11-04-2016 LETTING ITEM 030

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	1
		ILLINOIS	CONTRACT NO. 60W06	

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROPOSED HIGHWAY PLANS

FAP 104: IL. RTE. 59 (FOX LAKE ROAD) IL 132 (GRAND AVE.) TO HIGHWOODS DRIVE AND AT PETITE LAKE ROAD

SECTION 105N-4

PROJECT: ACNHPP-0104(021)

RESURFACING, INTERSECTION RECONSTRUCTION AND TRAFFIC SIGNAL INSTALLATION

LAKE COUNTY

C-91-159-13

LOCATION OF SECTION INDICATED THUS:-

PROJECT IS LOCATED IN THE VILLAGE OF LAKE VILLA

TRAFFIC DATA:

ILL ROUTE 59:
2011 ADT = 10,600 – 13,500
SPEED LIMIT = 45 MPH

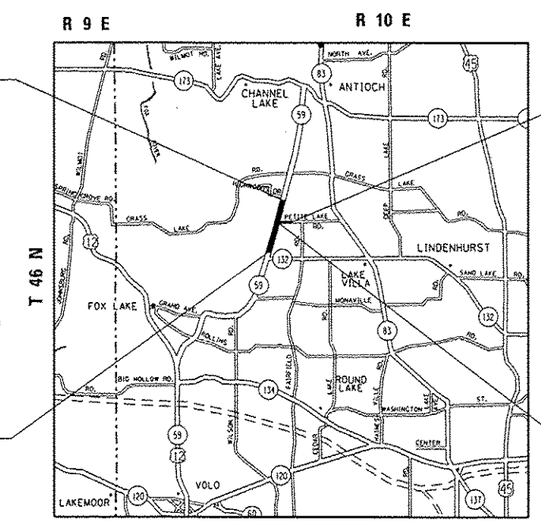
PETITE LAKE ROAD:
2008 ADT = 3,500
SPEED LIMIT = 40 MPH

IL ROUTE 59 PROJECT ENDS STATION 526+88

IL ROUTE 59 RESURFACING LIMITS:
STA. 447+96 TO STA. 491+58.60 &
STA. 508+42.30 TO STA. 526+88

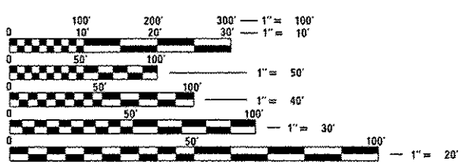
IL 59 INTERSECTION LIMIT:
STA. 491+58.60 TO STA. 508+42.30

IL ROUTE 59 PROJECT BEGINS STATION 447+96



RECONSTRUCTION
PETITE LAKE ROAD PROJECT ENDS STATION 208+67

RECONSTRUCTION
PETITE LAKE ROAD PROJECT BEGINS STATION 200+45.32



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED August 12, 2016
REGIONAL ENGINEER

Sept 30, 2016
Maureen M. Addis, P.E. for
ENGINEER OF DESIGN AND ENVIRONMENT

Sept 30, 2016
DIRECTOR OF PROGRAM DEVELOPMENT

2

PROJECT ENGINEER: JENPAI CHANG (847) 705-4432
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

ILL ROUTE 59: GROSS AND NET LENGTH = 7,892.00 FT. = 1.49 MILES

PETITE LAKE ROAD: GROSS AND NET LENGTH = 848.42 FT. = 0.16 MILE

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

CONTRACT NO. 60W06

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2-3	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
4-11	SUMMARY OF QUANTITIES
12-13	SCHEDULE OF QUANTITIES
14-18	EXISTING AND PROPOSED TYPICAL SECTIONS
19	ALIGNMENT, TIES, AND BENCHMARKS
20-24	RIGHT OF WAY PLATS
25-28	SUE INVESTIGATION AND UNDERGROUND UTILITIES
29-31	PROPOSED DETOUR PLAN
32-39	PROPOSED ROADWAY PLAN AND PROFILE
40	INTERSECTION GRADING PLAN
41-42	SUGGESTED STAGING PLAN
43-45	PROPOSED EROSION CONTROL PLAN
46-52	DRAINAGE AND UTILITIES PLAN
53-54	PROPOSED LANDSCAPING PLANS
55-59	PROPOSED PAVEMENT MARKING PLANS
60-70	TRAFFIC SIGNAL PLAN AND DETAILS
71-76	PROPOSED BOX CULVERT PLANS
77-80	LAKE COUNTY DIVISION OF TRANSPORTATION STANDARDS AND DETAILS
81	DRIVEWAY DETAILS DISTANCE BETWEEN R.O.W. AND FACE OF CURB / EDGE OF SHOULDER >= 4.5 m (15') (BD-01)
82	DRIVEWAY DETAILS DISTANCE BETWEEN R.O.W. AND FACE OF CURB / EDGE OF SHOULDER < 4.5 m (15') (BD-02)
83	OUTLET FOR CONCRETE CURB AND GUTTER (BD-03)
84	DETAILS OF STORM SEWER CONNECTION TO EXISTING SEWER (BD-07)
85	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)
86	MANHOLE WITH RESTRICTOR PLATE (BD-12)
87	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
88	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
89	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
90	DETAILS FOR DEPRESSED CURB AND GUTTER AND SHOULDER TREATMENT AT TBT TY 1 SPL (BD-34)
91	BENCHING DETAIL FOR EMBANKMENT WIDENING (BD-51)
92	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
93	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)
94	DISTRICT ONE TYPICAL PAVEMENT MARKING (TC-13)
95	PAVEMENT MARKING LETTERS AND SYMBOL FOR TRAFFIC STAGING (TC-16)
96	ARTERIAL ROAD INFORMATION SIGN (TC-21)
97	ARTERIAL ROAD INFORMATION SIGN (TC-22)
98	DRIVEWAY ENTRANCE SIGNING (TC-26)
99	PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE (BM-20)
100-113	CROSS SECTIONS

LIST OF STATE STANDARDS:

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEM
406201-01	MAILBOX TURNOUT
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
482001-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
482011-03	HMA SHLD. STRIPS / SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
542311-06	GRATING FOR CONCRETE FLARED END SECTION FOR 24" THRU 54" (600mm THRU 1350mm) PIPE
602001-02	CATCH BASIN TYPE A
602011-02	CATCH BASIN TYPE C
602401-03	MANHOLE TYPE A
602601-04	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
604001-04	FRAME AND LIDS TYPE 1
604091-03	FRAME AND GRATE TYPE 24
604036-03	GRATE TYPE 8
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630001-10	STEEL PLATE BEAM GUARDRAIL
630201-06	PCC / HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
630301-06	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
725001	REFLECTOR AND TERMINAL MARKER PLACEMENT
782006	REFLECTOR MARKERS AND MOUNTING DETAILS
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 24, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45 MPH
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS DAY ONLY
701326-04	LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING OPERATIONS FOR SPEED >= 45 MPH
701336-06	LANE CLOSURE, 2L, 2W, WORK AREA IN SERIES FOR SPEEDS >= 45 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-05	TRAFFIC CONTOL DEVICES
814001-03	HANDHOLES
814006-02	DOUBLE HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
877001-06	STEEL MAST ARM ASSEMBLY AND POLE 16 FEET THROUGH 55 FEET
878001-10	CONCRETE FOUNDATION DETAILS
880001-01	SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS

LIST OF LAKE COUNTY STANDARDS:

STANDARD NO.	DESCRIPTION
LC2050	TEMPORARY DITCH CHECK INSTALLATION FOR ROADWAY OR DITCH
LC5402	JOINT TIES FOR CONCRETE PIPE
LC7800	TYPICAL PAVEMENT MARKINGS FOR COUNTY HIGHWAYS

GENERAL NOTES:

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND LAKE COUNTY AND THE VILLAGE OF LAKE VILLA.
3. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
4. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
5. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
6. WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H)
7. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
8. THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.
9. ANY DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
10. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
11. LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER, WITH THE TYPE SPECIFIED ON THE PLANS), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

Rev.

GENERAL NOTES CONTINUED:

12. DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

13. FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAMES AND LIDS UNLESS OTHERWISE SPECIFIED IN THE PLANS.

14. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE PROJECT LIMITS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.

15. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKING ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

16. THE CONTRACTOR SHALL CONTACT THE DISTRICT TRAFFIC CONTROL SUPERVISOR AT (847)705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

17. THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, AREA TRAFFIC FIELD ENGINEER (OR TECHNICIAN), AT (847) 715-8419 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

18. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM AND FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.

19. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

20. FOR STORM SEWER CONSTRUCTED UNDER ROADWAY, BACKFILLING METHODS TWO AND THREE AUTHORIZED UNDER PROVISIONS OF ARTICLE 550.07 OF THE STANDARD SPECIFICATIONS WILL NOT BE ALLOWED.

20. THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF BURIED STRUCTURES ACCORDING TO THE STATION OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT, UPON COMPLETION OF THE WORK. THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

21. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

22. THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR$^{S}/_{32}$ S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) and USE/WASTE REVIEW(BDE 2290) SUBMITTALS, THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION II.G.1 AND 2 of the SWPPP. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

23. ALL PIPE UNDERDRAINS SHALL BE PLACED AT A DEPTH OF 30" BELOW THE TOP OF THE PROPOSED PAVEMENT OR AS DEEP AS POSSIBLE AND IN ACCORDANCE WITH CHECK SHEET #19 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONA. THE COST OF MAKING PIPE UNDERDRAIN CONNECTIONS TO DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST OF PIPE UNDERDRAINS ITEM.

24. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

25. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES.

26. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEMS SUCH AS FOUNDATION, CONDUIT, HANHOLE, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACE SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC, SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS 250 AND 252 RESPECTIVELY.

27. IF THIS CONTRACT REQUIRES THE SERVICES OF AN ELECTRICAL CONTRACTOR, THE CONTRACTOR SHALL BE RESPONSIBLE AT HIS/HER OWN EXPENSE FOR LOCATING EXISTING IDOT ELECTRICAL FACILITIES PRIOR TO PERFORMING ANY WORK, IF THIS CONTRACT DOES NOT REQUIRE THE SERVICES OF AN ELECTRICAL CONTRACTOR, THE CONTRACTOR MAY REQUEST ONE FREE LOCATE FOR EXISTING IDOT ELECTRICAL FACILITY FROM THE DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR PRIOR TO THE START OF ANY WORK. ADDITIONAL REQUESTS SHALL BE AT THE EXPENSE OF THE CONTRACTOR. THE LOCATIONS OF UNDERGROUND TRAFFIC FACILITIES DOES NOT RELIEVE THE CONTRACTOR OF THEIR RESPONSIBILITY TO REPAIR ANY FACILITIES DAMAGED DURING CONSTRUCTION AT THEIR EXPENSE.

28. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.

29. FULL DEPTH SAWCUT OF REMOVAL ITEMS AS NOTED ON THE PLANS, SPECIFIED IN THE STANDARD SPECIFICATIONS, OR AS REQUIRED BY THE RESIDENT ENGINEER SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

30. THIS PROJECT REQUIRES A US ARMY CORPS OF ENGINEERS (USACE) 404 PERMIT THAT WILL BE SECURED BY THE DEPARTMENT. ALL CONDITIONS OF THE 404 PERMIT, FOUND IN THE SPECIAL PROVISIONS, MUST BE FOLLOWED. AS A CONDITION OF THIS PERMIT, THE CONTRACTOR WILL NEED TO SUBMIT AN IN-STREAM WORK PLAN (INCLUDING WORK WITHIN WETLANDS) TO THE DEPARTMENT FOR APPROVAL. GUIDELINES ON ACCEPTABLE IN-STREAM WORK TECHNIQUES (INCLUDING WORK WITHIN WETLANDS) CAN BE FOUND ON THE USACE WEBSITE. THE USACE DEFINES AND DETERMINES IN-STREAM WORK. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT AN IN-STREAM WORK PLAN (INCLUDING WORK WITHIN WETLANDS) WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

COMMITMENT:

- NO CONSTRUCTION ACTIVITIES WILL BE ALLOWED IN THE MONTH OF DECEMBER FOR THE DRIVEWAYS ACCESS (APPROX. AT STA. 203+37.5, STA. 204+03, AND STA. 205+32.70) ALONG THE NORTH SIDE ON PETITE LAKE ROAD.
- THE DEPARTMENT WILL AVOID THE BARN DURING CONSTRUCTION. (APPROXIMATELY AT STATION 204+03).

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	FED LAKE VILLA TWP 0004
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	247		109	138			
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	186		99	87			
20101000	TEMPORARY FENCE	FOOT	1000			1000			
20101200	TREE ROOT PRUNING	EACH	12		6	6			
20200100	EARTH EXCAVATION	CU YD	13480		6211	7269			
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	1200		1200				
20700220	POROUS GRANULAR EMBANKMENT	CU YD	8		8				
20800150	TRENCH BACKFILL	CU YD	202		92	110			
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	4815			4815			
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	10900	20	6140	4487			253
25000210	SEEDING, CLASS 2A	ACRE	2.3		1.3	0.95			0.05
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	138		78	57			3
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	138		78	57			3
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	138		78	57	* SPECIALTY ITEMS		3
							▲ NON-PARTICIPATING ITEMS		
25100630	EROSION CONTROL BLANKET	SQ YD	8870		6140	2730			

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	FED LAKE VILLA TWP 0004
* 25100635	HEAVY DUTY EROSION CONTROL BLANKET	SQ YD	2010			2010			
* 25200110	SODDING, SALT TOLERANT	SQ YD	1060	1060					
* 25200200	SUPPLEMENTAL WATERING	UNIT	53			53			
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	600		500	100			
28000305	TEMPORARY DITCH CHECKS	FOOT	518		366	152			
28000400	PERIMETER EROSION BARRIER	FOOT	2625		1625	1000			
28000500	INLET AND PIPE PROTECTION	EACH	46		24	22			
28100105	STONE RIPRAP, CLASS A3	SQ YD	1122		1110	12			
28100107	STONE RIPRAP, CLASS A4	SQ YD	28		28				
28200200	FILTER FABRIC	SQ YD	1258		1138	120			
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	7293		2665	4628			
35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD	2642		520	622			1500
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	547		547				
35501312	HOT-MIX ASPHALT BASE COURSE, 7"	SQ YD	377		377				
35600704	HOT-MIX ASPHALT BASE COURSE WIDENING, 7"	SQ YD	1308		1308		* SPECIALTY ITEMS		
							▲ NON-PARTICIPATING ITEMS		

15

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P145409\CADData\Design\P145409DRAFT.dgn	USER NAME = ledezmorm	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
	PLOT DATE = 8/22/2016	DATE -	REVISED -

15

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 59: IL 132 TO HIGHWOODS DR. AND AT PETITE LAKE RD.
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	4
		CONTRACT NO. 60W06		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	FED LAKE VILLA TWP 0004
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	2160			2160			
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	23195	17575	5620				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	48	39	9				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1607	1074	533				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	266	266					
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	1327			1327			
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	336		336				
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	379		63	72			244
40603565	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70	TON	3510	2504	1006				
42001300	PROTECTIVE COAT	SQ YD	867	40	247	580			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	410		410				
42400800	DETECTABLE WARNINGS	SQ FT	54		40	14			

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	
44000100	PAVEMENT REMOVAL	SQ YD	2317		92	2225			
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	31561	24793	6768				
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1575		1065	510			
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	151		151				
44004250	PAVED SHOULDER REMOVAL	SQ YD	1248		1248				
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SQ YD	726	552	174				
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SQ YD	461	331	130				
44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SQ YD	351	221	130				
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	170	170					
48203019	HOT-MIX ASPHALT SHOULDERS, 5 1/2"	SQ YD	2831		2831				
50102400	CONCRETE REMOVAL	CU YD	1.7		1.7				
50105220	PIPE CULVERT REMOVAL	FOOT	499		305	194			
50200100	STRUCTURE EXCAVATION	CU YD	8		8				
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	6130		6130				
50901720	BICYCLE RAILING	FOOT	353		353				
52200015	PERMANENT SHEET PILING	SQ FT	595		595				
54001001	BOX CULVERT END SECTIONS, CULVERT NO. 1	EACH	1		1				

* SPECIALTY ITEMS

▲ NON-PARTICIPATING ITEMS

12

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16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 59: IL 132 TO HIGHWOODS DR. AND AT PETITE LAKE RD.
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	5

CONTRACT NO. 60W06

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	
54002020	EXPANSION BOLTS 3/4 INCH	EACH	24		24				
52200020	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	131		131				
54003000	CONCRETE BOX CULVERTS	CU YD	20.3		20.3				
54213657	PRECAST REINFORCED CONCRETE FLARED END	EACH	1		1				
	SECTIONS 12"								
54213660	PRECAST REINFORCED CONCRETE FLARED END	EACH	25		17	8			
	SECTIONS 15"								
54261424	CONCRETE END SECTION, STANDARD 542001,	EACH	4		2	2			
	24", 1:4								
542A0220	PIPE CULVERTS, CLASS A, TYPE 1 15"	FOOT	318		208	110			
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	482		72	410			
550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	484		354	130			
550A0120	STORM SEWERS, CLASS A, TYPE 1 24"	FOOT	105		24	81			
550A4000	STORM SEWERS, CLASS A, TYPE 1	FOOT	270		270				
	EQUIVALENT ROUND-SIZE 18"								
55100700	STORM SEWER REMOVAL 15"	FOOT	24		24				
55100900	STORM SEWER REMOVAL 18"	FOOT	218		218				
55101200	STORM SEWER REMOVAL 24"	FOOT	24		24		* SPECIALTY ITEMS		
							▲ NON-PARTICIPATING ITEMS		
60107600	PIPE UNDERDRAINS 4"	FOOT	2465		800	1665			

14

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	
60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE	EACH	8		3	5			
	24 FRAME AND GRATE								
60204505	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE	EACH	1		1				
	8 GRATE								
60205040	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE	EACH	2			2			
	24 FRAME AND GRATE								
60207605	CATCH BASINS, TYPE C, TYPE 8 GRATE	EACH	1			1			
60208240	CATCH BASINS, TYPE C, TYPE 24 FRAME AND	EACH	5		1	4			
	GRATE								
60219000	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 8	EACH	1			1			
	GRATE								
60219540	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 24	EACH	1		1				
	FRAME AND GRATE								
60221700	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 8	EACH	4		4				
	GRATE								
60250200	CATCH BASINS TO BE ADJUSTED	EACH	11	10		1			
60500050	REMOVING CATCH BASINS	EACH	1			1			
60500205	FILLING CATCH BASINS	EACH	1			1			
							* SPECIALTY ITEMS		
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	1		1		▲ NON-PARTICIPATING ITEMS		

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12

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 59: IL 132 TO HIGHWOODS DR. AND AT PETITE LAKE RD.
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	6
		CONTRACT NO. 60W06		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

SUMMARY OF QUANTITIES

URBAN

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE: IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	
60600605	CONCRETE CURB, TYPE B	FOOT	180			180			
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	1680			1680			
60610400	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24	FOOT	675		675				
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	100		100				
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	2		2				
63200310	GUARDRAIL REMOVAL	FOOT	306		306				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	2600		1300	1300			
* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1		1				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	5		3	2			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	5	7				
67100100	MOBILIZATION	L SUM	1	0.25	0.75				
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	21		21				
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	1		1				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	7692	4380	2508	804			

* SPECIALTY ITEMS

△ NON-PARTICIPATING ITEMS

SUMMARY OF QUANTITIES

URBAN

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE: IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	2564	1460	836	268			
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	598	254.8	270.4	72.8			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	34820	24670	7490	2660			
70300230	TEMPORARY PAVEMENT MARKING - LINE 5"	FOOT	1671			1671			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1630	852	581	197			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1280	447	585	248			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	120	43	52	25			
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1085	1085					
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	885	885					
70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	14	14					
70600332	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	9	9					
* 72000100	SIGN PANEL - TYPE 1	SQ FT	7				7		
* 72000200	SIGN PANEL - TYPE 2	SQ FT	21				21		
* 72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	12		9	3			

* SPECIALTY ITEMS

△ NON-PARTICIPATING ITEMS

14

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		CHECKED -	REVISED -		
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	PLOT DATE = 8/22/2016				

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	7
		CONTRACT NO. 60W06		

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

URBAN

	SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
	CODE NO	ITEM	UNIT		IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	
*	72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	4		2	2			
*	72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	2		2				
*	78000100	THERMOPLASTIC PAVEMENT MARKING -	SQ FT	597.2	254	270.4	72.8			
		LETTERS AND SYMBOLS								
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	34820	24670	7490	2660			
		4"								
*	78000300	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	1671			1671			
		5"								
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	1630	852	581	197			
		6"								
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	1412	447	585	380			
		12"								
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE	FOOT	120	43	52	25			
		24"								
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	394	299	95				
*	78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	3		3				
*	78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	77		77				
~~*~~	~~72501000~~	~~TERMINAL MARKER - DIRECT APPLIED~~	~~EACH~~	~~2~~		~~2~~				
	78300200	RAISED REFLECTIVE PAVEMENT MARKER	EACH	314	238	76				
		REMOVAL								
*	81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL,	FOOT	1021				1021		
		2" DIA.								

* Specialty Items

URBAN

	SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
	CODE NO	ITEM	UNIT		IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	
*	81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL,	FOOT	81				81		
		3" DIA.								
*	81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL,	FOOT	257				257		
		4" DIA.								
*	81400100	HANDHOLE	EACH	4				4		
*	81400200	HEAVY-DUTY HANDHOLE	EACH	6				6		
*	81400300	DOUBLE HANDHOLE	EACH	1				1		
*	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO.	FOOT	653				653		
		14 2C								
*	87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO.	FOOT	932				932		
		14 3C								
*	87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO.	FOOT	1309				1309		
		14 5C								
*	87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO.	FOOT	814				814		
		14 7C								
*	87301295	ELECTRIC CABLE IN CONDUIT, SIGNAL NO.	FOOT	251				251		
		20 3C								
*	87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO.	FOOT	1510				1510		
		14 1 PAIR								

12

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 59: IL 132 TO HIGHWOODS DR. AND AT PETITE LAKE RD.
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	8
		CONTRACT NO. 60W06		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	
* 87301800	ELECTRIC CABLE IN CONDUIT, SERVICE, NO.	FOOT	447				447		
	4 2 C								
* 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT	FOOT	891				891		
	GROUNDING CONDUCTOR, NO. 6 1C								
* 87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL	EACH	4				4		
	16 FT.								
* 87700160	STEEL MAST ARM ASSEMBLY AND POLE, 24	EACH	1				1		
	FT.								
* 87700230	STEEL MAST ARM ASSEMBLY AND POLE, 38	EACH	1				1		
	FT.								
* 87702920	STEEL COMBINATION MAST ARM ASSEMBLY AND	EACH	1				1		
	POLE 38 FT.								
87800100	CONCRETE FOUNDATION, TYPE A	FOOT	16				16		
*									
87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4				4		
* 87800400	CONCRETE FOUNDATION, TYPE E 30-INCH	FOOT	37				37		
	DIAMETER								
* 88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION,	EACH	4				4		
	MAST-ARM MOUNTED								
* 88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION,	EACH	4				4		
	BRACKET MOUNTED								

*Specialty Items

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	
* 88030070	SIGNAL HEAD, LED, 1-FACE, 4-SECTION,	EACH	3				3		
	BRACKET MOUNTED								
* 88030080	SIGNAL HEAD, LED, 1-FACE, 4-SECTION,	EACH	1				1		
	MAST ARM MOUNTED								
* 88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION,	EACH	1				1		
	BRACKET MOUNTED								
* 88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION,	EACH	1				1		
	MAST-ARM MOUNTED								
* 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE,	EACH	4				4		
	BRACKET MOUNTED WITH COUNTDOWN TIMER								
* 88200400	TRAFFIC SIGNAL BACKPLATE, FORMED	EACH	6				6		
	PLASTIC								
* 88500100	INDUCTIVE LOOP DETECTOR	EACH	7				7		
* 88600100	DETECTOR LOOP, TYPE I	FOOT	308				308		
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	20	20					
* 88600700	PREFORMED DETECTOR LOOP	FOOT	265				265		
* 88700200	LIGHT DETECTOR	EACH	2					2	
* 88700300	LIGHT DETECTOR AMPLIFIER	EACH	1					1	
* 88800100	PEDESTRIAN PUSH-BUTTON	EACH	4				4		

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 59: IL 132 TO HIGHWOODS DR. AND AT PETITIE LAKE RD.
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	9
		CONTRACT NO. 60W06		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

URBAN

	SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
	CODE NO	ITEM	UNIT		IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	
*	C2000652	SHRUB, ARONIA MELANOCARPA AUTUMN MAGIC,	EACH	70		70				
		(AUTUMN MAGIC BLACK CHOKEBERRY), 2'								
		HEIGHT, BALLED AND BURLAPPED								
*	C2003348	SHRUB, HAMAMELIS VIRGINIANA (COMMON	EACH	80			80			
		WITCHHAZEL), 4' HEIGHT, BALLED AND								
		BURLAPPED								
*	C2011948	SHRUB, VIBURNUM DENTATUM RALPH SENIOR	EACH	70		70				
		(AUTUMN JAZZ ARROWWOOD VIBURNUM), 4'								
		HEIGHT, BALLED AND BURLAPPED								
*	C2012436	SHRUB, VIBURNUM LENTAGO (NANNYBERRY	EACH	70		70				
		VIBURNUM), 3' HEIGHT, BALLED AND								
		BURLAPPED								
*	C2C016G3	SHRUB, CORNUS RACEMOSA (GRAY DOGWOOD),	EACH	50			50			
		CONTAINER GROWN, 3-GALLON								
*	C2C06004	SHRUB, RHUS COPALLINA VAR. LATIFOLIA	EACH	192		70	122			
		MORTON (PRAIRIE FLAME SHINING SUMAC),								
		3' HEIGHT, CONTAINER								
*	C2C09636	SHRUB, SAMBUCUS CANADENSIS (AMERICAN	EACH	116		76	40			
		ELDER), 3' HEIGHT, CONTAINER								
	X0301852	DEWATERING STRUCTURE NO. 1	EACH	1		1				
*	X0324085	EMERGENCY VEHICLE PRIORITY SYSTEM LINE	FOOT	251					251	
		SENSOR CABLE, NO. 20 3/C								

Δ = 100% STATE
X = Specialty Items

URBAN

	SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
	CODE NO	ITEM	UNIT		IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	
	X0327979	PAVEMENT MARKING REMOVAL - GRINDING	SQ FT	15239	9440	3747	2052			
*	X1400107	FULL-ACTUATED CONTROLLER AND TYPE SUPER	EACH	1				1		
		P CABINET								
*	X1400150	SERVICE INSTALLATION, GROUND MOUNTED,	EACH	1				1		
		METERED								
	X2020110	GRADING AND SHAPING SHOULDERS	UNIT	80	80					
	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	18		12	6			
	X4401198	HOT-MIX ASPHALT SURFACE REMOVAL,	SQ YD	4285		4285				
		VARIABLE DEPTH								
	~~X5200015~~	~~PERMANENT SHEET PILING~~	~~SQ FT~~	~~595~~		~~595~~				
Δ	X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	400	400					
	X6030310	FRAMES AND LIDS TO BE ADJUSTED	EACH	10	10					
		(SPECIAL)								
	X7010216	TRAFFIC CONTROL AND PROTECTION,	L SUM	1	0.25	0.75				
		(SPECIAL)								
	X7040125	PINNING TEMPORARY CONCRETE BARRIER	EACH	323		323				
*	X8620200	UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1				1		
	Z0004562	COMBINATION CONCRETE CURB AND GUTTER	FOOT	120	120					
		REMOVAL AND REPLACEMENT								
	Z0013798	CONSTRUCTION LAYOUT	L SUM	1		1				

9

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 59: IL 132 TO HIGHWOODS DR. AND AT PETITE LAKE RD.
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	10
		CONTRACT NO. 60W06		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	10	10					
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	204.9	204.9					
Z0064800	SELECTIVE CLEARING	UNIT	19			19			
~~S2200820~~	~~TEMPORARY SOIL RETENTION SYSTEM~~	~~SQ FT~~	~~131~~		~~131~~				
	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 8	EACH	1		1				
	GRATE, RESTRICTOR PLATE								
Ø Z0076600	TRAINEES	HOUR	500	500					
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOUR	500	500					
	Ø 0042 Δ NP= 100% STATE								

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 59 RESURF 0005	IL 59 WIDENING RESURF 0004	PETITE LAKE RD RECON 0004	FED STATE COUNTY 0021	LAKE VILLA FIRE DEPT 0021	

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 59: IL 132 TO HIGHWOODS DR. AND AT PETITE LAKE RD.
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	11

CONTRACT NO. 60W06

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

EARTHWORK SCHEDULE (LAKE COUNTY)
IL. 59 (FOX LAKE ROAD)

① LOCATION	② EARTH EXCAVATION	③ EARTH EXCAVATION ADJUSTED FOR SHRINKAGE	④ EMBANKMENT	⑤ EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
STATION	CUBIC YARD	CUBIC YARD	CUBIC YARD	CUBIC YARD
IL. RTE 59				
STA. 490+00 TO STA. 491+00	81.75	69.50	00.25	+69.25
STA. 491+00 TO STA. 492+00	173.25	147.50	02.00	+145.50
STA. 492+00 TO STA. 493+00	296.75	252.25	05.25	+247.00
STA. 493+00 TO STA. 494+00	297.25	252.75	01.25	+251.50
STA. 494+00 TO STA. 495+00	316.25	268.75	17.00	+251.75
STA. 495+00 TO STA. 496+00	141.25	120.00	69.25	+50.75
STA. 496+00 TO STA. 497+00	187.75	159.50	80.00	+79.50
STA. 497+00 TO STA. 498+00	670.25	569.75	20.25	+549.50
STA. 498+00 TO STA. 499+00	609.25	517.75	04.75	+513.00
STA. 499+00 TO STA. 500+00	445.50	378.75	00.00	+378.75
STA. 500+00 TO STA. 501+00	514.00	437.00	89.75	+347.25
STA. 501+00 TO STA. 502+00	489.25	415.75	121.75	+294.00
STA. 502+00 TO STA. 503+00	274.00	233.00	428.50	-195.50
STA. 503+00 TO STA. 504+00	155.75	132.50	602.00	-469.50
STA. 504+00 TO STA. 505+00	473.50	402.5	85.25	+317.25
STA. 505+00 TO STA. 506+00	513.25	436.25	05.75	+430.50
STA. 506+00 TO STA. 507+00	258.75	220.00	68.00	+152.00
STA. 507+00 TO STA. 508+00	195.75	166.50	60.00	+106.50
STA. 508+00 TO STA. 508+46	116.75	99.25	14.75	+84.50
TOTAL (1)	6211.00	5280.00	1876.00	+3604.00

EARTHWORK SCHEDULE CONTINUATION (LAKE COUNTY)
PETITE LAKE ROAD

① LOCATION	② EARTH EXCAVATION	③ EARTH EXCAVATION ADJUSTED FOR SHRINKAGE	④ EMBANKMENT	⑤ EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
STATION	CUBIC YARD	CUBIC YARD	CUBIC YARD	CUBIC YARD
PETITE LAKE ROAD				
STA. 200+50 TO STA. 202+00	3029.75	2375.25	00.00	+2375.25
STA. 202+00 TO STA. 203+00	1818.50	1545.75	00.00	+1545.75
STA. 203+00 TO STA. 204+00	1281.25	1089.00	01.50	+1087.50
STA. 204+00 TO STA. 205+00	288.25	245.00	81.50	+163.50
STA. 205+00 TO STA. 206+00	219.25	186.25	120.25	+66.00
STA. 206+00 TO STA. 207+00	293.00	249.00	285.25	-36.25
STA. 207+00 TO STA. 208+00	223.50	190.00	387.00	-197.00
STA. 208+00 TO STA. 208+65.07	114.75	97.50	160.25	-62.75
TOTAL (2)	7269.00	5978.00	1036.00	+4942.00
SUMMARY				
TOTAL (1)	6211.00	5280.00	1876.00	+3604.00
TOTAL (2)	7269.00	5978.00	1036.00	+4942.00
GRAND TOTAL	13480.00	11258.00	2912.00	+8546.00

COLUMN 1: LOCATION FROM PLAN

COLUMN 2: QUANTITY OF EARTH EXCAVATION (CUT) FROM CROSS SECTIONS

COLUMN 3: QUANTITY OF EARTH EXCAVATION (CUT) ADJUSTED FOR A SHRINKAGE FACTOR OF 15%

COLUMN 4: QUANTITY OF EMBANKMENT (FILL) FROM CROSS SECTIONS

COLUMN 5: (COLUMN 3 - COLUMN 4)
(+) = QUANTITY OF FURNISHED EXCAVATION NEEDED
(-) = QUANTITY TO BE WASTED

PETITE LAKE RD. PIPE UDERDRAIN, 4" (NORTH SIDE)

STATION	DESCRIPTION	LENGTH
STA. 201+03.71 - STA. 203+86.12	LONGITUDINAL (NORTH)	282.82'
STA. 203+86.12 - STA. 205+15.69	LONGITUDINAL (NORTH)	129.80'
STA. 205+15.69 - STA. 206+95.86	LONGITUDINAL (NORTH)	179.76'
	TOTAL	592.38'

PETITE LAKE RD. PIPE UDERDRAIN, 4" (SOUTH SIDE)

STATION	DESCRIPTION	LENGTH
STA. 200+44.40 - STA. 205+15.59	LONGITUDINAL (SOUTH)	470.82'
STA. 205+15.69 - STA. 206+94.60	LONGITUDINAL (SOUTH)	179.40'
	TOTAL	650.22'

IL RTE. 59, PIPE UNDERDRAIN, 4" (WEST SIDE)

STATION	DESCRIPTION	LENGTH
STA. 492+00 TO STA. 493+00	LONGITUDINAL	100'
STA. 501+00 TO STA. 502+00	LONGITUDINAL	100'
STA. 503+00 TO STA. 504+00	LONGITUDINAL	100'
STA. 505+50 TO STA. 506+50	LONGITUDINAL	100'
	TOTAL	400'

IL RTE. 59, PIPE UNDERDRAIN, 4" (EAST SIDE)

STATION	DESCRIPTION	LENGTH
STA. 492+00 TO STA. 493+00	LONGITUDINAL	100'
STA. 501+00 TO STA. 502+00	LONGITUDINAL	100'
STA. 503+00 TO STA. 504+00	LONGITUDINAL	100'
STA. 505+50 TO STA. 506+50	LONGITUDINAL	100'
	TOTAL	400'

TREE REMOVAL SCHEDULE (LAKE COUNTY)
IL. RTE. 59 (FOX LAKE ROAD) - RIGHT SIDE

LOCATION STATION	OFFSTET (FEET)	DESCRIPTION			REASON FOR REMOVAL
		CIRCUMFERENCE (INCH)	6 TO 15 UNITS	OVER 15 UNITS	
494+35.39	26.24 (RT)	30.00	10		IN PROPOSED HMA SHOULDER
494+75.78	25.84 (RT)	30.00	10		IN PROPOSED HMA SHOULDER
496+31.53	34.58 (RT)	35.00	11		IN FRONT SLOPE (EMBANKMENT AREA)
498+02.36	25.59 (RT)	50.00		16	IN PROPOSED HMA SHOULDER
498+16.71	27.55 (RT)	50.00		16	IN PROPOSED HMA SHOULDER
498+29.48	29.09 (RT)	50.00		16	IN PROPOSED HMA SHOULDER
498+48.60	26.68 (RT)	48.00		16	IN PROPOSED HMA SHOULDER
498+48.60	26.68 (RT)	48.00		16	IN PROPOSED HMA SHOULDER
498+83.69	29.77 (RT)	60.00		19	IN BACK OF CURB (EXCAVATIOON AREA)
500+79.48	45.89 (RT)	22.00	7		IN BACK SLOPE (EXCAVATION AREA)
500+99.87	43.73 (RT)	24.00	8		IN BACK SLOPE (EXCAVATION AREA)
501+32.84	43.99 (RT)	28.00	9		IN BACK SLOPE (EXCAVATION AREA)
501+64.66	44.28 (RT)	36.00	12		IN BACK SLOPE (EXCAVATION AREA)
		TOTAL	67	83	

TREE REMOVAL SCHEDULE (LAKE COUNTY)
PETITE LAKE ROAD - RIGHT SIDE

LOCATION STATION	OFFSTET (FEET)	DESCRIPTION			REASON FOR REMOVAL
		CIRCUMFERENCE (INCH)	6 TO 15 UNITS	OVER 15 UNITS	
200+39.99	35.38 (RT)	38.00	12		IN MULTI PATH (EXCAVATION AREA)
200+88.70	32.63 (RT)	42.00	14		IN PARKWAY (EXCAVATION AREA)
201+21.24	34.27 (RT)	36.00	12		IN MULTI PATH (EXCAVATION AREA)
201+83.10	31.47 (RT)	24	8		IN PARKWAY (EXCAVATION AREA)
202+52.76	33.29 (RT)	60.00		19	IN MULTI PATH (EXCAVATION AREA)
202+89.31	34.65 (RT)	36.00	12		IN MULTI PATH (EXCAVATION AREA)
203+00.23	33.71 (RT)	18.00	6		IN MULTI PATH (EXCAVATION AREA)
203+40.92	35.43 (RT)	48.00		16	IN MULTI PATH (EXCAVATION AREA)
203+85.36	37.51 (RT)	54.00		17	IN MULTI PATH (EXCAVATION AREA)
204+56.91	36.52 (RT)	42.00	14		IN MULTI PATH (EXCAVATION AREA)
205+17.06	40.18 (RT)	60.00		19	IN MULTI PATH (EMBANKMENT AREA)
205+38.35	35.17 (RT)	48.00		16	IN MULTI PATH (EMBANKMENT AREA)
206+29.35	37.75 (RT)	42.00	14		IN MULTI PATH (EMBANKMENT AREA)
		TOTAL	92	87	

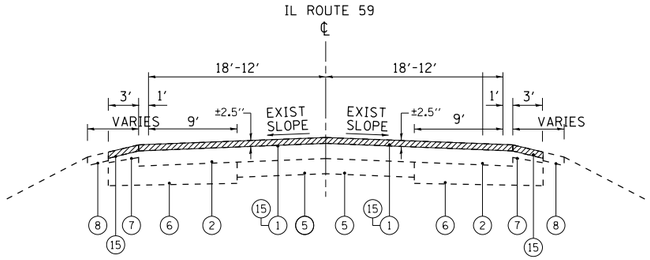
TREE REMOVAL SCHEDULE (LAKE COUNTY)
IL. RTE. 59 (FOX LAKE ROAD) - LEFT SIDE

LOCATION STATION	OFFSTET (FEET)	DESCRIPTION			REASON FOR REMOVAL
		CIRCUMFERENCE (INCH)	6 TO 15 UNITS	OVER 15 UNITS	
496+05.85	28.31 (LT)	36	12		IN FRONT SLOPE (EMBANKMENT AREA)
497+69.67	27.03 LRT)	18.00	6		IN FRONT SLOPE (EXCAVATION AREA)
496+21.25	26.51 (LT)	18.00	6		IN BACK OF CURB (EXCAVATION AREA)
499+04.12	27.23 (LT)	48.00		16	IN FRONT SLOPE (EXCAVATION AREA)
503+88.77	40.40 (LT)	24.00	8		IN FRONT SLOPE (EXCAVATION AREA)
508+30.06	43.06 (LT)	30.00	10		IN FRONT SLOPE (EXCAVATION AREA)
		TOTAL	42	16	

TREE REMOVAL SCHEDULE (LAKE COUNTY)
PETITE LAKE ROAD - LEFT SIDE

LOCATION STATION	OFFSTET (FEET)	DESCRIPTION			REASON FOR REMOVAL
		CIRCUMFERENCE (INCH)	6 TO 15 UNITS	OVER 15 UNITS	
200+75.29	31.78 (LT)	20.00	6		IN BACK SLOPE (EXCAVATION AREA)
200+94.32	31.15 (LT)	28.00	9		IN BACK SLOPE (EXCAVATION AREA)
201+10.60	31.05 (LT)	36.00	12		IN BACK SLOPE (EXCAVATION AREA)
201+61.91	31.44 (LT)	28.00	9		IN BACK SLOPE (EXCAVATION AREA)
201+78.26	31.29 (LT)	30.00	10		IN BACK SLOPE (EXCAVATION AREA)
		TOTAL	46		

REMOVAL ITEMS

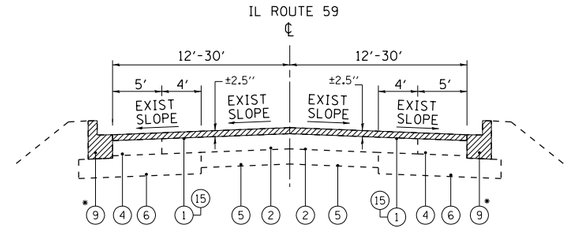


STA. 447+96 TO STA. 449+14
3' EXIST. HMA SHLD
3' AGG. SHLD.
STA. 449+14 TO STA. 453+00
8' HMA SHLD
STA. 472+18 TO COLUMBIA BAY RD
EXIST. CONC. C&G

STA. 447+96 TO STA. 451+45
8' EXIST. HMA SHLD
3' AGG. SHLD.

IL 59 (FOX LAKE RD)
EXISTING TYPICAL SECTION
RESURFACING AREA

STA. 447+96 TO STA. 451+08
STA. 472+18 TO COLUMBIA BAY RD.

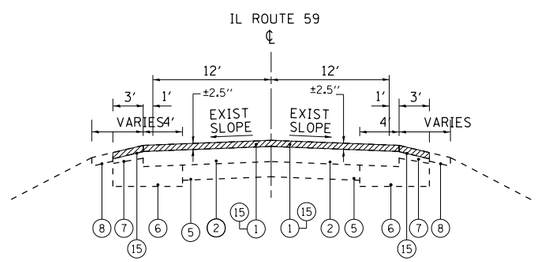


STA. 461+36 TO STA. 463+70
1' EXIST. HMA SHLD
3' AGG. SHLD.
STA. 480+16 TO STA. 483+26
3' EXIST. HMA SHLD
3' EXIST. AGG. SHLD
STA. 483+26 TO DERING LN.
EXIST DEPRESSED C&G
14' EXIST. HMA SHLD

IL 59 (FOX LAKE RD)
EXISTING TYPICAL SECTION
RESURFACING AREA

STA. 461+36 TO STA. 472+18
STA. 481+16 TO STA. 486+08

STA. 461+36 TO STA. 464+74
1' EXIST. HMA SHLD.
3' EXIST. AGG. SHLD.
STA. 481+16 TO STA. 484+29
3' EXIST. HMA SHLD.
3' EXIST. AGG. SHLD.

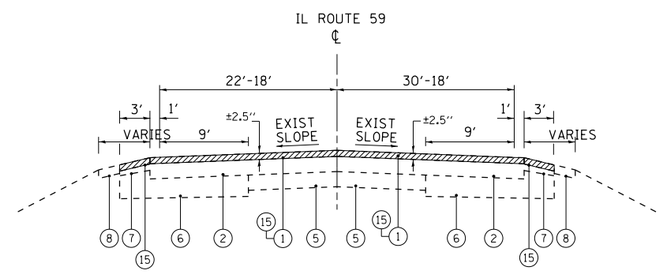


STA. 478+45 TO STA. 484+30
3' EXIST. HMA SHLD
3' AGG. SHLD.

STA. 478+45 TO STA. 484+30
3' EXIST. HMA SHLD
3' AGG. SHLD.

IL 59 (FOX LAKE RD)
EXISTING TYPICAL SECTION
RESURFACING AREA

STA. 451+08 TO STA. 461+36
COLUMBIA BAY RD. TO STA. 481+16
STA. 508+42.30 TO STA. 526+88



W. DERING LANE @ IL 59
EXIST C&G AT RADIUS RETURN
STA. 488+60 TO STA. 491+58.60
3' EXIST HMA SHLD

IL 59 (FOX LAKE RD)
EXISTING TYPICAL SECTION
AT DERING LANE
RESURFACING AREA

STA. 486+08 TO STA. 491+58.6

W. DERING LANE @ IL 59
EXIST C&G AT RADIUS RETURN
STA. 488+60 TO STA. 491+58.60
3' EXIST HMA SHLD

NOTE:

ADDITIONAL SUBBASE AGGREGATE MATERIAL UNDER AND BEHIND CURB AND GUTTER/HMA SHLD. 5½'' SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED AS INCLUDED IN THE COST PER SQUARE YARD OF ''AGGREGATE SUBGRADE IMPROVEMENT, 12''.

ANY SAW CUTTING REQUIRED TO REMOVE AN ITEM ADJACENT TO AN ITEM TO BE SAVED WILL BE CONSIDERED AS PART OF THE REMOVAL ITEM AND WILL NOT BE PAID FOR SEPARATELY.

''THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING''.

* PROFILE CORRECTION FROM STA 499+00 TO 505+00

LEGEND

1. EXISTING HMA SURFACING, ±2.5''
2. EXISTING HMA BASE COURSE, ±6''
3. EXISTING HMA PAVEMENT, ±11''
4. EXISTING HMA BASE CSE WIDENING, ±9''
5. EXISTING PCC PAVEMENT, ±6''
6. EXISTING SUB-BASE GRAN. MATERIAL
7. EXISTING HMA SHOULDER
8. EXISTING AGGREGATE SHOULDER, ±6''
9. EXISTING CONC. COMB. C&G TYPE B-6.24
10. EXISTING CONC. COMB. C&G TYPE B-6.12
11. PROPOSED PAVEMENT REMOVAL
12. PROPOSED SHOULDER PAVEMENT REMOVAL
13. * PROPOSED HMA BINDER COURSE, IL-19.0, N70, VARIES (PROFILE CORRECTION BINDER) (USED WHEN THICKNESS IS > 2¼'')
14. PROPOSED HMA BASE COURSE, 7'' (USED WHEN WIDENING GREATER OR EQUAL TO 6')
15. PROPOSED HMA SURF. REMOVAL, 2-1/2''
16. PROPOSED POLY. HMA SURF. CSE MIX ''E'', N70, 1-3/4''
17. PROPOSED POLY. LEVELING BINDER (MM), IL-4.75, N50, 3/4''
18. PROPOSED HMA BASE COURSE WIDENING, 7'' (USED WHEN WIDENING LESS THAN 6')
19. PROPOSED HMA SHOULDER, 5½''
20. PROPOSED HMA SURF CSE MIX ''D'', N50 (IL-9.5mm), 3''
21. PROPOSED HMA SURF. CSE MIX ''D'', N50, 2'' (PETITE LAKE ROAD)
22. PROPOSED HMA BINDER COURSE, 7.5'' (PETITE LAKE ROAD)
23. PROPOSED HMA SURF CSE MIX ''D'', N50 (IL-9.5mm), 3''
24. PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12''
25. PROPOSED CONC. COMB. C&G REM. & REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE ENGINEER)
26. PROPOSED COMB. C&G TYPE B-6-24
27. PROPOSED COMB. C&G TYPE M-6.24
28. PROPOSED AGG. BASE COURSE TYPE B, 6'' (SHARED PATH)
29. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
30. PROPOSED GRADING AND SHAPING SHOULDERS
31. PROPOSED PIPE UNDERDRAIN, 4''
32. PROPOSED LANDSCAPED PARKWAY
33. PROPOSED SWALE/DITCH
34. PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

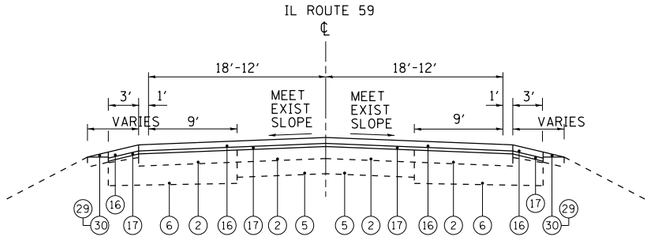
IL 59 (IL 132 TO W HIGHWOODS DR)
EXIST & PROP TYPICAL SECTIONS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	14

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT

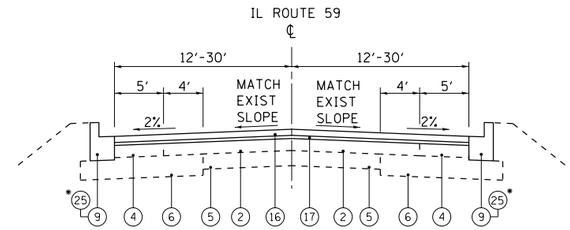


STA. 447+96 TO STA. 449+14
3' EXIST. HMA SHLD
3' AGG. SHLD.
STA. 449+14 TO STA. 453+00
8' HMA SHLD
STA. 472+18 TO COLUMBIA BAY RD
EXIST. CONC. C&G

STA. 447+96 TO STA. 451+45
8' EXIST. HMA SHLD
3' AGG. SHLD.

IL 59 (FOX LAKE RD)
PROPOSED TYPICAL SECTION
RESURFACING AREA

STA. 447+96 TO STA. 451+08
STA. 472+18 TO COLUMBIA BAY RD.

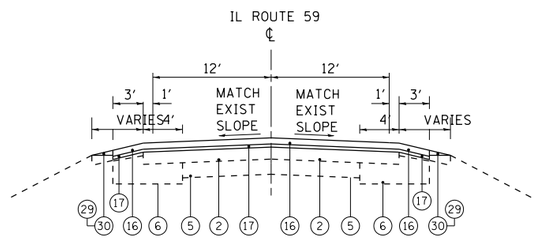


STA. 461+36 TO STA. 463+70
1' EXIST. HMA SHLD
3' AGG. SHLD.
STA. 480+16 TO STA. 483+26
3' EXIST. HMA SHLD
3' EXIST. AGG. SHLD
STA. 483+26 TO DERING LN.
EXIST DEPRESSED C&G
14' EXIST. HMA SHLD

IL 59 (FOX LAKE RD)
PROPOSED TYPICAL SECTION
RESURFACING AREA

STA. 461+36 TO STA. 472+18
STA. 481+16 TO STA. 486+08

STA. 461+36 TO STA. 464+74
1' EXIST. HMA SHLD.
3' EXIST. AGG. SHLD.
STA. 481+16 TO STA. 484+29
3' EXIST. HMA SHLD.
3' EXIST. AGG. SHLD.

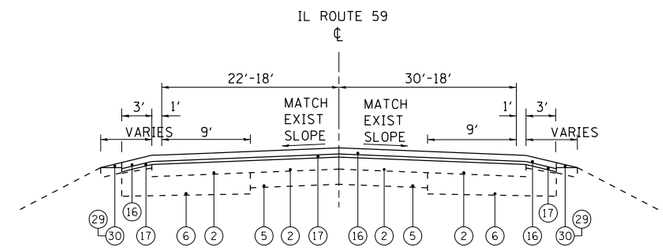


STA. 478+45 TO STA. 484+30
3' EXIST. HMA SHLD
3' AGG. SHLD.

STA. 478+45 TO STA. 484+30
3' EXIST. HMA SHLD
3' AGG. SHLD.

IL 59 (FOX LAKE RD)
PROPOSED TYPICAL SECTION
RESURFACING AREA

STA. 451+08 TO STA. 461+36
COLUMBIA BAY RD. TO STA. 481+16
STA. 508+42.30 TO STA. 526+88



W. DERING LANE @ IL 59
EXIST C&G AT RADIUS RETURN
STA. 488+60 TO STA. 491+58.60
3' EXIST HMA SHLD

W. DERING LANE @ IL 59
EXIST C&G AT RADIUS RETURN
STA. 488+60 TO STA. 491+58.60
3' EXIST HMA SHLD

IL 59 (FOX LAKE RD)
PROPOSED TYPICAL SECTION
AT DERING LANE
RESURFACING AREA

STA. 486+08 TO STA. 491+58.6

NOTE:

ADDITIONAL SUBBASE AGGREGATE MATERIAL UNDER AND BEHIND CURB AND GUTTER/HMA SHLD. 5½" SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED AS INCLUDED IN THE COST PER SQUARE YARD OF "AGGREGATE SUBGRADE IMPROVEMENT, 12".

ANY SAW CUTTING REQUIRED TO REMOVE AN ITEM ADJACENT TO AN ITEM TO BE SAVED WILL BE CONSIDERED AS PART OF THE REMOVAL ITEM AND WILL NOT BE PAID FOR SEPARATELY.

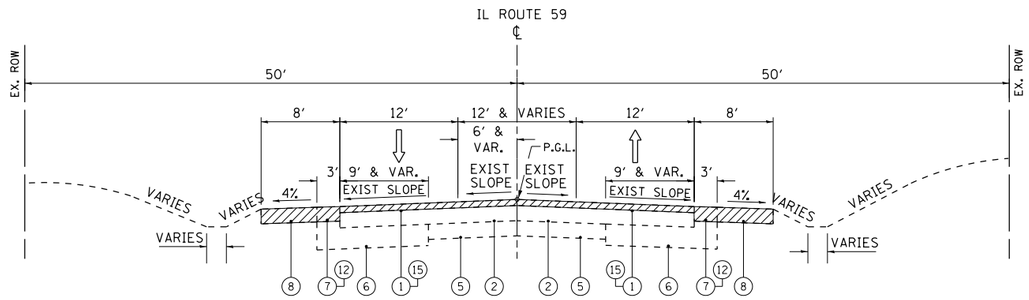
"THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING".

* PROFILE CORRECTION FROM STA 499+00 TO 505+00

LEGEND

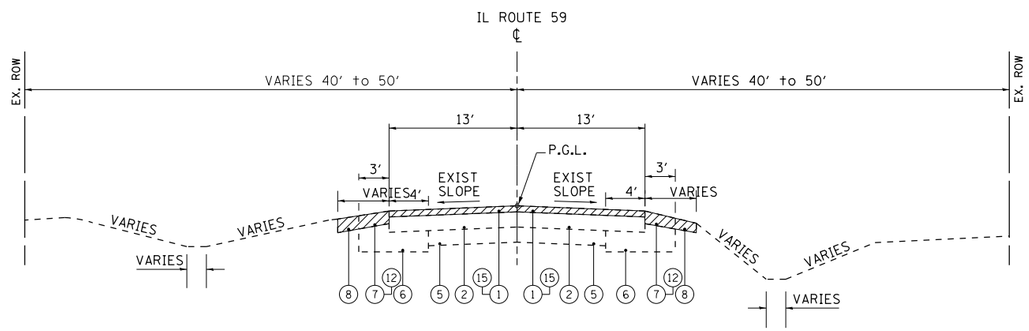
1. EXISTING HMA SURFACING, ±2.5"
2. EXISTING HMA BASE COURSE, ±6"
3. EXISTING HMA PAVEMENT, ±11"
4. EXISTING HMA BASE CSE WIDENING, ±9"
5. EXISTING PCC PAVEMENT, ±6"
6. EXISTING SUB-BASE GRAN. MATERIAL
7. EXISTING HMA SHOULDER
8. EXISTING AGGREGATE SHOULDER, ±6"
9. EXISTING CONC. COMB. C&G TYPE B-6.24
10. EXISTING CONC. COMB. C&G TYPE B-6.12
11. PROPOSED PAVEMENT REMOVAL
12. PROPOSED SHOULDER PAVEMENT REMOVAL
13. * PROPOSED HMA BINDER COURSE, IL-19.0, N70, VARIES (PROFILE CORRECTION BINDER) (USED WHEN THICKNESS IS > 2¼")
14. PROPOSED HMA BASE COURSE, 7" (USED WHEN WIDENING GREATER OR EQUAL TO 6')
15. PROPOSED HMA SURF. REMOVAL, 2-1/2"
16. PROPOSED POLY. HMA SURF. CSE MIX "E", N70, 1-3/4"
17. PROPOSED POLY. LEVELING BINDER (MM), IL-4.75, N50, 3/4"
18. PROPOSED HMA BASE COURSE WIDENING, 7" (USED WHEN WIDENING LESS THAN 6')
19. PROPOSED HMA SHOULDER, 5½"
20. PROPOSED HMA SURF CSE MIX "D", N50 (IL-9.5mm), 3"
21. PROPOSED HMA SURF. CSE MIX "D", N50, 2" (PETITE LAKE ROAD)
22. PROPOSED HMA BINDER COURSE, 7.5" (PETITE LAKE ROAD)
23. PROPOSED HMA SURF CSE MIX "D", N50 (IL-9.5mm), 3"
24. PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"
25. PROPOSED CONC. COMB. C&G REM. & REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE ENGINEER)
26. PROPOSED COMB. C&G TYPE B-6-24
27. PROPOSED COMB. C&G TYPE M-6.24
28. PROPOSED AGG. BASE COURSE TYPE B, 6" (SHARED PATH)
29. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
30. PROPOSED GRADING AND SHAPING SHOULDERS
31. PROPOSED PIPE UNDERDRAIN, 4"
32. PROPOSED LANDSCAPED PARKWAY
33. PROPOSED SWALE/DITCH
34. PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

REMOVAL ITEMS



IL 59 (FOX LAKE RD)
EXISTING TYPICAL SECTION

STA. 491+63 TO 494+42



IL 59 (FOX LAKE RD)
EXISTING TYPICAL SECTION

STA. 494+42 TO 508+47

NOTE:

ADDITIONAL SUBBASE AGGREGATE MATERIAL UNDER AND BEHIND CURB AND GUTTER/HMA SHLD. 5½" SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED AS INCLUDED IN THE COST PER SQUARE YARD OF "AGGREGATE SUBGRADE IMPROVEMENT, 12".

ANY SAW CUTTING REQUIRED TO REMOVE AN ITEM ADJACENT TO AN ITEM TO BE SAVED WILL BE CONSIDERED AS PART OF THE REMOVAL ITEM AND WILL NOT BE PAID FOR SEPARATELY.

"THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING".

* PROFILE CORRECTION FROM STA 499+00 TO 505+00

LEGEND

1. EXISTING HMA SURFACING, ±2.5"
2. EXISTING HMA BASE COURSE, ±6"
3. EXISTING HMA PAVEMENT, ±11"
4. EXISTING HMA BASE CSE WIDENING, ±9"
5. EXISTING PCC PAVEMENT, ±6"
6. EXISTING SUB-BASE GRAN. MATERIAL
7. EXISTING HMA SHOULDER
8. EXISTING AGGREGATE SHOULDER, ±6"
9. EXISTING CONC. COMB. C&G TYPE B-6.24
10. EXISTING CONC. COMB. C&G TYPE B-6.12
11. PROPOSED PAVEMENT REMOVAL
12. PROPOSED SHOULDER PAVEMENT REMOVAL
13. * PROPOSED HMA BINDER COURSE, IL-19.0, N70, VARIES (PROFILE CORRECTION BINDER) (USED WHEN THICKNESS IS > 2¼")
14. PROPOSED HMA BASE COURSE, 7" (USED WHEN WIDENING GREATER OR EQUAL TO 6')
15. PROPOSED HMA SURF. REMOVAL, 2-1/2"
16. PROPOSED POLY. HMA SURF. CSE MIX "E", N70, 1-3/4"
17. PROPOSED POLY. LEVELING BINDER (MM), IL-4.75, N50, 3/4"
18. PROPOSED HMA BASE COURSE WIDENING, 7" (USED WHEN WIDENING LESS THAN 6')
19. PROPOSED HMA SHOULDER, 5½"
20. PROPOSED HMA SURF CSE MIX "D", N50 (IL-9.5mm), 3"
21. PROPOSED HMA SURF. CSE MIX "D", N50, 2" (PETITE LAKE ROAD)
22. PROPOSED HMA BINDER COURSE, 7.5" (PETITE LAKE ROAD)
23. PROPOSED HMA SURF CSE MIX "D", N50 (IL-9.5mm), 3"
24. PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"
25. PROPOSED CONC. COMB. C&G REM. & REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE ENGINEER)
26. PROPOSED COMB. C&G TYPE B-6-24
27. PROPOSED COMB. C&G TYPE M-6.24
28. PROPOSED AGG. BASE COURSE TYPE B, 6" (SHARED PATH)
29. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
30. PROPOSED GRADING AND SHAPING SHOULDERS
31. PROPOSED PIPE UNDERDRAIN, 4"
32. PROPOSED LANDSCAPED PARKWAY
33. PROPOSED SWALE/DITCH
34. PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS (%) @ NDES	
RESURFACING: (IL 59)		
POLY. HMA SURFACE COURSE MIX "E", N70, 1¾" (IL-9.5mm)	4% @ 70 GYR.	QCP
POLY. LEVELING BINDER (MM) IL-4.75, N50, 3/4"	3.5% @ 50 GYR.	QCP
WIDENING: (IL 59)		
POLY. HMA SURFACE COURSE MIX "E", N70, 1¾" (IL-9.5mm)	4% @ 70 GYR.	QCP
POLY. LEVELING BINDER (MM) (IL-4.75, N50, 3/4"	3.5% @ 50 GYR.	QCP
HMA BASE COURSE WIDENING (HMA BINDER IL-19mm), 7"	4% @ 70 GYR.	QCP (note 1)
HMA BASE COURSE (HMA BINDER IL-19mm), 7"	4% @ 70 GYR.	QCP (note 1)
PROFILE CORRECTION BINDER (HMA BINDER COURSE IL-19.0, N70), THICKNESS > 2¼"	4% @ 70 GYR.	QCP (note 1)
PETITE LAKE RD: (RECONSTRUCTION)		
HMA SURFACE COURSE MIX "D", N50, 2" (IL-9.5mm)	4% @ 50 GYR.	QC/QA
HMA BINDER COURSE IL-19, N50, 7½"	4% @ 50 GYR.	QCP
SHOULDER: (IL 59 WIDENING SECTION)		
POLY. HMA SURFACE COURSE MIX "E", N70, 1¾" (IL-9.5mm)	4% @ 70 GYR.	QCP
POLY. LEVELING BINDER (MM) IL-4.75, N50, 3/4"	3.5% @ 50 GYR.	QCP
HMA SHOULDER (HMA BINDER IL-19 mm), 5½"	4% @ 70 GYR.	QCP (note 1)
SHOULDER: (IL 59 RESURFACING SECTION)		
POLY. HMA SURFACE COURSE MIX "E", N70, 1¾" (IL-9.5mm)	4% @ 70 GYR.	QCP
POLY. LEVELING BINDER (MM) IL-4.75, N50, 3/4"	3.5% @ 50 GYR.	QCP
IL 59: HOT-MIX ASPHALT PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm), 12-1/2"	4% @ 70 GYR.	QC/QA
IL 59 & PETITE LAKE RD: (SHARED PATH)		
HMA SURFACE COURSE MIX "D", N50 (IL 9.5mm), 3"	4% @ 50 GYR.	QC/QA
PETITE LAKE RD.: HMA DRIVEWAY (P.E.)		
HMA SURFACE COURSE MIX "D", N50, (IL 9.5mm), 2-1/2"	4% @ 50 GYR.	QC/QA
IL 59: HMA DRIVEWAY (P.E.)		
HMA SURFACE COURSE MIX "D", N50, (IL 9.5mm), 2"	4% @ 50 GYR.	QC/QA
HMA BASE COURSE (IL-19mm), 6"	4% @ 50 GYR.	QC/QA
QMP DESIGNATION QUALITY CONTROL/QUALITY ASSURANCE (QC/QA) QUALITY CONTROL FOR PERFORMANCE (QCP)		

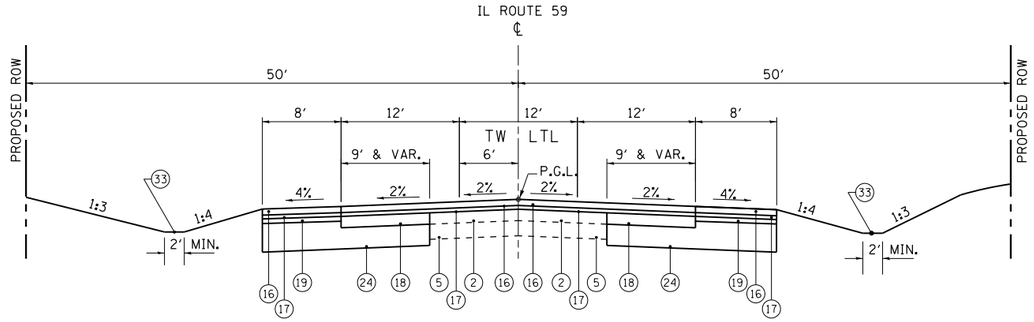
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 POUND PER SQUARE YARD-INCH

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS / SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

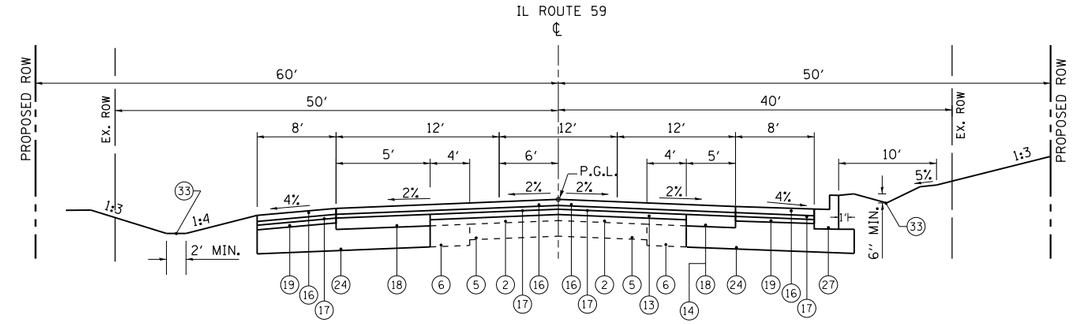
"FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS".

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

note 1:
THE DENSITY FOR THIS ITEM WILL BE ASSIGEND 100% PAY FACTOR, BE TESTED RANDOMLY AT THE FREQUENCY REQUIRED IN THE HMA-QUALITY CONTROL FOR PERFOMANCE SPECIAL PROVISION, AND SHALL MEET MINIMUM QC/QA DENSITY REQUIREMENTS IN ACCORDANCE WITH ART. 1030 OF THE STANDARD SPECIFICATIONS



IL 59 (FOX LAKE RD)
PROPOSED TYPICAL SECTION
STA. 491+58.6 TO 495+20



IL 59 (FOX LAKE RD)
PROPOSED TYPICAL SECTION
STA. 497+85 TO 502+15

LEGEND

1. EXISTING HMA SURFACING, ±2.5″
2. EXISTING HMA BASE COURSE, ±6″
3. EXISTING HMA PAVEMENT, ±11″
4. EXISTING HMA BASE CSE WIDENING, ±9″
5. EXISTING PCC PAVEMENT, ±6″
6. EXISTING SUB-BASE GRAN. MATERIAL
7. EXISTING HMA SHOULDER
8. EXISTING AGGREGATE SHOULDER, ±6″
9. EXISTING CONC. COMB. C&G TYPE B-6.24
10. EXISTING CONC. COMB. C&G TYPE B-6.12
11. PROPOSED PAVEMENT REMOVAL
12. PROPOSED SHOULDER PAVEMENT REMOVAL
13. * PROPOSED HMA BINDER COURSE, IL-19.0, N70, VARIES (PROFILE CORRECTION BINDER) (USED WHEN THICKNESS IS > 2¼″)
14. PROPOSED HMA BASE COURSE, 7″ (USED WHEN WIDENING GREATER OR EQUAL TO 6′)
15. PROPOSED HMA SURF. REMOVAL, 2-1/2″
16. PROPOSED POLY. HMA SURF. CSE MIX ″E″, N70, 1-3/4″
17. PROPOSED POLY. LEVELING BINDER (MM), IL-4.75, N50, 3/4″
18. PROPOSED HMA BASE COURSE WIDENING, 7″ (USED WHEN WIDENING LESS THAN 6′)
19. PROPOSED HMA SHOULDER, 5½″
20. PROPOSED HMA SURF CSE MIX ″D″, N50 (IL-9.5mm), 3″
21. PROPOSED HMA SURF. CSE MIX ″D″, N50, 2″ (PETITE LAKE ROAD)
22. PROPOSED HMA BINDER COURSE, 7.5″ (PETITE LAKE ROAD)
23. PROPOSED HMA SURF CSE MIX ″D″, N50 (IL-9.5mm), 3″
24. PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12″
25. PROPOSED CONC. COMB. C&G REM. & REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE ENGINEER)
26. PROPOSED COMB. C&G TYPE B-6-24
27. PROPOSED COMB. C&G TYPE M-6.24
28. PROPOSED AGG. BASE COURSE TYPE B, 6″ (SHARED PATH)
29. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
30. PROPOSED GRADING AND SHAPING SHOULDERS
31. PROPOSED PIPE UNDERDRAIN, 4″
32. PROPOSED LANDSCAPED PARKWAY
33. PROPOSED SWALE/DITCH
34. PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

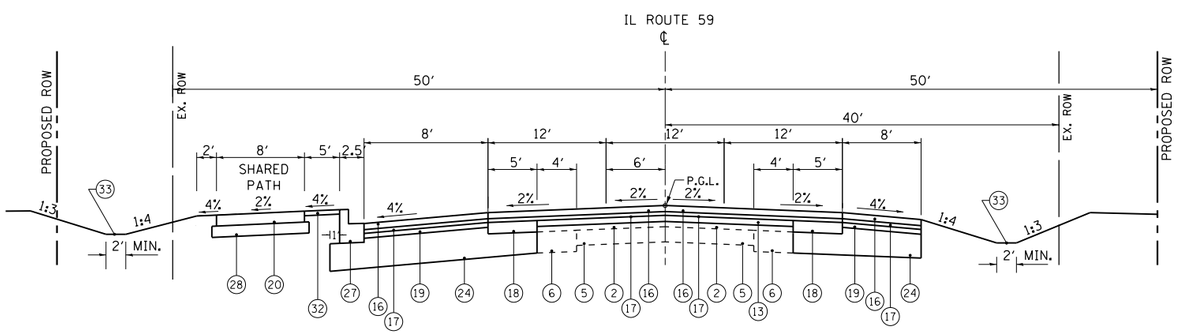
NOTE:

ADDITIONAL SUBBASE AGGREGATE MATERIAL UNDER AND BEHIND CURB AND GUTTER/HMA SHLD. 5½″ SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED AS INCLUDED IN THE COST PER SQUARE YARD OF ″AGGREGATE SUBGRADE IMPROVEMENT, 12″.

ANY SAW CUTTING REQUIRED TO REMOVE AN ITEM ADJACENT TO AN ITEM TO BE SAVED WILL BE CONSIDERED AS PART OF THE REMOVAL ITEM AND WILL NOT BE PAID FOR SEPARATELY.

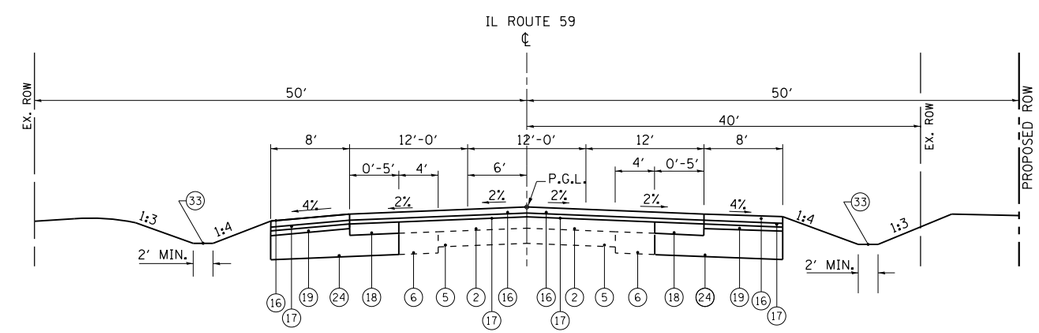
″THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING″.

* PROFILE CORRECTION FROM STA 499+00 TO 505+00



PROPOSED SHARED PATH:
STA. 500+38.44 TO RAVINE DR.

IL 59 (FOX LAKE RD)
PROPOSED TYPICAL SECTION
STA. 495+20 TO 497+85
STA. 502+15 TO 505+47



IL 59 (FOX LAKE RD)
PROPOSED TYPICAL SECTION
STA. 505+47 TO 508+42.3

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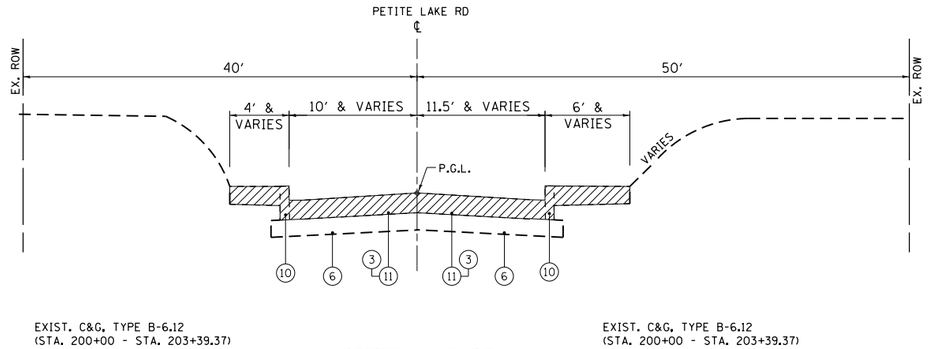
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING & PROPOSED TYPICAL SECTIONS
IL. ROUTE 59 AT PETITE LAKE RD.

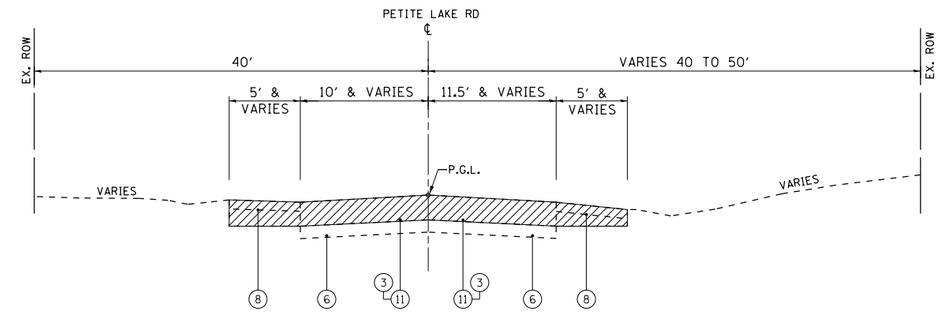
SCALE: NONE | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	17
		CONTRACT NO. 60W06		
		ILLINOIS FED. AID PROJECT		



PETITE LAKE RD
EXISTING TYPICAL SECTION
STA. 200+00 TO 203+39.37
(0+00 TO 3+39.37, LCDOT (LAKE COUNTY DOT))

REMOVAL ITEMS



PETITE LAKE RD
EXISTING TYPICAL SECTION
STA. 203+39.37 TO 208+67
(3+39.37 TO 8+67, LCDOT)

NOTE:

ADDITIONAL SUBBASE AGGREGATE MATERIAL UNDER AND BEHIND CURB AND GUTTER/HMA SHLD. 5½″ SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED AS INCLUDED IN THE COST PER SQUARE YARD OF ″AGGREGATE SUBGRADE IMPROVEMENT, 12″.

ANY SAW CUTTING REQUIRED TO REMOVE AN ITEM ADJACENT TO AN ITEM TO BE SAVED WILL BE CONSIDERED AS PART OF THE REMOVAL ITEM AND WILL NOT BE PAID FOR SEPARATELY.

″THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING″.

* PROFILE CORRECTION FROM STA 499+00 TO 505+00

LEGEND

1. EXISTING HMA SURFACING, ±2.5″
2. EXISTING HMA BASE COURSE, ±6″
3. EXISTING HMA PAVEMENT, ±11″
4. EXISTING HMA BASE CSE WIDENING, ±9″
5. EXISTING PCC PAVEMENT, ±6″
6. EXISTING SUB-BASE GRAN. MATERIAL
7. EXISTING HMA SHOULDER
8. EXISTING AGGREGATE SHOULDER, ±6″
9. EXISTING CONC. COMB. C&G TYPE B-6.24
10. EXISTING CONC. COMB. C&G TYPE B-6.12
11. PROPOSED PAVEMENT REMOVAL
12. PROPOSED SHOULDER PAVEMENT REMOVAL
13. * PROPOSED HMA BINDER COURSE, IL-19.0, N70, VARIES (PROFILE CORRECTION BINDER) (USED WHEN THICKNESS IS > 2¼″)
14. PROPOSED HMA BASE COURSE, 7″ (USED WHEN WIDENING GREATER OR EQUAL TO 6′)
15. PROPOSED HMA SURF. REMOVAL, 2-1/2″
16. PROPOSED POLY. HMA SURF. CSE MIX ″E″, N70, 1-3/4″
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18. PROPOSED HMA BASE COURSE WIDENING, 7″ (USED WHEN WIDENING LESS THAN 6′)
19. PROPOSED HMA SHOULDER, 5½″
20. PROPOSED HMA SURF CSE MIX ″D″, N50 (IL-9.5mm), 3″
21. PROPOSED HMA SURF. CSE MIX ″D″, N50, 2″ (PETITE LAKE ROAD)
22. PROPOSED HMA BINDER COURSE, 7.5″ (PETITE LAKE ROAD)
23. PROPOSED HMA SURF CSE MIX ″D″, N50 (IL-9.5mm), 3″
24. PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12″
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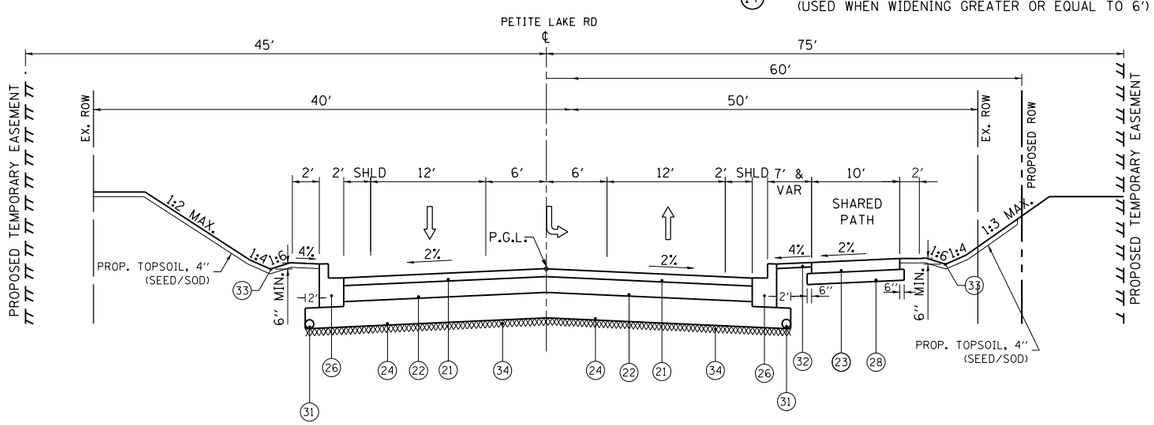
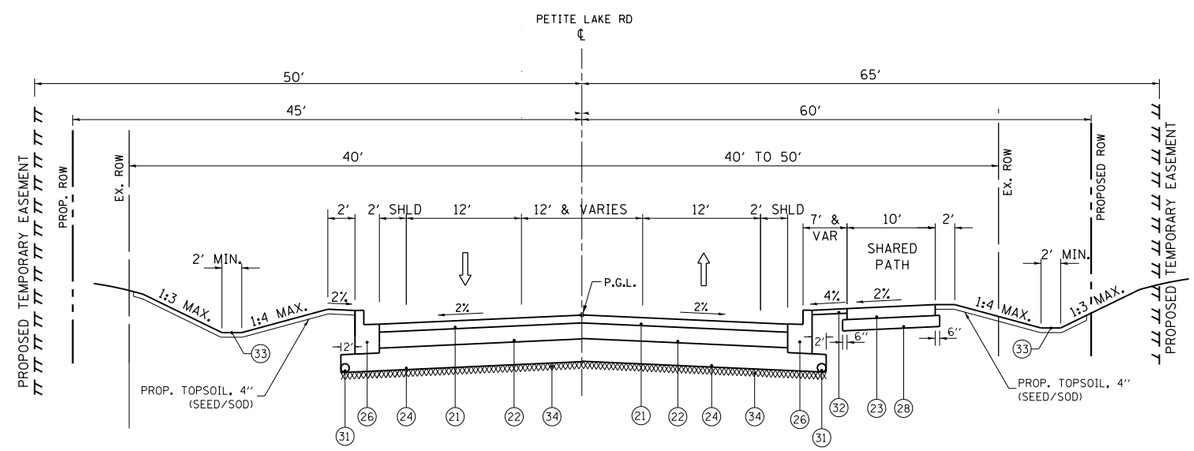
28. PROPOSED AGG. BASE COURSE TYPE B, 6″ (SHARED PATH)
29. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
30. PROPOSED GRADING AND SHAPING SHOULDERS
31. PROPOSED PIPE UNDERDRAIN, 4″
32. PROPOSED LANDSCAPED PARKWAY
33. PROPOSED SWALE/DITCH
34. PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION



PETITE LAKE RD
PROPOSED TYPICAL SECTION
STA. 200+93.5 TO 204+47
(0+93.5 TO 4+47, LCDOT)



PETITE LAKE RD
PROPOSED TYPICAL SECTION
STA. 204+47 TO 208+67
(4+47 TO 8+67, LCDOT)

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING & PROPOSED TYPICAL SECTIONS
IL ROUTE 59 AT PETITE LAKE RD.

SCALE: NONE | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	18

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT

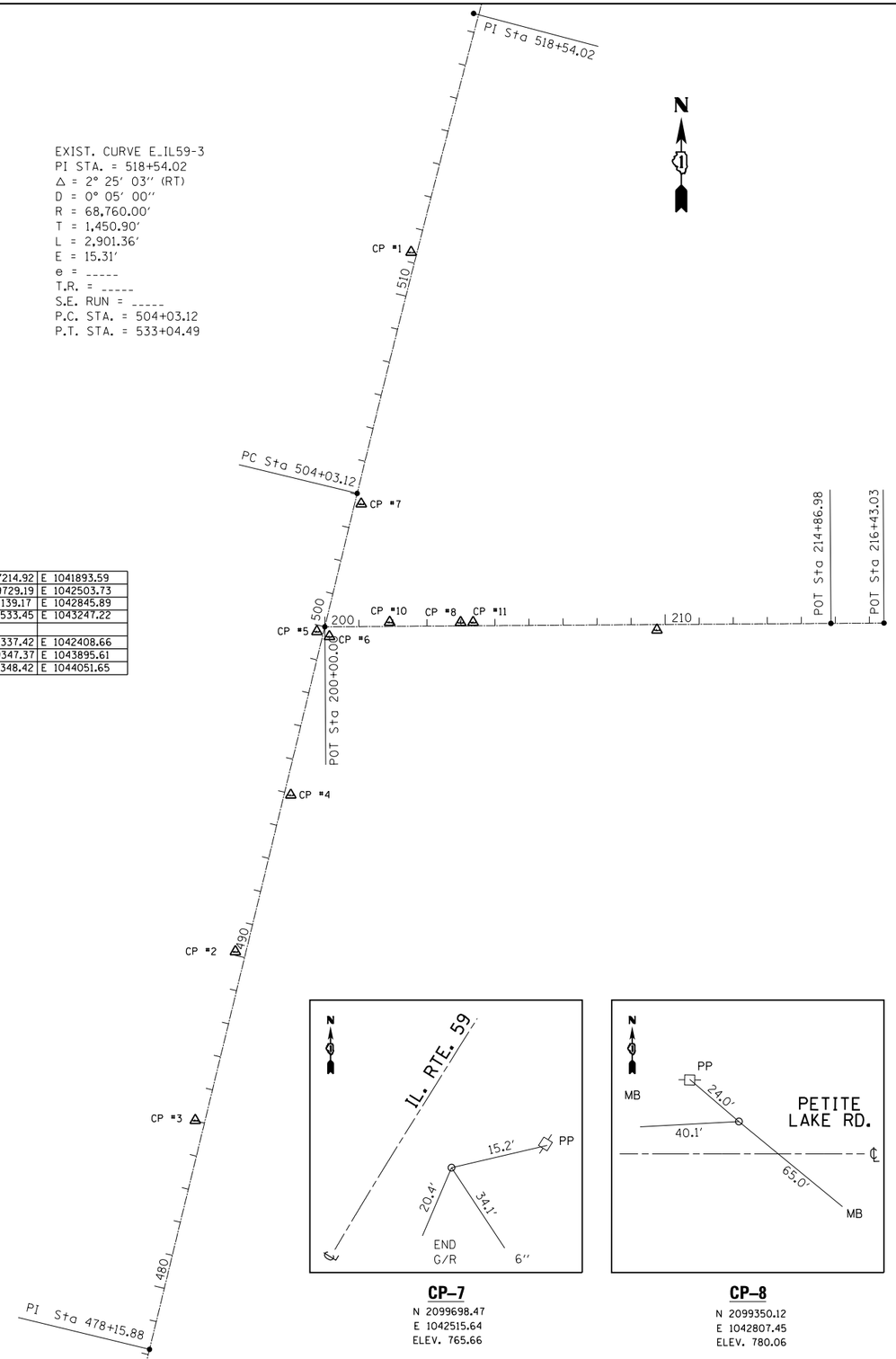
BENCHMARKS

BM 1 - SPIKE/SCREW FOUND IN PP EAST SIDE OF RTE. 59 @ RES #38923, ELEV. 777.50

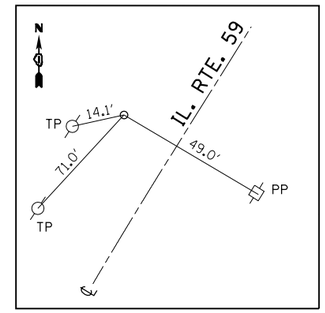
BM 2 - SPIKE & YELLOW TOP IN PP EAST SIDE RTE. 59 ACROSS FROM RES # 39122, ELEV. 774.30

BM 3 - SPIKE & YELLOW TOP IN PP NORTH SIDE PETITE LAKE RD. @ ENT. TO X-MAS TREE FARM HOUSE, ELEV. 778.61

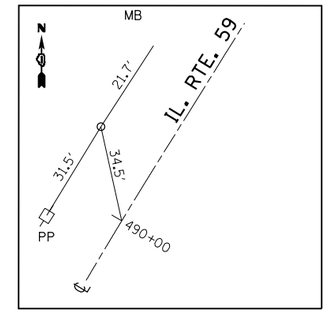
EXIST. CURVE E_IL59-3
PI STA. = 518+54.02
Δ = 2° 25′ 03″ (RT)
D = 0° 05′ 00″
R = 68,760.00′
T = 1,450.90′
L = 2,901.36′
E = 15.31′
e = -----
T.R. = -----
S.E. RUN = -----
P.C. STA. = 504+03.12
P.T. STA. = 533+04.49



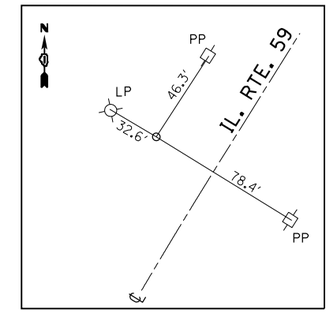
IL. 59	PI	STA. 478+15.88	N 2097214.92	E 1041893.59
	PC CURVE 1	STA. 504+03.12	N 2099729.19	E 1042503.73
	PI CURVE 1	STA. 518+54.02	N 2101139.17	E 1042845.89
	PT CURVE 1	STA. 533+04.49	N 2102533.45	E 1043247.22
PETITE LAKE RD.	PI	STA. 200+00	N 2099337.42	E 1042408.66
	PI	STA. 214+86.98	N 2099347.37	E 1043895.61
	PI	STA. 216+43.03	N 2099348.42	E 1044051.65



CP–1
N 2100437.82
E 1042662.36
ELEV. 788.76



CP–2
N 2098383.52
E 1042145.65
ELEV. 773.48



CP–3
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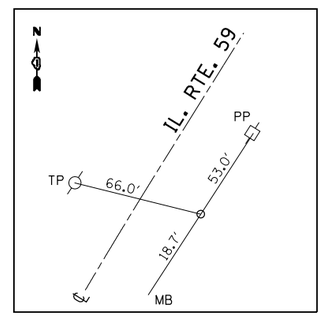
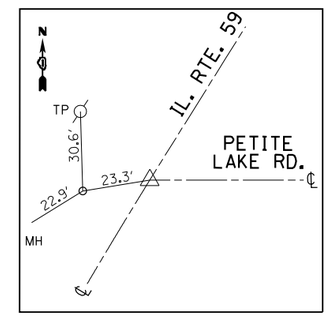
E 1042027.87
ELEV. 777.00



CP–4
N 2098843.22
E 1042308.48
ELEV. 774.63



CP–5
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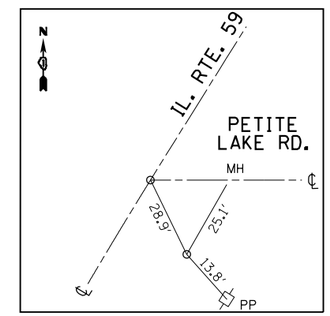
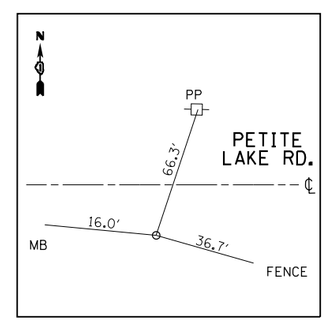
E 1042385.55
ELEV. 771.68



CP–6
N 2099308.83
E 1042422.28
ELEV. 771.93

CP–7
N 2099698.47
E 1042515.64
ELEV. 765.66

CP–8
N 2099350.12
E 1042807.45
ELEV. 780.06



CP–9
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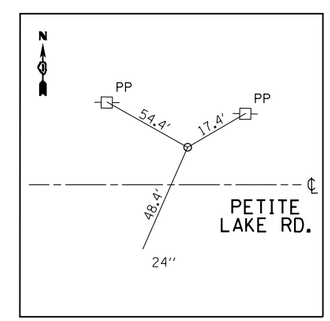
E 1043384.69
ELEV. 780.59



CP–10
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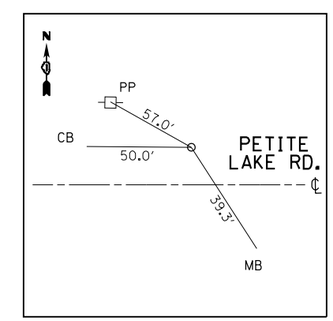
E 1042599.06
ELEV. 778.27



CP–11
N 2099350.50
E 1042844.40
ELEV. 779.52

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		DRAWN -	REVISED -
	PLOT SCALE = 400.0000 ′ / in.	CHECKED -	REVISED -
Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT AND BENCHMARKS
IL. ROUTE 59 (FOX LAKE RD.) AT PETITE LAKE ROAD

SCALE: 1″=200′ | SHEET OF SHEETS | STA. TO STA.

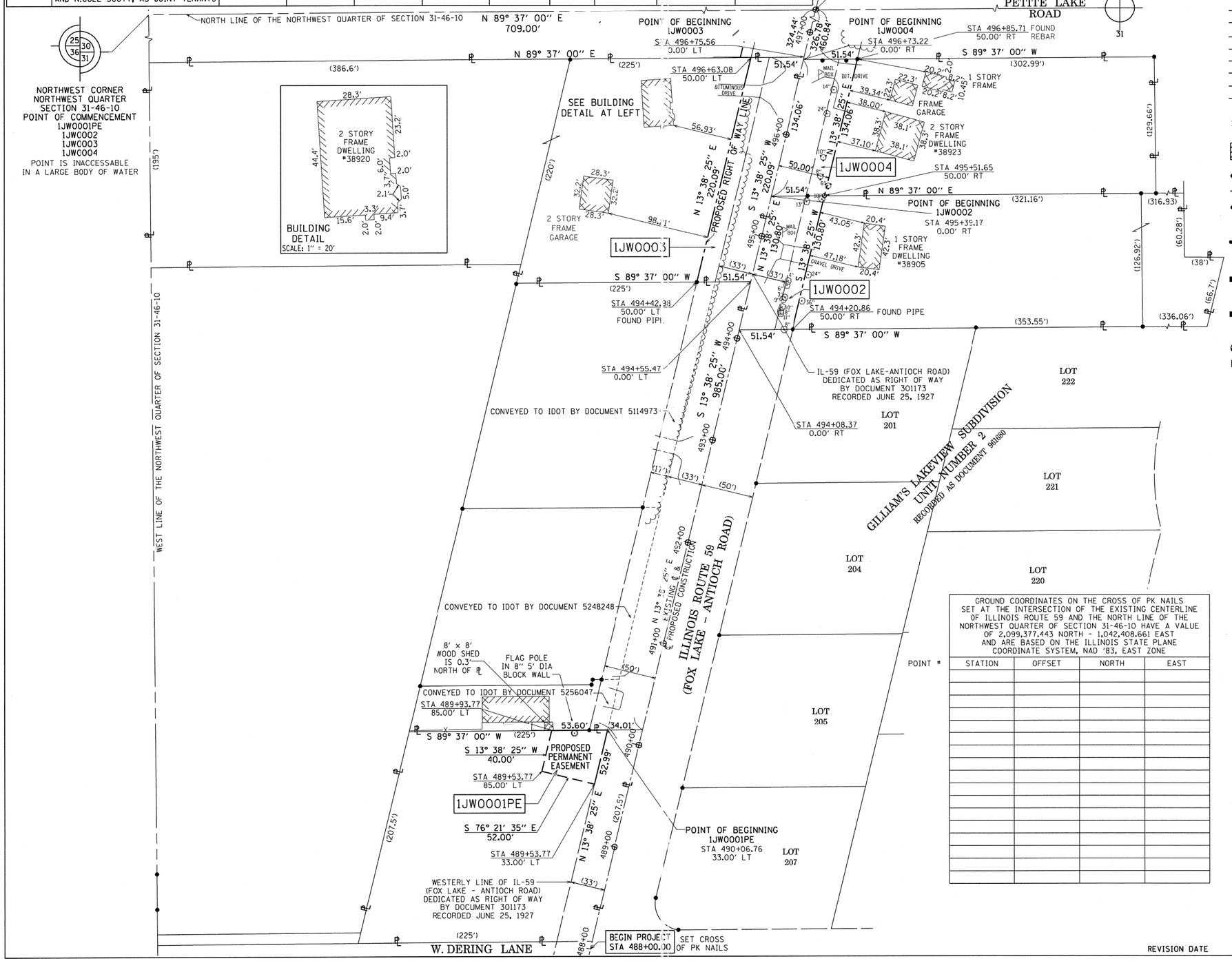
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	19

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT

PART OF THE NORTHWEST QUARTER OF SECTION 31, TOWNSHIP 46 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT AREA SQUARE FEET	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JW0001PE	LAKE VILLA COMMUNITY CONSOLIDATED SCHOOL DISTRICT NUMBER 41	1.072		0.157	1.072	0.056	2418	DRAINAGE	02-31-100-012	
1JW0002	NANCY R. BOLIN, A SINGLE PERSON	1.950	0.150	0.099	1.800				02-31-100-067 02-31-100-068	
1JW0003	BRUCE E. ROSEMANN AND EMILY A. ROSEMANN, HIS WIFE, IN JOINT TENANCY	2.810	0.253	0.167	2.557				02-31-100-009	
1JW0004	GLENDA M. SCOTT, ANDREW A. SCOTT AND NICOLE SCOTT, AS JOINT TENANTS	1.055	0.154	0.102	0.901				02-31-100-032	



GROUND COORDINATES ON THE CROSS OF PK NAILS SET AT THE INTERSECTION OF THE EXISTING CENTERLINE OF ILLINOIS ROUTE 59 AND THE NORTH LINE OF THE NORTHWEST QUARTER OF SECTION 31-46-10 HAVE A VALUE OF 2,099,377.443 NORTH - 1,042,408.661 EAST AND ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, NAD '83, EAST ZONE

POINT #	STATION	OFFSET	NORTH	EAST

LEGEND

SECTION CORNER (9 10 16 15)

QUARTER SECTION CORNER (16 15)

- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINES
- PROPERTY (DEED) LINE
- APL APPARENT PROPERTY LINE
- CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- 129.32' MEASURED DIMENSION
- 129.32' (COMP) COMPUTED DIMENSION
- (129.32') RECORDED DIMENSION
- EXISTING BUILDING

N

GRAPHIC SCALE FEET 0 50 SCALE: 1" = 50'

BEARINGS AND COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, NAD83, EAST ZONE, AND THE EXISTING CENTERLINE OF ILLINOIS ROUTE 59 BEARS N 13° 38' 25" E

- ● IRON PIPE OR ROD FOUND
- + CUT CROSS FOUND OR SET
- ⊕ PK OR "MAG" NAIL SET
- ○ REBAR SET
- ● TI T2 T3 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ● BT1 BT2 BT3 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- ■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS).
- □ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)
)SS
COUNTY OF COOK)

THIS IS TO CERTIFY THAT I, WILLIAM J. FLEMING, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 31, TOWNSHIP 46 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT CHICAGO, ILLINOIS THIS 28TH DAY OF JANUARY, A.D. 2013.

STATE OF ILLINOIS WILLIAM J. FLEMING 35-3226 PROFESSIONAL LAND SURVEYOR

ILLINOIS PROFESSIONAL LAND SURVEYOR 35-3226
LICENSE EXPIRATION DATE: 11/30/2014

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS OF PRACTICE FOR A BOUNDARY SURVEY.

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Illinois Professional Design Corporation 184-000938

RECEIVED
JAN 28 2013
PLATS & LEGALS

PTB 161/017
WORK ORDER 1
GRAEF PROJECT 20123007.01

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
ILLINOIS ROUTE 59

SECTION: AT PETITE LAKE ROAD COUNTY: LAKE
PROJECT JOB NO.: R-91-025-12
STATION 488+00.00 TO STATION 497+00.00
SCALE: 1" = 50' SHEET 2 OF 6

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

REVISION DATE REVISION MADE BY

ROUTE ILLINOIS ROUTE 59 SECTION AT PETITE LAKE ROAD COUNTY LAKE JOB NO. R-91-025-12 REGISTERED ON

PART OF THE SOUTHWEST QUARTER OF SECTION 30 AND NORTHWEST QUARTER OF SECTION 31, TOWNSHIP 46 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA		EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
						ACRES	SQUARE FEET			
1JW0005TE	HANS G. LUEDTKE, AS TRUSTEE UNDER A TRUST AGREEMENT DATED THE 23RD DAY OF MAY, 2008, AND DESIGNATED AS TRUST NO. 38956	2.240				0.030	1318	GRADING	02-31-101-002	
1JW0007 1JW0007TE	ULRICH KUEHN AND FRIEDEL KUEHN, HIS WIFE, IN JOINT TENANCY	2.615	0.011		2.604	0.034	1470	GRADING	02-31-101-001	
1JW0008	SHARON F. DAVIS, SHARON DAVIS AND FERN Y. WALASIAK AS JOINT TENANTS; AND MARTIN W. DAVIS; ALL AS TO AN UNDETERMINED PERCENTAGE INTEREST	1.505	0.138	0.076	1.367				02-30-300-016	
1JW0010	JUDY A. MARTIN AKA JUDITH A. MARTIN	0.540	0.034		0.506				02-30-309-014 02-30-309-015 02-30-309-016	
1JW0016	SHARON F. DAVIS, SHARON DAVIS AND FERN Y. WALASIAK AS JOINT TENANTS; AND MARTIN W. DAVIS; ALL AS TO AN UNDETERMINED PERCENTAGE INTEREST	3.849	0.284	0.154	3.565				02-30-300-025	

ILLINOIS ROUTE 59
EXISTING ℄ &
℄ PROPOSED
CONSTRUCTION

Δ= 02° 25' 03"
R = 68,760.00'
L= 2901.37'
T= 1450.90'
E= 15.31'

PC STA 504+03.10
N - 2,099,729.170
E - 1,042,503.722

PI STA 518+54.00
N - 2,101,139.150
E - 1,042,845.882

PT STA 533+04.47
N - 2,102,533.443
E - 1,043,247.214

LEGEND

SECTION CORNER (9 10 16 15)

QUARTER SECTION CORNER (16 15)

- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINES
- PROPERTY (DEED) LINE
- APL — APPARENT PROPERTY LINE
- CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- 129.32' — MEASURED DIMENSION
- 129.32' (COMP) — COMPUTED DIMENSION
- (129.32') — RECORDED DIMENSION
- EXISTING BUILDING

N

GRAPHIC SCALE
FEET
0 50
SCALE: 1" = 50'

BEARINGS AND COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, NAD83, EAST ZONE, AND THE EXISTING CENTERLINE OF ILLINOIS ROUTE 59 BEARS N 13° 38' 25" E

- ● IRON PIPE OR ROD FOUND
- + CUT CROSS FOUND OR SET
- ⊕ PK OR "MAG" NAIL SET
- ○ 5/8" REBAR SET
- ● TI, T2, T3 — THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ● BTI, BT2, BT3 — THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- ■ M — STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS).
- □ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)
)SS
COUNTY OF COOK)

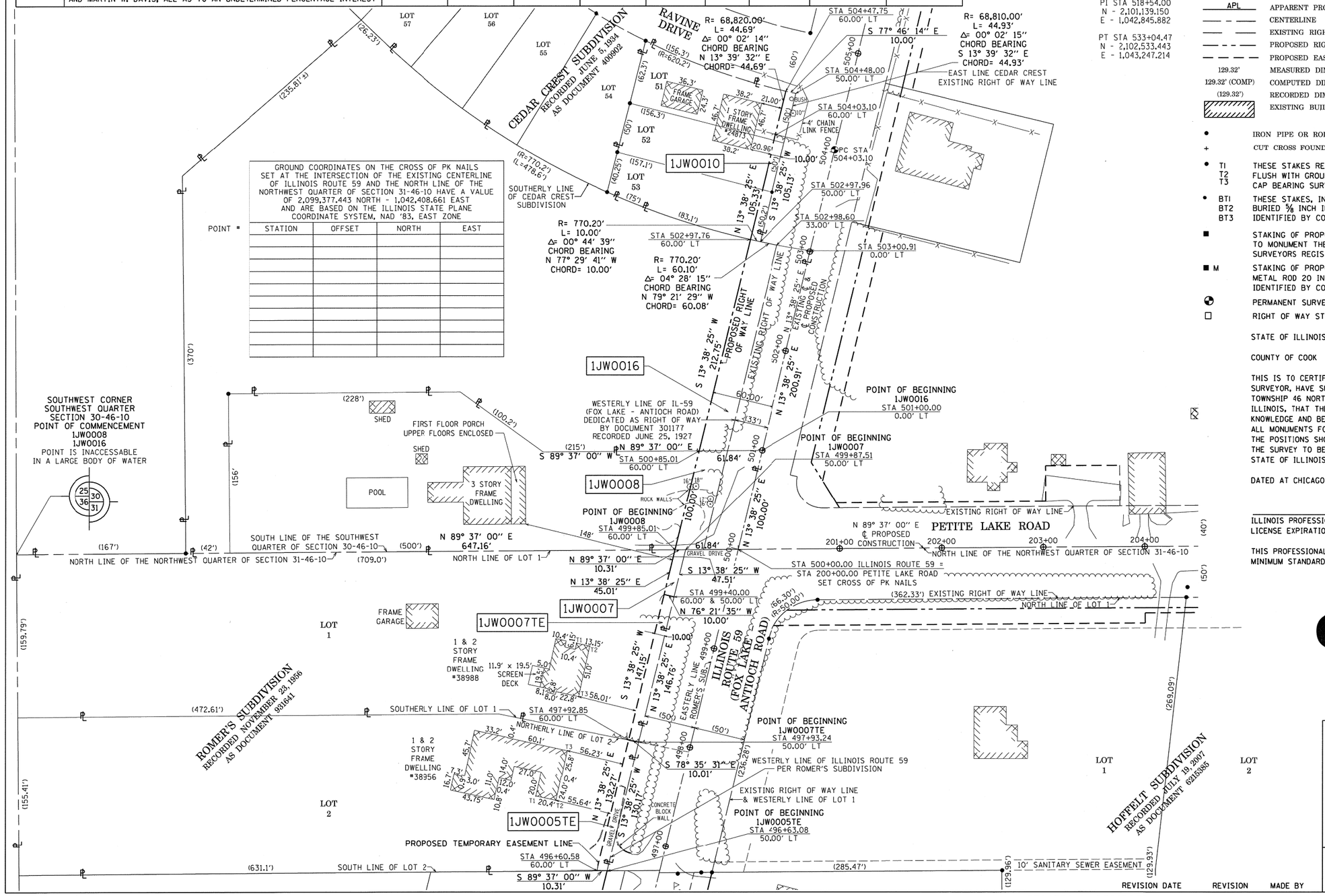
THIS IS TO CERTIFY THAT I, WILLIAM J. FLEMING, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTIONS 30 & 31, TOWNSHIP 46 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT CHICAGO, ILLINOIS THIS 21ST DAY OF MAY, A.D. 2013.

ILLINOIS PROFESSIONAL LAND SURVEYOR 35-3226
LICENSE EXPIRATION DATE: 11/30/2012

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS OF PRACTICE FOR A BOUNDARY SURVEY.

STATE OF ILLINOIS
WILLIAM J. FLEMING
35-3226
PROFESSIONAL LAND SURVEYOR



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Illinois Professional Design
Corporation 184-000938

PTB 161/017
WORK ORDER 1
GRAEF PROJECT
20123007.01

PLAT OF HIGHWAYS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILLINOIS ROUTE 59

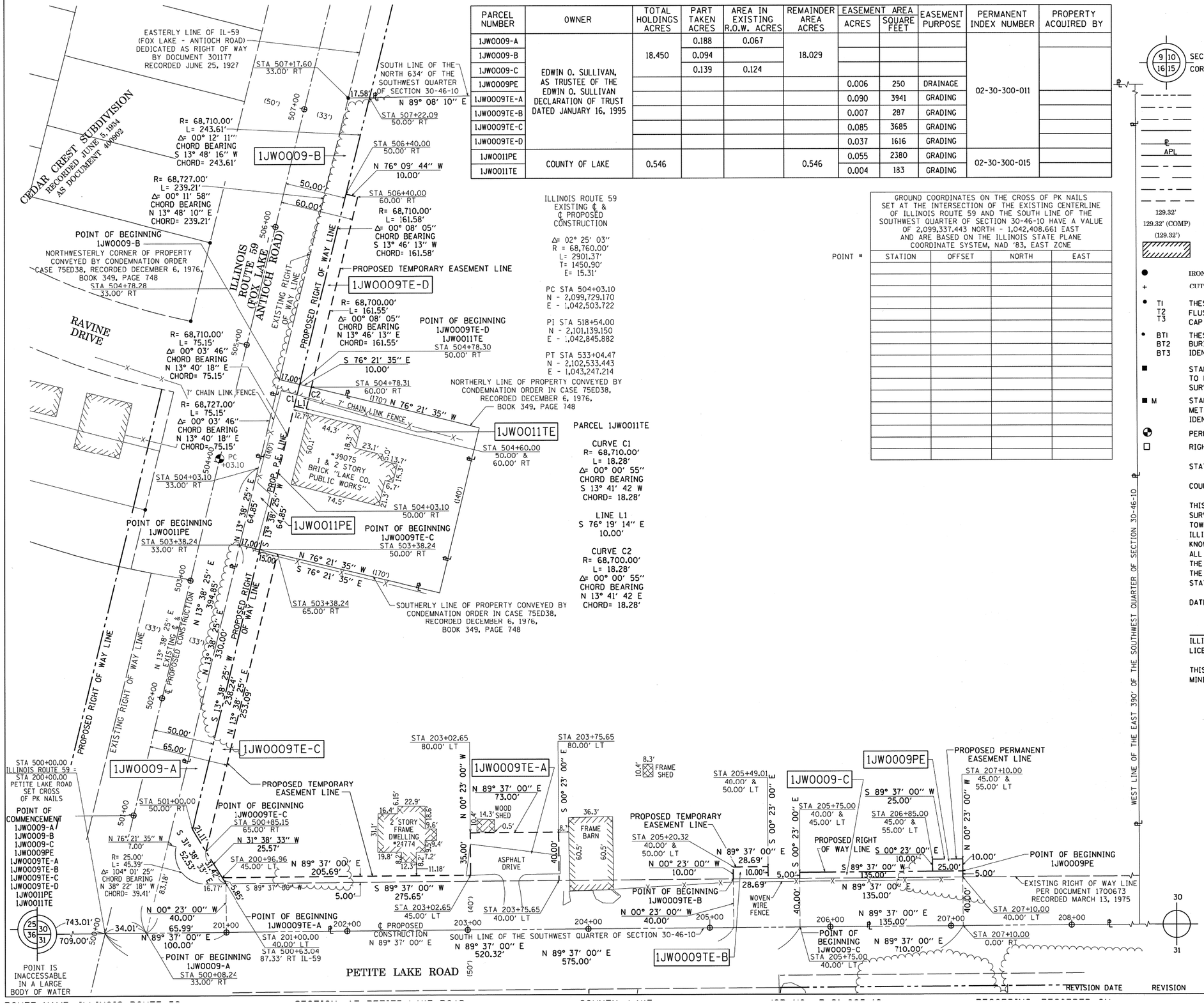
SECTION: AT PETITE LAKE ROAD COUNTY: LAKE
PROJECT JOB NO.: R-91-025-12
STATION 497+00.00 TO STATION 505+00.00
SCALE: 1" = 50' SHEET 3 OF 6

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

RECEIVED
MAY 21 2013
PLATS & LEGALS

REVISION DATE | REVISION | MADE BY

PART OF THE SOUTHWEST QUARTER OF SECTION 30, TOWNSHIP 46 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.



PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT AREA SQUARE FEET	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JW0009-A	EDWIN O. SULLIVAN, AS TRUSTEE OF THE EDWIN O. SULLIVAN DECLARATION OF TRUST DATED JANUARY 16, 1995	18.450	0.188	0.067	18.029				02-30-300-011	
1JW0009-B			0.094							
1JW0009-C			0.139	0.124						
1JW0009PE						0.006	250	DRAINAGE		
1JW0009TE-A						0.090	3941	GRADING		
1JW0009TE-B						0.007	287	GRADING		
1JW0009TE-C						0.085	3685	GRADING		
1JW0009TE-D						0.037	1616	GRADING		
1JW0011PE	COUNTY OF LAKE	0.546			0.546	0.055	2380	GRADING	02-30-300-015	
1JW0011TE						0.004	183	GRADING		

ILLINOIS ROUTE 59 EXISTING ℄ & ℄ PROPOSED CONSTRUCTION

Δ= 02° 25' 03"
R = 68,760.00'
L= 2901.37'
T= 1450.90'
E= 15.31'

PC STA 504+03.10
N - 2,099,729.170
E - 1,042,503.722

PI STA 518+54.00
N - 2,101,139.150
E - 1,042,845.882

PT STA 533+04.47
N - 2,102,533.443
E - 1,043,247.214

GROUND COORDINATES ON THE CROSS OF PK NAILS SET AT THE INTERSECTION OF THE EXISTING CENTERLINE OF ILLINOIS ROUTE 59 AND THE SOUTH LINE OF THE SOUTHWEST QUARTER OF SECTION 30-46-10 HAVE A VALUE OF 2,099,337.443 NORTH - 1,042,408.661 EAST AND ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, NAD '83, EAST ZONE

POINT #	STATION	OFFSET	NORTH	EAST

LEGEND

SECTION CORNER (9 10 16 15)

QUARTER SECTION CORNER (16 15)

- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINES
- PROPERTY (DEED) LINE
- APL — APPARENT PROPERTY LINE
- CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- 129.32' — MEASURED DIMENSION
- 129.32' (COMP) — COMPUTED DIMENSION
- (129.32') — RECORDED DIMENSION
- EXISTING BUILDING

N

GRAPHIC SCALE FEET 0 40
SCALE: 1" = 40'

BEARINGS AND COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, NAD83, EAST ZONE, AND THE EXISTING CENTERLINE OF ILLINOIS ROUTE 59 BEARS N 13° 38' 25" E

- ● IRON PIPE OR ROD FOUND
- ⊕ PK OR "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- ○ 5/8" REBAR SET
- • T1 T2 T3 — THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- • BT1 BT2 BT3 — THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
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STATE OF ILLINOIS)
)SS
COUNTY OF COOK)

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DATED AT CHICAGO, ILLINOIS THIS 21ST DAY OF MAY, A.D. 2013.

ILLINOIS PROFESSIONAL LAND SURVEYOR 35-3226
LICENSE EXPIRATION DATE: 11/30/2014

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STATE OF ILLINOIS WILLIAM J. FLEMING 35-3226 PROFESSIONAL LAND SURVEYOR

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Illinois Professional Design Corporation 184-000938

PTB 161/017
WORK ORDER 1
GRAEF PROJECT 20123007.01

PLAT OF HIGHWAYS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

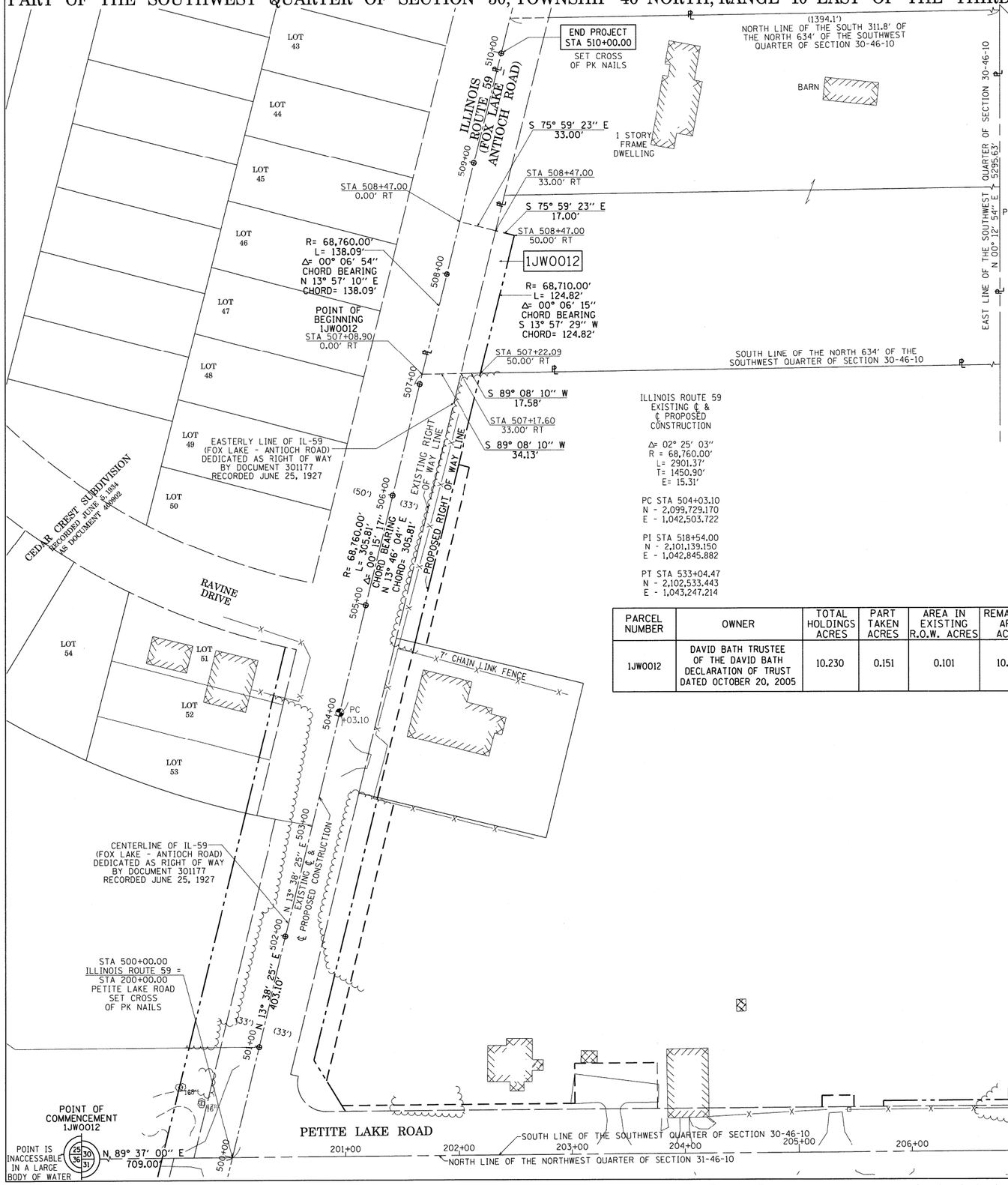
ILLINOIS ROUTE 59

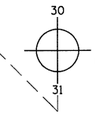
SECTION: AT PETITE LAKE — COUNTY: LAKE
PROJECT — JOB NO.: R-91-025-12
STATION 500+00.00 TO STATION 507+00.00
SCALE: 1" = 40' — SHEET 4 OF 6

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

RECEIVED
MAY 21 2013
PLATS & LEGALS

PART OF THE SOUTHWEST QUARTER OF SECTION 30, TOWNSHIP 46 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.





FOUND ½" IRON ROD
PER MONUMENT RECORD
RECORDED 10/08/1997 AS
DOCUMENT 4030811

GROUND COORDINATES ON THE CROSS OF PK NAILS SET AT THE INTERSECTION OF THE EXISTING CENTERLINE OF ILLINOIS ROUTE 59 AND THE NORTH LINE OF THE NORTHWEST QUARTER OF SECTION 31-46-10 HAVE A VALUE OF 2,099,377.443 NORTH - 1,042,408.661 EAST AND ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, NAD '83, EAST ZONE

STATION	OFFSET	NORTH	EAST

LEGEND

- SECTION CORNER (9 10 / 16 15)
- QUARTER SECTION CORNER (16 – 15)
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINES
- ℘ PROPERTY (DEED) LINE
- APL APPARENT PROPERTY LINE
- CENTERLINE
- EXISTING RIGHT OF WAY LINE
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- PROPOSED EASEMENT
- 129.32' MEASURED DIMENSION
- 129.32' (COMP) COMPUTED DIMENSION
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- EXISTING BUILDING

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GRAPHIC SCALE
FEET
0 50
SCALE: 1" = 50'

BEARINGS AND COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, NAD83, EAST ZONE, AND THE EXISTING CENTERLINE OF ILLINOIS ROUTE 59 BEARS N 13° 38' 25" E

- ● IRON PIPE OR ROD FOUND
- ⊕ PK OR "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- ○ 5/8" REBAR SET
- ● TI, T2, T3 — THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- • BT1, BT2, BT3 — THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- ■ M — STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ◐ PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS).
- □ RIGHT OF WAY STAKING PROPOSED TO BE SET.

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT AREA SQUARE FEET	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JW0012	DAVID BATH TRUSTEE OF THE DAVID BATH DECLARATION OF TRUST DATED OCTOBER 20, 2005	10.230	0.151	0.101	10.079				02-30-300-020	

STATE OF ILLINOIS)
)SS
COUNTY OF COOK)

THIS IS TO CERTIFY THAT I, WILLIAM J. FLEMING, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 30, TOWNSHIP 46 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT CHICAGO, ILLINOIS THIS 13TH DAY OF SEPTEMBER, A.D. 2012.

STATE OF ILLINOIS
WILLIAM J. FLEMING
35-3226
PROFESSIONAL LAND SURVEYOR

ILLINOIS PROFESSIONAL LAND SURVEYOR 35-3226
LICENSE EXPIRATION DATE: 11/30/2012

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS OF PRACTICE FOR A BOUNDARY SURVEY.

GRAEF
8501 West Higgins Road
Suite 280
Chicago, IL 60631-2801
773 / 399 0112
www.graef-usa.com
Illinois Professional Design
Corporation 184-000938

RECEIVED
SEP 13 2012
PLATS & LEGALS

PTB 161/017
WORK ORDER 1
GRAEF PROJECT
20123007.01

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILLINOIS ROUTE 59

SECTION: AT PETITE LAKE ROAD COUNTY: LAKE
PROJECT JOB NO.: R-91-025-12
STATION 506+00.00 TO STATION 510+00.00
SCALE: 1" = 50' SHEET 5 OF 6

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

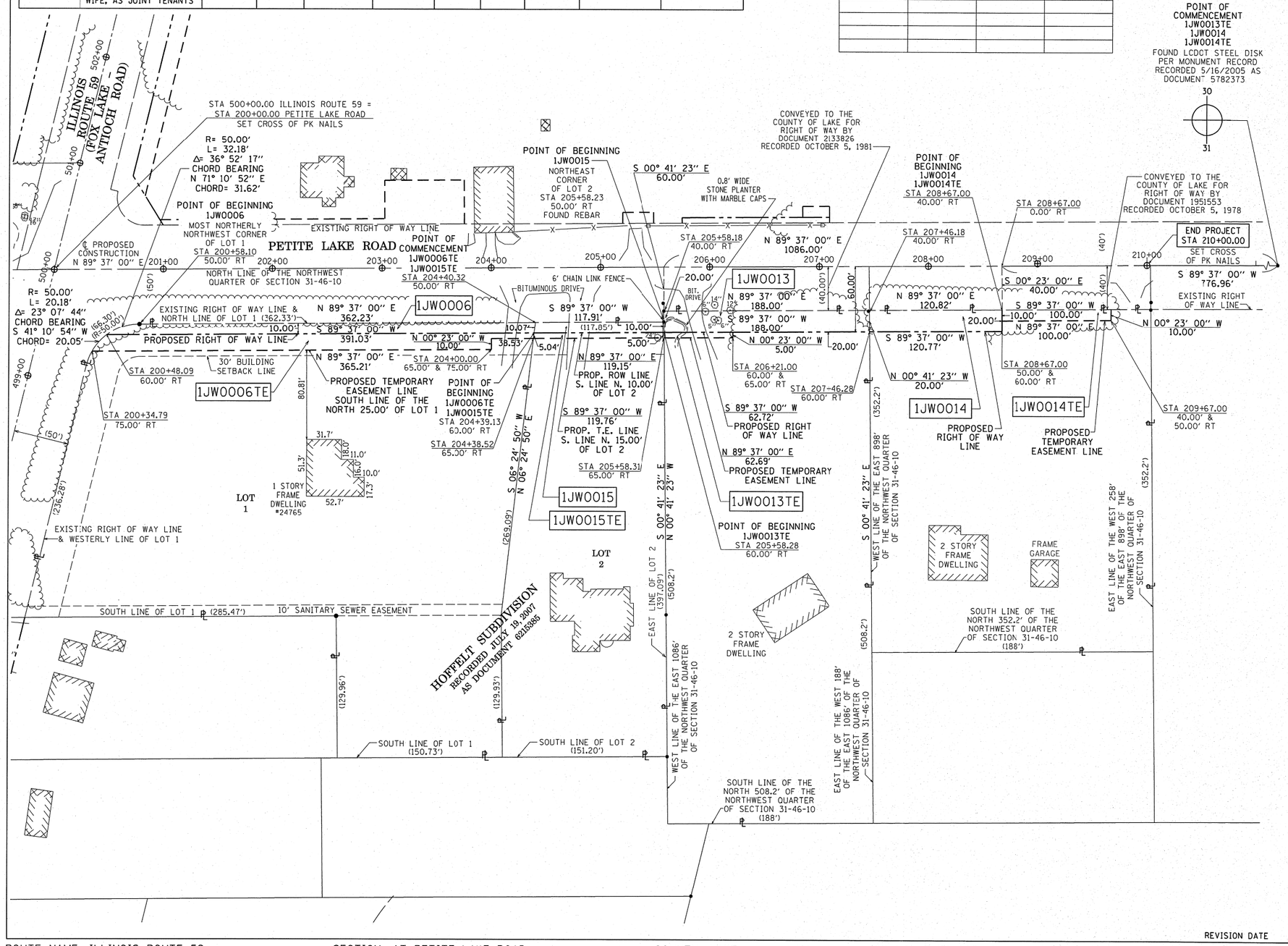
REVISION DATE	REVISION	MADE BY

PART OF THE NORTHWEST QUARTER OF SECTION 31, TOWNSHIP 46 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS.

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT AREA SQUARE FEET	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JW0006 1JW0006TE	JOSEPH E. HOFFELT AND PEGGY A. HOFFELT, HIS WIFE, AS JOINT TENANTS	3.010	0.088		2.922	0.128	5586	GRADING	02-31-107-001	
1JW0013 1JW0013TE	GREGORY BOCKWINKEL	2.021	0.086		1.935	0.007	314	GRADING	02-31-100-035	
1JW0014 1JW0014TE	WILLIAM C. REISS AND ANDREA REISS, AS TENANTS BY THE ENTIRETY	2.086	0.055		2.031	0.023	1000	GRADING	02-31-100-042	
1JW0015 1JW0015TE	JOSEPH E. HOFFELT AND PEGGY A. HOFFELT, HIS WIFE, AS JOINT TENANTS	1.277	0.027		1.250	0.014	597	GRADING	02-31-107-002	

GROUND COORDINATES ON THE CROSS OF PK NAILS SET AT THE INTERSECTION OF THE EXISTING CENTERLINE OF ILLINOIS ROUTE 59 AND THE NORTH LINE OF THE NORTHWEST QUARTER OF SECTION 31-46-10 HAVE A VALUE OF 2,099,377.443 NORTH - 1,042,408.661 EAST AND ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, NAD '83, EAST ZONE

POINT #	STATION	OFFSET	NORTH	EAST



LEGEND

SECTION CORNER (9 10 16 15)

QUARTER SECTION CORNER (16 15)

- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINES
- PROPERTY (DEED) LINE
- APL — APPARENT PROPERTY LINE
- CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- 129.32' — MEASURED DIMENSION
- 129.32' (COMP) — COMPUTED DIMENSION
- (129.32') — RECORDED DIMENSION
- EXISTING BUILDING

N

GRAPHIC SCALE FEET
0 50
SCALE: 1" = 50'

BEARINGS AND COORDINATES ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, NAD83, EAST ZONE, AND THE EXISTING CENTERLINE OF ILLINOIS ROUTE 59 BEARS N 13° 38' 25" E

- ● IRON PIPE OR ROD FOUND
- + CUT CROSS FOUND OR SET
- ⊕ PK OR "MAG" NAIL SET
- ○ 58" REBAR SET
- • T1 T2 T3 — THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- • BT1 BT2 BT3 — THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ■ — STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- ■ M — STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS).
- □ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)
)SS
COUNTY OF COOK)

THIS IS TO CERTIFY THAT I, WILLIAM J. FLEMING, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 31, TOWNSHIP 46 NORTH, RANGE 10 EAST OF THE THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT CHICAGO, ILLINOIS THIS 7TH DAY OF NOVEMBER, A.D. 2013.

STATE OF ILLINOIS — WILLIAM J. FLEMING 35-3226 — PROFESSIONAL LAND SURVEYOR

ILLINOIS PROFESSIONAL LAND SURVEYOR 35-3226
LICENSE EXPIRATION DATE: 11/30/2014

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS OF PRACTICE FOR A BOUNDARY SURVEY.

GRAEF
8501 West Higgins Road
Suite 280
Chicago, IL 60631-2801
773 / 399 0112
www.graef-usa.com
Illinois Professional Design Corporation 184-000938

RECEIVED
GF NOV 07 2013
PLATS & LEGALS

PTB 161/017
WORK ORDER 1
GRAEF PROJECT 20123007.01

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

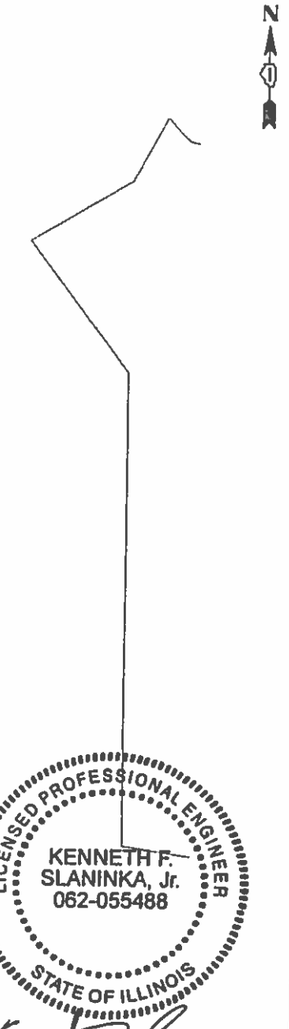
ILLINOIS ROUTE 59

SECTION: AT PETITE LAKE ROAD COUNTY: LAKE
PROJECT JOB NO.: R-91-025-12
STATION 200+00.00 TO STATION 210+00.00
SCALE: 1" = 50' SHEET 6 OF 6

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

REVISION DATE REVISION MADE BY

ROUTE NAME: ILLINOIS ROUTE 59 SECTION: AT PETITE LAKE ROAD COUNTY: LAKE JOB NO.: R-91-025-12 RECORDING: RECORDED ON



LICENSED PROFESSIONAL ENGINEER
KENNETH F. SLANINKA, Jr.
062-055488
STATE OF ILLINOIS

signature

10/29/13
date

license expires 11-30-15

AERIAL
UNKNOWN
OIL
CABLE TV
TELEPHONE
GAS
ELECTRIC
WATER
FORCE MAIN
FIBER OPTIC
END OF INFORMATION
TBE TEST HOLE

UTILITY OWNERS
AT&T = TELEPHONE
COM-ED = ELECTRIC
LAKE COUNTY PUBLIC WORKS = FORCE MAIN
NICOR = GAS

Utilities shown on these plans as depicted in the legend have been investigated by Cardno TBE in accordance with SUE Industry Standards. All other information shown has been provided to Cardno TBE by others. Cardno TBE's QL"B" SUE field investigation was performed 10/4/13 through 10/21/13. Changes to utilities afte 10/21/13 may have been made and therefore may result in variances from this plan. Consideration should be given to updating this plan if deemed advisable prior to final design and construction.

ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.

Cardno TBE

Dynasty Group
Engineers & Surveyors

TBE Job No. IL09510536
SUE Plan Page: Cover

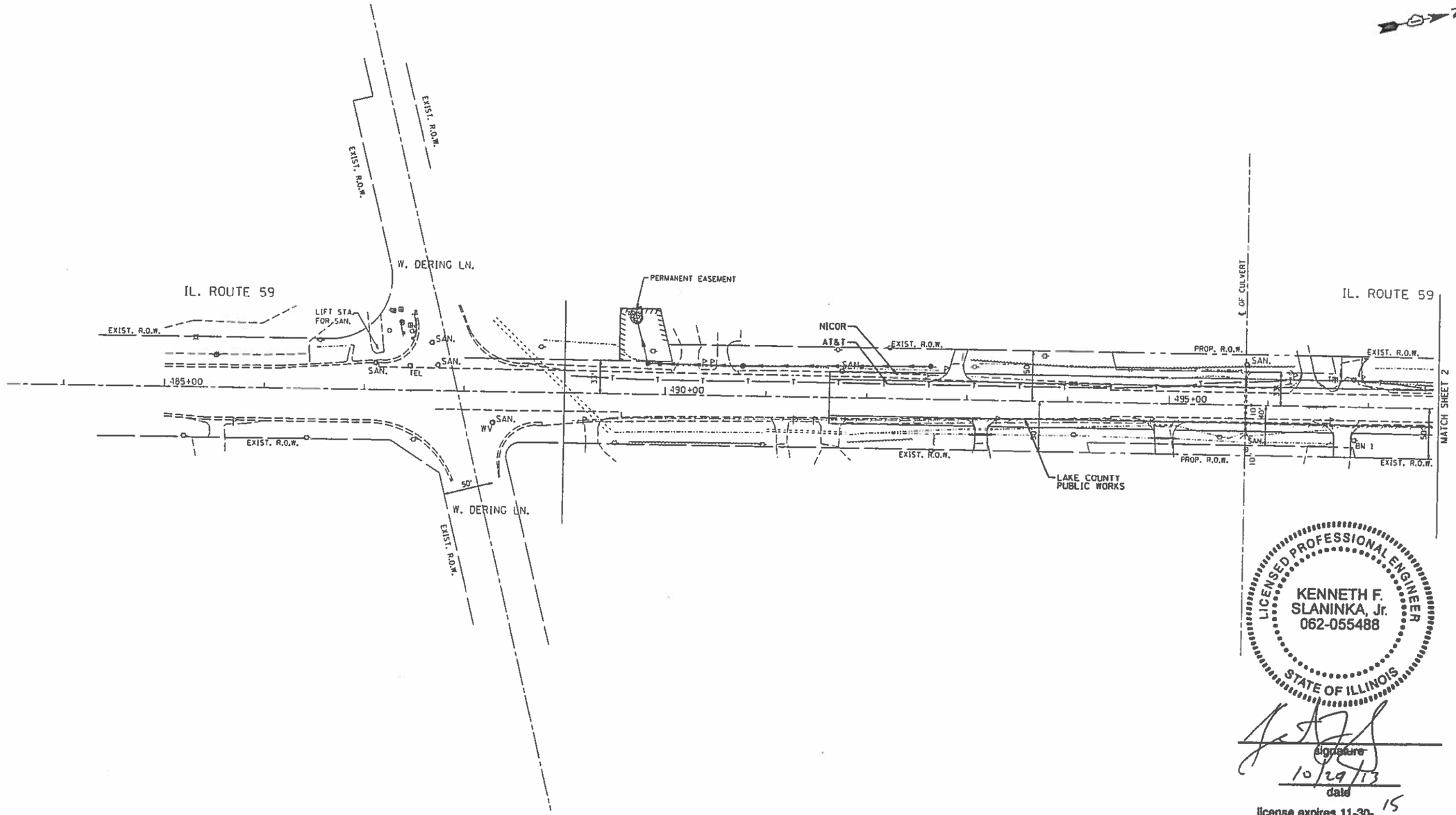
Utility Quality Level "A" : Visually Verified Test Hole
Utility Quality Level "B" : Designating/non Visually Verified Test Hole
Utility Quality Level "C" : Research with Survey
Utility Quality Level "D" : Records Research

DESIGNED	ER	REVISED
DRAWN	SRK	REVISED
CHECKED	KFS	REVISED
DATE	10/28/13	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 59 at Petite Lake Road
Lake Villa, Illinois

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	105-N-4	Lake	113	25
		Contract No. 60W06		
FED. ROAD DIST. NO. _	ILLINOIS	IDOT Project No.		



LICENSED PROFESSIONAL ENGINEER
KENNETH F. SLANINKA, Jr.
062-055488
STATE OF ILLINOIS

signature

10/29/13
date

license expires 11-30-15

A — A	AERIAL
	UNKNOWN
O — O	OIL
CTV — CTV	CABLE TV
T — T	TELEPHONE
	GAS
E — E	ELECTRIC
W	WATER
	FORCE MAIN
FO — FO	FIBER OPTIC
	END OF INFORMATION
	TBE TEST HOLE

UTILITY OWNERS

AT&T = TELEPHONE
COM-ED = ELECTRIC
LAKE COUNTY PUBLIC WORKS = FORCE MAIN
NICOR = GAS

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20 0 METERS 20 40
50 0 FEET 50 100

Cardno TBE

Dynasty Group
Engineers & Surveyors

TBE Job No. IL09510536
SUE Plan Page: 1 of 3

Utility Quality Level "A" : Visually Verified Test Hole
Utility Quality Level "B" : Designating/non Visually Verified Test Hole
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Utility Quality Level "D" : Records Research

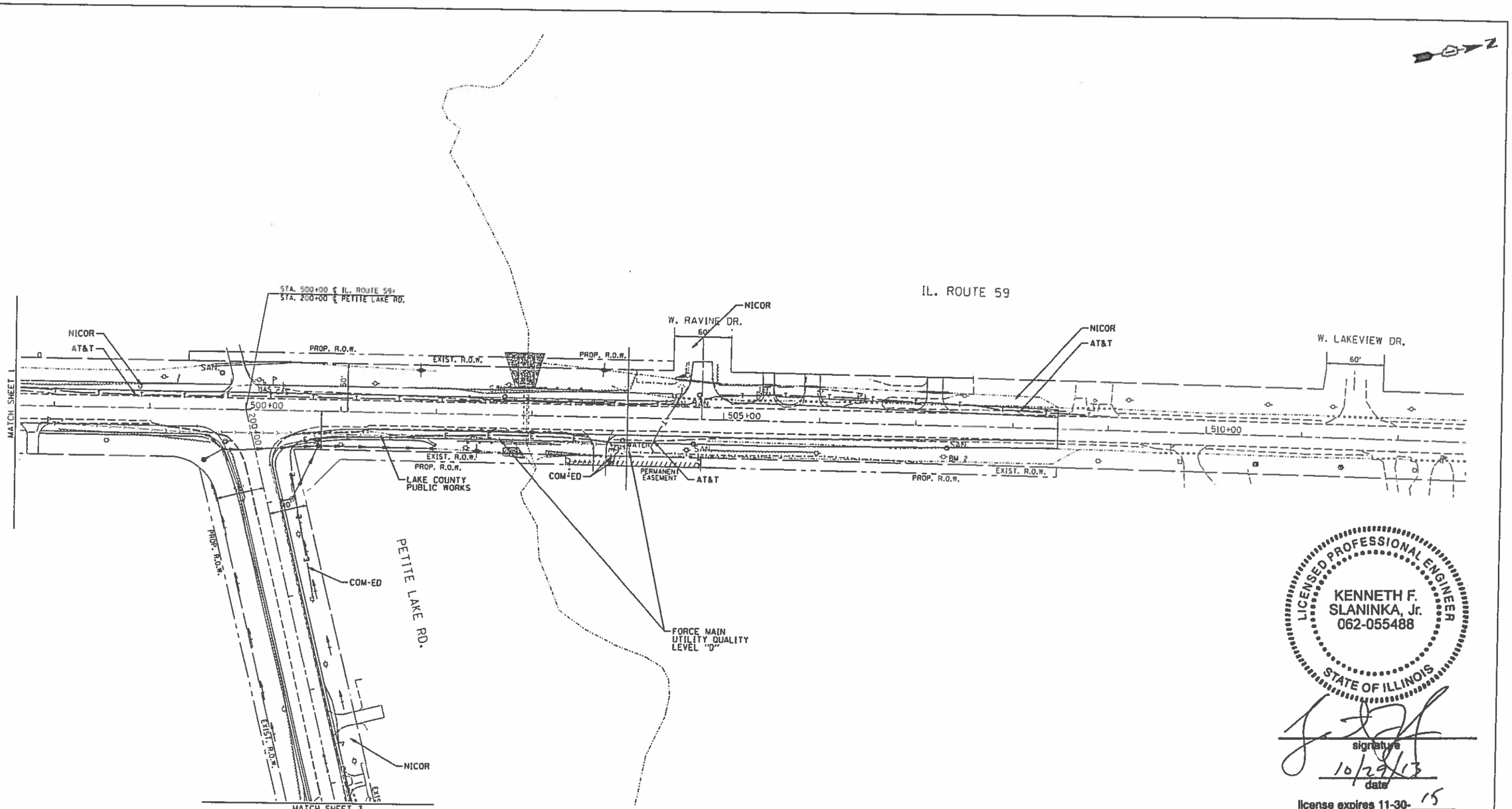
DESIGNED	ER	REVISED
DRAWN	SRK	REVISED
CHECKED	KFS	REVISED
DATE	10/28/13	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 59 at Petite Lake Road
Lake Villa, Illinois

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	105-N-4	Lake	113	26
		Contract No. 60W06		

FED. ROAD DIST. NO. _ ILLINOIS IDOT Project No.



LICENSED PROFESSIONAL ENGINEER
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STATE OF ILLINOIS

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10/29/13
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A	AERIAL
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O	OIL
CTV	CABLE TV
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20 0 METERS 20 40
50 0 FEET 50 100

Cardno TBE

Dynasty Group
Engineers & Surveyors

TBE Job No. IL09510536
SUE Plan Page: 2 of 3

Utility Quality Level "A" : Visually Verified Test Hole
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Utility Quality Level "C" : Research with Survey
Utility Quality Level "D" : Records Research

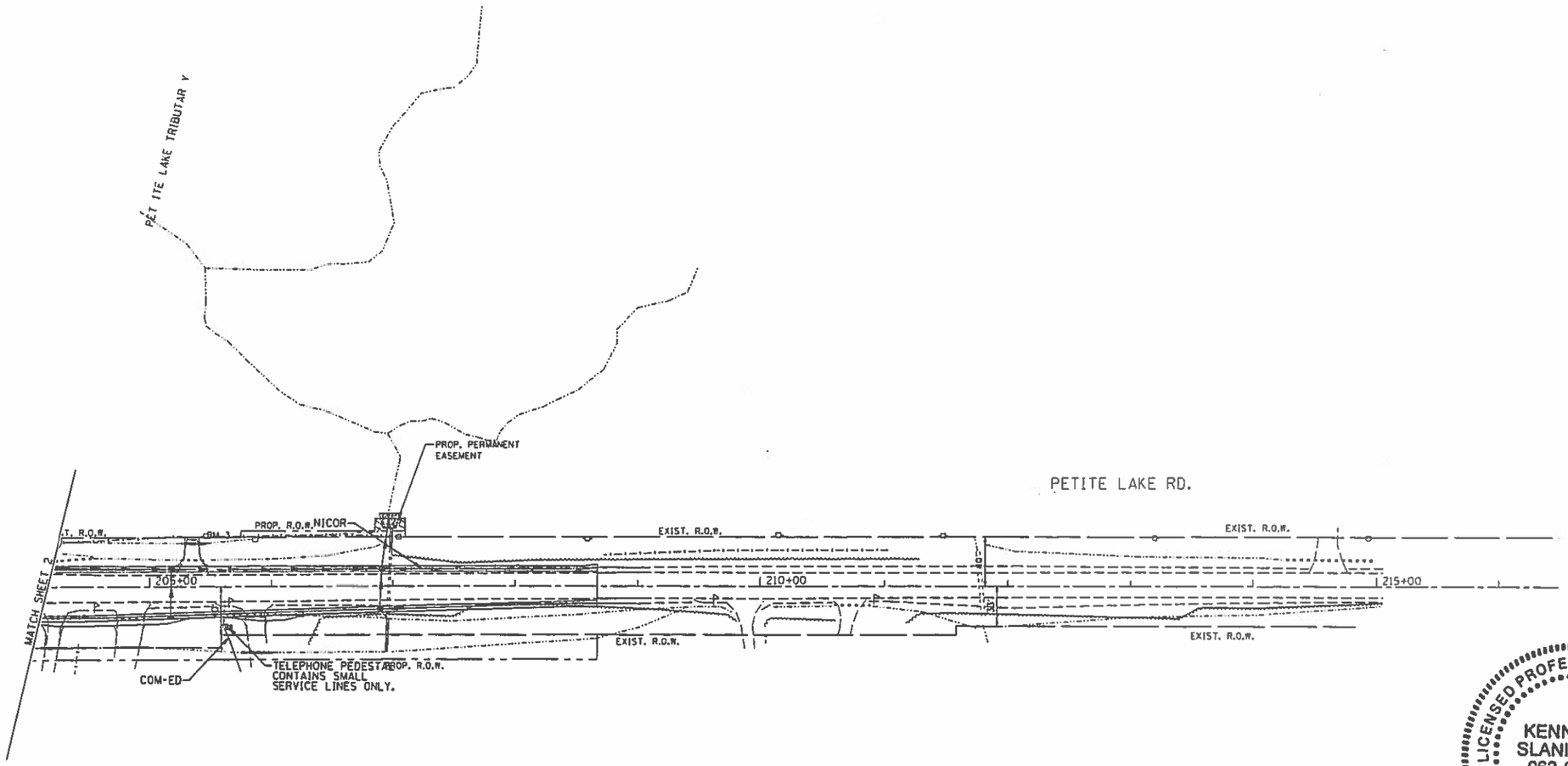
DESIGNED	ER	REVISED
DRAWN	SRK	REVISED
CHECKED	KFS	REVISED
DATE	10/28/13	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 59 at Petite Lake Road
Lake Villa, Illinois

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	105-N-4	LAKE	113	27
		Contract No. 60W06		

FED. ROAD DIST. NO. _ ILLINOIS IDOT Project No.



LICENSED PROFESSIONAL ENGINEER
KENNETH F. SLANINKA, Jr.
062-055488
STATE OF ILLINOIS

signature

10/29/13
date

license expires 11-30-15

A	AERIAL
	UNKNOWN
O	OIL
CTV	CABLE TV
T	TELEPHONE
	GAS
E	ELECTRIC
W	WATER
	FORCE MAIN
FO	FIBER OPTIC
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20 0 METERS 20 40
50 0 FEET 50 100

Cardno TBE

Dynasty Group
Engineers & Surveyors

TBE Job No. JL09510536
SUE Plan Pages 3 of 3

Utility Quality Level "A" : Visually Verified Test Hole
Utility Quality Level "B" : Designating/non Visually Verified Test Hole
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Utility Quality Level "D" : Records Research

DESIGNED	ER	REVISED
DRAWN	SRK	REVISED
CHECKED	KFS	REVISED
DATE	10/28/13	REVISED

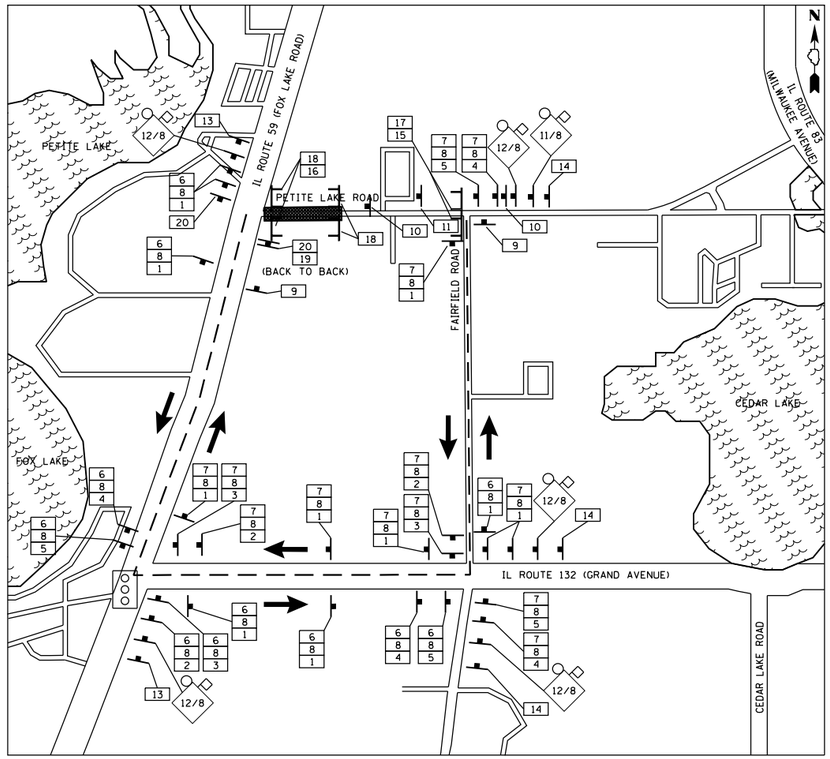
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 59 at Petite Lake Road
Lake Villa, Illinois

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	105-N-4	Lake	113	28

Contract No. 60W06

FED. ROAD DIST. NO. _ ILLINOIS IDOT Project No.



SIGN DESCRIPTION AND QUANTITIES ARE SHOWN ON SHEET $DT3

SPECIAL DETOUR NOTES:

1. THE CONTRACTOR SHALL PAY PARTICULAR ATTENTION TO THE DETOUR GENERAL NOTES. SEE SHEET 31 FOR THE DETOUR GENERAL NOTES.

2. SEE SHEET 32 FOR INFORMATION ON THE DESIGN AND LOCATION OF THE DETOUR INFORMATION SIGNS.

3. A MINIMUM OF FOUR (4) TYPE III BARRICADES WILL BE NEEDED FOR THIS DETOUR AND ROAD CLOSURE. SEE DETOUR GENERAL NOTE 9.

4. PRIOR TO AND AT THE CONCLUSION OF THE DETOUR PLAN, THE TRAFFIC CONTROL PLAN SHOWN ON SHEETS 40 TO 41 SHALL BE IN EFFECT.

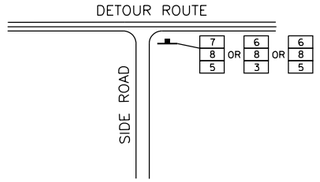
THE DETOUR SHALL NOT START PRIOR TO JUNE 15, 2017 AND SHALL BE REMOVED PRIOR TO AUGUST 15, 2017. THE DURATION OF THE DETOUR SHALL NOT EXCEED 50 CALENDAR DAYS.

THE LIQUIDATED DAMAGES FOR EXCEEDING THE DURATION SHALL BE $2,500.00 PER CALENDAR DAY.

LEGEND

- 48" X 48" CONSTRUCTION WARNING SIGN WITH AMBER FLASHING LIGHT (NUMBER DENOTES TYPE)
- M4-9 SERIES DETOUR SIGN WITH ROAD NAME & DIRECTION PLATES (NUMBER DENOTES TYPE)
- OTHER DETOUR SIGN (NUMBER DENOTES TYPE)
- CONSTRUCTION WORK ZONE
- DETOUR ROUTE DIRECTION
- SIGN POST
- SIGNALIZED INTERSECTION
- TYPE III BARRICADE
- DETOUR ROUTE

TYPICAL INTERSECTION ALONG DETOUR ROUTE



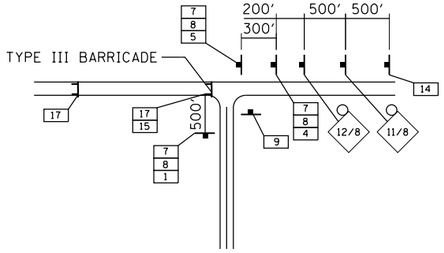
DERING LANE
HALIE COURT
COLUMBIA BAY ROAD
WARREN AVENUE

BUENA AVENUE
SUNSET LANE
ENTRANCE DRIVE
CEDAR LAKE LANE

TYPICAL SIGN ASSEMBLY

M3-2 OR M3-4
SPECIAL
M4-9 OR M4-9R OR M4-9L

TYPICAL INTERSECTION AT POINT OF DETOUR



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		DRAWN -	REVISED -
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 9/13/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETOUR PLAN
IL. ROUTE 59 AT PETITE LAKE RD

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	29

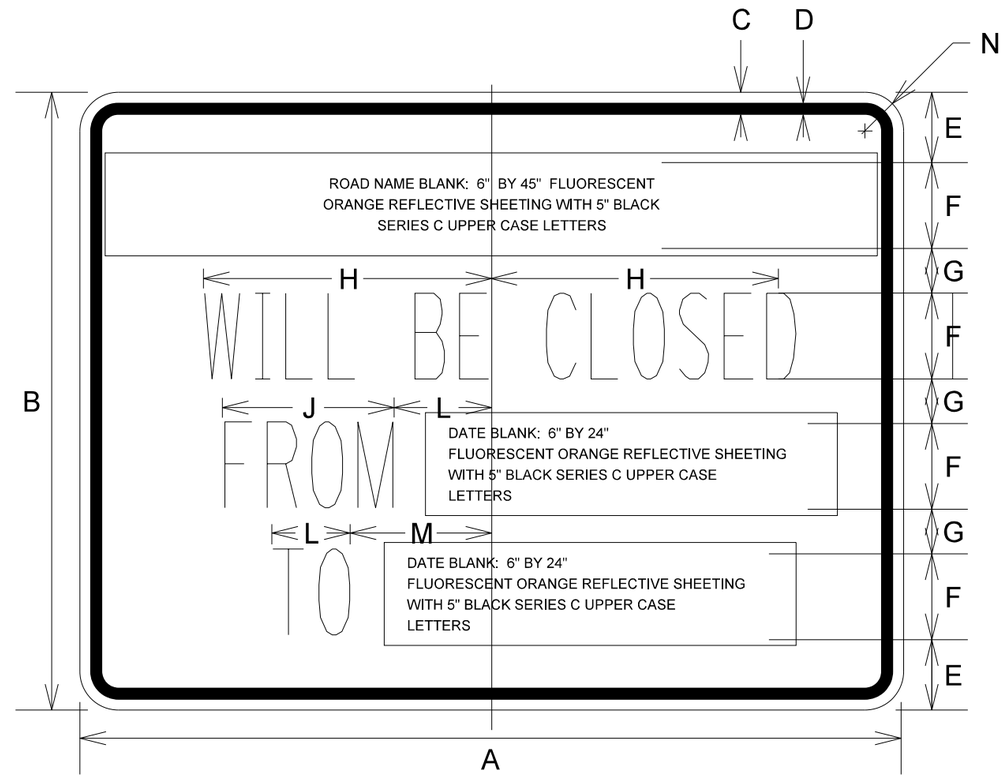
CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT

NUMBER OF SIGNS 7

DATES SHOWN ARE FOR EXAMPLE ONLY

NUMBER OF SIGNS ____



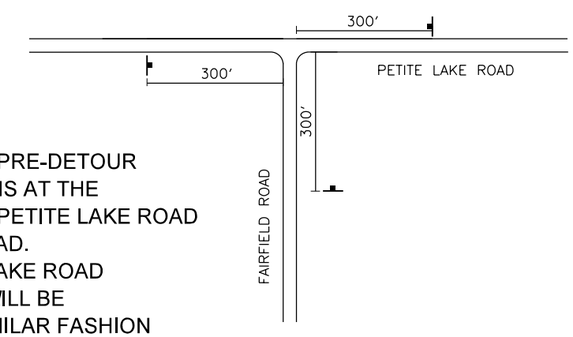
NOTE: SIGN SHEETING SHALL BE FLUORESCENT ORANGE WITH 5 INCH (125 mm) BLACK SERIES C LETTERS

DIMENSIONS (ENGLISH)

A	48"
B	36"
C	5/8"
D	7/8"
E	4 1/8"
F	5"
G	2 5/8"
H	9 3/4"
J	12"
K	5 1/2"
L	5 1/2"
M	8 1/2"
N	2 1/4"

LOCATIONS OF PRE-DETOUR INFORMATION SIGNS BY INTERSECTION

W.B. PETITE LAKE ROAD APPROACHING FARIFIELD ROAD
E.B. PETITE LAKE ROAD APPROACHING FARIFIELD ROAD
N.B. FARIFIELD ROAD APPROACHING PETITE LAKE ROAD
N.B. ILLINOIS ROUTE 59 APPROACHING PETITE LAKE ROAD
S.B. ILLINOIS ROUTE 59 APPROACHING PETITE LAKE ROAD
W.B. PETITE LAKE ROAD APPROACHING ILLINOIS ROUTE 59
E.B. PETITE LAKE ROAD, EAST OF ILLINOIS ROUTE 59



SIGN LAYOUT FOR PRE-DETOUR INFORMATION SIGNS AT THE INTERSECTION OF PETITE LAKE ROAD AND FAIRFIELD ROAD.
SIGNS AT PETITE LAKE ROAD AND IL ROUTE 59 WILL BE INSTALLED IN A SIMILAR FASHION

NOTE:
THESE SIGNS SHALL BE INSTALLED 7-10 CALENDAR DAYS PRIOR TO THE DETOUR AND ROAD CLOSURE. THE SIGNS SHALL BE REMOVED THE DAY THE DETOUR BEGINS.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P145409\DraftingData\Design\P145409-sht-cover.dgn	USER NAME = ledezmarm	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 9/13/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETOUR PLAN
IL. ROUTE 59 AT PETITE LAKE RD

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	30

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT

SCHEDULE OF QUANTITIES

SIGN NO.		SIGN TYPE	QUANTITY
1	DETOUR	M4-9-3024	13
2	DETOUR	M4-9R-3024	3
3	DETOUR	M4-9R-3024	9
4	DETOUR	M4-9L-3024	4
5	DETOUR	M4-9L-3024	7
6	EAST	M3-2-2412	21
7	WEST	M3-4-2412	15
8	PETITE LAKE ROAD	BLACK/ORANGE SPECIAL-VAR 6" BLACK LETTERS	42
9	END DETOUR	M4-8A-2418	2
10	ROAD CLOSED 500 FT	W20-1-4848	1
11	ROAD CLOSED AHEAD	W20-3-4848	1
12	DETOUR AHEAD	W20-3-4848	5
13	PETITE LAKE ROAD CLOSED USE IL RTE 132	BLACK/ORANGE SPECIAL-6030 6" BLACK LETTERS	2
14	PETITE LAKE ROAD CLOSED USE IL RTE 132	BLACK/ORANGE SPECIAL-6030 6" BLACK LETTERS	3
15	DETOUR	M4-10L-4818	1
16	DETOUR	M4-10R-4818	2
17	ROAD CLOSED TO THRU TRAFFIC	R11-4-6030	4
18	ROAD CLOSED	R11-2-4830	4
19		R3-1 30"x30"	1
20		R3-2 30"x30"	2

DETOUR GENERAL NOTES

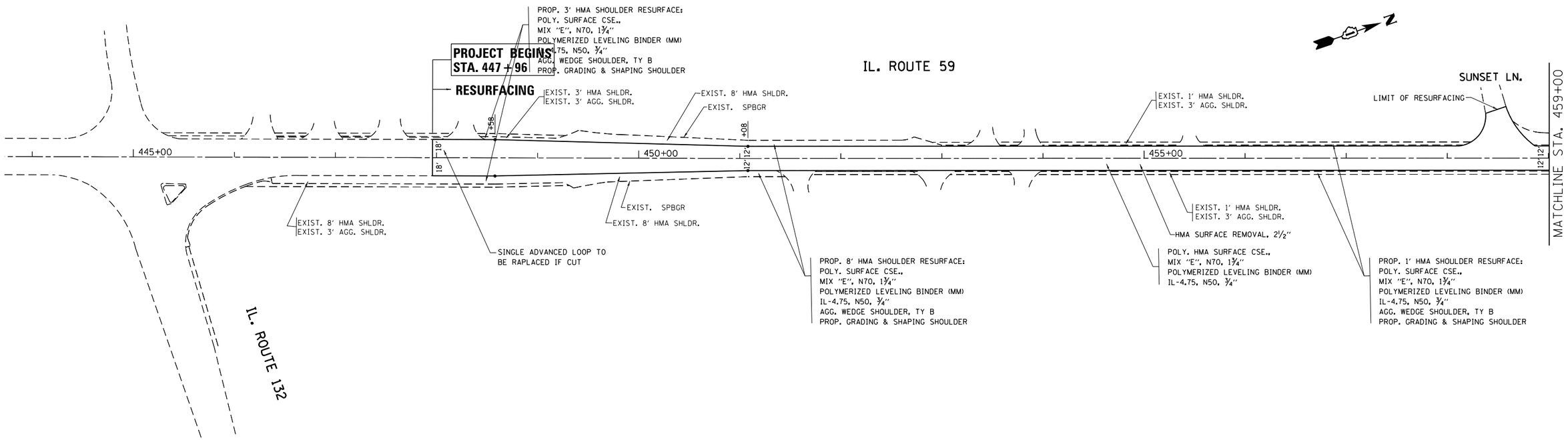
1. ALL SIGNING SHALL BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JAN. 1, 2012", " THE QUALITY STANDARD FOR WORK ZONE TRAFFIC CONTROL DEVICES ADOPTED 2010", THE DETAILS IN THESE PLANS, THE LATEST EDITION OF THE STATE OF ILLINOIS "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", THE SPECIAL PROVISIONS FOR TRAFFIC CONTROL AND PROTECTION (LC-T-SECTION 700) AND THE L.C.D.O.T. DETOUR PROCEDURES AND GUIDELINES.
2. THE ENGINEER SHALL BE NOTIFIED IN WRITING AT LEAST THREE WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT. THE ENGINEER SHALL DETERMINE THE HOUR OF CLOSURE. THE ENGINEER WILL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES.
3. IF DEEMED NECESSARY BY THE ENGINEER A PRE-CONSTRUCTION MEETING WITH THE CONTRACTOR SHALL BE HELD AT LEAST TWO WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT.
4. THE CONTRACTOR SHALL SUPPLY TO THE ENGINEER THE NAMES AND TELEPHONE NUMBERS OF HIS REPRESENTATIVES ON THE CONSTRUCTION SITE AND HIS REPRESENTATIVE RESPONSIBLE FOR THE DETOUR SIGNING PRIOR TO THE START OF THE WORK. THE LAKE COUNTY DIVISION OF TRANSPORTATION REPRESENTATIVE FOR THIS DETOUR IS:

   TOM BENNECKE
   LAKE COUNTY DIVISION OF TRANSPORTATION
   TRAFFIC MANAGEMENT CENTER/TRAFFIC ENGINEERING
   600W. WINCHESTER ROAD
   LIBERTYVILLE, ILLINOIS 60048
   (847) 377-7476

5. IF REQUESTED BY THE CONTRACTOR IN WRITING AT LEAST THREE WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT THE ENGINEER WILL FIELD LOCATE THE POSITIONS OF ANY SIGNS.
6. LONGITUDINAL DIMENSIONS SHOWN ON THESE PLANS MAY BE ADJUSTED TO FIT FIELD CONDITIONS, WITH THE APPROVAL OF THE ENGINEER.
7. THE ROAD SHALL NOT BE CLOSED UNTIL ALL SIGNING IS ERECTED IN ACCORDANCE WITH THE DETOUR PLAN AND INSPECTED AND APPROVED BY THE ENGINEER.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL BARRICADES, SIGNS, LIGHTS, AND OTHER DEVICES INSTALLED BY HIM ARE IN PLACE AND OPERATING 24 HOURS EACH DAY INCLUDING SUNDAYS AND HOLIDAYS DURING THE TIME THE DETOUR IS IN EFFECT.
9. THE TRAFFIC CONTROL SHOWN ON THE DETOUR PLAN IS THE MINIMUM NECESSARY TO ENSURE THIS ROAD CLOSURE. THE CONTRACTOR SHALL MAKE ALL CHANGES IN TRAFFIC CONTROL THAT IS DEEMED NECESSARY BY THE ENGINEER. ADDITIONS AND DELETIONS OF TRAFFIC CONTROL FOR THIS DETOUR SHALL BE CONSIDERED INCIDENTAL TO THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION".
10. ALL EXISTING SIGNING THAT IS NOT APPLICABLE WHILE THE DETOUR IS IN EFFECT SHALL BE COMPLETELY COVERED BY THE CONTRACTOR, IN A MANNER APPROVED BY THE ENGINEER.
11. ALL DETOUR SIGNING SHALL BE POST MOUNTED IF THE ROAD CLOSURE IS TO EXCEED FOUR (4) CALENDAR DAYS.
12. ALL DETOUR SIGNING EXCEPT REGULATORY SIGNS SHALL HAVE BLACK LEGENDS ON FLUORESCENT ORANGE SHEETING AND STANDARD BLACK BORDERS. THE FLUORESCENT ORANGE REFLECTIVE SHEETING SHALL MEET THE REQUIREMENTS OF SECTION 1091 OF THE STANDARD SPECIFICATIONS. ALL DETOUR SIGNING SHALL BE NEW OR LIKE NEW CONDITION. THE ENGINEER SHALL BE THE SOLE JUDGE OF THE CONDITION OF THE SIGNS.
13. THE SIZES OF ALL SIGNS NOT SPECIFIED IN THESE PLANS SHALL BE AS REQUIRED BY THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
14. AS A MINIMUM, ALL AMBER FLASHING LIGHTS THAT ARE REQUIRED FOR THIS DETOUR SHALL MEET THE REQUIREMENTS FOR TYPE A-LOW INTENSITY FLASHING LIGHTS IN ARTICLE 1106.02 OF THE STANDARD SPECIFICATIONS. ALL LIGHTS SHALL OPERATE DURING THE HOURS OF DARKNESS. ONLY LIGHTS THAT HAVE BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION SHALL BE USED.
15. ALL BARRICADES SHALL HAVE REFLECTORIZED STRIPING ON BOTH SIDES OF THE BARRICADES.
16. THE "ROAD CLOSED" (R11-2), THE "ROAD CLOSED XX MILES AHEAD LOCAL TRAFFIC ONLY" (R11-3) AND THE "ROAD CLOSED TO THRU TRAFFIC" (R11-4) SIGNS SHALL BE MOUNTED ABOVE THE TOP OF THE BARRICADE. ALL TYPE III BARRICADES SHALL HAVE TWO (2) AMBER TYPE A-LOW INTENSITY FLASHING LIGHTS SPACED NEAR THE CENTERLINE OF THE SUPPORTS.
17. THE ROAD NAME SIGN SHALL HAVE A BLACK LEGEND ON FLUORESCENT ORANGE REFLECTIVE SHEETING. THE CAPITAL LETTERS SHALL BE 6" WITH 5" LOWER CASE.
18. DURING NON-WORKING HOURS AT THE POINT OF ROAD CLOSURE TO ALL TRAFFIC THE CONTRACTOR SHALL PROVIDE A MEANS TO RESTRAIN THE BARRICADES FROM EASY MOVEMENT BY VANDALS. THE CHOSEN METHOD SHALL BE APPROVED BY THE ENGINEER.
19. CONSTRUCTION EQUIPMENT SHALL NOT BE PARKED IMMEDIATELY BEHIND THE TYPE III BARRICADES DURING NON-WORKING HOURS. IN ANY EVENT ARTICLE 701.04 OF THE STANDARD SPECIFICATIONS SHALL APPLY.
20. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE VISIBILITY OF ALL DETOUR AND CONSTRUCTION SIGNING, INCLUDING BRUSHING BACK VEGETATION IF DEEMED NECESSARY BY THE ENGINEER PER ARTICLE 201.10
21. THE FOLLOWING ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD IS APPLICABLE FOR THIS WORK:

    STANDARD 701006, 701301, 701306, 701326, 701901

22. THE ENGINEER SHALL BE NOTIFIED AT LEAST TWO (2) HOURS BEFORE THE ROAD IS TO BE OPENED TO TRAFFIC. THE ENGINEER WILL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES.



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

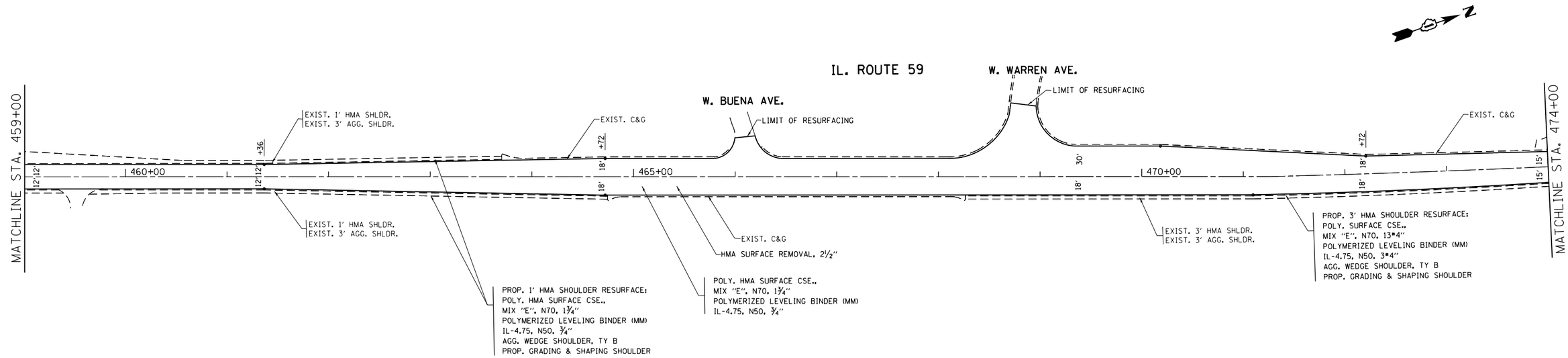
EXISTING AND PROPOSED ROADWAY PLAN
IL. ROUTE 59 AT PETITE LAKE RD

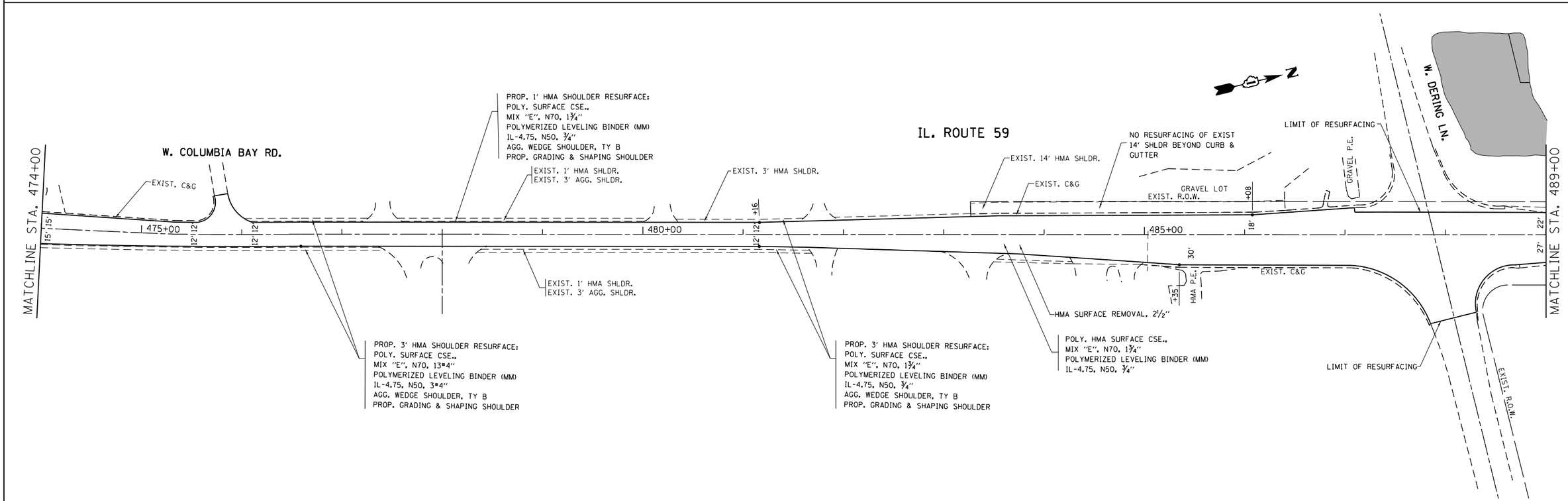
SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	32

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT





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	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -			104	105N-4	LAKE	113 / 33
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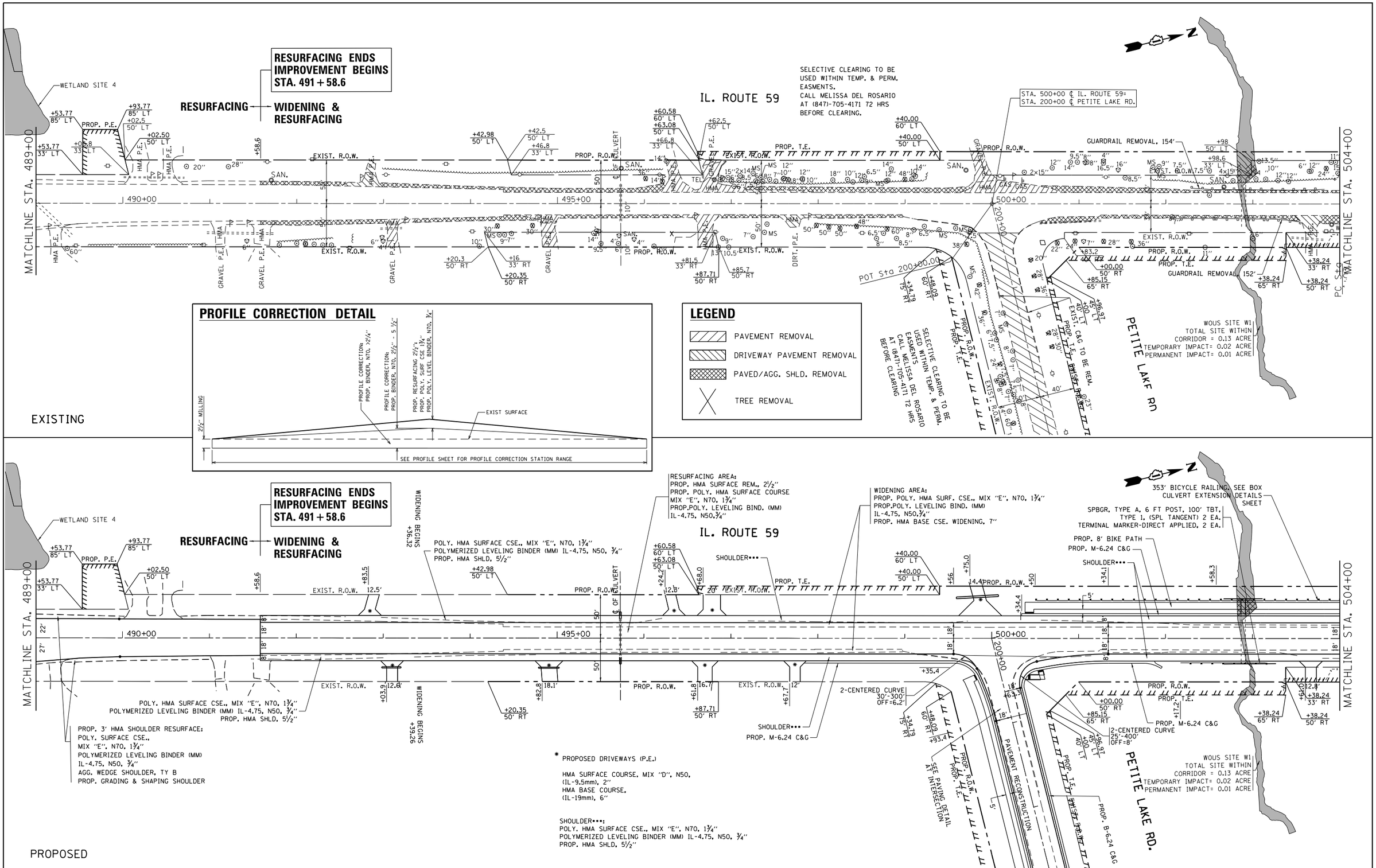
							ILLINOIS FED. AID PROJECT		



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		DRAWN -	REVISED -			104	105N-4	LAKE	113 / 34
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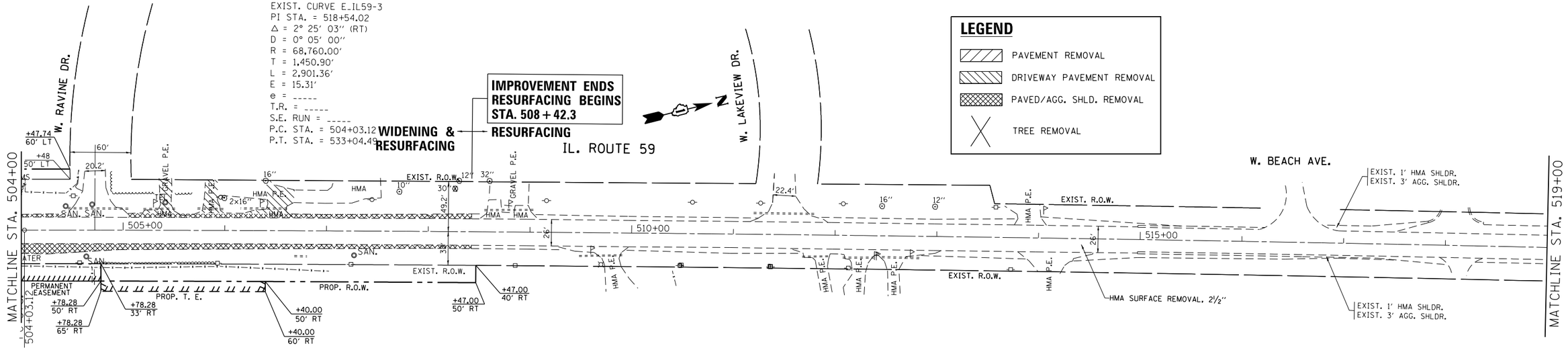

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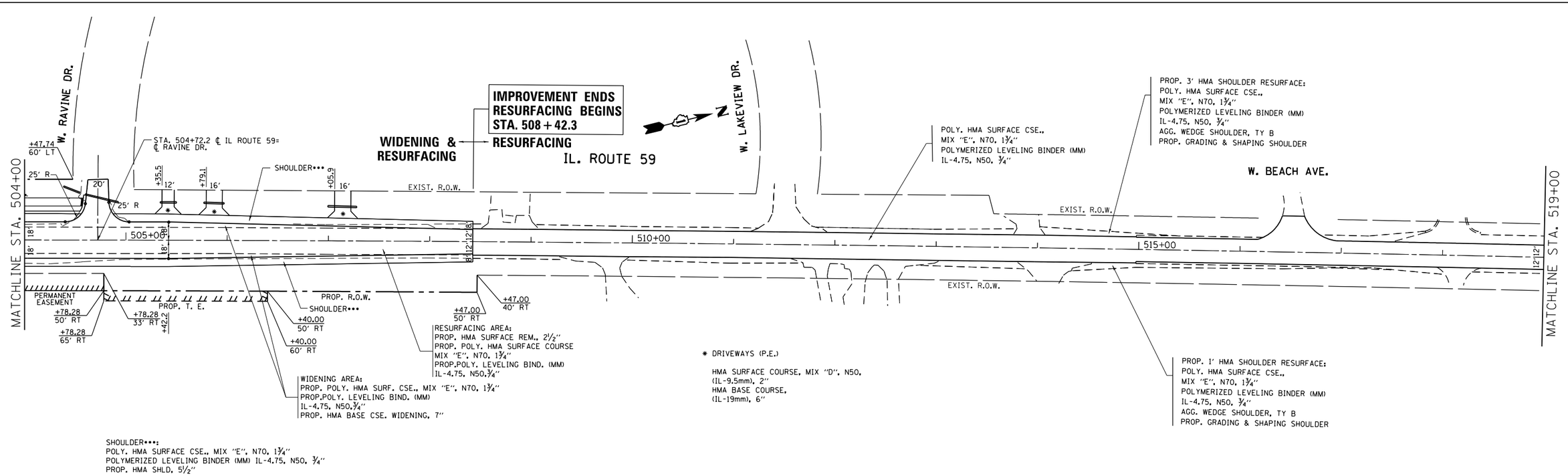

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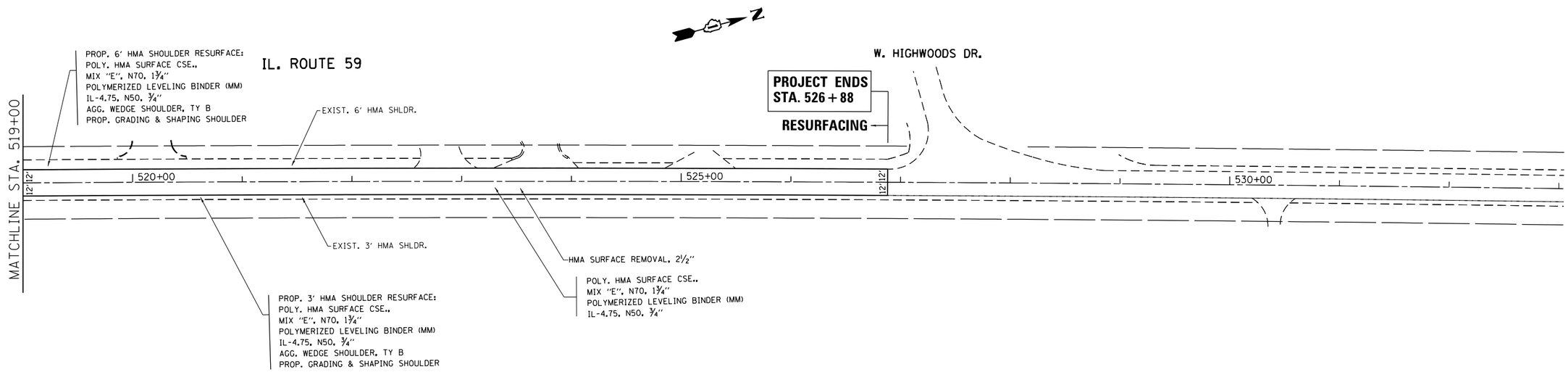
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLAN
IL. ROUTE 59 AT PETITE LAKE RD

SCALE: 1″=50′ | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	35
		CONTRACT NO. 60W06		
	ILLINOIS	FED. AID PROJECT		



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		DRAWN -	REVISED -			104	105N-4	LAKE	113	36
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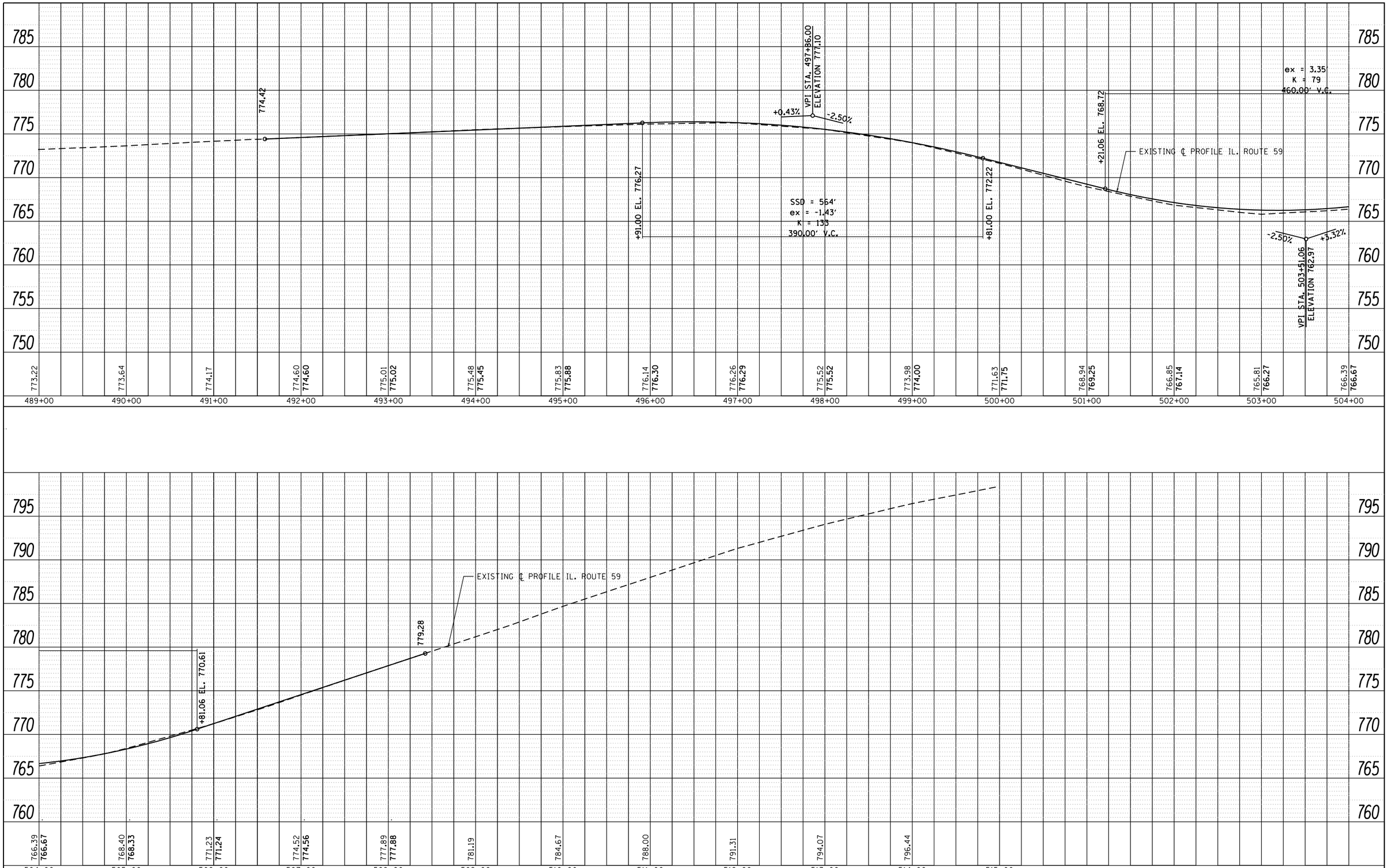

PLAN
NOTE BOOK NO. ____
SURVEYED ____
PLOTTED ____
ALIGNMENT CHECKED ____
RT. OF WAY CHECKED ____
CADD FILE NAME ____
BY ____ DATE ____

PROFILE
NOTE BOOK NO. ____
SURVEYED ____
PLOTTED ____
GRADES CHECKED ____
B.M. NOTED ____
STRUCTURE NOTAT'NS CHKD ____
BY ____ DATE ____

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

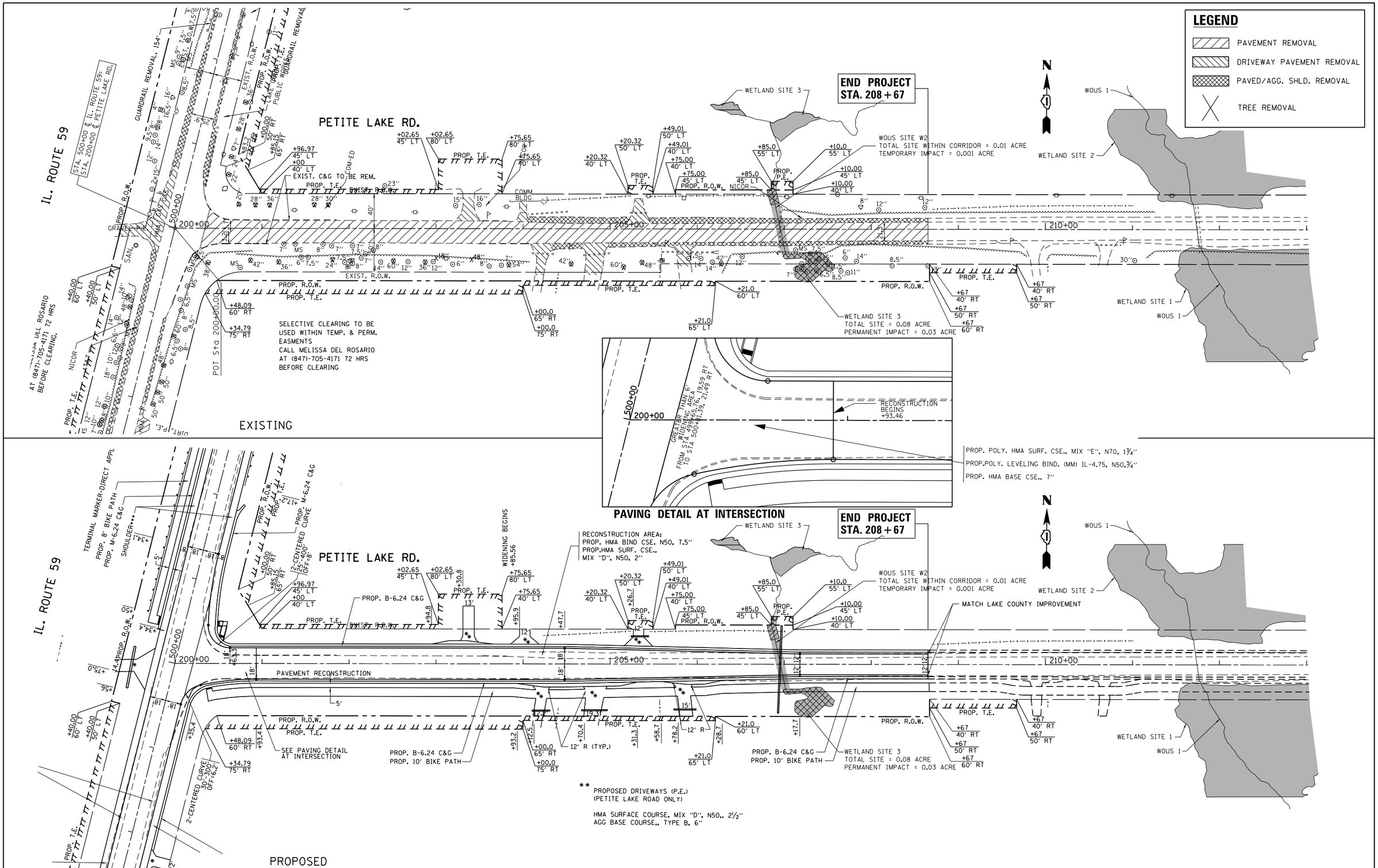
EXISTING AND PROPOSED ROADWAY PROFILE
IL. ROUTE 59 AT PETITE LAKE RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	37

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

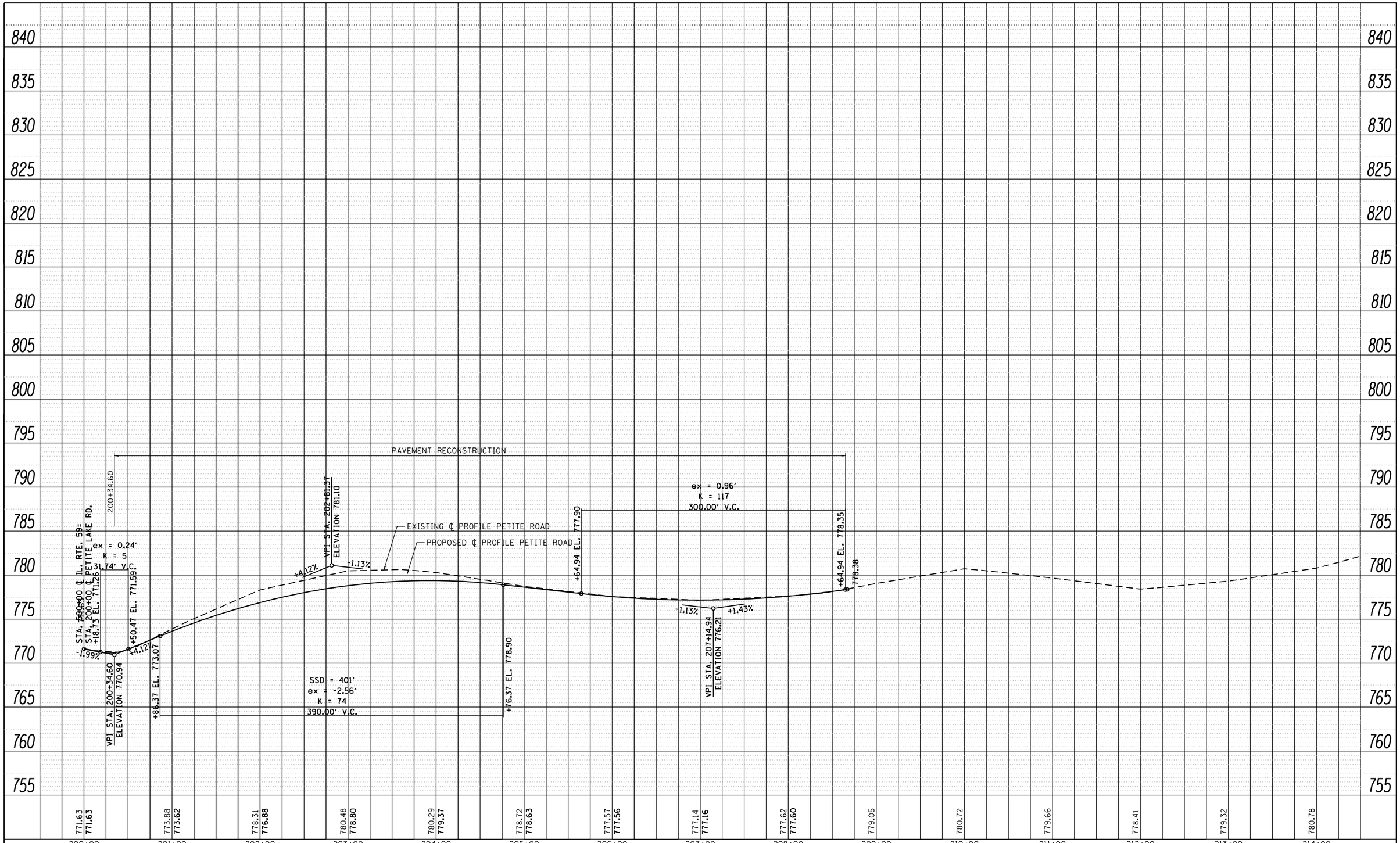
EXISTING AND PROPOSED ROADWAY PLAN
IL. ROUTE 59 AT PETITE LAKE RD

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	38

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



PLAN		BY	DATE
SURVEYED			
PLOTTED			
ALIGNMENT CHECKED			
RT. OF WAY CHECKED			
CADD FILE NAME			
NOTE BOOK NO.			

PROFILE		BY	DATE
SURVEYED			
PLOTTED			
GRADES CHECKED			
B.M. NOTED			
STRUCTURE NOTAT'NS CHK'D			
NOTE BOOK NO.			

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PROFILE
IL. ROUTE 59 AT PETITE LAKE RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

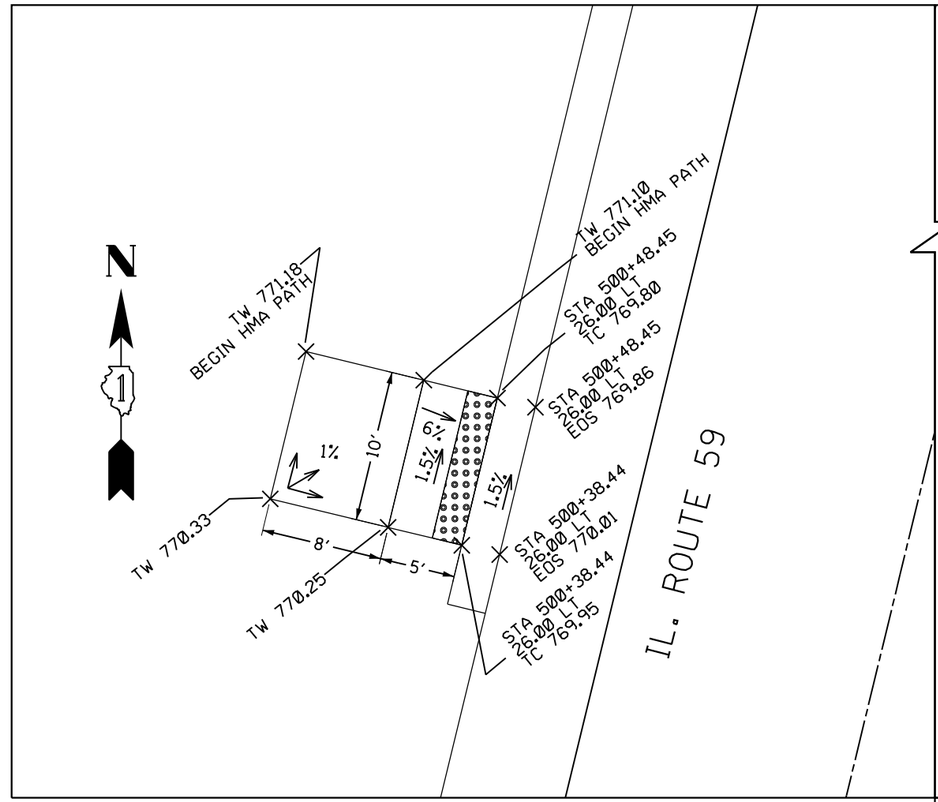
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	39

CONTRACT NO. 60W06

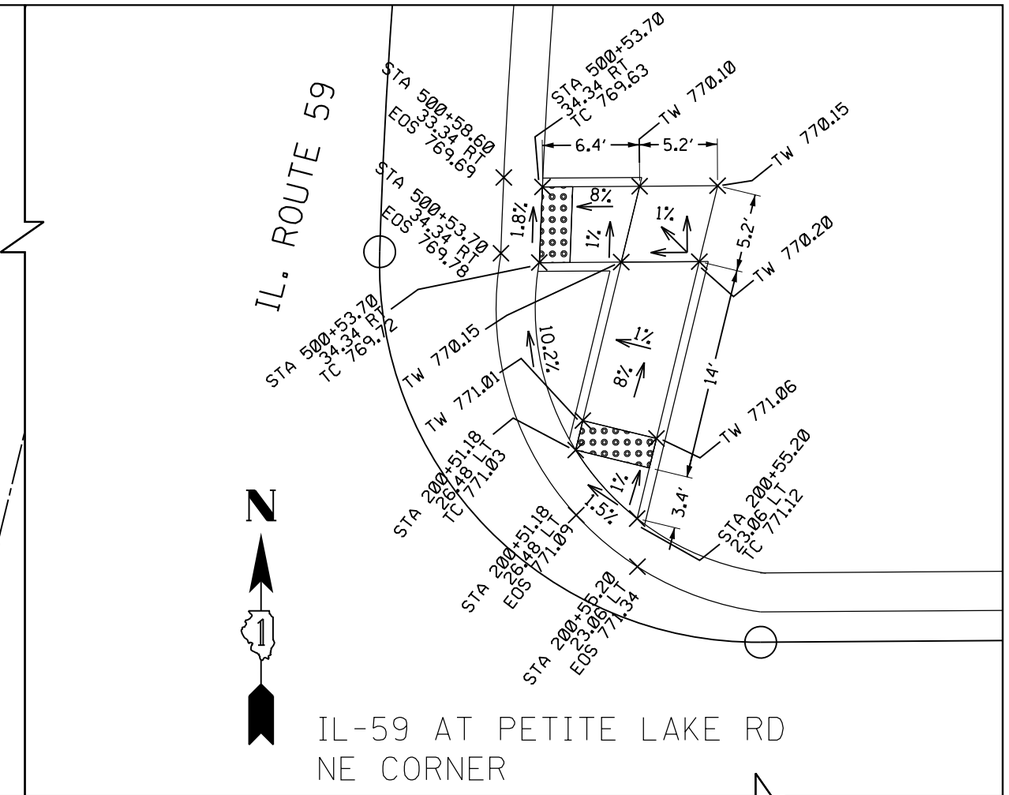
ILLINOIS FED. AID PROJECT

LEGEND:

EOS	EDGE OF SHOULDER
EOP	EDGE OF PAVEMENT
TC	TOP OF CURB
TW	TOP OF WALK

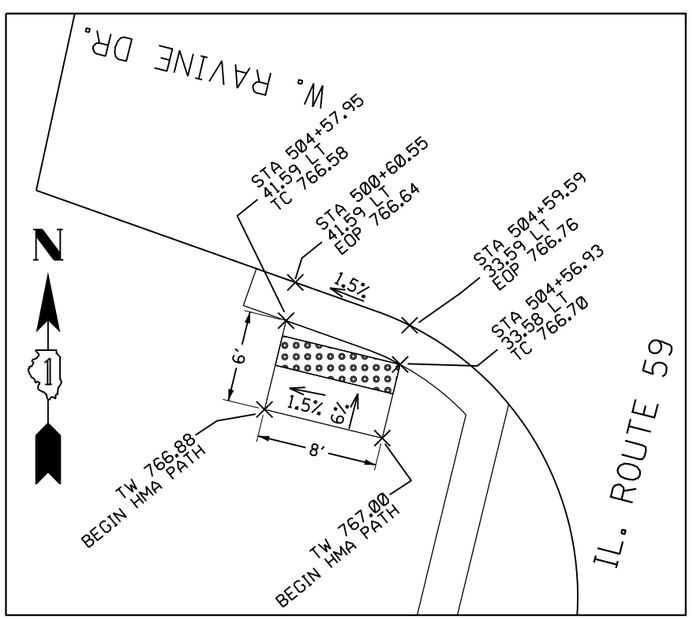


IL-59 AT PETITE LAKE RD
NW CORNER

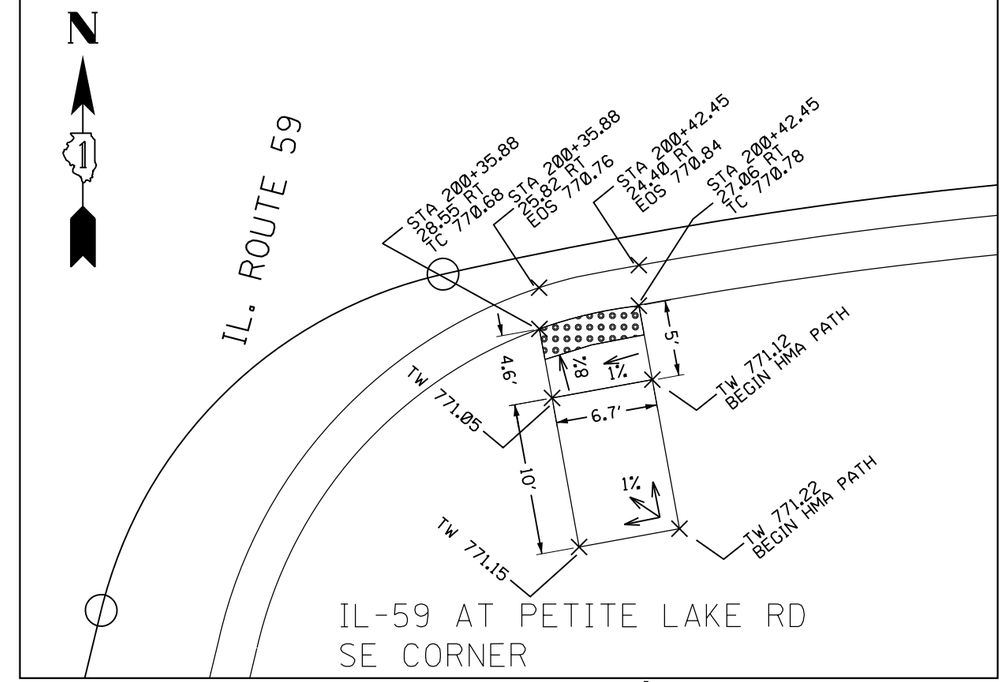


IL-59 AT PETITE LAKE RD
NE CORNER

PETITE LAKE RD.



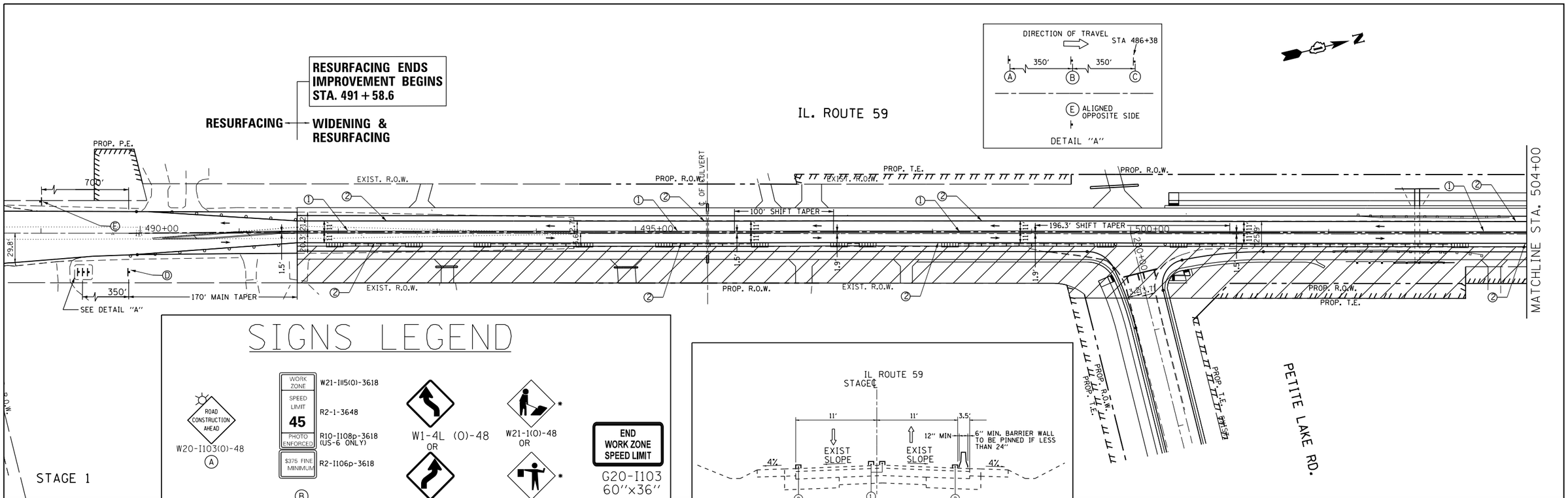
IL-59 AT W. RAVINE DR
SW CORNER



IL-59 AT PETITE LAKE RD
SE CORNER

INTERSECTION GRADING PLAN
IL. RTE. 59 (FOX LAKE RD.) AT PETITE LAKE ROAD

1" = 5'



SIGNS LEGEND

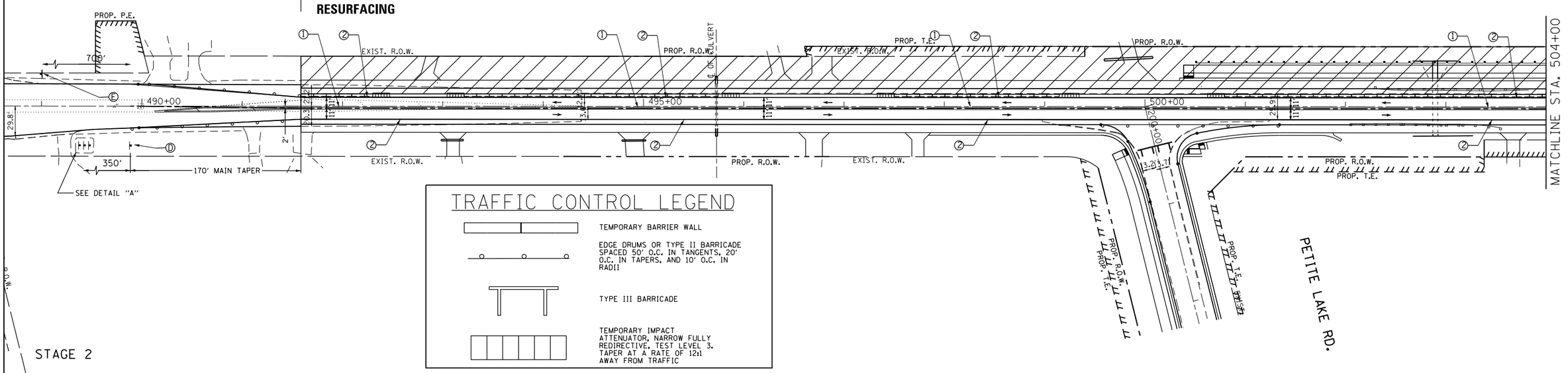
ROAD CONSTRUCTION AHEAD	W20-I103(0)-48 (A)
WORK ZONE / SPEED LIMIT 45 / PHOTO ENFORCED	W21-I115(0)-3618
	R2-1-3648
	R10-I108p-3618 (US-6 ONLY)
$375 FINE MINIMUM	R2-I106p-3618
	(B)
	W1-4L (0)-48 OR W1-4(0)-48 (C)
	W21-1(0)-48 OR W20-7(0)-48 (D)
END WORK ZONE SPEED LIMIT	G20-I103 60"x36" (E)

* TO BE REMOVED WHEN WORKERS OR FLAGGERS ARE NOT PRESENT FOR AN HOUR OR MORE

IL 59 (FOX LAKE RD)
STAGE 1 TYPICAL SECTION
STA. 491+58 TO 508+42

TEMPORARY MARKINGS LEGEND

1. DOUBLE YELLOW, 4"
2. WHITE EDGE LINE, 4"



TRAFFIC CONTROL LEGEND

- TEMPORARY BARRIER WALL
- EDGE DRUMS OR TYPE II BARRICADE SPACED 50' O.C. IN TANGENTS, 20' O.C. IN TAPERS, AND 10' O.C. IN RADII
- TYPE III BARRICADE
- TEMPORARY IMPACT ATTENUATOR, NARROW FULLY REDIRECTIVE, TEST LEVEL 3. TAPER AT A RATE OF 12:1 AWAY FROM TRAFFIC

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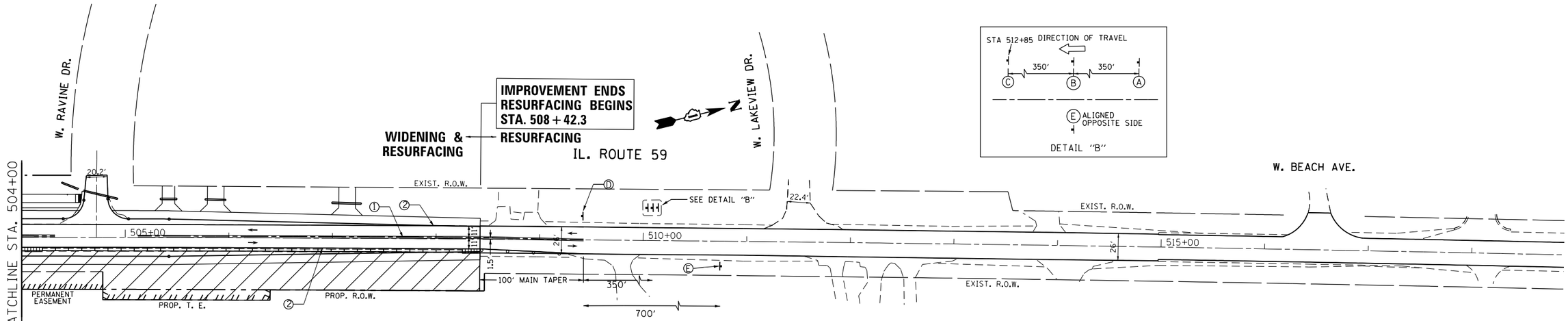
			REVISED -
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 9/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

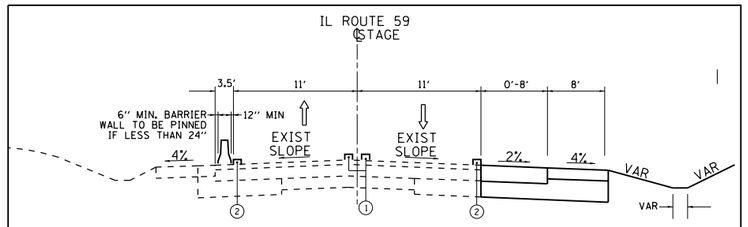
SUGGESTED STAGING PLAN
IL. ROUTE 59 AT PETITE LAKE RD

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	41
		CONTRACT NO. 60W06		
	ILLINOIS FED. AID PROJECT			

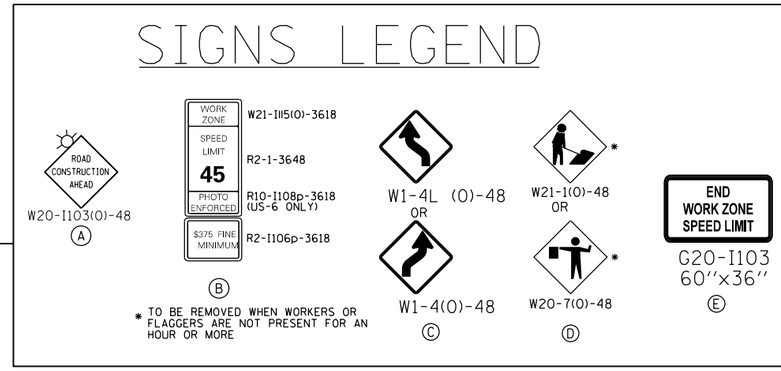


STAGE 1



IL 59 (FOX LAKE RD)
STAGE 2 TYPICAL SECTION
STA. 491+58 TO 508+42

SIGNS LEGEND

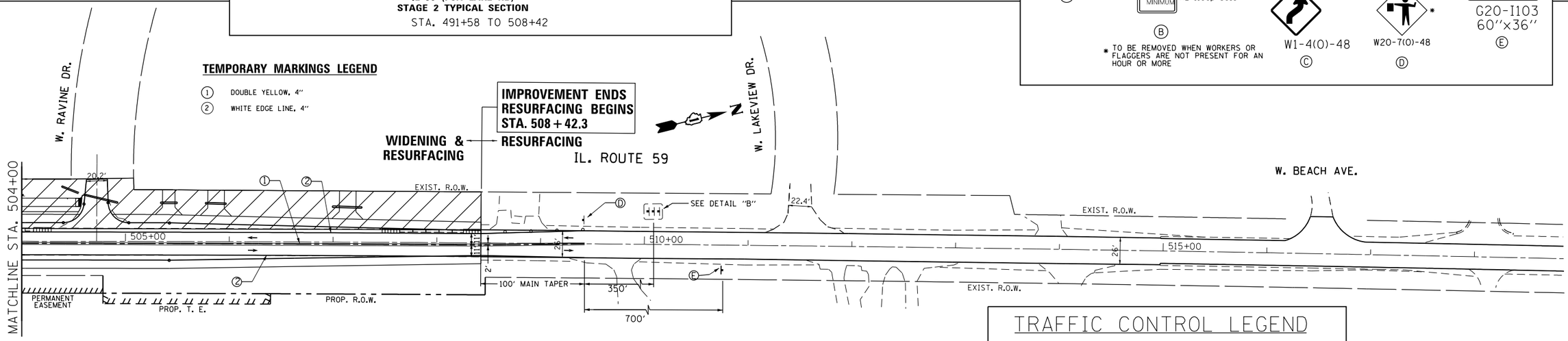


* TO BE REMOVED WHEN WORKERS OR FLAGGERS ARE NOT PRESENT FOR AN HOUR OR MORE

STAGE 2

TEMPORARY MARKINGS LEGEND

1. DOUBLE YELLOW, 4"
2. WHITE EDGE LINE, 4"



TRAFFIC CONTROL LEGEND

- TEMPORARY BARRIER WALL
- EDGE DRUMS OR TYPE II BARRICADE SPACED 50' O.C. IN TANGENTS, 20' O.C. IN TAPERS, AND 10' O.C. IN RADII
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- TEMPORARY IMPACT ATTENUATOR, NARROW FULLY REDIRECTIVE, TEST LEVEL 3. TAPER AT A RATE OF 12:1 AWAY FROM TRAFFIC

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Default	PLOT DATE = 9/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGING PLAN
IL. ROUTE 59 AT PETITE LAKE RD

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	42

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT

EROSION CONTROL GENERAL NOTES

1. TEMPORARY EROSION CONTROL SEEDING TO BE PROVIDED AT ALL ERODIBLE BARE EARTH AREAS.

2. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN INLET PROTECTION AT ALL DRAINAGE STRUCTURES AND AT OTHER LOCATIONS DIRECTED BY THE ENGINEER OR SHOWN ON THE PLANS. INLET PROTECTION SHALL BE PLACED IMMEDIATELY AFTER THE AREA HAS BEEN DISTURBED.

3. ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND OF THE APPLICABLE STATE STANDARDS FOR THE ENTIRE DURATION OF THE CONTRACT OR UNTIL SUCH TIME AS DIRECTED BY THE ENGINEER.

4. PERIMETER EROSION CONTROL BARRIER SHALL BE PLACED 12 INCHES FROM THE R.O.W. OR EASEMENT OR AS DIRECTED BY THE ENGINEER.

5. AT ANY AREA WHERE THERE IS NO PROPOSED GRADING THE EXISTING GROUND COVER SHALL REMAIN.

6. DURING CONSTRUCTION AND WHEN ERODIBLE SOILS ARE EXPOSED, ALL DRAINAGE SRUCTURES NOT PROTECTED WITH INLET OR PIPE PROTECTION SHALL BE KEPT SEALED TO PREVENT ACCESS FROM EROSION. THIS WORK SHALL BE CONSTRUCTED AS PART OF THE COST OF VARIOUS DRAINAGE STRUCTURES.

7. THE CONTRACTOR WILL ASSUME RESPONSIBILITY FOR MAINTENANCE OF ALL SOIL EROSION CONTROL DURING CONSTRUCTION.

8. THE CONTRACTOR SHALL CHECK ALL ESC MEASURES WEEKLY AND AFTER EACH RAINFALL, 0.5 INCHES OR GREATER IN A 24 HOUR PERIOD, OR EQUIVALENT SNOWFALL. ADDITIONALLY DURING WINTER MONTHS, ALL MEASURES SHOULD BE CHECKED BY THE CONTRACTOR AFTER EACH SIGNIFICANT SNOWMELT

9. THE CONTRACTOR SHALL CHECK ALL ESC MEASURES WEEKLY AND AFTER EACH RAINFALL, 0.5 INCHES OR GREATER IN A 24 HOUR PERIOD, OR EQUIVALENT SNOWFALL. ADDITIONALLY DURING WINTER MONTHS, ALL MEASURES SHOULD BE CHECKED BY THE CONTRACTOR AFTER EACH SIGNIFICANT SNOWMELT

10. THE CONTRACTOR SHOULD PROVIDE TO THE RE A PLAN TO ENSURE THAT A STABILIZED FLOW LINE WILL BE PROVIDED DURING STORM SEWER CONSTRUCTION. THE USE OF A STABILIZED FLOW LINE BETWEEN INSTALLED STORM SEWER AND OPEN DISTURBANCE WILL REDUCE THE POTENTIAL FOR THE OFFSITE DISCHARGE OF SEDIMENT-BEARING WATERS, ESPECIALLY WHEN RAIN IS FORECASTED, SO THAT FLOW WILL NOT ERODE. LACK OF APPROVED PLAN OR FAILURE TO COMPLY WILL RESULT IN AN ESC DEFICIENCY DEDUCTION.

11. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.

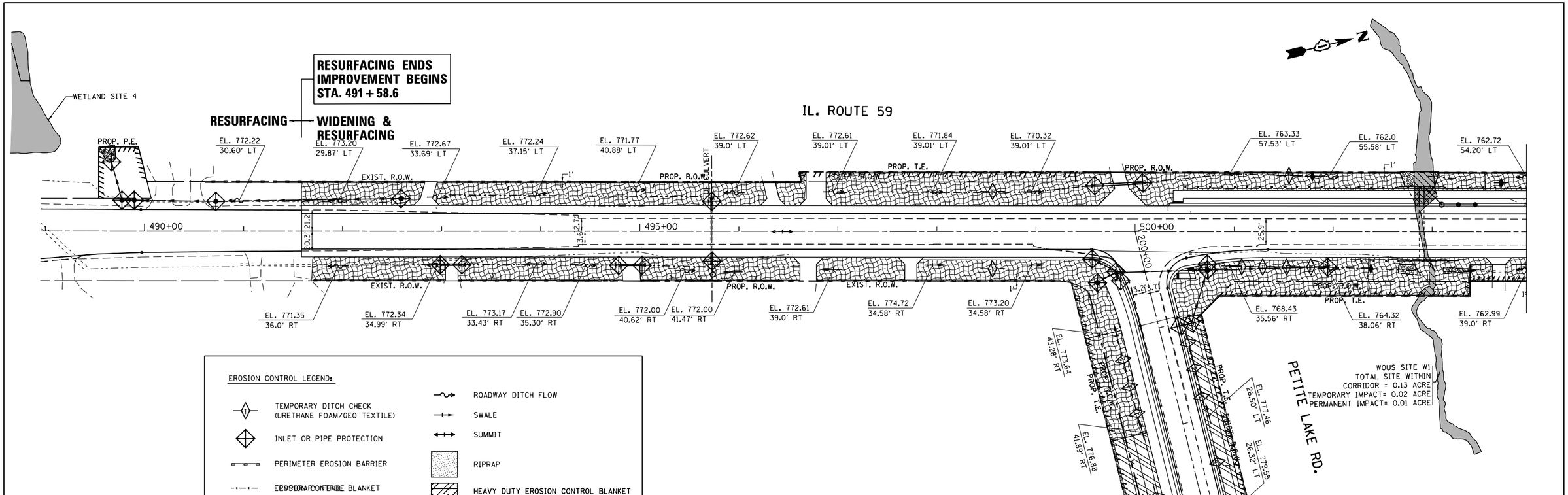
12. TEMPORARY OR PERMANENT STABILIZATION SHALL BE INITIATED IMMEDIATELY UPON COMPLETION OF DISTURBANCE OR IF THE WORK AREA IS TO BE LEFT UNDISTURBED FOR 14 DAYS OR MORE

13. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG FINAL GRADING AND SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.

14. EROSION CONTROL ITEMS ARE CONSIDERED TO BE A HIGH PRIORITY ON THIS CONTRACT. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE RE.

15. THIS PROJECT REQUIRES A US ARMY CORPS OF ENGINEERS (USACE) 404 PERMIT THAT WILL BE SECURED BY THE DEPARTMENT. ALL CONDITIONS OF THE 404 PERMIT, FOUND IN THE SPECIAL PROVISIONS, MUST BE FOLLOWED. AS A CONDITION OF THIS PERMIT, THE CONTRACTOR WILL NEED TO SUBMIT AN IN-STREAM WORK PLAN (INCLUDING WORK WITHIN WETLANDS) TO THE DEPARTMENT FOR APPROVAL. GUIDELINES ON ACCEPTABLE IN-STREAM WORK TECHNIQUES (INCLUDING WORK WITHIN WETLANDS) CAN BE FOUND ON THE USACE WEBSITE. THE USACE DEFINES AND DETERMINES IN-STREAM WORK. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT AN IN-STREAM WORK PLAN (INCLUDING WORK WITHIN WETLANDS) WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

16. "WETLANDS NO INTRUSION" SIGNAGE SHOULD ALSO BE PROVIDED AT THE BOUNDARY OF ALL UN-IMPACTED WETLANDS AND/OR WOUS. THE CONTRACTOR CAN BORROW THE SIGNS FROM THE BUREAU OF MAINTENANCE. INCLUDE TEMPORARY FENCING AND WETLAND SIGNAGE WITHIN THE EROSION AND SEDIMENT CONTROL STRATEGY.



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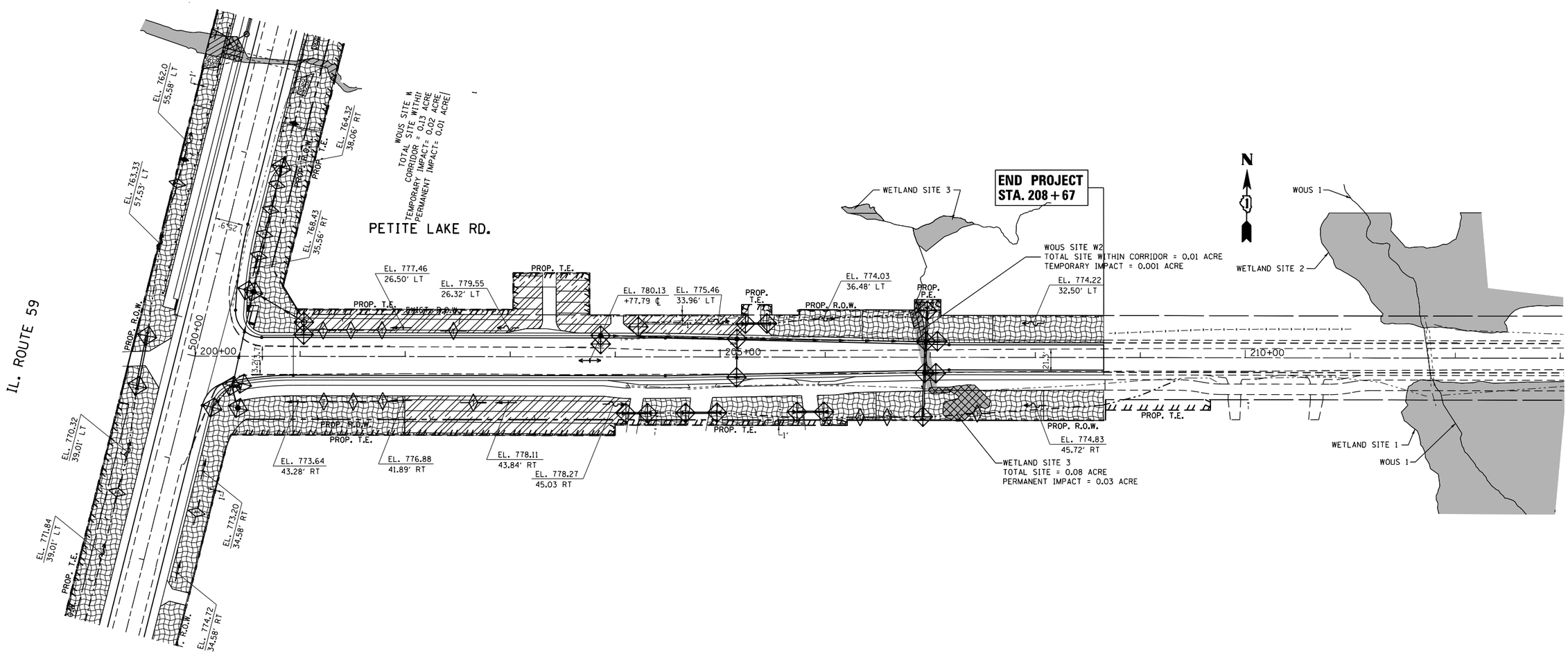
		DRAWN -	REVISED -			104	105N-4	LAKE	113	44
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -			CONTRACT NO. 60W06				
Default	PLOT DATE = 8/19/2016	DATE -	REVISED -		SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.	ILLINOIS FED. AID PROJECT				



EROSION CONTROL LEGEND:

- TEMPORARY DITCH CHECK (URETHANE FOAM/GEO TEXTILE)
- INLET OR PIPE PROTECTION
- PERIMETER EROSION BARRIER
- TEMPORARY FENCE
- EROSION CONTROL BLANKET
- ROADWAY DITCH FLOW
- SWALE
- SUMMIT
- RIPRAP
- HEAVY DUTY EROSION CONTROL BLANKET

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Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

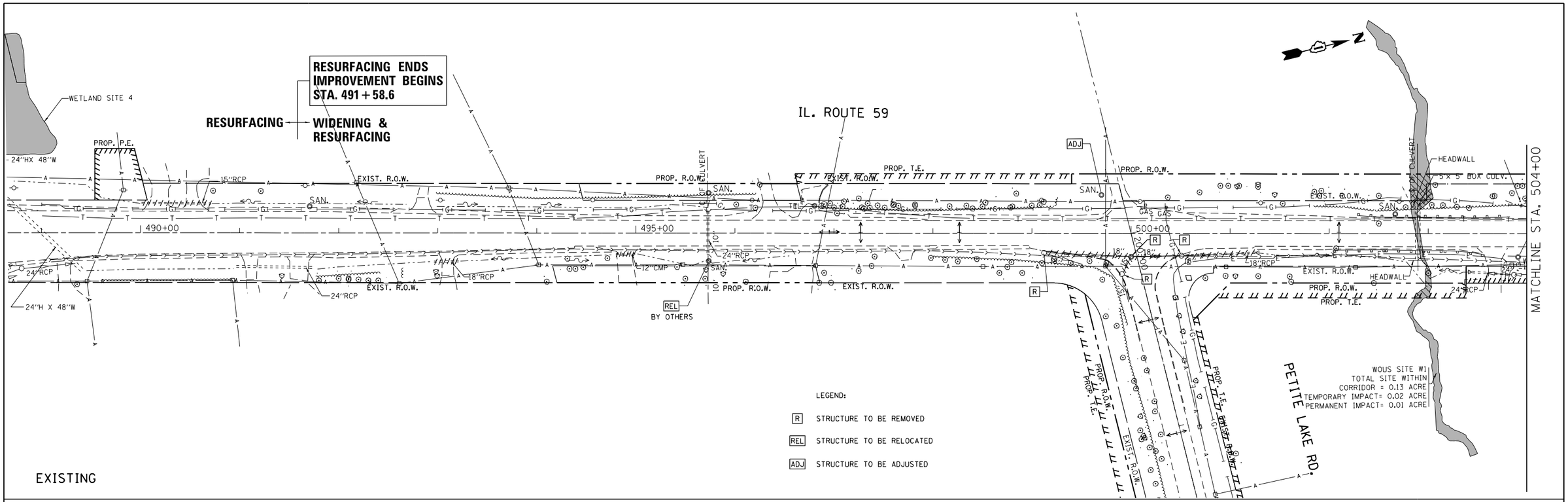
EROSION CONTROL PLAN
IL. ROUTE 59 AT PETITE LAKE RD

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

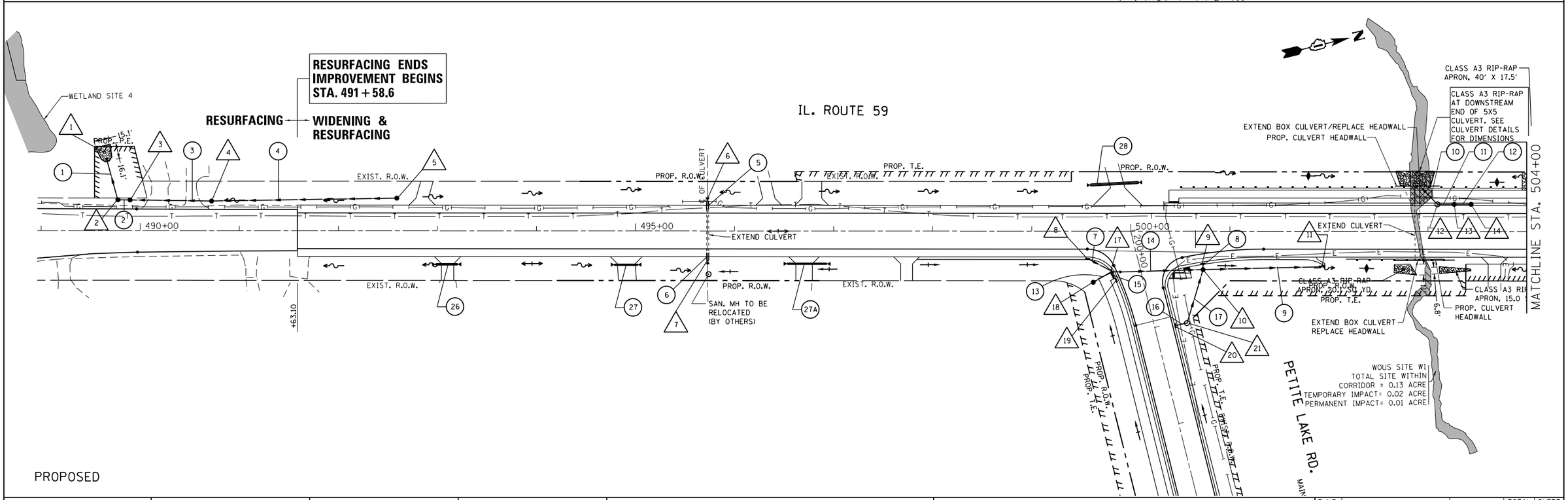
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	45

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



EXISTING



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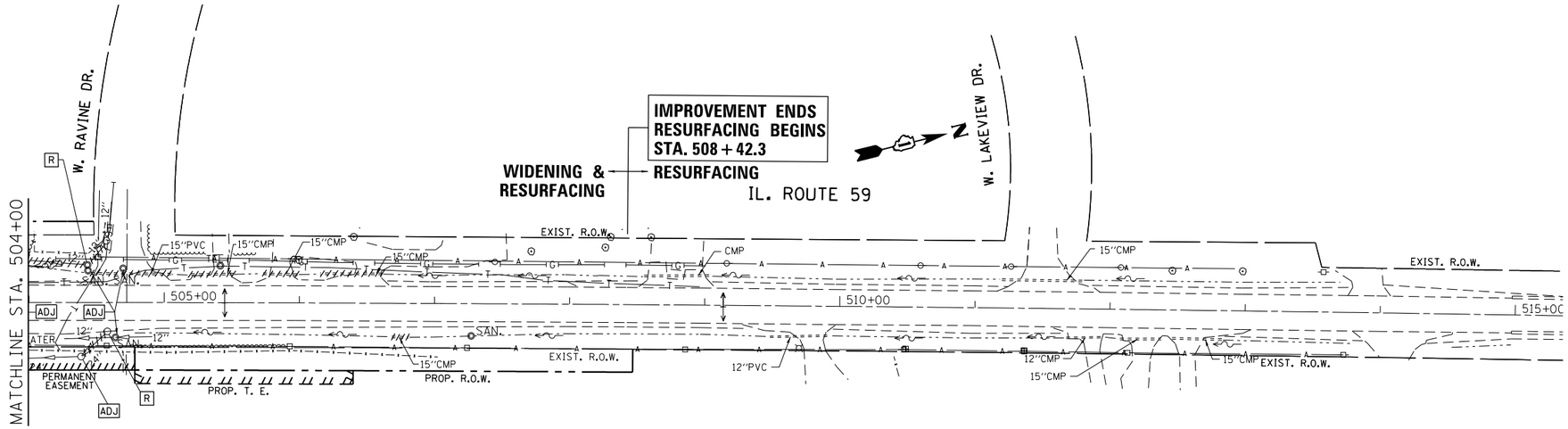
Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRAINAGE AND UTILITIES PLAN
IL. ROUTE 59 AT PETITE LAKE RD

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	46
		CONTRACT NO. 60W06		
		ILLINOIS FED. AID PROJECT		



EXISTING SANITARY STRUCTURES
TO BE ADJUSTED BY OTHERS:

- STA. 503+90.66, 23.67 FT. (RT.)
  T.G. 765.99
- STA. 504+43.62, 23.65 FT. (LT.)
  T.G. 766.07
- STA. 504+63.93, 25.82 FT. (RT.)
  T.G. 767.28

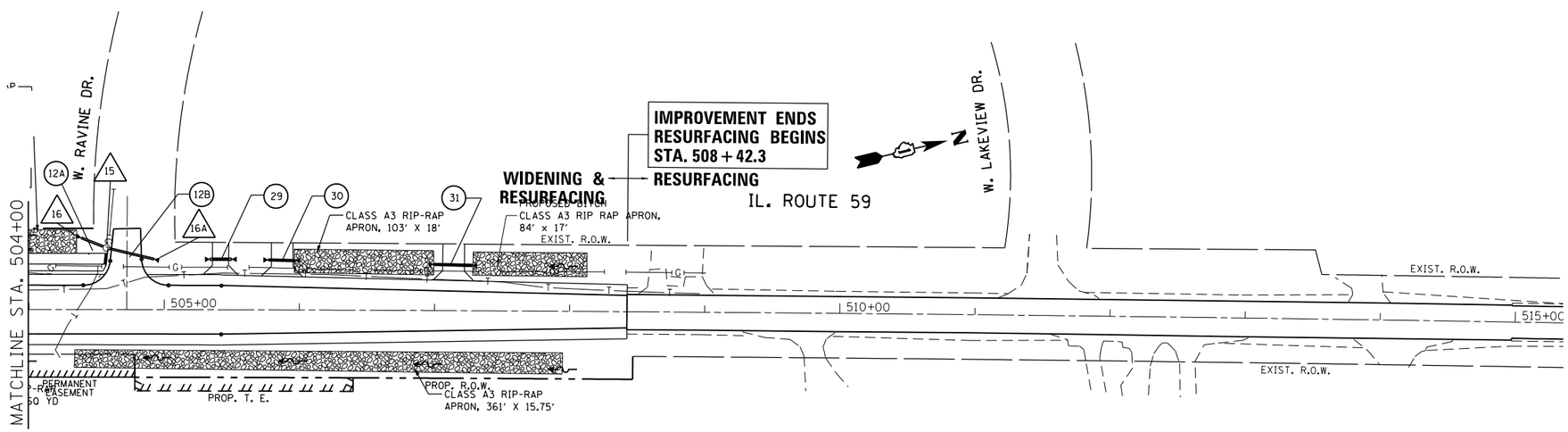
LEGEND:

R STRUCTURE TO BE REMOVED

REL STRUCTURE TO BE RELOCATED

ADJ STRUCTURE TO BE ADJUSTED

EXISTING



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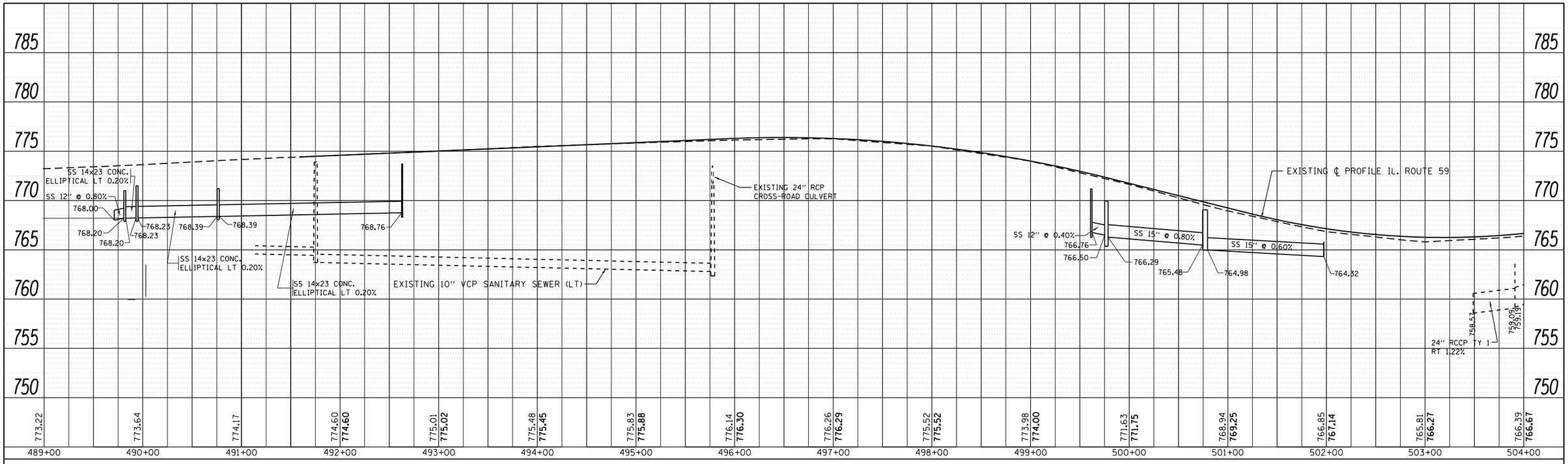
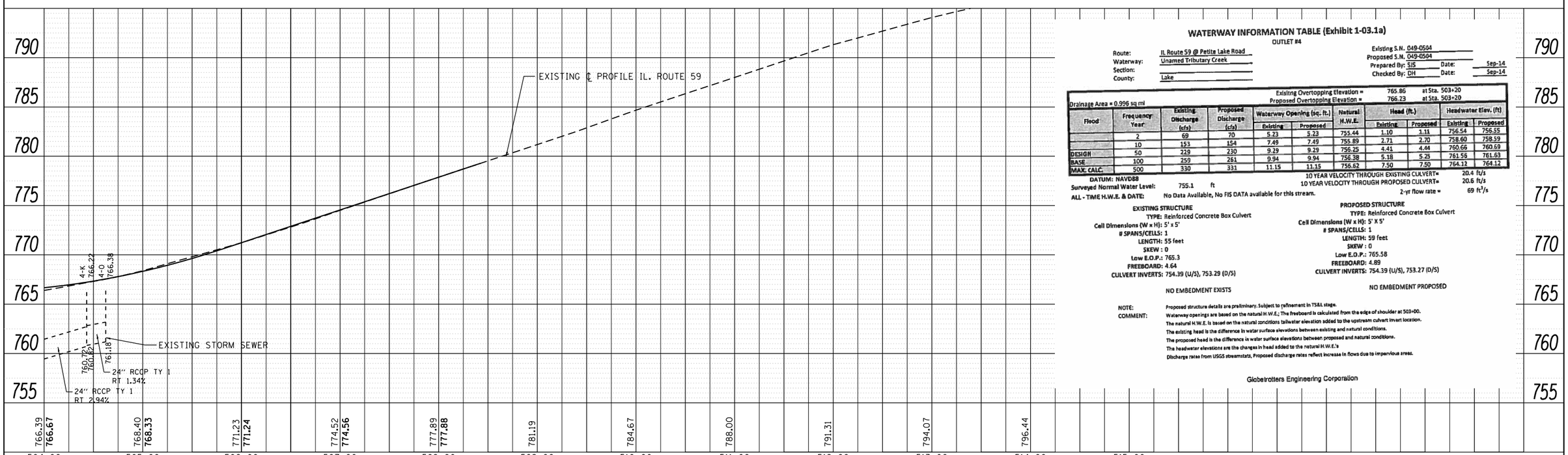
						104	105N-4	LAKE	113	47
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -					CONTRACT NO. 60W06		
	PLOT DATE = 8/19/2016	DATE -	REVISED -		SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.		ILLINOIS	FED. AID PROJECT		





WATERWAY INFORMATION TABLE (Exhibit 1-03.1a)

OUTLET #4

Route: IL Route 59 @ Petite Lake Road
Waterway: Unamed Tributary Creek
Section:
County: Lake

Existing S.N. 049-0504
Proposed S.N. 049-0504
Prepared By: SJS Date: Sep-14
Checked By: DH Date: Sep-14

Drainage Area = 0.996 sq mi

Existing Overtopping Elevation = 765.86 at Sta. 503+20
Proposed Overtopping Elevation = 766.23 at Sta. 503+20

Flood	Frequency Year	Existing Discharge (cfs)	Proposed Discharge (cfs)	Waterway Opening (sq. ft.)		Natural H.W.E.	Head (ft.)		Headwater Elev. (ft)	
				Existing	Proposed		Existing	Proposed	Existing	Proposed
	2	69	70	5.23	5.23	755.44	1.10	1.11	756.54	756.55
	10	153	154	7.49	7.49	755.89	2.71	2.70	758.60	758.59
DESIGN	50	229	230	9.29	9.29	756.25	4.41	4.44	760.66	760.69
BASE	100	259	261	9.94	9.94	756.38	5.18	5.25	761.56	761.63
MAX. CALC.	500	330	331	11.15	11.15	756.62	7.50	7.50	764.12	764.12

DATUM: NAVD88

10 YEAR VELOCITY THROUGH EXISTING CULVERT= 20.4 ft/s
10 YEAR VELOCITY THROUGH PROPOSED CULVERT= 20.6 ft/s

Surveyed Normal Water Level: 755.1 ft

2-yr flow rate = 69 ft^3/s

ALL - TIME H.W.E. & DATE: No Data Available, No FIS DATA available for this stream.

EXISTING STRUCTURE
TYPE: Reinforced Concrete Box Culvert
Cell Dimensions (W x H): 5' x 5'
SPANS/CELLS: 1
LENGTH: 55 feet
SKEW : 0
Low E.O.P.: 765.3
FREEBOARD: 4.64
CULVERT INVERTS: 754.39 (U/S), 753.29 (D/S)

NO EMBEDMENT EXISTS

PROPOSED STRUCTURE
TYPE: Reinforced Concrete Box Culvert
Cell Dimensions (W x H): 5' X 5'
SPANS/CELLS: 1
LENGTH: 59 feet
SKEW : 0
Low E.O.P.: 765.58
FREEBOARD: 4.89
CULVERT INVERTS: 754.39 (U/S), 753.27 (D/S)

NO EMBEDMENT PROPOSED

NOTE:
COMMENT:
Proposed structure details are preliminary. Subject to refinement in TS&L stage.
Waterway openings are based on the natural H.W.E.; The freeboard is calculated from the edge of shoulder at 503+00.
The natural H.W.E. is based on the natural conditions tailwater elevation added to the upstream culvert invert location.
The existing head is the difference in water surface elevations between existing and natural conditions.
The proposed head is the difference in water surface elevations between proposed and natural conditions.
The headwater elevations are the changes in head added to the natural H.W.E.'s
Discharge rates from USGS streamstats. Proposed discharge rates reflect increase in flows due to impervious areas.

Globetrotters Engineering Corporation

PLAN	BY	DATE
SURVEYED		
PLOTTED		
ALIGNMENT CHECKED		
RT. OF WAY CHECKED		
CADD FILE NAME		
NOTE BOOK NO.		

PROFILE	BY	DATE
SURVEYED		
PLOTTED		
GRADES CHECKED		
B.M. NOTED		
STRUCTURE NOTATIONS CHKD		
NOTE BOOK NO.		

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Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

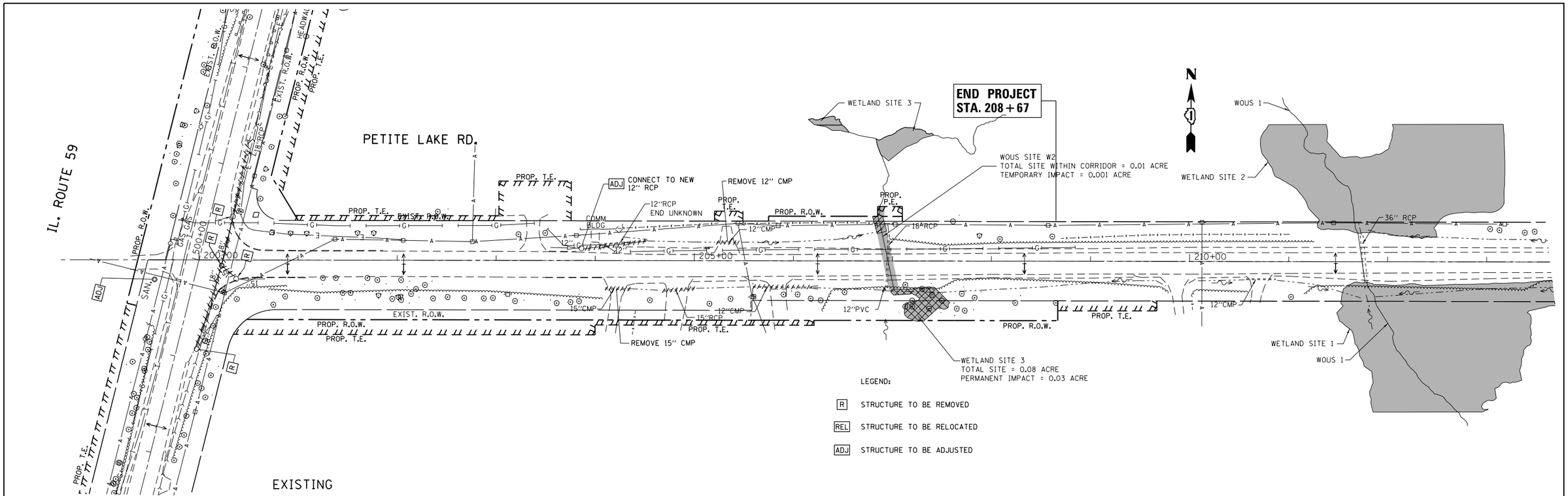
DRAINAGE PROFILE
IL. ROUTE 59 AT PETITE LAKE RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	48

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



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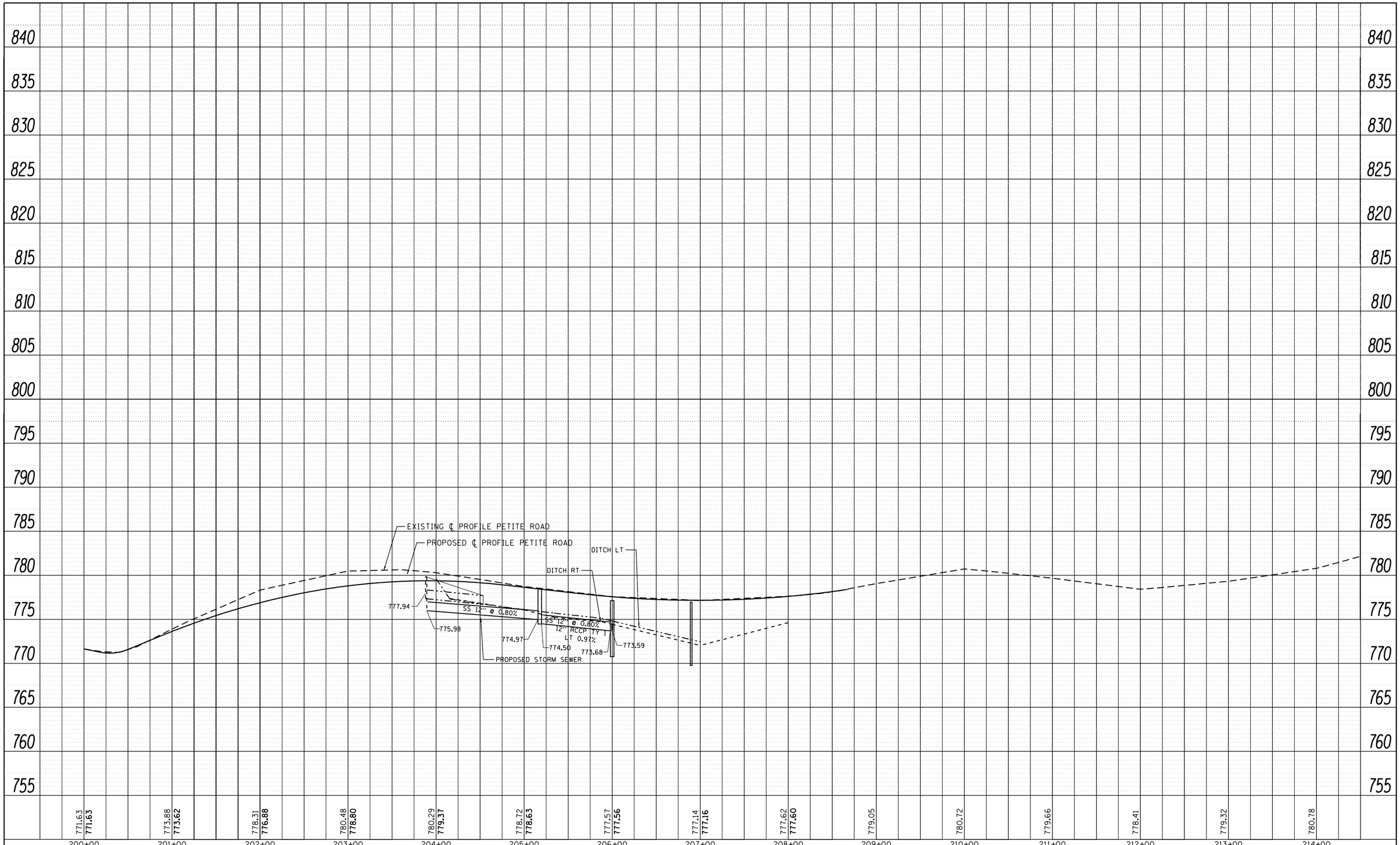
							ILLINOIS	FED. AID PROJECT	



PLAN
NOTE BOOK NO.
SURVEYED
PLOTTED
ALIGNMENT CHECKED
RT. OF WAY CHECKED
CADD FILE NAME
BY
DATE

PROFILE
NOTE BOOK NO.
SURVEYED
PLOTTED
GRADES CHECKED
B.M. NOTED
STRUCTURE NOTAT'NS CHKD
BY
DATE

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Default	PLOT DATE = 8/29/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRAINAGE PROFILE
IL. ROUTE 59 AT PETITE LAKE RD.

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	50

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT

STRUCTURES:

1
PRC FL-END SEC, 12 IN
STA 489+66.61, 69.98 FT (LT)
T.G.
INV. 768.00
T.B.

FLAT TOP 2
MH-T-A, 5 FT. DIA. W/ T-8 GRATE
RESTRICTOR PLATE
STA. 489+77.24, 31.17 FT. (LT)
T.G. 770.95
INV. 768.20 (NE)
INV. 768.20 (SW)
T.B.

FLAT TOP 3
MH, T-A, 5 FT. DIA. W/ T-8 GRATE
STA. 489+89.63, 31.02 FT. (LT)
T.G. 771.14
INV. 768.23 (NE)
INV. 768.23 (SW)
T.B.

FLAT TOP 4
MH, T-A. 5 FT. DIA. W/ T-8 GRATE
STA. 490+71.93, 30.26 FT. (LT)
T.G. 772.20
INV. 768.39 (NE)
INV. 768.39 (SW)
T.B.

5
CB, T-A, 5 FT DIA. W/ T-8 GRATE
STA. 492+58.90, 32.92 FT (LT)
T.G. 773.79
INV. 768.76
T.B.

6
CONC-END SEC. 1:4, 24 IN
STA. 495+71.99, 41.79 FT. (LT)
T.G.
INV. 771.42
T.B.

7
CONC-END SEC. 1:4, 24 IN
STA. 495+72.91, 38.04 FT (RT)
T.G.
INV. 771.50
T.B.

8
CB, T-A. 4 FT. DIA. W/ T-24 F&G
STA. 499+56.59, 27.00 FT. (RT)
T.G. 772.13
INV. 766.76 (NE)
T.B.

9
CB, T-C, W/ T-24 F&G
STA. 500+72.67, 31.84 FT. (RT)
T.G. 769.15
INV. 765.62 (SE)
T.B.

FLAT TOP 10
MH, T-A. 5 FT. DIA. W/ T-8 GRATE
STA. 500+73.29, 38.66 FT. (RT)
T.G. 769.20
INV. 764.98 (NE)
INV. 765.60 (NW)
INV. 765.66 (SE)
INV. 765.48 (SW)
T.B.

11
PRC FL-END SEC. 15 IN
STA. 501+32.65, 37.88 FT. (RT)
T.G.
INV. 764.32
T.B.

12
MH, T-A. 4 FT. DIA. W/ T-24 F&G
STA. 503+08.66, 27.00 FT. (LT)
T.G. 765.57
INV. 756.69 (SW)
INV. 759.86 (NE)
T.B.

13
CB, T-A. 4 FT. DIA. W/ T-24 F&G
STA. 503+18.66, 27.00 FT. (LT)
T.G. 765.56
INV. 759.96 (SW)
INV. 760.46 (NE)
T.B.

14
CB, T-A, 4 FT. DIA. W/ T-24 F&G
STA. 503+28.66, 27.00 FT. (LT)
T.G. 765.57
INV. 760.54 (SW)
T.B.

15
CB, T-A 4 FT. DIA. W/ T-8 GRATE (EXIST)
STA. 504+56.98, 45.56 FT. (LT)
T.G. 765.21 (EXIST)
INV. 762.11 (NW) EXIST
INV. 762.61 (NE)
INV. 761.86 (SW)
T.B.

16
PRC FL-END SEC. 15 IN
STA. 503+47, 50.70 FT (LT)
T.G.
INV. 756.06
T.B.

A 16
PRC FL-END SEC. 15 IN
STA. 504+91.49, 40.73 FT (LT)
T.G.
INV. 764.22
T.B.

17
CB, T-A, 5 FT. DIA. W/ T-24 F&G
STA. 200+34.81, 27.25 FT. (RT)
T.G. 770.60
INV. 766.31 (NE)
INV. 766.68 (NE)
INV. 766.47 (SE)
INV. 766.50 (SW)
T.B.

18
CB, T-C. W/ T-8 GRATE
STA. 200+40.83, 49.27 FT. (RT)
T.G. 772.06
INV. 766.56 (NW)
T.B.

FLAT TOP 19
CB, T-A. 4 FT. DIA. W/ T-24 F&G
STA. 200+44.40, 25.00 FT. (RT)
T.G. 770.87
INV. 766.74 (NW)
T.B.

20
CB, T-C. W/ T-24 F&G
STA. 201+03.71, 20.98 FT. (LT)
T.G. 773.48
INV. 768.10 (N)
T.B.

21
MH, T-A. 4 FT. DIA. W/ T-8 GRATE
STA. 201+03.71, 27.01 FT. (LT)
T.G. 773.32
INV. 766.25 (NW)
INV. 767.44 (S)
T.B.

22
CB, T-A. 4 FT. DIA. (EXIST) ADJ
W/NEW T-1F CL
STA. 203+86.12, 12.23 FT. (LT)
T.G. 779.13
INV. 777.94 (W) EXIST
INV. 776.19 (N)
T.B.

FLAT TOP 23
CB, T-A. 4 FT. DIA. W/ T-24 F&G
STA. 203+86.12, 20.98 FT. (LT)
T.G. 778.97
INV. 775.98 (E)
INV. 776.15 (S)
T.B.

FLAT TOP 24
CB, T-A. 4 FT. DIA. W/ T-24 F&G
STA. 205+15.69, 19.47 FT. (LT)
T.G. 778.09
INV. 774.97 (W)
INV. 774.50 (E)
INV. 774.75 (S)
T.B.

FLAT TOP A 24
CB, T-A. 4 FT. DIA. W/ T-24 F&G
STA. 206+00, 16.20 FT. (LT)
T.G. 777.13
INV. 773.68 (W)
INV. 773.59 (N)
T.B.

25
CB, T-C. W/ T-24 F&G
STA. 205+15.59, 19.49 FT. (RT)
T.G. 778.09
INV. 775.10 (N) EXIST
T.B.

26
CB, T-A. 5 FT. DIA. W/ T-24 F&G
STA. 206+95.86, 15.46 FT. (LT)
T.G. 776.85
INV. 770.65 (N) EXIST
INV. 770.75 (S) EXIST
INV. 773.31 (E)
T.B.

27
CB, T-A. 5 FT. DIA. W/ T-24 F&G
STA. 206+94.60, 15.51 FT. (RT)
T.G. 776.87
INV. 771.34 (N) EXIST
INV. 771.45 (S) EXIST
INV. 773.30 (E)
T.B.

28
CB, T-C. W/ T-24 F&G
STA. 207+04.90, 15.31 FT. (RT)
T.G. 776.86
INV. 773.40 (W)
T.B.

29
CB, T-C. W/ T-24 F&G
STA. 207+06.36, 15.26 FT. (LT)
T.G. 776.87
INV. 773.42 (W)
T.B.

30
CONC-END SEC. 1:4, 24 IN
STA. 206+92.94, 56.30 FT. (RT)
T.G.
INV. 772.32
T.B.

31
CONC-END SEC. 1:4, 24 IN
STA. 206+97.44, 44.35 FT. (LT)
T.G.
INV. 770.05
T.B.

32
PRC FL-END SEC, 12 IN
STA 206+00, 33.65 FT (LT)
T.G.
INV. 773.45
T.B.

STORM SEWERS:

(1)
12" DIA. S.S. CLASS A, T-1, 32.36 FT.
T.G.
INV.
T.B.

(2)
S.S., T-1, REINF. CONC. ELLIPTICAL PIPE
SPAN 23, RISE 14, 7.89 FT
T.G.
INV.
T.B.

(3)
S.S., T-1, REINF. CONC. ELLIPTICAL PIPE
SPAN 23, RISE 14, 77.82 FT
T.G.
INV.
T.B.

(4)
S.S., T-1, REINF. CONC. ELLIPTICAL PIPE
SPAN 23, RISE 14, 182.51 FT
T.G.
INV.
T.B.

(5)
24" DIA. S.S. CLASS A, T-1, 13.57 FT. (EXT.)
T.G.
INV.
T.B. 2.40 CU YD

(6)
24" DIA. S.S. CLASS A, T-1, 10.29 FT. (EXT.)
T.G.
INV.
T.B. 2.78 CU YD

(7)
12" DIA. S.S. CLASS A, T-1, 23.69 FT.
T.G.
INV.
T.B. 5.59 CU YD

(8)
12" DIA. S.S. CLASS A, T-1, 2.79 FT.
T.G.
INV.
T.B.

(9)
15" DIA. S.S. CLASS A, T-1, 117.33 FT.
T.G.
INV.
T.B.

(10)
15" DIA. S.S. CLASS A, T-1, 16.53 FT.
T.G.
INV.
T.B. 17.85 CU YD

(11)
12" DIA. S.S. CLASS A, T-1, 6.53 FT.
T.G.
INV.
T.B. 2.23 CU YD

(12)
12" DIA. S.S. CLASS A, T-1, 6.53 FT.
T.G.
INV.
T.B. 1.68 CU YD

(12 A)
15" DIA. S.S. CLASS A, T-1, 103.56 FT.
T.G.
INV.
T.B.

(12 B)
15" DIA. S.S. CLASS A, T-1, 26.25 FT.
T.G.
INV.
T.B. 3.18 CU YD

(13)
12" DIA. S.S. CLASS A, T-1, 18.00 FT.
T.G.
INV.
T.B. 6.14 CU YD

(14)
15" DIA. S.S. CLASS A, T-1, 87.19 FT.
T.G.
INV.
T.B. 29.73 CU YD

(15)
12" DIA. S.S. CLASS A, T-1, 5.67 FT.
T.G.
INV.
T.B. 0.50 CU YD

(16)
12" DIA. S.S. CLASS A, T-1, 8.10 FT.
T.G.
INV.
T.B. 2.76 CU YD

(17)
12" DIA. S.S. CLASS A, T-1, 52.13 FT.
T.G.
INV.
T.B.

(18)
12" DIA. S.S. CLASS A, T-1, 4.00 FT.
T.G.
INV.
T.B. 0.50 CU YD

(19)
12" DIA. S.S. CLASS A, T-1, 125.77 FT.
T.G.
INV.
T.B. 16.60 CU YD

(20)
12" DIA. S.S. CLASS A, T-1, 35.47 FT.
T.G.
INV.
T.B. 4.68 CU YD

(21)
12" DIA. S.S. CLASS A, T-1, 80.15 FT.
T.G.
INV.
T.B. 13.39 CU YD

(21 A)
12" DIA. S.S. CLASS A, T-1, 14.55 FT.
T.G.
INV.
T.B. 2.43 CU YD

(22)
24" DIA. S.S. CLASS A, T-1, 21.19 FT.
T.G.
INV.
T.B. 29.38 CU YD

(23)
12" DIA. S.S. CLASS A, T-1, 7.57 FT.
T.G.
INV.
T.B. 1.00 CU YD

(24)
24" DIA. S.S. CLASS A, T-1, 33.39 FT.
T.G.
INV.
T.B. 6.82 CU YD

(24 A)
24" DIA. S.S. CLASS A, T-1, 25.76 FT.
T.G.
INV.
T.B. 9.10 CU YD

(25)
12" DIA. S.S. CLASS A, T-1, 7.02 FT.
T.G.
INV.
T.B. 1.00 CU YD

PIPE CULVERTS:

(26)
15" DIA. PIPE CULVERT, CLASS A, T-1
WITH PRC FL-END SEC, 25.36 FT.
STA. 493+10.27, 33.74 FT (RT)
T.G.
INV. 772.69 (NE) STA 493+20.40, 34.13 (RT)
INV. 772.35 (SW) STA 492+98.40, 33.83 (RT)
T.B. 3.44 CU YD

(27)
15" DIA. PIPE CULVERT, CLASS A, T-1
WITH PRC FL-END SEC, 30.79 FT.
STA. 494+89.02, 34.07 FT (RT)
T.G.
INV. 772.73 (NE) 495+02.40, 34.13 (RT)
INV. 771.99 (SW) 494+78.61, 34.09 (RT)
T.B. 4.35 CU YD

(27 A)
15" DIA. PIPE CULVERT, CLASS A, T-1
WITH PRC FL-END SEC, 30.79 FT.
STA. 496+73.18, 33.00 FT (RT)
T.G.
INV. 771.76(NE) 496+93.05, 33.00(RT)
INV. 771.90 (SW) 496+53.30, 33.00 (RT)
T.B. 4.35 CU YD

(28)
15" DIA. PIPE CULVERT, CLASS A, T-1
WITH PRC FL-END SEC, 27.96 FT.
STA. 499+84.35, 47.96 FT (LT)
T.G.
INV. 768.62 (NE) STA 500+07.71, 49.14 (RT)
INV. 768.40 (SW) STA 499+59.32, 46.66 (RT)
T.B. 6.60 CU YD

(29)
15" DIA. PIPE CULVERT, CLASS A, T-1
WITH PRC FL-END SEC, 25.16 FT.
STA. 505+41.55, 37.35 FT (LT)
T.G.
INV. 765.92 (NE) STA 505+49.79, 37.36 (LT)
INV. 765.62 (SW) STA 505+33.79, 37.34 (LT)
T.B. 3.74 CU YD

(30)
15" DIA. PIPE CULVERT, CLASS A, T-1
WITH PRC FL-END SEC, 28.65 FT.
STA. 505+86.57, 36.76 FT (LT)
T.G.
INV. 767.22 (NE) 505+96.92, 36.52 (LT)
INV. 766.96 (SW) 505+76.94, 37.01 (LT)
T.B. 4.00 CU YD

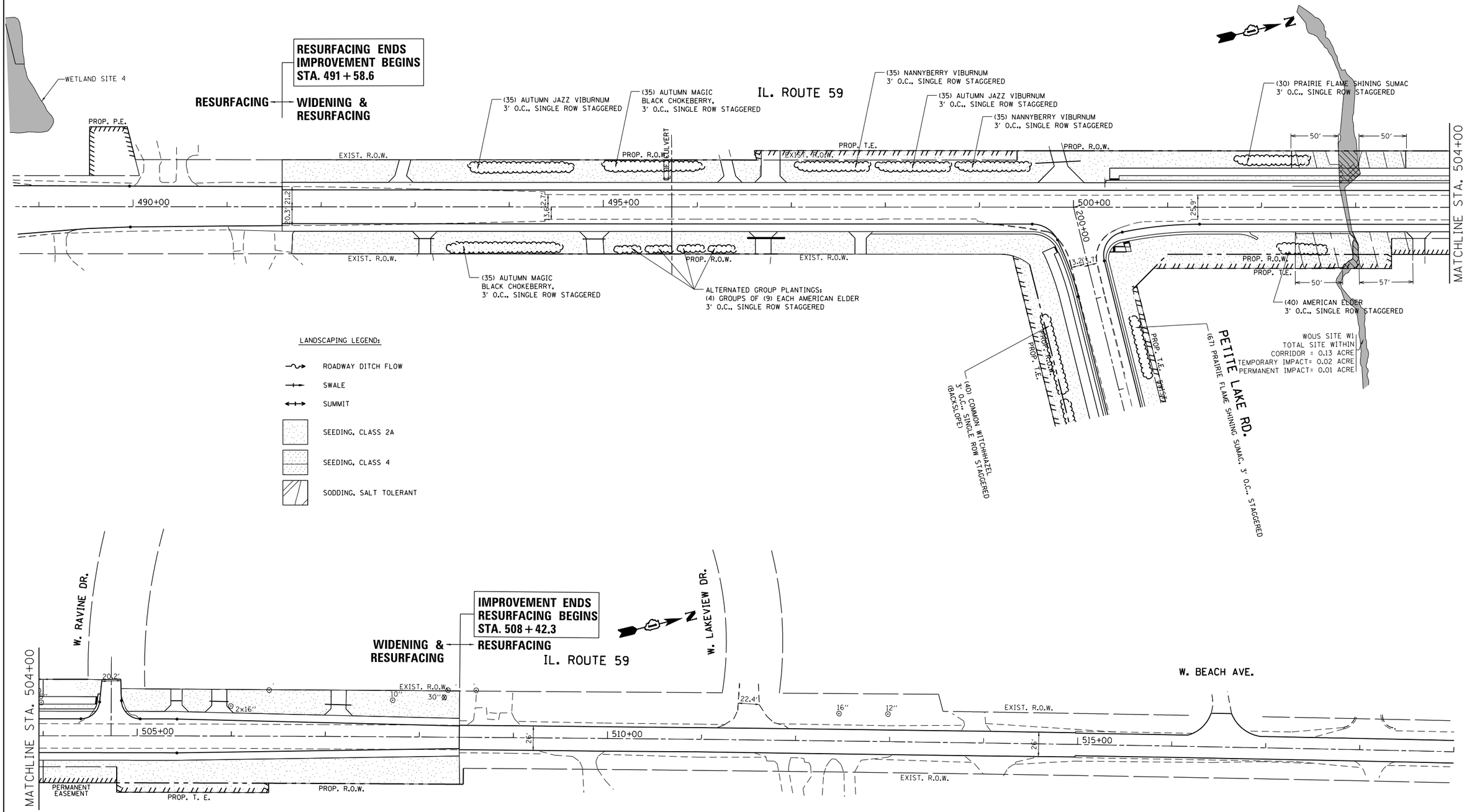
(31)
15" DIA. PIPE CULVERT, CLASS A, T-1
WITH PRC FL-END SEC, 28.66 FT.
STA. 507+13.88, 33.83 FT (LT)
T.G.
INV. 771.13 (NE) 507+27.70, 33.52 (LT)
INV. 770.85 (SW) 506+99.31, 34.16 (LT)
T.B. 4.00 CU YD

(32)
15" DIA. PIPE CULVERT, CLASS A, T-1
WITH PRC FL-END SEC, 24.91 FT.
STA. 204+20, 53.67 FT (RT)
T.G.
INV. 776.45 (E) 204+30.86, 53.61 (RT)
INV. 776.70 (W) 204+10.20, 53.73 (RT)
T.B. 3.37 CU YD

(33)
15" DIA. PIPE CULVERT, CLASS A, T-1
WITH PRC FL-END SEC, 32.28 FT.
STA. 204+80, 53.20 FT (RT)
T.G.
INV. 775.25 (E) 204+97.04, 53.10 (RT)
INV. 775.58 (W) 204+67.03, 53.29 (RT)
T.B. 4.71 CU YD

(34)
15" DIA. PIPE CULVERT, CLASS A, T-1
WITH PRC FL-END SEC, 24.75 FT.
STA. 205+32.46, 31.87 FT (LT)
T.G.
INV. 774.41 (E) 205+44.32, 31.86 (LT)
INV. 774.66 (W) 205+24.04, 31.86 (LT)
T.B. 3.41 CU YD

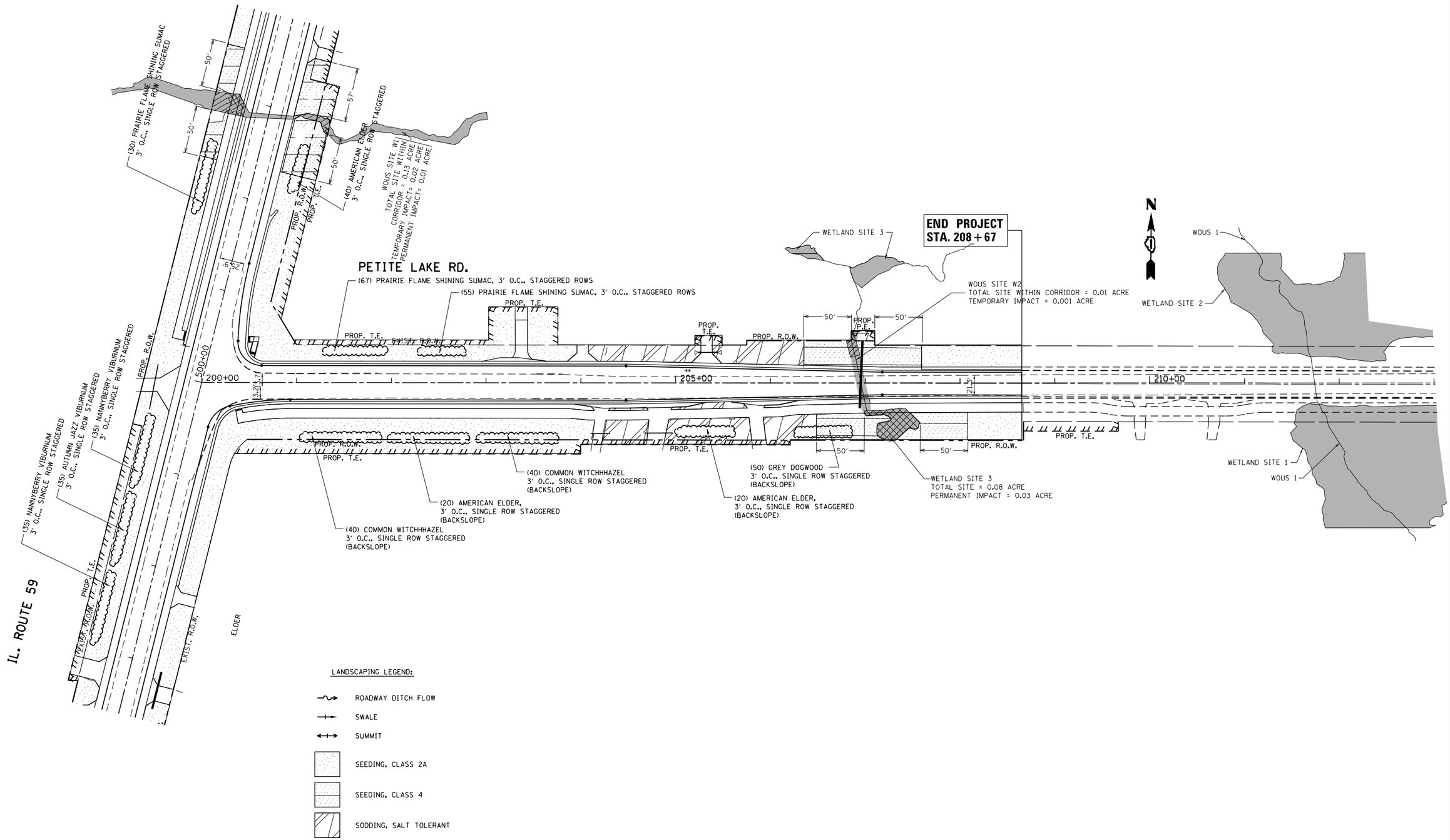
(35)
15" DIA. PIPE CULVERT, CLASS A, T-1
WITH PRC FL-END SEC, 27.64 FT.
STA. 205+86.59, 52.14 FT (RT)
T.G.
INV. 774.96 (E) 205+98.81, 52.10 (RT)
INV. 774.68 (W) 205+73.14, 52.14 (RT)
T.B. 3.83 CU YD



SELECTIVE CLEARING TO BE USED WITHIN TEMP. & PERM. EASMENTS.
CALL MELISSA DEL ROSARIO AT (847)-705-4171 72 HRS

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P145409\DraftData\Design\P145409-sht-cover.dgn	USER NAME = ledezmarm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED LANDSCAPING PLAN IL. ROUTE 59 AT PETITE LAKE RD
		DRAWN -	REVISED -		
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -		
Default	PLOT DATE = 9/19/2016	DATE -	REVISED -		SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	53
		CONTRACT NO. 60W06		
	ILLINOIS	FED. AID PROJECT		



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Default	PLOT DATE = 9/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

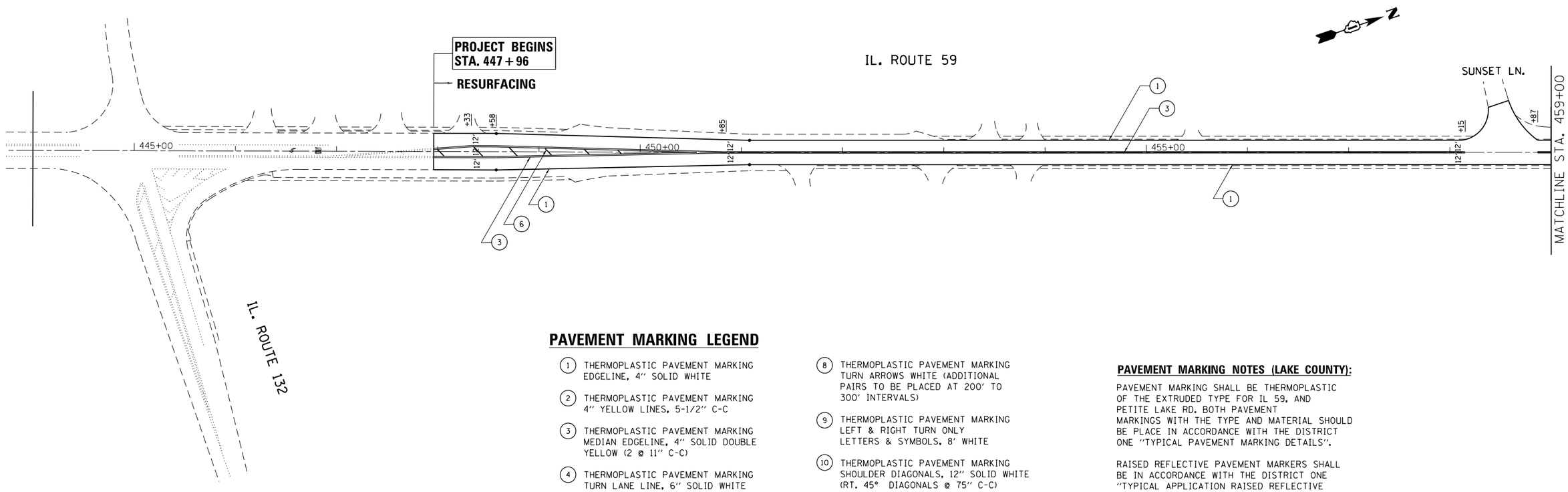
PROPOSED LANDSCAPING PLAN
IL. ROUTE 59 AT PETITE LAKE RD

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	54

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



PAVEMENT MARKING LEGEND

(1) THERMOPLASTIC PAVEMENT MARKING EDGELINE, 4" SOLID WHITE

(2) THERMOPLASTIC PAVEMENT MARKING 4" YELLOW LINES, 5-1/2" C-C

(3) THERMOPLASTIC PAVEMENT MARKING MEDIAN EDGELINE, 4" SOLID DOUBLE YELLOW (2 @ 11" C-C)

(4) THERMOPLASTIC PAVEMENT MARKING TURN LANE LINE, 6" SOLID WHITE

✳ (5) THERMOPLASTIC PAVEMENT MARKING CROSSWALK - 12" WIDTH 2" SPACING SOLID WHITE

(6) THERMOPLASTIC PAVEMENT MARKING 12" SOLID YELLOW (RT. 45° DIAGONALS @ 20' C-C)

(7) THERMOPLASTIC PAVEMENT MARKING STOP BAR, 24" SOLID WHITE

(8) THERMOPLASTIC PAVEMENT MARKING TURN ARROWS WHITE (ADDITIONAL PAIRS TO BE PLACED AT 200' TO 300' INTERVALS)

(9) THERMOPLASTIC PAVEMENT MARKING LEFT & RIGHT TURN ONLY LETTERS & SYMBOLS, 8' WHITE

(10) THERMOPLASTIC PAVEMENT MARKING SHOULDER DIAGONALS, 12" SOLID WHITE (RT. 45° DIAGONALS @ 75" C-C)

(11) THERMOPLASTIC PAVEMENT MARKING DOTTED LINE, 6" SOLID WHITE (2' LINE 6' SPACE)

✳ (12) THERMOPLASTIC PAVEMENT MARKING EDGE LINE, 5" SOLID WHITE (PETITE LAKE ROAD ONLY)

PAVEMENT MARKING NOTES (LAKE COUNTY):

PAVEMENT MARKING SHALL BE THERMOPLASTIC OF THE EXTRUDED TYPE FOR IL 59, AND PETITE LAKE RD. BOTH PAVEMENT MARKINGS WITH THE TYPE AND MATERIAL SHOULD BE PLACE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL PAVEMENT MARKING DETAILS".

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS".

THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, TRAFFIC FIELD ENGINEER AT (847) 715-8419 A MINIMUM OF TWO WEEKS PRIOR TO THE PLACEMENT OF ANY PERMANENT PAVEMENT MARKINGS.

✳ REFER TO LAKE COUNTY PAVEMENT MARKING STANDARDS AND DETAILS

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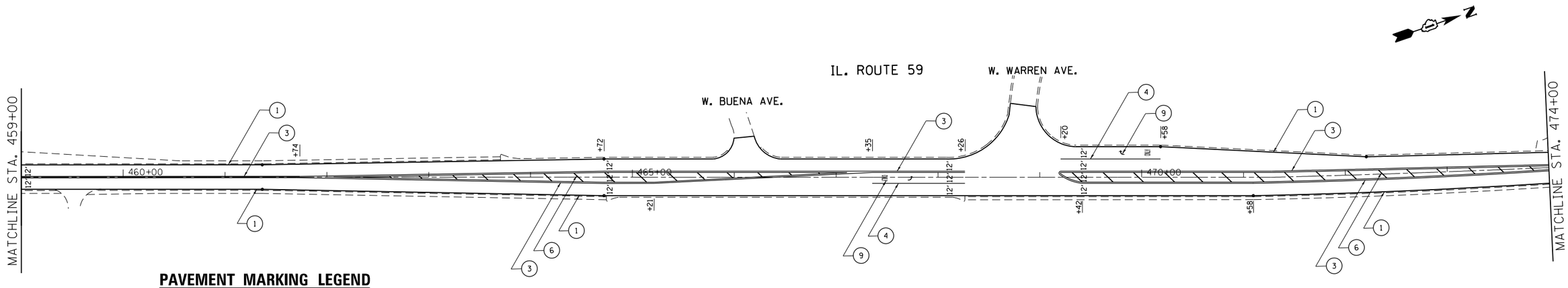
		DRAWN -	REVISED -
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED PAVEMENT MARKING PLAN
IL. ROUTE 59 AT PETITE LAKE RD

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	55
		CONTRACT NO. 60W06		
	ILLINOIS	FED. AID PROJECT		



PAVEMENT MARKING LEGEND

1. THERMOPLASTIC PAVEMENT MARKING EDGELINE, 4" SOLID WHITE
2. THERMOPLASTIC PAVEMENT MARKING 4" YELLOW LINES, 5-1/2" C-C
3. THERMOPLASTIC PAVEMENT MARKING MEDIAN EDGELINE, 4" SOLID DOUBLE YELLOW (2 @ 11" C-C)
4. THERMOPLASTIC PAVEMENT MARKING TURN LANE LINE, 6" SOLID WHITE
5. ✳ THERMOPLASTIC PAVEMENT MARKING CROSSWALK - 12" WIDTH 2" SPACING SOLID WHITE
6. THERMOPLASTIC PAVEMENT MARKING 12" SOLID YELLOW (RT. 45° DIAGONALS @ 20' C-C)
7. THERMOPLASTIC PAVEMENT MARKING STOP BAR, 24" SOLID WHITE
8. THERMOPLASTIC PAVEMENT MARKING TURN ARROWS WHITE (ADDITIONAL PAIRS TO BE PLACED AT 200' TO 300' INTERVALS)
9. THERMOPLASTIC PAVEMENT MARKING LEFT & RIGHT TURN ONLY LETTERS & SYMBOLS, 8' WHITE
10. THERMOPLASTIC PAVEMENT MARKING SHOULDER DIAGONALS, 12" SOLID WHITE (RT. 45° DIAGONALS @ 75" C-C)
11. THERMOPLASTIC PAVEMENT MARKING DOTTED LINE, 6" SOLID WHITE (2' LINE 6' SPACE)
12. ✳ THERMOPLASTIC PAVEMENT MARKING EDGE LINE, 5" SOLID WHITE (PETITE LAKE ROAD ONLY)

PAVEMENT MARKING NOTES (LAKE COUNTY):

PAVEMENT MARKING SHALL BE THERMOPLASTIC OF THE EXTRUDED TYPE FOR IL 59, AND PETITE LAKE RD. BOTH PAVEMENT MARKINGS WITH THE TYPE AND MATERIAL SHOULD BE PLACE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL PAVEMENT MARKING DETAILS".

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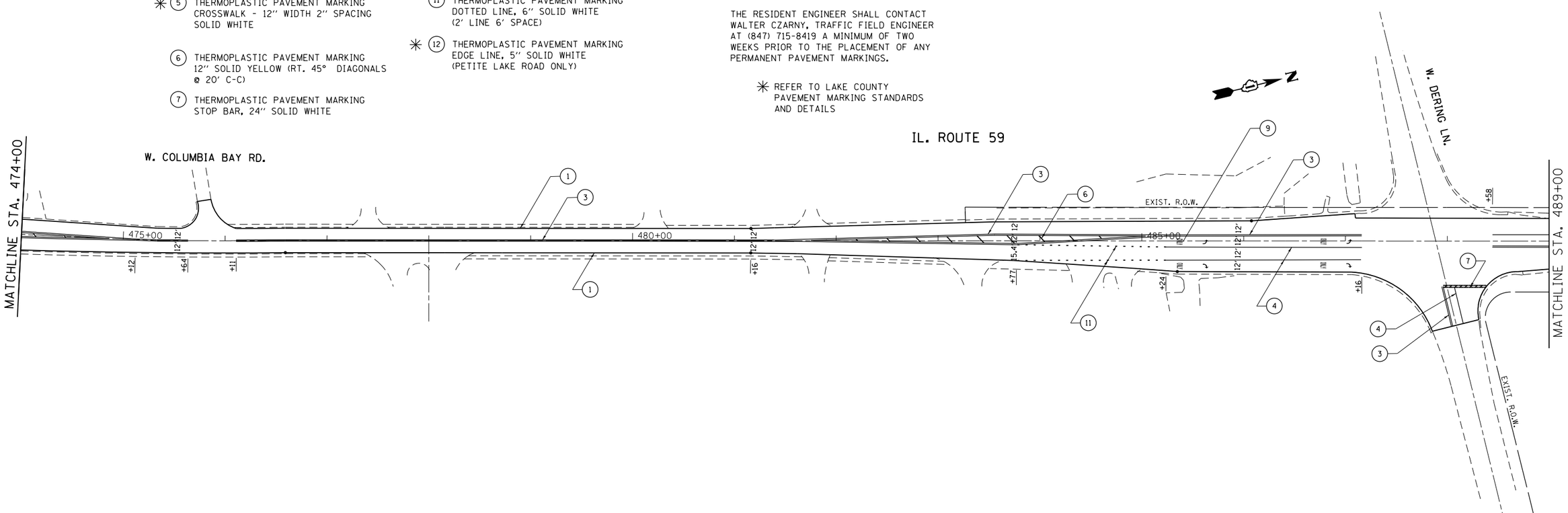

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			REVISED -
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Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

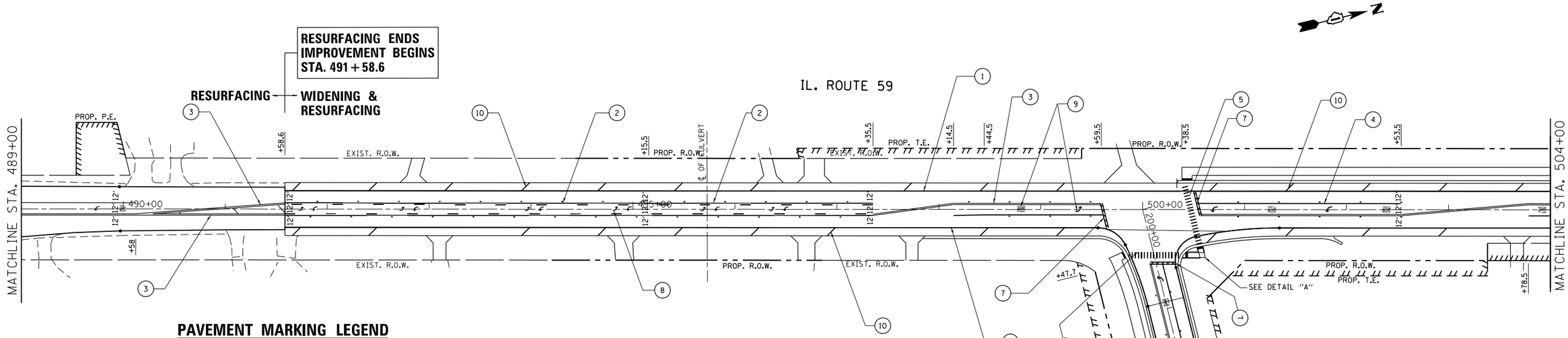
PROPOSED PAVEMENT MARKING PLAN
IL. ROUTE 59 AT PETITE LAKE RD

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	56

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



PAVEMENT MARKING LEGEND

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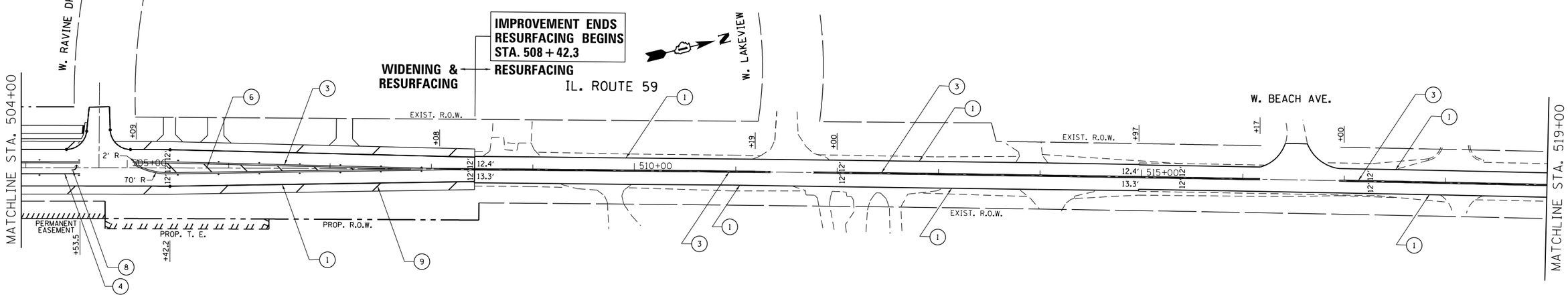

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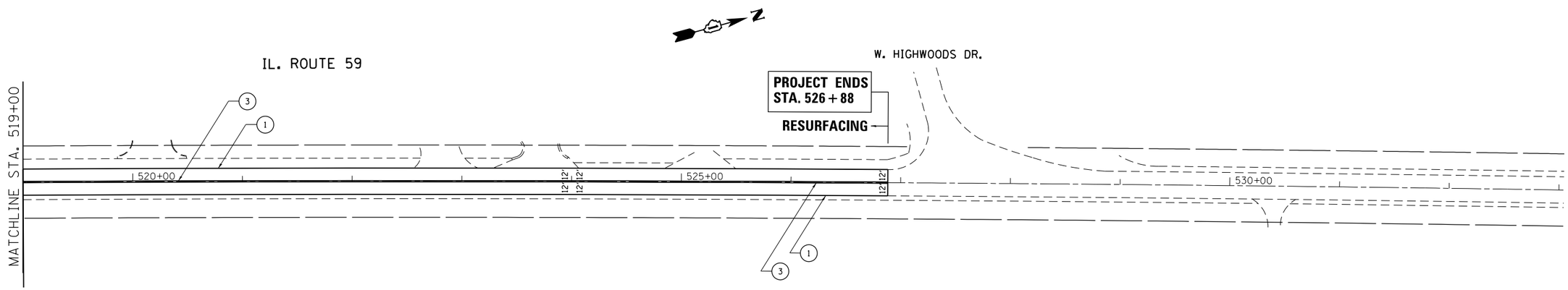
		DRAWN -	REVISED -			104	105N-4	LAKE	113	57
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -			CONTRACT NO. 60W06				
Default	PLOT DATE = 8/19/2016	DATE -	REVISED -		SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.	ILLINOIS FED. AID PROJECT				



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RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS".

THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, TRAFFIC FIELD ENGINEER AT (847) 715-8419 A MINIMUM OF TWO WEEKS PRIOR TO THE PLACEMENT OF ANY PERMANENT PAVEMENT MARKINGS.

✳ REFER TO LAKE COUNTY PAVEMENT MARKING STANDARDS AND DETAILS

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Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

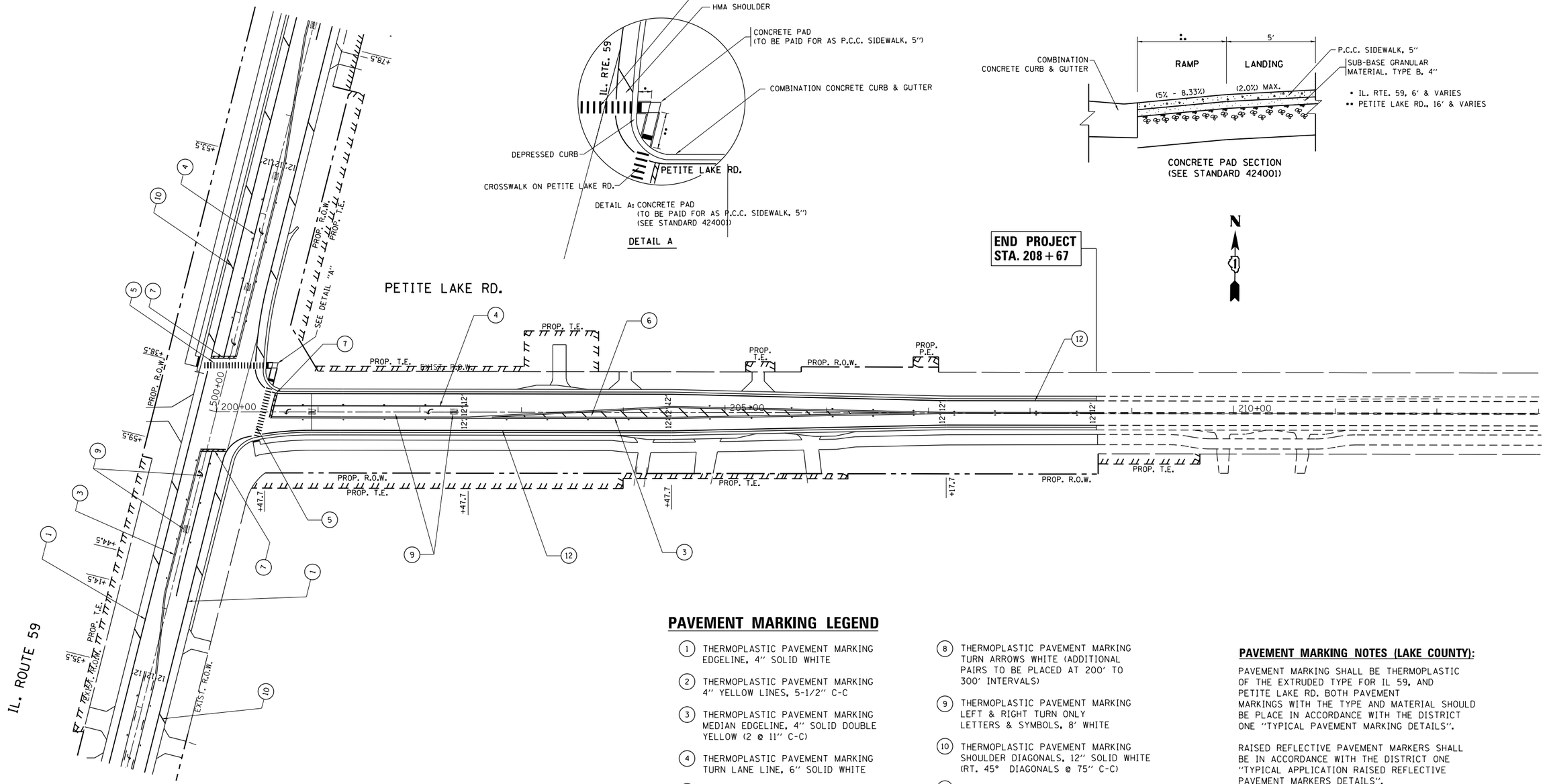
PROPOSED PAVEMENT MARKING PLAN
IL. ROUTE 59 AT PETITE LAKE RD

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	58

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



PAVEMENT MARKING LEGEND

1. THERMOPLASTIC PAVEMENT MARKING EDGELINE, 4" SOLID WHITE
2. THERMOPLASTIC PAVEMENT MARKING 4" YELLOW LINES, 5-1/2" C-C
3. THERMOPLASTIC PAVEMENT MARKING MEDIAN EDGELINE, 4" SOLID DOUBLE YELLOW (2 @ 11" C-C)
4. THERMOPLASTIC PAVEMENT MARKING TURN LANE LINE, 6" SOLID WHITE
5. ✳ THERMOPLASTIC PAVEMENT MARKING CROSSWALK - 12" WIDTH 2" SPACING SOLID WHITE
6. THERMOPLASTIC PAVEMENT MARKING 12" SOLID YELLOW (RT. 45° DIAGONALS @ 20' C-C)
7. THERMOPLASTIC PAVEMENT MARKING STOP BAR, 24" SOLID WHITE
8. THERMOPLASTIC PAVEMENT MARKING TURN ARROWS WHITE (ADDITIONAL PAIRS TO BE PLACED AT 200' TO 300' INTERVALS)
9. THERMOPLASTIC PAVEMENT MARKING LEFT & RIGHT TURN ONLY LETTERS & SYMBOLS, 8' WHITE
10. THERMOPLASTIC PAVEMENT MARKING SHOULDER DIAGONALS, 12" SOLID WHITE (RT. 45° DIAGONALS @ 75" C-C)
11. THERMOPLASTIC PAVEMENT MARKING DOTTED LINE, 6" SOLID WHITE (2' LINE 6' SPACE)
12. ✳ THERMOPLASTIC PAVEMENT MARKING EDGE LINE, 5" SOLID WHITE (PETITE LAKE ROAD ONLY)

PAVEMENT MARKING NOTES (LAKE COUNTY):

PAVEMENT MARKING SHALL BE THERMOPLASTIC OF THE EXTRUDED TYPE FOR IL 59, AND PETITE LAKE RD. BOTH PAVEMENT MARKINGS WITH THE TYPE AND MATERIAL SHOULD BE PLACE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL PAVEMENT MARKING DETAILS".

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✳ REFER TO LAKE COUNTY PAVEMENT MARKING STANDARDS AND DETAILS

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		DRAWN -	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED PAVEMENT MARKING PLAN
IL. ROUTE 59 AT PETITE LAKE RD

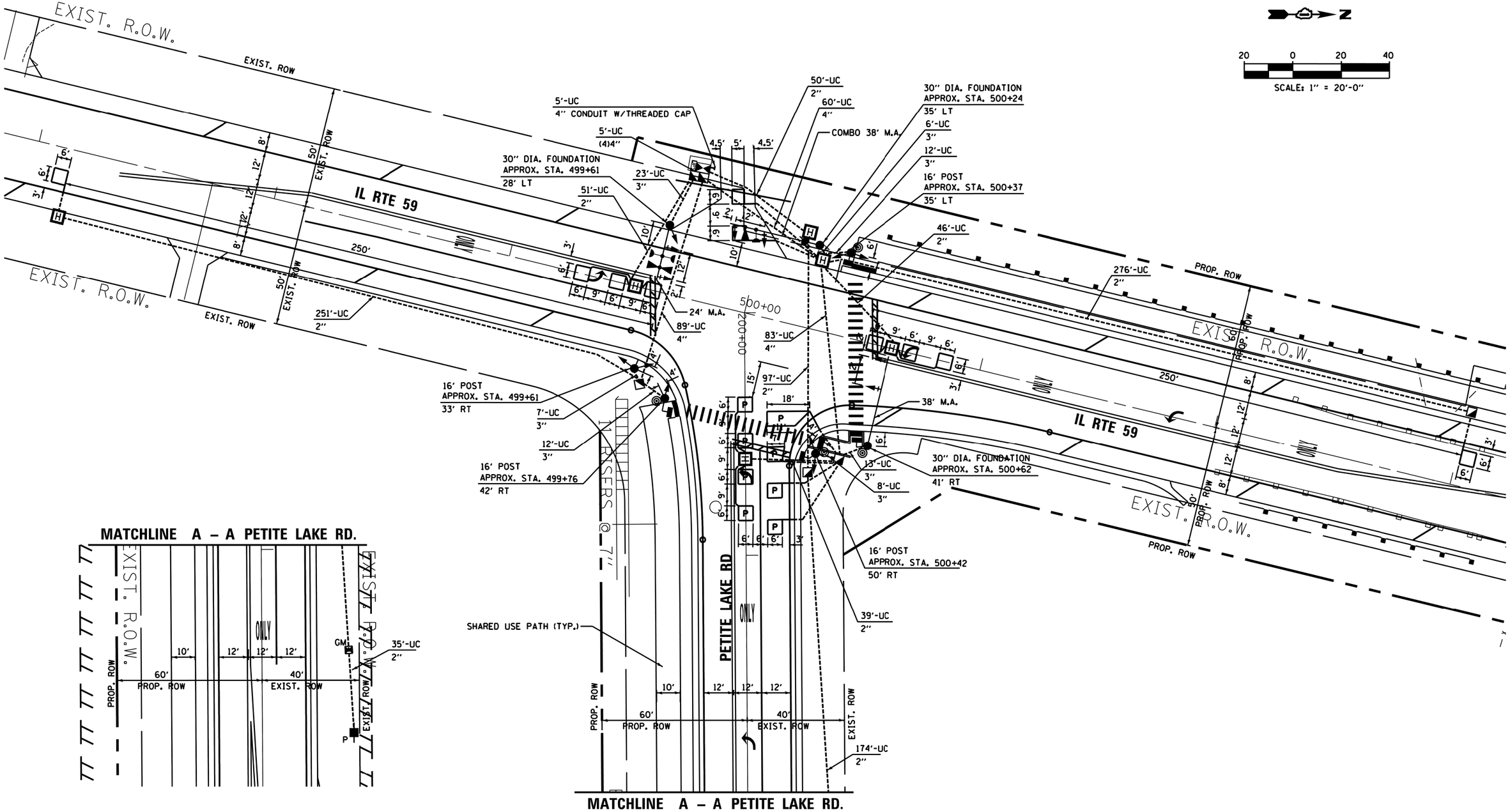
SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	59

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT

CIVIL ENGINEERING CONSULTANTS
RWA
Regina Webster & Associates, Inc.
8619 W. Bryn Mawr Ave., Suite 602
Chicago, IL 60631-3551
773-283-2600 Fax: 773-283-2602
www.RWAengineers.com



NOTES:

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.

TS 21635

FILE NAME = S:\WP\Design\JC\Design Let Projects\Consultant\60W06 - IL 59 @ Petite Lake Rd\New Roadway\NewWorking File LW Revised Submission\Revised\60W06_TS-59-PETITE.dgn	USER NAME = curryja	DESIGNED - DW	REVISED -
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	PLOT DATE = 8/10/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODERNIZATION PLAN
ILLINOIS ROUTE 59 AND PETITE LAKE ROAD

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	60

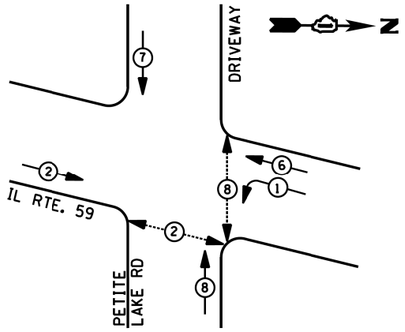
CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT

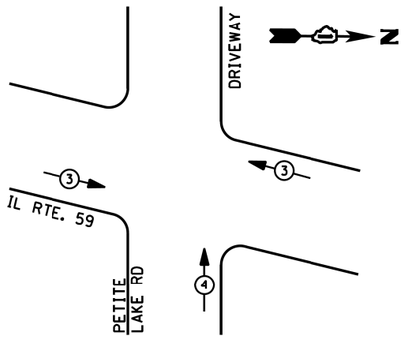
60W06 TS-59-PETITE.dgn 8/10/2016 3:45:09 PM User=curryja

CIVIL ENGINEERING CONSULTANTS
RWA
Regina Webster & Associates, Inc.
8619 W. Bryn Mawr Ave., Suite 602
Chicago, IL 60631-3551
773-283-2600 Fax: 773-283-2602
www.RWAengineers.com

CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM



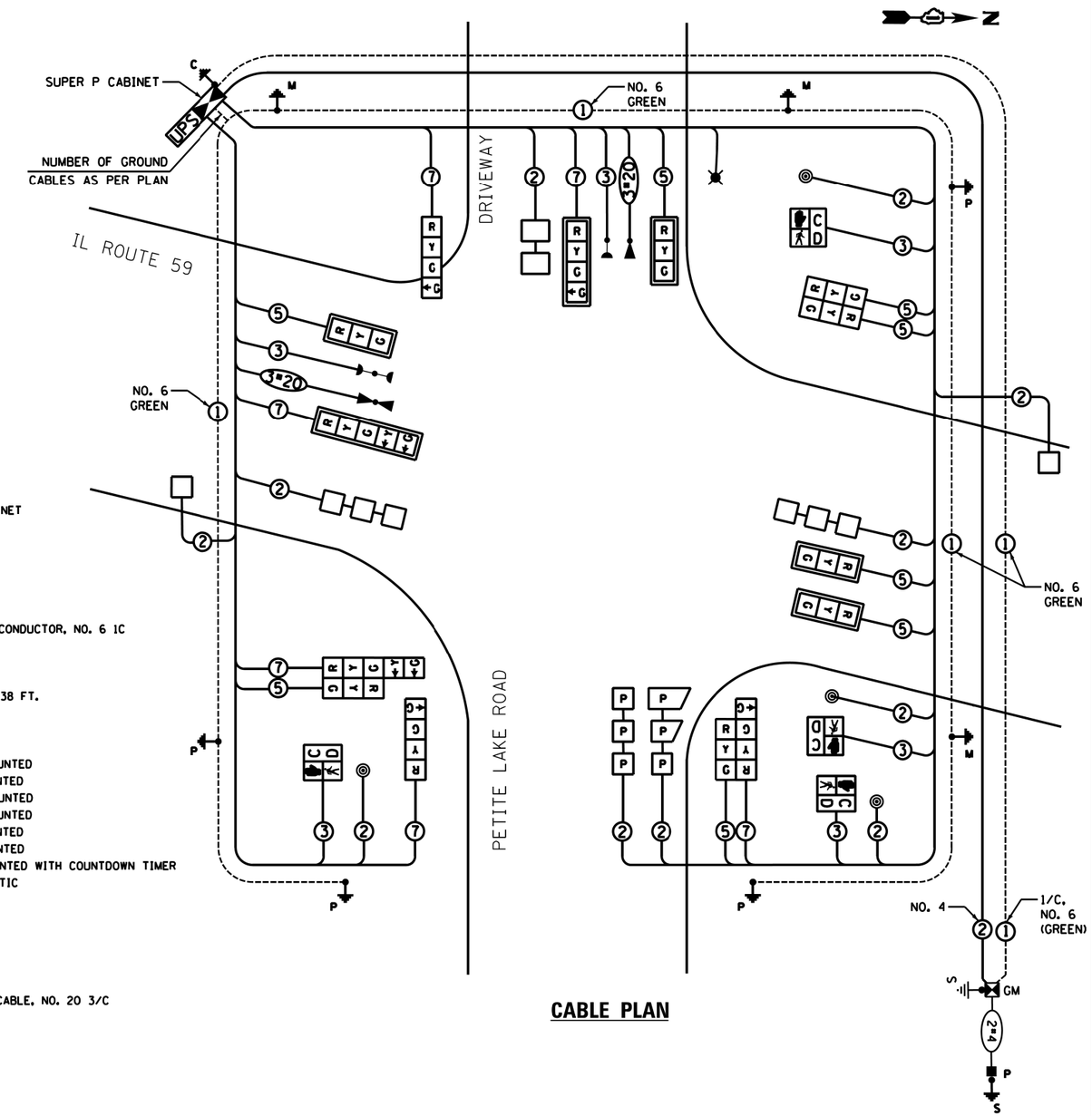
LEGEND

- DUAL ENTRY PHASE
- PEDESTRIAN PHASE
- SINGLE ENTRY PHASE
- OVERLAP
- * NUMBER REFERS TO ASSOCIATED PHASE

SCHEDULE OF QUANTITIES

	QUANTITY	UNIT	ITEM
	7	SQ FT	SIGN PANEL - TYPE 1
	21	SQ FT	SIGN PANEL - TYPE 2
	1	EACH	SERVICE INSTALLATION - GROUND MOUNTED, METERED
	1,021	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
	81	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.
	257	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
	4	EACH	HANDHOLE
	6	EACH	HEAVY-DUTY HANDHOLE
	1	EACH	DOUBLE HANDHOLE
	1	EACH	FULL-ACTUATED CONTROLLER AND TYPE SUPER P CABINET
	653	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
	932	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
	1,309	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
	814	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
	1,510	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
	447	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 4 2 C
	891	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
	4	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.
	1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.
	1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 38 FT.
	1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 38 FT.
	16	FOOT	CONCRETE FOUNDATION, TYPE A
	4	FOOT	CONCRETE FOUNDATION, TYPE C
	37	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
	4	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
	3	EACH	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, BRACKET MOUNTED
	1	EACH	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, MAST ARM MOUNTED
	1	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED
	4	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
	1	EACH	SIGNAL HEAD, LED, 1-FACE, 5 SECTION, BRACKET MOUNTED
	4	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
	6	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC
	7	EACH	INDUCTIVE LOOP DETECTOR
	308	FOOT	DETECTOR LOOP, TYPE I
	265	FOOT	PREFORMED DETECTOR LOOP
*	2	EACH	LIGHT DETECTOR
*	1	EACH	LIGHT DETECTOR AMPLIFIER
	4	EACH	PEDESTRIAN PUSH-BUTTON
*	251	FOOT	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C
	1	EACH	UNINTERRUPTABLE POWER SUPPLY, SPECIAL

* 100% COST TO THE LAKE VILLA FIRE DISTRICT



CABLE PLAN

I. D. O. T.
TRAFFIC SIGNAL INSTALLATION
ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	% OPERATIONS	TOTAL WATTAGE
SIGNAL (RED)	14		17	0.50	119
(YELLOW)	14		25	0.25	88
(GREEN)	18		15	0.25	68
ARROW	4		12	0.10	5
PED. SIGNAL	4		25	1.00	100
CONTROLLER	1		100	1.00	100
ILLUM. SIGN			25	0.05	--
VIDEO SYSTEM	--		-	1.00	--
FLASHER LED					
				TOTAL =	480

ENERGY COSTS- BILLED TO: IDOT DISTRICT 1
201 WEST CENTER COURT
SCHAUMBURG, IL 60196-1096

ENERGY SUPPLY - CONTACT TERRI BLECK
PHONE 847-816-5239
COMMONWEALTH EDISON

TS 21635

FILE NAME = S:\WP\Design\JC\Design Let Projects\Consultant\60W06 - IL 59 @ Petite Lake Rd\New Design\New Working File LVO Revised Submittal\60W06_CAB-59-PETITE.dgn	USER NAME = curryja	DESIGNED - DW	REVISED -
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	PLOT SCALE = 40.000 ' / in.	CHECKED - KMM	REVISED -
	PLOT DATE = 8/10/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

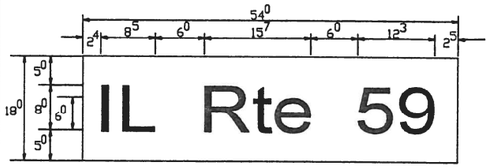
IL ROUTE 59 AND PETITE LAKE ROAD
CABLE PLAN, SCHEDULE OF QUANTITIES, PHASE DESIGNATION DIAGRAM
& EMERGENCY VEHICLE PREEMPTION SEQUENCE

SCALE: N.T.S. | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	61

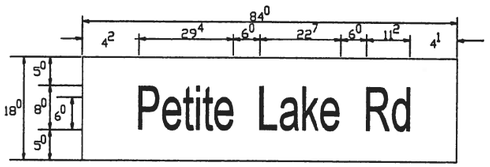
CONTRACT NO. 60W06
ILLINOIS FED. AID PROJECT

PANEL SIGN DESIGN TYPE 1



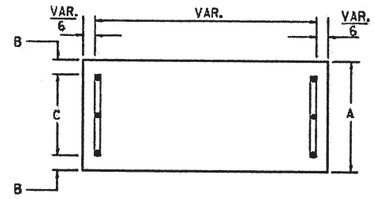
___ Sq. M. each
6.75 Sq. Ft. each
1 Required
Design Series D

PANEL SIGN DESIGN TYPE 2

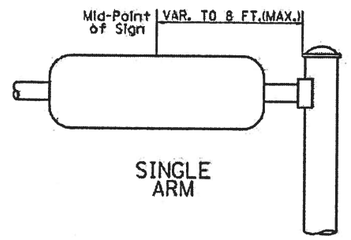


___ Sq. M. each
10.5 Sq. Ft. each
2 Required
Design Series D

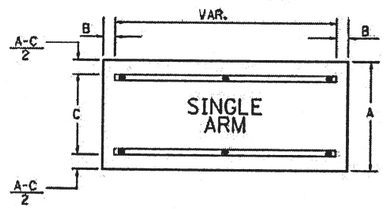
SUPPORTING CHANNELS



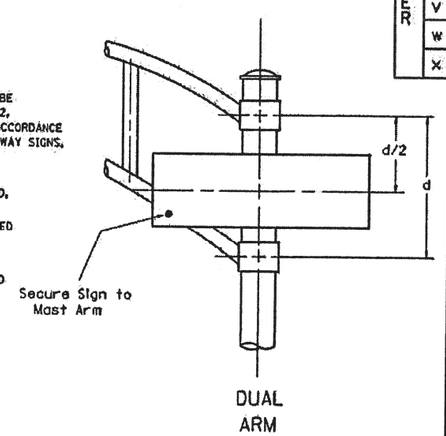
A	B	C
18"	2"	14"



SUPPORTING CHANNELS



A	B	C
18"	2"	12"
30"	2"	22"



GENERAL NOTES

1. WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
2. ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
3. THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 8'-0".
4. ALL BORDERS SHALL BE ¾" WIDE AND CORNER RADIUS SHALL BE 2-1/4 ".
5. SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:

* J.O. HERBERT CO.
  MIDLOTHIAN, VA.
* WESTERN REMAC INC.
  WOODRIDGE, IL.

PARTS LISTING:

SIGN CHANNEL	PART #HP0053 (MED. CHANNEL)
SIGN SCREWS	¼" x 14 x 1" H.W.H. #3 SELF TAPPING WITH NEOPRENE WASHER
BRACKETS	PART #HP0034 (UNIVERSAL) CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BRACKET OF THE ABOVE PRODUCT.

SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM shall be used. See Note #5.

Upper Case To Lower Case
Spacing Chart 8-6 Inch Series "C & D"

FIRST LETTER	SECOND LETTER: a c d e g o q		b h i k l m n p r u		f w		j		s t		v y		x		z	
SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
A W X	1²	1⁴	1⁴	1⁵	1²	1⁴	0⁶	1⁰	1¹	1⁴	0⁶	1⁰	1¹	1²	1²	1⁴
B	1⁴	1⁵	2⁰	2¹	1⁴	1⁵	1¹	1²	1⁴	1⁵	1²	1⁴	1²	1⁴	1⁶	1⁷
C E G	1⁴	1⁵	2⁰	2¹	1²	1⁴	0⁶	1⁰	1²	1⁴	1²	1⁴	1⁴	1⁵	1⁴	1⁵
D O Q R	1⁴	1⁵	2⁰	2¹	1⁴	1⁵	0⁶	1⁰	1²	1⁴	1²	1⁴	1⁴	1⁵	1⁴	1⁵
F	0⁵	0⁶	1⁴	1⁵	0⁶	1⁰	0⁵	0⁶	0⁶	1⁰	0⁶	1⁰	0⁶	1⁰	1¹	1²
H I M N	2⁰	2¹	2²	2⁴	2⁰	2¹	1⁴	1⁵	1⁶	1⁷	1⁶	1⁷	2⁰	2¹	2⁰	2¹
J U	2⁰	2¹	2⁰	2¹	1⁶	1⁷	1⁴	1⁵	1⁶	1⁷	1⁶	1⁷	1⁶	1⁷	2⁰	2¹
K L	1¹	1²	1⁶	1⁷	1¹	1²	0⁵	0⁶	1¹	1²	1¹	1²	1¹	1²	1²	1⁴
P	1²	1⁴	1⁴	1⁵	1²	1⁴	0⁵	0⁶	1¹	1²	1¹	1²	1²	1⁴	1²	1⁴
S	1²	1⁴	1⁶	1⁷	1²	1⁴	0⁶	1⁰	1²	1⁴	1²	1⁴	1²	1⁴	1²	1⁴
T	1¹	1²	1⁶	1⁷	0⁶	1⁰	0⁶	1⁰	1¹	1²	1¹	1²	1¹	1²	1²	1⁴
V	0⁶	1⁰	1⁴	1⁵	1¹	1²	0⁶	1⁰	1²	1⁴	1²	1⁴	1²	1⁴	1²	1⁴
Y	0⁵	0⁶	1⁴	1⁵	0⁶	1⁰	0⁵	0⁶	0⁵	0⁷	0⁵	0⁶	0⁶	1⁰	1¹	1²
Z	1⁶	1⁷	2²	2⁴	1⁶	1⁷	1²	1⁴	1⁶	1⁷	1⁶	1⁷	1⁶	1⁷	2⁰	2¹

Lower Case To Lower Case
Spacing Chart 6 Inch Series "C & D"

FIRST LETTER	SECOND LETTER: a c d e g o q		b h i k l m n p r u		f w		j		s t		v y		x		z	
SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
a d h g l j l m n q u	1⁶	1⁷	2²	2⁴	1⁶	1⁷	1²	1⁴	1⁴	1⁵	1⁴	1⁵	1⁶	1⁷	1⁶	1⁷
b f k o p s	1²	1⁴	1⁶	1⁷	1¹	1²	0⁵	0⁶	1¹	1²	1¹	1²	1²	1⁴	1²	1⁴
c e	1²	1⁴	1⁶	1⁷	1²	1⁴	0⁶	1⁰	1²	1⁴	1²	1⁴	1²	1⁴	1²	1⁴
r	0⁶	1⁰	1²	1⁴	0⁶	1⁰	0³	0³	0⁵	0⁶	0⁵	0⁶	0⁶	1⁰	0⁶	1⁰
t z	1²	1⁴	1⁶	1⁷	1²	1⁴	0⁶	1⁰	1¹	1²	1¹	1²	1²	1⁴	1²	1⁴
v y	1¹	1²	1⁴	1⁵	1¹	1²	0⁵	0⁶	0⁶	1⁰	0⁶	1⁰	1¹	1²	1¹	1²
w	1¹	1²	1⁴	1⁵	1¹	1²	0⁵	0⁶	1¹	1²	1¹	1²	1¹	1²	1²	1⁴
x	1²	1⁴	1⁶	1⁷	1¹	1²	0⁵	0⁶	1¹	1²	1¹	1²	1¹	1²	1²	1⁴

Number To Number
Spacing Chart 8 Inch Series "C & D"

FIRST NUMBER	SECOND NUMBER: 0		1		2		3		4		5		6		7		8		9	
SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
0 9	1⁶	1⁷	1⁶	1⁷	1⁴	1⁵	1²	1⁴	1⁴	1⁵	1⁴	1⁵	1⁶	1⁷	1²	1⁴	1⁶	1⁷	1⁶	1⁷
1	2⁰	2¹	2⁰	2¹	2⁰	2¹	1⁶	1⁷	1⁴	1⁵	2⁰	2¹	2⁰	2¹	1⁴	1⁵	2⁰	2¹	2⁰	2¹
2 3 4	1⁴	1⁵	1⁴	1⁵	1⁴	1⁵	1²	1⁴	1²	1⁴	1⁴	1⁵	1⁴	1⁵	1¹	1²	1⁶	1⁷	1⁴	1⁵
5	1⁴	1⁵	1⁴	1⁵	1⁴	1⁵	1¹	1²	1¹	1²	1⁴	1⁵	1⁴	1⁵	1¹	1²	1⁴	1⁵	1⁴	1⁵
6	1⁶	1⁷	1⁴	1⁵	1⁴	1⁵	1²	1⁵	1²	1⁴	1⁴	1⁵	1⁴	1⁵	1¹	1²	1⁴	1⁵	1⁴	1⁵
7	1²	1⁴	1²	1⁴	1⁴	1⁵	1²	1⁵	0⁵	0⁶	1²	1⁴	1⁴	1⁵	1¹	1²	1⁴	1⁵	1²	1⁴
8	1⁶	1⁷	1⁶	1⁷	1⁴	1⁵	1²	1⁵	1²	1⁴	1⁴	1⁵	1⁶	1⁷	1²	1⁴	1⁶	1⁷	1⁴	1⁵

EXAMPLE, 2③ DENOTES $\frac{3''}{8}$

UPPER AND LOWER CASE LETTER WIDTHS

LETTERS	6 INCH UPPER CASE LETTERS SERIES C	6 INCH UPPER CASE LETTERS SERIES D	8 INCH UPPER CASE LETTERS SERIES C	8 INCH UPPER CASE LETTERS SERIES D	LETTERS	6 INCH LOWER CASE LETTERS SERIES C	6 INCH LOWER CASE LETTERS SERIES D
A	3⁶	5⁰	5⁰	6⁵	a	3⁵	4²
B	3²	4⁰	4³	5³	b	3⁵	4²
C	3²	4⁰	4³	5³	c	3⁵	4¹
D	3²	4⁰	4³	5³	d	3⁵	4²
E	3⁰	3⁵	4⁰	4⁷	e	3⁵	4²
F	3⁰	3⁵	4⁰	4⁷	f	2³	2⁶
G	3²	4⁰	4³	5³	g	3⁵	4²
H	3²	4⁰	4³	5³	h	3⁵	4²
I	0⁷	0⁷	1¹	1²	i	1¹	1¹
J	3⁰	3⁶	4⁰	5⁰	j	2⁰	2²
K	3²	4¹	4³	5⁴	k	3⁵	4²
L	3⁰	3⁵	4⁰	4⁷	l	1¹	1¹
M	3⁷	4⁵	5¹	6¹	m	6⁰	7⁰
N	3²	4⁰	4³	5³	n	3⁵	4²
O	3⁴	4²	4⁵	5⁵	o	3⁶	4³
P	3²	4⁰	4³	5³	p	3⁵	4²
Q	3⁴	4²	4⁵	5⁵	q	3⁵	4²
R	3²	4⁰	4³	5³	r	2⁶	3²
S	3²	4⁰	4³	5³	s	3⁶	4²
T	3⁰	3⁵	4⁰	4⁷	t	2⁷	3²
U	3²	4⁰	4³	5³	u	3⁵	4²
V	3⁵	4⁴	4⁷	6⁰	v	4²	4⁷
W	4⁴	5²	6⁰	7⁰	w	5⁵	6⁴
X	3⁴	4⁰	4⁵	5³	x	4⁴	5¹
Y	3⁶	5⁰	5⁰	6⁶	y	4⁶	5³
Z	3²	4⁰	4³	5³	z	3⁶	4³

NUMBER	6 INCH SERIES C	6 INCH SERIES D	8 INCH SERIES C	8 INCH SERIES D
1	1²	1⁴	1⁵	2⁰
2	3²	4⁰	4³	5³
3	3²	4⁰	4³	5³
4	3⁵	4³	4⁷	5⁷
5	3²	4⁰	4³	5³
6	3²	4⁰	4³	5³
7	3²	4⁰	4³	5³
8	3²	4⁰	4³	5³
9	3²	4⁰	4³	5³
0	3⁴	4²	4⁵	5⁵

TS-10

FILE NAME = #FILE#	USER NAME = #USER#	DESIGNED - DW	REVISED -
		DRAWN - LV	REVISED -
	PLOT SCALE = #SCALE#	CHECKED - KMM	REVISED -
	PLOT DATE = #DATE#	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

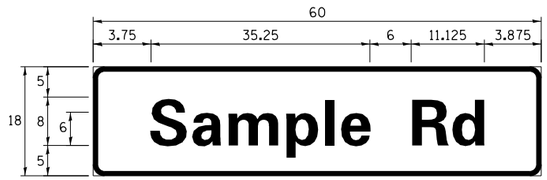
MAST ARM MOUNTED
STREET NAME SIGNS

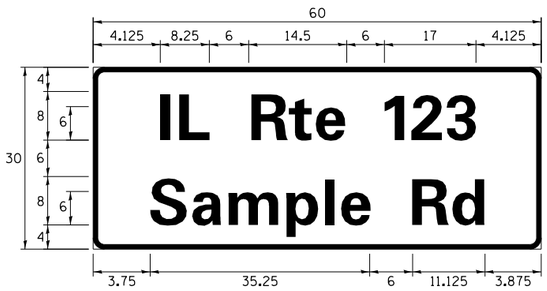
SCALE: N.T.S. | SHEET NO. OF SHEETS | STA. TO STA.

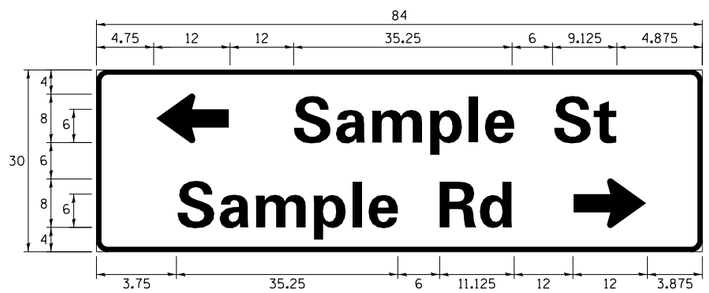
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	62

CONTRACT NO. 60W06
ILLINOIS FED. AID PROJECT

SIGN PANEL – TYPE 1 OR TYPE 2







DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D OR C	-	1 OR 2	ZZ	-

ALL DIMENSIONS ARE IN INCHES EXCEPT NOTED OTHERWISE

COMMON STREET NAME ABBREVIATIONS AND WIDTHS

NAME	ABBREVATION	WIDTH (INCH)	
		SERIES "C"	SERIES "D"
AVENUE	Ave	15.000	18.250
BOULEVARD	Blvd	17.125	20.000
CIRCLE	Cir	11.125	13.000
COURT	Ct	8.250	9.625
DRIVE	Dr	8.625	10.125
HIGHWAY	Hwy	18.375	22.000
ILLINOIS	IL	7.000	8.250
LANE	Ln	9.125	10.750
PARKWAY	Pkwy	23.375	27.375
PLACE	Pl	7.125	7.750
ROAD	Rd	9.625	11.125
ROUTE	Rte	12.625	14.500
STREET	St	8.000	9.125
TERRACE	Ter	12.625	14.625
TRAIL	Tr	7.750	9.125
UNITED STATES	US	10.375	12.250

GENERAL NOTES

1. WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.

2. ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ SHEETING)

3. THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS SHALL BE ¾" WIDE. CORNER RADIUS SHALL BE 1-7/8". THE SPACING BETWEEN THE WORDS SHOULD BE 6", IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL. A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.

4. A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH. IF SERIES "D" DOES NOT FIT ON A 8'-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THE ABBREVIATION CANNOT FIT ON THE FIRST LINE.

5. LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLOGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.

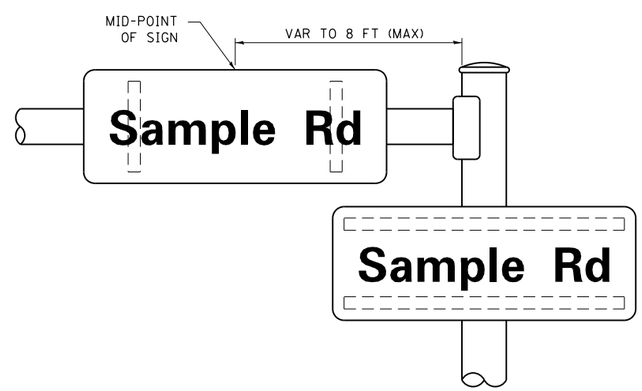
6. SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

LOCAL SUPPLIERS:	PARTS LISTING:	
- J.O. HERBERT COMPANY, INC MIDLOTHIAN, VA	SIGN CHANNEL	PART #HPN053 (MED. CHANNEL)
	SIGN SCREWS	1/4" x 14 x 1" H.W.H. #3 SELF TAPPING WITH NEOPRENE WASHER
- WESTERN REMAC, INC. WOODRIDGE, IL	BRACKETS	PART #HPN034 (UNIVERSAL) CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

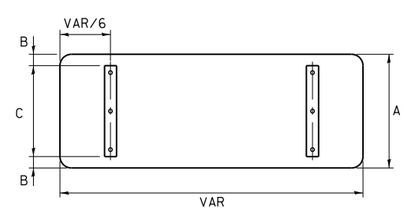
OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BRACKET OF THE ABOVE PRODUCT.

MOUNTING LOCATION

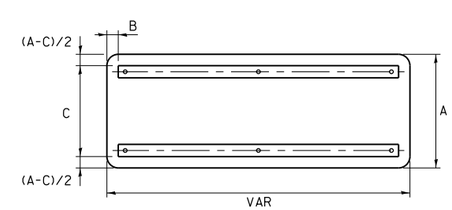
ARM OR POLE MOUNTED



SUPPORTING CHANNELS



A	B	C
18"	2"	14"
30"	2"	24"



A	B	C
18"	2"	12"
30"	2"	22"

STANDARD ALPHABETS SPACING CHART

(8") UPPER CASE AND (6") LOWER CASE

FHWA SERIES "C"				FHWA SERIES "D"			
CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)	CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)
A	0.240	5.122	0.240	A	0.240	6.804	0.240
B	0.880	4.482	0.480	B	0.960	5.446	0.400
C	0.720	4.482	0.720	C	0.800	5.446	0.800
D	0.880	4.482	0.720	D	0.960	5.446	0.800
E	0.880	4.082	0.480	E	0.960	4.962	0.400
F	0.880	4.082	0.240	F	0.960	4.962	0.240
G	0.720	4.482	0.720	G	0.800	5.446	0.800
H	0.880	4.482	0.880	H	0.960	5.446	0.960
I	0.880	1.120	0.880	I	0.960	1.280	0.960
J	0.240	4.082	0.880	J	0.240	5.122	0.960
K	0.880	4.482	0.480	K	0.960	5.604	0.400
L	0.880	4.082	0.240	L	0.960	4.962	0.240
M	0.880	5.284	0.880	M	0.960	6.244	0.960
N	0.880	4.482	0.880	N	0.960	5.446	0.960
O	0.720	4.722	0.720	O	0.800	5.684	0.800
P	0.880	4.482	0.720	P	0.960	5.446	0.240
Q	0.720	4.722	0.720	Q	0.800	5.684	0.800
R	0.880	4.482	0.480	R	0.960	5.446	0.400
S	0.480	4.482	0.480	S	0.400	5.446	0.400
T	0.240	4.082	0.240	T	0.240	4.962	0.240
U	0.880	4.482	0.880	U	0.960	5.446	0.960
V	0.240	4.962	0.240	V	0.240	6.084	0.240
W	0.240	6.084	0.240	W	0.240	7.124	0.240
X	0.240	4.722	0.240	X	0.400	5.446	0.400
Y	0.240	5.122	0.240	Y	0.240	6.884	0.240
Z	0.480	4.482	0.480	Z	0.400	5.446	0.400
a	0.320	3.842	0.640	a	0.400	4.562	0.720
b	0.720	4.082	0.480	b	0.800	4.802	0.480
c	0.480	4.002	0.240	c	0.480	4.722	0.240
d	0.480	4.082	0.720	d	0.480	4.802	0.800
e	0.480	4.082	0.320	e	0.480	4.722	0.320
f	0.320	2.480	0.160	f	0.320	2.882	0.160
g	0.480	4.082	0.720	g	0.480	4.802	0.800
h	0.720	4.082	0.640	h	0.800	4.722	0.720
i	0.720	1.120	0.720	i	0.800	1.280	0.800
j	0.000	2.320	0.720	j	0.000	2.642	0.800
k	0.720	4.322	0.160	k	0.800	5.122	0.160
l	0.720	1.120	0.720	l	0.800	1.280	0.800
m	0.720	6.724	0.640	m	0.800	7.926	0.720
n	0.720	4.082	0.640	n	0.800	4.722	0.720
o	0.480	4.082	0.480	o	0.480	4.882	0.480
p	0.720	4.082	0.480	p	0.800	4.802	0.480
q	0.480	4.082	0.720	q	0.480	4.802	0.800
r	0.720	2.642	0.160	r	0.800	3.042	0.160
s	0.320	3.362	0.240	s	0.320	3.762	0.240
t	0.080	2.882	0.080	t	0.080	3.202	0.080
u	0.640	4.082	0.720	u	0.720	4.722	0.800
v	0.160	4.722	0.160	v	0.160	5.684	0.160
w	0.160	7.524	0.160	w	0.160	9.046	0.160
x	0.000	5.202	0.000	x	0.000	6.244	0.000
y	0.160	4.962	0.160	y	0.160	6.004	0.160
z	0.240	3.362	0.240	z	0.240	4.002	0.240
1	0.720	1.680	0.880	1	0.800	2.000	0.960
2	0.480	4.482	0.480	2	0.800	5.446	0.800
3	0.480	4.482	0.480	3	1.440	5.446	0.800
4	0.240	4.962	0.720	4	0.160	6.004	0.960
5	0.480	4.482	0.480	5	0.800	5.446	0.800
6	0.720	4.482	0.720	6	0.800	5.446	0.800
7	0.240	4.482	0.720	7	0.560	5.446	0.560
8	0.480	4.482	0.480	8	0.800	5.446	0.800
9	0.480	4.482	0.480	9	0.800	5.446	0.800
0	0.720	4.722	0.720	0	0.800	5.684	0.800
-	0.240	2.802	0.240	-	0.240	2.802	0.240

TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET	R		
RAILROAD CONTROL CABINET		R	R
COMMUNICATIONS CABINET	R CC	ECC	CC
MASTER CONTROLLER		EMC	MC
MASTER MASTER CONTROLLER		EMMC	MMC
UNINTERRUPTABLE POWER SUPPLY	R UPS	EUPS	UPS
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT	R	P	P
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT	R T	P T	P T
STEEL MAST ARM ASSEMBLY AND POLE	R		
ALUMINUM MAST ARM ASSEMBLY AND POLE	R		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE	R		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA	R PTZ	PTZ	PTZ
SIGNAL POST	R		
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM	R		
GUY WIRE	R		
SIGNAL HEAD	R		
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)			2
SIGNAL HEAD WITH BACKPLATE	R		
SIGNAL HEAD OPTICALLY PROGRAMMED	R "P"	"P"	"P"
FLASHER INSTALLATION (S DENOTES SOLAR POWER)	R "F"	"F"	"F"
PEDESTRIAN SIGNAL HEAD	R		
PEDESTRIAN PUSHBUTTON DETECTOR	R		
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR	R APS	APS	APS
ILLUMINATED SIGN "NO LEFT TURN"	R		
ILLUMINATED SIGN "NO RIGHT TURN"	R		
DETECTOR LOOP, TYPE I			
PREFORMED DETECTOR LOOP		P	P
MICROWAVE VEHICLE SENSOR	R M	M	M
VIDEO DETECTION CAMERA	R V	V	V
VIDEO DETECTION ZONE			
PAN, TILT, ZOOM CAMERA	R PTZ	PTZ	PTZ
WIRELESS DETECTOR SENSOR	R W	W	W
WIRELESS ACCESS POINT	R		

ITEM	REMOVAL	EXISTING	PROPOSED
EMERGENCY VEHICLE LIGHT DETECTOR	R		
CONFIRMATION BEACON	R		
HANDHOLE	R		
HEAVY DUTY HANDHOLE	R H	H	H
DOUBLE HANDHOLE	R		
JUNCTION BOX	R		
UNDERGROUND CONDUIT, GALVANIZED STEEL (UC)			
TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE	R		
COMMON TRENCH			CT
COILABLE NONMETALLIC CONDUIT (EMPTY)			CNC
SYSTEM ITEM		S	S
INTERSECTION ITEM		I	IP
REMOVE ITEM	R		
RELOCATE ITEM	RL		
ABANDON ITEM	A		
12" (300mm) TRAFFIC SIGNAL SECTION		R	R
12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE		R Y G	
SIGNAL FACE		R Y G Y G	R Y G Y G
SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD "RB" INDICATES REFLECTIVE BACKPLATE		R Y G Y G "P"	R Y G Y G "P"
12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL		DW W	
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED			
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID			
PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER		C D	C D
RADIO INTERCONNECT	R		
RADIO REPEATER	R ERR	ERR	RR
DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED		5	5
GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)		1	1

ITEM	REMOVAL	EXISTING	PROPOSED
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE		I	I
COAXIAL CABLE		C	C
VENDOR CABLE FOR CAMERA		V	V
COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED		6	6
FIBER OPTIC CABLE NO. 62.5/125, MM12F		12F	
FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F		24F	24F
FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F		36F	36F
GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE		C	C
CONTROLLER CABINET AND FOUNDATION TO BE REMOVED	RCF		
STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED	RMF		
ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED	RMF		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED	RMF		
SIGNAL POST AND FOUNDATION TO BE REMOVED	RPF		
INTERSECTION & SAMPLING (SYSTEM) DETECTOR		IS	IS
SAMPLING (SYSTEM) DETECTOR		S	S
QUEUE DETECTOR		Q	Q
PREFORMED QUEUE DETECTOR		PQ	PQ
PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		PIS	PIS
PREFORMED SAMPLING (SYSTEM) DETECTOR		PS	PS

RAILROAD SYMBOLS

	EXISTING	PROPOSED
RAILROAD CONTROL CABINET	R	R
RAILROAD CANTILEVER MAST ARM		
FLASHING SIGNAL		
CROSSING GATE		
CROSSBUCK		

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

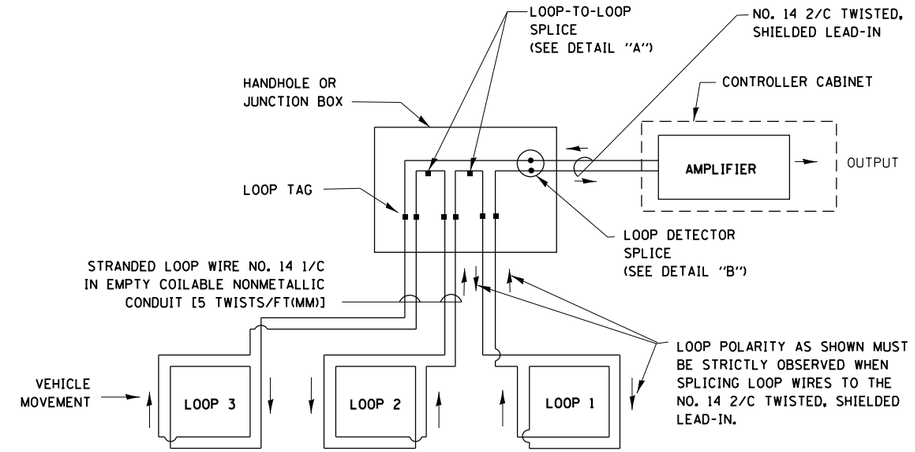
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

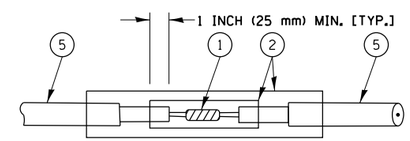
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

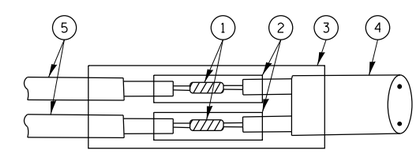


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



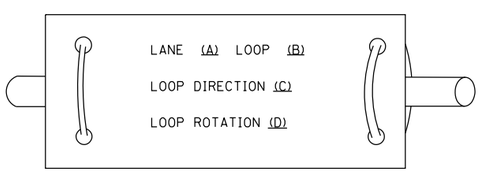
DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP

LOOP LEAD-IN CABLE TAG

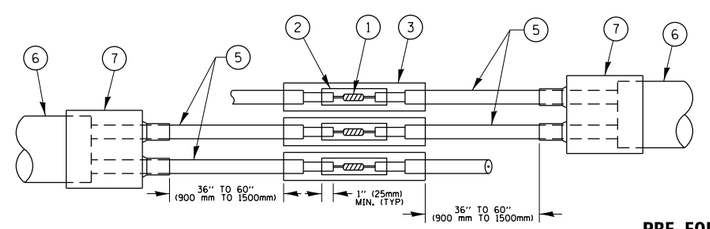


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

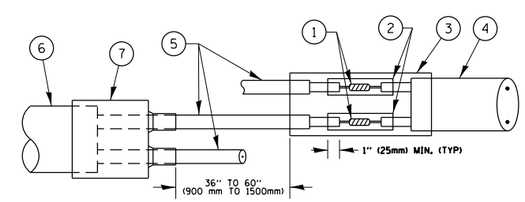
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETAIL "A"
LOOP-TO-LOOP SPLICE

PRE-FORMED LOOP



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

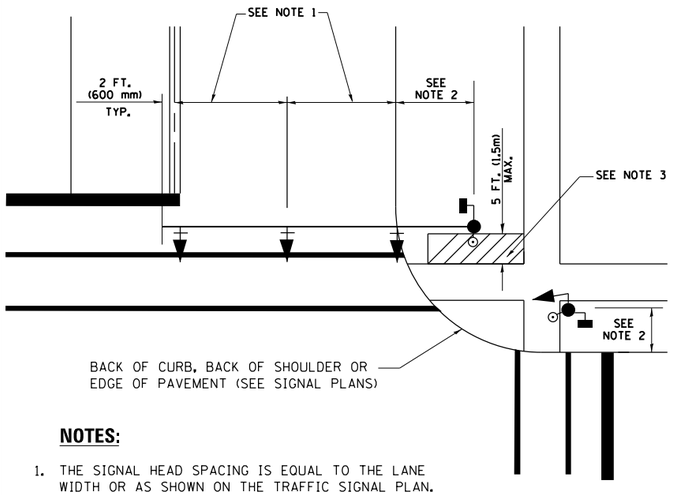
LOOP DETECTOR SPLICE

1. WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
2. WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
3. WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
4. NO. 14 2/C TWISTED, SHIELDED CABLE.
5. LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
6. PRE-FORMED LOOP
7. XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454	USER NAME = ledezmarm	DESIGNED - DAD	REVISED - DAG 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS / SHEET NO.
		DRAWN - BCK	REVISED -					104	105N-4	LAKE	113 / 65
	PLOT SCALE = 100.0000 ' / in.	CHECKED - DAD	REVISED -					TS-05		CONTRACT NO. 60W06	
	PLOT DATE = 8/19/2016	DATE - 10-28-09	REVISED -		SCALE: NONE	SHEET NO. 2 OF 7 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

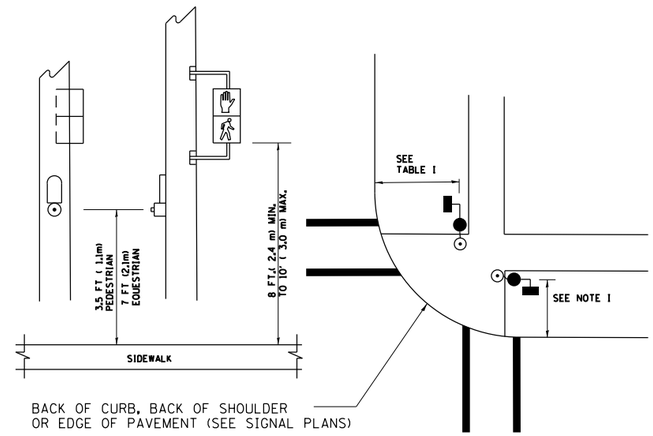
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

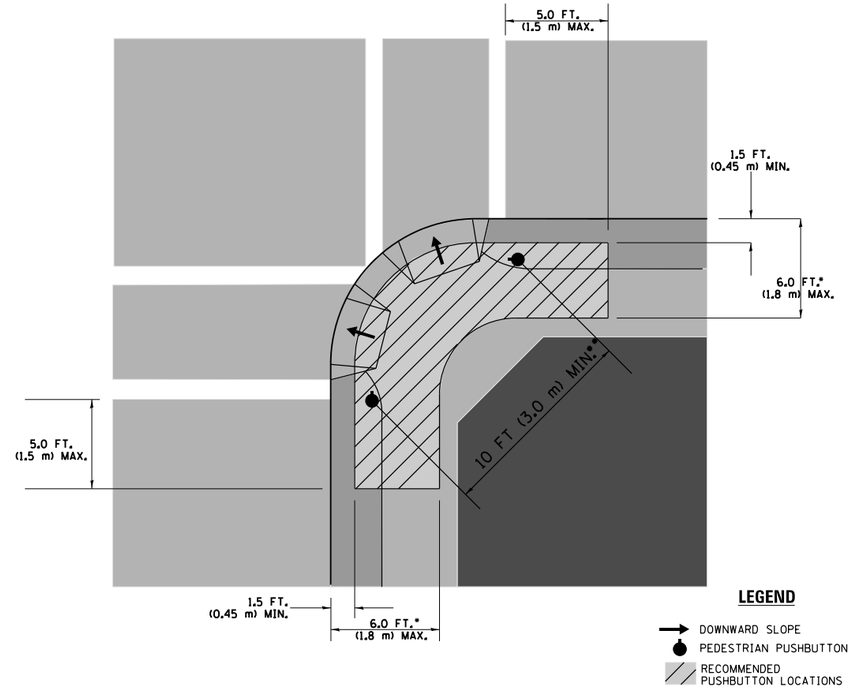
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

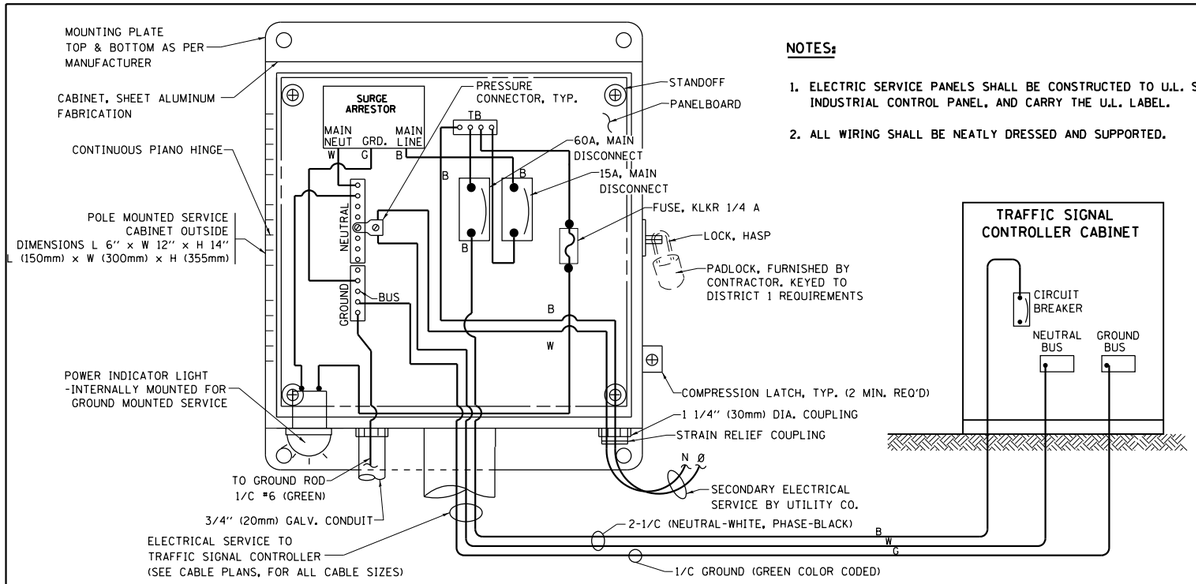
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

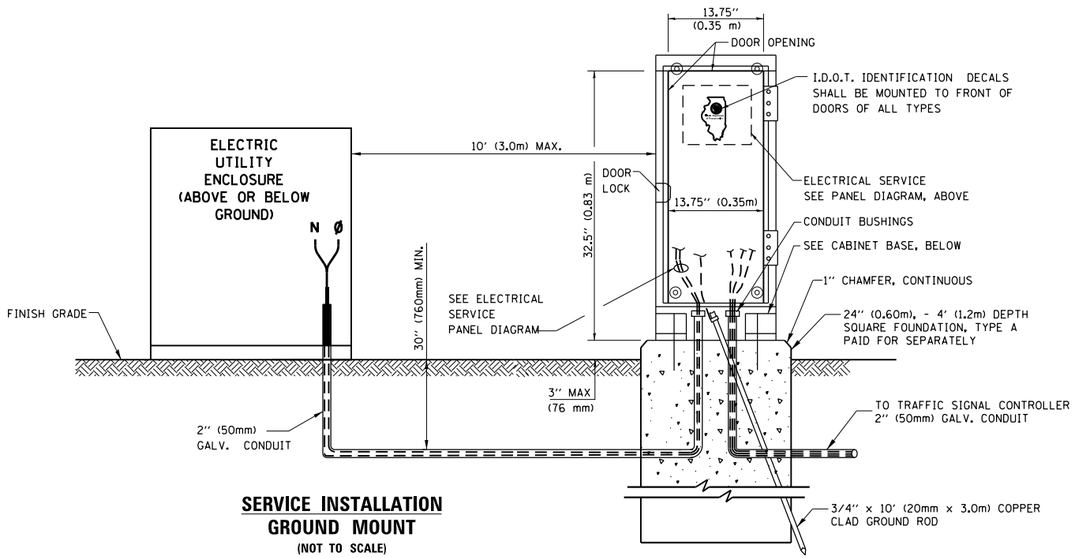


NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

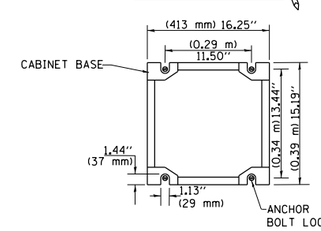
ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)

(NOT TO SCALE)



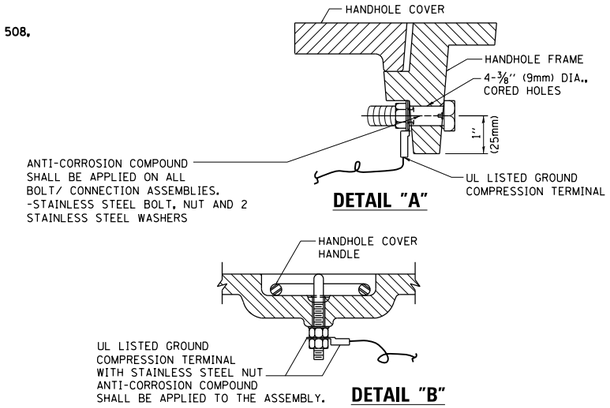
SERVICE INSTALLATION
GROUND MOUNT

(NOT TO SCALE)



CABINET – BASE BOLT PATTERN

(NOT TO SCALE)

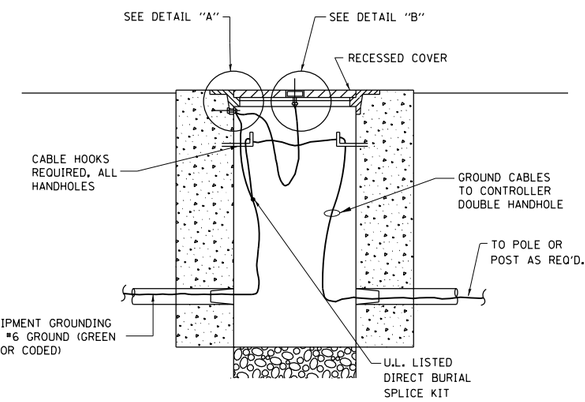


DETAIL "A"

DETAIL "B"

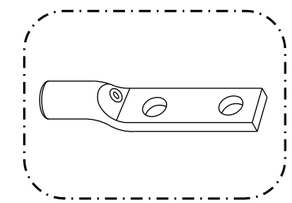
NOTES:
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

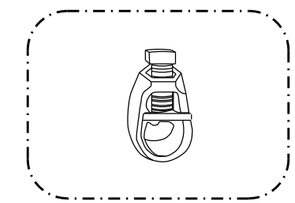


HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)



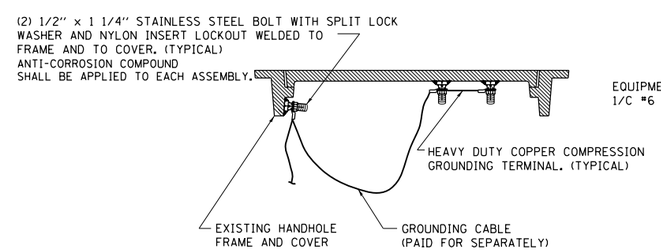
HEAVY-DUTY COMPRESSION TERMINAL
(BURNDY TYPE YGHA OR APPROVED EQUAL)



3/4" (20mm) HEAVY-DUTY GROUND ROD CLAMP
(BURNDY TYPE GRC OR APPROVED EQUAL)

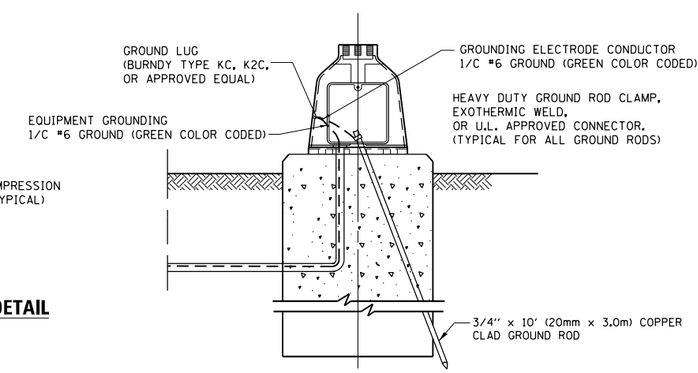
NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
  6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
  13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
  5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)



MAST ARM POLE / POST–GROUNDING DETAIL

(NOT TO SCALE)

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454\DraftingData\DesignStd.dgn	USER NAME = ledezmarm	DESIGNED - DAD	REVISED - DAG 1-1-14
		DRAWN - BCK	REVISED -
	PLOT SCALE = 100.0000 ' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 8/19/2016	DATE - 10-28-09	REVISED -

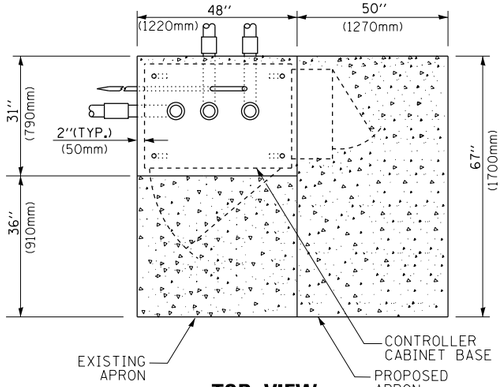
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

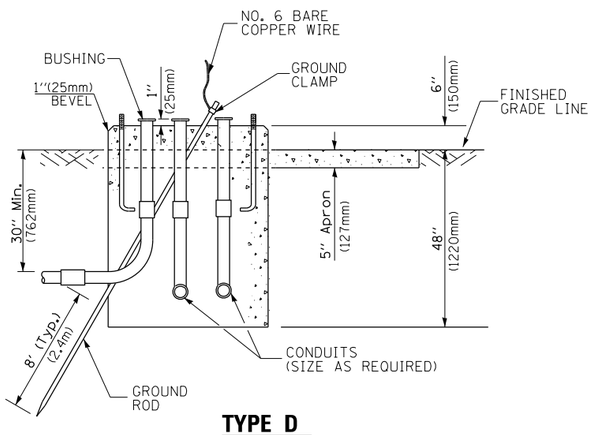
SCALE: NONE | SHEET NO. 4 OF 7 SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	67
	TS-05	CONTRACT NO. 60W06		

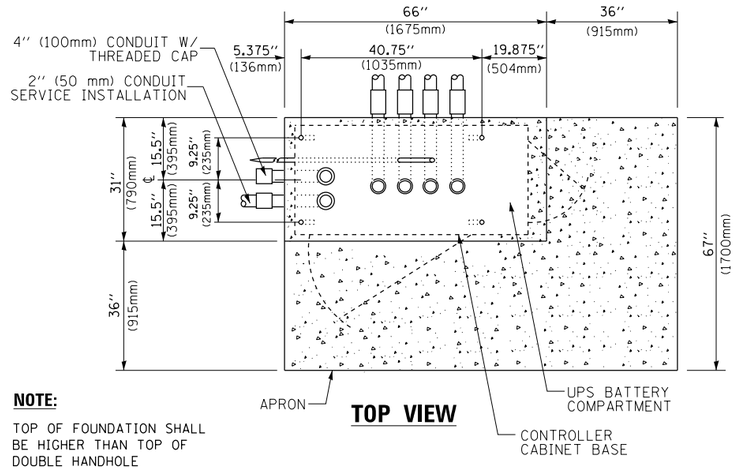
FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT



TOP VIEW



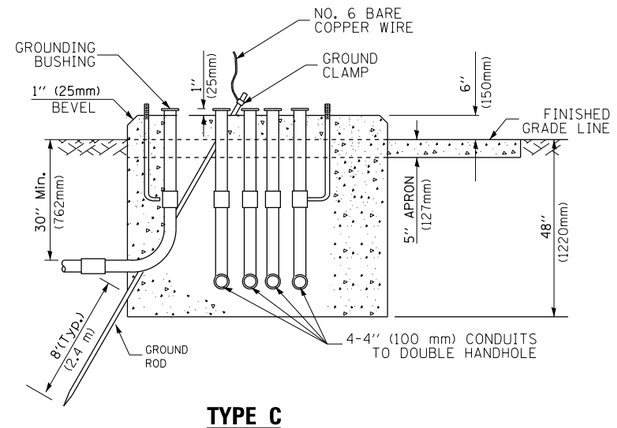
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



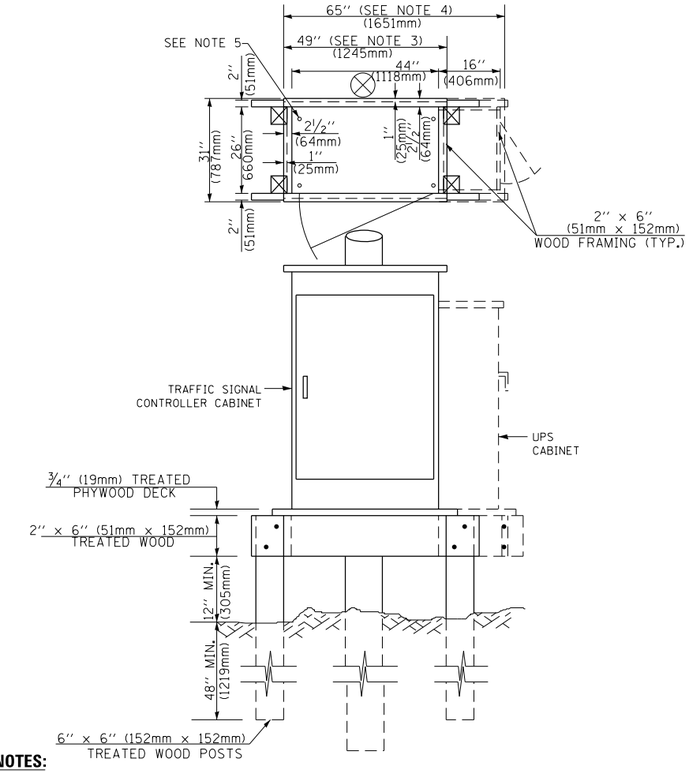
NOTE:

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE

TOP VIEW



TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

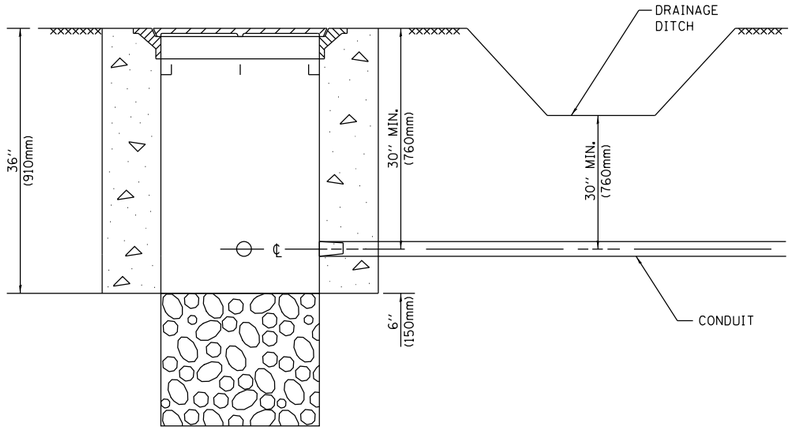
DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
4. For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

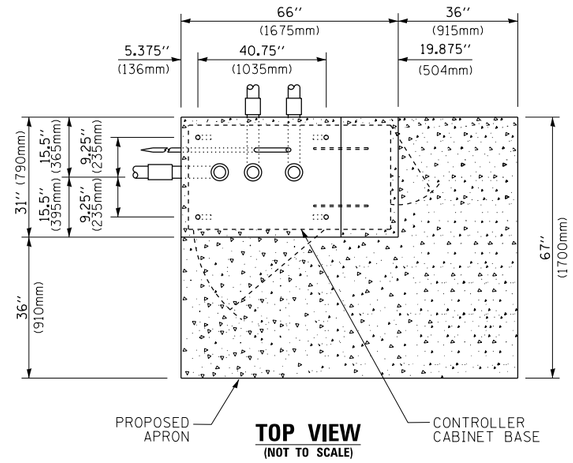


NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

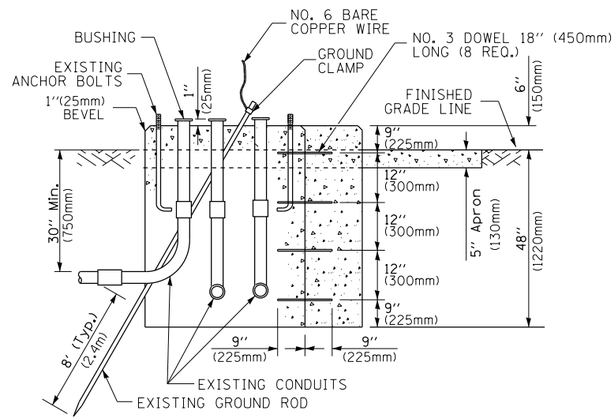
HANDHOLE WITH MINIMUM CONDUIT DEPTH

(NOT TO SCALE)



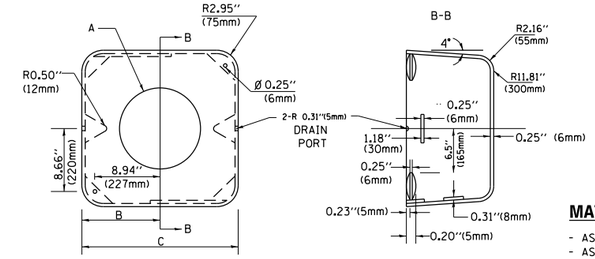
TOP VIEW

(NOT TO SCALE)



MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION

(NOT TO SCALE)



MATERIAL:

- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

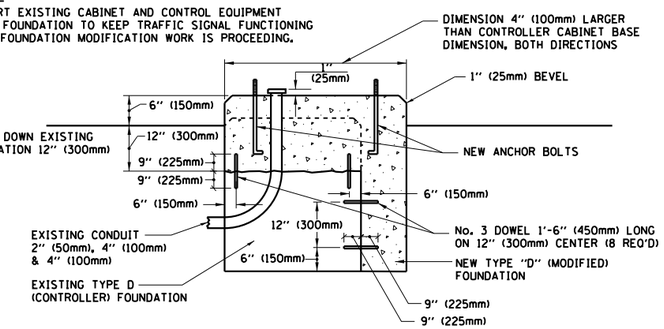
SHROUD

NOTES:

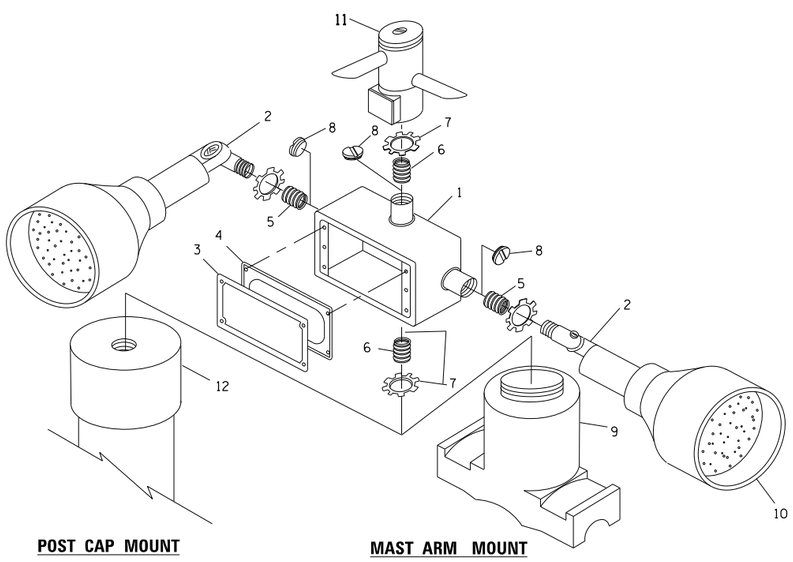
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



POST CAP MOUNT

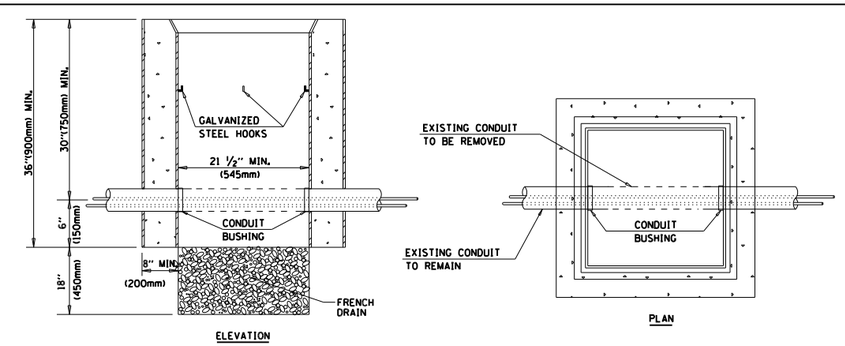
MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	¾"(19 mm) CLOSE NIPPLE
7	¾"(19 mm) LOCKNUT
8	¾"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

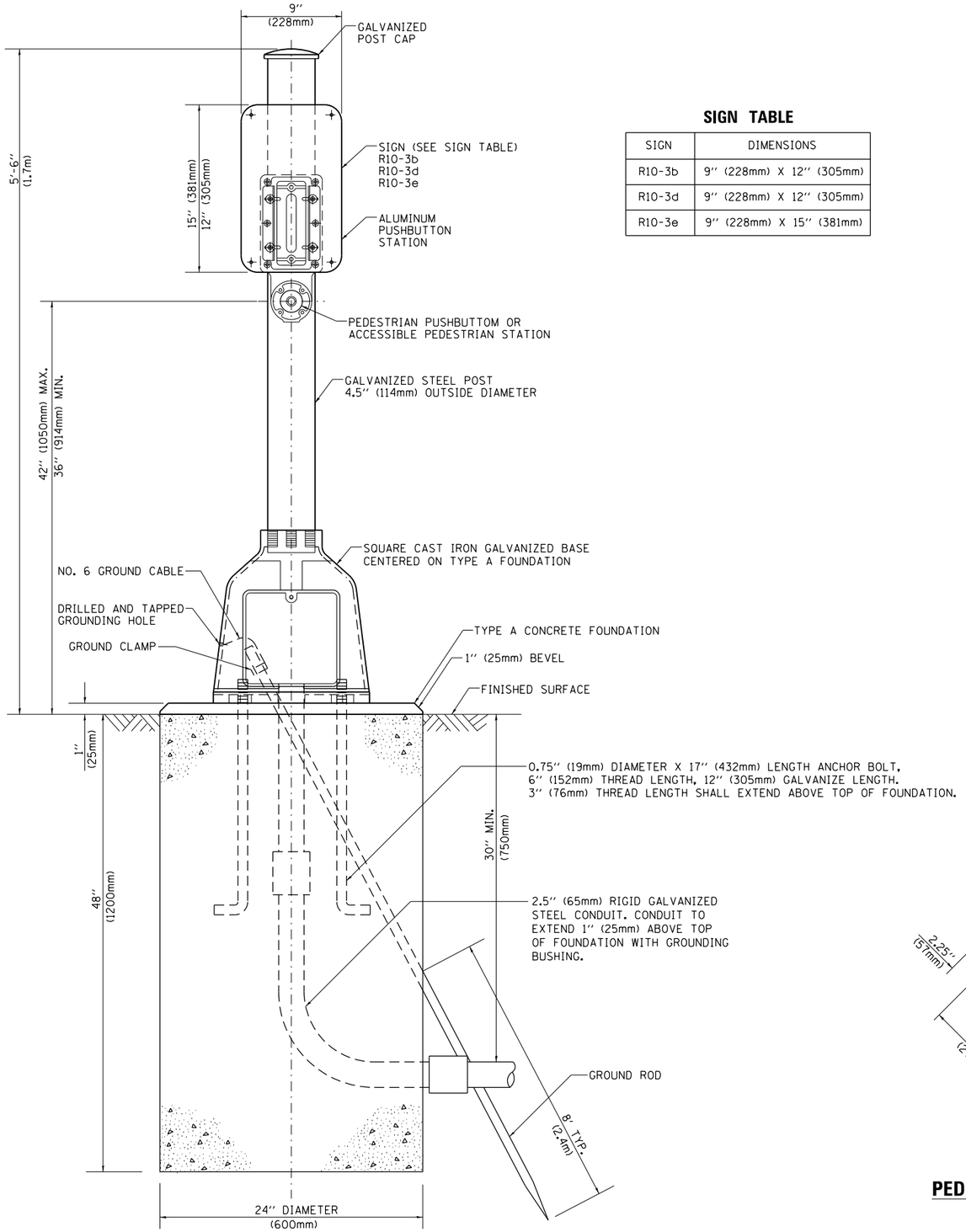
1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
   ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
   ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 "(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



NOTES:

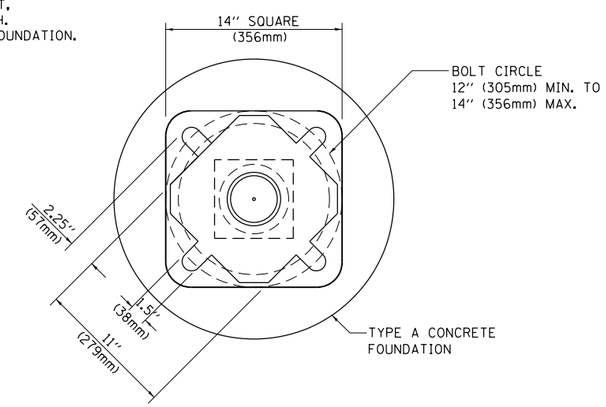
1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT



SIGN TABLE

SIGN	DIMENSIONS
R10-3b	9'' (228mm) X 12'' (305mm)
R10-3d	9'' (228mm) X 12'' (305mm)
R10-3e	9'' (228mm) X 15'' (381mm)



BOLT PATTERN

PEDESTRIAN PUSH BUTTON POST, TYPE A

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454	USER NAME = ledezmarm	DESIGNED - DAG	REVISED - DAG 1-1-14
		DRAWN - DAG	REVISED -
	PLOT SCALE = 100.0000 ' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 8/19/2016	DATE - 10/1/2012	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

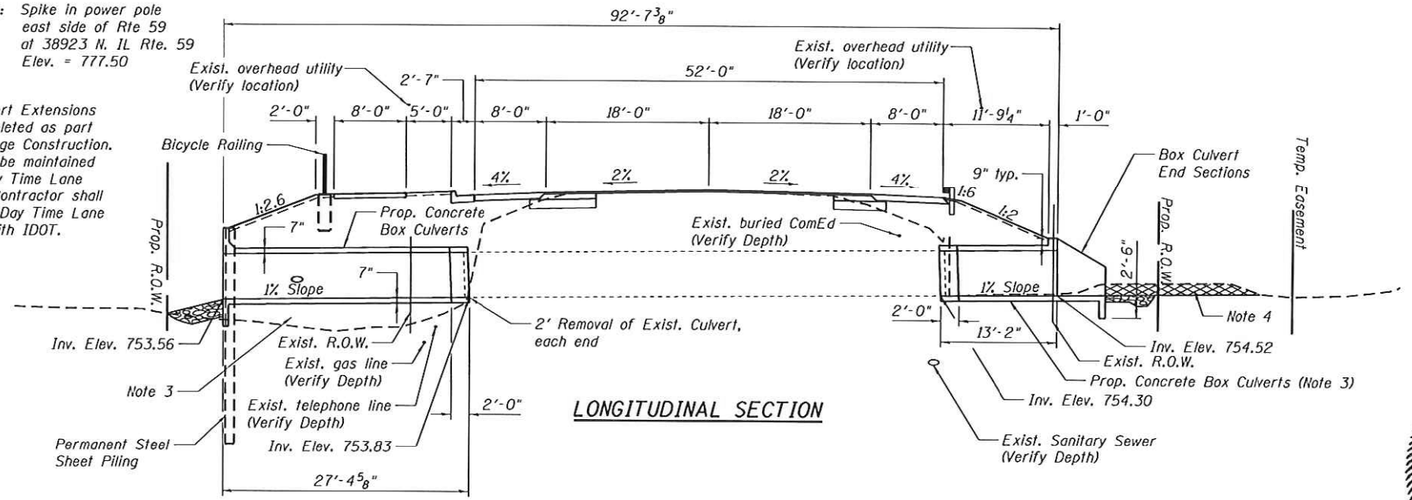
SCALE: NONE | SHEET NO. 7 OF 7 SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	70
	TS-05	CONTRACT NO. 60W06		

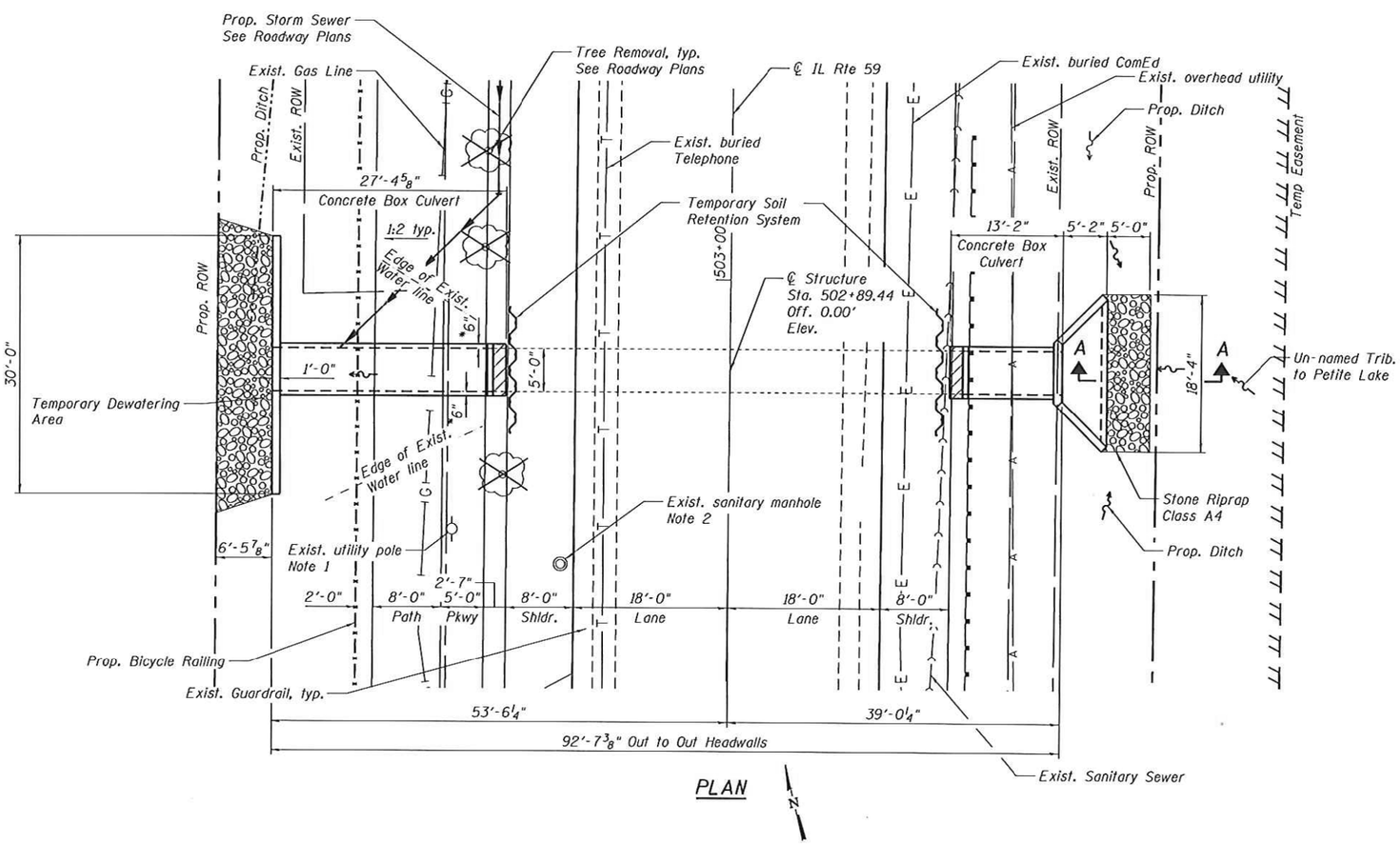
FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

Benchmark: Spike in power pole east side of Rte 59 at 38923 N. IL Rte. 59 Elev. = 777.50

Prop. Culvert Extensions to be completed as part of Pre-Stage Construction. Traffic to be maintained utilizing Day Time Lane Closures. Contractor shall coordinate Day Time Lane Closures with IDOT.



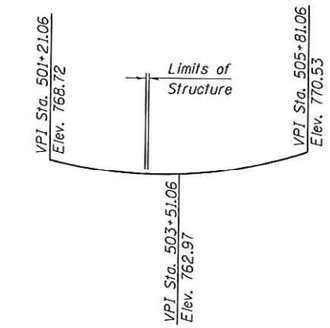
LONGITUDINAL SECTION



PLAN

INDEX OF SHEETS

1. General Plan and Elevation
2. General Notes
3. Bicycle Railing Details
4. End Section Details
5. Culvert Details
6. Culvert Details



PROFILE
Along ℄ Roadway

JASON M. WHYTE
062-060462
REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS
EXP. 11/30/17

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS

2012 AASHTO LRFD Bridge Design Specifications, w/ 2013 Interims

DESIGN STRESSES

FIELD UNITS

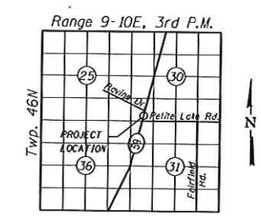
f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)

SEISMIC DATA

Pending Geotechnical Report

NOTES

1. Utility pole where noted on plans appears to have a supporting guide wire only with no overhead Electrical line attached. Contractor to verify if in service.
2. Contractor to verify location of sanitary sewer lines connected to manhole.
3. Contractor shall install additional bedding material as needed to fill in existing low grade and/or to provide a stable base in locations where soft soils are encountered in keeping with the Geotechnical Report, by Testing Service Corporation, dated 9/15/2014. Additional bedding material shall be included in the Unit Price for CONCRETE BOX CULVERTS.
4. Contractor shall excavate local high grade within existing channel bed as needed within the limits of proposed right-of-way and temporary easment to match channel invert with proposed culvert invert at upstream end. This work shall be measured and paid for as Removal and Disposal of Unuitable Materials.



LOCATION SKETCH

GENERAL PLAN AND ELEVATION
(F.A.P. 0104) ILLINOIS ROUTE 59
OVER UN-NAMED TRIB. TO PETITE LAKE
SECTION NO. 105N-4
LAKE COUNTY
STATION 502+93.98

COMPANY NAME: #COMPANY_NAME#
PROJECT CONTACT: #PROJECT_CONTACT#
CLIENT: #CLIENT#
DATE PLOTTED: 6/12/2015 4:18:32 PM
FILE NAME: P:\5409-ipt-IPE.dgn
PLOT DRIVER: IL_dot_bw.pltcfg
PEN TABLE: plotbw.tbl

HRGreen
HRGreen.com
Illinois Professional Design Firm # 184-001322

USER NAME =	DESIGNED - TOR	REVISED
	CHECKED - JMW	REVISED
PLOT SCALE =	DRAWN - WJH	REVISED
PLOT DATE =	CHECKED - RGD	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET NO. OF SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0104	105N-4	LAKE		
		CONTRACT NO. 12377		
		ILLINOIS FED. AID PROJECT		

GENERAL NOTES

1. Concrete Box Culverts 5'x5' shall conform to the requirements of Article 540.05 of the Standard Specifications.

   Fill varies from 6" to 6'-9"

2. BOX CULVERT END SECTIONS shall be paid for as noted in Article 540.08, and The Contract Unit Price for BOX CULVERT END SECTIONS shall include all porous granular bedding material, cast in place wingwalls, headwalls, and aprons, cast in place portions between cells, reinforcement, excavation, backfill, and preformed joint filler.

3. Contractor shall maintain streamflow in accordance with the Standard Specifications Article 502.

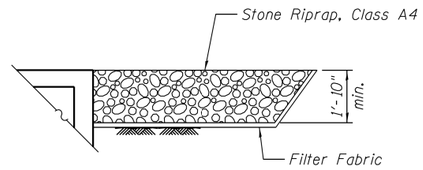
4. Reinforcement bars shall conform to the requirements of ASTM A 760 Gr 60. See Special Provisions.

5. Reinforcement bars designated (F) shall be epoxy coated

6. Diversion and Construction activities shall not be permitted to cause water levels upstream to rise more than 2'-0" above the normal pool elevation.

7. Cover from the face of Concrete to Reinforcement bars shall be 3" from surfaces formed against earth and 2" for all other surfaces unless otherwise shown.

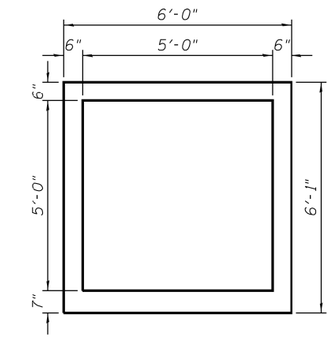
8. Chloride containing admixtures shall not be used for Class SI concrete.



SECTION A-A

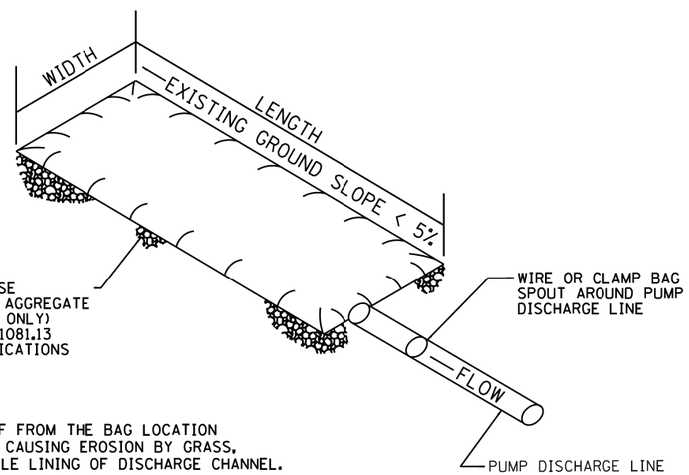
TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Removal and Disposal of Unsuitable Material	Cu. Yd.	8.0
Porous Granular Embankment	Cu. Yd.	8.0
Stone Riprap, Class A4	Sq. Yd.	28
Filter Fabric	Sq. Yd.	28
Concrete Removal	Cu. Yd.	1.7
Structure Excavation	Cu. Yd.	8.0
Reinforcement Bars, Epoxy Coated	Pound	6,130
Bicycle Railing	Foot	353
Box Culvert End Sections, Culvert 1	Each	1
Expansion Bolts 3/4 Inch	Each	24
Concrete Box Culverts	Cu. Yd.	20.3
Dewatering Structure No. 1	Each	1
Permanent Sheet Piling	Sq. Ft.	595
Temporary Soil Retention System	Sq. Ft.	131



SECTION THRU C.I.P. BARREL

Note: Contractor to verify dimensions of existing box culvert in the field.



ENSURE THAT RUNOFF FROM THE BAG LOCATION IS PREVENTED FROM CAUSING EROSION BY GRASS, STONE, OR GEOTEXTILE LINING OF DISCHARGE CHANNEL.

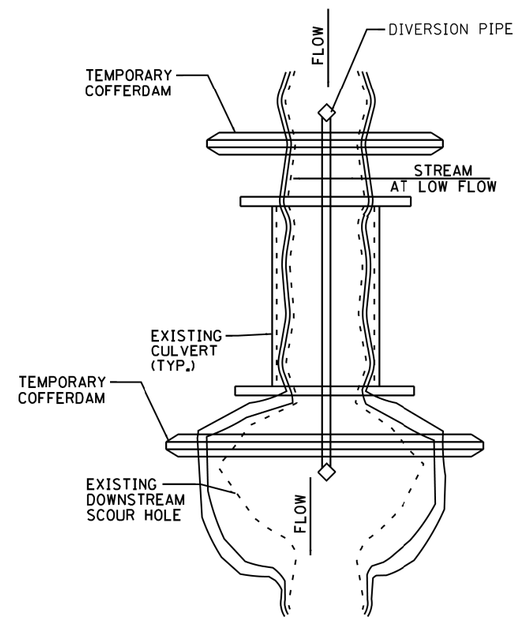
REMOVE SILT WHEN BAG IS HALF FULL OR MORE OFTEN IF RECOMMENDED BY THE MAUNFACUTURER. DISPOSE OF SILT BY TRANSPORTING THE FILLED BAG TO DISPOSAL AREA. SLIT THE BAG, BLEND SILT INTO EXISTING TOPOGRAPHY, SEED AND MULCH

MAINTAIN EXTRA FILTER BAGS ON SITE. REPLACE DAMAGED BAGS PROMPTLY.

TYPICAL FLOW RATE: 10 GPM x W x L

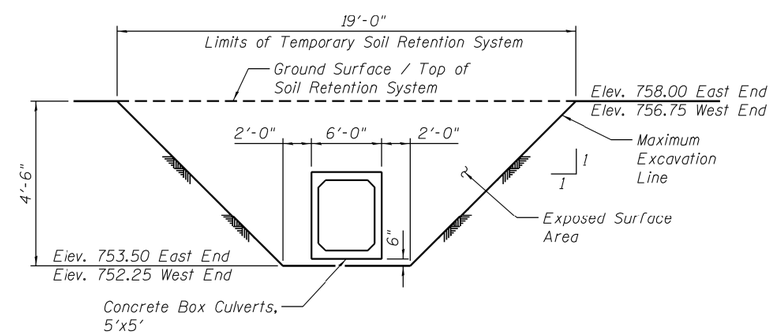
FIGURE S1. SEDIMENT FILTER BAG

NOTE: DEWATERING SYSTEM DETAILS SHALL BE USED IN CONJUCTION WITH THE PROJECT SPECIAL PROVISIONS.



TEMPORARY COFFERDAM WITH DIVERSION PIPE

NOTE: FLOW DEPTH < 1'-0" DURING DRY MONTHS AND LOW FLOW CONDITIONS.



TEMPORARY SOIL RETENTION SYSTEM DETAIL

- 2 Locations

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PROJECT CONTACT: $PROJECT_CONTACT$
CLIENT: $CLIENT$
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PLOT NAME: 01425-sediment-control.dgn
PLOT DRIVER: IL_Land_Desktop.cfg
PEN TABLE: plotlabel.tbl

HRGreen.com Illinois Professional Design Firm # 184-001322	USER NAME =	DESIGNED - TOR	REVISED ___	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES AND BILL OF MATERIAL STRUCTURE NO.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED - JMW	REVISED ___			0104	105N-4	LAKE	113	72
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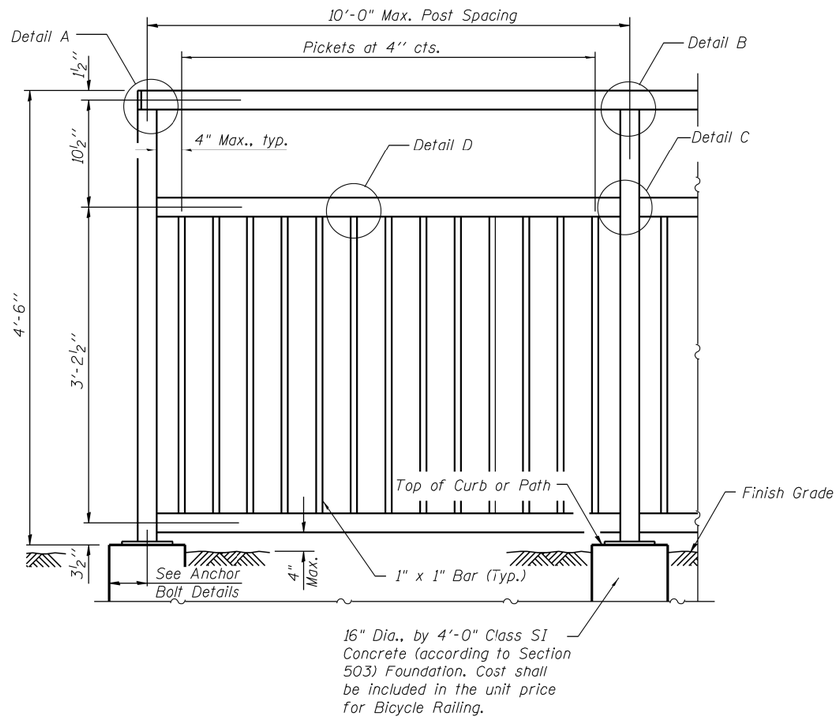
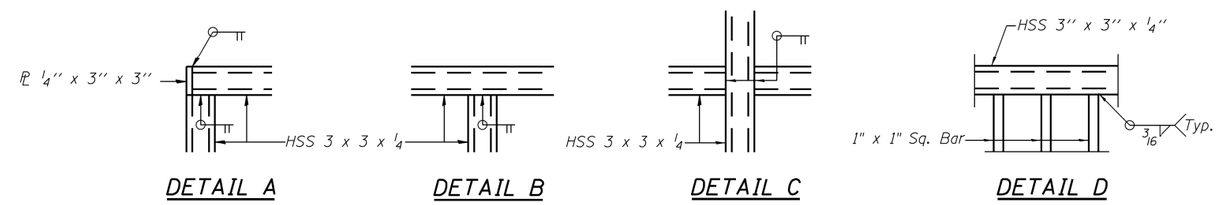
	PLOT DATE =	CHECKED - RGD	REVISED ___		SHEET NO. __ OF ___ SHEETS			ILLINOIS FED. AID PROJECT		

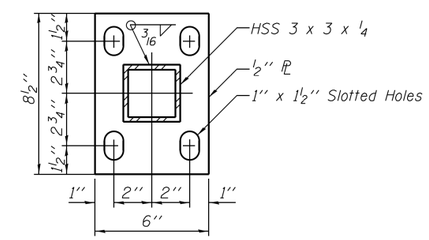


BICYCLE RAILING

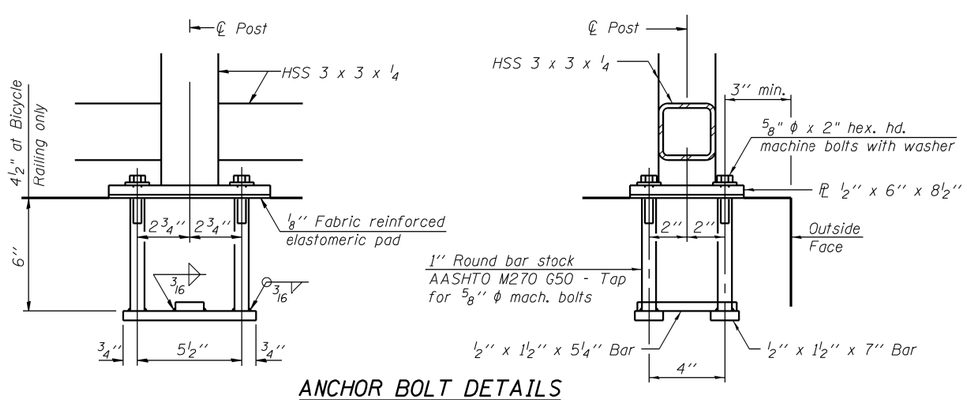


NOTES

All post, railing, pickets, splices, and anchor devices shall be powder coated black. The powder coating system to be used shall be approved by the engineer prior to the coating process.

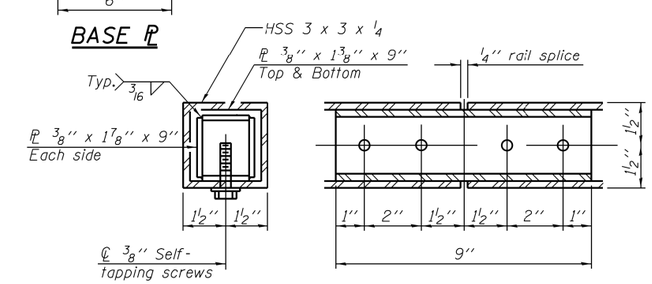


BASE PL



ANCHOR BOLT DETAILS

In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting ⅝'' ϕ anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications, but no greater than 6".



RAIL SPLICE

BILL OF MATERIAL

Item	Unit	Quantity
Bicycle Railing	Foot	353

COMPANY NAME: HRGREEN
PROJECT CONTACT: Sean Connor
CLIENT: Illinois Department of Transportation
DATE PLOTTED: 6/17/2014 8:12:57 AM
FILE NAME: P:\HRGSA\Bike Railing.dgn
PLOT DRIVER: ILLINOIS.pltcfg, Greenscale.pltcfg
PEN TABLE: Struct 11x17.tbl

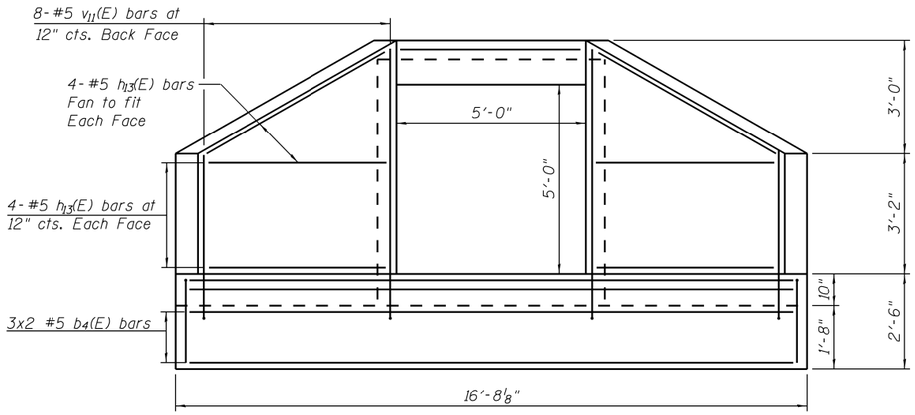
HRGreen
HRGreen.com
Illinois Professional Design Firm # 184-001322

USER NAME =	DESIGNED - TOR	REVISED	
	CHECKED - JMW	REVISED	
PLOT SCALE =	DRAWN - WJH	REVISED	
PLOT DATE =	CHECKED - RGD	REVISED	

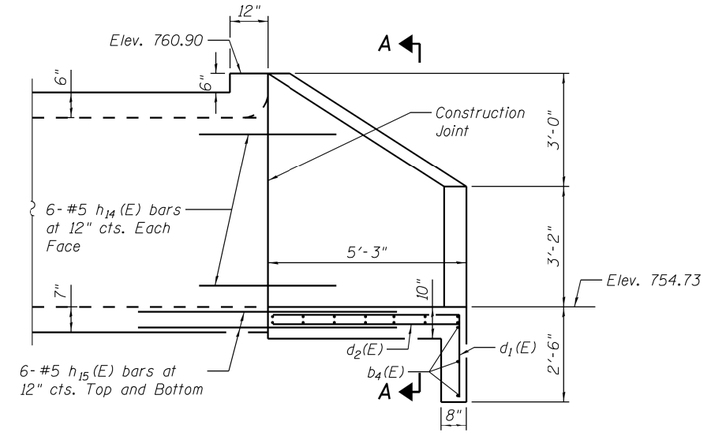
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BICYCLE RAILING
STRUCTURE NO.
SHEET NO. OF SHEETS

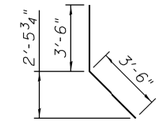
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0104	105N-4	LAKE	113	73
		CONTRACT NO. 12377		
	ILLINOIS	FED. AID PROJECT		



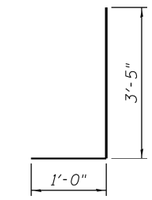
END ELEVATION



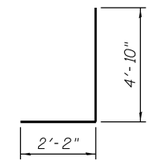
HALF SIDE ELEVATION



BAR h14(E)



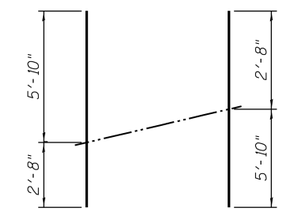
BAR n1(E)



BAR d1(E)

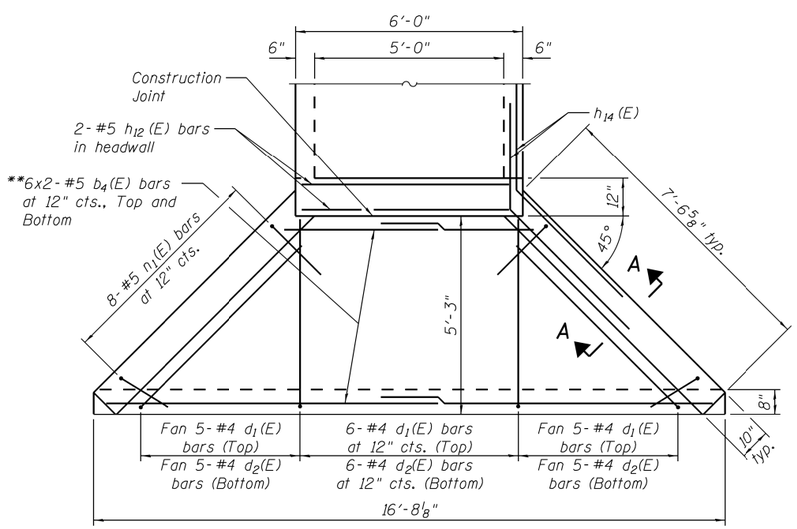
MINIMUM BAR LAP

#5 bar = 3'-3"



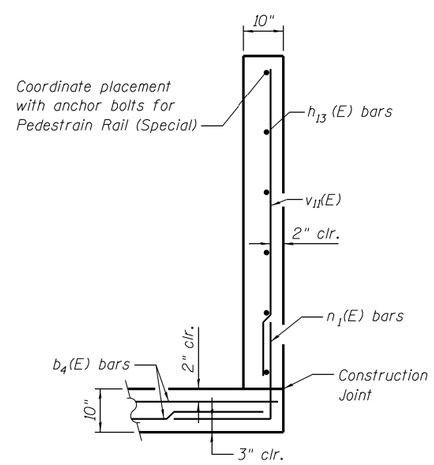
v11(E) FIELD CUTTING DIAGRAM

* Order v11(E) bars full length. Cut as shown and use remainder of bars in opposite wall.



** Increase lap to fit

PLAN



SECTION A-A

BILL OF MATERIAL FOR END SECTION

Bar	No.	Size	Length	Shape
b4(E)	30	#5	9'- 2"	
d1(E)	16	#4	7'- 0"	
d2(E)	16	#4	4'- 10"	
h13(E)	32	#5	7'- 1"	
h14(E)	24	#5	7'- 0"	
h15(E)	12	#5	6'- 10"	
n1(E)	16	#5	4'- 5"	
v11(E)	8	#5	8'- 6"	
Box Culvert End Sections, Culvert 1			Each	1
Reinforcement Bars, Epoxy Coated			Pound	1,060

BOX CULVERT END SECTIONS shall be paid for as noted in Article 540.08, and the Contract Unit Price for BOX CULVERT END SECTIONS shall include all porous granular bedding material, cast in place wingwalls, headwalls, and aprons, cast in place portions between cells, reinforcement, excavation, backfill, and preformed joint filler.

COMPANY NAME: HRGREEN
PROJECT CONTACT: Sean Connor
CLIENT: Illinois Department of Transportation
DATE PLOTTED: 6/17/2014 8:12:58 AM
FILE NAME: P:\45-0004-Twinbeds\t.dgn
PLOT DRIVER: ILDOT.pltcfg, Hybrid.pltcfg
PEN TABLE: Struct 11x17.tbl

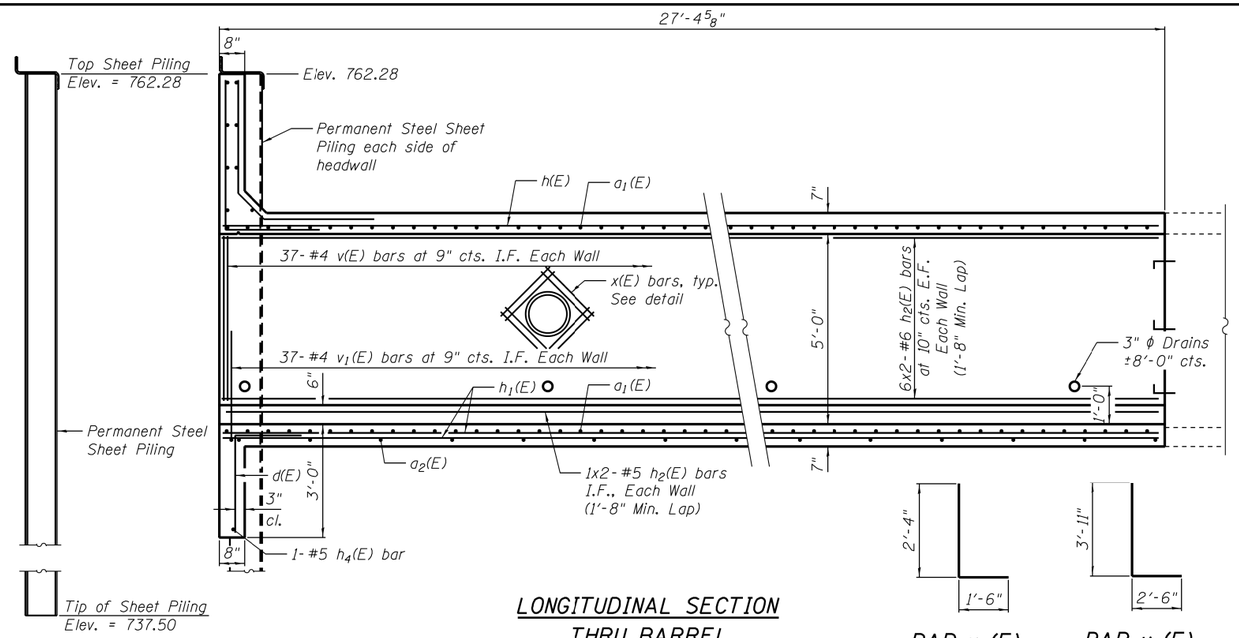
HRGreen.com
Illinois Professional Design Firm # 184-001322
HRGreen

USER NAME =	DESIGNED - TOR	REVISED	
	CHECKED - JMW	REVISED	
PLOT SCALE =	DRAWN - WJH	REVISED	
PLOT DATE =	CHECKED - RGD	REVISED	

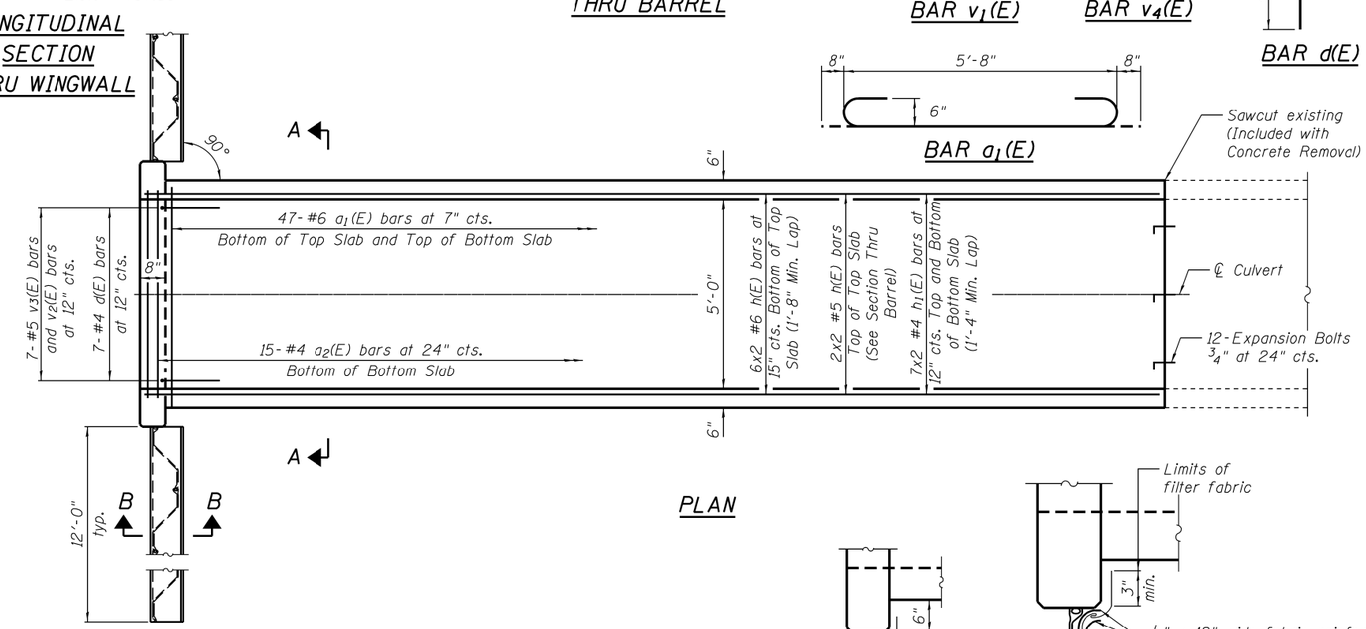
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

END SECTION DETAILS
STRUCTURE NO.
SHEET NO. OF SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0104	105N-4	LAKE	113	74
		CONTRACT NO. 12377		
		ILLINOIS FED. AID PROJECT		

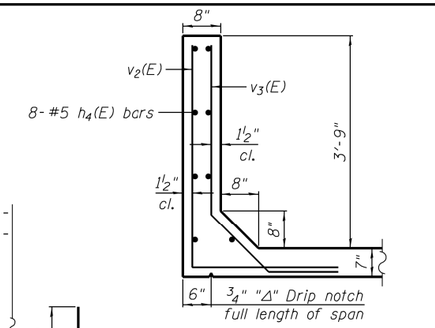


LONGITUDINAL SECTION THRU BARREL

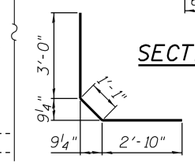


PLAN

LONGITUDINAL SECTION THRU WINGWALL



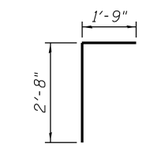
SECTION THRU HEADWALL



BAR v3(E)

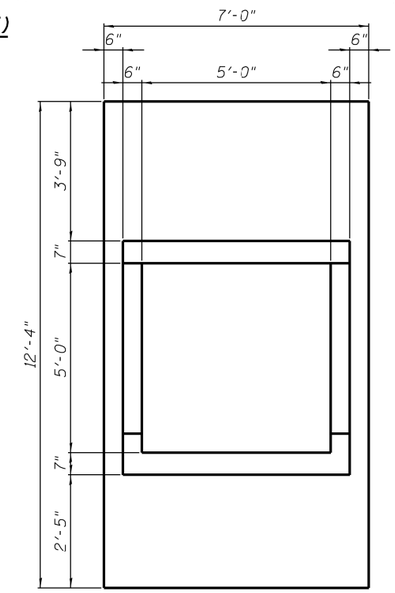
BAR v1(E)

BAR v4(E)

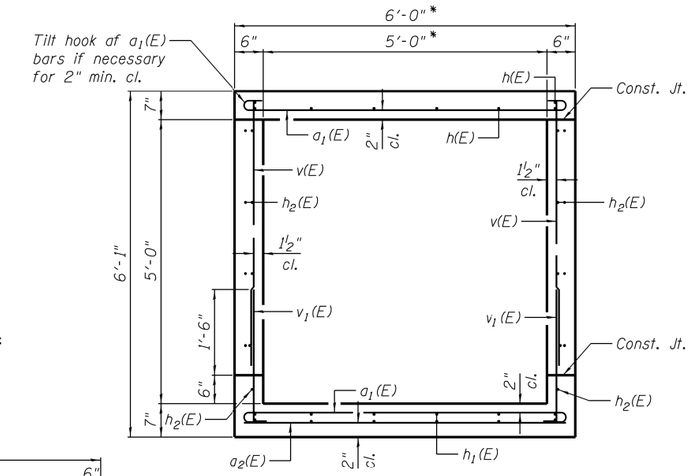


BAR d(E)

BAR a1(E)



SECTION A-A

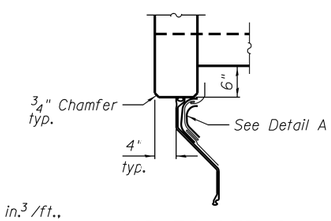


SECTION THRU BARREL

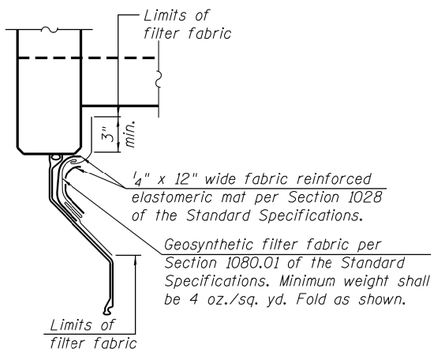
* Dim. of Existing Culvert opening to be verified in Field (Match Existing)

BILL OF MATERIAL

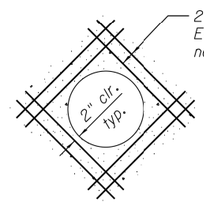
Bar	No.	Size	Length	Shape
a1(E)	94	#6	7'- 0"	
a2(E)	15	#4	5'- 8"	
d(E)	7	#4	4'- 5"	
h(E)	16	#6	14'- 5"	
h1(E)	28	#4	14'- 3"	
h2(E)	52	#6	14'- 5"	
h4(E)	9	#5	6'- 8"	
v(E)	74	#4	5'- 9"	
v1(E)	74	#4	3'- 10"	
v2(E)	7	#5	6'- 5"	
v3(E)	7	#5	6'- 11"	
x(E)	16	#5	3'- 0"	
Concrete Box Culverts			Cu. Yd.	13.3
Reinforcement Bars, Epoxy Coated			Pound	3,490
Expansion Bolts 3/4 Inch			Each	12



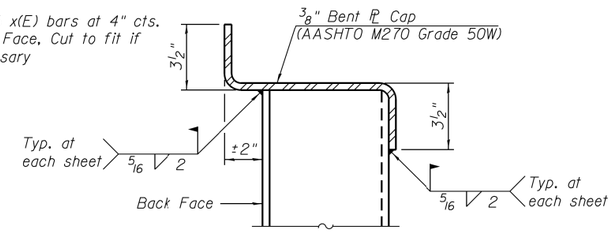
CORNER DETAIL



DETAIL A



DETAIL AT OPENING



SECTION B-B

Notes

1. Bars indicated thus 12 x 4-#5 etc. indicates 12 lines of bars with 4 lengths per line.
2. At the Contractor's option, a longer v1(E) bar may be ordered to replace the v(E) bar. No reduction in quantities shall be made for this substitution.
3. The minimum effective section modulus of the permanent steel sheet pile wall shall be 17 in.^{3}/ft., and the minimum embedment depth shall be 16'-0".
4. Sheet piling shall not be driven until the concrete strength has attained a minimum flexural strength of 650 psi or a minimum compressive strength of 3500 psi.
5. The cost of furnishing and installing the fabricated steel cap, elastomeric mat, and filter fabric shall be included in the cost of Permanent Steel Sheet Piling.
6. Expansion bolts shall be 3/4" ϕ hooked bolts. Hooked bolts shall extend a minimum of 9" into new concrete.

COMPANY NAME: $COMPANY_NAME$
PROJECT CONTACT: $PROJECT_CONTACT$
CLIENT: $CLIENT$
DATE PLOTTED: 8/16/2016 1:49:50 PM
FILE NAME: P:\Projects\Culvert101.dgn
PLOT DRIVER: IL_pdf_bw.pltcfg
PEN TABLE: pletable.tbl

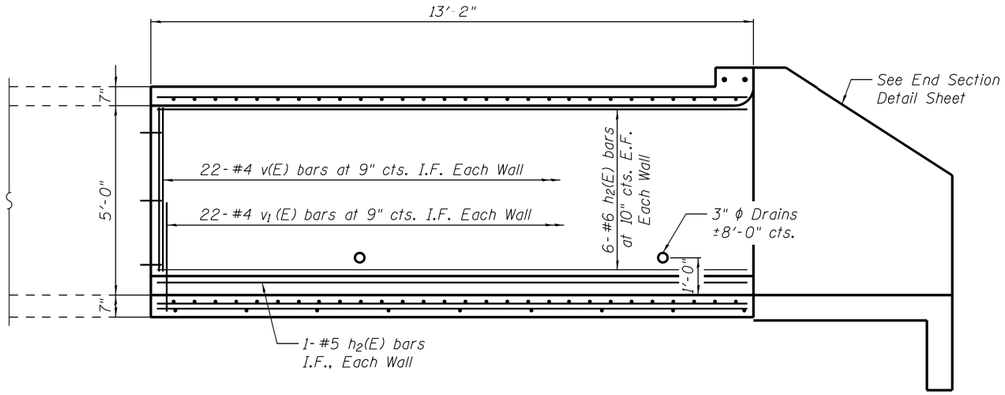
HRGreen
HRGreen.com
Illinois Professional Design Firm # 184-001322

USER NAME =	DESIGNED - TOR	REVISED ___
	CHECKED - JMW	REVISED ___
PLOT SCALE =	DRAWN - WJH	REVISED ___
PLOT DATE =	CHECKED - RGD	REVISED ___

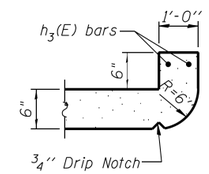
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CULVERT DETAILS
STRUCTURE NO.
SHEET NO. __ OF ___ SHEETS

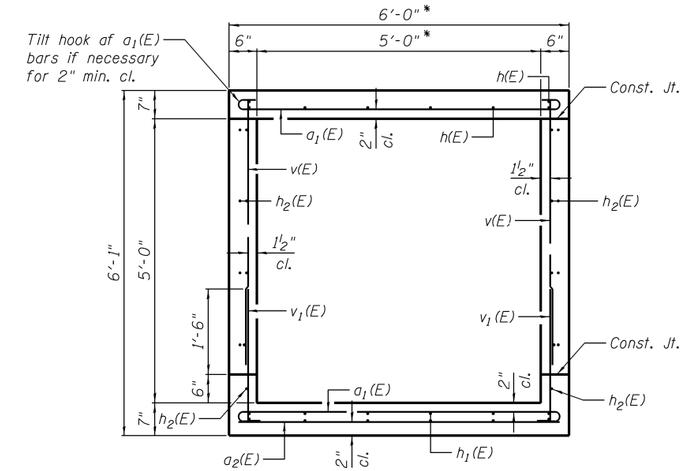
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0104	105N-4	LAKE	113	75
		CONTRACT NO. 12377		
		ILLINOIS FED. AID PROJECT		



LONGITUDINAL SECTION

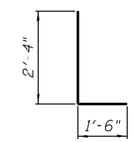


SECTION THRU HEADWALL

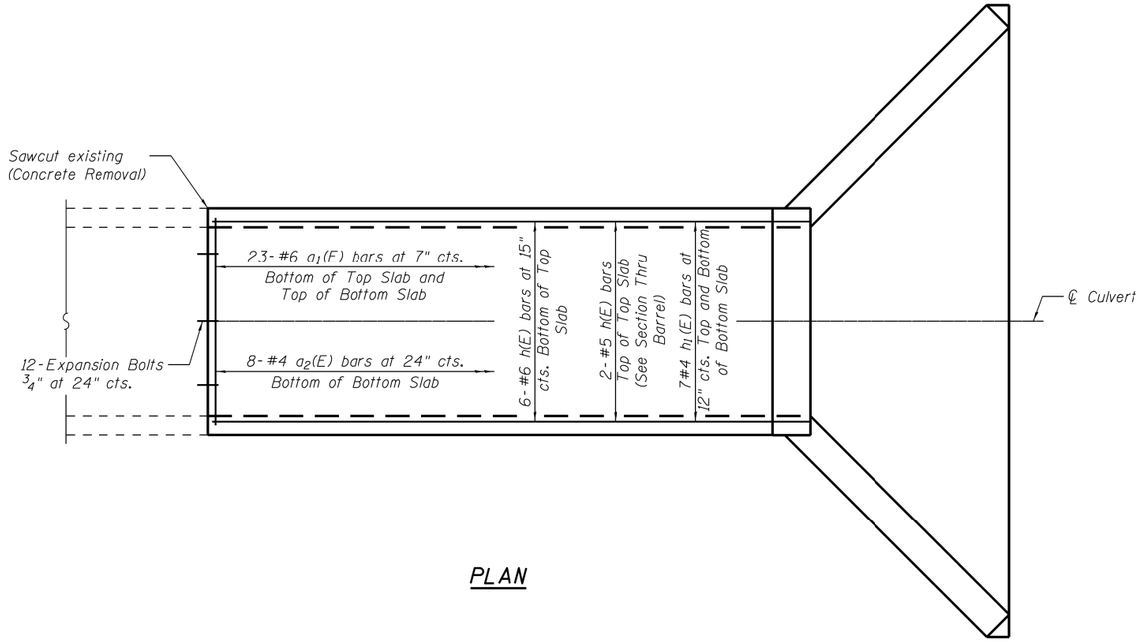


SECTION THRU BARREL

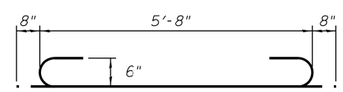
* Dim. of Existing Culvert opening to be verified in Field (Match Existing)



BAR v_1(E)



PLAN



BAR a_1(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a_1(E)	46	#6	7'- 0"	
a_2(E)	8	#4	5'- 8"	
h(E)	8	#6	12'- 10"	
h_1(E)	14	#4	12'- 10"	
h_2(E)	26	#6	12'- 10"	
h_3(E)	2	#5	5'- 8"	
v(E)	44	#4	5'- 9"	
v_1(E)	44	#4	3'- 10"	
Concrete Box Culverts			Cu. Yd.	7.0
Reinforcement Bars, Epoxy Coated			Pound	1,580
Expansion Bolts 3/4 Inch			Each	12

Notes

1. Bars indicated thus 12 x 4-#5 etc. indicates 12 lines of bars with 4 lengths per line.
2. At the Contractor's option, a longer v_1(E) bar may be ordered to replace the v(E) bar. No reduction in quantities shall be made for this substitution.
3. Expansion bolts shall be 3/4" ϕ hooked bolts. Hooked bolts shall extend a minimum of 9" into new concrete.

COMPANY NAME: HRGREEN
PROJECT CONTACT: Sean Connor
CLIENT: Illinois Department of Transportation
DATE PLOTTED: 6/17/2014 8:13:01 AM
PLOT NAME: pw:\\pwdesk-pw01.bentley.com:pwdesk-pw01\Documents\Projects\Culvert02.dgn
PLOT DRIVER: ILLINOIS.Cfg, Resolution.pltcfg
PEN TABLE: Struct 11x17.tbl

HRGreen.com
Illinois Professional Design Firm # 184-001322

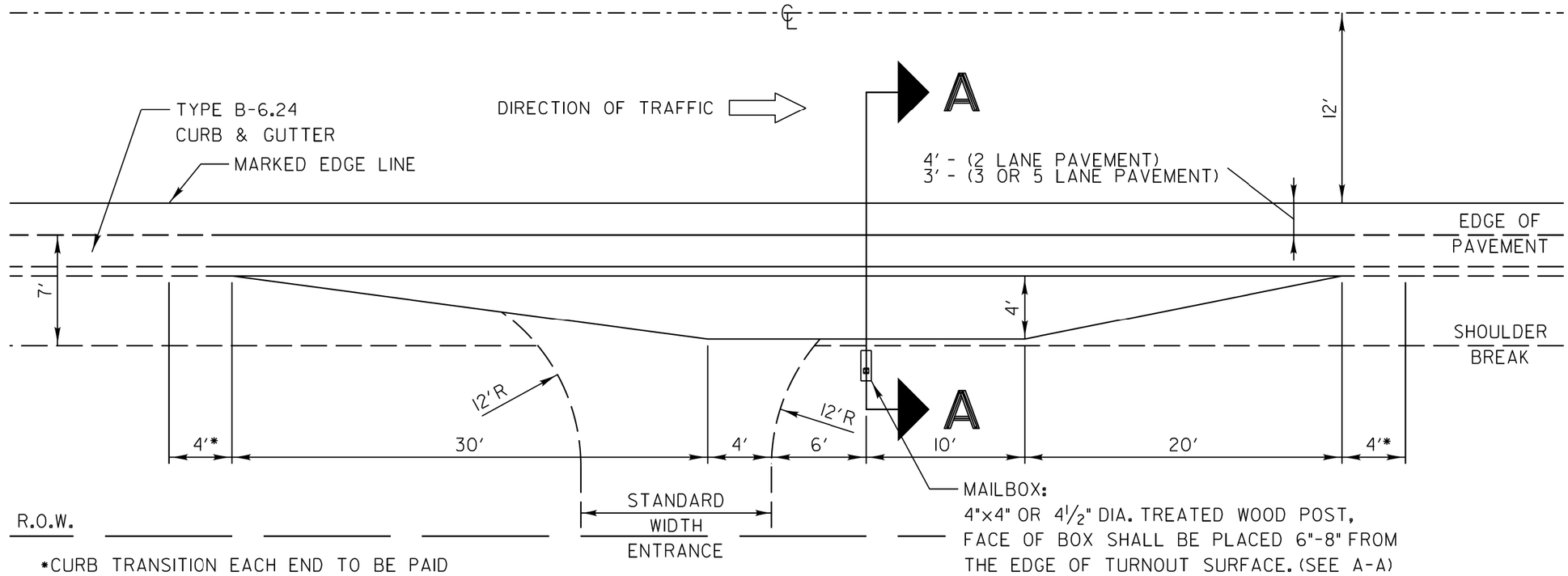
USER NAME =	DESIGNED - TOR	REVISED	
	CHECKED - JMW	REVISED	
PLOT SCALE =	DRAWN - WJH	REVISED	
PLOT DATE =	CHECKED - RGD	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

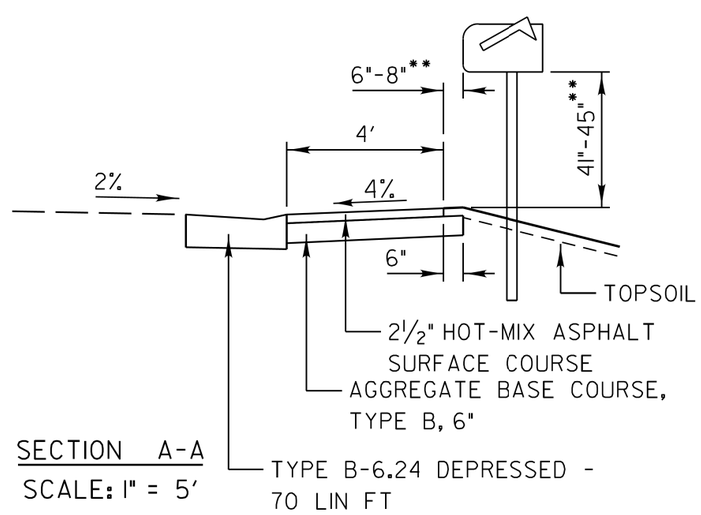
CULVERT DETAILS
STRUCTURE NO.
SHEET NO. OF SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0104	105N-4	LAKE	113	76

CONTRACT NO. 12377
ILLINOIS FED. AID PROJECT



*CURB TRANSITION EACH END TO BE PAID FOR AT THE CONTRACT UNIT PRICE FOR COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24.



SECTION A-A
SCALE: 1" = 5'

** USPS RESIDENTIAL MAILBOX STANDARD

DESIGN NOTES:

1. The Resident Engineer shall contact the local postmaster prior to installing the new mailbox posts for the local mounting height of the mailbox. The standard mounting height is 3½'; however, local post-masters may vary this height.
2. The standard mailbox post is a 4"x4" or a 4½" dia. treated wood post. The standard bury length of the post is 2'. Do not shorten or exceed this bury length.
3. Depending on the location of the shoulder break with respect to the edge of the mailbox turnout the overall post length may vary from 5' 8" to 6' 4" to maintain the 2' bury length and the 3½' mounting height.

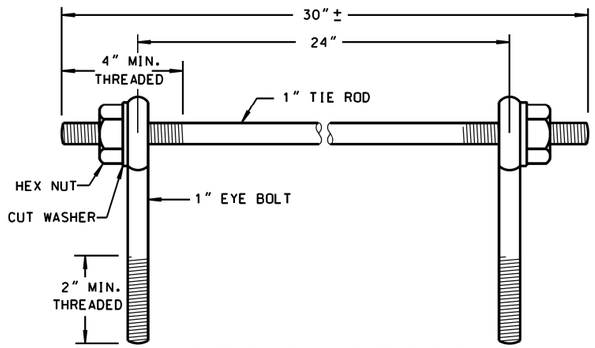
REVISIONS	DATE
6"-8" BOX OFFSET ADDED	12/22/11
Revised Type A to Type B Aggregate	12/20/12
Revised Curb Transition	1/7/15
Increased Paved Shldr to 3' for 3&5 Lane Pvmt	8/3/2015

LakeCounty
Division of Transportation

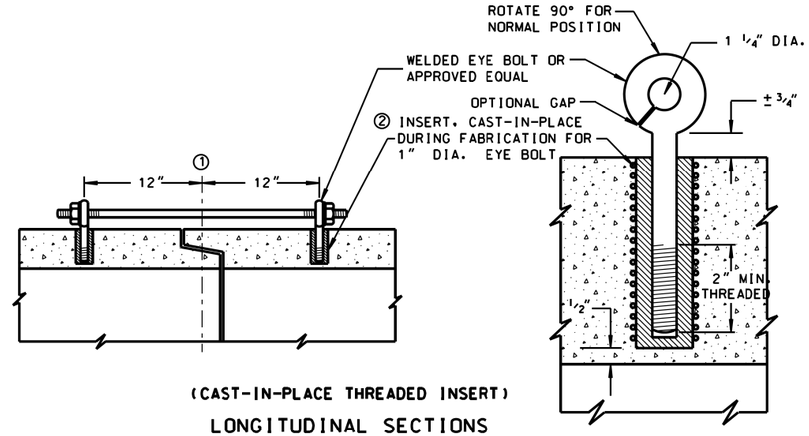
APPROVED BY: M. G. ZEMAITIS
DATE: APRIL 1, 2007

MAILBOX TURNOUT ALONG CURBED ROADS

LC4121



EYE BOLTS AND TIE ROD



(CAST-IN-PLACE THREADED INSERT)

LONGITUDINAL SECTIONS

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS. JOINT TIES AND HARDWARE SHALL BE GALVANIZED STEEL.

CONCRETE CULVERT PIPE AND SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT THE LAST THREE JOINTS BEFORE A FLARED END SECTION

THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR CONCRETE PIPE.

UNLESS OTHERWISE STATED IN THE CONTRACT THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE CULVERT PIPE AS INDICTED ON THE PLANS AND BY THIS DETAIL WILL BE CONSIDERED INCLUDED IN THE COST OF PIPE CULVERTS OR STORM SEWERS.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR GALVANIZED STEEL JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

① ℄ OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.

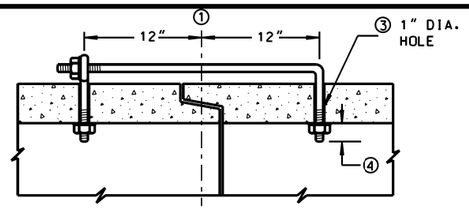
② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.

③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED 12" FROM ℄ OF TONGUE AND GROOVE.

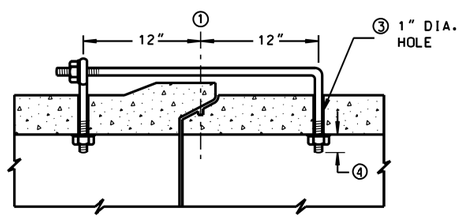
④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2".

⑤ OPENING TO BE ROD DIAMETER + 1 INCH.

⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.



(TONGUE & GROOVE PIPE)

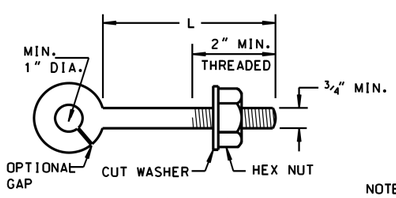


(MODIFIED BELL PIPE)

LONGITUDINAL SECTION

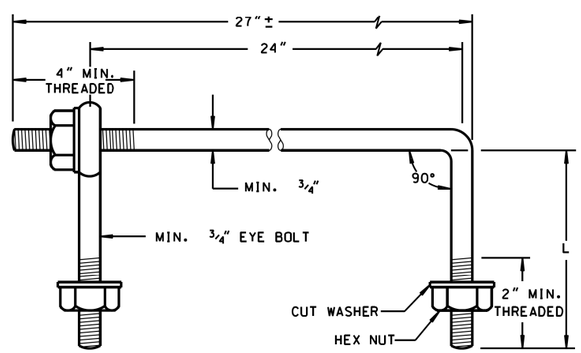
EYE BOLT DIMENSION TABLE

PIPE SIZE	L = LENGTH	
	TONGUE & GROOVE PIPE	MODIFIED BELL PIPE
18" TO 24"	4 1/2"	6 1/4"
30"	5"	7"
36"	5 1/2"	7"
42"	6"	
48"	6 1/2"	
60"	7 1/2"	
66"	8"	



EYE BOLT

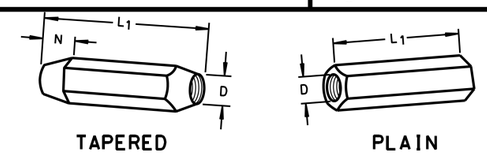
NOTE:
TWO EYE BOLTS MAY BE USED WITH A 30" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.



EYE BOLT AND TIE ROD

(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)

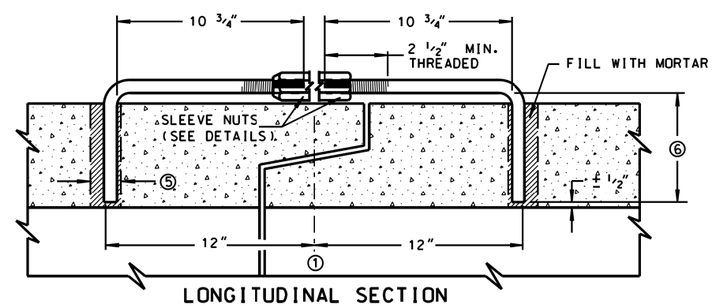
EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)



TAPERED PLAIN

RIGHT AND LEFT THREADS

SLEEVE NUTS



LONGITUDINAL SECTION

(JOINT TIES FOR 12" TO 108" DIA. CONCRETE PIPE)

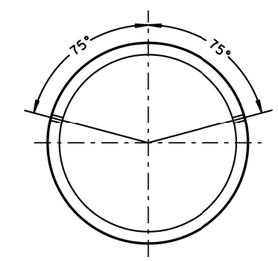
ADJUSTABLE TIE ROD (ALTERNATE NO. 3)

ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L_1	N
12-60	5/8	5/8	5	1/2
66-84	3/4	3/4	5	1/2
90-108	1	1	7	1 7/16

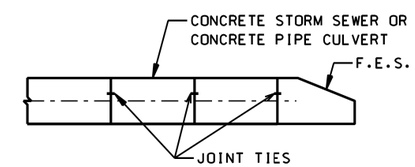
DIMENSIONS SHOWN ARE IN INCHES

NOT TO SCALE



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



TYPICAL JOINT TIE LOCATIONS

REVISIONS	DATE
ADDED JOINT TIE MAT'L	7/8/11

LakeCounty Division of Transportation

APPROVED BY: M. ZEMAITIS
DATE: 04/01/11

JOINT TIES FOR CONCRETE PIPE

LC5402

REVISIONS / REMARKS					
NO.	DESCRIPTION	DATE	BY	SURVEYOR:	/
		/ /		DSGNR/LIAISON:	
		/ /		PLOTTED BY:	hdrmc 7/15/2011

LakeCounty Division of Transportation

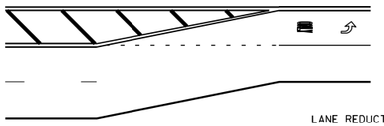
PROJECT NAME: LAKE COUNTY STANDARDS & DETAILS

ROUTE	SECTION	SECTION NUMBER	SHEET	SHEETS
CH		- - -	XX	XX

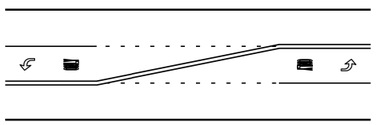
FILE NAME: C:\Documents and Settings\hdrmc\My Documents\To ITS\LC5402 JOINT TIES FOR CONCRETE PIPE.dgn

TYPICAL PAVEMENT MARKINGS

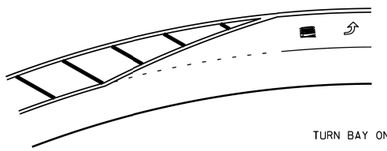
TYPICAL MINI-SKIP PAVEMENT MARKINGS



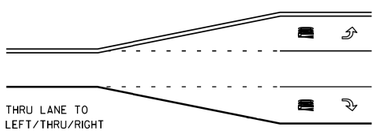
LANE REDUCTION



DOUBLE TAPER



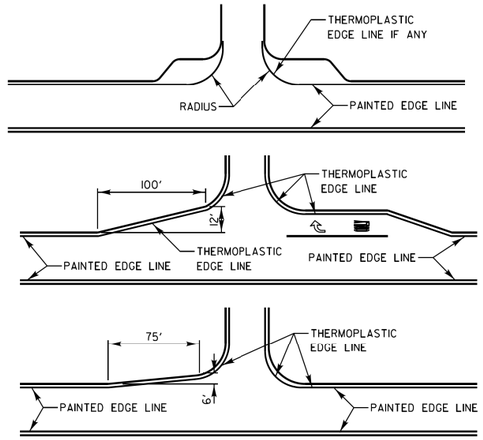
TURN BAY ON CURVE



THRU LANE TO
LEFT/THRU/RIGHT

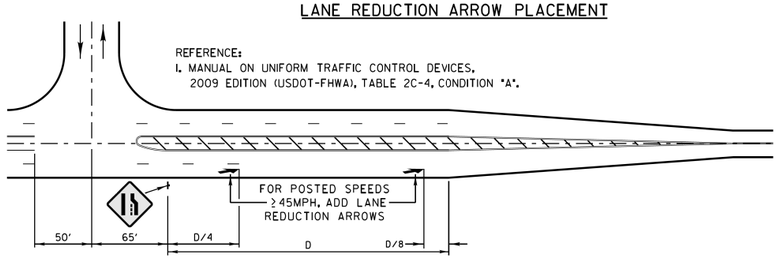
MINI-SKIPS ARE 2 FEET WHITE LINE WITH 6 FEET SPACING. THE MINI-SKIP IS THE SAME WIDTH AS THE PAVEMENT MARKING LINE, IT EXTENDS.

EDGE LINE RADII AT SIDE STREETS



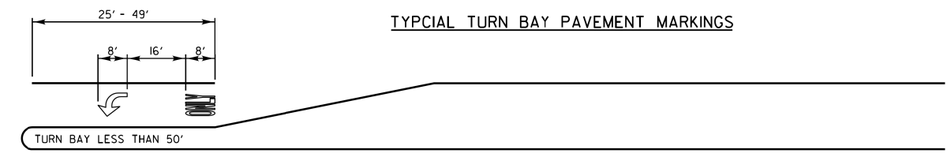
LANE REDUCTION ARROW PLACEMENT

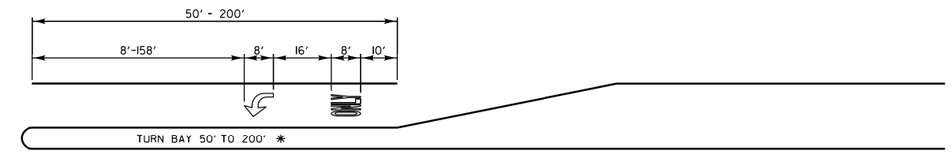
REFERENCE:
1. MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, 2009 EDITION (USDOT-FHWA), TABLE 2C-4, CONDITION "A".

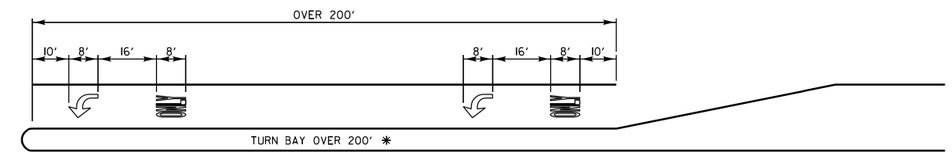


DESIGN SPEED MPH	D (FEET)
<45	N/A
45	775' [1]
50	885' [1]
55	990' [1]

TYPICAL TURN BAY PAVEMENT MARKINGS







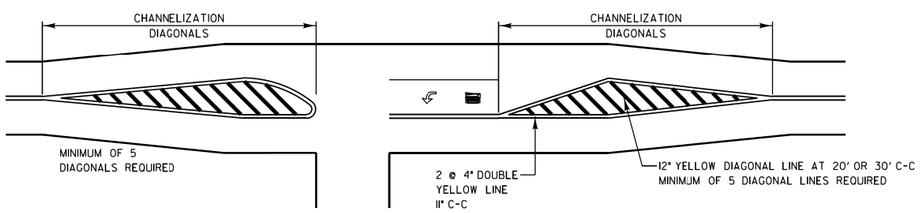
AREA = 15.6 SQ. FT.

✱ AT INTERSECTIONS WITH VIDEO DETECTION, THE ARROW AND ONLY PAVEMENT MARKINGS SHALL BE A MINIMUM OF 30' BEHIND THE STOP BAR.

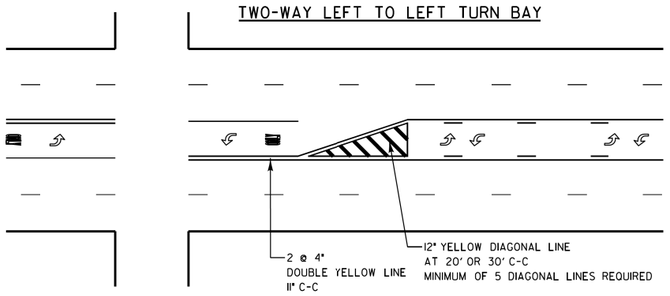
ONLY AREA = 20.8 SQ FT.

FULL SIZE LETTERS (8') AND ARROWS SHALL BE USED. TURN LANES IN EXCESS OF 400' IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW W/ "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW W/ "ONLY".

TWO LANE ROAD



TWO-WAY LEFT TO LEFT TURN BAY

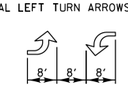


TYPICAL DIAGONAL SPACING

DIAGONAL SPACING

SPEED LIMIT RANGE	CONTINUOUS	INTERSECTION CHANNELIZATION
30-45 MPH	75 FT.	20 FT.
OVER 45 MPH	150 FT.	30 FT.

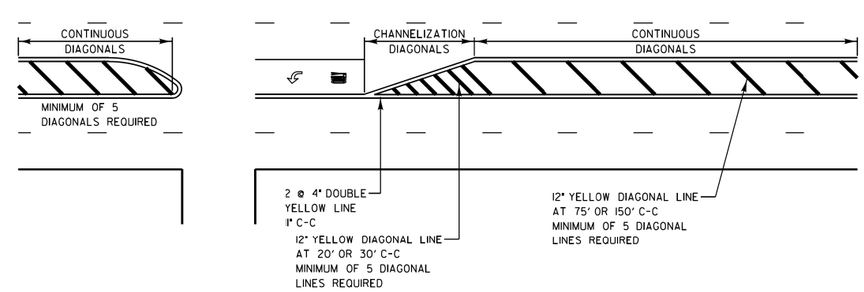
DUAL LEFT TURN ARROWS



31.2 SQ. FT. MINIMUM
OF 2 SETS REQUIRED

A MINIMUM OF TWO PAIRS OF DUAL LEFT TURN ARROWS SHALL BE USED. THE DUAL LEFT TURN ARROWS SHALL BE WHITE IN COLOR. THE INTERVAL BETWEEN SETS OF DUAL LEFT TURN ARROWS SHOULD BE 200' AND 300'.

3 OR 5 LANE ROAD



REVISIONS	DATE
SEPARATED RAILROAD SHEET	06/02/08
ADDED LANE REDUCTION ARROWS	07/1/12
RAISED TO RECESSED MARKERS	12/2/13
REMOVE STATION NUMBERS	5/30/14

LakeCounty
Division of Transportation

APPROVED BY: A. KHAWAJA
DATE: APRIL 1, 2007

TYPICAL PAVEMENT MARKINGS FOR COUNTY HIGHWAYS

SHEET 1 OF 2

LC7800

REVISIONS / REMARKS					
NO.	DESCRIPTION	DATE	BY	SURVEYOR:	/
		/ /		DSGNR/LIAISON:	
		/ /		PLOTTED BY:	hdrmc 5/30/2014

LakeCounty
Division of Transportation

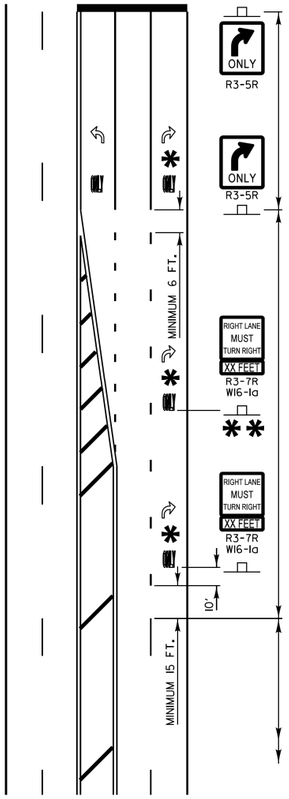
PROJECT NAME
LAKE COUNTY STANDARDS & DETAILS

ROUTE	SECTION	SECTION NUMBER	SHEET	SHEETS
CHXX	XXX	XX-XXXXX-XX-XX	XXX	XXX

FILE NAME: C:\Users\hdrmc\Desktop\LC7800 TYPICAL PAVEMENT MARKINGS FOR COUNTY HIGHWAYS.dgn

TYPICAL PAVEMENT MARKINGS AND RECESSED PAVEMENT MARKERS

THRU LANE TO TURN LANE CONVERSION



TURN LANE
6" WHITE LINE
(ADDITIONAL PAVEMENT MARKINGS AS SHOWN ON SHEET ONE OF THE L.C.D.O.T TYPICAL PAVEMENT MARKINGS DETAIL SHEETS)

TRANSITION ZONE
6" WHITE 3'/12' SKIP DASH
LANE LINE

THRU LANE
4 " WHITE 10'/30' SKIP DASH
LANE LINE

MINIMUM TRANSITION ZONE LENGTH

POSTED SPEED	LENGTH
25 M.P.H.	255 FT
30 M.P.H.	330 FT
35 M.P.H.	405 FT
40 M.P.H.	480 FT
45 M.P.H.	555 FT
50 M.P.H.	630 FT
55 M.P.H.	705 FT

* LOCATION OF PAV'T MARKINGS

(MEASURED FROM BEGINNING OF TRANSITION ZONE)

POSTED SPEED	LOCATION OF PAV'T MARKINGS
25 M.P.H.	10 FT, 260 FT
30 M.P.H.	10 FT, 170 FT, 340 FT
35 M.P.H.	10 FT, 210 FT, 410 FT
40 M.P.H.	10 FT, 170FT, 330 FT, 490 FT
45 M.P.H.	10 FT, 190 FT, 370 FT, 560 FT
50 M.P.H.	10 FT, 170 FT, 330 FT, 490 FT, 640 FT
55 M.P.H.	10 FT, 180 FT, 350 FT, 520 FT, 710 FT

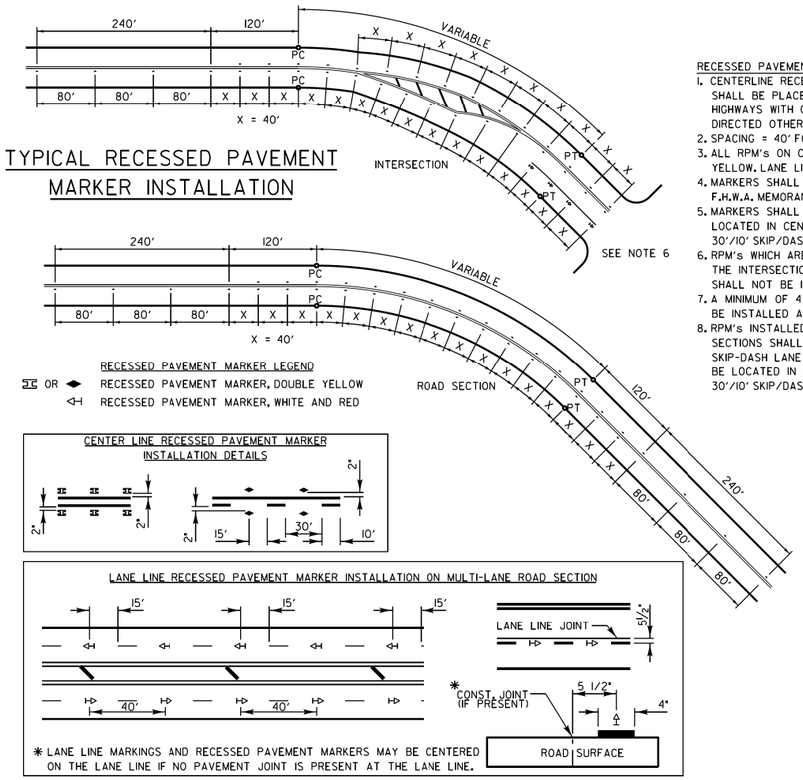
** FOR POSTED SPEEDS 40 M.P.H. OR GREATER A SECOND R3-7/W16-1a SIGN INSTALLATION SHALL BE LOCATED HALFWAY BETWEEN THE BEGINNING OF THE TRANSITION ZONE AND THE BEGINNING OF THE TURN LANE

PAVEMENT MARKING GUIDELINES

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE OF 2 LANE PAVEMENT	4 IN.	SKIP-DASH	YELLOW	10 FT. LINE WITH 30 FT. SPACE
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 IN. 2 @ 4 IN.	SOLID SOLID	YELLOW YELLOW	5 1/2 IN. C-C FROM SKIP-DASH CENTERLINE 11 IN. C-C (OMIT SKIP-DASH CENTERLINE BETWEEN)
CENTERLINE ON MULTI-LANE UNDIVIDED	2 @ 4 IN.	SOLID	YELLOW	11 IN. C-C
LANE LINES	4 IN.	SKIP-DASH	WHITE	10 FT. LINE WITH 30 FT. SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2 FT. LINE WITH 6 FT. SPACE
EDGE LINES	5 IN. WHITE 4 IN. YELLOW	SOLID	WHITE - RIGHT YELLOW - LEFT	OUTLINE RAISED MEDIANS IN YELLOW
TURN LANE MARKINGS	6 IN. LINE FULL SIZE LETTERS AND SYMBOLS (8 FT.)	SOLID	WHITE	TURN ARROW 15.6 SQ. FT. STRAIGHT ARROW 11.5 SQ. FT. ONLY 20.8 SQ. FT. COMB. ARROW 26.0 SQ. FT.
TWO WAY LEFT TURN MARKING	2 @ 4 IN. EACH DIRECTION 8 FT. LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10 FT. LINE WITH 30 FT. SPACE FOR SKIP-DASH 5 1/2 IN. C-C BETWEEN SKIP-DASH LINE AND SOLID LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK	12 IN. @ 90°	SOLID	WHITE	12 IN. LONGITUDINAL BAR WITH 24 IN. SPACE 6 FT. TO 12 FT. WIDE SEE TYPICAL CROSSWALK MARKING DETAIL
STOP BARS	24 IN.	SOLID	WHITE	PLACE 4 FT. IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE PLACE AT DESIRED STOPPING POINT.
PAINTED MEDIANS	2 @ 4 IN. & 11 IN. C-C 12 IN. DIAGONALS @ 45° NO DIAGONALS USED FOR 4 FT. WIDE MEDIAN	SOLID	YELLOW: 2-WAY TRAFFIC WHITE: 1-WAY TRAFFIC	11 IN. C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING DETAIL MINIMUM OF 5 DIAGONALS
GORE MARKING AND CHANNELIZING LINES	8 IN. WITH 12 IN. DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS 15 FT. C-C (LESS THAN 30 M.P.H.) 20 FT. C-C (30 TO 45 M.P.H.) 30 FT. C-C (OVER 45 M.P.H.) MINIMUM OF 5 DIAGONALS
R.R. CROSSING	24 IN. TRANSVERSE LINES RR IS 6 FT. LETTER 16 IN. LINE FOR "X"	SOLID	WHITE	SEE I.D.O.T. STD. 78000I SQ. FT. AREA OF: "R" = 3.6 SQ. FT. / "R" "X" = 54.0 SQ. FT.
SHOULDER DIAGONALS	12 IN. @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50 FT. C-C (LESS THAN 30 M.P.H.) 75 FT. C-C (30 TO 45 M.P.H.) 150 FT. C-C (OVER 45 M.P.H.) MINIMUM OF 5 DIAGONALS

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO PART III "MARKINGS" IN THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", AND THE LATEST I.D.O.T. HIGHWAY STANDARD 78000I.

TYPICAL RECESSED PAVEMENT MARKER INSTALLATION



RECESSED PAVEMENT MARKER LEGEND

OR RECESSED PAVEMENT MARKER, DOUBLE YELLOW

RECESSED PAVEMENT MARKER, WHITE AND RED

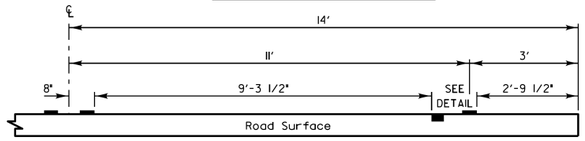
CENTER LINE RECESSED PAVEMENT MARKER INSTALLATION DETAILS

LANE LINE RECESSED PAVEMENT MARKER INSTALLATION ON MULTI-LANE ROAD SECTION

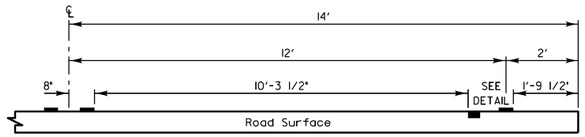
* LANE LINE MARKINGS AND RECESSED PAVEMENT MARKERS MAY BE CENTERED ON THE LANE LINE IF NO PAVEMENT JOINT IS PRESENT AT THE LANE LINE.

PAVEMENT CROSS SECTION SHOWING TYPICAL PAVEMENT MARKINGS

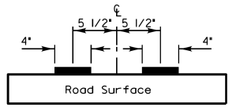
(2-LANE ROADWAY)



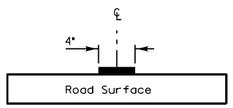
11' LANE WIDTH



12' LANE WIDTH



DOUBLE STRIPE CENTERLINE DETAIL



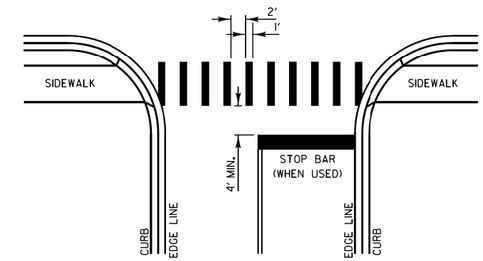
SINGLE STRIPE CENTERLINE DETAIL

NOTE:
CENTERLINE MARKINGS ARE 4" LINES AT 11" CENTERS.

RECESSED PAVEMENT MARKER NOTES:

1. CENTERLINE RECESSED PAVEMENT MARKERS (RPM's) SHALL BE PLACED ON ALL TWO AND THREE LANE HIGHWAYS WITH CURVES OVER 3 1/2 DEGREES, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
2. SPACING = 40' FOR CENTERLINE MARKERS.
3. ALL RPM's ON CENTERLINE ARE 2-WAY YELLOW. LANE LINE MARKERS ARE WHITE/RED.
4. MARKERS SHALL BE INSTALLED ACCORDING TO F.H.W.A. MEMORANDUM HIO-21.
5. MARKERS SHALL BE FIELD ADJUSTED TO BE LOCATED IN CENTER OF THE 30' GAP OF A 30'/10' SKIP/DASH CENTERLINE.
6. RPM's WHICH ARE TO BE LOCATED WITHIN THE INTERSECTION OF A CROSS STREET, SHALL NOT BE INSTALLED.
7. A MINIMUM OF 4 WHITE/RED MARKERS SHALL BE INSTALLED ALONG THE TURN LANE LINE.
8. RPM's INSTALLED ON MULTI-LANE ROAD SECTIONS SHALL BE INSTALLED ON THE WHITE SKIP-DASH LANE LINE ONLY. THESE RPM's SHALL BE LOCATED IN CENTER OF THE 30' GAP OF A 30'/10' SKIP/DASH LANE LINE.

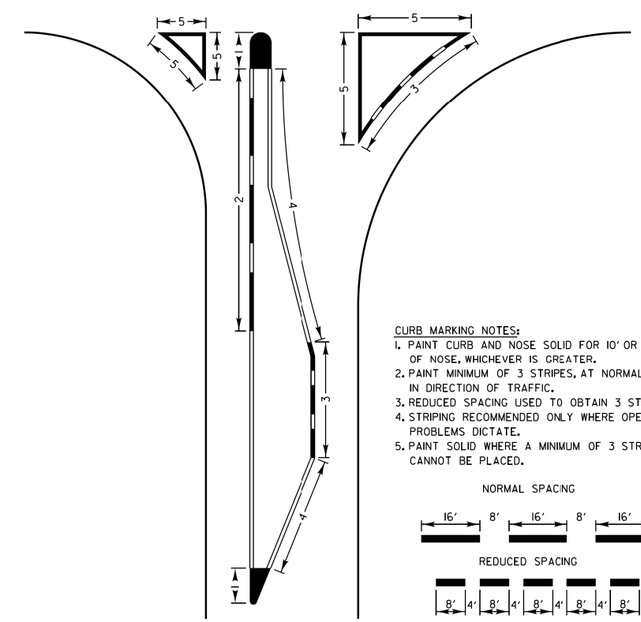
CROSSWALKS



CROSSWALK NOTES:

1. WIDTH OF THE CROSSWALK IS GENERALLY 6' EXCEPT AT SCHOOL CROSSINGS AND BICYCLE CROSSINGS, WHICH CAN BE 8'.
2. THE STOP BAR SHOULD BE INSTALLED A MINIMUM OF 4' IN ADVANCE OF THE CROSSWALK.

CURB MARKING



CURB MARKING NOTES:

1. PAINT CURB AND NOSE SOLID FOR 10' OR RADIUS OF NOSE, WHICHEVER IS GREATER.
2. PAINT MINIMUM OF 3 STRIPES, AT NORMAL SPACING, IN DIRECTION OF TRAFFIC.
3. REDUCED SPACING USED TO OBTAIN 3 STRIPE MINIMUM.
4. STRIPING RECOMMENDED ONLY WHERE OPERATIONAL PROBLEMS DICTATE.
5. PAINT SOLID WHERE A MINIMUM OF 3 STRIPES CANNOT BE PLACED.

NORMAL SPACING

REDUCED SPACING

REVISIONS	DATE
SEPARATED RAILROAD SHEET	06/02/08
REVISED RPM DETAILS	07/1/12
RAISED TO RECESSED MARKERS	12/2/13
REMOVE STATION NUMBERS	5/30/14

LakeCounty
Division of Transportation

APPROVED BY: A. KHAWAJA
DATE: APRIL 1, 2007

TYPICAL PAVEMENT MARKINGS FOR COUNTY HIGHWAYS

SHEET 2 OF 2

LC7800

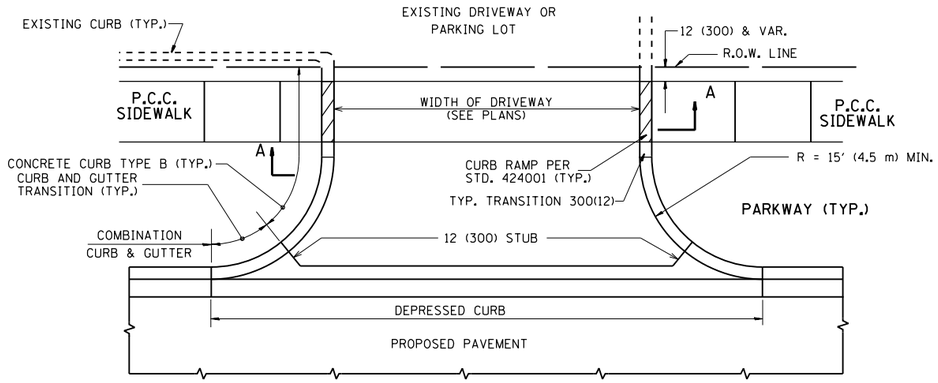
REVISIONS / REMARKS					
NO.	DESCRIPTION	DATE	BY	SURVEYOR:	/
		/ /		DSGNR/LIAISON:	
		/ /		PLOTTED BY:	hdrmc 5/30/2014

LakeCounty
Division of Transportation

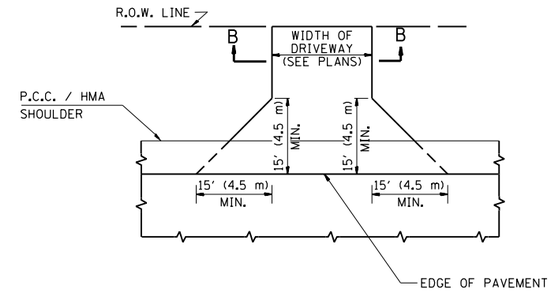
PROJECT NAME
LAKE COUNTY STANDARDS & DETAILS

ROUTE	SECTION	SECTION NUMBER	SHEET	SHEETS
CHXX	XXX	XX-XXXXX-XX-XX	XXX	XXX

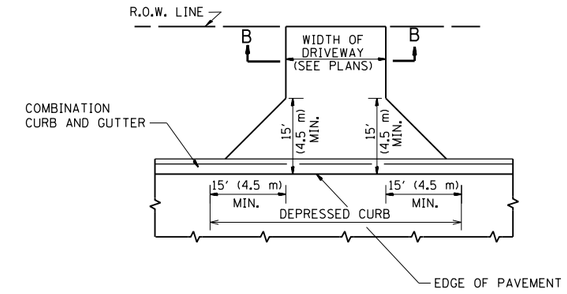
FILE NAME: C:\Users\hdrmc\Desktop\LC7800 TYPICAL PAVEMENT MARKINGS FOR COUNTY HIGHWAYS.dgn



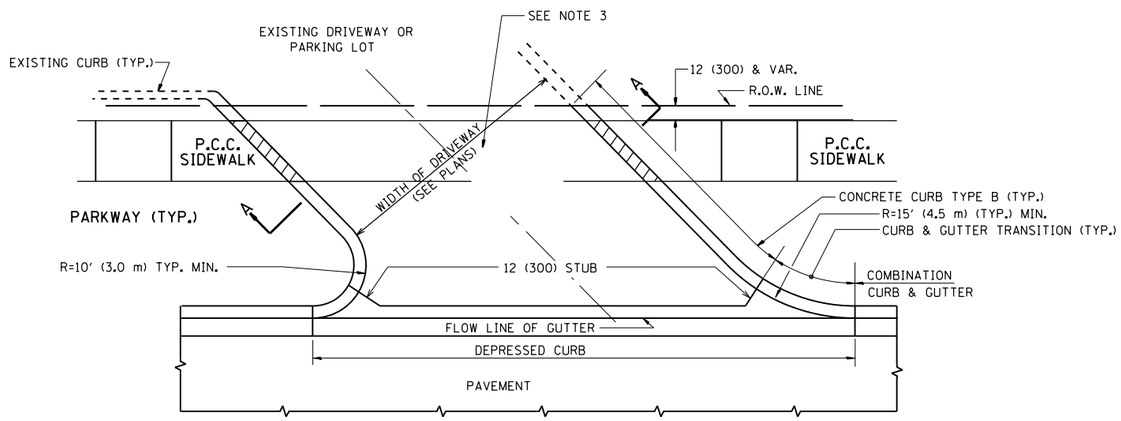
WITH CONCRETE CURB, TYPE B



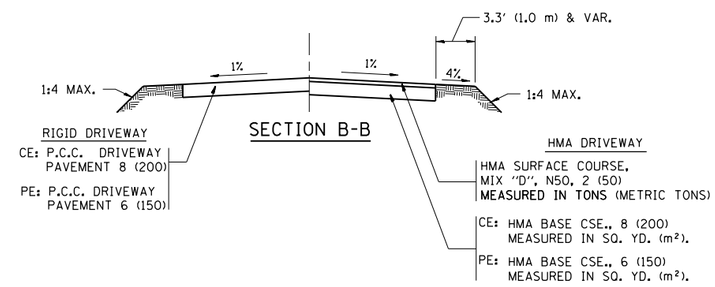
ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



WITH CONCRETE CURB, TYPE B

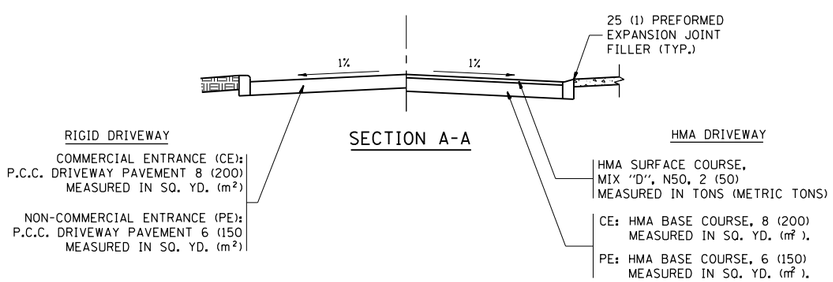


SECTION B-B

RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m^2).



SECTION A-A

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454\Drawings\Data\Design\DistStd.dgn	USER NAME = ledezmarm	DESIGNED - R. SHAH	REVISED - P. LaFLUER 04-15-03
			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 06-11-08
	PLOT DATE = 8/19/2016	DATE - 11-04-95	REVISED - R. BORO 09-06-11

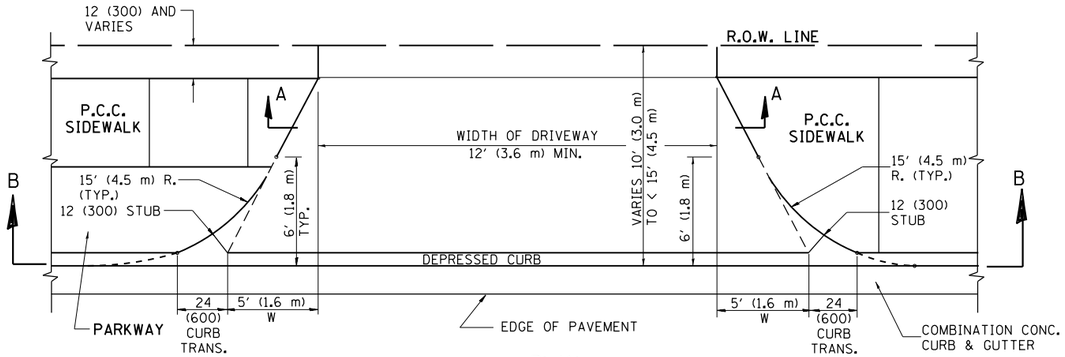
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA.

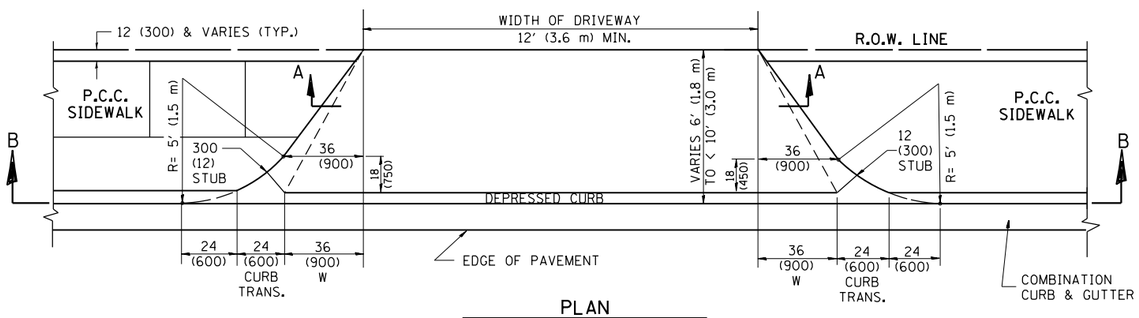
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	81
	BD0156-07 (BD-01)	CONTRACT NO. 60W06		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT



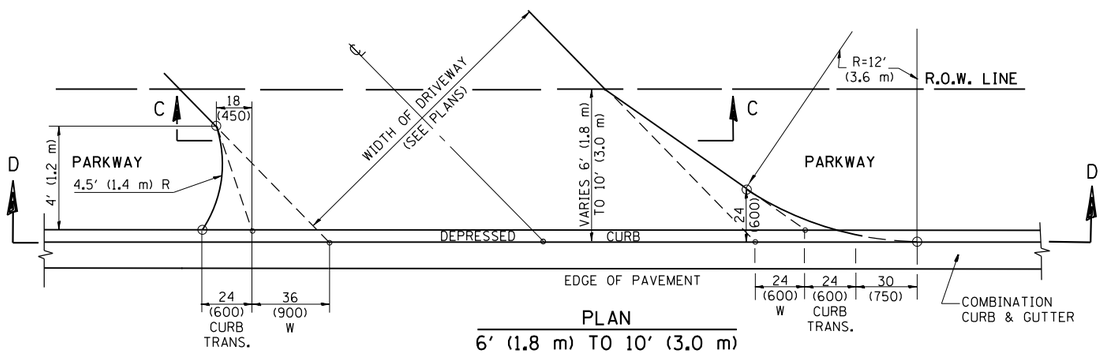
PLAN

10' (3.0 m) TO < 15' (4.5 m)



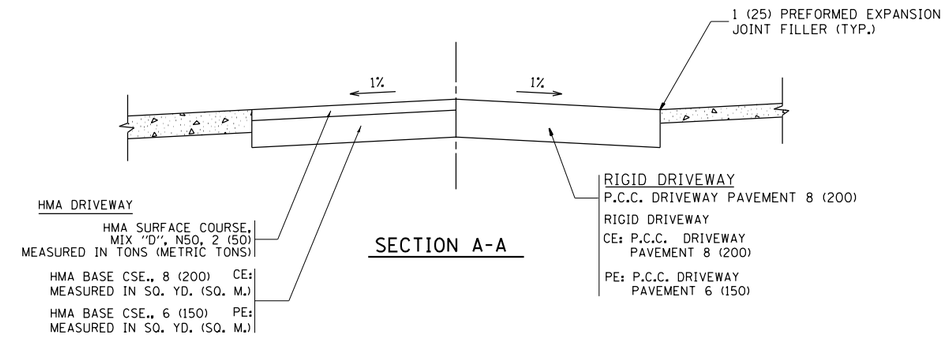
PLAN

6' (1.8 m) TO < 10' (3.0 m)

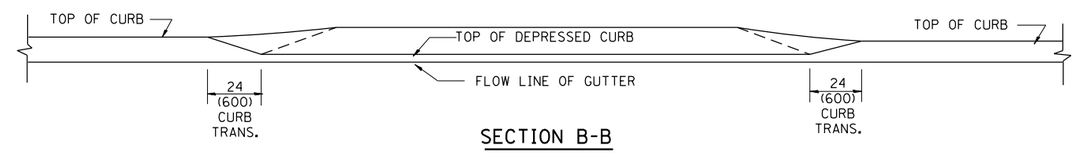


PLAN

6' (1.8 m) TO 10' (3.0 m)



SECTION A-A



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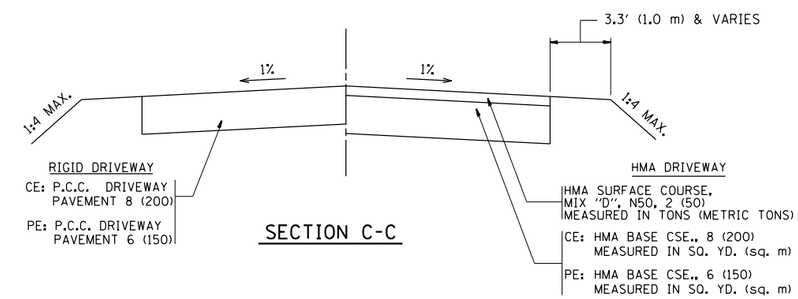
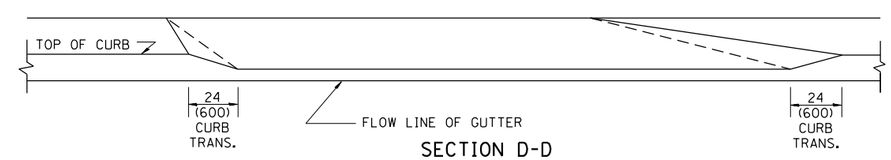


SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

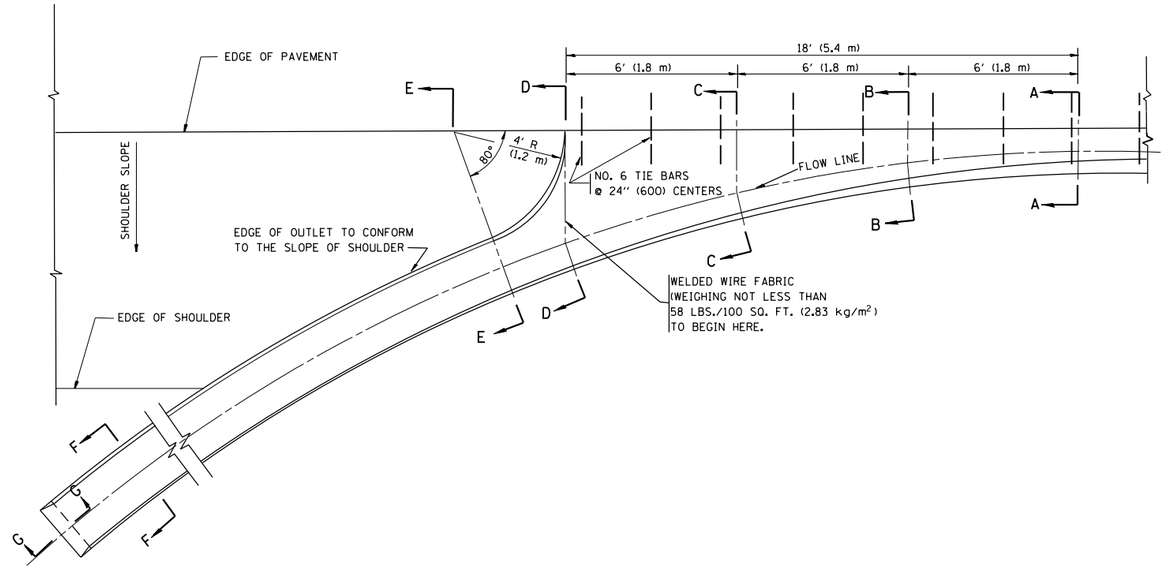
THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

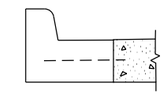
"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454\DRAWING	USER NAME = ledezmarm	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY DETAILS DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)
	Data\Design\DistStd.dgn		REVISED - P. LaFLEUR 04-15-03		
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07		SCALE: NONE · SHEET NO. 1 OF 1 SHEETS · STA. · TO STA.
	PLOT DATE = 8/19/2016	DATE - 11-06-95	REVISED - R. BORO 01-01-07		

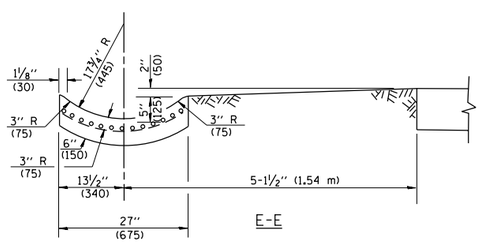
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	82
BD400-02 (BD-02)		CONTRACT NO. 60W06		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



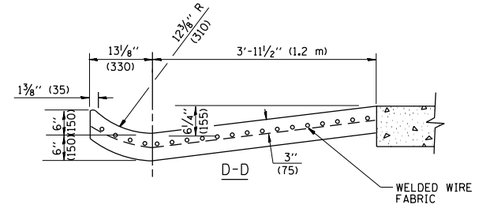


A-A ✳

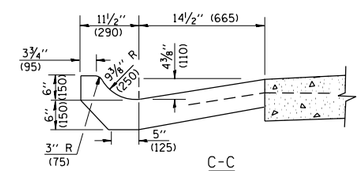
✳ DIMENSIONS OF THE CURB & GUTTER AT SECTION A-A ARE SHOWN ON STATE STANDARD 606001.
FOR DETAILS OF OUTLET FOR CONCRETE CURB & GUTTER, TYPE B-6.24 (B-15.60) SEE STATE STANDARD 606006.



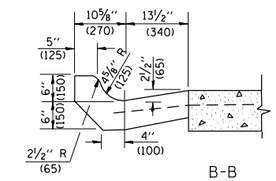
E-E



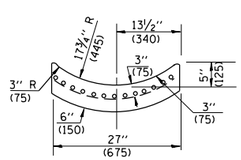
D-D



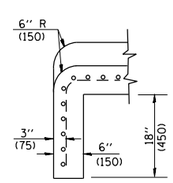
C-C



B-B



F-F



G-G

GENERAL NOTES

GUTTER OUTLET SHALL BE TIED TO THE PAVEMENT IN ACCORDANCE WITH DETAILS FOR LONGITUDINAL CONSTRUCTION JOINT SHOWN ON STANDARD 420001.

TIE BARS SHALL BE NO. 20 (NO.6) AT 24" (600) CENTERS UNLESS OTHERWISE SHOWN.

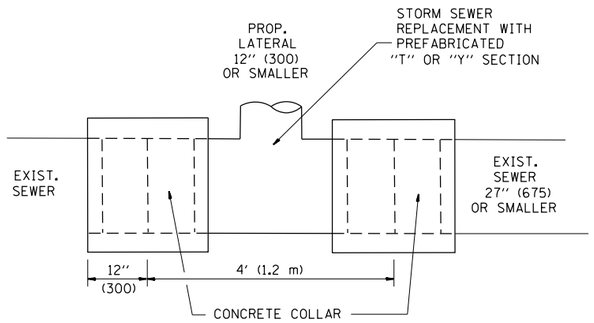
IF THE AVERAGE GRADE OF PAVEMENT FOR THE DISTANCE FROM SECTION A-A TO D-D EXCEEDS 2%, THIS DISTANCE SHALL BE INCREASED 6' (1.8 m) FOR EACH 1% INCREASE IN GRADE.

QUANTITIES

FOR SECTION A-A TO E-E AND CURTAIN WALL=
1.25 CU. YDS. (0.96 m^3) CLASS SI CONCRETE (OUTLET) FOR 9" (225) PAV'T.
1.27 CU. YDS. (0.96 m^3) CLASS SI CONCRETE (OUTLET) FOR 10" (250) PAV'T.
FOR SECTION F-F=
0.045 CU. YDS. (0.03 m^3) CLASS SI CONCRETE PER ft. (m).

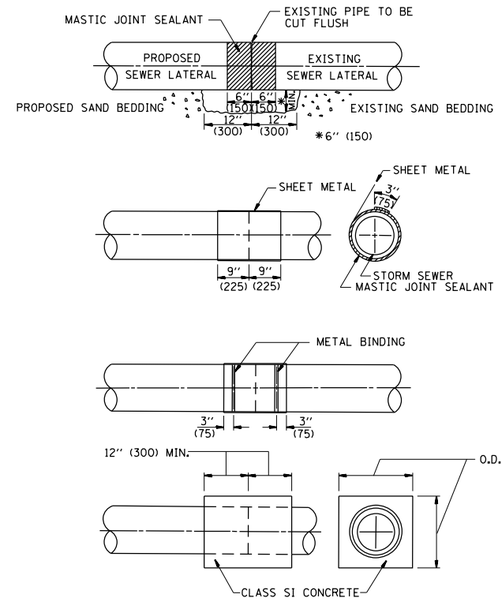
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454\DrawingData\Design\DistStd.dgn	USER NAME = ledezmerm	DESIGNED - M. DE YONG	REVISED - R. SHAH 09-09-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	OUTLET FOR CONCRETE CURB AND GUTER			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. SHAH 10-25-94					104	105N-4	LAKE		83
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - E. GOMEZ 12-21-00					BD600-01 (BD-03)		CONTRACT NO. 60W06		
	PLOT DATE = 8/19/2016	DATE - 08-04-86	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER
OF 27" (675) OR SMALLER



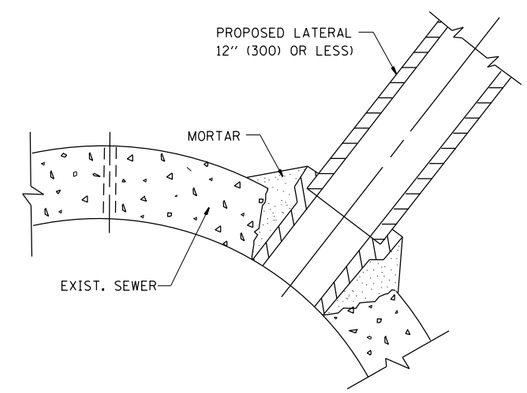
DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERANCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



DETAIL "C"

PROPOSED LATERAL
CONNECTION TO EXISTING SEWER
OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.

II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:

A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".

B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

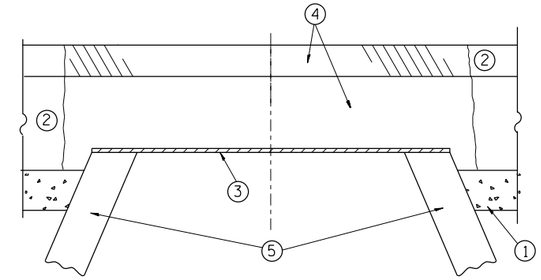
BASIS OF PAYMENT

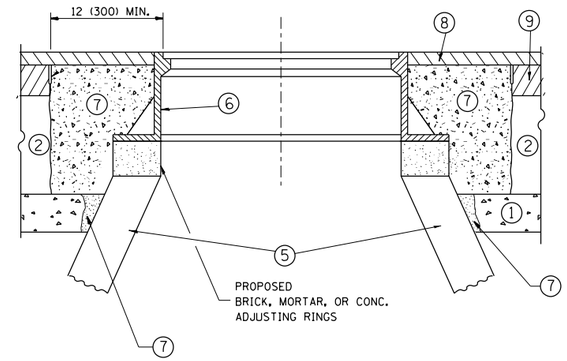
TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.





NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS PP-1* CONCRETE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

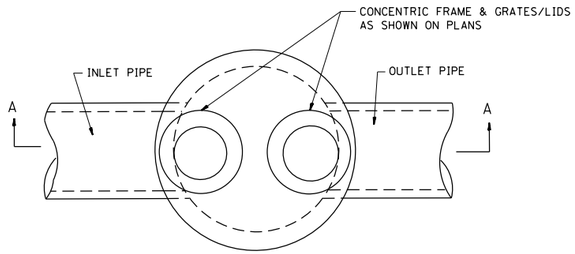
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454\DrawingData\Design\DistStd.dgn	USER NAME = ledezmarm	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
		CHECKED -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000 ' / in.		REVISED - R. BORO 03-09-11
	PLOT DATE = 8/19/2016	DATE - 10-25-94	REVISED - R. BORO 12-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

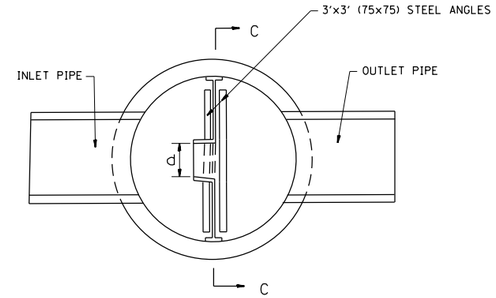
DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA.

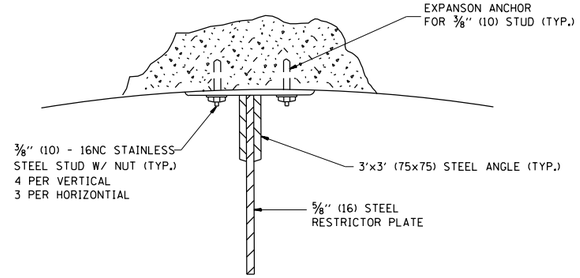
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	85
BD600-03 (BD-8)		CONTRACT NO. 60W06		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



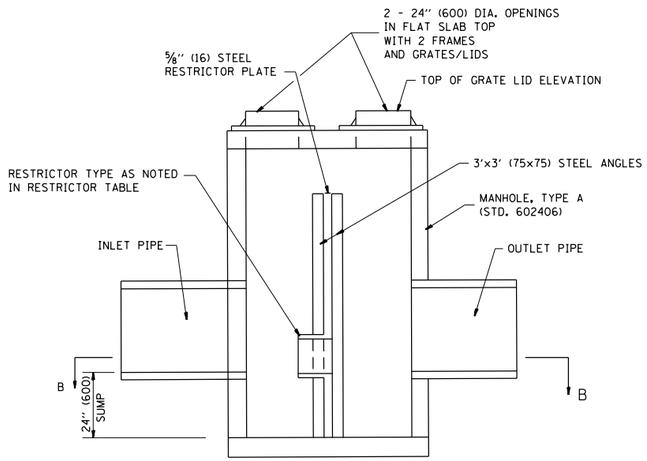
PLAN



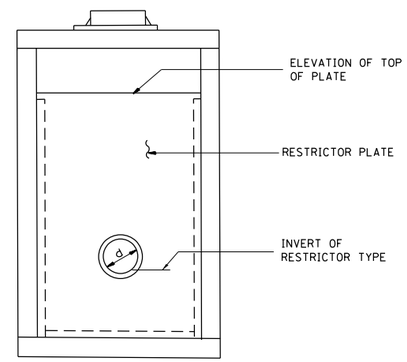
SECTION B-B



ANGLE FASTENER DETAIL



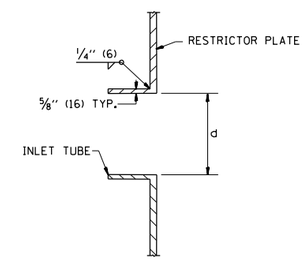
SECTION A-A



SECTION C-C

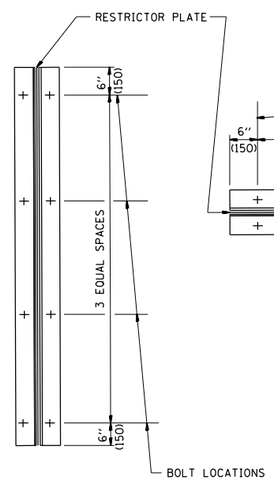
NOTES:

1. ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION.
2. ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.
3. BASIS OF PAYMENT: "MANHOLES ,TYPE A, 6 FT. (1.8 m)-DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE" EACH

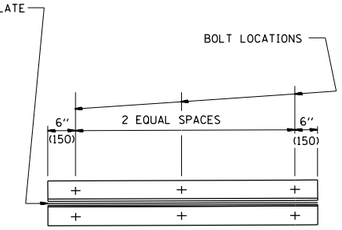


INLET TUBE DETAIL

STATION	MANHOLE DIAMETER	FRAME AND GRATE	RESTRICTOR TYPE	INSIDE RESTRICTOR TYPE DIAMETER in. (mm) (d)	INVERT OF RESTRICTOR TYPE	ELEVATION OF TOP OF PLATE OVERFLOW



TYPICAL VERTICAL ANGLES
LOOKING TOWARD MANHOLE WALL



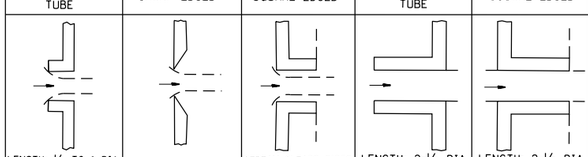
TYPICAL HORIZONTIAL
ANGLES LOOKING TOWARD
BOTTOM OF MANHOLE

TOTAL BOLTS REQUIRED: 22

NOTE:

1. ANGLES SHOULD BE 3x3x3/8 (75x75x75)
2. VERTICAL ANGLES SHOULD EXTEND FROM THE BOTTOM OF THE RESTRICTOR PLATE TO THE TOP.
3. HORIZONTIAL ANGLES SHOULD EXTEND FROM VERTICAL ANGLE TO VERTICAL ANGLE.

STEEL ANGLE BOLTING DETAILS

RESTRICTOR TYPE					
1	2	3	4	5	6
RE-ENTRANT TUBE	SHARP EDGED	SQUARE EDGED	RE-ENTRANT TUBE	SQUARE EDGED	ROUNDED
					
LENGTH: 1/2 TO 1 DIA.		STREAM CLEARS SIDES	LENGTH: 2-1/2 DIA.	LENGTH: 2-1/2 DIA.	
C=.52	C=.61	C=.61	C=.73	C=.82	C=.98

VALUES OF "C" FOR CIRCULAR
AND SQUARE ORIFICES

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

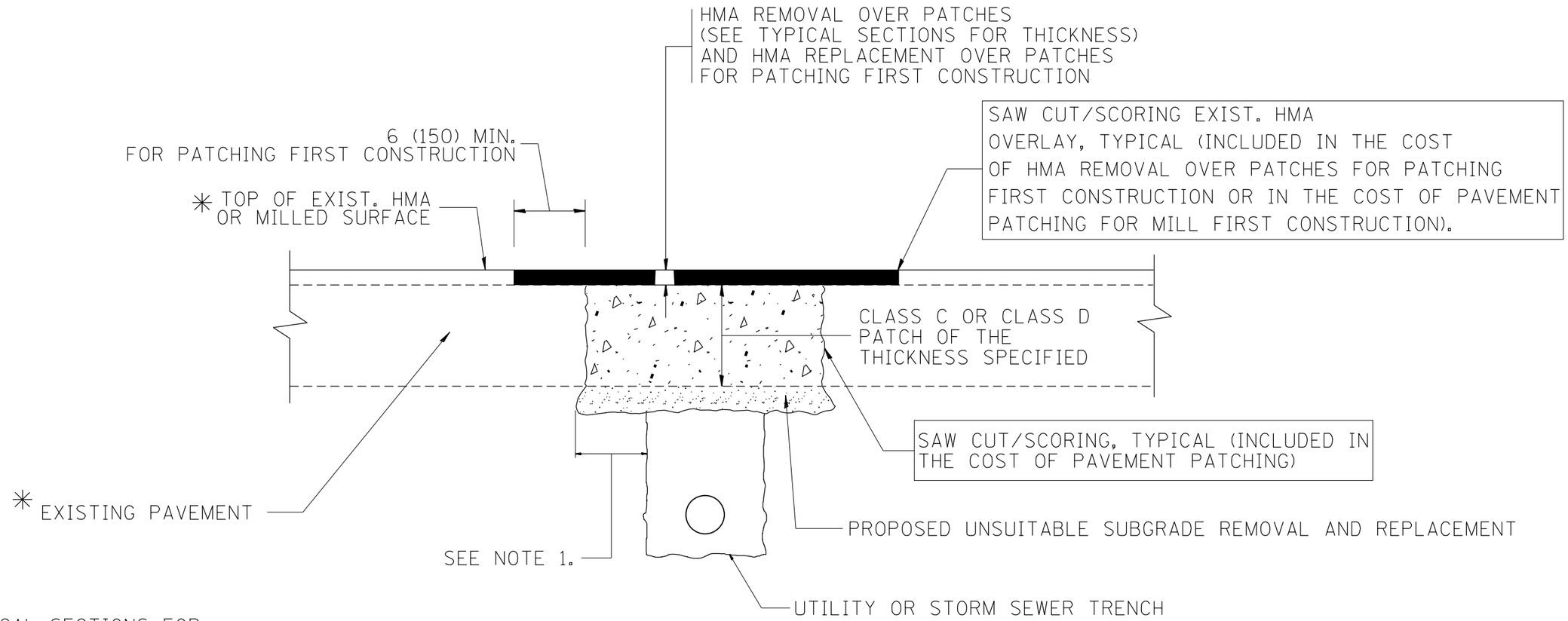
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454	USER NAME = ledezmarm	DESIGNED - R. SHAH	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - E. GOMEZ 08-28-00
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - M. GOMEZ 01-08-01
	PLOT DATE = 8/19/2016	DATE - 09-09-94	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MANHOLE WITH
RESTRICTOR PLATE

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	86
BD600-04	(BD-12)	CONTRACT NO. 60W06		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

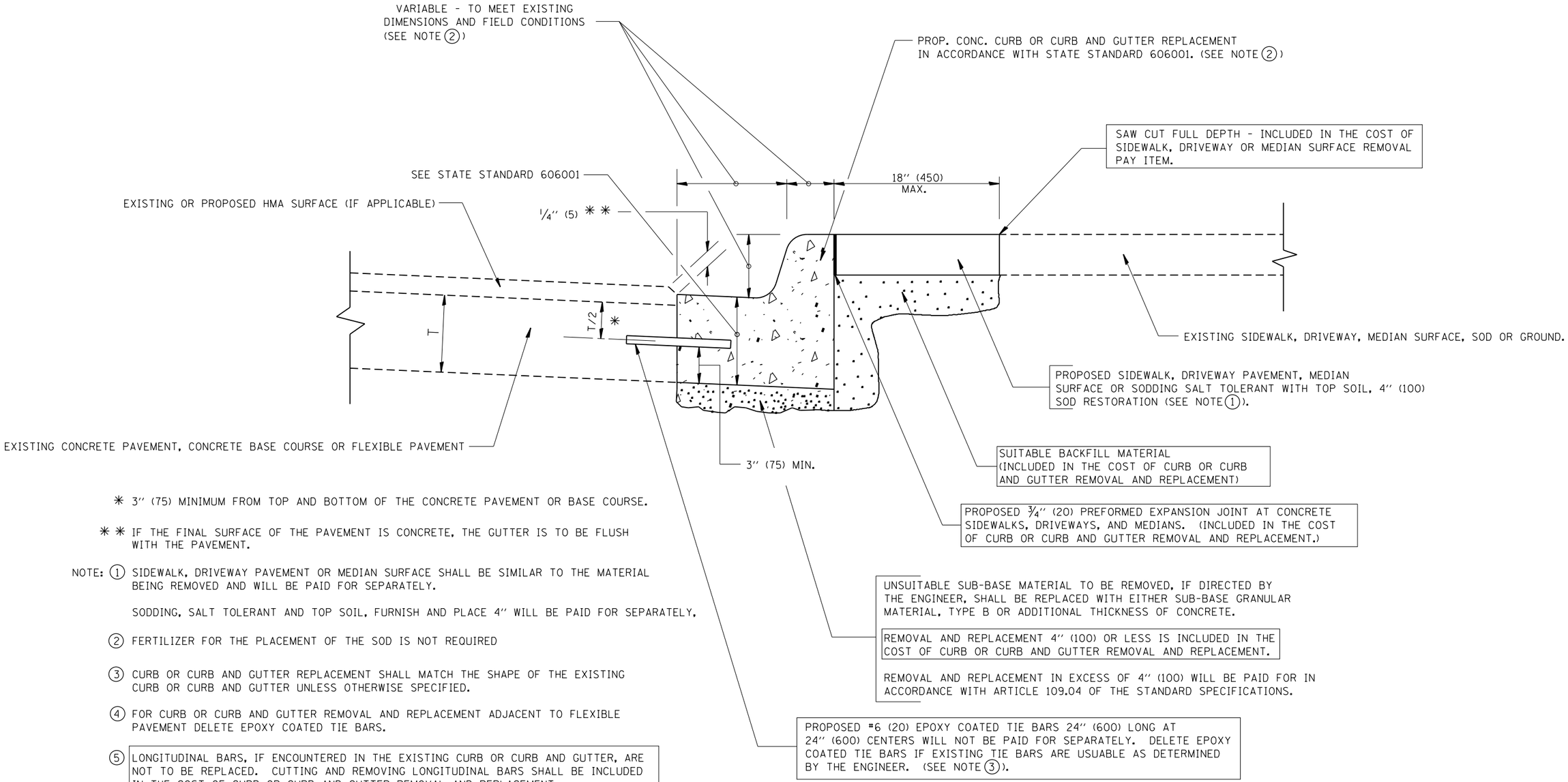
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST $4\frac{1}{2}$ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454[illegible]\Data\Design\DistStd.dgn	USER NAME = ledezmarm	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000 ' / in.	DRAWN	REVISED - R. BORO 01-01-07			104	105N-4	LAKE	113	87
		CHECKED -	REVISED - R. BORO 09-04-07			BD400-04 (BD-22)		CONTRACT NO. 60W06		
	PLOT DATE = 8/19/2016	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

(2) FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

(3) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(4) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(5) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(7) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(8) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

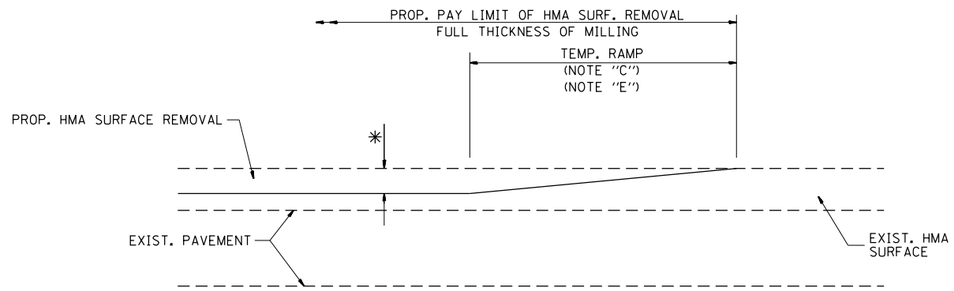
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

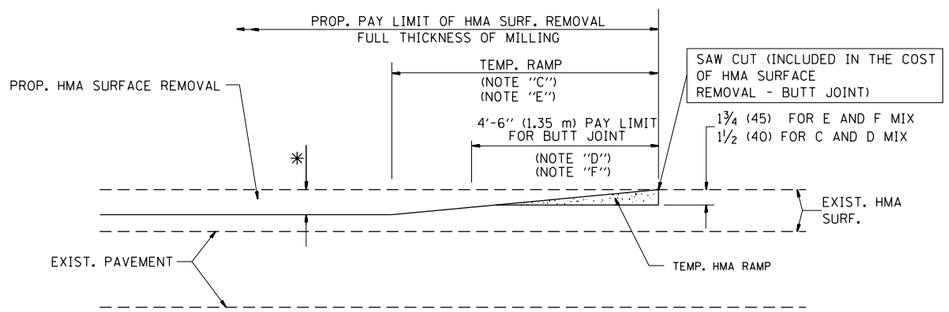
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454	USER NAME = ledezmarm	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - A. ABBAS 03-21-97			104	105N-4	LAKE	113	88
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24)		CONTRACT NO. 60W06		
	PLOT DATE = 8/19/2016	DATE - 03-11-94	REVISED - R. BORO 12-15-09		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

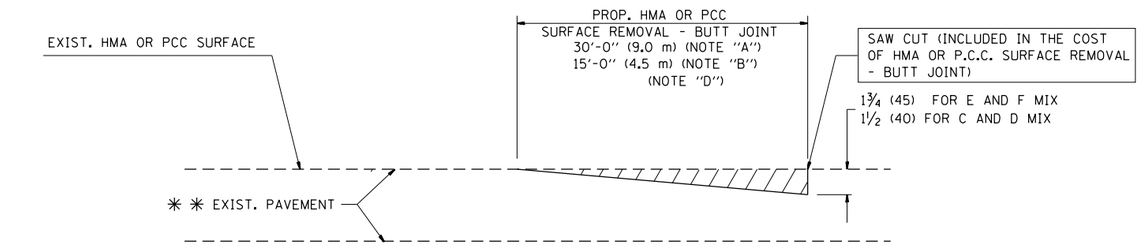


HMA CONSTRUCTED TEMPORARY RAMP

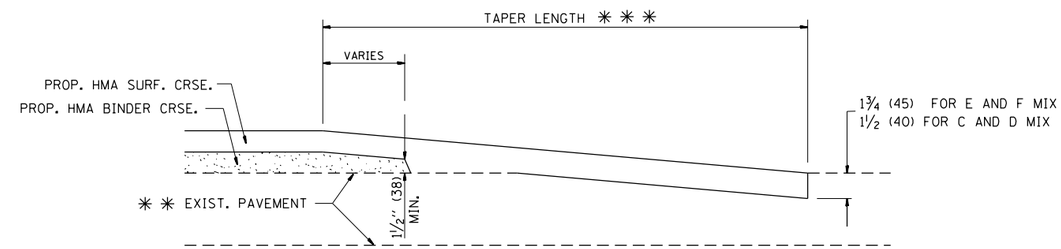
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



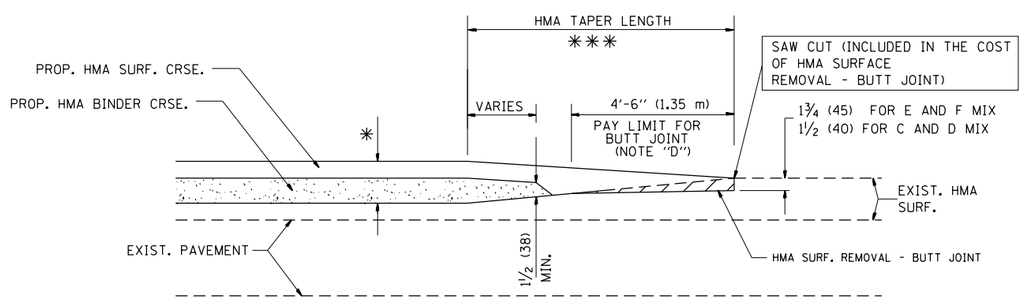
BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

✳ ✳ PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

✳ SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

✳ ✳ ✳ 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

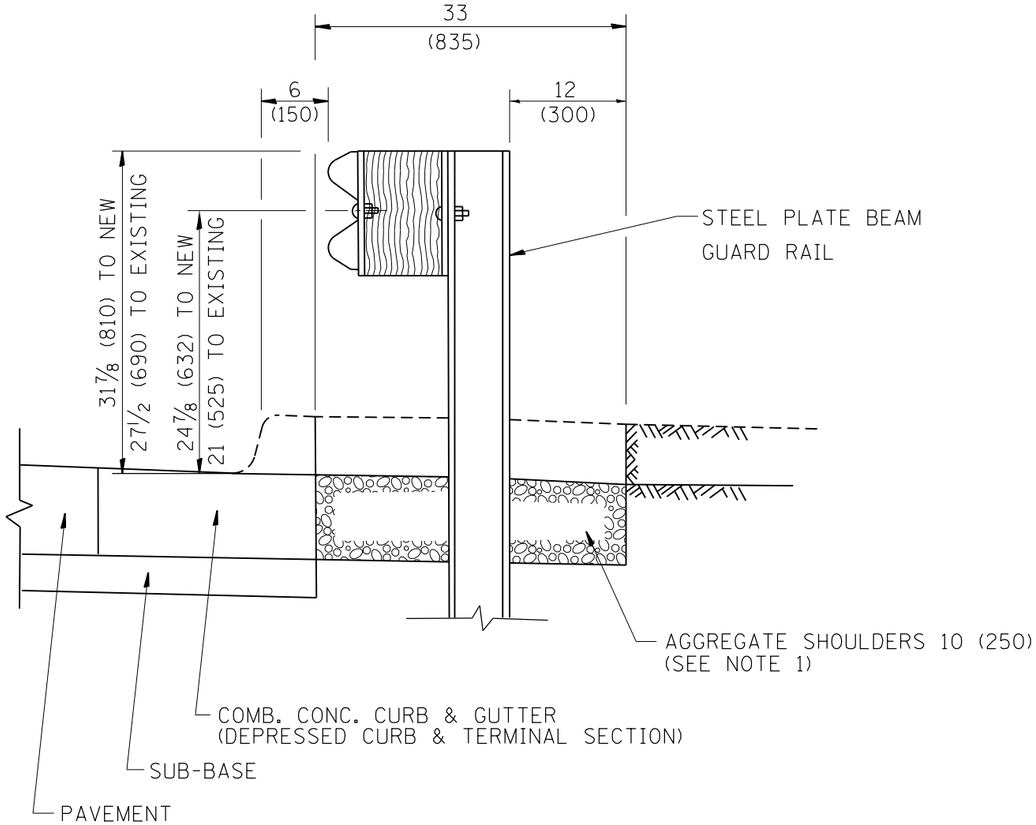
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454	USER NAME = ledezmarm	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
		DRAWN -	REVISED - A. ABBAS 03-21-97	
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01	
	PLOT DATE = 8/19/2016	DATE - 06-13-90	REVISED - R. BORO 01-01-07	

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	89
BD400-05 BD32		CONTRACT NO. 60W06		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT



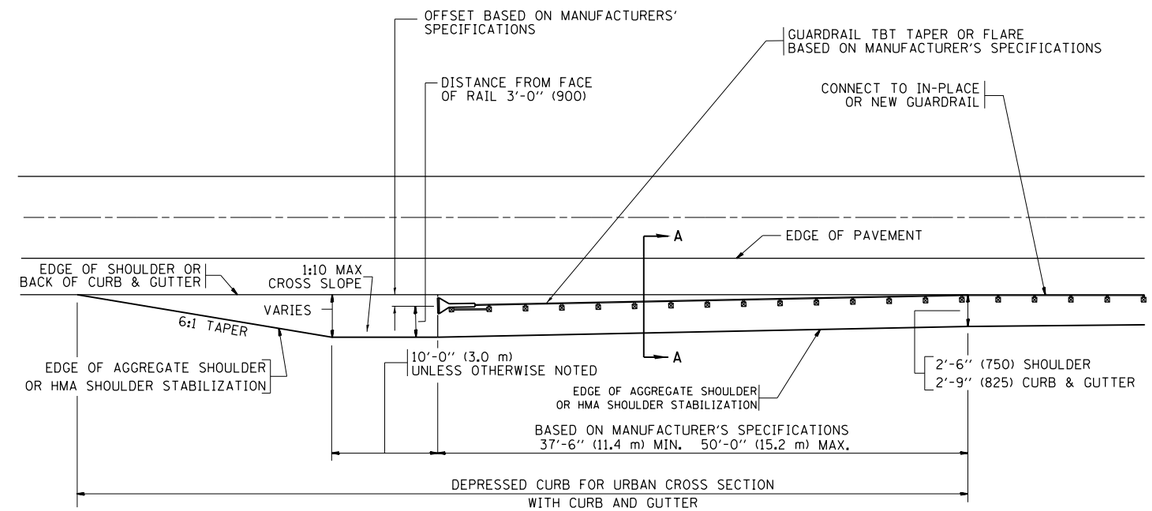
SECTION A-A

NOTES:

1. THE AGGREGATE SHOULDER, 10" OR HMA SHOULDER, 6" (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER

[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]



DEPRESSED CURB AND GUTTER AND SHOULDER TREATMENT AT TBT TY. 1 SPL.

BASIS OF PAYMENT: HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDERS 6" (150 mm)".

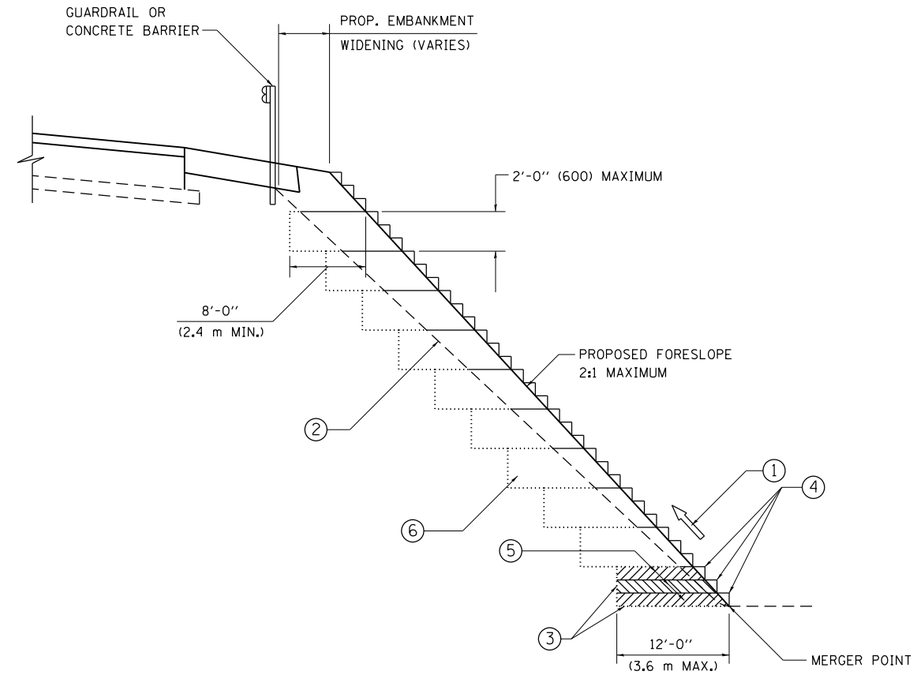
STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454	USER NAME = ledezmarm	DESIGNED - M. DE YONG	REVISED - E. GOMEZ 08-28-00	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR DEPRESSED CURB & GUTTER AND SHOULDER TREATMENT AT TBT TY 1 SPL.
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07		
	PLOT DATE = 8/19/2016	DATE - 09-22-90	REVISED - R. BORO 12-08-2008		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.
			REVISED - R. BORO 09-14-2009		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	90
BD600-10 (BD 34)		CONTRACT NO. 60W06		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TYPICAL BENCHING DETAIL FOR EMBANKMENT

NOTES:

1. CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
2. EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
3. BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
4. TRIM TO FINAL SLOPE.
5. EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
6. EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
7. SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN - CADD	REVISED -
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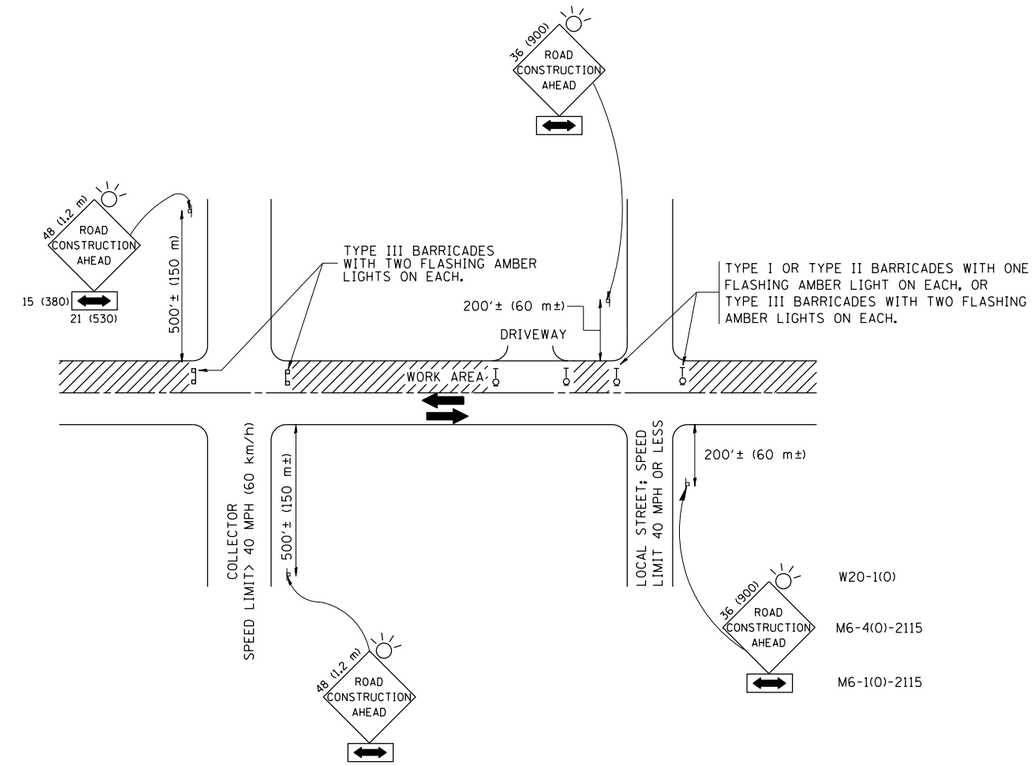
Default	PLOT DATE = 8/19/2016	DATE - 06-16-04	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BENCHING DETAIL
FOR EMBANKMENT WIDENING

SCALE: | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	91
	BD-51	CONTRACT NO. 60W06		
	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454\Workdata\Design\DistStd.dgn	USER NAME = ledezmarm	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 8/19/2016	DATE - 06-89	REVISED -T. RAMMACHER 01-06-00

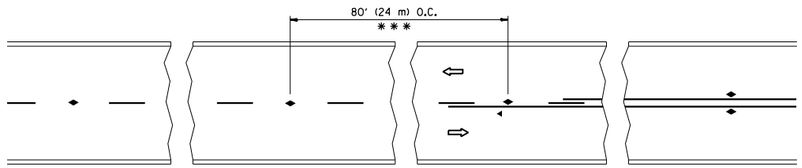
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA.

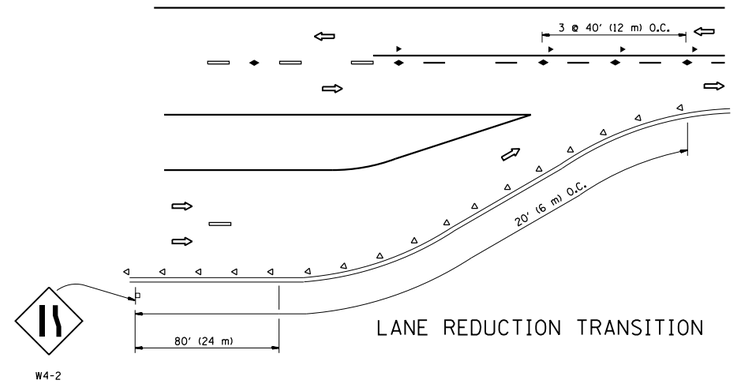
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	92
	TC-10	CONTRACT NO. 60W06		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

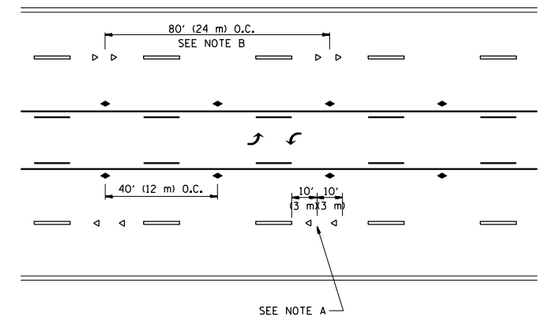


* * * REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

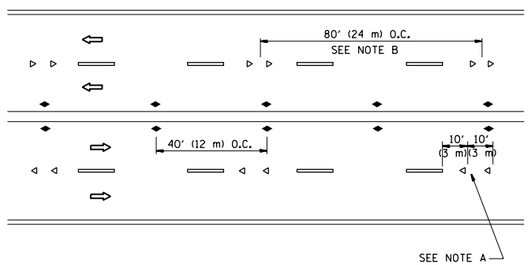
TWO-LANE/TWO-WAY



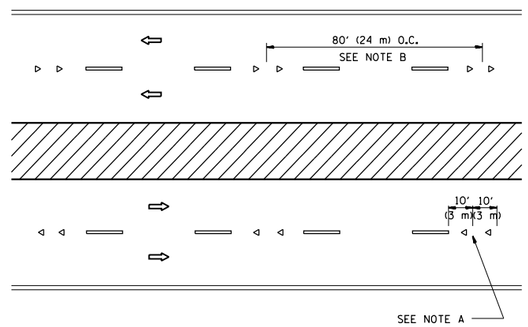
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

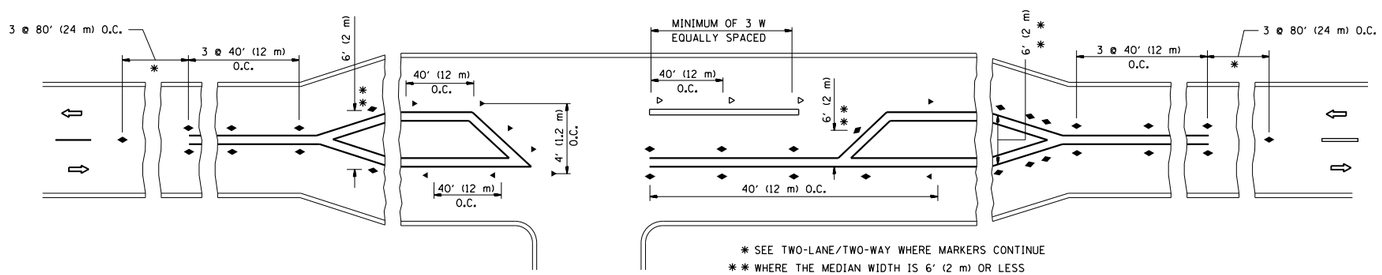
LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

* * WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454\DrawingData\Design\DistStd.dgn	USER NAME = ledezmarm	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
		DRAWN -	REVISED - T. RAMMACHER 03-12-99	
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00	
	PLOT DATE = 8/19/2016	DATE -	REVISED - C. JUCIUS 09-09-09	

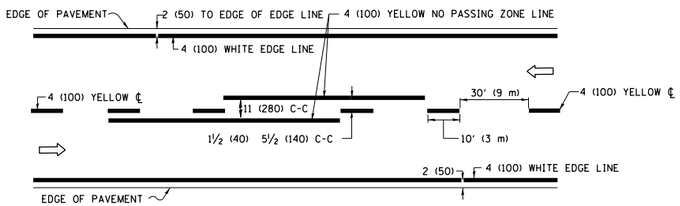
TYPICAL APPLICATIONS

RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

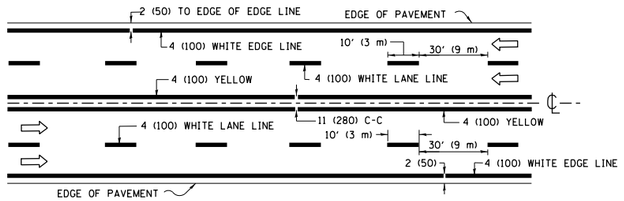
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	93
	TC-11	CONTRACT NO. 60W06		

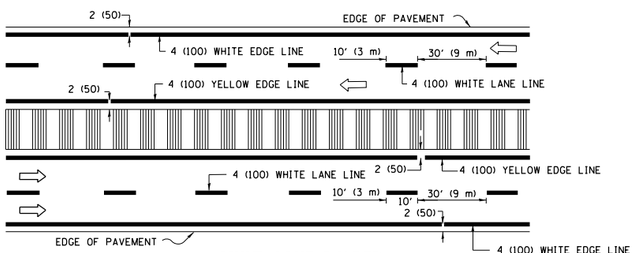
FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT



2–LANE ROADWAY



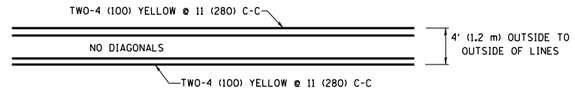
MULTI–LANE UNDIVIDED



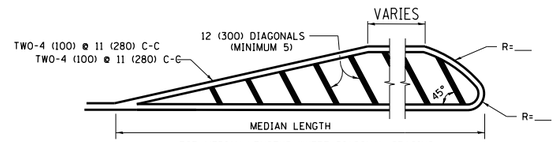
MULTI–LANE DIVIDED

WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING



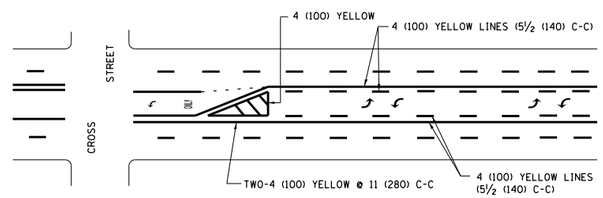
4' (1.2 m) WIDE MEDIANS ONLY



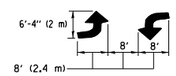
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

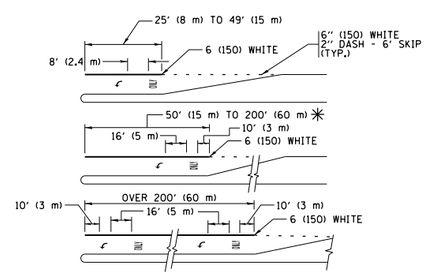


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

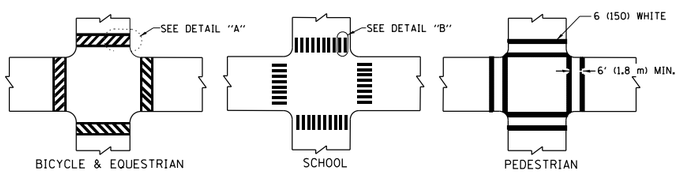


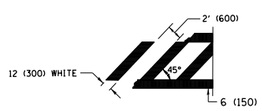
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

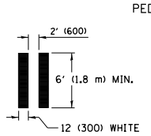
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING





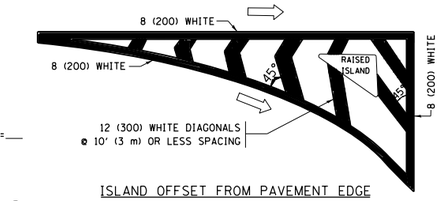
DETAIL "A"



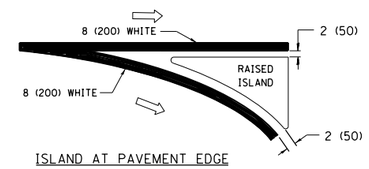
DETAIL "B"

TYPICAL CROSSWALK MARKING

✳ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

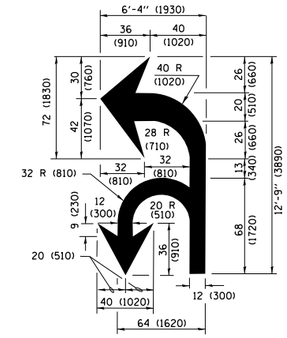


ISLAND OFFSET FROM PAVEMENT EDGE

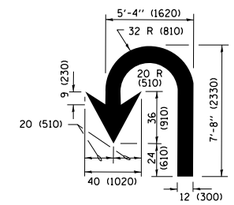


ISLAND AT PAVEMENT EDGE

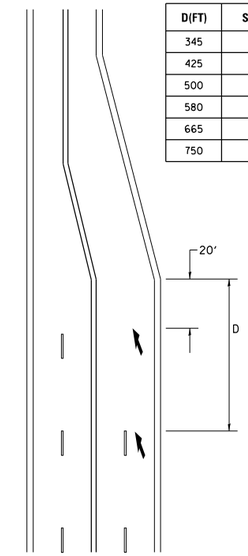
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U–TURN



U–TURN



D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

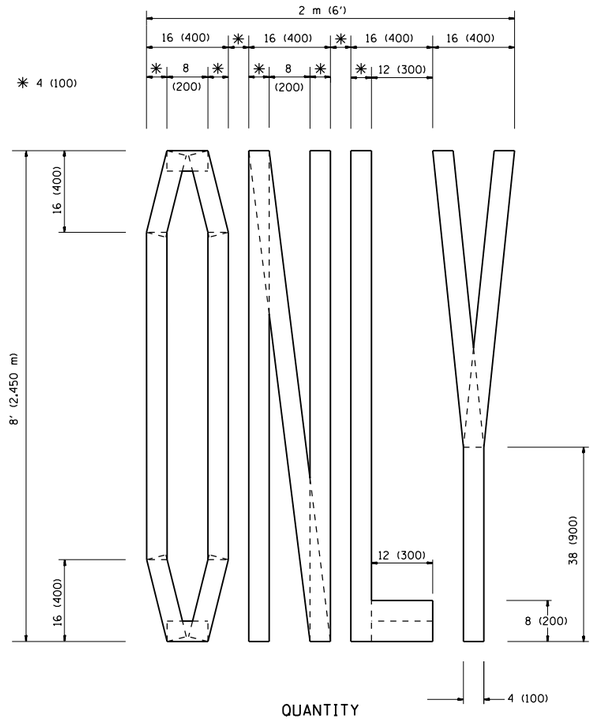
LANE REDUCTION TRANSITION

✳ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

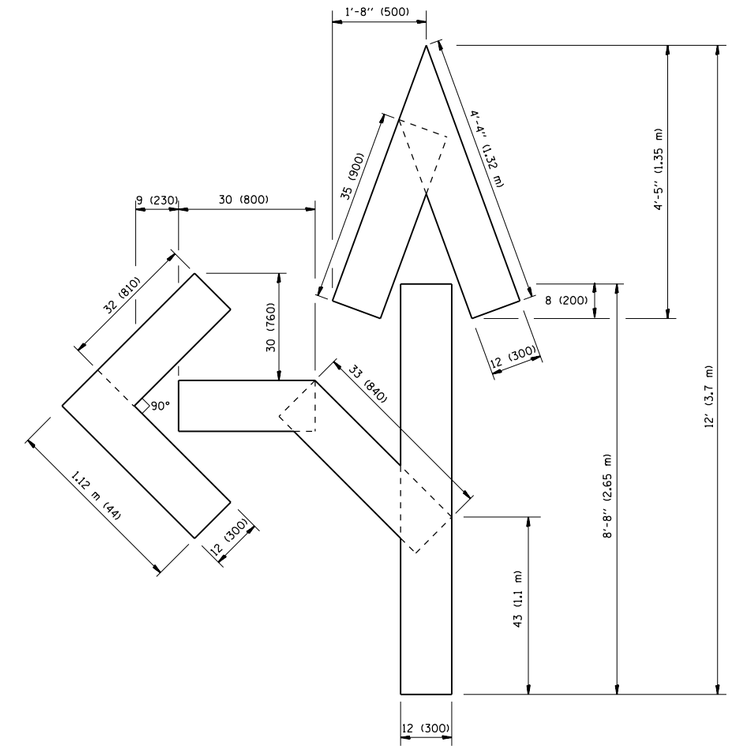
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

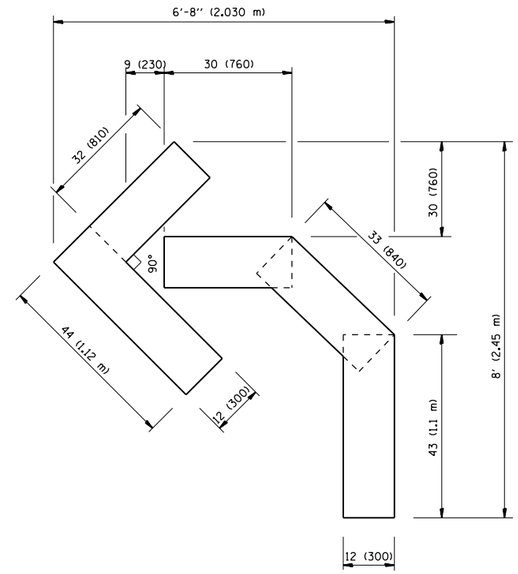
All dimensions are in inches (millimeters) unless otherwise shown.



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454\DRAWData\Design\DistStd.dgn	USER NAME = ledezmarm	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
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	PLOT DATE = 8/19/2016	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

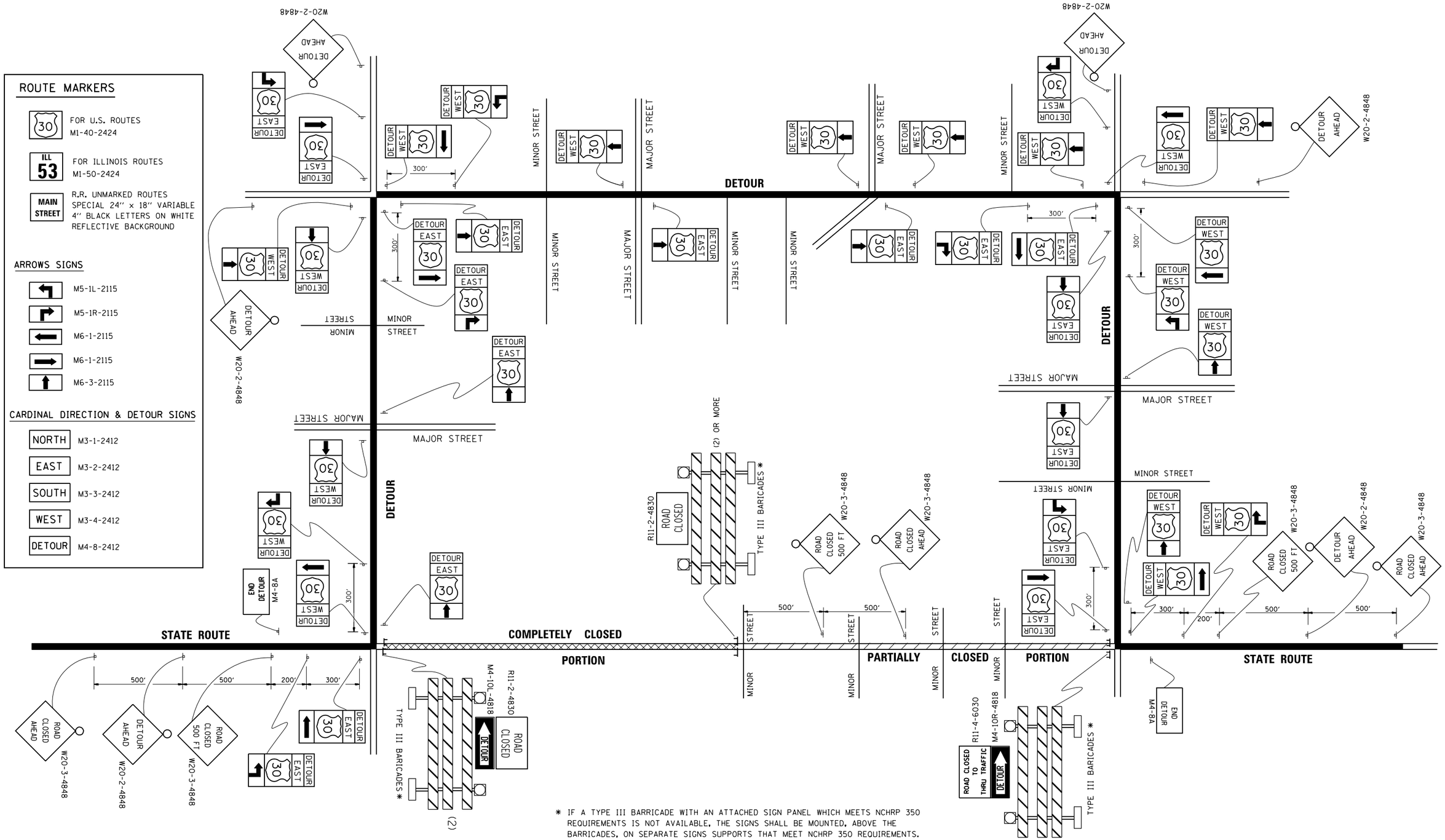
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

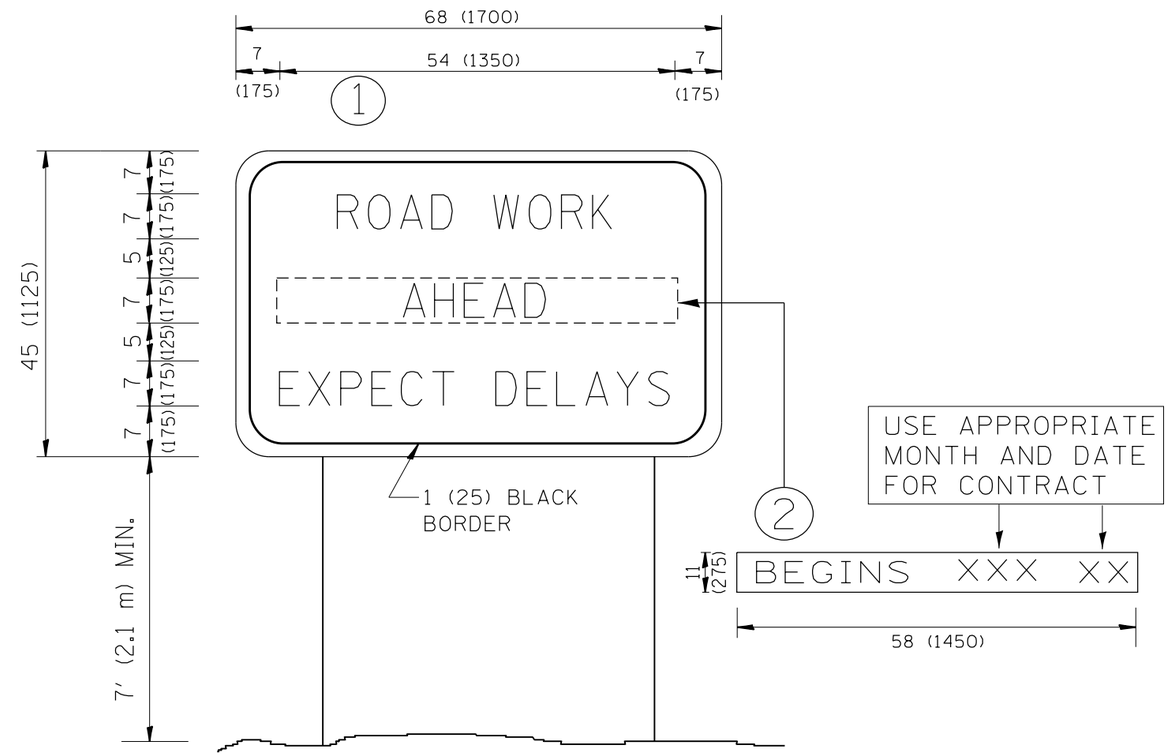
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	95
	TC-16	CONTRACT NO. 60W06		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT



* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454[illegible]\Data\Design\DistStd.dgn	USER NAME = ledezmarm	DESIGNED -	REVISED - 10-18-02	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETOUR SIGNING FOR CLOSING STATE HIGHWAYS	F.A.P. RTE. 104	SECTION 105N-4	COUNTY LAKE	TOTAL SHEETS 113 / SHEET NO. 96
		DRAWN -	REVISED - R. BORO 09-14-09				TC-21	CONTRACT NO. 60W06	
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -						
	PLOT DATE = 8/19/2016	DATE -	REVISED -		SCALE: NONE / SHEET NO. 1 OF 1 SHEETS / STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	

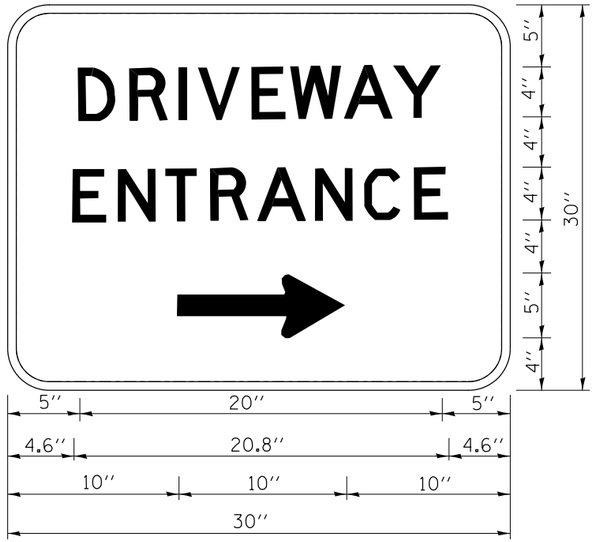


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN 1 WITH INSTALLED PANEL 2 ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL 2 SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

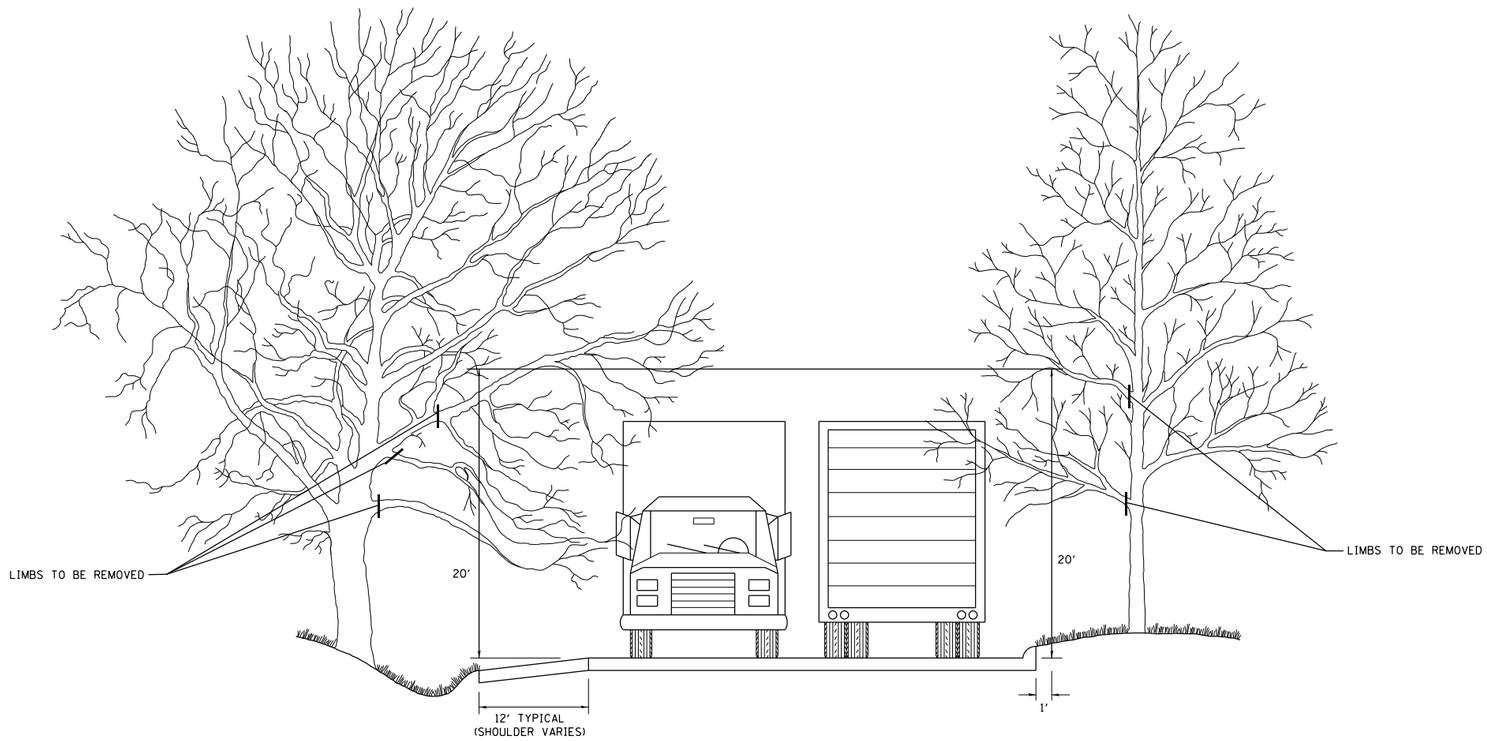
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\P1454[illegible]\Data\Design\DistStd.dgn	USER NAME = ledezmarm	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. MIRS 12-11-97			104	105N-4	LAKE	113	97
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99			TC-22		CONTRACT NO. 60W06		
	PLOT DATE = 8/19/2016	DATE -	REVISED - C. JUCIUS 01-31-07		SCALE: NONE / SHEET NO. 1 OF 1 SHEETS / STA. / TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



3.0'' RADIUS, 0.5'' BORDER, WHITE ON GREEN; REFLECTORIZED
''DRIVEWAY'' D; ''ENTRANCE'' D; STANDARD ARROW CUSTOM 12.0'' x 5.0''

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM ''TEMPORARY INFORMATION SIGNING''.



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		DRAWN -	REVISED -	
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	PLOT DATE = 8/19/2016	DATE -	REVISED -	

PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE

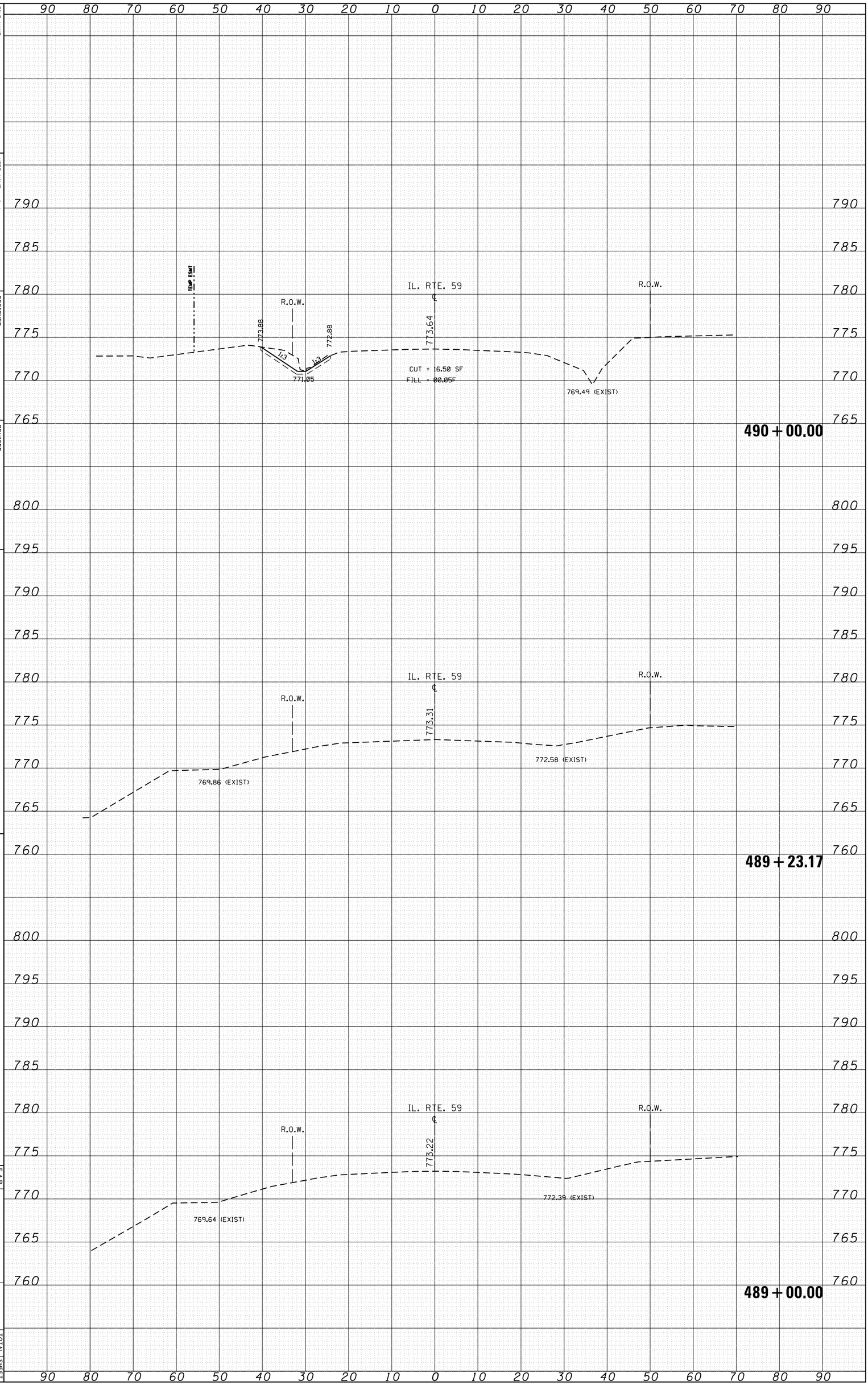
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	99
BM-20		CONTRACT NO. 60W06		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		



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USER NAME = ledezmarm

PLOT SCALE = 20.0000 ' / in.

PLOT DATE = 8/19/2016

DESIGNED -

CHECKED -

DATE -

REVISED -

REVISED -

REVISED -

REVISED -

Default

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL ROUTE 59 AT PETITE LAKE RD.

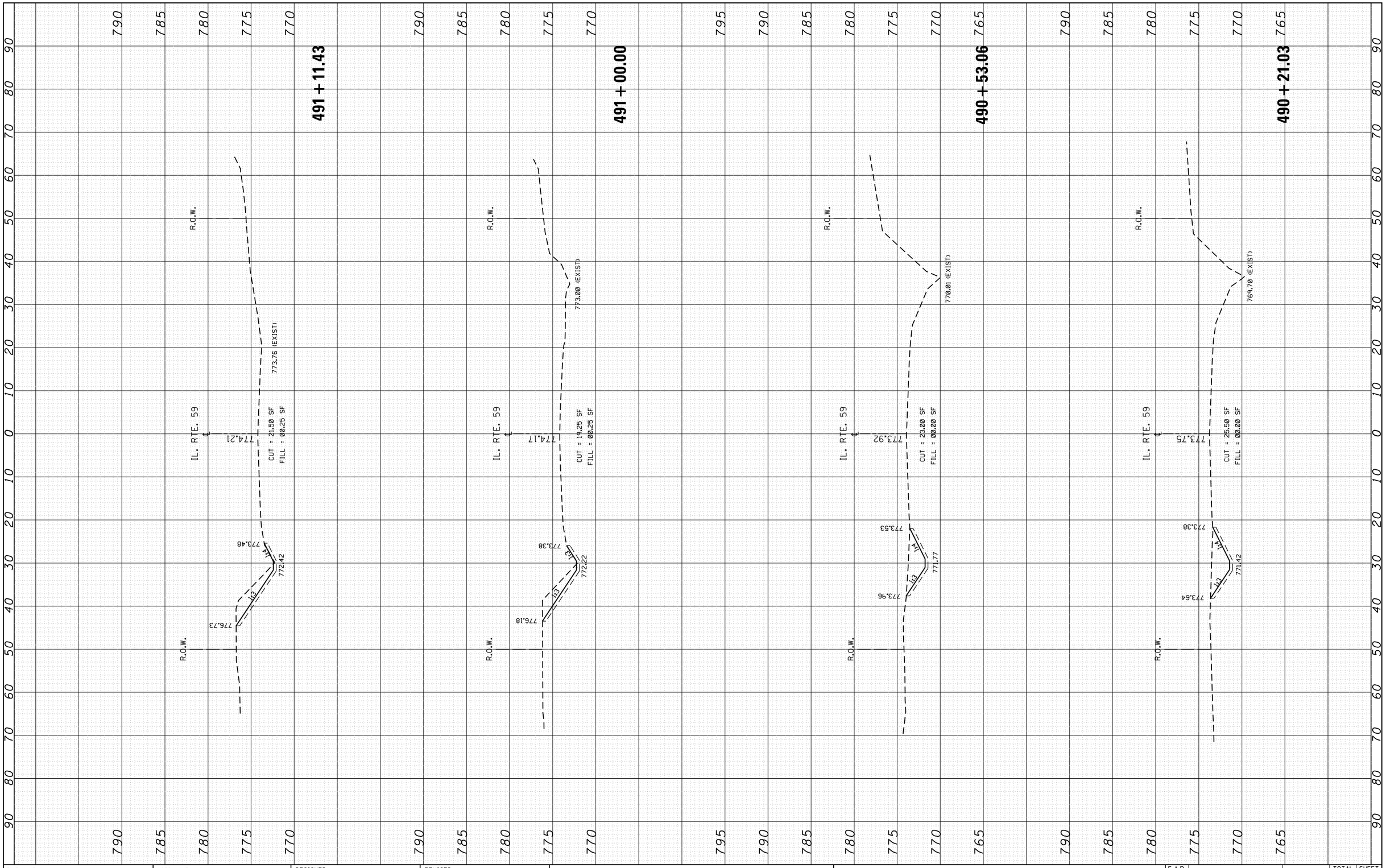
SCALE:

SHEET OF SHEETS STA. 489+00.00 TO STA. 490+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	100

ILLINOIS FED. AID PROJECT

CONTRACT NO. 60W06



FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

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		DRAWN -	REVISED -
	PLOT SCALE = 20.0000 ' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

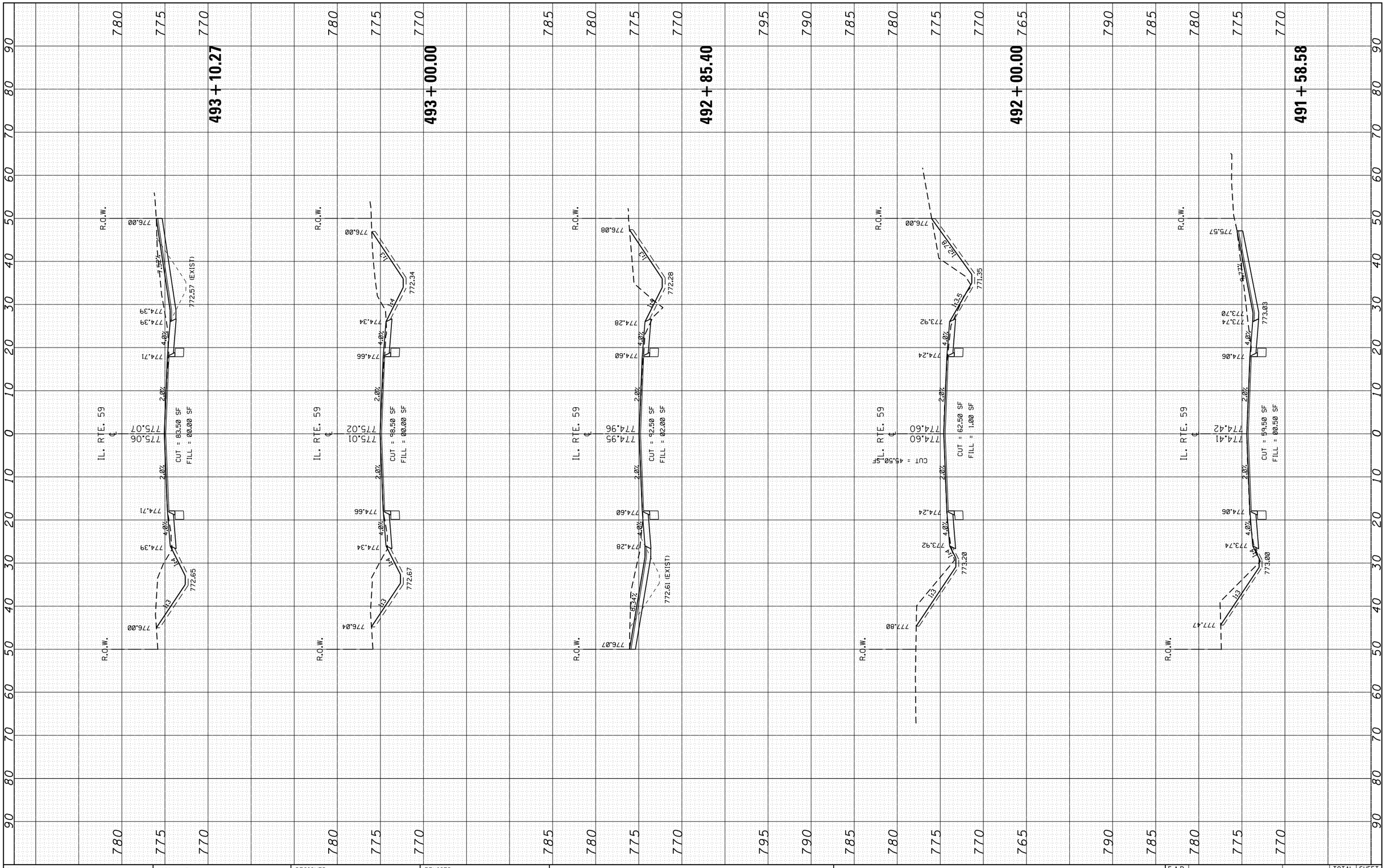
CROSS SECTIONS
IL. ROUTE 59 AT PETITE LAKE RD.

SCALE: | SHEET OF SHEETS | STA. 490+21.03 TO STA. 491+11.43

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	101

CONTRACT NO. 60W06

ILLINOIS | FED. AID PROJECT



FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
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	AREAS		
NO.	AREAS CHECKED		

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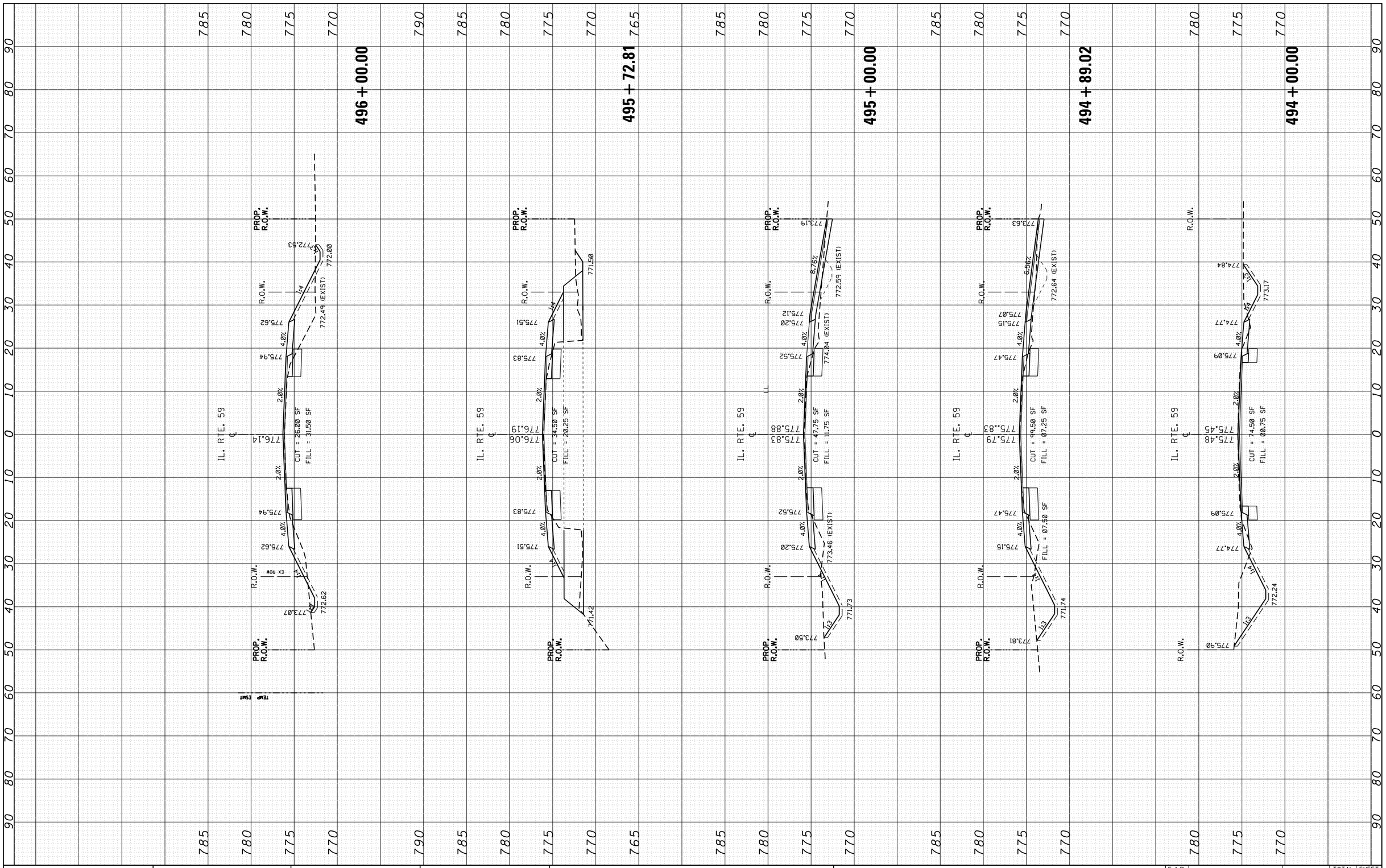
Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
IL. ROUTE 59 AT PETITE LAKE RD.

SCALE: | SHEET OF SHEETS | STA. 491+58.58 TO STA. 493+10.27

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4105N-4	LAKE	113	102
		CONTRACT NO. 60W06		
		ILLINOIS FED. AID PROJECT		



FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
AREAS CHECKED		
NOTE BOOK NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
AREAS CHECKED		
NOTE BOOK NO.		

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Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

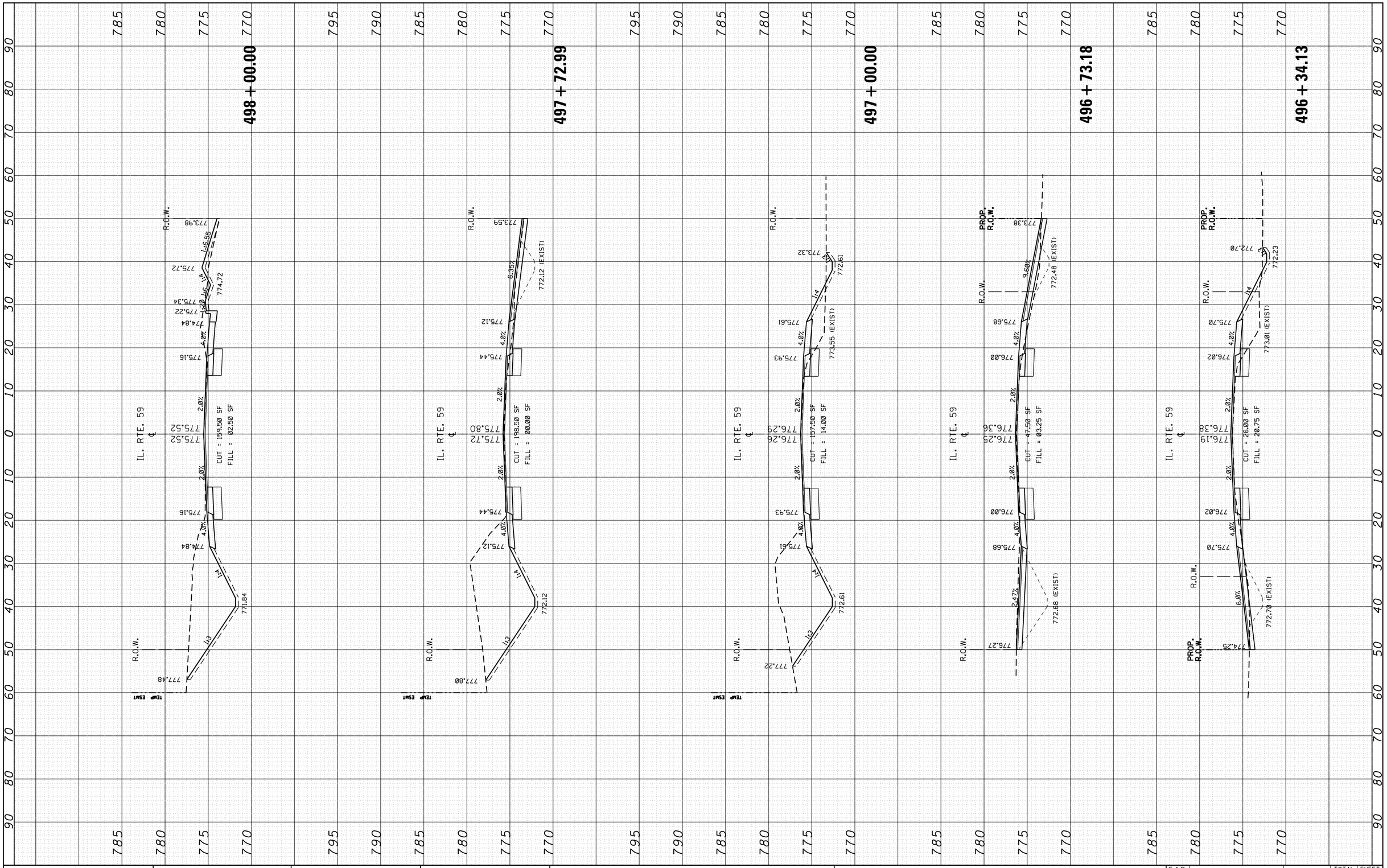
CROSS SECTIONS
IL. ROUTE 59 AT PETITE LAKE RD.

SCALE: | SHEET OF SHEETS | STA. 494+00.00 TO STA. 496+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	103

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
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	AREAS		
NO.	AREAS CHECKED		

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Default	PLOT DATE = 8/25/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

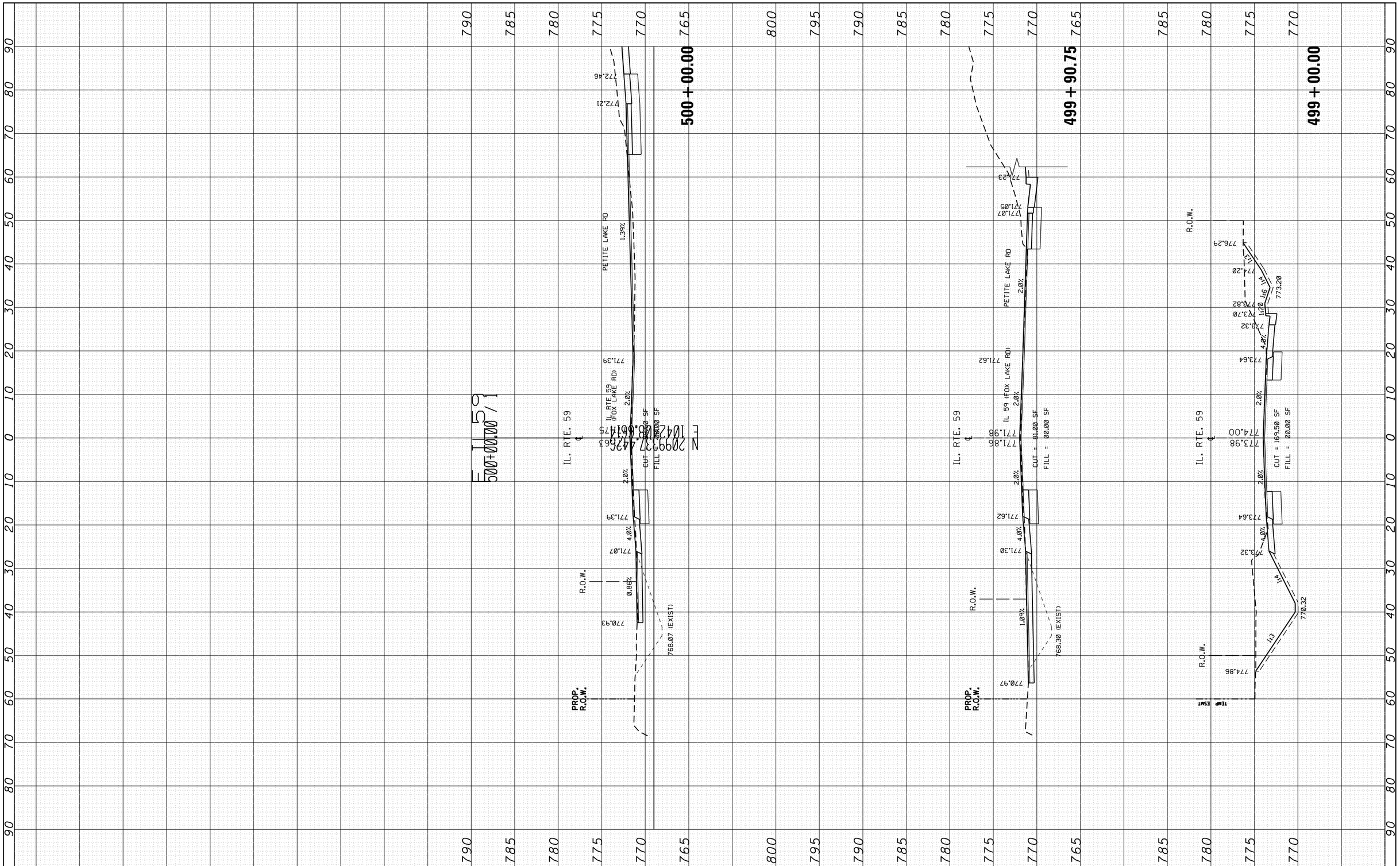
CROSS SECTIONS
IL. ROUTE 59 AT PETITE LAKE RD.

SCALE: | SHEET OF SHEETS | STA. 496+34.13 TO STA. 498+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	104

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
AREAS CHECKED		
NOTE BOOK		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
AREAS CHECKED		
NOTE BOOK		
NO.		

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Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

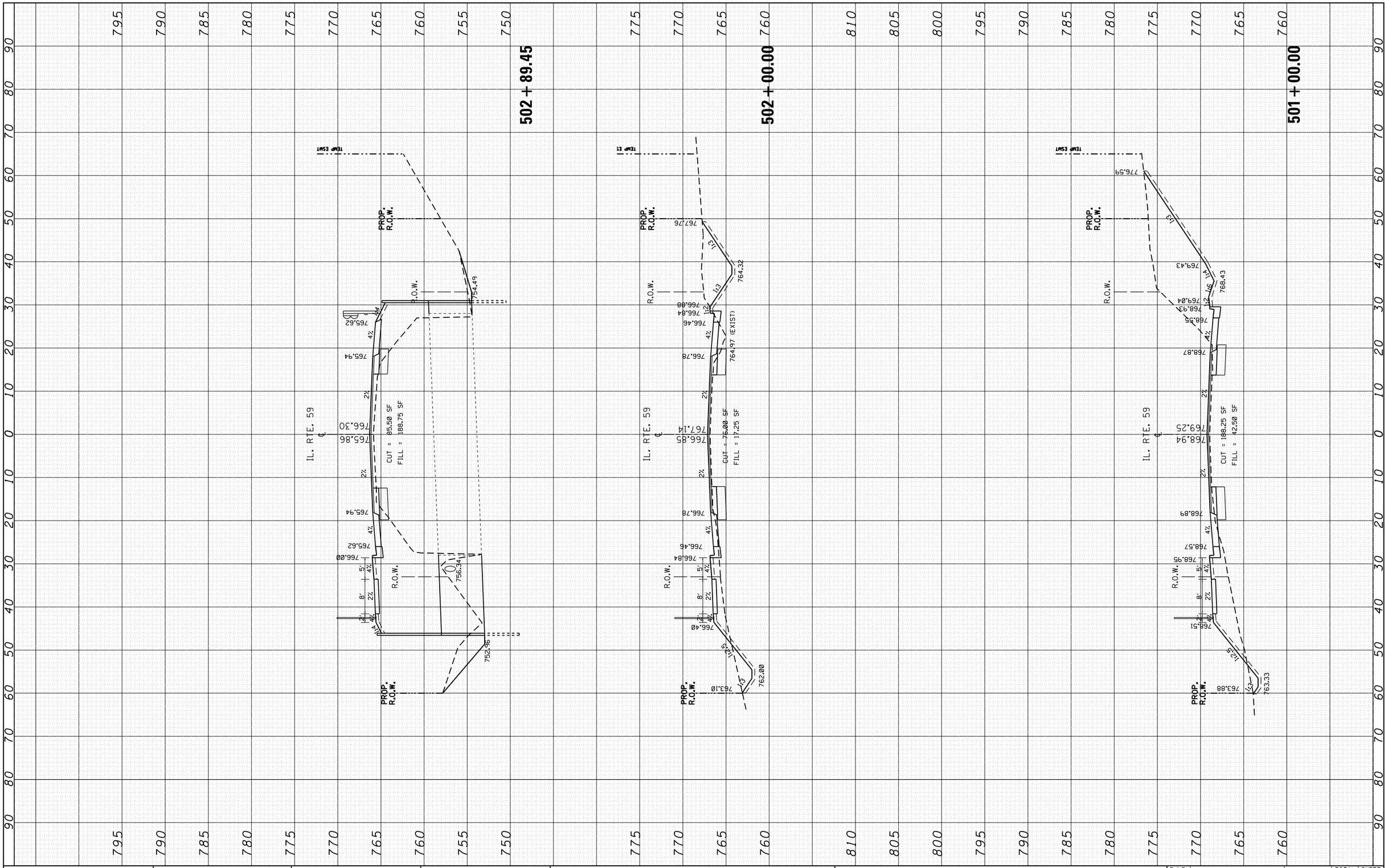
CROSS SECTIONS
IL. ROUTE 59 AT PETITE LAKE RD.

SCALE: | SHEET OF SHEETS | STA. 499+00.00 TO STA. 500+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	105

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

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Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

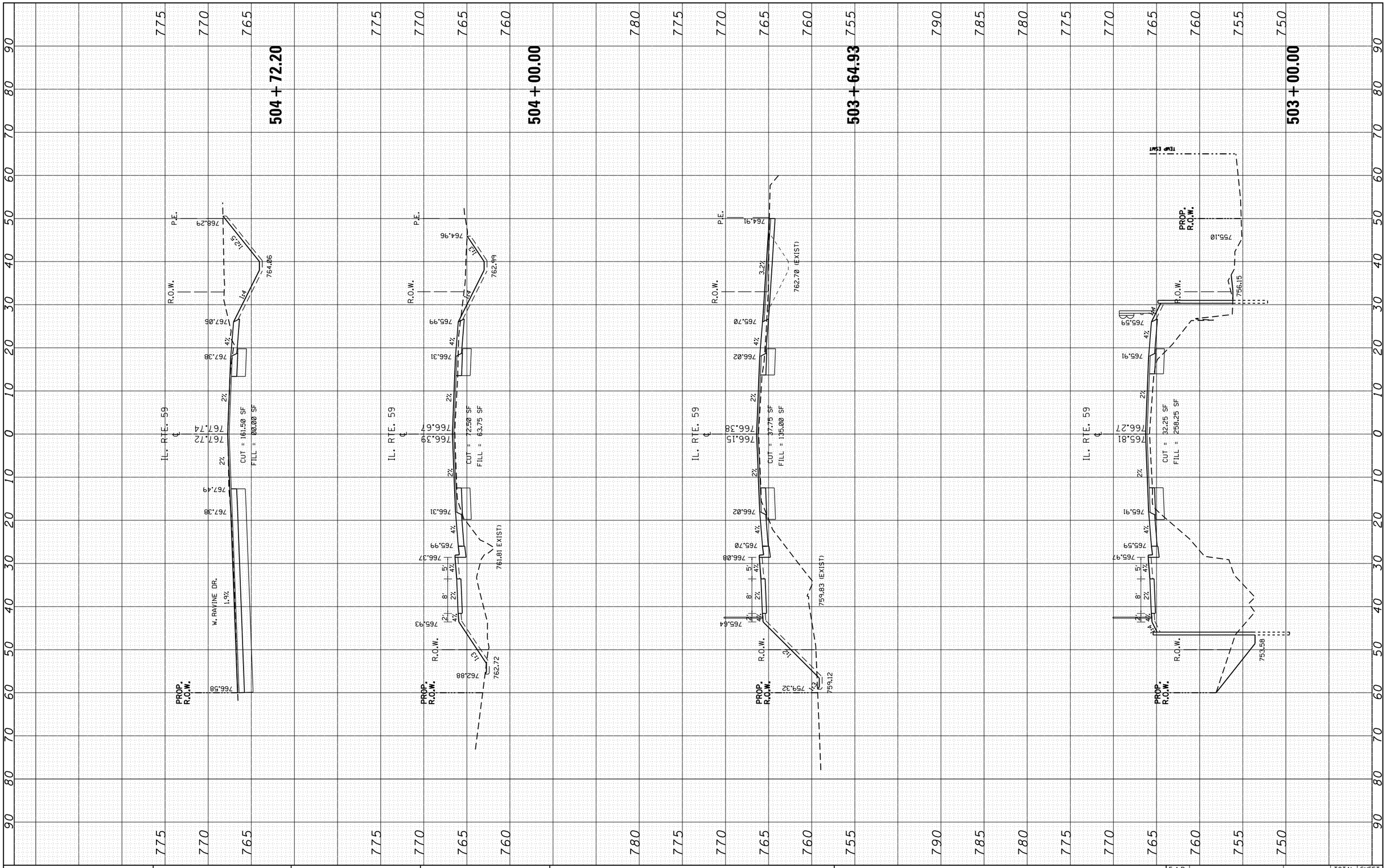
CROSS SECTIONS
IL. ROUTE 59 AT PETITE LAKE RD.

SCALE: | SHEET OF SHEETS | STA. 501+00.00 TO STA. 502+89.45

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	106

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
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NO.	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

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Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

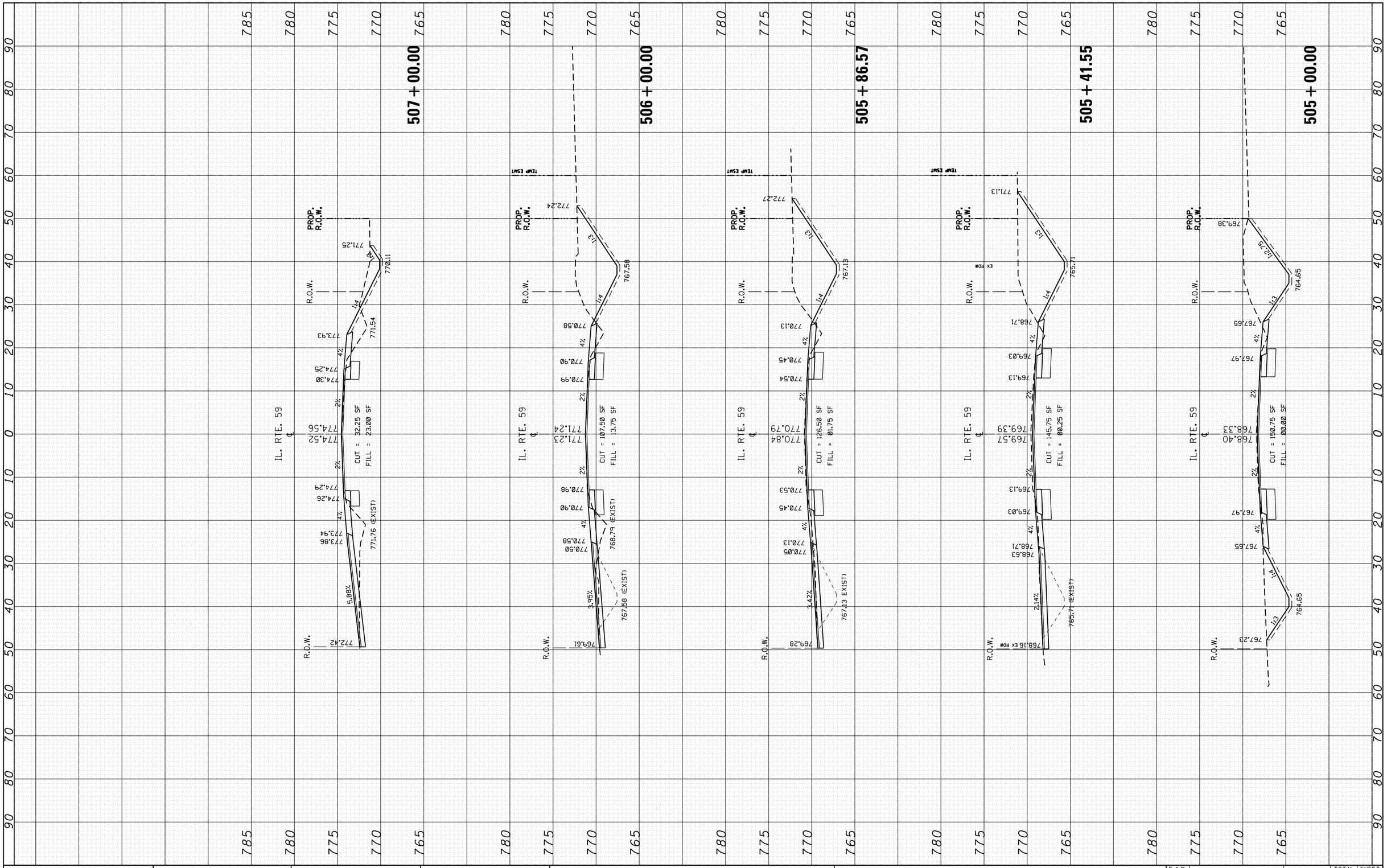
CROSS SECTIONS
IL. ROUTE 59 AT PETITE LAKE RD.

SCALE: | SHEET OF SHEETS | STA. 503+00.00 TO STA. 504+72.20

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	107

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
AREAS CHECKED		

NOTE BOOK NO.

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
AREAS CHECKED		

NOTE BOOK NO.

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Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

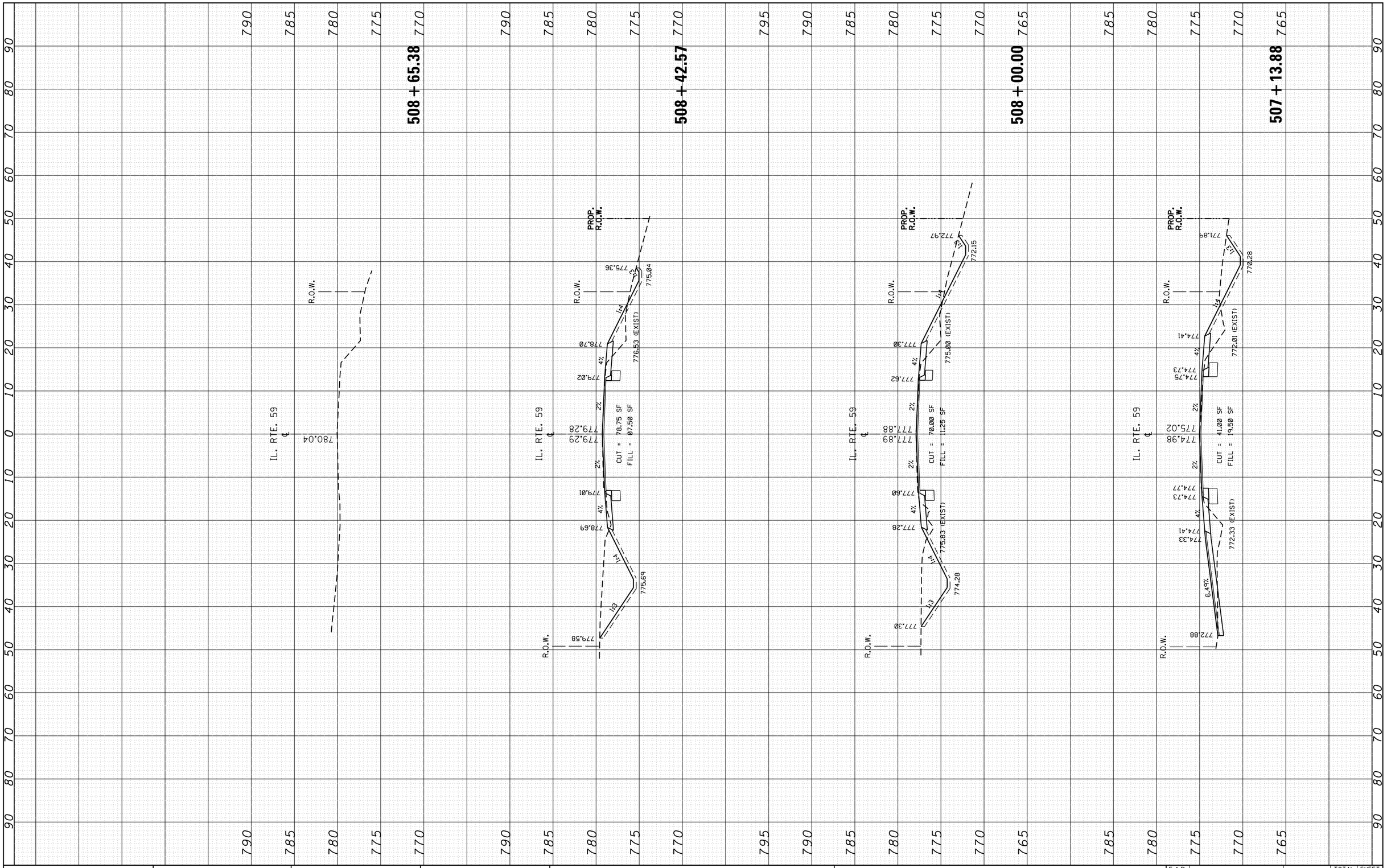
CROSS SECTIONS
IL. ROUTE 59 AT PETITE LAKE RD.

SCALE: | SHEET OF SHEETS | STA. 505+00.00 TO STA. 507+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	108

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
AREAS CHECKED		
NOTE BOOK NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
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AREAS		
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NOTE BOOK NO.		

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Default	PLOT DATE = 8/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

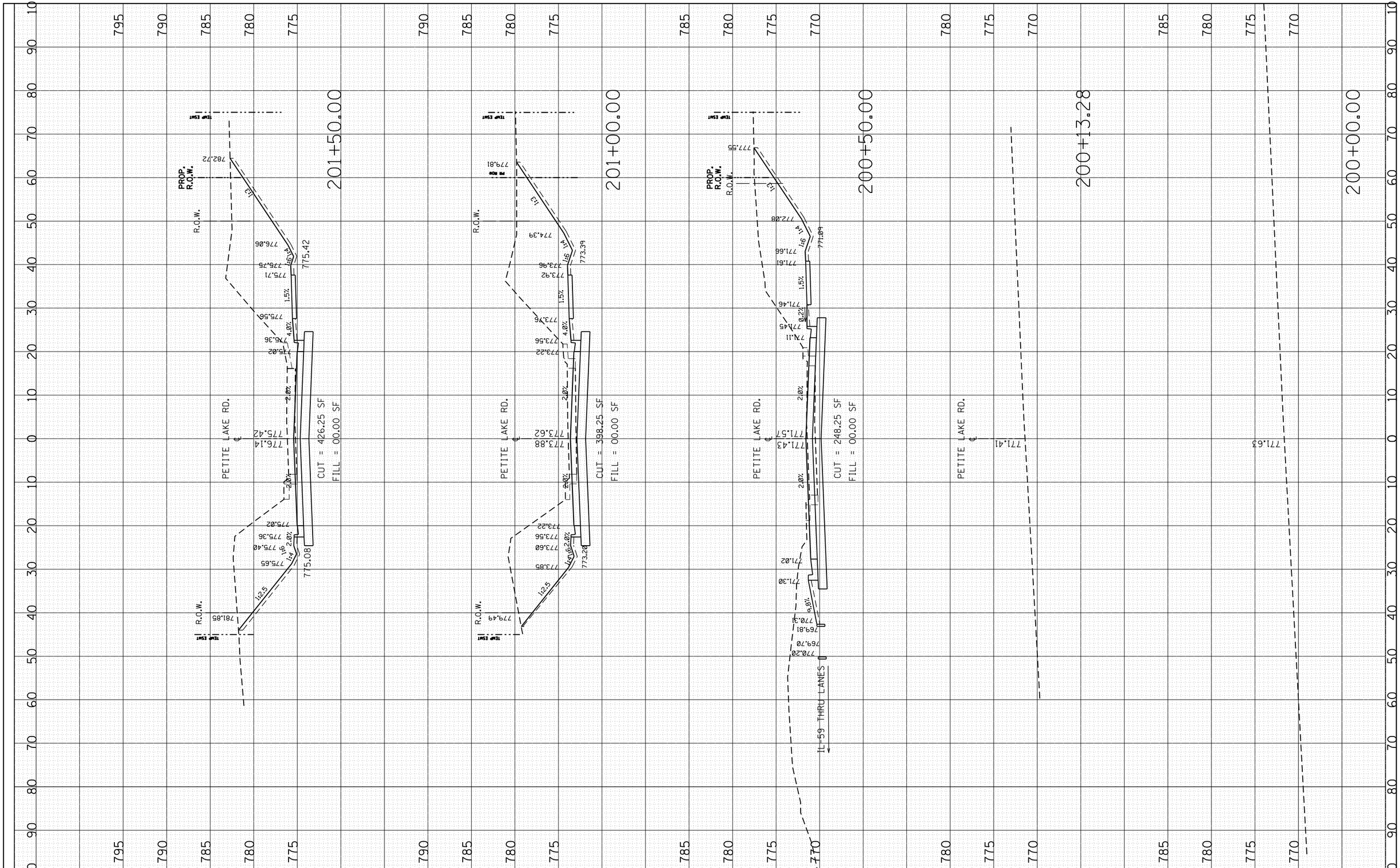
CROSS SECTIONS
IL. ROUTE 59 AT PETITE LAKE RD.

SCALE: | SHEET OF SHEETS | STA. 507+13.88 TO STA. 508+65.38

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	109

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
AREAS CHECKED		
NOTE BOOK		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
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TEMPLATE		
AREAS		
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USER NAME = ledezmarm

PLOT SCALE = 20.0000 ' / in.

PLOT DATE = 9/19/2016

DESIGNED -
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CHECKED -
DATE -

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

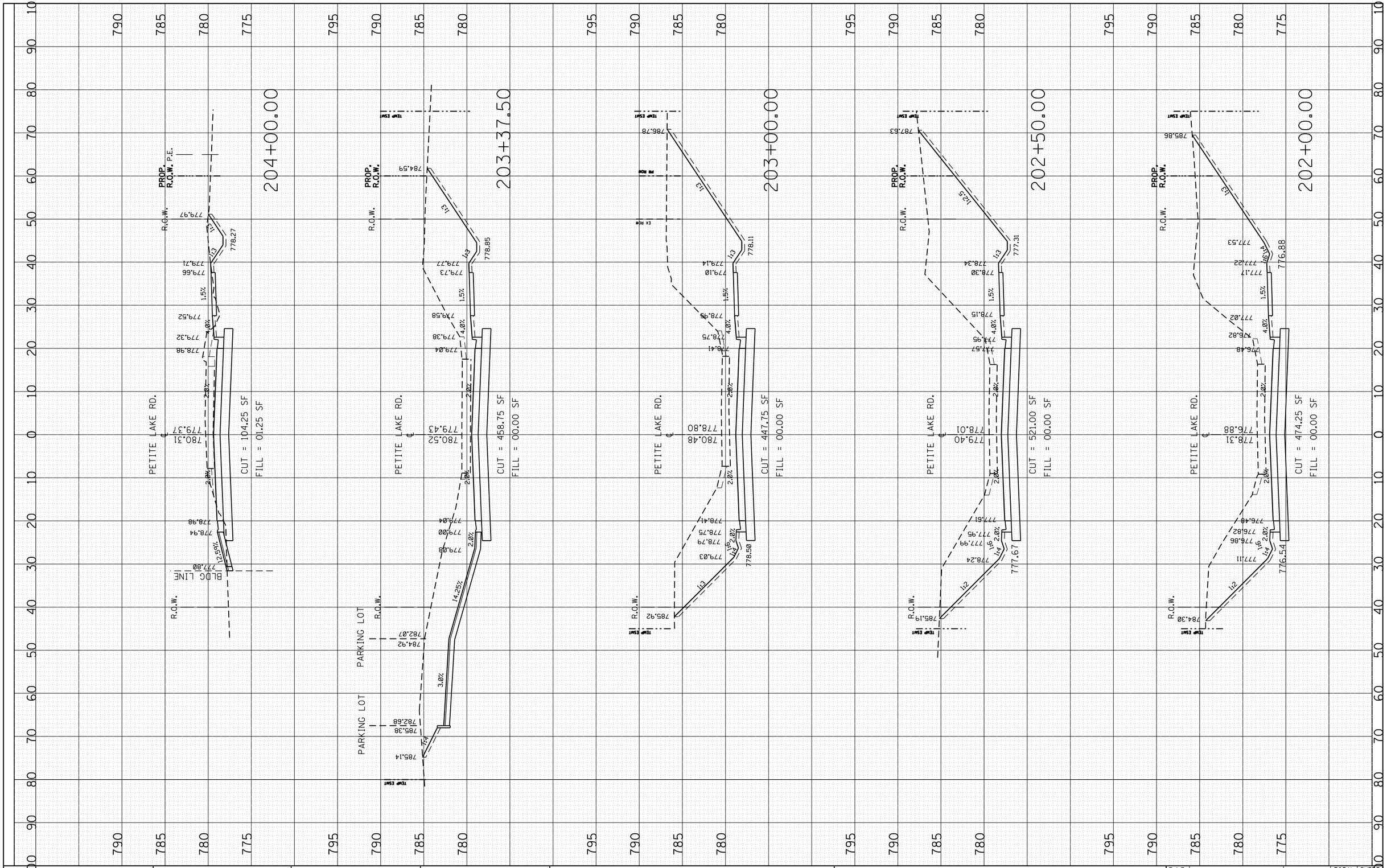
CROSS SECTION
PETITE LAKE ROAD

SCALE: SHEET OF SHEETS STA. 200+00.00 TO STA. 201+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	110

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



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Default	PLOT DATE = 9/19/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTION
PETITE LAKE ROAD

SCALE: | SHEET OF SHEETS | STA. 202+00.00 TO STA. 204+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	111

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		
NO.			

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
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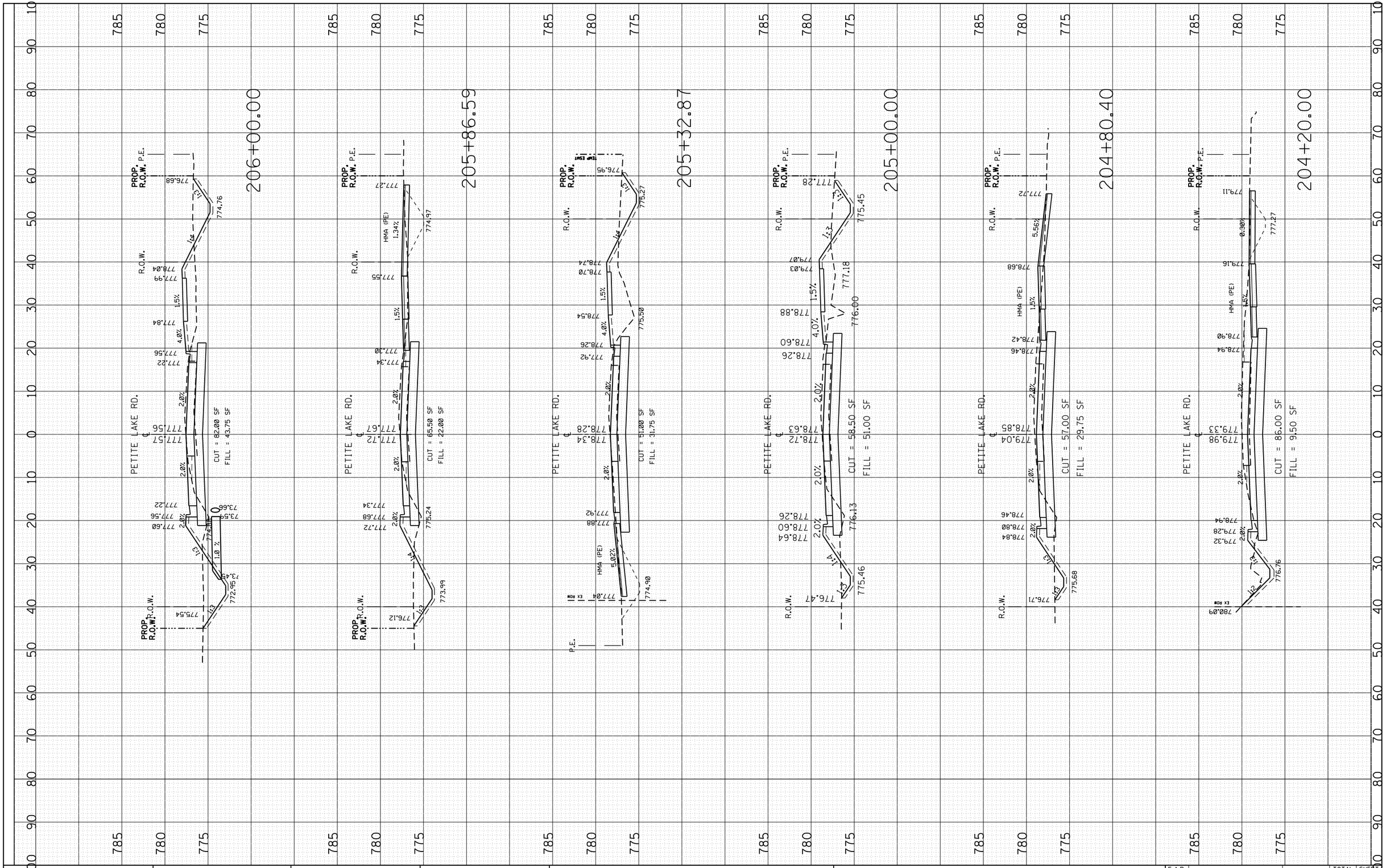

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
AREAS CHECKED		
NOTE BOOK NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
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TEMPLATE		
AREAS		
AREAS CHECKED		
NOTE BOOK NO.		

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PLOT SCALE = 20.0000 ' / in.

PLOT DATE = 9/19/2016

DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

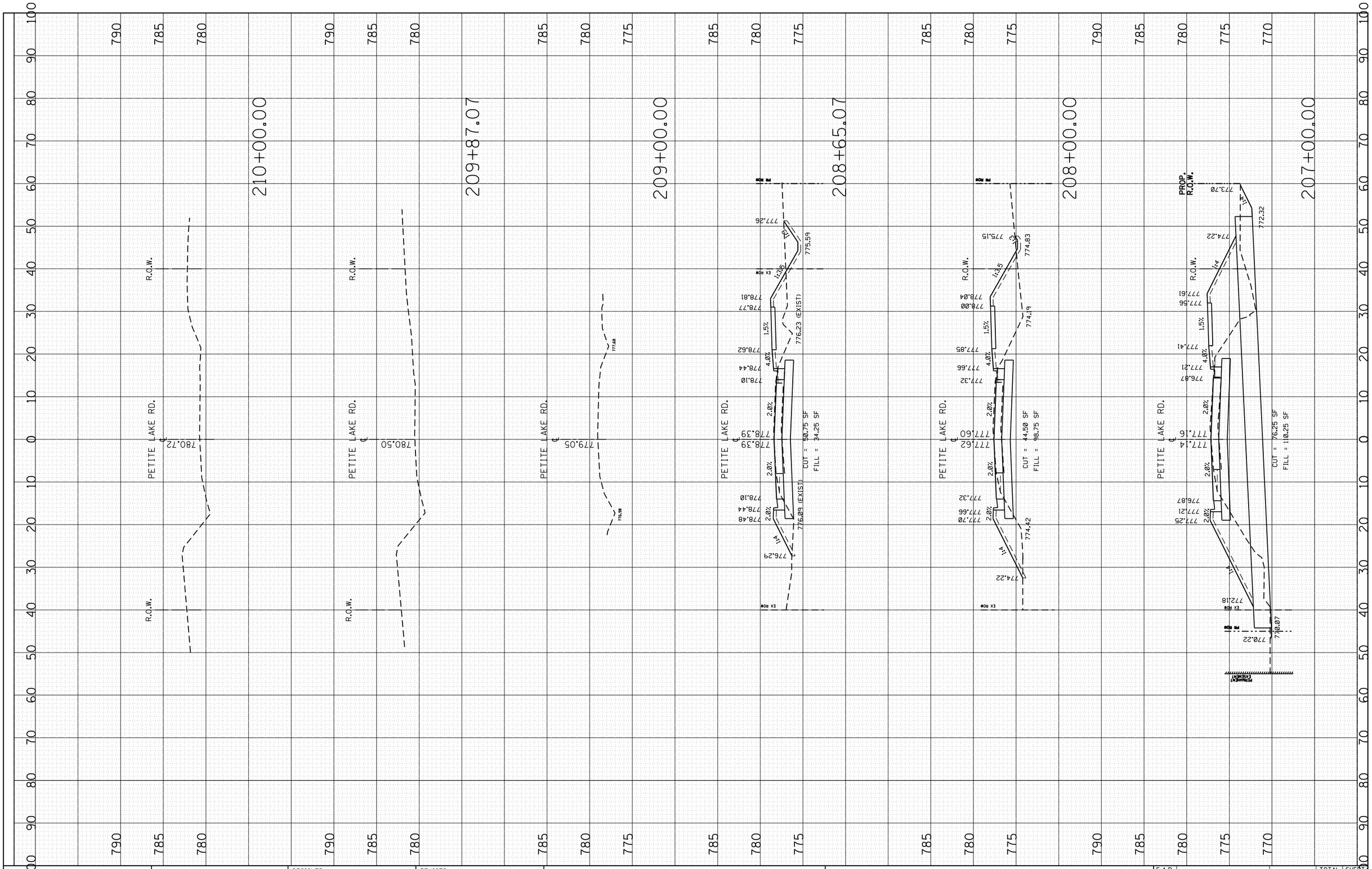
CROSS SECTION
PETITE LAKE ROAD

SCALE: | SHEET OF SHEETS | STA. 204+20.00 TO STA. 206+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	112

CONTRACT NO. 60W06

ILLINOIS FED. AID PROJECT



FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
	AREAS		
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ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
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	TEMPLATE		
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Default

USER NAME = ledezmarm
PLOT SCALE = 20.0000 ' / in.
PLOT DATE = 8/19/2016

DESIGNED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTION
PETITE LAKE ROAD

SCALE: SHEET OF SHEETS STA. 207+00.00 TO STA. 210+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
104	105N-4	LAKE	113	113
		CONTRACT NO. 60W06		
		ILLINOIS FED. AID PROJECT		