

GENERAL NOTES

Fasteners shall be AASHTO M164M Type 1 mechanically galvanized bolts. Bolts 20 mm ϕ , holes 22 mm ϕ , unless otherwise noted.

Calculated mass of Structural Steel = 55100 kg (M270M Gr 250)

The Inorganic zinc rich primer / Acrylic / Acrylic Paint System shall be used for shop and field painting of new structural steel except where otherwise noted. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Interstate Green, Munsell No. 7.5G 4/8. See Special Provision for Cleaning and Painting New Metal Structures.

The existing structural steel coating on existing beams 2 thru 9 and 10 thru 17 and the diaphragms located between these beams including pier bearings contains lead. See Sheet 3 of 24 for the location of these existing beams. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Cleaning and painting of the existing structural steel shall be as specified in the Special Provision for Cleaning and Painting Existing Steel Structures. All existing beams, bearings and other structural steel within 1.5 m (measured along the beam) of either side of deck joints shall be cleaned per Near White Blast Cleaning - SSPC-SF10. The exterior surfaces and bottom of the bottom flange of the fascia beams shall be cleaned per Power Tool Cleaning - Commercial Grade. All remaining structural steel shall be cleaned per Power Tool Cleaning - Modified SSPC-SF3.

The designated areas cleaned per Near White Blast Cleaning - SSPC-SF10 and per Power Tool Cleaning - Commercial Grade shall be painted according to the requirements of Paint System 1 - OZ/E/U. The designated areas cleaned per Power Tool Cleaning - Modified SSPC-SF3 shall be painted according to the requirements of Paint System 2 - PS/EM/U. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No 5B 7/1. The color of the final finish coat for this exterior and bottom flange of the fascia beams shall be Interstate Green, Munsell No. 7.5G 4/8.

No field welding is permitted except as specified in the contract documents.

Reinforcement bars shall conform to the requirements of AASHTO M31M or M322M Grade 400. See Special Provisions.

Reinforcement Bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of 3 mm. Adjustment shall be made either by grinding the surface or by shimming the bearings.

Concrete Sealer shall be applied to the designated areas at the abutments.

Any existing reinforcement bars which are intended to be incorporated into the new construction that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Slip forming of the concrete parapets will not be allowed.

All dimensions are in millimeters (mm) except as noted.

Slope wall shall be reinforced with welded wire fabric 152x152 - MW25.8xMW25.8 with a mass of 2.91 kg/m².

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer.

Any cracks that cannot be removed by grinding 1/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

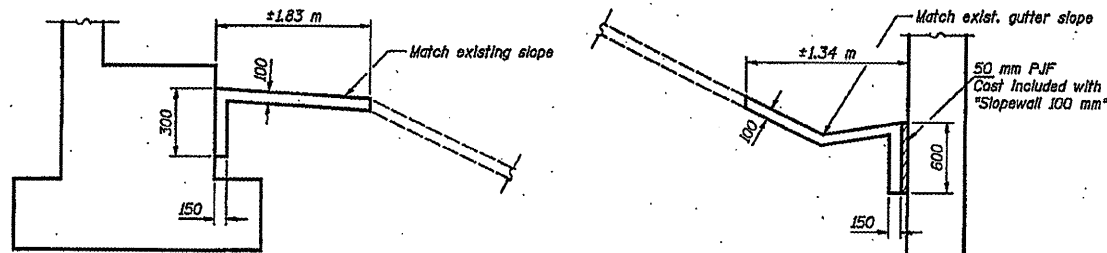
The SSPC OP-1 and OP-2 painting contractor certifications will be required for this bridge.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
CONCRETE REMOVAL	m ³	70.0	18.9	88.9
STRUCTURE EXCAVATION	m ³		119	119
PREFORMED JOINT STRIP SEAL	m	16.9		16.9
PREFORMED JOINT SEAL 64 mm	m	63.5		63.5
CONCRETE STRUCTURES	m ³		130.8	130.8
CONCRETE SUPERSTRUCTURE	m ³	141.9		141.9
PROTECTIVE COAT	m ²	582		582
FURNISHING AND ERECTING STRUCTURAL STEEL NO. 1	L. Sum		1	1
REINFORCEMENT BARS, EPOXY COATED	kg	20980	5460	26440
FURNISHING METAL PILE SHELLS 305 mm x 4.55 mm	m		216	216
DRIVING PILES	m		216	216
NAME PLATES	Each			2
CONCRETE SEALER	m ²		19.8	19.8
DRAINAGE SCUPPERS, DS-33	Each			2
FLOOR DRAINS	Each			4
PROTECTIVE SHIELD	m ²			236
SLOPE WALL REMOVAL	m ²		46	46
SLOPE WALL 100 mm	m ²		40	40
TEMPORARY SHEET PILING	m ²		106	106
BRIDGE DECK GROOVING	m ²	392		392
CLEANING AND PAINTING STEEL BRIDGE	L. Sum			1
CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES	L. Sum			1
STUD SHEAR CONNECTORS	Each	336		336
ELASTOMERIC BEARING ASSEMBLY, TYPE II	Each	8		8
ANCHOR BOLTS M24	Each	40		40

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SECTION AT ABUTMENT

SECTION AT PIER

SLOPE WALL DETAILS

ILLINOIS DEPARTMENT OF TRANSPORTATION	
SHEET TITLE GENERAL NOTES AND TOTAL BILL OF MATERIAL	
PROJECT F.A.I. 55 OVER BUSINESS U.S. 51 SECTION 5T-4HBR-4 STATION 37+474.206 MOLEAN COUNTY STRUCTURE NO. 057-0024 (N.B.) & STRUCTURE NO. 057-0025 (S.B.)	PROJECT NO. 08049-5
DATE 07/15/2009	DESIGNED BY TFB/CFC
CHECKED BY CNE/MCB	DRAWN BY 2
COOMBE-BLOXDORF P.C. Engineers / Land Surveyors Springfield, Illinois Design Firm License No. 184-002703	
OF 24 SHEETS	