3(E) bars shall be included in the cost per cubic yard for Concrete Superstructure.
3. #4 d4(E) bars shall be seated in their final position prior to the placement of the concrete shoulder. Bars shall be not mucked into place.
4. Provide joints in facing to match those in the existing barrier. See Sheet B7 for details.
5. Minimum lap length for #4 bars shall be 1'-8".
6. For Typical Plan see Sheet B5.
7. For Existing Concrete Repair Details see Sheet B3.
8. For Facing Details at Existing Light Pole Foundation see Sheet B5.

Sheet B4 of 16

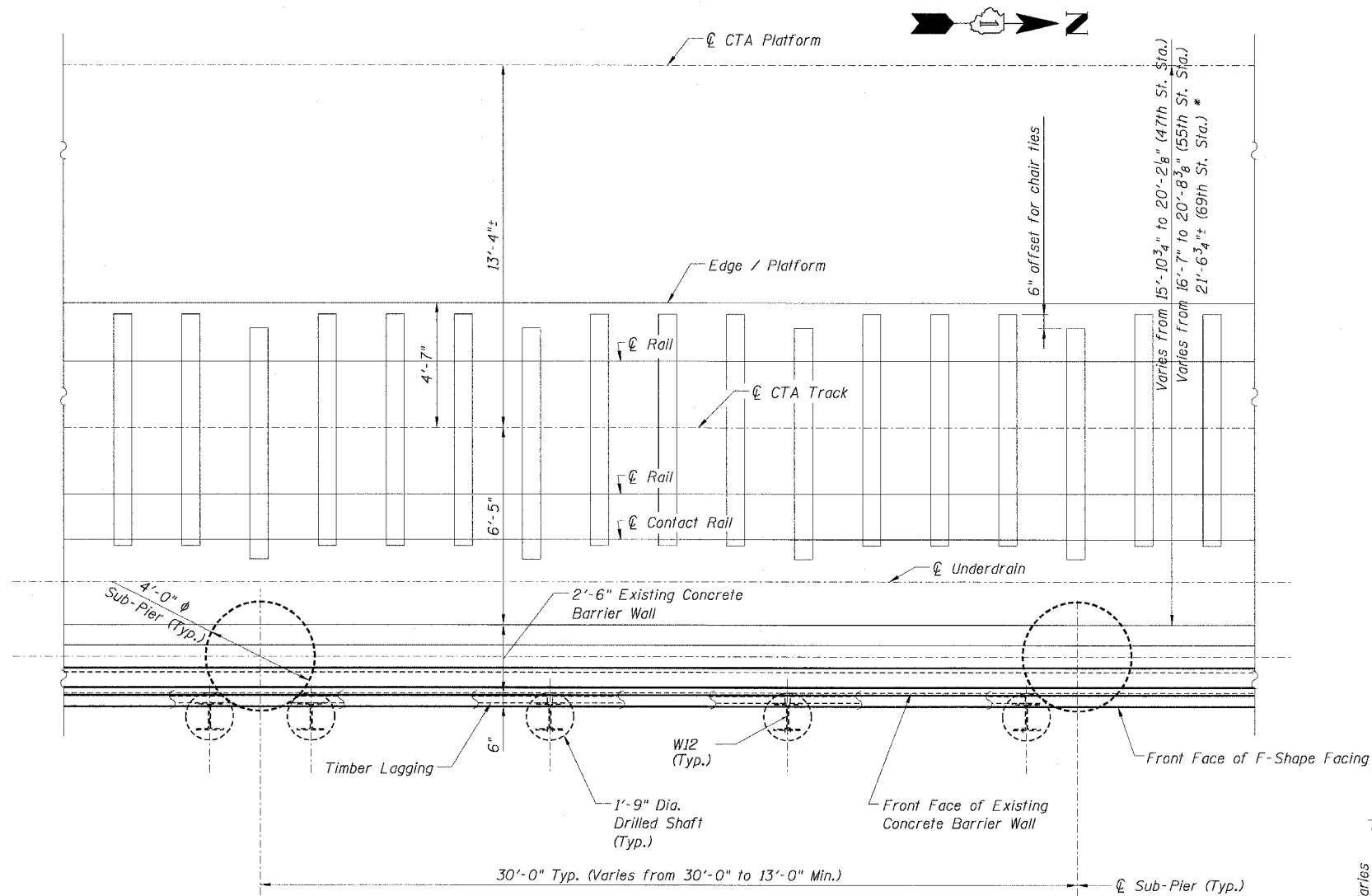
REVISIONS	
NAME	DATE
MJP	8-12-05

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
71ST STREET TO 31ST STREET (NB EXPRESS LANES)
EXISTING BARRIER WALL MODIFICATIONS
AT CTA STATIONS
PROPOSED T/S SHOULDER AT OR ABOVE EXISTING
TYPICAL SECTIONS & DETAILS
SCALE: None DRAWN BY: KWT
DATE: July 7, 2005 CHECKED BY: RDP

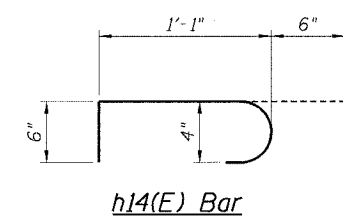
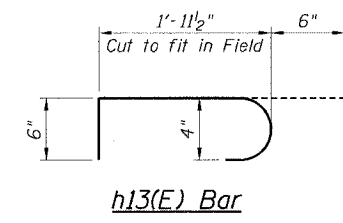
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE. 90/94	SECTION •	COUNTY COOK	TOTAL SHEETS 556	SHEET NO. 373
STA.		TO STA.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
• (1818, ETC, 2324.6-1PIR-8 62300				



PLAN - (NORTH BOUND SHOWN)
(SOUTH BOUND SIMILAR)

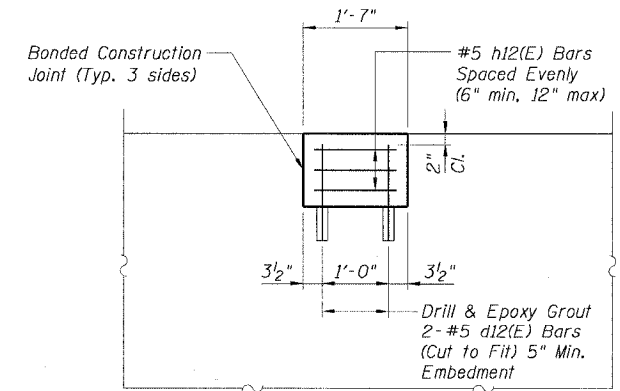
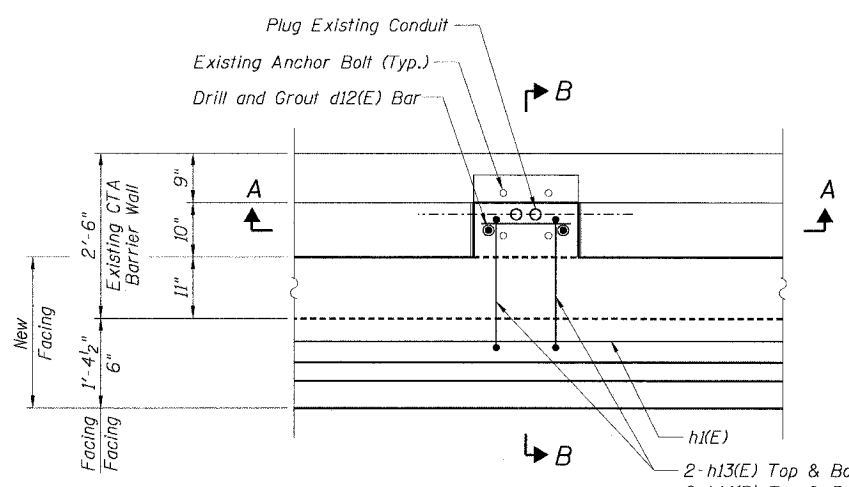


BAR SCHEDULE
FOR 1'-4 1/2" FACING @ ONE LIGHT FDN.

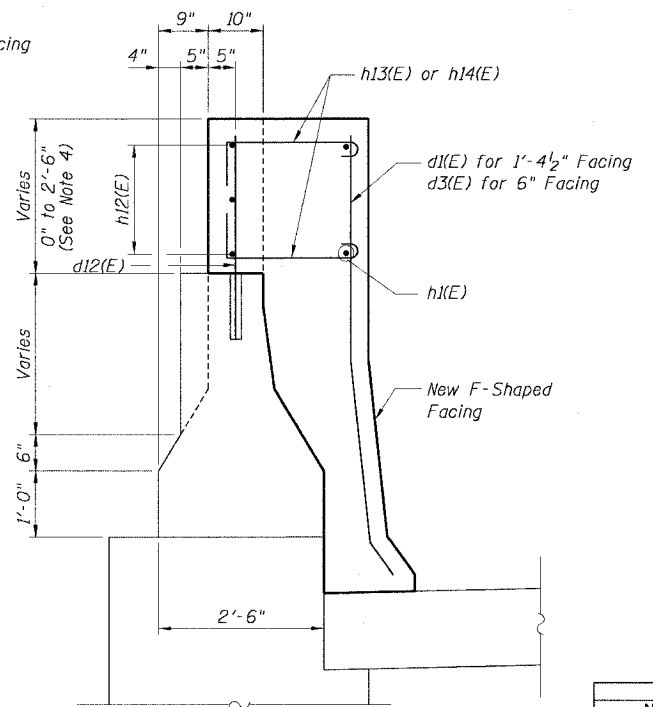
Bar	No.	Size	Length	Shape
d12(E)	2	#5	2'-9"	—
h12(E)	3	#4	1'-4"	—
h13(E)	4	#4	3'-0"	⌋

BAR SCHEDULE
FOR 6" FACING @ ONE LIGHT FDN.

Bar	No.	Size	Length	Shape
d12(E)	2	#5	2'-9"	—
h12(E)	3	#4	1'-4"	—
h14(E)	4	#4	2'-1"	⌋



SECTION A-A
FACING DETAILS AT
EXISTING LIGHT POLE FOUNDATION



SECTION B-B

NOTES:

- Existing anchor bolts shall remain in place and be blast cleaned to gray metal.
- Drilling and grouting #4 d12(E) bars shall be in accordance with Section 584 of the Standard Specifications.
- The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 d12(E) bars and blast cleaning existing anchor bolts shall be included in the cost per cubic yard for Concrete Superstructure.
- For depth of existing light pole notch less than 10", do not use reinforcement bars.
- Cast new concrete monolithically with new F-Shape Facing.
- For Facing Details and Sections see Sheet B2.

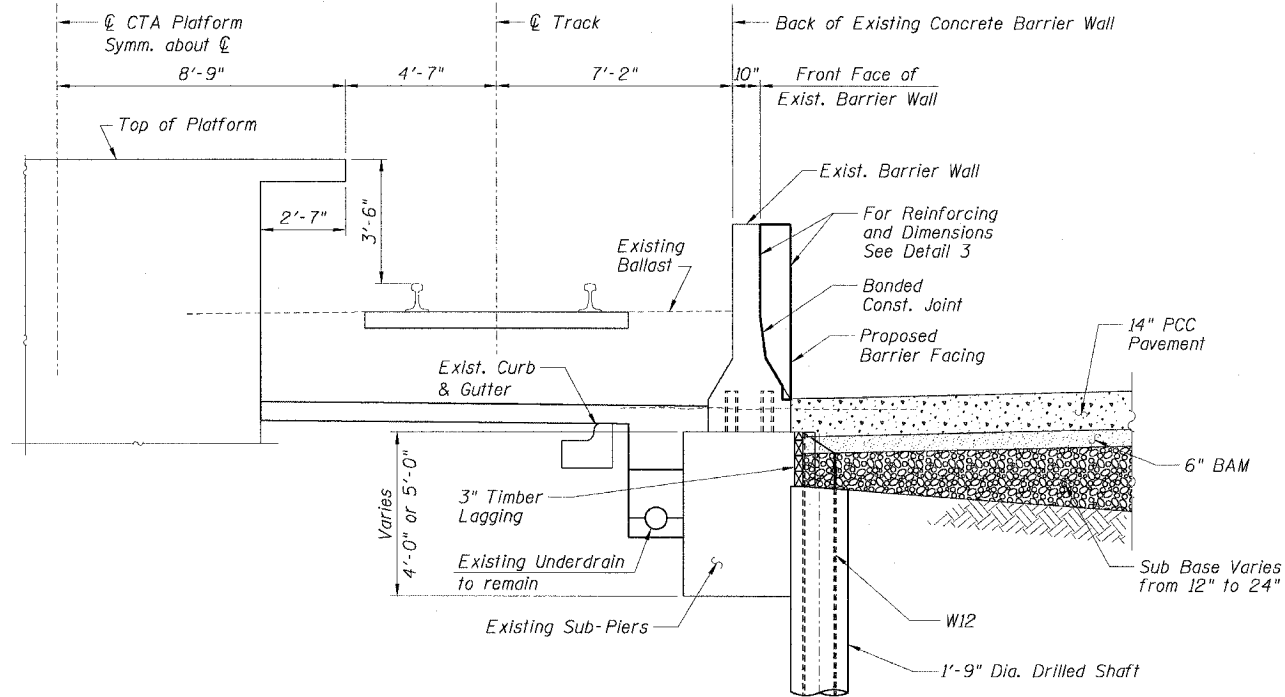
CTE | AECOM

REVISIONS	
NAME	DATE
MJP	8-12-05

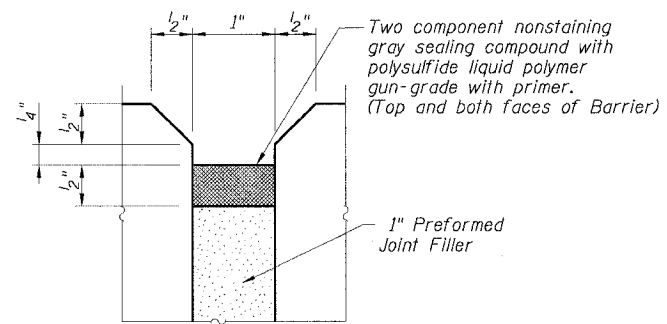
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ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
71ST STREET TO 31ST STREET (NB EXPRESS LANES)
EXISTING BARRIER WALL MODIFICATIONS
AT CTA STATIONS
PROPOSED T/SHOULDER AT OR ABOVE EXISTING
TYPICAL PLAN
SCALE: None DRAWN BY: KWT/CHD
DATE: July 7, 2005 CHECKED BY: RDP

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	*	COOK	556	375
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		
* (1818, ETC. 2324.6-1)PR-8			62300	



TYPICAL SECTION
BETWEEN SUB-PIERS
AT 69TH ST. STATION (NB)



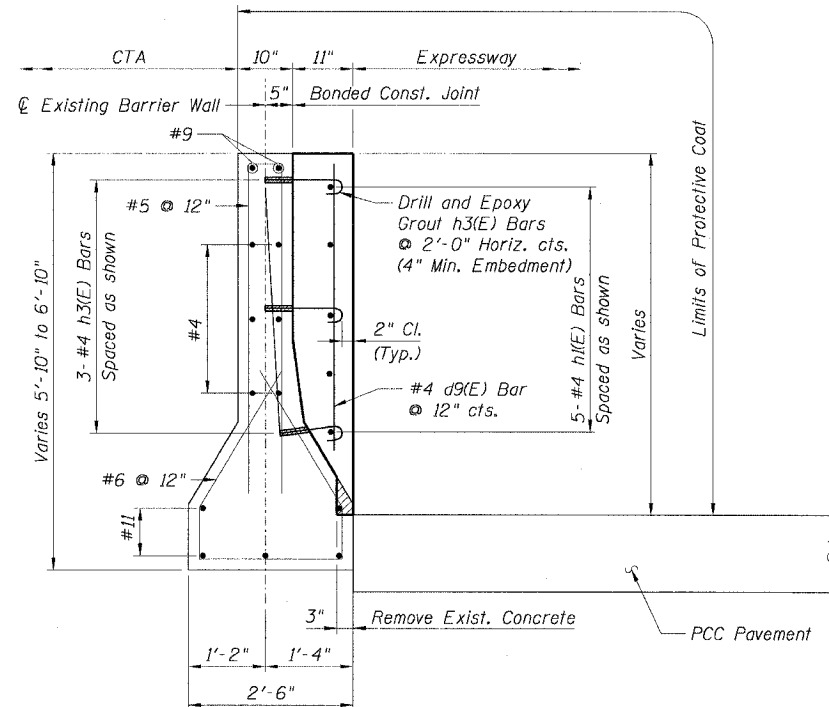
EXPANSION JOINT DETAIL
AT EXISTING JOINTS

BILL OF MATERIAL

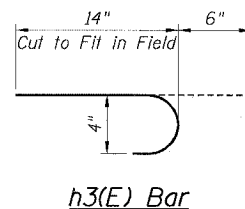
Bar	No.	Size	Length	Shape
d9(E)		#4	4'-2"	—
h1(E)		#4		—
h3(E)		#4	1'-8"	U
Item	Unit	Quantity		
Furnishing Soldier Piles W12x—	Foot			
Drilling and Setting				
Soldier Piles (in Soil)	Cu. Ft.			
Untreated Timber Lagging	Sq. Ft.			
Concrete Removal	Cu. Yd.			
Reinforcement Bars, Epoxy Coated	Pound			
Concrete Superstructure	Cu. Yd.			
Protective Coat	Sq. Yd.			

NOTES:

1. Drilling and grouting of #4 h3(E) bars shall be in accordance with Section 584 of the Standard Specifications.
2. The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h3(E) bars shall be included in the cost per cubic yard for Concrete Superstructure.
3. Provide joints in facing to match those in the existing barrier.
4. The cost of furnishing and installing sealing compound shall be included in the cost per cubic yard for Concrete Superstructure.
5. Minimum lap length for #4 bars shall be 1'-8".
6. For Existing Concrete Repair Details see Sheet B3.
7. For Facing Details at Existing Light Pole Foundation see Sheet B5.



DETAIL 3



h3(E) Bar

LEGEND:

	Concrete Removal
	PCC Pavement
	BAM
	Sub Base
	Earth

REVISIONS	
NAME	DATE
MJP	8-12-05

Sheet B7 of 16
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
71ST STREET TO 31ST STREET (NB EXPRESS LANES)
EXISTING BARRIER WALL MODIFICATIONS
AT CTA STATIONS
TYPICAL SECTIONS & DETAILS
FOR 69th ST. STATION (NB)
SCALE: None DRAWN BY: KWT
DATE: July 7, 2005 CHECKED BY: RDP

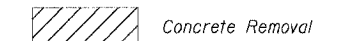
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	*	COOK	556	376
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		
* (1818, ETC, 2324.6-1)PR-8			62300	

**BILL OF MATERIAL
FOR ONE TRANSITION**

Bar	No.	Size	Length	Shape
d1(E)	17	#4	6'-5"	—
d6(E)	50	#4	6'-0"	—
h6(E)	112	#4	1'-9"	—
h7(E)	8	#4	29'-0"	—
h8(E)	16	#4	21'-5"	—
Item	Unit	Quantity		
Reinforcement Bars, Epoxy Coated	Pound	788		
Concrete Superstructure	Cu. Yd.	18.0		
Protective Coat	Sq. Yd.	57		
Concrete Removal	Cu. Yd.	1.1		

LEGEND:



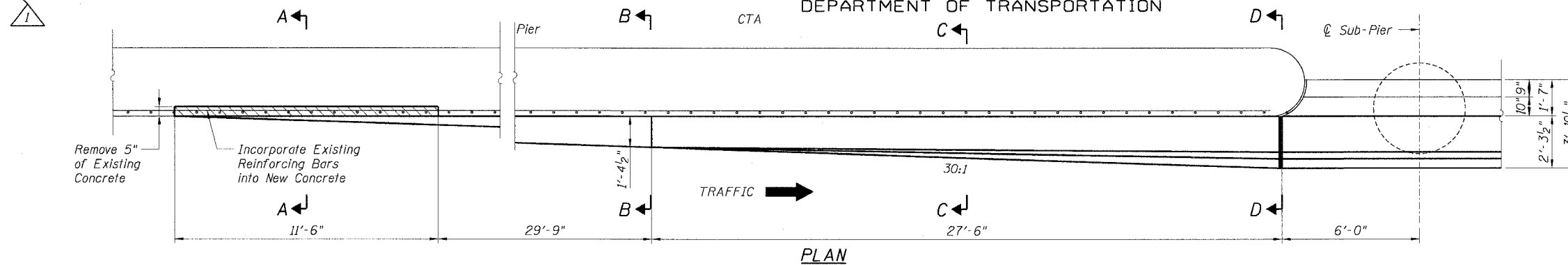
NOTES:

- Drilling and grouting of #4 h2(E) and h6(E) bars shall be in accordance with Section 584 of the Standard Specifications.
- The cost of drilling holes, furnishing and placing Epoxy Grout and setting h2(E) and h6(E) bars shall be included in the cost per cubic yard for Concrete Superstructure.
- Exposed existing reinforcement bars shall remain in place and be blast cleaned to gray metal. Reinforcement bars that have been cut or have lost 25% or more of their original cross sectional area shall be supplemented by new epoxy coated reinforcement bars.
- The cost of supplementing and cleaning existing reinforcement bars shall be included in the cost per cubic yard for Concrete Removal.
- For Refaced Barrier Wall Details see Sheets B2 and B3.
- For locations of Transition, see Roadway Plans.

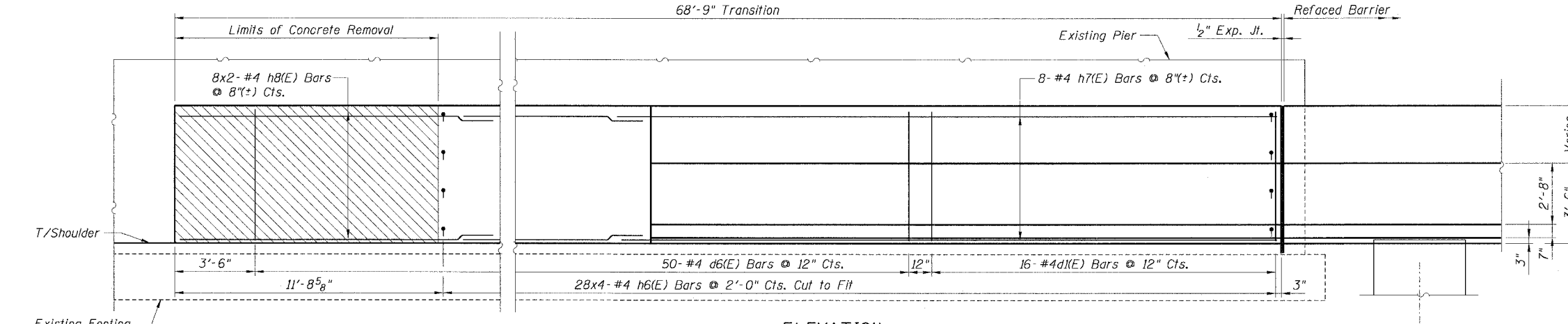
Sheet B8 of 16

REVISIONS	
NAME	DATE
MJP	8-12-05

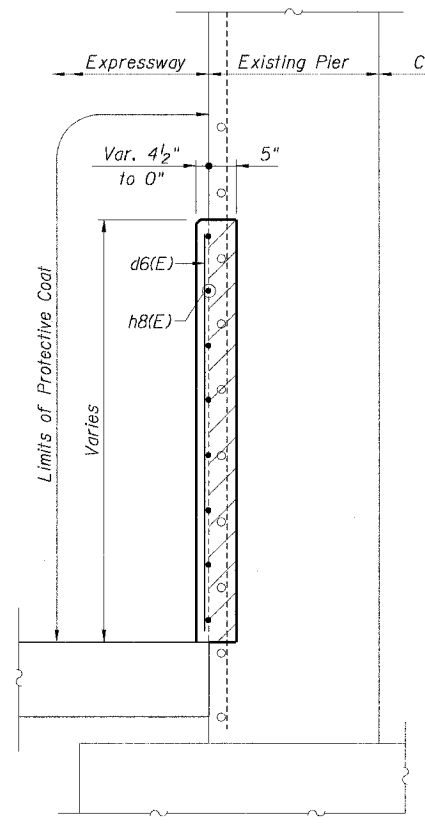
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
71ST STREET TO 31ST STREET (NB EXPRESS LANES)
EXISTING BARRIER WALL MODIFICATIONS
AT CTA STATIONS
PROPOSED T/S SHOULDER BELOW EXISTING
REFACED BARRIER TO PIER
TRANSITION DETAILS (TAPER)
SCALE: None DRAWN BY: CHD
DATE: July 7, 2005 CHECKED BY: RDP



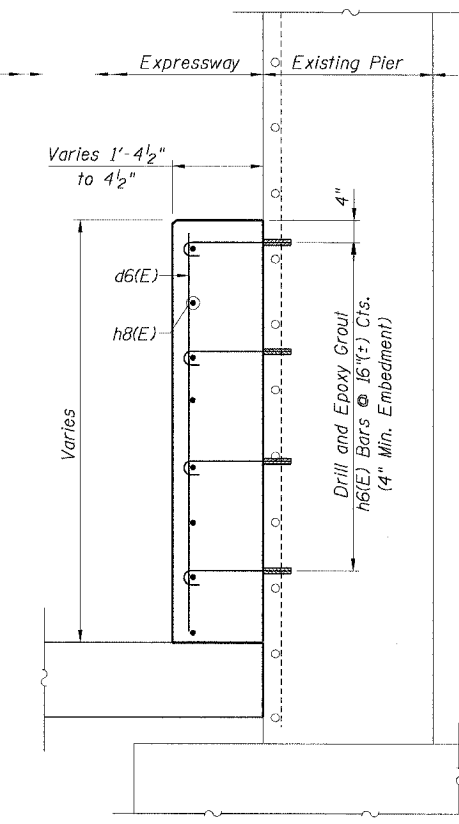
PLAN



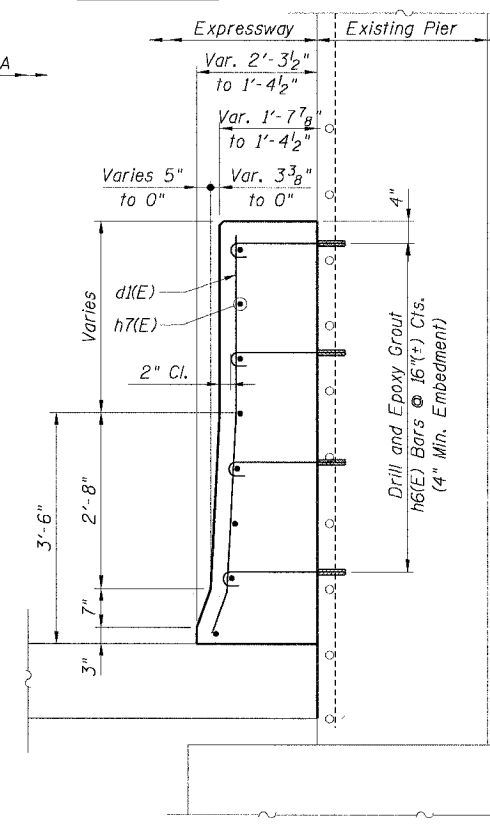
ELEVATION



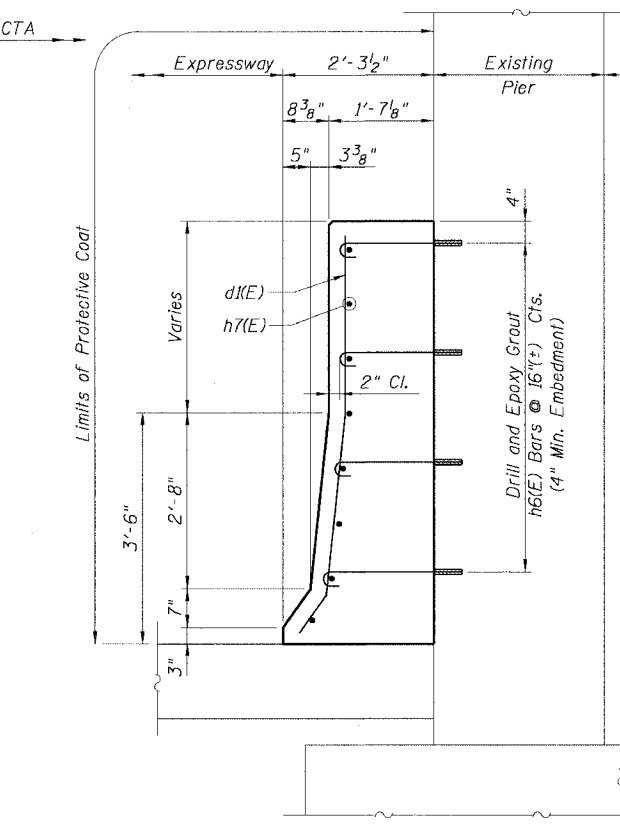
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

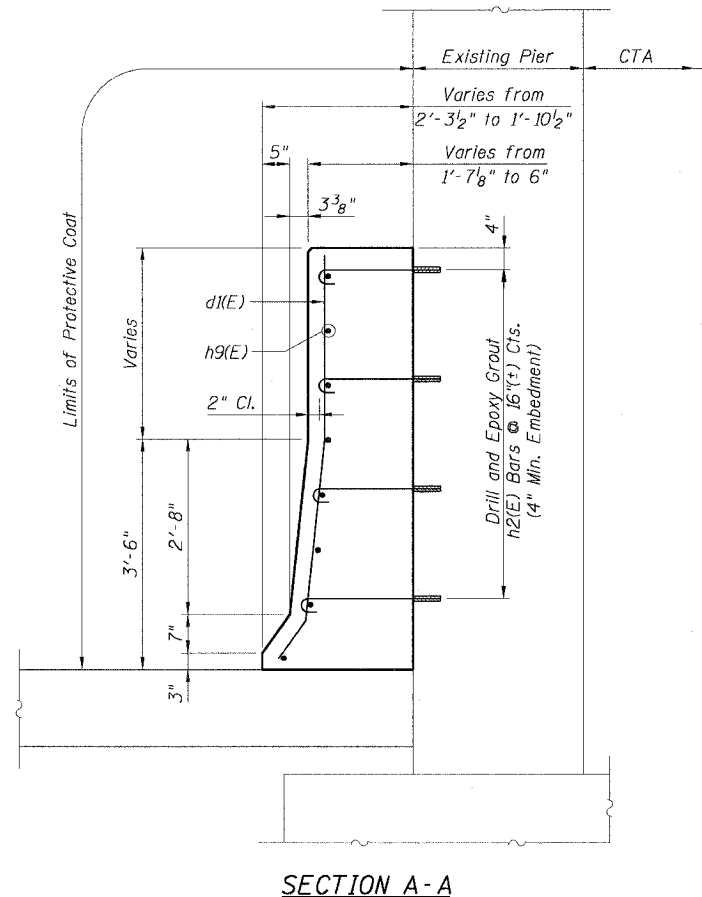
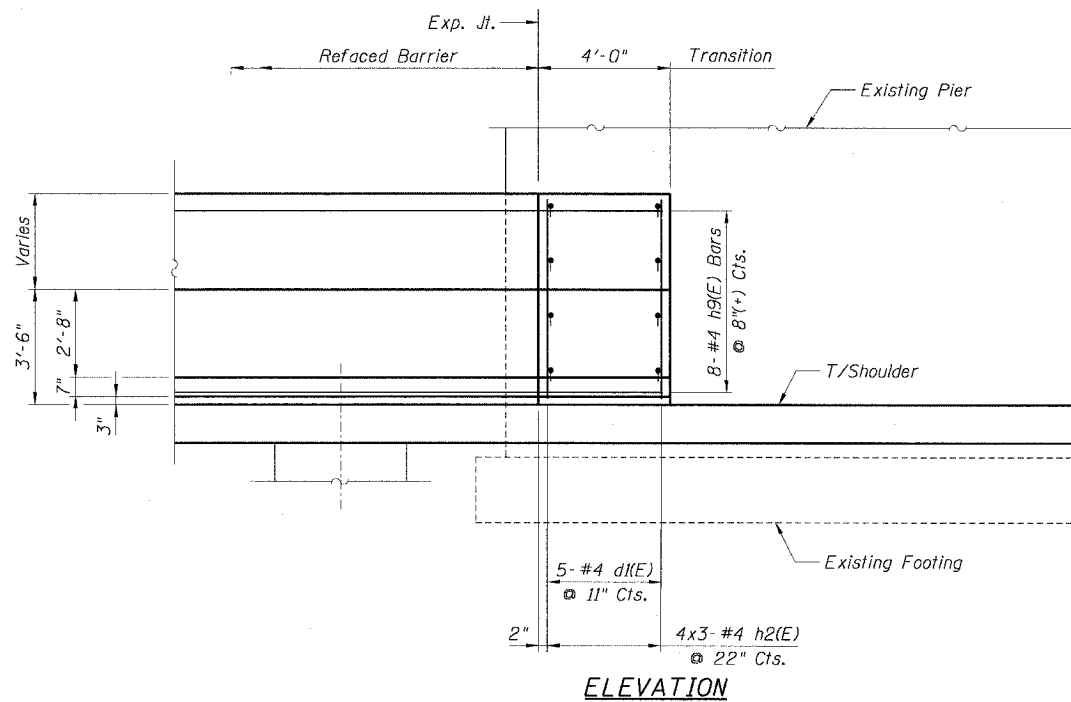
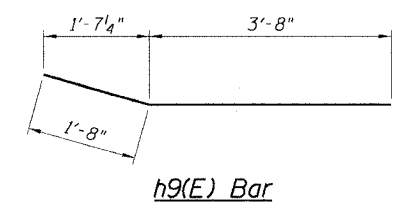
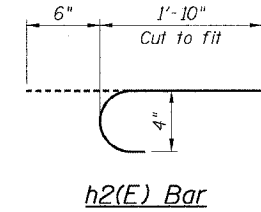
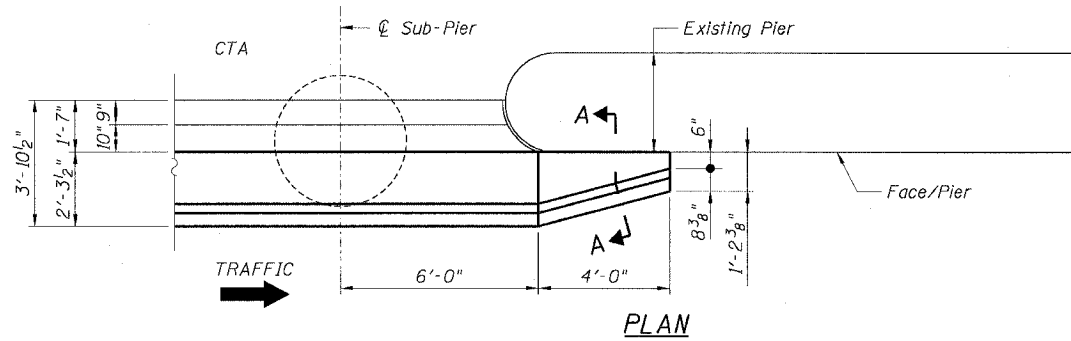
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94		COOK	556	377
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		
(181B, ETC. 2324.6-1P)R-8				62300

**BILL OF MATERIAL
FOR ONE TRANSITION**

Bar	No.	Size	Length	Shape
d(E)	5	#4	6'-5"	—
h2(E)	12	#4	2'-4"	⌋
h9(E)	8	#4	5'-4"	—
Item	Unit	Quantity		
Reinforcement Bars, Epoxy Coated	Pound	56		
Concrete Superstructure	Cu. Yd.	1.4		
Protective Coat	Sq. Yd.	4		



LEGEND:

Concrete Removal

NOTES:

1. Drilling and grouting of #4 h2(E) bars shall be in accordance with Section 584 of the Standard Specifications.
2. The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h2(E) bars shall be included in the cost per cubic yard for Concrete Superstructure.
3. For d(E) bar details, see Sheet B2.
4. For Refaced Barrier Wall Details, see Sheets B2 and B3.
5. For locations of Transition, see Roadway Plans.

REVISIONS	
NAME	DATE
MJP	8-12-05

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
71ST STREET TO 31ST STREET (NB EXPRESS LANES)
EXISTING BARRIER WALL MODIFICATIONS
AT CTA STATIONS
PROPOSED T/S SHOULDER BELOW EXISTING
REFACED BARRIER TO PIER
TRANSITION DETAILS (NO TAPER)
SCALE: None DRAWN BY: CHD
DATE: July 7, 2005 CHECKED BY: MJP

CTE | AECOM

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	*	COOK	556	378
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		
* (1B1B, ETC, 2324.6-1PIR-8			62300	

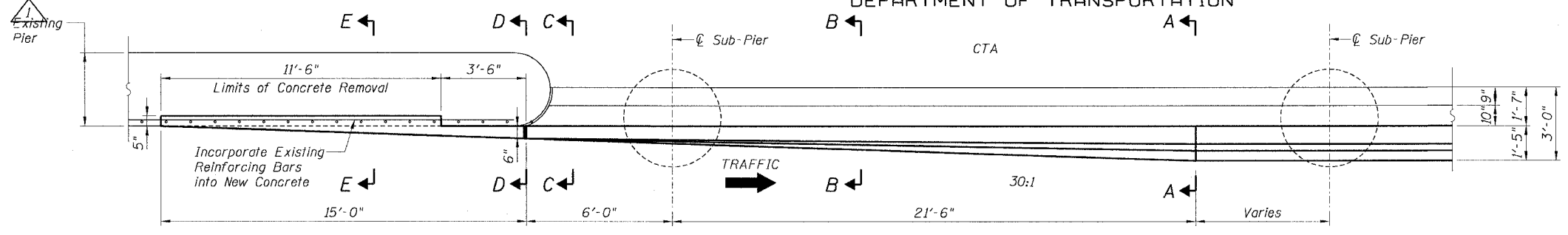
**BILL OF MATERIAL
FOR ONE TRANSITION**

Bar	No.	Size	Length	Shape
d3(E)	15	#4	5'-2"	—
d5(E)	25	#4	4'-8"	—
h3(E)	48	#4	1'-8"	⌋
h4(E)	6	#4	27'-2"	—
h5(E)	6	#4	14'-8"	—
h6(E)	27	#4	1'-2"	⌋
Item	Unit	Quantity		
Reinforcement Bars, Epoxy Coated	Pound	372		
Concrete Superstructure	Cu. Yd.	4.7		
Protective Coat	Sq. Yd.	25		
Concrete Removal	Cu. Yd.	0.9		

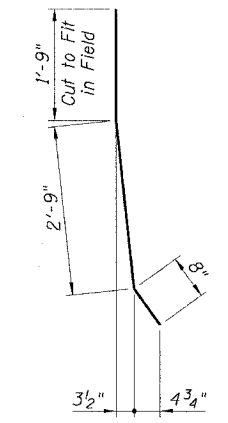
NOTES:

- Drilling and grouting of #4 h3(E) and h6(E) bars shall be in accordance with Section 584 of the Standard Specifications.
- The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h3(E) and h6(E) bars shall be included in the cost per cubic yard for Concrete Superstructure.
- Exposed existing reinforcement bars shall remain in place and be blast cleaned to gray metal. Reinforcement bars that have been cut or have lost 25% or more of their original cross sectional area shall be supplemented by new epoxy coated reinforcement bars.
- The cost of supplementing and cleaning existing reinforcement bars shall be included in the cost per cubic yard for Concrete Removal.
- Provide joints in facing to match those in the existing barrier. See Sheet B7 for details.
- For Soldier Pile and Lagging Retention System, see Sheets B1, B4 and B5.
- For Refaced Barrier Wall Details, see Sheets B4 and B5.
- For Sections A-A, B-B and C-C, see Sheet B11.
- For Existing Concrete Repair Details, see Sheet B3.
- For Facing Details at Existing Light Pole Foundation, see Sheet B5.
- For locations of Transition, see Roadway Plans.

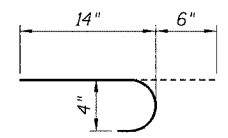
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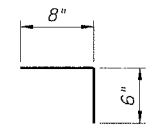
PLAN



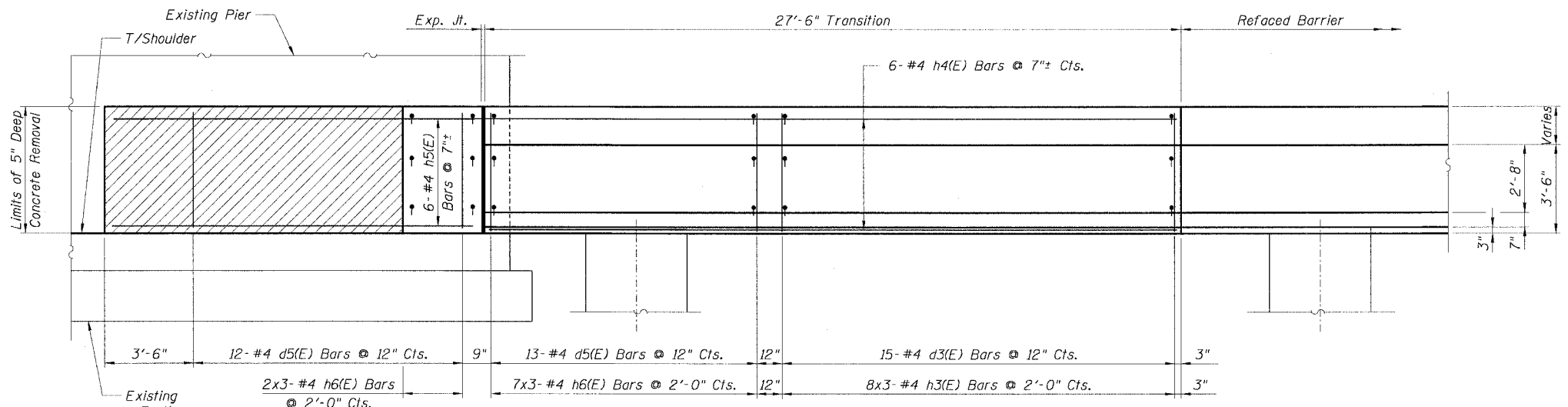
d3(E) Bar



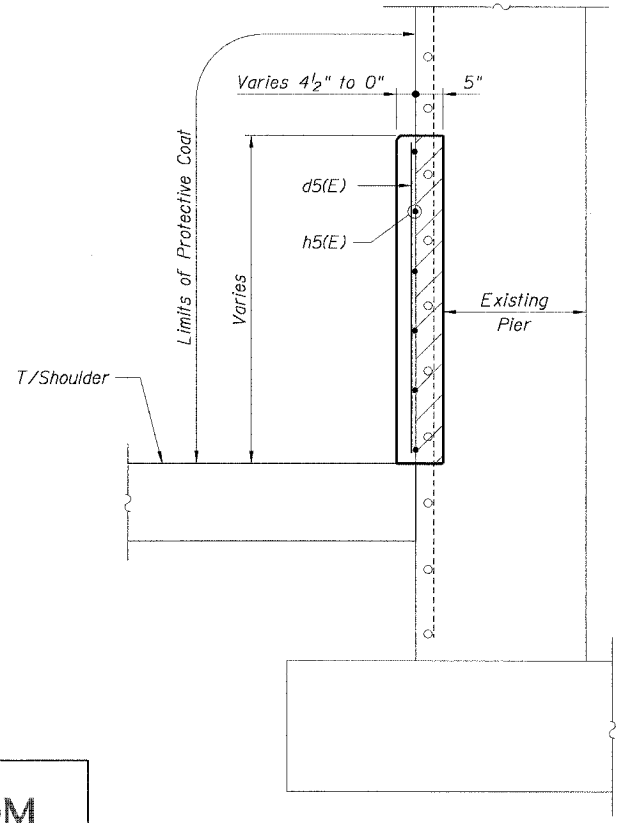
h3(E) Bar



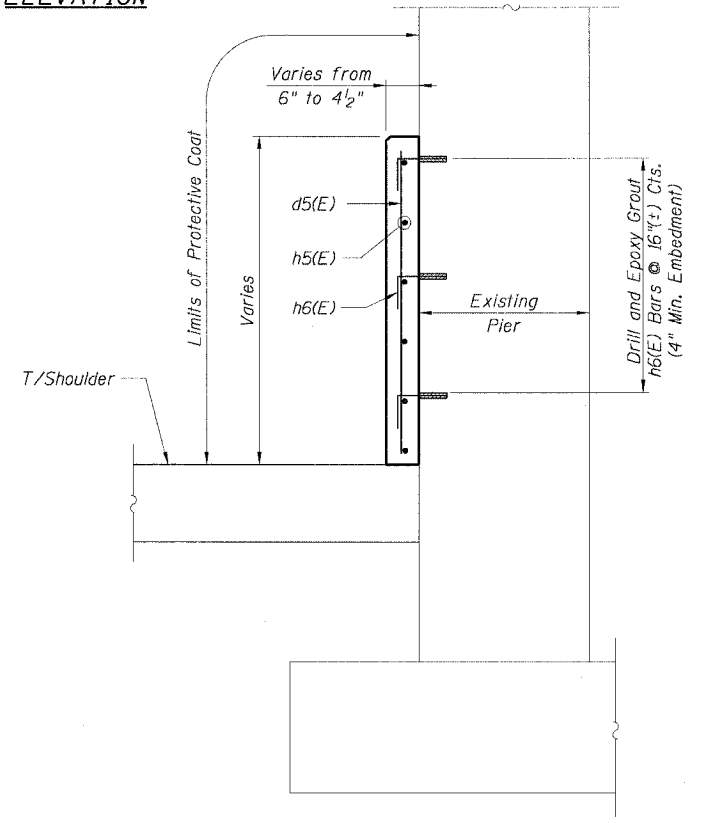
h6(E) Bar



ELEVATION

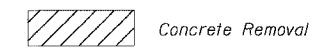


SECTION E-E



SECTION D-D

LEGEND:



Concrete Removal

REVISIONS	
NAME	DATE
MJP	8-12-05

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
71ST STREET TO 31ST STREET (NB EXPRESS LANES)
EXISTING BARRIER WALL MODIFICATIONS
AT CTA STATIONS
PROPOSED T/S SHOULDER AT OR ABOVE EXISTING
REFACED BARRIER TO PIER
TRANSITION DETAILS (TAPER)
SCALE: None DRAWN BY: CHD
DATE: July 7, 2005 CHECKED BY: MJP

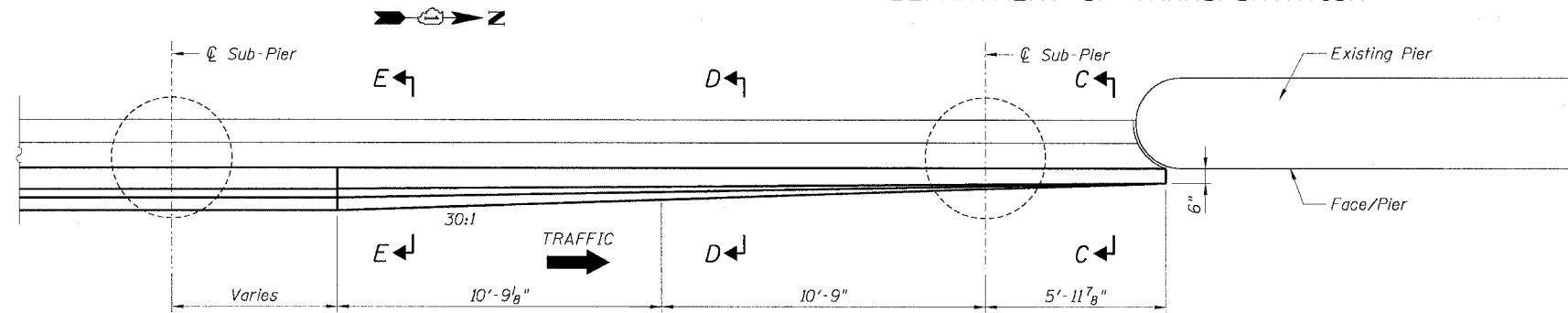
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

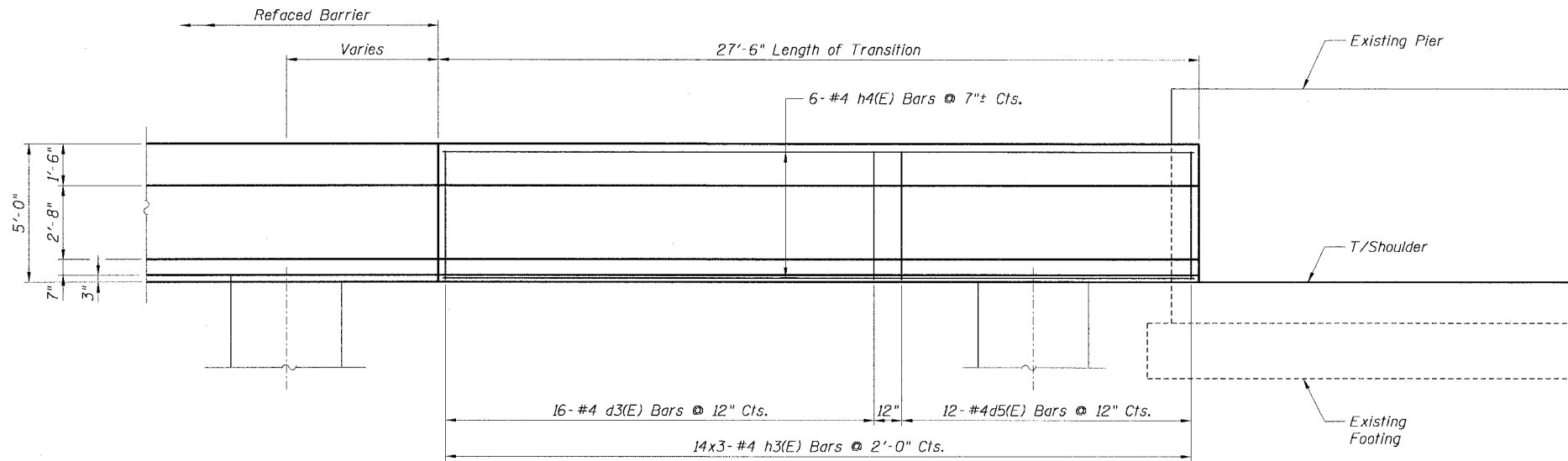
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	*	COOK	556	379
STA.	TO STA.			
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
* (1818, ETC. 2324.6-1P1R-8				62300

**BILL OF MATERIAL
FOR ONE TRANSITION**

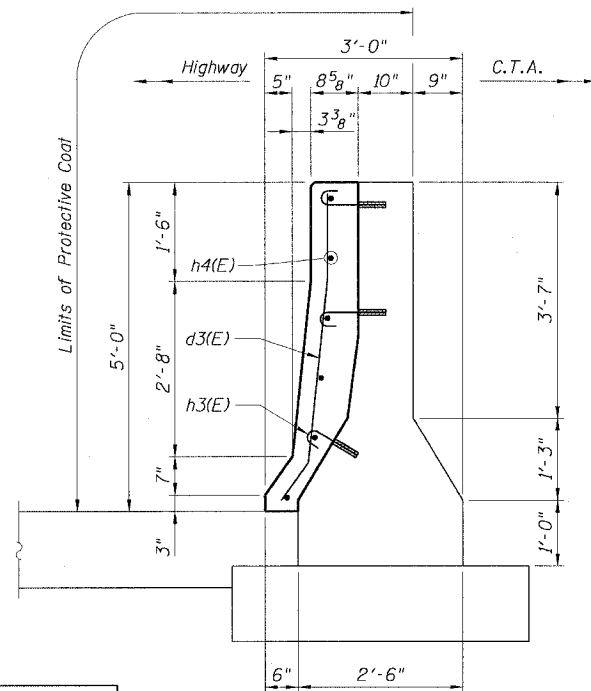
Bar	No.	Size	Length	Shape
d3(E)	16	#4	5'-3"	—
d5(E)	12	#4	4'-8"	—
h3(E)	42	#4	1'-6"	—
h4(E)	6	#4	26'-8"	—
Item	Unit	Quantity		
Reinforcement Bars, Epoxy Coated		250		
Concrete Structures		3.0		
Protective Coat		19.6		
Concrete Removal				



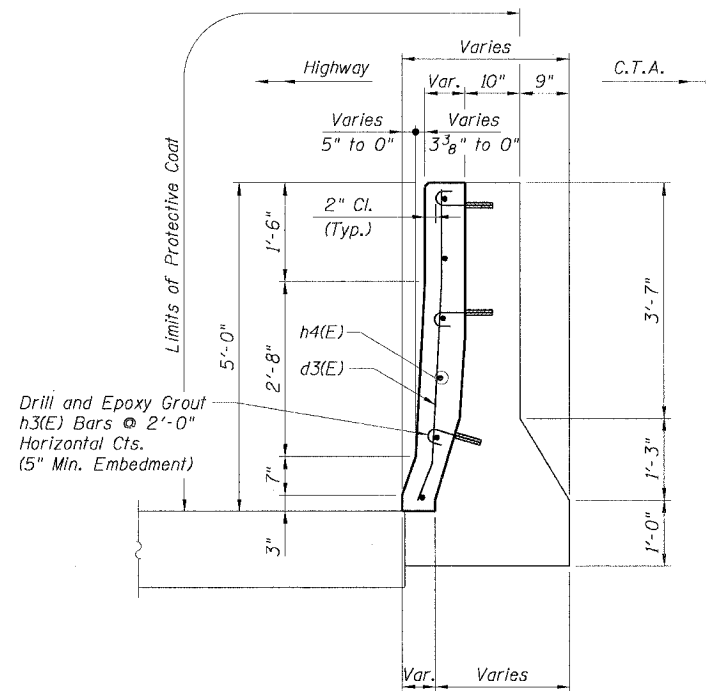
PLAN



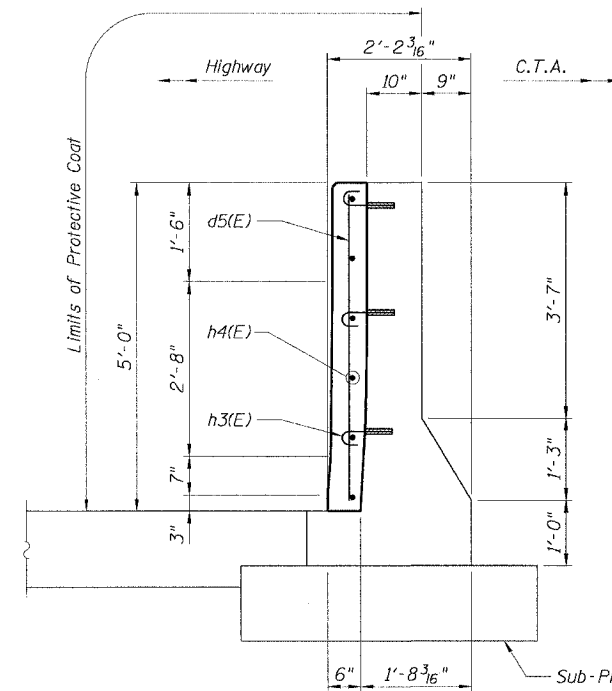
ELEVATION



SECTION E-E

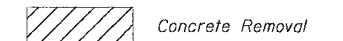


SECTION D-D



SECTION C-C

LEGEND:



NOTES:

1. Drilling and grouting of #4 h3(E) bars shall be in accordance with Section 584 of the Standard Specifications.
2. The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h3(E) bars shall be included in the cost per cubic yard for Concrete Structures.
3. Provide joints in facing to match those in the existing barrier.
4. For Refaced Barrier details, see Sheets B4 and B5.
5. For locations of Transition, see Roadway Plans.

REVISIONS	NAME	DATE

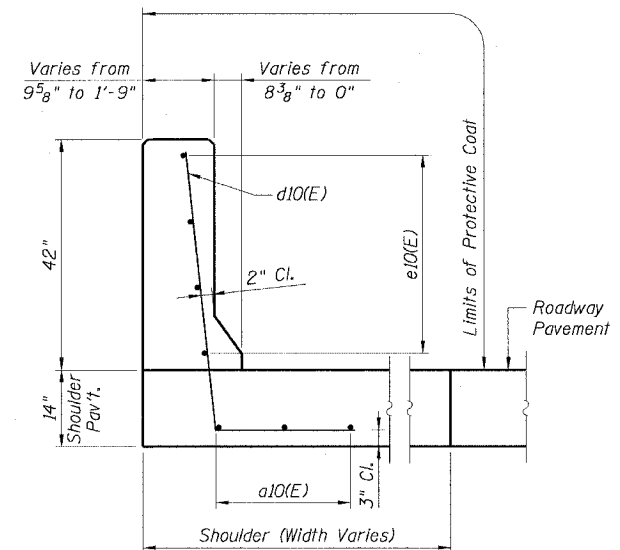
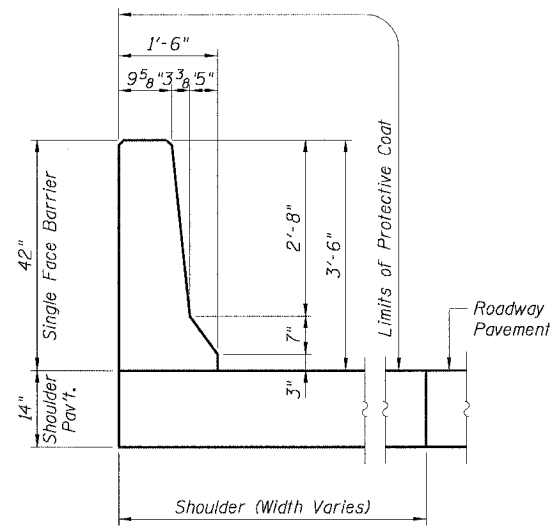
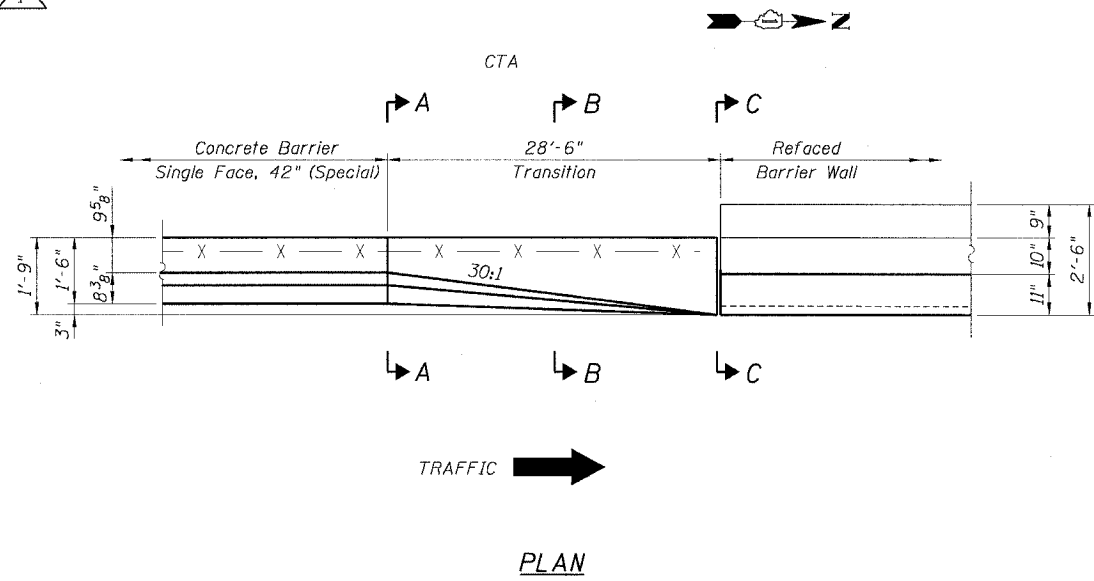
Sheet B11 of 16
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
**EXISTING BARRIER WALL MODIFICATIONS
AT CTA STATIONS**
**PROPOSED T/S SHOULDER AT OR ABOVE EXISTING
REFACED BARRIER TO PIER
TRANSITION DETAILS (NO TAPER)**
SCALE: None
DATE: JULY 7, 2005
DRAWN BY: CHD
CHECKED BY: RDP

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303 East Wacker Drive, Suite 600, Chicago, Illinois 60601-5276
T 312.336.0200 F 312.336.1029 www.cte.com

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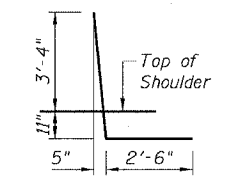
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94		COOK	556	380
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		
(1818, ETC, 2324.6-1PIR-B			62300	

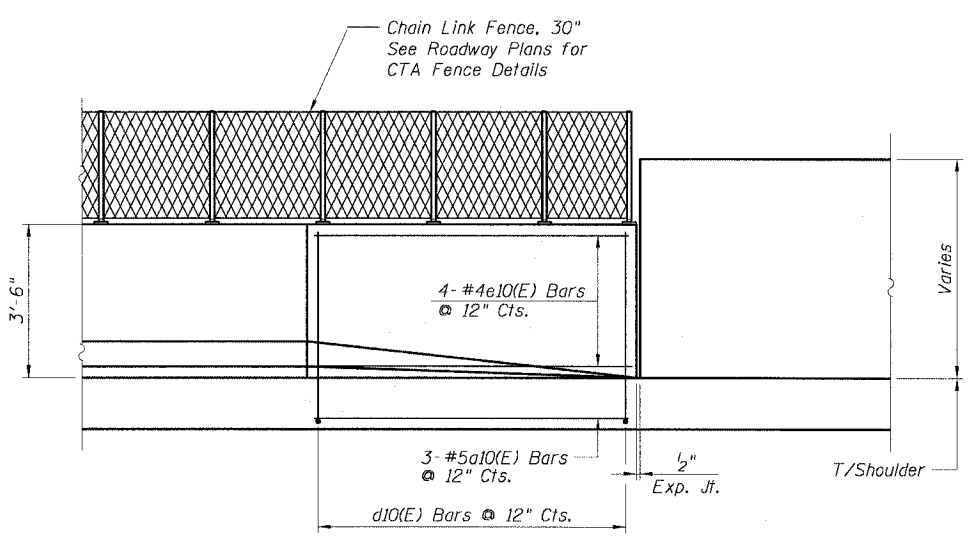


BILL OF MATERIAL FOR ONE TRANSITION

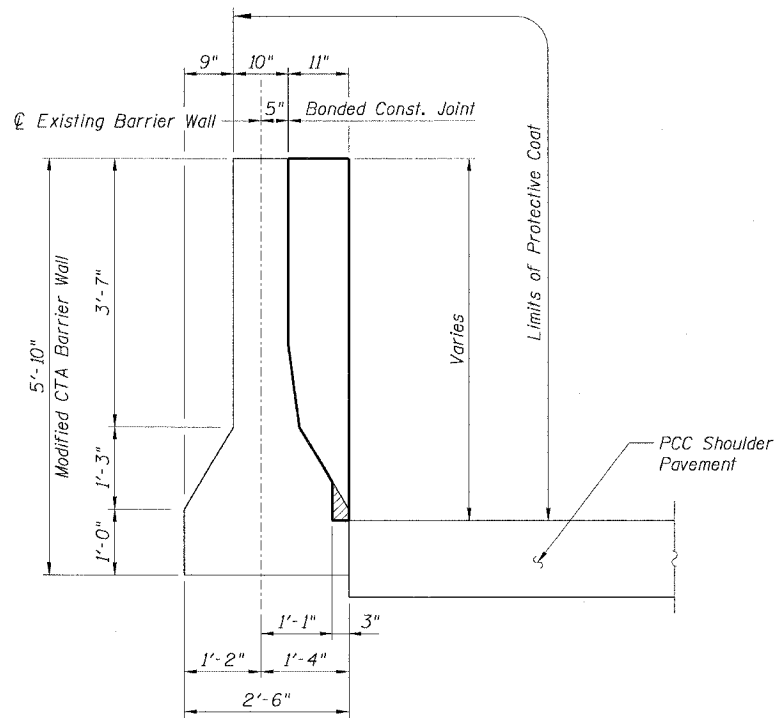
Bar	No.	Size	Length	Shape
a10	3	#5	28'-2"	—
d10	29	#6	6'-9"	L
e10	4	#4	28'-2"	—
Item			Unit	Quantity
Reinforcement Bars, Epoxy Coated			Pound	457
Concrete Superstructure			Cu. Yd.	5.2
Protective Coat			Sq. Yd.	15



BAR d10(E)



ELEVATION



SECTION C-C

NOTES:

- For alternative vertical reinforcing details, see "Concrete Barrier Single Face, 42" Alternates".
- For existing CTA Barrier modifications - vertical facing, see sheet B7.
- For Expansion Joint details see Sheet B6.
- For locations of Transition, see Roadway Plans.

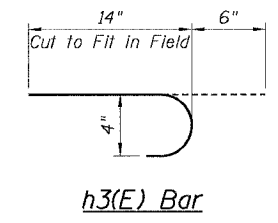
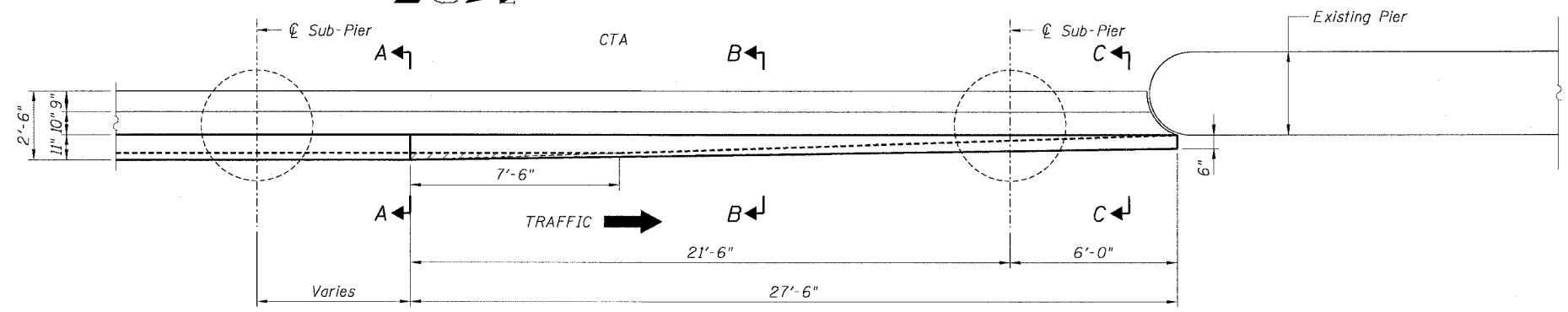
CTE | AECOM

REVISIONS	
NAME	DATE
MJP	8-12-05

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
71ST STREET TO 31ST STREET (NB EXPRESS LANES)
EXISTING BARRIER WALL MODIFICATIONS
AT CTA STATIONS
PROPOSED T/SOULDER AT OR ABOVE EXISTING
NEW 42" BARRIER TO VERT. REFACED BARRIER
TRANSITION DETAILS (TAPER)
SCALE: None DRAWN BY: CHD
DATE: July 7, 2005 CHECKED BY: RDP

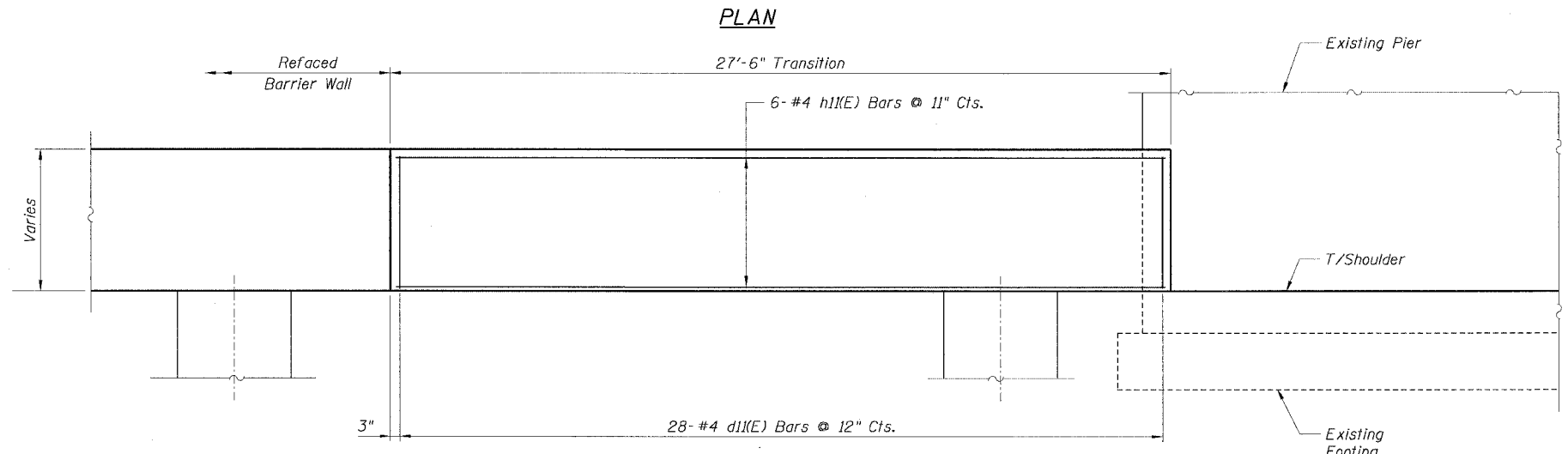
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	*	COOK	556	381
STA.	TO STA.			
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
* (1818, ETC, 2324.6-1PIR-8		62300		



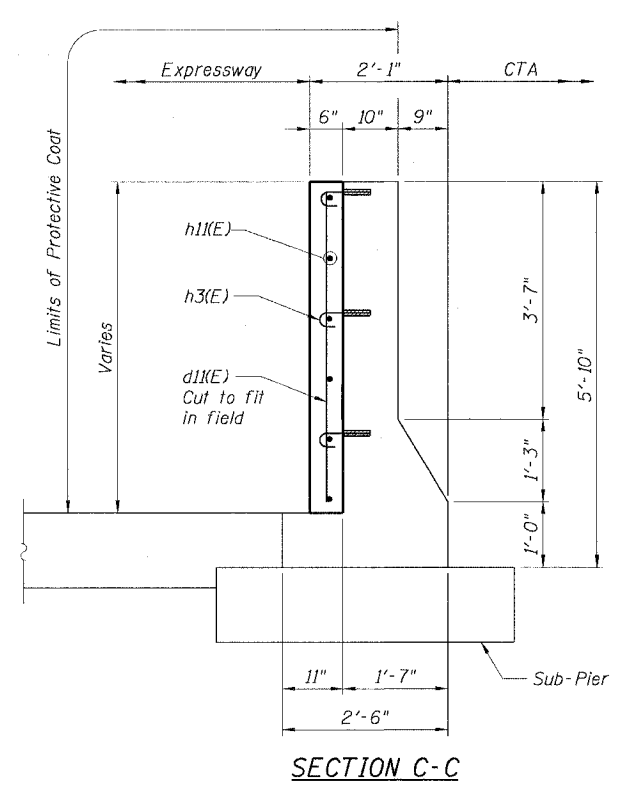
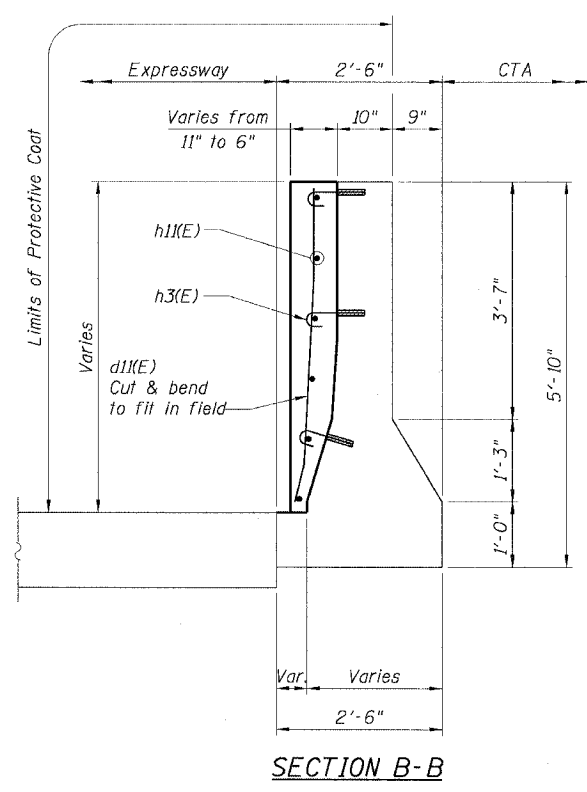
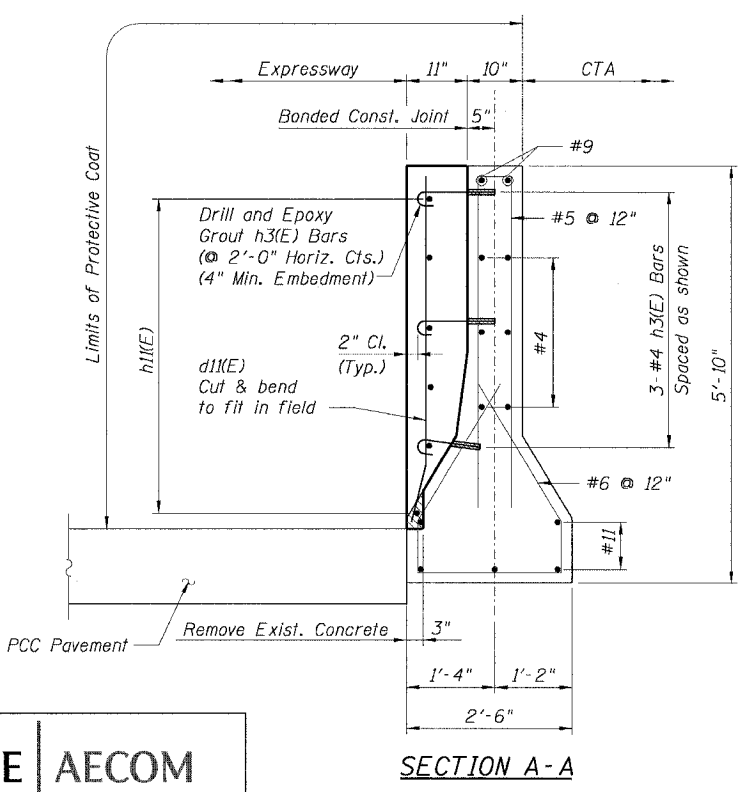
**BILL OF MATERIAL
FOR ONE TRANSITION**

Bar	No.	Size	Length	Shape
d1(E)	28	#4	4'-8"	—
h3(E)	42	#4	1'-8"	U
h1(E)	6	#4	24'-8"	—
<hr/>				
Item	Unit	Quantity		
Reinforcement Bars, Epoxy Coated	Pound	233		
Concrete Superstructure	Cu. Yd.	3.2		
Protective Coat	Sq. Yd.	20		
Concrete Removal	Cu. Yd.	0.1		



LEGEND:
 Concrete Removal

- NOTES:**
1. Drilling and grouting of #4 h3(E) bars shall be in accordance with Section 584 of the Standard Specifications.
 2. The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h3(E) bars shall be included in the cost per cubic yard for Concrete Superstructure.
 3. Provide joints in facing to match those in the existing barrier. See Sheet B7 for details.
 4. For Soldier Pile and Lagging Retention System see Sheets B1, B4 and B5.
 5. For Refaced Barrier Wall Details, see Sheet B7.
 6. For Existing Concrete Repair Details, see Sheet B3.
 7. For Facing Details at Existing Light Pole Foundation, see Sheet B5.
 8. For locations of Transition, see Roadway Plans.



CTE | AECOM

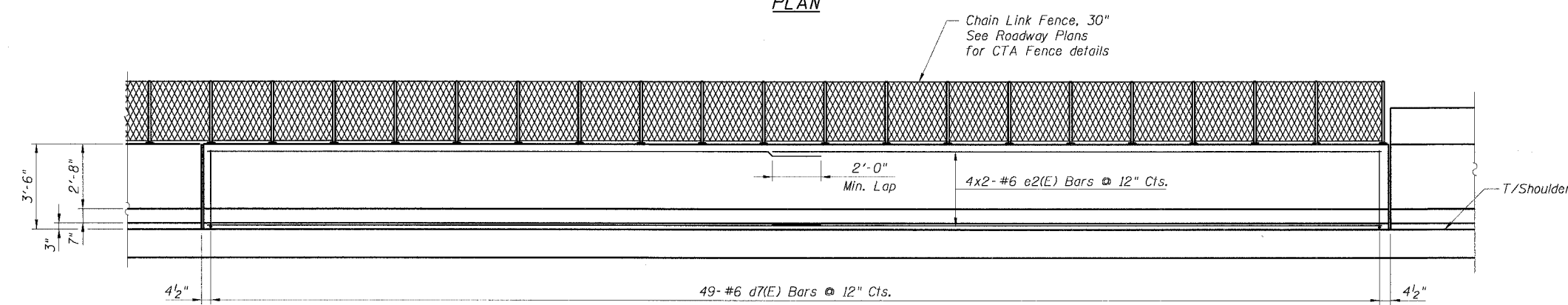
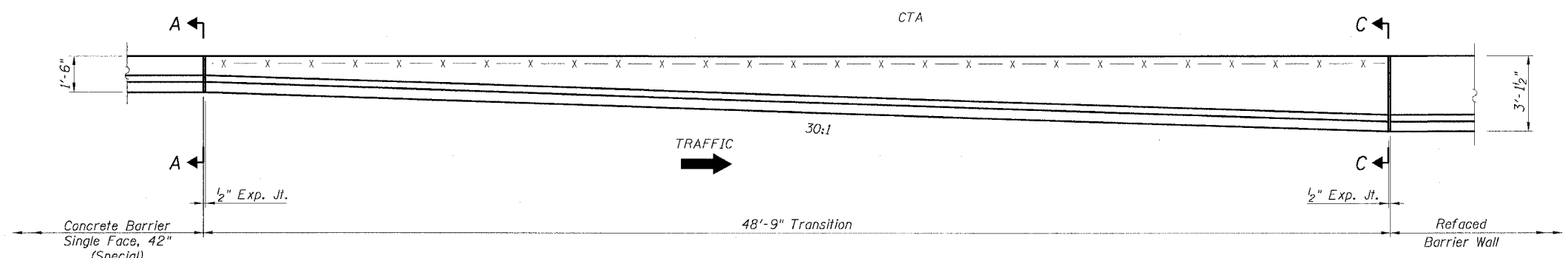
REVISIONS	
NAME	DATE
MJP	8-12-05

Sheet B13 of 16
 ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
 71ST STREET TO 31ST STREET (NB EXPRESS LANES)
 EXISTING BARRIER WALL MODIFICATIONS
 AT CTA STATIONS
 FOR 69TH ST. STATION (NB)
 VERT. REFACED BARRIER TO PIER
 TRANSITION DETAILS (NO TAPER)
 SCALE: None DRAWN BY: CHD
 DATE: July 7, 2005 CHECKED BY: RDP

5/10/2005 9:55:48 AM
 P:\94466-STRUCTURE & DIVISIONS\CT A-BARRIER\RT 09B132A-CT A-B7

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

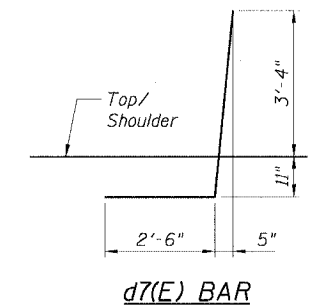
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94		COOK	556	382
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		
• (1818, ETC, 2324.6-1PJR-8			62300	



BILL OF MATERIAL FOR ONE TRANSITION

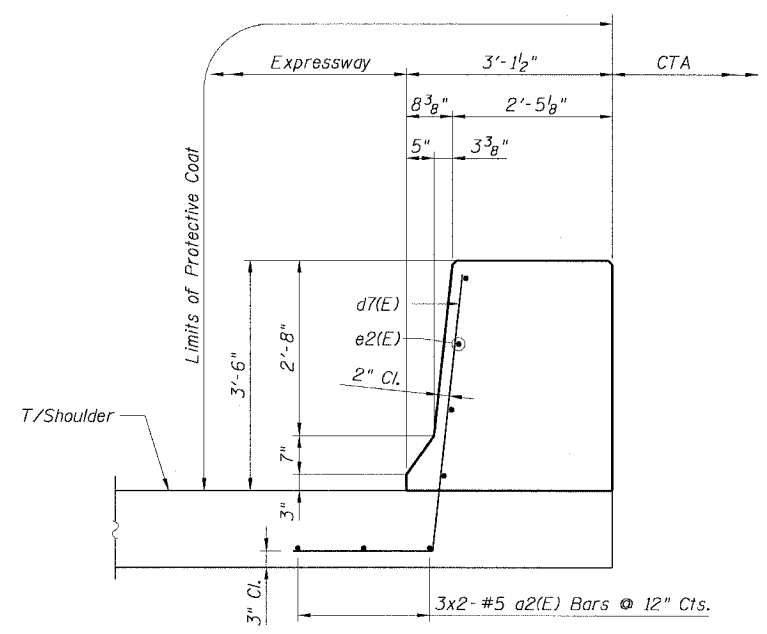
Bar	No.	Size	Length	Shape
a2(E)	6	#5	25'-3"	—
d7(E)	49	#6	6'-9"	—
e2(E)	8	#6	25'-3"	—

Item	Unit	Quantity
Reinforcement Bars, Epoxy Coated	Pound	958
Concrete Superstructure	Cu. Yd.	11.7
Protective Coat	Sq. Yd.	28.5



NOTES:

1. Provide Construction Joint in the transition.
2. For Expansion Joint Details, see Sheet B6.
3. For Section A-A, see Sheet B15.
4. For alternative vertical reinforcing details, see "Concrete Barrier Single Face, 42" Alternatives".
5. For Refaced Barrier Wall Details, see Sheets B2 and B3.
6. For Soldier Pile and Logging Retention System, see Sheets B1 thru B3.
7. For Concrete Barrier Single Face, 42" (Special) Details, see Roadway details.
8. For locations of Transition, see Roadway Plans.



CTE | AECOM

REVISIONS	
NAME	DATE
MJP	8-12-05

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
71ST STREET TO 31ST STREET (NB EXPRESS LANES)
EXISTING BARRIER WALL MODIFICATIONS
AT CTA STATIONS
PROPOSED T/S SHOULDER BELOW EXISTING
REFACED BARRIER TO NEW 42" BARRIER
TRANSITION DETAILS

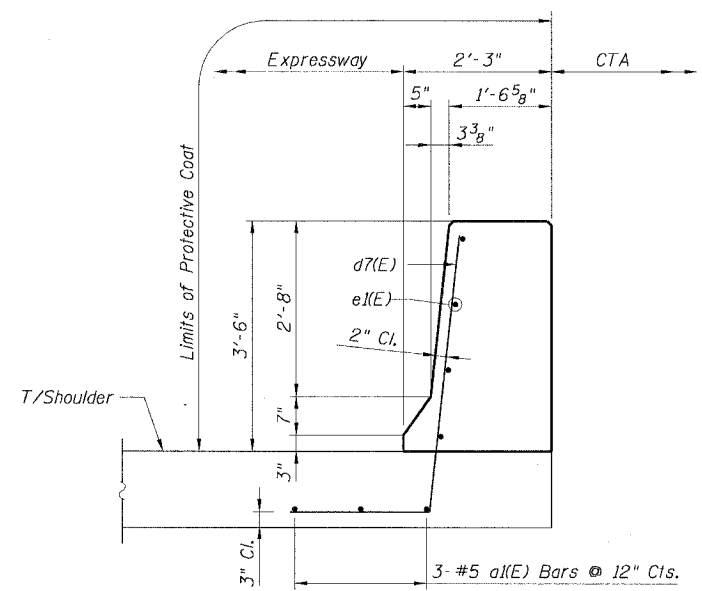
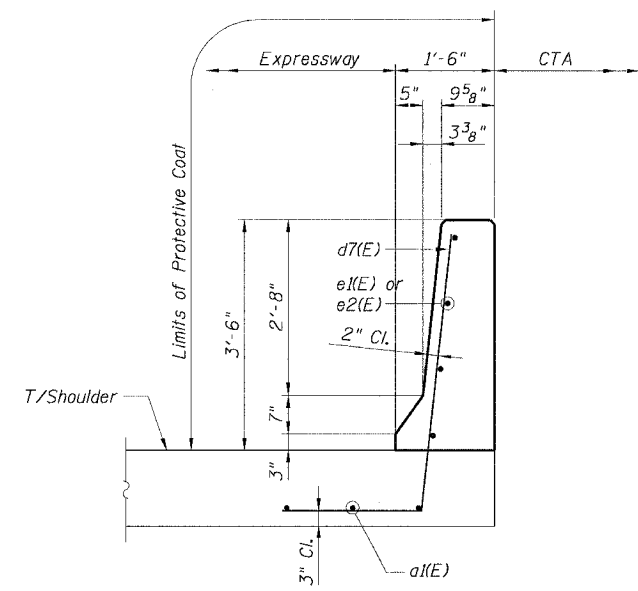
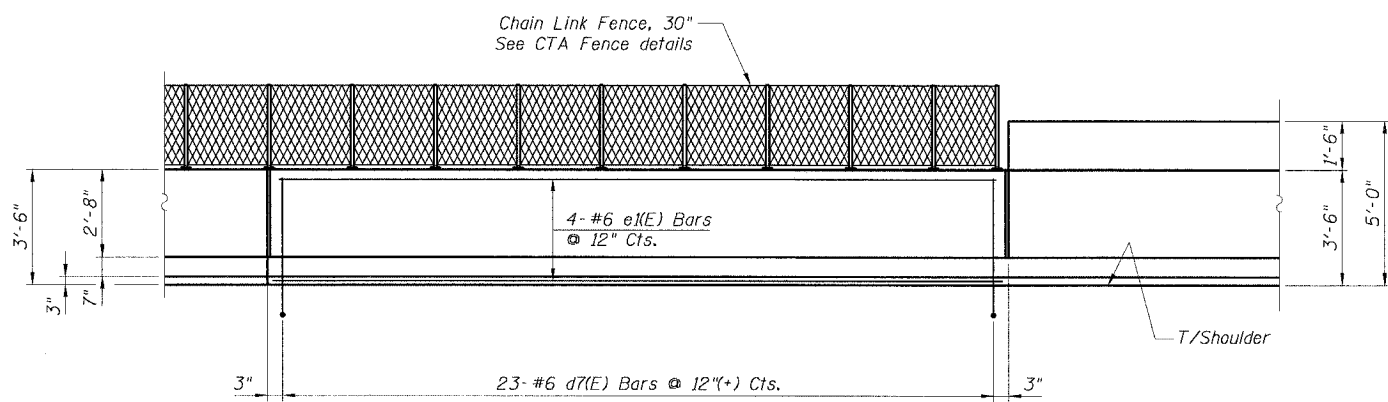
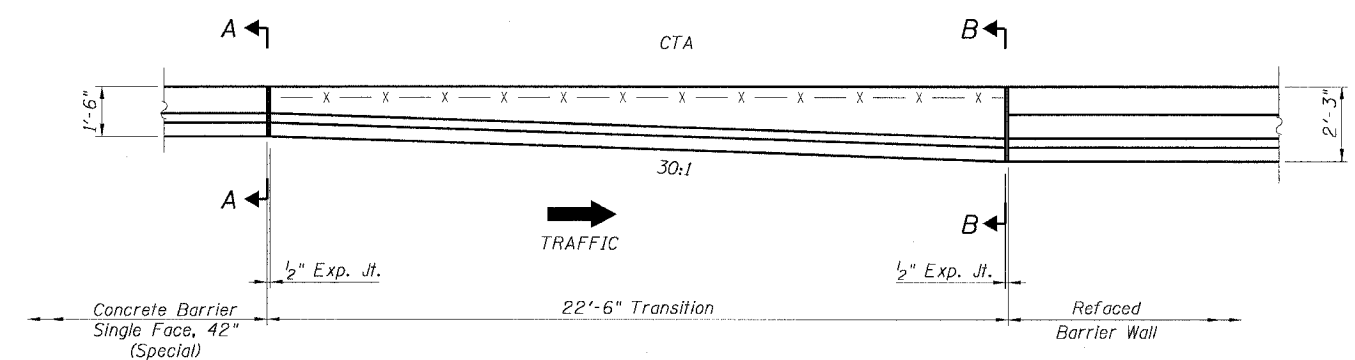
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DATE: July 7, 2005 CHECKED BY: RDP

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RT.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	*	COOK	556	383
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		
* (181B, ETC. 2324.6-1)PR-B			62300	

**BILL OF MATERIAL
FOR ONE TRANSITION**

Bar	No.	Size	Length	Shape
a(E)	3	#5	22'-2"	
d7(E)	23	#6	6'-9"	
e(E)	4	#6	22'-2"	
Item	Unit	Quantity		
Reinforcement Bars, Epoxy Coated	Pound	436		
Concrete Superstructure	Cu. Yd.	4.2		
Protective Coat	Sq. Yd.	12		



NOTES:

1. For Expansion Joint Details, see Sheet B6.
2. For alternative vertical reinforcing details, see "Concrete Barrier Single Face, 42" Alternatives".
3. For d7(E) bar details, see Sheet B14.
4. For Refaced Barrier Wall details, see Sheets B4 and B5.
5. For Soldier Pile and Lagging Retention System, see Sheets B1, B4 and B5.
6. For Concrete Barrier Single Face, 42" (Special), see Roadway details.
7. For locations of Transition, see Roadway Plans.

CTE | AECOM

REVISIONS	
NAME	DATE
MJP	8-12-05

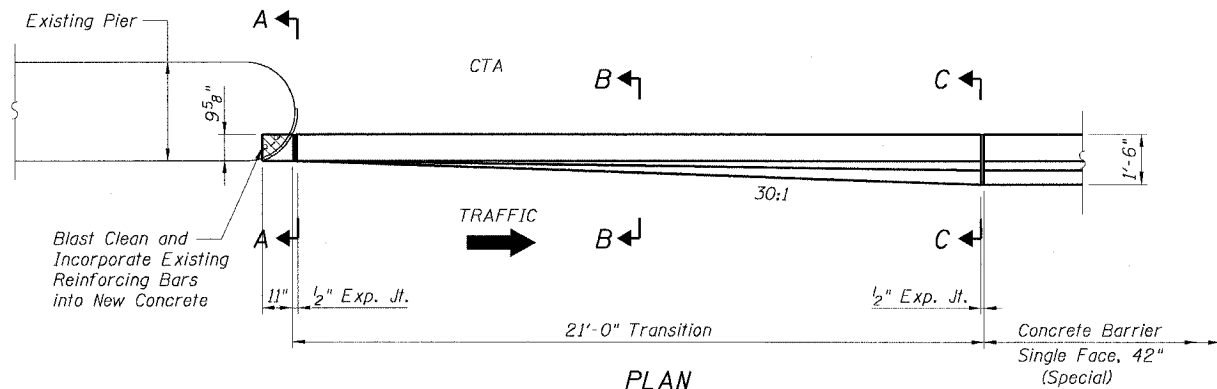
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
71ST STREET TO 31ST STREET (NB EXPRESS LANES)
EXISTING BARRIER WALL MODIFICATIONS
AT CTA STATIONS
PROPOSED T/S SHOULDER AT OR ABOVE EXISTING
REFACED BARRIER TO NEW 42" BARRIER
TRANSITION DETAILS
SCALE: None DRAWN BY: CHD
DATE: July 7, 2005 CHECKED BY: RDP

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

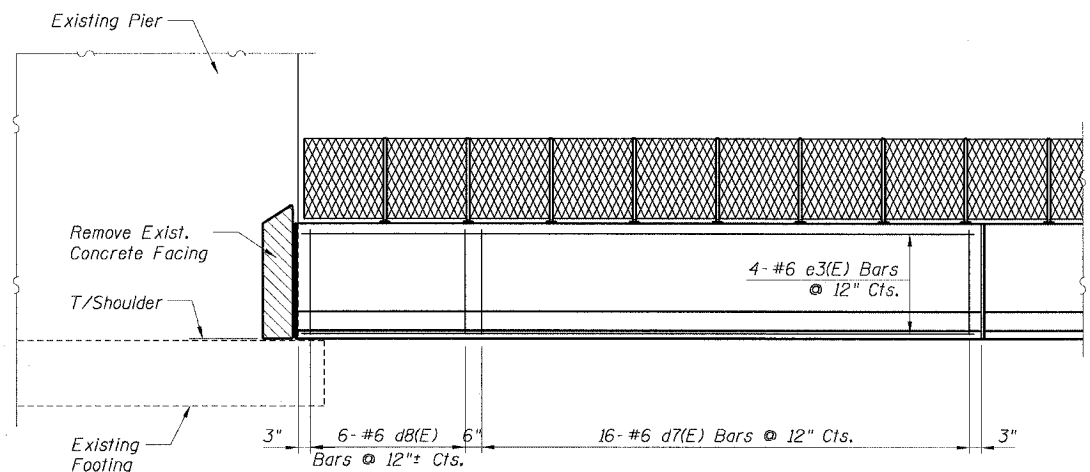
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94		COOK	556	384
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		
(181B, ETC, 2324.6-1PJR-8			62300	

**BILL OF MATERIAL
FOR ONE TRANSITION**

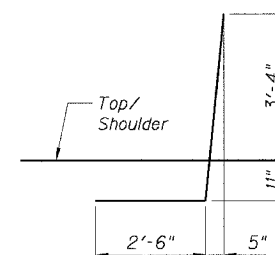
Bar	No.	Size	Length	Shape
a3(E)	3	#5	20'-8"	—
d7(E)	15	#6	6'-9"	—
d8(E)	5	#6	6'-9"	—
e3(E)	4	#6	20'-8"	—
Item	Unit	Quantity		
Reinforcement Bars, Epoxy Coated	Pound	392		
Concrete Superstructure	Cu. Yd.	2.6		
Protective Coat	Sq. Yd.	10		
Concrete Removal	Cu. Yd.	0.1		



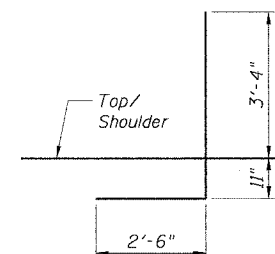
PLAN



ELEVATION



d7(E) BAR



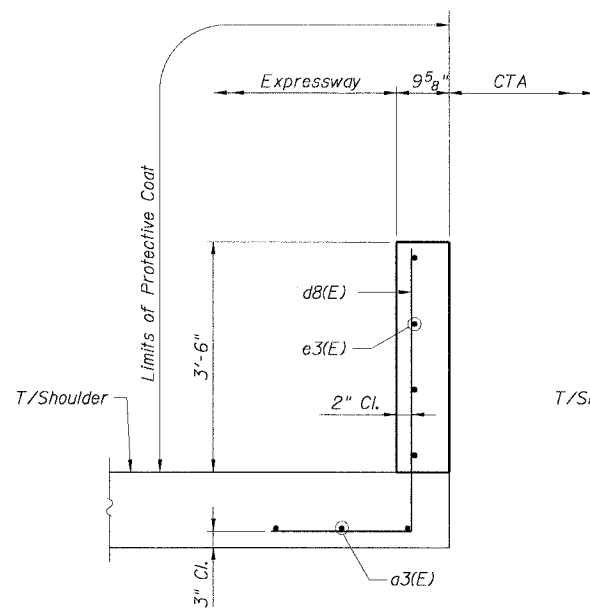
d8(E) BAR

LEGEND:

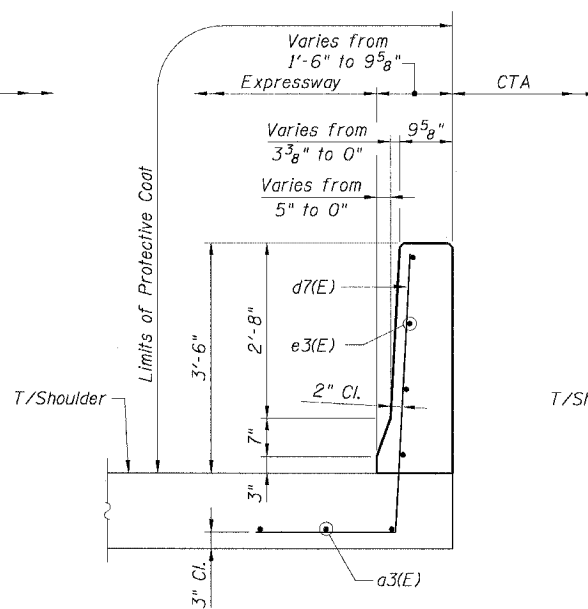
Concrete Removal

NOTES:

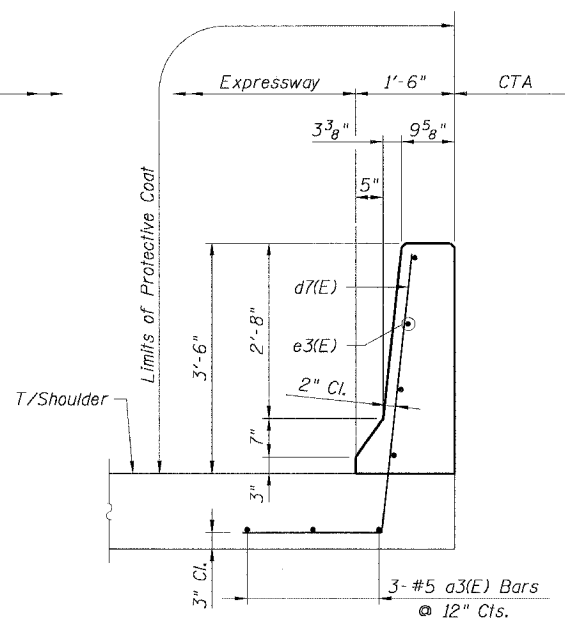
1. For Expansion Joint Details see Sheet B6.
2. For alternative vertical reinforcing details see "Concrete Barrier Single Face, 42" Alternatives".
3. For Concrete Barrier Single Face, 42" (Special), see Roadway details.
4. For locations of Transition, see Roadway Plans.



SECTION A-A



SECTION B-B



SECTION C-C

CTE | AECOM

REVISIONS	
NAME	DATE
MJP	8-12-05

Sheet B16 of 16
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)
71ST STREET TO 31ST STREET (NB EXPRESS LANES)
EXISTING BARRIER WALL MODIFICATIONS
AT CTA STATIONS
PROP. T/S SHOULDER ABOVE OR BELOW EXISTING
NEW 42" BARRIER TO PIER
TRANSITION DETAILS
SCALE: None DRAWN BY: CHD
DATE: July 7, 2005 CHECKED BY: RDP

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90	*	COOK	556	433
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
62300		(1818, ETC., 2324.6-1PR-8)		

Benchmark: BM No. 3540

"□" Cut on the Southwest corner of traffic control box at the Northeast corner of 39th and LaSalle Streets. El. 12.33

Existing Structure: The existing structure was built in 1962 under Section S-2323.2-4B and is identified as S.N. 016-2625. The structure is a 12'-0" clear span by 14'-0" clear height double barrel reinforced concrete box culvert. It conveys sanitary and storm water under the Dan Ryan Expressway for the MWRDGC. The length of culvert under the expressway is 325'-0" and the culvert transitions horizontally, vertically and in cross section to connect to an existing circular brick sewer under the centerline of 39th Street. The top slab of the culvert is the expressway pavement and consists of a minimum of 1'-3" of reinforced concrete with an approximately 5" thick overlay (either concrete or bituminous). The centerline of the culvert has no skew relative to the Dan Ryan baselines. There are 20'-6" long approach slabs on each side of the culvert over the top of the traffic lanes. Barrier wall is attached to the top slab and separates the express and local lanes of the expressway and the expressway from the CTA tracks. The barrier is supported by PCC shoulders outside the limits of the culvert. The culvert will be rehabilitated utilizing stage construction. Portions of the top slab and all components of the culvert below grade shall be re-used.

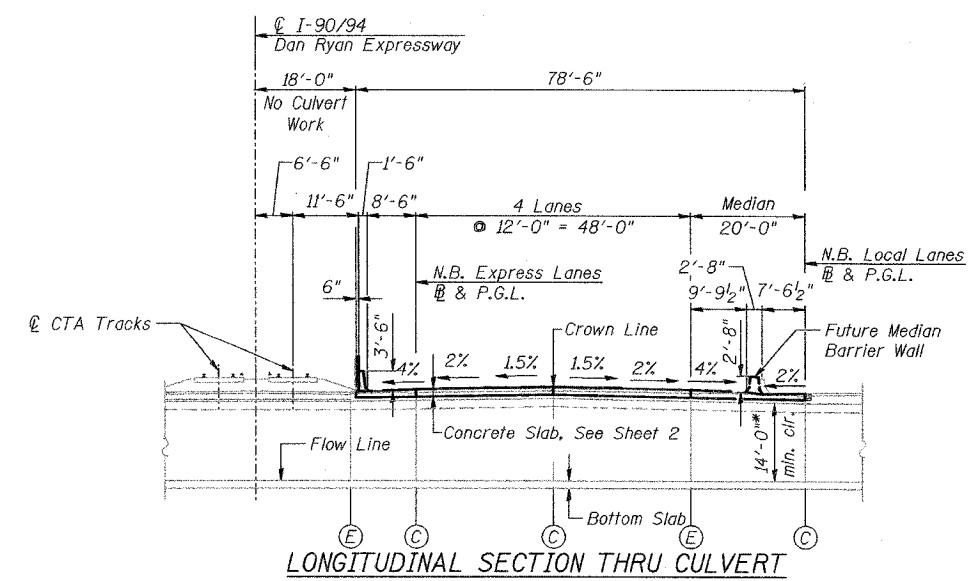
All elevations are based on the Chicago City Datum (C.C.D.)

GENERAL NOTES

Reinforcement bars shall conform to the requirements of AASHTO M 31 or M 322 Grade 60.

Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

All construction joints shall be bonded.



LONGITUDINAL SECTION THRU CULVERT

* No work will encroach inside the culvert or change existing opening
 *** Jacking pits and Junction chambers for proposed storm sewer pushes under CTA tracks. See Drainage Details.

TOTAL BILL OF MATERIAL

ITEM	UNIT	QTY.
Bridge Approach Pavement	Sq. Yd.	456
Bituminous Surface Removal, 5"	Sq. Yd.	143
Bituminous Surface Removal (Variable Depth)	Sq. Yd.	82
Approach Slab Removal	Sq. Yd.	219
Concrete Superstructure	Cu. Yd.	112.2
Bridge Deck Grooving	Sq. Yd.	218
Protective Coat	Sq. Yd.	265
Reinforcement Bars, Epoxy Coated	Pound	23,450
Bar Splicers	Each	257
Concrete Slab Hydro-Demolition	Sq. Yd.	237
CTA Fence	Foot	87
Video Taping of MWRD Culvert	Foot	245
Temporary CTA Ballast Retention	L. Sum	!

DESIGN SPECIFICATIONS

2002 AASHTO

LOADING HS20-44 & ALT.

Allow 50#/sq. ft. for future wearing surface.

DESIGN STRESSES

FIELD UNITS

New:
 f'c = 3,500 psi
 fy = 60,000 psi (Reinforcement)

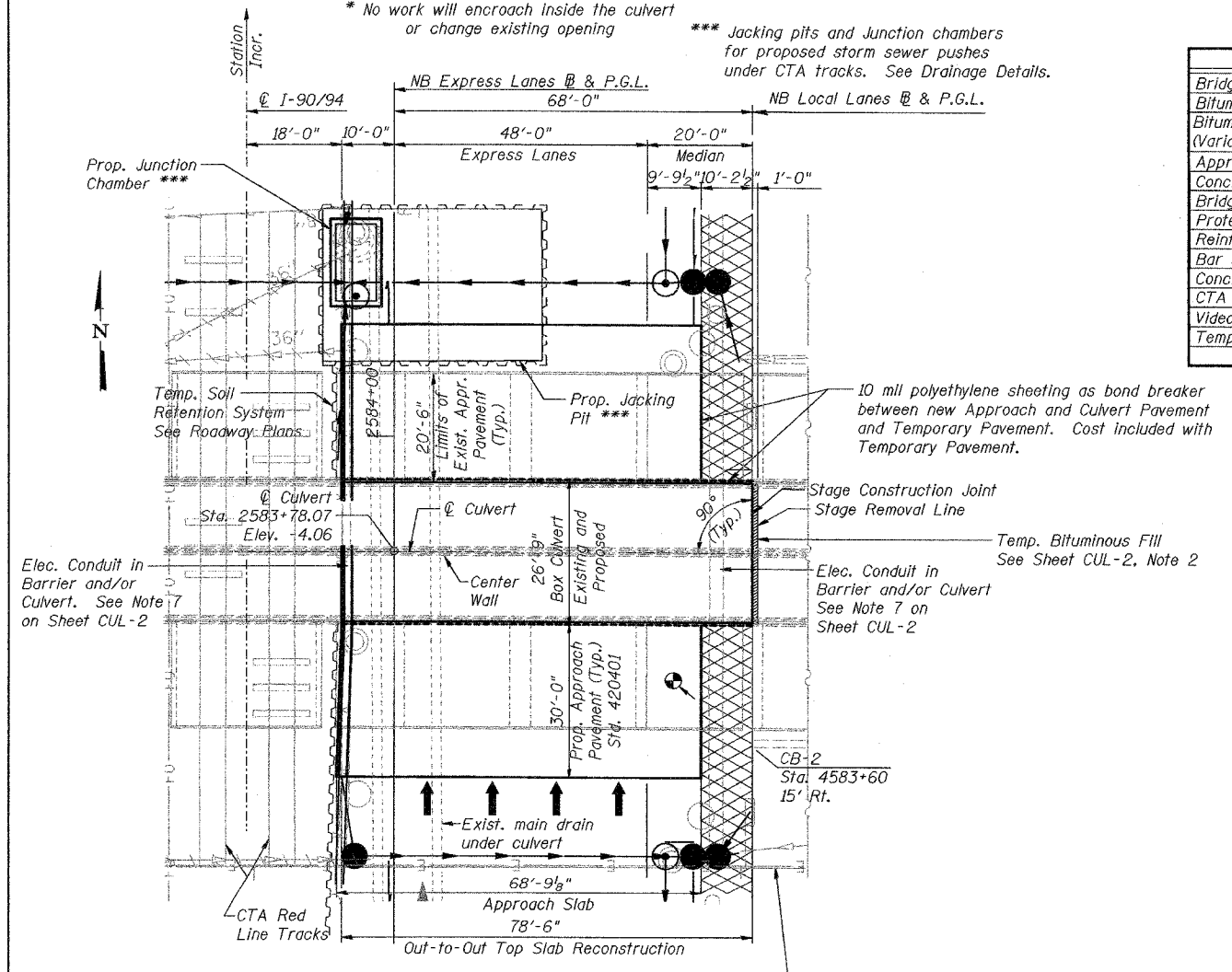
Existing:
 f'c = 3,500 psi
 fy = 40,000 psi (Reinforcement)

LEGEND

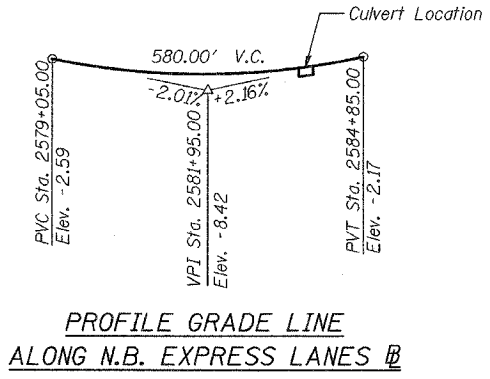
- CB-2 = Soil Boring Location
- = Exist. Catch basin
- ⊙ = Exist. Manhole
- = Exist. Inlet
- = Exist. Storm sewer
- = Exist. Underdrain
- = Prop. Catch basin
- ⊙ = Prop. Manhole
- = Prop. Storm sewer
- = Prop. Underdrain
- ▨ = Temporary Pavement
- ▩ = Temporary Bituminous Fill

ABBREVIATIONS

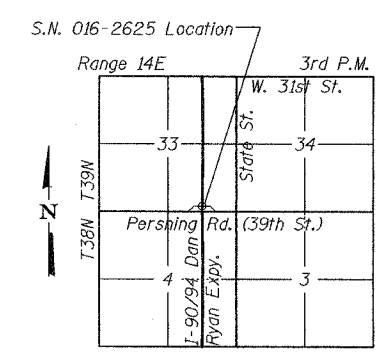
- MWRDGC = Metropolitan Water Reclamation or MWRD District of Greater Chicago
- CTA = Chicago Transit Authority
- E = Expansion Joint
- C = Construction Joint
- P.G.L. = Profile Grade Line
- NB = Northbound
- SB = Southbound



PARTIAL PLAN



PROFILE GRADE LINE ALONG N.B. EXPRESS LANES



LOCATION SKETCH

BOWMAN, BARRETT & ASSOCIATES INC.
 CONSULTING ENGINEERS
 Chicago, Illinois
 312.228.0100
 www.bbainc.com

APPROVED
 FOR STRUCTURAL ADEQUACY ONLY

Ralph C. Adams
 ENGINEER OF BRIDGES AND STRUCTURES

Brian L. Umbright
 Date: July 7, 2005
 License
 Expires: November 30, 2006

REVISIONS	NAME	DATE
ADDENDUM 1		8/12/05

SHEET CUL-1 OF CUL-14

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94/90 (DAN RYAN EXPRESSWAY)
 31ST STREET TO 71ST STREET
 NB EXPRESS LANE RECONSTRUCTION
 MWRD CULVERT, N. OF 39TH ST.,
 COOK COUNTY, S.N. 016-2625
 GENERAL PLAN

SCALE: N.T.S. DRAWN BY: DJM
 DATE: 07/07/05 CHECKED BY: BLU

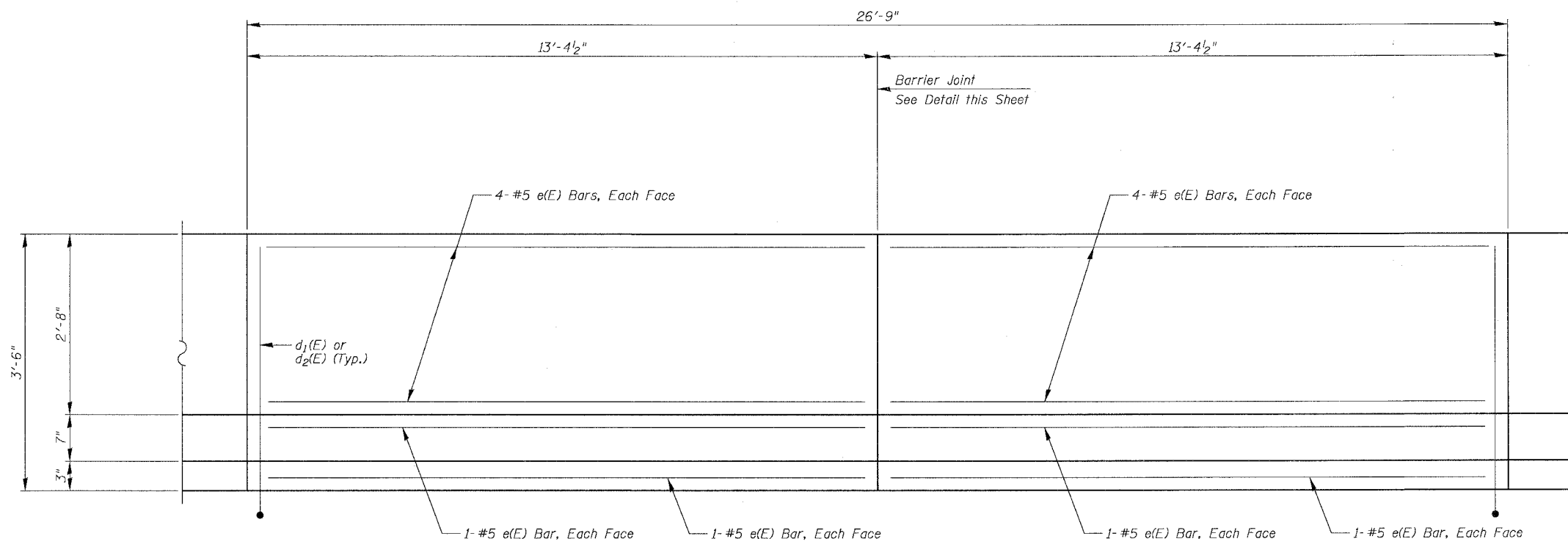
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F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90		COOK	556	437
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
62300	*181B, ETC, 2324.6-1P1R-8			

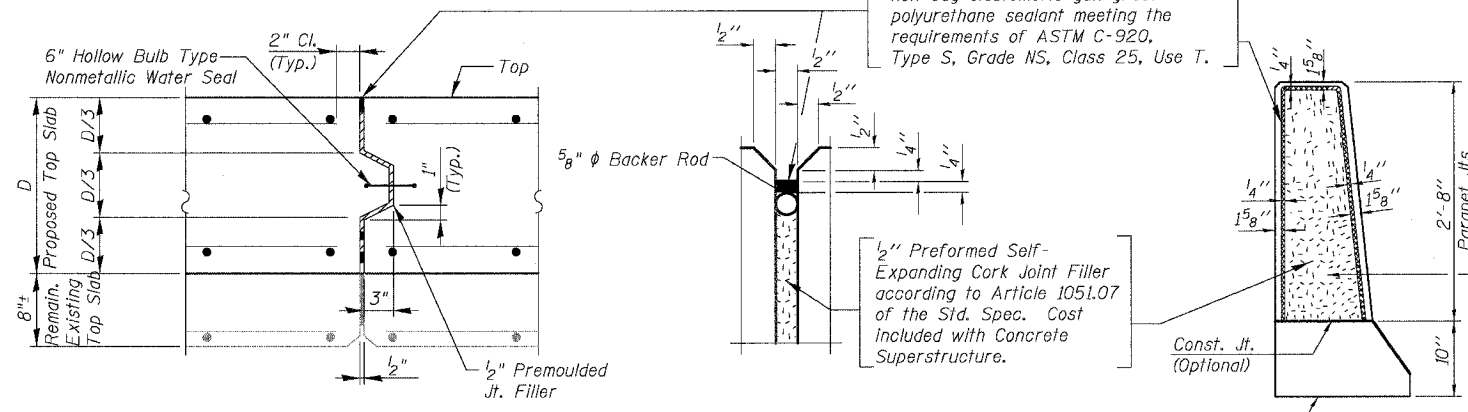
**TOP SLAB
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	27	#5	10'-2"	—
a ₁ (E)	27	#5	23'-8"	—
a ₂ (E)	27	#5	21'-10"	—
a ₃ (E)	29	#5	10'-2"	—
a ₄ (E)	29	#5	23'-8"	—
a ₅ (E)	29	#5	19'-8"	—
a ₆ (E)	27	#6	4'-6"	—
b(E)	79	#7	26'-5"	—
b ₁ (E)	79	#7	8'-0"	—
b ₂ (E)	157	#7	28'-1"	—
b ₃ (E)	79	#7	4'-0"	—
b ₄ (E)	5	#5	26'-5"	—
d ₁ (E)	27	#6	5'-7"	L
d ₂ (E)	27	#5	5'-9"	L
e(E)	24	#5	13'-0"	—
Concrete Superstructure			Cu. Yd.	104.1
Bridge Deck Grooving			Sq. Yd.	812
Protective Coat			Sq. Yd.	235
Reinforcement Bars, Epoxy Coated			Pound	19,260
Bar Splicers			Each	133
CTA Fence			Foot	27

Reinforcement bars designated (E) shall be epoxy coated.
CTA Fence not shown for clarity. See Roadway Plans for Fence details.
Cost of water seal, P.J.F. and Sealant included in the cost of Concrete Superstructure.

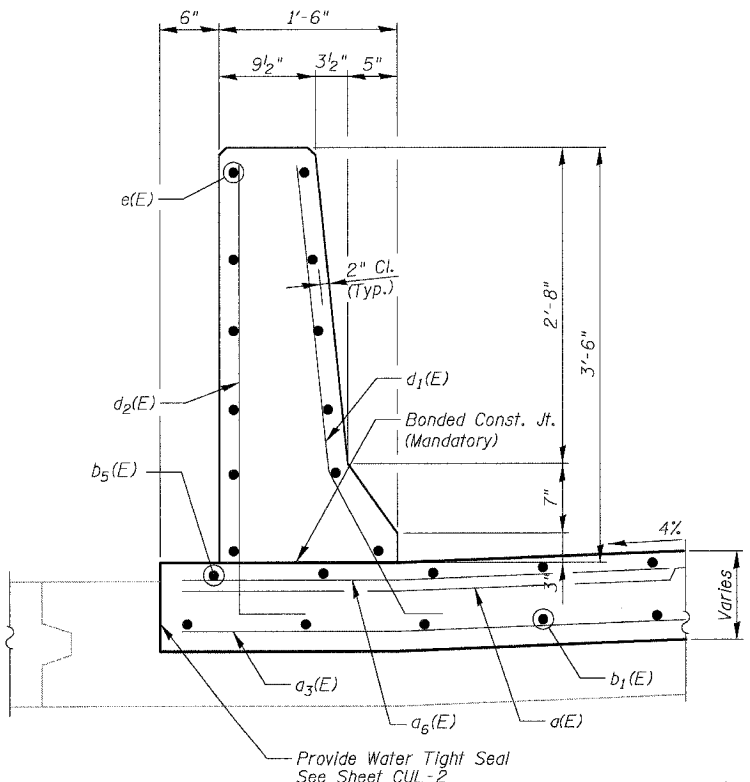
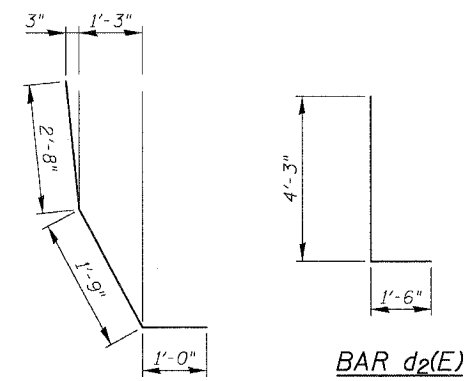


INSIDE ELEVATION OF SINGLE FACE BARRIER

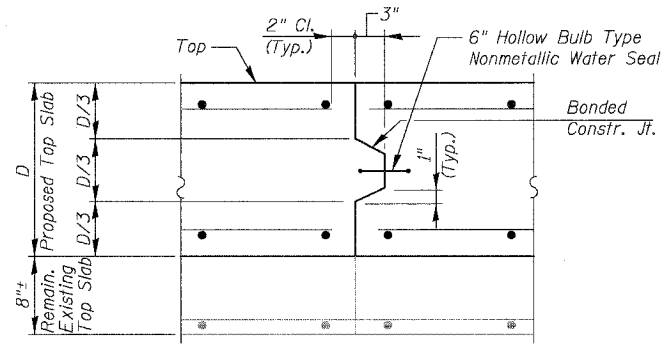


CULVERT EXPANSION JOINT
Do not seal until after grooving operations

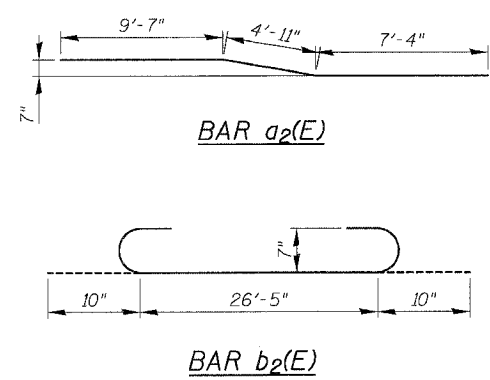
BARRIER JOINT DETAILS



SECTION THRU SINGLE FACE BARRIER



CULVERT CONSTRUCTION JOINT



REVISIONS	
NAME	DATE
ADDENDUM 1	8/12/05

SHEET CUL-5 OF CUL-14
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94/90 (DAN RYAN EXPRESSWAY)
31ST STREET TO 71ST STREET
NB EXPRESS LANE RECONSTRUCTION
MWRD CULVERT, N. OF 39TH ST.,
COOK COUNTY, S.N. 016-2625
TOP SLAB DETAILS
SCALE: N.T.S. DRAWN BY: MTR
DATE: 07/07/05 CHECKED BY: BLU

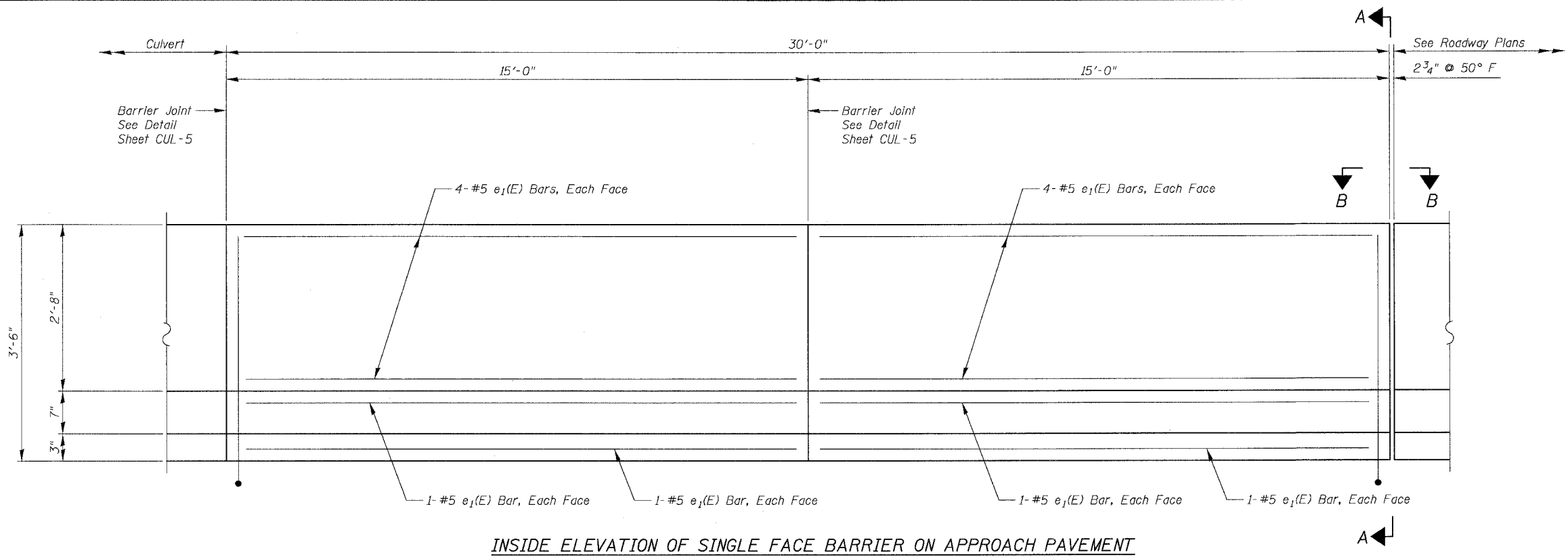
BOWMAN, BARRETT & ASSOCIATES INC.
CONSULTING ENGINEERS
Chicago, Illinois
312.228.0100
www.bbainc.com

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90	*	COOK	556	439
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
62300		*0818, ETC, 2324.6-1P1R-8		

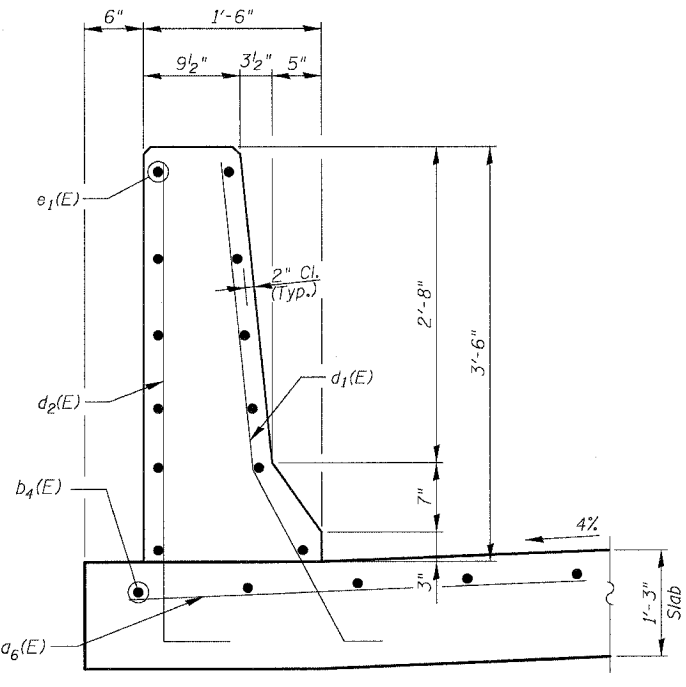
**APPROACH PAVEMENT
BILL OF MATERIAL
TOTAL FOR 2 APPROACH SLABS**

Bar	No.	Size	Length	Shape
$a_6(E)$	42	#6	4'-6"	—
$b_5(E)$	10	#5	29'-6"	—
$d_1(E)$	60	#6	5'-7"	L
$d_2(E)$	60	#5	5'-9"	L
$e(E)$	48	#5	14'-8"	—
Bridge Approach Pavement			Sq. Yd.	456
Concrete Superstructure			Cu. Yd.	8.1
Protective Coat			Sq. Yd.	30
Reinforcement Bars, Epoxy Coated			Pound	2,190
Bar Splicers			Each	124
CTA Fence			Foot	60

See IDOT Standard 420401 for typical approach pavement reinforcing and details. Additional reinforcement bars, bar splicers, and concrete for single face barrier not shown on Std. 420401 shall be paid for as Reinforcement Bars, Epoxy Coated, Bar Splicers, and Concrete Superstructure. Reinforcement bars designated (E) shall be epoxy coated. CTA Fence not shown for clarity. See Roadway Plans for fence details. See Sheet CUL-4 for treatment of Approach Slab/Culvert Slab joint. Apply Protective Coat to top and inside face of single face barrier.

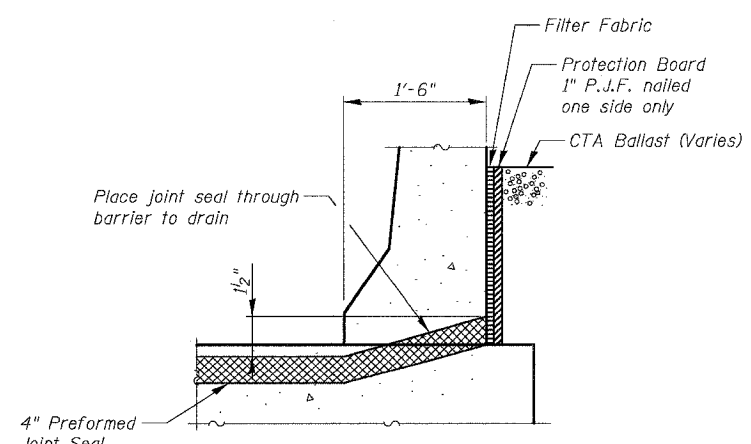


INSIDE ELEVATION OF SINGLE FACE BARRIER ON APPROACH PAVEMENT

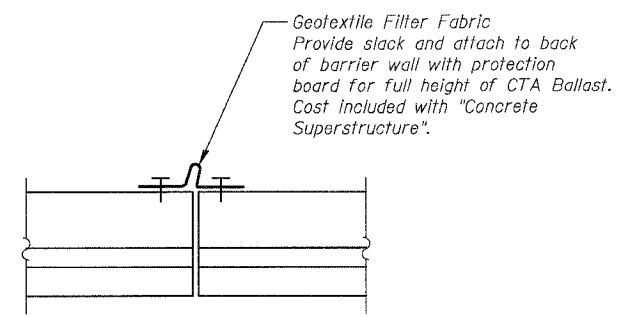


SECTION THRU SINGLE FACE BARRIER

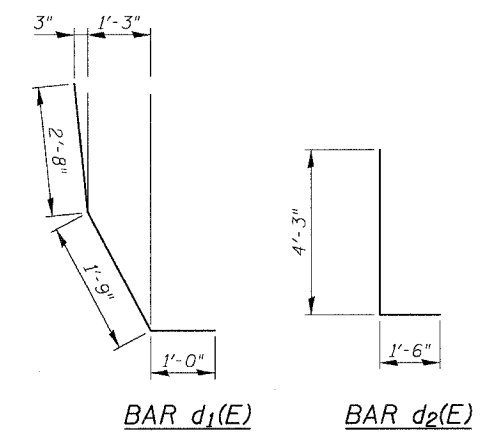
Note: Standard Approach Pavement reinforcement not shown for clarity



SECTION A-A



VIEW B-B



BAR $d_1(E)$

BAR $d_2(E)$

BOWMAN, BARRETT & ASSOCIATES INC.
CONSULTING ENGINEERS
Chicago, Illinois
312.228.0100
www.bbandainc.com

REVISIONS	
NAME	DATE
ADDENDUM 1	8/12/05

SHEET CUL-7 OF CUL-14
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94/90 (DAN RYAN EXPRESSWAY)
31ST STREET TO 71ST STREET
NB EXPRESS LANE RECONSTRUCTION
MWRD CULVERT, N. OF 39TH ST.,
COOK COUNTY, S.N. 016-2625
APPROACH SLAB DETAILS 2
SCALE: N.T.S. DRAWN BY: MTR
DATE: 07/07/05 CHECKED BY: BLU

8/17/2005 8:36:04 AM

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity (Tension in kips) = $1.25 \times f_y \times A_t$
- ② Minimum *Pull-out Strength (Tension in kips) = $1.25 \times f_{s_{allow}} \times A_t$

Where f_y = Yield strength of lapped reinforcement bars in ksi.
 $f_{s_{allow}}$ = Allowable tensile stress in lapped reinforcement bars in ksi (Service Load)
 A_t = Tensile stress area of lapped reinforcement bars.
 * = 28 day concrete

BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	5.9
#5	2'-0"	23.0	9.2
#6	2'-7"	33.1	13.3
#7	3'-5"	45.1	18.0
#8	4'-6"	58.9	23.6
#9	5'-9"	75.0	30.0
#10	7'-3"	95.0	38.0
#11	9'-0"	117.4	46.8

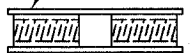
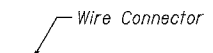
Bar splicer assemblies shall be according to Section 508 of the Standard Specifications, except as noted. The furnishing and installation of bar splicer assemblies will be measured and paid for at the contract unit price each for "BAR SPLICERS."

The diameter of this part is the same as the diameter of the bar spliced.

ROLLED THREAD DOWEL BAR



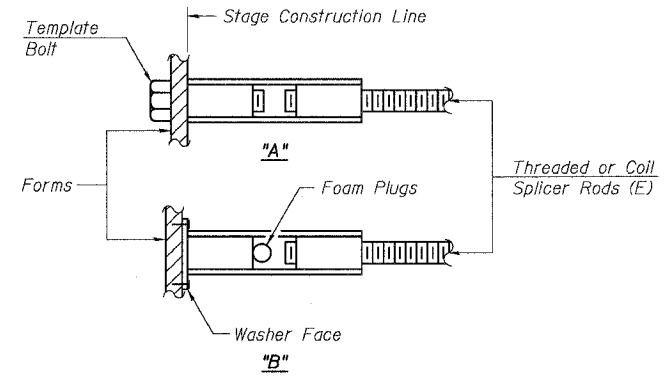
** ONE PIECE



WELDED SECTIONS

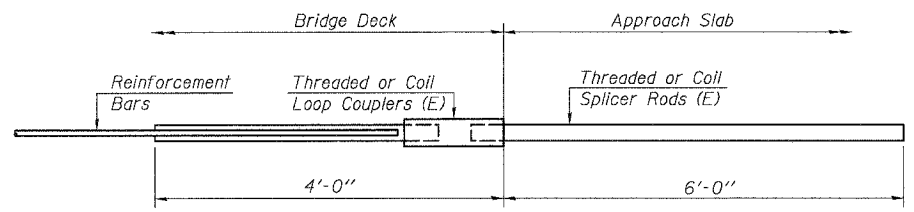
BAR SPLICER ASSEMBLY ALTERNATIVES

** Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



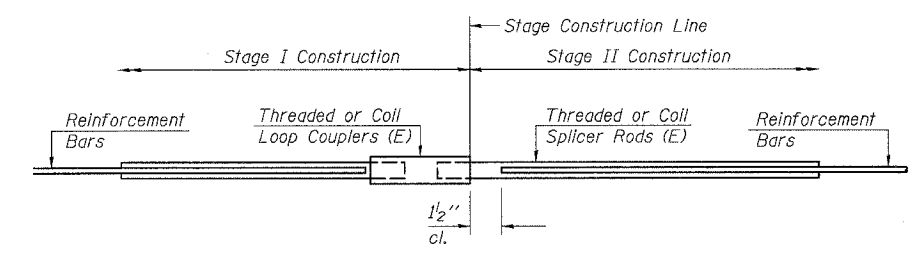
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



FOR CULVERT SLAB

Bar Splicer for #7 bar
Min. Capacity = 45.1 kips - tension
Min. Pull-out Strength = 18.0 kips-tension
No. Required = 79



STANDARD

Bar Size	No. Assemblies Required	Location
#5	54	Culvert Slab
#4	16	Approach Slab
#5	60	Approach Slab
#5	48	Approach Sleeper Slab

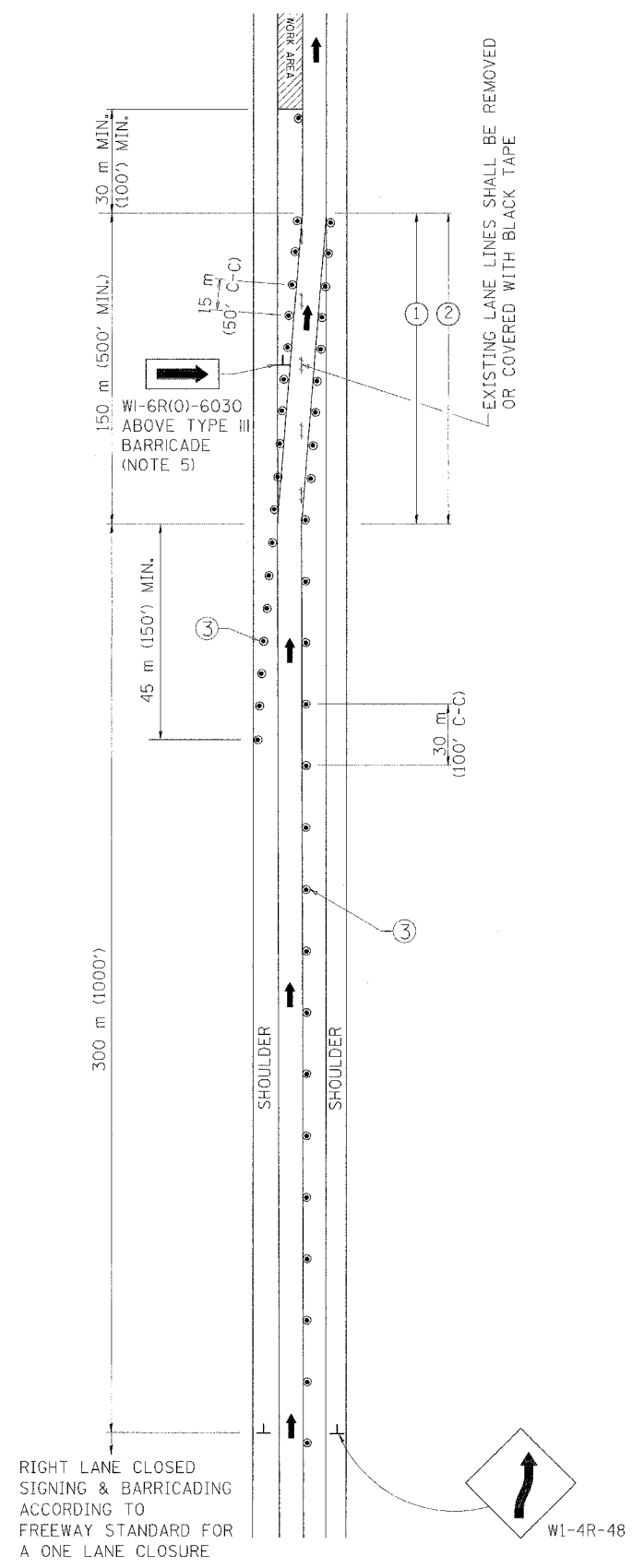
REVISIONS	
NAME	DATE
ADDENDUM 1	8/12/05

SHEET CUL-8 OF CUL-14
 ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94/90 (DAN RYAN EXPRESSWAY)
 31ST STREET TO 71ST STREET
 NB EXPRESS LANE RECONSTRUCTION
 MWRD CULVERT, N. OF 39TH ST.,
 COOK COUNTY, S.N. 016-2625
BAR SPLICER DETAILS
 SCALE: N.T.S. DRAWN BY: MTR
 DATE: 07/07/05 CHECKED BY: BLU

8/11/2005

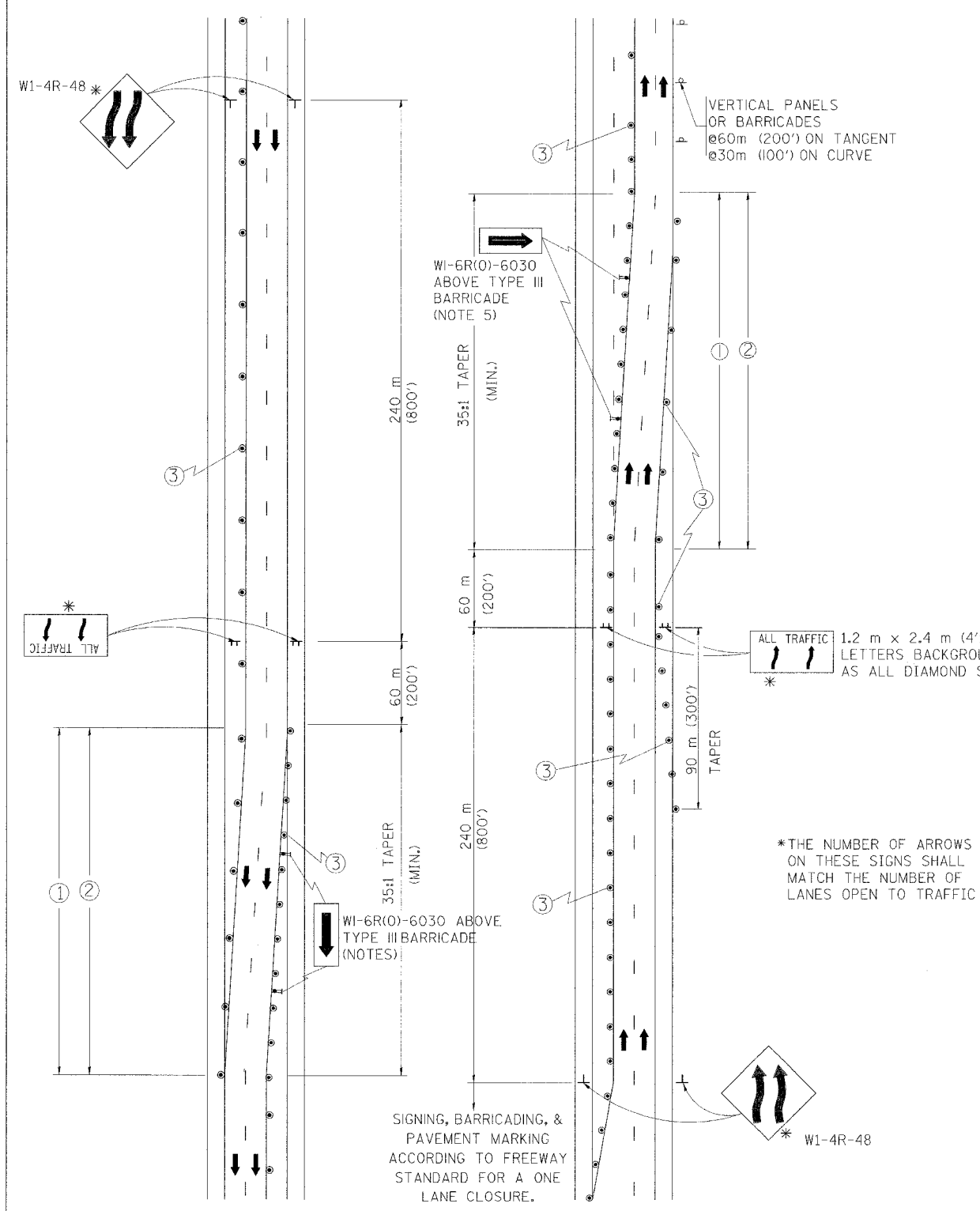
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			55	43
STA. _____ TO STA. _____		FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT _____		

SINGLE LANE WEAVE



RIGHT LANE CLOSED SIGNING & BARRICADING ACCORDING TO FREEWAY STANDARD FOR A ONE LANE CLOSURE

MULTI-LANE WEAVE



*THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC

GENERAL NOTES

- EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED OR COVERED WITH BLACK TAPE. PAVEMENT MARKING REMOVAL OR BLACK TAPE SHALL NOT BE REQUIRED FOR LANE CLOSURES UNDER 24 HOURS IN DURATION.
- CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 90 m (300') ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVE LANE LINES SHALL BE 3 M-9 M (10'-30') SKIP DASH, WHITE.
- PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 15 m (50') C-C SPACING IN TAPERS AND 30 m (100') C-C SPACING IN TANGENTS.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS DIRECTLY IN FRONT OF THE BARRICADE.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN

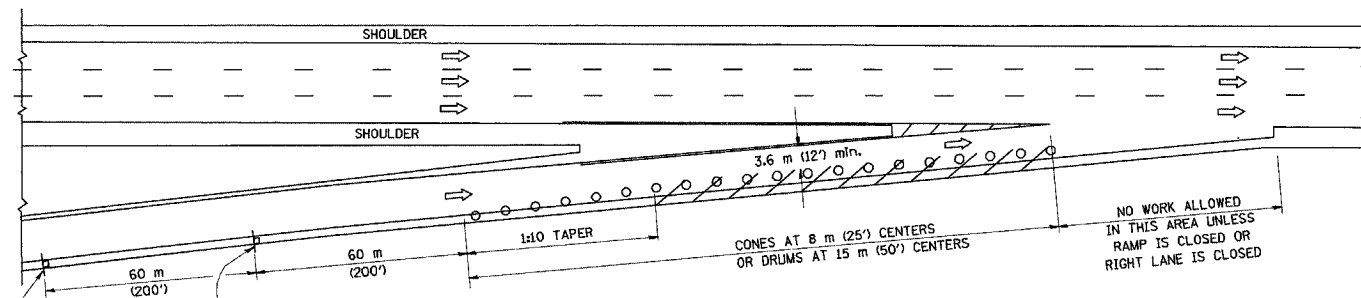
ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL DETAILS
 FOR FREEWAY
 SINGLE & MULTI-LANE WEAVE

REVISIONS	
NAME	DATE
DWS	2/87
DWS	1/90
DWS	12/27/94
DWS	11/96
JAF	4/03

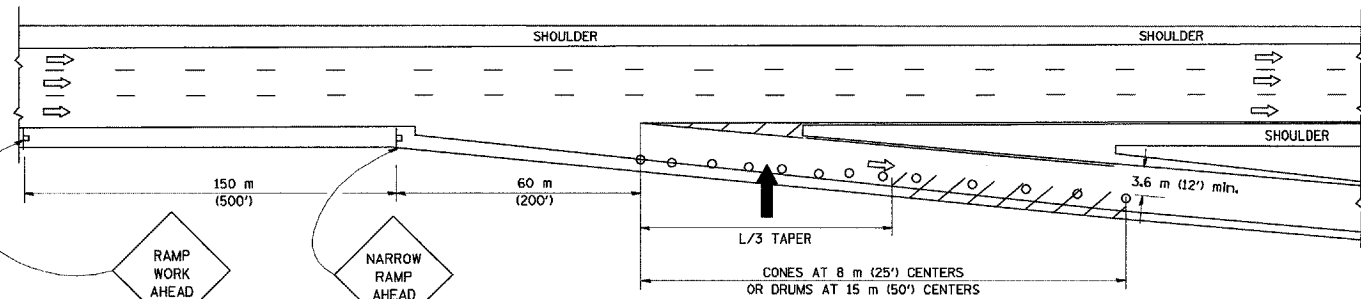
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 DRAWN BY R.H.
 CHECKED BY TC-9

Revised sheet 4/12/05

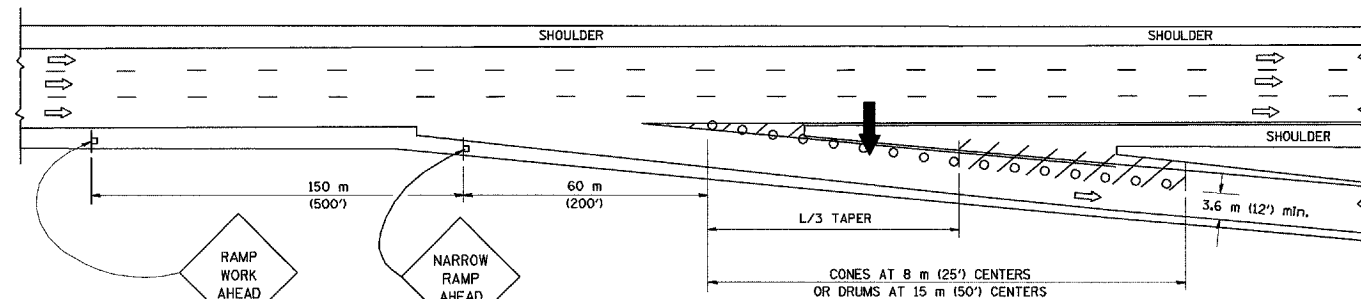
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

- ARROWBOARD
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- CONES - 700 (28) IN HEIGHT

GENERAL NOTES

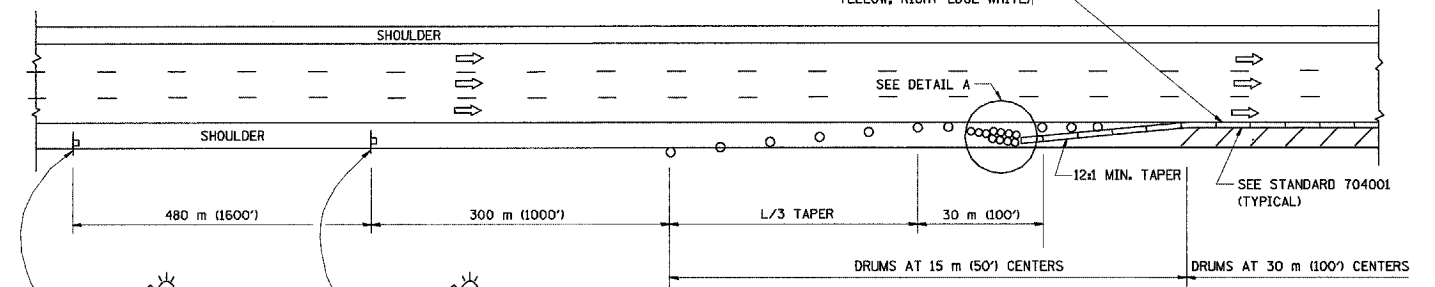
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SPEED LIMIT	METRIC	ENGLISH
80 km/h (45 mph)	$L=0.65(W)S$	$L=(W)S$

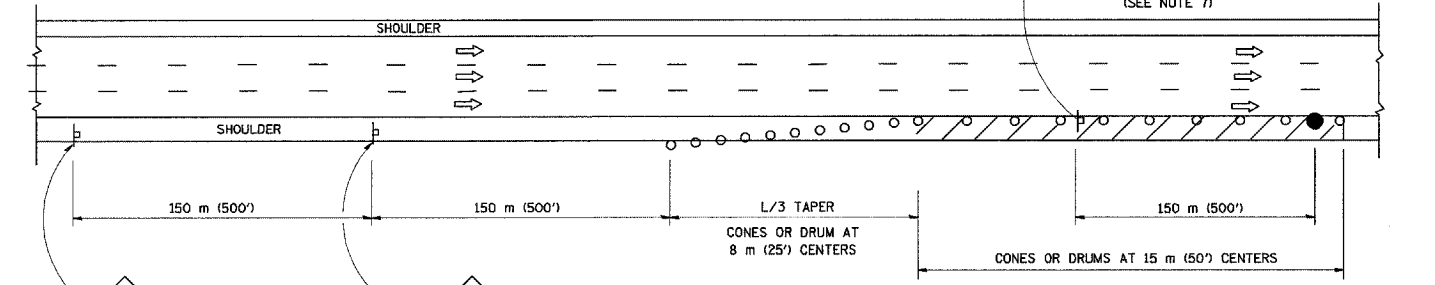
W = WIDTH OF OFFSET IN METERS (FEET)
S = NORMAL POSTED SPEED KM/H (MPH)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			556	457
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCRUCH IN AN AREA CLOSER THAN 4.5 m (15') TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.

ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350 COMPLIANT FOR POSTED SPEED.

DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

REVISIONS	
NAME	DATE
DWS	11/96
JAF	12/02
NCHRP 350	04/03

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES PARTIAL RAMP CLOSURES

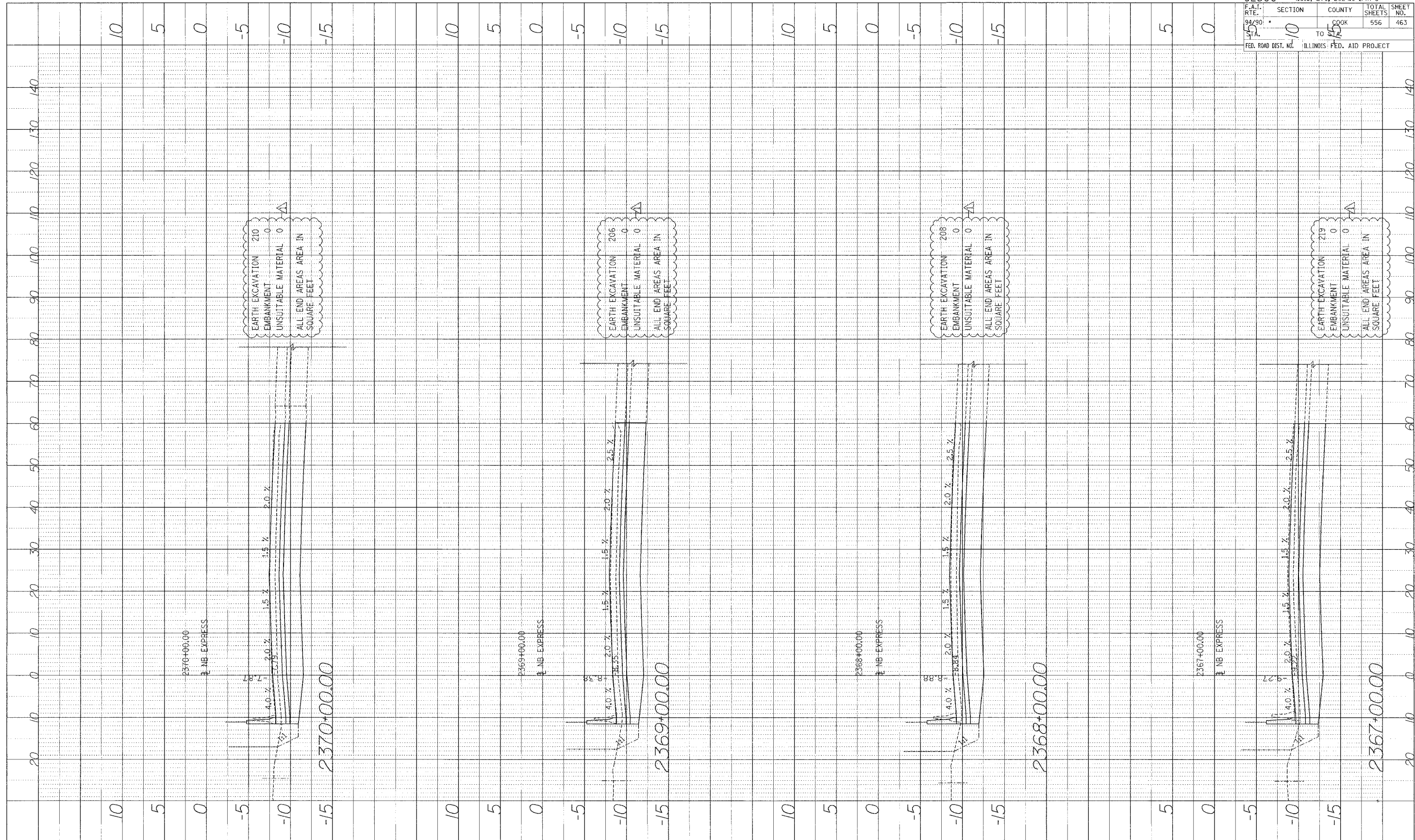
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CHECKED BY: TC-17
REVISION DATE: 04/24/03

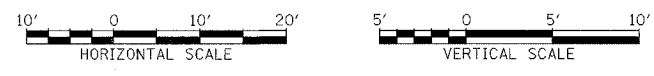
Revised sheet 8/12/05

PROFILE REVISIONS
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 NO. 32
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62300 • (1818, ETC, 2324.6-IPR-8	
F.A.I. RT.:	SECTION
94/90	10
TO	COOK
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT
TOTAL SHEETS	SHEET NO.
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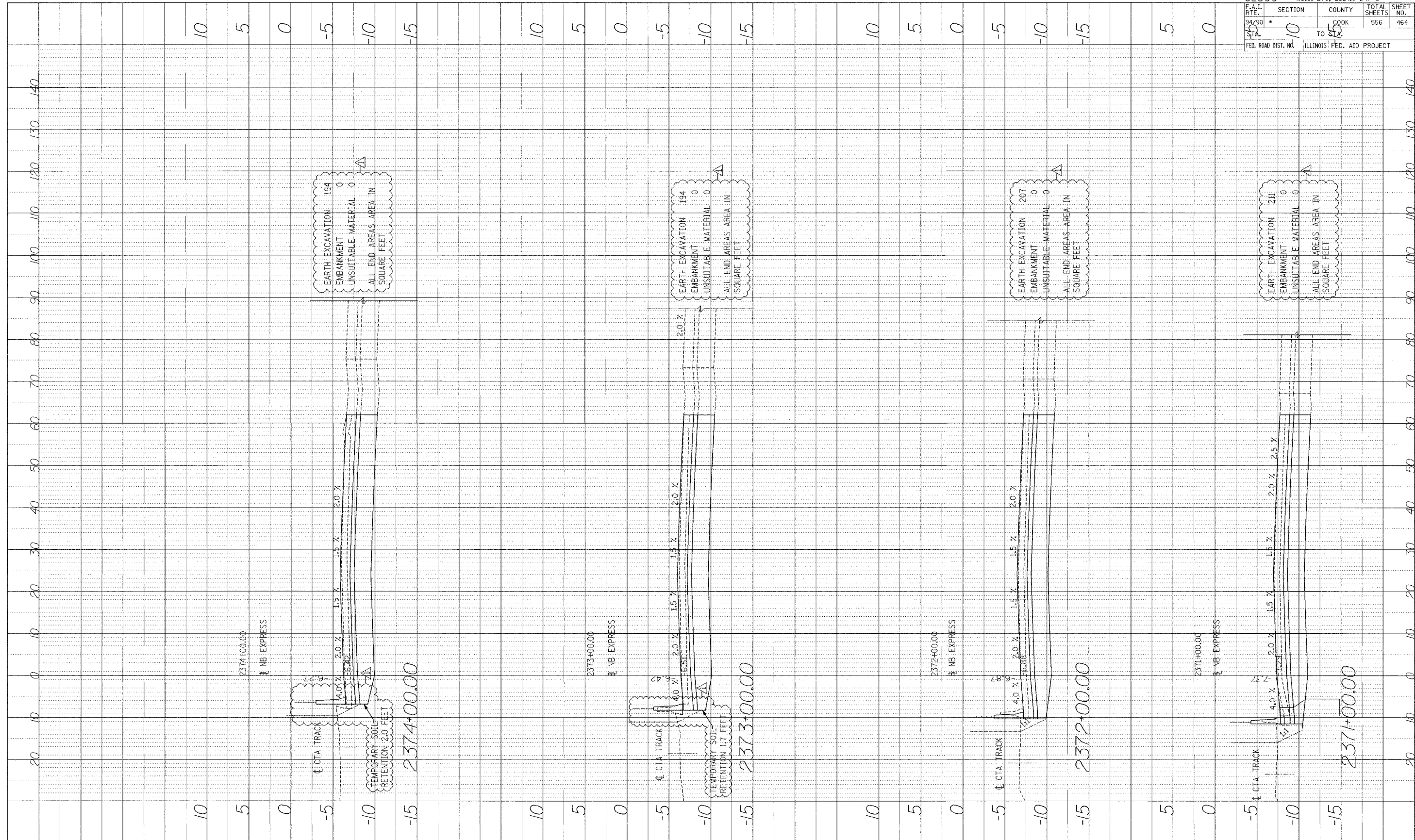
Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



STA.

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 DATE 11/11/11
 BY [Signature]
 CHECKED [Signature]
 NO. 11-11-11-11



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TO	FROM	NO.	NO.
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ILLINOIS FED. AID PROJECT			

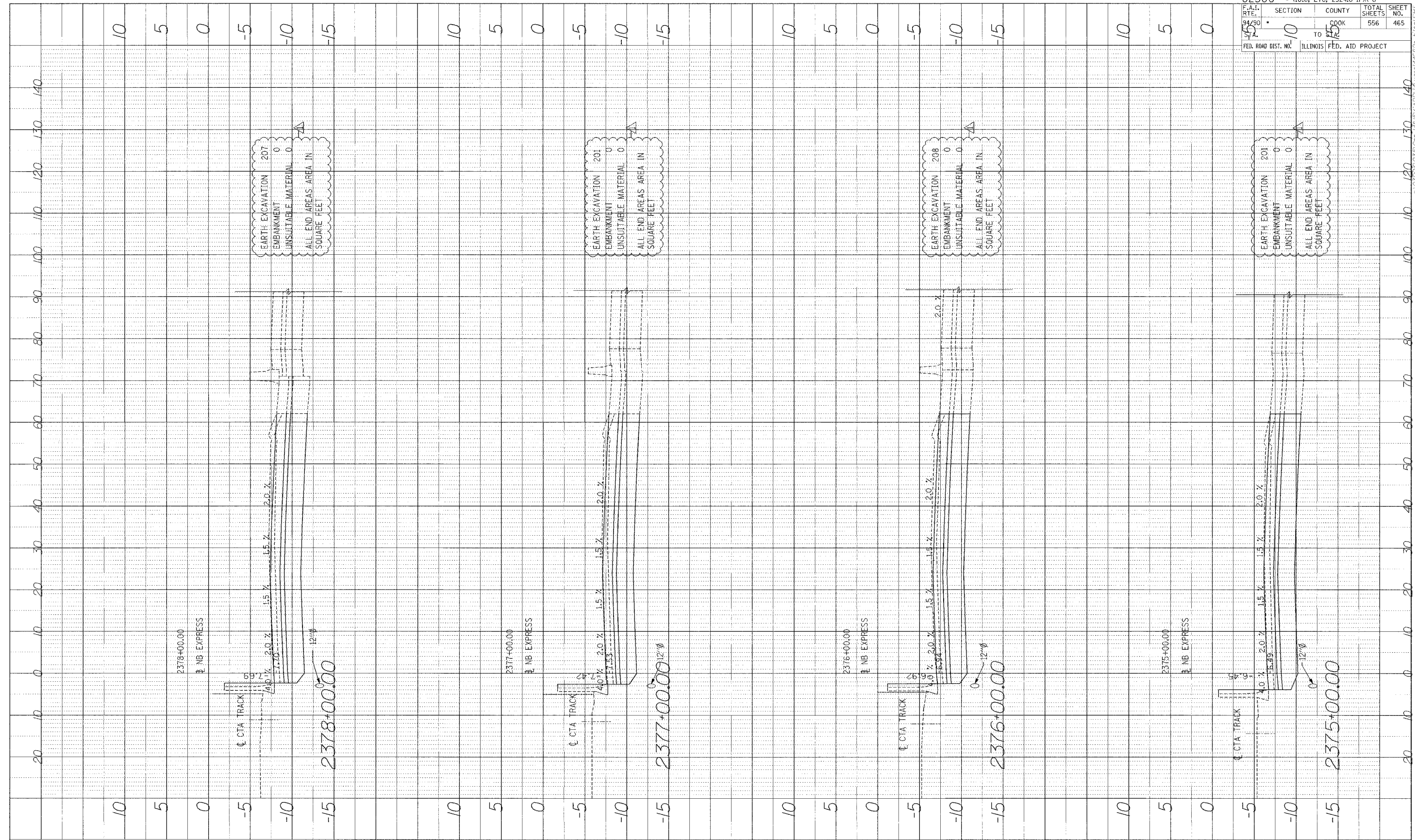
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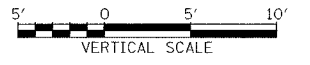
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 CHECKED BY
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 NO. OF SHEETS THIS SHEET



62300 • (1818, ETC, 2324.6-1P)R-8	
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COUNTY COOK	TOTAL SHEETS 556
FED. ROAD DIST. NO. ILLINOIS	FED. AID PROJECT

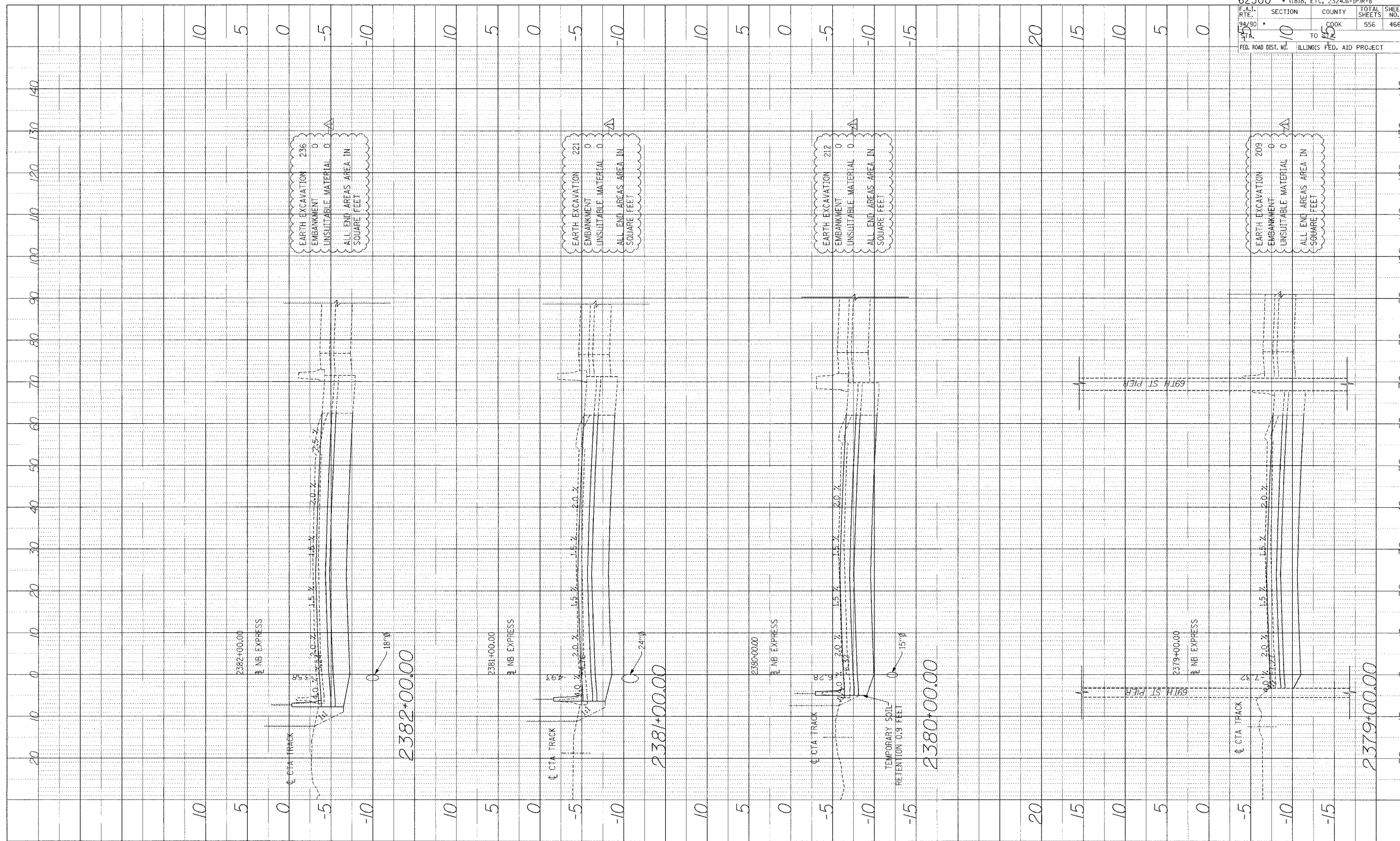
Edwards and Kelcey
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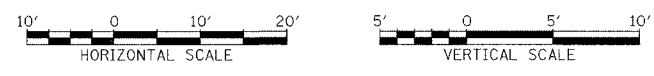
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 NO. S BOOK RECORDED
 NO. STRUCTURE NOTATION CHVD



62300 • (1818, ETC, 2324.6-1P1R-8	
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TO SHEET 45	COUNTY COOK
FED. ROAD DIST. NO. ILLINOIS	FED. AID PROJECT
TOTAL SHEETS 556	SHEET NO. 466

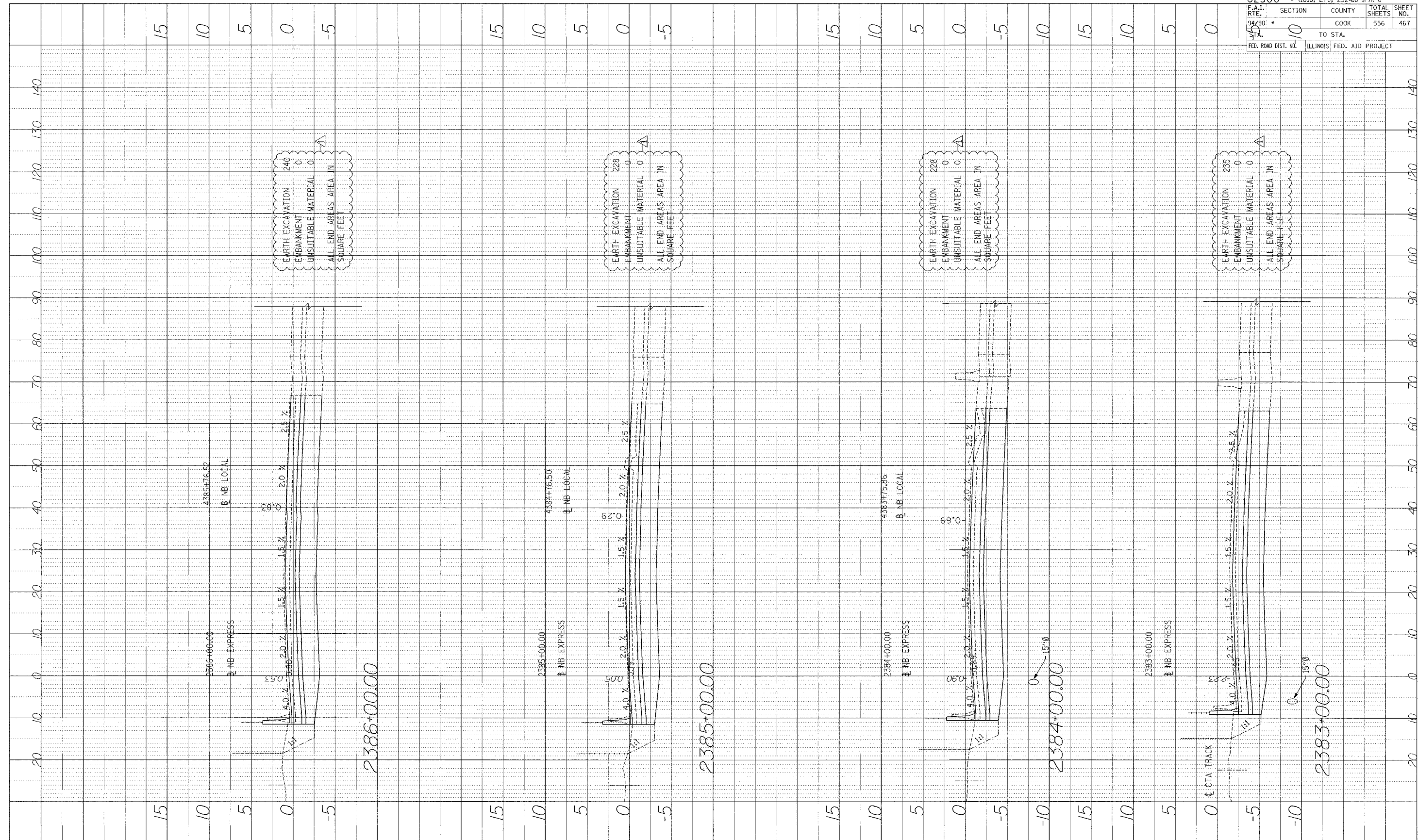
Edwards AND Kelcey
 ONE NORTH FRANKLIN CHICAGO, IL 60606
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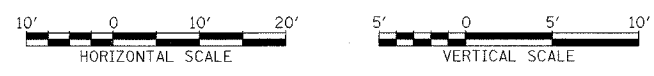
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 NO. DATE STRUCTURE NO. 1-NB-CPD



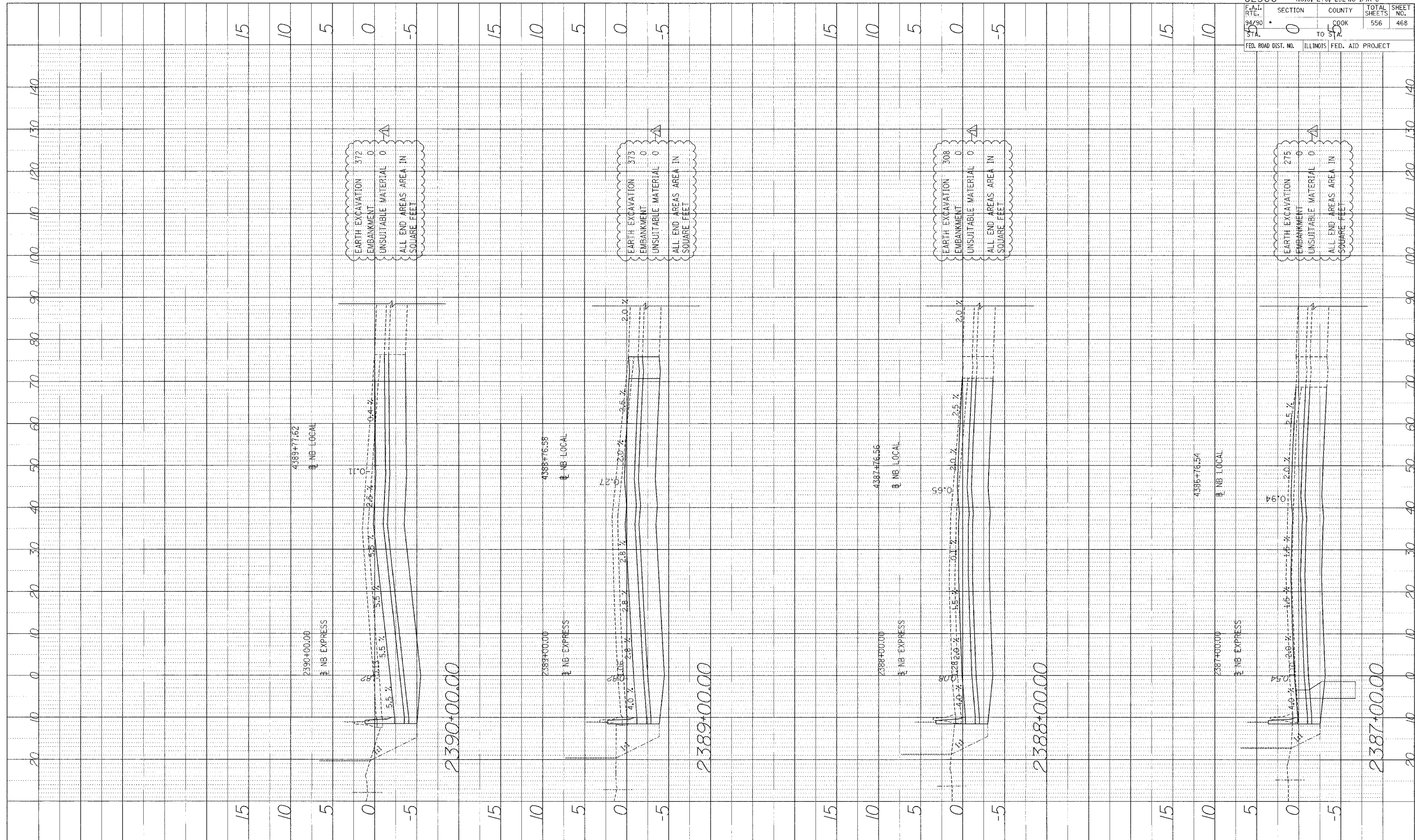
Edwards AND Kelcey
 ONE NORTH FRANKLIN CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
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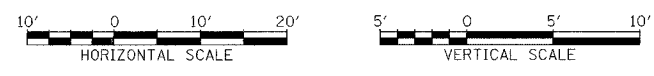
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STA. 49	TO STA. 57
FED. ROAD DIST. NO. ILLINOIS	FED. AID PROJECT
COUNTY COOK	TOTAL SHEETS 556
	SHEET NO. 468

Edwards AND Kelcey
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 PHONE: (312) 251-3000
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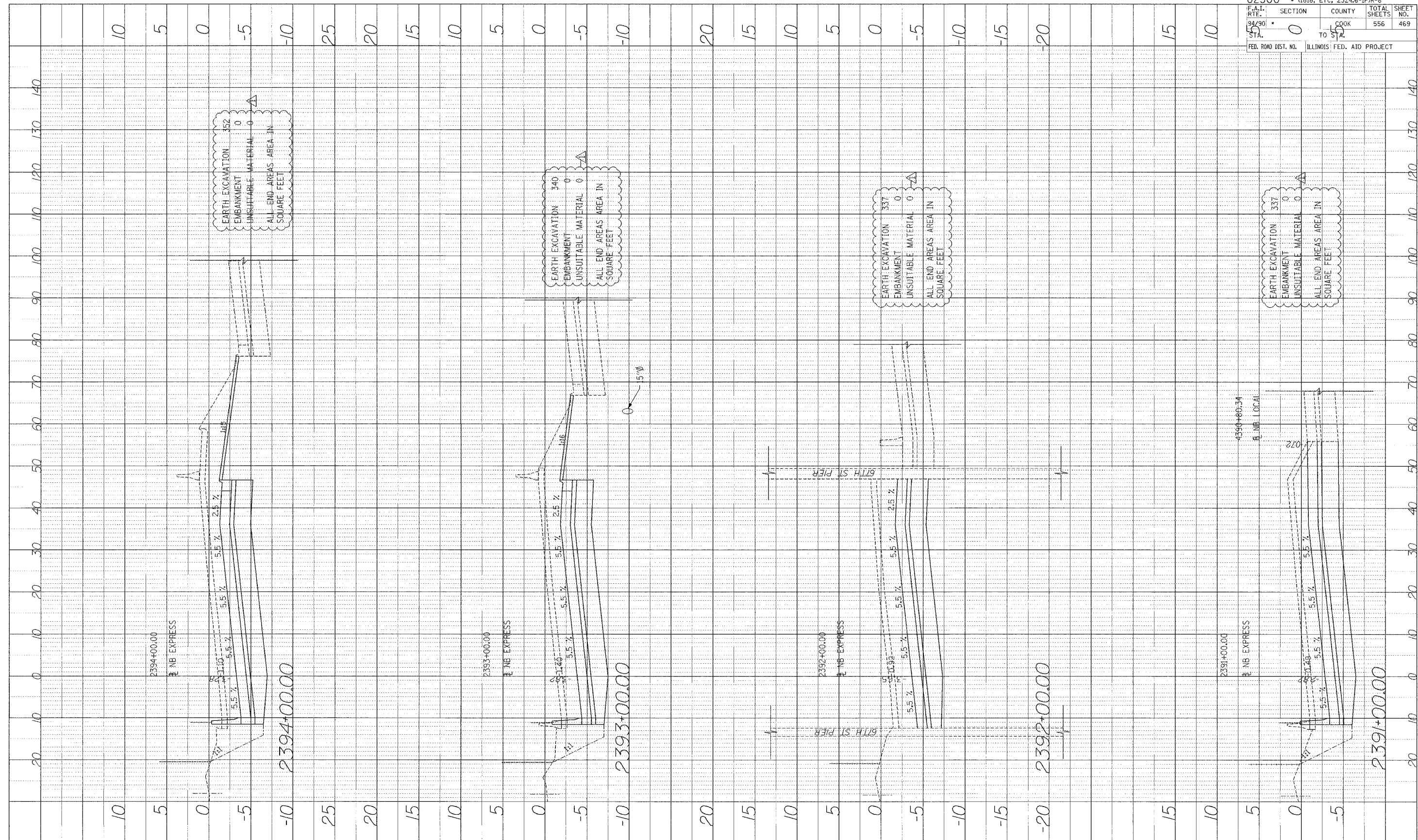


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 CHECKED BY: SHEET NO.:
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 PROJECT: ILLINOIS FED. AID PROJECT

62300 • (1818, ETC, 2324.6-IPR-8)
 ILLINOIS FED. AID PROJECT



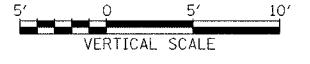
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 UNSUITABLE MATERIAL 0
 ALL END AREAS AREA IN SQUARE FEET

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EARTH EXCAVATION 337
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EARTH EXCAVATION 337
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 UNSUITABLE MATERIAL 0
 ALL END AREAS AREA IN SQUARE FEET

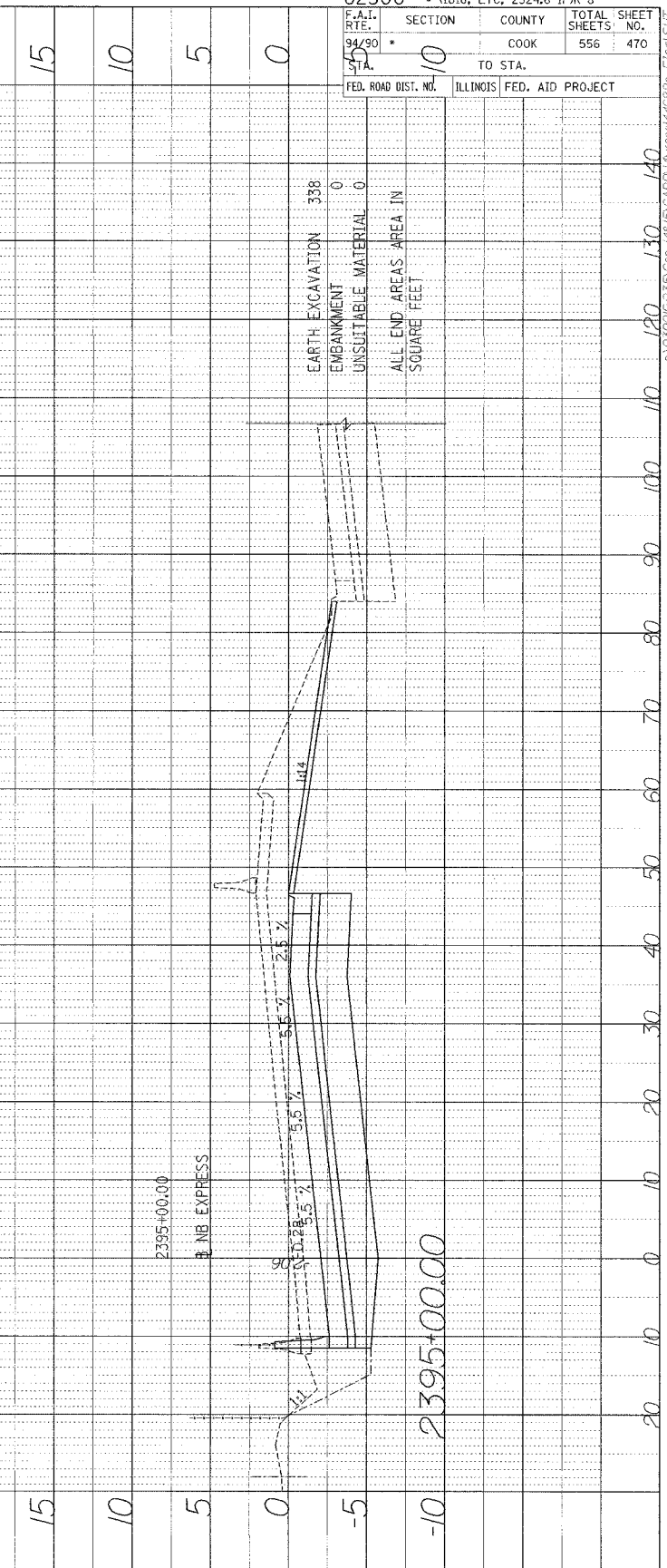
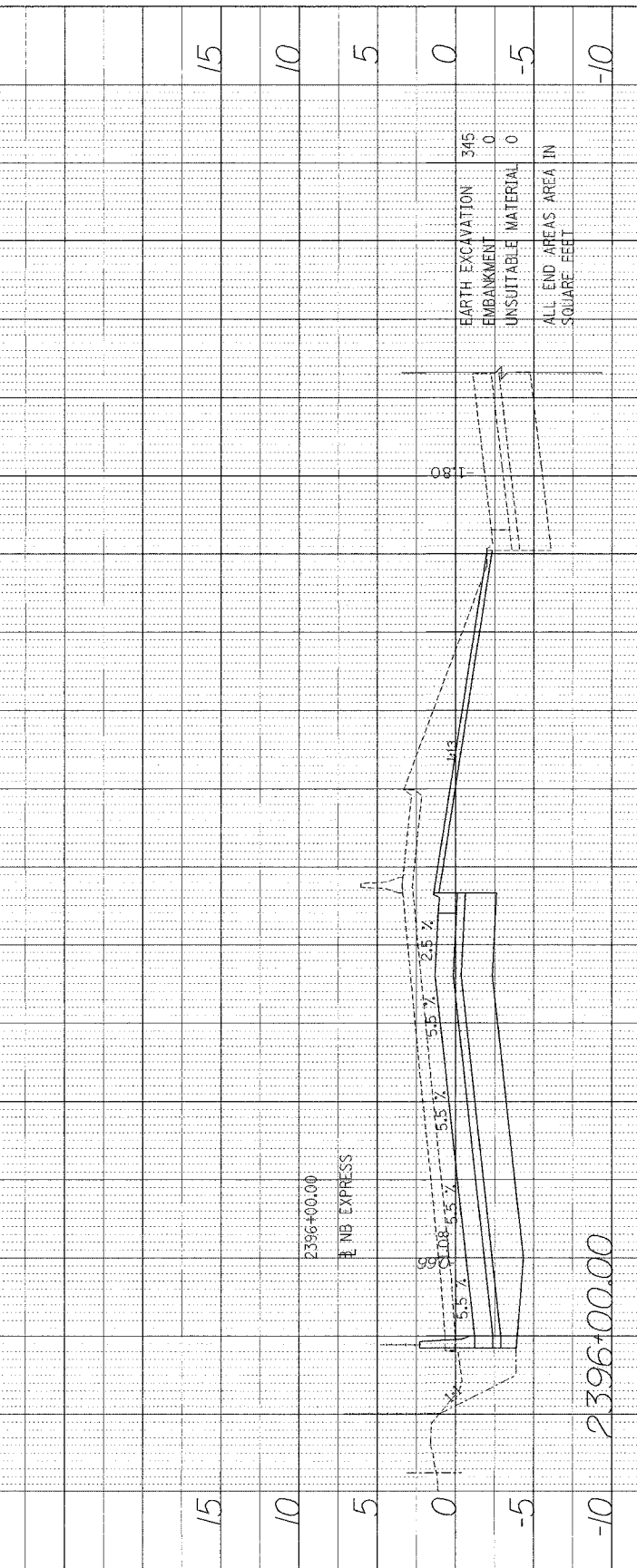
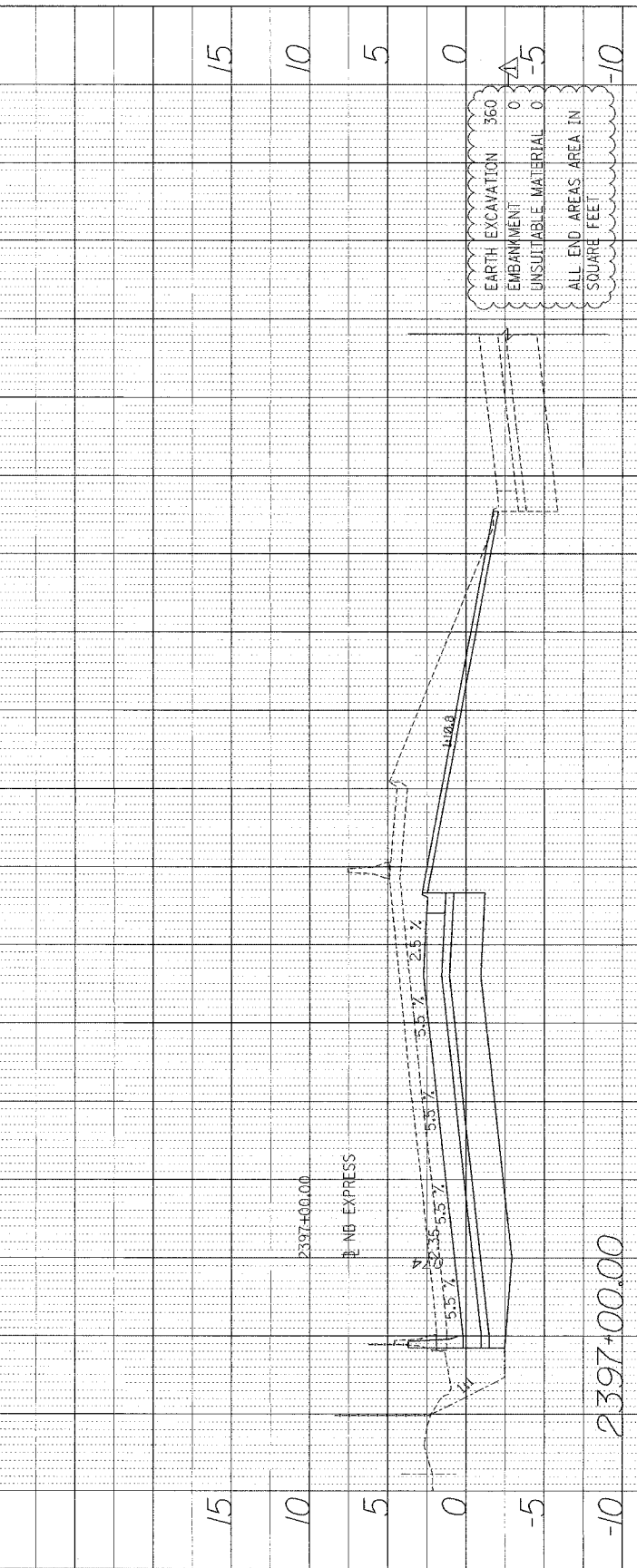
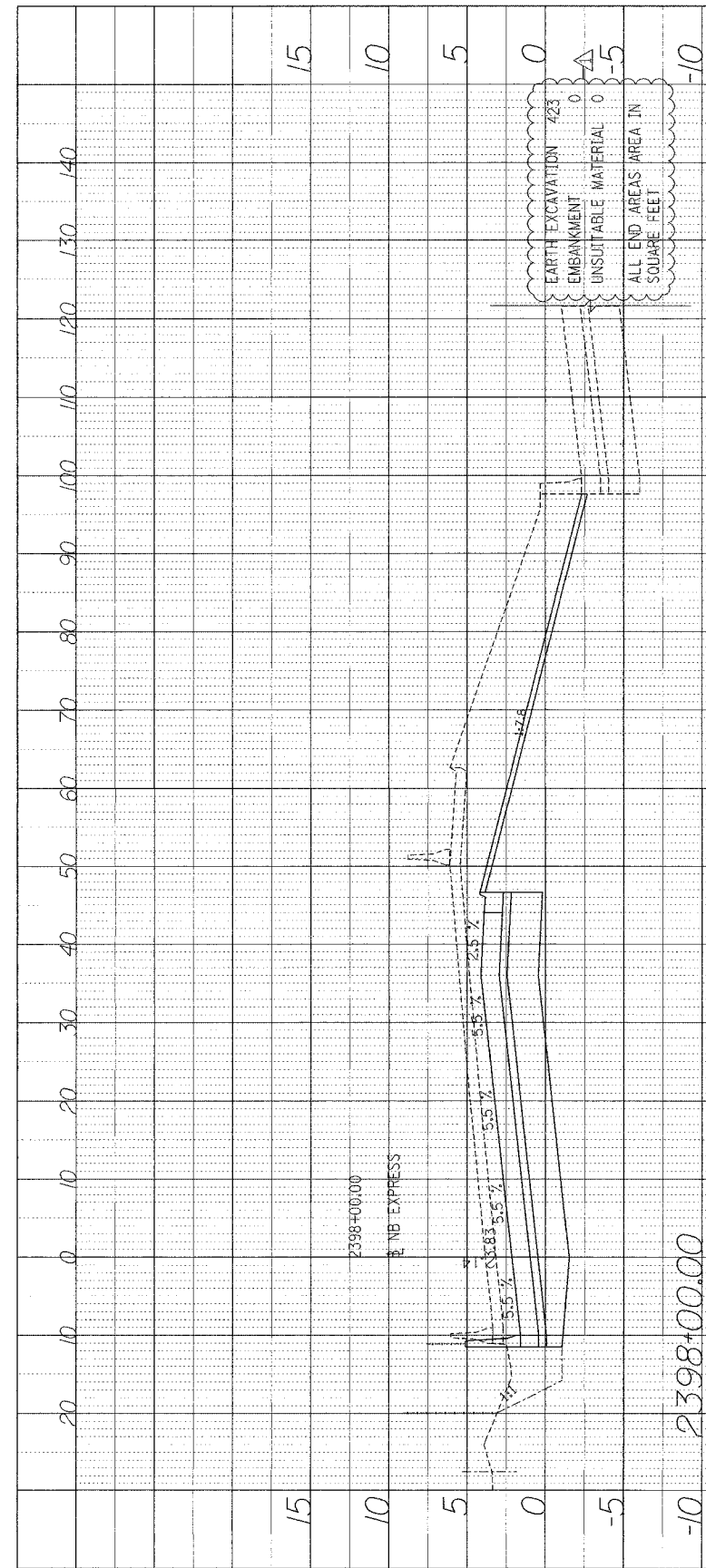
Edwards AND Kelcey
 ONE NORTH FRANKLIN CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



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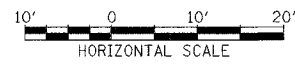
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PROFILE
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 CHECKED BY: _____
 NOTE BOOK NO. _____
 REFERENCE DRAWING NO. _____



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F.A.I. SECTION	COUNTY TOTAL SHEET
94/90	COOK 556 470
TO STA.	
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

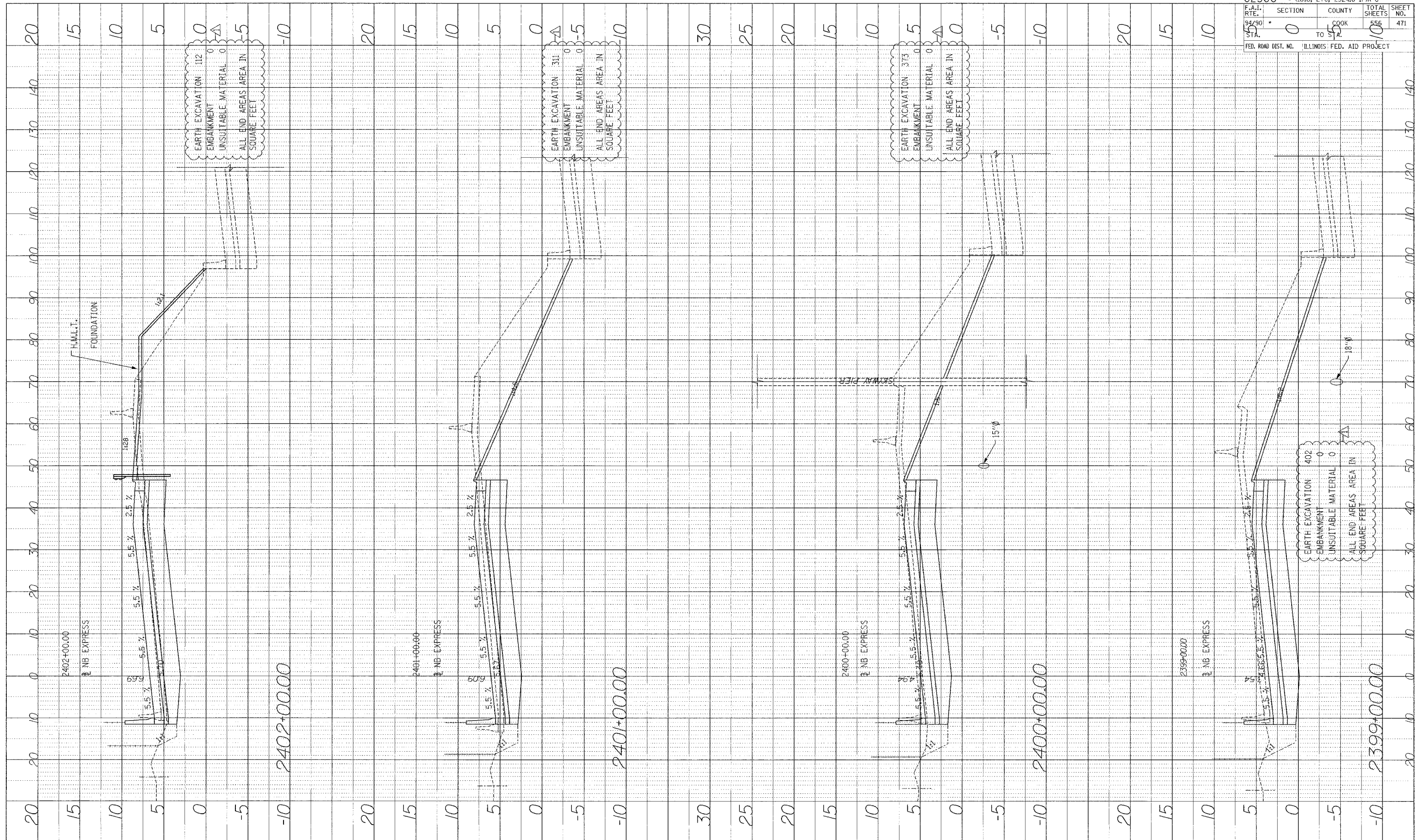
Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
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PROFILE SUBMITTED BY DATE
 PLOTTED BY
 NOTE BOOK NO. INCHES
 STRUCTURE NOTATING CHRD.



62300 • (1818, ETC, 2324.6-1P1R-8	
F.A.I. RTE. 94/90	SECTION 0
STA. 54	COUNTY COOK
	TOTAL SHEETS 556
	SHEET NO. 471
FED. ROAD DIST. NO. 'ILLINOIS' FED. AID PROJECT	

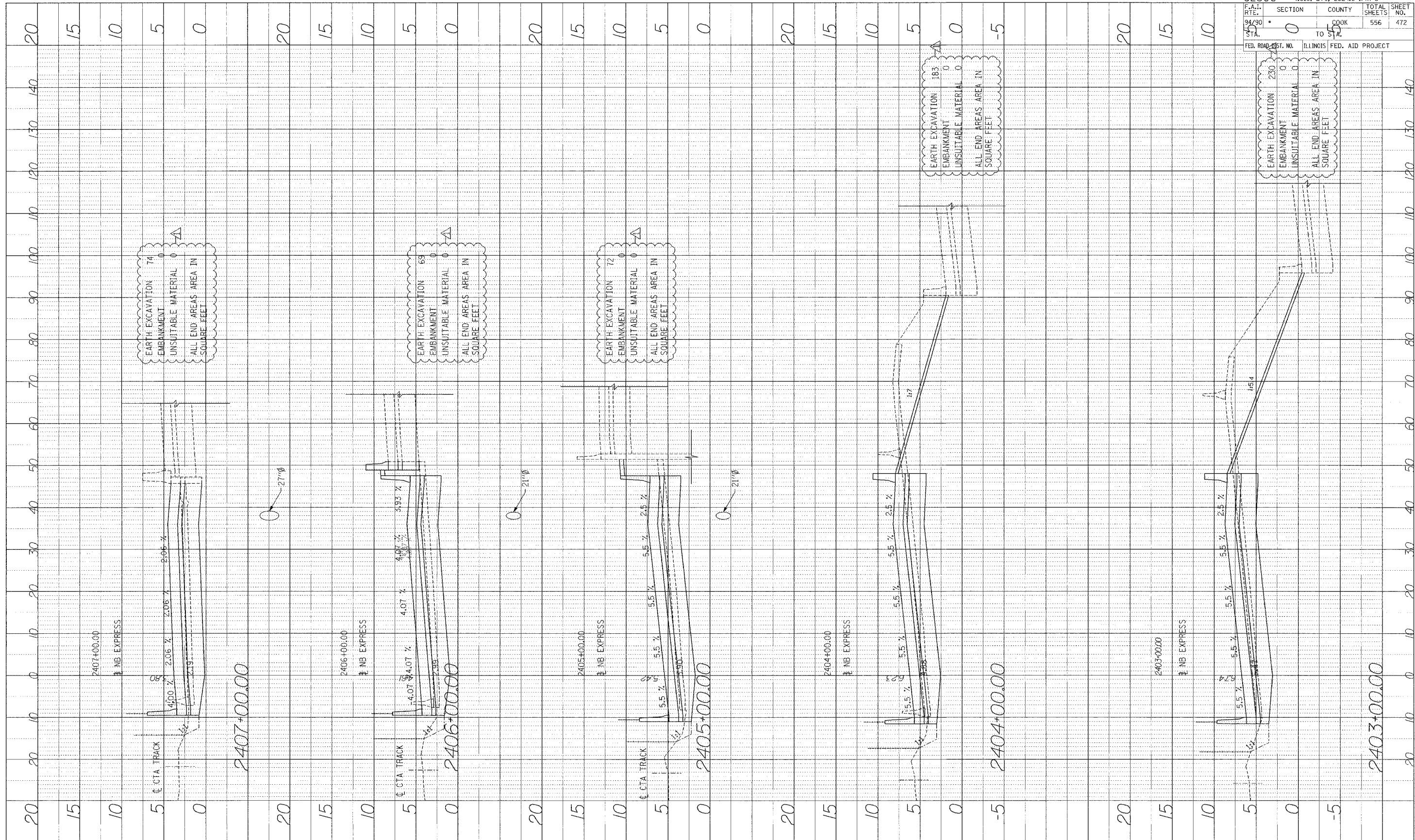
Edwards AND Kelcey
 ONE NORTH FRANKLIN CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



STA.

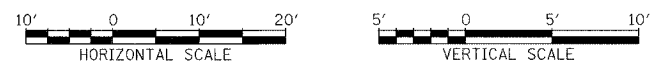
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PROFILE SURVEYED BY DATE
 PLANED BY
 NOTE BOOK NO. NOTES
 NO. STRUCTURE NOTED IN CHD



62300		• (1818, ETC, 2324.6-IPR-8	
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS
94/30	0	COOK	556
DATA	TO STA.	ILLINOIS	FED. AID PROJECT

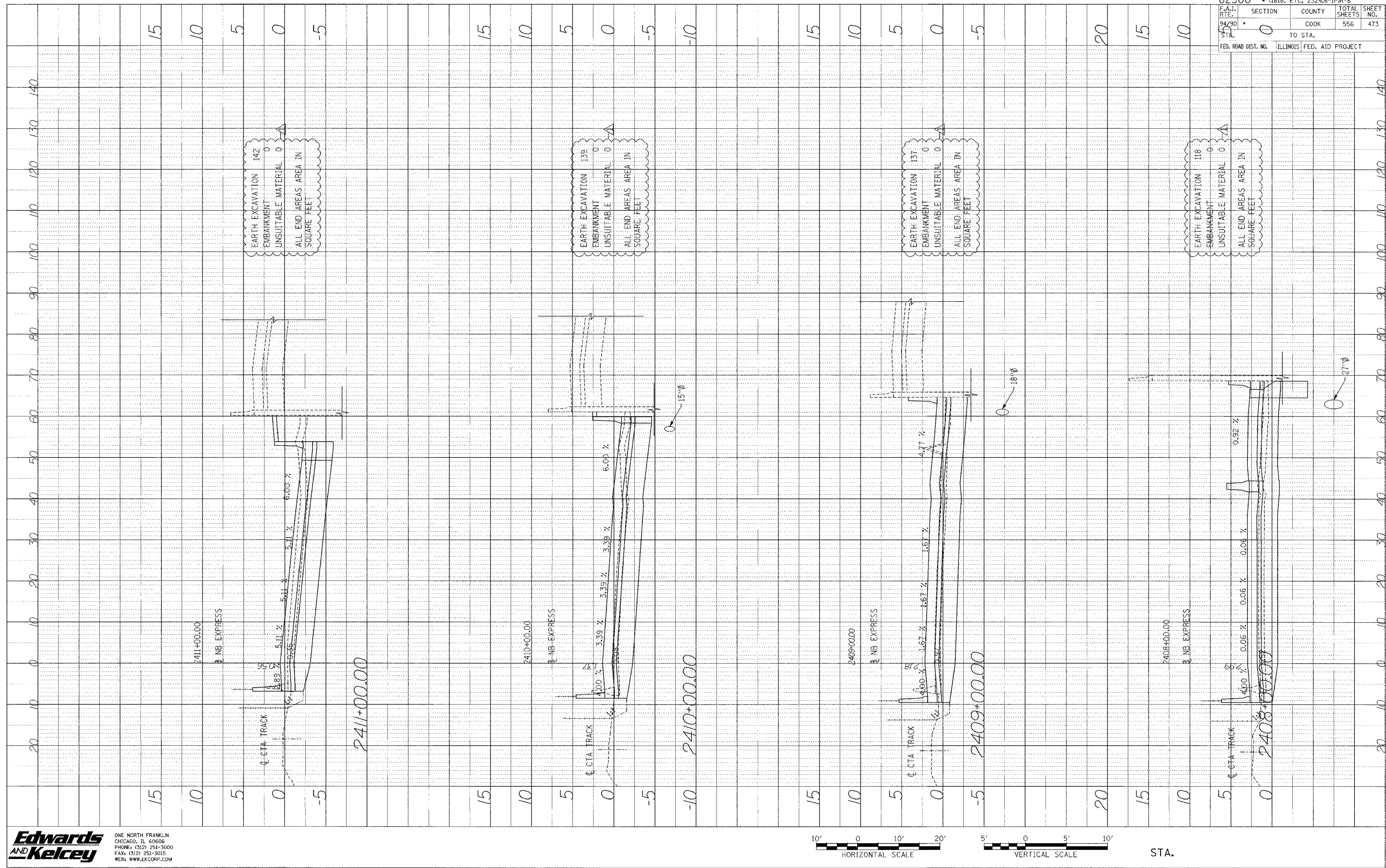
Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



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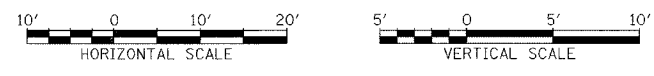
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PROJECT:	SUBMITTED:	BY:	DATE:
NEW YORK	BY CITY:	BY:	DATE:
NO.:	NO.:	NO.:	NO.:



62300 • (1818, ETC, 2324, 6-IPR-8			
F.A.I. NO.	SECTION	COUNTY	TOTAL SHEETS
94/90	0	COOK	556
DATE	TO STA.	FED. AID PROJECT	
1/21			

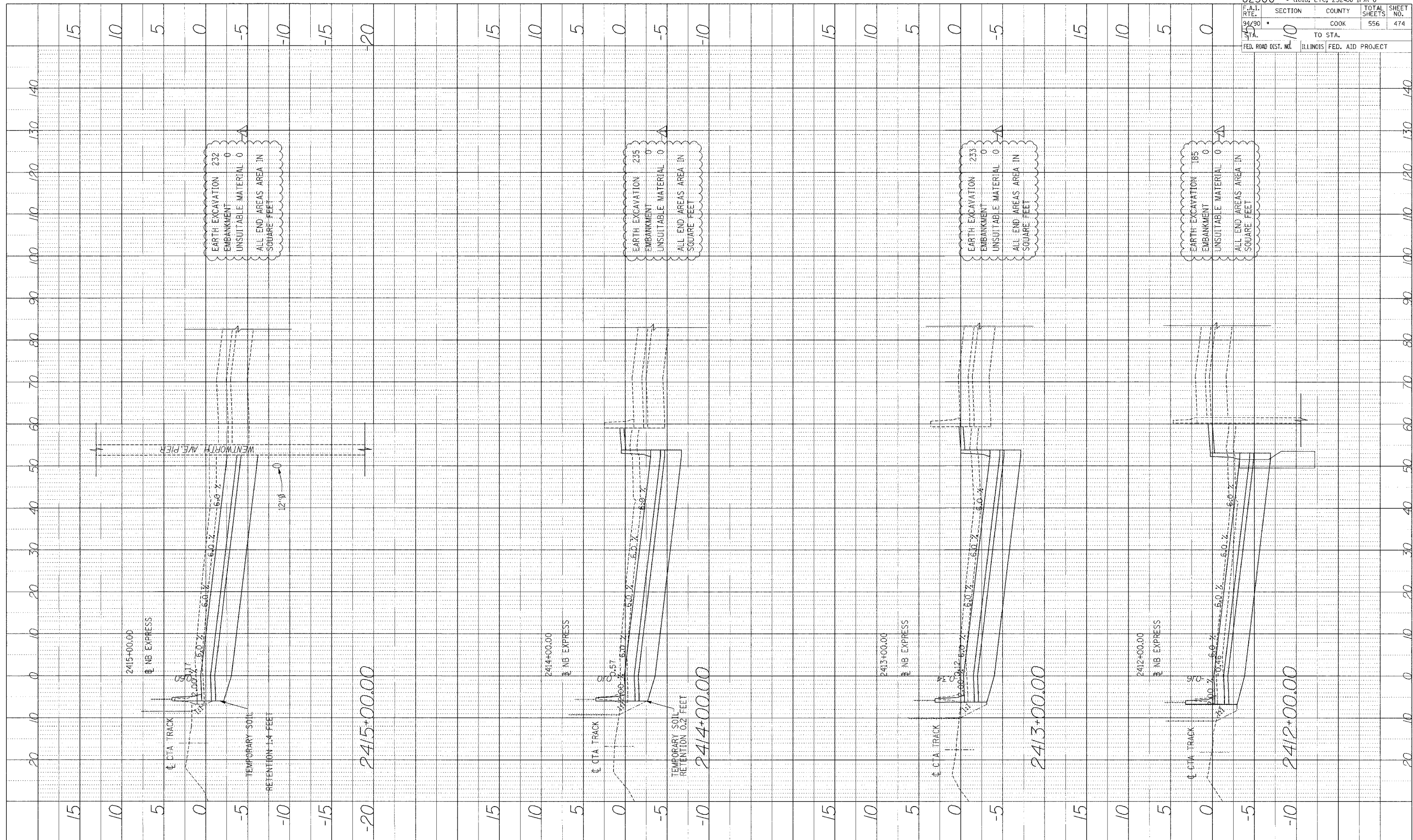
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 CHICAGO, IL 60606
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 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



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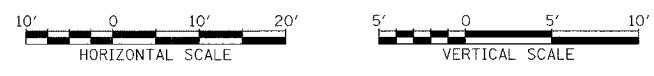
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PROFILE	REVISED	BY	DATE
NOTE BOOK	BY		
NO.			



62300	• (1818, ETC, 2324.6-1PR-8	
F.A.I. RTE.	SECTION	COUNTY
94/90	10	COOK
STA.	TO STA.	556
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT

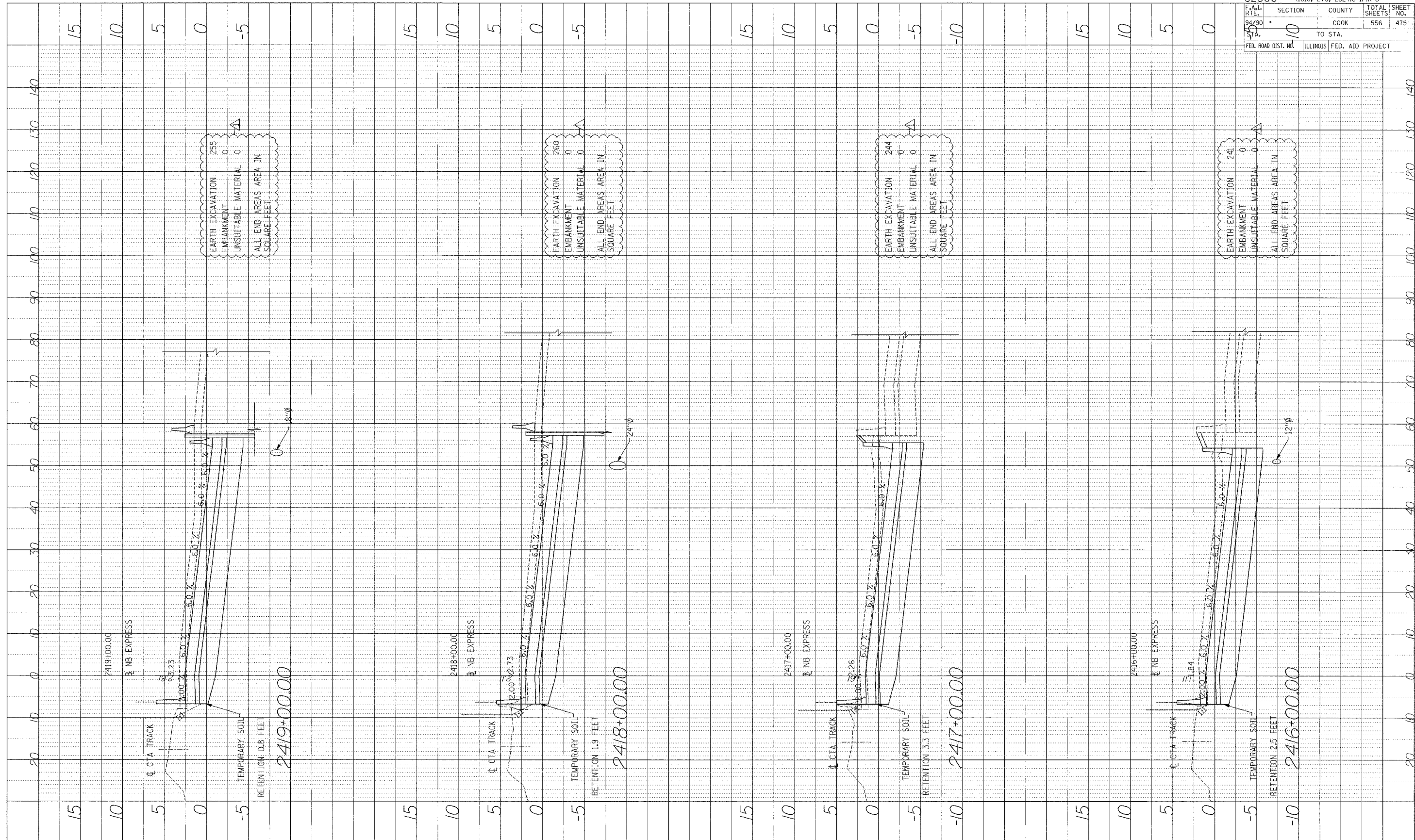
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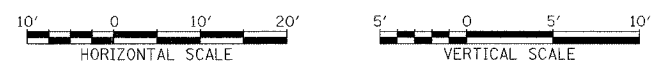
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PROJECT: SUBMITTED BY: DATE:
 NO. OF SHEETS: 3 OF 11
 DATE: 10/15/09
 PROJECT: STRUCTURE RETAINING WALL



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 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM

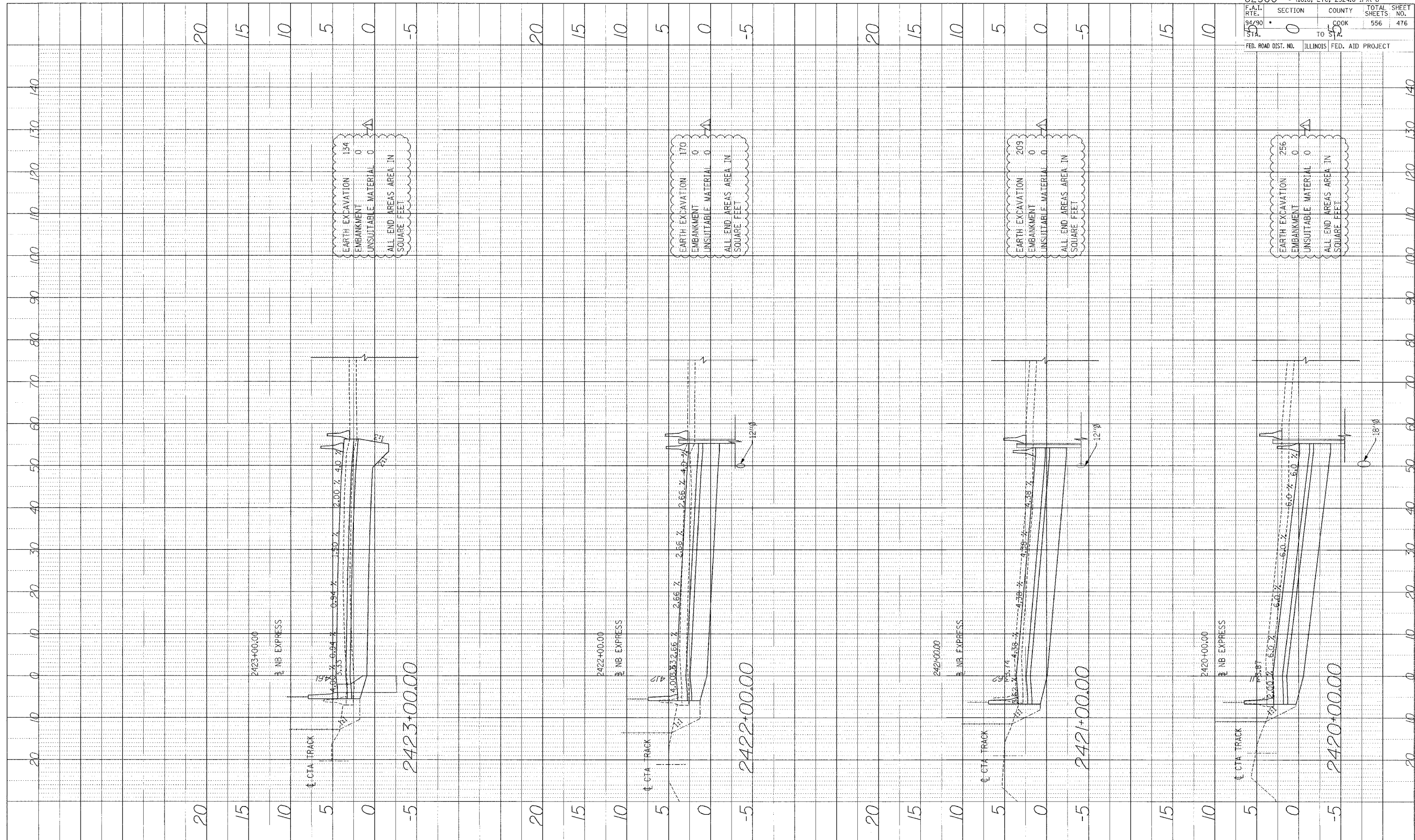


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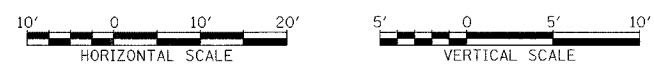
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F.A.I. R.T.E. NO. 99/90	SECTION 10
TO STA. 10	COUNTY COOK
FED. ROAD DIST. NO. ILLINOIS	FED. AID PROJECT
TOTAL SHEETS 556	SHEET NO. 475

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PROF. I.E. DESIGNED BY: DATE:
 NO. F. BOOK NO. CHECKED BY: DATE:
 NO. STRUCTURE NOTATION: C/P/D



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 ONE NORTH FRANKLIN CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM

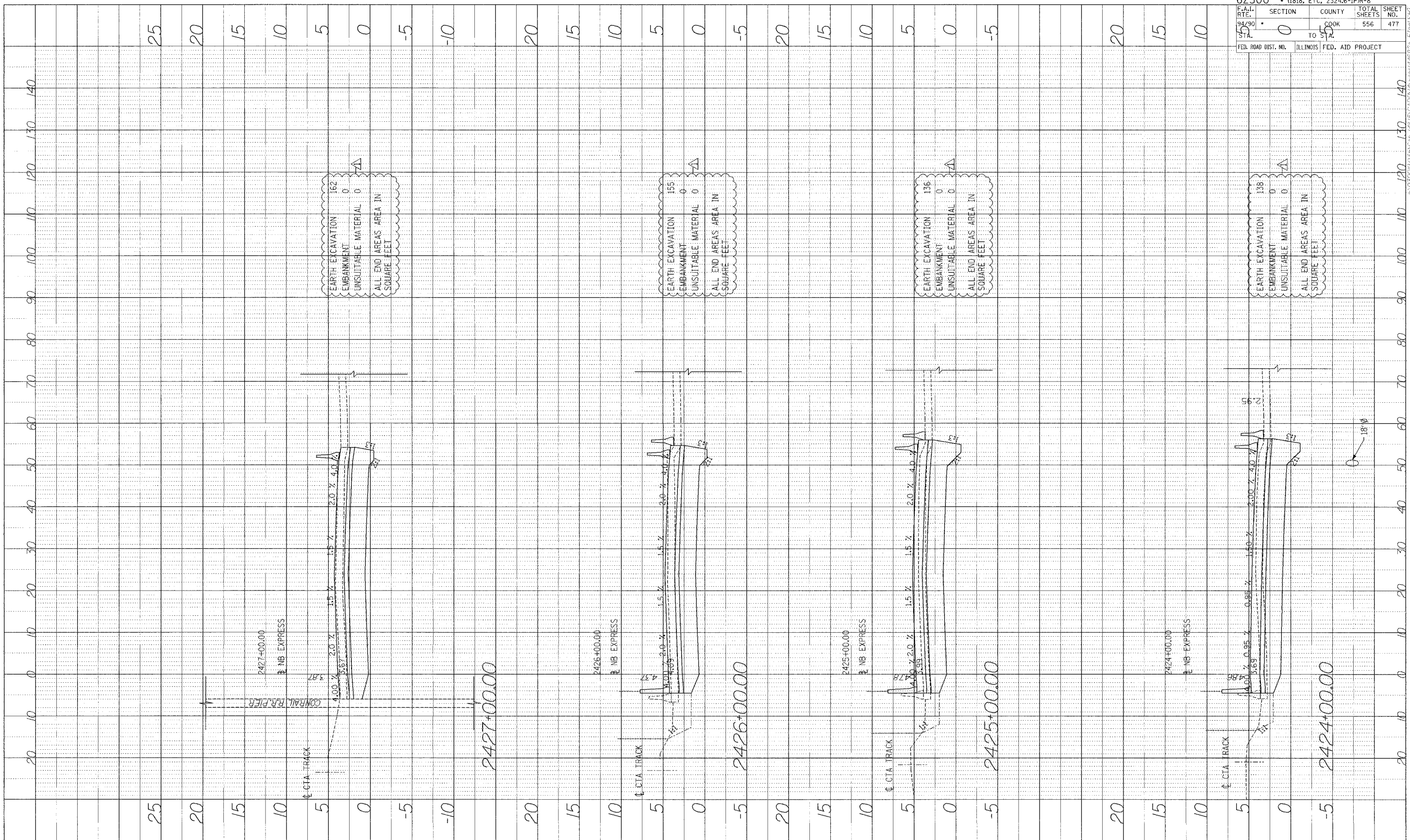


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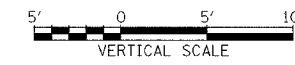
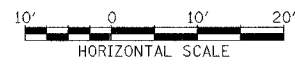
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 F.A.I. SECTION COUNTY TOTAL SHEET
 RTE. 0 0 COOK 556 476
 STA. 0 TO 50
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

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PROFILE BOOK NO. _____ DATE _____
 PROJECT NO. _____ BY _____
 TITLE _____
 DRAWN BY _____
 CHECKED BY _____
 DATE _____



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 CHICAGO, IL 60606
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 FAX: (312) 251-3015
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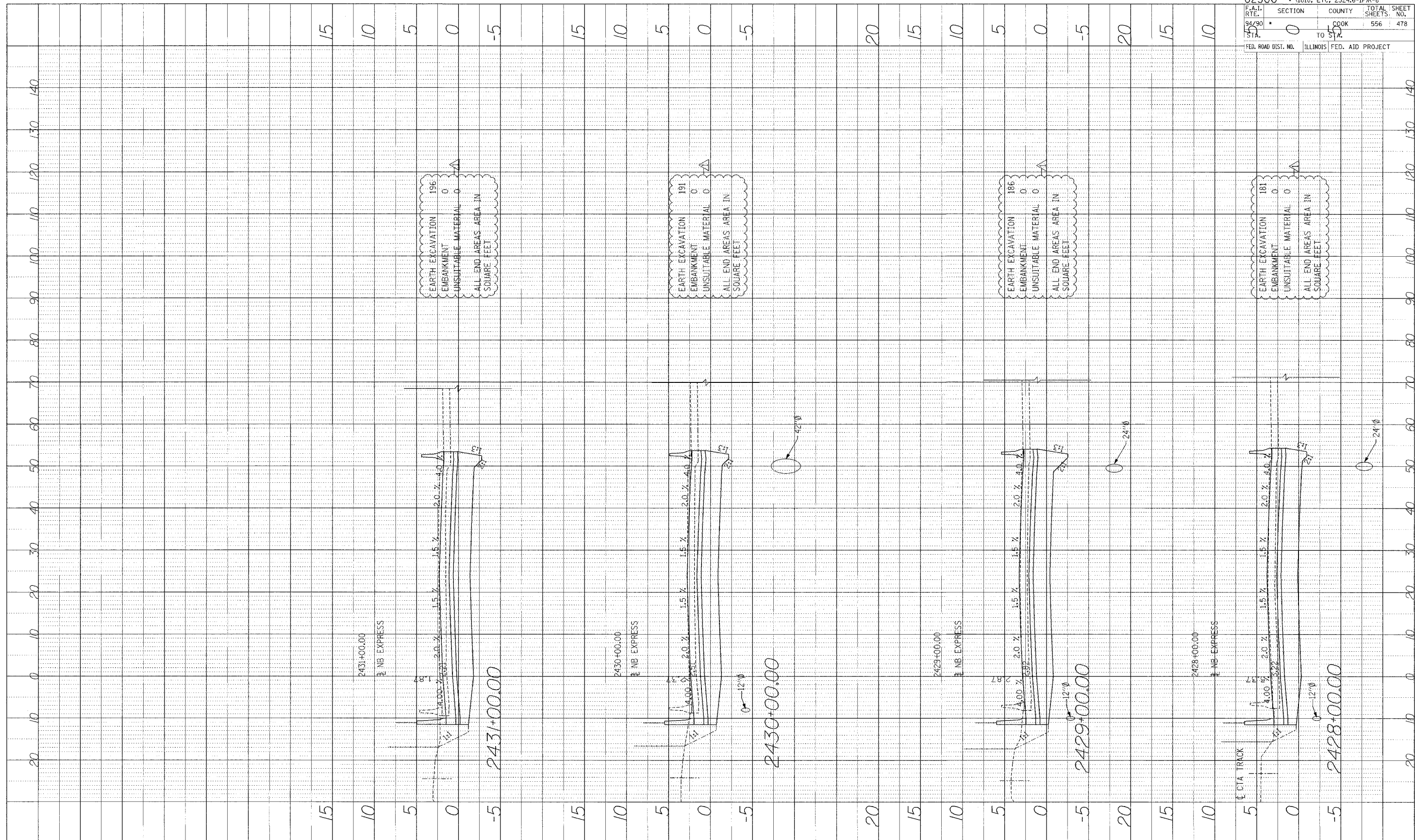


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62300 • (1818, ETC, 2324.6-1P)R-8	
F.A.I. RTE. 94/90	SECTION 0
STA. 15	COUNTY COOK
	TOTAL SHEETS 556
	SHEET NO. 477
ILLINOIS FED. AID PROJECT	

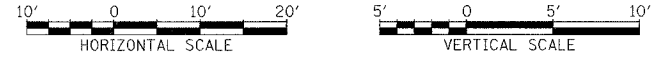
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PROFILE NUMBERED BY DATE
 NOTED BY DATE
 NO. STRUC. LIP. NOTATION OTHER



62300 • (1818, ETC, 2324.6-1PR-8
 F.A.I. SECTION COUNTY TOTAL SHEET
 RTE. 0 0 COOK 556 478
 STA. 55 TO STA.
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

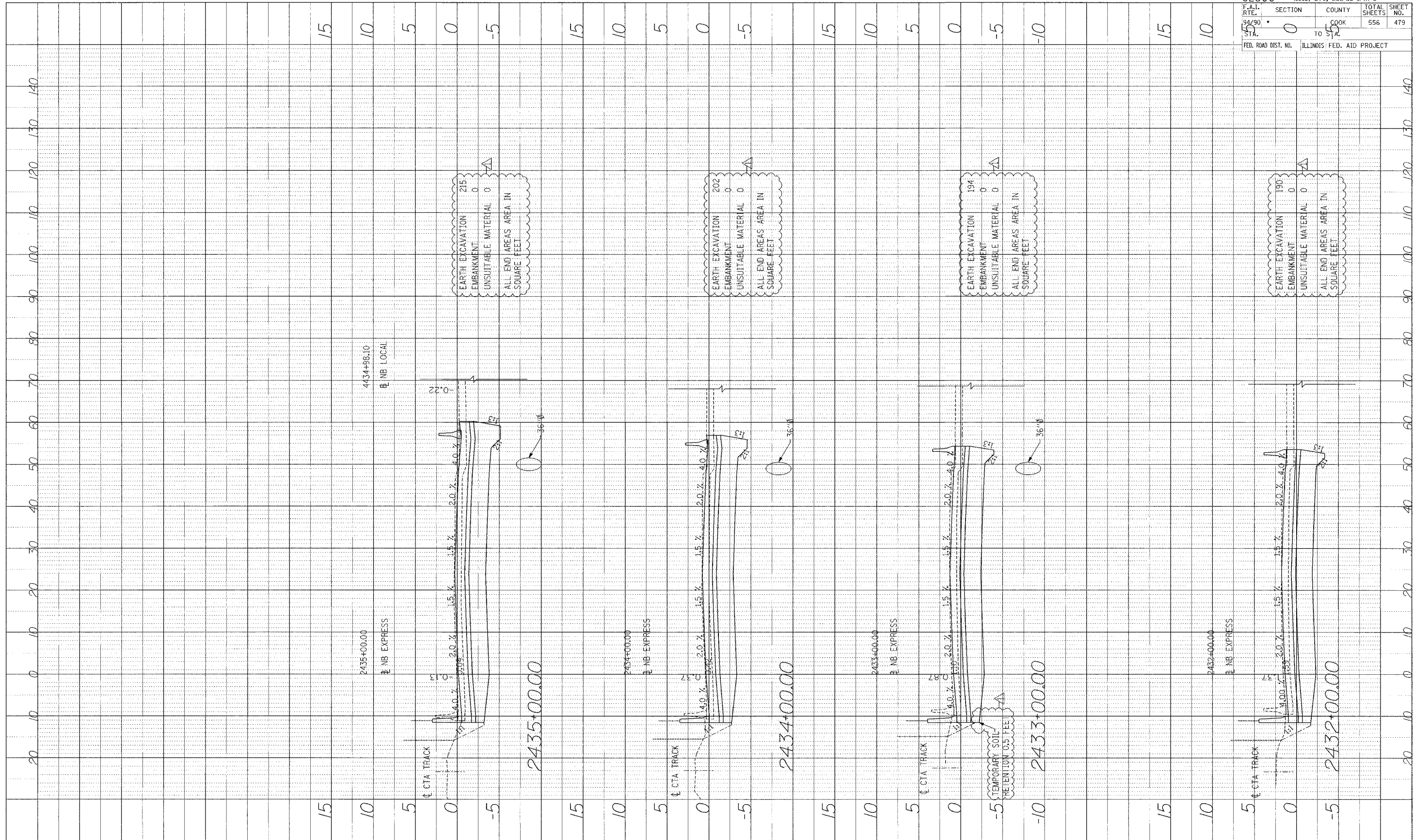
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 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
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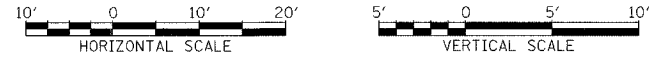
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PROJECT NO. _____ BY _____ DATE _____
 NO. 2 BOOK _____
 NO. _____ STR. KITING NOTATION: CP10



62300 • (1818, ETC, 2324.6-1PR-8		TOTAL SHEETS		SHEET NO.	
F.A.I. RTE.	SECTION	COUNTY	556	479	
94/90 STA.	0	COOK			
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

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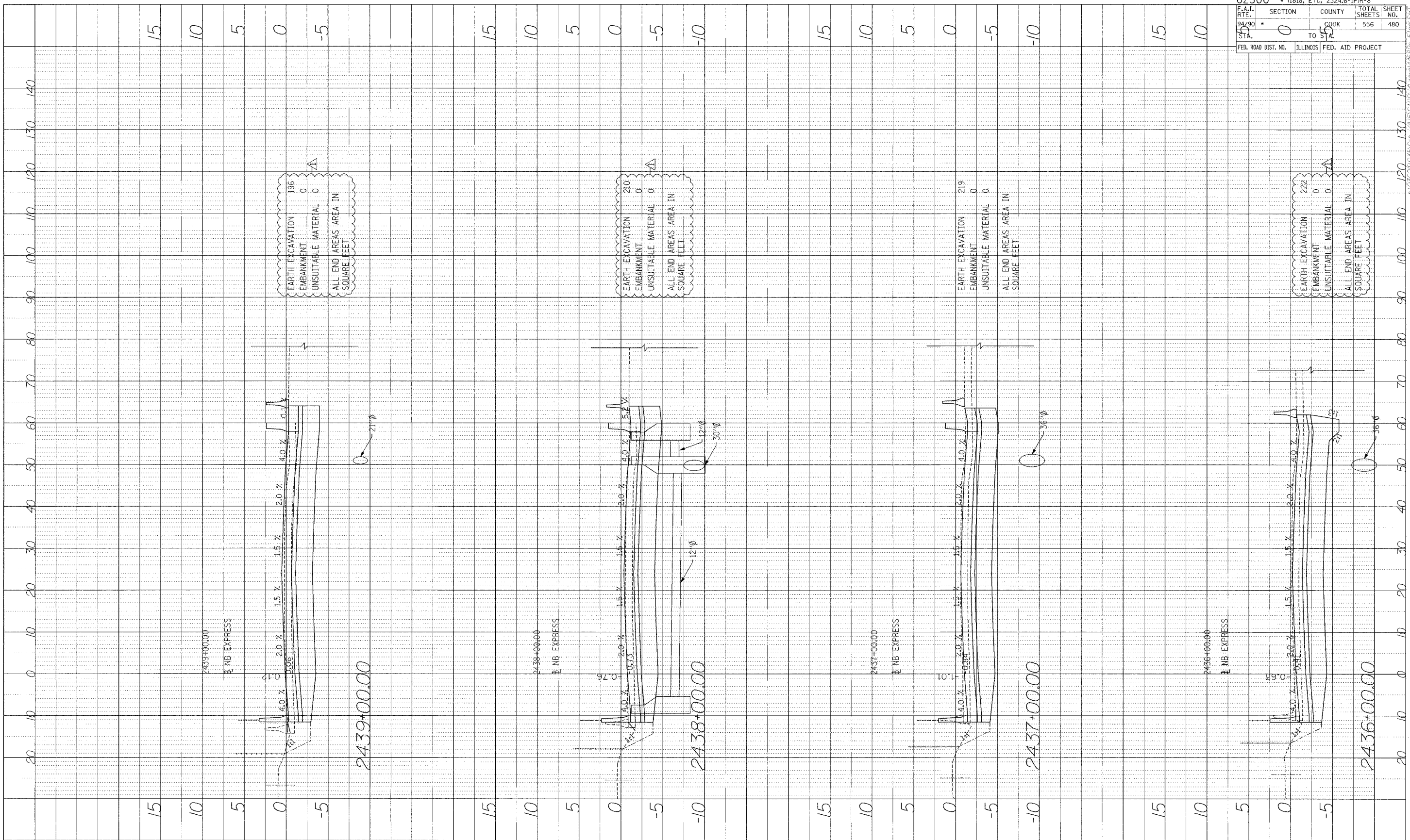


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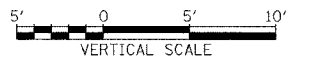
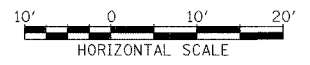
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PROFILE
 REVISIONS
 DATE
 NO. BY
 DETAILED
 CHECKED
 DATE
 NO. BY
 DATE

62300 • (1818, ETC. 2324.6-1P)R-8			
F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS
94/90	0	COOK	556
STA.	15	TO STA.	480
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	



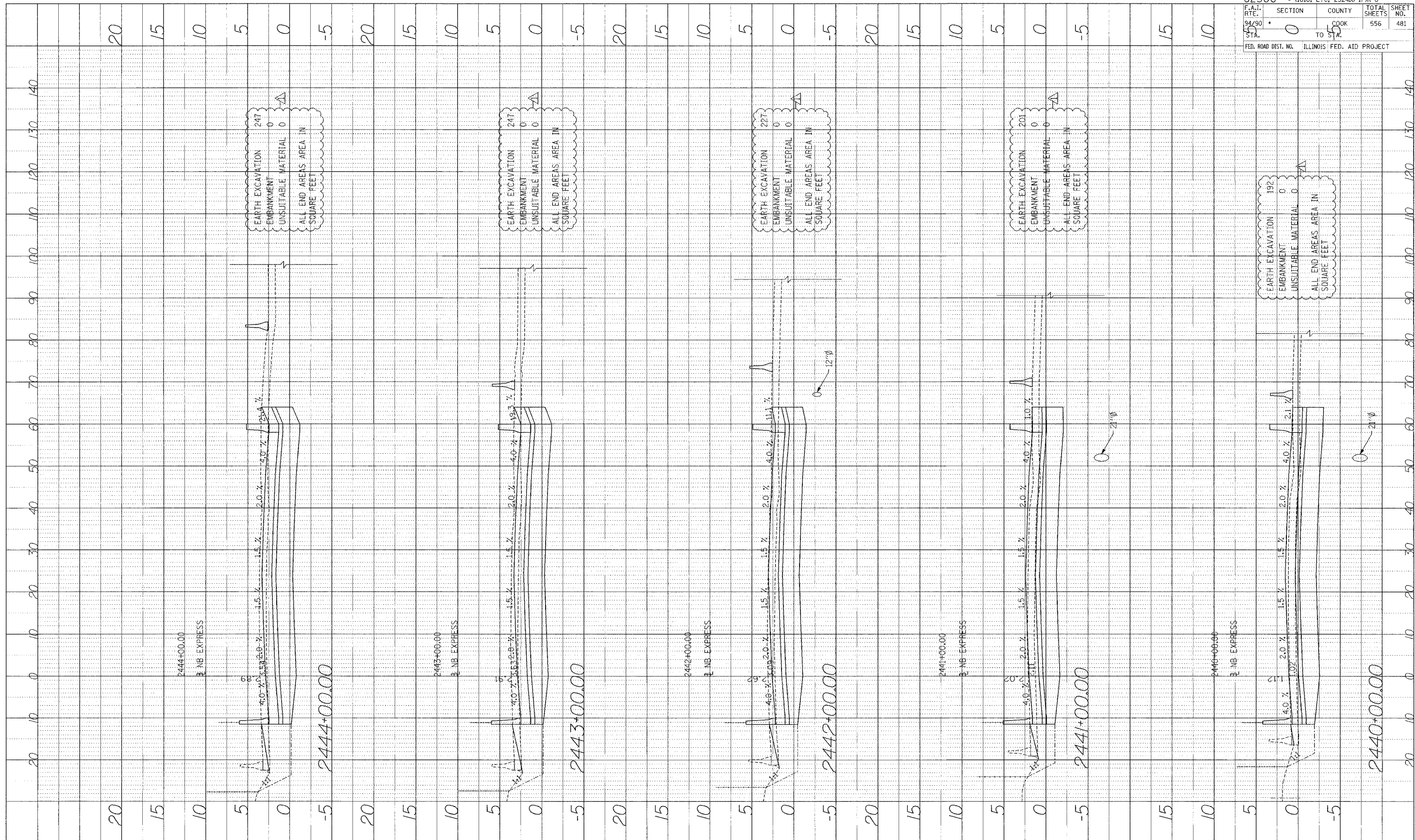
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PROFILE SURVEYED BY: _____ DATE: _____
 PLOTTED BY: _____
 CHECKED BY: _____
 DATE: _____
 STRUCTURE NO. _____



F.A.I. No. 62300	SECTION 0	COUNTY COOK	TOTAL SHEETS 556	SHEET NO. 481
STA. 49+90	TO STA. 50+00	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		

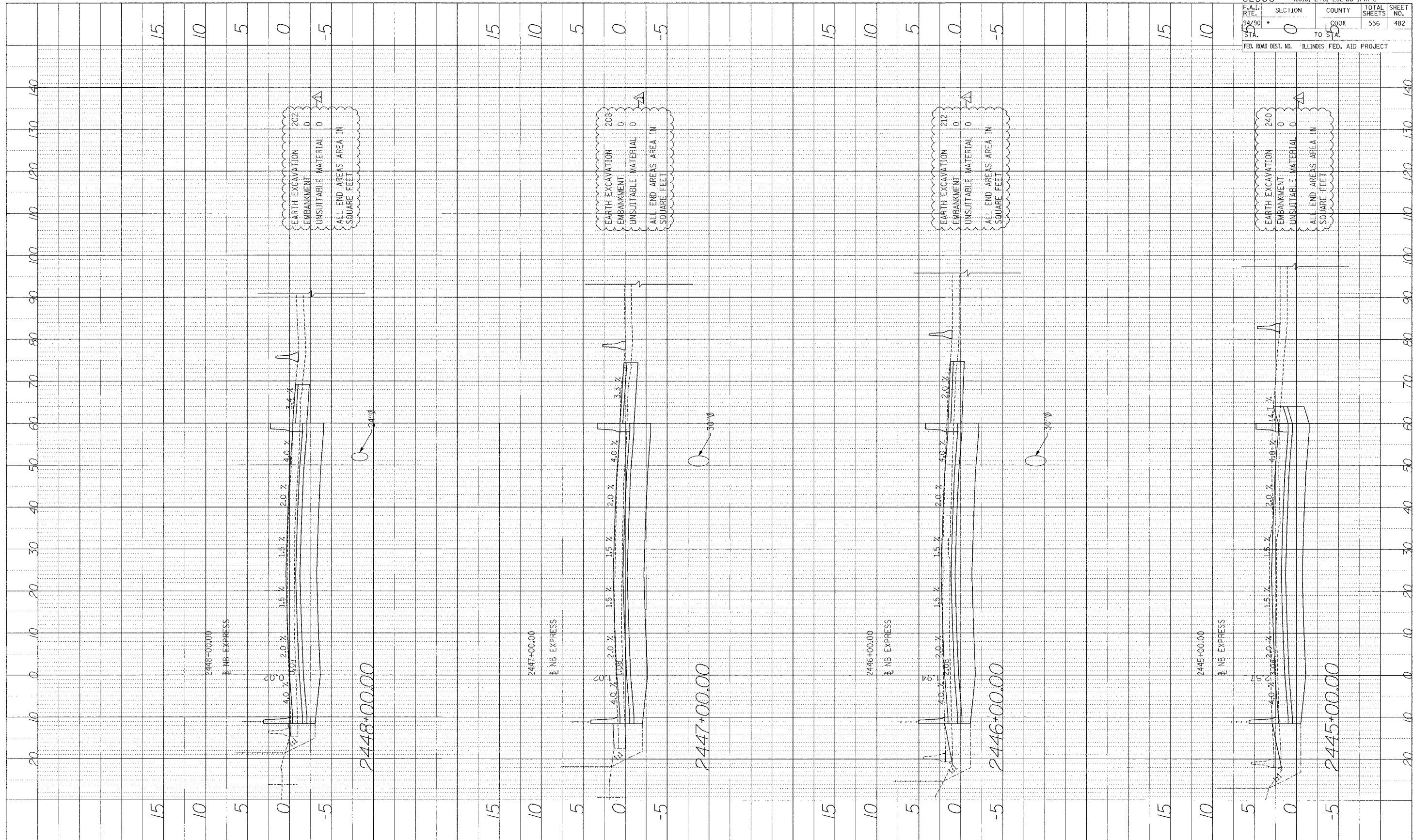
Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-5000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



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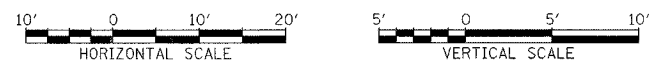
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PROFILE SUBMITTED BY DATE
 NOTE BOOK PLANNED BY
 NO. STRUCTURE INDICATED



62300		• (1818, ETC, 2324.6-IPR-8	
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS
94/90	0	COOK	556
STA.	TO STA.	ILLINOIS	FED. AID PROJECT
			SHEET NO. 482

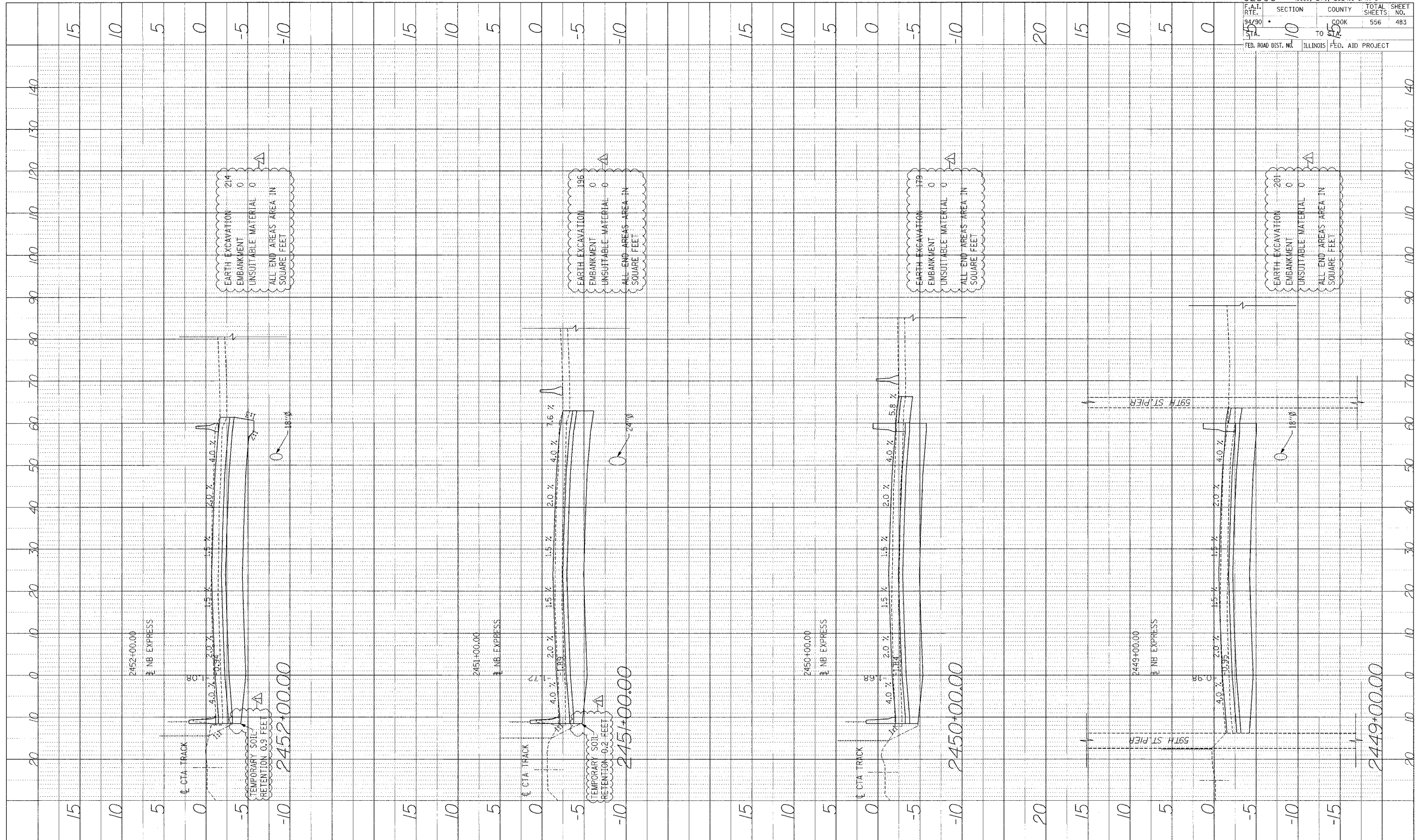
Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



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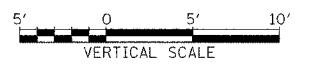
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PROFILE
 DRAWING NO. 31 DATE
 NOTE BOOK NO. 10/15/07
 NO. STRUCTURE NOTATION: CHFD



62300	(1818, ETC, 2324.6-IPR-8
F.A.I. RTE. 94/90	SECTION 10
TO STA. 140	COUNTY COOK
FED. ROAD DIST. NO. ILLINOIS	FED. AID PROJECT
TOTAL SHEETS: 556	SHEET NO.: 483

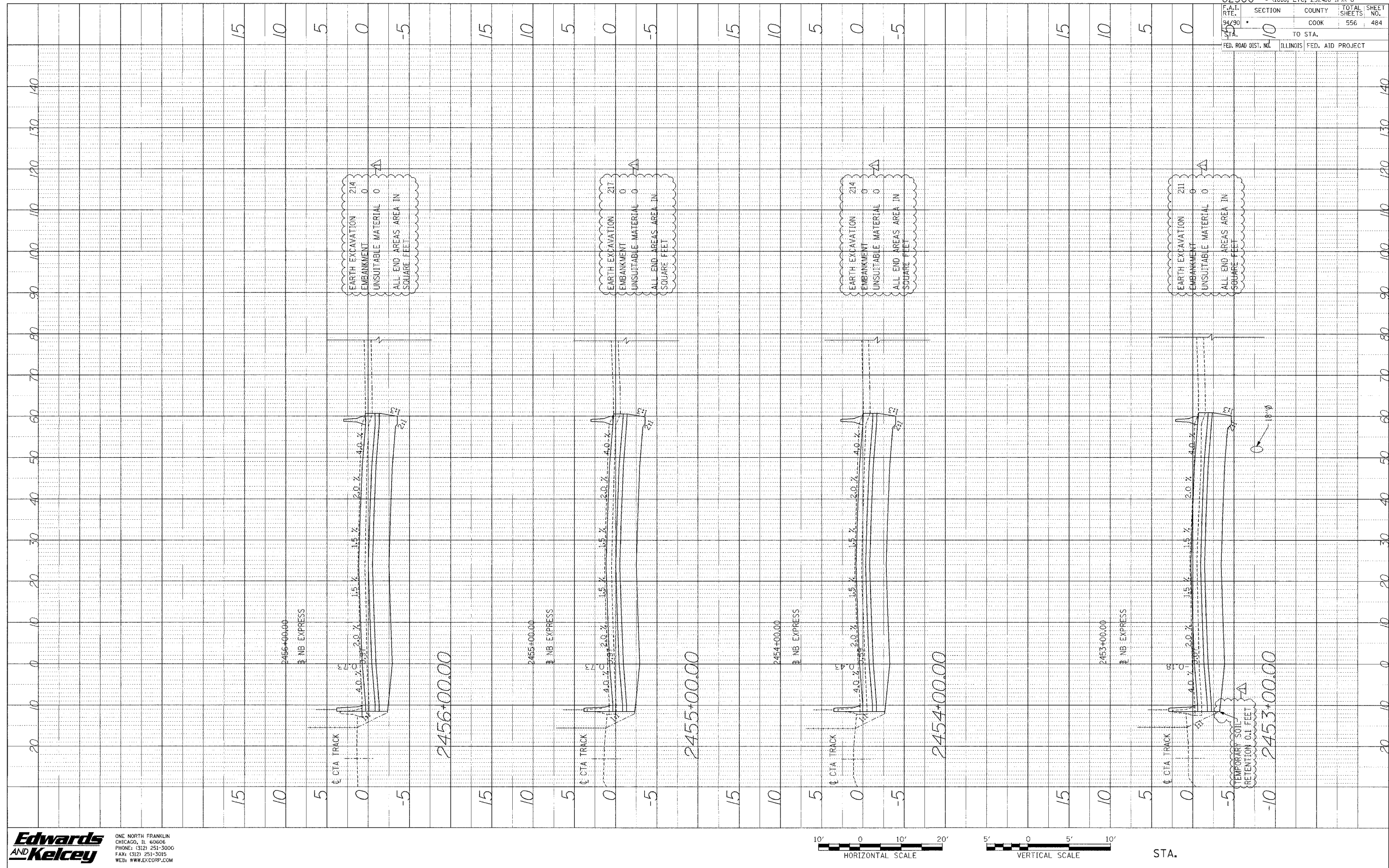
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 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



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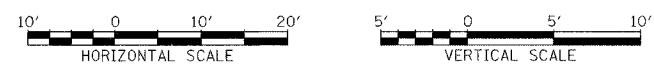
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PROFILE SUBMITTED BY DATE
 REVISIONS
 NOTE BOOK NO. CHECKED BY
 STRUCTURE NO. DATE



62300 • (1818, ETC, 2324.6-IPR-8	
F.A.I. RTE. 94/90	SECTION 10
COUNTY COOK	TOTAL SHEETS 556
TO STA. 10	SHEET NO. 484
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

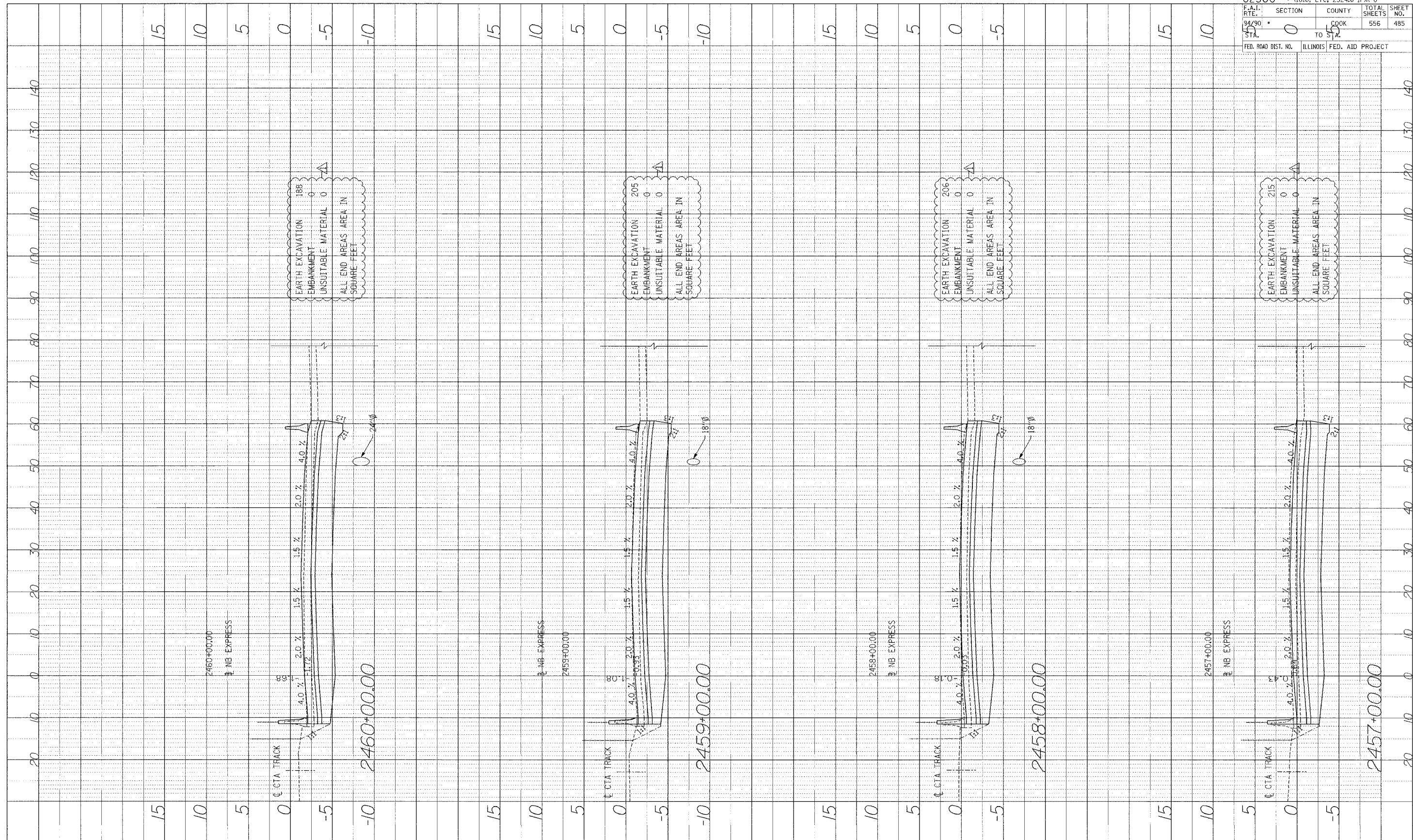
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 WEB: WWW.EDCORP.COM



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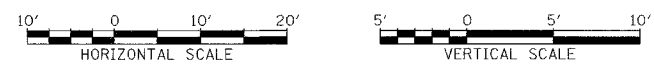
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PROFILE REVISIONS BY DATE
 NOTE BOOK NO. DATE REVISIONS
 NO. STRUCTURE NOTATION OTHERS



62300	(1818, ETC, 2324.6-1P1R-8)
F.A.I. SECTION	COUNTY TOTAL SHEETS SHEET NO.
94/90	COOK 556 485
STA. 0	TO STA. 140
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

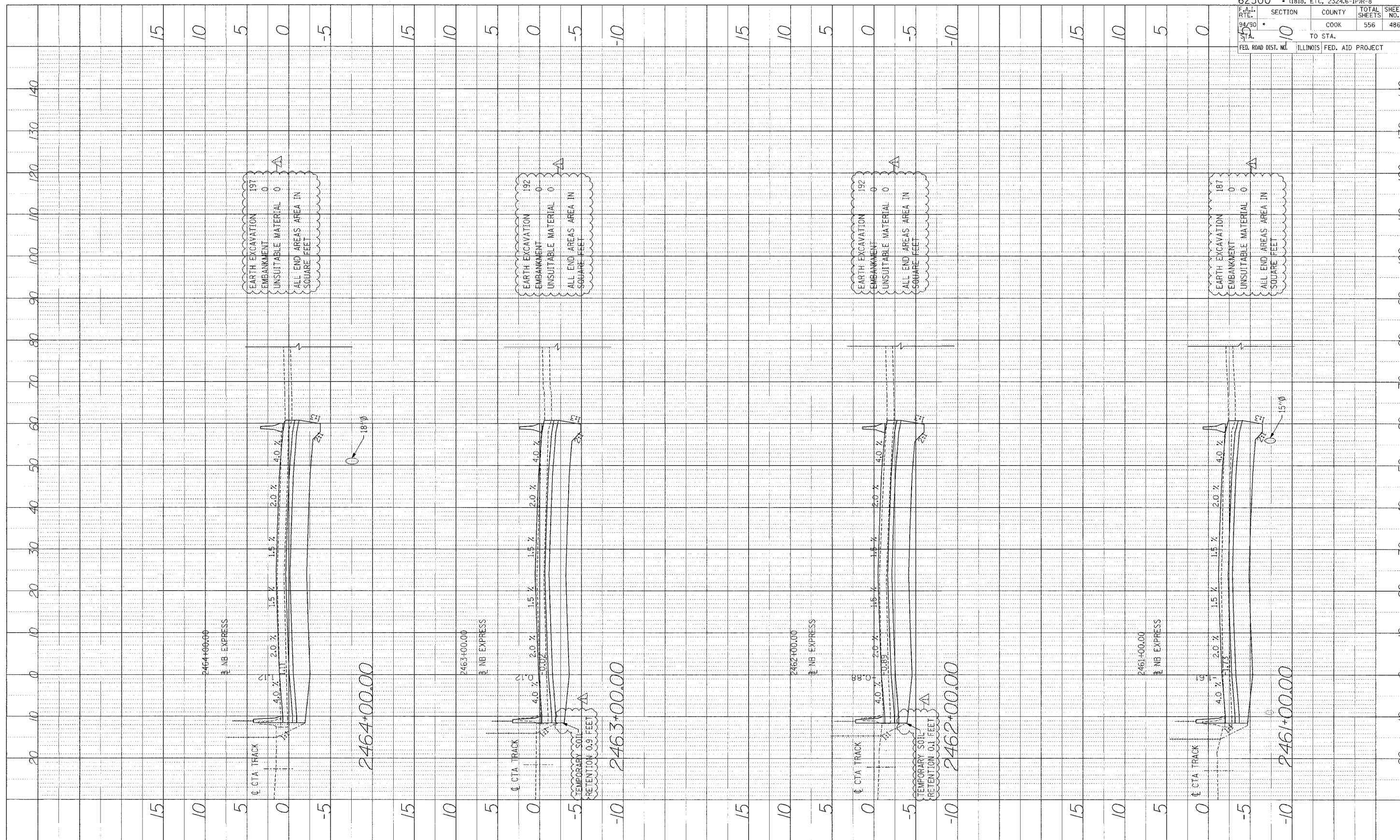
Edwards AND Kelcey
 ONE NORTH FRANKLIN CHICAGO, IL 60606
 PHONE: (312) 251-3000 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



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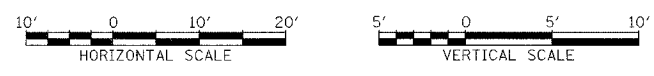
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PROFILE SUBMITTED BY DATE
 PLANTED BY
 NOTE BOOK NO. REVISIONS
 NO. STRUCTURE NO. DATE



62300 • (1818, ETC, 2324.6-IPR-8	
F.A.I. R.T.E. 94/90	SECTION COUNTY TOTAL SHEETS SHEET NO.
55	COOK 556 486
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT
10	TO STA. 140

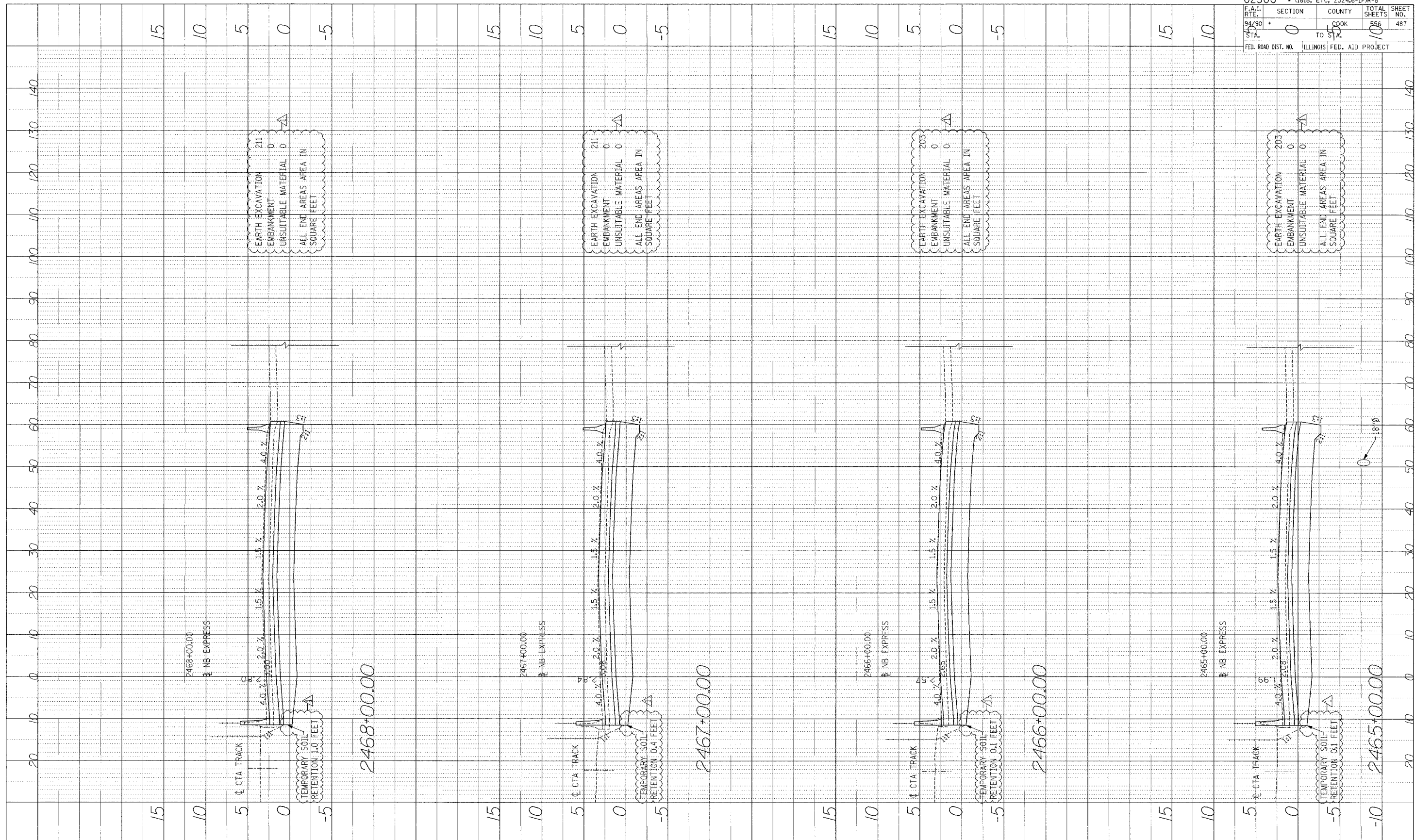
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 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



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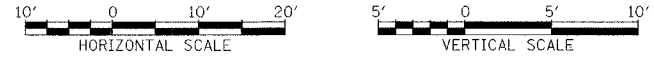
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PROFILE SUBMITTED BY _____ DATE _____
 CALCULATED BY _____ DATE _____
 NOTE BOOK NO. _____
 NO. _____ STRUCTURE NOTATION CHFD



62300 • (1818, ETC, 2324, 6-IPR-8)	
F.A.I. RTE. 94/90	SECTION 0
STA. 55	COUNTY COOK
TO STA. 100	TOTAL SHEETS 556
FED. ROAD DIST. NO. ILLINOIS	FED. AID PROJECT
	SHEET NO. 487

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 CHICAGO, IL 60606
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 WEB: WWW.EKCORP.COM

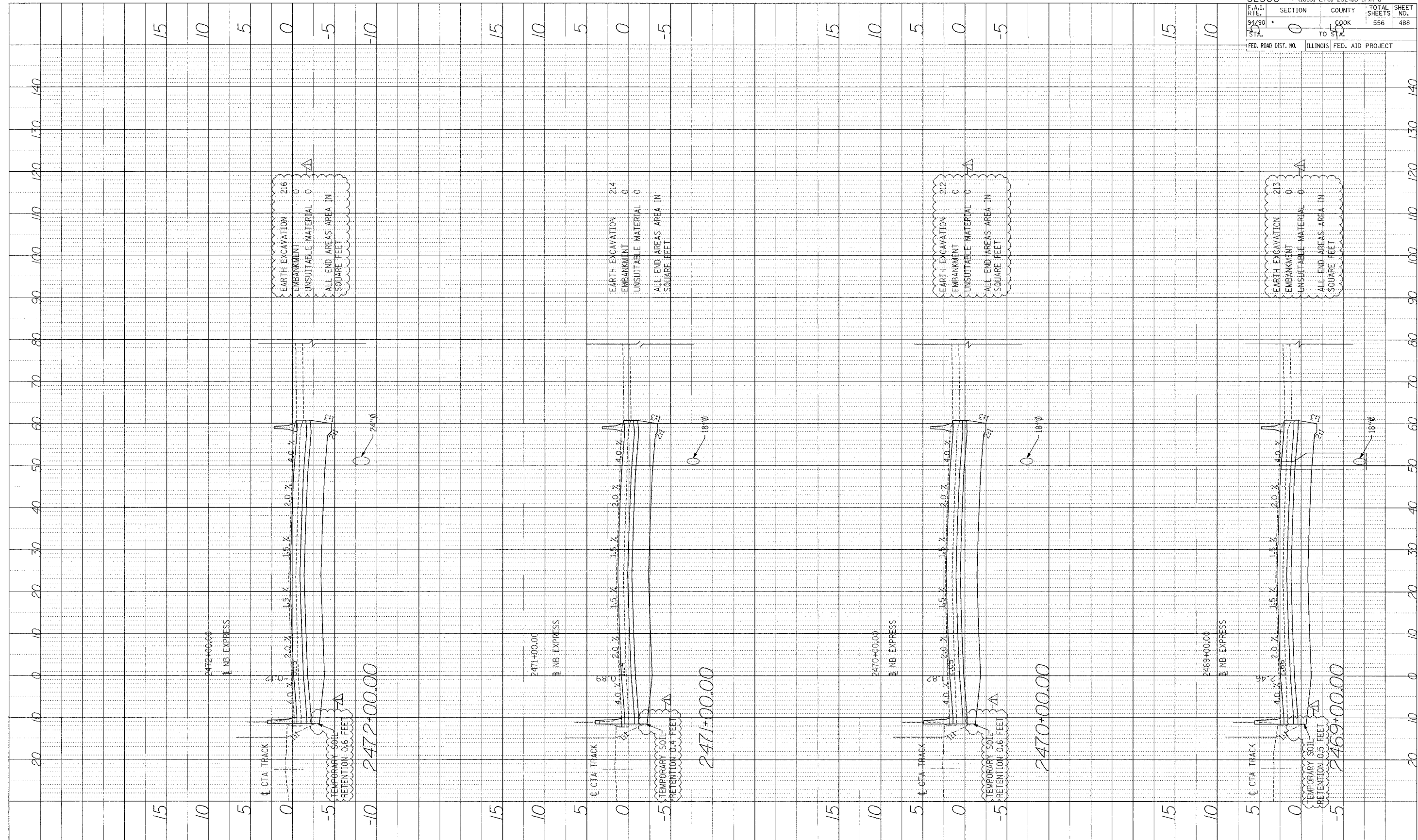


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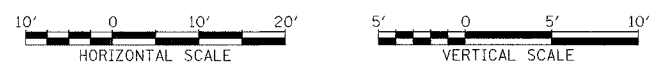
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PROFILE SURVEYED BY: DATE: _____
 PLOTTED BY: _____
 NOTE BOOK NO. _____
 STRUCTURE INTERFERENCE OFFICE NO. _____

62300 • (1818, ETC, 2324.6-1PR-8
 F.A.I. SECTION COUNTY TOTAL SHEETS SHEET NO.
 94/90 * 0 COOK 556 488
 STA. TO STA.
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



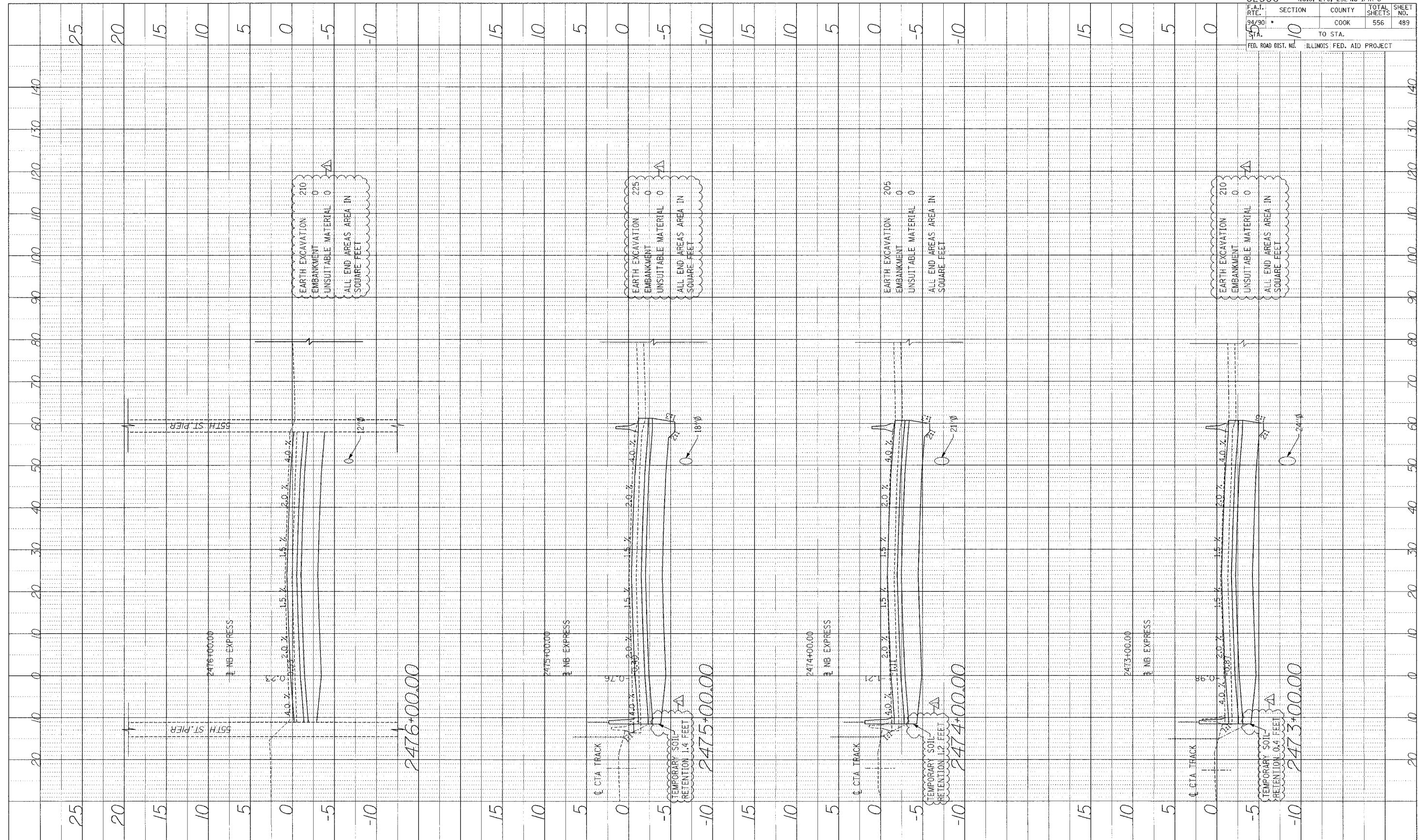
Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3100
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



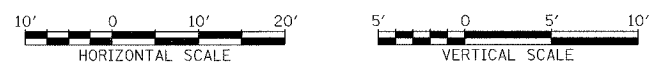
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PROFILE SHEET NO. DATE
 PLOTTED BY
 CHECKED BY
 NOTE NO. 1001
 STRUCTURE RETAINING CURB



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 ONE NORTH FRANKLIN CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM

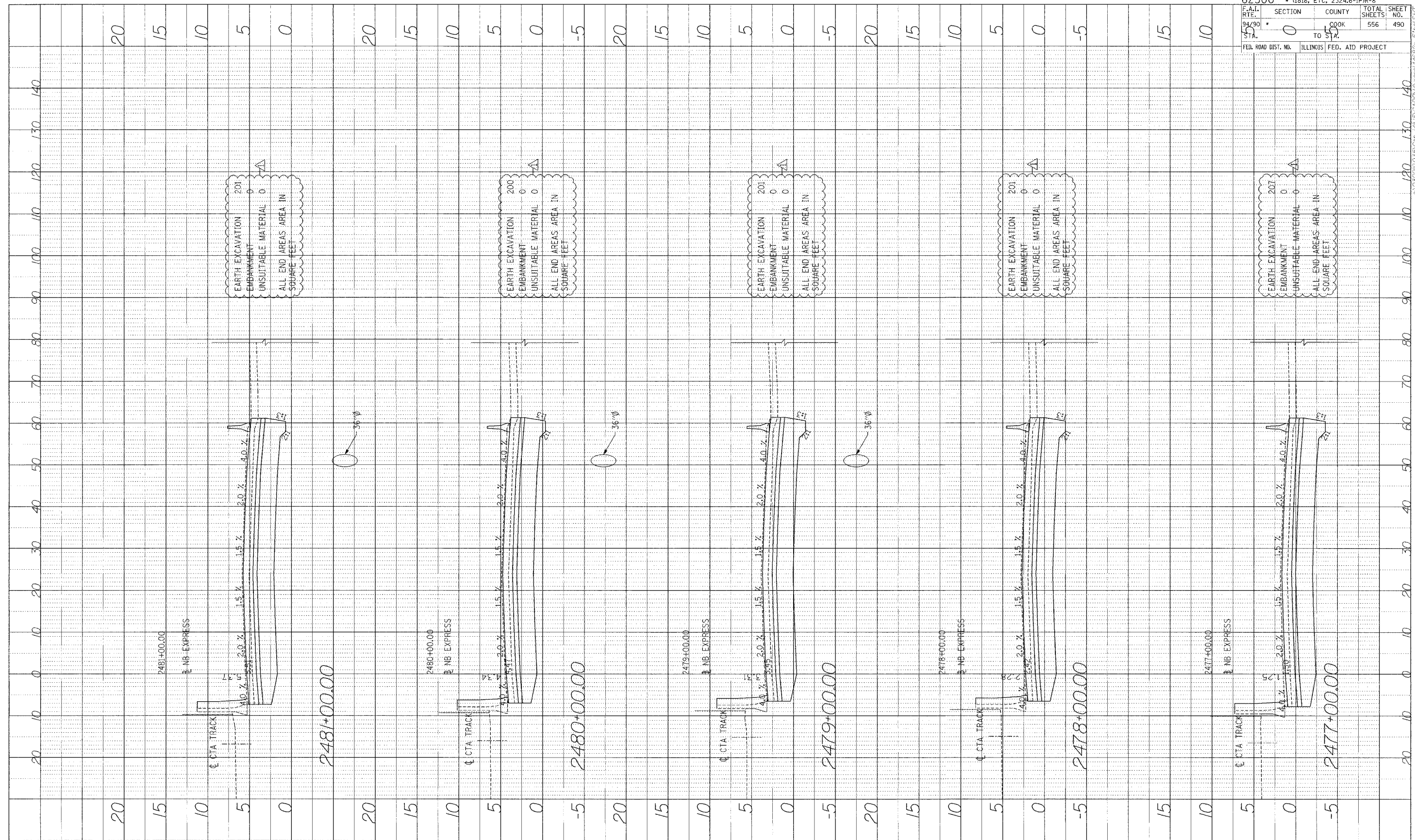


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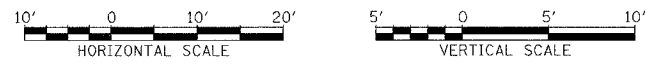
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PROFILE
 DRAWN BY: _____ DATE: _____
 CHECKED BY: _____
 NOTE BOOK NO. _____
 NO. _____

62300 • (1818, ETC. 2324.6-1)PJR-8
 SECTION COUNTY TOTAL SHEETS SHEET NO.
 94/90 TO COOK 556 490
 STA. TO STA.
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



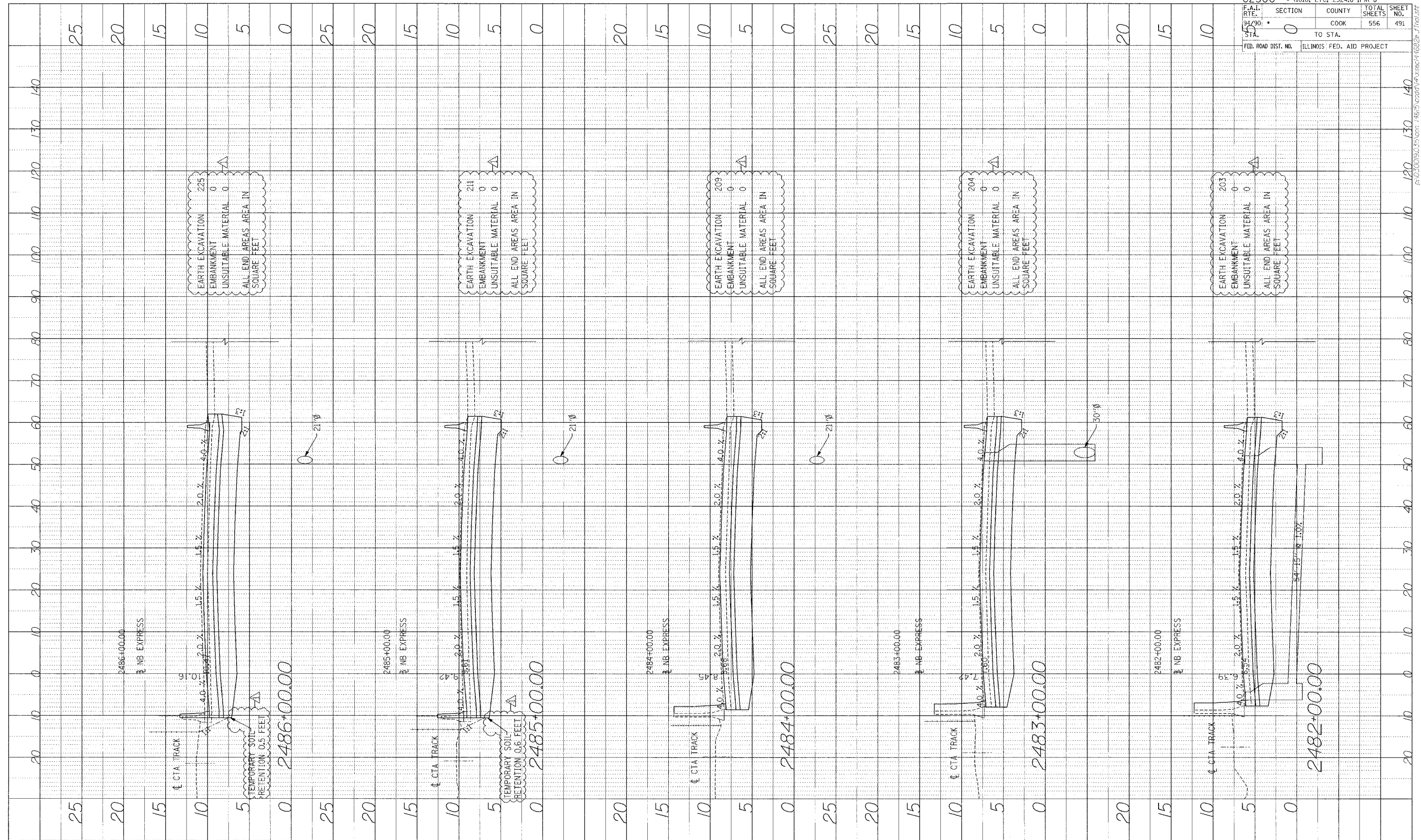
Edwards AND Kelcey
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 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



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PROFILE NUMBER: _____ BY: _____ DATE: _____
 GRADES OBTAINED FROM: _____
 NOTE BOOK NO.: _____
 STRUCTURE INDICATIVE OF THE: _____



62300 • (1818, ETC, 2324.6-1)PR-8	
F.A.I. RTE. 94/90	SECTION 0
COOK	COUNTY 556
TO STA.	TOTAL SHEETS 491
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

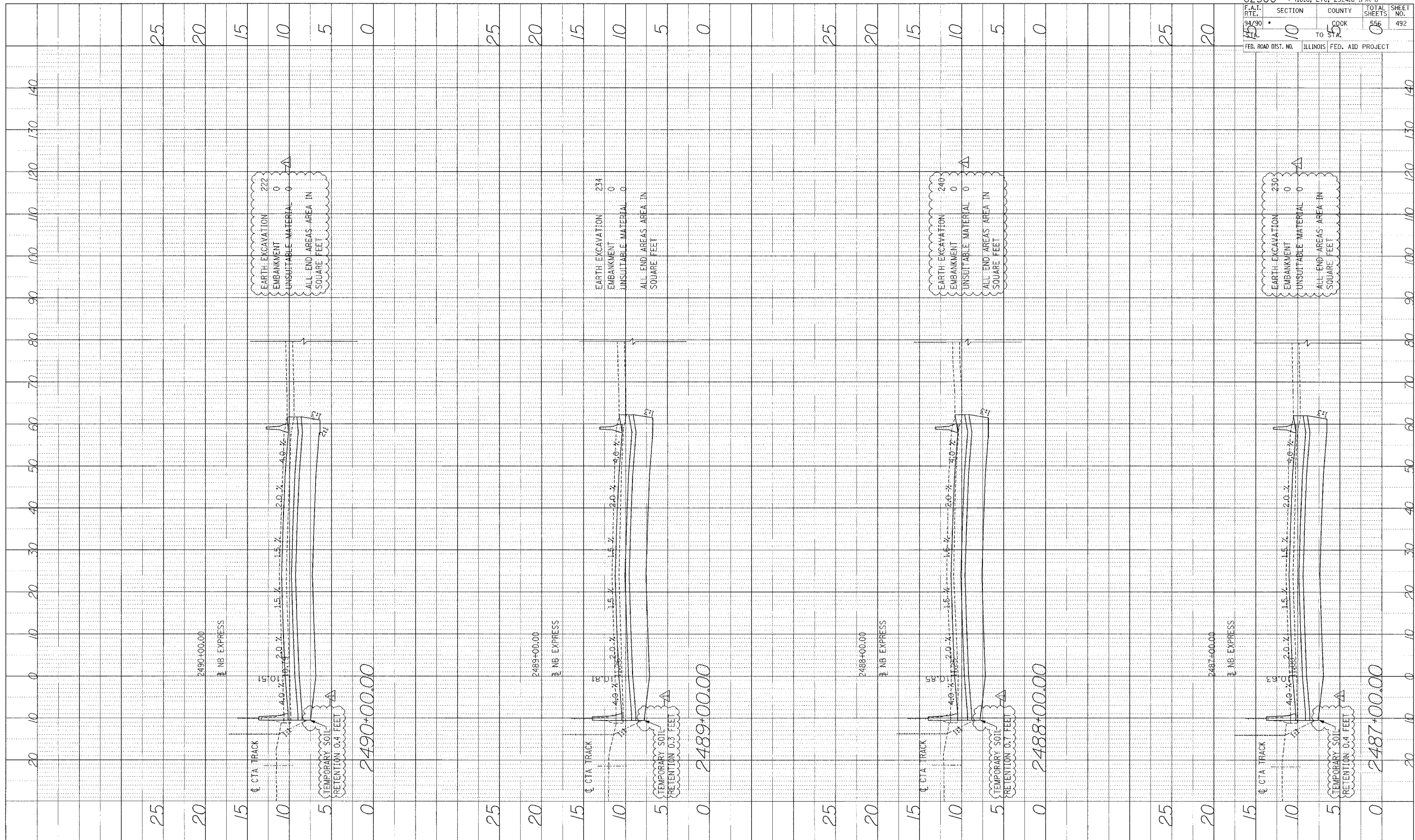
Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



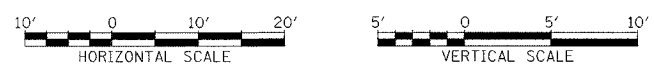
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PROFILE SURVEY
 PLOTTED
 NOTE BOOK
 NO. _____
 STRUCTURE ROTATING (CHD)



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 CHICAGO, IL 60606
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 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM

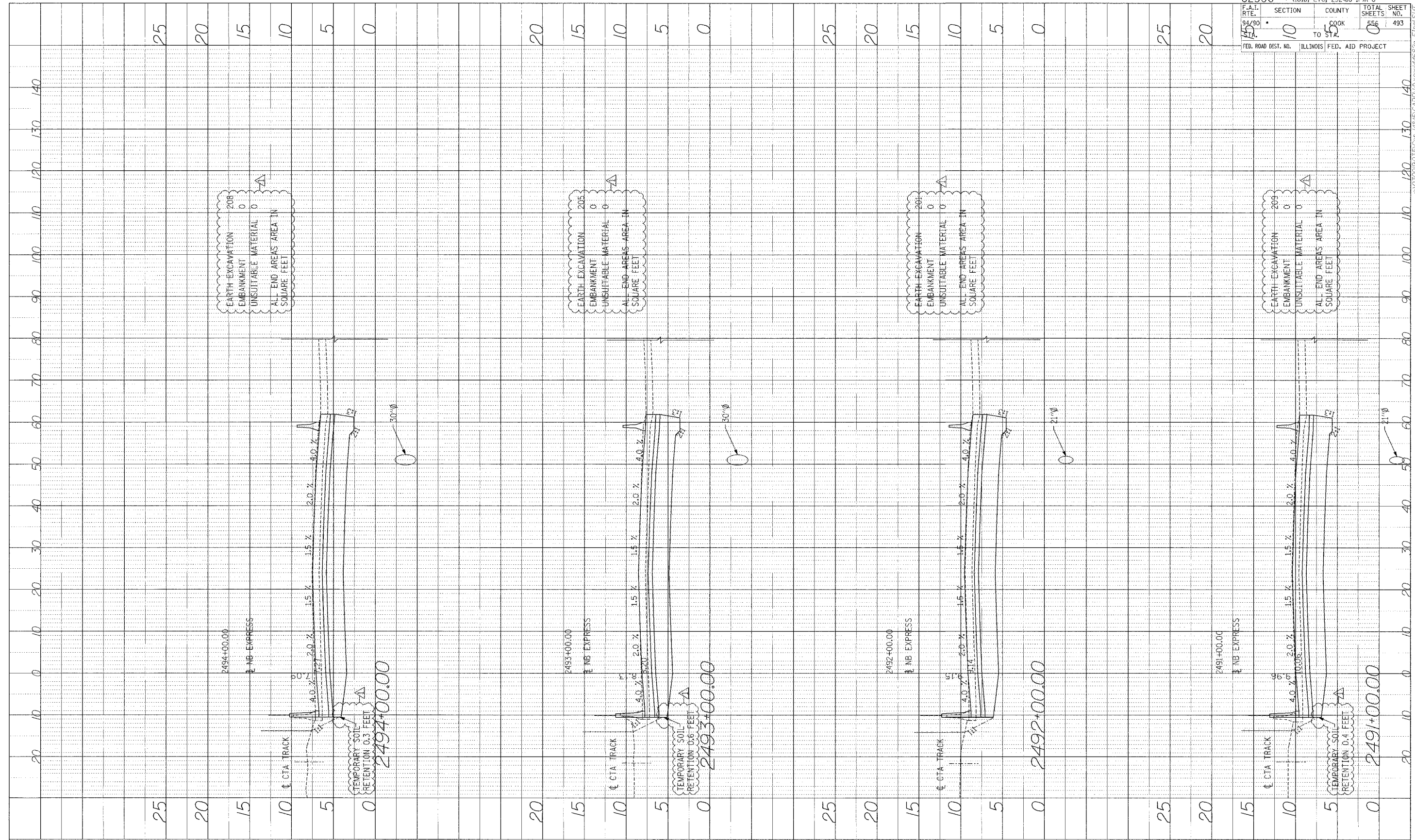


STA.

62300 • (1818, ETC, 2324,6-IPR-8
 F.A.I. SECTION COUNTY TOTAL SHEETS SHEET NO.
 94/90 10 COOK 556 492
 15 TO STA. 5
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

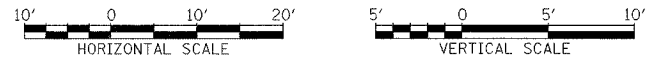
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PROFILE NUMBER: _____ BY: _____ DATE: _____
 DRAWN BY: _____
 CHECKED BY: _____
 NO. _____



62300 • (1818, ETC, 2324.6-1)PR-8	
F.A.L. RTE. 94/90	SECTION 10
TO STA. 55	COUNTY COOK
FED. ROAD DIST. NO. ILLINOIS	FED. AID PROJECT
TOTAL SHEETS 556	SHEET NO. 493

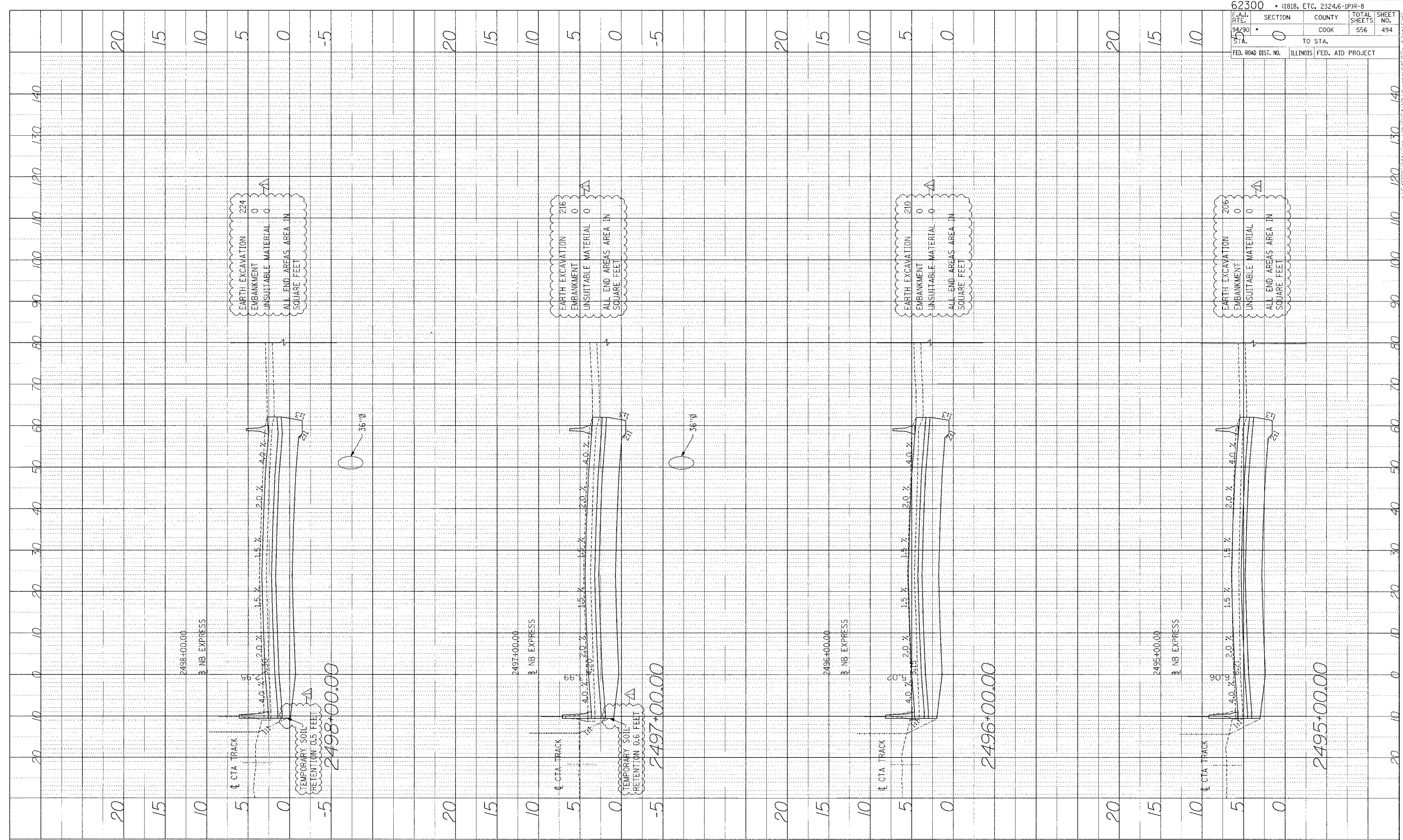
Edwards and Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



STA.

P:\030019035\Con 148\ICADD\A\Aves\44620-1\Prof\11T

PROFILE SURVEYED BY DATE
 OFFICER, CHECKER BY
 NO. STA. L.S. DATE
 NO. STA. L.S. DATE



Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM

10' 0 10' 20'
 HORIZONTAL SCALE

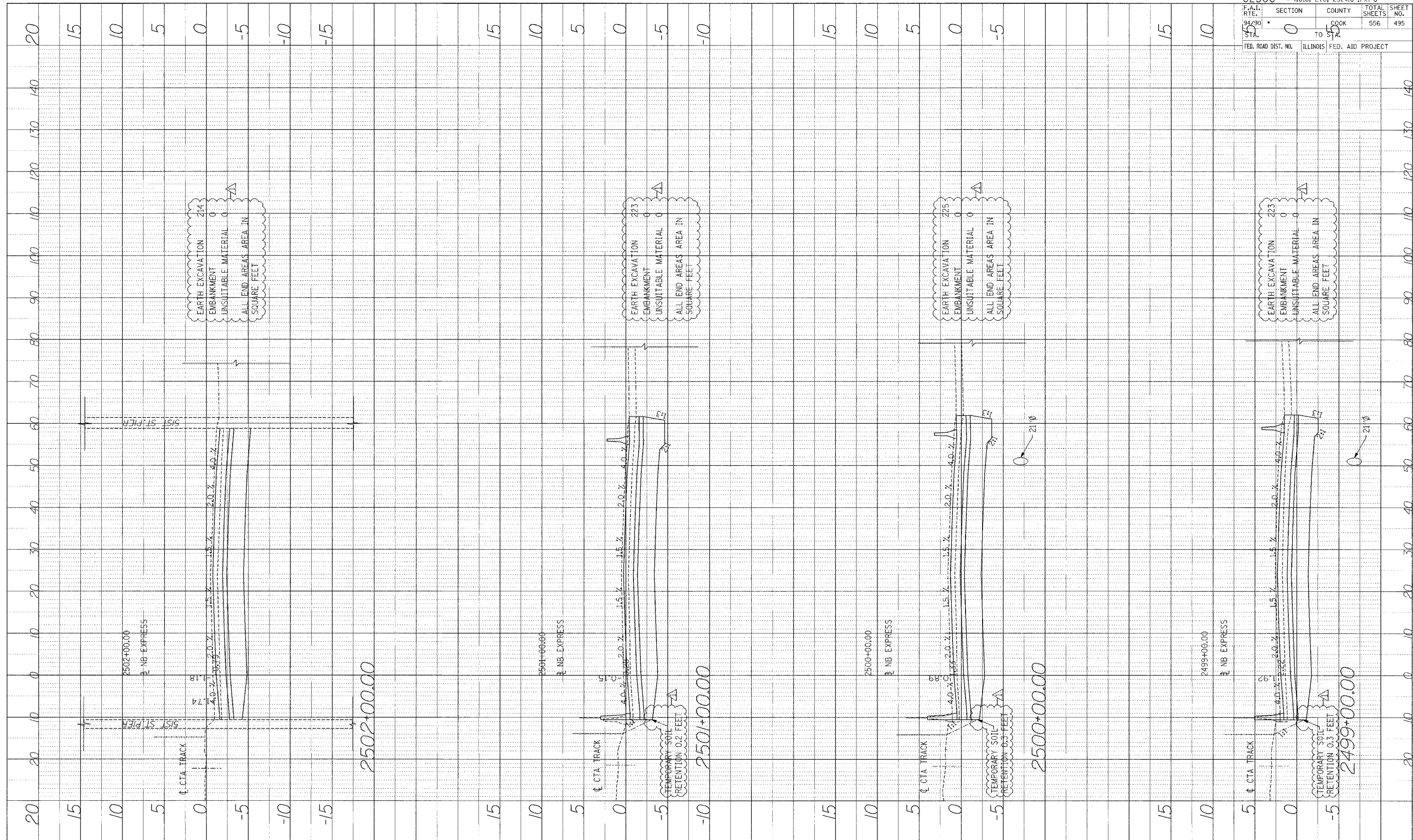
5' 0 5' 10'
 VERTICAL SCALE

STA.

62300 • (1818, ETC, 2324.6-1P)R-8	
F.A.I. SECTION	COUNTY TOTAL SHEETS
94/90 TO STA.	COOK 556 494
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

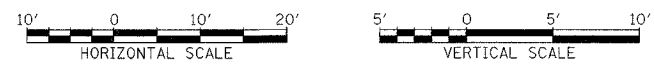
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PROFILE REQUESTED BY DATE
 NOTED BY
 NO. OF BOOKS BY
 NO. OF SHEETS BY
 NO. OF STRUCTURE RETAINING WALLS



62300	(1818, ETC, 2324.6-1PR-8
F.A.I. R.T.E. STA. 94/90	SECTION 0 TO 50
COUNTY COOK	TOTAL SHEETS 556
FED. ROAD DIST. NO. ILLINOIS	SHEET NO. 495
FED. AID PROJECT	

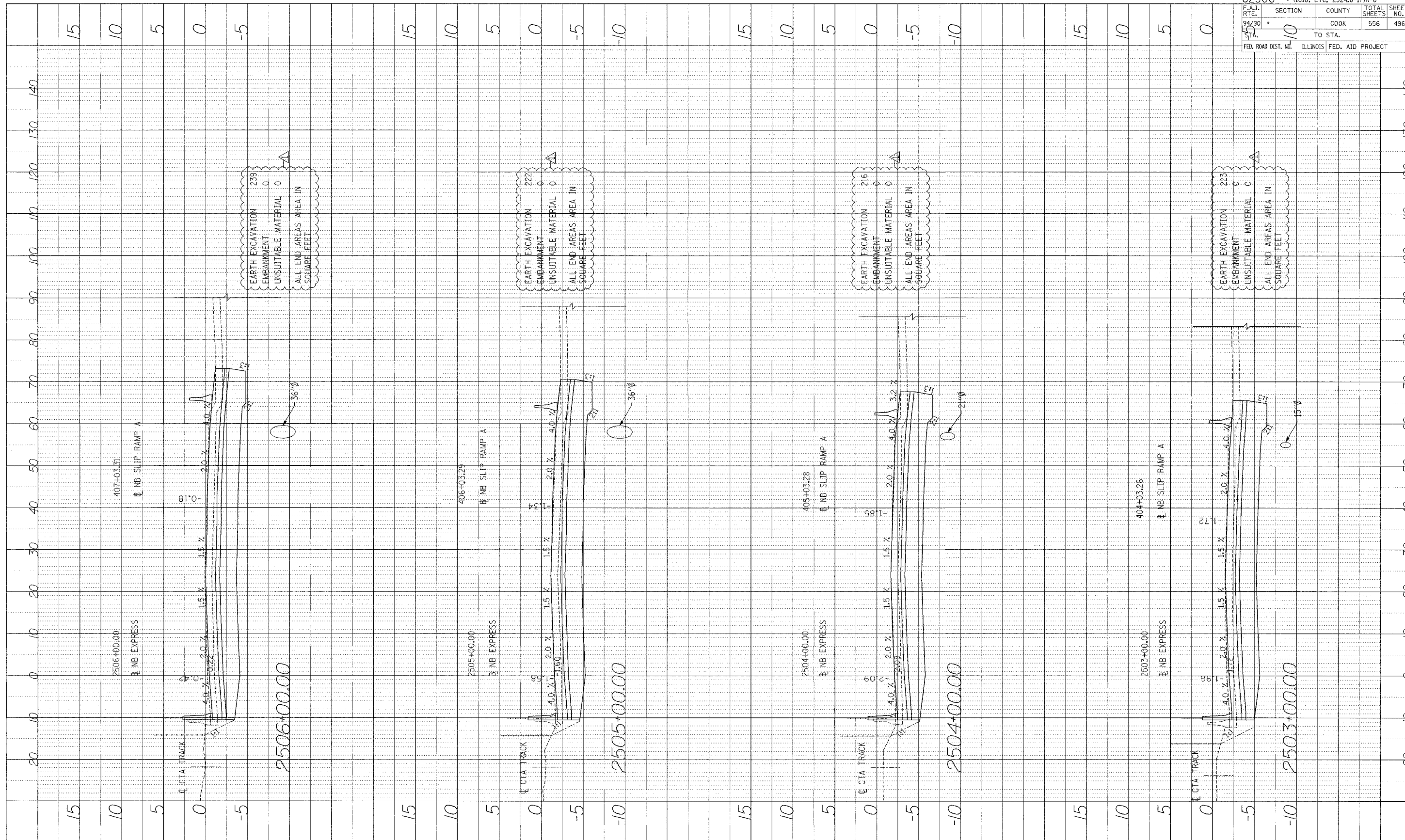
Edwards AND Kelcey
 ONE NORTH FRANKLIN CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



STA.

PA0300903500014815VADVAAX9014682P Final.SHT

PROFILE NUMBERED BY DATE
 FACILITY PROJECT BY
 NOTE BOOK SCALE PROJECTED BY
 NO. STRUCTURE REFERENCE CROSS



EARTH EXCAVATION 239
 EMBANKMENT 0
 UNSUITABLE MATERIAL 0
 ALL END AREAS AREA IN SQUARE FEET

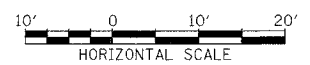
EARTH EXCAVATION 222
 EMBANKMENT 0
 UNSUITABLE MATERIAL 0
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 EMBANKMENT 0
 UNSUITABLE MATERIAL 0
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 EMBANKMENT 0
 UNSUITABLE MATERIAL 0
 ALL END AREAS AREA IN SQUARE FEET

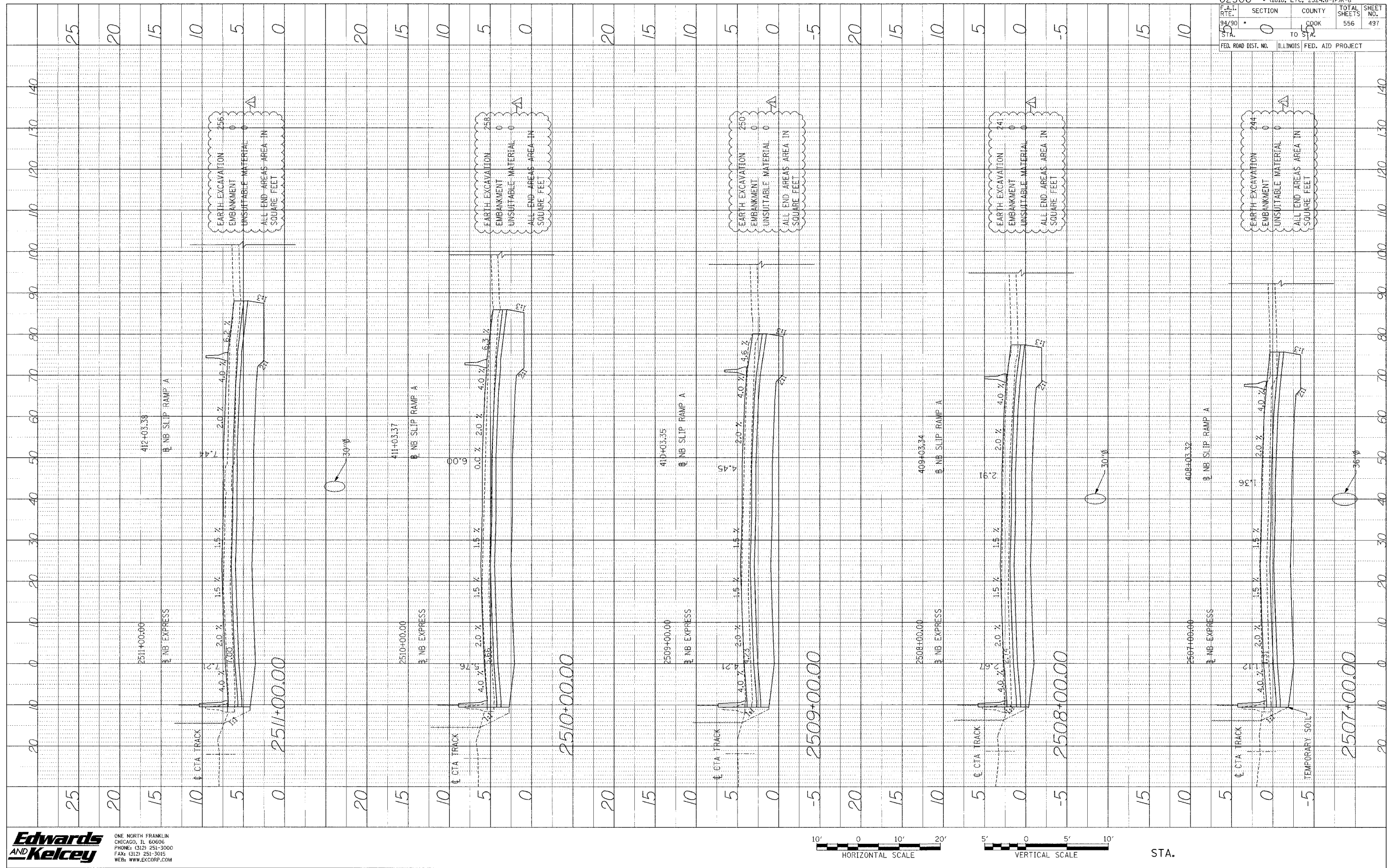
F.A.I. RTE. 94/30	SECTION 10	COUNTY COOK	TOTAL SHEETS 556	SHEET NO. 496
TO STA. 10		ILLINOIS FED. AID PROJECT		

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 ONE NORTH FRANKLIN CHICAGO, IL 60606
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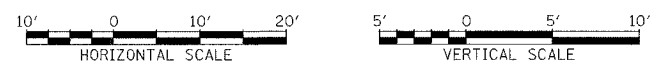
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PROFILE SURVEY BY: DATE:
 ACTE BODY: P. OLITO
 NO. : 10110
 STRUCTURE: NB EXPRESS
 NO. : 10110



62300 • (1818, ETC, 2324,6-1PR-8	
F.A.I. R.T.E. 94/90	SECTION 0
STA. 141	COUNTY COOK
	TOTAL SHEETS 556
	SHEET NO. 497
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

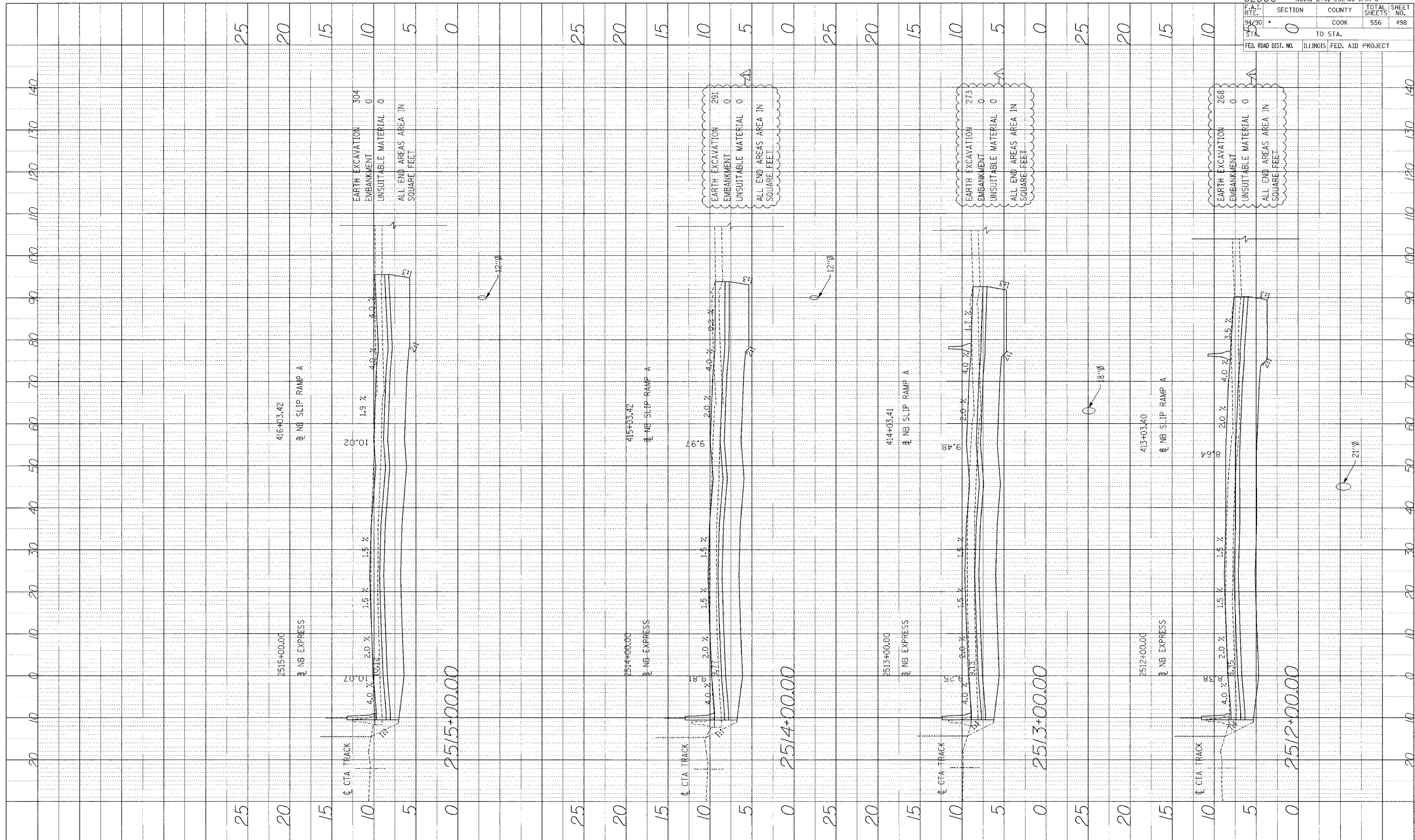
Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EDKORP.COM



STA.

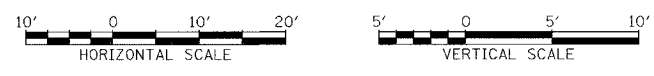
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PROFILE REVISIONS
 DATE BY
 NOTE BOOK
 NO. STRUCTURE NO. TYPE ORID



62300 • (1818, ETC, 2324.6-IPR-8			
F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS
94/90	0	COOK	556
DATA	TO STA.		498
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	

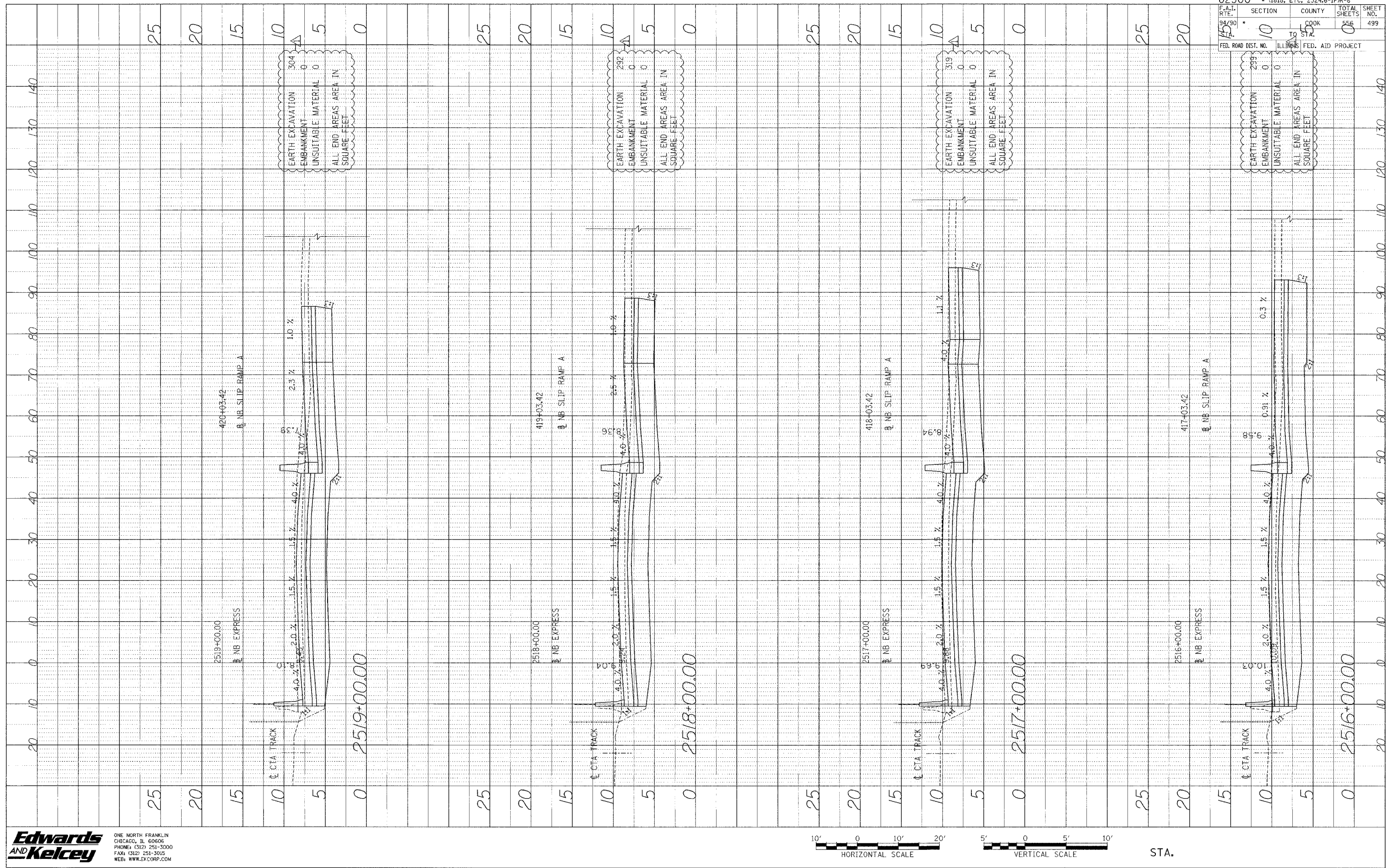
Edwards AND Kelcey
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 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
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STA.

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PROFILE SUBMITTED BY: _____ DATE: _____
 PLOTTED BY: _____
 NOTE BOOK NO. _____
 STRUCTURE NO. _____



EARTH EXCAVATION 304
 EMBANKMENT 0
 UNSUITABLE MATERIAL 0
 ALL END AREAS AREA IN SQUARE FEET 0

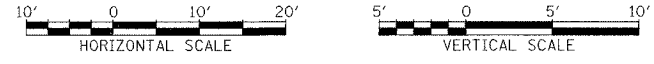
EARTH EXCAVATION 292
 EMBANKMENT 0
 UNSUITABLE MATERIAL 0
 ALL END AREAS AREA IN SQUARE FEET 0

EARTH EXCAVATION 319
 EMBANKMENT 0
 UNSUITABLE MATERIAL 0
 ALL END AREAS AREA IN SQUARE FEET 0

EARTH EXCAVATION 295
 EMBANKMENT 0
 UNSUITABLE MATERIAL 0
 ALL END AREAS AREA IN SQUARE FEET 0

62300	(1818, ETC. 2324.6-IPR-8)		
F.A.I. RTE. 94/90	SECTION 10	COUNTY COOK	TOTAL SHEETS 556
145	STA. 145	ILLINOIS	SHEET NO. 499
FED. ROAD DIST. NO.	FED. AID PROJECT		

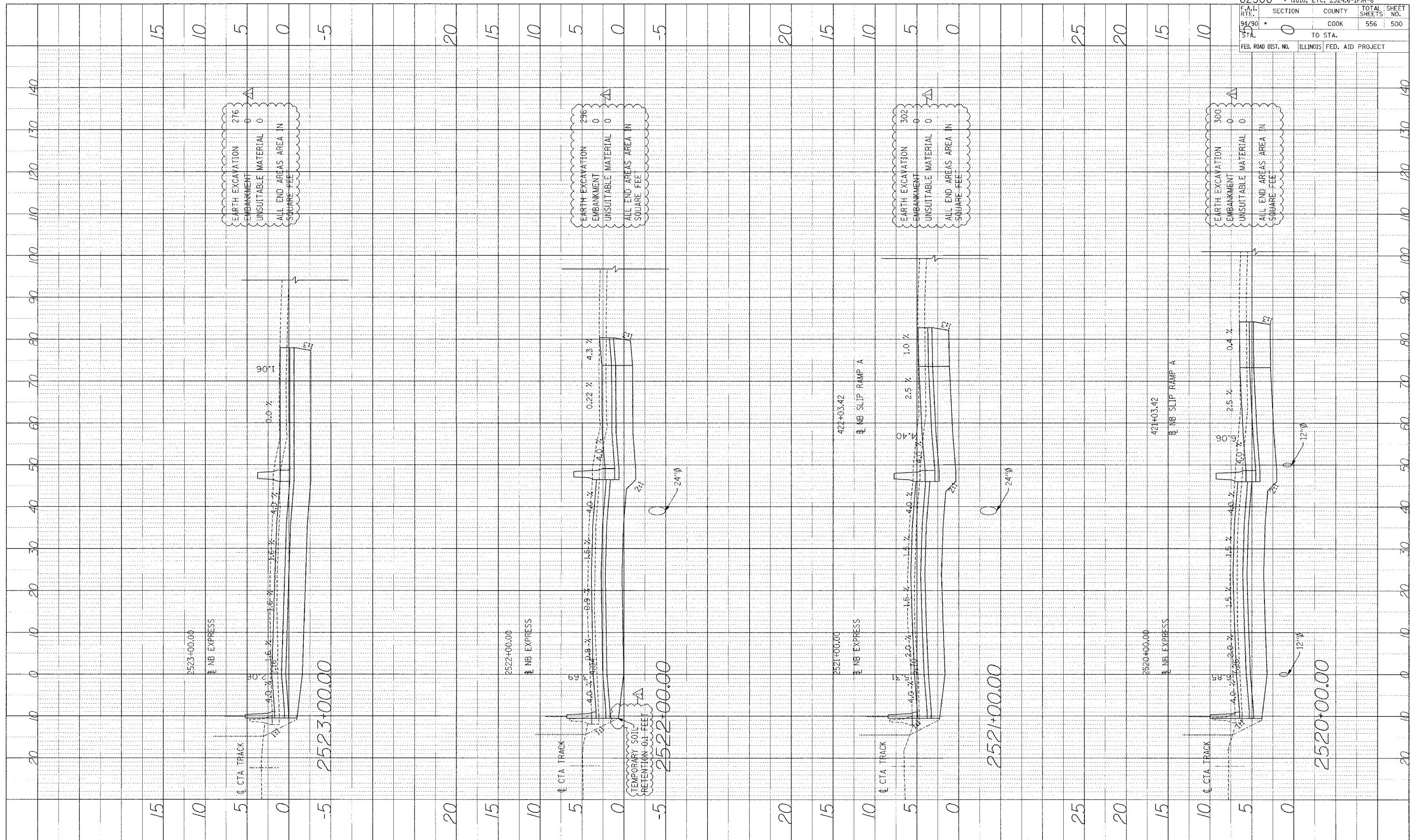
Edwards AND Kelcey
 ONE NORTH FRANKLIN CHICAGO, IL 60606
 PHONE: (312) 251-3000
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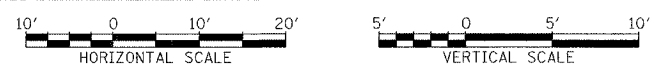
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PROFILE SUBMITTED BY DATE
 PLOTTED BY
 NOTE BOOK SERIAL NO. DATE
 NO. STRUCTURE NO. CHG



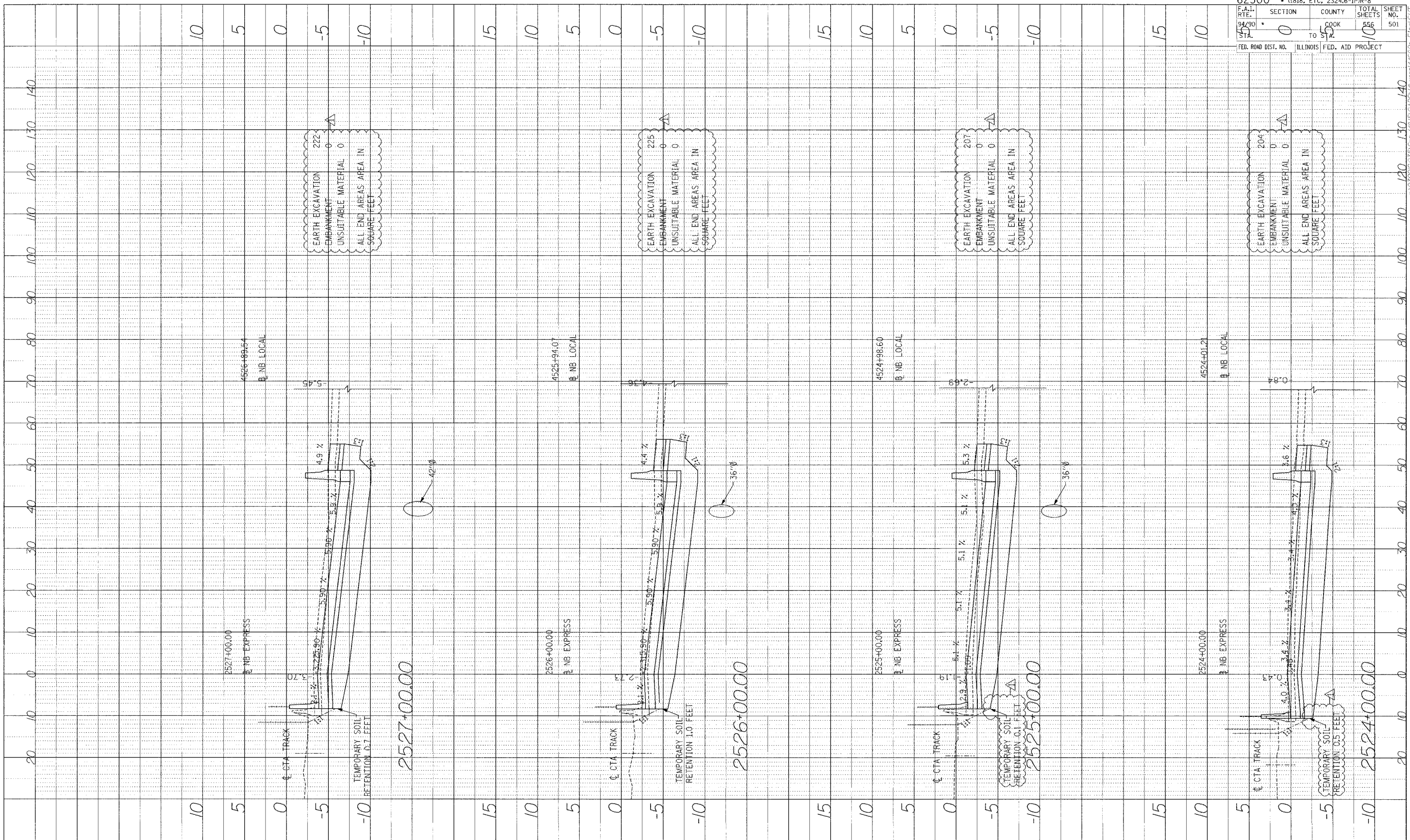
Edwards AND Kelcey
 ONE NORTH FRANKLIN CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



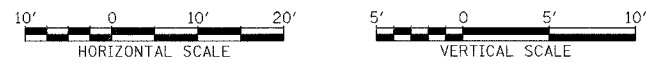
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F.A.I. RTE. 94/90 STA. 45	SECTION 0
COUNTY COOK	TOTAL SHEETS 556
TO STA. 50	SHEET NO. 500
FED. ROAD DIST. NO. ILLINOIS	FED. AID PROJECT

PAG 50093035.Con 14818NAD04\FX88944528.Final.SIT

PROFILE PLANTED BY: DATE:
 NOTE BOOK NO. FRANKS CHECKED: STRUCTURE NUMBER: OTHER:
 NO. STRUCTURE NUMBER: OTHER:



Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM

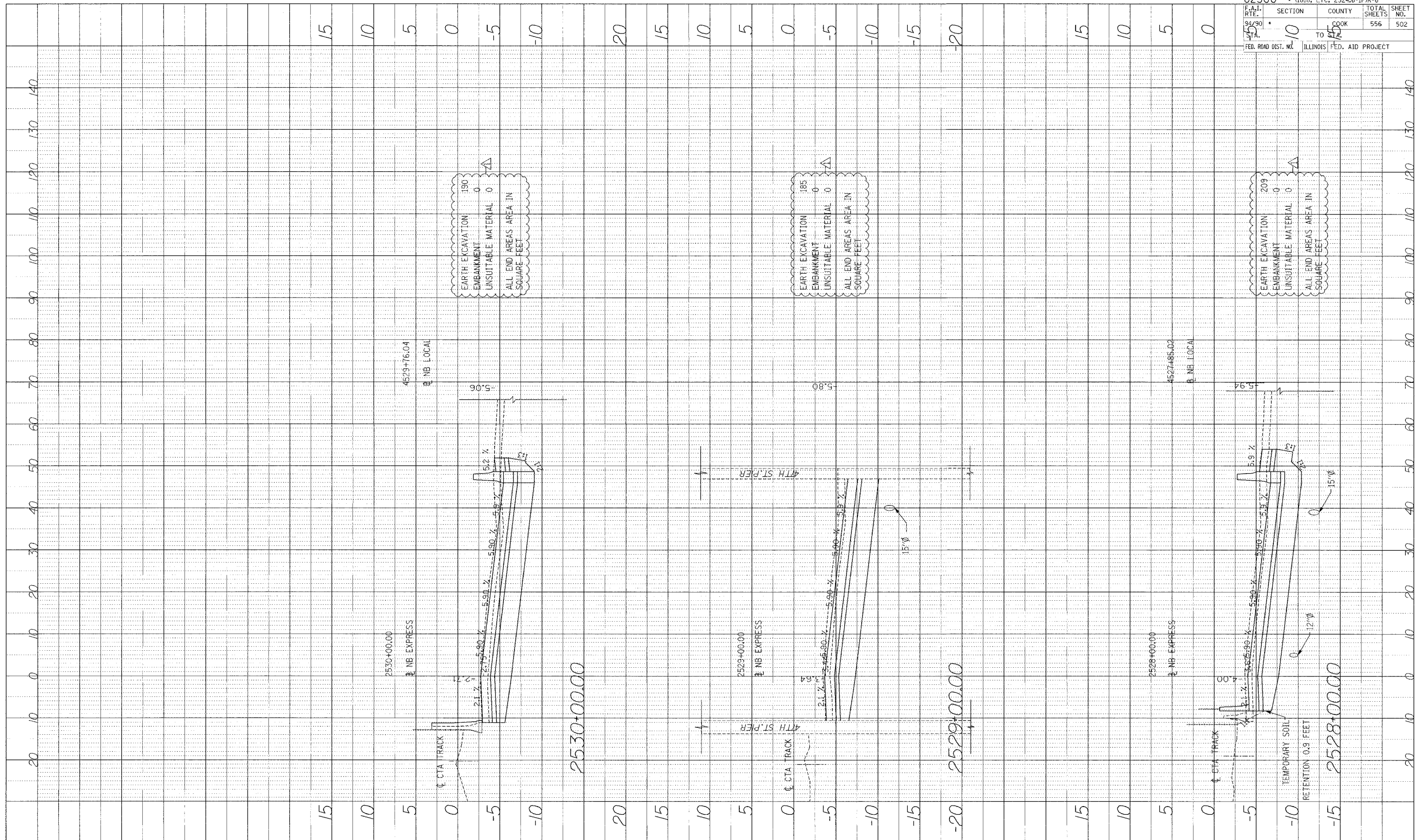


STA.

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F.A.I. RTE.	SECTION	COUNTY	556	501	
94/90	0	COOK	10	10	
STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	

P:\03006035\CAD\1481\50-AD\1481\50-AD\Final\51T

PROJECT: SURVEYED
 DRAWN: []
 DATE: []
 CHECKED: []
 DATE: []
 TITLE: []



62300 • (1818, ETC, 2324.6-IPR-8
 F.A.I. RTE. 94/90
 SECTION 15
 COUNTY COOK
 TOTAL SHEETS 556
 SHEET NO. 502
 TO ILLINOIS
 FED. ROAD DIST. NO. ILLINOIS
 FED. AID PROJECT

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 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-5000
 FAX: (312) 251-3015
 WEB: WWW.ECKORP.COM

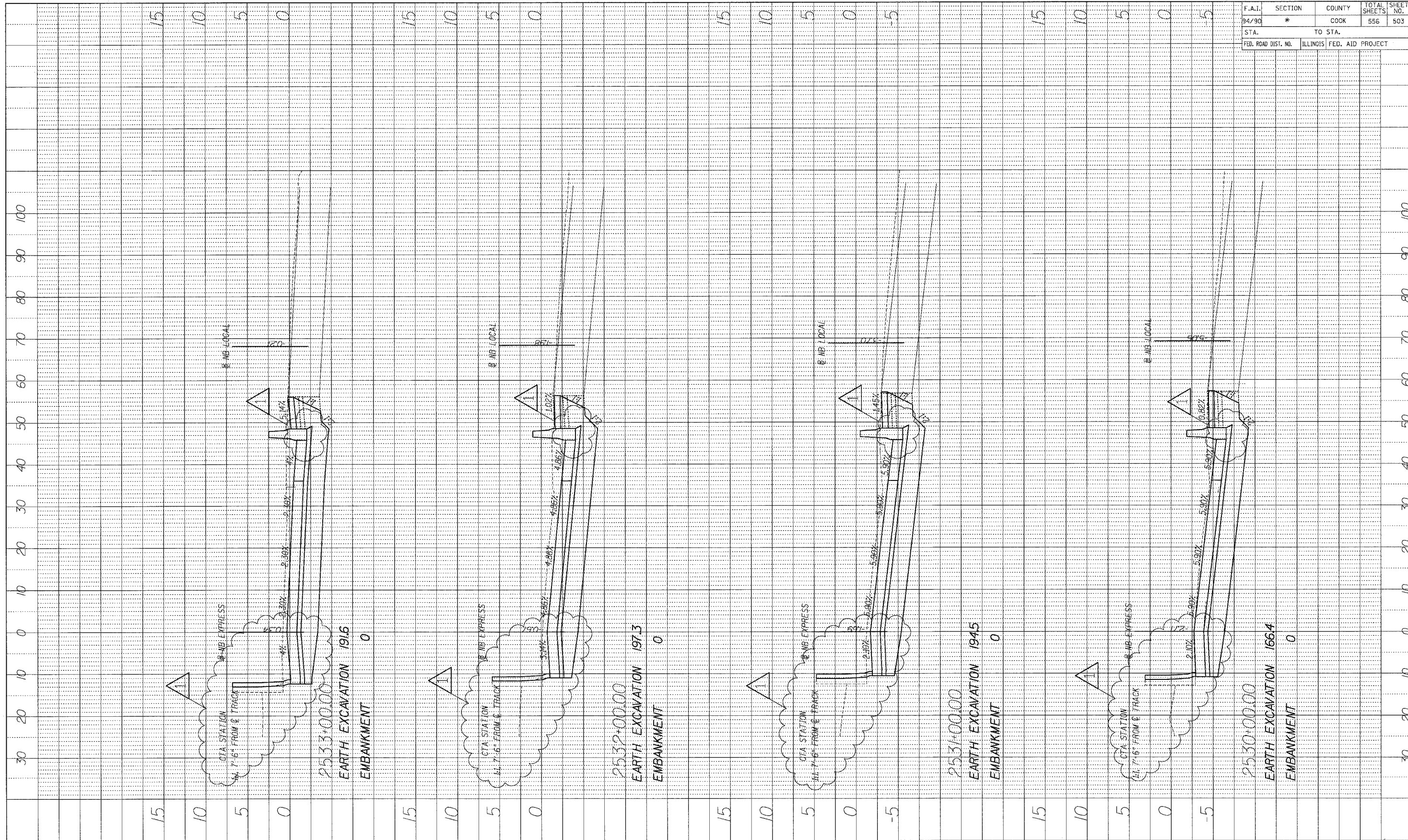


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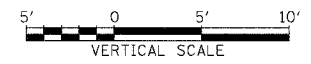
F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90	*	COOK	556	503
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

PROFILE	SURVEYED	BY	DATE
NOTE BOOK	GRADES CHECKED		
NO.	BY		
	NOTATIONS		



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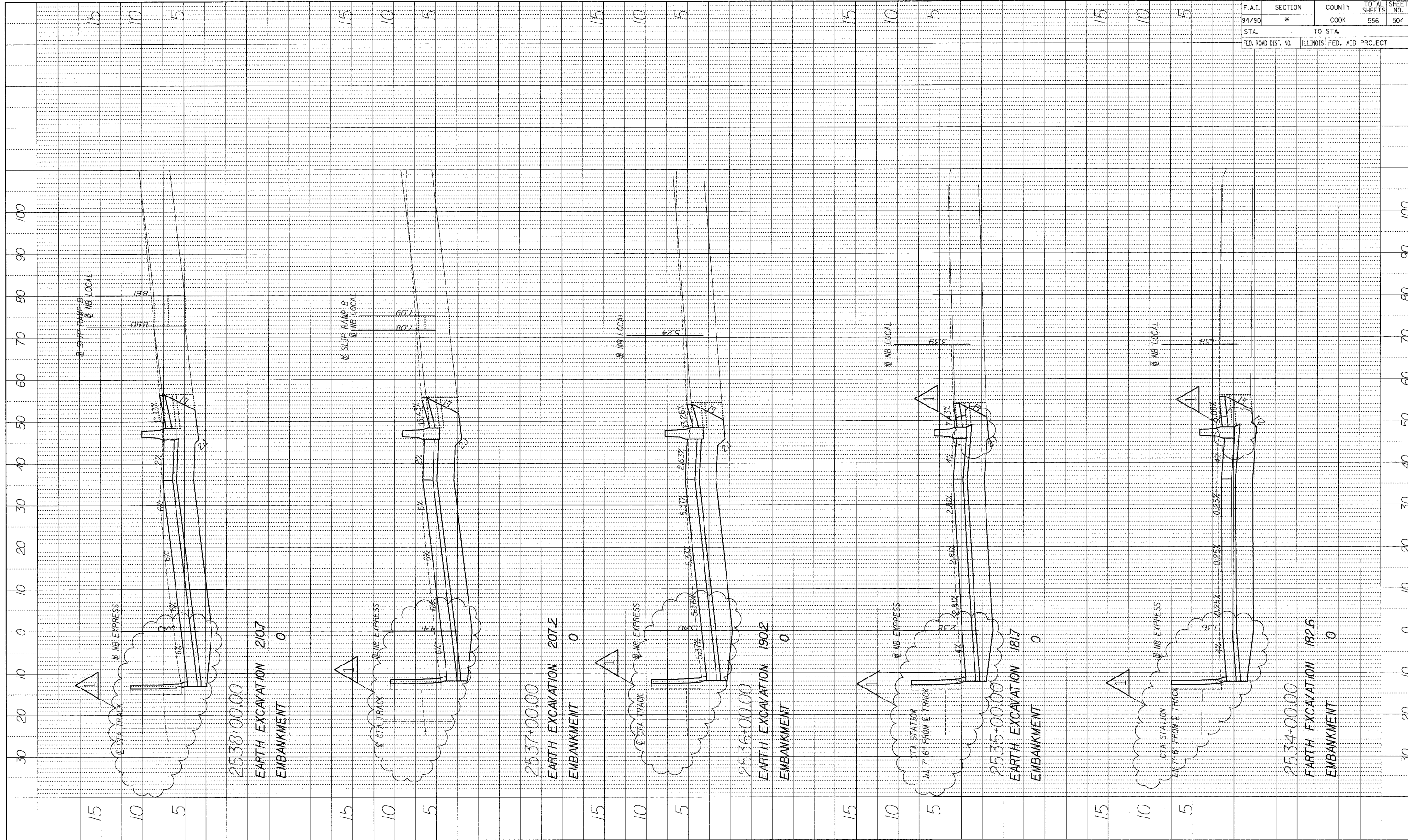
1 ADDENDUM 1 8/12/05



STA.+... TO STA.+...

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94/90	*	COOK	556	504
STA. _____		TO STA. _____		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PROFILE SURVEYED	DATE
GRADES CHECKED	BY
NOTE BOOK	
NO.	



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1 ADDENDUM 1 8/12/05

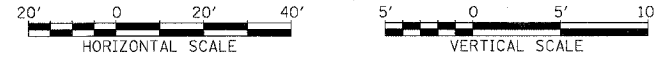
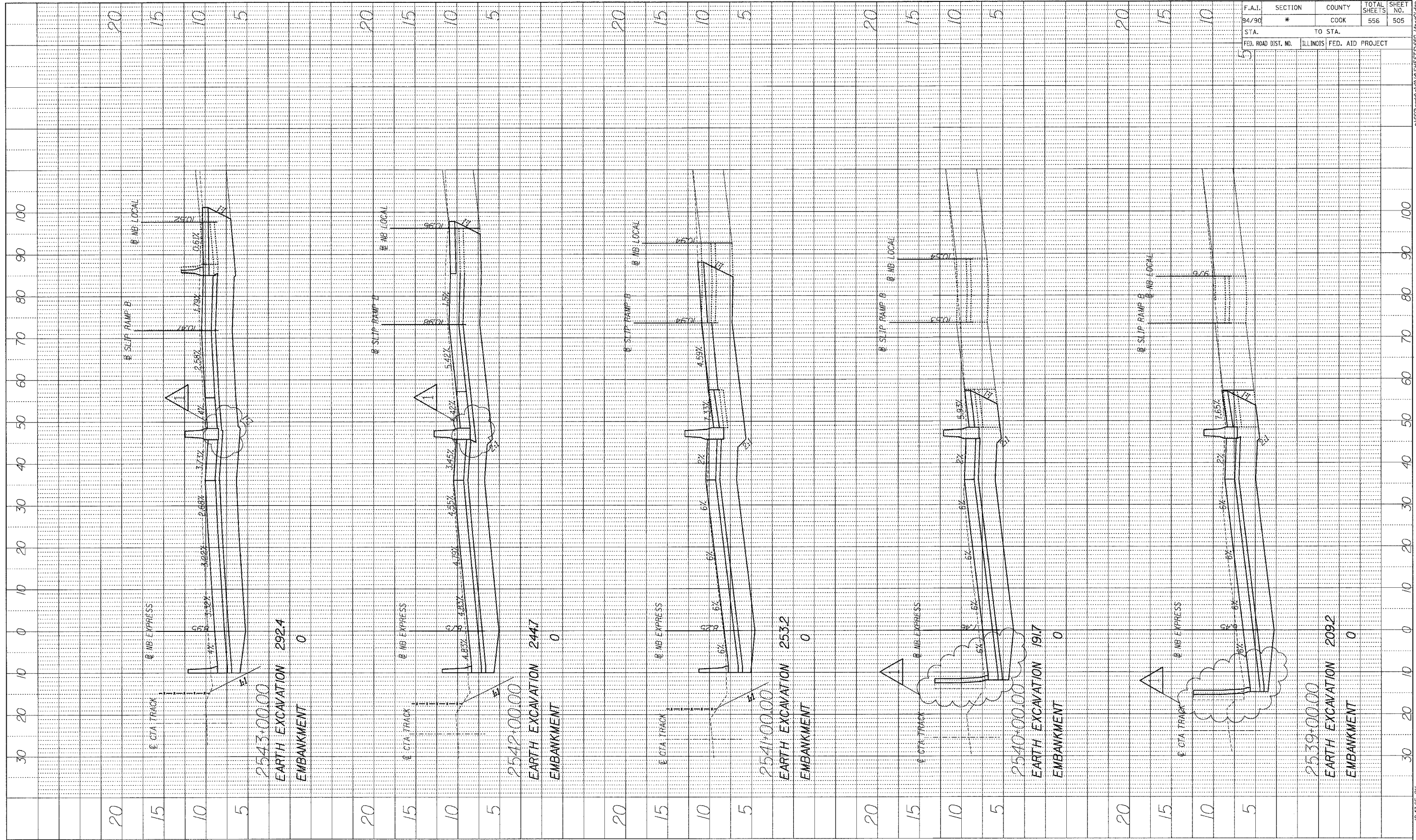


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94/90	*	COOK	556	505
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

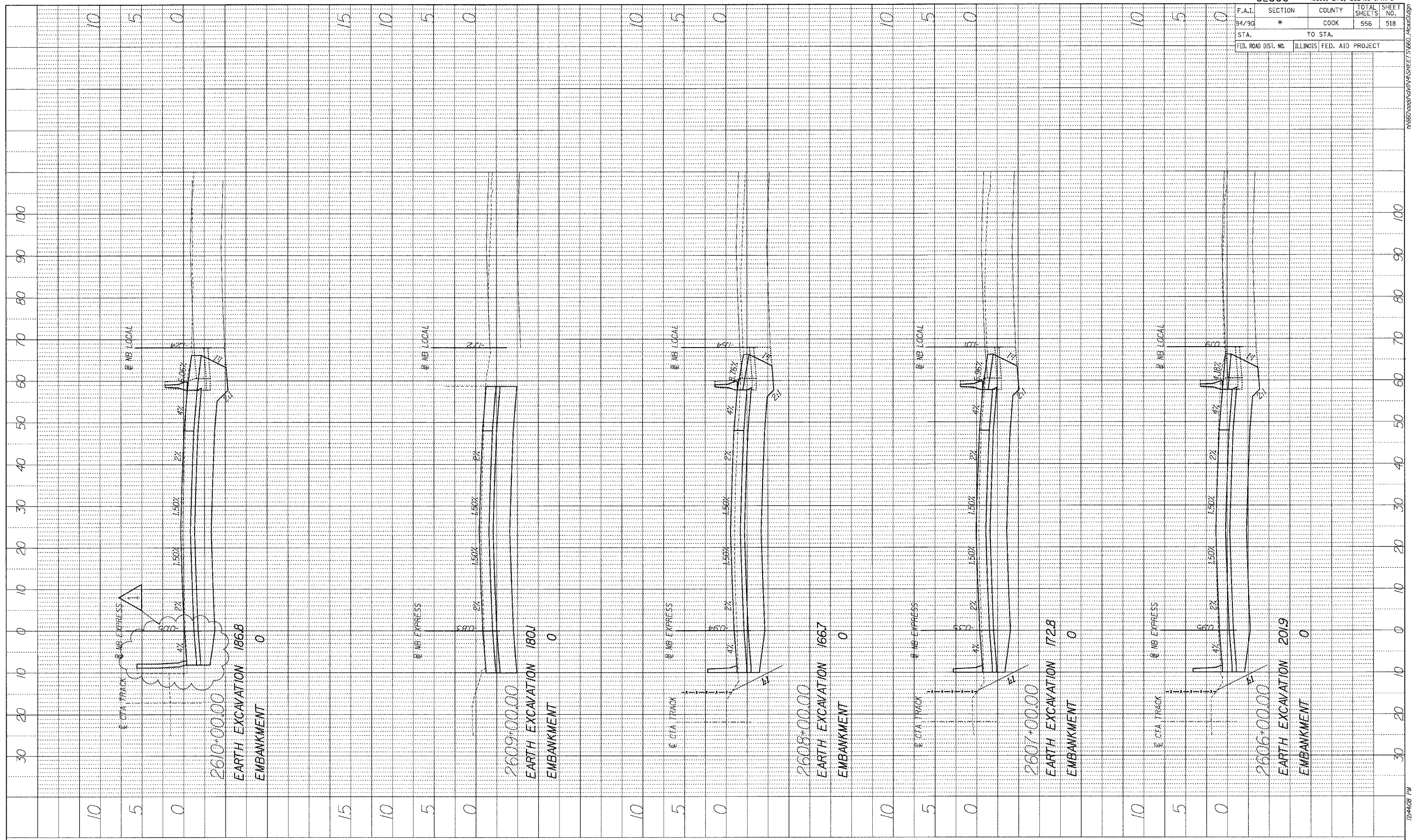
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NOTE BOOK	GRADES CHECKED		
	PLAN NOTED		
	FIELD DATA		
	REVISIONS		



12/4/2005 12:43:25 PM

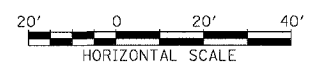
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94/90	*	COOK	556	518
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PROFILE	SURVELED	BY	DATE
NOTE BOOK	GRADES CHECKED		
NO.	REVISIONS		
	DESCRIPTION		
	DATE		



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1 ADDENDUM 1 8/12/05

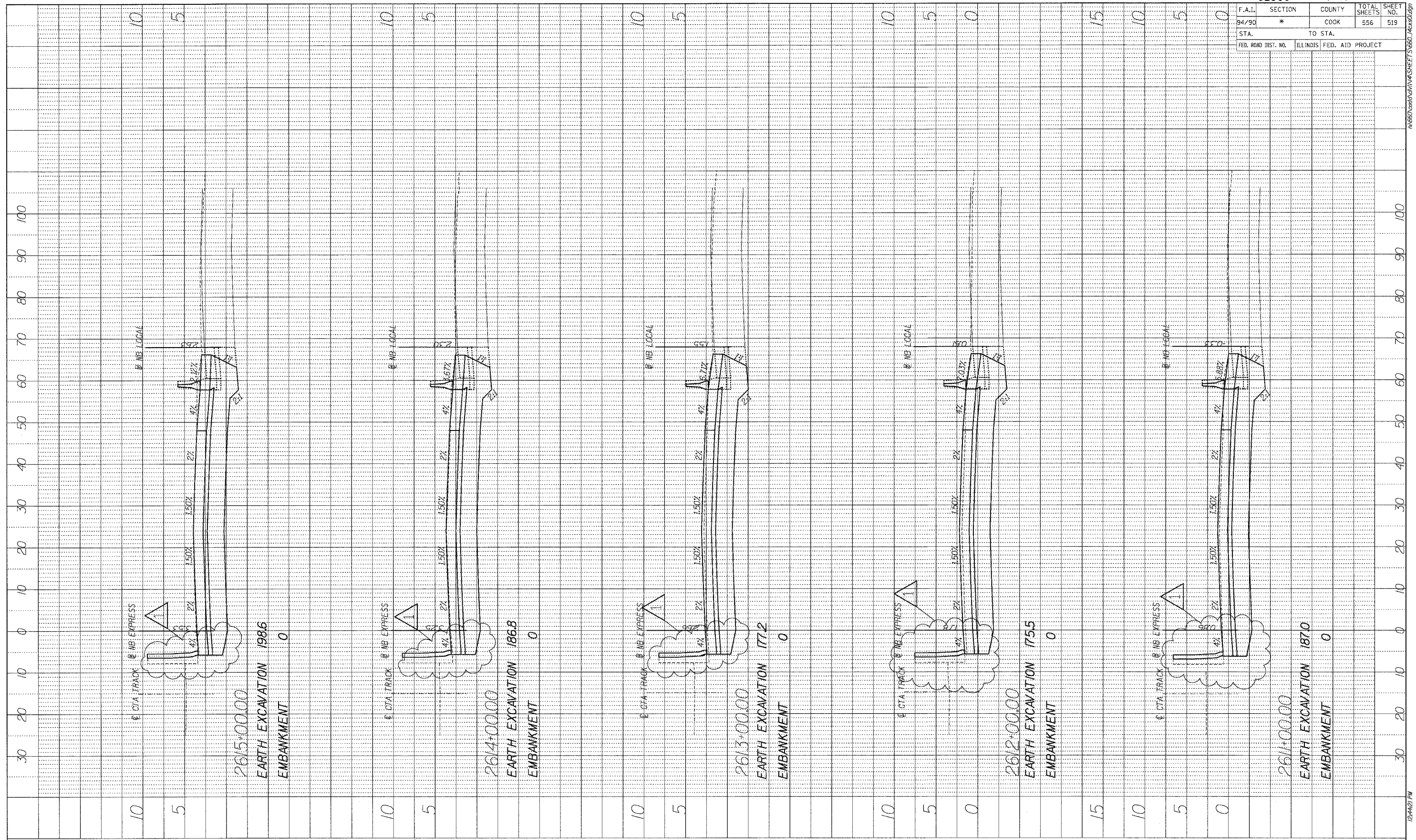


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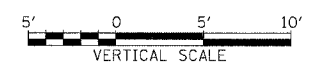
F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90	*	COOK	556	519
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PROFILE	DATE
SURVEYED	BY
GRADES CHECKED	
BLM. NOTED	
STRUCTURE NOTATIONS	



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1 ADDENDUM 1 8/12/05

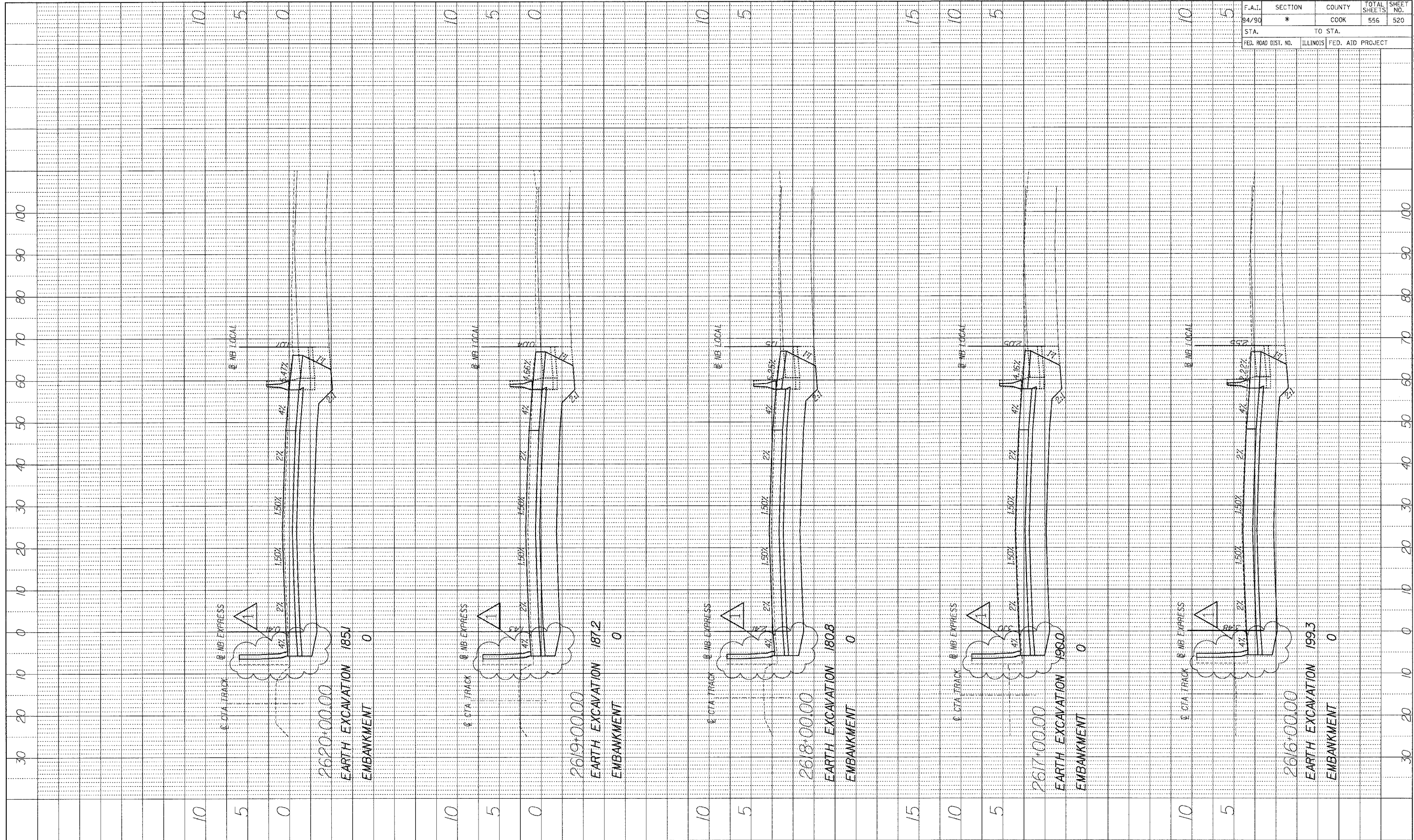


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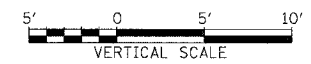
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84/90	*	COOK	556	520
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PROFILE	SURVEYED	DATE
NOTE BOOK	DESIGNED	
NO.	CHECKED	
	STRUCTURE NOTATIONS	CPWD



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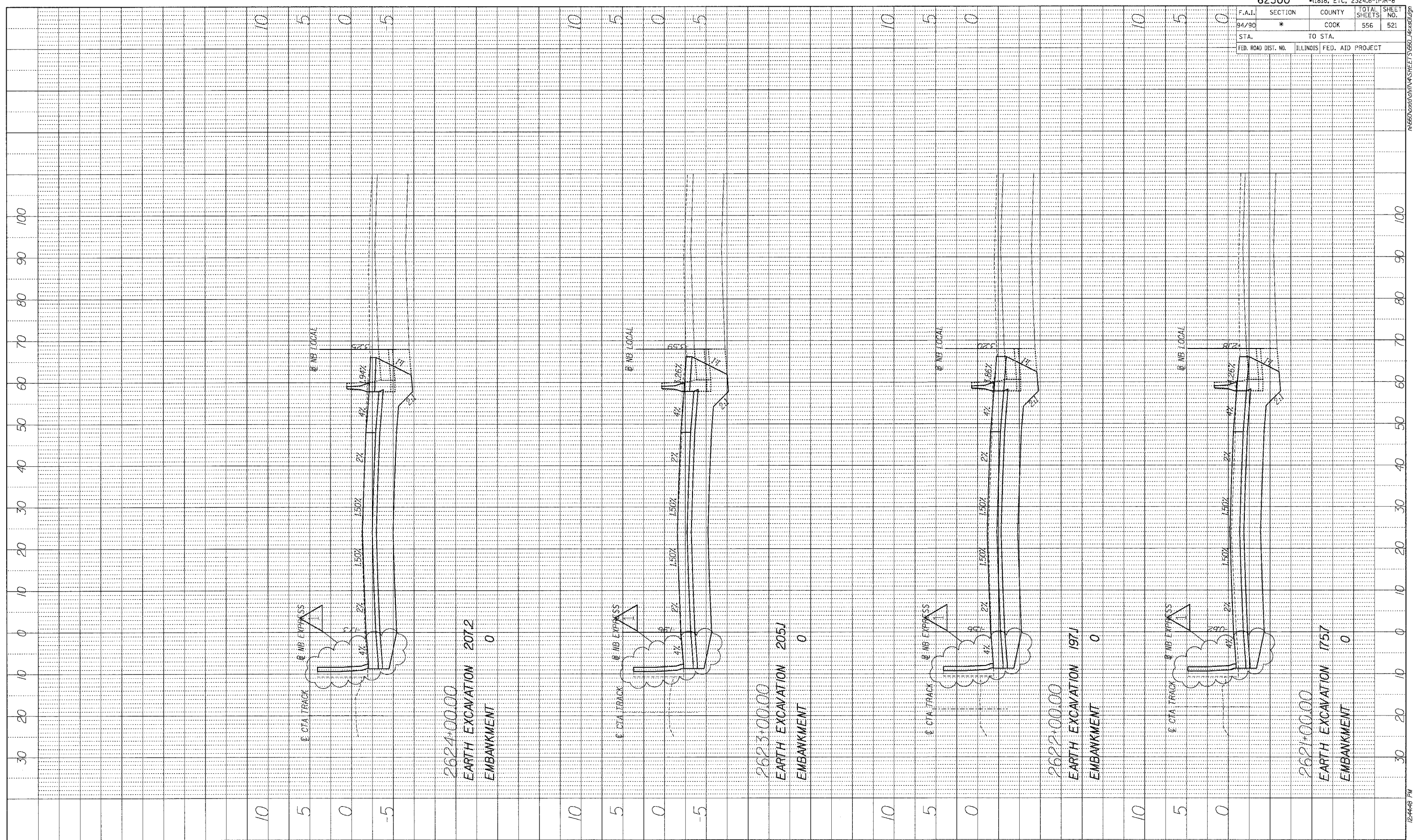
ADDENDUM 1 8/12/05



STA. ----+-- TO STA. ----+--

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90	*	COOK	556	521
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PROFILE	SURVEY	DATE
NOTE BOOK	GRADES CHECKED	
NO.	STRUCTURE NOTATIONS C/P/K/D	



2624+00.00
EARTH EXCAVATION 207.2
EMBANKMENT 0

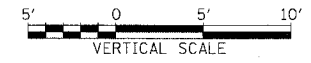
2623+00.00
EARTH EXCAVATION 205.1
EMBANKMENT 0

2622+00.00
EARTH EXCAVATION 197.1
EMBANKMENT 0

2621+00.00
EARTH EXCAVATION 175.7
EMBANKMENT 0

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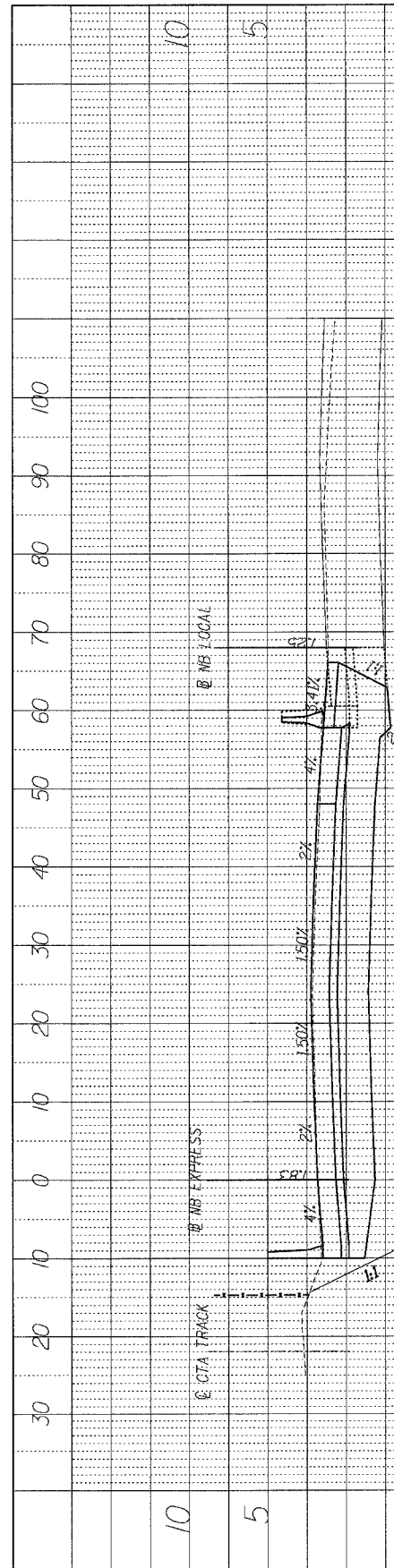
1 ADDENDUM 1 8/12/05



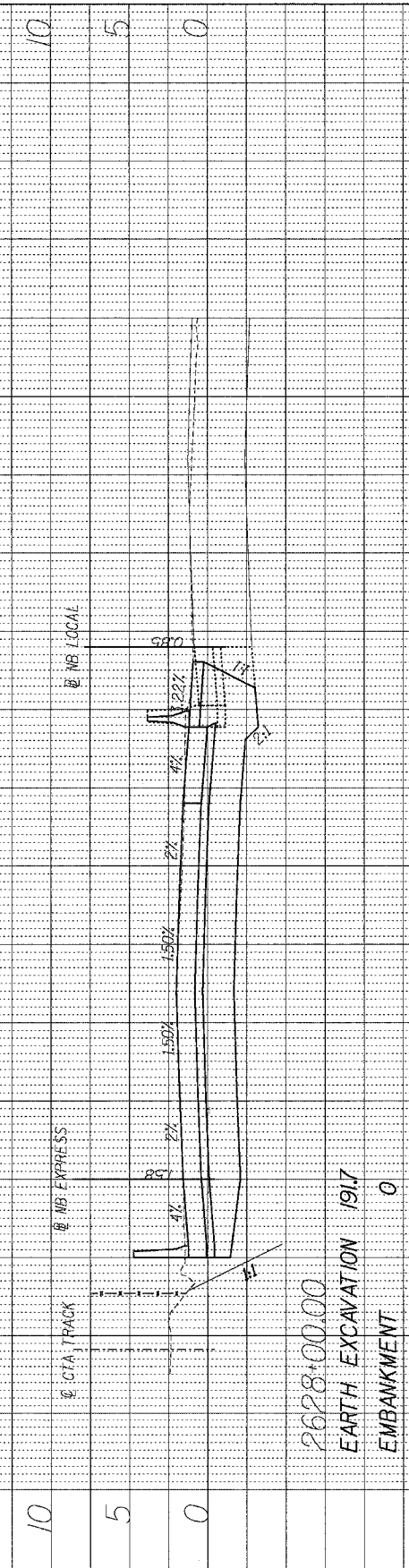
STA.+.. TO STA.+..

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90	*	COOK	556	522
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

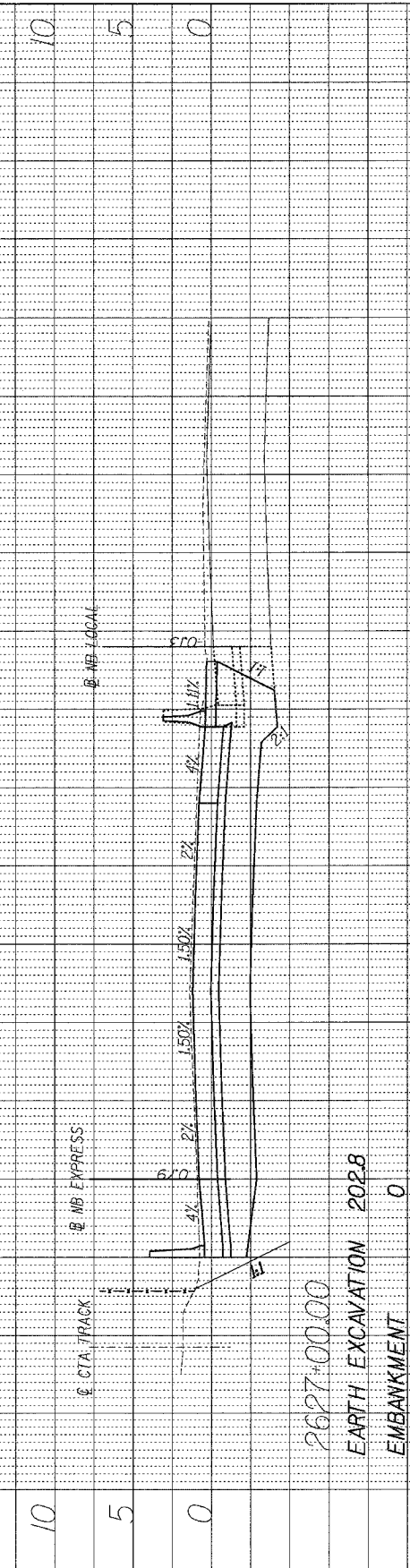
PROFILE	DATE
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	
DATE	



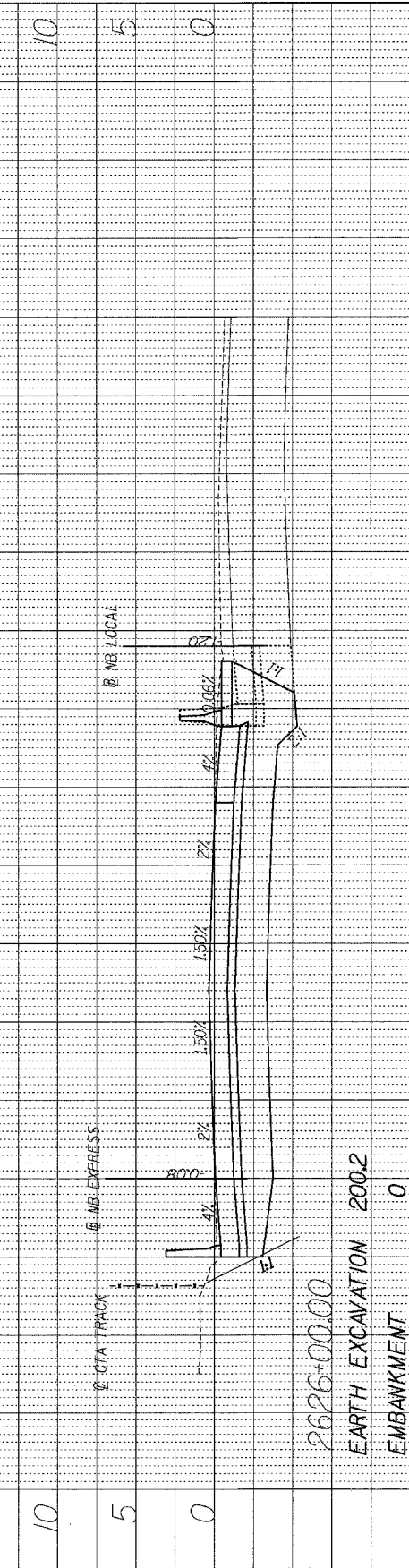
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EARTH EXCAVATION 1909
EMBANKMENT 0



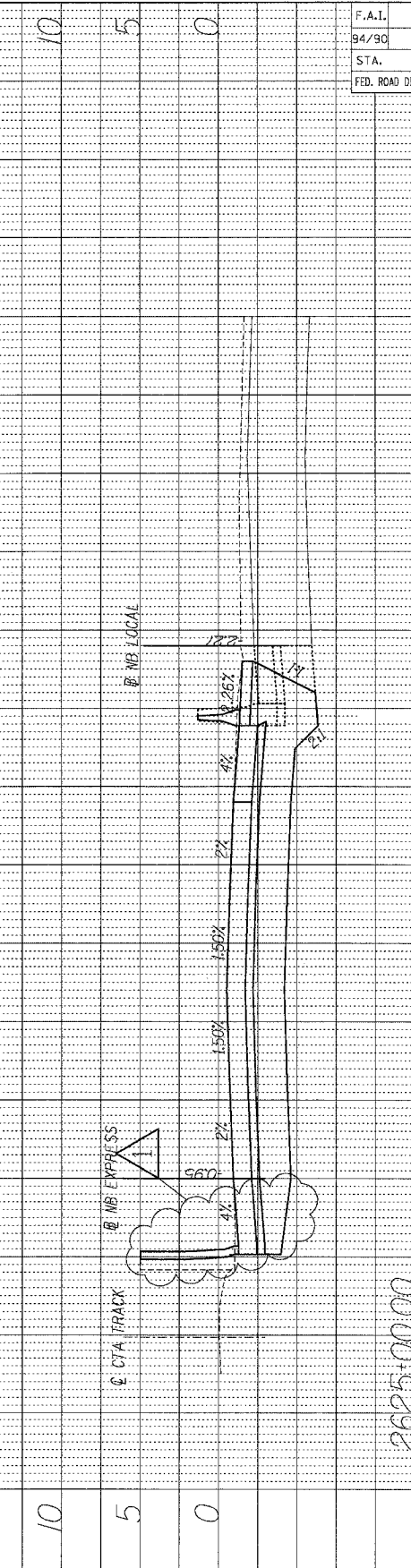
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EARTH EXCAVATION 1917
EMBANKMENT 0



2627+00.00
EARTH EXCAVATION 2028
EMBANKMENT 0



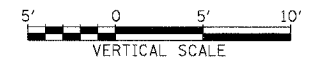
2626+00.00
EARTH EXCAVATION 2002
EMBANKMENT 0



2625+00.00
EARTH EXCAVATION 1968
EMBANKMENT 0

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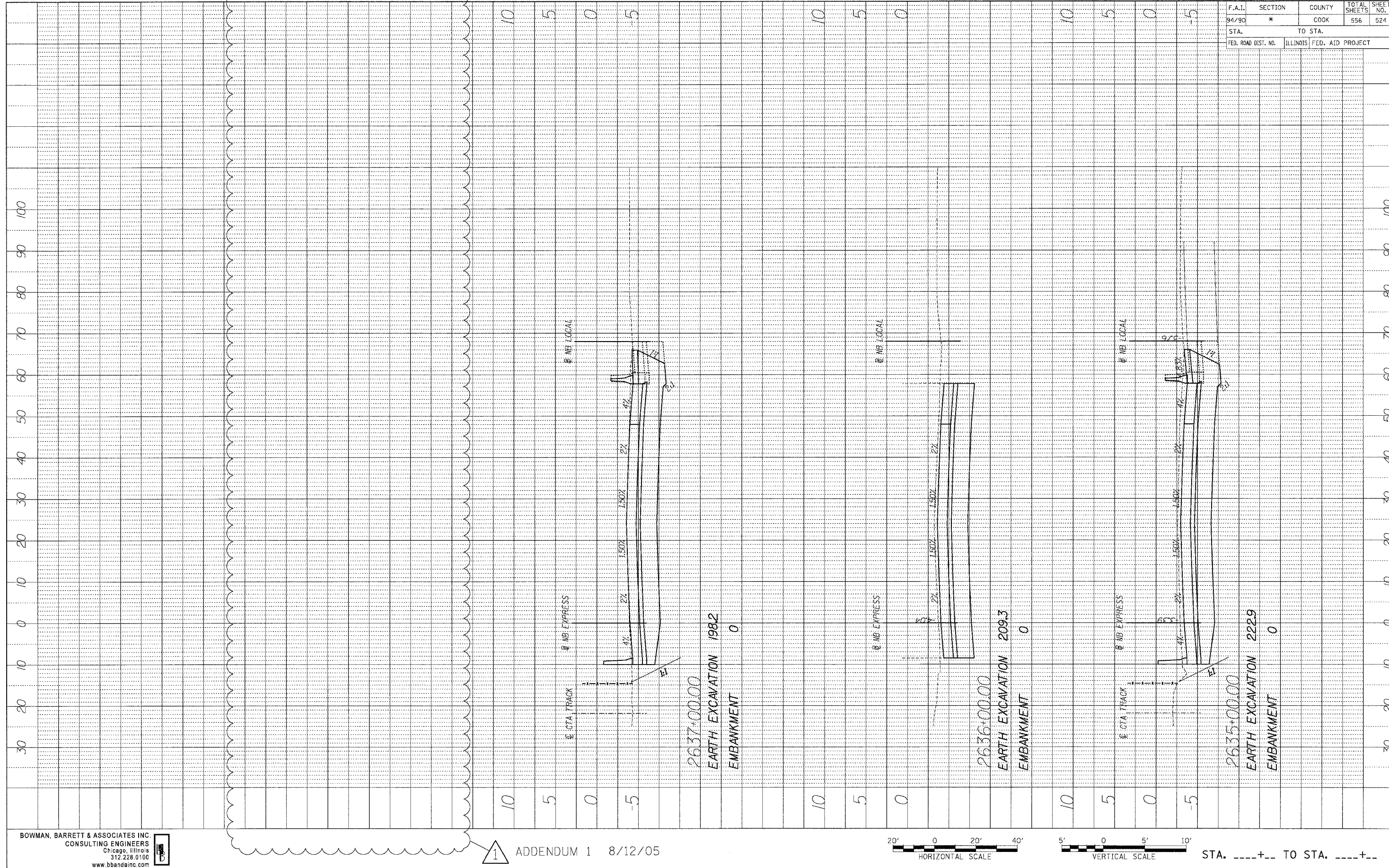
1 ADDENDUM 1 8/12/05



STA.+.. TO STA.+..

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90	*	COOK	556	524
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

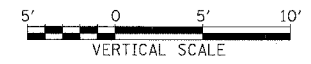
PROFILE SURVEYED	DATE
BY	
NOTE BOOK	
CHAINS CHECKED	
BLM. NOTED	
NO. _____	
STRUCTURE NOTATIONS CHECKED	



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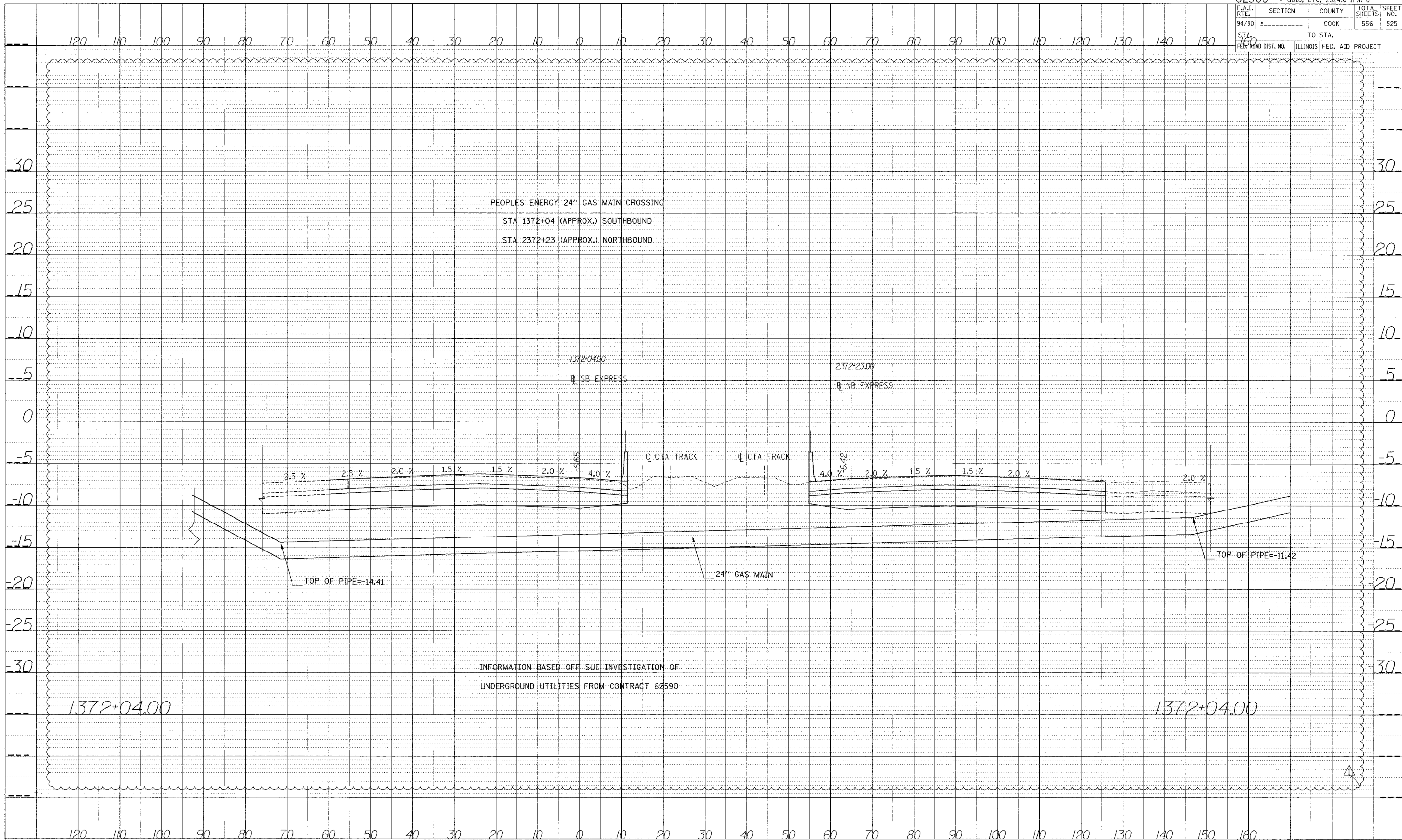


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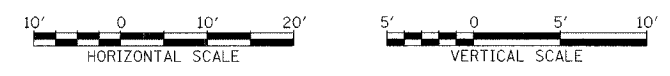
62300 • (1818, ETC, 2324.6-1PR-8	
F.A.I. SECTION	COUNTY
94/90	COOK
TOTAL SHEETS	SHEET NO.
556	525
STA. 150	TO STA.
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT



PROFILE SURVEYED BY: DATE: _____
 CHECKED BY: _____
 NOTE BOOK NO.: _____
 STRUCTURE NO.: _____

INFORMATION BASED OFF SUE INVESTIGATION OF
 UNDERGROUND UTILITIES FROM CONTRACT 62590

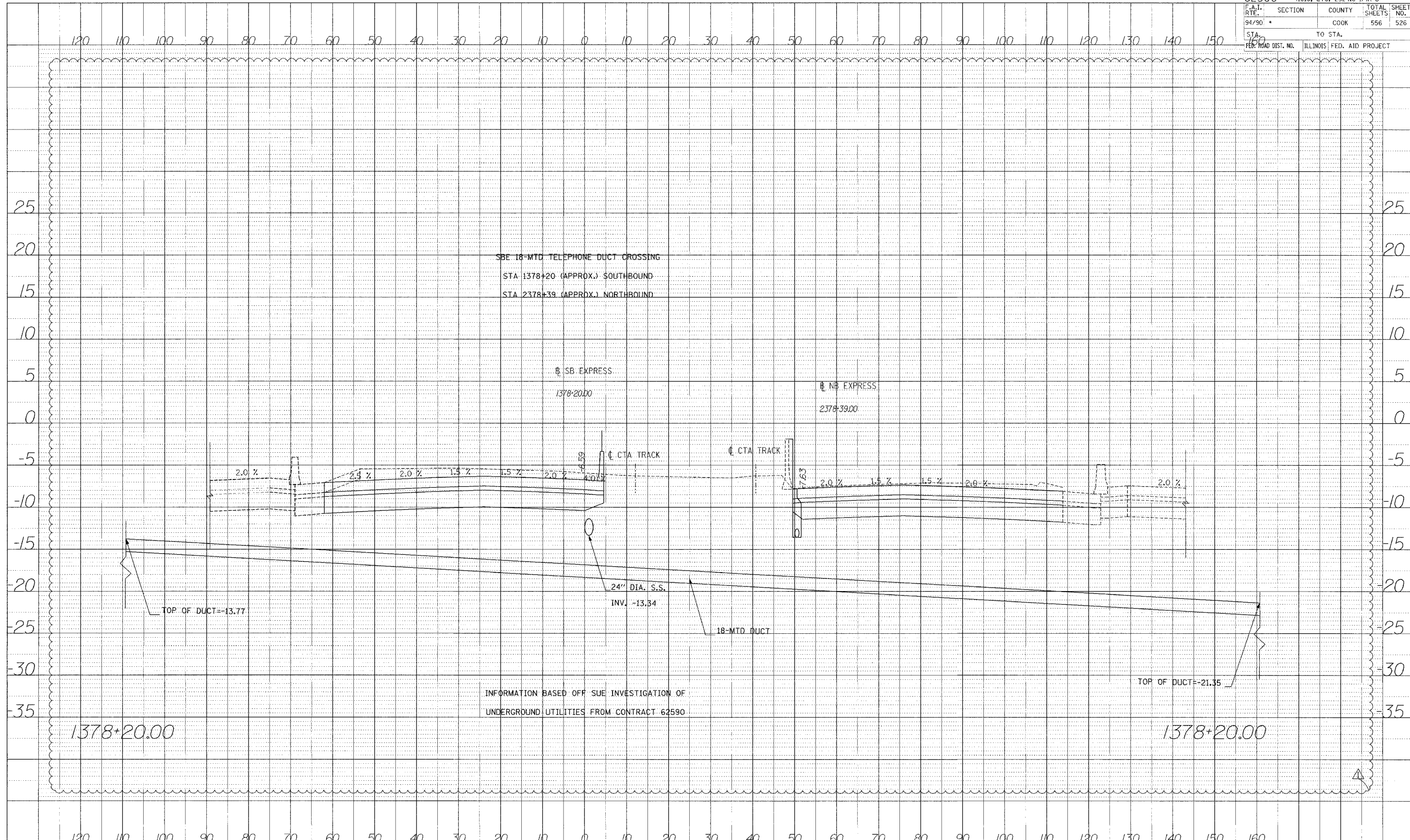
Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKORP.COM



STA.

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DATE: _____
 BY: _____
 CHECKED: _____
 DATE: _____
 NO. _____
 PROJECT NO. _____

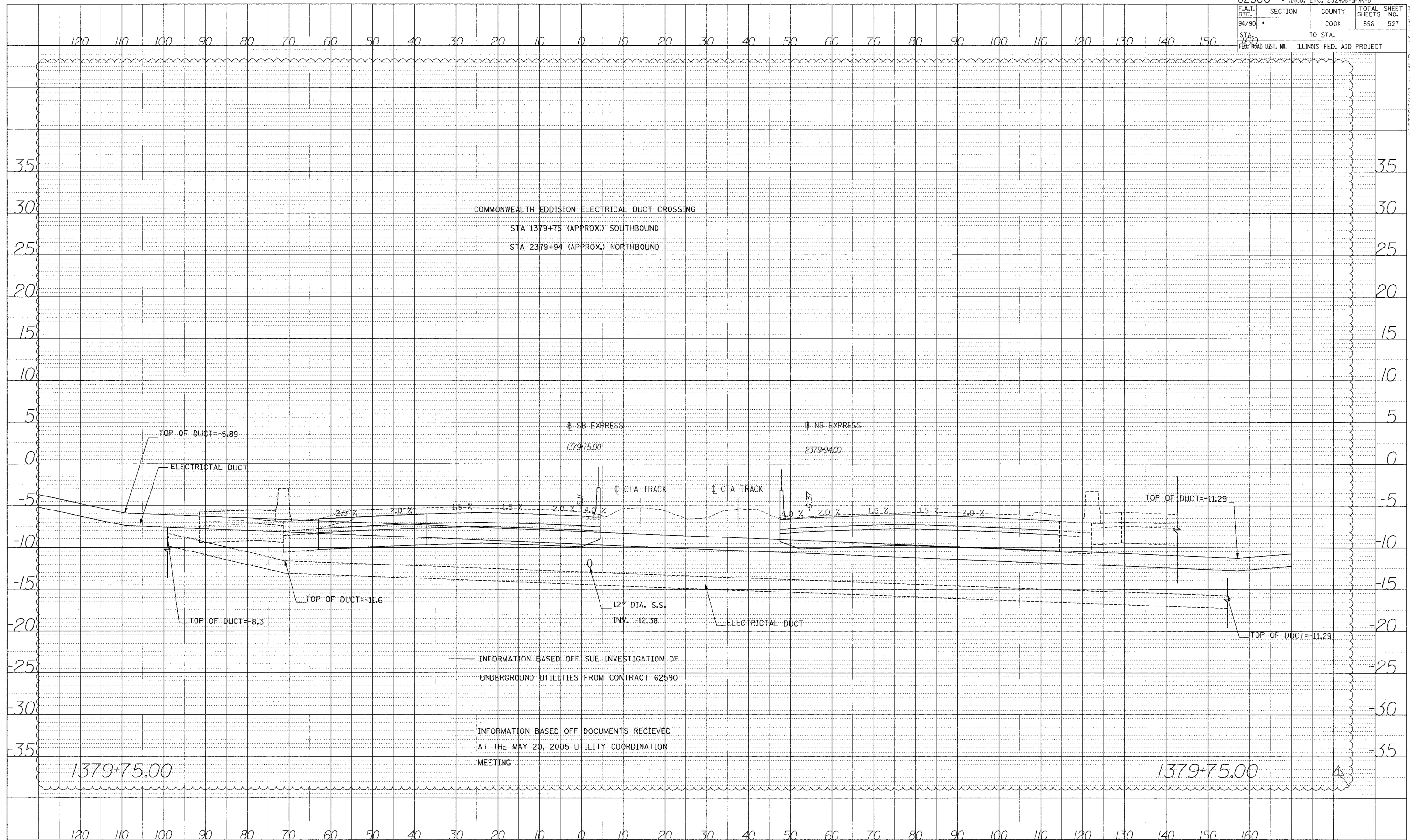


INFORMATION BASED OFF SUE INVESTIGATION OF
 UNDERGROUND UTILITIES FROM CONTRACT 62590



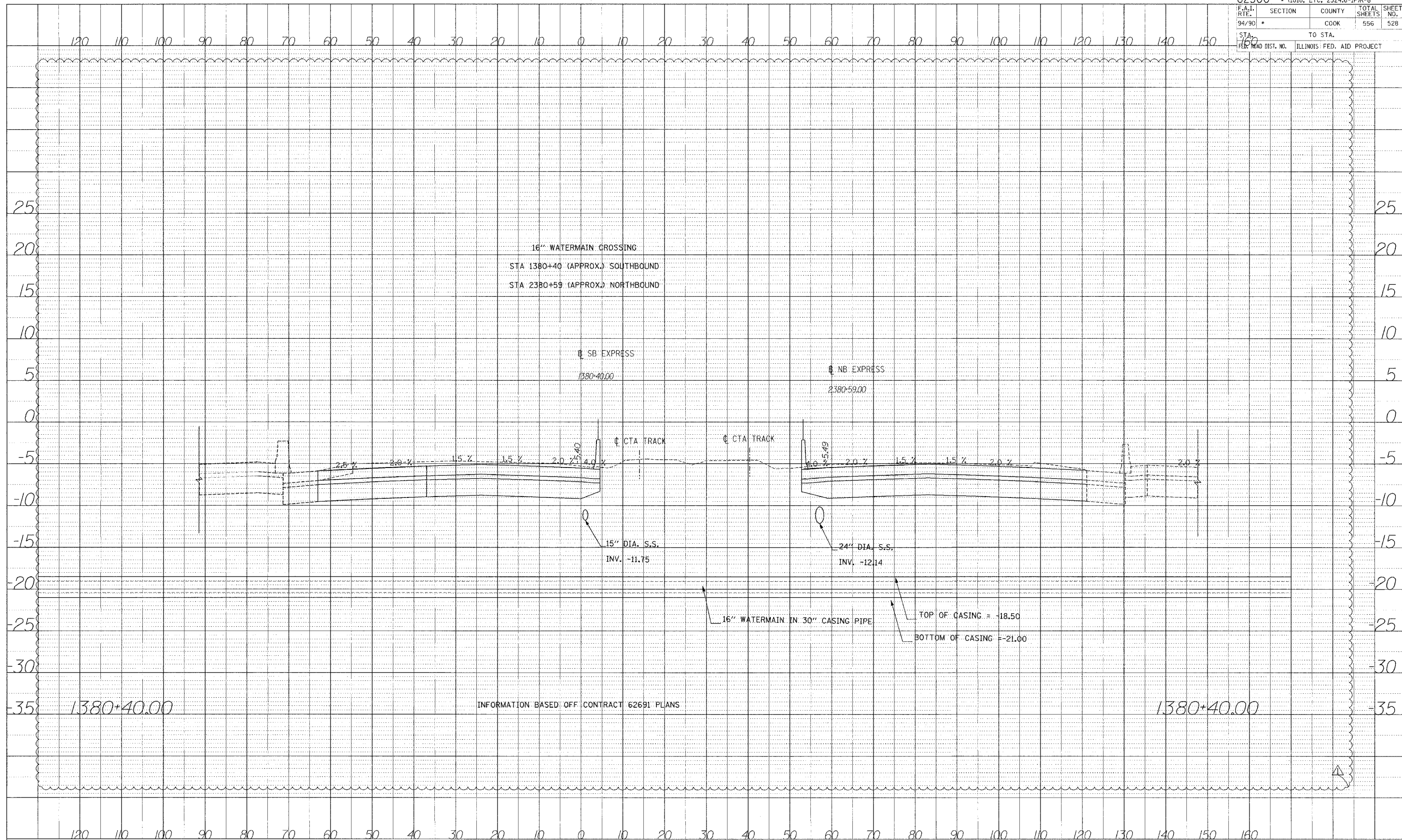
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BY	
PROF. ILL. SURVEYED	
DESIGNED	
CHECKED	
IN CHARGE	
NO.	



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62300 * (1818, ETC, 2324.6-1PR-8			
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS
94/90		COOK	556
STA. 160	TO STA.		SHEET NO. 528
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



PROFILE SURVEYED BY: []
 GRADES CHECKED BY: []
 NOTE BOOK NO.: []
 DATE: []

1380+40.00

INFORMATION BASED OFF CONTRACT 62691 PLANS

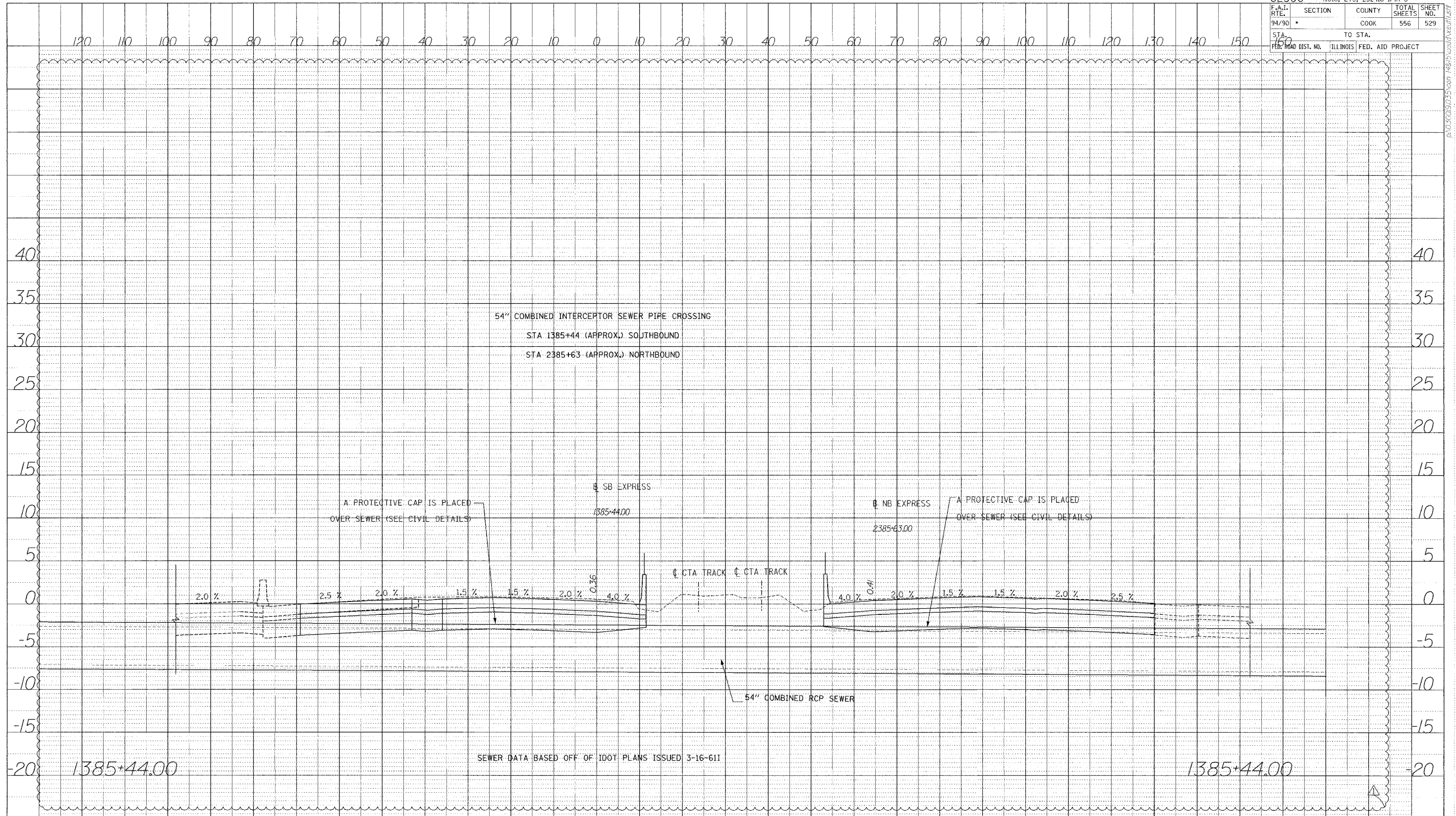
1380+40.00

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 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
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 FAX: (312) 251-3015
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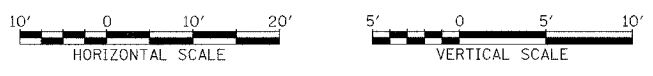
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62300 • (1818, ETC, 2324.6-1P)R-8				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90		COOK	556	529
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



DATE	
BY	
DESIGNED	
CHECKED	
IN CHARGE	
PROJECT NO.	
DATE	
BY	
DESIGNED	
CHECKED	
IN CHARGE	
PROJECT NO.	

Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM

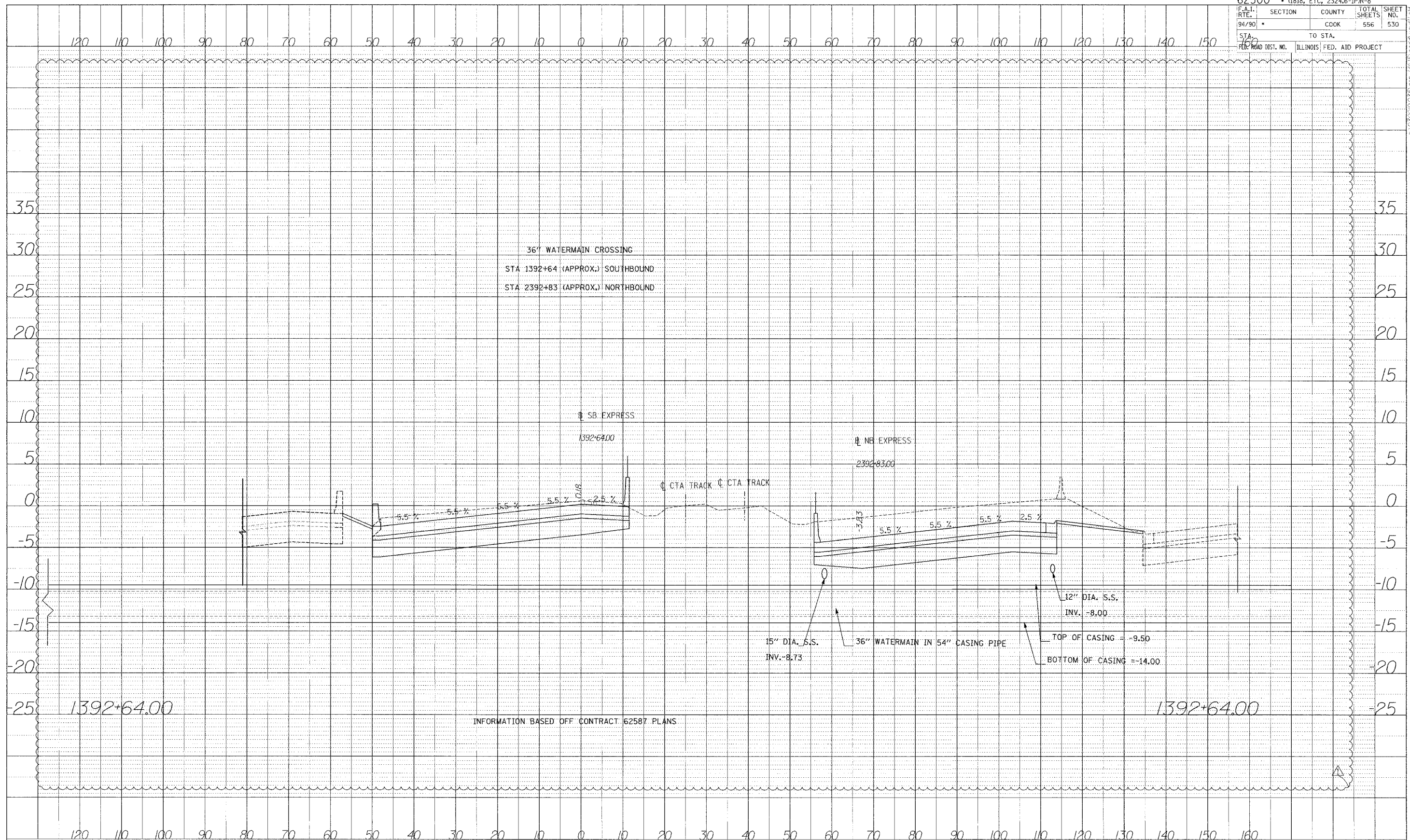


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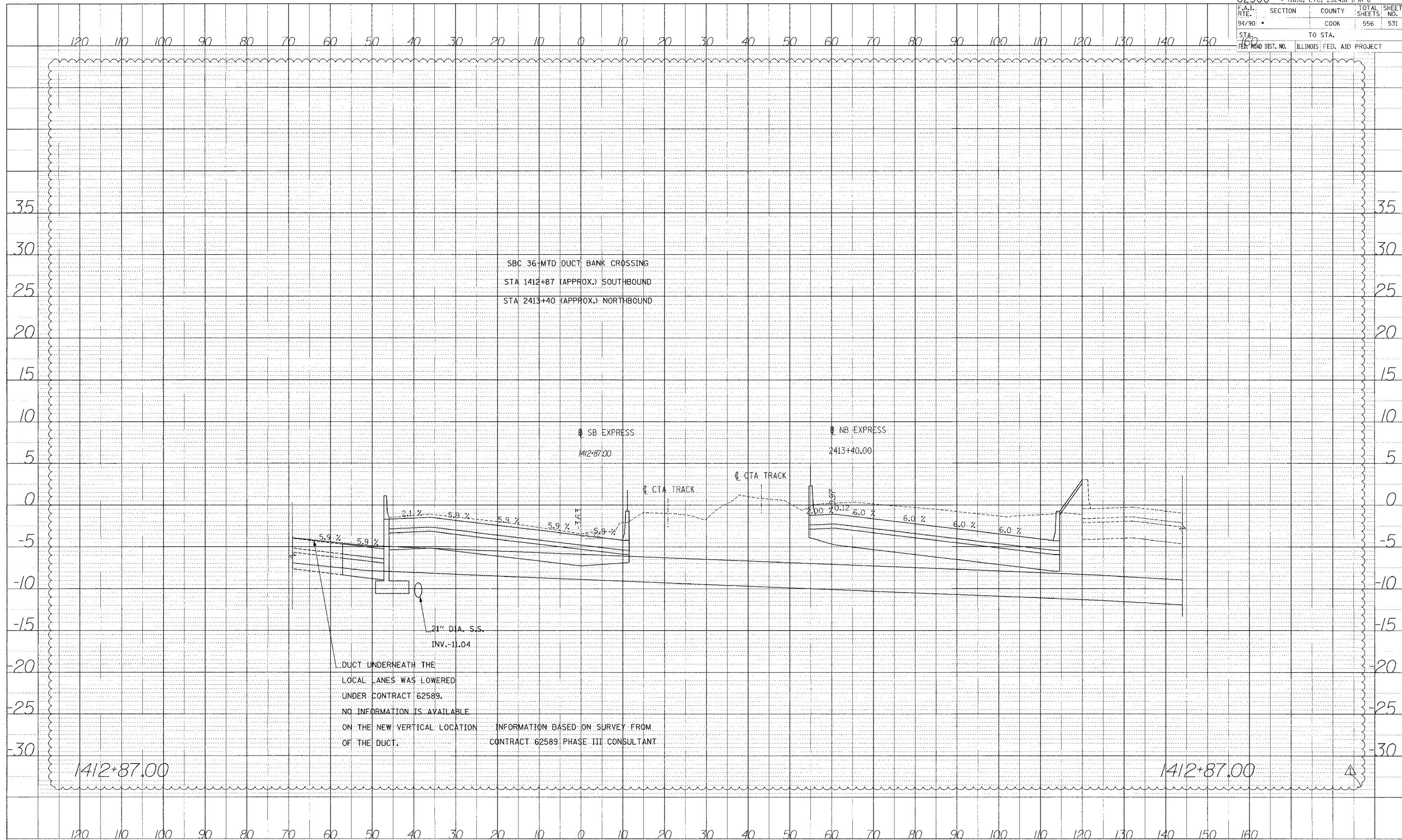
PA:03/09/09:03:5 van 14845 van01.xat/jl/1578

62300 • (1818, ETC, 2324.6-1)IR-8	
F.A.I. SECTION	COUNTY TOTAL SHEETS
94/90 • COOK	556 530
STA. TO STA.	
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

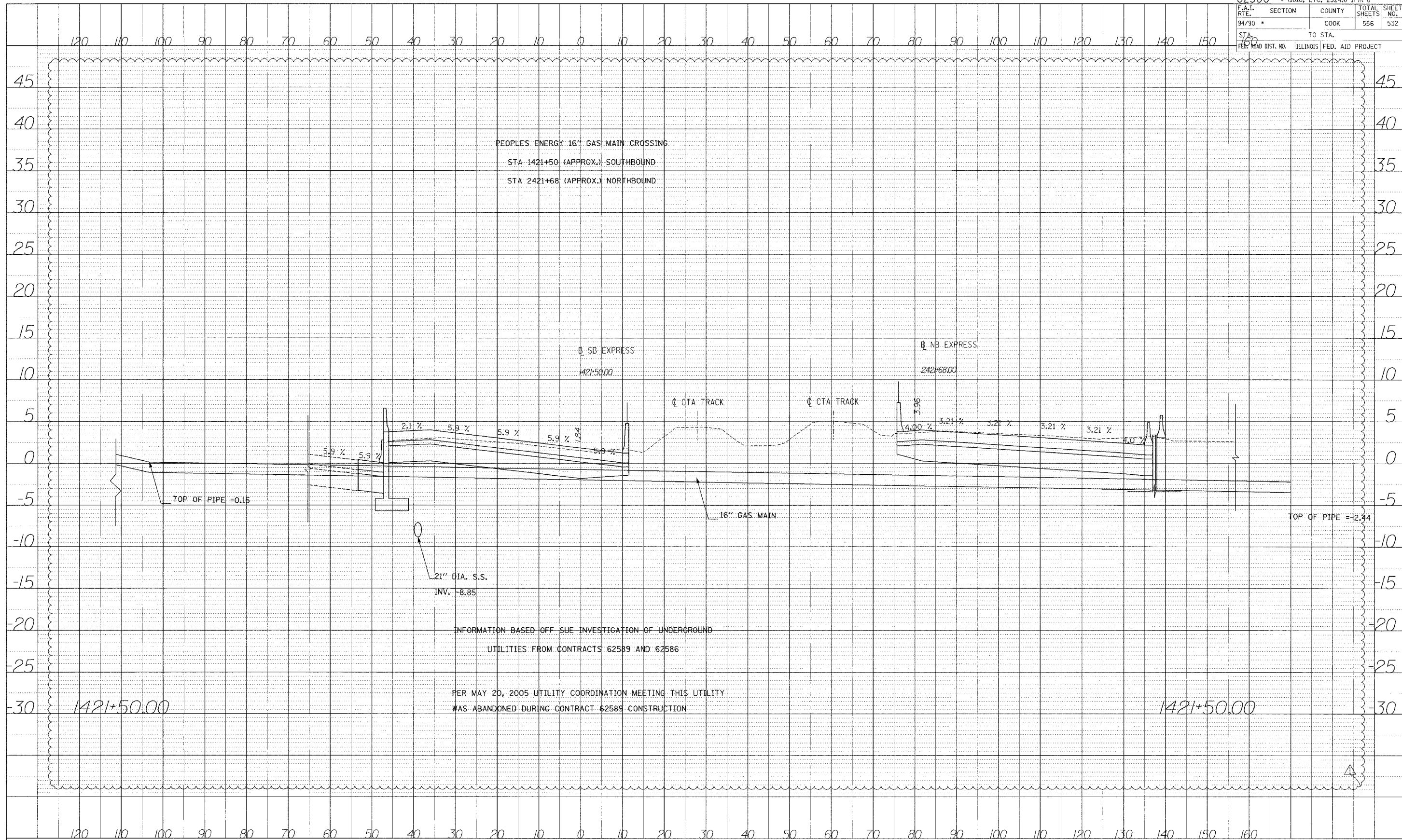
DATE _____ BY _____
 PROFILE SURVEYED _____
 DRAWINGS CHECKED _____
 NOTE BOOK _____
 PLANTING AND STATUS OTHER _____



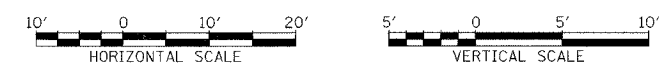
DATE: _____ BY: _____
 CHECKED: _____
 SURVEYED: _____
 PROFILE NO. _____
 NOTE: CHECK FOR CHANGES TO THE ORIGINAL PLAN AND PROFILE.
 NO. _____



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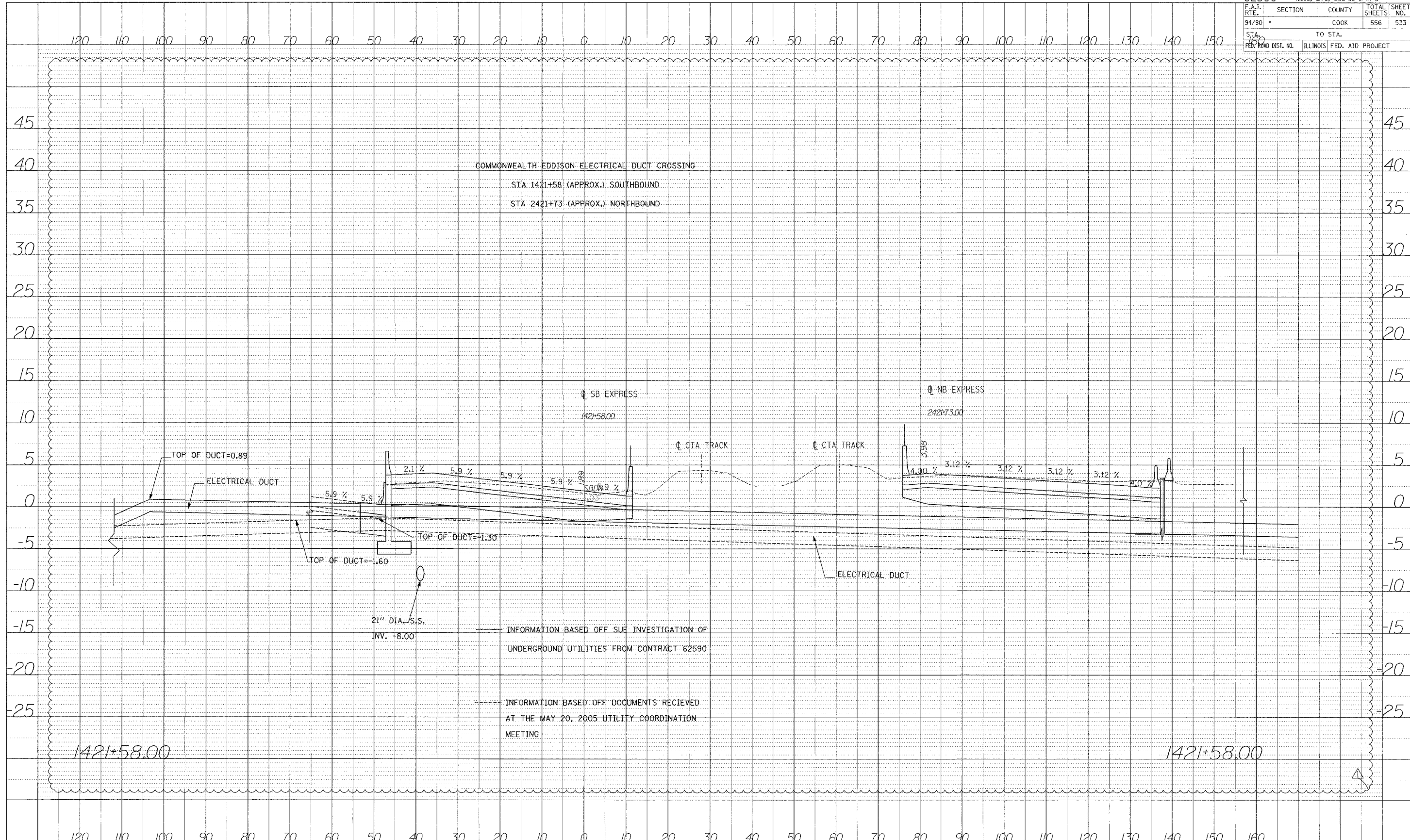


PROJECT NO. DATE
 SURVEYED BY
 GRADES CHECKED BY
 NOTE BOOK NO.
 INSTRUMENT ACTION NO. C-760



STA.

62300 • 1818, ETC, 2324.6-IPR-8			
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS
94/90		COOK	556
STA. 160	TO STA.		SHEET NO. 533
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	

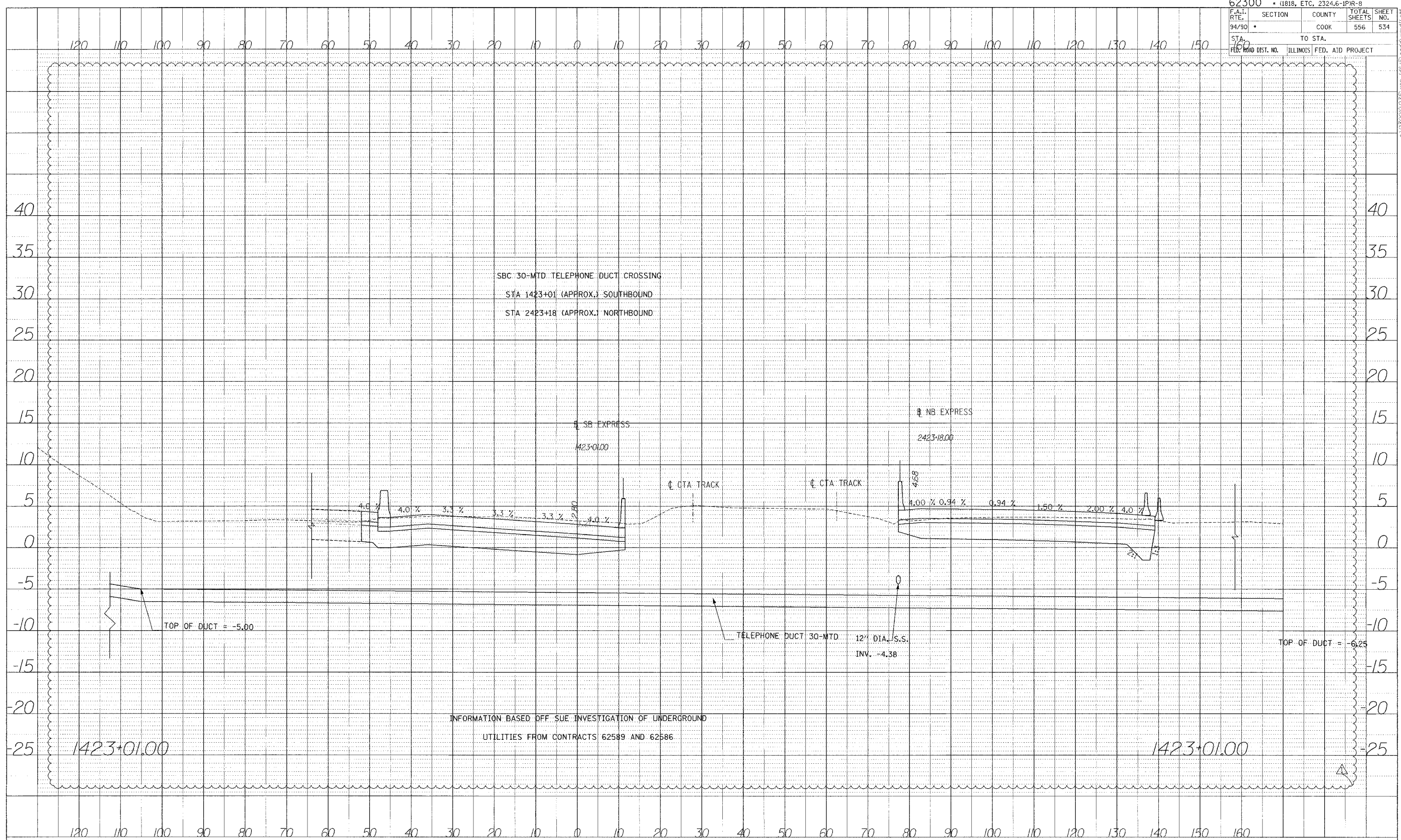


PROFILE SUBMITTED BY: []
 DATE: []
 NOTICED: []
 CHECKED: []
 NO. []



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 CHICAGO, IL 60606
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62300 • (1818, ETC, 2324.6-IPR-8				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90		COOK	556	534
STA. 160		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



DATE	BY
PROFILE	BY
NOTED	BY
CHECKED	BY
DATE	BY

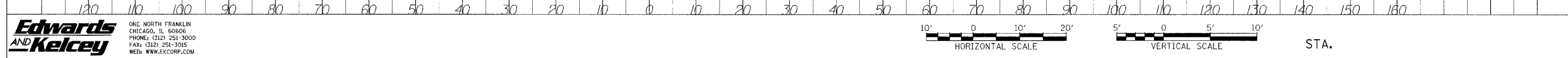
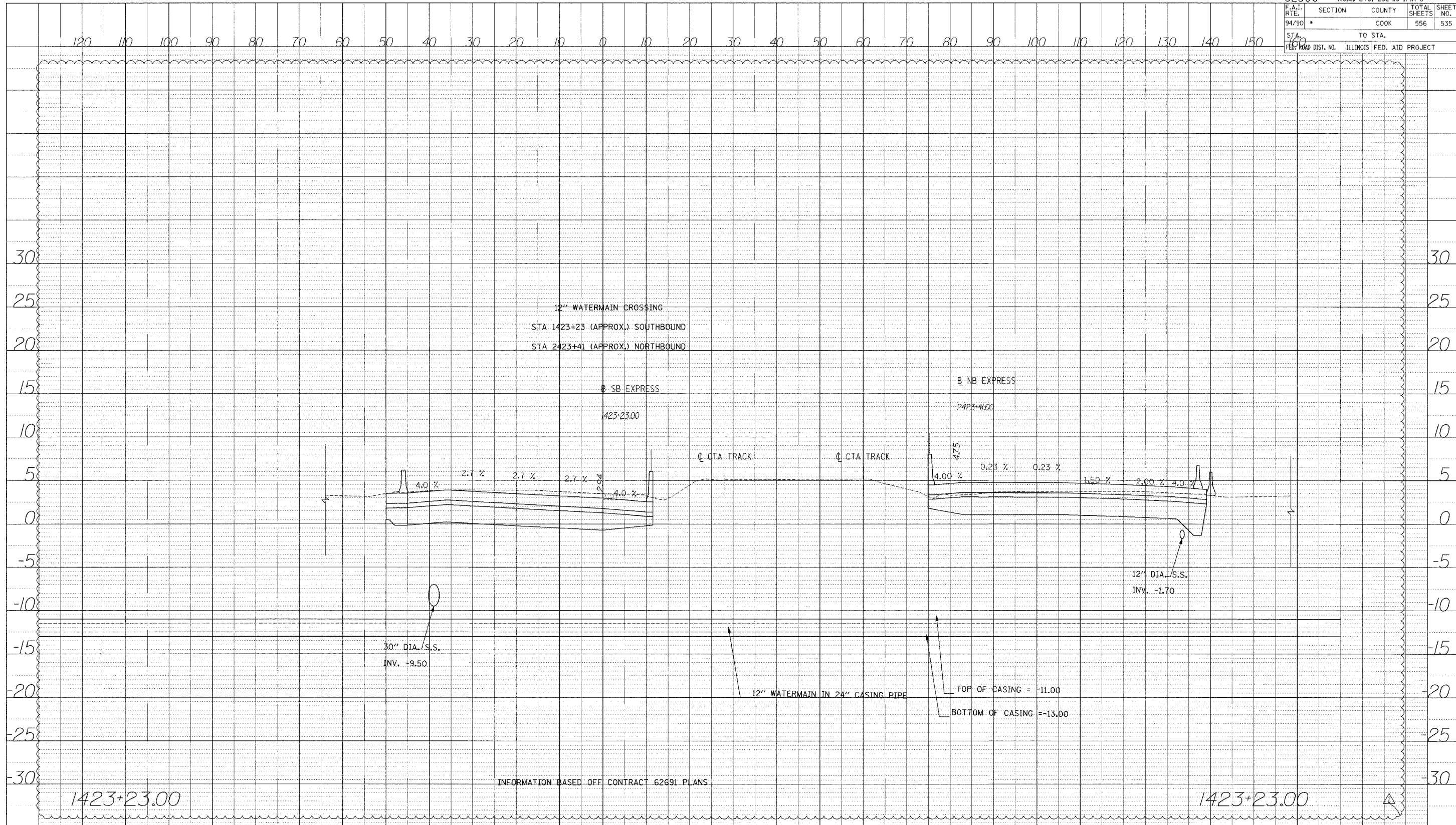
Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



STA.

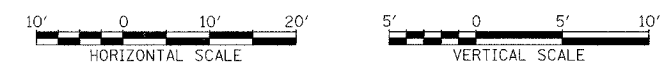
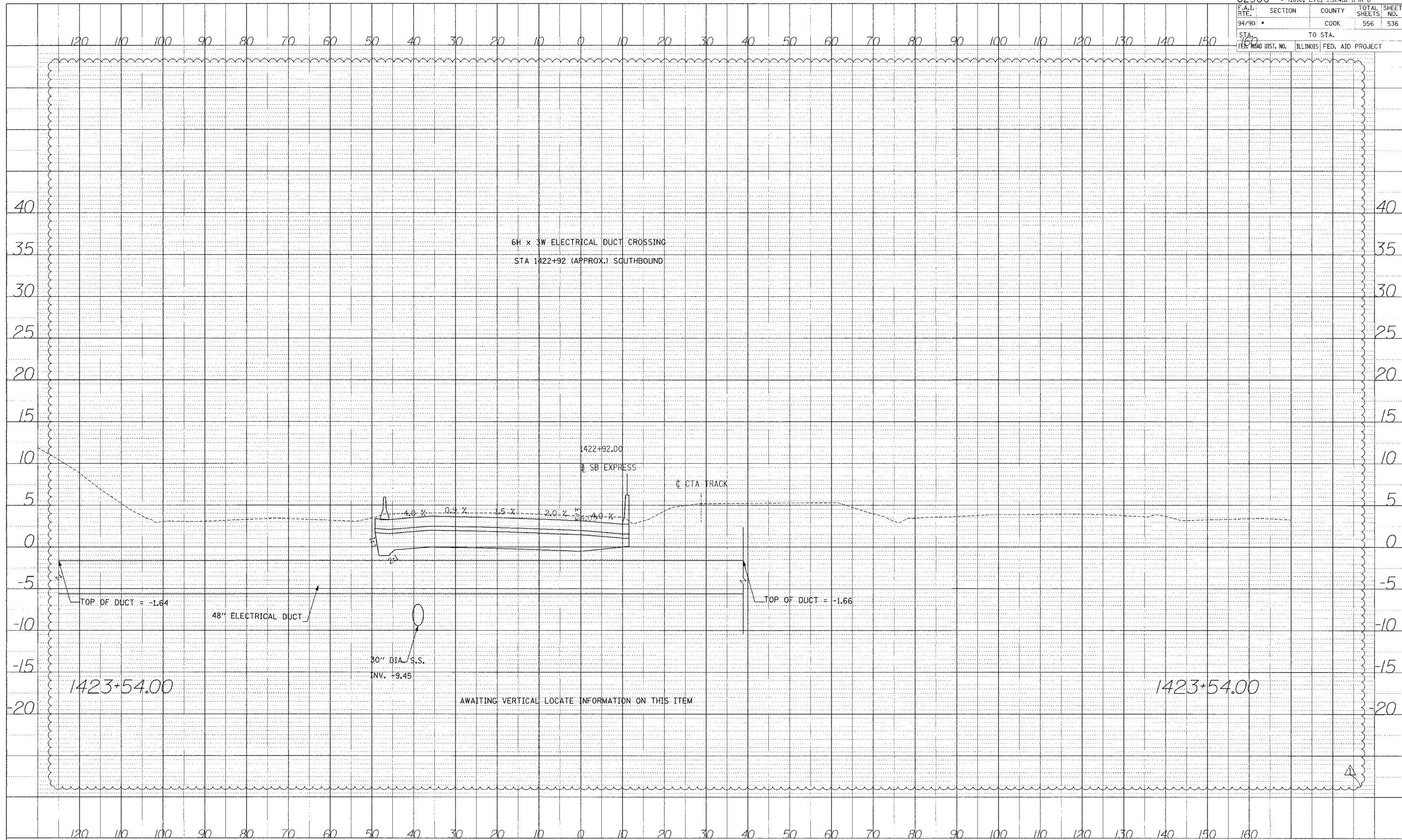
62300 • (1818, ETC, 2324.6-IPR-8)				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90		COOK	556	535
STA.	TO STA.			
160				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

DATE _____
 BY _____
 PROJECT _____
 DRAWING NO. _____
 REVISION NO. _____
 REVISION DATE _____
 REVISION DESCRIPTION _____
 APPROVED _____
 GRADES CHECKED _____
 DATE CHECKED _____
 DISTRIBUTION AND/OR OTHER _____
 NO. _____



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 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM

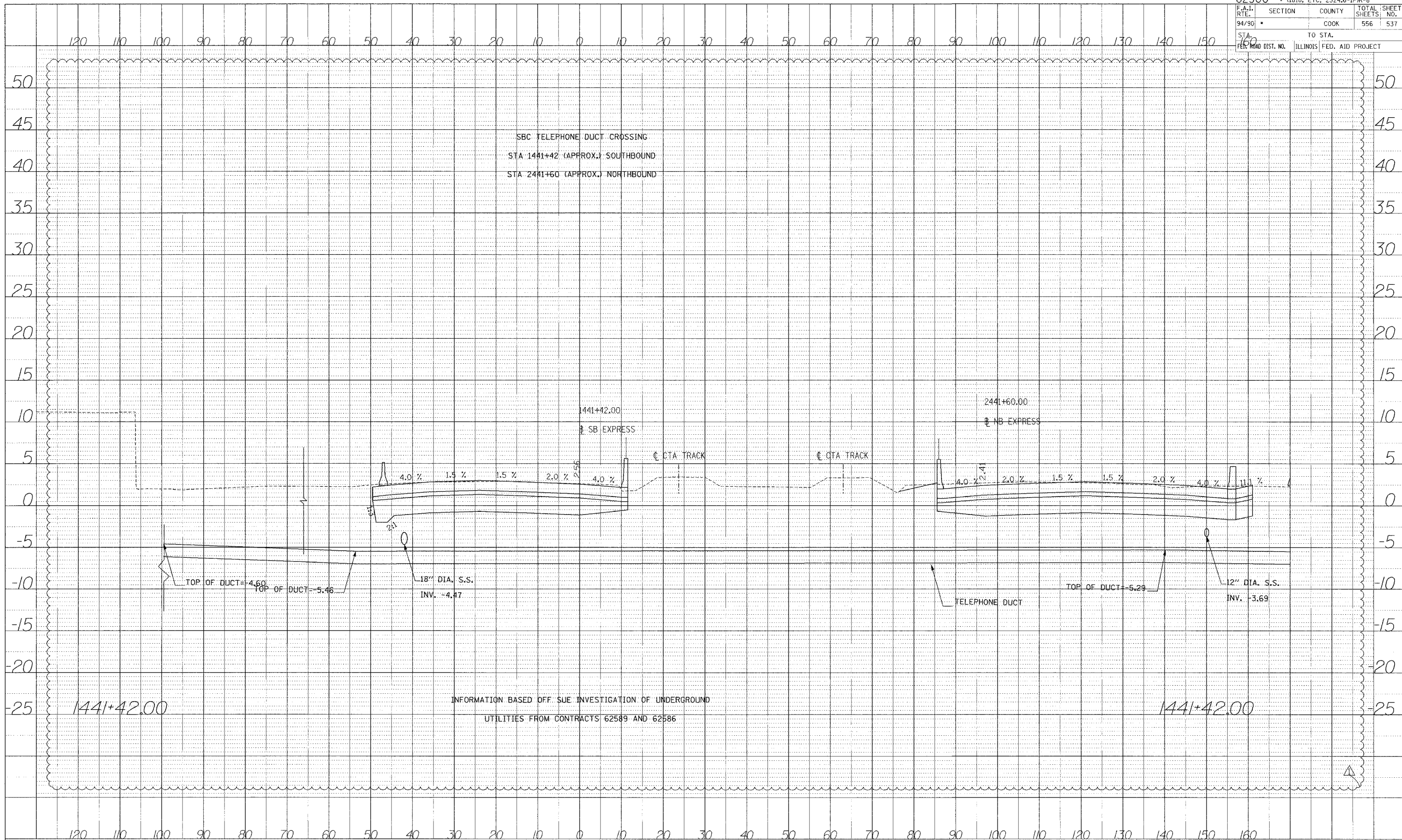
PROJECT SURVEYED BY DATE
 GRADES CHECKED
 NOTE BOOK NO.
 DATE



STA.

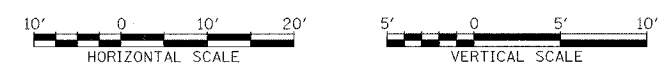
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90		COOK	556	537

STA. 160 TO STA. 160
 ILLINOIS FED. AID PROJECT



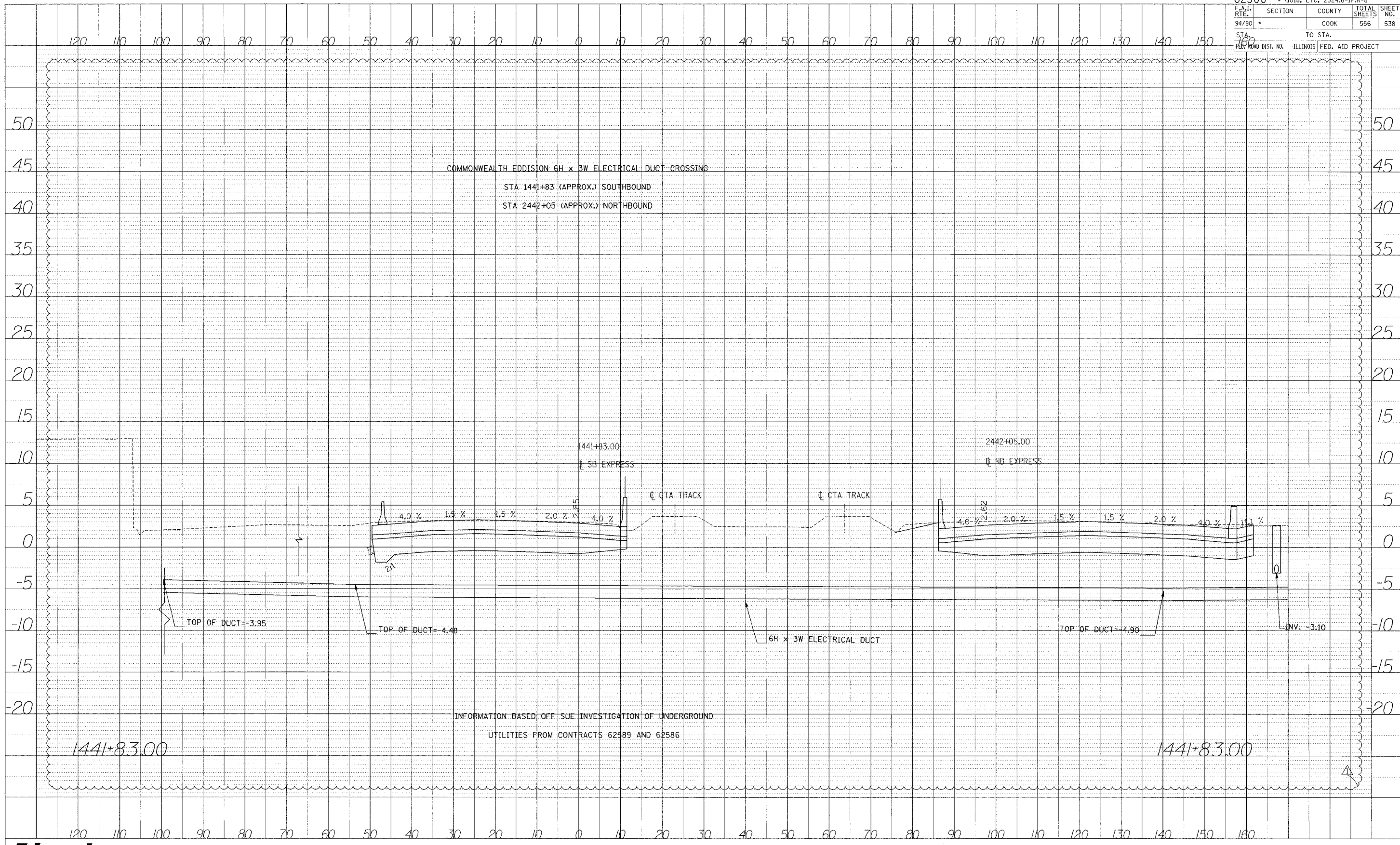
INFORMATION BASED OFF SUE INVESTIGATION OF UNDERGROUND UTILITIES FROM CONTRACTS 62589 AND 62586

PROFILE
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 CHECKED
 NO. OF SHEETS
 DATE



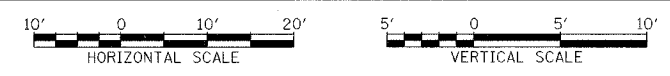
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62300 * (1818, ETC, 2324.6-IPR-8				
F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90 *		COOK	556	538
STA. 160	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



PROFILE SURVEYED BY DATE
 DRAWN BY
 CHECKED BY
 NOTE BOOK NO.
 PROJECT NO. (OPTIONAL CHFD)

Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
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62300 • (1818, ETC, 2324.6-1PR-8				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90 *		COOK	556	539
STA. 160		TO STA.		
PER. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

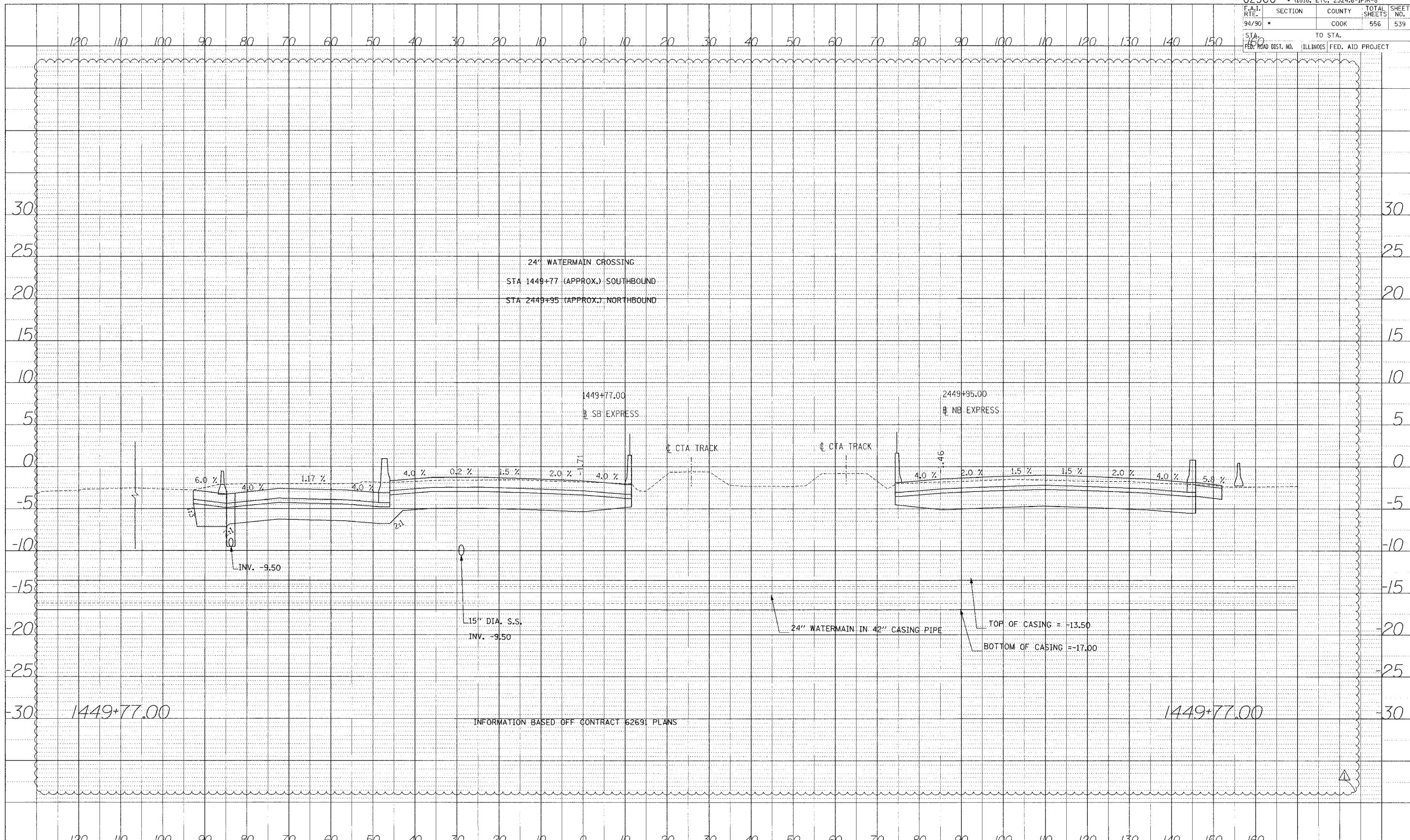
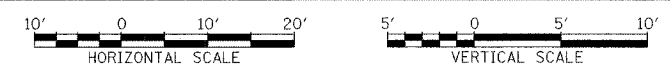


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 CHECKED BY: _____
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 STRUCTURE STATION: C/P/D

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 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM

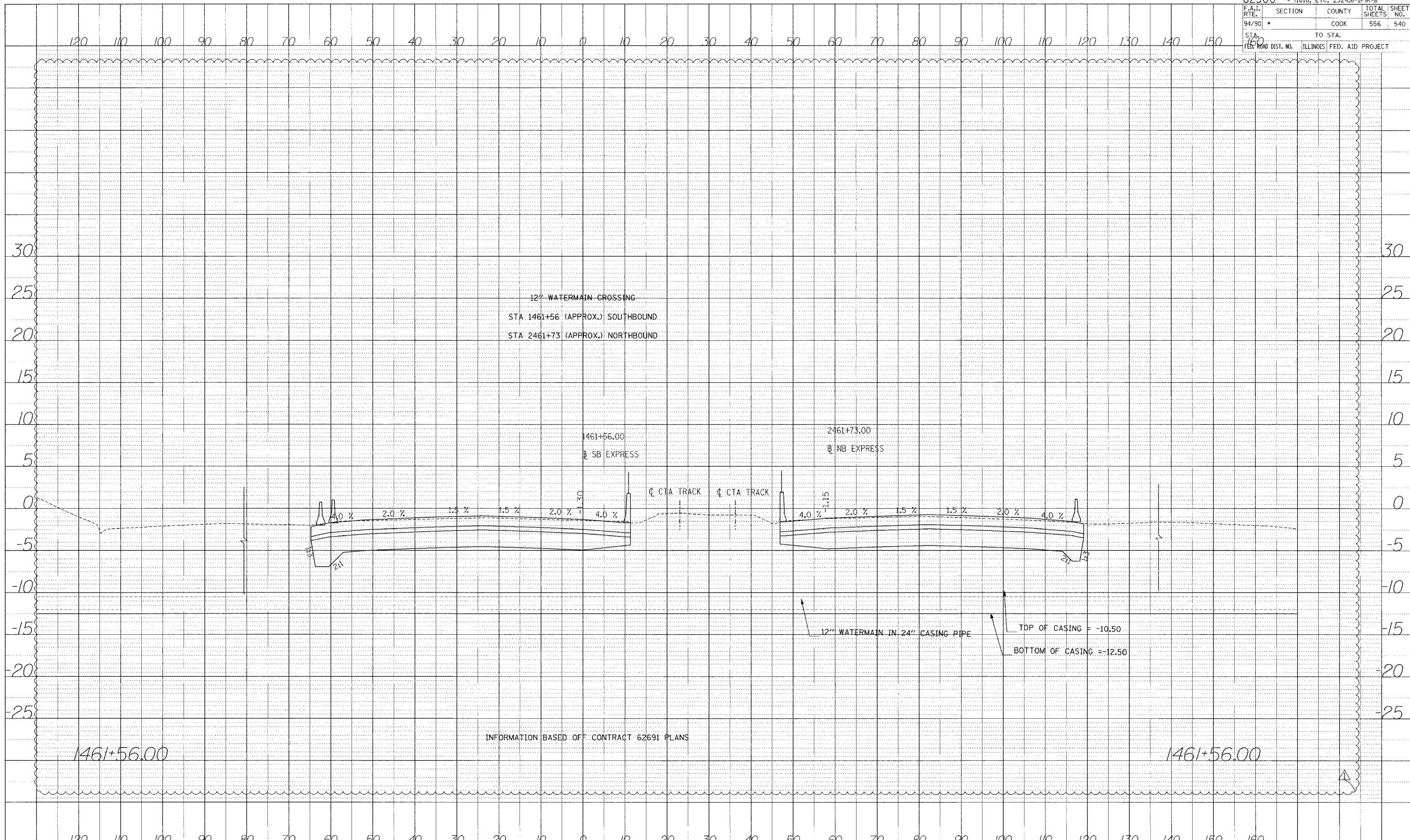


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INFORMATION BASED OFF CONTRACT 62691 PLANS

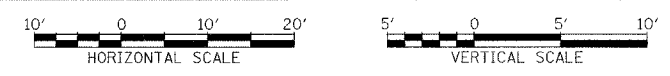
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90	*	COOK	556	540
STA. 150		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

PROFILE DRAWN BY: _____ DATE: _____
 CHECKED BY: _____
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 NO. _____
 STRUCTURE NO. (IF APPLICABLE) _____



INFORMATION BASED OFF CONTRACT 62691 PLANS

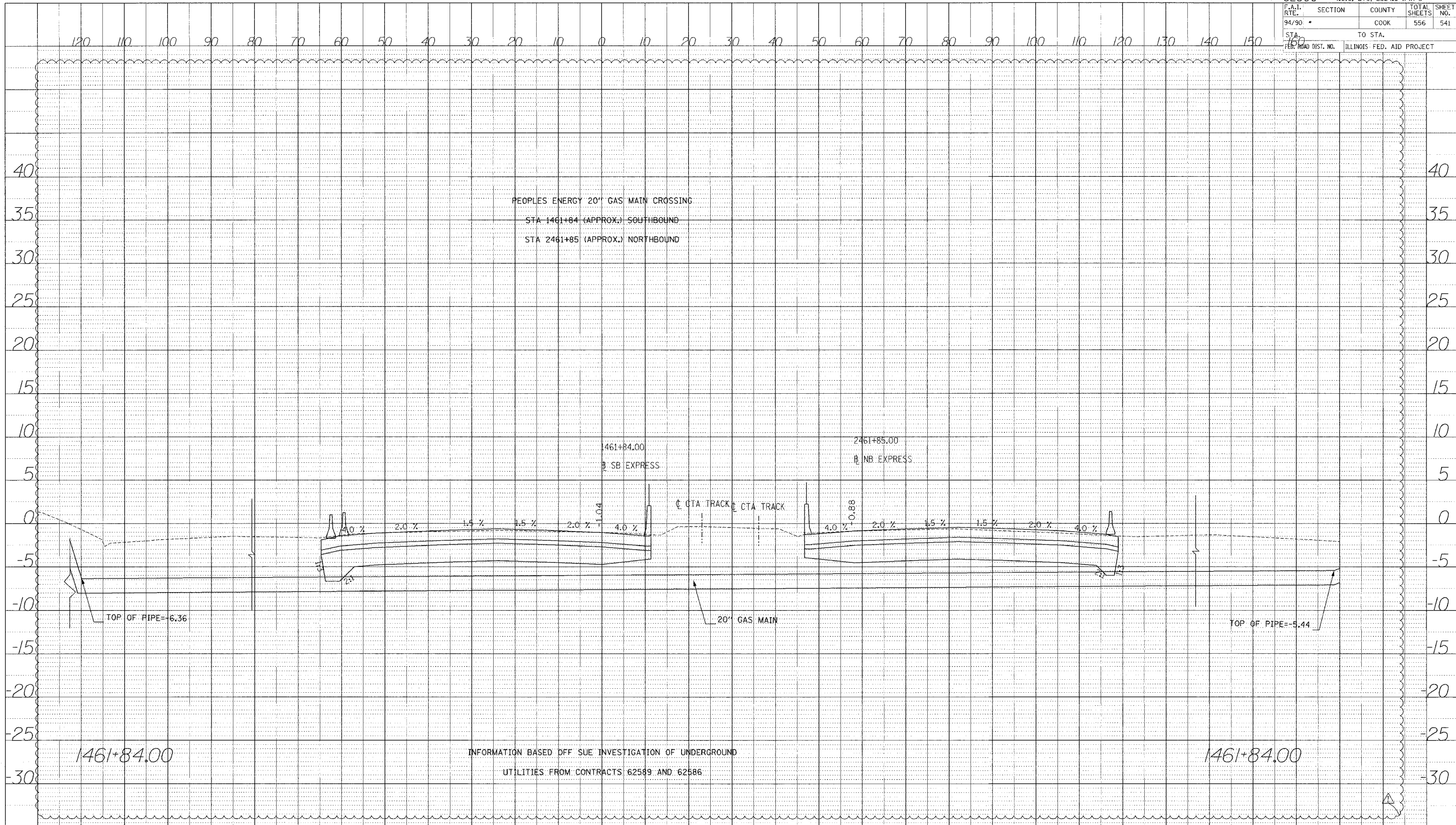
Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



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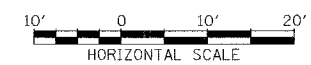
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62300 • (1818, ETC, 2324.6-IPR-8				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90		COOK	556	541
STA. 150		TO STA.		
FER. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



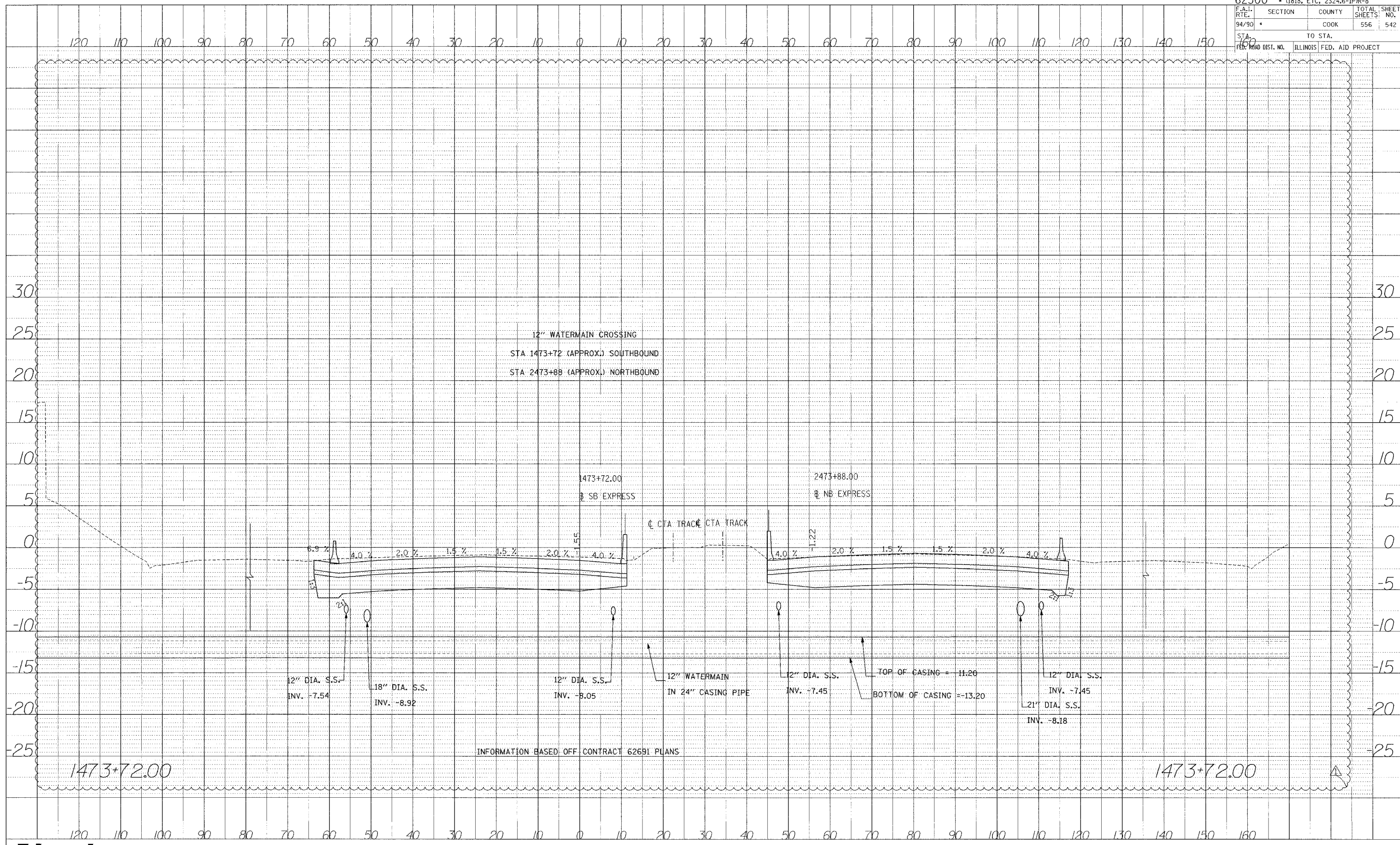
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 CHECKED BY: _____
 NOTE BOOK NO.: _____
 STRUCTURE INVENTORY CHIT: _____

Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 251-3000
 FAX: (312) 251-3015
 WEB: WWW.EKCORP.COM



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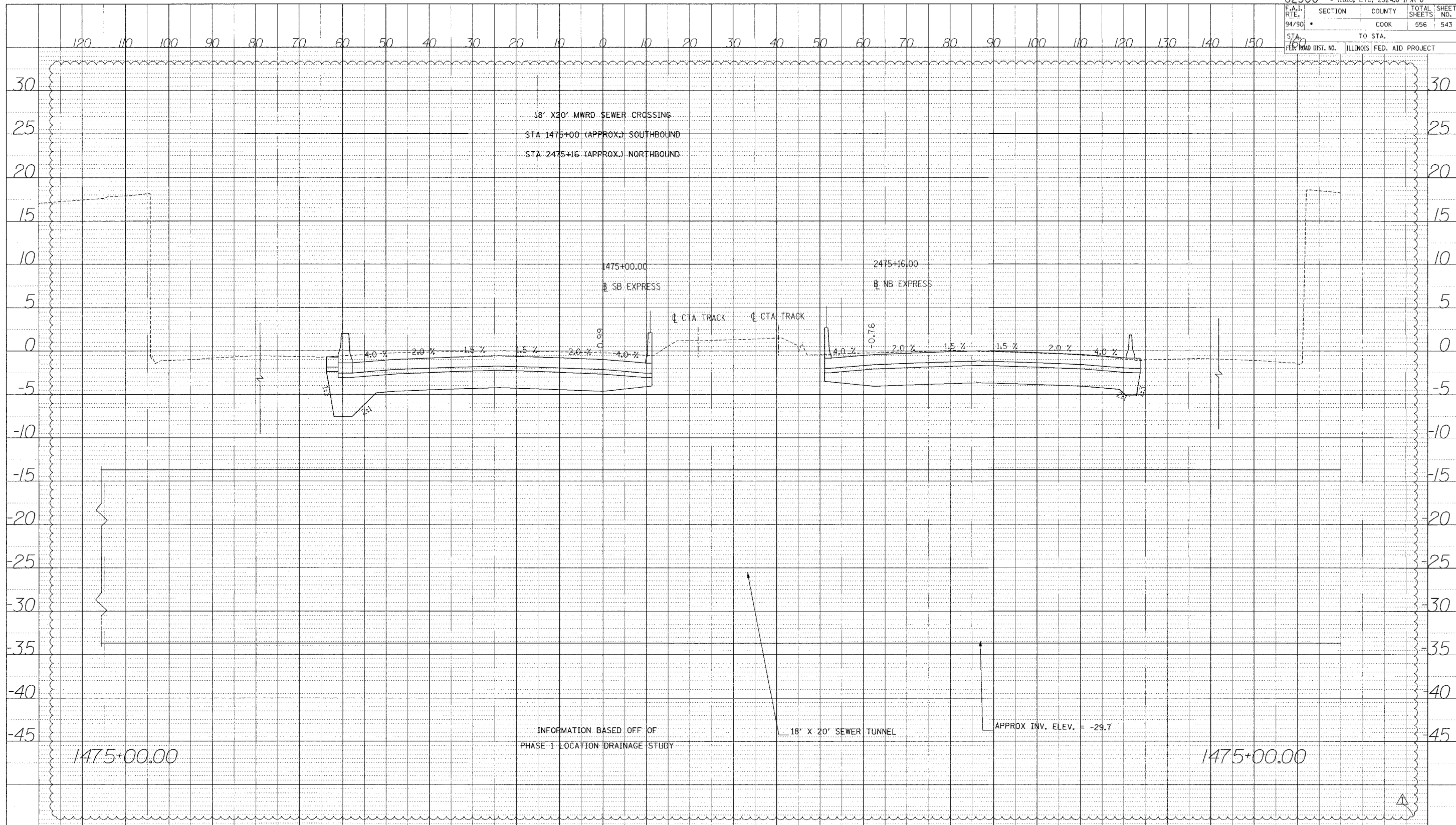
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 REVISIONS CHECKED BY DATE
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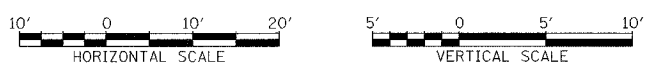
INFORMATION BASED OFF CONTRACT 62691 PLANS



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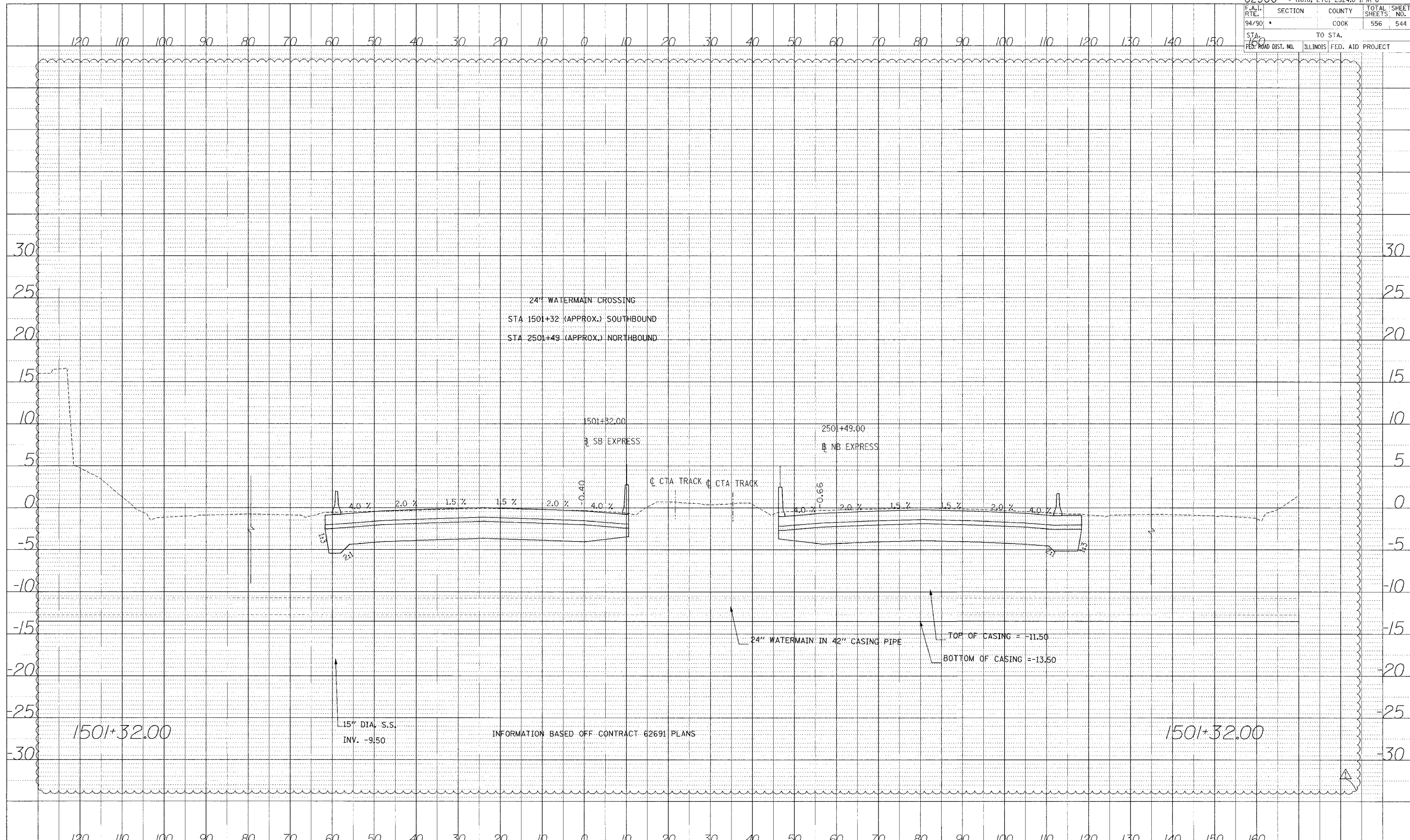


DATE _____ BY _____
 SURVEYED _____
 GRADES CHECKED _____
 NOTE BOOK NO. _____
 PLAN NUMBER AND DATE OF CHG. _____



STA.

PROFILE SHEET
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 BY
 CHECKED
 DATE
 NO.

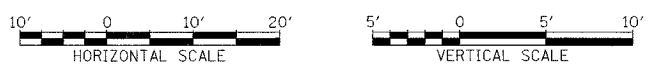


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15" DIA. S.S.
 INV. -9.50

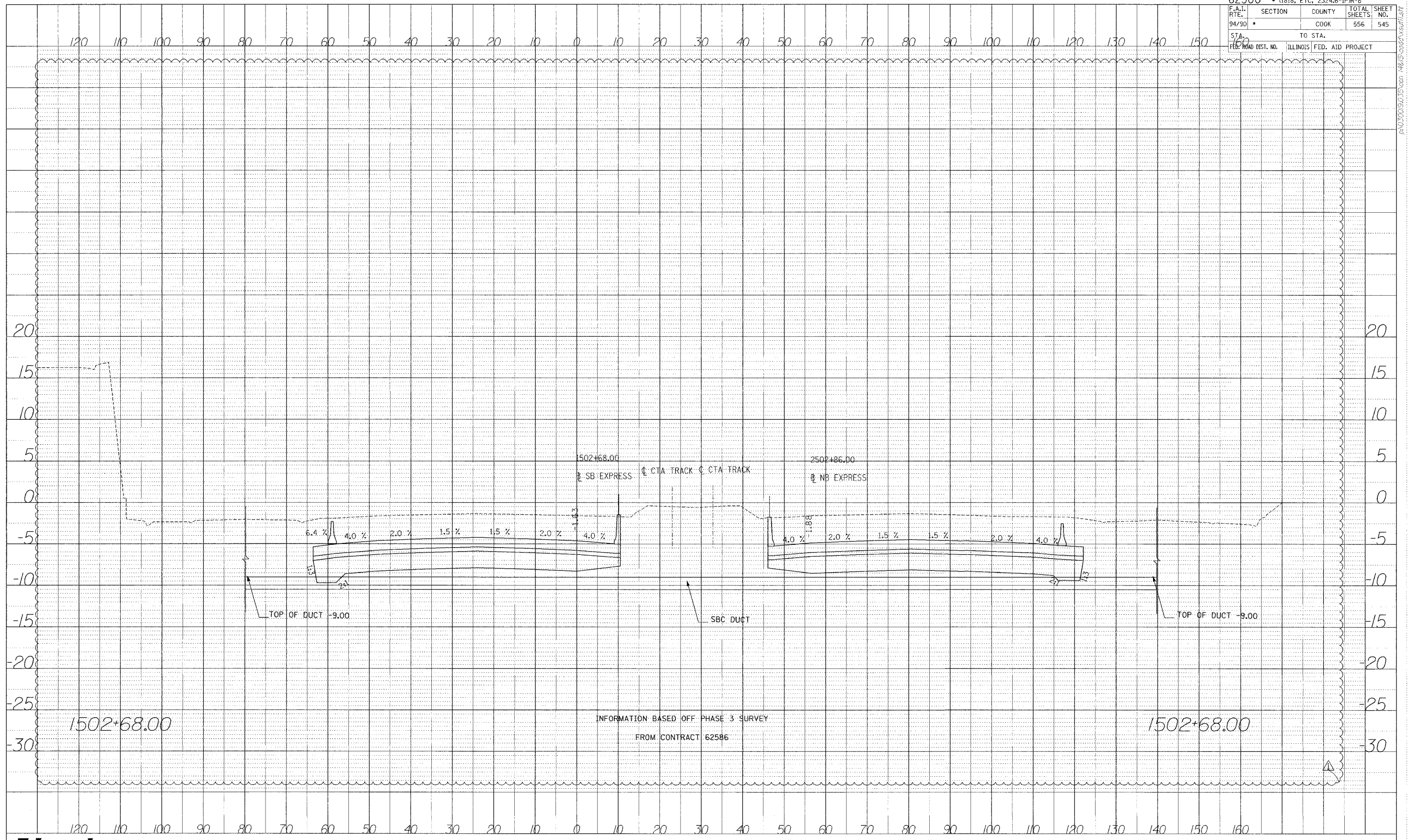
INFORMATION BASED OFF CONTRACT 62691 PLANS

1501+32.00



STA.

62300 • (1818, ETC. 2324.6-1)PR-8			
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
94/90		COOK	556 545
STA.	TO STA.		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



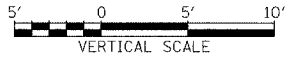
PROJECT	DATE
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BY	
CHECKED	
DATE	
NO.	

1502+68.00

1502+68.00

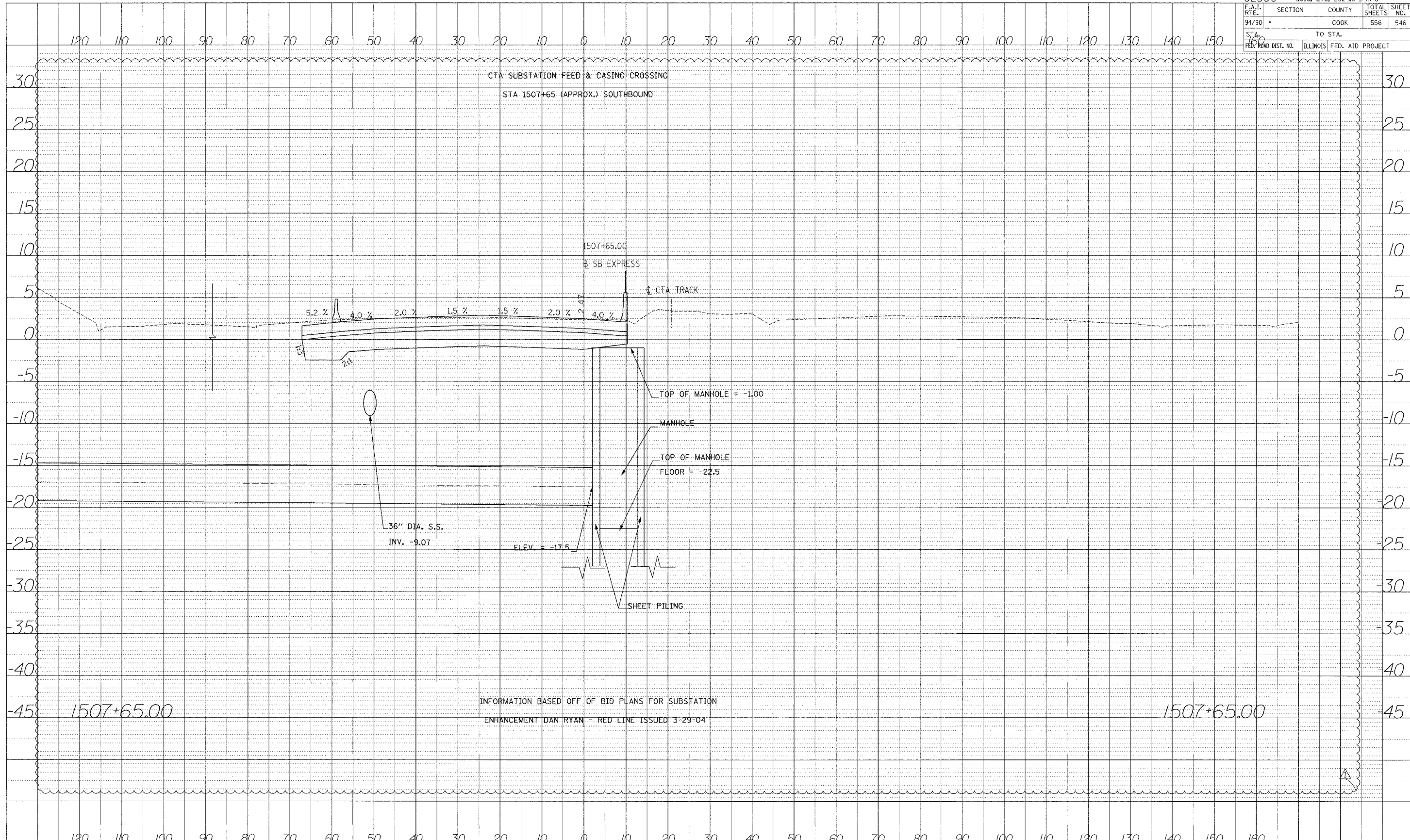
INFORMATION BASED OFF PHASE 3 SURVEY
FROM CONTRACT 62586

Edwards AND Kelcey
ONE NORTH FRANKLIN
CHICAGO, IL 60606
PHONE: (312) 251-3000
FAX: (312) 251-3015
WEB: WWW.EKCORP.COM



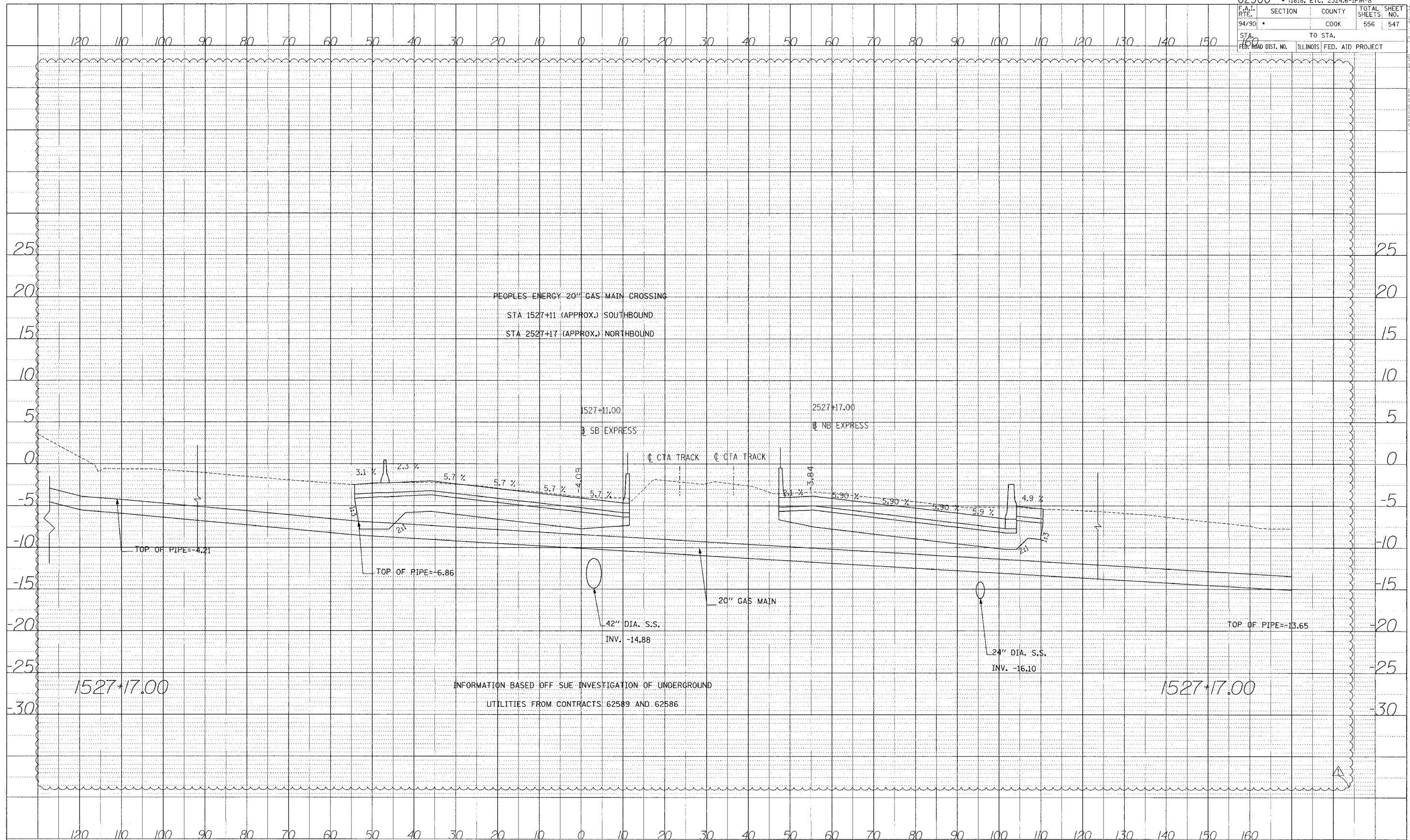
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 CHECKED BY: _____
 DRAWN BY: _____
 NO. _____

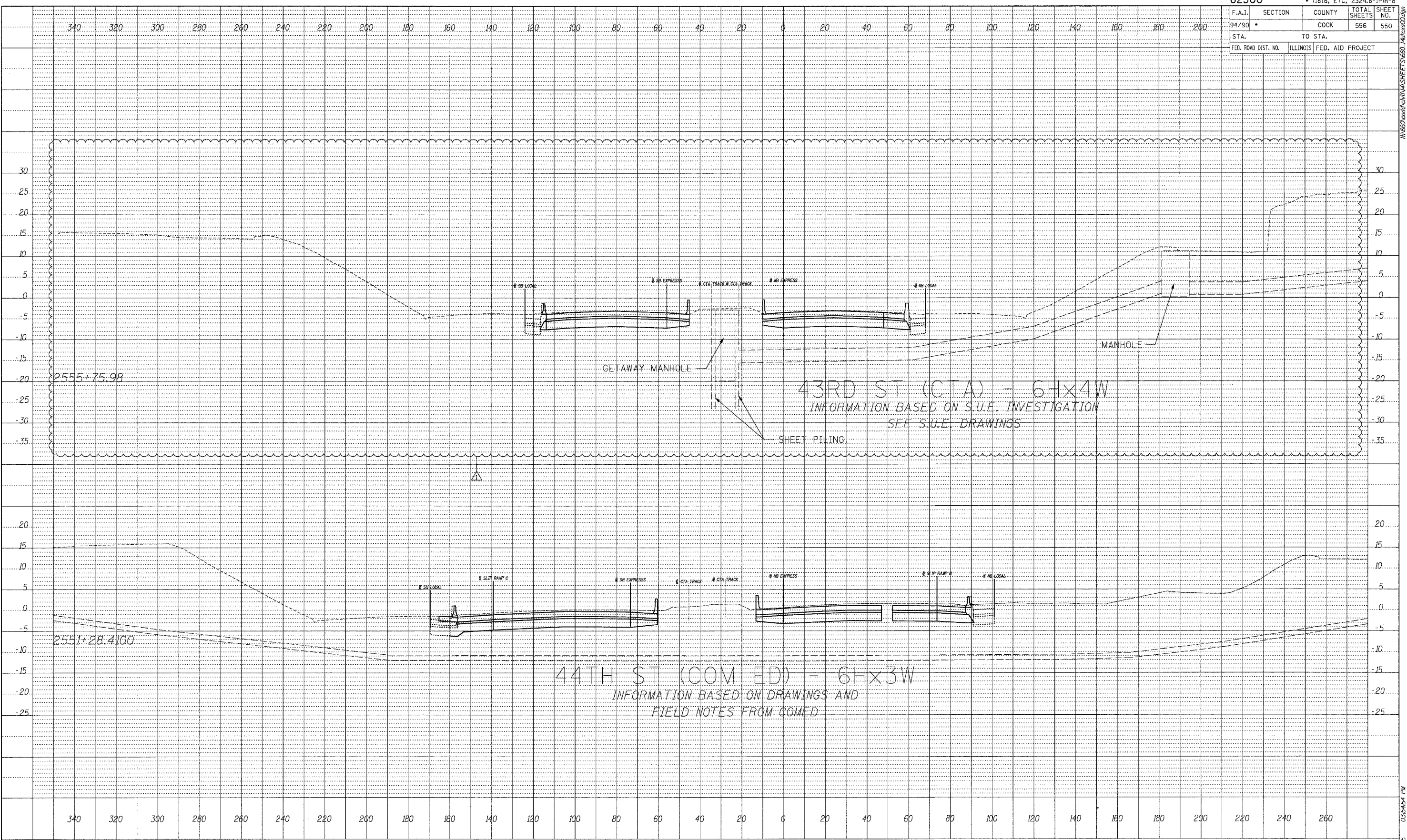
PROFILE DRAWING
 DATE: _____
 BY: _____
 CHECKED: _____
 DATE: _____
 NO. _____



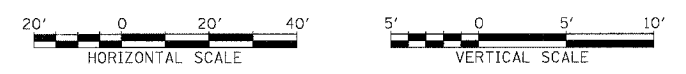
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F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90		COOK	556	550
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

DATE	
BY	
PROFILE SURVEYED	
AS NOTED	
RECORDED	
NOTE BOOK	
NO.	
STRUCTURE NOTATION CHHD	



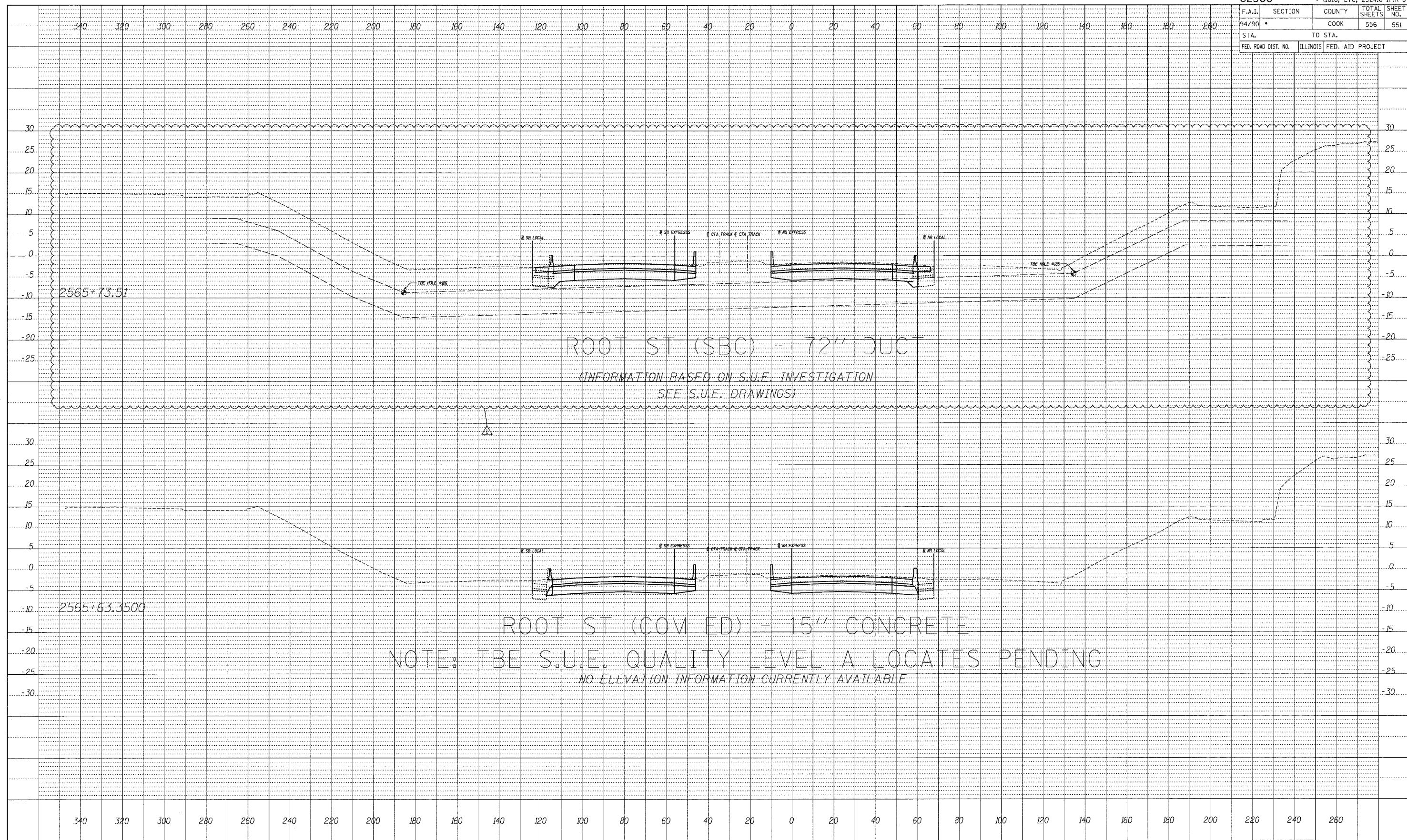
BOWMAN, BARRETT & ASSOCIATES INC.
 CONSULTING ENGINEERS
 Chicago, Illinois
 312.228.0100
 www.bbainc.com



STA. ___+___ TO STA. ___+___

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90		COOK	556	551
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID	PROJECT	

DATE	BY
PROFILE	REVIEWED
NOTE BOOK	DATE
NO.	
	CHANGES CHECKED
	DATE
	BY
	NO. OF CHANGES
	DATE
	BY
	NO. OF CHANGES
	DATE
	BY
	NO. OF CHANGES
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	BY
	NO. OF CHANGES
	DATE



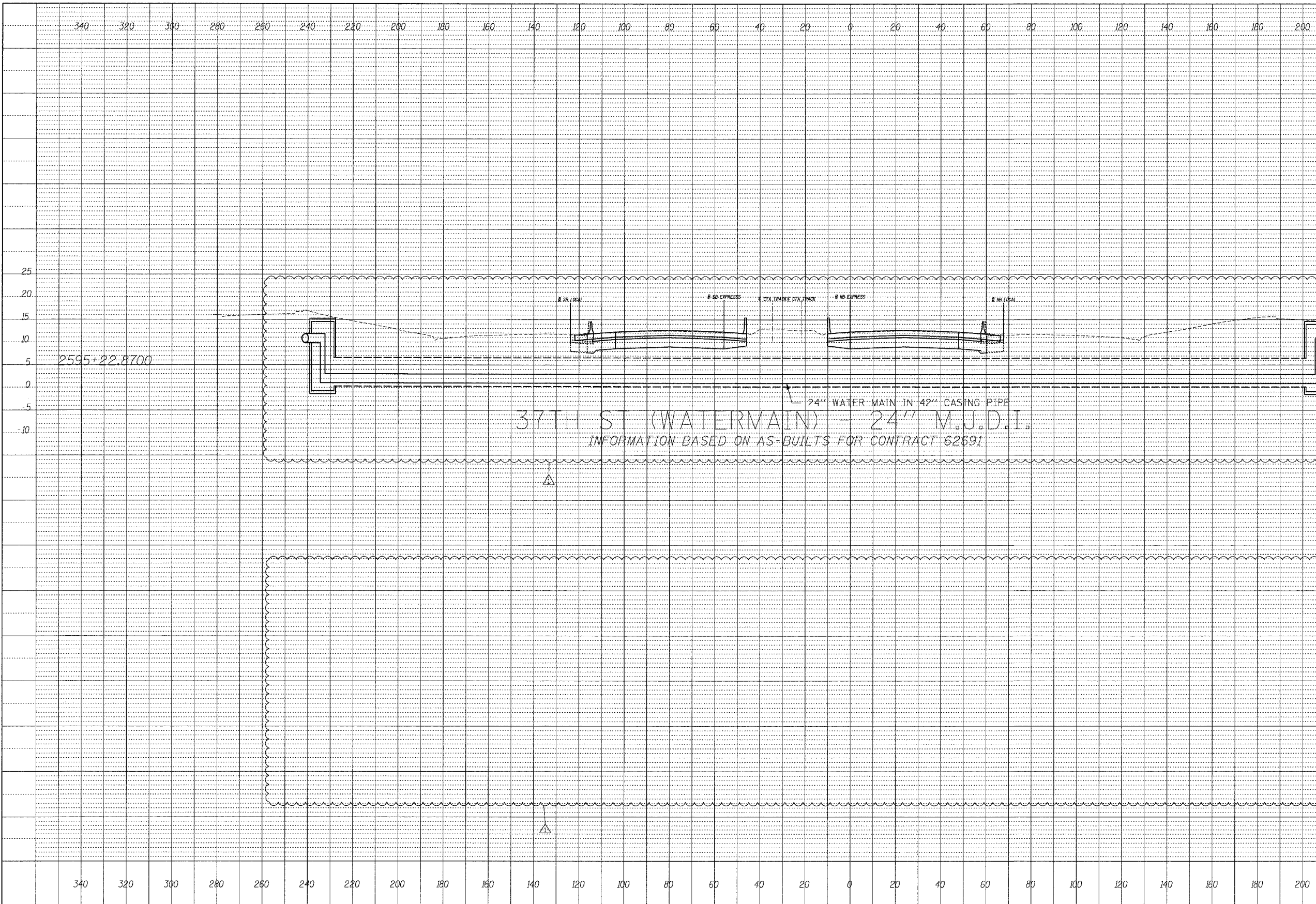
ROOT ST (COM ED) - 15" CONCRETE
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 NO ELEVATION INFORMATION CURRENTLY AVAILABLE

BOWMAN, BARRETT & ASSOCIATES INC.
 CONSULTING ENGINEERS
 Chicago, Illinois
 312.228.0100
 www.bbandainc.com



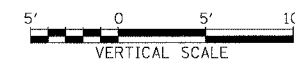
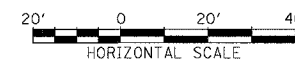
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F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90		COOK	556	553
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID	PROJECT	



PROFILE	BY	DATE
NO. _____		
NO. _____		
NO. _____		
NO. _____		

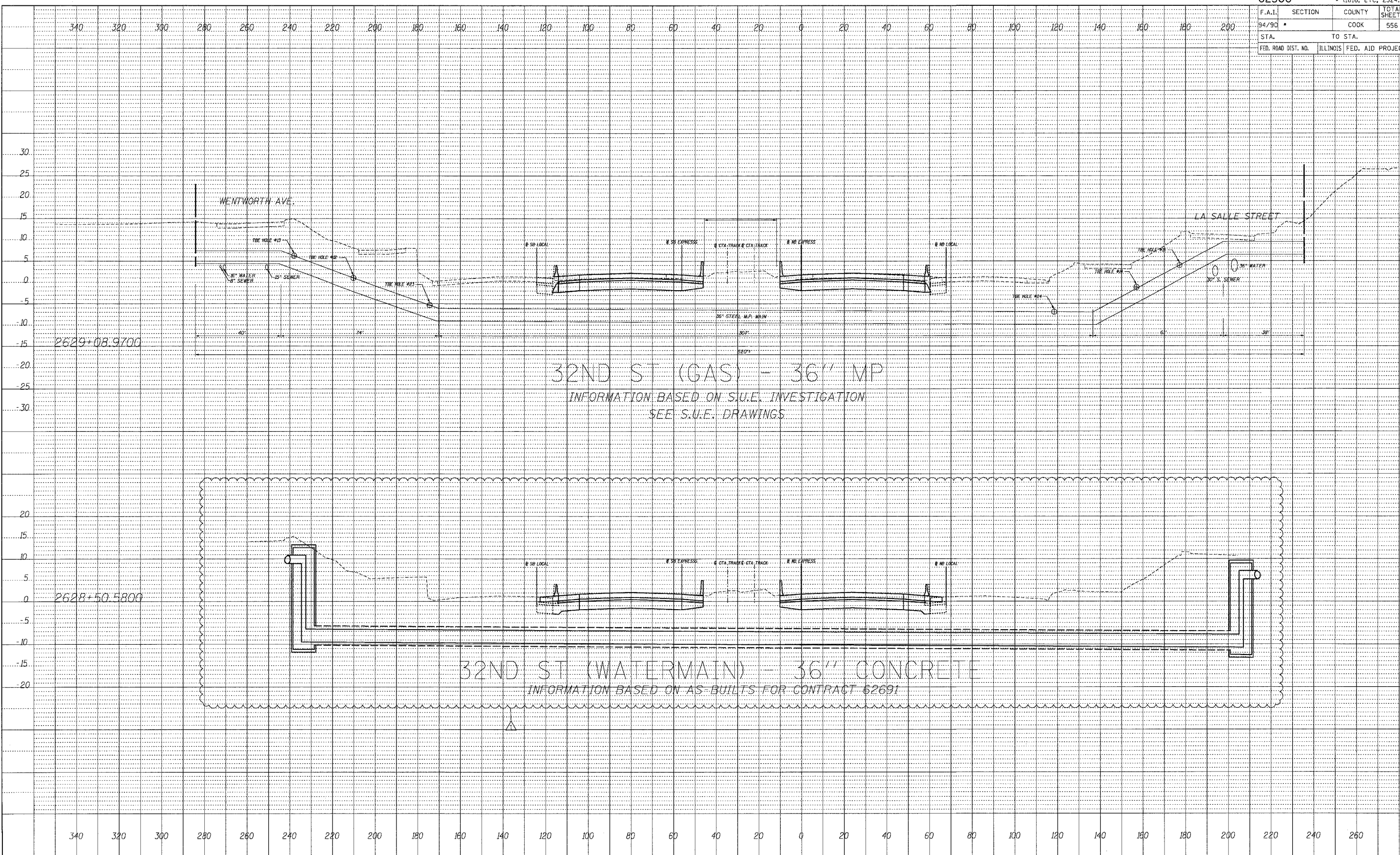
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 CONSULTING ENGINEERS
 Chicago, Illinois
 312.228.0100
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STA. ____+__ TO STA. ____+__

03/26/06 PM

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90		COOK	556	555
STA. _____		TO STA. _____		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID	PROJECT	



DATE	BY

BOWMAN, BARRETT & ASSOCIATES INC.
CONSULTING ENGINEERS
Chicago, Illinois
312.228.0100
www.bbainc.com

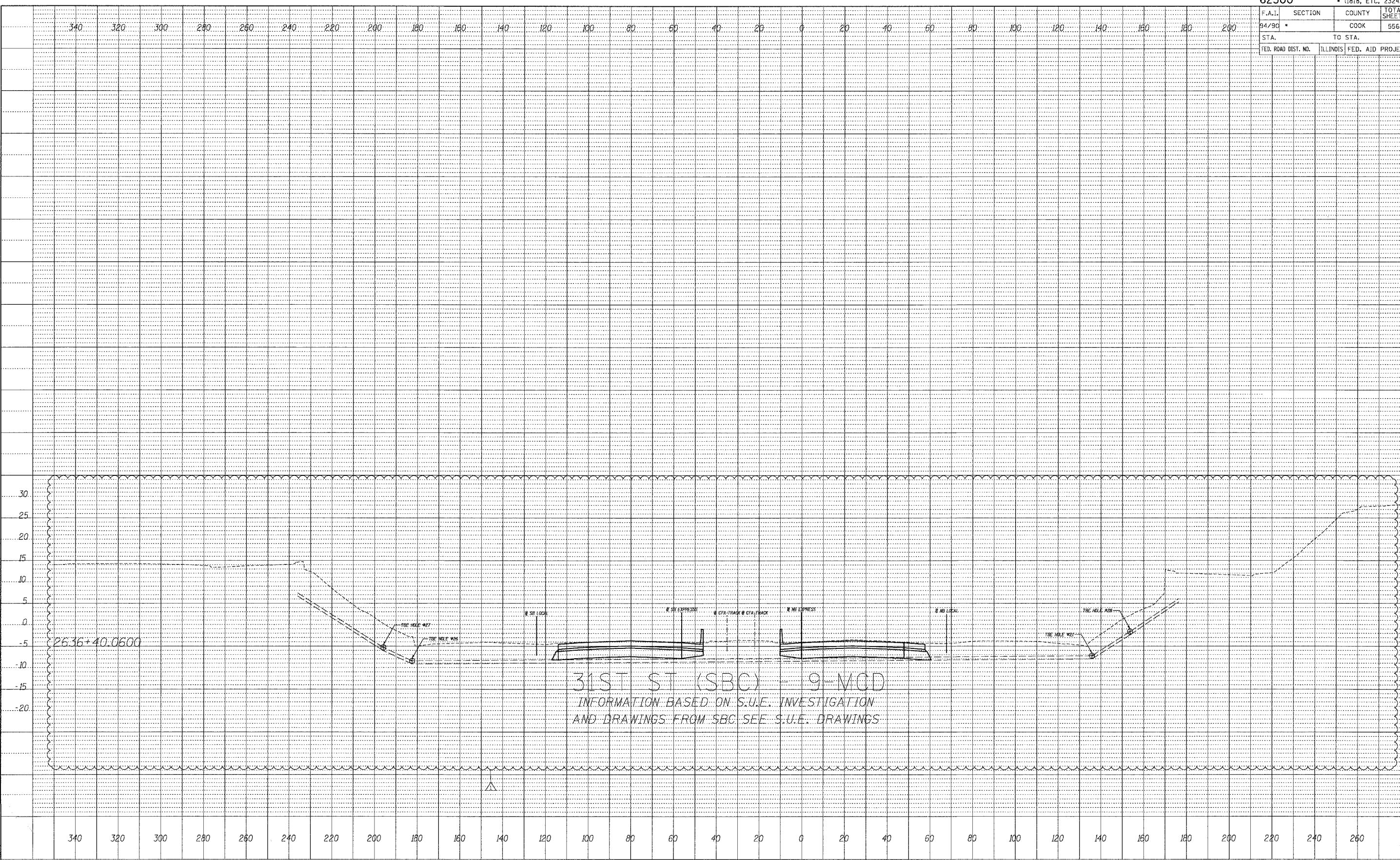


STA. ____+__ TO STA. ____+__

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F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90		COOK	556	556
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PROFILE	SURVEYED	DATE
PLANTED	CHECKED	
NOTE BOOK	NO. NOTED	
NO.	STRUCTURE NOTATIONS	CHRD



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