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CONSTRUCTION PLANS FOR WILLARD AIRPORT

UNIVERSITY OF ILLINOIS SAVOY, IL

IL. PROJ. NO: CMI-5043

REHABILITATE GENERAL AVIATION APRONS

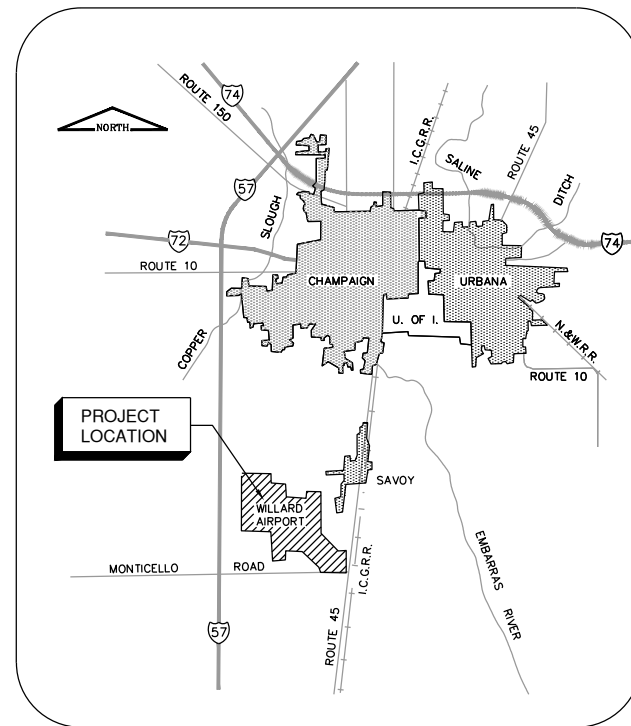
| SUMMARY OF QUANTITIES | | | |
|-----------------------|-------------------------------|-------|----------|
| BASE BID | | | |
| ITEM NO. | DESCRIPTION | UNIT | QUANTITY |
| AR150510 | ENGINEERS FIELD OFFICE | L SUM | 1 |
| AR150520 | MOBILIZATION | L SUM | 1 |
| AR401610 | BITUMINOUS SURFACE COURSE | TON | 190 |
| AR401650 | BITUMINOUS PAVEMENT MILLING | SQ YD | 1,275 |
| AR401655 | BUTT JOINT CONSTRUCTION | SQ YD | 350 |
| AR501509 | 9" PCC PAVEMENT | SQ YD | 1,650 |
| AR501510 | 10" PCC PAVEMENT | SQ YD | 1,725 |
| AR501516 | 16" PCC PAVEMENT | SQ YD | 145 |
| AR501530 | PCC TEST BATCH | EACH | 1 |
| AR501900 | REMOVE PCC PAVEMENT | SQ YD | 3,375 |
| AR501916 | 16" PCC PAVEMENT REMOVAL | SQ YD | 145 |
| AR501922 | PCC SPALL REPAIR | SQ FT | 60 |
| AR510511 | TIE DOWN - TYPE A | EACH | 9 |
| AR510512 | TIE DOWN - TYPE B | EACH | 7 |
| AR510901 | REMOVE TIE DOWN - TYPE A | EACH | 9 |
| AR603510 | BITUMINOUS TACK COAT | GAL | 250 |
| AR605510 | JOINT SEALING FILLER | FOOT | 10,500 |
| AR620520 | PAVEMENT MARKING - WATERBORNE | SQ FT | 2,375 |

| ADDITIVE ALTERNATE #1 - SELECT PCC PANEL REPAIR | | | |
|---|---------------------|-------|----------|
| ITEM NO. | DESCRIPTION | UNIT | QUANTITY |
| AS501510 | 10" PCC PAVEMENT | SQ YD | 240 |
| AS501900 | REMOVE PCC PAVEMENT | SQ YD | 240 |
| AS510512 | TIE DOWN - TYPE B | EACH | 2 |
| AS510515 | GROUND ROD | EACH | 1 |

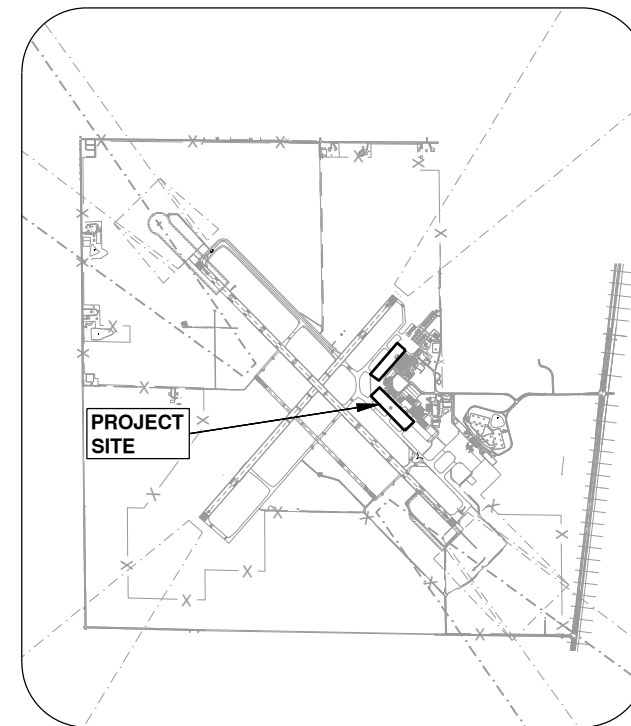
| ADDITIVE ALTERNATE #2 - SELECT PCC PANEL REPAIR 2 | | | |
|---|---------------------|-------|----------|
| ITEM NO. | DESCRIPTION | UNIT | QUANTITY |
| AT501510 | 10" PCC PAVEMENT | SQ YD | 475 |
| AT501900 | REMOVE PCC PAVEMENT | SQ YD | 475 |

| ADDITIVE ALTERNATE #3 - MISC. SPALL REPAIR | | | |
|--|------------------|-------|----------|
| ITEM NO. | DESCRIPTION | UNIT | QUANTITY |
| AU501922 | PCC SPALL REPAIR | SQ FT | 100 |

NOVEMBER 08, 2024



LOCATION MAP



SITE PLAN

CALL J.U.I.E.
BEFORE EXCAVATING
1-800-892-0123

UNIVERSITY OF ILLINOIS - WILLARD AIRPORT

TOWNSHIP: T 18 N
RANGE: R 8 E
COUNTY: CHAMPAIGN
SECTION 2, 3, 10 AND 11

TAXIWAY A

AIRPLANE DESIGN GROUP - ADG III
TAXIWAY DESIGN GROUP III
DESIGN APPROACH CATEGORY - C

GROUND CONTROL RADIO FREQUENCY - 121.8
ATIS FREQUENCY - 124.85
MAXIMUM HEIGHT OF EQUIPMENT
ABOVE GROUND IS 25 FT.

UNIVERSITY OF ILLINOIS

APPROVED *[Signature]*

PRINTED Timothy A. Bannon, A.A.E.

DATE September 13, 2024

Christy B. Bann

September 13, 2024
Exp. 11/30/2025

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SUBMITTED BY *Christy B. Bann*

DATE September 13, 2024

CMT JOB NUMBER: 21001968

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FILE: 210001968-GI001.dwg
UPDATE BY: Chris Groth
PLOT DATE: 9/27/2024 11:23 AM

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Date: Friday, September 20, 2024 8:55:10 AM

0. GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G OR LATEST, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS. THE CSPP CONSISTS OF THIS SHEET AND SHEETS GC001- GC101.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G OR LATEST. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF, LOCAL FAA ATO AND RPR. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.
- THE CSPP AS WRITTEN HAS BEEN APPROVED BY THE AIRPORT AND THE FAA. PROPOSED CHANGES TO THE WORK LIMITS SHALL BE COORDINATED THROUGH THE FAA FOR AIRSPACE ANALYSIS AND WILL REQUIRE A MINIMUM OF 30 DAYS TO REVIEW.

2. PHASING

- PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

3. CONSTRUCTION ACTIVITY AREAS

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. WORK ZONE LIGHTING FOR NIGHTWORK

- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTION. LIGHTS SHALL CONSIST OF VEHICLE OR MOVEABLE POLE-MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL NOT INTERFERE WITH AIR OPERATIONS OR ATCT CONTROLLER SIGHT LINES. ANY WORK BEING PERFORMED UNDER INSUFFICIENT ARTIFICIAL LIGHTING, IN THE RPR'S JUDGMENT, SHALL BE STOPPED UNTIL SUCH TIME AS ADDITIONAL LIGHTING IS PROVIDED. ALL WORK PERFORMED DURING THAT TIME WILL NOT BE ACCEPTABLE UNTIL PROPER INSPECTION & TESTING CAN BE MADE.
- ARTIFICIAL LIGHTING SHALL NOT BE AIMED AT THE ATCT OR THE APPROACH ENDS OF AN ACTIVE RUNWAY.

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
- CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE VILLAGE, UNIVERSITY, COUNTY, TOWNSHIP, OR I.D.O.T.
- ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIR OPERATIONS AREA (AOA) SHALL ATTEND THE APPROPRIATE DRIVERS TRAINING PROGRAM ADMINISTERED BY THE AIRPORT. ONLY THOSE INDIVIDUALS WHO RECEIVE TRAINING AND DRIVING PRIVILEGES WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
- DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE OR ATTAIN DRIVER'S PRIVILEGES BUT SHALL SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR PERSONNEL WHO IS BADGED AND HAS OBTAINED DRIVERS PRIVILEGES STATED IN #4 ABOVE. CONTRACTOR DELIVERY ESCORT PROCEDURES SHALL BE APPROVED BY THE AIRPORT PRIOR TO INITIATING ESCORT PROCEDURES.
- CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED CMI/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
- ALL VEHICLE AND EQUIPMENT OPERATORS UTILIZED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. FOD MANAGEMENT

- THE CONTRACTOR SHALL DISCARD ANY FOREIGN OBJECT DEBRIS (FOD) ON THE AIRFIELD PAVEMENTS.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES AT THE END OF EACH WORKING DAY, REGARDLESS OF THE WORK AREA BEING OPEN OR CLOSED TO AIR TRAFFIC.

7. PROTECTION OF NAVAIDS

- THE CONTRACTOR SHALL MAINTAIN A 100' DISTANCE BETWEEN HIS OPERATIONS AND ANY FAA-OWNED NAVAID (TYPICALLY ORANGE). CONTRACTOR SHALL CONTACT ATCT PRIOR TO ENTERING AN ILS CRITICAL AREA AS SHOWN ON THE SITE PLAN.
- ANY WORK WHICH AFFECTS A NAVAID WILL BE COORDINATED WITH FAA ATO THROUGH THE AIRPORT. WORK IN A NAVAID CRITICAL AREA IS RESTRICTED AND SUBJECT TO AVAILABILITY BASED ON RUNWAY CONFIGURATION AND WEATHER CONDITIONS AND MAY BE POSTPONED BY THE AIRPORT AT ANY TIME.

8. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY PUBLIC SAFETY OR THE RPR IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED AND LOCKED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.
- THE CONTRACTOR SHALL MAINTAIN THE SITE TO LIMIT STANDING WATER AND TALL GRASS TO REDUCE THEIR ATTRACTION AND DISRUPTION TO WILDLIFE HABITAT.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS/ARFF PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
- CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.
PUBLIC SAFETY
FIRE MARSHAL JOHN CUMBEE - DIRECTOR OF PUBLIC SAFETY
OFFICE (217) 244-8764
CELL (217) 202-8213

AIRPORT OPERATIONS
ANDREW SMITH - ASSISTANT DIRECTOR OF OPERATIONS AND MAINTENANCE
OFFICE (217) 333-8623
CELL (217) 238-1714

ENGINEER
CHRIS GROTH P.E. - PROJECT ENGINEER
(217) 787-8050
RPR TO BE DETERMINED
OFFICE (217) 787-8050

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOB SITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2G OR LATEST MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL REQUEST FINAL OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR TO THE AREA BEING REOPENED. PUBLIC SAFETY WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-04 & 70-05 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. HAZARDOUS MATERIALS MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT & RESPONSE PLAN AND KEEP COPIES ON THE OBSITE OF MATERIAL SAFETY DATA SHEETS FOR ALL MATERIALS HANDLED ON THE JOBSITE.
- FUELING OPERATIONS SHALL NOT OCCUR IN ANY ACTIVE OBJECT FREE AREAS.

13. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

14. SPECIAL CONDITIONS

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. CONTRACTOR SHALL COORDINATE WITH ADJACENT CONTRACTOR(S) TO PROVIDE UNHINDERED ACCESS TO EACH WORK AREA AND ALLOW FOR THE TIMELY PROSECUTION AND PROGRESS OF ANY OTHER WORK BEING PERFORMED AT THE AIRPORT.

15. RUNWAY & TAXIWAY VISUAL AIDS

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2G OR LATEST EDITION.

16. MARKING & SIGNS FOR ACCESS ROUTES

- BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLAN SHEET.

17. HAZARD MARKING & LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5D OR LATEST EDITIONS AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED AND/OR YELLOW LIGHTS AND FLAG PLACEMENT.

18. PROTECTION

- ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 93' FROM THE TAXIWAY CENTERLINE OF 50' TAXIWAYS AND 130' FROM THE CENTERLINE OF 75' TAXIWAYS, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
- ALL WORK REQUIRED ON AN ACTIVE APRON OR INSIDE OF AN ACTIVE SAFETY AREA, WHICH EXTENDS 70' FROM THE APRON'S EDGE OF PAVEMENT, WILL REQUIRE A PORTION OF THAT APRON TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

19. OTHER LIMITATIONS ON CONSTRUCTION

- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE CONTRACT DOCUMENTS.



License No. 184-000613

CONSULTANTS

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

100%
NOVEMBER 08, 2024

REHABILITATE GENERAL
AVIATION APRONS

OWNER



UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, IL

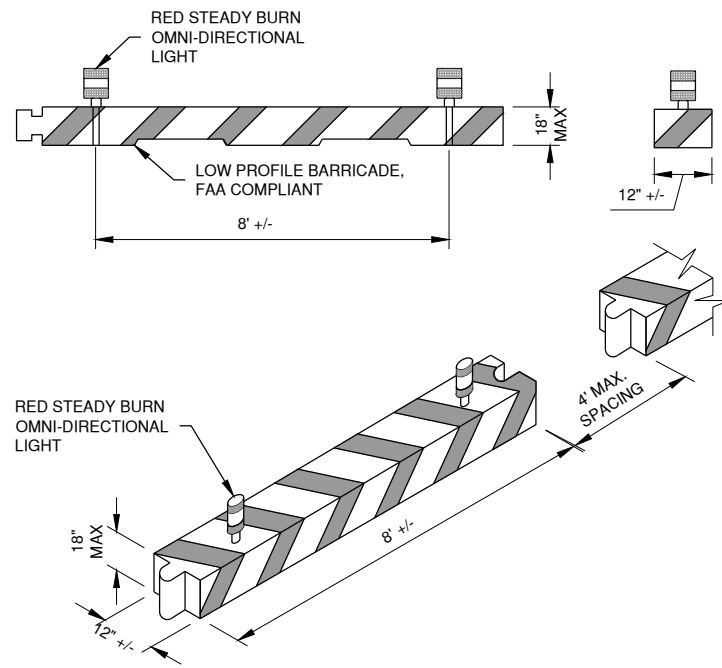
MARK | DATE | DESCRIPTION

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IL PROJ. NO: CMI-5043
CMT PROJECT NO: 21001968
CAD DWG FILE: 21001968-GC000.DWG
DESIGNED BY: MRK
DRAWN BY: MRK
CHECKED BY: MJD
APPROVED BY: CBG
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SHEET TITLE

CONSTRUCTION
SAFETY AND PHASING
NOTES

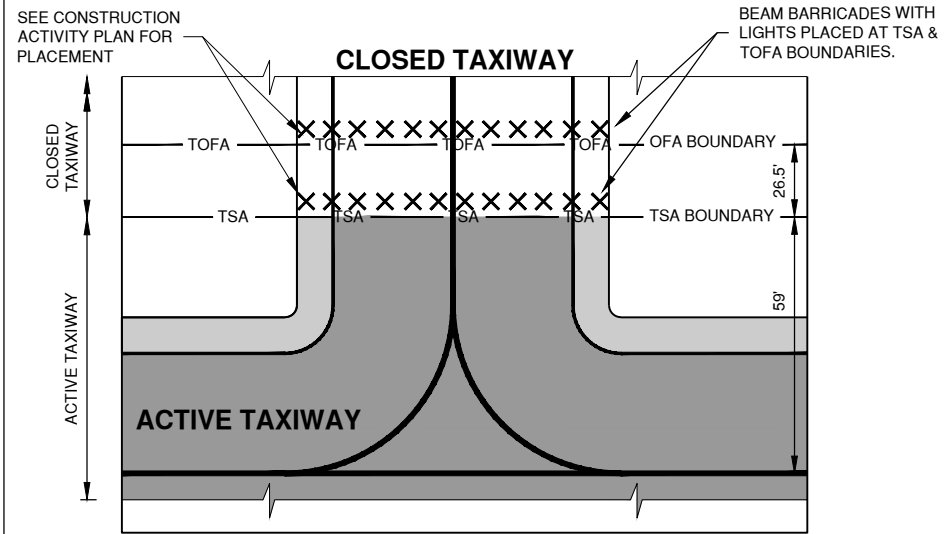
GC001
SHEET 3 OF 26



1 BEAM BARRICADE DETAIL
N.T.S.

BEAM BARRICADE NOTES

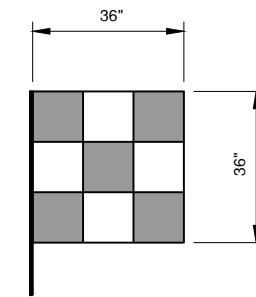
1. BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY WIND, JET OR PROP BLAST.
2. BARRICADE MUST BE OF LOW MASS AND EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AS SHOWN IN PLANS AND AS DIRECTED BY THE RPR.
5. BARRICADES SHALL BE COMPLIANT WITH FAA AC 150/5370-2 (LATEST VERSION).



2 CLOSED TAXIWAY/ACTIVE TAXIWAY BARRICADE DETAIL
N.T.S.

NIGHT WORK RUNWAY CLOSURE NOTES

1. WORK WITHIN THE RUNWAY SAFETY AREA MAY NOT BEGIN UNTIL THE RUNWAY HAS BEEN CLOSED, NAVAIDS DISABLED, EDGE LIGHTING, CENTERLINE LIGHTING, AND TDZ LIGHTING DISABLED IN THE VAULT AND THE LIGHTED CLOSURE MARKERS ARE IN PLACE.
2. UPON COMPLETION OF WORK IN NIGHTTIME OFF-PEAK CLOSURE, NO EXCAVATIONS OR STOCKPILES SHALL EXIST IN EXCESS OF 3" AND ALL GRADES SHALL MEET FAA REQUIREMENTS. BARRICADES AND CLOSURE MARKERS SHALL BE REMOVED. AIRPORT OPERATIONS WILL INSPECT RUNWAY AT 4:30 AM.
3. SEE SECTION 80 OF THE SPECIFICATIONS FOR INFORMATION REGARDING LIQUIDATED DAMAGES AND REOPENING THE RUNWAY.
4. PRIOR TO REOPENING THE RUNWAY, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE DEBRIS WITHIN THE RUNWAY SAFETY AREA.



3 EQUIPMENT & VEHICLE SIGNAL FLAG
N.T.S. (ORANGE / WHITE)

SIGNAL FLAG NOTES

1. ALL CONTRACTOR VEHICLES AND EQUIPMENT SHALL DISPLAY COMPANY LOGO PLACARDS AND FLAG.
2. WHEN WORKING PRIOR TO DAWN OR AFTER DUSK, A 360 DEGREE ROTATING AMBER BEACON IS REQUIRED ON ALL EQUIPMENT AND TRUCKS.
3. CONTRACTOR SHALL REPLACE FLAGS THAT ARE WORN AND INEFFECTIVE.



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NOVEMBER 08, 2024

REHABILITATE GENERAL
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OWNER



UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
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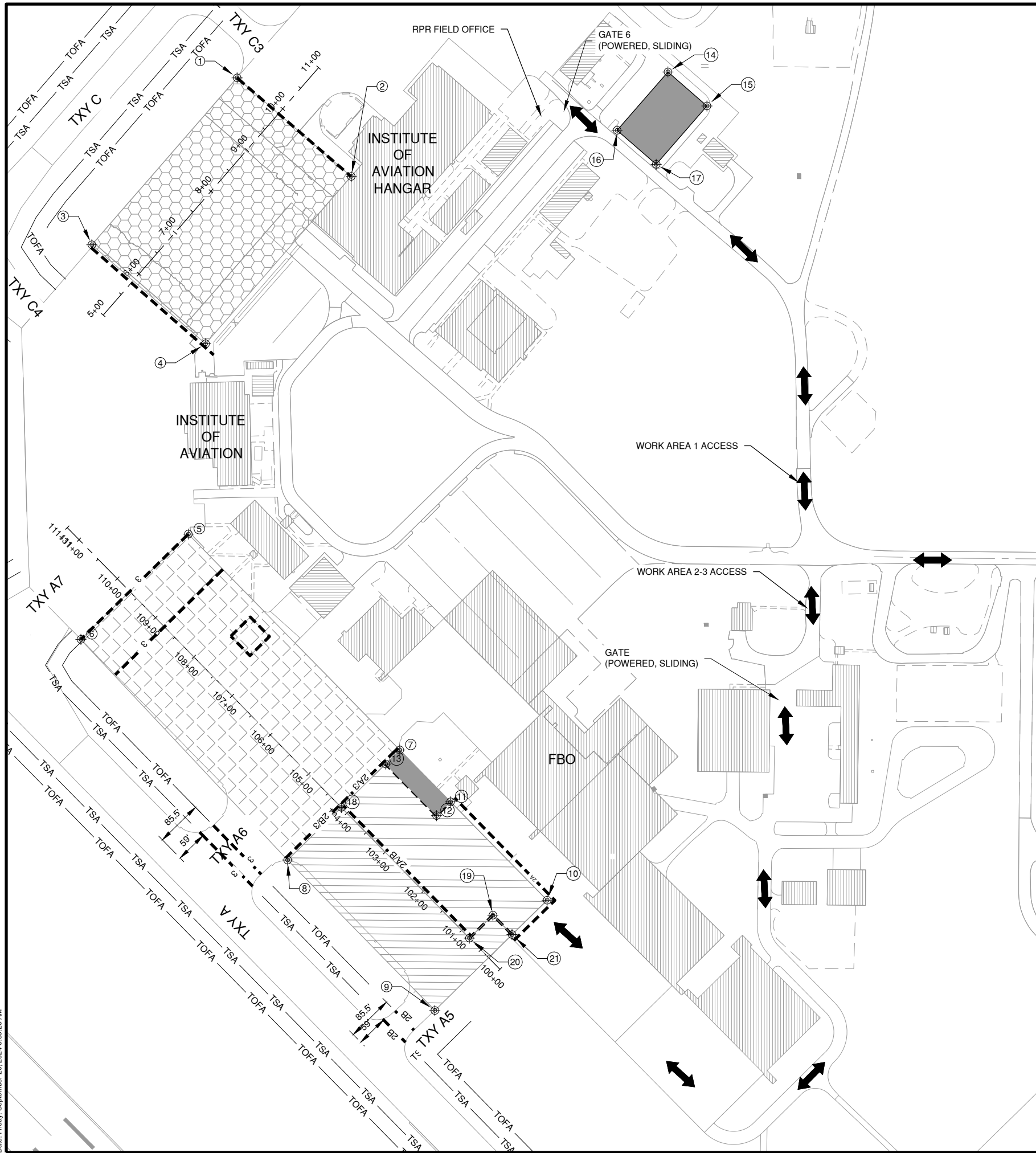
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AIP PROJ. NO:
IL PROJ. NO: CMI-5043
CMT PROJECT NO: 21001968
CAD DWG FILE: 21001968-GC000.DWG
DESIGNED BY: MRK
DRAWN BY: MRK
CHECKED BY: MJD
APPROVED BY: CBG
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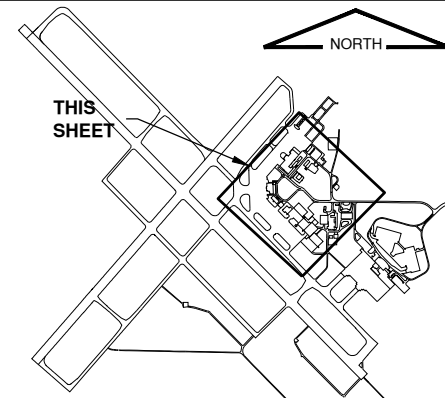
SHEET TITLE
**CONSTRUCTION
SAFETY AND PHASING
DETAILS**

GC002
SHEET 4 OF 26

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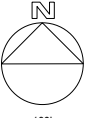


KEYMAP



License No. #####

CONSULTANTS



THIS BAR IS EQUAL TO 2' AT FULL SCALE (34X22).

LEGEND

- CONTRACTORS STAGING AND STORAGE AREA
- WORK AREA 1
- WORK AREA 2A
- WORK AREA 2B
- WORK AREA 3
- CONTRACTORS ACCESS ROUTE
- AIRCRAFT TAXI ROUTE
- GATE GUARD
- BEAM BARRICADES / WORK AREA
- CRITICAL POINT

100%
NOVEMBER 08, 2024

REHABILITATE GENERAL AVIATION APRONS

OWNER



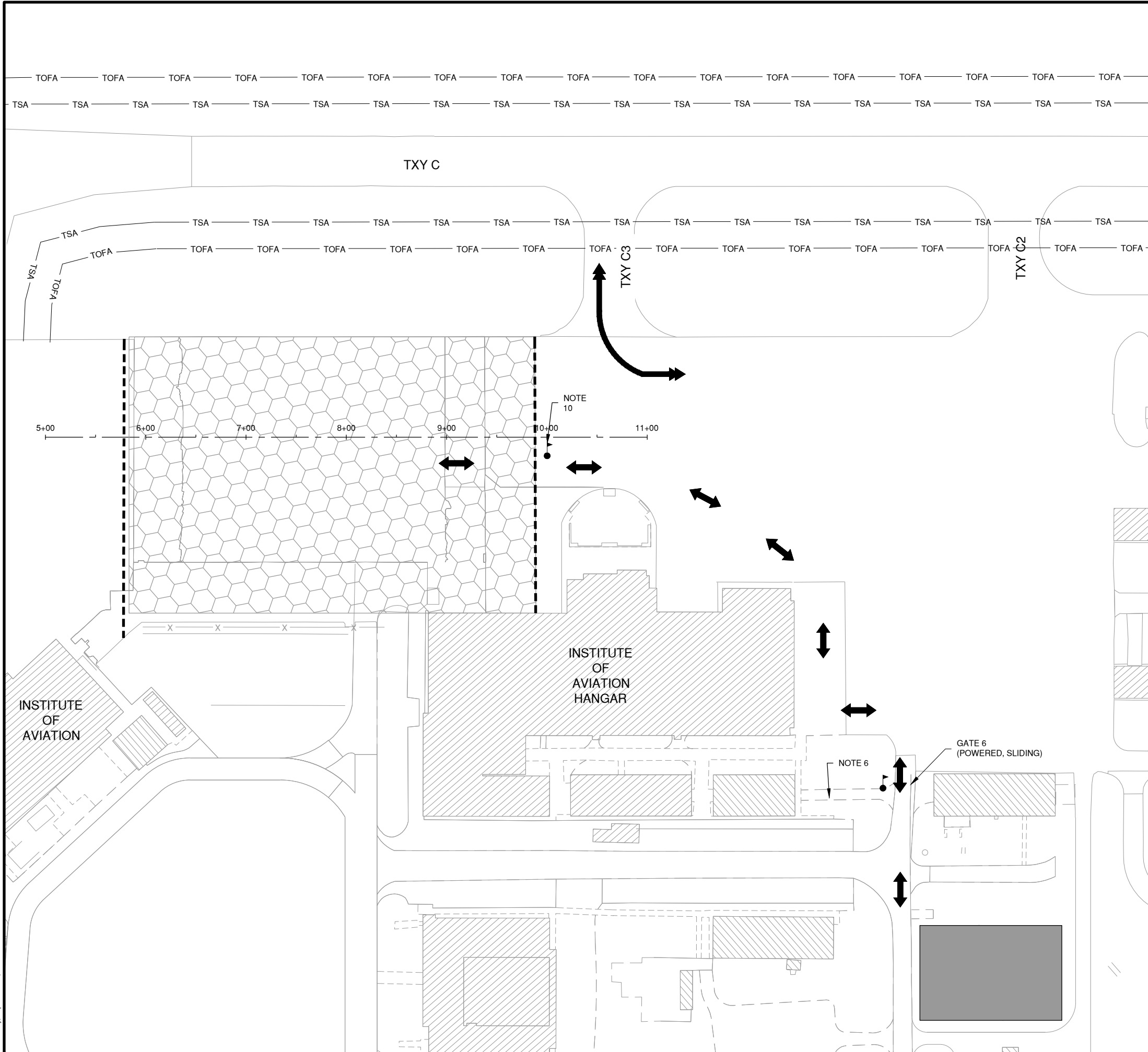
UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, IL

| CRITICAL POINT TABLE | | | | | |
|----------------------|-----------------|-----------------|------------------|--------------------|------------------------|
| POINT | LATITUDE | LONGITUDE | GROUND ELEVATION | OBSTRUCTION HEIGHT | ABOVE GROUND ELEVATION |
| 1 | N40° 02' 30.66" | W88° 16' 14.59" | 746 | 25' | 771 |
| 2 | N40° 02' 28.88" | W88° 16' 11.91" | 747 | 25' | 772 |
| 3 | N40° 02' 27.65" | W88° 16' 18.01" | 747 | 25' | 772 |
| 4 | N40° 02' 25.86" | W88° 16' 15.34" | 749 | 25' | 774 |
| 5 | N40° 02' 22.42" | W88° 16' 15.75" | 749 | 25' | 774 |
| 6 | N40° 02' 20.52" | W88° 16' 18.27" | 747 | 25' | 772 |
| 7 | N40° 02' 18.52" | W88° 16' 10.78" | 746 | 25' | 771 |
| 8 | N40° 02' 16.53" | W88° 16' 13.42" | 746 | 25' | 771 |
| 9 | N40° 02' 13.81" | W88° 16' 09.96" | 745 | 25' | 770 |
| 10 | N40° 02' 15.80" | W88° 16' 07.31" | 746 | 25' | 771 |
| 11 | N40° 02' 17.58" | W88° 16' 09.58" | 745 | 25' | 770 |
| 12 | N40° 02' 17.33" | W88° 16' 09.92" | 745 | 25' | 770 |
| 13 | N40° 02' 18.26" | W88° 16' 11.11" | 746 | 25' | 771 |
| 14 | N40° 02' 30.76" | W88° 16' 04.46" | 747 | 25' | 772 |
| 15 | N40° 02' 30.15" | W88° 16' 03.54" | 747 | 25' | 772 |
| 16 | N40° 02' 29.71" | W88° 16' 05.66" | 747 | 25' | 772 |
| 17 | N40° 02' 29.10" | W88° 16' 04.74" | 747 | 25' | 772 |
| 18 | N40° 02' 17.48" | W88° 16' 12.15" | 746 | 25' | 771 |
| 19 | N40° 02' 15.53" | W88° 16' 08.59" | 746 | 25' | 771 |
| 20 | N40° 02' 15.11" | W88° 16' 09.14" | 746 | 25' | 771 |
| 21 | N40° 02' 15.18" | W88° 16' 08.14" | 746 | 25' | 771 |

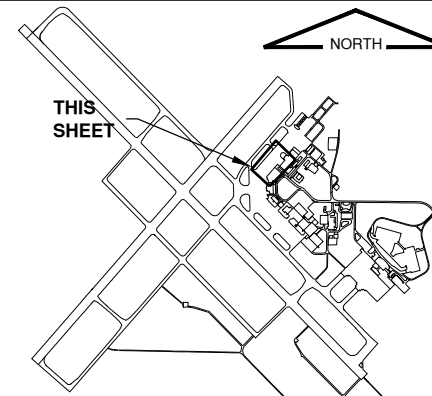
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IL PROJ. NO: CMI-5043
CMT PROJECT NO: 21001968
CAD DWG FILE: 21001968-GC100.DWG
DESIGNED BY: MRK
DRAWN BY: MRK
CHECKED BY: MJD
APPROVED BY: CBG
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SHEET TITLE
CONSTRUCTION
ACTIVITY PLAN
OVERVIEW



KEYMAP



License No. #####

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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

LEGEND

- CONTRACTORS STAGING AND STORAGE AREA
- WORK AREA 1
- CONTRACTORS ACCESS ROUTE
- AIRCRAFT TAXI ROUTE
- GATE GUARD / FLAGGER
- BEAM BARRICADES

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NOVEMBER 08, 2024

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UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, IL

NOTES

1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON ANY WORK AREA.
2. THE AIRPORT WILL REQUIRE 7 DAYS NOTICE PRIOR TO INITIATING WORK IN ANY WORK AREAS TO COORDINATE WITH THE INSTITUTE OF AVIATION. THIS WORK AREA IS INTENDED TO BE SCHEDULED AROUND THE INSTITUTE OF AVIATION'S OPERATIONS.
3. WORK AREAS MAY BE ACTIVE CONCURRENTLY WITH THE APPROVAL OF THE AIRPORT.
4. STAGING AND STORAGE AREA SHALL BE RESTORED TO ORIGINAL CONDITION PRIOR TO PROJECT COMPLETION.
5. COORDINATION WITH THE INSTITUTE OF AVIATION AND ALL INVOLVED PARTIES WILL BE REQUIRED WHEN REPAINTING EXISTING MARKINGS.
6. RPR FIELD OFFICE LOCATION TO BE APPROVED BY AIRPORT PRIOR TO PLACEMENT.
7. CONTRACTOR SHALL CONTINUOUSLY SWEEP ACCESS ROUTE AND KEEP FREE OF F.O.D.
8. GATE GUARDS SHALL BE USED IF POWERED GATE IS TO REMAIN OPEN FOR AN EXTENDED PERIOD OF TIME.
9. CONSTRUCTION TRAFFIC SHALL YIELD TO AIRCRAFT AND AIRPORT OPERATIONS AT ALL TIMES.
10. FLAGGER DURING PAVING / HIGH TRAFFIC DAYS TO DIRECT TRAFFIC.

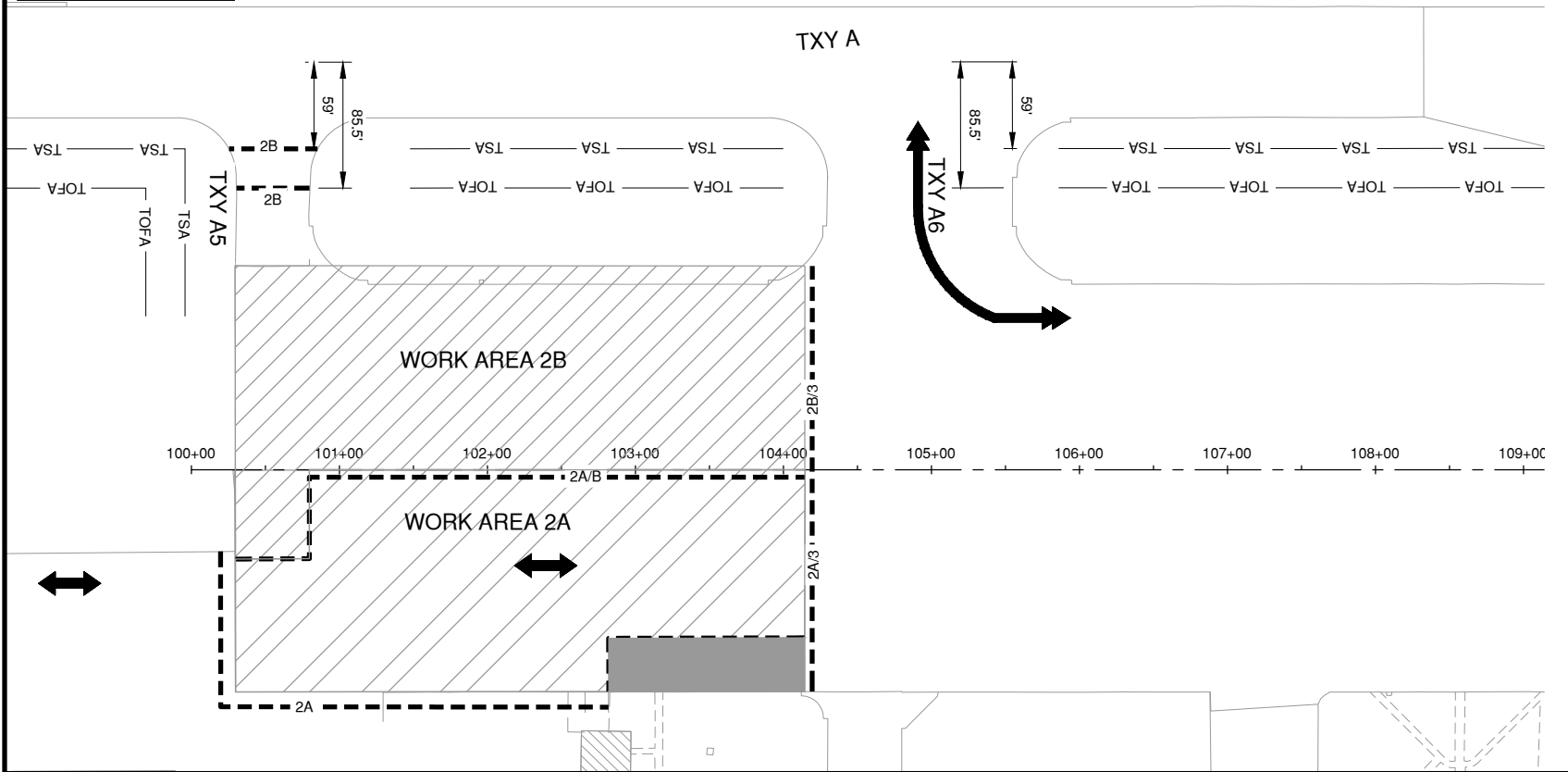
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| IL PROJ. NO: CMI-5043 | |
| CMT PROJECT NO: 21001968 | |
| CAD DWG FILE: 21001968-GC100.DWG | |
| DESIGNED BY: MRK | |
| DRAWN BY: MRK | |
| CHECKED BY: MJD | |
| APPROVED BY: CBG | |
| COPYRIGHT: | |

SHEET TITLE
**CONSTRUCTION
ACTIVITY PLAN 1**

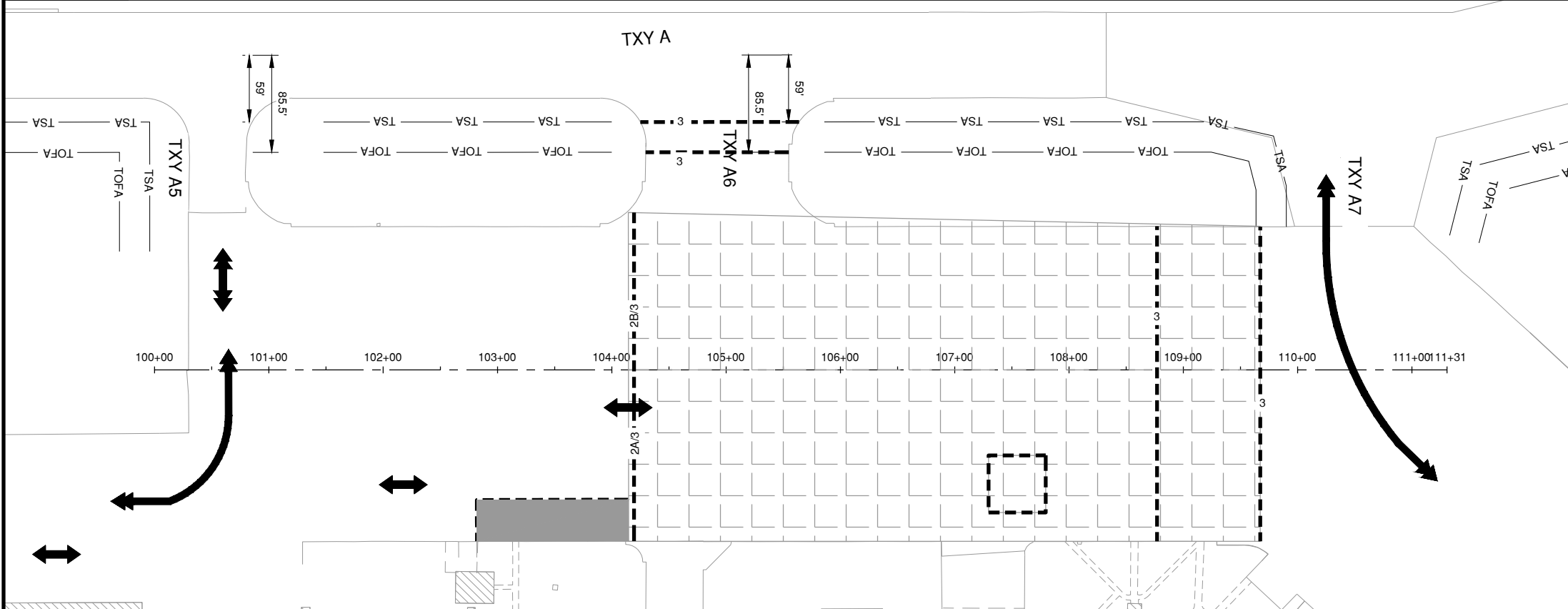
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Date: Friday, September 20, 2024 8:55:29 AM

WORK AREA 2A / 2B



| LOCATION | PAVEMENT STATUS | | | |
|----------------|-----------------|------|--------|--------|
| | 1 | 2A | 2B | 3 |
| RUNWAY 14L/32R | OPEN | OPEN | OPEN | OPEN |
| RUNWAY 4/22 | OPEN | OPEN | OPEN | OPEN |
| TAXIWAY A | OPEN | OPEN | OPEN | OPEN |
| TAXIWAY A5 | OPEN | OPEN | CLOSED | OPEN |
| TAXIWAY A6 | OPEN | OPEN | OPEN | CLOSED |
| TAXIWAY A7 | OPEN | OPEN | OPEN | OPEN |
| TAXIWAY C | OPEN | OPEN | OPEN | OPEN |
| TAXIWAY C3 | OPEN | OPEN | OPEN | OPEN |
| TAXIWAY C4 | OPEN | OPEN | OPEN | OPEN |

WORK AREA 3



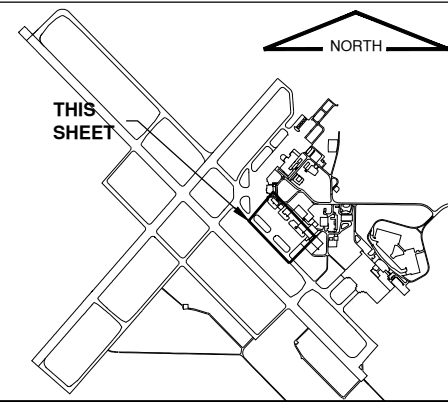
WORK AREA 2A NOTES

1. WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE FULL DEPTH PANEL REMOVAL AND REPLACEMENT, INSTALLATION OF TIE DOWNS, JOINT SEALING, AND PAVEMENT MARKINGS.
2. PHASE 2A IS A HIGH PRIORITY WORK AREA. SCHEDULING THIS WORK MAY REQUIRE 24 OPERATIONS TO COMPLETE.
3. PHASE 2A SHALL BE COMPLETED IN 7 CALENDAR DAYS.

WORK AREA 2B NOTES

1. WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE FULL DEPTH PANEL REMOVAL AND REPLACEMENT, INSTALLATION OF TIE DOWNS, JOINT SEALING, AND PAVEMENT MARKINGS.
2. WORK IN THIS PHASE REQUIRES THE CLOSURE OF TAXIWAY A5.

KEYMAP



LEGEND

- CONTRACTORS STAGING AND STORAGE AREA
- WORK AREA 2A
- WORK AREA 2B
- WORK AREA 3
- CONTRACTORS ACCESS ROUTE
- AIRCRAFT TAXI ROUTE
- BEAM BARRICADE / WORK AREA TXY A5/A6

NOTES

1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON ANY WORK AREA.
2. THE AIRPORT WILL REQUIRE 7 DAYS NOTICE PRIOR TO INITIATING WORK IN ANY WORK AREAS TO COORDINATE WITH THE INSTITUTE OF AVIATION & ISSUE NOTAMS.
3. WORK AREAS MAY BE ACTIVE CONCURRENTLY WITH THE APPROVAL OF THE AIRPORT.
4. STAGING AND STORAGE AREA SHALL BE RESTORED TO ORIGINAL CONDITION. PRIOR TO PROJECT COMPLETION.

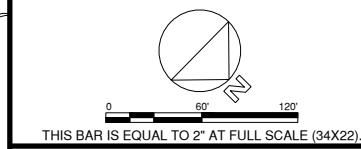
WORK AREA 3 NOTES

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2. WORK IN THIS PHASE REQUIRES THE CLOSURE OF TAXIWAY A6.
3. BARRICADE SPACING AT STATION 104+20 SHALL HAVE AN INTERVAL TO ALLOW FOR ARFF ACCESS.



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NOVEMBER 08, 2024

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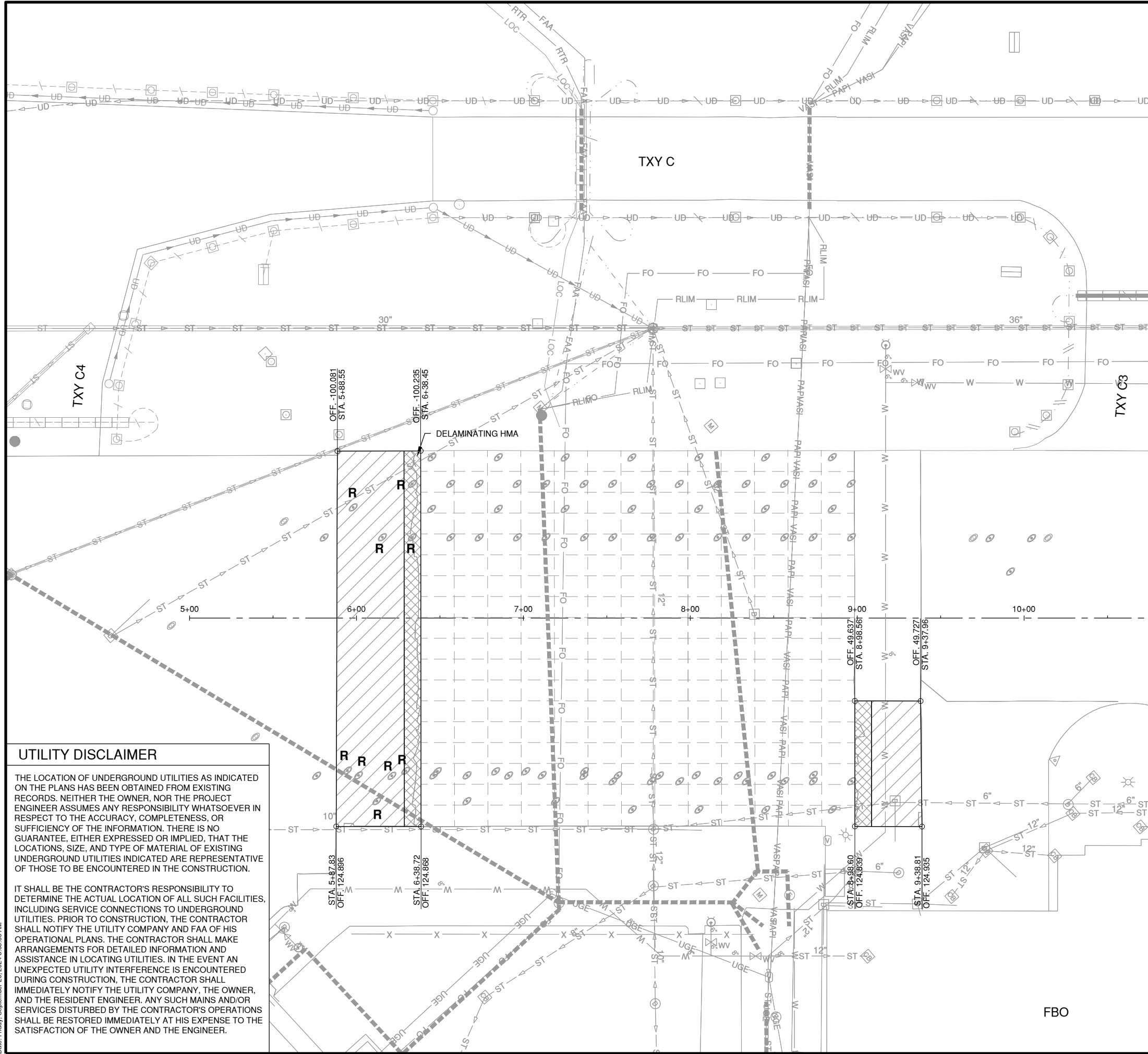
UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, IL

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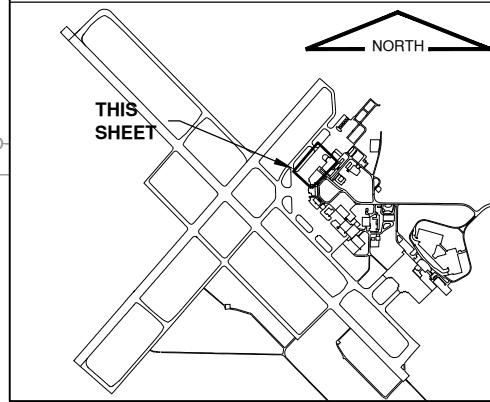
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| IL PROJ. NO: CMI-5043 | |
| CMT PROJECT NO: 21001968 | |
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| DESIGNED BY: MRK | |
| DRAWN BY: MRK | |
| CHECKED BY: MJD | |
| APPROVED BY: CBG | |
| COPYRIGHT: | |

SHEET TITLE
**CONSTRUCTION
ACTIVITY PLAN 2**

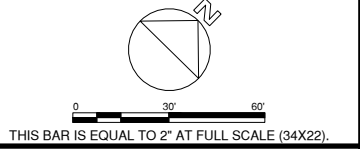
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KEYMAP



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LEGEND

- BITUMINOUS PAVEMENT MILLING
- NEW BUTT JOINT MILLING
- EXISTING DUCT BANK
- R** TO BE REMOVED
- EXISTING RUNWAY OR TAXIWAY EDGE LIGHT
- EXISTING GUIDANCE SIGN
- EXISTING HANDHOLE
- EXISTING SPLICE CAN
- EXISTING ELECTRICAL MANHOLE
- EXISTING STORM SEWER MANHOLE
- EXISTING STORM SEWER INLET
- SURVEY CONTROL POINT
- EXISTING ELECTRICAL CIRCUITS
- EXISTING VASI CIRCUIT
- EXISTING PAPI CIRCUIT
- EXISTING WATER LINE
- EXISTING GAS LINE
- EXISTING SANITARY SEWER LINE
- EXISTING FAA POWER / CONTROL CABLE
- EXISTING STORM SEWER LINE
- EXISTING UNDERDRAIN
- EXISTING LOCALIZER CIRCUIT
- EXISTING TIE DOWN
- EXISTING WATER VALVE
- EXISTING STORM SEWER MANHOLE
- EXISTING STORM SEWER INLET
- EXISTING CLEAN OUT
- EXISTING FIRE HYDRANT
- EXISTING OVERHEAD LIGHT

100%
NOVEMBER 08, 2024

REHABILITATE GENERAL AVIATION APRONS



UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, IL

UTILITY DISCLAIMER

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY AND FAA OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

NOTES

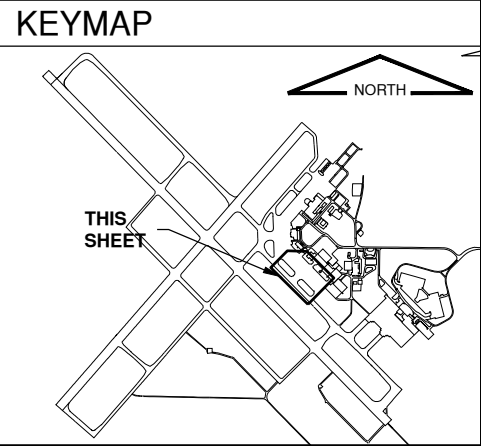
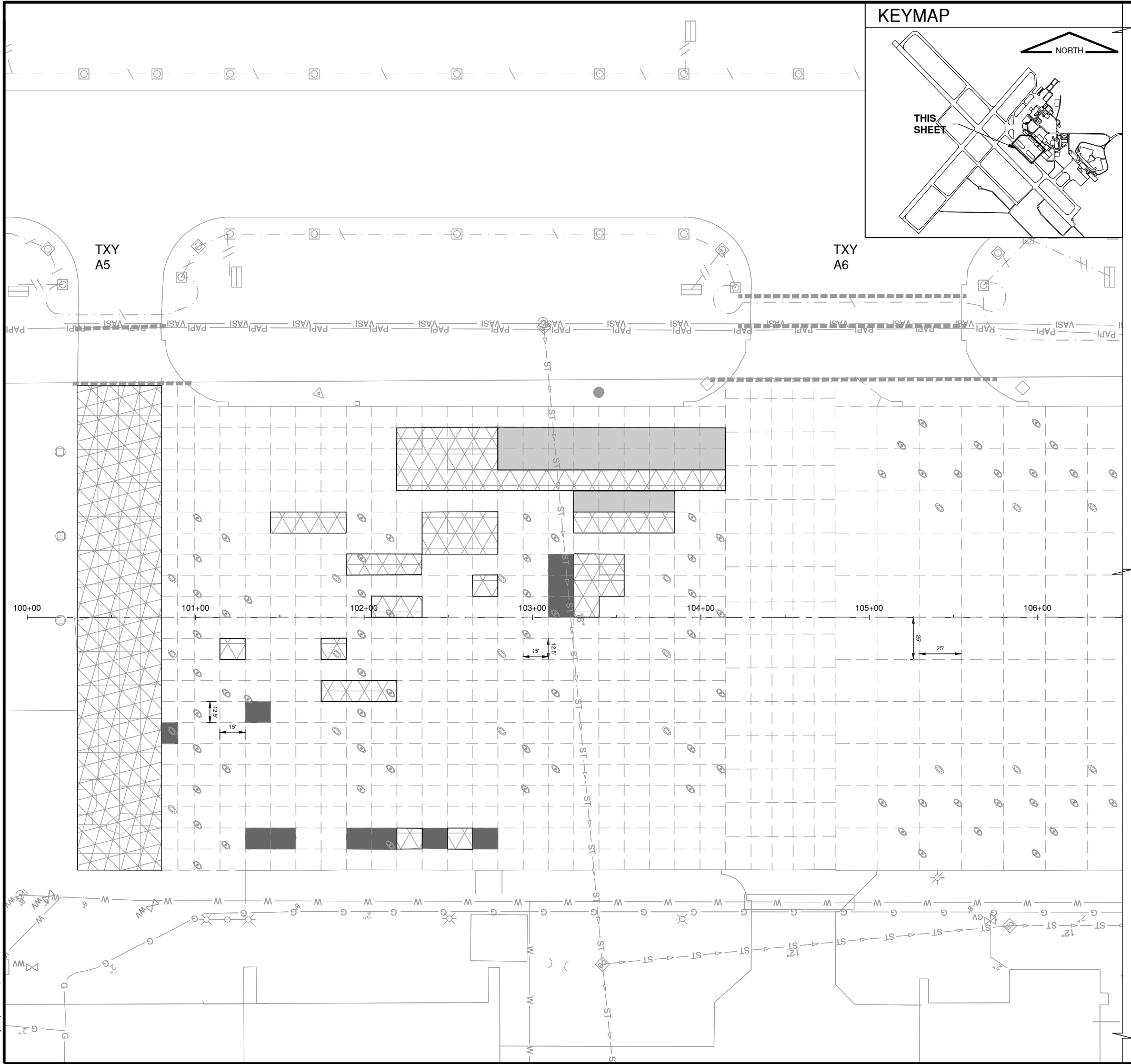
1. PAVEMENT MILLING SHALL INCLUDE MILLING INTO EXISTING HMA + PCC PAVEMENT AS SHOWN IN TYPICAL SECTIONS.
2. PAVEMENT STRUCTURES SHOWN WERE TAKEN FROM RECORD DRAWINGS AND ARE CONSIDERED REPRESENTATIVE OF THE "AS CONSTRUCTED" PAVEMENT SECTION WITH SOME VARIABILITY FROM THE THICKNESS INDICATED TO BE EXPECTED. THERE WILL BE NO ADDITIONAL PAYMENT TO THE CONTRACTOR DUE TO VARIATIONS IN SIZE OR QUANTITY OF EXISTING FEATURES.
3. EXISTING TIE DOWNS ON THIS SHEET ARE "TYPE A" ACCORDING TO RECORD DRAWINGS.

| MARK | DATE | DESCRIPTION |
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EXISTING CONDITIONS AND REMOVALS 1

CD101
SHEET 8 OF 26

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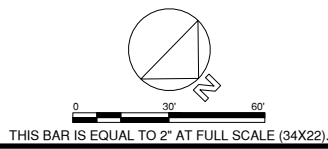
LEGEND

| | |
|----------|---------------------------------------|
| | REMOVE 9"-10" PCC PANEL - BB |
| | REMOVE 9"-10" PCC PANEL - AA1 |
| | REMOVE 9"-10" PCC PANEL - AA2 |
| | REMOVE 16"-18" & VAR. PCC PANEL - BB |
| | EXISTING DUCT BANK |
| R | TO BE REMOVED |
| | EXISTING RUNWAY OR TAXIWAY EDGE LIGHT |
| | EXISTING GUIDANCE SIGN |
| | EXISTING HANDHOLE |
| | EXISTING SPLICE CAN |
| | EXISTING ELECTRICAL MANHOLE |
| | EXISTING STORM SEWER MANHOLE |
| | EXISTING STORM SEWER INLET |
| | SURVEY CONTROL POINT |
| | EXISTING ELECTRICAL CIRCUITS |
| | EXISTING VASI CIRCUIT |
| | EXISTING PAPI CIRCUIT |
| | EXISTING WATER LINE |
| | EXISTING GAS LINE |
| | EXISTING SANITARY SEWER LINE |
| | EXISTING FAA POWER / CONTROL CABLE |
| | EXISTING STORM SEWER LINE |
| | EXISTING UNDERDRAIN |
| | EXISTING LOCALIZER CIRCUIT |
| | EXISTING TIE DOWN |
| | EXISTING WATER VALVE |
| | EXISTING STORM SEWER MANHOLE |
| | EXISTING STORM SEWER INLET |
| | EXISTING CLEAN OUT |
| | EXISTING FIRE HYDRANT |
| | EXISTING OVERHEAD LIGHT |



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NOVEMBER 08, 2024

REHABILITATE GENERAL AVIATION APRONS



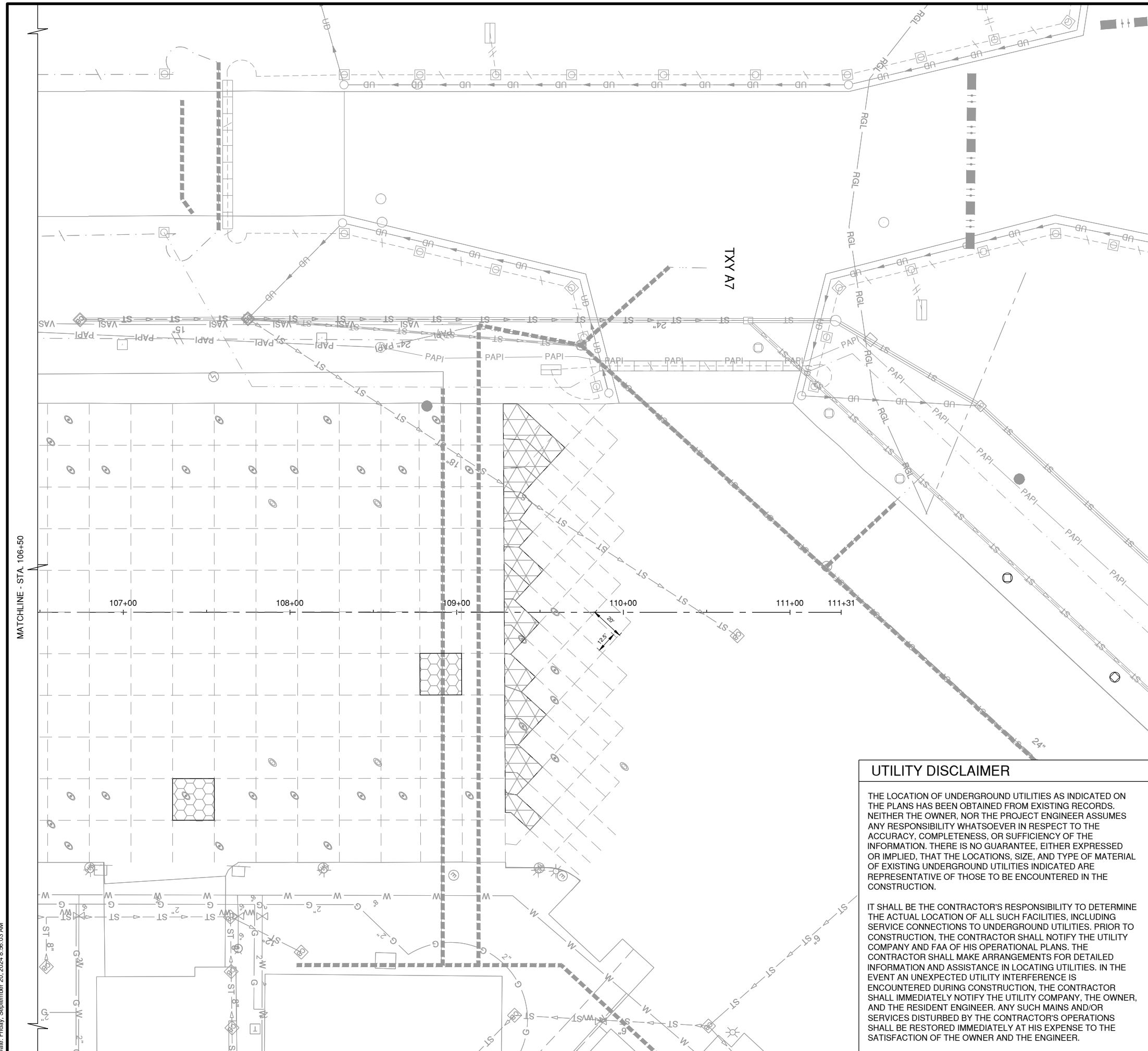
UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, IL

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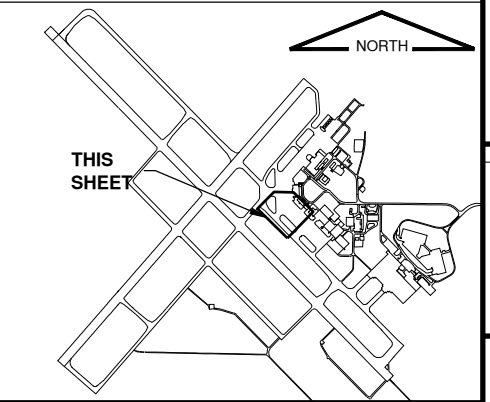
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| DESIGNED BY: MRK | |
| DRAWN BY: MRK | |
| CHECKED BY: MJD | |
| APPROVED BY: CBG | |
| COPYRIGHT: | |

SHEET TITLE
EXISTING CONDITIONS AND REMOVALS 2
CD102
SHEET 9 OF 26

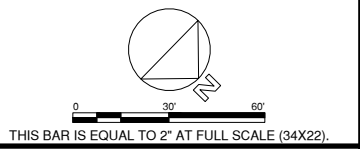
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KEYMAP



License No. 184-000613
CONSULTANTS



LEGEND

- REMOVE 9'-10" PCC PANEL - BB
- REMOVE 9'-10" PCC PANEL - AA1
- REMOVE 9'-10" PCC PANEL - AA2
- REMOVE 16'-18" & VAR. PCC PANEL - BB
- EXISTING DUCT BANK
- R** TO BE REMOVED
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- EXISTING WATER VALVE
- EXISTING STORM SEWER MANHOLE
- EXISTING STORM SEWER INLET
- EXISTING CLEAN OUT
- EXISTING FIRE HYDRANT
- EXISTING OVERHEAD LIGHT

UTILITY DISCLAIMER

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NOVEMBER 08, 2024

REHABILITATE GENERAL AVIATION APRONS



UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, IL

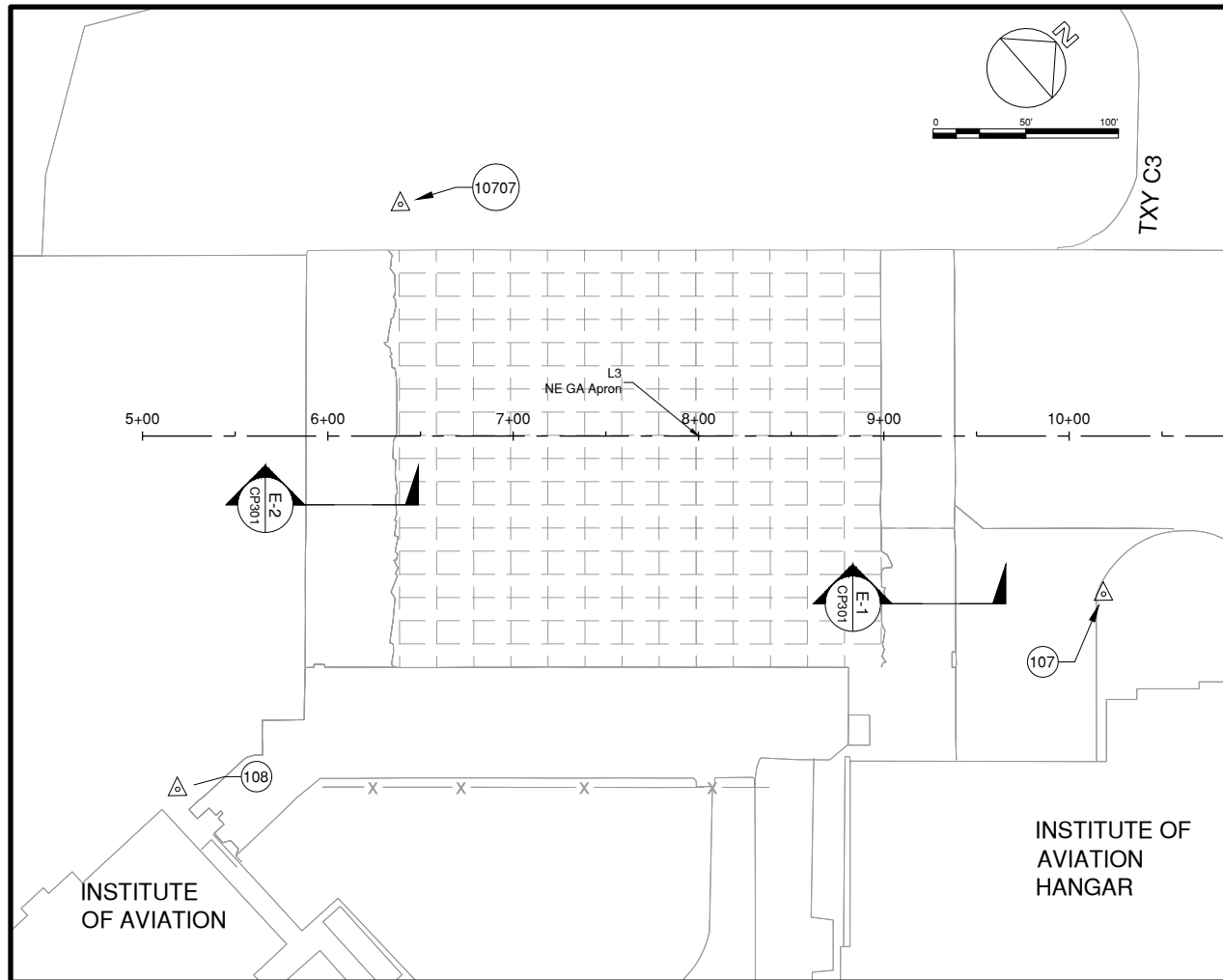
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EXISTING CONDITIONS AND REMOVALS 3

CD103
SHEET 10 OF 26

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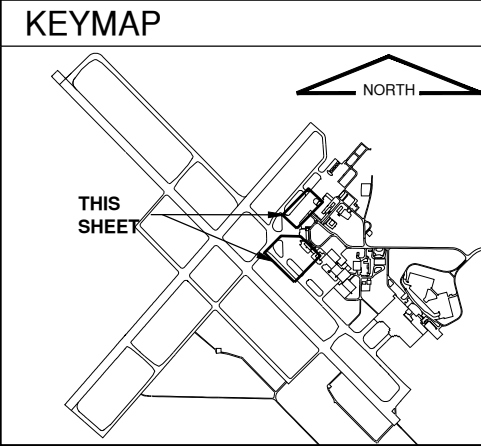
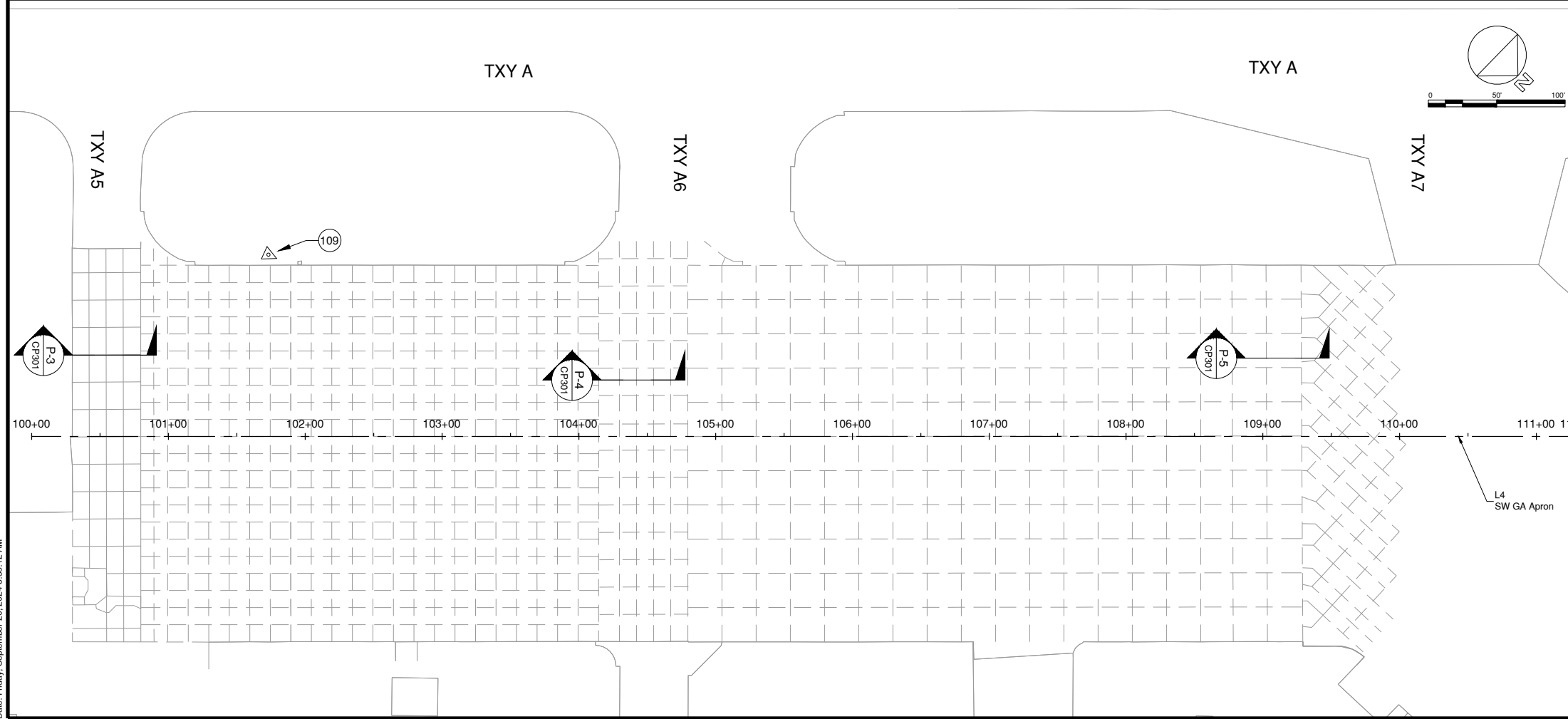
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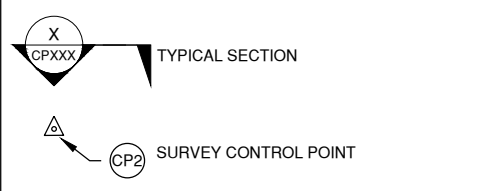
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|---------------------|-------------|-------------|-----------|
| POINT | NORTHING | EASTING | ELEVATION |
| 107 | 1229062.206 | 1001940.721 | 746.18 |
| 108 | 1228616.231 | 1001692.195 | 749.09 |
| 109 | 1227561.381 | 1002045.536 | 744.46 |
| 10707 | 1228914.119 | 1001533.173 | 745.32 |

| ALIGNMENT DATA | | | | | |
|-------------------|-------------|------------------------------|----------------------------------|------------------------------|------------------------------------|
| ALIGNMENT SEGMENT | ALIGNMENT | BEGIN STATION/ PI STATION | NORTHING/ EASTING | END STATION/ CURVE RADIUS | NORTHING/ EASTING |
| L3 | NE GA Apron | STA. 5+00.00 | N:1228727.1135 E:1001536.0511 | STA. 11+00.00 | N: 1229179.6843 E: 1001929.9797 |

| ALIGNMENT DATA | | | | | |
|-------------------|-------------|------------------------------|----------------------------------|------------------------------|------------------------------------|
| ALIGNMENT SEGMENT | ALIGNMENT | BEGIN STATION/ PI STATION | NORTHING/ EASTING | END STATION/ CURVE RADIUS | NORTHING/ EASTING |
| L4 | SW GA Apron | STA. 100+00.00 | N:1227530.7828 E:1002261.5222 | STA. 111+30.67 | N: 1228338.4549 E: 1001470.2687 |



LEGEND



NOTES

1. ALL PROJECT BENCHMARKS AND CONTROL POINTS SHOULD BE FIELD CHECKED FOR POSSIBLE DISTURBANCE PRIOR TO UTILIZING THEM FOR STAKING PURPOSES.
2. IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM THE COORDINATES BASED ON THE DIMENSIONS SHOWN IN THE PLANS.
3. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING THIS DRAWING WITH THE OTHER DRAWINGS THAT CONTAIN DIMENSIONS TO ENSURE THAT THE PLAN DIMENSIONS ARE CONSISTENT WITH THE COORDINATES PRESENTED ON THE PLAN. FOLLOWING STAKEOUT OF ANY FACILITY BY COORDINATES, THE CONTRACTOR SHALL CONFIRM CONSISTENT WITH THE PLAN DIMENSIONS PRIOR TO CONSTRUCTION.

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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

100%
 NOVEMBER 08, 2024

REHABILITATE GENERAL AVIATION APRONS

OWNER

UNIVERSITY OF ILLINOIS
 WILLARD AIRPORT
 SAVOY, IL

| MARK | DATE | DESCRIPTION |
|------|------|-------------|
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| AIP PROJ. NO: | |
| IL PROJ. NO: CMI-5043 | |
| CMT PROJECT NO: 21001968 | |
| CAD DWG FILE: 21001968-CP100.DWG | |
| DESIGNED BY: MRK | |
| DRAWN BY: MRK | |
| CHECKED BY: MJD | |
| APPROVED BY: CBG | |
| COPYRIGHT: | |

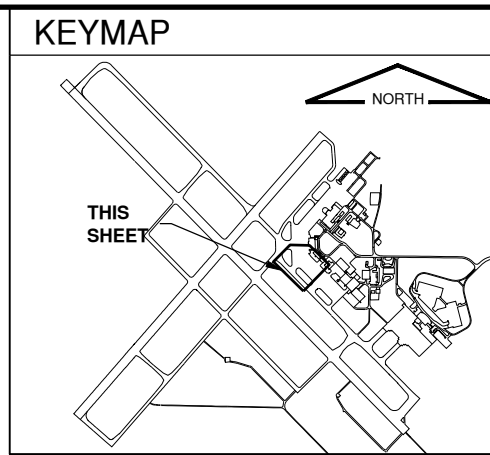
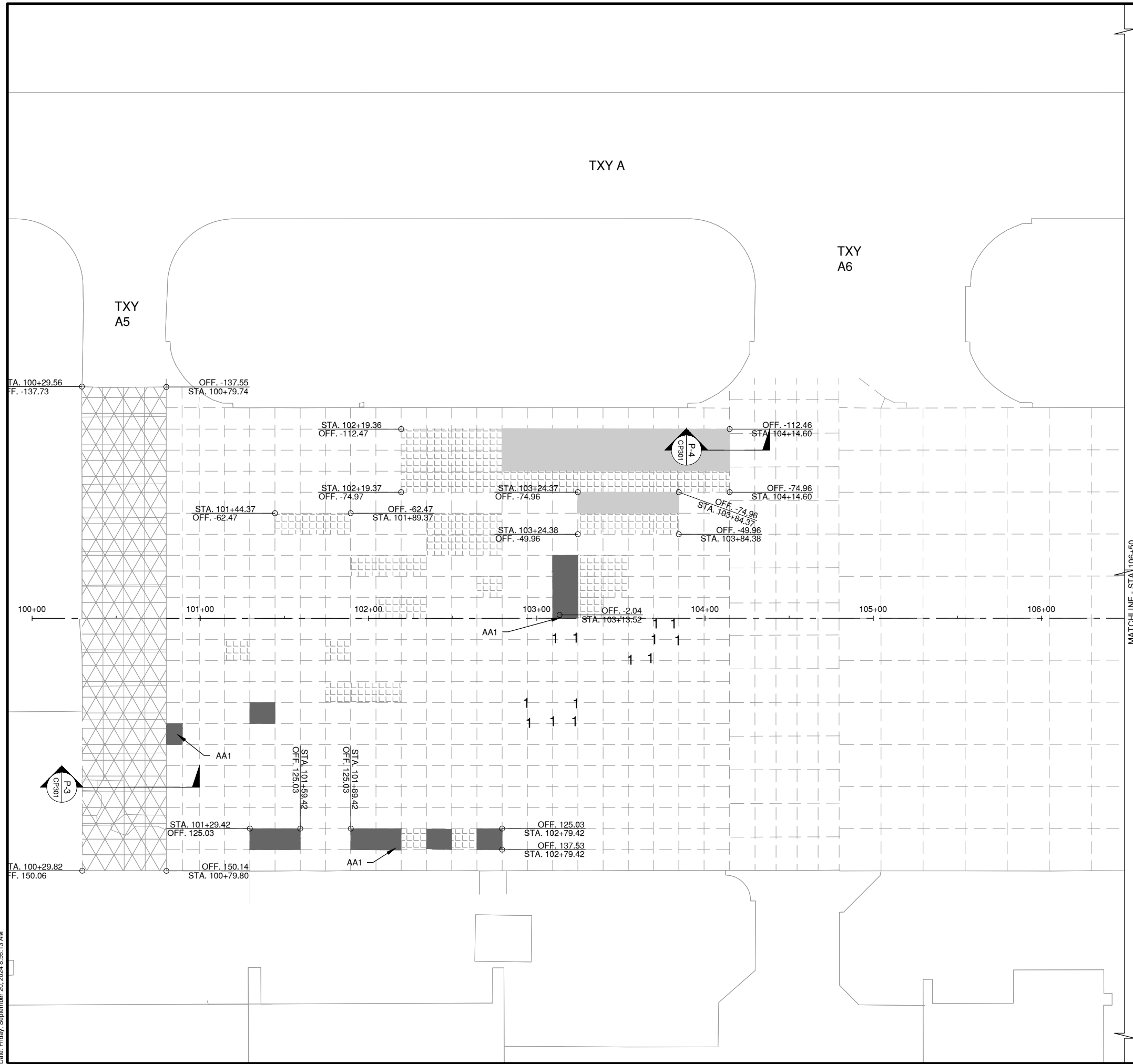
SHEET TITLE

PROJECT CONTROL AND ALIGNMENT DATA

CP100

SHEET 11 OF 26

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 Date: Friday, September 20, 2024 8:56:13 AM



CMT
 License No. 184-000613
 CONSULTANTS

LEGEND

- NEW 9" & VAR. PCC PANEL REPLACEMENT
- NEW 10" & VAR. PCC PANEL REPLACEMENT
- NEW 16"-18" & VAR. PCC PANEL REPLACEMENT
- NEW 10" & VAR. PCC PANEL REPLACEMENT - AA1
- NEW 10" & VAR. PCC PANEL REPLACEMENT - AA2
- PCC SPALL REPAIR - BB
SEE DETAIL 7 CP501
- TYPICAL SECTION

100%
 NOVEMBER 08, 2024

NOTES

- CONTRACTOR SHALL REGRADE AND RECOMPACT BASE TO THE SATISFACTION OF THE RPR IF EXISTING BASE IS DISTURBED AFTER PAVEMENT REMOVAL.
- FINAL PANEL LOCATIONS TO BE DETERMINED BY THE RESIDENT ENGINEER. JOINT SPALL PANEL REPAIRS SHOWN ARE APPROXIMATE. EXACT REPAIR TYPE AND LOCATION SHALL BE COMPLETED AS DIRECTED BY THE ENGINEER.
- ADDITIONAL QUANTITY OF SPALL REPAIR IS INCLUDED IN PLANNED QUANTITY FOR THE RESIDENT ENGINEER TO USE AT THEIR DISCRETION. ADDITIVE ALTERNATE 3 INCLUDES SUPPLEMENTAL QUANTITY AS WELL.
- ALL JOINTS WITH PATCHES SHALL BE REESTABLISHED TO PREVENT RANDOM CRACKING.
- CONTRACTOR SHALL TAKE SPECIAL CARE NOT TO DAMAGE ADJACENT PANELS.
- CONTRACTOR SHALL PROVIDE DUST CONTROL FOR ALL OPERATIONS GENERATING DUST. DUST CONTROL SHALL BE CONSIDERED INCIDENTAL TO THE WORK FOR THE PROPOSED IMPROVEMENTS.

REHABILITATE GENERAL AVIATION APRONS

OWNER

 UNIVERSITY OF ILLINOIS
 WILLARD AIRPORT
 SAVOY, IL

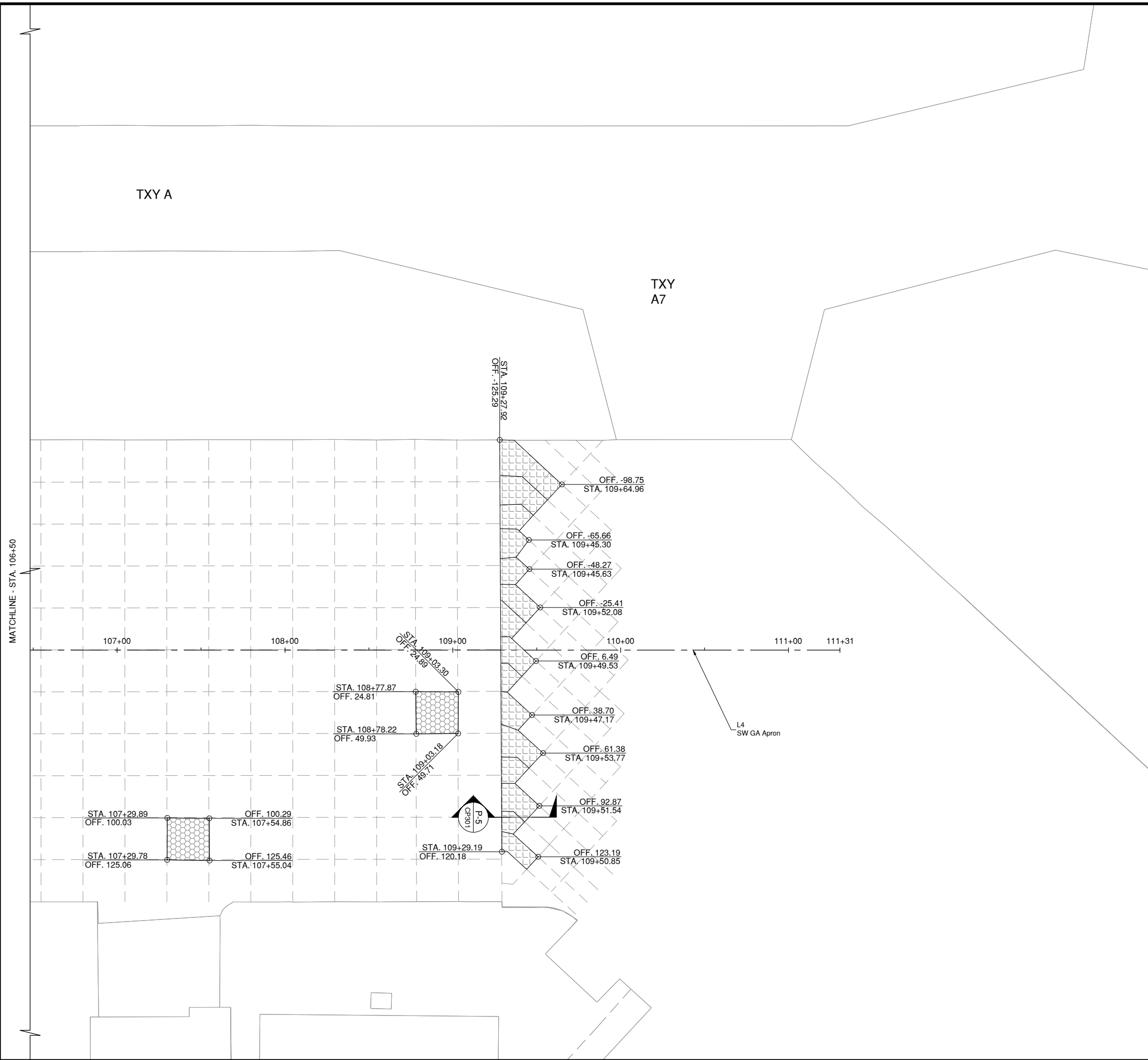
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| AIP PROJ. NO: | |
| IL PROJ. NO: CMI-5043 | |
| CMT PROJECT NO: 21001968 | |
| CAD DWG FILE: 21001968-CP100.DWG | |
| DESIGNED BY: MRK | |
| DRAWN BY: MRK | |
| CHECKED BY: MJD | |
| APPROVED BY: CBG | |
| COPYRIGHT: | |

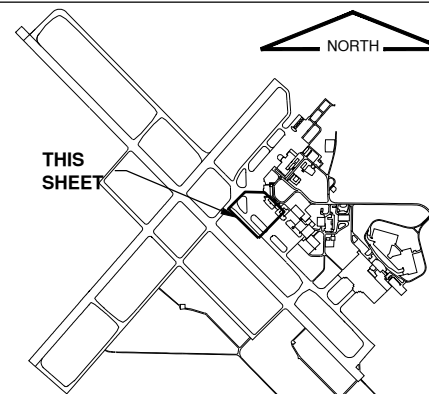
SHEET TITLE
PROPOSED IMPROVEMENTS 2

CP102
 SHEET 13 OF 26

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Date: Friday, September 20, 2024 8:56:14 AM



KEYMAP



LEGEND

- NEW 9" & VAR. PCC PANEL REPLACEMENT
- NEW 10" & VAR. PCC PANEL REPLACEMENT
- NEW 16"-18" & VAR. PCC PANEL REPLACEMENT
- TYPICAL SECTION

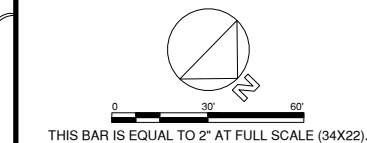
NOTES

- CONTRACTOR SHALL REGRADE AND RECOMPACT BASE TO THE SATISFACTION OF THE RPR IF EXISTING BASE IS DISTURBED AFTER PAVEMENT REMOVAL.
- FINAL PANEL LOCATIONS TO BE DETERMINED BY THE RESIDENT ENGINEER.



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NOVEMBER 08, 2024

REHABILITATE GENERAL
AVIATION APRONS

OWNER



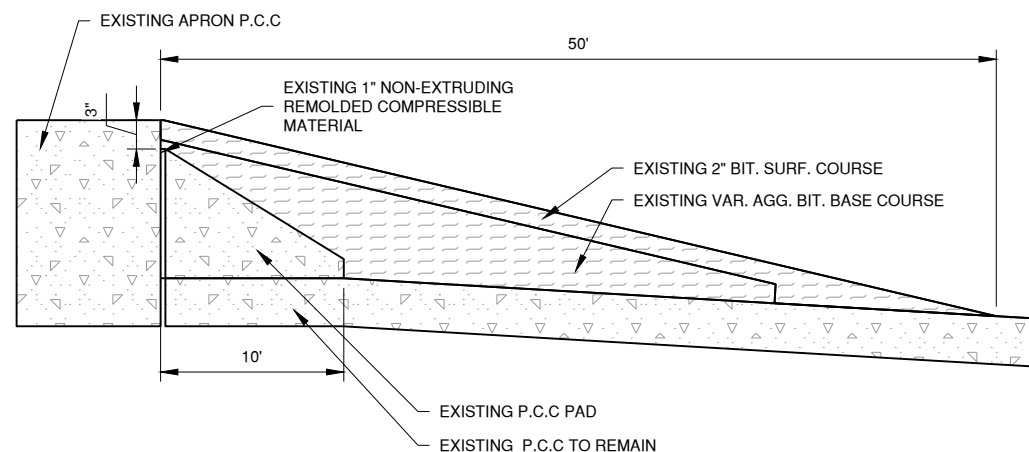
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WILLARD AIRPORT
SAVOY, IL

MARK | DATE | DESCRIPTION

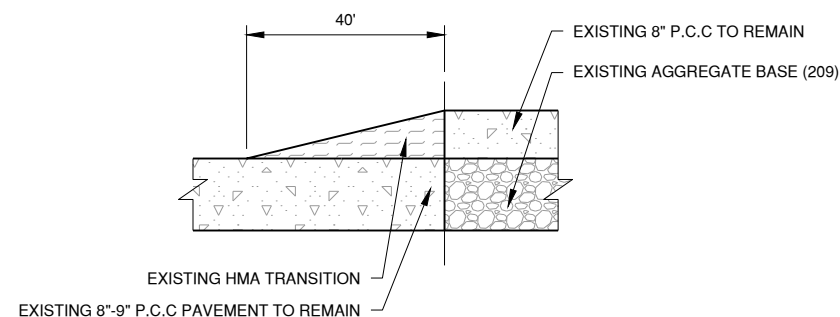
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| CMT PROJECT NO: 21001968 | |
| CAD DWG FILE: 21001968-CP100.DWG | |
| DESIGNED BY: MRK | |
| DRAWN BY: MRK | |
| CHECKED BY: MJD | |
| APPROVED BY: CBG | |
| COPYRIGHT: | |

SHEET TITLE
**PROPOSED
IMPROVEMENTS 3**

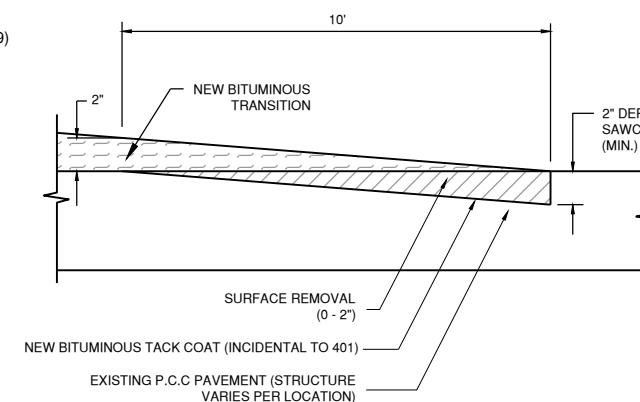
CP103
SHEET 14 OF 26



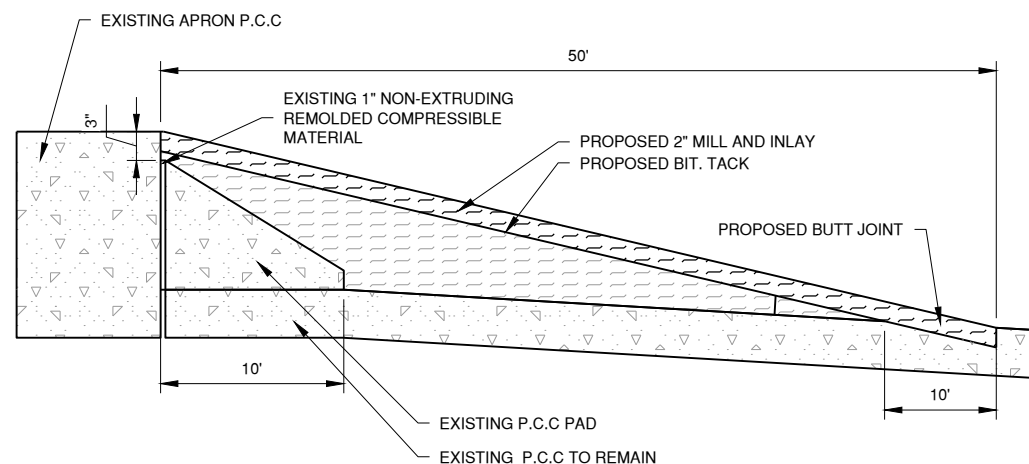
EXISTING TYPICAL SECTION HMA TRANSITION
N.T.S. **E-1**
CD101



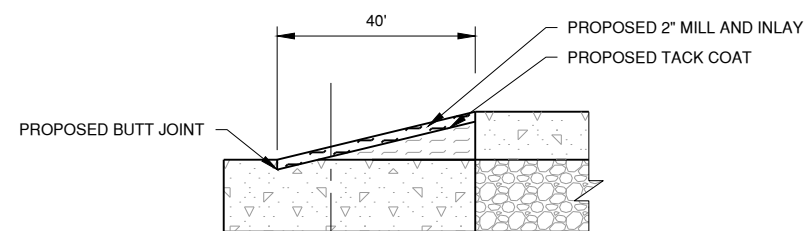
EXISTING TYPICAL SECTION HMA TRANSITION
N.T.S. **E-2**
CD101



BUTT JOINT CONSTRUCTION DETAIL (AR401655)
NOT TO SCALE



PROPOSED TYPICAL SECTION HMA TRANSITION
N.T.S. **P-1**
CP101



PROPOSED TYPICAL SECTION HMA TRANSITION
N.T.S. **P-2**
CP101

EXISTING CONDITIONS NOTES

1. PAVEMENT STRUCTURES SHOWN WERE TAKEN FROM RECORD DRAWINGS AND ARE CONSIDERED REPRESENTATIVE OF THE "AS CONSTRUCTED" PAVEMENT SECTION WITH SOME VARIABILITY FROM THE THICKNESS INDICATED TO BE EXPECTED. THERE WILL BE NO ADDITIONAL PAYMENT TO THE CONTRACTOR DUE TO VARIATIONS IN SIZE OR QUANTITY OF EXISTING FEATURES.
2. CONTRACTOR SHALL REGRADE AND RECOMPACT BASE TO THE SATISFACTION OF THE RPR IF EXISTING BASE IS DISTURBED AFTER PAVEMENT REMOVAL.

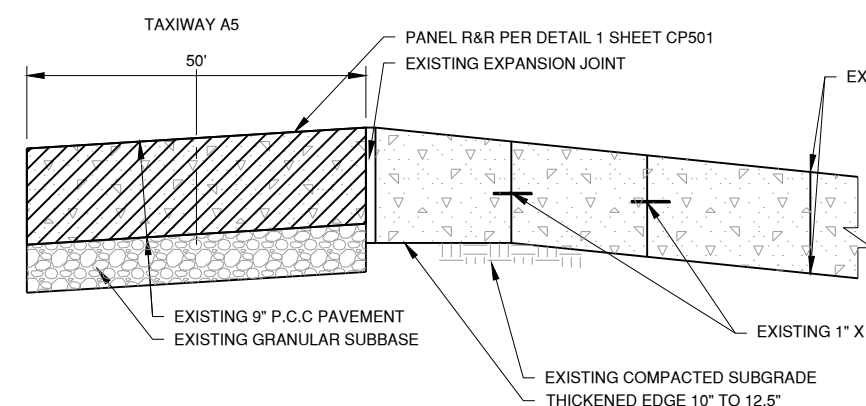
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NOVEMBER 08, 2024

REHABILITATE GENERAL
AVIATION APRONS

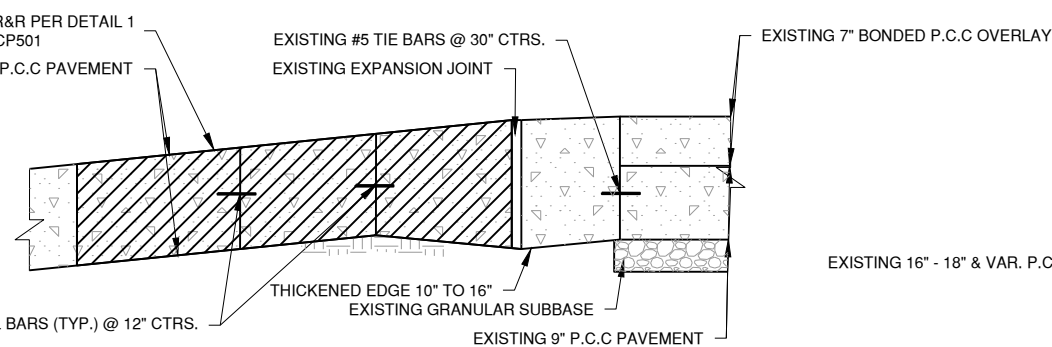
OWNER



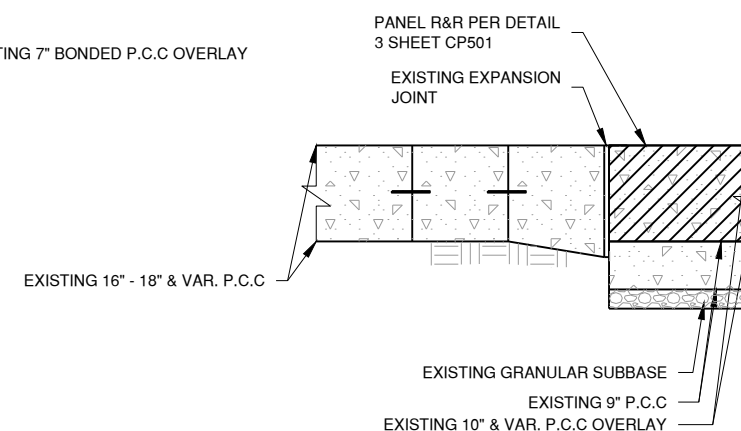
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SAVOY, IL



PROPOSED TYPICAL SECTION GA APRON
N.T.S. **P-3**
CD101



PROPOSED TYPICAL SECTION GA APRON
N.T.S. **P-4**
CD101

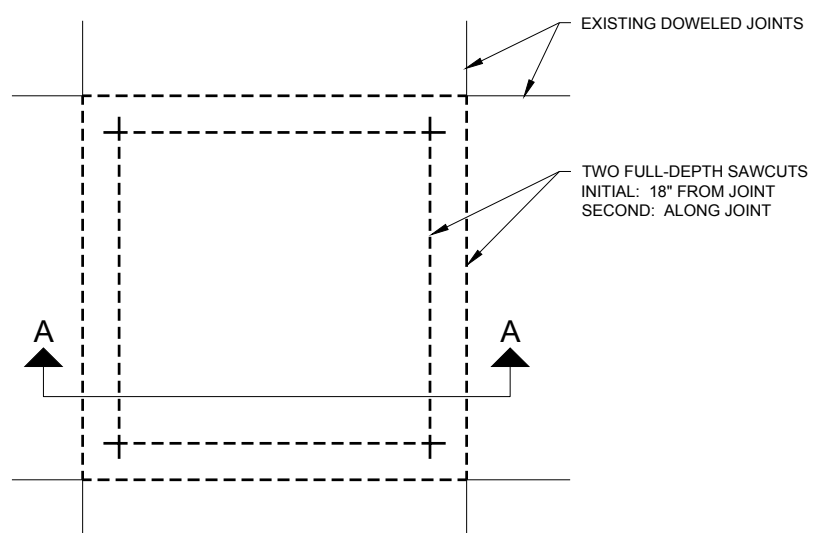


PROPOSED TYPICAL SECTION GA APRON
N.T.S. **P-5**
CD101

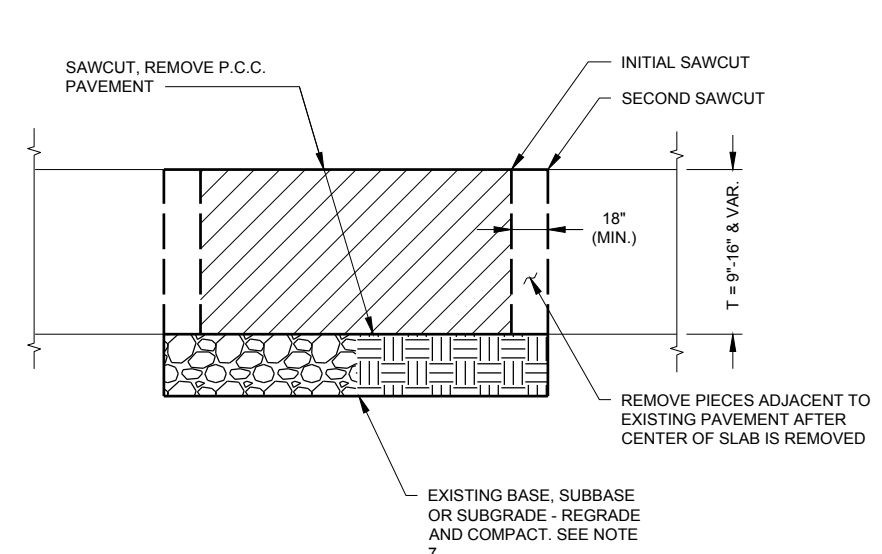
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| IL PROJ. NO: CMI-5043 | |
| CMT PROJECT NO: 21001968 | |
| CAD DWG FILE: 21001968-CP300.DWG | |
| DESIGNED BY: MRK | |
| DRAWN BY: MRK | |
| CHECKED BY: MJD | |
| APPROVED BY: CBG | |
| COPYRIGHT: | |

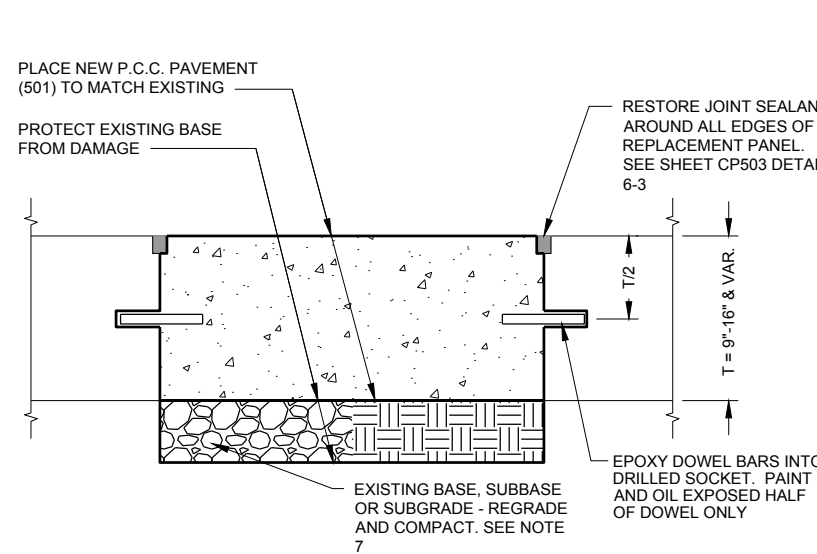
SHEET TITLE
TYPICAL SECTIONS



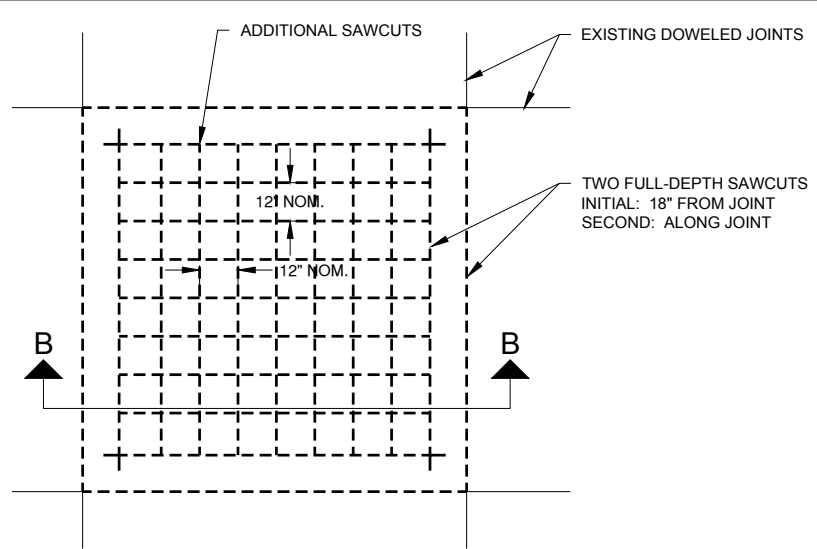
1 FULL PANEL DEMOLITION SAWCUT DETAIL
N.T.S.



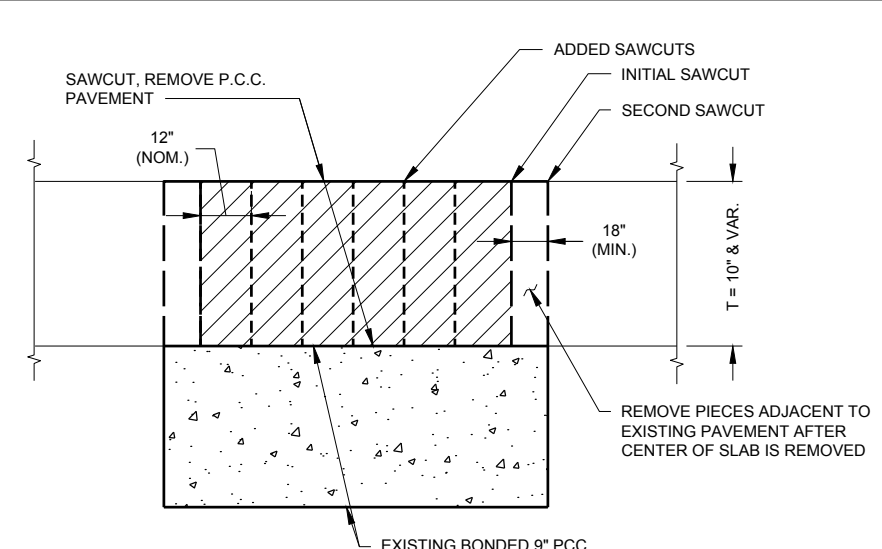
2 PCC PANEL - REMOVAL DETAIL (SECTION A-A)
N.T.S.



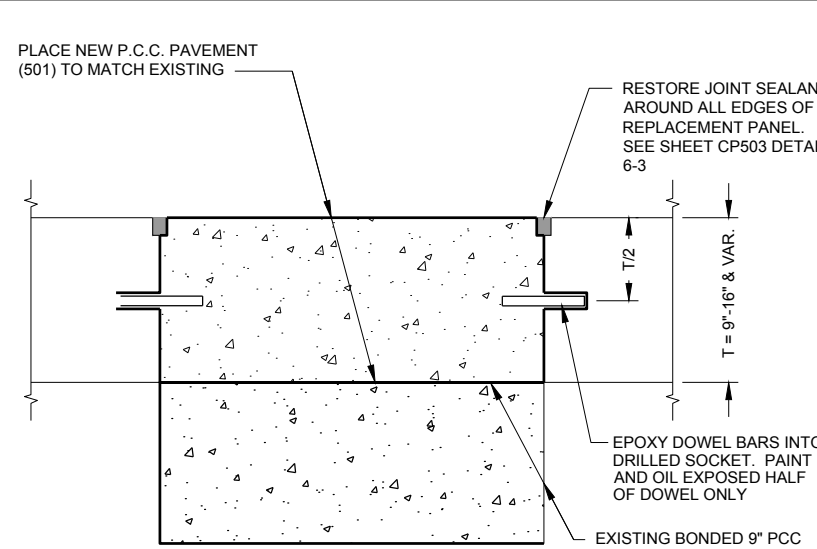
5 PCC PANEL - PLACEMENT DETAIL
N.T.S.



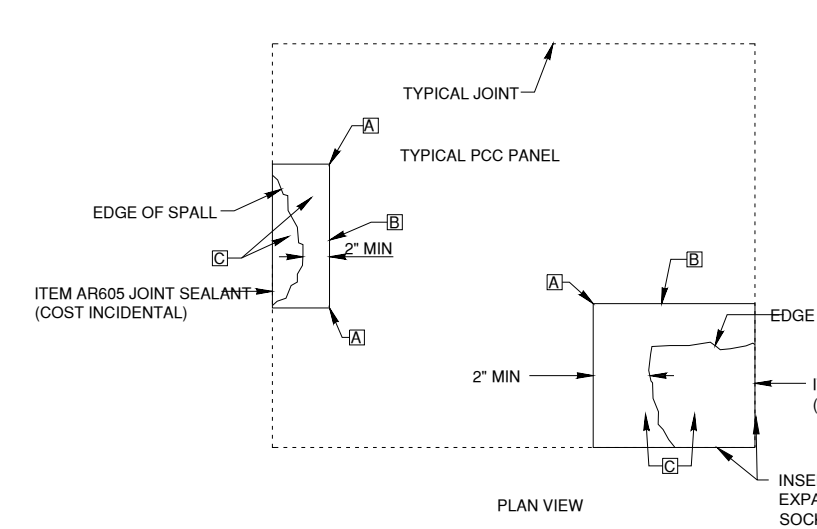
3 FULL PANEL DEMOLITION W/ BONDED PCC
N.T.S.



4 BONDED OVERLAY - REMOVAL DETAIL (SECTION B-B)
N.T.S.



6 PCC PANEL - PLACEMENT DETAIL
N.T.S.



7 PCC SPALL REPAIR
N.T.S.

SPALL REPAIR NOTES

- A. CONTRACTOR SHALL NOT OVERSAW. WORK SHALL BE ACCORDING TO STEPS STATED IN 501-3.24.
 - B. CONTRACTOR SHALL SAWCUT 3" DEPTH EXCEPT AS NECESSARY TO NOT OVERSAW.
 - C. CONTRACTOR SHALL REMOVE EXISTING PCC DOWN TO UNDISTURBED MATERIAL. VOID SHALL BE FILLED WITH ELASTOMERIC CONCRETE AS SPECIFIED IN ITEM 501 FOR SPALL REPAIR.
1. SAWCUTS MAY BE MADE A MAXIMUM OF 24 HOURS PRIOR TO PAVEMENT REMOVAL. HOWEVER, ALL SAWCUT CLURRY AND DUST MUST BE REMOVED BY THE END OF THE WORK SHIFT.
 2. MINIMUM DISTANCE BETWEEN ADJACENT EDGE SPALL PATCHES IS 2 FEET. IF LESS SPACE IS REQUIRED, PATCHES SHOULD BE COMBINED AND CONTINUOUS. IF DISTANCE BETWEEN SPALL REPAIR AND SLAB CORNER IS LESS THAN 18 INCHES, EXTEND PATCH TO SLAB CORNER
 3. SEE JOINT SEALING DETAILS ON SHEET CP503.
 4. SPALL WIDTH SHALL NOT EXCEED 24"
 5. SHOULD THE CONTRACTOR'S OPERATION DAMAGE ADJACENT PAVEMENT, THE CONTRACTOR SHALL REPAIR AS DIRECTED BY THE RESIDENT ENGINEER AT THE CONTRACTOR'S EXPENSE.
 6. WORK SHOWN SHALL BE INCIDENTAL TO PAY ITEM 501922- PCC SPALL REPAIR

REMOVE AND REPLACE PCC PANEL NOTES

1. DEMOLITION SHALL FOLLOW DETAILS 1-4. PCC REPLACEMENT SHALL FOLLOW DETAIL 5-6.
2. TO PROTECT UNDERLYING ELECTRICAL CONDUITS FROM DAMAGE, GUILLOTINE OR HOE RAM SHALL NOT BE USED TO BREAK PAVEMENT IN THE REMOVAL AREA.
3. FOR MULTIPLE PANEL REPLACEMENTS REQUIRING A TYPE B OR C CONTRACTION JOINT, SAW AND SEAL PER DETAIL 6-2 SHEET CP503.
4. SAW CUT OVERCUTS SHALL BE THOROUGHLY CLEANED AND COMPLETELY SEALED.
5. WHERE A LONGITUDINAL CRACK EXISTS IN AN ADJACENT PANEL, PLACE 2 LAYERS OF TWO #4 REBAR, 3' LONG AND CENTERED ON THE CRACK, AS SHOWN IN DETAIL 3.
6. PANEL REMOVAL AND REPLACEMENT SHALL BE AS SHOWN ON SHEET CP101-CP103 AND AS DIRECTED BY THE RESIDENT ENGINEER.
7. CONTRACTOR SHALL REPLACE BASE COURSE AGGREGATE LOSS DUE TO PAVEMENT REMOVALS W/ P-209 AGGREGATE & COMPACT TO SATISFACTION OF THE RPR.

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REHABILITATE GENERAL
AVIATION APRONS

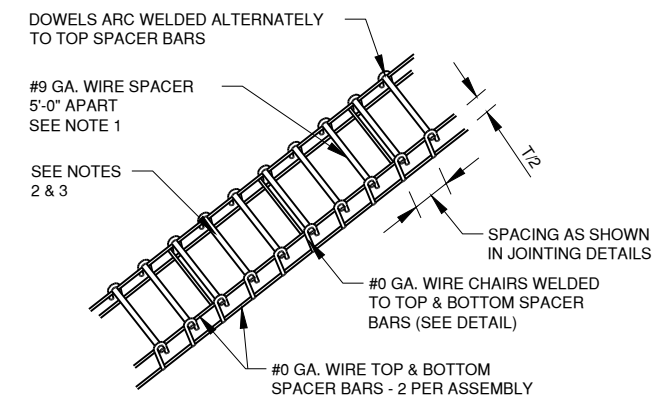


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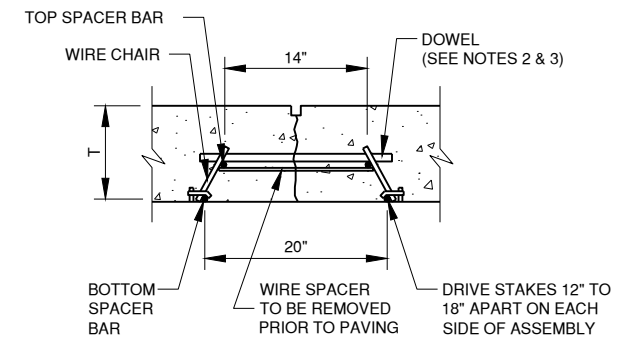
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| IL PROJ. NO: CMI-5043 | |
| CMT PROJECT NO: 21001968 | |
| CAD DWG FILE: 21001968-CP500.DWG | |
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| DRAWN BY: MRK | |
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| COPYRIGHT: | |

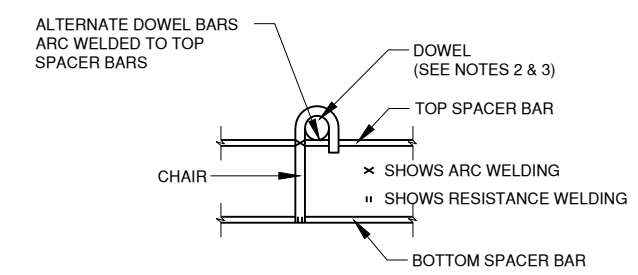
SHEET TITLE
PAVING DETAILS 2



3 DOWEL BASKET ASSEMBLY
N.T.S.



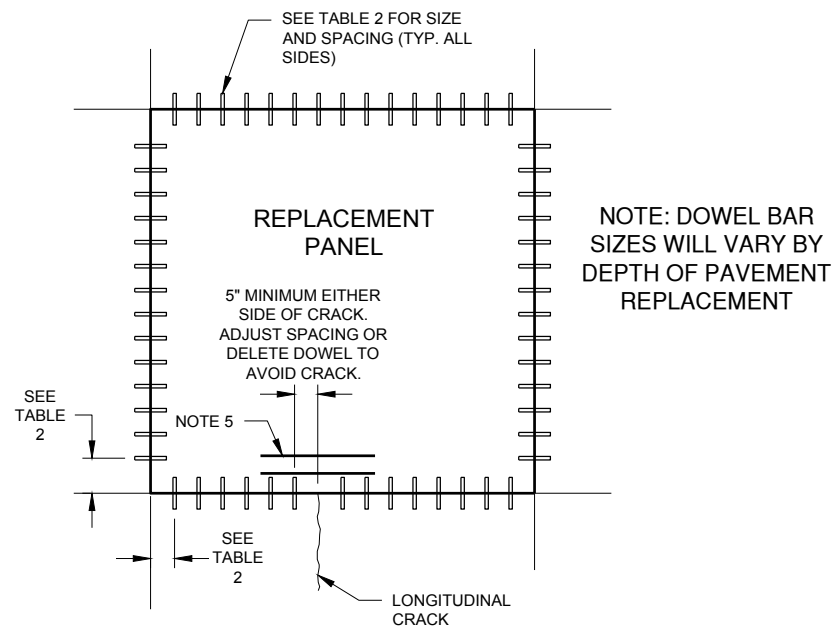
4 DOWEL BAR INSTALLATION
N.T.S.



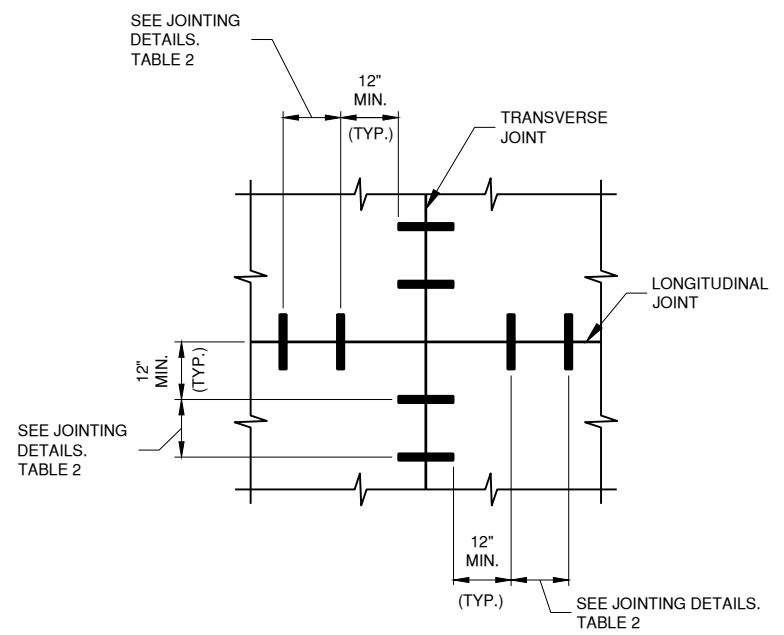
5 TYPICAL DOWEL BASKET ELEVATION SHOWING CHAIR
N.T.S.

DOWEL BASKET NOTES

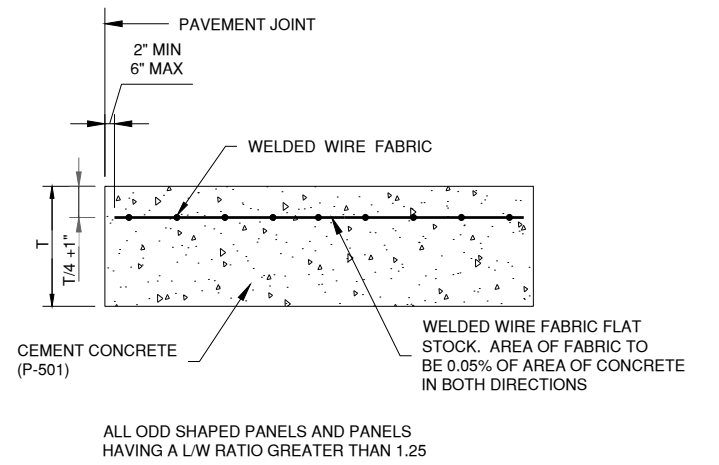
- #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
- DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 2.
- DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.



2 DOWEL BAR LAYOUT
N.T.S.

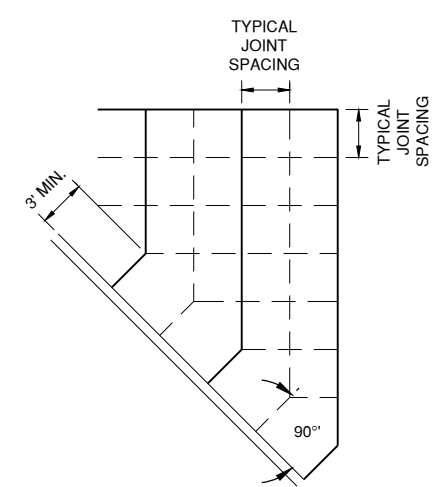


1 DOWEL PLACEMENT
N.T.S.



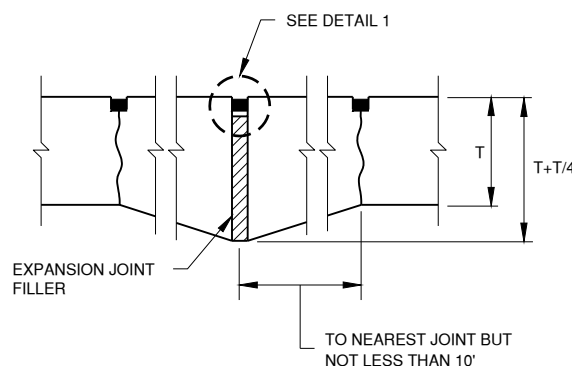
7 ODD SHAPE PANEL REINFORCEMENT
N.T.S.
SYMBOL R

ALL ODD SHAPED PANELS AND PANELS HAVING A L/W RATIO GREATER THAN 1.25

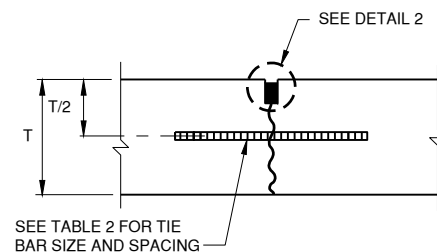


6 JOINTING AT SKEWED EDGE
N.T.S.

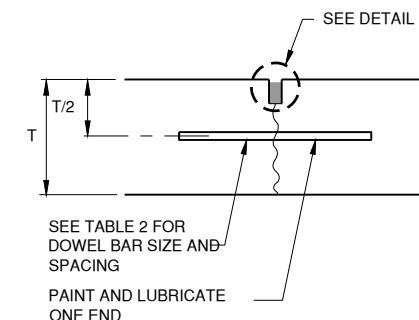
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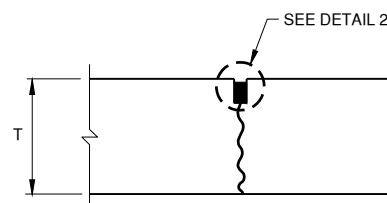
1 TYPE A THICKENED ISOLATION
N.T.S. SYMBOL — A —



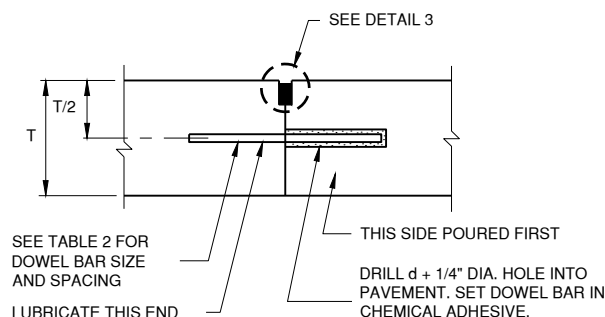
2 TYPE B HINGED (TIED) CONTRACTION
N.T.S. SYMBOL — B —



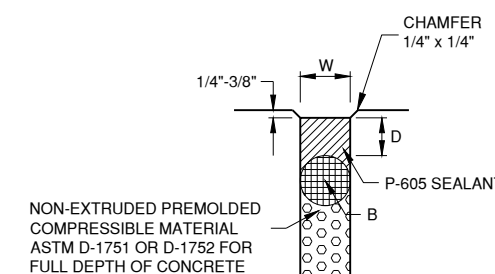
3 TYPE C DOWELED CONTRACTION
N.T.S. SYMBOL — C —



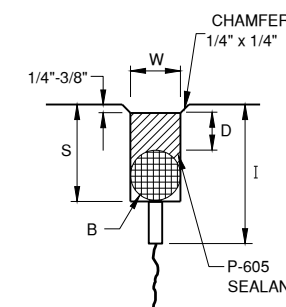
4 TYPE D DUMMY CONTRACTION
N.T.S. SYMBOL — D —



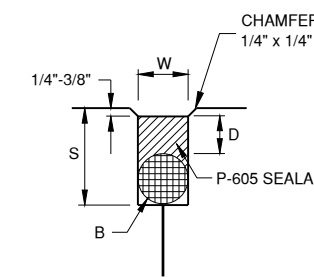
5 JOINT SEALING DETAILS
N.T.S. SYMBOL — E —



DETAIL 1



DETAIL 2



DETAIL 3

6 JOINT SEALING DETAILS
N.T.S.

| PAVEMENT THICKNESS T - INCHES | DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES $I=(T/3) \pm 1/4"$ |
|-------------------------------|--|
| 5 | 1.67" |
| 6 | 2.00" |
| 7 | 2.33" |
| 8 | 2.67" |
| 9 | 3.00" |
| 10 | 3.33" |
| 11 | 3.67" |
| 12 | 4.00" |
| 13 | 4.33" |
| 14 | 4.67" |
| 15 | 5.00" |
| 16 | 5.33" |
| 17 | 5.67" |
| 18 | 6.00" |

| PAVEMENT THICKNESS T - INCHES | DOWEL BAR DETAILS | | | TIE BAR DETAILS | | |
|-------------------------------|-------------------|--------|---------|-----------------|--------|---------|
| | DIA. | LENGTH | SPACING | BAR SIZE | LENGTH | SPACING |
| 5 | 5/8" | 12" | 12" | #4 | 24" | 30" |
| 6 | 3/4" | 18" | 12" | #5 | 30" | 30" |
| 7 | 3/4" | 18" | 12" | #5 | 30" | 30" |
| 8 | 1" | 18" | 12" | #5 | 30" | 30" |
| 9 | 1" | 18" | 12" | #5 | 30" | 30" |
| 10 | 1" | 18" | 12" | #5 | 30" | 30" |
| 11 | 1" | 18" | 12" | #5 | 30" | 30" |
| 12 | 1" | 18" | 12" | #5 | 30" | 30" |
| 13 | 1 - 1/4" | 20" | 15" | #5 | 30" | 30" |
| 14 | 1 - 1/4" | 20" | 15" | #5 | 30" | 30" |
| 15 | 1 - 1/4" | 20" | 15" | #5 | 30" | 30" |
| 16 | 1 - 1/4" | 20" | 15" | #5 | 30" | 30" |
| 17 | 1 - 1/2" | 20" | 18" | #5 | 30" | 30" |
| 18 | 1 - 1/2" | 20" | 18" | #5 | 30" | 30" |

| | DETAIL 1 | DETAIL 2 | DETAIL 3 |
|-------------------------------------|-------------|----------|----------|
| W=WIDTH OF SEALANT RESERVOIR (IN.) | 3/4 +/- 1/8 | 1/2 | 1/2 |
| D=DEPTH OF SEALANT RESERVOIR (IN.) | 3/4 +/- 1/8 | 1/4 | 1/4 |
| B=BACKER ROD DIAMETER (IN.) | 7/8 | 5/8 | 5/8 |
| S=SECOND SAWCUT DEPTH (IN.) MINIMUM | N/A | 1-3/8 | 1-3/8 |

JOINT NOTES

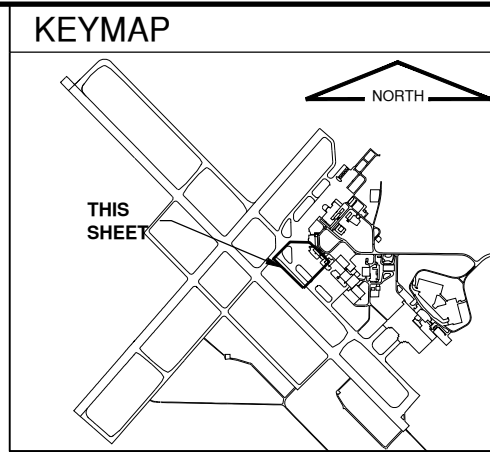
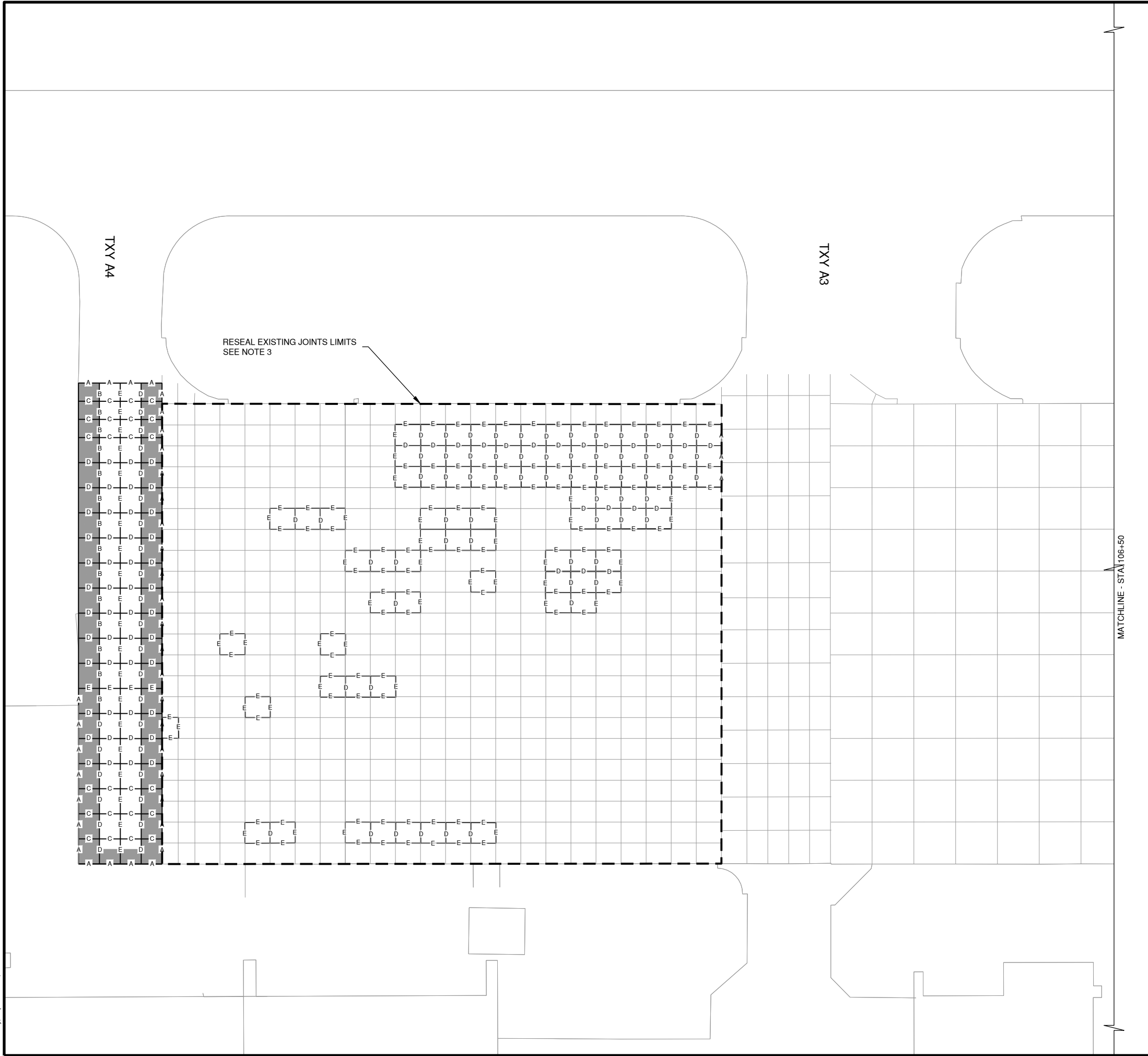
- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A CHAMFER OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A CHAMFER > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.

MARK | DATE | DESCRIPTION

AIP PROJ. NO:
IL PROJ. NO: CMI-5043
CMT PROJECT NO: 21001968
CAD DWG FILE: 21001968-CP500.DWG
DESIGNED BY: MRK
DRAWN BY: MRK
CHECKED BY: MJD
APPROVED BY: CBG
COPYRIGHT:

JOINTING DETAILS

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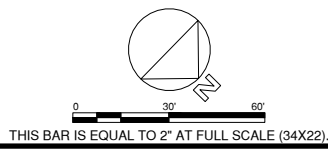
- ### LEGEND
- A — TYPE A ISOLATION JOINT (3/4")
 - B — TYPE B HINGED (TIED) CONTRACTION JOINT
 - C — TYPE C DOWELED CONTRACTION JOINT
 - D — TYPE D DUMMY CONTRACTION JOINT
 - E — TYPE E DOWELED CONSTRUCTION JOINT
 - █ THICKENED EDGE
 - R** REINFORCED PANEL

- ### NOTES
1. THE CONTRACTOR MAY SUBMIT AN ALTERNATE JOINTING/PAVING PLAN FOR REVIEW AND CONSIDERATION NOT LESS THAN 14 DAYS PRIOR TO PAVING OPERATIONS. THE PAVING PLAN SHALL INCLUDE, BUT NOT LIMITED TO, THE CONTRACTORS PROPOSED CHANGES TO PAVING DIRECTION, LANE WIDTHS, JOINTING TYPE, CONTRACTORS PLAN FOR "BOXING OUT" AROUND INLETS, LIGHTS, HAND HOLES, OR OTHER PAVEMENT PENETRATIONS, PLANS TO PREVENT PREMATURE CRACKING IN "BOXED OUT" PANELS, CONTRACTORS PLAN FOR PAVING AGAINST ADJACENT STRUCTURES, AND ANY OTHER REVISION OR ALTERATION THAT THE CONTRACTOR SHOULD PROPOSE THAT DIFFERS FROM THE PLAN SHEETS.
 2. JOINT PLAN SHOWN ASSUMES ADDITIVE ALTERNATE WORK WILL BE INCLUDED. JOINTING MAY BE REVISED TO ADJUST FOR ACTUAL AWARD.
 3. ALL JOINTS NOT REPAIRED BY PCC PANEL REPLACEMENT OR SPALL REPAIRS SHALL BE CLEANED AND RESEALED. THIS WORK SHALL BE PAID FOR UNDER AR605510.



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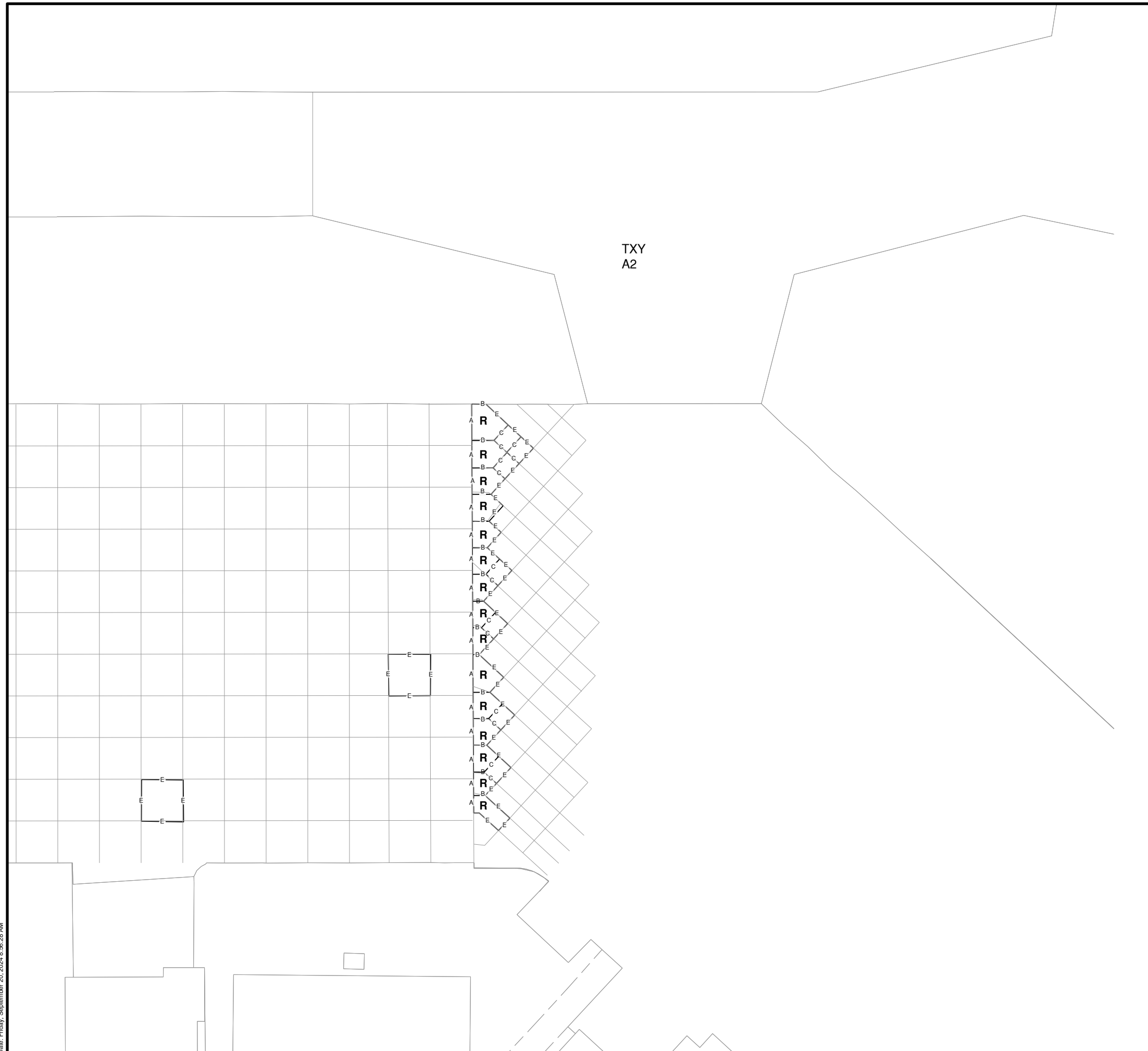


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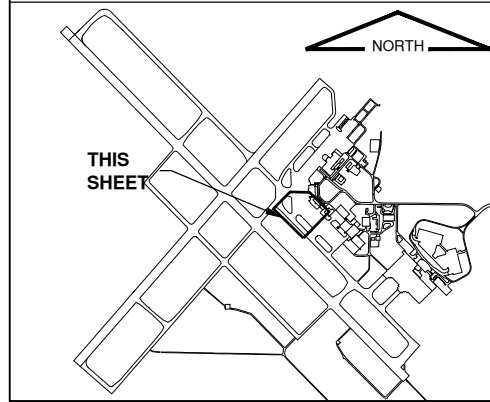
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 IL PROJ. NO: CMI-5043
 CMT PROJECT NO: 21001968
 CAD DWG FILE: 21001968-CJ100.DWG
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 DRAWN BY: MRK
 CHECKED BY: MJD
 APPROVED BY: CBG
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SHEET TITLE
JOINTING PLAN 1
 SHEET 19 OF 26



KEYMAP

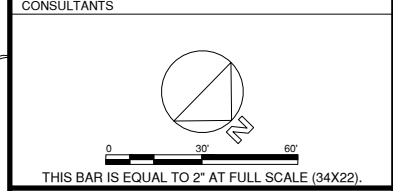


LEGEND

- A — TYPE A ISOLATION JOINT (3/4")
- B — TYPE B HINGED (TIED) CONTRACTION JOINT
- C — TYPE C DOWELED CONTRACTION JOINT
- D — TYPE D DUMMY CONTRACTION JOINT
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- █ THICKENED EDGE
- R** REINFORCED PANEL

NOTES

1. THE CONTRACTOR MAY SUBMIT AN ALTERNATE JOINTING/PAVING PLAN FOR REVIEW AND CONSIDERATION NOT LESS THAN 14 DAYS PRIOR TO PAVING OPERATIONS. THE PAVING PLAN SHALL INCLUDE, BUT NOT LIMITED TO, THE CONTRACTORS PROPOSED CHANGES TO PAVING DIRECTION, LANE WIDTHS, JOINTING TYPE, CONTRACTORS PLAN FOR "BOXING OUT" AROUND INLETS, LIGHTS, HAND HOLES, OR OTHER PAVEMENT PENETRATIONS, PLANS TO PREVENT PREMATURE CRACKING IN "BOXED OUT" PANELS, CONTRACTORS PLAN FOR PAVING AGAINST ADJACENT STRUCTURES, AND ANY OTHER REVISION OR ALTERATION THAT THE CONTRACTOR SHOULD PROPOSE THAT DIFFERS FROM THE PLAN SHEETS.
2. JOINT PLAN SHOWN ASSUMES ADDITIVE ALTERNATE WORK WILL BE INCLUDED. JOINTING MAY BE REVISED TO ADJUST FOR ACTUAL AWARD.



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| IL PROJ. NO: CMI-5043 | |
| CMT PROJECT NO: 21001968 | |
| CAD DWG FILE: | 21001968-CJ100.DWG |
| DESIGNED BY: | MRK |
| DRAWN BY: | MRK |
| CHECKED BY: | MJD |
| APPROVED BY: | CBG |
| COPYRIGHT: | |

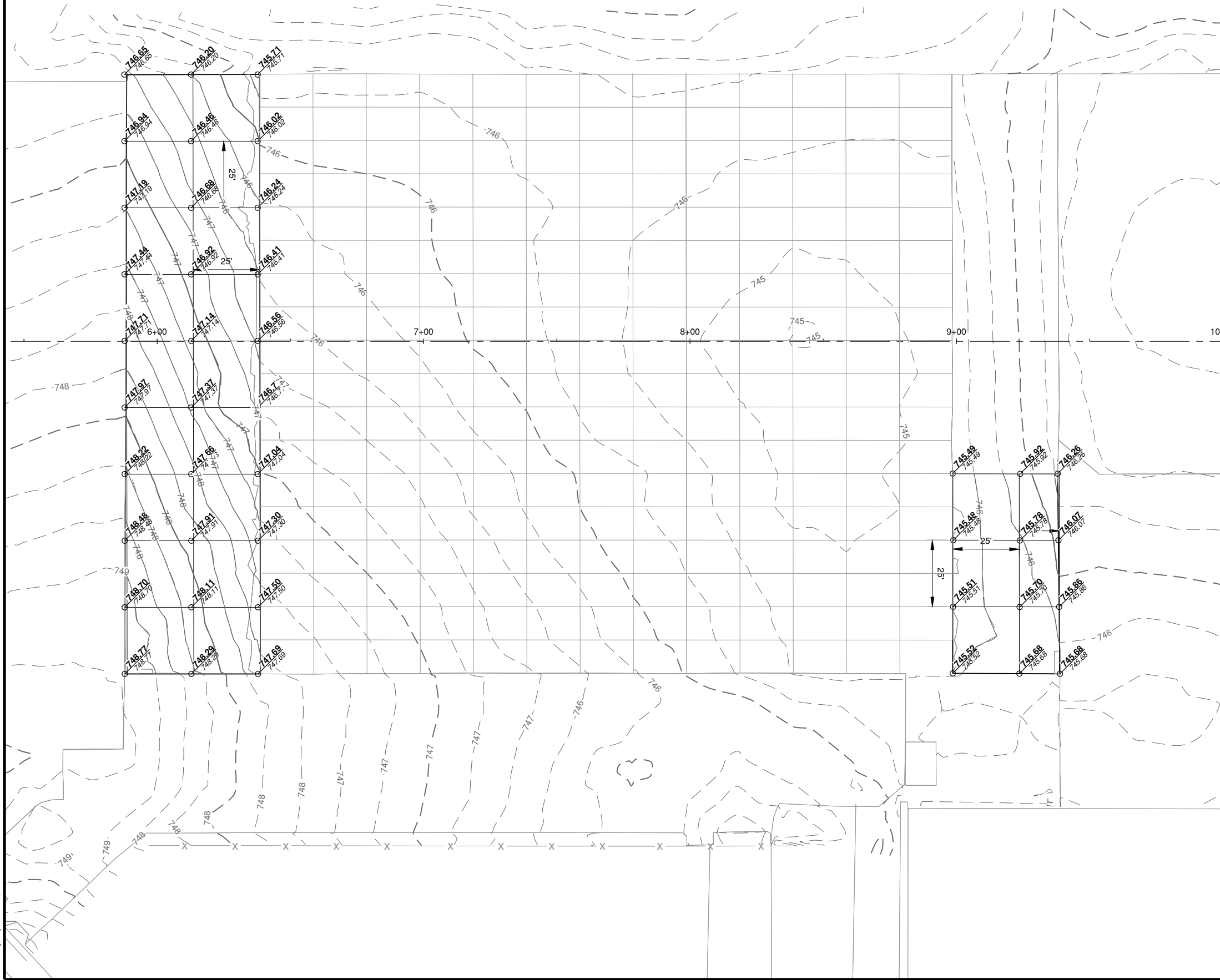
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JOINTING PLAN 2

CJ102

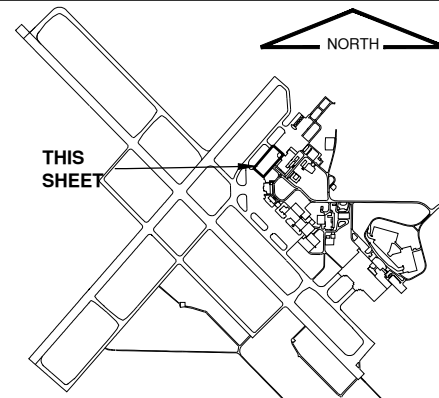
SHEET **20** OF **26**

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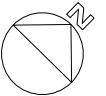


KEYMAP



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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

LEGEND

- PROPOSED CONTOURS
- EXISTING CONTOURS
- SPOT ELEVATION

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 NOVEMBER 08, 2024

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 SAVOY, IL

NOTES

1. MATCH EXISTING GRADES AT ALL BUILDINGS, PAVEMENTS, CATCH BASINS, AND OTHER SURFACE FEATURES (CLEAN OUTS, MANHOLES, ETC.).

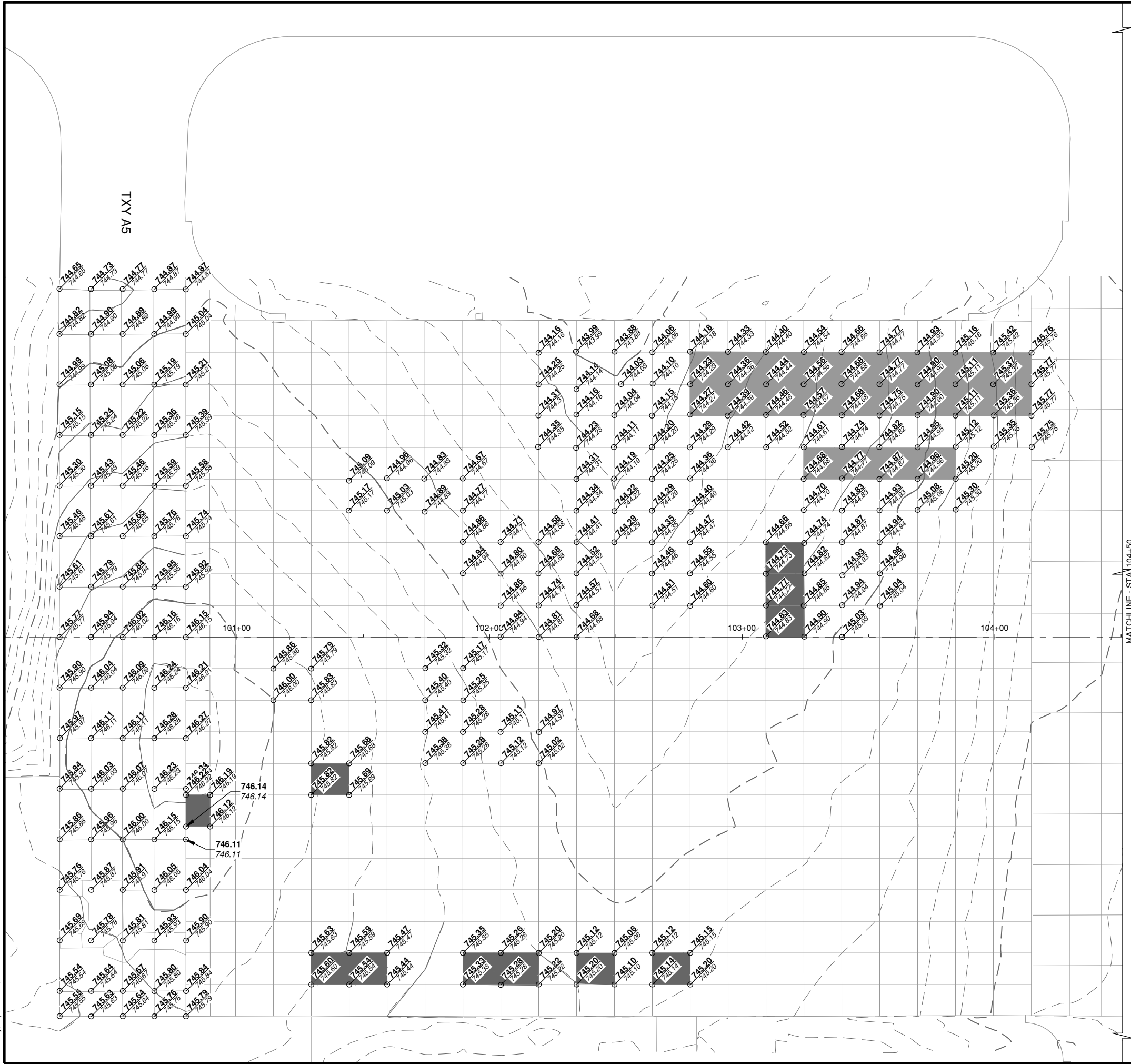
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| CMT PROJECT NO: 21001968 | |
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| DRAWN BY: MRK | |
| CHECKED BY: MJD | |
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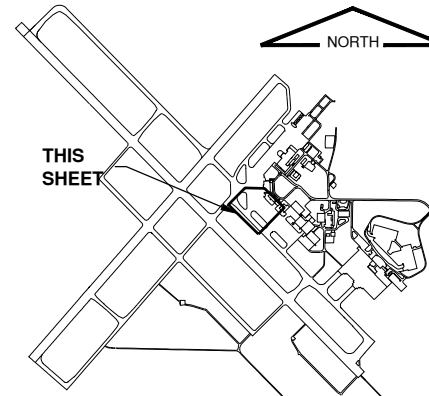
SHEET TITLE
STAKING PLAN 1

CS101
 SHEET 21 OF 26






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KEYMAP



LEGEND

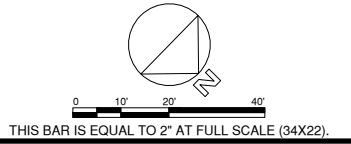
-  PROPOSED CONTOURS
-  EXISTING CONTOURS
-  SPOT ELEVATION
-  ADD ALT 1
-  ADD ALT 2

NOTES

1. MATCH EXISTING GRADES AT ALL BUILDINGS, PAVEMENTS, CATCH BASINS, AND OTHER SURFACE FEATURES (CLEAN OUTS, MANHOLES, ETC.).



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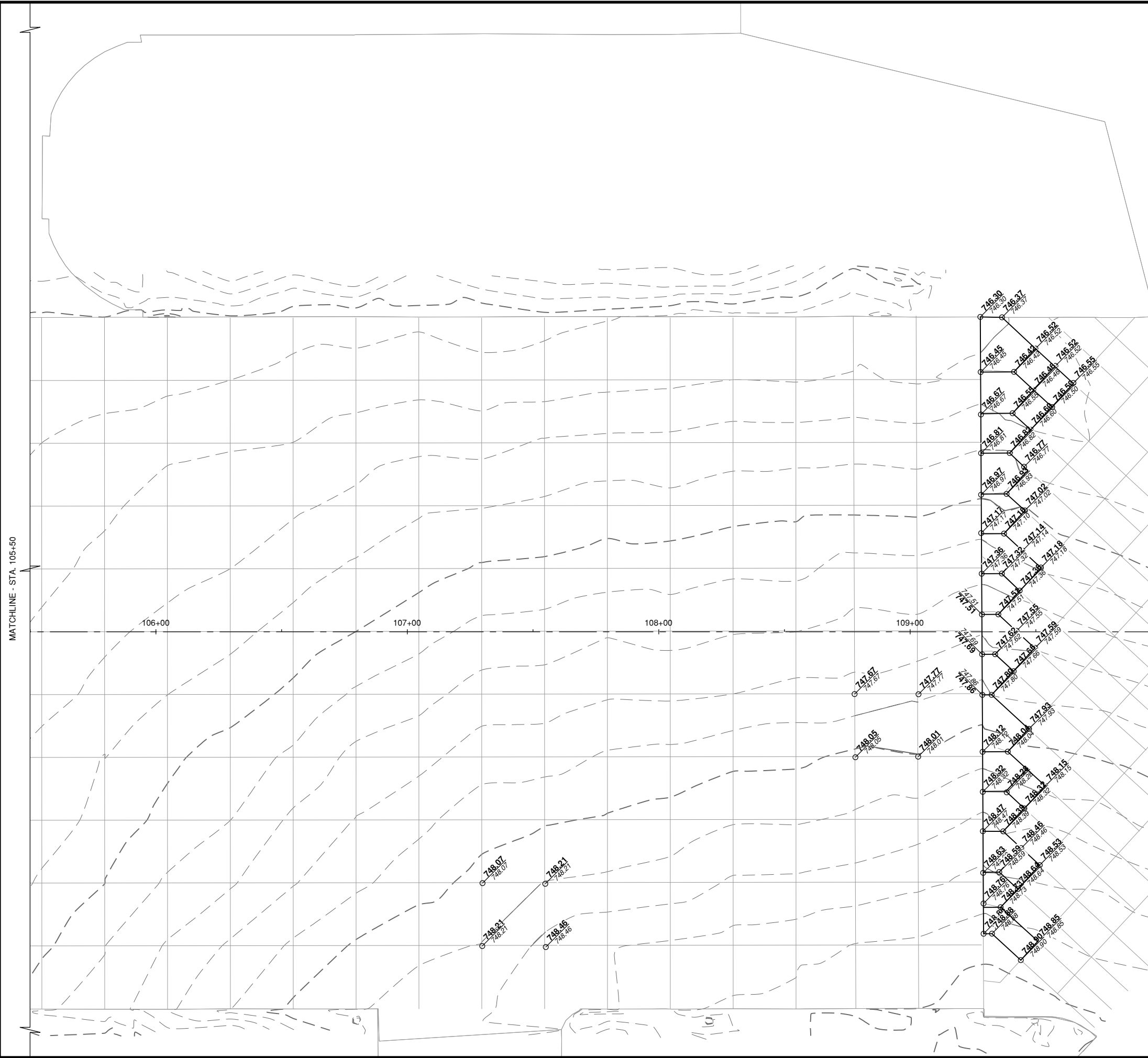
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SAVOY, IL

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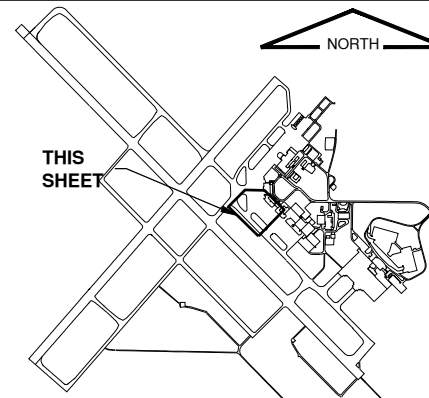
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 DESIGNED BY: MRK
 DRAWN BY: MRK
 CHECKED BY: MJD
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 COPYRIGHT:

SHEET TITLE
STAKING PLAN 2

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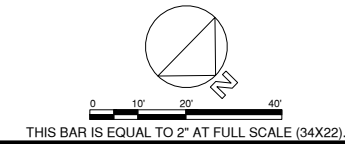


KEYMAP



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LEGEND

- PROPOSED CONTOURS
- EXISTING CONTOURS
- SPOT ELEVATION

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 NOVEMBER 08, 2024

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NOTES

1. MATCH EXISTING GRADES AT ALL BUILDINGS, PAVEMENTS, CATCH BASINS, AND OTHER SURFACE FEATURES (CLEAN OUTS, MANHOLES, ETC.).

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| DRAWN BY: MRK | |
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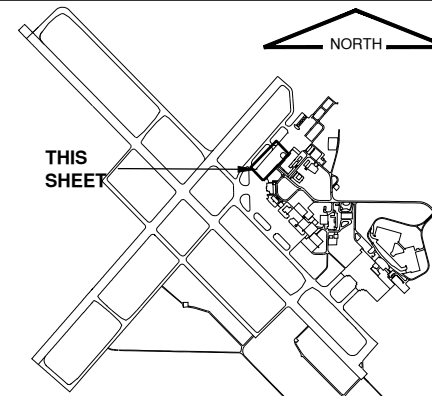
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STAKING PLAN 3

TXY C

TXY C4

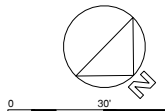
TXY C3

KEYMAP



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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

LEGEND

- NEW MARKING
- EXISTING MARKING
- NEW TIE DOWN LAYOUT MARKING
- NEW TIE DOWN - TYPE A

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NOVEMBER 08, 2024

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NOTES

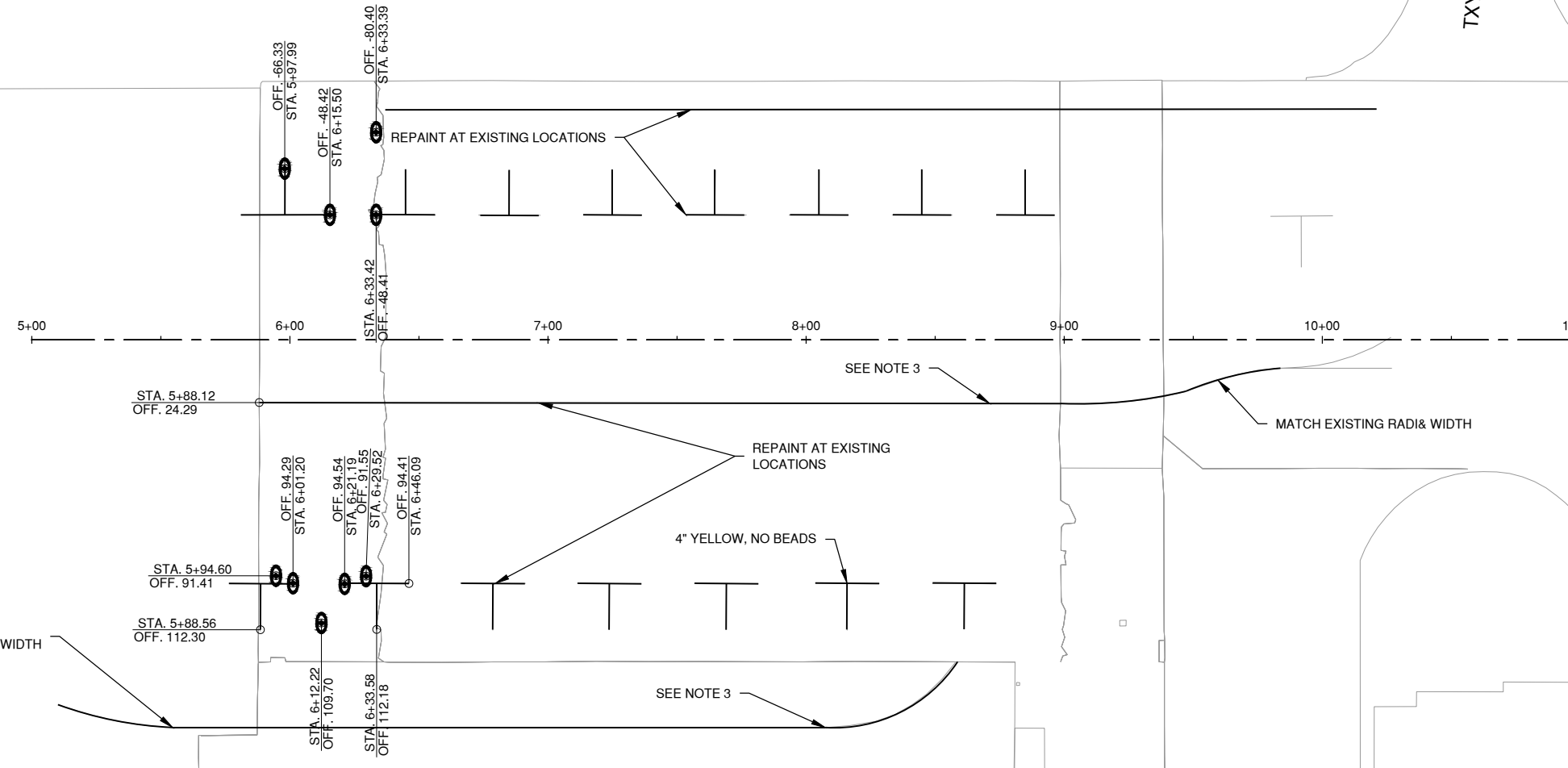
1. MATCH EXISTING TAXIWAY CENTERLINES.
2. EXISTING MARKINGS TO BE REPAINTED SHALL BE PREPARED ACCORDING TO SPECIFICATIONS PRIOR TO REMARKING.
3. TAXIWAY CENTERLINES SHALL BE SOLID YELLOW, 6" IN WIDTH AND WILL INCLUDE A 3" BLACK BORDER.

MARK | DATE | DESCRIPTION

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| AIP PROJ. NO: | |
| IL PROJ. NO: CMI-5043 | |
| CMT PROJECT NO: 21001968 | |
| CAD DWG FILE: 21001968-CM100.DWG | |
| DESIGNED BY: MRK | |
| DRAWN BY: MRK | |
| CHECKED BY: MJD | |
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SHEET TITLE
MARKING AND TIE DOWN PLAN 1

CM101
SHEET 24 OF 26



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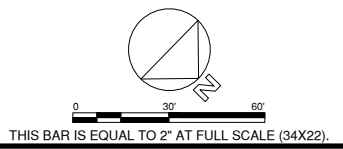
INSTITUTE OF AVIATION

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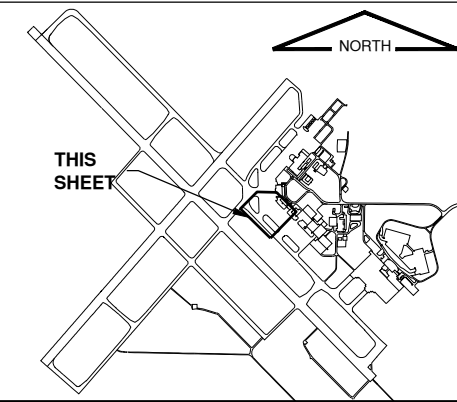


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KEYMAP



LEGEND

- NEW MARKING
- EXISTING MARKING
- NEW TIE DOWN LAYOUT MARKING
- NEW TIE DOWN - TYPE B
- NEW GROUND ROD - AA1

NOTES

1. MATCH EXISTING TAXIWAY CENTERLINES.
2. EXISTING MARKINGS TO BE REPAINTED SHALL BE PREPARED ACCORDING TO SPECIFICATIONS PRIOR TO REMARKING.
3. TAXIWAY CENTERLINES SHALL BE SOLID YELLOW, 6" IN WIDTH AND WILL INCLUDE A 3" BLACK BORDER.

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NOVEMBER 08, 2024

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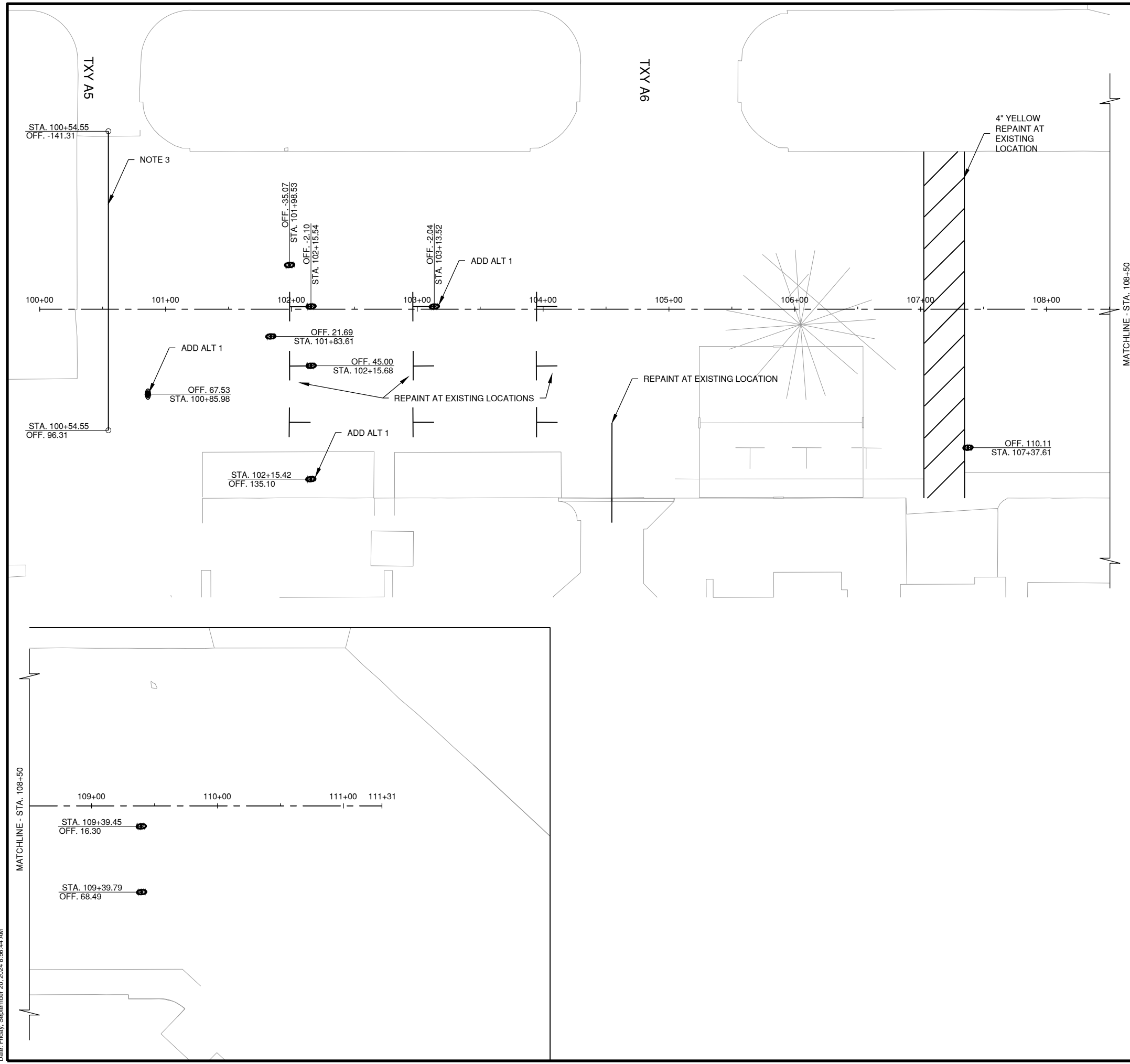
UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, IL

MARK | DATE | DESCRIPTION

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| IL PROJ. NO: CM1-5043 | |
| CMT PROJECT NO: 21001968 | |
| CAD DWG FILE: 21001968-CM100.DWG | |
| DESIGNED BY: MRK | |
| DRAWN BY: MRK | |
| CHECKED BY: MJD | |
| APPROVED BY: CBG | |
| COPYRIGHT: | |

MARKING AND TIE DOWN PLAN 2

CM102
SHEET 25 OF 26



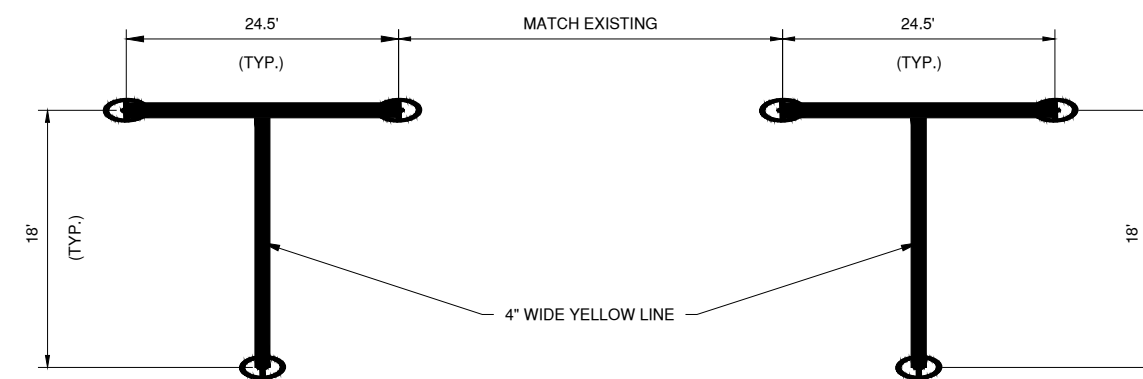
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| IL PROJ. NO: | CM1-5043 |
| CMT PROJECT NO: | 21001968 |
| CAD DWG FILE: | 21001968-CM500.DWG |
| DESIGNED BY: | MRK |
| DRAWN BY: | MRK |
| CHECKED BY: | MJD |
| APPROVED BY: | CBG |
| COPYRIGHT: | |

SHEET TITLE
**MARKING AND TIE
DOWN DETAILS**

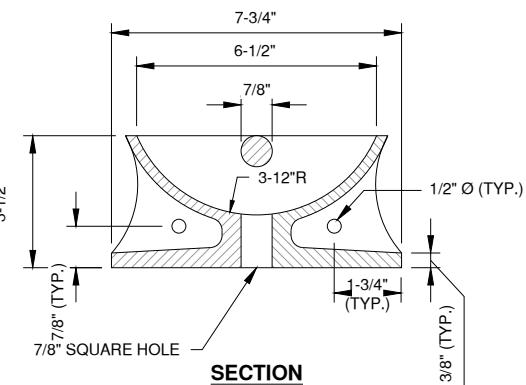
4 TAXIWAY CENTERLINE DETAIL
N.T.S.



5 TIE DOWN LAYOUT DETAIL
N.T.S.

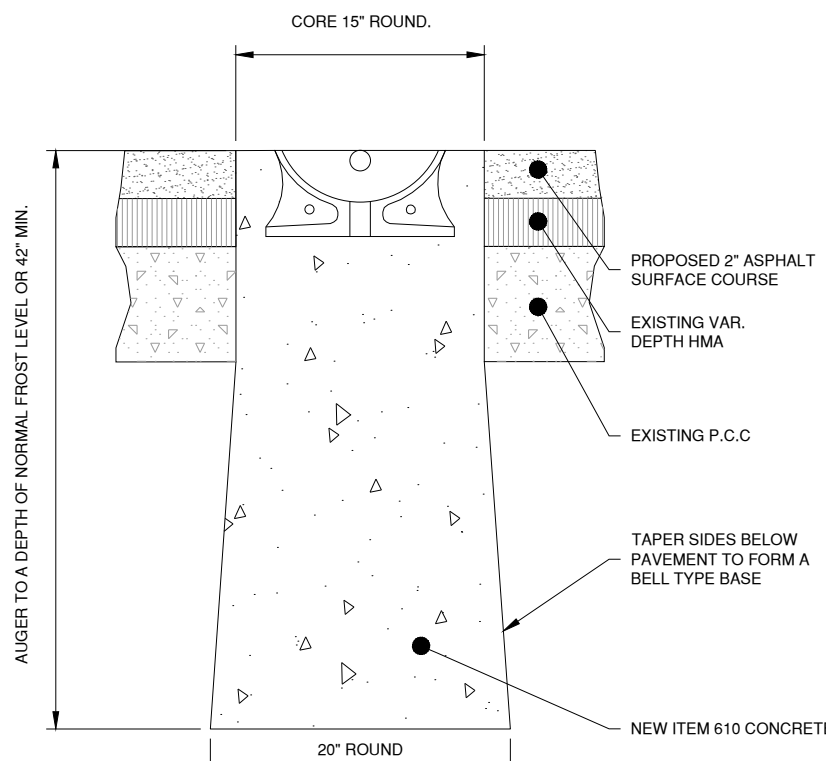
- NOTE:**
- ALL DIMENSIONS AND SPACINGS ARE TO BE FIELD VARIED PRIOR TO CONSTRUCTION. PROPOSED DIMENSIONS AND SPACINGS SHALL MATCH EXISTING CONDITIONS.

PLAN

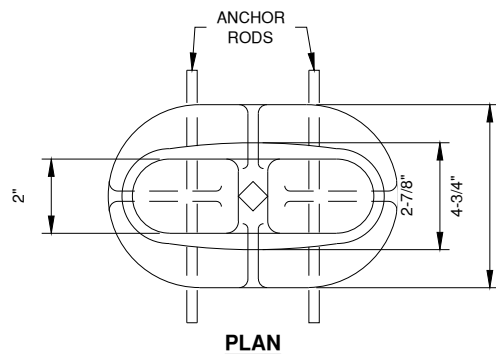


SECTION

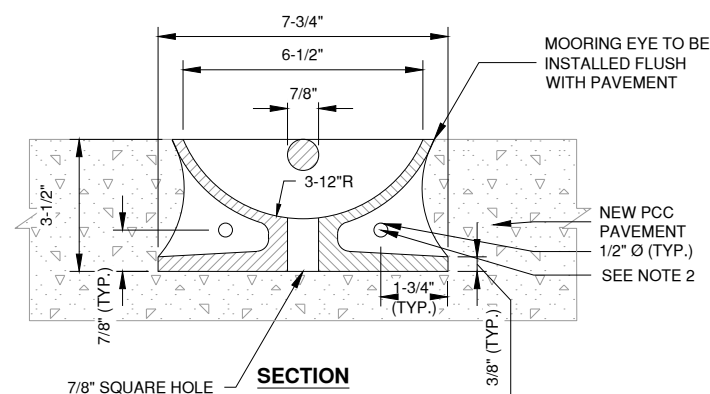
1 TIE DOWN DETAIL (TYPE A)
N.T.S.



1

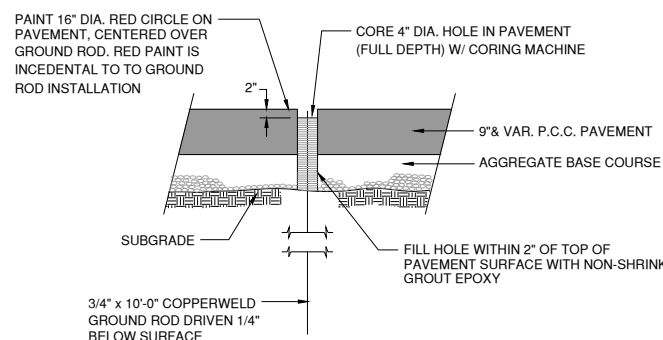


PLAN



SECTION

2 TIE DOWN DETAIL (TYPE B)
N.T.S.



3 GROUND ROD DETAIL
N.T.S.

NOTES:

- MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. R3490 OR APPROVED EQUIVALENT.
- ANCHOR RODS TO BE NO. 3 DEFORMED REINFORCING STEEL 15" LONG AND SHALL BE BENT DOWNWARD AT 45°.
- TYPE A MOORING EYES SHALL BE INSTALLED AFTER PAVING OPERATIONS ARE COMPLETE BY CORING THROUGH NEW PAVEMENT.