

11-08-2024 LETTING ITEM 111

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR LIST OF HIGHWAY STANDARDS SEE SHEET NO. 2

FUNCTIONAL CLASSIFICATION
LIBERTY ST - MAJOR COLLECTOR

TRAFFIC DATA
2018 ADT = 6,300

POSTED SPEED LIMIT
LIBERTY ST = 30-40 MPH

DESIGN SPEED LIMIT
LIBERTY ST = 30-40 MPH

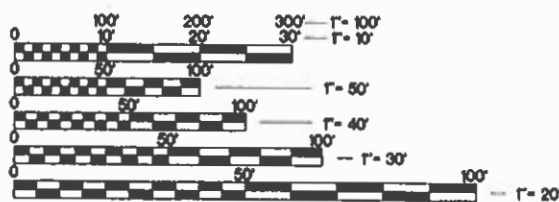
FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E., SCHAUMBURG, IL

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU ROUTE 3570 (LIBERTY ST.)
EOLA ROAD TO COMMONS DR.
ROADWAY RESURFACING
SECTION NO.: 23-00364-00-RS
PROJECT NO.: 56R5(585)
CITY OF AURORA
DUPAGE COUNTY

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3570	23-00364-00-RS	DUPAGE	25	1
		ILLINOIS	CONTRACT NO. 61K65	



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
811 OR 1-800-892-0123

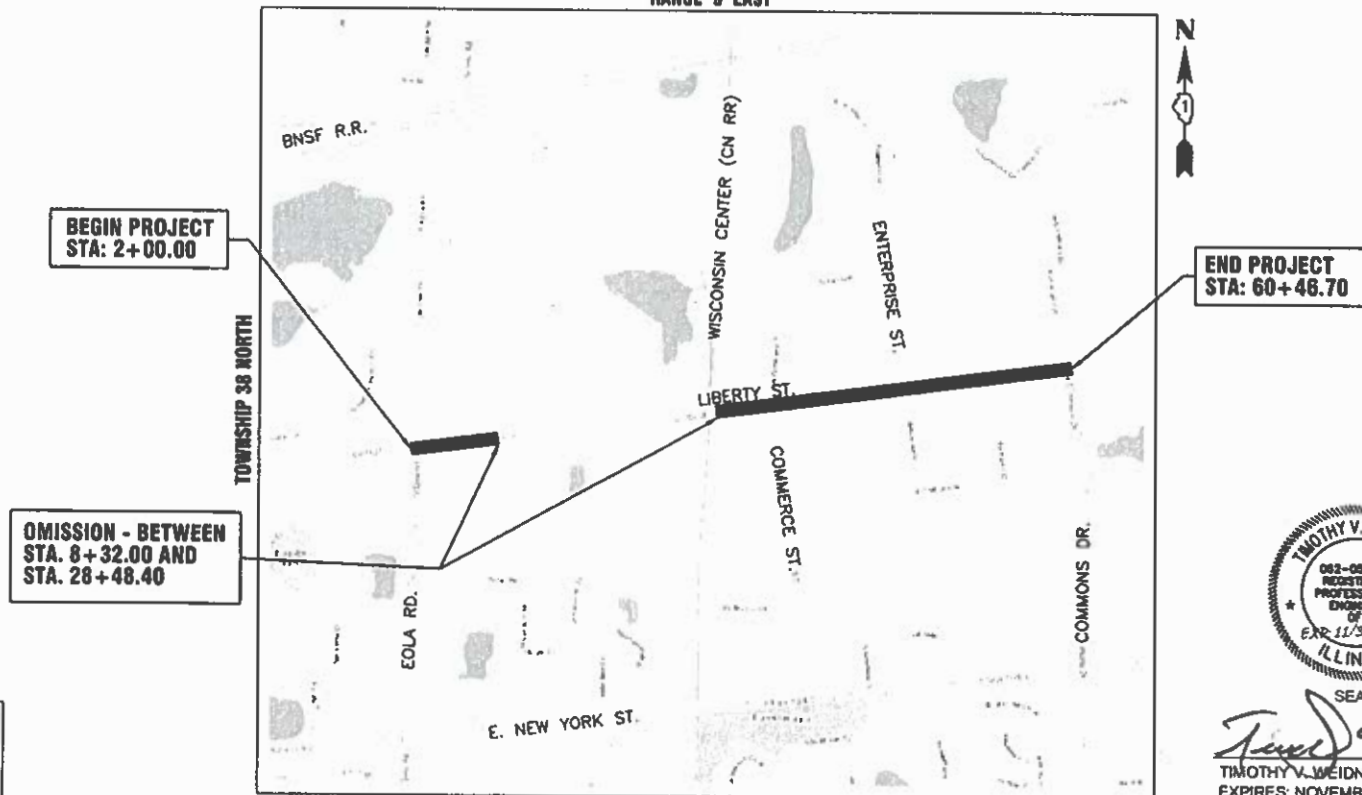
Dial 811 or 1-800-892-0123 JULIE DESIGN TICKET NUMBER: # X000760492



WITH THE FOLLOWING:
COUNTY DUPAGE
CITY-TOWNSHIP AURORA-AURORA TOWNSHIP
SEC. & 1/4 SEC. NO. 23-00364-00-RS-38 N.-B.E.
(2) Working Days before you dig
(Excluding Sat., Sun. & Holidays)

CONTRACT NO. 61K65

C-91-245-24
RANGE 9 EAST



DUPAGE COUNTY - NAPERVILLE TOWNSHIP
THIRD PRINCIPAL MERIDIAN

LOCATION MAP
NOT TO SCALE

GROSS LENGTH OF PROJECT = 5,846.70 FEET (1.107 MILES)
NET LENGTH OF PROJECT = 3,868.70 FEET (0.733 MILES)



SEAL
TIMOTHY V. WEIDNER, P.E.
EXPIRES: NOVEMBER 30, 2025

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED July 29, 2024
Timothy V. Weidner
CITY OF AURORA, ENGINEERING COORDINATOR

PASSED September 4, 2024
C.F. Richter / JAF
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW September 4, 2024
[Signature]
REGIONAL ENGINEER

PLANS PREPARED BY
THE CITY OF AURORA
PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, DETAILS, HIGHWAY STANDARDS AND GENERAL NOTES
3 - 5	SUMMARY OF QUANTITIES
6 - 7	TYPICAL SECTIONS
8 - 13	RESURFACING PLANS
14	EROSION CONTROL DETAILS
15 - 25	I.D.O.T. DISTRICT 1 DETAILS

ILLINOIS URBAN MANUAL EROSION CONTROL DETAILS

IUM-6545B	TEMPORARY CONCRETE WASHOUT FACILITY - STRAW BALE
IUM-561C	INLET PROTECTION - PAVED AREAS CURB PROTECTION
IUM-561D	INLET PROTECTION - PAVED AREAS DROP-IN PROTECTION

DISTRICT ONE DETAILS

BD-08	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
BD-24	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-16	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
TC-22	ARTERIAL ROAD INFORMATION SIGN
TS-05	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS
TS-07	DISTRICT ONE - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

HIGHWAY STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
420001-10	PAVEMENT JOINTS
442201-03	CLASS C AND D PATCHES
604001-05	FRAME AND LIDS TYPE 1
604051-04	FRAME AND LIDS TYPE 11
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5M) TO 24" (600 MM) FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701601-09	URBAN LANE CLOSURE MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-09	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKING
886001-01	DETECTOR LOOP INSTALLATIONS

GENERAL NOTES

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", JANUARY 1, 2022 AND SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, JANUARY 1, 2024.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS. IN ADDITION, THE CONTRACTOR MUST VERIFY THE ENGINEER'S LINE AND GRADE STAKES. IN THE EVENT OF A DISCREPANCY BETWEEN THE PLANS AND FIELD CONDITIONS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY THE IDENTIFIED DISCREPANCIES.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON CITY, STATE, OR PRIVATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
- THE STORAGE OF EQUIPMENT AND/OR MATERIALS WITHIN THE RIGHT-OF-WAY OF ANY STREET AND/OR PARK PROPERTY SHALL REQUIRE PRIOR APPROVAL OF THE ENGINEER.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
- SIDEWALK REMOVAL AND REPLACEMENT AND COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT AS SHOWN ON THE PLANS IS FOR INFORMATIONAL PURPOSES ONLY. ACTUAL LOCATIONS AND QUANTITIES ARE TO BE DETERMINED AND MARKED BY THE ENGINEER PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPLACEMENT OF ANY DETECTOR LOOPS DAMAGED DURING CONSTRUCTION.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS.
- THE CONTRACTOR SHALL VERIFY THAT ALL CRACKS, JOINTS, AND FLANGWAYS ARE CLEAN AND DRY PRIOR TO PLACEMENT OF MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS.
- THE CONTRACTOR SHALL MAINTAIN THE SITE IN A CLEAN AND ORDERLY MANNER. DEBRIS AND SURPLUS MATERIAL SHALL BE REMOVED AND RESTORATION SHALL PROCEED AS THE WORK PROCEEDS. IF THE ENGINEER SO DIRECTS, THE CONTRACTOR SHALL STOP ALL OTHER WORK AND CONCENTRATE ON CLEAN-UP AND RESTORATION. DEBRIS AND SURPLUS MATERIAL SHALL BE DISPOSED BY THE CONTRACTOR OFF-SITE.
- DRIVEWAY ENTRANCES WILL BE KEPT OPEN TO TRAFFIC AT ALL TIMES. THE CONTRACTOR WILL BE ALLOWED TO CLOSE A MAXIMUM OF HALF THE AREA OF ANY ONE ENTRANCE AT ANY TIME. IT IS ESSENTIAL THAT THE ENTRANCES REMAIN OPEN AND 'DRIVE-ABLE' FOR TWO-WAY TRAFFIC AT ALL TIMES. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING TRAFFIC CONTROL AND PROTECTION. WHERE NEW CURB AND GUTTER IS TO BE INSTALLED ACROSS A DRIVEWAY, IT IS EXPECTED THAT ONLY HALF OF THE DRIVEWAY ENTRANCE MAY BE REMOVED AND REPLACED AT ANY ONE TIME. ONLY AFTER PROPER CONCRETE CURE TIME HAS OCCURRED MAY THE CONTRACTOR BEGIN REMOVAL AND REPLACEMENT OPERATIONS ON THE REMAINING HALF OF THE CURB AND GUTTER. THE CONTRACTOR WILL NOT BE ALLOWED TO CLOSE A HALF OF DRIVEWAY ENTRANCE FOR MORE THAN 48 HOURS UNDER ANY CIRCUMSTANCE.
- CONTRACTOR SHALL TAKE PRECAUTION BY PRESERVING EXISTING TREES WITHIN THE RIGHT OF WAY. IF ANY DAMAGE OCCURS, TREES SHALL BE REPLACED IN KIND PER ARTICLE 201.07 REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL REQUIREMENTS STATED HEREIN
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1.5 INCHES WHERE THE SPEED IS 45 MPH OR LESS, WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.
- ALL NITROGEN, PHOSPHOROUS, AND POTASSIUM FERTILIZER NUTRIENTS HAVE BEEN INTENTIONALLY OMITTED FROM THE CONTRACT ON THE SEEDING OR SODDING APPLICATION.
- IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COORDINATE WITH THE WISCONSIN CENTRAL (CN) RAILROAD WHENEVER CONSTRUCTION ACTIVITY IS WITHIN 25 FEET OF THE RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY THE WISCONSIN CENTRAL (CN) RAILROAD TO MONITOR ON-COMING TRAIN TRAFFIC, AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD RIGHT-OF-WAY MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05.

UTILITIES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNER OF ALL EXISTING UTILITIES FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. ALL UTILITY COMPANIES AND THE CITY OF AURORA SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER.
- ONLY PRECAST CONCRETE ADJUSTMENT RINGS, MAXIMUM OF 12 INCHES IN HEIGHT, WILL BE ALLOWED IN THE ADJUSTMENT OF CATCH BASINS, MANHOLES, INLETS AND VALVE VAULT STRUCTURES. COMMON BRICK WILL NOT BE ALLOWED.
- THE CONTRACTOR SHALL ENSURE THAT ALL WATER SYSTEM VALVES (IN VALVE BOXES AND VALVE VAULTS) SHALL REMAIN READILY ACCESSIBLE TO THE CITY FOR EMERGENCY OPERATIONS AND NOT BURIED DURING CONSTRUCTION, UNLESS APPROVED BY THE ENGINEER. THE LOCATIONS OF ALL WATER FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES. FOR ALL OTHER STRUCTURES THAT NEED TO BE ADJUSTED THAT ARE NOT WATER, THE CONTRACTOR CAN CHOOSE TO ADJUST THEM ACCORDING TO BD-08. THE CONTRACTOR CAN ALSO CHOOSE TO BURY WATER STRUCTURES ACCORDING TO BD-08 WITH APPROVAL BY THE ENGINEER. THE CONTRACTOR WOULD NEED TO PROVIDE A DETAILED SCHEDULE OF MILLING, PAVING, AND ADJUSTMENT TIMELINES FOR THE ENGINEER'S REVIEW. APPROVAL BY THE ENGINEER IS NOT GUARANTEED AND WOULD BE CONTINGENT ON THE CONTRACTOR BURYING WATER STRUCTURES LAST AND ADJUSTING THEM FIRST. THE CONTRACTOR IS TO DETERMINE ANY PROTECTION OR RAMPING THAT IS REQUIRED AROUND STRUCTURES IF THEY ARE NOT BURIED. OPEN LID STORM MANHOLE STRUCTURES IN THE PAVEMENT MAY NOT BE ABLE TO BE ADJUSTED ACCORDING TO BD-08.
- THE INDISCRIMINATE USE OF FIRE HYDRANTS OR EXISTING STREAMS, CREEKS, WETLANDS OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN THEIR YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE ENGINEER PRIOR TO USE OF THE WATER.

SIGNING AND STRIPING

- ALL EXISTING SIGNS (INCLUDING THOSE LOCATED ON UTILITY/LIGHT POLES) THAT DO NOT CONFLICT WITH THE IMPROVEMENTS SHALL REMAIN IN PLACE UNLESS DIRECTED BY THE ENGINEER.
- SIGNS SHALL NOT BE MOVED OR COVERED UNTIL PROGRESS OF WORK NECESSITATES IT.
- SEE IDOT DISTRICT ONE DETAILS TC-13 (DISTRICT ONE TYPICAL PAVEMENT MARKINGS), AND TC-16 (SHORT TERM PAVEMENT MARKINGS LETTERS AND SYMBOLS) AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.
- GRINDING OF PAVEMENT MARKINGS ON NEWLY CONSTRUCTED HOT-MIX ASPHALT SHALL NOT BE PERMITTED.

COMMITMENTS

NONE

COMPANY NAME: PROJECT CONTACT: 8/19/2024 12:41 PM
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

INDEX OF SHEETS, DETAILS, HIGHWAY STANDARDS AND GENERAL NOTES

SCALE: N.T.S. SHEET NO. 01 OF 01 SHEETS STA. TO STA.

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3570	23-00364-00-RS	DUPAGE	25	2
CONTRACT NO. 61K65				
ILLINOIS FED. AID PROJECT				

CODE NUMBER	ITEM DESCRIPTION	UNIT	QUANTITY
			ROADWAY 80 % FEDERAL 20 % LOCAL 0005
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	193
25200110	SODDING, SALT TOLERANT	SQ YD	193
25200200	SUPPLEMENTAL WATERING	UNIT	10.5
31101000	SUBBASE GRANULAR MATERIAL, TYPE B	TON	65
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	15183
40600370	LONGITUDINAL JOINT SEALANT	FOOT	8206
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	9.0
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	186
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	1238
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	2520
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1750
42400800	DETECTABLE WARNINGS	SQ FT	210
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SQ YD	22492
44000600	SIDEWALK REMOVAL	SQ FT	1750
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	110
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	220

‡ INDICATES SPECIALTY ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET NO. 01 OF 03 SHEETS STA. TO STA.

FAU RT#	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3570	23-00364-00-RS	DUPAGE	25	3
CONTRACT NO. 61K65				
ILLINOIS FED. AID PROJECT				

CODE NUMBER	ITEM DESCRIPTION	UNIT	QUANTITY	
			ROADWAY 80 % FEDERAL 20 % LOCAL	0005
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	450	
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	340	
60260500	INLETS TO BE ADJUSTED WITH NEW TYPE 3 FRAME AND GRATE	EACH	1	
67100100	MOBILIZATION	L SUM	1	
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	4632	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1544	
t 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	341	
t 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4897	
t 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	920	
t 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	791	
t 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	149	
t 78011000	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS	SQ FT	341	

t INDICATES SPECIALTY ITEM

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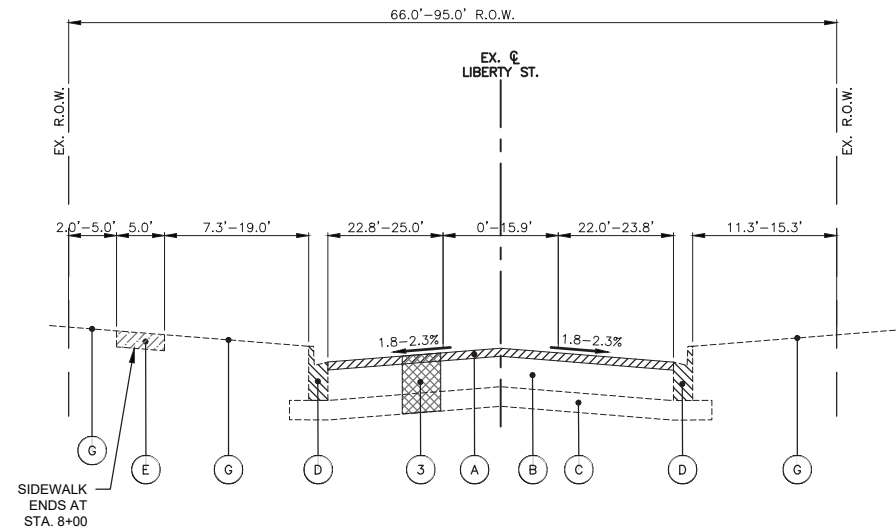
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

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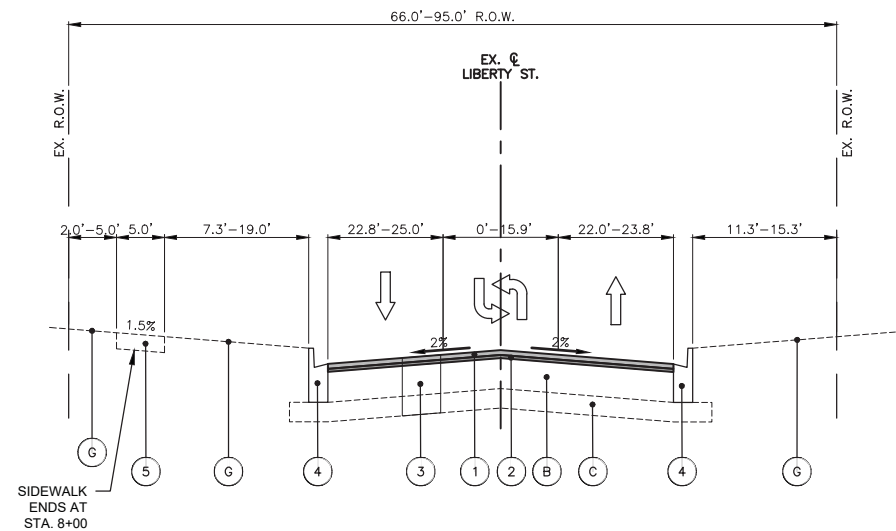
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3570	23-00364-00-RS	DUPAGE	25	4
CONTRACT NO. 61K65				
ILLINOIS FED. AID PROJECT				



EXISTING TYPICAL SECTION - LIBERTY ST.

STA: 2+00.0 - STA: 8+32.0

OMISSION BETWEEN STA. 8+32.0 AND 28+48.4



PROPOSED TYPICAL SECTION - LIBERTY ST.

STA: 2+00.0 - STA: 8+32.0

OMISSION BETWEEN STA. 8+32.0 AND 28+48.4

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

OPERATION	MIXTURE TYPE	AIR VOIDS @ NDES	QMP
PAVEMENT RESURFACING	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 2"	4% @ 70 GYR.	LR1030-2
	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 1"	3.5% @ 50 GYR.	LR1030-2
CLASS D PATCHES	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 8"	4% @ 70 GYR.	LR1030-2
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA) PER LR1030-2			

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE P HMA BC IL-4.75 N50.

EXISTING LEGEND

- A HOT-MIX ASPHALT SURFACE REMOVAL, 3.0"
- B EXISTING HMA PAVEMENT, 11"
- C AGGREGATE SUBBASE, 4.0"
- D COMBINATION CONCRETE CURB & GUTTER, VARIES FROM TYPE B-6.12 TO B-6.24
- E PORTLAND CEMENT CONCRETE SIDEWALK, 5.0"
- F LANDSCAPED OR PCC MEDIAN
- G EXISTING GROUND

PROPOSED LEGEND

- 1 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 2"
- 2 POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 1"
- 3 CLASS D PATCHES, WITH 4" SUBBASE GRANULAR MATERIAL AS NEEDED, 8" (SPOT REMOVAL & REPLACEMENT AS DIRECTED BY THE ENGINEER)
- 4 COMBINATION CONCRETE CURB & GUTTER, VARIES FROM TYPE B-6.12 TO B-6.24 (SPOT REMOVAL & REPLACEMENT AS DIRECTED BY THE ENGINEER)
- 5 PCC SIDEWALK, 5" (SPOT REMOVAL & REPLACEMENT AS DIRECTED BY THE ENGINEER)

NOTES:

- THE PATCHING QUANTITIES HAVE BEEN ESTIMATED BASED ON FIELD OBSERVATIONS DURING DESIGN. ACTUAL QUANTITIES TO BE DETERMINED DURING CONSTRUCTION BY THE ENGINEER.
- NO PROPOSED WORK WILL TAKE PLACE OUTSIDE OF THE EXISTING R.O.W.
- OMISSION BETWEEN STA. 8+32.0 AND 28+48.4

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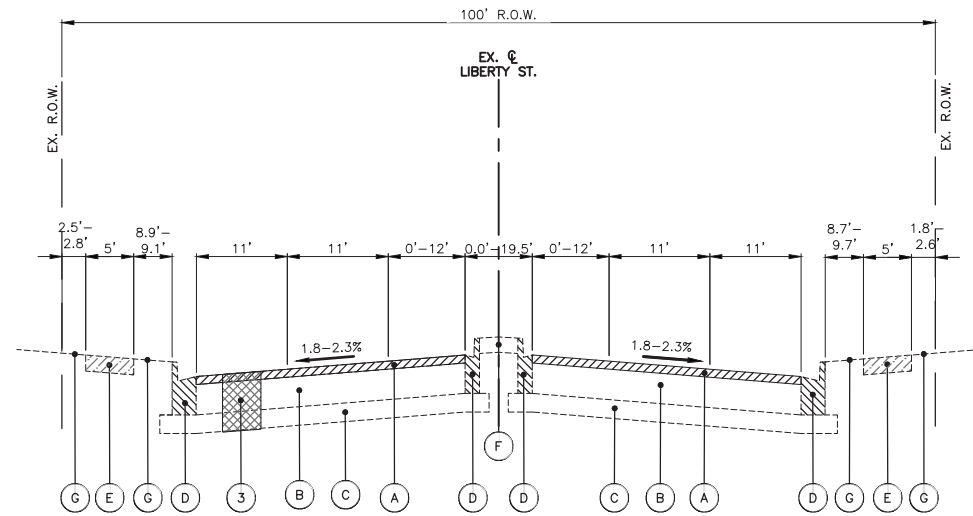
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**LIBERTY STREET
TYPICAL SECTIONS**

SCALE: N.T.S. SHEET NO. 01 OF 02 SHEETS STA. TO STA.

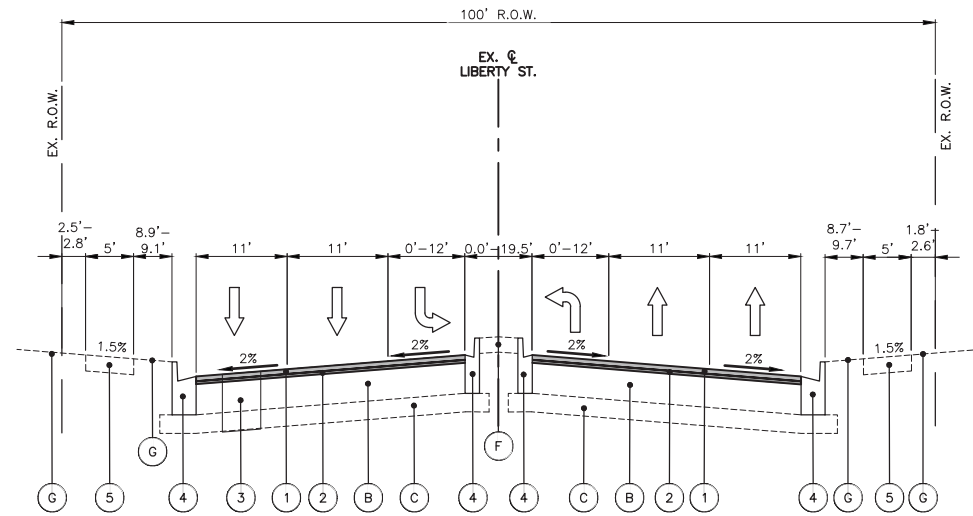
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3570	23-00364-00-RS	DUPAGE	25	6
CONTRACT NO. 61K65				
ILLINOIS FED. AID PROJECT				



EXISTING TYPICAL SECTION - LIBERTY ST.

STA: 28+48.4 - STA: 60+46.7

OMISSION BETWEEN STA. 8+32.0 AND 28+48.4



PROPOSED TYPICAL SECTION - LIBERTY ST.

STA: 28+48.4 - STA: 60+46.7

OMISSION BETWEEN STA. 8+32.0 AND 28+48.4

EXISTING LEGEND

- (A) HOT-MIX ASPHALT SURFACE REMOVAL, 3.0"
- (B) EXISTING HMA PAVEMENT, 11"
- (C) AGGREGATE SUBBASE, 4.0"
- (D) COMBINATION CONCRETE CURB & GUTTER, VARIES FROM TYPE B-6.12 TO B-6.24
- (E) PORTLAND CEMENT CONCRETE SIDEWALK TO REMAIN IN PLACE, 5.0"
- (F) LANDSCAPED OR PCC MEDIAN
- (G) EXISTING GROUND

NOTES:

- THE PATCHING QUANTITIES HAVE BEEN ESTIMATED BASED ON FIELD OBSERVATIONS DURING DESIGN. ACTUAL QUANTITIES TO BE DETERMINED DURING CONSTRUCTION BY THE ENGINEER.
- NO PROPOSED WORK WILL TAKE PLACE OUTSIDE OF THE EXISTING R.O.W.

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 2"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 1"
- (3) CLASS D PATCHES, WITH 4" SUBBASE GRANULAR MATERIAL AS NEEDED, 8" (SPOT REMOVAL & REPLACEMENT AS DIRECTED BY THE ENGINEER)
- (4) COMBINATION CONCRETE CURB & GUTTER, VARIES FROM TYPE B-6.12 TO B-6.24 (SPOT REMOVAL & REPLACEMENT AS DIRECTED BY THE ENGINEER)
- (5) TOPSOIL FURNISH AND PLACE, 4" AND SODDING, SALT TOLERANT (ONLY IN AREAS OF LANDSCAPE RESTORATION AS DIRECTED BY THE ENGINEER)

COMPANY NAME:
 PROJECT CONTACT:
 DATE PLOTTED: 8/19/2024 12:41 PM
 CLIENT: Liberty St-Typ Sections
 FILE NAME: Liberty St-Typ Sections
 PLOT DRIVER: DWG To PDF.pc3
 PEN TABLE: ILDOT-Standard.ctb

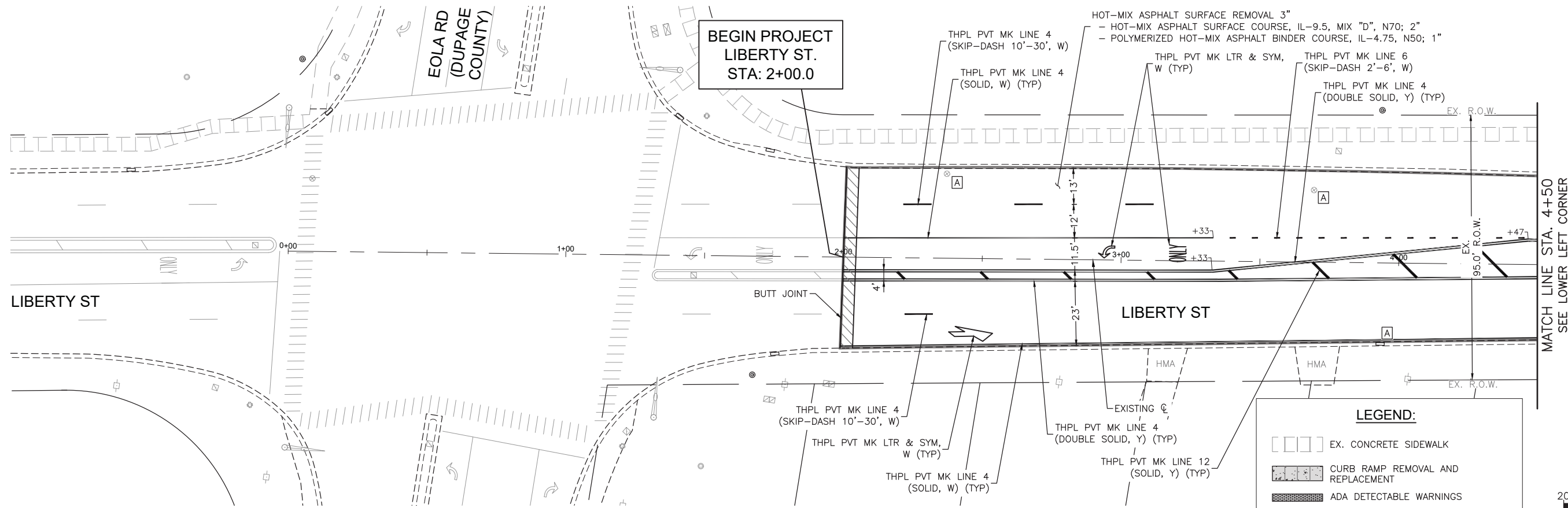
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PLOT DATE = 8/19/2024	DATE - 03/01/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**LIBERTY STREET
TYPICAL SECTIONS**

SCALE: N.T.S. SHEET NO. 02 OF 02 SHEETS STA. TO STA.

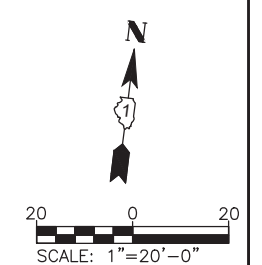
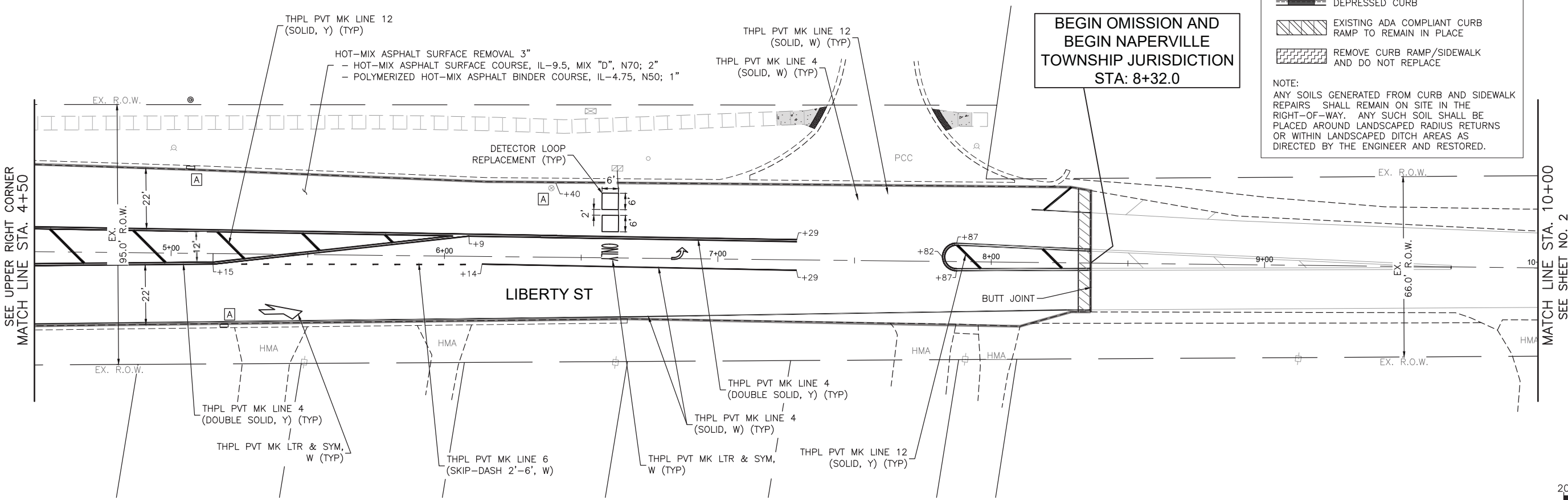
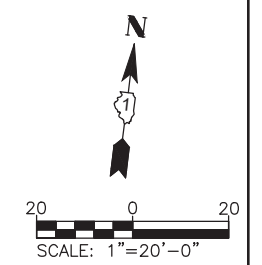
FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3570	23-00364-00-RS	DUPAGE	25	7
CONTRACT NO. 61K65				
ILLINOIS FED. AID PROJECT				



LEGEND:

- EX. CONCRETE SIDEWALK
- CURB RAMP REMOVAL AND REPLACEMENT
- ADA DETECTABLE WARNINGS
- CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT
- DEPRESSED CURB
- EXISTING ADA COMPLIANT CURB RAMP TO REMAIN IN PLACE
- REMOVE CURB RAMP/SIDEWALK AND DO NOT REPLACE

NOTE:
 ANY SOILS GENERATED FROM CURB AND SIDEWALK REPAIRS SHALL REMAIN ON SITE IN THE RIGHT-OF-WAY. ANY SUCH SOIL SHALL BE PLACED AROUND LANDSCAPED RADIUS RETURNS OR WITHIN LANDSCAPED DITCH AREAS AS DIRECTED BY THE ENGINEER AND RESTORED.



COMPANY NAME: [REDACTED]
 PROJECT CONTACT: [REDACTED]
 CLIENT: [REDACTED]
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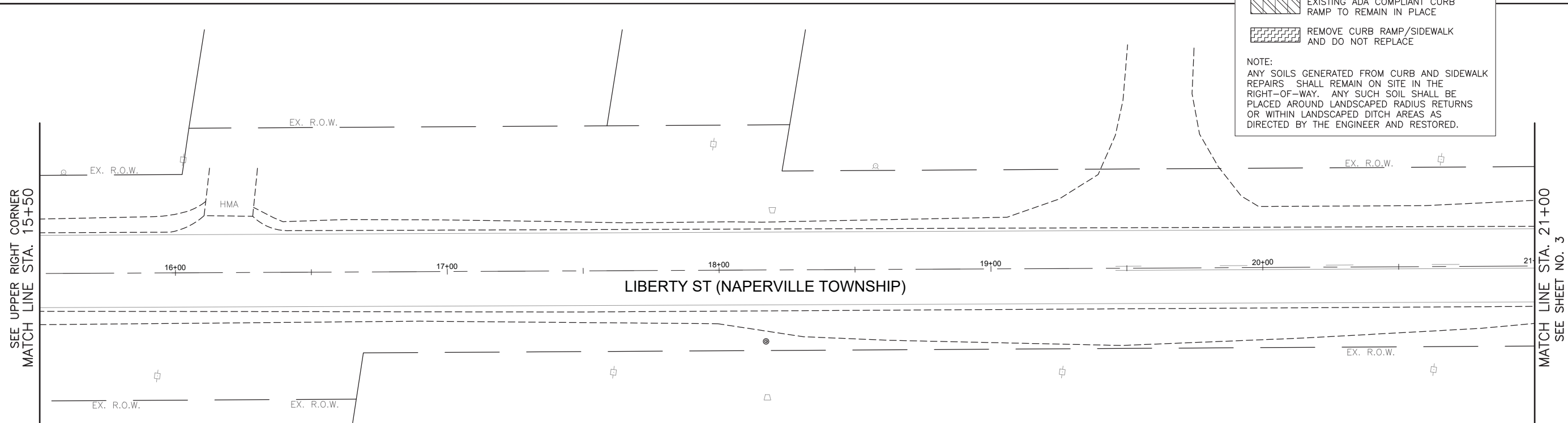
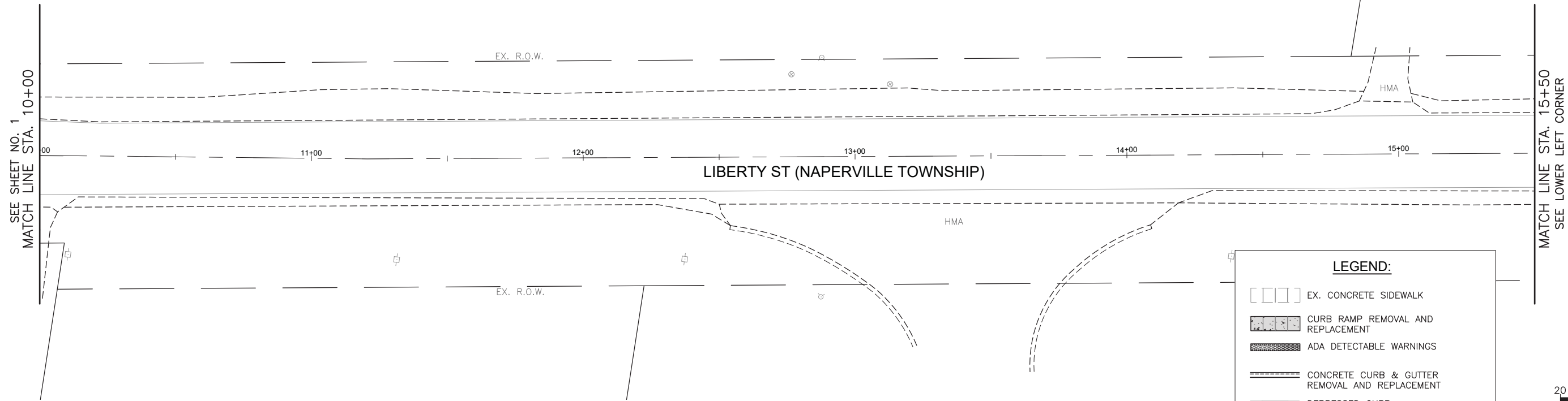
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PLOT DATE = 8/19/2024	DATE - 03/01/2024	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LIBERTY STREET
RESURFACING AND PAVEMENT MARKING PLAN

SCALE: 1"=20' SHEET NO. 1 OF 6 SHEETS STA. 2+00 TO STA. 10+00

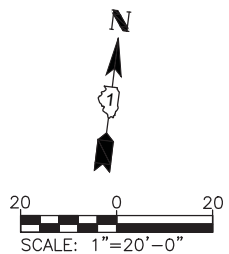
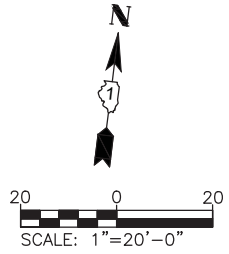
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3570	23-00364-00-RS	DUPAGE	25	8
CONTRACT NO. 61K65				
ILLINOIS FED. AID PROJECT				



LEGEND:

- EX. CONCRETE SIDEWALK
- CURB RAMP REMOVAL AND REPLACEMENT
- ADA DETECTABLE WARNINGS
- CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT
- DEPRESSED CURB
- EXISTING ADA COMPLIANT CURB RAMP TO REMAIN IN PLACE
- REMOVE CURB RAMP/SIDEWALK AND DO NOT REPLACE

NOTE:
 ANY SOILS GENERATED FROM CURB AND SIDEWALK REPAIRS SHALL REMAIN ON SITE IN THE RIGHT-OF-WAY. ANY SUCH SOIL SHALL BE PLACED AROUND LANDSCAPED RADIUS RETURNS OR WITHIN LANDSCAPED DITCH AREAS AS DIRECTED BY THE ENGINEER AND RESTORED.



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PLOT DATE = 8/19/2024	DATE - 03/01/2024	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**LIBERTY STREET
 RESURFACING AND PAVEMENT MARKING PLAN**

SCALE: 1"=20' SHEET NO. 2 OF 6 SHEETS STA. 10+10 TO STA. 21+00

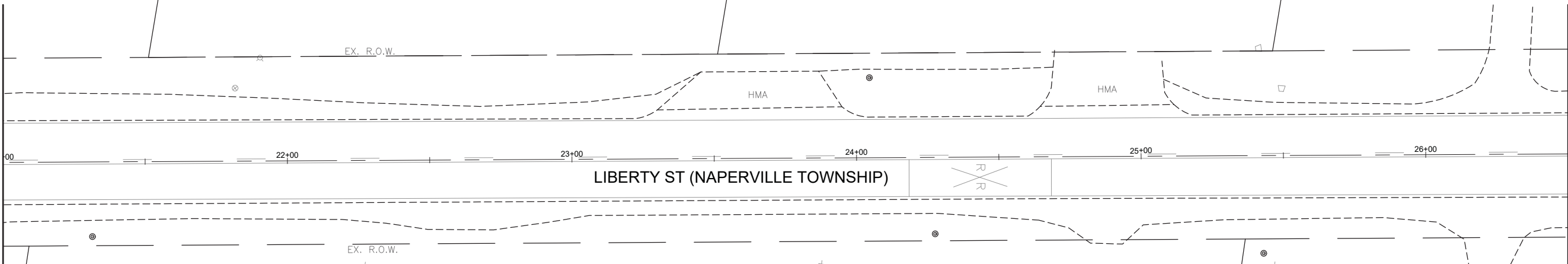
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3570	23-00364-00-RS	DUPAGE	25	9
CONTRACT NO. 61K65				
ILLINOIS FED. AID PROJECT				

SEE SHEET NO. 2
MATCH LINE STA. 21+00

MATCH LINE STA. 26+50
SEE LOWER LEFT CORNER

SEE UPPER RIGHT CORNER
MATCH LINE STA. 26+50

MATCH LINE STA. 32+00
SEE SHEET NO. 4

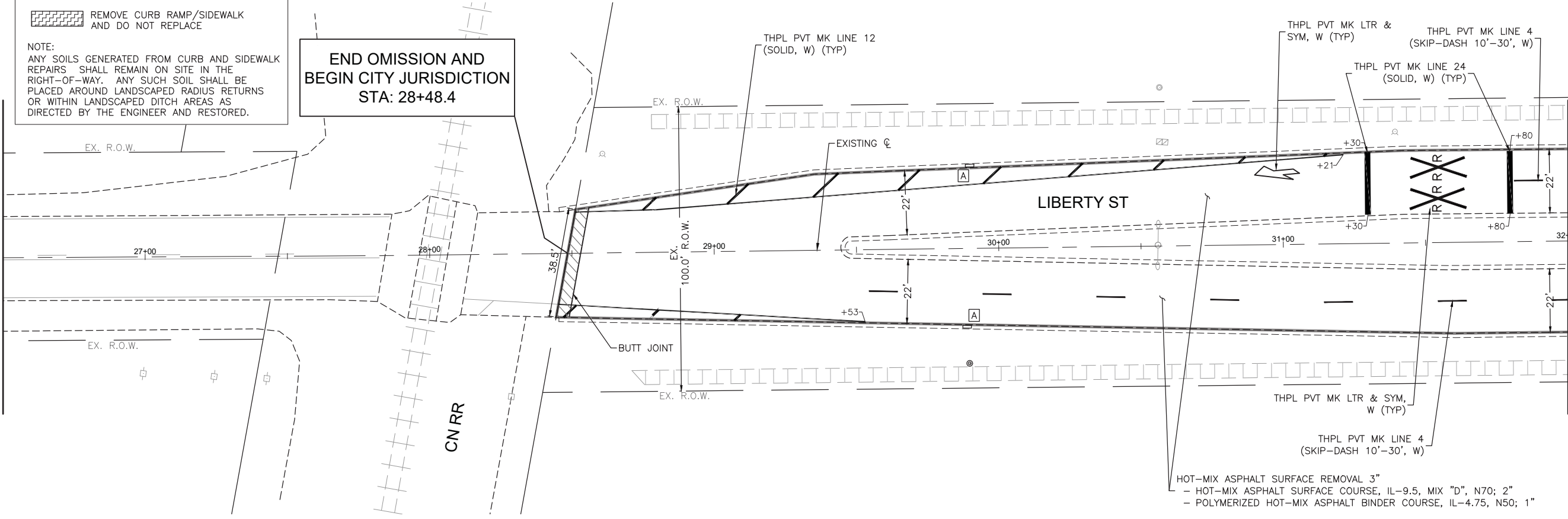


LEGEND:

- EX. CONCRETE SIDEWALK
- CURB RAMP REMOVAL AND REPLACEMENT
- ADA DETECTABLE WARNINGS
- CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT
- DEPRESSED CURB
- EXISTING ADA COMPLIANT CURB RAMP TO REMAIN IN PLACE
- REMOVE CURB RAMP/SIDEWALK AND DO NOT REPLACE

NOTE:
ANY SOILS GENERATED FROM CURB AND SIDEWALK REPAIRS SHALL REMAIN ON SITE IN THE RIGHT-OF-WAY. ANY SUCH SOIL SHALL BE PLACED AROUND LANDSCAPED RADIUS RETURNS OR WITHIN LANDSCAPED DITCH AREAS AS DIRECTED BY THE ENGINEER AND RESTORED.

**END OMISSION AND
BEGIN CITY JURISDICTION
STA: 28+48.4**



HOT-MIX ASPHALT SURFACE REMOVAL 3"
- HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70; 2"
- POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 1"

COMPANY NAME: [REDACTED]
PROJECT CONTACT: [REDACTED]
CLIENT: [REDACTED]
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FILE NAME: Liberty St_Resurf Plans_Ph 2
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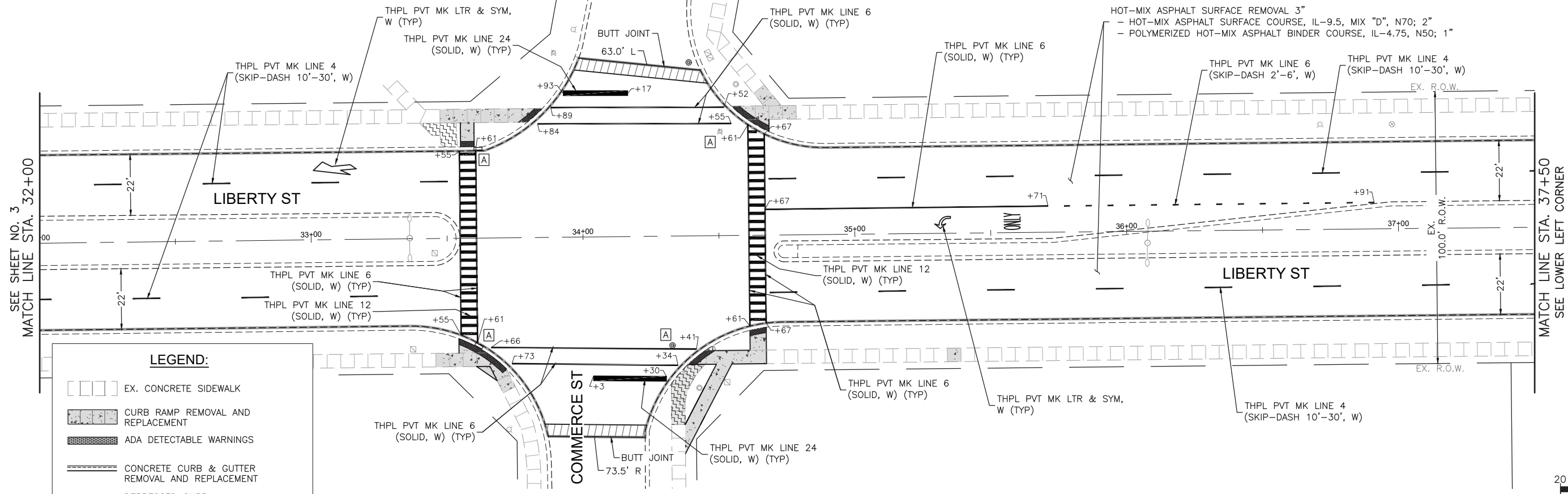
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PLOT DATE = 8/19/2024	DATE - 03/01/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**LIBERTY STREET
RESURFACING AND PAVEMENT MARKING PLAN**

SCALE: 1"=20' SHEET NO. 3 OF 6 SHEETS STA. 21+00 TO STA. 32+00

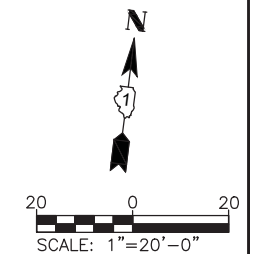
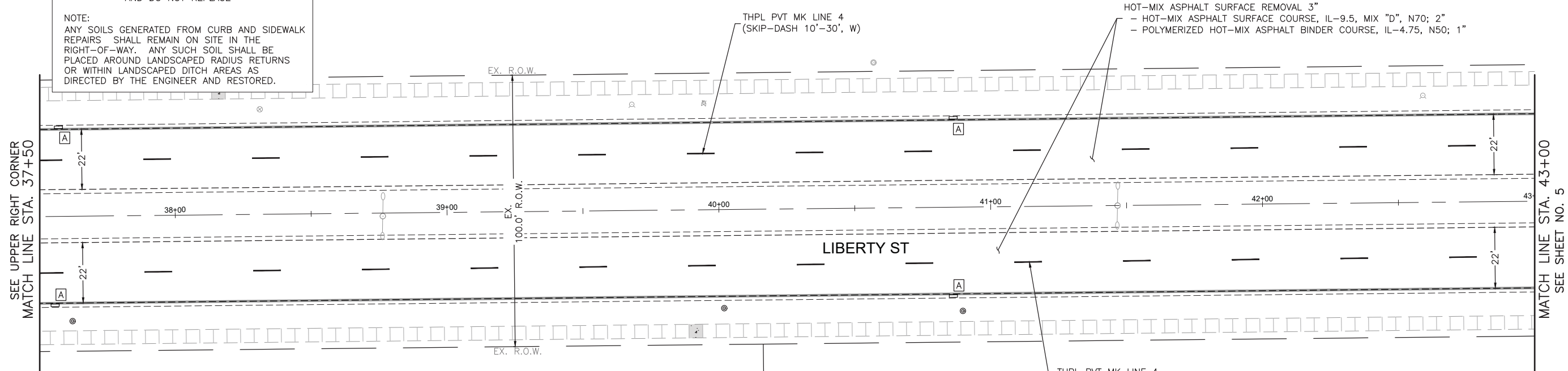
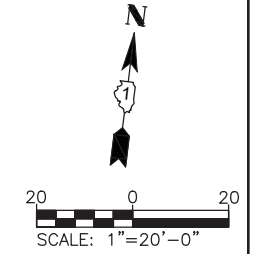
FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3570	23-00364-00-RS	DUPAGE	25	10
CONTRACT NO. 61K65				
ILLINOIS FED. AID PROJECT				



LEGEND:

- EX. CONCRETE SIDEWALK
- CURB RAMP REMOVAL AND REPLACEMENT
- ADA DETECTABLE WARNINGS
- CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT
- DEPRESSED CURB
- EXISTING ADA COMPLIANT CURB RAMP TO REMAIN IN PLACE
- REMOVE CURB RAMP/SIDEWALK AND DO NOT REPLACE

NOTE:
 ANY SOILS GENERATED FROM CURB AND SIDEWALK REPAIRS SHALL REMAIN ON SITE IN THE RIGHT-OF-WAY. ANY SUCH SOIL SHALL BE PLACED AROUND LANDSCAPED RADIUS RETURNS OR WITHIN LANDSCAPED DITCH AREAS AS DIRECTED BY THE ENGINEER AND RESTORED.



COMPANY NAME:
 PROJECT CONTACT:
 CLIENT:
 DATE PLOTTED: 8/19/2024 12:41 PM
 FILE NAME: Liberty St_Resurf Plans_Ph 2
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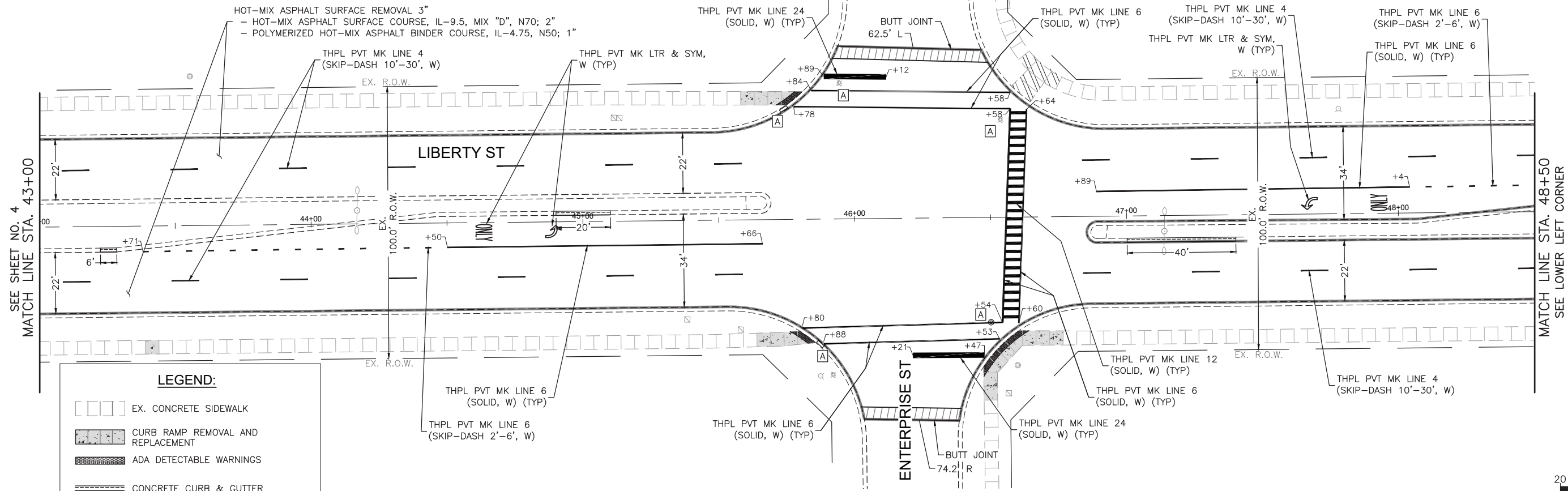
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PLOT DATE = 8/19/2024	DATE - 03/01/2024	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**LIBERTY STREET
 RESURFACING AND PAVEMENT MARKING PLAN**

SCALE: 1"=20' SHEET NO. 4 OF 6 SHEETS STA. 32+00 TO STA. 43+00

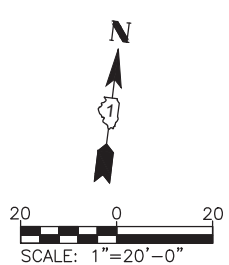
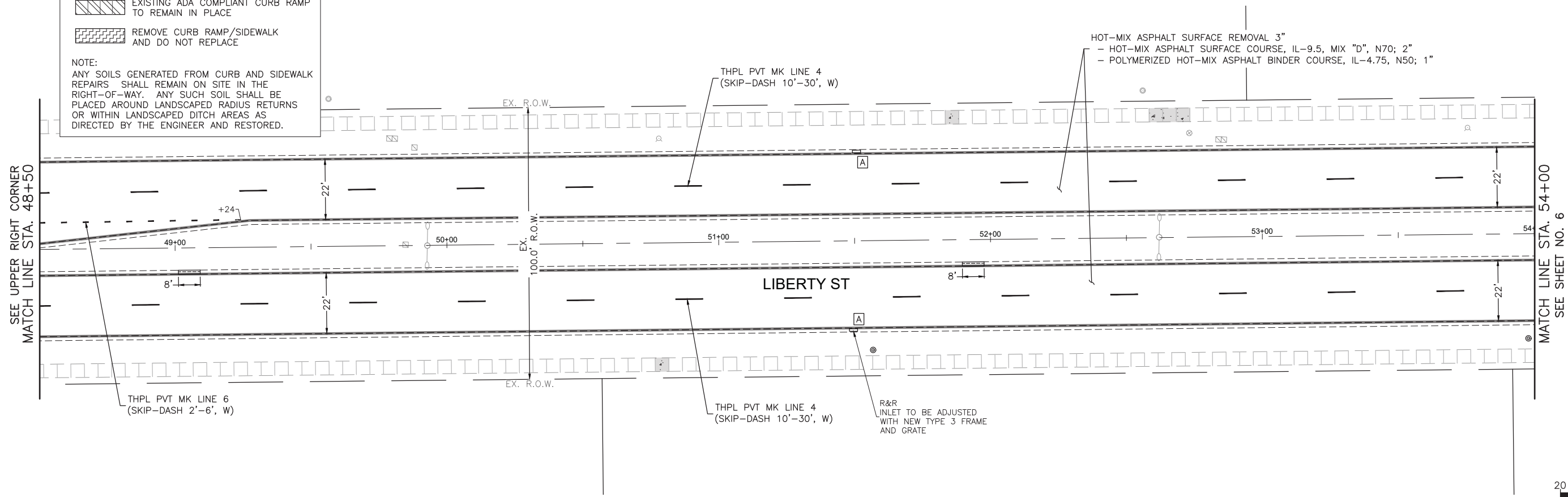
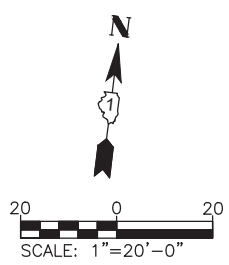
FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3570	23-00364-00-RS	DUPAGE	25	11
CONTRACT NO. 61K65				
ILLINOIS FED. AID PROJECT				



LEGEND:

- [] EX. CONCRETE SIDEWALK
- [] CURB RAMP REMOVAL AND REPLACEMENT
- [] ADA DETECTABLE WARNINGS
- [] CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT
- [] DEPRESSED CURB
- [] EXISTING ADA COMPLIANT CURB RAMP TO REMAIN IN PLACE
- [] REMOVE CURB RAMP/SIDEWALK AND DO NOT REPLACE

NOTE:
 ANY SOILS GENERATED FROM CURB AND SIDEWALK REPAIRS SHALL REMAIN ON SITE IN THE RIGHT-OF-WAY. ANY SUCH SOIL SHALL BE PLACED AROUND LANDSCAPED RADIUS RETURNS OR WITHIN LANDSCAPED DITCH AREAS AS DIRECTED BY THE ENGINEER AND RESTORED.



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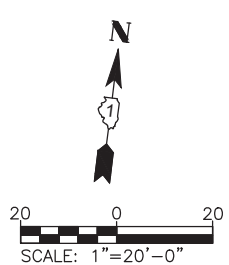
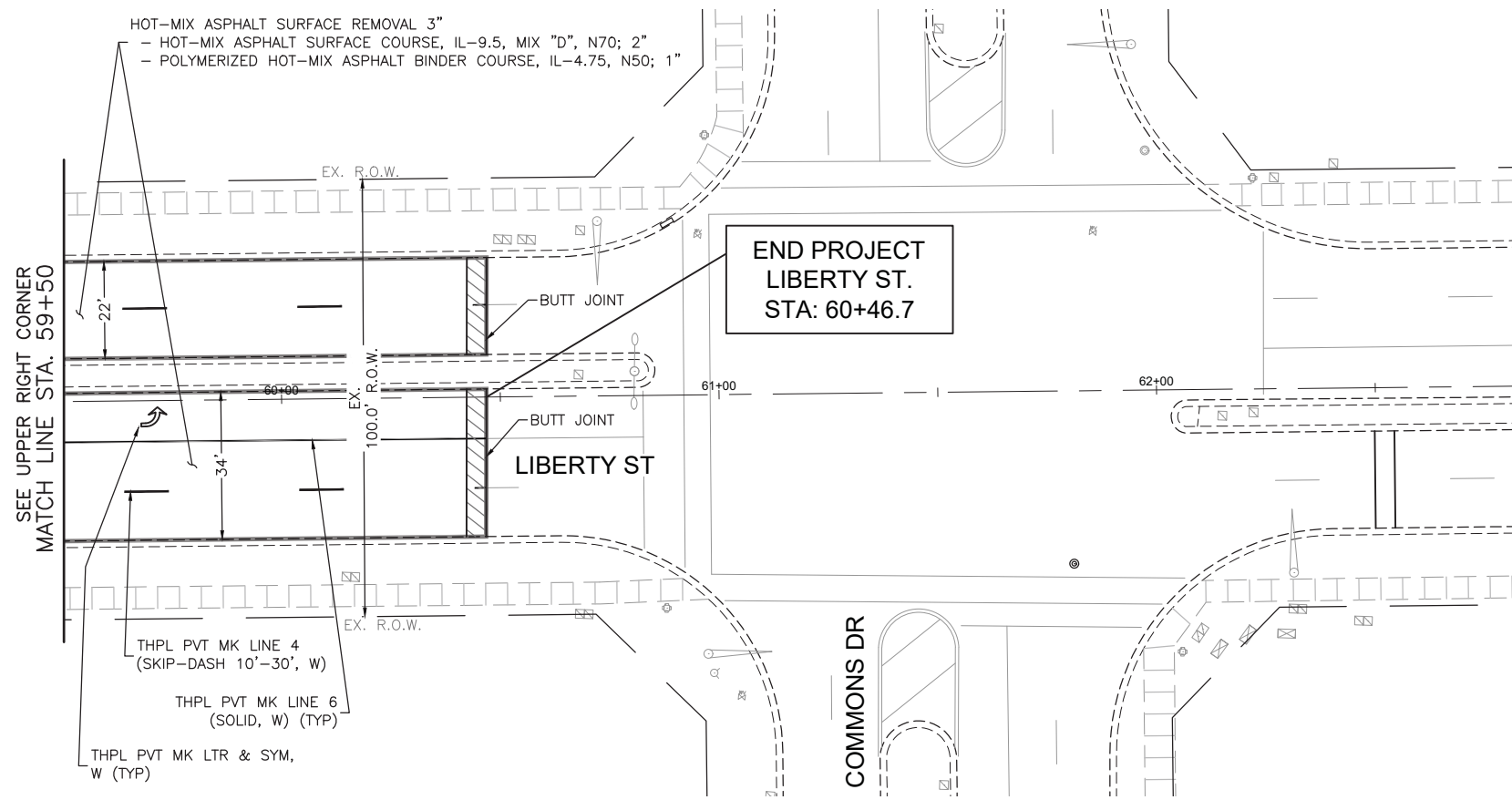
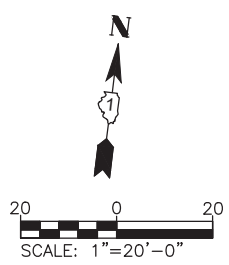
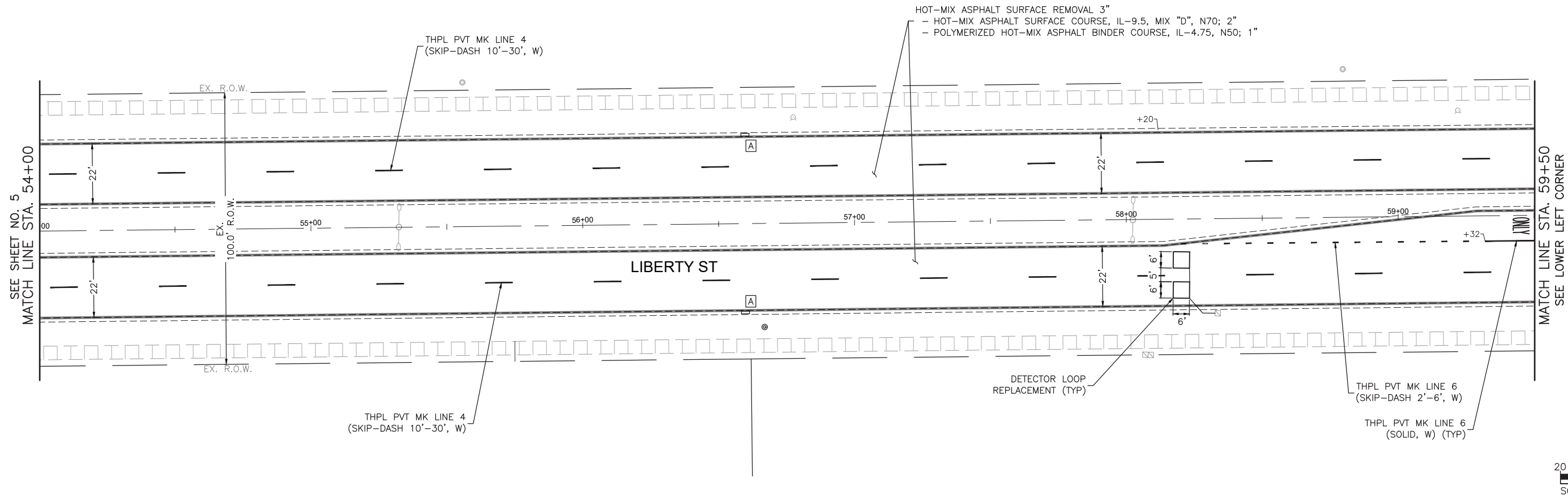
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PLOT DATE = 8/19/2024	DATE - 03/01/2024	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**LIBERTY STREET
 RESURFACING AND PAVEMENT MARKING PLAN**

SCALE: 1"=20' SHEET NO. 5 OF 6 SHEETS STA. 43+00 TO STA. 54+00

FAU RTE. 3570	SECTION 23-00364-00-RS	COUNTY DUPAGE	TOTAL SHEETS 25	SHEET NO. 12
CONTRACT NO. 61K65				
ILLINOIS FED. AID PROJECT				



LEGEND:

- EX. CONCRETE SIDEWALK
- CURB RAMP REMOVAL AND REPLACEMENT
- ADA DETECTABLE WARNINGS
- CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT
- DEPRESSED CURB
- EXISTING ADA COMPLIANT CURB RAMP TO REMAIN IN PLACE
- REMOVE CURB RAMP/SIDEWALK AND DO NOT REPLACE

NOTE:
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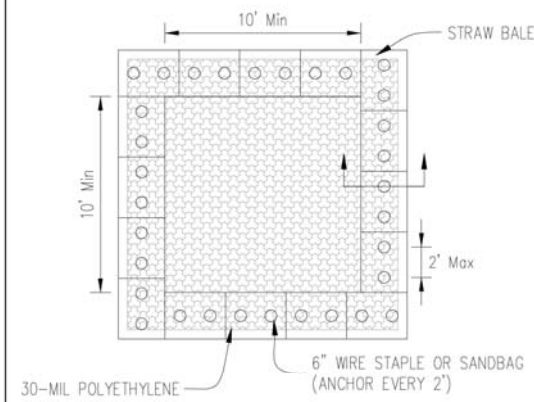
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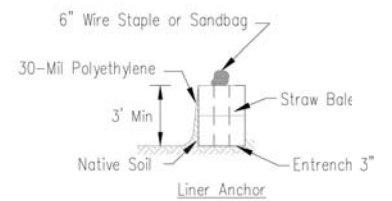
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

LIBERTY STREET RESURFACING AND PAVEMENT MARKING PLAN	
FAU RTE. 3570	SECTION 23-00364-00-RS
COUNTY DUPAGE	TOTAL SHEETS 25
SHEET NO. 6 OF 6 SHEETS STA. 54+00 TO STA. 64+50	

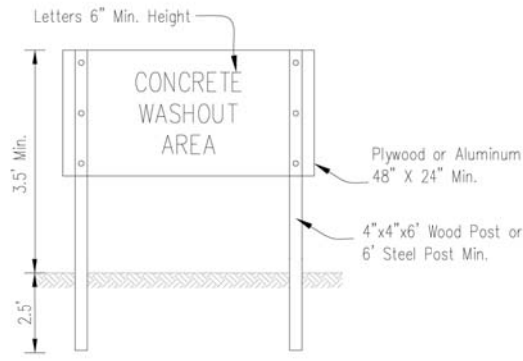
CONTRACT NO. 61K65	
ILLINOIS	FED. AID PROJECT



PLAN VIEW



STRAW BALE ANCHOR SECTIONS

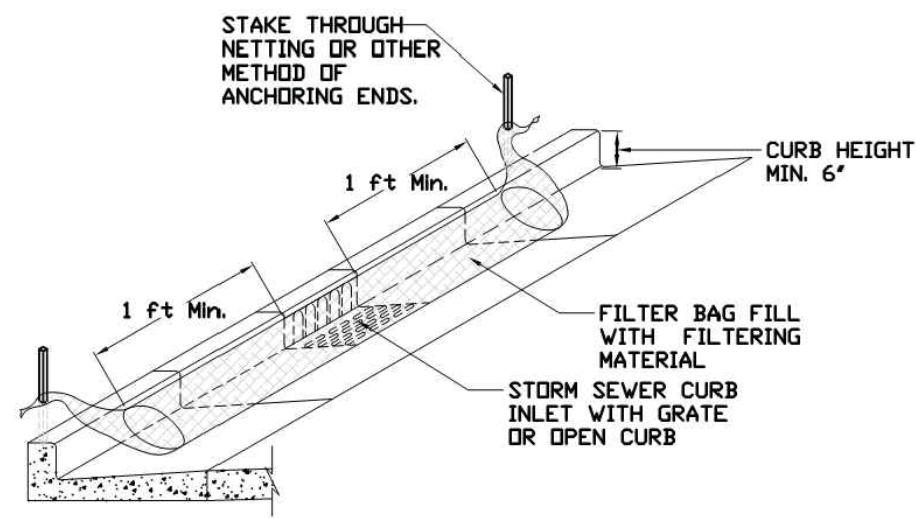


SIGN DETAIL

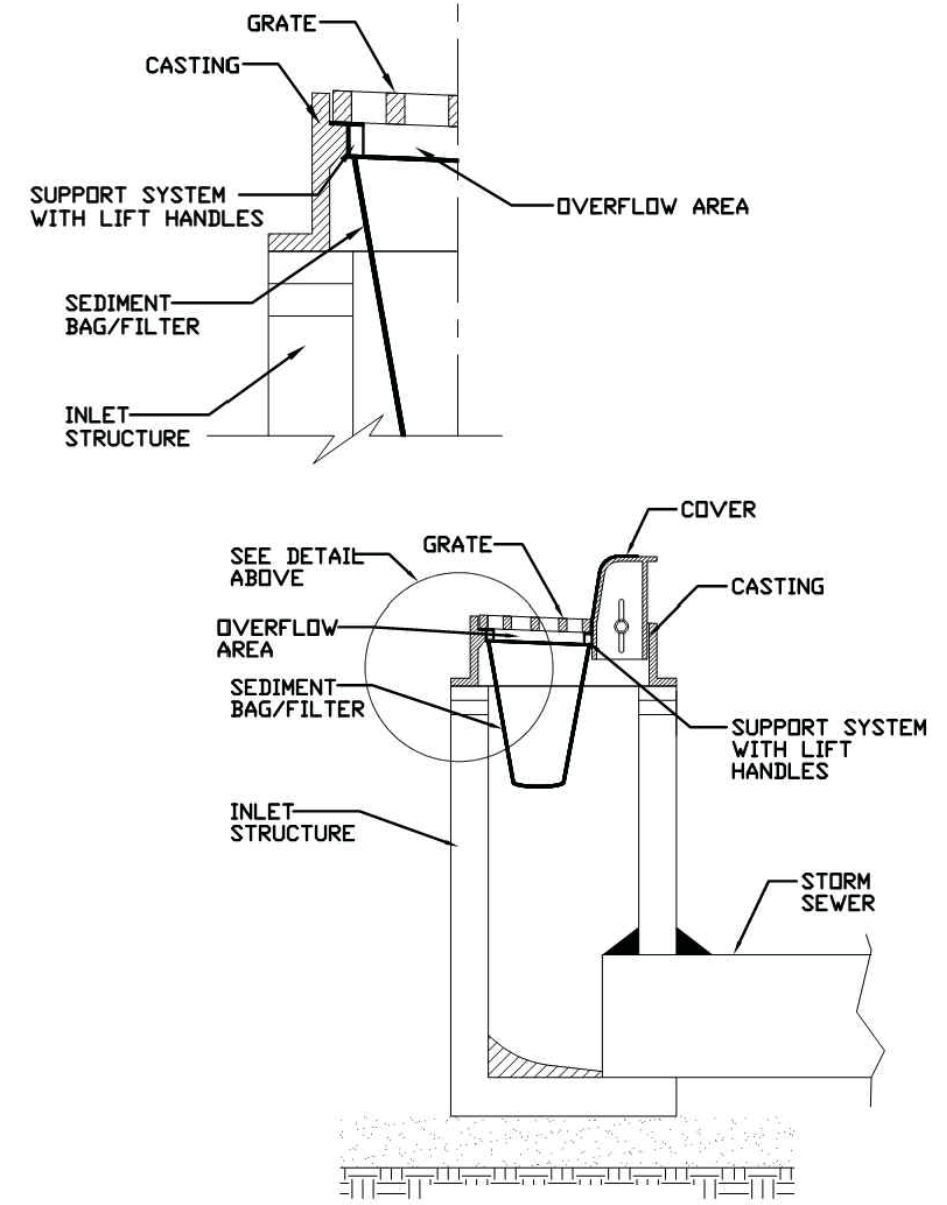
NOTES:

- Maintaining temporary concrete washout facilities shall include removing and disposing of hardened concrete and/or slurry and returning the facilities to a functional condition.
- Facility shall be cleaned or reconstructed in a new area once washout becomes two-thirds full.
- Each straw bale is to be staked in place using (2) 2"x2"x4" wooden stakes.

INLET PROTECTION - PAVED AREAS
CURB PROTECTION



INLET PROTECTION - PAVED AREAS
DROP-IN PROTECTION



<p>COMPANY NAME: _____</p> <p>PROJECT CONTACT: _____</p> <p>CLIENT: _____</p> <p>DATE PLOTTED: 8/19/2024 12:42 PM</p> <p>FILE NAME: Liberty-Details</p> <p>PLOT DRIVER: DWG To PDF.pc3</p> <p>PEN TABLE: SEC-Standard.ctb</p>	<p>DESIGNED: _____</p> <p>DRAWN: B. JOHNSON</p> <p>CHECKED: _____</p> <p>APPROVED: _____</p> <p>DATE: 6/23</p>	<p>TEMPORARY CONCRETE WASHOUT FACILITY - STRAW BALE</p>
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<p>REFERENCE</p> <p>Project _____</p> <p>Designed _____ Date _____</p> <p>Checked _____ Date _____</p> <p>Approved _____ Date _____</p>	
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<p>STANDARD DWG. NO.</p> <p>IUM-561C</p> <p>SHEET 1 OF 1</p> <p>DATE 01-11-11</p>	
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<p>REFERENCE</p> <p>Project _____</p> <p>Designed _____ Date _____</p> <p>Checked _____ Date _____</p> <p>Approved _____ Date _____</p>	<p>STANDARD DWG. NO.</p> <p>IUM-561D</p> <p>SHEET 1 OF 1</p> <p>DATE 01-11-11</p>	
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<p>TEMPORARY CONCRETE WASHOUT FACILITY - STRAW BALE</p>	
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<p>STANDARD DWG. NO.</p> <p>IUM-561D</p> <p>SHEET 1 OF 1</p> <p>DATE 01-11-11</p>	
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COMPANY NAME: _____
PROJECT CONTACT: _____
CLIENT: _____
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PEN TABLE: SEC-Standard.ctb

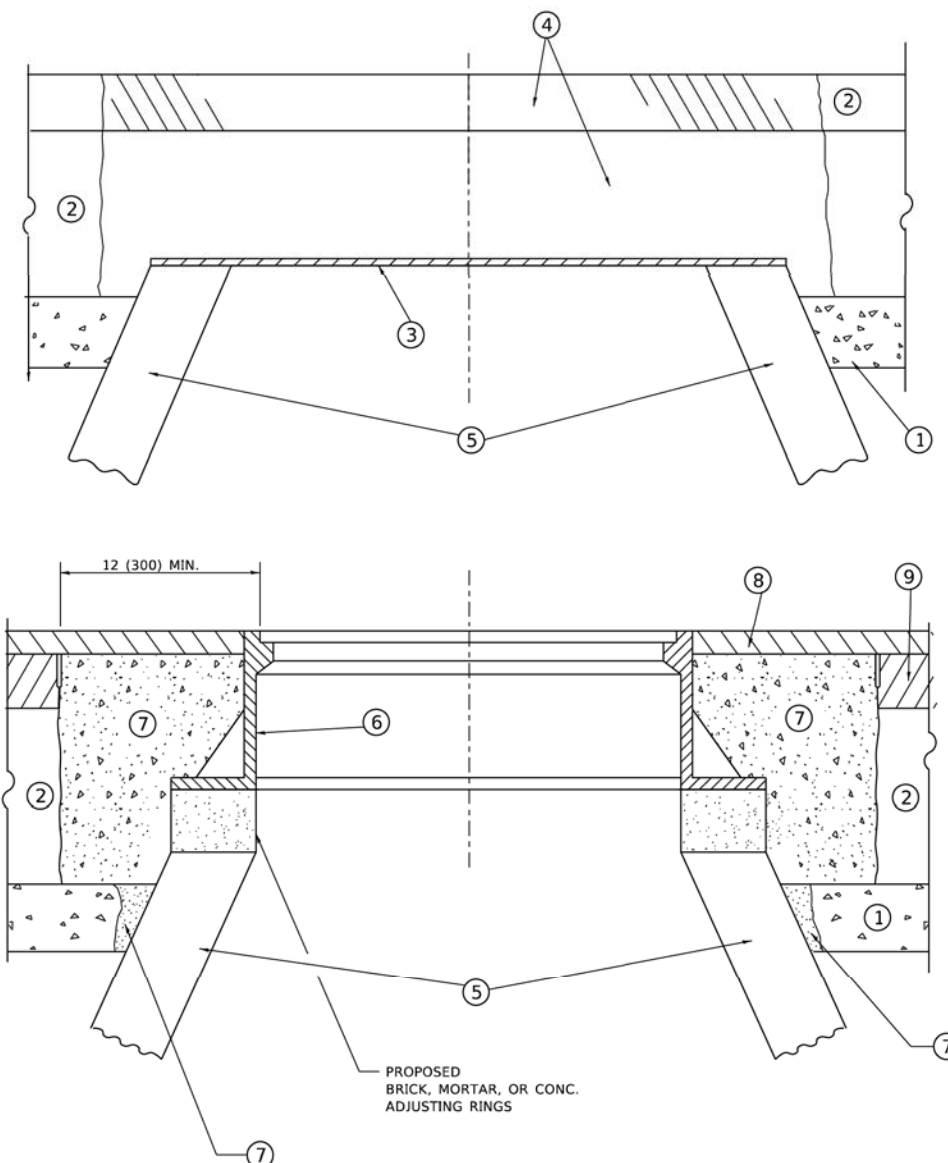
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PLOT DATE = 8/19/2024	DATE - 03/01/2024	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL DETAILS

SCALE: N.T.S. SHEET NO. 01 OF 01 SHEETS STA. TO STA.

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3570	23-00364-00-RS	DUPAGE	25	14
CONTRACT NO. 61K65				
ILLINOIS FED. AID PROJECT				



DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

NOTES

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-2* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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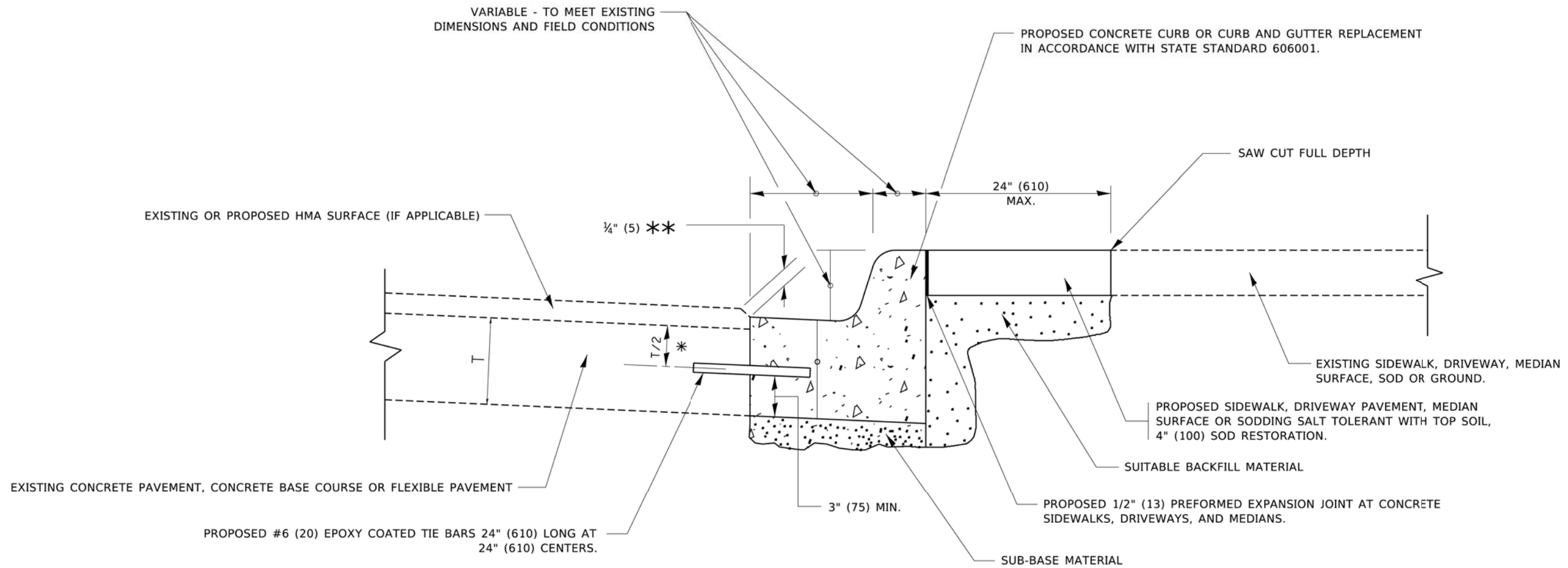
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	DRAWN -	REVISED - R. BORO 12-06-11
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - K. SMITH 11-18-22
PLOT DATE = 9/15/2023	DATE - 10-25-94	REVISED - K. SMITH 09-15-23

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3570	23-00364-00-RS	DUPAGE	25	15
BD600-03 (BD-08)			CONTRACT NO. 61K65	
ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- *** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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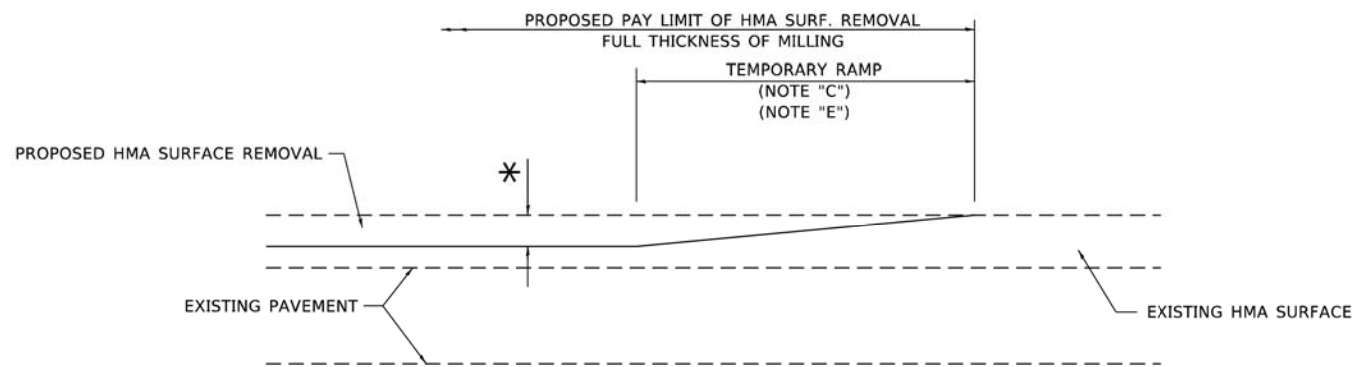
USER NAME = footemj	DESIGNED - A. HOUSEH	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 01-22-01
PLOT SCALE = 50.0000" / in.	CHECKED -	REVISED - R. BORO 12-15-09
PLOT DATE = 7/11/2019	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

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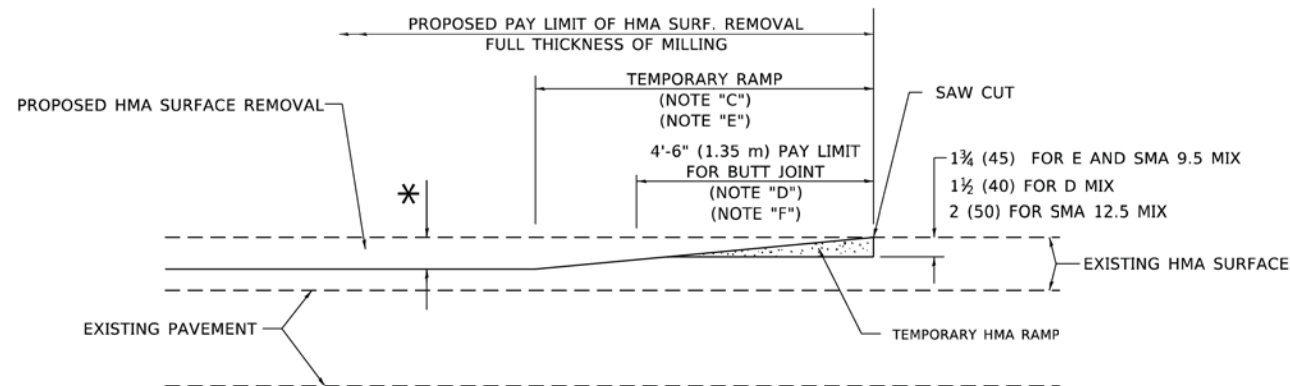
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BD600-06 (BD-24)		CONTRACT NO. 61K65		
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

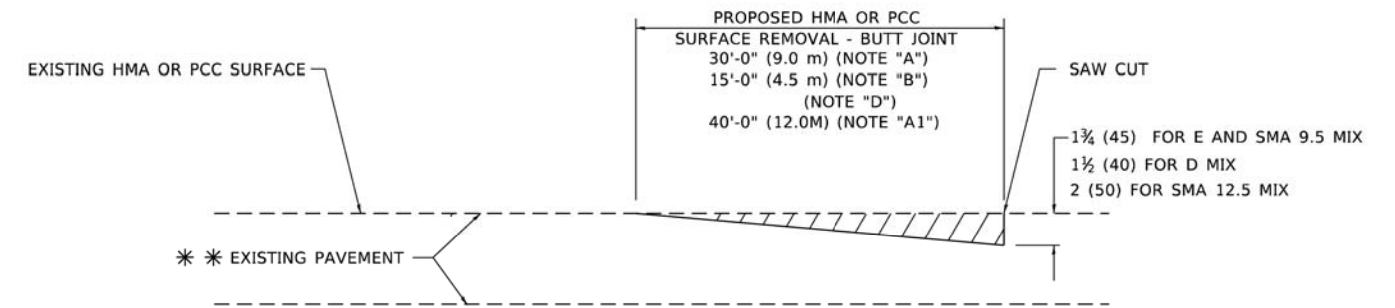


HMA CONSTRUCTED TEMPORARY RAMP

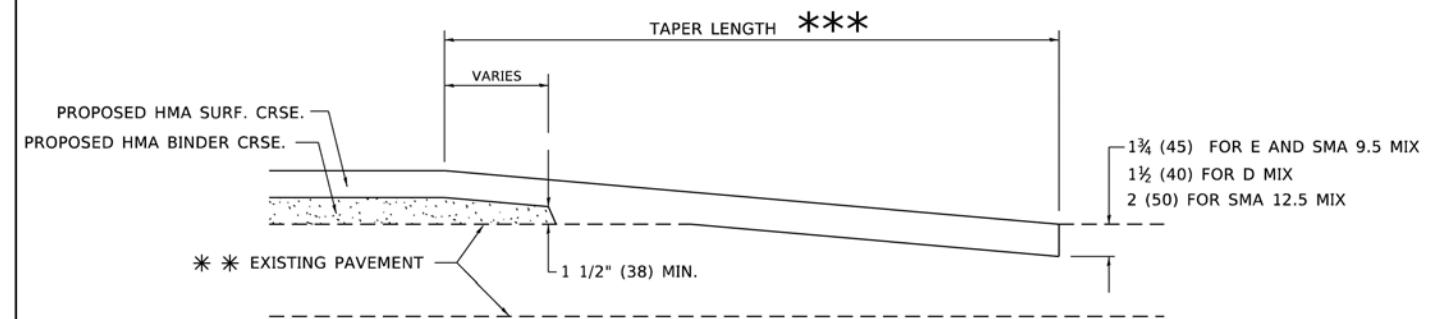
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

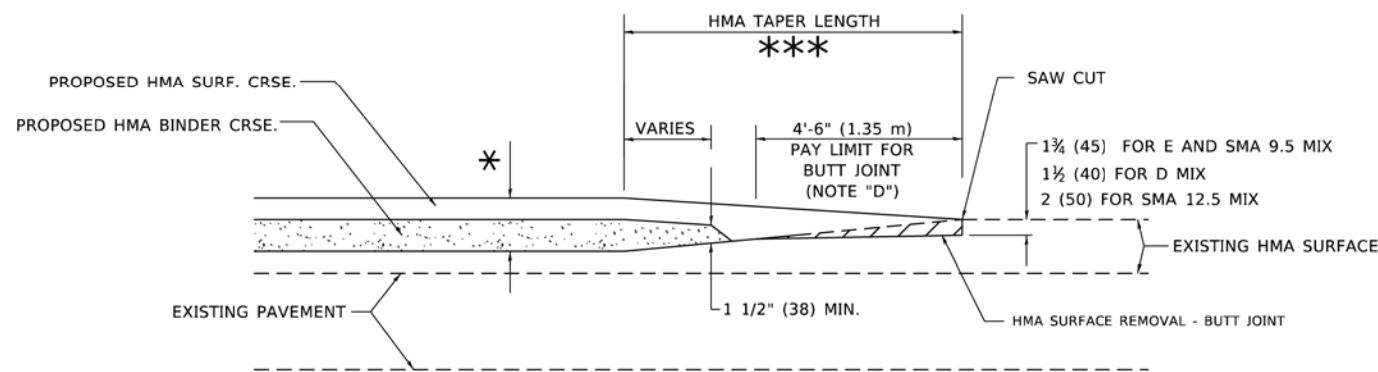
*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
 - *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
 - 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

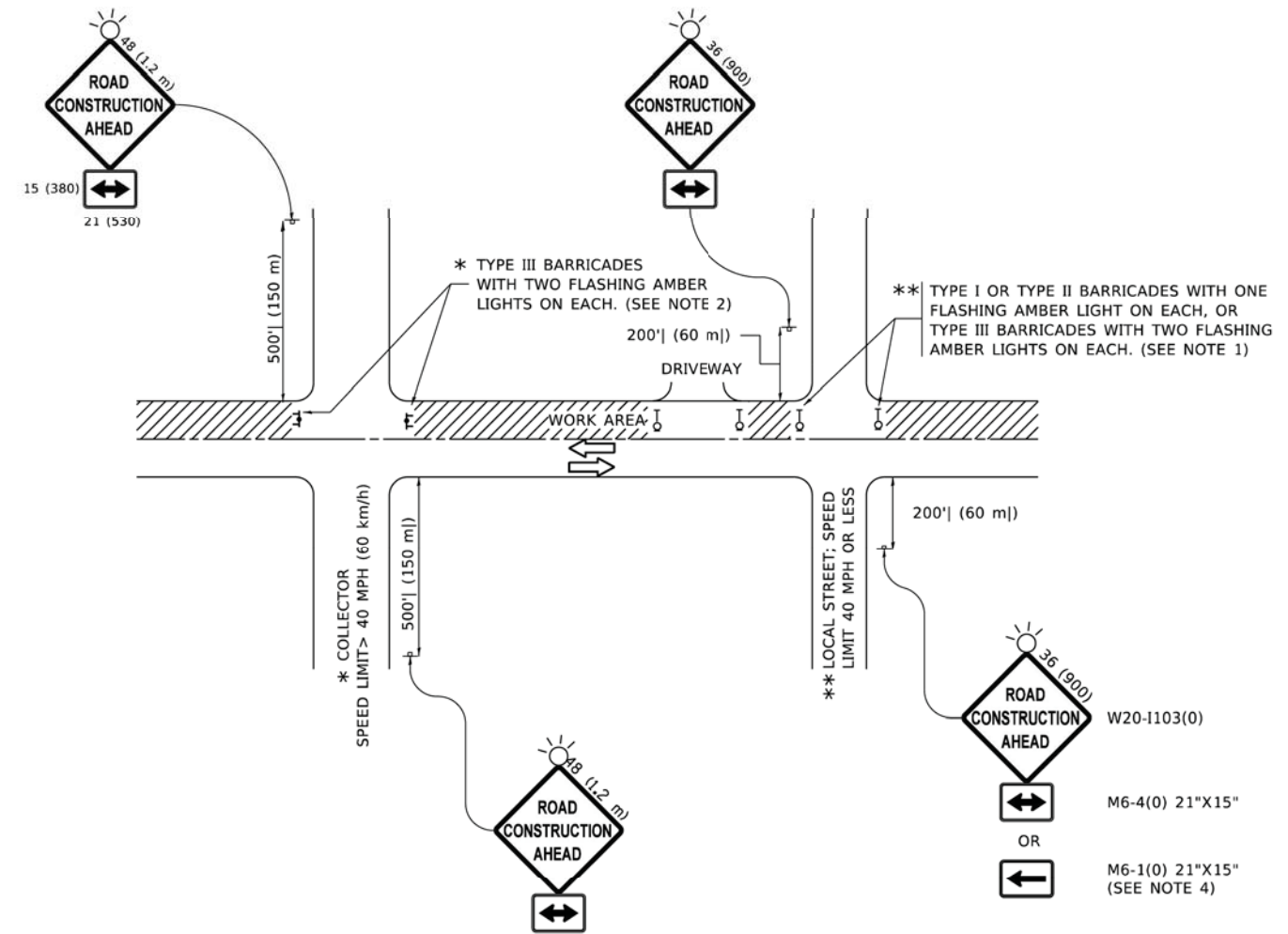
BUTT JOINT AND
HMA TAPER DETAILS

USER NAME = Lawrence.DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD400-05 BD-32		CONTRACT NO. 61K65		
ILLINOIS FED. AID PROJECT				

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NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

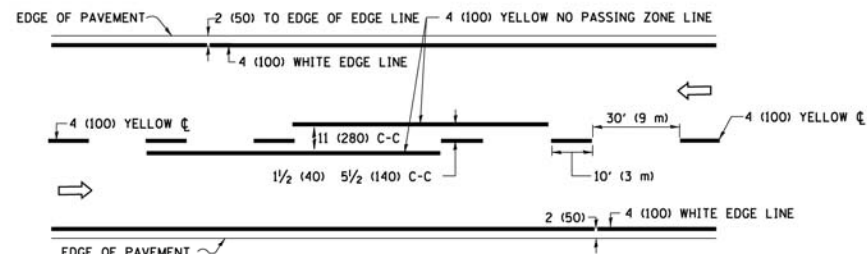
All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = Lawrence.DeManche	DESIGNED - L.H.A.	REVISED - T. RAMMACHER 01-06-00
	DRAWN -	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 5/3/2024	DATE - 06-89	REVISED - D. SENDERAK 05-03-24

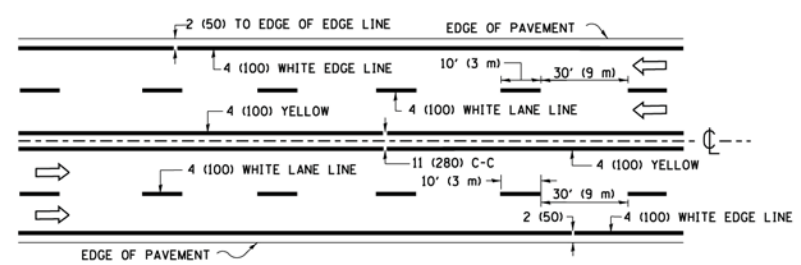
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. _____ TO STA. _____	

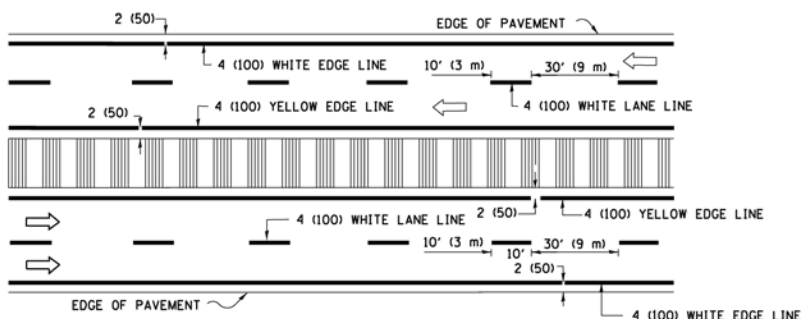
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3570	23-00364-00-RS	DUPAGE	25	19
TC-10			CONTRACT NO. 61K65	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

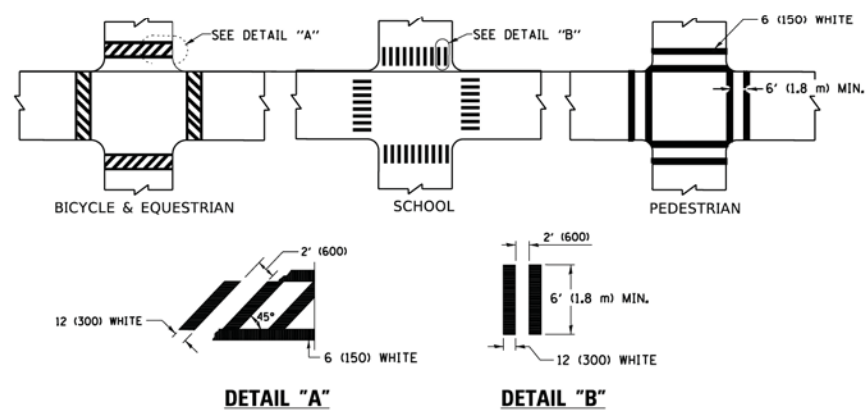


MULTI-LANE UNDIVIDED



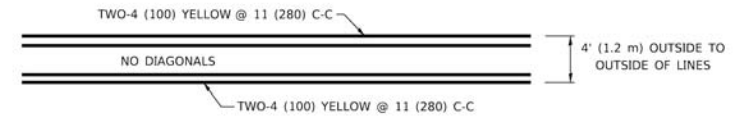
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

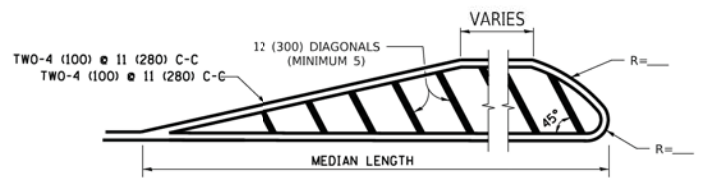


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



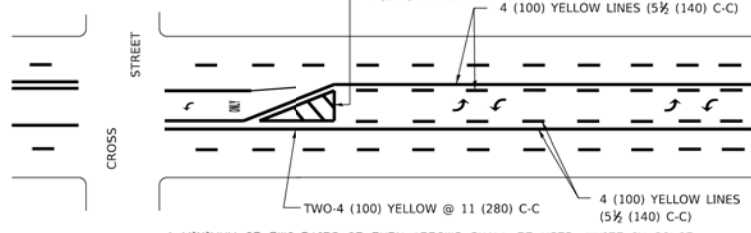
4' (1.2 m) WIDE MEDIANS ONLY



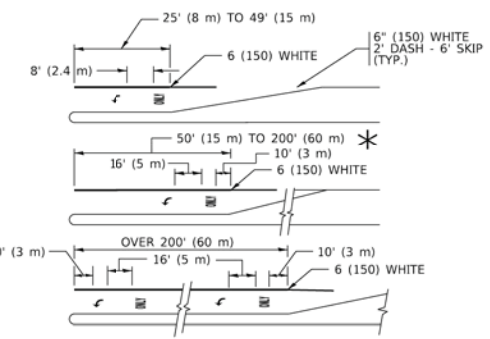
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE



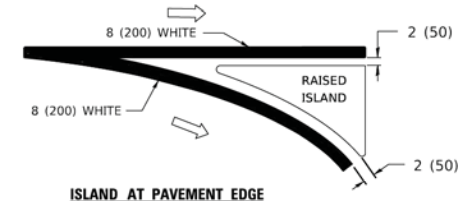
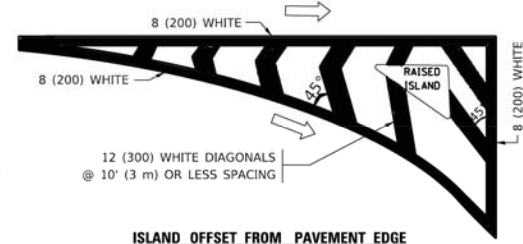
MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING



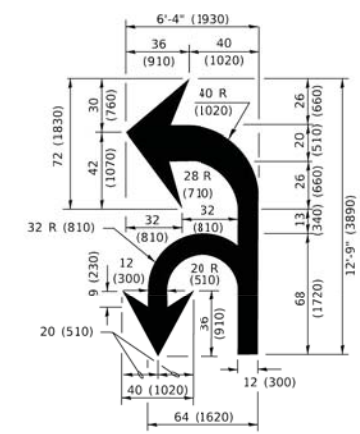
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

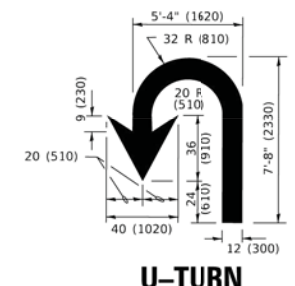
TYPICAL LEFT (OR RIGHT) TURN LANE
TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



LANE REDUCTION TRANSITION
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" 15 6" (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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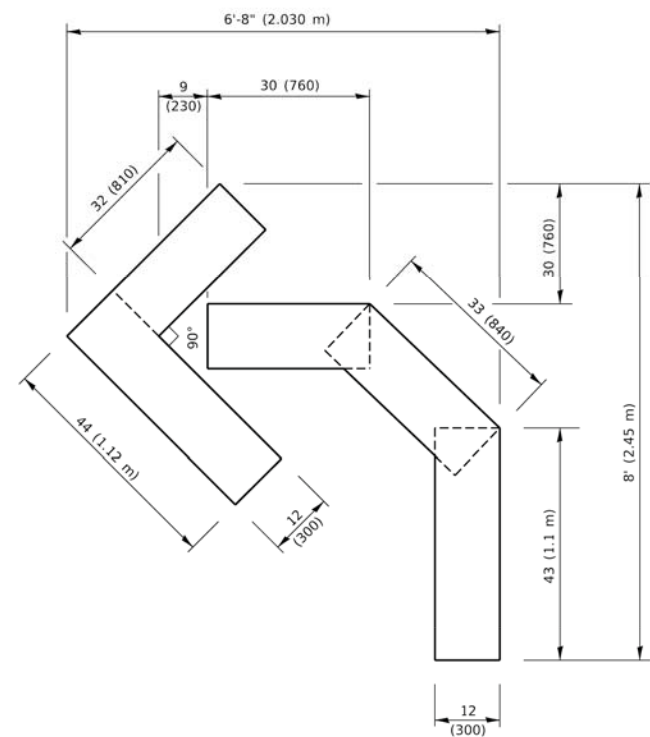
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	DRAWN -	REVISED - C. JUCIUS 07-01-13
PLOT SCALE = 50.0000" / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

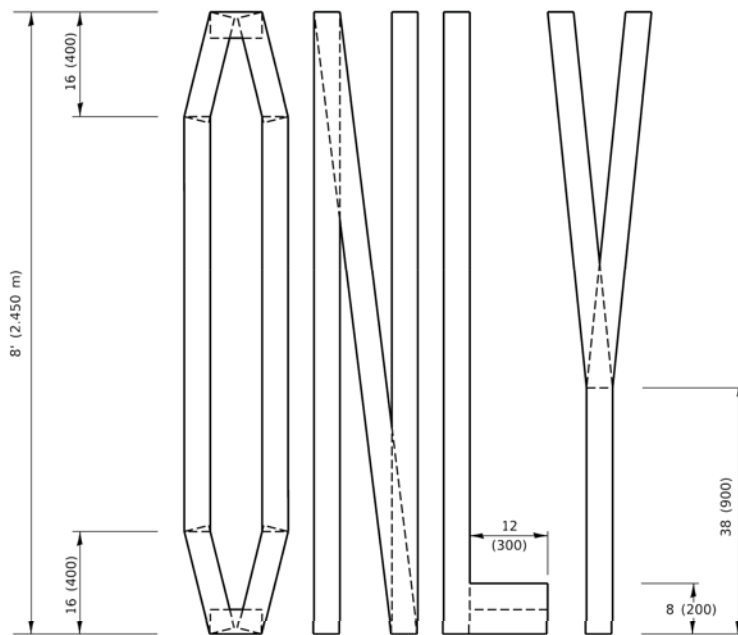
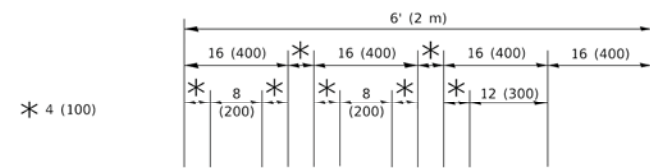
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3570	23-00364-00-RS	DUPAGE	25	20
TC-13				CONTRACT NO. 61K65
ILLINOIS FED. AID PROJECT				



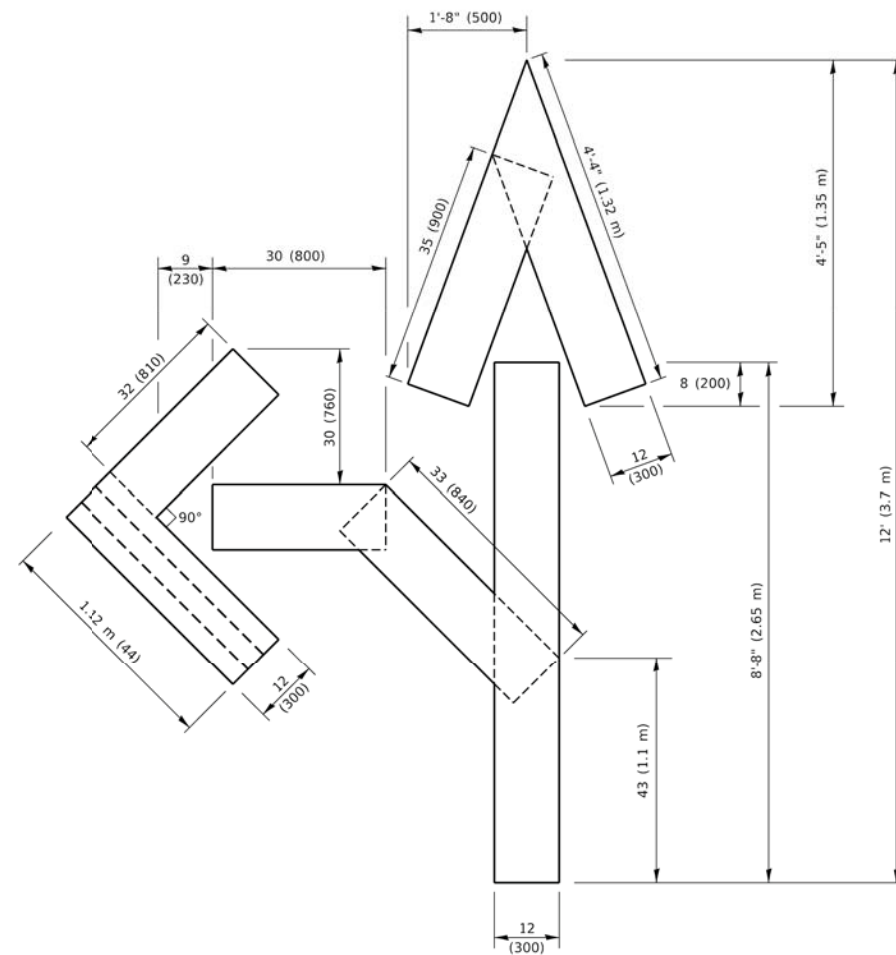
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

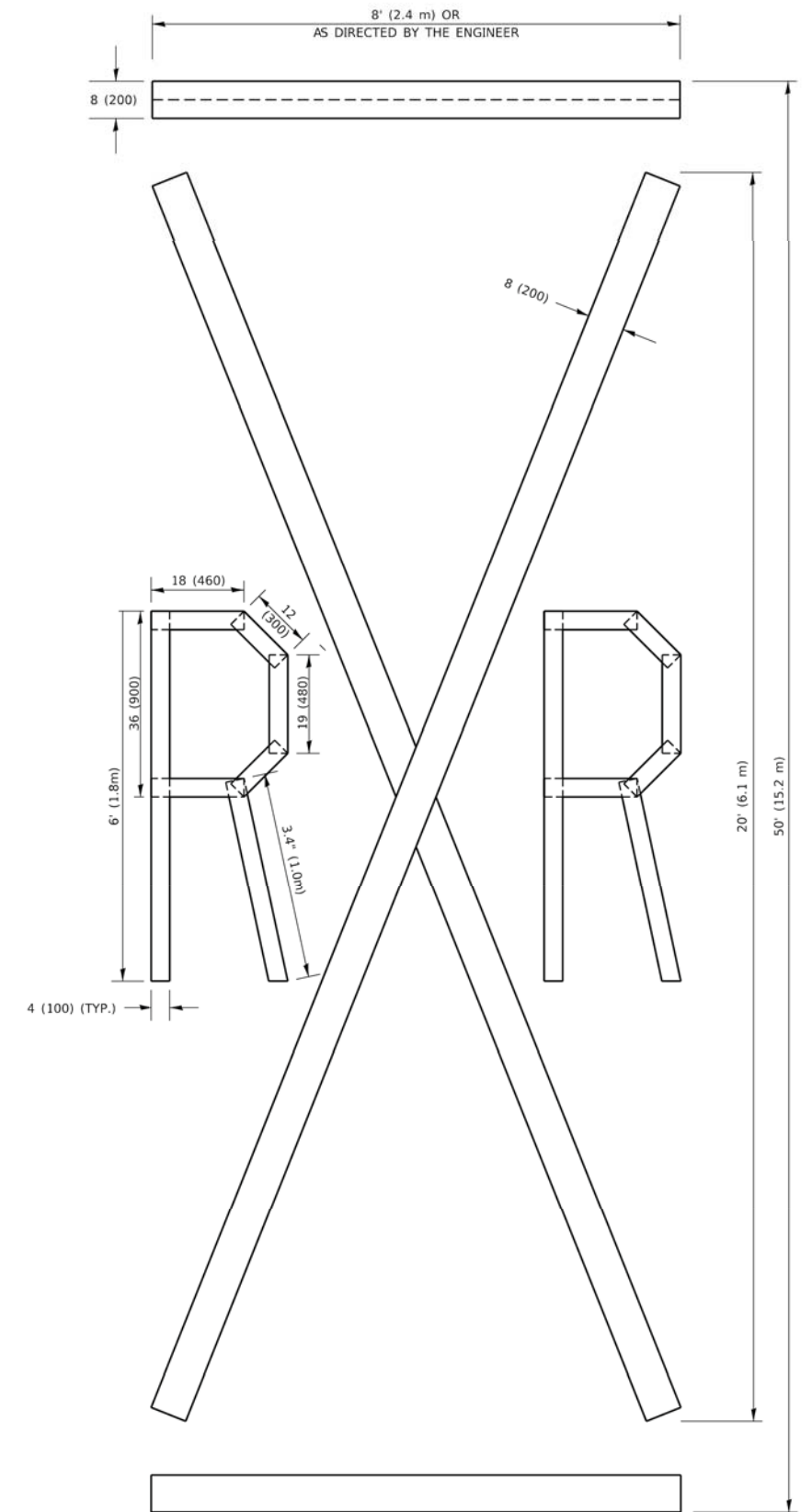


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT SCALE = 50.0068" / in.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 3/4/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

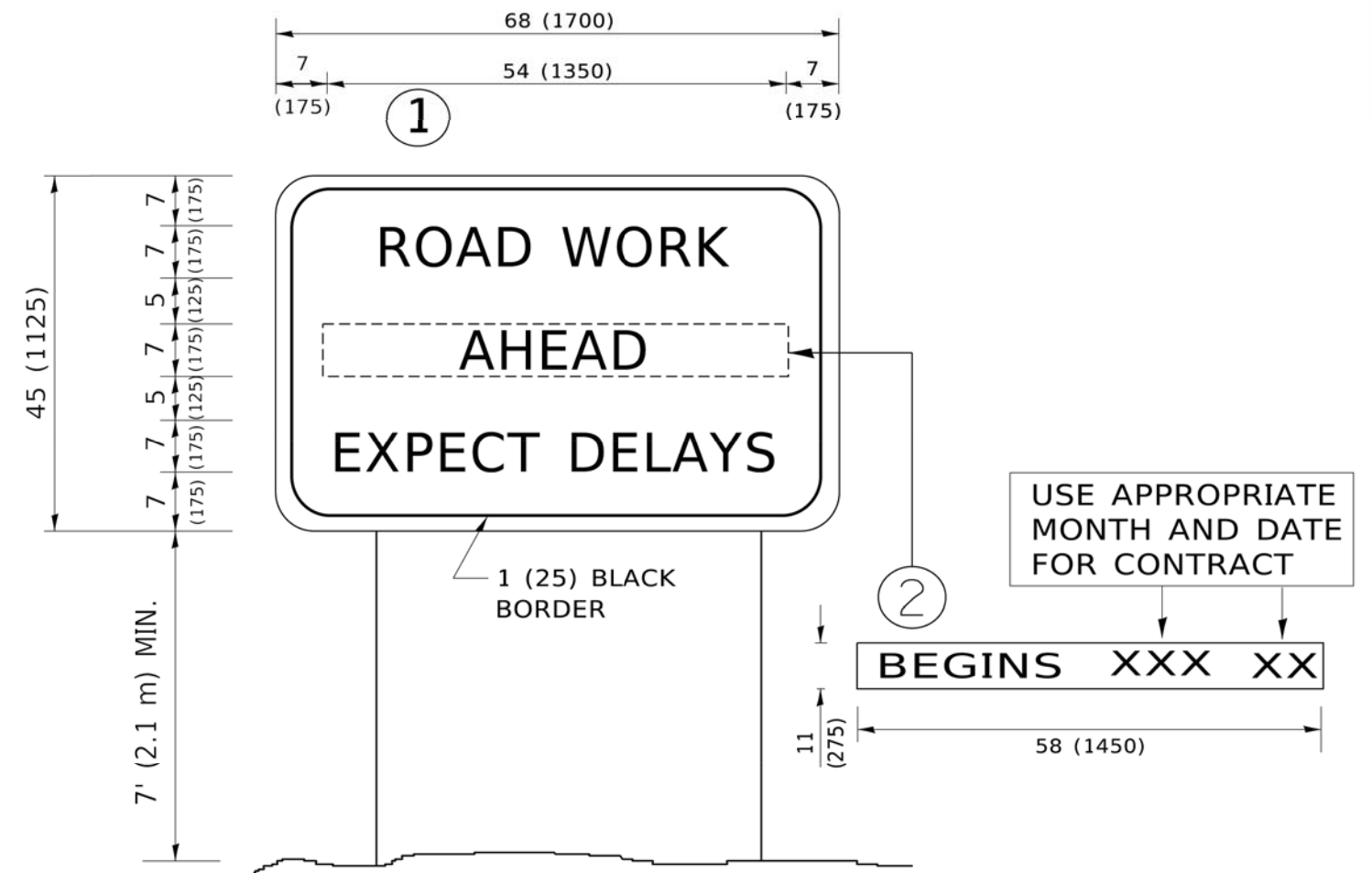
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3570	23-00364-00-RS	DUPAGE	25	21
TC-16			CONTRACT NO. 61K65	
ILLINOIS FED. AID PROJECT				

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NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
 UNLESS OTHERWISE SHOWN.

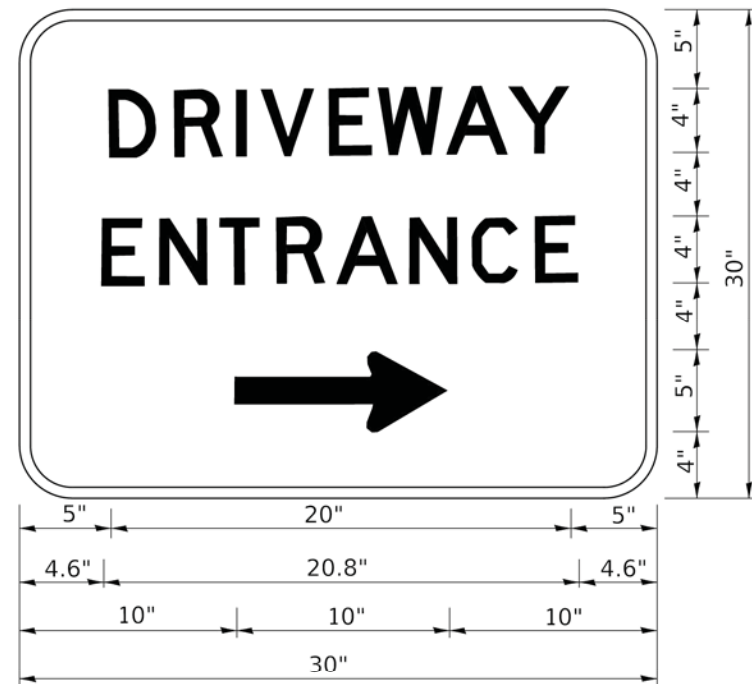
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PLOT SCALE = 50.0000" / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
 INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3570	23-00364-00-RS	DUPAGE	25	22
TC-22			CONTRACT NO. 61K65	
ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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	DRAWN -	REVISED -
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PLOT DATE = 8/6/2021	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

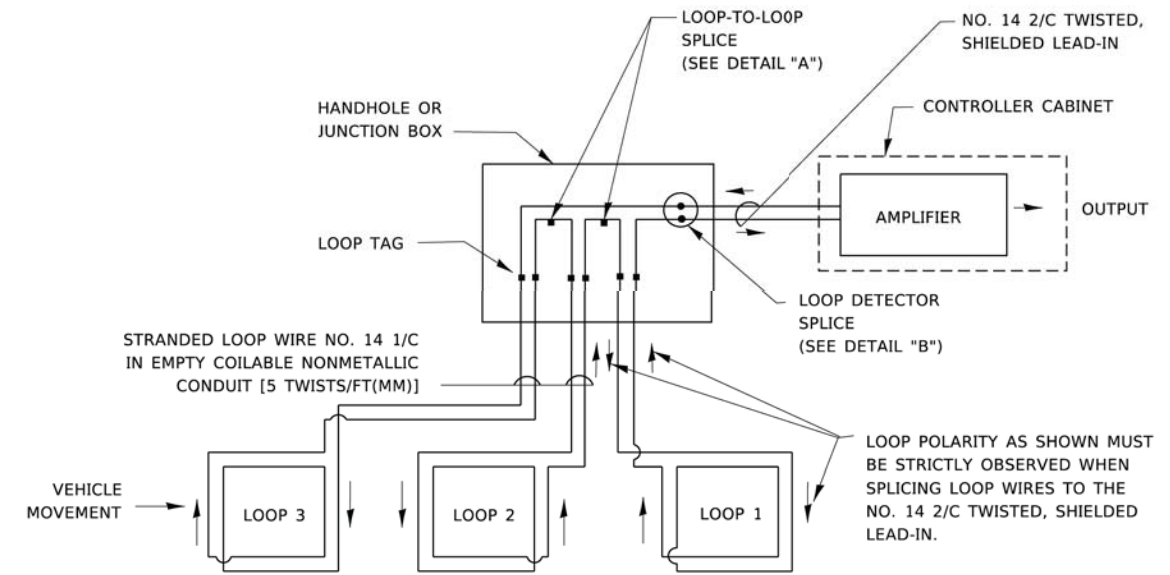
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3570	23-00364-00-RS	DUPAGE	25	23
TC-26			CONTRACT NO. 61K65	
ILLINOIS FED. AID PROJECT				

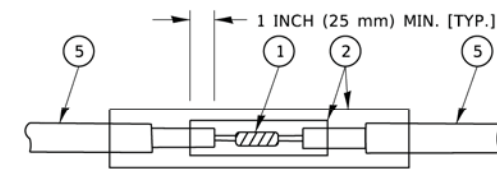
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

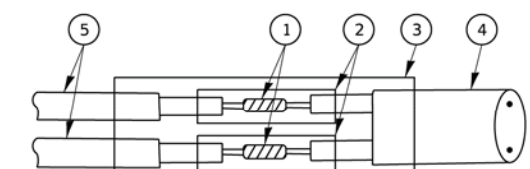


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



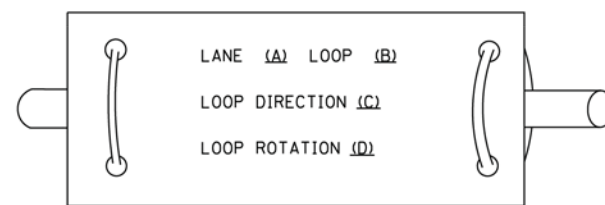
DETAIL "A"
LOOP-TO-LOOP SPLICE



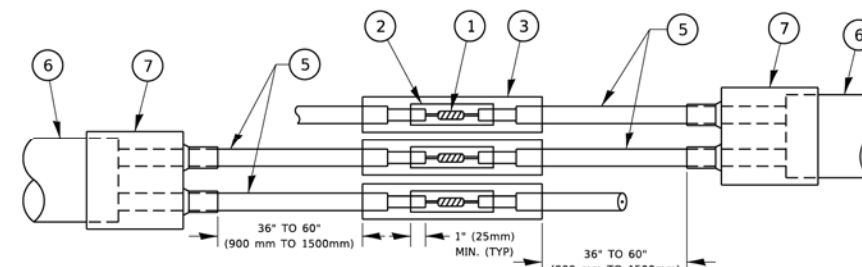
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP

LOOP LEAD-IN CABLE TAG

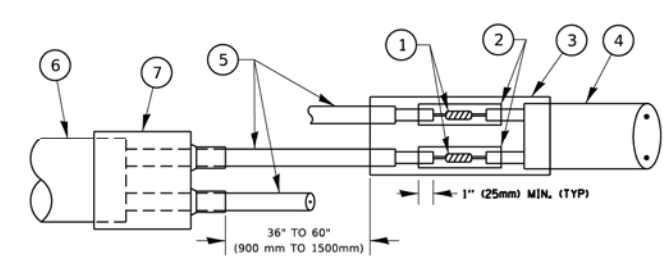


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETAIL "A"
LOOP-TO-LOOP SPLICE

PREFORMED LOOP



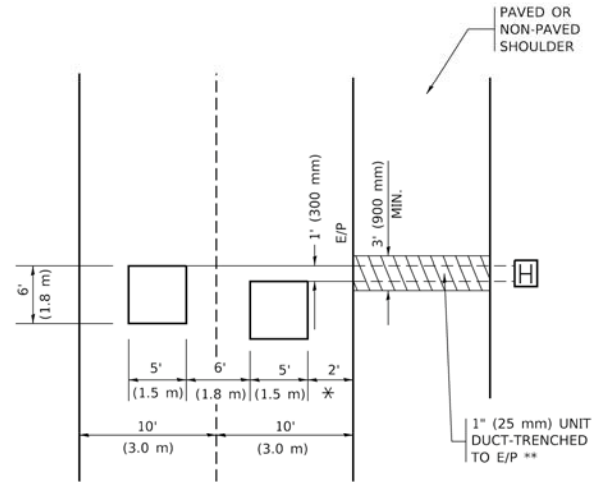
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PREFORMED LOOP
- 6 XL POLYOLEFIN 2 CONDUCTOR
- 7 BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



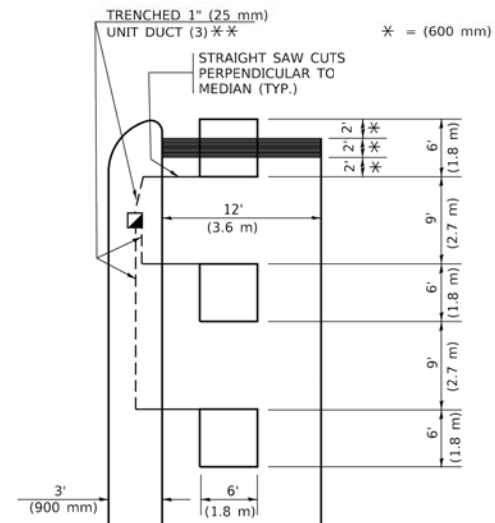
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



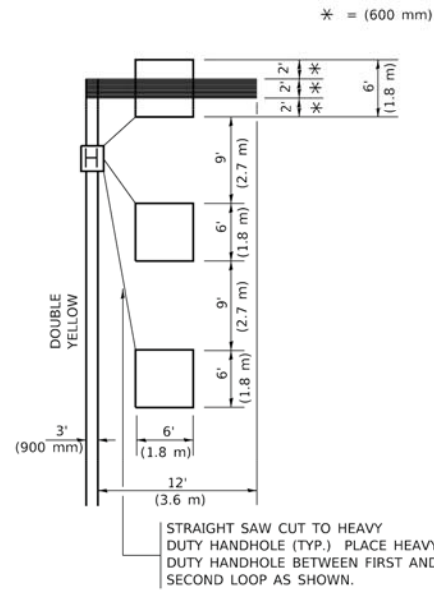
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

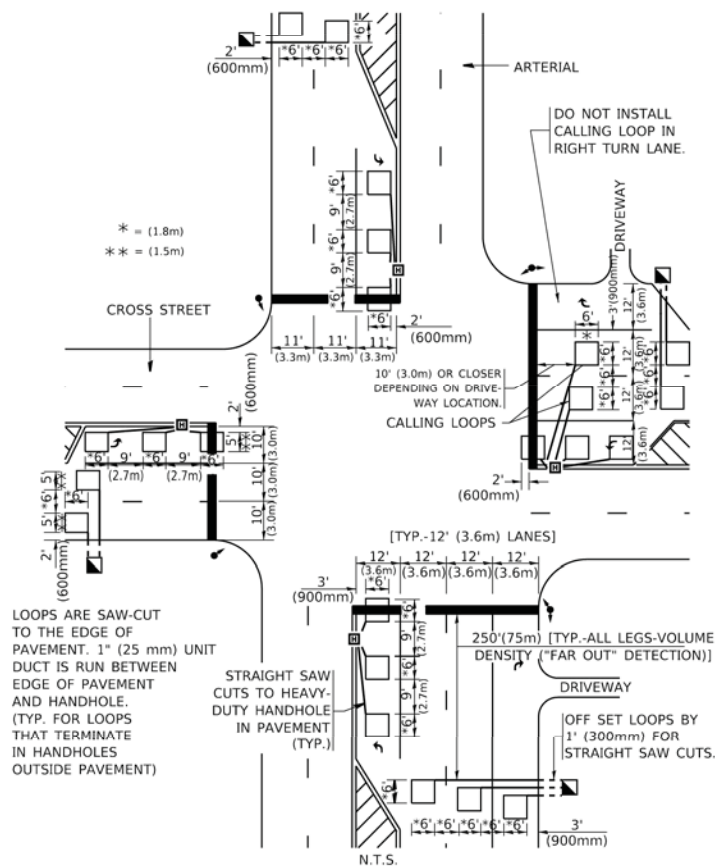
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE). USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

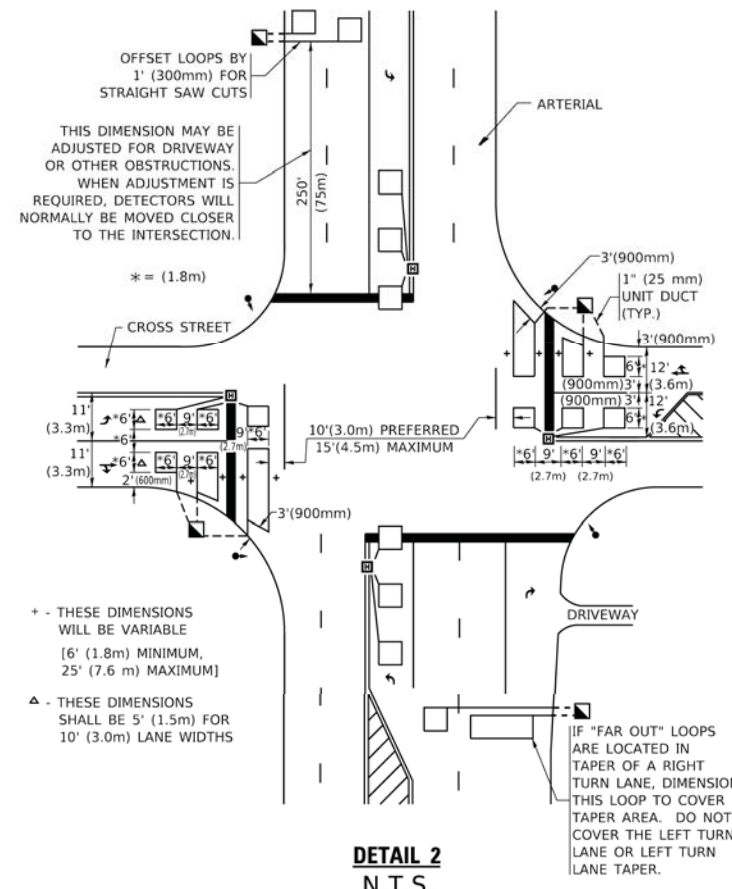
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

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PLOT DATE = 3/4/2019	DATE -	REVISIONS -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3570	23-00364-00-RS	DUPAGE	25	25
TS-07			CONTRACT NO. 61K65	
ILLINOIS FED. AID PROJECT				