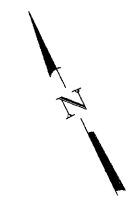
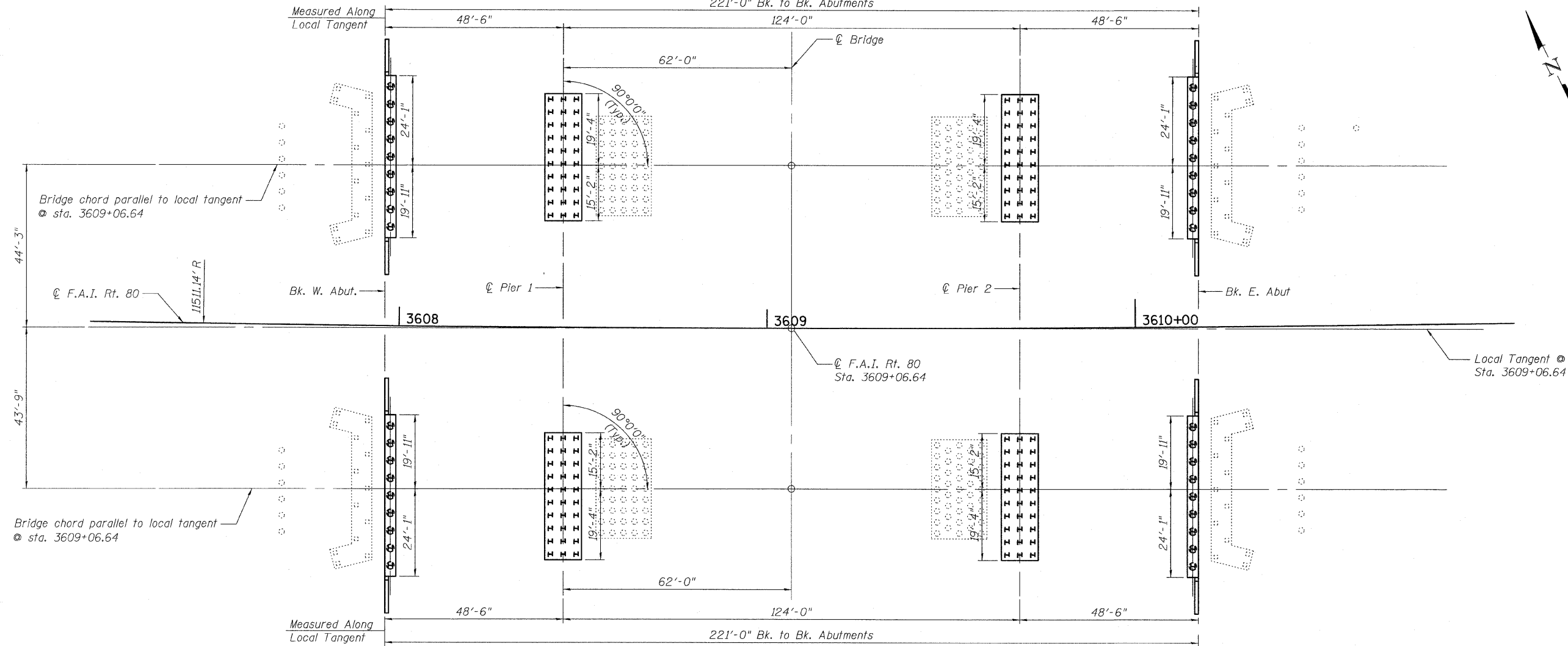
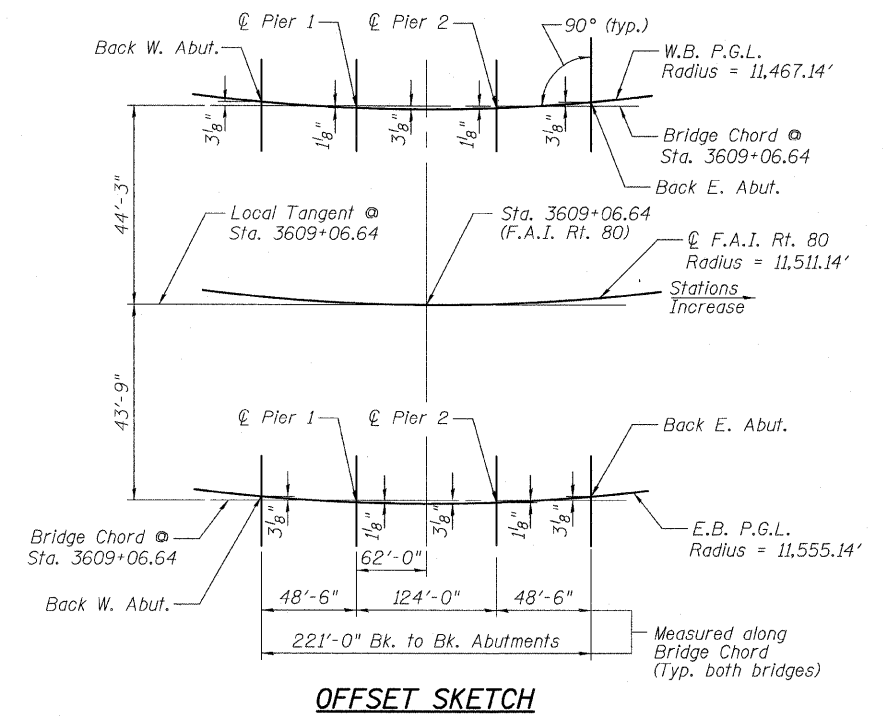


STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**FOOTING LAYOUT**



**OFFSET SKETCH**

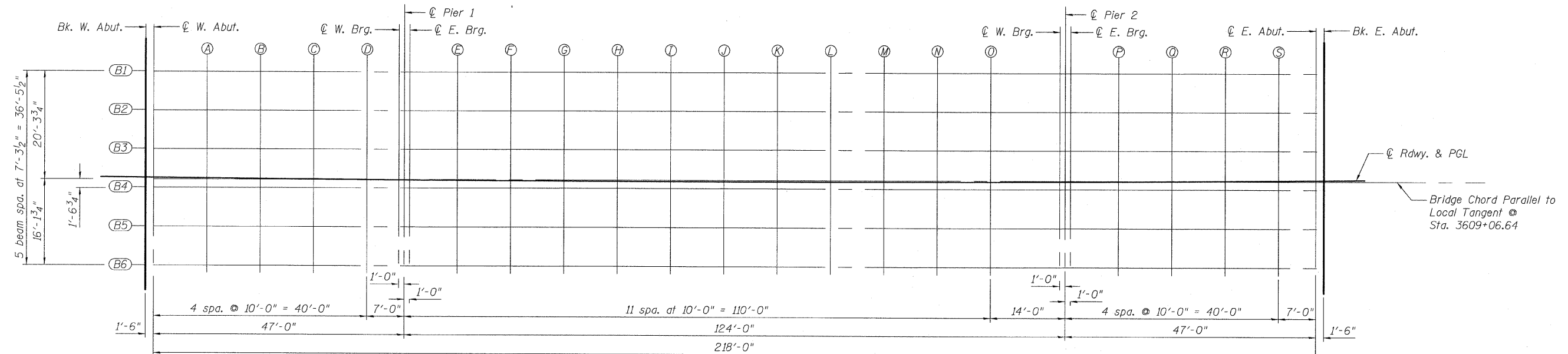
**OFFSET SKETCH  
AND FOOTING LAYOUT  
STRUCTURE NO. 006-0174 (EB)  
STRUCTURE NO. 006-0175 (WB)**

|          |     |
|----------|-----|
| DESIGNED | IM  |
| CHECKED  | PDF |
| DRAWN    | IM  |
| CHECKED  | PDF |

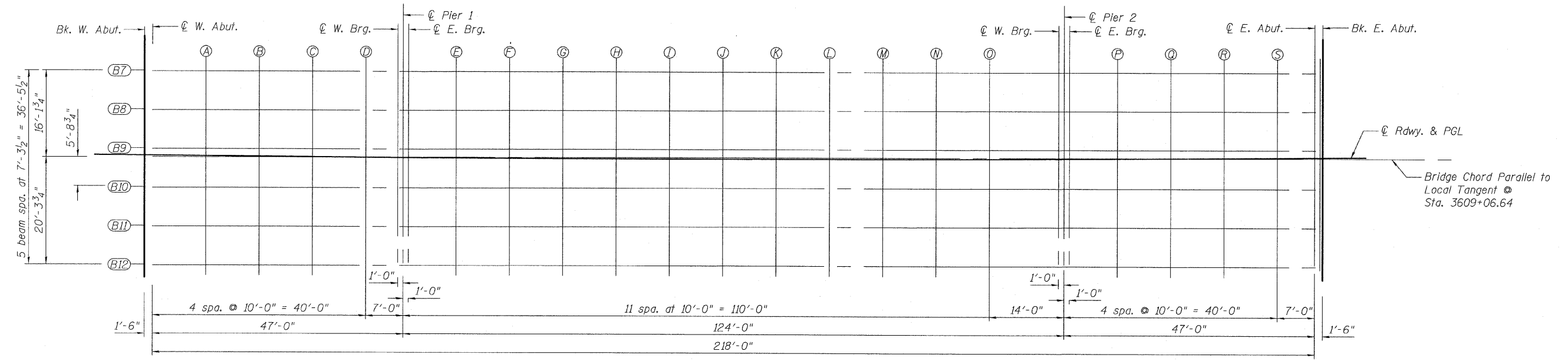
|                     |                    |          |                  |              |           |
|---------------------|--------------------|----------|------------------|--------------|-----------|
| SHEET NO. 3         | F.A. RTE.          | SECTION  | COUNTY           | TOTAL SHEETS | SHEET NO. |
|                     | 80                 | *        | BUREAU           | 347          | 201       |
| 39 SHEETS           | CONTRACT NO. 66908 |          |                  |              |           |
| FED. ROAD DIST. NO. |                    | ILLINOIS | FED. AID PROJECT |              |           |

TYLIN INTERNATIONAL

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DEPARTMENT OF TRANSPORTATION



ELEVATION LOCATION PLAN - WESTBOUND



ELEVATION LOCATION PLAN - EASTBOUND

TOP OF SLAB ELEVATION LAYOUT  
STRUCTURE NO. 006-0174 (EB)  
STRUCTURE NO. 006-0175 (WB)

|          |      |
|----------|------|
| DESIGNED | I.M. |
| CHECKED  | PDF  |
| DRAWN    | I.M. |
| CHECKED  | PDF  |

|                          |                    |              |                  |                     |                  |
|--------------------------|--------------------|--------------|------------------|---------------------|------------------|
| SHEET NO. 4<br>39 SHEETS | F.A. RTE.<br>80    | SECTION<br>* | COUNTY<br>BUREAU | TOTAL SHEETS<br>341 | SHEET NO.<br>262 |
|                          | CONTRACT NO. 66908 |              |                  |                     |                  |
| FED. ROAD DIST. NO.      |                    | ILLINOIS     | FED. AID PROJECT |                     |                  |

TYLINTERNATIONAL

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**P.G.L.**

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. W. Abut. | 3607+96.56 | 0.00   | 647.54                       | 647.54   |
| @ W. Abut.   | 3607+98.06 | 0.00   | 647.55                       | 647.55   |
| A            | 3608+08.02 | 0.00   | 647.61                       | 647.61   |
| B            | 3608+17.98 | 0.00   | 647.67                       | 647.67   |
| C            | 3608+27.94 | 0.00   | 647.73                       | 647.73   |
| D            | 3608+37.91 | 0.00   | 647.79                       | 647.79   |
| @W. Brg.     | 3608+43.88 | 0.00   | 647.82                       | 647.82   |
| @ Pier 1     | 3608+44.88 | 0.00   | 647.83                       | 647.83   |
| @E. Brg.     | 3608+45.88 | 0.00   | 647.83                       | 647.83   |
| E            | 3608+54.84 | 0.00   | 647.88                       | 647.92   |
| F            | 3608+64.80 | 0.00   | 647.94                       | 648.01   |
| G            | 3608+74.77 | 0.00   | 647.99                       | 648.09   |
| H            | 3608+84.73 | 0.00   | 648.04                       | 648.16   |
| I            | 3608+94.69 | 0.00   | 648.09                       | 648.22   |
| J            | 3609+04.65 | 0.00   | 648.14                       | 648.28   |
| K            | 3609+14.61 | 0.00   | 648.18                       | 648.31   |
| L            | 3609+24.58 | 0.00   | 648.23                       | 648.35   |
| M            | 3609+34.54 | 0.00   | 648.27                       | 648.37   |
| N            | 3609+44.50 | 0.00   | 648.32                       | 648.40   |
| O            | 3609+54.46 | 0.00   | 648.36                       | 648.41   |
| @W. Brg.     | 3609+67.41 | 0.00   | 648.41                       | 648.41   |
| @ Pier 2     | 3609+68.41 | 0.00   | 648.41                       | 648.41   |
| @E. Brg.     | 3609+69.40 | 0.00   | 648.42                       | 648.42   |
| P            | 3609+78.37 | 0.00   | 648.45                       | 648.45   |
| Q            | 3609+88.33 | 0.00   | 648.49                       | 648.49   |
| R            | 3609+98.29 | 0.00   | 648.52                       | 648.52   |
| S            | 3610+08.26 | 0.00   | 648.56                       | 648.56   |
| @ E. Abut.   | 3610+15.23 | 0.00   | 648.58                       | 648.58   |
| Bk. E. Abut. | 3610+16.72 | 0.00   | 648.58                       | 648.58   |

**BEAM B7**

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. W. Abut. | 3607+96.41 | -15.88 | 647.30                       | 647.30   |
| @ W. Abut.   | 3607+97.91 | -15.88 | 647.31                       | 647.31   |
| A            | 3608+07.88 | -15.97 | 647.37                       | 647.37   |
| B            | 3608+17.86 | -16.05 | 647.43                       | 647.43   |
| C            | 3608+27.83 | -16.12 | 647.49                       | 647.49   |
| D            | 3608+37.81 | -16.19 | 647.55                       | 647.55   |
| @W. Brg.     | 3608+43.80 | -16.22 | 647.58                       | 647.58   |
| @ Pier 1     | 3608+44.79 | -16.23 | 647.59                       | 647.59   |
| @E. Brg.     | 3608+45.79 | -16.23 | 647.59                       | 647.59   |
| E            | 3608+54.77 | -16.28 | 647.64                       | 647.68   |
| F            | 3608+64.74 | -16.32 | 647.69                       | 647.76   |
| G            | 3608+74.72 | -16.35 | 647.74                       | 647.84   |
| H            | 3608+84.70 | -16.37 | 647.79                       | 647.91   |
| I            | 3608+94.67 | -16.39 | 647.84                       | 647.97   |
| J            | 3609+04.65 | -16.39 | 647.89                       | 648.03   |
| K            | 3609+14.62 | -16.39 | 647.94                       | 648.07   |
| L            | 3609+24.60 | -16.38 | 647.98                       | 648.10   |
| M            | 3609+34.58 | -16.36 | 648.03                       | 648.13   |
| N            | 3609+44.55 | -16.33 | 648.07                       | 648.15   |
| O            | 3609+54.53 | -16.29 | 648.11                       | 648.16   |
| @W. Brg.     | 3609+67.50 | -16.23 | 648.17                       | 648.17   |
| @ Pier 2     | 3609+68.50 | -16.23 | 648.17                       | 648.17   |
| @E. Brg.     | 3609+69.49 | -16.22 | 648.17                       | 648.17   |
| P            | 3609+78.47 | -16.17 | 648.21                       | 648.21   |
| Q            | 3609+88.45 | -16.10 | 648.25                       | 648.25   |
| R            | 3609+98.42 | -16.03 | 648.28                       | 648.28   |
| S            | 3610+08.40 | -15.94 | 648.32                       | 648.32   |
| @ E. Abut.   | 3610+15.38 | -15.88 | 648.34                       | 648.34   |
| Bk. E. Abut. | 3610+16.88 | -15.88 | 648.35                       | 648.35   |

**BEAM B8**

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. W. Abut. | 3607+96.48 | -8.57  | 647.41                       | 647.41   |
| @ W. Abut.   | 3607+97.98 | -8.59  | 647.42                       | 647.42   |
| A            | 3608+07.95 | -8.68  | 647.48                       | 647.48   |
| B            | 3608+17.91 | -8.76  | 647.54                       | 647.54   |
| C            | 3608+27.88 | -8.83  | 647.60                       | 647.60   |
| D            | 3608+37.85 | -8.90  | 647.66                       | 647.66   |
| @W. Brg.     | 3608+43.84 | -8.93  | 647.69                       | 647.69   |
| @ Pier 1     | 3608+44.83 | -8.94  | 647.69                       | 647.69   |
| @E. Brg.     | 3608+45.83 | -8.94  | 647.70                       | 647.70   |
| E            | 3608+54.80 | -8.99  | 647.75                       | 647.79   |
| F            | 3608+64.77 | -9.03  | 647.80                       | 647.87   |
| G            | 3608+74.74 | -9.06  | 647.85                       | 647.95   |
| H            | 3608+84.71 | -9.08  | 647.90                       | 648.02   |
| I            | 3608+94.68 | -9.10  | 647.95                       | 648.08   |
| J            | 3609+04.65 | -9.10  | 648.00                       | 648.14   |
| K            | 3609+14.62 | -9.10  | 648.05                       | 648.18   |
| L            | 3609+24.59 | -9.09  | 648.09                       | 648.21   |
| M            | 3609+34.56 | -9.07  | 648.14                       | 648.24   |
| N            | 3609+44.53 | -9.04  | 648.18                       | 648.26   |
| O            | 3609+54.50 | -9.00  | 648.22                       | 648.27   |
| @W. Brg.     | 3609+67.46 | -8.94  | 648.28                       | 648.28   |
| @ Pier 2     | 3609+68.46 | -8.94  | 648.28                       | 648.28   |
| @E. Brg.     | 3609+69.45 | -8.93  | 648.28                       | 648.28   |
| P            | 3609+78.43 | -8.88  | 648.32                       | 648.32   |
| Q            | 3609+88.39 | -8.81  | 648.36                       | 648.36   |
| R            | 3609+98.36 | -8.74  | 648.39                       | 648.39   |
| S            | 3610+08.33 | -8.65  | 648.43                       | 648.43   |
| @ E. Abut.   | 3610+15.31 | -8.59  | 648.45                       | 648.45   |
| Bk. E. Abut. | 3610+16.81 | -8.57  | 648.46                       | 648.46   |

**BEAM B9**

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. W. Abut. | 3607+96.55 | -1.28  | 647.52                       | 647.52   |
| @ W. Abut.   | 3607+98.05 | -1.30  | 647.53                       | 647.53   |
| A            | 3608+08.01 | -1.39  | 647.59                       | 647.59   |
| B            | 3608+17.97 | -1.47  | 647.65                       | 647.65   |
| C            | 3608+27.93 | -1.54  | 647.71                       | 647.71   |
| D            | 3608+37.90 | -1.61  | 647.77                       | 647.77   |
| @W. Brg.     | 3608+43.87 | -1.64  | 647.80                       | 647.80   |
| @ Pier 1     | 3608+44.87 | -1.64  | 647.80                       | 647.80   |
| @E. Brg.     | 3608+45.87 | -1.65  | 647.81                       | 647.81   |
| E            | 3608+54.83 | -1.69  | 647.86                       | 647.90   |
| F            | 3608+64.80 | -1.73  | 647.91                       | 647.98   |
| G            | 3608+74.76 | -1.77  | 647.96                       | 648.06   |
| H            | 3608+84.72 | -1.79  | 648.01                       | 648.13   |
| I            | 3608+94.69 | -1.80  | 648.06                       | 648.19   |
| J            | 3609+04.65 | -1.81  | 648.11                       | 648.25   |
| K            | 3609+14.61 | -1.81  | 648.16                       | 648.29   |
| L            | 3609+24.58 | -1.80  | 648.20                       | 648.32   |
| M            | 3609+34.54 | -1.79  | 648.25                       | 648.35   |
| N            | 3609+44.51 | -1.75  | 648.29                       | 648.37   |
| O            | 3609+54.47 | -1.71  | 648.33                       | 648.38   |
| @W. Brg.     | 3609+67.42 | -1.65  | 648.38                       | 648.38   |
| @ Pier 2     | 3609+68.42 | -1.64  | 648.39                       | 648.39   |
| @E. Brg.     | 3609+69.41 | -1.64  | 648.39                       | 648.39   |
| P            | 3609+78.38 | -1.59  | 648.43                       | 648.43   |
| Q            | 3609+88.34 | -1.52  | 648.46                       | 648.46   |
| R            | 3609+98.31 | -1.44  | 648.50                       | 648.50   |
| S            | 3610+08.27 | -1.36  | 648.54                       | 648.54   |
| @ E. Abut.   | 3610+15.24 | -1.30  | 648.56                       | 648.56   |
| Bk. E. Abut. | 3610+16.74 | -1.28  | 648.56                       | 648.56   |

**TOP OF SLAB ELEVATIONS - I  
STRUCTURE NO. 006-0174 (EB)**

|          |     |
|----------|-----|
| DESIGNED | IM  |
| CHECKED  | PDF |
| DRAWN    | IM  |
| CHECKED  | PDF |

|                     |                    |          |                  |              |           |
|---------------------|--------------------|----------|------------------|--------------|-----------|
| SHEET NO. 5         | F.A. RTE.          | SECTION  | COUNTY           | TOTAL SHEETS | SHEET NO. |
|                     | 80                 | *        | BUREAU           | 394          | 203       |
| 39 SHEETS           | CONTRACT NO. 66908 |          |                  |              |           |
| FED. ROAD DIST. NO. |                    | ILLINOIS | FED. AID PROJECT |              |           |

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**BEAM B10**

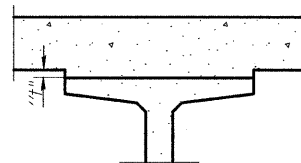
| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. W. Abut. | 3607+96.62 | 6.01   | 647.63                       | 647.63   |
| W. Abut.     | 3607+98.11 | 5.99   | 647.64                       | 647.64   |
| A            | 3608+08.07 | 5.90   | 647.70                       | 647.70   |
| B            | 3608+18.03 | 5.82   | 647.76                       | 647.76   |
| C            | 3608+27.98 | 5.75   | 647.82                       | 647.82   |
| D            | 3608+37.94 | 5.69   | 647.88                       | 647.88   |
| W. Brg.      | 3608+43.91 | 5.65   | 647.91                       | 647.91   |
| Pier 1       | 3608+44.91 | 5.65   | 647.91                       | 647.91   |
| E. Brg.      | 3608+45.91 | 5.64   | 647.92                       | 647.92   |
| E            | 3608+54.87 | 5.60   | 647.97                       | 648.01   |
| F            | 3608+64.82 | 5.56   | 648.02                       | 648.09   |
| G            | 3608+74.78 | 5.52   | 648.07                       | 648.17   |
| H            | 3608+84.74 | 5.50   | 648.12                       | 648.24   |
| I            | 3608+94.70 | 5.49   | 648.17                       | 648.30   |
| J            | 3609+04.65 | 5.48   | 648.22                       | 648.36   |
| K            | 3609+14.61 | 5.48   | 648.27                       | 648.40   |
| L            | 3609+24.57 | 5.49   | 648.31                       | 648.43   |
| M            | 3609+34.52 | 5.51   | 648.36                       | 648.46   |
| N            | 3609+44.48 | 5.54   | 648.40                       | 648.48   |
| O            | 3609+54.44 | 5.58   | 648.44                       | 648.49   |
| W. Brg.      | 3609+67.38 | 5.64   | 648.49                       | 648.49   |
| Pier 2       | 3609+68.38 | 5.65   | 648.50                       | 648.50   |
| E. Brg.      | 3609+69.37 | 5.65   | 648.50                       | 648.50   |
| P            | 3609+78.33 | 5.70   | 648.54                       | 648.54   |
| Q            | 3609+88.29 | 5.77   | 648.57                       | 648.57   |
| R            | 3609+98.25 | 5.85   | 648.61                       | 648.61   |
| S            | 3610+08.20 | 5.93   | 648.64                       | 648.64   |
| E. Abut.     | 3610+15.17 | 5.99   | 648.67                       | 648.67   |
| Bk. E. Abut. | 3610+16.67 | 6.01   | 648.67                       | 648.67   |

**BEAM B11**

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. W. Abut. | 3607+96.69 | 13.30  | 647.74                       | 647.74   |
| W. Abut.     | 3607+98.18 | 13.29  | 647.75                       | 647.75   |
| A            | 3608+08.13 | 13.20  | 647.81                       | 647.81   |
| B            | 3608+18.08 | 13.11  | 647.87                       | 647.87   |
| C            | 3608+28.03 | 13.04  | 647.93                       | 647.93   |
| D            | 3608+37.98 | 12.98  | 647.99                       | 647.99   |
| W. Brg.      | 3608+43.95 | 12.94  | 648.02                       | 648.02   |
| Pier 1       | 3608+44.95 | 12.94  | 648.02                       | 648.02   |
| E. Brg.      | 3608+45.94 | 12.93  | 648.03                       | 648.03   |
| E            | 3608+54.90 | 12.89  | 648.08                       | 648.12   |
| F            | 3608+64.85 | 12.85  | 648.13                       | 648.20   |
| G            | 3608+74.80 | 12.82  | 648.18                       | 648.28   |
| H            | 3608+84.75 | 12.79  | 648.23                       | 648.35   |
| I            | 3608+94.70 | 12.78  | 648.28                       | 648.41   |
| J            | 3609+04.65 | 12.77  | 648.33                       | 648.47   |
| K            | 3609+14.60 | 12.77  | 648.38                       | 648.51   |
| L            | 3609+24.56 | 12.79  | 648.42                       | 648.54   |
| M            | 3609+34.51 | 12.81  | 648.47                       | 648.57   |
| N            | 3609+44.46 | 12.83  | 648.51                       | 648.59   |
| O            | 3609+54.41 | 12.87  | 648.55                       | 648.60   |
| W. Brg.      | 3609+67.34 | 12.93  | 648.60                       | 648.60   |
| Pier 2       | 3609+68.34 | 12.94  | 648.61                       | 648.61   |
| E. Brg.      | 3609+69.33 | 12.94  | 648.61                       | 648.61   |
| P            | 3609+78.29 | 13.00  | 648.65                       | 648.65   |
| Q            | 3609+88.24 | 13.06  | 648.68                       | 648.68   |
| R            | 3609+98.19 | 13.14  | 648.72                       | 648.72   |
| S            | 3610+08.14 | 13.22  | 648.75                       | 648.75   |
| E. Abut.     | 3610+15.11 | 13.29  | 648.78                       | 648.78   |
| Bk. E. Abut. | 3610+16.60 | 13.30  | 648.78                       | 648.78   |

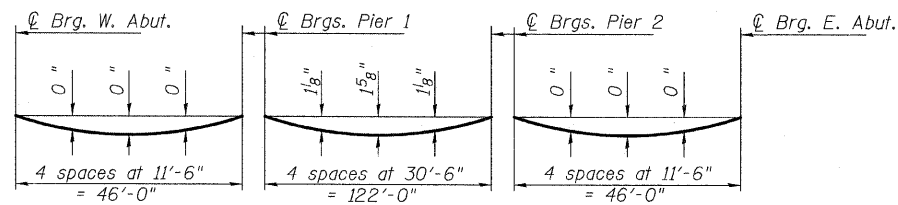
**BEAM B12**

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. W. Abut. | 3607+96.76 | 20.59  | 647.85                       | 647.85   |
| W. Abut.     | 3607+98.25 | 20.58  | 647.86                       | 647.86   |
| A            | 3608+08.19 | 20.49  | 647.92                       | 647.92   |
| B            | 3608+18.14 | 20.41  | 647.98                       | 647.98   |
| C            | 3608+28.08 | 20.33  | 648.04                       | 648.04   |
| D            | 3608+38.03 | 20.27  | 648.09                       | 648.09   |
| W. Brg.      | 3608+43.99 | 20.24  | 648.13                       | 648.13   |
| Pier 1       | 3608+44.99 | 20.23  | 648.13                       | 648.13   |
| E. Brg.      | 3608+45.98 | 20.22  | 648.14                       | 648.14   |
| E            | 3608+54.93 | 20.18  | 648.19                       | 648.23   |
| F            | 3609+64.88 | 20.14  | 648.20                       | 648.27   |
| G            | 3608+74.82 | 20.11  | 648.29                       | 648.39   |
| H            | 3608+84.77 | 20.08  | 648.34                       | 648.46   |
| I            | 3608+94.71 | 20.07  | 648.39                       | 648.52   |
| J            | 3609+04.66 | 20.06  | 648.44                       | 648.58   |
| K            | 3609+14.60 | 20.07  | 648.49                       | 648.62   |
| L            | 3609+24.54 | 20.08  | 648.53                       | 648.65   |
| M            | 3609+34.49 | 20.10  | 648.58                       | 648.68   |
| N            | 3609+44.43 | 20.13  | 648.62                       | 648.70   |
| O            | 3609+54.38 | 20.16  | 648.66                       | 648.71   |
| W. Brg.      | 3609+67.31 | 20.22  | 648.71                       | 648.71   |
| Pier 2       | 3609+68.30 | 20.23  | 648.72                       | 648.72   |
| E. Brg.      | 3609+69.29 | 20.24  | 648.72                       | 648.72   |
| P            | 3609+78.24 | 20.29  | 648.75                       | 648.75   |
| Q            | 3609+88.19 | 20.35  | 648.79                       | 648.79   |
| R            | 3609+98.13 | 20.43  | 648.83                       | 648.83   |
| S            | 3610+08.08 | 20.51  | 648.86                       | 648.86   |
| E. Abut.     | 3610+15.04 | 20.58  | 648.89                       | 648.89   |
| Bk. E. Abut. | 3610+16.53 | 20.59  | 648.89                       | 648.89   |



To determine "f": After all precast prestressed beams have been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflections" shown below, minus slab thickness, equals the fillet heights "f" above top flanges of beams.

**FILLET HEIGHTS**



**DEAD LOAD DEFLECTION DIAGRAM**

Note: (Includes weight of concrete, excluding beams).  
The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown below.

|          |     |
|----------|-----|
| DESIGNED | IM  |
| CHECKED  | PDF |
| DRAWN    | IM  |
| CHECKED  | PDF |

**TOP OF SLAB ELEVATIONS - II  
STRUCTURE NO. 006-0174 (EB)**

|             |                     |         |                           |              |                    |
|-------------|---------------------|---------|---------------------------|--------------|--------------------|
| SHEET NO. 6 | F.A. RTE.           | SECTION | COUNTY                    | TOTAL SHEETS | SHEET NO.          |
|             | 80                  | *       | BUREAU                    | 344          | 204                |
| 39 SHEETS   | FED. ROAD DIST. NO. |         | ILLINOIS FED. AID PROJECT |              | CONTRACT NO. 66908 |

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

P.G.L.

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. W. Abut. | 3607+95.72 | 0.00   | 647.25                       | 647.25   |
| W. Abut.     | 3607+97.22 | 0.00   | 647.26                       | 647.26   |
| A            | 3608+07.26 | 0.00   | 647.32                       | 647.32   |
| B            | 3608+17.30 | 0.00   | 647.37                       | 647.37   |
| C            | 3608+27.34 | 0.00   | 647.43                       | 647.43   |
| D            | 3608+37.38 | 0.00   | 647.48                       | 647.48   |
| W. Brg.      | 3608+43.40 | 0.00   | 647.51                       | 647.51   |
| Pier 1       | 3608+44.41 | 0.00   | 647.52                       | 647.52   |
| E. Brg.      | 3608+45.41 | 0.00   | 647.52                       | 647.52   |
| E            | 3608+54.44 | 0.00   | 647.57                       | 647.61   |
| F            | 3608+64.48 | 0.00   | 647.62                       | 647.69   |
| G            | 3608+74.52 | 0.00   | 647.67                       | 647.77   |
| H            | 3608+84.56 | 0.00   | 647.72                       | 647.84   |
| I            | 3608+94.60 | 0.00   | 647.76                       | 647.89   |
| J            | 3609+04.64 | 0.00   | 647.81                       | 647.95   |
| K            | 3609+14.67 | 0.00   | 647.85                       | 647.98   |
| L            | 3609+24.71 | 0.00   | 647.90                       | 648.02   |
| M            | 3609+34.75 | 0.00   | 647.94                       | 648.04   |
| N            | 3609+44.79 | 0.00   | 647.98                       | 648.06   |
| O            | 3609+54.83 | 0.00   | 648.02                       | 648.07   |
| W. Brg.      | 3609+67.88 | 0.00   | 648.06                       | 648.06   |
| Pier 2       | 3609+68.88 | 0.00   | 648.07                       | 648.07   |
| E. Brg.      | 3609+69.89 | 0.00   | 648.07                       | 648.07   |
| P            | 3609+78.92 | 0.00   | 648.10                       | 648.10   |
| Q            | 3609+88.96 | 0.00   | 648.14                       | 648.14   |
| R            | 3609+99.00 | 0.00   | 648.17                       | 648.17   |
| S            | 3610+09.04 | 0.00   | 648.20                       | 648.20   |
| E. Abut.     | 3610+16.06 | 0.00   | 648.22                       | 648.22   |
| Bk. E. Abut. | 3610+17.57 | 0.00   | 648.23                       | 648.23   |

BEAM B1

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. W. Abut. | 3607+95.52 | -20.03 | 646.95                       | 646.95   |
| W. Abut.     | 3607+97.03 | -20.04 | 646.96                       | 646.96   |
| A            | 3608+07.09 | -20.13 | 647.01                       | 647.01   |
| B            | 3608+17.14 | -20.22 | 647.07                       | 647.07   |
| C            | 3608+27.20 | -20.29 | 647.12                       | 647.12   |
| D            | 3608+37.26 | -20.35 | 647.18                       | 647.18   |
| W. Brg.      | 3608+43.29 | -20.39 | 647.21                       | 647.21   |
| Pier 1       | 3608+44.29 | -20.39 | 647.21                       | 647.21   |
| E. Brg.      | 3608+45.30 | -20.40 | 647.22                       | 647.22   |
| E            | 3608+54.35 | -20.44 | 647.26                       | 647.30   |
| F            | 3608+64.41 | -20.48 | 647.31                       | 647.38   |
| G            | 3608+74.46 | -20.52 | 647.36                       | 647.46   |
| H            | 3608+84.52 | -20.54 | 647.41                       | 647.53   |
| I            | 3608+94.58 | -20.55 | 647.46                       | 647.59   |
| J            | 3609+04.63 | -20.56 | 647.50                       | 647.64   |
| K            | 3609+14.69 | -20.56 | 647.55                       | 647.68   |
| L            | 3609+24.75 | -20.55 | 647.59                       | 647.71   |
| M            | 3609+34.80 | -20.53 | 647.63                       | 647.73   |
| N            | 3609+44.86 | -20.50 | 647.67                       | 647.75   |
| O            | 3609+54.91 | -20.46 | 647.71                       | 647.76   |
| W. Brg.      | 3609+67.99 | -20.40 | 647.76                       | 647.76   |
| Pier 2       | 3609+68.99 | -20.39 | 647.76                       | 647.76   |
| E. Brg.      | 3609+70.00 | -20.39 | 647.76                       | 647.76   |
| P            | 3609+79.05 | -20.33 | 647.80                       | 647.80   |
| Q            | 3609+89.10 | -20.27 | 647.83                       | 647.83   |
| R            | 3609+99.16 | -20.19 | 647.87                       | 647.87   |
| S            | 3610+09.22 | -20.11 | 647.90                       | 647.90   |
| E. Abut.     | 3610+16.26 | -20.04 | 647.92                       | 647.92   |
| Bk. E. Abut. | 3610+17.76 | -20.03 | 647.93                       | 647.93   |

BEAM B2

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. W. Abut. | 3607+95.59 | -12.74 | 647.06                       | 647.06   |
| W. Abut.     | 3607+97.10 | -12.75 | 647.07                       | 647.07   |
| A            | 3608+07.15 | -12.84 | 647.12                       | 647.12   |
| B            | 3608+17.20 | -12.92 | 647.18                       | 647.18   |
| C            | 3608+27.25 | -13.00 | 647.23                       | 647.23   |
| D            | 3608+37.30 | -13.06 | 647.29                       | 647.29   |
| W. Brg.      | 3608+43.33 | -13.10 | 647.32                       | 647.32   |
| Pier 1       | 3608+44.33 | -13.10 | 647.32                       | 647.32   |
| E. Brg.      | 3608+45.34 | -13.11 | 647.33                       | 647.33   |
| E            | 3608+54.38 | -13.15 | 647.37                       | 647.41   |
| F            | 3608+64.43 | -13.19 | 647.42                       | 647.49   |
| G            | 3608+74.48 | -13.22 | 647.47                       | 647.57   |
| H            | 3608+84.53 | -13.25 | 647.52                       | 647.64   |
| I            | 3608+94.58 | -13.26 | 647.57                       | 647.70   |
| J            | 3609+04.63 | -13.27 | 647.61                       | 647.75   |
| K            | 3609+14.68 | -13.27 | 647.65                       | 647.78   |
| L            | 3609+24.73 | -13.26 | 647.70                       | 647.82   |
| M            | 3609+34.78 | -13.24 | 647.74                       | 647.84   |
| N            | 3609+44.83 | -13.21 | 647.78                       | 647.86   |
| O            | 3609+54.88 | -13.17 | 647.82                       | 647.87   |
| W. Brg.      | 3609+67.95 | -13.11 | 647.87                       | 647.87   |
| Pier 2       | 3609+68.95 | -13.10 | 647.87                       | 647.87   |
| E. Brg.      | 3609+69.96 | -13.10 | 647.87                       | 647.87   |
| P            | 3609+79.00 | -13.04 | 647.91                       | 647.91   |
| Q            | 3609+89.05 | -12.98 | 647.94                       | 647.94   |
| R            | 3609+99.10 | -12.90 | 647.98                       | 647.98   |
| S            | 3610+09.15 | -12.82 | 648.01                       | 648.01   |
| E. Abut.     | 3610+16.19 | -12.75 | 648.03                       | 648.03   |
| Bk. E. Abut. | 3610+17.69 | -12.74 | 648.03                       | 648.03   |

BEAM B3

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. W. Abut. | 3607+95.67 | -5.45  | 647.17                       | 647.17   |
| W. Abut.     | 3607+97.17 | -5.46  | 647.18                       | 647.18   |
| A            | 3608+07.21 | -5.55  | 647.23                       | 647.23   |
| B            | 3608+17.26 | -5.63  | 647.29                       | 647.29   |
| C            | 3608+27.30 | -5.71  | 647.34                       | 647.34   |
| D            | 3608+37.34 | -5.77  | 647.40                       | 647.40   |
| W. Brg.      | 3608+43.37 | -5.80  | 647.43                       | 647.43   |
| Pier 1       | 3608+44.37 | -5.81  | 647.43                       | 647.43   |
| E. Brg.      | 3608+45.38 | -5.82  | 647.44                       | 647.44   |
| E            | 3608+54.42 | -5.86  | 647.48                       | 647.52   |
| F            | 3608+64.46 | -5.90  | 647.53                       | 647.60   |
| G            | 3608+74.50 | -5.93  | 647.58                       | 647.68   |
| H            | 3608+84.55 | -5.96  | 647.63                       | 647.75   |
| I            | 3608+94.59 | -5.97  | 647.68                       | 647.81   |
| J            | 3609+04.64 | -5.98  | 647.72                       | 647.86   |
| K            | 3609+14.68 | -5.98  | 647.76                       | 647.89   |
| L            | 3609+24.72 | -5.96  | 647.81                       | 647.93   |
| M            | 3609+34.77 | -5.94  | 647.85                       | 647.95   |
| N            | 3609+44.81 | -5.91  | 647.89                       | 647.97   |
| O            | 3609+54.85 | -5.88  | 647.93                       | 647.98   |
| W. Brg.      | 3609+67.91 | -5.82  | 647.98                       | 647.98   |
| Pier 2       | 3609+68.91 | -5.81  | 647.98                       | 647.98   |
| E. Brg.      | 3609+69.92 | -5.80  | 647.98                       | 647.98   |
| P            | 3609+78.96 | -5.75  | 648.02                       | 648.02   |
| Q            | 3609+89.00 | -5.68  | 648.05                       | 648.05   |
| R            | 3609+99.04 | -5.61  | 648.08                       | 648.08   |
| S            | 3610+09.09 | -5.52  | 648.12                       | 648.12   |
| E. Abut.     | 3610+16.12 | -5.46  | 648.14                       | 648.14   |
| Bk. E. Abut. | 3610+17.62 | -5.45  | 648.14                       | 648.14   |

|          |     |
|----------|-----|
| DESIGNED | IM  |
| CHECKED  | PDF |
| DRAWN    | IM  |
| CHECKED  | PDF |

TOP OF SLAB ELEVATIONS - I  
STRUCTURE NO. 006-0175 (WB)

|                     |           |                    |                  |              |           |
|---------------------|-----------|--------------------|------------------|--------------|-----------|
| SHEET NO. 7         | F.A. RTE. | SECTION            | COUNTY           | TOTAL SHEETS | SHEET NO. |
|                     | 80        | *                  | BUREAU           | 344          | 205       |
| 39 SHEETS           |           | CONTRACT NO. 66908 |                  |              |           |
| FED. ROAD DIST. NO. |           | ILLINOIS           | FED. AID PROJECT |              |           |

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DEPARTMENT OF TRANSPORTATION

**BEAM B4**

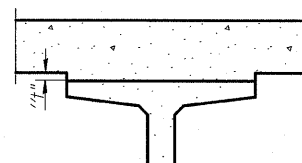
| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. W. Abut. | 3607+95.74 | 1.85   | 647.28                       | 647.28   |
| W. Abut.     | 3607+97.24 | 1.83   | 647.29                       | 647.29   |
| A            | 3608+07.28 | 1.74   | 647.34                       | 647.34   |
| B            | 3608+17.31 | 1.66   | 647.40                       | 647.40   |
| C            | 3608+27.35 | 1.59   | 647.45                       | 647.45   |
| D            | 3608+37.39 | 1.52   | 647.51                       | 647.51   |
| W. Brg.      | 3608+43.41 | 1.49   | 647.54                       | 647.54   |
| W. Pier 1    | 3608+44.41 | 1.48   | 647.54                       | 647.54   |
| E. Brg.      | 3608+45.42 | 1.48   | 647.55                       | 647.55   |
| E            | 3608+54.45 | 1.43   | 647.59                       | 647.63   |
| F            | 3608+64.49 | 1.39   | 647.64                       | 647.71   |
| G            | 3608+74.52 | 1.36   | 647.69                       | 647.79   |
| H            | 3608+84.56 | 1.33   | 647.74                       | 647.86   |
| I            | 3608+94.60 | 1.32   | 647.78                       | 647.91   |
| J            | 3609+04.64 | 1.31   | 647.83                       | 647.97   |
| K            | 3609+14.67 | 1.32   | 647.87                       | 648.00   |
| L            | 3609+24.71 | 1.33   | 647.92                       | 648.04   |
| M            | 3609+34.75 | 1.35   | 647.96                       | 648.06   |
| N            | 3609+44.79 | 1.38   | 648.00                       | 648.08   |
| O            | 3609+54.82 | 1.41   | 648.04                       | 648.09   |
| W. Brg.      | 3609+67.87 | 1.48   | 648.09                       | 648.09   |
| W. Pier 2    | 3609+68.87 | 1.48   | 648.09                       | 648.09   |
| E. Brg.      | 3609+69.88 | 1.49   | 648.09                       | 648.09   |
| P            | 3609+78.91 | 1.54   | 648.13                       | 648.13   |
| Q            | 3609+88.95 | 1.61   | 648.16                       | 648.16   |
| R            | 3609+98.98 | 1.68   | 648.19                       | 648.19   |
| S            | 3610+09.02 | 1.77   | 648.23                       | 648.23   |
| E. Abut.     | 3610+16.05 | 1.83   | 648.25                       | 648.25   |
| Bk. E. Abut. | 3610+17.55 | 1.85   | 648.25                       | 648.25   |

**BEAM B5**

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. W. Abut. | 3607+95.81 | 9.14   | 647.39                       | 647.39   |
| W. Abut.     | 3607+97.31 | 9.12   | 647.40                       | 647.40   |
| A            | 3608+07.34 | 9.03   | 647.45                       | 647.45   |
| B            | 3608+17.37 | 8.95   | 647.51                       | 647.51   |
| C            | 3608+27.40 | 8.88   | 647.56                       | 647.56   |
| D            | 3608+37.43 | 8.81   | 647.62                       | 647.62   |
| W. Brg.      | 3608+43.45 | 8.78   | 647.65                       | 647.65   |
| W. Pier 1    | 3608+44.45 | 8.77   | 647.65                       | 647.65   |
| E. Brg.      | 3608+45.46 | 8.77   | 647.66                       | 647.66   |
| E            | 3608+54.48 | 8.72   | 647.70                       | 647.74   |
| F            | 3608+64.51 | 8.68   | 647.75                       | 647.82   |
| G            | 3608+74.55 | 8.65   | 647.80                       | 647.90   |
| H            | 3608+84.58 | 8.63   | 647.85                       | 647.97   |
| I            | 3608+94.61 | 8.61   | 647.89                       | 648.02   |
| J            | 3609+04.64 | 8.61   | 647.94                       | 648.08   |
| K            | 3609+14.67 | 8.61   | 647.98                       | 648.11   |
| L            | 3609+24.70 | 8.62   | 648.03                       | 648.15   |
| M            | 3609+34.73 | 8.64   | 648.07                       | 648.17   |
| N            | 3609+44.76 | 8.67   | 648.11                       | 648.19   |
| O            | 3609+54.79 | 8.71   | 648.15                       | 648.20   |
| W. Brg.      | 3609+67.83 | 8.77   | 648.19                       | 648.19   |
| W. Pier 2    | 3609+68.83 | 8.77   | 648.20                       | 648.20   |
| E. Brg.      | 3609+69.84 | 8.78   | 648.20                       | 648.20   |
| P            | 3609+78.87 | 8.83   | 648.23                       | 648.23   |
| Q            | 3609+88.90 | 8.90   | 648.27                       | 648.27   |
| R            | 3609+98.93 | 8.97   | 648.30                       | 648.30   |
| S            | 3610+08.96 | 9.06   | 648.34                       | 648.34   |
| E. Abut.     | 3610+15.98 | 9.12   | 648.36                       | 648.36   |
| Bk. E. Abut. | 3610+17.48 | 9.14   | 648.36                       | 648.36   |

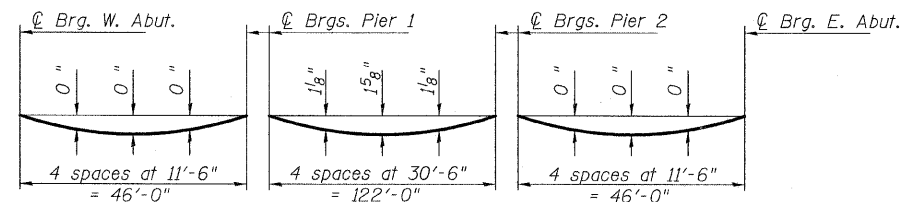
**BEAM B6**

| Location     | Station    | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|--------------|------------|--------|------------------------------|--|
| Bk. W. Abut. | 3607+95.88 | 16.43  | 647.50                       | 647.50   |
| W. Abut.     | 3607+97.38 | 16.41  | 647.51                       | 647.51   |
| A            | 3608+07.40 | 16.32  | 647.56                       | 647.56   |
| B            | 3608+17.43 | 16.24  | 647.62                       | 647.62   |
| C            | 3608+27.45 | 16.17  | 647.67                       | 647.67   |
| D            | 3608+37.48 | 16.10  | 647.72                       | 647.72   |
| W. Brg.      | 3608+43.49 | 16.07  | 647.76                       | 647.76   |
| W. Pier 1    | 3608+44.49 | 16.06  | 647.76                       | 647.76   |
| E. Brg.      | 3608+45.50 | 16.06  | 647.77                       | 647.77   |
| E            | 3608+54.52 | 16.01  | 647.81                       | 647.85   |
| F            | 3608+64.54 | 15.97  | 647.86                       | 647.93   |
| G            | 3608+74.57 | 15.94  | 647.91                       | 648.01   |
| H            | 3608+84.59 | 15.92  | 647.96                       | 648.08   |
| I            | 3608+94.61 | 15.90  | 648.00                       | 648.13   |
| J            | 3609+04.64 | 15.90  | 648.05                       | 648.19   |
| K            | 3609+14.66 | 15.90  | 648.09                       | 648.22   |
| L            | 3609+24.69 | 15.91  | 648.13                       | 648.25   |
| M            | 3609+34.71 | 15.93  | 648.18                       | 648.28   |
| N            | 3609+44.74 | 15.96  | 648.22                       | 648.30   |
| O            | 3609+54.76 | 16.00  | 648.25                       | 648.30   |
| W. Brg.      | 3609+67.79 | 16.06  | 648.30                       | 648.30   |
| W. Pier 2    | 3609+68.80 | 16.06  | 648.31                       | 648.31   |
| E. Brg.      | 3609+69.80 | 16.07  | 648.31                       | 648.31   |
| P            | 3609+78.82 | 16.12  | 648.34                       | 648.34   |
| Q            | 3609+88.84 | 16.19  | 648.38                       | 648.38   |
| R            | 3609+98.87 | 16.27  | 648.41                       | 648.41   |
| S            | 3610+08.89 | 16.35  | 648.44                       | 648.44   |
| E. Abut.     | 3610+15.91 | 16.41  | 648.47                       | 648.47   |
| Bk. E. Abut. | 3610+17.41 | 16.43  | 648.47                       | 648.47   |



To determine "1": After all precast prestressed beams have been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflections" shown below, minus slab thickness, equals the fillet heights "1" above top flanges of beams.

**FILLET HEIGHTS**



**DEAD LOAD DEFLECTION DIAGRAM**

Note: (Includes weight of concrete, excluding beams).

The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown below.

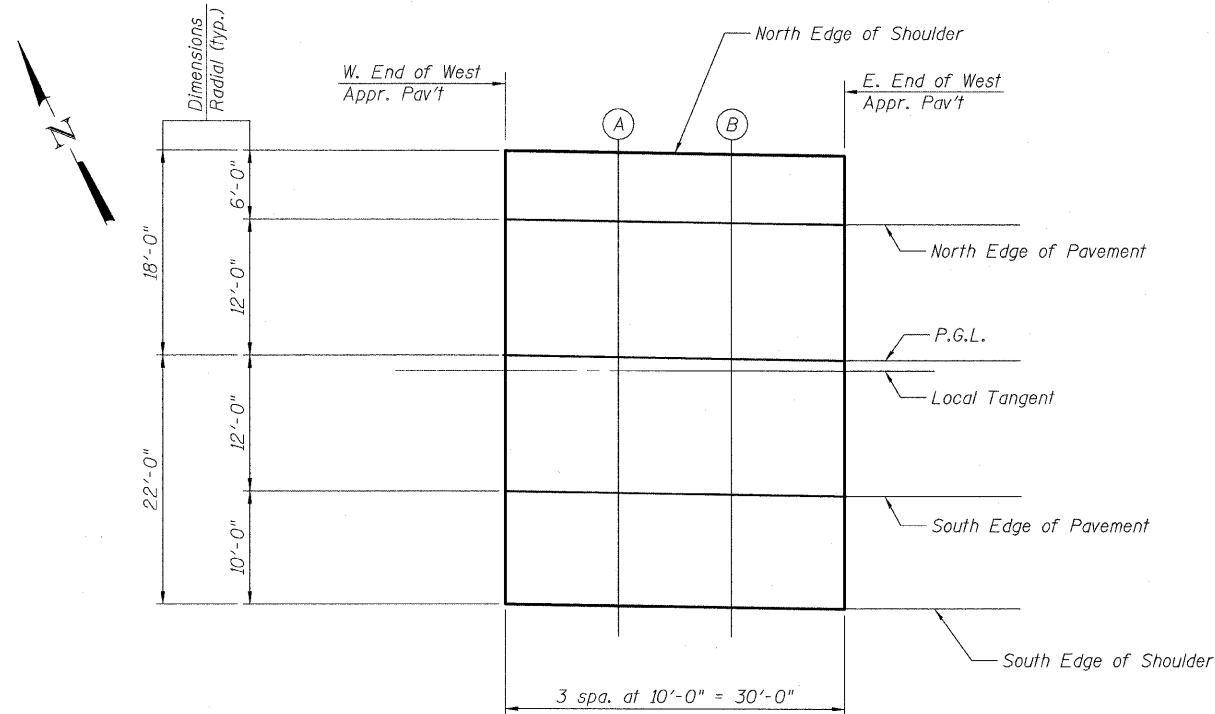
|          |     |
|----------|-----|
| DESIGNED | IM  |
| CHECKED  | PDF |
| DRAWN    | IM  |
| CHECKED  | PDF |

**TOP OF SLAB ELEVATIONS - II**  
**STRUCTURE NO. 006-0175 (WB)**

|             |                     |         |                           |              |           |
|-------------|---------------------|---------|---------------------------|--------------|-----------|
| SHEET NO. 8 | F.A. RTE.           | SECTION | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|             | 80                  | *       | BUREAU                    | 344          | 206       |
| 39 SHEETS   | FED. ROAD DIST. NO. |         | ILLINOIS FED. AID PROJECT |              |           |
|             |                     |         | CONTRACT NO. 66908        |              |           |

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DEPARTMENT OF TRANSPORTATION



**PLAN**  
West Approach

**NORTH EDGE OF SHOULDER**

| Location                | Station    | Offset  | Theoretical Grade Elevations |
|-------------------------|------------|---------|------------------------------|
| W. End West Appr. Pav't | 3607+66.45 | -18.00' | 647.07                       |
| A                       | 3607+76.43 | -18.00' | 647.14                       |
| B                       | 3607+86.41 | -18.00' | 647.20                       |
| E. End West Appr. Pav't | 3607+96.39 | -18.00' | 647.27                       |

**NORTH EDGE OF PAVEMENT**

| Location                | Station    | Offset  | Theoretical Grade Elevations |
|-------------------------|------------|---------|------------------------------|
| W. End West Appr. Pav't | 3607+66.53 | -12.00' | 647.16                       |
| A                       | 3607+76.50 | -12.00' | 647.23                       |
| B                       | 3607+86.47 | -12.00' | 647.29                       |
| E. End West Appr. Pav't | 3607+96.45 | -12.00' | 647.36                       |

**P.G.L. EASTBOUND**

| Location                | Station    | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End West Appr. Pav't | 3607+66.67 | 0.00'  | 647.35                       |
| A                       | 3607+76.63 | 0.00'  | 647.41                       |
| B                       | 3607+86.60 | 0.00'  | 647.48                       |
| E. End West Appr. Pav't | 3607+96.56 | 0.00'  | 647.54                       |

**SOUTH EDGE OF PAVEMENT**

| Location                | Station    | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End West Appr. Pav't | 3607+66.82 | 12.00' | 647.53                       |
| A                       | 3607+76.77 | 12.00' | 647.59                       |
| B                       | 3607+86.72 | 12.00' | 647.66                       |
| E. End West Appr. Pav't | 3607+96.67 | 12.00' | 647.72                       |

**SOUTH EDGE OF SHOULDER**

| Location                | Station    | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End West Appr. Pav't | 3607+66.94 | 22.00' | 647.68                       |
| A                       | 3607+76.88 | 22.00' | 647.74                       |
| B                       | 3607+86.83 | 22.00' | 647.81                       |
| E. End West Appr. Pav't | 3607+96.77 | 22.00' | 647.87                       |

**NORTH EDGE OF SHOULDER**

| Location                | Station    | Offset  | Theoretical Grade Elevations |
|-------------------------|------------|---------|------------------------------|
| W. End East Appr. Pav't | 3610+16.89 | -18.00' | 648.31                       |
| A                       | 3610+26.87 | -18.00' | 648.34                       |
| B                       | 3610+36.85 | -18.00' | 648.37                       |
| E. End East Appr. Pav't | 3610+46.83 | -18.00' | 648.40                       |

**NORTH EDGE OF PAVEMENT**

| Location                | Station    | Offset  | Theoretical Grade Elevations |
|-------------------------|------------|---------|------------------------------|
| W. End East Appr. Pav't | 3610+16.84 | -12.00' | 648.40                       |
| A                       | 3610+26.81 | -12.00' | 648.43                       |
| B                       | 3610+36.85 | -12.00' | 648.46                       |
| E. End East Appr. Pav't | 3610+46.83 | -12.00' | 648.49                       |

**P.G.L. EASTBOUND**

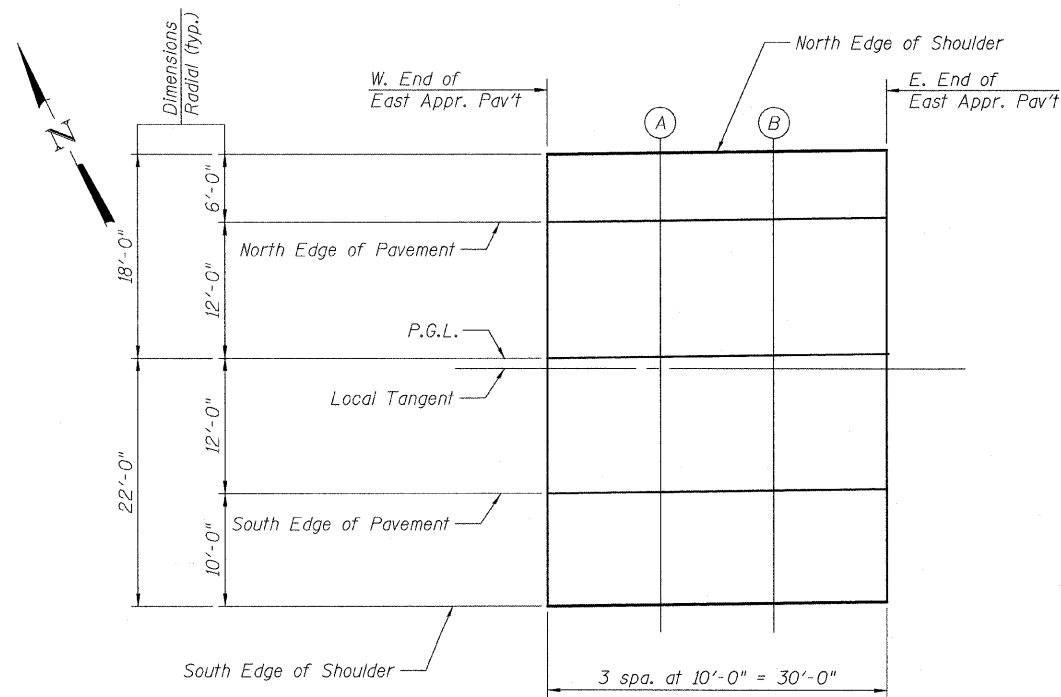
| Location                | Station    | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End East Appr. Pav't | 3610+16.71 | 0.00'  | 648.58                       |
| A                       | 3610+26.68 | 0.00'  | 648.61                       |
| B                       | 3610+36.65 | 0.00'  | 648.64                       |
| E. End East Appr. Pav't | 3610+46.61 | 0.00'  | 648.67                       |

**SOUTH EDGE OF PAVEMENT**

| Location                | Station    | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End East Appr. Pav't | 3610+16.61 | 12.00' | 648.76                       |
| A                       | 3610+26.56 | 12.00' | 648.79                       |
| B                       | 3610+36.51 | 12.00' | 648.82                       |
| E. End East Appr. Pav't | 3610+46.46 | 12.00' | 648.85                       |

**SOUTH EDGE OF SHOULDER**

| Location                | Station    | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End East Appr. Pav't | 3610+16.51 | 22.00' | 648.91                       |
| A                       | 3610+26.46 | 22.00' | 648.94                       |
| B                       | 3610+36.40 | 22.00' | 648.97                       |
| E. End East Appr. Pav't | 3610+46.34 | 22.00' | 649.00                       |



**PLAN**  
East Approach

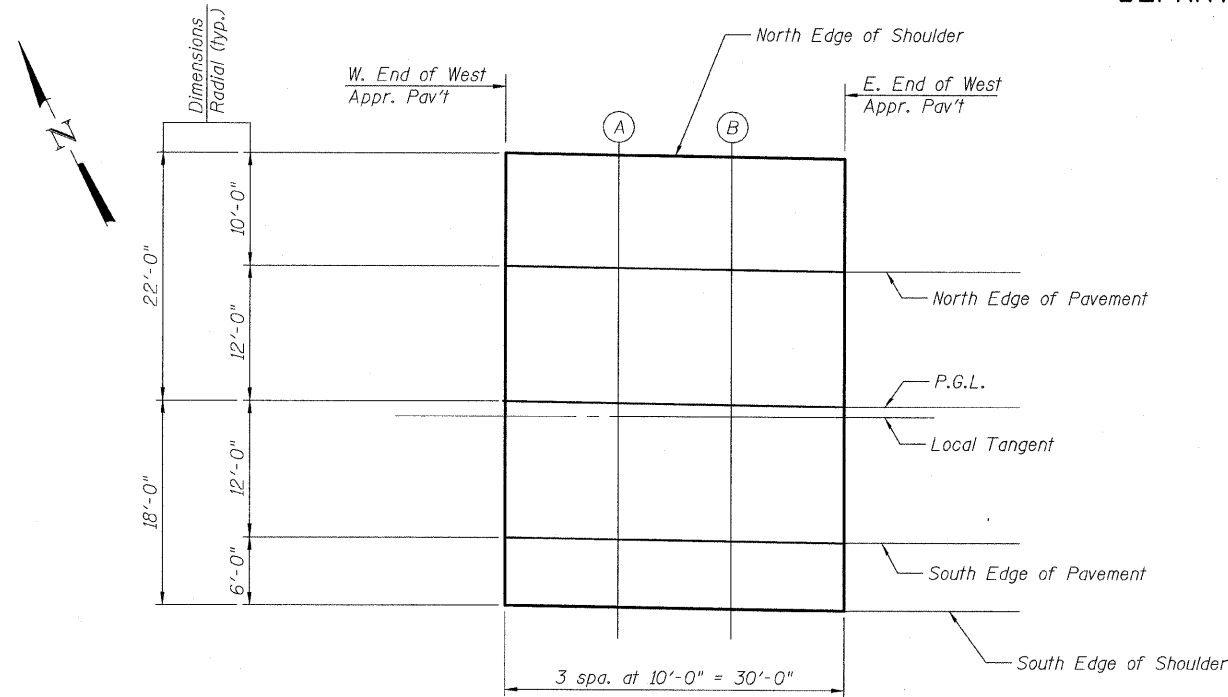
|          |      |
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| DESIGNED | I.M. |
| CHECKED  | PDF  |
| DRAWN    | I.M. |
| CHECKED  | PDF  |

**TOP OF APPROACH SLAB  
ELEVATIONS  
STRUCTURE NO. 006-0174 (EB)**

|                     |                    |                           |        |              |           |
|---------------------|--------------------|---------------------------|--------|--------------|-----------|
| SHEET NO. 9         | F.A. RTE.          | SECTION                   | COUNTY | TOTAL SHEETS | SHEET NO. |
|                     | 80                 | *                         | BUREAU | 344          | 207       |
| 39 SHEETS           | CONTRACT NO. 66908 |                           |        |              |           |
| FED. ROAD DIST. NO. |                    | ILLINOIS FED. AID PROJECT |        |              |           |

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**PLAN**  
West Approach

**NORTH EDGE OF SHOULDER**

| Location                 | Station    | Offset  | Theoretical Grade Elevations |
|--------------------------|------------|---------|------------------------------|
| W. End West Appr. Pav't  | 3607+65.33 | -22.00' | 646.73                       |
| A                        | 3607+75.39 | -22.00' | 646.79                       |
| B                        | 3607+85.44 | -22.00' | 646.85                       |
| E. End West. Appr. Pav't | 3607+95.50 | -22.00' | 646.91                       |

**NORTH EDGE OF PAVEMENT**

| Location                 | Station    | Offset  | Theoretical Grade Elevations |
|--------------------------|------------|---------|------------------------------|
| W. End West Appr. Pav't  | 3607+65.45 | -12.00' | 646.88                       |
| A                        | 3607+75.50 | -12.00' | 646.94                       |
| B                        | 3607+85.55 | -12.00' | 647.00                       |
| E. End West. Appr. Pav't | 3607+95.60 | -12.00' | 647.06                       |

**P.G.L. WESTBOUND**

| Location                 | Station    | Offset | Theoretical Grade Elevations |
|--------------------------|------------|--------|------------------------------|
| W. End West Appr. Pav't  | 3607+65.56 | 0.00'  | 647.06                       |
| A                        | 3607+75.64 | 0.00'  | 647.12                       |
| B                        | 3607+85.68 | 0.00'  | 647.19                       |
| E. End West. Appr. Pav't | 3607+95.71 | 0.00'  | 647.25                       |

**SOUTH EDGE OF PAVEMENT**

| Location                 | Station    | Offset | Theoretical Grade Elevations |
|--------------------------|------------|--------|------------------------------|
| W. End West Appr. Pav't  | 3607+65.75 | 12.00' | 647.24                       |
| A                        | 3607+75.77 | 12.00' | 647.31                       |
| B                        | 3607+85.80 | 12.00' | 647.37                       |
| E. End West. Appr. Pav't | 3607+95.83 | 12.00' | 647.43                       |

**SOUTH EDGE OF SHOULDER**

| Location                 | Station    | Offset | Theoretical Grade Elevations |
|--------------------------|------------|--------|------------------------------|
| W. End West Appr. Pav't  | 3607+65.82 | 18.00' | 647.33                       |
| A                        | 3607+75.84 | 18.00' | 647.40                       |
| B                        | 3607+85.87 | 18.00' | 647.46                       |
| E. End West. Appr. Pav't | 3607+95.89 | 18.00' | 647.52                       |

**NORTH EDGE OF SHOULDER**

| Location                | Station    | Offset  | Theoretical Grade Elevations |
|-------------------------|------------|---------|------------------------------|
| W. End East Appr. Pav't | 3610+17.78 | -22.00' | 647.89                       |
| A                       | 3610+27.84 | -22.00' | 647.92                       |
| B                       | 3610+37.90 | -22.00' | 647.95                       |
| E. End East Appr. Pav't | 3610+47.95 | -22.00' | 647.97                       |

**NORTH EDGE OF PAVEMENT**

| Location                | Station    | Offset  | Theoretical Grade Elevations |
|-------------------------|------------|---------|------------------------------|
| W. End East Appr. Pav't | 3610+17.68 | -12.00' | 648.04                       |
| A                       | 3610+27.73 | -12.00' | 648.07                       |
| B                       | 3610+37.78 | -12.00' | 648.10                       |
| E. End East Appr. Pav't | 3610+47.83 | -12.00' | 648.12                       |

**P.G.L. WESTBOUND**

| Location                | Station    | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End East Appr. Pav't | 3610+17.56 | 0.00'  | 648.22                       |
| A                       | 3610+27.61 | 0.00'  | 648.25                       |
| B                       | 3610+37.64 | 0.00'  | 648.28                       |
| E. End East Appr. Pav't | 3610+47.68 | 0.00'  | 648.30                       |

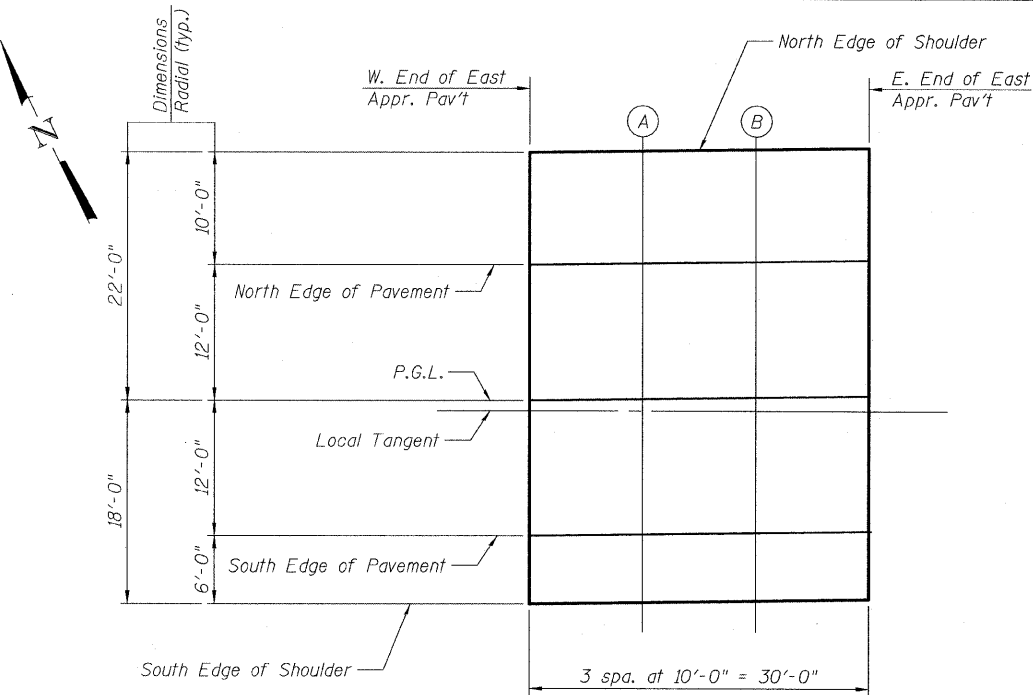
**SOUTH EDGE OF PAVEMENT**

| Location                | Station    | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End East Appr. Pav't | 3610+17.45 | 12.00' | 648.40                       |
| A                       | 3610+27.48 | 12.00' | 648.43                       |
| B                       | 3610+37.51 | 12.00' | 648.46                       |
| E. End East Appr. Pav't | 3610+47.54 | 12.00' | 648.48                       |

**SOUTH EDGE OF SHOULDER**

| Location                | Station    | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End East Appr. Pav't | 3610+17.39 | 18.00' | 648.49                       |
| A                       | 3610+27.42 | 18.00' | 648.52                       |
| B                       | 3610+37.44 | 18.00' | 648.55                       |
| E. End East Appr. Pav't | 3610+47.46 | 18.00' | 648.57                       |

**TOP OF APPROACH SLAB  
ELEVATIONS  
STRUCTURE NO. 006-0175 (WB)**



**PLAN**  
East Approach

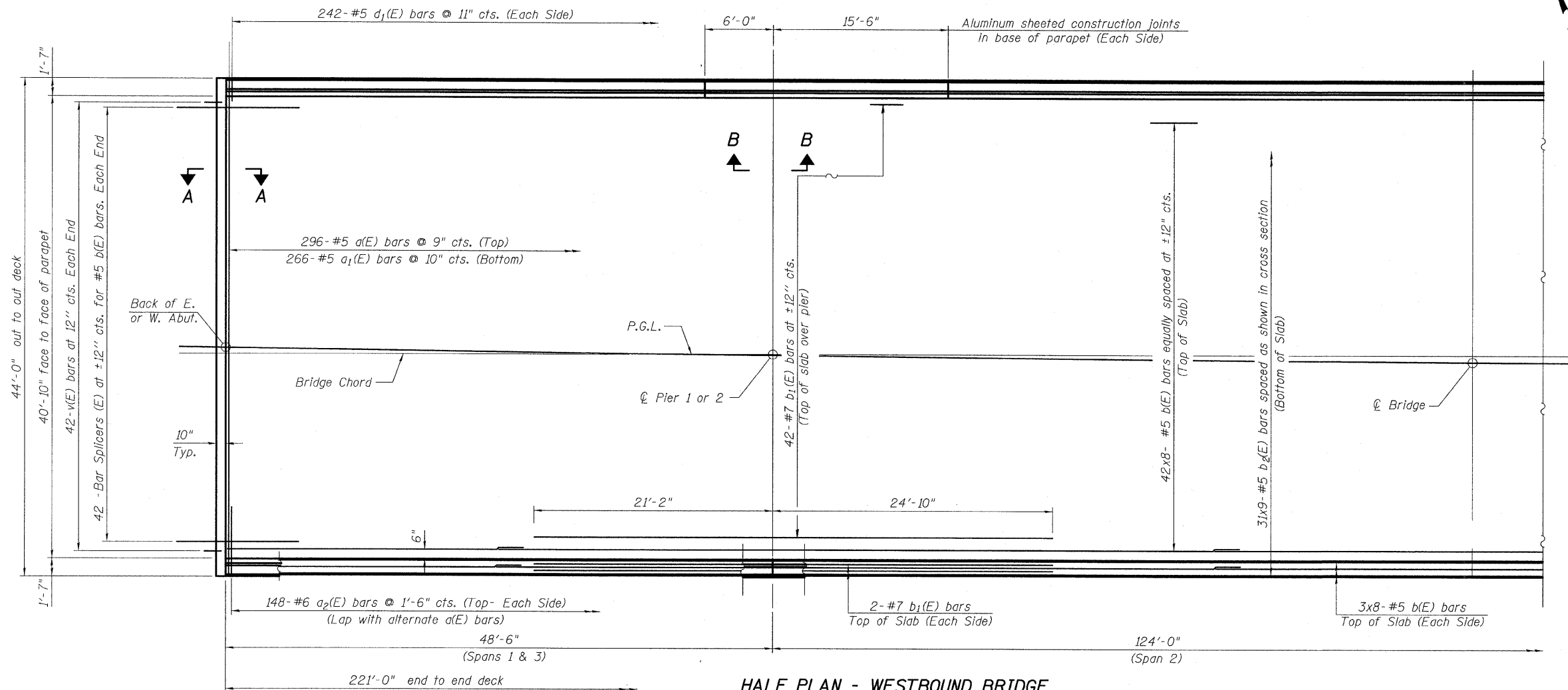
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|----------|------|
| DESIGNED | I.M. |
| CHECKED  | PDF  |
| DRAWN    | I.M. |
| CHECKED  | PDF  |

|                           |                    |                           |                  |                     |                  |
|---------------------------|--------------------|---------------------------|------------------|---------------------|------------------|
| SHEET NO. 10<br>39 SHEETS | F.A. RTE.<br>80    | SECTION<br>*              | COUNTY<br>BUREAU | TOTAL SHEETS<br>394 | SHEET NO.<br>208 |
|                           | CONTRACT NO. 66908 |                           |                  |                     |                  |
| FED. ROAD DIST. NO.       |                    | ILLINOIS FED. AID PROJECT |                  |                     |                  |

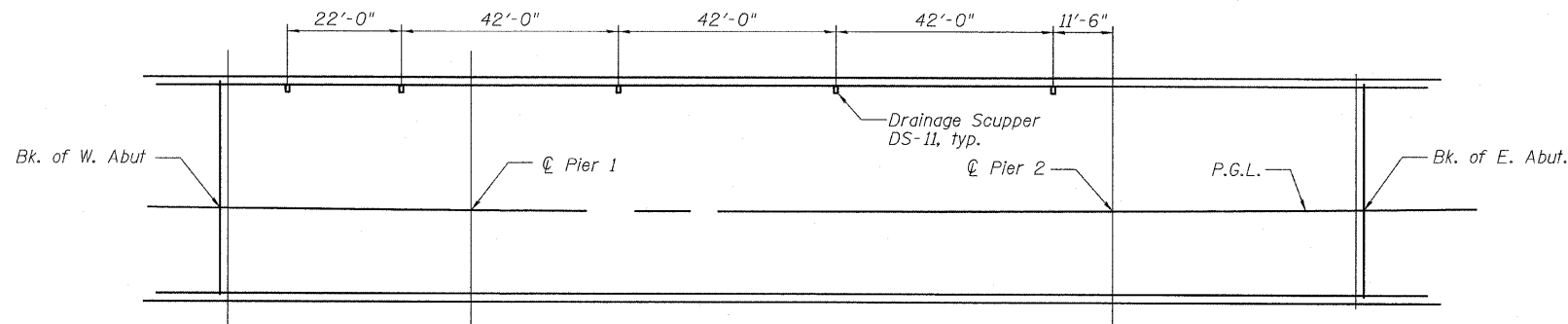
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DEPARTMENT OF TRANSPORTATION



**HALF PLAN - WESTBOUND BRIDGE**  
(Eastbound Bridge by rotation thru 180°)



**DRAINAGE SCUPPER LOCATION PLAN**

(Eastbound Bridge Only)

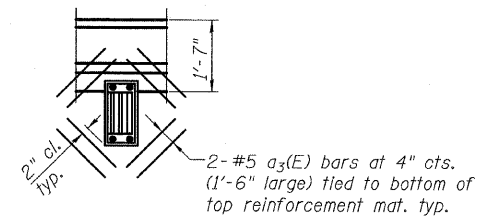
Note:  
Scuppers shall be placed 4" into concrete parapet.

**MINIMUM BAR LAPS**

#5 bar = 2'-2"  
#6 bar = 2'-7"

**NOTES:**

1. See Sheet 14 of 39 for Superstructure details and Bill of Materials.
2. For Sections A-A and B-B and diaphragm details see Sheet 13 of 39.
3. Bars indicated thus: 20x3-#5 etc. indicates 20 lines of bars with 3 lengths per line.
4. See Sheet 12 of 39 for Parapet reinforcement.



**PLAN**

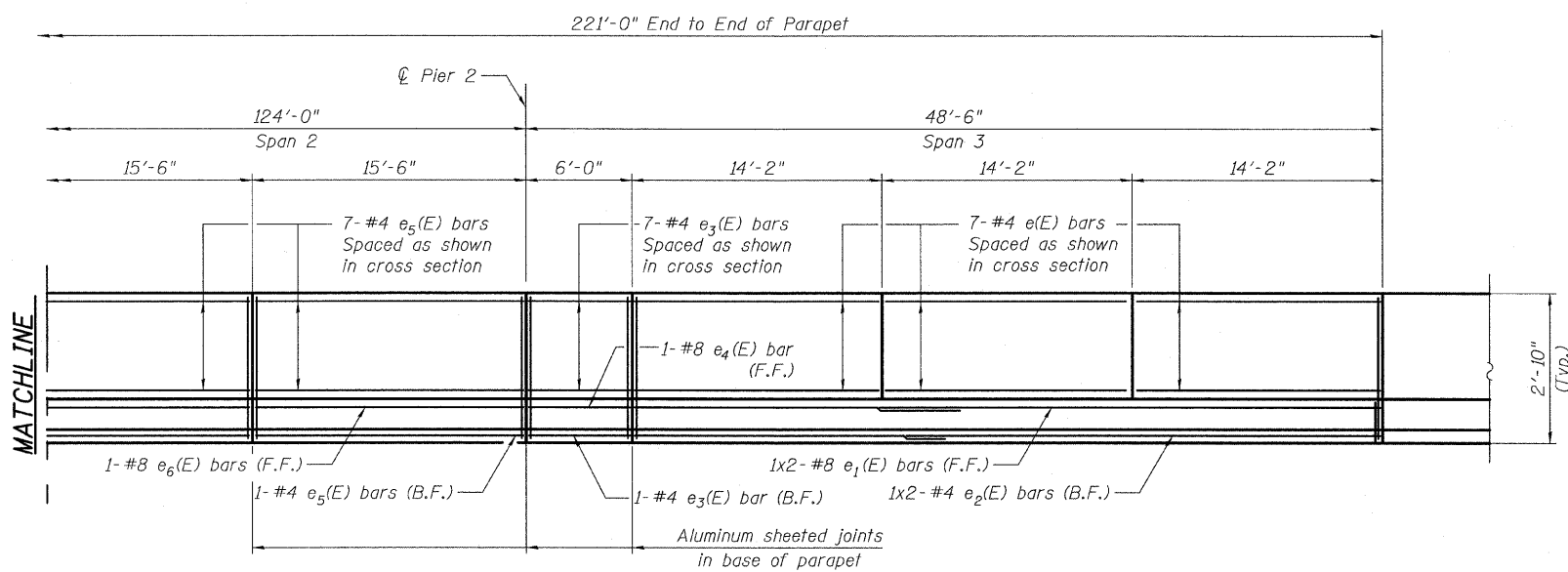
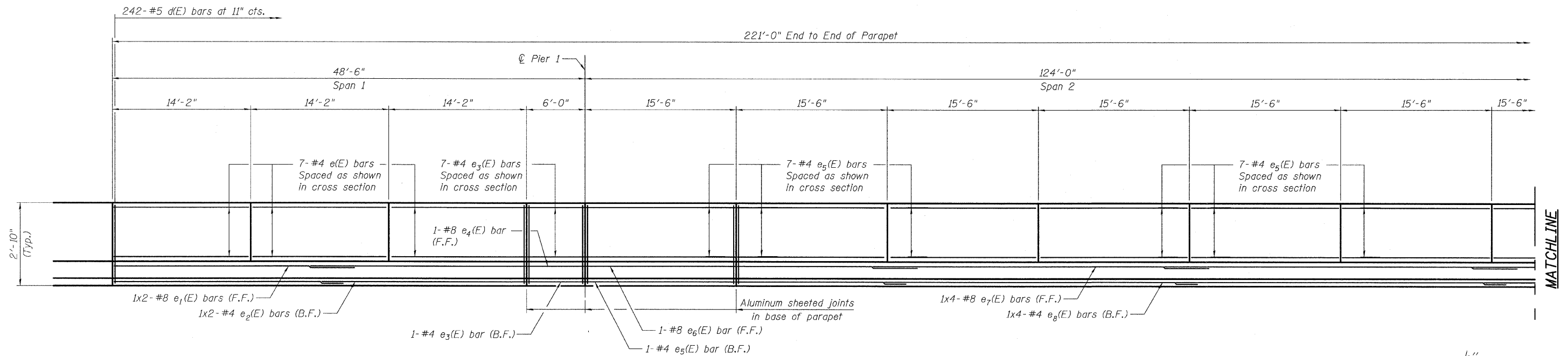
Note:  
Cut longitudinal reinforcement to clear drainage scuppers.

|          |      |
|----------|------|
| DESIGNED | I.M. |
| CHECKED  | PDF  |
| DRAWN    | I.M. |
| CHECKED  | PDF  |

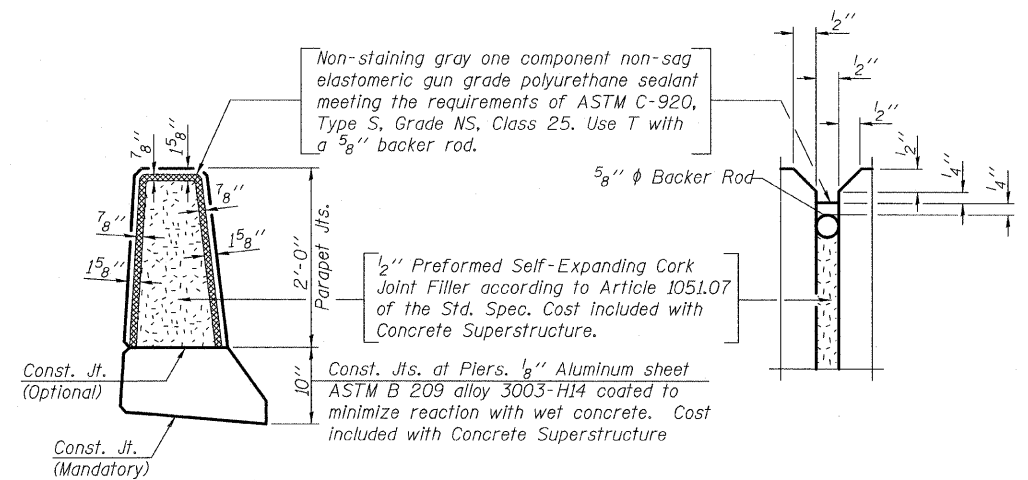
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|---------------------------|--------------------|---------------------------|------------------|---------------------|------------------|
| SHEET NO. 11<br>39 SHEETS | F.A. RTE.<br>80    | SECTION<br>*              | COUNTY<br>BUREAU | TOTAL SHEETS<br>394 | SHEET NO.<br>209 |
|                           | CONTRACT NO. 66908 |                           |                  |                     |                  |
| FED. ROAD DIST. NO.       |                    | ILLINOIS FED. AID PROJECT |                  |                     |                  |

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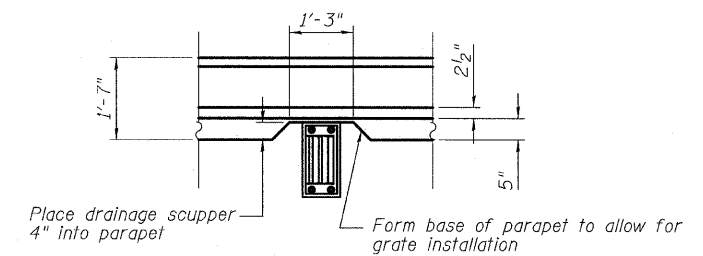
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DEPARTMENT OF TRANSPORTATION



INSIDE ELEVATION OF PARAPET



PARAPET JOINT DETAILS



PLAN AT DRAINAGE SCUPPER  
(EB Bridge only)

|          |      |
|----------|------|
| DESIGNED | I.M. |
| CHECKED  | PDF  |
| DRAWN    | I.M. |
| CHECKED  | PDF  |

MINIMUM BAR LAPS

#4 bar = 1'-4"  
#8 bar = 3'-5"

LEGEND:

F.F. - Denotes Front Face  
B.F. - Denotes Back Face

SUPERSTRUCTURE DETAILS I

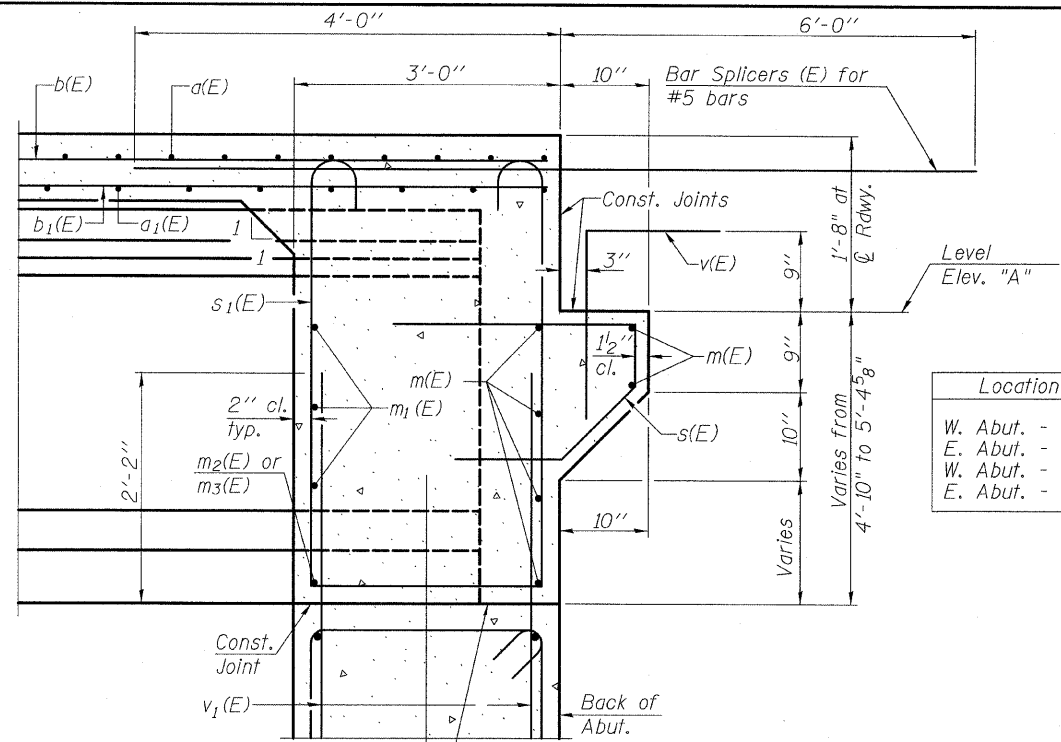
STRUCTURE NO. 006-0174 (EB)

STRUCTURE NO. 006-0175 (WB)

|                     |                    |                           |        |              |           |
|---------------------|--------------------|---------------------------|--------|--------------|-----------|
| SHEET NO. 12        | F.A. RTE.          | SECTION                   | COUNTY | TOTAL SHEETS | SHEET NO. |
|                     | 80                 | *                         | BUREAU | 374          | 210       |
| 39 SHEETS           | CONTRACT NO. 66908 |                           |        |              |           |
| FED. ROAD DIST. NO. |                    | ILLINOIS FED. AID PROJECT |        |              |           |

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DEPARTMENT OF TRANSPORTATION



| Location      | Elev. "A" |
|---------------|-----------|
| W. Abut. - WB | 645.59    |
| E. Abut. - WB | 646.56    |
| W. Abut. - EB | 645.88    |
| E. Abut. - EB | 646.92    |

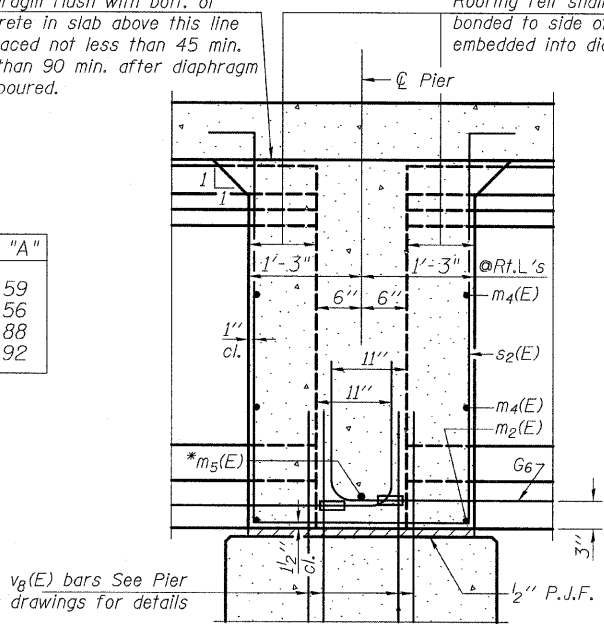
**SECTION A-A**

Beam ends shall be set on an initial 1/2" min. grout (2:1 sand and portland cement, very dry mix) to provide full bearing. Any excess grout squeezed out from under the beam shall be removed. Cost included with Concrete Structures.

Notes:  
Reinforcement bars in diaphragm are billed with superstructure on sheet 14 of 39.  
Concrete in diaphragm is included with Concrete Superstructure on sheet 14 of 39.  
For details of bars s(E), s1(E) and s2(E) see sheet 14 of 39.  
The s(E), s1(E) and s2(E) bars shall be placed parallel to the beams. Spacing for these bars shall be at right angles to the beams.  
Cost of 90 Lb. roofing felt is included with Concrete Superstructure. The side retainer shall be galvanized after shop fabrication according to AASHTO M 111. Cost of side retainer and anchor bolts shall be included with Concrete Structures.  
Anchor bolt assemblies shall be galvanized according to Article 1006.09 of the Standard Specifications.  
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.  
Anchor bolts for side retainers may be cast in place or installed in holes drilled before or after members are in place.  
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

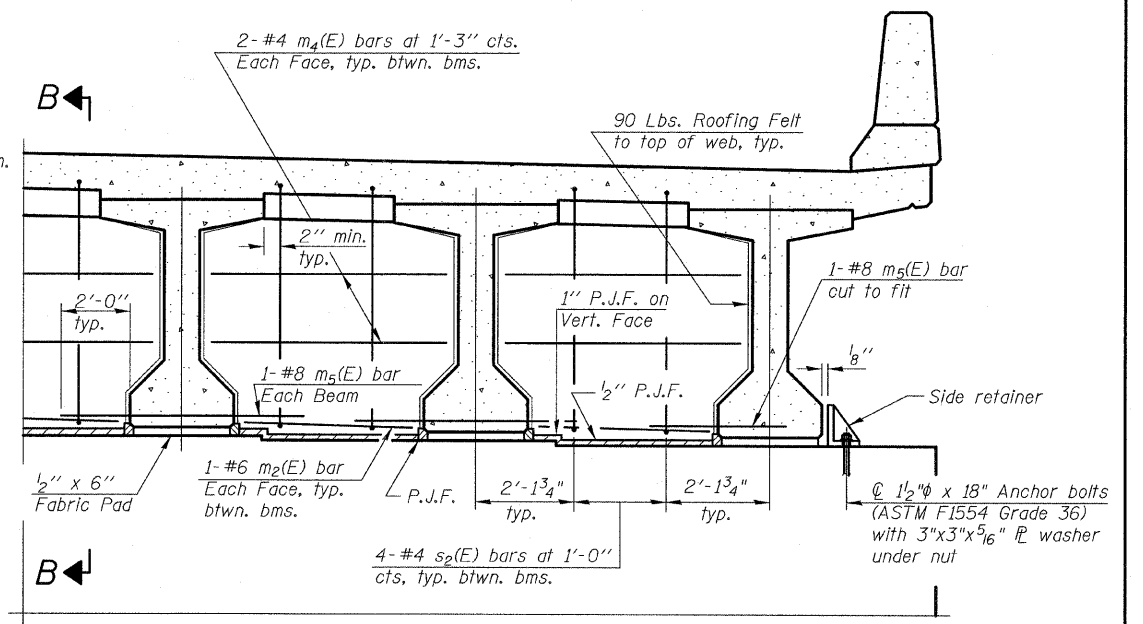
Pour diaphragm flush with bott. of slab. Concrete in slab above this line shall be placed not less than 45 min. nor more than 90 min. after diaphragm has been poured.

Roofing felt shall be bonded to side of beam embedded into diaphragm.

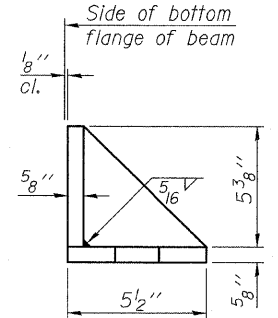


**SECTION B-B**

Dimensions along C of beam, except as shown.  
\* Tightly fasten the #8 bars together with No. 9 wire ties.

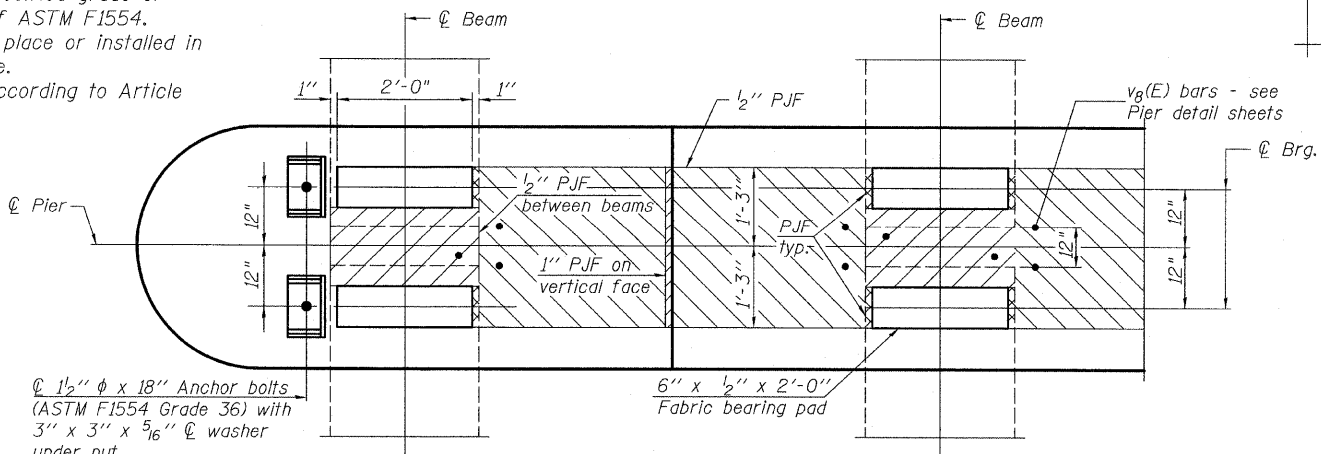
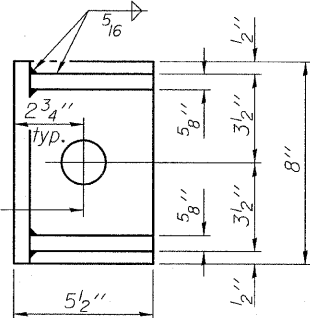


**DIAPHRAGM AT PIER**



**SIDE RETAINER**

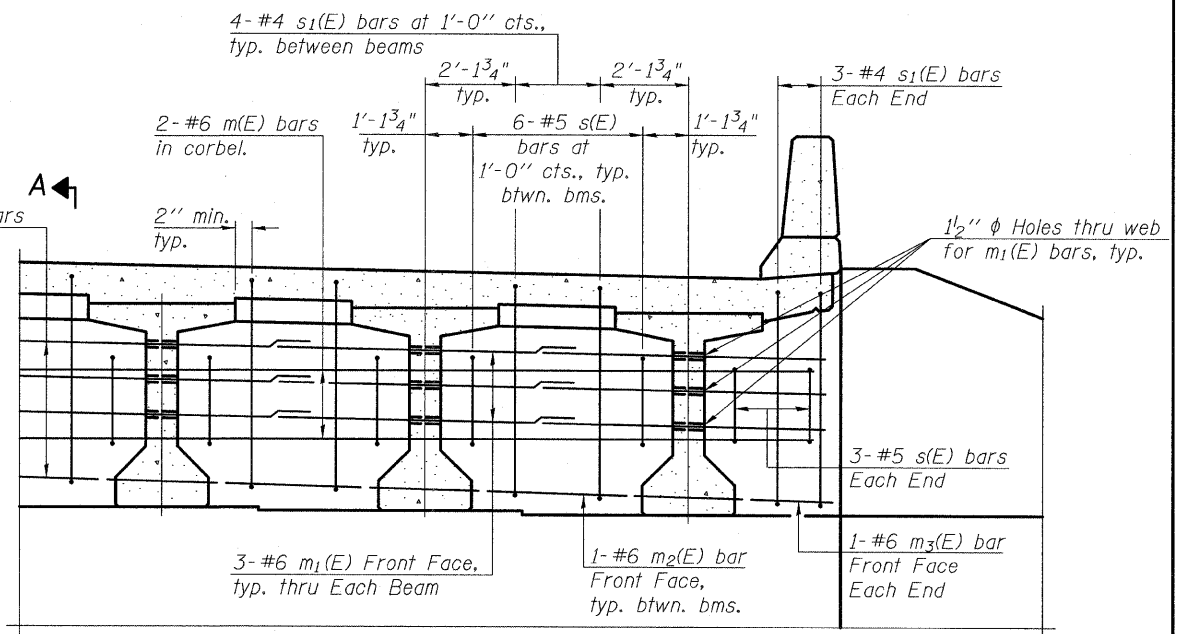
(2 required each side of pier).  
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



**PLAN AT PIER**

(Showing bearing pad and P.J.F. details)

|          |     |
|----------|-----|
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| DRAWN    | IM  |
| CHECKED  | PDF |



**DIAPHRAGM ELEVATION AT ABUTMENT**

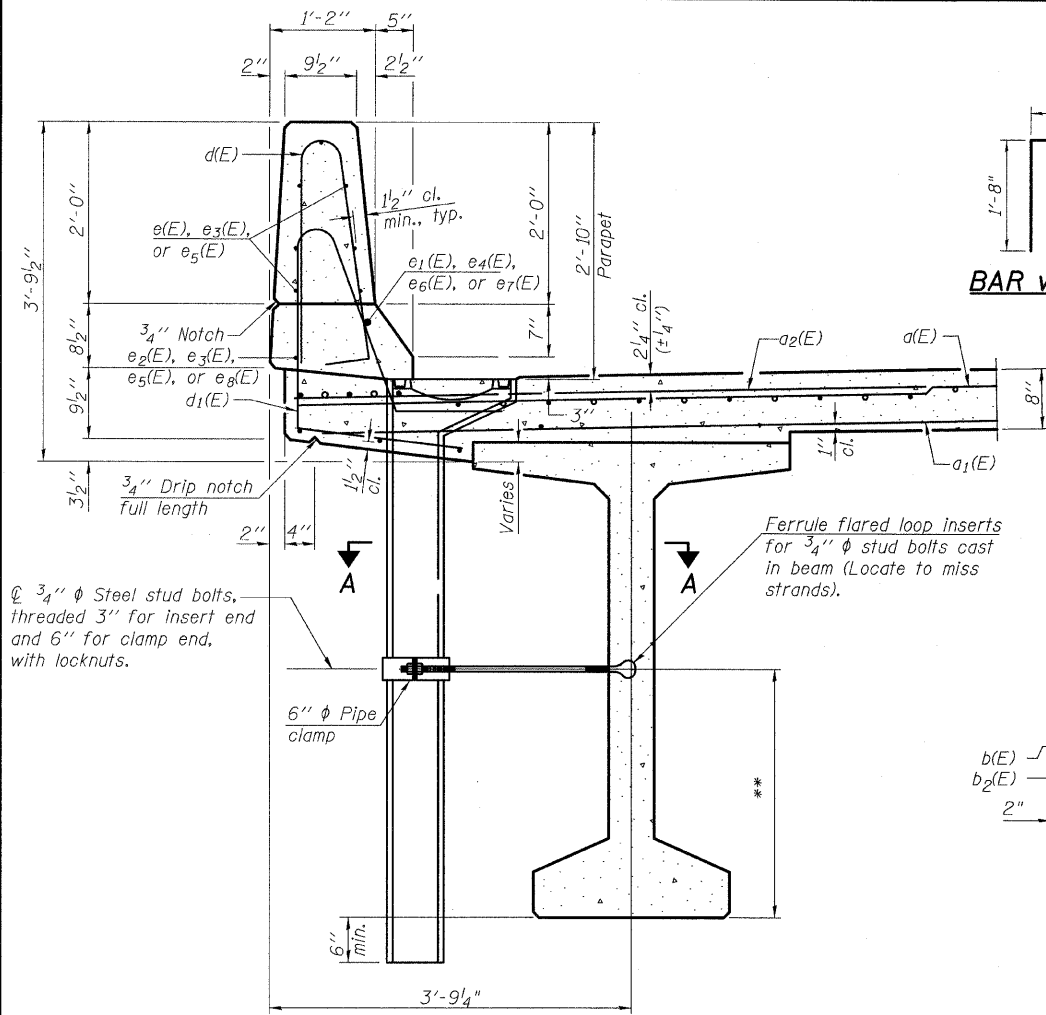
**MIN. BAR LAP**  
#6 bar = 2'-9"

**SUPERSTRUCTURE DETAILS II**  
STRUCTURE NO. 006-0174 (EB)  
STRUCTURE NO. 006-0175 (WB)

|                           |                    |              |                  |                     |                 |
|---------------------------|--------------------|--------------|------------------|---------------------|-----------------|
| SHEET NO. 13<br>39 SHEETS | F.A. RTE.<br>80    | SECTION<br>* | COUNTY<br>BUREAU | TOTAL SHEETS<br>344 | SHEET NO.<br>21 |
|                           | CONTRACT NO. 66908 |              |                  |                     |                 |
| FED. ROAD DIST. NO.       |                    | ILLINOIS     | FED. AID PROJECT |                     |                 |

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DEPARTMENT OF TRANSPORTATION



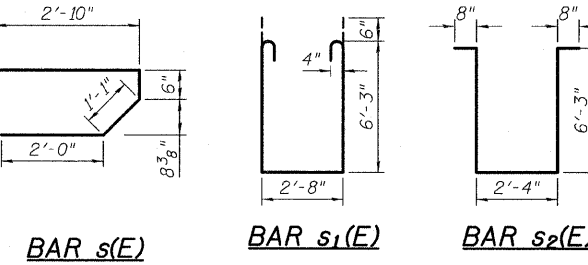
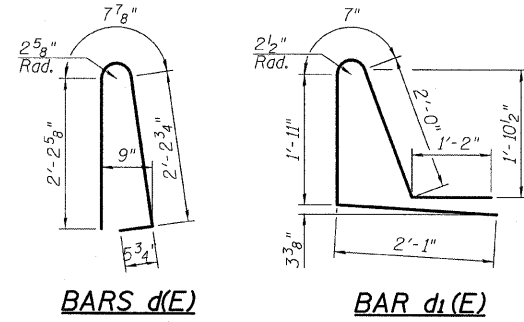
**SECTION THRU PARAPET**

\*\*For insert locations See PPC I-Beam details.

3/4"  $\phi$  Steel stud bolts, threaded 3" for insert end and 6" for clamp end, with locknuts.

Ferrule flared loop inserts for 3/4"  $\phi$  stud bolts cast in beam (Locate to miss strands).

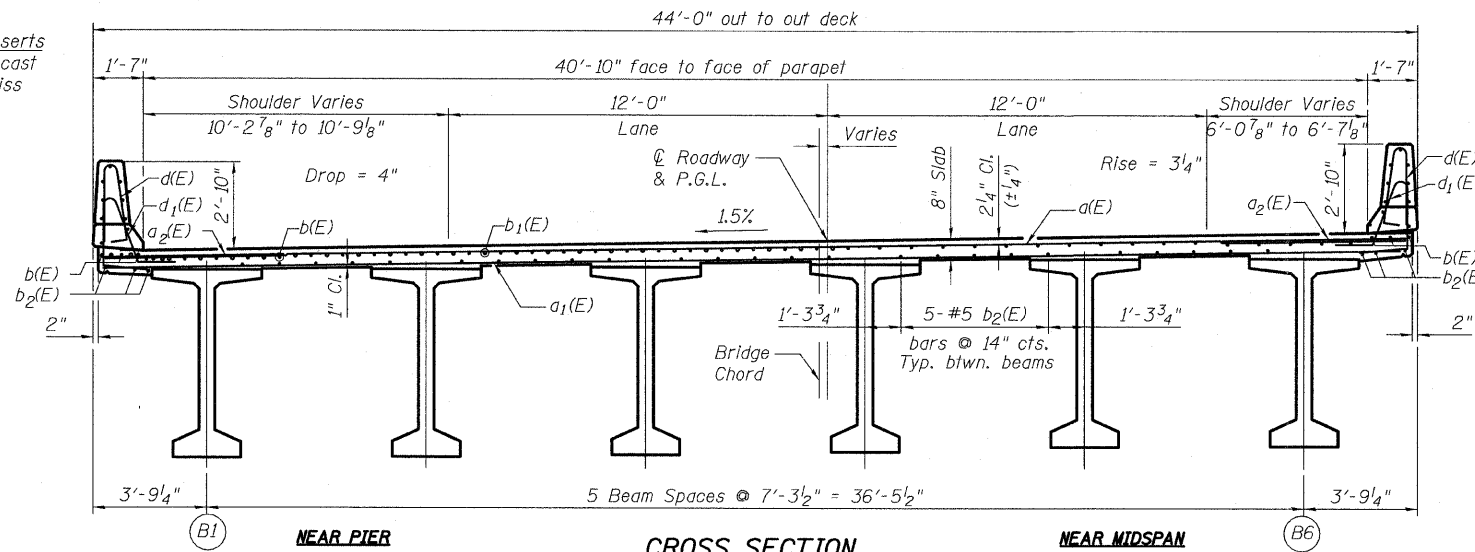
**BAR v(E)**



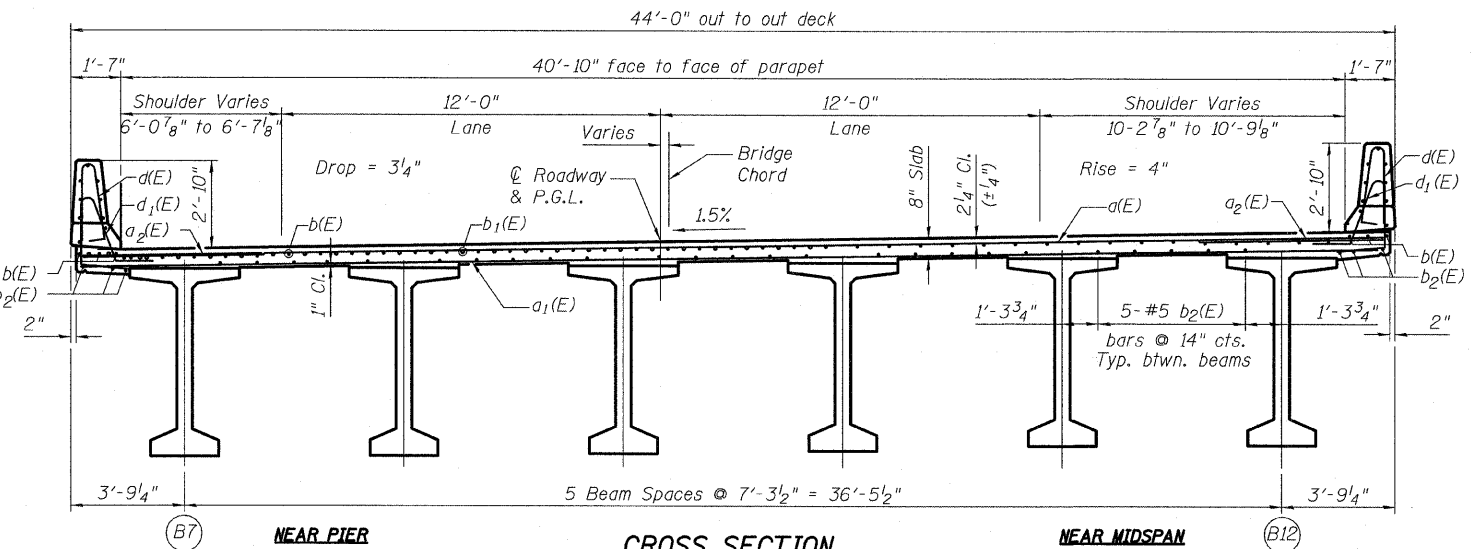
**BILL OF MATERIAL**  
(Each Bridge - 2 Thus)

| Bar                              | No. | Size | Length  | Shape  |
|----------------------------------|-----|------|---------|--------|
| a(E)                             | 296 | #5   | 43'-3"  | —      |
| a1(E)                            | 266 | #5   | 42'-4"  | —      |
| a2(E)                            | 296 | #6   | 6'-0"   | —      |
| a3(E)                            | 40  | #5   | 1'-6"   | —      |
| b(E)                             | 384 | #5   | 29'-7"  | —      |
| b1(E)                            | 92  | #7   | 46'-0"  | —      |
| b2(E)                            | 279 | #5   | 26'-6"  | —      |
| d(E)                             | 484 | #5   | 5'-7"   | U      |
| d1(E)                            | 484 | #5   | 7'-9"   | L      |
| e(E)                             | 84  | #4   | 13'-11" | —      |
| e1(E)                            | 8   | #8   | 22'-10" | —      |
| e2(E)                            | 8   | #4   | 21'-10" | —      |
| e3(E)                            | 32  | #4   | 5'-9"   | —      |
| e4(E)                            | 4   | #8   | 5'-9"   | —      |
| e5(E)                            | 112 | #4   | 15'-3"  | —      |
| e6(E)                            | 4   | #8   | 15'-3"  | —      |
| e7(E)                            | 8   | #8   | 26'-8"  | —      |
| e8(E)                            | 8   | #4   | 24'-7"  | —      |
| m(E)                             | 12  | #6   | 43'-9"  | —      |
| m1(E)                            | 36  | #6   | 10'-1"  | —      |
| m2(E)                            | 30  | #6   | 4'-9"   | —      |
| m3(E)                            | 4   | #6   | 2'-4"   | —      |
| m4(E)                            | 40  | #4   | 6'-5"   | —      |
| m5(E)                            | 12  | #8   | 6'-2"   | —      |
| s(E)                             | 72  | #5   | 6'-5"   | U      |
| s1(E)                            | 52  | #4   | 16'-2"  | U      |
| s2(E)                            | 40  | #4   | 16'-2"  | U      |
| v(E)                             | 84  | #5   | 3'-4"   | L      |
| Reinforcement Bars, Epoxy Coated |     |      | POUND   | 70,050 |
| Concrete Superstructure          |     |      | CU YD   | 410.3  |

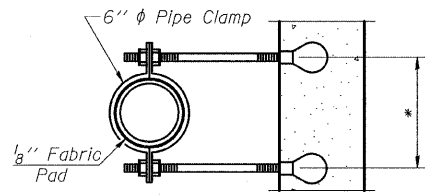
\* Required only at EB Bridge



**CROSS SECTION**  
Westbound - Looking East



**CROSS SECTION**  
Eastbound - Looking East



**SECTION A-A**  
\*Dimension as required by Pipe Clamp

|          |     |
|----------|-----|
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| CHECKED  | PDF |
| DRAWN    | IM  |
| CHECKED  | PDF |

**SUPERSTRUCTURE DETAILS III**  
STRUCTURE NO. 006-0174 (EB)  
STRUCTURE NO. 006-0175 (WB)

|                           |                    |              |                  |                     |                  |
|---------------------------|--------------------|--------------|------------------|---------------------|------------------|
| SHEET NO. 14<br>39 SHEETS | F.A. RTE.<br>80    | SECTION<br>* | COUNTY<br>BUREAU | TOTAL SHEETS<br>344 | SHEET NO.<br>212 |
|                           | CONTRACT NO. 66908 |              |                  |                     |                  |
| FED. ROAD DIST. NO.       |                    | ILLINOIS     | FED. AID PROJECT |                     |                  |

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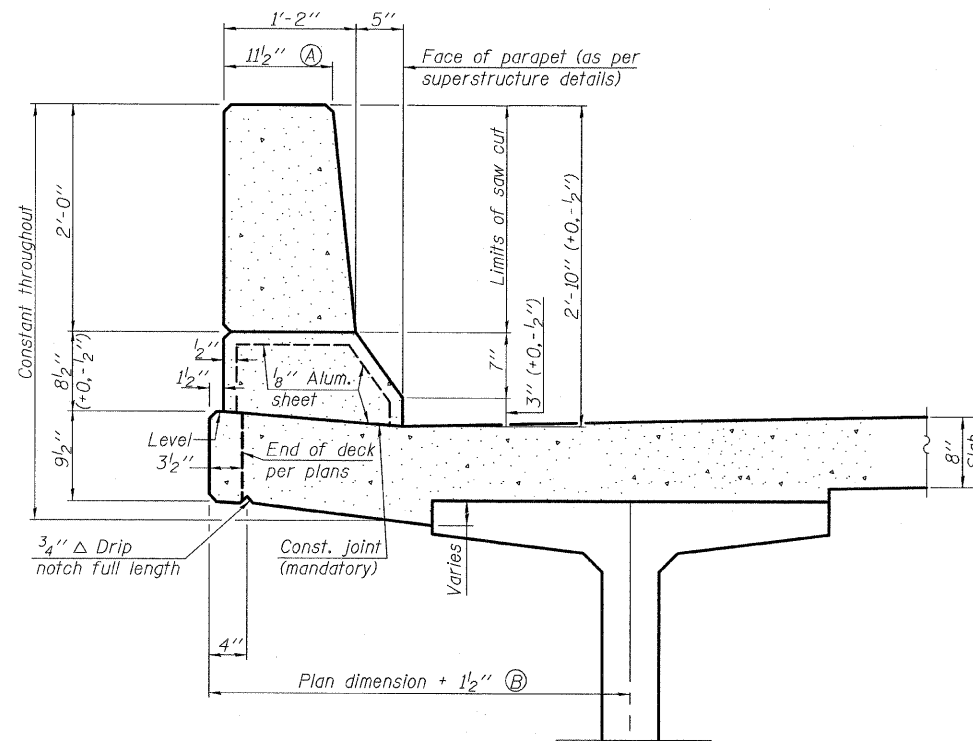
\* 06-I7BR & BR-1,7VB-M, 6BR & 6, 7 RS-1 & 11

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DEPARTMENT OF TRANSPORTATION

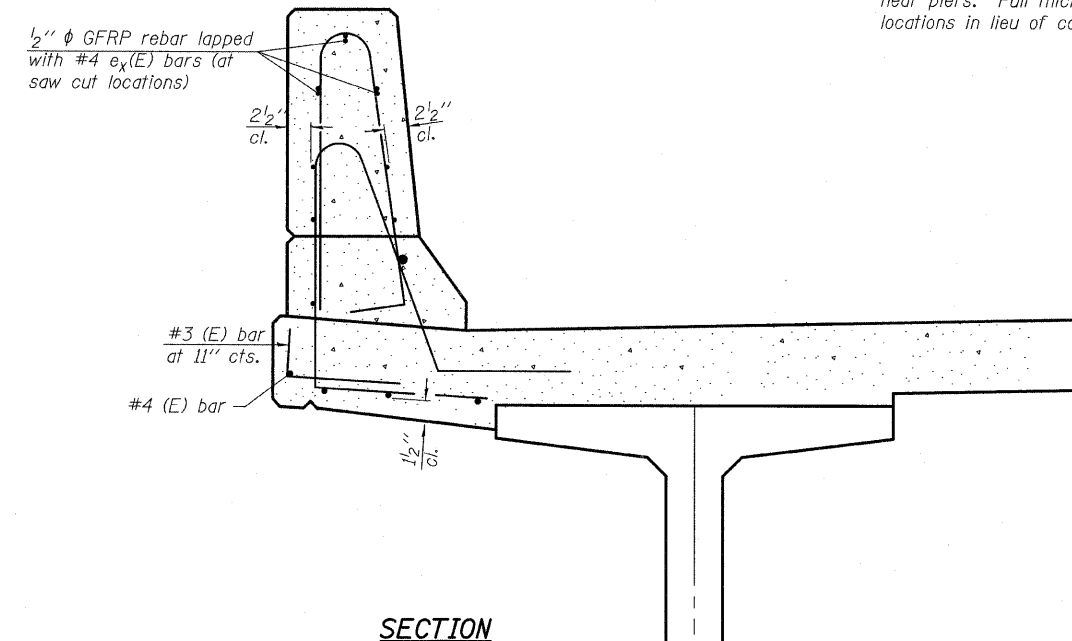
**GENERAL NOTES**

All dimensions shall remain the same as shown on contract plans, except dimensions A and B which are to be revised as shown to provide additional clearance. Additional concrete needed to revise dimension A and B= 0.0165 cu. yds./ft. of parapet.

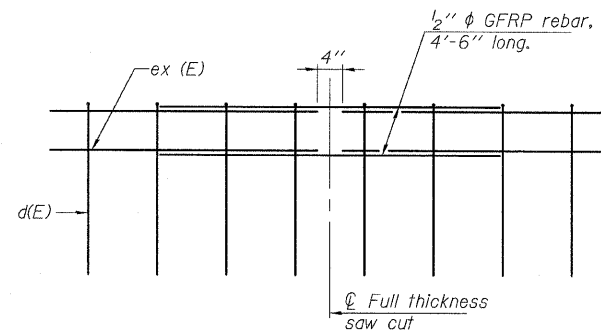
Place aluminum sheet in curb portion at and near piers. Full thickness saw cut at all joint locations in lieu of cork joint filler.



**SECTION**  
(Showing dimensions)

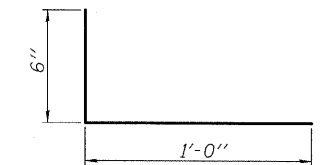


**SECTION**  
(Showing reinforcement clearances for slip forming and additional reinforcement bars)



**GFRP REBAR STIFFENING DETAIL**

(Place as shown in parapet section at each parapet joint location.)



**#3 (E) BAR**

**CONCRETE PARAPET  
SLIPFORMING OPTION**

STRUCTURE NO. 006-0174 (EB)  
STRUCTURE NO. 006-0175 (WB)

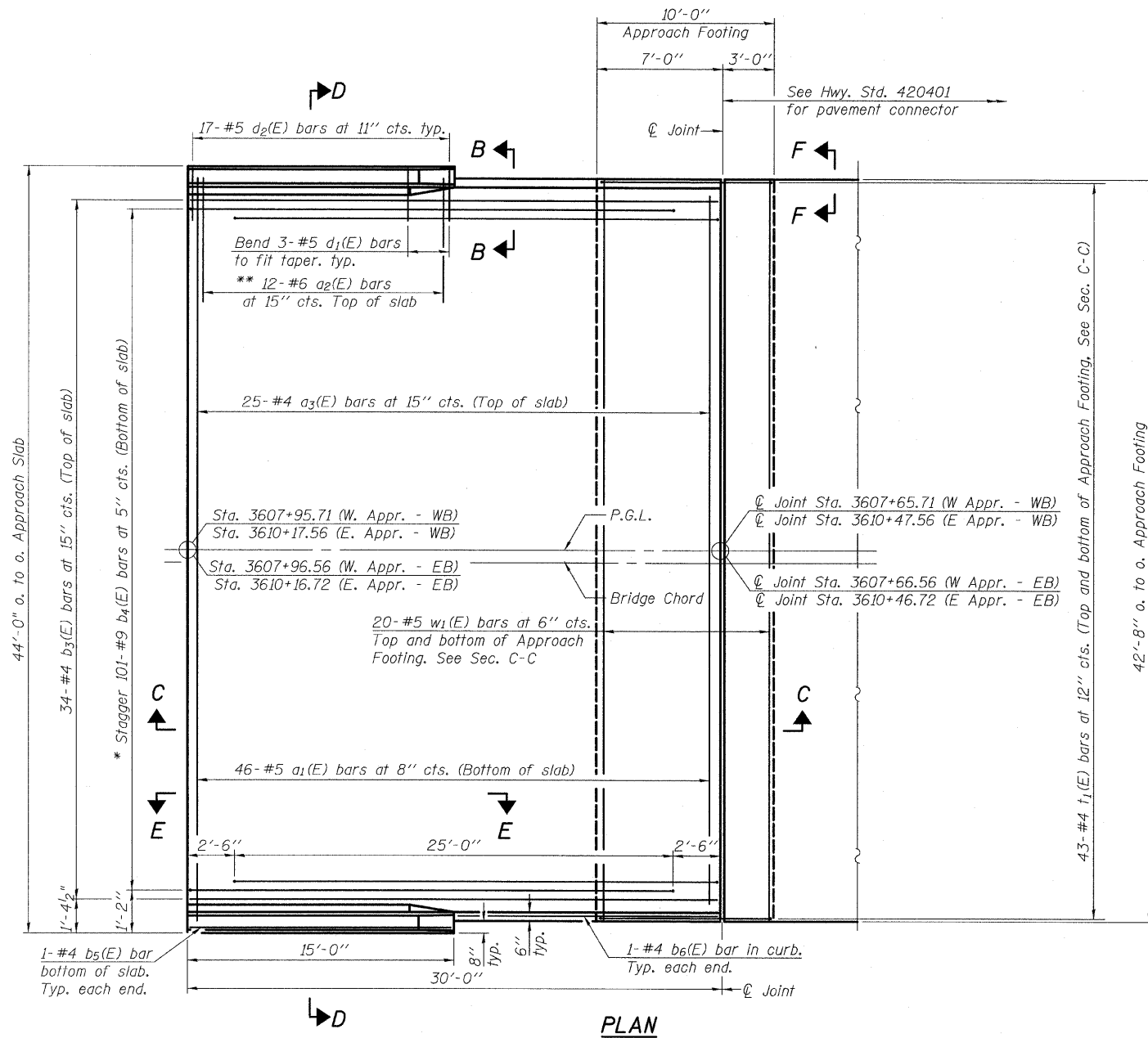
|          |    |
|----------|----|
| DESIGNED | SP |
| CHECKED  | DF |
| DRAWN    | SP |
| CHECKED  | DF |

|                           |                    |              |                  |                     |                  |
|---------------------------|--------------------|--------------|------------------|---------------------|------------------|
| SHEET NO. 15<br>39 SHEETS | F.A. RTE.<br>80    | SECTION<br>* | COUNTY<br>BUREAU | TOTAL SHEETS<br>344 | SHEET NO.<br>213 |
|                           | CONTRACT NO. 66908 |              |                  |                     |                  |
| FED. ROAD DIST. NO.       |                    | ILLINOIS     | FED. AID PROJECT |                     |                  |

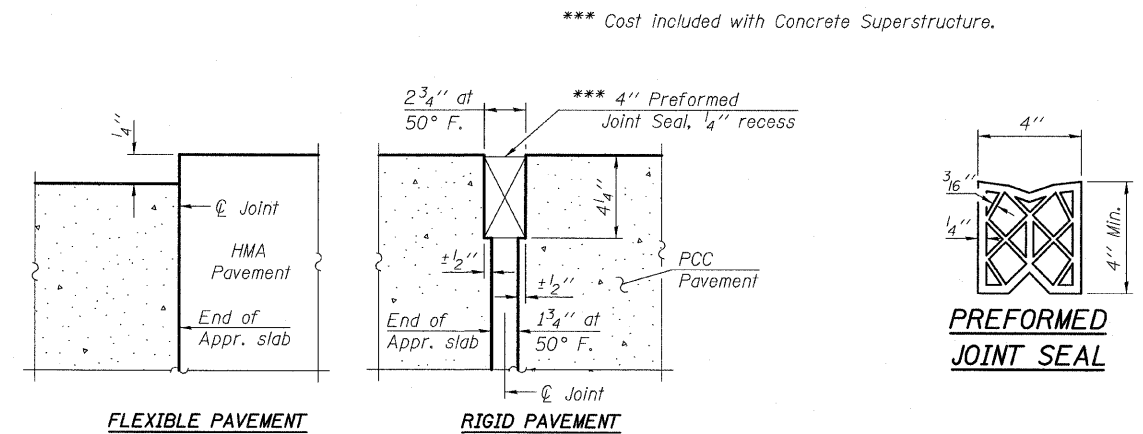
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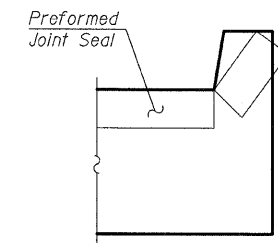
Notes:  
See sheet 17 of 39 for Sections C-C & D-D and View E-E.  
 $a_1(E)$ ,  $a_3(E)$ , and  $w_1(E)$  bar spacings measured perpendicular to  $\text{C} \text{ Rdwy.}$



\* Tilt #9  $b_4(E)$  bars as required to maintain clearance.  
\*\* Alternate with  $a(E)$  bars, typ. ea. parapet.

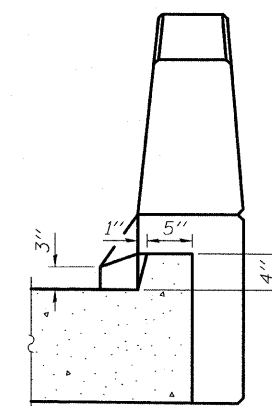


**DETAIL A**



**VIEW F-F**

Angle Preformed Joint Seal at 45° at curbs when req'd for drainage.



**VIEW B-B**

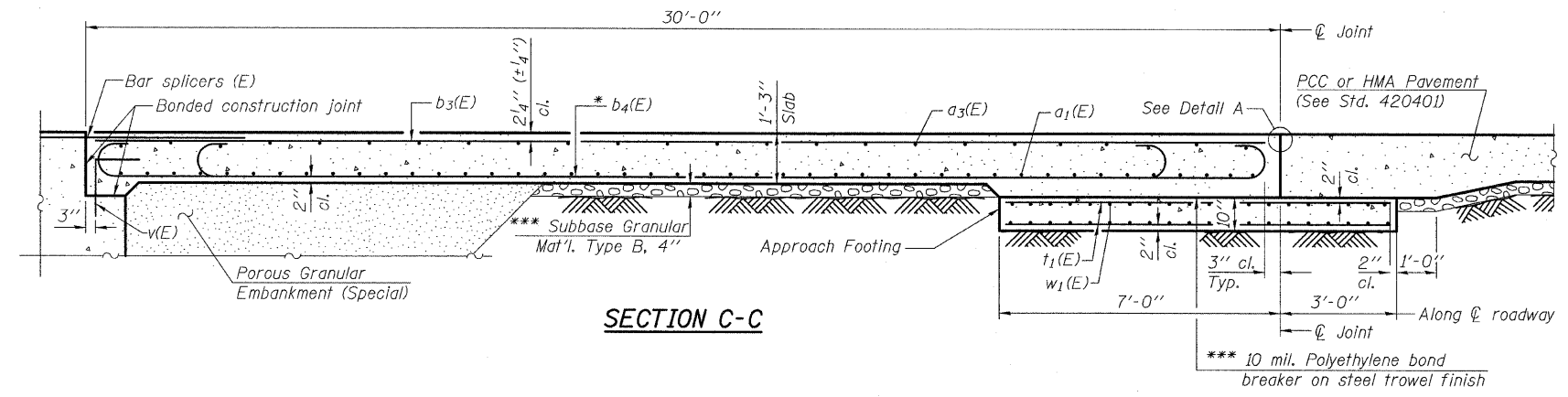
**BRIDGE APPROACH SLAB DETAILS**  
**(SHEET 1 OF 2)**  
**STRUCTURE NO. 006-0174 (EB)**  
**STRUCTURE NO. 006-0175 (WB)**

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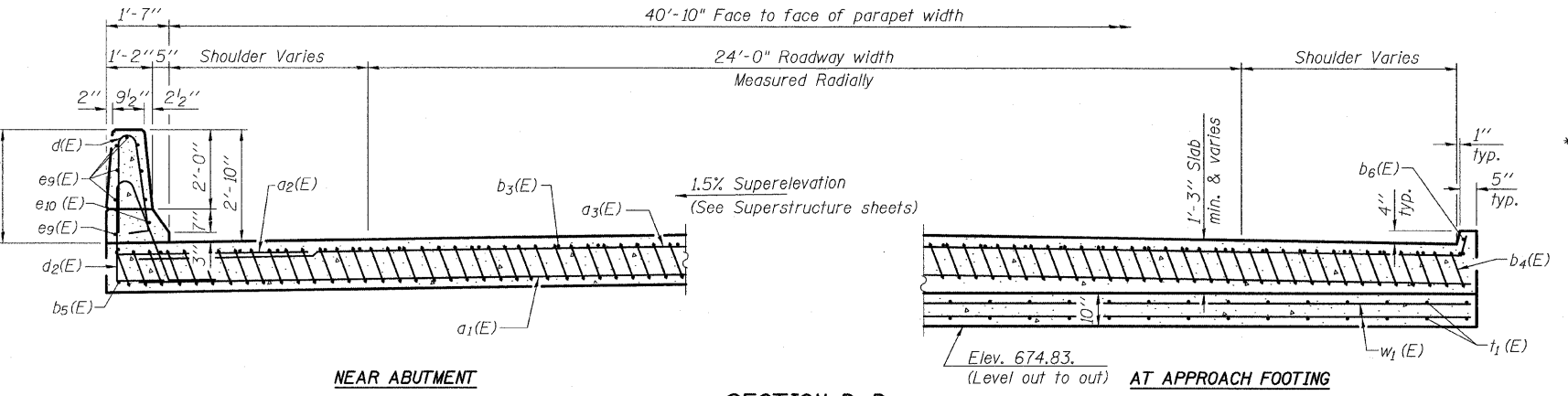
|                     |                    |                           |        |              |           |
|---------------------|--------------------|---------------------------|--------|--------------|-----------|
| SHEET NO. 16        | F.A. RTE.          | SECTION                   | COUNTY | TOTAL SHEETS | SHEET NO. |
|                     | 80                 | *                         | BUREAU | 374          | 214       |
| 39 SHEETS           | CONTRACT NO. 66908 |                           |        |              |           |
| FED. ROAD DIST. NO. |                    | ILLINOIS FED. AID PROJECT |        |              |           |

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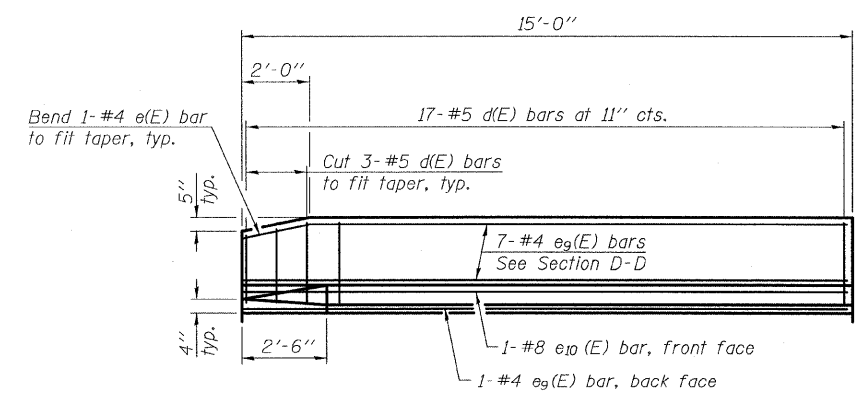
SECTION C-C



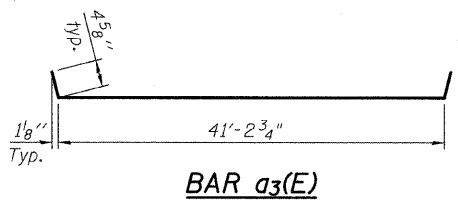
NEAR ABUTMENT

SECTION D-D

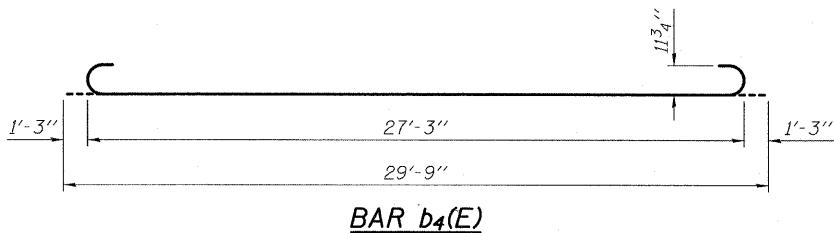
(See Plan for dimensions not shown)



VIEW E-E

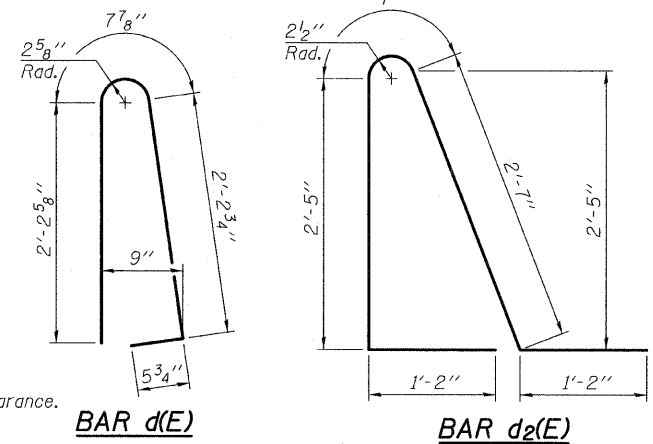


BAR a3(E)



BAR b4(E)

Notes:  
See sheet 16 of 39 for Detail A and View B-B.  
Approach slab and parapet concrete shall be paid for as Concrete Superstructure.  
Approach footing concrete shall be paid for as Concrete Structures.  
Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.  
For v(E) bar details, see sheet 13 of 39.  
The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.  
For bar splicer details, see sheet 32 of 39.  
Cost of excavation for approach footing included with Concrete Structures.  
For Porous Granular Embankment (Special) and drainage treatment details, see sheet 2 of 39.



BAR d(E)

BAR d2(E)

\* Tilt #9 b4(E) bars as required to maintain clearance.  
\*\*\* Cost included with Concrete Superstructure.

TWO APPROACHES  
BILL OF MATERIAL

(Each Bridge - 2 Thus)

| Bar    | No. | Size | Length                           | Shape   |       |
|--------|-----|------|----------------------------------|---------|-------|
| a3(E)  | 50  | #4   | 42'-0"                           | —       |       |
| a1(E)  | 92  | #5   | 42'-4"                           | —       |       |
| a2(E)  | 48  | #6   | 6'-0"                            | —       |       |
| b3(E)  | 68  | #4   | 29'-8"                           | —       |       |
| b4(E)  | 202 | #9   | 29'-9"                           | —       |       |
| b5(E)  | 4   | #4   | 14'-8"                           | —       |       |
| b6(E)  | 4   | #4   | 14'-8"                           | —       |       |
| d(E)   | 68  | #5   | 5'-7"                            | ⌒       |       |
| d2(E)  | 68  | #5   | 7'-11"                           | ⌒       |       |
| e9(E)  | 32  | #4   | 14'-8"                           | —       |       |
| e10(E) | 4   | #8   | 14'-8"                           | —       |       |
| t1(E)  | 172 | #4   | 9'-8"                            | —       |       |
| w1(E)  | 80  | #5   | 42'-4"                           | —       |       |
|        |     |      | Concrete Superstructure          | Cu. Yd. | 128.9 |
|        |     |      | Concrete Structures              | Cu. Yd. | 26.4  |
|        |     |      | Reinforcement Bars, Epoxy Coated | Pound   | 38080 |

(1) 4,650 pounds of total weight is considered Reinforcement Bars, Epoxy Coated for the Substructure.

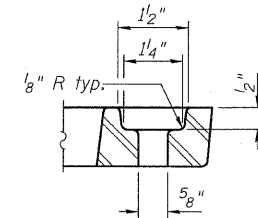
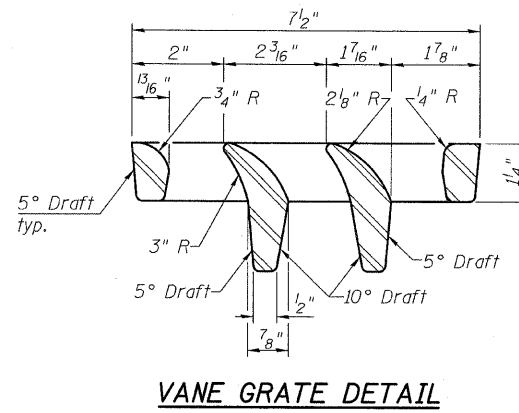
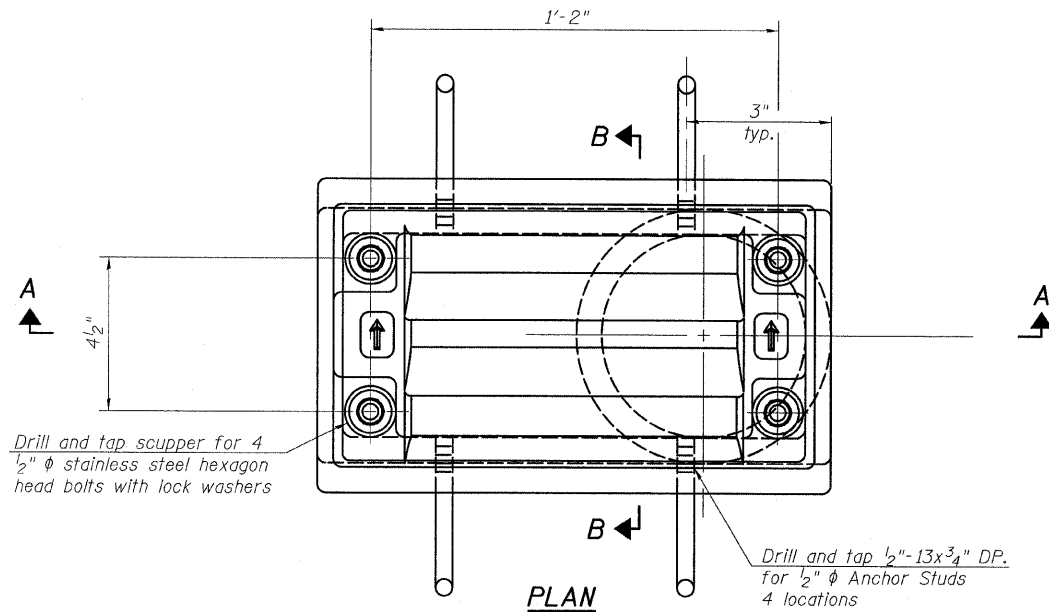
BRIDGE APPROACH SLAB DETAILS  
(SHEET 2 OF 2)  
STRUCTURE NO. 006-0174 (EB)  
STRUCTURE NO. 006-0175 (WB)

|          |     |
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|                           |                    |                           |                  |                     |                  |
|---------------------------|--------------------|---------------------------|------------------|---------------------|------------------|
| SHEET NO. 17<br>39 SHEETS | F.A. RTE.<br>80    | SECTION<br>*              | COUNTY<br>BUREAU | TOTAL SHEETS<br>344 | SHEET NO.<br>215 |
|                           | CONTRACT NO. 66908 |                           |                  |                     |                  |
| FED. ROAD DIST. NO.       |                    | ILLINOIS FED. AID PROJECT |                  |                     |                  |

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DEPARTMENT OF TRANSPORTATION



Notes:

All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 35B.  
Bolts, anchor studs, washers and nuts shall conform to the requirements of ASTM A 307 and shall be galvanized according to AASHTO M 232.

Downspouts located on the exterior side of a painted steel fascia beam shall be painted with the finish coat specified for the exterior side of the fascia beam.

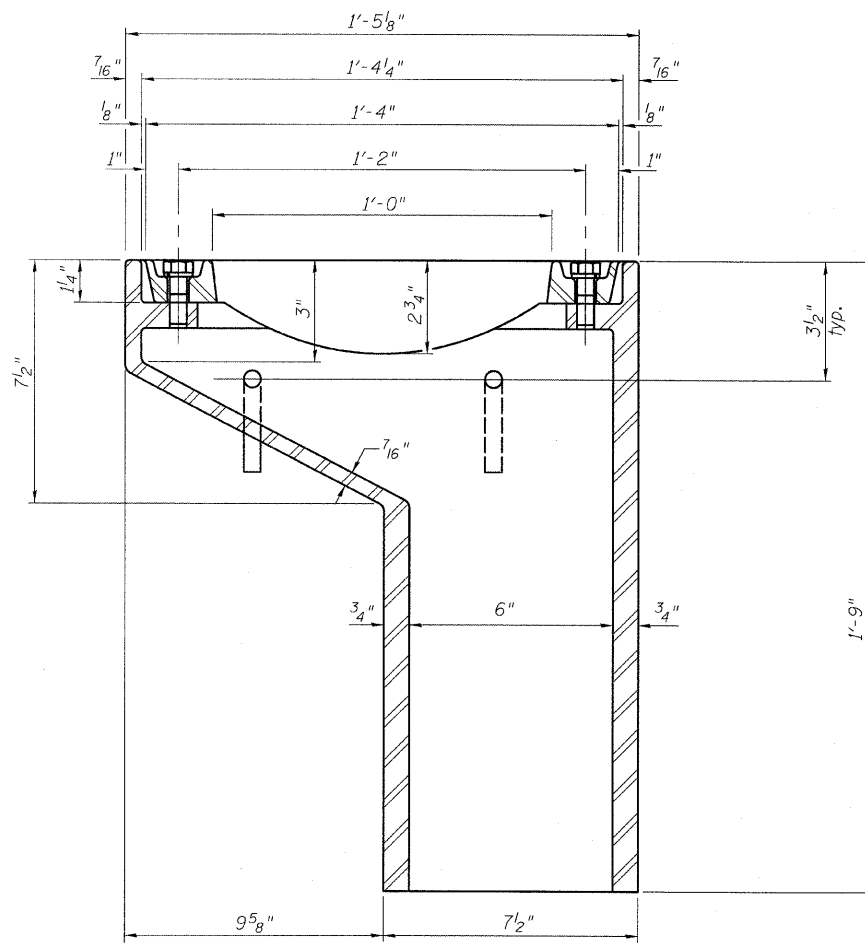
As an alternate, bolts, anchor studs, washers and nuts may be stainless steel according to Article 1006.29(d) of the Standard Specifications.

Structural steel weldments of equal sections and of the same configuration may be substituted for the cast iron scupper frame. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval. Structural steel weldments shall not be substituted for the cast iron scupper grate. Structural steel frames and downspouts shall be galvanized according to AASHTO M111.

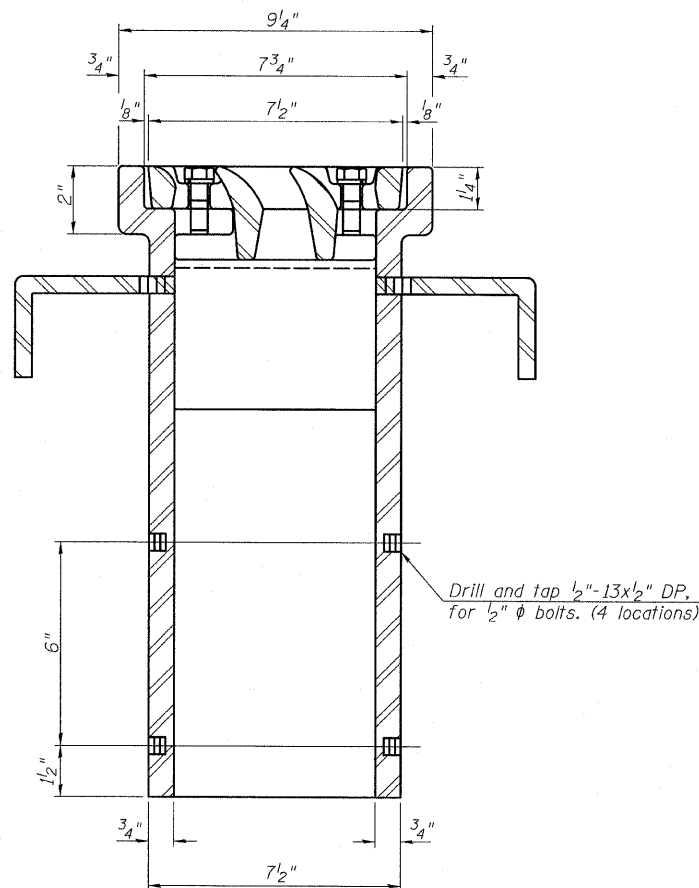
The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.

Cost of the Grate, Frame, Downspout, Anchor Studs, Bolts, Washers and Nuts including complete installation of the scupper shall be paid for at the contract unit price each for Drainage Scupper, DS-11.

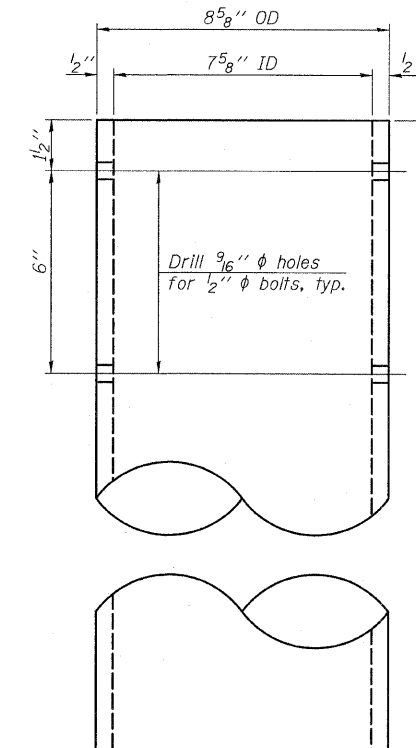
Alternate fiberglass downspout conforming to ASTM D 2996 with a short-time rupture strength hoop tensile stress of 30,000 psi min. may be used in lieu of the cast iron or steel equivalent.



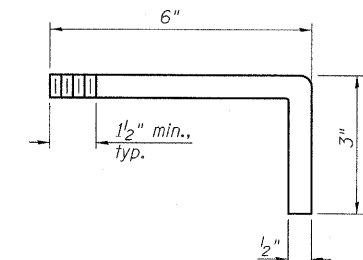
**SECTION A-A**  
See sheet 12 of 39 for scupper location relative to parapet.



**SECTION B-B**



**DOWNSPOUT**



**ANCHOR STUD DETAIL**

**BILL OF MATERIAL**

| ITEM                    | UNIT | QUANTITY |
|-------------------------|------|----------|
| Drainage Scupper, DS-11 | Each | 5        |

**DRAINAGE SCUPPER, DS-11  
STRUCTURE NO. 006-0174 (EB)**

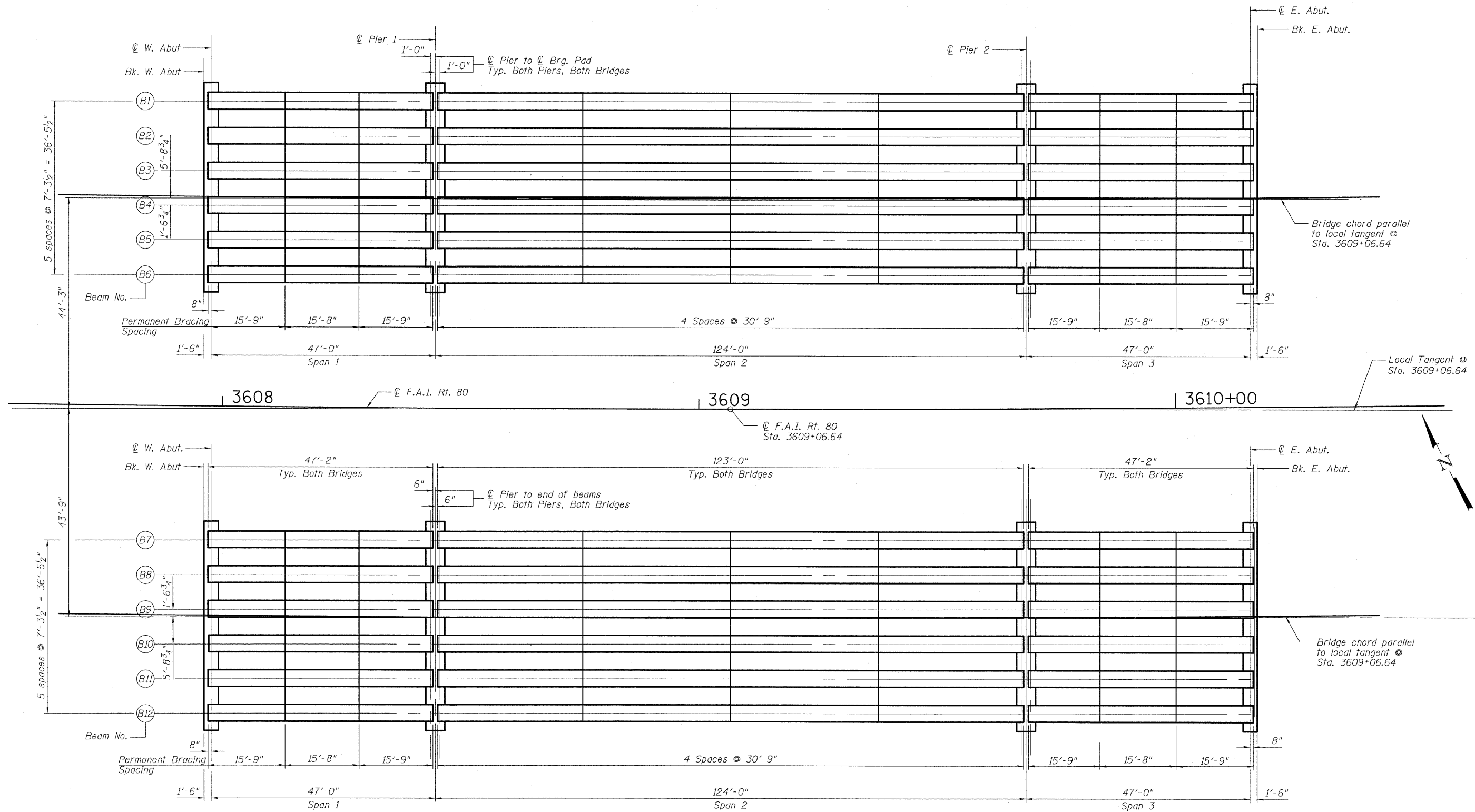
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| SHEET NO.           | F.A. RTE. | SECTION | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|---------------------|-----------|---------|---------------------------|--------------|-----------|
| 18                  | 80        | *       | BUREAU                    | 347          | 216       |
| 39 SHEETS           |           |         |                           |              |           |
| FED. ROAD DIST. NO. |           |         | ILLINOIS FED. AID PROJECT |              |           |
| CONTRACT NO. 66908  |           |         |                           |              |           |

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**NOTES:**

1. For Beam Details, see Sheets 20 thru 23 of 39.
2. For Bill of Material, see Sheet 23 of 39.

**FRAMING PLAN**

72" PPC Bulb T-Beams (typ.)

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| DRAWN    | IM  |
| CHECKED  | PDF |

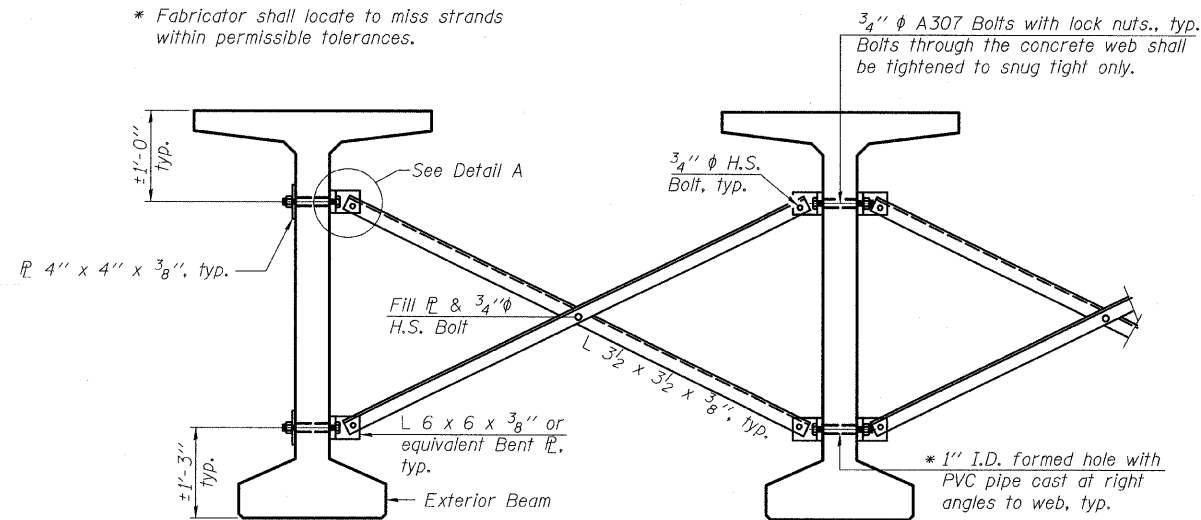
**FRAMING PLAN**  
**STRUCTURE NO. 006-0174 (EB)**  
**STRUCTURE NO. 006-0175 (WB)**

|                           |                    |                           |                  |                     |                  |
|---------------------------|--------------------|---------------------------|------------------|---------------------|------------------|
| SHEET NO. 19<br>39 SHEETS | F.A. RTE.<br>80    | SECTION<br>*              | COUNTY<br>BUREAU | TOTAL SHEETS<br>344 | SHEET NO.<br>217 |
|                           | CONTRACT NO. 66908 |                           |                  |                     |                  |
| FED. ROAD DIST. NO.       |                    | ILLINOIS FED. AID PROJECT |                  |                     |                  |

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\*06-L7BR & BR-1,7VB-M, 6BR & 6, 7 RS-1 & IJ

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

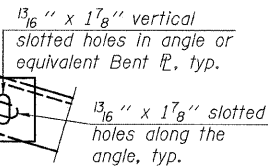


\* Fabricator shall locate to miss strands within permissible tolerances.

3/4"  $\phi$  A307 Bolts with lock nuts., typ.  
Bolts through the concrete web shall be tightened to snug tight only.

Notes:

All material for bracing shall be hot dip galvanized according to AASHTO M111 unless otherwise noted.  
Two hardened washers are required for each set of oversized holes.  
All holes shall be 13/16"  $\phi$  unless otherwise noted.  
5/16" x 3" x 3" plate washers are required over all slotted holes.  
All bolts shall be galvanized according to AASHTO M232.  
Bracing shall be installed as beams are erected and tightened as soon as possible during erection.



DETAIL A

**PERMANENT BRACING DETAILS**

Cost of permanent bracing included with Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 72"

|          |                    | 0.4 Sp. 1<br>0.6 Sp. 3 | Pier | 0.5 Sp. 2 |
|----------|--------------------|------------------------|------|-----------|
| $I$      | (in <sup>4</sup> ) | 545894                 |      | 545894    |
| $I'$     | (in <sup>4</sup> ) | 1040040                |      | 1040040   |
| $S_b$    | (in <sup>3</sup> ) | 14915                  |      | 14915     |
| $S_b'$   | (in <sup>3</sup> ) | 19980                  |      | 19980     |
| $S_t$    | (in <sup>3</sup> ) | 15421                  |      | 15421     |
| $S_t'$   | (in <sup>3</sup> ) | 56520                  |      | 56520     |
| $Q$      | (k/')              | 1.571                  |      | 1.571     |
| $M_Q$    | (k)                | 415                    |      | 2923      |
| $S_Q$    | (k/')              | 0.5                    | 0.5  | 0.5       |
| $M_{sQ}$ | (k)                | -81                    | 534  | 427       |
| $M_L$    | (k)                | 328                    | 658  | 719       |
| $M_I$    | (k)                | 96                     | 158  | 144       |

$I$ : Non-composite moment of inertia of beam section (in<sup>4</sup>).  
 $I'$ : Composite moment of inertia of beam section (in<sup>4</sup>).  
 $S_b$ : Non-composite section modulus for the bottom fiber of the prestressed beam (in<sup>3</sup>).  
 $S_b'$ : Composite section modulus for the bottom fiber of the prestressed beam (in<sup>3</sup>).  
 $S_t$ : Non-composite section modulus for the top fiber of the prestressed beam (in<sup>3</sup>).  
 $S_t'$ : Composite section modulus for the top fiber of the prestressed beam (in<sup>3</sup>).  
 $Q$ : Un-factored non-composite dead load (kips/ft.).  
 $M_Q$ : Un-factored moment due to non-composite dead load conservatively taken at 0.5 of the span (kip-ft.).  
 $s_Q$ : Un-factored long-term composite (superimposed) dead load (kips/ft.).  
 $M_s Q$ : Un-factored moment due to long-term composite (superimposed) dead load (kip-ft.).  
 $M_L$ : Un-factored live load moment on the composite section (kip-ft.).  
 $M_I$ : Un-factored moment due to impact on the composite section (kip-ft.).

|             |     | Abut. | Pier 1 Span 1<br>Pier 2 Span 3 | Pier 1 Span 2<br>Pier 2 Span 2 |
|-------------|-----|-------|--------------------------------|--------------------------------|
| $R_Q$       | (k) | 36.1  | 38.1                           | 97.4                           |
| * $R_s Q$   | (k) | 0.4   | 27.0                           | 27.0                           |
| * $R_L$     | (k) | 36.9  | 31.8                           | 31.8                           |
| * $R_I$     | (k) | 10.7  | 7.6                            | 7.6                            |
| $R_{Total}$ | (k) | 84.1  | 104.5                          | 163.8                          |

\* The total  $R_s Q$ ,  $R_L$ , and impact reactions are assumed to be distributed evenly to each bearing line at a pier regardless of the span ratios. The bearing design at a pier is based on the maximum reactions of either span.

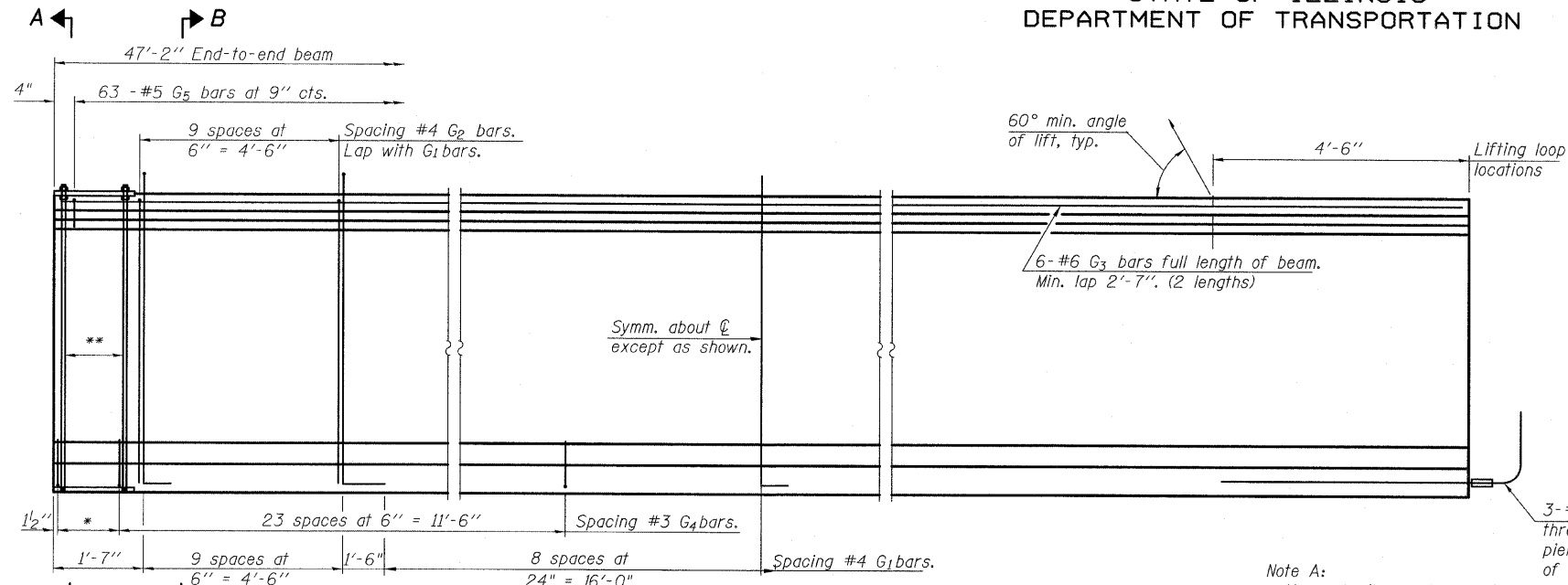
**72" PPC BULB-T BEAM DETAILS**  
**PERMANENT BRACING**  
**AND MOMENT TABLES**  
**STRUCTURE NO. 006-0174 (EB)**  
**STRUCTURE NO. 006-0175 (WB)**

|          |     |
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|              |                     |                  |          |                    |           |
|--------------|---------------------|------------------|----------|--------------------|-----------|
| SHEET NO. 20 | F.A. RTE.           | SECTION          | COUNTY   | TOTAL SHEETS       | SHEET NO. |
|              | 80                  | *                | BUREAU   | 374                | 218       |
| 39 SHEETS    | FED. ROAD DIST. NO. |                  | ILLINOIS | CONTRACT NO. 66908 |           |
|              |                     | FED. AID PROJECT |          |                    |           |

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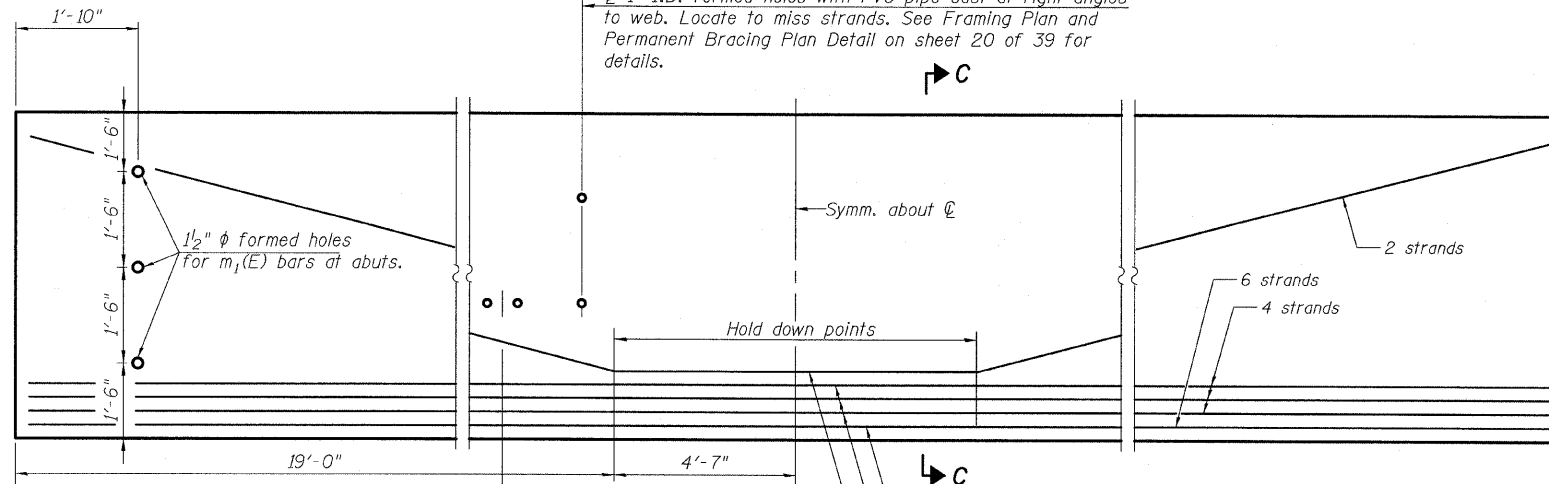
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**ELEVATION OF BEAM**  
(Showing reinforcement & dimensions)

\* 4 spaces at 3 1/4" = 1'-1".  
\*\* 5- 3/4" φ threaded dowel rods at 3 1/4" cts., each face.

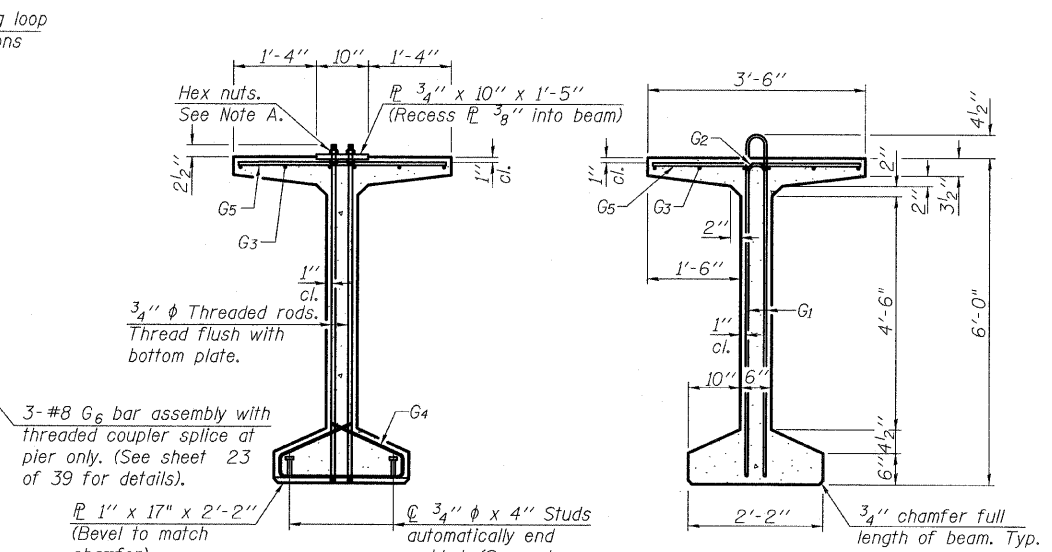
1" I.D. formed holes with PVC pipe cast at right angles to web. Locate to miss strands. See Framing Plan and Permanent Bracing Plan Detail on sheet 20 of 39 for details.



**ELEVATION OF BEAM**  
(Showing prestressing steel)

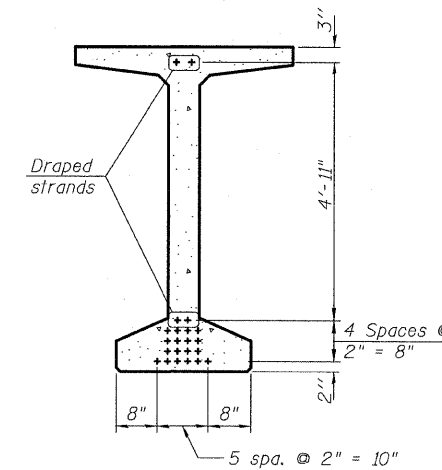
Inserts for scupper pipe clamps. Beam 7 only, locate to miss strands. See sheet 11 of 39 for scupper locations.

Note A:  
Hex nuts (top and bottom) with lock washers (top). Only tighten sufficiently to compress lock washers.



**SECTION A-A**

**SECTION B-B**



**SECTION C-C**

**\*\*\*BAR LIST**  
**ONE BEAM ONLY**

| Bar            | No. | Size | Length | Shape |
|----------------|-----|------|--------|-------|
| G <sub>1</sub> | 37  | #4   | 13'-5" | NL    |
| G <sub>2</sub> | 20  | #4   | 11'-8" | N     |
| G <sub>3</sub> | 12  | #6   | 25'-0" | —     |
| G <sub>4</sub> | 56  | #3   | 4'-11" | —     |
| G <sub>5</sub> | 63  | #5   | 3'-4"  | —     |
| G <sub>6</sub> | 3   | #8   | 6'-6"  | —     |

\*\*\*For information only

Notes:  
See sheet 23 of 39 for additional details and Bill of Material.  
Required release strength, f'ci, shall be 5,000 psi.

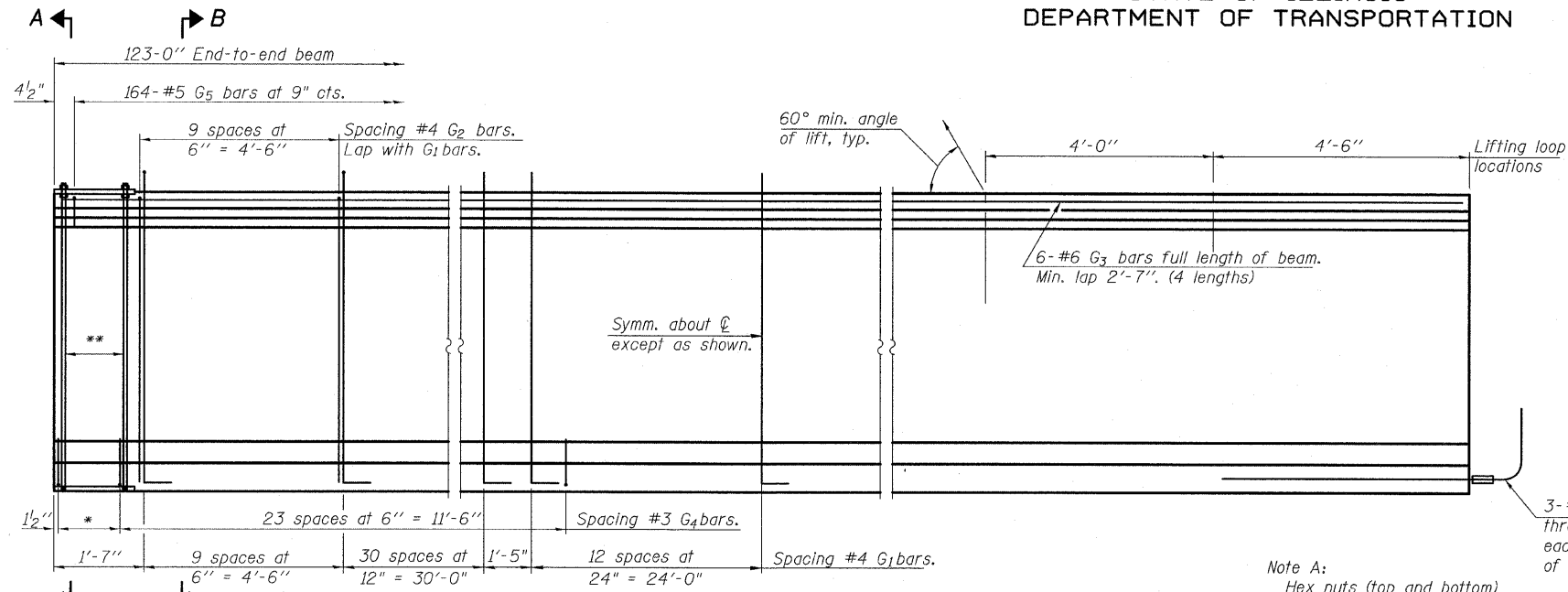
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**72" PPC BULB T-BEAM SPANS 1 & 3**  
**STRUCTURE NO. 006-0174 (EB)**  
**STRUCTURE NO. 006-0175 (WB)**

|                           |                    |                           |                  |                     |                  |
|---------------------------|--------------------|---------------------------|------------------|---------------------|------------------|
| SHEET NO. 21<br>39 SHEETS | F.A. RTE.<br>80    | SECTION<br>*              | COUNTY<br>BUREAU | TOTAL SHEETS<br>344 | SHEET NO.<br>219 |
|                           | CONTRACT NO. 66908 |                           |                  |                     |                  |
| FED. ROAD DIST. NO.       |                    | ILLINOIS FED. AID PROJECT |                  |                     |                  |

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**ELEVATION OF BEAM**  
(Showing reinforcement & dimensions)

\* 4 spaces at 3 1/4" = 1'-1".  
\*\* 5-3/4"  $\phi$  threaded dowel rods at 3 1/4" cts., each face.

Note A:  
Hex nuts (top and bottom) with lock washers (top). Only tighten sufficiently to compress lock washers.

3-#8 G<sub>6</sub> bar assembly with threaded coupler splice at each end. (See sheet 23 of 39 for details).

3/4"  $\phi$  Threaded rods. Thread flush with bottom plate.

3/4"  $\phi$  x 10" x 1'-5" (Recess 3/8" into beam)

Hex nuts. See Note A.

1'-4" 10" 1'-4"

2 1/2"

G<sub>5</sub> G<sub>3</sub>

1" cl.

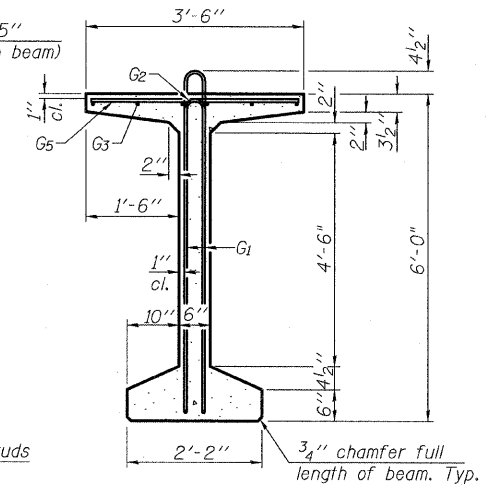
1" cl.

G<sub>4</sub>

3/4"  $\phi$  x 4" Studs automatically end welded. (Space to miss strands).

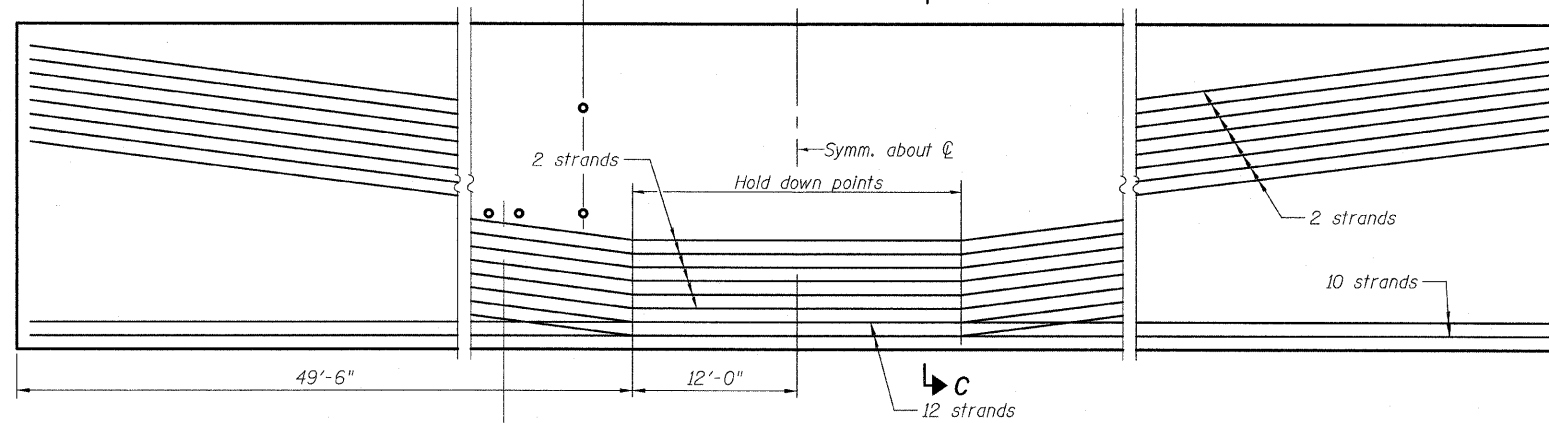
1" x 17" x 2'-2" (Bevel to match chamfer).

**SECTION A-A**



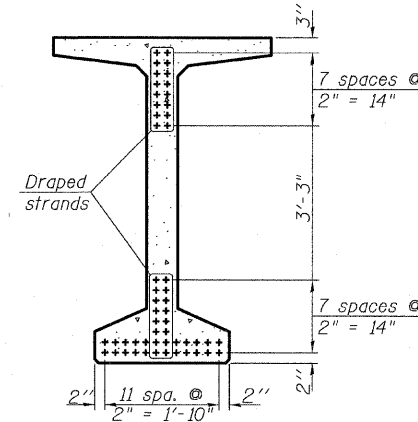
**SECTION B-B**

1" I.D. formed holes with PVC pipe cast at right angles to web. Locate to miss strands. See Framing Plan and Permanent Bracing Plan Detail on sheet 20 of 39 for details.



**ELEVATION OF BEAM**  
(Showing prestressing steel)

Inserts for scupper pipe clamps. Beam 7 only, locate to miss strands. See sheet 11 of 39 for scupper locations.



**SECTION C-C**

**\*\*\*BAR LIST  
ONE BEAM ONLY**

| Bar            | No. | Size | Length | Shape |
|----------------|-----|------|--------|-------|
| G <sub>1</sub> | 105 | #4   | 13'-5" | ⊔     |
| G <sub>2</sub> | 20  | #4   | 11'-8" | ⊔     |
| G <sub>3</sub> | 24  | #6   | 33'-4" | ⊔     |
| G <sub>4</sub> | 56  | #3   | 4'-11" | ⊔     |
| G <sub>5</sub> | 164 | #5   | 3'-4"  | ⊔     |
| G <sub>6</sub> | 6   | #8   | 6'-6"  | ⊔     |

\*\*\*For information only

Notes:  
See sheet 23 of 39 for additional details and Bill of Material.  
Required release strength,  $f'_{ci}$ , shall be 5,000 psi.

|          |     |
|----------|-----|
| DESIGNED | IM  |
| CHECKED  | PDF |
| DRAWN    | IM  |
| CHECKED  | PDF |

**72" PPC BULB T-BEAM SPAN 2  
STRUCTURE NO. 006-0174 (EB)  
STRUCTURE NO. 006-0175 (WB)**

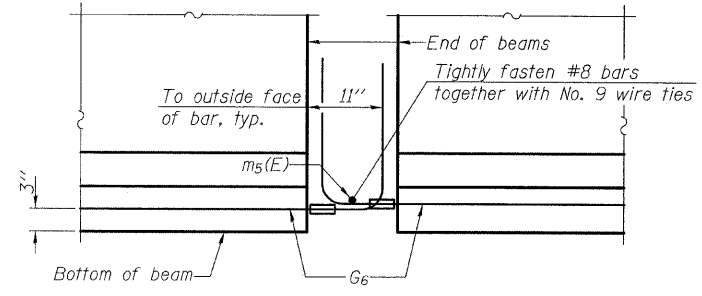
|              |                              |           |                                     |                  |               |
|--------------|------------------------------|-----------|-------------------------------------|------------------|---------------|
| SHEET NO. 22 | F.A. RTE. 80                 | SECTION * | COUNTY BUREAU                       | TOTAL SHEETS 324 | SHEET NO. 220 |
| 39 SHEETS    | FED. ROAD DIST. NO. ILLINOIS |           | FED. AID PROJECT CONTRACT NO. 66908 |                  |               |

TYL INTERNATIONAL

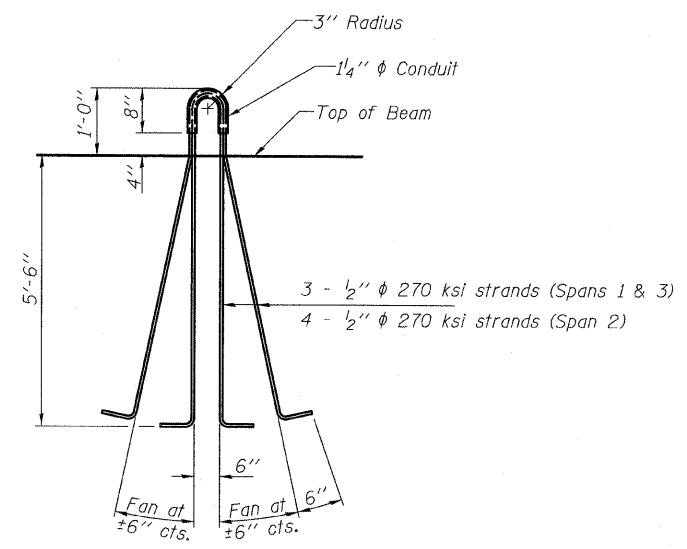
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**NOTES**

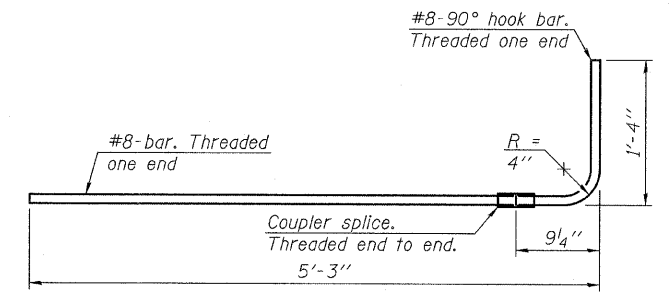
Inserts for  $\frac{3}{4}$ "  $\phi$  threaded dowel rods, when specified, are to be two strut, ferrule type for interior beams and single ferrule, flared loop type for exterior beams.  
Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270. The nominal diameter shall be  $\frac{1}{2}$ " and the nominal cross-sectional area shall be 0.153 sq. in.  
Reinforcement bars shall conform to ASTM A 706, Grade 60. (See Special Provisions). A minimum 2 $\frac{1}{2}$ "  $\phi$  lifting pin shall be used to engage the lifting loops during handling. Tilt G<sub>6</sub> bars when necessary to maintain  $\frac{1}{2}$ " clearance.  
The top and bottom plates shall be AASHTO M270 Grade 50. The bottom rods and studs shall be galvanized according to AASHTO M111. Threaded rods shall be ASTM F 1554 Grade 55.  
The G<sub>6</sub> bar assembly shall have the threaded ends oversized to ensure no reduction in cross sectional area after threading. The coupler splice shall be capable of developing 125 percent of the yield strength of the reinforcement bar.



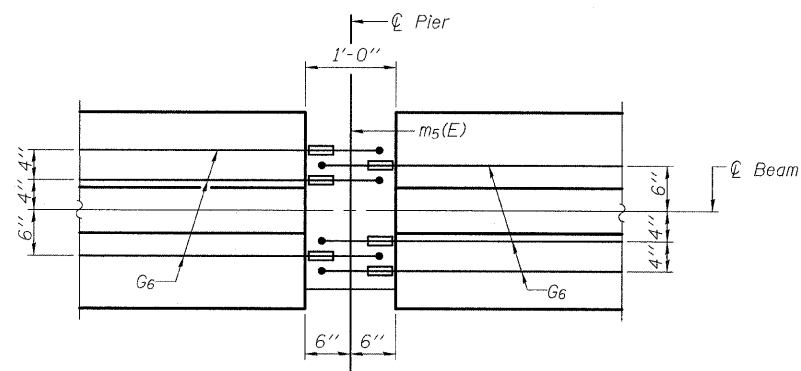
**ELEVATION OF BEAM AT PIER**



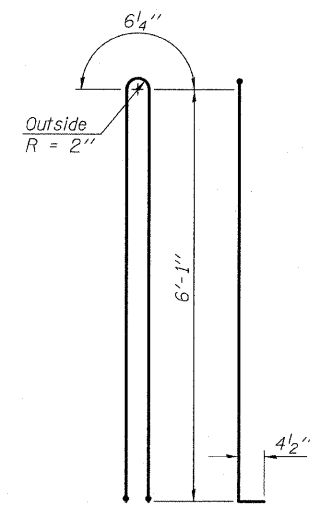
**LIFTING LOOP DETAIL**



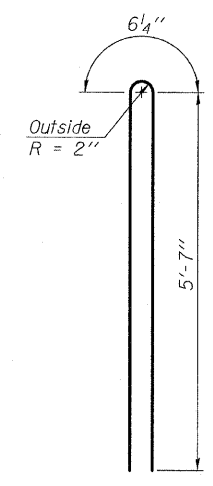
**G<sub>6</sub> BAR ASSEMBLY**



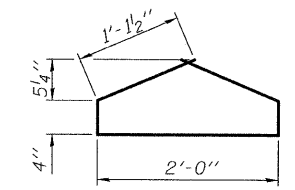
**PLAN OF BEAM AT PIER**



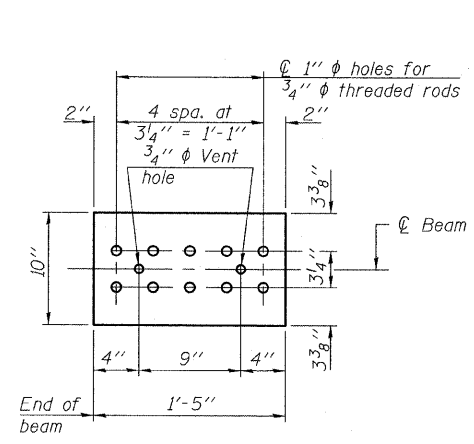
**BAR G<sub>1</sub>**



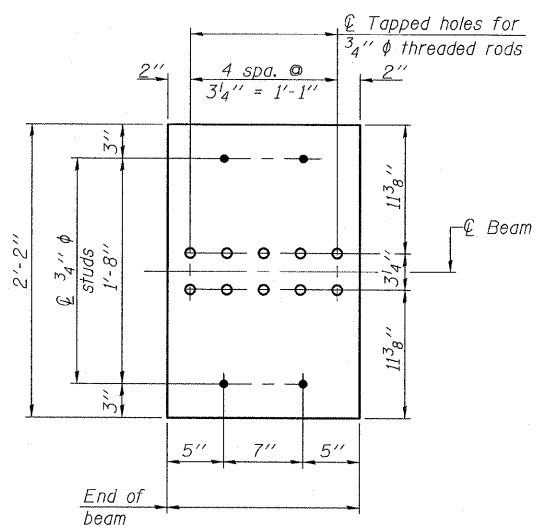
**BAR G<sub>2</sub>**



**BAR G<sub>4</sub>**



**TOP PLATE**



**BOTTOM PLATE**

**BILL OF MATERIAL**

(Total for 2 Bridges)

| Item   | Unit | Total |
|--|------|-------|
| Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 72" | Ft.  | 2,608 |

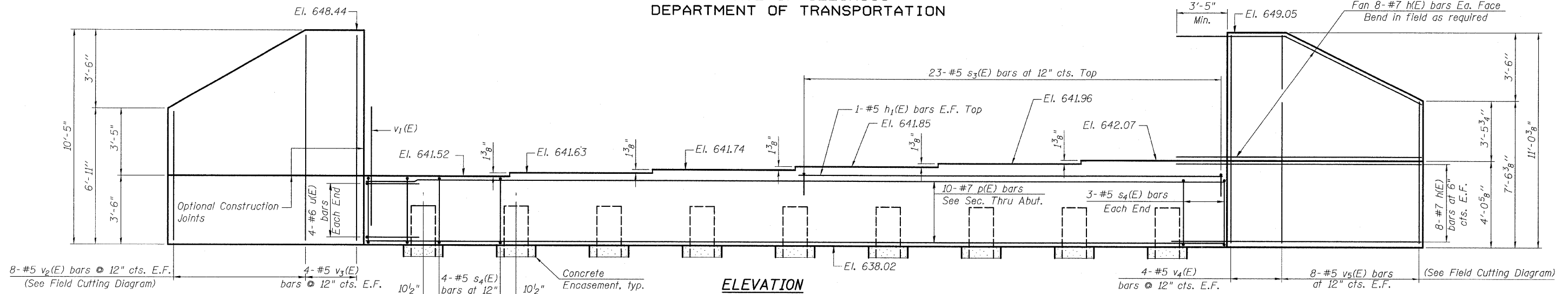
**72" PPC BULB T-BEAM DETAILS**  
**STRUCTURE NO. 006-0174 (EB)**  
**STRUCTURE NO. 006-0175 (WB)**

|          |     |
|----------|-----|
| DESIGNED | AD  |
| CHECKED  | PDF |
| DRAWN    | PK  |
| CHECKED  | PDF |

|                           |                    |                           |                  |                     |                  |
|---------------------------|--------------------|---------------------------|------------------|---------------------|------------------|
| SHEET NO. 23<br>39 SHEETS | F.A. RTE.<br>80    | SECTION<br>*              | COUNTY<br>BUREAU | TOTAL SHEETS<br>344 | SHEET NO.<br>221 |
|                           | CONTRACT NO. 66908 |                           |                  |                     |                  |
| FED. ROAD DIST. NO.       |                    | ILLINOIS FED. AID PROJECT |                  |                     |                  |

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DEPARTMENT OF TRANSPORTATION



**PILE DATA**

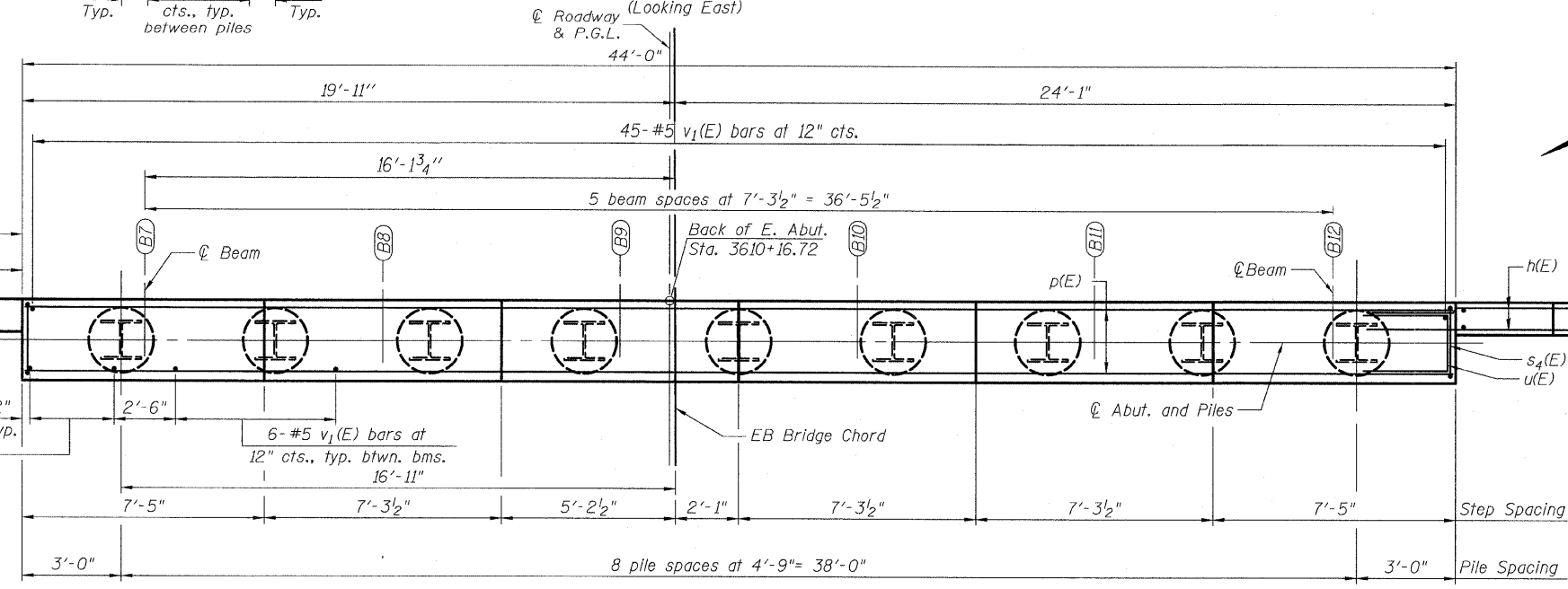
|                                 |               |
|---------------------------------|---------------|
| Type:                           | Steel HP14x73 |
| Nominal Required Bearing:       | 270 kips      |
| Allowable Resistance Available: | 90 kips       |
| Est. Length:                    | 36'           |
| No. Production Piles:           | 8 Piles       |
| No. Test Piles:                 | 1 Pile        |

**LAP SPLICES**

| Bar | Lap   |
|-----|-------|
| #4  | 1'-8" |
| #5  | 2'-2" |
| #6  | 2'-7" |
| #7  | 3'-5" |
| #8  | 4'-6" |

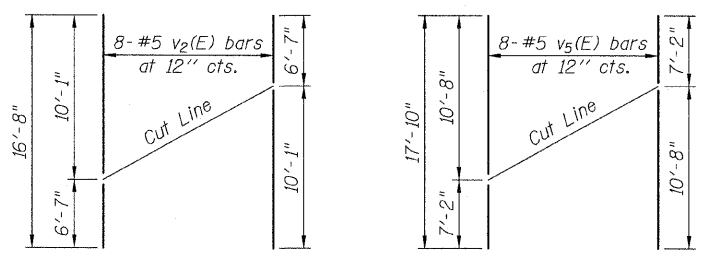
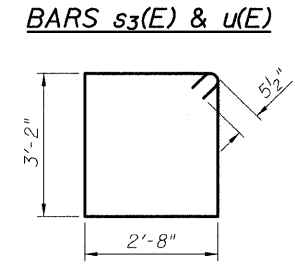
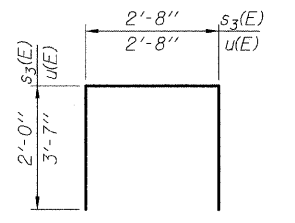
**NOTES:**

1. Pour steps monolithically with cap.
2. For details of piles and Concrete Encasement, see sheet 33 of 39.



**LEGEND**

E.F. - Denotes Each Face.



**FIELD CUTTING DIAGRAMS**

Order bars full length. Cut as shown and use remainder of bars in opposite face.

**BILL OF MATERIAL**

| Bar   | No. | Size | Length | Shape |
|-------|-----|------|--------|-------|
| h(E)  | 64  | #7   | 13'-3" | —     |
| h1(E) | 2   | #5   | 21'-8" | —     |
| p(E)  | 10  | #7   | 43'-8" | —     |
| s3(E) | 23  | #5   | 6'-8"  | □     |
| s4(E) | 38  | #5   | 12'-7" | □     |
| u(E)  | 8   | #6   | 9'-10" | □     |
| v1(E) | 81  | #5   | 4'-4"  | —     |
| v2(E) | 8   | #5   | 16'-8" | —     |
| v3(E) | 8   | #5   | 10'-1" | —     |
| v4(E) | 8   | #5   | 10'-8" | —     |

**BILL OF MATERIAL**

| Bar                              | No. | Size  | Length  | Shape |
|----------------------------------|-----|-------|---------|-------|
| v5(E)                            | 8   | #5    | 17'-10" | —     |
| Structure Excavation             |     | CU YD | 78      |       |
| Concrete Structures              |     | CU YD | 25.4    |       |
| Reinforcement Bars, Epoxy Coated |     | POUND | 4280    |       |
| Furnishing Steel Pile, HP14x73   |     | FOOT  | 288     |       |
| Driving Piles                    |     | FOOT  | 288     |       |
| Test Pile Steel, HP14x73         |     | EACH  | 1       |       |
| Concrete Encasement              |     | CU YD | 5.0     |       |

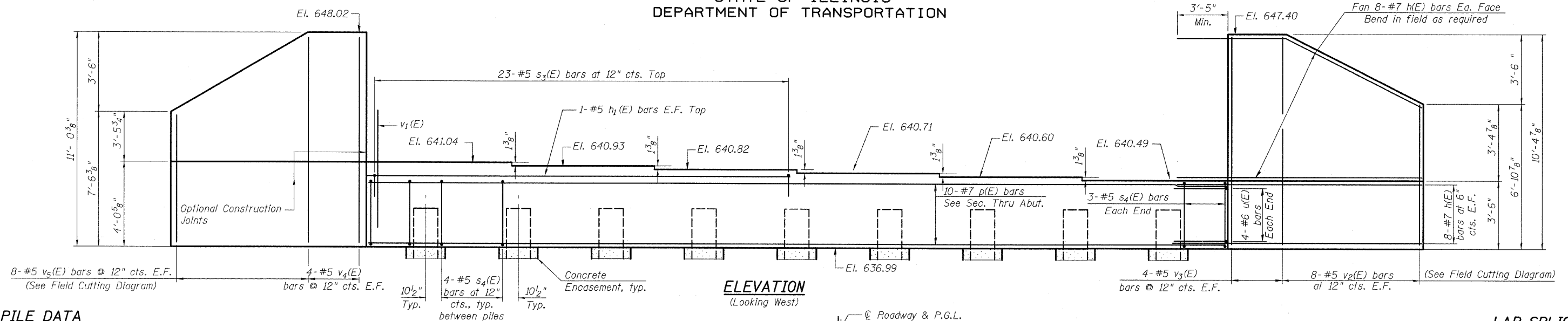
**EAST ABUTMENT  
STRUCTURE NO. 006-0174 (EB)**

|          |       |
|----------|-------|
| DESIGNED | IM/AD |
| CHECKED  | PDF   |
| DRAWN    | IM    |
| CHECKED  | PDF   |

|              |   |           |                    |                  |               |
|--------------|---|-----------|--------------------|------------------|---------------|
| SHEET NO. 24 | F.A. RTE. 80                                  | SECTION * | COUNTY BUREAU      | TOTAL SHEETS 324 | SHEET NO. 222 |
| 39 SHEETS    | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |           | CONTRACT NO. 66908 |                  |               |

TYLIN INTERNATIONAL

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**PILE DATA**

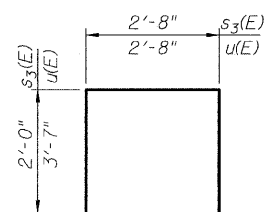
Type: Steel HP14x73  
 Nominal Required Bearing: 270 kips  
 Allowable Resistance Available: 90 kips  
 Est. Length: 57'  
 No. Production Piles: 8 Piles  
 No. Test Piles: 1 Pile

**NOTES:**

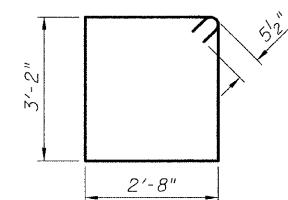
1. Pour steps monolithically with cap.
2. For details of piles and Concrete Encasement, see sheet 33 of 39.

**LEGEND**

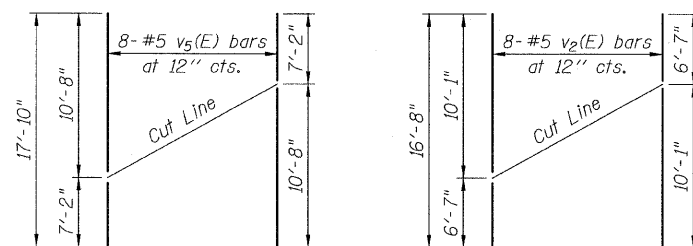
E.F. - Denotes Each Face.



**BARS s<sub>3</sub>(E) & u(E)**



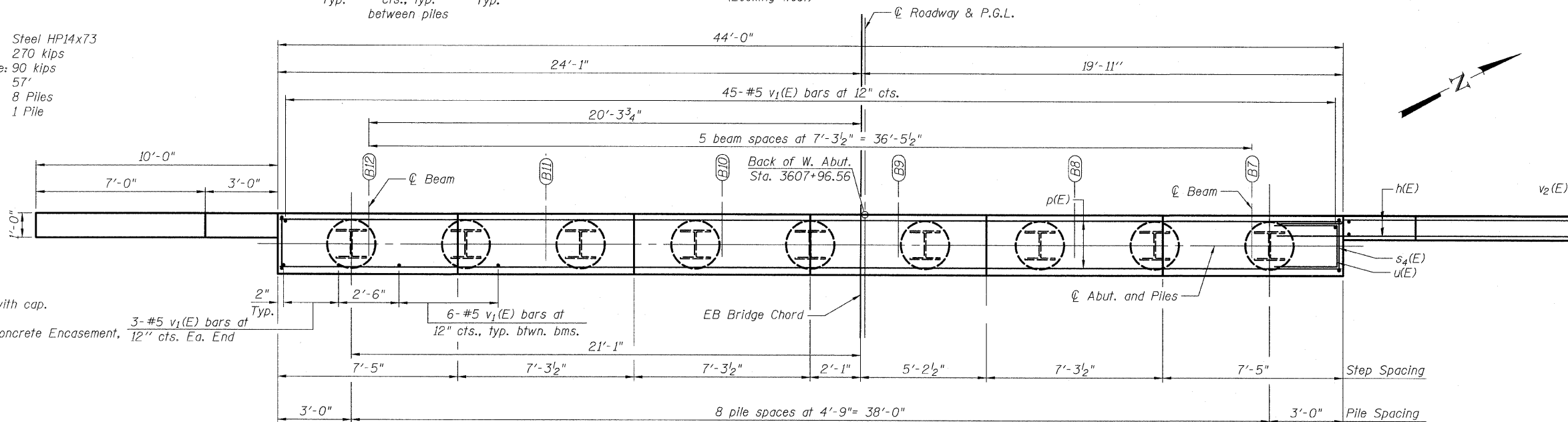
**BAR s<sub>4</sub>(E)**



**FIELD CUTTING DIAGRAMS**

Order bars full length. Cut as shown and use remainder of bars in opposite face.

**PLAN**



**LAP SPLICES**

| Bar | Lap   |
|-----|-------|
| #4  | 1'-8" |
| #5  | 2'-2" |
| #6  | 2'-7" |
| #7  | 3'-5" |
| #8  | 4'-6" |

**BILL OF MATERIAL**

| Bar                | No. | Size | Length | Shape |
|--------------------|-----|------|--------|-------|
| h(E)               | 64  | #7   | 13'-3" | —     |
| h <sub>1</sub> (E) | 2   | #5   | 21'-8" | —     |
| p(E)               | 10  | #7   | 43'-8" | —     |
| s <sub>3</sub> (E) | 23  | #5   | 6'-8"  | □     |
| s <sub>4</sub> (E) | 38  | #5   | 12'-7" | □     |
| u(E)               | 8   | #6   | 9'-10" | □     |
| v <sub>1</sub> (E) | 81  | #5   | 4'-4"  | —     |
| v <sub>2</sub> (E) | 8   | #5   | 16'-8" | —     |
| v <sub>3</sub> (E) | 8   | #5   | 10'-1" | —     |
| v <sub>4</sub> (E) | 8   | #5   | 10'-8" | —     |

**BILL OF MATERIAL**

| Bar                              | No. | Size  | Length  | Shape |
|----------------------------------|-----|-------|---------|-------|
| v <sub>5</sub> (E)               | 8   | #5    | 17'-10" | —     |
| Structure Excavation             |     | CU YD |         | 78    |
| Concrete Structures              |     | CU YD |         | 25.4  |
| Reinforcement Bars, Epoxy Coated |     | POUND |         | 4280  |
| Furnishing Steel Pile, HP14x73   |     | FOOT  |         | 456   |
| Driving Piles                    |     | FOOT  |         | 456   |
| Test Pile Steel, HP14x73         |     | EACH  |         | 1     |
| Concrete Encasement              |     | CU YD |         | 5.0   |

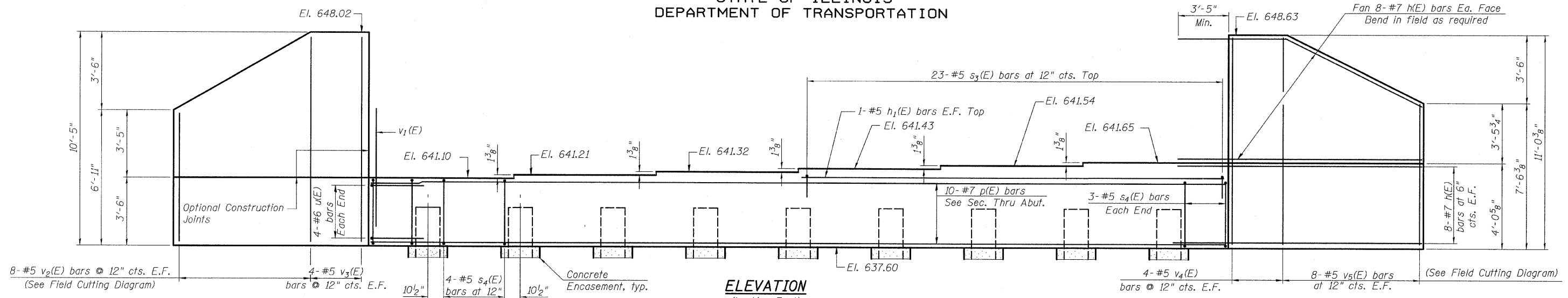
**WEST ABUTMENT  
STRUCTURE NO. 006-0174 (EB)**

|          |       |
|----------|-------|
| DESIGNED | IM/AD |
| CHECKED  | PDF   |
| DRAWN    | IM    |
| CHECKED  | PDF   |

|                           |                    |                           |                  |                     |                  |
|---------------------------|--------------------|---------------------------|------------------|---------------------|------------------|
| SHEET NO. 25<br>39 SHEETS | F.A. RTE.<br>80    | SECTION<br>*              | COUNTY<br>BUREAU | TOTAL SHEETS<br>344 | SHEET NO.<br>223 |
|                           | CONTRACT NO. 66908 |                           |                  |                     |                  |
| FED. ROAD DIST. NO.       |                    | ILLINOIS FED. AID PROJECT |                  |                     |                  |

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**PILE DATA**

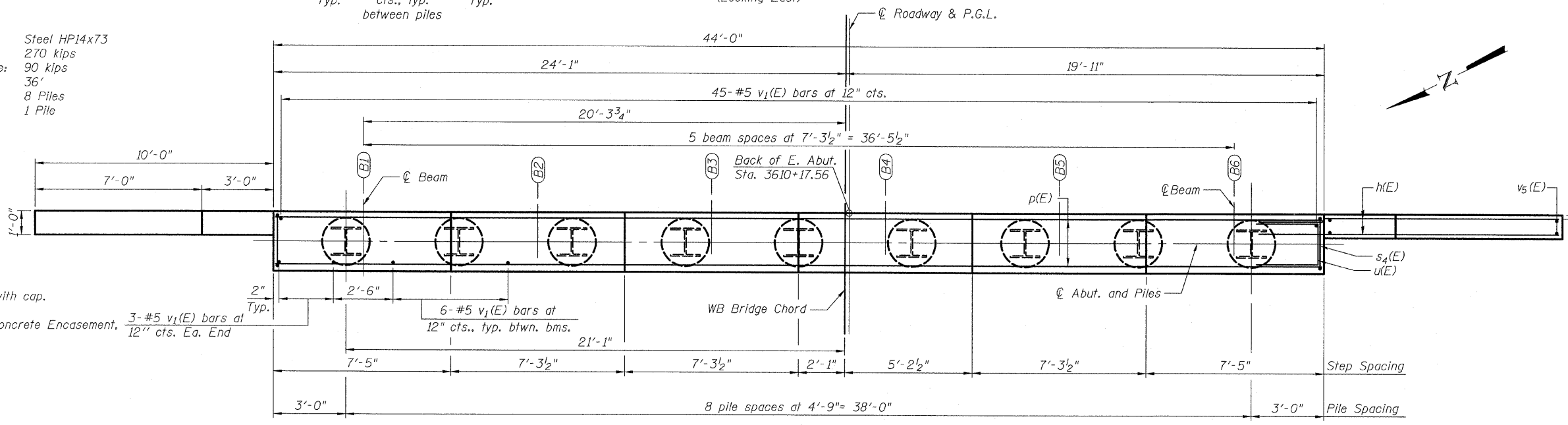
Type: Steel HP14x73  
 Nominal Required Bearing: 270 kips  
 Allowable Resistance Available: 90 kips  
 Est. Length: 36'  
 No. Production Piles: 8 Piles  
 No. Test Piles: 1 Pile

**LAP SPLICES**

| Bar | Lap   |
|-----|-------|
| #4  | 1'-8" |
| #5  | 2'-2" |
| #6  | 2'-7" |
| #7  | 3'-5" |
| #8  | 4'-6" |

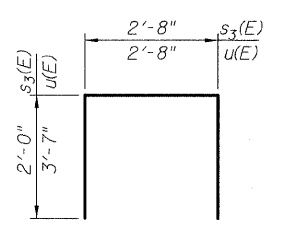
**NOTES:**

1. Pour steps monolithically with cap.
2. For details of piles and Concrete Encasement, see sheet 33 of 39.

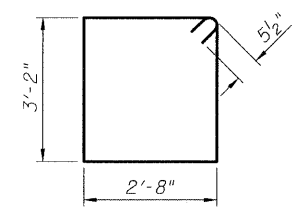


**LEGEND**

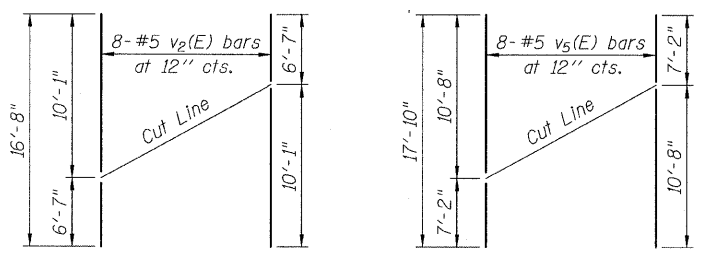
E.F. - Denotes Each Face.



**BARS s<sub>3</sub>(E) & u(E)**



**BAR s<sub>4</sub>(E)**



**FIELD CUTTING DIAGRAMS**

Order bars full length. Cut as shown and use remainder of bars in opposite face.

**BILL OF MATERIAL**

| Bar                | No. | Size | Length | Shape |
|--------------------|-----|------|--------|-------|
| h(E)               | 64  | #7   | 13'-3" | —     |
| h <sub>1</sub> (E) | 2   | #5   | 21'-8" | —     |
| p(E)               | 10  | #7   | 43'-8" | —     |
| s <sub>3</sub> (E) | 23  | #5   | 6'-8"  | □     |
| s <sub>4</sub> (E) | 38  | #5   | 12'-7" | □     |
| u(E)               | 8   | #6   | 9'-10" | □     |
| v <sub>1</sub> (E) | 81  | #5   | 4'-4"  | —     |
| v <sub>2</sub> (E) | 8   | #5   | 16'-8" | —     |
| v <sub>3</sub> (E) | 8   | #5   | 10'-1" | —     |
| v <sub>4</sub> (E) | 8   | #5   | 10'-8" | —     |

**BILL OF MATERIAL**

| Bar                              | No. | Size  | Length  | Shape |
|----------------------------------|-----|-------|---------|-------|
| v <sub>5</sub> (E)               | 8   | #5    | 17'-10" | —     |
| Structure Excavation             |     | CU YD | 78      |       |
| Concrete Structures              |     | CU YD | 25.4    |       |
| Reinforcement Bars, Epoxy Coated |     | POUND | 4280    |       |
| Furnishing Steel Pile, HP14x73   |     | FOOT  | 288     |       |
| Driving Piles                    |     | FOOT  | 288     |       |
| Test Pile Steel, HP14x73         |     | EACH  | 1       |       |
| Concrete Encasement              |     | CU YD | 5.0     |       |

**EAST ABUTMENT  
STRUCTURE NO. 006-0175 (WB)**

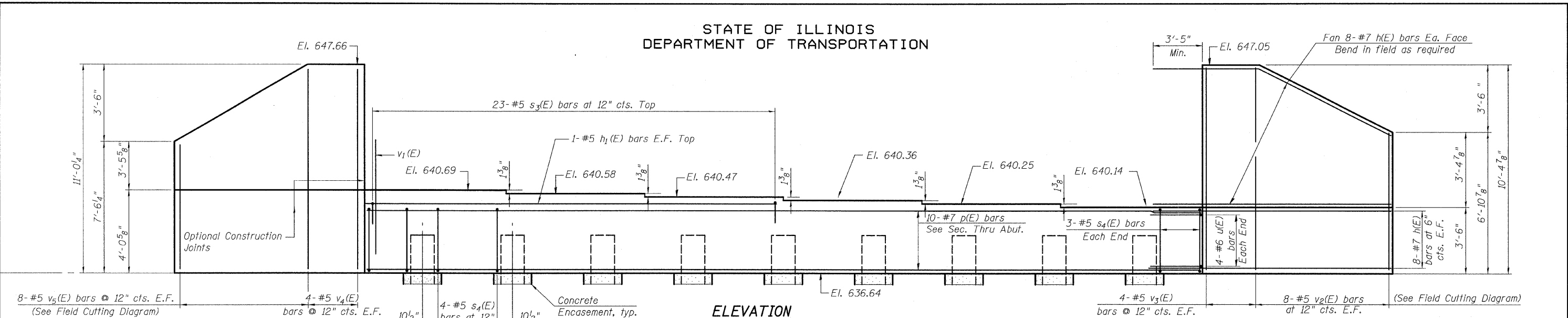
|          |       |
|----------|-------|
| DESIGNED | IM/AD |
| CHECKED  | PDF   |
| DRAWN    | IM    |
| CHECKED  | PDF   |

|                           |                    |                           |                  |                     |                  |
|---------------------------|--------------------|---------------------------|------------------|---------------------|------------------|
| SHEET NO. 26<br>39 SHEETS | F.A. RTE.<br>80    | SECTION<br>*              | COUNTY<br>BUREAU | TOTAL SHEETS<br>344 | SHEET NO.<br>224 |
|                           | CONTRACT NO. 66908 |                           |                  |                     |                  |
| FED. ROAD DIST. NO.       |                    | ILLINOIS FED. AID PROJECT |                  |                     |                  |

TYLINT INTERNATIONAL



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**PILE DATA**

Type: Steel HP14x73  
Nominal Required Bearing: 270 kips  
Allowable Resistance Available: 90 kips  
Est. Length: 49'  
No. Production Piles: 8 Piles  
No. Test Piles: 1 Pile

**NOTES:**

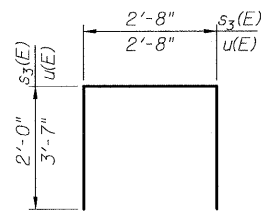
1. Pour steps monolithically with cap.
2. For details of piles and Concrete Encasement, see sheet 33 of 39.

**LAP SPLICES**

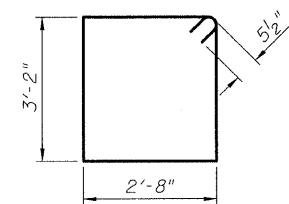
| Bar | Lap   |
|-----|-------|
| #4  | 1'-8" |
| #5  | 2'-2" |
| #6  | 2'-7" |
| #7  | 3'-5" |
| #8  | 4'-6" |

**LEGEND**

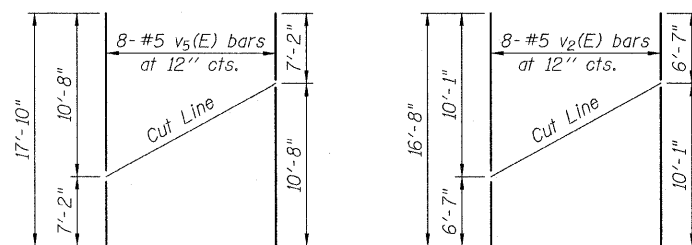
E.F. - Denotes Each Face.



**BARS s<sub>3</sub>(E) & u(E)**



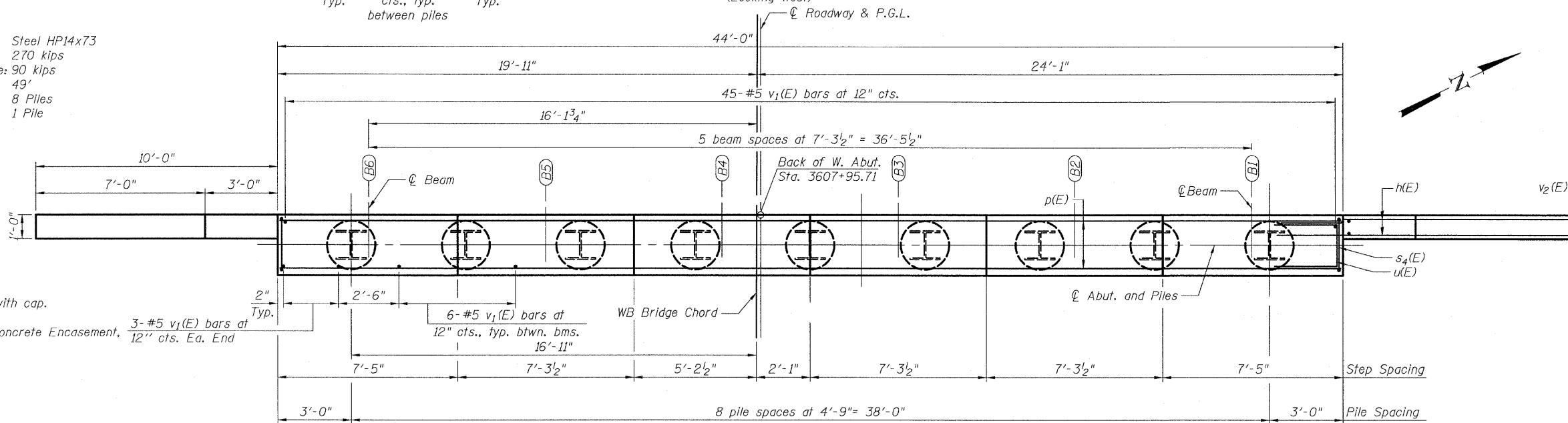
**BARS s<sub>4</sub>(E)**



**FIELD CUTTING DIAGRAMS**

Order bars full length. Cut as shown and use remainder of bars in opposite face.

**ELEVATION**  
(Looking West)



**PLAN**

**BILL OF MATERIAL**

| Bar                | No. | Size | Length | Shape |
|--------------------|-----|------|--------|-------|
| h(E)               | 64  | #7   | 13'-3" | —     |
| h <sub>1</sub> (E) | 2   | #5   | 21'-8" | —     |
| p(E)               | 10  | #7   | 43'-8" | —     |
| s <sub>3</sub> (E) | 23  | #5   | 6'-8"  | □     |
| s <sub>4</sub> (E) | 38  | #5   | 12'-7" | □     |
| u(E)               | 8   | #6   | 9'-10" | □     |
| v <sub>1</sub> (E) | 81  | #5   | 4'-4"  | —     |
| v <sub>2</sub> (E) | 8   | #5   | 16'-8" | —     |
| v <sub>3</sub> (E) | 8   | #5   | 10'-1" | —     |
| v <sub>4</sub> (E) | 8   | #5   | 10'-8" | —     |

**BILL OF MATERIAL**

| Bar                              | No. | Size  | Length  | Shape |
|----------------------------------|-----|-------|---------|-------|
| v <sub>5</sub> (E)               | 8   | #5    | 17'-10" | —     |
| Structure Excavation             |     | CU YD | 78      |       |
| Concrete Structures              |     | CU YD | 25.4    |       |
| Reinforcement Bars, Epoxy Coated |     | POUND | 4280    |       |
| Furnishing Steel Pile, HP14x73   |     | FOOT  | 392     |       |
| Driving Piles                    |     | FOOT  | 392     |       |
| Test Pile Steel, HP14x73         |     | EACH  | 1       |       |
| Concrete Encasement              |     | CU YD | 5.0     |       |

**WEST ABUTMENT**

**STRUCTURE NO. 006-0175 (WB)**

|          |       |
|----------|-------|
| DESIGNED | IM/AD |
| CHECKED  | PDF   |
| DRAWN    | IM    |
| CHECKED  | PDF   |

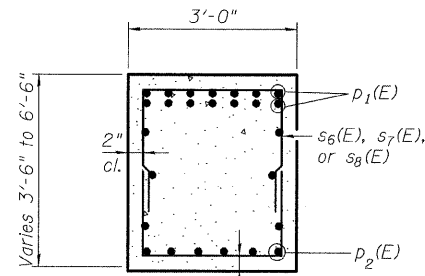
|              |   |           |                    |                  |               |
|--------------|---|-----------|--------------------|------------------|---------------|
| SHEET NO. 27 | F.A. RTE. 80                                  | SECTION * | COUNTY BUREAU      | TOTAL SHEETS 344 | SHEET NO. 225 |
| 39 SHEETS    | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |           | CONTRACT NO. 66908 |                  |               |

TYLINT INTERNATIONAL

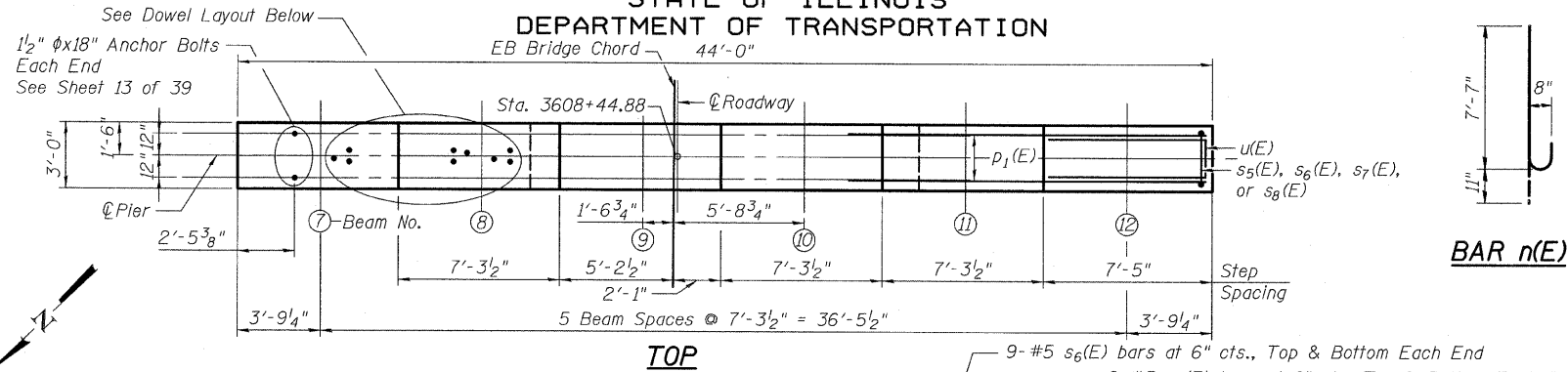
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**BILL OF MATERIAL**

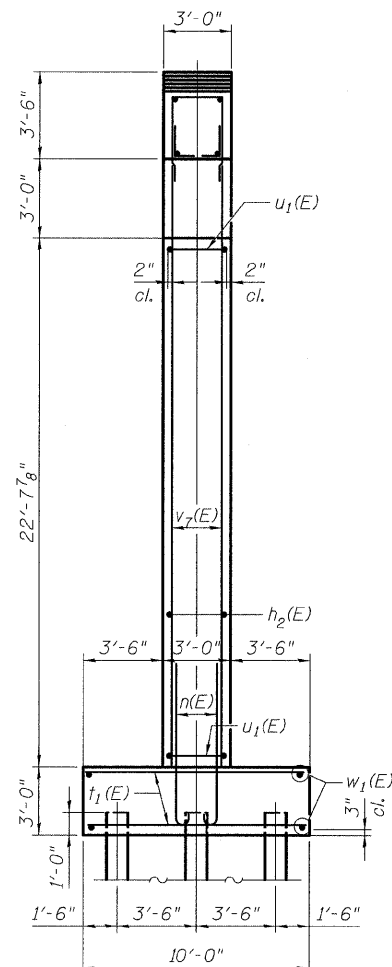
| Bar                              | No. | Size | Length  | Shape |
|----------------------------------|-----|------|---------|-------|
| $h_1(E)$                         | 4   | #5   | 21'-8"  | —     |
| $h_2(E)$                         | 48  | #5   | 17'-2"  | —     |
| $h_3(E)$                         | 6   | #5   | 43'-8"  | —     |
| $h_4(E)$                         | 6   | #5   | 35'-2"  | —     |
| $n(E)$                           | 80  | #8   | 8'-6"   | —     |
| $p_1(E)$                         | 14  | #10  | 43'-8"  | —     |
| $p_2(E)$                         | 12  | #7   | 16'-4"  | —     |
| $s_3(E)$                         | 23  | #5   | 6'-8"   | □     |
| $s_5(E)$                         | 27  | #5   | 11'-8"  | □     |
| $s_6(E)$                         | 36  | #5   | 11'-0"  | □     |
| $s_7(E)$                         | 36  | #5   | 10'-0"  | □     |
| $s_8(E)$                         | 36  | #5   | 9'-0"   | □     |
| $t_1(E)$                         | 70  | #8   | 9'-8"   | —     |
| $u(E)$                           | 6   | #6   | 9'-10"  | —     |
| $u_1(E)$                         | 46  | #5   | 14'-4"  | —     |
| $v_6(E)$                         | 54  | #8   | 26'-8"  | —     |
| $v_7(E)$                         | 26  | #8   | 26'-11" | —     |
| $v_8(E)$                         | 30  | #8   | 4'-2"   | —     |
| $w_1(E)$                         | 20  | #8   | 34'-2"  | —     |
| Reinforcement Bars, Epoxy Coated |     |      | POUND   | 18360 |
| Structure Excavation             |     |      | CU YD   | 189   |
| Concrete Structures              |     |      | CU YD   | 116.7 |
| Furnishing Steel Piles HP14x73   |     |      | Foot    | 1189  |
| Driving Piles                    |     |      | Foot    | 1189  |
| Test Pile Steel HP14x73          |     |      | Each    | 1     |



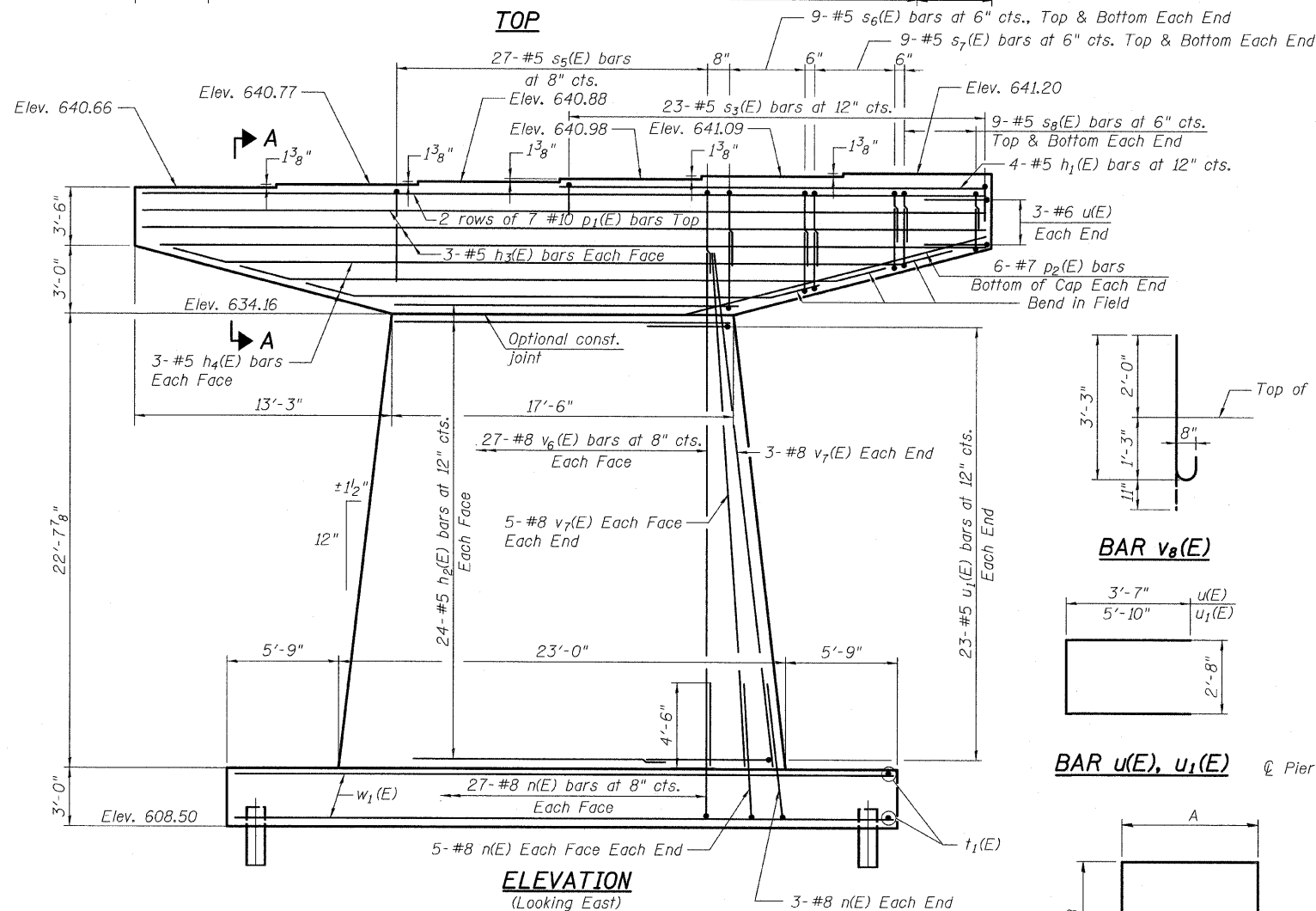
**SECTION A-A**



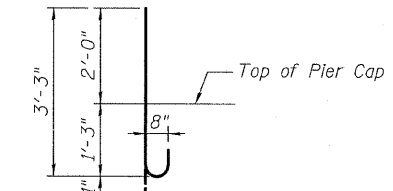
**TOP**



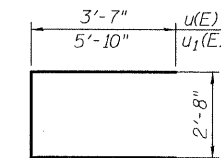
**END VIEW**



**ELEVATION**  
(Looking East)



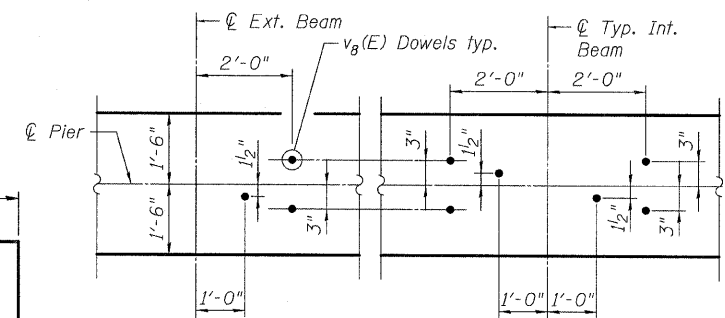
**BAR  $v_8(E)$**



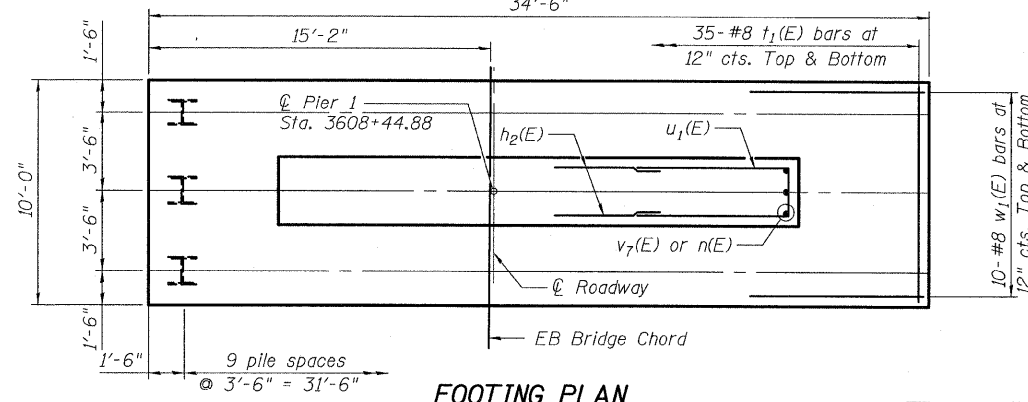
**BAR  $u(E), u_1(E)$**

**BARS  $s_3(E), s_5(E)$  THRU  $s_8(E)$**   
**A & B DIMENSIONS**

| Bar      | A     | B     |
|----------|-------|-------|
| $s_3(E)$ | 2'-8" | 2'-0" |
| $s_5(E)$ | 2'-8" | 4'-6" |
| $s_6(E)$ | 2'-8" | 4'-2" |
| $s_7(E)$ | 2'-8" | 3'-8" |
| $s_8(E)$ | 2'-8" | 3'-2" |



**DOWEL LAYOUT**



**FOOTING PLAN**

**NOTES:**

- Space reinforcement in cap to miss anchor bolts.
- Pour steps monolithically with cap.
- For details of piles, see sheet 32 of 39.

**PILE DATA**

Type: HP 14X73  
Nominal Required Bearing: 270 kips  
Allowable Resistance Available: 90 kips  
Est. Length: 48'  
No. Production Piles: 29 Piles  
No. Test Piles: 1 Pile

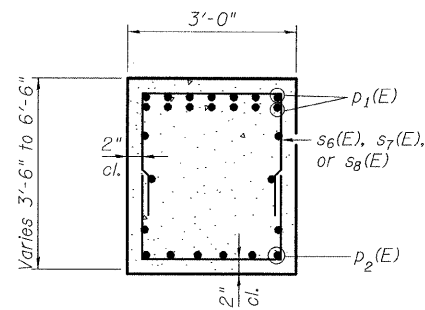
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| CHECKED PDF |
| DRAWN IM    |
| CHECKED PDF |

|                     |           |         |                           |              |           |
|---------------------|-----------|---------|---------------------------|--------------|-----------|
| SHEET NO 28         | F.A. RTE. | SECTION | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| 39 SHEETS           | 80        | *       | BUREAU                    | 344          | 226       |
| FED. ROAD DIST. NO. |           |         | ILLINOIS FED. AID PROJECT |              |           |
| CONTRACT NO. 66908  |           |         |                           |              |           |

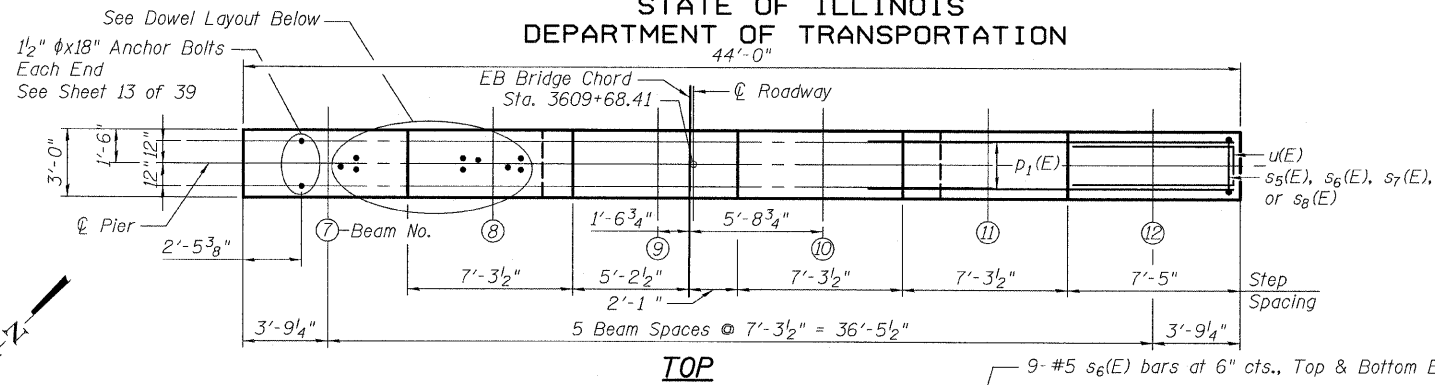
TYLIN INTERNATIONAL

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BILL OF MATERIAL

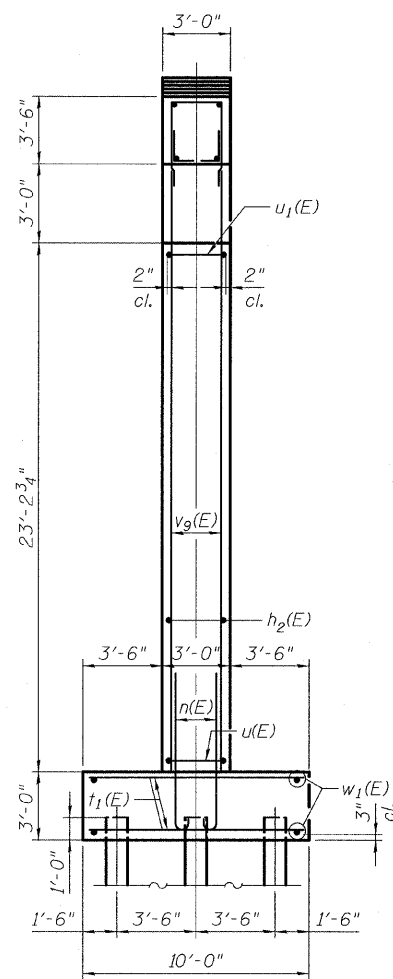


SECTION A-A

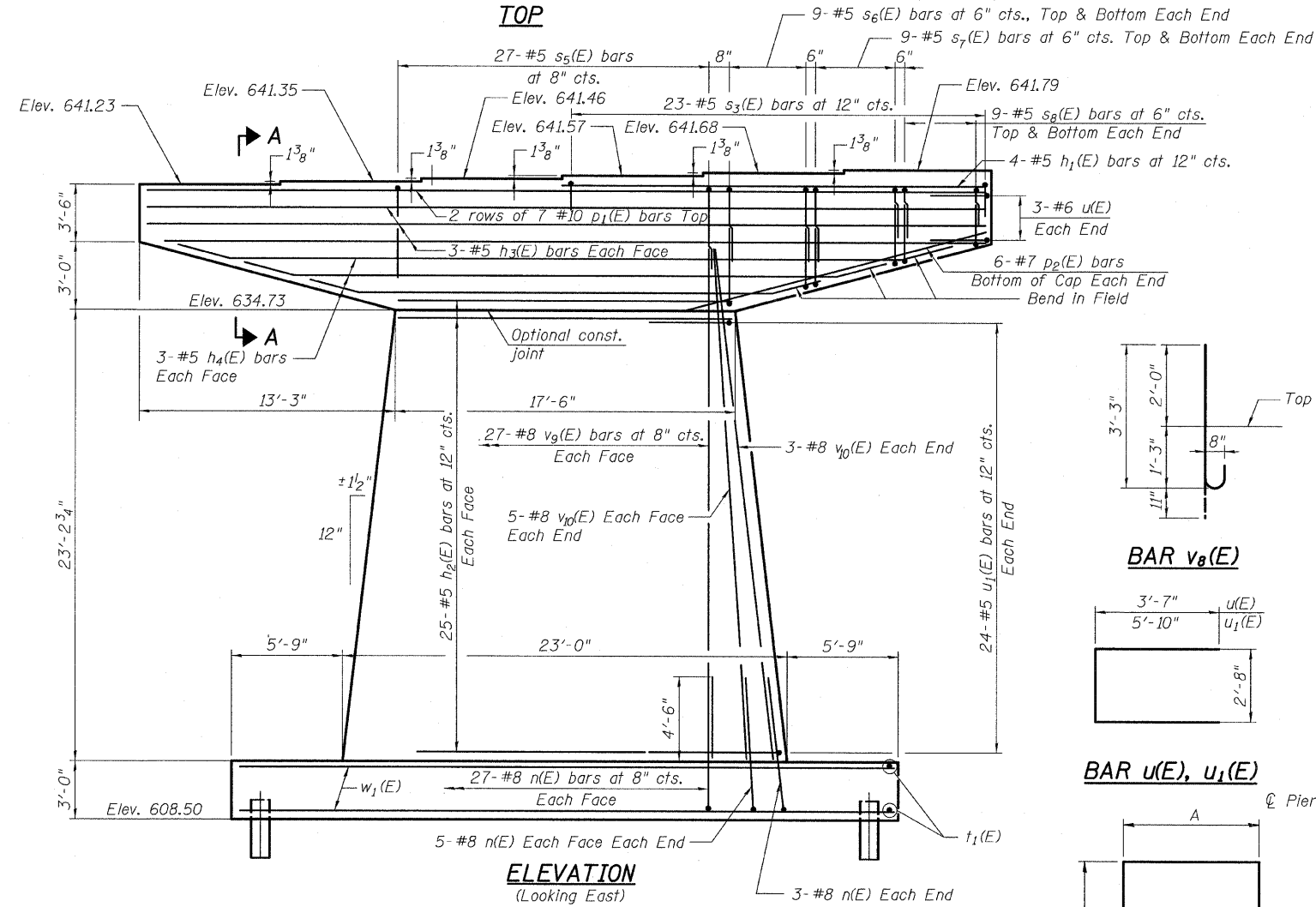


TOP

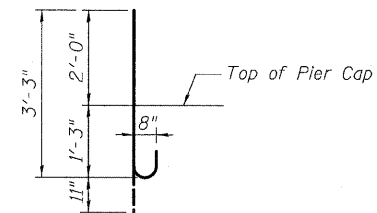
BAR n(E)



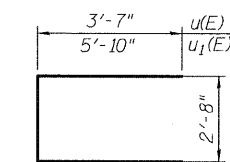
END VIEW



ELEVATION  
(Looking East)



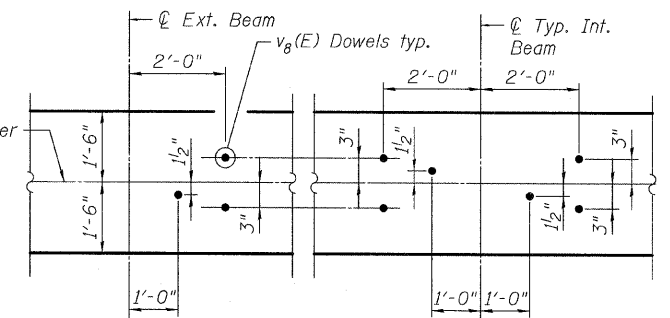
BAR v8(E)



BAR u(E), u1(E)

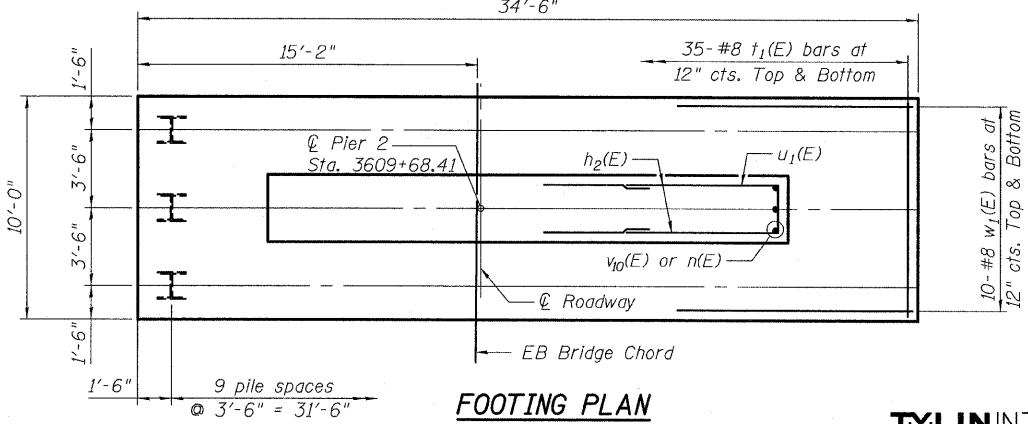
BARS s3(E), s5(E) THRU s8(E)  
A & B DIMENSIONS

| Bar   | A     | B     |
|-------|-------|-------|
| s3(E) | 2'-8" | 2'-0" |
| s5(E) | 2'-8" | 4'-6" |
| s6(E) | 2'-8" | 4'-2" |
| s7(E) | 2'-8" | 3'-8" |
| s8(E) | 2'-8" | 3'-2" |



DOWEL LAYOUT

PIER 2  
STRUCTURE NO. 006-0174 (EB)



FOOTING PLAN

NOTES:

- Space reinforcement in cap to miss anchor bolts.
- Pour steps monolithically with cap.
- For details of piles, see sheet 32 of 39.

PILE DATA

Type: HP 14x73  
Nominal Required Bearing: 270 kips  
Allowable Resistance Available: 90 kips  
Est. Length: 41'  
No. Production Piles: 29 Piles  
No. Test Piles: 1 Pile

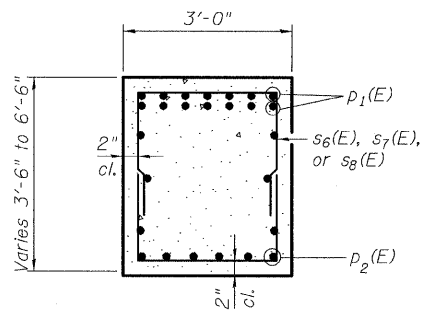
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|----------|-----|
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| CHECKED  | PDF |
| DRAWN    | IM  |
| CHECKED  | PDF |

|                     |              |                           |                    |                 |               |
|---------------------|--------------|---------------------------|--------------------|-----------------|---------------|
| SHEET NO. 29        | F.A. RTE. 80 | SECTION *                 | COUNTY BUREAU      | TOTAL SHEETS 34 | SHEET NO. 227 |
| 39 SHEETS           |              |                           | CONTRACT NO. 66908 |                 |               |
| FED. ROAD DIST. NO. |              | ILLINOIS FED. AID PROJECT |                    |                 |               |

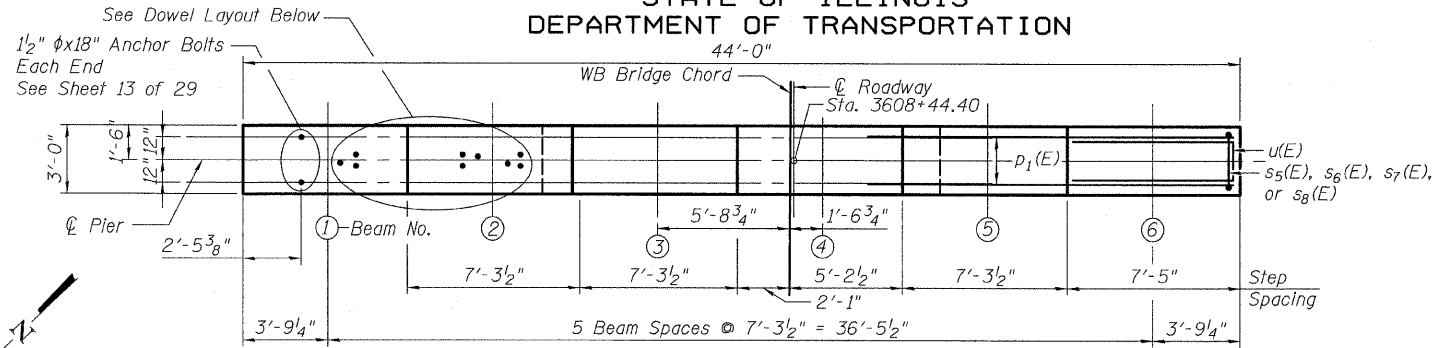
TYLIN INTERNATIONAL

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**BILL OF MATERIAL**

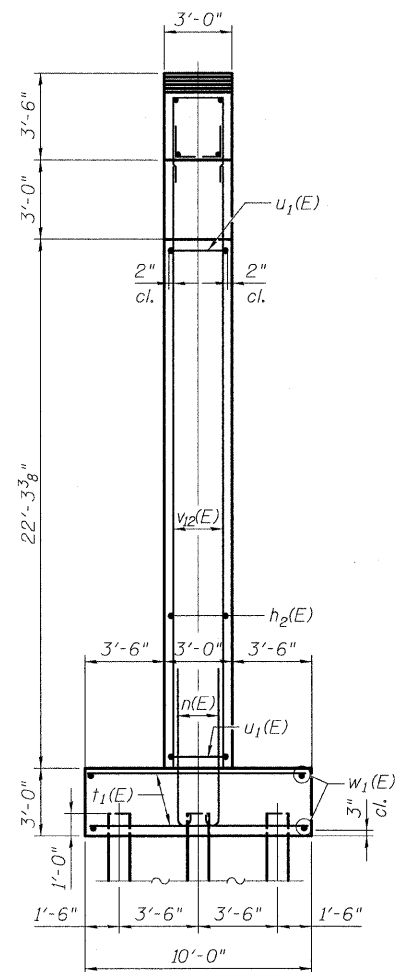


**SECTION A-A**

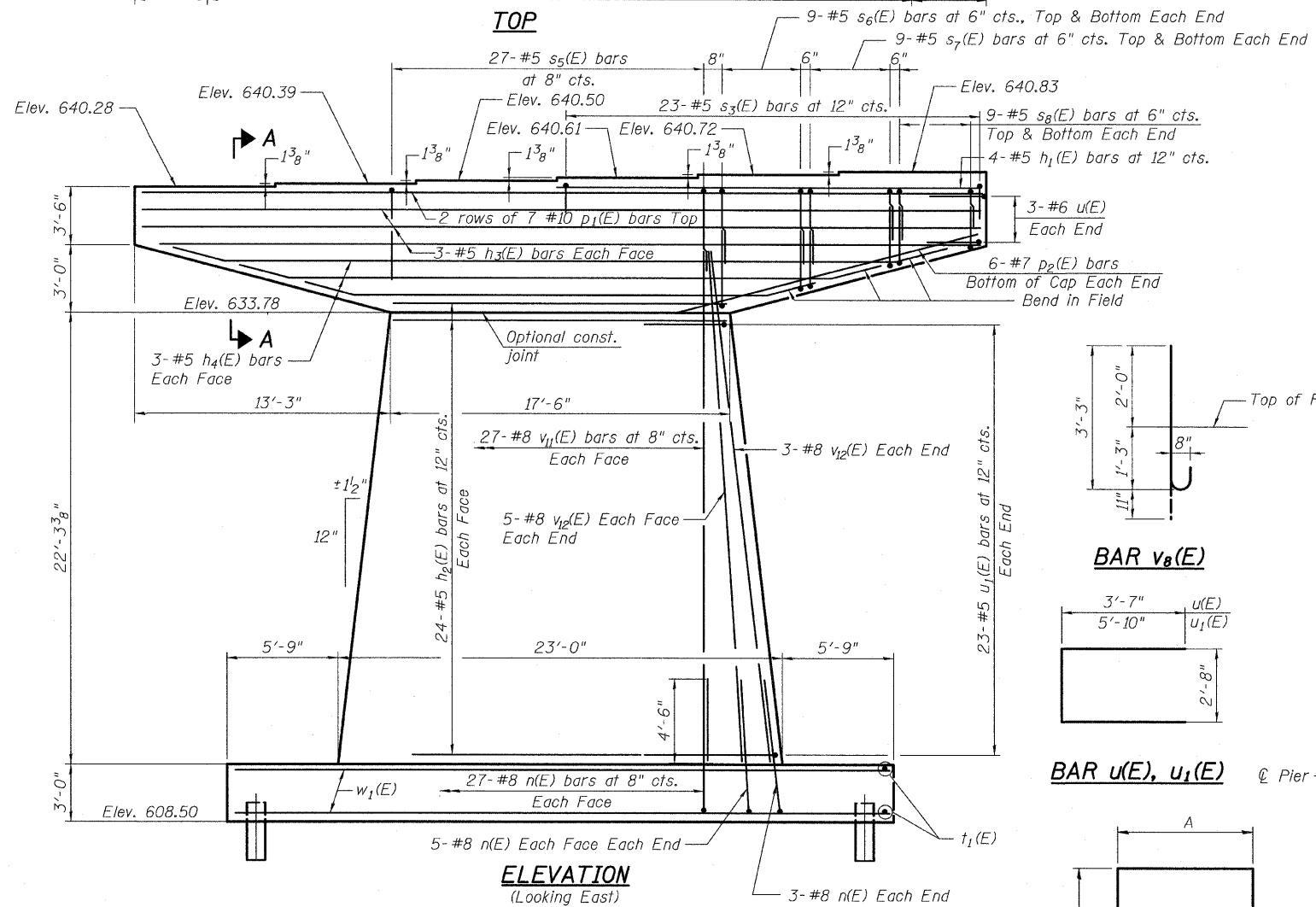


**TOP**

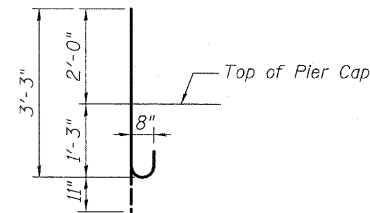
**BAR n(E)**



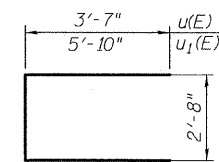
**END VIEW**



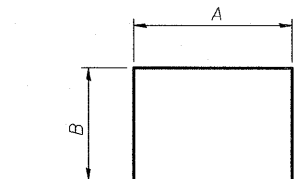
**ELEVATION**  
(Looking East)



**BAR v8(E)**

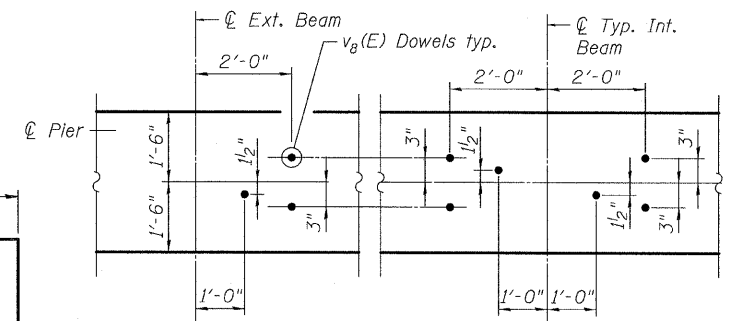


**BAR u(E), u1(E)**



**BARS s3(E), s5(E) THRU s8(E)**

| Bar   | A     | B     |
|-------|-------|-------|
| s3(E) | 2'-8" | 2'-0" |
| s5(E) | 2'-8" | 4'-6" |
| s6(E) | 2'-8" | 4'-2" |
| s7(E) | 2'-8" | 3'-8" |
| s8(E) | 2'-8" | 3'-2" |



**DOWEL LAYOUT**

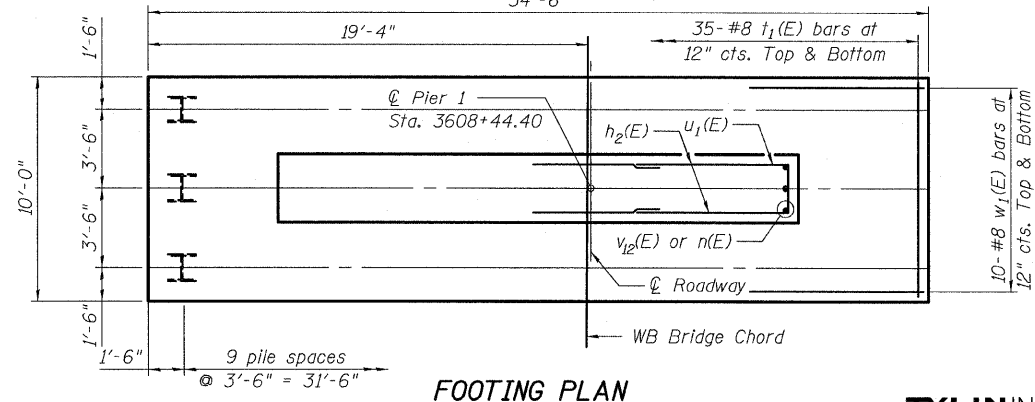
**NOTES:**

1. Space reinforcement in cap to miss anchor bolts.
2. Pour steps monolithically with cap.
3. For details of piles, see sheet 32 of 39.

**PILE DATA**

Type: HP 14X73  
Nominal Required Bearing: 270 kips  
Allowable Resistance Available: 90 kips  
Est. Length: 48'  
No. Production Piles: 29 Piles  
No. Test Piles: 1 Pile

|             |
|-------------|
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| CHECKED PDF |
| DRAWN IM    |
| CHECKED PDF |



**FOOTING PLAN**

**PIER 1**  
**STRUCTURE NO. 006-0175 (WB)**

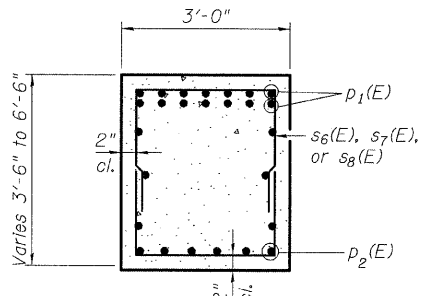
|              |   |           |                    |                 |               |
|--------------|---|-----------|--------------------|-----------------|---------------|
| SHEET NO. 30 | F.A. RTE. 80                                  | SECTION * | COUNTY BUREAU      | TOTAL SHEETS 34 | SHEET NO. 228 |
| 39 SHEETS    | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |           | CONTRACT NO. 66908 |                 |               |

TYLIN INTERNATIONAL

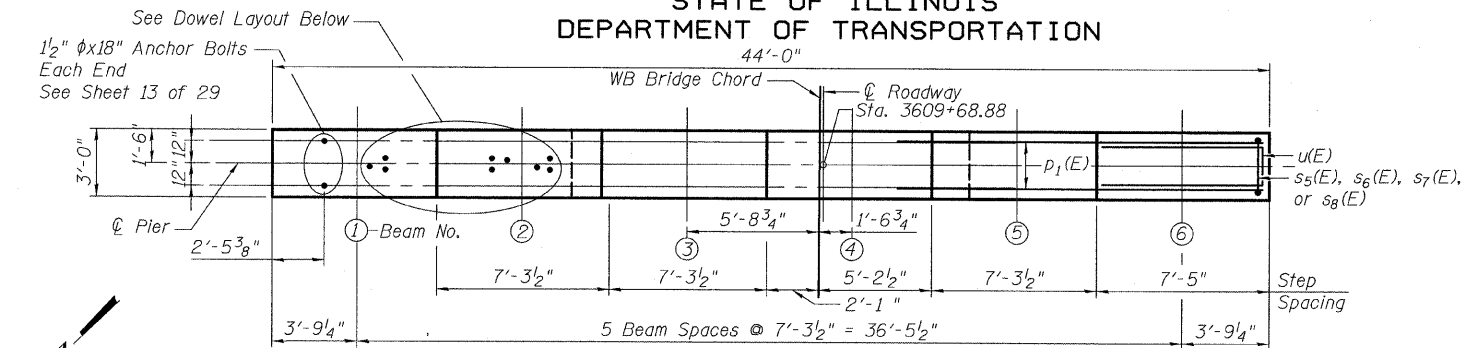
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BILL OF MATERIAL

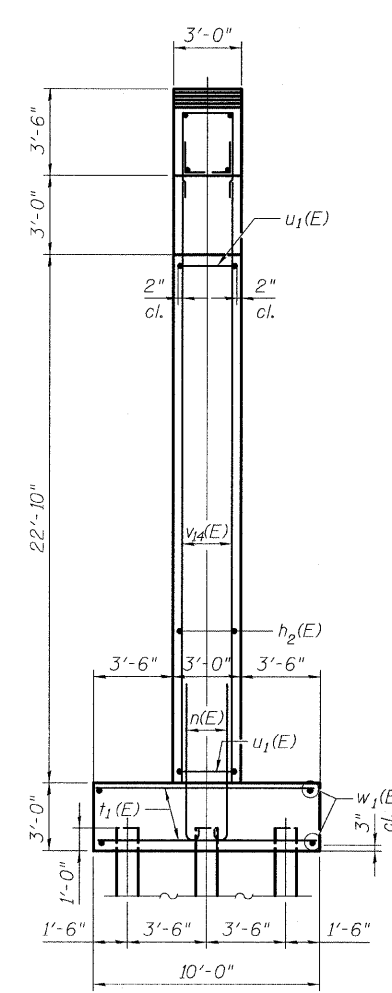
| Bar                              | No. | Size  | Length  | Shape |
|----------------------------------|-----|-------|---------|-------|
| $h_1(E)$                         | 4   | #5    | 21'-8"  | —     |
| $h_2(E)$                         | 50  | #5    | 17'-2"  | —     |
| $h_3(E)$                         | 6   | #5    | 43'-8"  | —     |
| $h_4(E)$                         | 6   | #5    | 35'-2"  | —     |
| $n(E)$                           | 80  | #8    | 8'-6"   | —     |
| $p_1(E)$                         | 14  | #10   | 43'-8"  | —     |
| $p_2(E)$                         | 12  | #7    | 16'-4"  | —     |
| $s_3(E)$                         | 23  | #5    | 6'-8"   | □     |
| $s_5(E)$                         | 27  | #5    | 11'-8"  | □     |
| $s_6(E)$                         | 36  | #5    | 11'-0"  | □     |
| $s_7(E)$                         | 36  | #5    | 10'-0"  | □     |
| $s_8(E)$                         | 36  | #5    | 9'-0"   | □     |
| $t_1(E)$                         | 70  | #8    | 9'-8"   | —     |
| $u(E)$                           | 6   | #6    | 9'-10"  | —     |
| $u_1(E)$                         | 48  | #5    | 14'-4"  | —     |
| $v_8(E)$                         | 30  | #8    | 4'-2"   | —     |
| $v_{13}(E)$                      | 54  | #8    | 26'-10" | —     |
| $v_{14}(E)$                      | 26  | #8    | 27'-0"  | —     |
| $w_1(E)$                         | 20  | #8    | 34'-2"  | —     |
| Reinforcement Bars, Epoxy Coated |     | POUND | 18460   |       |
| Structure Excavation             |     | CU YD | 189     |       |
| Concrete Structures              |     | CU YD | 117.1   |       |
| Furnishing Steel Piles HP14x73   |     | Foot  | 1,189   |       |
| Driving Piles                    |     | Foot  | 1,189   |       |
| Test Pile Steel HP14x73          |     | Each  | 1       |       |



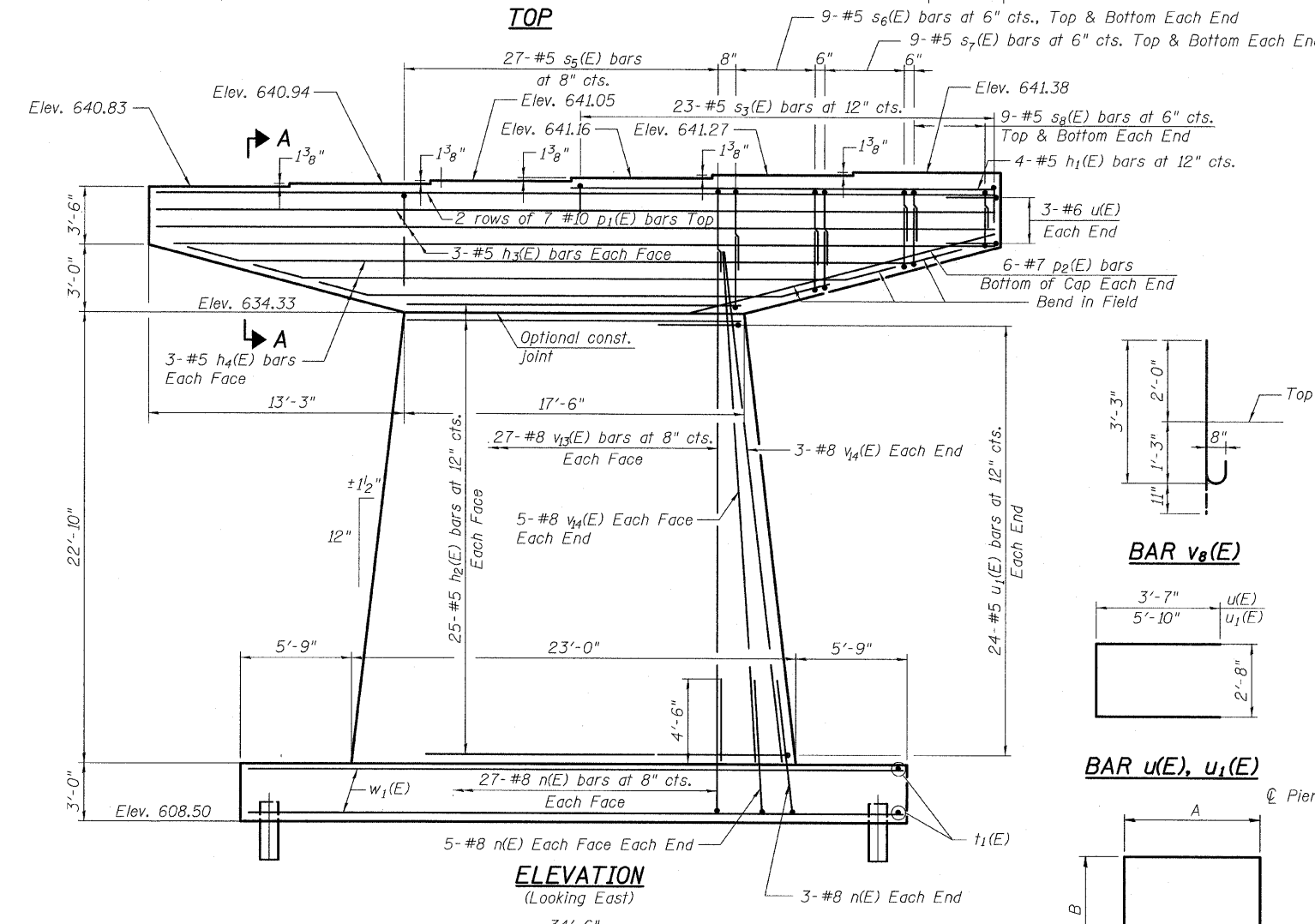
SECTION A-A



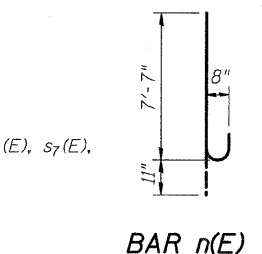
TOP



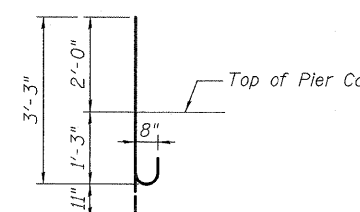
END VIEW



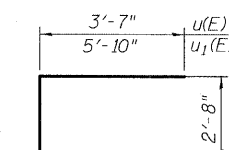
ELEVATION  
(Looking East)



BAR n(E)



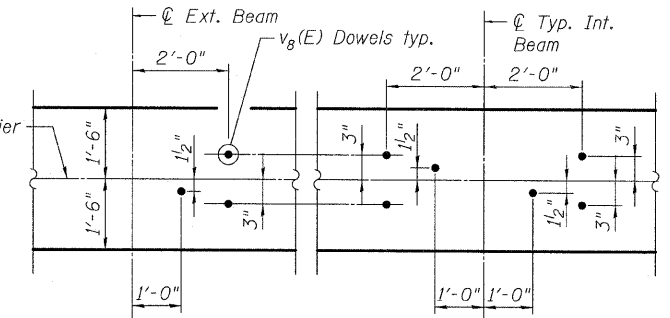
BAR v8(E)



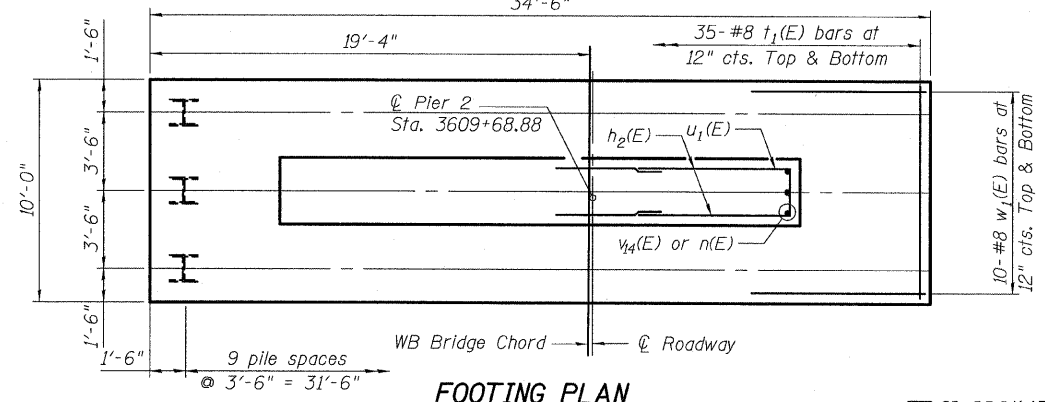
BAR u(E), u1(E)

BARS s3(E), s5(E) THRU s8(E)  
A & B DIMENSIONS

| Bar      | A     | B     |
|----------|-------|-------|
| $s_3(E)$ | 2'-8" | 2'-0" |
| $s_5(E)$ | 2'-8" | 4'-6" |
| $s_6(E)$ | 2'-8" | 4'-2" |
| $s_7(E)$ | 2'-8" | 3'-8" |
| $s_8(E)$ | 2'-8" | 3'-2" |



DOWEL LAYOUT



FOOTING PLAN

NOTES:

- Space reinforcement in cap to miss anchor bolts.
- Pour steps monolithically with cap.
- For details of piles, see sheet 32 of 39.

PILE DATA

Type: HP 14X73  
Nominal Required Bearing: 270 kips  
Allowable Resistance Available: 90 kips  
Est. Length: 41'  
No. Production Piles: 29 Piles  
No. Test Piles: 1 Pile

|             |
|-------------|
| DESIGNED AD |
| CHECKED PDF |
| DRAWN IM    |
| CHECKED PDF |

|   |              |           |                    |                  |               |
|---|--------------|-----------|--------------------|------------------|---------------|
| SHEET NO. 31                                  | F.A. RTE. 80 | SECTION * | COUNTY BUREAU      | TOTAL SHEETS 344 | SHEET NO. 229 |
| 39 SHEETS                                     |              |           | CONTRACT NO. 66908 |                  |               |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |              |           |                    |                  |               |

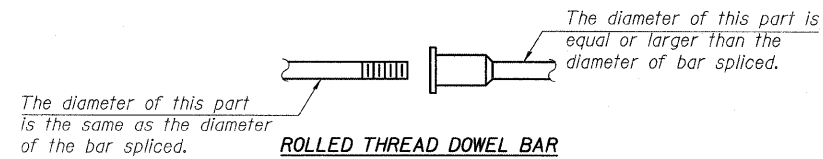
TYLIN INTERNATIONAL

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**NOTES**

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.  
Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.  
All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.  
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.  
Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

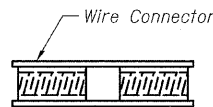
- ① Minimum Capacity (Tension in kips) =  $1.25 \times f_y \times A_t$
  - ② Minimum \*Pull-out Strength (Tension in kips) =  $0.66 \times f_y \times A_t$
- Where  $f_y$  = Yield strength of lapped reinforcement bars in ksi.  
 $A_t$  = Tensile stress area of lapped reinforcement bars.  
\* = 28 day concrete



**ROLLED THREAD DOWEL BAR**



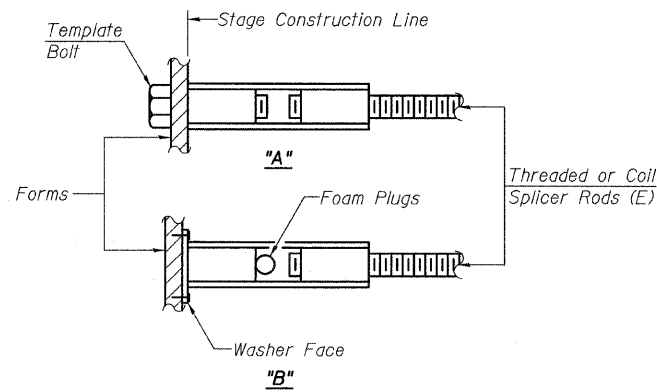
**\*\* ONE PIECE**



**WELDED SECTIONS**

**BAR SPLICER ASSEMBLY ALTERNATIVES**

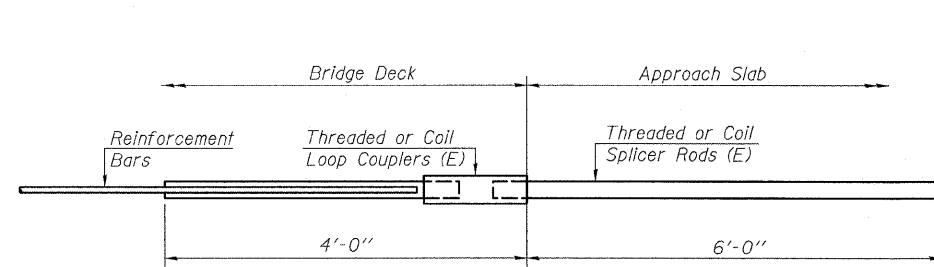
\*\*Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



**INSTALLATION AND SETTING METHODS**

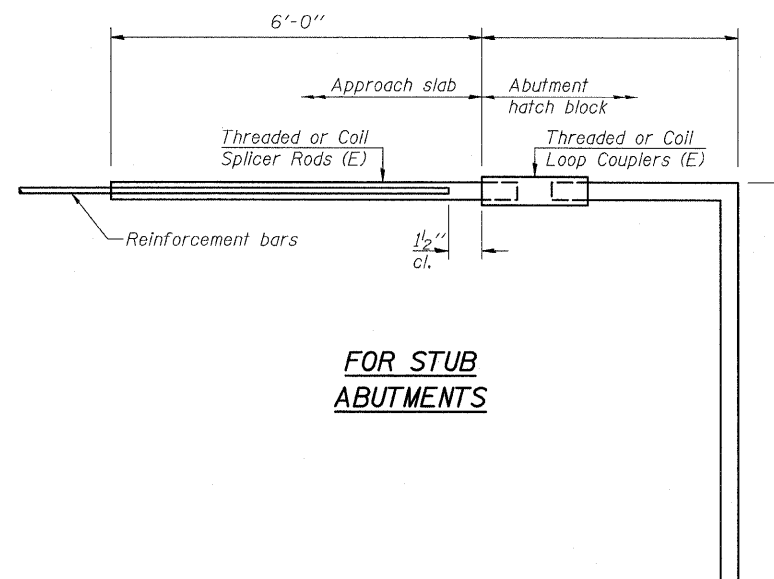
"A" : Set bar splicer assembly by means of a template bolt.  
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
(E) : Indicates epoxy coating.

| BAR SPLICER ASSEMBLIES |                                 |                              |                                       |
|------------------------|---------------------------------|------------------------------|---------------------------------------|
| Bar Size to be Spliced | Splicer Rod or Dowel Bar Length | Strength Requirements        |                                       |
|                        |                                 | Min. Capacity kips - tension | Min. Pull-Out Strength kips - tension |
| #4                     | 1'-8"                           | 14.7                         | 7.9                                   |
| #5                     | 2'-2"                           | 23.0                         | 12.3                                  |
| #6                     | 2'-7"                           | 33.1                         | 17.4                                  |
| #7                     | 3'-5"                           | 45.1                         | 23.8                                  |
| #8                     | 4'-6"                           | 58.9                         | 31.3                                  |
| #9                     | 5'-9"                           | 75.0                         | 39.6                                  |
| #10                    | 7'-3"                           | 95.0                         | 50.3                                  |
| #11                    | 9'-0"                           | 117.4                        | 61.8                                  |



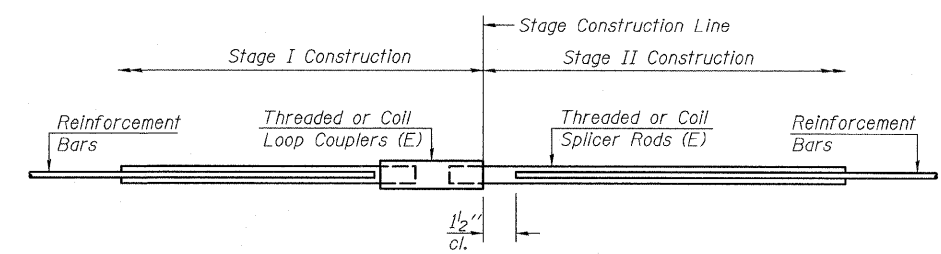
**FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

|  |
|--|
| Bar Splicer for #5 bar                       |
| Min. Capacity = 23.0 kips - tension          |
| Min. Pull-out Strength = 12.3 kips - tension |
| No. Required = 84 (EB), 84 (WB)              |



**FOR STUB ABUTMENTS**

|  |
|--|
| Bar Splicer for #5 bar                       |
| Min. Capacity = 23.0 kips - tension          |
| Min. Pull-out Strength = 12.3 kips - tension |
| No. Required =                               |



**STANDARD**

| Bar Size | No. Assemblies Required | Location |
|----------|-------------------------|----------|
|          |                         |          |
|          |                         |          |
|          |                         |          |
|          |                         |          |

|          |    |
|----------|----|
| DESIGNED | SP |
| CHECKED  | DF |
| DRAWN    | SP |
| CHECKED  | DF |

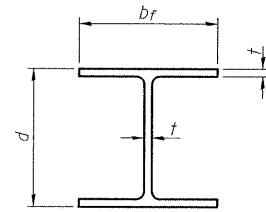
**BAR SPLICER ASSEMBLY DETAILS**

STRUCTURE NO. 006-0174 (EB)  
STRUCTURE NO. 006-0175 (WB)

|                    |                     |         |                           |              |           |
|--------------------|---------------------|---------|---------------------------|--------------|-----------|
| SHEET NO. 32       | F.A. RTE.           | SECTION | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|                    | 80                  | *       | BUREAU                    | 344          | 236       |
| 39 SHEETS          | FED. ROAD DIST. NO. |         | ILLINOIS FED. AID PROJECT |              |           |
| CONTRACT NO. 66908 |                     |         |                           |              |           |

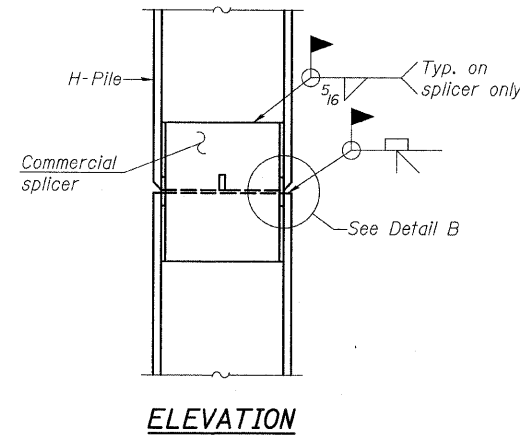
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DEPARTMENT OF TRANSPORTATION

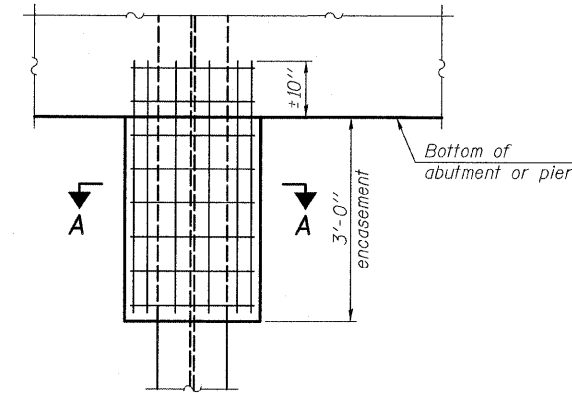


STEEL PILE TABLE

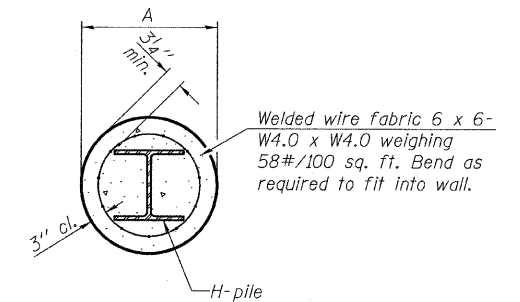
| Designation | Depth d | Flange width br | Web and Flange thickness t | Encasement diameter A |
|-------------|---------|-----------------|----------------------------|-----------------------|
| HP 14x117   | 14 1/4" | 14 7/8"         | 13/16"                     | 30"                   |
| x102        | 14"     | 14 3/4"         | 1/4"                       | 30"                   |
| x89         | 13 7/8" | 14 3/4"         | 5/8"                       | 30"                   |
| x73         | 13 5/8" | 14 5/8"         | 1/2"                       | 30"                   |
| HP 12x84    | 12 1/4" | 12 1/4"         | 1/16"                      | 24"                   |
| x74         | 12 1/8" | 12 1/4"         | 5/8"                       | 24"                   |
| x63         | 12"     | 12 1/8"         | 1/2"                       | 24"                   |
| x53         | 11 3/4" | 12"             | 7/16"                      | 24"                   |
| HP 10x57    | 10"     | 10 1/4"         | 9/16"                      | 24"                   |
| x42         | 9 3/4"  | 10 1/8"         | 7/16"                      | 24"                   |
| HP 8x36     | 8"      | 8 1/8"          | 7/16"                      | 18"                   |



ELEVATION



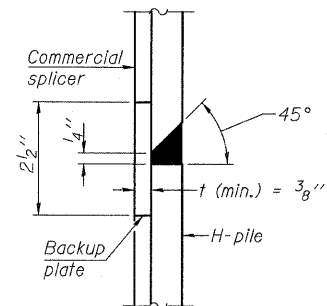
ELEVATION



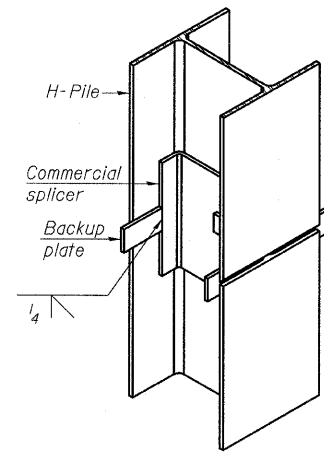
SECTION A-A

Note:  
Forms for encasement may be omitted when soil conditions permit.

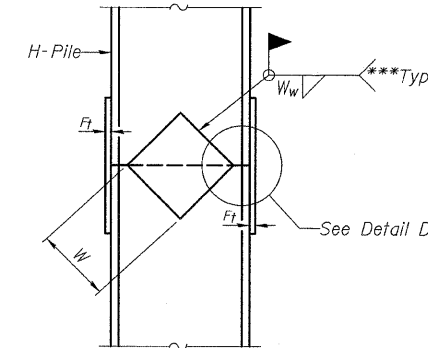
PILE ENCASUREMENT



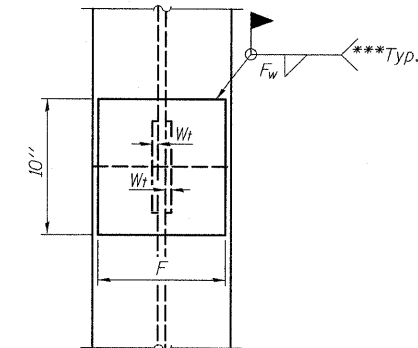
DETAIL "B"



ISOMETRIC VIEW

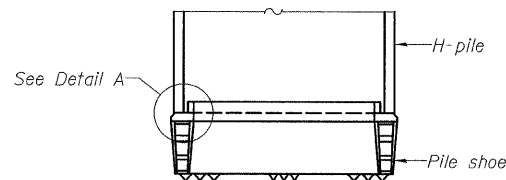


ELEVATION

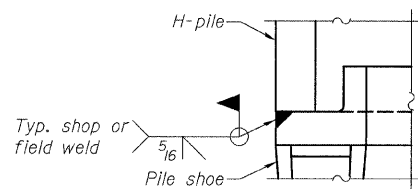


END VIEW

| Designation | F       | F <sub>t</sub> | F <sub>w</sub> | W      | W <sub>t</sub> | W <sub>w</sub> |
|-------------|---------|----------------|----------------|--------|----------------|----------------|
| HP 14x117   | 12 1/2" | 1"             | 7/8"           | 7 3/4" | 5/8"           | 1/2"           |
| x102        | 12 1/2" | 7/8"           | 3/4"           | 7 3/4" | 5/8"           | 1/2"           |
| x89         | 12 1/2" | 3/4"           | 11/16"         | 7 3/4" | 5/8"           | 1/2"           |
| x73         | 12 1/2" | 5/8"           | 9/16"          | 7 3/4" | 5/8"           | 1/2"           |
| HP 12x84    | 10"     | 7/8"           | 11/16"         | 6 1/2" | 5/8"           | 1/2"           |
| x74         | 10"     | 7/8"           | 11/16"         | 6 1/2" | 5/8"           | 1/2"           |
| x63         | 10"     | 5/8"           | 1/2"           | 6 1/2" | 1/2"           | 3/8"           |
| x53         | 10"     | 5/8"           | 1/2"           | 6 1/2" | 1/2"           | 3/8"           |
| HP 10x57    | 8"      | 3/4"           | 9/16"          | 5 1/4" | 1/2"           | 3/8"           |
| x42         | 8"      | 5/8"           | 9/16"          | 5 1/4" | 1/2"           | 3/8"           |
| HP 8x36     | 7"      | 5/8"           | 7/16"          | 4 1/4" | 1/2"           | 3/8"           |

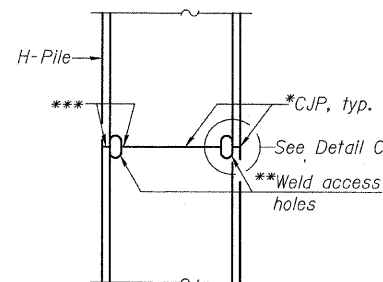


ELEVATION

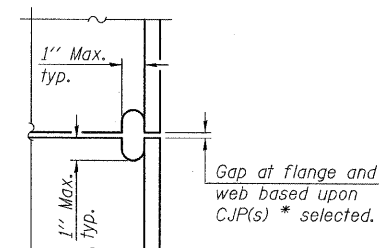


DETAIL A

H-PILE SHOE ATTACHMENT

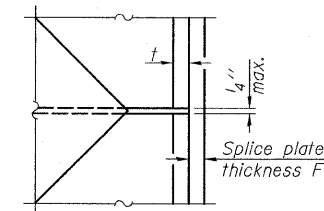


ELEVATION



DETAIL C

COMPLETE PENETRATION WELD SPLICE



DETAIL D

WELDED PLATE FIELD SPLICE

|          |     |
|----------|-----|
| DESIGNED | SP  |
| CHECKED  | PDF |
| DRAWN    | SP  |
| CHECKED  | PDF |

- \* Use joint conforming to Figure 3.4 in AWS D1.1, Structure Welding Code - Steel.
- \*\* Preparation per Fig. 5.2 in AWS D1.1, Structure Welding Code - Steel.
- \*\*\* Interrupt welds 1/4" from end of each pile.

Note:  
The steel H-piles shall be according to AASHTO M270 Grade 50.

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STEEL H PILE DETAILS  
STRUCTURE NO. 006-0174 (EB)  
STRUCTURE NO. 006-0175 (WB)

|                     |                    |                           |        |              |           |
|---------------------|--------------------|---------------------------|--------|--------------|-----------|
| SHEET NO. 33        | F.A. RTE.          | SECTION                   | COUNTY | TOTAL SHEETS | SHEET NO. |
|                     | 80                 | *                         | BUREAU | 344          | 231       |
| 39 SHEETS           | CONTRACT NO. 66908 |                           |        |              |           |
| FED. ROAD DIST. NO. |                    | ILLINOIS FED. AID PROJECT |        |              |           |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**SOIL BORING LOG**

Page 1 of 2

ROUTE FAI 80 DESCRIPTION P92-038-03 I-80 over Negro Creek, 1.75 m. E. of IL 89 Date 3/8/05  
SECTION 06-7B-1R&06-7VB LOCATION Hall Twp. - 17 NW, SEC. , TWP. 16N, RNG. 11E LOGGED BY W. Garza  
COUNTY Bureau DRILLING METHOD Hollow Stem Auger HAMMER TYPE B-53 Diedrich Automatic

| STRUCT. NO.<br>Station | DEPTH<br>H | BLOW<br>S | UNCONSOLIDATED<br>Qu | MOISTURE<br>T | Surface Water Elev.<br>Stream Bed Elev. | DEPTH<br>H | BLOW<br>S | UNCONSOLIDATED<br>Qu | MOISTURE<br>T |
|------------------------|------------|-----------|----------------------|---------------|---|------------|-----------|----------------------|---------------|
|                        |            |           |                      |               |   |            |           |                      |               |
| 602.00                 |            |           |                      |               | 560.0 ft                                | 5          |           |                      |               |
|                        |            |           |                      |               | 558.5 ft                                | 6          | 2.7       | 23                   |               |
|                        |            |           |                      |               |   | 8          | B         |                      |               |
| 600.00                 |            |           |                      |               |   | 5          |           |                      |               |
|                        |            |           |                      |               |   | 7          | 5.0       | 13                   |               |
|                        |            |           |                      |               |   | 12         | B         |                      |               |
| 598.00                 |            |           |                      |               |   | 7          |           |                      |               |
|                        |            |           |                      |               |   | 11         | 5.4       | 12                   |               |
|                        |            |           |                      |               |   | 17         | B         |                      |               |
| 595.50                 |            |           |                      |               |   | 9          |           |                      |               |
|                        |            |           |                      |               |   | 9          | 5.7       | 12                   |               |
|                        |            |           |                      |               |   | 15         | S         |                      |               |
| 593.00                 |            |           |                      |               |   | 9          |           |                      |               |
|                        |            |           |                      |               |   | 11         | 6.4       | 12                   |               |
|                        |            |           |                      |               |   | 15         | B         |                      |               |
| 590.50                 |            |           |                      |               |   | 8          |           |                      |               |
|                        |            |           |                      |               |   | 12         | 7.4       | 14                   |               |
|                        |            |           |                      |               |   | 19         | B         |                      |               |
| 588.00                 |            |           |                      |               |   | 12         |           |                      |               |
|                        |            |           |                      |               |   | 16         |           |                      |               |
|                        |            |           |                      |               |   | 22         |           |                      |               |
| 585.50                 |            |           |                      |               |   | 10         |           |                      |               |
|                        |            |           |                      |               |   | 14         |           |                      |               |
|                        |            |           |                      |               |   | 17         |           |                      |               |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)  
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)  
BBS, from 137 (Rev. 8-99)



**SOIL BORING LOG**

Page 2 of 2

ROUTE FAI 80 DESCRIPTION P92-038-03 I-80 over Negro Creek, 1.75 m. E. of IL 89 Date 3/8/05  
SECTION 06-7B-1R&06-7VB LOCATION Hall Twp. - 17 NW, SEC. , TWP. 16N, RNG. 11E LOGGED BY W. Garza  
COUNTY Bureau DRILLING METHOD Hollow Stem Auger HAMMER TYPE B-53 Diedrich Automatic

| STRUCT. NO.<br>Station | DEPTH<br>H | BLOW<br>S | UNCONSOLIDATED<br>Qu | MOISTURE<br>T | Surface Water Elev.<br>Stream Bed Elev. | DEPTH<br>H | BLOW<br>S | UNCONSOLIDATED<br>Qu | MOISTURE<br>T |
|------------------------|------------|-----------|----------------------|---------------|---|------------|-----------|----------------------|---------------|
|                        |            |           |                      |               |   |            |           |                      |               |
| 563.00                 |            |           |                      |               | 560.0 ft                                | 22         |           |                      |               |
|                        |            |           |                      |               | 558.5 ft                                | 23         |           |                      |               |
|                        |            |           |                      |               |   | 31         |           |                      |               |
| 560.50                 |            |           |                      |               |   | 16         |           |                      |               |
|                        |            |           |                      |               |   | 20         |           |                      |               |
|                        |            |           |                      |               |   | 29         |           |                      |               |
| 558.00                 |            |           |                      |               |   | 6          |           |                      |               |
|                        |            |           |                      |               |   | 16         |           |                      |               |
|                        |            |           |                      |               |   | 38         |           |                      |               |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)  
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)  
BBS, from 137 (Rev. 8-99)

|            |
|------------|
| DESIGNED   |
| CHECKED    |
| DRAWN MAU  |
| CHECKED AD |

BORING LOGS - 1  
STRUCTURE NO. 006-0174 (EB)  
STRUCTURE NO. 006-0175 (WB)

|                           |                    |              |                           |                     |                  |
|---------------------------|--------------------|--------------|---------------------------|---------------------|------------------|
| SHEET NO. 34<br>39 SHEETS | F.A. RTE.<br>80    | SECTION<br>* | COUNTY<br>BUREAU          | TOTAL SHEETS<br>394 | SHEET NO.<br>232 |
|                           | CONTRACT NO. 66908 |              | ILLINOIS FED. AID PROJECT |                     |                  |

TYLINT INTERNATIONAL



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



Illinois Department  
of Transportation  
Division of Highways  
IDOT

SOIL BORING LOG

Page 1 of 2

Date 3/9/05

ROUTE FAI 80 DESCRIPTION P92-038-03 I-80 over Negro Creek, 1.75 m. E. of IL 89 LOGGED BY W. Garza

SECTION 06-7B-1R&06-7VB LOCATION Hall Twp. - 17 NW, SEC., TWP. 16N, RNG. 11E

COUNTY Bureau DRILLING METHOD Hollow Stem Auger HAMMER TYPE B-53 Diedrich Automatic

| STRUCT. NO.                                | Station              | D    | B     | U     | M   | Surface Water Elev.                              | D    | B     | U     | M   |
|--|----------------------|------|-------|-------|-----|--|------|-------|-------|-----|
| BORING NO.                                 | Station              | P    | L     | C     | O   | Stream Bed Elev.                                 | P    | L     | C     | O   |
| Offset                                     | Ground Surface Elev. | H    | S     | Qu    | T   | Groundwater Elev.:                               | H    | S     | Qu    | T   |
| 80.00ft RI CL                              | 605.5 ft             | (ft) | (/6") | (tsf) | (%) | First Encounter                                  | (ft) | (/6") | (tsf) | (%) |
|  |                      |      |       |       |     | Upon Completion                                  |      |       |       |     |
|  |                      |      |       |       |     | Wash   |      |       |       |     |
|  |                      |      |       |       |     | After  |      |       |       |     |
|  |                      |      |       |       |     | Hrs.   |      |       |       |     |
| Rock and mud                               |                      |      |       |       |     | VERY STIFF gray SILTY CLAY TILL                  | 5    |       |       |     |
|  |                      |      |       |       |     |  | 7    | 3.7   | 14    |     |
|  | 603.00               |      |       |       |     |  | 13   | B     |       |     |
| MEDIUM tan/brown dirty SAND & GRAVEL       |                      | 4    |       |       |     | HARD gray SILTY CLAY TILL                        | 9    |       |       |     |
|  | 601.50               | 6    |       |       |     |  | 13   | 4.1   | 14    |     |
|  |                      | 7    |       |       |     |  | 18   | S     |       |     |
| MEDIUM gray clean medium coarse SAND       |                      |      | 5     |       |     | STIFF gray SILT                                  | 11   |       |       |     |
|  | 599.00               | 3    |       |       |     |  | 17   | 1.9   | 17    |     |
|  |                      | 8    |       |       |     |  | 39   | S     |       |     |
| LOOSE/MEDIUM gray clean medium coarse SAND |                      |      | 6     |       |     | LOOSE gray fine SAND                             | 3    |       |       |     |
|  | 596.50               | 4    |       |       |     |  | 2    |       |       |     |
|  |                      | 6    |       |       |     |  | 5    |       |       |     |
| MEDIUM gray fine SAND                      |                      |      | 5     |       |     | MEDIUM gray fine SAND                            | 10   |       |       |     |
|  | 593.50               | 8    |       |       |     |  | 9    |       |       |     |
|  |                      | 11   |       |       |     |  | 11   |       |       |     |
| VERY STIFF gray SILTY CLAY TILL            |                      |      | 3     |       |     | Wash   | 36   |       |       |     |
|  | 591.50               | 3    | 2.5   | 14    |     | DENSE gray fine SAND with medium GRAVEL          | 22   |       |       |     |
|  |                      | 7    | B     |       |     |  | 23   |       |       |     |
| Wash                                       |                      |      |       |       |     |  |      |       |       |     |
| VERY STIFF gray SILTY CLAY TILL            |                      | 3    |       |       |     | Wash   | 13   |       |       |     |
|  | 589.00               | 5    | 2.3   | 15    |     | VERY STIFF gray SILT with CLAY lens              | 20   | 3.5   | 20    |     |
|  |                      | 7    | B     |       |     |  | 24   | S     |       |     |
| VERY STIFF gray SILTY CLAY TILL            |                      |      | 2     |       |     | Wash   | 17   |       |       |     |
|  | 586.50               | 3    | 2.1   | 13    |     | DENSE gray well cemented SAND with medium GRAVEL | 23   |       |       |     |
|  |                      | 6    | B     |       |     |  | 24   |       |       |     |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)  
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, from 137 (Rev. 8-99)



Illinois Department  
of Transportation  
Division of Highways  
IDOT

SOIL BORING LOG

Page 2 of 2

Date 3/9/05

ROUTE FAI 80 DESCRIPTION P92-038-03 I-80 over Negro Creek, 1.75 m. E. of IL 89 LOGGED BY W. Garza

SECTION 06-7B-1R&06-7VB LOCATION Hall Twp. - 17 NW, SEC., TWP. 16N, RNG. 11E

COUNTY Bureau DRILLING METHOD Hollow Stem Auger HAMMER TYPE B-53 Diedrich Automatic

| STRUCT. NO.                             | Station              | D    | B     | U     | M   | Surface Water Elev.                | D    | B     | U     | M   |
|---|----------------------|------|-------|-------|-----|------------------------------------|------|-------|-------|-----|
| BORING NO.                              | Station              | P    | L     | C     | O   | Stream Bed Elev.                   | P    | L     | C     | O   |
| Offset                                  | Ground Surface Elev. | H    | S     | Qu    | T   | Groundwater Elev.:                 | H    | S     | Qu    | T   |
| 80.00ft RI CL                           | 605.5 ft             | (ft) | (/6") | (tsf) | (%) | First Encounter                    | (ft) | (/6") | (tsf) | (%) |
|   |                      |      |       |       |     | Upon Completion                    |      |       |       |     |
|   |                      |      |       |       |     | Wash                               |      |       |       |     |
|   |                      |      |       |       |     | After                              |      |       |       |     |
|   |                      |      |       |       |     | Hrs.                               |      |       |       |     |
| Wash                                    |                      |      |       |       |     | MEDIUM gray medium coarse SAND     | 10   |       |       |     |
|   |                      |      |       |       |     |                                    | 13   |       |       |     |
|   | 564.00               |      |       |       |     |                                    | 15   |       |       |     |
| Wash                                    |                      |      |       |       |     | VERY DENSE gray well-cemented SAND | 23   |       |       |     |
|   |                      |      |       |       |     |                                    | 32   |       |       |     |
|   | 561.50               |      |       |       |     |                                    | 39   |       |       |     |
| VERY DENSE gray well-cemented fine SAND |                      |      |       |       |     |                                    |      |       |       |     |
|   | 559.00               |      |       |       |     |                                    | 20   |       |       |     |
|   |                      |      |       |       |     |                                    | 31   |       |       |     |
|   |                      |      |       |       |     |                                    | 65   |       |       |     |
| End of Boring                           |                      |      |       |       |     |                                    |      |       |       |     |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)  
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, from 137 (Rev. 8-99)

|          |     |
|----------|-----|
| DESIGNED |     |
| CHECKED  |     |
| DRAWN    | MAU |
| CHECKED  | AD  |

BORING LOGS - 2  
STRUCTURE NO. 006-0174 (EB)  
STRUCTURE NO. 006-0175 (WB)

|                     |           |                           |                    |              |           |
|---------------------|-----------|---------------------------|--------------------|--------------|-----------|
| SHEET NO. 35        | F.A. RTE. | SECTION                   | COUNTY             | TOTAL SHEETS | SHEET NO. |
| 39 SHEETS           | 80        | *                         | BUREAU             | 314          | 233       |
|                     |           |                           | CONTRACT NO. 66908 |              |           |
| FED. ROAD DIST. NO. |           | ILLINOIS FED. AID PROJECT |                    |              |           |

TYLINT INTERNATIONAL

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



SOIL BORING LOG

Page 1 of 3

ROUTE FAI 80 DESCRIPTION P92-038-03 I-80 bridge over Negro Creek, 1.75 m. E. of IL 89 LOGGED BY W. Garza  
SECTION 06-7B-1R&06-7VB LOCATION Hall Twp. - 17 NW, SEC. , TWP. 16N, RNG. 11E  
COUNTY Bureau DRILLING METHOD Hollow Stem Auger HAMMER TYPE B-53 Diedrich Automatic

| STRUCT. NO.                                  | Station         | D    | B     | U     | M   | Surface Water Elev.                 | D  | B   | U  | M |
|--|-----------------|------|-------|-------|-----|-------------------------------------|----|-----|----|---|
|  | 1758+70         | P    | L     | C     | O   | 560.0 ft                            | T  | O   | S  | O |
|  |                 | H    | S     | Qu    | I   | 558.5 ft                            | S  | W   | Qu | I |
| BORING NO.                                   | B-3b            |      |       |       |     | Groundwater Elev.:                  |    |     |    |   |
| Station                                      | 1757+34         |      |       |       |     | First Encounter                     |    |     |    |   |
| Offset                                       | 6.00R Rt CL Med |      |       |       |     | Upon Completion                     |    |     |    |   |
| Ground Surface Elev.                         | 646.8 ft        | (ft) | (/6") | (tsf) | (%) | After                               |    |     |    |   |
| SOFT brown SANDY LOAM                        |                 |      |       | 0.4   | 15  | STIFF/VERY STIFF gray CLAY LOAM     | 7  |     |    |   |
|  |                 |      |       | P     |     |                                     | 8  | 2.0 | 25 |   |
|  | 644.30          |      |       |       |     |                                     | 10 | B   |    |   |
| STIFF brown CLAY LOAM with SAND lens         | 642.80          | 2    | 2     | 1.4   | 14  | MEDIUM tan dirty fine dry SAND      | 4  |     |    |   |
|  |                 | 5    | 5     | B     |     |                                     | 8  |     |    |   |
|  | 642.80          |      |       |       |     |                                     | 10 |     |    |   |
| VERY STIFF brown SANDY LOAM with GRAVEL lens | 640.30          | 5    | 10    | 2.6   | 11  | VERY STIFF gray/tan SILTY CLAY LOAM | 6  |     |    |   |
|  |                 | 12   | 12    | S     |     |                                     | 6  | 3.3 | 20 |   |
|  | 640.30          |      |       |       |     |                                     | 7  | B   |    |   |
| SOFT gray SILTY CLAY                         | 637.80          | 3    | 4     | 1.8   | 25  | VERY STIFF gray SILTY LOAM          | 2  |     |    |   |
|  |                 | 8    | 8     | B     |     |                                     | 5  | 2.2 | 24 |   |
|  | 637.80          |      |       |       |     |                                     | 7  | S   |    |   |
| STIFF gray SANDY LOAM                        | 635.30          | 3    | 6     | 1.8   | 11  | STIFF gray SILTY LOAM with ORGANICS | 3  |     |    |   |
|  |                 | 7    | 7     | S     |     |                                     | 5  | 1.2 | 23 |   |
|  | 635.30          |      |       |       |     |                                     | 7  | S   |    |   |
| VERY STIFF brown LOAM                        | 632.30          | 4    | 5     | 3.9   | 11  | STIFF grayish tan SILTY CLAY LOAM   | 3  |     |    |   |
|  |                 | 10   | 10    | B     |     |                                     | 4  | 1.2 | 18 |   |
|  | 632.30          |      |       |       |     |                                     | 7  | P   |    |   |
| MEDIUM grayish tan dirty fine SAND           | 629.80          | 10   | 10    |       | 12  | MEDIUM tan brown LOAM               | 1  |     |    |   |
|  |                 | 10   | 10    |       |     |                                     | 1  | 0.8 | 16 |   |
|  | 629.80          |      |       |       |     |                                     | 4  | B   |    |   |
| STIFF tan/brown SILTY CLAY LOAM              | 627.80          | 3    | 5     | 1.6   | 24  | STIFF gray SILT                     | 4  |     |    |   |
|  |                 | 9    | 9     | B     |     |                                     | 6  | 1.7 | 25 |   |
|  | 627.80          |      |       |       |     |                                     | 8  | S   |    |   |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)  
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)  
BBS, from 137 (Rev. 8-99)

|            |
|------------|
| DESIGNED   |
| CHECKED    |
| DRAWN MAU  |
| CHECKED AD |



SOIL BORING LOG

Page 2 of 3

ROUTE FAI 80 DESCRIPTION P92-038-03 I-80 bridge over Negro Creek, 1.75 m. E. of IL 89 LOGGED BY W. Garza  
SECTION 06-7B-1R&06-7VB LOCATION Hall Twp. - 17 NW, SEC. , TWP. 16N, RNG. 11E  
COUNTY Bureau DRILLING METHOD Hollow Stem Auger HAMMER TYPE B-53 Diedrich Automatic

| STRUCT. NO.   | Station         | D    | B     | U     | M   | Surface Water Elev.                             | D  | B   | U  | M |
|---|-----------------|------|-------|-------|-----|---|----|-----|----|---|
|   | 1758+70         | P    | L     | C     | O   | 560.0 ft  | T  | O   | S  | O |
|   |                 | H    | S     | Qu    | I   | 558.5 ft  | S  | W   | Qu | I |
| BORING NO.  | B-3b            |      |       |       |     | Groundwater Elev.:                              |    |     |    |   |
| Station   | 1757+34         |      |       |       |     | First Encounter                                 |    |     |    |   |
| Offset  | 6.00R Rt CL Med |      |       |       |     | Upon Completion                                 |    |     |    |   |
| Ground Surface Elev.                                | 646.8 ft        | (ft) | (/6") | (tsf) | (%) | After   |    |     |    |   |
| VERY STIFF dark brown SILTY LOAM                    | 605.30          | 4    | 4     | 2.1   | 32  | VERY STIFF gray SILTY CLAY TILL                 | 6  |     |    |   |
|   |                 | 5    | 5     | B     |     |   | 7  | 2.9 | 14 |   |
|   | 605.30          |      |       |       |     |   | 11 | B   |    |   |
| VERY STIFF dark gray CLAY LOAM                      | 602.80          | 3    | 5     | 2.5   | 20  | VERY STIFF gray SILTY CLAY TILL                 | 7  |     |    |   |
|   |                 | 9    | 9     | B     |     |   | 11 | 3.9 | 12 |   |
|   | 602.80          |      |       |       |     |   | 13 | B   |    |   |
| VERY STIFF dark gray CLAY LOAM                      | 600.30          | 3    | 5     | 2.5   | 24  | HARD gray SILTY CLAY TILL                       | 7  |     |    |   |
|   |                 | 9    | 9     | B     |     |   | 9  | 4.3 | 13 |   |
|   | 600.30          |      |       |       |     |   | 13 | B   |    |   |
| STIFF gray SILTY CLAY LOAM with SILT lens           | 597.30          | 1    | 5     | 1.7   | 18  | HARD gray SILTY CLAY TILL                       | 9  |     |    |   |
|   |                 | 6    | 6     | B     |     |   | 13 | 6.2 | 13 |   |
|   | 597.30          |      |       |       |     |   | 19 | B   |    |   |
| VERY LOOSE/LOOSE gray clean medium coarse SAND      | 595.30          | 1    | 1     |       |     | VERY STIFF gray SILTY CLAY TILL                 | 5  |     |    |   |
|   |                 | 1    | 1     |       |     |   | 8  | 3.9 | 13 |   |
|   | 595.30          |      |       |       |     |   | 13 | B   |    |   |
| Wash MEDIUM gray clean medium coarse SAND with COAL | 592.80          | 5    | 8     |       |     | VERY STIFF gray SILTY CLAY with SAND lens       | 5  |     |    |   |
|   |                 | 13   | 13    |       |     |   | 8  | 3.1 | 19 |   |
|   | 592.80          |      |       |       |     |   | 22 | S   |    |   |
| Wash MEDIUM gray fine SAND                          | 590.30          | 8    | 12    |       |     | VERY DENSE gray fine SAND with medium SAND lens | 34 |     |    |   |
|   |                 | 17   | 17    |       |     |   | 41 |     |    |   |
|   | 590.30          |      |       |       |     |   | 53 |     |    |   |
| MEDIUM gray fine and medium SAND                    | 587.30          | 10   | 12    |       |     | VERY DENSE gray fine SAND with medium SAND lens | 38 |     |    |   |
|   |                 | 15   | 15    |       |     |   | 36 |     |    |   |
|   | 587.30          |      |       |       |     |   | 38 |     |    |   |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)  
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)  
BBS, from 137 (Rev. 8-99)

BORING LOGS -3  
STRUCTURE NO. 006-0174 (EB)  
STRUCTURE NO. 006-0175 (WB)

|              |                              |                  |                    |                  |               |
|--------------|------------------------------|------------------|--------------------|------------------|---------------|
| SHEET NO. 36 | F.A. RTE. 80                 | SECTION *        | COUNTY BUREAU      | TOTAL SHEETS 349 | SHEET NO. 234 |
| 39 SHEETS    | FED. ROAD DIST. NO. ILLINOIS |                  | CONTRACT NO. 66908 |                  |               |
|              |                              | FED. AID PROJECT |                    |                  |               |

TYLIN INTERNATIONAL



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



Illinois Department of Transportation  
Division of Highways  
IDOT

SOIL BORING LOG

Page 1 of 3

Date 3/16/05

ROUTE FAI 80 DESCRIPTION P92-038-03 I-80 bridge over Negro Creek, 1.75 m. E. of IL 89 LOGGED BY W. Garza

SECTION 06-7B-1R&06-7VB LOCATION Hall Twp. - 17 NW, SEC. , TWP. 16N, RNG. 11E

COUNTY Bureau DRILLING METHOD Hollow Stem Auger HAMMER TYPE B-53 Diedrich Automatic

| STRUCT. NO.<br>Station  | DEPTH (ft) | BLOW COUNT (blows/ft) | UCS (tsf) | MOISTURE (%) | Soil Description  | DEPTH (ft) | BLOW COUNT (blows/ft) | UCS (tsf) | MOISTURE (%) |
|---|------------|-----------------------|-----------|--------------|---|------------|-----------------------|-----------|--------------|
| 1758+70   |            |                       |           |              | Surface Water Elev. 560.0 ft<br>Stream Bed Elev. 558.5 ft                       |            |                       |           |              |
| BORING NO. B-4b<br>Station 1760+10<br>Offset 20.00ft Lt CL<br>Ground Surface Elev. 647.7 ft |            |                       |           |              | Groundwater Elev.:<br>First Encounter 592.2 ft<br>Upon Completion<br>After Hrs. |            |                       |           |              |
| MEDIUM brown SILTY CLAY LOAM  |            |                       | 0.8 P     | 20           | STIFF dark gray SILTY LOAM  | 4          | 8                     | 1.8 S     | 19           |
| STIFF brown SILTY CLAY LOAM   | 645.20     | 4                     | 1.6 B     |              | VERY STIFF gray CLAY LOAM TILL  | 7          | 7                     | 3.5 B     | 20           |
| VERY STIFF dark brown/gray CLAY LOAM  | 643.70     | 5                     |           |              | STIFF gray SILTY CLAY LOAM  | 3          | 7                     | 1.7 B     | 24           |
| STIFF brown LOAM  | 641.20     | 2                     | 1.8 B     | 11           | STIFF gray SILTY CLAY LOAM  | 6          | 7                     | 2.0 P     | 19           |
| VERY STIFF brown SANDY LOAM   | 638.70     | 4                     | 2.3 S     | 12           | VERY STIFF gray SILTY CLAY  | 6          | 7                     | 3.1 B     | 21           |
| VERY STIFF gray SILTY CLAY LOAM   | 636.20     | 7                     | 2.7 P     | 22           | STIFF green/gray SILTY CLAY TILL  | 3          | 6                     | 1.8 S     | 17           |
| VERY STIFF brown SANDY LOAM   | 633.70     | 3                     | 4.0 P     | 11           | HARD gray/gray CLAY LOAM TILL   | 3          | 6                     | 4.1 S     | 11           |
| HARD dark brown SILTY CLAY LOAM   | 631.20     | 4                     | 4.1 B     | 20           | STIFF dark gray LOAM  | 3          | 7                     | 1.8 S     | 21           |
|   | 628.70     | 9                     |           |              |   |            |                       |           |              |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)  
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)  
BBS, from 137 (Rev. 8-99)



Illinois Department of Transportation  
Division of Highways  
IDOT

SOIL BORING LOG

Page 2 of 3

Date 3/16/05

ROUTE FAI 80 DESCRIPTION P92-038-03 I-80 bridge over Negro Creek, 1.75 m. E. of IL 89 LOGGED BY W. Garza

SECTION 06-7B-1R&06-7VB LOCATION Hall Twp. - 17 NW, SEC. , TWP. 16N, RNG. 11E

COUNTY Bureau DRILLING METHOD Hollow Stem Auger HAMMER TYPE B-53 Diedrich Automatic

| STRUCT. NO.<br>Station  | DEPTH (ft) | BLOW COUNT (blows/ft) | UCS (tsf) | MOISTURE (%) | Soil Description  | DEPTH (ft) | BLOW COUNT (blows/ft) | UCS (tsf) | MOISTURE (%) |
|---|------------|-----------------------|-----------|--------------|---|------------|-----------------------|-----------|--------------|
| 1758+70   |            |                       |           |              | Surface Water Elev. 560.0 ft<br>Stream Bed Elev. 558.5 ft                       |            |                       |           |              |
| BORING NO. B-4b<br>Station 1760+10<br>Offset 20.00ft Lt CL<br>Ground Surface Elev. 647.7 ft |            |                       |           |              | Groundwater Elev.:<br>First Encounter 592.2 ft<br>Upon Completion<br>After Hrs. |            |                       |           |              |
| VERY STIFF gray LOAM  | 606.20     | 3                     | 2.3 P     | 20           | HARD gray SILTY CLAY LOAM TILL with SAND & GRAVEL lens                          | 9          | 16                    | 6.0 S     | 11           |
| STIFF gray SILTY CLAY LOAM with ORGANICS  | 603.70     | 4                     | 1.1 S     | 23           | DENSE gray fine SAND  | 6          | 9                     |           |              |
| STIFF dark gray SILTY CLAY LOAM with SAND lens  | 600.70     | 2                     | 1.2 B     | 23           | MEDIUM gray fine SAND   | 7          | 14                    |           |              |
| MEDIUM gray dirty fine SAND, very moist   | 598.70     | 2                     |           |              | Wash DENSE gray fine SAND   | 9          | 18                    |           |              |
| MEDIUM gray dirty medium SAND with GRAVEL, very moist                                       | 595.70     | 5                     |           |              | MEDIUM gray fine SAND   | 12         | 16                    |           |              |
| HARD gray SILTY CLAY TILL   | 593.70     | 3                     | 4.1 B     | 13           |   |            |                       |           |              |
| HARD gray SILTY CLAY TILL   | 591.20     | 5                     | 4.3 B     | 13           | VERY DENSE gray fine SAND   | 18         | 24                    |           |              |
| HARD gray SILTY CLAY LOAM TILL  | 588.70     | 5                     | 5.0 B     | 12           | DENSE gray clean medium SAND with GRAVEL  | 17         | 21                    |           |              |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)  
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)  
BBS, from 137 (Rev. 8-99)

|            |
|------------|
| DESIGNED   |
| CHECKED    |
| DRAWN MAU  |
| CHECKED AD |

BORING LOGS - 5  
STRUCTURE NO. 006-0174 (EB)  
STRUCTURE NO. 006-0175 (WB)

|                     |              |                           |                    |                  |               |
|---------------------|--------------|---------------------------|--------------------|------------------|---------------|
| SHEET NO. 38        | F.A. RTE. 80 | SECTION *                 | COUNTY BUREAU      | TOTAL SHEETS 344 | SHEET NO. 236 |
| 39 SHEETS           |              |                           | CONTRACT NO. 66908 |                  |               |
| FED. ROAD DIST. NO. |              | ILLINOIS FED. AID PROJECT |                    |                  |               |

TYLIN INTERNATIONAL

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



Illinois Department  
of Transportation  
Division of Highways  
IDOT

SOIL BORING LOG

Page 3 of 3

Date 3/16/05

ROUTE FAI 80 DESCRIPTION P92-038-03 I-80 bridge over Negro Creek, 1.75 m. E. of I. 89 LOGGED BY W. Garza  
SECTION 06-7B-1R&06-7VB LOCATION Hall Twp. - 17 NW, SEC. , TWP. 16N, RNG. 11E  
COUNTY Bureau DRILLING METHOD Hollow Stem Auger HAMMER TYPE B-53 Diederich Automatic

| STRUCT. NO.                           | DEPTH | BULGE | UCS   | METHOD | Surface Water Elev.          | Stream Bed Elev. |
|---------------------------------------|-------|-------|-------|--------|------------------------------|------------------|
| Station                               | ft    | (/6") | (tsf) | (%)    | ft                           | ft               |
| <u>1758+70</u>                        |       |       |       |        | <u>560.0</u>                 | <u>558.5</u>     |
| <b>BORING NO.</b> <u>B-4b</u>         |       |       |       |        | <b>Groundwater Elev.:</b>    |                  |
| Station <u>1760+10</u>                |       |       |       |        | First Encounter <u>592.2</u> | ft <u>∇</u>      |
| Offset <u>20.00ft LI CL</u>           |       |       |       |        | Upon Completion _____        | ft               |
| Ground Surface Elev. <u>647.7</u>     | ft    |       |       |        | After _____                  | ft               |
| HARD gray SILTY CLAY with SHALEY CLAY | 7     |       | 4.0   | 20     |                              |                  |
| 566.20                                | 12    |       | B     |        |                              |                  |
| End of Boring                         | 19    |       |       |        |                              |                  |
|                                       | -85   |       |       |        |                              |                  |
|                                       | -90   |       |       |        |                              |                  |
|                                       | -95   |       |       |        |                              |                  |
|                                       | -100  |       |       |        |                              |                  |

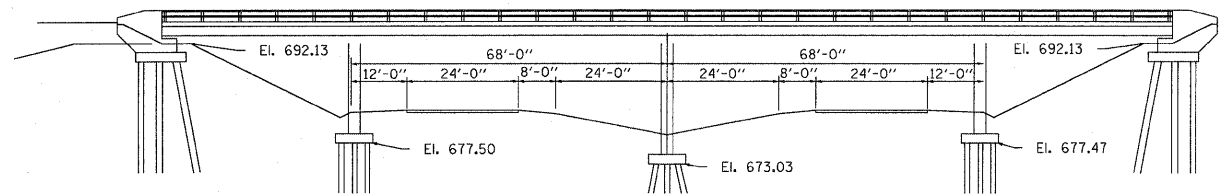
The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)  
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)  
BBS, from 137 (Rev. 8-99)

|            |
|------------|
| DESIGNED   |
| CHECKED    |
| DRAWN MAU  |
| CHECKED AD |

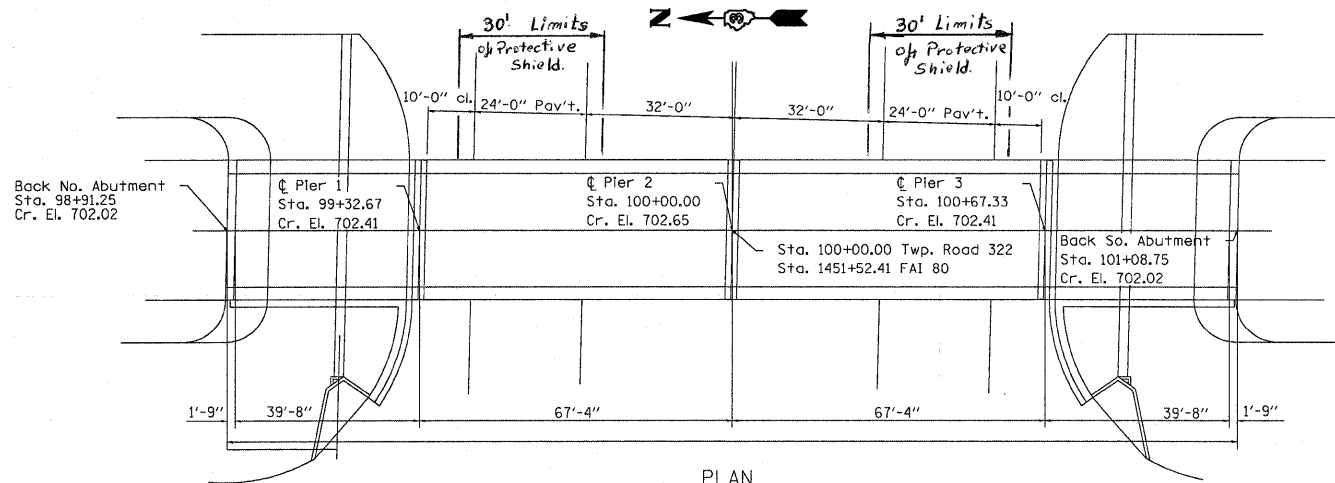
BORING LOGS - 6  
STRUCTURE NO. 006-0174 (EB)  
STRUCTURE NO. 006-0175 (WB)

|              |                     |           |  |                  |               |
|--------------|---------------------|-----------|--|------------------|---------------|
| SHEET NO. 39 | F.A. RTE. 80        | SECTION * | COUNTY BUREAU                                | TOTAL SHEETS 344 | SHEET NO. 237 |
| 39 SHEETS    | FED. ROAD DIST. NO. |           | ILLINOIS FED. AID PROJECT CONTRACT NO. 66908 |                  |               |

TYLIN INTERNATIONAL



ELEVATION



PLAN

PROPOSED SCOPE

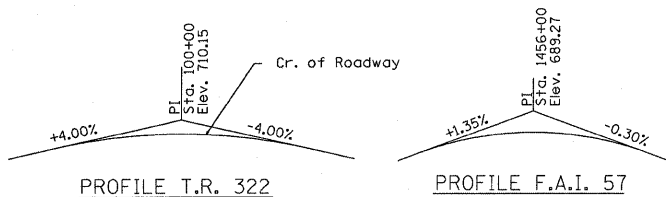
1. Deck slab Repair
2. Formed Concrete Repair on North Pier
3. Fill gap between abutment and slopewall with CLSM
4. Plug deck drains within 10' of substructure units

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

TOTAL BILL OF MATERIAL

| ITEM  | UNIT    | SUPER | SUB | TOTAL |
|---|---------|-------|-----|-------|
| DECK SLAB REPAIR (PARTIAL DEPTH)                | SQ YD   | 17.20 |     | 17.20 |
| STRUCTURAL REPAIR OF CONCRETE (6" <sup>5</sup>) | SQ FT   |       | 3.0 | 3.0   |
| PLUG EXISTING DECK DRAINS                       | EACH    | 8     |     | 8     |
| CONTROL LOW-STRENGTH MATERIAL                   | CU YD   |       | 1   | 1     |
| PROTECTIVE SHIELD                               | SQ. YD. | 160   |     | 160   |
|   |         |       |     |       |
|   |         |       |     |       |
|   |         |       |     |       |
|   |         |       |     |       |
|   |         |       |     |       |
|   |         |       |     |       |
|   |         |       |     |       |
|   |         |       |     |       |
|   |         |       |     |       |
|   |         |       |     |       |
|   |         |       |     |       |
|   |         |       |     |       |



PROFILE T.R. 322

PROFILE F.A.I. 57

DESIGN STRESSES

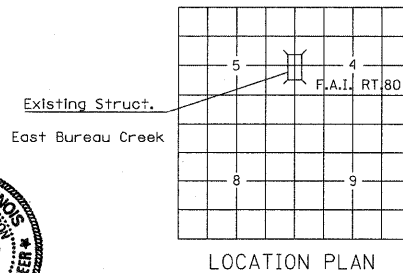
F'c = 1400 psi  
 Vc = 75 psi  
 fs = 20,000 psi (Reinf)  
 fs = 18,000 psi (Struct)  
 n = 10

CURVE DATA (F.A.I.)

Δ 1'-00'-31"  
 D 0'-10'-00"  
 R 34377.48  
 T 2145.86  
 I 4286.17

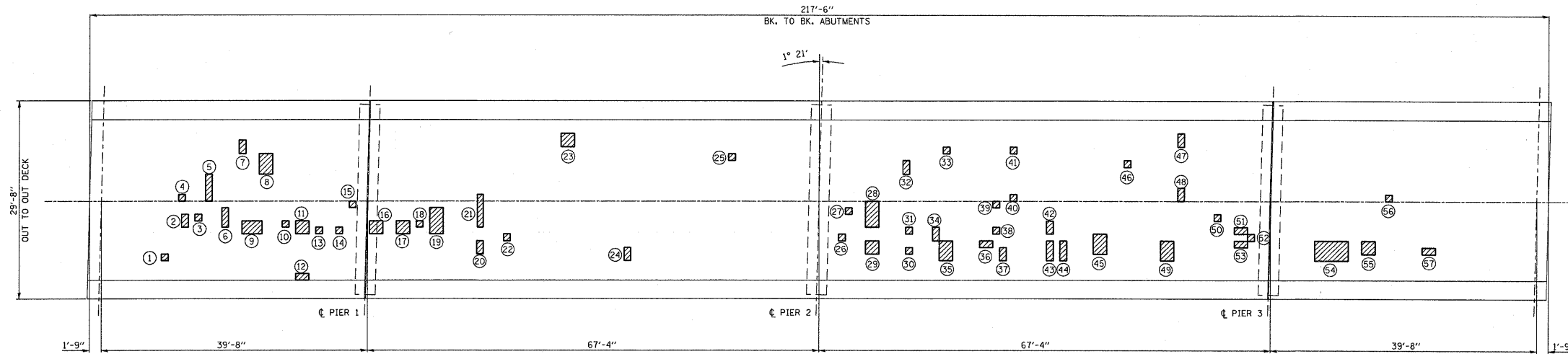


Expires Nov. 30, 2010



LOCATION PLAN

GENERAL PLAN & ELEVATION  
 T.R. 322 OVER FAI 80  
 STA. 1451+52.41  
 FAI RT. 80 PROJECT I-80-2(18) 50  
 SECTION 05-6 HB-1 BUREAU COUNTY



DECK SLAB REPAIR (PARTIAL)

## DECK PATCHING PLAN

S.N. 006-0127

DECK SURVEY DATE: 07/11/06  
COMPLETED BY: DISTRICT

| PATCH NO. | DECK SLAB REPAIR (PARTIAL DEPTH) | DECK SLAB REPAIR (FD 11) | DECK SLAB REPAIR (FD 11 2) |
|-----------|----------------------------------|--------------------------|----------------------------|
|           | SO. YD.                          | SO. YD.                  | SO. YD.                    |
| 1         | 0.11                             |                          |                            |
| 2         | 0.22                             |                          |                            |
| 3         | 0.11                             |                          |                            |
| 4         | 0.11                             |                          |                            |
| 5         | 0.44                             |                          |                            |
| 6         | 0.33                             |                          |                            |
| 7         | 0.22                             |                          |                            |
| 8         | 0.67                             |                          |                            |
| 9         | 0.67                             |                          |                            |
| 10        | 0.11                             |                          |                            |
| 11        | 0.44                             |                          |                            |
| 12        | 0.22                             |                          |                            |
| 13        | 0.11                             |                          |                            |
| 14        | 0.11                             |                          |                            |
| 15        | 0.11                             |                          |                            |
| 16        | 0.44                             |                          |                            |
| 17        | 0.44                             |                          |                            |
| 18        | 0.11                             |                          |                            |
| 19        | 0.89                             |                          |                            |
| 20        | 0.22                             |                          |                            |
| 21        | 0.50                             |                          |                            |
| 22        | 0.11                             |                          |                            |
| 23        | 0.44                             |                          |                            |
| 24        | 0.22                             |                          |                            |
| 25        | 0.11                             |                          |                            |
| 26        | 0.11                             |                          |                            |

(CONTINUED)

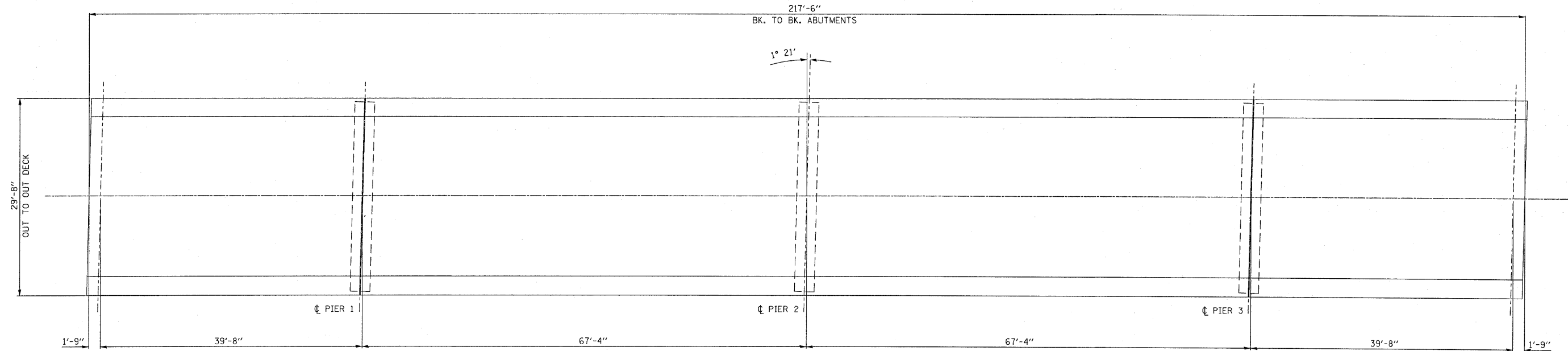
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|--------|-------|--|--|
| 27     | 0.11  |  |  |
| 28     | 0.89  |  |  |
| 29     | 0.44  |  |  |
| 30     | 0.11  |  |  |
| 31     | 0.11  |  |  |
| 32     | 0.22  |  |  |
| 33     | 0.11  |  |  |
| 34     | 0.22  |  |  |
| 35     | 0.67  |  |  |
| 36     | 0.22  |  |  |
| 37     | 0.22  |  |  |
| 38     | 0.11  |  |  |
| 39     | 0.11  |  |  |
| 40     | 0.11  |  |  |
| 41     | 0.11  |  |  |
| 42     | 0.22  |  |  |
| 43     | 0.33  |  |  |
| 44     | 0.33  |  |  |
| 45     | 0.67  |  |  |
| 46     | 0.11  |  |  |
| 47     | 0.22  |  |  |
| 48     | 0.22  |  |  |
| 49     | 0.67  |  |  |
| 50     | 0.11  |  |  |
| 51     | 0.22  |  |  |
| 52     | 0.11  |  |  |
| 53     | 0.22  |  |  |
| 54     | 1.67  |  |  |
| 55     | 0.44  |  |  |
| 56     | 0.11  |  |  |
| 57     | 0.22  |  |  |
| TOTALS | 17.20 |  |  |


TOTAL DECK AREA=  
716.95 SQ. YDS.


2.4% PARTIAL DEPTH  
PATCHING REQUIRED  
ON DECK

\*\*06-[7BR&BR-1,7VB-M,6BR&6,7 RS-1 & I]

|   |                             |                |               |   |  |                |               |   |              |           |
|---|-----------------------------|----------------|---------------|---|--|----------------|---------------|---|--------------|-----------|
| FILE NAME =   | USER NAME = braboygo        | DESIGNED - --- | REVISED - --- | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>BRIDGE REPAIR PLANS SN 006-0127</b> | F.A. RTE. I-80 | SECTION **    | COUNTY  | TOTAL SHEETS | SHEET NO. |
| ct:\pw\work\p\dot\braboygo\d0112640\vd36908-sht-details.DGN | PLOT SCALE = 50.7353' / IN. | CHECKED - ---  | REVISED - --- |   | SCALE: _____                           |                |               | BUREAU  | 344          | 239       |
| PLOT DATE = Sep 07, 2009 - 01:48:39 PM                      | DATE - _____                | REVISED - ---  | REVISED - --- |   | SHEET NO. ___ OF ___ SHEETS            | STA. _____     | TO STA. _____ | CONTRACT NO. 66908<br>ILLINOIS FED. AID PROJECT |              |           |



 DECK SLAB REPAIR (PARTIAL)

 DECK SLAB REPAIR (FD TYPE II)

## DECK PATCHING RECORD

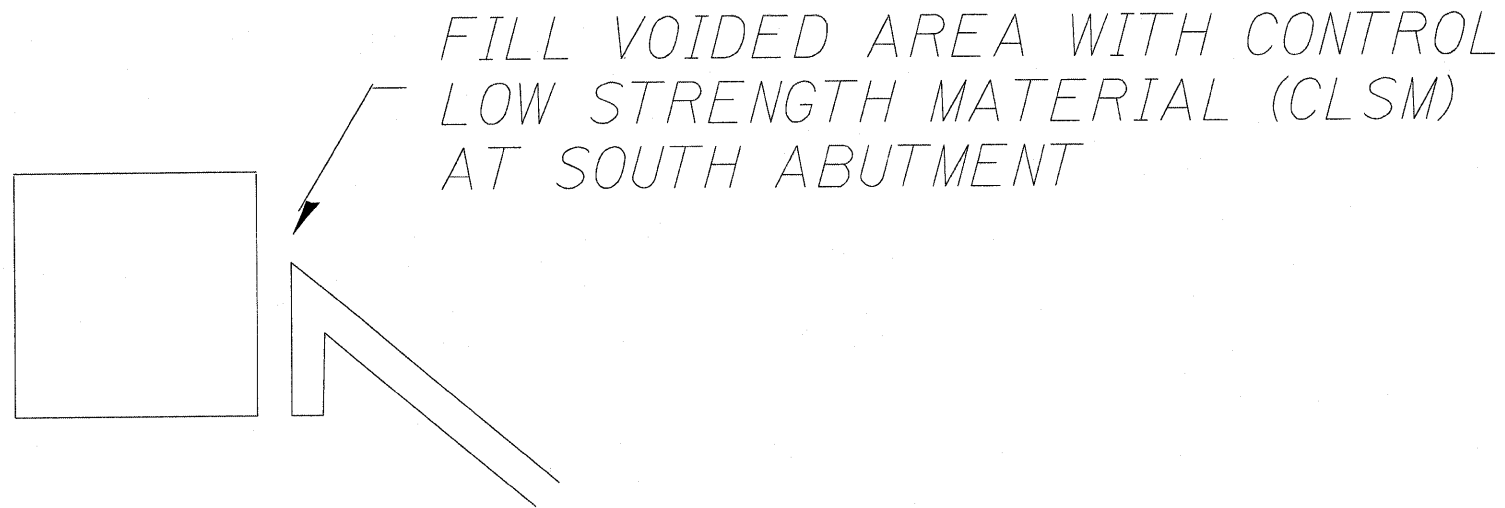
S.N. 006-0127

ACTUAL DECK PATCHING AREAS AND LOCATIONS  
SHALL BE SHOWN ON AS BUILT PLANS

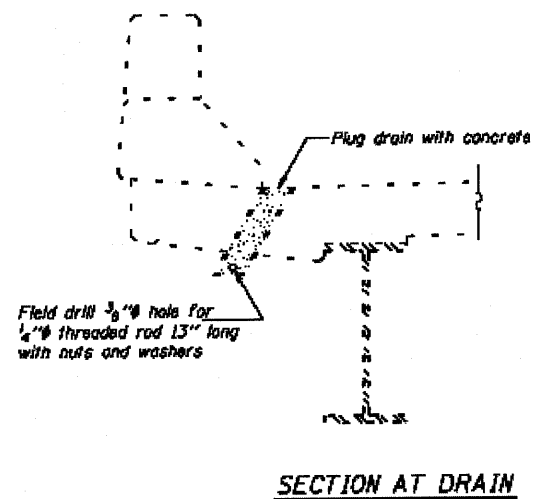
**\*\*06-[7BR&BR-1,7VB-M,6BR&6,7 RS-1 & I]**

|  |   |                       |                           |   |  |                                 |                          |                    |                        |                     |  |
|--|---|-----------------------|---------------------------|---|--|---------------------------------|--------------------------|--------------------|------------------------|---------------------|--|
| FILE NAME =<br>c:\pwork\pwork\braboyco\d0112640\d368 | USER NAME = braboyco<br>908-shr-details.dgn | DESIGNED -<br>DRAWN - | REVISED -<br>REVISED -    | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>BRIDGE REPAIR PLANS SN 006-0127</b> | F.A.<br>RTE.<br>I-80            | SECTION<br>**            | COUNTY<br>BUREAU   | TOTAL<br>SHEETS<br>344 | SHEET<br>NO.<br>240 |  |
| PLOT SCALE = 50.7353' / IN.                          | CHECKED -                                   | REVISED -             | SCALE: _____              |   |  | SHEET NO. _____ OF _____ SHEETS | STA. _____ TO STA. _____ | CONTRACT NO. 66908 |                        |                     |  |
| PLOT DATE = Sep 07, 2009 - 01:48:31 PM               | DATE -                                      | REVISED -             | ILLINOIS FED. AID PROJECT |   |  |                                 |                          |                    |                        |                     |  |

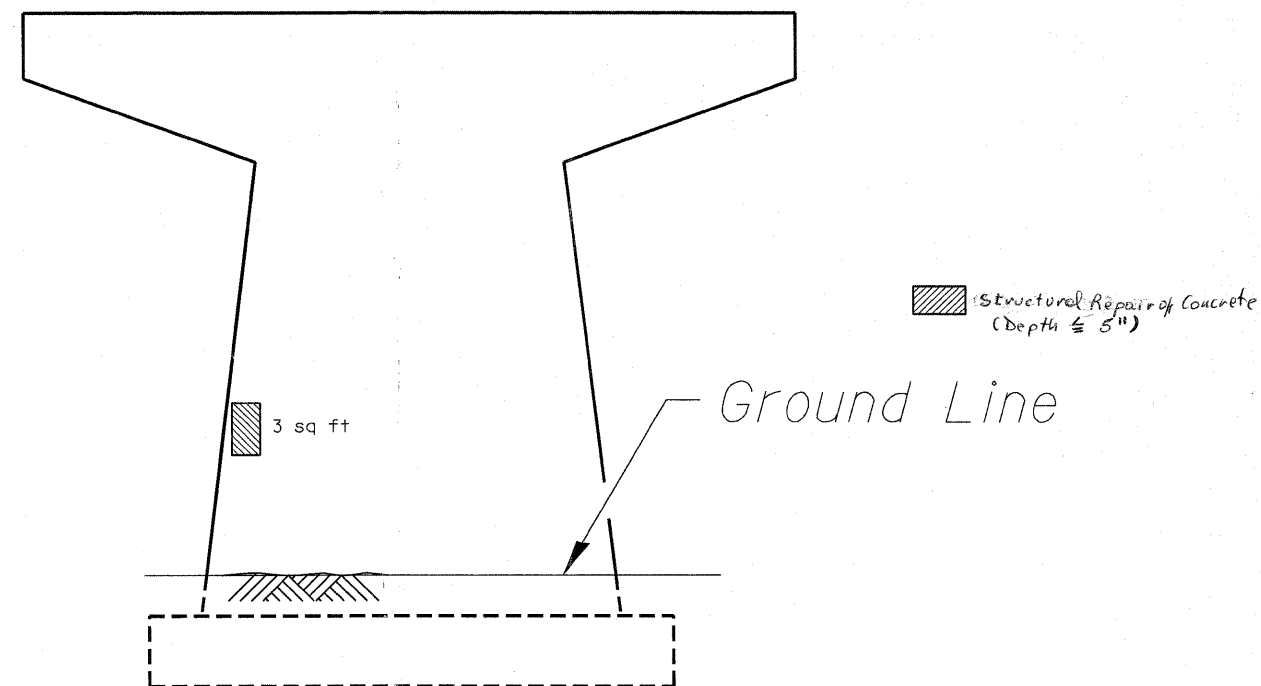




ABUTMENT/SLOPEWALL REPAIR



DRAIN ELIMINATION DETAIL



NORTH PIER SOUTH FACE

\*\*06-[7BR&BR-1,7VB-M,6BR&6,7 RS-1 & I]

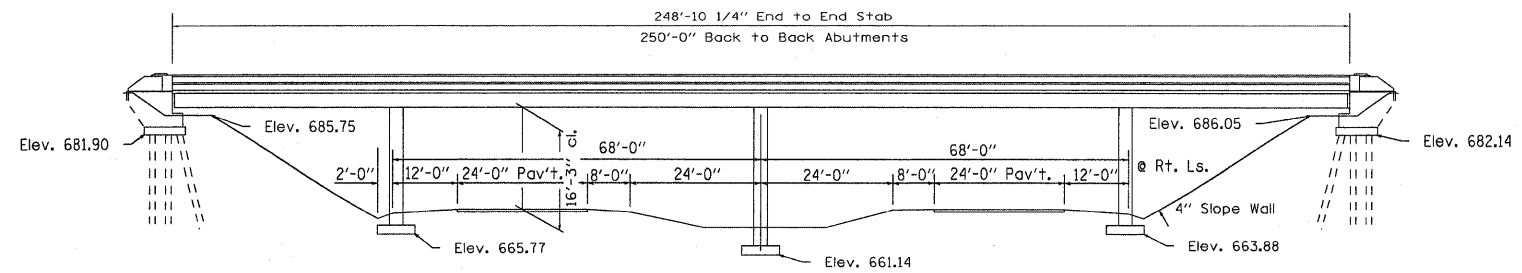
|  |  |            |          |
|--|--|------------|----------|
| FILE NAME =                                | USER NAME = braboygc                   | DESIGNED - | REVISD - |
| ci:\pw_work\p\p\dot\braboygc\d0112640\d368 | 908-shr-details.DGN                    | DRAWN -    | REVISD - |
|  | PLOT SCALE = 50.0638' / IN.            | CHECKED -  | REVISD - |
|  | PLOT DATE = Sep 07, 2009 - 01:48:22 PM | DATE -     | REVISD - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

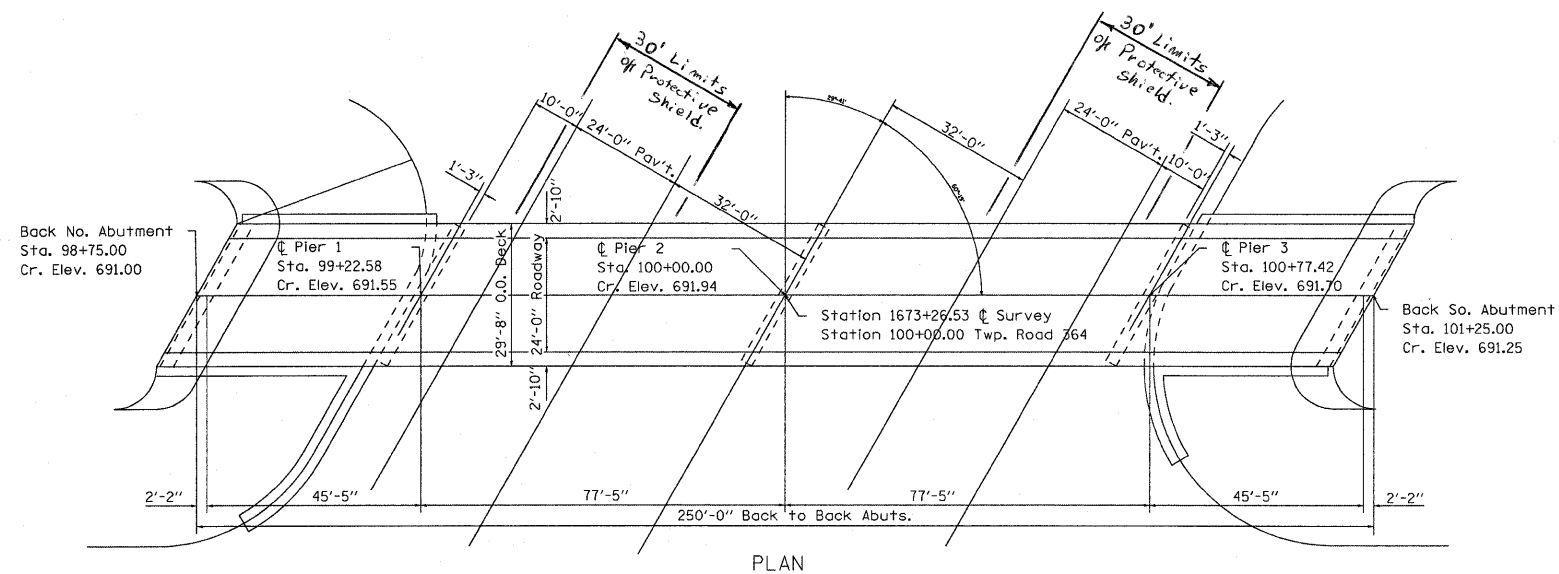
BRIDGE REPAIR PLANS SN 006-0127

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_ OF \_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

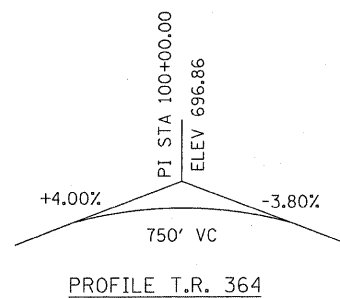
|                           |         |        |                    |           |
|---------------------------|---------|--------|--------------------|-----------|
| F.A. RTE.                 | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| I-80                      | **      | BUREAU | 344                | 241       |
| ILLINOIS FED. AID PROJECT |         |        | CONTRACT NO. 66908 |           |



ELEVATION

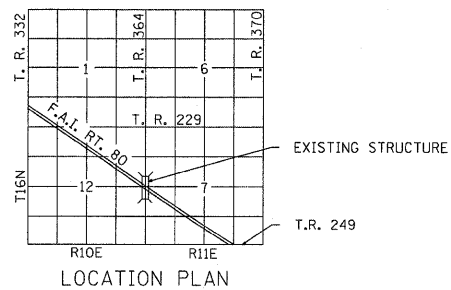


PLAN



DESIGN STRESSES (EXISTING)

F'C = 1400 psi  
Vc = 75 psi  
Fs = 20000 psi (Reinf)  
Fs = 20000 psi (Struct)  
n = 10



PROPOSED SCOPE

1. Deck slab Repair
2. Fill gap between abutment and slopewall with CLSM
3. Plug deck drains within 10' of substructure units
4. Extend remaining deck drains

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

TOTAL BILL OF MATERIAL

| ITEM                            | UNIT    | SUPER | SUB | TOTAL |
|---------------------------------|---------|-------|-----|-------|
| DECK SLAB REPAIR, PARTIAL DEPTH | SQ YD   | 14.94 |     | 14.94 |
| PLUG EXISTING DECK DRAINS       | EACH    | 6.0   |     | 6.0   |
| FLOOR DRAIN EXTENSIONS          | EACH    | 2.0   |     | 2.0   |
| CONTROL LOW-STRENGTH MATERIAL   | CU YD   |       | 1.5 | 1.5   |
| PROTECTIVE SHIELD               | SQ. YD. | 160   |     | 160   |
|                                 |         |       |     |       |
|                                 |         |       |     |       |
|                                 |         |       |     |       |
|                                 |         |       |     |       |
|                                 |         |       |     |       |
|                                 |         |       |     |       |
|                                 |         |       |     |       |
|                                 |         |       |     |       |
|                                 |         |       |     |       |
|                                 |         |       |     |       |
|                                 |         |       |     |       |
|                                 |         |       |     |       |
|                                 |         |       |     |       |
|                                 |         |       |     |       |
|                                 |         |       |     |       |
|                                 |         |       |     |       |

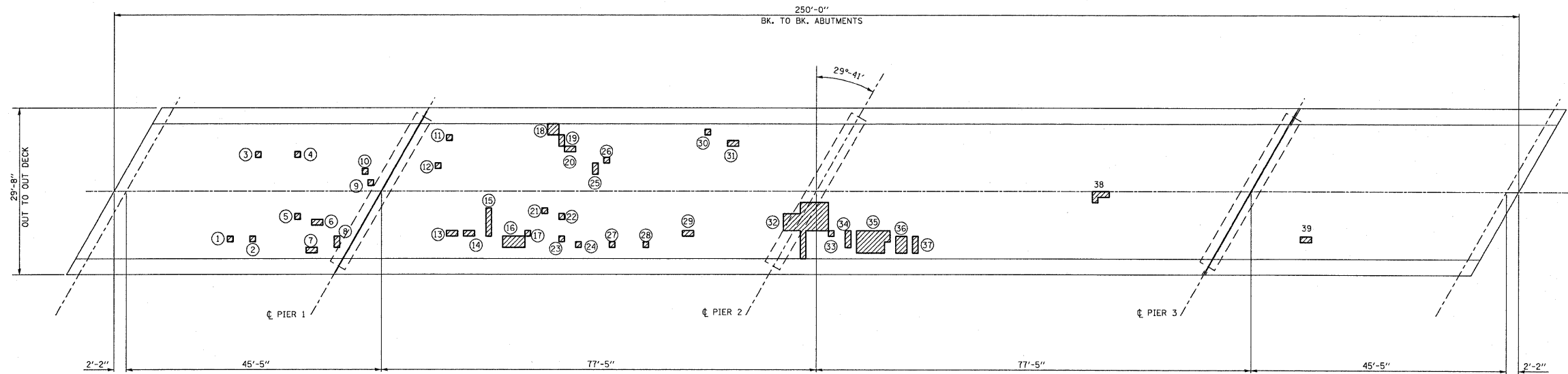
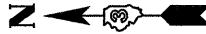
GENERAL PLAN & ELEVATION  
T.R. 364 OVER FAI 80  
STA. 1673+26.53  
FAI RT.80 PROJECT I-80-2(12) 67  
SECTION 06-7 HB BUREAU COUNTY



Expires Nov. 30, 2010

\*\*06-[7BR&BR-1.7VB-M,6BR&6.7 RS-1 & I]

|   |                             |            |           |   |  |                           |         |        |              |           |  |
|---|-----------------------------|------------|-----------|---|--|---------------------------|---------|--------|--------------|-----------|--|
| FILE NAME =   | USER NAME = breboypc        | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>BRIDGE REPAIR PLANS SN 006-0129</b> | F.A. RTE.                 | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |  |
| c:\p\work\p\dot\breboypc\d0112640\d364708-sht-details.DGN | PLOT SCALE = 50.0638' / IN. | DRAWN -    | REVISED - |   |  | I-80                      | **      | BUREAU | 344          | 242       |  |
| PLOT DATE = Sep 07, 2009 - 01:48:15 PM                    | DATE -                      | CHECKED -  | REVISED - |   |  | CONTRACT NO. 66908        |         |        |              |           |  |
|   |                             | DATE -     | REVISED - |   |  | ILLINOIS FED. AID PROJECT |         |        |              |           |  |



## DECK PATCHING PLAN

S.N. 006-0129

DECK SURVEY DATE: 07/11/2006  
COMPLETED BY: DISTRICT

DECK SLAB REPAIR (PARTIAL)

| PATCH NO. | DECK SLAB REPAIR (PARTIAL DEPTH) | DECK SLAB REPAIR (FD TY 1) | DECK SLAB REPAIR (FD TY 2) |  |
|-----------|----------------------------------|----------------------------|----------------------------|--|
|           | SO. YD.                          | SO. YD.                    | SO. YD.                    |  |
| 1         | 0.11                             |                            |                            |  |
| 2         | 0.11                             |                            |                            |  |
| 3         | 0.11                             |                            |                            |  |
| 4         | 0.11                             |                            |                            |  |
| 5         | 0.11                             |                            |                            |  |
| 6         | 0.22                             |                            |                            |  |
| 7         | 0.22                             |                            |                            |  |
| 8         | 0.22                             |                            |                            |  |
| 9         | 0.11                             |                            |                            |  |
| 10        | 0.11                             |                            |                            |  |
| 11        | 0.11                             |                            |                            |  |
| 12        | 0.11                             |                            |                            |  |
| 13        | 0.22                             |                            |                            |  |
| 14        | 0.22                             |                            |                            |  |
| 15        | 0.56                             |                            |                            |  |
| 16        | 0.89                             |                            |                            |  |
| 17        | 0.11                             |                            |                            |  |
| 18        | 0.44                             |                            |                            |  |
| 19        | 0.22                             |                            |                            |  |
| 20        | 0.22                             |                            |                            |  |
| 21        | 0.11                             |                            |                            |  |
| 22        | 0.11                             |                            |                            |  |

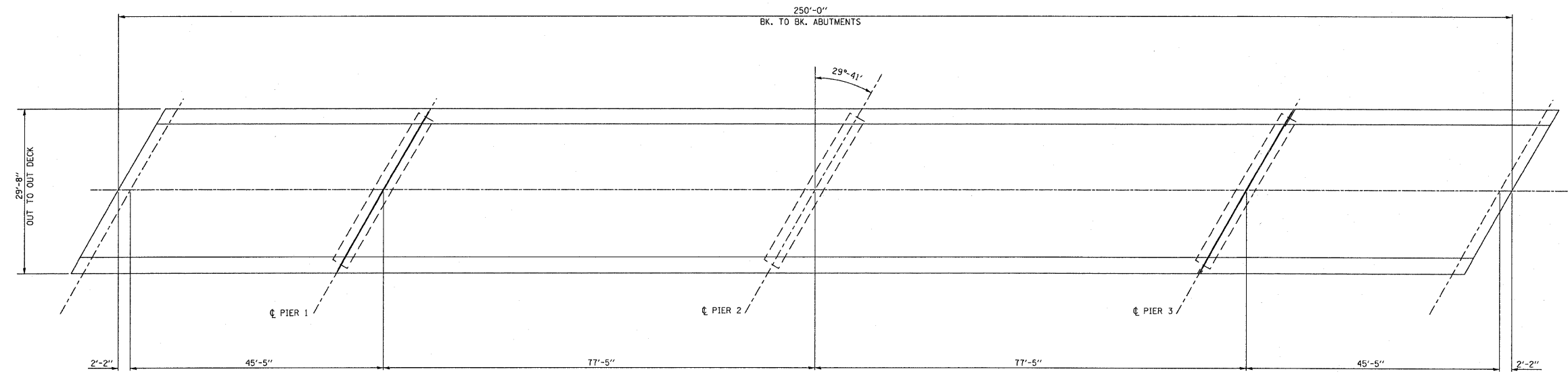
| PATCH NO.     | DECK SLAB REPAIR (PARTIAL DEPTH) | DECK SLAB REPAIR (FD TY 1) | DECK SLAB REPAIR (FD TY 2) |  |
|---------------|----------------------------------|----------------------------|----------------------------|--|
|               | SO. YD.                          | SO. YD.                    | SO. YD.                    |  |
| 23            | 0.11                             |                            |                            |  |
| 24            | 0.11                             |                            |                            |  |
| 25            | 0.22                             |                            |                            |  |
| 26            | 0.11                             |                            |                            |  |
| 27            | 0.11                             |                            |                            |  |
| 28            | 0.11                             |                            |                            |  |
| 29            | 0.22                             |                            |                            |  |
| 30            | 0.11                             |                            |                            |  |
| 31            | 0.22                             |                            |                            |  |
| 32            | 4.33                             |                            |                            |  |
| 33            | 0.11                             |                            |                            |  |
| 34            | 0.33                             |                            |                            |  |
| 35            | 2.44                             |                            |                            |  |
| 36            | 0.67                             |                            |                            |  |
| 37            | 0.33                             |                            |                            |  |
| 38            | 0.44                             |                            |                            |  |
| 39            | 0.22                             |                            |                            |  |
| <b>TOTALS</b> | <b>14.94</b>                     |                            |                            |  |

TOTAL DECK AREA=  
824.08 SQ. YDS.

1.81% PARTIAL DEPTH  
PATCHING REQUIRED  
ON DECK

\*\*06-[7BR&BR-1.7VB-M.6BR&6.7 RS-1 & I]

|   |                             |                             |               |   |  |                    |           |                           |                |             |
|---|-----------------------------|-----------------------------|---------------|---|--|--------------------|-----------|---------------------------|----------------|-------------|
| FILE NAME =   | USER NAME = braboypc        | DESIGNED - ---              | REVISED - --- | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>BRIDGE REPAIR PLANS SN 006-0129</b> | F.A. RTE. =        | SECTION = | COUNTY =                  | TOTAL SHEETS = | SHEET NO. = |
| ct:\pwork\pwork\braboypc\d0112640\d386908-sht-details.DGN | PLOT SCALE = 50.0638' / IN. | DRAWN - ---                 | REVISED - --- |   |  | 1-80               | **        | BUREAU                    | 344            | 243         |
| PLOT DATE = Sep 07, 2009 - 01:48:07 PM                    | DATE - ---                  | CHECKED - ---               | REVISED - --- |   |  | CONTRACT NO. 66908 |           | ILLINOIS FED. AID PROJECT |                |             |
| SCALE: _____  |                             | SHEET NO. ___ OF ___ SHEETS |               | STA. _____ TO STA. _____                                  |  |                    |           |                           |                |             |



- DECK SLAB REPAIR (PARTIAL)
- DECK SLAB REPAIR (FD TYPE II)

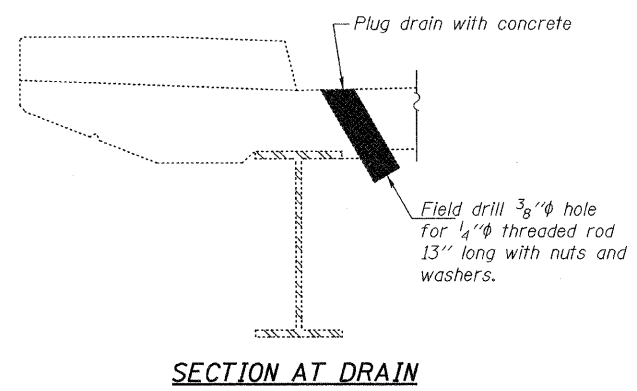
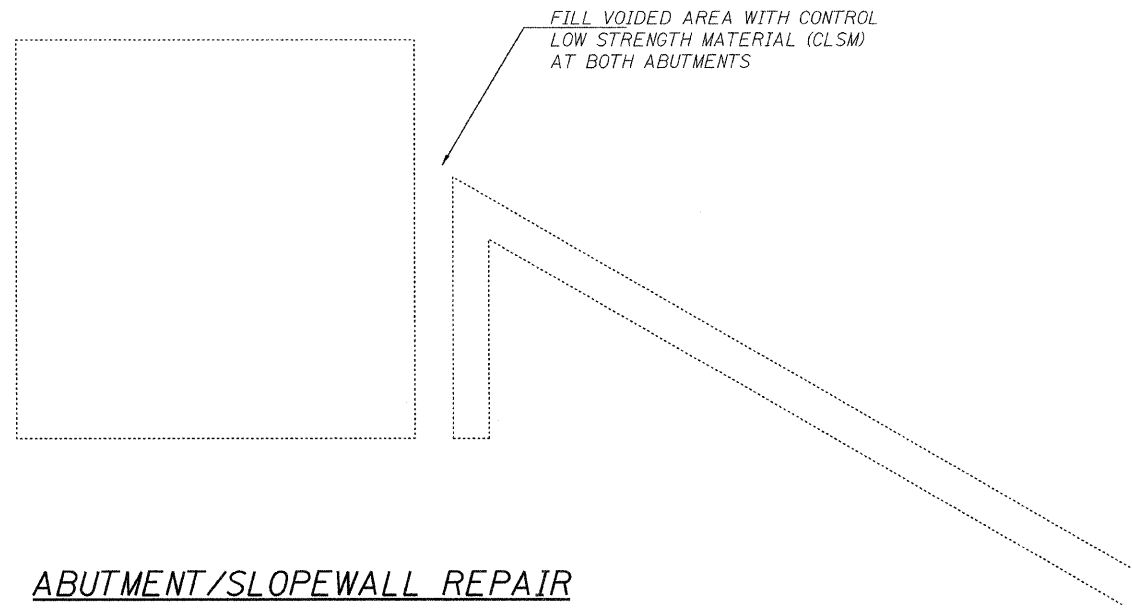
## DECK PATCHING RECORD

S.N. 006-0129

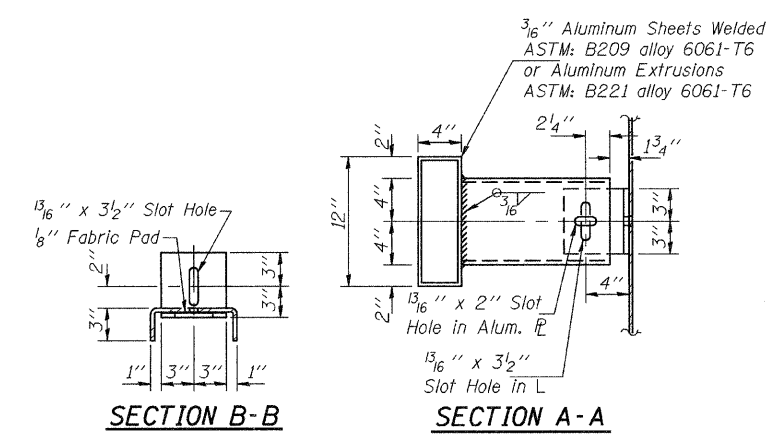
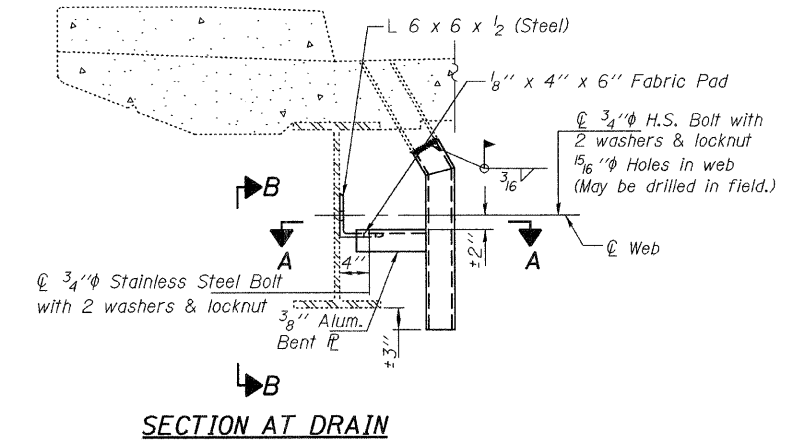
ACTUAL DECK PATCHING AREAS AND LOCATIONS  
SHALL BE SHOWN ON AS BUILT PLANS

\*\*06-[7BR&BR-1,7VB-M,6BR&6.7 RS-1 & I]

|   |                      |            |           |   |  |                           |         |        |              |           |  |
|---|----------------------|------------|-----------|---|--|---------------------------|---------|--------|--------------|-----------|--|
| FILE NAME =   | USER NAME = braboypc | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>BRIDGE REPAIR PLANS SN 006-0129</b> | F.A. RTE.                 | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |  |
| ci\pw\work\p\idot\braboypc\d0112640\d368908-sht-details.DGN |                      | DRAWN -    | REVISED - |   |  | 1-80                      | **      | BUREAU | 344          | 244       |  |
| PLOT SCALE = 50.0638' / IN.                                 |                      | CHECKED -  | REVISED - |   |  | CONTRACT NO. 66908        |         |        |              |           |  |
| PLOT DATE = Sep 07, 2009 - 01:47:57 PM                      |                      | DATE -     | REVISED - |   |  | ILLINOIS FED. AID PROJECT |         |        |              |           |  |



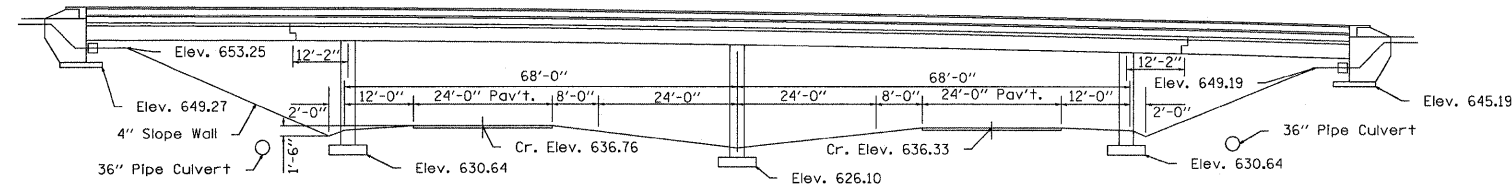
**DRAIN ELIMINATION DETAIL**



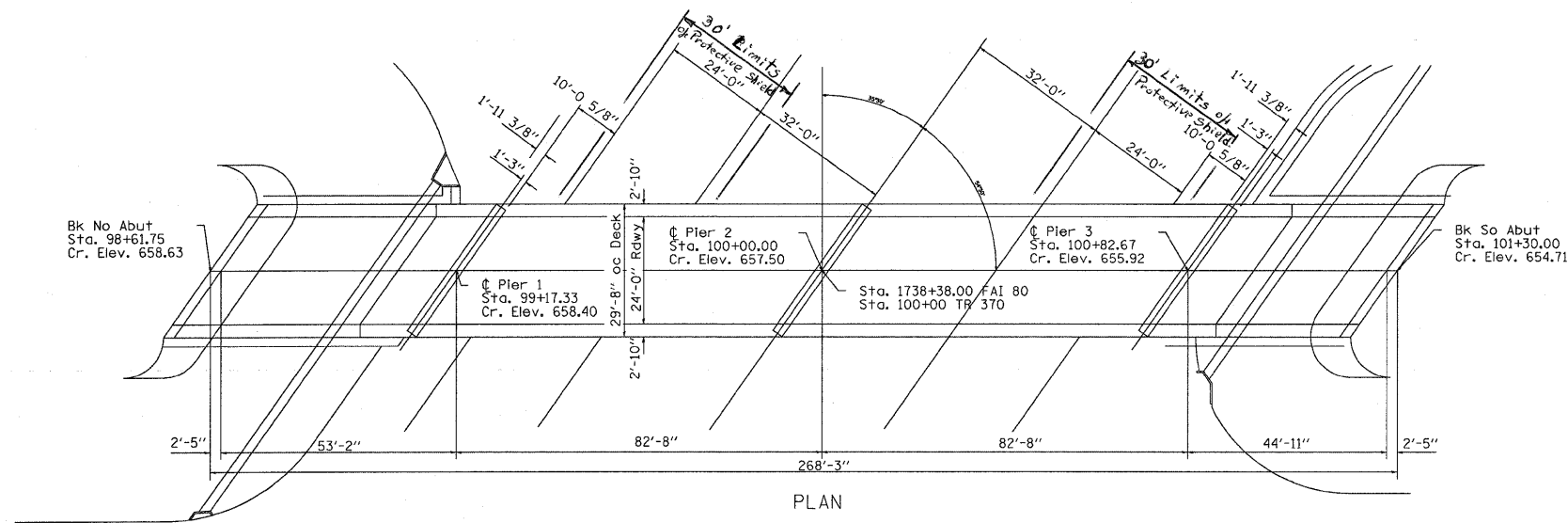
**DRAIN EXTENSION DETAIL**

|             |                      |            |           |   |  |                           |         |        |              |           |  |
|-------------|----------------------|------------|-----------|---|--|---------------------------|---------|--------|--------------|-----------|--|
| FILE NAME = | USER NAME = #USER*   | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>BRIDGE REPAIR PLANS SN 006-0129</b> | F.A. RTE.                 | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |  |
| #FILE#      |                      | DRAWN -    | REVISED - |   |  | I-80                      | **      | BUREAU | 344          | 245       |  |
|             | PLOT SCALE = #SCALE* | CHECKED -  | REVISED - |   |  | CONTRACT NO. 66908        |         |        |              |           |  |
|             | PLOT DATE = #DATE*   | DATE -     | REVISED - |   |  | ILLINOIS FED. AID PROJECT |         |        |              |           |  |
|             |                      |            |           | SCALE:  | SHEET NO. OF SHEETS                    | STA. TO STA.              |         |        |              |           |  |

\*\*06-T7BR&BR-1,7VB-M,6BR&6,7 RS-1 &I



ELEVATION



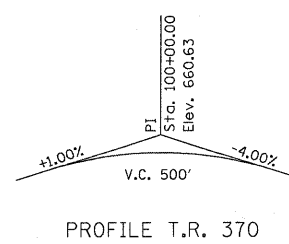
PLAN

PROPOSED SCOPE

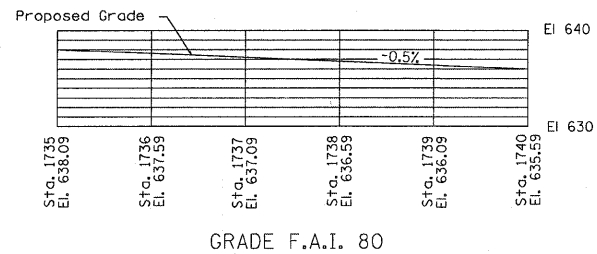
- 1. Deck slab Repair
2. Extend deck drains

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.



PROFILE T.R. 370

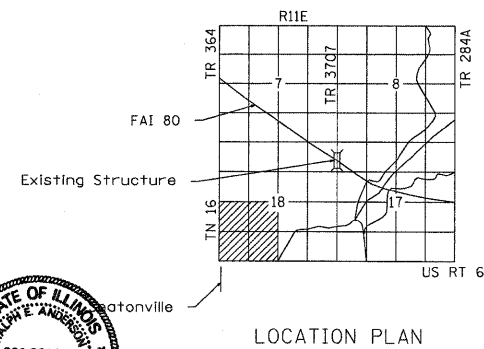


GRADE F.A.I. 80

LOADING H 15-S12-44

DESIGN STRESSES

fd = 1400 psi Super
vc = 75 psi Footings
fs = 20,000 psi Reinf
fs = 18,000 psi Struct
n = 10



LOCATION PLAN

TOTAL BILL OF MATERIAL

Table with columns: ITEM, UNIT, SUPER, SUB, TOTAL. Rows include DECK SLAB REPAIR (PARTIAL DEPTH), FLOOR DRAIN EXTENSIONS, PROTECTIVE SHIELD.

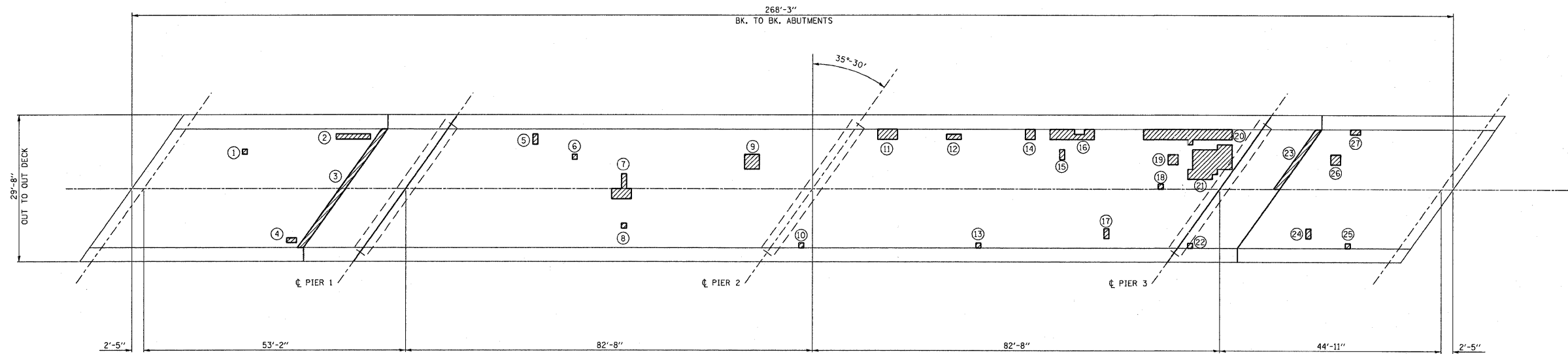
GENERAL PLAN & ELEVATION
T.R. 370 OVER FAI 80
STA. 1738+38
FAI RT. 80 PROJECT I-80-2(12) 67
SECTION 6-7HB-1 BUREAU COUNTY



Expires Nov. 30, 2010

Project information table including FILE NAME, USER NAME, DESIGNED, REVISED, DRAWN, CHECKED, DATE, and project details like BRIDGE REPAIR PLANS SN 006-0133, SECTION \*\*, COUNTY BUREAU, CONTRACT NO. 66908.

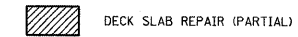
\*\*06-[7BR&BR-1,7VB-M,6BR&6,7 RS-1 & I]



## DECK PATCHING PLAN

S.N. 006-0133

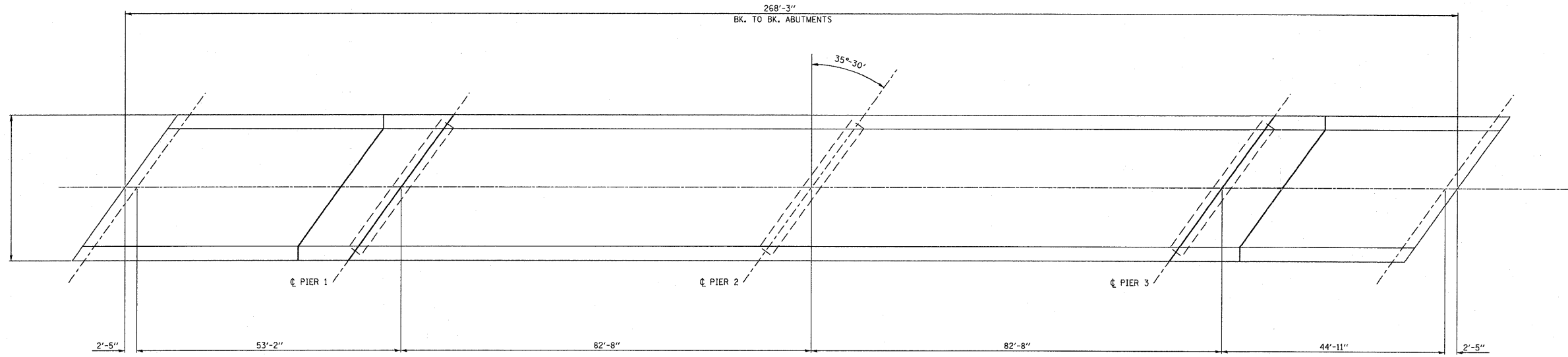
DECK SURVEY DATE: 07/11/2006  
COMPLETED BY: DISTRICT




| PATCH NO.     | DECK SLAB REPAIR (PARTIAL DEPTH) | DECK SLAB REPAIR (FD TY 1) | DECK SLAB REPAIR (FD TY 2) |  |
|---------------|----------------------------------|----------------------------|----------------------------|--|
|               | SQ. YD.                          | SQ. YD.                    | SQ. YD.                    |  |
| 1             | 0.11                             |                            |                            |  |
| 2             | 0.78                             |                            |                            |  |
| 3             | 3.27                             |                            |                            |  |
| 4             | 0.22                             |                            |                            |  |
| 5             | 0.22                             |                            |                            |  |
| 6             | 0.11                             |                            |                            |  |
| 7             | 1.22                             |                            |                            |  |
| 8             | 0.11                             |                            |                            |  |
| 9             | 1.00                             |                            |                            |  |
| 10            | 0.11                             |                            |                            |  |
| 11            | 0.89                             |                            |                            |  |
| 12            | 0.33                             |                            |                            |  |
| 13            | 0.11                             |                            |                            |  |
| 14            | 0.44                             |                            |                            |  |
| 15            | 0.22                             |                            |                            |  |
| 16            | 1.78                             |                            |                            |  |
| 17            | 0.22                             |                            |                            |  |
| 18            | 0.11                             |                            |                            |  |
| 19            | 0.44                             |                            |                            |  |
| 20            | 4.11                             |                            |                            |  |
| 21            | 5.11                             |                            |                            |  |
| 22            | 0.11                             |                            |                            |  |
| 23            | 1.64                             |                            |                            |  |
| 24            | 0.22                             |                            |                            |  |
| 25            | 0.11                             |                            |                            |  |
| 26            | 0.44                             |                            |                            |  |
| 27            | 0.22                             |                            |                            |  |
| <b>TOTALS</b> | <b>23.65</b>                     |                            |                            |  |

\*\*06-[7BR&BR-1,7VB-M,6BR&6,7 RS-1 & I]

|             |  |                |               |   |  |                |                             |                          |                    |               |                           |
|-------------|--|----------------|---------------|---|--|----------------|-----------------------------|--------------------------|--------------------|---------------|---------------------------|
| FILE NAME = | USER NAME = braboypc                   | DESIGNED - --- | REVISED - --- | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>BRIDGE REPAIR PLANS SN 006-0133</b> | F.A. RTE. I-80 | SECTION **                  | COUNTY BUREAU            | TOTAL SHEETS 344   | SHEET NO. 247 |                           |
|             | PLOT SCALE = 50.0638' / IN.            | CHECKED - ---  | REVISED - --- |   |  | SCALE: _____   | SHEET NO. ___ OF ___ SHEETS | STA. _____ TO STA. _____ | CONTRACT NO. 66908 |               | ILLINOIS FED. AID PROJECT |
|             | PLOT DATE = Sep 07, 2009 - 01:47:28 PM | DATE - ---     | REVISED - --- |   |  |                |                             |                          |                    |               |                           |



 DECK SLAB REPAIR (PARTIAL)

 DECK SLAB REPAIR (FD TYPE II)

## DECK PATCHING RECORD

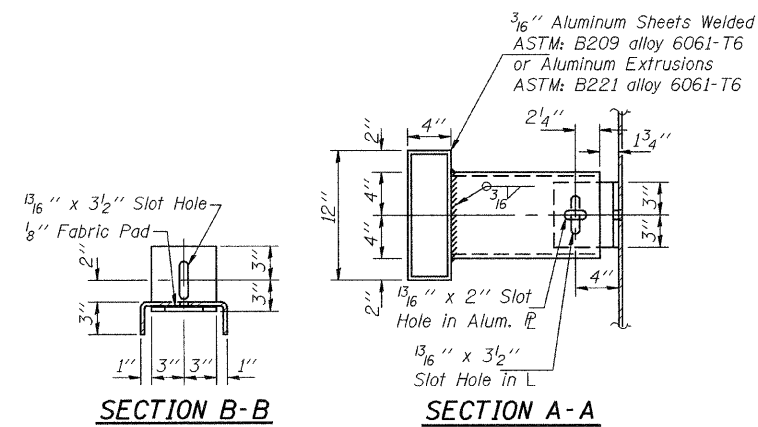
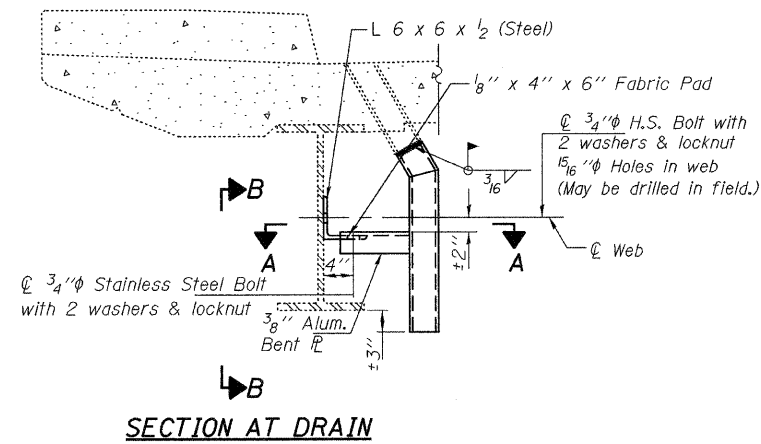
S.N. 006-0133

ACTUAL DECK PATCHING AREAS AND LOCATIONS  
SHALL BE SHOWN ON AS BUILT PLANS

**\*\*06-(7BR&BR-1,7VB-M,6BR&6,7 RS-1 & I)**

|   |                      |                |                           |   |  |                             |                          |                    |                     |                  |  |
|---|----------------------|----------------|---------------------------|---|--|-----------------------------|--------------------------|--------------------|---------------------|------------------|--|
| FILE NAME =<br>c:\pw\work\p\dot\braboyce\d0112640\d368908-sht-details.DGN | USER NAME = braboyce | DESIGNED - --- | REVISED - ---             | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>BRIDGE REPAIR PLANS SN 006-0133</b> | F.A. RTE.<br>I-80           | SECTION<br>**            | COUNTY<br>BUREAU   | TOTAL SHEETS<br>344 | SHEET NO.<br>248 |  |
| PLOT SCALE = 50.0638' / IN.   | CHECKED - ---        | REVISED - ---  | SCALE: _____              |   |  | SHEET NO. ___ OF ___ SHEETS | STA. _____ TO STA. _____ | CONTRACT NO. 66908 |                     |                  |  |
| PLOT DATE = Sep 07, 2009 - 01:47:19 PM                                    | DATE - _____         | REVISED - ---  | ILLINOIS FED. AID PROJECT |   |  |                             |                          |                    |                     |                  |  |
|   |                      |                |                           |   |  |                             |                          |                    |                     |                  |  |



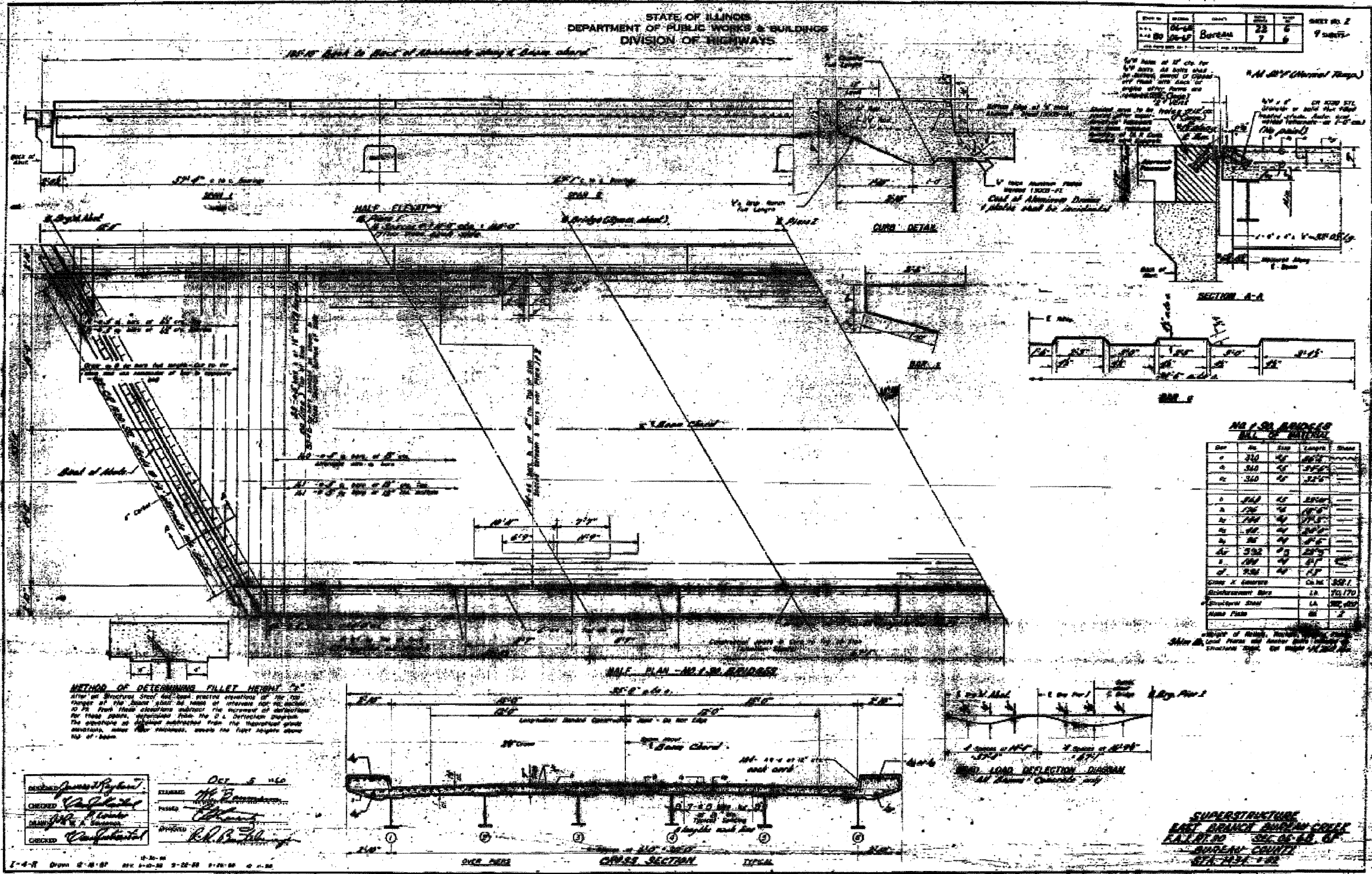


DRAIN EXTENSION DETAIL

\*\*06-I7BR&BR-1,7VB-M,6BR&6,7 RS-1 &I1

|             |                      |            |           |   |  |                    |           |        |              |           |         |
|-------------|----------------------|------------|-----------|---|--|--------------------|-----------|--------|--------------|-----------|---------|
| FILE NAME = | USER NAME = #USER#   | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>BRIDGE REPAIR PLANS SN 006-0133</b> | F.A. RTE.          | SECTION   | COUNTY | TOTAL SHEETS | SHEET NO. |         |
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|             | PLOT DATE = #DATE#   | CHECKED -  | REVISED - |   |  | CONTRACT NO. 66908 |           |        |              |           |         |
|             |                      | DATE -     | REVISED - |   |  | SCALE:             | SHEET NO. | OF     | SHEETS       | STA.      | TO STA. |

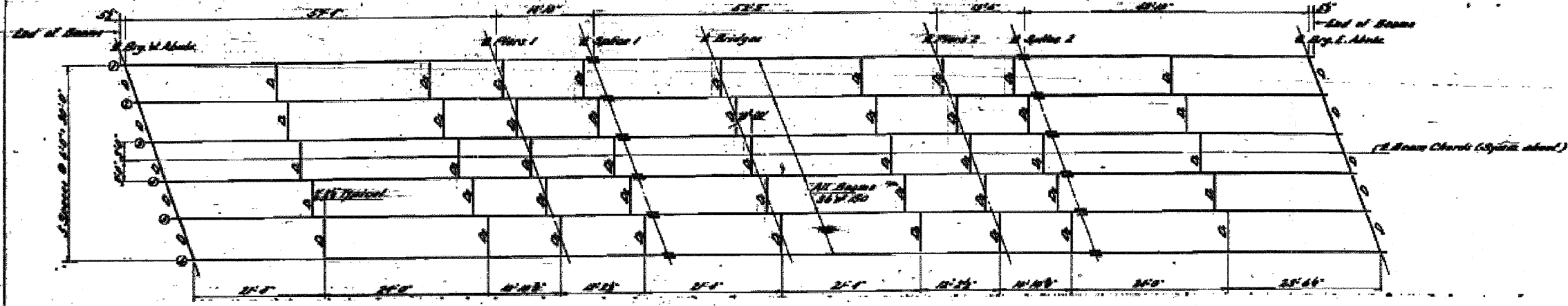




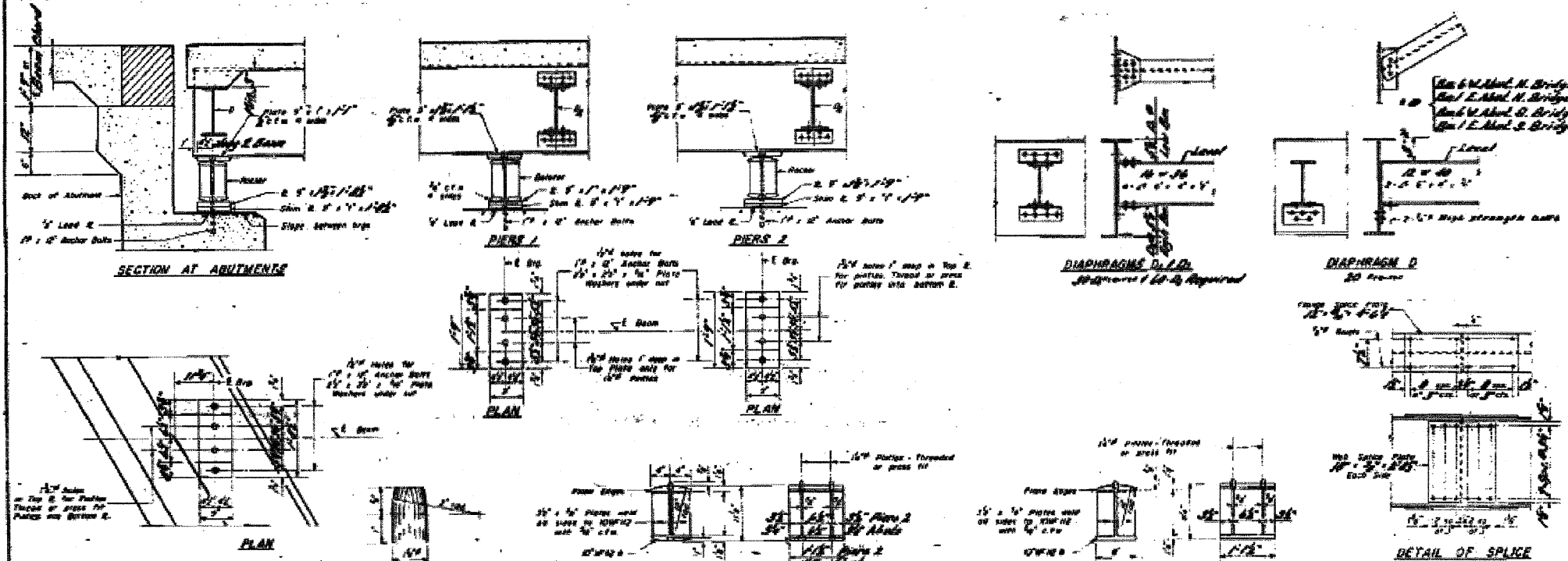
STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|       |       |        |       |        |
|-------|-------|--------|-------|--------|
| DATE  | NO.   | BY     | CHKD. | APP'D. |
| 10-25 | 10-25 | Bureau | 7     | 7      |

SHEET NO. 4  
9 SHEETS



FRAMING PLAN - NO. 1 SO. BRIDGES



ELEVATION TOP OF BEAMS

|          | North Bridge |        |        |        |        |        |
|----------|--------------|--------|--------|--------|--------|--------|
|          | Beam 1       | Beam 2 | Beam 3 | Beam 4 | Beam 5 | Beam 6 |
| Abutment | 661.00       | 661.00 | 661.00 | 661.00 | 661.00 | 661.00 |
| Pier 1   | 661.00       | 661.00 | 661.00 | 661.00 | 661.00 | 661.00 |
| Splice 1 | 661.00       | 661.00 | 661.00 | 661.00 | 661.00 | 661.00 |
| Pier 2   | 661.00       | 661.00 | 661.00 | 661.00 | 661.00 | 661.00 |
| Splice 2 | 661.00       | 661.00 | 661.00 | 661.00 | 661.00 | 661.00 |
| Abutment | 661.00       | 661.00 | 661.00 | 661.00 | 661.00 | 661.00 |

|          | South Bridge |        |        |        |        |        |
|----------|--------------|--------|--------|--------|--------|--------|
|          | Beam 1       | Beam 2 | Beam 3 | Beam 4 | Beam 5 | Beam 6 |
| Abutment | 661.00       | 661.00 | 661.00 | 661.00 | 661.00 | 661.00 |
| Pier 1   | 661.00       | 661.00 | 661.00 | 661.00 | 661.00 | 661.00 |
| Splice 1 | 661.00       | 661.00 | 661.00 | 661.00 | 661.00 | 661.00 |
| Pier 2   | 661.00       | 661.00 | 661.00 | 661.00 | 661.00 | 661.00 |
| Splice 2 | 661.00       | 661.00 | 661.00 | 661.00 | 661.00 | 661.00 |
| Abutment | 661.00       | 661.00 | 661.00 | 661.00 | 661.00 | 661.00 |

NOTE: Elev. are @ top of top flanges.

TABLE OF L-DIMENSIONS

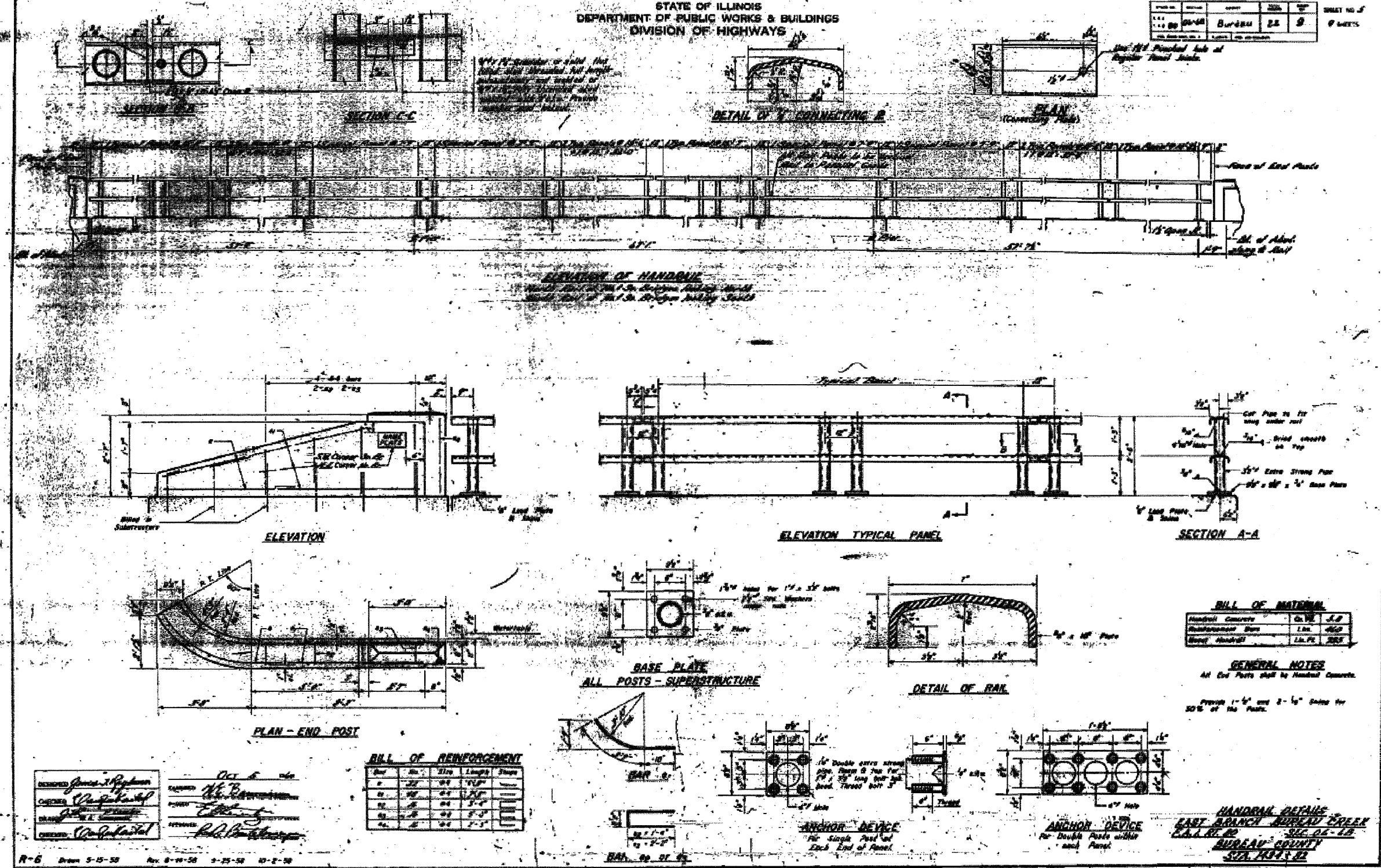
| Beam     | North Bridge |     |     | South Bridge |     |     |
|----------|--------------|-----|-----|--------------|-----|-----|
|          | 1            | 2   | 3   | 1            | 2   | 3   |
| Span     | 10'          | 10' | 10' | 10'          | 10' | 10' |
| Pier 1   | 10'          | 10' | 10' | 10'          | 10' | 10' |
| Pier 2   | 10'          | 10' | 10' | 10'          | 10' | 10' |
| Abutment | 10'          | 10' | 10' | 10'          | 10' | 10' |

DESIGNED: *John J. Ryburn*  
 CHECKED: *W. J. ...*  
 DATE: *Oct 5 1925*

STRUCTURAL STEEL  
 EAST RIVER BRIDGE  
 F.A.L. & S. CO. OF ILL.  
 BUREAU COUNTY  
 STA. 100 + 00

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|             |         |        |              |           |
|-------------|---------|--------|--------------|-----------|
| PROJECT NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 11-80       | 60-48   | Bureau | 344          | 253       |



DESIGNED: James J. Pappas  
CHECKED: [Signature]  
DRAWN: [Signature]  
DATE: [Signature]

R-6 Drawn 5-15-58 Rev. 8-11-58 9-25-58 10-2-58

|   |  |            |           |
|---|--|------------|-----------|
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|   | PLOT SCALE = 50.0638' / IN.            | CHECKED -  | REVISED - |
|   | PLOT DATE = Sep 07, 2009 - 01:42:56 PM | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

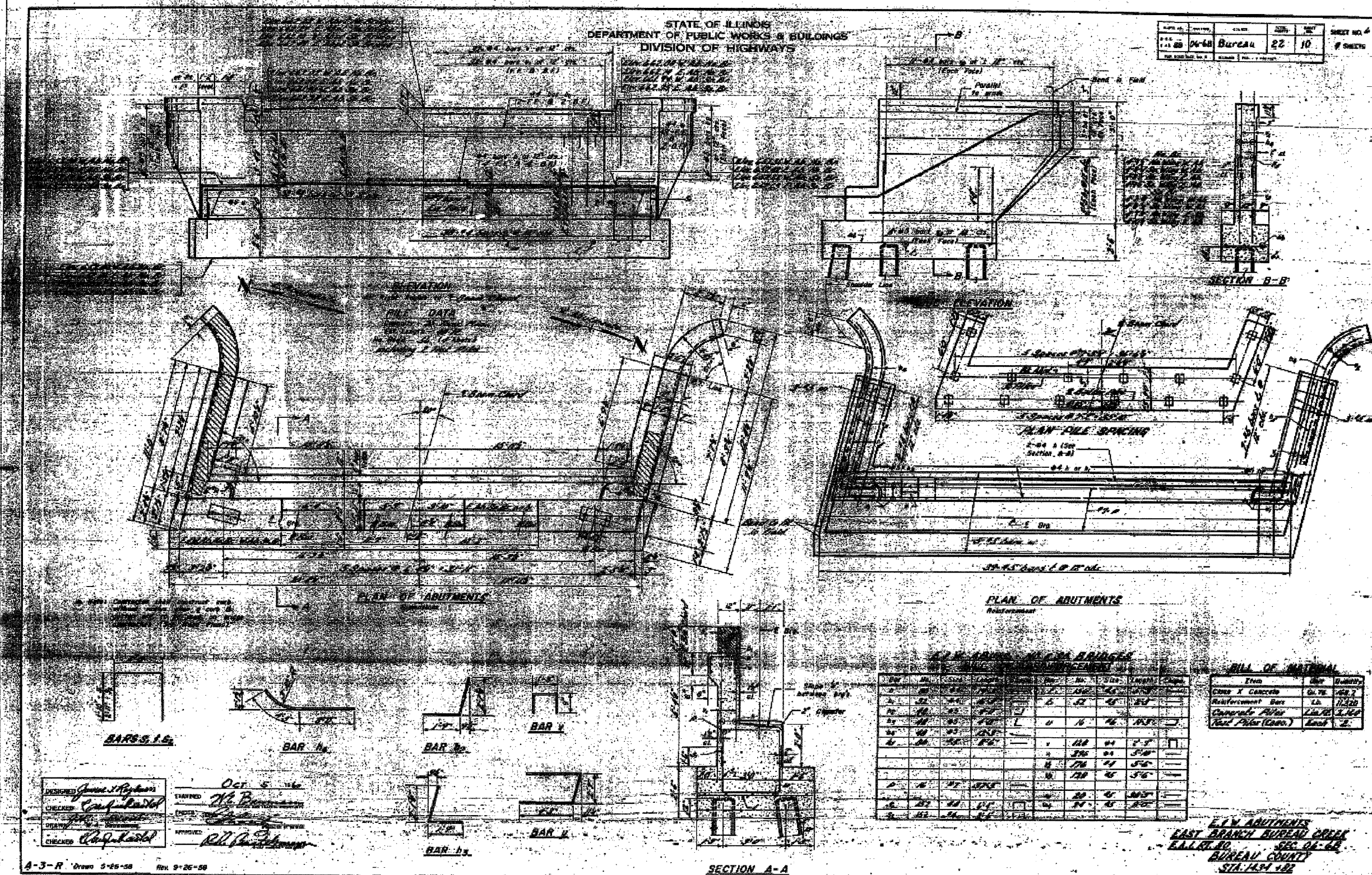
EXISTING BRIDGE PLANS (FOR INFORMATION ONLY)

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_\_ OF \_\_\_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.J. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-80                      | **      | BUREAU | 344          | 253       |
|                           |         |        | 66908        |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|              |       |        |       |           |
|--------------|-------|--------|-------|-----------|
| PROJECT NO.  | DATE  | BY     | SCALE | SHEET NO. |
| 11-88        | 10-68 | Bureau | 22    | 10        |
| TOTAL SHEETS |       |        |       | 27 SHEETS |



**TABLE OF MATERIALS**

| NO. | QTY. | DESCRIPTION        | UNIT    | QTY. | UNIT    | QTY. | UNIT    | QTY. | UNIT    |
|-----|------|--------------------|---------|------|---------|------|---------|------|---------|
| 1   | 1.00 | Concrete           | cu. yd. | 1.00 | cu. yd. | 1.00 | cu. yd. | 1.00 | cu. yd. |
| 2   | 1.00 | Reinforcement Bars | lb.     | 1.00 | lb.     | 1.00 | lb.     | 1.00 | lb.     |
| 3   | 1.00 | Formwork           | sq. ft. | 1.00 | sq. ft. | 1.00 | sq. ft. | 1.00 | sq. ft. |

DESIGNED: *James S. Reynolds*  
 CHECKED: *Confidential*  
 DATE: *Oct 5 1968*

STAMPED: *H. B. [Signature]*  
 CHECKED: *[Signature]*  
 APPROVED: *R. J. [Signature]*

A-3-R Drawn 5-26-58 Rev 9-26-58

|   |                      |            |           |
|---|----------------------|------------|-----------|
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| PLOT SCALE = 50.0638' / IN.                                 | CHECKED -            | REVISED -  | REVISED - |
| PLOT DATE = Sep 07, 2009 - 01:42:47 PM                      | DATE -               | REVISED -  | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING BRIDGE PLANS (FOR INFORMATION ONLY)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

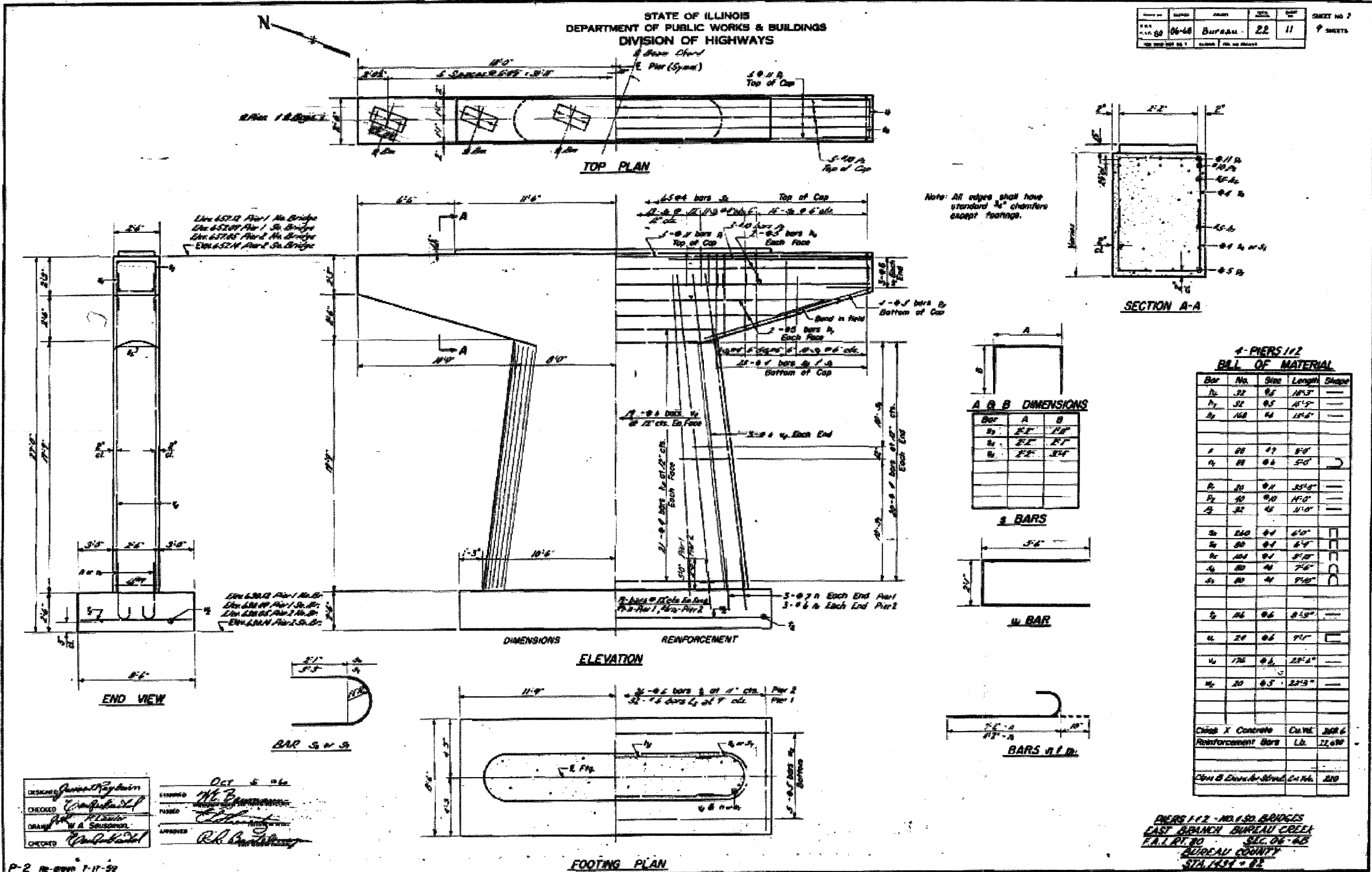
\*\*06-[7BR&BR-1,7VB-M,6BR&6.7 RS-1 & 1

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|--------|--------------|-----------|
| I-80        | **      | BUREAU | 344          | 254       |
|             |         |        |              | 66908     |

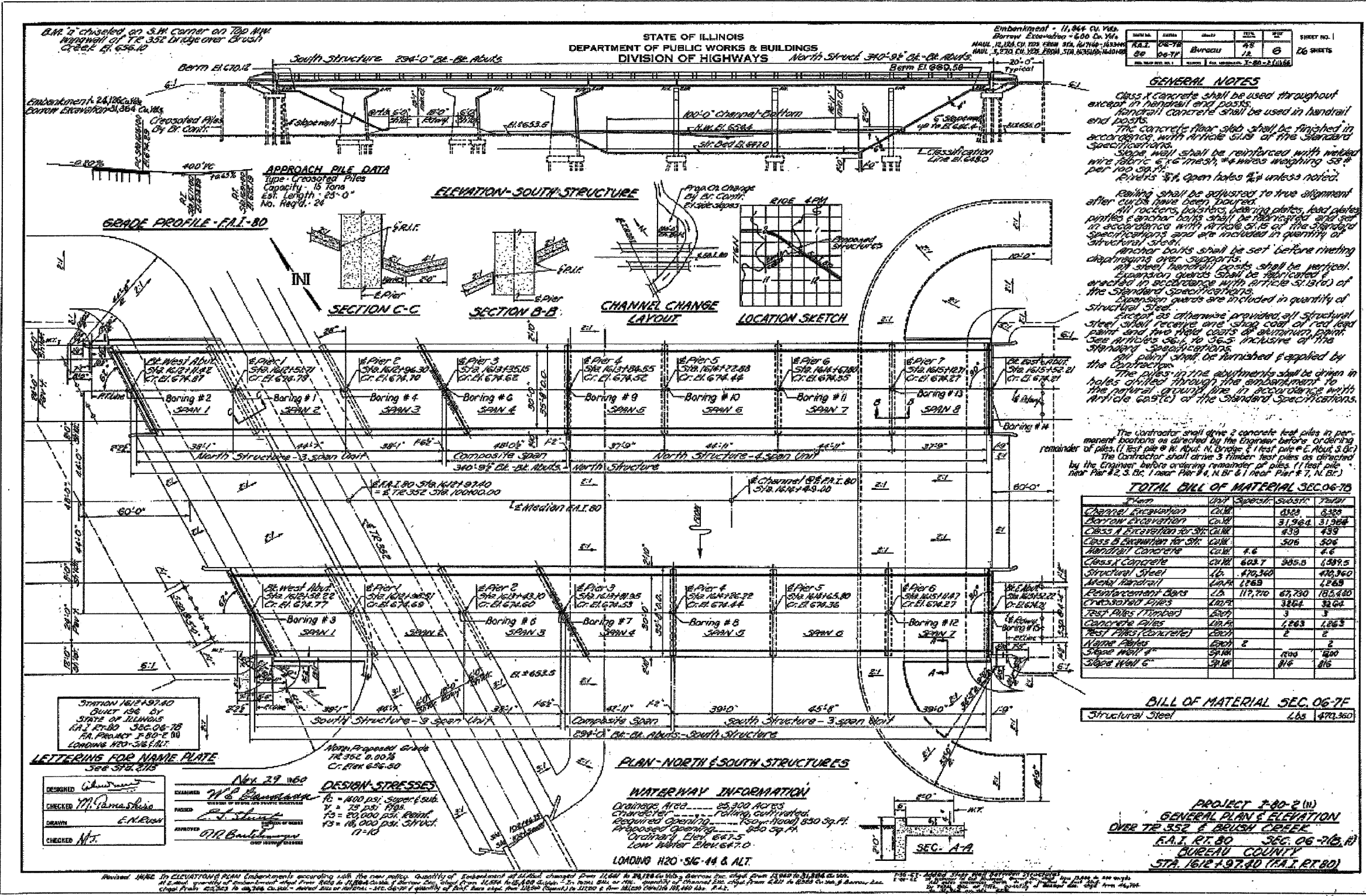
ILLINOIS FED. AID PROJECT

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|             |       |        |    |    |             |
|-------------|-------|--------|----|----|-------------|
| PROJECT NO. | 06-48 | BUREAU | 22 | 11 | SHEET NO. 7 |
|             |       |        |    |    | 9 SHEETS    |



DESIGNED: *James P. ...*  
CHECKED: *...*  
DRAWN: *...*  
DATE: *Oct 5 '06*



|          |       |      |       |
|----------|-------|------|-------|
| DATE     | BY    | CHKD | APP'D |
| 08-27-78 | BR/BO | 12   | 6     |
| 08-27-78 | BR/BO | 12   | 6     |

**GENERAL NOTES**

Class X concrete shall be used throughout except in handrail end posts.

Handrail concrete shall be used in handrail end posts.

The concrete floor slab shall be finished in accordance with Article 615 of the Standard Specifications.

Slope wall shall be reinforced with welded wire fabric 6"x6" mesh, #4 wires weighing 50# per 100 sq. ft.

Rivers 3/4" open holes 1/4" unless noted.

Reinforcing shall be adjusted to true alignment after curbs have been poured.

All rockers, bolsters, bearing plates, lead plates, pintles & anchor bolts shall be fabricated and set in accordance with Article 615 of the Standard Specifications and are included in quantity of structural steel.

Anchor bolts shall be set before riveting diaphragms over supports.

All steel handrail posts shall be vertical.

Expansion guards shall be fabricated & erected in accordance with Article 615 of the Standard Specifications.

Expansion guards are included in quantity of structural steel.

Except as otherwise provided all structural steel shall receive one coating of red lead paint in accordance with Article 615 of the Standard Specifications.

All paint shall be furnished & applied by the Contractor.

The piles in the abutments shall be driven in holes drilled through the embankment to the natural ground line in accordance with Article 605(a) of the Standard Specifications.

The Contractor shall drive 2 concrete test piles in permanent locations as directed by the Engineer before ordering remainder of piles. (1 test pile @ N. Abut. N. Bridge & 1 test pile @ E. Abut. S. Br.)

The Contractor shall drive 3 timber test piles as directed by the Engineer before ordering remainder of piles. (1 test pile near Pier #2, S. Br.; 1 near Pier #4, N. Br. & 1 near Pier #7, N. Br.)

**TOTAL BILL OF MATERIAL SEC. 06-7E**

| Item                        | Unit    | Quantity | Unit Price | Total    |
|-----------------------------|---------|----------|------------|----------|
| Channel Excavation          | Cu Yd   | 4329     | 8.38       | 36276    |
| Borrow Excavation           | Cu Yd   | 31964    | 31.96      | 1021500  |
| Class X Excavation for 3/4" | Cu Yd   | 439      | 4.39       | 1907     |
| Class X Excavation for 3/4" | Cu Yd   | 506      | 5.06       | 2560     |
| Handrail Concrete           | Cu Yd   | 4.6      | 4.6        | 21       |
| Class X Concrete            | Cu Yd   | 603.7    | 985.5      | 594885   |
| Structural Steel            | Lb.     | 470360   | 470.36     | 17540000 |
| Handrail                    | Lb.     | 1269     | 1.27       | 1611     |
| Reinforcement Bars          | Lb.     | 117,710  | 67.70      | 7968000  |
| Crossed Piles               | Lb.     | 3364     | 33.64      | 113000   |
| Test Piles (Timber)         | Each    | 3        | 3          | 9        |
| Concrete Piles              | Lb.     | 1263     | 126.3      | 159500   |
| Test Piles (Concrete)       | Each    | 2        | 2          | 4        |
| Name Plates                 | Each    | 2        | 2          | 4        |
| Slope Wall #                | Sq. Ft. | 100      | 100        | 10000    |
| Slope Wall #                | Sq. Ft. | 114      | 114        | 12996    |

**BILL OF MATERIAL SEC. 06-7E**

|                  |     |         |
|------------------|-----|---------|
| Structural Steel | Lb. | 470,360 |
|------------------|-----|---------|

**APPROACH PILE DATA**

Type: Crossed Piles

Capacity: 15 tons

Est. Length: 23'-0"

No. Hqs: 24

**GRADE PROFILE - F.A.I.-80**

**SECTION C-C**

**SECTION B-B**

**CHANNEL CHANGE LAYOUT**

**LOCATION SKETCH**

**PLAN-NORTH & SOUTH STRUCTURES**

**ELEVATION-SOUTH STRUCTURE**

**DESIGN STRESSES**

R<sub>c</sub> = 1800 psi, Super f. sub.

V = 25 psi, 199

F<sub>s</sub> = 20,000 psi, Rivet.

F<sub>s</sub> = 18,000 psi, Struct.

17-10

**WATERWAY INFORMATION**

Drainage Area: 25,300 ACRES

Channel: 100' x 100'

Proposed Opening: 650 Sq. Ft.

Ordinary Elev: 647.5

Low Water Elev: 647.0

LOADING H20-516-44 & ALT.

**LETTERING FOR NAME PLATE**

See 372, 215

**DESIGNED** [Signature]

**CHECKED** [Signature]

**DRAWN** [Signature]

**CHECKED** [Signature]

**NOV 29 1960**

**DESIGNED STRESSES**

**WATERWAY INFORMATION**

**LETTERING FOR NAME PLATE**

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**DRAWN** [Signature]

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**NOV 29 1960**

**DESIGNED STRESSES**

**WATERWAY INFORMATION**

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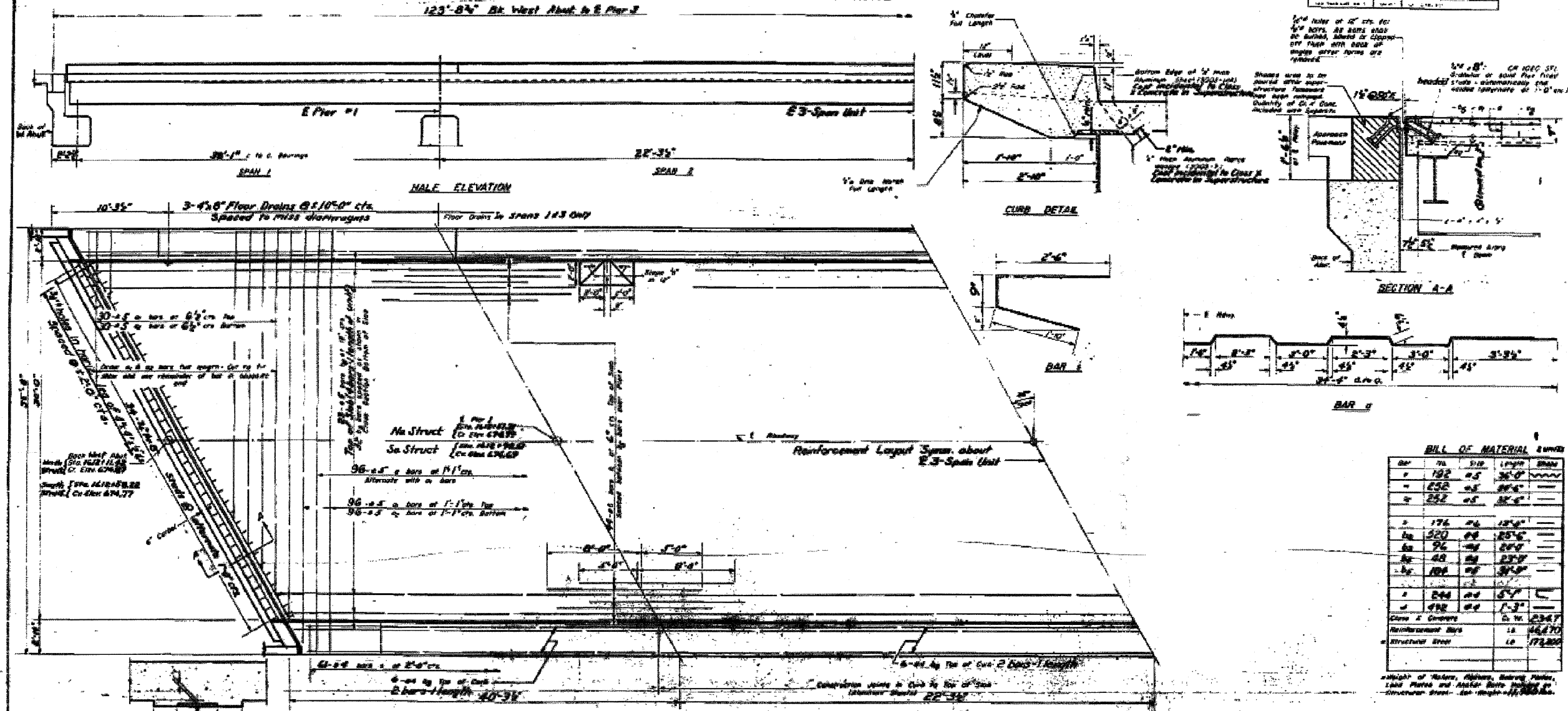
**CHECKED** [Signature]

**NOV 29 1960**



STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|          |         |        |              |           |
|----------|---------|--------|--------------|-----------|
| FILE NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 067B     | Bureau  | 45     | 7            | 26        |

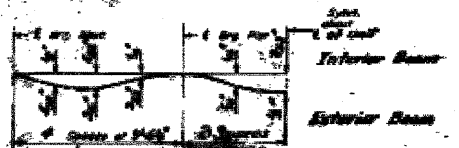
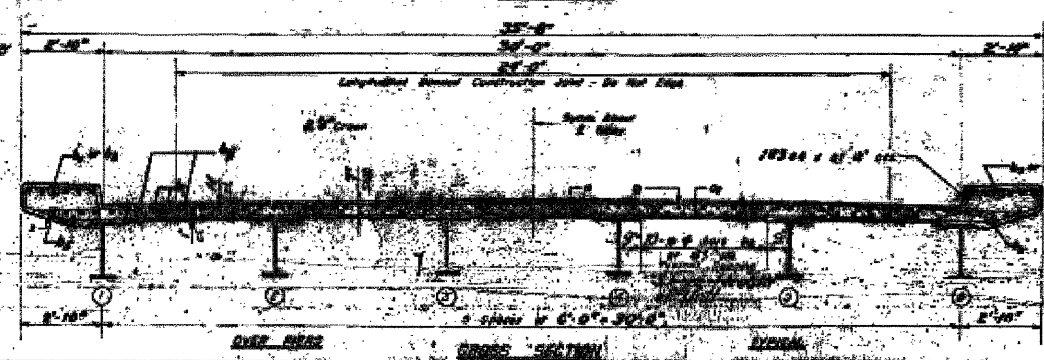


**BILL OF MATERIAL**

| NO.                | QTY | SIZE | LENGTH | WEIGHT |
|--------------------|-----|------|--------|--------|
| 1                  | 152 | #5   | 36'-0" |        |
| 2                  | 252 | #5   | 36'-0" |        |
| 3                  | 176 | #6   | 18'-0" |        |
| 4a                 | 520 | #6   | 25'-0" |        |
| 4b                 | 76  | #6   | 36'-0" |        |
| 5a                 | 48  | #6   | 27'-0" |        |
| 5b                 | 108 | #6   | 36'-0" |        |
| 6                  | 264 | #6   | 6'-0"  |        |
| 7                  | 492 | #6   | 7'-3"  |        |
| Class 2 Concrete   |     |      |        | 5.74   |
| Reinforcement Bars |     |      |        | 13     |
| Structural Steel   |     |      |        | 17,820 |

**METHOD OF DETERMINING FILLET HEIGHT "F"**  
After the structural steel has been erected elevations of the top flange of the beam shall be taken at intervals...  
The elevations are obtained subtracted from the structural steel elevations, minus floor thickness, equals the fillet height above top of beam.

DESIGNED: *Calvin...*  
CHECKED: *M. Parnell*  
DRAWN: *W. S. Robinson*  
DATE: *Nov 28 1920*

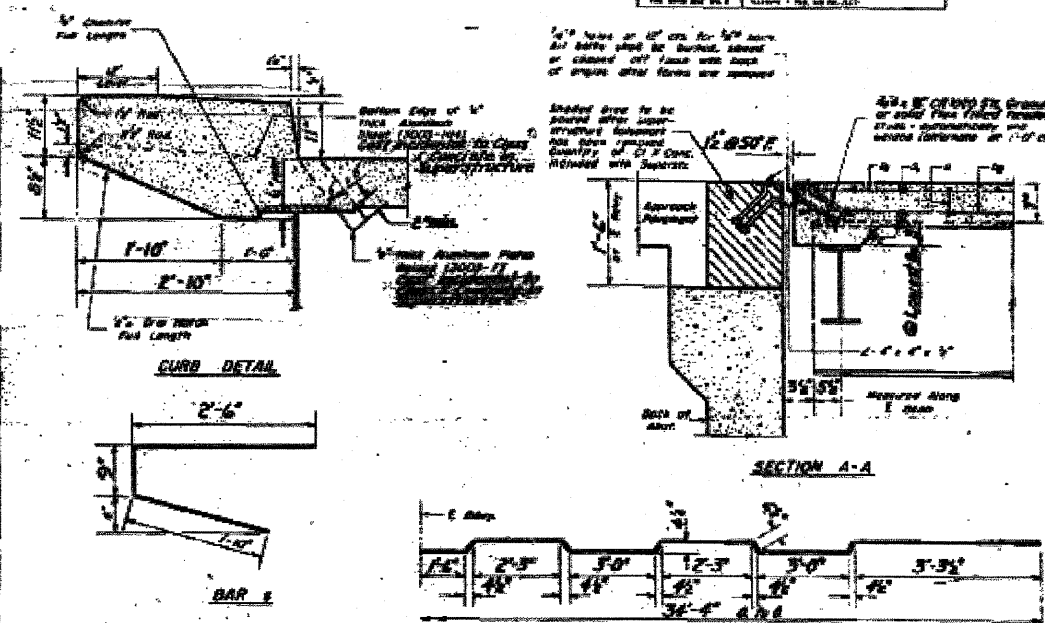
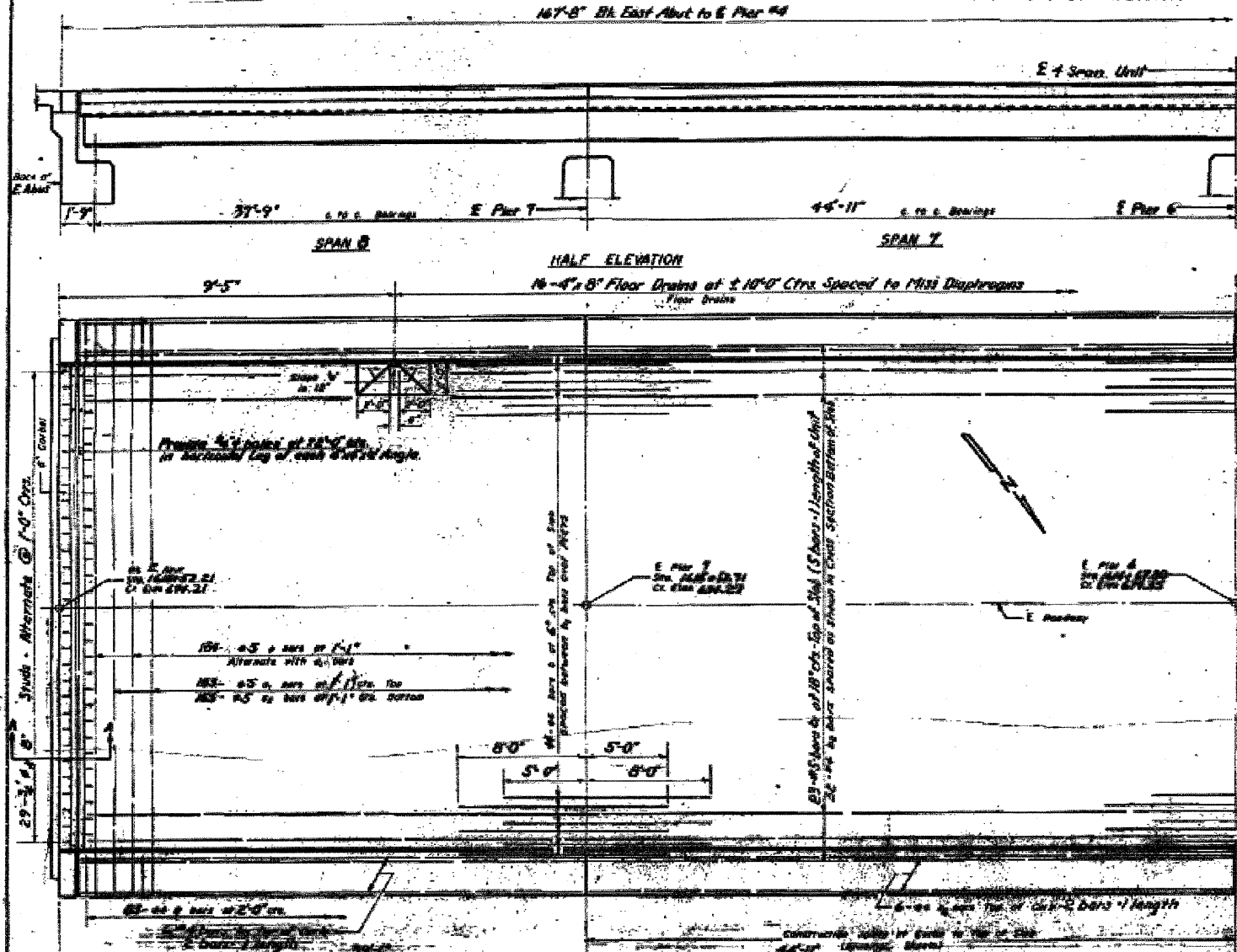


**DEAD LOAD DEFLECTIVE CURVATURE**  
Should be subtracted from total curvature  
the weight of structural steel.

**SLAB BEAMS  
3-SPAN UNIT SKewed  
NORTH AND SOUTH STRUCTURES  
OVER I-55 & BRUSH CREEK  
F.R.I. R.L. 80 SEC. 01-78  
BUREAU COUNTY  
STA. 142+00.00 (F.R.I. R.L. 80)**

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

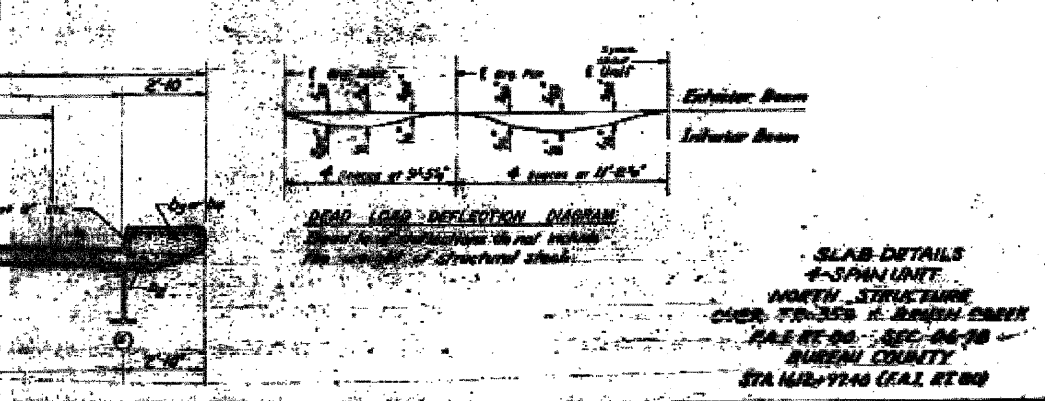
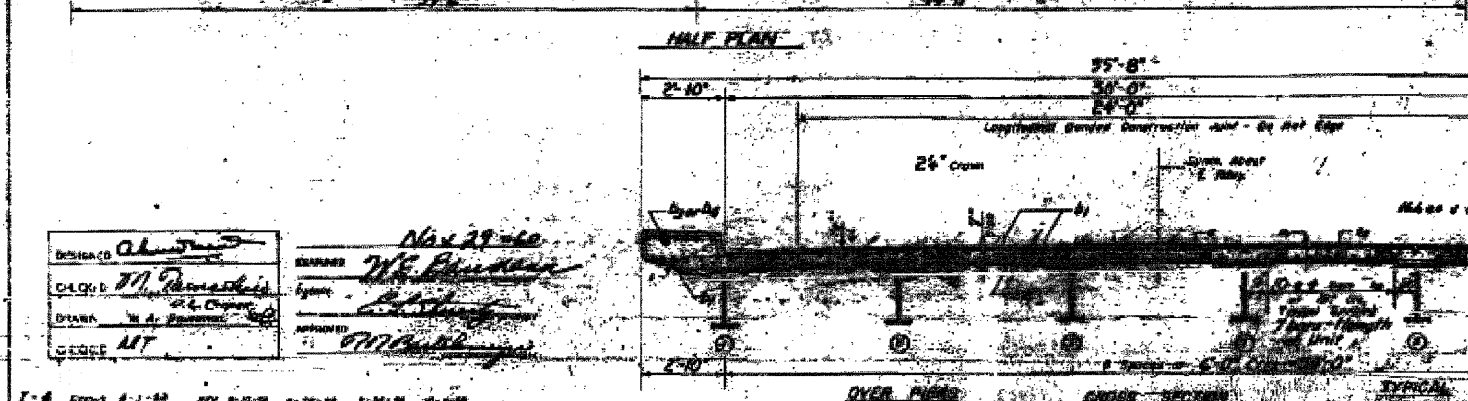
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|-------------|---------|--------|-------|-----------|--------------|
| PROJECT NO. | SECTION | DATE   | SCALE | SHEET NO. | TOTAL SHEETS |
| 80          | 20-18   | Bureau | 45    | 8         | 26           |



**METHOD OF DETERMINING FILET HEIGHT "Y"**  
After an structural steel bar has been erected, measure the height of the flange of the beam, and the height of the web, and the distance between the flange and the web. From these measurements subtract the amount of deflection for the web, determined from the I. L. deflection tables. The difference of obtained subtracted from the measured height, minus four tenths, equals the fillet height along top of beam.

**BILL OF MATERIAL**

| Bar No.           | Size | Length  | Weight |
|-------------------|------|---------|--------|
| 134               | #5   | 360'    |        |
| 135               | #5   | 34'     |        |
| 136               | #5   | 32'     |        |
| 137               | #6   | 18'     |        |
| 138               | #5   | 38'     |        |
| 139               | #4   | 25'     |        |
| 140               | #4   | 20'     |        |
| 141               | #4   | 20'     |        |
| 142               | #4   | 1'-0"   |        |
| 143               | #4   | 1'-0"   |        |
| Class of Concrete |      | Co. No. | 1722   |
| Reinforcing Bars  |      | L.S.    | 31,500 |
| Structural Steel  |      | L.S.    | 15,549 |

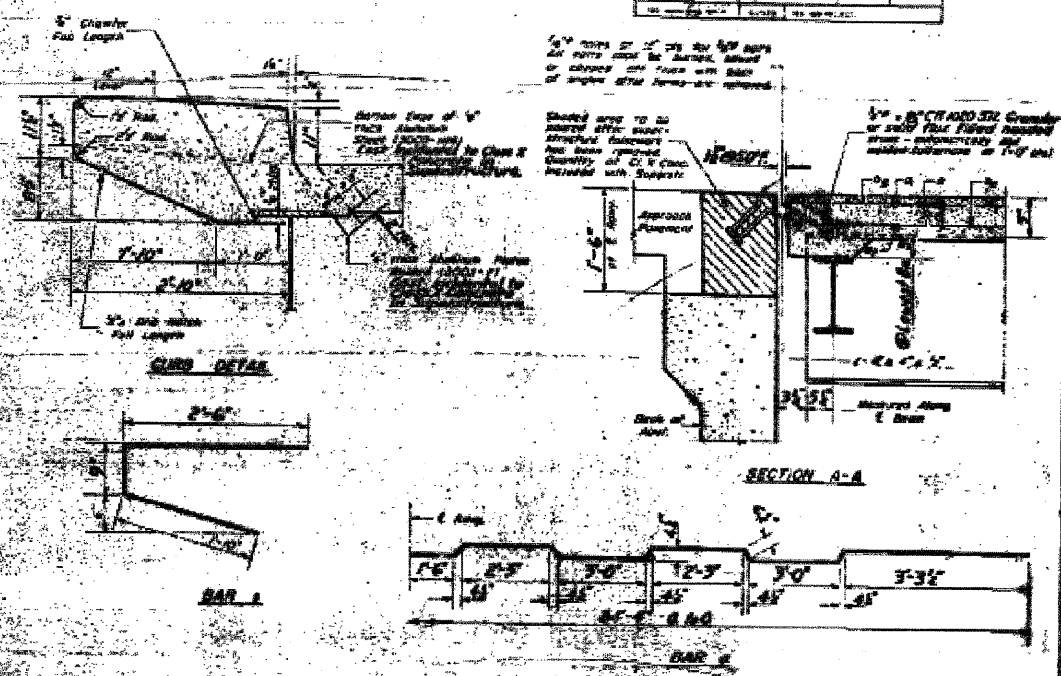
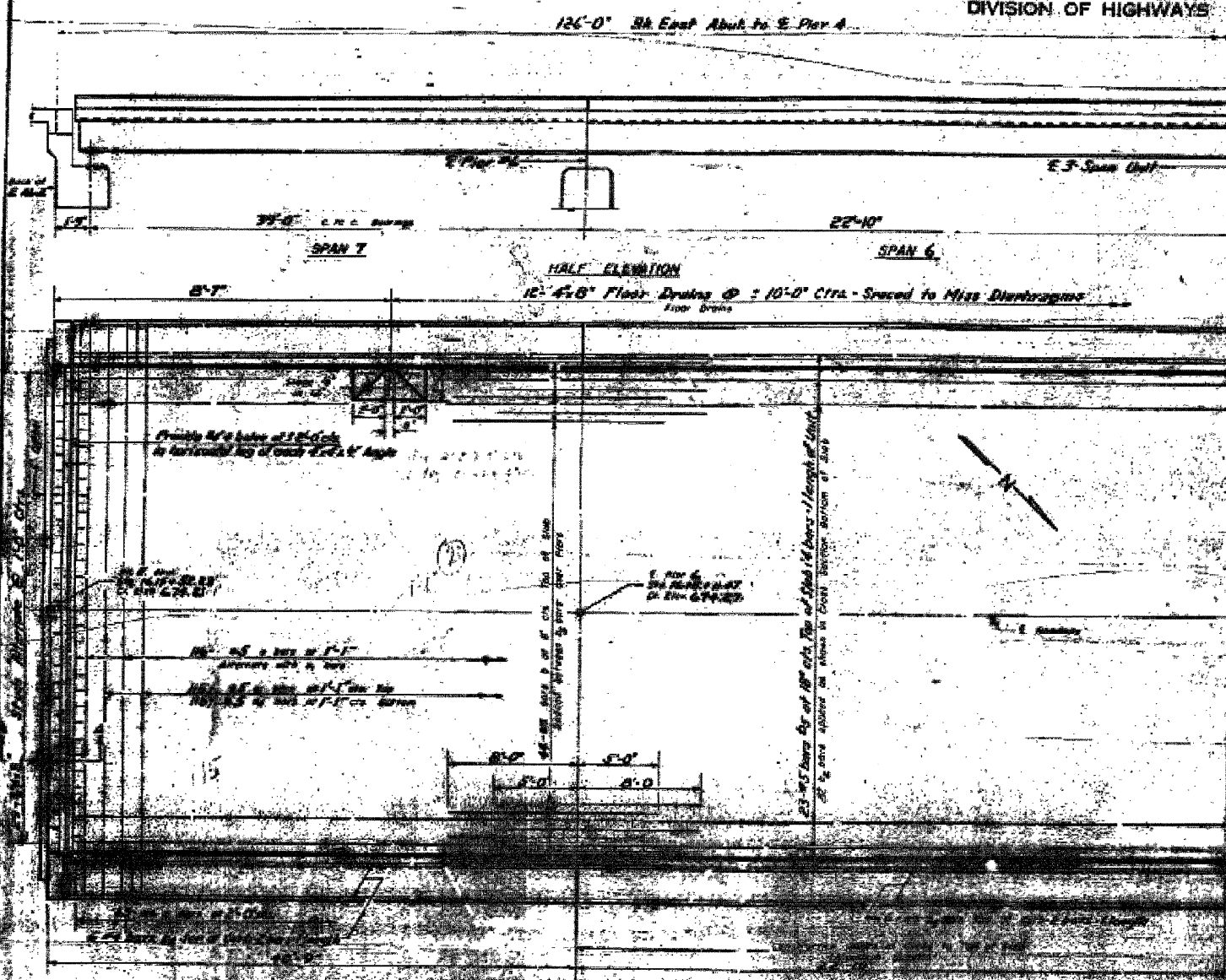


DESIGNED: *Ch...*  
CHECKED: *M. P...*  
DRAWN: *M. P...*  
SCALE: *1/4" = 1'-0"*  
DATE: *Nov 29 1960*

**SLAB DETAILS**  
4-SPAN UNIT  
NORTH STRUCTURE  
OVER TR-350 A. BROWN CREEK  
RTE 87-80 - SEC 06-18  
BUREAU COUNTY  
STA 142+77.40 (EAL RT 80)

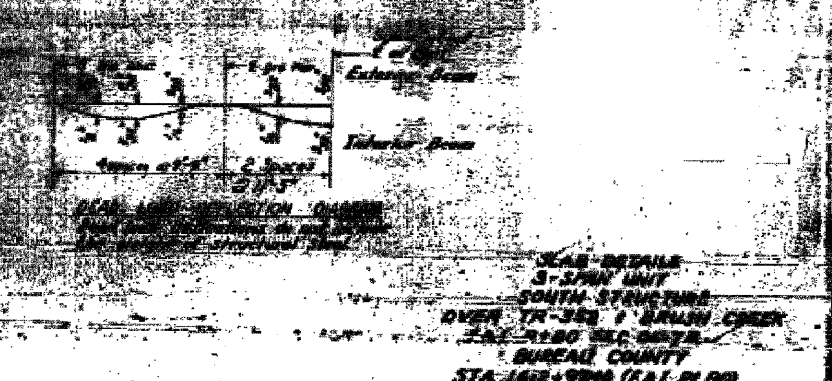
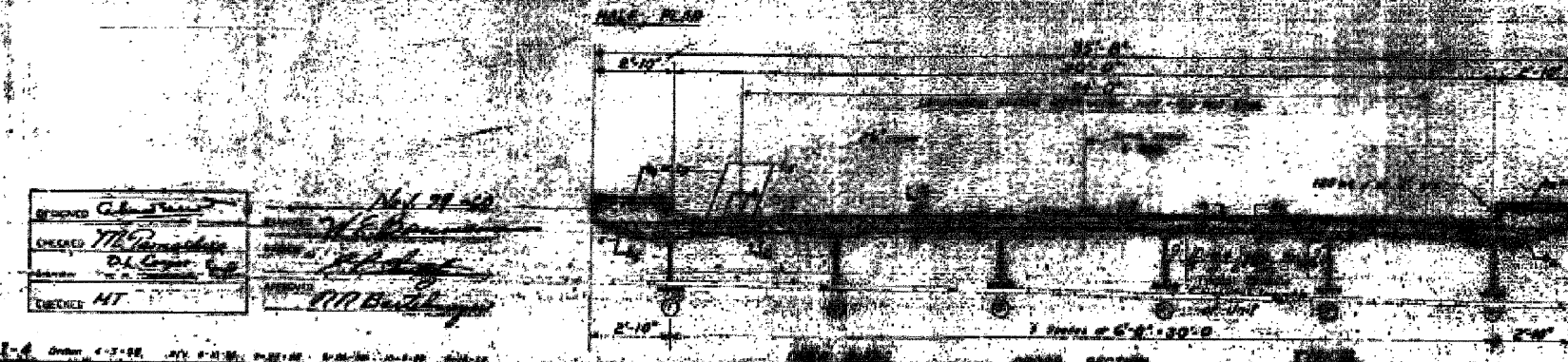
STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|         |     |                 |      |      |              |
|---------|-----|-----------------|------|------|--------------|
| DATE    | NO. | BY              | REV. | DATE | SHEET NO. OF |
| 10/1/52 | 10  | W. J. B. Bureau | 45   | 0    | 26 SHEETS    |



**BILL OF MATERIALS**

| NO. | QTY | SIZE | LENGTH | WEIGHT |
|-----|-----|------|--------|--------|
| 1   | 116 | #5   | 36'-0" | 1160   |
| 2   | 110 | #6   | 32'-0" | 1100   |
| 3   | 118 | #8   | 32'-0" | 1180   |
| 4   | 85  | #6   | 18'-0" | 850    |
| 5   | 550 | #4   | 25'-0" | 5500   |
| 6   | 48  | #6   | 25'-0" | 480    |
| 7   | 32  | #6   | 25'-0" | 320    |
| 8   | 32  | #6   | 25'-0" | 320    |
| 9   | 125 | #4   | 6'-0"  | 1250   |
| 10  | 800 | #4   | 7'-0"  | 8000   |
| 11  | 125 | #4   | 1'-0"  | 1250   |
| 12  | 10  | #4   | 6'-0"  | 1000   |

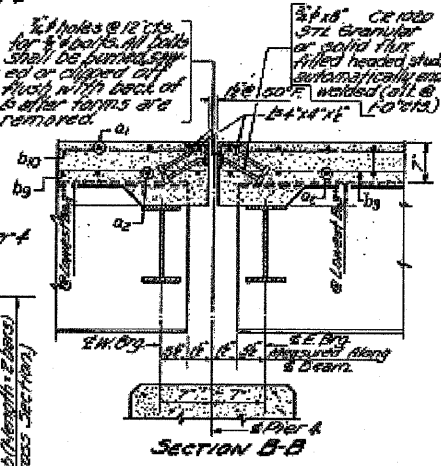
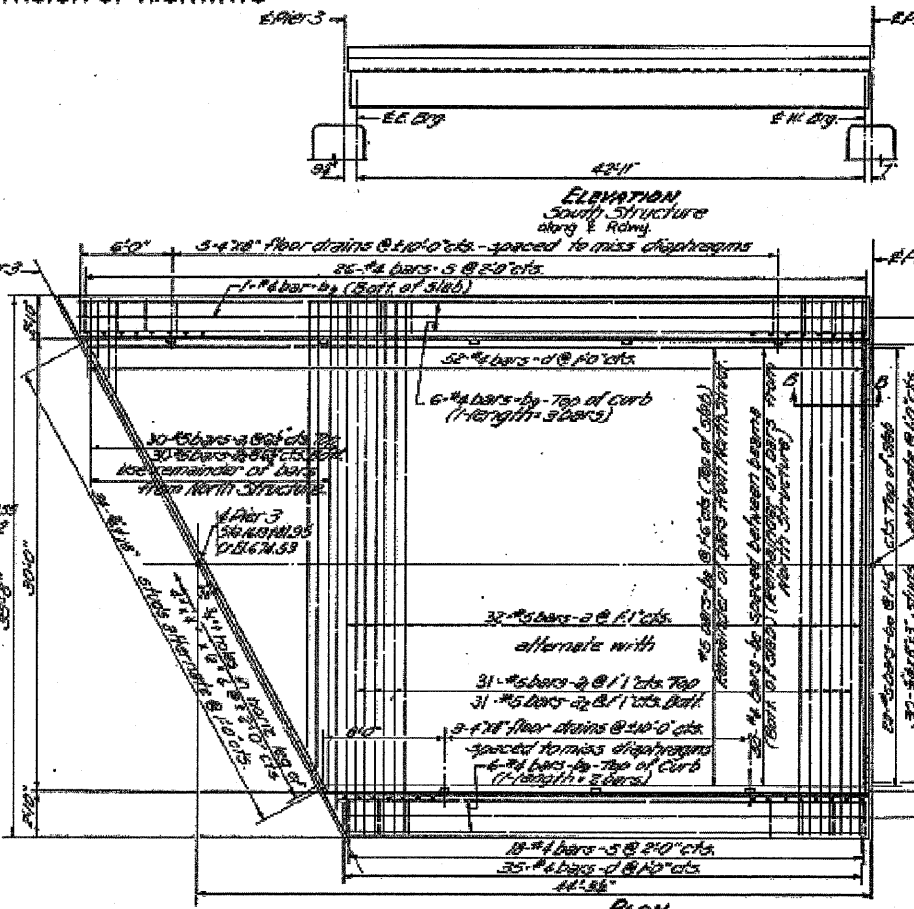
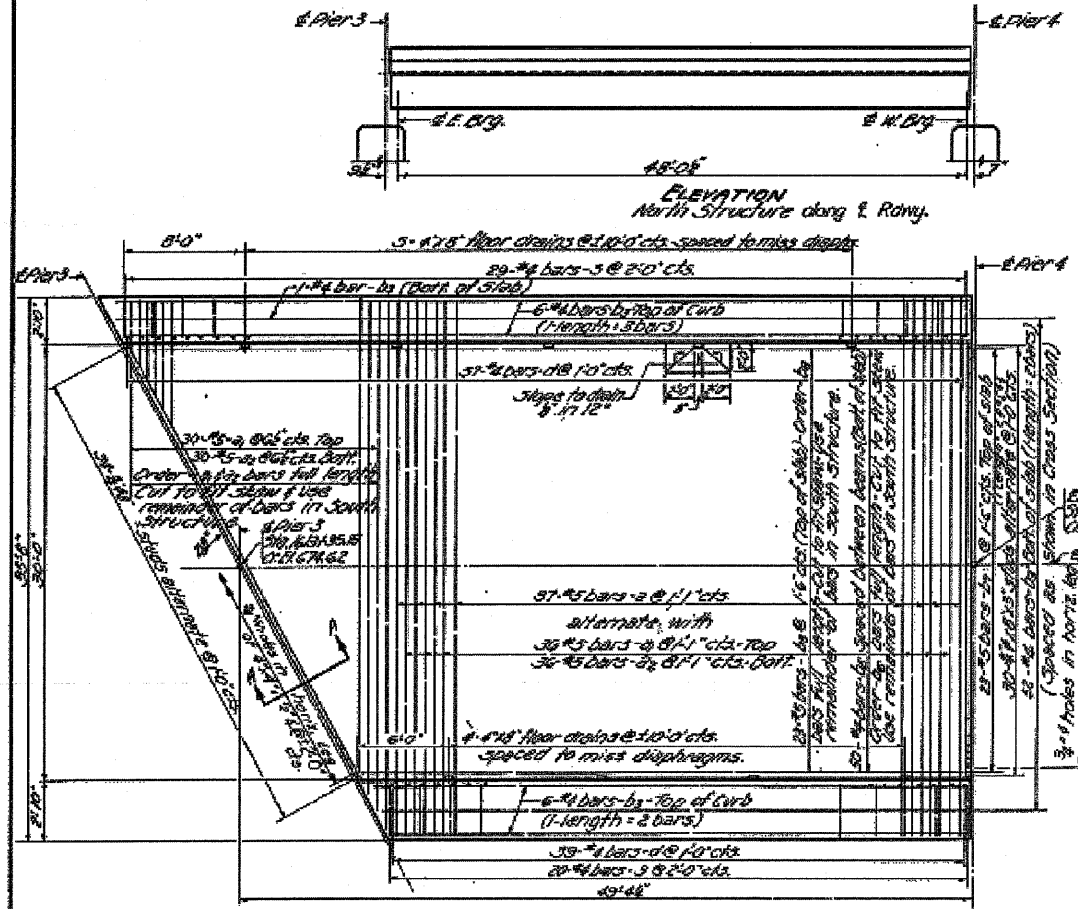


DESIGNED: *W. J. B. Bureau*  
CHECKED: *W. J. B. Bureau*  
DATE: *10/1/52*

SLAB DETAILS  
3-SPAN UNIT  
SOUTH STEEL-TIE  
OVER TR-300 & BRUSH CREEK  
2.2' x 18.0' SLAB DATA  
BUREAU COUNTY  
STA. 1422+00.00 (K&L RE 00)

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

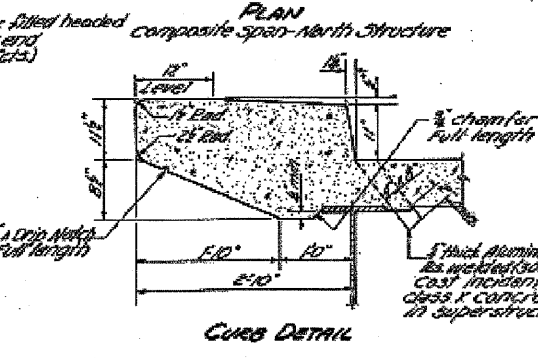
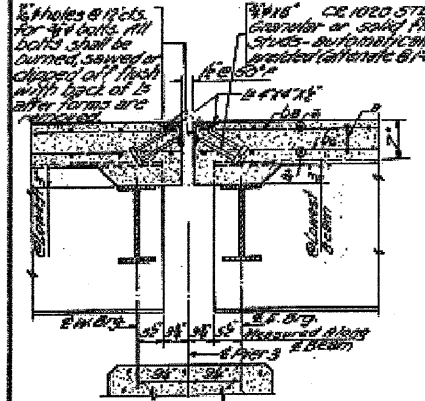
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|---------------|---------------|---------------|---------------|---------------|
| DATE          | SECTION       | COUNTY        | TOTAL SHEETS  | SHEET NO.     |
| F.A.I. 80     | 06-7B         | Bureau        | 344           | 260           |
| NO. OF SHEETS | NO. OF SHEETS | NO. OF SHEETS | NO. OF SHEETS | NO. OF SHEETS |
| 26            | 7             | 7             | 7             | 7             |



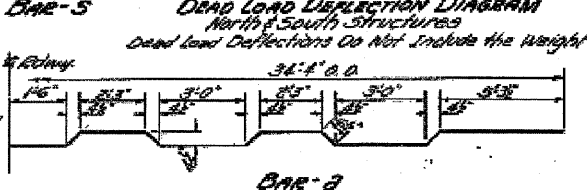
**BILL OF MATERIAL**

| BAR            | No. | SIZE | LENGTH | SHAPE |
|----------------|-----|------|--------|-------|
| a              | 69  | #5   | 36'-0" | U     |
| b              | 97  | #5   | 36'-0" | U     |
| c              | 97  | #5   | 32'-6" | U     |
| d <sub>1</sub> | 134 | #4   | 20'-0" | U     |
| d <sub>2</sub> | 50  | #4   | 17'-3" | U     |
| d <sub>3</sub> | 46  | #5   | 21'-0" | U     |
| d <sub>4</sub> | 23  | #5   | 17'-0" | U     |
| d <sub>5</sub> | 134 | #4   | 18'-6" | U     |
| d <sub>6</sub> | 23  | #5   | 35'-9" | U     |
| e              | 103 | #4   | 1'-3"  | U     |
| f              | 93  | #4   | 3'-1"  | U     |

CLASS I CONCRETE Com. 30.4  
REINFORCEMENT BARS A.S.T.M. A618  
STRUCTURAL STEEL A.S.T.M. A36



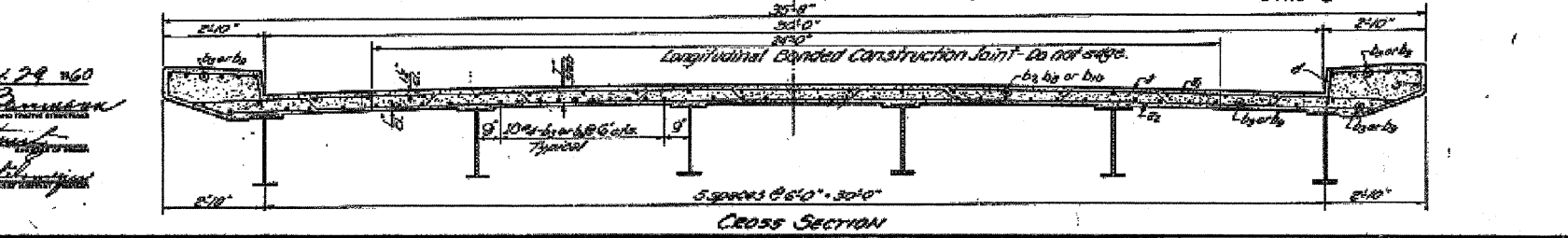
**METHOD OF DETERMINING FILLET HEIGHT 'f'**  
After all structural steel has been erected elevations of the top flanges of the beams shall be taken at intervals shown on sheet rock. From these elevations subtract the dimensions of the deflections for these beams, determined from D.L. Deflection Diagram. The elevations so obtained subtracted from the theoretical grade elevation minus floor thickness, equals the fillet height above top of beam.



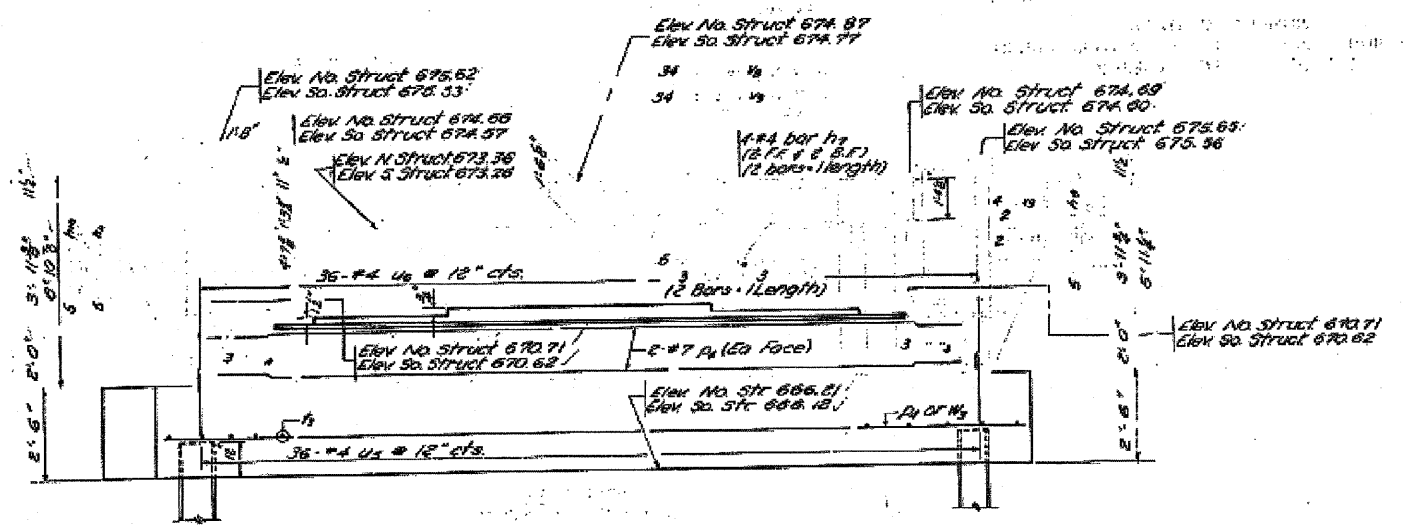
**SECTION A-A**

|          |                |
|----------|----------------|
| DESIGNED | W.B. Bannister |
| CHECKED  | M. Bannister   |
| DRAWN    | J. Bannister   |
| CHECKED  | M.T.           |

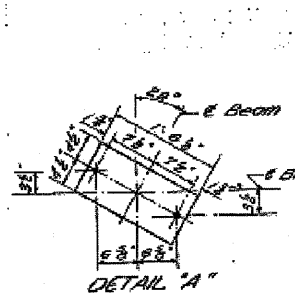
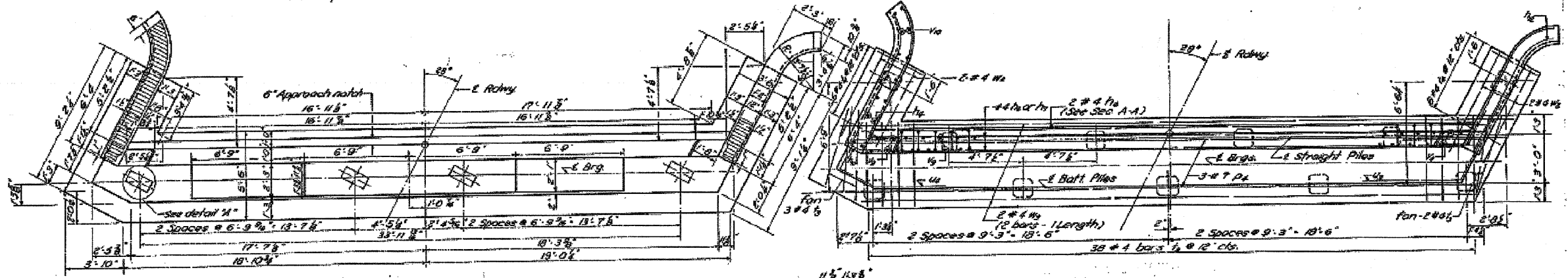
NOV. 29 1960



**SUPERSTRUCTURE DETAILS**  
COMPOSITE SPANS  
NORTH & SOUTH STRUCTURES  
OVER TR. 352 ABRAHAM CREEK  
F.A.I. RT. 80 SEC. 06-7(B, F)  
BUREAU COUNTY  
STA. 1612+97.40 (F.A.I. RT. 80)



Type - Concrete Piles  
 Capacity - 30 Tons  
 Est. Length - 32'-0" No. Str - 27'-0" So. Str  
 No. Req'd - 21 test Pile in North Structure



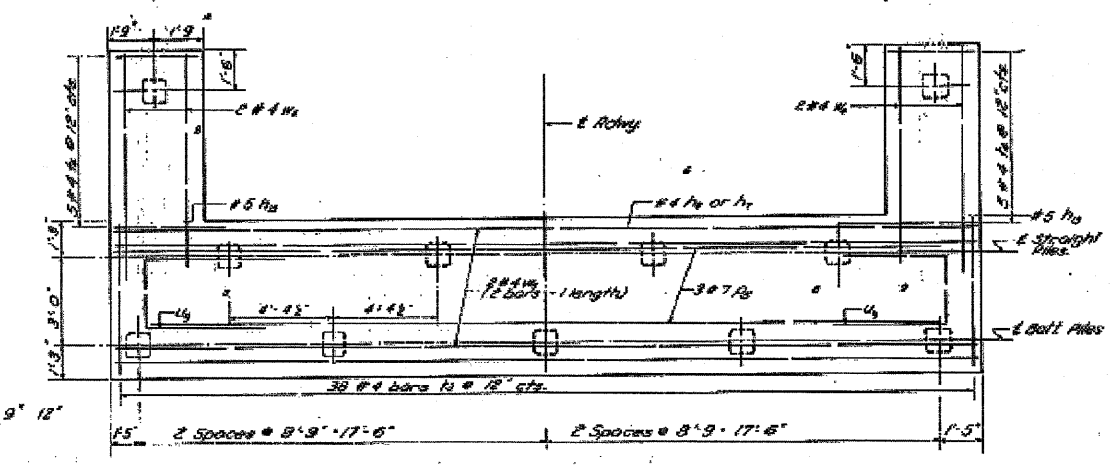
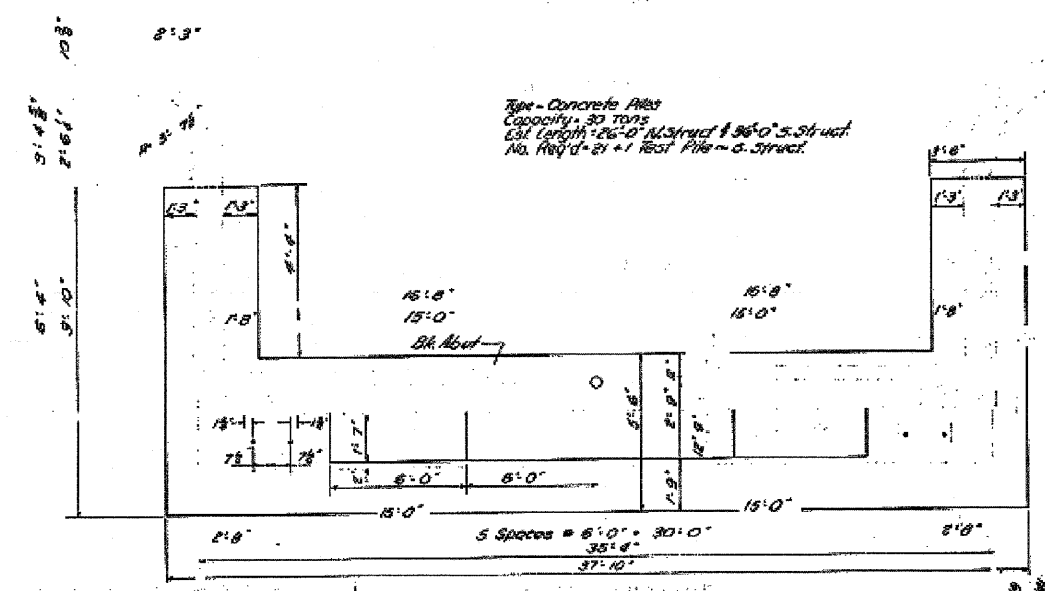
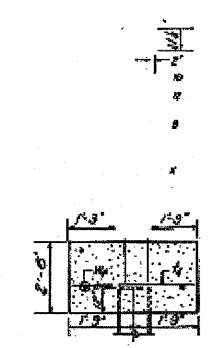
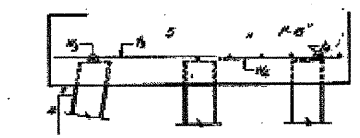
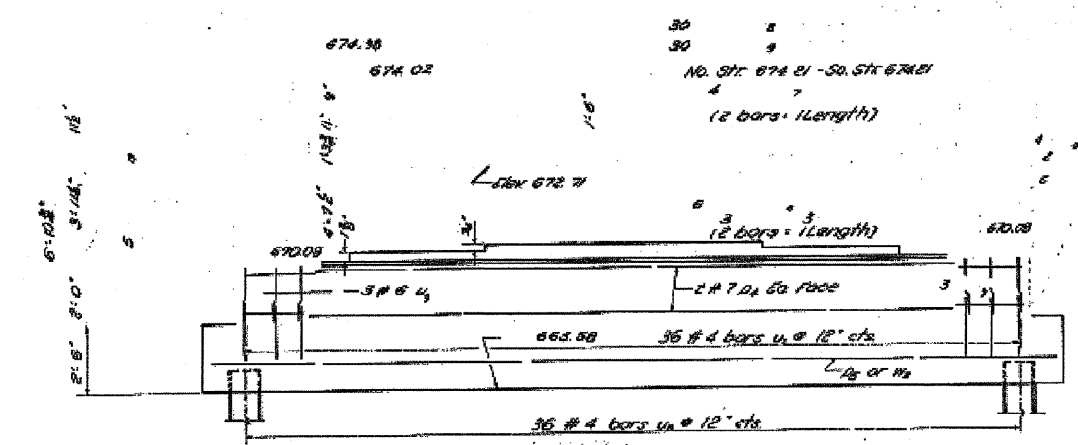
Not 29 60  
 W.B. Gammann  
 M. J. [Signature]  
 J.A. Putnam  
 M.J.  
 P.H. Bartelmeier

2 ABUTMENTS

| NO. | SIZE   | QTY | TYPE | LENGTH | REMARKS |
|-----|--------|-----|------|--------|---------|
| 32  | 10'-3" | 14  | #7   | 36'-0" |         |
| 16  | 17'-3" |     |      |        |         |
| 32  | 7'-3"  |     |      |        |         |
| 20  | 4'-3"  | 88  | #4   | 5'-3"  |         |
| 10  | 3'-6"  | 24  | #4   | 3'-3"  |         |
| 10  | 4'-3"  |     |      |        |         |
| 24  | 9'-6"  | 68  | #4   | 2'-9"  |         |
|     |        | 152 | #4   | 3'-0"  |         |
| 6   | 10'-0" | 56  | #4   | 8'-3"  |         |
| 8   | 10'-6" | 40  | #5   | 3'-9"  |         |
| 72  | 0'-0"  |     |      |        |         |
| 72  | 6'-6"  | 8   | #4   | 7'-0"  |         |
|     |        | 8   | #4   | 20'-0" |         |

84.8  
 4680  
 Concrete Piles Lin. Ft. 617  
 Test Piles Concrete Each 1

WEST ABUTMENT  
 NO. 8 SO. STRUCTURES  
 OVER TR 352 & BRUSH CREEK  
 F.A.I. RT. 80 SEC. 06-7B  
 BUREAU COUNTY  
 STA. 1612 + 97.40 (F.A.I. RT. 80)



2 ABUTMENTS

|    |    |     |        |     |     |     |        |
|----|----|-----|--------|-----|-----|-----|--------|
| Pa | 32 | # 4 | 19'-3" | 47  | 72  | # 4 | 6'-0"  |
| Pa | 16 | # 4 | 17'-3" | 46  | 72  | # 4 | 7'-6"  |
| Pa | 32 | # 6 | 7'-3"  | 46  | 12  | # 6 | 10'-5" |
| Pa | 40 | # 5 | 8'-0"  | 1/4 | 60  | # 4 | 2'-9"  |
| Pa | 24 | # 5 | 9'-6"  | 1/4 | 136 | # 4 | 5'-0"  |
| Pb | 14 | # 7 | 35'-0" | 1/4 | 58  | # 4 | 6'-3"  |
| Pc | 76 | # 4 | 5'-3"  | 1/4 | 40  | # 5 | 3'-9"  |
| Pd | 20 | # 4 | 3'-3"  | 1/4 | 8   | # 4 | 20'-0" |
|    |    |     |        | 1/4 | 8   | # 4 | 5'-0"  |

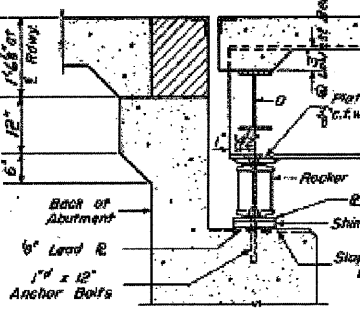
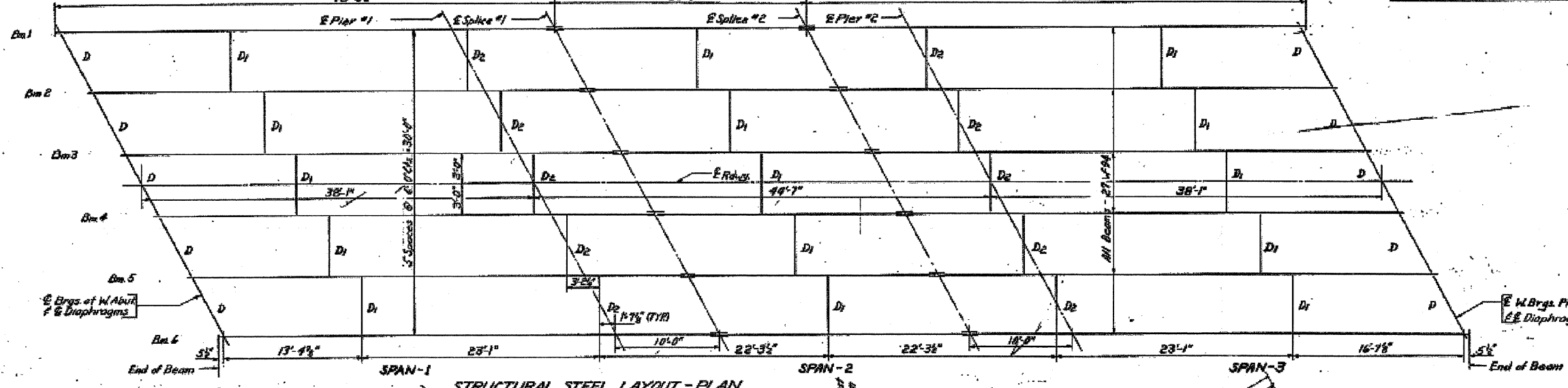
745  
4480  
Concrete Piles Lin. Ft. 646  
Test Piles (Concrete) Each 1

**EAST ABUTMENT  
NO. & 50 STRUCTURES  
OVER TR 352  
& BRUSH CREEK  
F.A. I. RT. 80 SEC. 06-7B  
BUREAU COUNTY  
STA. 1612 + 97.40 (F.A. I. RT. 80)**

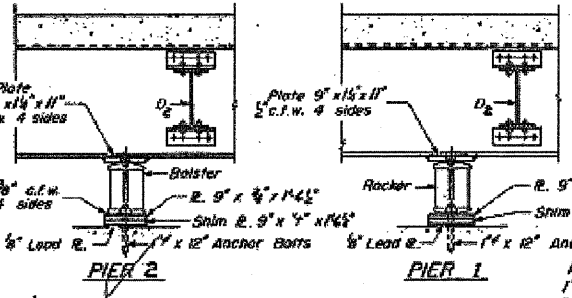
REVISED  
M. Parnowski  
J.A. Ashman  
NT  
MAY 29 60  
W.E. Bannan  
C.J. Hunt  
R.R. Buthman

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|              |         |        |              |           |
|--------------|---------|--------|--------------|-----------|
| WORK NO.     | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| F.A.I. 98-78 | 80      | Bureau | 48           | 11        |
| 26 SHEETS    |         |        |              |           |

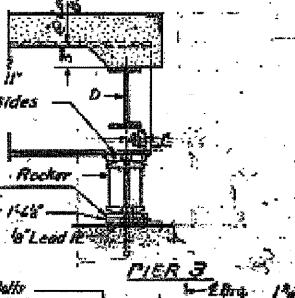


SECTION AT W. ABUTMENT

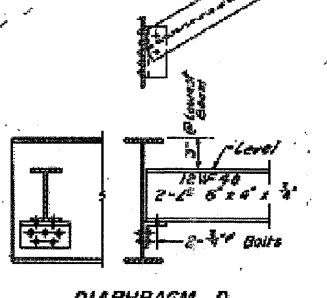


PIER 2

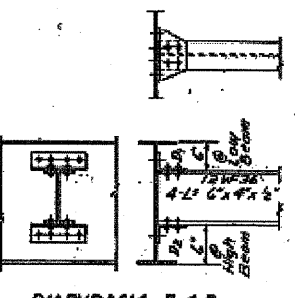
PIER 1



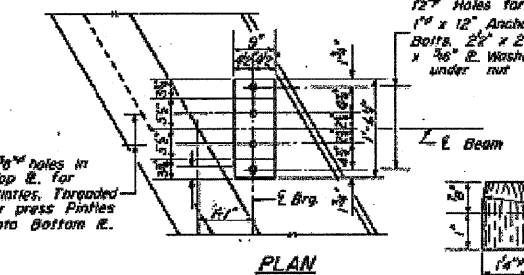
PIER 3



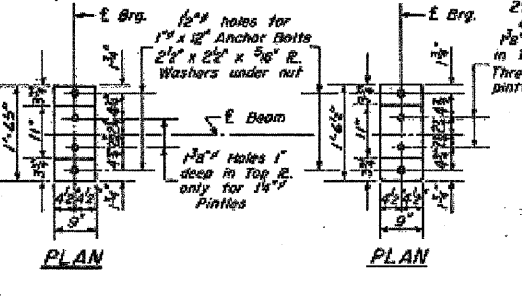
DIAPHRAGM D



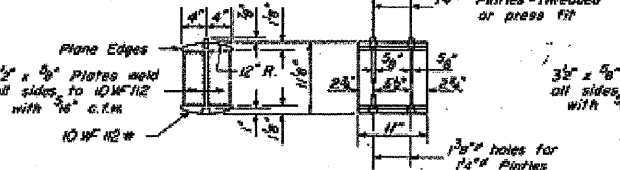
DIAPHRAGM D1 & D2



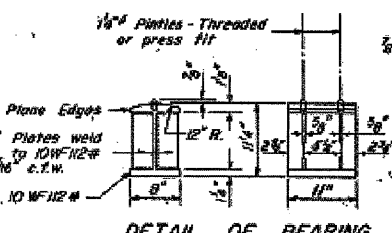
PLAN



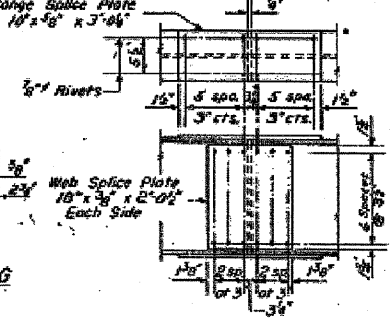
DETAIL OF PINTLE



DETAIL OF BEARING AT PIER 1 & 3 AND W. ABUT.



DETAIL OF BEARING AT PIER 2



DETAIL OF SPLICE - 24 Required

NOTE: For Top of Beam Elevations and Table of 'd' Dimensions see sheet D.

STRUCTURAL STEEL DETAILS.  
3-SPAN CONTINUOUS UNIT, SKEWED  
NORTH AND SOUTH STRUCTURES  
OVER I.R. 352 AND BRUSH CREEK  
F.A.I. 78-80 SEC. 06-7(12,1)  
BUREAU COUNTY  
STA. 1612 + 974.0 (F.A.I. RT 80)

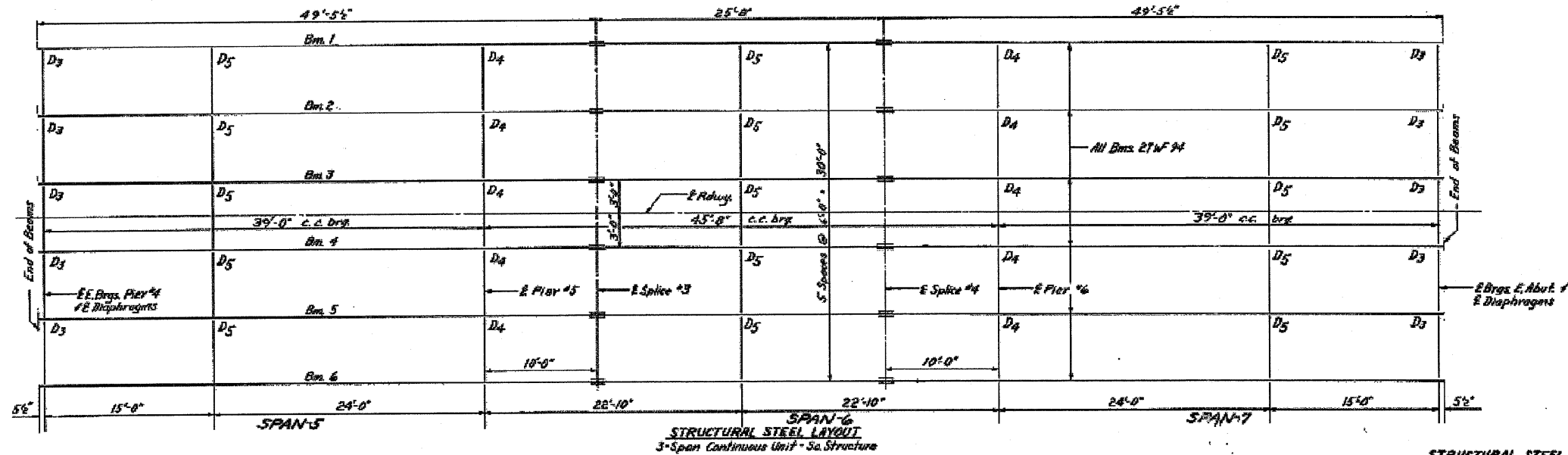
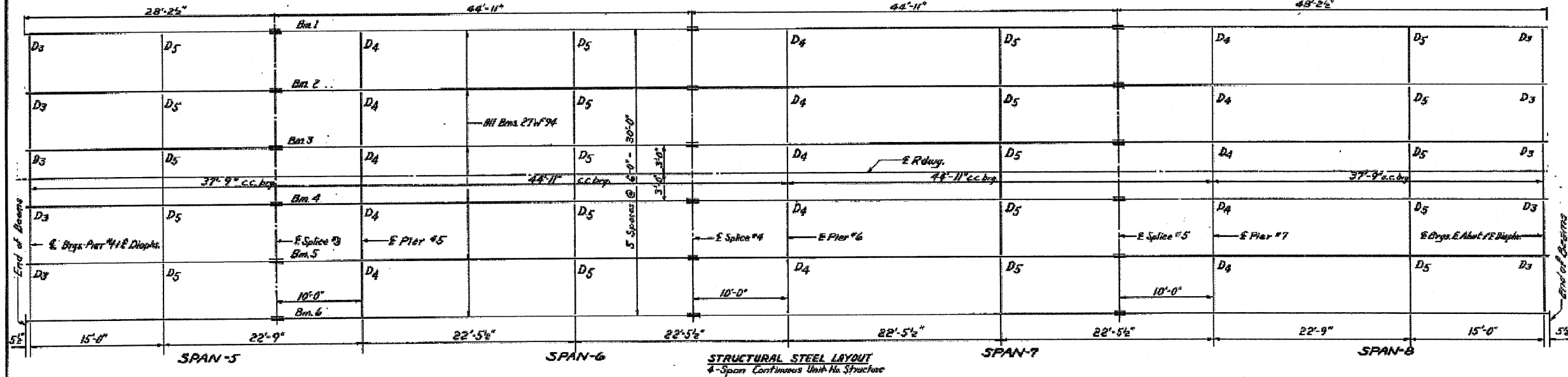
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|----------|-----------------|------|------------|
| DESIGNED | Checked         | DATE | Nov 29 '60 |
| CHECKED  | M. Parnoch      | DATE |            |
| DRAWN    | W. A. Sausonoff | DATE |            |
| CHECKED  | RAT             | DATE |            |

I-2-R Re-drawn 9-23-59

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|                |         |           |              |
|----------------|---------|-----------|--------------|
| PROJECT NO.    | SECTION | SHEET NO. | TOTAL SHEETS |
| F.A.I. 80      | DE-28   | 45        | 12           |
| NO. 1612-97.40 | Bureau  | 12        | 9            |
|                |         |           |              |

SHEET NO. 7  
26 SHEETS



|                    |             |                    |      |
|--------------------|-------------|--------------------|------|
| DESIGNED           | DATE        | EXAMINED           | DATE |
| <i>[Signature]</i> | Nov. 29 '60 | <i>[Signature]</i> |      |
| CHECKED            |             | PASSED             |      |
| <i>[Signature]</i> |             | <i>[Signature]</i> |      |
| DRAWN              |             | APPROVED           |      |
| <i>[Signature]</i> |             | <i>[Signature]</i> |      |
| CHECKED            |             |                    |      |
| MIT                |             |                    |      |

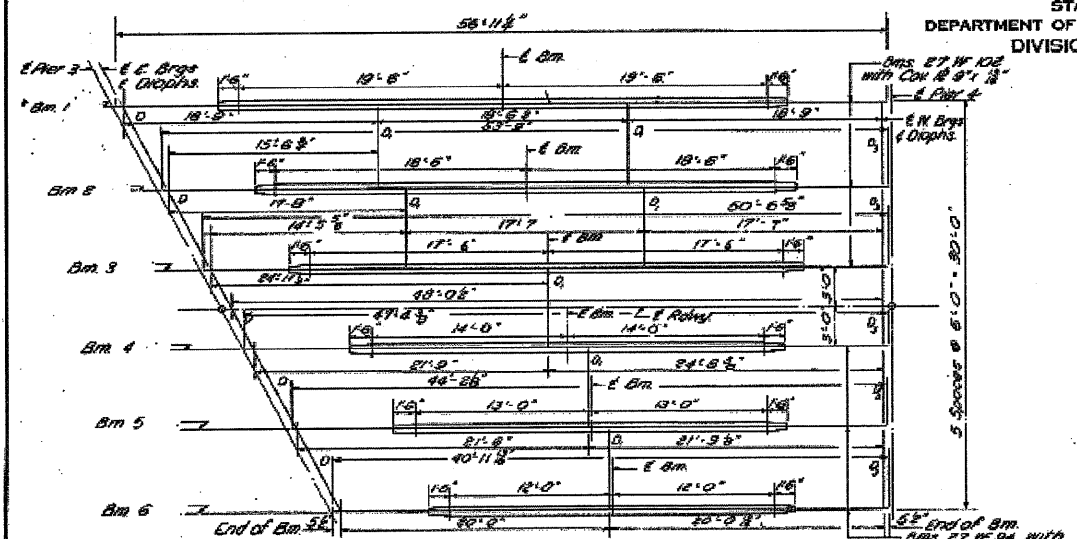
NOTE: For details, Top of Beam Elevations and Table of "Dimensions" see sheet B

STRUCTURAL STEEL LAYOUT  
4-SPAN CONTINUOUS UNIT-NO. STRUCTURE  
3-SPAN CONTINUOUS UNIT-Sa STRUCTURE  
OVER TR-352 AND BRUSH CREEK  
F.A.I. RT. 80 - SEC. 06-7(C,F)  
BUREAU COUNTY  
STR. 1612-97.40 (F.A.I. RT. 80)

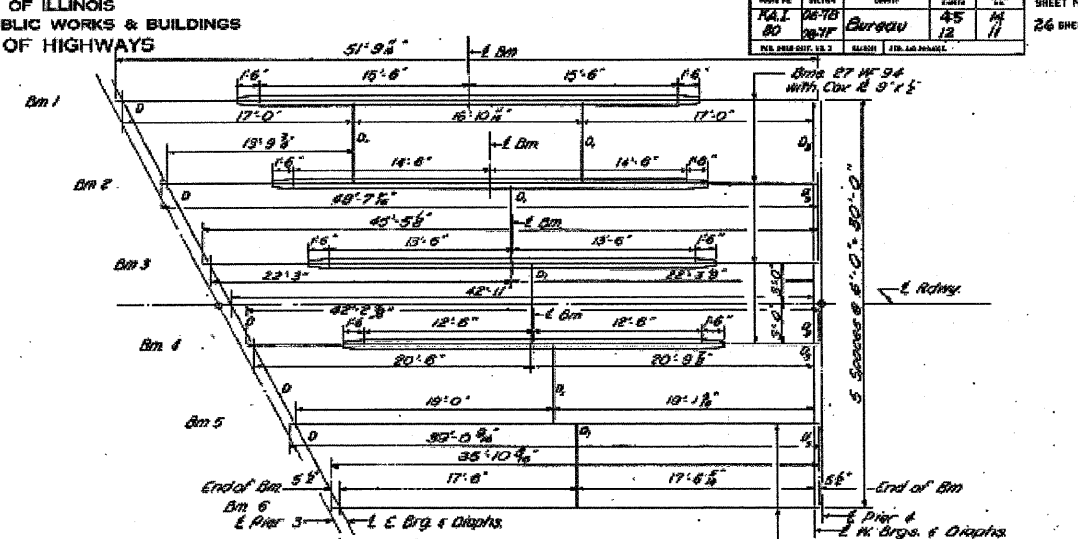


STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

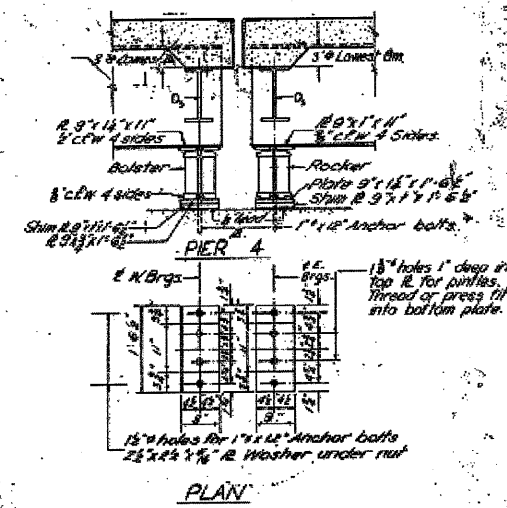
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|------------------|---------------------|---------------------|--------------|-----------|
| DATE             | DESIGN              | CHECKED             | TOTAL SHEETS | SHEET NO. |
| 10/1/60          | 10/1/60             | BURBANO             | 45           | 11        |
| FILE NO. 10-1-60 | PROJECT NO. 10-1-60 | SCALE: 1/8" = 1'-0" | 26 SHEETS    |           |



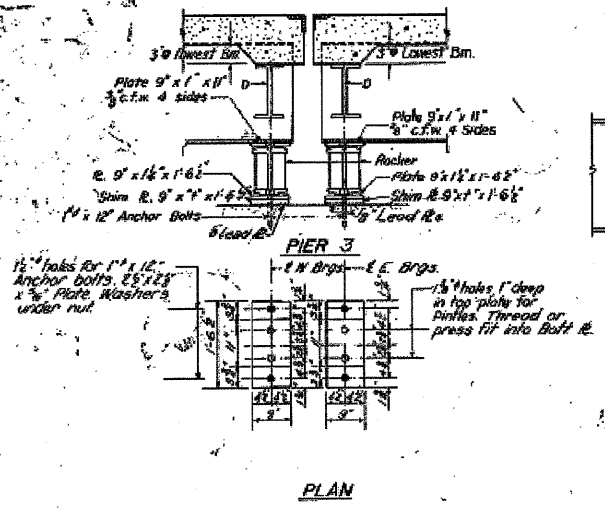
STRUCTURAL STEEL LAYOUT  
NORTH STRUCTURE - COMPOSITE SPAN



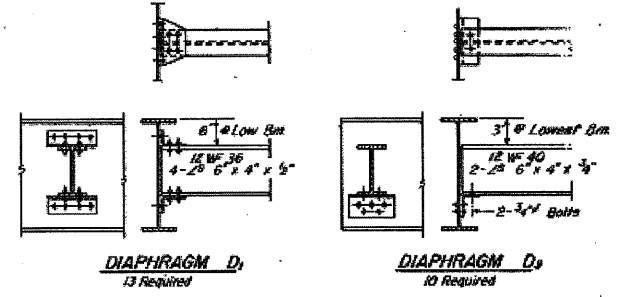
STRUCTURAL STEEL LAYOUT  
SOUTH STRUCTURE - COMPOSITE SPAN



PLAN

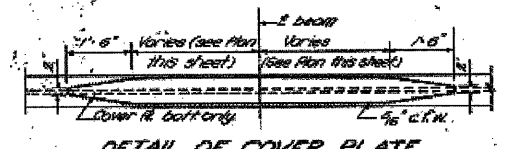


PLAN

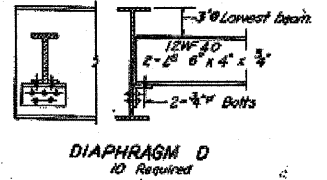


DIAPHRAGM D  
13 Required

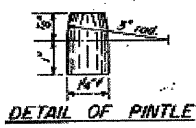
DIAPHRAGM D  
10 Required



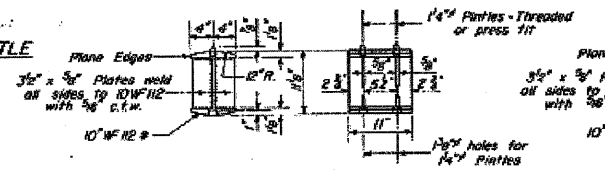
DETAIL OF COVER PLATE



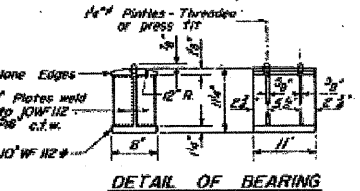
DIAPHRAGM D  
10 Required



DETAIL OF PINTLE



DETAIL OF BEARING  
AT PIER 3



DETAIL OF BEARING  
AT PIER 4

STRUCTURAL STEEL DETAILS  
COMPOSITE SPANS  
NORTH & SOUTH STRUCTURES  
OVER I.R. 352 & BRUSH CREEK  
F. A. I. RT. 80 SEC. 05 - 7(B.F.)  
BUREAU COUNTY  
STA. 1612 + 87.40 (F. A. I. RT. 80)

DESIGNED: *Chadman*  
CHECKED: *M. Ramackis*  
DRAWN: *A. Putnam*  
CHECKED: *M.T.*

DATE: Nov. 29 '60  
APPROVED: *W.B. Boush*  
PROJECT: *Chadman*  
DRAWING NO.: *10-1-60*

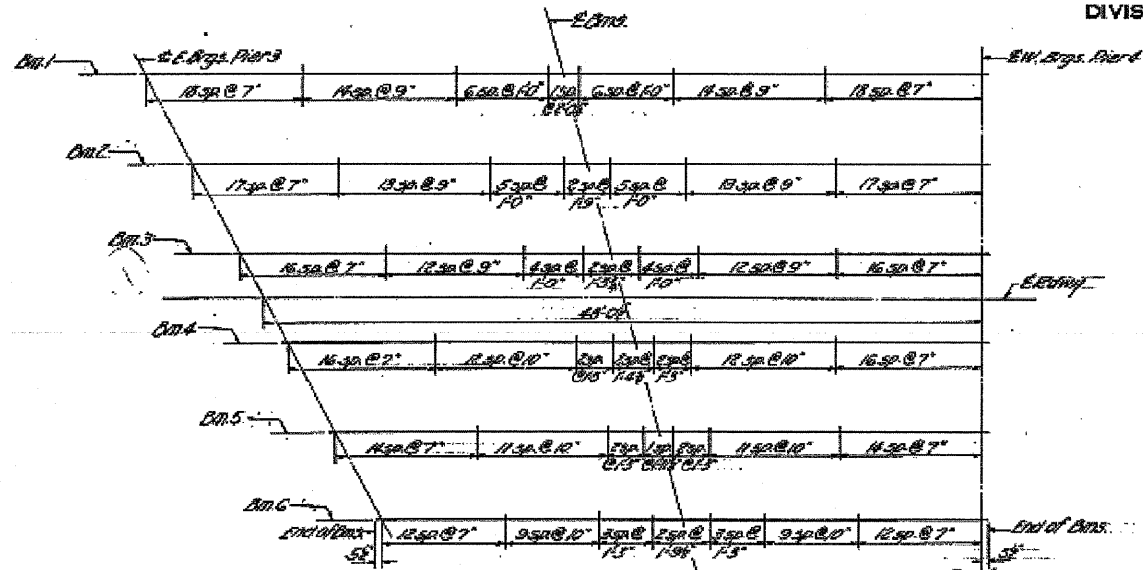
I-2 Re-drawn 6-10-59

|   |                      |            |           |   |  |                           |                 |          |              |            |               |
|---|----------------------|------------|-----------|---|--|---------------------------|-----------------|----------|--------------|------------|---------------|
| FILE NAME =                               | USER NAME = braboygo | DESIGNED - | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | EXISTING BRIDGE PLANS (FOR INFORMATION ONLY) | F.A.I. RTE.               | SECTION         | COUNTY   | TOTAL SHEETS | SHEET NO.  |               |
| or \\pw-work\pindot\braboygo\d0112640\d36 | 908-shr-detail.DGN   | DRAWN -    | REVISED - |   |  | I-80                      | **              | BUREAU   | 344          | 265        |               |
| PLOT SCALE = 50.7353 / IN.                | CHECKED -            | REVISED -  | REVISED - |   |  | SCALE: _____              | SHEET NO. _____ | OF _____ | SHEETS       | STA. _____ | TO STA. _____ |
| PLOT DATE = Sep 07, 2009 - 02:18:36 PM    | DATE -               | REVISED -  | REVISED - |   |  | ILLINOIS FED. AID PROJECT |                 |          |              |            |               |
|   |                      |            |           |   |  | 66908                     |                 |          |              |            |               |

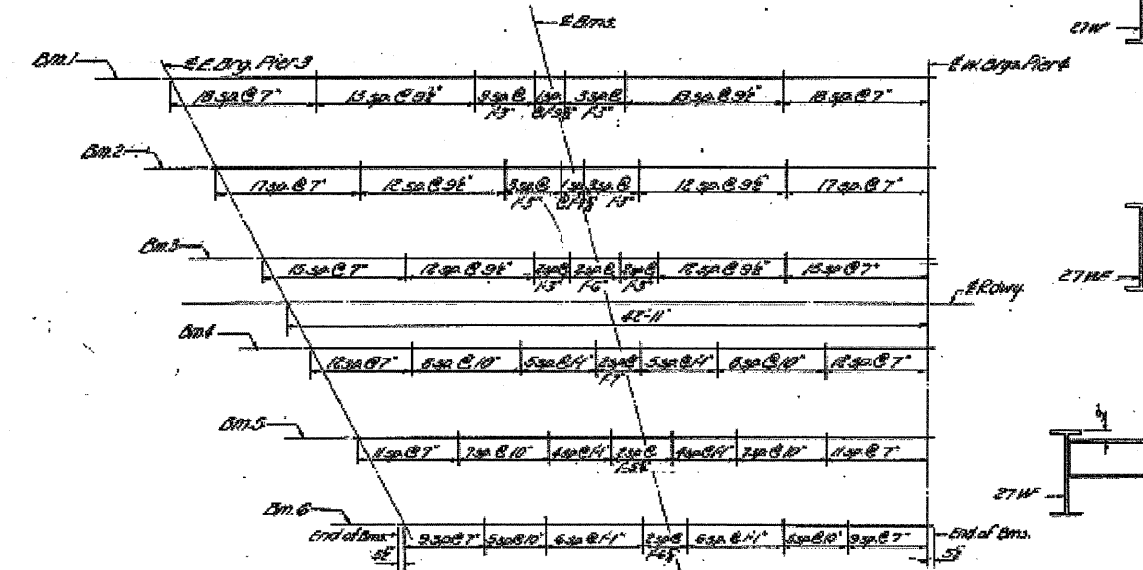
\*\*06-[7BR&BR-1,7VB-M,6BR&6,7 RS-1 & 1

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

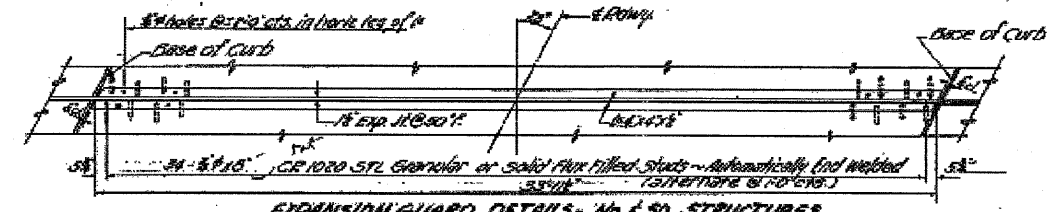
|        |    |      |       |           |
|--------|----|------|-------|-----------|
| DATE   | BY | CHKD | APP'D | SHEET NO. |
| 80     | BR | BR   | BR    | 45        |
| BUREAU |    |      |       | 75        |
|        |    |      |       | 12        |
|        |    |      |       | 26 SHEETS |



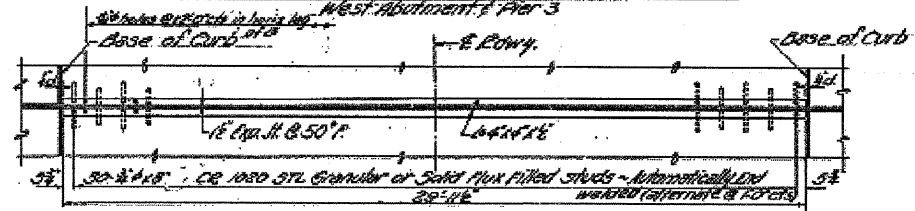
PLAN - No. STRUCTURE COMPOSITE SPAN  
Showing Spacing of Shear Connectors



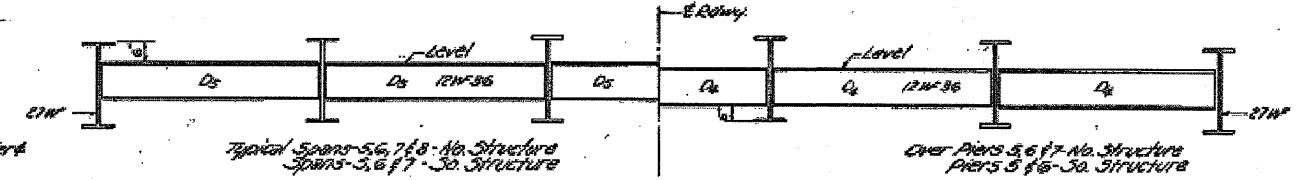
PLAN - So. STRUCTURE COMPOSITE SPAN  
Showing Spacing of Shear Connectors



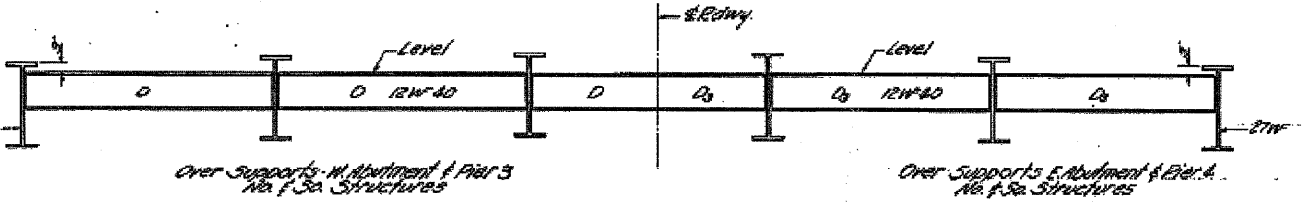
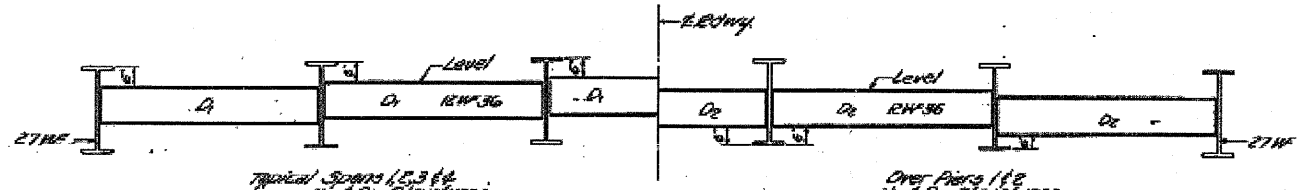
EXPANSION GUARD DETAILS - No. 1 So. STRUCTURES



EXPANSION GUARD DETAILS - No. 1 So. STRUCTURES

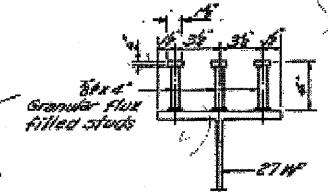


DIAPHRAGM DETAILS



|          |                   |
|----------|-------------------|
| DESIGNED | Checked           |
| CHECKED  | M. T. [Signature] |
| DRAWN    | [Signature]       |
| CHECKED  | M.T.              |

|          |             |
|----------|-------------|
| EXAMINED | Nov 29 '60  |
| DRAWN    | [Signature] |
| APPROVED | [Signature] |



DETAIL OF SHEAR CONNECTOR

SHEAR CONNECTORS, EXPANSION GUARDS & DIAPHRAGM DETAILS.  
NORTH & SOUTH STRUCTURES  
OVER TE 352 & BRUSH CREEK  
E.I. RT. 80 SEC. 06-71(B), E.  
BUREAU COUNTY  
STA. 1612+97.40 (E.I. RT. 80)

|   |                      |            |           |
|---|----------------------|------------|-----------|
| FILE NAME =   | USER NAME = braboyco | DESIGNED - | REVISED - |
| c:\pwwork\pwwork\braboyco\d0112640\vd369908-sht-details.DGN |                      | DRAWN -    | REVISED - |
|   |                      | CHECKED -  | REVISED - |
|   |                      | DATE -     | REVISED - |
|   |                      |            |           |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING BRIDGE PLANS (FOR INFORMATION ONLY)

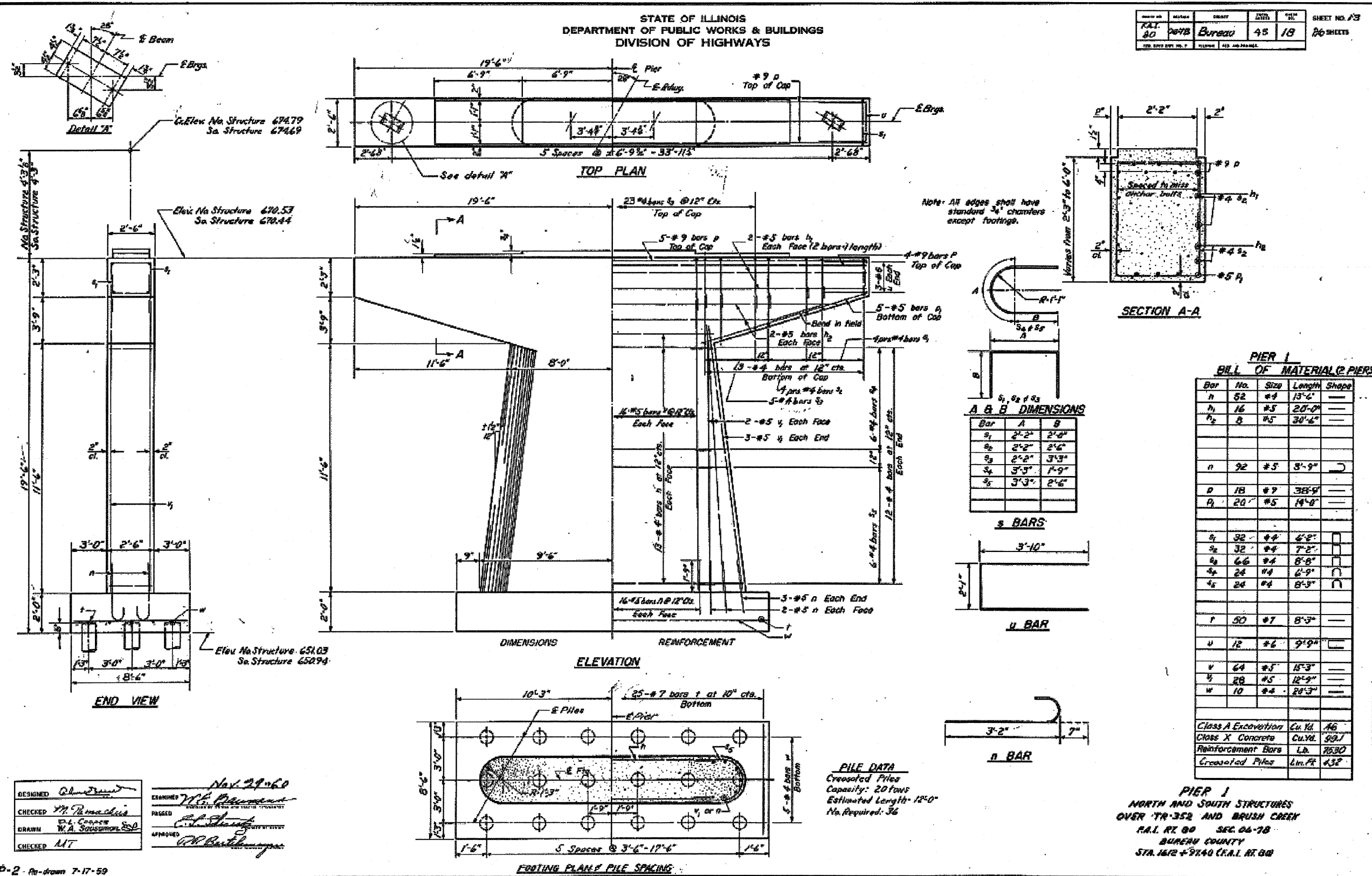
SCALE: SHEET NO. OF SHEETS STA. TO STA.

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-80                      | **      | BUREAU | 344          | 266       |
|                           |         |        |              | 66908     |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

\*\*06-[7BR&BR-1,7VB-M,6BR&6,7 RS-1 & I

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

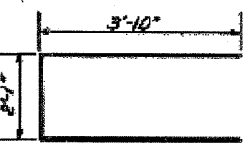
|             |      |        |      |       |              |
|-------------|------|--------|------|-------|--------------|
| PROJECT NO. | DATE | DRAWN  | DATE | SCALE | SHEET NO. OF |
| FAI 80      | 0478 | Bureau | 45   | 18    | 26 SHEETS    |



A & B DIMENSIONS

| Bar            | A     | B     |
|----------------|-------|-------|
| s <sub>1</sub> | 2'-2" | 2'-0" |
| s <sub>2</sub> | 2'-2" | 2'-4" |
| s <sub>3</sub> | 2'-2" | 3'-3" |
| s <sub>4</sub> | 3'-3" | 1'-9" |
| s <sub>5</sub> | 3'-3" | 2'-6" |

s BARS



u BAR



n BAR

PIER 1  
BILL OF MATERIAL @ PIERS

| Bar No.                      | Size | Length | Shape |
|------------------------------|------|--------|-------|
| n                            | #2   | 13'-6" | —     |
| h <sub>1</sub>               | #5   | 20'-0" | —     |
| h <sub>2</sub>               | #5   | 30'-6" | —     |
| n                            | #5   | 3'-9"  | —     |
| p                            | #7   | 38'-9" | —     |
| p <sub>1</sub>               | #5   | 14'-0" | —     |
| s <sub>1</sub>               | #4   | 6'-2"  | —     |
| s <sub>2</sub>               | #4   | 7'-2"  | —     |
| s <sub>3</sub>               | #4   | 8'-8"  | —     |
| s <sub>4</sub>               | #4   | 6'-9"  | —     |
| s <sub>5</sub>               | #4   | 8'-5"  | —     |
| r                            | #7   | 8'-3"  | —     |
| u                            | #6   | 9'-9"  | —     |
| v                            | #5   | 15'-3" | —     |
| y                            | #5   | 12'-9" | —     |
| w                            | #4   | 20'-3" | —     |
| Class A Excavation Cu.Yd. 16 |      |        |       |
| Class X Concrete Cu.Yd. 89.1 |      |        |       |
| Reinforcement Bars Lb. 7830  |      |        |       |
| Crested Piles Lin.Ft. 432    |      |        |       |

**PILE DATA**  
Crested Piles  
Capacity: 20 tons  
Estimated Length: 12'-0"  
No. Required: 36

**PIER 1**  
NORTH AND SOUTH STRUCTURES  
OVER TR-352 AND BRUSH CREEK  
F.A.I. RT. 80 SEC. 04-78  
BUREAU COUNTY  
STA. 1612 + 9140 (F.A.I. RT. 80)

DESIGNED: *Albert...*  
CHECKED: *M. R...*  
DRAWN: *D.L. Cooney*  
CHECKED: *MT*

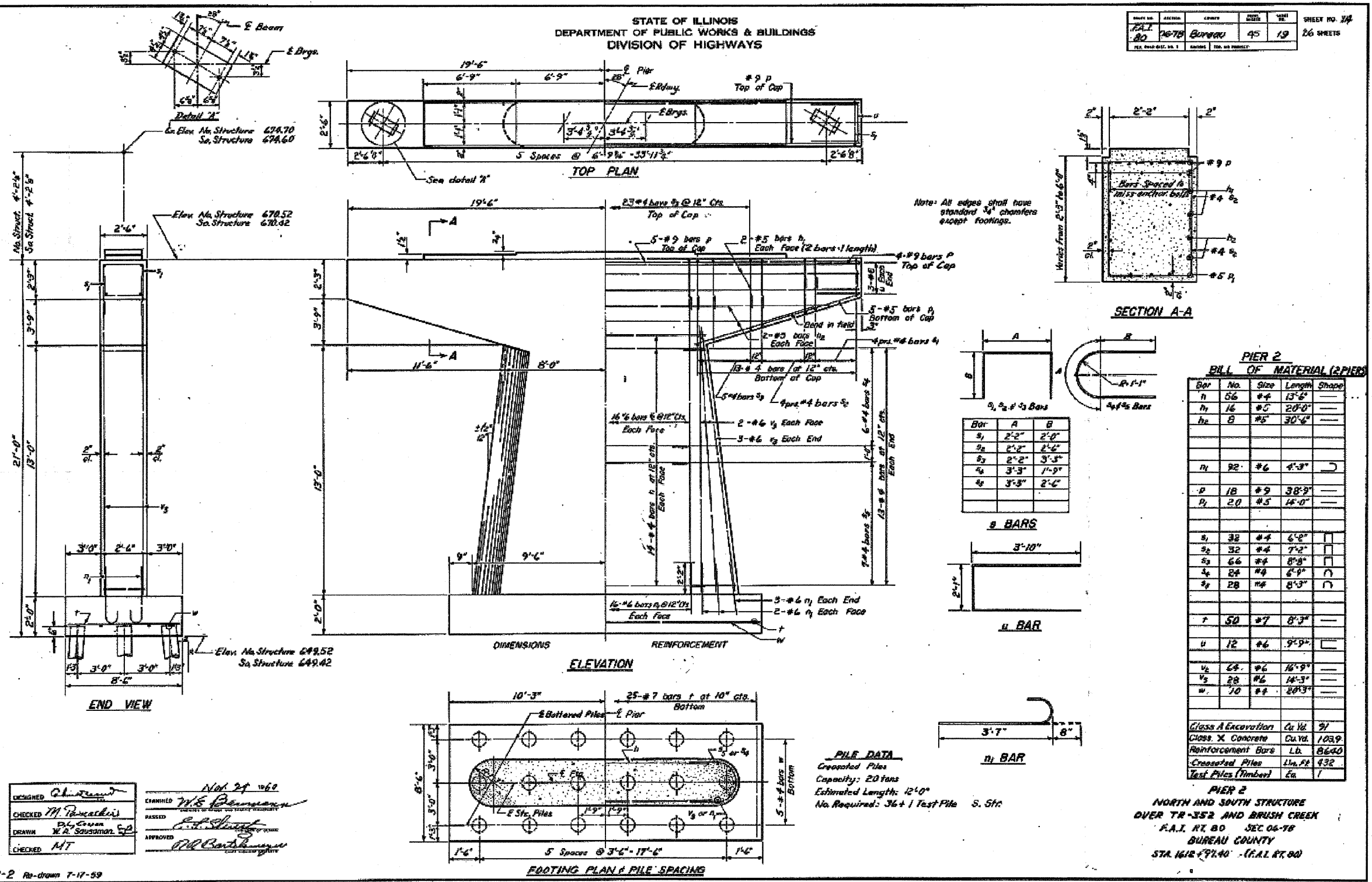
EXAMINED: *W.H. P...*  
PASSED: *E.L. S...*  
APPROVED: *W.B. B...*

Nov. 29, 1960

P-2 - Re-drawn 7-17-59

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|           |         |        |              |           |
|-----------|---------|--------|--------------|-----------|
| DATE      | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| F.A.I. 80 | 16-78   | Bureau | 45           | 19        |
| 26 SHEETS |         |        |              |           |



Note: All edges shall have standard 3/4" chamfers except roofings.

**PIER 2**

**BILL OF MATERIAL (2 PIERS)**

| Bar                 | No. | Size | Length | Shape    |      |
|---------------------|-----|------|--------|----------|------|
| n                   | 36  | #4   | 13'-6" | —        |      |
| n <sub>1</sub>      | 16  | #5   | 20'-0" | —        |      |
| n <sub>2</sub>      | 8   | #5   | 30'-6" | —        |      |
| n <sub>3</sub>      | 32  | #6   | 4'-3"  | —        |      |
| p                   | 18  | #9   | 38'-9" | —        |      |
| p <sub>1</sub>      | 20  | #5   | 14'-0" | —        |      |
| s <sub>1</sub>      | 32  | #4   | 6'-8"  | —        |      |
| s <sub>2</sub>      | 32  | #4   | 7'-2"  | —        |      |
| s <sub>3</sub>      | 66  | #4   | 8'-8"  | —        |      |
| s <sub>4</sub>      | 24  | #4   | 6'-9"  | —        |      |
| s <sub>5</sub>      | 28  | #4   | 8'-3"  | —        |      |
| t                   | 50  | #7   | 8'-3"  | —        |      |
| u                   | 12  | #6   | 9'-9"  | —        |      |
| v <sub>1</sub>      | 24  | #6   | 16'-9" | —        |      |
| v <sub>2</sub>      | 28  | #6   | 14'-3" | —        |      |
| w                   | 10  | #4   | 20'-3" | —        |      |
| Glass A Excavation  |     |      |        | Cu Yd    | 91   |
| Class X Concrete    |     |      |        | Cu Yd    | 1039 |
| Reinforcement Bars  |     |      |        | Lb.      | 8640 |
| Crested Piles       |     |      |        | Lbs. Ft. | 432  |
| Test Piles (Timber) |     |      |        | Eq.      | 1    |

**PIER 2**  
NORTH AND SOUTH STRUCTURE  
OVER TR-352 AND BRUSH CREEK  
F.A.I. RT. 80 SEC. 06-78  
BUREAU COUNTY  
STA. 1612+97.40 - (F.A.I. RT. 80)

DESIGNED: *Chapman*  
CHECKED: *M. Jankovic*  
DRAWN: *D. L. Gowan*  
CHECKED: *MT*

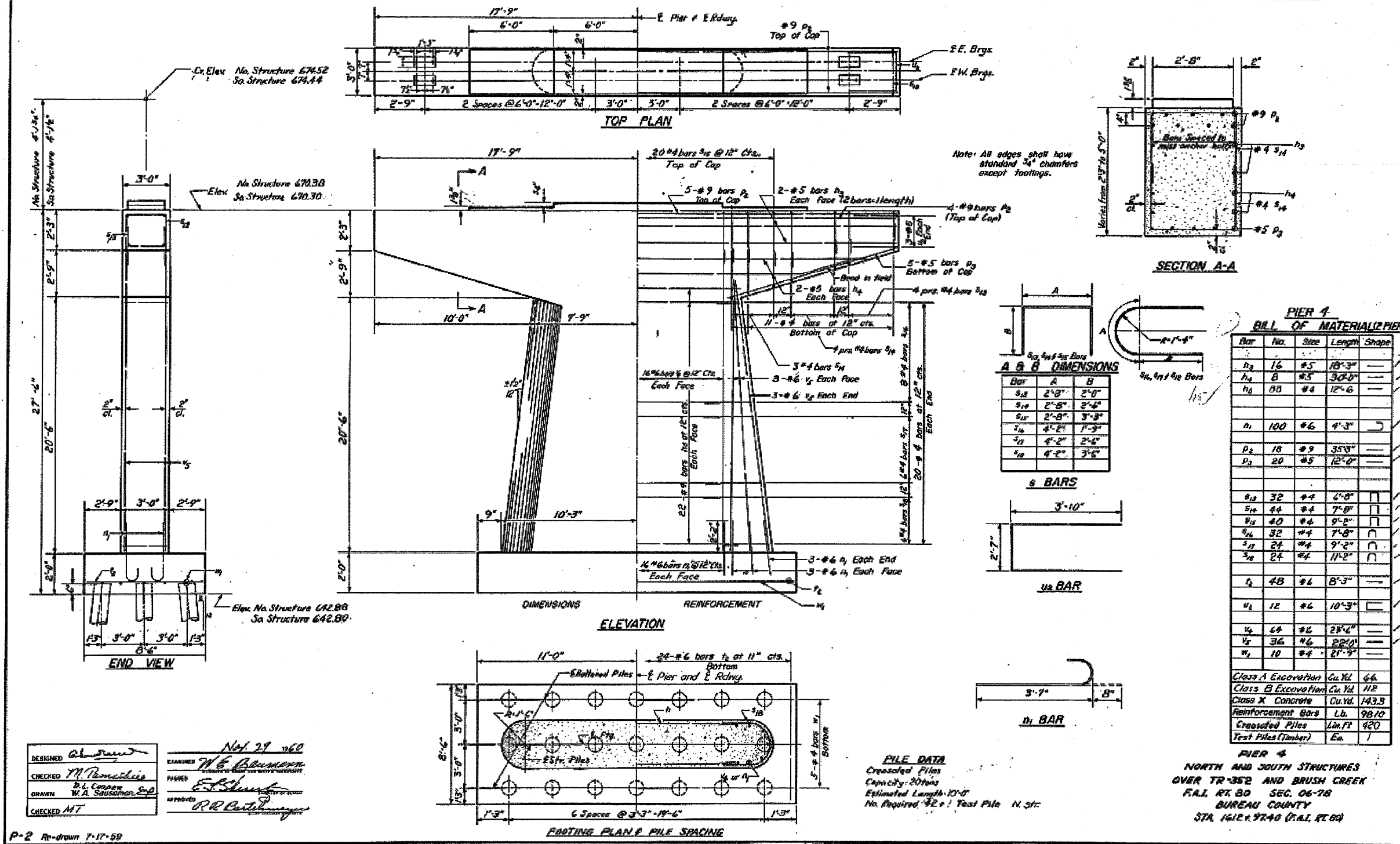
NOV 24 1960  
ENGINEER: *W. E. Blumstein*  
PASSED: *E. J. Smith*  
APPROVED: *W. E. Blumstein*

P-2 Re-drawn 7-17-59



STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|        |         |        |     |     |              |
|--------|---------|--------|-----|-----|--------------|
| DATE   | SECTION | NO.    | NO. | NO. | SHEET NO. 26 |
| FAL 80 | 26-78   | Bureau | 45  | 21  | 26 SHEETS    |



Note: All edges shall have standard 3/4" chamfers except footings.

**A & B DIMENSIONS**

| Bar             | A     | B     |
|-----------------|-------|-------|
| S <sub>18</sub> | 2'-8" | 2'-0" |
| S <sub>19</sub> | 2'-8" | 2'-6" |
| S <sub>21</sub> | 2'-8" | 3'-8" |
| S <sub>26</sub> | 4'-2" | 1'-3" |
| S <sub>27</sub> | 4'-2" | 2'-6" |
| S <sub>28</sub> | 4'-2" | 3'-6" |

**PIER 4  
BILL OF MATERIALS (PIERS)**

| Bar                 | No.      | Size  | Length | Shape |
|---------------------|----------|-------|--------|-------|
| S <sub>2</sub>      | 16       | #5    | 18'-3" | —     |
| S <sub>4</sub>      | 8        | #5    | 30'-0" | —     |
| S <sub>6</sub>      | 88       | #4    | 12'-6" | —     |
| S <sub>1</sub>      | 100      | #6    | 4'-3"  | ⌋     |
| P <sub>2</sub>      | 18       | #9    | 35'-0" | —     |
| P <sub>3</sub>      | 20       | #5    | 12'-0" | —     |
| S <sub>13</sub>     | 32       | #4    | 2'-8"  | ⌋     |
| S <sub>14</sub>     | 44       | #4    | 7'-8"  | ⌋     |
| S <sub>15</sub>     | 40       | #4    | 9'-2"  | ⌋     |
| S <sub>16</sub>     | 32       | #4    | 7'-8"  | ⌋     |
| S <sub>17</sub>     | 24       | #4    | 9'-2"  | ⌋     |
| S <sub>18</sub>     | 24       | #4    | 11'-2" | ⌋     |
| S <sub>2</sub>      | 48       | #6    | 8'-3"  | —     |
| U <sub>2</sub>      | 12       | #6    | 10'-3" | ⌋     |
| V <sub>2</sub>      | 44       | #6    | 28'-6" | —     |
| V <sub>3</sub>      | 36       | #6    | 22'-0" | —     |
| W <sub>2</sub>      | 10       | #4    | 21'-9" | —     |
| Class A Excavation  | Cu. Yd.  | 66    |        |       |
| Class B Excavation  | Cu. Yd.  | 112   |        |       |
| Class X Concrete    | Cu. Yd.  | 143.5 |        |       |
| Reinforcement Bars  | Lb.      | 9810  |        |       |
| Crossed Piles       | Lin. Ft. | 420   |        |       |
| Test Piles (Timber) | Eq.      | 1     |        |       |

**PILE DATA**  
Crossed Piles  
Capacity: 20 tons  
Estimated Length: 10'-0"  
No. Required: 2 + 1 Test Pile N. Str.

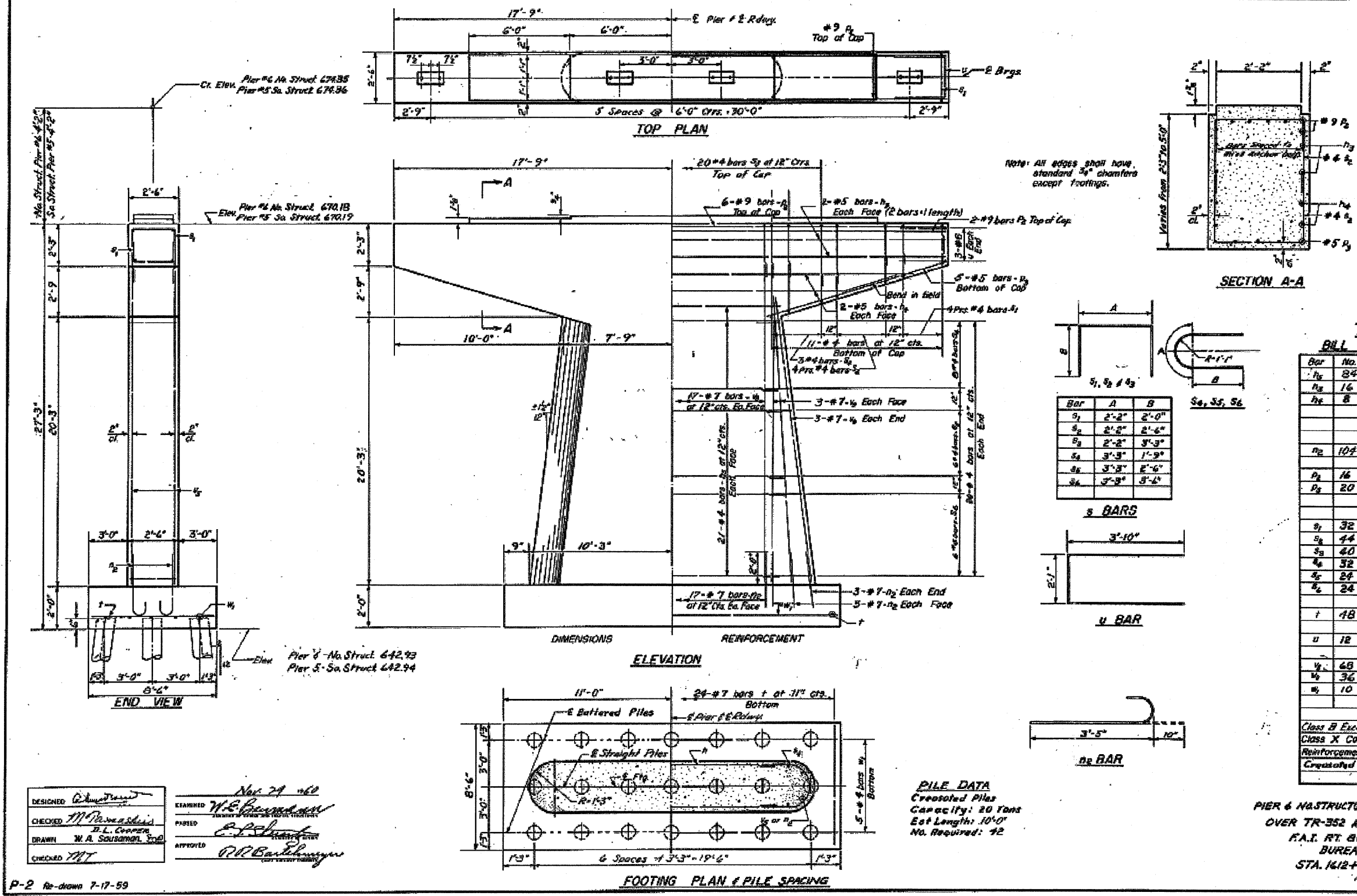
**PIER 4  
NORTH AND SOUTH STRUCTURES  
OVER TR-352 AND BRUSH CREEK  
F.A.I. RT. 80 SEC. 06-78  
BUREAU COUNTY  
STA. 161E+3740 (F.A.I. RT. 80)**

DESIGNED: *Alm...*  
CHECKED: *M. Penabaz...*  
DRAWN: *D.L. Carson*  
CHECKED: *M.T.*  
EXAMINED: *W.E. Bowman*  
IN CHARGE: *E.T. ...*  
APPROVED: *P.R. ...*  
No. 29 1960

P-2 Re-drawn 7-17-59

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|             |         |           |              |
|-------------|---------|-----------|--------------|
| PROJECT NO. | SECTION | SHEET NO. | TOTAL SHEETS |
| F.A.I. 80   | 06-7B   | 45        | 22           |
| BUREAU      |         | 26 SHEETS |              |



DESIGNED: *Robert Brown*  
CHECKED: *M. J. ...*  
DRAWN: *W. A. Sausman, Sr.*  
CHECKED: *MT*

ESTABLISHED: *W. E. ...*  
PASSED: *E. J. ...*  
APPROVED: *R. B. ...*

Nov 24 '60

P-2 Re-drawn 7-17-59

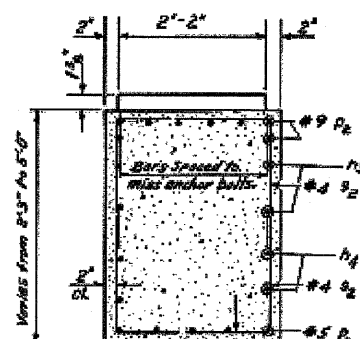
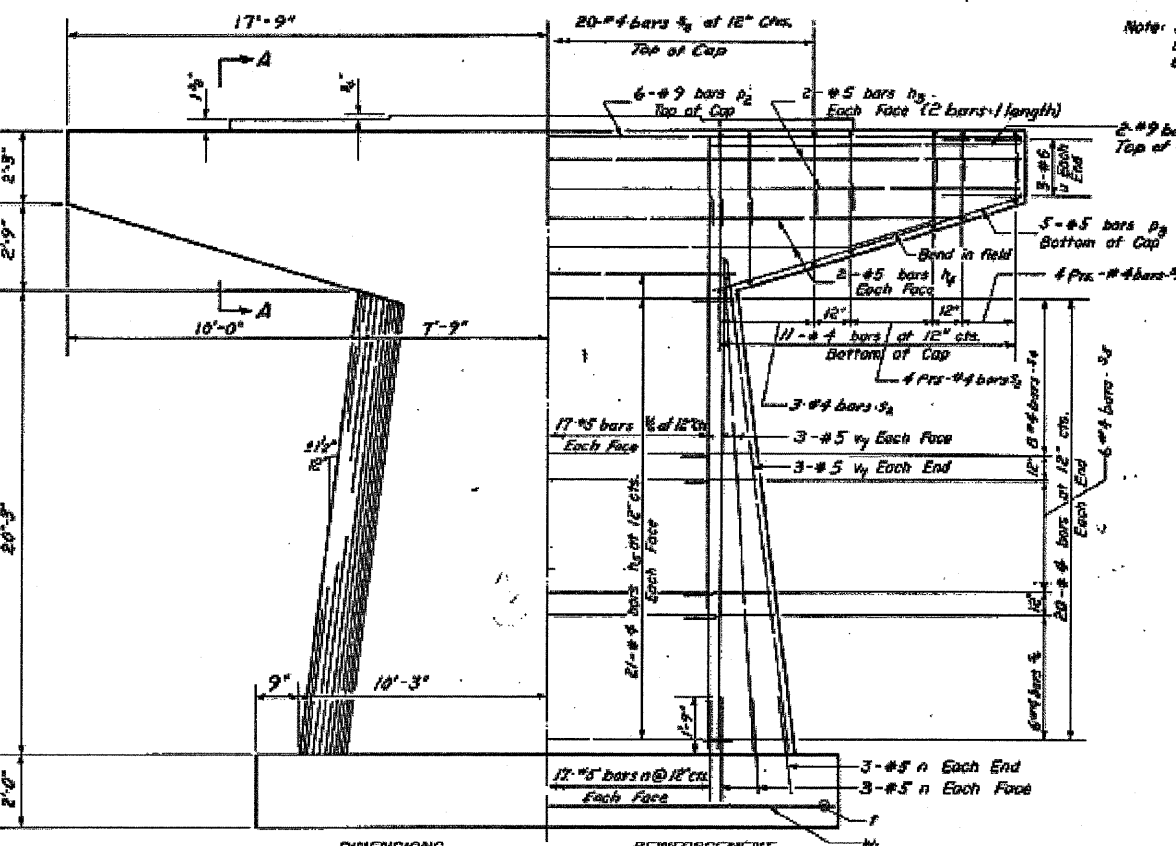
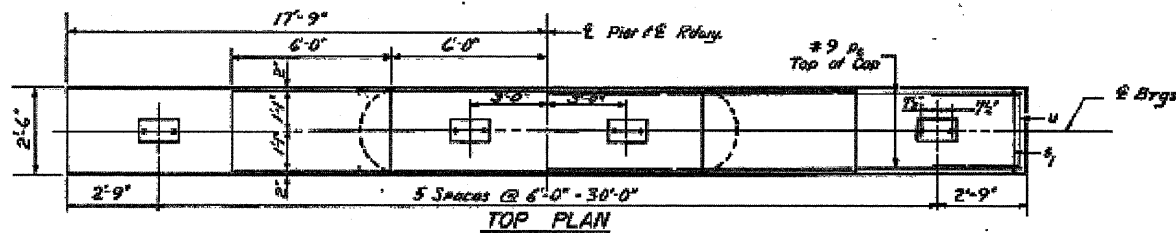
STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|        |          |        |     |      |            |
|--------|----------|--------|-----|------|------------|
| DATE   | REVISION | BY     | NO. | DATE | SHEET NO.  |
| K.A.I. | 06-78    | Bureau | 45  | 23   | B-6 SHEETS |

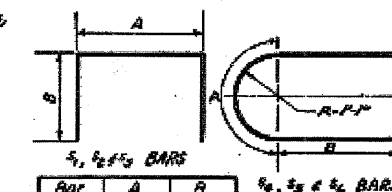
Ch. Elev. No. Struct. Pier #5-674.44  
Pier #7-674.27  
So. Struct. Pier #6-674.27

Elev. No. Struct. Pier #5-670.20  
Pier #7-670.03  
So. Struct. Pier #6-670.04

Elev. No. Struct. Pier #5-642.95  
Pier #7-642.78  
So. Struct. Pier #6-642.79



Note: All edges shall have standard 3/4" chamfers except footings.

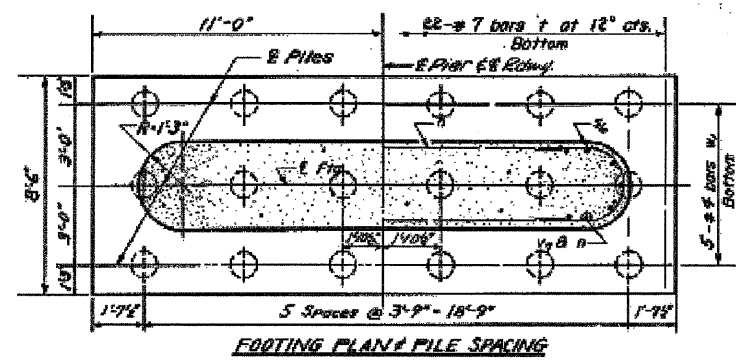


PIER #7, NO. STRUCTURE  
PIER #6, SO. STRUCTURE  
BILL OF MATERIAL (3 PIERS)

| Bar No.             | Size     | Length | Shape |
|---------------------|----------|--------|-------|
| h <sub>0</sub>      | #4       | 12'-6" | —     |
| h <sub>1</sub>      | #5       | 18'-3" | —     |
| h <sub>2</sub>      | #5       | 30'-0" | —     |
| n                   | #5       | 3'-9"  | —     |
| p <sub>1</sub>      | #9       | 35'-9" | —     |
| p <sub>2</sub>      | #5       | 16'-0" | —     |
| s <sub>1</sub>      | #4       | 6'-2"  | □     |
| s <sub>2</sub>      | #4       | 7'-2"  | □     |
| s <sub>3</sub>      | #4       | 8'-8"  | □     |
| s <sub>4</sub>      | #4       | 6'-9"  | □     |
| s <sub>5</sub>      | #4       | 8'-3"  | □     |
| s <sub>6</sub>      | #4       | 10'-3" | □     |
| t                   | #7       | 8'-3"  | —     |
| u                   | #6       | 9'-9"  | □     |
| v <sub>1</sub>      | #5       | 23'-6" | —     |
| v <sub>2</sub>      | #5       | 21'-3" | —     |
| v <sub>3</sub>      | #4       | 27'-9" | —     |
| Class A Excavation  | Cu. Yd.  | 174    |       |
| Class B Excavation  | Cu. Yd.  | 174    |       |
| Class X Concrete    | Cu. Yd.  | 185.3  |       |
| Reinforcement Bars  | Lb.      | 12,280 |       |
| Crossed Piles       | Lin. Ft. | 540    |       |
| Test Piles (Number) | Eq.      | 1      |       |

PIERS #7, NO. STRUCTURE / PIER #6, SO. STRUCTURE  
OVER TN 352 AND BRUSH CREEK  
F.A.I. RT. 80 SEC. 06-7B  
BUREAU COUNTY  
STA. 1618+97.40 (F.A.I. RT. 80)

PILE DATA  
Crossed Piles  
Capacity: 20 Tons  
Est. Length: 10'-0"  
No. Required: 54 + 1 Test Pile @ Pier #7 - N. Structure



DESIGNED: *Almond*  
CHECKED: *M. J. ...*  
DRAWN: *W. A. Soudaman*  
CHECKED: *TNT*

CLAIMED: *W. E. ...*  
PASSED: *P. ...*  
APPROVED: *R. ...*

Nov 29 1968

P-2 Re-drawn 7-17-59





B.M. 94 3/4" iron pipe 127 feet South of Sta. 1764+50.0  
 109 feet East of a Railroad right of way fence 0.5 ft  
 North of an East-West Fence Elev. 627.436.

STATE OF ILLINOIS  
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
 DIVISION OF HIGHWAYS

|          |        |     |      |           |
|----------|--------|-----|------|-----------|
| DATE     | BY     | NO. | REV. | SHEET NO. |
| 11-28-78 | J.R.P. | 43  | 4    | 13 SHEETS |

GENERAL NOTES

Class X Concrete shall be used throughout. Coarse aggregate which is to be used in parapet handrails and end posts must be absolutely free of chert, flint, limonite, lignite, and soft sandstone. The concrete floor slab shall be finished in accordance with Article 51.19 of the Standard Specifications.

Slope wall shall be reinforced with welded wire fabric 6"x8" mesh, #4 wires, weighing 53 # per 100 Sq. Ft.

Layout of slope walls may be varied to suit ground conditions in the field, as directed by the Engineer.

Piles 3/4" Open Holes 1/2" unless noted. Permanent forms will not be permitted in forming the concrete floor.

All bolsters, rockers, bearing plates, lead plates, pintles, and anchor bolts shall be fabricated and set in accordance with Article 51.15 of the Standard Specifications and are included in quantity of Structural Steel.

Anchor bolts shall be set before erecting diaphragms over supports.

The following surfaces of expansion guards shall be given two shop coats of red lead paint; the backs of the vertical legs of the 4"x4"x1/2" Ls. Expansion guards are included in quantity of Structural Steel. Est. Weight = 3440 Lbs.

Except as otherwise provided, all Structural Steel shall receive one shop coat of red lead paint and two field coats of aluminum paint. See Articles 56.1 to 56.5 inclusive of the Standard Specifications.

All paint shall be furnished and applied by the Contractor.

The Contractor shall drive 2 Concrete test piles in permanent locations, and 1 timber test pile at or near Pier 1, South Bridge, as directed by the Engineer, before ordering remainder of piles.

Holes shall be prepared to natural ground for Co. piles driven in embankment in accordance with Article 60.3 (c) of the Standard Specifications.

TOTAL BILL OF MATERIAL (2 BRIDGES) SEC. 6-7-1

| Item                        | Unit     | Subtotal | Subtotal | Total  |
|-----------------------------|----------|----------|----------|--------|
| Channel Excavation          | Cu. Yds. |          | 3000     | 3000   |
| Class A Excavation for Str. | Cu. Yds. |          | 810      | 810    |
| Class B Excavation for Str. | Cu. Yds. |          | 810      | 810    |
| Class X Concrete            | Cu. Yds. | 524.0    | 293.88   | 817.88 |
| Structural Steel            | Lbs.     | 808820   |          | 808820 |
| Aluminum Handrail           | Lin. Ft. | 8380     | 6470     | 938.0  |
| Reinforcement Bars          | Lbs.     | 104440   |          | 104440 |
| Crested Piles               | Lin. Ft. | 100000   | 810.0    | 100810 |
| Test Piles - Timber         | Each     | 1        |          | 1      |
| Test Piles - Concrete       | Lin. Ft. |          | 2376     | 2376   |
| Name Plates                 | Each     | 2        |          | 2      |
| Slope Wall 4'               | Sq. Yds. |          | 2840     | 2840   |
| Slope Wall 6'               | Sq. Yds. |          | 2160     | 2160   |
| Borrow Excavation           | Cu. Yds. |          | 62350    | 62350  |

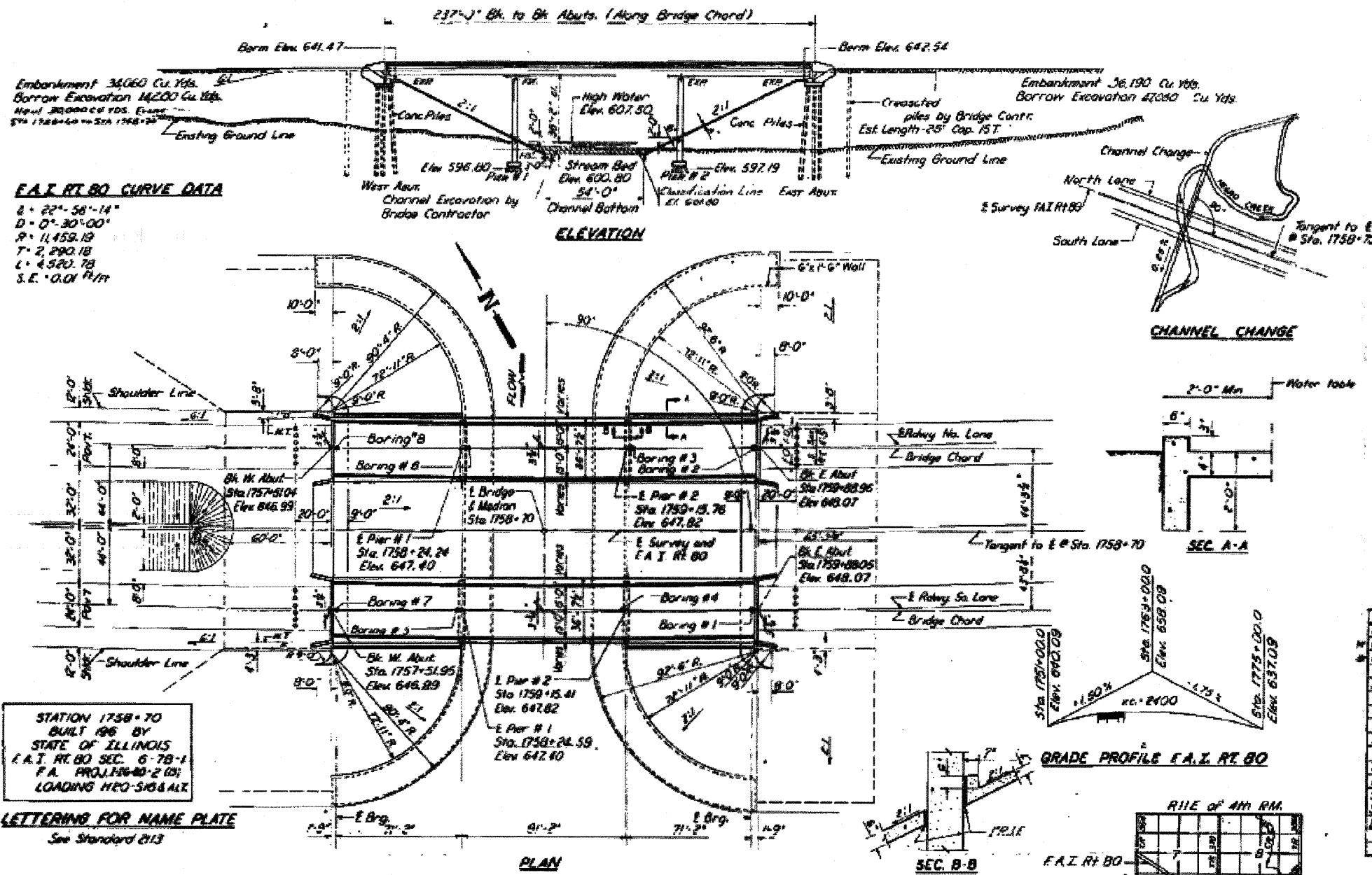
TOTAL BILL OF MATERIAL (2 BRIDGES) SEC. 6-7-1

|                  |      |        |
|------------------|------|--------|
| Structural Steel | Lbs. | 808820 |
|------------------|------|--------|

GENERAL PLAN AND ELEVATION  
 PROJECT 616-80-2710  
 NEEBO CREEK  
 E.A.T. RT. 80 SEC. 6-7-1  
 BUREAU COUNTY  
 STA. 1758+70

E.A.T. RT. 80 CURVE DATA

$\Delta = 22^\circ 58' 14''$   
 $D = 0^\circ 30' 00''$   
 $R = 11,459.12$   
 $T = 2,290.18$   
 $L = 4,520.78$   
 $S.E. = 0.01 \text{ Ft./ft.}$



STATION 1758+70  
 BUILT 196 BY  
 STATE OF ILLINOIS  
 E.A.T. RT. 80 SEC. 6-7-1  
 F.A. PROJ. 616-80-2710  
 LOADING H2O-SIG&ALT.

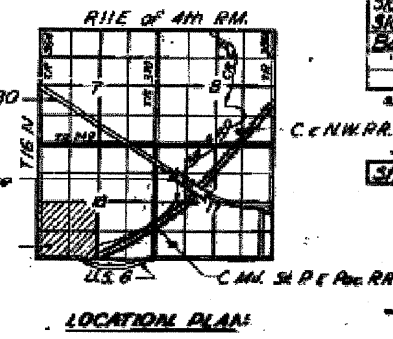
LETTERING FOR NAME PLATE  
 See Standard 213

DESIGNED: James H. Ponce  
 CHECKED: J.R.P.  
 DATE: Nov 2 1978

WATERWAY INFORMATION  
 Drainage Area: 8,004 Acres  
 Character: rolling, cultivated  
 Required Opening: (150 ft. fld.) 450 Sq. Ft.  
 Proposed Opening: 450 Sq. Ft.  
 Ordinary Water Elev.: 600.7

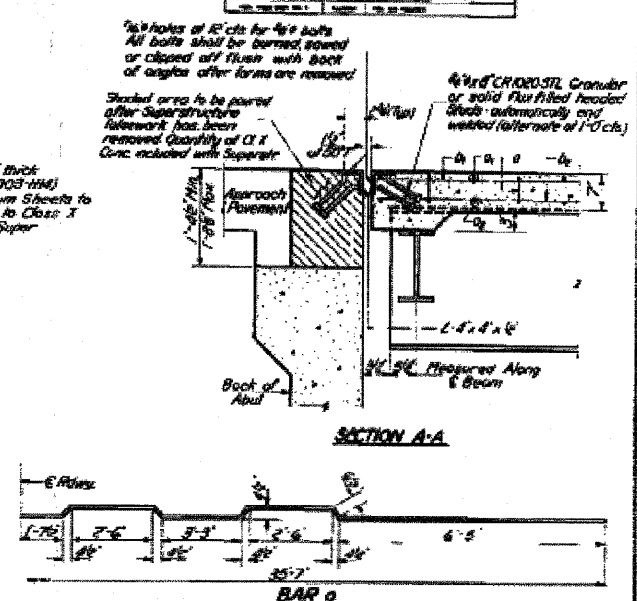
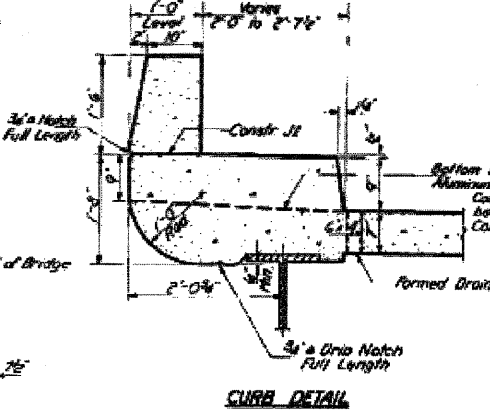
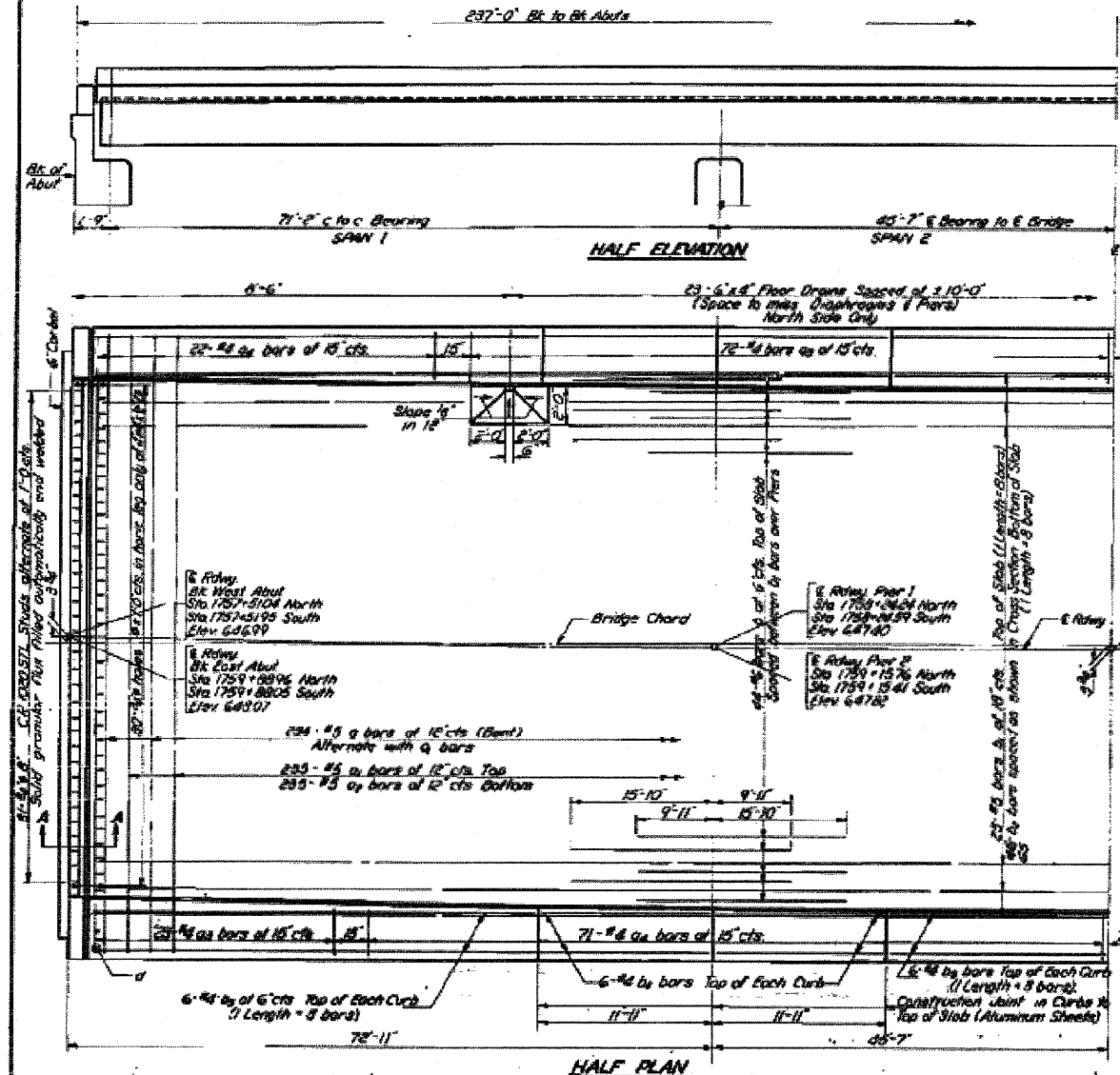
DESIGN STRESSES  
 $f_c = 400 \text{ psi Super \& Sub.}$   
 $v = 75 \text{ p.s.i. Figs.}$   
 $f_s = 20,000 \text{ p.s.i. Reinft}$   
 $f_a = 18,000 \text{ p.s.i. Struct.}$   
 $n = 10$

LOADING H2O-SIG-44 & ALT.



STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|            |         |         |           |              |
|------------|---------|---------|-----------|--------------|
| DESIGN NO. | PROJECT | SECTION | SHEET NO. | TOTAL SHEETS |
| 758-70     | BUREAU  | 6-7     | 5         | 13 SHEETS    |



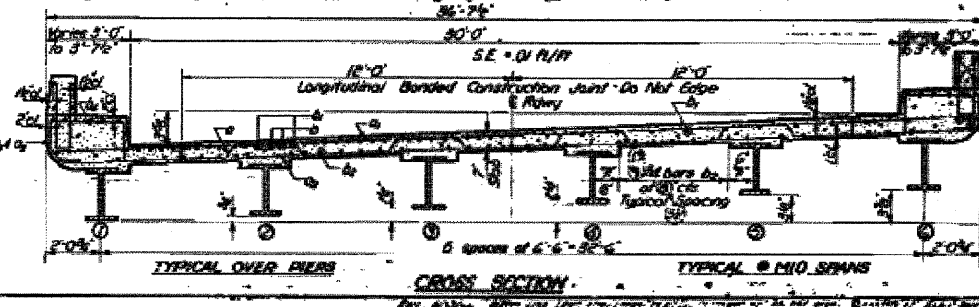
**METHOD OF DETERMINING FILLET HEIGHT 'Y'**  
After all Structural Steel has been erected elevations of the top flanges of the beams shall be taken at intervals not to exceed 10 ft. Subtract these elevations from the elevations top of slab corrected for dead load deflections. This difference minus the slab thickness equals the fillet height above top of beam.

**BILL OF MATERIAL (2 BRIDGES)**

| Bar No. | Size   | Length | Shape |
|---------|--------|--------|-------|
| 1       | 2 1/2" | 15'    | 3/4"  |
| 2       | 2 1/2" | 15'    | 3/4"  |
| 3       | 2 1/2" | 15'    | 3/4"  |
| 4       | 2 1/2" | 15'    | 3/4"  |
| 5       | 2 1/2" | 15'    | 3/4"  |
| 6       | 2 1/2" | 15'    | 3/4"  |
| 7       | 2 1/2" | 15'    | 3/4"  |
| 8       | 2 1/2" | 15'    | 3/4"  |
| 9       | 2 1/2" | 15'    | 3/4"  |
| 10      | 2 1/2" | 15'    | 3/4"  |
| 11      | 2 1/2" | 15'    | 3/4"  |
| 12      | 2 1/2" | 15'    | 3/4"  |
| 13      | 2 1/2" | 15'    | 3/4"  |
| 14      | 2 1/2" | 15'    | 3/4"  |
| 15      | 2 1/2" | 15'    | 3/4"  |
| 16      | 2 1/2" | 15'    | 3/4"  |
| 17      | 2 1/2" | 15'    | 3/4"  |
| 18      | 2 1/2" | 15'    | 3/4"  |
| 19      | 2 1/2" | 15'    | 3/4"  |
| 20      | 2 1/2" | 15'    | 3/4"  |
| 21      | 2 1/2" | 15'    | 3/4"  |
| 22      | 2 1/2" | 15'    | 3/4"  |
| 23      | 2 1/2" | 15'    | 3/4"  |
| 24      | 2 1/2" | 15'    | 3/4"  |
| 25      | 2 1/2" | 15'    | 3/4"  |
| 26      | 2 1/2" | 15'    | 3/4"  |
| 27      | 2 1/2" | 15'    | 3/4"  |
| 28      | 2 1/2" | 15'    | 3/4"  |
| 29      | 2 1/2" | 15'    | 3/4"  |
| 30      | 2 1/2" | 15'    | 3/4"  |
| 31      | 2 1/2" | 15'    | 3/4"  |
| 32      | 2 1/2" | 15'    | 3/4"  |
| 33      | 2 1/2" | 15'    | 3/4"  |
| 34      | 2 1/2" | 15'    | 3/4"  |
| 35      | 2 1/2" | 15'    | 3/4"  |
| 36      | 2 1/2" | 15'    | 3/4"  |
| 37      | 2 1/2" | 15'    | 3/4"  |
| 38      | 2 1/2" | 15'    | 3/4"  |
| 39      | 2 1/2" | 15'    | 3/4"  |
| 40      | 2 1/2" | 15'    | 3/4"  |
| 41      | 2 1/2" | 15'    | 3/4"  |
| 42      | 2 1/2" | 15'    | 3/4"  |
| 43      | 2 1/2" | 15'    | 3/4"  |
| 44      | 2 1/2" | 15'    | 3/4"  |
| 45      | 2 1/2" | 15'    | 3/4"  |
| 46      | 2 1/2" | 15'    | 3/4"  |
| 47      | 2 1/2" | 15'    | 3/4"  |
| 48      | 2 1/2" | 15'    | 3/4"  |
| 49      | 2 1/2" | 15'    | 3/4"  |
| 50      | 2 1/2" | 15'    | 3/4"  |
| 51      | 2 1/2" | 15'    | 3/4"  |
| 52      | 2 1/2" | 15'    | 3/4"  |
| 53      | 2 1/2" | 15'    | 3/4"  |
| 54      | 2 1/2" | 15'    | 3/4"  |
| 55      | 2 1/2" | 15'    | 3/4"  |
| 56      | 2 1/2" | 15'    | 3/4"  |
| 57      | 2 1/2" | 15'    | 3/4"  |
| 58      | 2 1/2" | 15'    | 3/4"  |
| 59      | 2 1/2" | 15'    | 3/4"  |
| 60      | 2 1/2" | 15'    | 3/4"  |
| 61      | 2 1/2" | 15'    | 3/4"  |
| 62      | 2 1/2" | 15'    | 3/4"  |
| 63      | 2 1/2" | 15'    | 3/4"  |
| 64      | 2 1/2" | 15'    | 3/4"  |
| 65      | 2 1/2" | 15'    | 3/4"  |
| 66      | 2 1/2" | 15'    | 3/4"  |
| 67      | 2 1/2" | 15'    | 3/4"  |
| 68      | 2 1/2" | 15'    | 3/4"  |
| 69      | 2 1/2" | 15'    | 3/4"  |
| 70      | 2 1/2" | 15'    | 3/4"  |
| 71      | 2 1/2" | 15'    | 3/4"  |
| 72      | 2 1/2" | 15'    | 3/4"  |
| 73      | 2 1/2" | 15'    | 3/4"  |
| 74      | 2 1/2" | 15'    | 3/4"  |
| 75      | 2 1/2" | 15'    | 3/4"  |
| 76      | 2 1/2" | 15'    | 3/4"  |
| 77      | 2 1/2" | 15'    | 3/4"  |
| 78      | 2 1/2" | 15'    | 3/4"  |
| 79      | 2 1/2" | 15'    | 3/4"  |
| 80      | 2 1/2" | 15'    | 3/4"  |
| 81      | 2 1/2" | 15'    | 3/4"  |
| 82      | 2 1/2" | 15'    | 3/4"  |
| 83      | 2 1/2" | 15'    | 3/4"  |
| 84      | 2 1/2" | 15'    | 3/4"  |
| 85      | 2 1/2" | 15'    | 3/4"  |
| 86      | 2 1/2" | 15'    | 3/4"  |
| 87      | 2 1/2" | 15'    | 3/4"  |
| 88      | 2 1/2" | 15'    | 3/4"  |
| 89      | 2 1/2" | 15'    | 3/4"  |
| 90      | 2 1/2" | 15'    | 3/4"  |
| 91      | 2 1/2" | 15'    | 3/4"  |
| 92      | 2 1/2" | 15'    | 3/4"  |
| 93      | 2 1/2" | 15'    | 3/4"  |
| 94      | 2 1/2" | 15'    | 3/4"  |
| 95      | 2 1/2" | 15'    | 3/4"  |
| 96      | 2 1/2" | 15'    | 3/4"  |
| 97      | 2 1/2" | 15'    | 3/4"  |
| 98      | 2 1/2" | 15'    | 3/4"  |
| 99      | 2 1/2" | 15'    | 3/4"  |
| 100     | 2 1/2" | 15'    | 3/4"  |

Class I Concrete Cu Yd 5840  
Reinforcement Bars L.B. 10150  
Structural Steel L.B. 10150  
Name Plates Each E

\* Weight of Rebar, Gages, Poles, Lead Poles and Anchor Bolts included in Structural Steel Est. Weight = 48,940#  
\* Weight of Expansion Devices included - Est Weight = 3440#

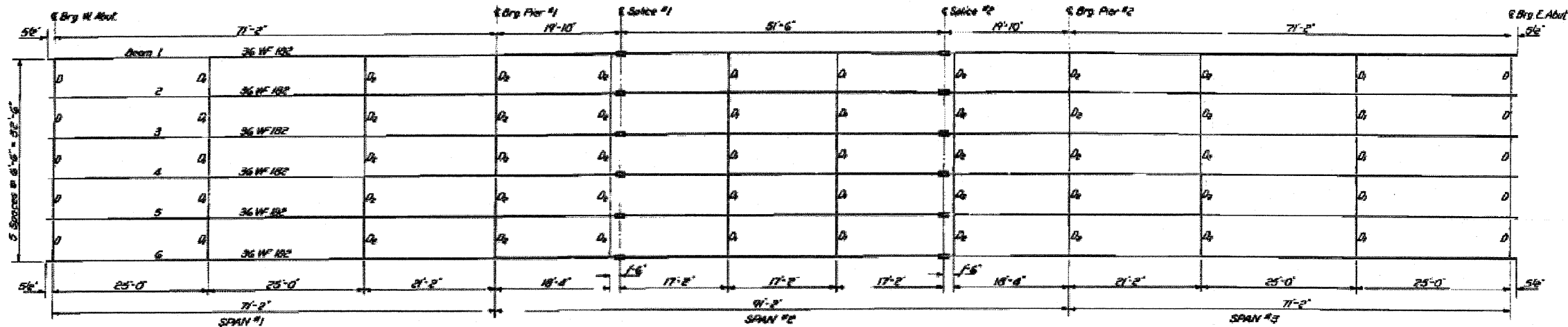


|          |                  |      |             |
|----------|------------------|------|-------------|
| DESIGNED | James M. Parnell | DATE | Nov. 2 1964 |
| CHECKED  | W. B. Parnell    |      |             |
| DRAWN    | J.L. Armstrong   |      |             |
| CHECKED  | W. B. Parnell    |      |             |

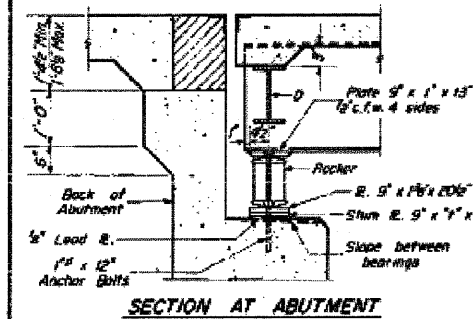
**SUPERSTRUCTURE  
BRIDGE OVER NEGRO CREEK  
F.A.I.R.D. SEC. 6-7(B).F-1  
BUREAU COUNTY  
STA. 1758+70.**

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

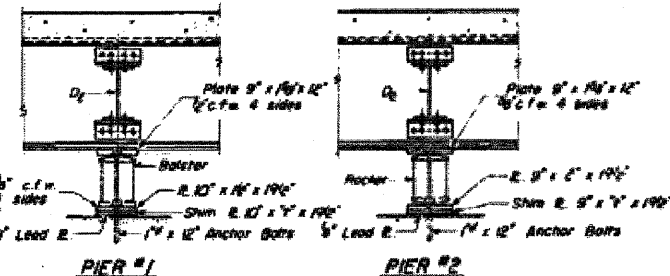
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|---------|--------|--------|---------------|-----------|
| DATE    | BY     | SCALE  | NO. OF SHEETS | SHEET NO. |
| 11-1-68 | W.A.S. | BUREAU | 43            | 13        |



LAYOUT OF STRUCTURAL STEEL

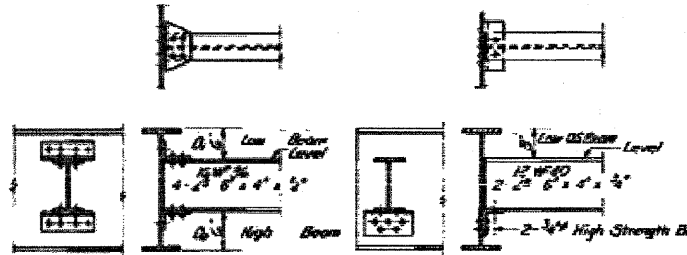


SECTION AT ABUTMENT



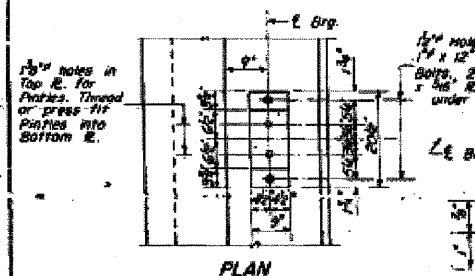
PIER #1

PIER #2

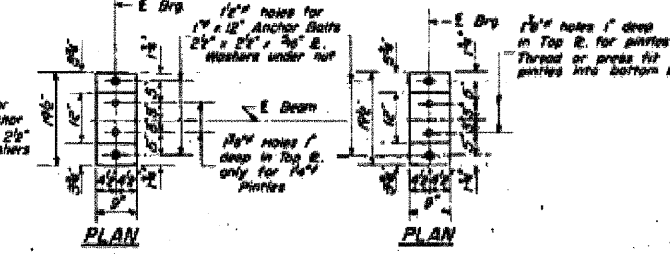


DIAPHRAGM D1 & D2

DIAPHRAGM D



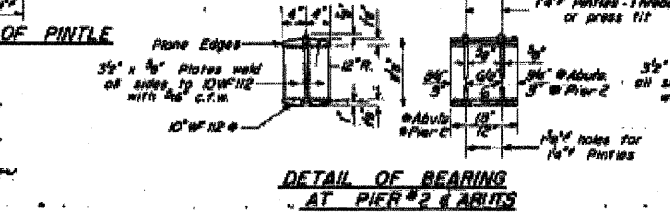
PLAN



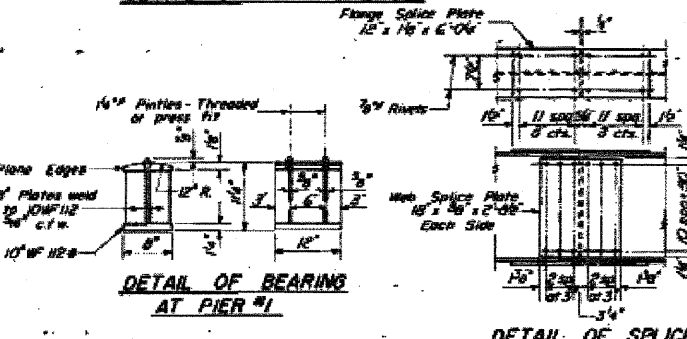
DETAIL OF PINTLE



DETAIL OF COVER PLATES



DETAIL OF BEARING AT PIER #2 & ABUTS



DETAIL OF BEARING AT PIER #1

DETAIL OF SPLICE

FOR FABRICATION ONLY

ELEVATION OF STEEL - TOP OF BEAMS  
These do not include Dead Load Deflection of Steel

| Beam | E. Brg. W. Abut. | E. Brg. Pier #1 | E. Splice #1 | E. Splice #2 | E. Brg. Pier #2 | E. Brg. E. Abut. |
|------|------------------|-----------------|--------------|--------------|-----------------|------------------|
| 1    | 646.533          | 646.530         | 646.67       | 646.907      | 646.990         | 647.306          |
| 2    | 646.818          | 646.645         | 646.736      | 646.972      | 647.063         | 647.511          |
| 3    | 646.888          | 646.711         | 646.802      | 647.038      | 647.129         | 647.636          |
| 4    | 646.481          | 646.776         | 646.867      | 647.103      | 647.194         | 647.681          |
| 5    | 646.525          | 646.862         | 646.953      | 647.169      | 647.260         | 647.586          |
| 6    | 646.580          | 646.907         | 646.998      | 647.234      | 647.325         | 647.690          |

TABLE OF 'Y' DIMENSIONS

| Beam             | 1 | 2  | 3 | 4  | 5 | 6  |
|------------------|---|----|---|----|---|----|
| E. Brg. W. Abut. | 0 | 4' | 0 | 2' | 0 | 3' |
| E. Brg. Pier #1  | 0 | 4' | 0 | 4' | 0 | 4' |
| E. Brg. Pier #2  | 0 | 4' | 0 | 4' | 0 | 4' |
| E. Brg. E. Abut. | 0 | 4' | 0 | 4' | 0 | 4' |

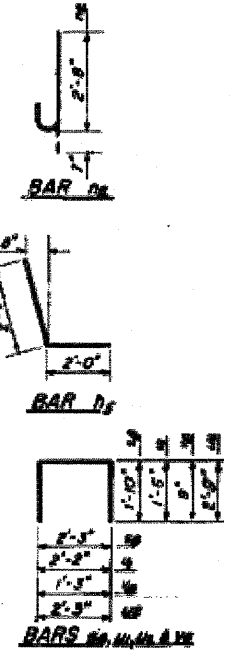
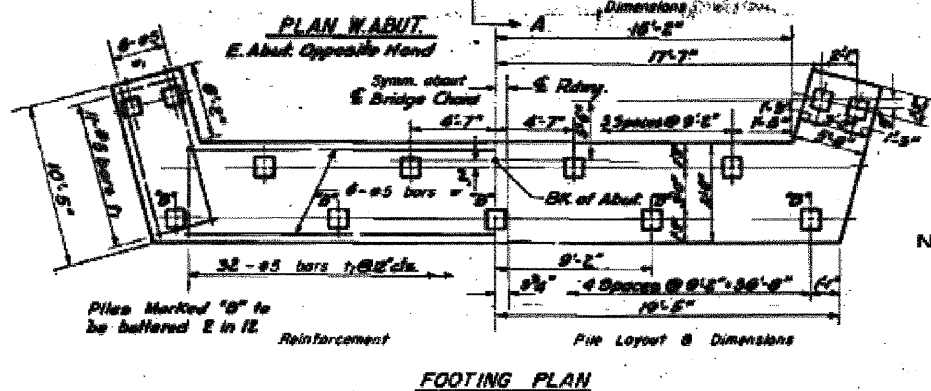
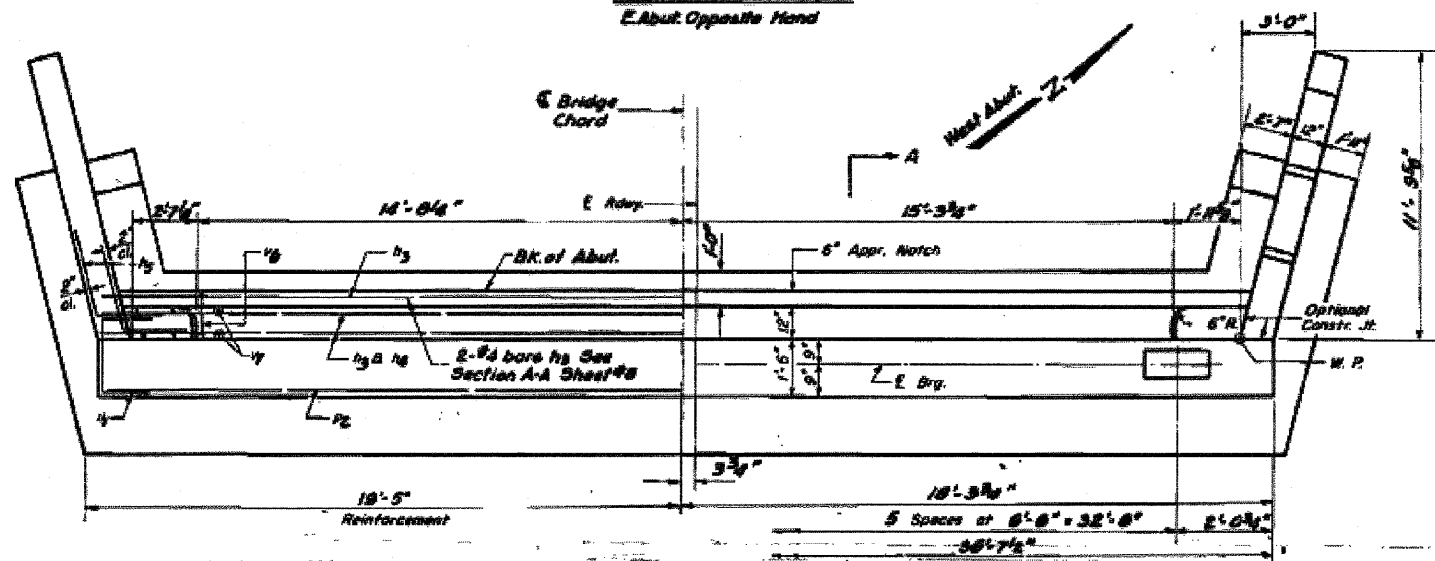
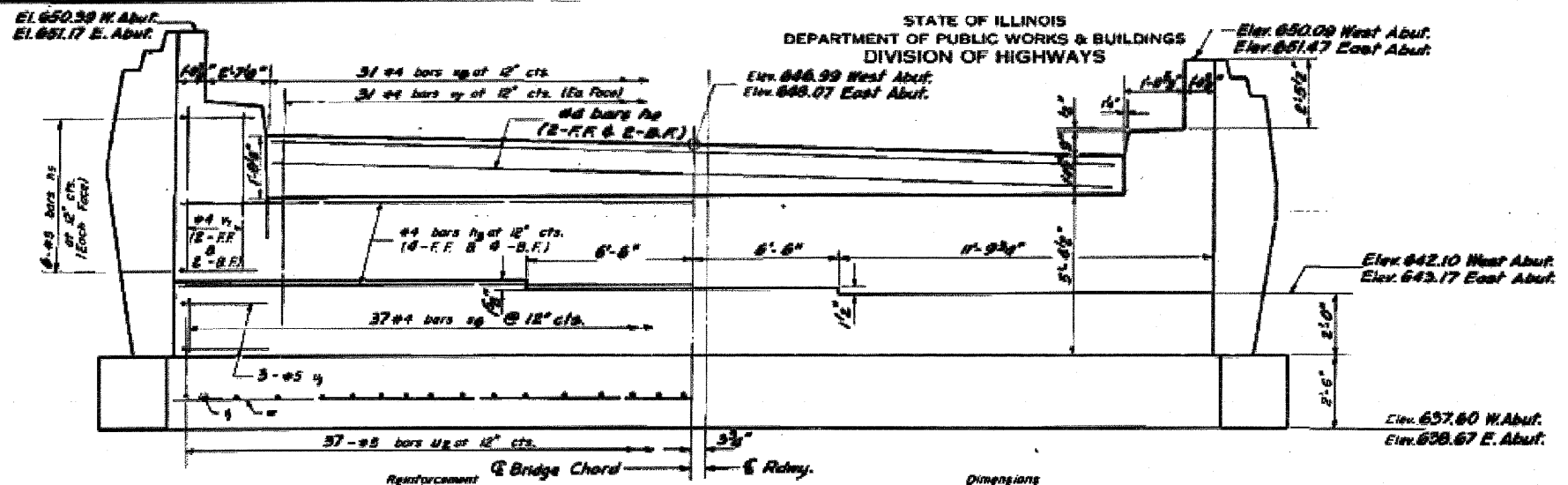
STRUCTURAL STEEL  
BRIDGE OVER NEGRO CREEK  
FAI RT. 90 SEC. 6-T881, P. 1  
BUREAU COUNTY  
STA. 1768+70

DESIGNED: James M. Ponce  
CHECKED: Dale H. S...  
DRAWN: W. A. Sausaman  
DATE: Nov 2 1968

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|               |               |               |               |               |
|---------------|---------------|---------------|---------------|---------------|
| NO. OF SHEETS | NO. OF SHEETS | NO. OF SHEETS | NO. OF SHEETS | NO. OF SHEETS |
| 13            | 13            | 13            | 13            | 13            |

SHEET NO. 7  
13 SHEETS



**2 BRIDGES  
E & W ABUTMENT  
BAR LIST**

| Bar | No. | Size | Length | Shape |
|-----|-----|------|--------|-------|
| 1   | 20  | #4   | 18'-9" | —     |
| 2   | 32  | #4   | 18'-6" | —     |
| 3   | 36  | #5   | 4'-8"  | L     |
| 12  | 144 | #5   | 4'-0"  | —     |
| 13  | 20  | #7   | 36'-5" | —     |
| 14  | 144 | #4   | 5'-11" | □     |
| 15  | 36  | #5   | 1'-0"  | —     |
| 16  | 24  | #5   | 5'-2"  | □     |
| 17  | 144 | #5   | 7'-0"  | □     |
| 18  | 124 | #4   | 2'-9"  | □     |
| 19  | 240 | #4   | 5'-9"  | —     |
| 20  | 48  | #5   | 16'-0" | —     |
| 21  | 8   | #5   | 4'-9"  | —     |

**2 BRIDGES  
E & W ABUTMENT  
BILL OF MATERIAL**

| Item                 | Unit     | Quantity |
|----------------------|----------|----------|
| Class X Concrete     | Cu. Yds. | 1,144.0  |
| Reinforcement Bars   | Lbs.     | 11,540   |
| Concrete Piles       | Lin. Ft. | 2,378    |
| Test Piles, Concrete | Each     | 2        |

**PILE DATA**  
Type - Concrete  
Capacity - 50 Ton Min.  
Est Length - 50 Feet E. Abut, 48 Feet W. Abut.  
No. Rock - 50 + 2 Test Piles

Note: One test pile to be driven in a permanent location each; West Abut. North Bridge and East Abut. South Bridge.

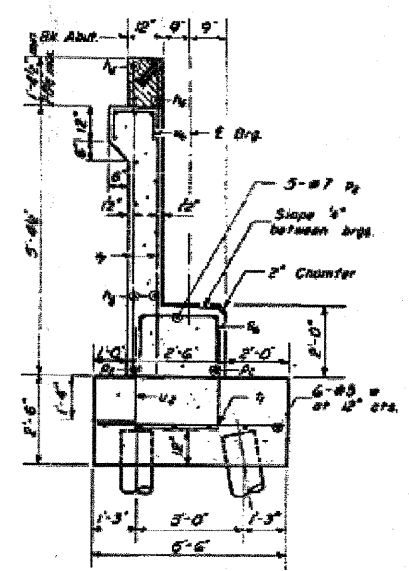
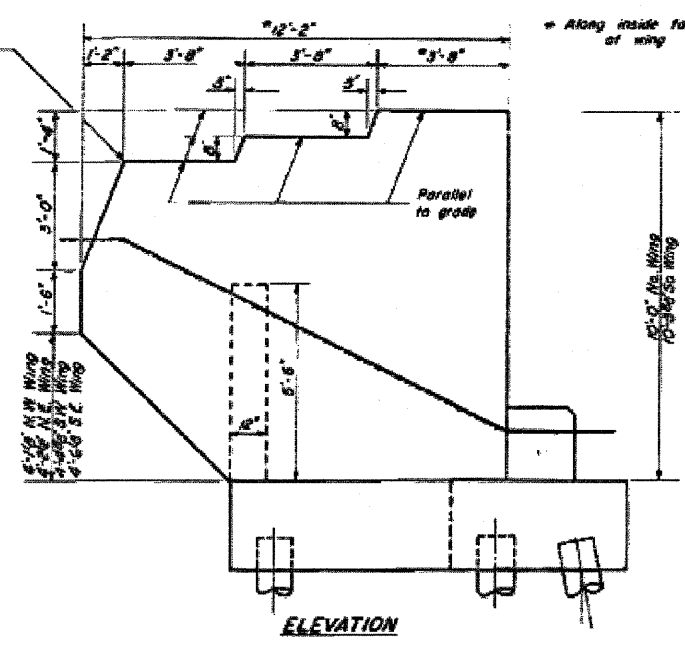
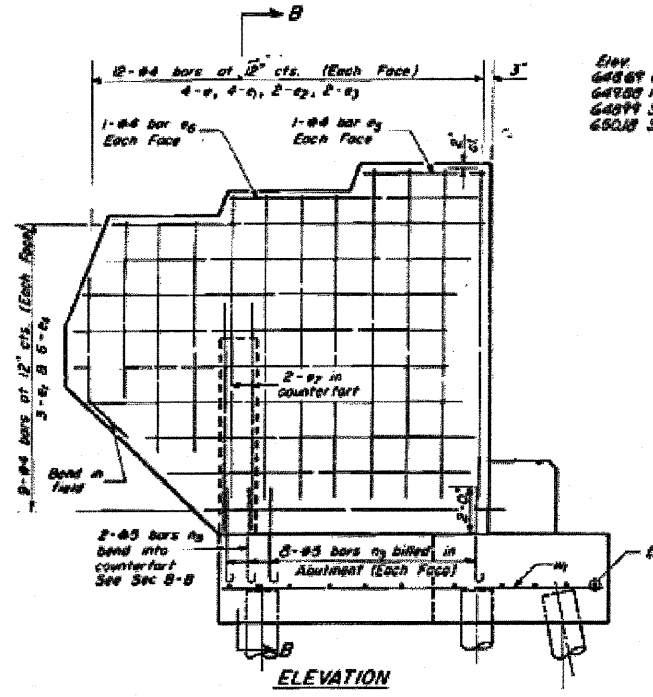
**EAST & WEST ABUTMENTS  
BRIDGE OVER NEGRO CREEK  
F.A.I. RT. 80 SEC. 8-7B-1  
BUREAU COUNTY  
STA. 1758+70**

DESIGNED: James M. Penner  
CHECKED: D. J. [Signature]  
DRAWN: [Signature]  
DATE: Nov 2 1966

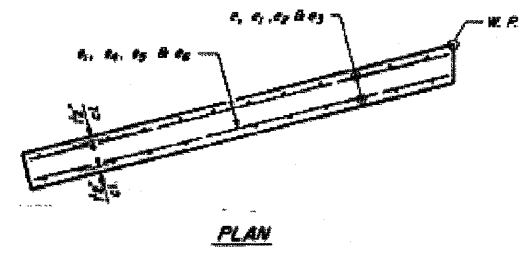
A-7 Drawn 1-27-60

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

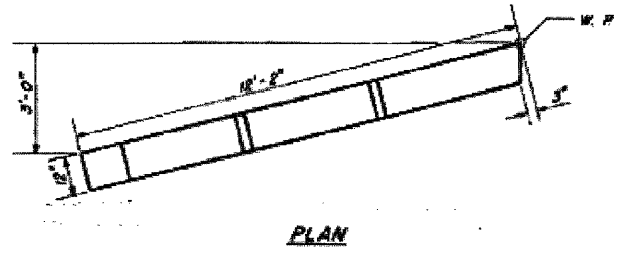
|             |        |              |       |
|-------------|--------|--------------|-------|
| DATE        | BY     | CHKD         | APP'D |
| 11-1-60     | DL-184 | BUREAU       | 4E    |
| SHEET NO. 1 |        | OF 13 SHEETS |       |



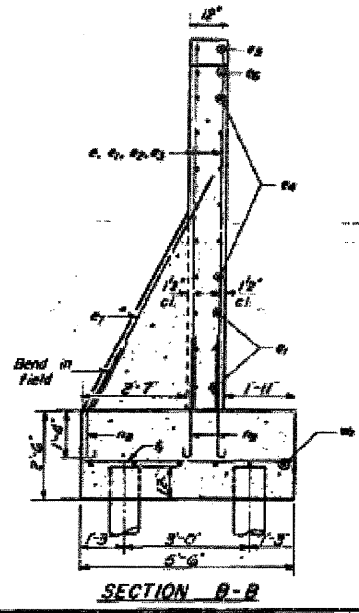
SECTION A-A



PLAN



PLAN



SECTION B-B

ONE END POST  
BILL OF MATERIAL

| Bar | No. | Size | Length | Shape |
|-----|-----|------|--------|-------|
| #1  | 8   | #4   | 9'-3"  | ---   |
| #2  | 4   | #4   | 8'-6"  | ---   |
| #3  | 4   | #4   | 6'-6"  | ---   |
| #4  | 4   | #4   | 5'-0"  | ---   |
| #5  | 12  | #4   | 10'-6" | ---   |
| #6  | 2   | #4   | 3'-6"  | ---   |
| #7  | 2   | #4   | 7'-0"  | ---   |
| #8  | 2   | #5   | 7'-0"  | ---   |

\* Class X Concrete Cu. Yds. 3.7  
\* Reinforcement Bars Lbs. 270

\* QUANTITIES INCLUDED WITH THE QUANTITIES ON SHEET 7 OF 13.

END POST  
BRIDGE OVER NEGRO CREEK  
F.A.I. RT. 60 SEC. 6-7B-1  
BUREAU COUNTY  
STA. 1755+70

DESIGNED: James H. Ponce  
CHECKED: D. J. P.  
DRAWN: W. A. Schaefer  
DATE: Nov 2 1960

AW-1 Drawn 1-29-60

|   |  |            |           |
|---|--|------------|-----------|
| FILE NAME =                             | USER NAME = breboypc                   | DESIGNED - | REVISED - |
| c:\pwwork\pwwork\breboypc\d0112640\d368 | 908-shd-details.DGN                    | DRAWN -    | REVISED - |
|   | PLOT SCALE = 50.0638' / IN.            | CHECKED -  | REVISED - |
|   | PLOT DATE = Sep 07, 2009 - 01:38:37 PM | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING BRIDGE PLANS (FOR INFORMATION ONLY)

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_ OF \_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

|                             |         |        |              |           |
|-----------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.                 | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-80                        | **      | BUREAU | 344          | 278       |
|                             |         |        | 66908        |           |
| [ILLINOIS] FED. AID PROJECT |         |        |              |           |

\*\*06-[7BR&BR-1,7VB-M,6BR&6.7 RS-1 & 1

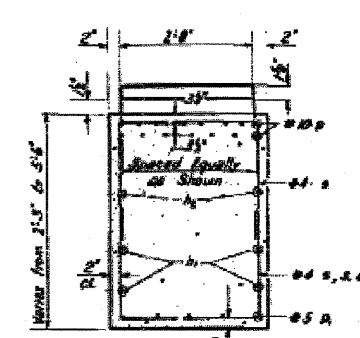
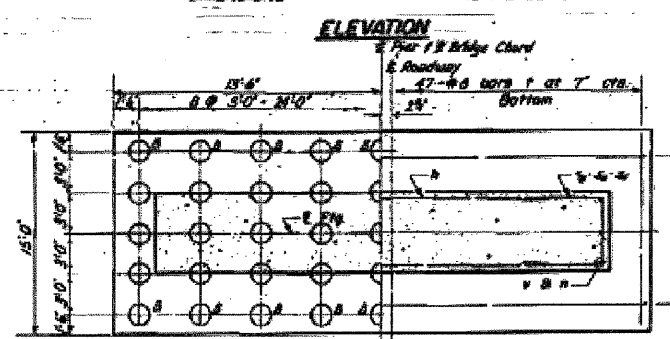
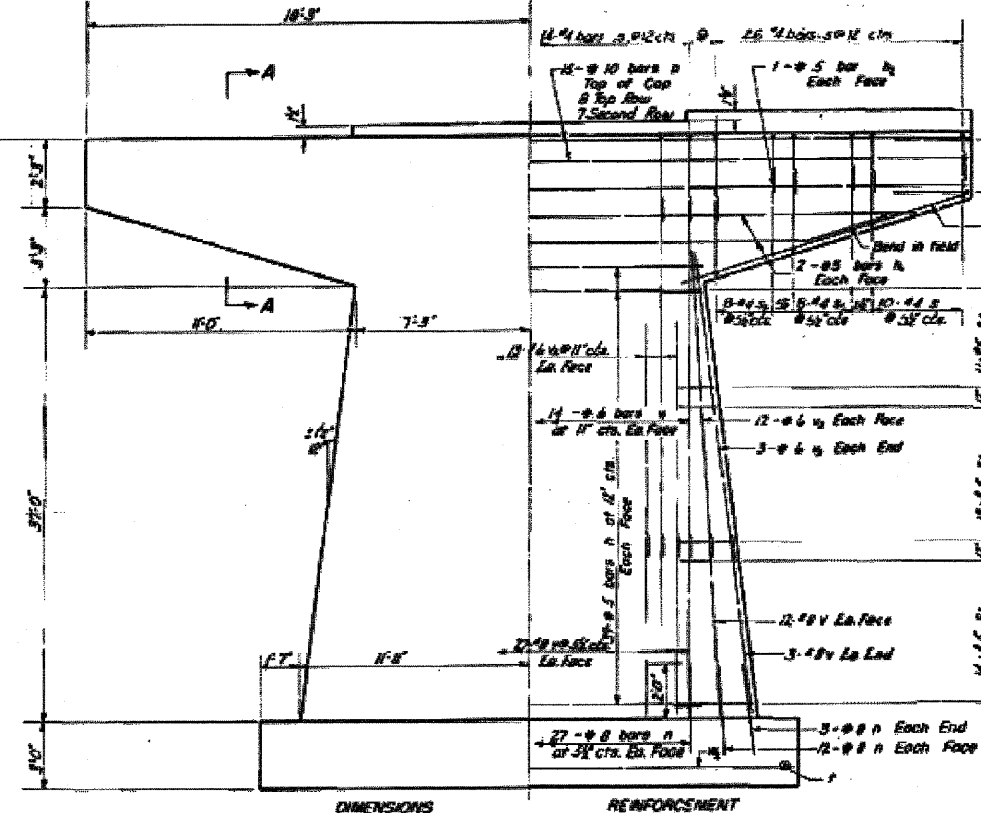
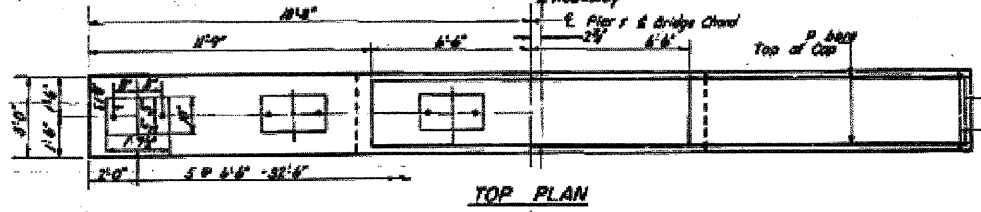
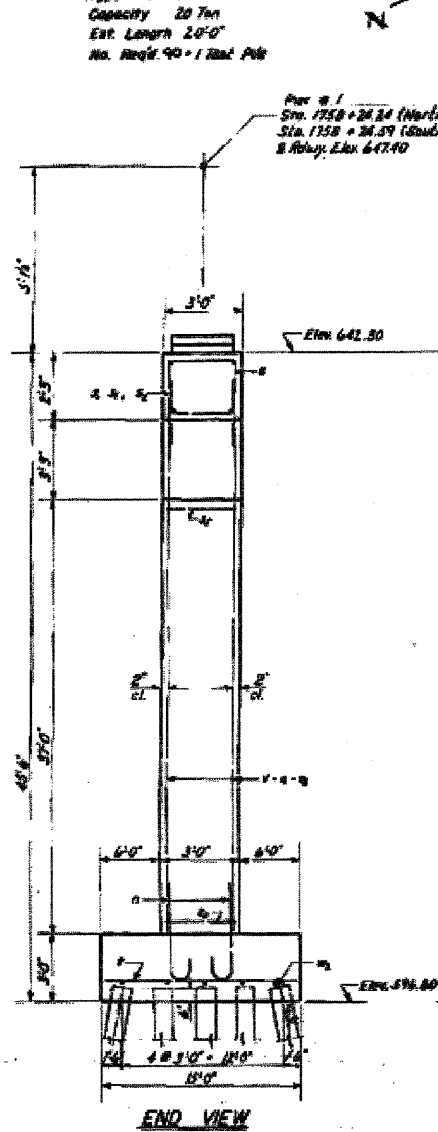
STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|         |         |         |         |         |
|---------|---------|---------|---------|---------|
| NO. 100 | NO. 100 | NO. 100 | NO. 100 | NO. 100 |
| 100     | 100     | 100     | 100     | 100     |
| 100     | 100     | 100     | 100     | 100     |
| 100     | 100     | 100     | 100     | 100     |

SHEET NO. 9  
12 SHEETS

**PILE DATA**  
Type - Crossed Pile  
Capacity 20 Ton  
Est. Length 24'-0"  
No. Bars 90 - 1 1/2" dia.

Pier # 1  
Sta. 1758 + 24.24 (North)  
Sta. 1758 + 24.29 (South)  
R. Hwy. E. No. 64740



Note: All edges shall have standard 1/4" chamfers except footings.

**A & B DIMENSIONS**

| Bar | A     | B     |
|-----|-------|-------|
| 1   | 2'-0" | 1'-0" |
| 2   | 2'-0" | 3'-0" |
| 3   | 2'-0" | 5'-1" |
| 4   | 1'-0" | 3'-0" |
| 5   | 1'-0" | 2'-0" |

**S BARS**



**U BAR**



**D BAR**



**2 BRIDGES  
PIER 1  
BILL OF MATERIAL**

| Bar                      | No. | Size      | Length  | Shape |
|--------------------------|-----|-----------|---------|-------|
| h                        | 126 | #5        | 14'-3"  | —     |
| i                        | 6   | #5        | 18'-3"  | —     |
| j                        | 8   | #5        | 18'-9"  | —     |
| k                        | 216 | #8        | 5'-7"   | —     |
| l                        | 30  | #10       | 34'-3"  | —     |
| m                        | 30  | #5        | 12'-0"  | —     |
| n                        | 172 | #4        | 5'-0"   | —     |
| o                        | 32  | #4        | 8'-8"   | —     |
| p                        | 32  | #4        | 10'-10" | —     |
| q                        | 36  | #5        | 14'-0"  | —     |
| r                        | 22  | #5        | 10'-8"  | —     |
| s                        | 48  | #5        | 7'-8"   | —     |
| t                        | 90  | #8        | 14'-9"  | —     |
| u                        | 12  | #6        | 10'-7"  | —     |
| v                        | 226 | #8        | 15'-0"  | —     |
| w                        | 36  | #6        | 23'-6"  | —     |
| x                        | 42  | #6        | 10'-8"  | —     |
| y                        | 108 | #6        | 20'-3"  | —     |
| z                        | 10  | #5        | 24'-6"  | —     |
| Class X Concrete         |     | Qty. Yds. | 284.8   |       |
| Reinforcement Bars       |     | Lbs.      | 35,480  |       |
| Crossed Piles            |     | Lin. Ft.  | 1800    |       |
| Steel Piles              |     | Each      | 1       |       |
| Class B Soil for Struct. |     | Cu. Yds.  | 126     |       |
| Class C Energy Spread    |     | Cu. Yds.  | 218     |       |

PIER 1  
F.A.I. RT. 80 SEC. 6-7B-1  
BUREAU COUNTY  
STA. 1758 + 20

DESIGNED James H. Ponce  
CHECKED A. J. S.  
DRAWN W. A. S.  
NOV 2 1960

P-2 Re-drawn 7-17-58 Rev. 8-25-59

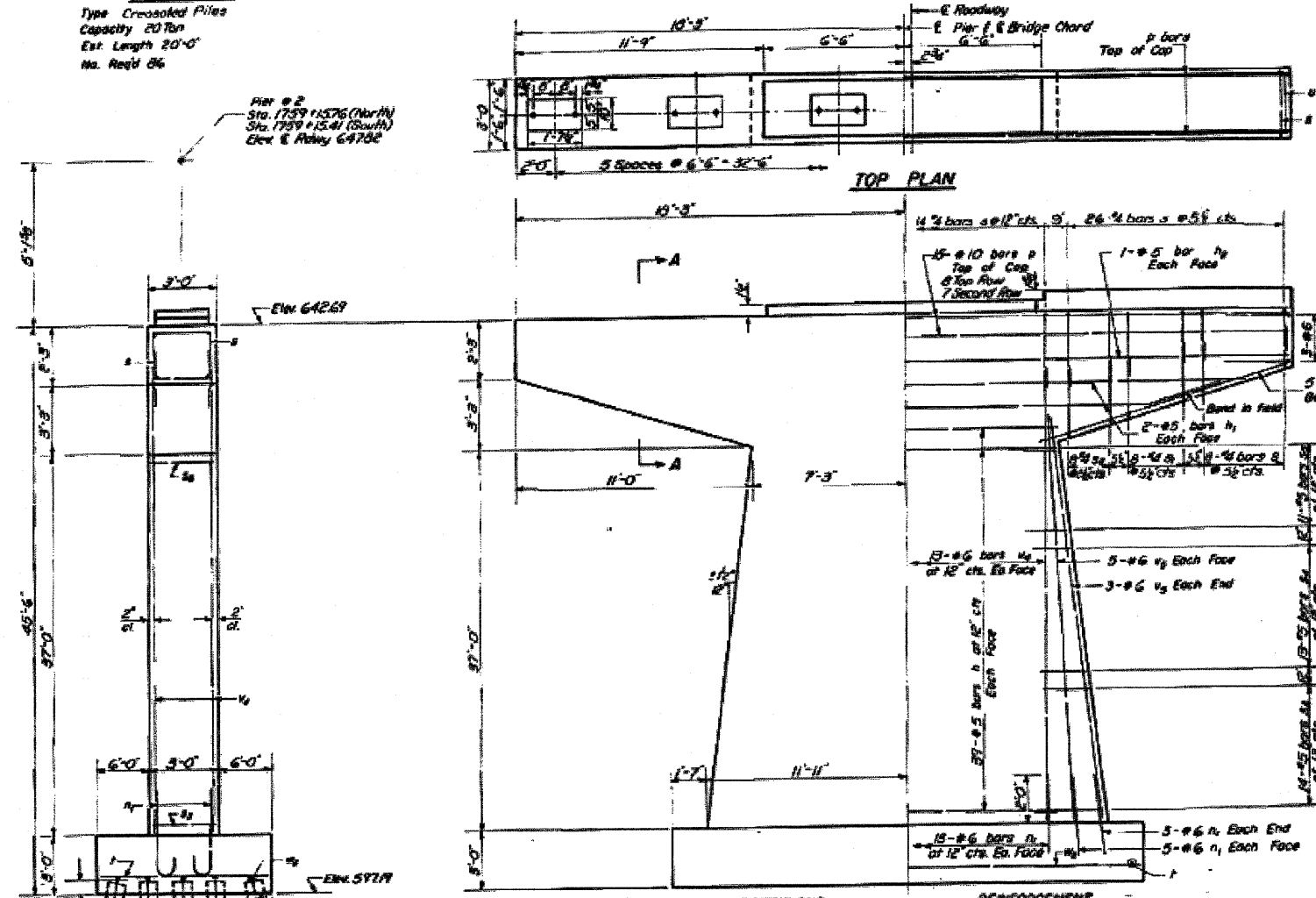
STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|         |         |         |           |              |
|---------|---------|---------|-----------|--------------|
| DATE    | BY      | PROJECT | NO.       | SHEET NO. OF |
| 11-1-80 | DA-78-1 | BUREAU  | 43        | 13           |
| SHEETS  |         |         | 13 SHEETS |              |

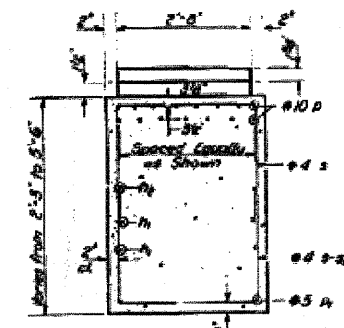
**FILE DATA**

Type Crossed Piles  
Capacity 20 Ton  
Est. Length 20'-0"  
No. Reqd 86

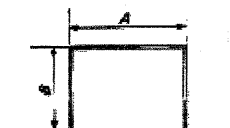
PIER #2  
Sta. 1759+15.76 (North)  
Sta. 1759+15.41 (South)  
Elev. & Rowly 64700



Note: All edges shall have standard 1/4" chamfers except footings.

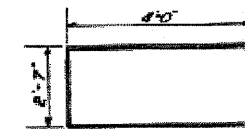


SECTION A-A



| Bar | A     | B      |
|-----|-------|--------|
| 1   | 2'-8" | 1'-11" |
| 2   | 2'-8" | 3'-0"  |
| 3   | 2'-8" | 4'-7"  |
| 4   | 2'-8" | 5'-8"  |
| 5   | 2'-8" | 3'-11" |
| 6   | 2'-8" | 2'-3"  |

3 BARS



U BAR



D BAR

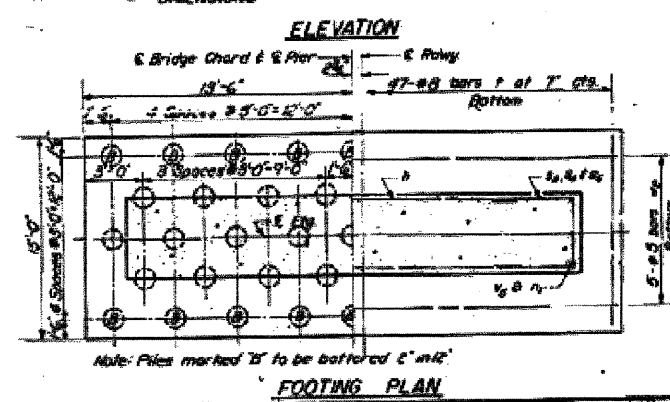
**2 BRIDGES  
PIER #2  
BILL OF MATERIAL**

| Bar                    | No. | Size | Length  | Shape  |
|------------------------|-----|------|---------|--------|
| n                      | 156 | #5   | 14'-3"  | —      |
| h                      | 16  | #5   | 18'-3"  | —      |
| h <sub>2</sub>         | 8   | #5   | 18'-9"  | —      |
|                        |     |      |         |        |
| n                      | 104 | #6   | 5'-0"   | —      |
| p                      | 30  | #10  | 36'-3"  | —      |
| h                      | 20  | #5   | 12'-0"  | —      |
|                        |     |      |         |        |
| h                      | 172 | #4   | 6'-6"   | □      |
| h                      | 32  | #4   | 8'-8"   | □      |
| h                      | 32  | #4   | 10'-10" | □      |
| h                      | 56  | #5   | 14'-0"  | □      |
| h                      | 52  | #5   | 10'-6"  | □      |
| h                      | 44  | #5   | 7'-6"   | □      |
|                        |     |      |         |        |
| f                      | 96  | #8   | 14'-9"  | —      |
| v                      | 12  | #6   | 10'-7"  | —      |
|                        |     |      |         |        |
| h                      | 52  | #6   | 48'-3"  | —      |
| h                      | 52  | #6   | 38'-0"  | —      |
| h                      | 10  | #5   | 26'-6"  | —      |
|                        |     |      |         |        |
| Class X Concrete       |     |      | Cu Yds  | 6060   |
| Reinforcement Bars     |     |      | Lbs     | 21,770 |
| Crossed Piles          |     |      | Lin Ft  | 1720   |
| Class A Exc. for Sheet |     |      | Cu Yds  | 396    |
| Class B Exc. for Sheet |     |      | Cu Yds  | 201    |

PIER #2  
F.A.I. RT. 60 SEC. 6-7B-1  
BUREAU COUNTY  
STA. 1758+70

DESIGNED: James K. Ponce  
CHECKED: Dale E. ...  
DRAWN: M.A. ...  
APPROVED: ...  
DATE: Nov. 2 1980

P-2 Re-drawn 7-17-55 Rev. 1-25-58



FOOTING PLAN

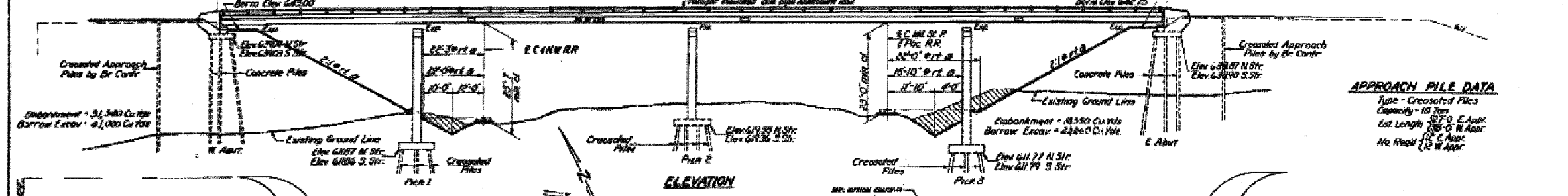
\*\*06-[7BR&BR-1,7VB-M,6BR&6.7 RS-1 & I



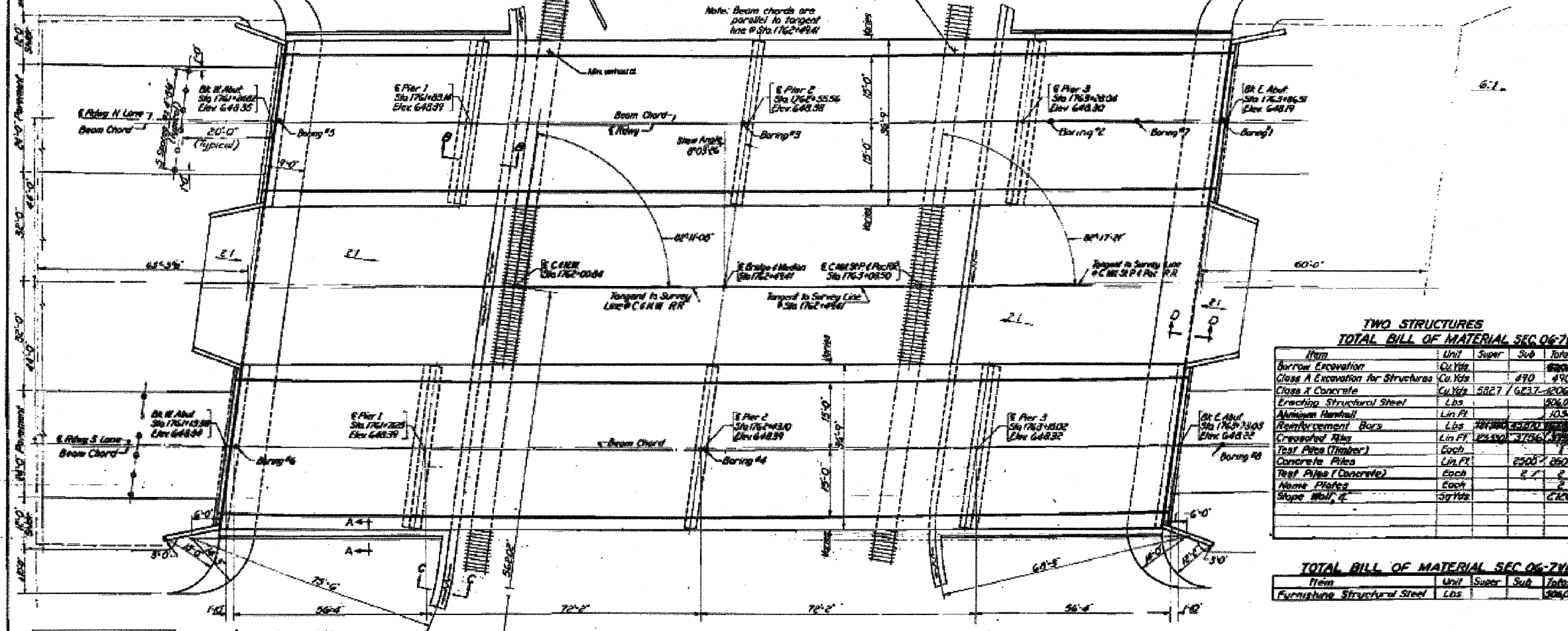
B.M. 94 3/4" iron pipe 107 feet South of Sta 176+50.0 109 feet East of a railroad right of way fence 0.5 ft North of an East-West fence Elev 674.36

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS  
240'-0" Br. RR Abut. (Along Bridge Chords)

|              |           |        |    |    |
|--------------|-----------|--------|----|----|
| PROJECT NO.  | 206713    | BUREAU | 43 | 17 |
| PROJECT NAME | 80104-777 |        | 14 | 9  |



**APPROACH PILE DATA**  
Type - Crossed Piles  
Capacity - 10 tons  
Est. Length - 10'-0" N. Abut.  
12'-0" S. Abut.  
No. Piles - 12 N. Abut.  
12 S. Abut.



**TWO STRUCTURES  
TOTAL BILL OF MATERIAL SEC. 06-71B**

| Item                              | Unit   | Super  | Sub    | Total   |
|-----------------------------------|--------|--------|--------|---------|
| Borrow Excavation                 | Cu Yds |        |        | 490     |
| Class A Excavation for Structures | Cu Yds |        |        | 490     |
| Class X Concrete                  | Cu Yds | 5827   | 6237   | 12064   |
| Erecting Structural Steel         | Lbs    |        |        | 306,000 |
| Aluminum Hardware                 | Lin Ft |        |        | 10,547  |
| Reinforcement Bars                | Lbs    |        |        | 24,000  |
| Crossed Piles                     | Lin Ft | 12,000 | 12,000 | 24,000  |
| Test Piles (Timber)               | Each   |        |        | 2,500   |
| Concrete Piles                    | Lin Ft |        |        | 2,500   |
| Test Piles (Concrete)             | Each   |        |        | 2       |
| Name Plates                       | Each   |        |        | 2       |
| Slope Wall, E.                    | Sq Yds |        |        | 2,100   |

**TOTAL BILL OF MATERIAL SEC. 06-71F**

| Item                        | Unit | Super | Sub | Total   |
|-----------------------------|------|-------|-----|---------|
| Furnishing Structural Steel | Lbs  |       |     | 306,000 |

DESIGNED M. J. ...  
CHECKED L. J. ...  
DRAWN J. L. Armstrong  
CHECKED G. F.

DATE: APR 20 1961  
SCALE: 1/4" = 1'-0"

NOTE: For Sec. A-A & Sec. B-B & Sec. C-C. See Sheet #2

**DESIGN STRESSES**  
f<sub>c</sub> = 1400 psi Super & Sub  
f<sub>s</sub> = 75 ksi  
f<sub>t</sub> = 2000 psi  
f<sub>v</sub> = 2000 psi  
LOADING H-20-S16-44 ALTERNATE

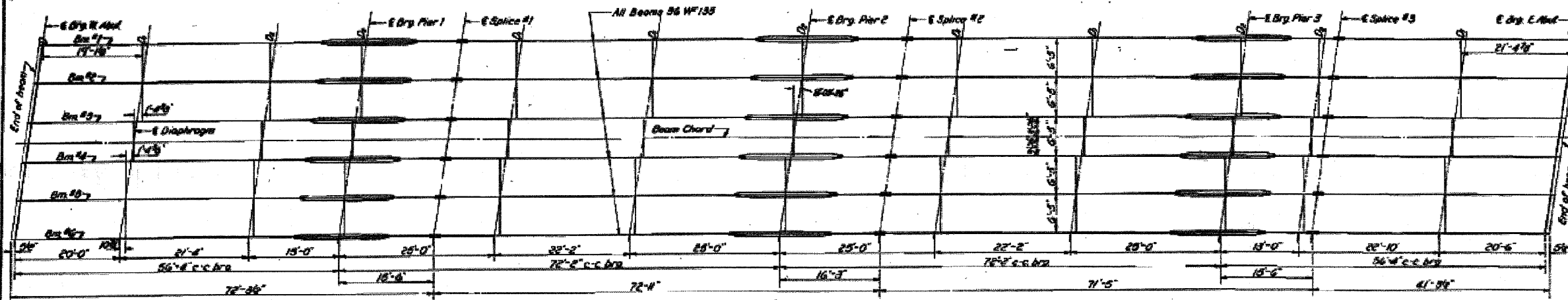
PROJ. ETC-2(13)69  
GENERAL PLAN & ELEVATION  
CENTRAL RAILROAD BRIDGE  
FAIRBANKS, ILLINOIS  
BUREAU COUNTY  
STA. 176+49.4



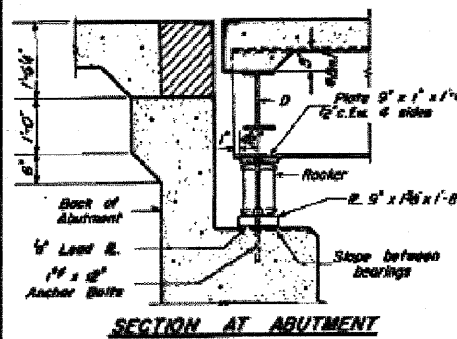
STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|     |         |        |       |        |
|-----|---------|--------|-------|--------|
| NO. | DATE    | BY     | CHKD. | APP'D. |
| 1   | 12-1-58 | BUREAU | 21    | 28     |
| 2   | 12-1-58 |        | 12    | 12     |

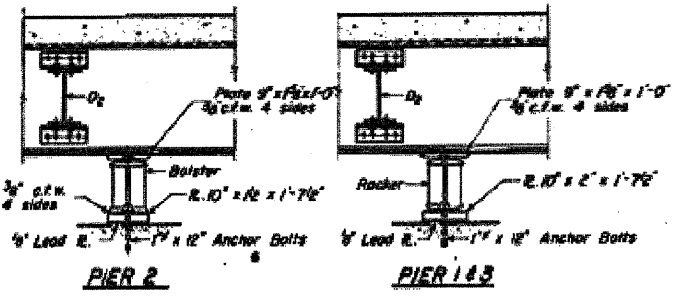
8 SHEETS



PLAN OF STRUCTURAL STEEL

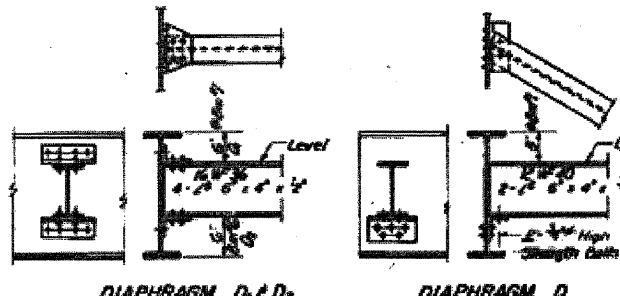


SECTION AT ABUTMENT



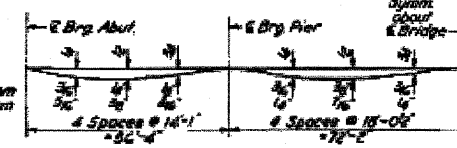
PIER 2

PIER 143



DIAPHRAGM D1 & D2  
NO Required

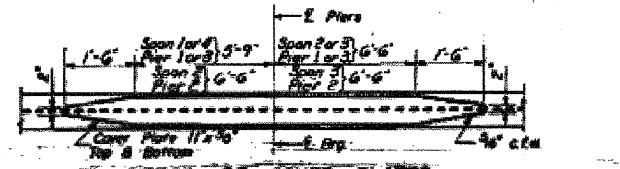
DIAPHRAGM D  
NO Required



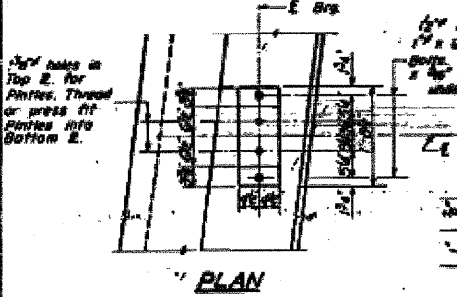
DEAD LOAD DEFLECTION DIAGRAM  
Includes weight of concrete only

ELEVATION TOP OF BEAMS

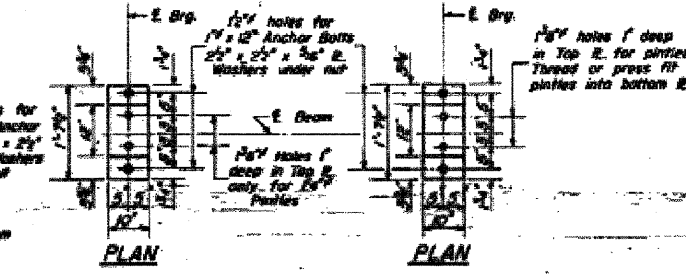
| Beam Location | 10' W. Abut. | Pier 1 | Splice 1 | Pier 2 | Splice 2 | Pier 3 | Splice 3 | 60' W. Abut. |
|---------------|--------------|--------|----------|--------|----------|--------|----------|--------------|
| Structure 1   | G4763        | G4760  | G4759    | G4753  | G4750    | G4750  | G4749    | G4746        |
| 2             | G4770        | G4766  | G4765    | G4761  | G4760    | G4757  | G4756    | G4752        |
| 3             | G4776        | G4772  | G4771    | G4767  | G4766    | G4762  | G4761    | G4759        |
| 4             | G4782        | G4779  | G4777    | G4774  | G4773    | G4769  | G4768    | G4765        |
| 5             | G4789        | G4785  | G4784    | G4780  | G4779    | G4775  | G4774    | G4771        |
| 6             | G4795        | G4791  | G4790    | G4786  | G4784    | G4781  | G4780    | G4778        |
| Structure 2   | G4762        | G4759  | G4759    | G4756  | G4755    | G4752  | G4751    | G4749        |
| 1             | G4766        | G4763  | G4763    | G4760  | G4761    | G4758  | G4757    | G4755        |
| 2             | G4772        | G4770  | G4771    | G4768  | G4769    | G4766  | G4765    | G4762        |
| 3             | G4778        | G4776  | G4777    | G4774  | G4775    | G4771  | G4770    | G4768        |
| 4             | G4784        | G4784  | G4784    | G4781  | G4780    | G4777  | G4776    | G4774        |
| 5             | G4790        | G4791  | G4790    | G4787  | G4786    | G4783  | G4782    | G4780        |



DETAIL OF COVER PLATES



PLAN



PLAN

PLAN

DETAIL OF PINTLE

DETAIL OF BEARING AT PIERS 143 & ABUT

DETAIL OF BEARING AT PIER 2

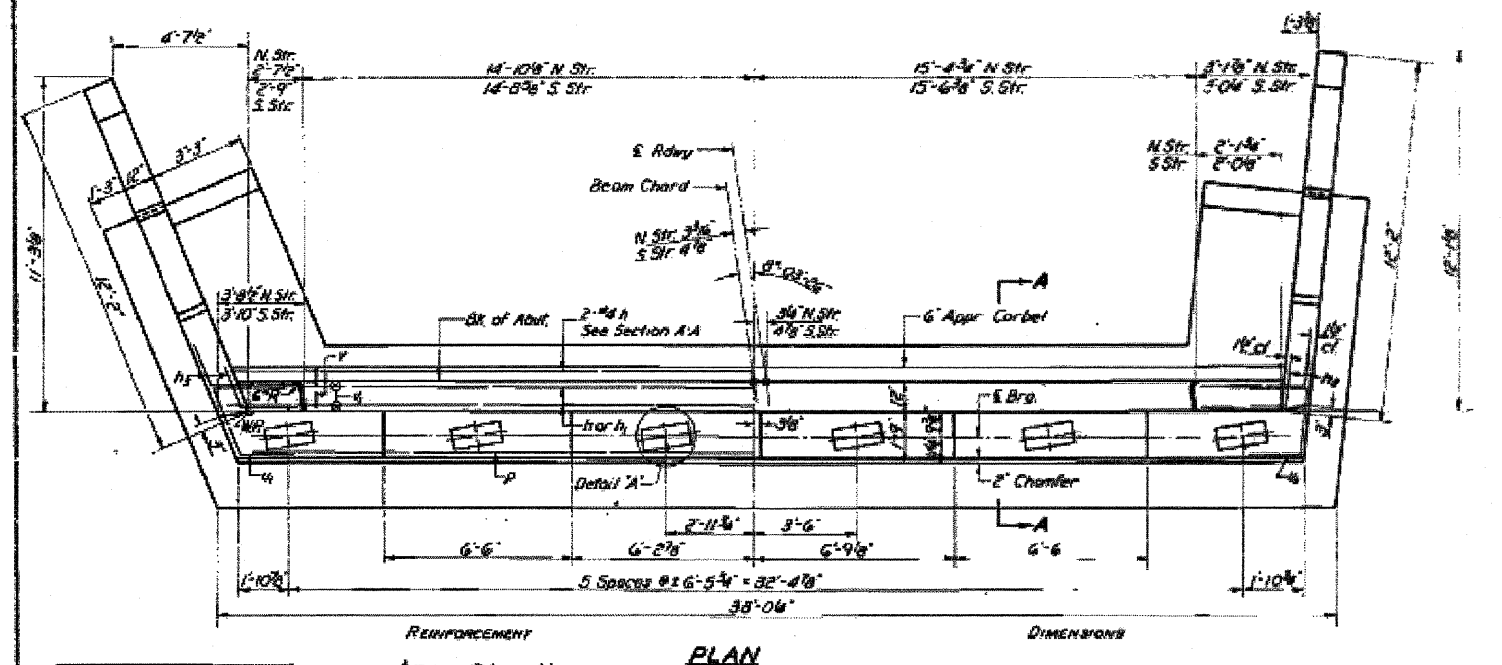
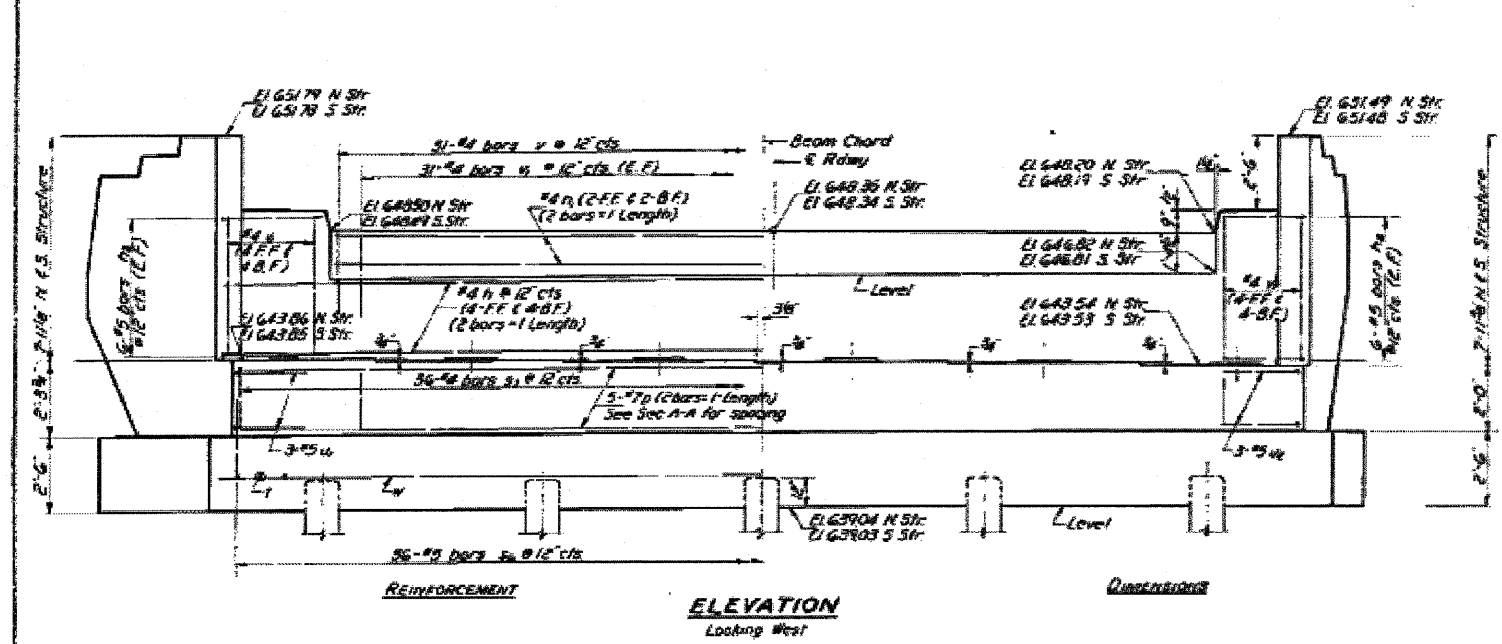
DETAIL OF SPLICE

DESIGNED BY: M. J. ...  
CHECKED BY: ...  
DATE: APR 23 1961

STRUCTURAL STEEL LAYOUT  
NORTH & SOUTH STRUCTURE  
R.A.I. RE. 80. SEC. 06-TVB.F  
BUREAU COUNTY  
STA 1762+49.41

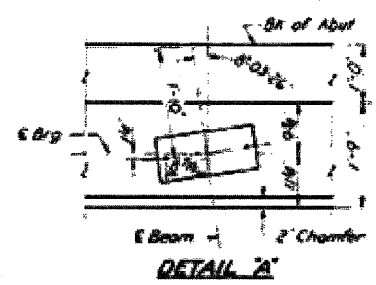
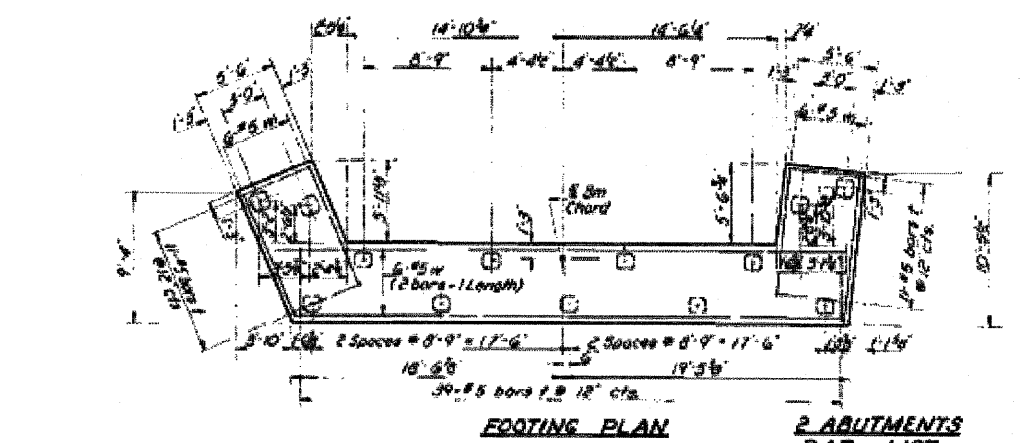
STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|             |        |        |       |           |
|-------------|--------|--------|-------|-----------|
| DATE        | BY     | CHKD   | APP'D | SHEET NO. |
| 11-80       | DE-7VB | BUREAU | 43    | 21        |
| PROJECT NO. |        |        |       | 5 SHEETS  |



|          |             |
|----------|-------------|
| DESIGNED | APR 24 1961 |
| CHECKED  |             |
| DRAWN    |             |
| APPROVED |             |

Note: For details of road post and Sec. A-A see Sheet #7.



**2 ABUTMENTS BAR LIST**

| Bar            | No. | Type | Length | Shape |
|----------------|-----|------|--------|-------|
| A              | 40  | #4   | 19'-0" | —     |
| A <sub>1</sub> | 16  | #4   | 15'-6" | —     |
| A <sub>2</sub> | 24  | #5   | 6'-0"  | —     |
| A <sub>3</sub> | 24  | #5   | 5'-0"  | —     |
| A              | 20  | #7   | 19'-6" | —     |
| A <sub>4</sub> | 72  | #5   | 8'-6"  | —     |
| A <sub>5</sub> | 72  | #4   | 6'-2"  | —     |
| A              | 122 | #5   | 5'-3"  | —     |
| A <sub>6</sub> | 6   | #5   | 5'-8"  | —     |
| A <sub>7</sub> | 6   | #5   | 5'-6"  | —     |
| A              | 62  | #4   | 2'-9"  | —     |
| A              | 156 | #4   | 6'-3"  | —     |
| A              | 24  | #5   | 19'-0" | —     |
| A              | 24  | #5   | 9'-6"  | —     |

**2 ABUTMENTS BILL OF MATERIAL**

| Item                  | Unit   | Quantity |
|-----------------------|--------|----------|
| Class X Concrete      | Cu Yds | 69.6     |
| Reinforcement Bars    | Lbs.   | 6320     |
| Concrete Piles        | Lin Ft | 1850     |
| Test Piles (Concrete) | Each   | 1        |

Note: Bill of Material includes Class X Concrete and Reinforcement Bars for End Posts.

**BAR S<sub>1</sub> & U<sub>1</sub>**

**PILE DATA**  
Type - Concrete Piles  
Capacity - 90 Tons  
Est. Length - 60'-0"  
No. Reqd - 25 + 1 Test Pile (N Structure)

**W. ABUT. N. & S. STRUCTURES**  
**F.A.I. RT. 80 SEC. 06-7VB**  
**BUREAU COUNTY**  
**STA. 1762+49.41**

Revised 3-27-62.

|   |  |            |           |
|---|--|------------|-----------|
| FILE NAME =                             | USER NAME = braboygo                   | DESIGNED - | REVISED - |
| ca:\pwwork\pwwork\braboygo\0112648\d368 | 988-sht-details.DGN                    | DRAWN -    | REVISED - |
|   | PLOT SCALE = 56.0638' / IN.            | CHECKED -  | REVISED - |
|   | PLOT DATE = Sep 07, 2009 - 01:37:42 PM | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING BRIDGE PLANS (FOR INFORMATION ONLY)

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_ OF \_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

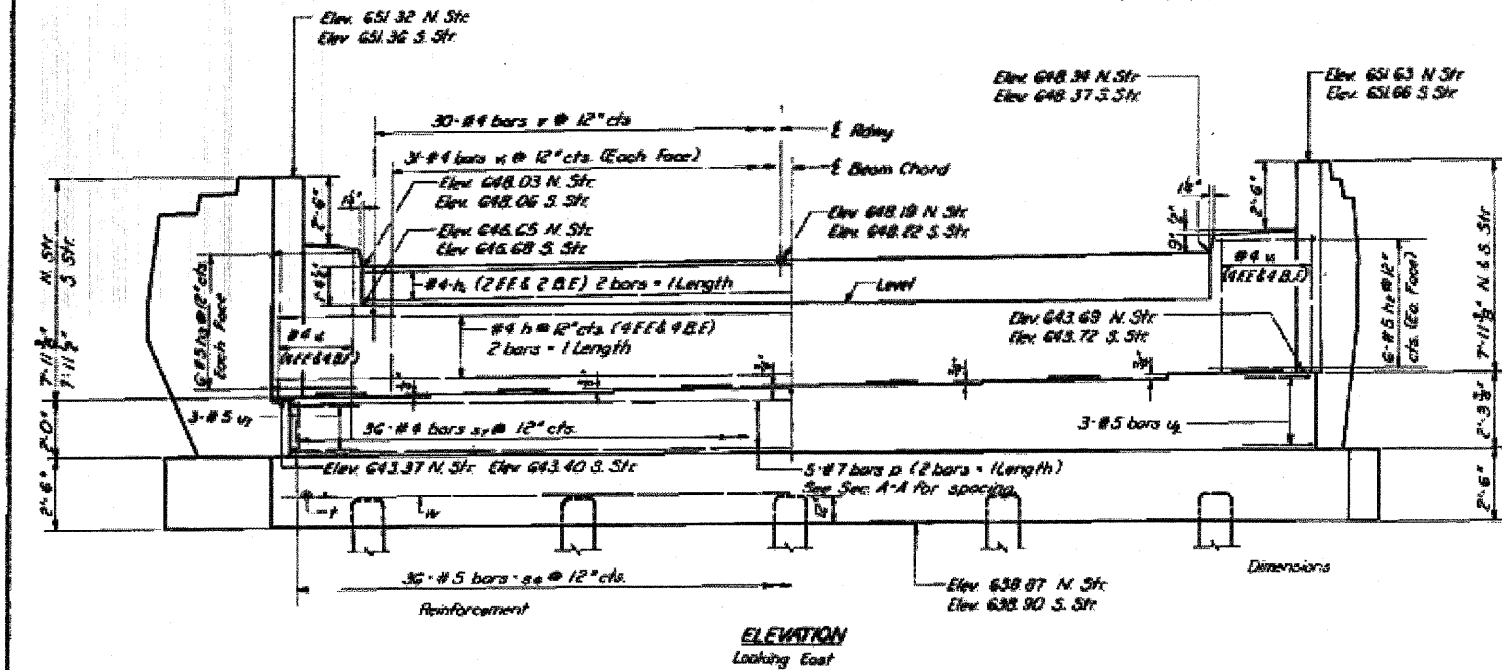
|             |         |        |              |           |
|-------------|---------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-80        | **      | BUREAU | 344          | 284       |
|             |         |        |              | 66908     |

\*\*06-7BR&BR-1.7VB-M.6BR&6.7 RS-1 & I

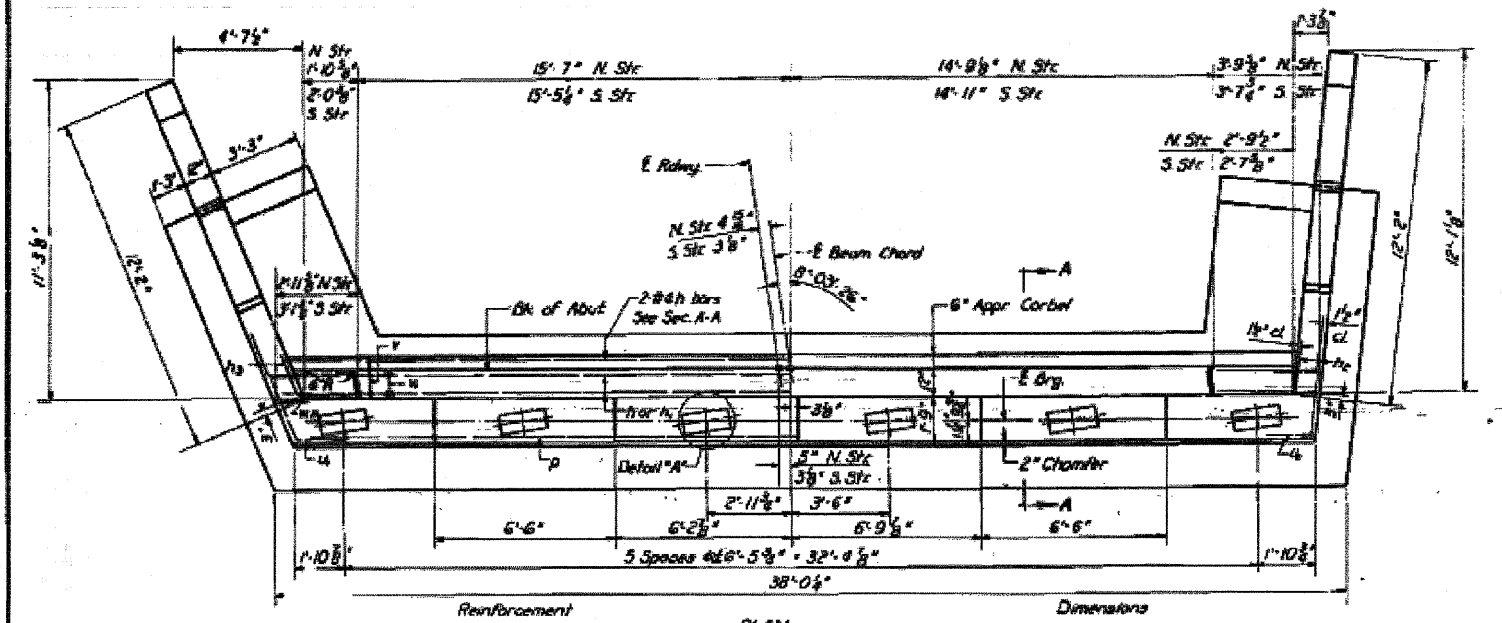
ILLINOIS FED. AID PROJECT

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

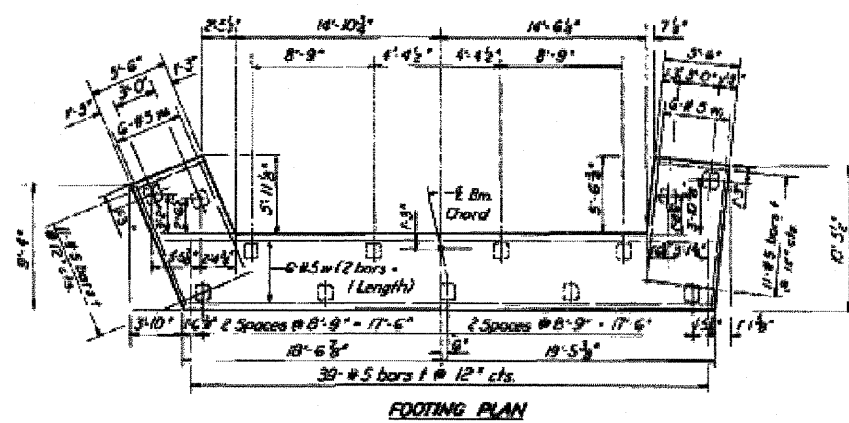
|             |           |           |              |
|-------------|-----------|-----------|--------------|
| PROJECT NO. | DATE      | SHEET NO. | TOTAL SHEETS |
| 10-18-60    | BUREAU 48 | 22        | 45 SHEETS    |



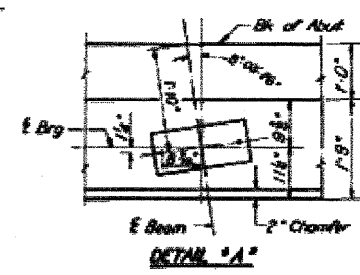
ELEVATION  
Looking East



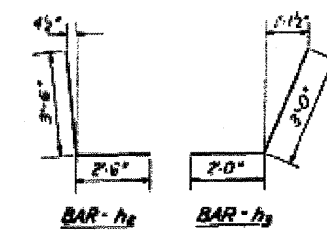
PLAN



FOOTING PLAN



DETAIL A-A



BAR - h2  
BAR - h3

2 ABUTTS  
BAR LIST

| Bar No. | Size | Length | Shape |
|---------|------|--------|-------|
| h       | # 4  | 18'-0" | —     |
| h1      | # 4  | 15'-6" | —     |
| h2      | # 5  | 6'-0"  | J     |
| h3      | # 5  | 5'-0"  | J     |
| p       | # 7  | 18'-6" | —     |
| 5a      | # 5  | 8'-6"  | □     |
| 5r      | # 4  | 6'-2"  | □     |
| f       | # 5  | 5'-3"  | —     |
| u1      | # 5  | 5'-8"  | —     |
| u2      | # 5  | 5'-8"  | —     |
| v       | # 4  | 2'-9"  | □     |
| w       | # 4  | 6'-3"  | —     |
| x       | # 5  | 19'-0" | —     |
| y       | # 5  | 8'-6"  | —     |

TWO ABUTMENTS  
BILL OF MATERIAL

| Item               | Unit   | Quantity |
|--------------------|--------|----------|
| Class II Concrete  | Cu Yds | 89.6     |
| Reinforcement Bars | Lbs    | 6320     |
| Concrete Piles     | Lin Ft | 1000     |
| Test Pile Concrete | Each   | 1        |

Note:  
Bill of Material includes Class II Concrete and Reinforcement Bars for end posts.

PILE DATA  
Type - Concrete Piles  
Capacity - 30 Tons  
Est. Length - 40'-0"  
No. Req'd - 25 = 1 Test Pile (3 Str.)

EAST ABUT N & S STRUCTURES  
E.A.T. RT. 02 SEC. 06-TVB  
BUREAU COUNTY  
STA. 1762 + 49.41

DESIGNED: [Signature]  
CHECKED: [Signature]  
DRAWN: M.T.  
CHECKED: G.R.  
DATE: APR 24 1961  
EXAMINED: [Signature]  
APPROVED: [Signature]

Note:  
For Details of end post and sec A-A see Sheet #7

|   |                      |            |           |
|---|----------------------|------------|-----------|
| FILE NAME =   | USER NAME = braboygo | DESIGNED - | REVISED - |
| ca:\pwwork\pwwork\braboygo\d0112640\d036908-shr-details.dgn |                      | DRAWN -    | REVISED - |
|   |                      | CHECKED -  | REVISED - |
|   |                      | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING BRIDGE PLANS (FOR INFORMATION ONLY)

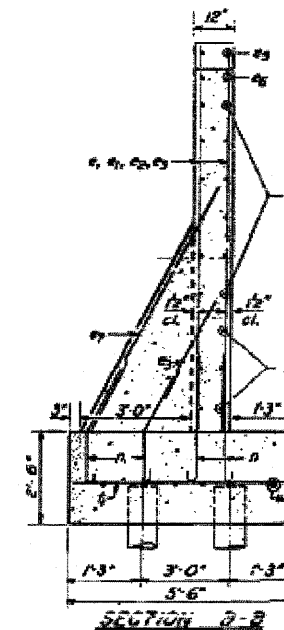
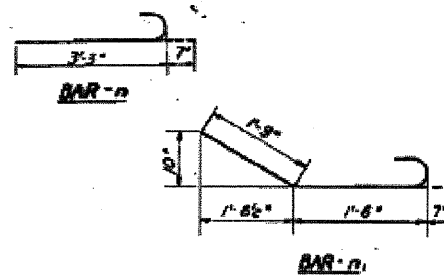
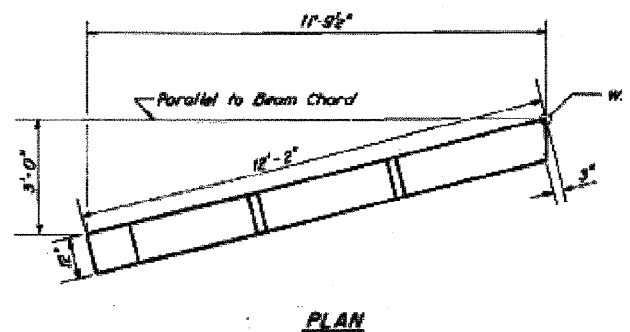
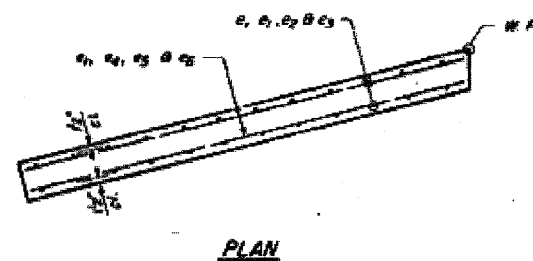
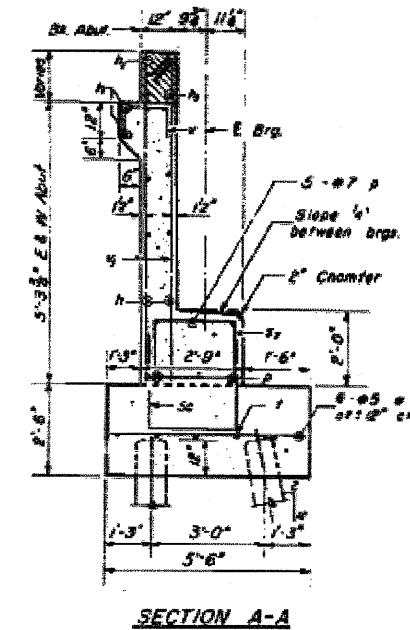
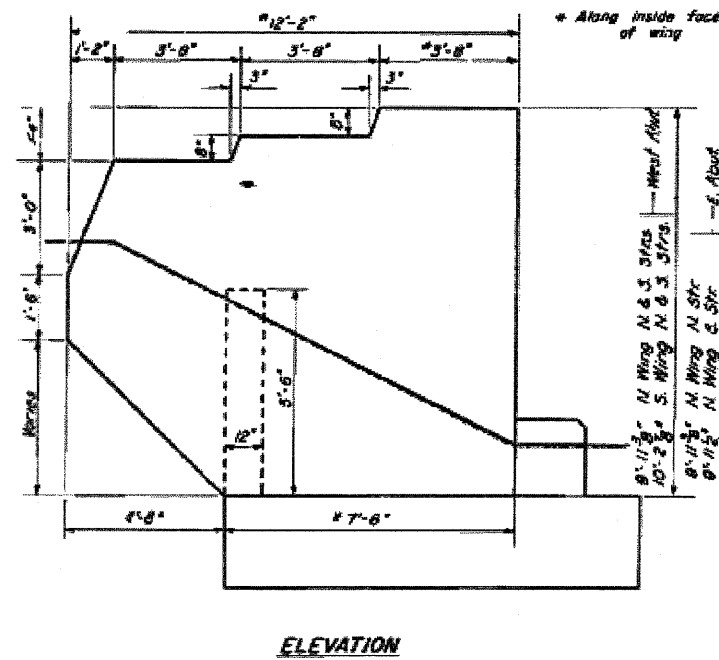
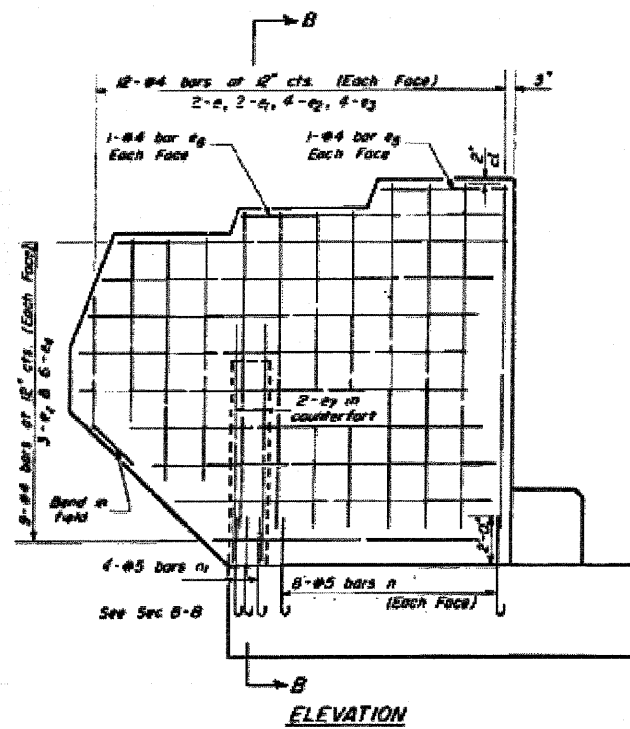
SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_\_ OF \_\_\_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

|                           |            |               |                  |               |
|---------------------------|------------|---------------|------------------|---------------|
| F.A.I. RTE. I-80          | SECTION ** | COUNTY BUREAU | TOTAL SHEETS 344 | SHEET NO. 285 |
|                           |            |               |                  | 66908         |
| ILLINOIS FED. AID PROJECT |            |               |                  |               |

\*\*06-7BR&BR-1,7VB-M,6BR&6,7 RS-1 & I

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|             |        |        |       |           |
|-------------|--------|--------|-------|-----------|
| DATE        | NO.    | BY     | CHKD. | DATE      |
| 1-1-60      | 06-718 | BUREAU | 43    | 23        |
| SHEET NO. 7 |        |        |       | 15 SHEETS |



**ONE END POST  
BILL OF MATERIAL**

| Bar                | No. | Size | Length | Shape        |
|--------------------|-----|------|--------|--------------|
| c                  | 4   | #4   | 5'-0"  | ---          |
| d                  | 6   | #4   | 7'-0"  | ---          |
| e                  | 8   | #4   | 8'-6"  | ---          |
| f                  | 8   | #4   | 9'-3"  | ---          |
| g                  | 12  | #4   | 10'-0" | ---          |
| h                  | 2   | #4   | 3'-3"  | ---          |
| i                  | 2   | #4   | 6'-0"  | ---          |
| j                  | 2   | #5   | 7'-0"  | ---          |
| n                  | 16  | #5   | 9'-10" | ---          |
| nr                 | 4   | #3   | 3'-10" | ---          |
| Class X Concrete   |     |      |        | Cu. Yds. 4.4 |
| Reinforcement Bars |     |      |        | Lbs. 333     |

Note:  
Quantities are included in Bill of  
Material Sheets #5 & #6

**ABUT WING DETAILS  
N. AND S. STRUCTURES  
F.A.I. RT. 60 SEC. 06-718  
BUREAU COUNTY  
STA. 1762+19.91**

DESIGNED *M. P. ...*  
CHECKED *...*  
DRAWN *W. A. Sausman*  
APPROVED *...*

AW-1 Drawn 1-28-60

|  |  |            |           |
|--|--|------------|-----------|
| FILE NAME =                              | USER NAME = braboyco                   | DESIGNED - | REVISED - |
| ca:\pwwork\pwwork\braboyco\d0112648\d368 | 908-sht-details.DGN                    | DRAWN -    | REVISED - |
|  | PLOT SCALE = 50.0638' / IN.            | CHECKED -  | REVISED - |
|  | PLOT DATE = Sep 07, 2009 - 01:37:18 PM | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING BRIDGE PLANS (FOR INFORMATION ONLY)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

|             |         |        |              |           |
|-------------|---------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-80        | **      | BUREAU | 344          | 286       |
|             |         |        |              | 66908     |

\*\*06-718&BR-1.7VB-M.6BR&6.7 RS-1 & I

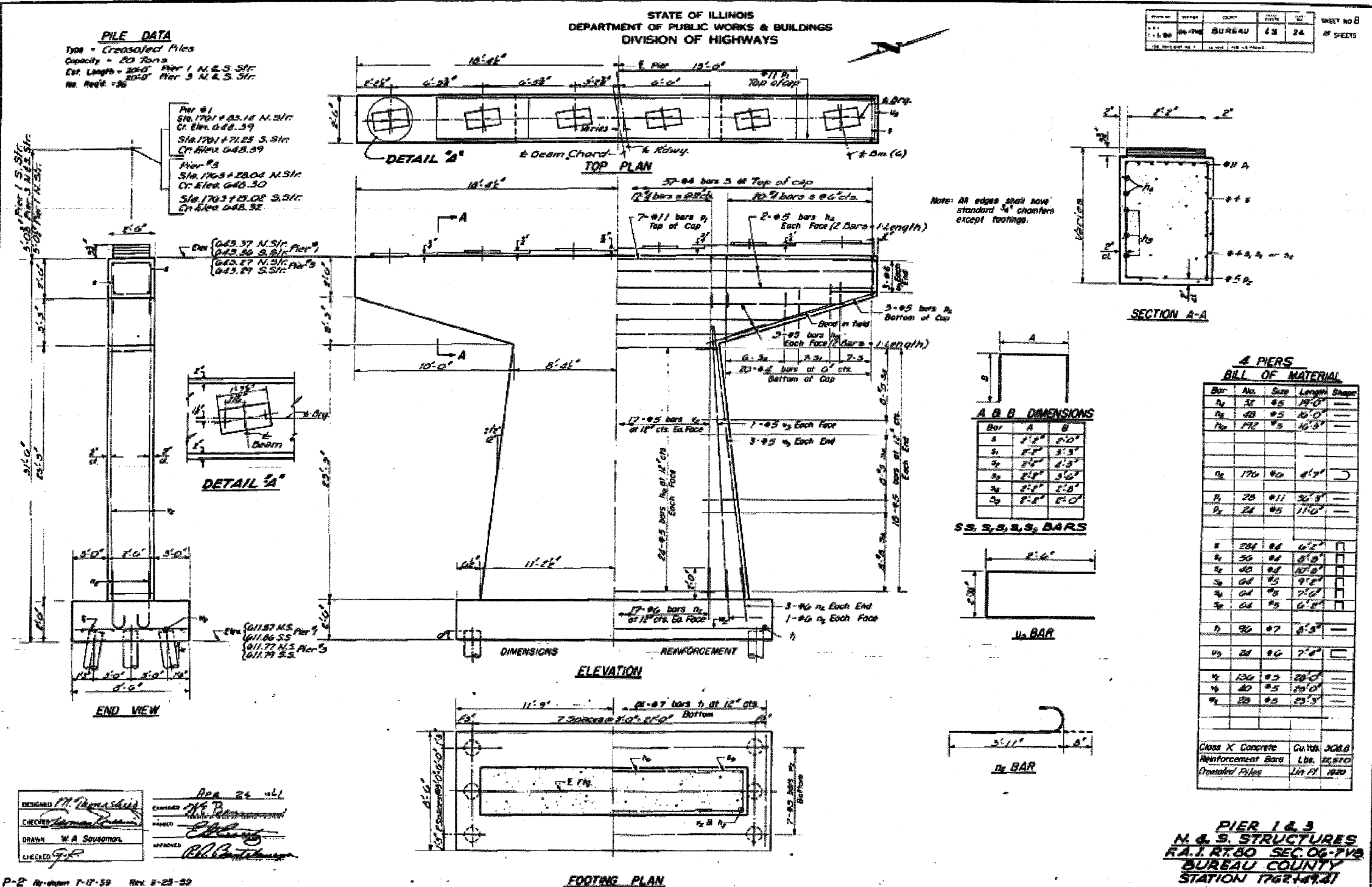
ILLINOIS FED. AID PROJECT

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|             |      |        |           |              |
|-------------|------|--------|-----------|--------------|
| PROJECT NO. | DATE | COUNTY | SHEET NO. | TOTAL SHEETS |
| 06-746      | 1929 | BUREAU | 43        | 24           |

**PILE DATA**  
Type - *Crossed Piles*  
Capacity - 20 Tons  
Est. Length - 20' Pier 1 N. & S. Str.  
No. Reqd. - 36

Pier #1  
Sta. 1701+23.16 N.S. Str.  
Ct. Elev. 648.39  
Sta. 1701+71.25 S.S. Str.  
Ct. Elev. 648.39  
Pier #3  
Sta. 1703+28.04 N.S. Str.  
Ct. Elev. 648.30  
Sta. 1703+13.02 S.S. Str.  
Ct. Elev. 648.32



DESIGNED: M. J. [Signature]  
CHECKED: [Signature]  
DRAWN: W. A. [Signature]  
CHECKED: [Signature]

DATE: Aug 24 '21

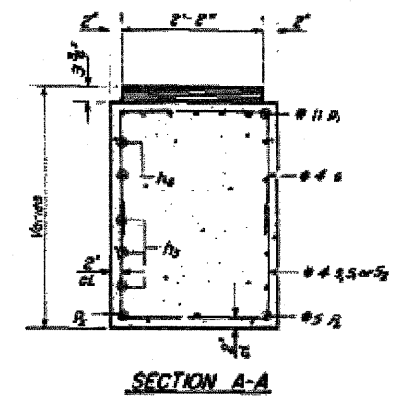
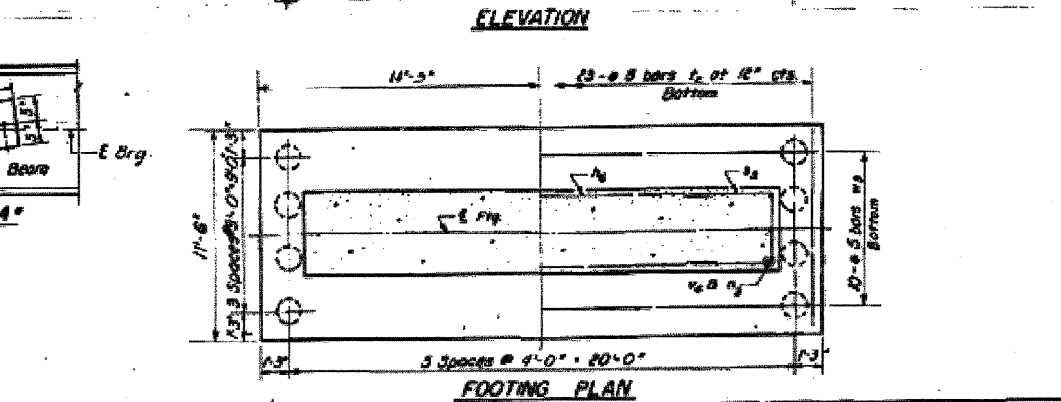
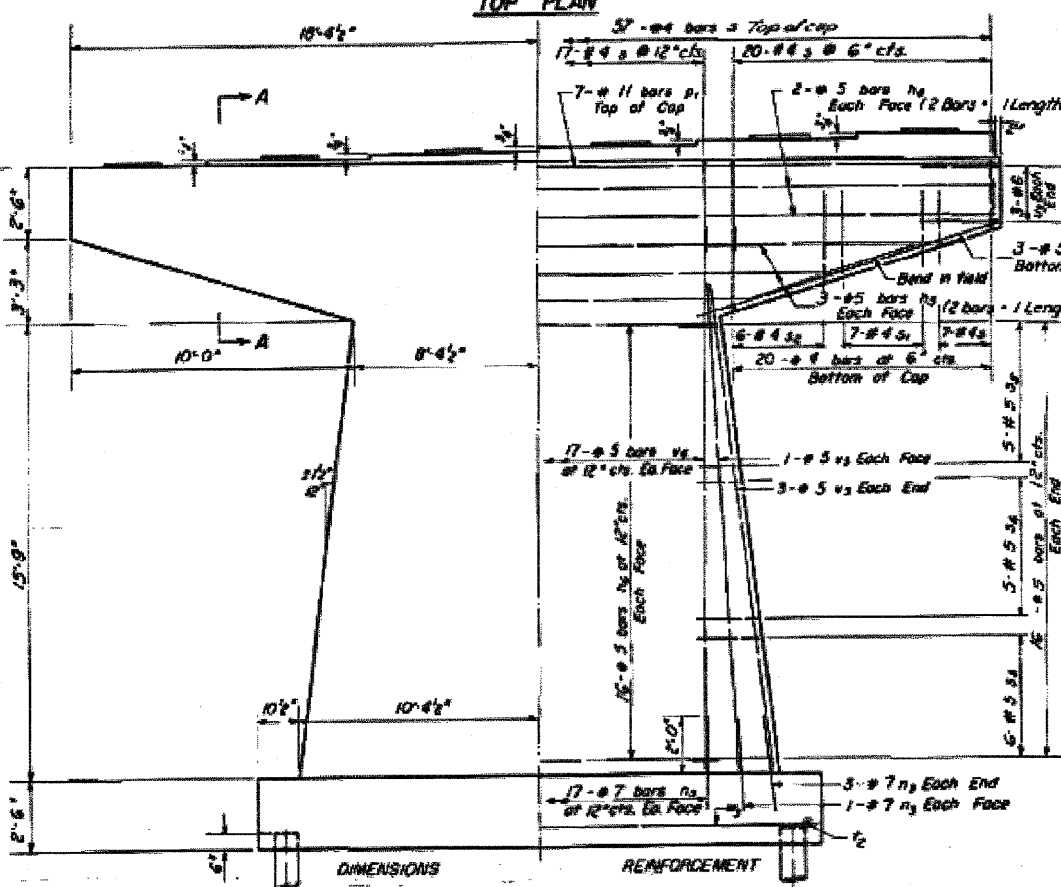
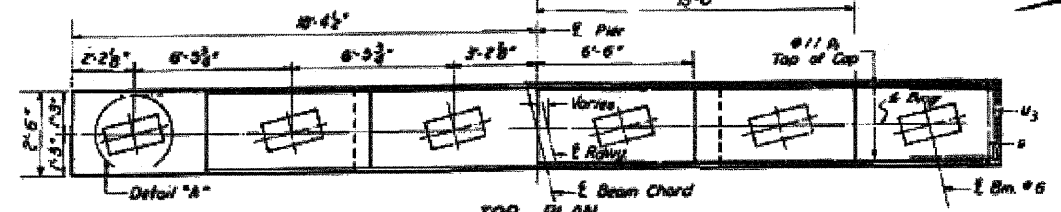
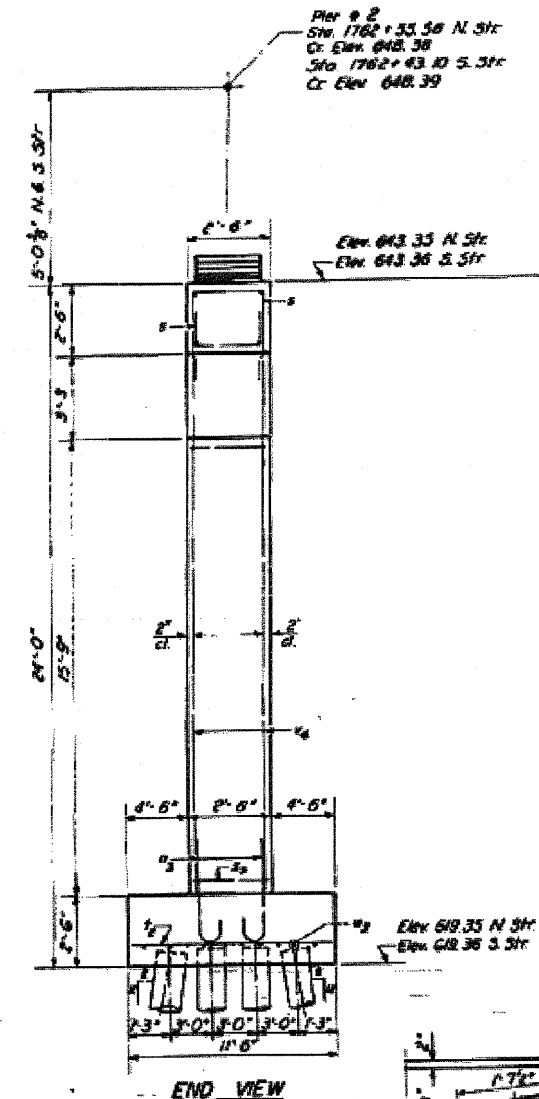
P-2 Revisions 7-17-29 Rev. 8-25-29

**PIER 1 & 3  
N. & S. STRUCTURES  
R.A.I. RT. 80 SEC. 06-746  
BUREAU COUNTY  
STATION 1702+49.41**

STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

|             |              |           |              |
|-------------|--------------|-----------|--------------|
| PROJECT NO. | DATE         | SHEET NO. | TOTAL SHEETS |
| 10-770      | BUREAU 43 25 | 11        | 11           |

**PILE DATA**  
Type - Crossed Piles  
Capacity - 20 Tons  
Est. Length 23'-0"  
No. Req'd. 18 + 1 Test Pile 5 Str.

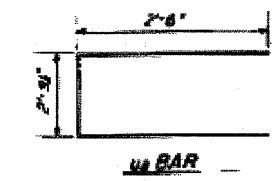


Note: All edges shall have standard 3/8" chamfers except footings.

**A B B DIMENSIONS**

| Bar | A     | B     |
|-----|-------|-------|
| 1   | 2'-2" | 2'-0" |
| 2   | 2'-2" | 3'-3" |
| 3   | 2'-2" | 4'-3" |
| 4   | 2'-2" | 5'-6" |
| 5   | 2'-2" | 7'-0" |

1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100



**PIERS  
BILL OF MATERIAL**

| Bar No. | Size | Length | Shape |
|---------|------|--------|-------|
| 1       | #5   | 19'-0" | —     |
| 2       | #5   | 18'-0" | —     |
| 3       | #5   | 18'-3" | —     |
| 4       | #7   | 6'-9"  | —     |
| 5       | #11  | 38'-3" | —     |
| 6       | #5   | 11'-6" | —     |
| 7       | #4   | 8'-2"  | □     |
| 8       | #4   | 8'-8"  | □     |
| 9       | #4   | 10'-8" | □     |
| 10      | #5   | 8'-2"  | □     |
| 11      | #5   | 7'-6"  | □     |
| 12      | #5   | 6'-2"  | □     |
| 13      | #8   | 11'-3" | —     |
| 14      | #6   | 7'-4"  | □     |
| 15      | #5   | 20'-3" | —     |
| 16      | #5   | 17'-6" | —     |
| 17      | #5   | 20'-3" | —     |
| 18      | #5   | 20'-3" | —     |
| 19      | #5   | 20'-3" | —     |
| 20      | #5   | 20'-3" | —     |
| 21      | #5   | 20'-3" | —     |
| 22      | #5   | 20'-3" | —     |
| 23      | #5   | 20'-3" | —     |
| 24      | #5   | 20'-3" | —     |
| 25      | #5   | 20'-3" | —     |
| 26      | #5   | 20'-3" | —     |
| 27      | #5   | 20'-3" | —     |
| 28      | #5   | 20'-3" | —     |
| 29      | #5   | 20'-3" | —     |
| 30      | #5   | 20'-3" | —     |
| 31      | #5   | 20'-3" | —     |
| 32      | #5   | 20'-3" | —     |
| 33      | #5   | 20'-3" | —     |
| 34      | #5   | 20'-3" | —     |
| 35      | #5   | 20'-3" | —     |
| 36      | #5   | 20'-3" | —     |
| 37      | #5   | 20'-3" | —     |
| 38      | #5   | 20'-3" | —     |
| 39      | #5   | 20'-3" | —     |
| 40      | #5   | 20'-3" | —     |
| 41      | #5   | 20'-3" | —     |
| 42      | #5   | 20'-3" | —     |
| 43      | #5   | 20'-3" | —     |
| 44      | #5   | 20'-3" | —     |
| 45      | #5   | 20'-3" | —     |
| 46      | #5   | 20'-3" | —     |
| 47      | #5   | 20'-3" | —     |
| 48      | #5   | 20'-3" | —     |
| 49      | #5   | 20'-3" | —     |
| 50      | #5   | 20'-3" | —     |
| 51      | #5   | 20'-3" | —     |
| 52      | #5   | 20'-3" | —     |
| 53      | #5   | 20'-3" | —     |
| 54      | #5   | 20'-3" | —     |
| 55      | #5   | 20'-3" | —     |
| 56      | #5   | 20'-3" | —     |
| 57      | #5   | 20'-3" | —     |
| 58      | #5   | 20'-3" | —     |
| 59      | #5   | 20'-3" | —     |
| 60      | #5   | 20'-3" | —     |
| 61      | #5   | 20'-3" | —     |
| 62      | #5   | 20'-3" | —     |
| 63      | #5   | 20'-3" | —     |
| 64      | #5   | 20'-3" | —     |
| 65      | #5   | 20'-3" | —     |
| 66      | #5   | 20'-3" | —     |
| 67      | #5   | 20'-3" | —     |
| 68      | #5   | 20'-3" | —     |
| 69      | #5   | 20'-3" | —     |
| 70      | #5   | 20'-3" | —     |
| 71      | #5   | 20'-3" | —     |
| 72      | #5   | 20'-3" | —     |
| 73      | #5   | 20'-3" | —     |
| 74      | #5   | 20'-3" | —     |
| 75      | #5   | 20'-3" | —     |
| 76      | #5   | 20'-3" | —     |
| 77      | #5   | 20'-3" | —     |
| 78      | #5   | 20'-3" | —     |
| 79      | #5   | 20'-3" | —     |
| 80      | #5   | 20'-3" | —     |
| 81      | #5   | 20'-3" | —     |
| 82      | #5   | 20'-3" | —     |
| 83      | #5   | 20'-3" | —     |
| 84      | #5   | 20'-3" | —     |
| 85      | #5   | 20'-3" | —     |
| 86      | #5   | 20'-3" | —     |
| 87      | #5   | 20'-3" | —     |
| 88      | #5   | 20'-3" | —     |
| 89      | #5   | 20'-3" | —     |
| 90      | #5   | 20'-3" | —     |
| 91      | #5   | 20'-3" | —     |
| 92      | #5   | 20'-3" | —     |
| 93      | #5   | 20'-3" | —     |
| 94      | #5   | 20'-3" | —     |
| 95      | #5   | 20'-3" | —     |
| 96      | #5   | 20'-3" | —     |
| 97      | #5   | 20'-3" | —     |
| 98      | #5   | 20'-3" | —     |
| 99      | #5   | 20'-3" | —     |
| 100     | #5   | 20'-3" | —     |

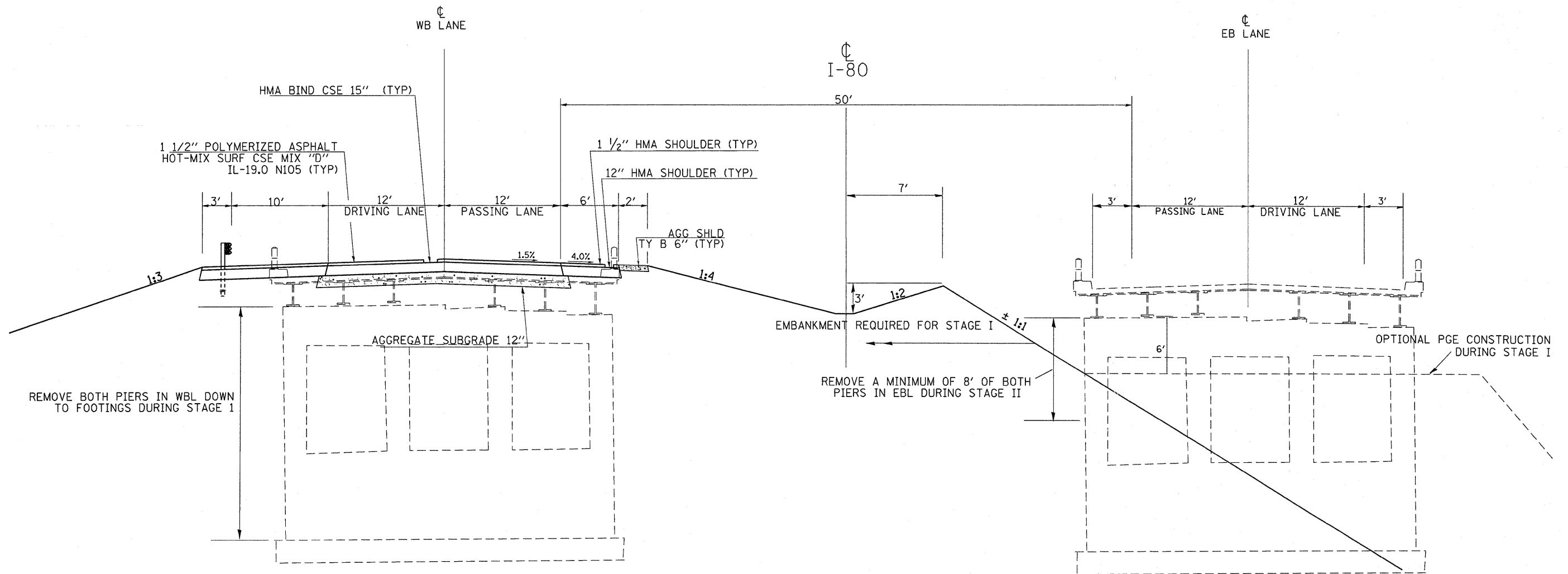
**PIER #2 N & S STRUCTURES  
E.I. RT. 60 SEC. 06-TVB  
BUREAU COUNTY  
STA. 1762 + 49.41**

DESIGNED: M. P. ...  
CHECKED: ...  
DRAWN: M. A. ...  
CHECKED: ...  
DATE: Dec 24 1961

P-2 Re-drawn 7-17-59 Rev. 11-25-59



NOTE: ALL ABUTMENTS TO BE REMOVED 1' BELOW EXISTING ABUTMENT ELEVATIONS

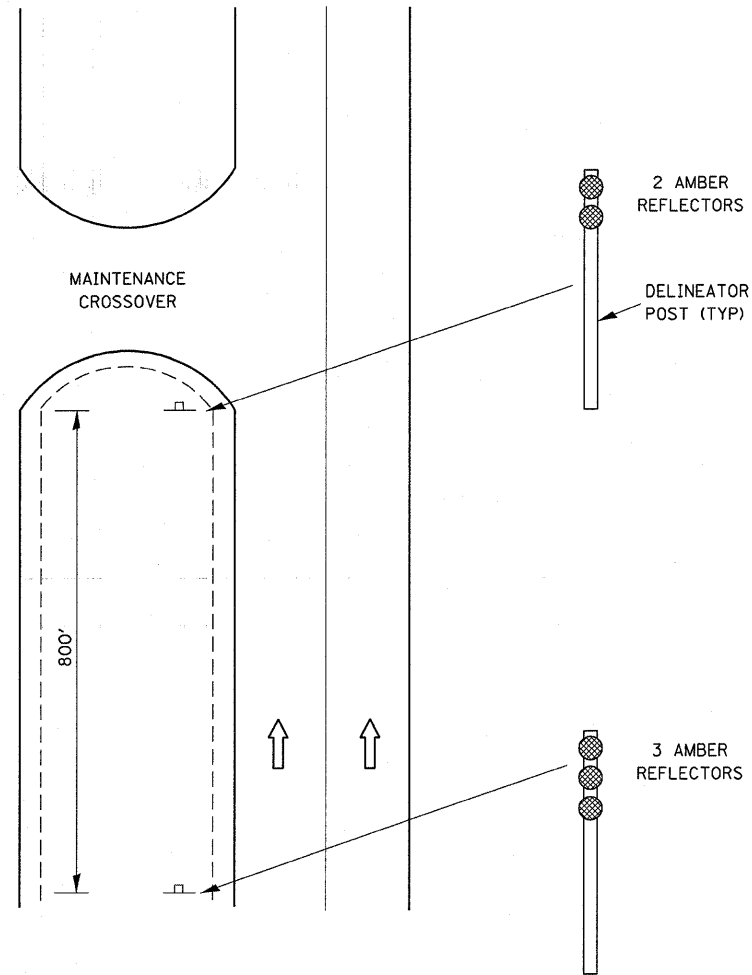


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|  |  | CHECKED -  | REVISED - |
|  |  | DATE -     | REVISED - |
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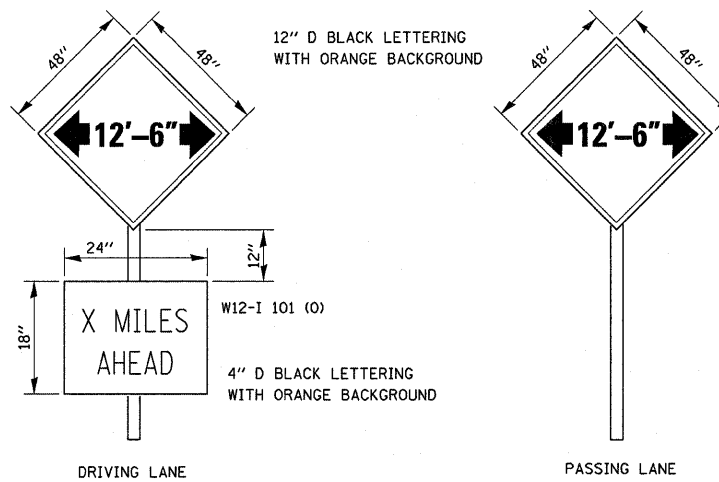
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

|              |                               |            |               |
|--------------|-------------------------------|------------|---------------|
| DETAILS      |                               |            |               |
| SCALE: _____ | SHEET NO. ____ OF ____ SHEETS | STA. _____ | TO STA. _____ |

|                                       |            |               |                    |               |
|---------------------------------------|------------|---------------|--------------------|---------------|
| **06-[7BR&BR-1,7VB-M,6BR&6,7 RS-1 & I |            |               |                    |               |
| F.A. RTE. I-80                        | SECTION ** | COUNTY BUREAU | TOTAL SHEETS 344   | SHEET NO. 289 |
|                                       |            |               | CONTRACT NO. 66908 |               |
| ILLINOIS FED. AID PROJECT             |            |               |                    |               |



**DELINEATION FOR MAINTENANCE CROSSOVER (TYPICAL FOR BOTH DIRECTIONS)**



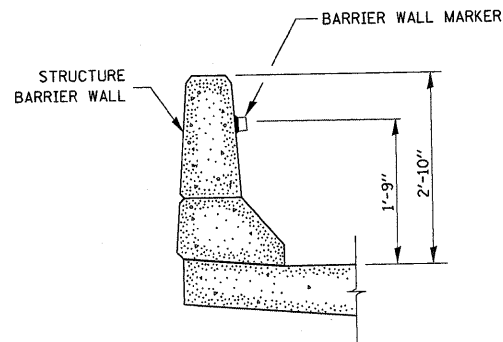
TO BE POST MOUNTED, FOR STAGE 1 ONLY. 10 NEEDED-6 NEEDED FOR WB TRAFFIC (TWO 1/2 MILE EAST OF I-39, TWO 1/2 MILE EAST OF IL 251, AND TWO 1/2 MILE EAST OF PLANK ROAD). AND 4 FOR EB TRAFFIC (TWO 1/2 MILE WEST OF IL 89, AND TWO 1/2 MILE WEST OF I-180)

THE ENGINEER WILL NOTIFY DISTRICT 3 BUREAU OF OPERATIONS 14 CALENDAR DAYS PRIOR TO INSTALLING ANY TRAFFIC CONTROL DEVICES THAT WILL RESTRICT THE PAVEMENT WIDTH.

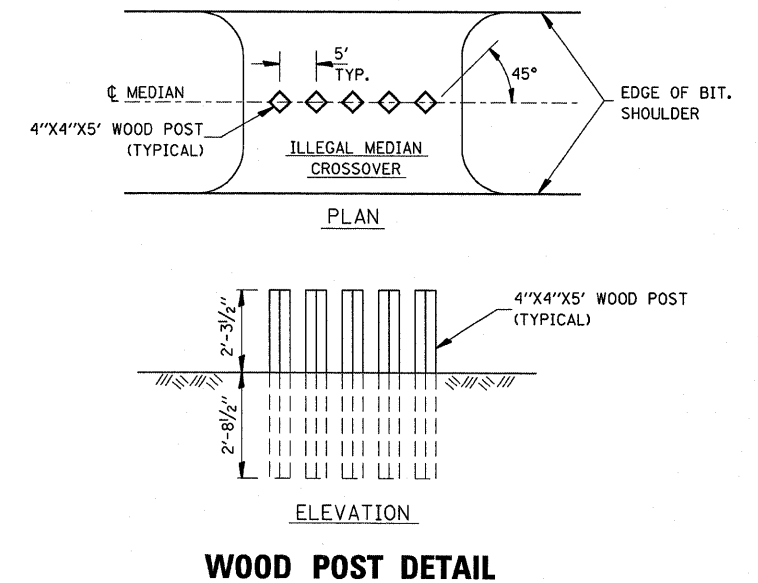
THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE ENGINEER TO MEET THIS REQUIREMENT.

COST OF SUPPLYING, INSTALLING, MAINTAINING AND REMOVING WIDTH RESTRICTION SIGNS SHALL BE PAID FOR AS WIDTH RESTRICTION SIGNING.

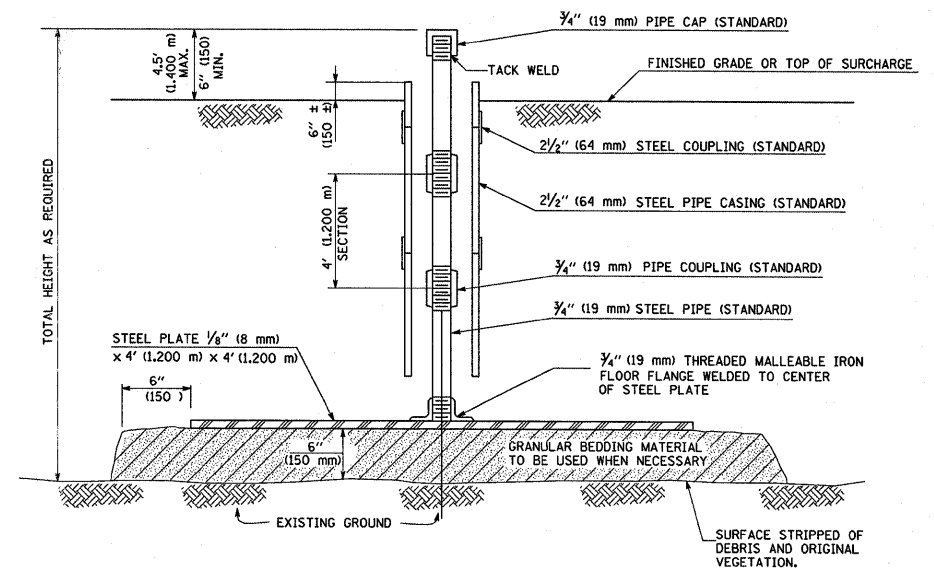
**WIDTH RESTRICTION SIGNING DETAILS**



**BARRIER WALL MARKER**



**WOOD POST DETAIL**



**SETTLEMENT PLATFORM DETAIL**

1. SEE SECTION 204.06 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION  
 2. TWO SETTLEMENT PLATFORMS SHALL BE CONSTRUCTED AT THE SITE OF THE ABANDONED RAILROAD STRUCTURES - ONE FOR STAGE I AND ONE FOR STAGE II CONSTRUCTION. THE COST IS INCLUDED IN THE COST OF FURNISHED EXCAVATION

|  |                      |            |           |
|--|----------------------|------------|-----------|
| FILE NAME =  | USER NAME = braboygo | DESIGNED - | REVISED - |
| ca:\piv_work\pivdot\braboygo\0112640\0368908-sht-details.DGN |                      | DRAWN -    | REVISED - |
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| PLOT DATE = Sep 07, 2009 - 01:36:32 PM                       |                      | DATE -     | REVISED - |

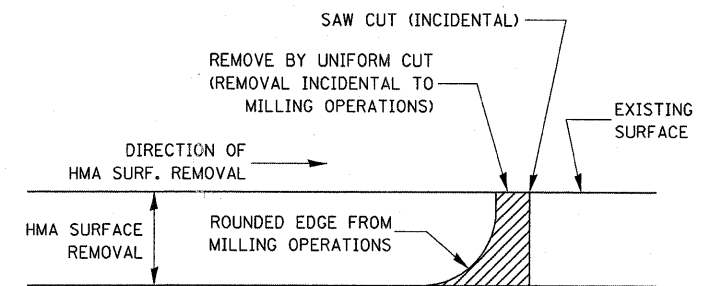
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS**

SCALE: \_\_\_\_\_ SHEET NO. \_\_\_ OF \_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

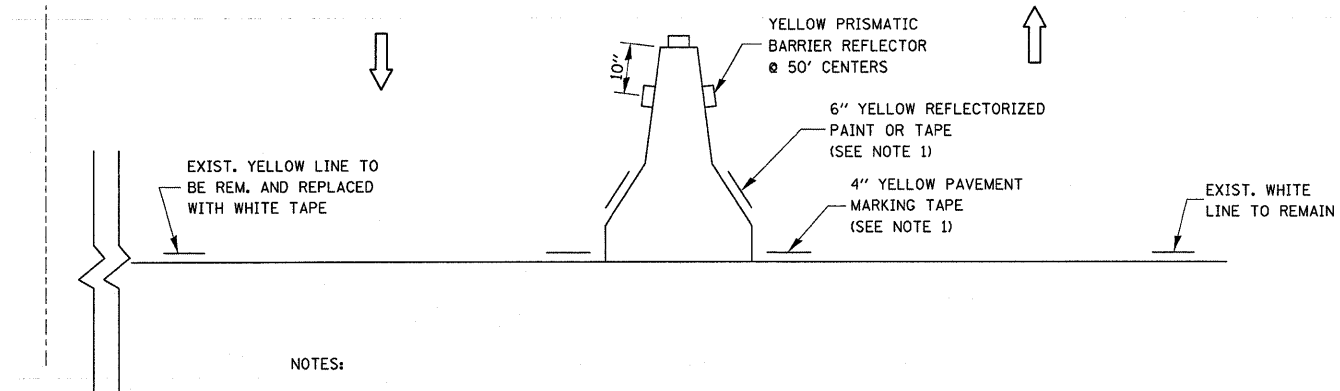
**\*\*06-[7BR&BR-1,7VB-M,6BR&6,7 RS-1 & I**

|                           |         |        |                    |           |
|---------------------------|---------|--------|--------------------|-----------|
| F.A. RTE. =               | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| I-80                      | **      | BUREAU | 344                | 290       |
|                           |         |        | CONTRACT NO. 66908 |           |
| ILLINOIS FED. AID PROJECT |         |        |                    |           |

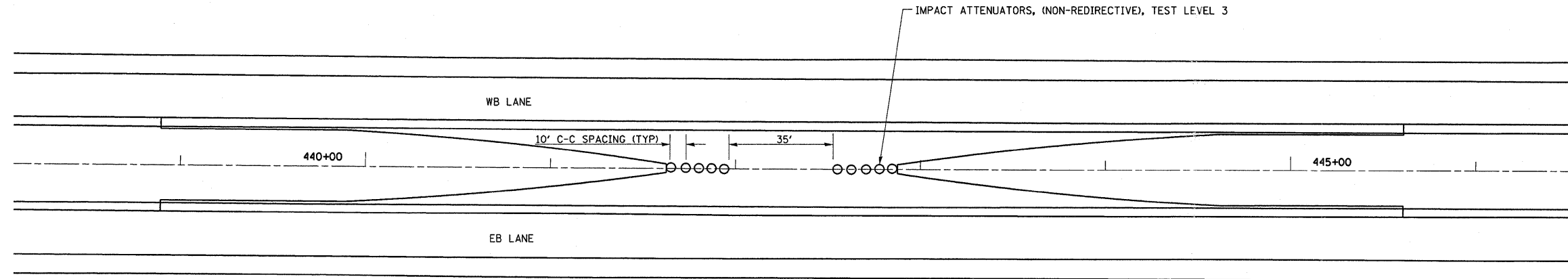


NOTE:  
 WHEN MILLING OPERATIONS PRODUCE A ROUNDED EDGE, THEN A SAW CUT SHALL BE USED TO MANUFACTURE A PERPENDICULAR EDGE AS SHOWN IN THE DETAIL. THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE USE OF THIS DETAIL.

**HMA DETAIL AT BUTT JOINTS**



- NOTES:
1. THE CONTRACTOR HAS THE OPTION OF USING EITHER THE LINE ON THE TEMPORARY CONCRETE BARRIER OR ON THE PAVEMENT.
  2. THE COST OF THE REFLECTORS IS INCLUDED IN THE COST OF THE TEMPORARY CONCRETE BARRIER.



**MEDIAN CROSSOVER IMPACT ATTENUATORS**

NOTE: IMPACT ATTENUATORS SHALL BE PLACED WHEN CROSSOVER CONSTRUCTION IS COMPLETE, AND SHALL BECOME THE PROPERTY OF THE STATE.

IMPACT ATTENUATORS TO BE PLACED AFTER STAGE II IS COMPLETE.

THIS WORK SHALL BE PAID FOR AS THE CONTRACT UNIT PRICES EACH AS IMPACT ATTENUATORS, (NON-REDIRECTIVE), TEST LEVEL 3

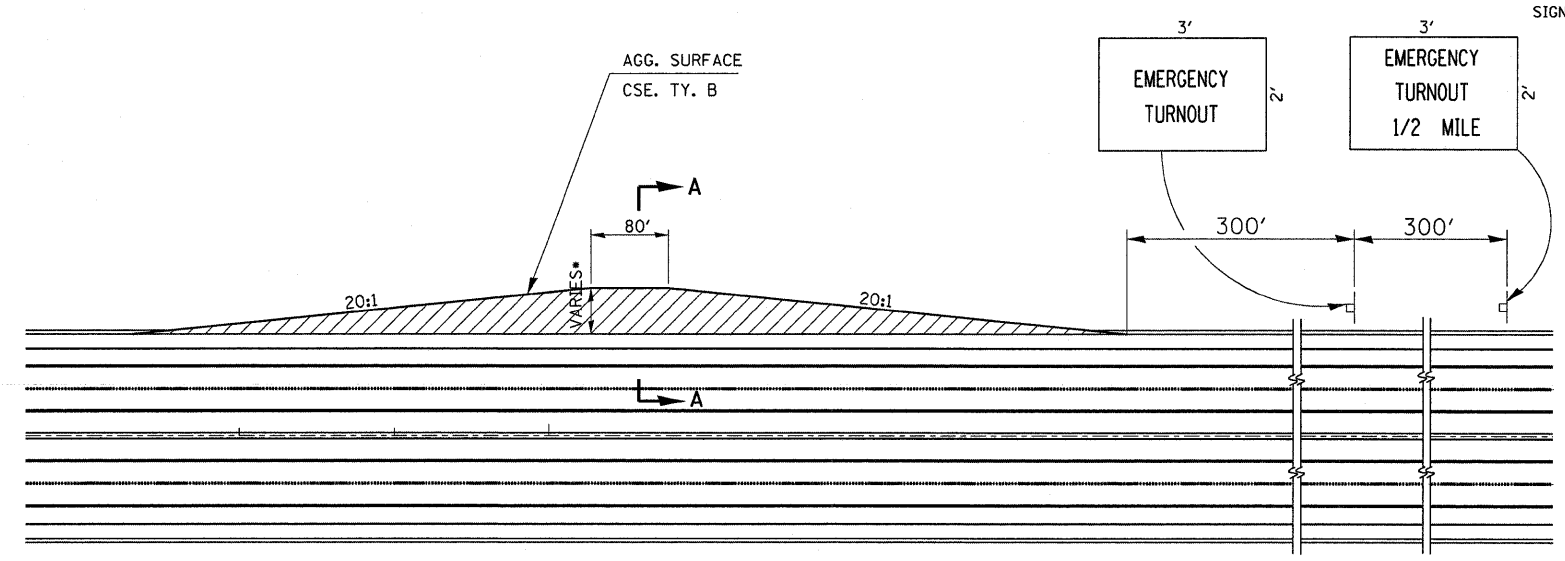
WESTERN MEDIAN CROSSOVER IS THE SAME AS THE ABOVE DETAIL. STA 342+00 TO 350+00

|  |                      |            |   |   |                |                    |         |                           |              |           |
|--|----------------------|------------|---|---|----------------|--------------------|---------|---------------------------|--------------|-----------|
| FILE NAME =  | USER NAME = braboypc | DESIGNED - | REVISED -   | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>DETAILS</b> | F.A. RTE. =        | SECTION | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| ct:\pw\work\pwsidot\braboypc\d0112640\d368908-sht-detail.s.DGN | DRAWN -              | REVISED -  | I-80  |   |                | **                 | BUREAU  | 344                       | 291          |           |
| PLOT SCALE = 50.0638' / IN.                                    | CHECKED -            | REVISED -  | SCALE: _____ SHEET NO. ___ OF ___ SHEETS STA. _____ TO STA. _____ |   |                | CONTRACT NO. 66908 |         | ILLINOIS FED. AID PROJECT |              |           |
| PLOT DATE = Sep 07, 2009 - 01:36:09 PM                         | DATE -               | REVISED -  |   |   |                |                    |         |                           |              |           |

\*\*06-[7BR&BR-1,7VB-M,6BR&6.7 RS-1 & I



•DISTANCE IS 6' FROM EOS ON OUTSIDE  
AND 8' ON MEDIAN SIDE



SIGNS ARE BLACK ON WHITE

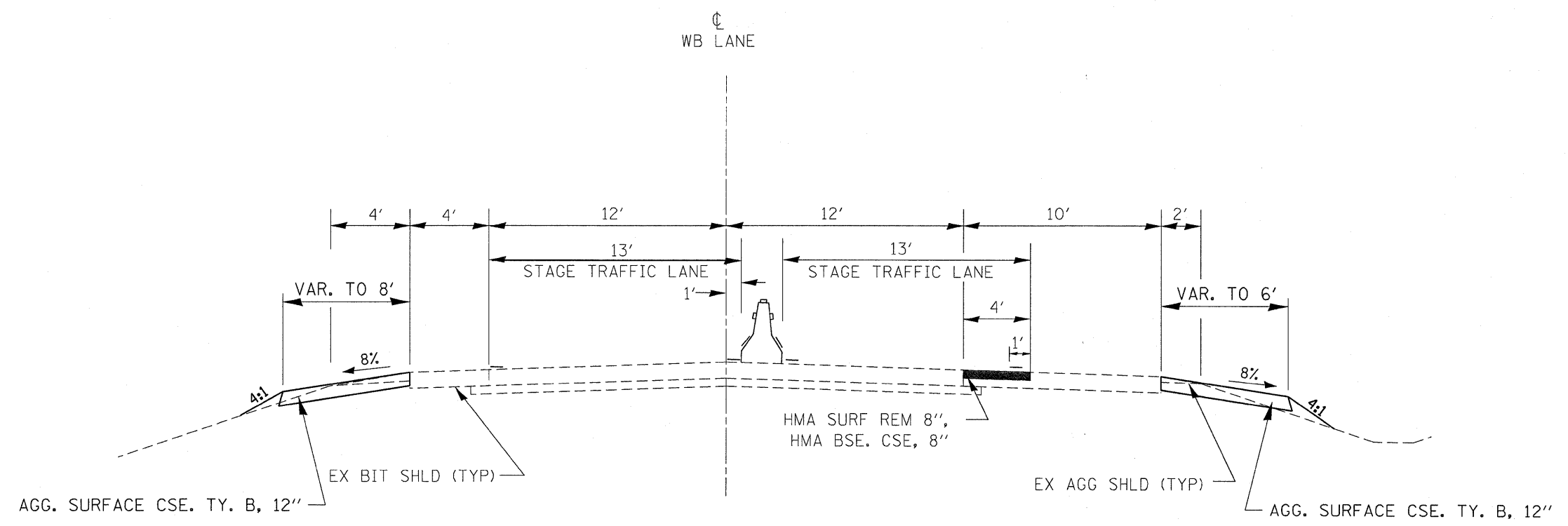
\*\* APPROXIMATE LOCATIONS ARE AS FOLLOWS:  
STA 1538+00 EB & WE/ STAGE I & STAGE II  
STA 1680+00 EB & WB/ STAGE I & STAGE II

NOTES:

- 1) THE CONTRACTOR SHALL PROVIDE FOR TEMPORARY EMERGENCY TURNOUTS AS SHOWN FOR BOTH EAST & WEST BOUND TRAFFIC DURING STAGES I AND II.
- 2) THERE WILL BE A TOTAL OF EIGHT EMERGENCY TURNOUTS THAT WILL BE CONSTRUCTED: 2 PER LANE PER STAGE. THE TURNOUTS SHALL BE SPACED AT NO LESS THAN 2.5 MILES CTS. EXACT LOCATIONS SHALL BE APPROVED BY THE ENGINEER. THE ENGINEER RESERVES THE RIGHT TO SELECT ALTERNATIVE LOCATIONS.\*\*
- 3) ALL LABOR, MATERIALS, AND EQUIPMENT REQUIRED TO CONSTRUCT, MAINTAIN AND REMOVE THE TEMPORARY EMERGENCY TURNOUTS AND SIGNING AS SHOWN SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR AGGREGATE SURFACE COURSE, TY. B.

### TEMPORARY EMERGENCY TURNOUTS

WESTBOUND OUTSIDE LANE SHOWN-EASTBOUND AND MEDIAN SIDES SIMILAR-SEE SECTION A-A



### SECTION A-A LOOKING DOWNSTATION

APPROXIMATE LOCATIONS ARE AS FOLLOWS:  
STA 1538+00 EB & WE/ STAGE I & STAGE II  
STA 1680+00 EB & WB/ STAGE I & STAGE II

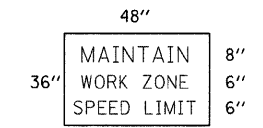
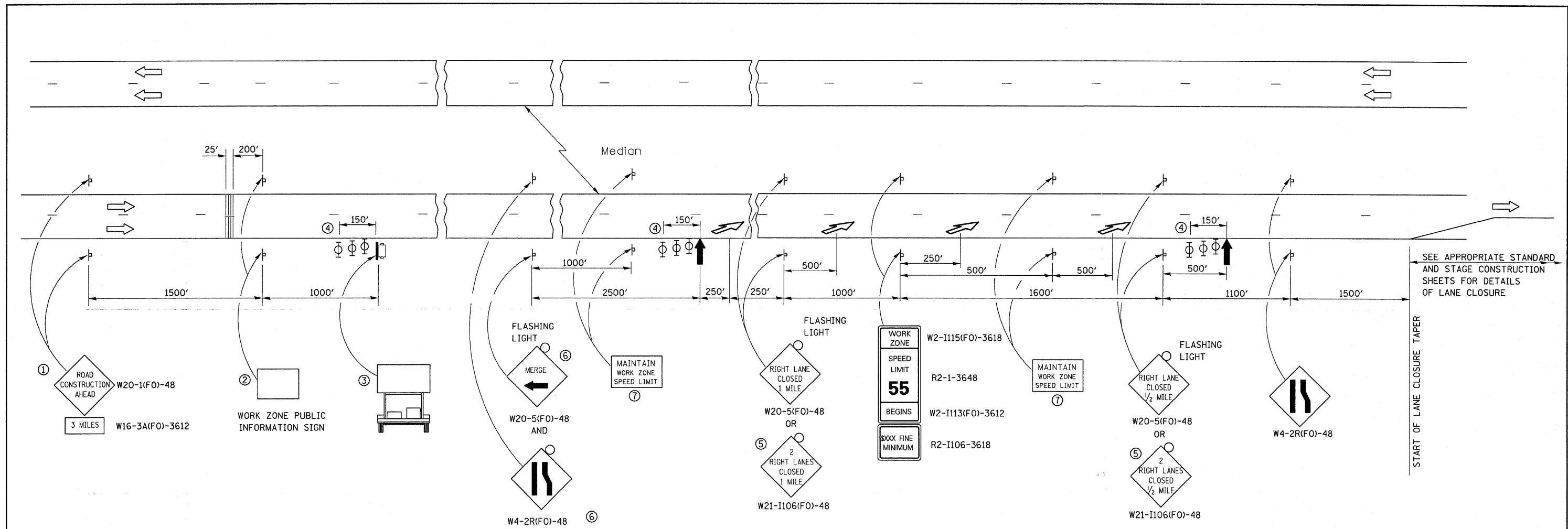
\*\*06-[7BR&BR-1,7VB-M,6BR&6,7 RS-1 & I

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| PLOT DATE = Sep 07, 2009 - 01:36:00 PM                    |                      | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                          |                             |
|--------------------------|-----------------------------|
| <b>DETAILS</b>           |                             |
| SCALE: _____             | SHEET NO. ___ OF ___ SHEETS |
| STA. _____ TO STA. _____ |                             |

|                    |            |               |                           |               |
|--------------------|------------|---------------|---------------------------|---------------|
| F.A. RTE. I-80     | SECTION ** | COUNTY BUREAU | TOTAL SHEETS 344          | SHEET NO. 292 |
| CONTRACT NO. 66908 |            |               | ILLINOIS FED. AID PROJECT |               |



- ⑦ 48"x36" FLUORESCENT ORANGE SIGN WITH BLACK LETTERS.
- ↑ ARROW BOARD
- ☐ PORTABLE CHANGEABLE MESSAGE SIGN
- ⊥ SIGN
- ⊕ TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH MONODIRECTIONAL FLASHING LIGHT
- ↘ LANE DROP ARROW - SEE STANDARD 780001
- ▨ TEMPORARY THERMOPLASTIC RUMBLE STRIPS

- ① THE ROAD CONSTRUCTION AHEAD SIGN SHALL BE LOCATED 3 MILES IN ADVANCE OF THE PROJECT LIMITS.
- ② THE MESSAGE AND SIZE OF THE WORK ZONE PUBLIC INFORMATION SIGN SHALL BE AS SPECIFIED BY THE DEPARTMENT.
- ③ TO BE PLACED IN THE MEDIAN WHEN FEASIBLE. THE MESSAGE BOARD SHALL BE USED TO DISPLAY STATUS OF LANES WITHIN THE PROJECT. THE PRIMARY MESSAGES SHALL BE:  
"RIGHT LANE CLOSED" / " x MILES AHEAD"  
"LEFT LANE CLOSED" / " x MILES AHEAD"  
"ALL LANES OPEN"
- ④ THREE, TYPE II BARRICADES, DRUMS, OR VERTICAL BARRICADES AT 50' CENTERS.
- ⑤ THIS SIGN SHALL BE USED WHEN 2 LANES ARE CLOSED.
- ⑥ WHEN THE LEFT LANE IS CLOSED, SWITCH THESE TWO SIGNS AND THE DIRECTION OF THE MERGE ARROW.

**GENERAL NOTE:**

THIS STANDARD IS USED WHERE AT ANY TIME A LANE IS CLOSED ON A FREEWAY/EXPRESSWAY.

WHEN THE LEFT LANE IS CLOSED, LEFT LANE CLOSED SIGNS SHALL BE SUBSTITUTED FOR THE RIGHT LANE CLOSED SIGNS.

THE FIRST TWO SIGNS AND THE MESSAGE BOARD ARE STATIONARY. THE OTHER SIGNS AND ARROWBOARDS SHALL BE MOVED AS NECESSARY TO MAINTAIN THE REQUIRED DISTANCE FROM THE START OF THE LANE CLOSURE TAPER(S).

SEE SPECIAL PROVISIONS.

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE SHOWN.

### STANDARD 701400 (SPECIAL)

|   |                      |            |           |
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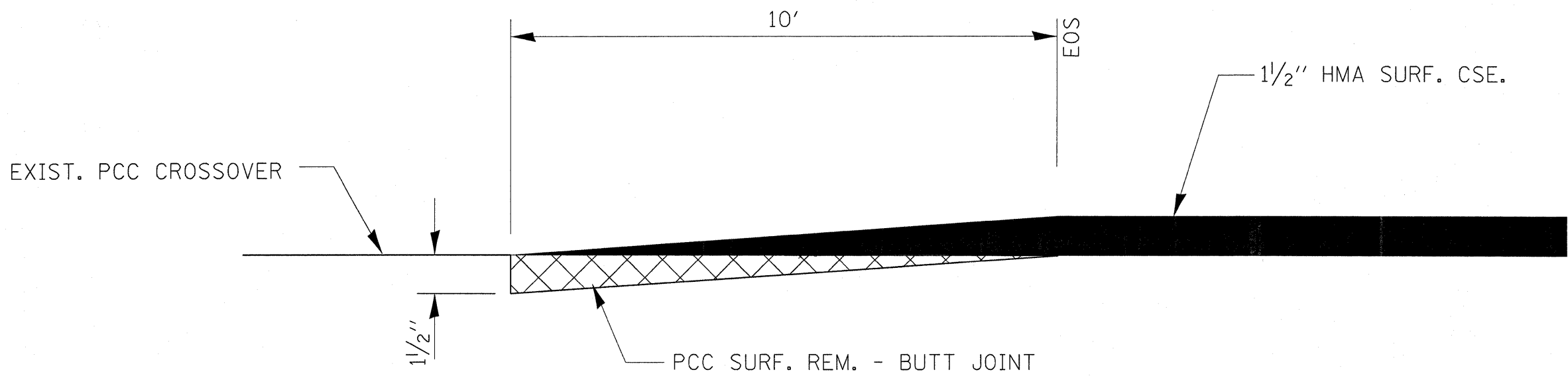
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DETAILS

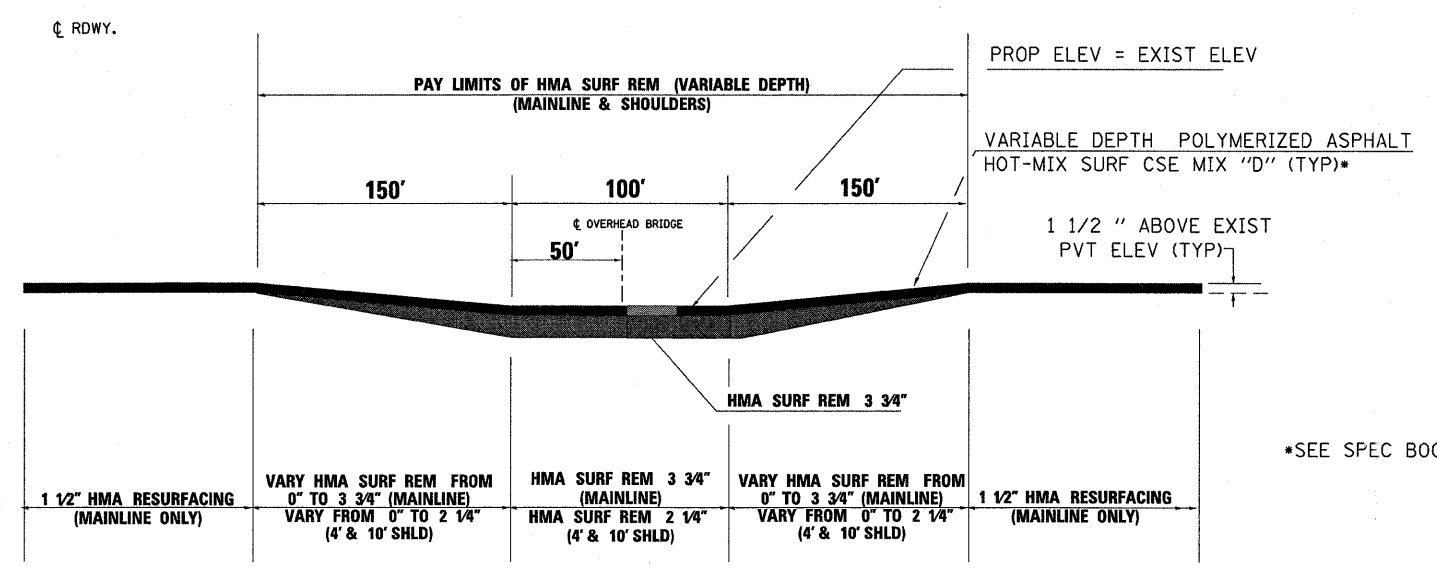
SCALE: \_\_\_\_\_ SHEET NO. \_\_\_\_ OF \_\_\_\_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

|   |         |        |                    |           |
|---|---------|--------|--------------------|-----------|
| F.A.T. RTE.                                   | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| I-80  | **      | BUREAU | 344                | 293       |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |         |        | CONTRACT NO. 66908 |           |

\*\*06-[7BR&BR-1,7VB-M,6BR&6,7 RS-1 & I]



**PCC SURF REMOVAL-BUTT JOINT**

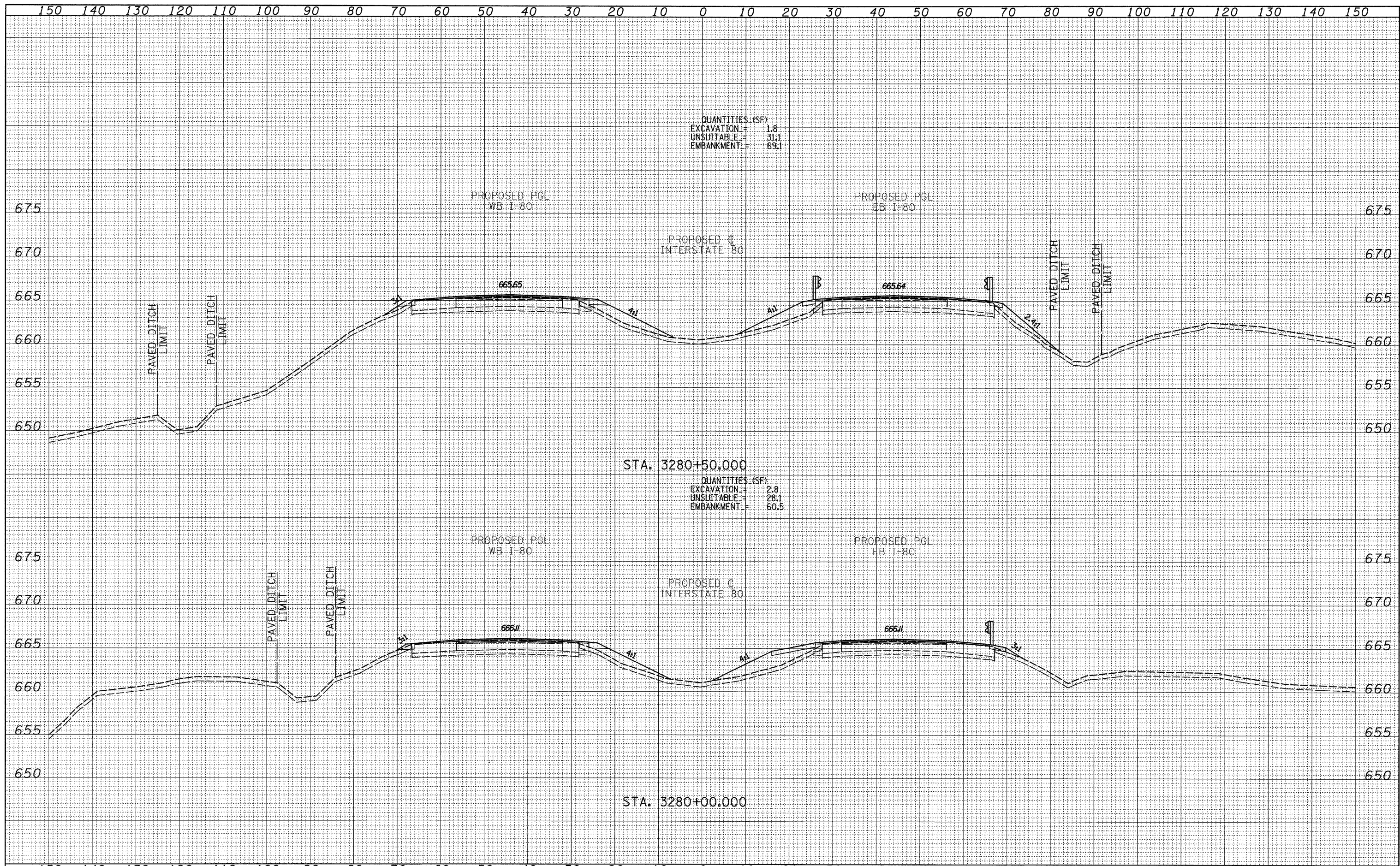


\*SEE SPEC BOOK FOR MAXIMUM LIFT THICKNESSES

**TREATMENT UNDER STRUCTURES TO MAINTAIN A MINIMUM OF 16'-0" CLEARANCE**

FOR S.N.'S 006-0127 (EB & WB), SN 006-0133 (EB ONLY), SN 006-0129 (EB & WB), AND SN 006-0132 (EB ONLY)  
 ALL OTHER STRUCTURES OVERLAY THE SAME AS THE REST OF THE MAINLINE  
 (R.E. TO VERIFY CLEARANCES OF ALL BRIDGES PRIOR TO CONSTRUCTION)

|   |  |  |  |  |  |  |  |  |  |  |  |   |  |  |  |                           |  |            |  |               |  |                  |  |               |  |
|---|--|--|--|--|--|--|--|--|--|--|--|---|--|--|--|---------------------------|--|------------|--|---------------|--|------------------|--|---------------|--|
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| **06-[7BR&BR-1,7VB-M,6BR&6.7 RS-1 & 1]  |  |  |  |  |  |  |  |  |  |  |  |   |  |  |  | ILLINOIS FED. AID PROJECT |  |            |  | 66908         |  |                  |  |               |  |



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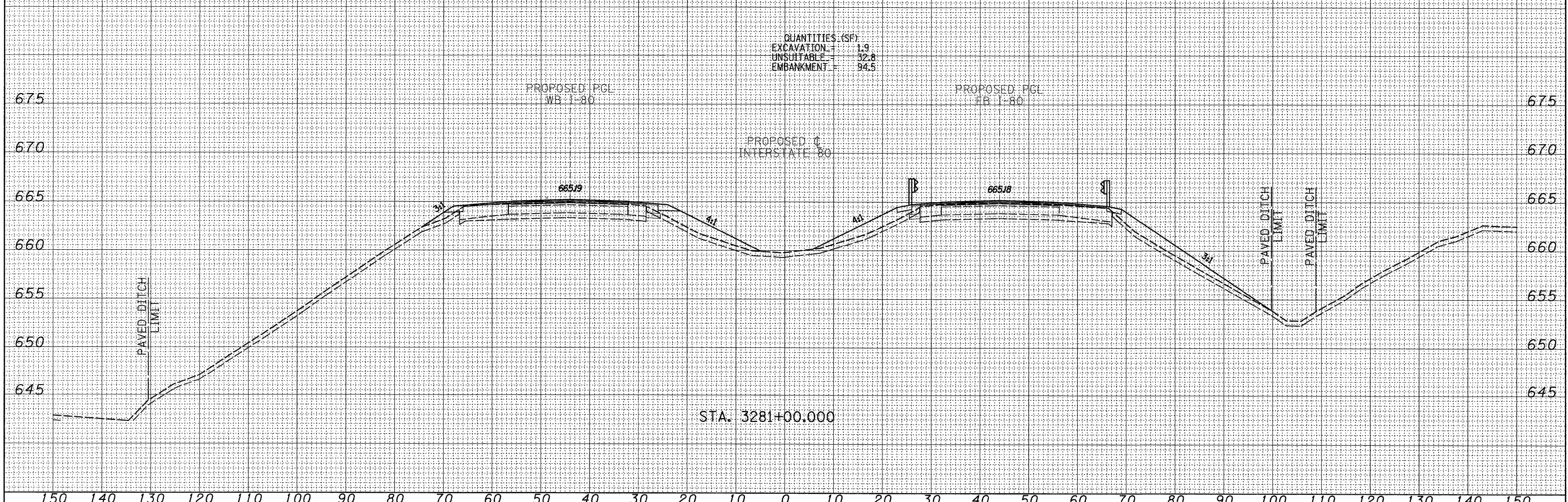
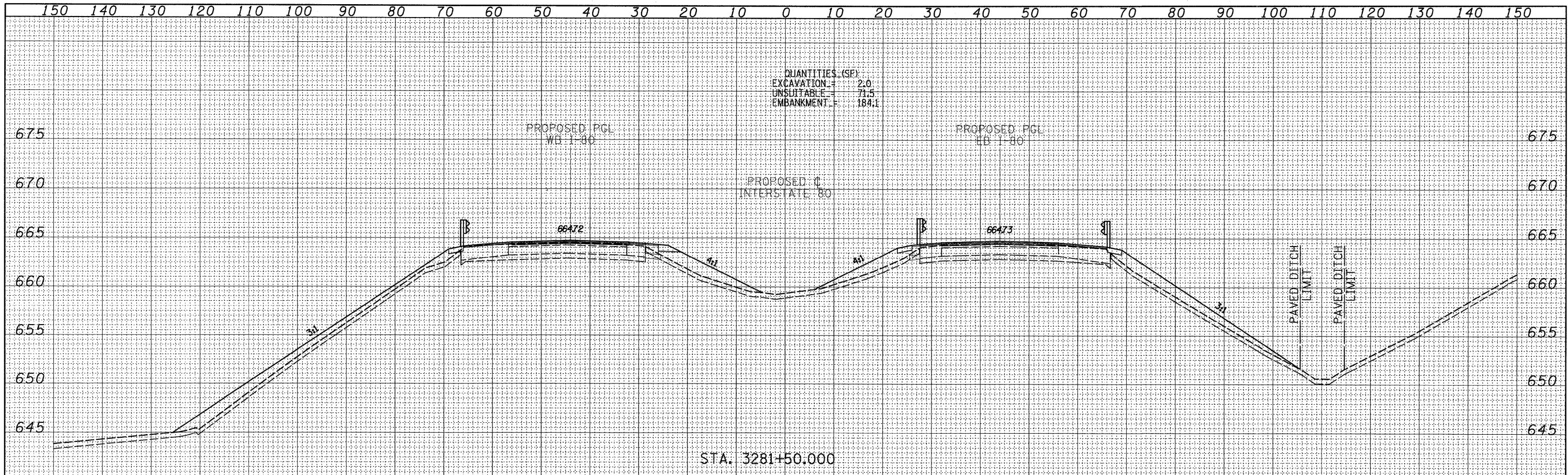
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 AREAS CHECKED  
 NO.

FY:602429, I-80, Road 602429.20, E, 20, 30, 31, 40

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| FILE NAME =<br>*FILEL* | USER NAME = *USER*    | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>PROPOSED CROSS SECTIONS</b> |           |    | F.A.<br>RTE.<br>80 | SECTION<br>*                         | COUNTY                    | TOTAL SHEETS<br>NO. |
| TYLIN INTERNATIONAL    | PLOT SCALE = *SCALE*  | DRAWN -    | REVISED - |   | SCALE:                         | SHEET NO. | OF | SHEETS             | STA. 3280+00.000 TO STA. 3280+50.000 | ILLINOIS FED. AID PROJECT | 344                 |
|                        | PLOT DATE = 8/14/2009 | CHECKED -  | REVISED - |   |                                |           |    |                    |                                      | FED. ROAD DIST. NO.       | 295                 |
|                        |                       | DATE -     | REVISED - |   |                                |           |    |                    |                                      | CONTRACT NO. 66908        |                     |

\* 06-17BR & BR-1, 7VB-M, 6BR & 6, 7 RS-1 & II



|                            |                       |            |           |                                     |   |                     |                           |        |   |           |
|----------------------------|-----------------------|------------|-----------|-------------------------------------|---|---------------------|---------------------------|--------|---|-----------|
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| #FILEL.S                   |                       | DRAWN -    | REVISED - | <b>DEPARTMENT OF TRANSPORTATION</b> | SCALE: SHEET NO. OF SHEETS STA. 3281+00.000 TO STA. 3281+50.000 | 80                  | .                         | BUREAU | 344   | 296       |
| <b>TYLIN INTERNATIONAL</b> | PLOT SCALE = *SCALE*  | CHECKED -  | REVISED - |                                     |   | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT |        | CONTRACT NO. 66908                            |           |
|                            | PLOT DATE = 8/14/2009 | DATE -     | REVISED - |                                     |   |                     |                           |        | • 06-C7BR & BR-1, TVB-M, 6BR & 6, 7 RS-1 & IJ |           |

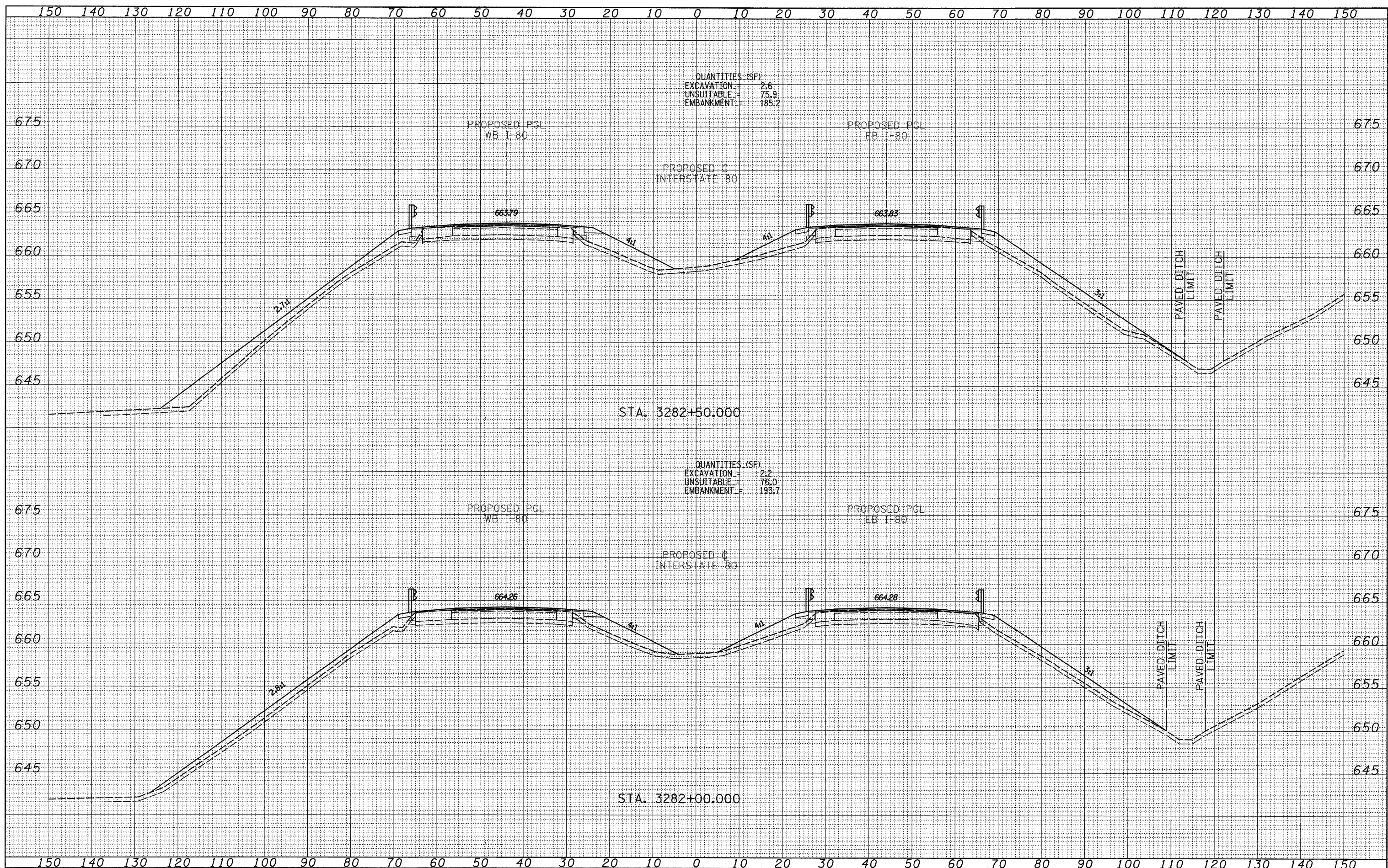
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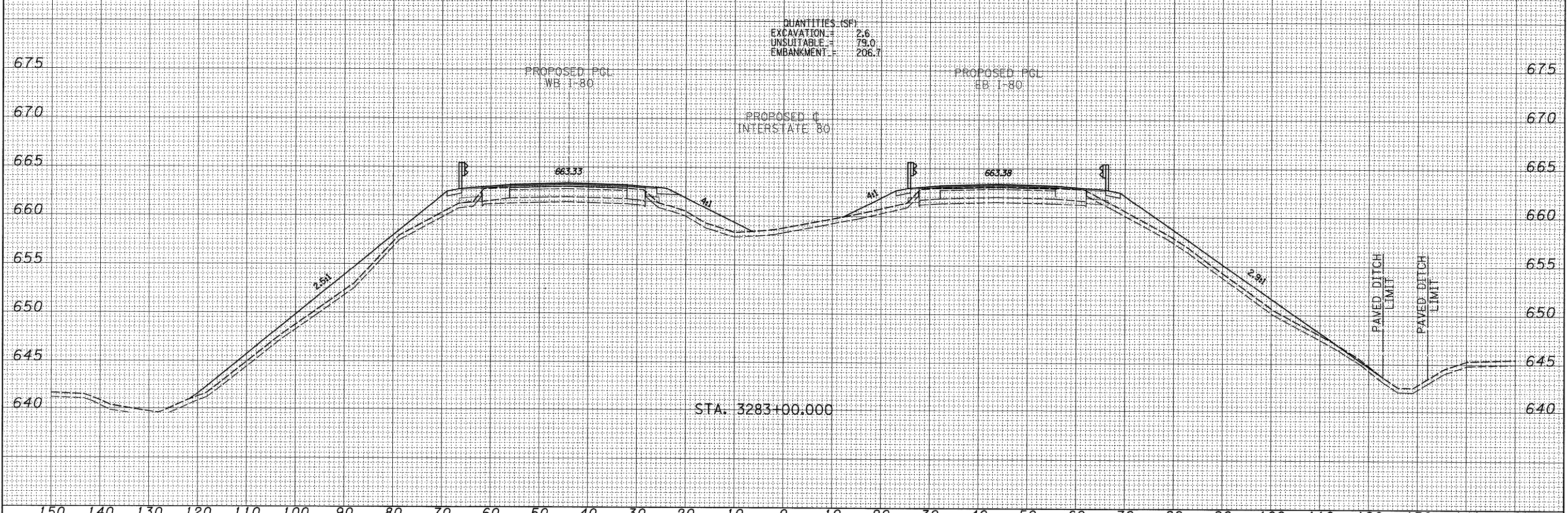
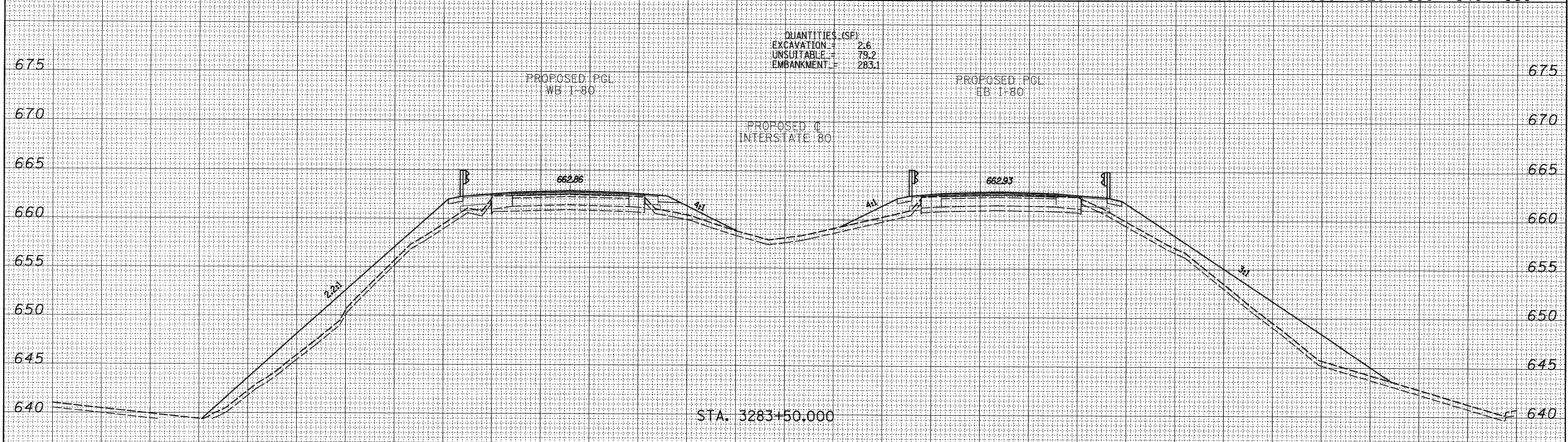
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PROPOSED CROSS SECTIONS**

SCALE: SHEET NO. OF SHEETS STA. 3282+00.000 TO STA. 3282+50.000

|   |              |        |                     |                  |
|---|--------------|--------|---------------------|------------------|
| F.A. RTE.<br>80                                 | SECTION<br>* | COUNTY | TOTAL SHEETS<br>344 | SHEET NO.<br>207 |
|   |              | BUREAU | CONTRACT NO. 66908  |                  |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |              |        |                     |                  |
| * 06-17BR & BR-1, 7VB-M, 6BR & 6, 7 RS-1 & 1J   |              |        |                     |                  |

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150



|                     |                       |            |           |   |                                |           |           |         |                                      |                     |                           |                    |
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| FILE NAME =         | USER NAME = #USER#    | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS</b><br><b>DEPARTMENT OF TRANSPORTATION</b> | <b>PROPOSED CROSS SECTIONS</b> |           | F.A. RTE. | SECTION | COUNTY                               | TOTAL SHEETS        | SHEET NO.                 |                    |
| #FILEL#             |                       | DRAWN -    | REVISED - |   | 80                             | .         | BUREAU    | 344     | 248                                  |                     |                           |                    |
| TYLIN INTERNATIONAL | PLOT SCALE = #SCALE#  | CHECKED -  | REVISED - |   | SCALE:                         | SHEET NO. | OF        | SHEETS  | STA. 3283+00.000 TO STA. 3283+50.000 | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | CONTRACT NO. 66908 |
|                     | PLOT DATE = 8/14/2009 | DATE -     | REVISED - |   |                                |           |           |         |                                      |                     |                           |                    |

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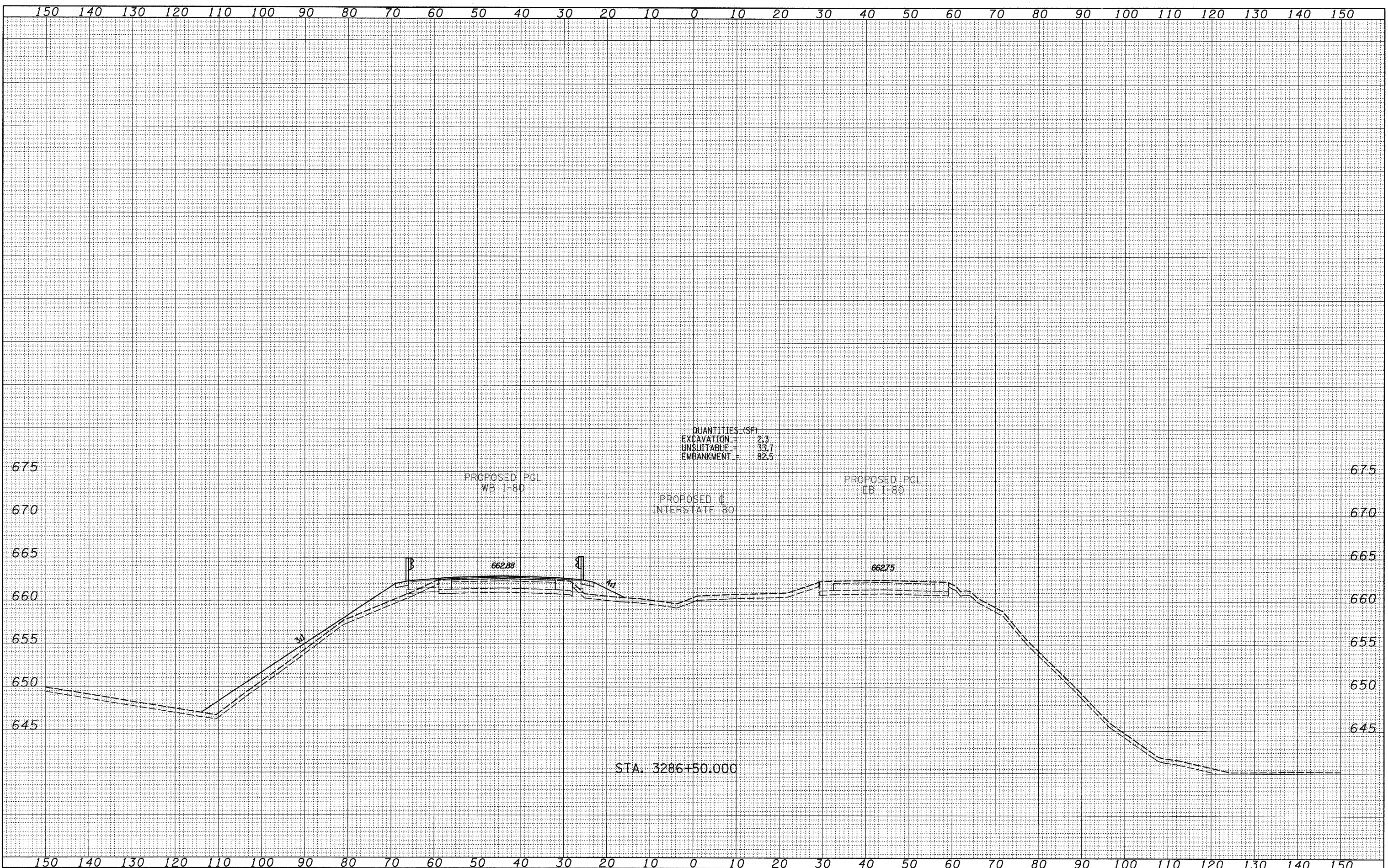
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• 06-L7BR & BR-1, TVB-M, 6BR & 6, 7 RS-1 & IJ

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**TYLIN INTERNATIONAL**

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| USER NAME = *USER*    | DESIGNED - | REVISED - |
| PLOT SCALE = *SCALE*  | DRAWN -    | REVISED - |
| PLOT DATE = 8/14/2009 | CHECKED -  | REVISED - |
|                       | DATE -     | REVISED - |

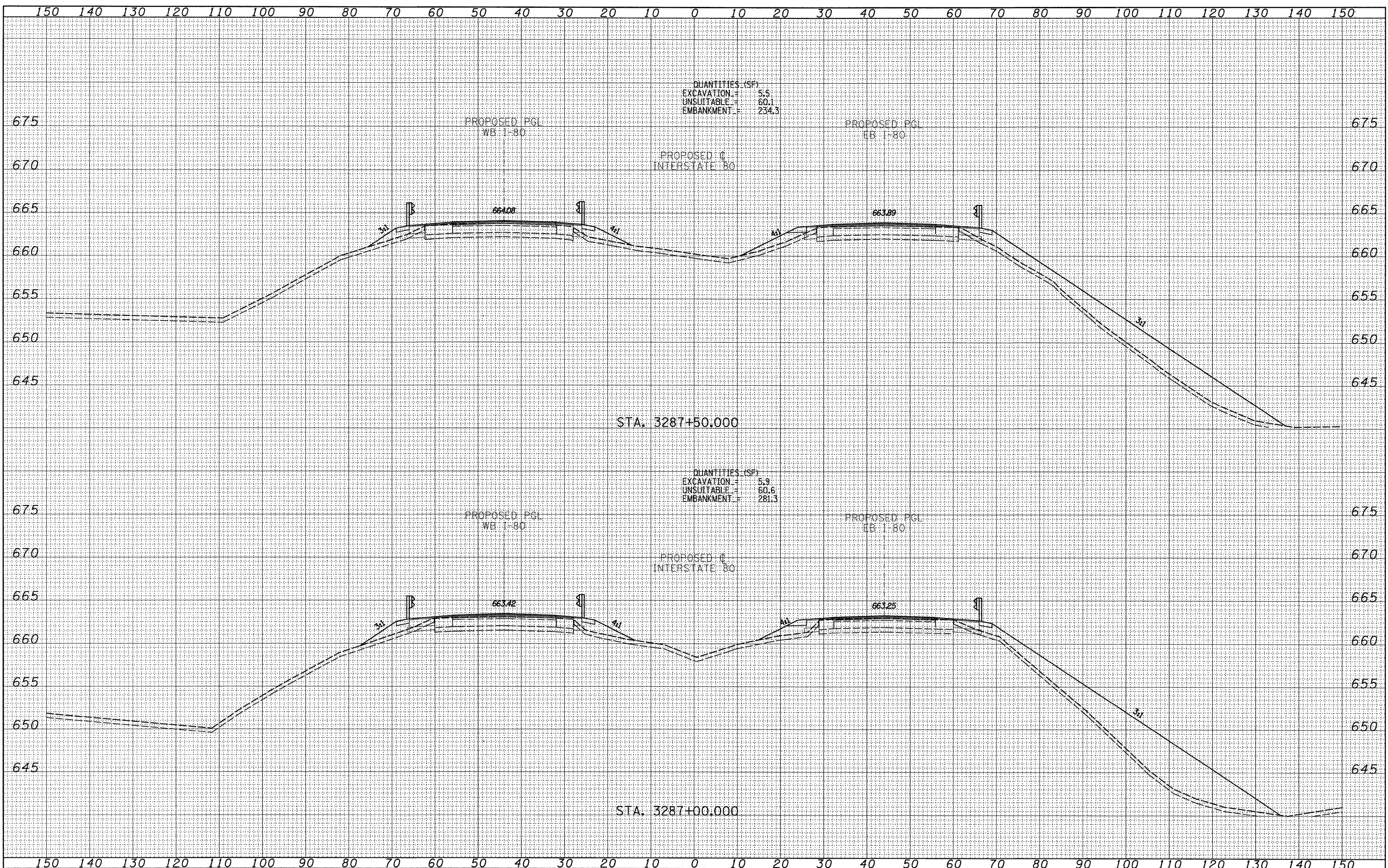
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED CROSS SECTIONS**

SCALE: SHEET NO. OF SHEETS STA. 3286+50.000 TO STA. 3286+50.000

|                     |              |                           |                     |                  |
|---------------------|--------------|---------------------------|---------------------|------------------|
| F.A. RTE.<br>80     | SECTION<br>• | COUNTY                    | TOTAL SHEETS<br>344 | SHEET NO.<br>299 |
| FED. ROAD DIST. NO. |              | ILLINOIS FED. AID PROJECT | CONTRACT NO. 66908  |                  |

• 06-CTBR & BR-1, 7VB-M, 6BR & 6, 7 RS-1 & II



FILE NAME =  
 #FILE#  
 TYLIN INTERNATIONAL

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = #USER#    | DESIGNED - | REVISED - |
| PLOT SCALE = #SCALE#  | DRAWN -    | REVISED - |
| PLOT DATE = 8/14/2009 | CHECKED -  | REVISED - |
|                       | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PROPOSED CROSS SECTIONS**

|        |                     |                                      |
|--------|---------------------|--------------------------------------|
| SCALE: | SHEET NO. OF SHEETS | STA. 3287+00.000 TO STA. 3287+50.000 |
|--------|---------------------|--------------------------------------|

|                     |              |                           |                     |                  |
|---------------------|--------------|---------------------------|---------------------|------------------|
| F.A. RTE.<br>80     | SECTION<br>* | COUNTY                    | TOTAL SHEETS<br>344 | SHEET NO.<br>300 |
| FED. ROAD DIST. NO. |              | ILLINOIS FED. AID PROJECT |                     |                  |
|                     |              | CONTRACT NO. 66908        |                     |                  |

• 06-I7BR & BR-1, TVB-M, 6BR & 6, 7 RS-1 & IJ

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