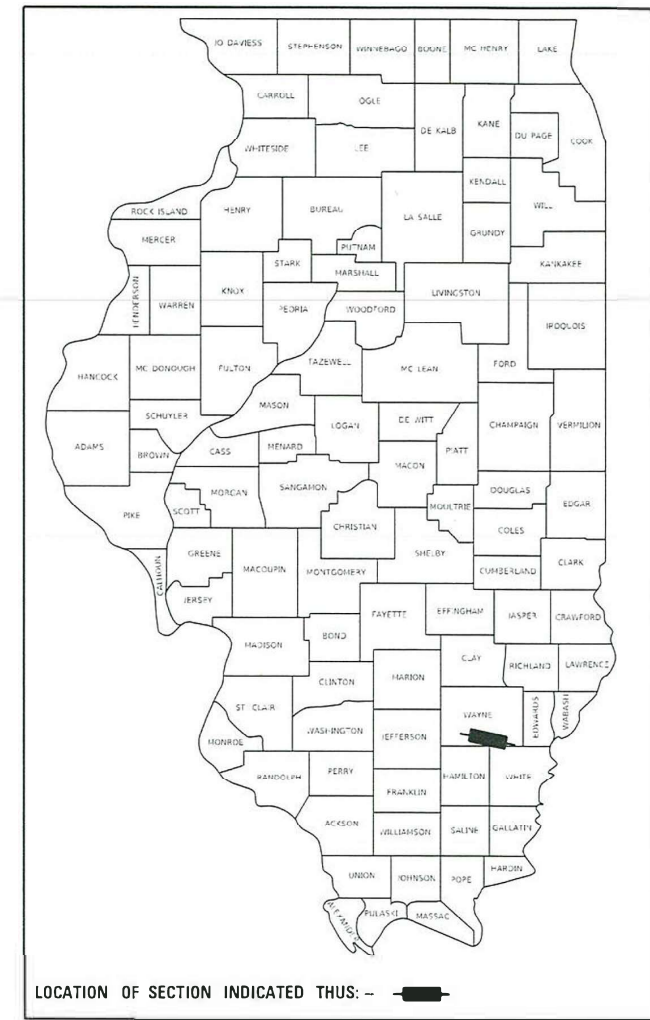


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	1
		ILLINOIS	CONTRACT NO. 74982	

D-97-059-20



FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROPOSED HIGHWAY PLANS

2019 ADT = 12,700

FAI ROUTE 64 (I-64)
SECTION 96-(2-1,3,3-1,4)RS-1
PROJECT NHPP-KH6V(110)
SMART
WAYNE COUNTY

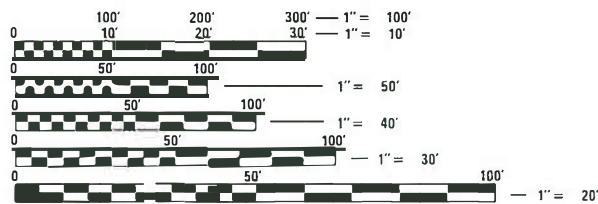
C-97-065-20

STATION EQUATIONS:

I-64 EASTBOUND
STA. 4221 + 08.50(BK) = STA. 4214 + 81.00(AH)
I-64 WESTBOUND
STA. 1020 + 82.74(BK) = STA. 4020 + 83.94(AH)
STA. 4053 + 43.47(BK) = STA. 1053 + 43.21(AH)
STA. 1220 + 55.52(BK) = STA. 4214 + 94.08(AH)
STA. 4256 + 75.34(BK) = STA. 4256 + 75.33(AH)

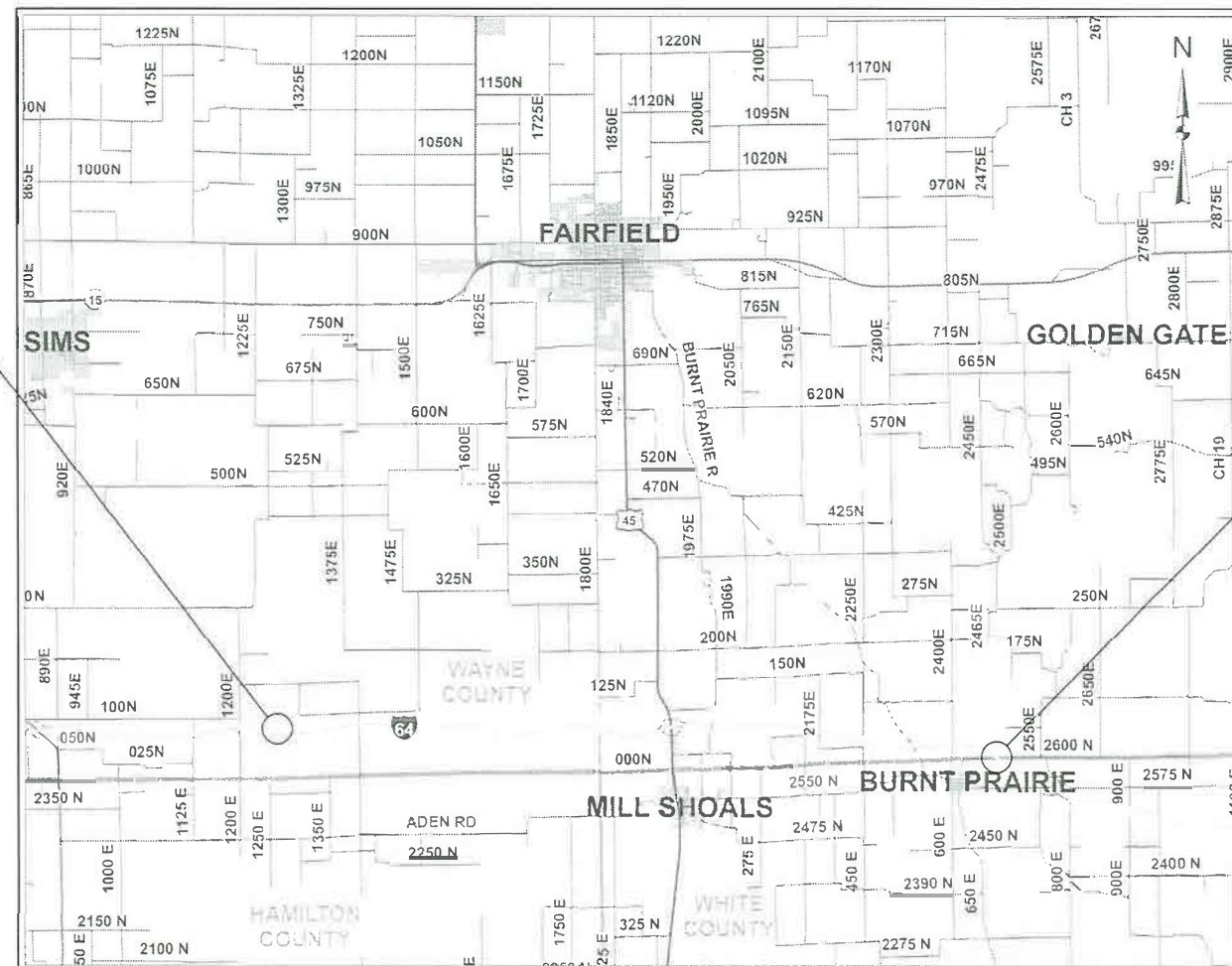
PROJECT BEGINS:
STA. 3986 + 60.55 (EB)
STA. 985 + 37.01 (WB)

PROJECT ENDS:
STA. 4632 + 36.48(EB)
STA. 4638 + 78.25 (WB)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811



GROSS LENGTH = 65,901.74 FT. = 12.481 MILE
NET LENGTH = 64,530.74 FT. = 12.222 MILE

PROJECT ENGINEER TOM RONAN
PROJECT MANAGER JEFF DAVISON

CONTRACT NO. 74982

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED AUGUST 13 2020
Jeffrey P. Myrland
REGIONAL ENGINEER

October 2, 2020
Scott A. Etk
ENGINEER OF DESIGN AND ENVIRONMENT

October 2, 2020
James J. Gu
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

1	COVER SHEET
2	INDEX, STANDARDS AND GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5-6	TYPICAL CROSS SECTIONS
7-18	SCHEDULE OF QUANTITIES
19-32	PLAN SHEETS
33-36	PAVEMENT MARKING SHEETS
37-43	DISTRICT DETAILS

HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
442001-04	CLASS A PATCHES
442101-09	CLASS C AND D PATCHES
642001-02	SHOULDER RUMBLE STRIPS, 16 IN.
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 M) TO 24" (600MM) FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 M) AWAY
701201-05	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS >= 45 MPH
701400-09	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-12	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS >= 45 MPH
701406-12	LANE CLOSURE, FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS >= 45 MPH
701456-05	PARTIAL RAMP CLOSURE FREEWAY/EXPRESSWAY
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

GENERAL NOTES

THIS PROJECT IS LOCATED ON FAI ROUTE 64 (I-67) IN WAYNE COUNTY, FROM 7 MILES WEST OF US 45 TO THE WHITE COUNTY LINE. THE WORK INCLUDED IN SECTION 96-(2-1,3,3-1,4)RS-1 CONSISTS OF PAVEMENT PATCHING, HOT-MIX ASPHALT SURFACE REMOVAL, HOT-MIX ASPHALT RESURFACING, HOT-MIX ASPHALT SHOULDERS, AGGREGATE SHOULDERS, PAVEMENT MARKING, TRAFFIC CONTROL, AND ANY OTHER WORK NECESSARY TO COMPLETE THE SECTION.

WHEN APPLYING SHORT-TERM PAVEMENT MARKINGS, TEMPORARY TAPE SHALL BE USED ON THE SURFACE COURSE AND PAINT SHALL BE USED ON MILLED SURFACES.

THE MATERIAL USED FOR AGGREGATE WEDGE SHOULDERS, TYPE B SHALL BE CRUSHED STONE, CRUSHED STONE OR CRUSHED CONCRETE.

THE MAINLINE QUANTITY FOR AGGREGATE WEDGE SHOULDERS, TYPE B WAS CALCULATED FOR THE AREAS AROUND THE DELINEATOR POSTS ON I-64.

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN THE CALCULATING PLAN QUANTITIES:

AGGREGATE SHOULDERS	2.05 TONS/CU YD
BITUMINOUS MATERIALS (PRIME COAT)	0.05 LBS/SQ FT
BITUMINOUS MATERIALS (TACK COAT)	0.025 LBS/SQ FT
HOT-MIX ASPHALT (OTHER THAN STONE MATRIX ASPHALT)	112 LBS/SQ YD/INCH

THE LONGITUDINAL JOINT SEALANT SHALL BE APPLIED TO THE MAINLINE MILLED SURFACE ONLY.

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

APPLICATION	AC/PG	DESIGN AIR VOIDS	MIXTURE COMPOSITION	FRICTION AGGREGATE	QUALITY MANAGEMENT
POLYMERIZED HMA SURFACE COURSE, IL-9.5, MIX "D", N90 (Mainline & Ramps)	SBS PG 76-22	4.0% @ N=90	IL - 9.5	MIXTURE D	PFP
CLASS "D" PAVEMENT PATCHING	PG 64-22	4.0% @ N=90	IL - 19.0	N/A	QC/QA
HMA SURFACE COURSE, MIX "C", N70 (Mainline/Ramp Shoulders and Xover's)	PG 64-22	4.0% @ N=70	IL - 9.5	MIXTURE C	QC/QA

REV. - MS

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OFFICE: District 7
PROJECT: 74982
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SHEETS: D74982-shc-gennote.dgn

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SCALE: SHEET OF SHEETS STA. TO STA.				ILLINOIS FED. AID PROJECT CONTRACT NO. 74982				

90% FED
10% STATE

90% FED
10% STATE

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005		
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	254497	254497		
40600990	TEMPORARY RAMP	SQ YD	2405	2405		
40604164	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N90	TON	30631	30631		
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	540296	540296		
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	25252	25252		
44201365	CLASS C PATCHES, TYPE II, 11 INCH	SQ YD	96	96		
44201371	CLASS C PATCHES, TYPE IV, 11 INCH	SQ YD	53	53		
44201781	CLASS D PATCHES, TYPE III, 11 INCH	SQ YD	192	192		
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	471	471		
48203100	HOT-MIX ASPHALT SHOULDERS	TON	20035	20035		
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	250321	250321		
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6		
67000600	ENGINEER'S FIELD LABORATORY	CAL MO	6	6		

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005		
67100100	MOBILIZATION	L SUM	1	1		
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	4	4		
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1		
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1	1		
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1		
70100825	TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	L SUM	1	1		
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	28	28		
70300100	SHORT TERM PAVEMENT MARKING	FOOT	46784	46784		
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	7797	7797		
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	111	111		
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	267781	267781		

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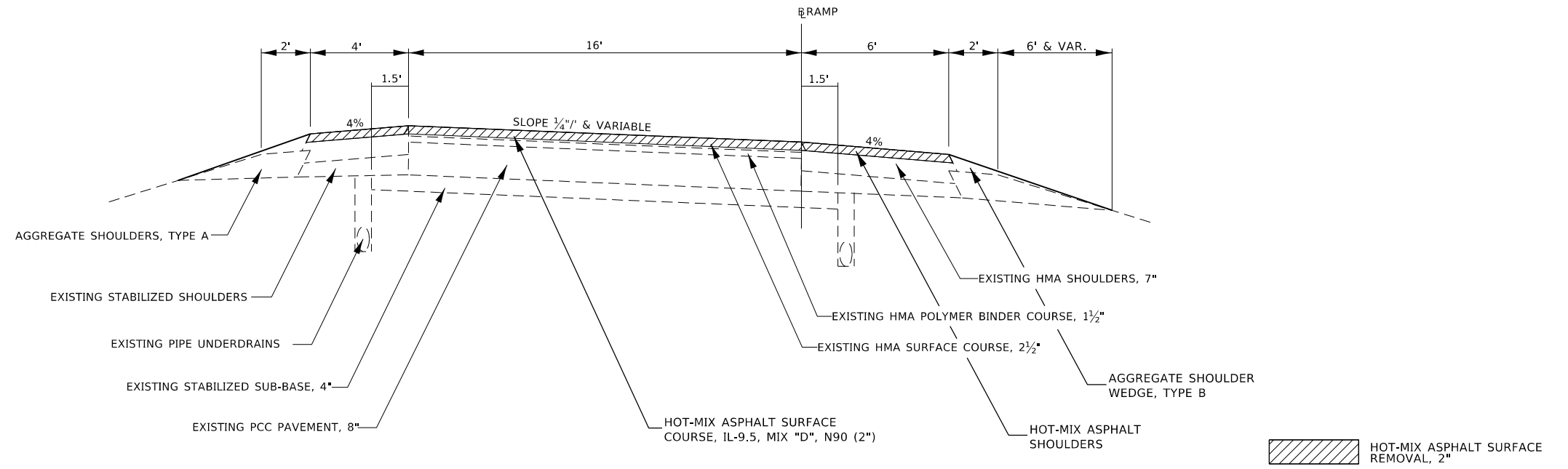
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	3
CONTRACT NO. 74982			ILLINOIS FED. AID PROJECT	

**TYPICAL SECTION F.A.I. 64 & U.S. 45 INTERCHANGE
RAMPS "A", "B", "C" AND "D"**



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PLOT DATE = 8/12/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL CROSS SECTIONS

SCALE: NA SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	6
CONTRACT NO. 74982			ILLINOIS FED. AID PROJECT	

EB PAVING SCHEDULE

LOCATION			LENGTH	PAVEMENT WIDTH	SHOULDER WIDTH	AREA	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	BITUMINOUS MATERIALS (TACK COAT)	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90	HOT-MIX ASPHALT SHOULDERS	LONGITUDINAL JOINT SEALANT
STATION	TO	STATION	FEET	FEET	FEET	SQ FT	SQ YD	POUND	TON	TON	FOOT
FAI 64 (EB)											
3986+59.62	TO	3994+98.13	838.5	24.0	16.0	33,540.4	3,726.7	1,677.0	250.4	125.2	838.5
3994+98.13	TO	4003+77.00	878.9	24.0	16.0	35,154.8	3,906.1	1,757.7	262.5	131.2	878.9
4003+77.00	TO	4012+55.65	878.7	24.0	16.0	35,146.0	3,905.1	1,757.3	262.4	131.2	878.7
4012+55.65	TO	4020+83.94	828.3	24.0	16.0	33,131.6	3,681.3	1,656.6	247.4	123.7	828.3
4020+83.94	TO	4053+43.47	3,259.5	24.0	16.0	130,381.2	14,486.8	6,519.1	973.5	486.8	3,259.5
4053+43.47	TO	4053+61.42	18.0	24.0	16.0	718.0	79.8	35.9	5.4	2.7	18.0
4053+61.42	TO	4056+62.23	300.8	24.0	16.0	12,032.4	1,336.9	601.6	89.8	44.9	300.8
4056+62.23	TO	4057+65.86	103.6	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
4057+65.86	TO	4084+25.15	2,659.3	24.0	16.0	106,371.6	11,819.1	5,318.6	794.2	397.1	2,659.3
4084+25.15	TO	4099+59.02	1,533.9	24.0	16.0	61,354.8	6,817.2	3,067.7	458.1	229.1	1,533.9
4099+59.02	TO	4122+15.51	2,256.5	24.0	16.0	90,259.6	10,028.8	4,513.0	673.9	337.0	2,256.5
4122+15.51	TO	4126+01.85	386.3	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
4126+01.85	TO	4129+51.71	349.9	24.0	16.0	13,994.4	1,554.9	699.7	104.5	52.2	349.9
4129+51.71	TO	4138+48.85	897.1	24.0	16.0	35,885.6	3,987.3	1,794.3	267.9	134.0	897.1
4138+48.85	TO	4167+97.09	2,948.2	24.0	16.0	117,929.6	13,103.3	5,896.5	880.5	440.3	2,948.2
4167+97.09	TO	4176+98.57	901.5	24.0	16.0	36,059.2	4,006.6	1,803.0	269.2	134.6	901.5
4176+98.57	TO	4209+69.37	3,270.8	24.0	16.0	130,832.0	14,536.9	6,541.6	976.9	488.4	3,270.8
4209+69.37	TO	4221+08.50	1,139.1	24.0	16.0	45,565.2	5,062.8	2,278.3	340.2	170.1	1,139.1
4221+08.50	=	4214+81.00	-627.5	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
4214+81.00	TO	4229+87.42	1,506.4	24.0	16.0	60,256.8	6,695.2	3,012.8	449.9	225.0	1,506.4
4229+87.42	TO	4233+15.59	328.2	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
4233+15.59	TO	4235+66.87	251.3	24.0	16.0	10,051.2	1,116.8	502.6	75.0	37.5	251.3
4235+66.87	TO	4256+42.12	2,075.3	24.0	16.0	83,010.0	9,223.3	4,150.5	619.8	309.9	2,075.3
4256+42.12	TO	4256+75.34	33.2	24.0	16.0	1,328.8	147.6	66.4	9.9	5.0	33.2
4256+75.34	TO	4283+44.16	2,668.8	24.0	16.0	106,752.8	11,861.4	5,337.6	797.1	398.5	2,668.8
4283+44.16	TO	4309+91.28	2,647.1	24.0	16.0	0.0	0.0	0.0	790.6	395.3	2,647.1
4309+91.28	TO	4316+16.54	625.3	24.0	16.0	25,010.4	2,778.9	1,250.5	186.7	93.4	625.3
4316+16.54	TO	4316+51.27	34.7	24.0	6.0	1,041.9	115.8	52.1	10.4	1.9	34.7
4316+51.27	TO	4317+95.25	144.0	24.0	6.0	0.0	0.0	0.0	0.0	0.0	0.0
4317+95.25	TO	4319+76.31	181.1	24.0	6.0	5,431.8	603.5	271.6	54.1	10.1	181.1
4319+76.31	TO	4321+46.72	170.4	24.0	14.5	6,560.8	729.0	328.0	50.9	23.1	170.4
4321+46.72	TO	4333+94.74	1,248.0	24.0	16.0	49,920.8	5,546.8	2,496.0	372.7	186.4	1,248.0
4333+94.74	TO	4335+44.74	150.0	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
4335+44.74	TO	4346+86.19	1,141.5	24.0	16.0	45,658.0	5,073.1	2,282.9	340.9	170.5	1,141.5
4346+86.19	TO	4350+11.37	325.2	24.0	17.0	13,332.4	1,481.4	666.6	97.1	51.6	325.2
4350+11.37	TO	4358+24.93	813.6	24.0	6.0	24,406.8	2,711.9	1,220.3	243.0	45.6	813.6

EB PAVING SCHEDULE CONT'D

LOCATION			LENGTH	PAVEMENT WIDTH	SHOULDER WIDTH	AREA	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	BITUMINOUS MATERIALS (TACK COAT)	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90	HOT-MIX ASPHALT SHOULDERS	LONGITUDINAL JOINT SEALANT
STATION	TO	STATION	FEET	FEET	FEET	SQ FT	SQ YD	POUND	TON	TON	FOOT
FAI 64 (EB)											
4358+24.93	TO	4358+57.95	33.0	24.0	6.0	990.6	110.1	49.5	9.9	1.8	33.0
4358+57.95	TO	4380+65.04	2,207.1	24.0	16.0	88,283.6	9,809.3	4,414.2	659.2	329.6	2,207.1
4380+65.04	TO	4396+79.61	1,614.6	24.0	16.0	64,582.8	7,175.9	3,229.1	482.2	241.1	1,614.6
4396+79.61	TO	4407+70.37	1,090.8	24.0	16.0	43,630.4	4,847.8	2,181.5	325.8	162.9	1,090.8
4407+70.37	TO	4423+07.35	1,537.0	24.0	16.0	61,479.2	6,831.0	3,074.0	459.0	229.5	1,537.0
4423+07.35	TO	4433+44.94	1,037.6	24.0	16.0	41,503.6	4,611.5	2,075.2	309.9	154.9	1,037.6
4433+44.94	TO	4478+83.85	4,538.9	24.0	16.0	181,556.4	20,172.9	9,077.8	1,355.6	677.8	4,538.9
4478+83.85	TO	4490+71.91	1,188.1	24.0	16.0	47,522.4	5,280.3	2,376.1	354.8	177.4	1,188.1
4490+71.91	TO	4511+19.70	2,047.8	24.0	16.0	114,676.2	12,741.8	5,733.8	611.6	305.8	2,047.8
4511+19.70	TO	4536+55.92	2,536.2	24.0	16.0	101,448.8	11,272.1	5,072.4	757.5	378.7	2,536.2
4536+55.92	TO	4588+43.69	5,187.8	24.0	16.0	1,707.7	189.7	85.4	1,549.4	774.7	5,187.8
4588+43.69	TO	4616+90.85	2,847.2	24.0	16.0	159,441.0	17,715.7	7,972.0	850.4	425.2	2,847.2
4616+90.85	TO	4617+89.09	98.2	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
4617+89.09	TO	4626+52.89	863.8	24.0	16.0	4,751.0	527.9	237.5	258.0	129.0	863.8
4626+52.89	TO	4632+36.48	583.6	24.0	16.0	32,681.0	3,631.2	1,634.1	174.3	87.1	583.6
EB SHEET TOTALS =						2,295,374	255,042	114,769	19,113	9,458	63,994

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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: NA SHEET 1 OF 12 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	7
CONTRACT NO. 74982				
		ILLINOIS	FED. AID PROJECT	

WB PAVING SCHEDULE

LOCATION			LENGTH	PAVEMENT WIDTH	SHOULDER WIDTH	AREA	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	BITUMINOUS MATERIALS (TACK COAT)	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90	HOT-MIX ASPHALT SHOULDERS	LONGITUDINAL JOINT SEALANT
STATION	TO	STATION	FEET	FEET	FEET	SQ FT	SQ YD	POUND	TON	TON	FOOT
FAI 64 (WB)											
985+37.01	TO	1003+25.60	1,788.6	24.0	16.0	71,543.6	7,949.3	3,577.2	534.2	267.1	1,788.6
1003+25.60	TO	1003+80.02	54.4	24.0	16.0	2,176.8	241.9	108.8	16.3	8.1	54.4
1003+80.02	TO	1020+82.74	1,702.7	24.0	16.0	68,108.8	7,567.6	3,405.4	508.5	254.3	1,702.7
1020+82.74	=	4020+83.94	0.0	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
4020+83.94	TO	4053+43.47	3,259.5	24.0	16.0	130,381.2	14,486.8	6,519.1	973.5	486.8	3,259.5
4053+43.47	=	1053+43.21	0.0	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
1053+43.21	TO	1056+72.20	329.0	24.0	16.0	13,159.6	1,462.2	658.0	98.3	49.1	329.0
1056+72.20	TO	1057+74.78	102.6	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
1057+74.78	TO	1098+44.65	4,069.9	24.0	16.0	162,794.8	18,088.3	8,139.7	1,215.5	607.8	4,069.9
1098+44.65	TO	1103+61.62	517.0	24.0	16.0	20,678.8	2,297.6	1,033.9	154.4	77.2	517.0
1103+61.62	TO	1119+50.52	1,588.9	24.0	16.0	63,556.0	7,061.8	3,177.8	474.6	237.3	1,588.9
1119+50.52	TO	1123+52.95	402.4	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
1123+52.95	TO	1127+27.93	375.0	24.0	16.0	14,999.2	1,666.6	750.0	112.0	56.0	375.0
1127+27.93	TO	1135+82.34	854.4	24.0	16.0	34,176.4	3,797.4	1,708.8	255.2	127.6	854.4
1135+82.34	TO	1173+17.21	3,734.9	24.0	16.0	149,394.8	16,599.4	7,469.7	1,115.5	557.7	3,734.9
1173+17.21	TO	1177+72.59	455.4	24.0	16.0	18,215.2	2,023.9	910.8	136.0	68.0	455.4
1177+72.59	TO	1209+05.88	3,133.3	24.0	16.0	125,331.6	13,925.7	6,266.6	935.8	467.9	3,133.3
1209+05.88	TO	1220+55.52	1,149.6	24.0	16.0	45,985.6	5,109.5	2,299.3	343.4	171.7	1,149.6
1220+55.52	=	4214+94.08	0.0	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
4214+94.08	TO	4228+81.14	1,387.1	24.0	16.0	55,482.4	6,164.7	2,774.1	414.3	207.1	1,387.1
4228+81.14	TO	4229+82.06	100.9	24.0	16.0	4,036.8	448.5	201.8	30.1	15.1	100.9
4229+82.06	TO	4233+07.94	325.9	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
4233+07.94	TO	4256+75.34	2,367.4	24.0	16.0	94,696.0	10,521.8	4,734.8	707.1	353.5	2,367.4
4256+75.34	=	4256+75.33	0.0	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
4256+75.33	TO	4283+44.16	2,668.8	24.0	16.0	106,753.2	11,861.5	5,337.7	797.1	398.5	2,668.8
4283+44.16	TO	4290+89.80	745.6	24.0	16.0	29,825.6	3,314.0	1,491.3	222.7	111.3	745.6
4290+89.80	TO	4291+90.69	100.9	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
4291+90.69	TO	4309+91.28	1,800.6	24.0	16.0	72,023.6	8,002.6	3,601.2	537.8	268.9	1,800.6
4309+91.28	TO	4311+41.14	149.9	24.0	6.0	4,495.9	499.5	224.8	44.8	8.4	149.9
4311+41.14	TO	4316+26.53	485.4	24.0	6.0	14,561.6	1,618.0	728.1	145.0	27.2	485.4
4316+26.53	TO	4316+38.39	11.9	24.0	6.0	355.8	39.5	17.8	3.5	0.7	11.9
4316+38.39	TO	4317+70.53	132.1	24.0	6.0	0.0	0.0	0.0	0.0	0.0	0.0
4317+70.53	TO	4319+90.48	219.9	24.0	6.0	6,598.5	733.2	329.9	65.7	12.3	219.9
4319+90.48	TO	4323+13.01	322.5	24.0	17.0	13,223.7	1,469.3	661.2	96.3	51.2	322.5
4323+13.01	TO	4334+09.62	1,096.6	24.0	16.0	43,864.4	4,873.8	2,193.2	327.5	163.8	1,096.6
4334+09.62	TO	4335+59.62	150.0	24.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0

WB PAVING SCHEDULE CONT'D

LOCATION			LENGTH	PAVEMENT WIDTH	SHOULDER WIDTH	AREA	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	BITUMINOUS MATERIALS (TACK COAT)	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90	HOT-MIX ASPHALT SHOULDERS	LONGITUDINAL JOINT SEALANT
STATION	TO	STATION	FEET	FEET	FEET	SQ FT	SQ YD	POUND	TON	TON	FOOT
FAI 64 (WB)											
4335+59.62	TO	4348+65.58	1,306.0	24.0	16.0	52,238.4	5,804.3	2,611.9	390.0	195.0	1,306.0
4348+65.58	TO	4351+85.99	320.4	24.0	18.5	13,617.4	1,513.0	680.9	95.7	55.3	320.4
4351+85.99	TO	4355+45.76	359.8	24.0	6.0	10,793.1	1,199.2	539.7	107.5	20.1	359.8
4355+45.76	TO	4358+24.93	279.2	24.0	16.0	11,166.8	1,240.8	558.3	83.4	41.7	279.2
4358+24.93	TO	4380+65.04	2,240.1	24.0	16.0	89,604.4	9,956.0	4,480.2	669.0	334.5	2,240.1
4380+65.04	TO	4396+79.61	1,614.6	24.0	16.0	64,582.8	7,175.9	3,229.1	482.2	241.1	1,614.6
4396+79.61	TO	4407+70.37	1,090.8	24.0	16.0	43,630.4	4,847.8	2,181.5	325.8	162.9	1,090.8
4407+70.37	TO	4423+07.35	1,537.0	24.0	16.0	61,479.2	6,831.0	3,074.0	459.0	229.5	1,537.0
4423+07.35	TO	4433+44.94	1,037.6	24.0	16.0	41,503.6	4,611.5	2,075.2	309.9	154.9	1,037.6
4433+44.94	TO	4478+83.85	4,538.9	24.0	16.0	181,556.4	20,172.9	9,077.8	1,355.6	677.8	4,538.9
4478+83.85	TO	4490+71.91	1,188.1	24.0	16.0	47,522.4	5,280.3	2,376.1	354.8	177.4	1,188.1
4490+71.91	TO	4511+16.70	2,044.8	24.0	16.0	81,791.6	9,088.0	4,089.6	610.7	305.4	2,044.8
4511+16.70	TO	4536+55.92	2,539.2	24.0	16.0	101,568.8	11,285.4	5,078.4	758.4	379.2	2,539.2
4536+55.92	TO	4588+43.69	5,187.8	24.0	16.0	207,510.8	23,056.8	10,375.5	1,549.4	774.7	5,187.8
4588+43.69	TO	4594+18.43	574.7	24.0	16.0	22,989.6	2,554.4	1,149.5	171.7	85.8	574.7
4594+18.43	TO	4618+53.59	2,435.2	24.0	16.0	97,406.4	10,822.9	4,870.3	727.3	363.7	2,435.2
4618+53.59	TO	4619+56.25	102.7	0.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
4619+56.25	TO	4621+67.82	211.6	0.0	16.0	3,385.1	376.1	169.3	0.0	31.6	0.0
4621+67.82	TO	4623+54.35	186.5	0.0	16.0	0.0	0.0	0.0	0.0	0.0	0.0
4623+54.35	TO	4638+78.25	1,523.9	24.0	16.0	60,956.0	6,772.9	3,047.8	455.1	227.6	1,523.9
WB SHEET TOTALS =						2,559,723	266,442	119,899	19,171	9,513	60,228

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PROJECT: 74982\CADD\DATA\CAD\DWG\74982-sch-schedule.dwg

USER NAME = steffemk	DESIGNED -	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 8/12/2020	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES			
SCALE:	SHEET 2	OF 12 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	8
CONTRACT NO. 74982				
ILLINOIS FED. AID PROJECT				

RAMP PAVING SCHEDULE

LOCATION		LENGTH	PAVEMENT WIDTH	SHOULDER WIDTH	AREA	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	BITUMINOUS MATERIALS (TACK COAT)	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90	HOT-MIX ASPHALT SHOULDERS	LONGITUDINAL JOINT SEALANT
STATION TO	STATION	FEET	FEET	FEET	SQ FT	SQ YD	SQ YD	POUND	TON	TON	FOOT
Ramp "A"											
100+23.05	TO 101+60.40	137.4	Variable	8.0	6,343.5	0.0	704.8	317.2	65.3	13.2	0.0
101+60.40	TO 114+97.90	1,337.5	16.0	10.0	34,775.0	0.0	3,863.9	1,738.8	266.3	166.4	0.0
114+97.90	TO 115+61.80	63.9	16.0	6.0	1,405.8	0.0	156.2	70.3	12.7	4.8	0.0
115+61.80	TO 116+17.21	55.4	15.0	6.0	1,163.6	0.0	129.3	58.2	10.3	4.1	0.0
116+17.21	= 0+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
0+00.00	TO 2+00.80	200.8	14.0	6.0	4,016.0	0.0	446.2	200.8	35.0	15.0	0.0
2+00.80	TO 4+27.14	226.3	16.0	6.0	4,979.5	0.0	553.3	249.0	45.1	16.9	0.0
4+27.14	TO 5+70.40	143.3	12.0	6.0	2,578.7	0.0	286.5	128.9	21.4	10.7	0.0
5+70.40	TO 10+49.93	479.5	5.8	6.0	5,658.5	0.0	628.7	282.9	34.6	35.8	0.0
Ramp "B"											
0+00.00	TO 3+59.40	359.4	10.5	6.0	5,930.1	0.0	658.9	296.5	47.0	26.8	0.0
3+59.40	TO 6+62.40	303.0	16.0	6.0	6,666.0	0.0	740.7	333.3	60.3	22.6	0.0
6+62.40	= 200+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
200+00.00	TO 200+17.20	17.2	16.0	6.0	378.4	0.0	42.0	18.9	3.4	1.3	0.0
200+17.20	TO 214+00.00	1,382.8	16.0	10.0	35,952.8	0.0	3,994.8	1,797.6	275.3	172.1	0.0
214+00.00	TO 215+21.90	121.9	Variable	8.0	6,156.0	0.0	684.0	307.8	64.5	15.0	0.0
Ramp "C"											
0+00.00	TO 0+38.80	38.8	2.0	10.0	465.6	0.0	51.7	23.3	1.0	4.8	0.0
0+38.80	TO 1+85.00	146.2	7.0	6.0	1,900.6	0.0	211.2	95.0	12.7	10.9	0.0
1+85.00	TO 3+59.40	174.4	13.5	6.0	3,400.8	0.0	377.9	170.0	29.3	13.0	0.0
3+59.40	TO 5+29.90	170.5	16.0	6.0	3,751.0	0.0	416.8	187.6	33.9	12.7	0.0
5+29.90	TO 6+62.40	132.5	16.0	10.0	3,445.0	0.0	382.8	172.3	26.4	16.5	0.0
6+62.40	= 300+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
300+00.00	TO 311+56.60	1,156.6	16.0	10.0	30,071.6	0.0	3,341.3	1,503.6	230.3	143.9	0.0
311+56.60	TO 312+75.60	119.0	Variable	8.0	6,059.1	0.0	673.2	303.0	63.6	14.9	0.0
Ramp "D"											
400+22.60	TO 401+42.50	119.9	Variable	8.0	5,949.6	0.0	661.1	297.5	62.1	13.0	0.0
401+42.50	TO 401+61.30	18.8	17.0	10.0	507.6	0.0	56.4	25.4	4.0	2.3	0.0
401+61.30	TO 415+94.00	1,432.7	16.0	10.0	37,250.2	0.0	4,138.9	1,862.5	285.3	178.3	0.0
415+94.00	TO 416+36.60	42.6	16.0	6.0	937.2	0.0	104.1	46.9	8.5	3.2	0.0
416+36.60	TO 417+13.25	76.7	15.0	6.0	1,609.7	0.0	178.9	80.5	14.3	5.7	0.0
417+13.25	= 0+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
0+00.00	TO 2+03.50	203.5	14.0	6.0	4,070.0	0.0	452.2	203.5	35.5	15.2	0.0
2+03.50	TO 10+49.60	846.1	8.0	6.0	11,845.4	0.0	1,316.2	592.3	84.2	63.2	0.0

MEDIAN CROSS-OVER PAVING SCHEDULE

LOCATION		LENGTH	PAVEMENT WIDTH	SHOULDER WIDTH	AREA	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	BITUMINOUS MATERIALS (TACK COAT)	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90	HOT-MIX ASPHALT SHOULDERS	LONGITUDINAL JOINT SEALANT
STATION TO	STATION	FEET	FEET	FEET	SQ FT	SQ YD	SQ YD	POUND	TON	TON	FOOT
Median Cross-Overs											
4031+42.00		58.0	22.0	NA	1,619.3	179.9	0.0	81.0	0.0	11.9	0.0
4269+11.00		58.0	22.0	NA	1,469.4	163.3	0.0	73.5	0.0	11.9	0.0
4376+25.00		58.0	22.0	NA	1,469.0	163.2	0.0	73.5	0.0	11.9	0.0
4598+13.70		127.9	22.0	NA	3,011.3	334.6	0.0	150.6	0.0	26.3	0.0
RAMP SHEET TOTALS =											
					234,836	841	25,252	11,742	1,832	1,065	0
WB SHEET TOTALS =											
					2,559,723	284,414	0	127,986	19,171	9,513	64,187
EB SHEET TOTALS =											
					2,295,374	255,042	0	114,769	19,113	9,458	63,994
PROJECT TOTALS =											
					5,089,933	540,296	25,252	254,497	40,116	20,035	128,181

MODEL: Default
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USER NAME = steffemk	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 8/12/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET 3 OF 12 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	9
CONTRACT NO. 74982				
ILLINOIS		FED. AID PROJECT		

EB TEMPORARY RAMP

LOCATION	LENGTH (FEET)	PAVT WIDTH (FEET)	SHLDR WIDTH (FEET)	AREA (SQ FT)	QUANTITY (SQ YD)
I-64 (EB)					
SN 096-0039					
3986+59.62 TO 3986+72.62	13.0	26.5	17.6	573.3	63.7
SN 096-0041					
4056+49.15 TO 4056+62.15	13.0	24.1	16.1	522.6	58.1
4057+65.94 TO 4057+78.94	13.0	24.2	16.1	523.9	58.2
SN 096-0043					
4122+02.62 TO 4122+15.51	13.0	29.2	19.4	631.8	70.2
4126+01.85 TO 4126+17.74	13.0	27.8	18.6	603.2	67.0
SN 096-0045					
4229+74.42 TO 4229+87.42	13.0	24.1	16.0	521.3	57.9
4233+15.59 TO 4233+28.59	13.0	24.1	16.0	521.3	57.9
SN 096-0047					
4291+02.97 TO 4291+15.86	13.0	24.9	16.6	539.5	59.9
4292+14.99 TO 4292+27.88	13.0	24.8	16.5	536.9	59.7
SN 096-0049					
4316+38.25 TO 4316+51.27	13.0	24.8	16.5	537.7	59.7
4317+95.25 TO 4318+08.26	13.0	24.8	24.4	640.1	71.1
SN 096-0059					
4333+81.74 TO 4333+94.74	13.0	24.2	16.2	525.2	58.4
4335+44.74 TO 4335+57.74	13.0	24.2	16.2	525.2	58.4
SN 096-0051					
4616+77.74 TO 4616+90.85	13.0	24.3	16.2	526.5	58.5
4617+89.09 TO 4618+02.20	13.0	24.3	16.2	526.5	58.5
END OF PROJECT					
4632+23.48 TO 4632+36.48	13.0	24.0	16.0	520.0	57.8
EB TOTAL =					975

WB TEMPORARY RAMP

LOCATION	LENGTH (FEET)	PAVT WIDTH (FEET)	SHLDR WIDTH (FEET)	AREA (SQ FT)	QUANTITY (SQ YD)
I-64 (WB)					
SN 096-0040					
985+37.01 TO 985+50.01	13.0	26.5	17.7	574.6	63.8
SN 096-0042					
1056+59.25 TO 1056+72.20	13.0	24.2	16.1	523.9	58.2
1057+74.78 TO 1057+87.73	13.0	24.2	16.1	523.9	58.2
SN 096-0044					
1119+37.38 TO 1119+50.52	13.0	30.4	20.3	659.1	73.2
1123+52.95 TO 1123+66.09	13.0	28.5	19.0	617.5	68.6
SN 096-0046					
4229+69.15 TO 4229+82.10	13.0	24.1	16.0	521.3	57.9
4233+07.94 TO 4233+20.85	13.0	24.1	16.0	521.3	57.9
SN 096-0048					
4290+76.69 TO 4290+89.80	13.0	24.9	16.6	539.5	59.9
4291+90.69 TO 4292+03.80	13.0	24.8	16.5	536.9	59.7
SN 096-0050					
4316+13.53 TO 4316+26.53	13.0	24.8	23.1	622.7	69.2
4317+70.53 TO 4317+83.53	13.0	24.8	26.5	666.9	74.1
SN 096-0053					
4333+96.62 TO 4334+09.62	13.0	24.2	16.2	525.2	58.4
4335+59.62 TO 4335+72.62	13.0	24.2	16.2	525.2	58.4
SN 096-0052					
4618+40.70 TO 4618+53.59	13.0	25.4	16.9	549.9	61.1
SN 096-0060					
4623+54.35 TO 4623+67.24	13.0	41.9	27.9	907.4	100.8
END OF PROJECT					
4638+65.25 TO 4638+78.25	13.0	24.0	16.0	520.0	57.8
WB TOTAL =					1,037

RAMP TEMPORARY RAMP

LOCATION	LENGTH (FEET)	PAVT WIDTH (FEET)	SHLDR WIDTH (FEET)	AREA (SQ FT)	QUANTITY (SQ YD)
RAMP "A"					
100+23.05 TO 100+30.05	7.0	117.9	NA	825.30	91.7
RAMP "B"					
215+14.92 TO 215+21.92	7.0	140.4	NA	982.80	109.2
RAMP "C"					
312+68.38 TO 312+75.62	7.0	147.8	NA	1,034.6	115.0
RAMP "D"					
400+22.63 TO 400+29.63	7.0	99.4	NA	695.8	77.3
RAMP TOTAL =					393.2
WB TOTAL =					1,037.3
EB TOTAL =					975.0
PROJECT TOTAL =					2,405

CLASS D PATCHES, TYPE III, 11 "

LOCATION	LENGTH (FEET)	PAVT WIDTH (FEET)	SHLDR WIDTH (FEET)	AREA (SQ FT)	QUANTITY (SQ YD)
I-64 (EB)					
4055+80.80 TO 4055+86.80	6.0	24	NA	144.0	16.0
4058+41.30 TO 4058+47.30	6.0	24	NA	144.0	16.0
4120+35.10 TO 4120+41.10	6.0	24	NA	144.0	16.0
4126+76.20 TO 4126+82.20	6.0	24	NA	144.0	16.0
4229+05.60 TO 4229+11.60	6.0	24	NA	144.0	16.0
4233+89.80 TO 4233+95.80	6.0	24	NA	144.0	16.0
I-64 (WB)					
1055+91.50 TO 1055+97.50	6.0	24	NA	144.0	16.0
1058+49.50 TO 1058+55.50	6.0	24	NA	144.0	16.0
1116+68.70 TO 1116+74.70	6.0	24	NA	144.0	16.0
1124+28.70 TO 1124+34.70	6.0	24	NA	144.0	16.0
4229+01.30 TO 4229+07.30	6.0	24	NA	144.0	16.0
4233+82.70 TO 4233+88.70	6.0	24	NA	144.0	16.0
SHEET TOTAL =					192.0

MODEL: Default
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USER NAME = steffenmk	DESIGNED -	REVISED -
DRAWN -	REVISOR -	
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 8/12/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET 4 OF 12 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	10
CONTRACT NO. 74982				
ILLINOIS		FED. AID PROJECT		

EB LONGITUDINAL JOINT SEALANT

LOCATION		LENGTH (FEET)	TOTAL QUANTITY (FEET)
I-64 (EB)			
3986+59.62	TO 3994+98.13	838.5	838.51
3994+98.13	TO 4003+77.00	878.9	878.87
4003+77.00	TO 4012+55.65	878.7	878.65
4012+55.65	TO 4020+83.94	828.3	828.29
4020+83.94	TO 4053+43.47	3,259.5	3,259.53
4053+43.47	TO 4053+61.42	18.0	17.95
4053+61.42	TO 4056+62.23	300.8	300.81
4056+62.23	TO 4057+65.86	103.6	0.00
4057+65.86	TO 4084+25.15	2,659.3	2,659.29
4084+25.15	TO 4099+59.02	1,533.9	1,533.87
4099+59.02	TO 4122+15.51	2,256.5	2,256.49
4122+15.51	TO 4126+01.85	386.3	0.00
4126+01.85	TO 4129+51.71	349.9	349.86
4129+51.71	TO 4138+48.85	897.1	897.14
4138+48.85	TO 4167+97.09	2,948.2	2,948.24
4167+97.09	TO 4176+98.57	901.5	901.48
4176+98.57	TO 4209+69.37	3,270.8	3,270.80
4209+69.37	TO 4221+08.50	1,139.1	1,139.13
4221+08.50	= 4214+81.00	0.0	0.00
4214+81.00	TO 4229+87.42	1,506.4	1,506.42
4229+87.42	TO 4233+15.59	328.2	0.00
4233+15.59	TO 4235+66.87	251.3	251.28
4235+66.87	TO 4256+42.12	2,075.3	2,075.25
4256+42.12	TO 4256+75.34	33.2	33.22
4256+75.34	TO 4283+44.16	2,668.8	2,668.82
4283+44.16	TO 4309+91.28	2,647.1	2,647.12
4309+91.28	TO 4316+16.54	625.3	625.26
4316+16.54	TO 4316+51.27	34.7	34.73
4316+51.27	TO 4317+95.25	144.0	0.00
4317+95.25	TO 4319+76.31	181.1	181.06
4319+76.31	TO 4321+46.72	170.4	170.41
4321+46.72	TO 4333+94.74	1,248.0	1,248.02
4333+94.74	TO 4335+44.74	150.0	0.00
4335+44.74	TO 4346+86.19	1,141.5	1,141.45
4346+86.19	TO 4350+11.37	325.2	325.18
4350+11.37	TO 4358+24.93	813.6	813.56
4358+24.93	TO 4358+57.95	33.0	33.02
4358+57.95	TO 4380+65.04	2,207.1	2,207.09
4380+65.04	TO 4396+79.61	1,614.6	1,614.57

EB LONGITUDINAL JOINT SEALANT CONT'D

LOCATION		LENGTH (FEET)	TOTAL QUANTITY (FEET)
I-64 (EB)			
4396+79.61	TO 4407+70.37	1,090.8	1,090.76
4407+70.37	TO 4423+07.35	1,537.0	1,536.98
4423+07.35	TO 4433+44.94	1,037.6	1,037.59
4433+44.94	TO 4478+83.85	4,538.9	4,538.91
4478+83.85	TO 4490+71.91	1,188.1	1,188.06
4490+71.91	TO 4511+19.70	2,047.8	2,047.79
4511+19.70	TO 4536+55.92	2,536.2	2,536.22
4536+55.92	TO 4588+43.69	5,187.8	5,187.77
4588+43.69	TO 4616+90.85	2,847.2	2,847.16
4616+90.85	TO 4617+89.09	98.2	0.00
4617+89.09	TO 4626+52.89	863.8	863.80
4626+52.89	TO 4632+36.48	583.6	583.59
SHEET TOTAL =			63,994.0

WB LONGITUDINAL JOINT SEALANT

LOCATION		LENGTH (FEET)	TOTAL QUANTITY (FEET)
I-64 (WB)			
985+37.01	TO 1003+25.60	1,788.6	1,788.59
1003+25.60	TO 1003+80.02	54.4	54.42
1003+80.02	TO 1020+82.74	1,702.7	1,702.72
1020+82.74	= 4020+83.94	0.0	0.00
4020+83.94	TO 4053+43.47	3,259.5	3,259.53
4053+43.47	= 1053+43.21	0.0	0.00
1053+43.21	TO 1056+72.20	329.0	328.99
1056+72.20	TO 1057+74.78	102.6	0.00
1057+74.78	TO 1098+44.65	4,069.9	4,069.87
1098+44.65	TO 1103+61.62	517.0	516.97
1103+61.62	TO 1119+50.52	1,588.9	1,588.90
1119+50.52	TO 1123+52.95	402.4	0.00
1123+52.95	TO 1127+27.93	375.0	374.98
1127+27.93	TO 1135+82.34	854.4	854.41
1135+82.34	TO 1173+17.21	3,734.9	3,734.87
1173+17.21	TO 1177+72.59	455.4	455.38
1177+72.59	TO 1209+05.88	3,133.3	3,133.29
1209+05.88	TO 1220+55.52	1,149.6	1,149.64
1220+55.52	= 4214+94.08	0.0	0.00
4214+94.08	TO 4228+81.14	1,387.1	1,387.06
4228+81.14	TO 4229+82.06	100.9	100.92
4229+82.06	TO 4233+07.94	325.9	0.00
4233+07.94	TO 4256+75.34	2,367.4	2,367.40
4256+75.34	= 4256+75.33	0.0	-0.01
4256+75.33	TO 4283+44.16	2,668.8	2,668.83
4283+44.16	TO 4290+89.80	745.6	745.64
4290+89.80	TO 4291+90.69	100.9	0.00
4291+90.69	TO 4309+91.28	1,800.6	1,800.59
4309+91.28	TO 4311+41.14	149.9	149.86
4311+41.14	TO 4316+26.53	485.4	485.39
4316+26.53	TO 4316+38.39	11.9	11.86
4316+38.39	TO 4317+70.53	132.1	0.00
4317+70.53	TO 4319+90.48	219.9	219.95
4319+90.48	TO 4323+13.01	322.5	322.53
4323+13.01	TO 4334+09.62	1,096.6	1,096.61
4334+09.62	TO 4335+59.62	150.0	0.00
4335+59.62	TO 4348+65.58	1,306.0	1,305.96
4348+65.58	TO 4351+85.99	320.4	320.41
4351+85.99	TO 4355+45.76	359.8	359.77

WB LONGITUDINAL JOINT SEALANT CONT'D

LOCATION		LENGTH (FEET)	TOTAL QUANTITY (FEET)
I-64 (WB)			
4355+45.76	TO 4358+24.93	279.2	279.17
4358+24.93	TO 4380+65.04	2,240.1	2,240.11
4380+65.04	TO 4396+79.61	1,614.6	1,614.57
4396+79.61	TO 4407+70.37	1,090.8	1,090.76
4407+70.37	TO 4423+07.35	1,537.0	1,536.98
4423+07.35	TO 4433+44.94	1,037.6	1,037.59
4433+44.94	TO 4478+83.85	4,538.9	4,538.91
4478+83.85	TO 4490+71.91	1,188.1	1,188.06
4490+71.91	TO 4511+16.70	2,044.8	2,044.79
4511+16.70	TO 4536+55.92	2,539.2	2,539.22
4536+55.92	TO 4588+43.69	5,187.8	5,187.77
4588+43.69	TO 4594+18.43	574.7	574.74
4594+18.43	TO 4618+53.59	2,435.2	2,435.16
4618+53.59	TO 4619+56.25	102.7	0.00
4619+56.25	TO 4621+67.82	211.6	0.00
4621+67.82	TO 4623+54.35	186.5	0.00
4623+54.35	TO 4638+78.25	1,523.9	1,523.90
SHEET TOTAL =			64,187.1
EB SHEET TOTAL =			63,994.0
PROJECT TOTAL =			128,181

MODEL: Default
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USER NAME = steffemk	DESIGNED -	REVISED -
PLOT SCALE = 100.0000 ' / in.	DRAWN -	REVISED -
PLOT DATE = 8/12/2020	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES			
SCALE:	SHEET 5	OF 12 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	11
CONTRACT NO. 74982				
		ILLINOIS	FED. AID PROJECT	

EB PAVEMENT MARKING SCHEDULE

LOCATION			LENGTH	SHORT-TERM PAVEMENT MARKING (WHITE SKIP-DASH)	SHORT-TERM PAVEMENT MARKING (WHITE SHOULDER SKIP)	SHORT-TERM PAVEMENT MARKING (YELLOW SHOULDER SKIP)	SHORT-TERM PAVEMENT MARKING REMOVAL	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	TEMPORARY PAVEMENT MARKING LINE-6" (SKIP-DASH)	TEMPORARY PAVEMENT MARKING LINE-6" (WHITE DOTTED)	TEMPORARY PAVEMENT MARKING LINE-6" (WHITE)	TEMPORARY PAVEMENT MARKING LINE-6" (YELLOW)	TEMPORARY PAVEMENT MARKING LINE-8" (WHITE)	TEMPORARY PAVEMENT MARKING LINE-12" (WHITE)	TEMPORARY PAVEMENT MARKING LINE-24" (WHITE)	THERMOPLASTIC PAVEMENT MARKING-LETTERS AND SYMBOLS	THERMOPLASTIC PAVEMENT MARKING LINE-6" (WHITE DOTTED)	THERMOPLASTIC PAVEMENT MARKING LINE-6" (WHITE)	THERMOPLASTIC PAVEMENT MARKING LINE-6" (YELLOW)	THERMOPLASTIC PAVEMENT MARKING LINE-6" (WHITE)	THERMOPLASTIC PAVEMENT MARKING LINE-12" (WHITE)	THERMOPLASTIC PAVEMENT MARKING LINE-24" (WHITE)	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID-LINE 6" (SKIP-DASH)	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B-LINE 6" (WHITE DOTTED)	RAISED REFLECTIVE PAVEMENT MARKERS (MONO CRYSTAL)	RAISED REFLECTIVE PAVEMENT MARKERS (MONO AMBER)	PAVEMENT MARKING REMOVAL (WATER BLASTING)	TEMPORARY PAVEMENT MARKING REMOVAL			
STATION	TO	STATION	FEET	FOOT	FOOT	FOOT	SQ FT	SQ FT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	EACH	SQ FT	SQ FT		
FAI 64 (EB)																															
3986+60.55	TO	4020+83.94	3,423.4	684.7	273.9	273.9	205.4	0.0	0.0	0.0	3,423.4	3,423.4	0.0	0.0	0.0	0.0	0.0	3,423.4	3,423.4	0.0	0.0	0.0	855.8	0.0	86	0	0	3,423.4			
4020+83.94	TO	4056+62.23	3,578.3	715.7	286.3	286.3	214.7	0.0	0.0	0.0	3,578.3	3,578.3	0.0	0.0	0.0	0.0	0.0	3,578.3	3,578.3	0.0	0.0	0.0	894.6	0.0	89	0	0	3,578.3			
4056+62.23	TO	4057+65.86	103.6	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	69	0.0			
4057+65.86	TO	4120+15.51	6,249.7	1,249.9	500.0	500.0	374.9	0.0	0.0	0.0	6,249.7	6,249.7	0.0	0.0	0.0	0.0	0.0	6,249.7	6,249.7	0.0	0.0	0.0	1,562.4	0.0	156	0	0	6,249.7			
4120+15.51	TO	4126+01.85	586.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	259	0.0			
4126+01.85	TO	4221+08.50	9,506.7	1,901.3	760.5	760.5	570.3	0.0	0.0	0.0	9,506.7	9,506.7	0.0	0.0	0.0	0.0	0.0	9,506.7	9,506.7	0.0	0.0	0.0	2,376.7	0.0	238	0	0	9,506.7			
4221+08.50	=	4214+81.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	0	0.0			
4214+81.00	TO	4229+87.42	1,506.4	301.3	120.5	120.5	90.4	0.0	0.0	0.0	1,506.4	1,506.4	0.0	0.0	0.0	0.0	0.0	1,506.4	1,506.4	0.0	0.0	0.0	376.6	0.0	38	0	0	1,506.4			
4229+87.42	TO	4233+15.59	328.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	219	0.0			
4233+15.59	TO	4256+75.33	2,359.7	471.9	188.8	188.8	141.6	0.0	0.0	0.0	2,359.7	2,359.7	0.0	0.0	0.0	0.0	0.0	2,359.7	2,359.7	0.0	0.0	0.0	589.9	0.0	59	0	0	2,359.7			
4256+75.33	TO	4291+15.86	3,440.5	688.1	275.2	275.2	206.4	0.0	0.0	0.0	3,440.5	3,440.5	0.0	0.0	0.0	0.0	0.0	3,440.5	3,440.5	0.0	0.0	0.0	860.1	0.0	86	0	0	3,440.5			
4291+15.86	TO	4292+14.99	99.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	67	0.0			
4292+14.99	TO	4316+15.64	2,400.7	480.1	192.1	192.1	144.0	0.0	0.0	0.0	2,400.7	2,400.7	0.0	0.0	0.0	0.0	0.0	2,400.7	2,400.7	0.0	0.0	0.0	600.2	0.0	60	0	0	2,400.7			
4316+15.64	TO	4316+54.41	38.8	7.8	3.1	3.1	2.3	0.0	0.0	38.8	38.8	38.8	0.0	0.0	0.0	0.0	38.8	0.0	38.8	0.0	0.0	0.0	9.7	0.0	1	0	0	58.2			
4316+54.41	TO	4317+98.41	144.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	144.0	0.0	0.0	0.0	0.0	0.0	0.0	144.0	0.0	0.0	0.0	144.0	0.0	0	0	96	72.0			
4317+98.41	TO	4319+19.64	121.2	24.2	9.7	9.7	7.3	0.0	0.0	121.2	0.0	121.2	0.0	0.0	0.0	0.0	121.2	0.0	121.2	0.0	0.0	0.0	30.3	0.0	3	0	0	121.2			
4319+19.64	TO	4321+96.72	277.1	55.4	22.2	22.2	16.6	0.0	0.0	0.0	0.0	277.1	277.1	0.0	0.0	0.0	0.0	0.0	277.1	277.1	0.0	0.0	69.3	0.0	14	0	0	323.3			
4321+96.72	TO	4333+94.74	1,198.0	239.6	95.8	95.8	71.9	0.0	0.0	0.0	1,198.0	1,198.0	0.0	0.0	0.0	0.0	0.0	1,198.0	1,198.0	0.0	0.0	0.0	299.5	0.0	30	0	0	1,198.0			
4333+94.74	TO	4335+44.74	150.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	100	0.0			
4335+44.74	TO	4346+36.19	1,091.5	218.3	87.3	87.3	65.5	0.0	0.0	0.0	1,091.5	1,091.5	0.0	0.0	0.0	0.0	0.0	1,091.5	1,091.5	0.0	0.0	0.0	272.9	0.0	27	0	0	1,091.5			
4346+36.19	TO	4351+07.87	471.7	94.3	37.7	37.7	28.3	0.0	0.0	0.0	0.0	471.7	471.7	0.0	0.0	0.0	0.0	0.0	471.7	471.7	0.0	0.0	117.9	0.0	12	0	0	550.3			
4351+07.87	TO	4358+57.95	750.1	150.0	60.0	60.0	45.0	0.0	0.0	750.1	0.0	750.1	0.0	0.0	0.0	0.0	750.1	0.0	750.1	0.0	0.0	0.0	187.5	0.0	19	0	0	750.1			
4358+57.95	TO	4407+70.37	4,912.4	982.5	393.0	393.0	294.7	0.0	0.0	0.0	4,912.4	4,912.4	0.0	0.0	0.0	0.0	0.0	4,912.4	4,912.4	0.0	0.0	0.0	1,228.1	0.0	123	0	0	4,912.4			
4407+70.37	TO	4463+02.59	5,532.2	1,106.4	442.6	442.6	331.9	0.0	0.0	0.0	5,532.2	5,532.2	0.0	0.0	0.0	0.0	0.0	5,532.2	5,532.2	0.0	0.0	0.0	1,383.1	0.0	138	0	0	5,532.2			
4463+02.59	TO	4511+16.70	4,814.1	962.8	385.1	385.1	288.8	0.0	0.0	0.0	4,814.1	4,814.1	0.0	0.0	0.0	0.0	0.0	4,814.1	4,814.1	0.0	0.0	0.0	1,203.5	0.0	120	0	0	4,814.1			
4511+16.70	TO	4553+07.84	4,191.1	838.2	335.3	335.3	251.4	0.0	0.0	0.0	4,191.1	4,191.1	0.0	0.0	0.0	0.0	0.0	4,191.1	4,191.1	0.0	0.0	0.0	1,047.8	0.0	105	0	0	4,191.1			
4553+07.84	TO	4588+43.69	3,535.8	707.2	282.9	282.9	212.1	0.0	0.0	0.0	3,535.8	3,535.8	0.0	0.0	0.0	0.0	0.0	3,535.8	3,535.8	0.0	0.0	0.0	884.0	0.0	88	0	0	3,535.8			
4588+43.69	TO	4616+90.85	2,847.2	569.4	227.8	227.8	170.8	0.0	0.0	0.0	2,847.2	2,847.2	0.0	0.0	0.0	0.0	0.0	2,847.2	2,847.2	0.0	0.0	0.0	711.8	0.0	71	0	0	2,847.2			
4616+90.85	TO	4617+89.09	98.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	65	0.0			
4617+89.09	TO	4632+36.48	1,447.4	289.5	115.8	115.8	86.8	0.0	0.0	0.0	1,447.4	1,447.4	0.0	0.0	0.0	0.0	0.0	1,447.4	1,447.4	0.0	0.0	0.0	361.8	0.0	36	0	0	1,447.4			
SHEET TOTALS =																															
				12739	5096	5096	3821	0	0	910	62074	63838	749	0	0	0	910	62035	63838	749	0	0	15923	144	1599	0	874	63910			

MODEL: Default
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USER NAME = steffenk	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -						64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	13
PLOT DATE = 8/12/2020	DATE -	REVISED -		CONTRACT NO. 74982				ILLINOIS	FED. AID PROJECT			
				SCALE:	SHEET 7	OF 12 SHEETS	STA.	TO STA.				

WB PAVEMENT MARKING SCHEDULE

LOCATION			LENGTH	SHORT-TERM PAVEMENT MARKING (WHITE SKIP-DASH)	SHORT-TERM PAVEMENT MARKING (WHITE SHOULDER SKIP)	SHORT-TERM PAVEMENT MARKING (YELLOW SHOULDER SKIP)	SHORT-TERM PAVEMENT MARKING REMOVAL	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	TEMPORARY PAVEMENT MARKING LINE-6" (SKIP-DASH)	TEMPORARY PAVEMENT MARKING LINE-6" (WHITE DOTTED)	TEMPORARY PAVEMENT MARKING LINE-6" (WHITE)	TEMPORARY PAVEMENT MARKING LINE-6" (YELLOW)	TEMPORARY PAVEMENT MARKING LINE-8" (WHITE)	TEMPORARY PAVEMENT MARKING LINE-12" (WHITE)	TEMPORARY PAVEMENT MARKING LINE-24" (WHITE)	THERMOPLASTIC PAVEMENT MARKING-LETTERS AND SYMBOLS	THERMOPLASTIC PAVEMENT MARKING LINE-6" (WHITE DOTTED)	THERMOPLASTIC PAVEMENT MARKING LINE-6" (WHITE)	THERMOPLASTIC PAVEMENT MARKING LINE-6" (YELLOW)	THERMOPLASTIC PAVEMENT MARKING LINE-8" (WHITE)	THERMOPLASTIC PAVEMENT MARKING LINE-12" (WHITE)	THERMOPLASTIC PAVEMENT MARKING LINE-24" (WHITE)	PERFORMED PLASSTIC PAVEMENT MARING, TYPE B-INLAID-LINE 6" (SKIP-DASH)	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B-LINE 6" (WHITE DOTTED)	RAISED REFLECTIVE PAVEMENT MARKERS (MONO CRYSTAL)	RAISED REFLECTIVE PAVEMENT MARKERS (MONO AMBER)	PAVEMENT MARKING REMOVAL (WATER BLASTING)	TEMPORARY PAVEMENT MARKING REMOVAL			
STATION	TO	STATION	FEET	FOOT	FOOT	FOOT	SQ FT	SQ FT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	EACH	SQ FT	SQ FT			
FDAI 64 (WB)																															
985+37.01	TO	1020+82.74	3,545.7	709.1	283.7	283.7	212.7	0.0	0.0	0.0	3,545.7	3,545.7	0.0	0.0	0.0	0.0	0.0	3,545.7	3,545.7	0.0	0.0	0.0	886.4	0.0	88.6	0.0	0.0	3,545.7			
1020+82.74	=	4020+83.94	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
4020+83.94	TO	4053+43.47	3,259.5	651.9	260.8	260.8	195.6	0.0	0.0	0.0	3,259.5	3,259.5	0.0	0.0	0.0	0.0	0.0	3,259.5	3,259.5	0.0	0.0	0.0	814.9	0.0	81.5	0.0	0.0	3,259.5			
4053+43.47	=	1053+43.21	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
1053+43.21	TO	1056+72.20	329.0	65.8	26.3	26.3	19.7	0.0	0.0	0.0	329.0	329.0	0.0	0.0	0.0	0.0	0.0	329.0	329.0	0.0	0.0	0.0	82.2	0.0	8.2	0.0	0.0	329.0			
1056+72.20	TO	1057+74.78	102.6	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	68.7	0.0			
1057+74.78	TO	1119+50.52	6,175.7	1,235.1	494.1	494.1	370.5	0.0	0.0	0.0	6,175.7	6,175.7	0.0	0.0	0.0	0.0	0.0	6,175.7	6,175.7	0.0	0.0	0.0	1,543.9	0.0	154.4	0.0	0.0	6,175.7			
1119+50.52	TO	1123+52.95	402.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	265.2	0.0			
1123+52.95	TO	1220+55.52	9,702.6	1,940.5	776.2	776.2	582.1	0.0	0.0	0.0	9,702.6	9,702.6	0.0	0.0	0.0	0.0	0.0	9,702.6	9,702.6	0.0	0.0	0.0	2,425.6	0.0	242.6	0.0	0.0	9,702.6			
1220+55.52	=	4214+94.08	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
4214+94.08	TO	4229+82.06	1,488.0	297.6	119.0	119.0	89.3	0.0	0.0	0.0	1,488.0	1,488.0	0.0	0.0	0.0	0.0	0.0	1,488.0	1,488.0	0.0	0.0	0.0	372.0	0.0	37.2	0.0	0.0	1,488.0			
4229+82.06	TO	4233+07.94	325.9	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	218.5	0.0			
4233+07.94	TO	4256+75.34	2,367.4	473.5	189.4	189.4	142.0	0.0	0.0	0.0	2,367.4	2,367.4	0.0	0.0	0.0	0.0	0.0	2,367.4	2,367.4	0.0	0.0	0.0	591.9	0.0	59.2	0.0	0.0	2,367.4			
4256+75.34	=	4256+75.33	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
4256+75.33	TO	4290+89.73	3,414.4	682.9	273.2	273.2	204.8	0.0	0.0	0.0	3,414.4	3,414.4	0.0	0.0	0.0	0.0	0.0	3,414.4	3,414.4	0.0	0.0	0.0	853.6	0.0	85.4	0.0	0.0	3,414.4			
4290+89.73	TO	4291+90.69	101.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	66.7	0.0			
4291+90.69	TO	4311+41.14	1,950.5	390.1	156.0	156.0	117.0	0.0	0.0	0.0	1,950.5	1,950.5	0.0	0.0	0.0	0.0	0.0	1,950.5	1,950.5	0.0	0.0	0.0	487.6	0.0	48.8	0.0	0.0	1,950.5			
4311+41.14	TO	4316+23.37	482.2	96.4	38.6	38.6	28.9	0.0	0.0	482.2	0.0	482.2	0.0	0.0	0.0	0.0	482.2	0.0	482.2	0.0	0.0	0.0	120.6	0.0	12.1	0.0	0.0	805.8			
4316+23.37	TO	4317+67.37	144.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	144.0	0.0	0.0	95.9	0.0			
4317+67.37	TO	4318+91.20	123.8	24.8	9.9	9.9	7.4	0.0	0.0	123.8	0.0	123.8	0.0	0.0	0.0	0.0	123.8	0.0	123.8	0.0	0.0	0.0	31.0	0.0	3.1	0.0	0.0	204.3			
4318+91.20	TO	4323+63.00	471.8	94.4	37.7	37.7	28.3	0.0	0.0	0.0	471.8	471.8	0.0	0.0	0.0	0.0	0.0	471.8	471.8	0.0	0.0	0.0	117.9	0.0	11.8	0.0	0.0	235.9			
4323+63.00	TO	4334+09.63	1,046.6	209.3	83.7	83.7	62.8	0.0	0.0	0.0	1,046.6	1,046.6	0.0	0.0	0.0	0.0	0.0	1,046.6	1,046.6	0.0	0.0	0.0	261.7	0.0	26.2	0.0	0.0	1,046.6			
4334+09.63	TO	4335+59.62	150.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	99.9	0.0			
4335+59.62	TO	4348+15.58	1,256.0	251.2	100.5	100.5	75.4	0.0	0.0	0.0	1,256.0	1,256.0	0.0	0.0	0.0	0.0	0.0	1,256.0	1,256.0	0.0	0.0	0.0	314.0	0.0	31.4	0.0	0.0	1,256.0			
4348+15.58	TO	4352+42.67	427.1	85.4	34.2	34.2	25.6	0.0	0.0	0.0	427.1	427.1	0.0	0.0	0.0	0.0	0.0	427.1	427.1	0.0	0.0	0.0	106.8	0.0	20.7	0.0	0.0	213.5			
4352+42.67	TO	4355+45.76	303.1	60.6	24.2	24.2	18.2	0.0	0.0	303.1	0.0	303.1	0.0	0.0	0.0	0.0	303.1	0.0	303.1	0.0	0.0	0.0	75.8	0.0	7.6	0.0	0.0	505.2			
4355+45.76	TO	4407+70.37	5,224.6	1,044.9	418.0	418.0	313.4	0.0	0.0	0.0	5,224.6	5,224.6	0.0	0.0	0.0	0.0	0.0	5,224.6	5,224.6	0.0	0.0	0.0	1,306.2	0.0	130.6	0.0	0.0	5,224.6			
4407+70.37	TO	4463+02.59	5,532.2	1,106.4	442.6	442.6	331.9	0.0	0.0	0.0	5,532.2	5,532.2	0.0	0.0	0.0	0.0	0.0	5,532.2	5,532.2	0.0	0.0	0.0	1,383.1	0.0	138.3	0.0	0.0	5,532.2			
4463+02.59	TO	4511+16.70	4,814.1	962.8	385.1	385.1	288.8	0.0	0.0	0.0	4,814.1	4,814.1	0.0	0.0	0.0	0.0	0.0	4,814.1	4,814.1	0.0	0.0	0.0	1,203.5	0.0	120.4	0.0	0.0	4,814.1			
4511+16.70	TO	4553+07.84	4,191.1	838.2	335.3	335.3	251.4	0.0	0.0	0.0	4,191.1	4,191.1	0.0	0.0	0.0	0.0	0.0	4,191.1	4,191.1	0.0	0.0	0.0	1,047.8	0.0	104.8	0.0	0.0	4,191.1			
4553+07.84	TO	4588+43.69	3,535.8	707.2	282.9	282.9	212.1	0.0	0.0	0.0	3,535.8	3,535.8	0.0	0.0	0.0	0.0	0.0	3,535.8	3,535.8	0.0	0.0	0.0	884.0	0.0	88.4	0.0	0.0	3,535.8			
4588+43.69	TO	4618+53.59	3,009.9	602.0	240.8	240.8	180.6	0.0	0.0	0.0	3,009.9	3,009.9	0.0	0.0	0.0	0.0	0.0	3,009.9	3,009.9	0.0	0.0	0.0	752.5	0.0	75.2	0.0	0.0	3,009.9			
4618+53.59	TO	4623+54.35	500.8	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	336.2	0.0			
4623+54.35	TO	4638+78.25	1,523.9	304.8	121.9	121.9	91.4	0.0	0.0	0.0	1,523.9	1,523.9	0.0	0.0	0.0	0.0	0.0	1,523.9	1,523.9	0.0	0.0	0.0	381.0	0.0	38.1	0.0	0.0	1,523.9			
SHEET TOTALS	=			12,835	5,134	5,134	3,850	0	0	909	62,367	64,175	899	0	0	0	909	62,367	64,175	899	0	0	16,044	144	1,614	0	1,151	64,332			

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USER NAME = steffemk	DESIGNED -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 8/12/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET 8 OF 12 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	14
CONTRACT NO. 74982				
ILLINOIS / FED. AID PROJECT				

RAMP PAVEMENT MARKING SCHEDULE

LOCATION			LENGTH	SHORT-TERM PAVEMENT MARKING (WHITE SKIP-DASH)	SHORT-TERM PAVEMENT MARKING (WHITE SHOULDER SKIP)	SHORT-TERM PAVEMENT MARKING (YELLOW SHOULDER SKIP)	SHORT-TERM PAVEMENT MARKING REMOVAL	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	TEMPORARY PAVEMENT MARKING LINE-6" (SKIP-DASH)	TEMPORARY PAVEMENT MARKING LINE-6" (WHITE DOTTED)	TEMPORARY PAVEMENT MARKING LINE-6" (WHITE)	TEMPORARY PAVEMENT MARKING LINE-6" (YELLOW)	TEMPORARY PAVEMENT MARKING LINE-8" (WHITE)	TEMPORARY PAVEMENT MARKING LINE-12" (WHITE)	TEMPORARY PAVEMENT MARKING LINE-24" (WHITE)	THERMOPLASTIC PAVEMENT MARKING-LETTERS AND SYMBOLS	THERMOPLASTIC PAVEMENT MARKING LINE-6" (WHITE DOTTED)	THERMOPLASTIC PAVEMENT MARKING LINE-6" (WHITE)	THERMOPLASTIC PAVEMENT MARKING LINE-6" (YELLOW)	THERMOPLASTIC PAVEMENT MARKING LINE-12" (WHITE)	THERMOPLASTIC PAVEMENT MARKING LINE-24" (WHITE)	PREFORMED PLASSTIC PAVEMENT MARING, TYPE B-INLAID-LINE 6" (SKIP-DASH)	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-LINE 6" (WHITE DOTTED)	RAISED REFLECTIVE PAVEMENT MARKERS (MONO CRYSTAL)	RAISED REFLECTIVE PAVEMENT MARKERS (MONO AMBER)	PAVEMENT MARKING REMOVAL (WATER BLASTING)	TEMPORARY PAVEMENT MARKING REMOVAL
STATION	TO	STATION	FEET	FOOT	FOOT	FOOT	SQ FT	SQ FT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	EACH	SQ FT	SQ FT
RAMP "A"																											
100+23.05	TO	101+43.45	120.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	148.1	127.8	0.0	0.0	0.0	0.0	0.0	148.1	127.8	0.0	0.0	0.0	0.0	0	0	0	138.0
101+43.45	TO	102+39.04	95.6	0.0	7.6	0.0	1.3	0.0	0.0	0.0	95.3	95.7	0.0	0.0	0.0	0.0	0.0	95.3	95.7	0.0	0.0	0.0	0.0	0	0	0	95.5
102+39.04	TO	106+98.19	459.2	0.0	36.7	0.0	6.1	0.0	0.0	0.0	458.5	445.0	0.0	0.0	0.0	0.0	0.0	458.5	445.0	0.0	0.0	0.0	0.0	0	0	0	451.8
106+98.19	TO	109+38.18	240.0	0.0	19.2	0.0	3.2	0.0	0.0	0.0	240.5	240.0	0.0	0.0	0.0	0.0	0.0	240.5	240.0	0.0	0.0	0.0	0.0	0	0	0	240.2
109+38.18	TO	114+47.13	508.9	0.0	40.7	0.0	6.8	0.0	0.0	0.0	559.2	569.9	0.0	0.0	0.0	0.0	0.0	559.2	569.9	0.0	0.0	0.0	0.0	0	0	0	564.6
114+47.13	TO	116+17.21	170.1	0.0	13.6	0.0	2.3	0.0	0.0	0.0	120.2	0.0	170.1	0.0	0.0	0.0	0.0	120.2	0.0	0.0	0.0	0.0	0.0	0	0	0	173.5
116+17.21	=	0+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	0	0.0
0+00.00	TO	3+00.00	300.0	0.0	24.0	0.0	4.0	0.0	0.0	0.0	300.0	0.0	300.0	0.0	0.0	0.0	0.0	300.0	0.0	0.0	0.0	0.0	0.0	0	0	0	350.0
3+00.00	TO	4+26.87	126.9	0.0	10.1	0.0	1.7	0.0	0.0	0.0	126.9	0.0	0.0	0.0	0.0	0.0	0.0	126.9	0.0	0.0	0.0	0.0	0.0	0	0	0	63.4
4+26.87	TO	5+70.15	143.3	0.0	11.5	0.0	1.9	0.0	0.0	0.0	143.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	143	71.6
5+70.15	TO	10+49.93	479.8	0.0	38.4	0.0	6.4	0.0	0.0	0.0	479.8	0.0	0.0	0.0	0.0	0.0	0.0	479.8	0.0	0.0	0.0	0.0	0.0	0	0	0	239.9
RAMP "B"																											
0+00.00	TO	3+02.69	302.7	0.0	24.2	0.0	4.0	0.0	0.0	0.0	302.7	0.0	0.0	0.0	0.0	0.0	0.0	302.7	0.0	0.0	0.0	0.0	0.0	16	0	0	151.3
3+02.69	TO	6+62.40	359.7	0.0	28.8	0.0	4.8	0.0	0.0	0.0	359.7	0.0	359.7	0.0	0.0	0.0	0.0	359.7	0.0	0.0	0.0	0.0	0.0	28	0	0	419.7
6+62.40	=	200+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	0	0.0
200+00.00	TO	200+66.55	66.5	0.0	5.3	0.0	0.9	0.0	0.0	0.0	66.5	0.0	66.5	0.0	0.0	0.0	0.0	66.5	0.0	0.0	0.0	0.0	0.0	4	0	0	77.6
200+66.55	TO	206+34.13	567.6	0.0	45.4	0.0	7.6	0.0	0.0	0.0	617.4	628.7	0.0	0.0	0.0	0.0	0.0	617.4	628.7	0.0	0.0	0.0	0.0	1	15	0	623.1
206+34.13	TO	208+74.14	240.0	0.0	19.2	0.0	3.2	0.0	0.0	0.0	240.0	240.0	0.0	0.0	0.0	0.0	0.0	240.0	240.0	0.0	0.0	0.0	0.0	0	0	0	240.0
208+74.14	TO	212+88.68	414.5	0.0	33.2	0.0	5.5	0.0	0.0	0.0	413.7	401.8	0.0	0.0	0.0	0.0	0.0	413.7	401.8	0.0	0.0	0.0	0.0	0	0	0	407.8
212+88.68	TO	214+00.00	111.3	0.0	8.9	0.0	1.5	24.3	0.0	0.0	111.3	111.3	0.0	0.0	0.0	24.3	0.0	111.3	111.3	0.0	0.0	0.0	0.0	0	0	0	111.3
214+00.00	TO	215+21.92	121.9	0.0	9.8	0.0	1.6	31.2	0.0	0.0	182.8	121.4	182.4	120.5	36.0	31.2	0.0	182.8	121.4	120.5	36.0	0.0	0.0	0	0	0	461.4
RAMP "C"																											
0+00.00	TO	0+38.72	38.7	0.0	3.1	0.0	0.5	0.0	0.0	0.0	38.7	0.0	0.0	0.0	0.0	0.0	0.0	38.7	0.0	0.0	0.0	0.0	0.0	2	0	0	38.7
0+38.72	TO	1+84.97	146.3	0.0	11.7	0.0	1.9	0.0	0.0	0.0	146.3	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	146	146.3
1+84.97	TO	3+02.75	117.8	0.0	9.4	0.0	1.6	0.0	0.0	0.0	117.8	0.0	0.0	0.0	0.0	0.0	0.0	117.8	0.0	0.0	0.0	0.0	0.0	5	0	0	117.8
3+02.75	TO	5+79.92	277.2	0.0	22.2	0.0	3.7	0.0	0.0	0.0	277.2	0.0	277.2	0.0	0.0	0.0	0.0	277.2	0.0	0.0	0.0	0.0	0.0	21	0	0	462.0
5+79.92	TO	6+62.40	82.5	0.0	6.6	0.0	1.1	0.0	0.0	0.0	82.5	82.5	0.0	0.0	0.0	0.0	0.0	82.5	82.5	0.0	0.0	0.0	0.0	1	2	0	209.2
6+62.40	=	300+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	0	0.0
300+00.00	TO	306+08.50	608.5	0.0	48.7	0.0	8.1	0.0	0.0	0.0	609.3	620.1	0.0	0.0	0.0	0.0	0.0	609.3	620.1	0.0	0.0	0.0	0.0	0	16	0	609.3
306+08.50	TO	308+48.50	240.0	0.0	19.2	0.0	3.2	0.0	0.0	0.0	239.5	240.8	0.0	0.0	0.0	0.0	0.0	239.5	240.8	0.0	0.0	0.0	0.0	0	0	0	239.5
308+48.50	TO	310+55.63	207.1	0.0	16.6	0.0	2.8	0.0	0.0	0.0	207.7	200.1	0.0	0.0	0.0	0.0	0.0	207.7	200.1	0.0	0.0	0.0	0.0	0	0	0	207.7
310+55.63	TO	311+56.62	101.0	0.0	8.1	0.0	1.3	24.3	0.0	0.0	100.5	101.0	0.0	0.0	0.0	24.3	0.0	100.5	101.0	0.0	0.0	0.0	0.0	0	0	0	100.5
311+56.62	TO	312+75.63	119.0	0.0	9.5	0.0	1.6	31.2	0.0	0.0	189.8	112.7	190.1	115.0	36.0	31.2	0.0	189.8	112.7	115.0	36.0	0.0	0.0	0	0	0	498.7

MODEL: Default
FILE: Model: PavementMarkingSchedule.dgn
PROJECT: 74982, CADData\CAD\Projects\DOT - Office\Drawings\74982-sch-schedule.dgn

USER NAME = steffemk	DESIGNED -	REVISED -
PLOT SCALE = 100.0000 ' / in.	DRAWN -	REVISED -
PLOT DATE = 8/12/2020	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET 9 OF 12 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	15
CONTRACT NO. 74982				
ILLINOIS		FED. AID PROJECT		

RAMP PAVEMENT MARKING SCHEDULE CONT'D

LOCATION			LENGTH	SHORT-TERM PAVEMENT MARKING (WHITE SKIP-DASH)	SHORT-TERM PAVEMENT MARKING (WHITE SHOULDER SKIP)	SHORT-TERM PAVEMENT MARKING (YELLOW SHOULDER SKIP)	SHORT-TERM PAVEMENT MARKING REMOVAL	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	TEMPORARY PAVEMENT MARKING LINE-6" (SKIP-DASH)	TEMPORARY PAVEMENT MARKING LINE-6" (WHITE DOTTED)	TEMPORARY PAVEMENT MARKING LINE-6" (WHITE)	TEMPORARY PAVEMENT MARKING LINE-6" (YELLOW)	TEMPORARY PAVEMENT MARKING LINE-8" (WHITE)	TEMPORARY PAVEMENT MARKING LINE-12" (WHITE)	TEMPORARY PAVEMENT MARKING LINE-24" (WHITE)	THERMOPLASTIC PAVEMENT MARKING-LETTERS AND SYMBOLS	THERMOPLASTIC PAVEMENT MARKING LINE-6" (WHITE DOTTED)	THERMOPLASTIC PAVEMENT MARKING LINE-6" (WHITE)	THERMOPLASTIC PAVEMENT MARKING LINE-6" (YELLOW)	THERMOPLASTIC PAVEMENT MARKING LINE-12" (WHITE)	THERMOPLASTIC PAVEMENT MARKING LINE-24" (WHITE)	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID-LINE 6" (SKIP-DASH)	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B-LINE 6" (WHITE DOTTED)	RAISED REFLECTIVE PAVEMENT MARKERS (MONO CRYSTAL)	RAISED REFLECTIVE PAVEMENT MARKERS (MONO AMBER)	PAVEMENT MARKING REMOVAL (WATER BLASTING)	TEMPORARY PAVEMENT MARKING REMOVAL		
STATION	TO	STATION	FEET	FOOT	FOOT	FOOT	SQ FT	SQ FT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	EACH	SQ FT	SQ FT		
RAMP "D"																													
400+22.63	TO	401+42.53	119.9	0.0	9.6	0.0	1.6	0.0	0.0	0.0	146.8	127.2	0.0	0.0	0.0	0.0	0.0	146.8	127.2	0.0	0.0	0.0	0.0	0	0	0	137.0		
401+42.53	TO	401+61.25	18.7	0.0	1.5	0.0	0.2	0.0	0.0	0.0	18.7	18.7	0.0	0.0	0.0	0.0	0.0	18.7	18.7	0.0	0.0	0.0	0.0	0	0	0	18.7		
401+61.25	TO	402+38.23	77.0	0.0	6.2	0.0	1.0	0.0	0.0	0.0	77.0	76.9	0.0	0.0	0.0	0.0	0.0	77.0	76.9	0.0	0.0	0.0	0.0	0	0	0	77.0		
402+38.23	TO	407+73.22	535.0	0.0	42.8	0.0	7.1	0.0	0.0	0.0	533.5	518.5	0.0	0.0	0.0	0.0	0.0	533.5	518.5	0.0	0.0	0.0	0.0	0	0	0	526.0		
407+73.22	TO	410+13.21	240.0	0.0	19.2	0.0	3.2	0.0	0.0	0.0	240.4	241.8	0.0	0.0	0.0	0.0	0.0	240.4	241.8	0.0	0.0	0.0	0.0	0	0	0	241.1		
410+13.21	TO	415+44.28	531.1	0.0	42.5	0.0	7.1	0.0	0.0	0.0	580.8	589.7	0.0	0.0	0.0	0.0	0.0	580.8	589.7	0.0	0.0	0.0	0.0	0	0	0	585.3		
415+44.28	TO	417+13.25	169.0	0.0	13.5	0.0	2.3	0.0	0.0	0.0	120.2	0.0	169.0	0.0	0.0	0.0	0.0	120.2	0.0	0.0	0.0	0.0	0.0	0	0	0	172.8		
417+13.25	=	0+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0	0	0	0.0		
0+00.00	TO	3+00.00	300.0	0.0	24.0	0.0	4.0	0.0	0.0	0.0	300.0	0.0	300.0	0.0	0.0	0.0	0.0	300.0	0.0	0.0	0.0	0.0	0.0	0	0	0	350.0		
3+00.00	TO	10+49.60	749.6	0.0	60.0	0.0	10.0	0.0	0.0	0.0	749.6	0.0	0.0	0.0	0.0	0.0	0.0	749.6	0.0	0.0	0.0	0.0	0.0	0	0	0	374.8		
RAMP TOTALS =				0	751	0	125	111	0	0	9,742	5,912	2,015	236	72	111	0	9,453	5,912	236	72	0	0	78	33	290	9,993		
WB TOTALS =				12,835	5,134	5,134	3,850	0	0	909	62,367	64,175	899	0	0	0	909	62,367	64,175	0	0	16,044	144	1,614	0	1,151	64,332		
EB TOTALS =				12,739	5,096	5,096	3,821	0	0	910	62,074	63,838	749	0	0	0	910	62,035	63,838	0	0	15,923	144	1,599	0	874	63,910		
PROJECT TOTALS =				25,574	10,980	10,230	7,797	111	0	1,819	134,183	133,925	3,663	236	72	111	1,819	133,855	133,925	236	72	31,967	288	3,292	33	2,315	138,235		

MODEL: Default
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PROJECT: 74982_CADD\DATA\CAD\harcam.dwg
PROJECT: 74982_CADD\DATA\CAD\harcam.dwg

USER NAME = steffenmk	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 8/12/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: SHEET 10 OF 12 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	16
CONTRACT NO. 74982				
		ILLINOIS	FED. AID PROJECT	

EB GROOVING FOR RECESSED PAVEMENT MARKING SCHEDULE

STATION	TO	STATION	LENGTH FEET	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS SQ FT	GROOVING FOR RECESSED PAVEMENT MARKING, 7" FOOT	GROOVING FOR RECESSED PAVEMENT MARKING, 9" FOOT	GROOVING FOR RECESSED PAVEMENT MARKING, 13" FOOT	GROOVING FOR RECESSED PAVEMENT MARKING, 25" FOOT
I-64 (EB)								
3986+60.55	TO	4020+83.94	3,423.4	0.0	6,846.8	0.0	0.0	0.0
4020+83.94	TO	4056+62.23	3,578.3	0.0	7,156.6	0.0	0.0	0.0
4056+62.23	TO	4057+65.86	103.6	0.0	0.0	0.0	0.0	0.0
4057+65.86	TO	4120+15.51	6,249.7	0.0	12,499.3	0.0	0.0	0.0
4120+15.51	TO	4126+01.85	586.3	0.0	0.0	0.0	0.0	0.0
4126+01.85	TO	4221+08.50	9,506.7	0.0	19,013.3	0.0	0.0	0.0
4221+08.50	=	4214+81.00	0.0	0.0	0.0	0.0	0.0	0.0
4214+81.00	TO	4229+87.42	1,506.4	0.0	3,012.8	0.0	0.0	0.0
4229+87.42	TO	4233+15.59	328.2	0.0	0.0	0.0	0.0	0.0
4233+15.59	TO	4256+75.33	2,359.7	0.0	4,719.5	0.0	0.0	0.0
4256+75.33	TO	4291+15.86	3,440.5	0.0	6,881.1	0.0	0.0	0.0
4291+15.86	TO	4292+14.99	99.1	0.0	0.0	0.0	0.0	0.0
4292+14.99	TO	4316+15.64	2,400.7	0.0	4,801.3	0.0	0.0	0.0
4316+15.64	TO	4316+54.41	38.8	0.0	77.5	0.0	0.0	0.0
4316+54.41	TO	4317+98.41	144.0	0.0	144.0	0.0	0.0	0.0
4317+98.41	TO	4319+19.64	121.2	0.0	242.5	0.0	0.0	0.0
4319+19.64	TO	4321+96.72	277.1	0.0	277.1	277.1	0.0	0.0
4321+96.72	TO	4333+94.74	1,198.0	0.0	2,396.0	0.0	0.0	0.0
4333+94.74	TO	4335+44.74	150.0	0.0	0.0	0.0	0.0	0.0
4335+44.74	TO	4346+36.19	1,091.5	0.0	2,182.9	0.0	0.0	0.0
4346+36.19	TO	4351+07.87	471.7	0.0	471.7	471.7	0.0	0.0
4351+07.87	TO	4358+57.95	750.1	0.0	1,500.2	0.0	0.0	0.0
4358+57.95	TO	4407+70.37	4,912.4	0.0	9,824.8	0.0	0.0	0.0
4407+70.37	TO	4463+02.59	5,532.2	0.0	11,064.4	0.0	0.0	0.0
4463+02.59	TO	4511+16.70	4,814.1	0.0	9,628.2	0.0	0.0	0.0
4511+16.70	TO	4553+07.84	4,191.1	0.0	8,382.3	0.0	0.0	0.0
4553+07.84	TO	4588+43.69	3,535.8	0.0	7,071.7	0.0	0.0	0.0
4588+43.69	TO	4616+90.85	2,847.2	0.0	5,694.3	0.0	0.0	0.0
4616+90.85	TO	4617+89.09	98.2	0.0	0.0	0.0	0.0	0.0
4617+89.09	TO	4632+36.48	1,447.4	0.0	2,894.8	0.0	0.0	0.0
SHEET TOTALS =				0	126,783	749	0	0

WB GROOVING FOR RECESSED PAVEMENT MARKING SCHEDULE

STATION	TO	STATION	LENGTH FEET	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS SQ FT	GROOVING FOR RECESSED PAVEMENT MARKING, 7" FOOT	GROOVING FOR RECESSED PAVEMENT MARKING, 9" FOOT	GROOVING FOR RECESSED PAVEMENT MARKING, 13" FOOT	GROOVING FOR RECESSED PAVEMENT MARKING, 25" FOOT
I-64 (WB)								
985+37.01	TO	1020+82.74	3,545.7	0.0	7,091.5	0.0	0.0	0.0
1020+82.74	=	4020+83.94	0.0	0.0	0.0	0.0	0.0	0.0
4020+83.94	TO	4053+43.47	3,259.5	0.0	6,519.1	0.0	0.0	0.0
4053+43.47	=	1053+43.21	0.0	0.0	0.0	0.0	0.0	0.0
1053+43.21	TO	1056+72.20	329.0	0.0	658.0	0.0	0.0	0.0
1056+72.20	TO	1057+74.78	102.6	0.0	0.0	0.0	0.0	0.0
1057+74.78	TO	1119+50.52	6,175.7	0.0	12,351.5	0.0	0.0	0.0
1119+50.52	TO	1123+52.95	402.4	0.0	804.9	0.0	0.0	0.0
1123+52.95	TO	1220+55.52	9,702.6	0.0	19,405.1	0.0	0.0	0.0
1220+55.52	=	4214+94.08	0.0	0.0	0.0	0.0	0.0	0.0
4214+94.08	TO	4229+82.06	1,488.0	0.0	2,976.0	0.0	0.0	0.0
4229+82.06	TO	4233+07.94	325.9	0.0	0.0	0.0	0.0	0.0
4233+07.94	TO	4256+75.34	2,367.4	0.0	4,734.8	0.0	0.0	0.0
4256+75.34	=	4256+75.33	0.0	0.0	0.0	0.0	0.0	0.0
4256+75.33	TO	4290+89.73	3,414.4	0.0	6,828.8	0.0	0.0	0.0
4290+89.73	TO	4291+90.69	101.0	0.0	0.0	0.0	0.0	0.0
4291+90.69	TO	4311+41.14	1,950.5	0.0	3,900.9	0.0	0.0	0.0
4311+41.14	TO	4316+23.37	482.2	0.0	964.5	0.0	0.0	0.0
4316+23.37	TO	4317+67.37	144.0	0.0	144.0	0.0	0.0	0.0
4317+67.37	TO	4318+91.20	123.8	0.0	247.7	0.0	0.0	0.0
4318+91.20	TO	4323+63.00	471.8	0.0	471.8	471.8	0.0	0.0
4323+63.00	TO	4334+09.63	1,046.6	0.0	2,093.3	0.0	0.0	0.0
4334+09.63	TO	4335+59.62	150.0	0.0	0.0	0.0	0.0	0.0
4335+59.62	TO	4348+15.58	1,256.0	0.0	2,511.9	0.0	0.0	0.0
4348+15.58	TO	4352+42.67	427.1	0.0	427.1	427.1	0.0	0.0
4352+42.67	TO	4355+45.76	303.1	0.0	606.2	0.0	0.0	0.0
4355+45.76	TO	4407+70.37	5,224.6	0.0	10,449.2	0.0	0.0	0.0
4407+70.37	TO	4463+02.59	5,532.2	0.0	11,064.4	0.0	0.0	0.0
4463+02.59	TO	4511+16.70	4,814.1	0.0	9,628.2	0.0	0.0	0.0
4511+16.70	TO	4553+07.84	4,191.1	0.0	8,382.3	0.0	0.0	0.0
4553+07.84	TO	4588+43.69	3,535.8	0.0	7,071.7	0.0	0.0	0.0
4588+43.69	TO	4618+53.59	3,009.9	0.0	6,019.8	0.0	0.0	0.0
4618+53.59	TO	4623+54.35	500.8	0.0	0.0	0.0	0.0	0.0
4623+54.35	TO	4638+78.25	1,523.9	0.0	3,047.8	0.0	0.0	0.0
SHEET TOTALS =				0	127,595	899	0	0

MODEL: Default
 FILE: Model: P:\pub\baronm.dwg
 PROJECT: 74982_CADD\Drawings\DOT - Office\Drawings\74982-sht-schedule.dgn

USER NAME = steffemk	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 8/12/2020	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES			
SCALE:	SHEET 11	OF 12 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	17
CONTRACT NO. 74982				
ILLINOIS FED. AID PROJECT				

RAMP GROOVING FOR RECESSED PAVEMENT MARKING SCHEDULE

STATION		TO	STATION	FEET	SQ FT	FOOT	FOOT	FOOT	FOOT
				LENGTH	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS	GROOVING FOR RECESSED PAVEMENT MARKING, 7"	GROOVING FOR RECESSED PAVEMENT MARKING, 9"	GROOVING FOR RECESSED PAVEMENT MARKING, 13"	GROOVING FOR RECESSED PAVEMENT MARKING, 25"
RAMP "A"									
100+23.05	TO		101+43.45	120.4	0.0	275.9	0.0	0.0	0.0
101+43.45	TO		102+39.04	95.6	0.0	191.0	0.0	0.0	0.0
102+39.04	TO		106+98.19	459.2	0.0	903.5	0.0	0.0	0.0
106+98.19	TO		109+38.18	240.0	0.0	480.5	0.0	0.0	0.0
109+38.18	TO		114+47.13	508.9	0.0	1,129.1	0.0	0.0	0.0
114+47.13	TO		116+17.21	170.1	0.0	120.2	170.1	0.0	0.0
116+17.21	=		0+00.00	0.0	0.0	0.0	0.0	0.0	0.0
0+00.00	TO		3+00.00	300.0	0.0	300.0	300.0	0.0	0.0
3+00.00	TO		4+26.87	126.9	0.0	126.9	0.0	0.0	0.0
4+26.87	TO		5+70.15	143.3	0.0	0.0	0.0	0.0	0.0
5+70.15	TO		10+49.93	479.8	0.0	479.8	0.0	0.0	0.0
RAMP "B"									
0+00.00	TO		3+02.69	302.7	0.0	302.7	0.0	0.0	0.0
3+02.69	TO		6+62.40	359.7	0.0	359.7	359.7	0.0	0.0
6+62.40	=		200+00.00	0.0	0.0	0.0	0.0	0.0	0.0
200+00.00	TO		200+66.55	66.5	0.0	66.5	66.5	0.0	0.0
200+66.55	TO		206+34.13	567.6	0.0	1,246.1	0.0	0.0	0.0
206+34.13	TO		208+74.14	240.0	0.0	480.0	0.0	0.0	0.0
208+74.14	TO		212+88.68	414.5	0.0	815.5	0.0	0.0	0.0
212+88.68	TO		214+00.00	111.3	24.3	222.6	0.0	0.0	0.0
214+00.00	TO		215+21.92	121.9	31.2	304.2	182.4	120.5	36.0
RAMP "C"									
0+00.00	TO		0+38.72	38.7	0.0	38.7	0.0	0.0	0.0
0+38.72	TO		1+84.97	146.3	0.0	0.0	0.0	0.0	0.0
1+84.97	TO		3+02.75	117.8	0.0	117.8	0.0	0.0	0.0
3+02.75	TO		5+79.92	277.2	0.0	277.2	277.2	0.0	0.0
5+79.92	TO		6+62.40	82.5	0.0	165.0	0.0	0.0	0.0
6+62.40	=		300+00.00	0.0	0.0	0.0	0.0	0.0	0.0
300+00.00	TO		306+08.50	608.5	0.0	1,229.4	0.0	0.0	0.0
306+08.50	TO		308+48.50	240.0	0.0	480.3	0.0	0.0	0.0
308+48.50	TO		310+55.63	207.1	0.0	407.8	0.0	0.0	0.0
310+55.63	TO		311+56.62	101.0	24.3	201.5	0.0	0.0	0.0
311+56.62	TO		312+75.63	119.0	31.2	302.5	190.1	115.0	36.0

RAMP GROOVING FOR RECESSED PAVEMENT MARKING SCHEDULE CONT'D

STATION		TO	STATION	FEET	SQ FT	FOOT	FOOT	FOOT	FOOT
				LENGTH	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS	GROOVING FOR RECESSED PAVEMENT MARKING, 7"	GROOVING FOR RECESSED PAVEMENT MARKING, 9"	GROOVING FOR RECESSED PAVEMENT MARKING, 13"	GROOVING FOR RECESSED PAVEMENT MARKING, 25"
RAMP "D"									
400+22.63	TO		401+42.53	119.9	0.0	274.0	0.0	0.0	0.0
401+42.53	TO		401+61.25	18.7	0.0	37.4	0.0	0.0	0.0
401+61.25	TO		402+38.23	77.0	0.0	153.9	0.0	0.0	0.0
402+38.23	TO		407+73.22	535.0	0.0	1,052.0	0.0	0.0	0.0
407+73.22	TO		410+13.21	240.0	0.0	482.2	0.0	0.0	0.0
410+13.21	TO		415+44.28	531.1	0.0	1,170.5	0.0	0.0	0.0
415+44.28	TO		417+13.25	169.0	0.0	120.2	169.0	0.0	0.0
417+13.25	=		0+00.00	0.0	0.0	0.0	0.0	0.0	0.0
0+00.00	TO		3+00.00	300.0	0.0	300.0	300.0	0.0	0.0
3+00.00	TO		10+49.60	749.6	0.0	749.6	0.0	0.0	0.0
RAMP SHEET TOTALS =					111	15,364	2,015	236	72
WB SHEET TOTALS =					0	127,595	899	0	0
EB SHEET TOTALS =					0	126,783	749	0	0
PROJECT TOTALS =					111	269,743	3,663	236	72

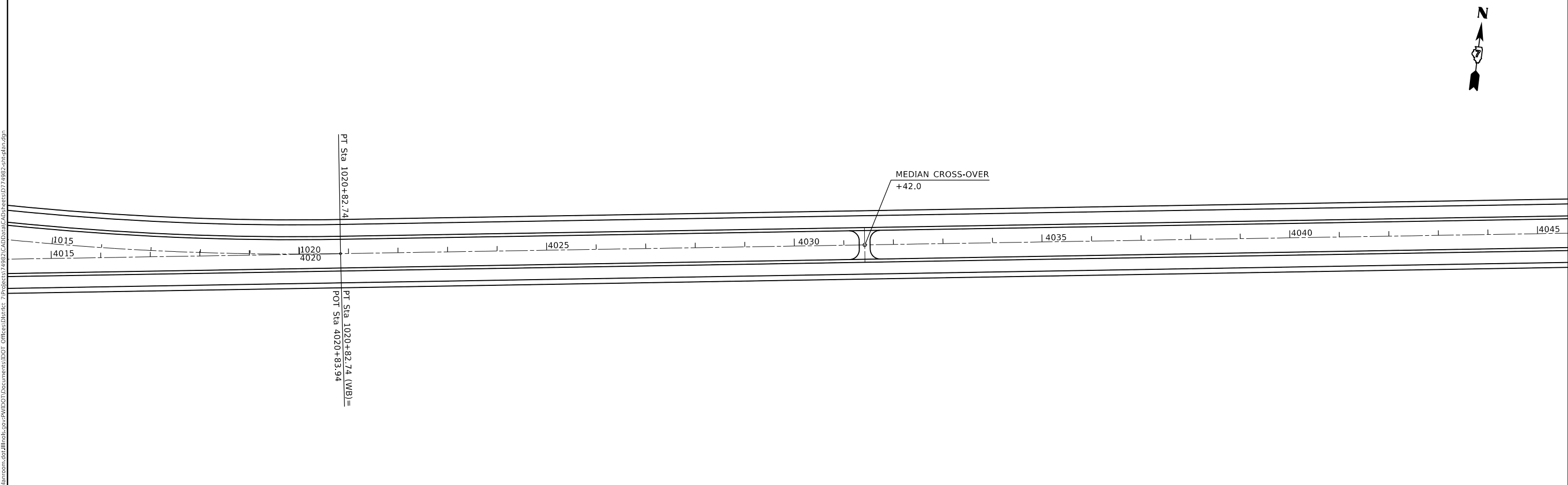
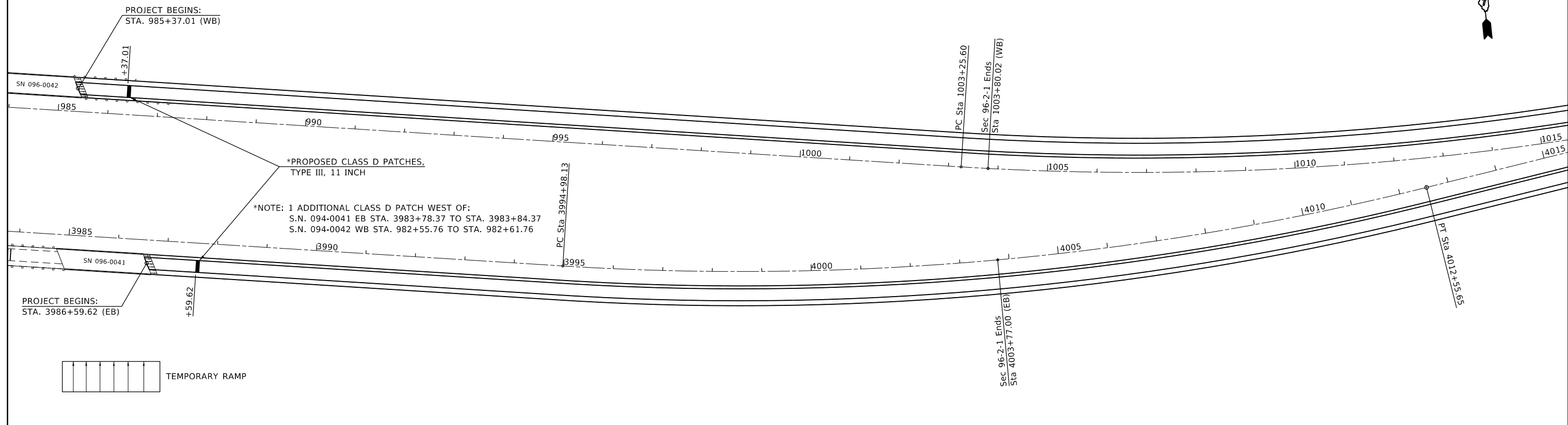
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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES			
SCALE:	SHEET 12	OF 12 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	18
CONTRACT NO. 74982				
ILLINOIS FED. AID PROJECT				



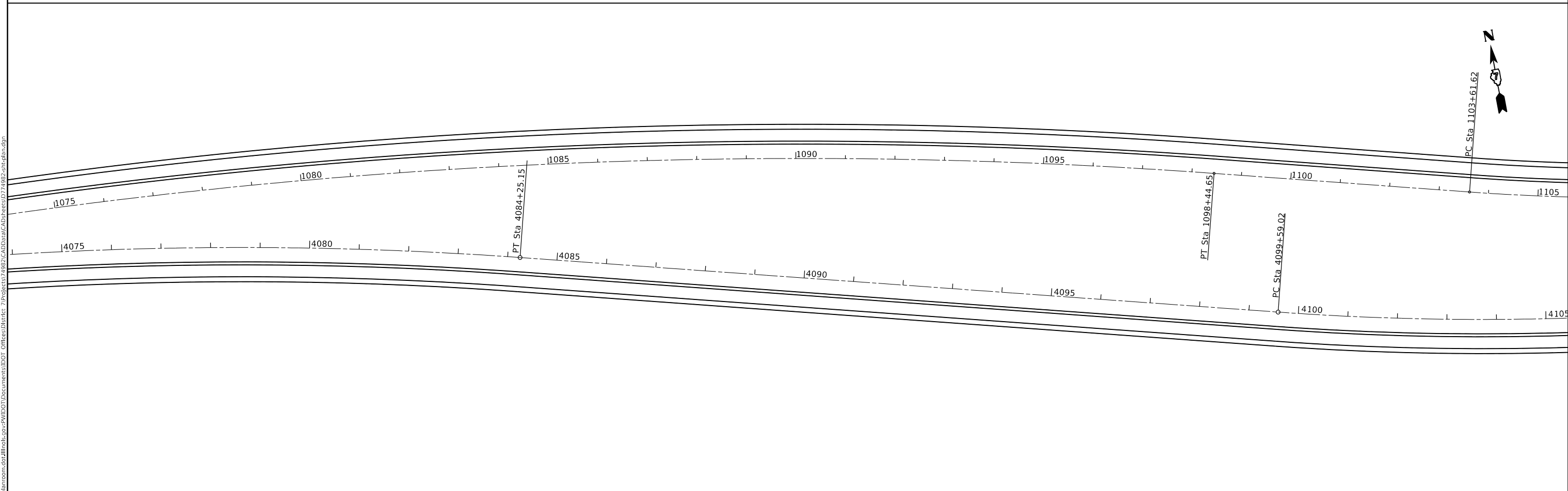
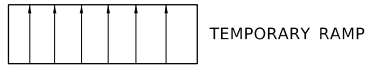
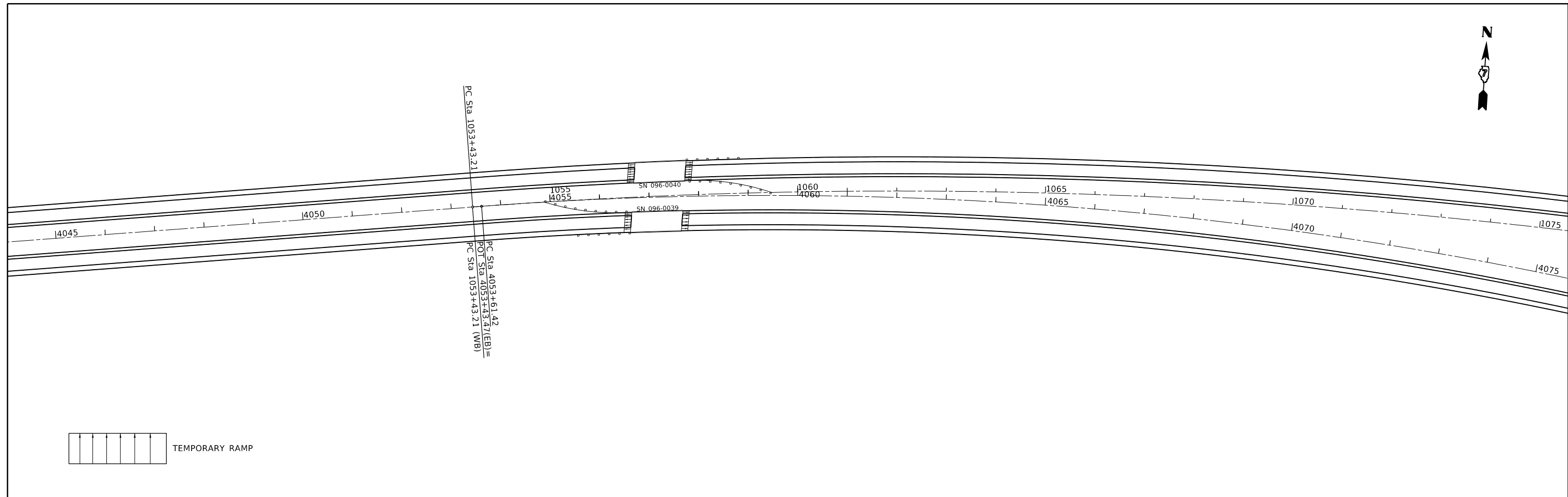
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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLANS SHEETS F.A.I. ROUTE 64		SCALE: 100	SHEET 1	OF 14 SHEETS	STA. 3985+00	TO STA. 4045+00
		ILLINOIS FED. AID PROJECT				

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	19
CONTRACT NO. 74982				



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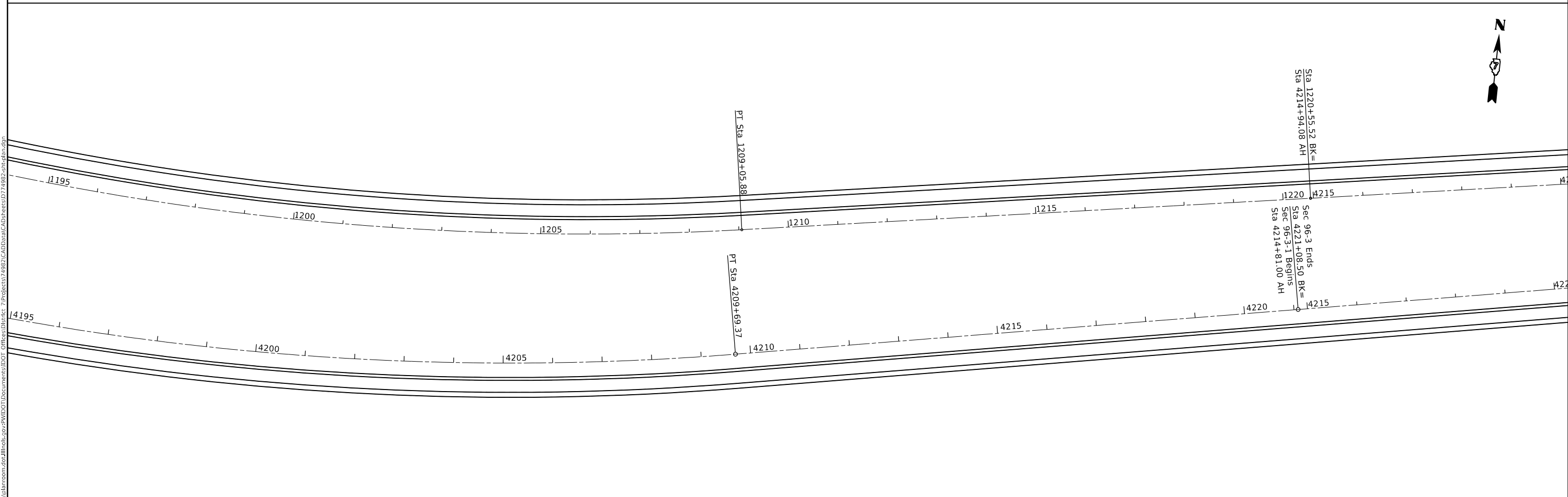
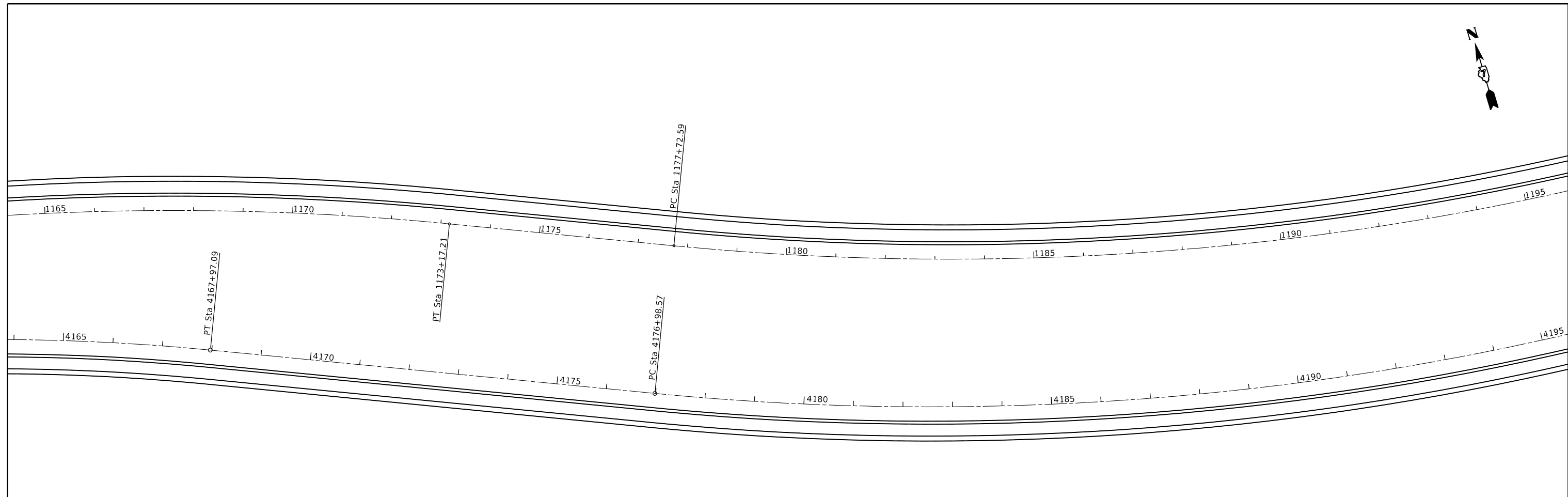
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PLOT DATE = 8/12/2020	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN SHEETS
F.A.I. ROUTE 64

SCALE: 100 SHEET 2 OF 14 SHEETS STA. 4045+00 TO STA. 4105+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	20
CONTRACT NO.				
ILLINOIS		FED. AID PROJECT		



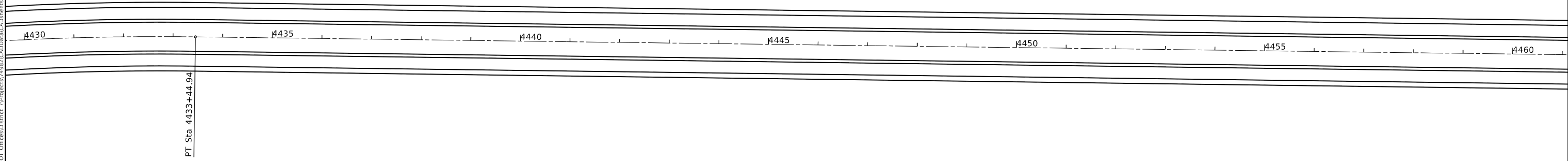
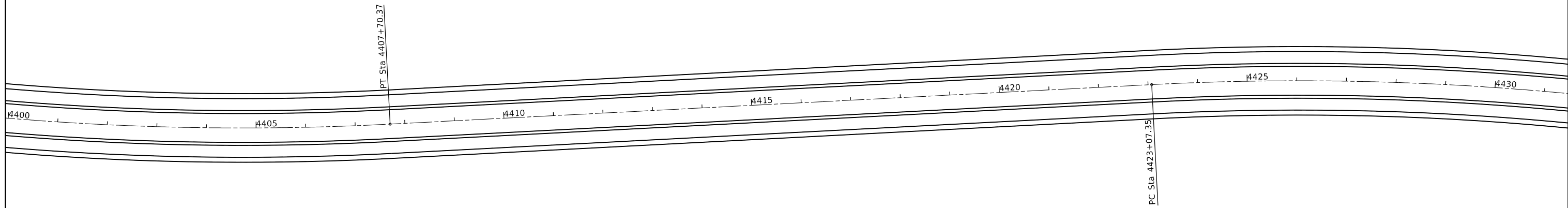
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PLOT DATE = 8/12/2020	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN SHEETS	
F.A.I. ROUTE 64	
SCALE: 100	SHEET 4 OF 14 SHEETS
STA. 4165+00	TO STA. 4220+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	22
CONTRACT NO. 74982				
ILLINOIS FED. AID PROJECT				



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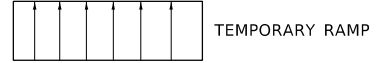
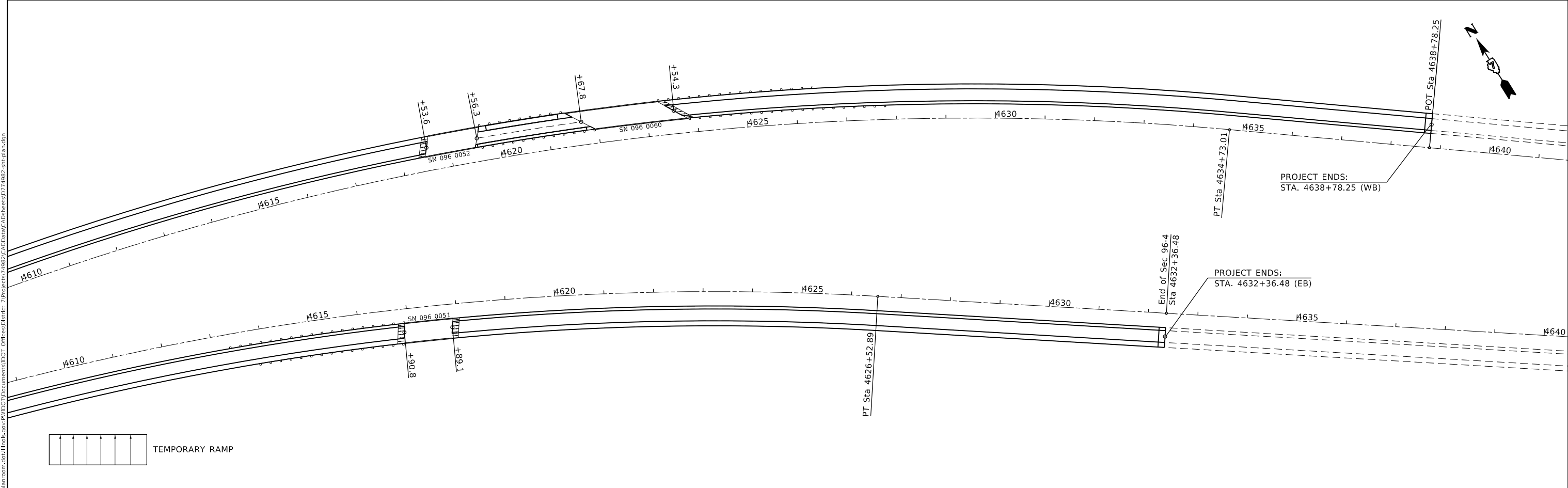
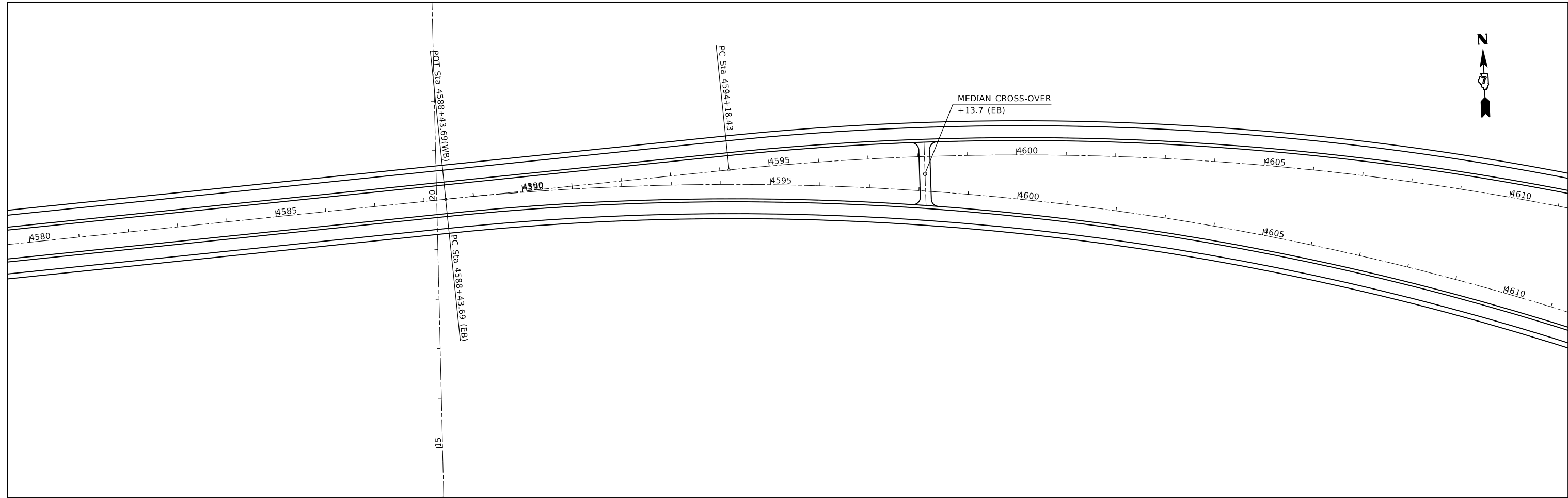
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PLOT DATE = 8/12/2020	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN SHEETS
F.A.I. ROUTE 64

SCALE: 100 SHEET 8 OF 14 SHEETS STA. 4400+00 TO STA. 4460+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	26
CONTRACT NO. 74982				
ILLINOIS		FED. AID PROJECT		



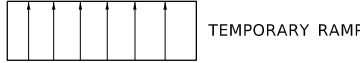
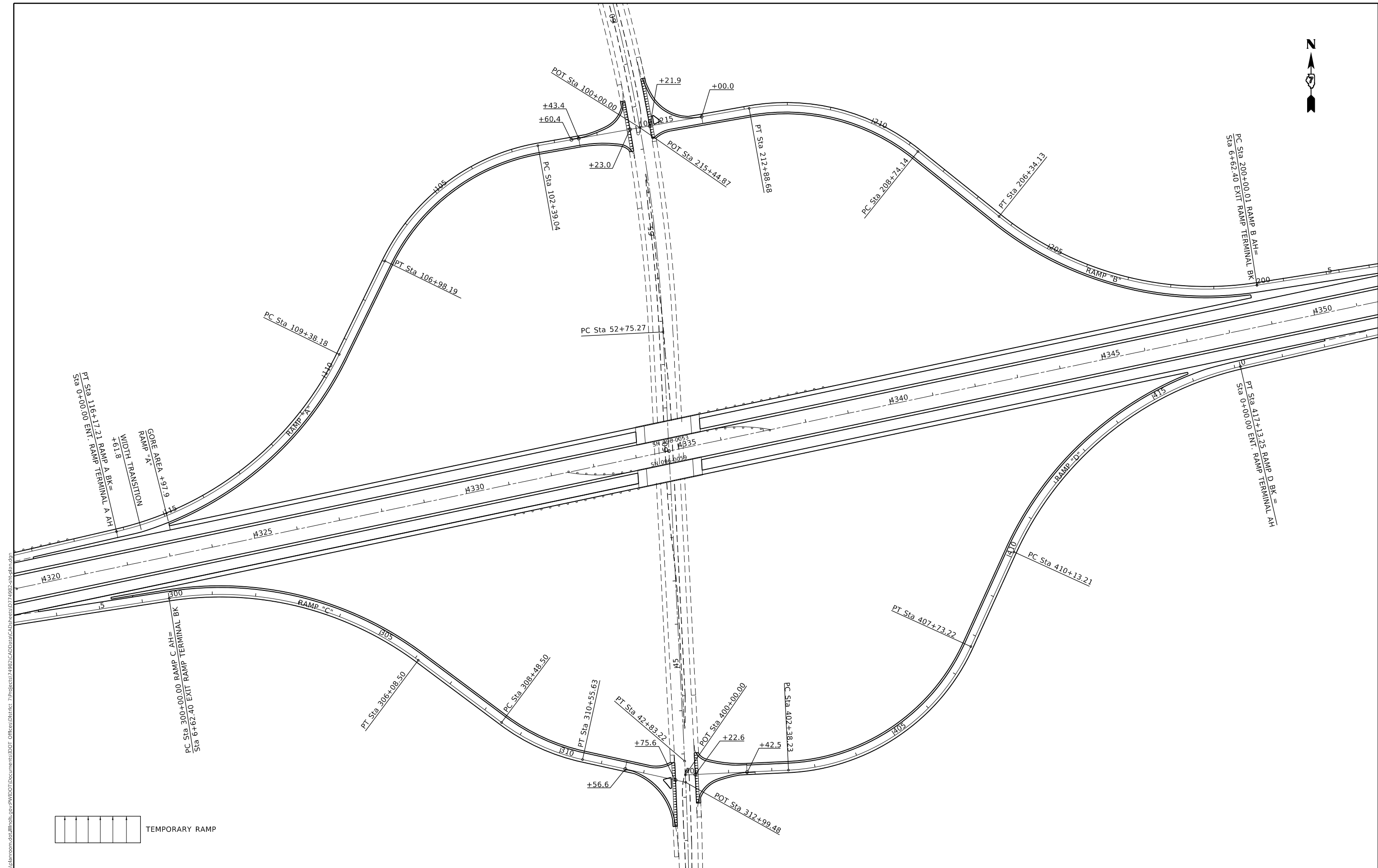
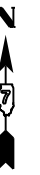
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PLOT DATE = 8/12/2020	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN SHEETS F.A.I. ROUTE 64			
SCALE: 100	SHEET 11	OF 14 SHEETS	STA. 4580+00 TO STA. 4640+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	29
CONTRACT NO. 74982				
		ILLINOIS	FED. AID PROJECT	



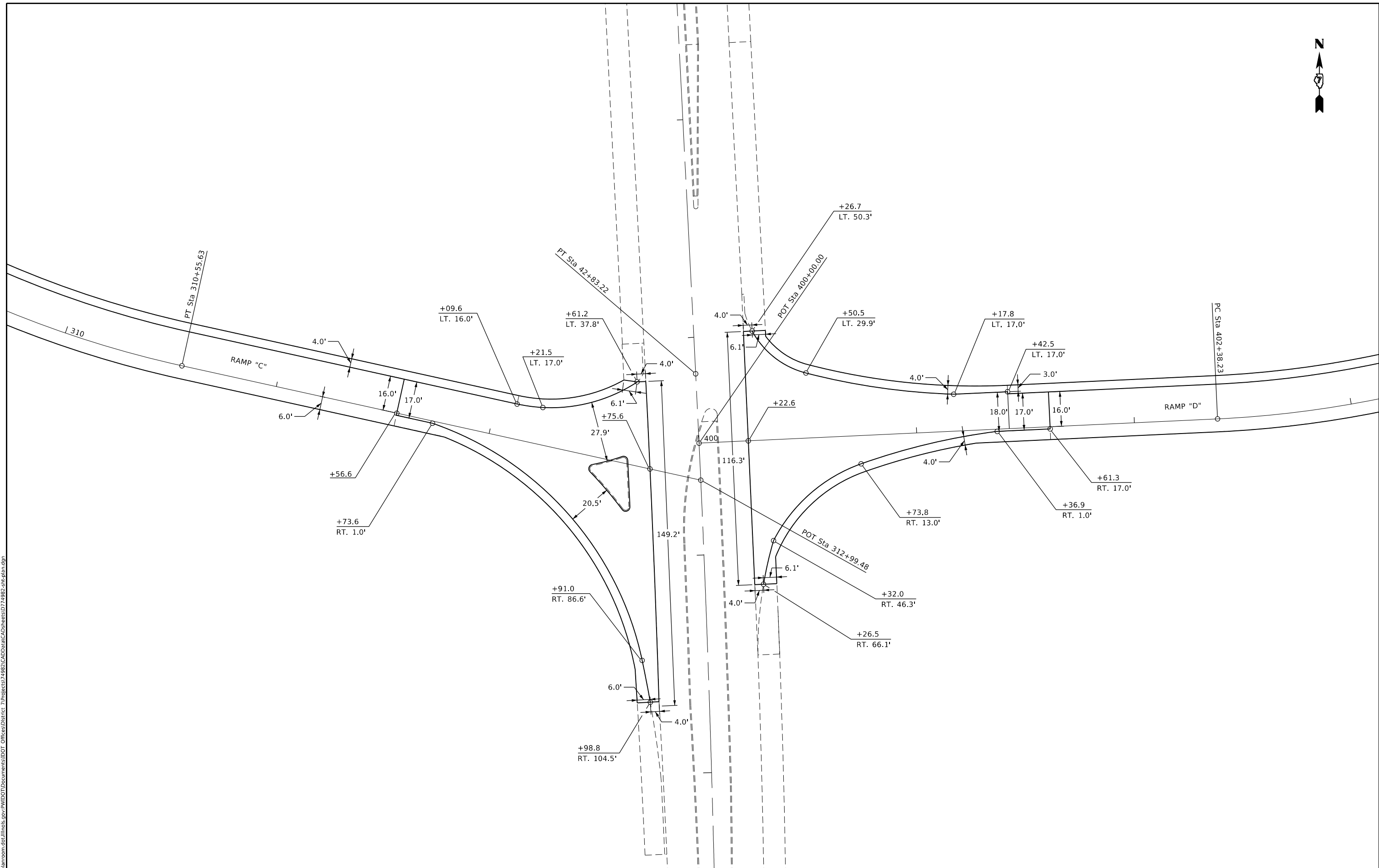
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PLOT DATE = 8/12/2020	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN SHEETS			
F.A.I. 64U.S. 45 INTERCHANGE			
SCALE: 100	SHEET 12 OF 14 SHEETS	STA. 4320+00	TO STA. 4350+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	30
CONTRACT NO. 74982				
ILLINOIS FED. AID PROJECT				



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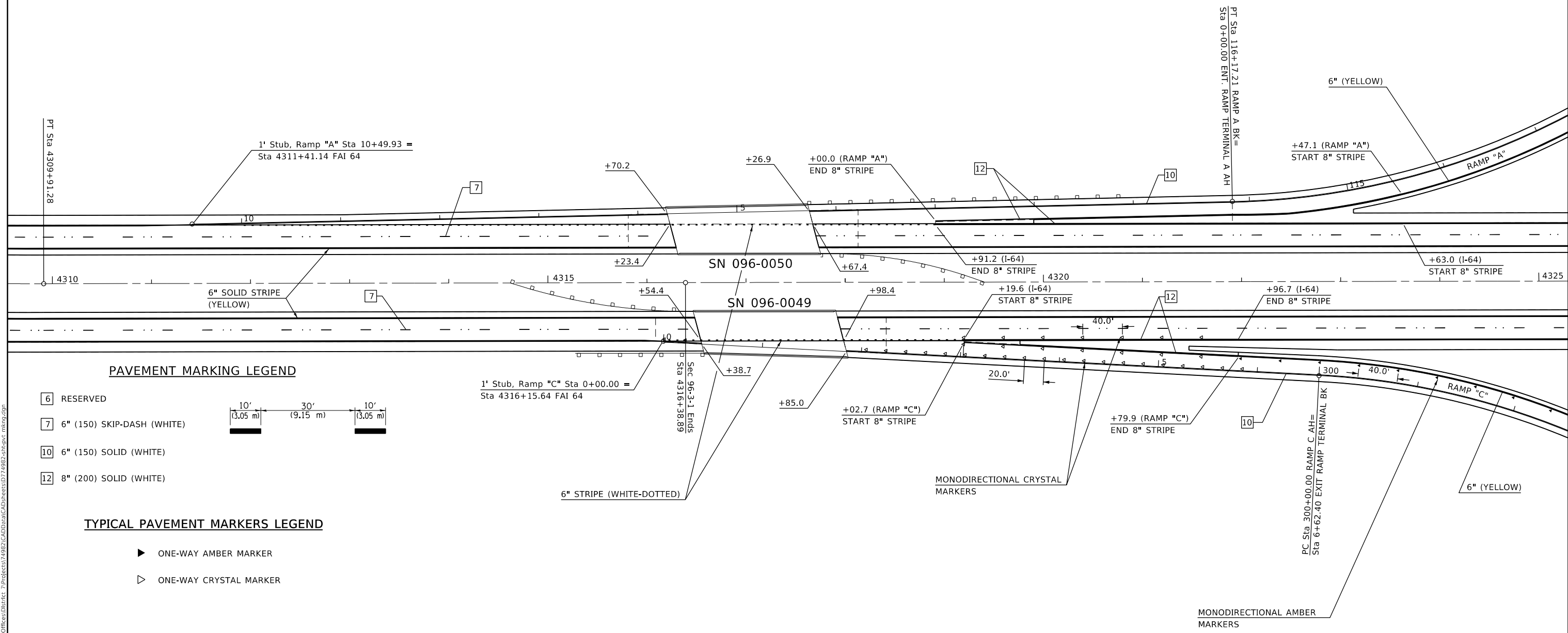
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
RAMPS "C"/"D" & U.S. 45

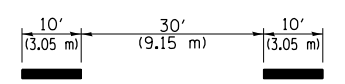
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	32
CONTRACT NO. 74982				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 10 6" (150) SOLID (WHITE)
- 12 8" (200) SOLID (WHITE)



TYPICAL PAVEMENT MARKERS LEGEND

- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

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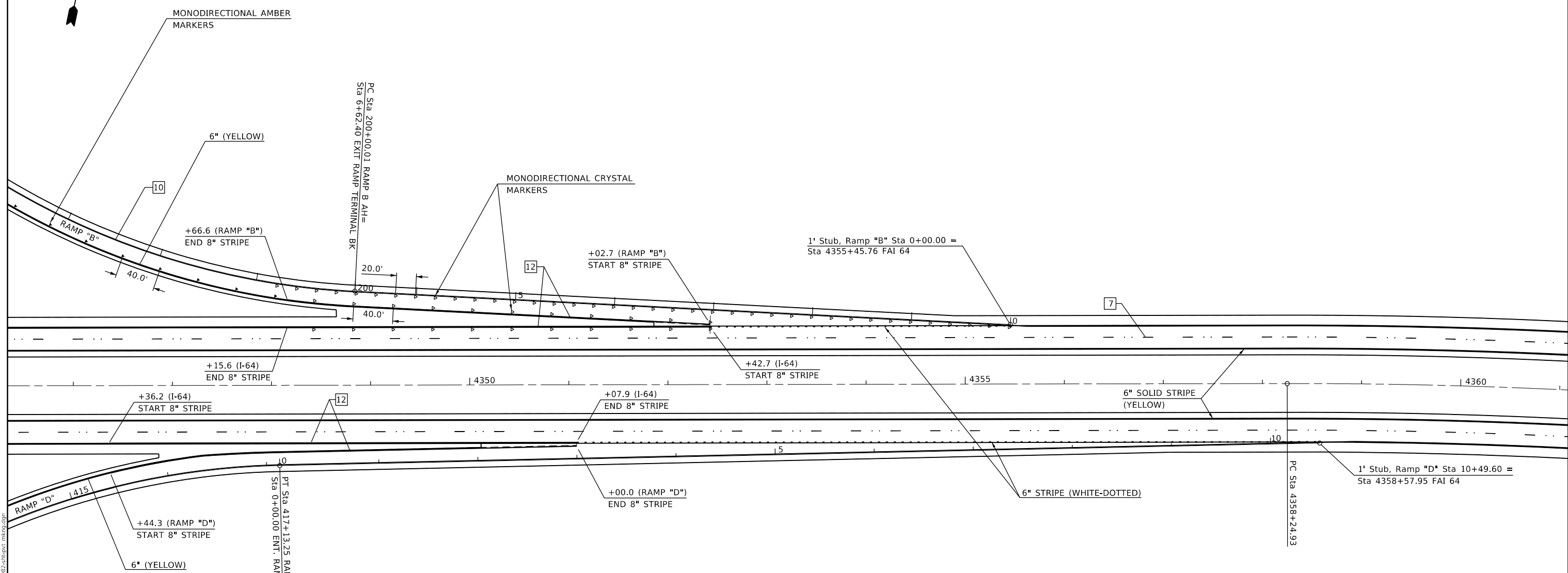
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING

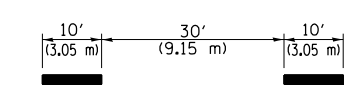
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	33
CONTRACT NO. 74982				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

- [6] RESERVED
- [7] 6" (150) SKIP-DASH (WHITE)
- [10] 6" (150) SOLID (WHITE)
- [12] 8" (200) SOLID (WHITE)



TYPICAL PAVEMENT MARKERS LEGEND

- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

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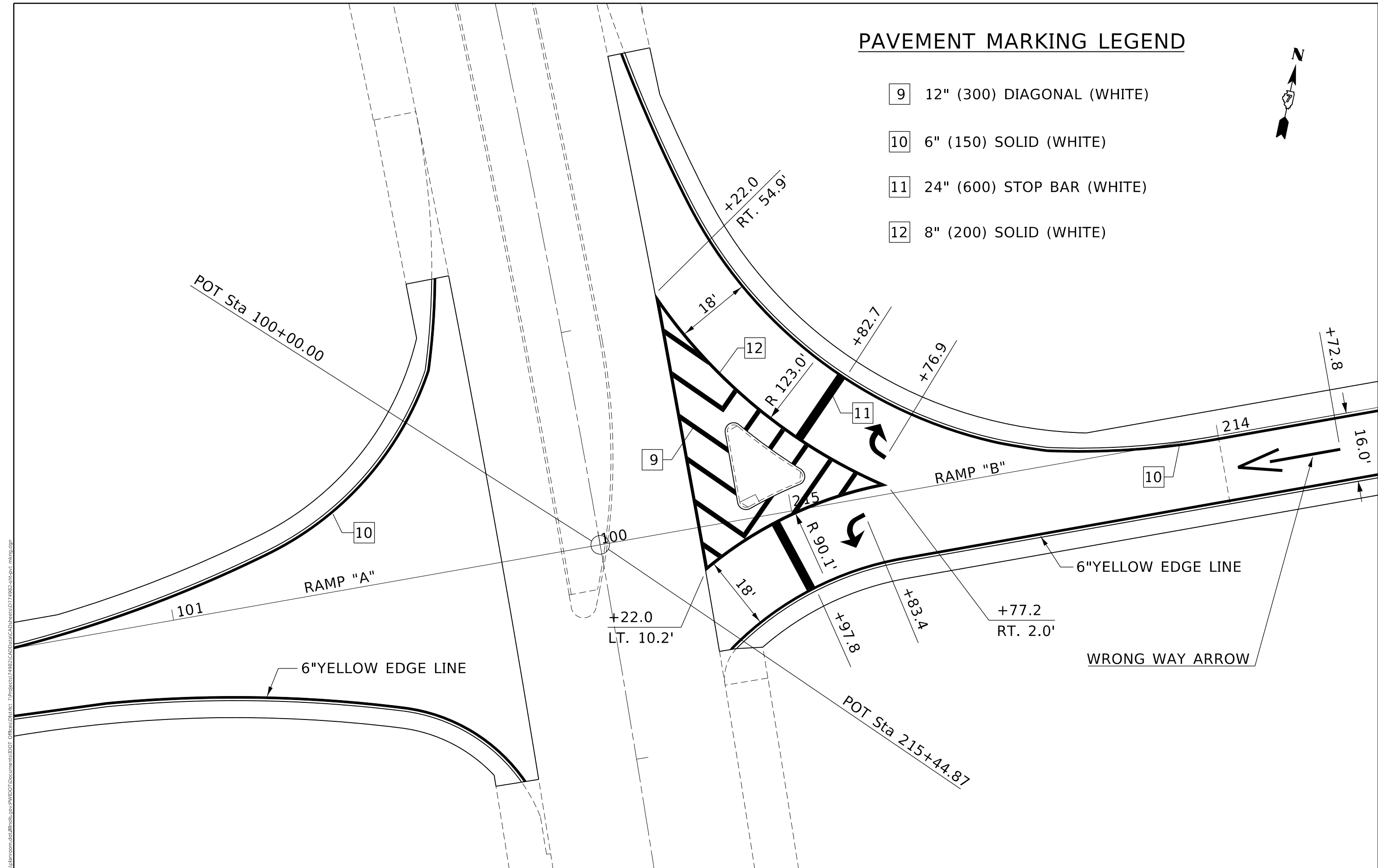
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING			
SCALE: 100	SHEET 2	OF 4 SHEETS	STA. 4346+00 TO STA. 4360+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	34
CONTRACT NO. 74982				
ILLINOIS FED. AID PROJECT				

PAVEMENT MARKING LEGEND

- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

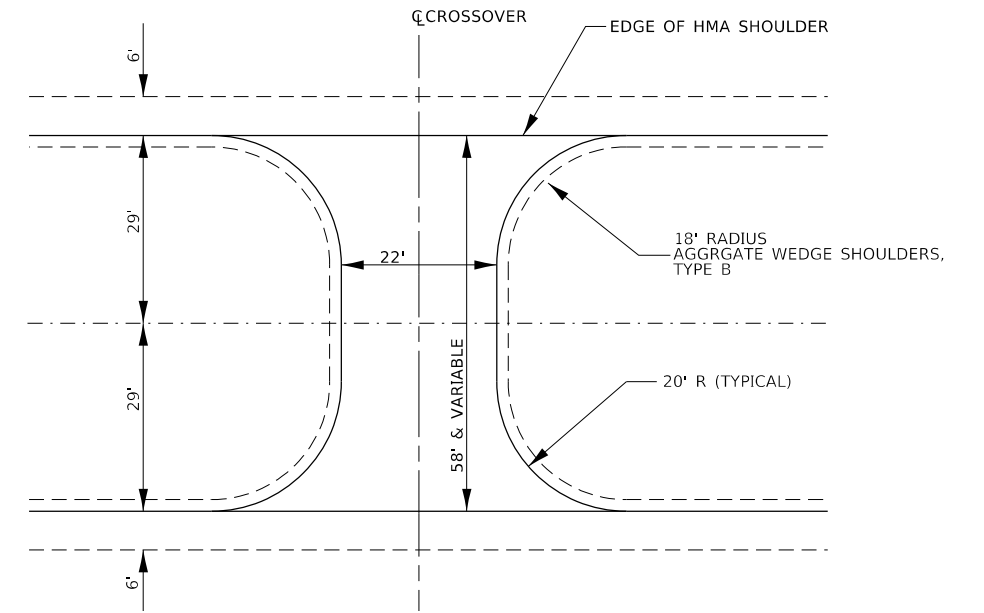
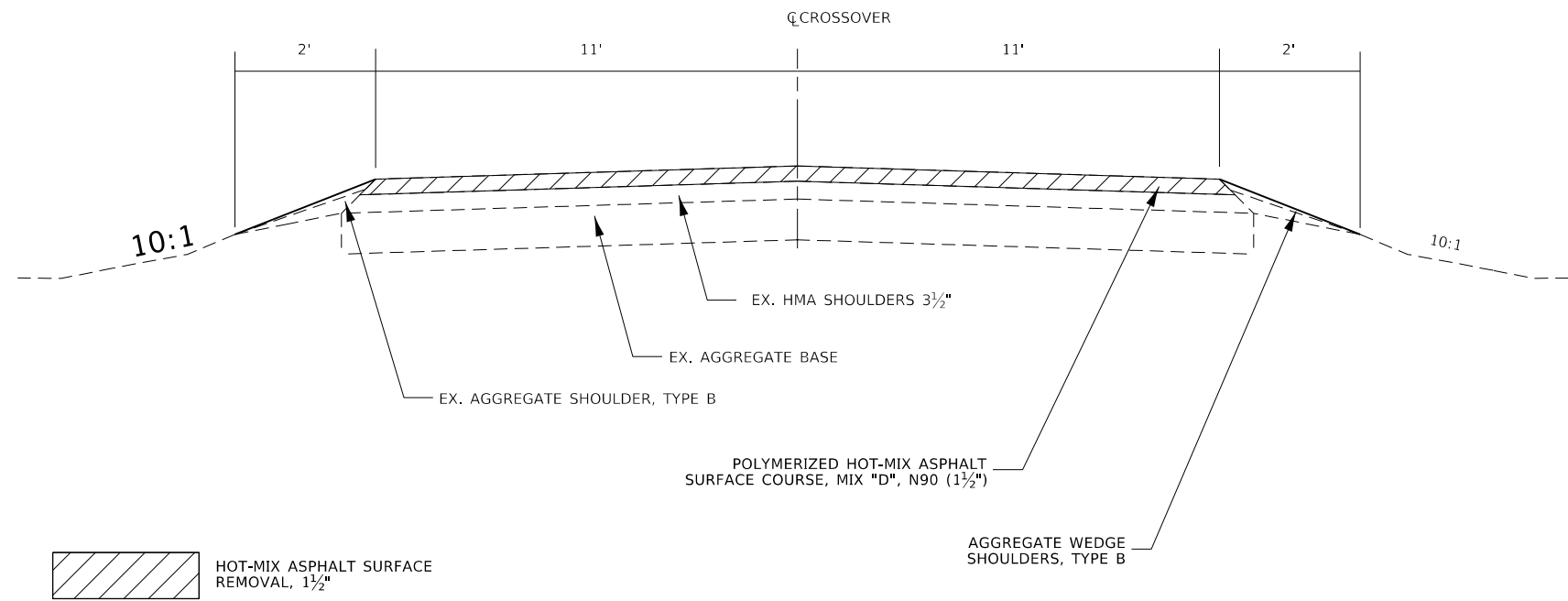
PAVEMENT MARKING
RAMP TERMINALS "A" & "B"

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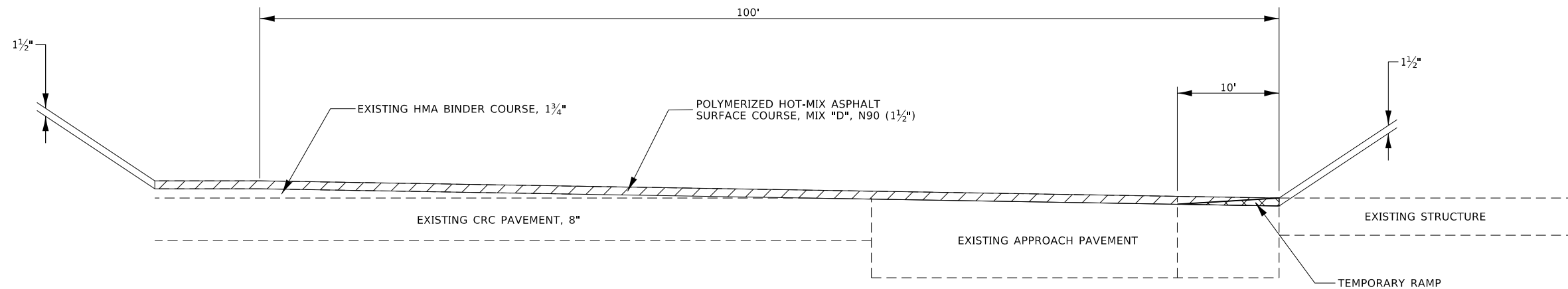
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64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	35
CONTRACT NO. 74982				
ILLINOIS FED. AID PROJECT				

DETAIL OF MEDIAN CROSSOVER
 QUANTITY INCLUDED IN HOT-MIX ASPHALT SHOULDERS

LOCATIONS		
STATION	WIDTH	LENGTH
4031+42.00	22.0 FT.	58.0 FT.
4269+11.00	22.0 FT.	58.0 FT.
4376+25.00	22.0 FT.	58.0 FT.
4598+13.68	22.0 FT.	127.9 FT.



DETAIL OF BUTT-JOINT AT STRUCTURES



 HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"

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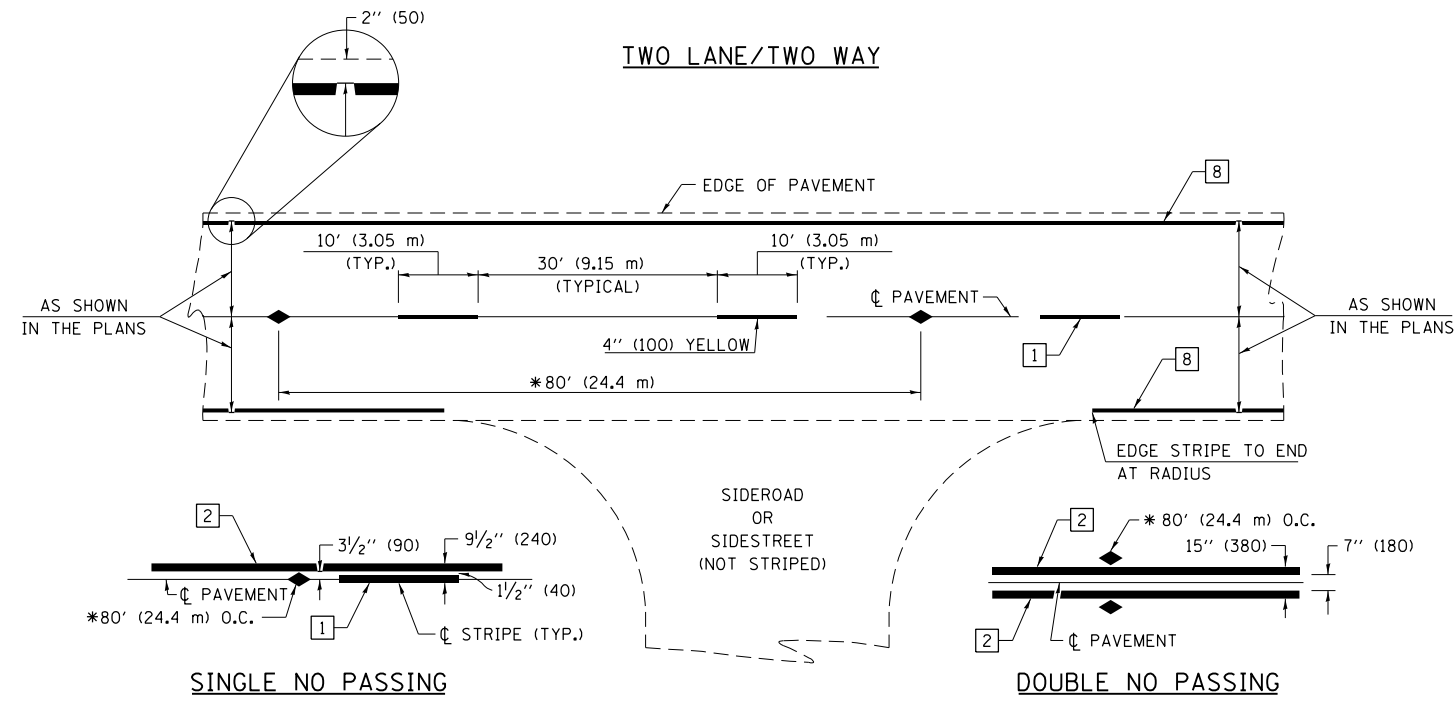
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	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DISTRICT DETAILS

SCALE: SHEET OF SHEETS STA. TO STA.

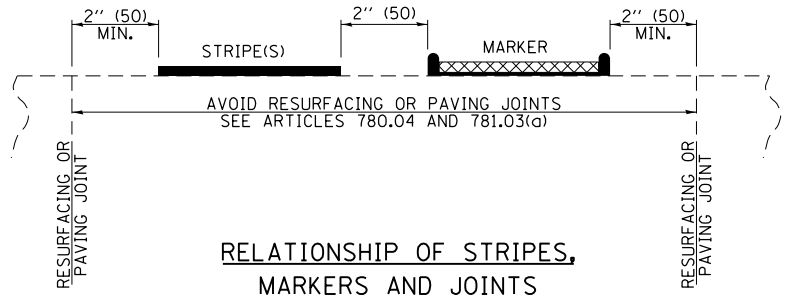
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64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	37
CONTRACT NO. 74982				
ILLINOIS FED. AID PROJECT				



* REDUCE TO 40' (12.2 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEEDS OF 45 mph (70 km/h) OR LESS.

PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 12" (300) SOLID WHITE
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) PARKING WHITE

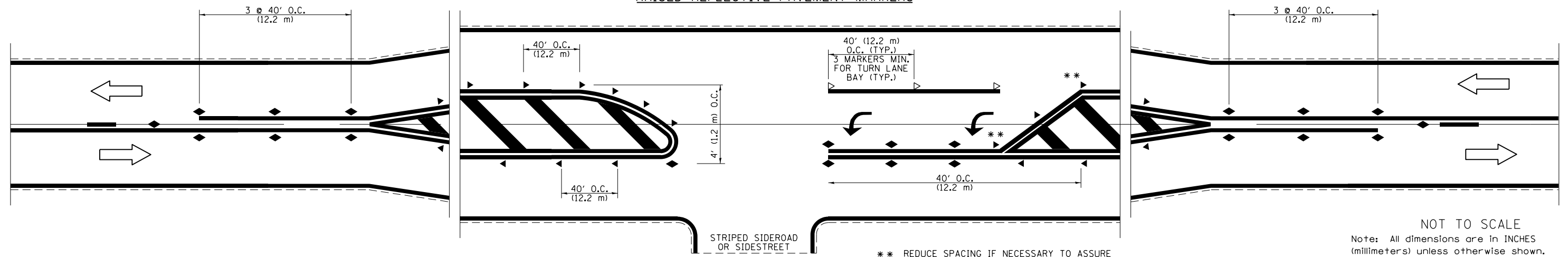


RELATIONSHIP OF STRIPES, MARKERS AND JOINTS

TYPICAL PAVEMENT MARKERS LEGEND

- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

RAISED REFLECTIVE PAVEMENT MARKERS



** REDUCE SPACING IF NECESSARY TO ASSURE MARKERS AT CORNER POINTS.

NOT TO SCALE
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

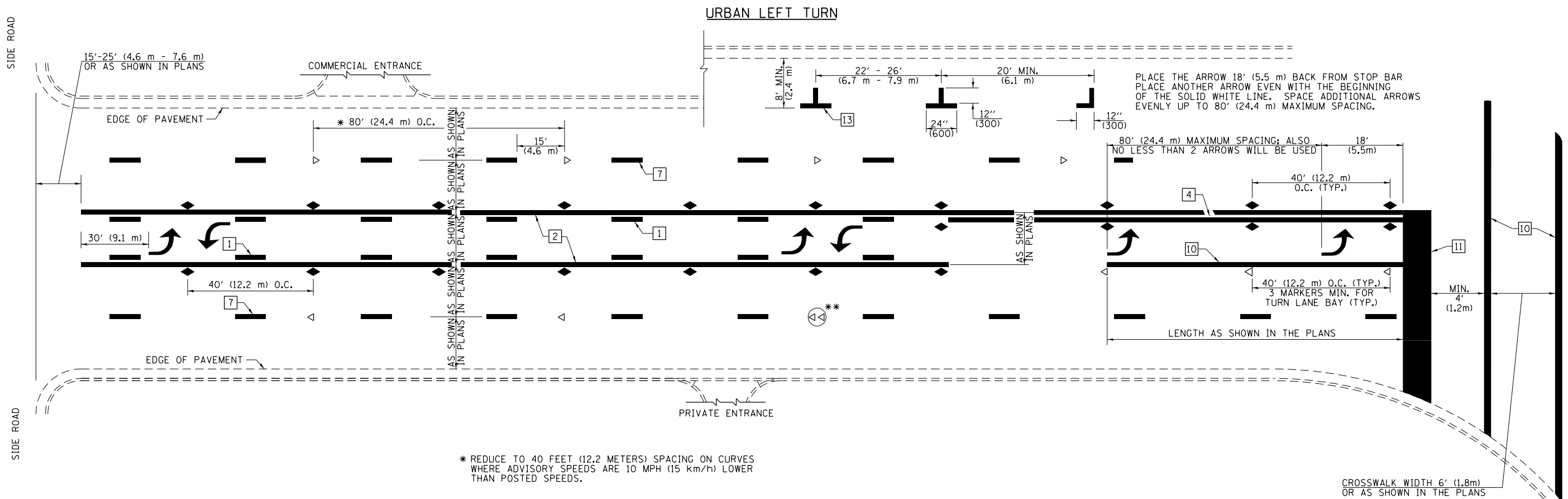
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 8/12/2020	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS
(RURAL & URBAN APPLICATIONS)

SCALE: SHEET NO. 1 OF 4 SHEETS STA. TO STA.

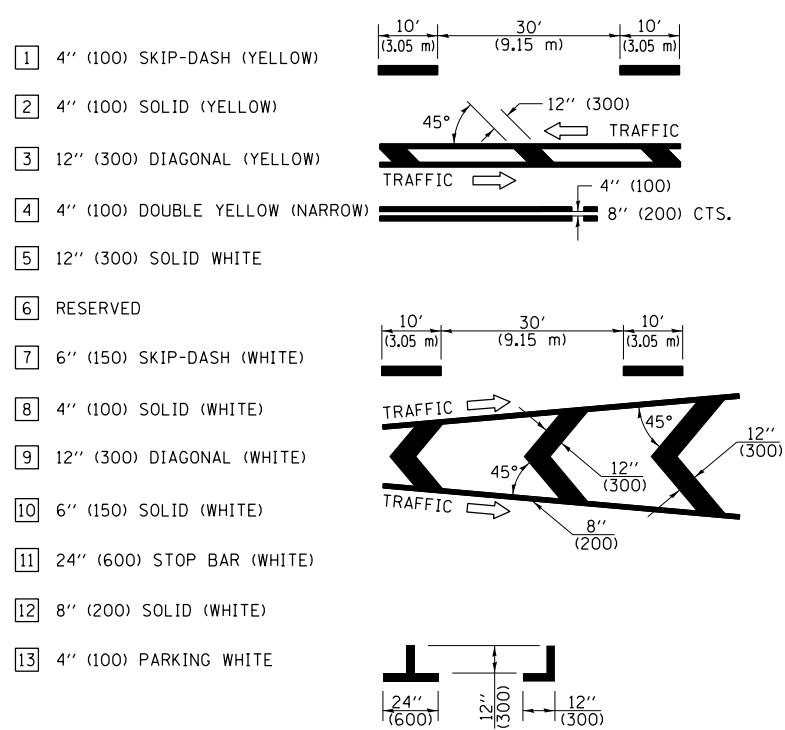
DISTRICT 7 DETAIL NO. 7800001				
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-12-1,3,3-1,4RS-1	WAYNE	43	38
CONTRACT NO. 74982				
ILLINOIS FED. AID PROJECT				



* REDUCE TO 40 FEET (12.2 METERS) SPACING ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.

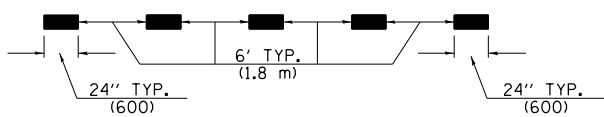
** DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

PAVEMENT MARKING LEGEND

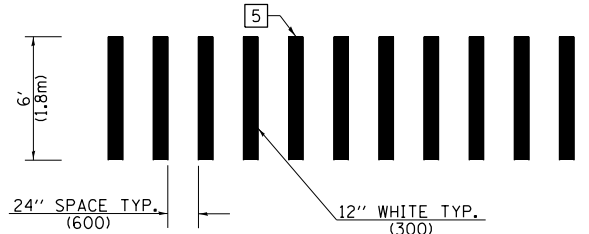


GENERAL NOTES

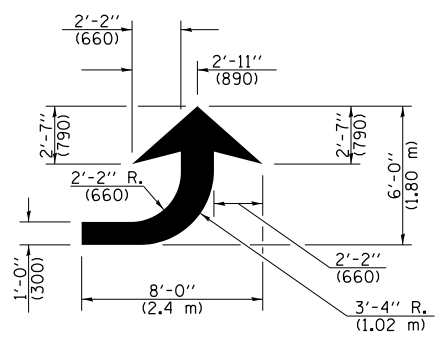
- TURN ARROW PAIRS SHALL BE PLACED AT 250' (75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE. USE A MINIMUM OF TWO PAIRS PER BLOCK.
- THE SOLID YELLOW PAVEMENT MARKINGS [2] SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
- THE SKIP-DASH PAVEMENT MARKINGS [1] OR [7] SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER.
- USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE LAST PAGE OF SECTION 780x FOR SYMBOLS TABLE)
- LANE LINE EXTENSIONS SHALL BE THE SAME COLOR AND WIDTH AS THE LANE LINE BEING EXTENDED.



LANE LINE EXTENSIONS

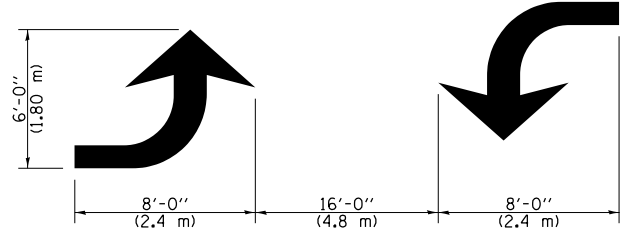


CROSSWALK DETAIL (DECATUR CITY LIMITS ONLY)



LEFT ARROW

REVERSE FOR RIGHT ARROW
AREA = 15.6 SQ. FT. (1.47 m²)
(WHITE)



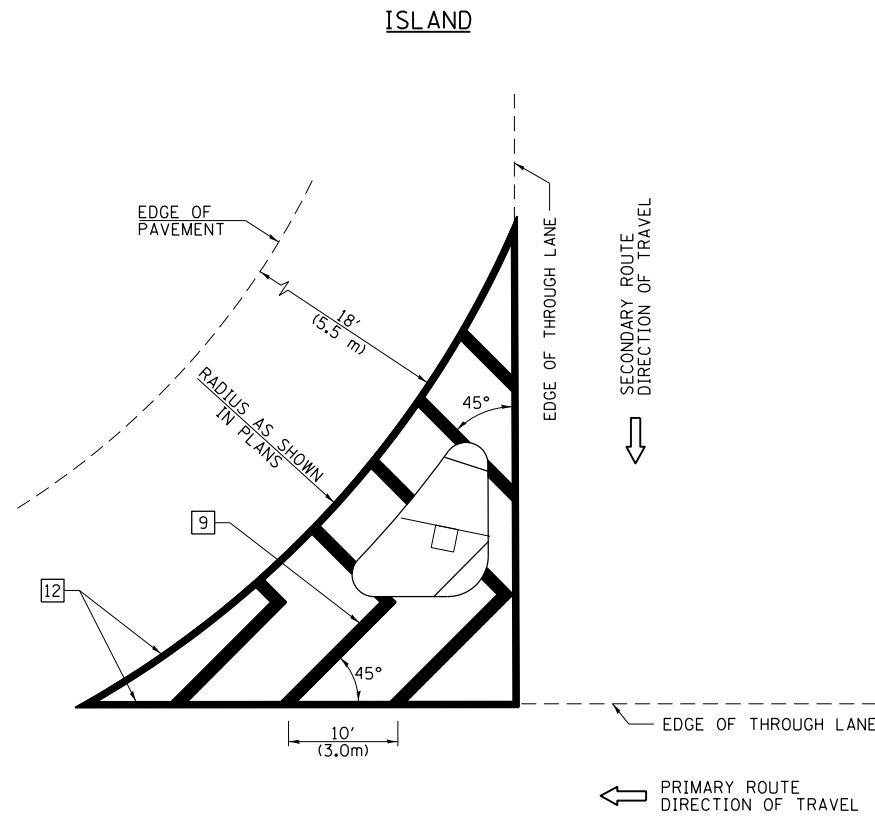
TYPICAL DOUBLE TURN ARROWS (WHITE)

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 7800001

FILE NAME =	USER NAME = steffennk	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 7\Projects\74982\DRAWN\CADsheets\0774982-sht-details.dgn		CHECKED -	REVISED -			64	96-(2-1,3,3-1,4)RS1	WAYNE	43	39
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -			CONTRACT NO. 74982				
PLOT DATE = 8/12/2020						ILLINOIS FED. AID PROJECT				



GENERAL NOTES

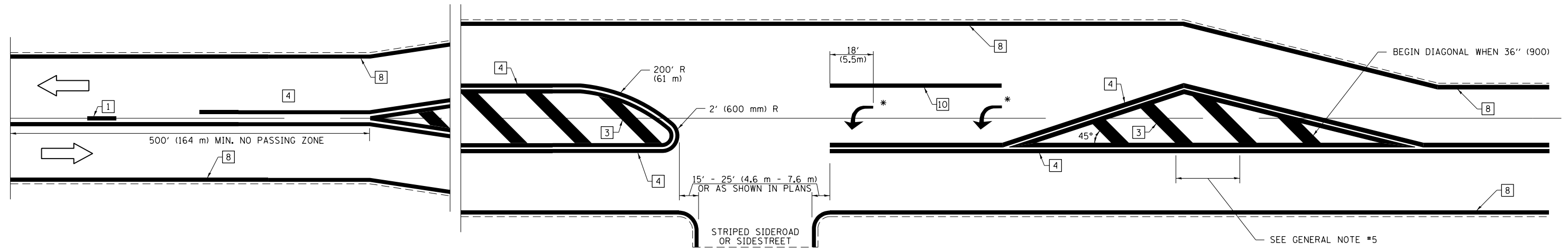
1. RAISED AND CORRUGATED MEDIANS SHALL BE OUTLINED WITH [2] IF PRESENT.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
5. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING:

< 30 MPH (< 50 km/h)	15' (4.5 m)
30-45 MPH (50-75 km/h)	20' (6.0 m)
> 45 MPH (> 75 km/h)	30' (9.0 m)

PAVEMENT MARKING LEGEND

- [1] 4" (100) SKIP-DASH (YELLOW)
 - [2] 4" (100) SOLID (YELLOW)
 - [3] 12" (300) DIAGONAL (YELLOW)
 - [4] 4" (100) DOUBLE YELLOW (NARROW)
 - [5] 12" (300) SOLID WHITE
 - [6] RESERVED
 - [7] 6" (150) SKIP-DASH (WHITE)
 - [8] 4" (100) SOLID (WHITE)
 - [9] 12" (300) DIAGONAL (WHITE)
 - [10] 6" (150) SOLID (WHITE)
 - [11] 24" (600) STOP BAR (WHITE)
 - [12] 8" (200) SOLID (WHITE)
 - [13] 4" (100) PARKING WHITE
-

RURAL LEFT TURN STRIPING



* PLACE AN ARROW 18' (5.5 m) BACK FROM STOP BAR. PLACE ANOTHER ARROW EVEN WITH THE BEGINNING OF THE SOLID WHITE LINE. SPACE ADDITIONAL ARROWS EVENLY UP TO 80' (24.4 m) MAXIMUM SPACING. USE MINIMUM OF 2 ARROWS.

NOT TO SCALE
 Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 7800001

FILE NAME =	USER NAME = steffenmk	DESIGNED -	REVISED -
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PLOT DATE = 8/12/2020	DATE -	REVISION	REVISION

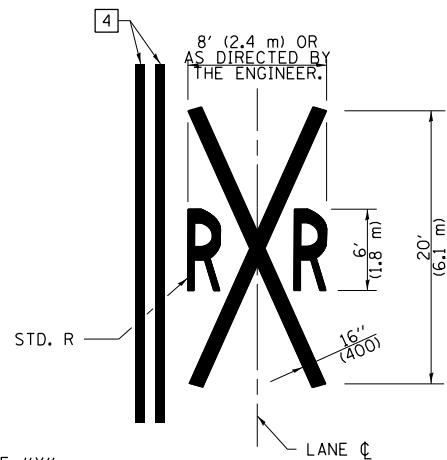
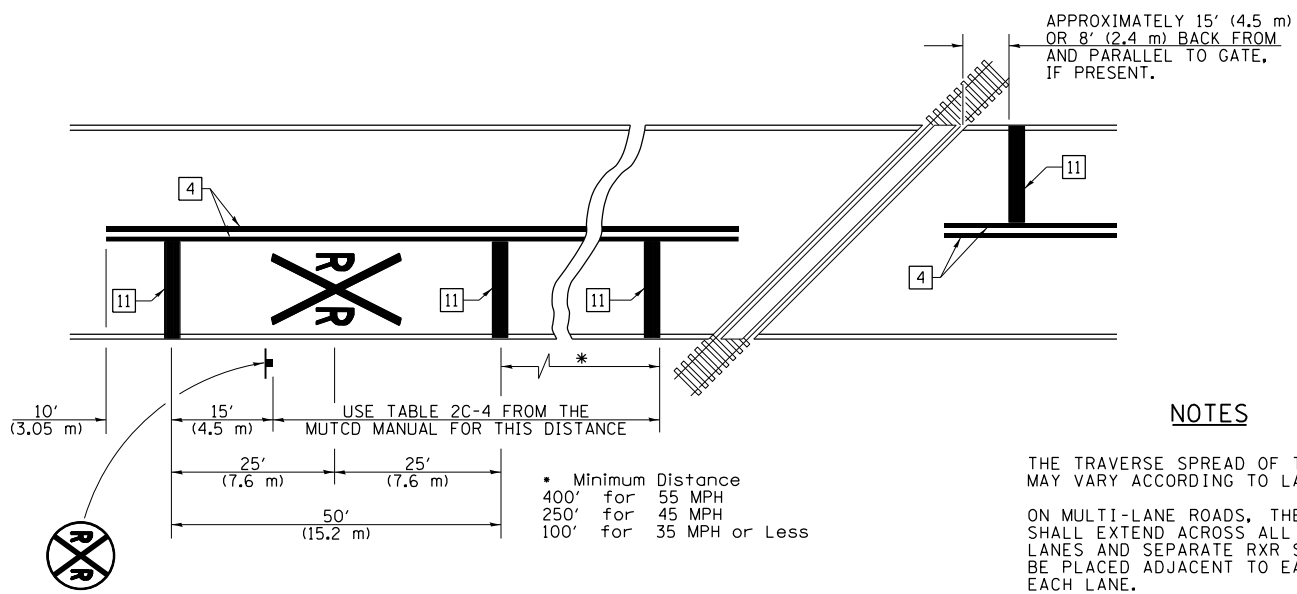
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS
 (RURAL & URBAN APPLICATIONS)**

SCALE: SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-12-1,3,3-1,4RS-1	WAYNE	43	40
CONTRACT NO. 74982				
ILLINOIS FED. AID PROJECT				

PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING



NOTES

THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

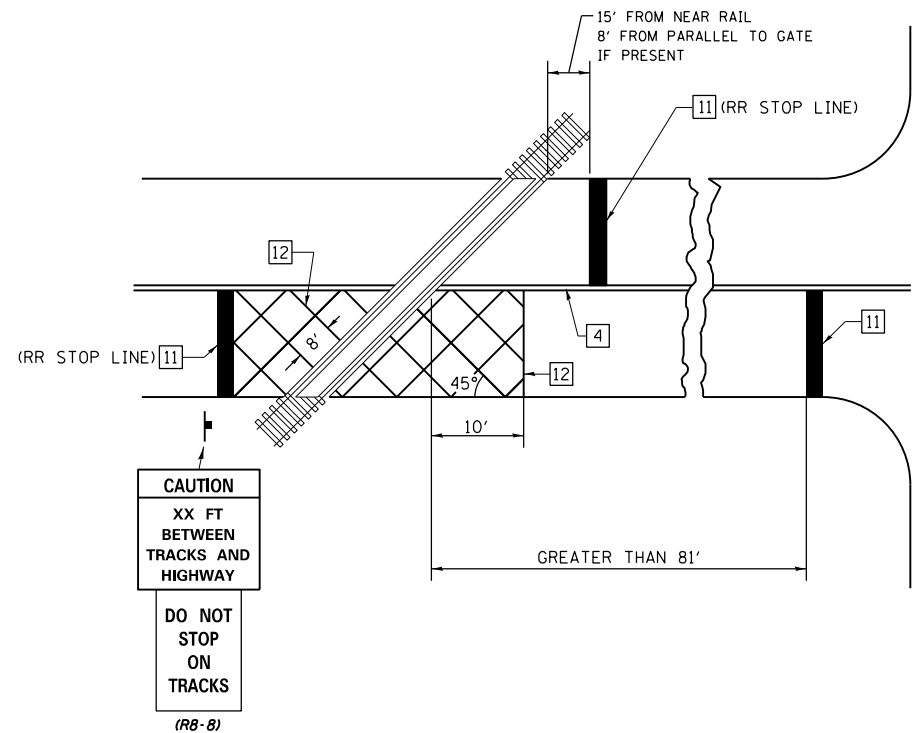
ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.

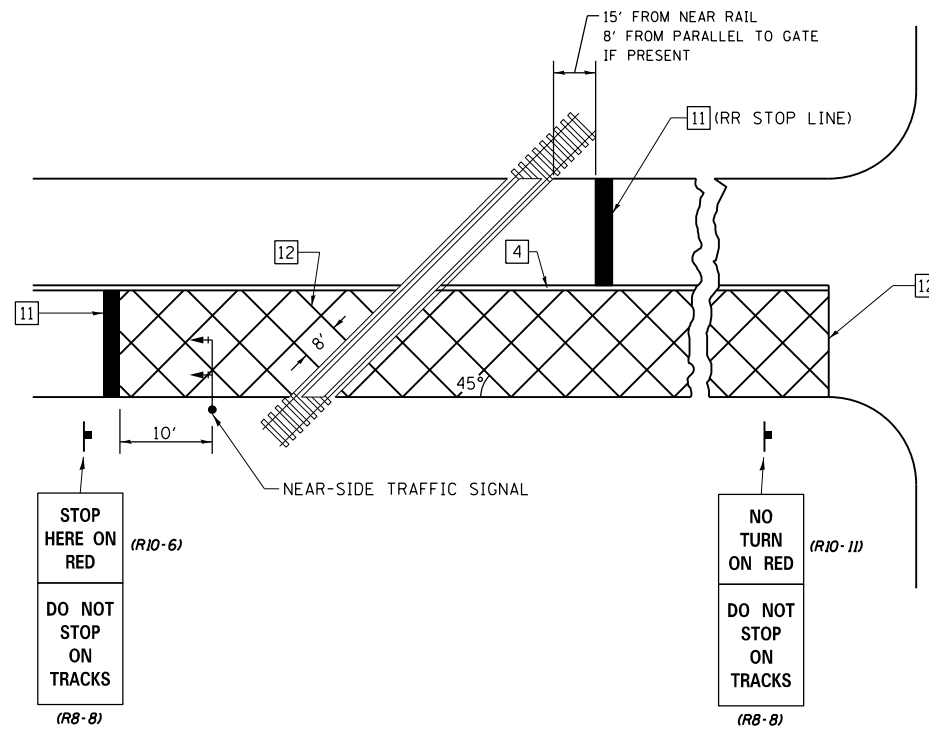
PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 12" (300) SOLID WHITE
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) PARKING WHITE

RAILROAD CROSSING WITH INTERCONNECT ONLY



RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



GENERAL NOTES

- SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.

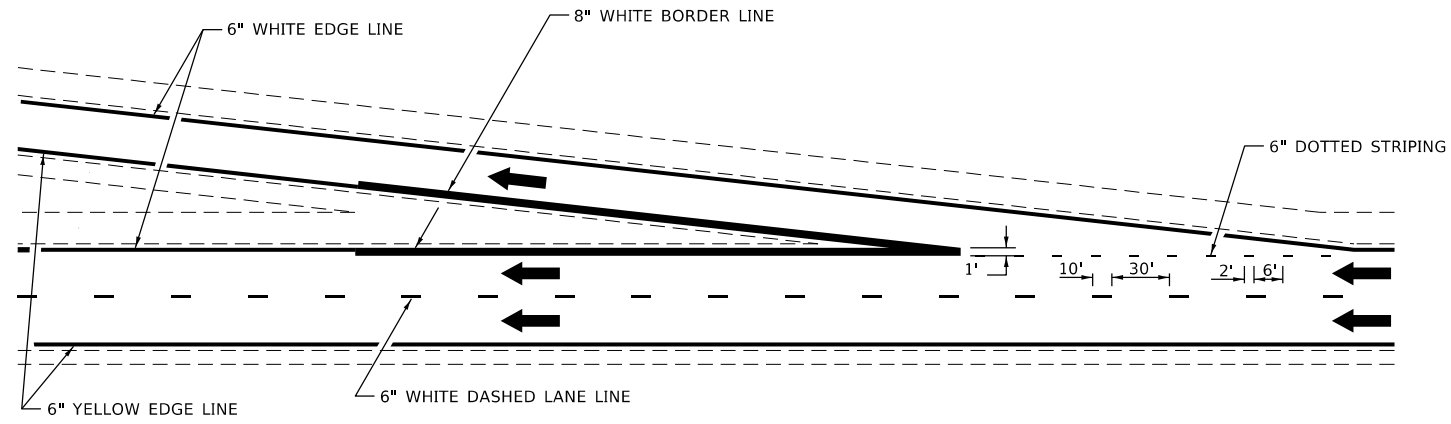
SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING

NOT TO SCALE

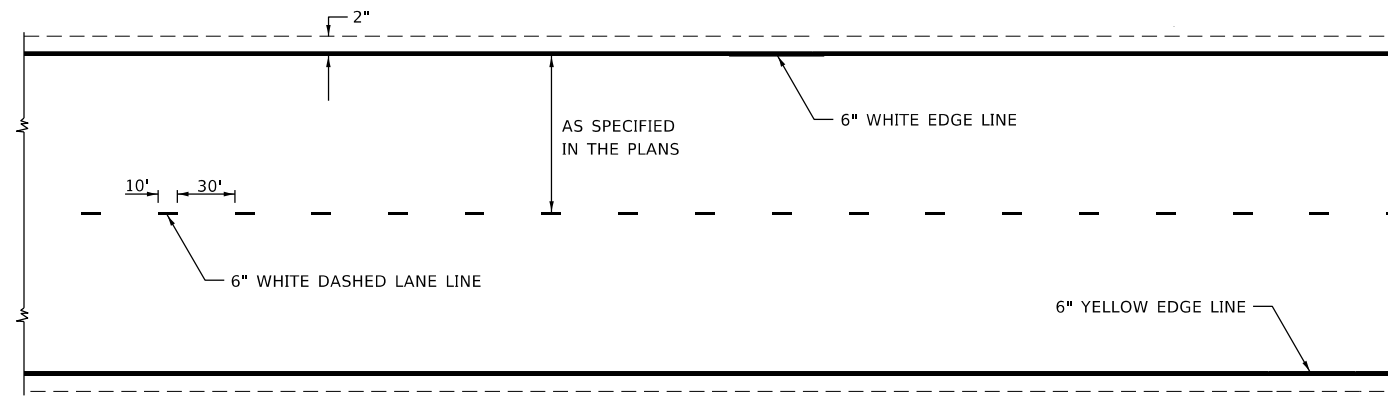
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 7800001

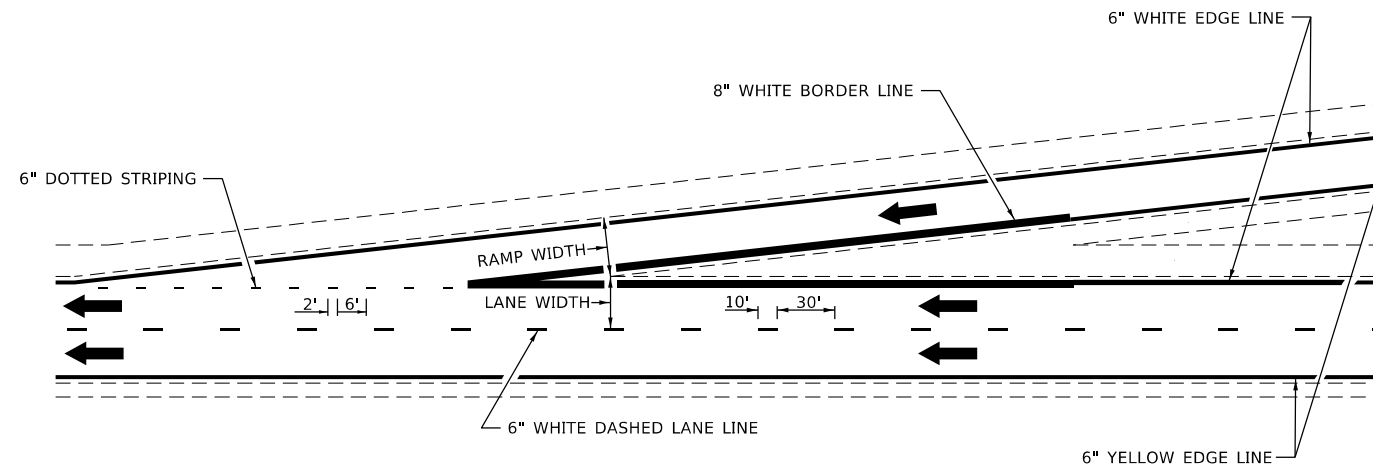
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pw:\planroom.dot.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 7\Projects\74982\DRAWN\CADsheets\0774982-sht-details.dgn		CHECKED -	REVISED -			64	96-12-1,3,3-1,4RS-1	WAYNE	43	41
PLOT SCALE = 100.0000' / 1in.		DATE -	REVISED -			CONTRACT NO. 74982				
PLOT DATE = 8/12/2020						ILLINOIS FED. AID PROJECT				



TYPICAL EXIT RAMP MARKING



TYPICAL CENTERLING & EDGELINE MARKINGS



TYPICAL ENTRANCE RAMP MARKING

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	DRAWN -	REVISED - DRM 01-09
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PLOT DATE = 8/12/2020	DATE -	REVISED - MAD 01-20

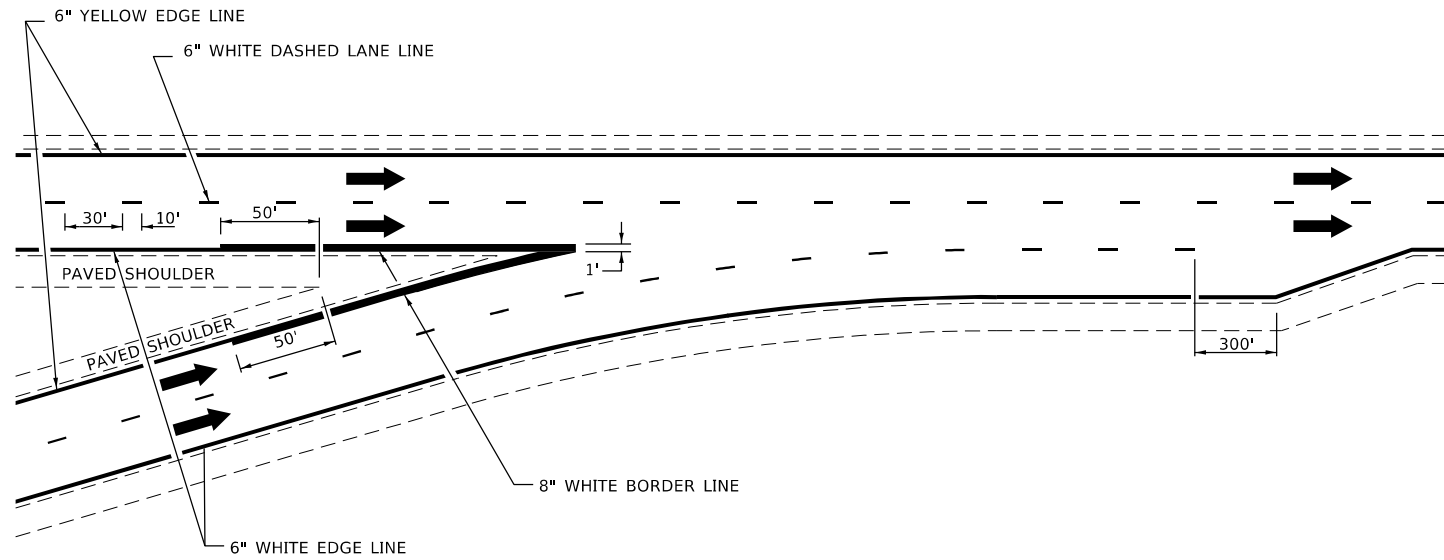
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS OF FREEWAY/EXPRESSWAY
PAVEMENT MARKING

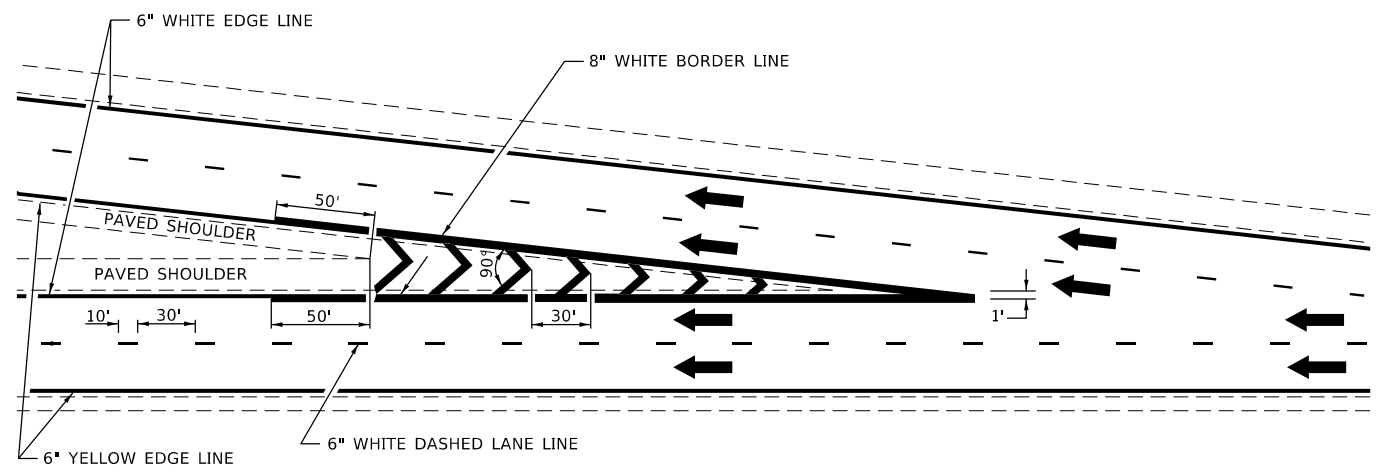
SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

DISTRICT 7 DETAIL NO. 7800002

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 74982	
			ILLINOIS FED. AID PROJECT	

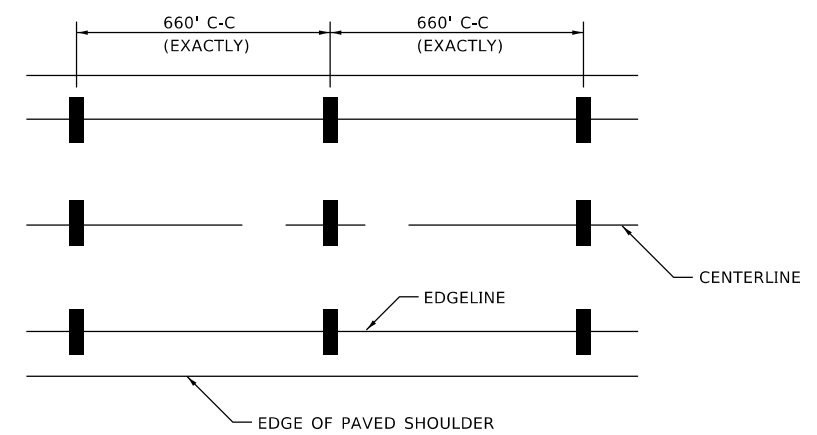


TYPICAL CONVERGENCE MARKING

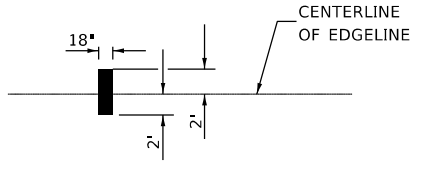


TYPICAL DIVERGENCE MARKING

AERIAL SPEED CHECK ZONES



IT WILL BE NECESSARY TO HAVE A REPRESENTATIVE OF THE STATE POLICE PRESENT SO THAT THE ACCURACY OF MEASUREMENT CAN BE ATTESTED TO IN COURT.



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PLOT DATE = 8/12/2020	DATE -	REVISED - DRM 01-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS OF FREEWAY/EXPRESSWAY
 PAVEMENT MARKING

SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.

DISTRICT 7 DETAIL NO. 7800002

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	96-(2-1,3,3-1,4)RS-1	WAYNE	43	43
CONTRACT NO. 74982				
ILLINOIS FED. AID PROJECT				