

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR SUMMARY OF QUANTITIES SEE SHEET NO. 6 THROUGH 18

STATE OF ILLINOIS 11-08-2013 LETTING ITEM 037

| | | | |
|--------------------|----------|--------------------|-----------|
| SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 1 |
| F.A.I. 74 | ILLINOIS | CONTRACT NO. 70570 | |

*440+1=441

D-95-032-07

DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

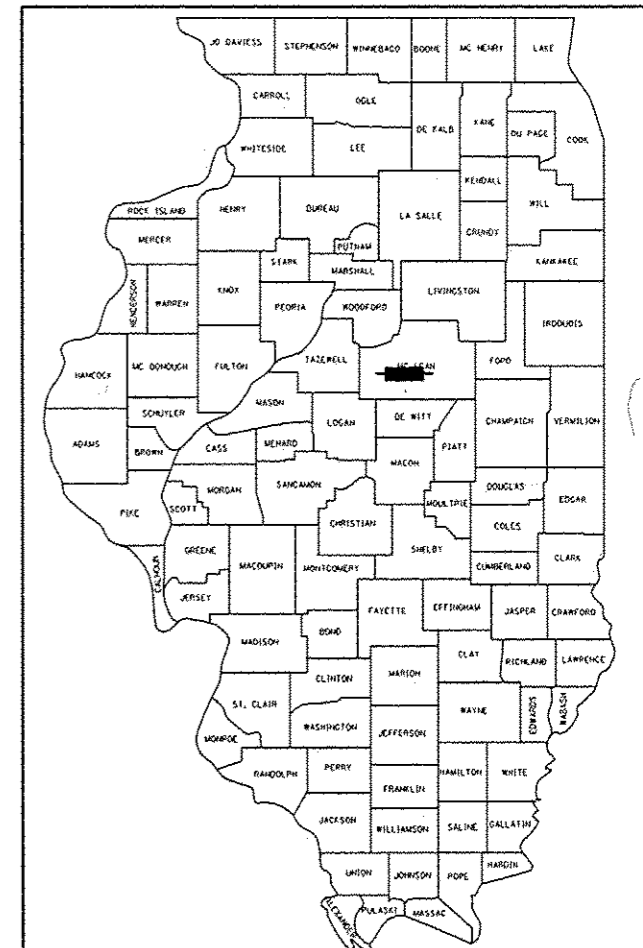
PROPOSED HIGHWAY PLANS

F.A.I. ROUTE 74 (I-74)

SECTION 57-20(HB,HB-1)BR-1
PROJECT ACNHPP-000S(945)
BRIDGE REPLACEMENT AND
VERTICAL REALIGNMENT
MCLEAN COUNTY

C-95-032-07

I-55 BUSN OVER I-74 SW OF BLOOMINGTON



LOCATION OF SECTION INDICATED THUS: - [black rectangle] -

NOTE: THE ROUTE FOR THESE PLANS IS FAI 74. ON SHEET TITLE BLOCKS WHERE FAP 704 IS SHOWN IT SHALL MEAN FAI 74.

CURRENT TRAFFIC DATA

| | LEG "A" | LEG "B" | LEG "C" | LEG "D" |
|----------|---------|---------|---------|---------|
| 2011 ADT | 5,200 | 7,600 | 9,400 | 20,700 |
| 2031 ADT | 6,000 | 9,700 | 12,900 | 28,200 |
| P.U. % | 93.4 | 90.6 | 91.9 | 60.9 |
| S.U. % | 2.4 | 5.5 | 3.8 | 5.0 |
| M.U. % | 4.2 | 3.9 | 4.3 | 34.1 |

LEG "A" IS VETERAN'S PARKWAY FROM I-55 TO RAMPS F & J.
LEG "B" IS VETERAN'S PARKWAY FROM RAMPS F & J TO RAMP M.
LEG "C" IS VETERAN'S PARKWAY FROM RAMP M TO HAMILTON ROAD.
LEG "D" IS I-74 UNDER VETERAN'S PARKWAY.

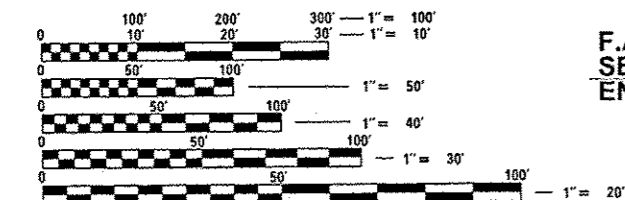
DESIGN DESIGNATION

LEG A: 640(31) OTHER PRINCIPAL ARTERIAL 7.11 (FD-20)
LEG B: 1080(31) OTHER PRINCIPAL ARTERIAL 7.11 (FD-20)
LEG C: 1410(31) OTHER PRINCIPAL ARTERIAL 7.11 (FD-20)

EXISTING SN 057-0062 AT STA. 635+75.15
CARRYING SB I-55 BUSN OVER WB I-74
TO BE REMOVED AND REPLACED.
PROPOSED SN 057-0254 AT STA. 635+73.19
3 SPAN STEEL BEAM BRIDGE 204'-6" BK-BK.
SKEW 30°-00'-00" LEFT FORWARD

EXISTING SN 057-0061 AT STA. 635+75.15
CARRYING NB I-55 BUSN OVER WB I-74
TO BE REMOVED AND REPLACED.
PROPOSED SN 057-0253 AT STA. 635+73.19
3 SPAN STEEL BEAM BRIDGE 204'-6" BK-BK.
SKEW 30°-00'-00" LEFT FORWARD

EXISTING SN 057-0060 AT STA. 638+50.46
CARRYING SB I-55 BUSN OVER EB I-74
TO BE REMOVED AND REPLACED.
PROPOSED SN 057-0252 AT STA. 638+48.84
3 SPAN STEEL BEAM BRIDGE 183'-0" BK-BK.
SKEW 3°-48'-00" LEFT FORWARD



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

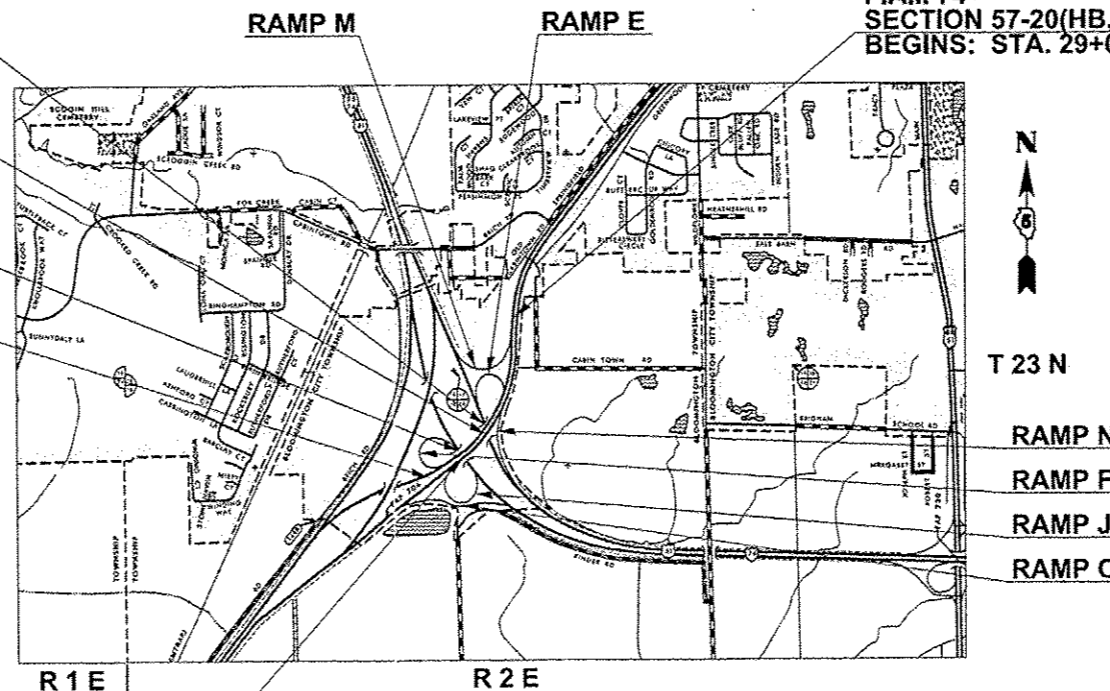
J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811 BLOOMINGTON TOWNSHIP

PROJECT ENGINEER: NANCY FASIG
SQUAD LEADER: BRIAN J. HOGAN
(217)-465-4181

CONTRACT NO. 70570

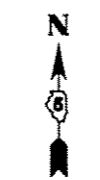
F.A.I. 74
SECTION 57-20(HB,HB-1)BR-1
ENDS: STA. 65+00.00

EXISTING SN 057-0059 AT STA. 638+50.46
CARRYING NB I-55 BUSN OVER EB I-74
TO BE REMOVED AND REPLACED.
PROPOSED SN 057-0251 AT STA. 638+48.84
3 SPAN STEEL BEAM BRIDGE 183'-0" BK-BK.
SKEW 3°-48'-00" LEFT FORWARD



GROSS LENGTH = 3,600.00 FT. = 0.682 MILE
NET LENGTH = 3,600.00 FT. = 0.682 MILE

F.A.I. 74
SECTION 57-20(HB,HB-1)BR-1
BEGINS: STA. 29+00.00



T 23 N

RAMP N
RAMP F
RAMP J
RAMP O

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
SUBMITTED 8/15/13
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
October 4, 2013
John D. Baranowski, P.E.
acting ENGINEER OF DESIGN AND ENVIRONMENT
October 4, 2013
Omer Osman, P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

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| | | | | | | | | | | | |
|-----------------------------------------|-----------------------------|------------|-----------|-----------------------------------------------------------|------------------------|--------------------|-------------------|-------------|--------------|-----------|---------------------------|
| FILE NAME : | USER NAME : detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | INDEX OF SHEETS | F.A.P. RTE: | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ci:\p\work\p\idot\detersbj\0157116\0570 | 70-ahtr-gennote.dgn | DRAWN - | REVISED - | | | 704 | 57-204HB,HB-1BR-1 | MCLEAN | 440 | 2 | |
| #MODELNAME# | PLOT SCALE : 40.0000' / in. | CHECKED - | REVISED - | | | CONTRACT NO. 70570 | | | | | |
| | PLOT DATE : 8/13/2013 | DATE - | REVISED - | | | SCALE: | SHEET 1 | OF 1 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT |

LIST OF HIGHWAY STANDARDS

| STANDARD NO. | DESCRIPTION | STANDARD NO. | DESCRIPTION |
|--------------|----------------------------------------------------------------------------------------|--------------|---------------------------------------------------------------------------------|
| 000001 - 06 | STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS | 701101 - 03 | OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE |
| 001001 - 02 | AREAS OF REINFORCEMENT BARS | 701106 - 02 | OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY |
| 001006 | DECIMAL OF AN INCH AND OF A FOOT | 701400 - 06 | APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY |
| 202001 - 01 | EARTH MEDIAN DITCH CHECK | 701401 - 07 | LANE CLOSURE, FREEWAY/EXPRESSWAY |
| 280001 - 07 | TEMPORARY EROSION CONTROL SYSTEMS | 701402 - 09 | LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH BARRIER |
| 406001 - 05 | ENTRANCE RAMP TERMINAL (FLEXIBLE RAMP PAVEMENT ADJACENT TO FLEXIBLE MAINLINE PAVEMENT) | 701406 - 06 | LANE CLOSURE, FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY |
| 406101 - 04 | EXIT RAMP TERMINAL (FLEXIBLE RAMP PAVEMENT ADJACENT TO FLEXIBLE MAINLINE PAVEMENT) | 701411 - 08 | LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS >= 45 MPH |
| 420001 - 07 | PAVEMENT JOINTS | 701426 - 05 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS >= 45 MPH |
| 420401 - 09 | BRIDGE APPROACH PAVEMENT CONNECTOR | 701901 - 02 | TRAFFIC CONTROL DEVICES |
| 442201 - 03 | CLASS C AND D PATCHES | 704001 - 07 | TEMPORARY CONCRETE BARRIER |
| 482001 - 02 | HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT | 720011 - 01 | METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS |
| 515001 - 03 | NAME PLATE FOR BRIDGES | 720021 - 02 | SIGN PANELS, EXTRUDED ALUMINUM TYPE |
| 542401 - 01 | METAL END SECTION FOR PIPE CULVERTS | 780001 - 03 | TYPICAL PAVEMENT MARKINGS |
| 542546 - 01 | FLUSH INLET BOX FOR MEDIAN | 781001 - 03 | TYPICAL APPLICATIONS OF RAISED REFLECTIVE PAVEMENT MARKINGS |
| 601001 - 04 | SUB-SURFACE DRAINS | 825016 - 02 | LIGHTING CONTROLLER, PEDESTAL MOUNTED, 480 V |
| 601101 - 01 | CONCRETE HEADWALL FOR PIPE DRAIN | 830001 - 01 | LIGHT POLE ALUMINUM MAST ARM |
| 610001 - 06 | SHOULDER INLET WITH CURB | 830026 | TEMPORARY ROADWAY LIGHTING |
| 630001 - 10 | STEEL PLATE BEAM GUARDRAIL | 836001 - 02 | LIGHT POLE FOUNDATION |
| 630301 - 06 | SHOULDER WIDENING FOR TYPE 1(SPECIAL) GUARDRAIL TERMINALS | | |
| 631011 - 09 | TRAFFIC BARRIER TERMINAL, TYPE 2 | | |
| 631026 - 05 | TRAFFIC BARRIER TERMINAL, TYPE 5 | | |
| 631031 - 11 | TRAFFIC BARRIER TERMINAL, TYPE 6 | | |
| 631033 - 04 | TRAFFIC BARRIER TERMINAL, TYPE 6B | | |
| 635001 - 01 | DELINEATORS | | |
| 635006 - 03 | REFLECTOR AND TERMINAL MARKER PLACEMENT | | |
| 635011 - 02 | REFLECTOR MARKER AND MOUNTING DETAILS | | |
| 642001 - 02 | SHOULDER RUMBLE STRIPS, 16 IN. | | |
| 667101 - 02 | PERMANENT SURVEY MARKERS | | |

| | | | | | | | | | | | |
|-------------------------------------------------------------|------------------------------|------------|-----------|-----------------------------------------------------------|----------------------------------|--------------------|---------------------|--------------|---------------------------|--------------|--|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | LIST OF HIGHWAY STANDARDS | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| et:\pvc\work\pvidot\detersbj\0157116\057878-shs-gannoia.dgn | 78-shs-gannoia.dgn | DRAWN - | REVISED - | | | 704 | 57-20(HB,HB-U)R-1 | MCLEAN | 440 | 3 | |
| #MODELNAME# | PLOT SCALE = 40,0000' / 1" = | CHECKED - | REVISED - | | | CONTRACT NO. 70570 | | | | | |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - | | | SCALE: | SHEET 1 OF 1 SHEETS | STA. TO STA. | ILLINOIS FED. AID PROJECT | | |

GENERAL NOTES

G.N.-100
ENGLISH UNITS OF MEASUREMENT SHALL GOVERN OVER AND SUPERSEDE ANY METRIC UNITS SHOWN IN THIS CONTRACT. WHERE INCLUDED, METRIC UNITS ARE FOR INFORMATION ONLY.

G.N.-100A
ELECTRONIC FILES AND/OR ELECTRONIC SURVEY INFORMATION INCLUDING CADD FILES WILL NOT BE AVAILABLE TO THE CONTRACTOR.

G.N.-105.07B
EXISTING STATE-OWNED AND MAINTAINED UTILITY LINES ARE SHOWN ON THE PLANS TO INDICATE THEIR PRESENCE AND APPROXIMATE LOCATION. THE CONTRACTOR SHALL SECURE AN APPROVED LOCATING FIRM TO LOCATE STATE-OWNED UTILITIES PRIOR TO COMMENCING ANY EXCAVATION IN THE VICINITY OF THESE LINES IN ACCORDANCE TO SECTION 803 OF THE STANDARD SPECIFICATIONS. SHOULD ANY OF THE LINES BE DAMAGED BY THE CONTRACTOR'S OPERATION, THE CONTRACTOR SHALL REPAIR THEM TO THE SATISFACTION OF THE ENGINEER AT NO COST TO THE STATE.

ALSO THERE MAY BE UTILITIES PRESENT WHICH WERE INSTALLED BY THE STATE BUT ARE MAINTAINED BY OTHERS (CITY, TOWN, ETC.) THE APPROXIMATE LOCATIONS OF THESE LINES ARE ALSO SHOWN ON THE PLANS ALONG WITH THE NAME OF THE MAINTAINING AGENCY. THE CONTRACTOR SHALL COORDINATE THE LOCATION OF THESE LINES WITH THE LOCAL AGENCY PRIOR TO COMMENCING ANY EXCAVATION OR BORING IN THEIR VICINITY. SHOULD THESE LINES BE DAMAGED BY THE CONTRACTOR'S OPERATIONS, THE CONTRACTOR SHALL REPAIR THEM TO THE SATISFACTION OF, AND AT NO COST TO, THE LOCAL AGENCY AND THE STATE.

G.N.-105.09A
ALL ELEVATIONS SHOWN IN THE PLANS ARE BASED ON NORTH AMERICAN VERTICAL DATUM OF 1988. (NAVD 88)

G.N.-202
GRADING SHALL BE DONE BY HAND AROUND LIGHT POLES, UTILITY POLES, SIGN POSTS, SHRUBS, TREES OR OTHER NATURAL OR MAN-MADE OBJECTS WHERE SHALLOW FILLS OR CUTS ARE ADJACENT TO THE ITEMS. IT IS THE INTENT THAT THE LIMITS OF CONSTRUCTION SUCH AS TO PRESERVE IN THE ORIGINAL STATE AS MUCH AREA OF TEMPORARY EASEMENTS AS POSSIBLE. THE DECISION AS TO ITEMS TO REMAIN IN PLACE SHALL BE AS DIRECTED BY THE ENGINEER.

THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE PER CUBIC YARD FOR EARTH EXCAVATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

G.N.-205
BENCHING PROCEDURES SHALL BE USED IN AREAS WHERE EXISTING EMBANKMENTS ARE WIDENED FOR THE PROPOSED PAVEMENT. STEPS SHALL BE CUT INTO THE EXISTING EMBANKMENT SLOPES AND SHALL HAVE THE FOLLOWING DIMENSIONS:
HORIZONTAL: 2
VERTICAL: 1

G.N.-250C - SPL
SEEDING, CLASS 7 AND MULCH, METHOD 2 IS INCLUDED IN THIS CONTRACT TO SEED NEW EMBANKMENT DURING TIME PERIODS WHEN PERMANENT SEEDING IS NOT ALLOWED. SOME OR ALL OF THE CLASS 7 SEEDING AND MULCH WILL BE DELETED IF IT IS POSSIBLE TO PLACE PERMANENT SEEDING ON THE EMBANKMENT AT THE TIME OF IT'S COMPLETION.

G.N.-280
TEMPORARY EROSION CONTROL SEEDING IS INCLUDED IN THIS CONTRACT TO SEED DISTURBED EARTH DURING TIME PERIODS WHEN PERMANENT SEEDING IS NOT ALLOWED. SOME OR ALL OF THE TEMPORARY EROSION CONTROL SEEDING WILL BE DELETED IF IT IS POSSIBLE TO PLACE PERMANENT SEEDING ON EARTH AT THE TIME OF THEIR COMPLETION.

G.N.-302
WHERE SMALL QUANTITIES OF LIME MODIFICATION ARE SHOWN IN THE PLANS, SUBBASE GRANULAR MATERIAL, TYPE A MAY BE SUBSTITUTED AND CONSTRUCTED ACCORDING TO THE APPLICABLE PORTIONS OF SECTION 311. THE DEPTH OF THE SUBBASE GRANULAR MATERIAL SHALL BE THE SAME AS THE PROPOSED DEPTH OF THE LIME MODIFICATION. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR PROCESSING LIME MODIFIED SOILS OF THE DEPTH SPECIFIED. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

G.N.-406
THE QUANTITIES INCLUDED IN THE PLANS FOR HOT-MIX ASPHALT RESURFACING ARE INTENDED TO GIVE THE COVERAGE SHOWN ON THE TYPICAL CROSS SECTIONS. IT IS NOT INTENDED TO INCREASE THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURE IN ORDER TO USE ALL OF THE QUANTITIES INCLUDED IN THE CONTRACT.

G.N.-406.05b
ALL LEVELING BINDER OR BINDER SHALL BE GIVEN A FOG COAT OF PRIME BEFORE THE SURFACE COURSE IS PLACED WHEN DIRECTED BY THE ENGINEER.

THE FOG COAT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER GALLON FOR BITUMINOUS MATERIAL (PRIME COAT) AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

G.N.-406.10
FOR MULTILANE RESURFACING

WHEN BEGINNING THE RESURFACING WITH NEW MIXTURES FOR LEVELING BINDER, BINDER COURSE, AND SURFACE COURSE MIXTURES, THE WORK WILL BE CONFINED TO THE INSIDE TRAFFIC LANE (PASSING LANE) FIRST. THE WORK WILL REMAIN ON THE INSIDE LANE UNTIL THE MIX HAS BEEN ADJUSTED AND APPROVED BY THE ENGINEER BEFORE ANY RESURFACING IS ALLOWED ON THE OUTSIDE (DRIVING) TRAFFIC LANE(S).

ANY DELAYS OR INCONVENIENCES CAUSED THE CONTRACTOR IN COMPLYING WITH THIS REQUIREMENT WILL BE CONSIDERED INCIDENTAL TO THE VARIOUS HOT-MIX ASPHALT PAY ITEMS, AS SHOWN IN THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

G.N.-406F
THIS JOB INCLUDES LEVELING BINDER OF 1-1/4 INCHES OR GREATER THICKNESS. LOCATIONS OF LEVELING BINDER EQUAL TO OR GREATER THAN 1-1/4 INCHES IN THICKNESS ARE AS FOLLOWS:
F.A.P. 704 SOUTHBOUND STA. 29+00.00 TO STA. 44+07.00
F.A.P. 704 NORTHBOUND STA. 32+60.00 TO STA. 43+62.00
RAMP N STA. 24+61.61 TO STA. 26+87.12
RAMP M STA. 30+00.00 TO STA. 37+79.28

THE ABOVE LIST MAY NOT BE ALL INCLUSIVE DUE TO CONSTRUCTION VARIATIONS, VARIATIONS BETWEEN PLOTTED CROSS-SECTIONS, OR OTHER REASONS. ALL APPLICABLE REQUIREMENTS OF SECTION 406 OF THE STANDARD SPECIFICATIONS WILL BE ENFORCED FOR ALL LEVELING BINDER CONSTRUCTED 1-1/4 INCHES OR THICKER.

G.N.-406H - MIXTURE REQUIREMENTS Contract: 70570

| Location | FAP 704 & FAI 74 | FAP 704 | FAP 704 |
|---------------------|-----------------------------------------------------------------------|----------------|----------------|
| Mixture Use | FG Binder, Class D Base Cse (Opt), Flex Connector, Patch (pvt & shld) | Poly FG Binder | Poly Surface |
| AC/PG | PG 64-22 | SBS PG 70-22 | SBS PG 70-22 |
| RAP % (Max) | * | * | * |
| Design Air Voids | 4.0% @ Ndes=90 | 4.0% @ Ndes=90 | 4.0% @ Ndes=90 |
| Mix Comp(Gradation) | IL 19.0 FG | IL 19.0 FG | IL 9.5 |
| Friction Aggregate | N.A. | N.A. | Mix D |

| Location | FAP 704 | FAP 704 | FAP 704 |
|---------------------|--------------------|--------------------------|-------------------------------------|
| Mixture Use | Bottom Lifts Shldr | Top Lift Shldr and Incid | Poly FG Leveling Binder (Mach Meth) |
| AC/PG | PG 64-22 | PG 64-22 | SBS PG 70-22 |
| RAP % (Max) | * | * | * |
| Design Air Voids | 2.0% @ Ndes=30 | 4.0% @ Ndes=30 | 4.0% @ Ndes=90 |
| Mix Comp(Gradation) | Other | IL 9.5L | IL 9.5 |
| Friction Aggregate | N.A. | Mix C | Mix C |

* See RAP - RAS BDE Special Provision

G.N.-442B - PATCHING SCHEDULES
THE PATCHING SCHEDULES INCLUDED IN THE PLANS REPRESENT THE BEST INFORMATION AVAILABLE AT THE TIME OF COMPLETION OF THE PLANS FOR LETTING. VARIATIONS IN LOCATION AND SIZES OF BOTH FULL-DEPTH AND PARTIAL-DEPTH PATCHES MAY OCCUR.

| | | | | | | | | | | |
|----------------------------------------------------------------|-----------------------|------------|--------------------|-----------------------------------------------------------|----------------------|---------------------------|---------------------|--------|-----------------|--------------|
| FILE NAME * | USER NAME = detarsbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GENERAL NOTES | F.A.P. RTE: | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| at:\pvc\work\pvc\dote\detarsbj\05157116\057070-aht-gennote.dgn | DRAWN - | REVISED - | 704 | | | 57-20HB,HB-UBR-1 | MCLEAN | 440 | 4 | |
| PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | CONTRACT NO. 70570 | | | | | | | |
| MODELNAME* | PLOT DATE = 8/13/2013 | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | |
| | | | | | | SCALE: | SHEET 1 OF 2 SHEETS | | STA. TO STA. | |

GENERAL NOTES

G.N.-482

ALL MATERIAL PLACED AS HOT-MIX ASPHALT SHOULDERS SHALL BE COMPACTED FROM 94.0 TO 98.4 PERCENT OF THE MAXIMUM THEORETICAL DENSITY. THIS REQUIREMENT SHALL APPLY TO IL 9.5L GRADATION SHOULDER MIXES AND OTHER MIXES (BOTTOM LIFT OF SHOULDERS). THIS MAXIMUM DENSITY SHALL BE DETERMINED FROM THE MOVING AVERAGE OF FOUR TESTS AS IN OTHER QC/QA TESTING. A NUCLEAR GAUGE DENSITY/CORE CORRELATION SHALL BE PERFORMED FOR THE IL 9.5L MIXES AND OTHER MIXES USING STANDARD CORRELATION PROCEDURES.

G.N.-542

BEFORE ORDERING PIPE CULVERTS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR THE EXACT LENGTHS.

G.N.-609

PRIOR TO ROUTING TRAFFIC ONTO THE SHOULDERS AS SHOWN IN THE STAGING PLANS, THE CONTRACTOR SHALL SECURE THE GRATINGS ON SHOULDER INLETS AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE PAID FOR ACCORDING TO ARTICLE 109.04.

G.N.-631

IF THE CONTRACTOR ELECTS TO USE THE ALTERNATE MOUNTING METHOD OF THRU DRILLING THE MOUNTING HOLES FOR THE TRAFFIC BARRIER TERMINALS, TYPE 6, THE HOLES SHALL BE DRILLED USING A CORE DRILL. A HAMMER DRILL WILL NOT BE ALLOWED.

G.N.-667

THE RESIDENT ENGINEER SHALL CONTACT THE PROGRAM DEVELOPMENT CHIEF OF SURVEYS PRIOR TO THE PRE-CONSTRUCTION CONFERENCE FOR INSTRUCTION AS TO SETTING OF TEMPORARY OR PERMANENT TIES FOR CENTERLINE ALIGNMENT CONTROL SURVEY MARKERS (PC'S, PT'S, AND PI'S). PROJECT IMPLEMENTATION PERSONNEL WILL BE RESPONSIBLE FOR LAYOUT OF THESE MARKERS.

G.N.-703A - SPL

SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE PAVEMENT AFTER ANY OF THE FOLLOWING IN AREAS OPEN TO TRAFFIC: COLD MILLING AND/OR PLACING BITUMINOUS MATERIALS (PRIME COAT), LEVELING BINDER (MACHINE METHOD), BINDER AND SURFACE COURSES. SHORT TERM PAVEMENT MARKING PLACED ON THE SURFACE, SHALL COINCIDE WITH THE FINAL PAVEMENT STRIPING. SHORT TERM PAVEMENT MARKING PLACED PRIOR TO THE SURFACE SHALL COINCIDE WITH THE EXISTING PAVEMENT MARKINGS. USE 4 FEET PER 40 FEET (OR 10% PER STATION).

G.N.-781- SPL

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH STANDARD 781001, AND THE DETAILS SHOWN IN THE PLANS. IF THERE IS ANY DISCREPANCY BETWEEN THE STANDARD AND THE DETAILS IN THE PLANS, THE DETAILS IN THE PLANS SHALL GOVERN. THE FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING THE RAISED REFLECTIVE PAVEMENT MARKERS.

PAVEMENT MARKERS ON F.A.P. 704 SHALL BE PLACED TWO (EACH) AT EVEN SPACING IN EVERY OTHER 30 FOOT (9 m) SPACE BETWEEN THE DASHED CENTERLINE STRIPES.

G.N.-1004.01

COARSE AGGREGATE GRADATION CA-10 MAY BE USED WHENEVER COARSE AGGREGATE CA-6 IS SPECIFIED IN THE STANDARD SPECIFICATIONS.

G.N.-Z0038

AN ALUMINUM TABLET OF THE TYPE SHOWN ON STANDARD 667101 SHALL BE PLACED ON THE PROPOSED STRUCTURE AS DIRECTED BY THE ENGINEER. THE BENCH MARK ELEVATION WILL BE ESTABLISHED AND MARKED BY THE DEPARTMENT. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR PERMANENT BENCH MARKS.

NO COMMITMENTS

| | | | | | | | | | | |
|----------------------------------------------------------|----------------------------------|------------|-----------|-----------------------------------------------------------|----------------------|----------------|--------------------|--------|-----------------|---------------------------|
| FILE NAME * | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GENERAL NOTES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pe-work\pedit\detersbj\0157116\057870-shr-ganno1a.dgn | | DRAWN - | REVISED - | | | 704 | 57-201HB,HB-118R-1 | MCLEAN | 440 | 5 |
| *MODELNAME* | PLOT SCALE = 40.0000' / 1" / in. | CHECKED - | REVISED - | | SCALE: | | | | | |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - | | SHEET 2 OF 2 SHEETS | STA. | TO STA. | | | CONTRACT NO. 70570 |
| | | | | | | | | | | ILLINOIS FED. AID PROJECT |

LOCATION OF WORK: **FAI 74** S.N. 057-0251 S.N. 057-0252 S.N. 057-0253 S.N. 057-0254
 URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE
 OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL
 STA. 29+00 TO STA. 65+00 STA. 57+19.05 TO STA. 58+99.57 STA. 57+12.20 TO STA. 58+97.86 STA. 51+55.69 TO STA. 53+56.48 STA. 51+14.85 TO STA. 53+23.26
 MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY
 FUNDING BREAKOUT: 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE
 CONSTRUCTION TYPE CODE: 0004 0011 0011 0011 0011

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | | | | |
|----------|-------------------------------------------------------------|--------|----------------|---------|-----|-----|-----|-----|
| 28200200 | FILTER FABRIC | SQ YD | 850.0 | 850.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 30200650 | PROCESSING MODIFIED SOIL 12" | SQ YD | 2,500.0 | 2,500.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 30201500 | LIME | TON | 57.0 | 57.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 31101200 | SUBBASE GRANULAR MATERIAL, TYPE B 4" | SQ YD | 320.0 | 320.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 31101900 | SUBBASE GRANULAR MATERIAL, TYPE C | TON | 205.0 | 205.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 40600100 | BITUMINOUS MATERIALS (PRIME COAT) | GALLON | 8,010.0 | 8,010.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 55.0 | 55.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 40600895 | CONSTRUCTING TEST STRIP | EACH | 2.0 | 2.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 40600909 | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N90 | TON | 710.0 | 710.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 6,050.0 | 6,050.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 40600990 | TEMPORARY RAMP | SQ YD | 235.0 | 235.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 40603092 | HOT-MIX ASPHALT BINDER COURSE, IL-19.0 FG, N90 | TON | 7,000.0 | 7,000.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 40603243 | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 | TON | 2,380.0 | 2,380.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 40603545 | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 | TON | 3,030.0 | 3,030.0 | 0.0 | 0.0 | 0.0 | 0.0 |

• DENOTES SPECIALTY ITEM

14

LOCATION OF WORK: **FAE 74** S.N. 057-0251 S.N. 057-0252 S.N. 057-0253 S.N. 057-0254
 URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE
 OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL
 STA. 29+00 TO STA. 65+00 STA. 57+19.05 TO STA. 58+99.57 STA. 57+12.20 TO STA. 58+97.86 STA. 51+55.69 TO STA. 53+56.48 STA. 51+14.85 TO STA. 53+23.26
 MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY
 FUNDING BREAKOUT: 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE
 CONSTRUCTION TYPE CODE: 0004 0011 0011 0011 0011

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | | | | | |
|----------|-----------------------------------------------|-------|----------------|---------|-----|-----|-----|-----|-----|
| 42001430 | BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE) | SQ YD | 270.0 | 270.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 44000100 | PAVEMENT REMOVAL | SQ YD | 1,755.0 | 1,755.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 44000155 | HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" | SQ YD | 7,190.0 | 7,190.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 44000400 | GUTTER REMOVAL | FOOT | 785.0 | 785.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 44004000 | PAVED DITCH REMOVAL | FOOT | 150.0 | 150.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 44004250 | PAVED SHOULDER REMOVAL | SQ YD | 680.0 | 680.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 44201765 | CLASS D PATCHES, TYPE II, 10 INCH | SQ YD | 16.0 | 16.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 44201771 | CLASS D PATCHES, TYPE IV, 10 INCH | SQ YD | 305.0 | 305.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 48100500 | AGGREGATE SHOULDERS, TYPE A 6" | SQ YD | 4,180.0 | 4,180.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 48100700 | AGGREGATE SHOULDERS, TYPE A 8" | SQ YD | 370.0 | 370.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 48203100 | HOT-MIX ASPHALT SHOULDERS | TON | 5,460.0 | 5,460.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 50100300 | REMOVAL OF EXISTING STRUCTURES NO. 1 | EACH | 1.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 50100400 | REMOVAL OF EXISTING STRUCTURES NO. 2 | EACH | 1.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 |
| 50100500 | REMOVAL OF EXISTING STRUCTURES NO. 3 | EACH | 1.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 |

14 * DENOTES SPECIALTY ITEM

| | | | | | |
|-------------------------|--------------------------|--------------------------------|--------------------------------|--------------------------------|--------------------------------|
| LOCATION OF WORK: | FAI 74 | S.N. 057-0251 | S.N. 057-0252 | S.N. 057-0253 | S.N. 057-0254 |
| | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE |
| | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL |
| | STA. 29+00 TO STA. 65+00 | STA. 57+19.05 TO STA. 58+99.57 | STA. 57+12.20 TO STA. 58+97.86 | STA. 51+55.69 TO STA. 53+56.48 | STA. 51+14.85 TO STA. 53+23.26 |
| | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY |
| FUNDING BREAKOUT: | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE |
| CONSTRUCTION TYPE CODE: | 0004 | 0011 | 0011 | 0011 | 0011 |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | | | | | |
|--------------------------|-------------------------------------------------|--------------|----------------|------------|-------------|-------------|-------------|-------------|-----|
| 50100600 | REMOVAL OF EXISTING STRUCTURES NO. 4 | EACH | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 |
| 50104400 | CONCRETE HEADWALL REMOVAL | EACH | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 50104650 | SLOPE WALL REMOVAL | SQ YD | 2,767.0 | 0.0 | 702.0 | 649.0 | 646.0 | 770.0 | |
| 50105220 | PIPE CULVERT REMOVAL | FOOT | 379.0 | 379.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 50157300 | PROTECTIVE SHIELD | SQ YD | 1,280.0 | 0.0 | 363.0 | 327.0 | 258.0 | 332.0 | |
| 50200100 | STRUCTURE EXCAVATION | CU YD | 3,144.0 | 0.0 | 726.0 | 632.0 | 872.0 | 914.0 | |
| 50300225 | CONCRETE STRUCTURES | CU YD | 1,378.3 | 0.0 | 375.1 | 308.6 | 333.5 | 361.1 | |
| 50300255 | CONCRETE SUPERSTRUCTURE | CU YD | 2,077.9 | 0.0 | 551.6 | 476.7 | 504.6 | 545.0 | |
| 50300260 | BRIDGE DECK GROOVING | SQ YD | 5,683.0 | 0.0 | 1,544.0 | 1,303.0 | 1,351.0 | 1,485.0 | |
| 50300280 | CONCRETE ENCASEMENT | CU YD | 12.4 | 0.0 | 0.0 | 0.0 | 6.2 | 6.2 | |
| 50300300 | PROTECTIVE COAT | SQ YD | 6,599.0 | 0.0 | 1,762.0 | 1,521.0 | 1,591.0 | 1,725.0 | |
| 50500105 | FURNISHING AND ERECTING STRUCTURAL STEEL | L SUM | 1.0 | 0.0 | 0.25 | 0.20 | 0.26 | 0.29 | |
| 50500505 | STUD SHEAR CONNECTORS | EACH | 28,641.0 | 0.0 | 7,452.0 | 5,985.0 | 6,972.0 | 8,232.0 | |
| 50800205 | REINFORCEMENT BARS, EPOXY COATED | POUND | 696,820.0 | 0.0 | 177,290.0 | 169,120.0 | 168,770.0 | 181,640.0 | |
| 50800515 | BAR SPLICERS | EACH | 4,682.0 | 0.0 | 1,088.0 | 1,172.0 | 1,193.0 | 1,229.0 | |
| * DENOTES SPECIALTY ITEM | | | | | | | | | |

| | | | | | |
|-------------------------|--------------------------|--------------------------------|--------------------------------|--------------------------------|--------------------------------|
| LOCATION OF WORK: | <i>FAI 74</i> | S.N. 057-0251 | S.N. 057-0252 | S.N. 057-0253 | S.N. 057-0254 |
| | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE |
| | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL |
| | STA. 29+00 TO STA. 65+00 | STA. 57+19.05 TO STA. 58+99.57 | STA. 57+12.20 TO STA. 58+97.86 | STA. 51+55.69 TO STA. 53+56.48 | STA. 51+14.85 TO STA. 53+23.26 |
| | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY |
| FUNDING BREAKOUT: | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE |
| CONSTRUCTION TYPE CODE: | 0004 | 0011 | 0011 | 0011 | 0011 |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | | | | |
|----------|-------------------------------------------|-------|----------------|-----|---------|---------|---------|---------|
| 51100100 | SLOPE WALL 4 INCH | SQ YD | 3,269.0 | 0.0 | 793.0 | 840.0 | 678.0 | 958.0 |
| 51200958 | FURNISHING METAL SHELL PILES 14" X 0.250" | FOOT | 2,297.0 | 0.0 | 1,217.0 | 0.0 | 1,080.0 | 0.0 |
| 51200959 | FURNISHING METAL SHELL PILES 14" X 0.312" | FOOT | 2,476.0 | 0.0 | 0.0 | 1,422.0 | 0.0 | 1,054.0 |
| 51201600 | FURNISHING STEEL PILES HP12X53 | FOOT | 1,352.0 | 0.0 | 0.0 | 0.0 | 680.0 | 672.0 |
| 51202305 | DRIVING PILES | FOOT | 6,125.0 | 0.0 | 1,217.0 | 1,422.0 | 1,760.0 | 1,726.0 |
| 51203200 | TEST PILE METAL SHELLS | EACH | 12.0 | 0.0 | 4.0 | 4.0 | 2.0 | 2.0 |
| 51203600 | TEST PILE STEEL HP12X53 | EACH | 4.0 | 0.0 | 0.0 | 0.0 | 2.0 | 2.0 |
| 51204650 | PILE SHOES | EACH | 178.0 | 0.0 | 58.0 | 46.0 | 38.0 | 36.0 |
| 51500100 | NAME PLATES | EACH | 4.0 | 0.0 | 1.0 | 1.0 | 1.0 | 1.0 |
| 52100010 | ELASTOMERIC BEARING ASSEMBLY, TYPE I | EACH | 60.0 | 0.0 | 18.0 | 14.0 | 14.0 | 14.0 |
| 52100520 | ANCHOR BOLTS, 1" | EACH | 120.0 | 0.0 | 36.0 | 28.0 | 28.0 | 28.0 |
| 52100530 | ANCHOR BOLTS, 1 1/4" | EACH | 120.0 | 0.0 | 36.0 | 28.0 | 28.0 | 28.0 |
| 54213447 | END SECTIONS 12" | EACH | 2.0 | 2.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 54213459 | END SECTIONS 24" | EACH | 2.0 | 2.0 | 0.0 | 0.0 | 0.0 | 0.0 |

• DENOTES SPECIALTY ITEM

| | | | | | |
|-------------------------|--------------------------|--------------------------------|--------------------------------|--------------------------------|--------------------------------|
| LOCATION OF WORK: | FAI 74 | S.N. 057-0251 | S.N. 057-0252 | S.N. 057-0253 | S.N. 057-0254 |
| | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE |
| | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL |
| | STA. 29+00 TO STA. 65+00 | STA. 57+19.05 TO STA. 58+99.57 | STA. 57+12.20 TO STA. 58+97.86 | STA. 51+55.69 TO STA. 53+56.48 | STA. 51+14.85 TO STA. 53+23.26 |
| | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY |
| FUNDING BREAKOUT: | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE |
| CONSTRUCTION TYPE CODE: | 0004 | 0011 | 0011 | 0011 | 0011 |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | | | | | |
|--------------------------|----------------------------------------------------------------|-------|----------------|---------|---------|---------|---------|---------|-----|
| 54244405 | FLUSH INLET BOX FOR MEDIAN, STANDARD 542546 | EACH | 2.0 | 2.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 54248510 | CONCRETE COLLAR | CU YD | 3.0 | 3.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 542A1069 | PIPE CULVERTS, CLASS A, TYPE 2 24" | FOOT | 25.0 | 25.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 542C0217 | PIPE CULVERTS, CLASS C, TYPE 1 12" | FOOT | 172.0 | 172.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 542C0229 | PIPE CULVERTS, CLASS C, TYPE 1 24" | FOOT | 126.0 | 126.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 58700300 | CONCRETE SEALER | SQ FT | 14,479.0 | 0.0 | 3,977.0 | 3,223.0 | 3,525.0 | 3,754.0 | 0.0 |
| 59100100 | GEOCOMPOSITE WALL DRAIN | SQ YD | 392.0 | 0.0 | 99.0 | 88.0 | 100.0 | 105.0 | 0.0 |
| 60108100 | PIPE UNDERDRAINS 4" (SPECIAL) | FOOT | 283 | 283 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 60261546 | INLETS TO BE ADJUSTED WITH NEW STANDARD 542546 FRAME AND GRATE | EACH | 1.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 60500060 | REMOVING INLETS | EACH | 3.0 | 3.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 60500305 | FILLING INLETS | EACH | 1.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 60600605 | CONCRETE CURB, TYPE B | FOOT | 40.0 | 40.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 60900515 | CONCRETE THRUST BLOCKS | EACH | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • 63000001 | STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS | FOOT | 7,612.5 | 7,612.5 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • DENOTES SPECIALTY ITEM | | | | | | | | | |

14

LOCATION OF WORK: **FAI 74** S.N. 057-0251 S.N. 057-0252 S.N. 057-0253 S.N. 057-0254

URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE

OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL

STA. 29+00 TO STA. 65+00 STA. 57+19.05 TO STA. 58+99.57 STA. 57+12.20 TO STA. 58+97.86 STA. 51+55.69 TO STA. 53+56.48 STA. 51+14.85 TO STA. 53+23.26

MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY

FUNDING BREAKOUT: 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE

CONSTRUCTION TYPE CODE: 0004 0011 0011 0011 0011

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | | | | | |
|--------------------------|----------------------------------------------------|--------|----------------|---------|-----|-----|-----|-----|-----|
| • 63100045 | TRAFFIC BARRIER TERMINAL, TYPE 2 | EACH | 7.0 | 7.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • 63100070 | TRAFFIC BARRIER TERMINAL, TYPE 5 | EACH | 6.0 | 6.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • 63100085 | TRAFFIC BARRIER TERMINAL, TYPE 6 | EACH | 8.0 | 8.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • 63100089 | TRAFFIC BARRIER TERMINAL, TYPE 6B | EACH | 12.0 | 12.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • 63100167 | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT | EACH | 13.0 | 13.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 63200310 | GUARDRAIL REMOVAL | FOOT | 7,460.0 | 7,460.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 63500105 | DELINEATORS | EACH | 151.0 | 151.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 64200116 | SHOULDER RUMBLE STRIPS, 16 INCH | FOOT | 8,370.0 | 8,370.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 66201120 | CONCRETE SHOULDER CURB | FOOT | 20.0 | 20.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 66900200 | NON-SPECIAL WASTE DISPOSAL | CU YD | 3,075 | 811 | 417 | 417 | 715 | 715 | 715 |
| * 66900450 | SPECIAL WASTE PLANS AND REPORTS | L SUM | 1.0 | 0.2 | 0.2 | 0.2 | 0.2 | 0.2 | 0.2 |
| * 66900530 | SOIL DISPOSAL ANALYSIS | EACH | 3 | 1 | 0.5 | 0.5 | 0.5 | 0.5 | 0.5 |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 15.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 | 3.0 |
| 67100100 | MOBILIZATION | L SUM | 1.0 | 0.2 | 0.2 | 0.2 | 0.2 | 0.2 | 0.2 |
| • DENOTES SPECIALTY ITEM | | | | | | | | | |

14

LOCATION OF WORK: **FAI 74** S.N. 057-0251 S.N. 057-0252 S.N. 057-0253 S.N. 057-0254

URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE

OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL

STA. 29+00 TO STA. 65+00 STA. 57+19.05 TO STA. 58+99.57 STA. 57+12.20 TO STA. 58+97.86 STA. 51+55.69 TO STA. 53+56.48 STA. 51+14.85 TO STA. 53+23.26

MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY

FUNDING BREAKOUT: 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE

CONSTRUCTION TYPE CODE: 0004 0011 0011 0011 0011

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | | | | | |
|--------------------------|------------------------------------------------------------------------|-------|----------------|----------|-----|-----|-----|-----|-----|
| * 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 3,200.0 | 3,200.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| * 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 22,400.0 | 22,400.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| * 70300250 | TEMPORARY PAVEMENT MARKING - LINE 8" | FOOT | 2,990.0 | 2,990.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| * 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 9,800.0 | 9,800.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 70400100 | TEMPORARY CONCRETE BARRIER | FOOT | 3,890.0 | 3,890.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 70400200 | RELOCATE TEMPORARY CONCRETE BARRIER | FOOT | 5,810.0 | 5,810.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 70600250 | IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3 | EACH | 4.0 | 4.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 70600251 | IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3 | EACH | 2.0 | 2.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 70600350 | IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3 | EACH | 12.0 | 12.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 70600352 | IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE, NARROW), TEST LEVEL 3 | EACH | 2.0 | 2.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| * 72000300 | SIGN PANEL - TYPE 3 | SQ FT | 646.0 | 646.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 72400330 | REMOVE SIGN PANEL - TYPE 3 | SQ FT | 650.0 | 650.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| * 73304000 | OVERHEAD SIGN STRUCTURE - BRIDGE MOUNTED | FOOT | 60.5 | 60.5 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 73600100 | REMOVE OVERHEAD SIGN STRUCTURE - SPAN | EACH | 1.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| * DENOTES SPECIALTY ITEM | | | | | | | | | |

14

| | | | | | |
|-------------------------|--------------------------|--------------------------------|--------------------------------|--------------------------------|--------------------------------|
| LOCATION OF WORK: | <i>FAI 74</i> | S.N. 057-0251 | S.N. 057-0252 | S.N. 057-0253 | S.N. 057-0254 |
| | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE |
| | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL |
| | STA. 29+00 TO STA. 65+00 | STA. 57+19.05 TO STA. 58+99.57 | STA. 57+12.20 TO STA. 58+97.86 | STA. 51+55.69 TO STA. 53+56.48 | STA. 51+14.85 TO STA. 53+23.26 |
| | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY |
| FUNDING BREAKOUT: | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE |
| CONSTRUCTION TYPE CODE: | 0004 | 0011 | 0011 | 0011 | 0011 |

| CODE NO. | ITEM | UNITS | TOTAL QUANTITY | | | | | | |
|--------------------------|------------------------------------------------------|-------|----------------|----------|-----|-----|-----|-----|-----|
| 73600200 | REMOVE OVERHEAD SIGN STRUCTURE - CANTILEVER | EACH | 1.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 73700300 | REMOVE CONCRETE FOUNDATION - OVERHEAD | EACH | 5.0 | 5.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • 78003110 | PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 4" | FOOT | 1,490.0 | 1,490.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • 78009004 | MODIFIED URETHANE PAVEMENT MARKING - LINE 4" | FOOT | 23,650.0 | 23,650.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • 78009008 | MODIFIED URETHANE PAVEMENT MARKING - LINE 8" | FOOT | 2,990.0 | 2,990.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 270.0 | 270.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • 78100105 | RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) | EACH | 30.0 | 0.0 | 6.0 | 8.0 | 8.0 | 8.0 | 8.0 |
| • 78200410 | GUARDRAIL MARKERS, TYPE A | EACH | 105.0 | 105.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • 78200530 | BARRIER WALL MARKERS, TYPE C | EACH | 15.0 | 0.0 | 4.0 | 3.0 | 3.0 | 3.0 | 5.0 |
| • 78201000 | TERMINAL MARKER - DIRECT APPLIED | EACH | 13.0 | 13.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 78300100 | PAVEMENT MARKING REMOVAL | SQ FT | 3,150.0 | 3,150.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 270.0 | 270.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • 80400100 | ELECTRIC SERVICE INSTALLATION | EACH | 1.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • 81028200 | UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA. | FOOT | 10.0 | 10.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • DENOTES SPECIALTY ITEM | | | | | | | | | |

| | | | | | | | | | | | | | |
|-------------------------------------------|-----------------------------|------------|-----------|-----------------------------------------------------------|------------------------------------------|--|--|--|--------------------|-------------------|--------|-----------------|--------------|
| FILE NAME : | USER NAME : detarsbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES | | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pvc\work\pvidot\detarsbj\d0197116\0578 | 78-ant-500.dgn | DRAWN - | REVISED - | | | | | | 704 | 57-201HB,HB-1BR-1 | MCLEAN | 440 | 14 |
| #MODELNAME# | PLOT SCALE = 48.0000' / in. | CHECKED - | REVISED - | | SCALE: SHEET 9 OF 13 SHEETS STA. TO STA. | | | | CONTRACT NO. 70570 | | | | |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | | |

14

LOCATION OF WORK: *FAI 74* S.N. 057-0251 S.N. 057-0252 S.N. 057-0253 S.N. 057-0254
 URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE URBAN MULTILANE
 OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL OTHER PRINCIPAL ARTERIAL
 STA. 29+00 TO STA. 65+00 STA. 57+19.05 TO STA. 58+99.57 STA. 57+12.20 TO STA. 58+97.86 STA. 51+55.69 TO STA. 53+56.48 STA. 51+14.85 TO STA. 53+23.26
 MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY MCLEAN COUNTY
 FUNDING BREAKOUT: 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE 90% FEDERAL, 10% STATE
 CONSTRUCTION TYPE CODE: 0004 0011 0011 0011 0011

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | | | | | |
|--------------------------|------------------------------------------------------------|--------|----------------|-------|-------|-------|-------|-------|-----|
| X0323583 | SPEED INDICATOR SIGN | CAL DA | 612.0 | 612.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| X5860110 | GRANULAR BACKFILL FOR STRUCTURES | CU YD | 630.0 | 0.0 | 161.0 | 142.0 | 163.0 | 164.0 | |
| X6015015 | REMOVE AND REPLACE CONCRETE HEADWALLS FOR PIPE UNDERDRAINS | EACH | 33 | 33 | 0.0 | 0.0 | 0.0 | 0.0 | |
| X6100120 | TYPE E INLET BOX, STANDARD 610001 (SPECIAL) | EACH | 1.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| X6100230 | TYPE F INLET BOX, STANDARD 610001 (SPECIAL) | EACH | 1.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| X6330200 | VERTICAL ADJUSTMENT OF TRAFFIC BARRIER TERMINAL | EACH | 1.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| X6330900 | VERTICAL ADJUSTMENT OF GUARDRAIL | FOOT | 150.0 | 150.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| X6350120 | DELINEATOR REMOVAL | EACH | 221.0 | 221.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| X7010216 | TRAFFIC CONTROL AND PROTECTION, (SPECIAL) | L SUM | 1.0 | 0.2 | 0.2 | 0.2 | 0.2 | 0.2 | |
| X7015005 | CHANGEABLE MESSAGE SIGN | CAL DA | 91.0 | 91.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| • DENOTES SPECIALTY ITEM | | | | | | | | | |

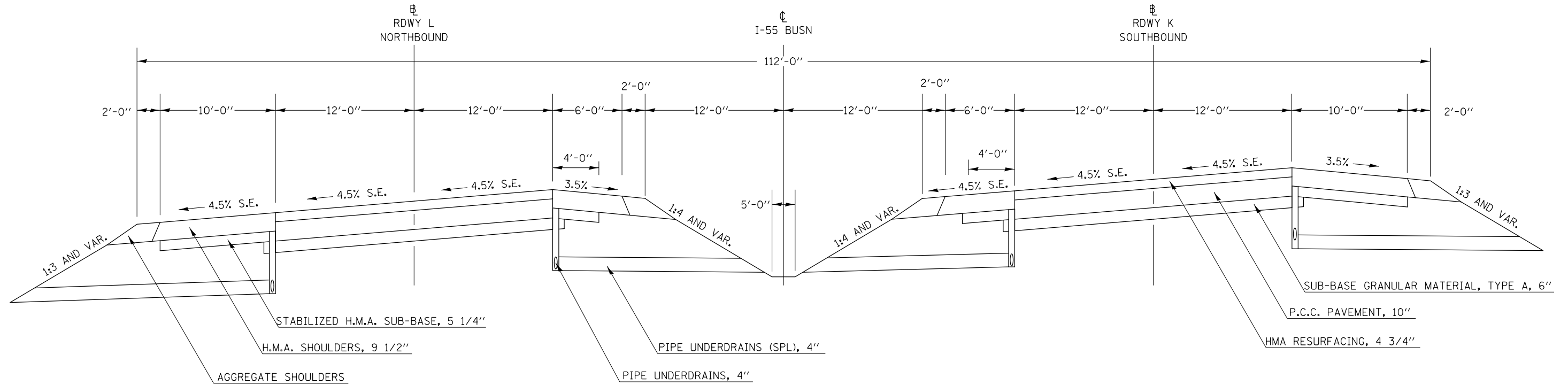
14

| | | | | | |
|-------------------------|--------------------------|--------------------------------|--------------------------------|--------------------------------|--------------------------------|
| LOCATION OF WORK: | FAI 74 | S.N. 057-0251 | S.N. 057-0252 | S.N. 057-0253 | S.N. 057-0254 |
| | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE | URBAN MULTILANE |
| | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL | OTHER PRINCIPAL ARTERIAL |
| | STA. 29+00 TO STA. 65+00 | STA. 57+19.05 TO STA. 58+99.57 | STA. 57+12.20 TO STA. 58+97.86 | STA. 51+55.69 TO STA. 53+56.48 | STA. 51+14.85 TO STA. 53+23.26 |
| | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY | MCLEAN COUNTY |
| FUNDING BREAKOUT: | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE | 90% FEDERAL, 10% STATE |
| CONSTRUCTION TYPE CODE: | 0004 | 0011 | 0011 | 0011 | 0011 |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | | | | | | |
|--------------------------|--------------------------------------------------------|-------|----------------|----------|---------|---------|---------|---------|---------|
| * X7200201 | WIDTH RESTRICTION SIGNING | L SUM | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 |
| • X7830070 | GROOVING FOR RECESSED PAVEMENT MARKING 5" | FOOT | 23,650.0 | 23,650.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • X7830076 | GROOVING FOR RECESSED PAVEMENT MARKING 9" | FOOT | 2,990.0 | 2,990.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • X8110522 | CONDUIT ATTACHED TO STRUCTURE, 2" DIA. STAINLESS STEEL | FOOT | 50.0 | 50.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| • X8410102 | TEMPORARY LIGHTING SYSTEM | L SUM | 1.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| XZ193300 | SURVEY MARKER, TYPE 1 (SPECIAL) | EACH | 1.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| XZ193400 | SURVEY MARKER, TYPE 2 (SPECIAL) | EACH | 1.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Z0002900 | BASE COURSE (OPTION) | SQ YD | 2,500.0 | 2,500.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Z0004638 | PAVEMENT BREAKING | SQ YD | 2,400.0 | 2,400.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Z0013798 | CONSTRUCTION LAYOUT | L SUM | 1.0 | 0.2 | 0.2 | 0.2 | 0.2 | 0.2 | 0.2 |
| Z0016702 | DETOUR SIGNING | L SUM | 1.0 | 0.2 | 0.2 | 0.2 | 0.2 | 0.2 | 0.2 |
| Z0018002 | DRAINAGE SCUPPERS, DS-11 | EACH | 2.0 | 0.0 | 2.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Z0029090 | DIAMOND GRINDING (BRIDGE SECTION) | SQ YD | 5,458.0 | 0.0 | 1,490.0 | 1,249.0 | 1,293.0 | 1,426.0 | 1,426.0 |
| Z0038700 | PERMANENT BENCH MARKS | EACH | 4.0 | 0.0 | 1.0 | 1.0 | 1.0 | 1.0 | 1.0 |
| • DENOTES SPECIALTY ITEM | | | | | | | | | |

EXISTING TYPICAL CROSS SECTION

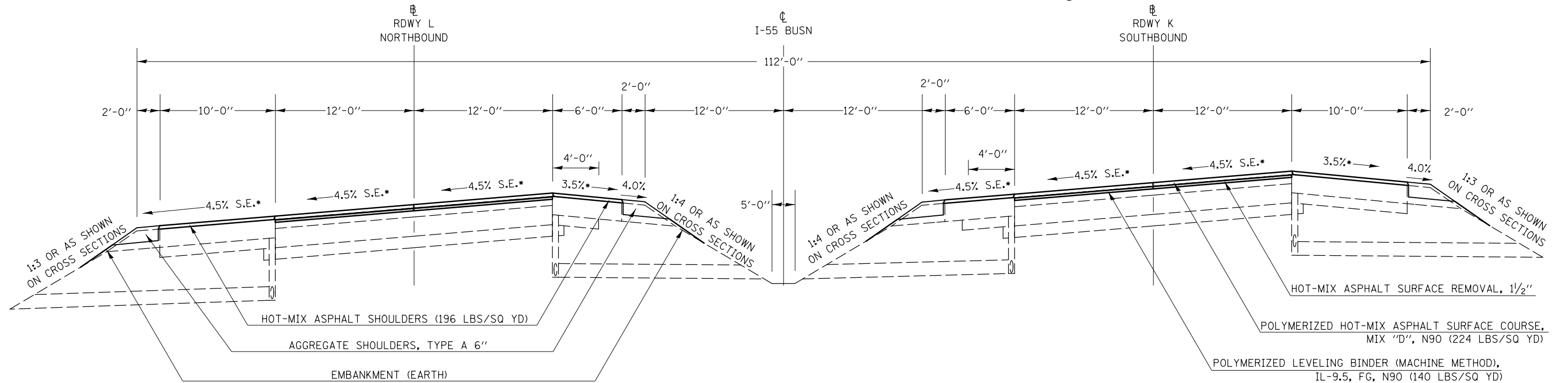
STATION TO STATION
 29+00.00 33+75.00 (ROADWAY K VETERANS PKWY SB)
 32+60.00 33+75.00 (ROADWAY L VETERANS PKWY NB)



* MATCH EXISTING PAVEMENT SLOPES

PROPOSED TYPICAL CROSS SECTION ①

STATION TO STATION
 29+00.00 33+75.00 (ROADWAY K VETERANS PKWY SB) ②
 32+60.00 33+75.00 (ROADWAY L VETERANS PKWY NB) ②



| | | | |
|--------------------------------------------------------------|----------------------|------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
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| PLOT DATE = 8/13/2013 | | DATE - | REVISED - |

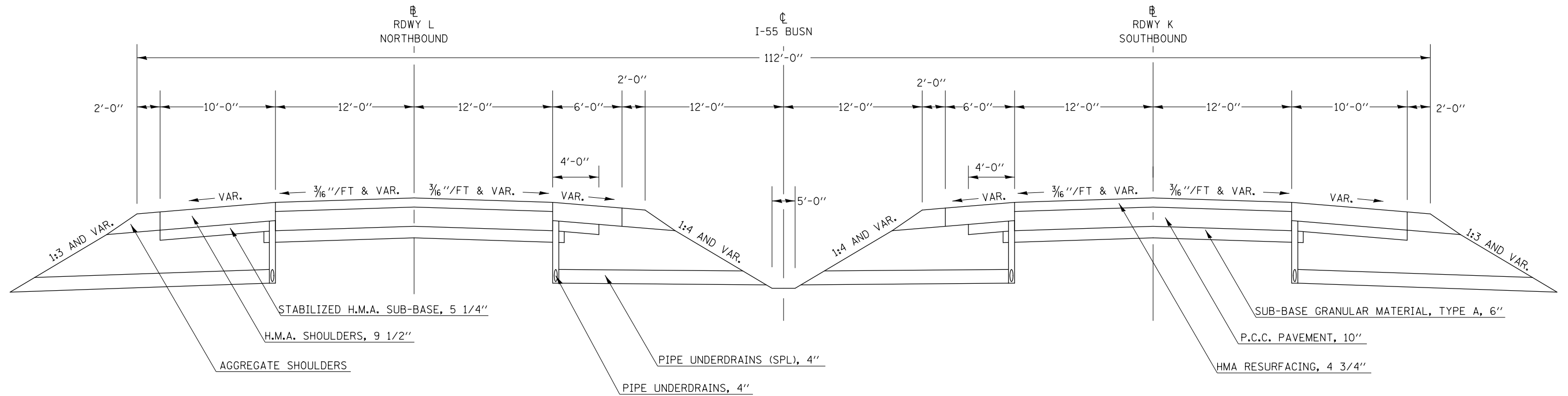
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

| | |
|------------------|--------------------------|
| TYPICAL SECTIONS | |
| SCALE: | SHEET NO. 1 OF 14 SHEETS |
| STA. | TO STA. |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 19 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

EXISTING TYPICAL CROSS SECTION

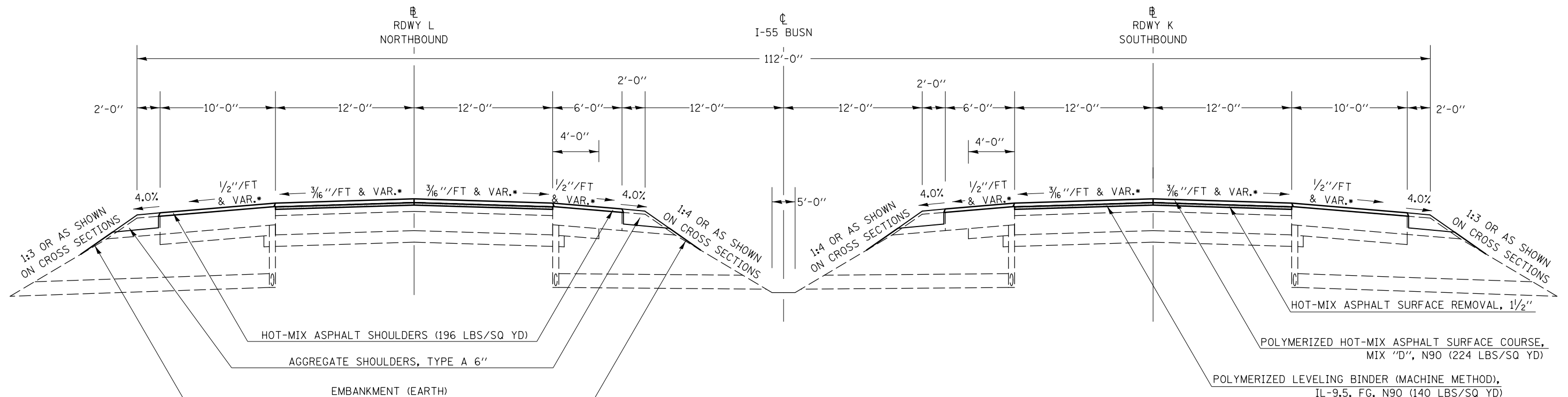
STATION TO STATION
 33+75.00 36+25.00 (ROADWAY L & K VETERANS PKWY) S.E. TRANSITION
 36+25.00 36+75.00 (ROADWAY L & K VETERANS PKWY)
 36+75.00 39+25.00 (ROADWAY L & K VETERANS PKWY) S.E. TRANSITION



* MATCH EXISTING PAVEMENT SLOPES

PROPOSED TYPICAL CROSS SECTION ②

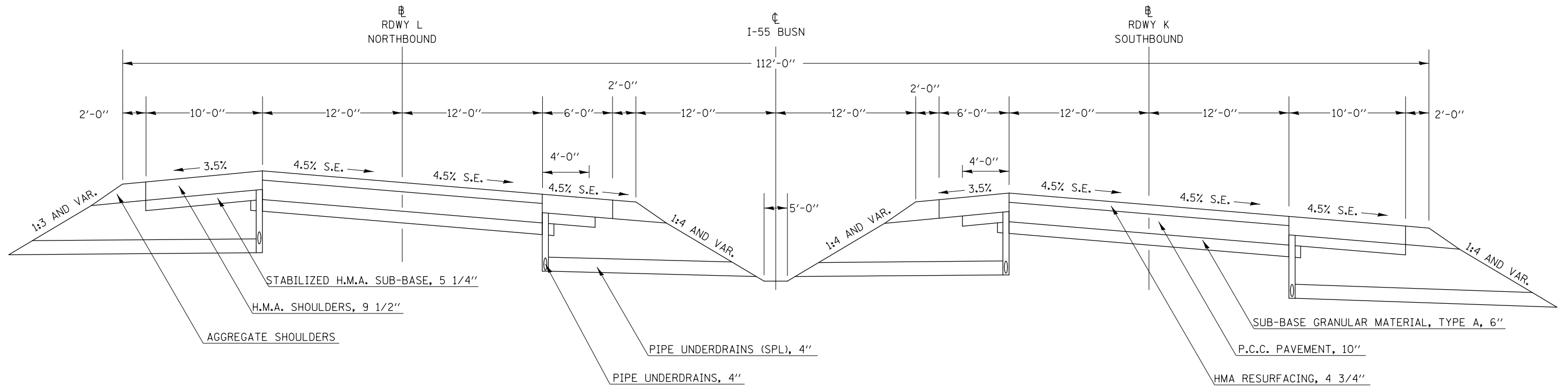
STATION TO STATION
 ① 33+75.00 36+25.00 (ROADWAY L & K VETERANS PKWY) S.E. TRANSITION
 36+25.00 36+75.00 (ROADWAY L & K VETERANS PKWY)
 36+75.00 39+25.00 (ROADWAY L & K VETERANS PKWY) S.E. TRANSITION ③



| | | | | | | | | | | | | |
|-------------------------------------------------------------|----------------------|------------|-----------|-----------------------------------------------------------|----------------------------------------------|--------------------|--------|--------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - | | SCALE: SHEET NO. 2 OF 14 SHEETS STA. TO STA. | | | CONTRACT NO. 70570 | | | | |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |

EXISTING TYPICAL CROSS SECTION

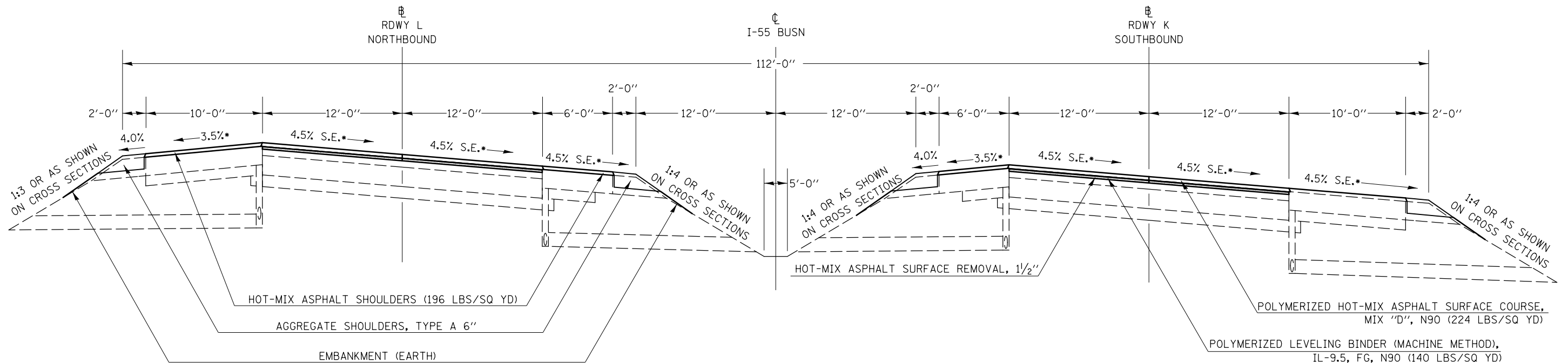
STATION TO STATION
39+25.00 42+20.00 (ROADWAY L & K VETERANS PKWY)



* MATCH EXISTING PAVEMENT SLOPES

PROPOSED TYPICAL CROSS SECTION ③

STATION TO STATION
② 39+25.00 42+20.00 (ROADWAY L & K VETERANS PKWY) ④



| | | | |
|-------------------------------------------------------------|----------------------|------------|-----------|
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| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

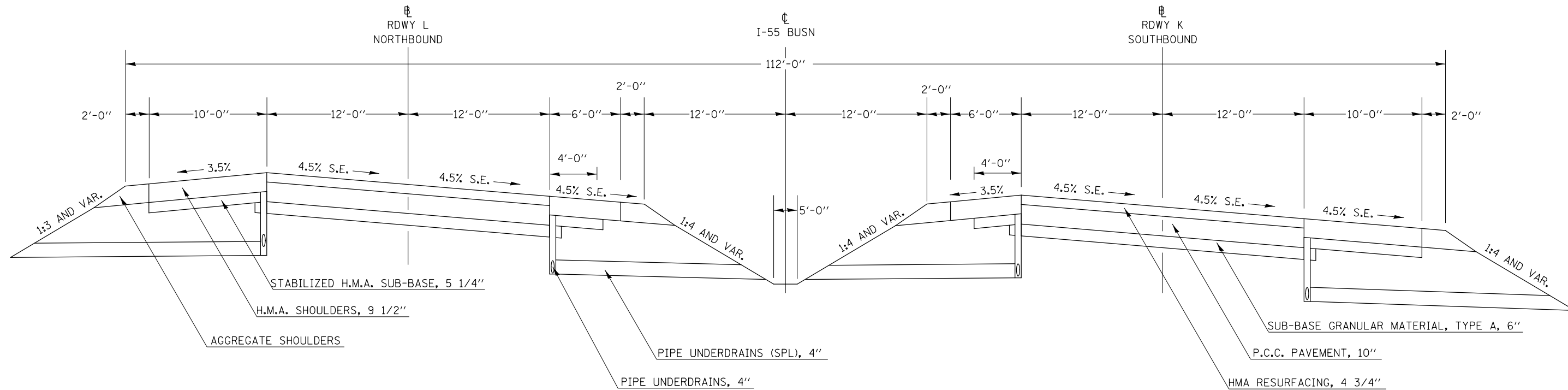
TYPICAL SECTIONS

SCALE: SHEET NO. 3 OF 14 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 21 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

EXISTING TYPICAL CROSS SECTION

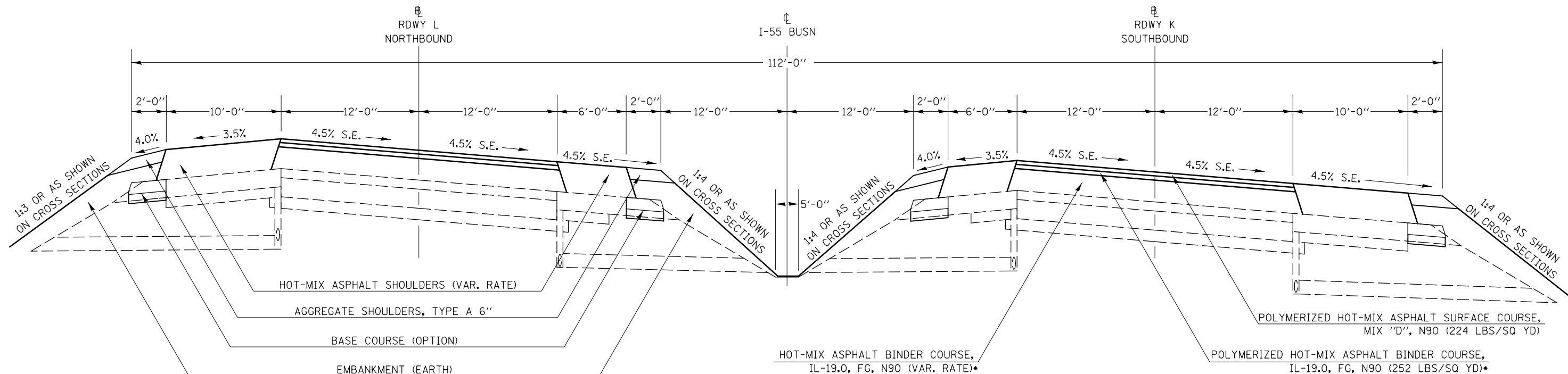
STATION TO STATION
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 42+20.00 51+90.34 (ROADWAY L VETERANS PKWY NB)
 53+39.73 56+50.00 (ROADWAY L VETERANS PKWY NB)



*STA. 42+20.00 TO STA. 43+62 (RDWY L VET PKWY NB)
 STA. 42+20.00 TO STA. 44+07 (RDWY K VET PKWY SB)
 MIX SHALL BE: POLYMERIZED LEVELING BINDER (MACHINE METHOD),
 IL-9.5, FG, N90 (140 LBS/SQ YD TO 252 LBS/SQ YD)

PROPOSED TYPICAL CROSS SECTION ④

STATION TO STATION
 ③ 42+20.00 50+90.00 (ROADWAY K VETERANS PKWY SB) ⑤
 ③ 42+20.00 51+55.69 (ROADWAY L VETERANS PKWY NB) ⑥
 ⑥ 53+56.48 56+50.00 (ROADWAY L VETERANS PKWY NB) ⑦



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| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

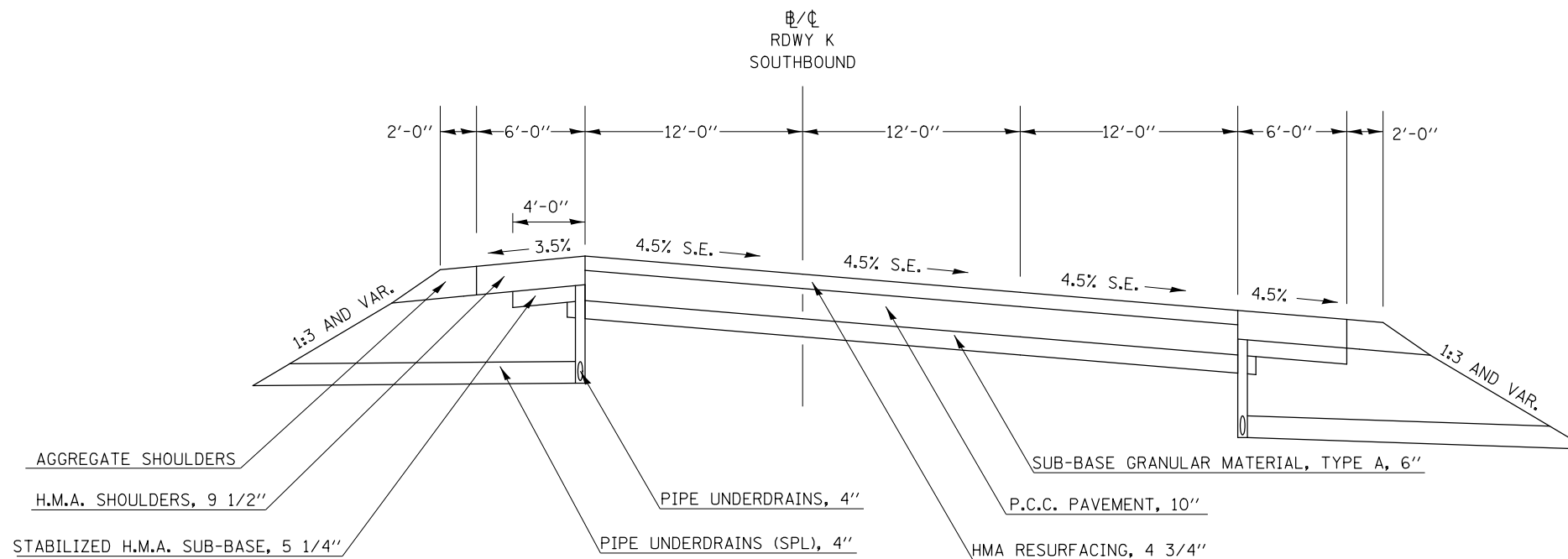
TYPICAL SECTIONS

SCALE: SHEET NO. 4 OF 14 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 22 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

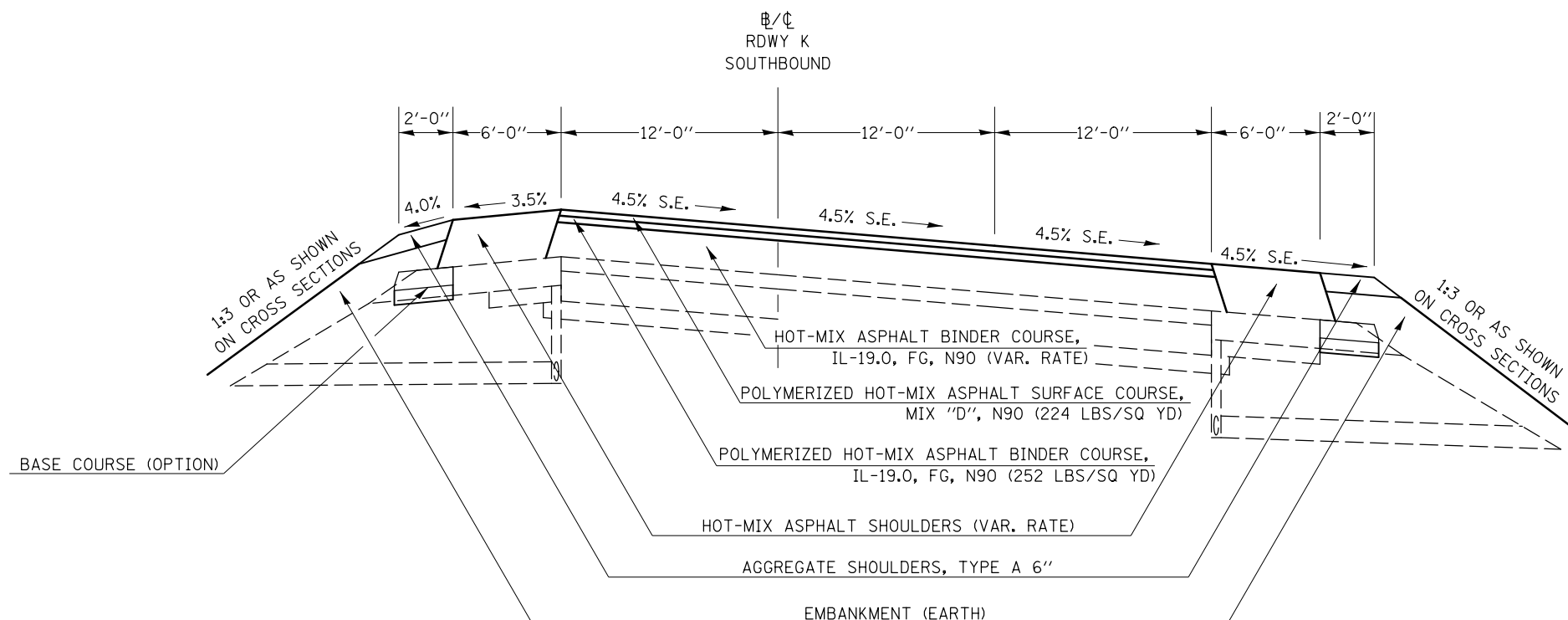
EXISTING TYPICAL CROSS SECTION

STATION TO STATION
 50+90.00 51+48.41 (ROADWAY K VETERANS PKWY SB)
 53+03.64 57+30.63 (ROADWAY K VETERANS PKWY SB)
 58+79.47 59+96.00 (ROADWAY K VETERANS PKWY SB)



PROPOSED TYPICAL CROSS SECTION ⑤

STATION TO STATION
 ④ 50+90.00 51+14.85 (ROADWAY K VETERANS PKWY SB) ⑥
 ⑥ 53+23.26 57+12.20 (ROADWAY K VETERANS PKWY SB) ⑧
 ⑧ 58+97.86 59+96.00 (ROADWAY K VETERANS PKWY SB) ⑨



| | | | |
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| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

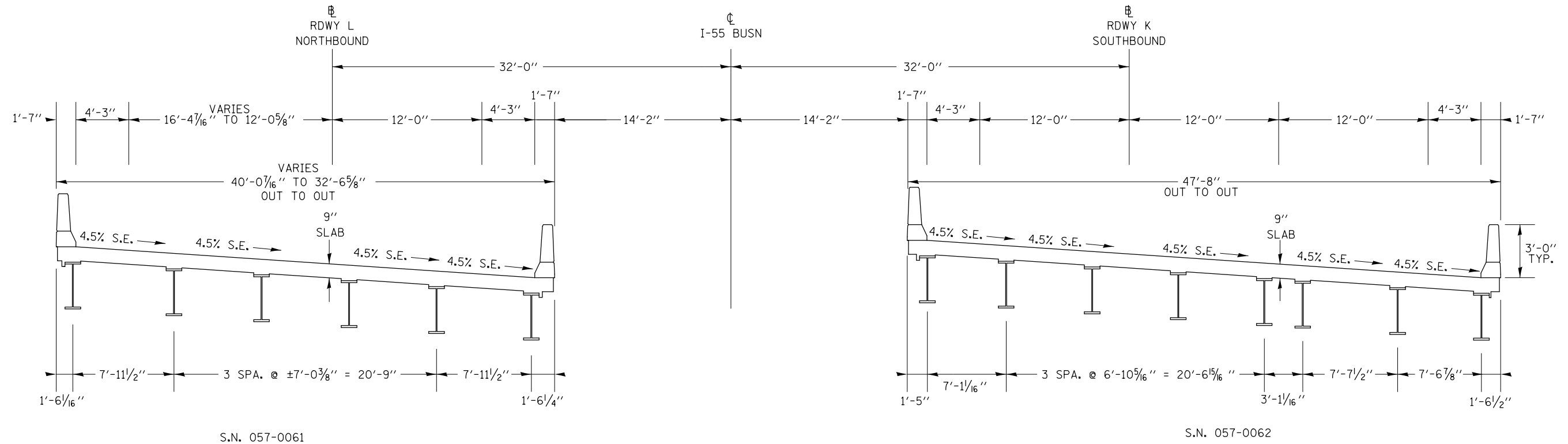
TYPICAL SECTIONS

SCALE: SHEET NO. 5 OF 14 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 23 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

EXISTING TYPICAL CROSS SECTION

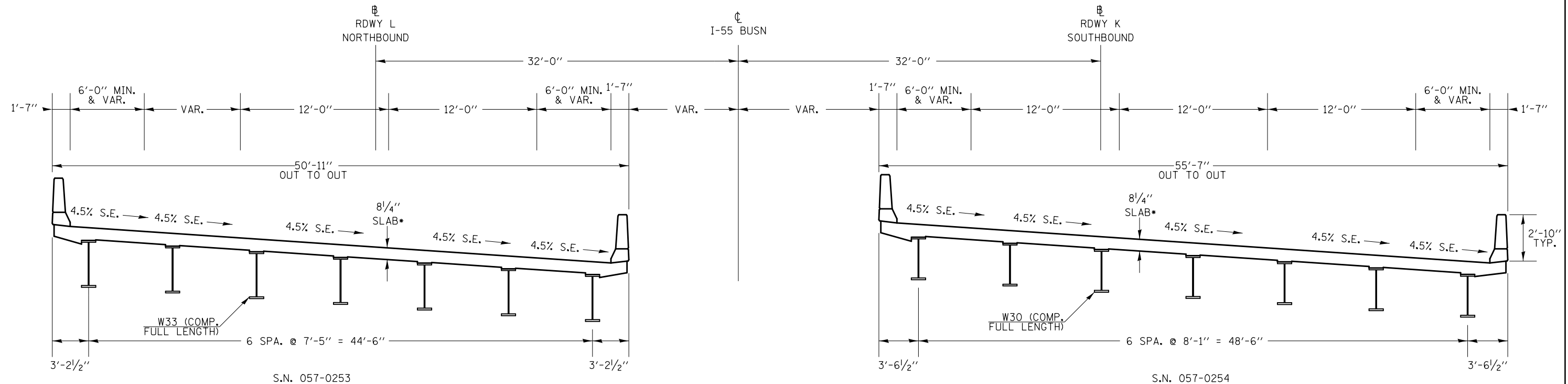
STATION TO STATION
 51+48.41 53+03.64 (ROADWAY K VETERANS PKWY SB)
 51+90.34 53+39.73 (ROADWAY L VETERANS PKWY NB)



*PRIOR TO DIAMOND GRINDING

PROPOSED TYPICAL CROSS SECTION ⑥

STATION TO STATION
 ⑤ 51+14.85 53+23.26 (ROADWAY K VETERANS PKWY SB) ⑤
 ④ 51+55.69 53+56.48 (ROADWAY L VETERANS PKWY NB) ④



| | | | |
|-----------------------------------------------------------------|----------------------|------------|-----------|
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| c:\pwork\pwork\pwork\detersbj\d0157116\d570570-sht-typicals.dgn | | DRAWN - | REVISED - |
| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |

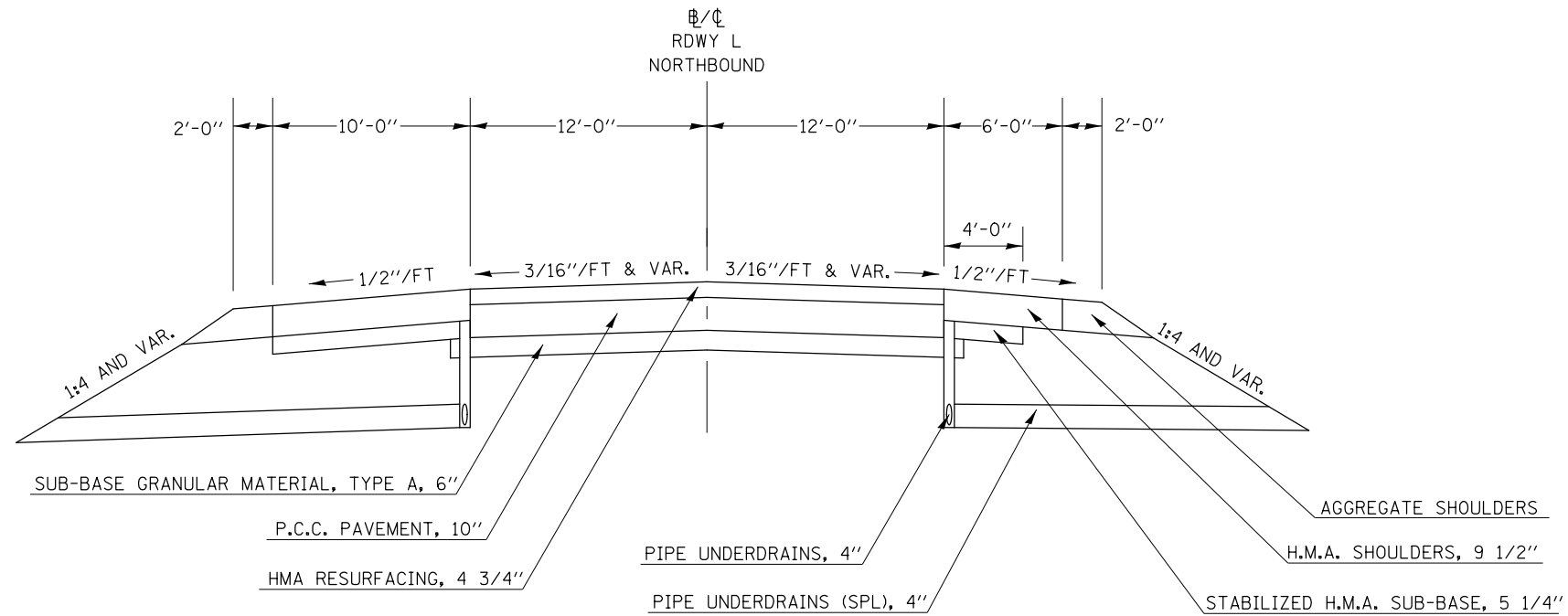
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | |
|-------------------------|--------------------------|
| TYPICAL SECTIONS | |
| SCALE: | SHEET NO. 6 OF 14 SHEETS |
| STA. | TO STA. |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 24 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

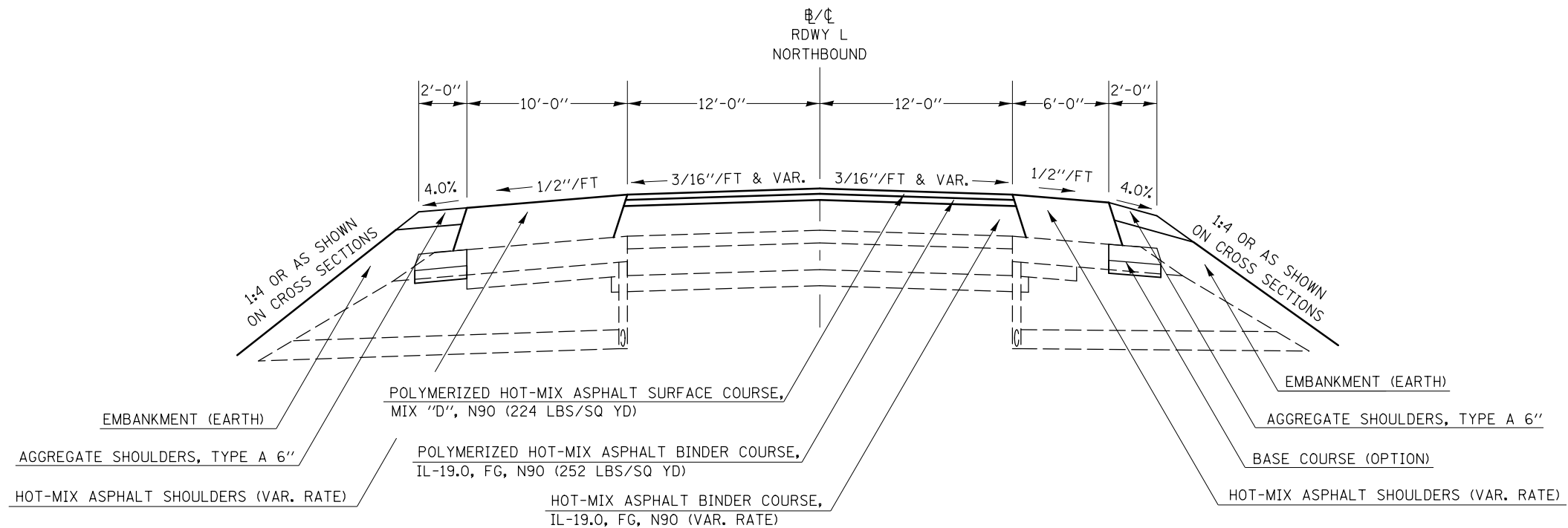
EXISTING TYPICAL CROSS SECTION

STATION TO STATION
 56+50.00 57+36.92 (ROADWAY L VETERANS PKWY NB) S.E. TRANSITION
 58+81.64 61+00.00 (ROADWAY L VETERANS PKWY NB) S.E. TRANSITION
 61+00.00 65+00.00 (ROADWAY L VETERANS PKWY NB)



PROPOSED TYPICAL CROSS SECTION ⑦

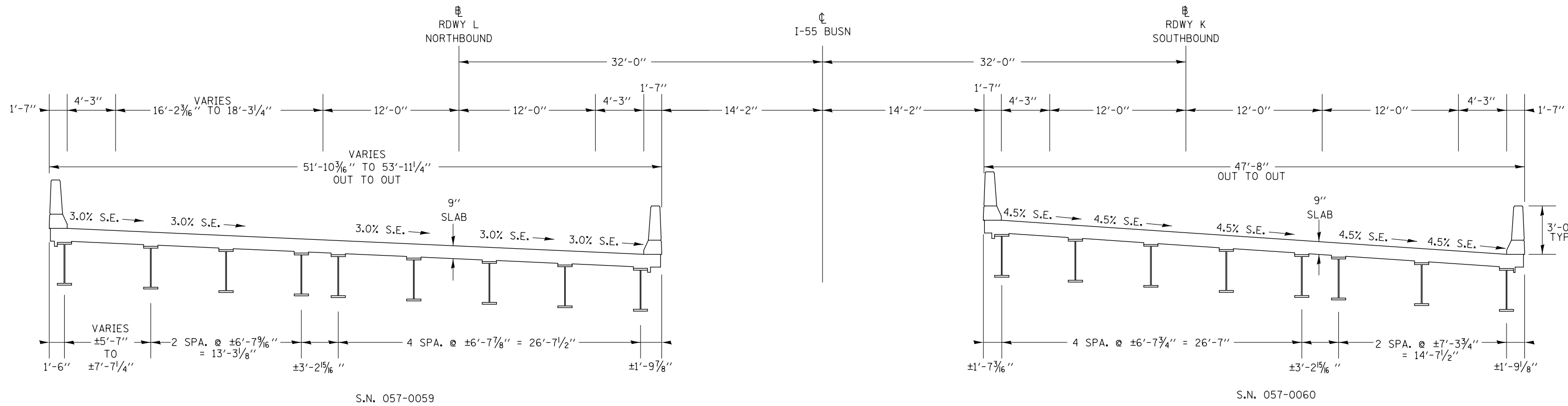
STATION TO STATION
 ④ 56+50.00 57+19.05 (ROADWAY L VETERANS PKWY NB) S.E. TRANSITION ⑧
 ⑧ 58+99.57 61+00.00 (ROADWAY L VETERANS PKWY NB) S.E. TRANSITION
 61+00.00 65+00.00 (ROADWAY L VETERANS PKWY NB)



| | | | | | | | | |
|--------------------------------------------------------------|----------------------|------------|-----------|-----------------------------------------------------------|-------------------------|--------------------|--------------------------|--------------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS | SCALE: | SHEET NO. 7 OF 14 SHEETS | STA. TO STA. |
| ci:\pw\work\p\dot\detersbj\d0157116\d570570-sht-typicals.dgn | | DRAWN - | REVISED - | | | | | |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - | | | | | |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - | | | | | |
| F.A.P. SECTION COUNTY TOTAL SHEETS SHEET NO. | | | | | | | | |
| 704 57-20(HB,HB-1)BR-1 MCLEAN 440 25 | | | | | | CONTRACT NO. 70570 | | |
| ILLINOIS FED. AID PROJECT | | | | | | | | |

EXISTING TYPICAL CROSS SECTION

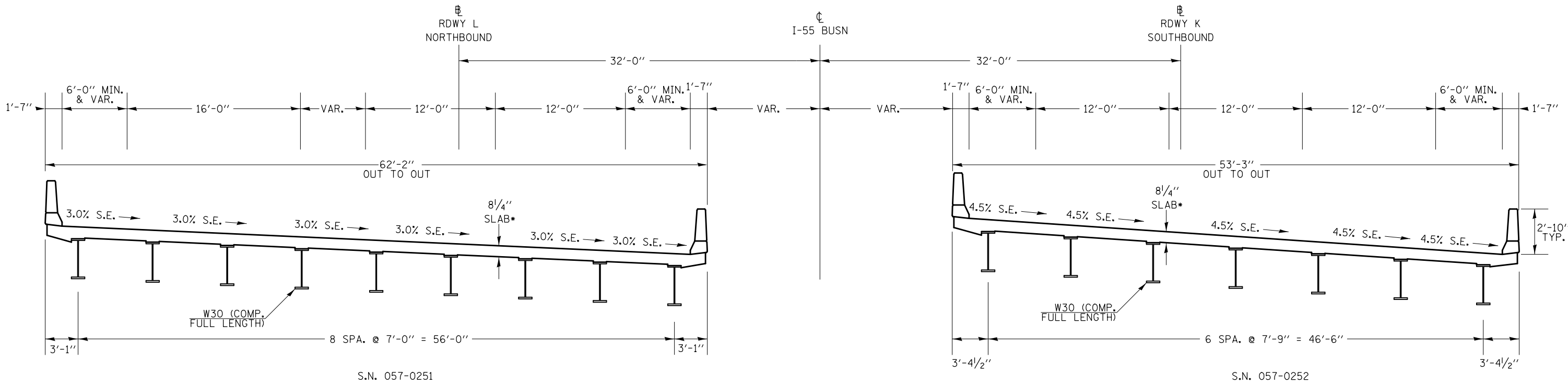
STATION TO STATION
 57+30.63 58+79.47 (ROADWAY K VETERANS PKWY SB)
 57+36.92 58+81.64 (ROADWAY L VETERANS PKWY NB)



*PRIOR TO DIAMOND GRINDING

PROPOSED TYPICAL CROSS SECTION 8

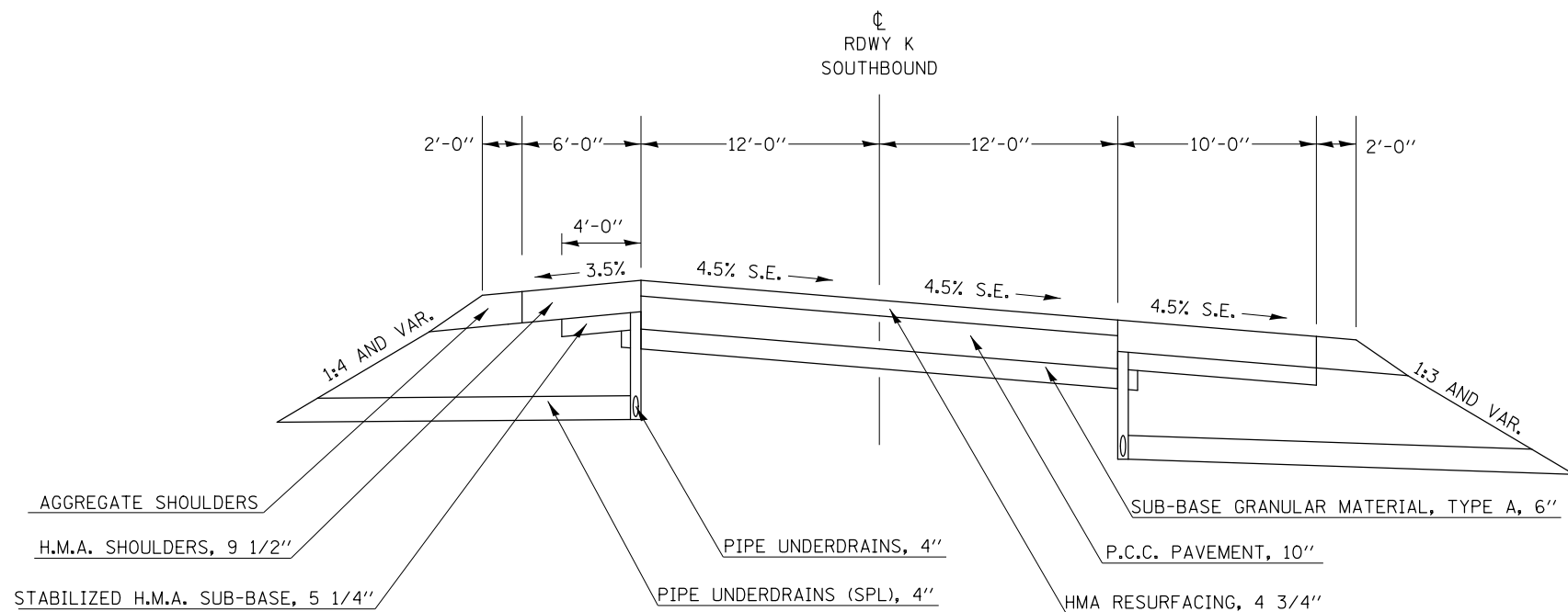
STATION TO STATION
 ⑤ 57+12.20 58+97.86 (ROADWAY K VETERANS PKWY SB) ⑤
 ⑦ 57+19.05 58+99.57 (ROADWAY L VETERANS PKWY NB) ⑦



| | | | | | | | | | | | | | |
|--------------------------------------------------------------|----------------------|------------|-----------|-----------------------------------------------------------|---------------------------|--------------------------|------|-------------|---------|--------------------|--------------|-----------|----|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ei:\pw\work\p1dot\detersbj\d0157116\d570570-sht-typicals.dgn | | DRAWN - | REVISED - | | SCALE: | SHEET NO. 8 OF 14 SHEETS | STA. | TO STA. | 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 26 |
| | | CHECKED - | REVISED - | | CONTRACT NO. 70570 | | | | | | | | |
| | | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | | |

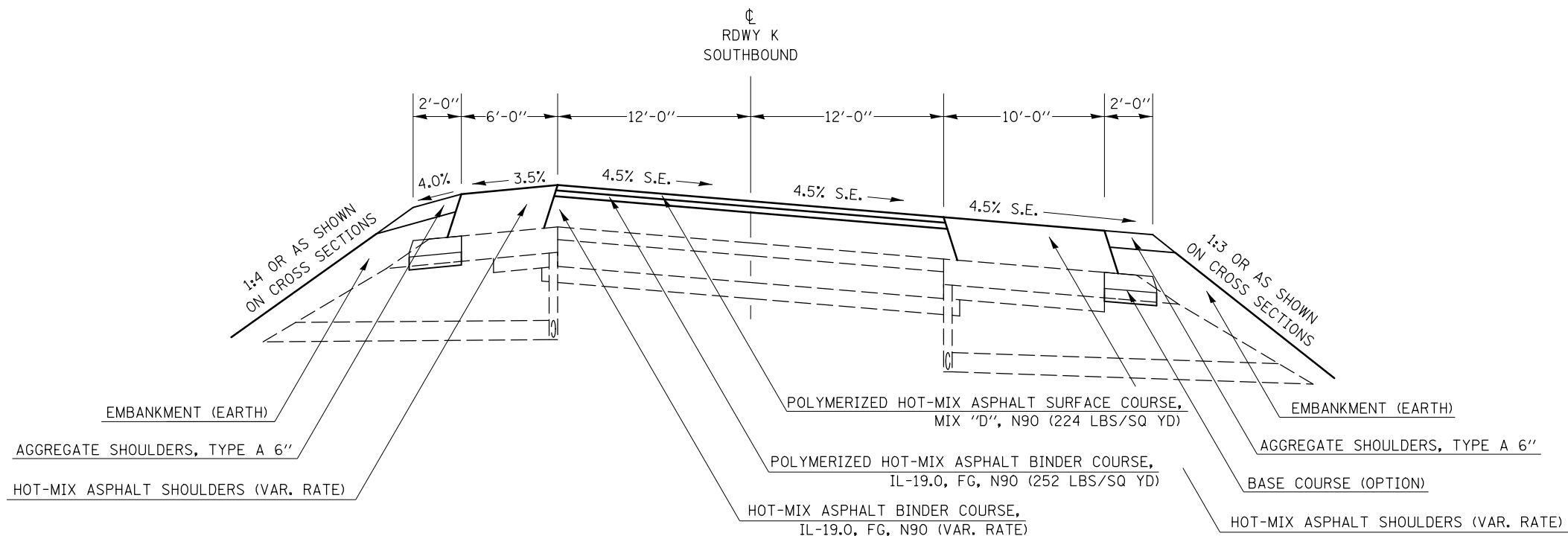
EXISTING TYPICAL CROSS SECTION

STATION TO STATION
59+96.00 65+00.00 (ROADWAY K VETERANS PKWY SB)



PROPOSED TYPICAL CROSS SECTION 9

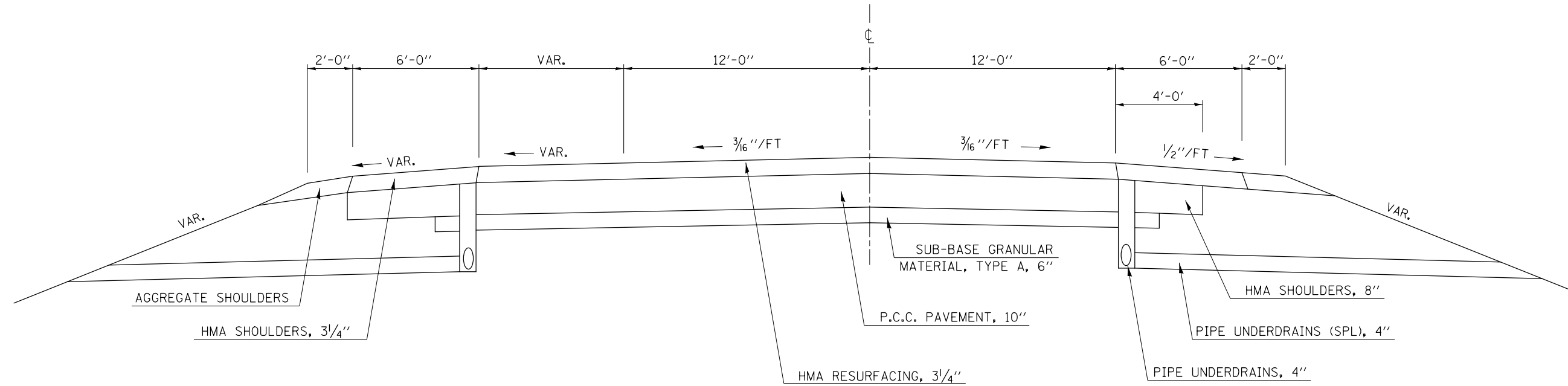
STATION TO STATION
⑤ 59+96.00 65+00.00 (ROADWAY K VETERANS PKWY SB)



| | | | | | | | | | | | | |
|---------------------------------------------------------------|----------------------|------------|-----------|-----------------------------------------------------------|-------------------------|--------------------------|--------|-------------|---------------------------|--------|--------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pw\work\p\dot\detersbj\d01571116\d570570-sht-typicals.dgn | | DRAWN - | REVISED - | | 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 27 | | | |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - | | CONTRACT NO. 70570 | | | | | | | |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - | | SCALE: | SHEET NO. 9 OF 14 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT | | | |

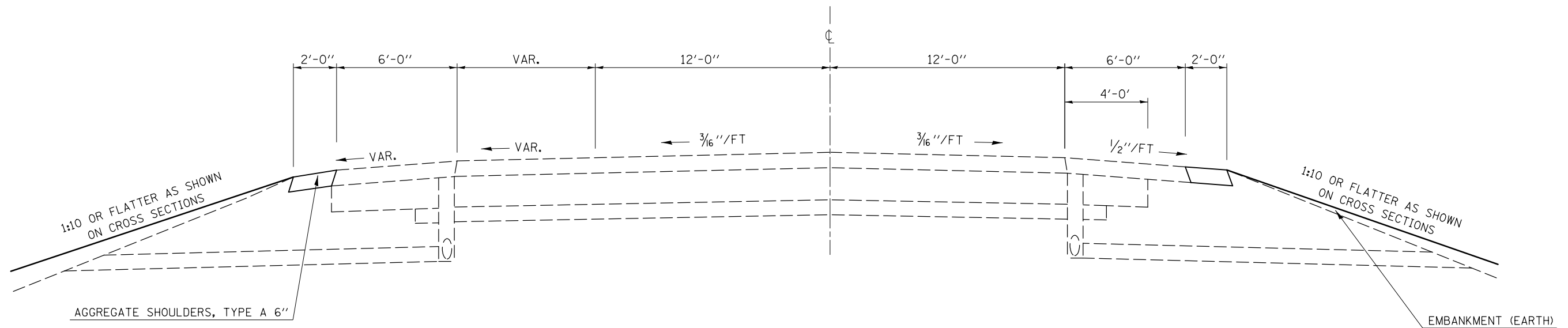
EXISTING TYPICAL CROSS SECTION

STATION TO STATION
634+00.00 638+00.00 (ROADWAY D I-74 WB)



PROPOSED TYPICAL CROSS SECTION 10

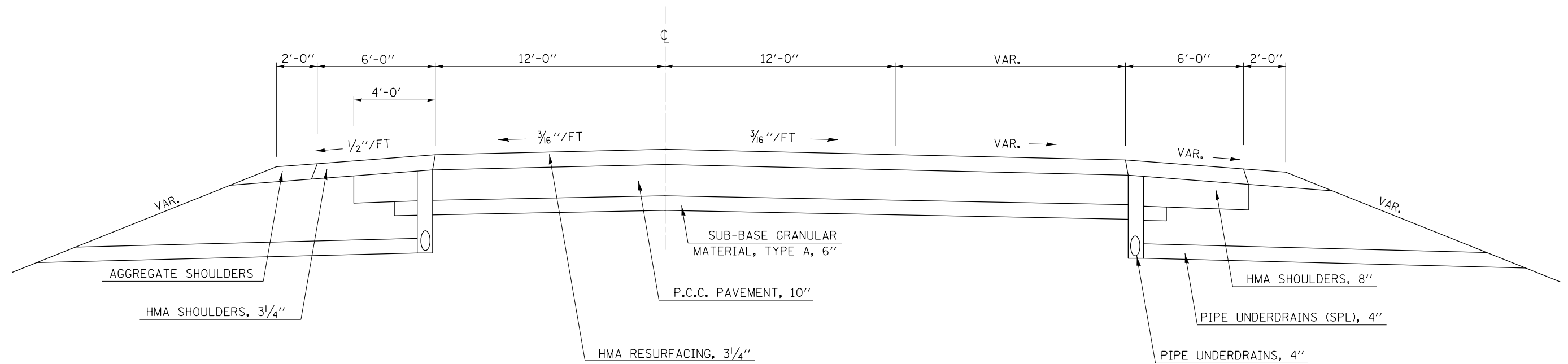
STATION TO STATION
634+00.00 638+00.00 (ROADWAY D I-74 WB)



| | | | | | | | | | | | | | |
|-------------|--------------------------------------------------------------|------------|-----------|-----------------------------------------------------------|---------------------------|---------------------------|------|-------------|---------|--------------------|--------------|-----------|----|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS | | | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| | et:\pw\work\p\dot\detersbj\d0157116\d570570-sht-typicals.dgn | DRAWN - | REVISED - | | SCALE: | SHEET NO. 10 OF 14 SHEETS | STA. | TO STA. | 74 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 28 |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | CONTRACT NO. 70570 | | | | | | | | |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | | |

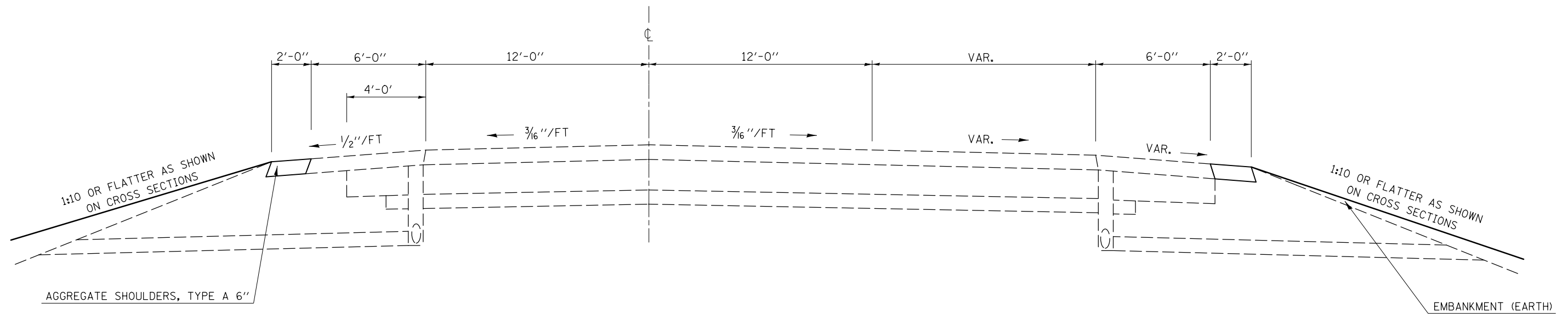
EXISTING TYPICAL CROSS SECTION

STATION TO STATION
635+50.00 639+50.00 (ROADWAY A I-74 EB)



PROPOSED TYPICAL CROSS SECTION 11

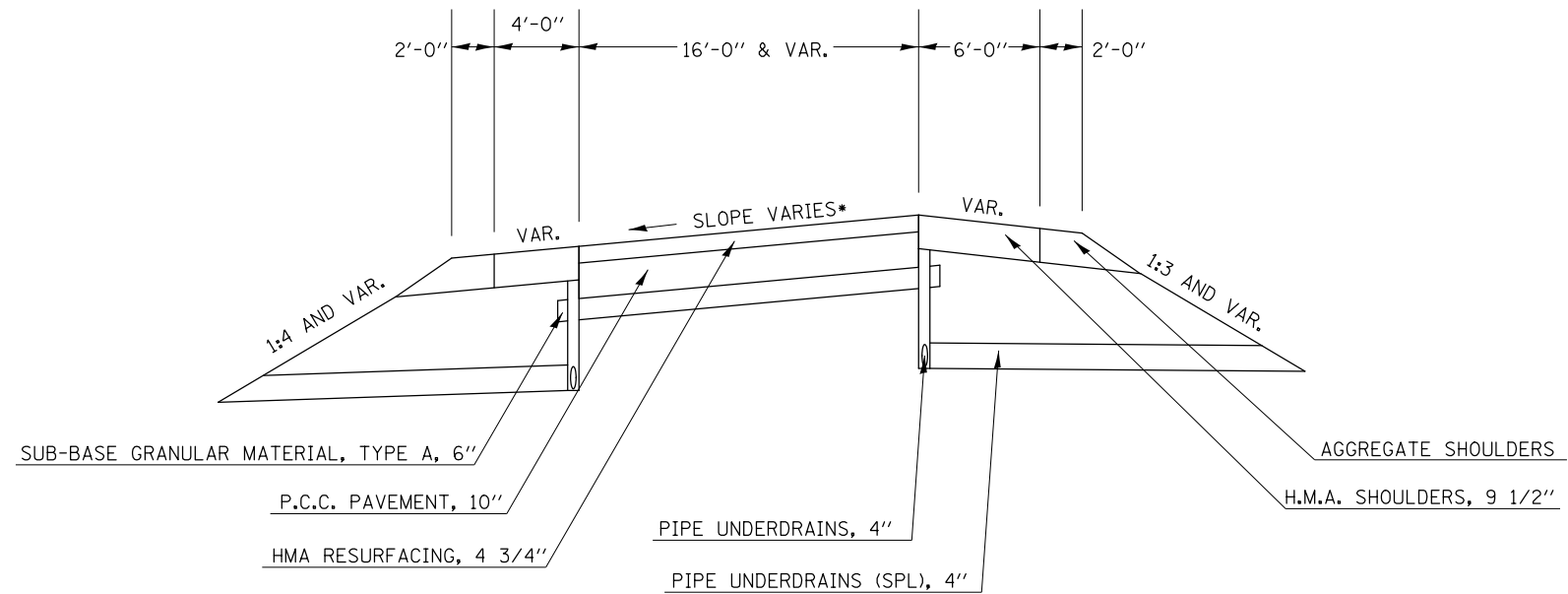
STATION TO STATION
635+50.00 639+50.00 (ROADWAY A I-74 EB)



| | | | | | | | | | | |
|------------------------------------------------------------|-----------------------------|------------|-----------|-----------------------------------------------------------|-------------------------|---------------------------|--------------------|---------|---------------------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ci:\pwork\pwork\detersbj\d0157116\d570570-sht-typicals.dgn | | DRAWN - | REVISED - | | | 74 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 29 |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | | CONTRACT NO. 70570 | | | | |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - | | SCALE: | SHEET NO. 11 OF 14 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT | |

EXISTING RAMP TYPICAL CROSS SECTION

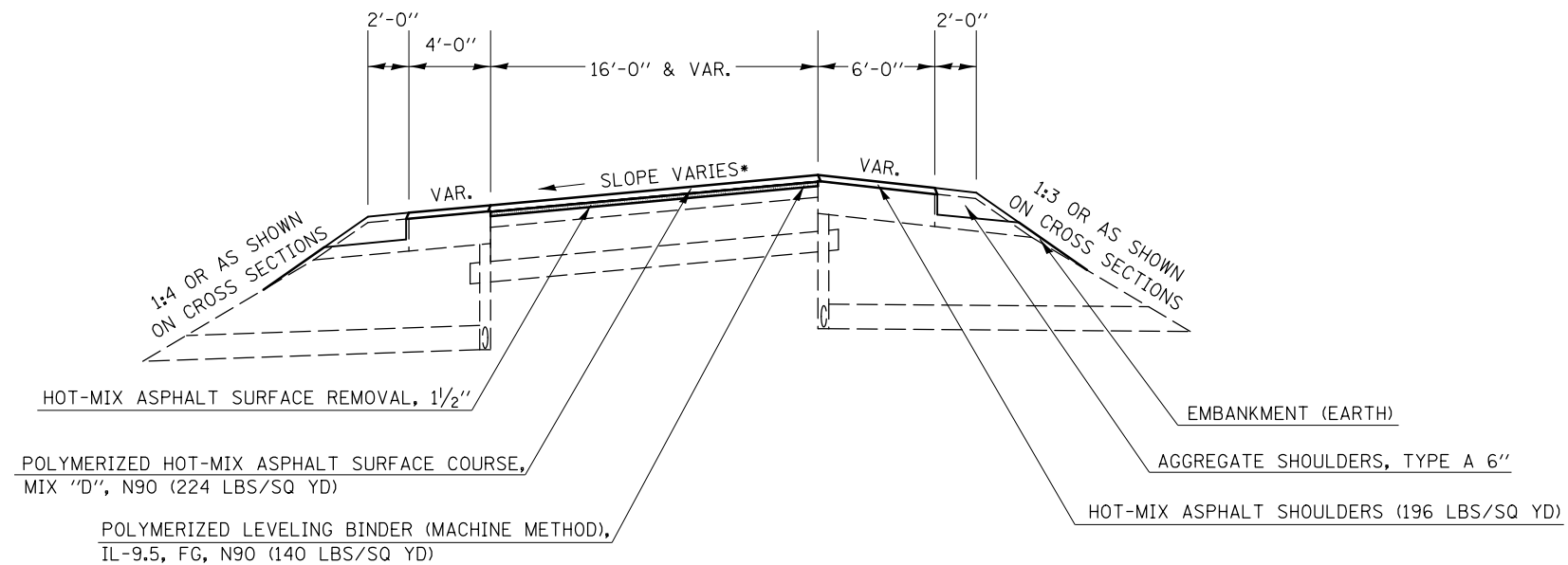
STATION TO STATION
 24+61.61 26+87.12 (RAMP N)
 30+00.00 37+79.28 (RAMP M)



* SLOPE VARIES IN SUPERELEVATION AREAS WITH A MAXIMUM OF 8.0% DIFFERENCE IN PAVEMENT GRADE AND SHOULDER GRADE.

PROPOSED RAMP TYPICAL CROSS SECTION 12

STATION TO STATION
 24+61.61 26+87.12 (RAMP N)
 30+00.00 37+79.28 (RAMP M)

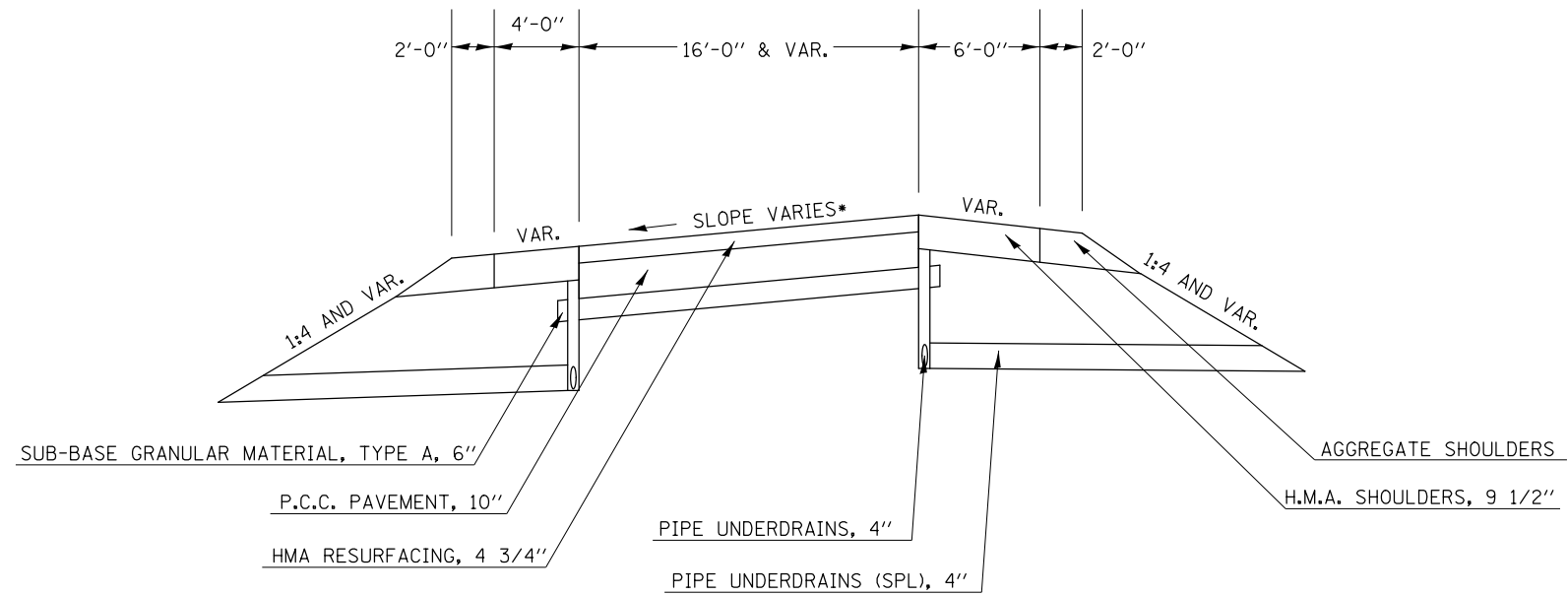


* SLOPE VARIES IN SUPERELEVATION AREAS WITH A MAXIMUM OF 8.0% DIFFERENCE IN PAVEMENT GRADE AND SHOULDER GRADE.

| | | | | | | | | | | | | | | |
|-------------|--------------------------------------------------------------|------------|-----------|-----------------------------------------------------------|-------------------------|---------------------------|---------------------------|-------------|--------------------|--------|--------------------|-----------|-----|----|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | | |
| | et:\pw\work\p\dot\detersbj\d0157116\d570570-sht-typicals.dgn | DRAWN - | REVISED - | | | SCALE: | SHEET NO. 12 OF 14 SHEETS | STA. | TO STA. | 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 30 |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | | | | | CONTRACT NO. 70570 | | | | | |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | | | | |

EXISTING RAMP TYPICAL CROSS SECTION

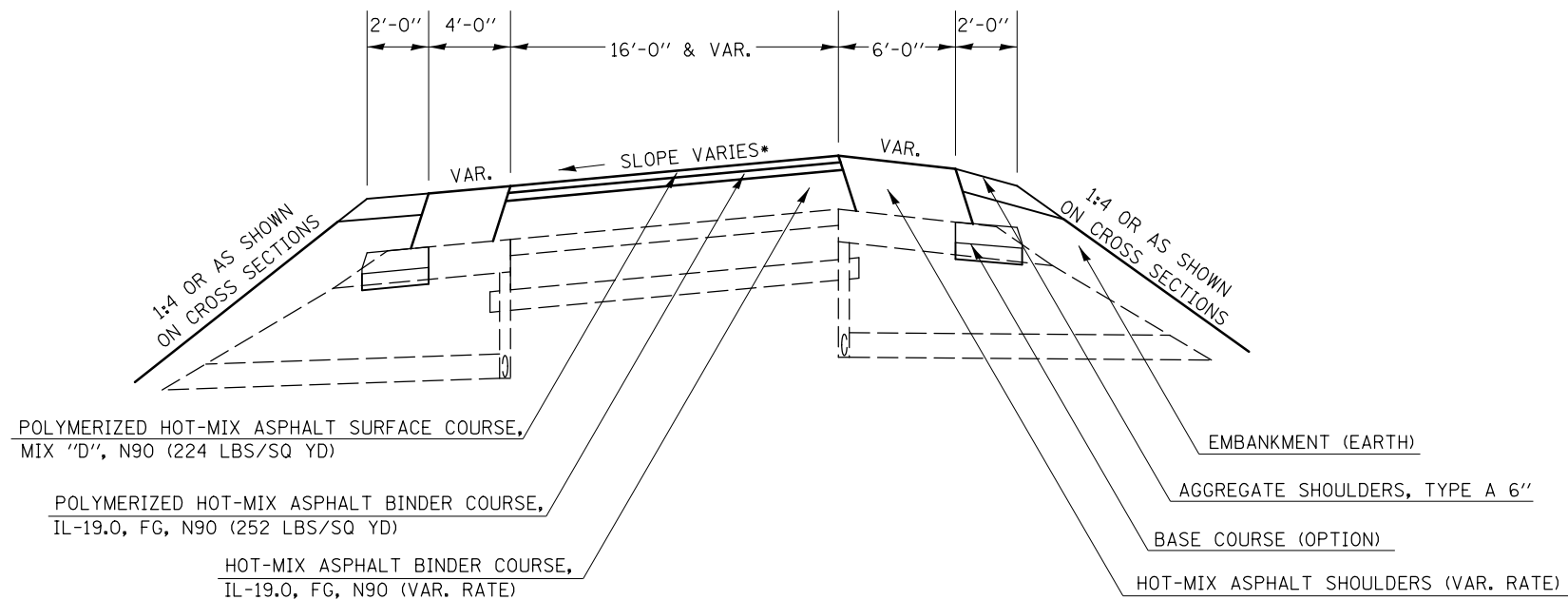
| STATION TO STATION | |
|--------------------|-------------------|
| 16+59.47 | 26+00.00 (RAMP E) |
| 9+92.00 | 23+00.00 (RAMP F) |
| 7+00.00 | 15+50.00 (RAMP J) |
| 6+00.00 | 24+61.61 (RAMP N) |



* SLOPE VARIES IN SUPERELEVATION AREAS WITH A MAXIMUM OF 8.0% DIFFERENCE IN PAVEMENT GRADE AND SHOULDER GRADE.

PROPOSED RAMP TYPICAL CROSS SECTION 13

| STATION TO STATION | |
|---------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------|
| 16+59.47 | 26+00.00 (RAMP E) |
| 9+92.00 | 23+00.00 (RAMP F) |
| 1007+00.00 | 1026+17.51 (RAMP J) |
| 6+00.00 | 10+73.00 (RAMP N) 14 |
| 14 19+02.00 | 24+61.61 (RAMP N) 12 |

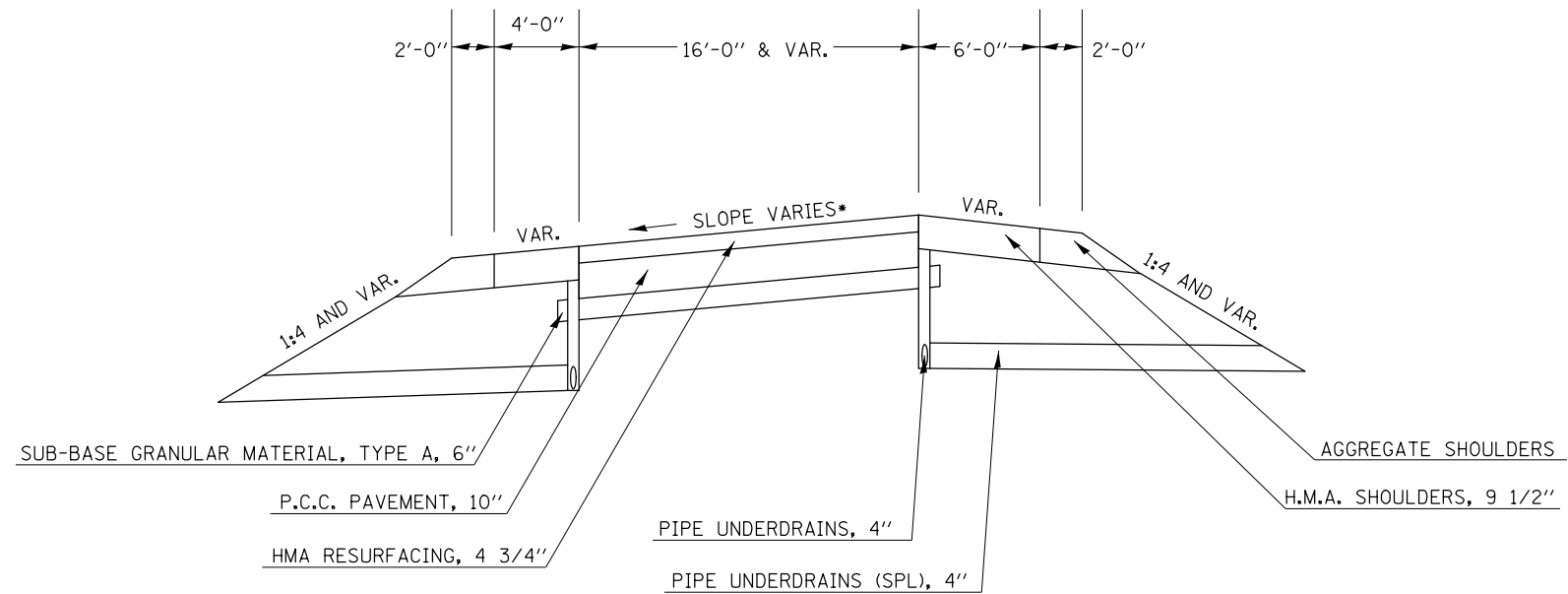


* SLOPE VARIES IN SUPERELEVATION AREAS WITH A MAXIMUM OF 8.0% DIFFERENCE IN PAVEMENT GRADE AND SHOULDER GRADE.

| | | | | | | | | | | | | | |
|-------------|----------------------|------------|-----------|-----------------------------------------------------------|---------------------------|---------------------------|------|-------------|---------|--------------------|--------------|-----------|----|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| | | DRAWN - | REVISED - | | SCALE: | SHEET NO. 13 OF 14 SHEETS | STA. | TO STA. | 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 31 |
| | | CHECKED - | REVISED - | | CONTRACT NO. 70570 | | | | | | | | |
| | | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | | |

EXISTING RAMP TYPICAL CROSS SECTION

STATION TO STATION
10+73.00 19+02.00 (RAMP N)

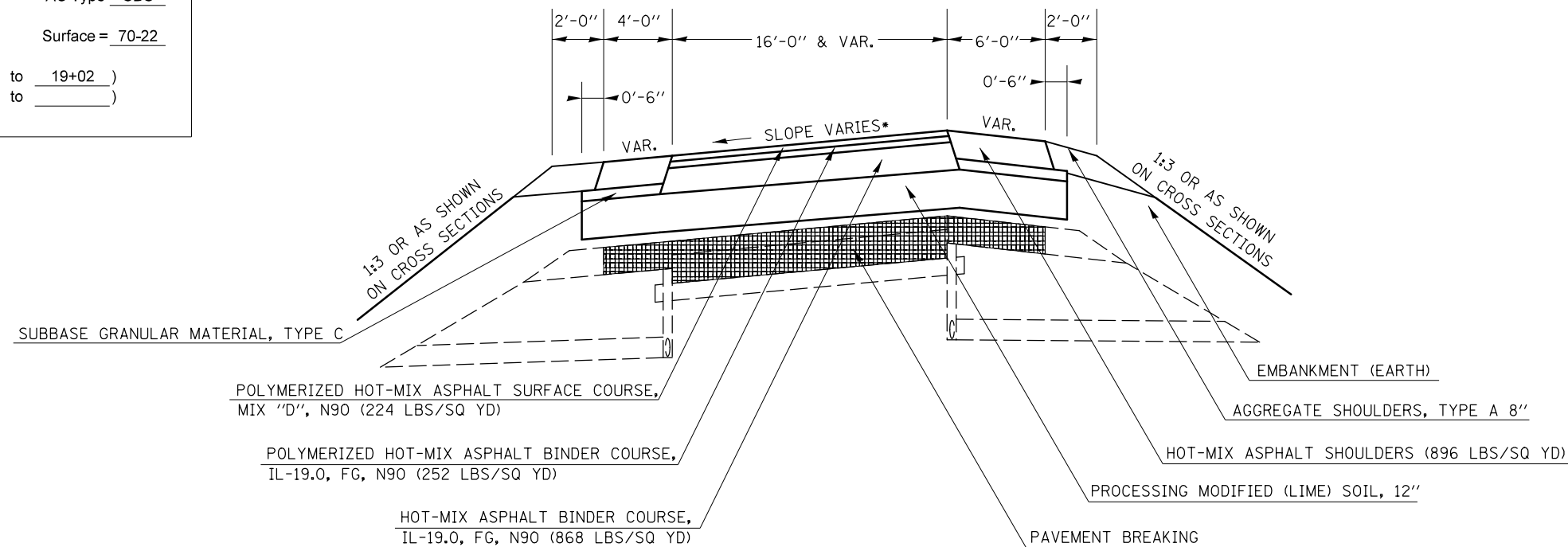


* SLOPE VARIES IN SUPERELEVATION AREAS WITH A MAXIMUM OF 8.0% DIFFERENCE IN PAVEMENT GRADE AND SHOULDER GRADE.

STRUCTURAL DESIGN TRAFFIC: Year 2024
 PV = 345 SU = 10 MU = 27
 ROAD/STREET CLASSIFICATION: Class = 1
 PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:
 P = 100% S = 100% M = 100%
 TRAFFIC FACTOR: Actual TF = 0.29 AC Type SBS
 Minimum TF = 7.11
 PG GRADE: Binder = 64-22 Surface = 70-22
 SUBGRADE SUPPORT RATING:
 SSR = POOR (Sta. 10+73 to 19+02)
 SSR = (Sta. to)

PROPOSED RAMP TYPICAL CROSS SECTION 14

STATION TO STATION
10+73.00 19+02.00 (RAMP N)



* SLOPE VARIES IN SUPERELEVATION AREAS WITH A MAXIMUM OF 8.0% DIFFERENCE IN PAVEMENT GRADE AND SHOULDER GRADE.

| | | | | | | | | | | | | | |
|---------------------------------------------------------------|----------------------|------------|-----------|-----------------------------------------------------------|---------------------------|---------------------------|------|-------------|---------|--------------------|--------------|-----------|----|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ci:\pw\work\pwidot\detersbj\d0157116\d570570-sht-typicals.dgn | | DRAWN - | REVISED - | | SCALE: | SHEET NO. 14 OF 14 SHEETS | STA. | TO STA. | 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 32 |
| | | CHECKED - | REVISED - | | CONTRACT NO. 70570 | | | | | | | | |
| | | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | | |

SCHEDULES OF QUANTITIES

EARTHWORK

| LOCATION | EARTH EXCAVATION 20200100 (CU YD) | EARTH EXCAVATION FOR BASE COURSE (OPTION) (CU YD) | STRUCTURE EXCAVATION 50200100 (CU YD) | EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (CU YD) | EMBANKMENT (CU YD) | EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD) | FURNISHED EXCAVATION 20400800 (CU YD) |
|--------------------|-----------------------------------|---------------------------------------------------|---------------------------------------|-------------------------------------------------|--------------------|-----------------------------------------------------|---------------------------------------|
| NBDL | 63.5 | 63.4 | 0.0 | 95.2 | 6398.9 | -6303.8 | 6303.8 |
| NBPL | 20.6 | 94.9 | 0.0 | 86.7 | 1216.3 | -1129.7 | 1129.7 |
| SBDL | 182.2 | 21.5 | 0.0 | 152.7 | 1861.2 | -1708.4 | 1708.4 |
| SBPL | 25.9 | 88.5 | 0.0 | 85.8 | 1138.4 | -1052.6 | 1052.6 |
| RAMP E LT. | 1.1 | 51.8 | 0.0 | 39.7 | 1733.5 | -1693.8 | 1693.8 |
| RAMP E RT. | 2.6 | 18.4 | 0.0 | 15.7 | 20.4 | -4.6 | 4.6 |
| RAMP F LT. | 14.5 | 45.9 | 0.0 | 45.3 | 1060.9 | -1015.6 | 1015.6 |
| RAMP F RT. | 11.4 | 78.0 | 0.0 | 67.0 | 2043.4 | -1976.3 | 1976.3 |
| RAMP N LT. | 0.3 | 31.3 | 0.0 | 23.7 | 5459.1 | -5435.4 | 5435.4 |
| RAMP N RT. | 10.1 | 15.6 | 0.0 | 19.2 | 3915.4 | -3896.1 | 3896.1 |
| RAMP J LT. | 1.1 | 41.5 | 0.0 | 31.9 | 397.5 | -365.6 | 365.6 |
| RAMP J RT. | 4.8 | 48.9 | 0.0 | 40.3 | 1021.4 | -981.1 | 981.1 |
| RAMP M LT. | 1.4 | 0.0 | 0.0 | 1.1 | 0.0 | 1.1 | -1.1 |
| RAMP M RT. | 7.7 | 0.0 | 0.0 | 5.8 | 5.5 | 0.3 | -0.3 |
| I-74 EB LT. | 1472.0 | 0.0 | 0.0 | 1104.0 | 96.6 | 1007.4 | -1007.4 |
| I-74 EB RT. | 1306.4 | 0.0 | 0.0 | 979.8 | 97.3 | 882.5 | -882.5 |
| I-74 WB LT. | 2939.3 | 0.0 | 0.0 | 2204.5 | 79.9 | 2124.5 | -2124.5 |
| I-74 WB RT. | 1433.0 | 0.0 | 0.0 | 1074.7 | 41.9 | 1032.9 | -1032.9 |
| S.N. 057-0251 | 0.0 | 0.0 | 726.0 | 544.5 | 0.0 | 544.5 | -544.5 |
| S.N. 057-0252 | 0.0 | 0.0 | 632.0 | 474.0 | 0.0 | 474.0 | -474.0 |
| S.N. 057-0253 | 0.0 | 0.0 | 872.0 | 654.0 | 0.0 | 654.0 | -654.0 |
| S.N. 057-0254 | 0.0 | 0.0 | 914.0 | 685.5 | 0.0 | 685.5 | -685.5 |
| SUB TOTALS: | 7498.0 | 599.4 | 3144.0 | | | | 18156.5 |
| TOTALS: | | 8097.4 | 3144.0 | | | | 18156.5 |
| ROUNDED: | | 8100.0 | | | | | 18160.0 |

NOTES:

1. THE SHRINKAGE FACTOR USED IS 25%.
2. EARTH EXCAVATION INCLUDES THE EXCAVATION QUANTITY FOR BASE COURSE (OPTION).
3. SHRINKAGE, EMBANKMENT, AND BALANCE IS FOR INFORMATION ONLY.
4. NO PAYMENT WILL BE ALLOWED FOR OVERHAUL.

EROSION CONTROL ITEMS

| STATION | ROUTE | OFFSET (FOOT) | INLET & PIPE PROTECTION 28000500 (EACH) | STATION | ROUTE | OFFSET (FOOT) | TEMP. DITCH CHECKS 28000305 (FOOT) |
|----------|----------|----------------|-----------------------------------------|----------|--------|----------------|------------------------------------|
| 30+00. | I-55B | 0.0 | 1.0 | 31+00. | I-55B | 0.0 | 20.0 |
| 37+50. | I-55B | 0.0 | 1.0 | 31+75. | I-55B | 0.0 | 20.0 |
| 37+50. | I-55B | -130.0 | 1.0 | 32+50. | I-55B | 0.0 | 20.0 |
| 44+00. | I-55B | 0.0 | 1.0 | 33+25. | I-55B | 0.0 | 20.0 |
| 50+82.2 | I-55B | -5.2 | 1.0 | 34+00. | I-55B | 0.0 | 20.0 |
| 56+46.6 | I-55B | -3.1 | 1.0 | 34+75. | I-55B | 0.0 | 20.0 |
| 66+50. | I-55B NB | -87.5 | 1.0 | 35+50. | I-55B | 0.0 | 20.0 |
| 29+50. | RAMP E | -44.5 | 1.0 | 36+25. | I-55B | 0.0 | 20.0 |
| 17+40. | RAMP F | 31.5 | 1.0 | 37+00. | I-55B | 0.0 | 20.0 |
| 17+40. | RAMP F | -115.0 | 1.0 | 38+50. | I-55B | 0.0 | 20.0 |
| 1003+00. | RAMP J | 20.5 | 1.0 | 39+25. | I-55B | 0.0 | 20.0 |
| 10+00. | RAMP N | -52.5 | 1.0 | 40+00. | I-55B | 0.0 | 20.0 |
| | | TOTAL = | 12.0 | 40+75. | I-55B | 0.0 | 20.0 |
| | | | | 41+50. | I-55B | 0.0 | 20.0 |
| | | | | 42+25. | I-55B | 0.0 | 20.0 |
| | | | | 43+00. | I-55B | 0.0 | 20.0 |
| | | | | 46+00. | I-55B | 0.0 | 20.0 |
| | | | | 47+25. | I-55B | 0.0 | 20.0 |
| | | | | 48+50. | I-55B | 0.0 | 20.0 |
| | | | | 54+00. | I-55B | 0.0 | 15.0 |
| | | | | 55+50. | I-55B | 0.0 | 15.0 |
| | | | | 59+75. | I-55B | 0.0 | 20.0 |
| | | | | 60+50. | I-55B | 0.0 | 20.0 |
| | | | | 61+00. | I-55B | 0.0 | 20.0 |
| | | | | 65+00. | I-55B | -36.8 | 20.0 |
| | | | | 23+50. | RAMP E | 57.0 | 30.0 |
| | | | | 1007+00. | RAMP J | -46.5 | 25.0 |
| | | | | 6+00. | RAMP N | 33.5 | 35.0 |
| | | | | 16+00. | RAMP N | -72.1 | 30.0 |
| | | | | 38+00. | RAMP M | 84.0 | 20.0 |
| | | | | | | TOTAL = | 630.0 |

AGGREGATE (PRIME COAT)

| LOCATION: | MAINLINE PAVEMENT AREA SQ YD | AGGREGATE (PRIME COAT) 40600300 TON |
|------------------|------------------------------|-------------------------------------|
| I-55 SBDL | 5,456.0 | 10.9 |
| I-55 SBPL | 4,114.2 | 8.2 |
| I-55 NBDL | 4,733.8 | 9.5 |
| I-55 NBPL | 3,689.1 | 7.4 |
| RAMP E | 1,577.6 | 3.2 |
| RAMP F | 2,339.4 | 4.7 |
| RAMP J | 1,487.0 | 3.0 |
| RAMP N | 3,358.6 | 6.7 |
| RAMP M | 60.8 | 0.1 |
| TOTAL | | 53.6 |
| ROUND TO: | | 55.0 |

TEMPORARY RAMPS

| STATION | ROUTE | VERTICAL DROPOFF (IN) | LENGTH (FEET) | WIDTH (FEET) | TEMPORARY RAMP 40600990 (SQ YD) |
|----------|-----------------|-----------------------|---------------|------------------|---------------------------------|
| 29+00. | SB FAP 704 | 3.25 | 10.83 | 24.0 | 28.9 |
| 29+00. | SB FAP 704 SHLD | 1.75 | 5.83 | 16.0 | 10.4 |
| 32+60. | NB FAP 704 | 3.25 | 10.83 | 24.0 | 28.9 |
| 32+60. | NB FAP 704 SHLD | 1.75 | 5.83 | 16.0 | 10.4 |
| 37+79.28 | RAMP M | 3.25 | 10.83 | 16.0 | 19.3 |
| 37+79.28 | RAMP M SHLD | 1.75 | 5.83 | 10.0 | 6.5 |
| 26+00. | RAMP E | 4.25 | 14.17 | 16.0 | 25.2 |
| 26+00. | RAMP E SHLD | 1.75 | 5.83 | 10.0 | 6.5 |
| 23+00. | RAMP F | 4.25 | 14.17 | 16.0 | 25.2 |
| 23+00. | RAMP F SHLD | 1.75 | 5.83 | 10.0 | 6.5 |
| 1007+00. | RAMP J | 4.25 | 14.17 | 16.0 | 25.2 |
| 1007+00. | RAMP J SHLD | 1.75 | 5.83 | 10.0 | 6.5 |
| 6+00. | RAMP N | 4.25 | 14.17 | 16.0 | 25.2 |
| 6+00. | RAMP N SHLD | 4.25 | 5.83 | 10.0 | 6.5 |
| | | | | TOTAL = | 231.2 |
| | | | | ROUND TO: | 235.0 |

| STATION | OFFSET (FOOT) | TO | STATION | OFFSET (FOOT) | ROUTE | PERIMETER EROSION BARRIER 28000400 (FOOT)* |
|---------|---------------|----|---------|---------------|------------------|--------------------------------------------|
| 6+00 | 62.0 | | 24+82 | 75.8 | RAMP N | 1,832.8 |
| 42+00 | -134.2 | | 37+50 | -131.0 | I-55B | 477.2 |
| 37+25 | -131.3 | | 29+50 | -133.6 | I-55B | 745.0 |
| 28+00 | 113.5 | | 37+50 | 175.6 | I-55B | 980.0 |
| | | | | | TOTAL = | 4,035.0 |
| | | | | | ROUND TO: | 4,050.0 |

*LENGTH MEASURED FROM CADD

SCHEDULES OF QUANTITIES

RIP RAP

| LOCATION | STATION | TO | STATION | OFFSET | LENGTH (FEET) | WIDTH (FEET) | FILTER FABRIC 28200200 (SQ YD) | STONE RIPRAP CLASS A4 28100107 (SQ YD) | STONE RIPRAP CLASS A4 (TONS) | BEDDING STONE (TONS) |
|-------------------------|---------|----|---------|--------|---------------|--------------|--------------------------------|----------------------------------------|------------------------------|----------------------|
| MEDIAN DITCH | 43+00. | | 44+05. | RT | 105.0 | 16.2 | 189.0 | 189.0 | 126 | 57 |
| SW APPROACH SN 057-0253 | 53+76.7 | | 53+82.7 | RT | 6.0 | 17.4 | 11.6 | 11.6 | 8 | 3 |
| SW APPROACH SN 057-0251 | 59+28.9 | | 59+34.9 | CL | 6.0 | 19.0 | 12.7 | 12.7 | 8 | 4 |
| MEDIAN DITCH | 61+00. | | 64+50. | CL | 350.0 | 16.2 | 630.0 | 630.0 | 420 | 189 |
| TOTALS = | | | | | | | 843.3 | 843.3 | 562 | 253 |
| ROUND TO: | | | | | | | 850.0 | 850.0 | | |

NOTES:

1. STONE RIPRAP AND BEDDING STONE TONNAGE QUANTITIES ARE ESTIMATES AND FOR INFORMATION ONLY.
2. STONE RIPRAP IS ESTIMATED BASED ON 1.5 TONS PER CU YD.
3. BEDDING STONE IS ESTIMATED BASED ON 1.8 TONS PER CU YD.

HMA SURFACE REMOVAL

| LOCATION | STATION | TO | STATION | LENGTH (FEET) | HOT-MIX ASPHALT SURFACE REMOVAL AREA TAKEN FROM CADD (SQ FT) | HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT 40600982 (SQ YD) |
|------------------|------------|----|------------|---------------|--------------------------------------------------------------|--------------------------------------------------------------|
| I-55 NBDL SHLD | 32+60.00 | | 32+95.00 | 35.0 | 320.8 | 35.6 |
| I-55 NBDL SHLD | 64+48.00 | | 65+00.00 | 52.0 | 511.3 | 56.8 |
| I-55 NBPL SHLD | 32+60.00 | | 32+95.00 | 35.0 | 196.2 | 21.8 |
| I-55 NBPL SHLD | 64+48.00 | | 65+00.00 | 52.0 | 278.5 | 30.9 |
| I-55 SBDL SHLD | 29+00.00 | | 29+35.00 | 35.0 | 347.8 | 38.6 |
| I-55 SBDL SHLD | 63+52.00 | | 65+00.00 | 148.0 | 1417.0 | 157.4 |
| I-55 SBPL SHLD | 29+00.00 | | 29+35.00 | 35.0 | 162.9 | 18.1 |
| I-55 SBPL SHLD | 63+52.00 | | 65+00.00 | 148.0 | 880.6 | 97.8 |
| LT. SHLD RAMP E | 25+20.00 | | 26+00.00 | 80.0 | 469.9 | 52.2 |
| RT. SHLD RAMP E | 25+20.00 | | 26+00.00 | 80.0 | 329.4 | 36.6 |
| LT. SHLD RAMP F | 17+72.00 | | 23+00.00 | 528.0 | 2206.6 | 245.2 |
| RT. SHLD RAMP F | 17+72.00 | | 23+00.00 | 528.0 | 3176.0 | 352.9 |
| LT. SHLD RAMP J | 1007+00.00 | | 1008+46.00 | 146.0 | 620.4 | 68.9 |
| RT. SHLD RAMP J | 1007+00.00 | | 1008+46.00 | 146.0 | 959.4 | 106.6 |
| RT. SHLD RAMP N | 6+00.00 | | 6+65.00 | 65.0 | 323.6 | 36.0 |
| LT. SHLD RAMP N | 6+77.00 | | 7+42.00 | 65.0 | 260.0 | 28.9 |
| LT. SHLD RAMP M | 37+44.28 | | 37+79.28 | 35.0 | 103.6 | 11.5 |
| RT. SHLD RAMP M | 37+44.28 | | 37+79.28 | 35.0 | 213.5 | 23.7 |
| SUB TOTAL SHLDS: | | | | | | 1419.7 |

HMA SURFACE REMOVAL SUMMARY

| LOCATION | HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" 44000155 (SQ YD) | HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT 40600982 (SQ YD) |
|----------------|----------------------------------------------------------|--------------------------------------------------------------|
| I-55 NBDL | 1442.2 | 370.8 |
| I-55 NBPL | 1279.7 | 376.3 |
| I-55 SBDL | 2029.3 | 580.4 |
| I-55 SBPL | 1771.1 | 580.8 |
| I-55 NBDL SHLD | | 92.5 |
| I-55 NBPL SHLD | | 52.7 |
| I-55 SBDL SHLD | | 196.1 |
| I-55 SBPL SHLD | | 115.9 |
| RAMP E | | 244.5 |
| RAMP F | | 1047.7 |
| RAMP J | | 480.9 |
| RAMP M | 344.2 | 60.8 |
| RAMP N | 323.0 | 885.6 |
| RAMP E SHLD | | 88.8 |
| RAMP F SHLD | | 598.1 |
| RAMP J SHLD | | 175.5 |
| RAMP M SHLD | | 35.2 |
| RAMP N SHLD | | 64.8 |
| TOTAL = | 7189.5 | 6047.6 |
| ROUND TO: | 7190.0 | 6050.0 |

HMA SURFACE REMOVAL (CONTINUED)

| LOCATION | STATION | TO | STATION | LENGTH (FEET) | HOT-MIX ASPHALT SURFACE REMOVAL AREA TAKEN FROM CADD (SQ FT) | HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" 44000155 (SQ YD) | HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT 40600982 (SQ YD) |
|------------------|------------|----|------------|---------------|--------------------------------------------------------------|----------------------------------------------------------|--------------------------------------------------------------|
| RAMP E | 24+72.00 | | 26+00.00 | 128.0 | 2200.8 | | 244.5 |
| RAMP F | 17+19.00 | | 23+00.00 | 581.0 | 9429.1 | | 1047.7 |
| RAMP J | 1007+00.00 | | 1009+62.00 | 262.0 | 4328.2 | | 480.9 |
| RAMP M | 35+61.15 | | 36+11.37 | 50.2 | 842.9 | 93.7 | |
| RAMP M | 36+11.37 | | 36+61.59 | 50.2 | 863.4 | 95.9 | |
| RAMP M | 36+61.59 | | 37+11.80 | 50.2 | 843.4 | 93.7 | |
| RAMP M | 37+11.80 | | 37+44.28 | 32.5 | 548.1 | 60.9 | |
| RAMP M | 37+44.28 | | 37+79.28 | 35.0 | 547.4 | | 60.8 |
| RAMP N | 21+24.00 | | 24+62.00 | 338.0 | 5689.4 | | 632.2 |
| RAMP N | 24+62.00 | | 24+82.12 | 20.1 | 303.3 | 33.7 | |
| RAMP N | 24+82.12 | | 25+33.39 | 51.3 | 715.1 | 79.5 | |
| RAMP N | 25+33.39 | | 25+84.65 | 51.3 | 666.4 | 74.0 | |
| RAMP N | 25+84.65 | | 26+35.89 | 51.2 | 628.7 | 69.9 | |
| RAMP N | 26+35.89 | | 26+87.12 | 51.2 | 593.0 | 65.9 | |
| RAMP N | 6+00.00 | | 7+42.00 | 142.0 | 2281.1 | | 253.5 |
| SUB TOTAL RAMPS: | | | | | | 667.1 | 2719.6 |

PATCHING

| STATION | TO | STATION | ROUTE | LANE | LENGTH (FT) | WIDTH (FT) | SUBBASE GRAN. MATL., TYPE B 4" 31101200 (SQ YD) | CLASS D PATCHES, TYPE II, 10 INCH 44201765 (SQ YD) | CLASS D PATCHES, TYPE IV, 10 INCH 44201771 (SQ YD) |
|-----------|----|-----------|-------|------|-------------|------------|-------------------------------------------------|----------------------------------------------------|----------------------------------------------------|
| 635+01.65 | | 636+56.65 | I-74 | WBPS | 155.0 | 6.0 | 103.3 | 0.0 | 103.3 |
| 637+75. | | 639+25. | I-74 | EBPS | 150.0 | 6.0 | 100.0 | 0.0 | 100.0 |
| 637+80. | | 639+30. | I-74 | EBDS | 150.0 | 6.0 | 100.0 | 0.0 | 100.0 |
| 50+76.29 | | 50+88.29 | I-55B | NBPS | 12.0 | 6.0 | 8.0 | 8.0 | 0.0 |
| 56+40.05 | | 56+52.05 | I-55B | SBDS | 12.0 | 6.0 | 8.0 | 8.0 | 0.0 |
| TOTALS = | | | | | | | 319.3 | 16.0 | 303.3 |
| ROUND TO: | | | | | | | 320.0 | 16.0 | 305.0 |

SCHEDULES OF QUANTITIES

HMA SURFACE REMOVAL (CONTINUED)

| LOCATION | STATION | TO | STATION | LENGTH (FEET) | HOT-MIX ASPHALT SURFACE REMOVAL AREA TAKEN FROM CADD (SQ FT) | HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" 44000155 (SQ YD) | HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT 40600982 (SQ YD) |
|---------------------|----------|----|----------|---------------|--------------------------------------------------------------|----------------------------------------------------------|--------------------------------------------------------------|
| I-55 NBDL | 32+60.00 | | 32+95.00 | 35.0 | 410.5 | | 45.6 |
| I-55 NBDL | 32+95.00 | | 33+50.00 | 55.0 | 663.9 | 73.8 | |
| I-55 NBDL | 33+50.00 | | 34+00.00 | 50.0 | 619.7 | 68.9 | |
| I-55 NBDL | 34+00.00 | | 34+50.00 | 50.0 | 614.0 | 68.2 | |
| I-55 NBDL | 34+50.00 | | 35+00.00 | 50.0 | 607.1 | 67.5 | |
| I-55 NBDL | 35+00.00 | | 35+50.00 | 50.0 | 610.3 | 67.8 | |
| I-55 NBDL | 35+50.00 | | 36+00.00 | 50.0 | 600.3 | 66.7 | |
| I-55 NBDL | 36+00.00 | | 36+50.00 | 50.0 | 595.1 | 66.1 | |
| I-55 NBDL | 36+50.00 | | 37+00.00 | 50.0 | 606.3 | 67.4 | |
| I-55 NBDL | 37+00.00 | | 37+50.00 | 50.0 | 623.1 | 69.2 | |
| I-55 NBDL | 37+50.00 | | 38+00.00 | 50.0 | 649.0 | 72.1 | |
| I-55 NBDL | 38+00.00 | | 38+50.00 | 50.0 | 757.5 | 84.2 | |
| I-55 NBDL | 38+50.00 | | 39+00.00 | 50.0 | 921.9 | 102.4 | |
| I-55 NBDL | 39+00.00 | | 39+50.00 | 50.0 | 1081.7 | 120.2 | |
| I-55 NBDL | 39+50.00 | | 40+00.00 | 50.0 | 1187.5 | 131.9 | |
| I-55 NBDL | 40+00.00 | | 40+50.00 | 50.0 | 654.3 | 72.7 | |
| I-55 NBDL | 40+50.00 | | 41+00.00 | 50.0 | 655.3 | 72.8 | |
| I-55 NBDL | 41+00.00 | | 41+50.00 | 50.0 | 649.9 | 72.2 | |
| I-55 NBDL | 41+50.00 | | 42+00.00 | 50.0 | 637.4 | 70.8 | |
| I-55 NBDL | 42+00.00 | | 42+20.00 | 20.0 | 245.5 | 27.3 | |
| I-55 NBDL | 42+20.00 | | 43+37.00 | 117.0 | 1434.9 | | 159.4 |
| I-55 NBDL | 63+76.00 | | 65+00.00 | 124.0 | 1492.3 | | 165.8 |
| I-55 NBPL | 32+60.00 | | 32+95.00 | 35.0 | 429.4 | | 47.7 |
| I-55 NBPL | 32+95.00 | | 33+50.00 | 55.0 | 651.8 | 72.4 | |
| I-55 NBPL | 33+50.00 | | 34+00.00 | 50.0 | 579.2 | 64.4 | |
| I-55 NBPL | 34+00.00 | | 34+50.00 | 50.0 | 598.2 | 66.5 | |
| I-55 NBPL | 34+50.00 | | 35+00.00 | 50.0 | 616.8 | 68.5 | |
| I-55 NBPL | 35+00.00 | | 35+50.00 | 50.0 | 632.2 | 70.2 | |
| I-55 NBPL | 35+50.00 | | 36+00.00 | 50.0 | 626.9 | 69.7 | |
| I-55 NBPL | 36+00.00 | | 36+50.00 | 50.0 | 617.4 | 68.6 | |
| I-55 NBPL | 36+50.00 | | 37+00.00 | 50.0 | 621.6 | 69.1 | |
| I-55 NBPL | 37+00.00 | | 37+50.00 | 50.0 | 627.2 | 69.7 | |
| I-55 NBPL | 37+50.00 | | 38+00.00 | 50.0 | 633.8 | 70.4 | |
| I-55 NBPL | 38+00.00 | | 38+50.00 | 50.0 | 643.1 | 71.5 | |
| I-55 NBPL | 38+50.00 | | 39+00.00 | 50.0 | 628.1 | 69.8 | |
| I-55 NBPL | 39+00.00 | | 39+50.00 | 50.0 | 613.5 | 68.2 | |
| I-55 NBPL | 39+50.00 | | 40+00.00 | 50.0 | 614.1 | 68.2 | |
| I-55 NBPL | 40+00.00 | | 40+50.00 | 50.0 | 625.9 | 69.5 | |
| I-55 NBPL | 40+50.00 | | 41+00.00 | 50.0 | 638.2 | 70.9 | |
| I-55 NBPL | 41+00.00 | | 41+50.00 | 50.0 | 646.9 | 71.9 | |
| I-55 NBPL | 41+50.00 | | 42+00.00 | 50.0 | 649.3 | 72.1 | |
| I-55 NBPL | 42+00.00 | | 42+20.00 | 20.0 | 253.7 | 28.2 | |
| I-55 NBPL | 42+20.00 | | 43+37.00 | 117.0 | 1443.1 | | 160.3 |
| I-55 NBPL | 63+76.00 | | 65+00.00 | 124.0 | 1513.8 | | 168.2 |
| SUB TOTALS I-55 NB: | | | | | 2721.9 | | 747.1 |

HMA SURFACE REMOVAL (CONTINUED)

| LOCATION | STATION | TO | STATION | LENGTH (FEET) | HOT-MIX ASPHALT SURFACE REMOVAL AREA TAKEN FROM CADD (SQ FT) | HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" 44000155 (SQ YD) | HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT 40600982 (SQ YD) |
|---------------------|----------|----|----------|---------------|--------------------------------------------------------------|----------------------------------------------------------|--------------------------------------------------------------|
| I-55 SBDL | 29+00.00 | | 29+35.00 | 35.0 | 461.0 | | 51.2 |
| I-55 SBDL | 29+35.00 | | 30+00.00 | 65.0 | 828.7 | 92.1 | |
| I-55 SBDL | 30+00.00 | | 30+50.00 | 50.0 | 604.6 | 67.2 | |
| I-55 SBDL | 30+50.00 | | 31+00.00 | 50.0 | 592.1 | 65.8 | |
| I-55 SBDL | 31+00.00 | | 31+50.00 | 50.0 | 608.9 | 67.7 | |
| I-55 SBDL | 31+50.00 | | 32+00.00 | 50.0 | 658.2 | 73.1 | |
| I-55 SBDL | 32+00.00 | | 32+50.00 | 50.0 | 744.8 | 82.8 | |
| I-55 SBDL | 32+50.00 | | 33+00.00 | 50.0 | 824.8 | 91.6 | |
| I-55 SBDL | 33+00.00 | | 33+50.00 | 50.0 | 874.2 | 97.1 | |
| I-55 SBDL | 33+50.00 | | 34+00.00 | 50.0 | 958.1 | 106.5 | |
| I-55 SBDL | 34+00.00 | | 34+50.00 | 50.0 | 1066.7 | 118.5 | |
| I-55 SBDL | 34+50.00 | | 35+00.00 | 50.0 | 1243.3 | 138.1 | |
| I-55 SBDL | 35+00.00 | | 35+50.00 | 50.0 | 1398.6 | 155.4 | |
| I-55 SBDL | 35+50.00 | | 36+00.00 | 50.0 | 596.3 | 66.3 | |
| I-55 SBDL | 36+00.00 | | 36+50.00 | 50.0 | 608.6 | 67.6 | |
| I-55 SBDL | 36+50.00 | | 37+00.00 | 50.0 | 609.6 | 67.7 | |
| I-55 SBDL | 37+00.00 | | 37+50.00 | 50.0 | 607.7 | 67.5 | |
| I-55 SBDL | 37+50.00 | | 38+00.00 | 50.0 | 601.8 | 66.9 | |
| I-55 SBDL | 38+00.00 | | 38+50.00 | 50.0 | 584.7 | 65.0 | |
| I-55 SBDL | 38+50.00 | | 39+00.00 | 50.0 | 582.0 | 64.7 | |
| I-55 SBDL | 39+00.00 | | 39+50.00 | 50.0 | 580.6 | 64.5 | |
| I-55 SBDL | 39+50.00 | | 40+00.00 | 50.0 | 576.8 | 64.1 | |
| I-55 SBDL | 40+00.00 | | 40+50.00 | 50.0 | 572.5 | 63.6 | |
| I-55 SBDL | 40+50.00 | | 41+00.00 | 50.0 | 567.4 | 63.0 | |
| I-55 SBDL | 41+00.00 | | 41+50.00 | 50.0 | 566.5 | 62.9 | |
| I-55 SBDL | 41+50.00 | | 42+00.00 | 50.0 | 572.0 | 63.6 | |
| I-55 SBDL | 42+00.00 | | 42+20.00 | 20.0 | 234.1 | 26.0 | |
| I-55 SBDL | 42+20.00 | | 43+82.00 | 162.0 | 1894.7 | | 210.5 |
| I-55 SBDL | 62+58.00 | | 65+00.00 | 242.0 | 2868.1 | | 318.7 |
| I-55 SBPL | 29+00.00 | | 29+35.00 | 35.0 | 432.4 | | 48.0 |
| I-55 SBPL | 29+35.00 | | 30+00.00 | 65.0 | 803.7 | 89.3 | |
| I-55 SBPL | 30+00.00 | | 30+50.00 | 50.0 | 629.6 | 70.0 | |
| I-55 SBPL | 30+50.00 | | 31+00.00 | 50.0 | 639.0 | 71.0 | |
| I-55 SBPL | 31+00.00 | | 31+50.00 | 50.0 | 640.7 | 71.2 | |
| I-55 SBPL | 31+50.00 | | 32+00.00 | 50.0 | 633.1 | 70.3 | |
| I-55 SBPL | 32+00.00 | | 32+50.00 | 50.0 | 620.3 | 68.9 | |
| I-55 SBPL | 32+50.00 | | 33+00.00 | 50.0 | 612.6 | 68.1 | |
| I-55 SBPL | 33+00.00 | | 33+50.00 | 50.0 | 617.1 | 68.6 | |
| I-55 SBPL | 33+50.00 | | 34+00.00 | 50.0 | 629.4 | 69.9 | |
| I-55 SBPL | 34+00.00 | | 34+50.00 | 50.0 | 639.7 | 71.1 | |
| I-55 SBPL | 34+50.00 | | 35+00.00 | 50.0 | 638.2 | 70.9 | |
| I-55 SBPL | 35+00.00 | | 35+50.00 | 50.0 | 620.8 | 69.0 | |
| I-55 SBPL | 35+50.00 | | 36+00.00 | 50.0 | 596.3 | 66.3 | |
| I-55 SBPL | 36+00.00 | | 36+50.00 | 50.0 | 616.7 | 68.5 | |
| I-55 SBPL | 36+50.00 | | 37+00.00 | 50.0 | 602.1 | 66.9 | |
| I-55 SBPL | 37+00.00 | | 37+50.00 | 50.0 | 596.7 | 66.3 | |
| I-55 SBPL | 37+50.00 | | 38+00.00 | 50.0 | 606.9 | 67.4 | |
| I-55 SBPL | 38+00.00 | | 38+50.00 | 50.0 | 608.0 | 67.6 | |
| I-55 SBPL | 38+50.00 | | 39+00.00 | 50.0 | 609.6 | 67.7 | |
| I-55 SBPL | 39+00.00 | | 39+50.00 | 50.0 | 611.9 | 68.0 | |
| I-55 SBPL | 39+50.00 | | 40+00.00 | 50.0 | 619.8 | 68.9 | |
| I-55 SBPL | 40+00.00 | | 40+50.00 | 50.0 | 628.2 | 69.8 | |
| I-55 SBPL | 40+50.00 | | 41+00.00 | 50.0 | 627.3 | 69.7 | |
| I-55 SBPL | 41+00.00 | | 41+50.00 | 50.0 | 622.1 | 69.1 | |
| I-55 SBPL | 41+50.00 | | 42+00.00 | 50.0 | 620.1 | 68.9 | |
| I-55 SBPL | 42+00.00 | | 42+20.00 | 20.0 | 250.2 | 27.8 | |
| I-55 SBPL | 42+20.00 | | 43+82.00 | 162.0 | 1940.1 | | 215.6 |
| I-55 SBPL | 62+58.00 | | 65+00.00 | 242.0 | 2854.8 | | 317.2 |
| SUB TOTALS I-55 SB: | | | | | 3800.4 | | 1161.2 |

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 1 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | POLYMERIZED LEVELING BINDER (MACHINE METHOD) AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 9.5, FG, N90 40600909 (TON) | HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603092 (TON) | POLYMERIZED HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603243 (TON) | SURFACE AVERAGE END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 40603545 (TON) |
|-------------------------------|----------|----|----------|------------------|--------------------------------------------------------------|------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------|--------------------------------------------------------------------|----------------------------------------------------------------------------|--------------------------------------------------------------------------------|----------------------------------------------------------------------------------------|--------------------------------------------------------|-------------------------------------------------------------------------------------|
| I-55 NBDL | | | | | | | | | | | | | |
| I-55 NBDL | 32+60.00 | | 33+00.00 | 40.00 | 10.5 | 1.3 | 3.8 | | | | | 2.0 | 6.1 |
| I-55 NBDL | 33+00.00 | | 33+50.00 | 50.00 | 13.4 | 1.3 | 4.8 | | | | | 2.1 | 7.7 |
| I-55 NBDL | 33+50.00 | | 34+00.00 | 50.00 | 13.8 | 1.3 | 4.8 | | | | | 2.1 | 7.7 |
| I-55 NBDL | 34+00.00 | | 34+50.00 | 50.00 | 13.6 | 1.3 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 NBDL | 34+50.00 | | 35+00.00 | 50.00 | 13.5 | 1.3 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 NBDL | 35+00.00 | | 35+50.00 | 50.00 | 13.6 | 1.3 | 4.7 | | | | | 2.0 | 7.6 |
| I-55 NBDL | 35+50.00 | | 36+00.00 | 50.00 | 13.3 | 1.2 | 4.6 | | | | | 2.0 | 7.5 |
| I-55 NBDL | 36+00.00 | | 36+50.00 | 50.00 | 13.2 | 1.2 | 4.6 | | | | | 2.0 | 7.4 |
| I-55 NBDL | 36+50.00 | | 37+00.00 | 50.00 | 13.5 | 1.2 | 4.6 | | | | | 2.0 | 7.4 |
| I-55 NBDL | 37+00.00 | | 37+50.00 | 50.00 | 13.8 | 1.2 | 4.6 | | | | | 2.0 | 7.4 |
| I-55 NBDL | 37+50.00 | | 38+00.00 | 50.00 | 14.4 | 1.2 | 4.6 | | | | | 2.0 | 7.4 |
| I-55 NBDL | 38+00.00 | | 38+50.00 | 50.00 | 16.8 | 1.6 | 5.8 | | | | | 2.5 | 9.4 |
| I-55 NBDL | 38+50.00 | | 39+00.00 | 50.00 | 20.5 | 2.0 | 7.5 | | | | | 3.2 | 12.0 |
| I-55 NBDL | 39+00.00 | | 39+50.00 | 50.00 | 24.0 | 2.2 | 8.4 | | | | | 3.6 | 13.4 |
| I-55 NBDL | 39+50.00 | | 40+00.00 | 50.00 | 26.4 | 2.4 | 8.8 | | | | | 3.8 | 14.1 |
| I-55 NBDL | 40+00.00 | | 40+50.00 | 50.00 | 27.7 | 2.5 | 9.2 | | | | | 4.0 | 14.8 |
| I-55 NBDL | 40+50.00 | | 41+00.00 | 50.00 | 28.6 | 2.7 | 10.0 | | | | | 4.3 | 16.0 |
| I-55 NBDL | 41+00.00 | | 41+50.00 | 50.00 | 29.4 | 2.7 | 9.9 | | | | | 4.4 | 16.5 |
| I-55 NBDL | 41+50.00 | | 42+00.00 | 50.00 | 30.2 | 2.7 | 10.2 | | | | | 4.5 | 16.9 |
| I-55 NBDL | 42+00.00 | | 42+50.00 | 50.00 | 13.8 | 1.3 | 4.7 | | | | | 2.0 | 7.6 |
| I-55 NBDL | 42+50.00 | | 43+00.00 | 50.00 | 13.6 | 1.3 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 NBDL | 43+00.00 | | 43+50.00 | 50.00 | 13.6 | 1.7 | 6.2 | | | | | 2.0 | 7.5 |
| I-55 NBDL | 43+50.00 | | 43+62.00 | 12.00 | 3.4 | 2.1 | 1.8 | | | | | 2.0 | 1.8 |
| I-55 NBDL | 43+62.00 | | 44+00.00 | 38.00 | 10.2 | | | | | 3.8 | 10.9 | 2.0 | 5.7 |
| I-55 NBDL | 44+00.00 | | 44+10.60 | 10.60 | 2.9 | | | | | 3.8 | 3.0 | 2.0 | 1.6 |
| I-55 NBDL | 44+10.60 | | 44+50.00 | 39.40 | 16.0 | | | 3.3 | 9.6 | 2.3 | 6.6 | 2.0 | 5.9 |
| I-55 NBDL | 44+50.00 | | 45+00.00 | 50.00 | 20.3 | | | 3.8 | 14.3 | 2.3 | 8.5 | 2.0 | 7.5 |
| I-55 NBDL | 45+00.00 | | 45+50.00 | 50.00 | 20.3 | | | 5.1 | 19.0 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBDL | 45+50.00 | | 46+00.00 | 50.00 | 20.3 | | | 6.5 | 24.3 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBDL | 46+00.00 | | 46+50.00 | 50.00 | 20.3 | | | 7.8 | 29.1 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBDL | 46+50.00 | | 47+00.00 | 50.00 | 20.3 | | | 8.5 | 31.6 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBDL | 47+00.00 | | 47+50.00 | 50.00 | 20.3 | | | 8.9 | 33.2 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBDL | 47+50.00 | | 48+00.00 | 50.00 | 20.3 | | | 9.5 | 35.5 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBDL | 48+00.00 | | 48+50.00 | 50.00 | 20.3 | | | 9.6 | 36.0 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBDL | 48+50.00 | | 49+00.00 | 50.00 | 21.5 | | | 10.0 | 37.3 | 2.4 | 9.0 | 2.1 | 7.9 |
| I-55 NBDL | 49+00.00 | | 49+50.00 | 50.00 | 23.4 | | | 10.7 | 40.0 | 2.6 | 9.8 | 2.3 | 8.6 |
| I-55 NBDL | 49+50.00 | | 50+00.00 | 50.00 | 25.2 | | | 11.5 | 43.0 | 2.8 | 10.6 | 2.5 | 9.3 |
| I-55 NBDL | 50+00.00 | | 50+50.00 | 50.00 | 27.0 | | | 13.4 | 50.1 | 3.0 | 11.3 | 2.7 | 9.9 |
| I-55 NBDL | 50+50.00 | | 51+00.00 | 50.00 | 28.8 | | | 16.2 | 60.5 | 3.2 | 12.0 | 2.8 | 10.6 |
| I-55 NBDL | 51+00.00 | | 51+29.38 | 29.38 | 15.6 | | | 17.7 | 38.8 | 3.3 | 7.3 | 2.9 | 6.4 |
| BRIDGE OMISSION S.N. 057-0253 | | | | | | | | | | | | | |
| I-55 NBDL | 53+89.92 | | 54+00.00 | 10.08 | 2.2 | | | 21.0 | 15.8 | 4.5 | 3.4 | 4.0 | 3.0 |
| I-55 NBDL | 54+00.00 | | 54+50.00 | 50.00 | 41.1 | | | 20.6 | 77.0 | 4.6 | 17.3 | 4.1 | 15.3 |
| I-55 NBDL | 54+50.00 | | 55+00.00 | 50.00 | 43.4 | | | 21.3 | 79.7 | 4.8 | 18.1 | 4.3 | 15.9 |
| I-55 NBDL | 55+00.00 | | 55+50.00 | 50.00 | 45.2 | | | 23.6 | 88.0 | 5.0 | 18.8 | 4.5 | 16.6 |

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 2 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | POLYMERIZED LEVELING BINDER (MACHINE METHOD) AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 9.5, FG, N90 40600909 (TON) | HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603092 (TON) | POLYMERIZED HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603243 (TON) | SURFACE AVERAGE END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 40603545 (TON) |
|-----------------------------------------|----------|----|----------|------------------|--------------------------------------------------------------|------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------|--------------------------------------------------------------------|----------------------------------------------------------------------------|--------------------------------------------------------------------------------|----------------------------------------------------------------------------------------|--------------------------------------------------------|-------------------------------------------------------------------------------------|
| I-55 NBDL | 55+50.00 | | 56+00.0 | 50.00 | 47.1 | | | 25.6 | 95.4 | 5.2 | 19.6 | 4.6 | 17.3 |
| I-55 NBDL | 56+00.00 | | 56+50.00 | 50.00 | 48.9 | | | 27.2 | 101.7 | 5.4 | 20.3 | 4.8 | 17.9 |
| I-55 NBDL | 56+50.00 | | 56+91.55 | 41.55 | 35.7 | | | 28.1 | 87.2 | 5.5 | 17.2 | 4.9 | 15.2 |
| I-55 NBDL BRIDGE OMISSION S.N. 057-0251 | | | | | | | | | | | | | |
| I-55 NBDL | 59+29.99 | | 59+52.11 | 22.12 | 6.0 | | | 10.9 | 18.0 | 2.3 | 3.8 | 2.0 | 3.3 |
| I-55 NBDL | 59+52.11 | | 60+02.87 | 50.76 | 20.3 | | | 10.7 | 40.5 | 2.3 | 8.7 | 2.0 | 7.6 |
| I-55 NBDL | 60+02.87 | | 60+53.72 | 50.85 | 20.4 | | | 10.2 | 38.8 | 2.3 | 8.7 | 2.0 | 7.7 |
| I-55 NBDL | 60+53.72 | | 61+04.72 | 51.00 | 20.5 | | | 9.2 | 34.9 | 2.3 | 8.8 | 2.0 | 7.7 |
| I-55 NBDL | 61+04.72 | | 61+55.92 | 51.20 | 20.5 | | | 7.6 | 29.1 | 2.3 | 8.8 | 2.0 | 7.7 |
| I-55 NBDL | 61+55.92 | | 62+07.36 | 51.44 | 20.6 | | | 6.1 | 23.3 | 2.3 | 8.9 | 2.0 | 7.8 |
| I-55 NBDL | 62+07.36 | | 62+59.10 | 51.74 | 20.7 | | | 4.5 | 17.5 | 2.3 | 9.0 | 2.0 | 7.9 |
| I-55 NBDL | 62+59.10 | | 63+09.60 | 50.50 | 19.8 | | | 3.8 | 14.3 | 2.3 | 8.8 | 2.0 | 7.7 |
| I-55 NBDL | 63+09.60 | | 63+11.18 | 1.58 | 0.7 | | | | | 4.4 | 0.5 | 2.0 | 0.2 |
| I-55 NBDL | 63+11.18 | | 63+63.67 | 52.49 | 14.0 | | | | | 3.5 | 13.6 | 2.0 | 8.0 |
| I-55 NBDL | 63+63.67 | | 64+16.61 | 52.94 | 14.2 | | | | | 2.4 | 9.7 | 2.1 | 8.1 |
| I-55 NBDL | 64+16.61 | | 64+70.03 | 53.42 | 14.2 | | | | | 2.3 | 9.3 | 2.1 | 8.3 |
| I-55 NBDL | 64+70.03 | | 65+00.0 | 29.97 | 7.5 | | | | | 2.3 | 5.2 | 2.1 | 4.7 |
| SUB-TOTAL I-55 NBDL: | | | | | 1191.0 | | 137.7 | | 1263.5 | | 367.6 | | 532.7 |
| I-55 NBPL | | | | | | | | | | | | | |
| I-55 NBPL | 32+60.00 | | 33+00.00 | 40.00 | 10.9 | 0.6 | 1.8 | | | | | 2.0 | 5.8 |
| I-55 NBPL | 33+00.00 | | 33+50.00 | 50.00 | 13.2 | 1.2 | 4.5 | | | | | 1.9 | 7.3 |
| I-55 NBPL | 33+50.00 | | 34+00.00 | 50.00 | 12.9 | 1.2 | 4.6 | | | | | 2.0 | 7.4 |
| I-55 NBPL | 34+00.00 | | 34+50.00 | 50.00 | 13.3 | 1.3 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 NBPL | 34+50.00 | | 35+00.00 | 50.00 | 13.7 | 1.2 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 NBPL | 35+00.00 | | 35+50.00 | 50.00 | 14.0 | 1.2 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 NBPL | 35+50.00 | | 36+00.0 | 50.00 | 13.9 | 1.3 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 NBPL | 36+00.00 | | 36+50.00 | 50.00 | 13.7 | 1.3 | 4.7 | | | | | 2.0 | 7.6 |
| I-55 NBPL | 36+50.00 | | 37+00.00 | 50.00 | 13.8 | 1.3 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 NBPL | 37+00.00 | | 37+50.00 | 50.00 | 13.9 | 1.2 | 4.6 | | | | | 2.0 | 7.5 |
| I-55 NBPL | 37+50.00 | | 38+00.00 | 50.00 | 14.1 | 1.2 | 4.6 | | | | | 2.0 | 7.4 |
| I-55 NBPL | 38+00.00 | | 38+50.00 | 50.00 | 14.3 | 1.2 | 4.6 | | | | | 2.0 | 7.4 |
| I-55 NBPL | 38+50.00 | | 39+00.00 | 50.00 | 14.0 | 1.2 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 NBPL | 39+00.00 | | 39+50.00 | 50.00 | 13.6 | 1.3 | 4.7 | | | | | 2.0 | 7.6 |
| I-55 NBPL | 39+50.00 | | 40+00.0 | 50.00 | 13.6 | 1.3 | 4.7 | | | | | 2.0 | 7.6 |
| I-55 NBPL | 40+00.00 | | 40+50.00 | 50.00 | 13.9 | 1.3 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 NBPL | 40+50.00 | | 41+00.00 | 50.00 | 14.2 | 1.2 | 4.6 | | | | | 2.0 | 7.5 |
| I-55 NBPL | 41+00.00 | | 41+50.00 | 50.00 | 14.4 | 1.2 | 4.3 | | | | | 2.0 | 7.5 |
| I-55 NBPL | 41+50.00 | | 42+00.00 | 50.00 | 14.4 | 1.2 | 4.4 | | | | | 2.0 | 7.6 |
| I-55 NBPL | 42+00.00 | | 42+50.00 | 50.00 | 14.1 | 1.3 | 4.7 | | | | | 2.0 | 7.6 |
| I-55 NBPL | 42+50.00 | | 43+00.00 | 50.00 | 13.7 | 1.3 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 NBPL | 43+00.00 | | 43+50.00 | 50.00 | 13.5 | 1.5 | 5.6 | | | | | 2.0 | 7.5 |
| I-55 NBPL | 43+50.00 | | 43+62.00 | 12.00 | 3.4 | 1.8 | 1.6 | | | | | 2.0 | 1.8 |
| I-55 NBPL | 43+62.00 | | 44+00.00 | 38.00 | 10.1 | | | | | 4.0 | 11.4 | 2.0 | 5.7 |
| I-55 NBPL | 44+00.00 | | 44+10.60 | 10.60 | 2.9 | | | | | 4.0 | 3.2 | 2.0 | 1.6 |
| I-55 NBPL | 44+10.60 | | 44+50.00 | 39.40 | 15.9 | | | 2.9 | 8.6 | 2.3 | 6.8 | 2.0 | 5.9 |
| I-55 NBPL | 44+50.00 | | 45+00.00 | 50.00 | 20.2 | | | 3.5 | 13.0 | 2.3 | 8.6 | 2.0 | 7.5 |

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 3 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | POLYMERIZED LEVELING BINDER (MACHINE METHOD) AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 9.5, FG, N90 40600909 (TON) | HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603092 (TON) | POLYMERIZED HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603243 (TON) | SURFACE AVERAGE END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 40603545 (TON) |
|----------------------|-------------------------------|----|----------|------------------|--------------------------------------------------------------|------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------|--------------------------------------------------------------------|----------------------------------------------------------------------------|--------------------------------------------------------------------------------|----------------------------------------------------------------------------------------|--------------------------------------------------------|-------------------------------------------------------------------------------------|
| I-55 NBPL | 45+00.00 | | 45+50.00 | 50.00 | 20.2 | | | 4.6 | 17.1 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 45+50.00 | | 46+00.00 | 50.00 | 20.2 | | | 5.9 | 22.1 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 46+00.00 | | 46+50.00 | 50.00 | 20.2 | | | 7.3 | 27.1 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 46+50.00 | | 47+00.00 | 50.00 | 20.2 | | | 8.0 | 29.8 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 47+00.00 | | 47+50.00 | 50.00 | 20.2 | | | 8.4 | 31.3 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 47+50.00 | | 48+00.00 | 50.00 | 20.2 | | | 9.0 | 33.4 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 48+00.00 | | 48+50.00 | 50.00 | 20.2 | | | 9.2 | 34.3 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 48+50.00 | | 49+00.00 | 50.00 | 20.2 | | | 8.9 | 33.2 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 49+00.00 | | 49+50.00 | 50.00 | 20.2 | | | 8.4 | 31.5 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 49+50.00 | | 50+00.00 | 50.00 | 20.2 | | | 8.0 | 30.0 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 50+00.00 | | 50+50.00 | 50.00 | 20.2 | | | 8.1 | 30.4 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 50+50.00 | | 51+00.00 | 50.00 | 20.2 | | | 8.8 | 32.8 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 51+00.00 | | 51+29.38 | 29.38 | 6.6 | | | 9.2 | 20.2 | 2.3 | 5.0 | 2.0 | 4.4 |
| I-55 NBPL | BRIDGE OMISSION S.N. 057-0253 | | | | | | | | | | | | |
| I-55 NBPL | 53+89.92 | | 54+00.00 | 10.08 | 4.4 | | | 9.2 | 6.9 | 2.3 | 1.7 | 2.0 | 1.5 |
| I-55 NBPL | 54+00.00 | | 54+50.00 | 50.00 | 20.2 | | | 8.9 | 33.3 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 54+50.00 | | 55+00.00 | 50.00 | 20.2 | | | 9.0 | 33.6 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 55+00.00 | | 55+50.00 | 50.00 | 20.2 | | | 9.5 | 35.5 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 55+50.00 | | 56+00.00 | 50.00 | 20.2 | | | 9.5 | 35.5 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 56+00.00 | | 56+50.00 | 50.00 | 20.2 | | | 9.0 | 33.6 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 NBPL | 56+50.00 | | 56+91.55 | 41.55 | 13.3 | | | 8.7 | 26.9 | 2.3 | 7.1 | 2.0 | 6.2 |
| I-55 NBPL | BRIDGE OMISSION S.N. 057-0251 | | | | | | | | | | | | |
| I-55 NBPL | 59+29.99 | | 59+52.11 | 22.12 | 6.1 | | | 9.8 | 16.3 | 2.3 | 3.8 | 2.0 | 3.3 |
| I-55 NBPL | 59+52.11 | | 60+02.87 | 50.76 | 20.3 | | | 9.8 | 37.3 | 2.3 | 8.7 | 2.0 | 7.6 |
| I-55 NBPL | 60+02.87 | | 60+53.72 | 50.85 | 20.3 | | | 9.6 | 36.3 | 2.3 | 8.7 | 2.0 | 7.7 |
| I-55 NBPL | 60+53.72 | | 61+04.72 | 51.00 | 20.3 | | | 8.9 | 33.8 | 2.3 | 8.8 | 2.0 | 7.7 |
| I-55 NBPL | 61+04.72 | | 61+55.92 | 51.20 | 20.4 | | | 7.7 | 29.4 | 2.3 | 8.8 | 2.0 | 7.7 |
| I-55 NBPL | 61+55.92 | | 62+07.36 | 51.44 | 20.5 | | | 6.0 | 23.2 | 2.3 | 8.9 | 2.0 | 7.8 |
| I-55 NBPL | 62+07.36 | | 62+59.10 | 51.74 | 20.6 | | | 4.4 | 16.9 | 2.3 | 9.0 | 2.0 | 7.9 |
| I-55 NBPL | 62+59.10 | | 63+09.60 | 50.50 | 20.6 | | | 3.6 | 13.4 | 2.3 | 8.8 | 2.0 | 7.7 |
| I-55 NBPL | 63+09.60 | | 63+11.18 | 1.58 | 0.4 | | | | | 4.0 | 0.5 | 2.0 | 0.2 |
| I-55 NBPL | 63+11.18 | | 63+63.67 | 52.49 | 13.7 | | | | | 3.1 | 12.1 | 2.0 | 8.0 |
| I-55 NBPL | 63+63.67 | | 64+16.61 | 52.94 | 14.1 | | | | | 2.2 | 8.7 | 2.1 | 8.1 |
| I-55 NBPL | 64+16.61 | | 64+70.03 | 53.42 | 14.5 | | | | | 2.3 | 9.4 | 2.1 | 8.4 |
| I-55 NBPL | 64+70.03 | | 65+00.00 | 29.97 | 8.7 | | | | | 2.4 | 5.3 | 2.1 | 4.8 |
| SUB-TOTAL I-55 NBPL: | | | | | 922.3 | | 101.5 | | 806.8 | | 291.3 | | 412.7 |
| I-55 SBDL | | | | | | | | | | | | | |
| I-55 SBDL | 29+00.00 | | 29+50.00 | 50.00 | 14.9 | 1.4 | 5.2 | | | | | 2.3 | 8.6 |
| I-55 SBDL | 29+50.00 | | 30+00.00 | 50.00 | 13.8 | 1.3 | 4.9 | | | | | 2.1 | 7.9 |
| I-55 SBDL | 30+00.00 | | 30+50.00 | 50.00 | 13.4 | 1.2 | 4.4 | | | | | 1.9 | 7.1 |
| I-55 SBDL | 30+50.00 | | 31+00.00 | 50.00 | 13.2 | 1.2 | 4.4 | | | | | 1.9 | 7.0 |
| I-55 SBDL | 31+00.00 | | 31+50.00 | 50.00 | 13.5 | 1.2 | 4.6 | | | | | 2.0 | 7.4 |
| I-55 SBDL | 31+50.00 | | 32+00.00 | 50.00 | 14.6 | 1.3 | 5.0 | | | | | 2.1 | 8.0 |
| I-55 SBDL | 32+00.00 | | 32+50.00 | 50.00 | 16.6 | 1.5 | 5.6 | | | | | 2.4 | 9.0 |
| I-55 SBDL | 32+50.00 | | 33+00.00 | 50.00 | 18.3 | 1.6 | 5.9 | | | | | 2.5 | 9.5 |
| I-55 SBDL | 33+00.00 | | 33+50.00 | 50.00 | 19.4 | 1.7 | 6.4 | | | | | 2.8 | 10.3 |

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 4 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | POLYMERIZED LEVELING BINDER (MACHINE METHOD) AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 9.5, FG, N90 40600909 (TON) | HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603092 (TON) | POLYMERIZED HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603243 (TON) | SURFACE AVERAGE END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 40603545 (TON) |
|-------------------------------|----------|----|----------|------------------|--------------------------------------------------------------|------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------|--------------------------------------------------------------------|----------------------------------------------------------------------------|--------------------------------------------------------------------------------|----------------------------------------------------------------------------------------|--------------------------------------------------------|-------------------------------------------------------------------------------------|
| I-55 SBDL | 33+50.00 | | 34+00.00 | 50.00 | 21.3 | 2.0 | 7.3 | | | | | 3.1 | 11.7 |
| I-55 SBDL | 34+00.00 | | 34+50.00 | 50.00 | 23.7 | 2.3 | 8.4 | | | | | 3.6 | 13.5 |
| I-55 SBDL | 34+50.00 | | 35+00.00 | 50.00 | 27.6 | 2.6 | 9.8 | | | | | 4.2 | 15.7 |
| I-55 SBDL | 35+00.00 | | 35+50.00 | 50.00 | 32.4 | 3.0 | 11.2 | | | | | 4.8 | 18.0 |
| I-55 SBDL | 35+50.00 | | 36+00.0 | 50.00 | 37.8 | 3.4 | 12.8 | | | | | 5.5 | 20.5 |
| I-55 SBDL | 36+00.00 | | 36+50.00 | 50.00 | 43.2 | 3.9 | 14.6 | | | | | 6.3 | 23.3 |
| I-55 SBDL | 36+50.00 | | 37+00.00 | 50.00 | 48.6 | 4.5 | 16.8 | | | | | 7.2 | 26.9 |
| I-55 SBDL | 37+00.00 | | 37+34.68 | 34.68 | 36.9 | 4.8 | 12.5 | | | | | 7.7 | 20.0 |
| I-55 SBDL | 37+34.68 | | 37+50.00 | 15.32 | 4.1 | 1.2 | 1.4 | | | | | 2.0 | 2.3 |
| I-55 SBDL | 37+50.00 | | 38+00.00 | 50.00 | 13.4 | 1.2 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 SBDL | 38+00.00 | | 38+50.00 | 50.00 | 13.0 | 1.2 | 4.6 | | | | | 2.0 | 7.4 |
| I-55 SBDL | 38+50.00 | | 39+00.00 | 50.00 | 12.9 | 1.2 | 4.4 | | | | | 1.9 | 7.1 |
| I-55 SBDL | 39+00.00 | | 39+50.00 | 50.00 | 12.9 | 1.2 | 4.6 | | | | | 2.0 | 7.4 |
| I-55 SBDL | 39+50.00 | | 40+00.0 | 50.00 | 12.8 | 1.2 | 4.3 | | | | | 1.9 | 6.9 |
| I-55 SBDL | 40+00.00 | | 40+50.00 | 50.00 | 12.7 | 1.2 | 4.4 | | | | | 1.9 | 7.0 |
| I-55 SBDL | 40+50.00 | | 41+00.00 | 50.00 | 12.6 | 1.2 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 SBDL | 41+00.00 | | 41+50.00 | 50.00 | 12.6 | 1.2 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 SBDL | 41+50.00 | | 42+00.00 | 50.00 | 12.7 | 1.3 | 4.8 | | | | | 2.1 | 7.7 |
| I-55 SBDL | 42+00.00 | | 42+50.00 | 50.00 | 12.8 | 1.3 | 4.9 | | | | | 2.1 | 7.8 |
| I-55 SBDL | 42+50.00 | | 43+00.00 | 50.00 | 13.0 | 1.3 | 4.8 | | | | | 2.1 | 7.7 |
| I-55 SBDL | 43+00.00 | | 43+50.00 | 50.00 | 13.1 | 1.3 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 SBDL | 43+50.00 | | 44+00.00 | 50.00 | 13.1 | 1.6 | 6.0 | | | | | 2.0 | 7.5 |
| I-55 SBDL | 44+00.00 | | 44+07.00 | 7.00 | 1.9 | 2.0 | 1.0 | | | | | 2.0 | 1.1 |
| I-55 SBDL | 44+07.00 | | 44+50.00 | 43.00 | 11.2 | | | | | 3.7 | 12.0 | 2.0 | 6.5 |
| I-55 SBDL | 44+50.00 | | 44+63.00 | 13.00 | 3.5 | | | | | 3.7 | 3.6 | 2.0 | 2.0 |
| I-55 SBDL | 44+63.00 | | 45+00.00 | 37.00 | 14.5 | | | 3.5 | 9.6 | 2.3 | 6.4 | 2.0 | 5.6 |
| I-55 SBDL | 45+00.00 | | 45+50.00 | 50.00 | 19.7 | | | 4.1 | 15.2 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBDL | 45+50.00 | | 46+00.0 | 50.00 | 19.7 | | | 5.2 | 19.5 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBDL | 46+00.00 | | 46+50.00 | 50.00 | 19.7 | | | 6.4 | 23.8 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBDL | 46+50.00 | | 47+00.00 | 50.00 | 19.7 | | | 6.9 | 25.9 | 2.3 | 8.5 | 2.0 | 7.5 |
| I-55 SBDL | 47+00.00 | | 47+50.00 | 50.00 | 19.7 | | | 7.4 | 27.6 | 2.2 | 8.4 | 2.0 | 7.5 |
| I-55 SBDL | 47+50.00 | | 48+00.00 | 50.00 | 19.7 | | | 7.9 | 29.4 | 2.3 | 8.4 | 2.0 | 7.5 |
| I-55 SBDL | 48+00.00 | | 48+50.00 | 50.00 | 19.7 | | | 7.9 | 29.7 | 2.3 | 8.4 | 2.0 | 7.5 |
| I-55 SBDL | 48+50.00 | | 49+00.00 | 50.00 | 19.7 | | | 8.1 | 30.4 | 2.3 | 8.4 | 2.0 | 7.5 |
| I-55 SBDL | 49+00.00 | | 49+50.00 | 50.00 | 19.6 | | | 8.4 | 31.3 | 2.3 | 8.4 | 2.0 | 7.5 |
| I-55 SBDL | 49+50.00 | | 50+00.0 | 50.00 | 19.6 | | | 8.9 | 33.1 | 2.3 | 8.4 | 2.0 | 7.5 |
| I-55 SBDL | 50+00.00 | | 50+50.00 | 50.00 | 19.7 | | | 9.3 | 34.8 | 2.3 | 8.4 | 2.0 | 7.5 |
| I-55 SBDL | 50+50.00 | | 50+80.66 | 30.66 | 9.5 | | | 9.4 | 21.5 | 2.3 | 5.2 | 2.0 | 4.6 |
| BRIDGE OMISSION S.N. 057-0254 | | | | | | | | | | | | | |
| I-55 SBDL | 53+50.16 | | 54+00.00 | 49.84 | 36.4 | | | 16.8 | 62.7 | 4.6 | 16.9 | 4.0 | 14.9 |
| I-55 SBDL | 54+00.00 | | 54+50.00 | 50.00 | 39.2 | | | 17.2 | 64.3 | 4.6 | 17.0 | 4.0 | 15.0 |
| I-55 SBDL | 54+50.00 | | 55+00.00 | 50.00 | 39.2 | | | 18.0 | 67.0 | 4.6 | 17.0 | 4.0 | 15.0 |
| I-55 SBDL | 55+00.00 | | 55+50.00 | 50.00 | 39.2 | | | 18.6 | 69.6 | 4.6 | 17.0 | 4.0 | 15.0 |
| I-55 SBDL | 55+50.00 | | 56+00.0 | 50.00 | 39.2 | | | 19.4 | 72.5 | 4.6 | 17.0 | 4.0 | 15.0 |
| I-55 SBDL | 56+00.00 | | 56+50.00 | 50.00 | 39.1 | | | 20.2 | 75.3 | 4.6 | 17.0 | 4.0 | 15.0 |
| I-55 SBDL | 56+50.00 | | 56+80.65 | 30.65 | 19.0 | | | 20.5 | 46.9 | 4.6 | 10.4 | 4.0 | 9.2 |

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 5 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | POLYMERIZED LEVELING BINDER (MACHINE METHOD) AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 9.5, FG, N90 40600909 (TON) | HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603092 (TON) | POLYMERIZED HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603243 (TON) | SURFACE AVERAGE END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 40603545 (TON) |
|-------------------------------|----------|----|----------|------------------|--------------------------------------------------------------|------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------|--------------------------------------------------------------------|----------------------------------------------------------------------------|--------------------------------------------------------------------------------|----------------------------------------------------------------------------------------|--------------------------------------------------------|-------------------------------------------------------------------------------------|
| BRIDGE OMISSION S.N. 057-0252 | | | | | | | | | | | | | |
| I-55 SBDL | 59+27.75 | | 59+50.00 | 22.25 | 12.4 | | | 18.0 | 30.0 | 4.5 | 7.5 | 4.0 | 6.7 |
| I-55 SBDL | 59+50.00 | | 60+00.00 | 50.00 | 39.2 | | | 17.9 | 66.9 | 4.5 | 16.9 | 4.0 | 15.0 |
| I-55 SBDL | 60+00.00 | | 60+50.00 | 50.00 | 19.7 | | | 8.1 | 30.1 | 2.3 | 8.4 | 2.0 | 7.5 |
| I-55 SBDL | 60+50.00 | | 61+00.00 | 50.00 | 19.7 | | | 6.3 | 23.4 | 2.3 | 8.5 | 2.0 | 7.5 |
| I-55 SBDL | 61+00.00 | | 61+50.00 | 50.00 | 19.7 | | | 4.2 | 15.7 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBDL | 61+50.00 | | 62+00.65 | 50.65 | 19.7 | | | 2.3 | 8.9 | 2.3 | 8.7 | 2.0 | 7.6 |
| I-55 SBDL | 62+00.65 | | 62+50.00 | 49.35 | 13.1 | | | | | 2.2 | 8.1 | 2.0 | 7.4 |
| I-55 SBDL | 62+50.00 | | 63+00.00 | 50.00 | 13.1 | | | | | 2.2 | 8.3 | 2.0 | 7.5 |
| I-55 SBDL | 63+00.00 | | 63+50.00 | 50.00 | 13.1 | | | | | 2.3 | 8.4 | 2.0 | 7.5 |
| I-55 SBDL | 63+50.00 | | 64+00.00 | 50.00 | 13.1 | | | | | 2.3 | 8.4 | 2.0 | 7.5 |
| I-55 SBDL | 64+00.00 | | 64+50.00 | 50.00 | 13.2 | | | | | 2.3 | 8.4 | 2.0 | 7.5 |
| I-55 SBDL | 64+50.00 | | 65+00.00 | 50.00 | 13.4 | | | | | 2.3 | 8.4 | 2.0 | 7.5 |
| SUB-TOTAL I-55 SBDL: | | | | | 1298.5 | | 203.8 | | 965.0 | | 341.2 | | 621.2 |
| I-55 SBPL | | | | | | | | | | | | | |
| I-55 SBPL | 29+00.00 | | 29+50.00 | 50.00 | 13.8 | 1.3 | 4.7 | | | | | 2.0 | 7.6 |
| I-55 SBPL | 29+50.00 | | 30+00.00 | 50.00 | 13.8 | 1.3 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 SBPL | 30+00.00 | | 30+50.00 | 50.00 | 14.0 | 1.2 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 SBPL | 30+50.00 | | 31+00.00 | 50.00 | 14.2 | 1.3 | 4.8 | | | | | 2.1 | 7.7 |
| I-55 SBPL | 31+00.00 | | 31+50.00 | 50.00 | 14.2 | 1.3 | 4.7 | | | | | 2.0 | 7.6 |
| I-55 SBPL | 31+50.00 | | 32+00.00 | 50.00 | 14.1 | 1.3 | 4.8 | | | | | 2.1 | 7.7 |
| I-55 SBPL | 32+00.00 | | 32+50.00 | 50.00 | 13.8 | 1.3 | 4.7 | | | | | 2.0 | 7.6 |
| I-55 SBPL | 32+50.00 | | 33+00.00 | 50.00 | 13.6 | 1.3 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 SBPL | 33+00.00 | | 33+50.00 | 50.00 | 13.7 | 1.3 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 SBPL | 33+50.00 | | 34+00.00 | 50.00 | 14.0 | 1.2 | 4.6 | | | | | 2.0 | 7.5 |
| I-55 SBPL | 34+00.00 | | 34+50.00 | 50.00 | 14.2 | 1.2 | 4.5 | | | | | 1.9 | 7.2 |
| I-55 SBPL | 34+50.00 | | 35+00.00 | 50.00 | 14.2 | 1.2 | 4.6 | | | | | 2.0 | 7.4 |
| I-55 SBPL | 35+00.00 | | 35+50.00 | 50.00 | 13.8 | 1.3 | 4.8 | | | | | 2.1 | 7.7 |
| I-55 SBPL | 35+50.00 | | 36+00.00 | 50.00 | 13.3 | 1.3 | 4.8 | | | | | 2.1 | 7.7 |
| I-55 SBPL | 36+00.00 | | 36+50.00 | 50.00 | 13.7 | 1.3 | 4.8 | | | | | 2.1 | 7.7 |
| I-55 SBPL | 36+50.00 | | 37+00.00 | 50.00 | 13.4 | 1.3 | 4.7 | | | | | 2.0 | 7.6 |
| I-55 SBPL | 37+00.00 | | 37+50.00 | 50.00 | 13.3 | 1.2 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 SBPL | 37+50.00 | | 38+00.00 | 50.00 | 13.5 | 1.3 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 SBPL | 38+00.00 | | 38+50.00 | 50.00 | 13.5 | 1.3 | 5.0 | | | | | 2.1 | 8.0 |
| I-55 SBPL | 38+50.00 | | 39+00.00 | 50.00 | 13.5 | 1.4 | 5.1 | | | | | 2.2 | 8.2 |
| I-55 SBPL | 39+00.00 | | 39+50.00 | 50.00 | 13.6 | 1.3 | 4.9 | | | | | 2.1 | 7.8 |
| I-55 SBPL | 39+50.00 | | 40+00.00 | 50.00 | 13.8 | 1.4 | 5.2 | | | | | 2.2 | 8.3 |
| I-55 SBPL | 40+00.00 | | 40+50.00 | 50.00 | 14.0 | 1.4 | 5.1 | | | | | 2.2 | 8.2 |
| I-55 SBPL | 40+50.00 | | 41+00.00 | 50.00 | 13.9 | 1.3 | 4.7 | | | | | 2.0 | 7.6 |
| I-55 SBPL | 41+00.00 | | 41+50.00 | 50.00 | 13.8 | 1.3 | 4.8 | | | | | 2.1 | 7.7 |
| I-55 SBPL | 41+50.00 | | 42+00.00 | 50.00 | 13.8 | 1.2 | 4.6 | | | | | 2.0 | 7.4 |
| I-55 SBPL | 42+00.00 | | 42+50.00 | 50.00 | 13.6 | 1.2 | 4.5 | | | | | 2.0 | 7.3 |
| I-55 SBPL | 42+50.00 | | 43+00.00 | 50.00 | 13.4 | 1.2 | 4.6 | | | | | 2.0 | 7.3 |
| I-55 SBPL | 43+00.00 | | 43+50.00 | 50.00 | 13.2 | 1.3 | 4.7 | | | | | 2.0 | 7.5 |
| I-55 SBPL | 43+50.00 | | 44+00.00 | 50.00 | 13.2 | 1.6 | 6.1 | | | | | 2.0 | 7.5 |
| I-55 SBPL | 44+00.00 | | 44+07.00 | 7.00 | 1.8 | 2.0 | 1.0 | | | | | 2.0 | 1.1 |

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 6 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | POLYMERIZED LEVELING BINDER (MACHINE METHOD) AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 9.5, FG, N90 40600909 (TON) | HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603092 (TON) | POLYMERIZED HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603243 (TON) | SURFACE AVERAGE END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 40603545 (TON) |
|----------------------|-------------------------------|----|----------|------------------|--------------------------------------------------------------|------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------|--------------------------------------------------------------------|----------------------------------------------------------------------------|--------------------------------------------------------------------------------|----------------------------------------------------------------------------------------|--------------------------------------------------------|-------------------------------------------------------------------------------------|
| I-55 SBPL | 44+07.00 | | 44+50.00 | 43.00 | 11.3 | | | | | 3.7 | 11.8 | 2.0 | 6.5 |
| I-55 SBPL | 44+50.00 | | 44+63.00 | 13.00 | 3.5 | | | | | 3.7 | 3.6 | 2.0 | 2.0 |
| I-55 SBPL | 44+63.00 | | 45+00.00 | 37.00 | 14.6 | | | 3.3 | 9.1 | 2.3 | 6.3 | 2.0 | 5.6 |
| I-55 SBPL | 45+00.00 | | 45+50.00 | 50.00 | 19.8 | | | 4.0 | 14.8 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 45+50.00 | | 46+00.00 | 50.00 | 19.8 | | | 5.1 | 19.2 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 46+00.00 | | 46+50.00 | 50.00 | 19.8 | | | 6.3 | 23.4 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 46+50.00 | | 47+00.00 | 50.00 | 19.8 | | | 6.8 | 25.4 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 47+00.00 | | 47+50.00 | 50.00 | 19.8 | | | 7.3 | 27.1 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 47+50.00 | | 48+00.00 | 50.00 | 19.8 | | | 7.8 | 29.0 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 48+00.00 | | 48+50.00 | 50.00 | 19.8 | | | 7.8 | 29.3 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 48+50.00 | | 49+00.00 | 50.00 | 19.8 | | | 8.3 | 31.0 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 49+00.00 | | 49+50.00 | 50.00 | 19.8 | | | 8.7 | 32.6 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 49+50.00 | | 50+00.00 | 50.00 | 19.8 | | | 9.4 | 35.0 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 50+00.00 | | 50+50.00 | 50.00 | 19.8 | | | 10.0 | 37.3 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 50+50.00 | | 50+80.66 | 30.66 | 12.4 | | | 10.1 | 23.1 | 2.3 | 5.3 | 2.0 | 4.6 |
| I-55 SBPL | BRIDGE OMISSION S.N. 057-0254 | | | | | | | | | | | | |
| I-55 SBPL | 53+50.16 | | 54+00.00 | 49.84 | 14.7 | | | 10.2 | 38.0 | 2.3 | 8.5 | 2.0 | 7.5 |
| I-55 SBPL | 54+00.00 | | 54+50.00 | 50.00 | 19.8 | | | 10.6 | 39.5 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 54+50.00 | | 55+00.00 | 50.00 | 19.8 | | | 10.8 | 40.4 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 55+00.00 | | 55+50.00 | 50.00 | 19.8 | | | 10.6 | 39.6 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 55+50.00 | | 56+00.00 | 50.00 | 19.8 | | | 10.6 | 39.6 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 56+00.00 | | 56+50.00 | 50.00 | 19.8 | | | 11.2 | 42.0 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 56+50.00 | | 56+80.65 | 30.65 | 10.5 | | | 11.8 | 27.0 | 2.3 | 5.3 | 2.0 | 4.6 |
| I-55 SBPL | BRIDGE OMISSION S.N. 057-0252 | | | | | | | | | | | | |
| I-55 SBPL | 59+27.75 | | 59+50.00 | 22.25 | 6.2 | | | 10.0 | 16.7 | 2.3 | 3.8 | 2.0 | 3.3 |
| I-55 SBPL | 59+50.00 | | 60+00.00 | 50.00 | 19.8 | | | 9.7 | 36.4 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 60+00.00 | | 60+50.00 | 50.00 | 19.8 | | | 8.7 | 32.5 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 60+50.00 | | 61+00.00 | 50.00 | 19.8 | | | 6.9 | 25.7 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 61+00.00 | | 61+50.00 | 50.00 | 19.8 | | | 4.8 | 17.9 | 2.3 | 8.6 | 2.0 | 7.5 |
| I-55 SBPL | 61+50.00 | | 62+00.65 | 50.65 | 19.8 | | | 3.0 | 11.5 | 2.3 | 8.7 | 2.0 | 7.6 |
| I-55 SBPL | 62+00.65 | | 62+50.00 | 49.35 | 13.2 | | | | | 2.5 | 9.0 | 2.0 | 7.4 |
| I-55 SBPL | 62+50.00 | | 63+00.00 | 50.00 | 13.2 | | | | | 2.4 | 9.1 | 2.0 | 7.5 |
| I-55 SBPL | 63+00.00 | | 63+50.00 | 50.00 | 13.2 | | | | | 2.3 | 8.4 | 2.0 | 7.5 |
| I-55 SBPL | 63+50.00 | | 64+00.00 | 50.00 | 13.2 | | | | | 2.3 | 8.4 | 2.0 | 7.5 |
| I-55 SBPL | 64+00.00 | | 64+50.00 | 50.00 | 13.1 | | | | | 2.3 | 8.4 | 2.0 | 7.5 |
| I-55 SBPL | 64+50.00 | | 65+00.00 | 50.00 | 13.0 | | | | | 2.3 | 8.4 | 2.0 | 7.5 |
| SUB-TOTAL I-55 SBPL: | | | | | 981.5 | | 144.8 | | 743.0 | | 276.6 | | 467.0 |
| SUB-TOTAL: | | | | | 4393.3 | | 587.8 | | 3778.2 | | 1276.7 | | 2033.6 |

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 7 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | *AGGREGATE SHOULDER AVG END AREA FROM CADD (SQ FT) | AGGREGATE SHOULDER AVG WIDTH (FOOT) | AGGREGATE SHOULDER, TYPE A 6" 48100500 (SQ YD) | *AGGREGATE SHOULDER (TON) | HOT-MIX ASPHALT SHOULDER AVERAGE END AREA FROM CADD (SQ FT) | HOT-MIX ASPHALT SHOULDERS 48203100 (TON) |
|-------------------------------|----------|----|----------|------------------|-----------------------------------------------------------|-------------------------------------------------------------|----------------------------------------------|---------------------------------------------------------|---------------------------------|----------------------------------------------------------------------|---------------------------------------------------|
| I-55 NBDL | 30+50.00 | | 31+00.00 | 50.00 | | 1.1 | 2.0 | 11.1 | 3.8 | | |
| I-55 NBDL | 31+00.00 | | 31+50.00 | 50.00 | | 1.1 | 2.0 | 11.1 | 3.8 | | |
| I-55 NBDL | 31+50.00 | | 32+00.00 | 50.00 | | 1.1 | 2.0 | 11.1 | 3.8 | | |
| I-55 NBDL | 32+00.00 | | 32+50.00 | 50.00 | | 1.1 | 2.0 | 11.1 | 3.8 | | |
| I-55 NBDL | 32+50.00 | | 32+60.00 | 10.00 | | 1.1 | 2.0 | 2.2 | 0.8 | | |
| I-55 NBDL | 32+60.00 | | 33+00.00 | 40.00 | 4.1 | 1.1 | 2.0 | 8.9 | 3.0 | 1.5 | 4.3 |
| I-55 NBDL | 33+00.00 | | 33+50.00 | 50.00 | 5.3 | 1.1 | 2.0 | 11.1 | 3.8 | 1.5 | 5.4 |
| I-55 NBDL | 33+50.00 | | 34+00.00 | 50.00 | 5.3 | 1.1 | 2.0 | 11.1 | 3.8 | 1.4 | 5.2 |
| I-55 NBDL | 34+00.00 | | 34+50.00 | 50.00 | 5.2 | 1.1 | 2.0 | 11.1 | 3.7 | 1.5 | 5.6 |
| I-55 NBDL | 34+50.00 | | 35+00.00 | 50.00 | 5.2 | 1.1 | 2.0 | 11.1 | 3.7 | 1.5 | 5.7 |
| I-55 NBDL | 35+00.00 | | 35+50.00 | 50.00 | 5.5 | 1.1 | 2.0 | 11.1 | 3.8 | 1.4 | 5.1 |
| I-55 NBDL | 35+50.00 | | 36+00.00 | 50.00 | 5.5 | 1.1 | 2.0 | 11.1 | 3.8 | 1.4 | 5.1 |
| I-55 NBDL | 36+00.00 | | 36+50.00 | 50.00 | 5.5 | 1.1 | 2.0 | 11.1 | 3.8 | 1.4 | 5.1 |
| I-55 NBDL | 36+50.00 | | 37+00.00 | 50.00 | 5.5 | 1.1 | 2.0 | 11.1 | 3.8 | 1.3 | 4.9 |
| I-55 NBDL | 37+00.00 | | 37+50.00 | 50.00 | 5.4 | 1.1 | 2.0 | 11.1 | 3.7 | 1.4 | 5.2 |
| I-55 NBDL | 37+50.00 | | 38+00.00 | 50.00 | 5.1 | 1.1 | 2.0 | 11.1 | 3.7 | 1.5 | 5.6 |
| I-55 NBDL | 38+00.00 | | 38+50.00 | 50.00 | 4.3 | 1.1 | 2.0 | 11.1 | 3.7 | 1.1 | 4.1 |
| I-55 NBDL | 38+50.00 | | 39+00.00 | 50.00 | 3.6 | 1.1 | 2.0 | 11.1 | 3.8 | 0.7 | 2.6 |
| I-55 NBDL | 39+00.00 | | 39+50.00 | 50.00 | 2.5 | 1.1 | 2.0 | 11.1 | 3.8 | 0.7 | 2.8 |
| I-55 NBDL | 39+50.00 | | 40+00.00 | 50.00 | 1.9 | 1.1 | 2.0 | 11.1 | 3.8 | 0.9 | 3.4 |
| I-55 NBDL | 40+00.00 | | 40+50.00 | 50.00 | 2.7 | 1.1 | 2.0 | 11.1 | 3.8 | 1.0 | 3.6 |
| I-55 NBDL | 40+50.00 | | 41+00.00 | 50.00 | 3.0 | 1.1 | 2.0 | 11.1 | 3.8 | 0.7 | 2.8 |
| I-55 NBDL | 41+00.00 | | 41+50.00 | 50.00 | 2.6 | 1.1 | 2.0 | 11.1 | 3.8 | 0.7 | 2.8 |
| I-55 NBDL | 41+50.00 | | 42+00.00 | 50.00 | 2.8 | 1.1 | 2.0 | 11.1 | 3.8 | 1.1 | 4.1 |
| I-55 NBDL | 44+73.42 | | 45+00.00 | 26.58 | 0.6 | | | | | 1.2 | 2.3 |
| I-55 NBDL | 45+00.00 | | 45+50.00 | 50.00 | 1.5 | | | | | 1.5 | 5.8 |
| I-55 NBDL | 45+50.00 | | 46+00.00 | 50.00 | 2.1 | | | | | 2.6 | 9.9 |
| I-55 NBDL | 46+00.00 | | 46+50.00 | 50.00 | 3.1 | | | | | 5.0 | 18.5 |
| I-55 NBDL | 46+50.00 | | 47+00.00 | 50.00 | 4.8 | | | | | 9.1 | 33.9 |
| I-55 NBDL | 47+00.00 | | 47+50.42 | 50.42 | 7.4 | | | | | 16.8 | 63.3 |
| I-55 NBDL | 47+50.42 | | 48+00.00 | 49.58 | 5.6 | 1.5 | 2.0 | 11.0 | 4.8 | 14.0 | 51.8 |
| I-55 NBDL | 48+00.00 | | 48+50.00 | 50.00 | 5.7 | 1.5 | 2.0 | 11.1 | 4.8 | 13.0 | 48.6 |
| I-55 NBDL | 48+50.00 | | 49+00.00 | 50.00 | 5.3 | 1.2 | 2.0 | 11.1 | 4.1 | 11.5 | 43.1 |
| I-55 NBDL | 49+00.00 | | 49+50.00 | 50.00 | 4.7 | 1.0 | 2.0 | 11.1 | 3.3 | 10.7 | 39.9 |
| I-55 NBDL | 49+50.00 | | 50+00.00 | 50.00 | 4.1 | 1.0 | 2.0 | 11.1 | 3.3 | 10.2 | 38.2 |
| I-55 NBDL | 50+00.00 | | 50+50.00 | 50.00 | 3.5 | 1.0 | 2.0 | 11.1 | 3.3 | 10.3 | 38.4 |
| I-55 NBDL | 50+50.00 | | 51+00.00 | 50.00 | 3.4 | 1.0 | 2.0 | 11.1 | 3.3 | 10.7 | 40.1 |
| I-55 NBDL | 51+00.00 | | 51+29.38 | 29.38 | 2.0 | 1.0 | 2.0 | 6.5 | 2.0 | 11.1 | 24.3 |
| I-55 NBDL | | | | | | | | | | | |
| BRIDGE OMISSION S.N. 057-0253 | | | | | | | | | | | |
| I-55 NBDL | 53+89.92 | | 54+00.00 | 10.08 | 0.1 | 1.0 | 2.0 | 2.2 | 0.7 | 9.4 | 7.1 |
| I-55 NBDL | 54+00.00 | | 54+50.00 | 50.00 | 3.4 | 1.0 | 2.0 | 11.1 | 3.3 | 9.4 | 35.1 |
| I-55 NBDL | 54+50.00 | | 55+00.00 | 50.00 | 3.4 | 1.0 | 2.0 | 11.1 | 3.3 | 9.4 | 35.1 |
| I-55 NBDL | 55+00.00 | | 55+50.00 | 50.00 | 3.4 | 1.0 | 2.0 | 11.1 | 3.3 | 9.5 | 35.4 |
| I-55 NBDL | 55+50.00 | | 56+00.00 | 50.00 | 3.4 | 1.0 | 2.0 | 11.1 | 3.3 | 9.6 | 35.8 |
| I-55 NBDL | 56+00.00 | | 56+50.00 | 50.00 | 3.4 | 1.0 | 2.0 | 11.1 | 3.3 | 9.9 | 37.1 |
| I-55 NBDL | 56+50.00 | | 56+91.55 | 41.55 | 2.6 | 1.0 | 2.0 | 9.2 | 2.8 | 10.3 | 31.8 |
| I-55 NBDL | | | | | | | | | | | |
| BRIDGE OMISSION S.N. 057-0251 | | | | | | | | | | | |
| I-55 NBDL | 59+29.99 | | 59+52.11 | 22.12 | 1.2 | | | | | 7.4 | 12.2 |
| I-55 NBDL | 59+52.11 | | 60+02.87 | 50.76 | 3.9 | | | | | 7.1 | 26.8 |
| I-55 NBDL | 60+02.87 | | 60+53.72 | 50.85 | 4.7 | | | | | 9.3 | 35.1 |
| I-55 NBDL | 60+53.72 | | 60+88.35 | 34.63 | 5.0 | | | | | 11.7 | 30.4 |
| I-55 NBDL | 60+88.35 | | 61+04.72 | 16.37 | 1.9 | 2.9 | 4.1 | 7.5 | 3.1 | 9.5 | 11.7 |
| I-55 NBDL | 61+04.72 | | 61+08.83 | 4.11 | 0.3 | 2.9 | 6.3 | 2.9 | 0.8 | 9.5 | 2.9 |
| I-55 NBDL | 61+08.83 | | 61+55.92 | 47.09 | 5.4 | 1.5 | 2.0 | 10.5 | 4.6 | 9.0 | 31.5 |
| I-55 NBDL | 61+55.92 | | 62+07.36 | 51.44 | 5.8 | 1.5 | 2.0 | 11.4 | 5.0 | 7.8 | 30.0 |
| I-55 NBDL | 62+07.36 | | 62+59.10 | 51.74 | 5.8 | 1.5 | 2.0 | 11.5 | 5.1 | 6.7 | 25.8 |
| I-55 NBDL | 62+59.10 | | 63+11.18 | 52.08 | 5.8 | 1.4 | 2.0 | 11.6 | 4.9 | 5.4 | 21.2 |
| I-55 NBDL | 63+11.18 | | 63+63.67 | 52.49 | 5.9 | 1.3 | 2.0 | 11.7 | 4.4 | 4.1 | 16.0 |
| I-55 NBDL | 63+63.67 | | 64+16.61 | 52.94 | 5.9 | 1.0 | 2.0 | 11.8 | 3.6 | 2.7 | 10.6 |
| I-55 NBDL | 64+16.61 | | 64+70.03 | 53.42 | 5.9 | 1.2 | 2.0 | 11.9 | 4.3 | 1.7 | 6.8 |
| I-55 NBDL | 64+70.03 | | 65+00.00 | 29.97 | 2.8 | 1.5 | 2.0 | 6.7 | 3.0 | 1.5 | 3.3 |
| SUB TOTALS NBDL: | | | | | 214.9 | | | 504.1 | 174.5 | | 1023.3 |

*SHOWN FOR INFORMATION ONLY

| | | | | | | | | | | | |
|------------------------------------------------------------|----------------------|------------|-----------|-----------------------------------------------------------|--------------------------------|--------------------|-----------------------|--------|--------------|---------------------------|--|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SCHEDULES OF QUANTITIES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| c:\pw\work\p\dot\detersbj\0157116\0570570-sht-schedule.dgn | | DRAWN - | REVISED - | | | 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 42 | |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - | | | CONTRACT NO. 70570 | | | | | |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - | | | SCALE: | SHEET 10 OF 27 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT | |

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 8 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | *AGGREGATE SHOULDER AVG END AREA FROM CADD (SQ FT) | AGGREGATE SHOULDER AVG WIDTH (FOOT) | AGGREGATE SHOULDER, TYPE A 6" 48100500 (SQ YD) | *AGGREGATE SHOULDER (TON) | HOT-MIX ASPHALT SHOULDER AVERAGE END AREA FROM CADD (SQ FT) | HOT-MIX ASPHALT SHOULDERS 48203100 (TON) |
|------------------|-------------------------------|----|----------|---------------|--------------------------------------------------|----------------------------------------------------|-------------------------------------|------------------------------------------------|---------------------------|-------------------------------------------------------------|------------------------------------------|
| I-55 NBPL | 32+60.00 | | 33+00.00 | 40.00 | 2.5 | 1.6 | 2.0 | 8.9 | 4.3 | 1.0 | 2.9 |
| I-55 NBPL | 33+00.00 | | 33+50.00 | 50.00 | 3.1 | 1.6 | 2.0 | 11.1 | 5.3 | 0.9 | 3.2 |
| I-55 NBPL | 33+50.00 | | 34+00.00 | 50.00 | 3.0 | 1.6 | 2.0 | 11.1 | 5.3 | 0.8 | 2.8 |
| I-55 NBPL | 34+00.00 | | 34+50.00 | 50.00 | 2.9 | 1.6 | 2.0 | 11.1 | 5.3 | 0.8 | 2.9 |
| I-55 NBPL | 34+50.00 | | 35+00.00 | 50.00 | 2.8 | 1.6 | 2.0 | 11.1 | 5.3 | 0.8 | 2.9 |
| I-55 NBPL | 35+00.00 | | 35+50.00 | 50.00 | 2.8 | 1.6 | 2.0 | 11.1 | 5.3 | 0.7 | 2.8 |
| I-55 NBPL | 35+50.00 | | 36+00.00 | 50.00 | 2.9 | 1.4 | 2.0 | 11.1 | 4.5 | 0.7 | 2.8 |
| I-55 NBPL | 36+00.00 | | 36+50.00 | 50.00 | 2.9 | 1.1 | 2.0 | 11.1 | 3.8 | 0.7 | 2.7 |
| I-55 NBPL | 36+50.00 | | 37+00.00 | 50.00 | 2.7 | 1.1 | 2.0 | 11.1 | 3.8 | 0.7 | 2.7 |
| I-55 NBPL | 37+00.00 | | 37+50.00 | 50.00 | 2.7 | 1.1 | 2.0 | 11.1 | 3.8 | 0.7 | 2.7 |
| I-55 NBPL | 37+50.00 | | 38+00.00 | 50.00 | 2.8 | 1.4 | 2.0 | 11.1 | 4.5 | 0.7 | 2.7 |
| I-55 NBPL | 38+00.00 | | 38+50.00 | 50.00 | 2.8 | 1.6 | 2.0 | 11.1 | 5.3 | 0.8 | 2.8 |
| I-55 NBPL | 38+50.00 | | 39+00.00 | 50.00 | 2.9 | 1.6 | 2.0 | 11.1 | 5.3 | 0.7 | 2.7 |
| I-55 NBPL | 39+00.00 | | 39+50.00 | 50.00 | 3.0 | 1.6 | 2.0 | 11.1 | 5.3 | 0.7 | 2.5 |
| I-55 NBPL | 39+50.00 | | 40+00.00 | 50.00 | 3.0 | 1.6 | 2.0 | 11.1 | 5.4 | 0.7 | 2.5 |
| I-55 NBPL | 40+00.00 | | 40+50.00 | 50.00 | 2.8 | 1.6 | 2.0 | 11.1 | 5.4 | 0.7 | 2.6 |
| I-55 NBPL | 40+50.00 | | 41+00.00 | 50.00 | 2.7 | 1.6 | 2.0 | 11.1 | 5.3 | 0.7 | 2.5 |
| I-55 NBPL | 41+00.00 | | 41+50.00 | 50.00 | 2.7 | 1.6 | 2.0 | 11.1 | 5.3 | 0.7 | 2.5 |
| I-55 NBPL | 41+50.00 | | 42+00.00 | 50.00 | 2.8 | 1.5 | 2.0 | 11.1 | 5.0 | 0.7 | 2.6 |
| I-55 NBPL | 42+00.00 | | 42+50.00 | 50.00 | 3.0 | 1.5 | 2.0 | 11.1 | 4.9 | 0.8 | 3.0 |
| I-55 NBPL | 42+50.00 | | 43+00.00 | 50.00 | 3.2 | 1.5 | 2.0 | 11.1 | 5.1 | 1.0 | 3.7 |
| I-55 NBPL | 43+00.00 | | 43+50.00 | 50.00 | 3.4 | 1.4 | 2.0 | 11.1 | 4.7 | 1.5 | 5.5 |
| I-55 NBPL | 43+50.00 | | 44+00.00 | 50.00 | 3.4 | 1.4 | 2.0 | 11.1 | 4.6 | 2.4 | 8.9 |
| I-55 NBPL | 44+00.00 | | 44+50.00 | 50.00 | 3.4 | 1.5 | 2.0 | 11.1 | 4.9 | 3.1 | 11.7 |
| I-55 NBPL | 44+50.00 | | 45+00.00 | 50.00 | 3.4 | 1.5 | 2.0 | 11.1 | 4.9 | 3.7 | 13.7 |
| I-55 NBPL | 45+00.00 | | 45+50.00 | 50.00 | 3.4 | 1.5 | 2.0 | 11.1 | 4.9 | 4.2 | 15.7 |
| I-55 NBPL | 45+50.00 | | 46+00.00 | 50.00 | 3.4 | 1.5 | 2.0 | 11.1 | 4.9 | 4.8 | 18.0 |
| I-55 NBPL | 46+00.00 | | 46+50.00 | 50.00 | 3.4 | 1.5 | 2.0 | 11.1 | 4.9 | 5.4 | 20.3 |
| I-55 NBPL | 46+50.00 | | 47+00.00 | 50.00 | 3.4 | 1.5 | 2.0 | 11.1 | 4.9 | 5.9 | 22.0 |
| I-55 NBPL | 47+00.00 | | 47+50.00 | 50.00 | 3.4 | 1.5 | 2.0 | 11.1 | 4.9 | 6.3 | 23.4 |
| I-55 NBPL | 47+50.00 | | 48+00.00 | 50.00 | 3.4 | 1.5 | 2.0 | 11.1 | 4.9 | 6.5 | 24.1 |
| I-55 NBPL | 48+00.00 | | 48+50.00 | 50.00 | 3.4 | 1.5 | 2.0 | 11.1 | 4.9 | 6.3 | 23.6 |
| I-55 NBPL | 48+50.00 | | 49+00.00 | 50.00 | 3.4 | 1.5 | 2.0 | 11.1 | 4.9 | 6.1 | 22.9 |
| I-55 NBPL | 49+00.00 | | 49+50.00 | 50.00 | 3.4 | 1.5 | 2.0 | 11.1 | 4.9 | 6.1 | 22.7 |
| I-55 NBPL | 49+50.00 | | 50+00.00 | 50.00 | 3.4 | 1.5 | 2.0 | 11.1 | 4.9 | 6.0 | 22.5 |
| I-55 NBPL | 50+00.00 | | 50+50.00 | 50.00 | 3.4 | 1.5 | 2.0 | 11.1 | 4.9 | 5.9 | 22.1 |
| I-55 NBPL | 50+50.00 | | 51+00.00 | 50.00 | 3.4 | 1.5 | 2.0 | 11.1 | 4.9 | 5.9 | 22.1 |
| I-55 NBPL | 51+00.00 | | 51+29.38 | 29.38 | 0.7 | 1.5 | 2.0 | 6.5 | 2.9 | 6.0 | 13.1 |
| I-55 NBPL | BRIDGE OMISSION S.N. 057-0253 | | | | | | | | | | |
| I-55 NBPL | 53+89.92 | | 54+00.00 | 10.08 | 1.0 | 1.0 | 2.0 | 2.2 | 0.7 | 6.4 | 4.8 |
| I-55 NBPL | 54+00.00 | | 54+50.00 | 50.00 | 3.4 | 1.0 | 2.0 | 11.1 | 3.3 | 6.4 | 24.1 |
| I-55 NBPL | 54+50.00 | | 55+00.00 | 50.00 | 3.4 | 1.0 | 2.0 | 11.1 | 3.3 | 6.6 | 24.5 |
| I-55 NBPL | 55+00.00 | | 55+50.00 | 50.00 | 3.4 | 1.0 | 2.0 | 11.1 | 3.3 | 6.6 | 24.6 |
| I-55 NBPL | 55+50.00 | | 56+00.00 | 50.00 | 3.4 | 1.0 | 2.0 | 11.1 | 3.3 | 6.4 | 23.8 |
| I-55 NBPL | 56+00.00 | | 56+50.00 | 50.00 | 3.4 | 1.0 | 2.0 | 11.1 | 3.3 | 6.1 | 22.7 |
| I-55 NBPL | 56+50.00 | | 56+91.55 | 41.55 | 2.1 | 1.0 | 2.0 | 9.2 | 2.8 | 5.9 | 18.4 |
| I-55 NBPL | BRIDGE OMISSION S.N. 057-0251 | | | | | | | | | | |
| I-55 NBPL | 59+29.99 | | 59+52.11 | 22.12 | 1.0 | 1.0 | 2.0 | 4.9 | 1.5 | 6.6 | 11.0 |
| I-55 NBPL | 59+52.11 | | 60+02.87 | 50.76 | 3.4 | 1.0 | 2.0 | 11.3 | 3.4 | 6.8 | 25.6 |
| I-55 NBPL | 60+02.87 | | 60+53.72 | 50.85 | 3.4 | 1.0 | 2.0 | 11.3 | 3.4 | 6.6 | 25.2 |
| I-55 NBPL | 60+53.72 | | 61+04.72 | 51.00 | 3.4 | 1.0 | 2.0 | 11.3 | 3.4 | 6.1 | 23.1 |
| I-55 NBPL | 61+04.72 | | 61+55.92 | 51.20 | 3.4 | 1.2 | 2.0 | 11.4 | 4.2 | 5.4 | 20.7 |
| I-55 NBPL | 61+55.92 | | 62+07.36 | 51.44 | 3.4 | 1.5 | 2.0 | 11.4 | 5.0 | 4.7 | 17.9 |
| I-55 NBPL | 62+07.36 | | 62+59.10 | 51.74 | 3.4 | 1.5 | 2.0 | 11.5 | 5.1 | 3.9 | 15.2 |
| I-55 NBPL | 62+59.10 | | 63+11.18 | 52.08 | 3.5 | 1.4 | 2.0 | 11.6 | 5.0 | 3.1 | 12.2 |
| I-55 NBPL | 63+11.18 | | 63+63.67 | 52.49 | 3.5 | 1.3 | 2.0 | 11.7 | 4.6 | 2.3 | 8.8 |
| I-55 NBPL | 63+63.67 | | 64+16.61 | 52.94 | 3.5 | 1.1 | 2.0 | 11.8 | 3.9 | 1.3 | 5.2 |
| I-55 NBPL | 64+16.61 | | 64+70.03 | 53.42 | 3.3 | 1.0 | 2.0 | 11.9 | 3.4 | 0.8 | 3.3 |
| I-55 NBPL | 64+70.03 | | 65+00.00 | 29.97 | 2.0 | 1.0 | 2.0 | 6.7 | 1.9 | 0.8 | 1.8 |
| SUB TOTALS NBPL: | | | | | 172.4 | | | 609.1 | 249.6 | | 683.7 |

*SHOWN FOR INFORMATION ONLY

| | | | | | | | | | | |
|-------------------------------------------------------------|-----------------------------|------------|-----------|-----------------------------------------------------------|--------------------------------|--------------------|-----------------------|--------|---------------------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SCHEDULES OF QUANTITIES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pw\work\p\dot\detersbj\0157116\0570570-sht-schedule.dgn | DRAWN - | REVISED - | 704 | | | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 43 | |
| *MODELNAME* | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | | CONTRACT NO. 70570 | | | ILLINOIS FED. AID PROJECT | |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - | | | SCALE: | SHEET 11 OF 27 SHEETS | STA. | TO STA. | |

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 9 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | *AGGREGATE SHOULDER AVG END AREA FROM CADD (SQ FT) | AGGREGATE SHOULDER AVG WIDTH (FOOT) | AGGREGATE SHOULDER, TYPE A 6" 48100500 (SQ YD) | *AGGREGATE SHOULDER (TON) | HOT-MIX ASPHALT SHOULDER AVERAGE END AREA FROM CADD (SQ FT) | HOT-MIX ASPHALT SHOULDERS 48203100 (TON) |
|-----------------------------------------------|----------|----|----------|------------------|-----------------------------------------------------------|-------------------------------------------------------------|----------------------------------------------|---------------------------------------------------------|---------------------------------|----------------------------------------------------------------------|---------------------------------------------------|
| I-55 SBDL | 29+00.00 | | 29+50.00 | 50.00 | 5.6 | 1.1 | 2.0 | 11.1 | 3.8 | 1.4 | 5.1 |
| I-55 SBDL | 29+50.00 | | 30+00.00 | 50.00 | 5.4 | 1.1 | 2.0 | 11.1 | 3.8 | 1.2 | 4.4 |
| I-55 SBDL | 30+00.00 | | 30+50.00 | 50.00 | 4.5 | 1.1 | 2.0 | 11.1 | 3.8 | 1.1 | 4.2 |
| I-55 SBDL | 30+50.00 | | 31+00.00 | 50.00 | 4.1 | 1.1 | 2.0 | 11.1 | 3.8 | 1.1 | 4.1 |
| I-55 SBDL | 31+00.00 | | 31+50.00 | 50.00 | 4.1 | 1.1 | 2.0 | 11.1 | 3.8 | 1.2 | 4.4 |
| I-55 SBDL | 31+50.00 | | 32+00.00 | 50.00 | 4.4 | 1.1 | 2.0 | 11.1 | 3.8 | 1.3 | 4.8 |
| I-55 SBDL | 32+00.00 | | 32+50.00 | 50.00 | 4.1 | 1.1 | 2.0 | 11.1 | 3.8 | 1.2 | 4.5 |
| I-55 SBDL | 32+50.00 | | 33+00.00 | 50.00 | 3.5 | 1.1 | 2.0 | 11.1 | 3.8 | 1.1 | 4.3 |
| I-55 SBDL | 33+00.00 | | 33+50.00 | 50.00 | 3.4 | 1.1 | 2.0 | 11.1 | 3.8 | 1.0 | 3.8 |
| I-55 SBDL | 33+50.00 | | 34+00.00 | 50.00 | 3.5 | 1.1 | 2.0 | 11.1 | 3.8 | 0.9 | 3.4 |
| I-55 SBDL | 34+00.00 | | 34+50.00 | 50.00 | 3.5 | 1.1 | 2.0 | 11.1 | 3.8 | 0.9 | 3.2 |
| I-55 SBDL | 34+50.00 | | 35+00.00 | 50.00 | 3.5 | 1.1 | 2.0 | 11.1 | 3.8 | 0.8 | 3.0 |
| I-55 SBDL | 35+00.00 | | 35+50.00 | 50.00 | 3.4 | 1.1 | 2.0 | 11.1 | 3.8 | 0.9 | 3.3 |
| I-55 SBDL | 35+50.00 | | 36+00.00 | 50.00 | 3.4 | 1.1 | 2.0 | 11.1 | 3.8 | 1.0 | 3.6 |
| I-55 SBDL | 36+00.00 | | 36+50.00 | 50.00 | 3.3 | 1.1 | 2.0 | 11.1 | 3.8 | 1.0 | 3.7 |
| I-55 SBDL | 36+50.00 | | 37+00.00 | 50.00 | 3.4 | 1.1 | 2.0 | 11.1 | 3.8 | 1.0 | 3.8 |
| I-55 SBDL | 37+00.00 | | 37+34.68 | 34.68 | 2.2 | 1.1 | 2.0 | 7.7 | 2.6 | 1.0 | 2.6 |
| I-55 SBDL | 37+34.68 | | 37+50.00 | 15.32 | 2.0 | 1.7 | 2.0 | 3.4 | 1.7 | 1.6 | 1.8 |
| I-55 SBDL | 37+50.00 | | 38+00.00 | 50.00 | 6.1 | 1.6 | 2.0 | 11.1 | 5.5 | 1.5 | 5.7 |
| I-55 SBDL | 38+00.00 | | 38+50.00 | 50.00 | 5.4 | 1.6 | 2.0 | 11.1 | 5.4 | 1.4 | 5.0 |
| I-55 SBDL | 38+50.00 | | 39+00.00 | 50.00 | 5.3 | 1.6 | 2.0 | 11.1 | 5.4 | 1.3 | 4.9 |
| I-55 SBDL | 39+00.00 | | 39+50.00 | 50.00 | 5.4 | 1.6 | 2.0 | 11.1 | 5.5 | 1.4 | 5.4 |
| I-55 SBDL | 39+50.00 | | 40+00.00 | 50.00 | 5.3 | 1.6 | 2.0 | 11.1 | 5.4 | 1.5 | 5.7 |
| I-55 SBDL | 40+00.00 | | 40+50.00 | 50.00 | 5.3 | 1.6 | 2.0 | 11.1 | 5.4 | 1.5 | 5.8 |
| I-55 SBDL | 40+50.00 | | 41+00.00 | 50.00 | 5.2 | 1.6 | 2.0 | 11.1 | 5.4 | 1.5 | 5.5 |
| I-55 SBDL | 41+00.00 | | 41+50.00 | 50.00 | 5.2 | 1.7 | 2.0 | 11.1 | 5.5 | 1.5 | 5.5 |
| I-55 SBDL | 41+50.00 | | 42+00.00 | 50.00 | 5.2 | 1.7 | 2.0 | 11.1 | 5.5 | 1.5 | 5.6 |
| I-55 SBDL | 42+00.00 | | 42+50.00 | 50.00 | 5.2 | 1.6 | 2.0 | 11.1 | 5.4 | 1.7 | 6.4 |
| I-55 SBDL | 42+50.00 | | 43+00.00 | 50.00 | 5.4 | 1.6 | 2.0 | 11.1 | 5.4 | 1.9 | 7.1 |
| I-55 SBDL | 43+00.00 | | 43+50.00 | 50.00 | 5.4 | 1.6 | 2.0 | 11.1 | 5.4 | 2.1 | 7.7 |
| I-55 SBDL | 43+50.00 | | 44+00.00 | 50.00 | 5.4 | 1.6 | 2.0 | 11.1 | 5.4 | 2.9 | 10.7 |
| I-55 SBDL | 44+00.00 | | 44+50.00 | 50.00 | 5.4 | 1.6 | 2.0 | 11.1 | 5.4 | 4.0 | 14.9 |
| I-55 SBDL | 44+50.00 | | 45+00.00 | 50.00 | 5.4 | 1.5 | 2.0 | 11.1 | 5.1 | 5.2 | 19.4 |
| I-55 SBDL | 45+00.00 | | 45+25.82 | 25.82 | 2.8 | 1.5 | 2.0 | 5.7 | 2.5 | 5.9 | 11.4 |
| I-55 SBDL | 45+25.82 | | 45+50.00 | 24.18 | 3.5 | | | | | 7.7 | 13.8 |
| I-55 SBDL | 45+50.00 | | 46+00.00 | 50.00 | 4.0 | | | | | 5.3 | 19.9 |
| I-55 SBDL | 46+00.00 | | 46+50.00 | 50.00 | 2.0 | | | | | 2.3 | 8.7 |
| I-55 SBDL | 46+50.00 | | 46+74.12 | 24.12 | 0.6 | | | | | 1.7 | 3.0 |
| I-55 SBDL | | | | | | | | | | | |
| BRIDGE OMISSION S.N. 057-0254/RAMP E OMISSION | | | | | | | | | | | |
| I-55 SBDL | 53+50.16 | | 54+00.00 | 49.84 | 2.9 | 1.0 | 2.0 | 11.1 | 3.3 | 6.0 | 22.2 |
| I-55 SBDL | 54+00.00 | | 54+50.00 | 50.00 | 3.2 | 1.0 | 2.0 | 11.1 | 3.3 | 6.2 | 23.1 |
| I-55 SBDL | 54+50.00 | | 55+00.00 | 50.00 | 3.2 | 1.0 | 2.0 | 11.1 | 3.3 | 6.3 | 23.6 |
| I-55 SBDL | 55+00.00 | | 55+50.00 | 50.00 | 3.2 | 1.0 | 2.0 | 11.1 | 3.3 | 6.5 | 24.4 |
| I-55 SBDL | 55+50.00 | | 56+00.00 | 50.00 | 3.2 | 1.0 | 2.0 | 11.1 | 3.3 | 6.9 | 25.7 |
| I-55 SBDL | 56+00.00 | | 56+50.00 | 50.00 | 3.2 | 1.0 | 2.0 | 11.1 | 3.3 | 6.9 | 25.8 |
| I-55 SBDL | 56+50.00 | | 56+80.65 | 30.65 | 1.4 | 1.0 | 2.0 | 6.8 | 2.0 | 6.8 | 15.6 |
| I-55 SBDL | | | | | | | | | | | |
| BRIDGE OMISSION S.N. 057-0252 | | | | | | | | | | | |
| I-55 SBDL | 59+27.75 | | 59+50.00 | 22.25 | 0.7 | 1.0 | 2.0 | 4.9 | 1.5 | 6.5 | 10.8 |
| I-55 SBDL | 59+50.00 | | 59+91.48 | 41.48 | 2.5 | 1.0 | 2.0 | 9.2 | 2.8 | 6.5 | 20.1 |
| I-55 SBDL | 60+86.05 | | 61+00.00 | 13.95 | 0.2 | | | | | 1.2 | 1.2 |
| I-55 SBDL | 61+00.00 | | 61+50.00 | 50.00 | 3.0 | | | | | 3.4 | 12.9 |
| I-55 SBDL | 61+50.00 | | 62+00.00 | 50.00 | 7.2 | | | | | 9.1 | 34.1 |
| I-55 SBDL | 62+00.00 | | 62+11.32 | 11.32 | 2.3 | | | | | 12.5 | 10.6 |
| I-55 SBDL | 62+11.32 | | 62+50.00 | 38.68 | 4.2 | 1.0 | 2.0 | 8.6 | 2.6 | 3.1 | 8.9 |
| I-55 SBDL | 62+50.00 | | 63+00.00 | 50.00 | 5.4 | 1.1 | 2.0 | 11.1 | 3.5 | 2.1 | 8.0 |
| I-55 SBDL | 63+00.00 | | 63+50.00 | 50.00 | 5.4 | 1.1 | 2.0 | 11.1 | 3.6 | 1.1 | 4.0 |
| I-55 SBDL | 63+50.00 | | 64+00.00 | 50.00 | 5.4 | 1.1 | 2.0 | 11.1 | 3.7 | 1.2 | 4.5 |
| I-55 SBDL | 64+00.00 | | 64+50.00 | 50.00 | 5.4 | 1.1 | 2.0 | 11.1 | 3.7 | 1.4 | 5.4 |
| I-55 SBDL | 64+50.00 | | 65+00.00 | 50.00 | 5.2 | 0.9 | 2.0 | 11.1 | 3.1 | 1.4 | 5.3 |
| SUB TOTAL SBDL: | | | | | 228.3 | | | 513.0 | 194.9 | | 526.6 |

*SHOWN FOR INFORMATION ONLY

| | | | | | | | | | | |
|---------------------------------------------------------------|----------------------|------------|--------------------|-----------------------------------------------------------|--------------------------------|-----------------------|--------------|---------------------------|--------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SCHEDULES OF QUANTITIES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ci:\pw\work\p\midot\detersbj\0157116\0570570-sht-schedule.dgn | DRAWN - | REVISED - | 704 | | | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 44 | |
| #MODELNAME# | CHECKED - | REVISED - | CONTRACT NO. 70570 | | | | | | | |
| PLOT DATE = 8/13/2013 | DATE - | REVISED - | SCALE: | | | SHEET 12 OF 27 SHEETS | STA. TO STA. | ILLINOIS FED. AID PROJECT | | |

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 10 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | *AGGREGATE SHOULDER AVG END AREA FROM CADD (SQ FT) | AGGREGATE SHOULDER AVG WIDTH (FOOT) | AGGREGATE SHOULDER, TYPE A 6" 48100500 (SQ YD) | *AGGREGATE SHOULDER (TON) | HOT-MIX ASPHALT SHOULDER AVERAGE END AREA FROM CADD (SQ FT) | HOT-MIX ASPHALT SHOULDERS 48203100 (TON) |
|------------------|-------------------------------|----|----------|---------------|--------------------------------------------------|----------------------------------------------------|-------------------------------------|------------------------------------------------|---------------------------|-------------------------------------------------------------|------------------------------------------|
| I-55 SBPL | 29+00.00 | | 29+50.00 | 50.00 | 2.6 | 1.5 | 2.0 | 11.1 | 4.9 | 0.6 | 2.4 |
| I-55 SBPL | 29+50.00 | | 30+00.00 | 50.00 | 2.7 | 1.8 | 2.0 | 11.1 | 5.9 | 0.7 | 2.5 |
| I-55 SBPL | 30+00.00 | | 30+50.00 | 50.00 | 2.8 | 1.6 | 2.0 | 11.1 | 5.4 | 0.6 | 2.4 |
| I-55 SBPL | 30+50.00 | | 31+00.00 | 50.00 | 2.8 | 1.6 | 2.0 | 11.1 | 5.4 | 0.7 | 2.5 |
| I-55 SBPL | 31+00.00 | | 31+50.00 | 50.00 | 2.9 | 1.6 | 2.0 | 11.1 | 5.3 | 0.7 | 2.6 |
| I-55 SBPL | 31+50.00 | | 32+00.00 | 50.00 | 2.9 | 1.6 | 2.0 | 11.1 | 5.4 | 0.7 | 2.5 |
| I-55 SBPL | 32+00.00 | | 32+50.00 | 50.00 | 3.0 | 1.6 | 2.0 | 11.1 | 5.3 | 0.7 | 2.5 |
| I-55 SBPL | 32+50.00 | | 33+00.00 | 50.00 | 3.0 | 1.6 | 2.0 | 11.1 | 5.3 | 0.7 | 2.6 |
| I-55 SBPL | 33+00.00 | | 33+50.00 | 50.00 | 2.9 | 1.6 | 2.0 | 11.1 | 5.5 | 0.7 | 2.6 |
| I-55 SBPL | 33+50.00 | | 34+00.00 | 50.00 | 2.8 | 1.4 | 2.0 | 11.1 | 4.6 | 0.7 | 2.6 |
| I-55 SBPL | 34+00.00 | | 34+50.00 | 50.00 | 3.0 | 1.1 | 2.0 | 11.1 | 3.8 | 0.7 | 2.7 |
| I-55 SBPL | 34+50.00 | | 35+00.00 | 50.00 | 3.1 | 1.1 | 2.0 | 11.1 | 3.8 | 0.7 | 2.7 |
| I-55 SBPL | 35+00.00 | | 35+50.00 | 50.00 | 3.0 | 1.1 | 2.0 | 11.1 | 3.8 | 0.7 | 2.6 |
| I-55 SBPL | 35+50.00 | | 36+00.00 | 50.00 | 2.9 | 1.4 | 2.0 | 11.1 | 4.6 | 0.7 | 2.6 |
| I-55 SBPL | 36+00.00 | | 36+50.00 | 50.00 | 2.9 | 1.6 | 2.0 | 11.1 | 5.4 | 0.7 | 2.6 |
| I-55 SBPL | 36+50.00 | | 37+00.00 | 50.00 | 2.9 | 1.6 | 2.0 | 11.1 | 5.3 | 0.7 | 2.7 |
| I-55 SBPL | 37+00.00 | | 37+50.00 | 50.00 | 3.2 | 1.6 | 2.0 | 11.1 | 5.3 | 0.8 | 2.8 |
| I-55 SBPL | 37+50.00 | | 38+00.00 | 50.00 | 3.1 | 1.6 | 2.0 | 11.1 | 5.3 | 0.8 | 2.8 |
| I-55 SBPL | 38+00.00 | | 38+50.00 | 50.00 | 3.0 | 1.6 | 2.0 | 11.1 | 5.2 | 0.9 | 3.3 |
| I-55 SBPL | 38+50.00 | | 39+00.00 | 50.00 | 3.1 | 1.5 | 2.0 | 11.1 | 5.2 | 1.0 | 3.8 |
| I-55 SBPL | 39+00.00 | | 39+50.00 | 50.00 | 3.1 | 1.6 | 2.0 | 11.1 | 5.3 | 0.9 | 3.2 |
| I-55 SBPL | 39+50.00 | | 40+00.00 | 50.00 | 3.1 | 1.6 | 2.0 | 11.1 | 5.3 | 0.9 | 3.3 |
| I-55 SBPL | 40+00.00 | | 40+50.00 | 50.00 | 3.0 | 1.6 | 2.0 | 11.1 | 5.3 | 1.0 | 3.8 |
| I-55 SBPL | 40+50.00 | | 41+00.00 | 50.00 | 2.9 | 1.6 | 2.0 | 11.1 | 5.3 | 0.9 | 3.3 |
| I-55 SBPL | 41+00.00 | | 41+50.00 | 50.00 | 2.9 | 1.6 | 2.0 | 11.1 | 5.3 | 0.9 | 3.3 |
| I-55 SBPL | 41+50.00 | | 42+00.00 | 50.00 | 3.0 | 1.6 | 2.0 | 11.1 | 5.3 | 1.0 | 3.8 |
| I-55 SBPL | 42+00.00 | | 42+50.00 | 50.00 | 3.1 | 1.6 | 2.0 | 11.1 | 5.3 | 0.8 | 3.1 |
| I-55 SBPL | 42+50.00 | | 43+00.00 | 50.00 | 3.2 | 1.6 | 2.0 | 11.1 | 5.3 | 0.8 | 2.9 |
| I-55 SBPL | 43+00.00 | | 43+50.00 | 50.00 | 3.3 | 1.6 | 2.0 | 11.1 | 5.3 | 1.1 | 4.1 |
| I-55 SBPL | 43+50.00 | | 44+00.00 | 50.00 | 3.3 | 1.4 | 2.0 | 11.1 | 4.8 | 1.7 | 6.4 |
| I-55 SBPL | 44+00.00 | | 44+50.00 | 50.00 | 3.3 | 1.4 | 2.0 | 11.1 | 4.5 | 2.6 | 9.8 |
| I-55 SBPL | 44+50.00 | | 45+00.00 | 50.00 | 3.3 | 1.5 | 2.0 | 11.1 | 4.9 | 3.6 | 13.5 |
| I-55 SBPL | 45+00.00 | | 45+50.00 | 50.00 | 3.3 | 1.5 | 2.0 | 11.1 | 4.9 | 4.4 | 16.6 |
| I-55 SBPL | 45+50.00 | | 46+00.00 | 50.00 | 3.3 | 1.5 | 2.0 | 11.1 | 4.9 | 5.0 | 18.6 |
| I-55 SBPL | 46+00.00 | | 46+50.00 | 50.00 | 3.3 | 1.5 | 2.0 | 11.1 | 4.9 | 5.3 | 19.9 |
| I-55 SBPL | 46+50.00 | | 47+00.00 | 50.00 | 3.3 | 1.5 | 2.0 | 11.1 | 4.9 | 5.5 | 20.6 |
| I-55 SBPL | 47+00.00 | | 47+50.00 | 50.00 | 3.3 | 1.5 | 2.0 | 11.1 | 4.9 | 5.6 | 21.0 |
| I-55 SBPL | 47+50.00 | | 48+00.00 | 50.00 | 3.3 | 1.5 | 2.0 | 11.1 | 4.9 | 5.8 | 21.6 |
| I-55 SBPL | 48+00.00 | | 48+50.00 | 50.00 | 3.3 | 1.5 | 2.0 | 11.1 | 4.9 | 5.9 | 22.2 |
| I-55 SBPL | 48+50.00 | | 49+00.00 | 50.00 | 3.3 | 1.5 | 2.0 | 11.1 | 4.9 | 6.1 | 22.7 |
| I-55 SBPL | 49+00.00 | | 49+50.00 | 50.00 | 3.3 | 1.5 | 2.0 | 11.1 | 4.9 | 6.6 | 24.5 |
| I-55 SBPL | 49+50.00 | | 50+00.00 | 50.00 | 3.3 | 1.2 | 2.0 | 11.1 | 4.1 | 7.3 | 27.4 |
| I-55 SBPL | 50+00.00 | | 50+50.00 | 50.00 | 3.3 | 1.0 | 2.0 | 11.1 | 3.3 | 8.0 | 29.8 |
| I-55 SBPL | 50+50.00 | | 50+80.66 | 30.66 | 2.5 | 1.0 | 2.0 | 6.8 | 2.0 | 8.3 | 19.0 |
| I-55 SBPL | BRIDGE OMISSION S.N. 057-0254 | | | | | | | | | | |
| I-55 SBPL | 53+50.16 | | 54+00.00 | 49.84 | 2.2 | 1.0 | 2.0 | 11.1 | 3.3 | 8.4 | 31.1 |
| I-55 SBPL | 54+00.00 | | 54+50.00 | 50.00 | 3.3 | 1.0 | 2.0 | 11.1 | 3.3 | 8.6 | 32.2 |
| I-55 SBPL | 54+50.00 | | 55+00.00 | 50.00 | 3.3 | 1.0 | 2.0 | 11.1 | 3.3 | 8.9 | 33.1 |
| I-55 SBPL | 55+00.00 | | 55+50.00 | 50.00 | 3.3 | 1.0 | 2.0 | 11.1 | 3.3 | 8.8 | 32.8 |
| I-55 SBPL | 55+50.00 | | 56+00.00 | 50.00 | 3.3 | 1.0 | 2.0 | 11.1 | 3.3 | 8.7 | 32.6 |
| I-55 SBPL | 56+00.00 | | 56+50.00 | 50.00 | 3.3 | 1.0 | 2.0 | 11.1 | 3.3 | 9.0 | 33.6 |
| I-55 SBPL | 56+50.00 | | 56+80.65 | 30.65 | 1.8 | 1.0 | 2.0 | 6.8 | 2.0 | 9.2 | 21.2 |
| I-55 SBPL | BRIDGE OMISSION S.N. 057-0252 | | | | | | | | | | |
| I-55 SBPL | 59+27.75 | | 59+50.00 | 22.25 | 1.0 | 1.5 | 2.0 | 4.9 | 2.2 | 8.5 | 14.2 |
| I-55 SBPL | 59+50.00 | | 60+00.00 | 50.00 | 3.3 | 1.5 | 2.0 | 11.1 | 4.9 | 8.2 | 30.5 |
| I-55 SBPL | 60+00.00 | | 60+50.00 | 50.00 | 3.3 | 1.5 | 2.0 | 11.1 | 4.9 | 7.2 | 27.0 |
| I-55 SBPL | 60+50.00 | | 61+00.00 | 50.00 | 3.3 | 1.5 | 2.0 | 11.1 | 4.9 | 5.9 | 22.2 |
| I-55 SBPL | 61+00.00 | | 61+50.00 | 50.00 | 3.2 | 1.5 | 2.0 | 11.1 | 4.9 | 4.7 | 17.6 |
| I-55 SBPL | 61+50.00 | | 62+00.00 | 50.00 | 3.3 | 1.5 | 2.0 | 11.1 | 4.9 | 3.7 | 13.7 |
| I-55 SBPL | 62+00.00 | | 62+50.00 | 50.00 | 3.3 | 1.4 | 2.0 | 11.1 | 4.7 | 2.8 | 10.4 |
| I-55 SBPL | 62+50.00 | | 63+00.00 | 50.00 | 3.3 | 1.3 | 2.0 | 11.1 | 4.4 | 2.0 | 7.4 |
| I-55 SBPL | 63+00.00 | | 63+50.00 | 50.00 | 3.3 | 1.2 | 2.0 | 11.1 | 4.1 | 1.1 | 4.2 |
| I-55 SBPL | 63+50.00 | | 64+00.00 | 50.00 | 3.3 | 1.1 | 2.0 | 11.1 | 3.7 | 0.7 | 2.8 |
| I-55 SBPL | 64+00.00 | | 64+50.00 | 50.00 | 3.3 | 1.3 | 2.0 | 11.1 | 4.2 | 0.9 | 3.3 |
| I-55 SBPL | 64+50.00 | | 65+00.00 | 50.00 | 3.3 | 1.1 | 2.0 | 11.1 | 3.5 | 0.8 | 3.1 |
| SUB TOTALS SBPL: | | | | | 192.9 | | | 684.5 | 289.2 | | 751.2 |

*SHOWN FOR INFORMATION ONLY

| | | | | | | | | | | | |
|-------------|-------------------------------------------------------------|------------|-----------|-----------------------------------------------------------|--------------------------------|--------------------|-----------------------|--------|--------------|---------------------------|--|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SCHEDULES OF QUANTITIES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| | c:\pw\work\p\dot\detersbj\d0157116\0570570-sht-schedule.dgn | DRAWN - | REVISED - | | | 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 45 | |
| #MODELNAME# | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | | CONTRACT NO. 70570 | | | | | |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - | | | SCALE: | SHEET 13 OF 27 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT | |

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 11 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | POLYMERIZED LEVELING BINDER (MACHINE METHOD) AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-9.5, FG, N90 40600909 (TON) | HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603092 (TON) | POLYMERIZED BINDER AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603243 (TON) | SURFACE AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 40603545 (TON) |
|--------------------|----------|----|----------|---------------|--------------------------------------------------|------------------------------------------------------------------------------|------------------------------------------------------------------------------|--------------------------------------------------------|----------------------------------------------------------------|----------------------------------------------------|----------------------------------------------------------------------------|-----------------------------------------|-------------------------------------------------------------------------|
| RAMP E | | | | | | | | | | | | | |
| RAMP E | 16+79.13 | | 16+98.49 | 19.36 | 6.2 | | | 9.5 | 13.8 | 2.3 | 3.3 | 2.0 | 2.9 |
| RAMP E | 16+98.49 | | 17+47.27 | 48.78 | 19.5 | | | 9.2 | 33.5 | 2.3 | 8.4 | 2.0 | 7.3 |
| RAMP E | 17+47.27 | | 17+96.05 | 48.78 | 19.8 | | | 8.5 | 31.1 | 2.3 | 8.5 | 2.0 | 7.4 |
| RAMP E | 17+96.05 | | 18+44.82 | 48.77 | 20.5 | | | 8.2 | 29.8 | 2.4 | 8.8 | 2.1 | 7.7 |
| RAMP E | 18+44.82 | | 18+93.58 | 48.76 | 21.5 | | | 8.3 | 30.3 | 2.5 | 9.2 | 2.2 | 8.1 |
| RAMP E | 18+93.58 | | 19+42.32 | 48.74 | 22.9 | | | 8.7 | 31.8 | 2.7 | 9.8 | 2.4 | 8.6 |
| RAMP E | 19+42.32 | | 19+91.05 | 48.73 | 24.7 | | | 9.0 | 32.6 | 2.9 | 10.5 | 2.5 | 9.2 |
| RAMP E | 19+91.05 | | 20+39.75 | 48.70 | 26.8 | | | 9.0 | 32.7 | 3.1 | 11.4 | 2.8 | 10.0 |
| RAMP E | 20+39.75 | | 20+88.42 | 48.67 | 27.4 | | | 8.3 | 30.3 | 3.2 | 11.6 | 2.8 | 10.1 |
| RAMP E | 20+88.42 | | 21+37.08 | 48.66 | 26.0 | | | 5.8 | 21.0 | 3.1 | 11.3 | 2.7 | 9.8 |
| RAMP E | 21+37.08 | | 21+86.04 | 48.96 | 26.2 | | | 3.8 | 13.8 | 3.1 | 11.5 | 2.7 | 10.0 |
| RAMP E | 21+86.04 | | 22+36.11 | 50.07 | 26.7 | | | 4.7 | 17.6 | 3.2 | 12.0 | 2.8 | 10.5 |
| RAMP E | 22+36.11 | | 22+50.00 | 13.89 | 9.3 | | | 6.4 | 6.7 | 3.2 | 3.3 | 2.8 | 2.9 |
| RAMP E | 22+50.00 | | 23+00.00 | 50.00 | 27.7 | | | 7.9 | 29.6 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP E | 23+00.00 | | 23+50.00 | 50.00 | 27.7 | | | 8.6 | 32.0 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP E | 23+50.00 | | 24+00.00 | 50.00 | 27.7 | | | 8.2 | 30.5 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP E | 24+00.00 | | 24+53.50 | 53.50 | 29.3 | | | 5.9 | 23.6 | 3.1 | 12.4 | 2.7 | 10.8 |
| RAMP E | 24+53.50 | | 25+00.00 | 46.50 | 17.4 | | | | | 3.1 | 10.6 | 2.7 | 9.4 |
| RAMP E | 25+00.00 | | 25+50.00 | 50.00 | 18.5 | | | | | 3.0 | 11.3 | 2.7 | 10.0 |
| RAMP E | 25+50.00 | | 26+00.00 | 50.00 | 19.5 | | | | | 3.0 | 11.0 | 2.6 | 9.8 |
| SUB TOTALS RAMP E: | | | | | 445.5 | | | | 440.4 | | 199.6 | | 174.8 |
| RAMP F | | | | | | | | | | | | | |
| RAMP F | 9+99.30 | | 10+48.11 | 48.81 | 21.0 | | | 9.1 | 33.0 | 2.5 | 9.1 | 2.2 | 8.0 |
| RAMP F | 10+48.11 | | 10+96.99 | 48.88 | 25.4 | | | 10.3 | 37.6 | 2.9 | 10.6 | 2.5 | 9.3 |
| RAMP F | 10+96.99 | | 11+46.03 | 49.04 | 26.2 | | | 13.2 | 48.4 | 3.1 | 11.4 | 2.7 | 10.0 |
| RAMP F | 11+46.03 | | 11+95.28 | 49.25 | 26.4 | | | 16.7 | 61.4 | 3.2 | 11.6 | 2.7 | 10.1 |
| RAMP F | 11+95.28 | | 12+44.81 | 49.53 | 26.5 | | | 21.9 | 81.1 | 3.2 | 11.8 | 2.8 | 10.2 |
| RAMP F | 12+44.81 | | 12+96.16 | 51.35 | 27.4 | | | 29.5 | 113.3 | 3.3 | 12.7 | 2.9 | 11.1 |
| RAMP F | 12+96.16 | | 13+50.00 | 53.84 | 31.8 | | | 34.2 | 137.3 | 3.3 | 13.1 | 2.8 | 11.4 |
| RAMP F | 13+50.00 | | 14+00.00 | 50.00 | 27.6 | | | 32.9 | 123.0 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP F | 14+00.00 | | 14+50.00 | 50.00 | 27.6 | | | 27.7 | 103.3 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP F | 14+50.00 | | 15+00.00 | 50.00 | 27.6 | | | 20.7 | 77.2 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP F | 15+00.00 | | 15+50.00 | 50.00 | 27.6 | | | 13.6 | 50.9 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP F | 15+50.00 | | 16+00.00 | 50.00 | 27.6 | | | 8.5 | 31.9 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP F | 16+00.00 | | 16+50.00 | 50.00 | 27.6 | | | 5.5 | 20.6 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP F | 16+50.00 | | 16+74.80 | 24.80 | 13.7 | | | 4.2 | 7.7 | 3.1 | 5.7 | 2.7 | 5.0 |
| RAMP F | 16+74.80 | | 17+00.00 | 25.20 | 9.3 | | | | | 4.7 | 8.8 | 2.7 | 5.1 |
| RAMP F | 17+00.00 | | 17+50.00 | 50.00 | 18.4 | | | | | 3.9 | 14.5 | 2.7 | 10.1 |
| RAMP F | 17+50.00 | | 18+00.00 | 50.00 | 18.4 | | | | | 3.1 | 11.4 | 2.7 | 10.1 |
| RAMP F | 18+00.00 | | 18+50.00 | 50.00 | 18.4 | | | | | 3.0 | 11.2 | 2.7 | 10.0 |
| RAMP F | 18+50.00 | | 19+00.00 | 50.00 | 18.4 | | | | | 3.0 | 11.2 | 2.7 | 10.0 |
| RAMP F | 19+00.00 | | 19+50.00 | 50.00 | 18.4 | | | | | 3.0 | 11.2 | 2.7 | 10.0 |
| RAMP F | 19+50.00 | | 20+00.00 | 50.00 | 18.4 | | | | | 3.0 | 11.2 | 2.7 | 10.0 |

*SHOWN FOR INFORMATION ONLY

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 12 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | POLYMERIZED LEVELING BINDER (MACHINE METHOD) AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-9.5, FG, N90 40600909 (TON) | HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603092 (TON) | POLYMERIZED BINDER AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603243 (TON) | SURFACE AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 40603545 (TON) |
|--------------------|------------|----|------------|------------------|--------------------------------------------------------------|------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------|--------------------------------------------------------------------|----------------------------------------------------------------------------|----------------------------------------------------------------|----------------------------------------------------------------------------------------|-----------------------------------------------------|-------------------------------------------------------------------------------------|
| RAMP F | 20+00.00 | | 20+50.00 | 50.00 | 18.3 | | | | | 3.0 | 11.2 | 2.7 | 10.0 |
| RAMP F | 20+50.00 | | 21+00.00 | 50.00 | 18.1 | | | | | 3.0 | 11.2 | 2.7 | 10.1 |
| RAMP F | 21+00.00 | | 21+50.00 | 50.00 | 17.9 | | | | | 2.9 | 11.0 | 2.6 | 9.9 |
| RAMP F | 21+50.00 | | 22+00.00 | 50.00 | 16.8 | | | | | 2.8 | 10.3 | 2.5 | 9.3 |
| RAMP F | 22+00.00 | | 22+50.00 | 50.00 | 17.3 | | | | | 2.7 | 10.3 | 2.5 | 9.2 |
| RAMP F | 22+50.00 | | 23+00.00 | 50.00 | 17.4 | | | | | 2.9 | 10.7 | 2.4 | 8.8 |
| SUB TOTALS RAMP F: | | | | | 589.1 | | | | 926.8 | | 299.8 | | 258.0 |
| RAMP J | | | | | | | | | | | | | |
| RAMP J | 1007+00.00 | | 1007+50.00 | 50.00 | 18.3 | | | | | 2.9 | 10.6 | 2.6 | 9.6 |
| RAMP J | 1007+50.00 | | 1008+00.0 | 50.00 | 18.4 | | | | | 3.0 | 11.3 | 2.7 | 10.1 |
| RAMP J | 1008+00.00 | | 1008+50.00 | 50.00 | 18.3 | | | | | 3.0 | 11.3 | 2.7 | 10.0 |
| RAMP J | 1008+50.00 | | 1009+00.0 | 50.00 | 18.3 | | | | | 3.0 | 11.3 | 2.7 | 10.0 |
| RAMP J | 1009+00.00 | | 1009+50.00 | 50.00 | 17.9 | | | | | 3.2 | 12.0 | 2.7 | 10.1 |
| RAMP J | 1009+50.00 | | 1010+00.00 | 50.00 | 18.3 | | | | | 4.5 | 16.8 | 2.7 | 10.1 |
| RAMP J | 1010+00.00 | | 1010+50.00 | 50.00 | 18.3 | | | | | 6.1 | 22.8 | 2.7 | 10.1 |
| RAMP J | 1010+50.00 | | 1010+75.50 | 25.50 | 9.3 | | | | | 6.6 | 12.6 | 2.7 | 5.1 |
| RAMP J | 1010+75.50 | | 1011+00.00 | 24.50 | 13.5 | | | 4.6 | 8.4 | 3.1 | 5.7 | 2.7 | 4.9 |
| RAMP J | 1011+00.00 | | 1011+50.00 | 50.00 | 27.5 | | | 7.2 | 26.8 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP J | 1011+50.00 | | 1012+00.00 | 50.00 | 27.5 | | | 12.4 | 46.4 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP J | 1012+00.00 | | 1012+24.38 | 24.38 | 16.6 | | | 18.7 | 34.0 | 3.5 | 6.3 | 3.0 | 5.5 |
| RAMP J | 1012+24.38 | | 1012+87.36 | 62.98 | 33.2 | | | 22.0 | 103.4 | 3.7 | 17.2 | 3.2 | 15.0 |
| RAMP J | 1012+87.36 | | 1013+44.55 | 57.19 | 30.2 | | | 22.1 | 94.2 | 3.4 | 14.4 | 2.9 | 12.6 |
| RAMP J | 1013+44.55 | | 1013+98.39 | 53.84 | 28.8 | | | 20.8 | 83.5 | 3.2 | 12.9 | 2.8 | 11.3 |
| RAMP J | 1013+98.39 | | 1014+50.47 | 52.08 | 27.7 | | | 18.7 | 72.9 | 3.1 | 12.2 | 2.7 | 10.6 |
| RAMP J | 1014+50.47 | | 1015+01.98 | 51.51 | 27.3 | | | 19.8 | 76.0 | 3.1 | 11.9 | 2.7 | 10.4 |
| RAMP J | 1015+01.98 | | 1015+23.51 | 21.53 | 8.0 | | | 21.2 | 34.2 | 3.1 | 5.0 | 2.7 | 4.3 |
| SUB TOTALS RAMP J: | | | | | 377.5 | | | | 579.7 | | 217.5 | | 169.7 |
| RAMP N | | | | | | | | | | | | | |
| RAMP N | 6+00.00 | | 6+50.00 | 50.00 | 17.8 | | | | | 2.9 | 11.0 | 2.6 | 9.8 |
| RAMP N | 6+50.00 | | 7+00.00 | 50.00 | 17.8 | | | | | 3.1 | 11.5 | 2.7 | 10.0 |
| RAMP N | 7+00.00 | | 7+50.00 | 50.00 | 17.9 | | | | | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP N | 7+50.00 | | 8+00.00 | 50.00 | 17.9 | | | | | 5.5 | 20.5 | 2.7 | 10.1 |
| RAMP N | 8+00.00 | | 8+02.72 | 2.72 | 1.0 | | | | | 7.9 | 1.6 | 2.7 | 0.5 |
| RAMP N | 8+02.72 | | 8+50.00 | 47.28 | 25.3 | | | 9.7 | 34.2 | 3.1 | 10.9 | 2.7 | 9.5 |
| RAMP N | 8+50.00 | | 9+00.00 | 50.00 | 26.8 | | | 10.4 | 39.0 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP N | 9+00.00 | | 9+50.00 | 50.00 | 26.8 | | | 14.2 | 52.9 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP N | 9+50.00 | | 10+00.00 | 50.00 | 26.8 | | | 20.3 | 75.7 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP N | 10+00.00 | | 10+50.00 | 50.00 | 26.8 | | | 25.9 | 96.6 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP N | 10+50.00 | | 11+00.00 | 50.00 | 26.8 | | | 19.8 | 73.9 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP N | 11+00.00 | | 11+50.00 | 50.00 | 26.8 | | | 11.2 | 41.9 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP N | 11+50.00 | | 12+00.00 | 50.00 | 26.8 | | | 11.2 | 41.9 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP N | 12+00.00 | | 12+50.00 | 50.00 | 26.8 | | | 11.2 | 41.9 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP N | 12+50.00 | | 13+00.00 | 50.00 | 26.8 | | | 11.2 | 41.9 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP N | 13+00.00 | | 13+50.00 | 50.00 | 26.8 | | | 11.2 | 41.9 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP N | 13+50.00 | | 14+00.00 | 50.00 | 26.8 | | | 11.2 | 41.9 | 3.1 | 11.6 | 2.7 | 10.1 |

*SHOWN FOR INFORMATION ONLY

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 13 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | POLYMERIZED LEVELING BINDER (MACHINE METHOD) AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-9.5, FG, N90 40600909 (TON) | HOT-MIX ASPHALT BINDER AVG. END AREA FROM CADD (SQ FT) | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603092 (TON) | POLYMERIZED BINDER AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603243 (TON) | SURFACE AVG. END AREA FROM CADD (SQ FT) | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 40603545 (TON) |
|--------------------|----------|----|----------|------------------|--------------------------------------------------------------|------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------|--------------------------------------------------------------------|----------------------------------------------------------------------------|----------------------------------------------------------------|----------------------------------------------------------------------------------------|-----------------------------------------------------|-------------------------------------------------------------------------------------|
| RAMP N | 14+00.00 | | 14+50.00 | 50.00 | 26.8 | | | 11.2 | 41.9 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP N | 14+50.00 | | 15+00.00 | 50.00 | 26.8 | | | 11.2 | 41.9 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP N | 15+00.00 | | 15+50.00 | 50.00 | 26.8 | | | 11.2 | 41.9 | 3.1 | 11.6 | 2.7 | 10.1 |
| RAMP N | 15+50.00 | | 15+81.71 | 31.71 | 19.4 | | | 11.9 | 28.3 | 3.3 | 7.8 | 2.9 | 6.8 |
| RAMP N | 15+81.71 | | 16+42.00 | 60.29 | 32.0 | | | 12.5 | 56.2 | 3.5 | 15.6 | 3.0 | 13.6 |
| RAMP N | 16+42.00 | | 16+99.92 | 57.92 | 31.2 | | | 12.1 | 52.5 | 3.4 | 14.6 | 2.9 | 12.7 |
| RAMP N | 16+99.92 | | 17+55.96 | 56.04 | 29.3 | | | 11.9 | 49.7 | 3.3 | 13.8 | 2.9 | 12.0 |
| RAMP N | 17+55.96 | | 18+10.50 | 54.54 | 29.0 | | | 11.7 | 47.5 | 3.2 | 13.1 | 2.8 | 11.4 |
| RAMP N | 18+10.50 | | 18+63.88 | 53.38 | 28.4 | | | 11.5 | 45.8 | 3.2 | 12.7 | 2.8 | 11.0 |
| RAMP N | 18+63.88 | | 19+16.38 | 52.50 | 27.9 | | | 19.3 | 75.8 | 3.1 | 12.3 | 2.7 | 10.7 |
| RAMP N | 19+16.38 | | 19+68.32 | 51.94 | 27.6 | | | 22.5 | 87.2 | 3.1 | 12.1 | 2.7 | 10.5 |
| RAMP N | 19+68.32 | | 20+19.94 | 51.62 | 24.9 | | | 14.6 | 56.4 | 3.1 | 12.0 | 2.7 | 10.4 |
| RAMP N | 20+19.94 | | 20+44.00 | 24.06 | 12.9 | | | 11.6 | 20.8 | 3.1 | 5.6 | 2.7 | 4.8 |
| RAMP N | 20+44.00 | | 20+71.42 | 27.42 | 9.7 | | | | | 9.9 | 20.2 | 2.7 | 5.5 |
| RAMP N | 20+71.42 | | 20+88.00 | 16.58 | 5.9 | | | | | 9.9 | 12.2 | 2.7 | 3.3 |
| RAMP N | 20+88.00 | | 21+22.83 | 34.83 | 12.3 | 7.5 | 19.5 | | | | | 2.7 | 7.0 |
| RAMP N | 21+22.83 | | 21+74.20 | 51.37 | 18.2 | 6.4 | 24.7 | | | | | 2.7 | 10.3 |
| RAMP N | 21+74.20 | | 22+25.56 | 51.36 | 19.2 | 5.1 | 19.7 | | | | | 2.8 | 10.8 |
| RAMP N | 22+25.56 | | 22+76.90 | 51.34 | 19.7 | 4.2 | 16.1 | | | | | 2.9 | 11.1 |
| RAMP N | 22+76.90 | | 23+28.23 | 51.33 | 18.9 | 2.9 | 11.3 | | | | | 2.8 | 10.7 |
| RAMP N | 23+28.23 | | 23+79.54 | 51.31 | 18.1 | 2.0 | 7.6 | | | | | 2.7 | 10.2 |
| RAMP N | 23+79.54 | | 24+30.84 | 51.30 | 17.8 | 1.7 | 6.4 | | | | | 2.7 | 10.2 |
| RAMP N | 24+30.84 | | 24+82.12 | 51.28 | 17.4 | 1.7 | 6.3 | | | | | 2.6 | 10.1 |
| SUB TOTALS RAMP N: | | | | | 892.8 | | 111.5 | | 1269.2 | | 381.0 | | 384.2 |
| RAMP M | | | | | | | | | | | | | |
| RAMP M | 37+44.28 | | 37+62.29 | 18.01 | 5.9 | 1.5 | 2.0 | | | | | 2.5 | 3.3 |
| RAMP M | 37+62.29 | | 37+79.28 | 16.99 | 6.3 | 1.5 | 1.9 | | | | | 2.5 | 3.1 |
| SUB TOTALS RAMP M: | | | | | 12.2 | | 4.0 | | | | | | 6.4 |
| SUB TOTAL = | | | | | 2317.1 | | 115.5 | | 3216.1 | | 1097.9 | | 993.1 |

*SHOWN FOR INFORMATION ONLY

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 14 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | *LT. AGGREGATE SHOULDER AVG. END AREA FROM CADD (SQ FT) | LT. AGGREGATE SHOULDER AVG. WIDTH (FOOT) | *RT. AGGREGATE SHOULDER AVG. END AREA FROM CADD (SQ FT) | RT. AGGREGATE SHOULDER AVG WIDTH (FOOT) | AGGREGATE SHOULDER, TYPE A 6" 48100500 (SQ YD) | AGGREGATE SHOULDER, TYPE A 8" 48100700 (SQ YD) | *AGGREGATE SHOULDERS (TON) | LT. HOT-MIX ASPHALT SHOULDER AVG. END AREA FROM CADD (SQ FT) | RT. HOT-MIX ASPHALT SHOULDER AVG. END AREA FROM CADD (SQ FT) | HOT MIX ASPHALT SHOULDER 48203100 (TON) |
|-------------------|----------|----|----------|---------------|--------------------------------------------------|---------------------------------------------------------|------------------------------------------|---------------------------------------------------------|-----------------------------------------|------------------------------------------------|------------------------------------------------|----------------------------|--------------------------------------------------------------|--------------------------------------------------------------|-----------------------------------------|
| RAMP E | | | | | | | | | | | | | | | |
| RAMP E | 16+79.13 | | 16+98.49 | 19.36 | 0.6 | 1.0 | 2.0 | | | 4.3 | | 1.3 | 6.4 | | 9.2 |
| RAMP E | 16+98.49 | | 17+47.27 | 48.78 | 3.2 | 1.0 | 2.0 | | | 10.8 | | 3.3 | 6.2 | | 22.5 |
| RAMP E | 17+47.27 | | 17+96.05 | 48.78 | 3.2 | 1.0 | 2.0 | | | 10.8 | | 3.3 | 5.9 | | 21.5 |
| RAMP E | 17+96.05 | | 18+44.82 | 48.77 | 3.2 | 1.0 | 2.0 | | | 10.8 | | 3.3 | 5.8 | | 21.0 |
| RAMP E | 18+44.82 | | 18+93.58 | 48.76 | 3.2 | 1.0 | 2.0 | | | 10.8 | | 3.3 | 5.7 | | 20.7 |
| RAMP E | 18+93.58 | | 19+42.32 | 48.74 | 3.2 | 1.0 | 2.0 | | | 10.8 | | 3.2 | 5.5 | | 20.2 |
| RAMP E | 19+42.32 | | 19+91.05 | 48.73 | 3.2 | 1.0 | 2.0 | | | 10.8 | | 3.2 | 5.1 | | 18.7 |
| RAMP E | 19+91.05 | | 20+39.75 | 48.70 | 3.2 | 1.0 | 2.0 | | | 10.8 | | 3.2 | 4.7 | | 17.0 |
| RAMP E | 20+39.75 | | 20+88.42 | 48.67 | 3.2 | 1.0 | 2.0 | | | 10.8 | | 3.2 | 4.5 | | 16.4 |
| RAMP E | 20+88.42 | | 21+37.08 | 48.66 | 3.2 | 1.0 | 2.0 | | | 10.8 | | 3.2 | 3.6 | | 13.2 |
| RAMP E | 21+37.08 | | 21+86.04 | 48.96 | 3.3 | 1.2 | 2.0 | | | 10.9 | | 3.9 | 2.6 | | 9.5 |
| RAMP E | 21+86.04 | | 22+04.77 | 18.73 | 1.2 | 1.5 | 2.0 | | | 4.2 | | 1.9 | 3.6 | | 5.0 |
| RAMP E | 22+04.77 | | 22+36.11 | 31.34 | 3.3 | 1.5 | 2.0 | 1.5 | 2.0 | 13.9 | | 6.2 | 4.7 | 2.8 | 17.6 |
| RAMP E | 22+36.11 | | 22+50.00 | 13.89 | 1.8 | 1.5 | 2.0 | 1.5 | 2.0 | 6.2 | | 2.8 | 4.7 | 2.9 | 7.8 |
| RAMP E | 22+50.00 | | 23+00.00 | 50.00 | 5.7 | 1.6 | 2.0 | 1.5 | 2.0 | 22.2 | | 10.2 | 5.0 | 3.1 | 30.2 |
| RAMP E | 23+00.00 | | 23+50.00 | 50.00 | 5.7 | 1.6 | 2.0 | 1.5 | 2.0 | 22.2 | | 10.2 | 5.5 | 3.0 | 32.1 |
| RAMP E | 23+50.00 | | 24+00.00 | 50.00 | 5.7 | 1.6 | 2.0 | 1.5 | 2.0 | 22.2 | | 10.2 | 6.0 | 2.8 | 32.8 |
| RAMP E | 24+00.00 | | 24+50.00 | 50.00 | 5.7 | 1.6 | 2.0 | 1.4 | 2.0 | 22.2 | | 10.1 | 4.9 | 2.4 | 27.4 |
| RAMP E | 24+50.00 | | 25+00.00 | 50.00 | 5.7 | 1.4 | 2.0 | 1.3 | 2.0 | 22.2 | | 9.2 | 2.6 | 1.7 | 16.0 |
| RAMP E | 25+00.00 | | 25+50.00 | 50.00 | 5.7 | 1.2 | 2.0 | 1.4 | 2.0 | 22.2 | | 8.5 | 1.2 | 0.9 | 7.9 |
| RAMP E | 25+50.00 | | 26+00.00 | 50.00 | 5.5 | 0.9 | 2.0 | 1.3 | 2.0 | 22.2 | | 7.4 | 0.9 | 0.6 | 5.6 |
| SUB-TOTAL RAMP E: | | | | | 79.0 | | | | | 292.5 | 0.0 | 111.0 | | | 372.3 |
| RAMP F | | | | | | | | | | | | | | | |
| RAMP F | 9+99.30 | | 10+48.11 | 48.81 | 3.2 | | | 1.0 | 2.0 | 10.8 | | 3.3 | | 6.0 | 21.9 |
| RAMP F | 10+48.11 | | 10+96.99 | 48.88 | 3.3 | | | 1.0 | 2.0 | 10.9 | | 3.3 | | 6.6 | 23.9 |
| RAMP F | 10+96.99 | | 11+46.03 | 49.04 | 3.3 | | | 1.0 | 2.0 | 10.9 | | 3.3 | | 8.4 | 30.8 |
| RAMP F | 11+46.03 | | 11+95.28 | 49.25 | 3.3 | | | 1.0 | 2.0 | 10.9 | | 3.3 | | 9.5 | 35.0 |
| RAMP F | 11+95.28 | | 12+02.38 | 7.10 | 0.4 | | | 1.0 | 2.0 | 1.6 | | 0.5 | | 9.8 | 5.2 |
| RAMP F | 12+02.38 | | 12+44.81 | 42.43 | 4.6 | 1.0 | 2.0 | 1.3 | 2.0 | 18.9 | | 6.5 | 6.7 | 10.3 | 53.9 |
| RAMP F | 12+44.81 | | 12+96.16 | 51.35 | 5.7 | 1.2 | 2.0 | 1.6 | 2.0 | 22.8 | | 9.7 | 7.5 | 12.7 | 77.3 |
| RAMP F | 12+96.16 | | 13+50.00 | 53.84 | 6.4 | 1.5 | 2.0 | 1.6 | 2.0 | 23.9 | | 11.0 | 8.6 | 13.9 | 90.3 |
| RAMP F | 13+50.00 | | 14+00.00 | 50.00 | 5.7 | 1.5 | 2.0 | 1.6 | 2.0 | 22.2 | | 10.2 | 8.6 | 12.7 | 79.5 |
| RAMP F | 14+00.00 | | 14+50.00 | 50.00 | 5.7 | 1.5 | 2.0 | 1.6 | 2.0 | 22.2 | | 10.2 | 7.7 | 11.2 | 70.6 |
| RAMP F | 14+50.00 | | 15+00.00 | 50.00 | 5.7 | 1.5 | 2.0 | 1.6 | 2.0 | 22.2 | | 10.2 | 6.5 | 9.0 | 57.6 |
| RAMP F | 15+00.00 | | 15+50.00 | 50.00 | 5.7 | 1.5 | 2.0 | 1.6 | 2.0 | 22.2 | | 10.2 | 4.9 | 6.9 | 43.8 |
| RAMP F | 15+50.00 | | 16+00.00 | 50.00 | 5.7 | 1.5 | 2.0 | 1.6 | 2.0 | 22.2 | | 10.2 | 3.6 | 5.4 | 33.6 |
| RAMP F | 16+00.00 | | 16+50.00 | 50.00 | 5.7 | 1.5 | 2.0 | 1.6 | 2.0 | 22.2 | | 10.2 | 2.9 | 4.3 | 26.8 |
| RAMP F | 16+50.00 | | 17+00.00 | 50.00 | 5.6 | 1.5 | 2.0 | 1.6 | 2.0 | 22.2 | | 10.2 | 2.4 | 3.2 | 20.7 |
| RAMP F | 17+00.00 | | 17+50.00 | 50.00 | 5.7 | 1.4 | 2.0 | 1.4 | 2.0 | 22.2 | | 9.1 | 1.7 | 2.0 | 14.1 |
| RAMP F | 17+50.00 | | 18+00.00 | 50.00 | 5.7 | 1.2 | 2.0 | 1.0 | 2.0 | 22.2 | | 7.6 | 1.1 | 1.1 | 8.5 |
| RAMP F | 18+00.00 | | 18+50.00 | 50.00 | 5.7 | 1.2 | 2.0 | 1.0 | 2.0 | 22.2 | | 7.5 | 0.7 | 0.9 | 6.0 |
| RAMP F | 18+50.00 | | 19+00.00 | 50.00 | 5.7 | 1.4 | 2.0 | 1.1 | 2.0 | 22.2 | | 8.3 | 0.8 | 0.9 | 6.1 |
| RAMP F | 19+00.00 | | 19+50.00 | 50.00 | 5.7 | 1.5 | 2.0 | 1.1 | 2.0 | 22.2 | | 8.7 | 0.8 | 0.9 | 6.1 |
| RAMP F | 19+50.00 | | 20+00.00 | 50.00 | 5.7 | 1.5 | 2.0 | 1.1 | 2.0 | 22.2 | | 8.9 | 0.6 | 0.9 | 5.5 |
| RAMP F | 20+00.00 | | 20+50.00 | 50.00 | 5.7 | 1.5 | 2.0 | 1.2 | 2.0 | 22.2 | | 9.1 | 0.6 | 0.9 | 5.5 |

*SHOWN FOR INFORMATION ONLY

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 15 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | *LT. AGGREGATE SHOULDER AVG. END AREA FROM CADD (SQ FT) | LT. AGGREGATE SHOULDER AVG. WIDTH (FOOT) | *RT. AGGREGATE SHOULDER AVG. END AREA FROM CADD (SQ FT) | RT. AGGREGATE SHOULDER AVG WIDTH (FOOT) | AGGREGATE SHOULDER, TYPE A 6" 48100500 (SQ YD) | AGGREGATE SHOULDER, TYPE A 8" 48100700 (SQ YD) | *AGGREGATE SHOULDERS (TON) | LT. HOT-MIX ASPHALT AVG. END AREA FROM CADD (SQ FT) | RT. HOT-MIX ASPHALT SHOULDER AVG. END AREA FROM CADD (SQ FT) | HOT MIX ASPHALT SHOULDER 48203100 (TON) |
|-------------------|------------|----|------------|---------------|--------------------------------------------------|---------------------------------------------------------|------------------------------------------|---------------------------------------------------------|-----------------------------------------|------------------------------------------------|------------------------------------------------|----------------------------|-----------------------------------------------------|--------------------------------------------------------------|-----------------------------------------|
| RAMP F | 20+50.00 | | 21+00.00 | 50.00 | 5.6 | 1.5 | 2.0 | 1.5 | 2.0 | 22.2 | | 9.9 | 0.7 | 0.9 | 5.7 |
| RAMP F | 21+00.00 | | 21+50.00 | 50.00 | 5.6 | 1.5 | 2.0 | 1.6 | 2.0 | 22.2 | | 10.3 | 0.7 | 0.9 | 5.9 |
| RAMP F | 21+50.00 | | 22+00.00 | 50.00 | 5.7 | 1.3 | 2.0 | 1.6 | 2.0 | 22.2 | | 9.5 | 0.6 | 1.0 | 6.0 |
| RAMP F | 22+00.00 | | 22+50.00 | 50.00 | 5.9 | 1.3 | 2.0 | 1.7 | 2.0 | 22.2 | | 10.0 | 0.7 | 1.0 | 6.0 |
| RAMP F | 22+50.00 | | 23+00.00 | 50.00 | 6.4 | 1.5 | 2.0 | 1.5 | 2.0 | 22.2 | | 9.8 | 0.7 | 0.9 | 6.1 |
| SUB-TOTAL RAMP F: | | | | | 138.9 | | | | | 533.0 | 0.0 | 220.8 | | | 752.5 |
| RAMP J | | | | | | | | | | | | | | | |
| RAMP J | 1007+00.00 | | 1007+50.00 | 50.00 | 5.7 | 0.9 | 2.0 | 1.4 | 2.0 | 22.2 | | 7.7 | 0.8 | 0.9 | 6.5 |
| RAMP J | 1007+50.00 | | 1008+00.00 | 50.00 | 5.6 | 1.0 | 2.0 | 1.7 | 2.0 | 22.2 | | 9.0 | 0.9 | 0.9 | 6.8 |
| RAMP J | 1008+00.00 | | 1008+50.00 | 50.00 | 5.7 | 0.9 | 2.0 | 1.4 | 2.0 | 22.2 | | 7.7 | 0.9 | 0.9 | 6.8 |
| RAMP J | 1008+50.00 | | 1009+00.00 | 50.00 | 5.7 | 0.8 | 2.0 | 1.1 | 2.0 | 22.2 | | 6.3 | 1.0 | 1.2 | 8.0 |
| RAMP J | 1009+00.00 | | 1009+50.00 | 50.00 | 5.7 | 1.1 | 2.0 | 1.2 | 2.0 | 22.2 | | 7.6 | 1.5 | 1.6 | 11.6 |
| RAMP J | 1009+50.00 | | 1010+00.00 | 50.00 | 5.7 | 1.3 | 2.0 | 1.4 | 2.0 | 22.2 | | 9.0 | 2.0 | 2.1 | 15.3 |
| RAMP J | 1010+00.00 | | 1010+50.00 | 50.00 | 5.7 | 1.4 | 2.0 | 1.2 | 2.0 | 22.2 | | 8.7 | 2.3 | 2.5 | 18.1 |
| RAMP J | 1010+50.00 | | 1011+00.00 | 50.00 | 5.7 | 1.5 | 2.0 | 1.0 | 2.0 | 22.2 | | 8.1 | 2.5 | 3.2 | 21.2 |
| RAMP J | 1011+00.00 | | 1011+50.00 | 50.00 | 5.7 | 1.5 | 2.0 | 1.0 | 2.0 | 22.2 | | 8.2 | 3.0 | 4.6 | 28.5 |
| RAMP J | 1011+50.00 | | 1012+00.00 | 50.00 | 5.7 | 1.5 | 2.0 | 1.0 | 2.0 | 22.2 | | 8.2 | 4.2 | 6.5 | 39.9 |
| RAMP J | 1012+00.00 | | 1012+24.38 | 24.38 | 3.2 | 1.5 | 2.0 | 1.0 | 2.0 | 10.8 | | 4.0 | 5.8 | 8.9 | 26.7 |
| RAMP J | 1012+24.38 | | 1012+87.36 | 62.98 | 6.9 | 1.5 | 2.0 | 1.0 | 2.0 | 28.0 | | 10.4 | 6.4 | 10.5 | 79.5 |
| RAMP J | 1012+87.36 | | 1013+44.55 | 57.19 | 6.3 | 1.5 | 2.0 | 1.0 | 2.0 | 25.4 | | 9.4 | 5.9 | 10.6 | 70.5 |
| RAMP J | 1013+44.55 | | 1013+51.24 | 6.69 | 0.7 | 1.5 | 2.0 | 1.0 | 2.0 | 3.0 | | 1.1 | 5.8 | 10.5 | 8.1 |
| RAMP J | 1013+51.24 | | 1013+70.67 | 19.43 | 1.5 | | | 1.0 | 2.0 | 4.3 | | 1.3 | 5.8 | 10.5 | 23.6 |
| RAMP J | 1013+70.67 | | 1013+98.39 | 27.72 | 2.3 | | | 1.0 | 2.0 | 6.2 | | 1.8 | | 10.0 | 20.8 |
| RAMP J | 1013+98.39 | | 1014+50.47 | 52.08 | 3.5 | | | 1.0 | 2.0 | 11.6 | | 3.5 | | 9.6 | 37.2 |
| RAMP J | 1014+50.47 | | 1015+01.98 | 51.51 | 3.4 | | | 1.0 | 2.0 | 11.4 | | 3.4 | | 10.3 | 39.5 |
| RAMP J | 1015+01.98 | | 1015+23.51 | 21.53 | 1.0 | | | 1.0 | 2.0 | 4.8 | | 1.4 | | 11.0 | 17.7 |
| SUB-TOTAL RAMP J: | | | | | 85.6 | | | | | 327.7 | 0.0 | 116.9 | | | 486.1 |
| RAMP N | | | | | | | | | | | | | | | |
| RAMP N | 6+00.00 | | 6+50.00 | 50.00 | 2.7 | | | 1.3 | 2.0 | 11.1 | | 4.5 | | 0.8 | 2.8 |
| RAMP N | 6+50.00 | | 6+76.90 | 26.90 | 1.7 | | | 1.6 | 2.0 | 6.0 | | 2.9 | | 0.8 | 1.5 |
| RAMP N | 6+76.90 | | 7+00.00 | 23.10 | 2.4 | 1.2 | 2.0 | 1.6 | 2.0 | 10.3 | | 4.2 | 1.5 | 1.3 | 4.9 |
| RAMP N | 7+00.00 | | 7+50.00 | 50.00 | 4.5 | 1.3 | 2.0 | 1.5 | 2.0 | 22.2 | | 9.4 | 1.7 | 2.1 | 14.0 |
| RAMP N | 7+50.00 | | 8+00.00 | 50.00 | 5.6 | 1.4 | 2.0 | 1.3 | 2.0 | 22.2 | | 9.0 | 2.7 | 2.7 | 19.8 |
| RAMP N | 8+00.00 | | 8+50.00 | 50.00 | 5.5 | 1.5 | 2.0 | 1.0 | 2.0 | 22.2 | | 8.2 | 4.1 | 3.8 | 29.8 |
| RAMP N | 8+50.00 | | 9+00.00 | 50.00 | 5.6 | 1.5 | 2.0 | 1.0 | 2.0 | 22.2 | | 8.2 | 4.9 | 5.5 | 38.8 |
| RAMP N | 9+00.00 | | 9+50.00 | 50.00 | 5.6 | 1.5 | 2.0 | 1.0 | 2.0 | 22.2 | | 8.2 | 5.5 | 7.2 | 47.4 |
| RAMP N | 9+50.00 | | 10+00.00 | 50.00 | 5.6 | 1.5 | 2.0 | 1.0 | 2.0 | 22.2 | | 8.2 | 6.8 | 8.6 | 57.3 |
| RAMP N | 10+00.00 | | 10+50.00 | 50.00 | 5.6 | 1.5 | 2.0 | 1.0 | 2.0 | 22.2 | | 8.2 | 8.0 | 10.2 | 67.7 |
| RAMP N | 10+50.00 | | 10+73.00 | 23.00 | 2.6 | 1.5 | 2.0 | 1.0 | 2.0 | 10.2 | | 3.8 | 8.4 | 11.2 | 33.7 |
| RAMP N | 10+73.00 | | 11+00.00 | 27.00 | 3.0 | 2.0 | 2.0 | 1.3 | 2.0 | | 12.0 | 6.1 | 2.7 | 4.0 | 13.4 |
| RAMP N | 11+00.00 | | 11+50.00 | 50.00 | 5.6 | 2.0 | 2.0 | 1.3 | 2.0 | 22.2 | | 11.2 | 2.7 | 4.0 | 24.9 |
| RAMP N | 11+50.00 | | 12+00.00 | 50.00 | 5.6 | 1.7 | 2.0 | 1.3 | 2.0 | 22.2 | | 10.0 | 2.7 | 4.0 | 24.9 |
| RAMP N | 12+00.00 | | 12+50.00 | 50.00 | 5.6 | 1.3 | 2.0 | 1.3 | 2.0 | 22.2 | | 8.7 | 2.7 | 4.0 | 24.9 |
| RAMP N | 12+50.00 | | 13+00.00 | 50.00 | 5.6 | 1.3 | 2.0 | 1.3 | 2.0 | 22.2 | | 8.7 | 2.7 | 4.0 | 24.9 |
| RAMP N | 13+00.00 | | 13+50.00 | 50.00 | 5.6 | 1.3 | 2.0 | 1.3 | 2.0 | 22.2 | | 8.7 | 2.7 | 4.0 | 24.9 |
| RAMP N | 13+50.00 | | 14+00.00 | 50.00 | 5.6 | 1.3 | 2.0 | 1.3 | 2.0 | 22.2 | | 8.7 | 2.7 | 4.0 | 24.9 |
| RAMP N | 14+00.00 | | 14+50.00 | 50.00 | 5.6 | 1.3 | 2.0 | 1.3 | 2.0 | 22.2 | | 8.7 | 2.7 | 4.0 | 24.9 |

*SHOWN FOR INFORMATION ONLY

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 16 OF 17)

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | *LT. AGGREGATE SHOULDER AVG. END AREA FROM CADD (SQ FT) | LT. AGGREGATE SHOULDER AVG. WIDTH (FOOT) | *RT. AGGREGATE SHOULDER AVG. END AREA FROM CADD (SQ FT) | RT. AGGREGATE SHOULDER AVG WIDTH (FOOT) | AGGREGATE SHOULDER, TYPE A 6" 48100500 (SQ YD) | AGGREGATE SHOULDER, TYPE A 8" 48100700 (SQ YD) | *AGGREGATE SHOULDERS (TON) | LT. HOT-MIX ASPHALT AVG. END AREA FROM CADD (SQ FT) | RT. HOT-MIX ASPHALT SHOULDER AVG. END AREA FROM CADD (SQ FT) | HOT MIX ASPHALT SHOULDER 48203100 (TON) |
|-------------------|----------|----|----------|---------------|--------------------------------------------------|---------------------------------------------------------|------------------------------------------|---------------------------------------------------------|-----------------------------------------|------------------------------------------------|------------------------------------------------|----------------------------|-----------------------------------------------------|--------------------------------------------------------------|-----------------------------------------|
| RAMP N | 14+50.00 | | 15+00.00 | 50.00 | 5.6 | 1.3 | 2.0 | 1.3 | 2.0 | | 22.2 | 8.7 | 2.7 | 4.0 | 24.9 |
| RAMP N | 15+00.00 | | 15+50.00 | 50.00 | 5.6 | 1.3 | 2.0 | 1.3 | 2.0 | | 22.2 | 8.7 | 2.7 | 4.0 | 24.9 |
| RAMP N | 15+50.00 | | 15+81.71 | 31.71 | 3.9 | 1.3 | 2.0 | 1.3 | 2.0 | | 14.1 | 5.5 | 2.9 | 4.3 | 16.9 |
| RAMP N | 15+81.71 | | 16+42.00 | 60.29 | 6.7 | 1.3 | 2.0 | 1.3 | 2.0 | | 26.8 | 10.5 | 3.0 | 4.5 | 33.7 |
| RAMP N | 16+42.00 | | 16+99.92 | 57.92 | 6.4 | 1.3 | 2.0 | 1.3 | 2.0 | | 25.7 | 10.1 | 2.9 | 4.4 | 31.4 |
| RAMP N | 16+99.92 | | 17+55.96 | 56.04 | 6.2 | 1.3 | 2.0 | 1.3 | 2.0 | | 24.9 | 9.7 | 2.8 | 4.3 | 29.7 |
| RAMP N | 17+55.96 | | 18+10.50 | 54.54 | 6.0 | 1.7 | 2.0 | 1.3 | 2.0 | | 24.2 | 10.9 | 2.8 | 4.2 | 28.3 |
| RAMP N | 18+10.50 | | 18+63.88 | 53.38 | 5.9 | 2.0 | 2.0 | 1.3 | 2.0 | | 23.7 | 12.0 | 2.7 | 4.1 | 27.3 |
| RAMP N | 18+63.88 | | 19+02.00 | 38.12 | 4.1 | 2.0 | 2.0 | 1.3 | 2.0 | | 16.9 | 8.6 | 2.7 | 4.1 | 19.4 |
| RAMP N | 19+02.00 | | 19+19.06 | 17.06 | 1.7 | 2.0 | 2.0 | 1.0 | 2.0 | 7.6 | | 3.5 | 2.7 | 10.7 | 17.2 |
| RAMP N | 19+19.06 | | 19+68.32 | 49.26 | 3.5 | | | 1.0 | 2.0 | 10.9 | | 3.3 | | 8.8 | 32.3 |
| RAMP N | 19+68.32 | | 20+19.94 | 51.62 | 3.4 | | | 1.0 | 2.0 | 11.5 | | 3.4 | | 5.5 | 21.2 |
| RAMP N | 20+19.94 | | 20+71.42 | 51.48 | 3.4 | | | 1.1 | 2.0 | 11.4 | | 3.7 | | 3.1 | 12.0 |
| RAMP N | 20+71.42 | | 21+22.83 | 51.41 | 3.4 | | | 1.1 | 2.0 | 11.4 | | 3.9 | | 1.9 | 7.3 |
| RAMP N | 21+22.83 | | 21+74.20 | 51.37 | 3.2 | | | 1.1 | 2.0 | 11.4 | | 3.6 | | 1.3 | 5.0 |
| RAMP N | 21+74.20 | | 22+25.56 | 51.36 | 2.4 | | | 1.1 | 2.0 | 11.4 | | 3.6 | | 0.9 | 3.5 |
| RAMP N | 22+25.56 | | 22+76.90 | 51.34 | 2.1 | | | 1.1 | 2.0 | 11.4 | | 3.9 | | 0.9 | 3.4 |
| RAMP N | 22+76.90 | | 23+28.23 | 51.33 | 2.4 | | | 1.1 | 2.0 | 11.4 | | 3.9 | | 0.9 | 3.3 |
| RAMP N | 23+28.23 | | 23+79.54 | 51.31 | 3.3 | | | 1.1 | 2.0 | 11.4 | | 3.9 | | 0.9 | 3.3 |
| RAMP N | 23+79.54 | | 24+30.84 | 51.30 | 3.3 | | | 1.2 | 2.0 | 11.4 | | 3.9 | | 0.9 | 3.3 |
| RAMP N | 24+30.84 | | 24+82.12 | 51.28 | 3.1 | | | 1.2 | 2.0 | 11.4 | | 3.9 | | 1.1 | 4.3 |
| SUB-TOTAL RAMP N: | | | | | 174.8 | | | | | 325.8 | 368.4 | 274.8 | | | 858.0 |
| RAMP M | 37+44.28 | | 37+62.29 | 18.01 | 1.8 | 1.1 | 2.0 | 1.1 | 2.0 | 8.0 | | 2.7 | 0.6 | 0.9 | 2.1 |
| RAMP M | 37+62.29 | | 37+79.28 | 16.99 | 1.7 | 1.1 | 2.0 | 1.1 | 2.0 | 7.6 | | 2.6 | 0.6 | 0.9 | 2.0 |
| RAMP M | 37+79.28 | | 38+13.44 | 34.16 | | | | 1.1 | 2.0 | 7.6 | | 2.6 | | | 0.0 |
| RAMP M | 38+13.44 | | 38+63.07 | 49.63 | | | | 1.1 | 2.0 | 11.0 | | 3.8 | | | 0.0 |
| RAMP M | 38+63.07 | | 39+13.13 | 50.06 | | | | 1.1 | 2.0 | 11.1 | | 3.8 | | | 0.0 |
| RAMP M | 39+13.13 | | 39+63.73 | 50.60 | | | | 1.1 | 2.0 | 11.2 | | 3.8 | | | 0.0 |
| SUB-TOTAL RAMP M: | | | | | 3.5 | | | | | 56.5 | 0.0 | 19.3 | | | 4.0 |
| SUB-TOTAL = | | | | | 481.9 | | | | | 1535.5 | 368.4 | 742.7 | | | 2472.9 |

*SHOWN FOR INFORMATION ONLY

| LOCATION | STATION | TO | STATION | LENGTH (FOOT) | *LT. AGGREGATE SHOULDER AVG. END AREA FROM CADD (SQ FT) | LT. AGGREGATE SHOULDER AVG WIDTH (FOOT) | *RT. AGGREGATE SHOULDER AVG. END AREA FROM CADD (SQ FT) | RT. AGGREGATE SHOULDER AVG WIDTH (FOOT) | AGGREGATE SHOULDER, TYPE A 6" 48100500 (SQ YD) | *AGGREGATE SHOULDERS (TON) |
|---------------------|-----------|----|----------|---------------|---------------------------------------------------------|-----------------------------------------|---------------------------------------------------------|-----------------------------------------|------------------------------------------------|----------------------------|
| I-74 EB | | | | | | | | | | |
| I-74 EB | 635+50.00 | | 636+00.0 | 50.00 | 1.6 | 2.0 | 1.1 | 2.0 | 22.2 | 9.1 |
| I-74 EB | 636+00.00 | | 636+50.0 | 50.00 | 1.4 | 2.0 | 1.1 | 2.0 | 22.2 | 8.3 |
| I-74 EB | 636+50.00 | | 637+00.0 | 50.00 | 1.1 | 2.0 | 1.1 | 2.0 | 22.2 | 7.5 |
| I-74 EB | 637+00.00 | | 637+50.0 | 50.00 | 1.1 | 2.0 | 1.1 | 2.0 | 22.2 | 7.5 |
| I-74 EB | 637+50.00 | | 638+00.0 | 50.00 | 1.1 | 2.0 | 1.1 | 2.0 | 22.2 | 7.5 |
| I-74 EB | 638+00.00 | | 638+50.0 | 50.00 | 1.1 | 2.0 | 1.1 | 2.0 | 22.2 | 7.5 |
| I-74 EB | 638+50.00 | | 639+00.0 | 50.00 | 1.1 | 2.0 | 1.1 | 2.0 | 22.2 | 7.5 |
| I-74 EB | 639+00.00 | | 639+50.0 | 50.00 | 1.1 | 2.0 | 1.1 | 2.0 | 22.2 | 7.1 |
| SUB TOTALS I-74 EB: | | | | | | | | | | 53.1 |
| I-74 WB | | | | | | | | | | |
| I-74 WB | 634+00.00 | | 634+50.0 | 50.00 | 1.0 | 2.0 | | | 11.1 | 3.3 |
| I-74 WB | 634+50.00 | | 635+00.0 | 50.00 | 1.1 | 2.0 | 1.1 | 2.0 | 22.2 | 7.1 |
| I-74 WB | 635+00.00 | | 635+50.0 | 50.00 | 1.1 | 2.0 | 1.1 | 2.0 | 22.2 | 7.6 |
| I-74 WB | 635+50.00 | | 636+00.0 | 50.00 | 1.1 | 2.0 | 1.2 | 2.0 | 22.2 | 7.6 |
| I-74 WB | 636+00.00 | | 636+50.0 | 50.00 | 1.1 | 2.0 | 1.2 | 2.0 | 22.2 | 7.6 |
| I-74 WB | 636+50.00 | | 637+00.0 | 50.00 | 1.1 | 2.0 | 1.1 | 2.0 | 22.2 | 7.5 |
| I-74 WB | 637+00.00 | | 637+50.0 | 50.00 | 1.1 | 2.0 | 1.1 | 2.0 | 22.2 | 7.3 |
| I-74 WB | 637+50.00 | | 638+00.0 | 50.00 | | | 1.1 | 2.0 | 11.1 | 3.6 |
| SUB TOTALS I-74 WB: | | | | | | | | | | 48.1 |

*SHOWN FOR INFORMATION ONLY

SCHEDULES OF QUANTITIES

HOT-MIX ASPHALT & SHOULDERS (SHEET 17 OF 17)

HOT-MIX ASPHALT & SHOULDER SUMMARY

| LOCATION | BITUMINOUS MATERIALS (PRIME COAT) 40600100 (GAL) | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-9.5, FG, N90 40600909 (TON) | HMA BINDER COURSE, IL-19.0, FG, N90 40603092 (TON) | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, FG, N90 40603243 (TON) | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 40603545 (TON) | AGGREGATE SHOULDER, TYPE A 6" 48100500 (SQ YD) | AGGREGATE SHOULDER, TYPE A 8" 48100700 (SQ YD) | *AGGREGATE SHOULDERS (TON) | HOT MIX ASPHALT SHOULDER 48203100 (TON) |
|------------------|-----------------------------------------------------------|---------------------------------------------------------------------------------------|-------------------------------------------------------------|-------------------------------------------------------------------------------------|----------------------------------------------------------------------------------|---------------------------------------------------------|---------------------------------------------------------|----------------------------------|--------------------------------------------------|
| I-55 NBDL | 1191.0 | 137.7 | 1263.5 | 367.6 | 532.7 | | | | |
| I-55 NBPL | 922.3 | 101.5 | 806.8 | 291.3 | 412.7 | | | | |
| I-55 SBDL | 1298.5 | 203.8 | 965.0 | 341.2 | 621.2 | | | | |
| I-55 SBPL | 981.5 | 144.8 | 743.0 | 276.6 | 467.0 | | | | |
| I-55 NBDL SHLD | 214.9 | | | | | 504.1 | | 174.5 | 1023.3 |
| I-55 NBPL SHLD | 172.4 | | | | | 609.1 | | 249.6 | 683.7 |
| I-55 SBDL SHLD | 228.3 | | | | | 513.0 | | 194.9 | 526.6 |
| I-55 SBPL SHLD | 192.9 | | | | | 684.5 | | 289.2 | 751.2 |
| RAMP E | 445.5 | | 440.4 | 199.6 | 174.8 | | | | |
| RAMP F | 589.1 | | 926.8 | 299.8 | 258.0 | | | | |
| RAMP J | 377.5 | | 579.7 | 217.5 | 169.7 | | | | |
| RAMP N | 892.8 | 111.5 | 1269.2 | 381.0 | 384.2 | | | | |
| RAMP M | 12.2 | 4.0 | | | 6.4 | | | | |
| RAMP E SHLD | 79.0 | | | | | 292.5 | | 111.0 | 372.3 |
| RAMP F SHLD | 138.9 | | | | | 533.0 | | 220.8 | 752.5 |
| RAMP J SHLD | 85.6 | | | | | 327.7 | | 116.9 | 486.1 |
| RAMP N SHLD | 174.8 | | | | | 325.8 | 368.4 | 274.8 | 858.0 |
| RAMP M SHLD | 3.5 | | | | | 56.5 | | 19.3 | 4.0 |
| I-74 EB | | | | | | 177.8 | | 53.1 | |
| I-74 WB | | | | | | 155.6 | | 48.1 | |
| TOTALS: | 8000.7 | 703.3 | 6994.3 | 2374.6 | 3026.7 | 4179.6 | 368.4 | 1752.0 | 5457.7 |
| ROUND TO: | 8010.0 | 710.0 | 7000.0 | 2380.0 | 3030.0 | 4180.0 | 370.0 | | 5460.0 |

*SHOWN FOR INFORMATION ONLY

SCHEDULES OF QUANTITIES

DRAINAGE ITEMS

| ROADWAY | STATION | OFFSET | 50104400 CONC. HEADWALL REMOVAL (EACH) | 50105220 PIPE CULVERT REMOVAL (FOOT) | Z0041600 PLUG EXISTING INLETS (EACH) | 60500060 REMOVING INLETS (EACH) | 60500305 FILLING INLETS (EACH) | 54244405 FLUSH INLET BOX FOR MEDIAN STAND 542546 (EACH) | X6100120 TYPE E INLET BOX STAND 610001 (SPL) (EACH) | X6100230 TYPE F INLET BOX STAND 610001 (SPL) (EACH) | 60261546 INLETS TO BE ADJUSTED W/ NEW STAND 542546 F & G (EACH) | 542C0217 PIPE CULV, CLASS C, TYPE 1 12" (FOOT) | 542C0229 PIPE CULV, CLASS C, TYPE 1 24" (FOOT) | 542A1069 PIPE CULV, CLASS A, TYPE 2 24" (FOOT) | 54248510 CONC COLLAR (CU YD) | 54213447 END SECTIONS 12" (EACH) | 54213459 END SECTIONS 24" (EACH) | 60900515 CONCRETE THRUST BLOCKS (EACH) | 60600605 CONCRETE CURB, TYPE B (FOOT) | 66201120 CONCRETE SHOULDER CURB (FOOT) | |
|----------------|----------|----------|----------------------------------------------------|--------------------------------------------------|--------------------------------------------------|------------------------------------------|-----------------------------------------|------------------------------------------------------------------------|--------------------------------------------------------------------|--------------------------------------------------------------------|--------------------------------------------------------------------------------|---------------------------------------------------------------|---------------------------------------------------------------|---------------------------------------------------------------|---------------------------------------|----------------------------------------------|----------------------------------------------|----------------------------------------------------|---------------------------------------------------|----------------------------------------------------|--|
| I-55B | 44+00.00 | 0.0' | | | | | | | | | 1 | | | | | | | | | | |
| I-55B | 50+00.00 | 1.0' LT | | 85 | | | | | | | | | | | | | | | | | |
| I-55B | 50+82.17 | 5.2' LT | 1 | 70 | | | 1 | 1 | | | | | 50 | 15 | 1.5 | | 1 | 1 | | | |
| I-55B | 53+02.00 | 62.0' RT | 1 | 68 | 1 | 1 | | | | | | | | | | | | | | | |
| I-55B | 53+60.00 | 63.4' RT | | | | | | | | 1 | | 80 | | | | 1 | | 1 | 20 | 10 | |
| I-55B | 56+46.64 | 3.1' LT | 1 | 82 | | | | 1 | | | | | 76 | 10 | 1.5 | | 1 | 1 | | | |
| I-55B | 58+93.00 | 62.3' RT | 1 | 74 | 1 | 1 | | | | | | | | | | | | | | | |
| I-55B | 59+50.00 | 61.2' RT | | | | | | | 1 | | | 92 | | | | 1 | | 1 | 20 | 10 | |
| TOTALS: | | | 4 | 379 | 2 | 3 | 1 | 2 | 1 | 1 | 1 | 172 | 126 | 25 | 3 | 2 | 2 | 4 | 40 | 20 | |

PIPE UNDERDRAINS

| ROADWAY | STATION | OFFSET | 60108100 PIPE UNDERDRAINS 4" (SPECIAL) (FOOT) | X6015015 REM AND REPLACE PCC HEADWALL FOR PIPE UNDERDRAINS (EACH) |
|---------------------------------------------|------------|----------|-----------------------------------------------------------|-------------------------------------------------------------------------------------|
| MEDIAN VETERAN'S PARKWAY | | | | |
| I-55B | 33+96.62 | 4.2' LT | 6 | 1 |
| I-55B | 37+29.88 | 5.6' LT | 4 | 1 |
| I-55B | 41+90.64 | 4.6' LT | 5 | 1 |
| I-55B | 51+05. | 5.3' LT | 22 | 1* |
| I-55B | 56+55.90 | 7.8' LT | 10 | 1* |
| I-55B | 64+50. | 68.1' LT | 2 | 1 |
| I-55B | 34+00. | 4.3' RT | 3 | 1 |
| I-55B | 39+04.24 | 6.7' RT | 3 | 1 |
| I-55B | 43+78.89 | 5.9' RT | 6 | 1 |
| I-55B | 49+75. | 0.5' RT | 6 | 1 |
| I-55B | 56+56.37 | 7.2' RT | 16 | 1* |
| I-55B | 62+50. | 7.7' RT | 4 | 1 |
| OUTSIDE SOUTHBOUND VETERAN'S PARKWAY | | | | |
| I-55B | 29+50. | 64.7' RT | 3 | 1 |
| I-55B | 43+05.82 | 66.4' RT | 5 | 1 |
| RAMP E | 24+99.57 | 18.7' LT | 8 | 1 |
| RAMP E | 19+00. | 19.0' RT | 4 | 1 |
| RAMP F | 12+00 | 26.6' RT | 8 | 1 |
| RAMP F | 14+48.49 | 26.5' RT | 12 | 1 |
| RAMP F | 14+50. | 41.0' LT | 9 | 1 |
| RAMP F | 16+47.45 | 19.4' RT | 5 | 1 |
| RAMP F | 16+45.05 | 38.7' LT | 9 | 1 |
| OUTSIDE NORTHBOUND VETERAN'S PARKWAY | | | | |
| I-55B | 35+00. | 62.9' LT | 1 | 1 |
| I-55B | 46+68.08 | 87.7' LT | 6 | 1 |
| RAMP N | 17+00.09 | 28.9' RT | 18 | 1 |
| RAMP N | 15+01.33 | 45.1' LT | 16 | 1 |
| RAMP N | 13+99.4 | 31.3' RT | 20 | 1 |
| RAMP N | 11+20.22 | 45.6' LT | 15 | 1 |
| RAMP N | 11+17.98 | 23.9' RT | 10 | 1 |
| RAMP J | 1023+65.5 | 24.2' RT | 14 | 1 |
| RAMP J | 1017+50. | 30.0' RT | 11 | 1 |
| RAMP J | 1012+51.3 | 22.8' RT | 10 | 1 |
| RAMP J | 1010+99.59 | 35.1' LT | 6 | 1 |
| RAMP J | 1009+96.18 | 18.4' RT | 6 | 1 |
| TOTALS: | | | 283 | 33 |

*DRAIN PIPE UNDERDRAIN (SPECIAL) TO MEDIAN INLET BOX

RUMBLE STRIPS

| LOCATION | STATION TO | STATION | LENGTH FOOT | SHOULDER RUMBLE STRIPS, 16 INCH 64200116 FOOT |
|-----------------------------------------|------------|-----------|----------------|--------------------------------------------------------|
| NBDL SHLD | 32+60.00 | 37+90.86 | 530.86 | 530.86 |
| NBDL SHLD | 47+50.47 | 48+60.00 | 109.53 | 109.53 |
| NBDL SHLD | 60+85.33 | 65+00.00 | 414.67 | 414.67 |
| NBPL SHLD | 32+60.00 | 51+29.38 | 1869.38 | 1869.38 |
| NBPL SHLD BRIDGE OMISSION S.N. 057-0253 | | | | |
| NBPL SHLD | 53+89.92 | 56+91.55 | 301.63 | 301.63 |
| NBPL SHLD BRIDGE OMISSION S.N. 057-0251 | | | | |
| NBPL SHLD | 59+29.99 | 65+00.00 | 570.01 | 570.01 |
| SBDL SHLD | 29+00.00 | 30+00.00 | 100.00 | 100.00 |
| SBDL SHLD | 37+34.68 | 45+25.82 | 791.14 | 791.14 |
| SBDL SHLD | 62+11.32 | 65+00.00 | 288.68 | 288.68 |
| SBPL SHLD | 29+00.00 | 50+80.66 | 2180.66 | 2180.66 |
| SBPL SHLD BRIDGE OMISSION S.N. 057-0254 | | | | |
| SBPL SHLD | 53+50.16 | 56+80.65 | 330.49 | 330.49 |
| SBPL SHLD BRIDGE OMISSION S.N. 057-0252 | | | | |
| SBPL SHLD | 59+27.75 | 65+00.00 | 572.25 | 572.25 |
| I-74 EBPL | 637+75.08 | 639+24.97 | 149.89 | 149.89 |
| I-74 WBPL | 635+01.65 | 636+56.65 | 155.00 | 155.00 |
| TOTAL = | | | | 8364.19 |
| ROUND TO: | | | | 8370.00 |

DELINEATORS

| DELINEATORS 63500105 (EACH) | | | | | | |
|------------------------------------------------|----------|----------|-------------------|------------------|-----------------|-----------------|
| LOCATION | STATION | STATION | SPACING (FEET) | LENGTH (FEET) | SINGLE WHITE | SINGLE AMBER |
| NORTHBOUND - ROADWAY L VETERANS PARKWAY | | | | | | |
| LT | 32+60.00 | 35+96.00 | 150.0 | 336.0 | | 3.0 |
| LT | 37+71.00 | 51+00.00 | 150.0 | 1329.0 | | 9.0 |
| NORTHBOUND - RAMP | | | | | | |
| RAMP N, LT | 6+75.00 | 12+00.00 | 100.0 | 525.0 | | 6.0 |
| RAMP N, RT | 6+00.00 | 8+00.00 | 100.0 | 200.0 | 2.0 | |
| RAMP J, LT | 7+00.00 | 13+50.00 | 40.0 | 650.0 | | 17.0 |
| RAMP J, RT | 7+00.00 | 10+50.00 | 40.0 | 350.0 | 9.0 | |
| SOUTHBOUND - ROADWAY K VETERANS PARKWAY | | | | | | |
| LT | 29+00.00 | 33+91.00 | 150.0 | 491.0 | | 4.0 |
| LT | 35+78.00 | 49+85.00 | 150.0 | 1407.0 | | 10.0 |
| RT | 37+35.00 | 45+25.00 | 150.0 | 790.0 | 6.0 | |
| LT | 59+00.00 | 65+00.00 | 150.0 | 600.0 | | 4.0 |
| SOUTHBOUND - RAMP | | | | | | |
| RAMP M, LT | 37+50.00 | | 55.0 | | | 1.0 |
| RAMP E, LT | 22+00.00 | 26+00.00 | 35.0 | 400.0 | 12.0 | |
| RAMP E, RT | 22+00.00 | 26+00.00 | 35.0 | 400.0 | | 12.0 |
| RAMP F, LT | 12+00.00 | 23+00.00 | 40.0 | 1100.0 | | 28.0 |
| RAMP F, RT | 12+00.00 | 23+00.00 | 40.0 | 1100.0 | 28.0 | |
| NORTHBOUND TOTAL = | | | | | 11.0 | 35.0 |
| NORTHBOUND TOTAL = | | | | | 46.0 | |
| SOUTHBOUND TOTAL = | | | | | 46.0 | 59.0 |
| SOUTHBOUND TOTAL = | | | | | 105.0 | |
| GRAND TOTAL = | | | | | 151.0 | |

SCHEDULES OF QUANTITIES

GUARDRAIL ITEMS

| STATION | OFFSET | TO | STATION | OFFSET | ROUTE | LANE | STEEL PLATE BEAM GUARDRAIL 6300001 (FOOT) | TRAFFIC BARRIER TERMINAL, TYPE 1, (SPECIAL) TANGENT 63100167 (EACH) | TRAFFIC BARRIER TERMINAL, TYPE 2 63100045 (EACH) | TRAFFIC BARRIER TERMINAL, TYPE 5 63100070 (EACH) | TRAFFIC BARRIER TERMINAL, TYPE 6 63100085 (EACH) | TRAFFIC BARRIER TERMINAL, TYPE 6B 63100089 (EACH) | GUARDRAIL REMOVAL 63200310 (FOOT) | GUARDRAIL MARKERS, TYPE A 78200410 (EACH) | TERMINAL MARKERS - DIRECT APPLIED 78201000 (EACH) |
|-----------|---------|----|-----------|---------|--------|------|-------------------------------------------|---------------------------------------------------------------------|--------------------------------------------------|--------------------------------------------------|--------------------------------------------------|---------------------------------------------------|-----------------------------------|-------------------------------------------|---------------------------------------------------|
| 23+56.35 | 15.64' | | 24+08.28 | 15.64' | RAMP F | DS | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 |
| 24+08.28 | 15.64' | | 24+49.18 | 15.64' | RAMP F | DS | 39.4 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 |
| 636+80.12 | 49.14' | | 637+52.31 | 49.14' | I-74 | EBDS | 73.1 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 328.4 | 1.0 | 0.0 |
| 637+52.31 | 49.14' | | 638+07.85 | 48.97' | I-74 | EBDS | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 1.0 | 0.0 |
| 638+19.01 | 48.87' | | 638+50.77 | 48.75' | I-74 | EBDS | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 |
| 638+50.77 | 48.75' | | 638+82.53 | 48.90' | I-74 | EBDS | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 1.0 | 0.0 |
| 636+31.78 | -28.60' | | 636+82.15 | -28.60' | I-74 | EBPS | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 |
| 636+82.15 | -28.60' | | 637+45.13 | -28.60' | I-74 | EBPS | 62.5 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 428.6 | 1.0 | 0.0 |
| 637+45.13 | -28.60' | | 638+01.8 | -28.79' | I-74 | EBPS | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 1.0 | 0.0 |
| 638+13.19 | -28.91' | | 638+45.6 | -29.07' | I-74 | EBPS | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 1.0 | 0.0 |
| 638+45.6 | -29.07' | | 638+78.01 | -28.96' | I-74 | EBPS | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 1.0 | 0.0 |
| 637+46.07 | 27.17' | | 636+96.51 | 27.17' | I-74 | WBPS | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 434.2 | 0.0 | 1.0 |
| 636+96.51 | 27.17' | | 636+40.27 | 27.53' | I-74 | WBPS | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 2.0 | 0.0 |
| 636+24.48 | 27.96' | | 635+90.49 | 28.88' | I-74 | WBPS | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 1.0 | 0.0 |
| 635+90.49 | 28.88' | | 635+56.5 | 29.80' | I-74 | WBPS | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 1.0 | 0.0 |
| 636+97.14 | -50.36' | | 636+47.14 | -50.36' | I-74 | WBDS | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 441.0 | 1.0 | 1.0 |
| 636+47.14 | -50.36' | | 635+90.9 | -50.00' | I-74 | WBDS | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 1.0 | 0.0 |
| 635+75.11 | -49.57' | | 635+41.12 | -48.65' | I-74 | WBDS | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 1.0 | 0.0 |
| 635+41.12 | -48.65' | | 635+07.14 | -47.73' | I-74 | WBDS | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 1.0 | 0.0 |
| 29+06.21 | 54.00' | * | 29+55.06 | 53.59' | I-55B | SBDS | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 |
| 29+55.06 | 53.59' | * | 30+00. | 52.14' | I-55B | SBDS | 46.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 |
| 30+00. | 8.14' | * | 39+30.6 | 7.77' | RAMP M | DS | 929.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 952.0 | 11.0 | 0.0 |
| 39+30.6 | 7.77' | * | 39+43.24 | 7.79' | RAMP M | DS | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 22+14.09 | -6.00' | * | 21+63.15 | -6.00' | RAMP E | DS | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 1.0 |
| 21+63.15 | -6.00' | * | 16+98.48 | -6.34' | RAMP E | DS | 462.5 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 515.1 | 5.0 | 0.0 |
| 16+98.48 | -6.34' | | 16+59.47 | -6.51' | RAMP E | DS | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 50+89.97 | 62.51' | | 50+94.38 | 62.52' | I-55B | SBDS | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 53+04.81 | 65.86' | | 53+19.92 | 66.02' | I-55B | SBDS | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 53+19.92 | 66.02' | | 56+64.45 | 62.52' | I-55B | SBDS | 337.5 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 441.0 | 3.0 | 0.0 |
| 56+64.45 | 62.52' | | 57+08.81 | 62.42' | I-55B | SBDS | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 1.0 | 0.0 |
| 58+97. | 62.81' | | 59+12.08 | 62.87' | I-55B | SBDS | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 59+12.08 | 62.87' | | 59+92.45 | 62.82' | I-55B | SBDS | 78.2 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 114.0 | 1.0 | 0.0 |
| 9+92. | 6.82' | | 11+90.77 | 6.82' | RAMP F | DS | 196.8 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 28.0 | 2.0 | 0.0 |
| 11+90.77 | 6.82' | | 12+03.4 | 6.82' | RAMP F | DS | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| 33+91.16 | 14.00' | * | 34+40.86 | 14.00' | I-55B | SBPS | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 1.0 |
| 34+40.86 | 14.00' | * | 35+65.5 | 14.00' | I-55B | SBPS | 125.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 127.7 | 2.0 | 0.0 |
| 35+65.5 | 14.00' | * | 35+78. | 14.00' | I-55B | SBPS | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 |
| 49+84.69 | 11.20' | | 50+34.94 | 11.20' | I-55B | SBPS | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 |
| 50+34.94 | 11.20' | | 50+85.18 | 11.20' | I-55B | SBPS | 50.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 143.0 | 1.0 | 0.0 |
| 50+85.18 | 11.20' | | 51+28.54 | 11.03' | I-55B | SBPS | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 |

SCHEDULES OF QUANTITIES

GUARDRAIL ITEMS (CONTINUED)

| STATION | OFFSET | TO | STATION | OFFSET | ROUTE | LANE | STEEL PLATE BEAM GUARDRAIL 63000001 (FOOT) | TRAFFIC BARRIER TERMINAL, TYPE 1, (SPECIAL) TANGENT 63100167 (EACH) | TRAFFIC BARRIER TERMINAL, TYPE 2 63100045 (EACH) | TRAFFIC BARRIER TERMINAL, TYPE 5 63100070 (EACH) | TRAFFIC BARRIER TERMINAL, TYPE 6 63100085 (EACH) | TRAFFIC BARRIER TERMINAL, TYPE 6B 63100089 (EACH) | GUARDRAIL REMOVAL 63200310 (FOOT) | GUARDRAIL MARKERS, TYPE A 78200410 (EACH) | TERMINAL MARKERS - DIRECT APPLIED 78201000 (EACH) | |
|------------|---------|----|------------|---------|--------|------|--------------------------------------------|---------------------------------------------------------------------|--------------------------------------------------|--------------------------------------------------|--------------------------------------------------|---------------------------------------------------|-----------------------------------|-------------------------------------------|---------------------------------------------------|--|
| 53+34.12 | 11.53' | | 53+48.86 | 11.52' | I-55B | SBPS | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| 53+48.86 | 11.52' | | 56+71.05 | 11.40' | I-55B | SBPS | 325.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 428.3 | 2.0 | 0.0 | |
| 56+71.05 | 11.40' | | 57+14.41 | 11.69' | I-55B | SBPS | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 1.0 | 0.0 | |
| 1010+39.43 | 6.00' | * | 1010+90.65 | 6.00' | RAMP J | DS | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 1.0 | |
| 1010+90.65 | 6.00' | * | 1015+09.65 | 6.30' | RAMP J | DS | 412.5 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 313.8 | 7.0 | 0.0 | |
| 1015+09.65 | 6.30' | | 1015+52.67 | 6.90' | RAMP J | DS | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| 1017+35.02 | 10.32' | | 1017+49.62 | 10.55' | RAMP J | DS | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| 1017+49.62 | 10.55' | | 1020+52.36 | 7.87' | RAMP J | DS | 312.5 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 410.4 | 3.0 | 0.0 | |
| 1020+52.36 | 7.87' | | 1020+95.37 | 8.17' | RAMP J | DS | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 1.0 | 0.0 | |
| 1022+99.17 | 9.27' | | 1023+13.78 | 9.42' | RAMP J | DS | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| 1023+13.78 | 9.42' | | 1025+99.89 | 12.86' | RAMP J | DS | 287.5 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 189.9 | 3.0 | 0.0 | |
| 1025+99.89 | 12.86' | | 1026+12.32 | 13.11' | RAMP J | DS | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| 11+96.29 | -20.00' | * | 12+45.6 | -20.00' | RAMP N | PS | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | |
| 12+45.6 | -20.00' | * | 18+00.4 | -20.00' | RAMP N | PS | 562.5 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 6.0 | 0.0 | |
| 18+00.4 | -20.00' | * | 18+12.73 | -20.00' | RAMP N | PS | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| 7+89.27 | 6.00' | * | 8+39.44 | 6.00' | RAMP N | DS | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | |
| 8+39.44 | 6.00' | * | 26+87.12 | 3.10' | RAMP N | DS | 1,837.5 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 593.4 | 18.0 | 0.0 | |
| 39+99.97 | -59.10' | * | 30+64.02 | -55.15 | I-55B | NBDS | 937.5 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 846.9 | 4.0 | 0.0 | |
| 30+64.02 | -55.15' | * | 30+51.21 | -55.10 | I-55B | NBDS | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| 60+93.63 | 18.60' | | 60+43.63 | 18.60' | I-55B | NBPS | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | |
| 60+43.63 | 18.60' | | 59+43.63 | 18.65' | I-55B | NBPS | 100.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 170.3 | 1.0 | 0.0 | |
| 59+43.63 | 18.65' | | 59+00.48 | 18.46' | I-55B | NBPS | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 1.0 | 0.0 | |
| 57+17.11 | -13.55' | | 57+02.53 | -13.42' | I-55B | NBPS | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| 57+02.53 | -13.42' | | 53+89.99 | -13.39' | I-55B | NBPS | 325.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 419.0 | 3.0 | 0.0 | |
| 53+89.99 | -13.39' | | 53+47.09 | -13.49' | I-55B | NBPS | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | |
| 37+70.61 | -13.86' | * | 37+20.61 | -13.97' | I-55B | NBPS | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 1.0 | |
| 37+20.61 | -13.97' | * | 36+08.11 | -14.24' | I-55B | NBPS | 112.5 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 127.4 | 2.0 | 0.0 | |
| 36+08.11 | -14.24' | * | 35+95.61 | -14.27' | I-55B | NBPS | 0.0 | 0.0 | 1.0 | 0.0 | 0.0 | 0.0 | 0.0 | 1.0 | 0.0 | |
| TOTALS = | | | | | | | 7,612.5 | 13.0 | 7.0 | 6.0 | 8.0 | 12.0 | 7,452.4 | 101.0 | 13.0 | |
| | | | | | | | | | | | | | ROUND TO: | 7,460.0 | 105.0 | |

*GUARDRAIL TO BE PLACED AT THE EDGE OF THE SHOULDER

| STATION | TO | STATION | ROUTE | LANE | VERTICAL ADJUST OF GUARDRAIL X6330900 (FOOT) |
|---------|----|---------|-------|------|----------------------------------------------|
| 63+50. | | 65+00. | I-55B | SBDL | 150.0 |

| STATION | TO | STATION | ROUTE | LANE | VERTICAL ADJUST OF TRAFFIC BARRIER TERMINAL X6330200 (EACH) |
|---------|----|---------|-------|------|-------------------------------------------------------------|
| 63+00. | | 63+50. | I-55B | SBDL | 1.0 |

SCHEDULES OF QUANTITIES

TEMPORARY CONCRETE BARRIER WALL

| TEMPORARY CONCRETE BARRIER 70400100 | | | | | | |
|----------------------------------------|------------|-------|---------|----|---------|---------|
| ROADWAY | LANE | STAGE | STATION | TO | STATION | LENGTH |
| FAP 704 | SOUTHBOUND | 1 | 47+25 | | 60+80 | 1,340.0 |
| FAP 704 | NORTHBOUND | 1 | 46+50 | | 62+15 | 1,590.0 |
| FAI 74 | WBDL | 1A | 634+70 | | 637+40 | 270.0 |
| FAI 74 | WBPL | 1A | 634+90 | | 637+40 | 250.0 |
| FAI 74 | EBDL | 1A | 637+10 | | 639+30 | 220.0 |
| FAI 74 | EBPL | 1A | 637+10 | | 639+30 | 220.0 |
| TOTAL = 3,890.0 | | | | | | |

TEMPORARY CONCRETE BARRIER WALL (CONTINUED)

| IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3 70600350 | | | | | |
|-----------------------------------------------------------------------------|-----------|-------|---------|-----|------|
| ROADWAY | LANE | STAGE | STATION | # | EACH |
| FAI 74 | WESTBOUND | 1B | 637+40 | 2.0 | |
| FAI 74 | EASTBOUND | 1B | 637+10 | 2.0 | |
| FAI 74 | WESTBOUND | 2A | 637+40 | 2.0 | |
| FAI 74 | EASTBOUND | 2A | 637+10 | 2.0 | |
| FAI 74 | WESTBOUND | 2B | 637+40 | 2.0 | |
| FAI 74 | EASTBOUND | 2B | 637+10 | 2.0 | |
| TOTAL = 12.0 | | | | | |

| IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE, NARROW), TEST LEVEL 3 70600352 | | | | |
|-------------------------------------------------------------------------------------|------------|-------|---------|--------|
| ROADWAY | LANE | STAGE | STATION | # EACH |
| FAP 704 | NORTHBOUND | 2 | 62+15 | 1.0 |
| FAP 704 | SOUTHBOUND | 2 | 47+25 | 1.0 |
| TOTAL = 2.0 | | | | |

| RELOCATE TEMPORARY CONCRETE BARRIER 70400200 | | | | | | |
|-------------------------------------------------|------------|-------|---------|----|---------|---------|
| ROADWAY | LANE | STAGE | STATION | TO | STATION | LENGTH |
| FAP 704 | SOUTHBOUND | 2 | 47+25 | | 60+80 | 1,340.0 |
| FAP 704 | NORTHBOUND | 2 | 46+50 | | 62+15 | 1,590.0 |
| FAI 74 | WBDL | 1B | 634+70 | | 637+40 | 270.0 |
| FAI 74 | WBPL | 1B | 634+90 | | 637+40 | 250.0 |
| FAI 74 | EBDL | 1B | 637+10 | | 639+30 | 220.0 |
| FAI 74 | EBPL | 1B | 637+10 | | 639+30 | 220.0 |
| FAI 74 | WBDL | 2A | 634+70 | | 637+40 | 270.0 |
| FAI 74 | WBPL | 2A | 634+90 | | 637+40 | 250.0 |
| FAI 74 | EBDL | 2A | 637+10 | | 639+30 | 220.0 |
| FAI 74 | EBPL | 2A | 637+10 | | 639+30 | 220.0 |
| FAI 74 | WBDL | 2B | 634+70 | | 637+40 | 270.0 |
| FAI 74 | WBPL | 2B | 634+90 | | 637+40 | 250.0 |
| FAI 74 | EBDL | 2B | 637+10 | | 639+30 | 220.0 |
| FAI 74 | EBPL | 2B | 637+10 | | 639+30 | 220.0 |
| TOTAL = 5,810.0 | | | | | | |

BARRIER WALL MARKER, TYPE C

| LOCATION/DESCRIPTION | LENGTH (FOOT) | BARRIER WALL MARKER, TYPE C 78200530 EACH |
|---------------------------------|------------------|-------------------------------------------------|
| S.N. 057-0254 SBDS PARAPET WALL | 204.50 | 2.0 |
| S.N. 057-0252 SBDS PARAPET WALL | 183.00 | 2.0 |
| S.N. 057-0254 SBPS PARAPET WALL | 204.50 | 3.0 |
| S.N. 057-0252 SBPS PARAPET WALL | 183.00 | 1.0 |
| S.N. 057-0253 NBDS PARAPET WALL | 204.50 | 2.0 |
| S.N. 057-0251 NBDS PARAPET WALL | 183.00 | 2.0 |
| S.N. 057-0253 NBPS PARAPET WALL | 204.50 | 1.0 |
| S.N. 057-0251 NBPS PARAPET WALL | 183.00 | 2.0 |
| TOTAL = | | 15.0 |
| ROUND TO: | | 15.0 |

OVERHEAD SIGN STRUCTURES

| ROADWAY | EXISTING SIGN STRUCTURE NUMBER | PROPOSED SIGN STRUCTURE NUMBER | SIGN | 72000300 SIGN PANEL - TYPE 3 (SQ FT) | 72400330 REMOVE SIGN PANEL - TYPE 3 (SQ FT) | 73304000 OVERHEAD SIGN STRUCTURE - BRIDGE MOUNTED (FOOT) | 73600100 REMOVE OVERHEAD SIGN STRUCTURE - SPAN (EACH) | 73600200 REMOVE OVERHEAD SIGN STRUCTURE - CANTILEVER (EACH) | 73700300 REMOVE CONCRETE FOUNDATION - OVERHEAD (EACH) |
|-----------|--------------------------------------|--------------------------------------|------|--------------------------------------------------|------------------------------------------------------------|-------------------------------------------------------------------------|----------------------------------------------------------------------|----------------------------------------------------------------------------|----------------------------------------------------------------------|
| I-74 EB | 5 S 057 I074 R133.90 | 5 B 057 I074 R133.90 | A | 152.00 | 160.0 | 45.5 | 1.0 | 0.0 | 4.0 |
| | | | B | 143.75 | 124.5 | | | | |
| | | | C | 200.00 | 211.0 | | | | |
| I-74 WB | 5 C 057 I074 L133.90 | 5 B 057 I074 L133.90 | D | 150.00 | 151.0 | 15.0 | 0.0 | 1.0 | 1.0 |
| TOTALS: | | | | 645.75 | 646.5 | 60.5 | 1.0 | 1.0 | 5.0 |
| ROUND TO: | | | | 646.00 | 650.0 | 60.5 | 1.0 | 1.0 | 5.0 |

| IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3 70600250 | | | | | |
|------------------------------------------------------------------------------|-----------|-------|---------|-----|------|
| ROADWAY | LANE | STAGE | STATION | # | EACH |
| FAI 74 | WESTBOUND | 1A | 637+40 | 2.0 | |
| FAI 74 | EASTBOUND | 1A | 637+10 | 2.0 | |
| TOTAL = 4.0 | | | | | |

| IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE, NARROW), TEST LEVEL 3 70600251 | | | | | |
|--------------------------------------------------------------------------------------|------------|-------|---------|-----|------|
| ROADWAY | LANE | STAGE | STATION | # | EACH |
| FAP 704 | NORTHBOUND | 1 | 62+15 | 1.0 | |
| FAP 704 | SOUTHBOUND | 1 | 47+25 | 1.0 | |
| TOTAL = 2.0 | | | | | |

SURVEY MARKERS

| | STATION | OFFSET | XZ193300 SURVEY MARKER TYPE 1 (SPECIAL) (EACH) | XZ193400 SURVEY MARKER TYPE 2 (SPECIAL) (EACH) |
|------------------------------|----------|--------|---------------------------------------------------------------|---------------------------------------------------------------|
| SOUTHBOUND VETERAN'S PARKWAY | | | | |
| POC #300 | 58+98.61 | 0.0' | 1 | 0 |
| NORTHBOUND VETERAN'S PARKWAY | | | | |
| POT #47 | 59+00.00 | 0.0' | 0 | 1 |
| TOTALS: | | | 1 | 1 |

SCHEDULES OF QUANTITIES

PAVEMENT MARKING REMOVAL

| LOCATION | STA | TO | STA | EDGE LINE LENGTH (FOOT) | SKIP DASH LENGTH (FOOT) | STRIPE WIDTH (INCHES) | PAVEMENT MARKING REMOVAL 78300100 (SQ FT) |
|-------------------------|-----------|----|-----------|-------------------------|-------------------------|-----------------------|-------------------------------------------|
| NBDL SHOULDER EL | 43+70.00 | | 47+80.00 | 410.00 | | 8 | 273.3 |
| NBDL SHOULDER EL | 47+80.00 | | 59+00.00 | 1120.00 | | 4 | 373.3 |
| NBDL SHOULDER EL | 58+80.00 | | 61+10.00 | 230.00 | | 8 | 153.3 |
| NBDL SHOULDER EL | 61+10.00 | | 65+90.00 | 480.00 | | 4 | 160.0 |
| SBDL SHOULDER EL | 42+10.00 | | 45+10.00 | 300.00 | | 4 | 100.0 |
| SBDL SHOULDER EL | 45+10.00 | | 46+30.00 | 120.00 | | 8 | 80.0 |
| SBDL SHOULDER EL | 50+50.00 | | 60+00.00 | 950.00 | | 4 | 316.7 |
| SBDL SHOULDER EL | 60+85.00 | | 62+30.00 | 145.00 | | 8 | 96.7 |
| SBDL SHOULDER EL | 62+30.00 | | 65+90.00 | 360.00 | | 4 | 120.0 |
| RAMP E SHOULDER EL - RT | 23+00.00 | | 22+25.00 | 75.00 | | 4 | 25.0 |
| RAMP E SHOULDER EL - RT | 22+25.00 | | 21+10.00 | 115.00 | | 8 | 76.7 |
| RAMP E SHOULDER EL - LT | 23+00.00 | | 17+00.00 | 600.00 | | 4 | 200.0 |
| RAMP F SHOULDER EL - RT | 10+00.00 | | 13+00.00 | 300.00 | | 4 | 100.0 |
| RAMP F SHOULDER EL - LT | 10+80.00 | | 12+20.00 | 140.00 | | 8 | 93.3 |
| RAMP F SHOULDER EL - LT | 12+20.00 | | 13+00.00 | 80.00 | | 4 | 26.7 |
| RAMP J SHOULDER EL - LT | 13+00.00 | | 15+70.00 | 270.00 | | 8 | 180.0 |
| RAMP J SHOULDER EL - RT | 13+00.00 | | 15+55.00 | 255.00 | | 4 | 85.0 |
| RAMP N SHOULDER EL - LT | 18+00.00 | | 19+00.00 | 100.00 | | 4 | 33.3 |
| RAMP N SHOULDER EL - LT | 19+00.00 | | 23+10.00 | 410.00 | | 8 | 273.3 |
| RAMP N SHOULDER EL - RT | 18+00.00 | | 24+70.00 | 670.00 | | 4 | 223.3 |
| I-74 WB CL | 633+70.00 | | 652+40.00 | | 467.5 | 4 | 155.8 |
| TOTAL: | | | | | | | 3145.7 |
| ROUND TO: | | | | | | | 3150.0 |

MODIFIED URETHANE PAVEMENT MARKING, LINE 4"

| LOCATION | STA | TO | STA | COLOR | EDGE LINE LENGTH (FOOT) | SKIP DASH LENGTH (FOOT) | MODIFIED URETHANE PAVEMENT MARKING, LINE 4" 78009004 (FOOT) |
|---------------------|------------|----|------------|--------|-------------------------|-------------------------|-------------------------------------------------------------|
| I-55 NBDL EDGE LINE | 32+60.00 | | 40+00.00 | WHITE | 740.0 | | 740.0 |
| RAMP N DS EDGE LINE | 26+87.12 | | 6+00.00 | WHITE | 2087.1 | | 2,087.1 |
| RAMP N PS EDGE LINE | 19+18.65 | | 6+00.00 | YELLOW | 1318.7 | | 1,318.7 |
| I-55 NBDL EDGE LINE | 47+50.00 | | 48+61.00 | WHITE | 111.0 | | 111.0 |
| RAMP J DS EDGE LINE | 1026+17.51 | | 1107+00.00 | WHITE | 1917.5 | | 1,917.5 |
| RAMP J PS EDGE LINE | 1013+66.89 | | 1007+00.00 | YELLOW | 666.9 | | 666.9 |
| I-55 NBDL EDGE LINE | 60+87.96 | | 65+90.00 | WHITE | 502.0 | | 502.0 |
| I-55 NBPL EDGE LINE | 32+60.00 | | 65+00.00 | YELLOW | 3240.0 | | 3,240.0 |
| I-55 SBDL EDGE LINE | 29+00.00 | | 30+00.00 | WHITE | 100.0 | | 100.0 |
| RAMP M DS EDGE LINE | 30+00.00 | | 37+79.28 | WHITE | 779.3 | | 779.3 |
| RAMP M PS EDGE LINE | 37+44.28 | | 37+79.28 | YELLOW | 35.0 | | 35.0 |
| I-55 SBDL EDGE LINE | 37+34.68 | | 45+25.00 | WHITE | 790.3 | | 790.3 |
| RAMP E PS EDGE LINE | 26+00.00 | | 22+06.50 | YELLOW | 393.5 | | 393.5 |
| RAMP E DS EDGE LINE | 26+00.00 | | 16+80.97 | WHITE | 919.0 | | 919.0 |
| I-55 SBDL EDGE LINE | 50+67.95 | | 59+89.41 | WHITE | 921.5 | | 921.5 |
| RAMP F DS EDGE LINE | 9+92.00 | | 23+00.00 | WHITE | 1308.0 | | 1,308.0 |
| RAMP F PS EDGE LINE | 12+02.10 | | 23+00.00 | YELLOW | 1097.9 | | 1,097.9 |
| I-55 SBDL EDGE LINE | 62+11.32 | | 65+90.00 | WHITE | 378.7 | | 378.7 |
| I-55 SBPL EDGE LINE | 29+00.00 | | 65+00.00 | YELLOW | 3600.0 | | 3,600.0 |
| I-55 NB CL | 51+26.10 | | 53+86.10 | WHITE | | 65.0 | 65.0 |
| I-55 NB CL | 56+89.46 | | 59+30.97 | WHITE | | 60.4 | 60.4 |
| I-55 SB CL | 50+84.43 | | 53+53.69 | WHITE | | 67.3 | 67.3 |
| I-55 SB CL | 56+81.78 | | 59+28.28 | WHITE | | 61.6 | 61.6 |
| I-74 WB CL | 633+70.00 | | 652+40.00 | WHITE | | 467.5 | 467.5 |
| I-74 WB EDGE LINE | 633+00.00 | | 638+00.00 | WHITE | 500.0 | | 500.0 |
| I-74 WB EDGE LINE | 633+00.00 | | 638+00.00 | YELLOW | 500.0 | | 500.0 |
| I-74 EB EDGE LINE | 636+00.00 | | 641+00.00 | WHITE | 500.0 | | 500.0 |
| I-74 EB EDGE LINE | 636+00.00 | | 641+00.00 | YELLOW | 500.0 | | 500.0 |
| WHITE TOTAL: | | | | | | | 12,276.2 |
| YELLOW TOTAL: | | | | | | | 11,352.0 |
| TOTAL: | | | | | | | 23,628.2 |
| ROUND TO: | | | | | | | 23,650.0 |

MODIFIED URETHANE PAVEMENT MARKING, LINE 8"

| LOCATION | STA | TO | STA | COLOR | MODIFIED URETHANE PAVEMENT MARKING, LINE 8" 78009008 (FOOT) |
|----------------|------------|----|------------|-------|-------------------------------------------------------------|
| RAMP N PS GORE | 23+51.06 | | 19+18.65 | WHITE | 432.4 |
| I-55 NBDL GORE | 43+27.96 | | 47+50.00 | WHITE | 422.0 |
| RAMP J PS GORE | 1018+73.08 | | 1013+66.89 | WHITE | 506.2 |
| I-55 NBDL GORE | 55+89.36 | | 60+87.96 | WHITE | 498.6 |
| RAMP M PS GORE | 35+25.58 | | 37+44.28 | WHITE | 218.7 |
| I-55 SBDL GORE | 35+16.10 | | 37+34.68 | WHITE | 218.6 |
| RAMP E PS GORE | 19+98.70 | | 22+06.50 | WHITE | 207.8 |
| I-55 SBDL GORE | 47+41.69 | | 45+25.00 | WHITE | 216.7 |
| RAMP F PS GORE | 10+71.94 | | 12+02.10 | WHITE | 130.2 |
| I-55 SBDL GORE | 60+75.83 | | 62+11.32 | WHITE | 135.5 |
| WHITE TOTAL: | | | | | 2986.7 |
| YELLOW TOTAL: | | | | | 0.0 |
| TOTAL: | | | | | 2,986.7 |
| ROUND TO: | | | | | 2,990.0 |

PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, LINE 4"

| LOCATION | STA | TO | STA | COLOR | LENGTH (FOOT) | SKIP DASH LENGTH (FOOT) | PREFORMED PLASTIC PAVEMENT MARKING, TYPE B, LINE 4" 78003110 (FOOT) |
|---------------|----------|----|----------|-------|---------------|-------------------------|---------------------------------------------------------------------|
| I-55 NB CL | 32+60.00 | | 51+26.10 | WHITE | 1866.1 | 466.5 | 470.0 |
| I-55 NB CL | 53+86.10 | | 56+89.46 | WHITE | 303.4 | 75.8 | 80.0 |
| I-55 NB CL | 59+30.97 | | 65+00.00 | WHITE | 569.0 | 142.3 | 150.0 |
| I-55 SB CL | 29+00.00 | | 50+84.43 | WHITE | 2184.4 | 546.1 | 550.0 |
| I-55 SB CL | 53+53.69 | | 56+81.78 | WHITE | 328.1 | 82.0 | 90.0 |
| I-55 SB CL | 59+28.28 | | 65+00.00 | WHITE | 571.7 | 142.9 | 150.0 |
| WHITE TOTAL: | | | | | | | 1,490.0 |
| YELLOW TOTAL: | | | | | | | 0.0 |
| TOTAL: | | | | | | | 1,490.0 |
| ROUND TO: | | | | | | | 1,490.0 |

SCHEDULES OF QUANTITIES

REMOVAL ITEMS

| DESCRIPTION | STATION | TO | STATION | ROUTE | LANE | AREA MEAS FROM CADD (SQ FT) | PAVEMENT REMOVAL 44000100 (SQ YD) | GUTTER REMOVAL 44000400 (FOOT) | PAVED DITCH REMOVAL 44004000 (FOOT) | PAVED SHOULDER REMOVAL 44004250 (SQ YD) | |
|-----------------------|-----------|----|-----------|-------|--------|-----------------------------|-----------------------------------|--------------------------------|-------------------------------------|-----------------------------------------|--|
| EXCESS SHOULDER | 637+80.00 | | 639+25.00 | I-74 | EBDS | 559.4 | | | | 62.2 | |
| EXCESS SHOULDER | 62+11.32 | | 64+11.76 | I-55B | SBDS | 919.5 | | | | 102.2 | |
| SE 057-0061 APPROACH | 51+27.94 | | 51+95.12 | I-55B | NBDS | 385.0 | | | | 42.8 | |
| SW 057-0061 APPROACH | 53+51.05 | | 54+01.98 | I-55B | NBDS | 197.4 | | | | 21.9 | |
| SE 057-0059 APPROACH | 56+86.74 | | 57+36.96 | I-55B | NBDS | 423.1 | | | | 47.0 | |
| SW 057-0059 APPROACH | 58+83.59 | | 59+35.39 | I-55B | NBDS | 266.1 | | | | 29.6 | |
| NE 057-0061 APPROACH | 51+11.78 | | 51+80.24 | I-55B | NBPS | 327.5 | | | | 36.4 | |
| NW 057-0061 APPROACH | 53+34.75 | | 53+85.68 | I-55B | NBPS | 207.6 | | | | 23.1 | |
| NE 057-0059 APPROACH | 56+82.01 | | 57+33.39 | I-55B | NBPS | 206.7 | | | | 23.0 | |
| NW 057-0059 APPROACH | 58+82.12 | | 59+34.93 | I-55B | NBPS | 250.3 | | | | 27.8 | |
| SE 057-0062 APPROACH | 50+86.94 | | 51+53.36 | I-55B | SBPS | 334.4 | | | | 37.2 | |
| SW 057-0062 APPROACH | 53+12.16 | | 53+66.00 | I-55B | SBPS | 228.8 | | | | 25.4 | |
| SE 057-0060 APPROACH | 56+77.21 | | 57+29.05 | I-55B | SBPS | 192.3 | | | | 21.4 | |
| SW 057-0060 APPROACH | 58+79.78 | | 59+34.49 | I-55B | SBPS | 288.4 | | | | 32.0 | |
| NE 057-0062 APPROACH | 50+61.84 | | 51+28.66 | I-55B | SBDS | 330.7 | | | | 36.7 | |
| NW 057-0062 APPROACH | 52+91.15 | | 53+47.54 | I-55B | SBDS | 345.3 | | | | 38.4 | |
| NE 057-0060 APPROACH | 56+72.61 | | 57+25.69 | I-55B | SBDS | 243.0 | | | | 27.0 | |
| NW 057-0060 APPROACH | 58+78.41 | | 59+34.09 | I-55B | SBDS | 273.6 | | | | 30.4 | |
| TYPE F INLET | 53+54.87 | | 53+65.13 | I-55B | SBDS | 53.5 | | | | 5.9 | |
| TYPE E INLET | 59+45.00 | | 59+55.00 | I-55B | SBDS | 52.3 | | | | 5.8 | |
| MEDIAN DITCH | 62+98.00 | | 64+48.00 | I-55B | MEDIAN | NA | | 150.0 | | | |
| E 057-0062 & 057-0061 | 634+74.94 | | 636+99.15 | I-74 | WBDS | NA | 224.2 | | | | |
| W 057-0062 & 057-0061 | 635+08.42 | | 637+25.33 | I-74 | WBPS | NA | 216.9 | | | | |
| E 057-0060 & 057-0059 | 637+79.87 | | 639+57.28 | I-74 | EBPS | NA | 177.4 | | | | |
| W 057-0060 & 057-0059 | 637+85.79 | | 639+50.1 | I-74 | EBDS | NA | 164.3 | | | | |
| 057-0061 EAST | 51+19.65 | | 51+87.57 | I-55B | NB | 1,706.6 | 189.6 | | | | |
| 057-0061 WEST | 53+41.48 | | 53+91.76 | I-55B | NB | 1,638.5 | 182.1 | | | | |
| 057-0059 EAST | 56+83.47 | | 57+34.27 | I-55B | NB | 2,052.8 | 228.1 | | | | |
| 057-0059 WEST | 58+82.75 | | 59+35.06 | I-55B | NB | 2,275.1 | 252.8 | | | | |
| 057-0062 EAST | 50+78.71 | | 51+44.7 | I-55B | SB | 2,362.0 | 262.4 | | | | |
| 057-0062 WEST | 53+05.4 | | 53+59.76 | I-55B | SB | 1,937.3 | 215.3 | | | | |
| 057-0060 EAST | 56+75.69 | | 57+27.82 | I-55B | SB | 1,851.6 | 205.7 | | | | |
| 057-0060 WEST | 58+79.31 | | 59+34.36 | I-55B | SB | 1,947.9 | 216.4 | | | | |
| TOTALS = | | | | | | | 1,752.4 | 782.8 | 150.0 | 676.1 | |
| ROUND TO: | | | | | | | 1,755.0 | 785.0 | 150.0 | 680.0 | |

PAVEMENT BREAKING

| STATION | TO | STATION | ROUTE | PROCESSING MODIFIED SOIL 12" 30200650 (SQ YD) | LIME 30201500 (TON) | SUBBASE GRANULAR MATERIAL, TYPE C 31101900 (TON) | PAVEMENT BREAKING Z0004638 (SQ YD) |
|----------|----|----------|--------|-----------------------------------------------|---------------------|--------------------------------------------------|------------------------------------|
| 10+73.00 | | 19+02.00 | RAMP N | 2,500.0 | 57.0 | 205.0 | 2,400.0 |

SPEED INDICATOR SIGN

| LOCATION | X0323583 SPEED INDICATOR SIGN (CAL DA) |
|----------------|----------------------------------------|
| I-74 EASTBOUND | 306 |
| I-74 WESTBOUND | 306 |
| TOTAL = | 612 |

SCHEDULES OF QUANTITIES

RAISED REFLECTIVE PAVEMENT MARKERS

| LOCATION | STA | TO | STA | LENGTH (FOOT) | MARKER SPACING (FOOT) | RAISED REFLECTIVE PAVEMENT MARKER 78100100 (EACH) | RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) 78100105 (EACH) | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL 78300200 (EACH) |
|--------------|-----------|----|----------|---------------|-----------------------|---------------------------------------------------|------------------------------------------------------------|-----------------------------------------------------------|
| * I-55 NB CL | 32+60.00 | | 51+26.10 | 1866.1 | 80 | 48 | | 48 |
| * I-55 NB CL | 53+86.10 | | 56+89.46 | 303.4 | 80 | 8 | | 8 |
| * I-55 NB CL | 59+30.97 | | 65+00.00 | 569.0 | 80 | 16 | | 16 |
| * I-55 SB CL | 29+00.00 | | 50+84.43 | 2184.4 | 80 | 56 | | 56 |
| * I-55 SB CL | 53+53.69 | | 56+81.78 | 328.1 | 80 | 10 | | 10 |
| * I-55 SB CL | 59+28.28 | | 65+00.00 | 571.7 | 80 | 16 | | 16 |
| RAMP M PS | 35+25.58 | | 37+44.28 | 218.7 | 40 | 6 | | 6 |
| RAMP M DS | 31+30.00 | | 37+44.28 | 614.3 | 20 | 31 | | 31 |
| I-55 SBDL | 35+16.10 | | 37+34.68 | 218.6 | 40 | 6 | | 6 |
| RAMP F PS | 10+71.94 | | 12+02.10 | 130.2 | 40 | 4 | | 4 |
| RAMP F DS | 9+92.00 | | 12+02.10 | 210.1 | 20 | 11 | | 11 |
| I-55 SBDL | 60+75.83 | | 62+11.32 | 135.5 | 40 | 4 | | 4 |
| * I-55 SB CL | 50+84.43 | | 53+53.69 | 269.26 | 80 | | 8 | |
| * I-55 SB CL | 56+81.78 | | 59+28.28 | 246.5 | 80 | | 8 | |
| * I-55 NB CL | 51+26.10 | | 53+86.10 | 260.0 | 80 | | 8 | |
| * I-55 NB CL | 56+89.46 | | 59+30.97 | 241.5 | 80 | | 6 | |
| I-74 WB CL | 633+70.00 | | 652+40.0 | 1870.0 | 80 | 48 | | 48 |
| TOTAL = | | | | | | 264 | 30 | 264 |
| ROUND : | | | | | | 270 | 30 | 270 |

* = DENOTES DOUBLE LANE LINE MARKER LOCATION

CHANGEABLE MESSAGE SIGN

| CHANGEABLE MESSAGE SIGN | X7015005 7 DAYS BEFORE CONSTRUCTION/ ROAD CLOSURES BEGIN (CAL DA) |
|----------------------------------------------|----------------------------------------------------------------------------------|
| LOCATION | |
| I-74 NORTHWEST OF THE JOB (EB LANES) | 7 |
| I-74 EAST OF THE JOB (WB LANES) | 7 |
| I-55 SOUTH OF THE JOB (NB LANES) | 7 |
| VETERANS PKWY NORTH OF THE JOB (SB LANES) | 7 |
| RAMP E (STAGE 2A & 2B) | 7 |
| RAMP F - CLOSED 2 TIMES (STAGE 1A, 2A, & 2B) | 14 |
| RAMP J (STAGE 2A & 2B) | 7 |
| RAMP N (LATE IN STAGE 2B) | 7 |
| FAI-74 WESTBOUND (STAGE 1) NIGHTTIME WORK | 7 |
| FAI-74 EASTBOUND (STAGE 1) NIGHTTIME WORK | 7 |
| FAI-74 WESTBOUND (STAGE 2) NIGHTTIME WORK | 7 |
| FAI-74 EASTBOUND (STAGE 2) NIGHTTIME WORK | 7 |
| TOTALS = | 91 |

NOTE: THE PERMANENT MESSAGE BOARDS AROUND THE AREA WILL BE USED AS NECESSARY FOR CONSTRUCTION INFORMATION.

BASE COURSE (OPTION)

| STATION | TO | STATION | ROUTE | LANE | LENGTH MEAS. FROM CADD (FT) | FROM EOS OUT WIDTH (FT) | BASE COURSE (OPTION) Z0002900 (SQ YD) |
|-----------|----|-----------|--------|------|-----------------------------|-------------------------|---------------------------------------|
| 25+50. | | 16+91. | RAMP E | DS | 844.6 | 2.0 | 187.7 |
| 25+00. | | 22+03. | RAMP E | PS | 327.0 | 2.0 | 72.7 |
| 53+65 | | 56+72. | I-55B | SBDS | 299.0 | 2.0 | 66.4 |
| 59+55. | | 59+92. | I-55B | SBDS | 36.4 | 2.0 | 8.1 |
| 9+92. | | 10+25. | RAMP F | DS | 32.7 | 2.0 | 7.3 |
| 10+25. | | 14+25. | RAMP F | DS | 392.4 | 3.0 | 130.8 |
| 14+25. | | 20+75. | RAMP F | DS | 631.7 | 2.0 | 140.4 |
| 12+37.1 | | 12+75. | RAMP F | PS | 41.0 | 2.0 | 9.1 |
| 12+75. | | 14+75. | RAMP F | PS | 218.5 | 3.0 | 72.8 |
| 14+75. | | 18+75. | RAMP F | PS | 434.5 | 2.0 | 96.6 |
| 43+75. | | 50+90.5 | I-55B | SBPS | 711.4 | 2.0 | 158.1 |
| 53+69. | | 56+78. | I-55B | SBPS | 307.3 | 3.0 | 102.4 |
| 59+34.5 | | 60+25. | I-55B | SBPS | 90.0 | 3.0 | 30.0 |
| 60+25. | | 64+25. | I-55B | SBPS | 397.7 | 2.0 | 88.4 |
| 64+25. | | 61+08.9 | I-55B | NBDS | 316.1 | 2.0 | 70.2 |
| 1007+25. | | 1013+50.3 | RAMP J | PS | 671.9 | 2.0 | 149.3 |
| 1008+50. | | 1012+00. | RAMP J | DS | 340.5 | 2.0 | 75.7 |
| 1012+00. | | 1012+50. | RAMP J | DS | 49.0 | 3.0 | 16.3 |
| 1012+50. | | 1013+50. | RAMP J | DS | 98.6 | 4.0 | 43.8 |
| 1013+50. | | 1014+50. | RAMP J | DS | 98.5 | 5.0 | 54.7 |
| 1014+50. | | 1015+16.8 | RAMP J | DS | 66.7 | 7.0 | 51.9 |
| 56+87.6 | | 56+25. | I-55B | NBDS | 64.5 | 5.0 | 35.8 |
| 56+25. | | 54+06.5 | I-55B | NBDS | 224.9 | 4.0 | 100.0 |
| 51+35.4 | | 50+25. | I-55B | NBDS | 113.1 | 7.0 | 88.0 |
| 50+25. | | 47+71. | I-55B | NBDS | 260.3 | 2.0 | 57.8 |
| 6+76.6 | | 7+25. | RAMP N | PS | 49.0 | 2.0 | 10.9 |
| 7+25. | | 8+75. | RAMP N | PS | 151.8 | 3.0 | 50.6 |
| 8+75. | | 9+25. | RAMP N | PS | 50.7 | 4.0 | 22.5 |
| 9+25. | | 10+73. | RAMP N | PS | 150.2 | 3.0 | 50.1 |
| 9+25. | | 10+73. | RAMP N | DS | 147.3 | 2.0 | 32.7 |
| 19+02. | | 20+25. | RAMP N | DS | 122.7 | 2.0 | 27.3 |
| 64+75. | | 59+36.5 | I-55B | NBPS | 538.5 | 2.0 | 119.7 |
| 56+81.3 | | 55+25. | I-55B | NBPS | 157.3 | 2.0 | 35.0 |
| 55+25. | | 53+83.3 | I-55B | NBPS | 142.5 | 3.0 | 47.5 |
| 51+08.6 | | 43+25. | I-55B | NBPS | 788.1 | 2.0 | 175.1 |
| TOTAL = | | | | | | 2,485.7 | |
| ROUND TO: | | | | | | 2,500.0 | |



EXIST. CURVE 40
 PI STA. = 27+77.97
 $\Delta = 37^\circ 30' 00''$ (LT)
 $D = 2^\circ 30' 00''$
 $R = 2,291.83'$
 $T = 777.97'$
 $L = 1,500.00'$
 $E = 128.44'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 20+00.00$
 $P.T. STA. = 35+00.00$

EXIST. CURVE 100
 PI STA. = 31+75.22
 $\Delta = 7^\circ 34' 44''$ (LT)
 $D = 2^\circ 09' 57''$
 $R = 2,645.48'$
 $T = 175.22'$
 $L = 349.93'$
 $E = 5.80'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 30+00.00$
 $P.T. STA. = 33+49.93$

EXIST. CURVE 162
 PI STA. = 32+73.94
 $\Delta = 158^\circ 04' 09''$ (LT)
 $D = 28^\circ 00' 00''$
 $R = 204.63'$
 $T = 1,056.12'$
 $L = 564.53'$
 $E = 871.13'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 22+17.82$
 $P.T. STA. = 27+82.36$

EXIST. CURVE 99
 PI STA. = 38+42.49
 $\Delta = 22^\circ 19' 36''$ (RT)
 $D = 8^\circ 17' 58''$
 $R = 690.36'$
 $T = 136.24'$
 $L = 269.02'$
 $E = 13.31'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 37+06.26$
 $P.T. STA. = 39+75.28$

EXIST. CURVE 97
 PI STA. = 45+28.78
 $\Delta = 106^\circ 44' 55''$ (RT)
 $D = 13^\circ 55' 31''$
 $R = 411.45'$
 $T = 553.50'$
 $L = 766.58'$
 $E = 278.23'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 39+75.28$
 $P.T. STA. = 47+41.86$

EXIST. CURVE 34
 PI STA. = 619+20.20
 $\Delta = 39^\circ 22' 04''$ (RT)
 $D = 2^\circ 00' 00''$
 $R = 2,864.78'$
 $T = 1,024.83'$
 $L = 1,968.38'$
 $E = 177.79'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 608+95.37$
 $P.T. STA. = 628+63.75$

EXIST. CURVE 94
 PI STA. = 48+79.23
 $\Delta = 22^\circ 10' 14''$ (RT)
 $D = 8^\circ 10' 19''$
 $R = 701.12'$
 $T = 137.37'$
 $L = 271.30'$
 $E = 13.33'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 47+41.86$
 $P.T. STA. = 50+13.16$

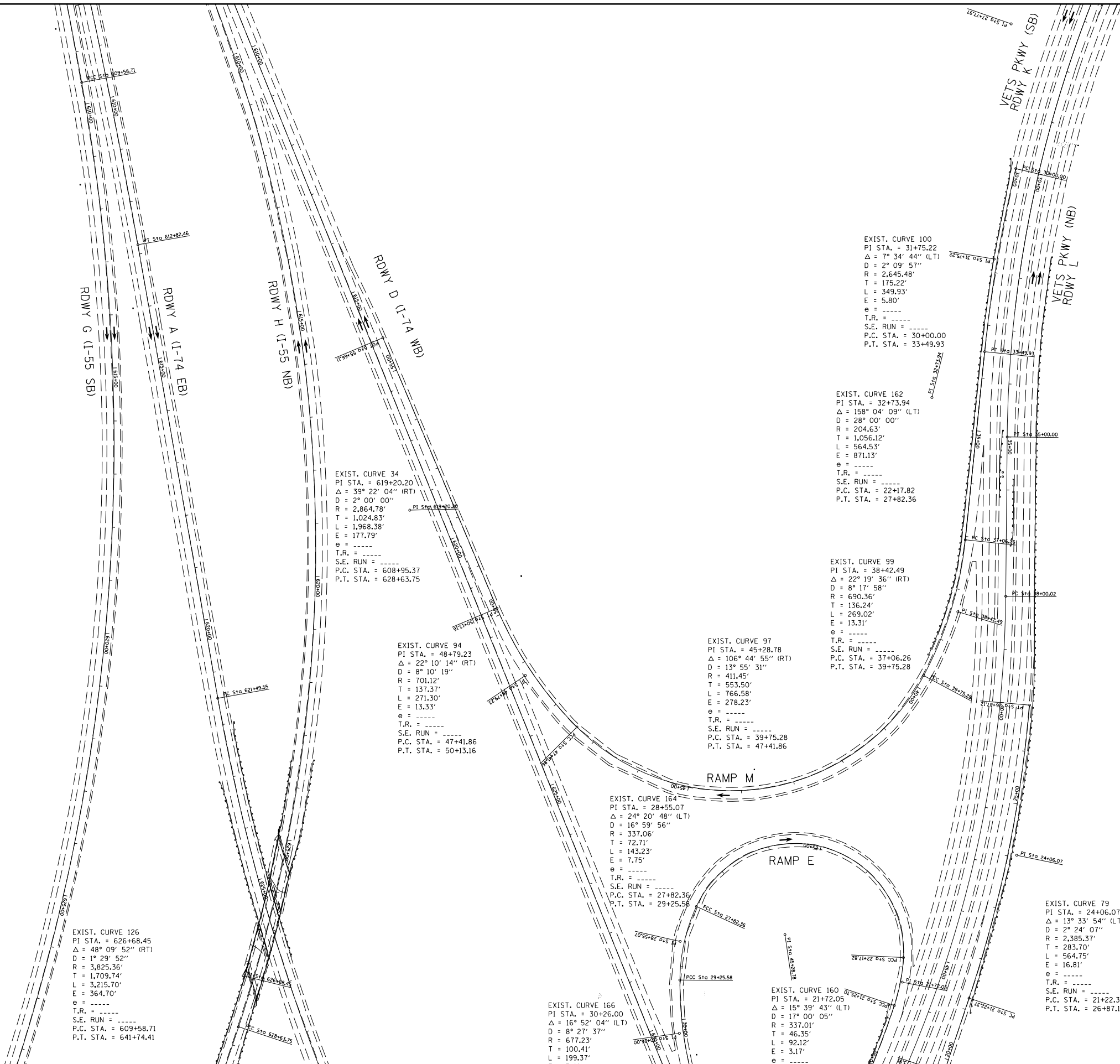
EXIST. CURVE 164
 PI STA. = 28+55.07
 $\Delta = 24^\circ 20' 48''$ (LT)
 $D = 16^\circ 59' 56''$
 $R = 337.06'$
 $T = 72.71'$
 $L = 143.23'$
 $E = 7.75'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 27+82.36$
 $P.T. STA. = 29+25.58$

EXIST. CURVE 166
 PI STA. = 30+26.00
 $\Delta = 16^\circ 52' 04''$ (LT)
 $D = 8^\circ 27' 37''$
 $R = 677.23'$
 $T = 100.41'$
 $L = 199.37'$
 $E = 7.46'$
 $e = \text{-----}$

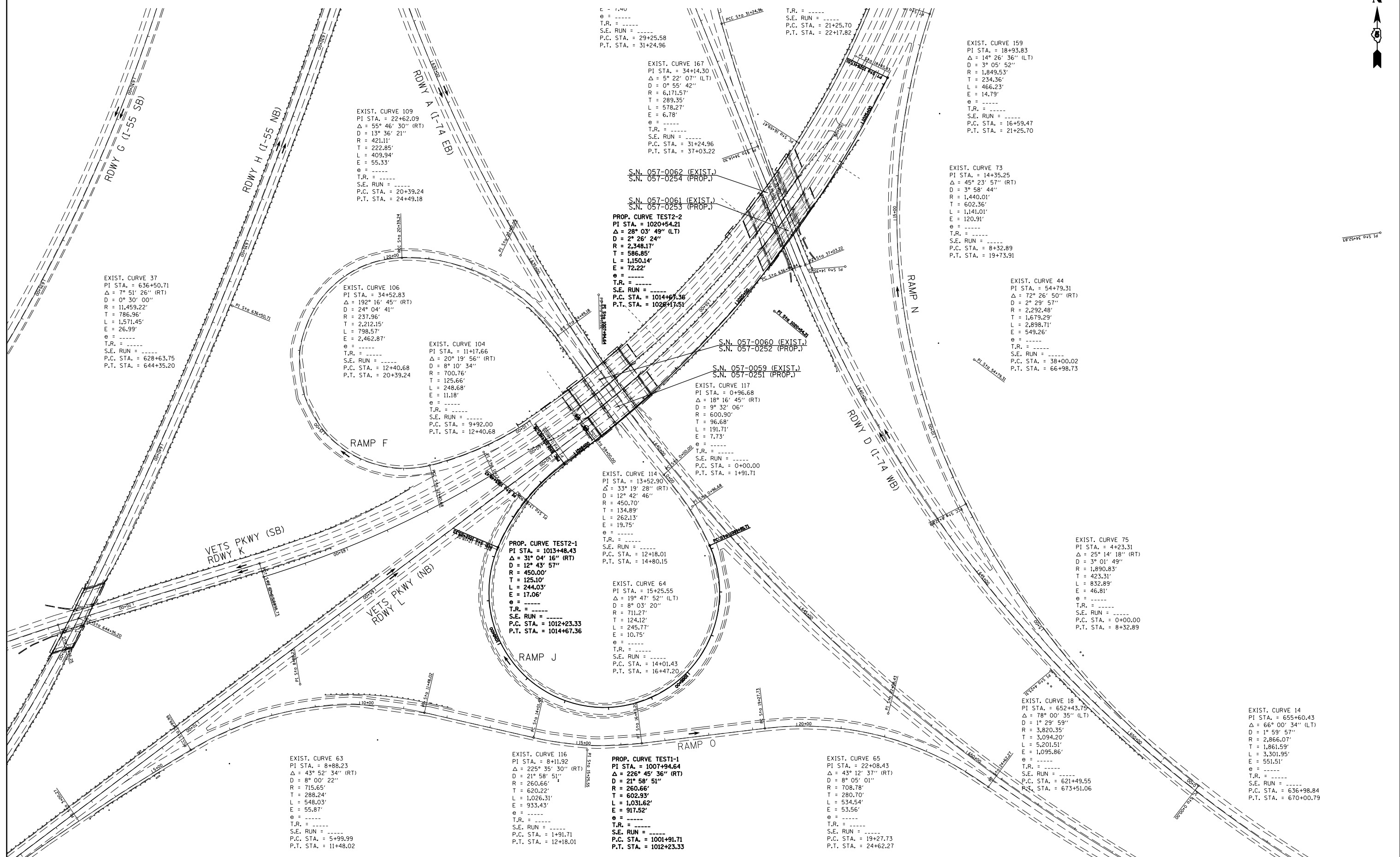
EXIST. CURVE 160
 PI STA. = 21+72.05
 $\Delta = 15^\circ 39' 43''$ (LT)
 $D = 17^\circ 00' 05''$
 $R = 337.01'$
 $T = 46.35'$
 $L = 92.12'$
 $E = 3.17'$
 $e = \text{-----}$

EXIST. CURVE 79
 PI STA. = 24+06.07
 $\Delta = 13^\circ 33' 54''$ (LT)
 $D = 2^\circ 24' 07''$
 $R = 2,385.37'$
 $T = 283.70'$
 $L = 564.75'$
 $E = 16.81'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 21+22.37$
 $P.T. STA. = 26+87.12$

EXIST. CURVE 126
 PI STA. = 626+68.45
 $\Delta = 48^\circ 09' 52''$ (RT)
 $D = 1^\circ 29' 52''$
 $R = 3,825.36'$
 $T = 1,709.74'$
 $L = 3,215.70'$
 $E = 364.70'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 609+58.71$
 $P.T. STA. = 641+74.41$



| | | | | | | | | | | |
|----------------------------------------------------------|----------------------|------------|---------------------------|-----------------------------------------------------------|---------------------------------|--------------------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN SHEET ALIGNMENT | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ei:\pw_work\p\midot\detersbj\0157116\0570570.sht.pln.dgn | DRAWN - | REVISED - | 704 | | | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 60 | |
| PLOT SCALE = 200.0000' / in. | CHECKED - | REVISED - | CONTRACT NO. 70570 | | | | | | | |
| PLOT DATE = 8/13/2013 | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | SCALE: | SHEET NO. 1 OF 2 SHEETS | STA. 26+64.88 TO STA. 47+10.16 | | | | |



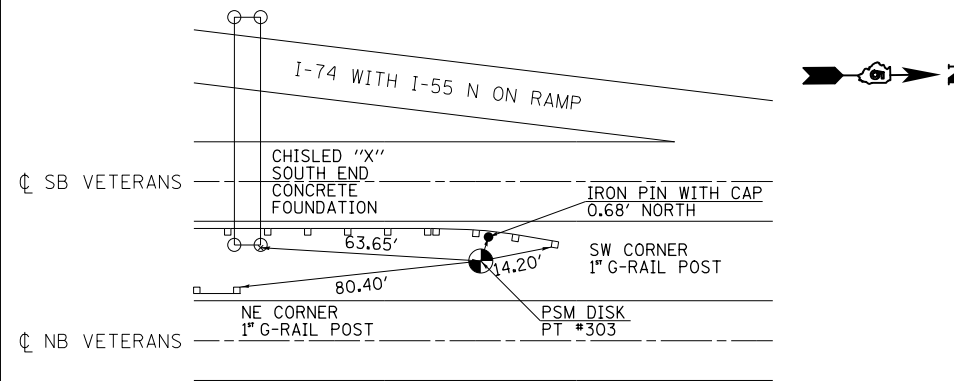
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|--------------------------------------------------------|----------------------|------------|---------------------------|-----------------------------------------------------------|---------------------------------|--------------------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN SHEET ALIGNMENT | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pw\work\p1dot\detersbj\d0157116\0570570-sht-pln.dgn | DRAWN - | REVISED - | 704 | | | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 61 | |
| PLOT SCALE = 200.0000' / in. | CHECKED - | REVISED - | CONTRACT NO. 70570 | | | | | | | |
| PLOT DATE = 8/13/2013 | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | SCALE: | SHEET NO. 2 OF 2 SHEETS | STA. 47+10.16 TO STA. 75+17.79 | | | | |

BENCHMARKS

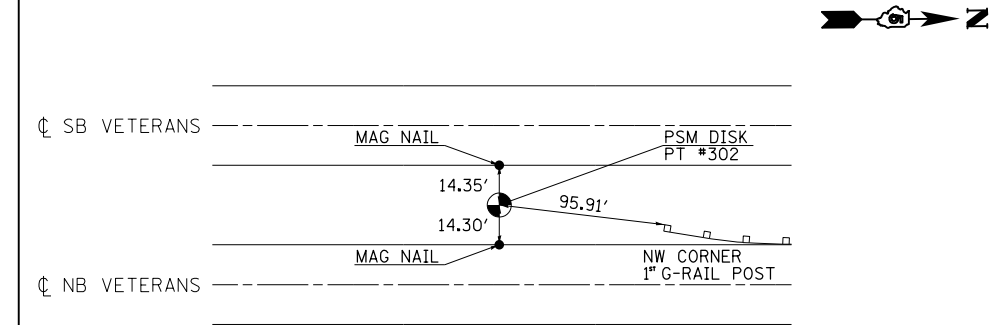
B.M. 4848-1 STA. AND OFFSET NOT STATED
 MCLEAN COUNTY
 CHISELED SQUARE
 SAID MARK IS A CHISELED SQUARE ON THE S.E. WINGWALL OF S.N. 057-0059.
 ELEV. = 809.67

B.M. 4848-2 STA. AND OFFSET NOT STATED
 MCLEAN COUNTY
 CHISELED SQUARE
 SAID MARK IS A CHISELED SQUARE ON THE S.W. WINGWALL OF S.N. 057-0061.
 ELEV. = 813.96

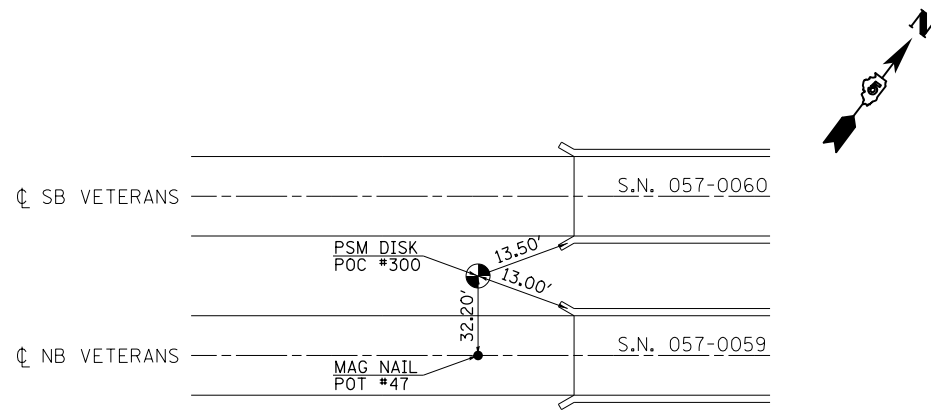
P.T. #303 STATION 35 + 00.00 PSM DISK



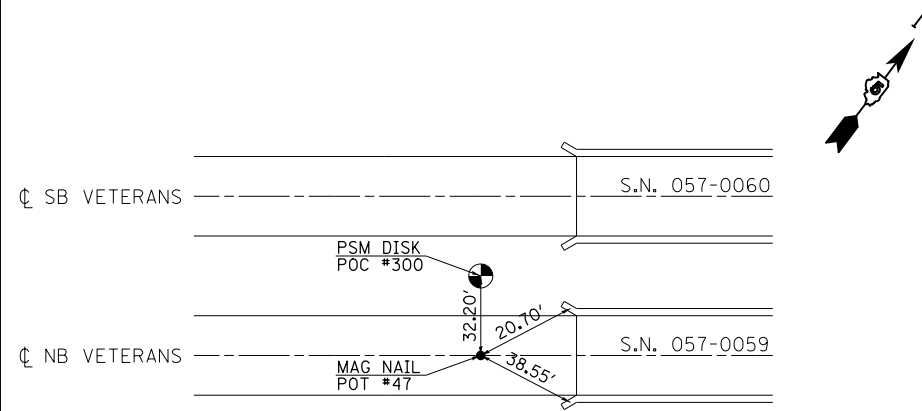
P.C. #302 STATION 38 + 00.02 PSM DISK



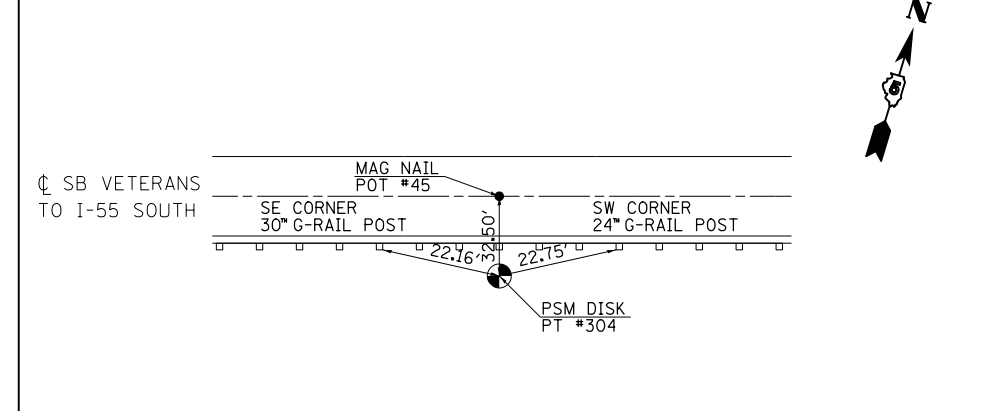
P.O.C. #300 STATION 58 + 98.61 PSM DISK



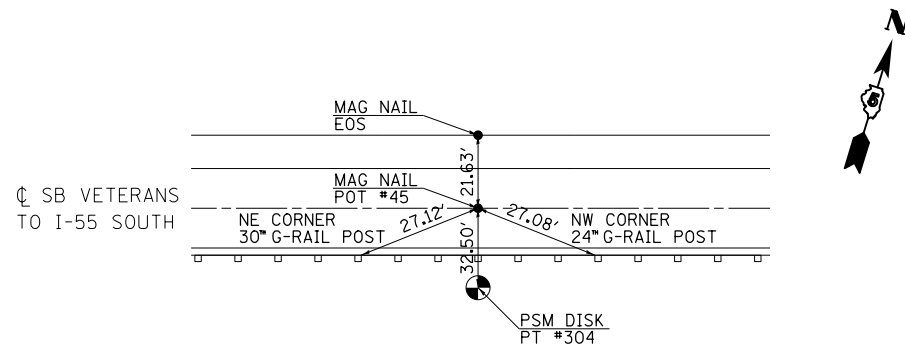
P.O.T. #47 STATION 59 + 00.00 MAGNAIL



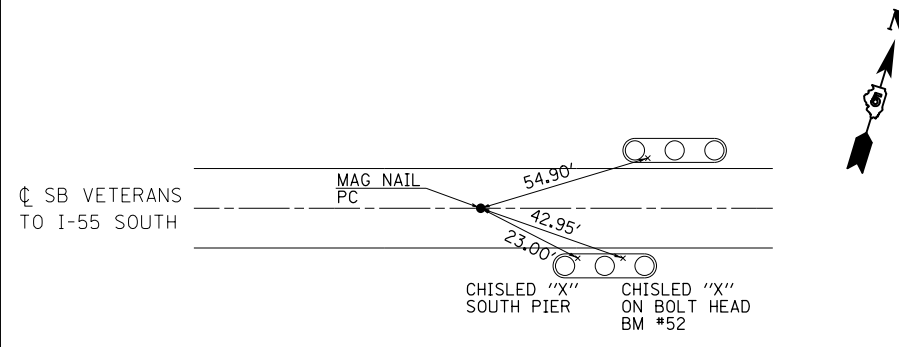
P.T. #304 STATION 66 + 98.73 BRASS DISK



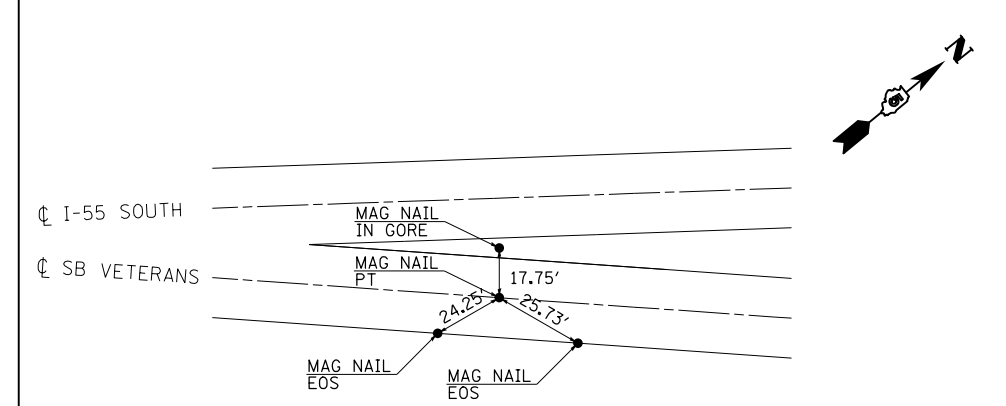
P.O.T. #45 STATION 67 + 00.00 MAGNAIL



P.C. STATION 71 + 76.25 MAGNAIL



P.T. STATION 85 + 27.48 MAGNAIL



| | | | |
|---------------------------------------------------------|----------------------|--------------------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - BJH 6/24/2011 | REVISED - |
| ei:\pw\work\p\dot\detersbj\d0157116\0570570-sht-ATB.dgn | | DRAWN - BJH 6/24/2011 | REVISED - |
| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

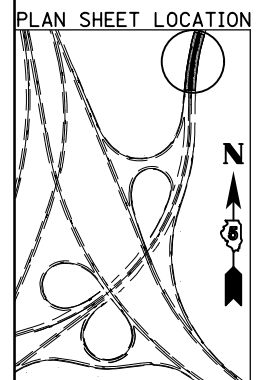
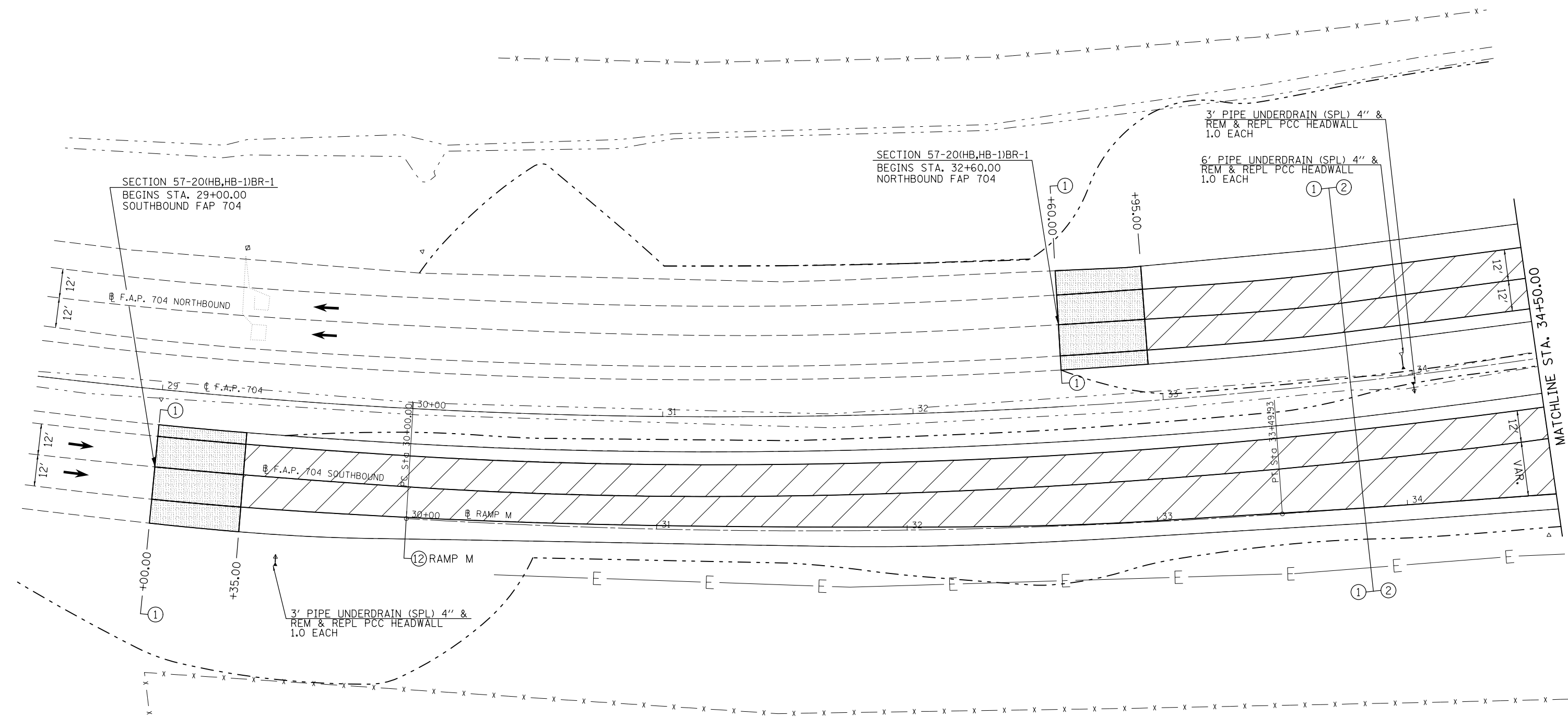
TIE POINTS AND BENCHMARKS

SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 62 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



- H.M.A. SURFACE REM., BUTT JOINT
- H.M.A. SURFACE REM., 1/2"



| | | | |
|-----------------------------------------------------------|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
| ei:\pw\work\p\idot\detersbj\d0157116\0570570-sht-plan.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 48.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 9/9/2013 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

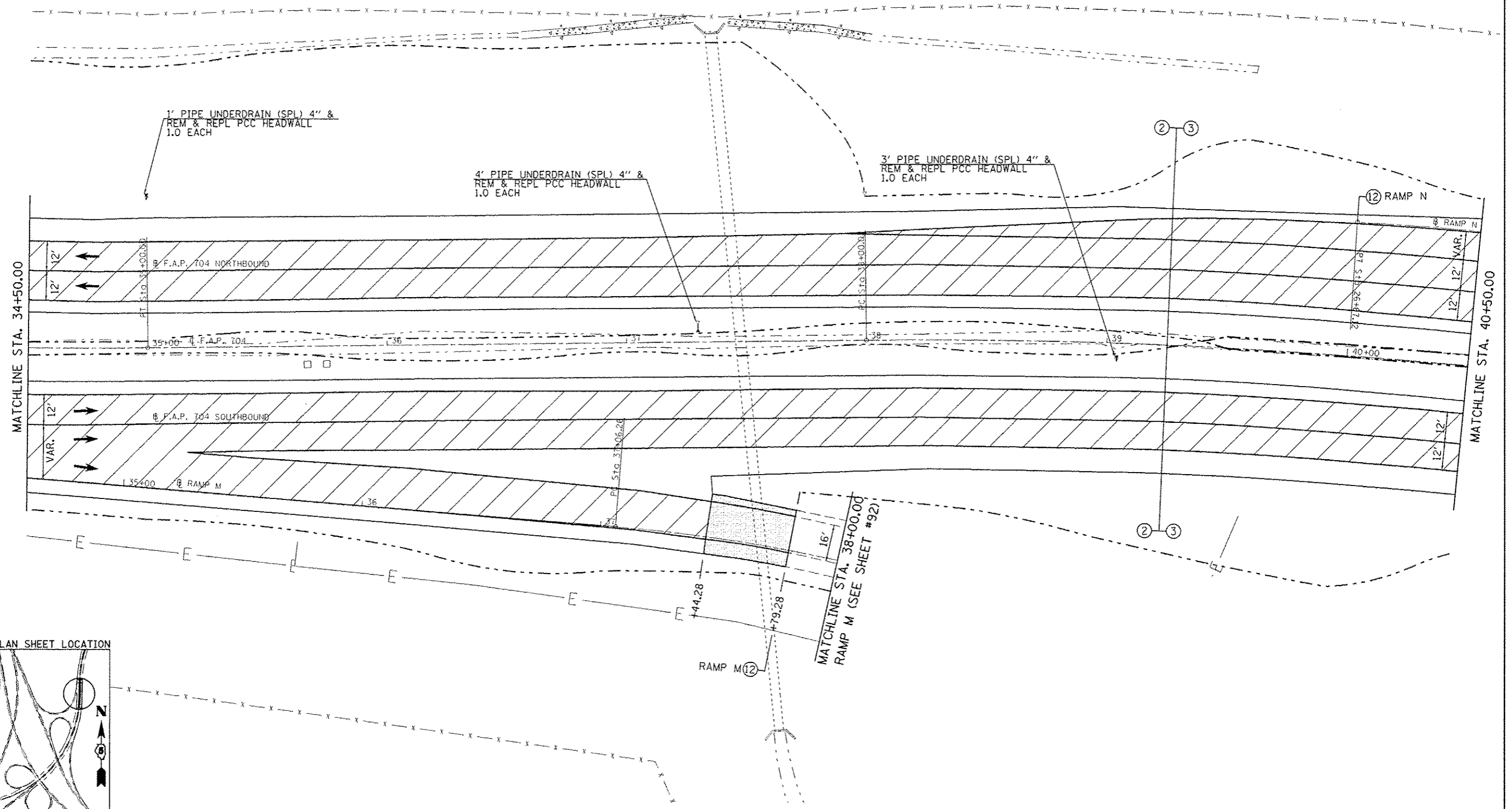
**PLAN SHEET
F.A.P. 704 (VETERAN'S PARKWAY)**

SCALE: SHEET 1 OF 7 SHEETS STA. 28+50.00 TO STA. 34+50.00

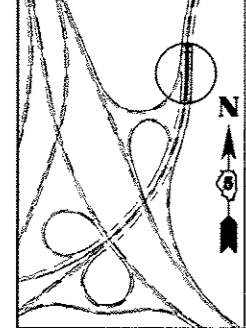
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 63 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



H.M.A. SURFACE REM., BUTT JOINT
 H.M.A. SURFACE REM., 1/2"



PLAN SHEET LOCATION



| | | | |
|-------------------------------------------|--------------------|-------------|------------|
| FILE NAME: \\pc\work\p\proj\1015\116\0570 | USER NAME: dsterby | DESIGNED: - | REVISED: - |
| 779-ant-plan.dgn | | DRAWN: - | REVISED: - |
| PLT SCALE: 40,0000 1/4 in. | | CHECKED: - | REVISED: - |
| PLT DATE: 9/9/2013 | | DATE: - | REVISED: - |

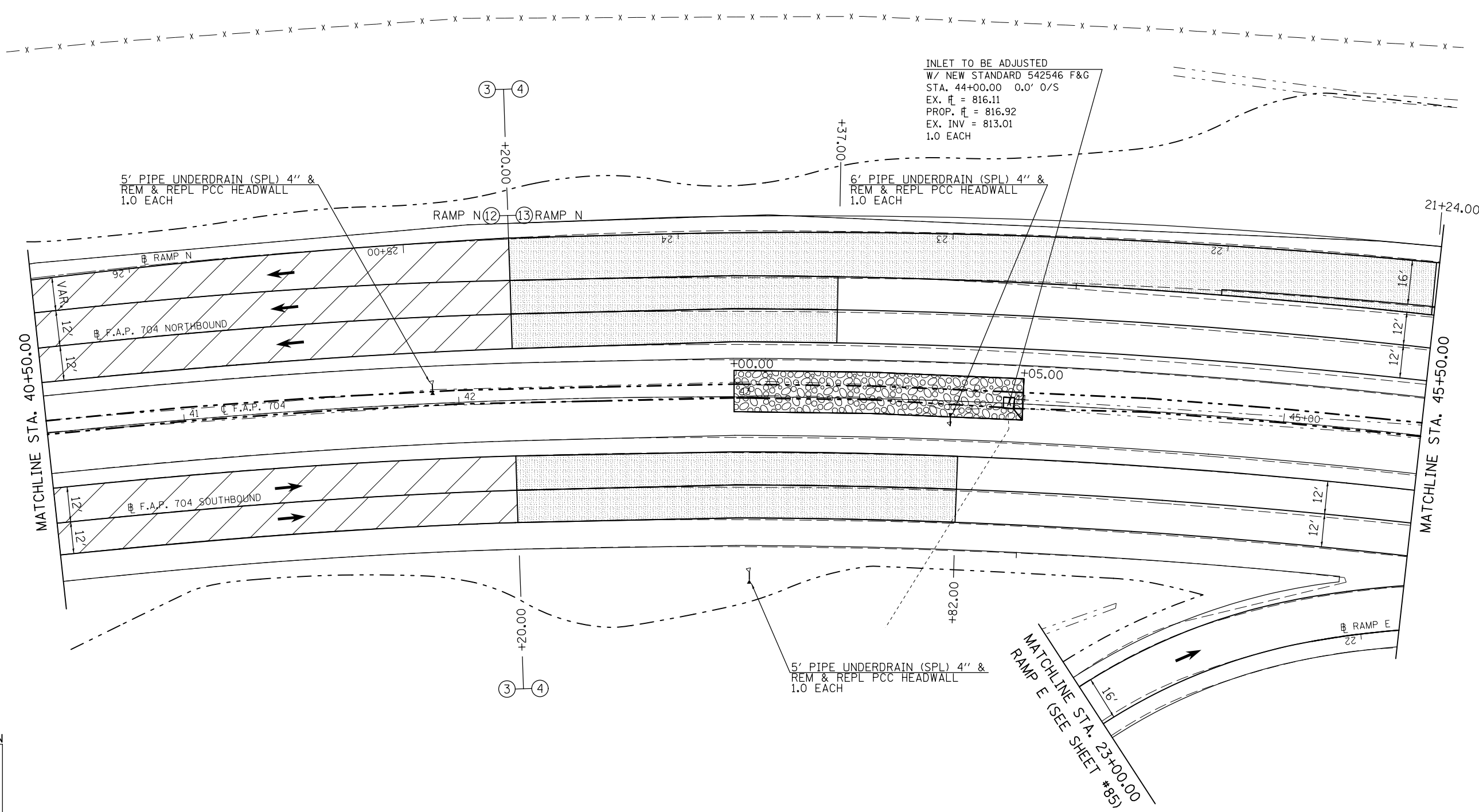
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
F.A.P. 704 (VETERAN'S PARKWAY)
 SCALE: SHEET 2 OF 7 SHEETS STA. 34+50.00 TO STA. 40+50.00

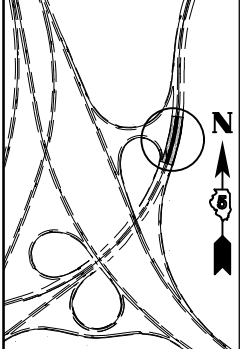
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|--------------------|--------|---------------------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 64 |
| CONTRACT NO. T0570 | | | ILLINOIS FED. AID PROJECT | |



- H.M.A. SURFACE REM., BUTT JOINT
- H.M.A. SURFACE REM., 1/2"
- STONE RIPRAP, CLASS A4, 16"
W/ 6" BEDDING STONE AND FILTER FABRIC



PLAN SHEET LOCATION




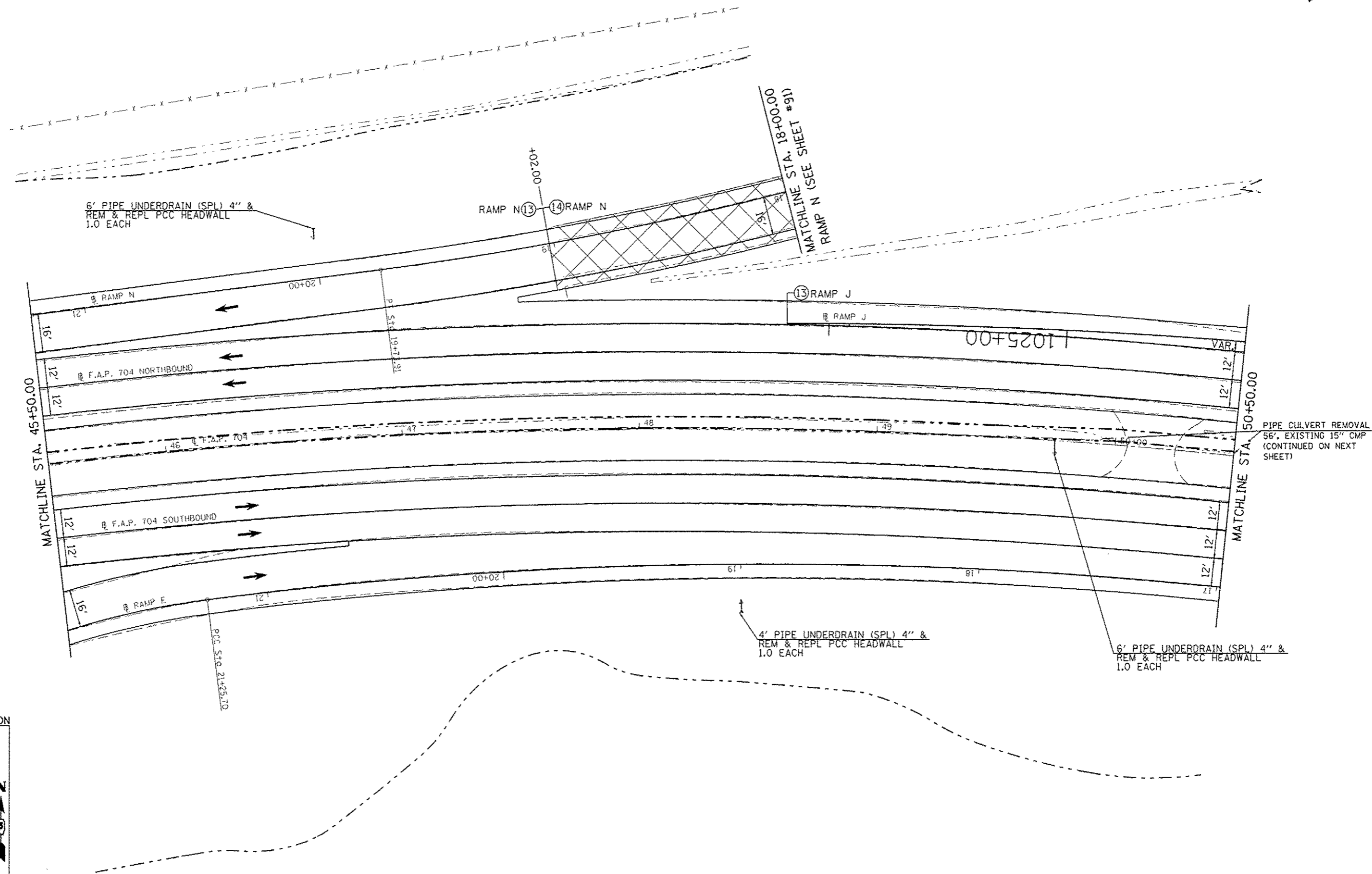
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| ei:\pw_work\p\idot\detersbj\d0157116\0570570-sht-plan.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

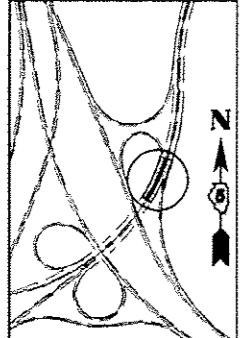
| | | | |
|---------------------------------------|---------|-------------|--------------------------------|
| PLAN SHEET | | | |
| F.A.P. 704 (VETERAN'S PARKWAY) | | | |
| SCALE: | SHEET 3 | OF 7 SHEETS | STA. 40+50.00 TO STA. 45+50.00 |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 65 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |






 PAVEMENT BREAKING



PLAN SHEET LOCATION



| | | | | | | | | | | | | |
|---------------------------------------------------|-----------------------------|-------------|------------|-----------------------------------------------------------|---------------------------------------|---------|--------------------|----------------------------|---------------------------|------------------|--------------|--|
| FILE NAME: c:\pipe\work\pedit\desnby\0157116\0570 | USER NAME: detersby | DESIGNED: - | REVISED: - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN SHEET | | F.A.P. RTE. 704 | SECTION 57-20(HB,HB-DIBR-1 | COUNTY MCLEAN | TOTAL SHEETS 440 | SHEET NO. 66 | |
| | 70-shl-plandgn | DRAWN: - | REVISED: - | | F.A.P. 704 (VETERAN'S PARKWAY) | | CONTRACT NO. 70570 | | ILLINOIS FED. AID PROJECT | | | |
| | PLOT SCALE: 40.0000 1/2 in. | CHECKED: - | REVISED: - | | SCALE: | SHEET 4 | OF 7 SHEETS | STA. 45+50.00 | TO STA. 50+50.00 | | | |
| | PLOT DATE: 3/9/2013 | DATE: - | REVISED: - | | | | | | | | | |

-  STONE RIPRAP, CLASS A4, 16"
-  W/ 6" BEDDING STONE AND FILTER FABRIC
-  CLASS D PATCHES, 10" AND SUBBASE GRAN. MATERIAL, TYPE B 4" SHALL BE USED AS NEEDED FOR PIPE CULVERT REM AND REPL
-  PAVED SHOULDER REMOVAL
-  PAVEMENT REMOVAL

REMOVE CONC HEADWALL, 1.0 EACH
REPLACE W/ END SECTIONS 24",
1.0 EACH

PIPE CULVERT REMOVAL 55',
EXISTING 24" BCCMP
REPLACE W/ 50' PIPE CULV,
CLASS C, TYPE 1, 24"

FLUSH INLET BOX FOR
MEDIAN STAND, 542546
STA. 50+82.17 5.2' LT.
PROP. H = 812.43
PROP. INV = 805.98
1.0 EACH

CONC COLLAR,
0.5 CU YD

CONC COLLAR,
0.5 CU YD

PIPE CULVERT REMOVAL
10', EXISTING 24" RCCP
REPLACE W/ 5' PIPE
CULV, CLASS A, TYPE 2,
24"

INLET TO BE FILLED
STA. 50+84 0.0' O/S
EX. H = 810.30
EX. INV = 805.98
1.0 EACH

PIPE CULVERT REMOVAL
29', EXISTING 15" CMP
(CONTINUED FROM PREV
SHEET)

FLEXIBLE CONNECTOR
STD 420401

BRIDGE APPROACH SLAB

EXISTING SIGN STRUCTURE 5 C 057 I074 L133.90
REMOVE OVERHEAD SIGN STRUCTURE - CANT. (1.0 EACH)
REMOVE SIGN PANEL - TYPE 3 (152.0 SQ FT)
REMOVE CONC. FOUNDATION - OVERHEAD (1.0 EACH)

14" PIPE UNDERDRAIN (SPL) 4" &
REM & REPL PCC HEADWALL
1.0 EACH

PIPE CULVERT REMOVAL
5', EXISTING 24" RCCP
REPLACE W/ 10' PIPE
CULV, CLASS A, TYPE 2,
24"

CAST IN PLACE CONCRETE THRUST
BLOCK 4'X4'X2', 1.0 EACH
CONC COLLAR,
0.5 CU YD

FLEXIBLE CONNECTOR
STD 420401

BRIDGE APPROACH SLAB

EXISTING S.N. 057-0061

3 SPAN STEEL BEAM BRIDGE @ STA. 635+75.15,
SKEWED 31°-11'-24" LEFT FORWARD, 156'-7" BK. TO BK.
REMOVAL OF EXISTING STRUCTURE NO. 3 1.0 EACH

PROPOSED S.N. 057-0253

3 SPAN W33 STEEL BEAMS
@ STA. 635+73.19, SKEWED 30°-0'-0" LEFT FORWARD
ON INTEGRAL ABUTMENTS
204'-6" BK. TO BK., 47'-9" HOR. CLEAR.

PROPOSED SIGN STRUCTURE 5 B 057 I074 L133.90
BRIDGE MOUNTED SIGN STRUCTURE (15.0 FT)
SIGN D (150.0 SQ FT)

BRIDGE APPROACH SLAB

FLEXIBLE CONNECTOR
STD 420401

FLEXIBLE CONNECTOR
STD 420401

BRIDGE APPROACH SLAB

CONC. CURB
TYPE B, 20"

TYPE F INLET BOX,
STANDARD 610001 (SPECIAL)
STA. 53+60.00 63.4' RT.
PROP. H = 810.93
PROP. INV = 808.29
1.0 EACH

CONC. SHOULDER CURB, 10'

CAST IN PLACE CONCRETE THRUST
BLOCK 2'X2'X2', 1.0 EACH

PLUG EXISTING INLETS
STA. 53+02 62.0' RT.
EX. H = 809.85
EX. INV = 807.86
1.0 EACH
PRIOR TO STAGE I

INLET TO BE REMOVED
STA. 53+02 62.0' RT.
EX. H = 809.85
EX. INV = 807.86
1.0 EACH

PIPE CULVERT REMOVAL 68',
EXISTING 15" BCCMP

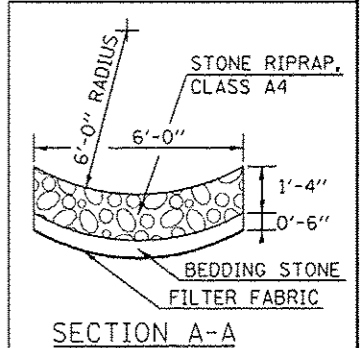
REMOVE CONC HEADWALL, 1.0 EACH

EXISTING S.N. 057-0062

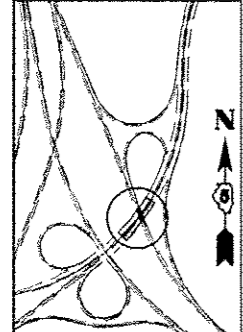
3 SPAN STEEL BEAM BRIDGE @ STA. 635+75.15,
SKEWED 31°-11'-24" LEFT FORWARD, 156'-7" BK. TO BK.
REMOVAL OF EXISTING STRUCTURE NO. 4 1.0 EACH

PROPOSED S.N. 057-0254

3 SPAN W30 STEEL BEAMS
@ STA. 635+73.19, SKEWED 30°-0'-0" LEFT FORWARD
ON INTEGRAL ABUTMENTS
204'-6" BK. TO BK., 52'-5" HOR. CLEAR.

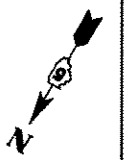





PLAN SHEET LOCATION

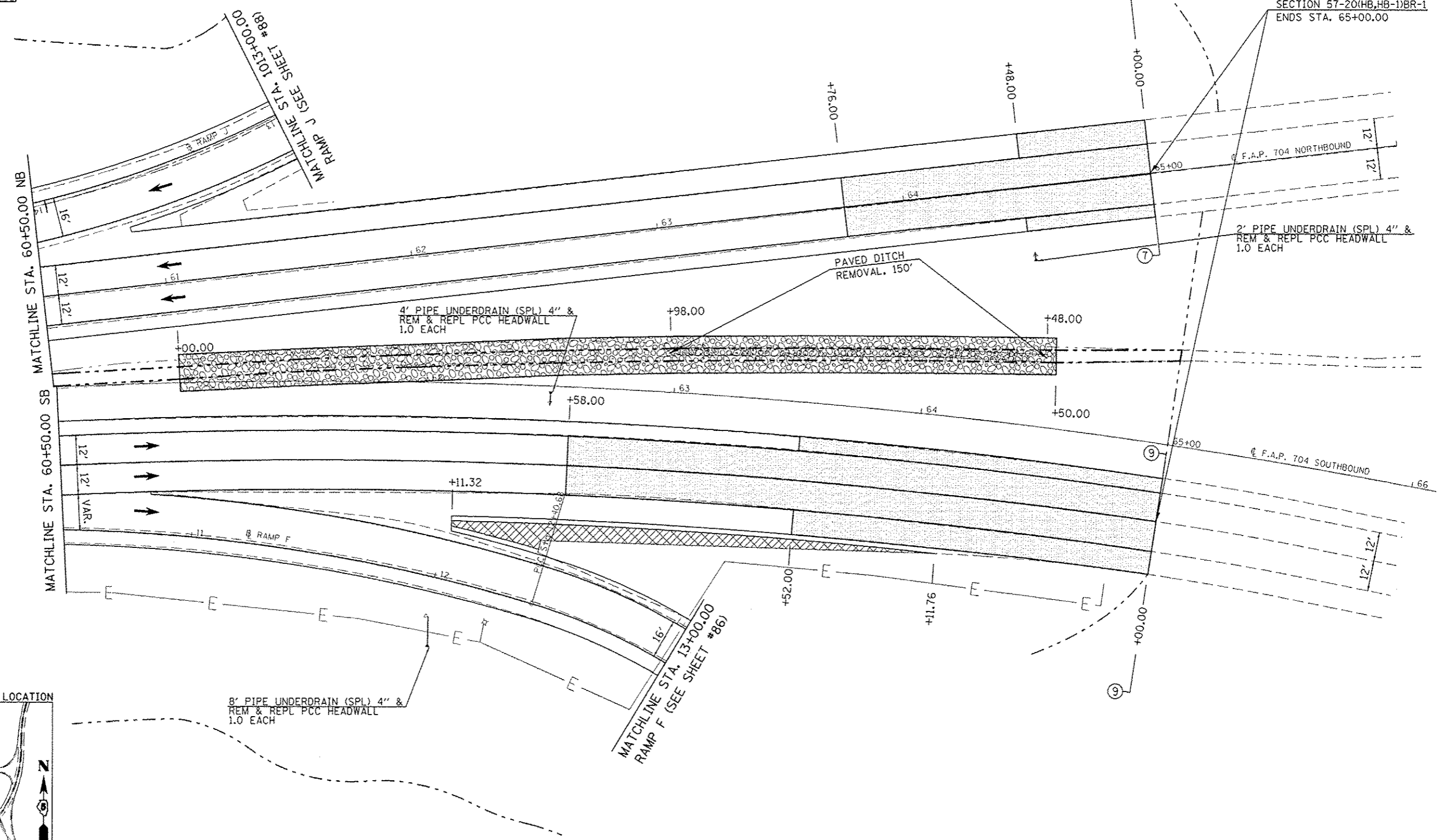


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|--------------------------------------------------------------|-----------------------------|------------|-----------|-----------------------------------------------------------|------------------------------------------------------|-----------------|----------------------------|--------------------------------|--------------------|--------------|---------------------------|
| FILE NAME = | USER NAME = dnterabj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN SHEET F.A.P. 704 (VETERAN'S PARKWAY) | F.A.P. RTE. 704 | SECTION 57-20(HB,HB-1)BR-1 | COUNTY MCLEAN | TOTAL SHEETS 440 | SHEET NO. 67 | |
| PROJECT WORK \pipedot\dnterabj\ad0197116\057070-shr-plan.dgn | PLLOT SCALE = 40,0000 1/16" | CHECKED - | REVISED - | | | SCALE: 1" = 40' | SHEET 5 OF 7 SHEETS | STA. 50+50.00 TO STA. 55+50.00 | CONTRACT NO. 70570 | | ILLINOIS FED. AID PROJECT |
| PLLOT DATE = 9/9/2013 | DATE - | DATE - | DATE - | | | | | | | | |
| | | | | | | | | | | | |

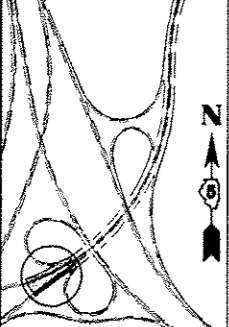
SEC. 19, T 23 N, R 2 E, 3RD PM



-  H.M.A. SURFACE REM., BUTT JOINT
-  STONE RIPRAP, CLASS A4, 16"
W/ 6" BEDDING STONE AND FILTER FABRIC
-  PAVED SHOULDER REMOVAL



PLAN SHEET LOCATION



| | | | |
|---------------------------------------|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = jsternby | DESIGNED - | REVISED - |
| c:\p\work\p\dot\sternby\00157116\0570 | 70-shr-plan.dgn | DRAWN - | REVISED - |
| | PLOT SCALE = 48.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 7/3/2013 | DATE - | REVISED - |

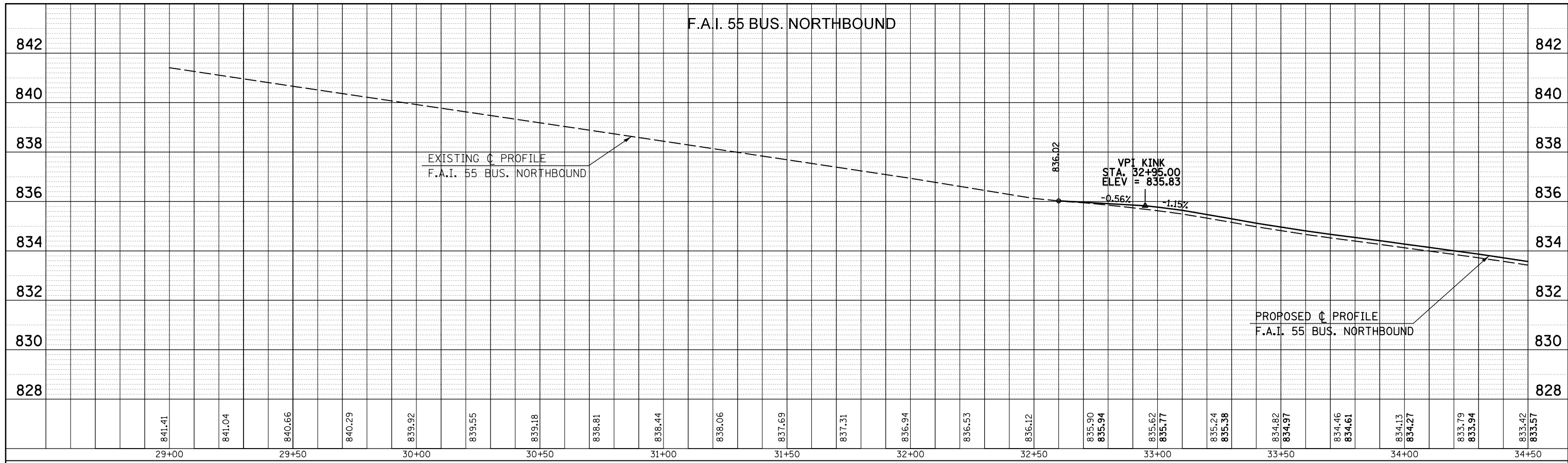
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
F.A.P. 704 (VETERAN'S PARKWAY)

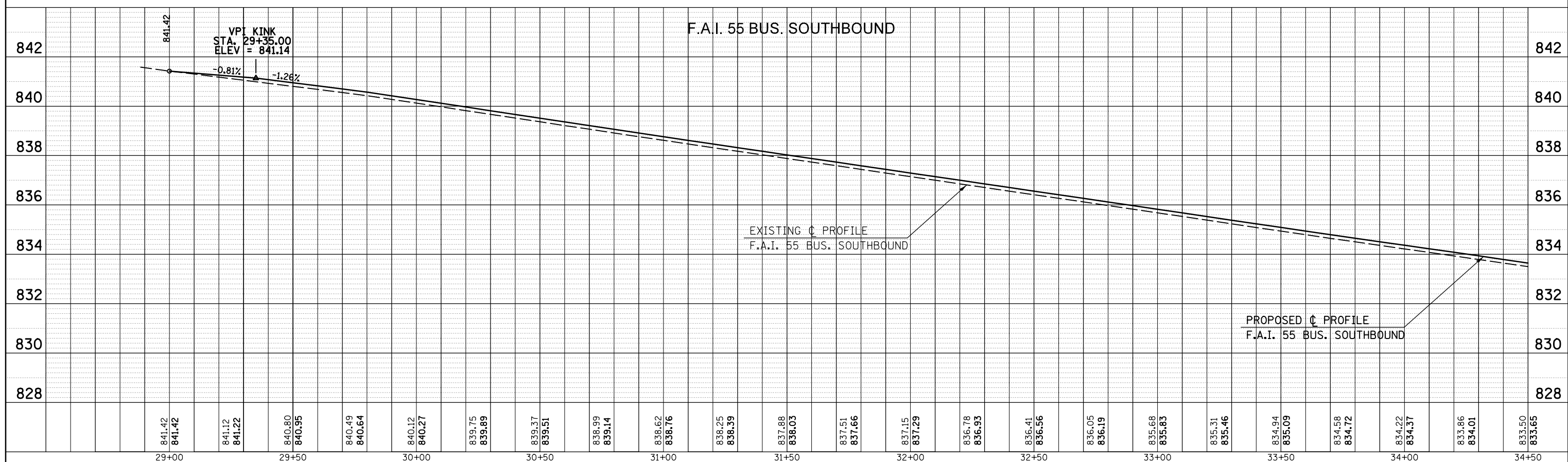
SCALE: SHEET 7 OF 7 SHEETS | STA. 60+50.00 TO STA. 66+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-UBR-1 | MCLEAN | 440 | 69 |
| CONTRACT NO. T0570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| | | |
|------|-------------------|------|
| PLAN | SURVEYED | DATE |
| | PLOTTED | BY |
| | ALIGNMENT CHECKED | |
| | NOTE BOOK NO. | |
| | CARD FILE NAME | |

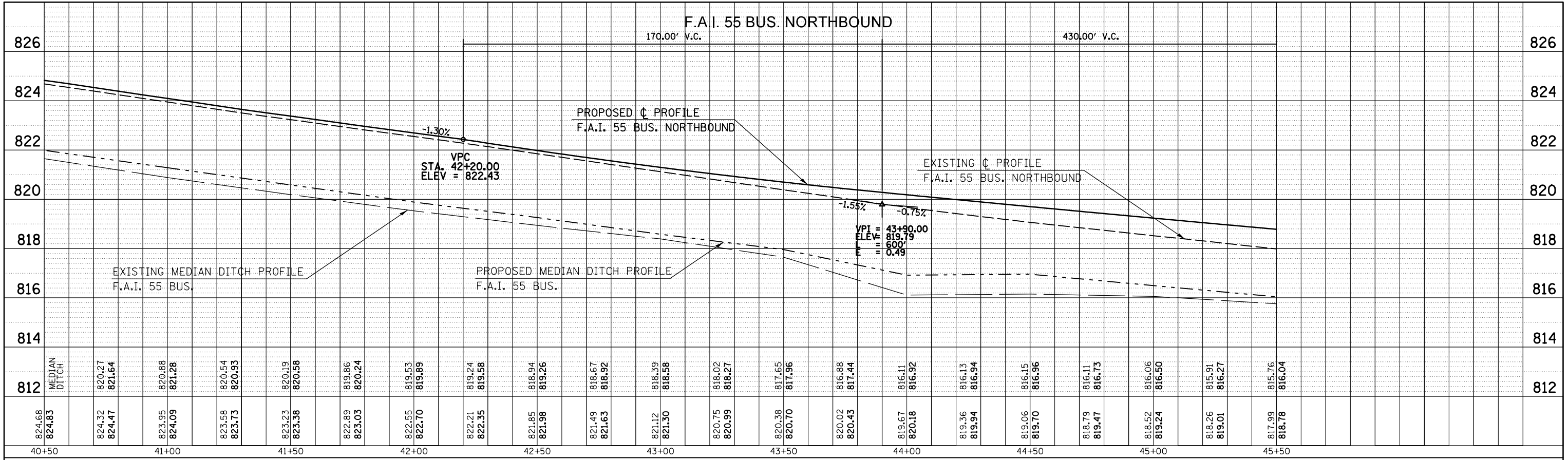


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|---------|-----------------------------|------|
| PROFILE | SURVEYED | DATE |
| | PLOTTED | BY |
| | GRADES CHECKED | |
| | STRUCTURE NOTATIONS CHECKED | |
| | NOTE BOOK NO. | |
| | NOTATIS CHECKED | |

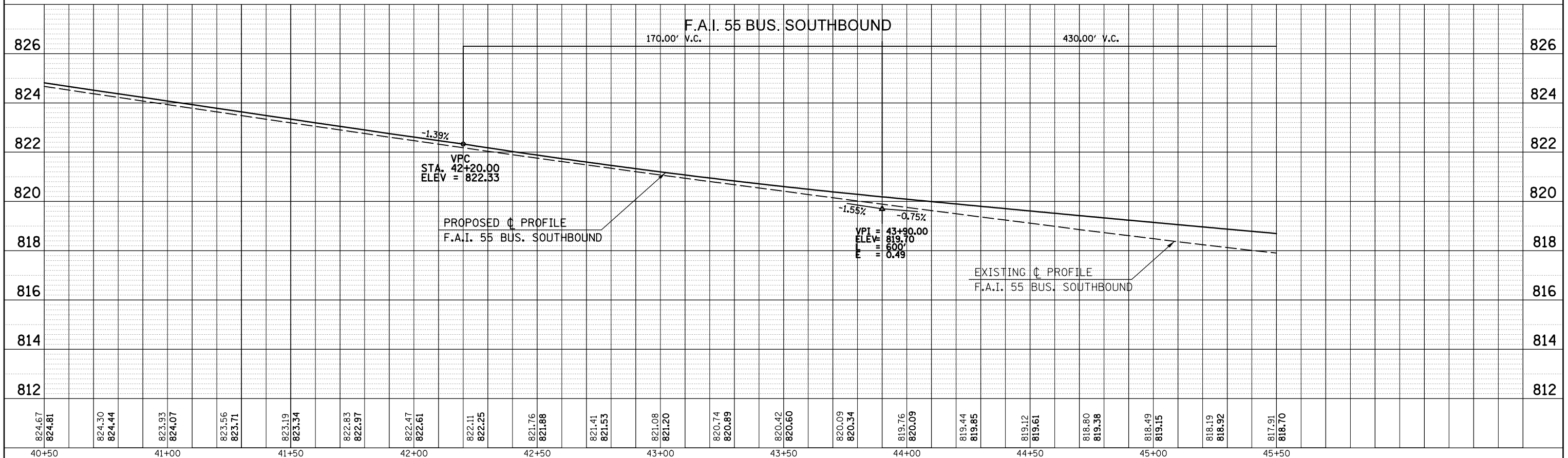


| | | | | | | | | | |
|-----------------------------------------------------------|----------------------|-------------------------|---------------------------|-----------------------------------------------------------------|---------------------------------|--------------------|--------|--------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | F.A.P. 704 PROFILE SHEET | F.A.P. SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwork\pwork\detersbj\d0157116\0570570-shr-profiles.dgn | DRAWN - | REVISED - | 704 | | | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 70 |
| PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | CONTRACT NO. 70570 | | | | | | |
| PLOT DATE = 8/13/2013 | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | |
| SCALE: | | SHEET NO. 1 OF 7 SHEETS | | STA. 28+50 TO STA. 34+50 | | | | | |

| | | |
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| PLAN | SURVEYED | DATE |
| | PLOTTED | |
| | ALIGNED | |
| | CHECKED | |
| | FILE NAME | |
| | NO. | |



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|---------|-----------------------------|------|
| PROFILE | SURVEYED | DATE |
| | GRADES CHECKED | |
| | STRUCTURE NOTATIONS CHECKED | |
| | NO. | |



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|----------------------------------------------------------|----------------------|
| FILE NAME = | USER NAME = detersbj |
| c:\pwork\pwork\detersbj\0157116\0570570-shp-profiles.dgn | |
| PLOT SCALE = 40.0000' / in. | |
| PLOT DATE = 8/13/2013 | |

| | |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - | REVISED - |

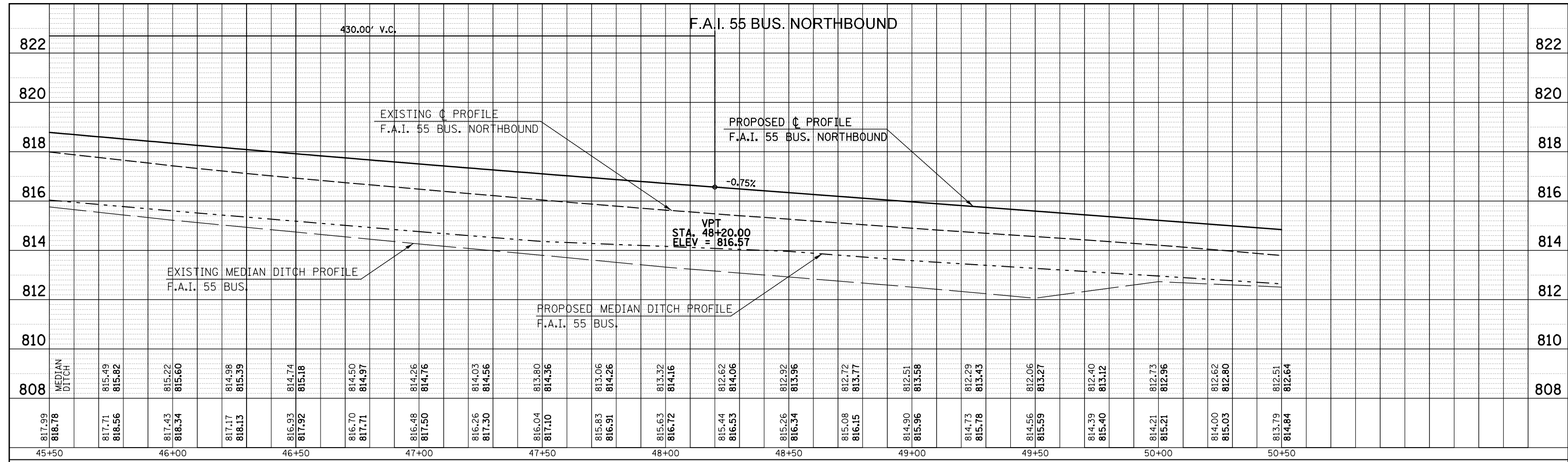
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

F.A.P. 704 PROFILE SHEET

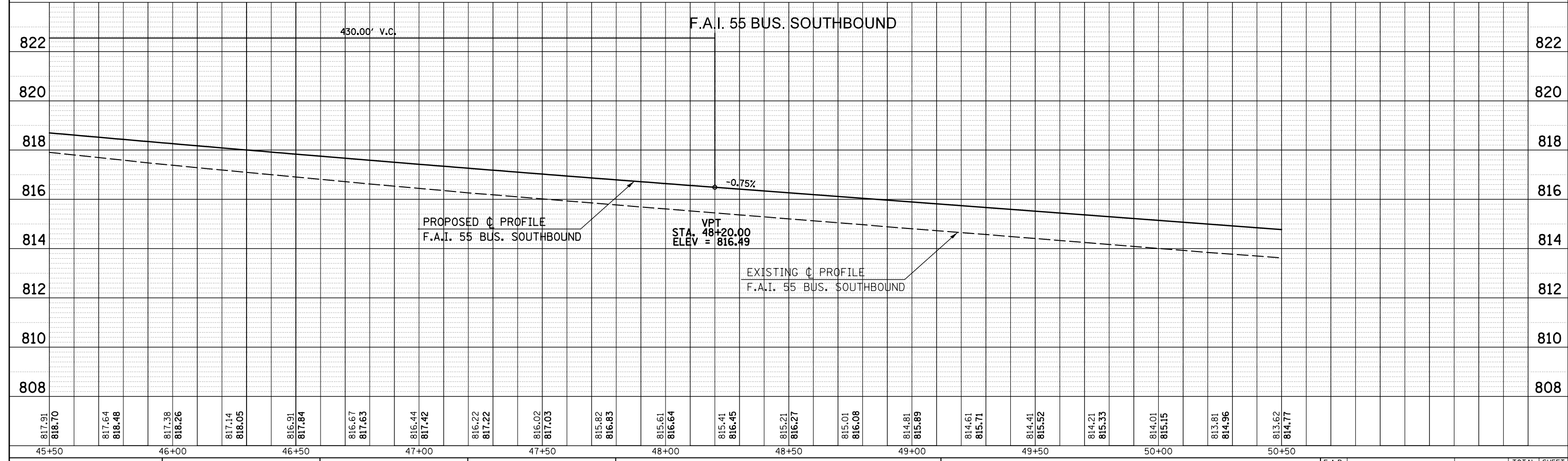
SCALE: SHEET NO. 3 OF 7 SHEETS STA. 40+50 TO STA. 45+50

| | | | | |
|---------------------------|--------------------|--------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 72 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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|---------------|-----------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| NOTE BOOK NO. | CHECKED | | |
| | ALIGNMENT | | |
| | FILE NAME | | |

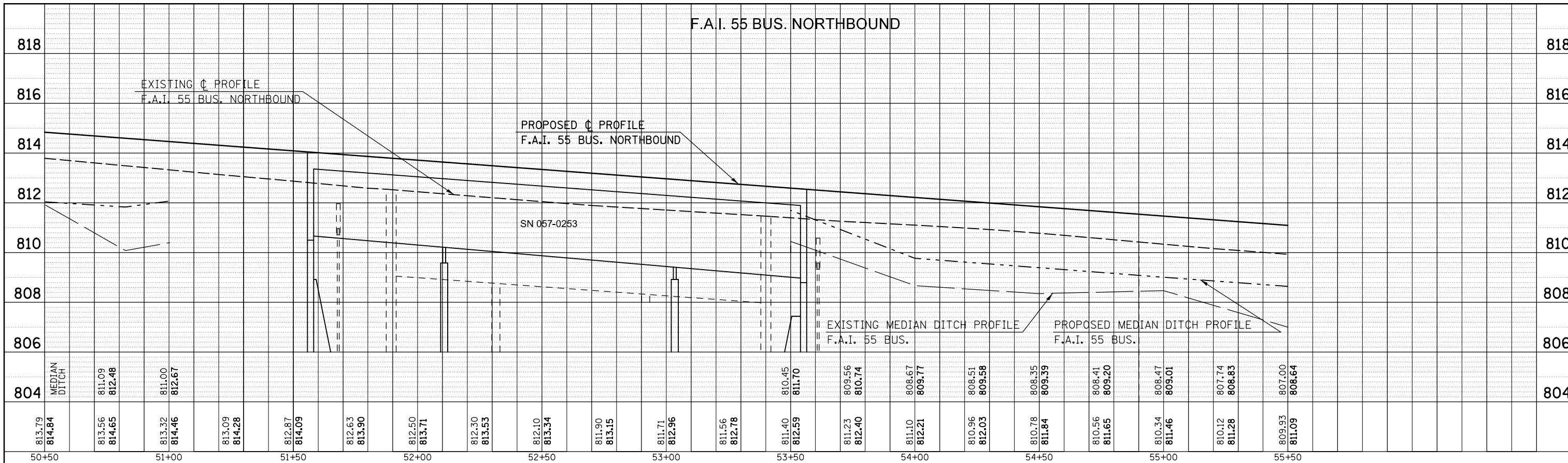


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|---------------|-----------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| NOTE BOOK NO. | CHECKED | | |
| | GRADES | | |
| | STRUCTURE | | |
| | NOTATIONS | | |
| | CHPAD | | |

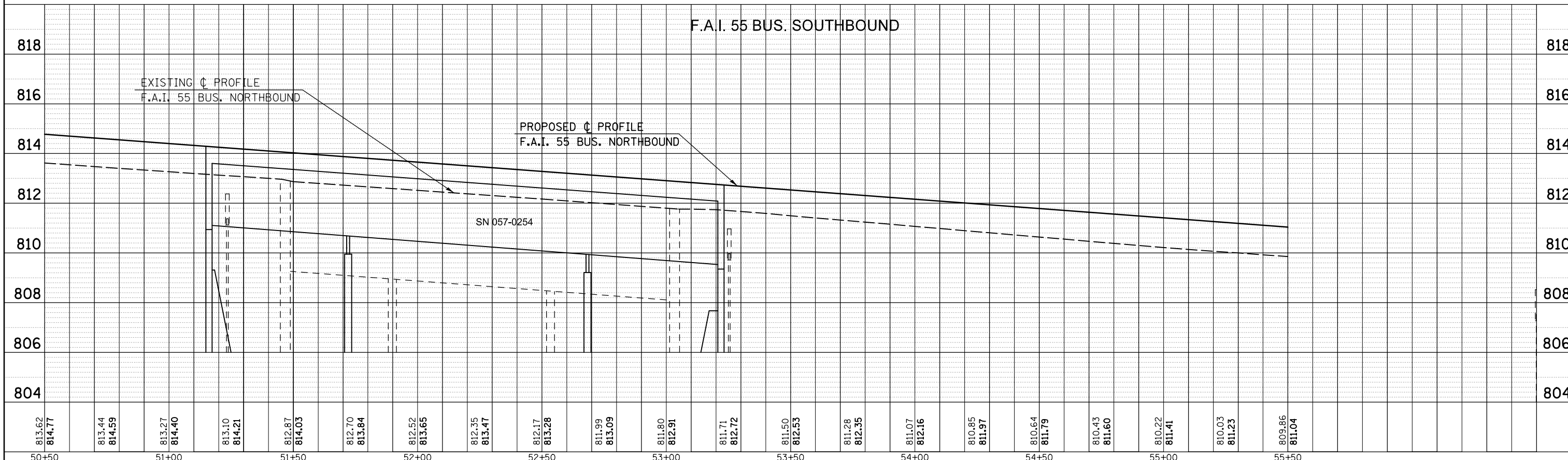


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|--------------------------------------------------------------------------|----------------------|------------|-----------|-------------------------------------------------------------------------|-------------------------|----------------------------------------|--------------------------|----------------------------|---------------|------------------|--------------|--|
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| PLOT SCALE = 40.0000' / in. | | | | | SHEET NO. 4 OF 7 SHEETS | | STA. 45+50 TO STA. 50+50 | CONTRACT NO. 70570 | | | | |
| PLOT DATE = 8/13/2013 | | | | | | | | ILLINOIS FED. AID PROJECT | | | | |
| | | | | | | | | | | | | |

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|---------------|----------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | ALIGNED | | |
| | CHECKED | | |
| | FILED | | |
| NOTE BOOK NO. | CARD FILE NAME | | |



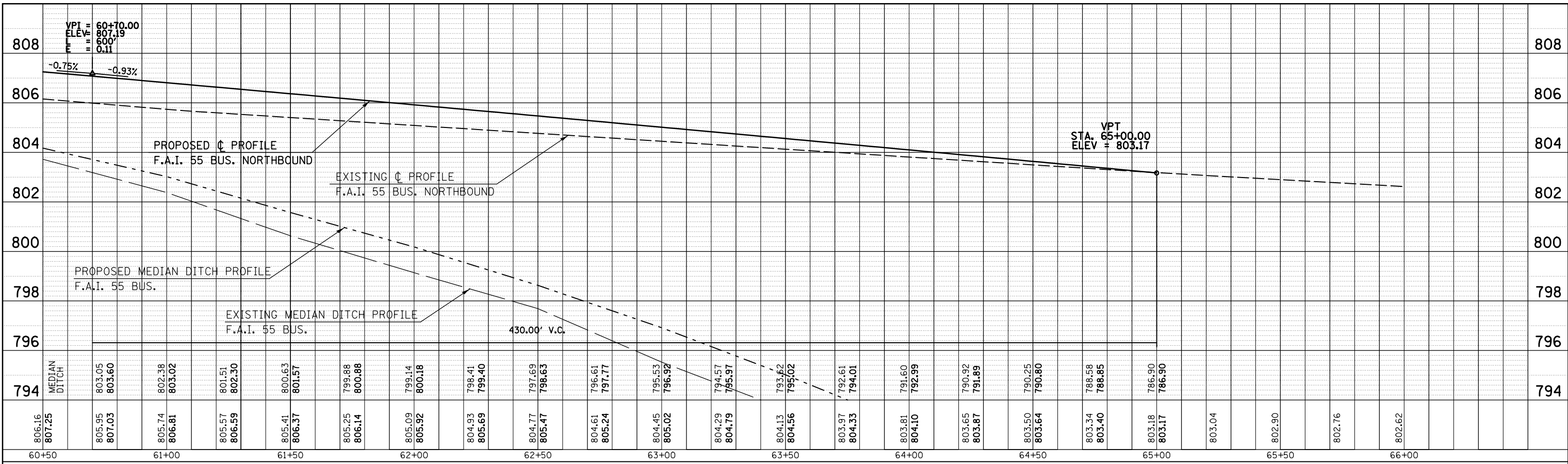
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|---------------|-----------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| NOTE BOOK NO. | NOTATION CHPKD | | |



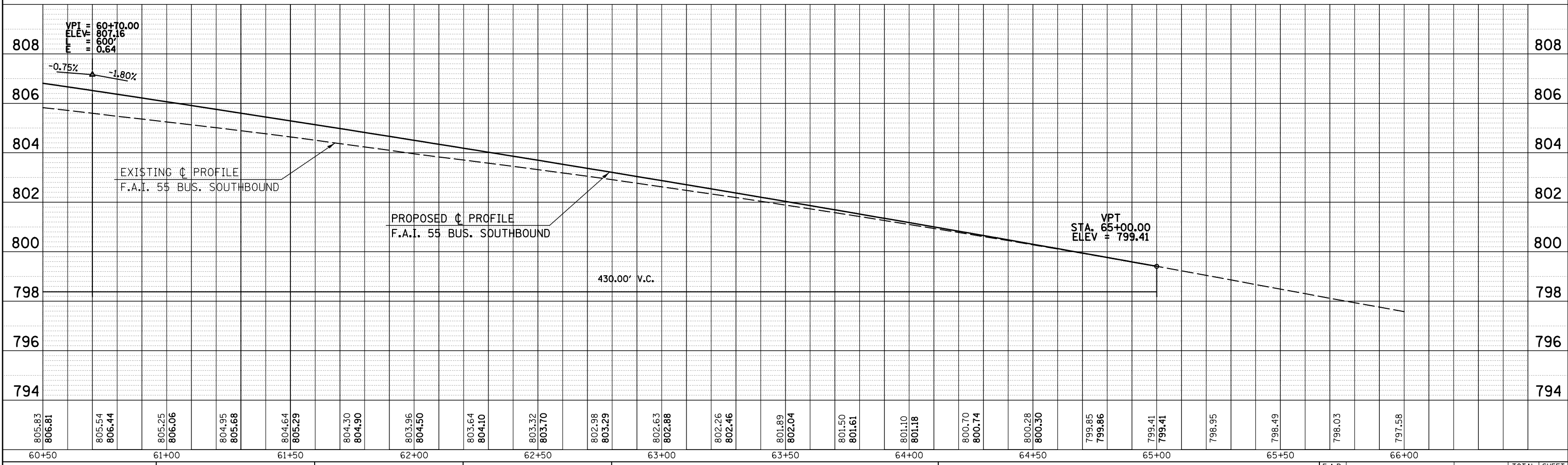
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|------------------------------------------------------------|----------------------|------------|-----------|-----------------------------------------------------------------|---------------------------------|---------------------------|--------------------|--------|--------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | F.A.P. 704 PROFILE SHEET | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwork\pwork\detersbj\05157116\05705704-shp-profiles.dgn | | DRAWN - | REVISED - | | | 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 74 |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - | | | CONTRACT NO. 70570 | | | | |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | |

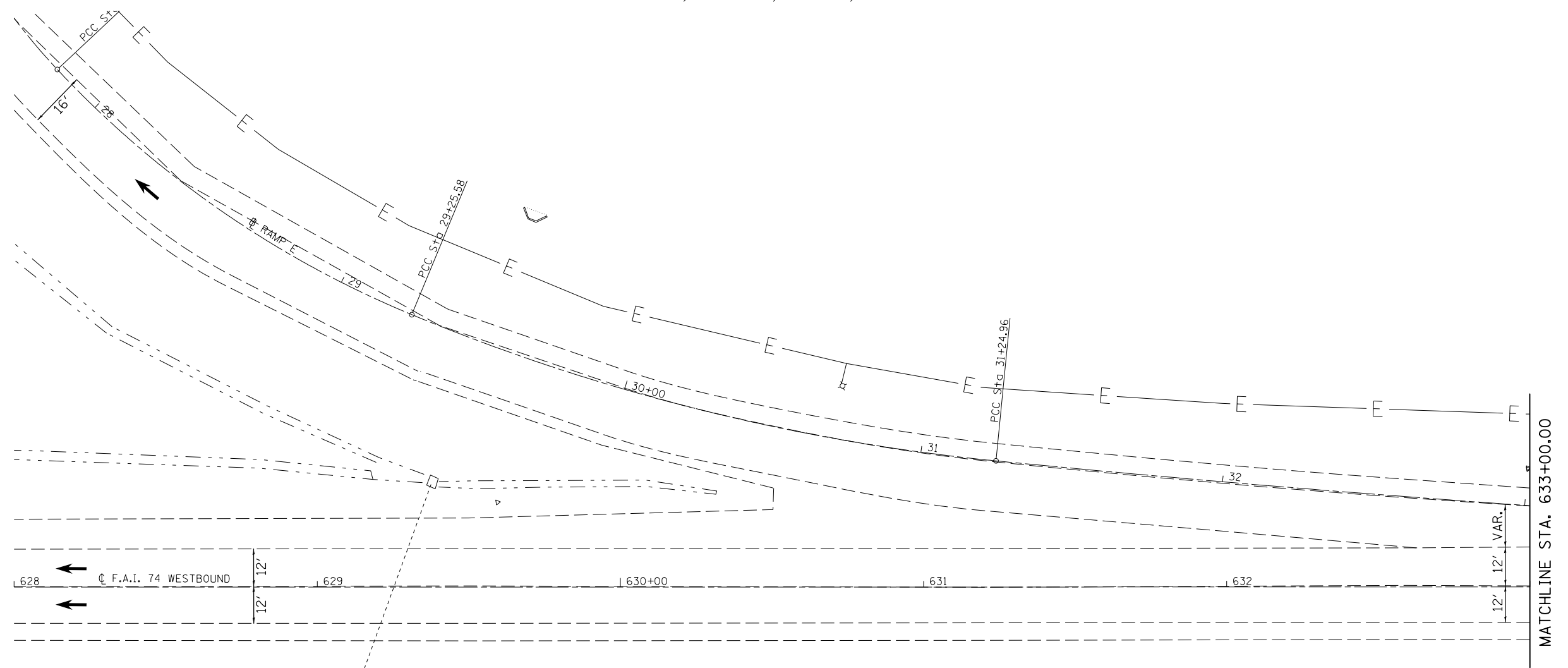
SCALE: SHEET NO. 5 OF 7 SHEETS STA. 50+50 TO STA. 55+50

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|------|-----------------------------|------|
| PLAN | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES CHECKED | |
| | STRUCTURE NOTATIONS CHECKED | |
| | NOTE BOOK NO. | |
| | CARD FILE NAME | |

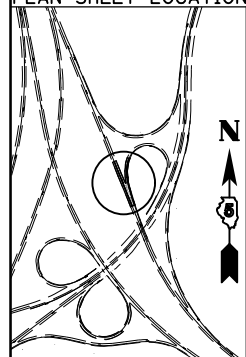


| | | |
|---------|-----------------------------|------|
| PROFILE | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES CHECKED | |
| | STRUCTURE NOTATIONS CHECKED | |
| | NOTE BOOK NO. | |
| | CARD FILE NAME | |





PLAN SHEET LOCATION



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|---------------------------------------------------------|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
| ei:\pw\work\p\idot\detersbj\0157116\0570570-sht-pln.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
FAI-74 WESTBOUND

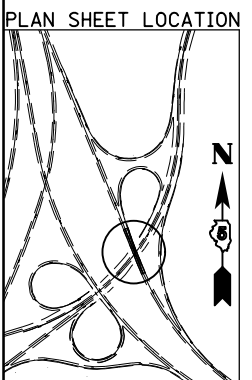
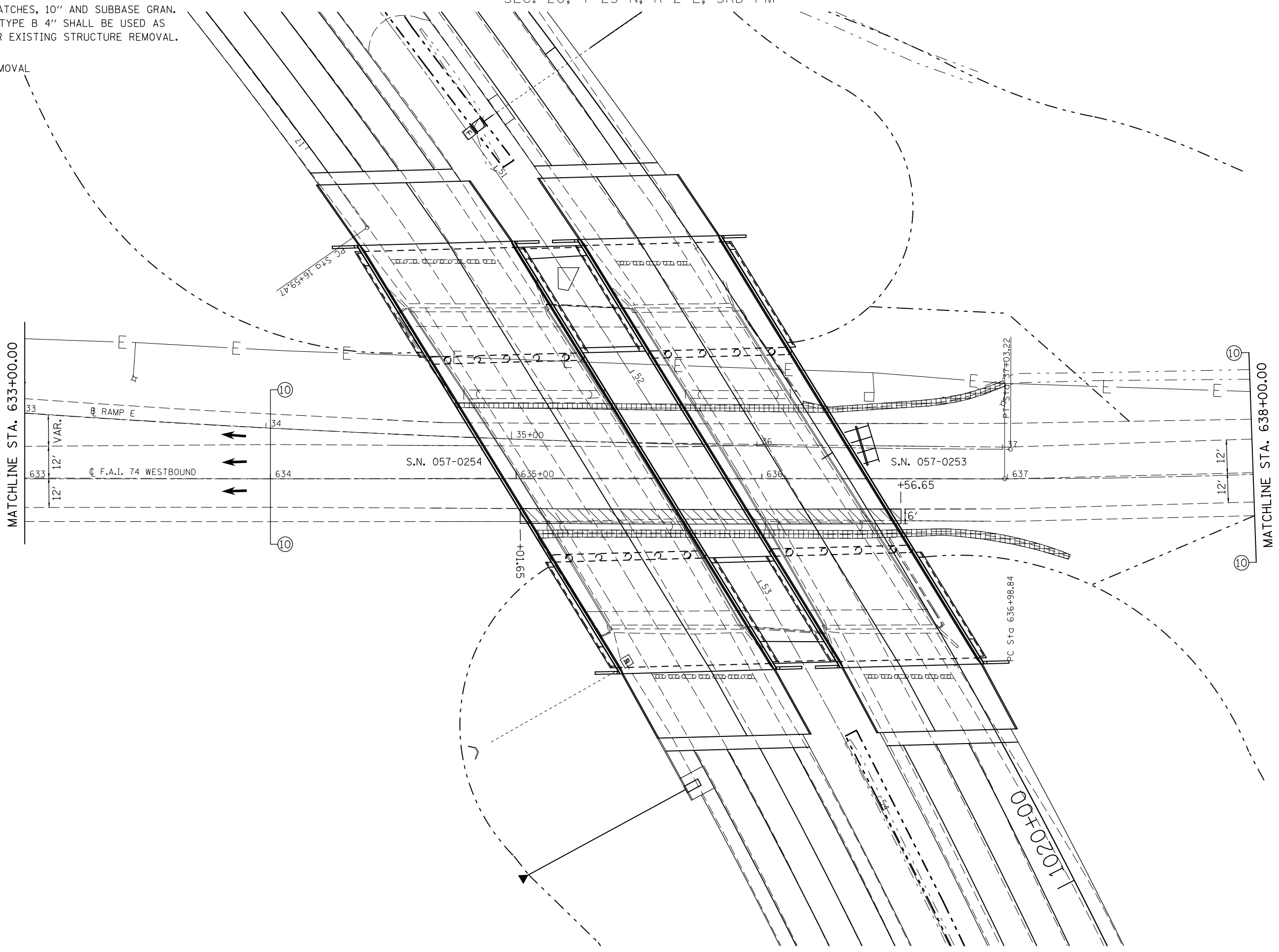
SCALE: SHEET 1 OF 3 SHEETS STA. 628+00.00 TO STA. 633+00.00

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 74 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 76 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

SEC. 20, T 23 N, R 2 E, 3RD PM



- CLASS D PATCHES, 10" AND SUBBASE GRAN. MATERIAL, TYPE B 4" SHALL BE USED AS NEEDED FOR EXISTING STRUCTURE REMOVAL.
- GUTTER REMOVAL



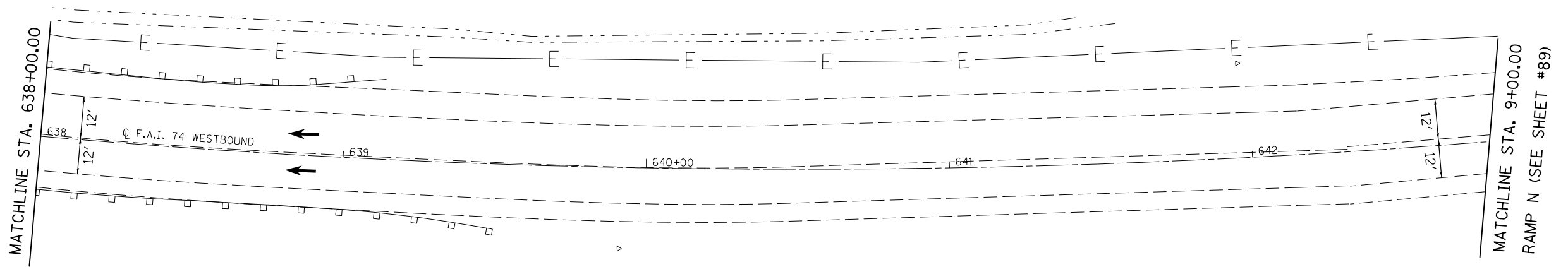
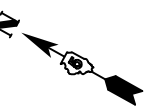
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|--------------------------------------------------------|----------------------|------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
| et:\pw\work\p\id\detersbj\0157116\0570570-sht-plan.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

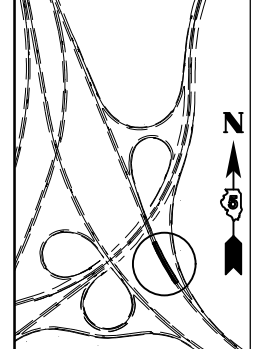
**PLAN SHEET
FAI-74 WESTBOUND**

SCALE: SHEET 2 OF 3 SHEETS STA. 633+00.00 TO STA. 638+00.00

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 74 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 77 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



PLAN SHEET LOCATION



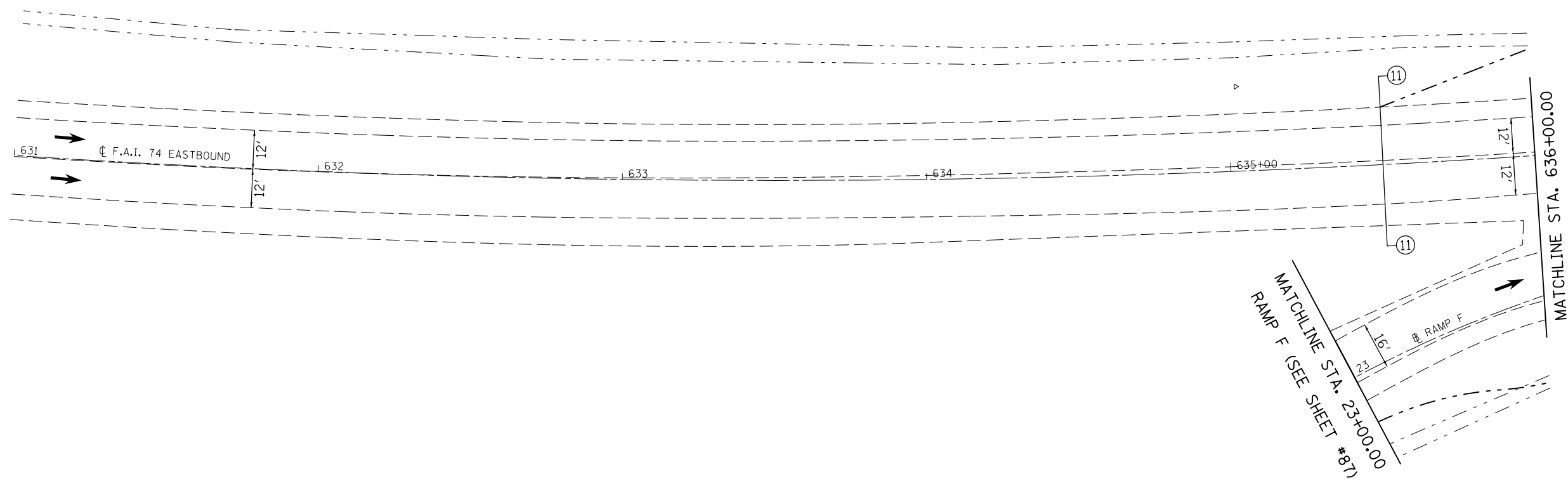
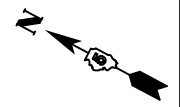
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|----------------------------------------------------------|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

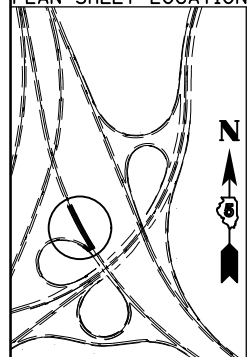
**PLAN SHEET
FAI-74 WESTBOUND**

SCALE: SHEET 3 OF 3 SHEETS STA. 638+00.00 TO STA. 642+78.31

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 74 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 78 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



PLAN SHEET LOCATION



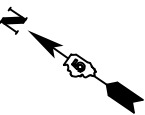
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|------------------------------------------------------------|----------------------|------------|-----------|
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| ei:\pw\work\p\idot\detersbj\d01571116\0570570-sht-plan.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

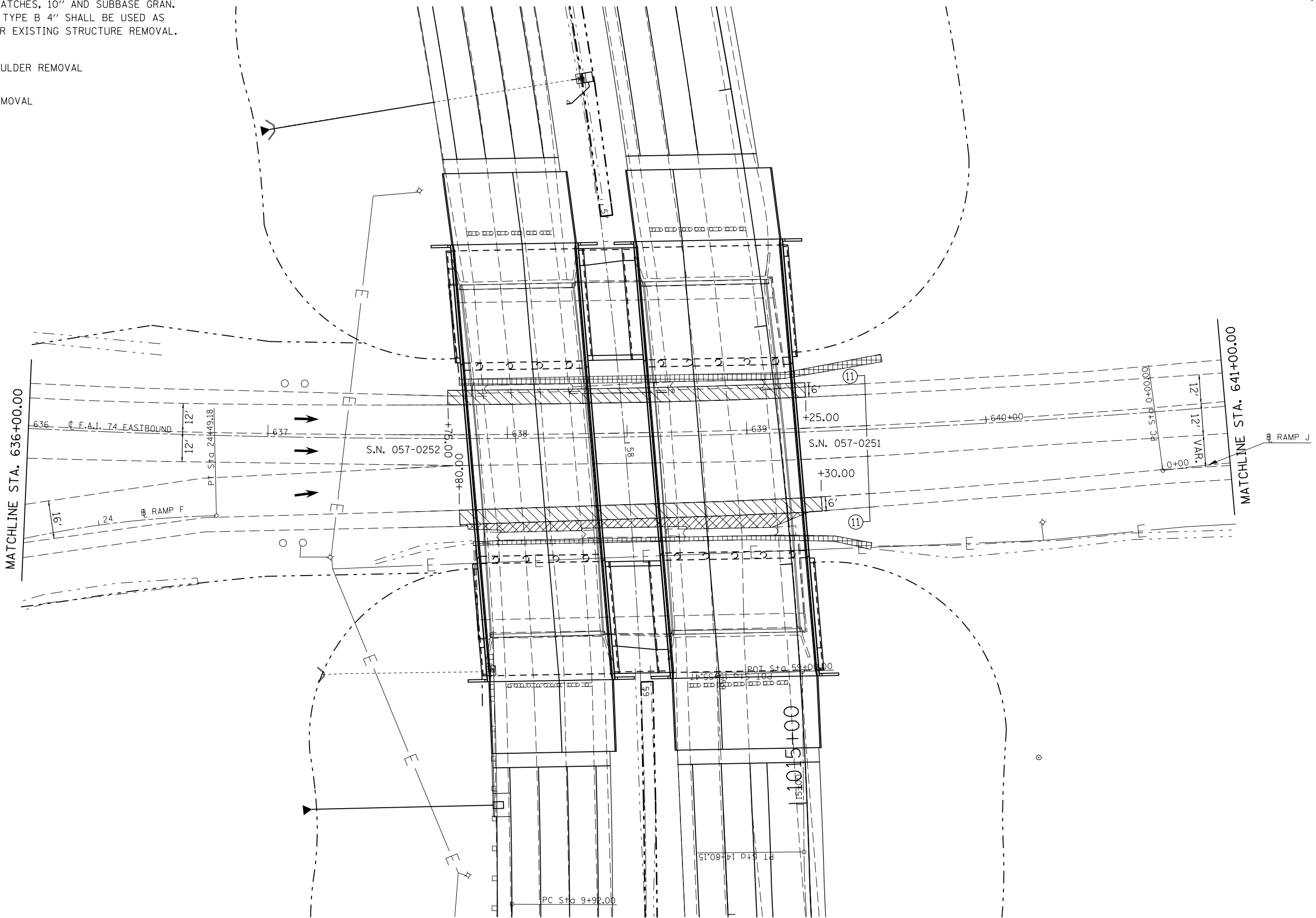
PLAN SHEET
FAI-74 EASTBOUND

SCALE: SHEET 1 OF 3 SHEETS STA. 631+00.00 TO STA. 636+00.00

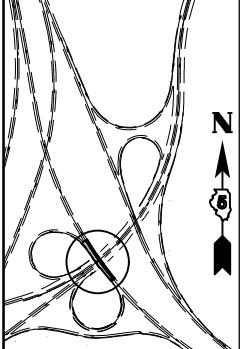
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------------|-----------|
| 74 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 79 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 70570 | |



- CLASS D PATCHES, 10" AND SUBBASE GRAN. MATERIAL, TYPE B 4" SHALL BE USED AS NEEDED FOR EXISTING STRUCTURE REMOVAL.
- PAVED SHOULDER REMOVAL
- GUTTER REMOVAL



PLAN SHEET LOCATION



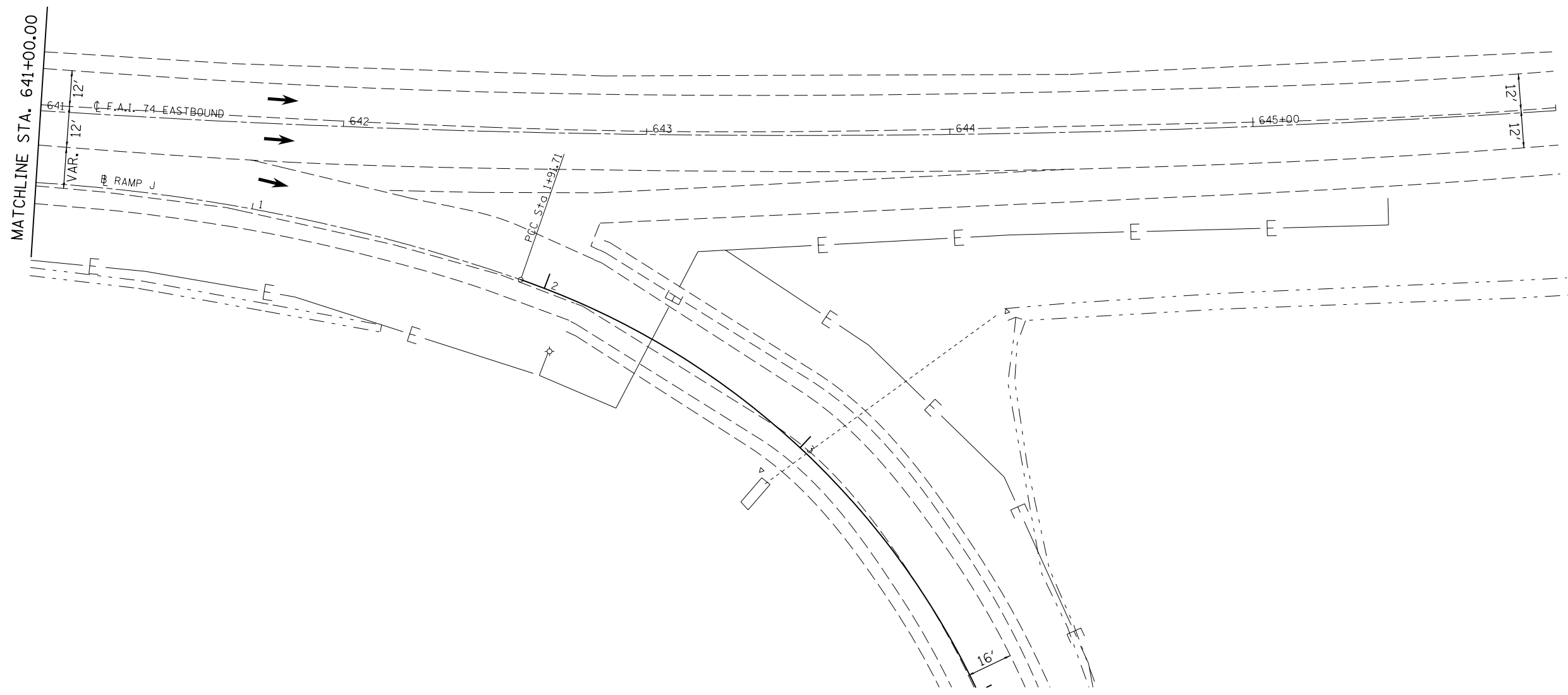
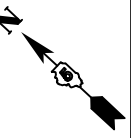
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| ei:\pw\work\p\idot\detersbj\0157116\0570570-sht-plan.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

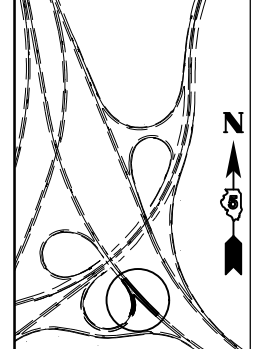
PLAN SHEET
FAI-74 EASTBOUND

SCALE: SHEET 2 OF 3 SHEETS STA. 636+00.00 TO STA. 641+00.00

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 74 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 80 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



PLAN SHEET LOCATION



| | | | |
|----------------------------------------------------------|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
| et:\pw\work\p\idot\detersbj\0157116\0570570-sht-plan.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - |

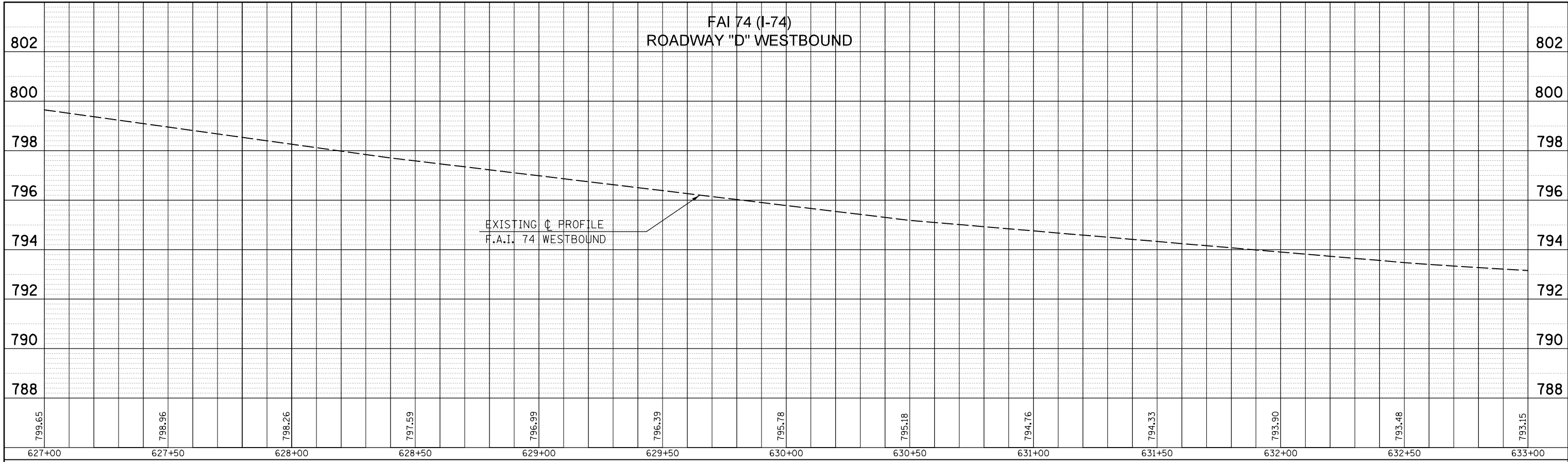
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PLAN SHEET
FAI-74 EASTBOUND**

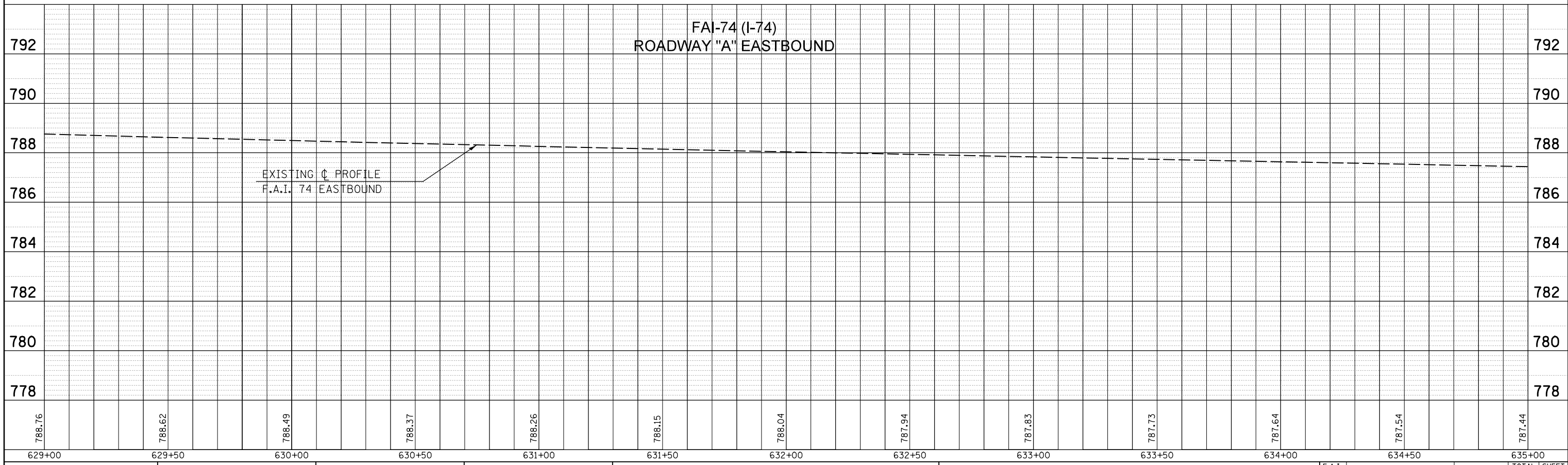
SCALE: SHEET 3 OF 3 SHEETS STA. 641+00.00 TO STA. 646+00.00

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 74 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 81 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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|---------------|-------------------|------|
| PLAN | SURVEYED | DATE |
| NOTE BOOK NO. | PLOTTED | BY |
| | ALIGNMENT CHECKED | |
| | GRADE FILE NAME | |



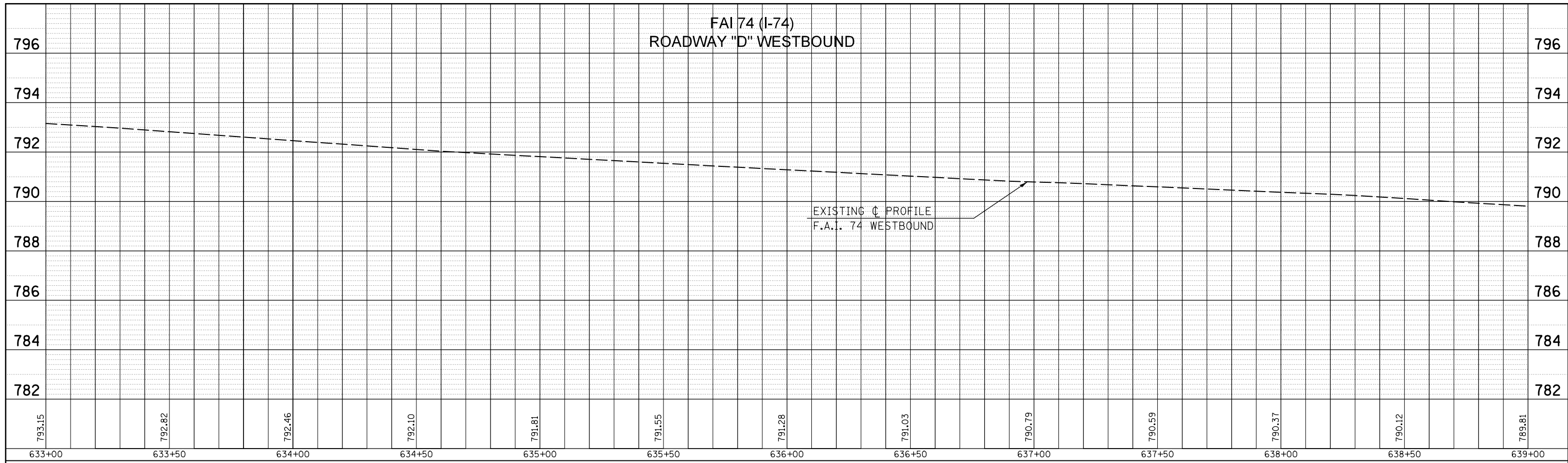
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| PROFILE | SURVEYED | DATE |
| NOTE BOOK NO. | GRADES CHECKED | BY |
| | STRUCTURE NOTATIONS CHECKED | |



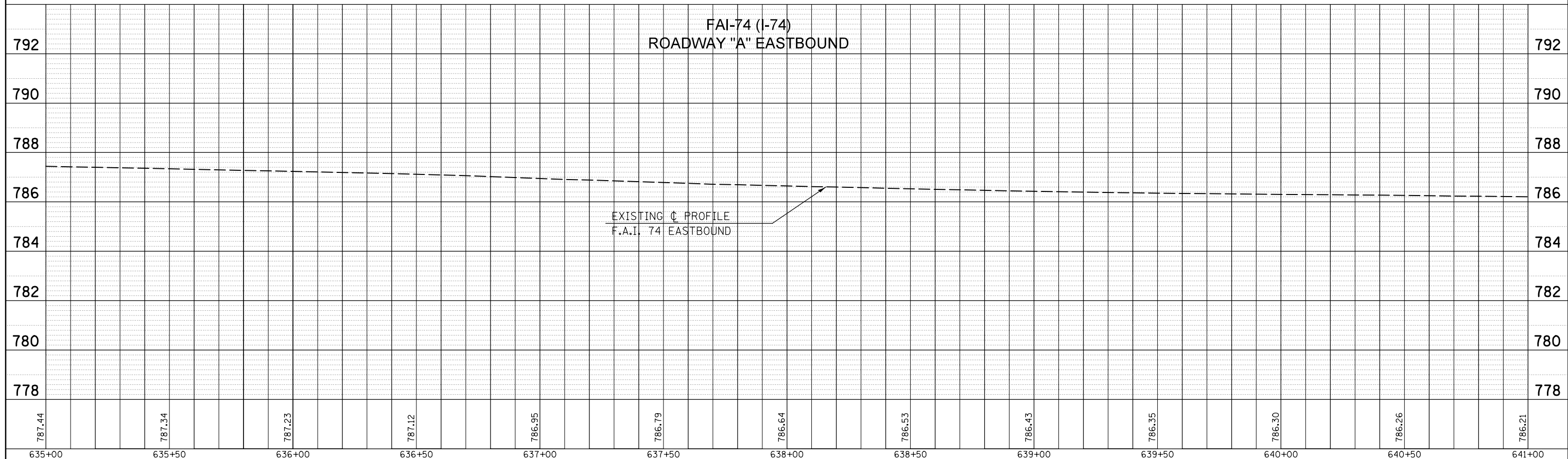
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|-----------------------------------------------------------|----------------------|------------|---------------------------|-----------------------------------------------------------------|--------------------------------|--------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | F.A.I. 74 PROFILE SHEET | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwork\pwork\detersbj\d0157116\0570570-sh1-profiles.dgn | DRAWN - | REVISED - | 74 | | | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 82 | |
| PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | CONTRACT NO. 70570 | | | | | | | |
| PLOT DATE = 8/13/2013 | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |

SCALE: SHEET NO. 1 OF 3 SHEETS STA. 627+00 TO STA. 635+00

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| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | ALIGNMENT CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |
| | CARD FILE NAME | | |

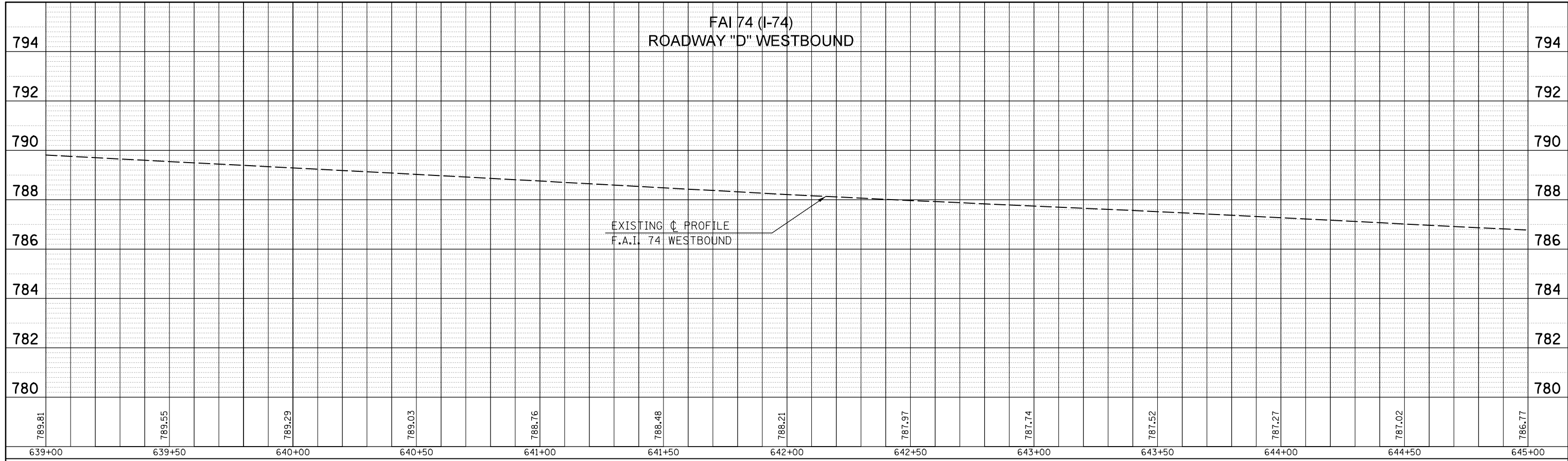


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| PROFILE | SURVEYED | BY | DATE |
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| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |
| | STRUCTURE NOTATIONS CHECKED | | |

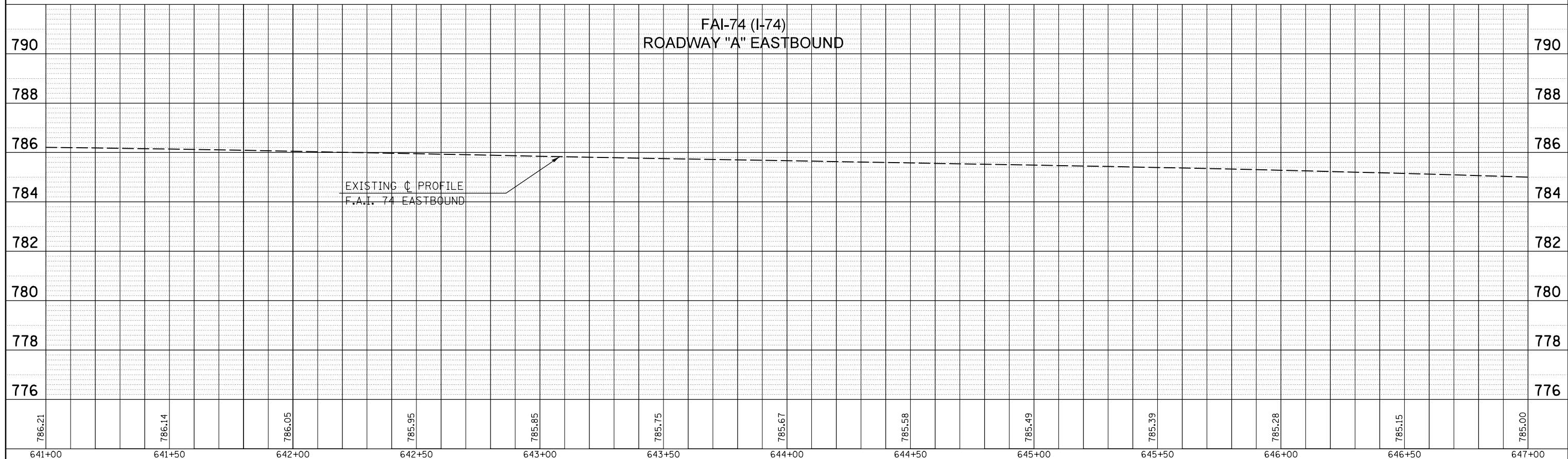


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| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | F.A.I. 74 PROFILE SHEET | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwork\pwork\detersbj\d0157116\0570570-sh1-profiles.dgn | DRAWN - | REVISED - | 74 | | | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 83 | |
| PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | CONTRACT NO. 70570 | | | | | | | |
| PLOT DATE = 8/13/2013 | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |
| SCALE: | | SHEET NO. 2 OF 3 SHEETS | | STA. 633+00 TO STA. 641+00 | | | | | | |

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| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | ALIGNMENT CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |
| | CARD FILE NAME | | |

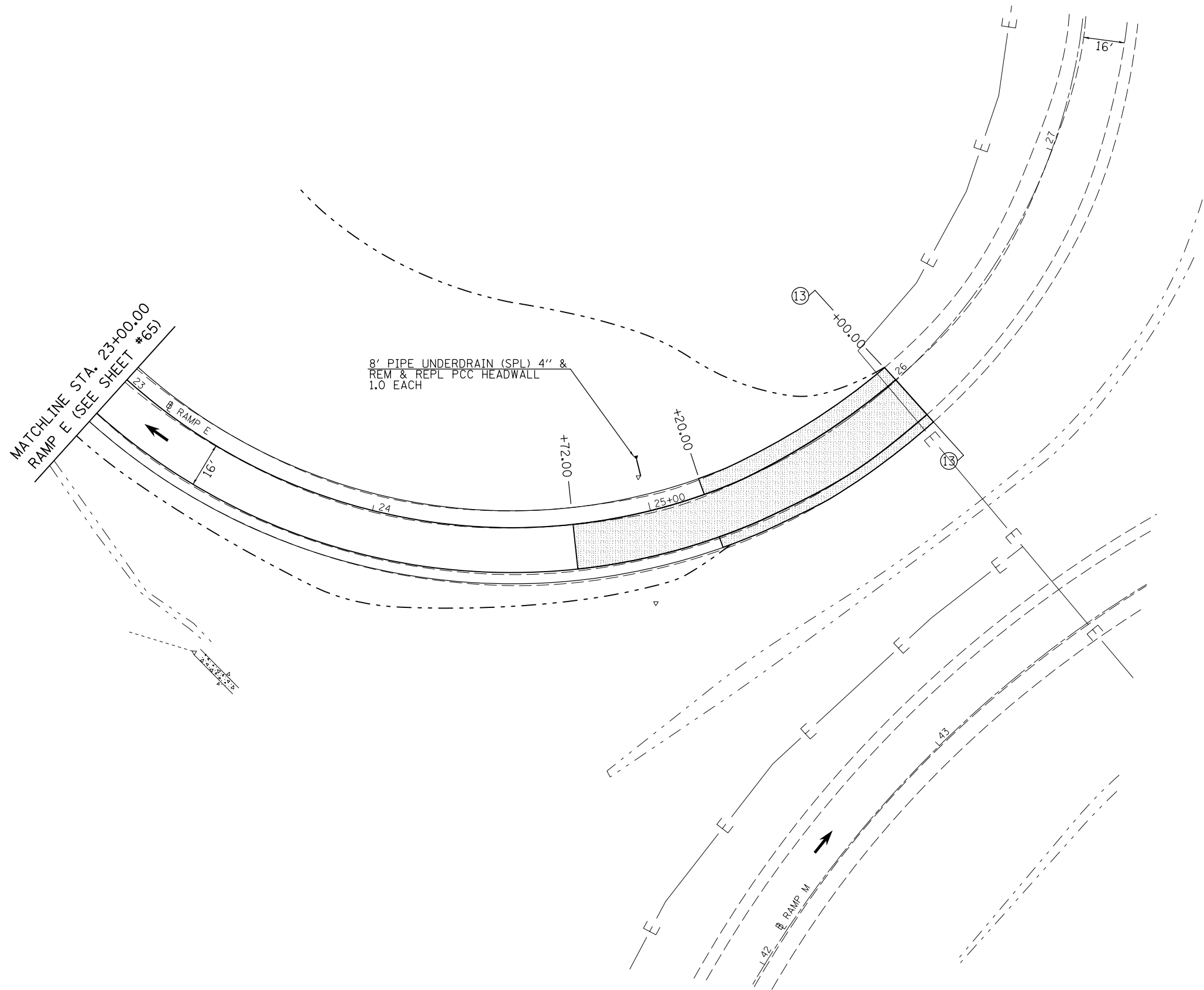


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|---------|-----------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
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| | ALIGNMENT CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |
| | CARD FILE NAME | | |

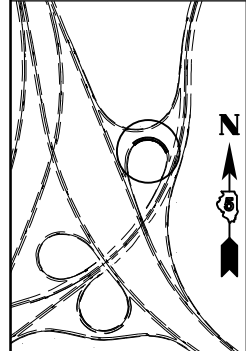


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|-----------------------------------------------------------|----------------------|------------|-----------|-----------------------------------------------------------------|--------------------------------|----------------------------------------------------|--------------------|--------|--------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | F.A.I. 74 PROFILE SHEET | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwork\pwork\detersbj\d0157116\0570570-shr-profiles.dgn | | DRAWN - | REVISED - | | | 74 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 84 |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - | | | CONTRACT NO. 70570 | | | | |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | |
| | | | | SCALE: | | SHEET NO. 3 OF 3 SHEETS STA. 639+00 TO STA. 647+00 | | | | |

 H.M.A. SURFACE REM., BUTT JOINT



PLAN SHEET LOCATION



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| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
| ei:\pw\work\p\idot\detersbj\d0157116\0570570-sht-plan.dgn | | DRAWN - | REVISED - |
| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |
| | | | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

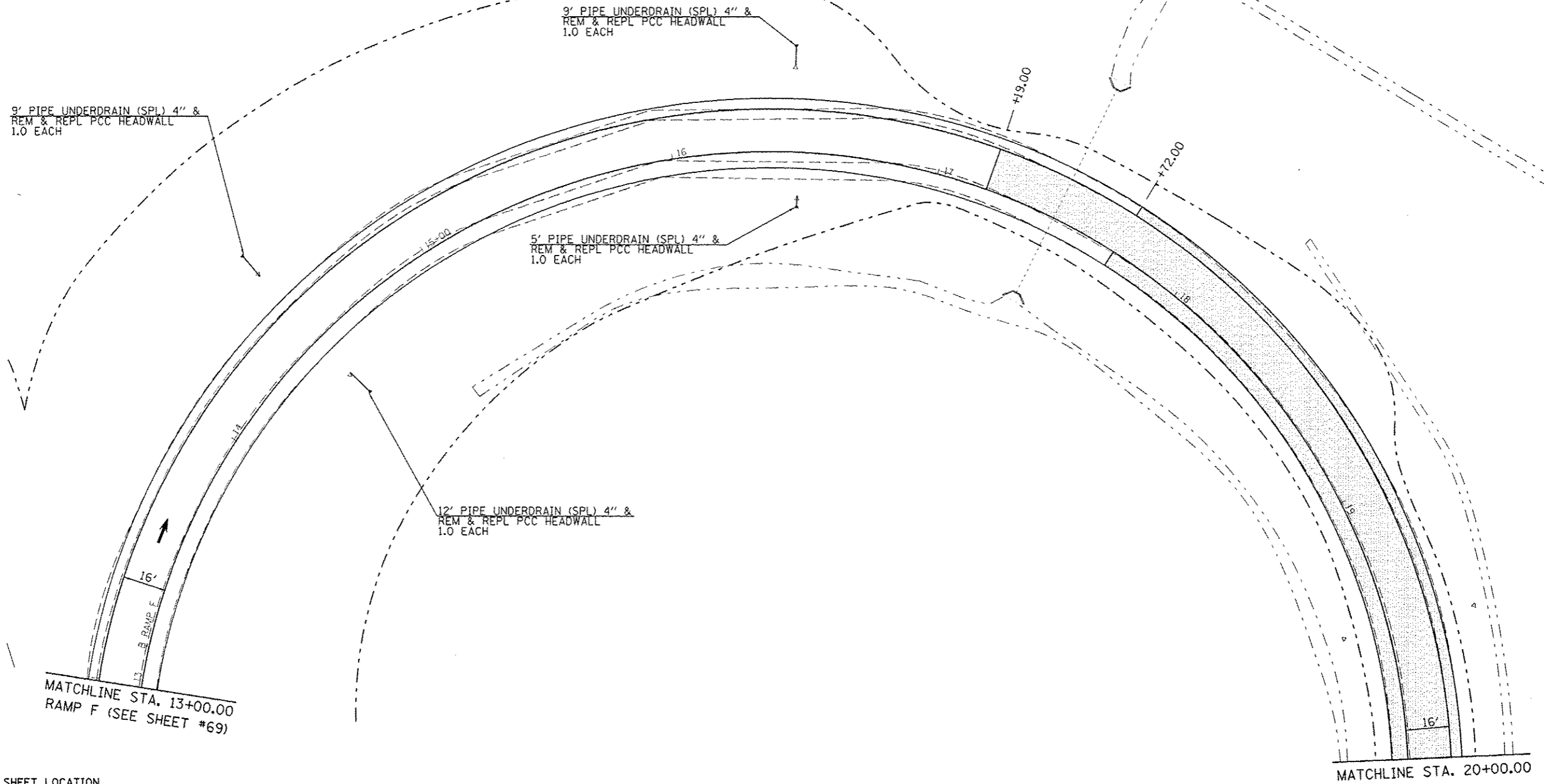
**PLAN SHEET
RAMP E**

SCALE: SHEET 1 OF 1 SHEETS STA. 23+00.00 TO STA. 27+00.00

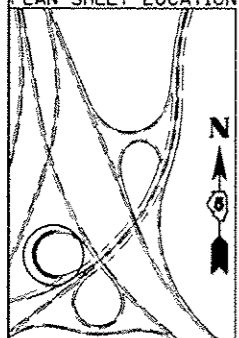
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 85 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



H.M.A. SURFACE REM., BUTT JOINT



PLAN SHEET LOCATION



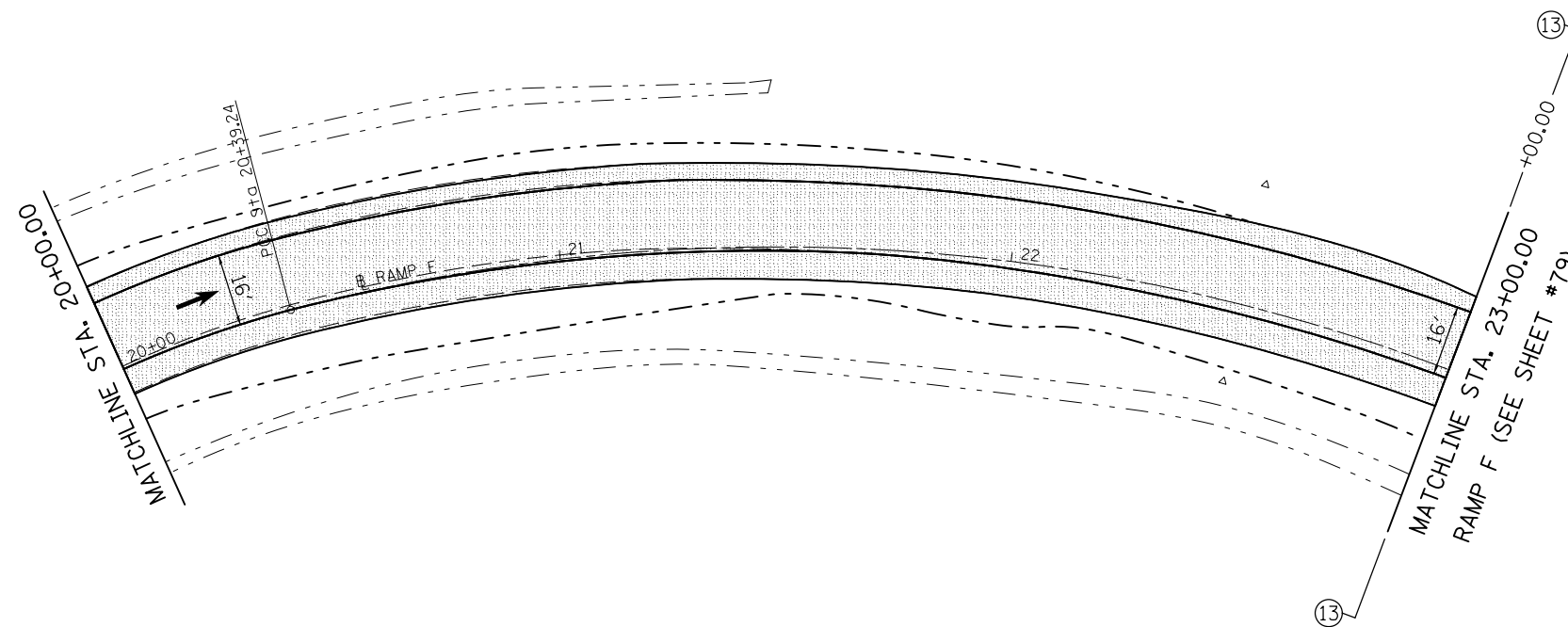
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| | PLOT SCALE = 30.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 9/9/2013 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

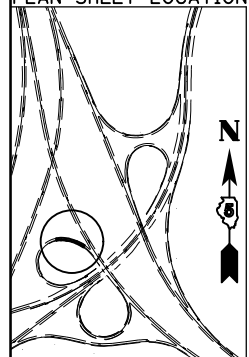
| | |
|------------------------------|----------------------------------------------------|
| PLAN SHEET RAMP F | |
| SCALE: | SHEET 1 OF 2 SHEETS STA. 13+00.00 TO STA. 20+00.00 |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 86 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

 H.M.A. SURFACE REM., BUTT JOINT



PLAN SHEET LOCATION



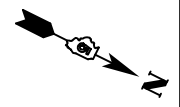
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

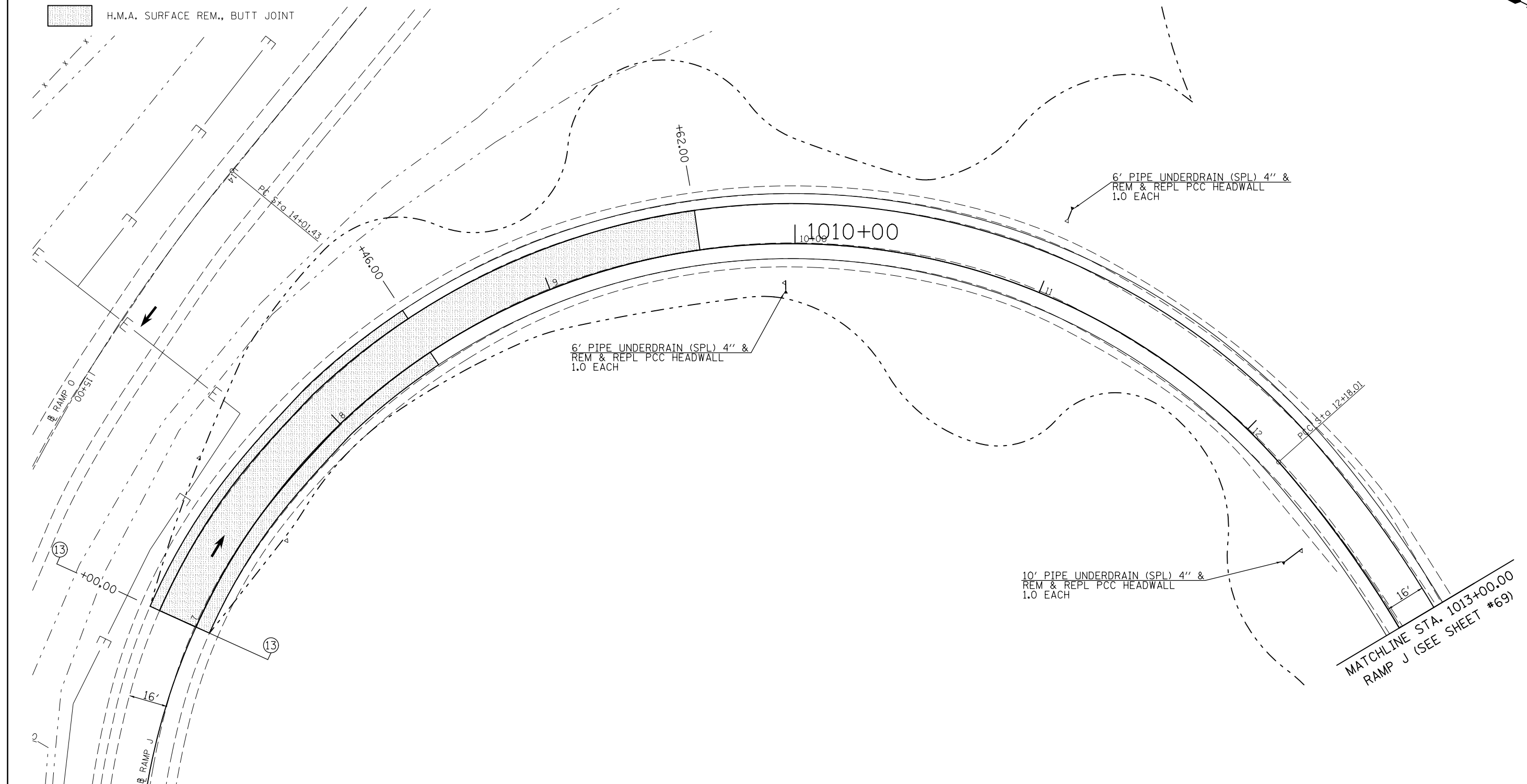
PLAN SHEET
RAMP F

SCALE: SHEET 2 OF 2 SHEETS STA. 20+00.00 TO STA. 23+00.00

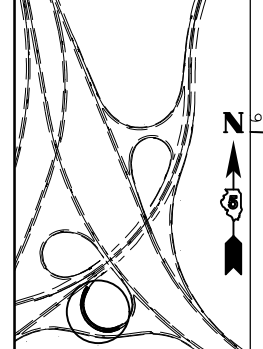
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 87 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



H.M.A. SURFACE REM., BUTT JOINT



PLAN SHEET LOCATION



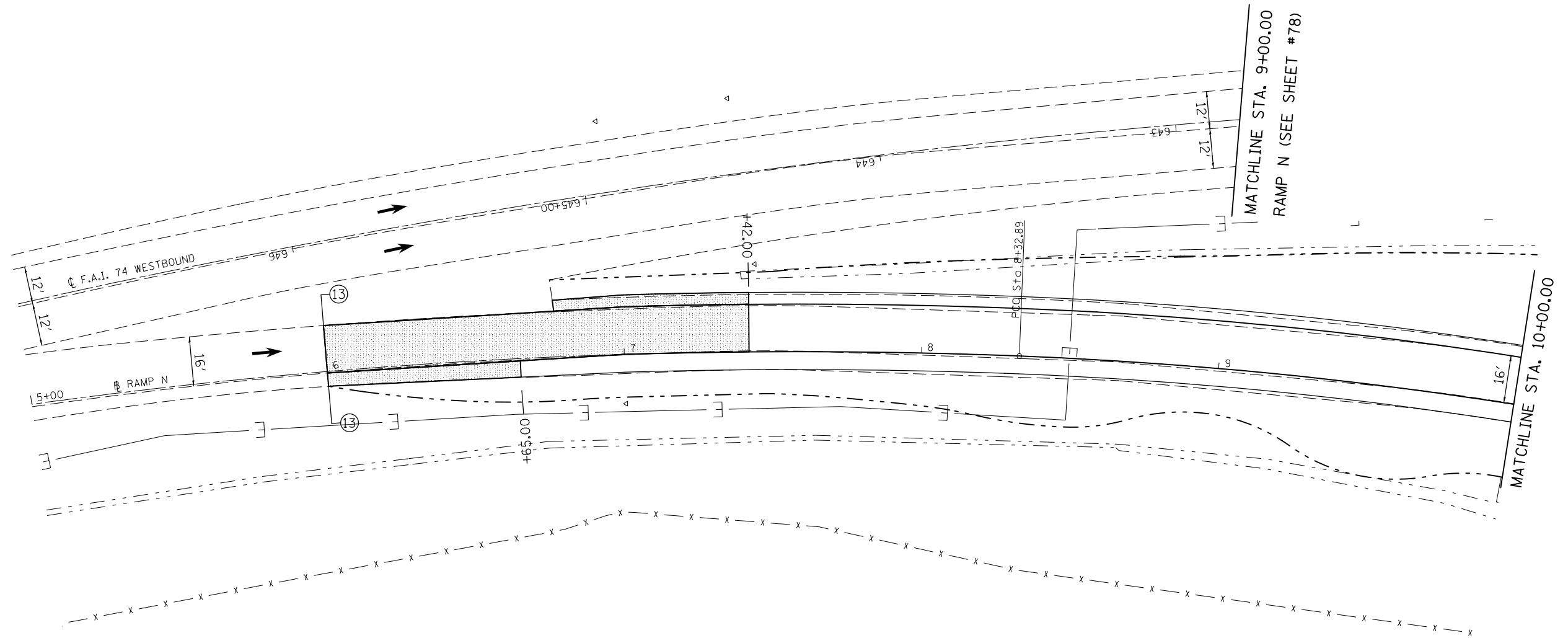
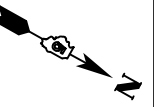
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| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
| ei:\pw\work\p\idot\detersbj\d0157116\0570570-sht-plan.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

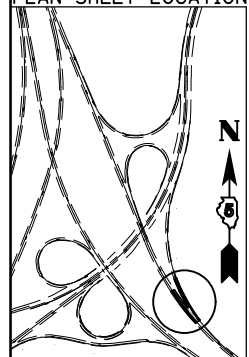
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|------------------------------|---------|-----------------|--------------------|
| PLAN SHEET RAMP J | | | |
| SCALE: | SHEET 1 | OF 1 | SHEETS |
| | | STA. 1006+00.00 | TO STA. 1013+00.00 |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 88 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

H.M.A. SURFACE REM., BUTT JOINT



PLAN SHEET LOCATION



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| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
| ei:\pw\work\p\idot\detersbj\0157116\0570570-sht-pln.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

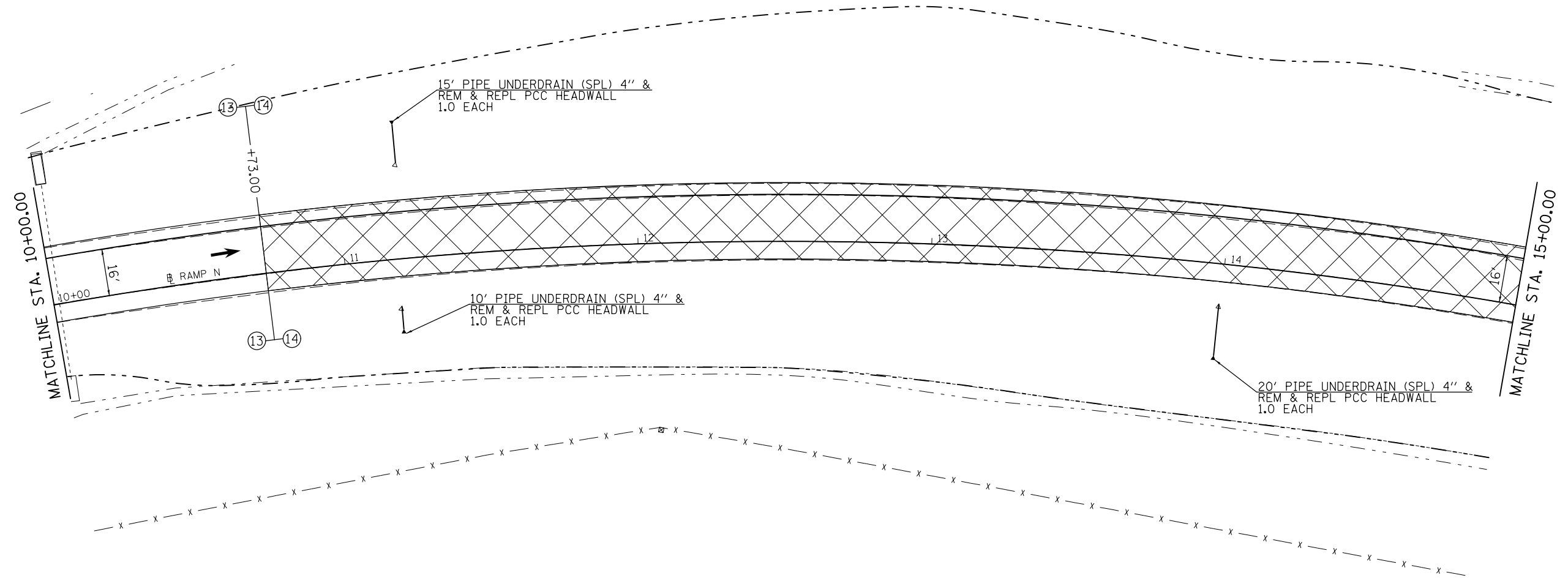
PLAN SHEET
RAMP N

SCALE: SHEET 1 OF 3 SHEETS STA. 5+00.00 TO STA. 10+00.00

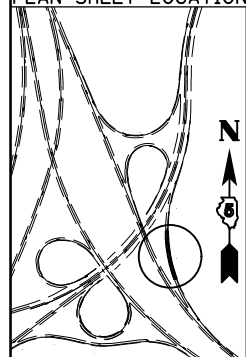
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 89 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



 PAVEMENT BREAKING



PLAN SHEET LOCATION



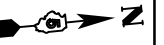
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

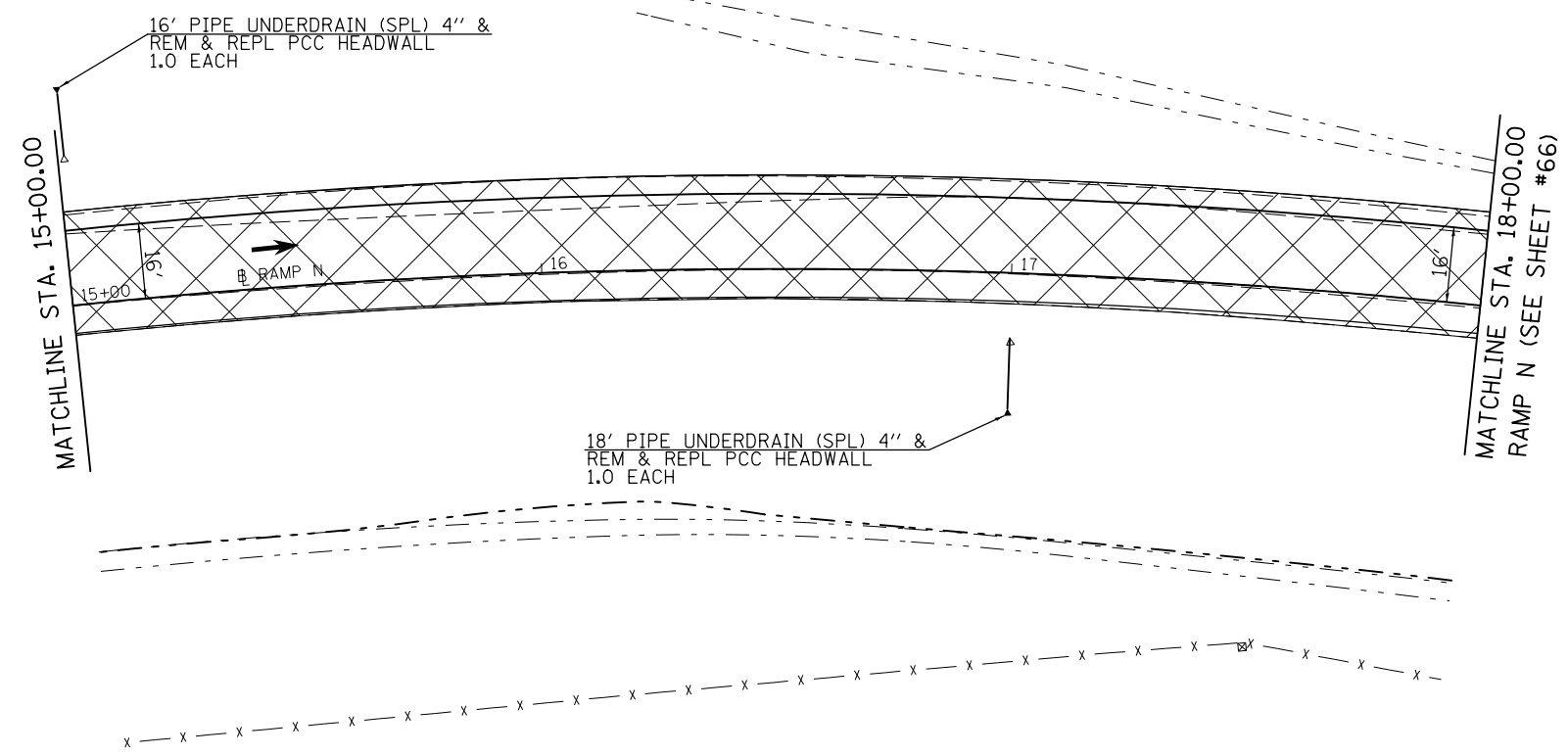
PLAN SHEET
RAMP N

SCALE: SHEET 2 OF 3 SHEETS STA. 10+00.00 TO STA. 15+00.00

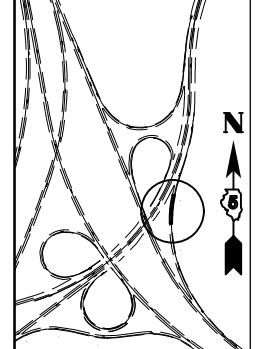
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 90 |
| CONTRACT NO. 70570 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



 PAVEMENT BREAKING



PLAN SHEET LOCATION



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| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - |

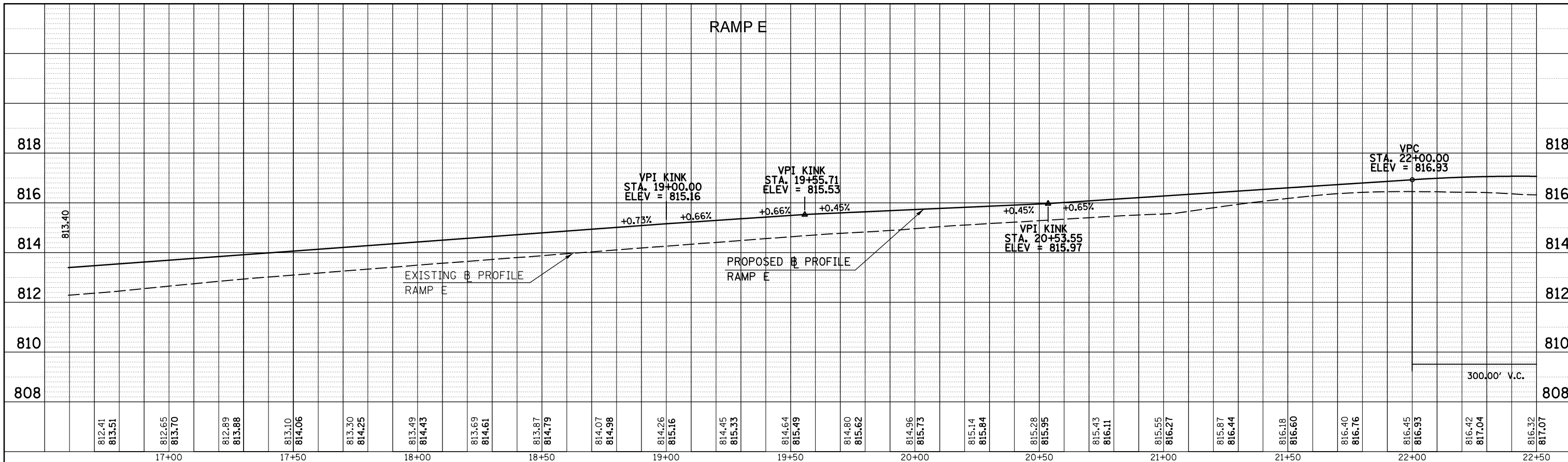
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PLAN SHEET
RAMP N**

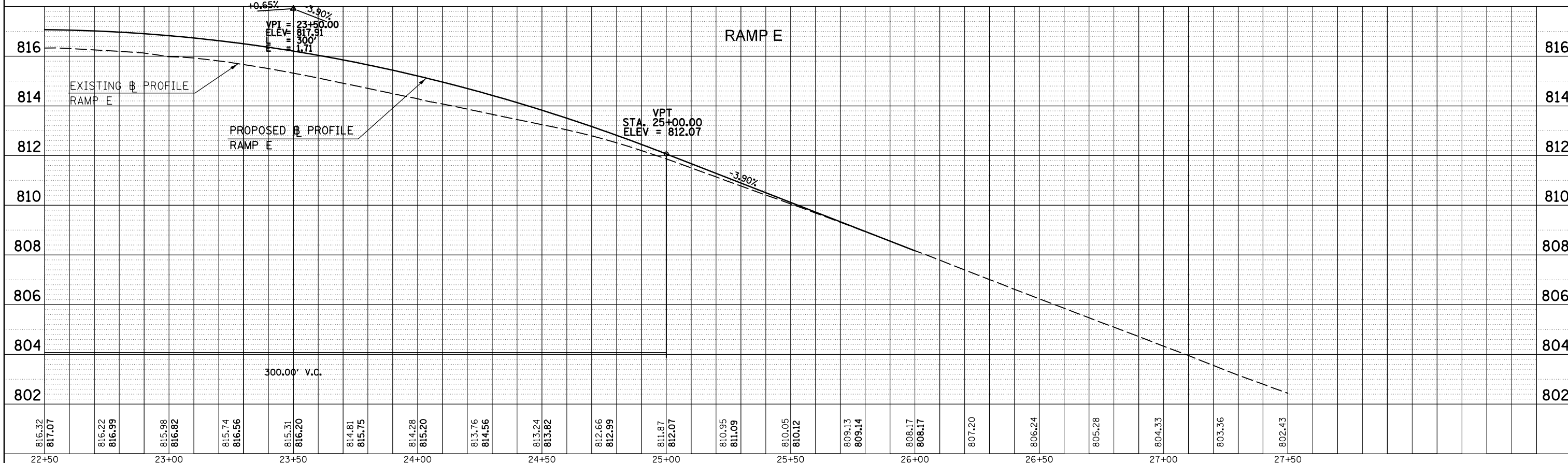
SCALE: SHEET 3 OF 3 SHEETS STA. 15+00.00 TO STA. 18+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|--------------------|--------|---------------------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 91 |
| CONTRACT NO. 70570 | | | ILLINOIS FED. AID PROJECT | |

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| PLAN | SURVEYED | BY | DATE |
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| | NOTE BOOK | | |
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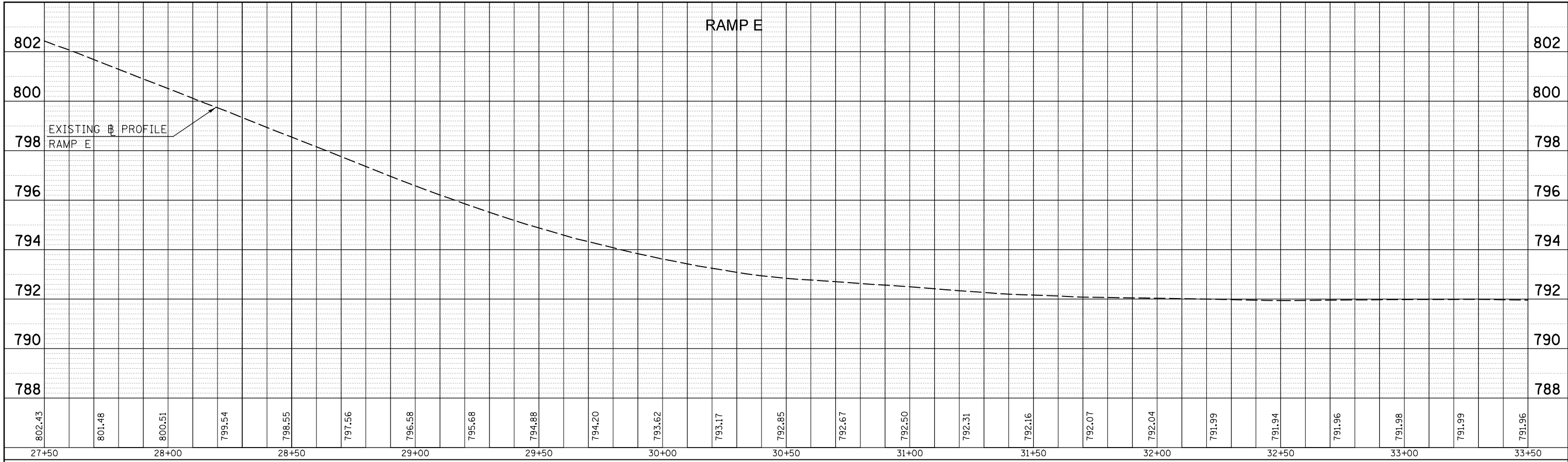


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| PROFILE | SURVEYED | BY | DATE |
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| | NO. | | |
| | STRUCTURE | | |
| | NOTATIONS | | |

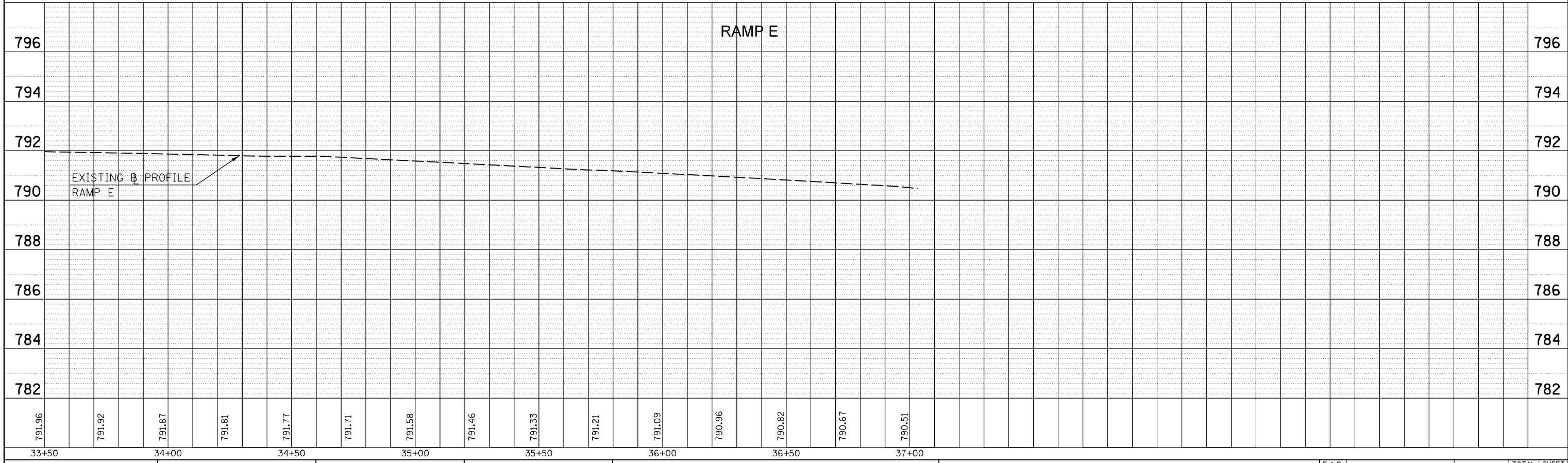


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| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | RAMP E PROFILE SHEET | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwork\pwork\detersbj\d0157116\0570570-shr-profiles.dgn | DRAWN - | REVISED - | 704 | | | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 93 | |
| PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | CONTRACT NO. 70570 | | | | | | | |
| PLOT DATE = 8/13/2013 | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |
| SCALE: | | SHEET NO. 1 OF 2 SHEETS | | STA. 16+50 TO STA. 27+50 | | | | | | |

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| PLAN | SURVEYED | DATE |
| NOTE BOOK NO. | PLOTTED | BY |
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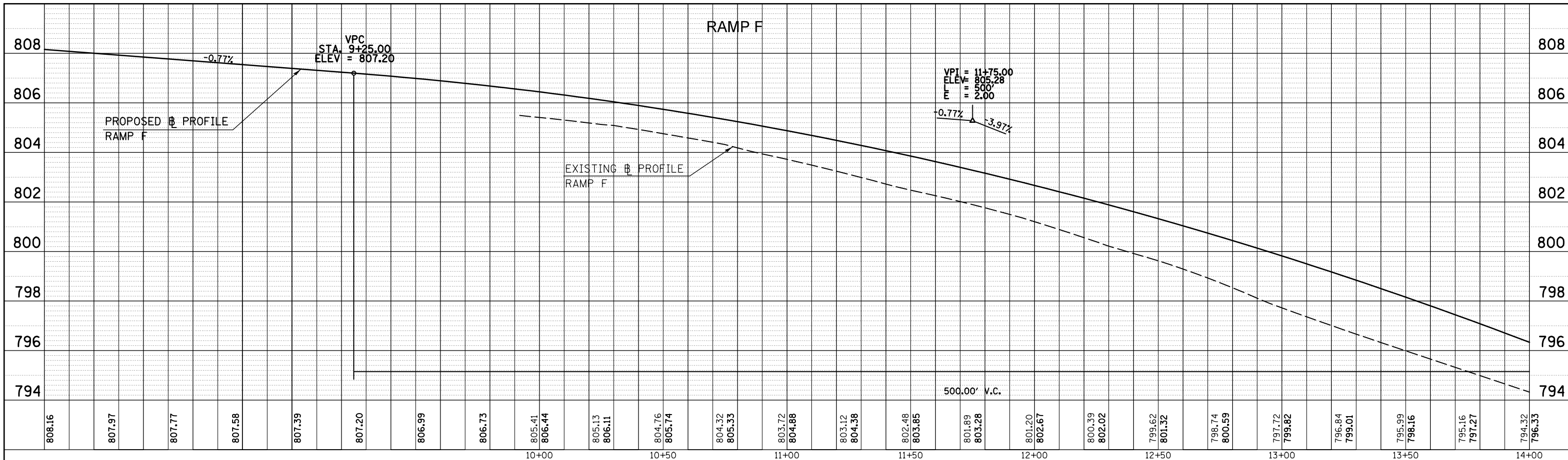


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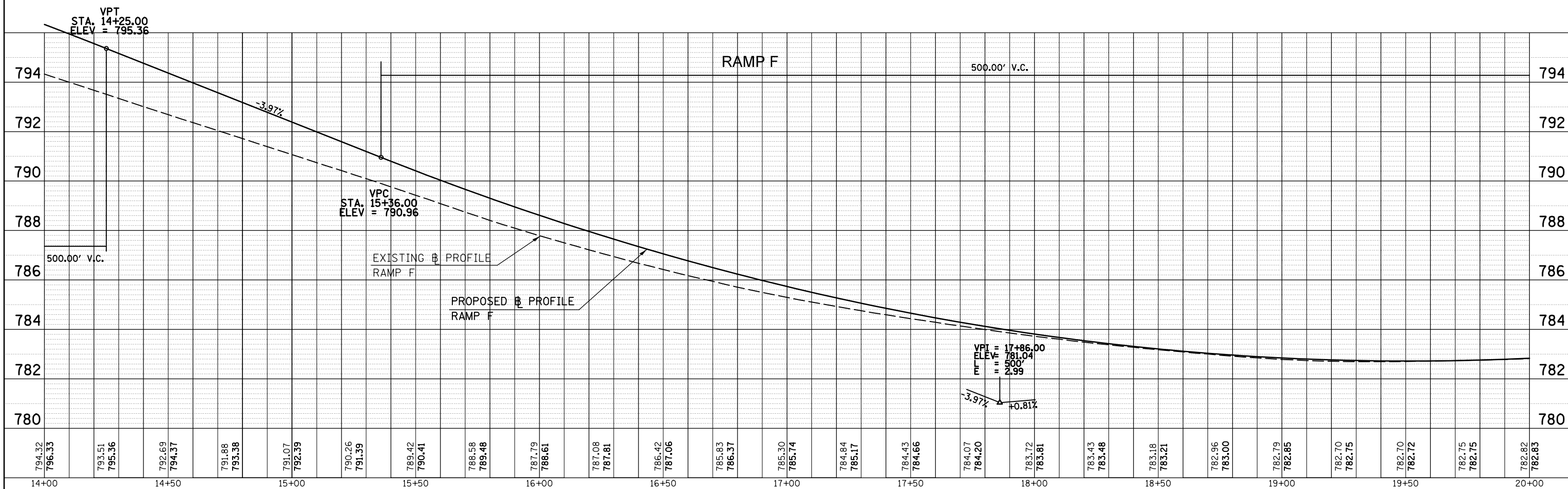


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|-----------------------------------------------------------|----------------------|------------|---------------------------|-----------------------------------------------------------|-----------------------------|-------------------------|---------|--------------------------|--------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | RAMP E PROFILE SHEET | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pwork\pwork\detersbj\d0157116\0570570-sht-profiles.dgn | DRAWN - | REVISED - | 704 | | | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 94 | |
| PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | CONTRACT NO. 70570 | | | | | | | |
| PLOT DATE = 8/13/2013 | DATE - | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | SCALE: | | SHEET NO. 2 OF 2 SHEETS | | STA. 27+50 TO STA. 37+00 | | |

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| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | ALIGNMENT CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| NOTE BOOK NO. | CARD FILE NAME | | |



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| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | ALIGNMENT CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| NOTE BOOK NO. | CARD FILE NAME | | |



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| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

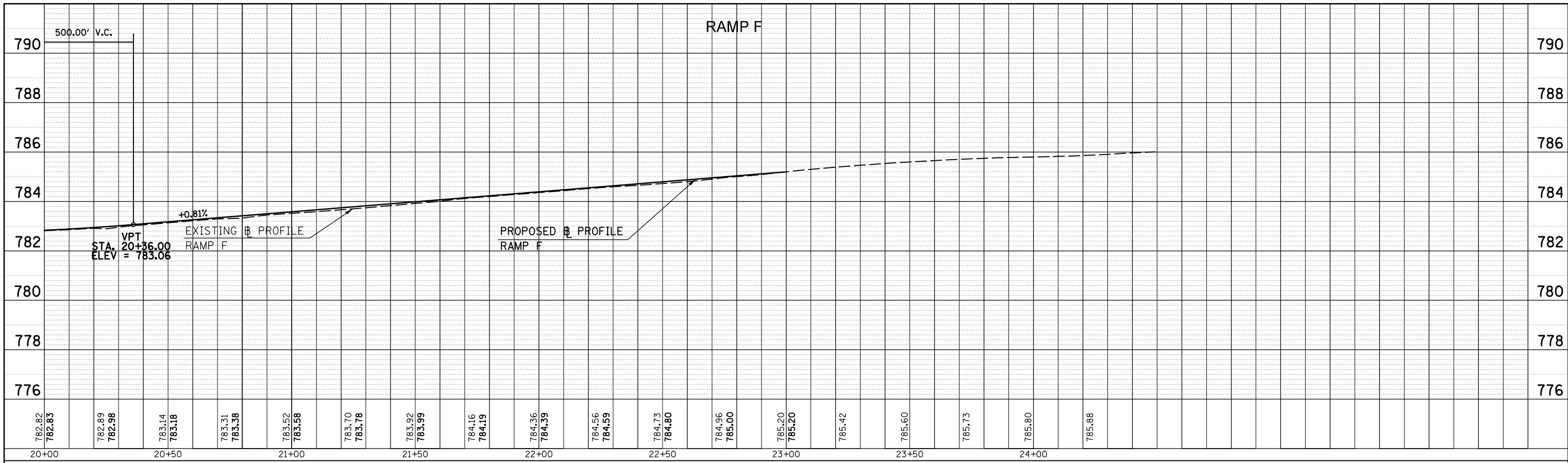
RAMP F PROFILE SHEET

SCALE: SHEET NO. 1 OF 2 SHEETS STA. 10+00 TO STA. 20+00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|--------|--------------|-----------|
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 95 |
| CONTRACT NO. | | | 70570 | |
| ILLINOIS FED. AID PROJECT | | | | |

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| PLAN | SURVEYED | BY | DATE |
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| | ALIGNED | | |
| | CHECKED | | |
| | FILE NAME | | |
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| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | STRUCTURE | | |
| | NOTATION | | |
| | NO. | | |



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| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
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| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |
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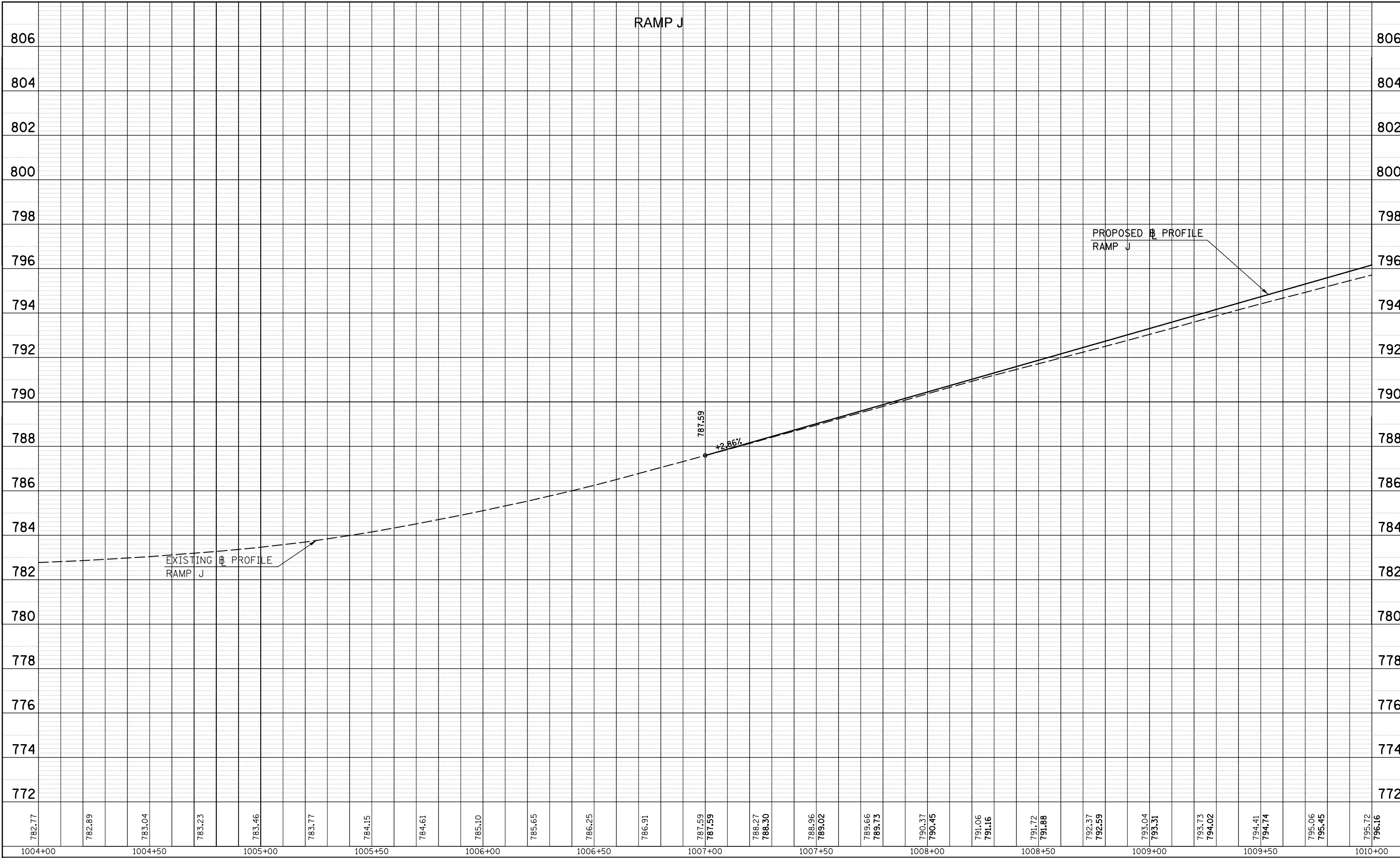
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

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| SCALE: | SHEET NO. 2 OF 2 SHEETS | STA. 20+00 TO STA. 24+50 |
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|--------------------|--------------------|--------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 96 |
| CONTRACT NO. 70570 | | | ILLINOIS FED. AID PROJECT | |

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| PLAN | SURVEYED | DATE |
| NOTE BOOK NO. | PLOTTED | BY |
| | ALIGNMENT CHECKED | |
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| PROFILE | SURVEYED | DATE |
| NOTE BOOK NO. | PLOTTED | BY |
| | GRADES CHECKED | |
| | STRUCTURE NOTATIONS CHECKED | |



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|----------------------------------------------------------|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
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| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

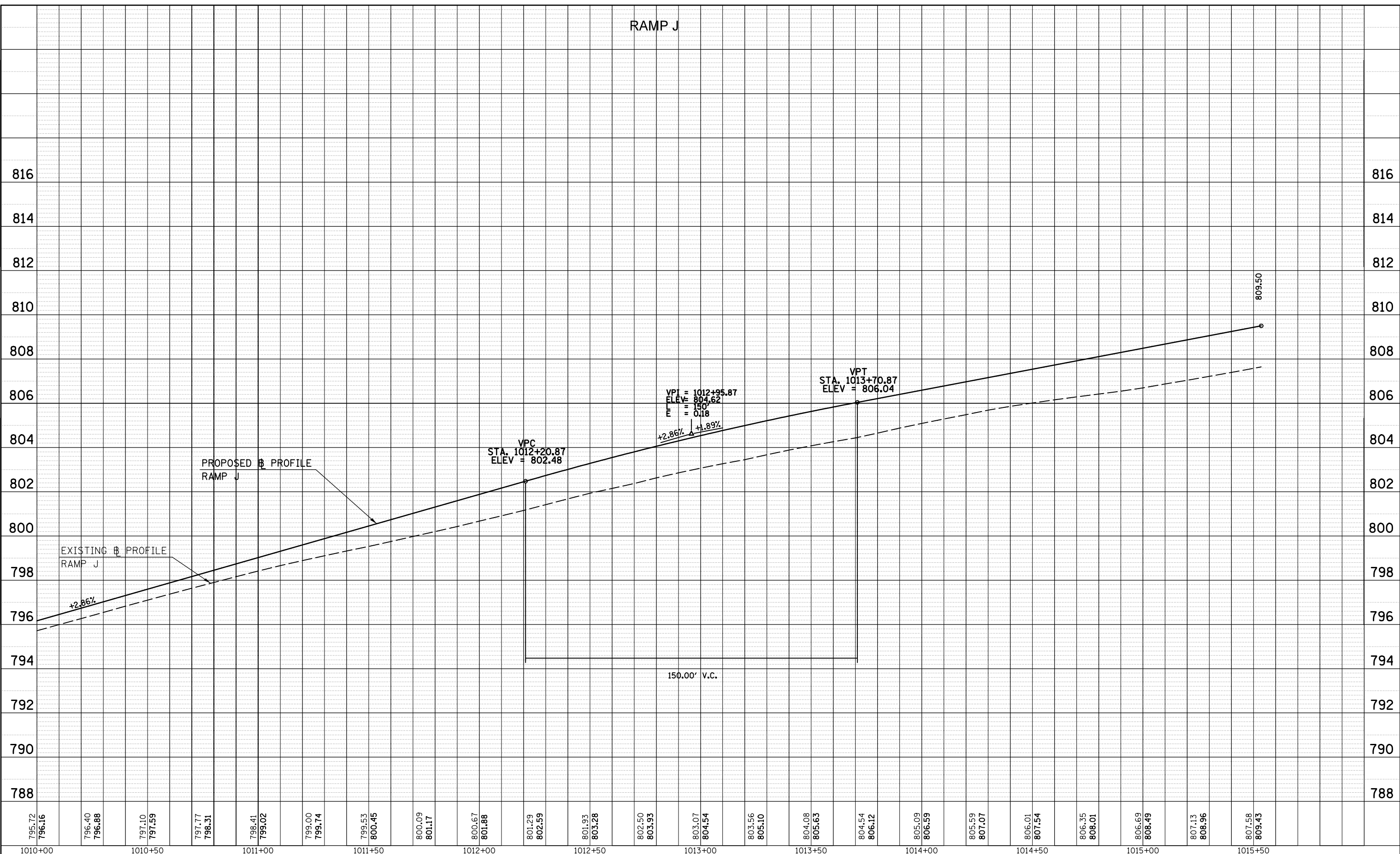
| | | |
|-----------------------------|-------------------------|------------------------------|
| RAMP J PROFILE SHEET | | |
| SCALE: | SHEET NO. 1 OF 2 SHEETS | STA. 1004+00 TO STA. 1010+00 |

| | | | | |
|--------------------|--------------------|--------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 97 |
| CONTRACT NO. 70570 | | | ILLINOIS FED. AID PROJECT | |

RAMP J

| | | | |
|------|---------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | NOTE BOOK NO. | | |
| | CHECKED | | |
| | FILE NAME | | |

| | | | |
|---------|---------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | NOTE BOOK NO. | | |
| | CHECKED | | |
| | FILE NAME | | |



| | | | |
|----------------------------------------------------------|----------------------|------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
| c:\pwork\pwork\detersbj\0157116\0570570-sht-profiles.dgn | | DRAWN - | REVISED - |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - |
| PLOT DATE = 8/13/2013 | | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

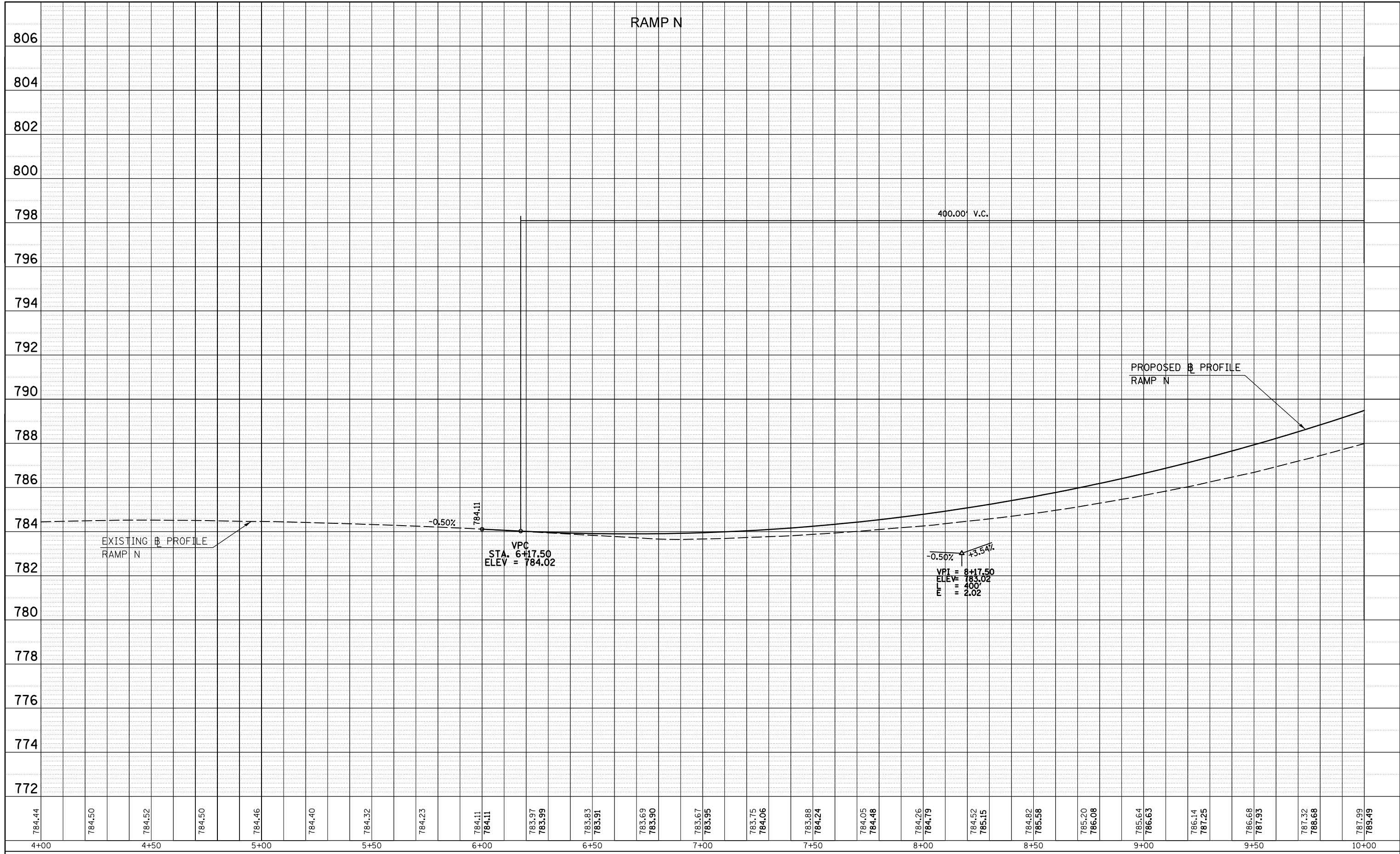
RAMP J PROFILE SHEET

SCALE: SHEET NO. 2 OF 2 SHEETS STA. 1010+00 TO STA. 1015+50

| | | | | |
|--------------------|--------------------|--------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 98 |
| CONTRACT NO. 70570 | | | ILLINOIS FED. AID PROJECT | |

| | | | |
|------|----------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | NOTE BOOK | | |
| | NO. | | |
| | CARD FILE NAME | | |

| | | | |
|---------|--------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | STRUCTURE | | |
| | NOT AT THIS OFFICE | | |



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|-----------------------------------------------------------|----------------------|------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
| c:\pwork\pwork\detersbj\d0157116\0570570-shr-profiles.dgn | | DRAWN - | REVISED - |
| | | CHECKED - | REVISED - |
| | | DATE - | REVISED - |
| | | | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

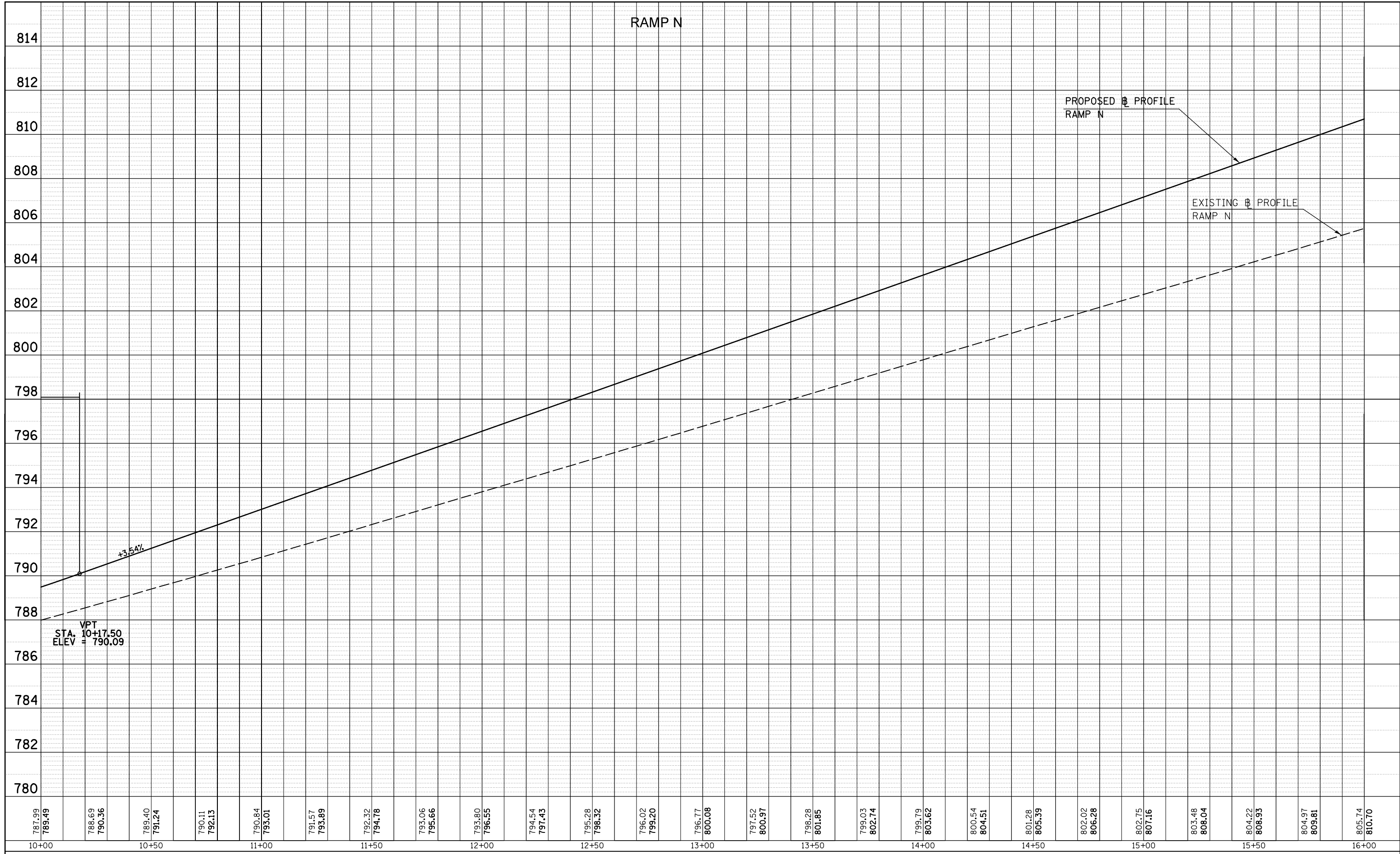
RAMP N PROFILE SHEET

SCALE: SHEET NO. 1 OF 4 SHEETS STA. 4+00 TO STA. 10+00

| | | | | |
|--------------------|--------------------|--------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 99 |
| CONTRACT NO. 70570 | | | ILLINOIS FED. AID PROJECT | |

| | | | |
|------|-----------------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |
| | CARD FILE NAME | | |

| | | | |
|---------|-----------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |
| | CARD FILE NAME | | |



| | | | |
|----------------------------------------------------------|-----------------------------|------------|-----------|
| FILE NAME = | USER NAME = detersbj | DESIGNED - | REVISED - |
| c:\pwork\pwork\detersbj\0157116\0570570-sht-profiles.dgn | | DRAWN - | REVISED - |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = 8/13/2013 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | |
|-----------------------------|--------------------------------------------------|
| RAMP N PROFILE SHEET | |
| SCALE: | SHEET NO. 2 OF 4 SHEETS STA. 10+00 TO STA. 16+00 |

| | | | | |
|--------------------|--------------------|--------|---------------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 704 | 57-20(HB,HB-1)BR-1 | MCLEAN | 440 | 100 |
| CONTRACT NO. 70570 | | | ILLINOIS FED. AID PROJECT | |