

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 870: IL 53 (ROHLWING RD)
AT CN RR, 0.1-MI SOUTH OF FULLERTON AVE
SECTION: FAP 0870 22 BJ
PROJECT: NHPP-JSQ5(268)
BRIDGE DECK OVERLAY, JOINT REPLACE/REPAIR
DUPAGE COUNTY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ	DUPAGE	38	1
		ILLINOIS	CONTRACT NO. 62T30	

FOR INDEX OF SHEETS AND HIGHWAY STANDARDS, SEE SHEET NO. 2

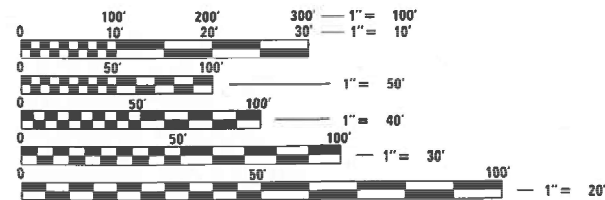
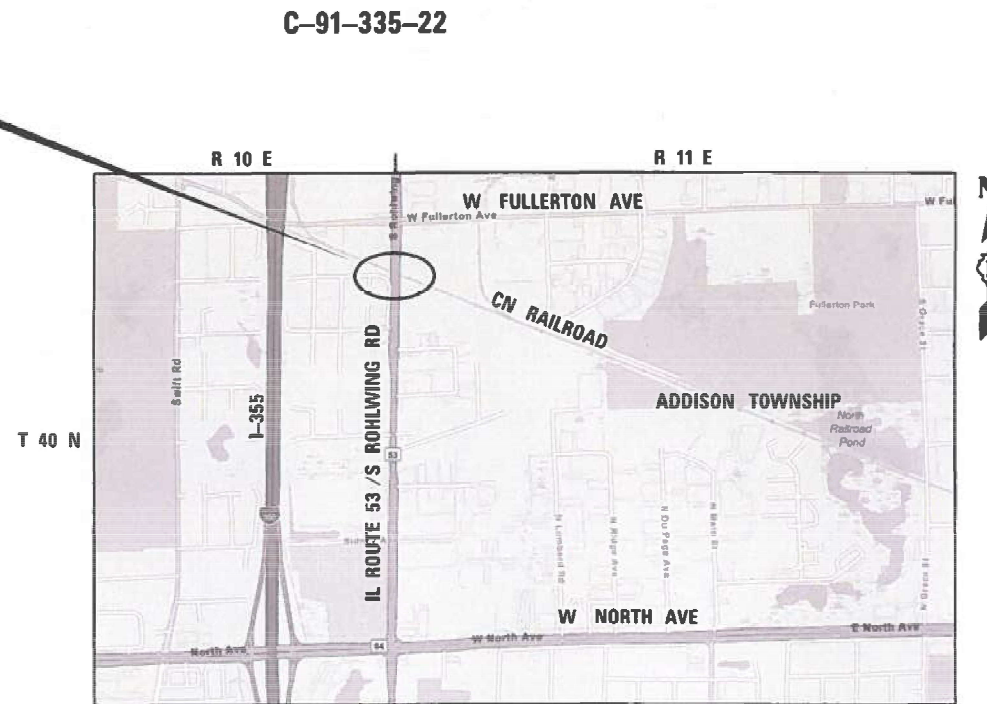
THIS PROJECT IS LOCATED IN THE VILLAGE OF ADDISON

TRAFFIC DATA

2023 ADT = 24,100 VPD
POSTED SPEED LIMIT = 40 MPH
OTHER PRINCIPAL ARTERIAL



PROJECT LOCATION
IL-53 (ROHLWING) OVER CN RR BRIDGE
STRUCTURE NO. 022-0075



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 OR 811
MEADE ELECTRIC CO. DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES 773-287-7672

PROJECT ENGINEER: VESELIN VELICHKOV (847) 705-4432
PROJECT MANAGER: FAWAD AQUEEL

CONTRACT NO. 62T30

GROSS LENGTH = 1062 FT. = 0.20 MILES
NET LENGTH = 307 FT. = 0.06 MILES



Alexander Carl Lane
ALEXANDER CARL LANE, P.E.
IL LIC. NO. 062-063261
EXP: 11/30/2025
DATE: 7/26/2024

THIS SEAL AND SIGNATURE PERTAINS TO SHEETS 1 TO 9

INFRASTRUCTURE ENGINEERING | INCORPORATED
1 South Wacker | Suite 2650 | Chicago, IL 60606
P 312.425.9560 | F 312.425.9564 | www.infrastructure-eng.com

CONTACT: ALEXANDER LANE (312) 477-0620

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED Aug 13th 2024

REGION 1 ENGINEER
[Signature]

October 4, 2024

ENGINEER OF DESIGN AND ENVIRONMENT
[Signature]

October 4, 2024

DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION
[Signature]

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INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS, HMA TABLE, AND GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5-7	TRAFFIC CONTROL PLANS
8	ROADWAY PLAN
9	PAVEMENT MARKING PLAN
10-32	STRUCTURAL DRAWINGS - S.N. 022-0075
33	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
34	PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE (BM-20)
35	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
36	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)
37	TYPICAL PAVEMENT MARKINGS (TC-13)
38	ARTERIAL ROAD INFORMATION SIGN (TC-22) / DRIVEWAY ENTRANCE SIGNING (TC-26)

STATE STANDARDS

STANDARD NO.	DRAWING NAME
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
606301-04	PC CONCRETE ISLANDS AND MEDIANS
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701502-09	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-09	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
700001-05	TYPICAL PAVEMENT MARKINGS
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

HMA TABLE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ Noles	QUALITY MANAGEMENT PROGRAM (QMP)
BUTT JOINT & HMA OVERLAY		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70, 1-3/4"	4% @ 70 Gyr	QC/QA
TEMPORARY PAVEMENT		
HOT-MIX ASPHALT BINDER COURSE, IL-19, N70 (10 INCH)	4% @ 70 Gyr	QC/QA
HOT-MIX ASPHALT STABILIZATION 6" AT STEEL PLATE BEAM GUARDRAIL		
HOT-MIX ASPHALT BINDER COURSE, IL-19, N70 (6 INCH)	4% @ 70 Gyr	QC/QA
QMP DESIGNATION: QUALITY CONTROL/ QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		
MIXTURE REQUIREMENT NOTES:		
1.THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.		
2.THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.		

GENERAL NOTES

- THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES.
- MEADE ELECTRIC COMPANY, THE IDOT DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR, LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES. CALL 773-287-7672 FOR THE INITIAL LOCATE. REQUEST FOR LOCATES OF PREVIOUSLY MARKED FACILITIES MAY BE AT THE CONTRACTOR'S EXPENSE.
- IN ADDITION TO FIELD REVIEW AND AERIAL DATA, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE BID PRICE FOR THE WORK.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO BEGINNING CONSTRUCTION AND ORDERING MATERIALS.
- THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.
- THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) AND USE/WASTE REVIEW (BDE 2290) SUBMITTALS, THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION 11.G.1. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICES AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSDURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSDURGA@ILLINOIS.GOV, A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, AREA TRAFFIC FIELD ENGINEER, AT WALTER.CZARNY@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE DEPARTMENT HAS DETERMINED THAT IN STREAM WORK IS NOT REQUIRED FOR THE WORK SPECIFIED IN THIS CONTRACT. THE DEPARTMENT HAS NOT OBTAINED A USACE PERMIT. IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING AN USACE PERMIT, IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER USACE PERMITS. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO SECURE AND COMPLY WITH A USACE PERMIT FOR CONTRACTOR'S ACTIVITIES WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM I.D.O.T. FIELD MAINTENANCE ENGINEERS.
- THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
- FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT. ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT SHALL BE EPOXY COATED UNLESS NOTED ON THE PLANS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS-RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTENT)" SHOWN IN PLANS.
- THE CENTERLINE IS FOR INFORMATION ONLY. IT WAS DEVELOPED FROM AERIALS AS A BEST FIT APPROXIMATION. ALIGNMENT, TIES, AND BENCHMARKS ARE NOT AVAILABLE.
- PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE AND SELECTIVE CLEARING SHALL BE DONE PRIOR TO CONSTRUCTION ACTIVITIES. CONTRACTOR SHALL CONTACT IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 ONE WEEK PRIOR TO STARTING WORK TO VERIFY FORESTRY LAYOUT.

MODEL: Default
FILE NAME: P:\P\22122-4675-00 IDOT Various Phase 2 (FB 206-002)11\10 16 8270D02CNCADD_Sheets\62730-sht-egeneral.dgn

<p>INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9560 F 312.425.9564 www.infrastructure-eng.com</p>	USER NAME = ALane	DESIGNED - CW	REVISED -
	PLOT SCALE = 20.0000' / in.	DRAWN - CW	REVISED -
		CHECKED - ACL	REVISED -
		DATE - 08/07/2024	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, STATE STANDARDS, HMA TABLE, AND GENERAL NOTES
IL-53 OVER CN RAILROAD**

SCALE: _____ STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 080 22 BJ	DUPAGE	38	2
			CONTRACT NO.	62T30
ILLINOIS FED. AID PROJECT				

PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				URBAN	
				80% FEDERAL 20% STATE	100% STATE
20200100	EARTH EXCAVATION	CU YD	15	15	
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	418	418	
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	430	430	
40604172	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70	TON	87	87	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	185.0	185.0	
44000100	PAVEMENT REMOVAL	SQ YD	287	287	
44000600	SIDEWALK REMOVAL	SQ FT	185	185	
44003100	MEDIAN REMOVAL	SQ FT	1,719	1,719	
50102400	CONCRETE REMOVAL	CU YD	53	53	
50157300	PROTECTIVE SHIELD	SQ YD	380	380	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	60	60	
50300300	PROTECTIVE COAT	SQ YD	1,706	1,706	
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	8,800	8,800	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	7,070	7,070	
50800515	BAR SPLICERS	EACH	112	112	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	318	318	
59300100	CONTROLLED LOW-STRENGTH MATERIAL	CU YD	15	15	
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	1,719	1,719	
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	15.0	15.0	
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1	
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1	

PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				URBAN	
				80% FEDERAL 20% STATE	100% STATE
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1	
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	1	1	
67100100	MOBILIZATION	L SUM	1	1	
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	220	220	
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	5,271	5,271	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	800	800	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	800	800	
70600241	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2	
70600341	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2	
* 72000100	SIGN PANEL - TYPE 1	SQ FT	7.5	7.5	
* 72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	1	1	
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	13.5	13.5	
* 78004635	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 7"	FOOT	594	594	
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	204	204	
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	4,138	4,138	
* 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	526	526	
* 78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	437	437	
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	153	153	
* 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	56	56	
* 78011040	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	594	594	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	4	4	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	4	4	
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	1,860	1,860	

* = SPECIALTY ITEM

MODEL: Default
FILE NAME: P:\P-22122-4675-00 IDOT Various Phase 2 (RTE 206-002) \WO 16 62T30DCNCARD_Sheets\62T30-Sht-500-01.dgn

INFRASTRUCTURE ENGINEERING <small>INCORPORATED</small> 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9560 F 312.425.9564 www.infrastructure-eng.com	USER NAME = ALane DESIGNED - CW DRAWN - CW CHECKED - ACL DATE - 08/07/2024	REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES IL-53 OVER CN RAILROAD	F.A.P. RTE. 870 SECTION FAP 0870 22 BJ COUNTY ILLINOIS TOTAL SHEETS 38 SHEET NO. 3
	PLOT SCALE = 20.0000' / in. DATE - 08/07/2024	SCALE: _____ TO STA. _____ TO STA. _____			DUPAGE CONTRACT NO. 62T30 ILLINOIS FED. AID PROJECT

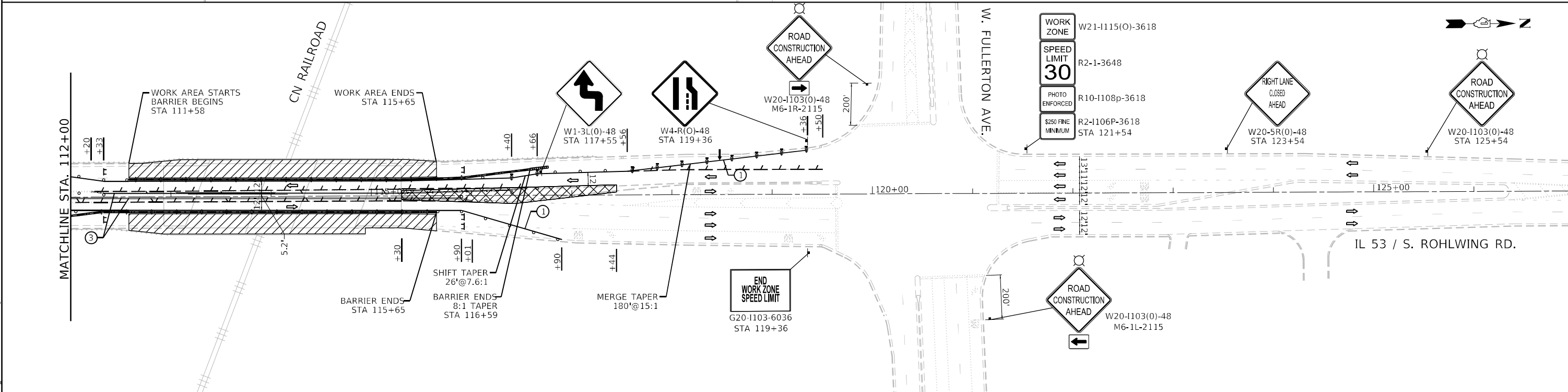
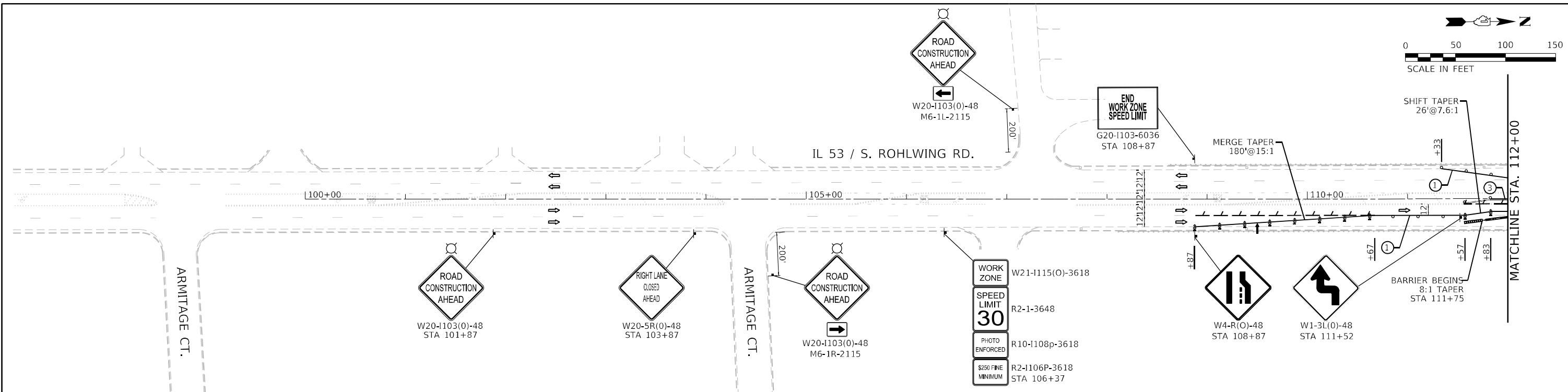
PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				URBAN		
				0059	80% FEDERAL 20% STATE	100% STATE
K1004595	PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE	L SUM	1		1	
X0300062	GRAFFITI REMOVAL	SQ YD	5	5		
X5030539	FLOOR DRAIN TO BE CLEANED	EACH	12		12	
X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FEET	708	708		
X5030250	BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	960	960		
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	60		60	
X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12		
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1		
X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	110	110		
Z0001700	APPROACH SLAB REPAIR (FULL DEPTH)	SQ YD	17	17		
Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	28	28		
Z0001903	STRUCTURAL STEEL REMOVAL	POUND	8,800	8,800		
Z0005216	HOT-MIX ASPHALT STABILIZATION 6" AT STEEL PLATE BEAM GUARD RAIL	SQ YD	49	49		
Z0006016	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 3/4 INCHES	SQ YD	1,115	1,115		
Z0007122	REMOVING AND RE-ERECTING EXISTING RAILING	FOOT	78	78		
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	1,115	1,115		
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	396	396		
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	1	1		
Z0018051	DRAINAGE SCUPPERS TO BE ADJUSTED	EACH	4	4		
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	4		4	
Z0029090	DIAMOND GRINDING (BRIDGE SECTION)	SQ YD	1,216	1,216		
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	62	62		
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1		

* = SPECIALTY ITEM

PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				URBAN		
				0059	80% FEDERAL 20% STATE	100% STATE
* X7830052	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR	EACH	110	110		
Z0049100	RAISED PAVEMENT MARKER REFLECTOR REPLACEMENT					
Z0062456	TEMPORARY PAVEMENT	SQ YD	287	287		
Z0064600	SELECTIVE CLEARING	ACRE	0.03	0.03		
Z0065700	SLOPE WALL REPAIR	SQ YD	87	87		
Z0073200	TEMPORARY SHORING AND CRIBBING	EACH	13	13		
Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1	1		

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INFRASTRUCTURE ENGINEERING <small>INCORPORATED</small> 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9580 F 312.425.9584 www.infrastructure-eng.com	USER NAME = ALane	DESIGNED - CW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES IL-53 OVER CN RAILROAD	F.A.P. RTE. 870	SECTION FAP 0870 22 BJ	COUNTY	TOTAL SHEETS 38	SHEET NO. 4
	PLOT SCALE = 20.0000' / in.	CHECKED - ACL	REVISED -			DUPAGE	CONTRACT NO. 62T30			
	DATE = 08/07/2024					ILLINOIS FED. AID PROJECT				
						SCALE:	STA.	TO STA.		



MOT SYMBOL LEGEND

- DRUM OR BARRICADE
- ⊞ DIRECTION INDICATOR BARRICADE
- ▨ WORK AREA
- ⊞ TYPE III BARRICADE WITH FLASHING LIGHTS
- ⊞ BARRICADE OR DRUM WITH FLASHING LIGHT
- ⇨ DIRECTION OF TRAFFIC
- ⊞ SIGN ON PORTABLE OR PERMANENT SUPPORT
- ▬ TEMPORARY CONCRETE BARRIER
- ▬ IMPACT ATTENUATOR
- ◆ CRYSTAL, BIDIRECTIONAL BARRIER WALL REFLECTOR
- ⊞ PAVEMENT MARKING REMOVAL
- ▨ TEMPORARY PAVEMENT

MOT PROPOSED LEGEND

- ① TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (WHITE)
- ② TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (YELLOW)
- ③ TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (DOUBLE YELLOW @ 11" C-C)

NOTE: REMOVE CONFLICTING PAVEMENT MARKING USING PAVEMENT MARKING REMOVAL - WATERBLASTING

MODEL: P:\infra\...
 FILE NAME: P:\infra\...
 USER: ALane

INFRASTRUCTURE ENGINEERING
 1 South Wacker | Suite 2650 | Chicago, IL 60606
 P 312.235.9598 | F 312.475.9594 | www.infrastructure-eng.com

USER NAME = ALane	DESIGNED - CW	REVISED -
PLOT SCALE = 100,000' +/-	DRAWN - CW	REVISED -
DATE - 08/07/2024	CHECKED - ACL	REVISED -

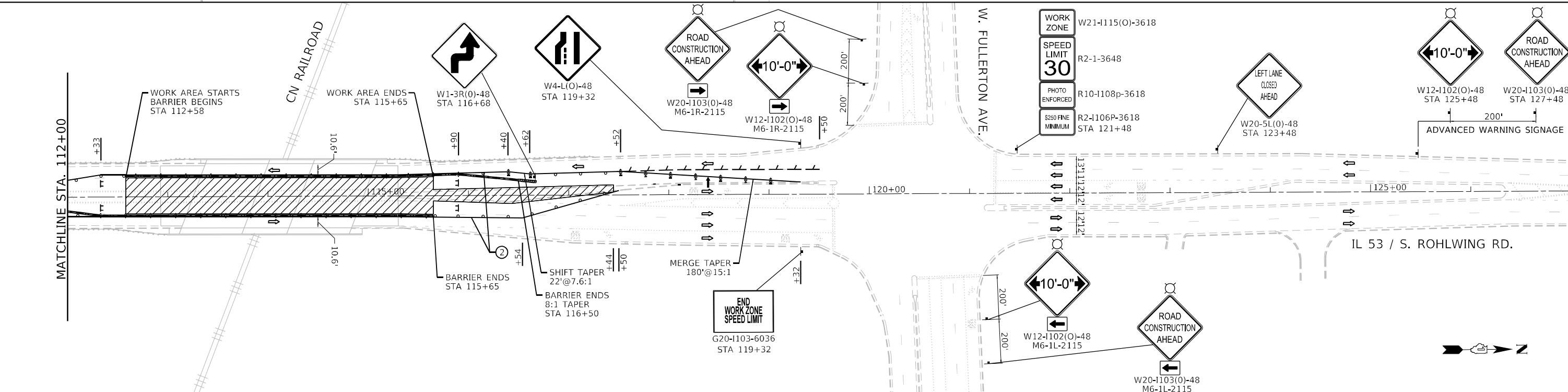
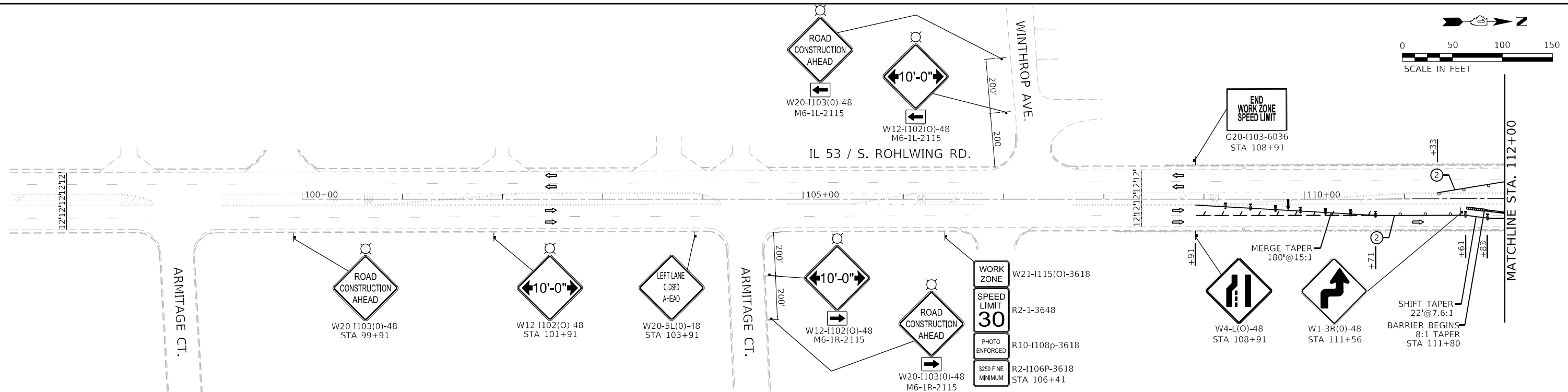
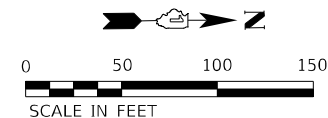
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLANS
STAGE 1**

SCALE: 1" = 50'

STA. TO STA.

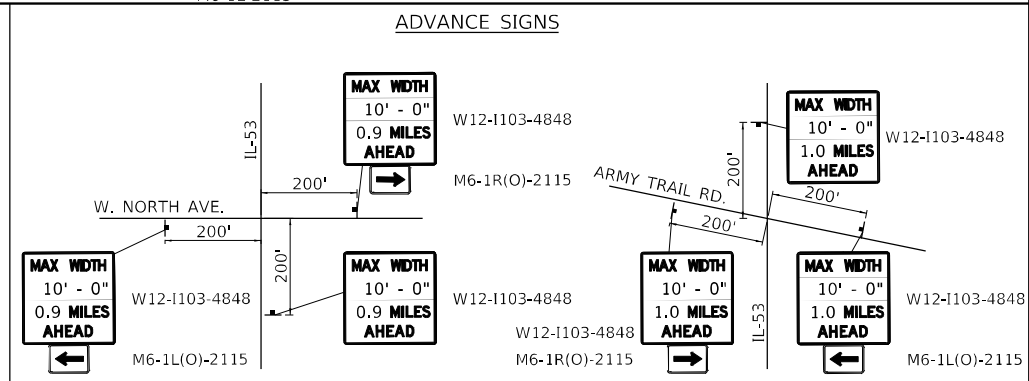
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ	DUPAGE	38	6
CONTRACT NO. 62T30			ILLINOIS FED. AID PROJECT	



- MOT SYMBOL LEGEND**
- DRUM OR BARRICADE
 - ⊕ DIRECTION INDICATOR BARRICADE
 - ▨ WORK AREA
 - ⊣ TYPE III BARRICADE WITH FLASHING LIGHTS
 - ⊞ BARRICADE OR DRUM WITH FLASHING LIGHT
 - ⇄ DIRECTION OF TRAFFIC
 - ⊣ SIGN ON PORTABLE OR PERMANENT SUPPORT
 - TEMPORARY CONCRETE BARRIER
 - ▨ IMPACT ATTENUATOR
 - ◇ CRYSTAL, BIDIRECTIONAL BARRIER WALL REFLECTOR
 - ⊣ PAVEMENT MARKING REMOVAL
 - ▨ TEMPORARY PAVEMENT

- MOT PROPOSED LEGEND**
- ① TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (WHITE)
 - ② TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (YELLOW)
 - ③ TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (DOUBLE YELLOW @ 11" C-C)

NOTE: REMOVE CONFLICTING PAVEMENT MARKING USING PAVEMENT MARKING REMOVAL - WATERBLASTING



USER NAME = ALane	DESIGNED - CW	REVISED -
PLOT SCALE = 100,000' +/-	DRAWN - CW	REVISED -
	CHECKED - ACL	REVISED -
	DATE - 08/07/2024	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLANS
STAGE 2**

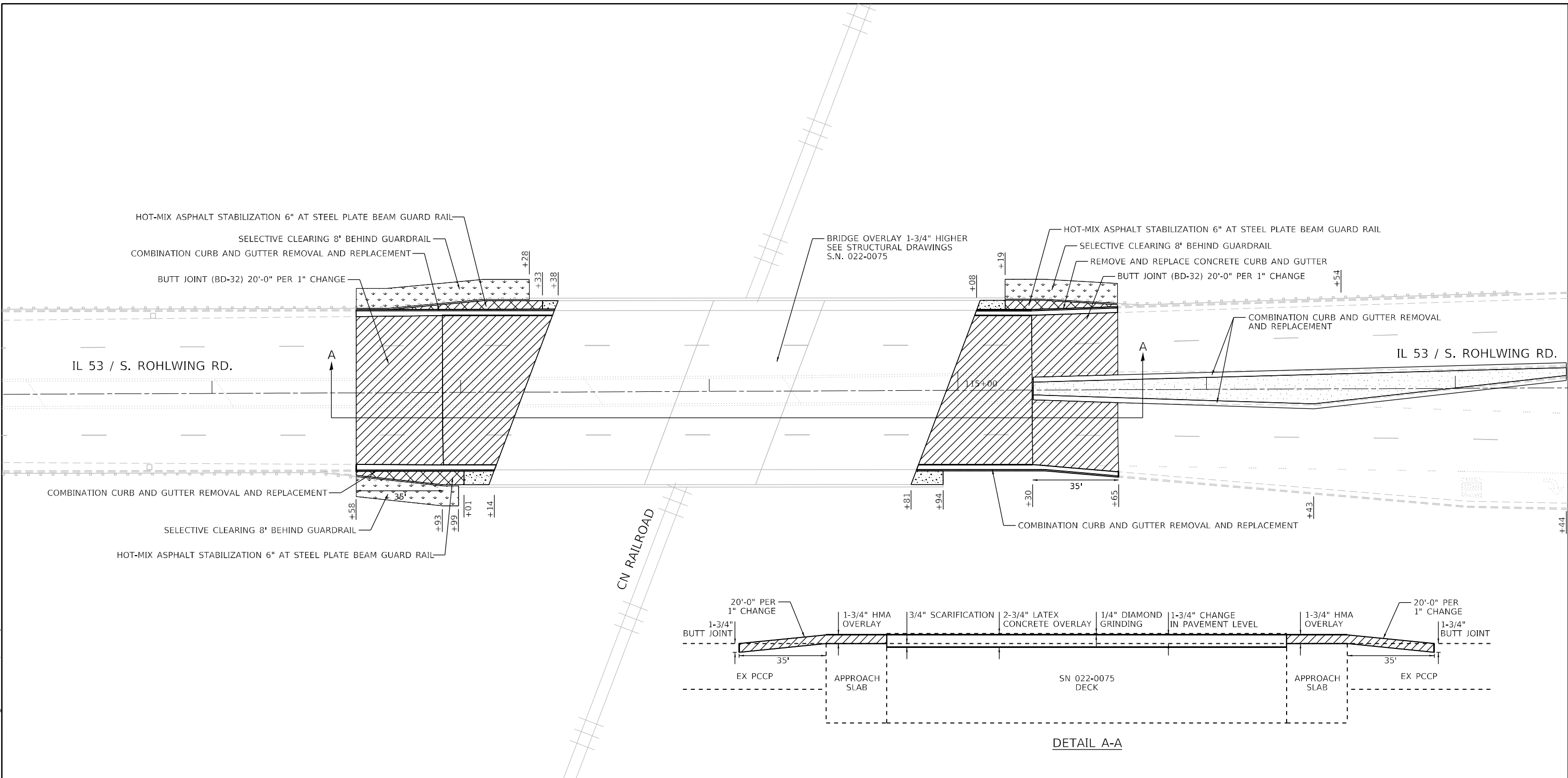
SCALE: 1" = 50'

STA. TO STA.

F.A.P. RTE. 870	SECTION FAP 0870 22 BJ	COUNTY DUPAGE	TOTAL SHEETS 38	SHEET NO. 7
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T30	

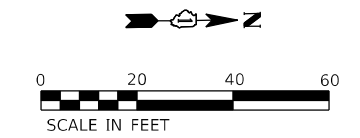
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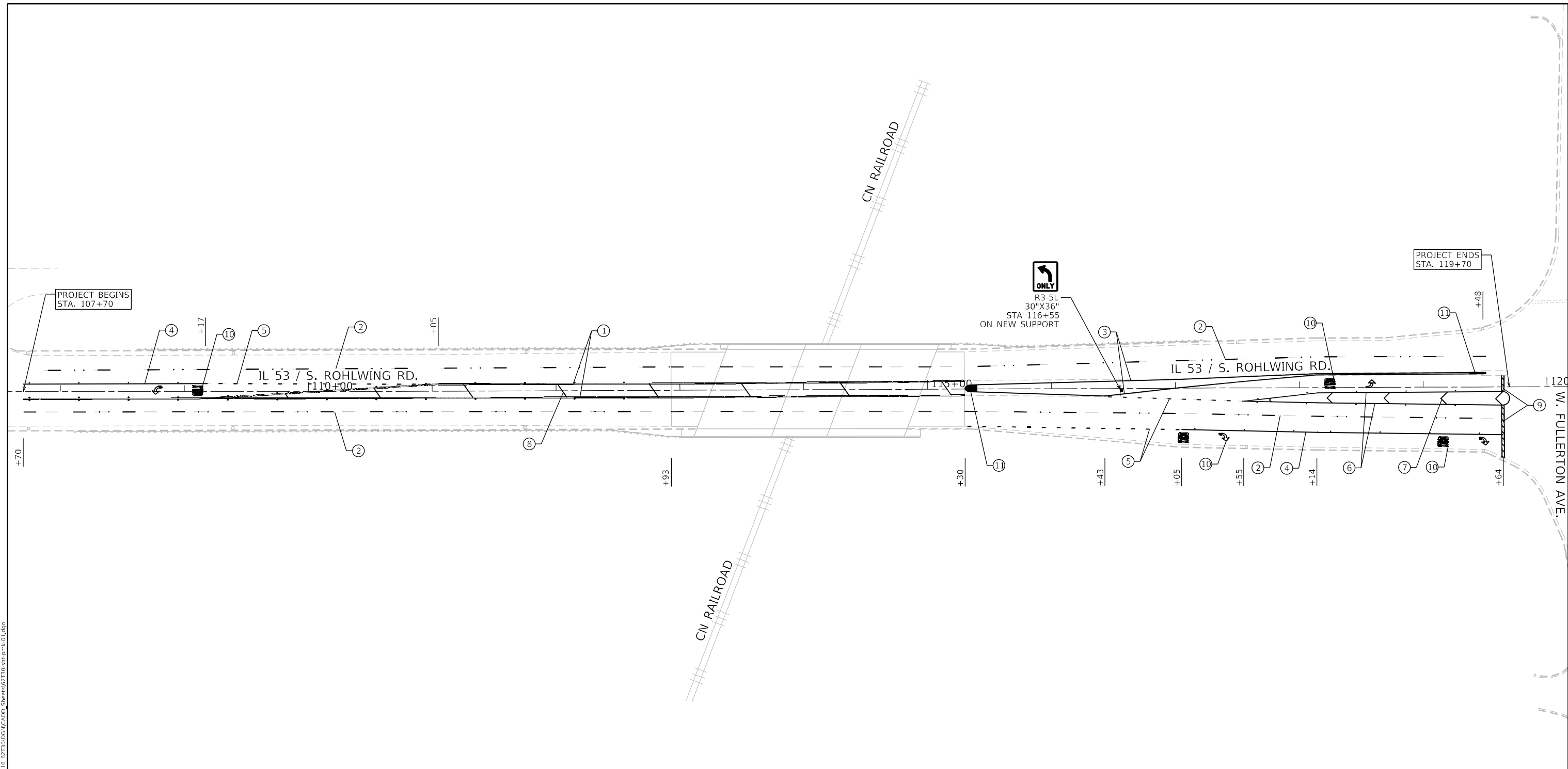


LEGEND

- POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70, 1-3/4"
- HOT-MIX ASPHALT STABILIZATION 6" AT STEEL PLATE BEAM GUARD RAIL
- CONCRETE MEDIAN SURFACE, 4 INCH
- REMOVE AND REPLACE SIDEWALK
- SELECTIVE CLEARING 8' BEHIND GUARDRAIL



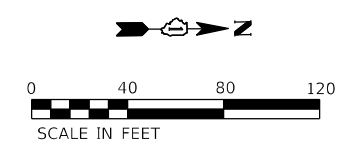
INFRASTRUCTURE ENGINEERING <small>INCORPORATED</small> 1 South Wacker Suite 2650 Chicago, IL 60606 <small>P 312.425.9599 F 312.425.9598 www.infrastructure-eng.com</small>	USER NAME = Alane PLOT SCALE = 40,000' / in.	DESIGNED - CW DRAWN - CW CHECKED - ACL DATE - 08/07/2024	REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN SCALE: 1" = 20' STA. TO STA.	F.A.P. RTE. 870 SECTION FAP 0870 22 BJ COUNTY DUPAGE TOTAL SHEETS 38 SHEET NO. 8 CONTRACT NO. 62T30 ILLINOIS FED. AID PROJECT
	<small>MODEL Path: \\... FILE NAME: \\...</small>					



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PAVEMENT MARKING LEGEND

- | | | | |
|---|---|---|---------------------------------|
| ① | LINE 2 @ 4" SOLID YELLOW, 11" C-C | ⑨ | LINE 24", SOLID, WHITE |
| ② | LINE 7", SKIP-DASH, WHITE (10' LINE - 30' SPACE)
GROOVING FOR RECESSED PAVEMENT MARKING 8" | ⑩ | LETTERS & SYMBOLS, SOLID, WHITE |
| ③ | LINE 4", SOLID, YELLOW - EDGE LINE ON CURB FACE | ⑪ | LETTERS & SYMBOLS, SOLID, WHITE |
| ④ | LINE 6", SOLID, WHITE | | |
| ⑤ | LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) | | |
| ⑥ | LINE 8", SOLID, WHITE | | |
| ⑦ | LINE 12", SOLID, WHITE (SEE DISTRICT DETAIL TC-13 FOR SPACING) | | |
| ⑧ | LINE 12", SOLID, YELLOW (SEE DISTRICT DETAIL TC-13 FOR SPACING) | | |



NOTES

1. MODIFIED URETHANE PAVEMENT MARKINGS SHALL BE USED FOR ALL PAVEMENT MARKINGS, WITH THE EXCEPTION OF SKIP-DASH LANE LINES WHICH SHALL BE PREFORMED PLASTIC.
2. ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
3. ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.
4. ALL RAISED REFLECTIVE PAVEMENT MARKERS WITHIN BRIDGE LIMITS SHALL BE REMOVED AND NOT REPLACED.

	USER NAME = Alane DRAWN - CW CHECKED - ACL DATE - 08/07/2024	DESIGNED - CW DRAWN - CW CHECKED - ACL DATE - 08/07/2024	REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING PLAN	F.A.P. RTE. 870	SECTION FAP 0870 22 BJ	COUNTY DUPAGE	TOTAL SHEETS 38	SHEET NO. 9	
	SCALE: 1" = 40' STA. TO STA.						CONTRACT NO. 62T30		ILLINOIS FED. AID PROJECT		

Existing Structure: SN 022-0075 was built as FAU 2573, Section 533-VB at Sta. 497+52.93 in 1935. The superstructure was replaced in 1985. Superstructures consist of a concrete deck on three span continuous composite welded steel plate girders and reinforced approach pavements. Substructures consist of open stub concrete abutments and concrete piers. Length is 174'-3 3/4" back-to-back abutments. Out-to-out deck width is 76'-0".

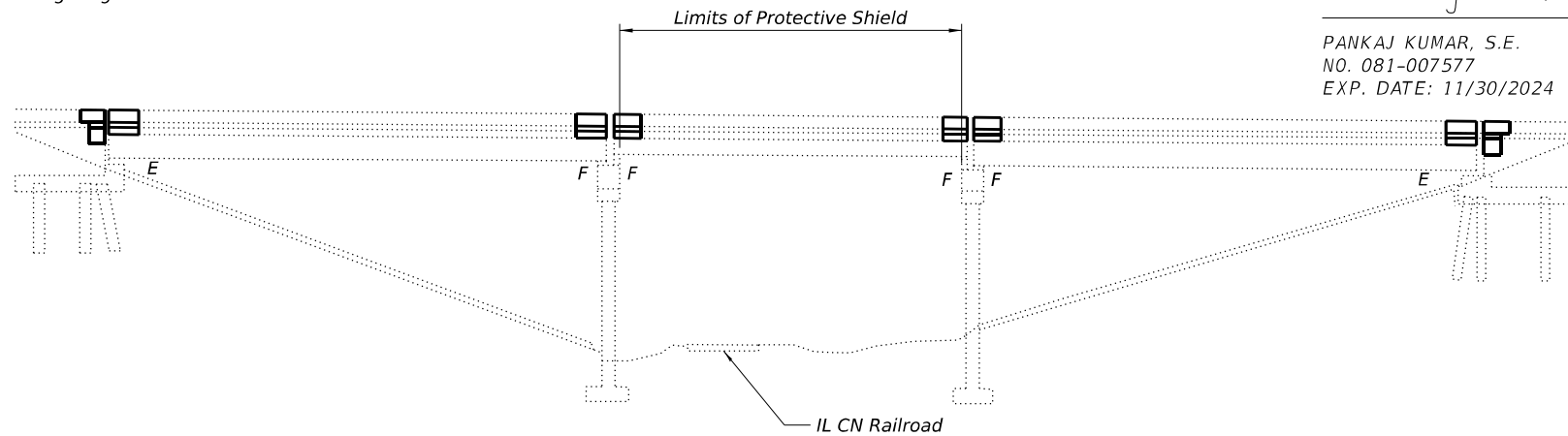
Traffic is to be maintained using Stage Construction.

No Salvage.



Pankaj Kumar

PANKAJ KUMAR, S.E.
NO. 081-007577
EXP. DATE: 11/30/2024



ELEVATION
(Looking West)

INDEX OF SHEETS

1. General Plan and Elevation
2. General Notes, Scope of Work, & Bill of Materials
3. Stage Construction Details
4. Temporary Concrete Barrier For Stage Construction
5. Deck Repair Plan
6. Abutment and Deck Concrete Removal
7. Abutment and Deck Concrete Removal Sections (Sheet 1 of 2)
8. Abutment and Deck Concrete Removal Sections (Sheet 2 of 2)
9. Abutment and Deck Alterations
10. Abutment and Deck Alteration Sections (Sheet 1 of 2)
11. Abutment and Deck Alteration Sections (Sheet 2 of 2)
12. Abutment and Deck Alteration Details
13. Deck Details at Drainage Structures
14. Approach Pavement Repair Plan
15. Preformed Joint Strip Seal - Sidewalk (Sheet 1 of 2)
16. Preformed Joint Strip Seal - Sidewalk (Sheet 2 of 2)
17. Framing Plan and Details
18. Diaphragm Details
19. Abutment Repairs
20. Pier 1 Repairs
21. Pier 2 Repairs
22. Slopewall Repairs & Replacement
23. Bar Splicer Assembly and Mechanical Splicer Details

DESIGN STRESSES

Exist. Construction

$f_c = 3,500$ psi (Superstructure Concrete)
 $f_y = 60,000$ psi (Reinforcement)
 $f_y = 50,000$ psi (Structural Steel)

New Construction

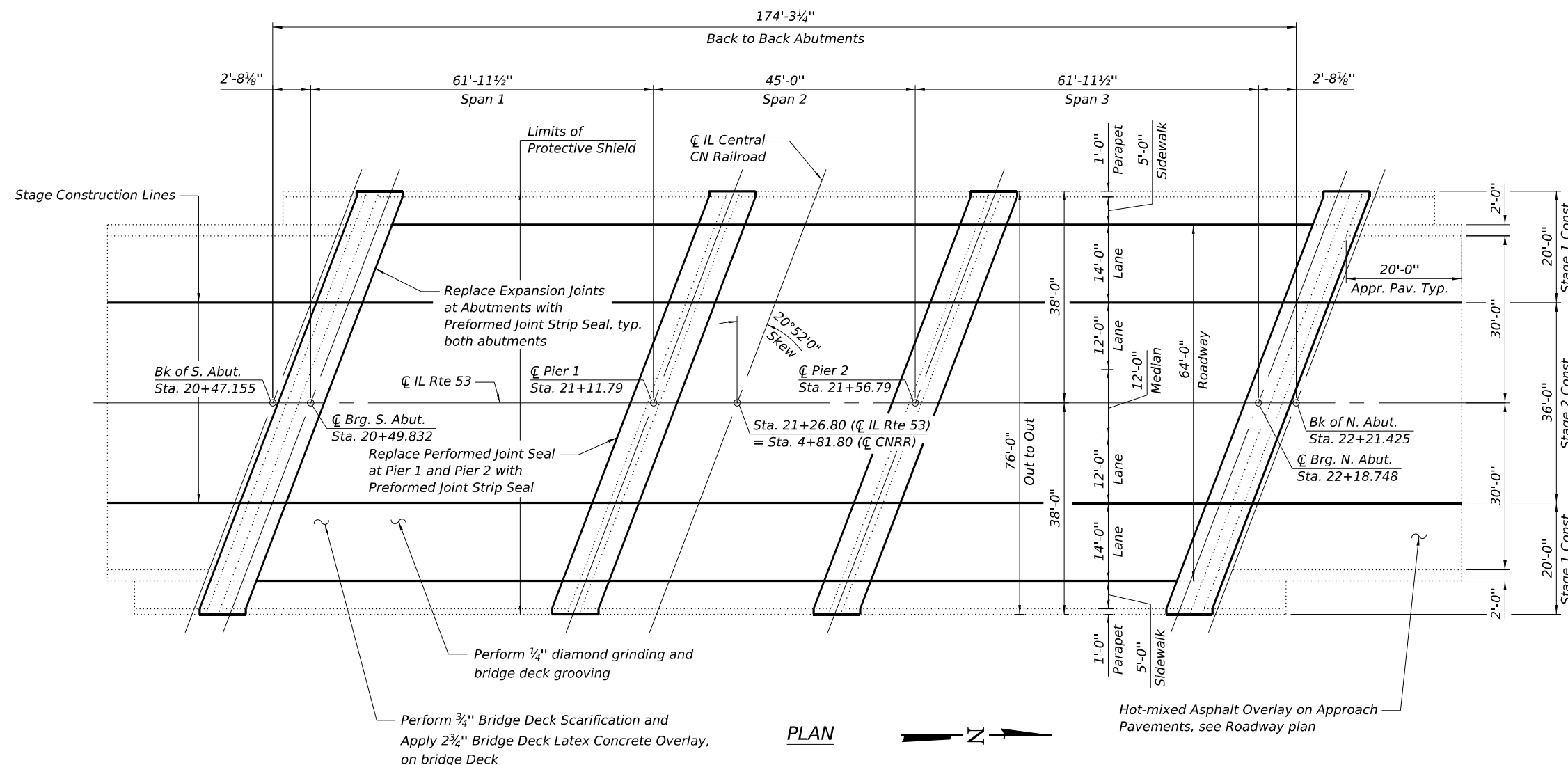
$f_c = 4,000$ psi (Superstructure Concrete)
 $f_y = 60,000$ psi (Reinforcement)

DESIGN SPECIFICATIONS

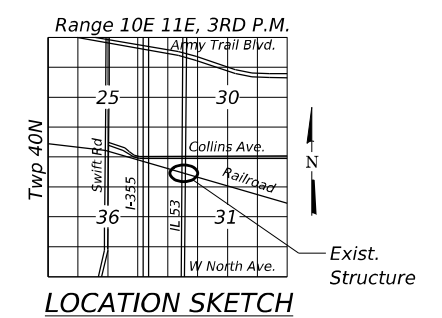
2002 AASHTO Standard Specifications for Highway Bridges (17th Edition)

LOADING HS20-44

No future wearing surface allowed



PLAN



LOCATION SKETCH

**GENERAL PLAN AND ELEVATION
IL ROUTE 53 OVER IL CN RAILROAD
FAP 0870 SECTION FAP 0870 22 BJ
DUPAGE COUNTY
STATION 21+26.80
STRUCTURE NO. 022-0075**

MODEL: Paf410; FILE NAME: P:\2222\2222\4675-00_IDOT_Verbois_Phase 2 (FBE_206-002)\1\16_6373\00\DC\DCADD_Sheets\62730-02-STR-01_C01E.dwg

 INFRASTRUCTURE ENGINEERING 1 South Wacker Suite 2650 Chicago, IL 60606 P: 312.425.9598 F: 312.425.9594 www.infrastructure-eng.com	USER NAME = Alane	DESIGNED - TY	REVISED -
	PLOT SCALE =	CHECKED - PK	REVISED -
	PLOT DATE = 9/27/2024	DRAWN - TY	REVISED -
		CHECKED - PK	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

SHEET 1 OF 23 SHEETS	ILLINOIS FED. AID PROJECT
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ	DUPAGE	38	10
CONTRACT NO. 62T30				

SCOPE OF WORK

- Place protective shield over CN RR tracks.
- Perform 3/4" bridge deck scarification on the bridge deck.
- Perform deck slab and approach pavement repair as required.
- Apply 2 3/4" bridge deck latex concrete overlay on the bridge deck.
- Replace transverse deck expansion joints with new preformed strip seal joints at both abutments.
- Clean and adjust bridge floor drains.
- Perform bridge deck grooving (longitudinal) on the bridge deck.
- Perform 1/4" diamond grinding on the bridge deck.
- Apply protective coat to the reconstructed top and interior surfaces of the parapets, sidewalks, and concrete overlay areas.
- Repair north and south slopewalls.
- Perform structural repair of concrete on the abutments and piers.
- Replace steel diaphragms as required.

TOTAL BILL OF MATERIAL

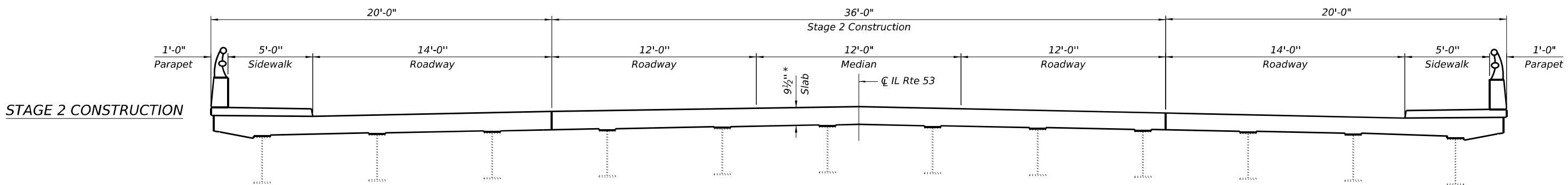
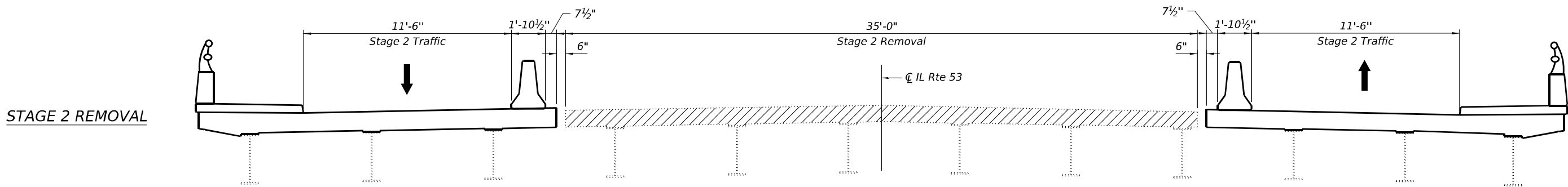
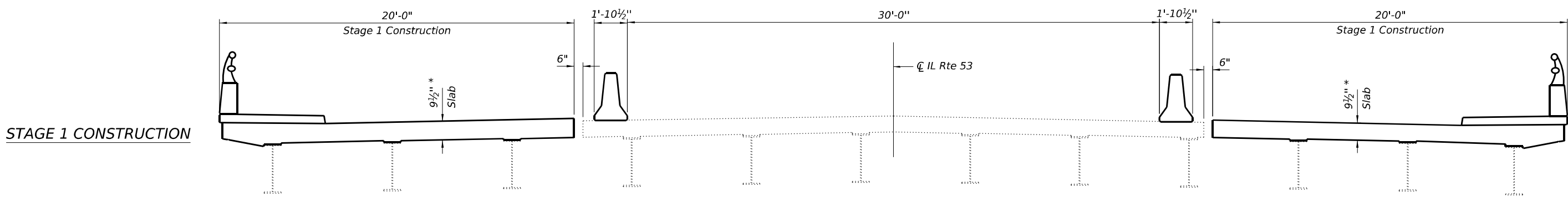
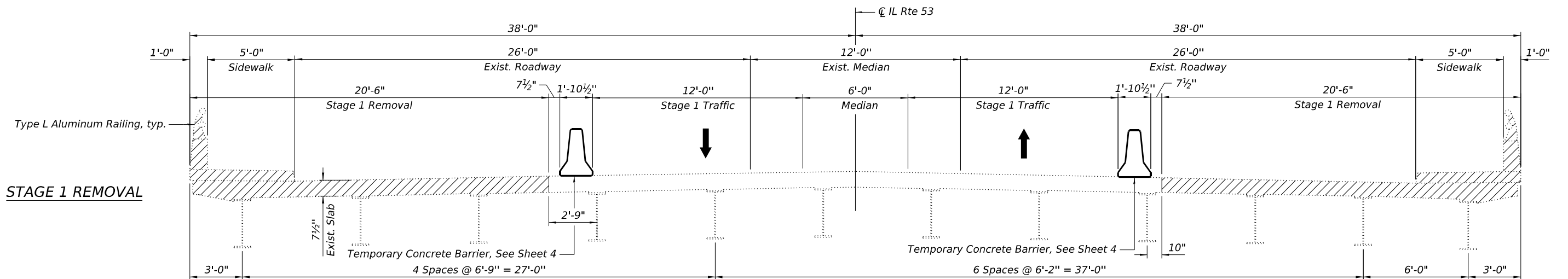
ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu Yd	52.1		52.1
Protective Shield	Sq Yd	380		380
Concrete Superstructure	Cu Yd	59.9		59.9
Protective Coat	Sq Yd	1,258		1,258
Furnishing and Erecting Structural Steel	Pound	8,800		8,800
Reinforcement Bars, Epoxy Coated	Pound	7,070		7,070
Bar Splicers	Each	112		112
Preformed Joint Strip Seal	Foot	318		318
Controlled Low-Strength Material	Cu Yd		15	15
Floor Drains to be Cleaned	Each	12		12
Bridge Deck Grooving (Longitudinal)	Sq Yd	960		960
Approach Slab Repair (Full Depth)	Sq Yd	17		17
Approach Slab Repair (Partial Depth)	Sq Yd	28		28
Structural Steel Removal	Pound	8,800		8,800
Bridge Deck Latex Concrete Overlay, 2 3/4" Inches	Sq Yd	1,115		1,115
Removing and Re-erecting Existing Railing	Foot	78		78
Bridge Deck Scarification, 3/4"	Sq Yd	1,115		1,115
Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)	Sq Ft		396	396
Deck Slab Repair (Full Depth, Type II)	Sq Yd	1		1
Diamond Grinding (Bridge Section)	Sq Yd	1,216		1,216
Railroad Protective Liability Insurance	L Sum		1	1
Slope Wall Repair	Sq Yd		87	87
Temporary Shoring and Cribbing	Each		13	13

* Quantity is for the top of deck and top and interior faces of new concrete for the parapets and sidewalks

GENERAL NOTES

- All structural steel shall conform to AASHTO Classification M270 Grade 36, unless otherwise noted.
- Fasteners shall be ASTM F3125 Grade A325 Type 1. Fasteners shall be hot dip galvanized. See Special Provision for "Hot Dip Galvanizing for Structural Steel." Bolts 3/4 in. diameter, holes 13/16 in. diameter, unless otherwise noted.
- No field welding is permitted except as specified in the contract documents.
- Reinforcement bars designated (E) shall be epoxy coated.
- Plan dimensions and details relative to the existing structure have been taken from existing plans and are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work; however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to address the presence of lead on this project.
- In addition to the requirements of Article 107.16, the contractor shall protect the surface of all bridge decks and bridge approach pavements in a manner satisfactory to the engineer before any equipment is allowed to cross the structure. Protection shall be provided for all equipment as defined in Article 107.16 regardless if track mounted or wheeled.
- Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.
- Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose detrimental foreign material shall be removed from the surfaces in contact with concrete (SSPC-SP3 standards). Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be paid for according to Article 109.04 of the Standard Specifications.
- All new structural steel shall be hot-dip galvanized. See Special Provisions for "Hot Dip Galvanizing for Structural Steel".
- Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures", and the Standard Specifications. Cost included with Furnishing and Erecting Structural Steel.
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened, and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system at the contractor's expense.

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NOTES

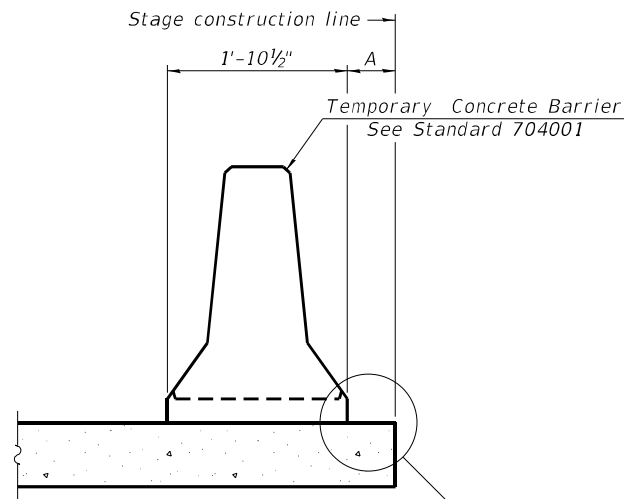
1. Looking north for all sections
2. Sections are shown near the expansion joints.
3. For quantity of Temporary Concrete Barriers, see roadway plans.

* Prior to grinding

LEGEND

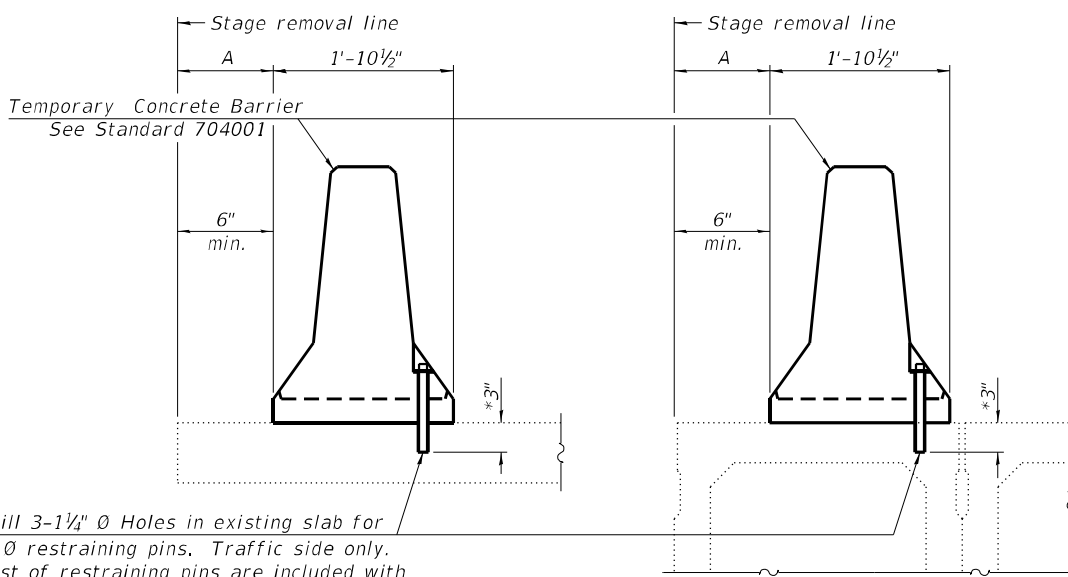
Concrete Removal at joint reconstruction

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When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1". See Detail I, II or III

NEW SLAB OR NEW DECK BEAM



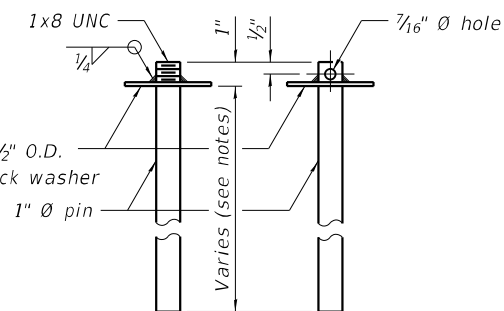
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

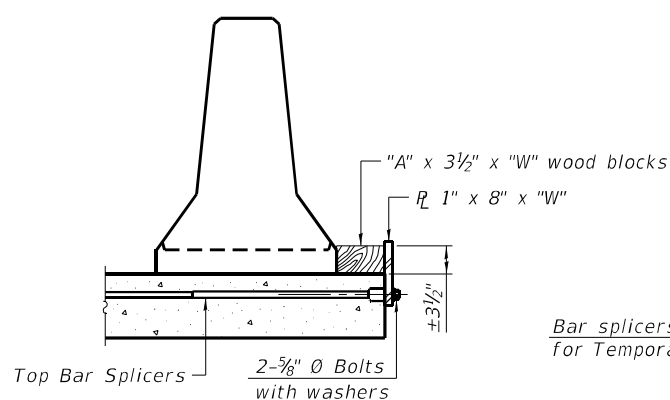
EXISTING DECK BEAM

SECTIONS THRU SLAB OR DECK BEAM

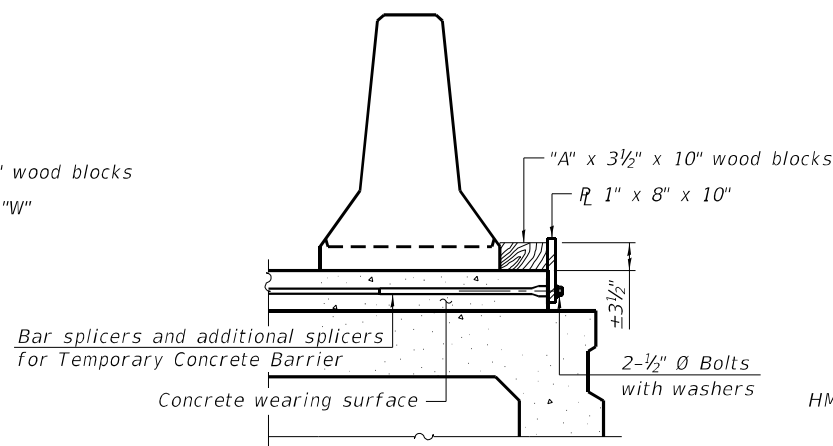


RESTRAINING PIN

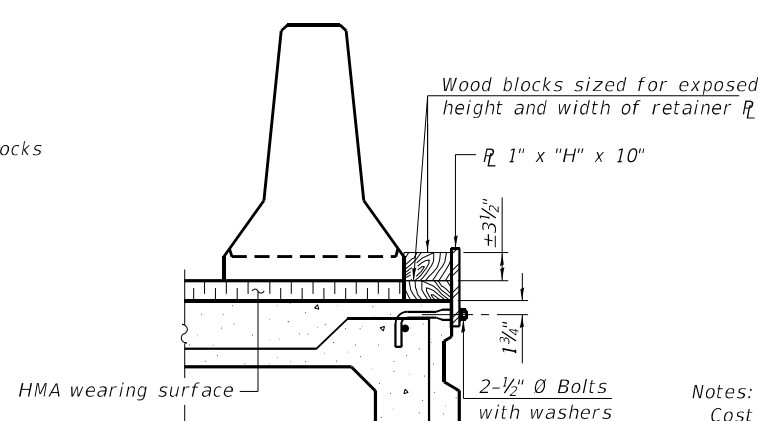
US Std. 1 1/16" I.D. x 2 1/2" O.D. x approx. 8 gauge thick washer



DETAIL I



DETAIL II

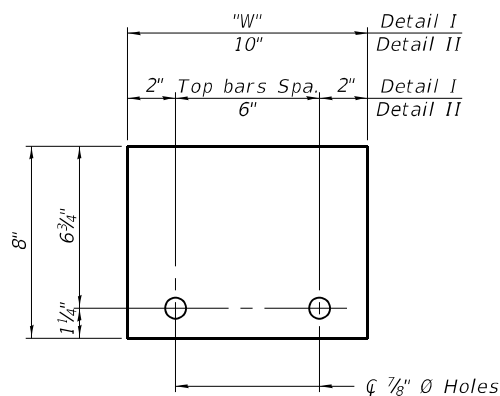


DETAIL III

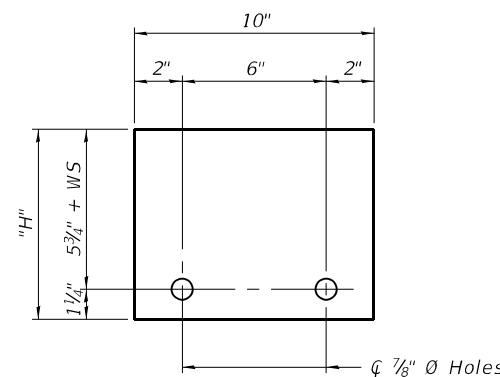
BAR SPLICER FOR #4 BAR - DETAIL III

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate \bar{C} of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.



STEEL RETAINER \bar{R} 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER \bar{R} 1" x "H" x 10"
(Detail III)

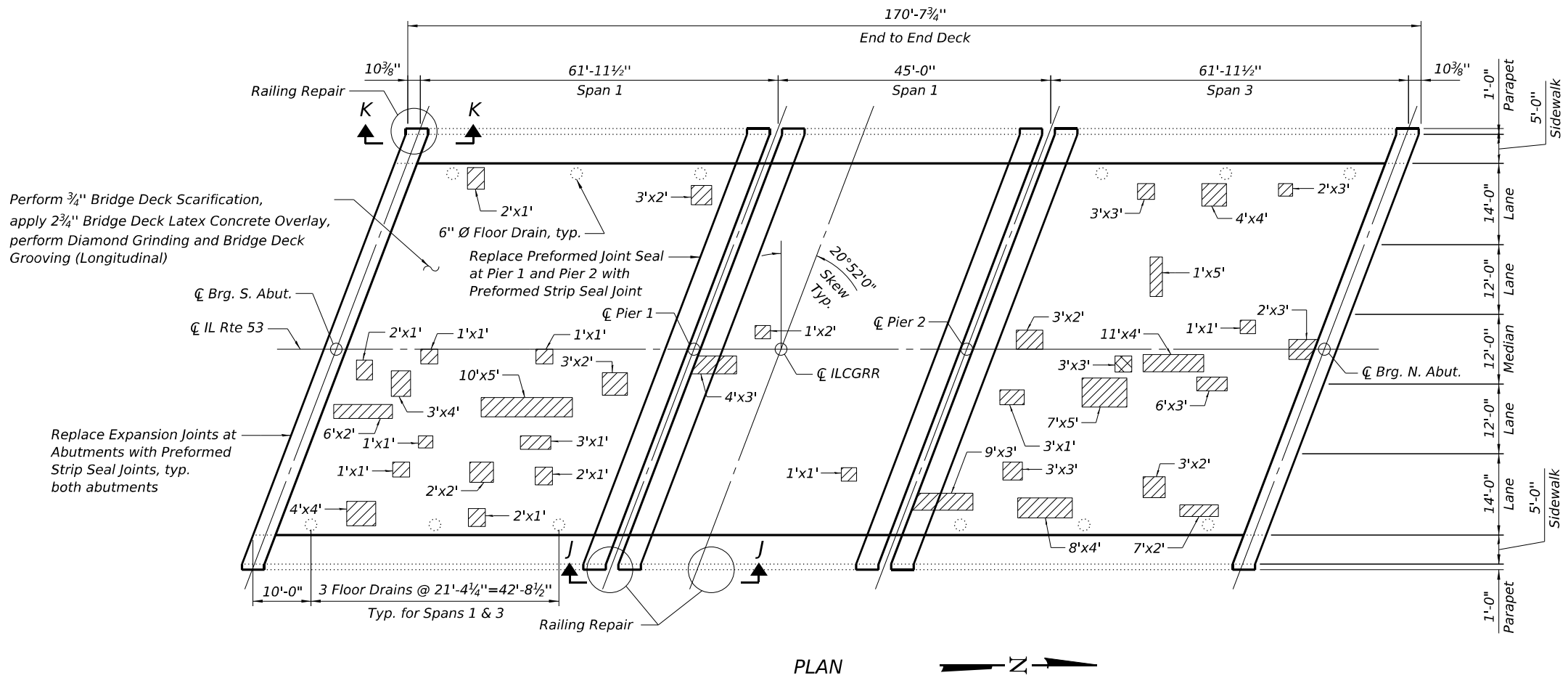
RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

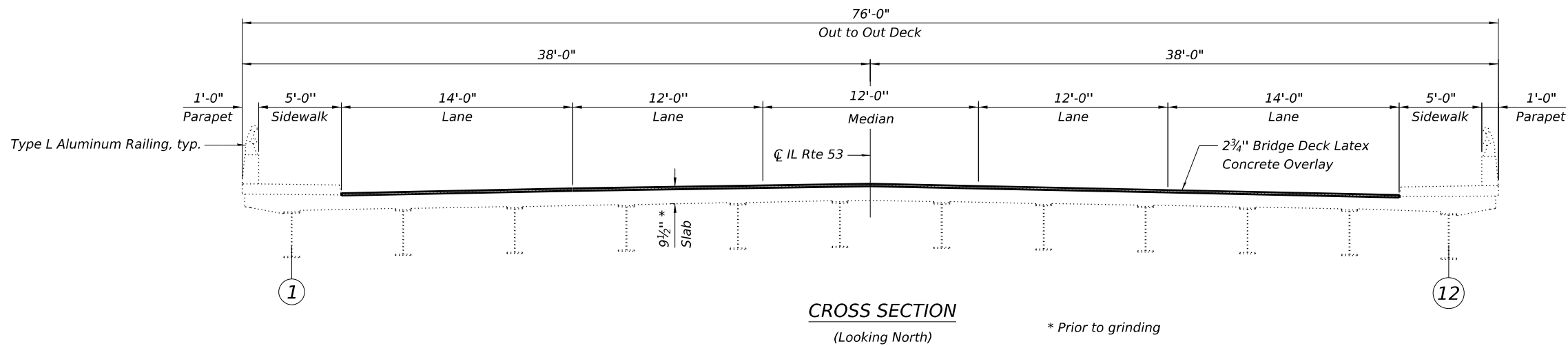
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10-12-2021

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PLAN



NOTES

1. Areas of Deck Slab Repair (Partial Depth) are provide for information only and shall be included in the cost of Bridge Deck Latex Concrete Overlay, $2\frac{3}{4}$ ".
2. Protective Coat shall be applied to the new concrete and overlay areas.
3. For Sections J-J & K-K, Rail Splice details, and bill of material, see Sheet 12 of 23.
4. For Floor Drain Adjustment Details, see Sheet 13 of 23.

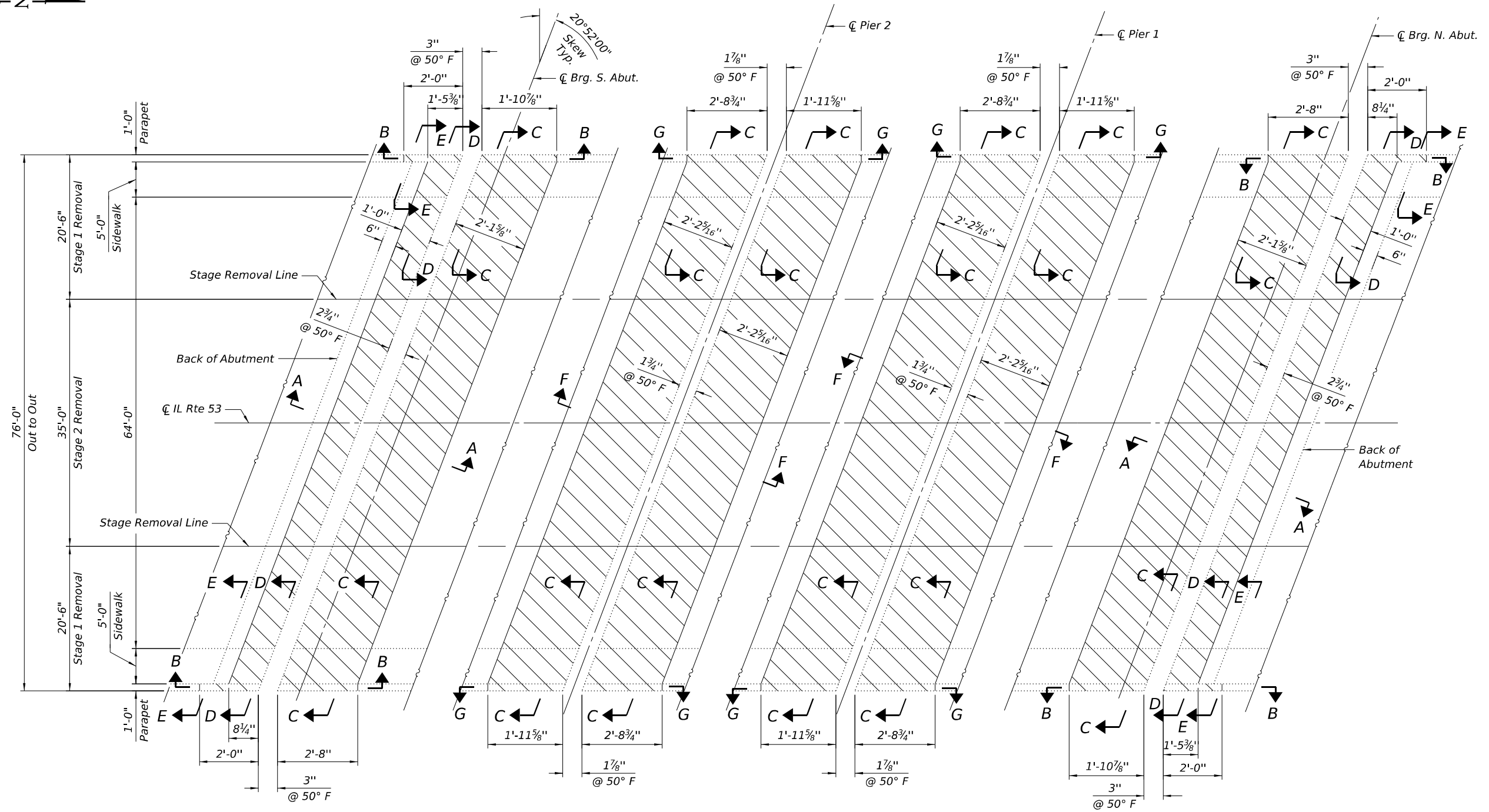
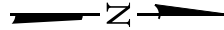
BILL OF MATERIAL

ITEM	UNIT	TOTAL
Protective Coat	Sq Yd	1,258
Bridge Deck Grooving (Longitudinal)	Sq Yd	960
Bridge Deck Latex Concrete Overlay, $2\frac{3}{4}$ "	Sq Yd	1,115
Bridge Deck Scarification, $\frac{3}{4}$ "	Sq Yd	1,115
Deck Slab Repair (Full Depth, Type II)	Sq Yd	1

LEGEND

- Deck Slab Repair (Partial Depth)
- Deck Slab Repair (Full Depth)

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PLAN

NOTE

1. For Sections A-A thru D-D, see Sheet 7 of 23.
2. For Sections E-E thru G-G, see Sheet 8 of 23.

LEGEND



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 PLOT DATE: 8/7/2024

INFRASTRUCTURE ENGINEERING
 1 South Wacker | Suite 2650 | Chicago, IL 60606
 P 312.425.9598 | F 312.425.9594 | www.infrastructure-eng.com

USER NAME = Alane	DESIGNED - TY	REVISD -
PLOT SCALE =	CHECKED - PK	REVISD -
PLOT DATE = 8/7/2024	DRAWN - TY	REVISD -
	CHECKED - PK	REVISD -

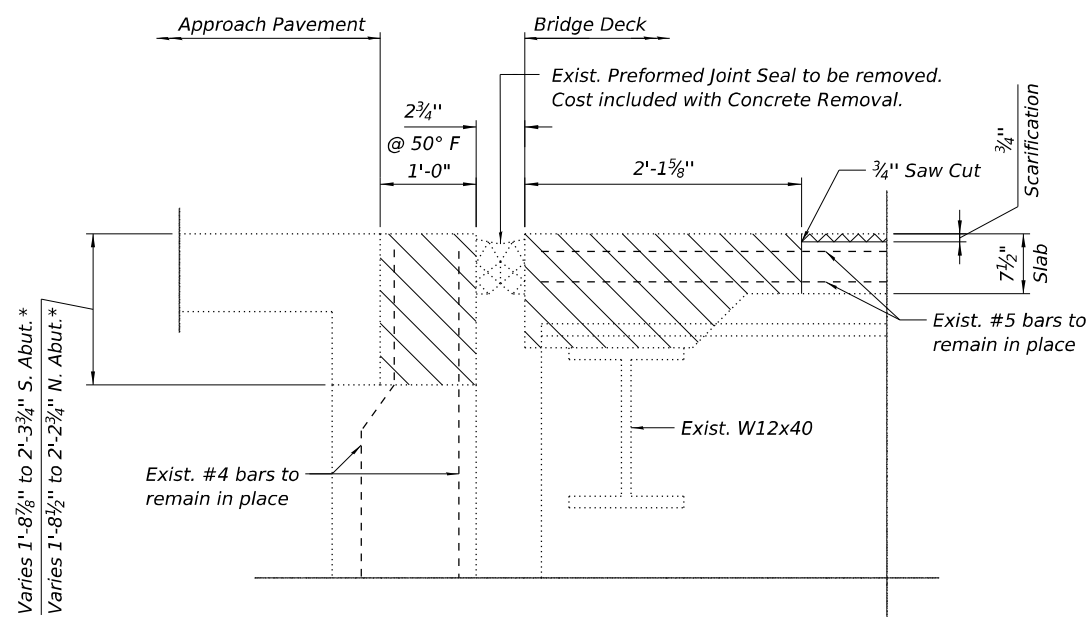
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ABUTMENT AND DECK CONCRETE REMOVAL
 STRUCTURE NO. 022-0075**

SHEET 6 OF 23 SHEETS

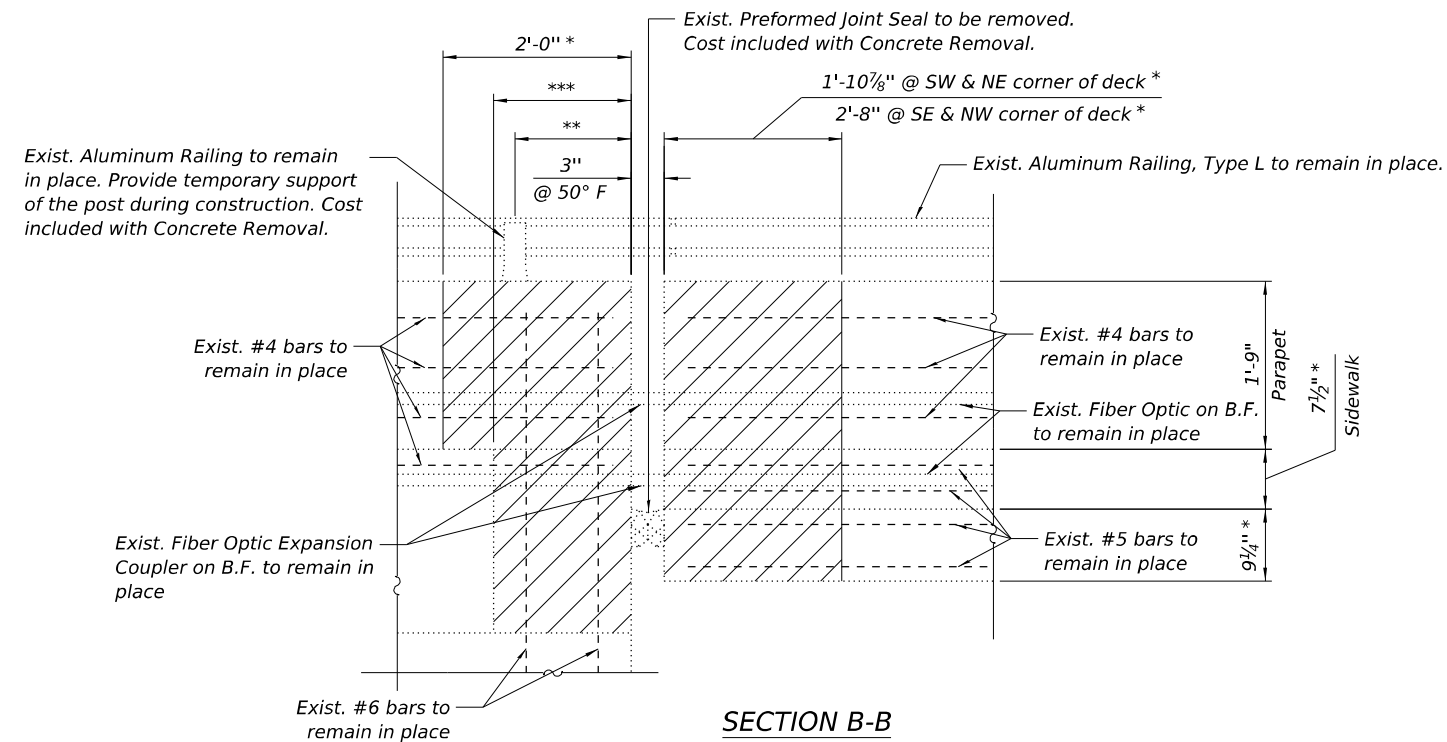
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ	DUPAGE	38	15
CONTRACT NO. 62T30				

ILLINOIS FED. AID PROJECT



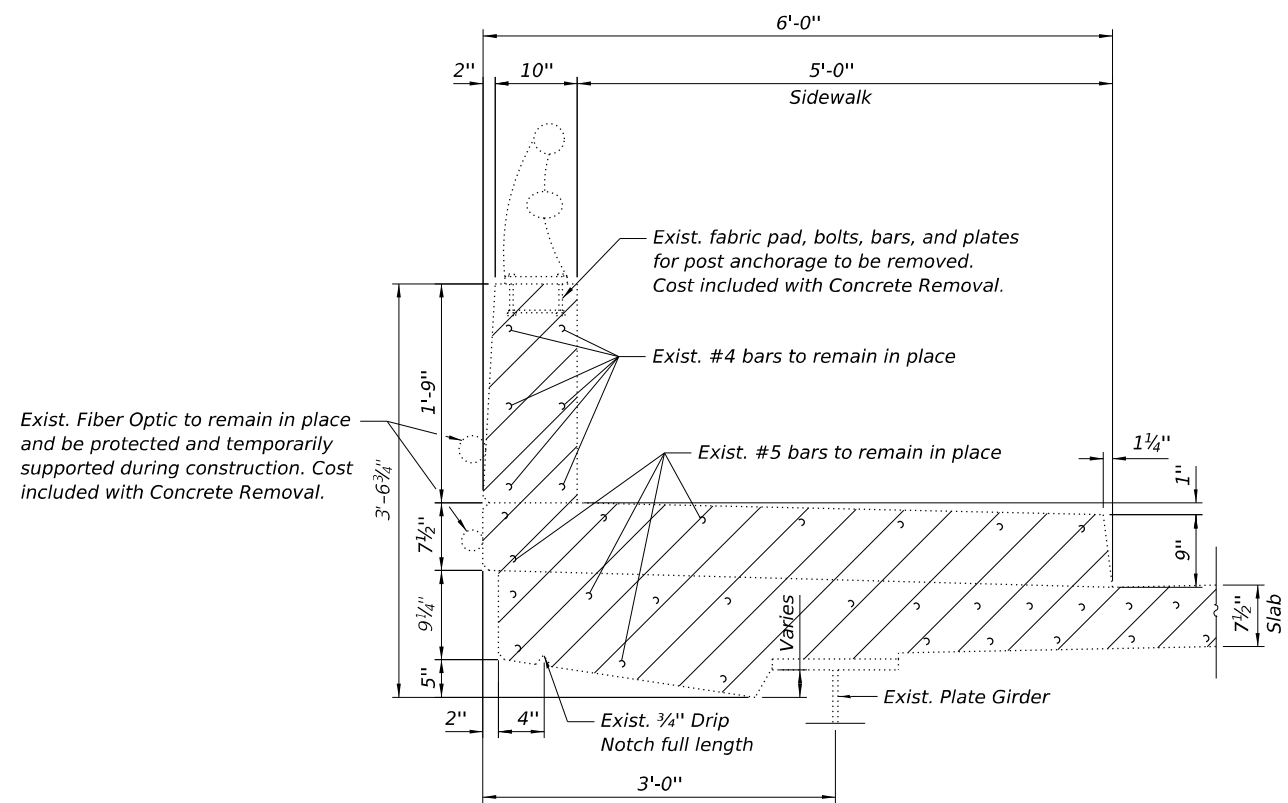
SECTION A-A

* Dimensions measured along the back face of parapets



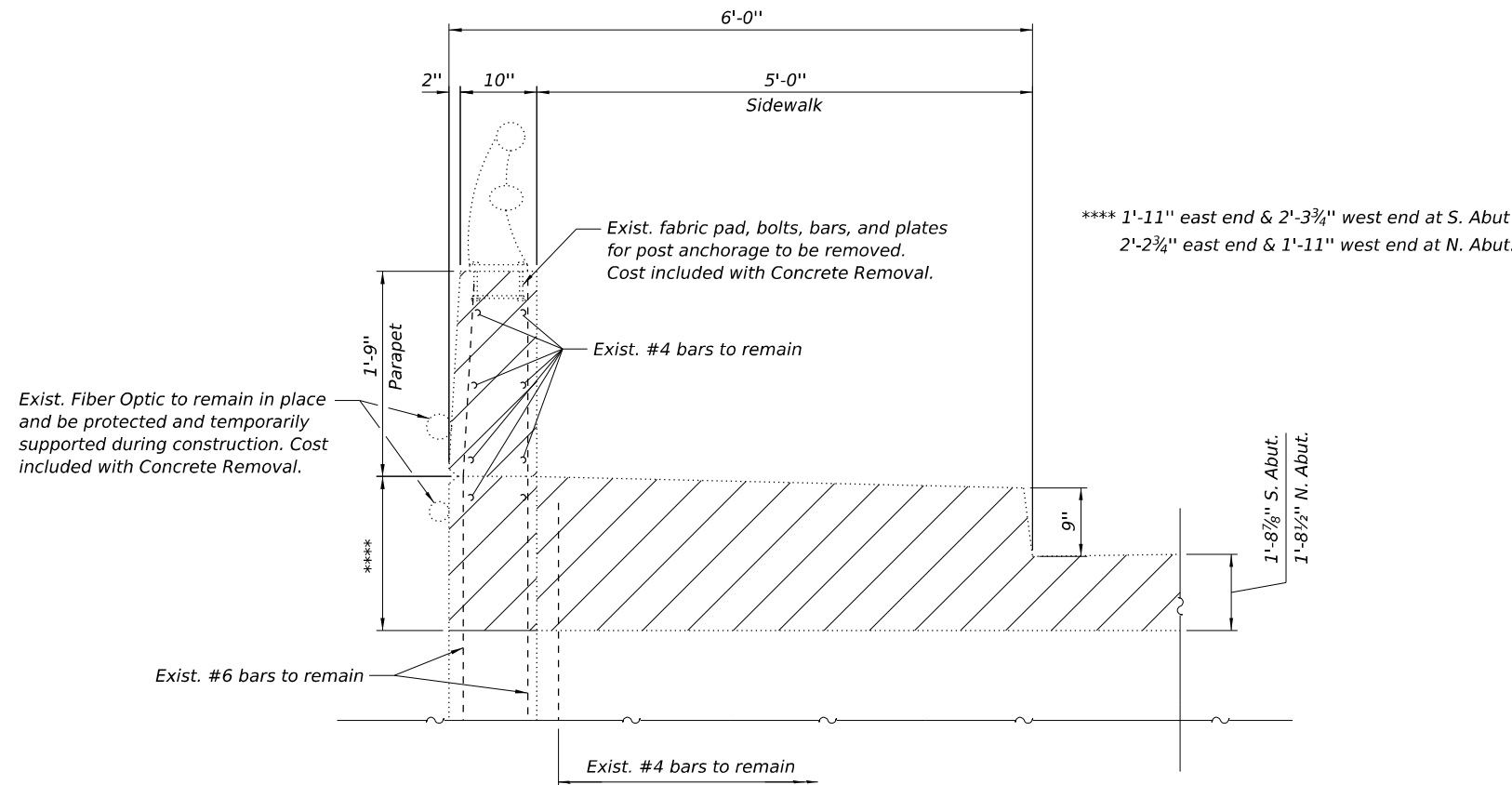
SECTION B-B

* Dimensions measured along the back face of parapets
 ** 1'-6³/₈" @ west wing wall. 9⁷/₈" @ east wing wall
 *** 1'-5³/₈" @ west end S. Abut. & east end N. Abut.
 8¹/₄" @ east end S. Abut. & west end N. Abut.



SECTION C-C

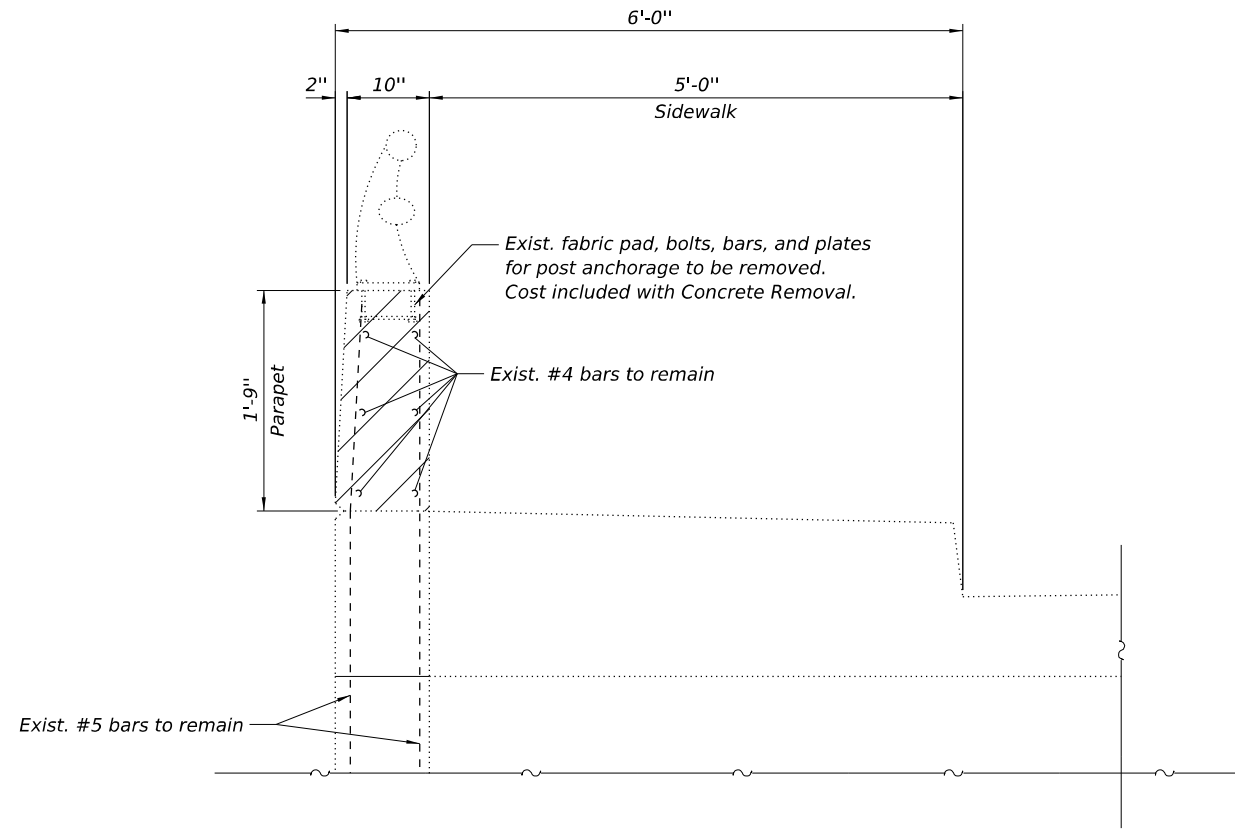
(Dimensions are at right angle to \perp roadway)



SECTION D-D

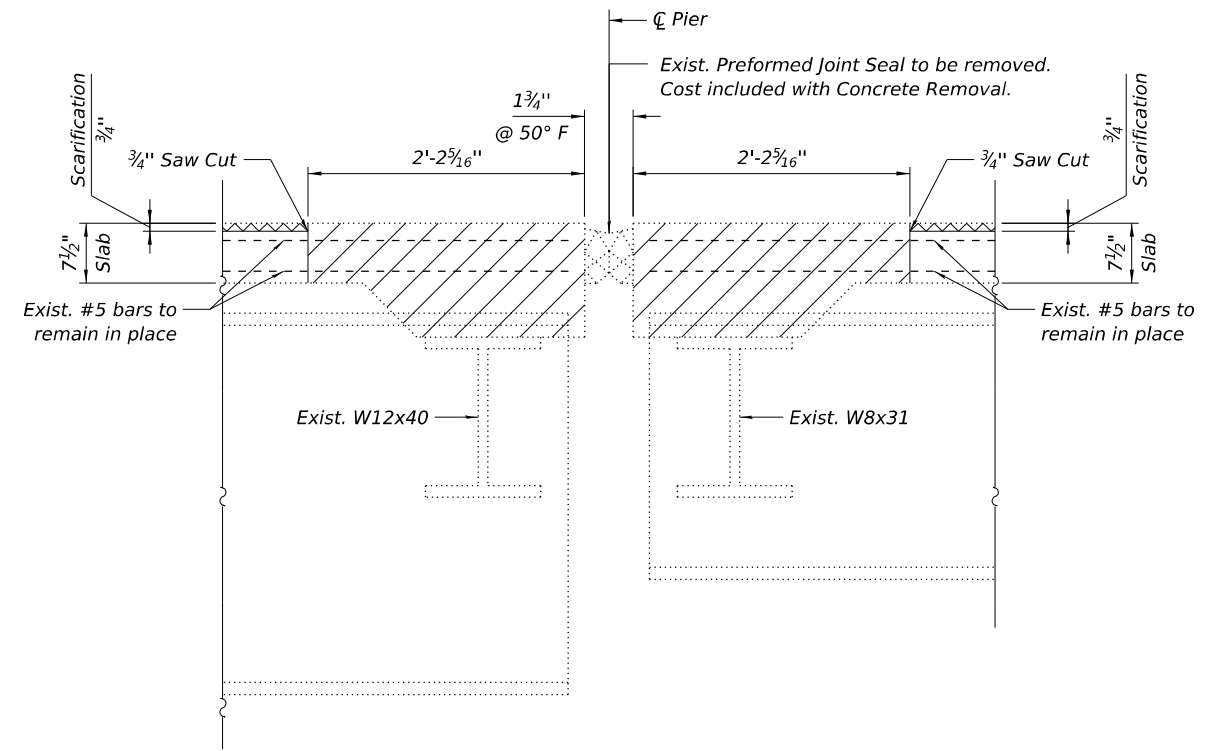
(Dimensions are at right angle to \perp roadway)

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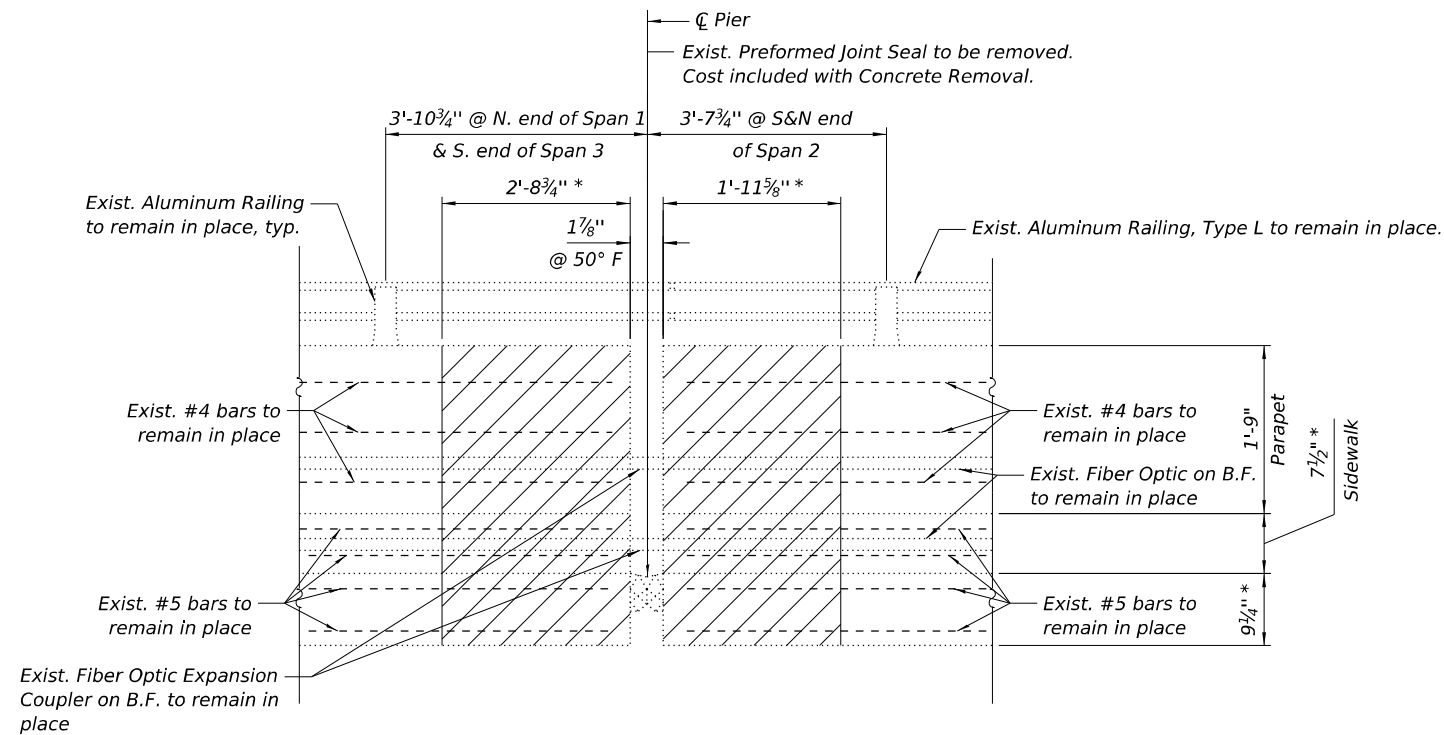


SECTION E-E

(Dimensions are at right angle to \varnothing roadway)



SECTION F-F



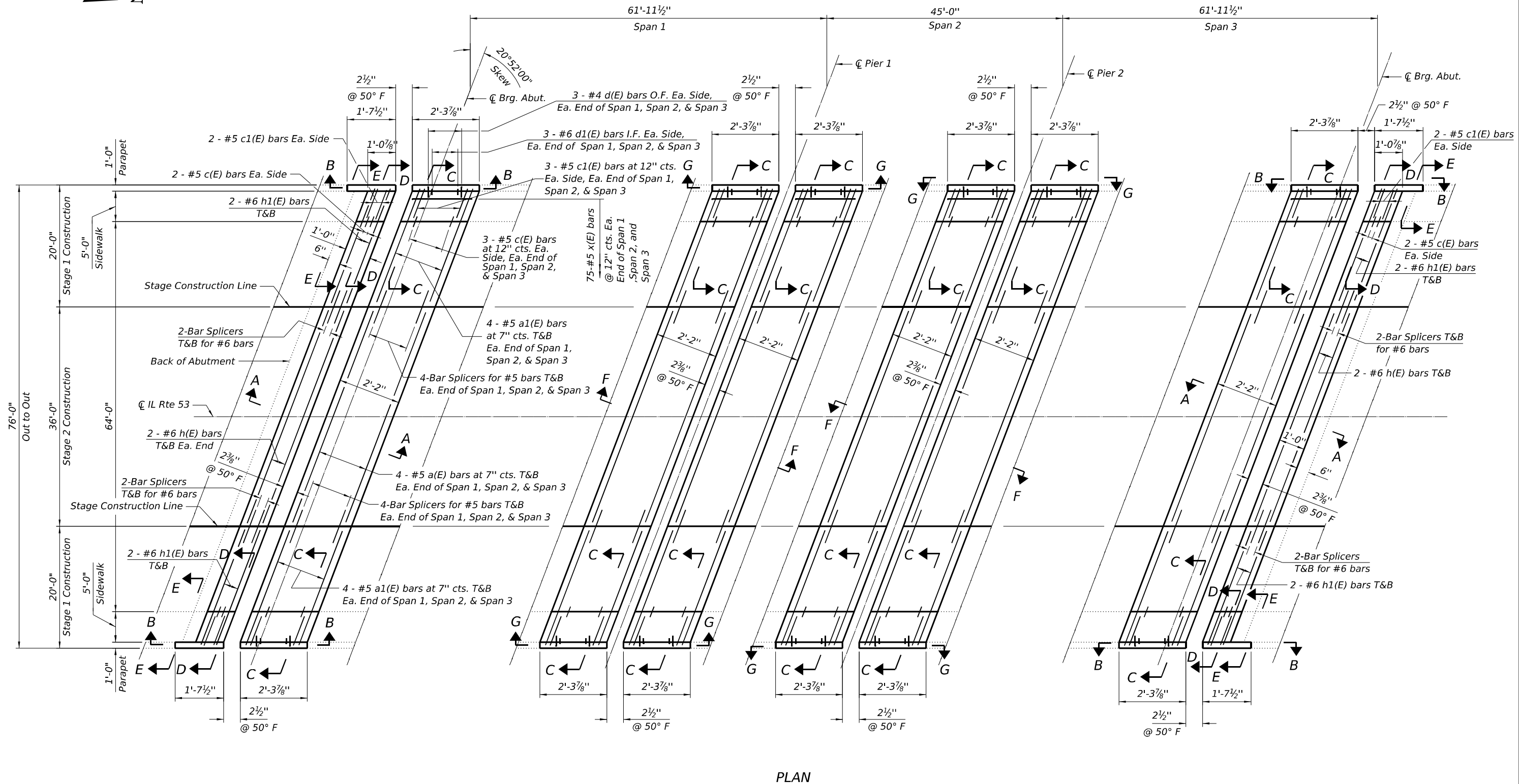
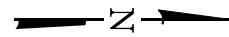
SECTION G-G

* Dimensions measured along the back face of parapets

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Concrete Removal	Cu Yd	52.1

MODEL Path:\01 - INFRA - 206-002\1\WO 16 63T300DCNCADD_Sheets\62T30-AbutRemB.dgn
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PLAN

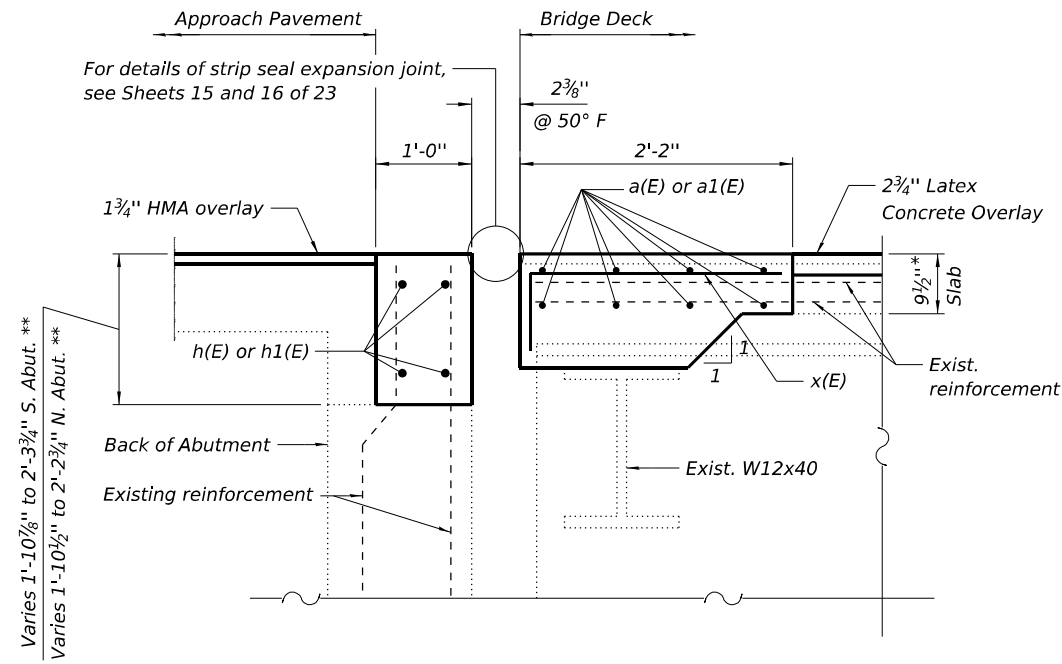
NOTE

1. For Sections A-A thru D-D, see Sheet 10 of 23.
2. For Sections E-E thru G-G, see Sheet 11 of 23.
3. For details and bill of material, see Sheet 12 of 23.

Ea. = Each
 O.F. = Outside Face
 I.F. = Inside Face
 T&B = Top and Bottom

MODEL Path: \\...
 FILE NAME: P:\222222\DOT_Various Phase 2 (FBI_206-002)\WO_16_63730\DCN\CADD_Sheets\62T30-Abt-STR-08_Deck&Abt.dgn
 1 South Wacker | Suite 2650 | Chicago, IL 60606
 P 312.425.6500 | F 312.425.5594 | www.infrastructure-eng.com

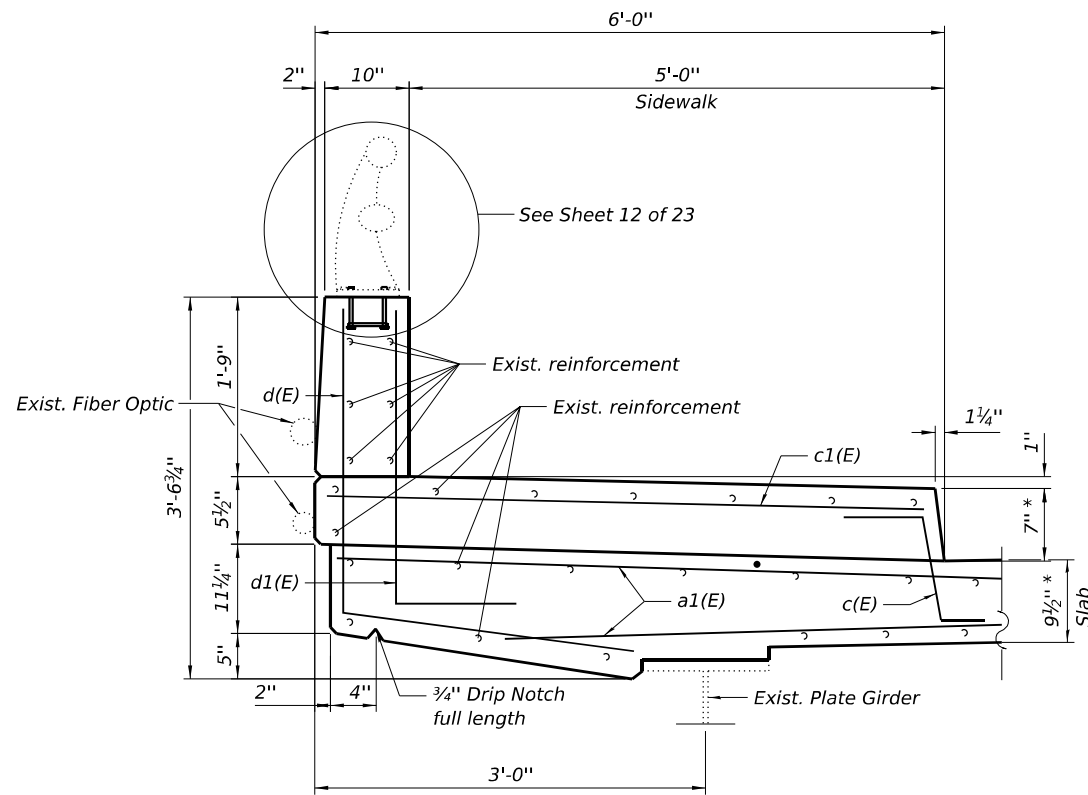
INFRASTRUCTURE ENGINEERING 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.6500 F 312.425.5594 www.infrastructure-eng.com	USER NAME = Alane	DESIGNED - TY	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ABUTMENT AND DECK CONCRETE ALTERATIONS STRUCTURE NO. 022-0075	F.A.P. RTE. = 870	SECTION = FAP 0870 22 BJ	COUNTY = DUPAGE	TOTAL SHEETS = 38	SHEET NO. = 18
	PLOT SCALE =	DRAWN - TY	REVISED -			CONTRACT NO. 62T30				
	PLOT DATE = 9/27/2024	CHECKED - PK	REVISED -			ILLINOIS FED. AID PROJECT				



SECTION A-A

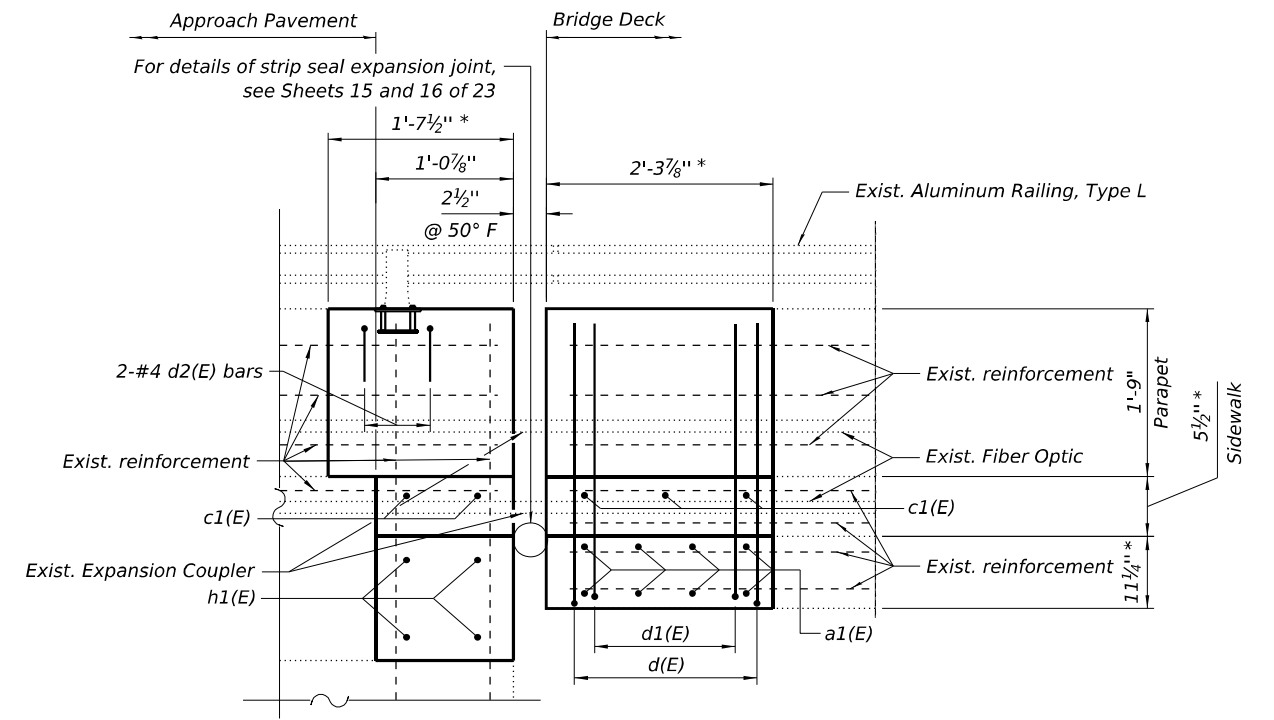
Varies 1'-10 7/8" to 2'-3 3/4" S. Abut. **
Varies 1'-10 1/2" to 2'-2 3/4" N. Abut. **

* Prior to grinding
** Dimensions measured along the front face of abutments & prior to grinding



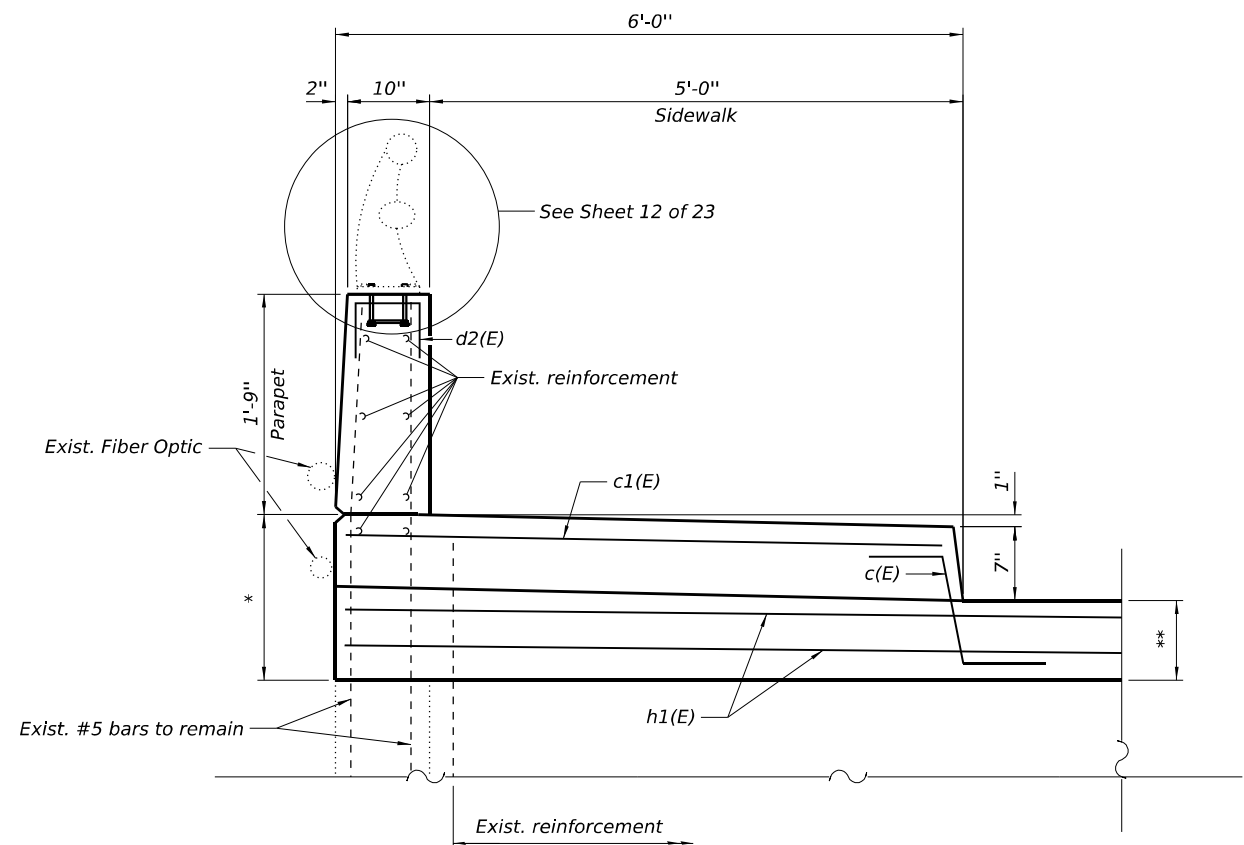
SECTION C-C

(Dimensions are at right angle to \bar{C} roadway)
* Prior to grinding



SECTION B-B

* Dimensions measured along the front face of abutments



SECTION D-D

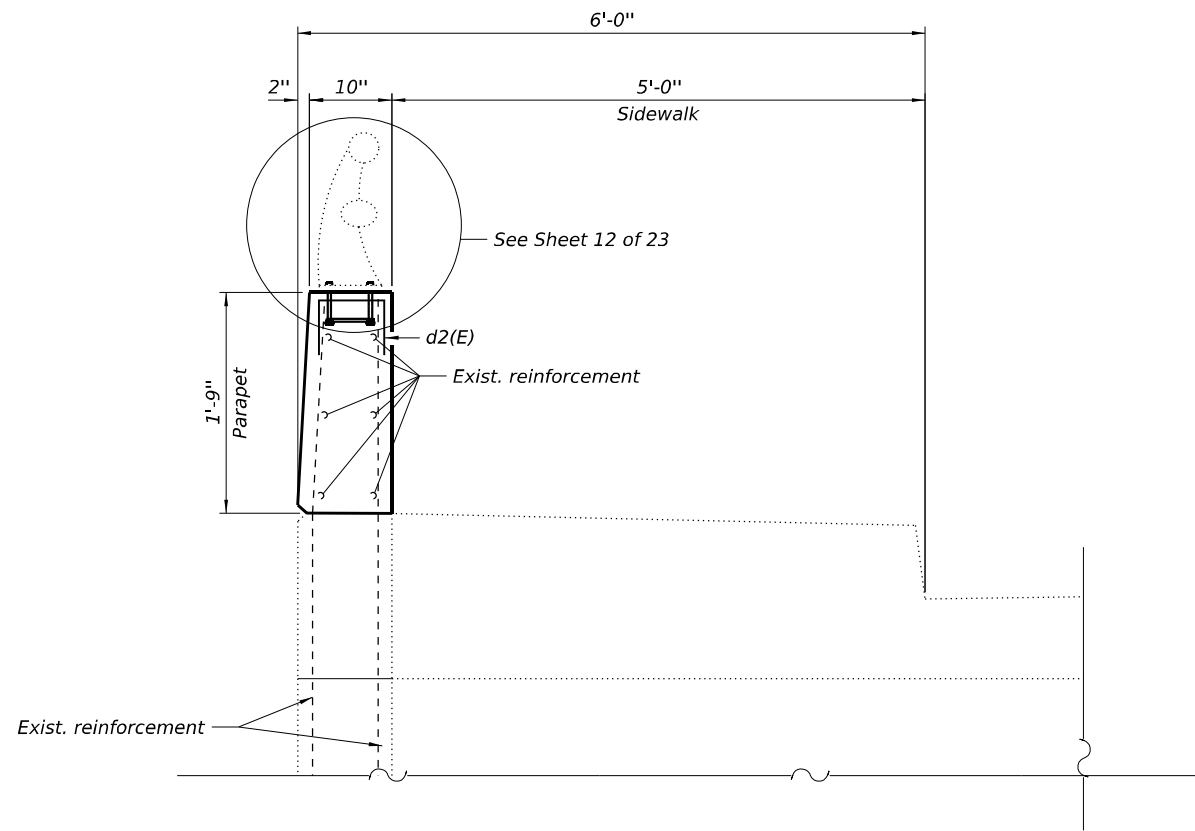
(Dimensions are at right angle to \bar{C} roadway)

* 1'-11" east end & 2'-3 3/4" west end of S. Abut
2'-2 3/4" east end & 1'-11" west end of N. Abut.
** 1'-10 7/8" at S. Abut. & 1'-10 1/2" of N. Abut
before grinding

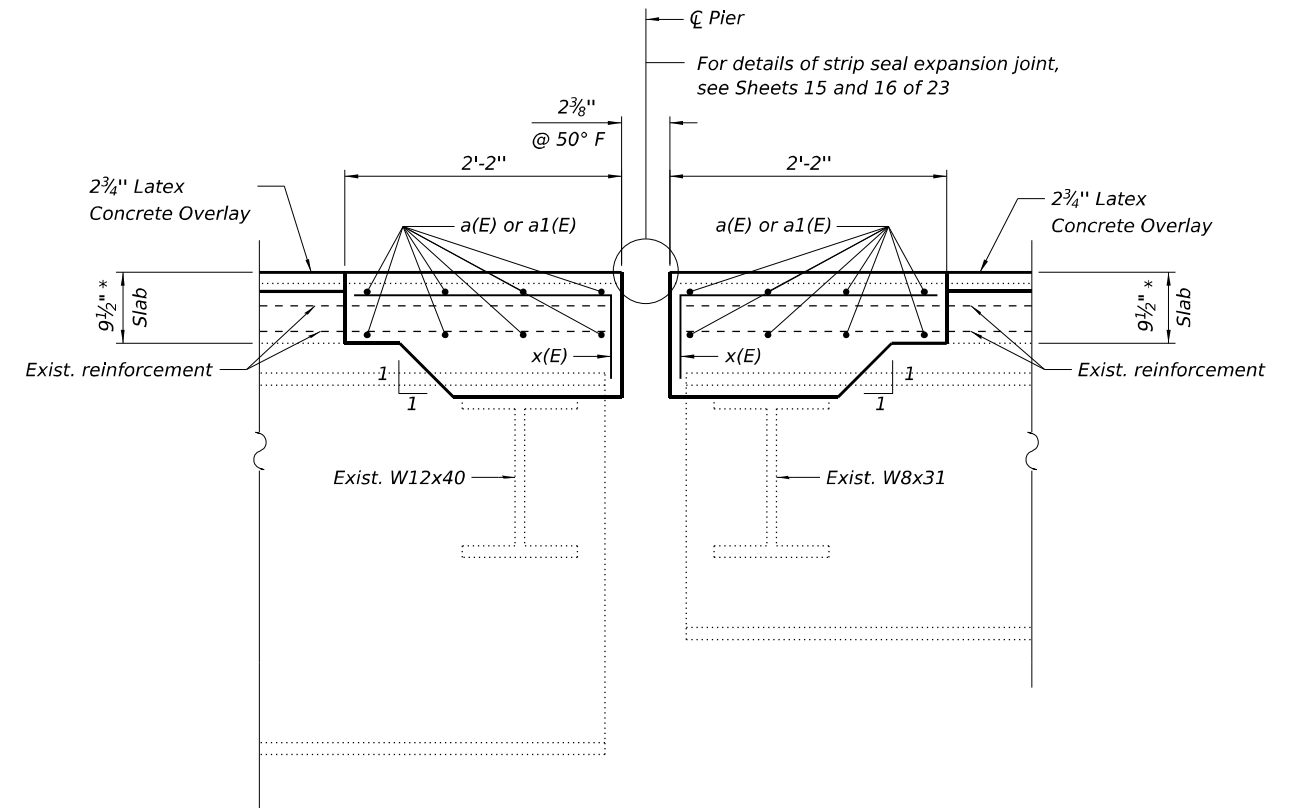
MODEL Pathfull
FILE NAME P:\222222\222222\DOT\Various Phase 2 (FTR 206-002)\16 63T30DCNCARD_Sheets\62T30-Abt-STR-09_RenSection.dgn

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CHECKED - PK	CHECKED - PK	REVISED -
PLOT SCALE =	DRAWN - TY	REVISED -
PLOT DATE = 9/27/2024	CHECKED - PK	REVISED -

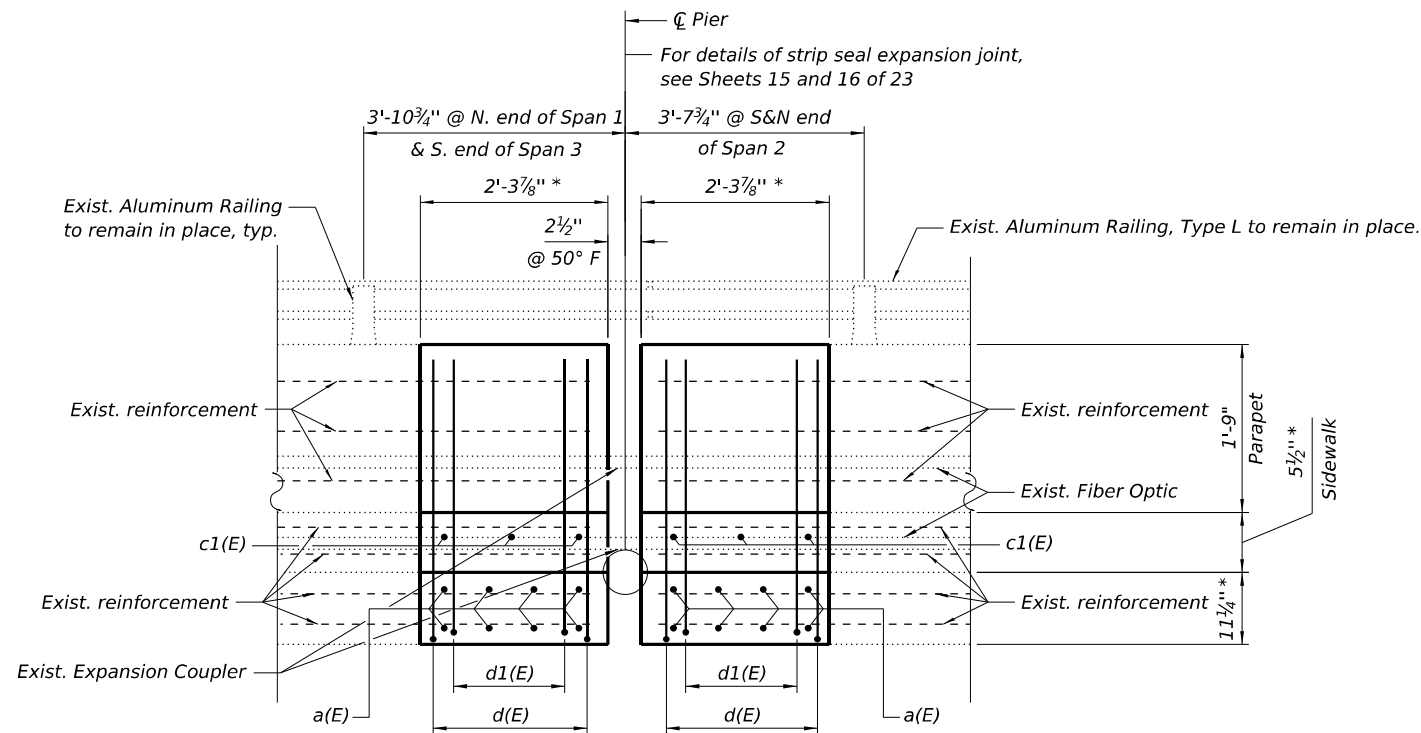
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870	FAP 0870 22 BJ	DUPAGE	38	19
CONTRACT NO. 62T30			ILLINOIS FED. AID PROJECT	



SECTION E-E
(Dimensions are at right angle to ϕ roadway)

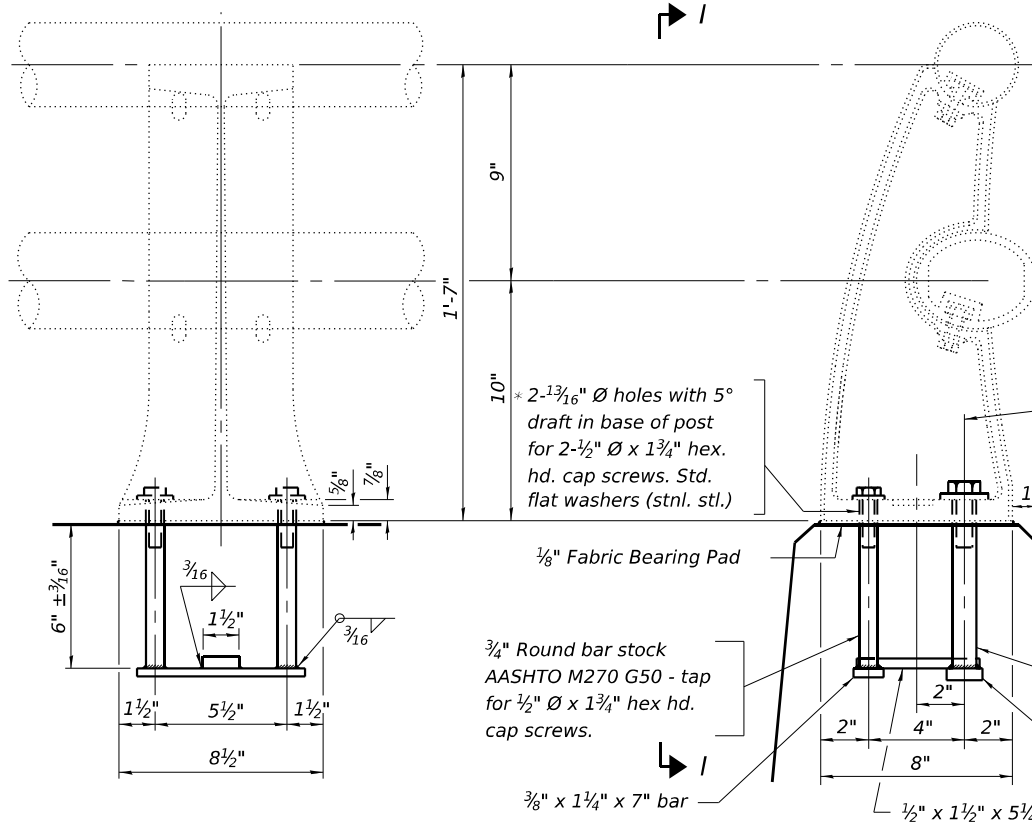


SECTION F-F
* Prior to grinding



SECTION G-G
* Dimensions measured along the back face of parapets

MODEL Path: \\...
FILE NAME: P:\022322-4675-00_IDOT_Various_Phase_2 (ITB_206-002)\WO_16_63T30IDCN\CADD_Sheets\62T30-21t-5T1-10A-Rep\SectionA.dgn



Note: Cost of furnishing and installing the rail post anchor device and fabric bearing pad is included with Concrete Superstructure.

* In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting stainless steel anchor rods of the same diameter and grade as the specified cap screws according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.

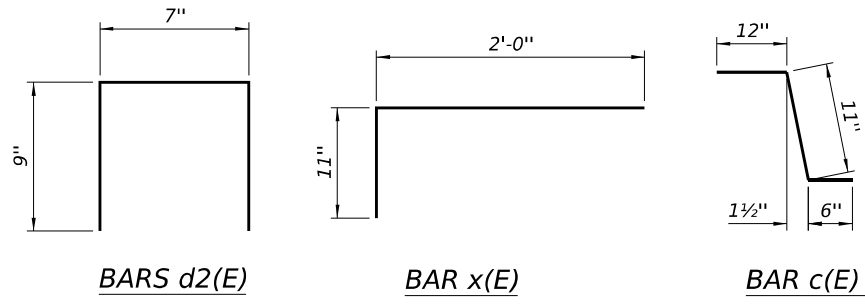
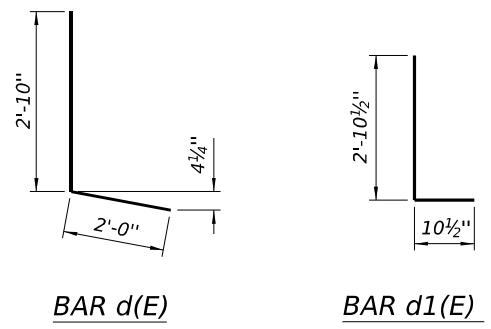
10" x 2-1 3/16" Ø holes with 5° draft in base of post for 2-1/2" Ø x 1 3/4" hex. hd. cap screws. Std. flat washers (stnl. stl.)

3/4" Round bar stock AASHTO M270 G50 - tap for 1/2" Ø x 1 3/4" hex hd. cap screws.

1 1/4" Round bar stock AASHTO M270 G50 Tap for 3/4" Ø x 2" hex hd. cap screws.

1/2" x 1 1/2" x 7" bar

1/2" x 1 1/2" x 5 1/4" bar



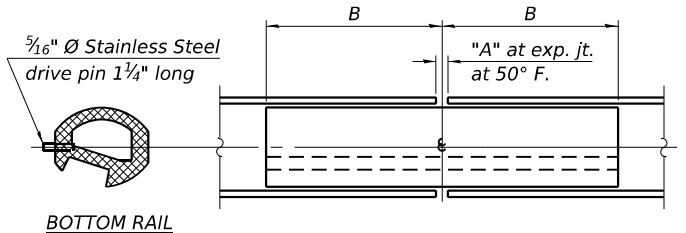
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	48	#5	38'-2"	—
a1(E)	96	#5	21'-0"	—
c(E)	44	#5	2'-5"	⌒
c1(E)	44	#5	6'-1"	—
d(E)	36	#4	4'-10"	⌒
d1(E)	36	#6	3'-9"	⌒
d2(E)	8	#4	2'-1"	⌒
h(E)	8	#6	38'-2"	—
h1(E)	16	#6	21'-0"	—
x(E)	450	#5	2'-11"	⌒
Concrete Superstructure			Cu Yd	59.9
Reinforcement Bars, Epoxy Coated			Pound	7,070
Removing and Re-erecting Existing Railing			Foot	78

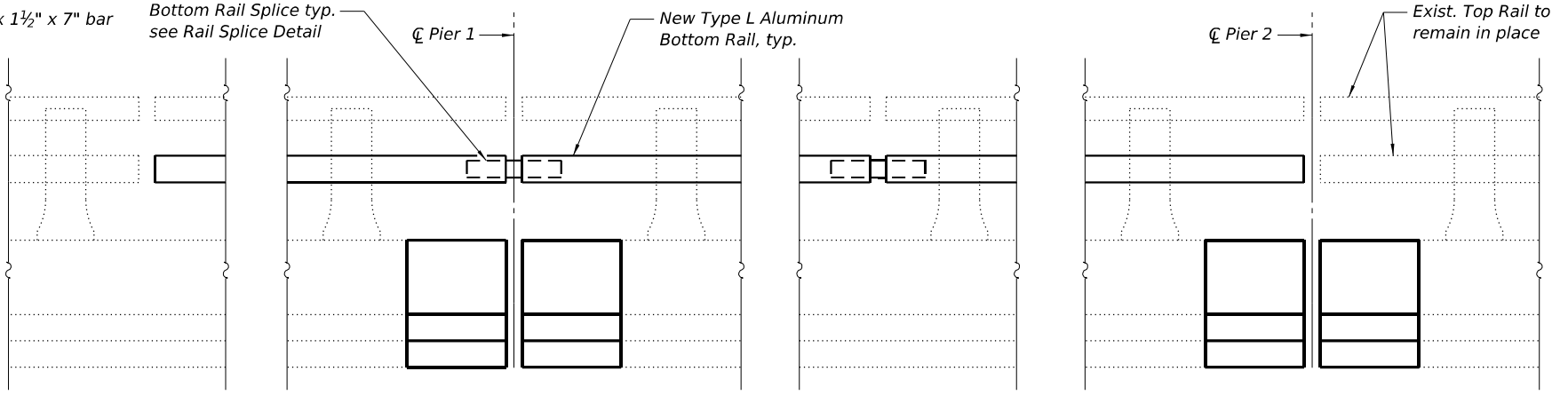
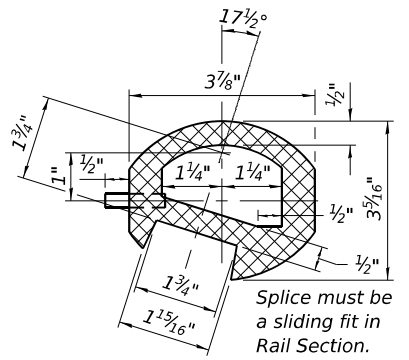
VIEW I-I

DETAIL H

RAIL POST DETAILS



RAIL SPLICE



NOTE

- Cost of removal of existing railing and reinstallation as required to install splice included with Removing and Re-erecting Existing Railing.
- Aluminum alloy extruded rail shall be according to ASTM B 221 (B 221M), Alloy 6061-T6 with a minimum yield of 35,000 psi (250,000 kPa), a minimum tensile strength of 38,000 psi (262,000 kPa), and an elongation of ten percent in 2 in. (50mm).

SPLICE DIMENSIONS

Location	T	A	B
All locs. not over exp. jts.	0	3/8"	1'-2"
Over Strip Seal Jt.	≤4"	2 1/2"	1'-2"

T = total movement along centerline of roadway at expansion joint.

MODEL Path: \\... INFRASTRUCTURE ENGINEERING INCORPORATED

INFRASTRUCTURE ENGINEERING INCORPORATED
 1 South Wacker | Suite 2650 | Chicago, IL 60606
 P 312.425.9598 | F 312.425.9594 | www.infrastructure-eng.com

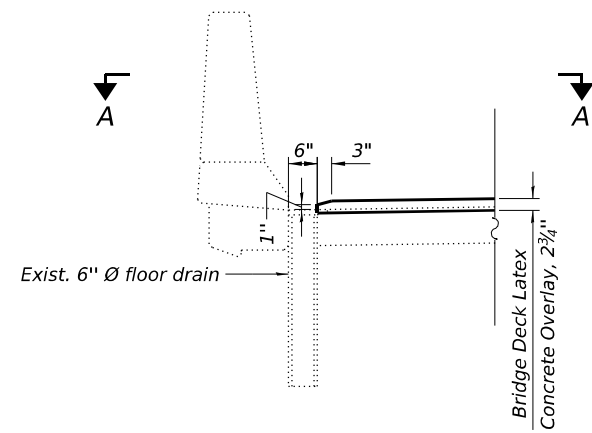
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	CHECKED - PK	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

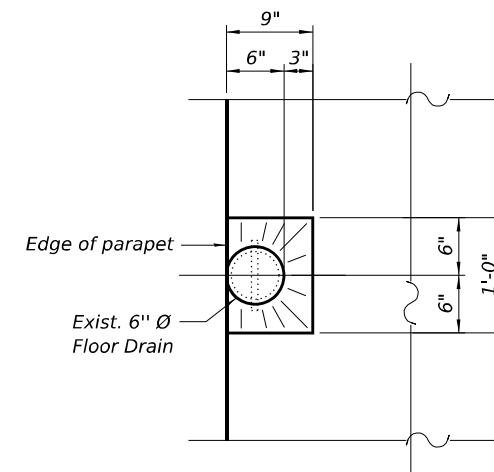
ABUTMENT AND DECK CONCRETE ALTERATION DETAILS
STRUCTURE NO. 022-0075

SHEET 12 OF 23 SHEETS

F.A.P. RTE. 870	SECTION FAP 0870 22 BJ	COUNTY DUPAGE	TOTAL SHEETS 38	SHEET NO. 21
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T30	



SECTION THRU FLOOR DRAINS



SECTION A-A

DRAINAGE SCUPPER DETAILS

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Floor Drains to be Cleaned	Each	12

MODEL Sheet
 FILE NAME: P:\22222-4675-00 IDOT Various Phase 2 (FBI 206-002)\WO 16 63T30DCN\CADD_Sheets\62T30-dt-5T3-1_DrainageDetails.dgn

INFRASTRUCTURE ENGINEERING INCORPORATED
 1 South Wacker | Suite 2650 | Chicago, IL 60606
 P 312.425.9598 | F 312.425.9598 | www.infrastructure-eng.com

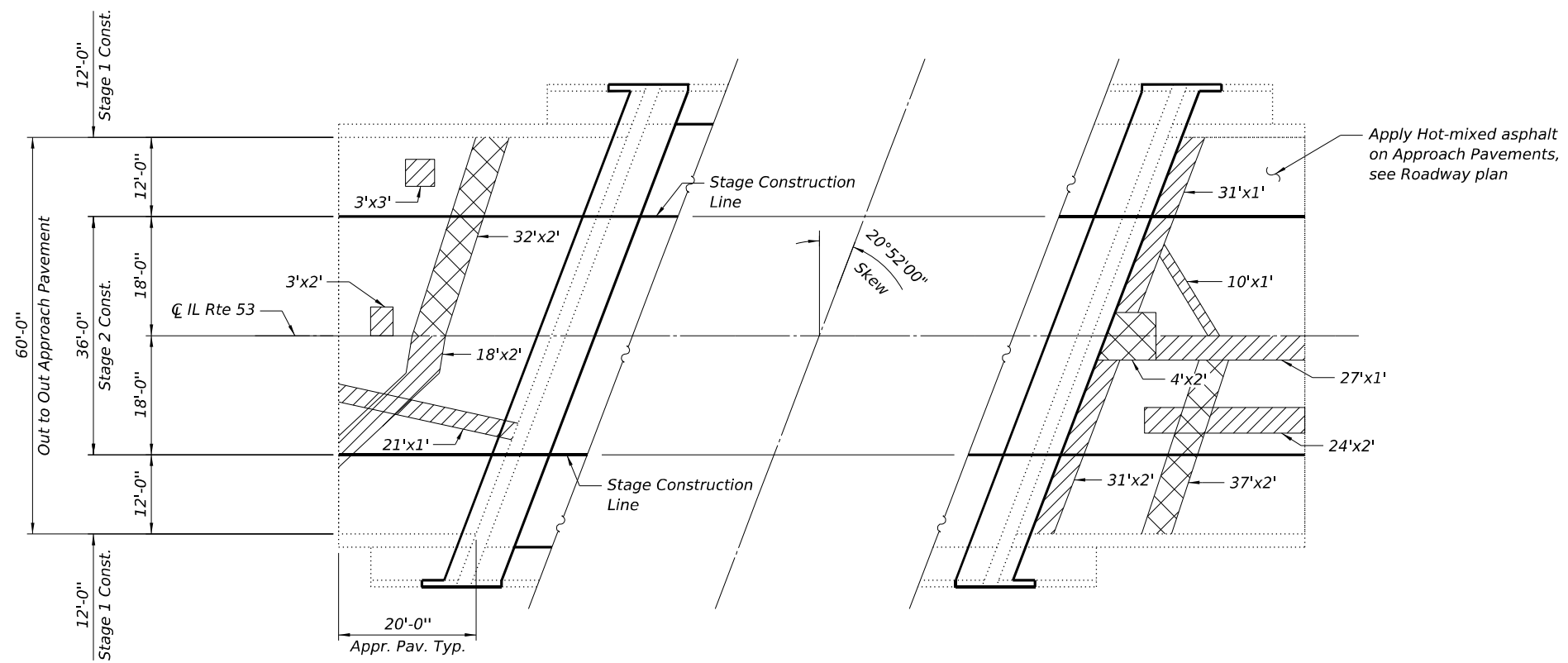
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PLOT DATE = 8/7/2024	CHECKED - PK	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

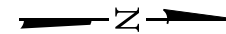
DECK DETAILS AT DRAINAGE STRUCTURES
STRUCTURE NO. 022-0075

SHEET 13 OF 23 SHEETS

F.A.P. RTE. 870	SECTION FAP 0870 22 BJ	COUNTY DUPAGE	TOTAL SHEETS 38	SHEET NO. 22
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T30	



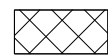
PLAN



LEGEND



Approach Slab Repair (Partial Depth)



Approach Slab Repair (Full Depth)

BILL OF MATERIAL *

ITEM	UNIT	TOTAL
Approach Slab Repair (Full Depth)	Sq Yd	17
Approach Slab Repair (Partial Depth)	Sq Yd	28

* Includes both approach pavements

MODEL Path: \\... FILE NAME: P:\22222222\4675-00 IDOT\Various Phase 2 (ITB 206-002)\WO 16 63T30\DC\DCADD_Sheets\62T30-02-01-19_SuprPav.dgn

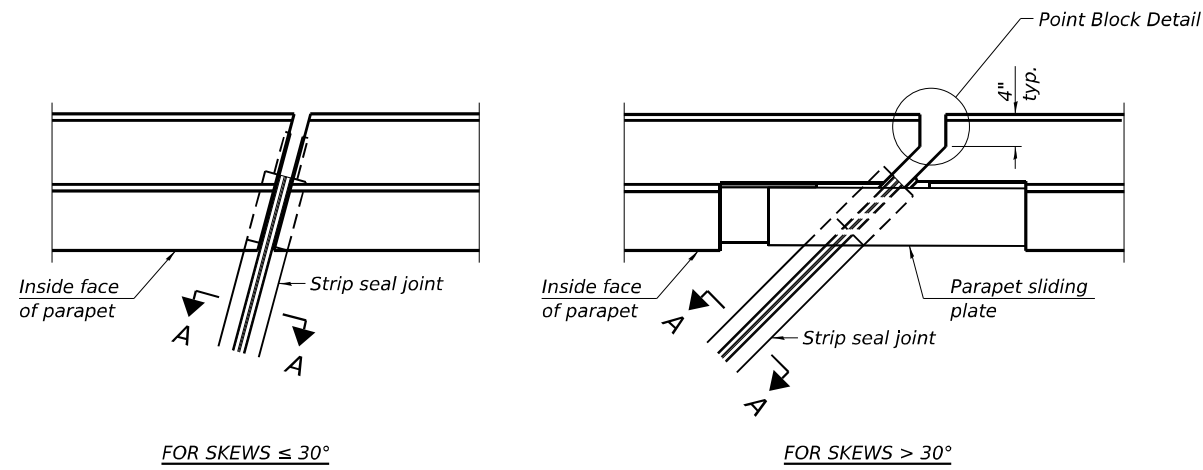
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PLOT DATE = 8/7/2024	CHECKED - PK	REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**APPROACH PAVEMENT REPAIR PLAN
STRUCTURE NO. 022-0075**

SHEET 14 OF 23 SHEETS

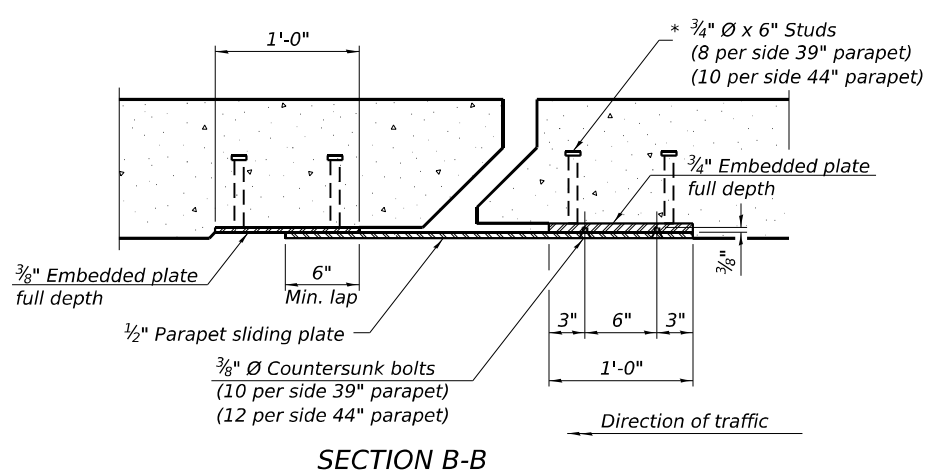
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ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T30	



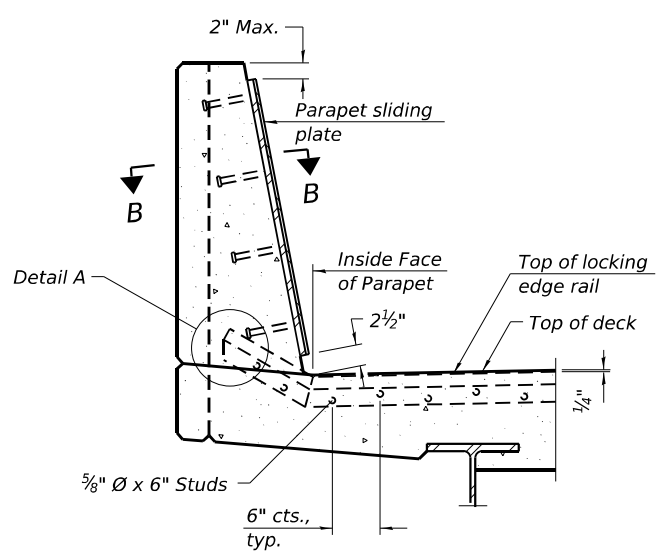
FOR SKEWS ≤ 30°

FOR SKEWS > 30°

PLAN AT PARAPET

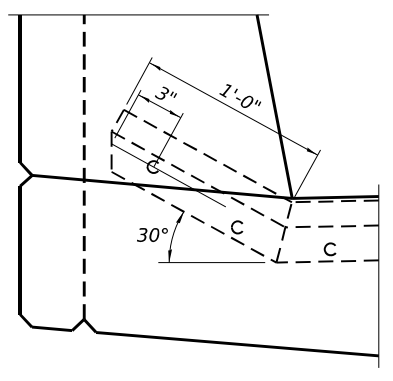


SECTION B-B

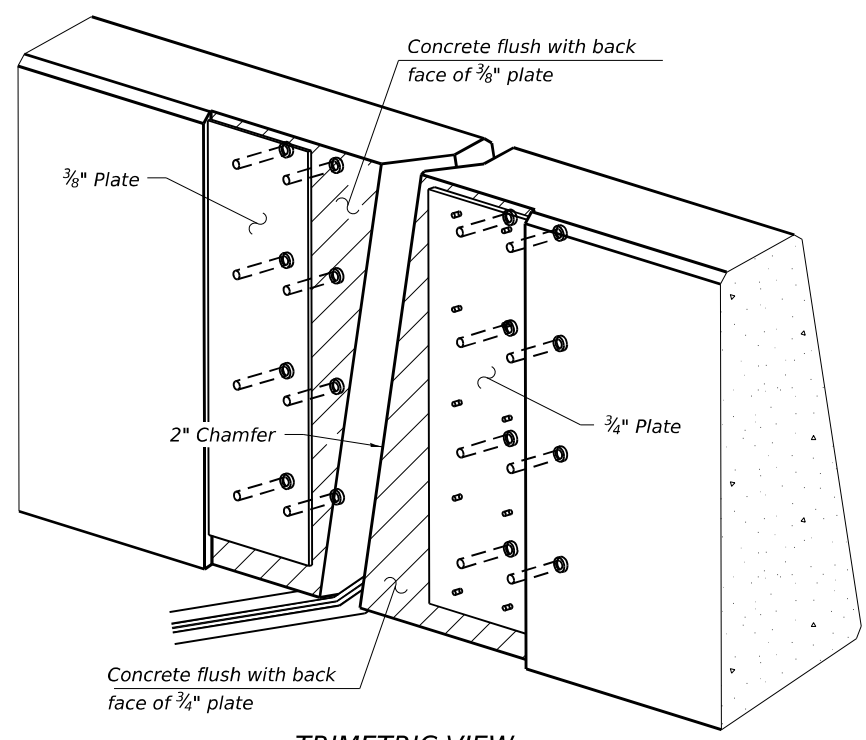


SECTION AT PARAPET

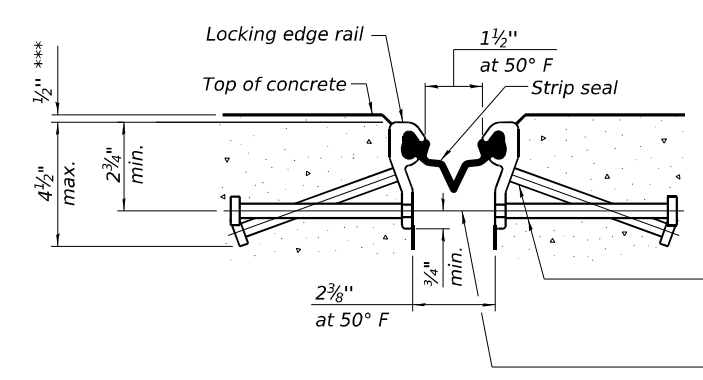
(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)



DETAIL A



TRIMETRIC VIEW
(Showing embedded plates only)



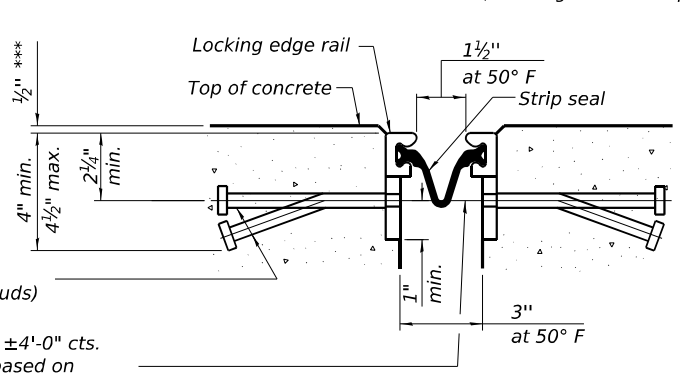
SHOWING ROLLED RAIL JOINT

* 5/8" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

3/8" Ø threaded rods in 7/16" Ø holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

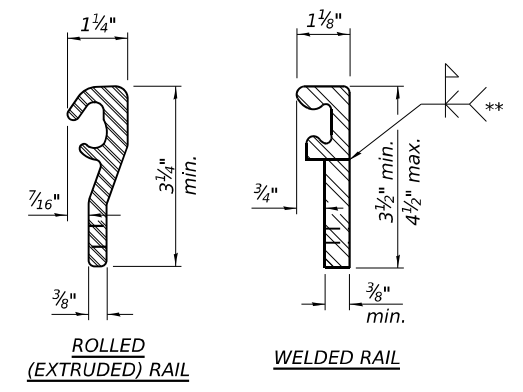
SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



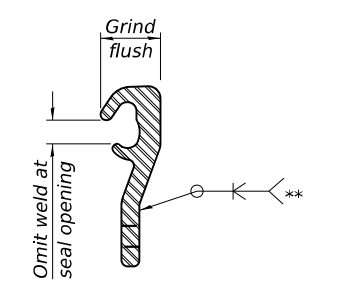
SHOWING WELDED RAIL JOINT

*** Prior to grinding



LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	318

MODEL Path: \\... FILE NAME: PJP-22-22-4675-00_IDOT_Vehic... Phase 2 (FTR) 206-0021\WO 16 63730DCNCARD_Sheets\2130-Str-16_PJSS1.dgn

EJ-SS-S

5-15-2023

(Sheet 1 of 2)

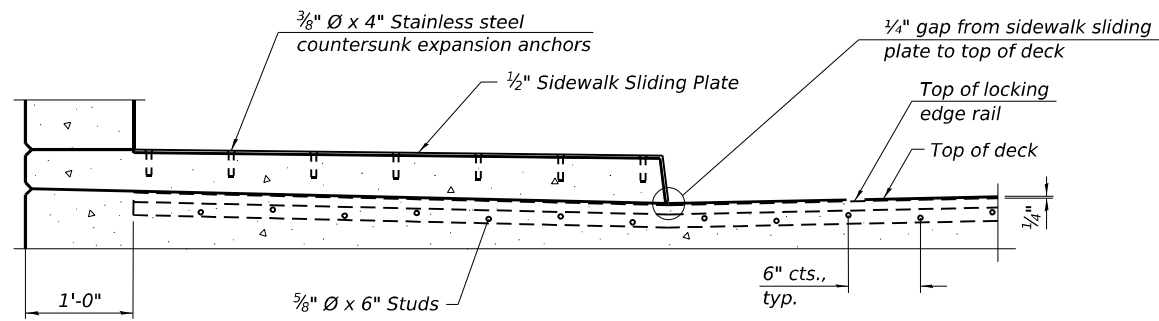
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

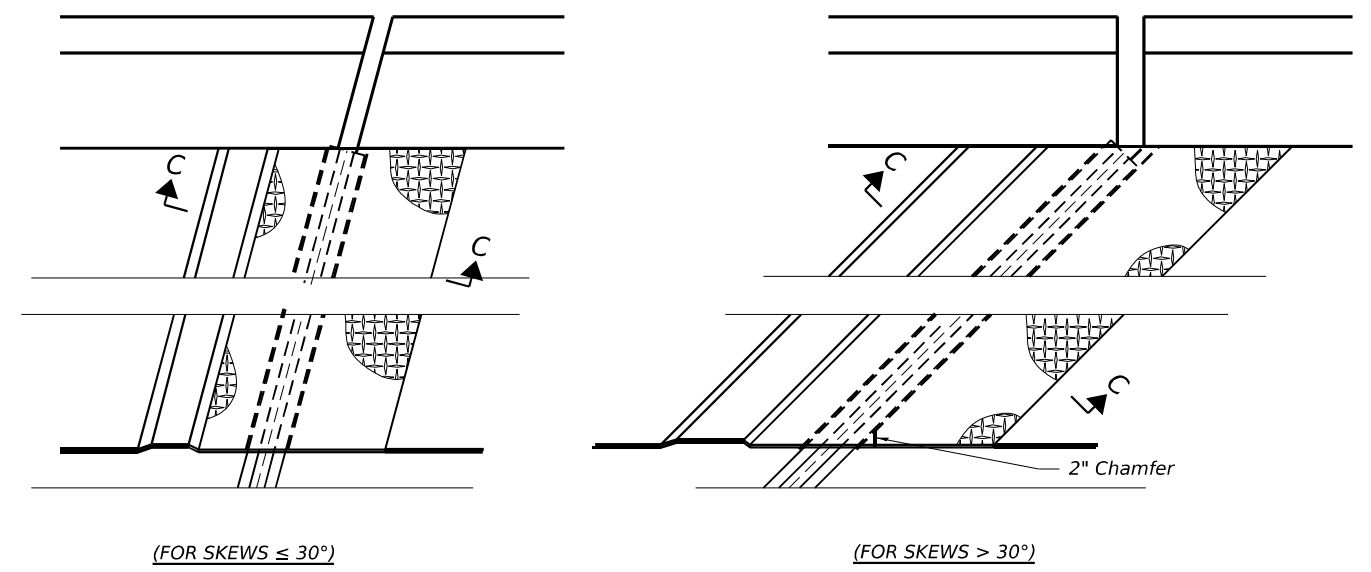
**PREFORMED JOINT STRIP SEAL – SIDEWALK (SHEET 1 OF 2)
STRUCTURE NO. 022-0075**

SHEET 15 OF 23 SHEETS

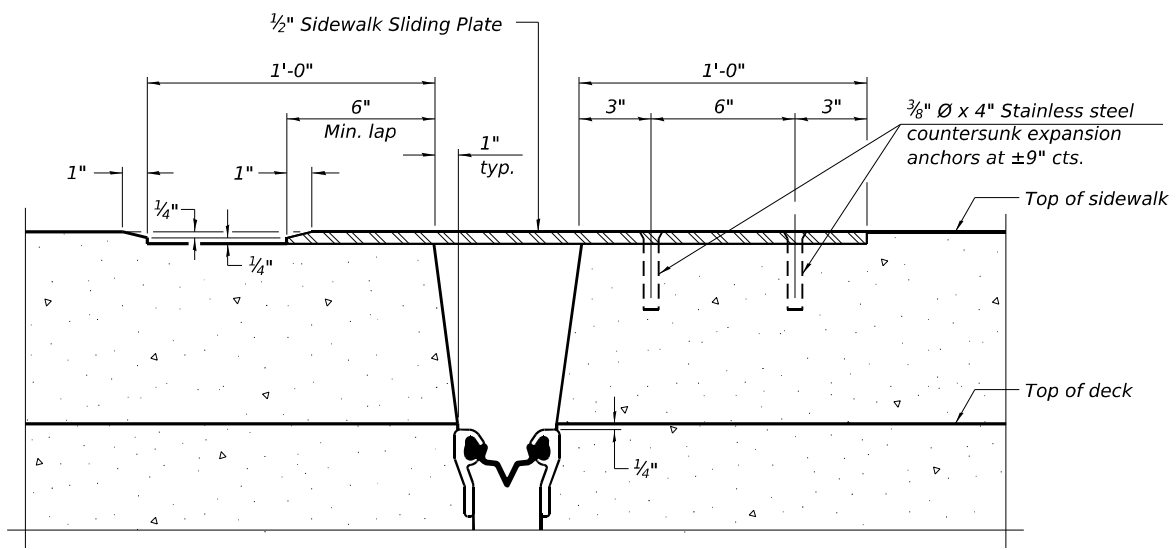
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ	DUPAGE	38	24
CONTRACT NO. 62T30			ILLINOIS FED. AID PROJECT	



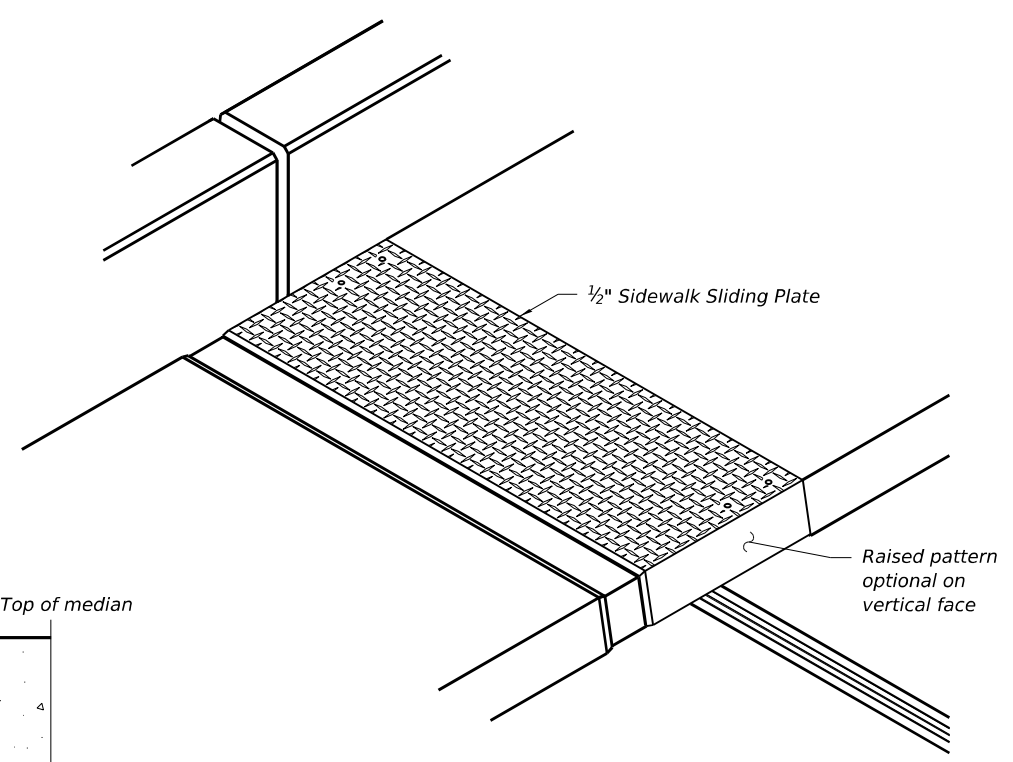
SECTION AT RAISED SIDEWALK



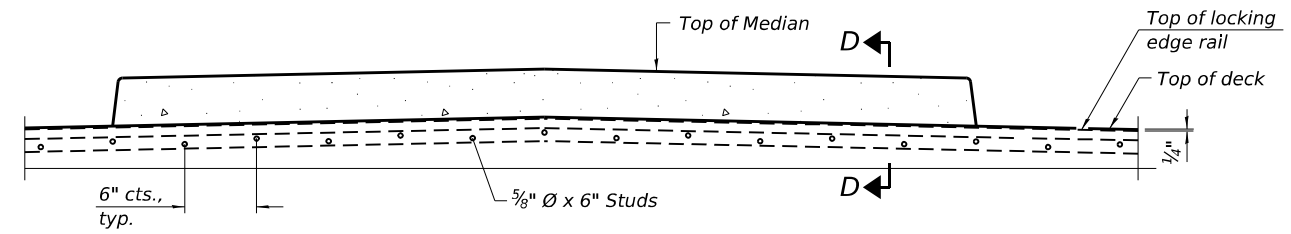
PLAN AT RAISED SIDEWALK



SECTION C-C

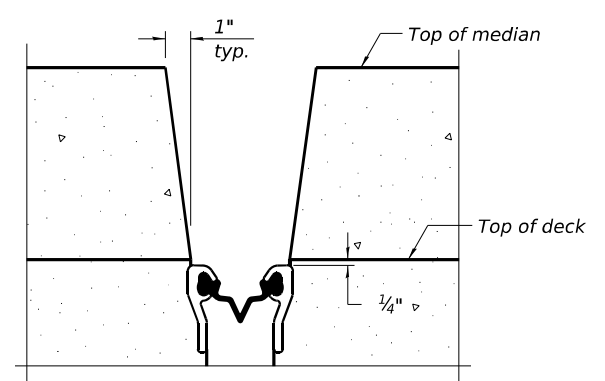


TRIMETRIC VIEW



SECTION AT MEDIAN

For skew > 30°, chamfer acute corners 2" similar to sidewalk.



SECTION D-D
(at Rt. L's)

EJ-SS-S

5-15-2023

(Sheet 2 of 2)

MODEL: P:\6461\... FILE NAME: P:\22222222\4675-00_IDOT_Veribus_Phase 2 (FTR_206-002)\16_63730\DC\DC\CADD_Sheets\62T30-Str-17_PJSS2.dgn

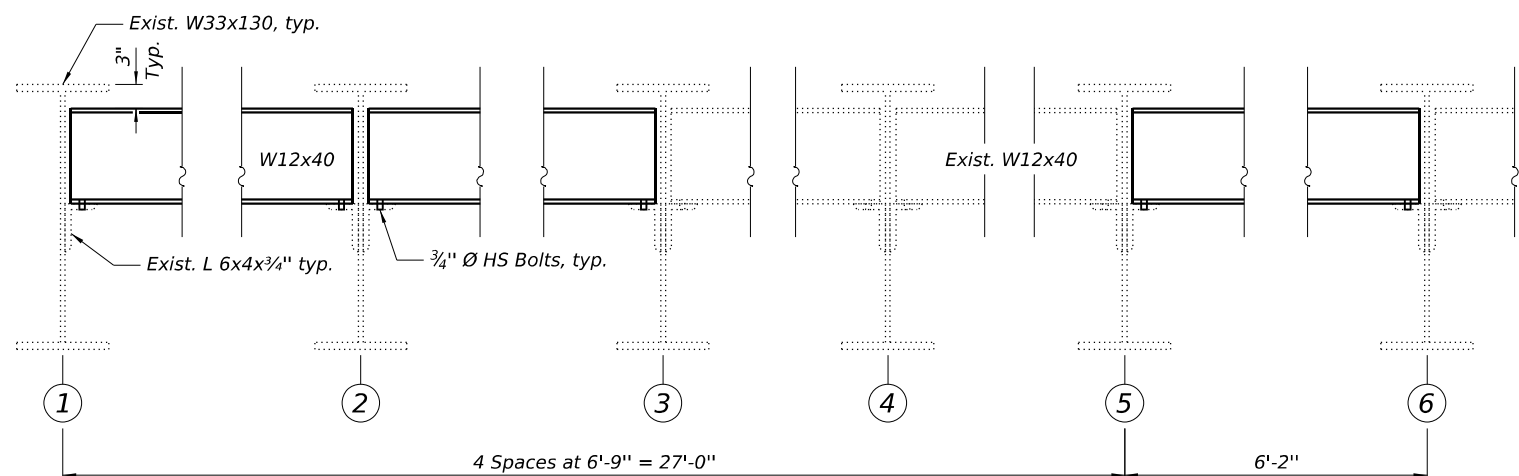
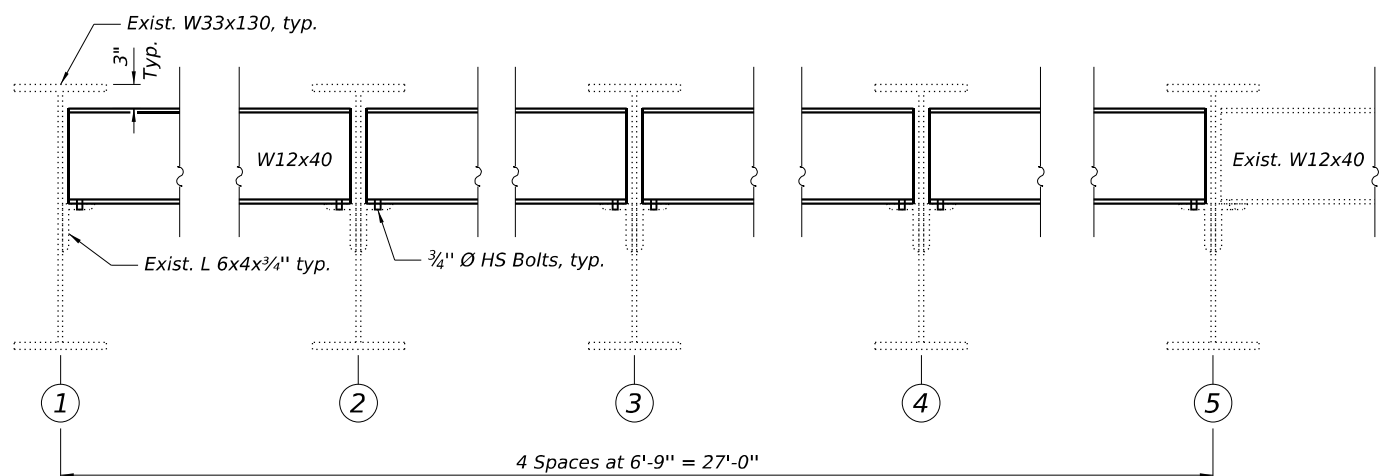
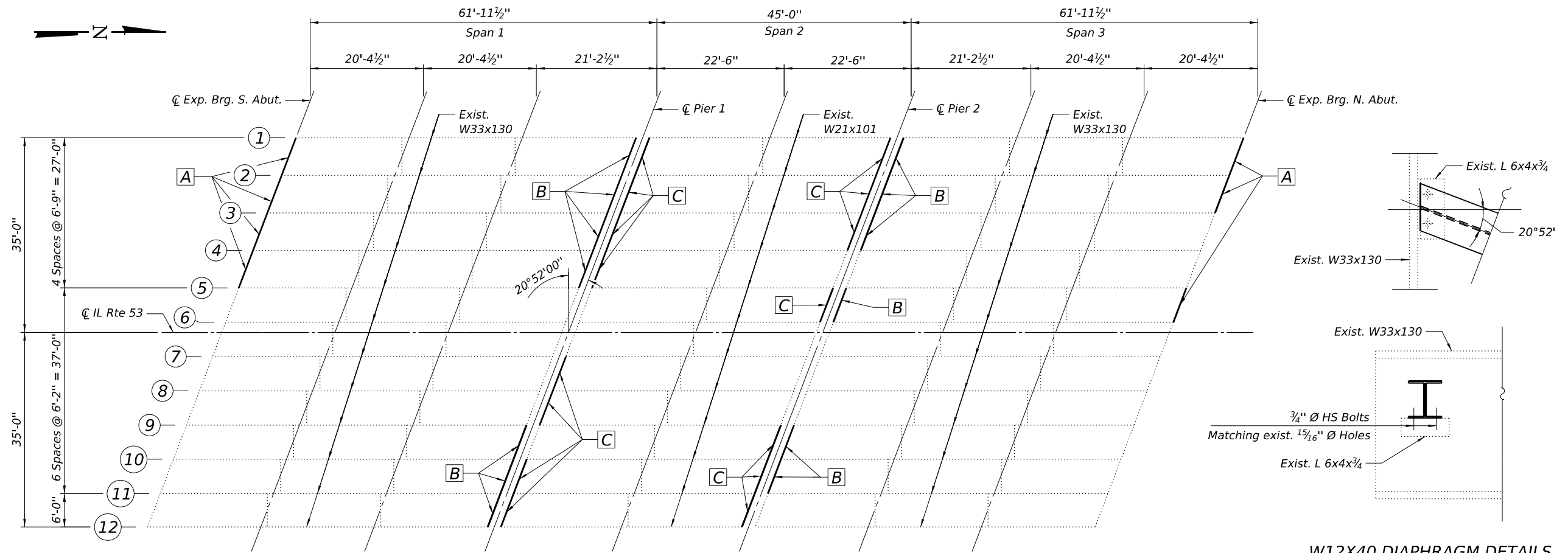
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PLOT SCALE =	CHECKED - PK	REVISED -
PLOT DATE = 8/7/2024	DRAWN - TY	REVISED -
	CHECKED - PK	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL – SIDEWALK (SHEET 2 OF 2)
STRUCTURE NO. 022-0075

SHEET 16 OF 23 SHEETS

F.A.P. RTE. 870	SECTION FAP 0870 22 BJ	COUNTY DUPAGE	TOTAL SHEETS 38	SHEET NO. 25
			CONTRACT NO. 62T30	
ILLINOIS FED. AID PROJECT				



LEGEND

- ① Girder Number
- A Remove and Replace w12x40 End Diaphragm
- B Remove and Replace w12x40 Pier Diaphragm
- C Remove and Replace w8x31 Pier Diaphragm

NOTES

1. For Diaphragm Replacement at Pier 1, Diaphragm Replacement at Pier 2, W8x31 Diaphragm Details, and Bill of Material, see Sheet 18 of 23.
2. All new structural steel shall be galvanized. See Special Provision for "Hot Dip Galvanizing for Structural Steel".
3. Cost of removal and re-installation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Furnishing and Erecting Structural Steel.
4. Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the special provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".
5. After the new beam is in its final position, the Engineer in the field shall check to see that the top flange is tight against the slab. If not, the Contractor shall inject epoxy between the existing concrete deck and the top flange of the beam. See Special Provision "Epoxy Injection".
6. Two hardened washers shall be required for each set of oversized holes.

MODEL: D:\64\11... FILE NAME: P:\22\2224675-00_IDOT_Vulnicor_Phase 2 (ITB)_206-002\1\10_16_63\T00DCNCADD_Sheets\62T30-02-14 Framing.dgn

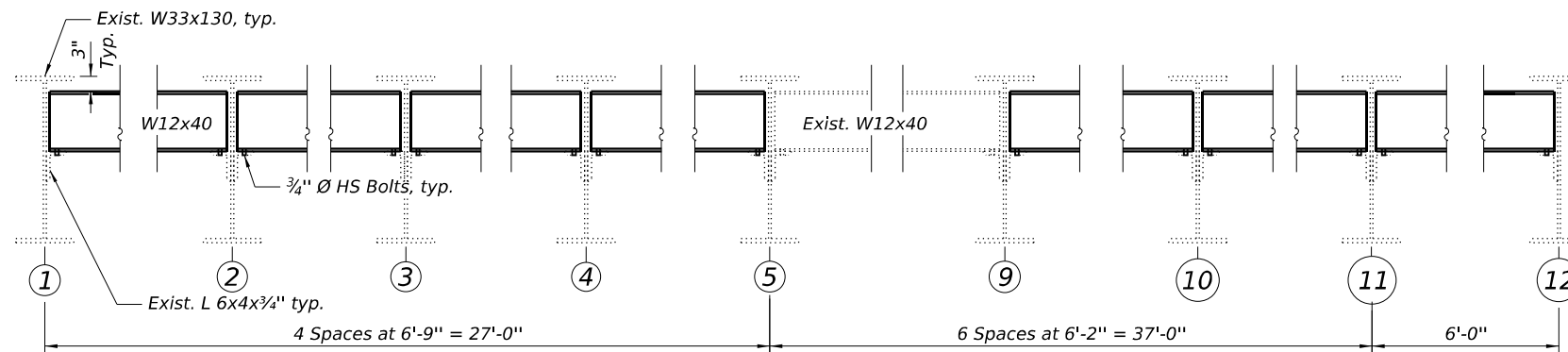
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CHECKED - PK	REVISIONS -	
PLOT SCALE =	DRAWN - TY	REVISED -
PLOT DATE = 8/7/2024	CHECKED - PK	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

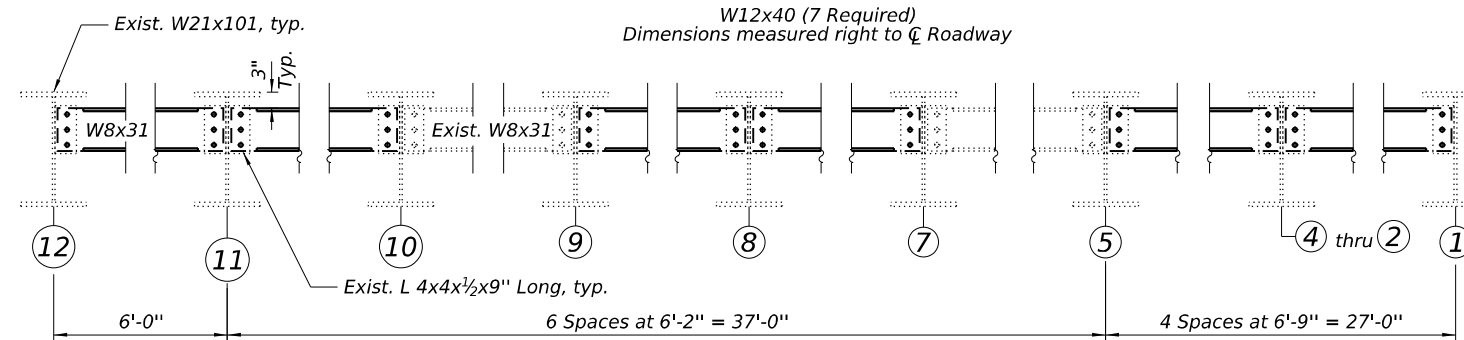
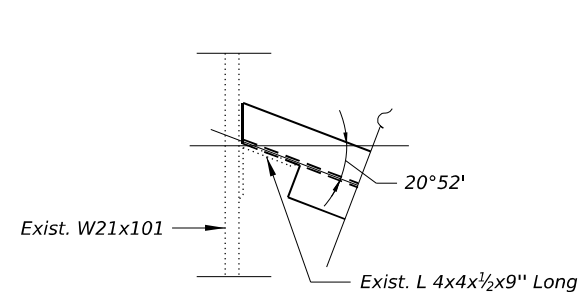
**FRAMING PLAN AND DETAILS
STRUCTURE NO. 022-0075**

SHEET 17 OF 23 SHEETS

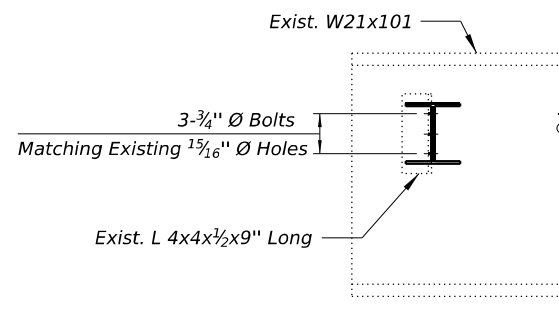
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ	DUPAGE	38	26
CONTRACT NO. 62T30			ILLINOIS FED. AID PROJECT	



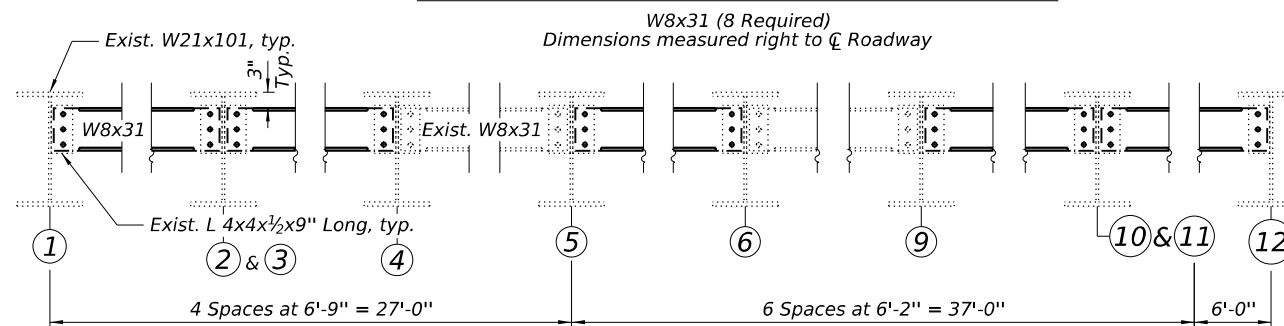
DIAPHRAGM REPLACEMENT AT SOUTH OF PIER 1



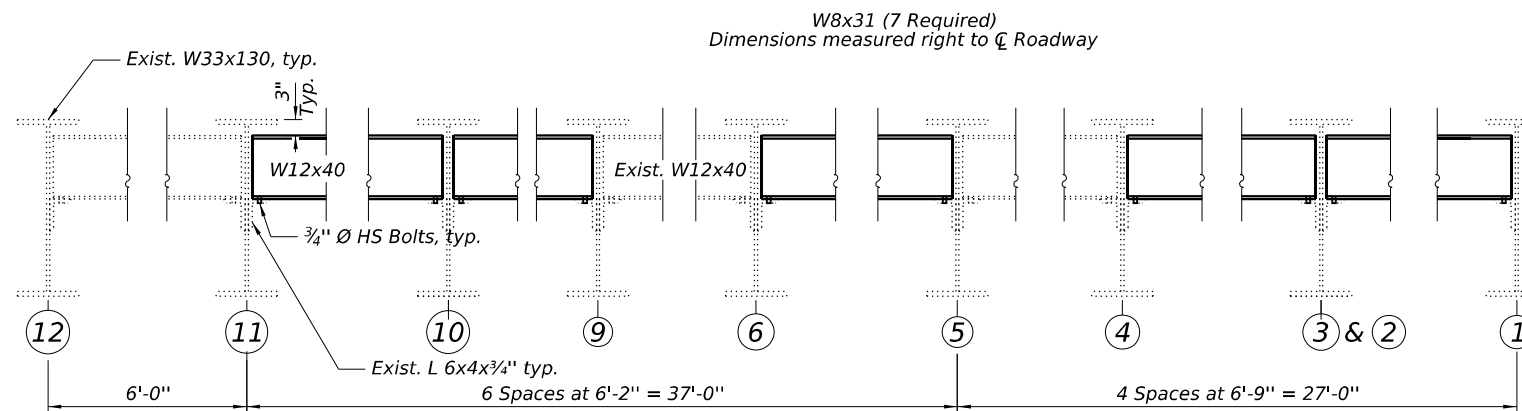
DIAPHRAGM REPLACEMENT AT NORTH OF PIER 1



W8X31 DIAPHRAGM DETAILS



DIAPHRAGM REPLACEMENT AT SOUTH OF PIER 2



DIAPHRAGM REPLACEMENT AT NORTH OF PIER 2

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Steel Removal	Pound	8,800
Furnishing & Erecting Structural Steel	Pound	8,800

MODEL Path: \\...
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	CHECKED - PK	REVISED -
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PLOT DATE = 9/27/2024	CHECKED - PK	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

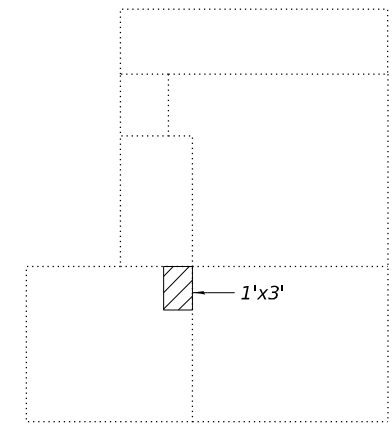
**DIAPHRAGM DETAILS
STRUCTURE NO. 022-0075**

SHEET 18 OF 23 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ	DUPAGE	38	27
CONTRACT NO. 62T30			ILLINOIS FED. AID PROJECT	



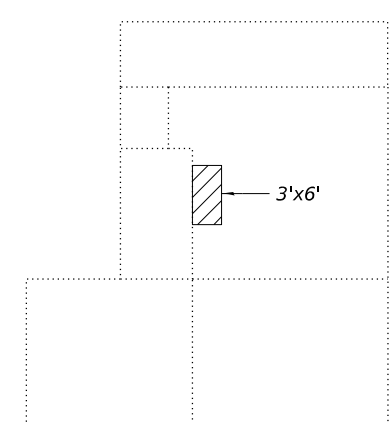
SOUTH ABUTMENT NORTH ELEVATION
(Looking South)



SECTION A-A



NORTH ABUTMENT SOUTH ELEVATION
(Looking North)



SECTION B-B

LEGEND



Structural Repair Of Concrete
(Depth Equal To Or Less Than 5 Inches)

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	Sq Ft	52

MODEL: D:\64\11
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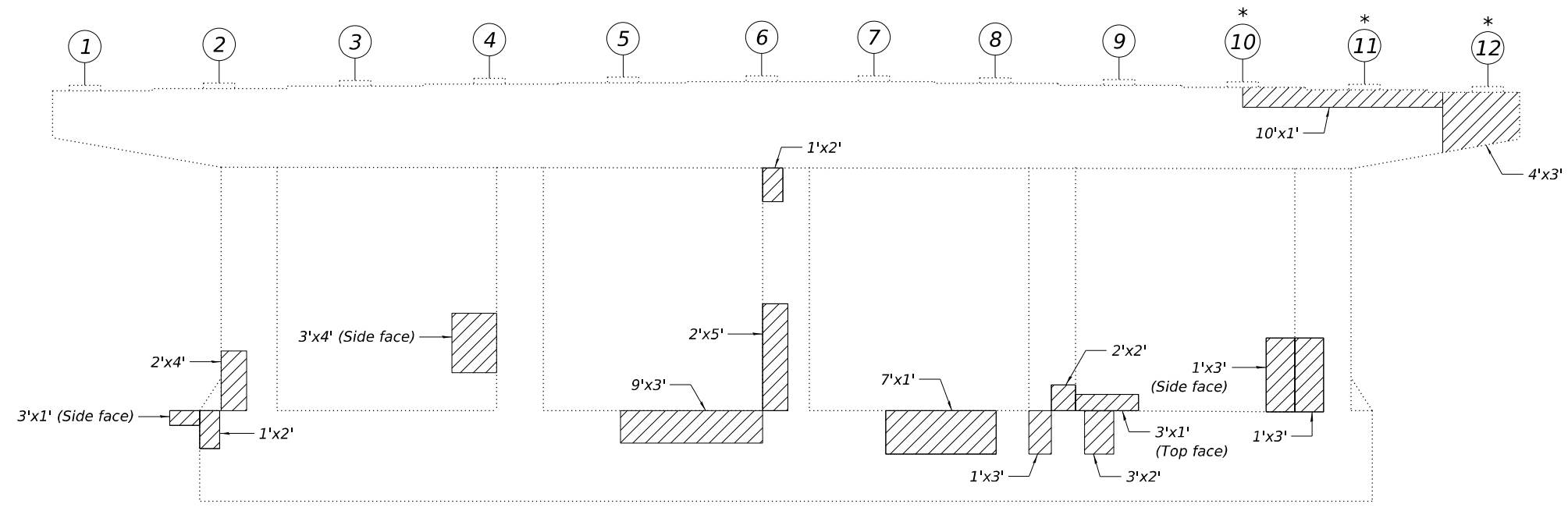
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	CHECKED - PK	REVISED -
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PLOT DATE = 8/7/2024	CHECKED - PK	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ABUTMENT REPAIRS
STRUCTURE NO. 022-0075**

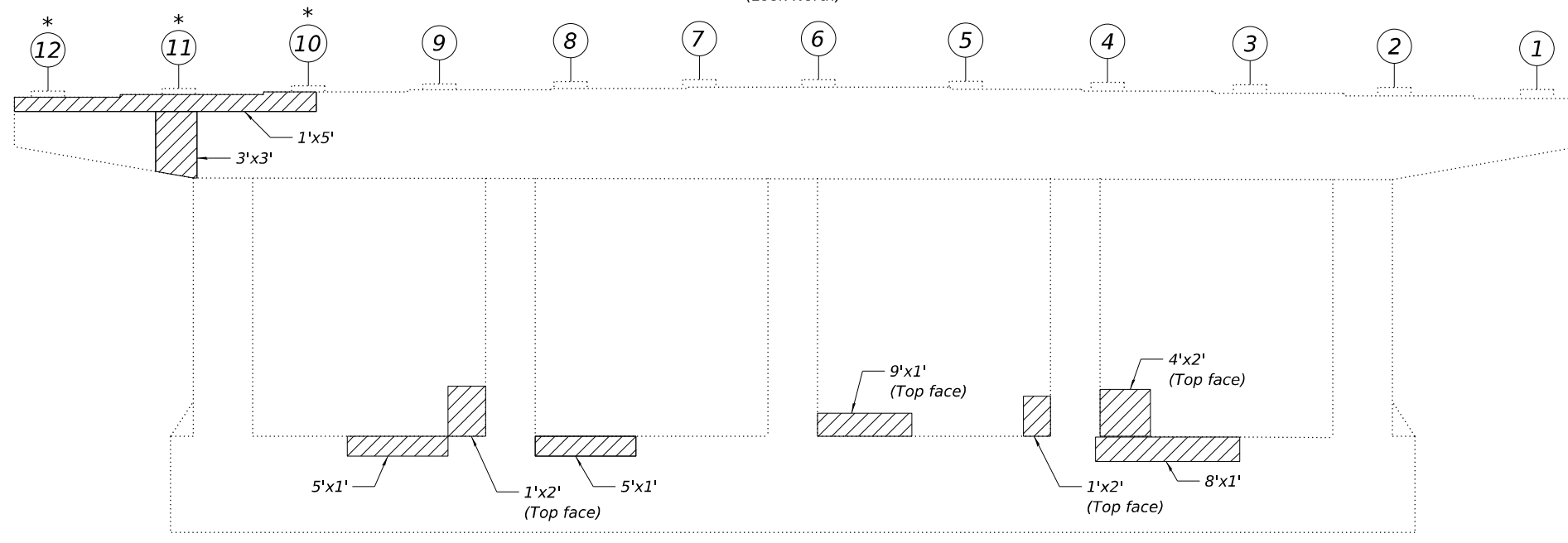
SHEET 19 OF 23 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ	DUPAGE	38	28
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T30	



PIER 1 SOUTH FACE

(Look North)



PIER 1 NORTH FACE

(Look South)

LEGEND

- Structural Repair Of Concrete
(Depth Equal To Or Less Than 5 Inches)
- 1 Girder Number
- * Beam Requires Temporary Shoring

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	Sq Ft	168
Temporary Shoring and Cribbing	Each	6

**INTERIOR BEAM REACTION
TABLE AT PIER 1**

	Pier 1 S.	Pier 1 N.
$R_{DL} (k)$	35.3	24.3
$R_{LL} (k)$	37.4	34.5
$Imp (k)$	10.1	10.3
$R_{Total} (k)$	82.8	69.1

MODEL Path: \\...
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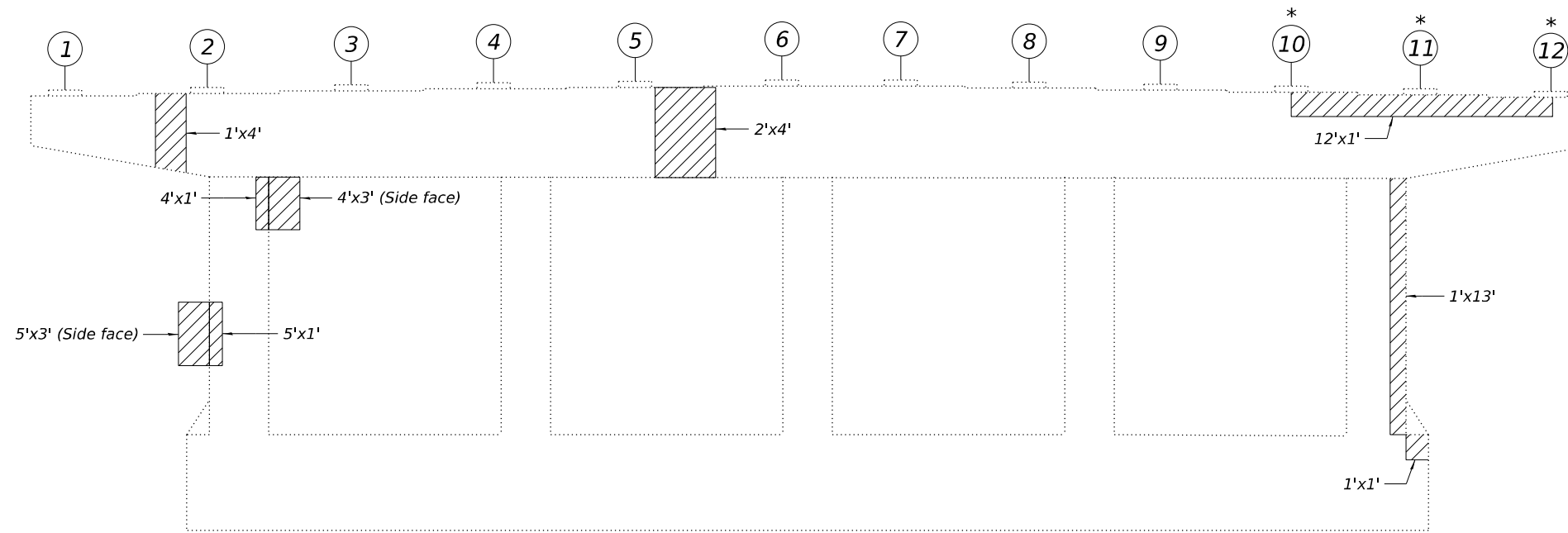
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CHECKED - PK	REVISIONS -	
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PLOT DATE = 9/27/2024	CHECKED - PK	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER 1 REPAIRS
STRUCTURE NO. 022-0075**

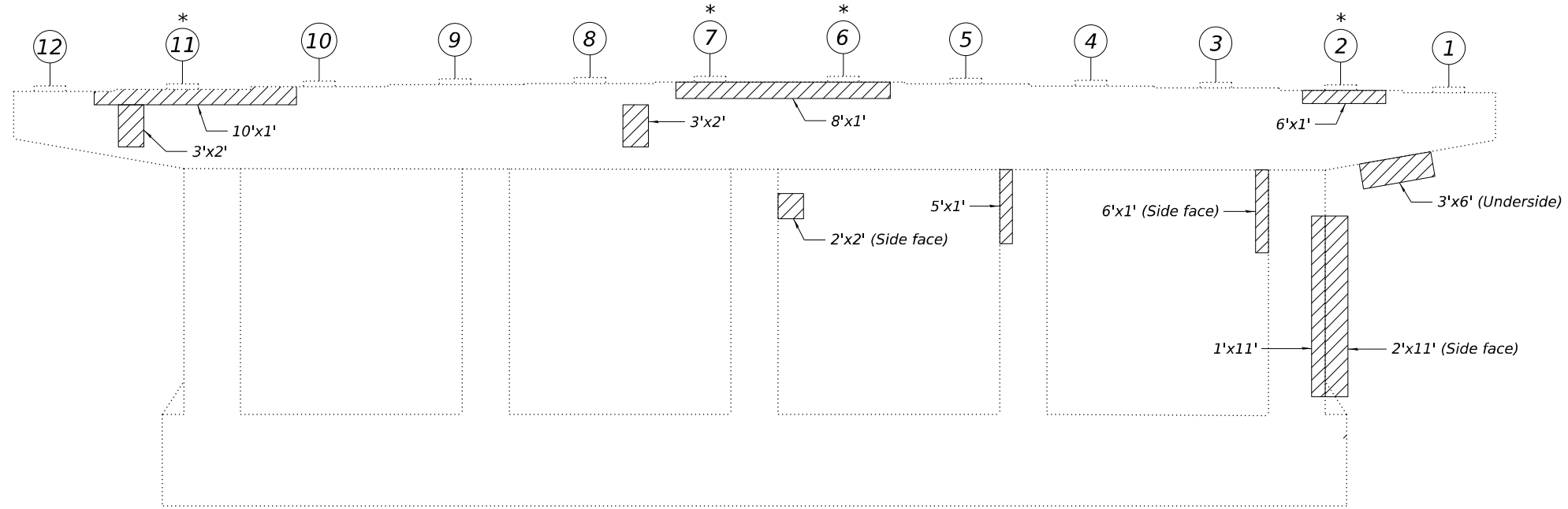
SHEET 20 OF 23 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ	DUPAGE	38	29
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T30	



Pier 2 South Face

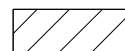
(Look North)



Pier 2 North Face

(Look South)

LEGEND

 Structural Repair Of Concrete
(Depth Equal To Or Less Than 5 Inches)

1 Girder Number

* Beam Requires Temporary Shoring

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	Sq Ft	176
Temporary Shoring and Cribbing	Each	7

**INTERIOR BEAM REACTION
TABLE AT PIER 2**

	Pier 2 S.	Pier 2 N.
$R_{DL} (k)$	24.3	35.3
$R_{LL} (k)$	34.5	37.4
$Imp (k)$	10.3	10.1
$R_{Total} (k)$	69.1	82.8

MODEL Path: \\... \DOT_Various Phase 2 (FBI_206-002)\WO 16 63T30\DC\NCADD_Sheets\62T30-Int-STR-13_Pier2Repair.dgn
FILE NAME: PIR_22T30-4675-00_IDOT_Various Phase 2 (FBI_206-002)\WO 16 63T30\DC\NCADD_Sheets\62T30-Int-STR-13_Pier2Repair.dgn

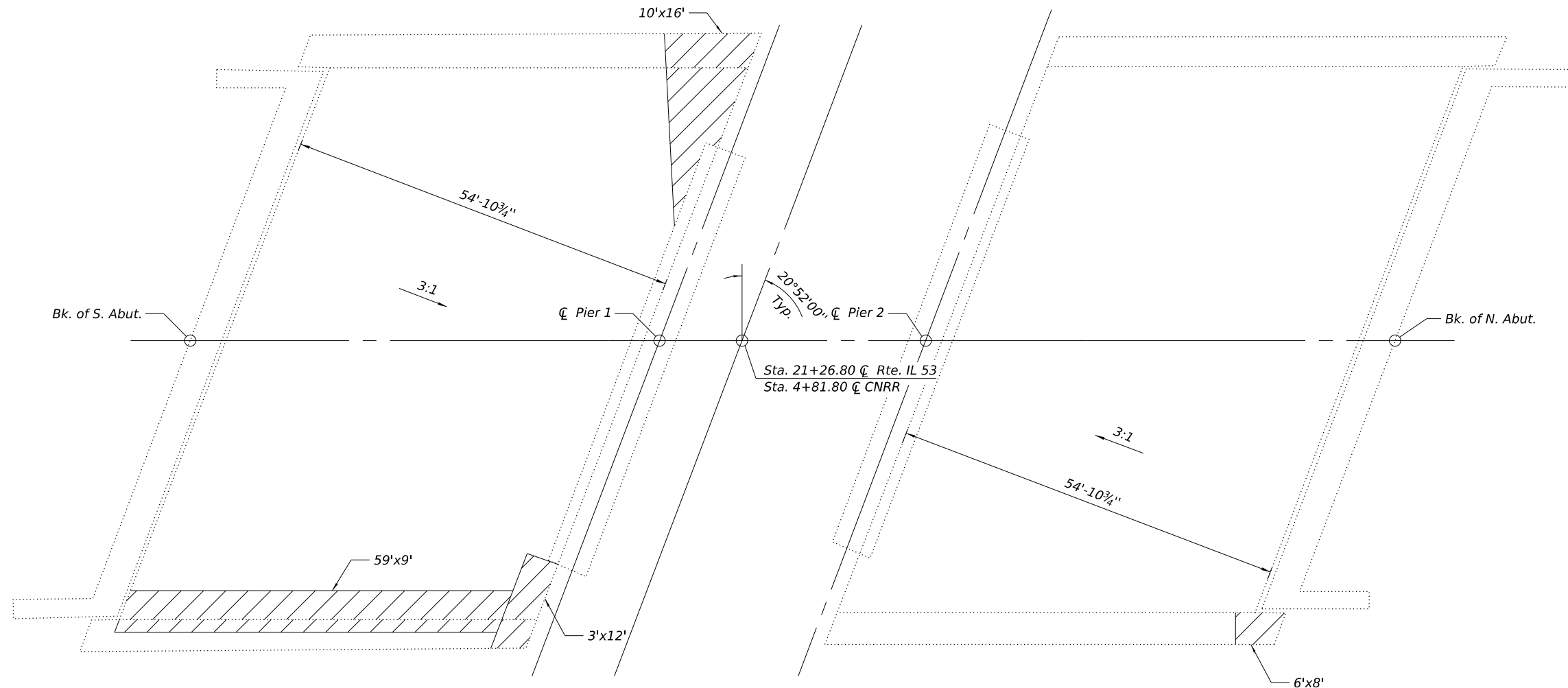
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

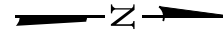
**PIER 2 REPAIRS
STRUCTURE NO. 022-0075**

SHEET 21 OF 23 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ	DUPAGE	38	30
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T30	



PLAN



LEGEND



Slope Wall Repair

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Controlled Low-Strength Material	Cu Yd	15
Slope Wall Repair	Sq Yd	87

MODEL: D:\64611
 FILE NAME: P:\22222-4675-00 IDOT VARIOUS Phase 2 (ITB 206-002)\WO 16 62T30\DC\DCADD_Sheets\62T30-21t-5T-18 SlopeWall.dgn

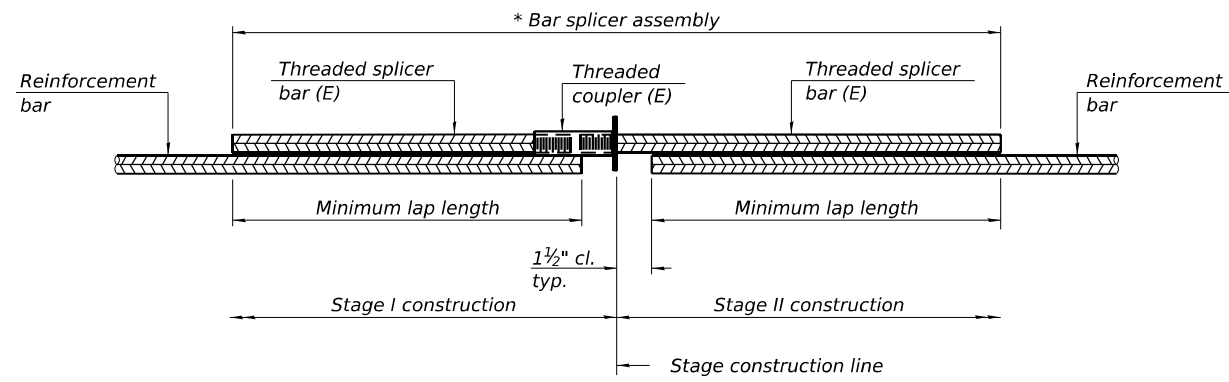
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PLOT DATE = 8/7/2024	CHECKED - PK	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SLOPEWALL REPAIRS & REPLACEMENT
STRUCTURE NO. 022-0075**

SHEET 22 OF 23 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ	DUPAGE	38	31
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T30	



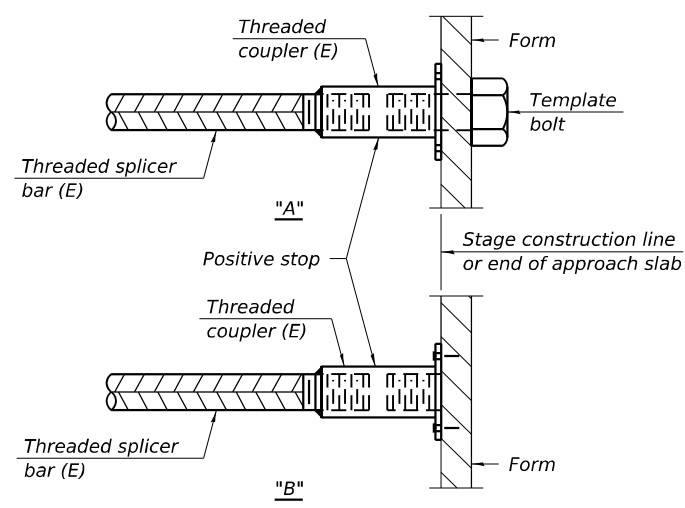
STANDARD BAR SPLICER ASSEMBLY PLAN

Only bar splicer assemblies as presented on the approved QPL list may be used.

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

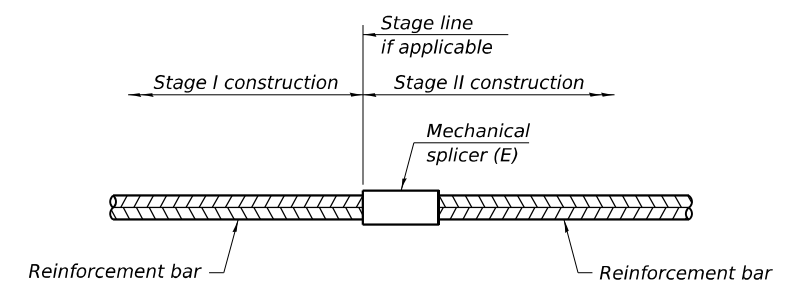
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
S. End Span 1 Deck	#5	16	3'-4"
N. End Span 1 Deck	#5	16	3'-4"
S. End Span 2 Deck	#5	16	3'-4"
N. End Span 2 Deck	#5	16	3'-4"
S. End Span 3 Deck	#5	16	3'-4"
N. End Span 3 Deck	#5	16	3'-4"
S. Abutment	#6	8	4'-0"
N. Abutment	#6	8	4'-0"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL Path: \\...
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BSD-1 5-15-2023

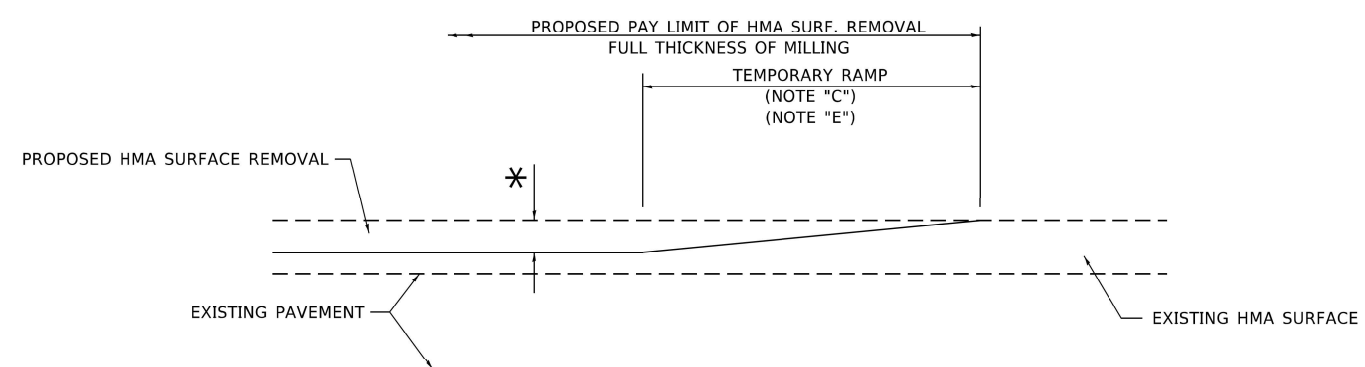
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PLOT DATE = 8/7/2024	DRAWN - TY	REVISIONS -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 STRUCTURE NO. 022-0075**

SHEET 23 OF 23 SHEETS

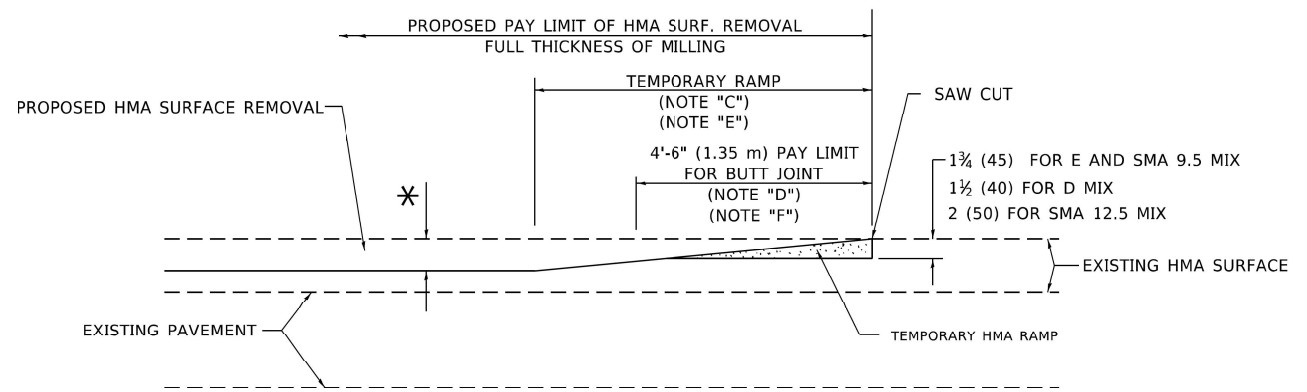
F.A.P. RTE. 870	SECTION FAP 0870 22 BJ	COUNTY DUPAGE	TOTAL SHEETS 38	SHEET NO. 32
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T30	



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

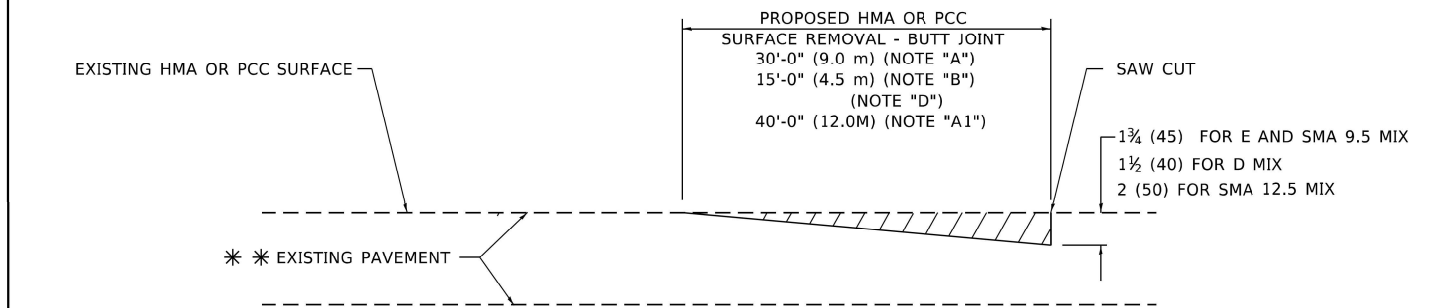


HMA CONSTRUCTED TEMPORARY RAMP

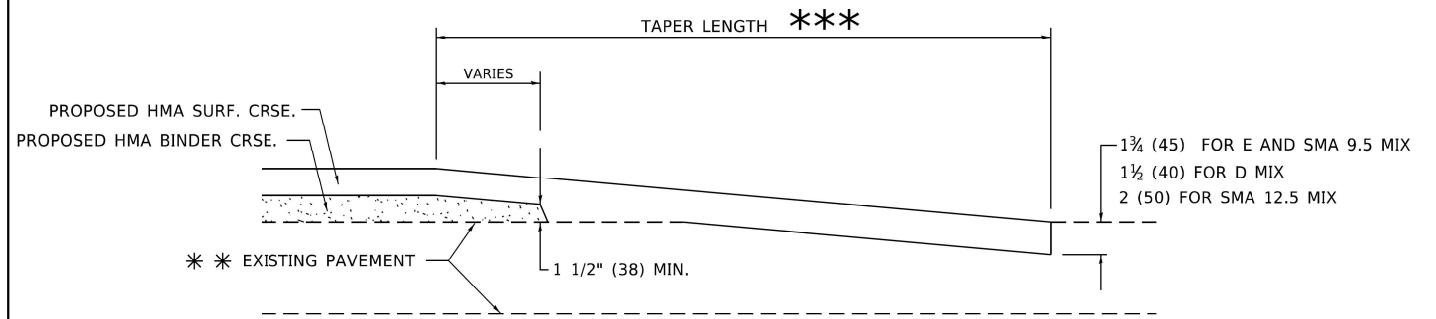
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

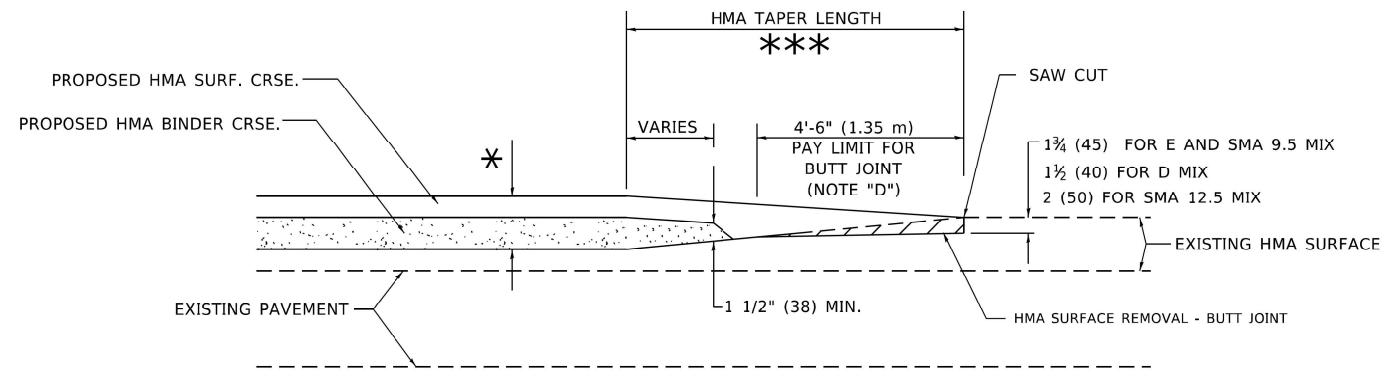
GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

MODEL: P:\64611\16_6370DCN\CADD_Sheets\6370-DIST-SURF-STANDARD-05-01.dgn

INFRASTRUCTURE ENGINEERING INCORPORATED
1 South Wacker | Suite 2650 | Chicago, IL 60606
P 312.425.9598 | F 312.425.9594 | www.infrastructure-eng.com

USER NAME = ALane	DESIGNED - CW	REVISED -
PLOT SCALE = 20,0000 * / in.	DRAWN - CW	REVISED -
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	DATE - 08/07/2024	

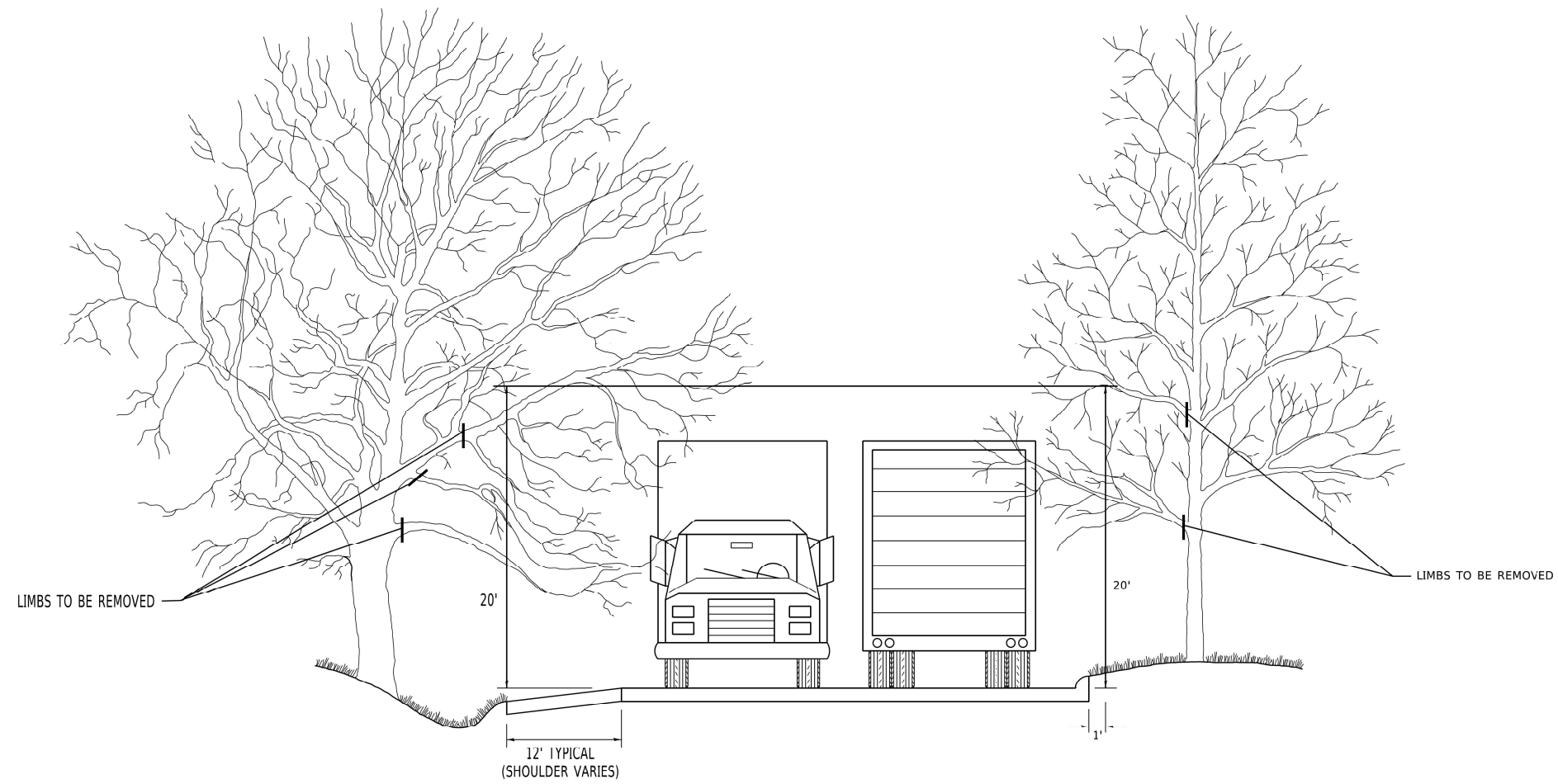
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - BUTT JOINT AND HMA TAPER DETAILS (BD-32)

SCALE: STA. TO STA.

F.A.P. RTE. 870	SECTION FAP 0870 22 BJ	COUNTY DUPAGE	TOTAL SHEETS 38	SHEET NO. 33
			CONTRACT NO. 62T30	
ILLINOIS FED. AID PROJECT				

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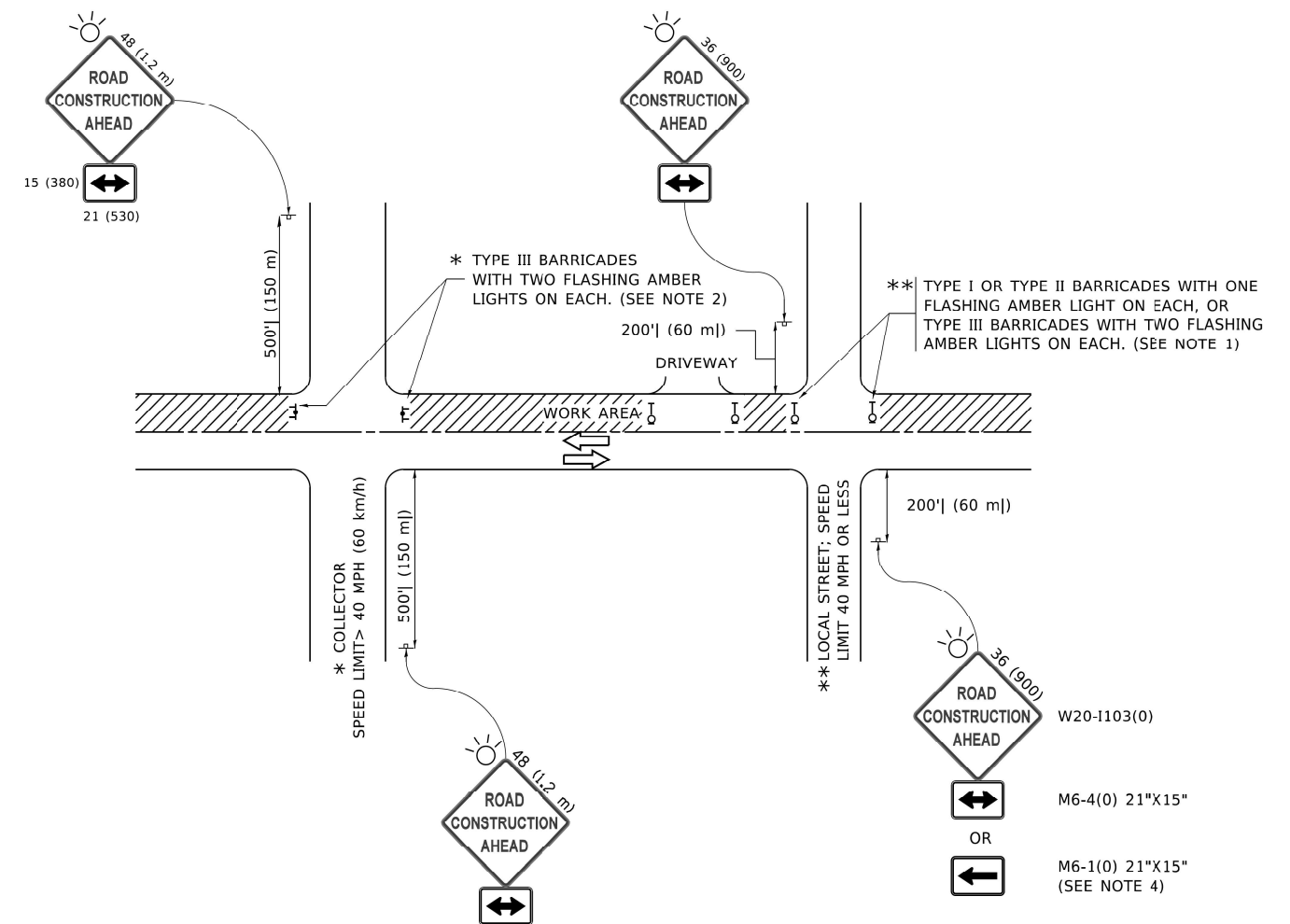
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DATE - 08/07/2024		

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE – PRUNING FOR SAFETY AND EQUIPMENT
 CLEARANCE (BM-20)**

SCALE: _____ STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ	DUPAGE	38	34
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T30	



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: P:\646\11
FILE NAME: P:\646\11\DOT_Vehicle_Phase 2 (FBI_206-002)\WO_16_637\00\CON\CADD_Sheets\637\30-SITE-STANDARD-05-03.dwg

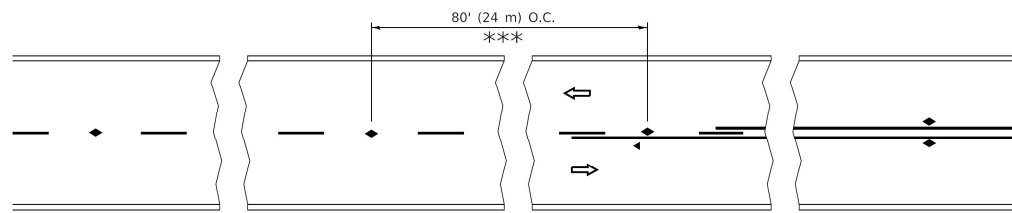
<p>INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9598 www.infrastructure-eng.com</p>	USER NAME = Alane	DESIGNED - CW	REVISED -
	PLOT SCALE = 20,0000 * / in.	DRAWN - CW	REVISED -
		CHECKED - ACL	REVISED -
		DATE - 08/07/2024	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE – TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)**

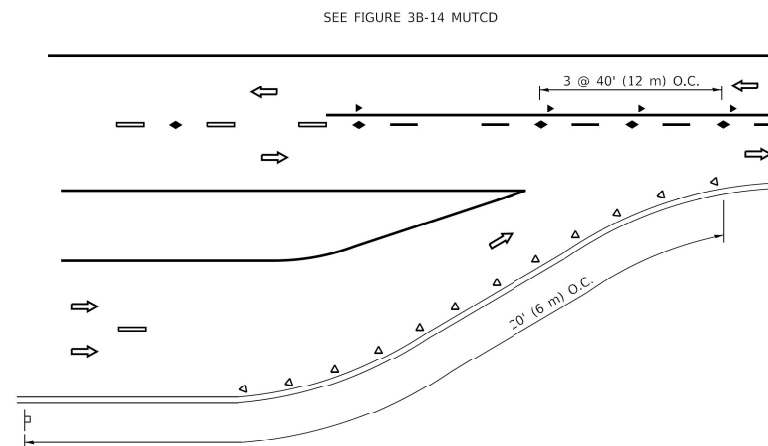
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CONTRACT NO. 62T30				
ILLINOIS FED. AID PROJECT				

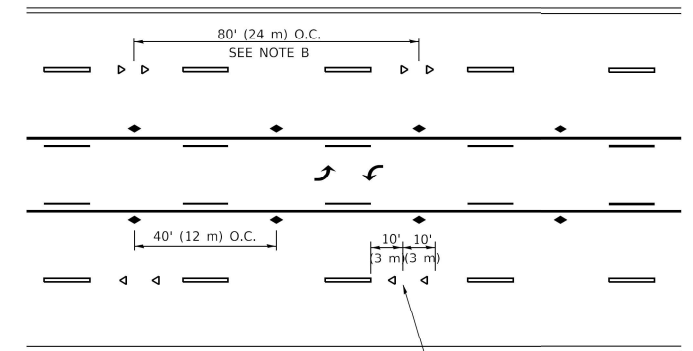


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

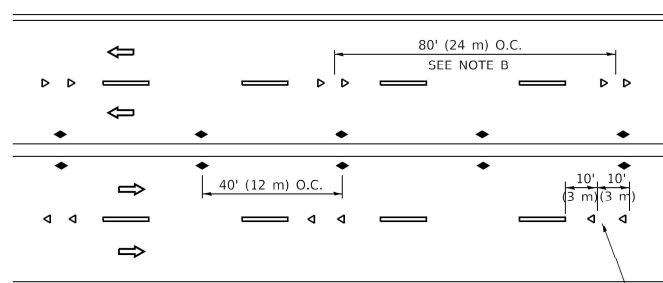
TWO-LANE/TWO-WAY



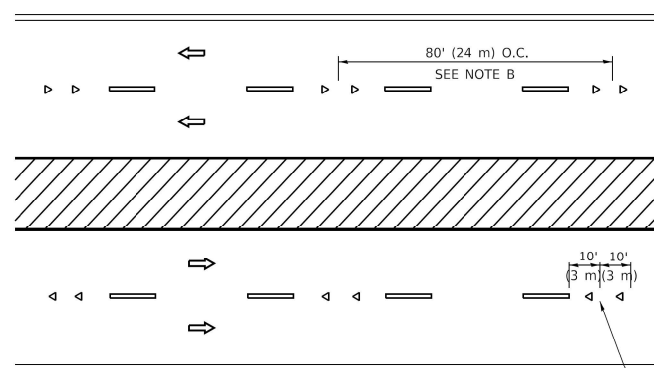
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

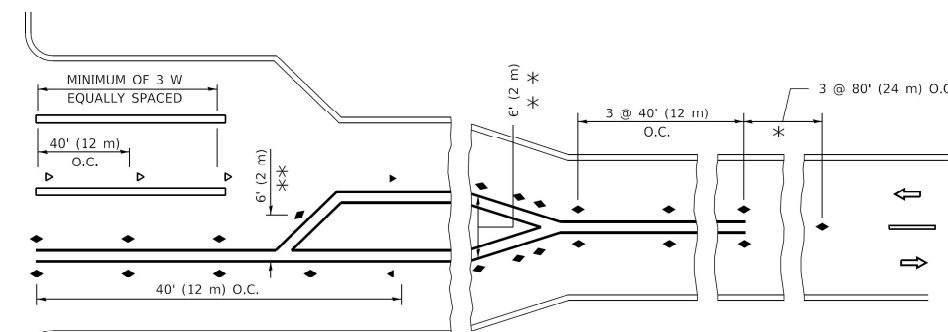
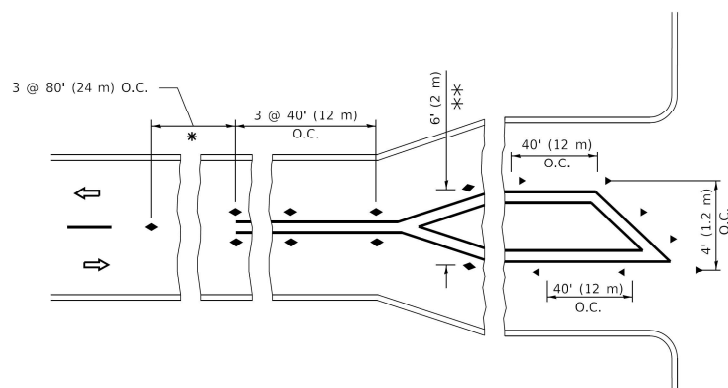
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 *** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL Path: \\... \PUB\206-002\1\WO_16_63\FIDUCIARY\CADD_Sheets\62130-51\F5-STANDARD\RD5-04.dgn

USER NAME = Alane	DESIGNED - CW	REVISED -
PLOT SCALE = 20,0000 * / in.	DRAWN - CW	REVISED -
DATE - 08/07/2024	CHECKED - ACL	REVISED -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ	DUPAGE	38	36
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62T30	

MODEL: P:\inf\11...
 FILE NAME: P:\22\22-4675-00_IDOT_Visibus Phase 2 (ITB)_206-002\1\W0 16_63T00DCNCADD_Sheets\62130-SIG-STD\DRD05-05.dwg

ROUTE MARKERS

FOR U.S. ROUTES
M1-40-2424

FOR ILLINOIS ROUTES
M1-50-2424

R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-2-2115

M6-3-2115

CARDINAL DIRECTION & DETOUR SIGNS

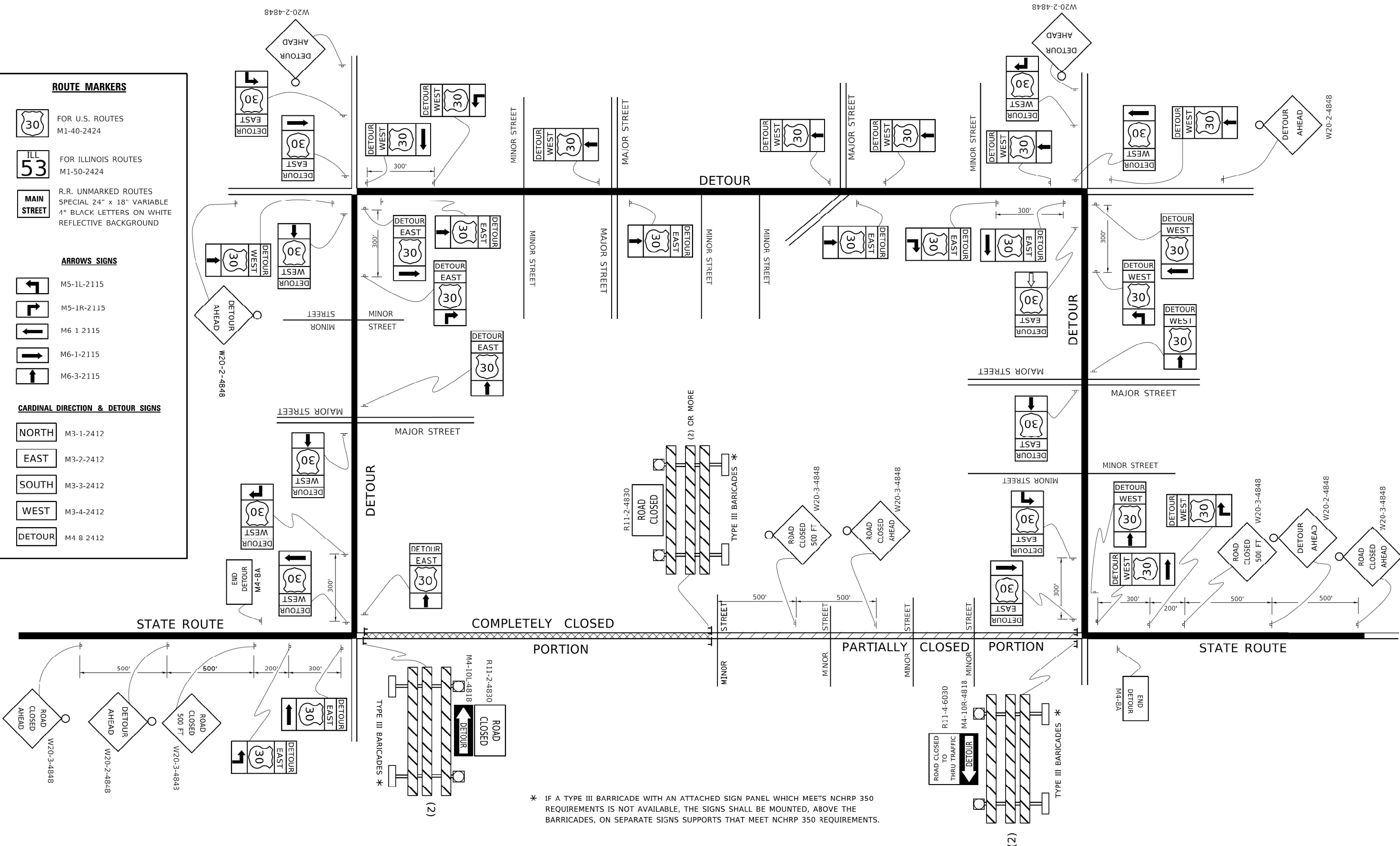
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-8-2412



* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

INFRASTRUCTURE ENGINEERING INCORPORATED
 1 South Wacker | Suite 2650 | Chicago, IL 60606
 P 312.425.9598 | F 312.425.9594 | www.infrastructure-eng.com

USER NAME = Alane	DESIGNED - CW	REVISED -
PLOT SCALE = 20,0000' */ in.	DRAWN - CW	REVISED -
DATE - 08/07/2024	CHECKED - ACL	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE - TYPICAL PAVEMENT MARKINGS (TC-13)

SCALE: STA. TO STA.

F.A.P. RTE. 870	SECTION FAP 0870 22 BJ	COUNTY DUPAGE	TOTAL SHEETS 38	SHEET NO. 37
CONTRACT NO. 62T30				
ILLINOIS FED. AID PROJECT				

