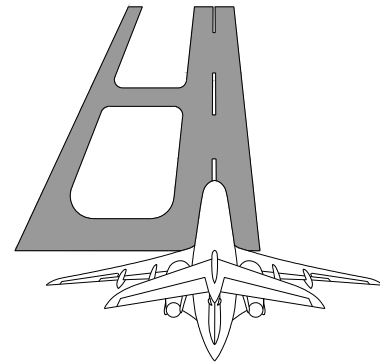


03A

FR048
TOTAL SHEETS:
17

CITY OF FREEPORT FREEPORT, ILLINOIS

CONSTRUCTION PLANS FOR FREEPORT-ALBERTUS AIRPORT REHABILITATE RUNWAY 6/24 PAPI SYSTEM



FREEPORT-ALBERTUS AIRPORT

APPROVED BY Angie Janssen
ANGIE JANSSSEN, AIRPORT MANAGER
DATE 09/12/2025



COMMON GROUND ALLIANCE
www.call811.com or
Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

TOWNSHIP: 26 NORTH
RANGE: 8 EAST
SECTION: 21

COUNTY: STEPHENSON
TOWNSHIP: SILVER CREEK

Illinois Professional Engineering Practice Act of 1989



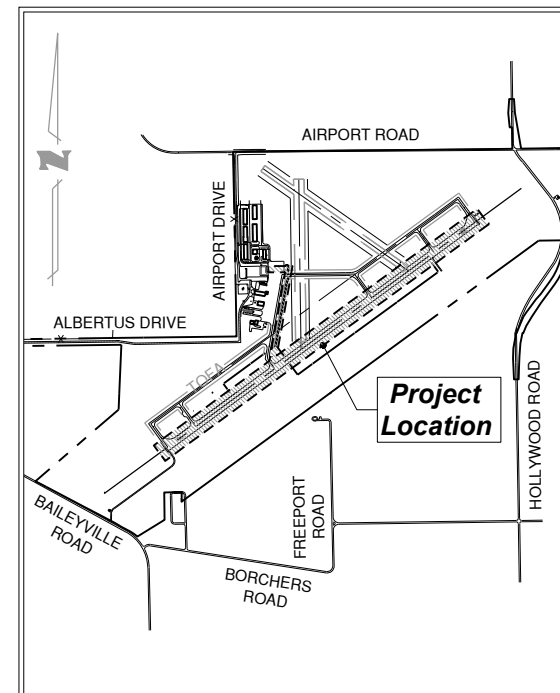
SUBMITTED BY Sheue Torng Lee
SHEUE TORNG LEE, P.E.

DATE SEPTEMBER 12, 2025
License Expires 11/30/2025

ILLINOIS PROJECT: FEP-5057
SBG PROJECT: 3-17-SBGP-TBD
SEPTEMBER 12, 2025
(NOVEMBER 7, 2025 LETTING)



LOCATION MAP



SITE PLAN

DESIGN INFORMATION

RUNWAY 6/24

AIRCRAFT DESIGN GROUP II
DESIGN APPROACH CATEGORY B
RUNWAY SAFETY AREA WIDTH = 150'

MAXIMUM EQUIPMENT HEIGHT - 25'
UNICOM FREQUENCY - 122.80

CMT DESIGN NUMBER: 22004977.00

INDEX TO SHEETS

- 1. COVER SHEET
- 2. INDEX TO SHEETS - SUMMARY OF QUANTITIES -GENERAL NOTES
- 3. SITE PLAN-PROJECT CONTROL PLAN
- 4. CONSTRUCTION ACTIVITY PLAN NOTES - 1
- 5. CONSTRUCTION ACTIVITY PLAN NOTES - 2
- 6. CONSTRUCTION ACTIVITY PLAN INDEX
- 7. CONSTRUCTION ACTIVITY PLAN - WORK AREA 1
- 8. CONSTRUCTION ACTIVITY PLAN - WORK AREA 2
- 9. CONSTRUCTION ACTIVITY PLAN - WORK AREA 3, 4 AND 5
- 10. CONSTRUCTION ACTIVITY PLAN DETAILS
- 11. ELECTRICAL PLAN - 1
- 12. ELECTRICAL PLAN - 2
- 13. ELECTRICAL PLAN - 3
- 14. ELECTRICAL PLAN - 4
- 15. ELECTRICAL DETAILS
- 16. PAPI DETAILS
- 17. ELECTRICAL VAULT PLAN

SUMMARY OF QUANTITIES

ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR110013	3" DIRECTIONAL BORE	FOOT	335	
AR125565	SPLICE CAN	EACH	10	
AR125615	PAPI (L-880 SYSTEM)	EACH	2	
AR125908	REMOVE PAPI	EACH	2	
AR150520	MOBILIZATION	L SUM	1	
AR800164	2 1/2" #6 XLP-USE, 1/2" #10 GND - 1" UD	FOOT	3,190	
AR800167	2 1/2" #10 XLP-USE, 1/2" #10 GND - 1" UD	FOOT	5,520	

STORM WATER POLLUTION PREVENTION NOTES:

THE FOLLOWING IS TO ESTABLISHED BEST MANAGEMENT PRACTICES TO BE INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REPLACING PAPI UNITS AND REMOVAL AND INSTALLATION OF ELECTRICAL AND MISCELLANEOUS ITEMS AT FREEPORT-ALBERTUS AIRPORT.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES:

- 1. INSTALL TEMPORARY EROSION CONTROL MEASURES.
- 2. REMOVAL AND INSTALLATION OF PAPI UNITS, ELECTRICAL AND MISCELLANEOUS ITEMS.
- 3. SEEDING AND MULCHING.
- 4. REMOVAL AND DISPOSAL OF TEMPORARY EROSION CONTROL MEASURES.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE THAT WILL BE DISTURBED BY TRENCHING, GRADING AND OTHER ACTIVITIES IS LESS THAN ONE (1) ACRE.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE PEGATONICA RIVER THROUGH A STORM SEWER SYSTEM.

SEDIMENTATION AND EROSION CONTROL NOTES

THE RESIDENT ENGINEER WILL PERFORM PERIODIC INSPECTION OF THE SITE TO IDENTIFY POTENTIAL SEDIMENT AND EROSION ISSUES.

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL, UNLESS OTHERWISE STATED.

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED WITHIN SEVEN (7) DAYS OF DISTURBANCE.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
- B. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF OFF SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE COST OF THE PROJECT.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION

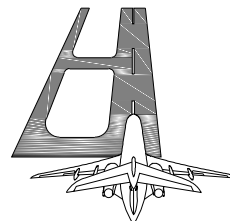
CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

SOIL EROSION AND SEDIMENT CONTROL NOTES:

- 1. ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
- 2. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
- 3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
- 4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- 5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
- 6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
- 7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, STEPHENSON COUNTY, FREEPORT-ALBERTUS AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
- 9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
- 10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
- 11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
- 12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
- 13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
- 14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
- 15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL. GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
- 16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

STAGING AND PROTECTION NOTES:

- 1. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE STORM WATER POLLUTION PREVENTION DEVICES AT THE STAGING AREA. THESE DEVICES, INCLUDING INLET PROTECTIONS, SILT FENCE, BALES, DITCH CHECKS, STABILIZED CONSTRUCTION ENTRANCES, ETC., SHALL NOT BE PAID FOR, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- 2. AT THE COMPLETION OF WORK, THE STAGING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS, AT NO ADDITIONAL COST TO THE CONTRACT. THIS WORK SHALL INCLUDE REMOVING ANY AND ALL STORM WATER POLLUTION PREVENTION DEVICES, RESTORING RUTTED AREAS, SEEDING AND MULCHING.
- 3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE STAGING AREA(S) IN A MANNER TO PREVENT POLLUTION AND SILTATION OF THE EXISTING STORM SEWER SYSTEM.
- 4. ALL EQUIPMENT FUELING AND GREASING SHALL BE COMPLETED AT THE STAGING AREA. SPILLS SHALL BE IMMEDIATELY CONTAINED AND THE AREA CLEANED AT NO ADDITIONAL COST THE CONTRACT.



FREEPORT - ALBERTUS AIRPORT



ENGINEERS AND CONSULTANTS

FREEPORT - ALBERTUS AIRPORT
FREEPORT, IL

CITY OF FREEPORT
ILLINOIS

APPROVED AS WORKING PLAN BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 6/24
PAPI SYSTEM

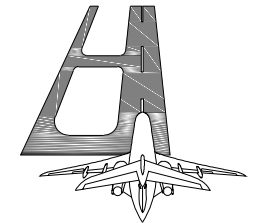
SHEET TITLE:
INDEX TO SHEETS -
SUMMARY OF QUANTITIES -
GENERAL NOTES

DESIGNED: STL DRAWN: JRO CHECKED: DJK

PROJECT NO.: 22004977-00
DATE: 09/12/2025

SHEET NO. REVISION

SHEET 2 OF 17 SHEETS



FREEPORT - ALBERTUS AIRPORT



FREEPORT - ALBERTUS AIRPORT
FREEPORT, IL

CITY OF FREEPORT
ILLINOIS

APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 6/24
PAPI SYSTEM

SHEET TITLE:

SITE PLAN-PROJECT
CONTROL PLAN

DESIGNED: STL DRAWN: JRO CHECKED: DJK

PROJECT NO.: 22004977-00
DATE: 09/12/2025

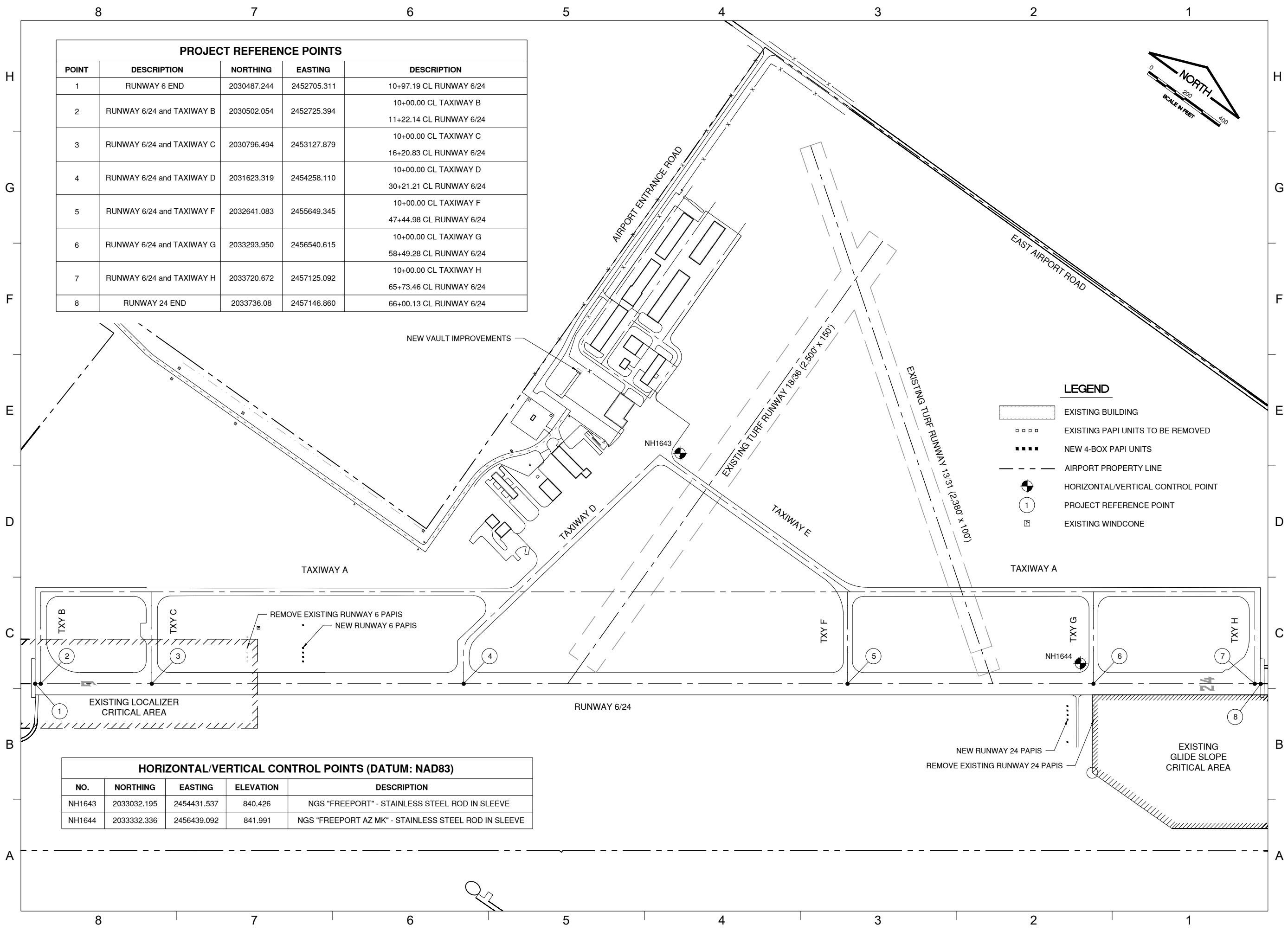
SHEET NO. REVISION

SHEET 3 OF 17 SHEETS

PROJECT REFERENCE POINTS				
POINT	DESCRIPTION	NORTHING	EASTING	DESCRIPTION
1	RUNWAY 6 END	2030487.244	2452705.311	10+97.19 CL RUNWAY 6/24
2	RUNWAY 6/24 and TAXIWAY B	2030502.054	2452725.394	10+00.00 CL TAXIWAY B 11+22.14 CL RUNWAY 6/24
3	RUNWAY 6/24 and TAXIWAY C	2030796.494	2453127.879	10+00.00 CL TAXIWAY C 16+20.83 CL RUNWAY 6/24
4	RUNWAY 6/24 and TAXIWAY D	2031623.319	2454258.110	10+00.00 CL TAXIWAY D 30+21.21 CL RUNWAY 6/24
5	RUNWAY 6/24 and TAXIWAY F	2032641.083	2455649.345	10+00.00 CL TAXIWAY F 47+44.98 CL RUNWAY 6/24
6	RUNWAY 6/24 and TAXIWAY G	2033293.950	2456540.615	10+00.00 CL TAXIWAY G 58+49.28 CL RUNWAY 6/24
7	RUNWAY 6/24 and TAXIWAY H	2033720.672	2457125.092	10+00.00 CL TAXIWAY H 65+73.46 CL RUNWAY 6/24
8	RUNWAY 24 END	2033736.08	2457146.860	66+00.13 CL RUNWAY 6/24

LEGEND

- EXISTING BUILDING
- EXISTING PAPI UNITS TO BE REMOVED
- NEW 4-BOX PAPI UNITS
- AIRPORT PROPERTY LINE
- HORIZONTAL/VERTICAL CONTROL POINT
- PROJECT REFERENCE POINT
- EXISTING WINDCONE



HORIZONTAL/VERTICAL CONTROL POINTS (DATUM: NAD83)				
NO.	NORTHING	EASTING	ELEVATION	DESCRIPTION
NH1643	2033032.195	2454431.537	840.426	NGS "FREEPORT" - STAINLESS STEEL ROD IN SLEEVE
NH1644	2033332.336	2456439.092	841.991	NGS "FREEPORT AZ MK" - STAINLESS STEEL ROD IN SLEEVE

GENERAL

1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G OR LATEST EDITION, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE NOTICE TO PROCEED, THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT THROUGH THE RESIDENT ENGINEER, FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G OR LATEST EDITION. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

1. COORDINATION

1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRE-CONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRE-CONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

1. TOTAL CONTRACT TIME SHALL BE 37 CALENDAR DAYS.
2. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

3. WORK AREAS AND DESCRIPTIONS

WORK AREA 1 (RUNWAY 6-24 AND TAXIWAY D (SOUTH OF TAXIWAY A) CLOSED)

WORK AREA 1 GENERALLY INCLUDES AIRFIELD CABLING, PAPI INSTALLATION, AND DIRECTIONAL BORING OF CONDUIT WITHIN THE RSA OF RUNWAY 6-24 AND TOFA OF TAXIWAY D.

WORK AREA 2A (RUNWAY 6-24 AND RUNWAY 13-31 CLOSED)

WORK AREA 2A GENERALLY INCLUDES AIRFIELD CABLING AND PAPI INSTALLATION WITHIN THE RSA OF RUNWAY 6-24 AND RPZ OF RUNWAY 13-31.

WORK AREA 2B (RUNWAY 6-24 AND RUNWAY 13-31 CLOSED)

WORK AREA 2B GENERALLY INCLUDES AIRFIELD CABLING WITHIN THE RSA OF RUNWAY 6-24 AND RPZ OF RUNWAY 18-36.

WORK AREA 3 (ALL RUNWAYS AND TAXIWAYS REMAIN OPENED)

WORK AREA 3 GENERALLY INCLUDES AIRFIELD CABLING OUTSIDE THE RSA OF ALL RUNWAYS AND TOFA OF ALL TAXIWAYS.

WORK AREA 4 (TAXIWAY E CLOSED)

WORK AREA 4 GENERALLY INCLUDES AIRFIELD CABLING AND DIRECTIONAL BORING OF CONDUIT WITHIN THE TOFA OF TAXIWAY E.

WORK AREA 5 (ALL RUNWAYS AND TAXIWAYS REMAIN OPENED)

WORK AREA 5 GENERALLY INCLUDES HOMERUN PAPI AIRFIELD CABLING TO THE EXISTING ELECTRICAL VAULT AND VAULT MODIFICATIONS.

4. RESTRICTIONS

GENERAL (ALL WORK AREAS)

ALL WORK AREAS WITHIN AN RSA OR TOFA MUST BE RESTORED AND INSPECTED TO MEET FAA SAFETY CRITERIA PER THE CONTRACTOR'S CSPP PRIOR TO RETURNING THE AREA TO AIR TRAFFIC OPERATIONS. SEE CSPP NOTES IN THE PLANS FOR ADDITIONAL INFORMATION ON INSPECTION REQUIREMENTS.

ALL WORK AREAS ARE LOCATED INSIDE THE AOA SECURITY FENCE, AND ARE SUBJECT TO THE LIMITATIONS DESCRIBED IN SPECIFICATIONS, PART 1 - GENERAL PROVISIONS.

WORK CONDUCTED UTILIZING EXTENDED CLOSURES OF AIRFIELD PAVEMENTS AS INDICATED IN THE PLANS WILL BE RESTRICTED TO WEEKDAYS ONLY BETWEEN THE HOURS OF 7:00 AM AND 5:00 PM. THE CLOSURES WILL ALLOW THE CONTRACTOR TO COMPLETE ALL WORK INSIDE THE RESPECTIVE TOFA OR RSA, AND TO A DISTANCE OUTSIDE THE TOFA OR RSA AS NOT TO ENCR OACH THE TOFA OR RSA WITH MANPOWER OR EQUIPMENT ONCE THE CLOSURE PERIOD IS CONCLUDED.

FOR ANY WORK ON THE AIRFIELD THAT DOES NOT REQUIRE A RUNWAY CLOSURE, WORK CAN BE CONDUCTED 24 HOURS A DAY, 7 DAYS A WEEK.

SIMULTANEOUS CLOSURES OF AIRFIELD PAVEMENTS TO COMPLETE WORK IN MORE THAN ONE WORK AREA AT A TIME WILL NOT BE ALLOWED EXCEPT AS SPECIFICALLY NOTED.

WORK AREA 1 (RUNWAY 6-24 AND TAXIWAY D (SOUTH OF TAXIWAY A) CLOSED)

SINGLE DAYTIME CLOSURES OF RUNWAY 6-24 AND TAXIWAY D ARE REQUIRED TO COMPLETE WORK WITHIN THE RSA OF RUNWAY 6-24 AND THE TOFA OF TAXIWAY D (SOUTH OF TAXIWAY A). AN EXTENDED CLOSURE OF RUNWAY 6-24 AND TAXIWAY D WILL NOT BE PERMITTED AT ANY TIME. SCHEDULED DAYTIME CLOSURE PERIODS SHALL BE LIMITED TO A 10-HOUR WINDOW (7:00 AM - 5:00 PM) ON WEEKDAYS ONLY. ALL LOCATIONS WITHIN THE RSA AND TOFA WILL BE RESTORED TO FAA SAFETY CRITERIA BEFORE RE-OPENING THE PAVEMENT TO AIR TRAFFIC OPERATIONS AT THE END OF THE CLOSURE PERIOD.

WORK WITHIN WORK AREA 1 CAN BE COMPLETED CONCURRENTLY WITH WORK AREA 2A, WORK AREA 2B, AND WORK AREA 3.

WORK AREA 2A (RUNWAY 6-24 AND RUNWAY 13-31 CLOSED)

SINGLE DAYTIME SIMULTANEOUS CLOSURES OF RUNWAY 6-24 AND RUNWAY 13-31 ARE REQUIRED TO COMPLETE WORK WITHIN THE RSA OF RUNWAY 6-24 AND THE RPZ OF RUNWAY 13-31. EXTENDED CLOSURES OF RUNWAY 6-24 AND RUNWAY 13-31 WILL NOT BE PERMITTED AT ANY TIME. SCHEDULED DAYTIME CLOSURE PERIODS SHALL BE LIMITED TO A 10-HOUR WINDOW (7:00 AM - 5:00 PM) ON WEEKDAYS ONLY. ALL LOCATIONS WITHIN THE RSA WILL BE RESTORED TO FAA SAFETY CRITERIA BEFORE RE-OPENING THE PAVEMENT TO AIR TRAFFIC OPERATIONS AT THE END OF THE CLOSURE PERIOD.

WORK WITHIN EXPEDITED WORK AREA 2A SHALL BE COMPLETED CONCURRENTLY WITH WORK AREA 1.

WORK AREA 2B (RUNWAY 6-24 AND RUNWAY 18-36 CLOSED)

SINGLE DAYTIME SIMULTANEOUS CLOSURES OF RUNWAY 6-24 AND RUNWAY 18-36 ARE REQUIRED TO COMPLETE WORK WITHIN THE RSA OF RUNWAY 6-24 AND THE RPZ OF RUNWAY 18-36. EXTENDED CLOSURES OF RUNWAY 6-24 AND RUNWAY 18-36 WILL NOT BE PERMITTED AT ANY TIME. SCHEDULED DAYTIME CLOSURE PERIODS SHALL BE LIMITED TO A 10-HOUR WINDOW (7:00 AM - 5:00 PM) ON WEEKDAYS ONLY. ALL LOCATIONS WITHIN THE RSA WILL BE RESTORED TO FAA SAFETY CRITERIA BEFORE RE-OPENING THE PAVEMENT TO AIR TRAFFIC OPERATIONS AT THE END OF THE CLOSURE PERIOD.

WORK WITHIN EXPEDITED WORK AREA 2B SHALL BE COMPLETED CONCURRENTLY WITH WORK AREA 1.

WORK AREA 3 (ALL RUNWAYS AND TAXIWAYS REMAIN OPENED)

WORK WITHIN WORK AREA 3 CAN BE COMPLETED CONCURRENTLY WITH WORK AREA 1.

WORK AREA 4 (TAXIWAY E CLOSED)

SINGLE DAYTIME CLOSURES OF TAXIWAY E ARE REQUIRED TO COMPLETE WORK WITHIN THE TOFA OF TAXIWAY E. AN EXTENDED CLOSURE OF TAXIWAY E WILL NOT BE PERMITTED AT ANY TIME. SCHEDULED DAYTIME CLOSURE PERIODS SHALL BE LIMITED TO A 10-HOUR WINDOW (7:00 AM - 5:00 PM) ON WEEKDAYS ONLY. ALL LOCATIONS WITHIN THE TOFA WILL BE RESTORED TO FAA SAFETY CRITERIA BEFORE RE-OPENING THE PAVEMENT TO AIR TRAFFIC OPERATIONS AT THE END OF THE CLOSURE PERIOD.

WORK AREA 5 (ALL RUNWAYS AND TAXIWAYS REMAIN OPENED)

WORK WITHIN WORK AREA 5 CAN BE COMPLETED CONCURRENTLY WITH ALL OTHER WORK AREAS.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. NAVAIDS THAT COULD BE AFFECTED

1. THE CONTRACTOR MUST COORDINATE WITH AIRPORT OPERATIONS/ENGINEER IN ADVANCE FOR ANY WORK WITHIN A NAVAID CRITICAL AREA OR AFFECTING THE VISUAL, TRANSMITTED SIGNAL OR POWER SUPPLY OF A NAVAID.
2. EDGE LIGHTS, THRESHOLD LIGHTS, VISUAL AIDS AND ALL ILS EQUIPMENT SHALL BE SHUT OFF FOR THE DURATION OF A CLOSURE PERIOD ON ANY ASSOCIATED PAVEMENTS. IF THE LIGHTING CIRCUIT MUST BE ON FOR OPEN PAVEMENT AREAS, CLOSED PAVEMENT AREA LIGHTS SHALL BE COVERED COMPLETELY.
3. EXCEPT WHERE NOTED IN THE PLANS, EXISTING COMMUNICATIONS EQUIPMENT AND NAVIGATIONAL AIDS (NAVAIDS) SHALL NOT BE DISTURBED BY THE CONTRACTOR AND SHALL BE PROTECTED FROM DAMAGE.
4. PRIOR TO BEGINNING SITE WORK, CONTRACTOR SHALL COORDINATE THROUGH AIRPORT OPERATIONS TO LOCATE, AND THE CONTRACTOR TO MARK ALL UNDERGROUND COMMUNICATIONS CABLES AND FACILITIES, WITHIN THE PROJECT AREA.
5. IF CONTRACTOR CAUSES INTERRUPTION OF POWER OR COMMUNICATIONS TO A NAVAID CONTRACTOR SHALL REPAIR WITHIN 24 HOURS AT THE CONTRACTOR'S COST. CONTRACTOR MUST COORDINATE REPAIR WITH AIRPORT OPERATIONS BEFORE ANY REPAIR IS MADE.

5. CONTRACTOR ACCESS

1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATE(S) SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE CLOSED DURING WORK HOURS.
3. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
4. CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE MARKED AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
5. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, ASPHALT, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA FENCE, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR PERSONNEL THAT HAS OBTAINED PROPER DRIVING PRIVILEGES.
6. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIRPORT UNICOM (122.70 MHZ) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY SATISFIED THE AIRPORT OF THEIR COMPETENCE MAY OPERATE THESE RADIOS.
7. THE CONTRACTORS STORAGE AND STAGING AREAS WILL BE AS SHOWN ON THE SITE PLAN.
8. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
9. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREAS.

10. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
11. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
12. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE COMMON TRAFFIC ADVISORY FREQUENCY. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
13. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
14. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
15. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
16. THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

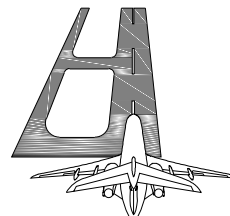
1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 10 DAYS NOTICE TO THE FAA AND AIRPORT PRIOR TO THE CLOSURE OF ANY RUNWAY SO THAT THE FAA MAY DEACTIVATE THE FAA - OWNED NAVAIDS.
3. THE CONTRACTOR SHALL GIVE A MINIMUM 30 DAYS NOTICE TO THE AIRPORT, AND PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, PRIOR TO CLOSING ANY RUNWAY OR TAXIWAY PAVEMENT SO THAT THE PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT FOR COORDINATION WITH THE AIRPORT TENANTS.
4. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
5. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.



FREEPORT - ALBERTUS AIRPORT



ENGINEERS AND CONSULTANTS

FREEPORT - ALBERTUS AIRPORT FREEPORT, IL

CITY OF FREEPORT ILLINOIS

APPROVED AS WORKING PLAN BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE RUNWAY 6/24 PAPI SYSTEM

SHEET TITLE:

CONSTRUCTION ACTIVITY PLAN NOTES - 1

DESIGNED: STL DRAWN: JRO CHECKED: DJK

PROJECT NO.: 22004977-00 DATE: 09/12/2025

SHEET NO. REVISION

SHEET 4 OF 17 SHEETS

10. INSPECTION REQUIREMENTS

1. THE CONTRACTOR SHALL INSPECT THE JOB SITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2G OR LATEST EDITION MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL REQUEST OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR TO THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

11. UNDERGROUND UTILITIES

1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. PENALTIES

1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2G OR LATEST EDITION.

15. MARKING AND SIGNS FOR ACCESS ROUTES

1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.

16. HAZARD MARKING AND LIGHTING

1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-50 OR LATEST EDITION AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

17. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

1. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTION.
2. LIGHTS SHALL CONSIST OF VEHICLE OR MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL NOT INTERFERE WITH AIR OPERATIONS. ANY WORK BEING PERFORMED UNDER INSUFFICIENT ARTIFICIAL LIGHTING, IN THE RESIDENT ENGINEER'S JUDGEMENT, SHALL BE STOPPED UNTIL SUCH TIME AS ADDITIONAL LIGHTING IS PROVIDED. ALL WORK PERFORMED DURING THAT TIME WILL NOT BE ACCEPTABLE UNTIL PROPER INSPECTION AND TESTING CAN BE MADE.

18. PROTECTION

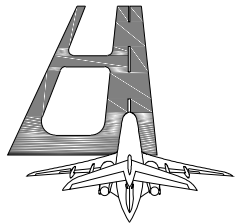
1. ALL WORK REQUIRED INSIDE OF A RUNWAY SAFETY AREA, WILL REQUIRE THE RUNWAY TO BE CLOSED.
2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WILL REQUIRE THE TAXIWAY TO BE CLOSED.

19. OTHER LIMITATIONS ON CONSTRUCTION

1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE CONTRACT DOCUMENTS.

CRITICAL POINT TABLE

POINT	WORK AREA	FACILITY CLOSED	NORTHING	EASTING	LATITUDE	LONGITUDE	GROUND ELEVATION	OBSTRUCTION HEIGHT (FT)	ABOVE GROUND ELEVATION
1	ALL	N/A	2033038.877	2453778.233	N42° 14' 48.93"	W89° 35' 09.88"	846	25	871
2	ALL	N/A	2033039.472	2453813.672	N42° 14' 48.93"	W89° 35' 09.41"	846	25	871
3	ALL	N/A	2032941.258	2453815.320	N42° 14' 47.96"	W89° 35' 09.39"	847	25	872
4	ALL	N/A	2032940.663	2453779.881	N42° 14' 47.96"	W89° 35' 09.87"	847	25	872
5	1	RWY 6/24 AND TXY D SOUTH OF TXY A	2029585.654	2451871.679	N42° 14' 14.94"	W89° 35' 35.54"	850	25	875
6	1	RWY 6/24 AND TXY D SOUTH OF TXY A	2029557.810	2451894.095	N42° 14' 14.67"	W89° 35' 35.24"	850	25	875
7	2B	RWY 6/24, TXY D SOUTH OF TXY A, AND RWY 18/36	2031462.587	2454500.345	N42° 14' 33.31"	W89° 35' 00.42"	846	25	871
8	2B	RWY 6/24, TXY D SOUTH OF TXY A, AND RWY 18/36	2031874.314	2455063.157	N42° 14' 37.34"	W89° 34' 52.90"	843	25	868
9	2A	RWY 6/24, TXY D SOUTH OF TXY A, AND RWY 13/31	2032678.229	2456163.915	N42° 14' 45.20"	W89° 34' 38.19"	840	25	865
10	2A	RWY 6/24, TXY D SOUTH OF TXY A, AND RWY 13/31	2033036.261	2456651.708	N42° 14' 48.71"	W89° 34' 31.67"	839	25	864
11	1	RWY 6/24 AND TXY D SOUTH OF TXY A	2034163.893	2458192.864	N42° 14' 59.74"	W89° 34' 11.08"	794	25	819
12	1	RWY 6/24 AND TXY D SOUTH OF TXY A	2034069.728	2458354.127	N42° 14' 58.80"	W89° 34' 08.94"	795	25	820
13	1	RWY 6/24 AND TXY D SOUTH OF TXY A	2034386.092	2458122.662	N42° 15' 01.94"	W89° 34' 11.99"	795	25	820
14	2A	RWY 6/24, TXY D SOUTH OF TXY A, AND RWY 13/31	2033214.519	2456521.276	N42° 14' 50.48"	W89° 34' 33.39"	843	25	868
15	2A	RWY 6/24, TXY D SOUTH OF TXY A, AND RWY 13/31	2032856.418	2456031.769	N42° 14' 46.97"	W89° 34' 39.93"	840	25	865
16	2B	RWY 6/24, TXY D SOUTH OF TXY A, AND RWY 18/36	2032052.477	2454932.821	N42° 14' 39.11"	W89° 34' 54.62"	844	25	869
17	2B	RWY 6/24, TXY D SOUTH OF TXY A, AND RWY 18/36	2031640.750	2454370.009	N42° 14' 35.08"	W89° 35' 02.14"	847	25	872
18	1	RWY 6/24 AND TXY D SOUTH OF TXY A	2031980.451	2454283.750	N42° 14' 38.44"	W89° 35' 03.25"	845	25	870
19	1	RWY 6/24 AND TXY D SOUTH OF TXY A	2031952.090	2454244.974	N42° 14' 38.16"	W89° 35' 03.77"	845	25	870
20	4	TXY E BTWN APRON AND RWY 18/36	2032908.775	2454456.322	N42° 14' 47.60"	W89° 35' 00.88"	840	25	865
21	4	TXY E BTWN APRON AND RWY 18/36	2033063.293	2454453.729	N42° 14' 49.12"	W89° 35' 00.90"	840	25	865
22	5	ALL FACILITIES REMAIN OPENED	2033140.264	2454452.438	N42° 14' 49.88"	W89° 35' 00.91"	841	25	866



FREEPORT - ALBERTUS AIRPORT



**FREEPORT - ALBERTUS AIRPORT
FREEPORT, IL**

CITY OF FREEPORT ILLINOIS

APPROVED AS WORKING PLAN BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE RUNWAY 6/24 PAPI SYSTEM

SHEET TITLE:

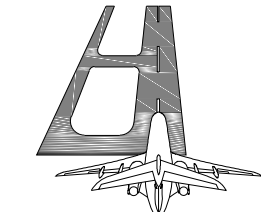
CONSTRUCTION ACTIVITY PLAN NOTES - 2

DESIGNED: STL DRAWN: JRO CHECKED: DJK

PROJECT NO.: 22004977-00
DATE: 09/12/2025

SHEET NO. REVISION

SHEET 5 OF 17 SHEETS



FREEPORT - ALBERTUS AIRPORT



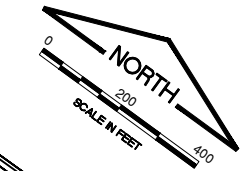
ENGINEERS AND CONSULTANTS

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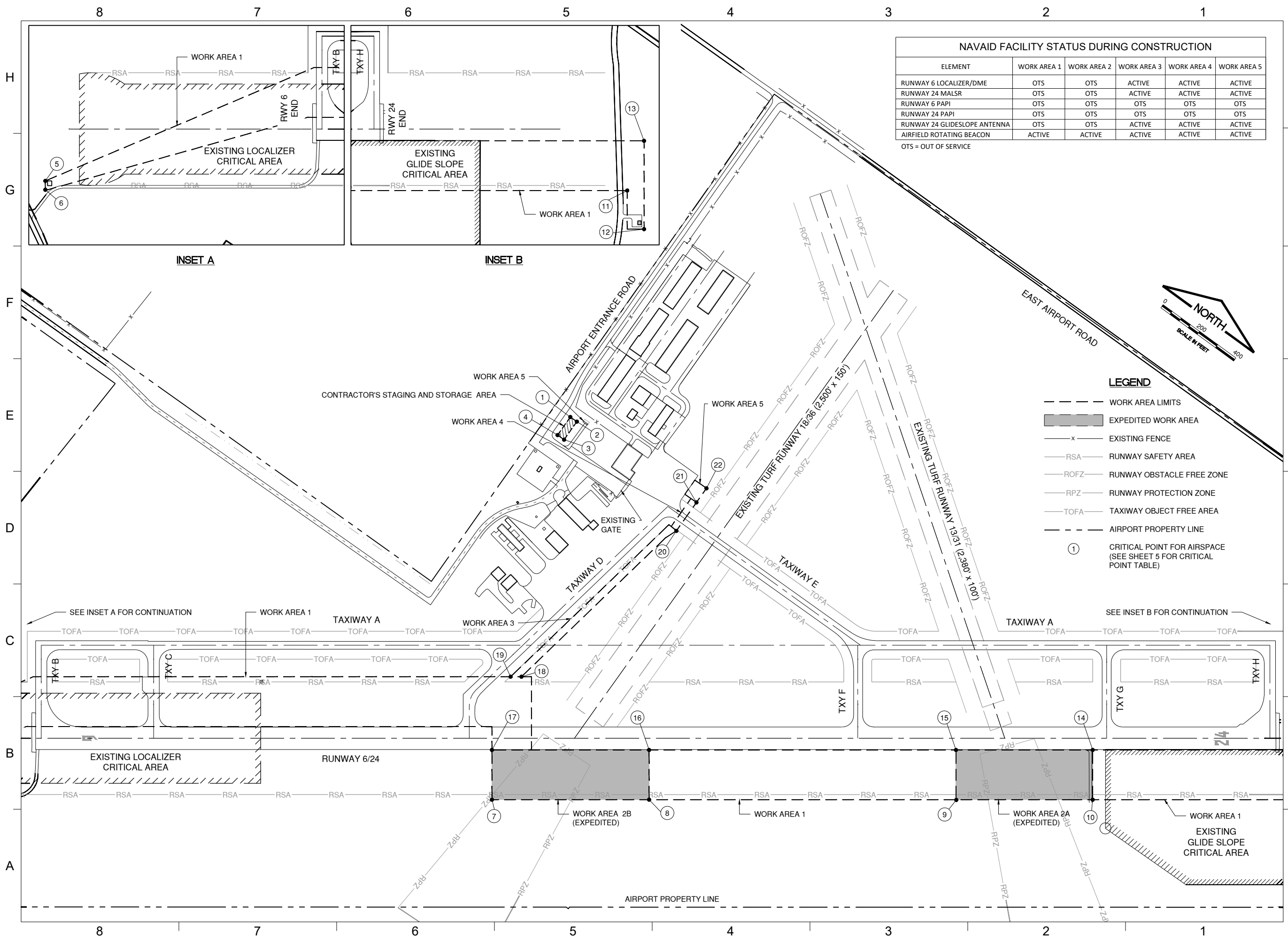
CITY OF FREEPORT
ILLINOIS

NAVAID FACILITY STATUS DURING CONSTRUCTION					
ELEMENT	WORK AREA 1	WORK AREA 2	WORK AREA 3	WORK AREA 4	WORK AREA 5
RUNWAY 6 LOCALIZER/DME	OTS	OTS	ACTIVE	ACTIVE	ACTIVE
RUNWAY 24 MALSR	OTS	OTS	ACTIVE	ACTIVE	ACTIVE
RUNWAY 6 PAPI	OTS	OTS	OTS	OTS	OTS
RUNWAY 24 PAPI	OTS	OTS	OTS	OTS	OTS
RUNWAY 24 GLIDESLOPE ANTENNA	OTS	OTS	ACTIVE	ACTIVE	ACTIVE
AIRFIELD ROTATING BEACON	ACTIVE	ACTIVE	ACTIVE	ACTIVE	ACTIVE

OTS = OUT OF SERVICE



- LEGEND**
- WORK AREA LIMITS
 - █ EXPEDITED WORK AREA
 - x- EXISTING FENCE
 - RSA --- RUNWAY SAFETY AREA
 - ROFZ --- RUNWAY OBSTACLE FREE ZONE
 - RPZ --- RUNWAY PROTECTION ZONE
 - TOFA --- TAXIWAY OBJECT FREE AREA
 - AIRPORT PROPERTY LINE
 - ① CRITICAL POINT FOR AIRSPACE (SEE SHEET 5 FOR CRITICAL POINT TABLE)



APPROVED AS WORKING PLAN BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE RUNWAY 6/24 PAPI SYSTEM

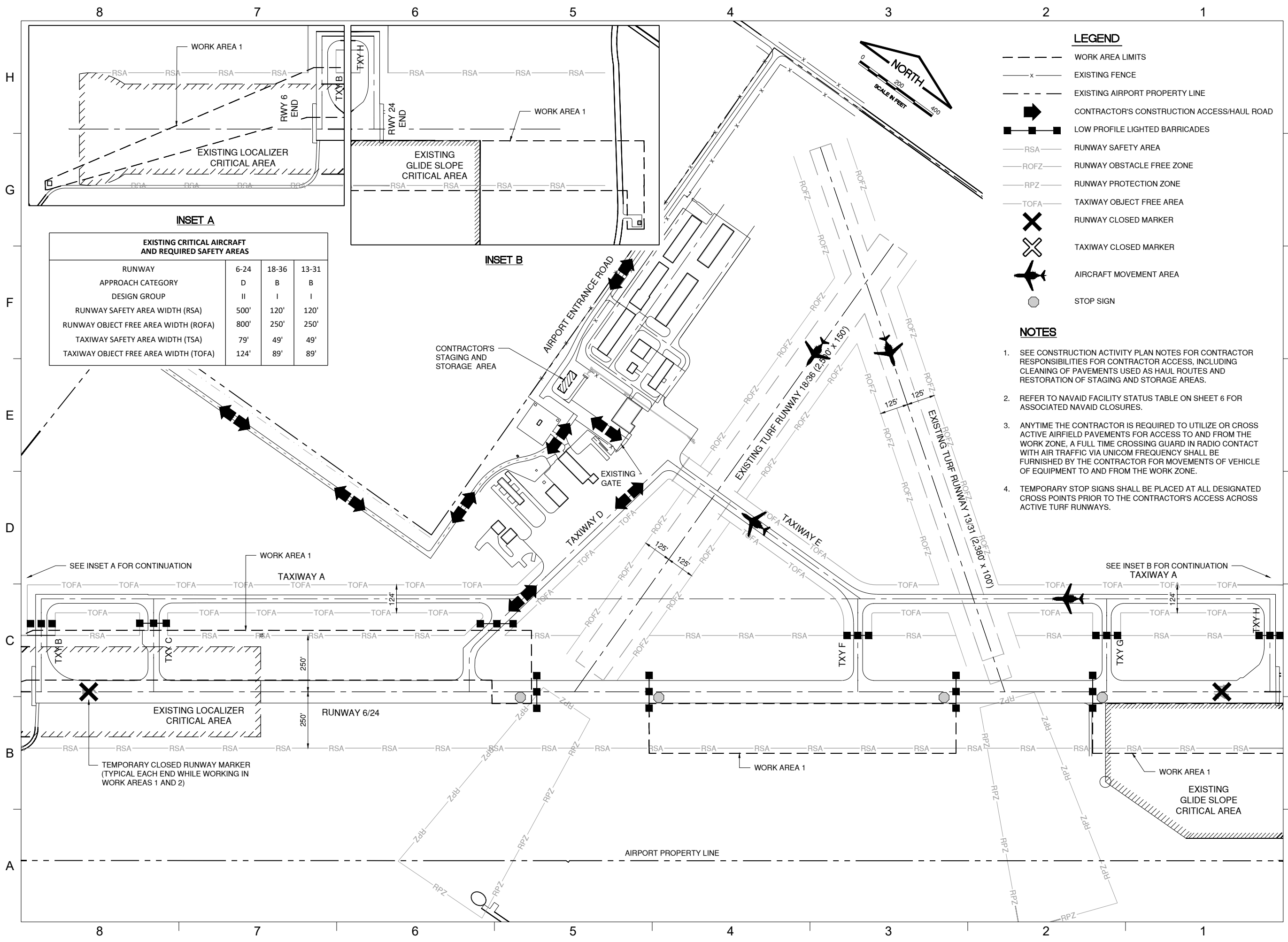
SHEET TITLE:
CONSTRUCTION ACTIVITY PLAN INDEX

DESIGNED: STL DRAWN: JRO CHECKED: STL

PROJECT NO.: 22004977-08
DATE: 08/15/2025

SHEET NO. REVISION

SHEET 6 OF 17 SHEETS



LEGEND

- WORK AREA LIMITS
- x- EXISTING FENCE
- - - EXISTING AIRPORT PROPERTY LINE
- ➔ CONTRACTOR'S CONSTRUCTION ACCESS/HAUL ROAD
- ■ ■ ■ ■ LOW PROFILE LIGHTED BARRICADES
- RSA — RUNWAY SAFETY AREA
- ROFZ — RUNWAY OBSTACLE FREE ZONE
- RPZ — RUNWAY PROTECTION ZONE
- TOFA — TAXIWAY OBJECT FREE AREA
- ✕ RUNWAY CLOSED MARKER
- ✕ TAXIWAY CLOSED MARKER
- ✈ AIRCRAFT MOVEMENT AREA
- STOP SIGN

NOTES

1. SEE CONSTRUCTION ACTIVITY PLAN NOTES FOR CONTRACTOR RESPONSIBILITIES FOR CONTRACTOR ACCESS, INCLUDING CLEANING OF PAVEMENTS USED AS HAUL ROUTES AND RESTORATION OF STAGING AND STORAGE AREAS.
2. REFER TO NAVAID FACILITY STATUS TABLE ON SHEET 6 FOR ASSOCIATED NAVAID CLOSURES.
3. ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH AIR TRAFFIC VIA UNICOM FREQUENCY SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLE OF EQUIPMENT TO AND FROM THE WORK ZONE.
4. TEMPORARY STOP SIGNS SHALL BE PLACED AT ALL DESIGNATED CROSS POINTS PRIOR TO THE CONTRACTOR'S ACCESS ACROSS ACTIVE TURF RUNWAYS.

EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS

RUNWAY	6-24	18-36	13-31
APPROACH CATEGORY	D	B	B
DESIGN GROUP	II	I	I
RUNWAY SAFETY AREA WIDTH (RSA)	500'	120'	120'
RUNWAY OBJECT FREE AREA WIDTH (ROFA)	800'	250'	250'
TAXIWAY SAFETY AREA WIDTH (TSA)	79'	49'	49'
TAXIWAY OBJECT FREE AREA WIDTH (TOFA)	124'	89'	89'



FREEPORT - ALBERTUS AIRPORT



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FREEPORT, IL**

CITY OF FREEPORT ILLINOIS

APPROVED AS WORKING PLAN BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE RUNWAY 6/24 PAPI SYSTEM

SHEET TITLE:
CONSTRUCTION ACTIVITY PLAN - WORK AREA 1

DESIGNED:	DRAWN:	CHECKED:
STL	JRO	STL

PROJECT NO.: 22004977-08
DATE: 04/28/2025

SHEET NO.	REVISION



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FREEPORT - ALBERTUS AIRPORT
FREEPORT, IL
CITY OF FREEPORT ILLINOIS

APPROVED AS WORKING PLAN BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE RUNWAY 6/24 PAPI SYSTEM

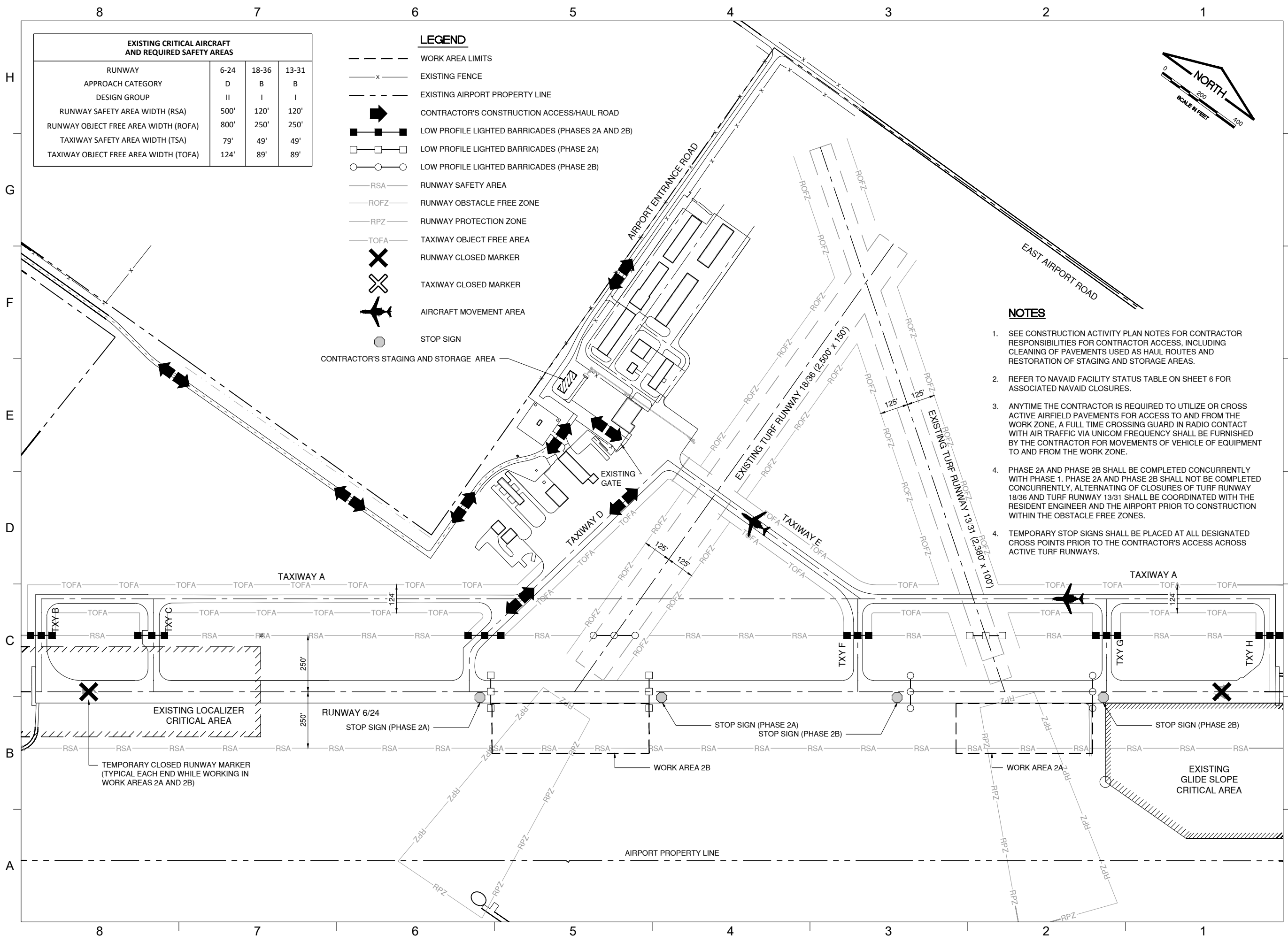
SHEET TITLE:
CONSTRUCTION ACTIVITY PLAN - WORK AREA 2

DESIGNED: STL DRAWN: JRO CHECKED: STL

PROJECT NO.: 22004977-08
DATE: 08/15/2025

SHEET NO. REVISION

SHEET 8 OF 17 SHEETS



EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS

RUNWAY	6-24	18-36	13-31
APPROACH CATEGORY	D	B	B
DESIGN GROUP	II	I	I
RUNWAY SAFETY AREA WIDTH (RSA)	500'	120'	120'
RUNWAY OBJECT FREE AREA WIDTH (ROFA)	800'	250'	250'
TAXIWAY SAFETY AREA WIDTH (TSA)	79'	49'	49'
TAXIWAY OBJECT FREE AREA WIDTH (TOFA)	124'	89'	89'

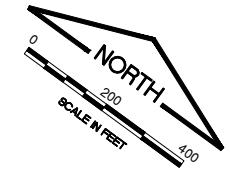
- LEGEND**
- WORK AREA LIMITS
 - x- EXISTING FENCE
 - - - EXISTING AIRPORT PROPERTY LINE
 - ➔ CONTRACTOR'S CONSTRUCTION ACCESS/HAUL ROAD
 - LOW PROFILE LIGHTED BARRICADES (PHASES 2A AND 2B)
 - LOW PROFILE LIGHTED BARRICADES (PHASE 2A)
 - LOW PROFILE LIGHTED BARRICADES (PHASE 2B)
 - RSA RUNWAY SAFETY AREA
 - ROFZ RUNWAY OBSTACLE FREE ZONE
 - RPZ RUNWAY PROTECTION ZONE
 - TOFA TAXIWAY OBJECT FREE AREA
 - ✕ RUNWAY CLOSED MARKER
 - ✕ TAXIWAY CLOSED MARKER
 - ✈ AIRCRAFT MOVEMENT AREA
 - STOP SIGN
 - CONTRACTOR'S STAGING AND STORAGE AREA

NOTES

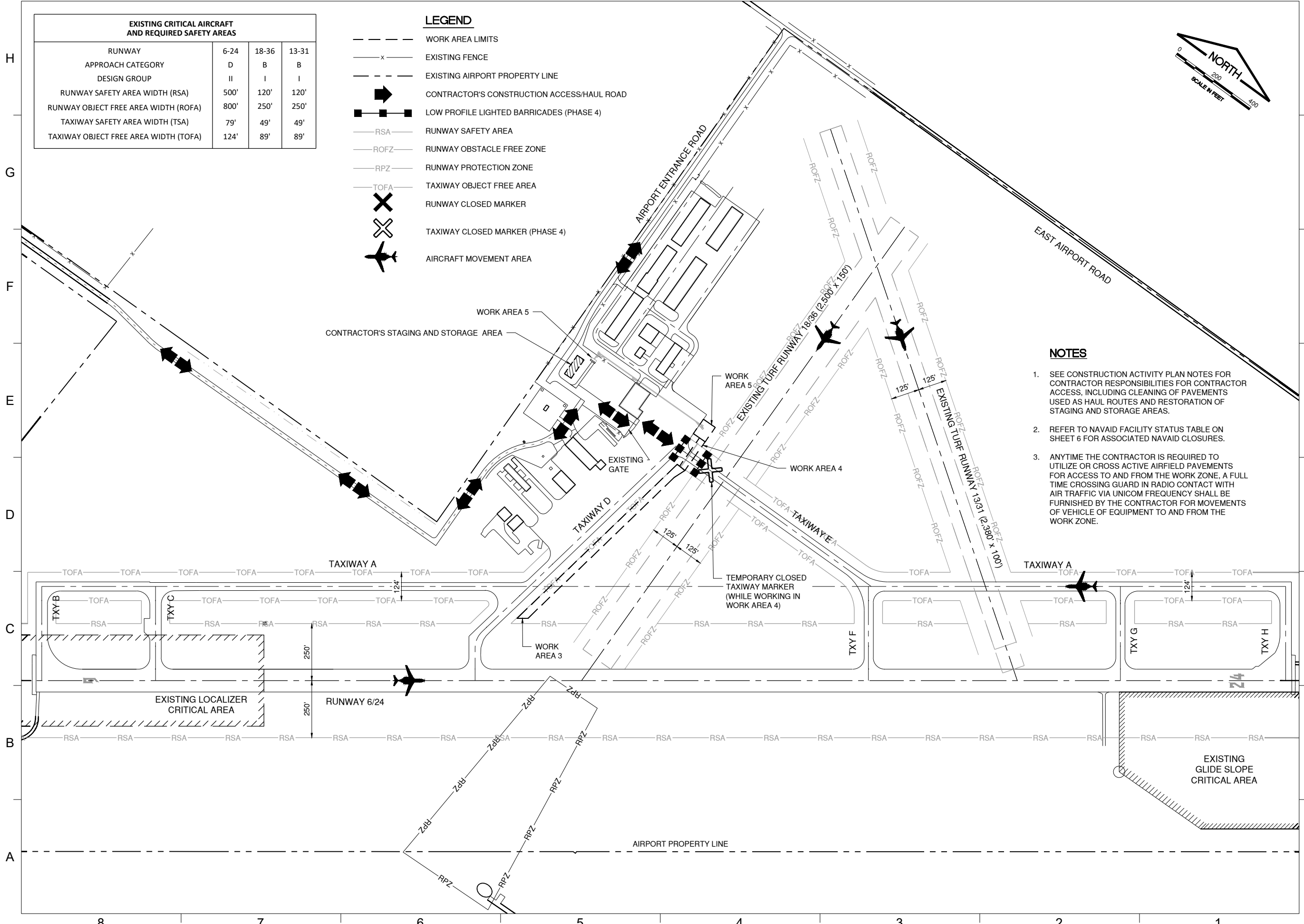
- SEE CONSTRUCTION ACTIVITY PLAN NOTES FOR CONTRACTOR RESPONSIBILITIES FOR CONTRACTOR ACCESS, INCLUDING CLEANING OF PAVEMENTS USED AS HAUL ROUTES AND RESTORATION OF STAGING AND STORAGE AREAS.
- REFER TO NAVAID FACILITY STATUS TABLE ON SHEET 6 FOR ASSOCIATED NAVAID CLOSURES.
- ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH AIR TRAFFIC VIA UNICOM FREQUENCY SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLE OF EQUIPMENT TO AND FROM THE WORK ZONE.
- PHASE 2A AND PHASE 2B SHALL BE COMPLETED CONCURRENTLY WITH PHASE 1. PHASE 2A AND PHASE 2B SHALL NOT BE COMPLETED CONCURRENTLY, ALTERNATING OF CLOSURES OF TURF RUNWAY 18/36 AND TURF RUNWAY 13/31 SHALL BE COORDINATED WITH THE RESIDENT ENGINEER AND THE AIRPORT PRIOR TO CONSTRUCTION WITHIN THE OBSTACLE FREE ZONES.
- TEMPORARY STOP SIGNS SHALL BE PLACED AT ALL DESIGNATED CROSS POINTS PRIOR TO THE CONTRACTOR'S ACCESS ACROSS ACTIVE TURF RUNWAYS.

EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS			
RUNWAY	6-24	18-36	13-31
APPROACH CATEGORY	D	B	B
DESIGN GROUP	II	I	I
RUNWAY SAFETY AREA WIDTH (RSA)	500'	120'	120'
RUNWAY OBJECT FREE AREA WIDTH (ROFA)	800'	250'	250'
TAXIWAY SAFETY AREA WIDTH (TSA)	79'	49'	49'
TAXIWAY OBJECT FREE AREA WIDTH (TOFA)	124'	89'	89'

- LEGEND**
- WORK AREA LIMITS
 - x- EXISTING FENCE
 - - - EXISTING AIRPORT PROPERTY LINE
 - ➔ CONTRACTOR'S CONSTRUCTION ACCESS/HAUL ROAD
 - LOW PROFILE LIGHTED BARRICADES (PHASE 4)
 - RSA— RUNWAY SAFETY AREA
 - ROFZ— RUNWAY OBSTACLE FREE ZONE
 - RPZ— RUNWAY PROTECTION ZONE
 - TOFA— TAXIWAY OBJECT FREE AREA
 - ✕ RUNWAY CLOSED MARKER
 - ✕ TAXIWAY CLOSED MARKER (PHASE 4)
 - ✈ AIRCRAFT MOVEMENT AREA



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CITY OF FREEPORT ILLINOIS



- NOTES**
- SEE CONSTRUCTION ACTIVITY PLAN NOTES FOR CONTRACTOR RESPONSIBILITIES FOR CONTRACTOR ACCESS, INCLUDING CLEANING OF PAVEMENTS USED AS HAUL ROUTES AND RESTORATION OF STAGING AND STORAGE AREAS.
 - REFER TO NAVAID FACILITY STATUS TABLE ON SHEET 6 FOR ASSOCIATED NAVAID CLOSURES.
 - ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH AIR TRAFFIC VIA UNICOM FREQUENCY SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLE OF EQUIPMENT TO AND FROM THE WORK ZONE.

APPROVED AS WORKING PLAN BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE RUNWAY 6/24 PAPI SYSTEM

SHEET TITLE:
CONSTRUCTION ACTIVITY PLAN - WORK AREA 3, 4 AND 5

DESIGNED:	DRAWN:	CHECKED:
STL	JRO	STL

PROJECT NO.: 22004977-08
DATE: 08/15/2025

SHEET NO.	REVISION
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CLOSED RUNWAY MARKER NOTES

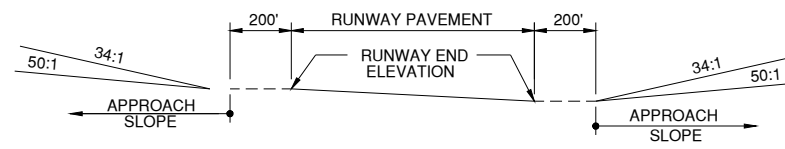
1. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING YELLOW CROSSES AT THE LOCATION AND DIMENSIONS DETAILED ON THE SEQUENCE OF CONSTRUCTION AND PER AC 150/5370-2 (LATEST EDITION). THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.
2. TEMPORARY CLOSED RUNWAY MARKERS SHALL BE YELLOW.
3. TEMPORARY MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
4. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
5. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
6. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

AIRFIELD LIGHTS AND SIGNS NOTES

1. CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
2. CONTRACTOR SHALL TURN OFF RUNWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES. CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY, THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.

CLOSED TAXIWAY MARKER DETAIL NOTES

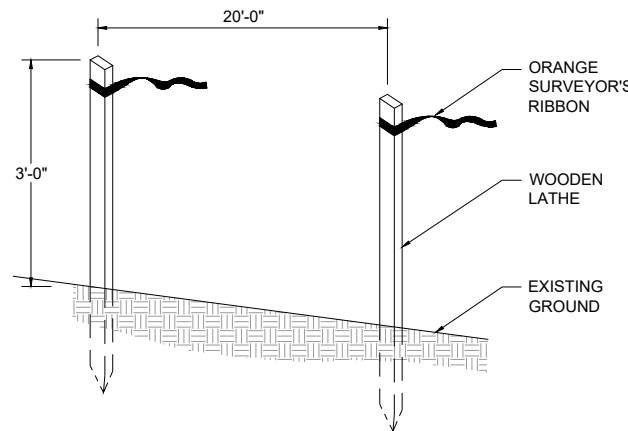
1. CLOSED TAXIWAY MARKERS SHALL BE PAINTED YELLOW WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OBJECT FREE AREA CRITERIA AND RUNWAY SAFETY AREA CRITERIA PER ADVISORY CIRCULAR 150/5300-13B (LATEST EDITION) AND ARE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT.
2. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
3. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. PLACE MARKERS OVER TAXIWAY CENTERLINE.
5. MARKERS SHALL BE ADEQUATELY SECURED TO PREVENT MOVEMENT BY PROPELLER WASH, JET BLAST OR OTHER WIND CURRENTS.
6. MARKERS ARE ONLY REQUIRED FOR CLOSURES EXCEEDING 72 HOURS.



**TYPICAL PROFILE
F.A.R. PART 77 IMAGINARY SURFACES**

NOT TO SCALE

RUNWAY END	ELEVATION	APPROACH SLOPE
6	859.27	34:1
24	841.47	50:1

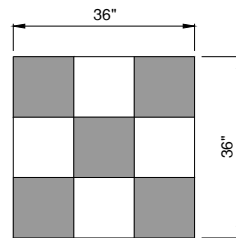


CONSTRUCTION SETBACK LINE DETAIL

NOT TO SCALE

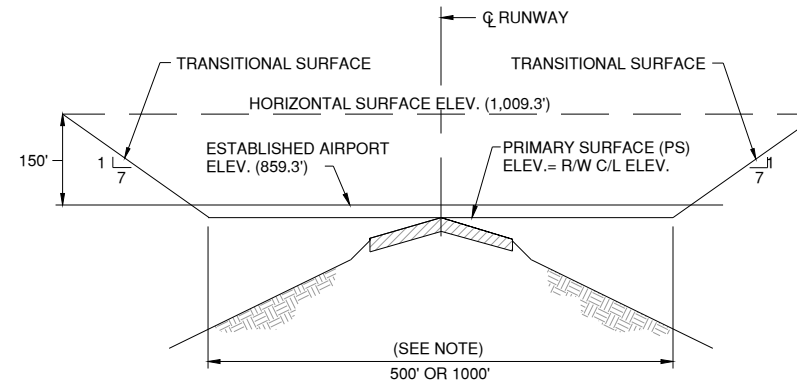
CONSTRUCTION SETBACK NOTES

1. CONTRACTOR SHALL MARKER THE RUNWAY SAFETY AREA AND TAXIWAY OBJECT FREE AREA PER THE CONSTRUCTION SETBACK LINE DETAIL AS DIRECTED BY THE ENGINEER.
2. ALL COST ASSOCIATED WITH THE CONSTRUCTION SETBACK LINE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



**CONSTRUCTION EQUIPMENT
AND TRUCK SIGNAL FLAG**

NOT TO SCALE



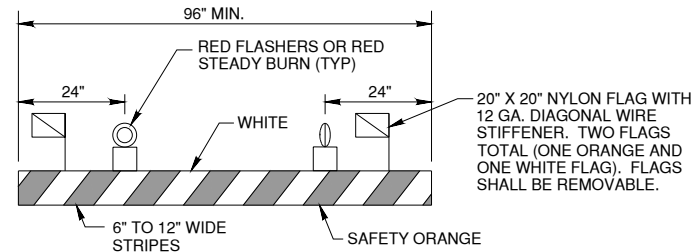
**TYPICAL SECTION
F.A.R. PART 77 IMAGINARY SURFACES**

NOT TO SCALE

NOTE:
IMAGINARY SURFACE REQUIREMENTS FOR EXISTING ACTIVE RUNWAYS (R/W) ARE SIMILAR EXCEPT PRIMARY SURFACE (PS) DIMENSIONS VARY RUNWAY 6-24 500' PS (250' LT & RT OF CENTERLINE)



PLAN VIEW



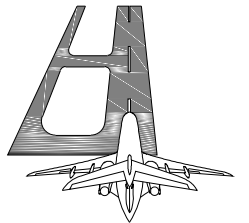
SIDE VIEW

AIRSIDE LOW PROFILE LIGHTED BARRICADE

NOT TO SCALE

BARRICADE NOTES

1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



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AIRPORT**



**FREEPORT - ALBERTUS
AIRPORT
FREEPORT, IL**

**CITY OF FREEPORT
ILLINOIS**

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

**REHABILITATE
RUNWAY 6/24
PAPI SYSTEM**

SHEET TITLE:

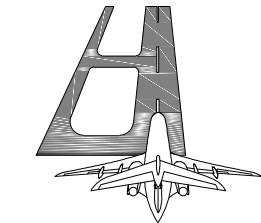
**CONSTRUCTION ACTIVITY
PLAN DETAILS**

DESIGNED:	DRAWN:	CHECKED:
STL	JRO	DJK

PROJECT NO.:	22004977-00
DATE:	09/12/2025

SHEET NO.	REVISION

SHEET 10 OF 17 SHEETS



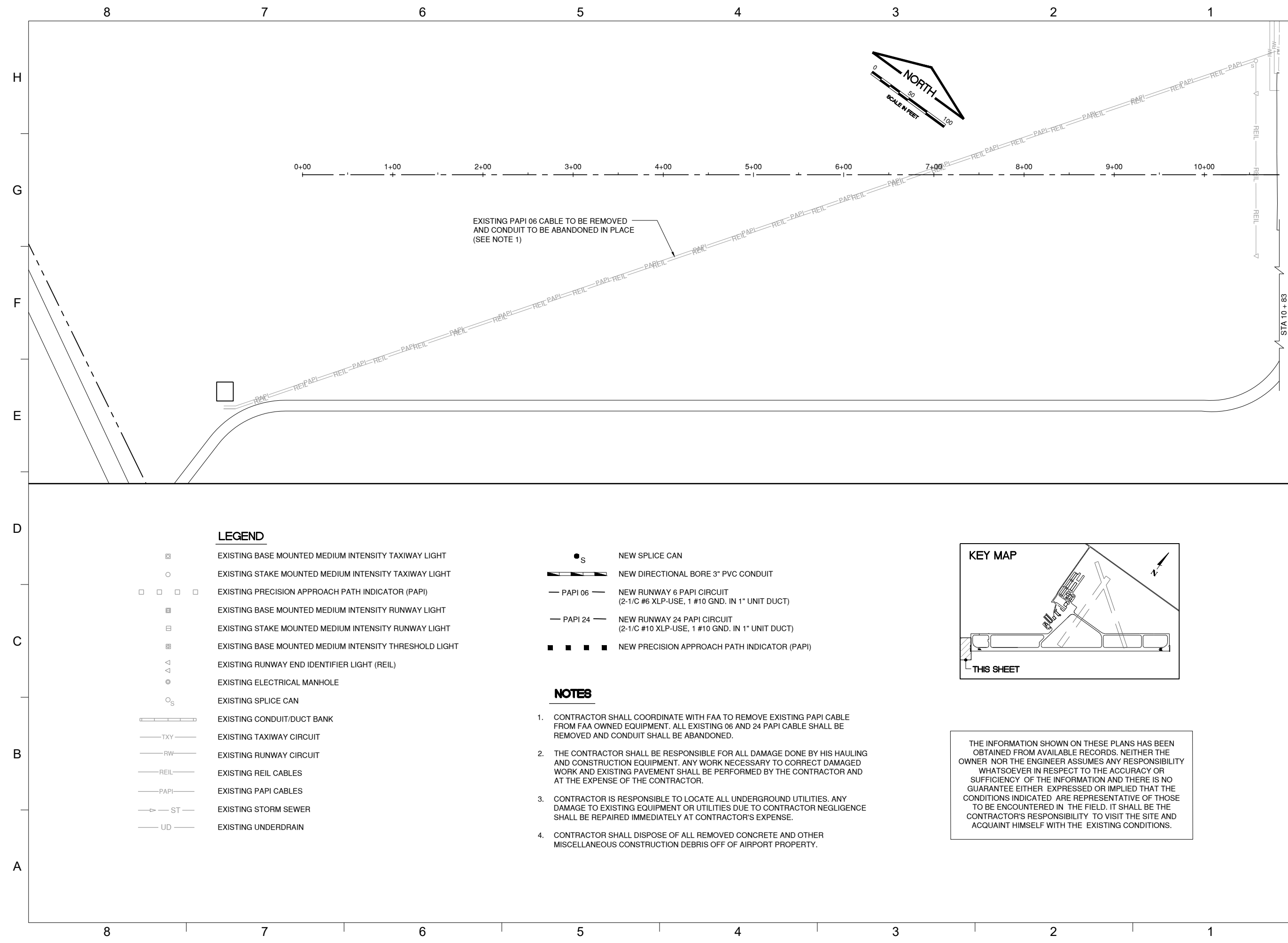
FREEPORT - ALBERTUS AIRPORT



ENGINEERS AND CONSULTANTS

FREEPORT - ALBERTUS AIRPORT
FREEPORT, IL

CITY OF FREEPORT
ILLINOIS



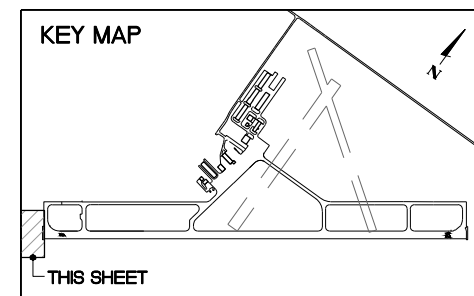
LEGEND

- ◻ EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- EXISTING STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- ◻ ◻ ◻ ◻ EXISTING PRECISION APPROACH PATH INDICATOR (PAPI)
- ◻ EXISTING BASE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT
- ◻ EXISTING STAKE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT
- ◻ EXISTING BASE MOUNTED MEDIUM INTENSITY THRESHOLD LIGHT
- △ EXISTING RUNWAY END IDENTIFIER LIGHT (REIL)
- EXISTING ELECTRICAL MANHOLE
- _S EXISTING SPLICE CAN
- ▬ EXISTING CONDUIT/DUCT BANK
- TXY— EXISTING TAXIWAY CIRCUIT
- RW— EXISTING RUNWAY CIRCUIT
- REIL— EXISTING REIL CABLES
- PAPI— EXISTING PAPI CABLES
- ST— EXISTING STORM SEWER
- UD— EXISTING UNDERDRAIN

- _S NEW SPLICE CAN
- ▬ NEW DIRECTIONAL BORE 3" PVC CONDUIT
- PAPI 06 — NEW RUNWAY 6 PAPI CIRCUIT (2-1/C #6 XLP-USE, 1 #10 GND. IN 1" UNIT DUCT)
- PAPI 24 — NEW RUNWAY 24 PAPI CIRCUIT (2-1/C #10 XLP-USE, 1 #10 GND. IN 1" UNIT DUCT)
- ■ ■ ■ NEW PRECISION APPROACH PATH INDICATOR (PAPI)

NOTES

1. CONTRACTOR SHALL COORDINATE WITH FAA TO REMOVE EXISTING PAPI CABLE FROM FAA OWNED EQUIPMENT. ALL EXISTING 06 AND 24 PAPI CABLE SHALL BE REMOVED AND CONDUIT SHALL BE ABANDONED.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
3. CONTRACTOR IS RESPONSIBLE TO LOCATE ALL UNDERGROUND UTILITIES. ANY DAMAGE TO EXISTING EQUIPMENT OR UTILITIES DUE TO CONTRACTOR NEGLIGENCE SHALL BE REPAIRED IMMEDIATELY AT CONTRACTOR'S EXPENSE.
4. CONTRACTOR SHALL DISPOSE OF ALL REMOVED CONCRETE AND OTHER MISCELLANEOUS CONSTRUCTION DEBRIS OFF OF AIRPORT PROPERTY.



THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

APPROVED AS WORKING PLAN BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 6/24
PAPI SYSTEM

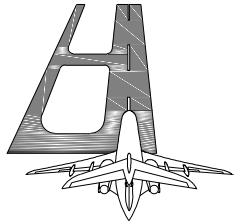
SHEET TITLE:

ELECTRICAL PLAN - 1

DESIGNED: STL	DRAWN: JRO	CHECKED: DJK
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PROJECT NO.:	22004977-00
DATE:	09/12/2025

SHEET NO.	REVISION
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FREEPORT - ALBERTUS AIRPORT



FREEPORT - ALBERTUS AIRPORT
FREEPORT, IL

CITY OF FREEPORT
ILLINOIS

APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 6/24
PAPI SYSTEM

SHEET TITLE:

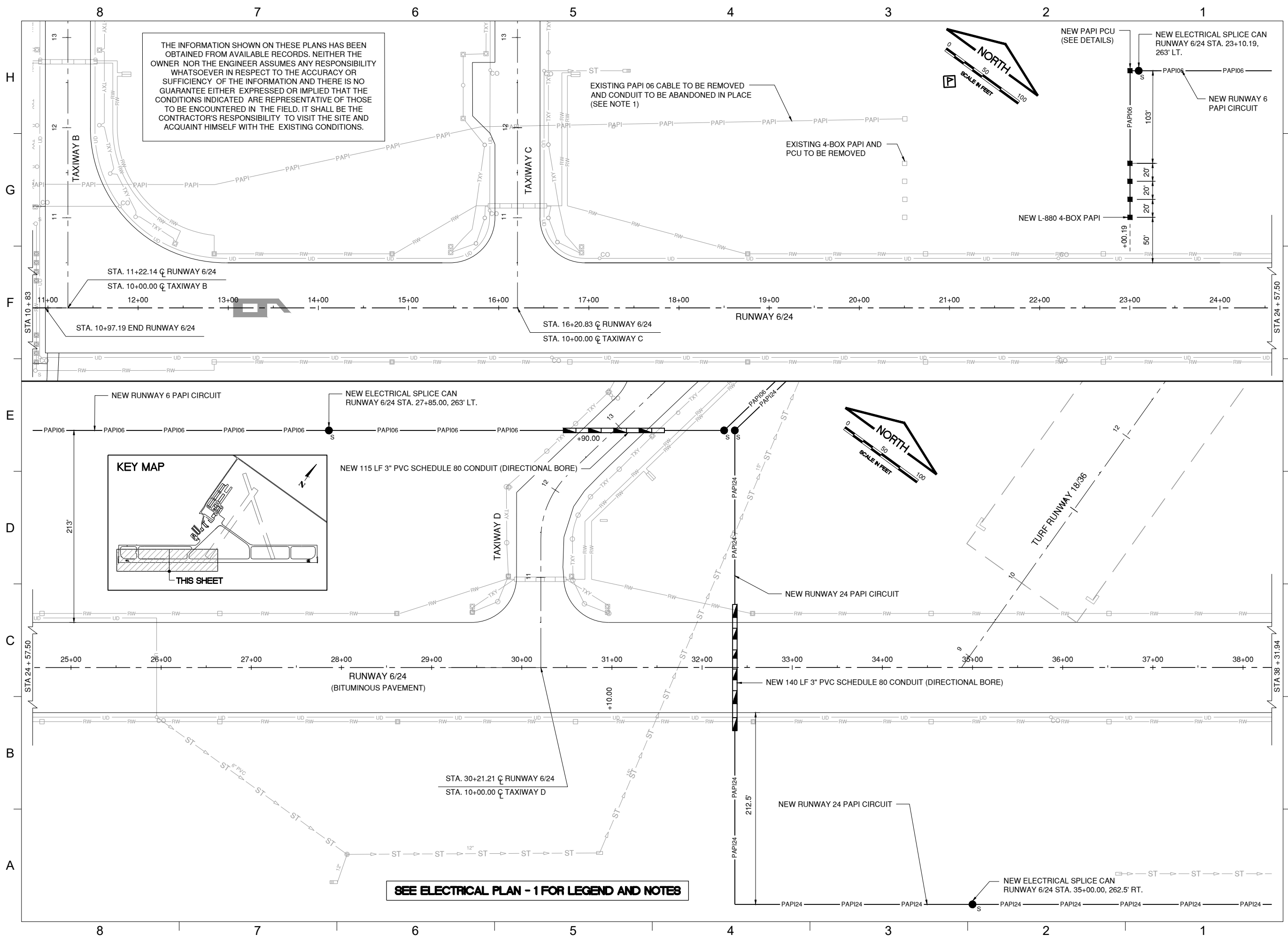
ELECTRICAL PLAN - 2

DESIGNED: STL DRAWN: JRO CHECKED: DJK

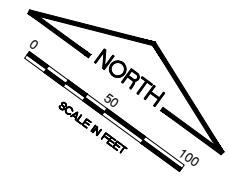
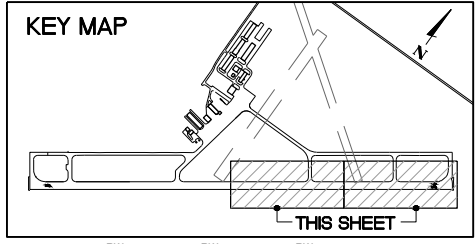
PROJECT NO.: 22004977-00
DATE: 09/12/2025

SHEET NO. REVISION

SHEET 12 OF 17 SHEETS



SEE ELECTRICAL PLAN - 1 FOR LEGEND AND NOTES



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CMT ENGINEERS AND CONSULTANTS

FREEPORT - ALBERTUS AIRPORT
FREEPORT, IL

CITY OF FREEPORT
ILLINOIS

APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 6/24
PAPI SYSTEM

SHEET TITLE:

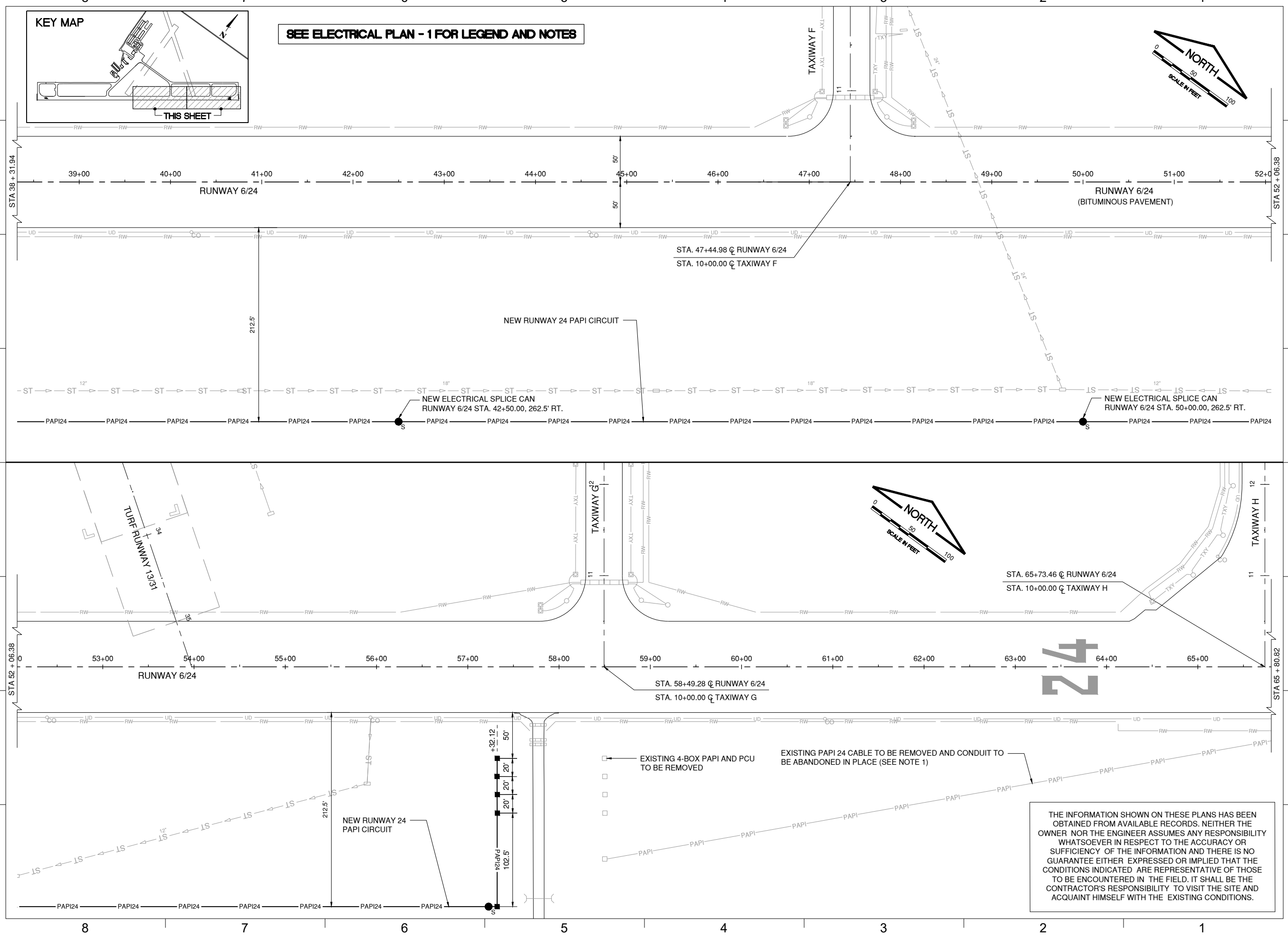
ELECTRICAL PLAN - 3

DESIGNED: STL DRAWN: JRO CHECKED: DJK

PROJECT NO.: 22004977-00
DATE: 09/12/2025

SHEET NO. REVISION

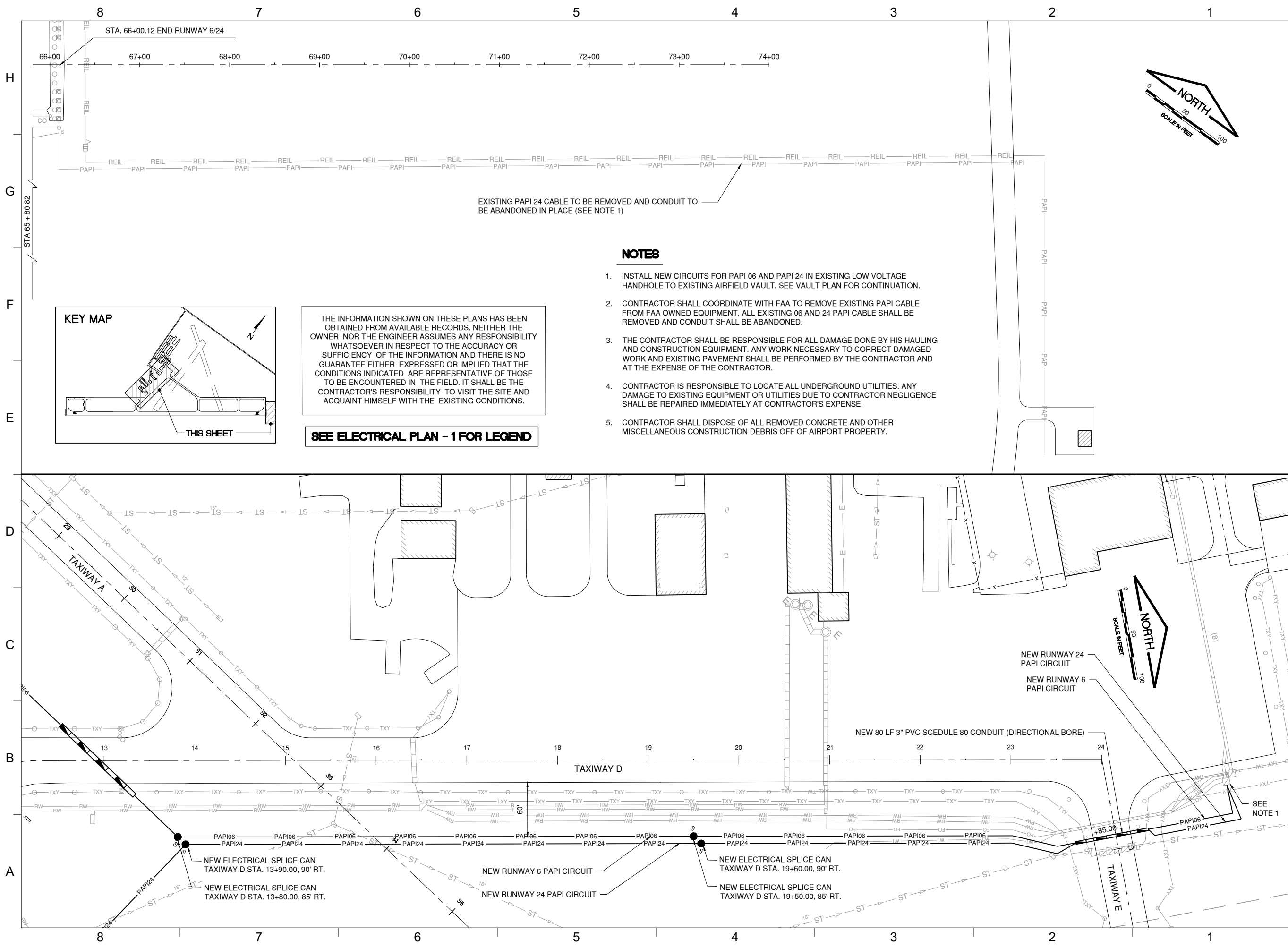
SHEET 13 OF 17 SHEETS



EXISTING 4-BOX PAPI AND PCU TO BE REMOVED

EXISTING PAPI 24 CABLE TO BE REMOVED AND CONDUIT TO BE ABANDONED IN PLACE (SEE NOTE 1)

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

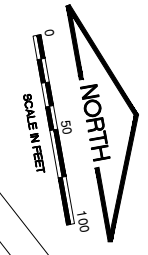
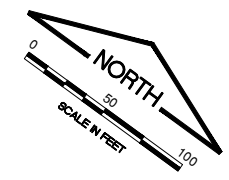


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FREEPORT, IL

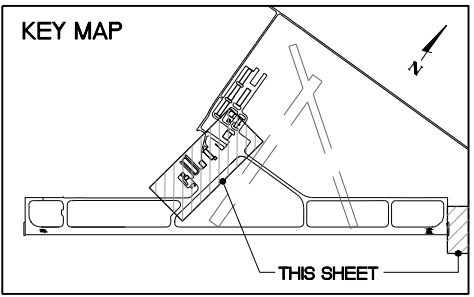
CITY OF FREEPORT ILLINOIS



EXISTING PAPI 24 CABLE TO BE REMOVED AND CONDUIT TO BE ABANDONED IN PLACE (SEE NOTE 1)

NOTES

1. INSTALL NEW CIRCUITS FOR PAPI 06 AND PAPI 24 IN EXISTING LOW VOLTAGE HANDHOLE TO EXISTING AIRFIELD VAULT. SEE VAULT PLAN FOR CONTINUATION.
2. CONTRACTOR SHALL COORDINATE WITH FAA TO REMOVE EXISTING PAPI CABLE FROM FAA OWNED EQUIPMENT. ALL EXISTING 06 AND 24 PAPI CABLE SHALL BE REMOVED AND CONDUIT SHALL BE ABANDONED.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
4. CONTRACTOR IS RESPONSIBLE TO LOCATE ALL UNDERGROUND UTILITIES. ANY DAMAGE TO EXISTING EQUIPMENT OR UTILITIES DUE TO CONTRACTOR NEGLIGENCE SHALL BE REPAIRED IMMEDIATELY AT CONTRACTOR'S EXPENSE.
5. CONTRACTOR SHALL DISPOSE OF ALL REMOVED CONCRETE AND OTHER MISCELLANEOUS CONSTRUCTION DEBRIS OFF OF AIRPORT PROPERTY.



THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

SEE ELECTRICAL PLAN - 1 FOR LEGEND

APPROVED AS WORKING PLAN BY:

REV DATE DESCRIPTION

PROJECT NAME:
REHABILITATE RUNWAY 6/24 PAPI SYSTEM

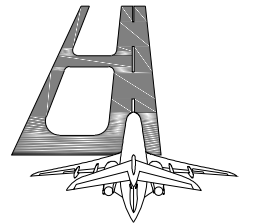
SHEET TITLE:
ELECTRICAL PLAN - 4

DESIGNED: STL DRAWN: JRO CHECKED: DJK

PROJECT NO.: 22004977-00
DATE: 09/12/2025

SHEET NO. REVISION

SHEET 14 OF 17 SHEETS



FREEPORT - ALBERTUS AIRPORT



FREEPORT - ALBERTUS AIRPORT
FREEPORT, IL

CITY OF FREEPORT ILLINOIS

APPROVED AS WORKING PLAN BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE RUNWAY 6/24 PAPI SYSTEM

SHEET TITLE:

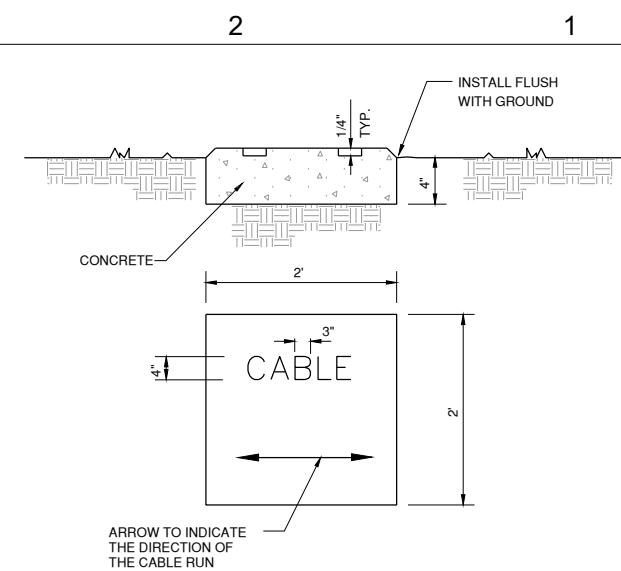
ELECTRICAL DETAILS

DESIGNED: STL DRAWN: JRO CHECKED: DJK

PROJECT NO.: 22004977-00
DATE: 09/12/2025

SHEET NO. REVISION

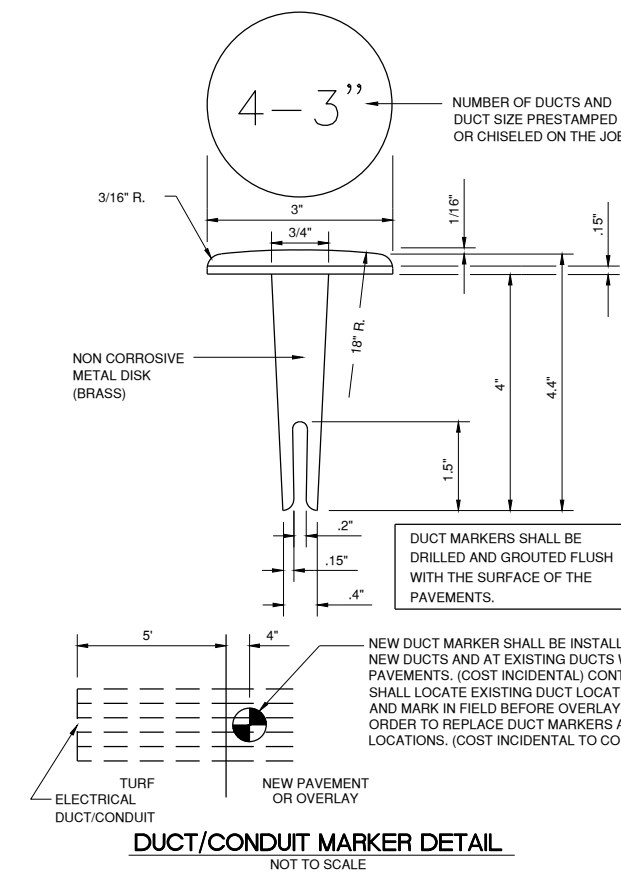
SHEET 15 OF 17 SHEETS



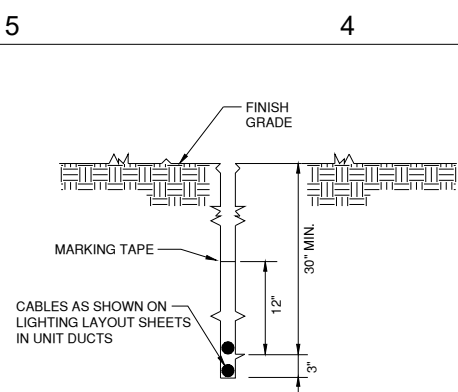
TURF CABLE MARKER DETAIL
NOT TO SCALE

NOTES

- 1.) CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE CABLE RUN.
- 2.) ITEM 610 CONCRETE SHALL BE USED.
- 3.) ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
- 4.) THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- 5.) 0.049 CU. YD. CONCRETE PER MARKER.
- 6.) CONTACTOR SHALL LOCATE EXISTING CABLE MARKERS IN THE FIELD BEFORE SHOULDER ADJUSTMENT BEGINS IN ORDER TO REPLACE CABLE MARKERS AT CORRECT LOCATIONS (COST INCIDENTAL TO CONTRACT).



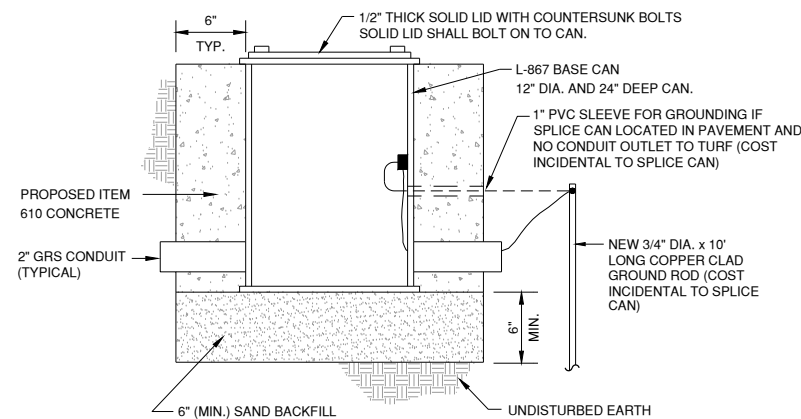
DUCT/CONDUIT MARKER DETAIL
NOT TO SCALE



PLOWED CABLE IN UNIT DUCT
NOT TO SCALE

NOTES

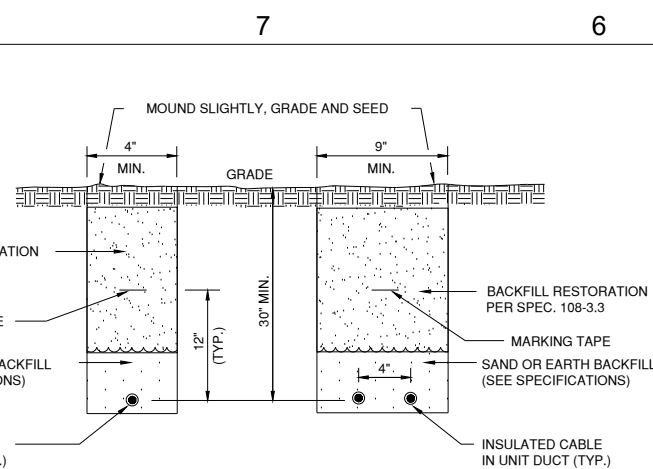
- 1. ONLY CABLES OF THE SAME CIRCUIT WILL BE ALLOWED TO BE PLOWED IN TOGETHER.
- 2. THE CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.



SPLICE CAN DETAIL
NOT TO SCALE

NOTES

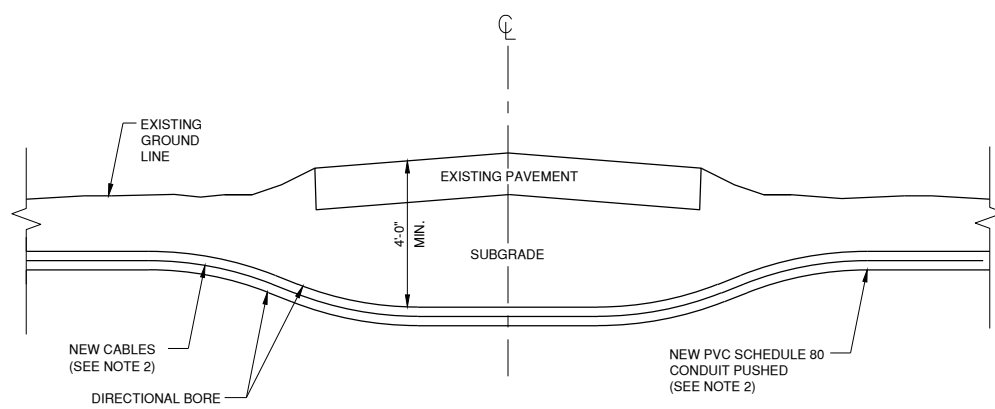
- 1. THE DEPTH OF THE DIRECTIONAL BORE SHALL BE NO LESS THAN 4.0' FROM THE PAVEMENT SURFACE AND SHALL NOT DISTURB EXISTING UNDERDRAINS OR NEW LIGHTS/CABLING.
- 2. REFER TO ELECTRICAL PLANS FOR CABLE AND CONDUIT INFORMATION.
- 3. THE LENGTH OF THE DIRECTION BORE SHALL EXTEND 20' BEYOND EXISING PAVEMENT EDGE.



TRENCH DETAIL FOR CABLE IN UNIT DUCT
NOT TO SCALE

NOTES

- 1. TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- 2. DEPTH OF TRENCHES FOR AIRFIELD LIGHTING SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- 3. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- 4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH RETURNING MATERIALS.
- 5. THE CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.



DIRECTIONAL BORE DETAIL
N.T.S.

8

7

6

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H

G

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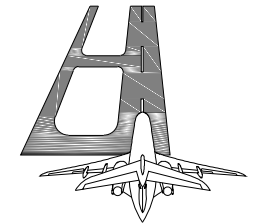
5

4

3

2

1



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FREEPORT, IL

CITY OF FREEPORT
ILLINOIS

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 6/24
PAPI SYSTEM

SHEET TITLE:

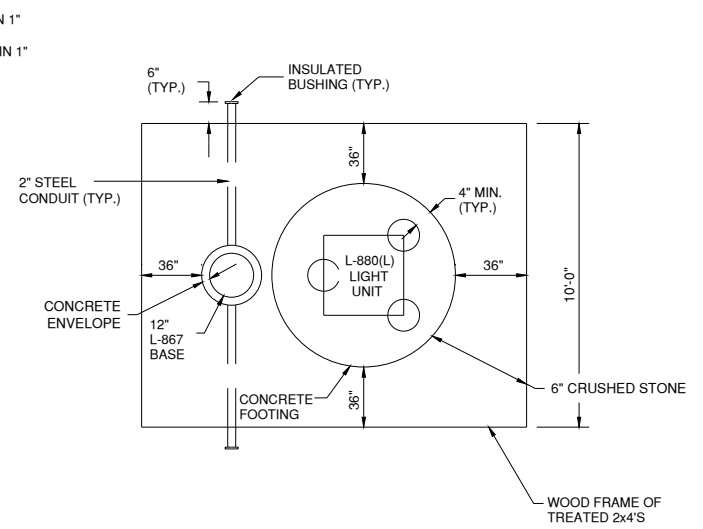
PAPI DETAILS

DESIGNED:	DRAWN:	CHECKED:
STL	JRO	DJK

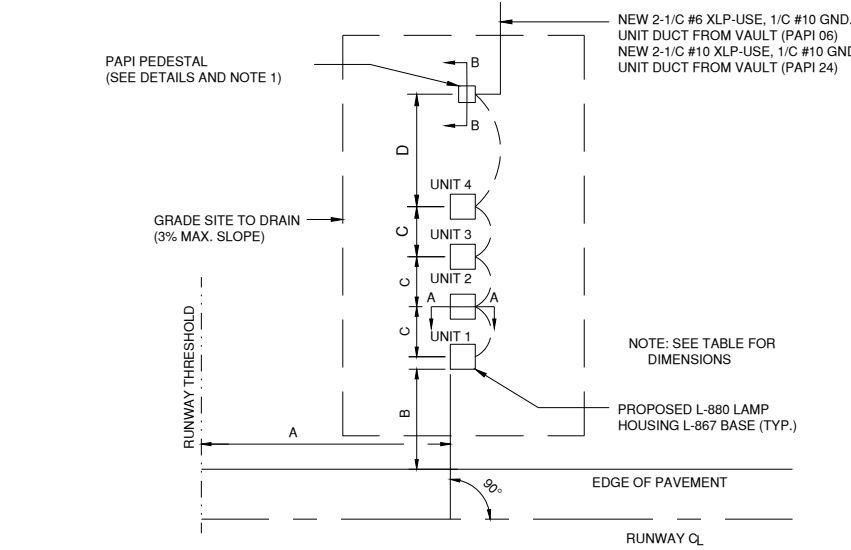
PROJECT NO.:	22004977-00
DATE:	09/12/2025

SHEET NO.	REVISION
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DESCRIPTION	RUNWAY 6 END	RUNWAY 24 END
DIMENSION A	1203.00'	868.00'
DIMENSION B	50'	50'
DIMENSION C	20'	20'
DIMENSION D	103'	102.5'
HEIGHT GROUP USED FOR SITING	1	1
THRESHOLD STATION	10+97.19	66+00.13
THRESHOLD ELEVATION	859.78	841.58
THRESHOLD CROSSING HEIGHT	50'	45'
STATION FOR MIDPOINT OF PROJECTORS	23+00.19	57+32.12
GLIDE PATH ANGLE	3°	3°
ELEVATION CL OF APERTURE UNIT 1	850.29	844.46
ELEVATION CL OF APERTURE UNIT 2	850.29	844.46
ELEVATION CL OF APERTURE UNIT 3	850.29	844.46
ELEVATION CL OF APERTURE UNIT 4	850.29	844.46
AIMING ANGLE UNIT 1	3° 30'	3° 30'
AIMING ANGLE UNIT 2	3° 10'	3° 10'
AIMING ANGLE UNIT 3	2° 50'	2° 50'
AIMING ANGLE UNIT 4	2° 30'	2° 30'

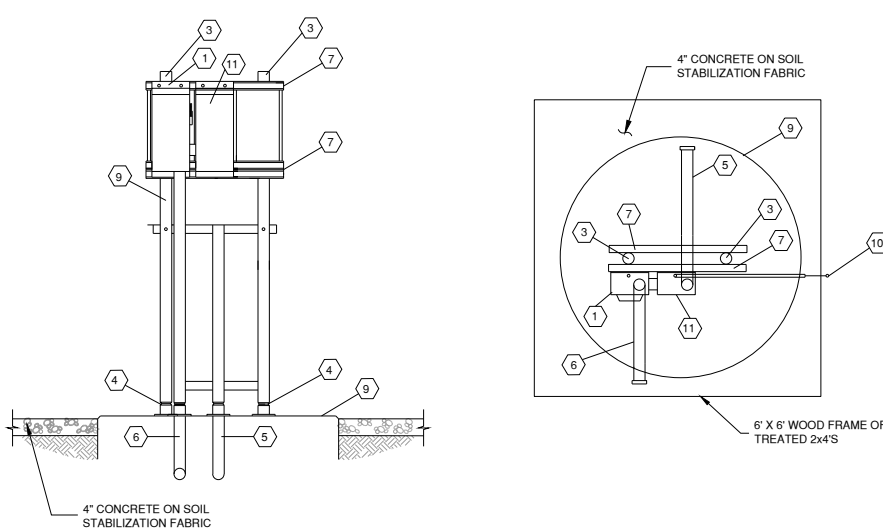


PLAN VIEW DETAIL
N.T.S.



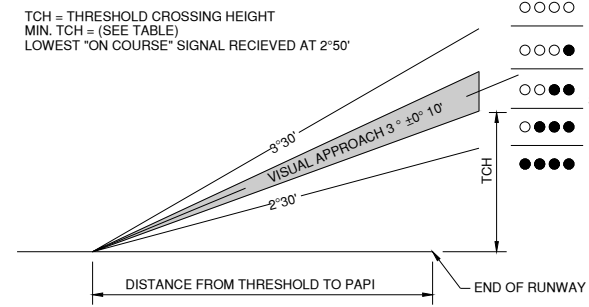
PAPI LIGHT UNIT LOCATION
N.T.S.

NOTE:
1. THE LOCATION OF THE PAPI PEDESTAL SHALL BE AS SHOWN ON THE PLANS, OUTSIDE OF RSA LIMITS.



PAPI PEDESTAL PLAN AND ELEVATION

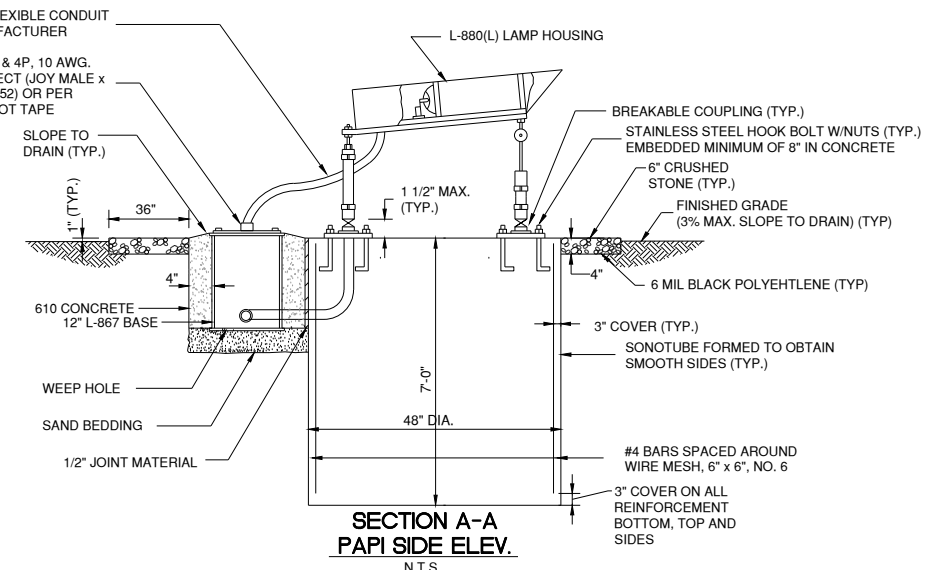
- | | |
|---|---|
| 1 HEAVY DUTY UNFUSED DISCONNECT, 600VAC, 20A, NEMA 3R. | 7 1-5/8\"/> |
| 2 NOT USED. | 8 NOT USED. |
| 3 2\"/> | |
| 4 FRANGIBLE COUPLINGS AND FLOOR FLANGES. ANCHOR TO CONCRETE FOUNDATION (TYP.). | 9 CONCRETE FOOTING, 36\"/> |
| 5 2\"/> | |
| 6 CONDUIT AND WIRING TO VAULT AS FOLLOWS:
PAPI 06: TWO #6 TYPE USE, ONE #10 GND IN 1\"/> | 10 3/4\"/> |
| | 11 NEMA 3R TRANSFORMER:
PAPI 06: NONE
PAPI 24: 3 KVA 480-120/240V |



RUNWAY TYPICAL PROFILE
N.T.S.

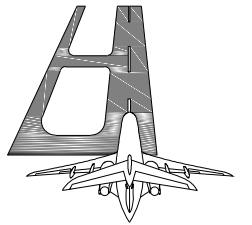
- NOTE:
- BASE DESIGN AND LAYOUT ON ADVISORY CIRCULAR 150/5340-30J. CONTRACTOR SHALL MEET THE REQUIREMENTS IN ADVISORY CIRCULAR 150/5345-28H.
 - SEE PAPI LIGHT UNIT LOCATION FOR UNIT LOCATIONS AND DIMENSIONS
 - THE VISUAL GLIDE PATH ANGLE IS THE CENTER OF THE ON COURSE ZONE AND IS MEASURED FROM THE HORIZONTAL.
 - PAPIS WERE LOCATED TO AVOID CONFLICTS WITH FUTURE RUNWAYS AND TAXIWAYS.
 - PAPI SYSTEM SHALL BE AS MANUFACTURED BY NEW BEDFORD PANOAMEX CORP. OR AS APPROVED BY FAA.
 - CONTRACTOR SHALL FIELD VERIFY EXISTING GROUND ELEVATIONS.

AIMING OF TYPE L-880 (4-BOX) PAPI RELATIVE TO PRESLECTED GLIDE PATH (3°00')	
LIGHT UNIT	AIMING ANGLE (IN MINUTES OF ARC)
	STANDARD INSTALLATION
UNIT NEAREST RUNWAY	30' BELOW GLIDE PATH
NEXT ADJACENT UNIT	10' ABOVE GLIDE PATH
NEXT ADJACENT UNIT	10' BELOW GLIDE PATH
NEXT ADJACENT UNIT	30' BELOW GLIDE PATH



SECTION A-A
PAPI SIDE ELEV.
N.T.S.

- GENERAL NOTES:
- FOUNDATIONS: FOUNDATIONS FOR MOUNTING LIGHT BOXES SHALL BE MADE OF ITEM 610 CONCRETE. ALL LIGHT BOXES SHALL BE FRANGIBLY MOUNTED TO THE FOUNDATION.
 - AZIMUTHAL AIMING: EACH LIGHT UNIT SHALL BE AIMED OUTWARD INTO THE APPROACH ZONE ON A LINE PARALLEL TO THE RUNWAY CENTERLINE WITHIN A TOLERANCE OF ±1/2 DEGREE.
 - MOUNTING HEIGHT TOLERANCES: THE BEAM CENTERS OF ALL LIGHT UNITS SHALL BE WITHIN ±1 INCH OF A HORIZONTAL PLANE AT THE ELEVATION GIVEN IN THE TABLE.
 - TOLERANCE ALONG LINE PERPENDICULAR TO RUNWAY: THE FRONT FACE OF EACH LIGHT UNIT IN A BAR SHALL BE LOCATED ON A LINE PERPENDICULAR TO THE RUNWAY CENTERLINE WITHIN ±6 INCHES.
 - PROVIDE FRANGIBLE MOUNTS FOR ALL LEGS OF LIGHT UNITS AND POWER ADAPTERS.
 - NUMBER AND CONFIGURATION OF LEGS PER MANUFACTURER. (3 LEGS MINIMUM)
 - GROUND EACH LAMP HOUSING AND POWER ADAPTER PER MANUFACTURER
 - THE POWER AND CONTROL UNIT SHALL BE STYLE A, CLASS II.
 - THE DIFFERENCE IN LATERAL SPACING BETWEEN THE LIGHT UNITS SHALL NOT EXCEED ONE FOOT.
 - EACH LIGHT UNIT SHALL HAVE TWO LAMPS AND SHALL PROVIDE A BEAM OF LIGHT SPLIT HORIZONTALLY TO PRODUCE WHITE LIGHT IN THE TOP SECTOR AND RED LIGHT IN THE BOTTOM SECTOR. THEY SHALL HAVE A MINIMUM OF THREE MOUNTING LEGS WHICH SHALL BE ADJUSTABLE TO PERMIT LEVELING WHERE ONE SIDE OF THE UNIT IS INSTALLED UP TO 1 INCH LOWER THAN THE OPPOSITE SIDE.
 - APPLY "NEVER SEEZ" OR APPROVED EQUAL TO ALL THREADED BOLTS AND CONNECTIONS.
 - THE COST OF THE PAPI SHALL INCLUDE ANY SITE WORK AROUND THE PAPI, INCLUDING BUT NOT LIMITED TO THE CRUSHED STONE, EARTHWORK AND LANDSCAPING.



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ENGINEERS AND CONSULTANTS

FREEPORT - ALBERTUS AIRPORT
FREEPORT, IL

CITY OF FREEPORT
ILLINOIS

APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 6/24
PAPI SYSTEM

SHEET TITLE:

ELECTRICAL VAULT PLAN

DESIGNED: STL DRAWN: JRO CHECKED: DJK

PROJECT NO.: 22004977-00
DATE: 09/12/2025

SHEET NO. REVISION

SHEET 17 OF 17 SHEETS

