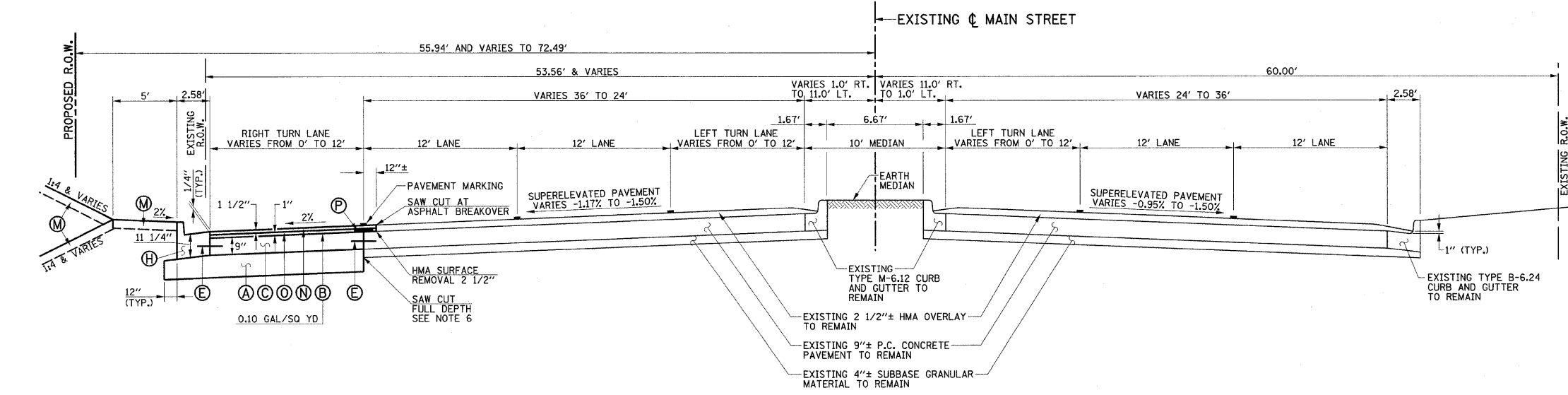


**PROPOSED TYPICAL CROSS SECTION
MAIN STREET**
STA. 294+16.00 TO STA. 298+91.64
(LOOKING SOUTH)

- PROPOSED TYPICAL SECTION KEY**
- (A) AGGREGATE BASE COURSE, TYPE A 12"
 - (B) BITUMINOUS MATERIALS (PRIME COAT) - SEE TYPICAL FOR APP. RATE
 - (C) PORTLAND CEMENT CONCRETE BASE COURSE 9"
 - (D) PORTLAND CEMENT CONCRETE PAVEMENT 8" (JOINTED)
 - (E) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 x 24" EPOXY COATED TIE BARS GROUTED IN PLACE OR NO. 6 x 30" EPOXY COATED TIE BARS FORMED IN PLACE AT 24" CENTERS (STD. 420001)
 - (F) SAWED LONGITUDINAL JOINT WITH NO. 6 x 30" EPOXY COATED TIE BARS AT 30" CENTERS (STD. 420001)
 - (G) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (STD. 606001)
 - (H) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (SPECIAL) (STD. 606001)
 - (I) PORTLAND CEMENT CONCRETE SIDEWALK 4" - (SEE NOTE 12)
 - (J) HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70 (2")
 - (K) AGGREGATE BASE COURSE, TYPE A 6"
 - (L) STABILIZED SUB-BASE - HOT-MIX ASPHALT, 4"
 - (M) TOPSOIL 4"
 - (N) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 (1 1/2")
 - (O) POLYMERIZED LEVELING BINDER (MACHINE METHOD), N90 (1")
 - (P) STRIP REFLECTIVE CRACK CONTROL TREATMENT



**PROPOSED TYPICAL CROSS SECTION
MAIN STREET**
STA. 298+91.64 TO STA. 303+68.00
(LOOKING SOUTH)

STRUCTURAL PAVEMENT DESIGN INFORMATION	
MAIN STREET	
STRUCTURAL DESIGN TRAFFIC:	YEAR 2018
PV =	18223 SU = 792 MU = 792
ROAD/STREET CLASSIFICATION:	CLASS I
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:	
P =	50% S = 50% M = 50%
TRAFFIC FACTOR:	TF = 6.01
SUBGRADE SUPPORT RATING:	SSR = "POOR"
MINIMUM STRUCTURAL DESIGN REQUIREMENTS:	
P.C. CONCRETE PAVEMENT	= 9"
GRANULAR SUBBASE	= 12"

NOTES

- THE COMBINATION CONCRETE CURB AND GUTTER ADJACENT TO NEW PCC BASE COURSE SHALL BE IN ACCORDANCE WITH STD. 606001 EXCEPT THAT IT SHALL BE CONSTRUCTED TO THE FULL THICKNESS OF THE BASE COURSE AND HOT-MIX ASPHALT SURFACING. THE COST OF THE CURB AND GUTTER, INCLUDING THE ADDITIONAL THICKNESS, SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (SPECIAL).
- SAWED TRANSVERSE CONTRACTION JOINTS 3" DEEP SHALL BE PLACED IN THE PROPOSED P.C. CONCRETE BASE COURSE IN PROLONGATION OF THE EXISTING PAVEMENT JOINTS IN ACCORDANCE WITH ARTICLE 420.05 (c) OF THE STANDARD SPECIFICATIONS. THE JOINTS IN THE P.C. CONCRETE BASE COURSE SHALL NOT BE SEALED. DOWEL BARS WILL NOT BE REQUIRED AT THE TRANSVERSE CONTRACTION JOINTS.
- SAWED CONTRACTION JOINTS 3" DEEP SHALL BE PLACED IN THE COMBINATION CONCRETE CURB AND GUTTER IN PROLONGATION OF THE BASE COURSE JOINTS AND THE JOINTS SHALL BE SEALED. THIS WORK SHALL BE IN ACCORDANCE WITH ARTICLE 606.07 OF THE STANDARD SPECIFICATIONS. DOWEL BARS WILL NOT BE REQUIRED AT THE CONTRACTION JOINTS IN THE CURB AND GUTTER.
- THE CURB AND GUTTER SHALL NOT BE POURED MONOLITHIC WITH THE P.C. CONCRETE BASE COURSE EXCEPT AT THE STUB LOCATIONS. THE TIE BARS BETWEEN THE P.C. CONCRETE BASE COURSE AND THE CURB AND GUTTER WILL BE REQUIRED.
- WHEN LONGITUDINAL CONSTRUCTION JOINTS ARE CONSTRUCTED IN THE P.C. CONCRETE BASE COURSE THE JOINTS SHALL BE TIED WITH NO. 6 x 24" EPOXY COATED TIE BARS GROUTED IN PLACE OR NO. 6 x 30" EPOXY COATED TIE BARS FORMED IN PLACE SPACED AT 24" CENTERS AS SHOWN ON STD. 420001.
- THE CONTRACTOR WILL BE REQUIRED TO SAW CUT AND REMOVE THE EXISTING PAVEMENT AS DIRECTED BY THE ENGINEER TO PROVIDE A CLEAN VERTICAL EDGE IF NO EXISTING JOINT IS VISIBLE. THE SAW CUTTING OF THE PAVEMENT WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF THE VARIOUS REMOVAL PAY ITEMS.
- THE FINISHED EARTHWORK SHALL HAVE VEGETATIVE SUSTAINING SOIL COVERING THE TOP 4" OF AREAS TO BE SEEDED. THE CONTRACTOR SHALL STOCKPILE TOPSOIL FROM THE EXCAVATION OPERATIONS. THE TOPSOIL SHALL MEET THE REQUIREMENTS OF ARTICLE 1081.05 OF THE STANDARD SPECIFICATIONS OR BE APPROVED BY THE ENGINEER. THE VEGETATIVE SUSTAINING SOIL REQUIRED WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR TOPSOIL EXCAVATION AND PLACEMENT. A TOWN QUANTITY FOR TOPSOIL FURNISH AND PLACE, 4" HAS BEEN PROVIDED TO ESTABLISH A UNIT PRICE IN CASE ADDITIONAL TOPSOIL MATERIAL IS REQUIRED. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
- THE TOPSOIL SHALL BE REMOVED TO A DEPTH OF 12" WITHIN THE SUBGRADE LIMITS OF ALL PROPOSED PAVED AREAS AS SHOWN ON THE CROSS SECTIONS AND STOCKPILED. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR TOPSOIL EXCAVATION AND PLACEMENT. THE EXCESS VOLUME OF TOPSOIL EXCAVATED AND NOT USED FOR TOPSOIL PLACEMENT SHALL BE PLACED AS EMBANKMENT IN FILL AREAS BEHIND THE PROPOSED BACK OF THE CURBS. TOPSOIL WILL NOT BE ALLOWED TO BE PLACED AS FILL UNDER PAVEMENTS, SIDEWALKS, OR PEDESTRIAN TRAILS. ANY EXCESS VOLUME OF TOPSOIL EXCAVATED WHICH IS NOT USED FOR TOPSOIL PLACEMENT OR IS PLACED IN THE EMBANKMENT AREAS SHALL BE REMOVED FROM THE SITE AND DELIVERED TO A SITE WITHIN THE CITY LIMITS AS DIRECTED BY THE ENGINEER. EXCESS TOPSOIL REMOVED FROM THE SITE WILL BE PAID FOR AS EARTH EXCAVATION. SEE THE SPECIAL PROVISION FOR TOPSOIL EXCAVATION AND PLACEMENT. EMBANKMENT WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE COST OF THE OTHER EARTHWORK ITEMS.
- ALL EXPOSED EARTH AREAS SHALL BE SEEDED, FERTILIZED, AND MULCHED IN ACCORDANCE WITH SECTIONS 250 AND 251 OF THE STANDARD SPECIFICATIONS. SEEDING, CLASS 1A AND MULCH, METHOD 3 SHALL BE USED. EROSION CONTROL BLANKETS SHALL BE SUBSTITUTED FOR THE MULCH AT LOCATIONS AS DIRECTED BY THE ENGINEER. A TOWN QUANTITY FOR EROSION CONTROL BLANKET HAS BEEN PROVIDED TO ESTABLISH A UNIT PRICE. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
- SEE PLAN AND PROFILE SHEETS AND HORIZONTAL ALIGNMENT AND CONTROL SHEET FOR DETAILED LOCATIONS OF EDGES OF PAVEMENTS, CURBS AND GUTTERS, SIDEWALKS AND RIGHT-OF-WAY LINES. SEE CROSS SECTIONS FOR DETAILED SIDE SLOPE RATIOS.
- THE EXISTING CONCRETE PAVEMENT SHALL BE PREPARED AND REPAIRED PRIOR TO PLACING THE HOT-MIX ASPHALT MATERIALS IN ACCORDANCE WITH SECTION 358 AND 406 OF THE STANDARD SPECIFICATIONS.
- THE P.C. CONCRETE SIDEWALKS SHALL BE THICKENED TO 6" OR 8" THROUGH DRIVEWAYS TO MATCH THE DRIVEWAY PAVEMENT THICKNESS. THE COST OF CONSTRUCTING THE P.C. CONCRETE SIDEWALK 6" OR 8" THICK THROUGH DRIVEWAYS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C. CONCRETE 4" AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. SEE THE DRIVEWAY DETAILS FOR ADDITIONAL INFORMATION.

SEE HORIZONTAL ALIGNMENT LAYOUT AND CONTROL SHEET FOR DETAILED LOCATIONS OF EXISTING R.O.W., PROPOSED R.O.W., PROPOSED PERMANENT EASEMENTS, AND TEMPORARY CONSTRUCTION EASEMENTS.

ILLINOIS DEPARTMENT OF TRANSPORTATION

**PROPOSED TYPICAL SECTIONS
MAIN STREET**

DATE : 6-09
DRAWN BY : J.L.B.
CHECKED BY : R.L.H.

SCALE : NONE